

GRE MAY 2026
PROJECT ID: WITH: N/A

1130-73-71

COUNTY:

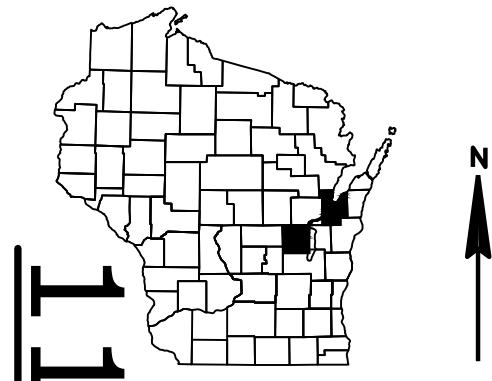
BROWN & WINNEBAGO

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT OSHKOSH - GREEN BAY USH 45 - IH 43 IH 41 BROWN & WINNEBAGO COUNTIES

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1130-73-71		

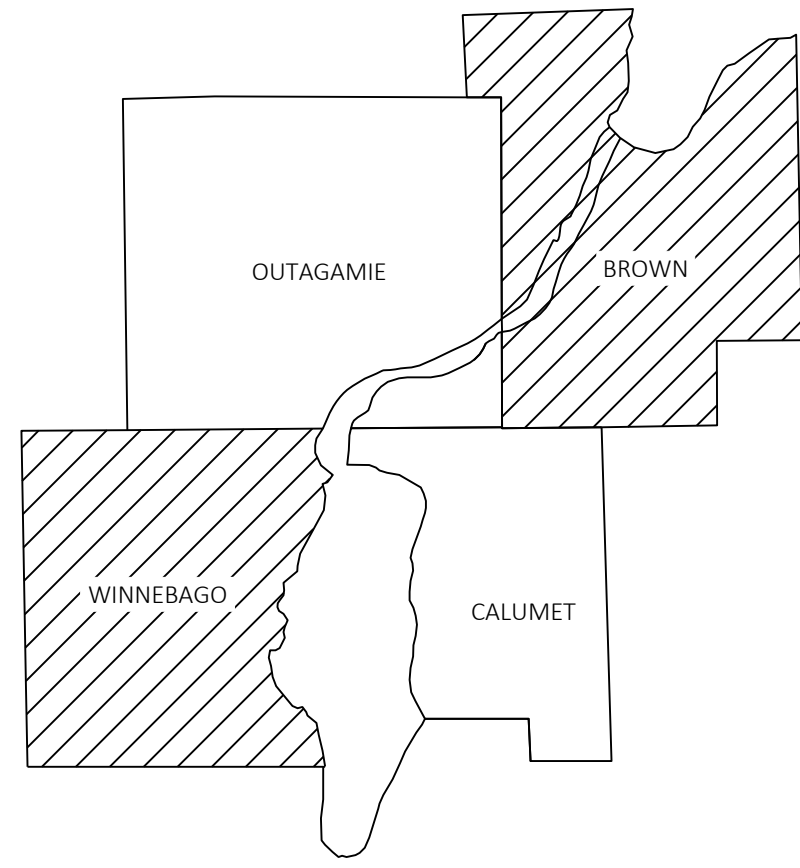
STATE PROJECT NUMBER
1130-73-71



- DESIGN DESIGNATION**
- A.A.D.T. =
 - A.A.D.T. =
 - D.H.V. =
 - D.D. =
 - T. =
 - DESIGN SPEED =
 - ESALS =

CONVENTIONAL SYMBOLS

PLAN		PROFILE	
CORPORATE LIMITS		GRADE LINE	
PROPERTY LINE		ORIGINAL GROUND	
LOT LINE		MARSH OR ROCK PROFILE (To be noted as such)	
LIMITED HIGHWAY EASEMENT		SPECIAL DITCH	
EXISTING RIGHT OF WAY		GRADE ELEVATION	
PROPOSED OR NEW R/W LINE		CULVERT (Profile View)	
SLOPE INTERCEPT		UTILITIES	
REFERENCE LINE		ELECTRIC	
EXISTING CULVERT		FIBER OPTIC	
PROPOSED CULVERT (Box or Pipe)		GAS	
COMBUSTIBLE FLUIDS		SANITARY SEWER	
MARSH AREA		STORM SEWER	
WOODED OR SHRUB AREA		TELEPHONE	
		WATER	
		UTILITY PEDESTAL	
		POWER POLE	
		TELEPHONE POLE	



LAYOUT
SCALE 0 _____
TOTAL NET LENGTH OF CENTERLINE = N/A

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), BROWN COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY	NE REGION
Surveyor	_____
Designer	P. KOZLOSKI
Project Manager	J. LANG
Regional Examiner	NE REGION
Regional Supervisor	A. FULCER

APPROVED FOR THE DEPARTMENT
DATE: 2/1/2026 P.E.
(Signature)

E

GENERAL NOTES

THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS. THE CONTRACTOR SHALL COORDINATE THEIR CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

THE CONTRACTOR WILL ENSURE THE EPOXY MATERIAL WILL NOT REACH SURFACE WATER. REPORT ANY INCIDENTAL SPILLS TO THE WDNR OFFICE WITHIN 24 HOURS OR THE TOLL FREE HOTLINE, (800) 943-0003, IF WDNR CONTACT IS NOT AVAILABLE.

ANY INCIDENTAL GROUND DISTURBANCES WILL BE RESTORED AS SOON AS CONDITIONS PERMIT, USE APPROPRIATE EROSION CONTROL DEVICES UNTIL RESTORATION OF DISTURBED SOILS ARE COMPLETE.

ANY MATERIAL INADVERTENTLY FALLING ONTO THE STREAMBED AND BANKS AND/OR WETLANDS SHALL BE REMOVED IMMEDIATELY.

WASTE OR EXCESS MATERIALS SHALL NOT BE DISPOSED OF IN FLOORPLAINS, WETLANDS OR WATERWAYS.

ORDER OF SECTION 2 DETAIL SHEETS

- GENERAL NOTES
- PROJECT OVERVIEW
- TRAFFIC CONTROL
- STAGE CONSTRUCTION
- DETOUR PLAN

CONTACTS

WISCONSIN DNR LIAISON

BROWN COUNTY
JIM DOPERALSKI, JR.
2984 SHAWANO AVENUE
GREEN BAY, WI 54313
(920) 412-0165
james.doperalski@wisconsin.gov

WISCONSIN DNR LIAISON

WINNEBAGO COUNTY
JAY SCHIEFELBEIN
2984 SHAWANO AVENUE
GREEN BAY, WI 54313
(920) 360-3784
jeremiah.schiefelbein@wisconsin.gov

BROWN COUNTY HIGHWAY COMMISSIONER

CHRIS HARDY, P.E.
2198 GLENDALE AVENUE
GREEN BAY, WI 54303
(920) 492-4925
chris.hardy@browncountywi.gov

WINNEBAGO COUNTY HIGHWAY COMMISSIONER

ROBERT DOEMEL, JR.
901 WEST COUNTY ROAD Y
OSHKOSH, WI 54901
(920) 232-1713
rdoemel@co.winnebago.wi.us

NE REGION SURVEY COORDINATOR

CORMAC MCINNIS, P.L.S.
944 VANDERPERREN WAY
GREEN BAY, WI 54304
(920) 492-5638
Cormac.McInnis@dot.wi.gov

NE REGION DESIGN PROJECT MANAGER

JOSHUA LANG, P.E.
944 VANDERPERREN WAY
GREEN BAY, WI 54304
(920) 492-4141
Joshua.Lang@dot.wi.gov

RYAN CAROLFI
 ANR PIPELINE CO - GAS/PETROLEUM
 10255 SOUTH WASHINGTON AVE
 MARSHFIELD, WI 54449
 (920) 344-4653
 ryan.carolfi@tcenergy.com

DUSTIN TEAFF
 BUG TUSSEL WIRELESS LCC - COMMUNICATION
 1262 CAMBER CT
 GREEN BAY, WI 54301
 (920) 254-3539
 dustin.teaff@kesexcavating.com

JUSTIN GIERACH
 CITY OF OSHKOSH - WATER/SEWER
 215 CHURCH AVE
 P.O. BOX 1130
 OSHKOSH, WI 54903-1130
 (920) 236-5065
 jgierach@ci.oshkosh.wi.us

ROB REINHART
 NEW WATER - SEWER
 2231 NORTH QUINCY ST
 GREEN BAY, WI 54302-1248
 (920) 438-1035
 rreinhart@newwater.us

CHARLES BARTELT
 AT&T WISCONSIN - COMMUNICATION
 70 E DIVISION ST
 FOND DU LAC, WI 54935
 (920) 929-1013
 cb1461@att.com

DOUG VOSBERG
 ATC MANAGEMENT, INC. - ELECTRICITY-TRANSMISSION/COMMUNICATION
 2489 RINDEN RD
 COTTAGE GROVE, WI 53527
 (608) 877-7650
 dvosberg@@atcllc.com

RYAN BIRDSALL
 LEVEL 3 COMMUNICATIONS LLC - COMMUNICATION
 710 E MIFFLIN ST
 MADISON, WI 53703
 (608) 658-2899
 ryan.birdsall@lumen.com

JEFF ROTHERMEL
 SPECTRUM - COMMUNICATION LINE
 3545 PLANK RD
 APPLETON, WI 54915
 (920) 496-1821
 jeff.rothermel@charter.com

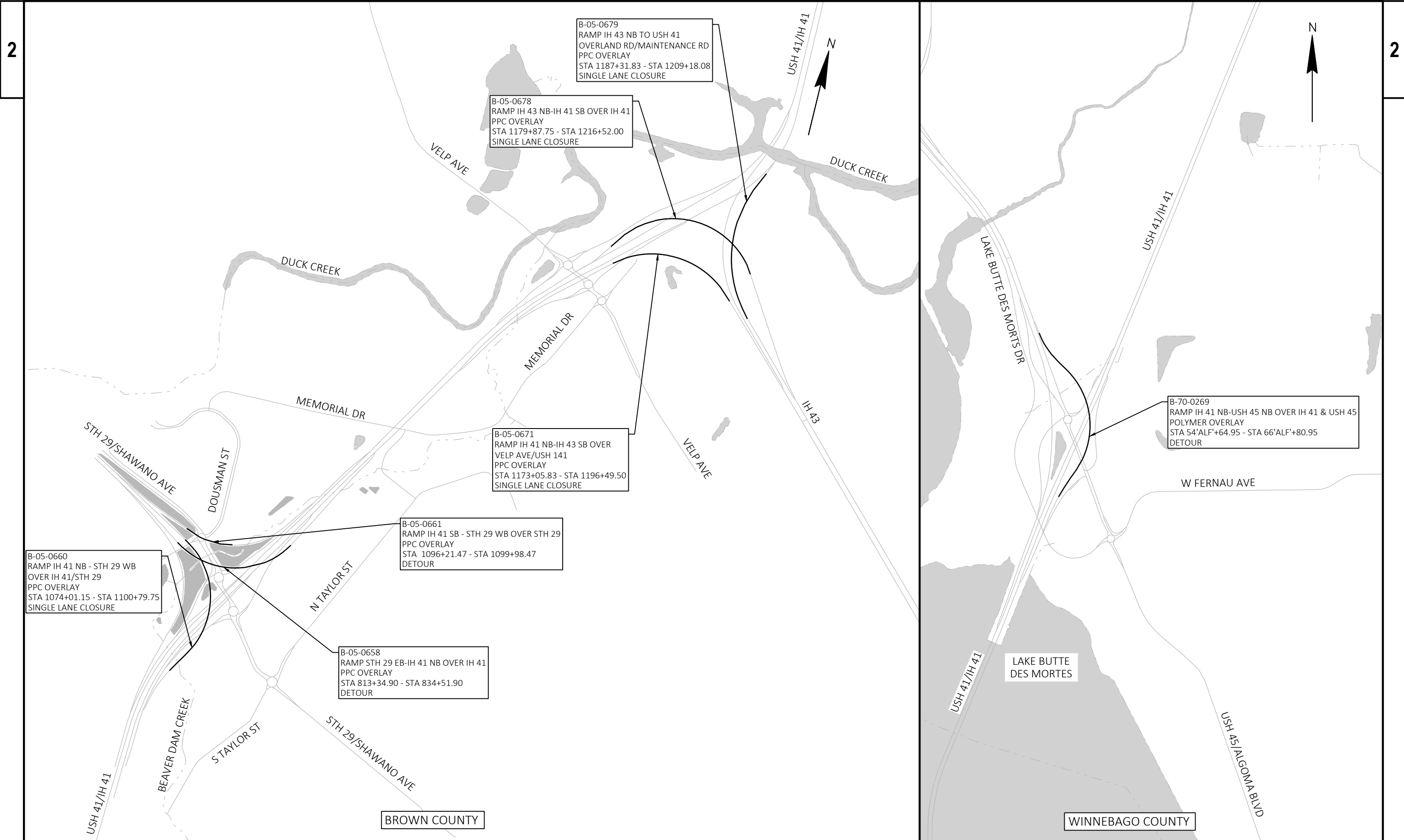
SCOTT ZELLNER
 WISCONSIN PUBLIC SERVICE CORP - ELECTRICITY
 2850 S ASHLAND AVE
 GREEN BAY, WI 54304
 (920) 617-5068
 scott.zellner@wisconsinpublicservice.com

ADAM LEY
 WISCONSIN PUBLIC SERVICE CORP - GAS/PETROLEUM
 2850 S ASHLAND AVE
 GREEN BAY, WI 54304
 (920) 304-2053
 adam.ley@wisconsinpublicservice.com

LORI KETTER
 WINDSTREAM KDL, LLC - COMMUNICATION
 969 WAUBE LN
 GREEN BAY, WI 54304
 (920) 410-6902
 lori.ketter@windstream.com

ERIC BECKER
 WIN TECHNOLOGY - COMMUNICATION
 4955 BULLIS FARM RD
 EAU CLAIRE, WI 53701
 (715) 797-2724
 eric.becker@wintechnology.com





2






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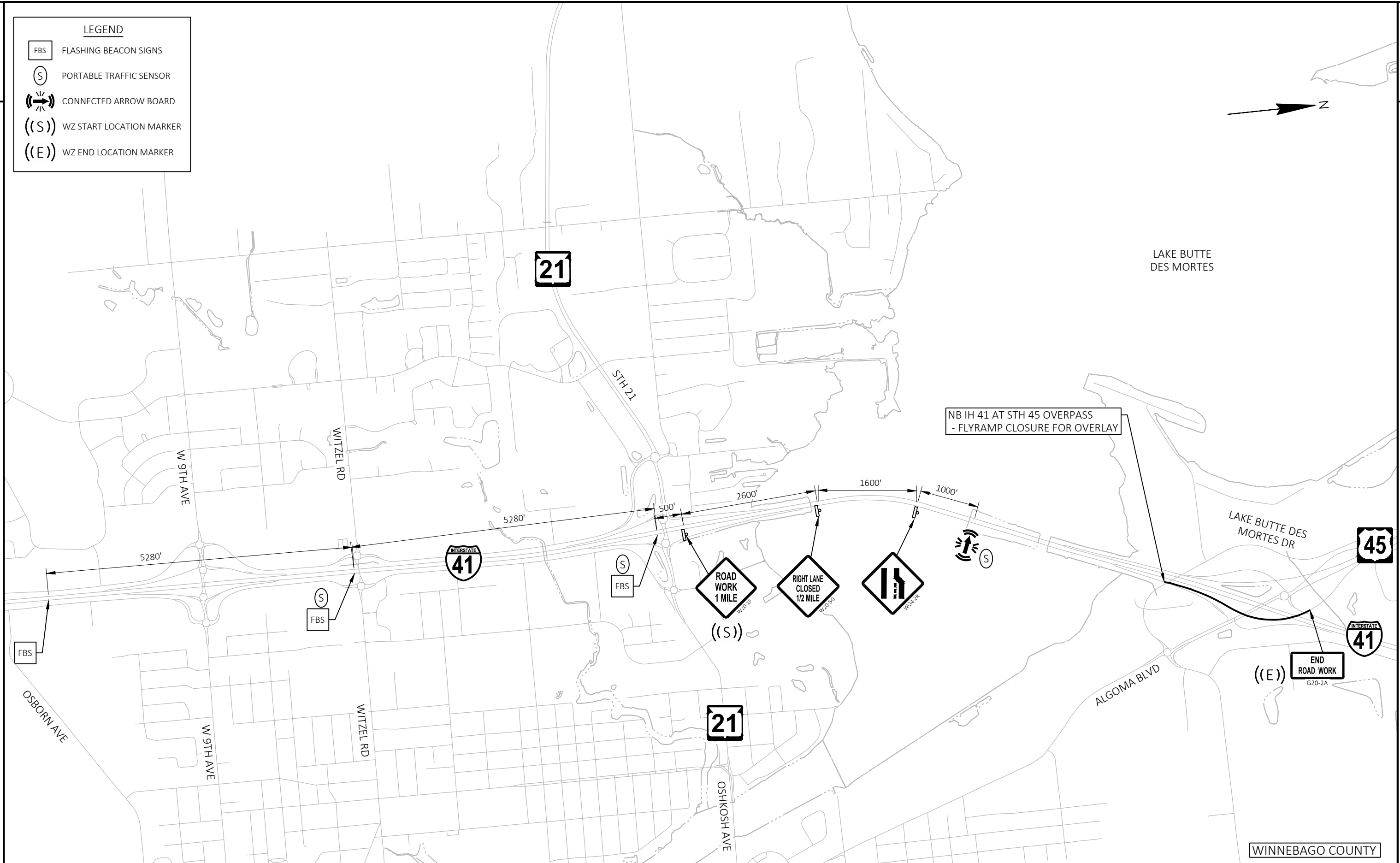
BROWN COUNTY

WINNEBAGO COUNTY

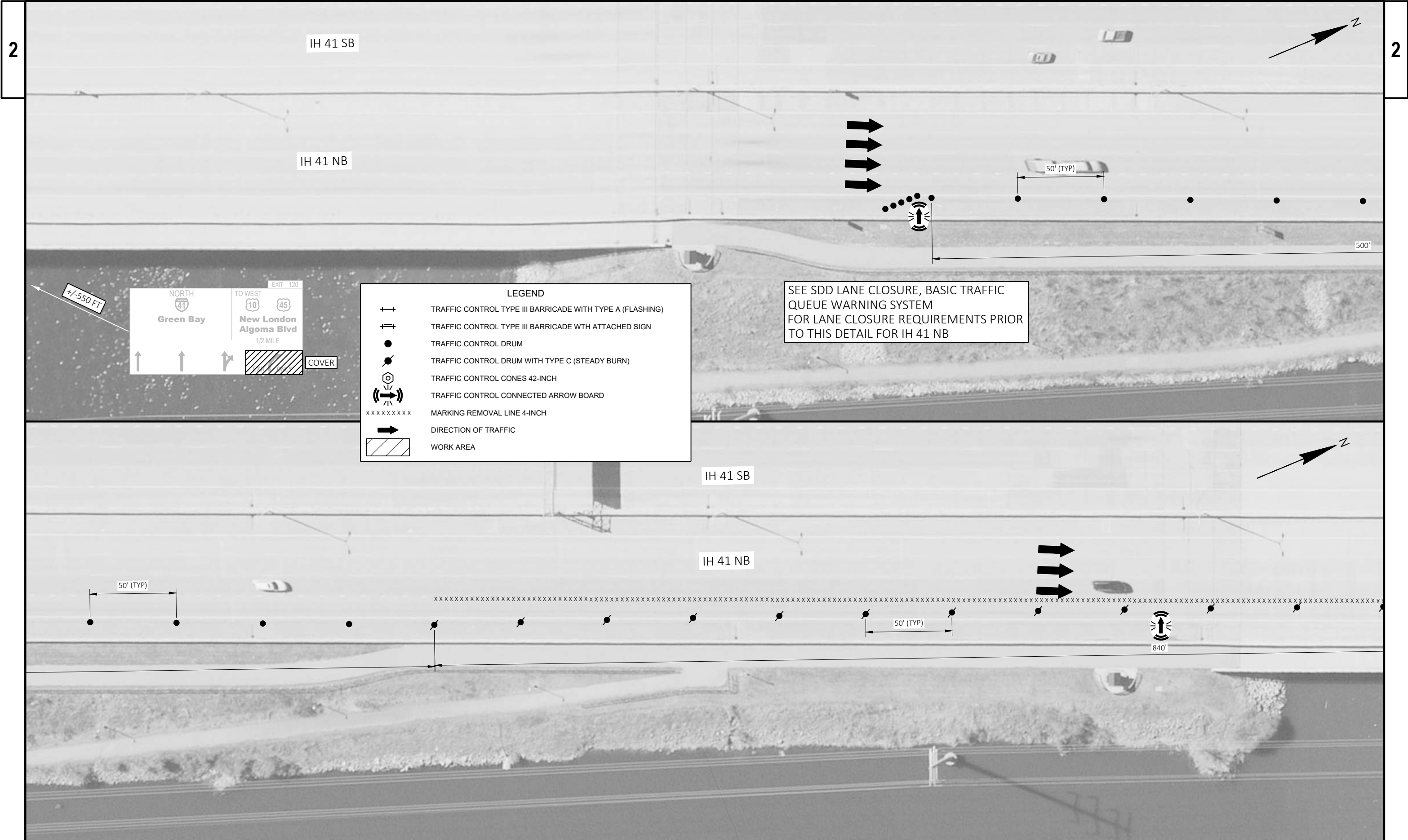
PROJECT NO: 1130-73-71	HWY: IH 41	COUNTY: BROWN & WINNEBAGO	PROJECT OVERVIEW	SHEET	E
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LEGEND

-  FLASHING BEACON SIGNS
-  PORTABLE TRAFFIC SENSOR
-  CONNECTED ARROW BOARD
-  WZ START LOCATION MARKER
-  WZ END LOCATION MARKER



PROJECT NO: 1130-73-71	HWY: IH 41	COUNTY: BROWN & WINNEBAGO	TRAFFIC CONTROL - IH 41 & ALGOMA BLVD	SHEET	E
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IH 41 SB

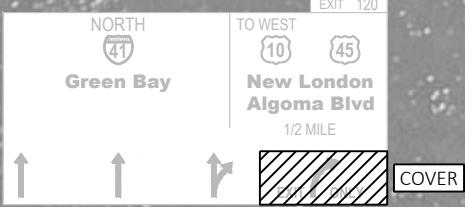
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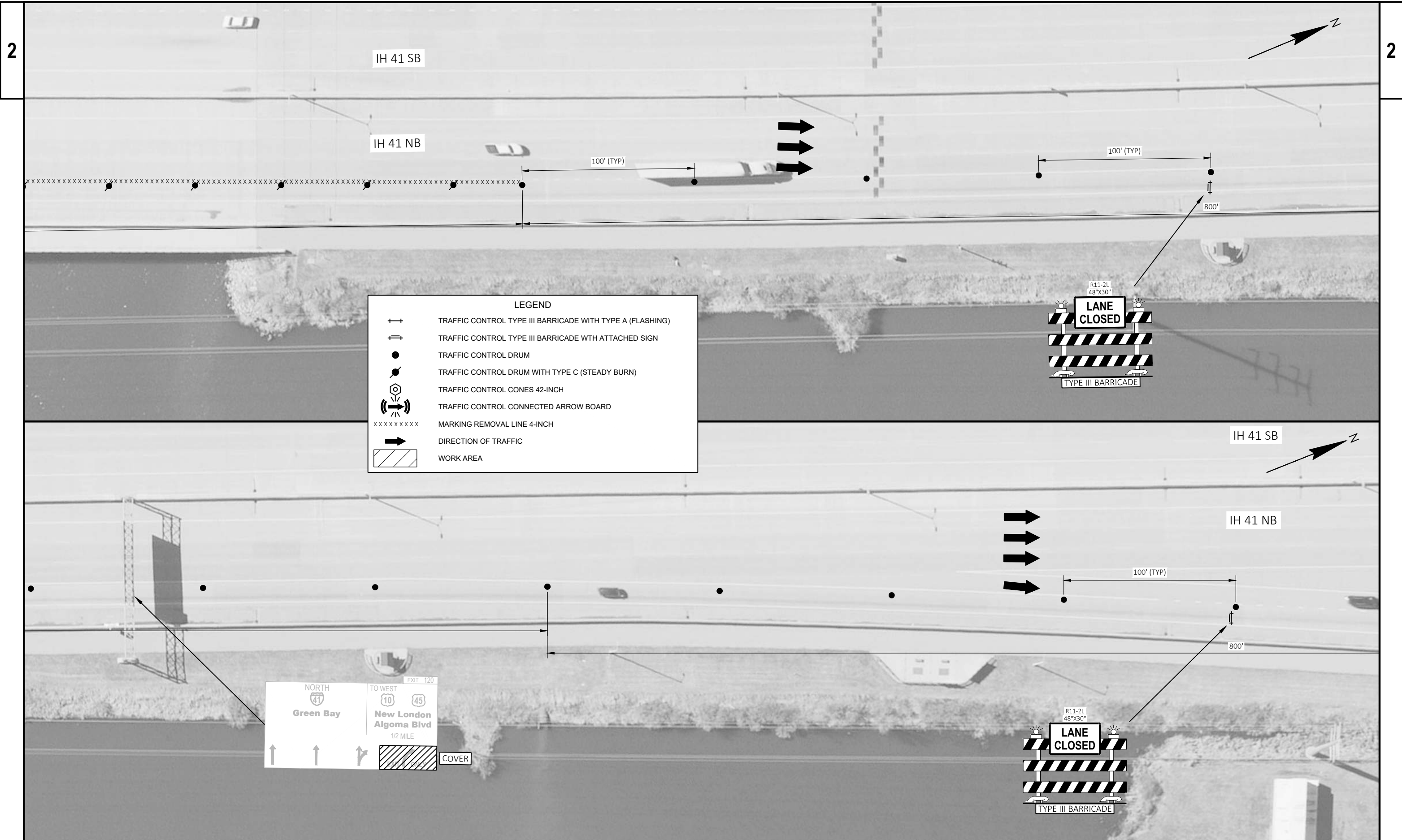
IH 41 SB

IH 41 NB

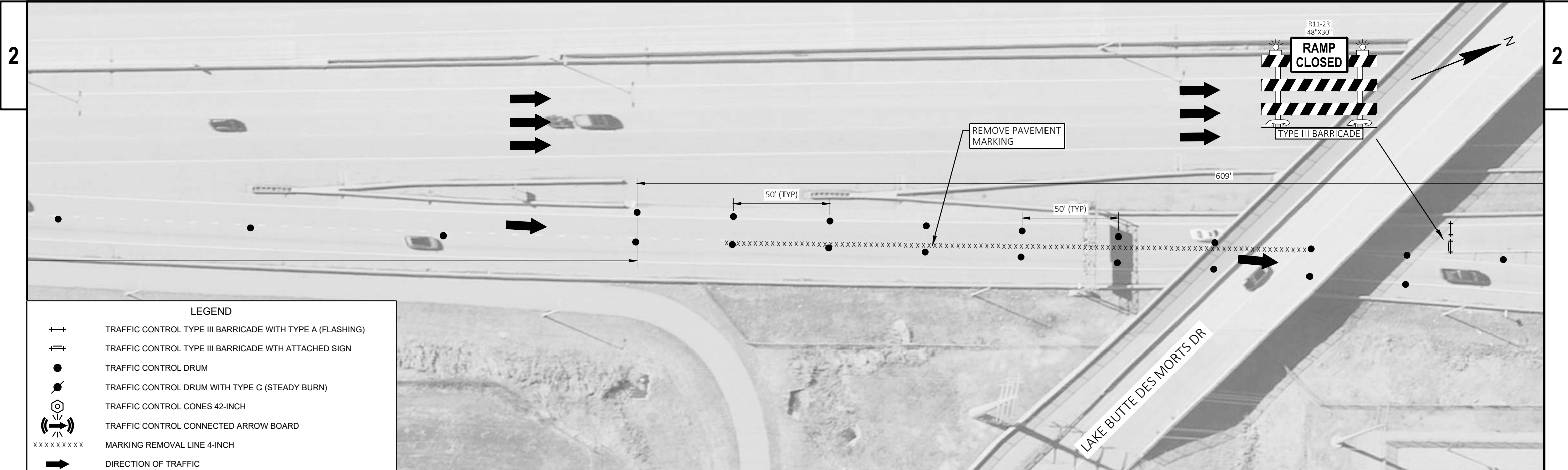
LEGEND	
	TRAFFIC CONTROL TYPE III BARRICADE WITH TYPE A (FLASHING)
	TRAFFIC CONTROL TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C (STEADY BURN)
	TRAFFIC CONTROL CONES 42-INCH
	TRAFFIC CONTROL CONNECTED ARROW BOARD
	MARKING REMOVAL LINE 4-INCH
	DIRECTION OF TRAFFIC
	WORK AREA

SEE SDD LANE CLOSURE, BASIC TRAFFIC QUEUE WARNING SYSTEM FOR LANE CLOSURE REQUIREMENTS PRIOR TO THIS DETAIL FOR IH 41 NB





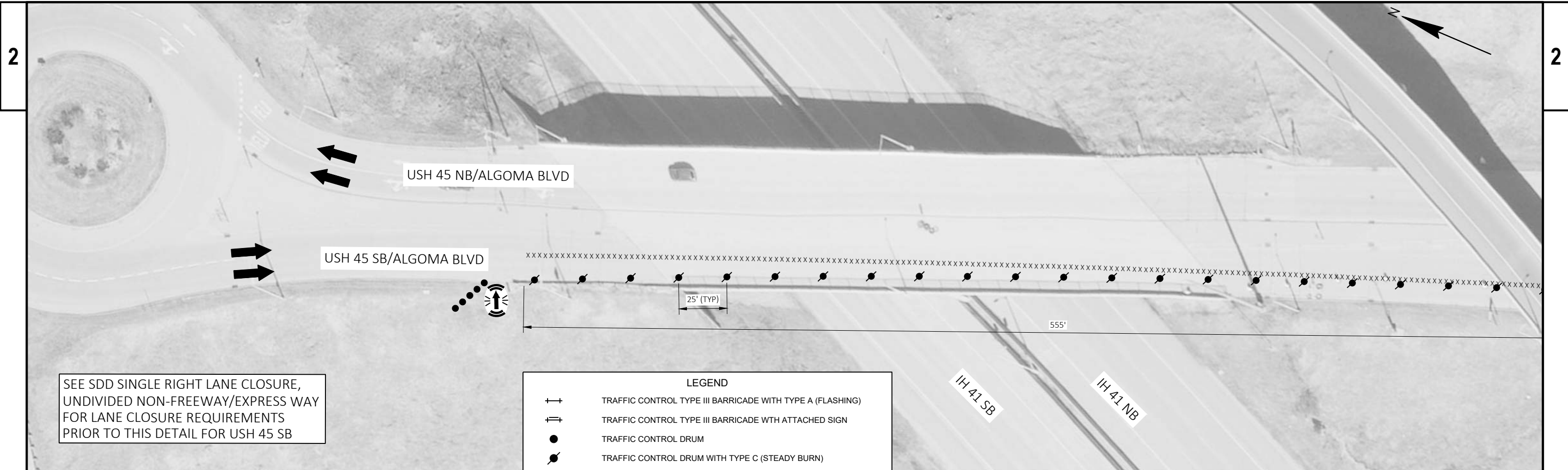
LEGEND	
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	TRAFFIC CONTROL CONNECTED ARROW BOARD
	MARKING REMOVAL LINE 4-INCH
	DIRECTION OF TRAFFIC
	WORK AREA



LEGEND

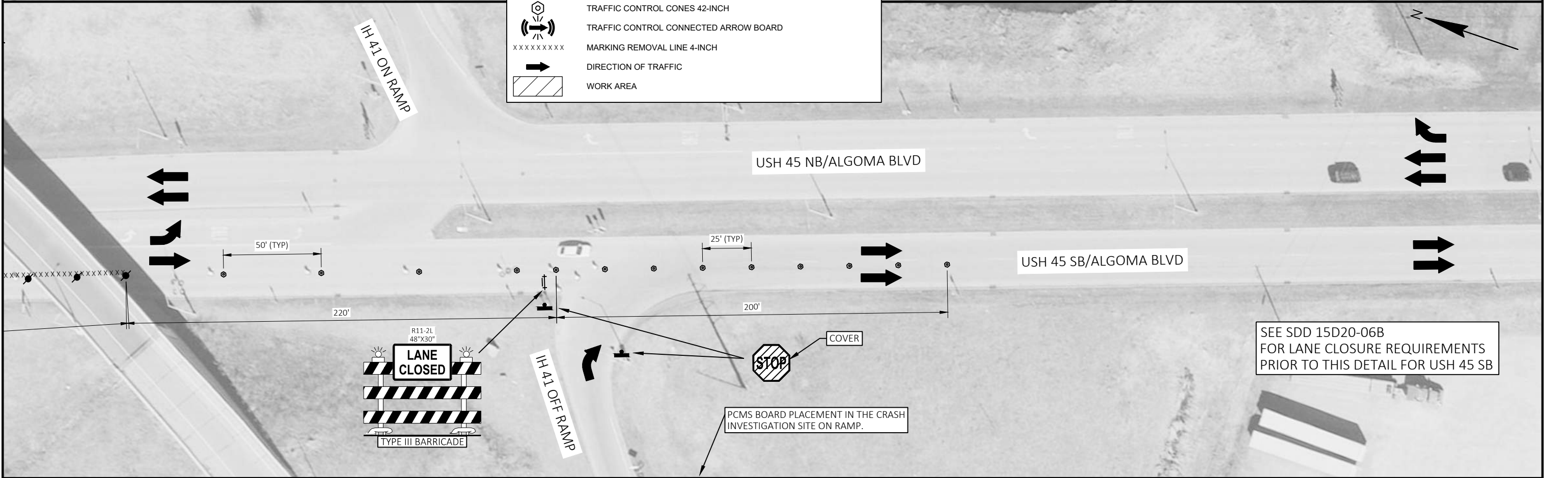
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	TRAFFIC CONTROL TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C (STEADY BURN)
	TRAFFIC CONTROL CONES 42-INCH
	TRAFFIC CONTROL CONNECTED ARROW BOARD
	MARKING REMOVAL LINE 4-INCH
	DIRECTION OF TRAFFIC
	WORK AREA










SEE SDD SINGLE RIGHT LANE CLOSURE, UNDIVIDED NON-FREEWAY/EXPRESS WAY FOR LANE CLOSURE REQUIREMENTS PRIOR TO THIS DETAIL FOR USH 45 SB

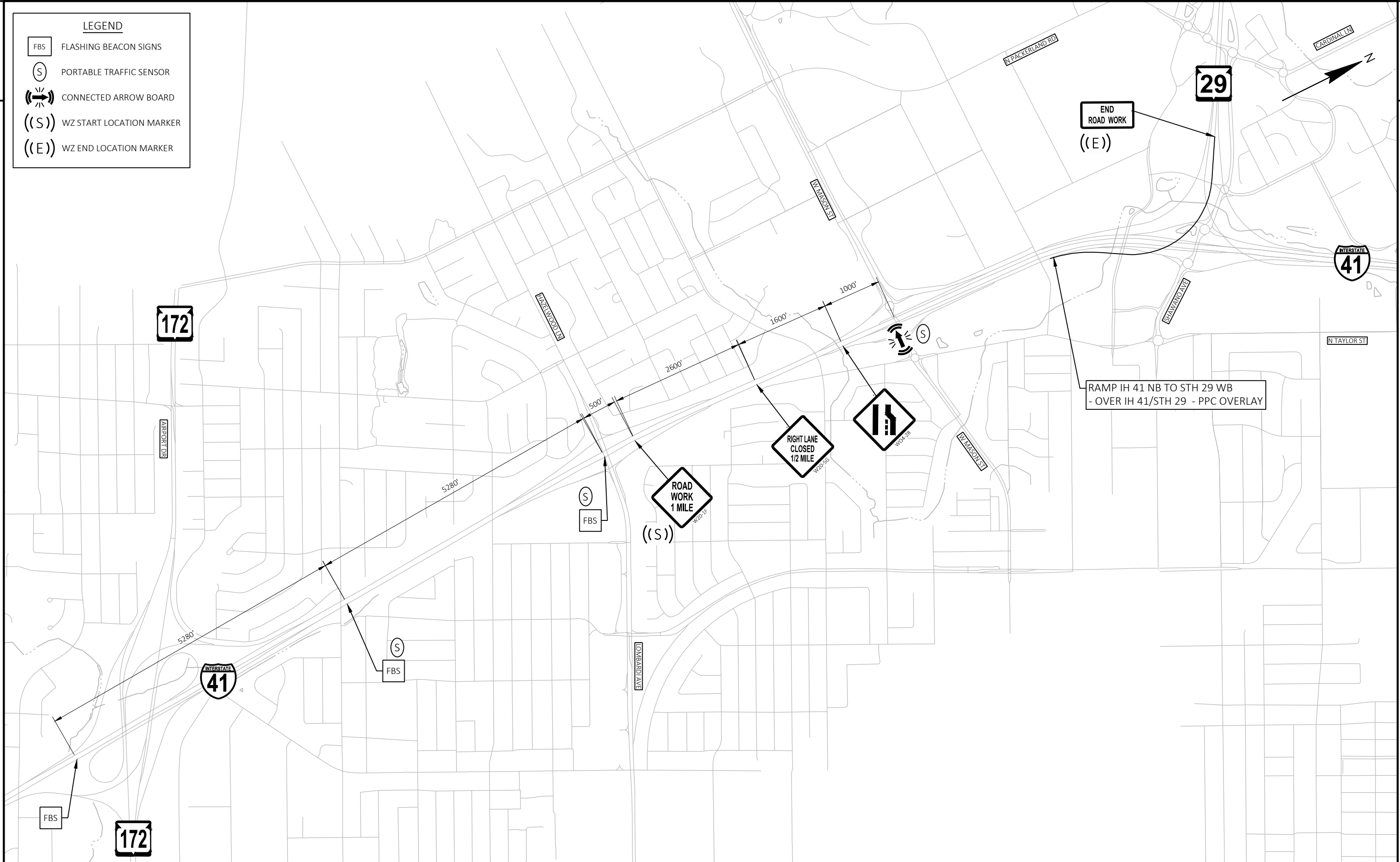
LEGEND	
	TRAFFIC CONTROL TYPE III BARRICADE WITH TYPE A (FLASHING)
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	TRAFFIC CONTROL CONES 42-INCH
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	MARKING REMOVAL LINE 4-INCH
	DIRECTION OF TRAFFIC
	WORK AREA

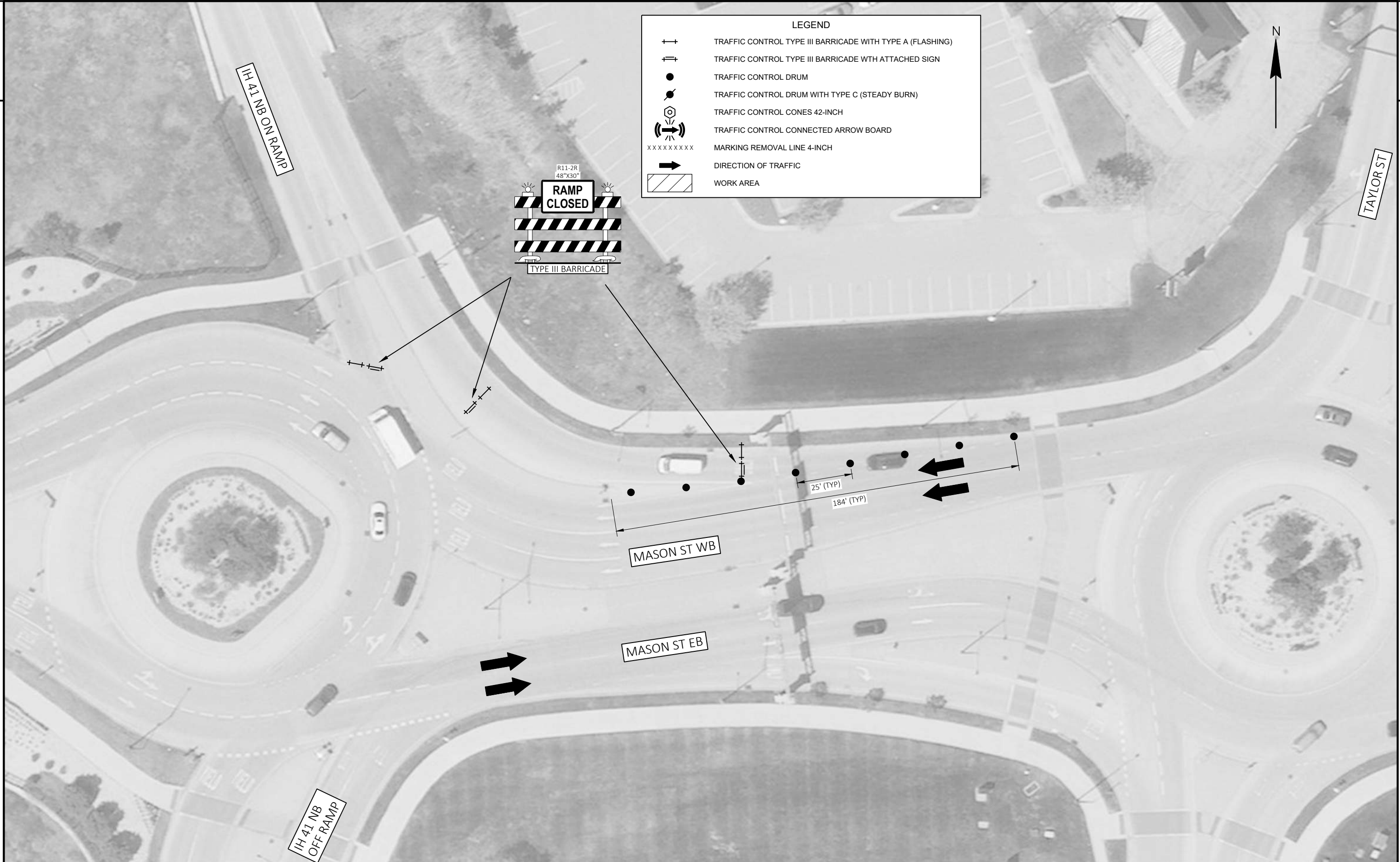


SEE SDD 15D20-06B FOR LANE CLOSURE REQUIREMENTS PRIOR TO THIS DETAIL FOR USH 45 SB

LEGEND

-  FLASHING BEACON SIGNS
-  PORTABLE TRAFFIC SENSOR
-  CONNECTED ARROW BOARD
-  WZ START LOCATION MARKER
-  WZ END LOCATION MARKER

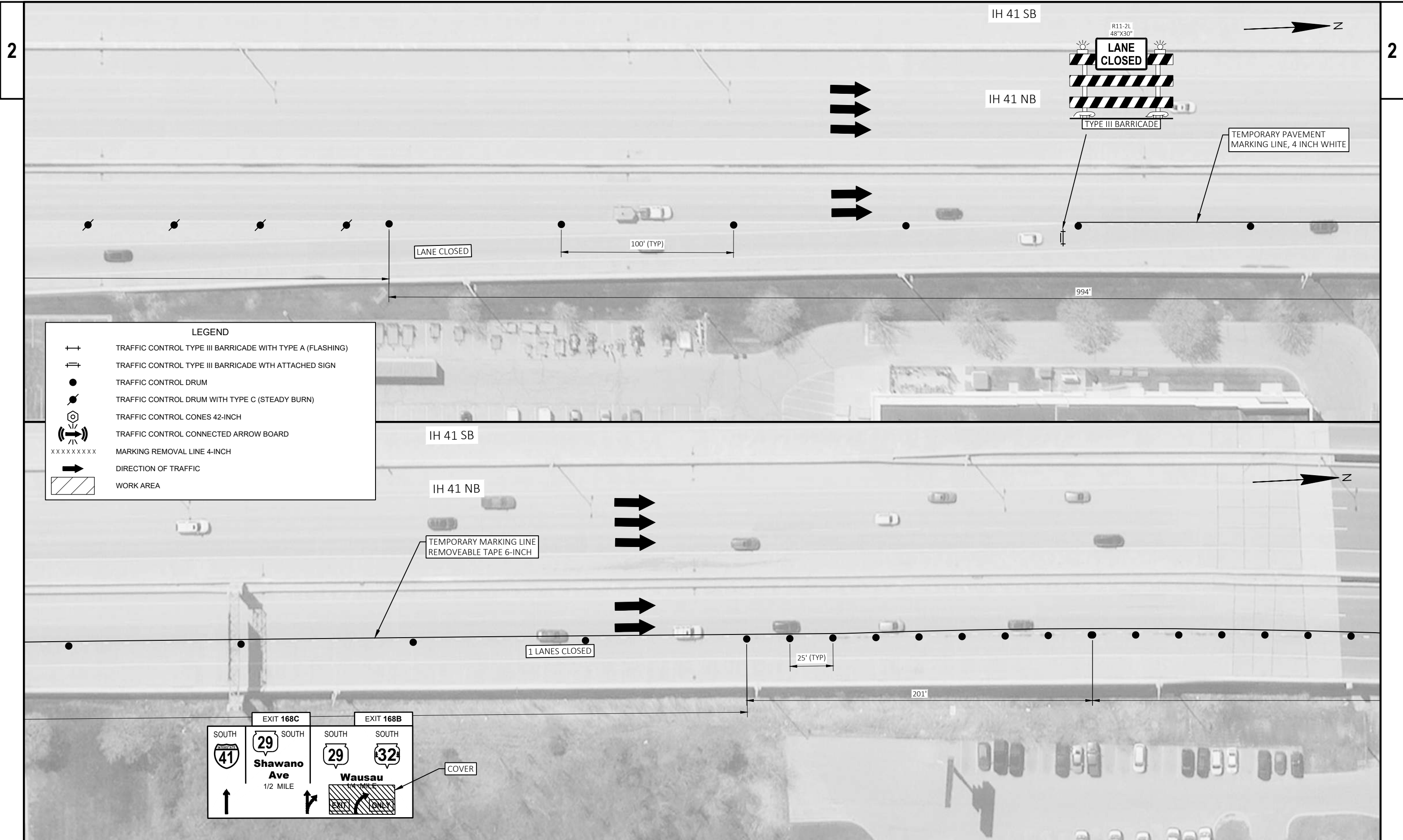




PROJECT NO: 1130-73-71	HWY: IH 41	COUNTY: BROWN & WINNEBAGO	TRAFFIC CONTROL - USH 54 & MASON ST	SHEET	E
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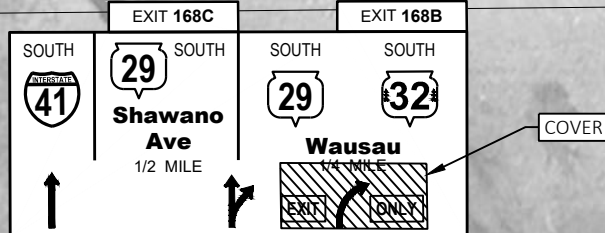


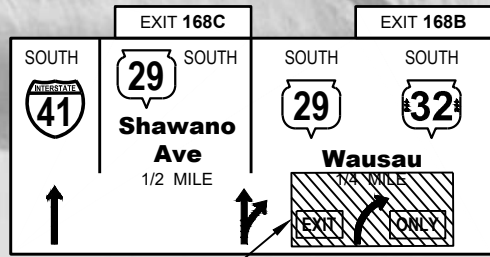
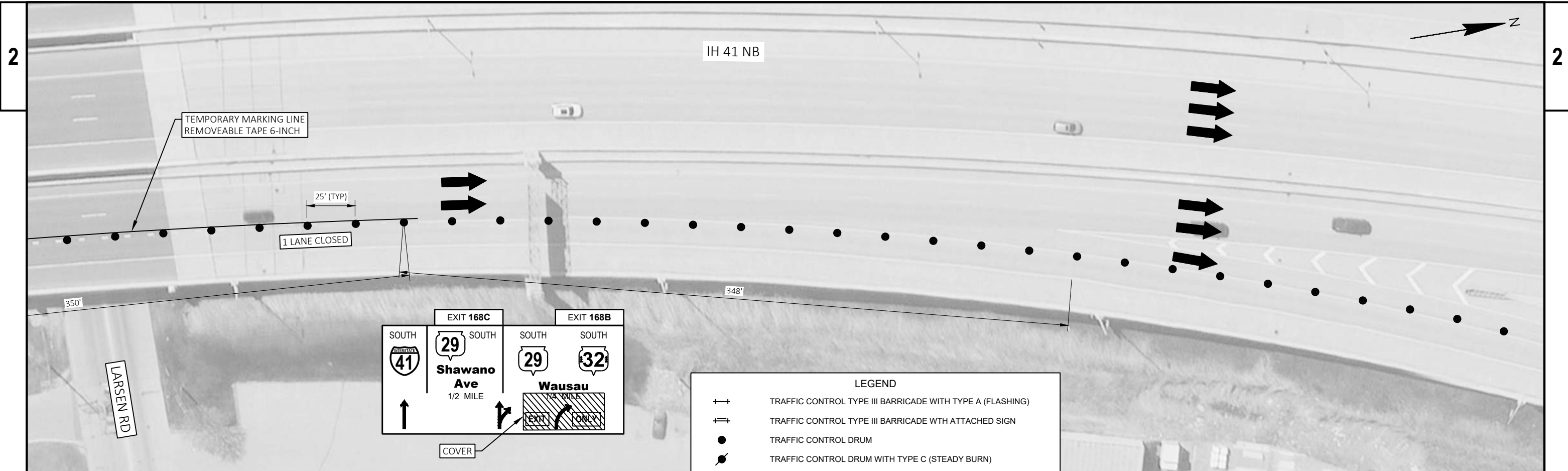
PROJECT NO: 1130-73-71	HWY: IH 41	COUNTY: BROWN & WINNEBAGO	TRAFFIC CONTROL - IH 41 NB TO USH 29 WB - STAGE 1	SHEET	E
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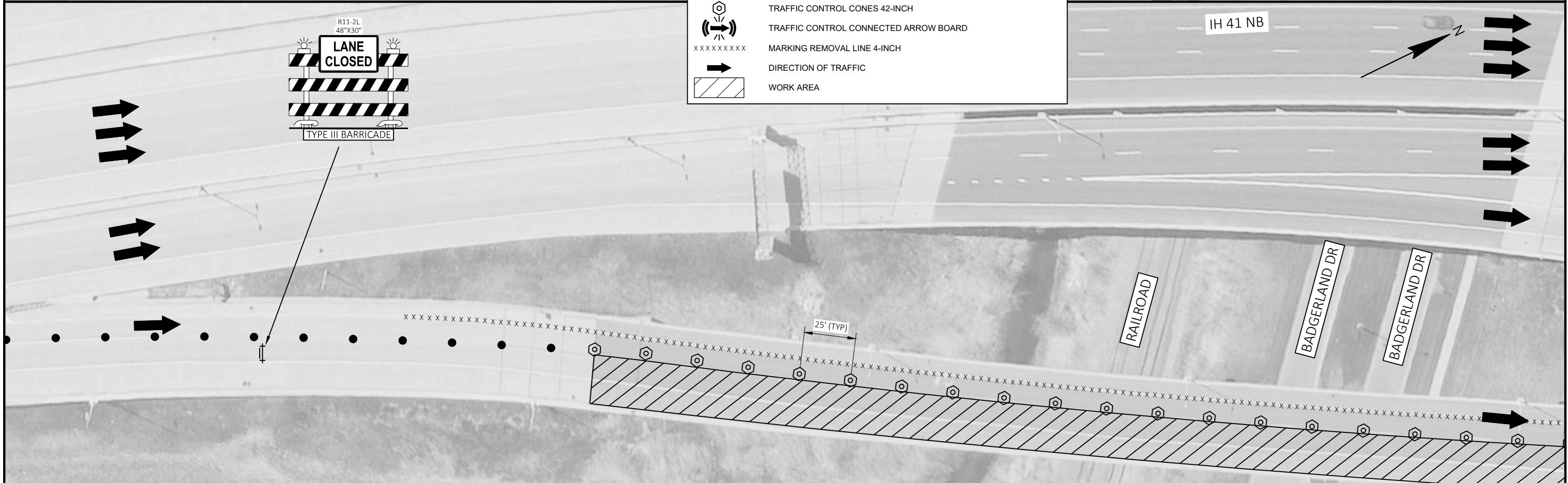
LEGEND

	TRAFFIC CONTROL TYPE III BARRICADE WITH TYPE A (FLASHING)
	TRAFFIC CONTROL TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C (STEADY BURN)
	TRAFFIC CONTROL CONES 42-INCH
	TRAFFIC CONTROL CONNECTED ARROW BOARD
	MARKING REMOVAL LINE 4-INCH
	DIRECTION OF TRAFFIC
	WORK AREA





LEGEND	
	TRAFFIC CONTROL TYPE III BARRICADE WITH TYPE A (FLASHING)
	TRAFFIC CONTROL TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C (STEADY BURN)
	TRAFFIC CONTROL CONES 42-INCH
	TRAFFIC CONTROL CONNECTED ARROW BOARD
	MARKING REMOVAL LINE 4-INCH
	DIRECTION OF TRAFFIC
	WORK AREA



PROJECT NO: 1130-73-71 HWY: IH 41 COUNTY: BROWN & WINNEBAGO TRAFFIC CONTROL - IH 41 NB TO USH 29 WB - STAGE 1 SHEET E



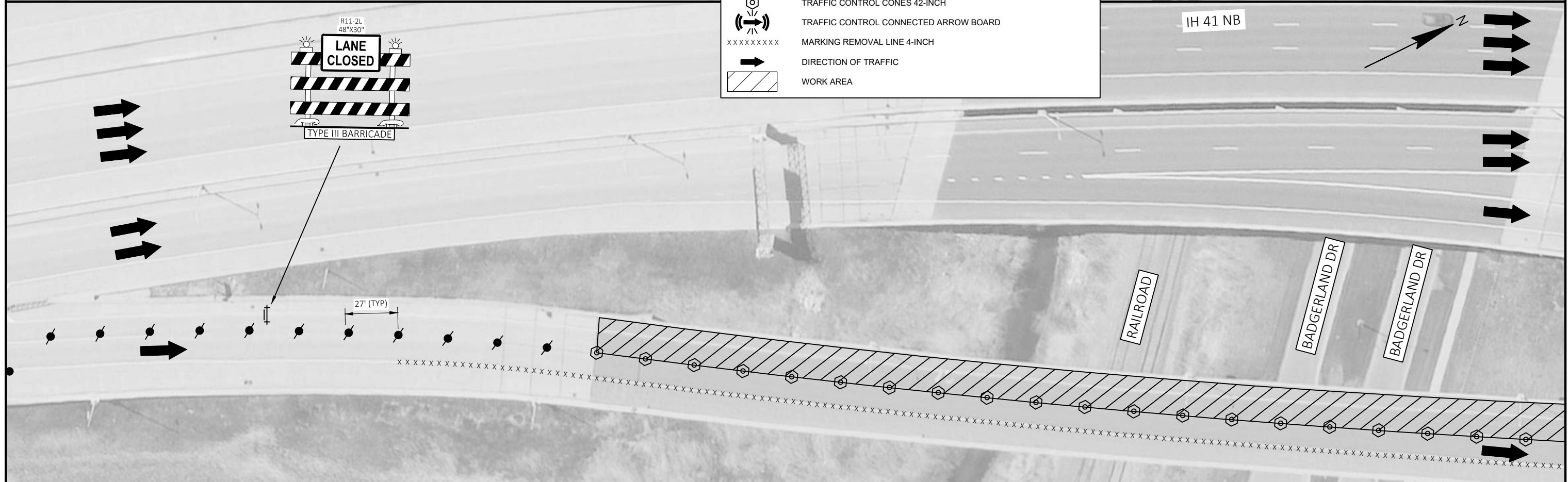
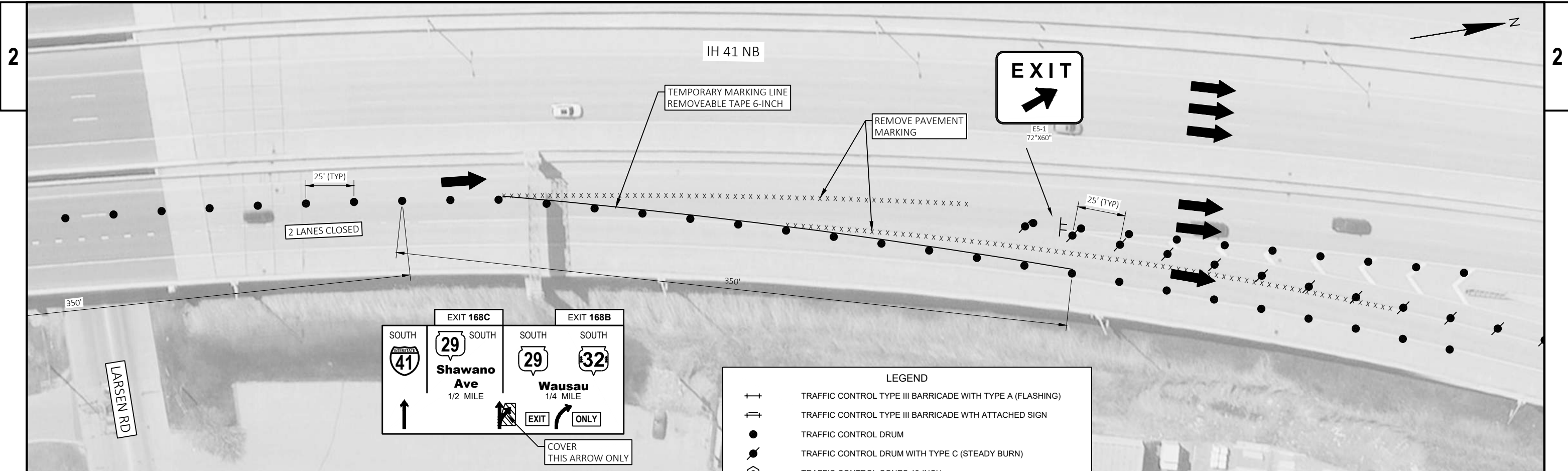


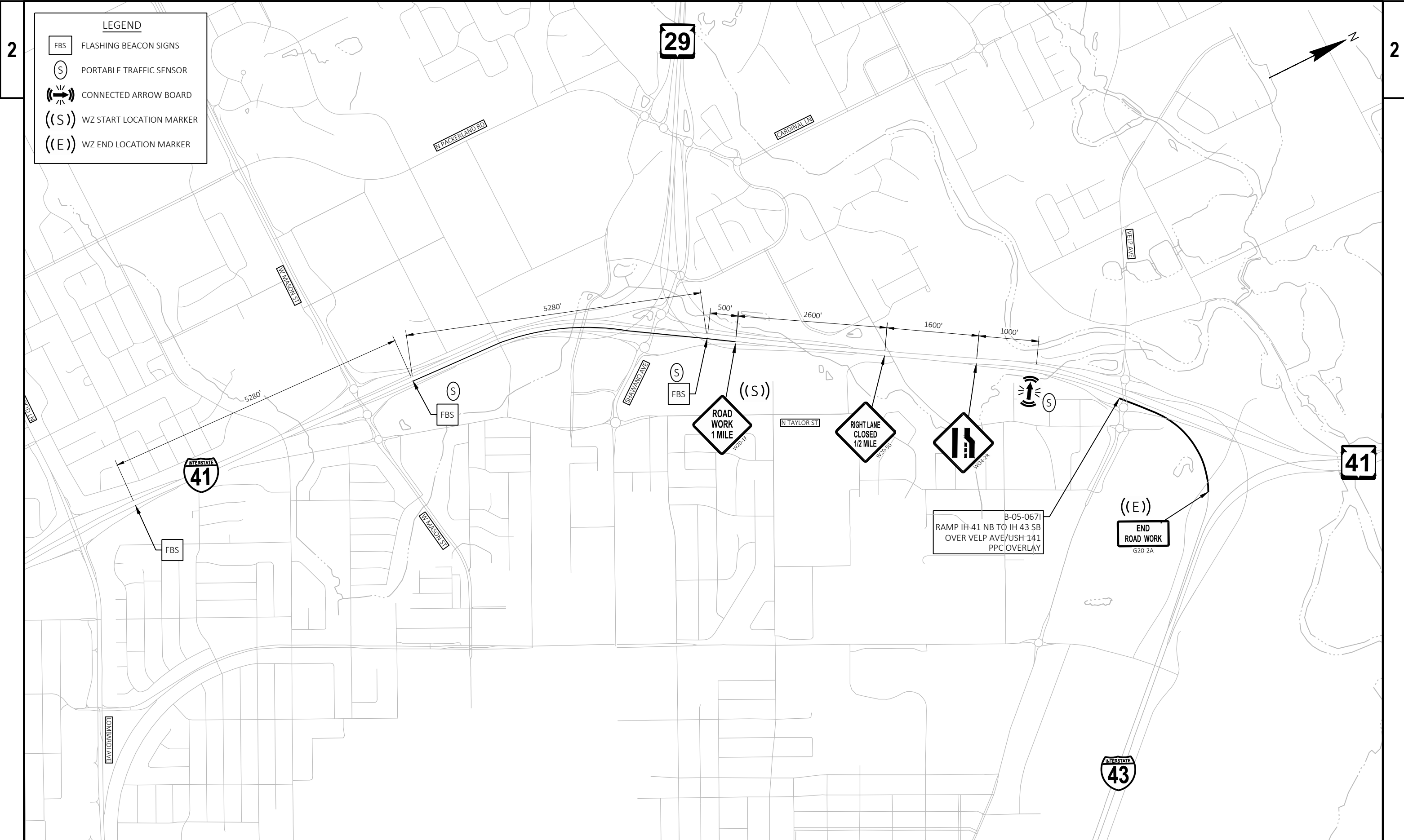
LEGEND

- TRAFFIC CONTROL TYPE III BARRICADE WITH TYPE A (FLASHING)
- TRAFFIC CONTROL TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C (STEADY BURN)
- TRAFFIC CONTROL CONES 42-INCH
- TRAFFIC CONTROL CONNECTED ARROW BOARD
- MARKING REMOVAL LINE 4-INCH
- DIRECTION OF TRAFFIC
- WORK AREA

EXIT 168C		EXIT 168B	
SOUTH 41	SOUTH 29 Shawano Ave 1/2 MILE	SOUTH 29	SOUTH 32 Wausau 1/4 MILE
↑	↑	↑	↑
	EXIT ONLY		

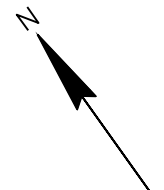
COVER THIS ARROW ONLY










LEGEND

- FBS FLASHING BEACON SIGNS
- S PORTABLE TRAFFIC SENSOR
- ↔ CONNECTED ARROW BOARD
- ((S)) WZ START LOCATION MARKER
- ((E)) WZ END LOCATION MARKER



LEGEND

-  FLASHING BEACON SIGNS
-  PORTABLE TRAFFIC SENSOR
-  CONNECTED ARROW BOARD
-  WZ START LOCATION MARKER
-  WZ END LOCATION MARKER

GREEN BAY
(LAKE MICHIGAN)

41

B-05-0678
RAMP IH 43 NB TO IH 41 SB
OVER IH 41
PPC OVERLAY

((E))
END
ROAD WORK



INTERSTATE
43

INTERSTATE
41

MEMORIAL DR

VELP AVE

BRANSON

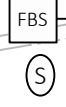
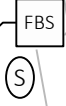
BAY BEACH RD

N WEBSTER AVE

N TAYLOR ST



((S))



1000'

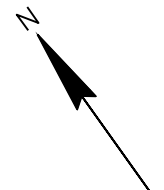
1600'

2600'

500'

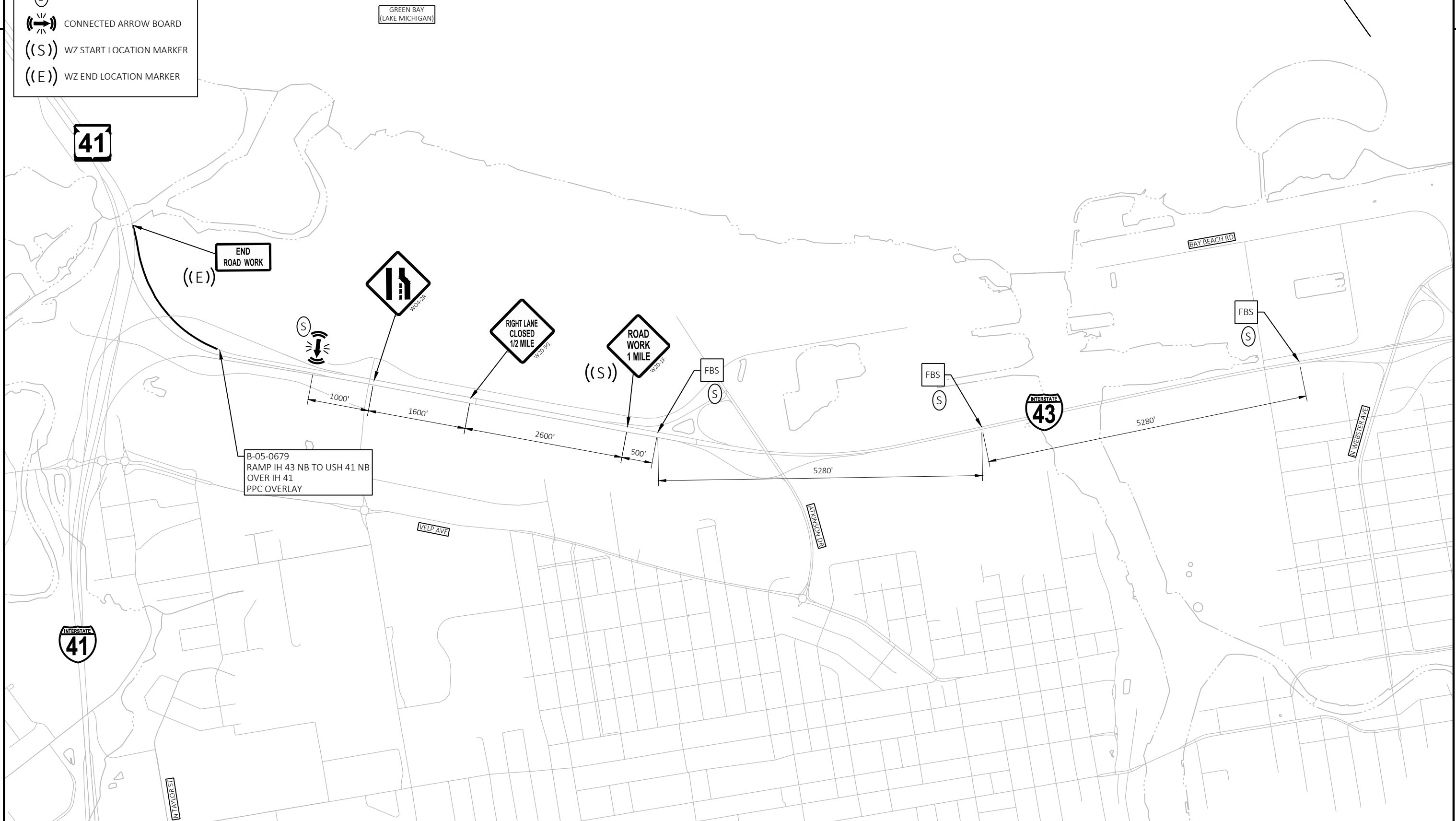
5280'

5280'

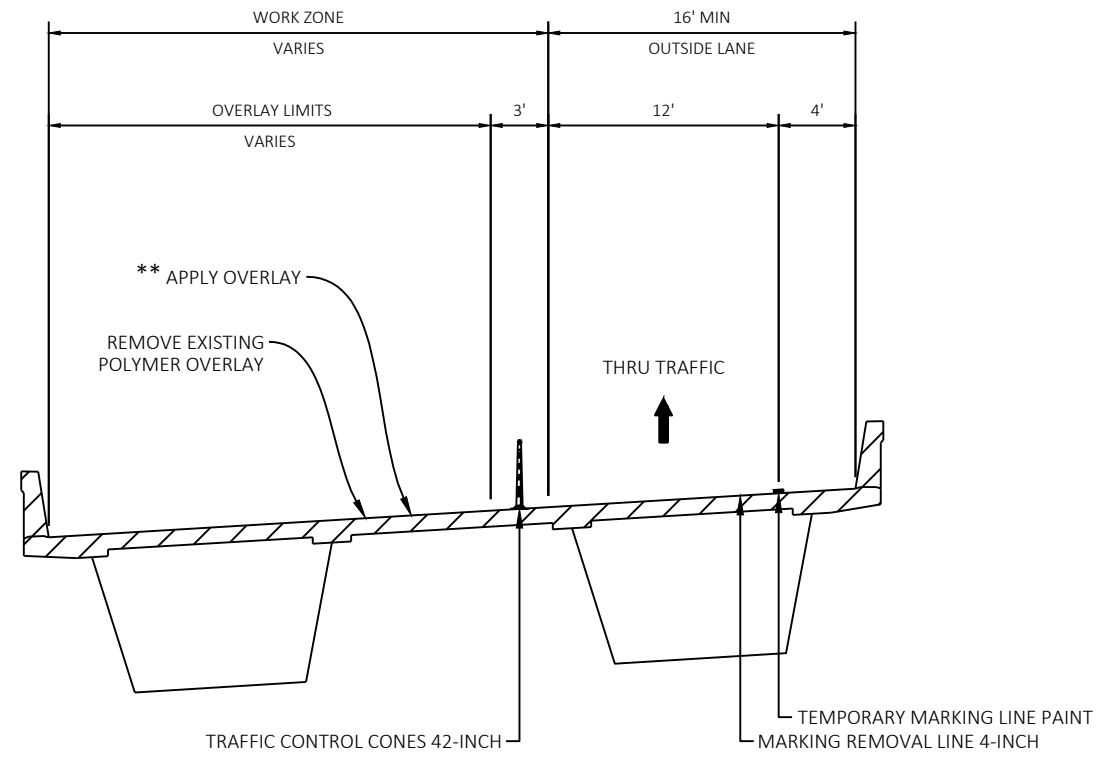


LEGEND

- FBS FLASHING BEACON SIGNS
- S PORTABLE TRAFFIC SENSOR
- CONNECTED ARROW BOARD
- ((S)) WZ START LOCATION MARKER
- ((E)) WZ END LOCATION MARKER



PROJECT NO: 1130-73-71	HWY: IH 41	COUNTY: BROWN & WINNEBAGO	TRAFFIC CONTROL - IH 43 NB TO USH 41 NB
			SHEET E

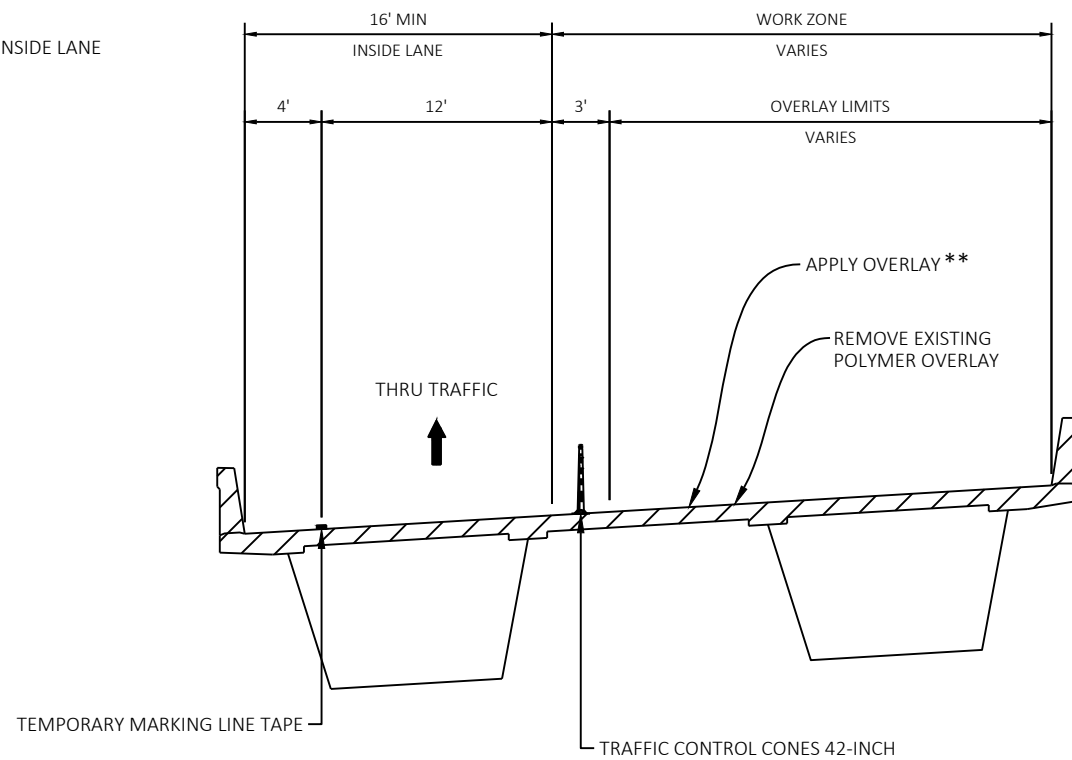


STAGE 1

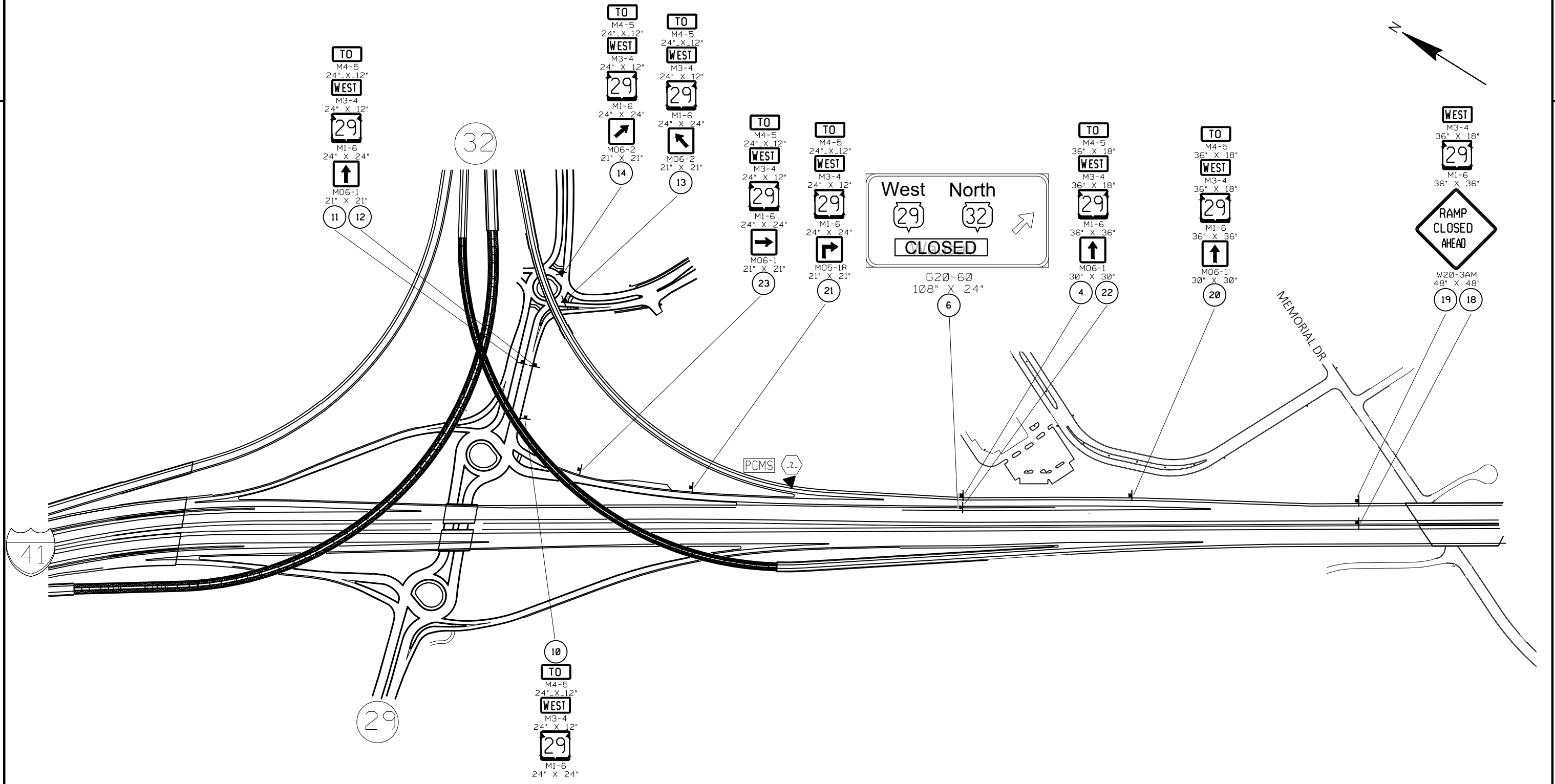
** POLYESTER POLYMER CONCRETE OVERLAY ON B-05-0660, B-05-0671, B-05-0678 & B-05-0679.

MAINTAIN A MINIMUM 16' OBSTRUCTIVE WIDTH.

STAGE 1 AND STAGE 2 IS THE CONTRACTORS CHOICE TO DO OUTSIDE LANE OR INSIDE LANE FIRST AT EACH STRUCTURE.



STAGE 2



LEGEND

- (x) SIGN NUMBER, REFER TO MISCELLANEOUS QUANTITY SHEET
- ▲ PCMS (x) PORTABLE CHANGEABLE MESSAGE SIGN
- ⇄ SIGN MOUNTED ON TYPE III BARRICADE
- ┆ POST MOUNTED SIGN

B-05-0661

SHEET 1 OF 2
PLAN SHEET PRODUCED
BY WisDOT-NE REGION

PROJECT NO: 1130-73-71

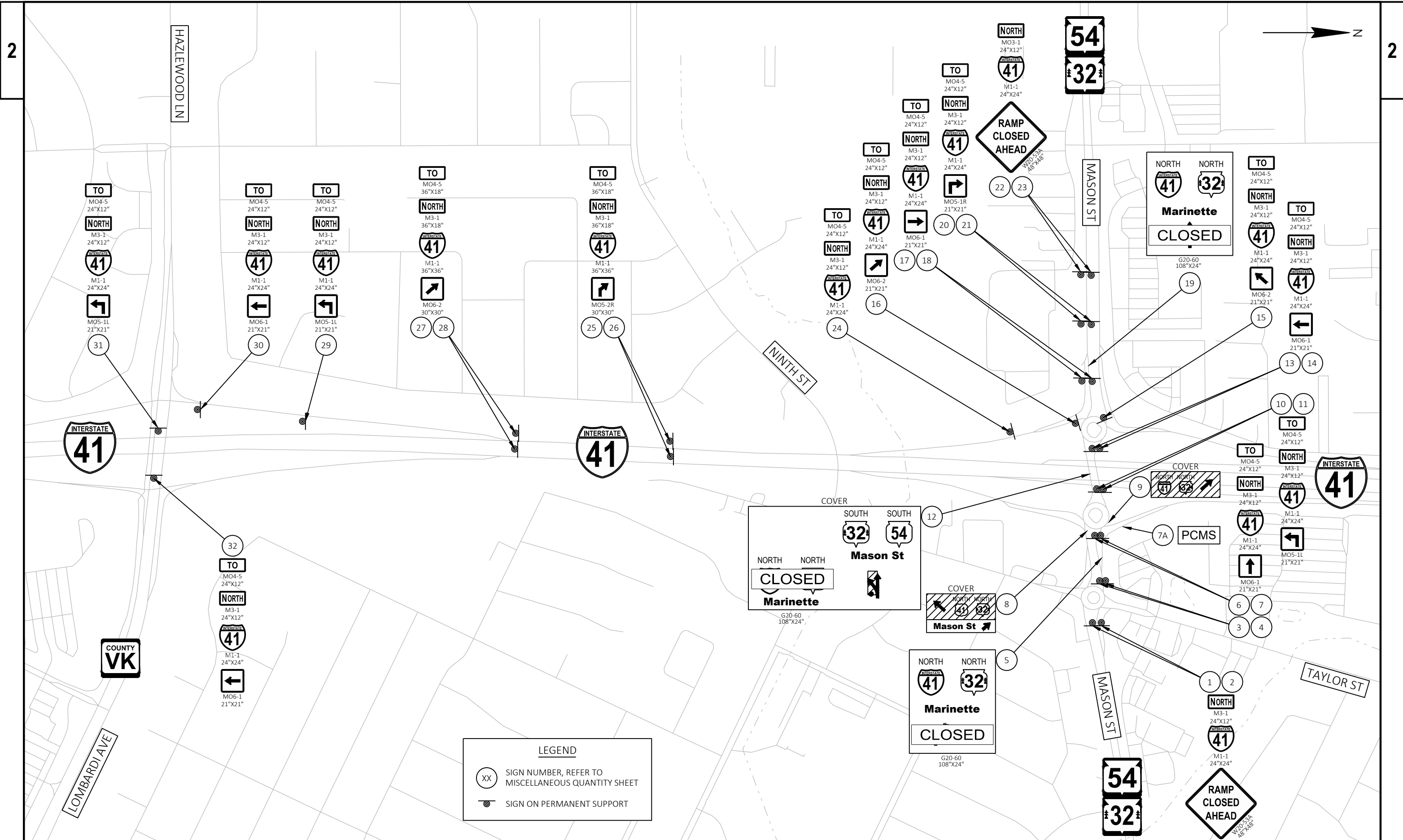
HWY: IH 41

COUNTY: BROWN & WINNEBAGO

DETOUR SIGNING DETAIL - 41 SB TO 29 WB RAMP CLOSURE

SHEET

E



PROJECT NO: 1130-73-71

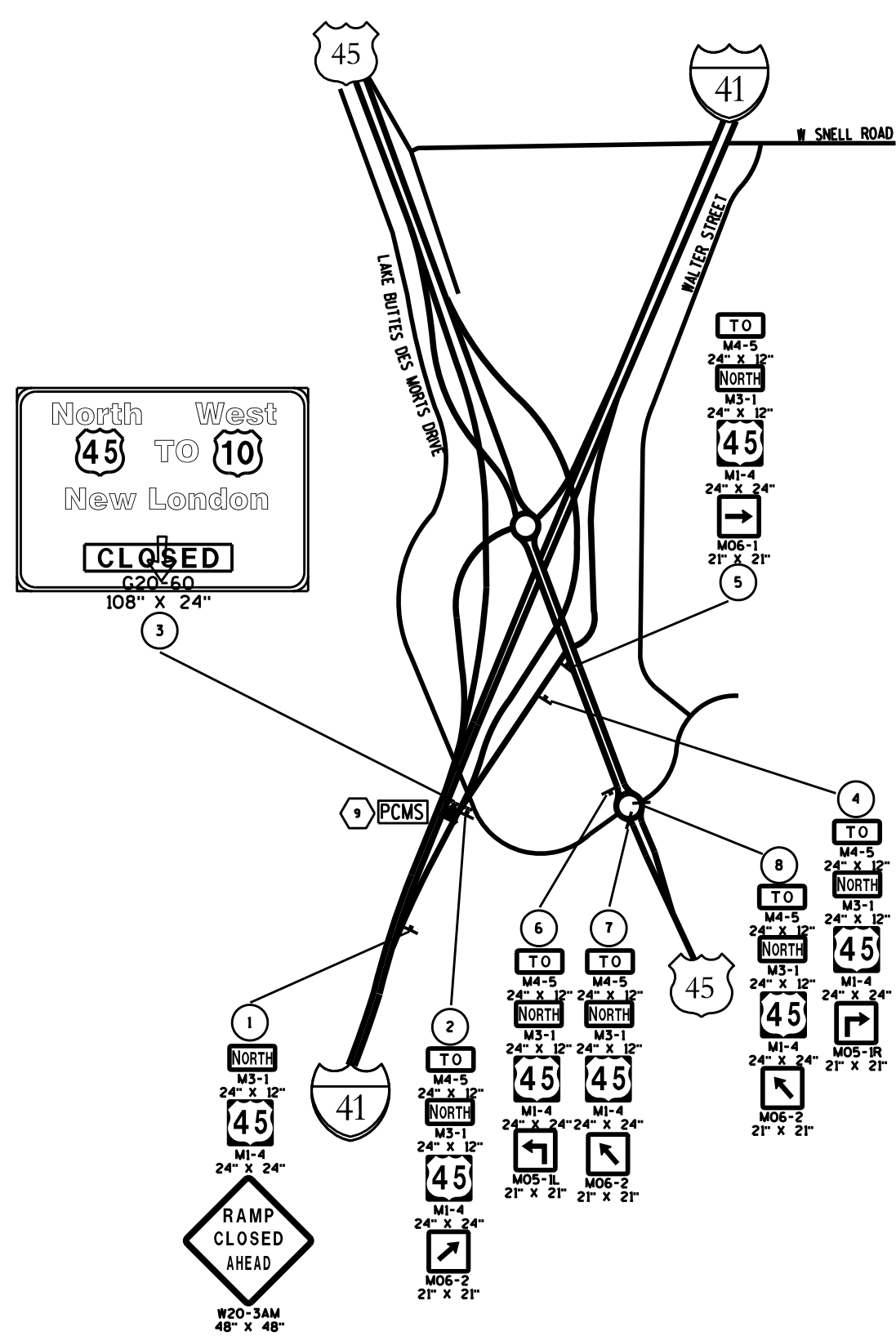
HWY: IH 41

COUNTY: BROWN & WINNEBAGO

DETOUR ROUTE

SHEET

E

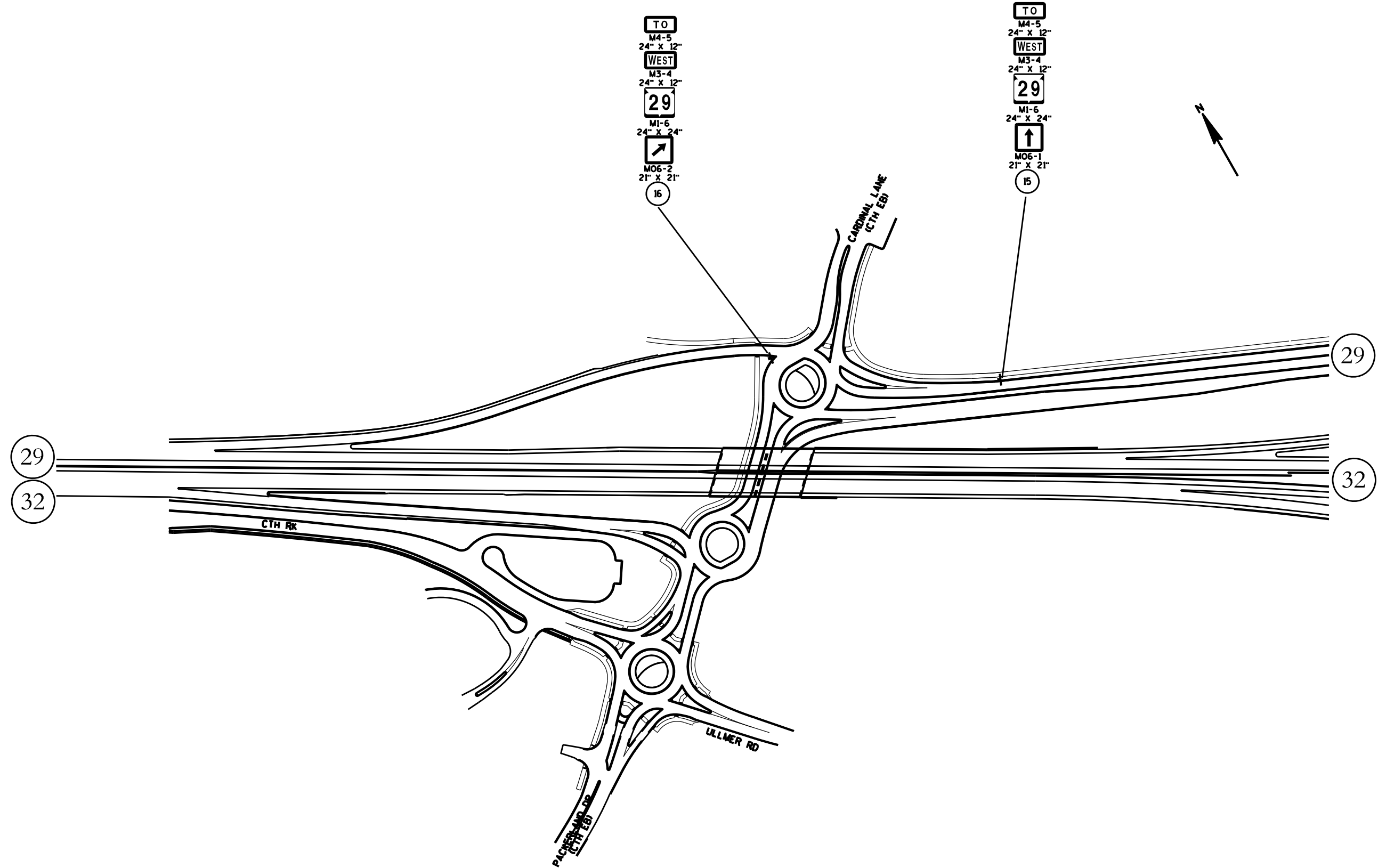


LEGEND

- x SIGN NUMBER, REFER TO MISCELLANEOUS QUANTITY SHEET
- PCMS x PORTABLE CHANGEABLE MESSAGE SIGN
- SIGN MOUNTED ON TYPE III BARRICADE
- POST MOUNTED SIGN

B-70-0269

PLAN SHEET PRODUCED BY WisDOT-NE REGION

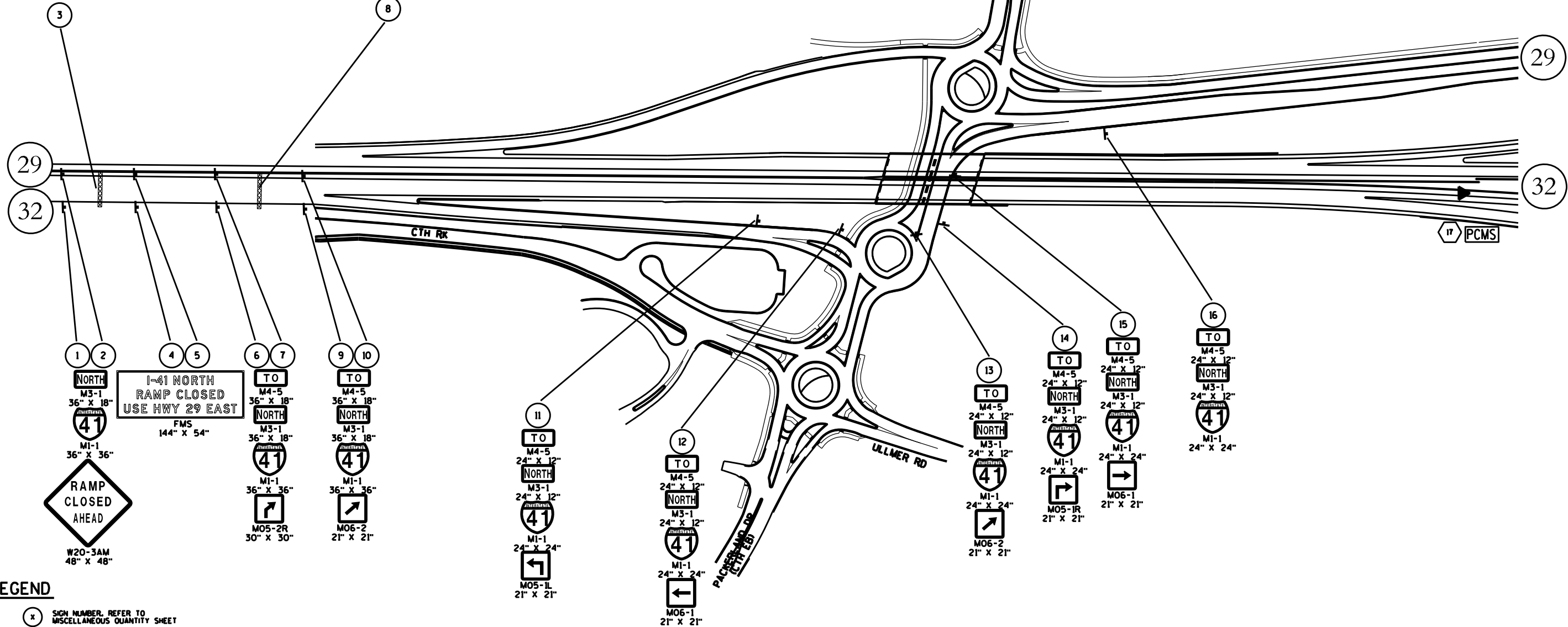
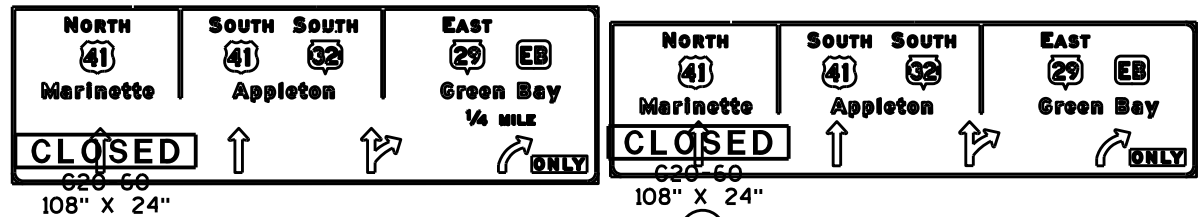


B-05-0661

LEGEND

- x SIGN NUMBER, REFER TO MISCELLANEOUS QUANTITY SHEET
- x PORTABLE CHANGEABLE MESSAGE SIGN
- PCMS PORTABLE CHANGEABLE MESSAGE SIGN
- ⇄ SIGN MOUNTED ON TYPE III BARRICADE
- POST MOUNTED SIGN

SHEET 2 OF 2
PLAN SHEET PRODUCED
BY WisDOT-NE REGION

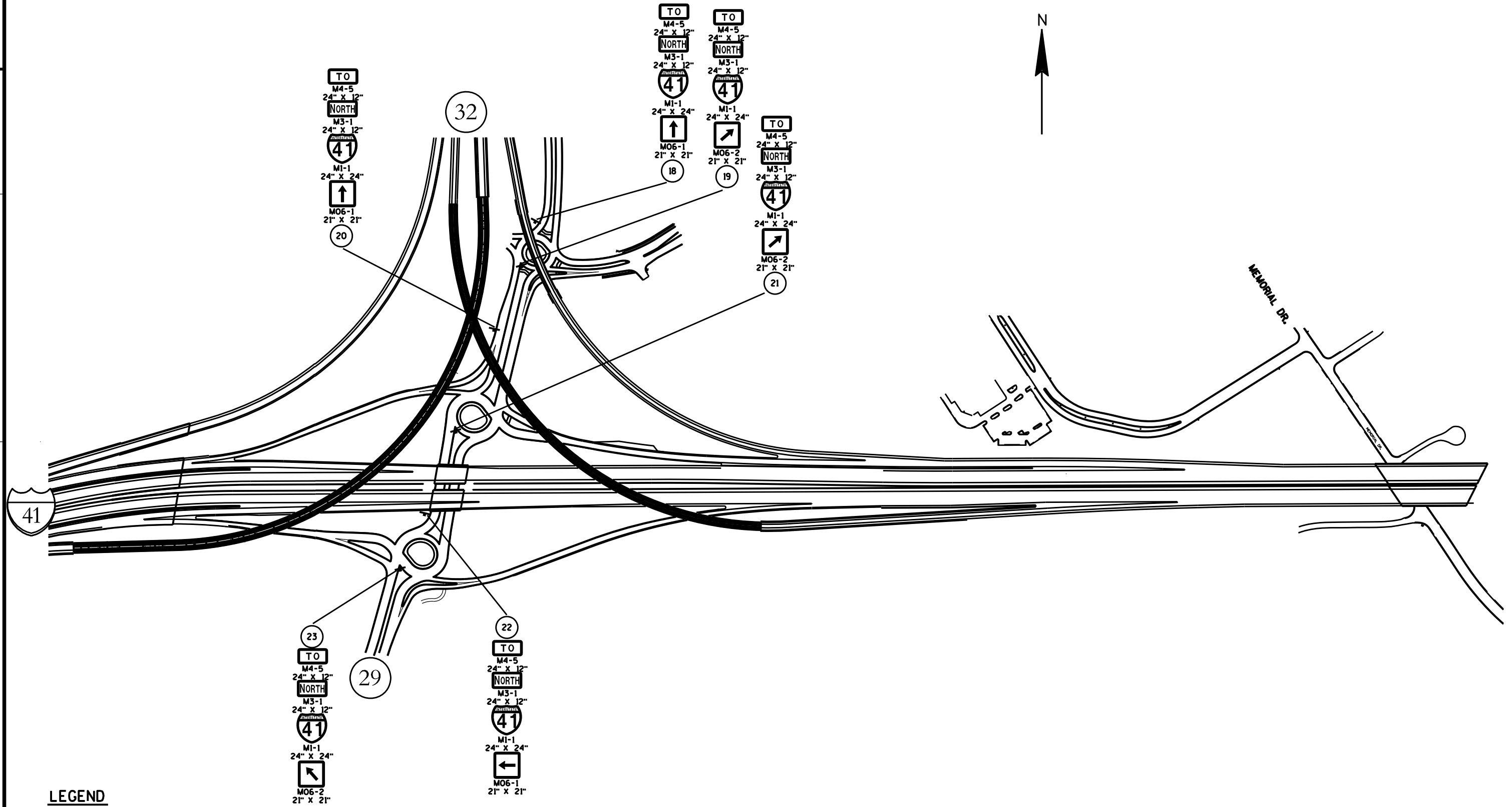


LEGEND

- ⊗ SIGN NUMBER, REFER TO MISCELLANEOUS QUANTITY SHEET
- ▲ PCMS ⊗ PORTABLE CHANGEABLE MESSAGE SIGN
- ↔ SIGN MOUNTED ON TYPE III BARRICADE
- POST MOUNTED SIGN

B-05-0658

SHEET 1 OF 2
PLAN SHEET PRODUCED
BY WisDOT-NE REGION



LEGEND

- x SIGN NUMBER, REFER TO MISCELLANEOUS QUANTITY SHEET
- x PORTABLE CHANGEABLE MESSAGE SIGN
- x SIGN MOUNTED ON TYPE III BARRICADE
- x POST MOUNTED SIGN

B-05-0658

SHEET 2 OF 2
PLAN SHEET PRODUCED
BY WisDOT-NE REGION

Estimate Of Quantities

1130-73-71

Line	Item	Item Description	Unit	Total	Qty
0002	509.0301	Preparation Decks Type 1	SY	7.000	7.000
0004	509.0302	Preparation Decks Type 2	SY	7.000	7.000
0006	509.0310.S	Sawing Pavement Deck Preparation Areas	LF	70.000	70.000
0008	509.0500	Cleaning Decks	SY	61,336.000	61,336.000
0010	509.1500	Concrete Surface Repair	SF	1.000	1.000
0012	509.5100.S	Polymer Overlay	SY	3,685.000	3,685.000
0014	509.9015.S	Removing Polymer Overlay (structure) 01. B-05-658	SY	7,811.000	7,811.000
0016	509.9015.S	Removing Polymer Overlay (structure) 02. B-05-660	SY	12,500.000	12,500.000
0018	509.9015.S	Removing Polymer Overlay (structure) 03. B-05-661	SY	1,386.000	1,386.000
0020	509.9015.S	Removing Polymer Overlay (structure) 04. B-05-671	SY	10,859.000	10,859.000
0022	509.9015.S	Removing Polymer Overlay (structure) 05. B-05-678	SY	18,580.000	18,580.000
0024	509.9015.S	Removing Polymer Overlay (structure) 06. B-05-679	SY	10,200.000	10,200.000
0026	509.9015.S	Removing Polymer Overlay (structure) 07. B-70-269	SY	3,685.000	3,685.000
0028	618.0100	Maintenance and Repair of Haul Roads (project) 01. 1130-73-71	EACH	1.000	1.000
0030	619.1000	Mobilization	EACH	1.000	1.000
0032	628.1905	Mobilizations Erosion Control	EACH	7.000	7.000
0034	628.1910	Mobilizations Emergency Erosion Control	EACH	7.000	7.000
0036	628.7015	Inlet Protection Type C	EACH	75.000	75.000
0038	642.5001	Field Office Type B	EACH	1.000	1.000
0040	643.0300	Traffic Control Drums	DAY	10,739.000	10,739.000
0042	643.0420	Traffic Control Barricades Type III	DAY	540.000	540.000
0044	643.0705	Traffic Control Warning Lights Type A	DAY	982.000	982.000
0046	643.0715	Traffic Control Warning Lights Type C	DAY	3,004.000	3,004.000
0048	643.0810	Traffic Control Connected Arrow Boards	DAY	188.000	188.000
0050	643.0900	Traffic Control Signs	DAY	4,282.000	4,282.000
0052	643.0910	Traffic Control Covering Signs Type I	EACH	2.000	2.000
0054	643.0920	Traffic Control Covering Signs Type II	EACH	6.000	6.000
0056	643.1000	Traffic Control Signs Fixed Message	SF	162.000	162.000
0058	643.1050	Traffic Control Signs PCMS	DAY	63.000	63.000
0060	643.1070	Traffic Control Cones 42-Inch	DAY	7,266.000	7,266.000
0062	643.1205.S	Basic Traffic Queue Warning System	DAY	85.000	85.000
0064	643.1220	Traffic Control Connected Work Zone Start and End Location Markers	DAY	188.000	188.000
0066	643.3165	Temporary Marking Line Paint 6-Inch	LF	9,525.000	9,525.000
0068	643.3180	Temporary Marking Line Removable Tape 6-Inch	LF	19,811.000	19,811.000
0070	643.5000	Traffic Control	EACH	1.000	1.000
0072	646.2020	Marking Line Epoxy 6-Inch	LF	32,619.000	32,619.000
0074	646.9000	Marking Removal Line 4-Inch	LF	12,161.000	12,161.000
0076	SPV.0035	Special 01. Rapid Set Deck Repair	CY	7.000	7.000
0078	SPV.0180	Special 01. Polyester Polymer Concrete Overlay	SY	61,336.000	61,336.000

TRAFFIC CONTROL DETOUR SIGN SUMMARY (29 EB TO 41 NB RAMP CLOSURE)

SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 10 DAYS	643.0900 SIGNS DAYS	643.1000 SIGNS FIXED MESSAGE SF	643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	REMARKS
1	STH 29/32 EB, PLACE 2000' W. OF CTH EB OFF-RAMP ON RIGHT SHOULDER	M 3-1	36"X18"	1	10	10			
	"	M 1-1	36"X36"	1	10	10			41
	"	W 20-3AM	48"X48"	1	10	10			
2	STH 29/32 EB, PLACE 2000' W. OF CTH EB OFF-RAMP IN MEDIAN	M 3-1	36"X18"	1	10	10			
	"	M 1-1	36"X36"	1	10	10			41
	"	W 20-3AM	48"X48"	1	10	10			
3	STH 29/32 EB, PLACE ON EXISTING OH TYPE I SIGN ON S-05-203 AS SHOWN	G 20-60	108"X24"	1	10	10			
4	STH 29/32 EB, PLACE 1500' W. OF CTH EB OFF-RAMP ON RIGHT SHOULDER	FMS	144"X54"	1			54		SEE SIGN DETAIL SHEET
5	STH 29/32 EB, PLACE 1500' W. OF CTH EB OFF-RAMP IN MEDIAN	FMS	144"X54"	1			54		SEE SIGN DETAIL SHEET
6	STH 29/32 EB, PLACE 1000' W. OF CTH EB OFF-RAMP ON RIGHT SHOULDER	M 4-5	36"X18"	1	10	10			
	"	M 3-1	36"X18"	1	10	10			
	"	M 1-1	36"X36"	1	10	10			41
	"	MO 5-2R	30"X30"	1	10	10			
7	STH 29/32 EB, PLACE 1000' W. OF CTH EB OFF-RAMP IN MEDIAN	M 4-5	36"X18"	1	10	10			
	"	M 3-1	36"X18"	1	10	10			
	"	M 1-1	36"X36"	1	10	10			41
	"	MO 5-2R	30"X30"	1	10	10			
8	STH 29/32 EB, PLACE ON EXISTING OH TYPE I SIGN ON S-05-204 AS SHOWN	G 20-60	108"X24"	1	10	10			
9	STH 29/32 EB, PLACE AT EXIT POINT TO CTH EB OFF-RAMP ON RIGHT SHOULDER	M 4-5	36"X18"	1	10	10			
	"	M 3-1	36"X18"	1	10	10			
	"	M 1-1	36"X36"	1	10	10			41
	"	MO 6-2	30"X30"	1	10	10			TILT RIGHT
10	STH 29/32 EB, PLACE AT EXIT POINT TO CTH EB OFF-RAMP IN MEDIAN	M 4-5	36"X18"	1	10	10			
	"	M 3-1	36"X18"	1	10	10			
	"	M 1-1	36"X36"	1	10	10			41
	"	MO 6-2	30"X30"	1	10	10			TILT RIGHT
11	STH 29/32 OFF-RAMP TO CTH EB, PLACE 750' PRIOR TO RAMP RAB INTERSECTION	M 4-5	24"X12"	1	10	10			
	"	M 3-1	24"X12"	1	10	10			
	"	M 1-1	24"X24"	1	10	10			41
	"	MO 5-1L	21"X21"	1	10	10			
12	STH 29/32 OFF-RAMP TO CTH EB, PLACE 100' PRIOR TO RAMP RAB INTERSECTION	M 4-5	24"X12"	1	10	10			
	"	M 3-1	24"X12"	1	10	10			
	"	M 1-1	24"X24"	1	10	10			41
	"	MO 6-1	21"X21"	1	10	10			LEFT
13	CTH EB, PLACE ABOVE EXISTING SPLITTER ISLAND SIGN AT NORTH LEG EXIT AT RAMP RAB INTERSECTION	M 4-5	24"X12"	1	10	10			
	"	M 3-1	24"X12"	1	10	10			
	"	M 1-1	24"X24"	1	10	10			41
	"	MO 6-2	21"X21"	1	10	10			TILT RIGHT
PAGE SUBTOTALS				38		360	108	0	

PLAN SHEET PRODUCED
BY WisDOT - NE REGION

PROJECT NUMBER: 1130-73-71

HWY: IH 41

COUNTY: BROWN & WINNEBAGO

MISCELLANEOUS QUANTITIES

SHEET

E

TRAFFIC CONTROL DETOUR SIGN SUMMARY (29 EB TO 41 NB RAMP CLOSURE)

SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 10 DAYS	643.0900 SIGNS DAYS	643.1000 SIGNS FIXED MESSAGE SF	643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	REMARKS
14	CTH EB, PLACE RIGHT OF SIGN STRUCTURE S-05-273 RIGHT COLUMN	M 4-5	24"X12"	1	10	10			
	"	M 3-1	24"X12"	1	10	10			
	"	M 1-1	24"X24"	1	10	10			41
	"	MO 5-1R	21"X21"	1	10	10			
15	CTH EB, UNDER STH 32, PLACE 150' N. OF SIGN STRUCTURE S-05-273	M 4-5	24"X12"	1	10	10			
	"	M 3-1	24"X12"	1	10	10			
	"	M 1-1	24"X24"	1	10	10			41
	"	MO 6-1	21"X21"	1	10	10			RIGHT
16	STH 29 EB, E. OF CTH EB RAB, PLACE RIGHT OF EXISTING J4-1 SIGN	M 4-5	24"X12"	1	10	10			
	"	M 3-1	24"X12"	1	10	10			
	"	M 1-1	24"X24"	1	10	10			41
17	STH 32, AT I-41 NB OFF-RAMP, PLACE ON SHOULDER, FIELD DETERMINE LOCATION	PCMS		1				7	PLACE IN ADVANCE OF CLOSURE
18	STH 29, W. OF DUCK CREEK PARKWAY, PLACE 150' PRIOR TO DUCK CREEK PARKWAY RAB INTERSECTION	M 4-5	24"X12"	1	10	10			
	"	M 3-1	24"X12"	1	10	10			
	"	M 1-1	24"X24"	1	10	10			41
	"	MO 6-1	21"X21"	1	10	10			AHEAD
19	STH 29, AT DUCK CREEK PARKWAY, PLACE ABOVE EXISTING SPLITTER ISLAND SIGN AT EAST LEG EXIT AT DUCK CREEK PARKWAY RAB INTERSECTION	M 4-5	24"X12"	1	10	10			
	"	M 3-1	24"X12"	1	10	10			
	"	M 1-1	24"X24"	1	10	10			41
	"	MO 6-2	21"X21"	1	10	10			TILT RIGHT
20	STH 29, W. OF I-41 SB RAMP, PLACE 150' W. OF I-41 SB RAMP INTERSECTION	M 4-5	24"X12"	1	10	10			
	"	M 3-1	24"X12"	1	10	10			
	"	M 1-1	24"X24"	1	10	10			41
	"	MO 6-1	21"X21"	1	10	10			AHEAD
21	STH 29, AT I-41 SB RAMP, PLACE ABOVE EXISTING SPLITTER ISLAND SIGN AT EAST LEG EXIT AT I-41 SB RAMP INTERSECTION	M 4-5	24"X12"	1	10	10			
	"	M 3-1	24"X12"	1	10	10			
	"	M 1-1	24"X24"	1	10	10			41
	"	MO 6-2	21"X21"	1	10	10			TILT RIGHT
22	STH 29, W. OF I-41 NB RAMP, PLACE 150' W. OF I-41 NB RAMP INTERSECTION	M 4-5	24"X12"	1	10	10			
	"	M 3-1	24"X12"	1	10	10			
	"	M 1-1	24"X24"	1	10	10			41
	"	MO 6-1	21"X21"	1	10	10			LEFT
23	STH 29, AT I-41 NB RAMP, PLACE ABOVE EXISTING SPLITTER ISLAND SIGN AT EAST LEG EXIT AT I-41 NB RAMP INTERSECTION	M 4-5	24"X12"	1	10	10			
	"	M 3-1	24"X12"	1	10	10			
	"	M 1-1	24"X24"	1	10	10			41
	"	MO 6-2	21"X21"	1	10	10			TILT LEFT

PAGE SUBTOTALS

36

350

0

7

29 EB TO 41 NB CLOSURE TOTALS

74

710

108

7

PLAN SHEET PRODUCED
BY WisDOT - NE REGION

PROJECT NUMBER: 1130-73-71

HWY: IH 41

COUNTY: BROWN & WINNEBAGO

MISCELLANEOUS QUANTITIES

SHEET

E

TRAFFIC CONTROL DETOUR SIGN SUMMARY (41 SB TO 29 WB RAMP CLOSURE)

SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 10 DAYS	643.0900 SIGNS DAYS	643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	REMARKS
1	I-41, N. OF STH 29/32 RAMP, PLACE 250' N. OF STH 29/32 RAMP ON RIGHT SHOULDER	M 3-4	36"x18"	1	10	10		
	"	M 1-6	36"x18"	1	10	10		29
	"	W 20-3AM	48"x48"	1	10	10		
2	I-41, N. OF STH 29/32 RAMP, PLACE 250' N. OF STH 29/32 RAMP IN MEDIAN	M 3-4	36"x18"	1	10	10		
	"	M 1-6	36"x18"	1	10	10		29
	"	W 20-3AM	48"x48"	1	10	10		
3	I-41 OFF-RAMP TO STH 29/32, PLACE 750' N. OF SIGN STRUCTURE S-05-186	M 4-5	36"x18"	1	10	10		
	"	M 3-4	36"x18"	1	10	10		
	"	M 1-6	36"x36"	1	10	10		29
	"	MO 6-1	30"x30"	1	10	10		AHEAD
4	I-41 OFF-RAMP TO STH 29/32, PLACE IN FRONT OF RIGHT COLUMN OF S-05-186	M 4-5	36"x18"	1	10	10		
	"	M 3-4	36"x18"	1	10	10		
	"	M 1-6	36"x36"	1	10	10		29
	"	MO 6-1	30"x30"	1	10	10		AHEAD
5	I-41 OFF-RAMP TO STH 29/32, PLACE IN MEDIAN BY LEFT COLUMN OF S-05-186	M 4-5	36"x18"	1	10	10		
	"	M 3-4	36"x18"	1	10	10		
	"	M 1-6	36"x36"	1	10	10		29
	"	MO 6-1	30"x30"	1	10	10		AHEAD
6	I-41 OFF-RAMP TO STH 29/32, PLACE ON OH TYPE I SIGN AS SHOWN	G 20-60	108"x24"	1	10	10		
7	I-41 OFF-RAMP TO STH 29/32 WB, PLACE ON RIGHT SHOULDER, FIELD DETERMINE LOCATION	PCMS		1			7	PLACE IN ADVANCE OF CLOSURE
8	I-41 OFF-RAMP TO STH 29 EB, PLACE 750' PRIOR TO RAB RAMP INTERSECTION	M 4-5	24"x12"	1	10	10		
	"	M 3-4	24"x12"	1	10	10		
	"	M 1-6	24"x24"	1	10	10		29
	"	MO 5-1R	21"x21"	1	10	10		
9	I-41 OFF-RAMP TO STH 29 EB, PLACE 250' PRIOR TO RAB RAMP INTERSECTION	M 4-5	24"x12"	1	10	10		
	"	M 3-4	24"x12"	1	10	10		
	"	M 1-6	24"x24"	1	10	10		29
	"	MO 6-1	21"x21"	1	10	10		RIGHT
10	STH 29 WB, PLACE 250' W. OF I-41 SB RAMP INTERSECTION	M 4-5	24"x12"	1	10	10		
	"	M 3-4	24"x12"	1	10	10		
	"	M 1-6	24"x24"	1	10	10		29
11	STH 29 WB, PLACE 250' E. OF DUCK CREEK PARKWAY RAB INTERSECTION IN MEDIAN	M 4-5	24"x12"	1	10	10		
	"	M 3-4	24"x12"	1	10	10		
	"	M 1-6	24"x24"	1	10	10		29
	"	MO 6-1	21"x21"	1	10	10		AHEAD
12	STH 29 WB, PLACE 250' E. OF DUCK CREEK PARKWAY RAB INTERSECTION ON RIGHT SHOULDER	M 4-5	24"x12"	1	10	10		
	"	M 3-4	24"x12"	1	10	10		
	"	M 1-6	24"x24"	1	10	10		29
	"	MO 6-1	21"x21"	1	10	10		AHEAD
13	STH 29 WB, PLACE ABOVE EXISTING SPLITTER ISLAND SIGN AT NORTH LEG EXIT IN DUCK CREEK PARKWAY RAB	M 4-5	24"x12"	1	10	10		
	"	M 3-4	24"x12"	1	10	10		
	"	M 1-6	24"x24"	1	10	10		29
	"	MO 6-2	21"x21"	1	10	10		TILT LEFT
14	STH 29 WB, PLACE ABOVE EXISTING SPLITTER ISLAND SIGN AT WEST LEG EXIT IN DUCK CREEK PARKWAY RAB	M 4-5	24"x12"	1	10	10		
	"	M 3-4	24"x12"	1	10	10		
	"	M 1-6	24"x24"	1	10	10		29
	"	MO 6-2	21"x21"	1	10	10		TILT RIGHT
15	STH 29 WB, PLACE 250' E. OF CTH EB RAB INTERSECTION ON RIGHT SHOULDER	M 4-5	24"x12"	1	10	10		
	"	M 3-4	24"x12"	1	10	10		
	"	M 1-6	24"x24"	1	10	10		29
	"	MO 6-1	21"x21"	1	10	10		AHEAD
16	STH 29 WB, PLACE ABOVE EXISTING SPLITTER ISLAND SIGN AT WEST LEG IN CTH EB RAB	M 4-5	24"x12"	1	10	10		
	"	M 3-4	24"x12"	1	10	10		
	"	M 1-6	24"x24"	1	10	10		29
	"	MO 6-2	21"x21"	1	10	10		TILT RIGHT

41 SB TO 29 WB CLOSURE TOTALS

55

540

7

PLAN SHEET PRODUCED BY WisDOT - NE REGION

PROJECT NUMBER: 1130-73-71

HWY: IH 41

COUNTY: BROWN & WINNEBAGO

MISCELLANEOUS QUANTITIES

SHEET

E

3

3

TRAFFIC CONTROL DETOUR SIGN SUMMARY (41 NB TO 45 NB RAMP CLOSURE)

SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 5 DAYS	643.0900 SIGNS DAYS	643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	REMARKS
1	I-41, AT EXIT RAMP TO US 45, PLACE AT THEORETICAL GORE PT ON RIGHT SHOULDER	M 3-1	24"x12"	1	5	5		
	"	M 1-4	24"x24"	1	5	5		45
	"	W 20-3AM	48"x48"	1	5	5		
2	I-41 OFF-RAMP TO US 45, PLACE IN FRONT OF RIGHT COLUMN FOR S-70-66	M 4-5	24"x12"	1	5	5		
	"	M 3-1	24"x12"	1	5	5		
	"	M 1-4	24"x24"	1	5	5		45
	"	MO 6-2	21"x21"	1	5	5		TILT RIGHT
3	I-41 OFF-RAMP TO US 45, PLACE ON OH TYPE I SIGN AS SHOWN ON S-70-66	G 20-60	108"x24"	1	5	5		
4	I-41 OFF-RAMP TO US 45 SB, PLACE 500' S. OF US 45 SB RAMP INTERSECTION	M 4-5	24"x12"	1	5	5		
	"	M 3-1	24"x12"	1	5	5		
	"	M 1-4	24"x24"	1	5	5		45
	"	MO 5-1R	21"x21"	1	5	5		
5	I-41 OFF-RAMP TO US 45 SB, PLACE RIGHT OF EXISTING R1-1 SIGN AT RAMP INTERSECTION	M 4-5	24"x12"	1	5	5		
	"	M 3-1	24"x12"	1	5	5		
	"	M 1-4	24"x24"	1	5	5		45
	"	MO 6-1	21"x21"	1	5	5		RIGHT
6	US 45, AT FERNAU RD RAB, PLACE RIGHT OF EXISTING R1-2 SIGN AT RAB	M 4-5	24"x12"	1	5	5		
	"	M 3-1	24"x12"	1	5	5		
	"	M 1-4	24"x24"	1	5	5		45
	"	MO 5-1L	21"x21"	1	5	5		
7	US 45, AT FERNAU RD RAB, PLACE ABOVE EXISTING SPLITTER ISLAND SIGN AT SOUTH LEG EXIT	M 4-5	24"x12"	1	5	5		
	"	M 3-1	24"x12"	1	5	5		
	"	M 1-4	24"x24"	1	5	5		45
	"	MO 6-2	21"x21"	1	5	5		TILT LEFT
8	US 45, AT FERNAU RD RAB, PLACE ABOVE EXISTING SPLITTER ISLAND SIGN AT EAST LEG EXIT	M 4-5	24"x12"	1	5	5		
	"	M 3-1	24"x12"	1	5	5		
	"	M 1-4	24"x24"	1	5	5		45
	"	MO 6-2	21"x21"	1	5	5		TILT LEFT
9	I-41 OFF-RAMP TO US 45, PLACE IN MEDIAN OPENING, FIELD DETERMINE EXACT LOCATION	PCMS		1			7	PLACE IN ADVANCE OF CLOSURE

41 NB TO 45 NB CLOSURE TOTALS

29

140

7

PLAN SHEET PRODUCED
BY WisDOT - NE REGION

PROJECT NUMBER: 1130-73-71

HWY: IH 41

COUNTY: BROWN & WINNEBAGO

MISCELLANEOUS QUANTITIES

SHEET

E

EROSION CONTROL SUMMARY

STATION TO STATION	LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH	628.7015 INLET PROTECTION TYPE C EACH	REMARKS
813+34.90 - 834+51.90	STH 29 EB - IH 41 NB	1	1	11	B-05-658
1074+01.15 - 1100+79.15	IH 41 NB - STH 29 WB	1	1	12	B-05-660
1096+24.41 - 1216+47.00	IH 41 SB - STH 29 WB	1	1	1	B-05-661
1173+05.83 - 1196+49.50	IH 41 NB - IH 43 SB	1	1	12	B-05-671
1179+87.75 - 1216+52.00	IH 43 NB - IH 41 SB	1	1	25	B-05-678
1187+31.83 - 1209+18.08	IH 43 NB - IH 41 NB	1	1	9	B-05-679
54'ALF+61.20 - 66ALF+8.95	USH 41 NB to USH 45 WB	1	1	5	B-70-269
TOTAL 0010		7	7	75	

TRAFFIC CONTROL DETOUR SUMMARY

LOCATION	* 643.0900 TRAFFIC CONTROL SIGNS DAY	** 643.0920 TRAFFIC CONTROL COVERING SIGNS TYPE II EACH	* 643.1000 TRAFFIC CONTROL SIGNS FIXED MESSAGE SF	* 643.1050 TRAFFIC CONTROL SIGNS PCMS DAY	REMARKS
STH 29 EB - IH 41 NB	710	-	108	7	B-05-658
IH 41 SB - STH 29 WB	540	-	0	7	B-05-661
H 41 NB - USH 45 WB	140	-	0	7	B-70-269
Mason Street	1,236	2	54	7	Closure
TOTAL 0010	2,626	2	162	28	

*QUANTITIES SHOWN ELSEWHERE
**ALL COVERING SIGN 1 CYCLE ONLY

PAVEMENT MARKING SUMMARY

STATION TO STATION	LOCATION	* 643.3165 TEMPORARY MARKING LINE PAINT 6- INCH LF	* 643.3180 TEMPORARY MARKING LINE REMOVABLE TAPE 6-INCH LF	** 646.2020 MARKING LINE EPOXY 6-INCH LF	** 646.9000 MARKING REMOVAL LINE 4- INCH LF	REMARKS
813+34.90 - 834+51.90	STH 29 EB - IH 41 NB	-	-	-	-	B-05-658 - STAGE 1
813+34.90 - 834+51.90	STH 29 EB - IH 41 NB	-	-	2,118	2118	B-05-658 - STRUCTURE
1074+01.15 - 1100+79.15	IH 41 NB - STH 29 WB	2678	880	-	-	B-05-660 - STAGE 1
1074+01.15 - 1100+79.15	IH 41 NB - STH 29 WB	-	4,078	3,348	2678	B-05-660 - STAGE2
1096+21.47 - 1216+47.00	IH 41 SB - STH 29 WB	-	-	-	-	B-05-661 - STAGE 1
1096+21.47 - 1216+47.00	IH 41 SB - STH 29 WB	-	-	374	374	B-05-661 - STRUCTURE
1173+05.83 - 1196+49.50	IH 41 NB - IH 43 SB	374	880	-	-	B-05-671 - STAGE 1
1173+05.83 - 1196+49.50	IH 41 NB - IH 43 SB	-	3,224	2,930	2344	B-05-671 - STAGE 2
1179+87.75 - 1216+52.00	IH 43 SB - IH 41 SB OVER IH 41	2344	880	-	-	B-05-678 - STAGE 1
1179+87.75 - 1216+52.00	IH 43 SB - IH 41 SB OVER IH 41	-	4,874	4,993	3994	B-05-678 - STAGE 2
1187+31.83 - 1209+25.18.08	IH 43 NB - IH 41 NB	3994	880	-	-	B-05-679 - STAGE 1
1187+31.83 - 1209+25.18.08	IH 43 NB - IH 41 NB	-	3,065	2,731	2185	B-05-679 - STAGE 2
54'ALF+64.95 - 66'ALF+81.95	USH 41 NB to USH 45 WB	-	780	-	-	B-70-269 - STAGE 1
54'ALF+64.95 - 66'ALF+81.95	USH 41 NB to USH 45 WB	-	-	1,216	1216	B-70-269 - STRUCTURE
	Mason Street	-	-	-	-	LEFT LANE CLOSURE
	Algoma Blvd	135	270	-	-	TRAFFIC CONTROL
TOTAL 0010		9,525	19,811	17,710	14,909	12,161
TOTAL		32619				

*TEMPORARY MARKING LINE PAINT WILL NOT BE REQUIRED ALONG THE 16-FOOT WORKZONE AREA.
**PERMANENT PAVEMENT MARKING SHALL BE DONE AT ONCE, BEFORE OPENING EACH INDIVIDUAL STRUCTURE.

TRAFFIC CONTROL DEVICE SUMMARY

STATION	TO	STATION	LOCATION	APPROX SERVICE PERIOD DAYS	643.0300		643.0420		643.0705		643.0715		643.0900		643.0910		643.0920		643.1050		643.1070		REMARKS
					NO. IN SERVICE	DAY	NO. IN SERVICE	DAY	NO. IN SERVICE	DAY	NO. IN SERVICE	DAY	NO. IN SERVICE	DAY	SIGNS	TYPE I	TYPE II	PCMS	CONES				
813+34.90	-	834+51.90	STH 29 EB - IH 41 NB	7	5	35	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	PRIOR TO WORK - B-05-658
				12	61	732	6	72	12	144	61	732	3	36	0	0	0	0	0	0	0	0	B-05-658
1074+01.15	-	1100+79.15	IH 41 NB - STH 29 WB	7	5	35	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	PRIOR TO WORK - B-05-660
				20	70	1,400	2	40	4	80	16	320	14	280	0	0	7	70	1,400				B-05-660
1096+21.47	-	1216+47.00	IH 41 SB - STH 29 WB	7	5	35	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	PRIOR TO WORK - B-05-661
				6	7	42	4	24	8	48	0	0	1	6	0	0	0	0	0	0	0	0	B-05-661
1173+05.83	-	1196+49.50	IH 41 NB - IH 43 SB	7	5	35	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	PRIOR TO WORK - B-05-671
				16	84	1,344	2	32	6	96	16	256	14	224	0	0	7	63	1,008				B-05-671
1179+87.75	-	1216+52.00	IH 43 NB - IH 41 SB	7	5	35	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	PRIOR TO WORK - B-05-678
				30	84	2,520	3	90	6	180	16	480	15	450	0	0	7	96	2,880				B-05-678
1187+31.83	-	1209+25.80.08	IH 43 NB - IH 41 NB	7	5	35	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	PRIOR TO WORK - B-05-679
				20	84	1,680	2	40	6	120	16	320	14	280	0	0	7	60	1,200				B-05-679
54ALF+64.95	-	66ALF+81.95	IH 41 NB - USH 45 WB	7	5	35	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	PRIOR TO WORK - B-70-269
				7	32	224	1	7	2	14	17	119	13	91	0	0	0	25	175				LANE SHIFT
			MASON STREET	7	5	35	5	35	-	-	-	-	-	-	-	-	-	-	-	-	-	-	PRIOR TO WORK - Mason St.
			"	20	3	60	5	100	10	200	0	0	1	20	0	0	0	0	0	0	0	0	Mason Street Roundabout
			"	10	57	570	5	50	10	100	0	0	5	50	0	0	0	54	540				Stage 1
			"	10	174	1,740	5	50	0	0	70	700	17	170	0	0	0	0	0	0	0	0	Stage 2
			ALGOMA BLVD	7	5	35	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	PRIOR TO WORK - Algoma Blv
			"	7	16	112	0	0	0	0	11	77	7	49	2	2	7	9	63				
			TOTAL 0010			10,739		540		982		3,004		1,656	2	2	35		7,266				

*QUANTITIES SHOWN ELSEWHERE
 **ALL COVERING SIGNS 1 CYCLE ONLY

TRAFFIC CONTROL DEVICE SUMMARY

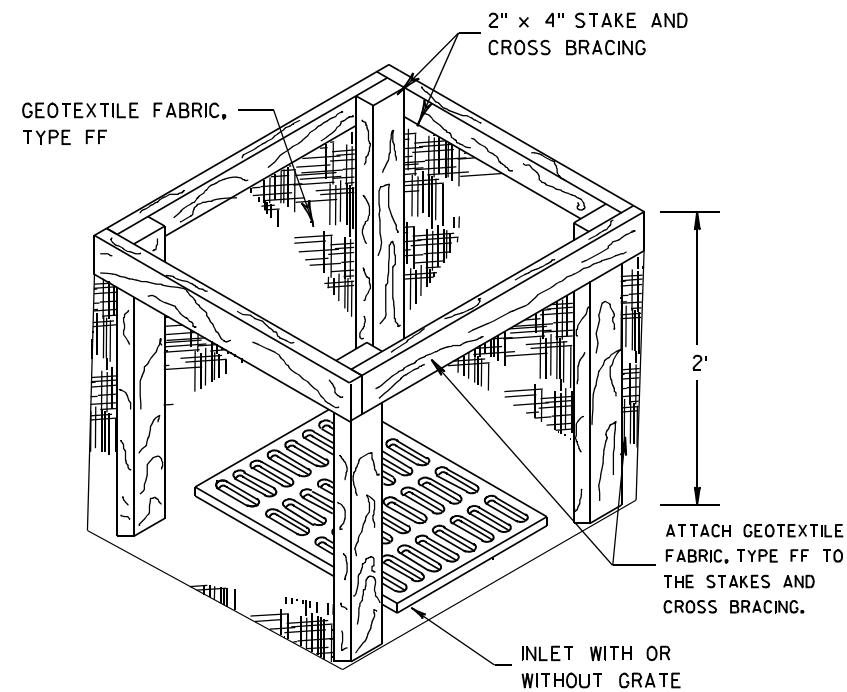
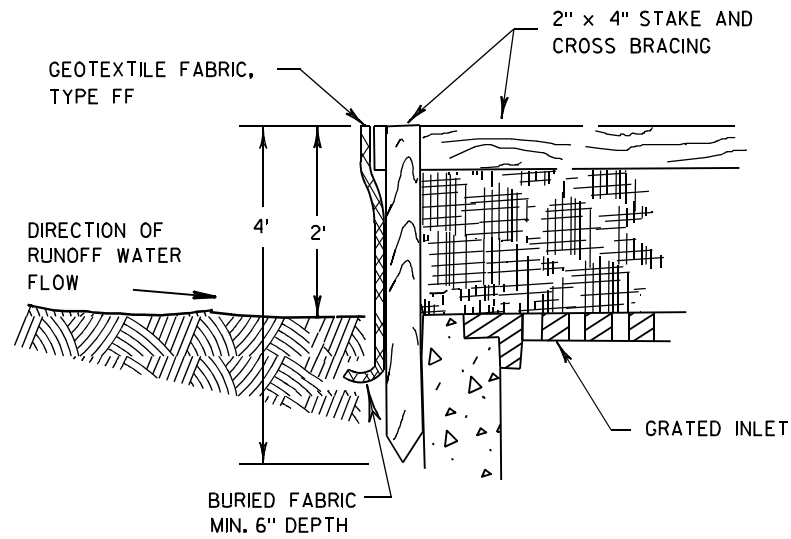
STATION	TO	STATION	LOCATION	APPROX. SERVICE PERIOD DAYS	643.0810		643.1220		Remarks
					TRAFFIC CONTROL CONNECTED ARROW BOARDS	DAY	CONNECTED WORK ZONE START AND END LOCATION MARKERS	DAY	
813+34.90	-	834+51.90	STH 29 EB - IH 41 NB	7	-	-	-	-	
				10	0	0	0	0	
1074+01.15	-	1100+79.15	IH 41 NB - STH 29 WB	7	-	-	-	-	
				18	2	36	2	36	Stage 1 & 2
1096+21.47	-	1216+47.00	IH 41 SB - STH 29 WB	7	-	-	-	-	
				10	0	0	0	0	
1173+05.83	-	1196+49.50	IH 41 NB - IH 43 SB	7	-	-	-	-	
				16	2	32	2	32	Stage 1 & 2
1179+87.75	-	1216+52.00	IH 43 NB - IH 41 SB	7	-	-	-	-	
				28	2	56	2	56	Stage 1 & 2
1187+31.83	-	1209+25.80.08	IH 43 NB - IH 41 NB	7	-	-	-	-	
				18	2	36	2	36	Stage 1 & 2
54ALF+64.95	-	66ALF+81.95	IH 41 NB - USH 45 WB	7	-	-	-	-	
				5	1	5	1	5	Stage 1 & 2
			TOTAL 0010			188		188	

643.1205.S BASIC TRAFFIC QUEUE WARNING SYSTEM				
Location	Stage	FLASHING BEACON SIGNS (FBS)	PORTABLE TRAFFIC SENSORS (PTS)	BASIC QUEUE WARNING SYSTEM (DAY)
IH 41 NB - STH 29 WB	1	6	3	9
	2	6	3	9
IH 41 NB - IH 43 SB	1	6	3	8
	2	6	3	8
IH 43 NB - IH 41 SB	1	6	3	14
	2	6	3	14
IH 43 NB - IH 41 NB	1	6	3	9
	2	6	3	9
IH 41 NB - USH 45 WB	1	6	3	5

These items are part of the QWS and are paid for one complete system per roadway

Standard Detail Drawing List

08E10-02	INLET PROTECTION TYPE A, B, C AND D
15C02-10A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-10B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-10C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C02-10D	ON RAMP LANE CLOSURE
15C02-10E	OFF RAMP LANE CLOSURE
15C02-10G	TRAFFIC CONTROL FOR ENTRANCE RAMP CLOSURE
15C08-24A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C08-24B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15D04-01	TRAFFIC CONTROL, RAMP CONSTRUCTION STAGING
15D12-16A	TRAFFIC CONTROL, VEHICLE ENTRANCE/EXIT OR HAUL ROAD
15D12-16B	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D12-16D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D12-16E	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D16-07	TRAFFIC CONTROL, EXIT RAMP CLOSURE
15D20-11B	TRAFFIC CONTROL, SINGLE RIGHT LANE CLOSURE, UNDIVIDED NON-FREEWAY/EXPRESSWAY
15D29-06	TRAFFIC CONTROL, VEHICLE ENTRANCE/EXIT OR HAUL ROAD
15D40-07B	TRAFFIC CONTROL, FULL LANE SHIFT MULTI LANE DIVIDED 50 MPH AND OVER
15D40-07D	TRAFFIC CONTROL, PARTIAL LANE SHIFT MULTI LANE DIVIDED 50 MPH AND GREATER
15D47-03B	TRAFFIC CONTROL, INGRESS/EGRESS WITHOUT BARRIER
15D49-05	TRAFFIC CONTROL, SYSTEM RAMP CLOSURE



INLET PROTECTION, TYPE A

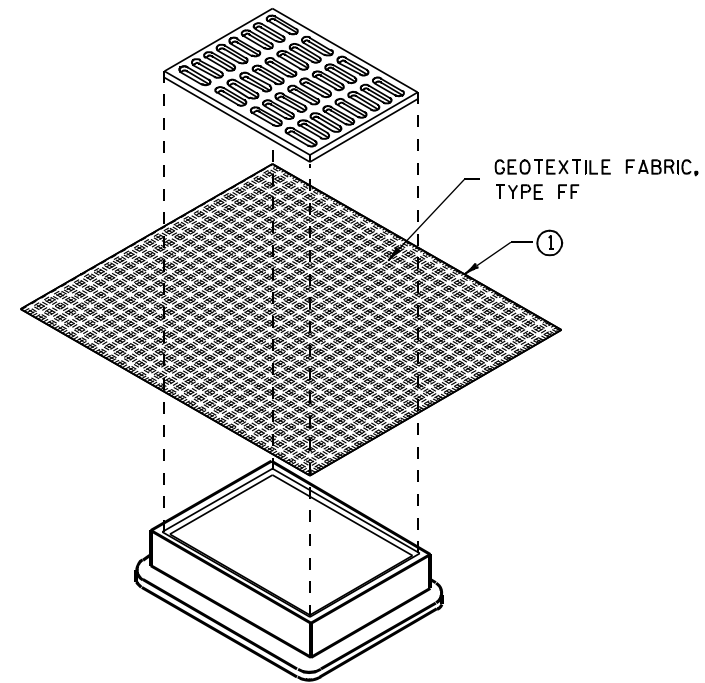
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

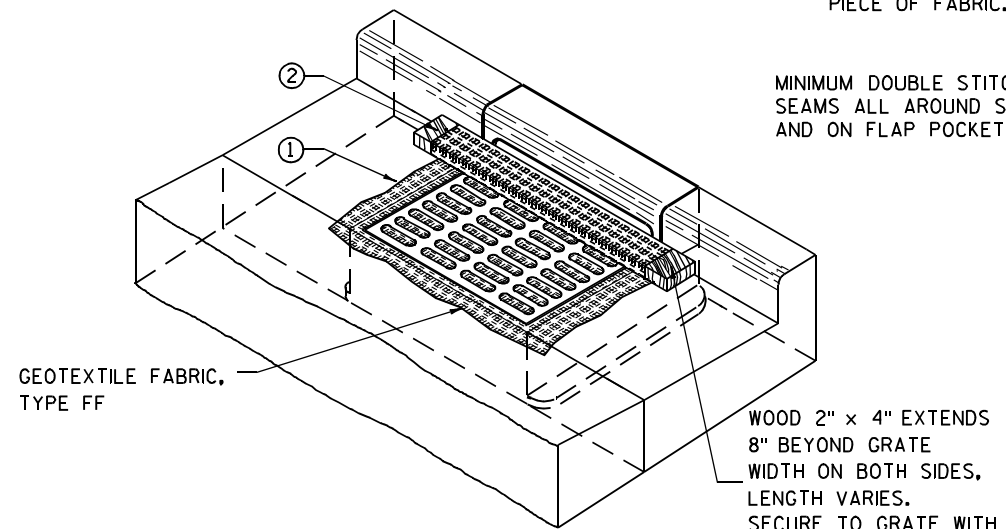
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

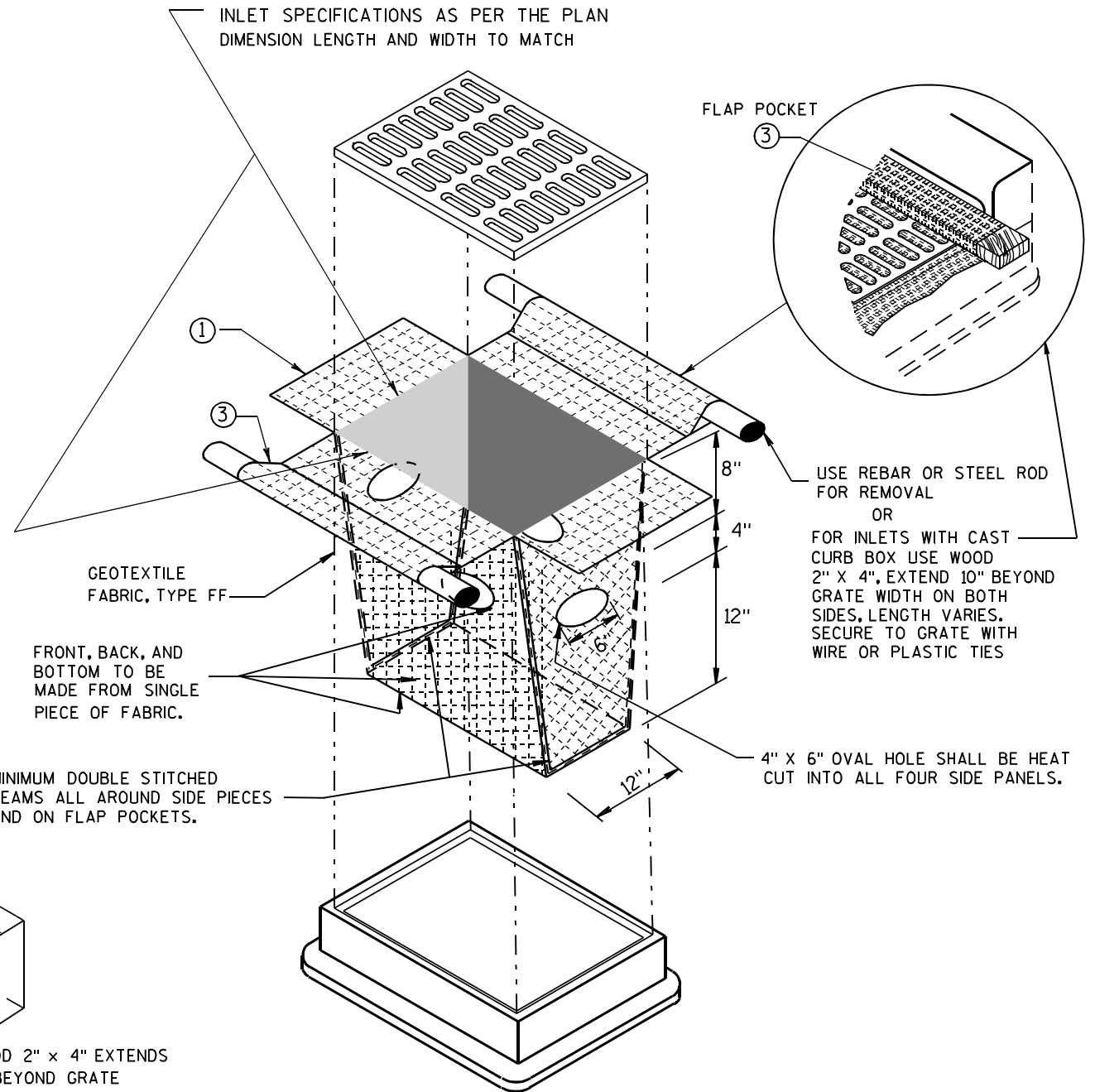
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

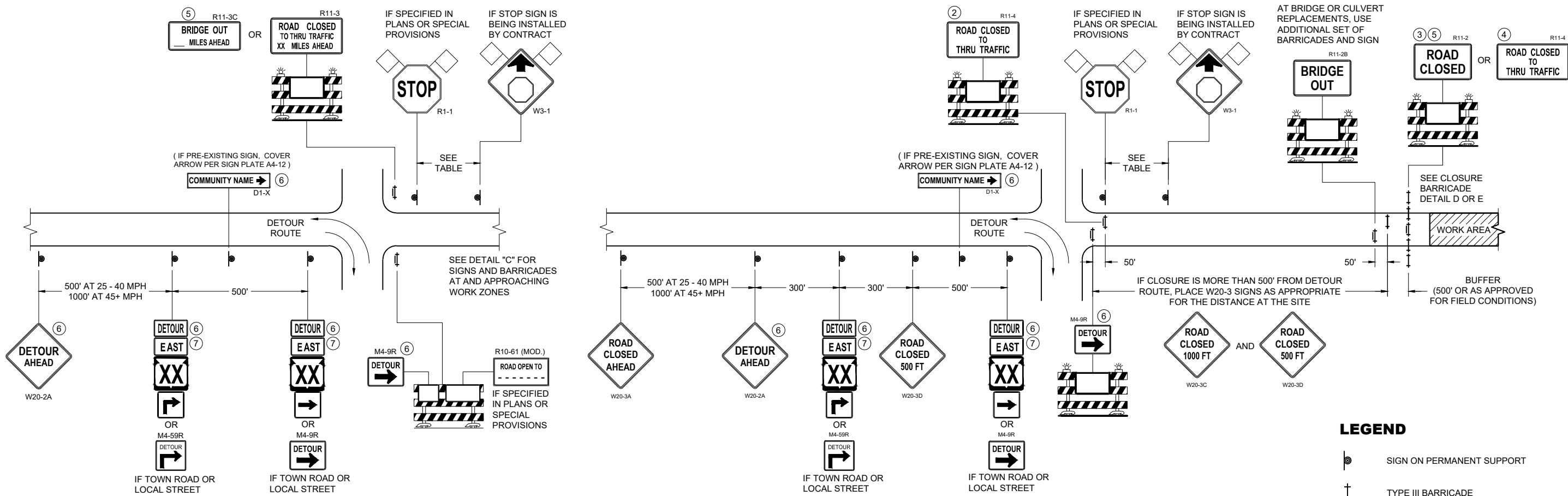
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



INLET PROTECTION, TYPE D

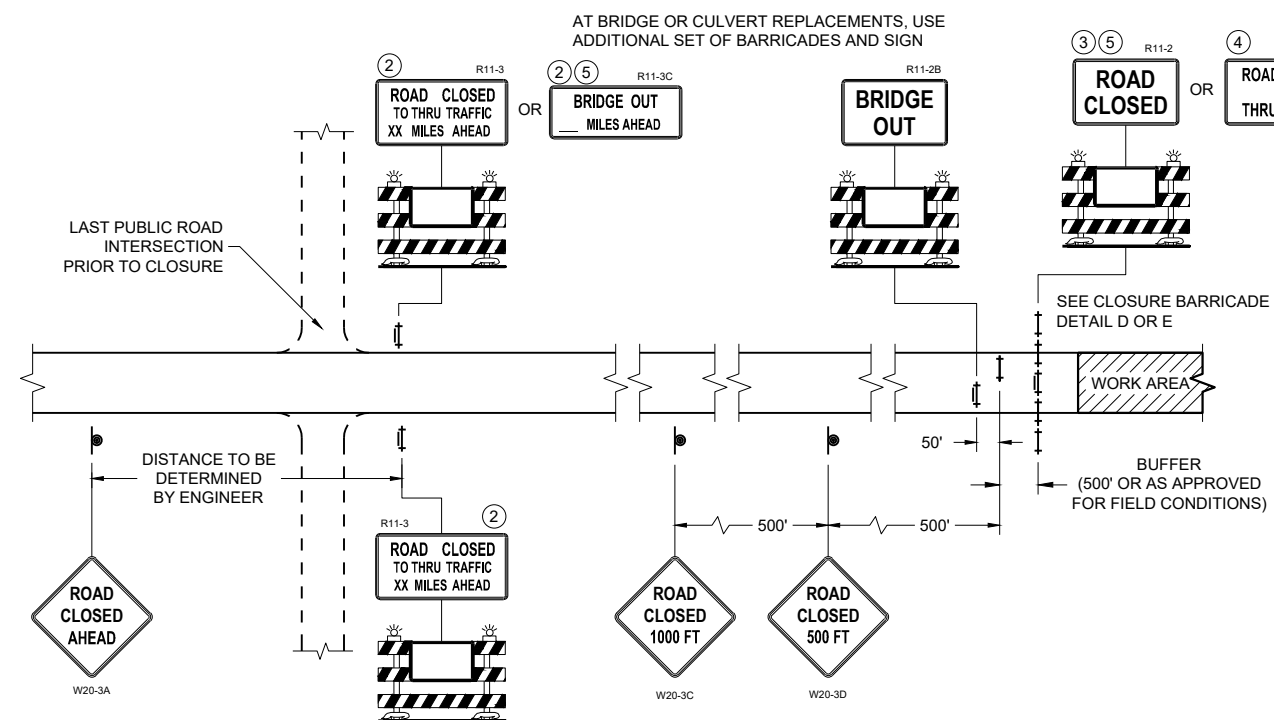
(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

INLET PROTECTION TYPE A, B, C, AND D	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/s/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR
 WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
 DETOUR ROUTE (1000 FEET IF URBAN)

DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR
 WORK ZONE LESS THAN 1/2 MILE FROM
 DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
-

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

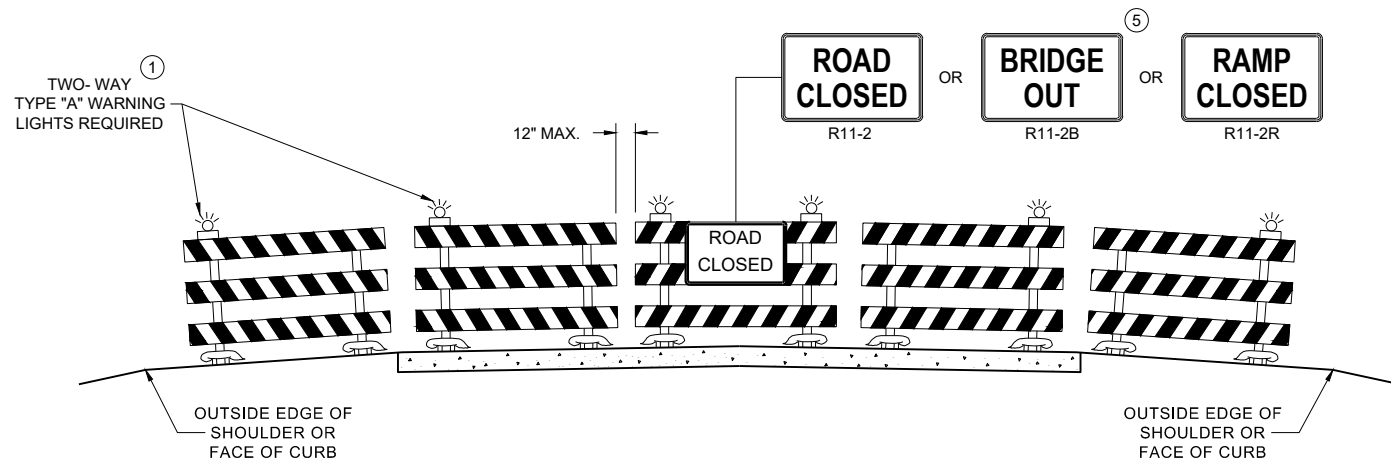
SEE SDD 15C2-SHEET "b" FOR GENERAL NOTES AND FOOTNOTES ① THROUGH ⑦

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

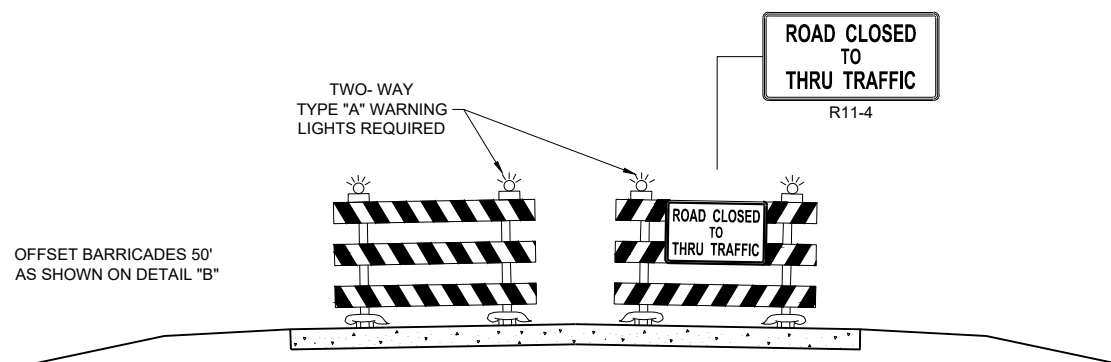
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
 November 2025 /S/ Andrew Heidtke
 DATE WORK ZONE ENGINEER

FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

PARTIAL NUMBERS ON SIGNS SHALL BE DISPLAYED AS A WHOLE NUMBER (AS NEEDED) FOLLOWED BY A FRACTION. SIGNS SHALL NOT DISPLAY NUMBERS IN DECIMAL FORM.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

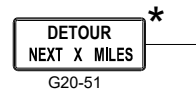
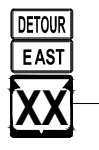
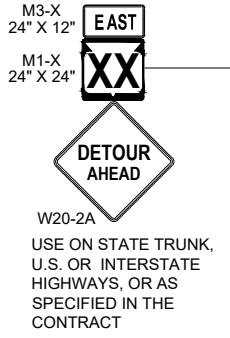
ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

SIGNS PLACED ON TYPE III BARRICADES THAT ARE SIZES OTHER THAN 48"X30" SHALL HAVE A CORRUGATED POLYPROPYLENE OR POLYETHYLENE PLASTIC SIGN BASE.

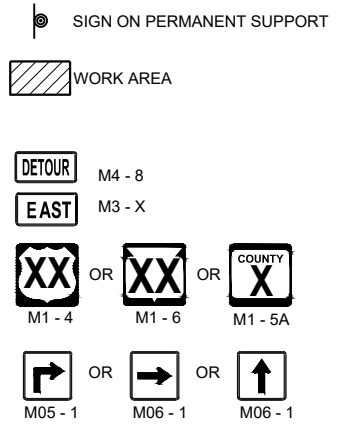
- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR VARIOUS CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2025 DATE	/s/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND



GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

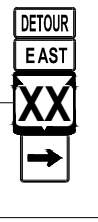
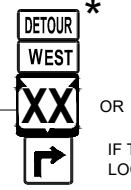
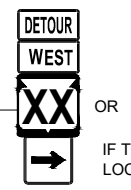
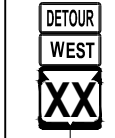
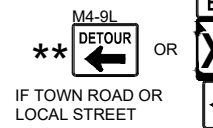
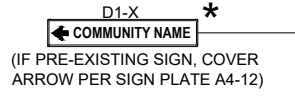
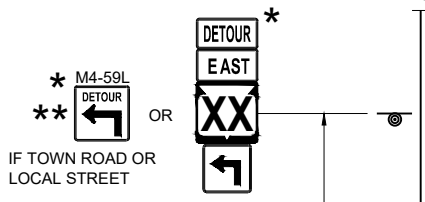
SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:
 M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
 M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
 M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
 M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
 M4-9 AND M4-59 SHALL BE 30" X 24"
 M4-8a SHALL BE 24" X 18"
 G20-51 SHALL BE 60" X 24"
 W20-2A SHALL BE 48" X 48"
 D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

- * OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- ** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

MATCH POINT



PLACE SIGNS BEYOND INTERSECTIONS WITH STATE OR COUNTY TRUNK HIGHWAYS OR AT 4 MILE MAXIMUM SPACING (4 BLOCKS IF URBAN AREA)

**DETAIL F
DETOUR SIGNING**

SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

DETOUR SIGNING FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2025 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL TO SIGN LAYOUT AND SPACING. SEE PROJECT TO SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- TO EAST MO4 - 5
- TO EAST M3 - X
- XX M1 - 6 OR XX M1 - 4 OR INTERSTATE XX M1 - 1
- ↶ OR ↷ OR ↵ M05 - 1 M06 - 1 M06 - 1

GENERAL NOTES

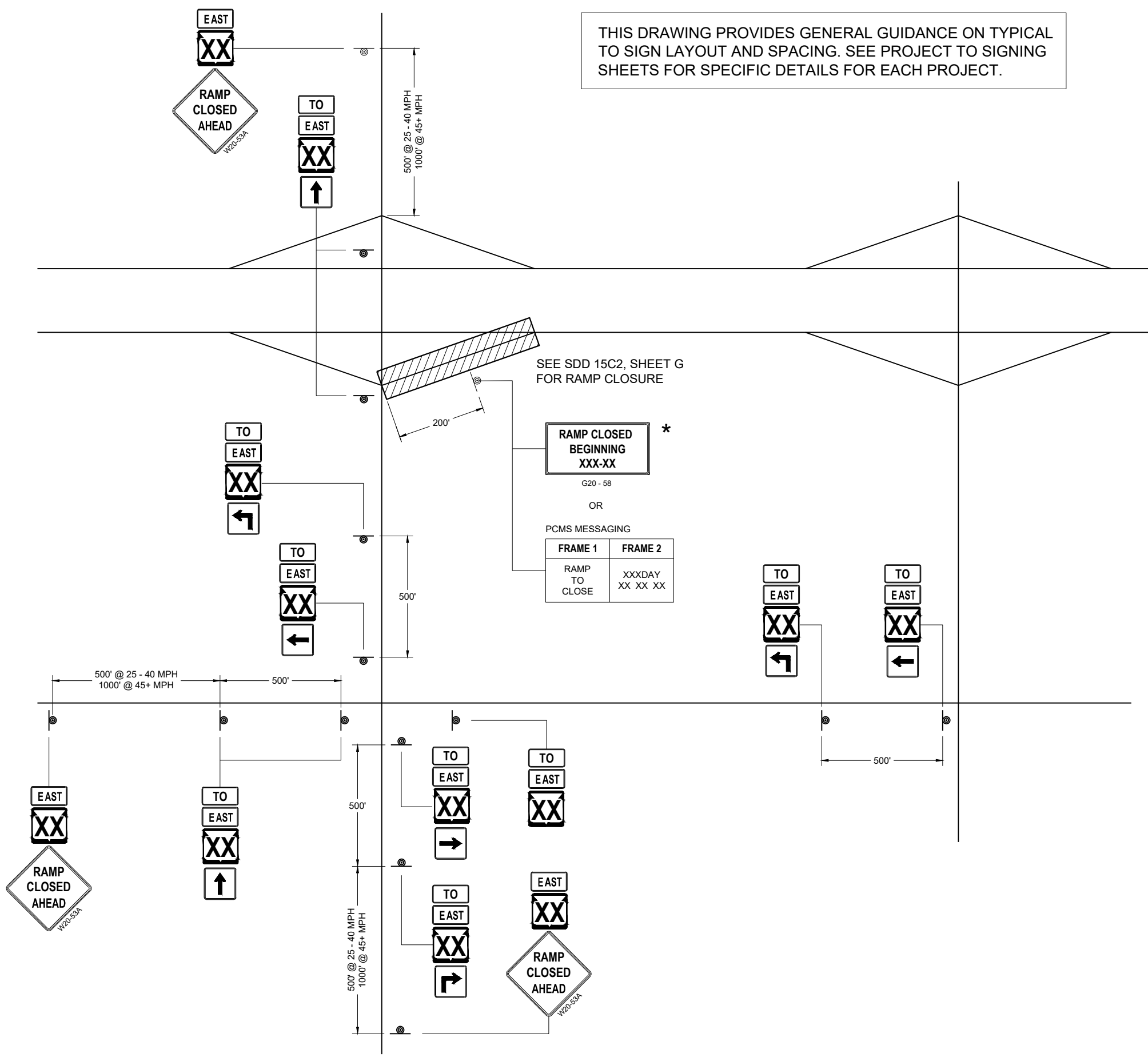
- THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- IF THERE ARE ANY ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE TO ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT TO SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.
- THE SPACING BETWEEN TRAFFIC CONTROL AND TO SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.
- ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.
- SIGNS THAT SHALL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- "MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- SIGN SIZES SHALL BE AS FOLLOW:
 M3 - X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS).
 MO4 - 5 SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS).
 M1 - 1, M1 - 4, AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS).
 MO5 - 1, MO5 - 2, AND MO6 - 1, SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS).
 W20 - 53A SHALL BE 48" X 48"
- * PLACE "RAMP CLOSED BEGINNING" SIGN 7 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR LAYOUT.

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SDD 15C02-10d

SDD 15C02-10d



**ON RAMP
LANE CLOSURE**












STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
 November 2025 /S/ Andrew Heidtke
 DATE WORK ZONE ENGINEER

FHWA

THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL "TO" MO-4 SIGN LAYOUT AND SPACING. SEE PROJECT TO SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

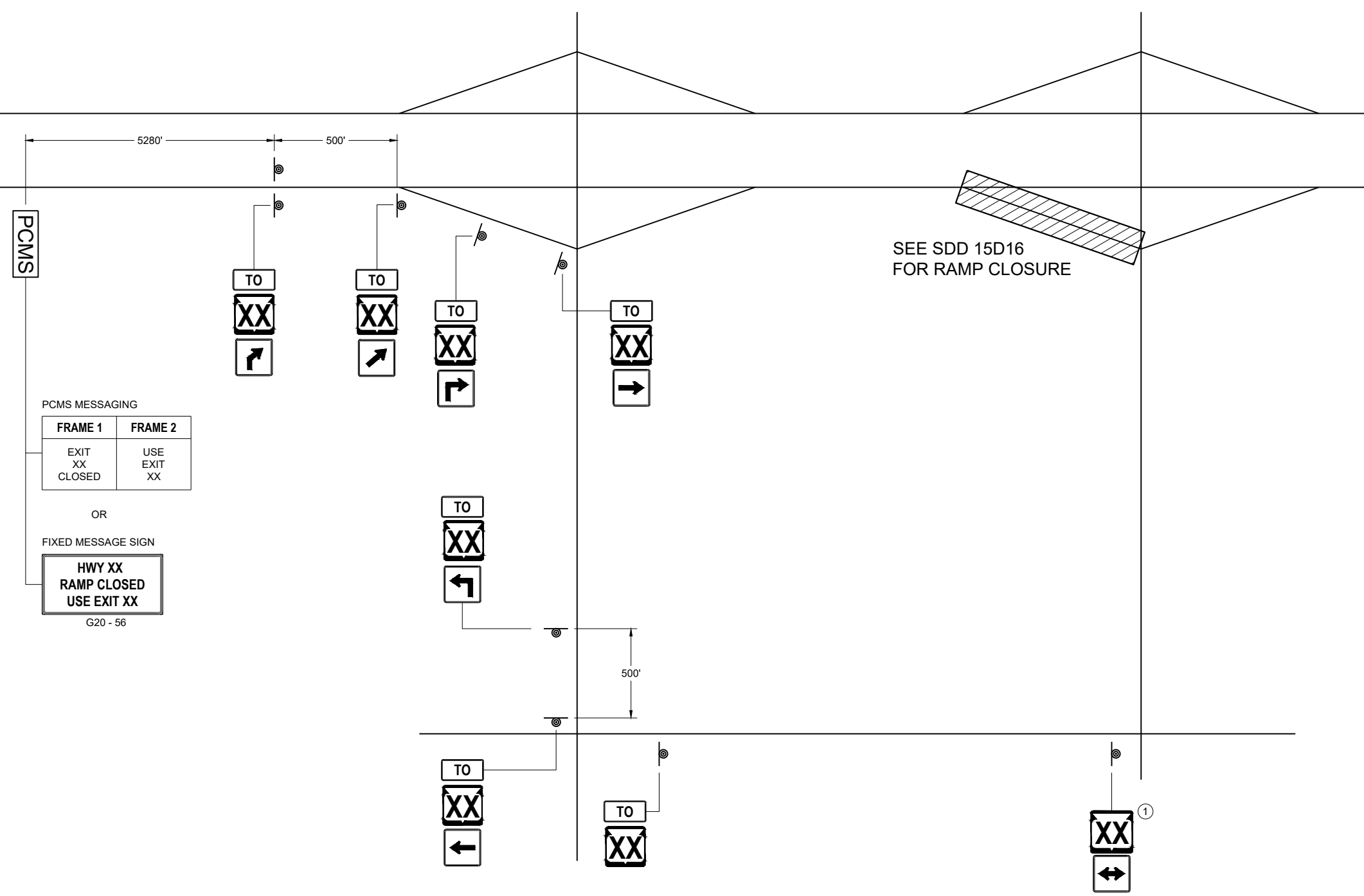
LEGEND

-  SIGN ON PERMANENT SUPPORT
-  PORTABLE CHANGEABLE MESSAGE SIGN
-  MO4 - 5
-  M1 - 4 OR  M1 - 6 OR  M1 - 5A
-  M05 - 1 OR  M05 - 2 OR  M06 - 1 OR  M06 - 2 OR  M06 - 4

GENERAL NOTES

- SEE SDD 15D16 "TRAFFIC CONTROL, EXIT RAMP CLOSURE" DETAIL FOR TRAFFIC CONTROL AT EXIT RAMP CLOSURE.
- THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- IF THERE ARE ANY ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE "TO" MO-4 ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT TO SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.
- THE SPACING BETWEEN TRAFFIC CONTROL AND "TO" MO-4 SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.
- ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.
- SIGNS THAT SHALL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- "MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- SIGN SIZES SHALL BE AS FOLLOW:
 - MO4 - 5 SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS).
 - M1 - 4, M1 - 5A, AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS).
 - M05 - 1, M05 - 2, AND M06 - 1, SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS).
- ① ONLY ADD IF THERE ARE NO EXISTING ROUTE MARKERS FOR THE INTERSECTING ROADWAY.

SEE SDD 15D16 FOR RAMP CLOSURE



PCMS MESSAGING

FRAME 1	FRAME 2
EXIT XX CLOSED	USE EXIT XX

OR

FIXED MESSAGE SIGN

HWY XX
RAMP CLOSED
USE EXIT XX

G20 - 56

**OFF RAMP
LANE CLOSURE**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2025 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

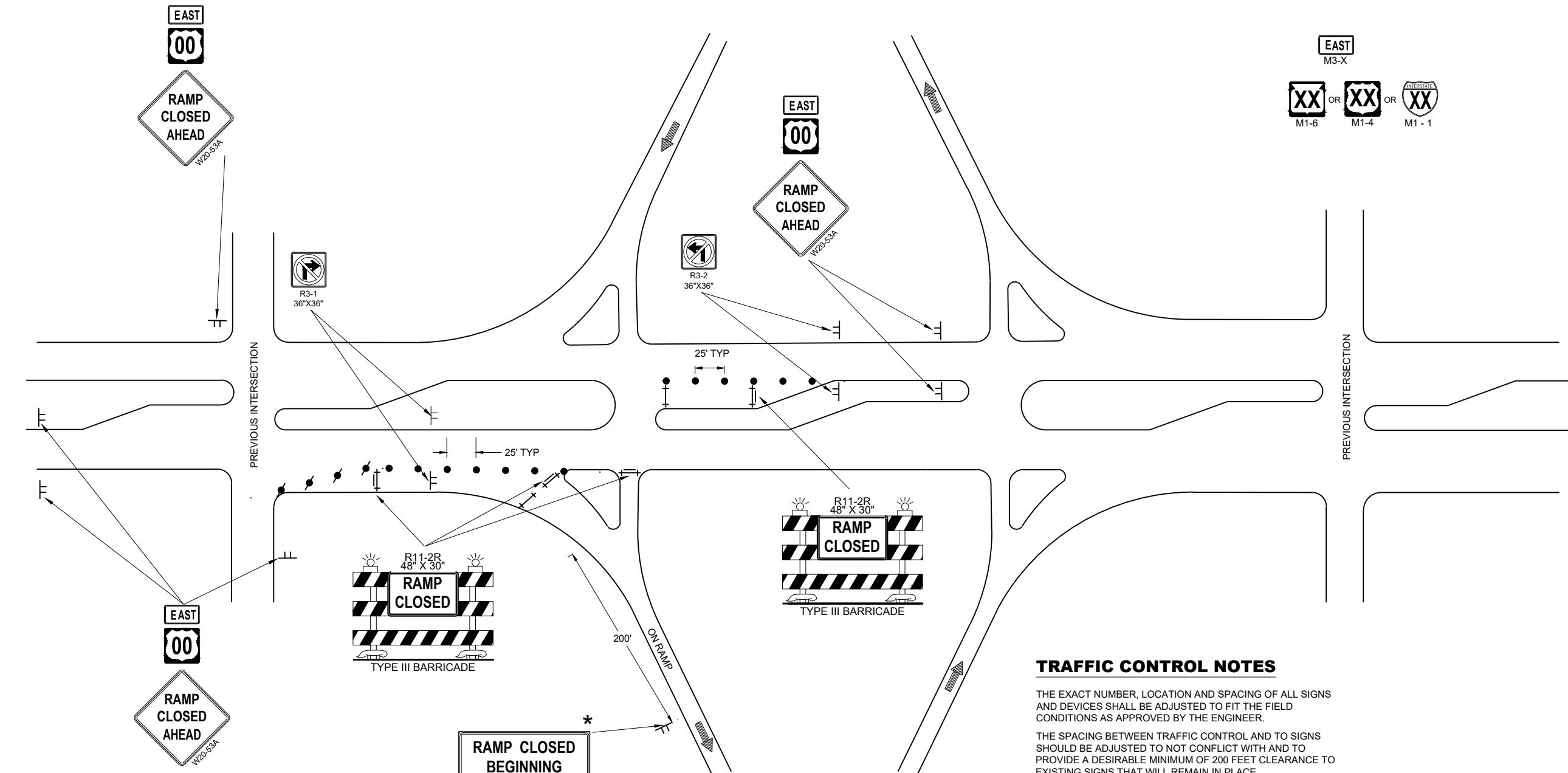
FHWA

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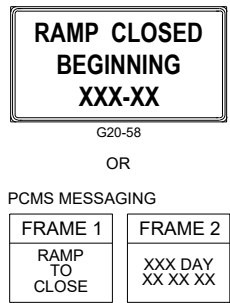
SDD 15C02-10e

SDD 15C02-10e



LEGEND

- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- ↑ TYPE III BARRICADE
- ↑↓ TYPE III BARRICADE WITH ATTACHED SIGN
- ↑ SIGN ON PERMANENT SUPPORT
- ↑ SIGN ON TEMPORARY SUPPORT
- DIRECTION OF TRAFFIC



TRAFFIC CONTROL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT THE FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL AND TO SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT SHALL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

SIGN SIZES SHALL BE AS FOLLOWS:

- M3-X SHALL BE 24"x24" (36" x18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-1, M1-4, AND M1-6 SHALL BE 24"x24" (36"x36" IF NEEDED TO MATCH EXISTING SIGNS)
- W20-53A SHALL BE 48"x48"

* PLACE "RAMP CLOSED BEGINNING" SIGN 7 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR LAYOUT.

TRAFFIC CONTROL FOR ENTRANCE RAMP CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2025 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER




FHWA

GENERAL NOTES

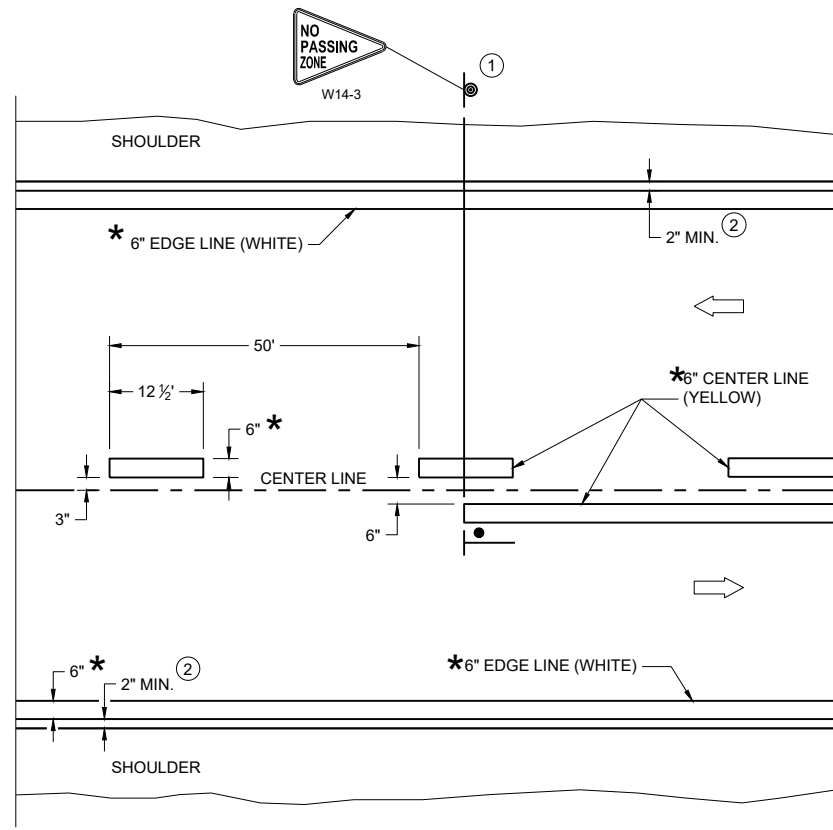
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

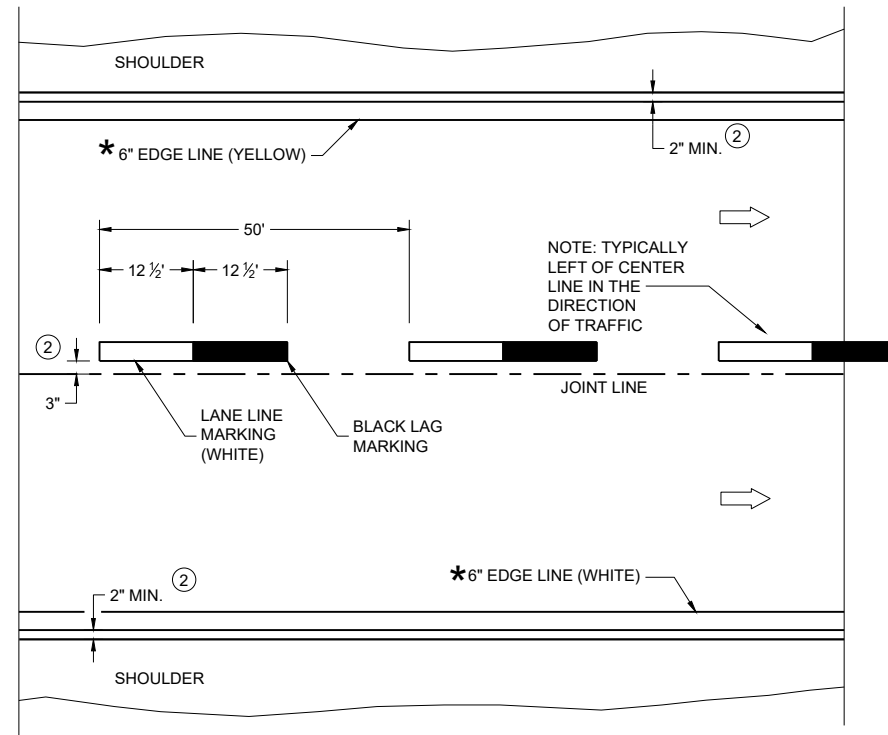
LEGEND

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

6

6

SDD 15C08-24a

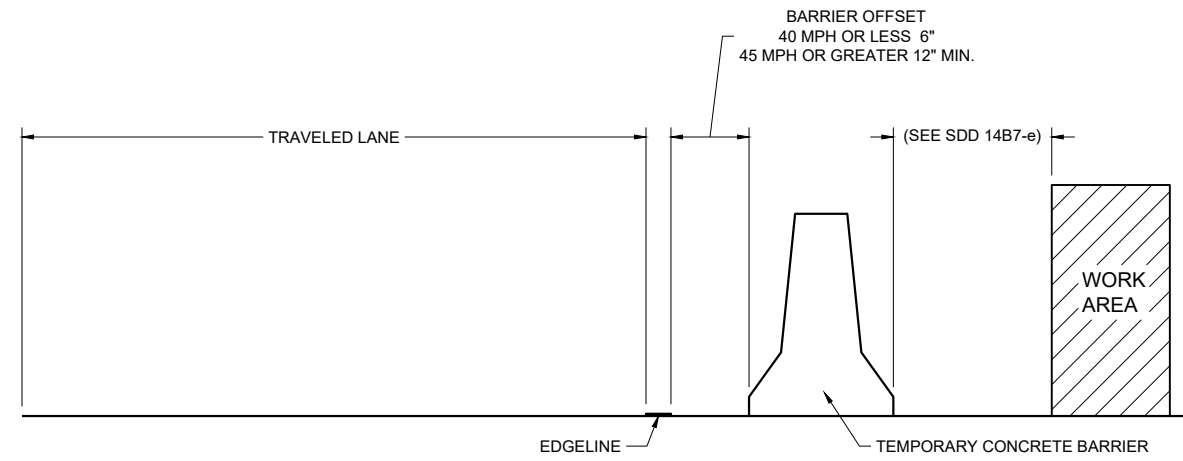
SDD 15C08-24a

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2024 /S/ Jeannie Silver
DATE Statewide Pavement Marking Engineer

FHWA



TEMPORARY BARRIER OFFSET FROM EDGELINE

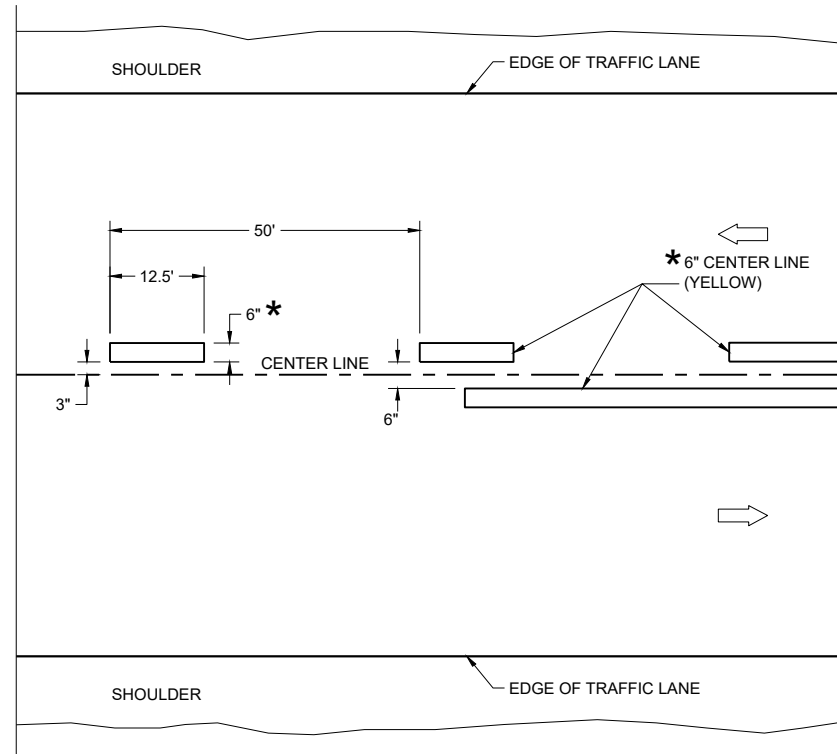
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

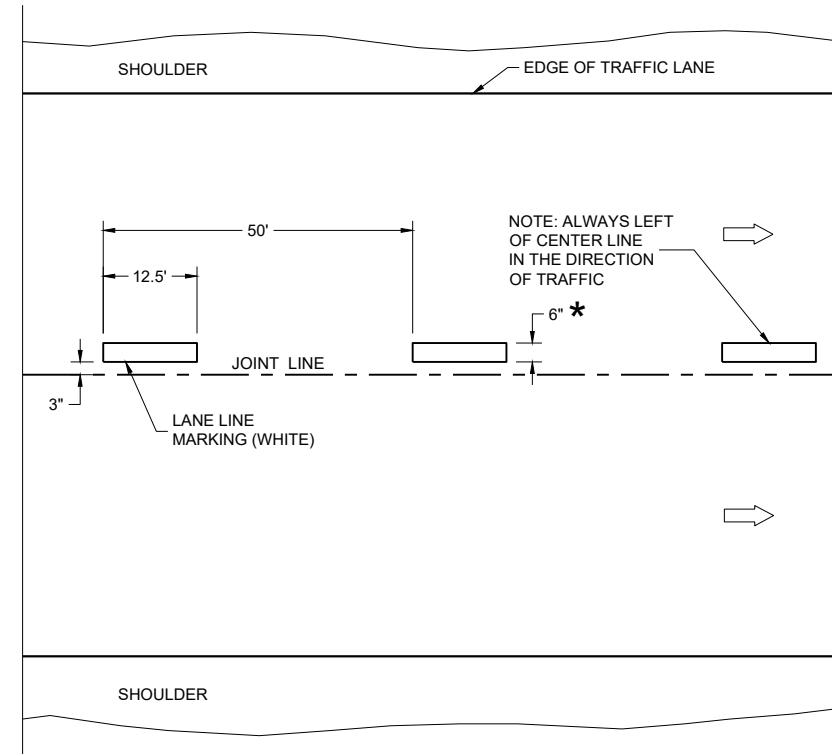
LEGEND

➡ DIRECTION OF TRAFFIC

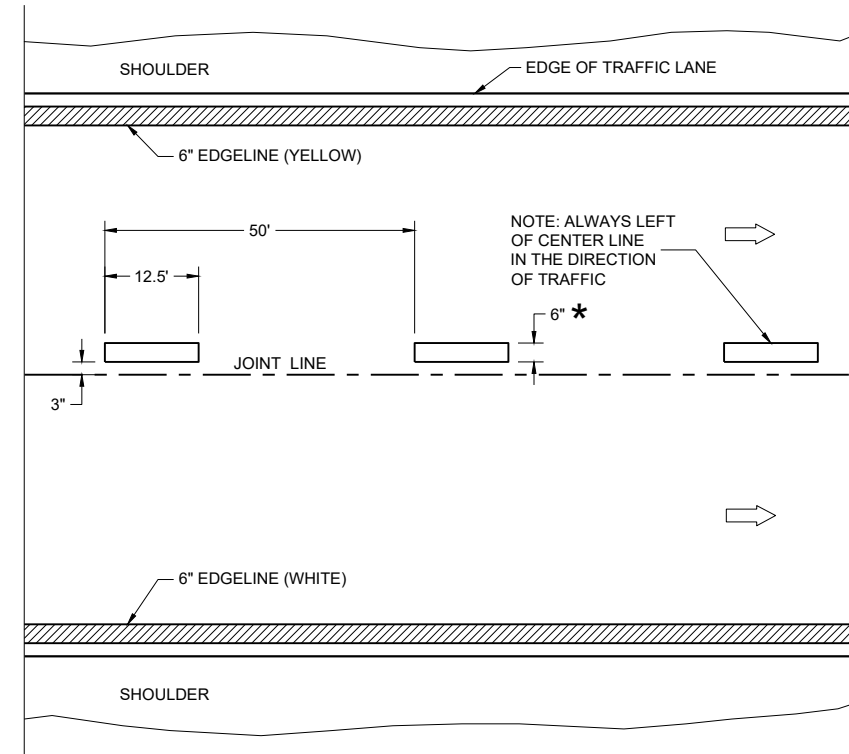
*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

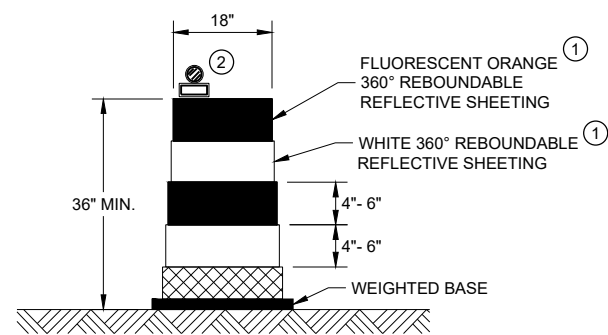
TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

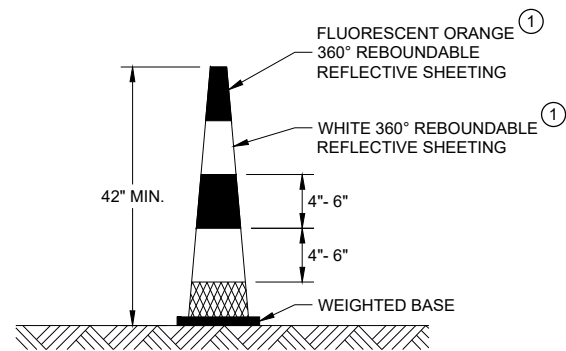
APPROVED
December 2024 /S/ Jeannie Silver
DATE Statewide Pavement Marking Engineer

FHWA



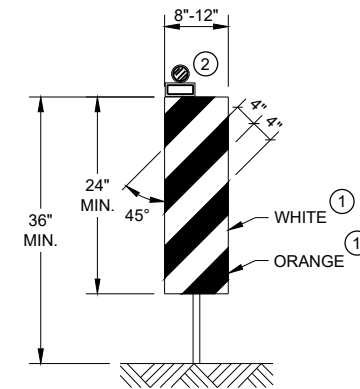
DRUM

BALLAST WIDTHS
RANGE FROM 24"-36"



42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS
BALLAST WIDTHS
RANGE FROM 14"-20"

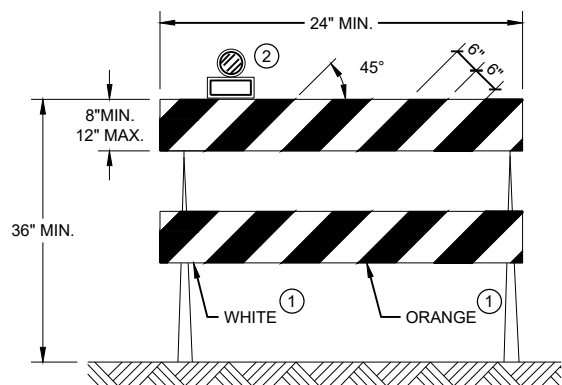


VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.

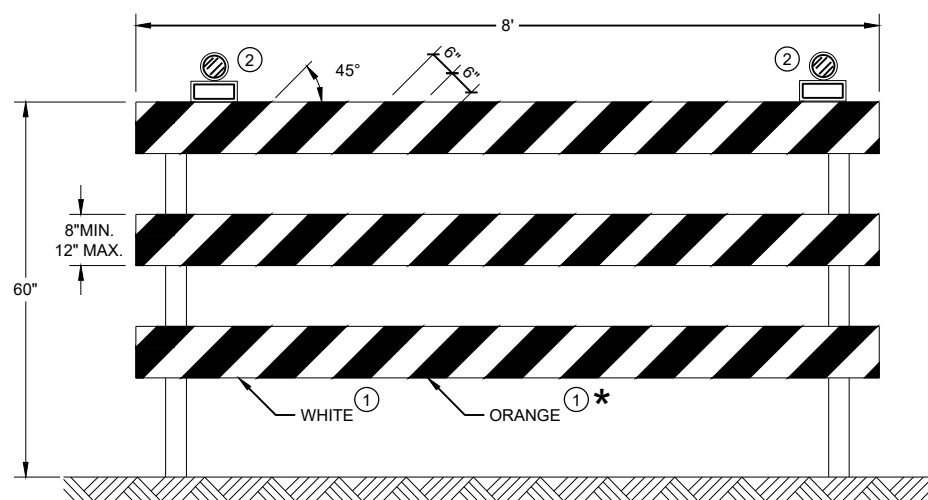
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.

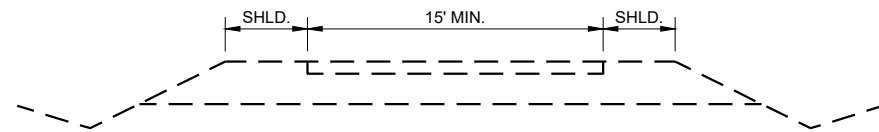


TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

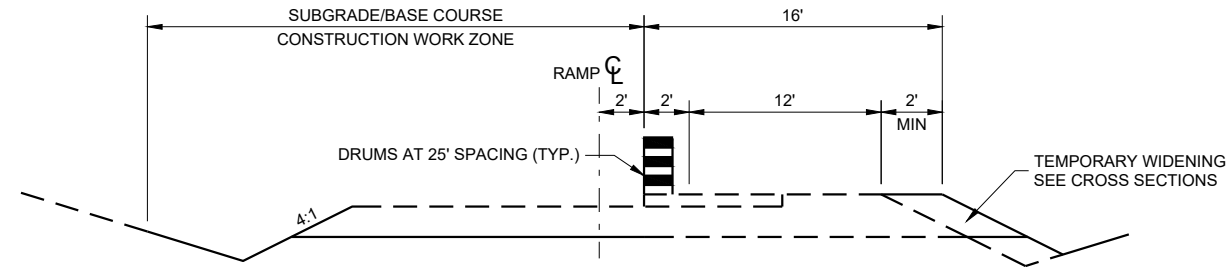
CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



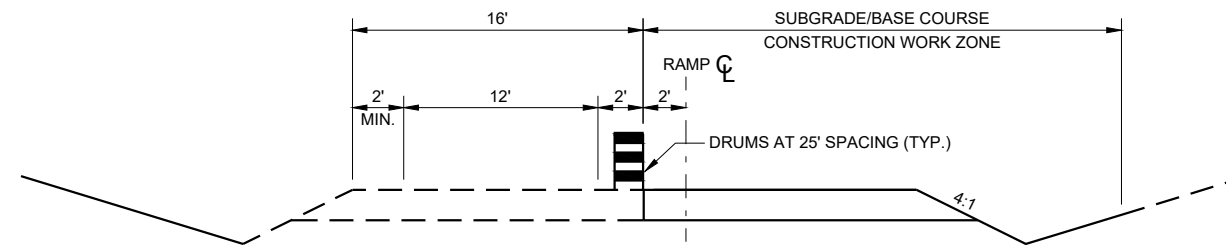
INITIAL RAMP - TYPICAL

GENERAL NOTES

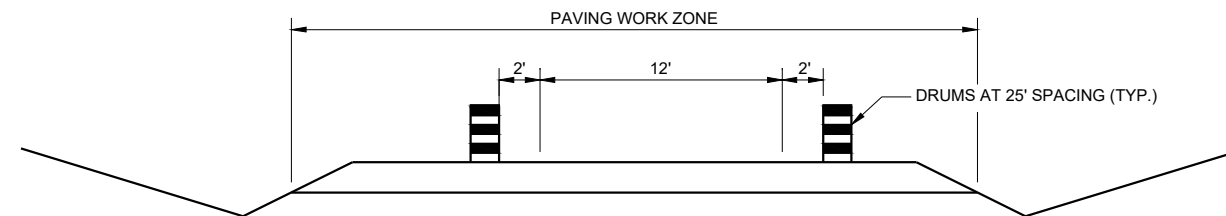
WORK SHALL BE SCHEDULED FOR ALL STAGES TO MINIMIZE INCONVENIENCE TO THE TRAFFIC USING THE RAMP WHILE IT IS ONLY BASE COURSE.
 IN STATE III, A MINIMUM 12' LANE WIDTH SHALL BE DELINEATED WITH DRUMS ALONG BOTH EDGES AT ALL TIMES UNTIL PAVED AND PAVEMENT MARKINGS ARE PLACED.



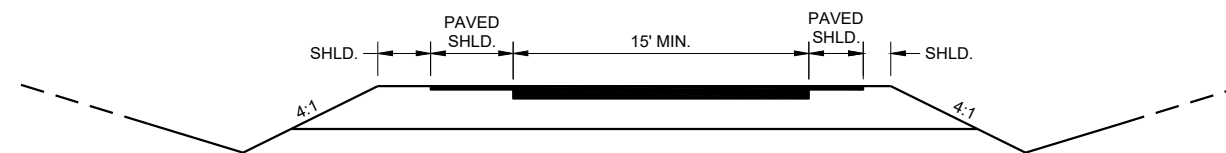
STAGE I - CONSTRUCTION TO SUBGRADE, LEFT



STAGE II - CONSTRUCTION TO SUBGRADE, RIGHT



STAGE III - TRAFFIC ON BASE COURSE



STAGE IV - PAVING

6

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SDD 15D04 - 01

SDD 15D04 - 01

TRAFFIC CONTROL, RAMP CONSTRUCTION STAGING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED July 1994 DATE	/S/ Chester J. Spang DIRECTOR, OFFICE OF TRAFFIC
<small>FHWA</small>	

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

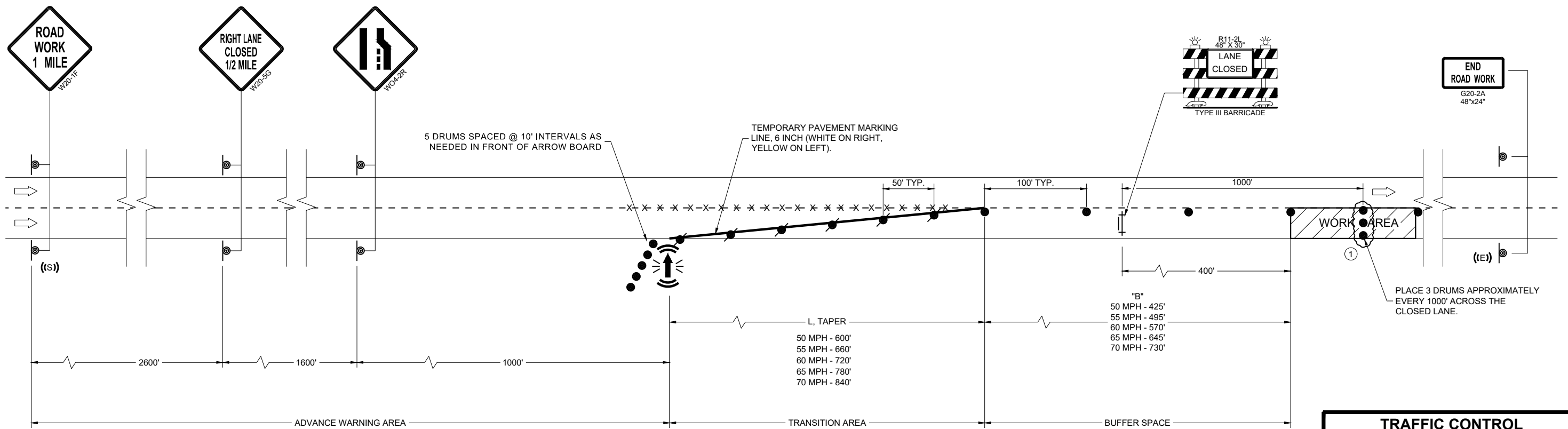
① DRUMS IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- ⊕ TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- × × × REMOVING PAVEMENT MARKINGS
- ➡ DIRECTION OF TRAFFIC
- ▨ WORK AREA
- ⊕ ((➡)) CONNECTED ARROW BOARD
- (S) WZ START LOCATION MARKER
- (E) WZ END LOCATION MARKER

6

SDD 15D12-16a



6

SDD 15D12-16a

**TRAFFIC CONTROL
LANE CLOSURE**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2025 /S/ Andrew Heidtke
DATE ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

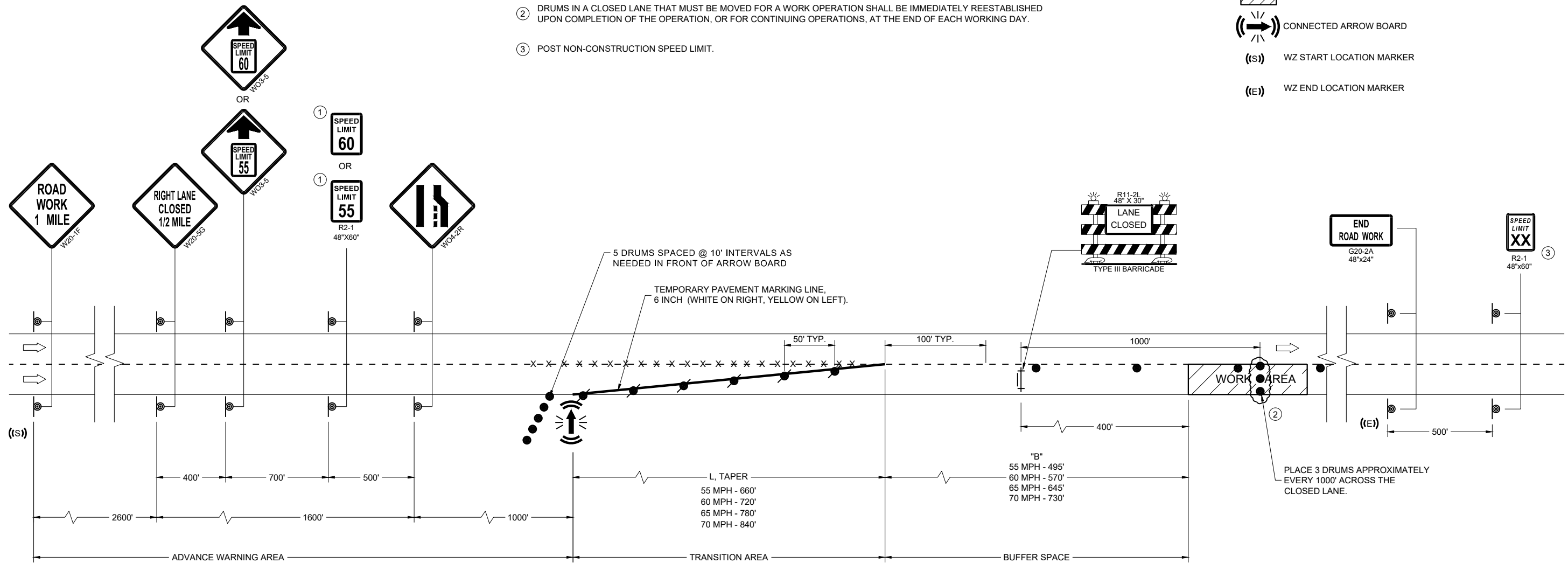
IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

- ① A SPEED LIMIT SIGN SHALL BE LOCATED 1500 FEET BEYOND THE END OF THE ACCELERATION LANE OF EACH ENTRANCE RAMP. PLACE A SPEED LIMIT SIGN A MINIMUM OF EVERY 3 MILES.
- ② DRUMS IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.
- ③ POST NON-CONSTRUCTION SPEED LIMIT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKINGS
- DIRECTION OF TRAFFIC
- WORK AREA
- CONNECTED ARROW BOARD
- WZ START LOCATION MARKER
- WZ END LOCATION MARKER



6















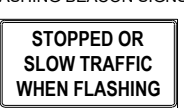
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SDD 15D12-16b

SDD 15D12-16b

TRAFFIC CONTROL, LANE CLOSURE, SPEED REDUCTION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2025 DATE	/s/ Andrew Heidtke ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  REMOVING PAVEMENT MARKINGS
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  WZ START LOCATION MARKER
-  WZ END LOCATION MARKER
-  CONNECTED ARROW BOARD
-  PORTABLE CHANGEABLE MESSAGE SIGN
-  PORTABLE TRAFFIC SENSOR (PTS)
-  FLASHING BEACON SIGNS
-  STOPPED OR SLOW TRAFFIC WHEN FLASHING
W08-76
96" x 48"

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS, INCLUDING FBS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON "COVERED" OR "DOWNED" SIGNS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS. WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

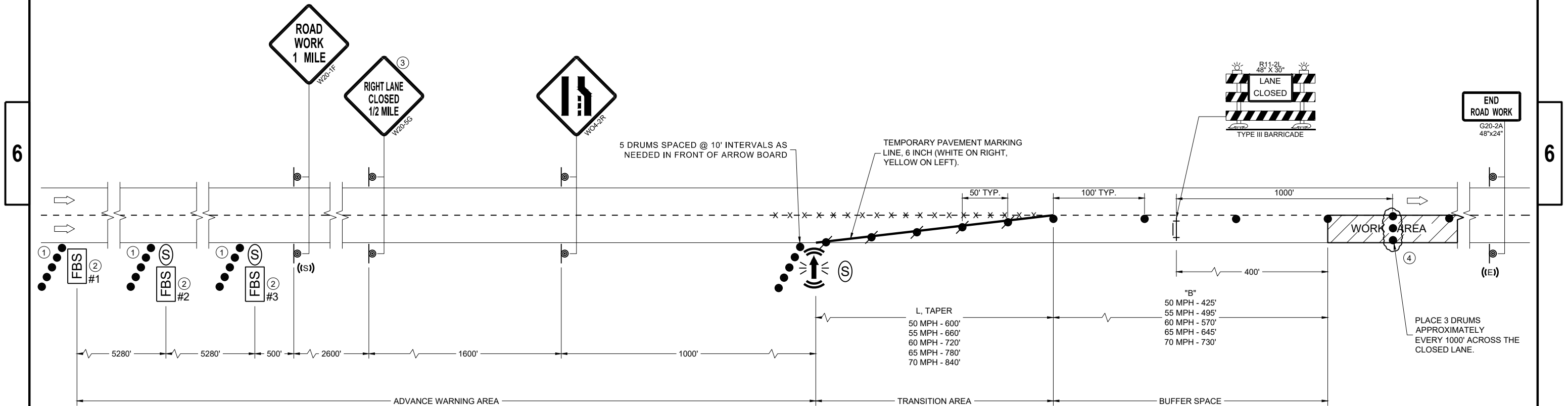
IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

PORTABLE TRAFFIC SENSOR (PTS) MAY BE MOUNTED ON THE FBS, ARROW BOARD OR OTHER TRAILER DEVICES.

- ① 5 DRUMS SPACED AT 10 FOOT INTERVALS AS NEEDED.
- ② IF THERE ARE MORE THAN TWO LANES OR IF SPECIFIED IN THE PLANS, PLACE FBS ON BOTH SIDES OF THE ROADWAY.
- ③ IF THERE IS AN APPROVED TEMPORARY SPEED DECLARATION, ADD WO-3-5 SIGNS 400 FEET AFTER THE W20-5G SIGNS AND ADD R2-1 SIGNS (48"x60") 700 FEET AFTER THE WO3-5 SIGNS. A SPEED LIMIT SIGN SHALL BE LOCATED 1500 FEET BEYOND THE END OF THE ACCELERATION LANE OF EACH ENTRANCE RAMP. PLACE A SPEED LIMIT SIGN A MINIMUM OF EVERY 3 MILES. INCLUDE A "RESUME SPEED LIMIT" SIGN 200 FEET MINIMUM (800 FEET DESIRABLE) BEYOND THE G30-3A "END ROAD WORK" SIGN
- ④ DRUMS IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.



SDD 15D12-16d

SDD 15D12-16d

TRAFFIC CONTROL, LANE CLOSURE, BASIC TRAFFIC QUEUE WARNING SYSTEM

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2025 /S/ Erin Schwark
DATE ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

FHWA

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- x-x-x- REMOVING PAVEMENT MARKINGS
- ➡ DIRECTION OF TRAFFIC
- ▨ WORK AREA
- ⊕ CONNECTED ARROW BOARD
- PCMS PORTABLE CHANGEABLE MESSAGE SIGN
- Ⓢ PORTABLE TRAFFIC SENSOR (PTS)
- (s) WZ START LOCATION MARKER
- (E) WZ END LOCATION MARKER

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS, INCLUDING FBS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON "COVERED" OR "DOWNED" SIGNS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

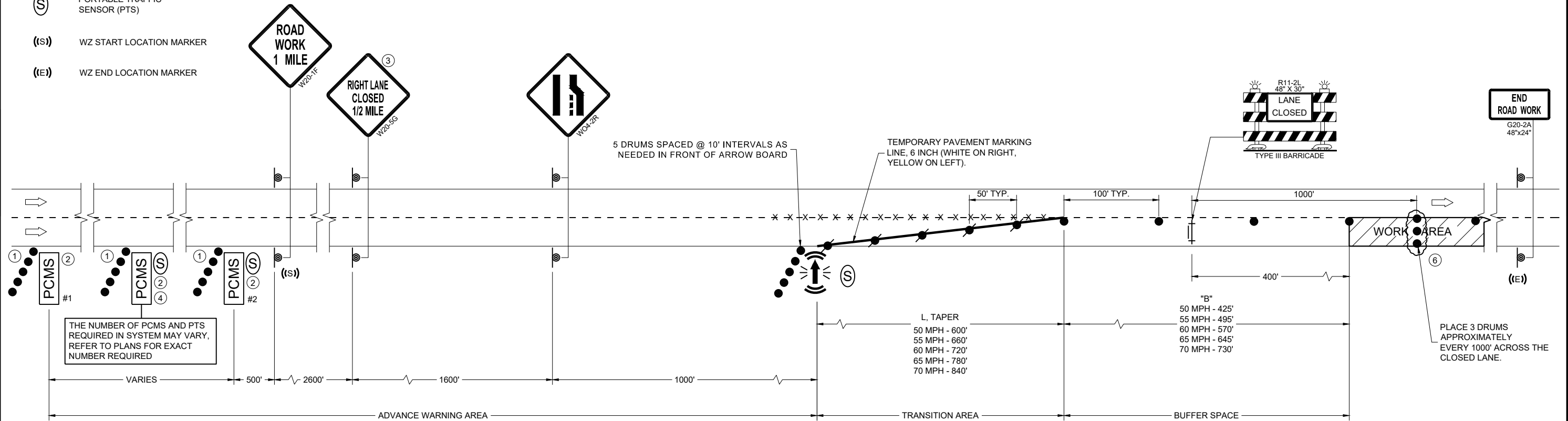
IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

PORTABLE TRAFFIC SENSOR (PTS) MAY BE MOUNTED ON PCMS, ARROW BOARD OR OTHER TRAILER DEVICES.

- ① 5 DRUMS SPACED AT 10 FOOT INTERVALS AS NEEDED.
- ② IF THERE ARE MORE THAN TWO LANES OR IF SPECIFIED IN THE PLANS, PLACE PCMS ON BOTH SIDES OF THE ROADWAY.
- ③ IF THERE IS AN APPROVED TEMPORARY SPEED DECLARATION, ADD WO-3-5 SIGNS 400 FEET AFTER THE W20-5G SIGNS AND ADD R2-1 SIGNS (48"x60") 700 FEET AFTER THE WO3-5 SIGNS. A SPEED LIMIT SIGN SHALL BE LOCATED 1500 FEET BEYOND THE END OF THE ACCELERATION LANE OF EACH ENTRANCE RAMP. PLACE A SPEED LIMIT SIGN A MINIMUM OF EVERY 3 MILES. INCLUDE A "RESUME SPEED LIMIT" SIGN 200 FEET MINIMUM (800 FEET DESIRABLE) BEYOND THE G30-3A "END ROAD WORK" SIGN.
- ④ PLACE PCMS EVERY ONE MILE BETWEEN PCMS #1 AND PCMS #2, OR AS THE ENGINEER DIRECTS. THE NUMBER OF PCMS MAY BE MORE THAN SHOWN ON THIS DETAIL.
- ⑤ PCMS SHALL FOLLOW ARROW BOARD STANDARDS WHEN DISPLAYING FLASHING FOUR CORNER CAUTION MODE OR ARROW MERGE MODE.
- ⑥ DRUMS IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.



⑤ PCMS MESSAGING

SPEEDS 0 MPH TO 19 MPH		SPEEDS 20 MPH TO 39 MPH		SPEEDS 40 MPH AND GREATER
FRAME 1	FRAME 2	FRAME 1	FRAME 2	
STOPPED TRAFFIC AHEAD	EXPECT DELAYS	SLOW TRAFFIC AHEAD	PREPARE TO STOP	 (FLASHING CAUTION MODE)

TRAFFIC CONTROL, LANE CLOSURE, TRAFFIC QUEUE WARNING SYSTEM

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2025 /S/ Erin Schwark
DATE ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

- LEGEND**
- TYPE III BARRICADE
 - TYPE III BARRICADE WITH ATTACHED SIGN
 - TRAFFIC CONTROL DRUM
 - SIGN ON PERMANENT SUPPORT
 - TYPE "A" WARNING LIGHT (FLASHING)
 - DIRECTION OF TRAFFIC

GENERAL NOTES

THIS RAMP CLOSURE DETAIL IS TYPICAL FOR CLOSING A RIGHT SIDE EXIT RAMP. FOR A LEFT SIDE EXIT RAMP, REVERSE THE TRAFFIC CONTROL.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS, TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

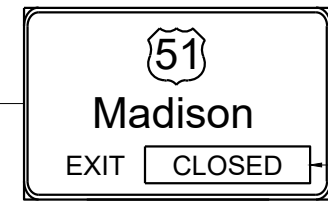
PLACE TEMPORARY PAVEMENT MARKING LINE IF RAMP CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WORK AREAS WITH A DROP OFF ALONG THE EDGE OF AN OPEN TRAVEL LANE SHALL BE LEVELED WITH TEMPORARY FILL WHEN THE CONTRACTOR IS NOT WORKING ADJACENT TO THE TRAVEL LANE. DRUMS SHALL BE PLACED ENTIRELY OUTSIDE THE TRAVEL LANE, ALLOWING THE FULL UNOBSTRUCTED LANE WIDTH, WHEN WORK IS NOT IN PROGRESS.

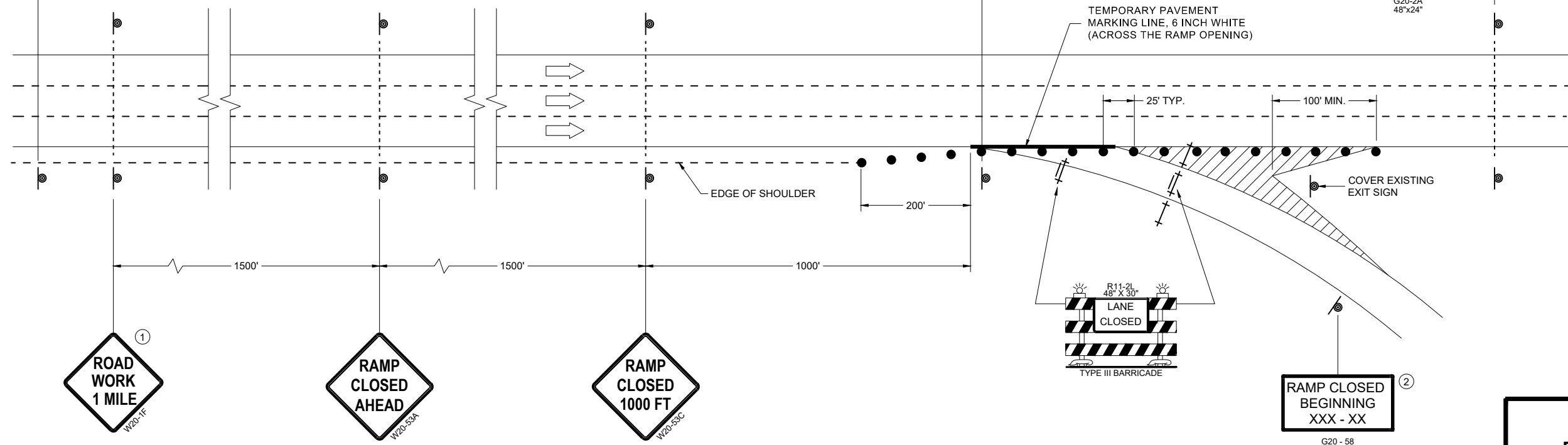
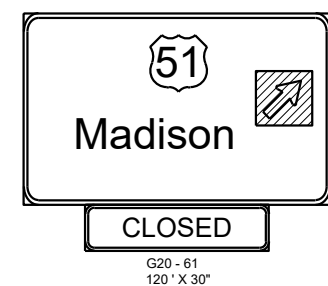
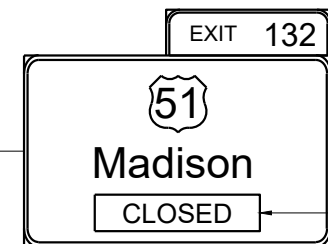
WHERE MEDIAN BARRIER IS IN PLACE, SIGNS SHOWN ON LEFT SIDE OF ROADWAY MAY BE OMITTED FOR RIGHT SIDE RAMP CLOSURES OF LESS THAN 12 - HOUR DURATION.

① W20 - 1A AND G20 - 2A SIGNS ARE NOT REQUIRED IF THE RAMP CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

② PLACE "RAMP CLOSED BEGINNING" SIGN OR PCMS 7 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR SIGN LAYOUT.



OR



RAMP CLOSED BEGINNING XXX - XX	
G20 - 58 OR PCMS MESSAGING	
FRAME 1	FRAME 2
RAMP TO CLOSE	XXXDAY XX XX XX

**TRAFFIC CONTROL,
EXIT RAMP CLOSURE**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2025 /S/ Andrew Heidtke
DATE DATE <position>

FHWA












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SDD 15D16-07

SDD 15D16-07

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  REMOVING PAVEMENT MARKINGS
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  CONNECTED ARROW BOARD
-  WZ START LOCATION MARKER
-  WZ END LOCATION MARKER

GENERAL NOTES

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE

IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

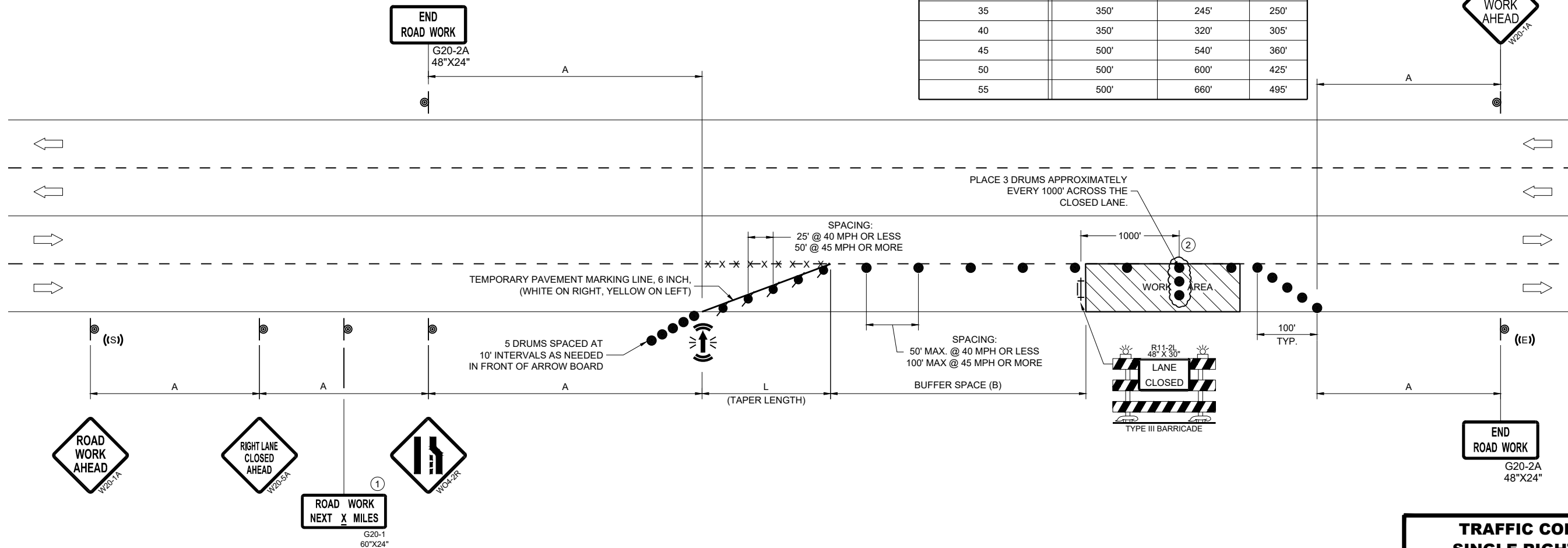
WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

- ① OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.
- ② DRUMS IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	155'
30	200'	180'	200'
35	350'	245'	250'
40	350'	320'	305'
45	500'	540'	360'
50	500'	600'	425'
55	500'	660'	495'

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SDD 15D20-11b

SDD 15D20-11b








**TRAFFIC CONTROL,
SINGLE RIGHT LANE
CLOSURE, UNDIVIDED
NON-FREEWAY/EXPRESSWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2025 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  SIGN ON PERMANENT SUPPORT
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  DIRECTION OF TRAFFIC
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

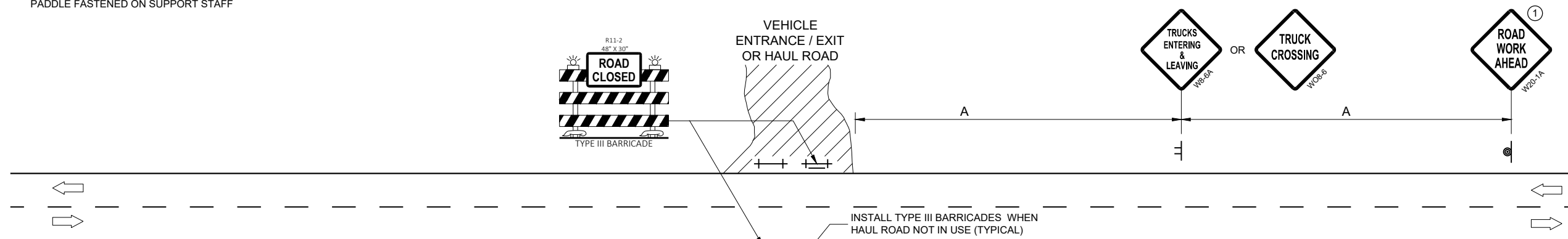
POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET
0-30	200'
35-40	350'
45-55	500'

GENERAL NOTES

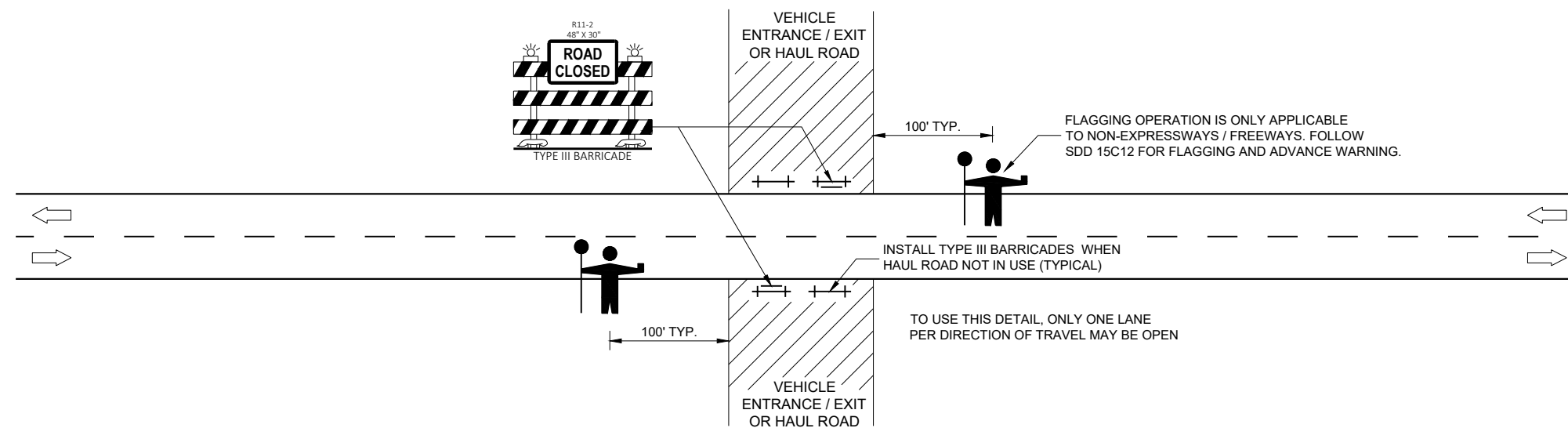
- ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.
- "WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.
- THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- WARNING SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- WHEN ACTIVITY REFLECTED BY THE SIGN IS NOT CURRENTLY TAKING PLACE, THE HIGHWAY SHALL BE RESTORED TO NORMAL CONDITION AND THE SIGNS SHALL BE REMOVED, COVERED OR TURNED AWAY FROM TRAFFIC.
- WHEN A SIDE ROAD OR RAMP INTERSECTS WITHIN THE ADVANCE SIGNING AREA, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND / OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.
- PLACE SIGNS ON BOTH SIDES IF USED ON DIVIDED HIGHWAY.
- ① THESE SIGNS ARE TO BE USED ONLY WHEN VEHICLE ENTRANCE / EXIT CONDITIONS ARE SEPARATED BY MORE THAN TWO MILES FROM PREVIOUS WORK AREA OR SIGNING OR AS DIRECTED BY THE ENGINEER.

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THIS ABOVE DETAIL TO BE USED WHEN CONSTRUCTION VEHICLE TRAFFIC YIELDS TO THE FREE FLOW OF MAINLINE OR RAMP TRAFFIC.









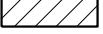
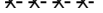

THIS DETAIL TO BE USED WHEN CONSTRUCTION WORK INCLUDING TRUCKING ACTIVITY REQUIRES MAINLINE TRAFFIC TO BE TEMPORARILY STOPPED IN ONE OR BOTH DIRECTIONS. DELAY TO HIGHWAY TRAFFIC SHALL BE MINIMIZED.

TRAFFIC CONTROL, VEHICLE ENTRANCE/EXIT OR HAUL ROAD	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2020 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

SDD 15D29 - 06

SDD 15D29 - 06

LEGEND

-  TYPE III BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE "A" WARNING LIGHT (FLASHING)
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
-  CONCRETE BARRIER TEMPORARY PRECAST

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR LANE SHIFT RIGHT - REVERSE FOR SHIFTING LEFT.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON ANY "COVERED" OR "DOWNED" SIGNS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINES IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

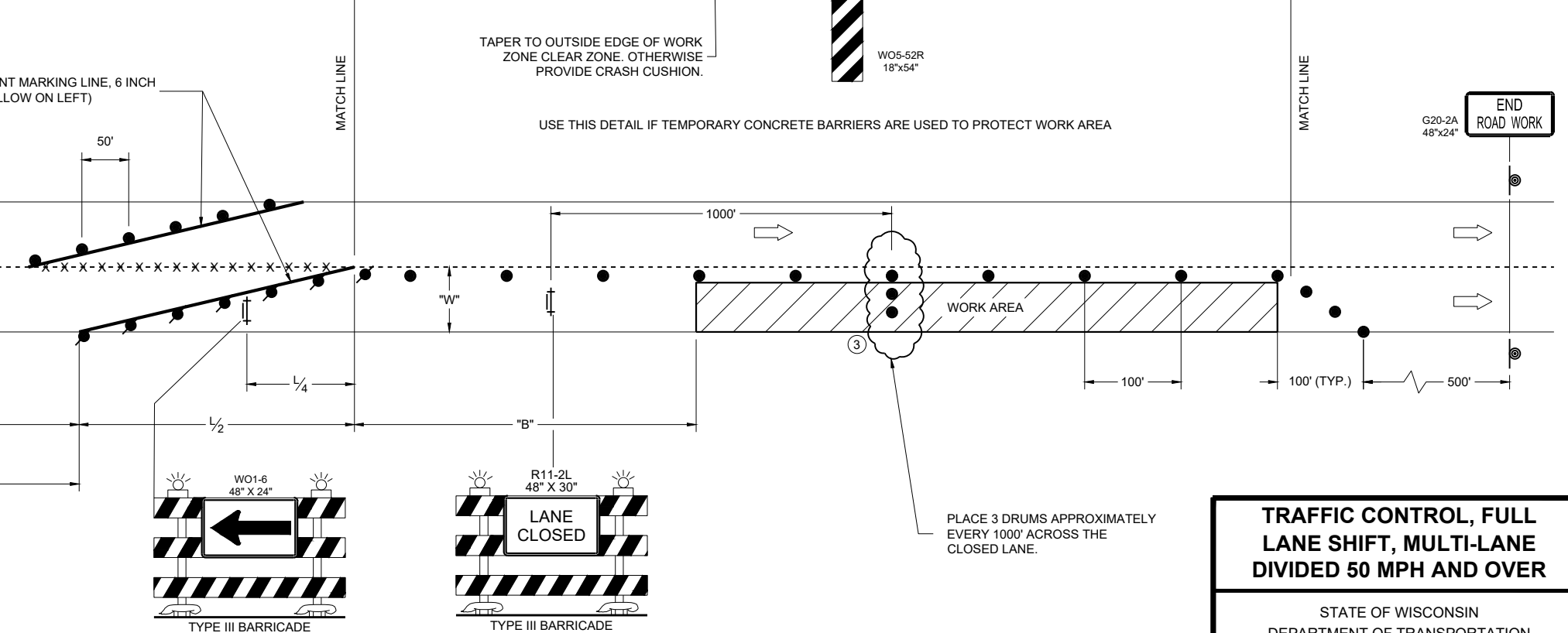
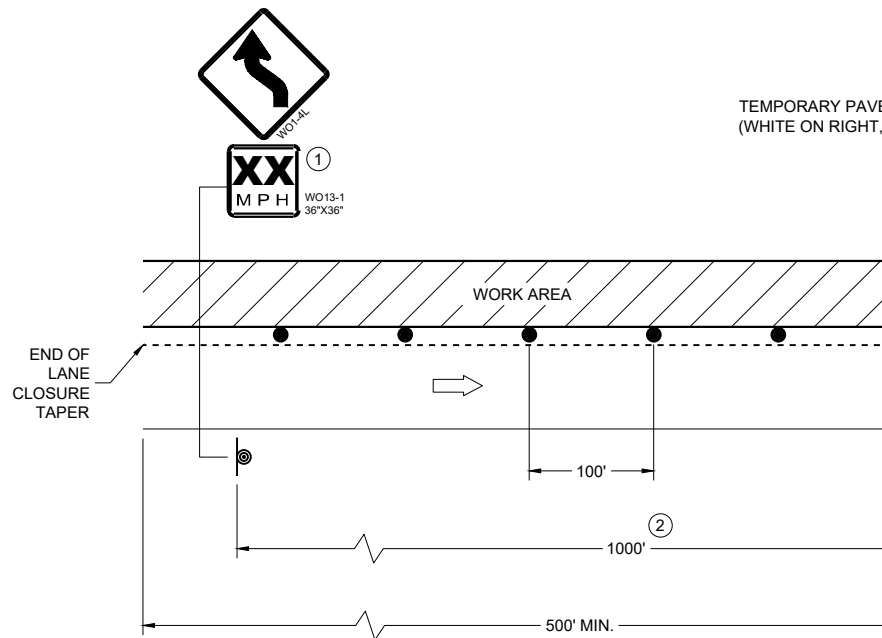
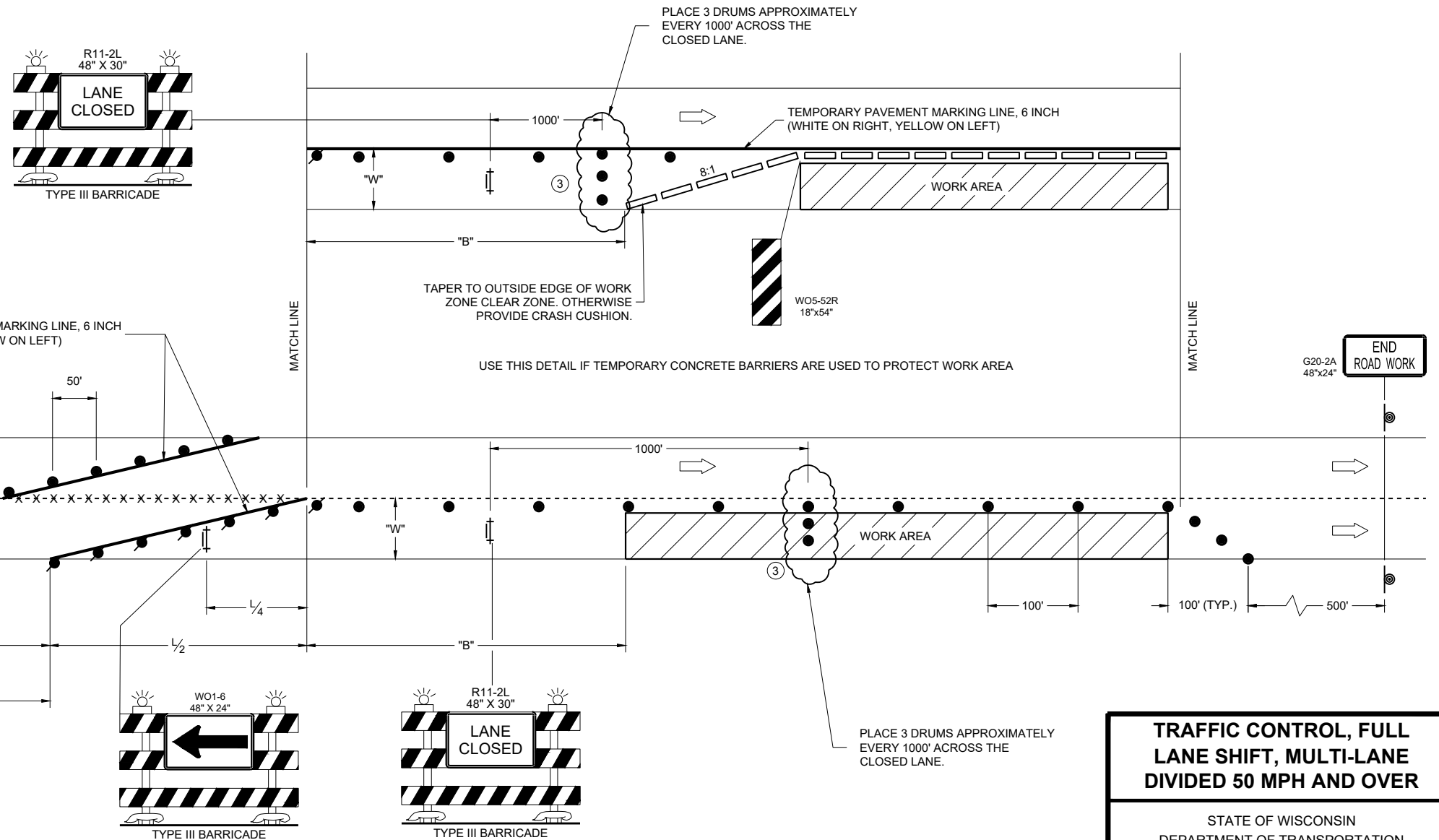
WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE SHIFT OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE SHIFT MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE THE LENGTH OF 1/2 THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

- ① USE ONLY IF DESIGN SPEED IS 10 MPH BELOW POSTED SPEED.
- ② IF BEGINNING OF LANE SHIFT IS 1200' OF LESS FROM THE END OF THE LANE CLOSURE TAPER, PLACE THE WO1-4L SIGN 200 FEET AFTER THE END OF THE LANE CLOSURE TAPER.
- ③ DRUMS IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	SHIFTING TAPER 1/2					BUFFER SPACE (B) FEET
	W, LATERAL OFFSET (FT)	10	11	12	13	
50	250	275	300	325	350	425
55	275	303	330	358	385	495
60	300	330	360	390	420	570
65	325	358	390	423	455	645
70	350	385	420	455	490	730



TRAFFIC CONTROL, FULL LANE SHIFT, MULTI-LANE DIVIDED 50 MPH AND OVER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2025 DATE /S/ Andrew Heidtke WORK ZONE ENGINEER

FHWA









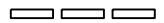
6

6

SDD 15D40-07b

SDD 15D40-07b

LEGEND

-  TYPE III BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE "A" WARNING LIGHT (FLASHING)
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
-  CONCRETE BARRIER TEMPORARY PRECAST

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR LANE SHIFT LEFT - REVERSE FOR SHIFTING RIGHT.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON ANY "COVERED" OR "DOWNED" SIGNS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINES IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

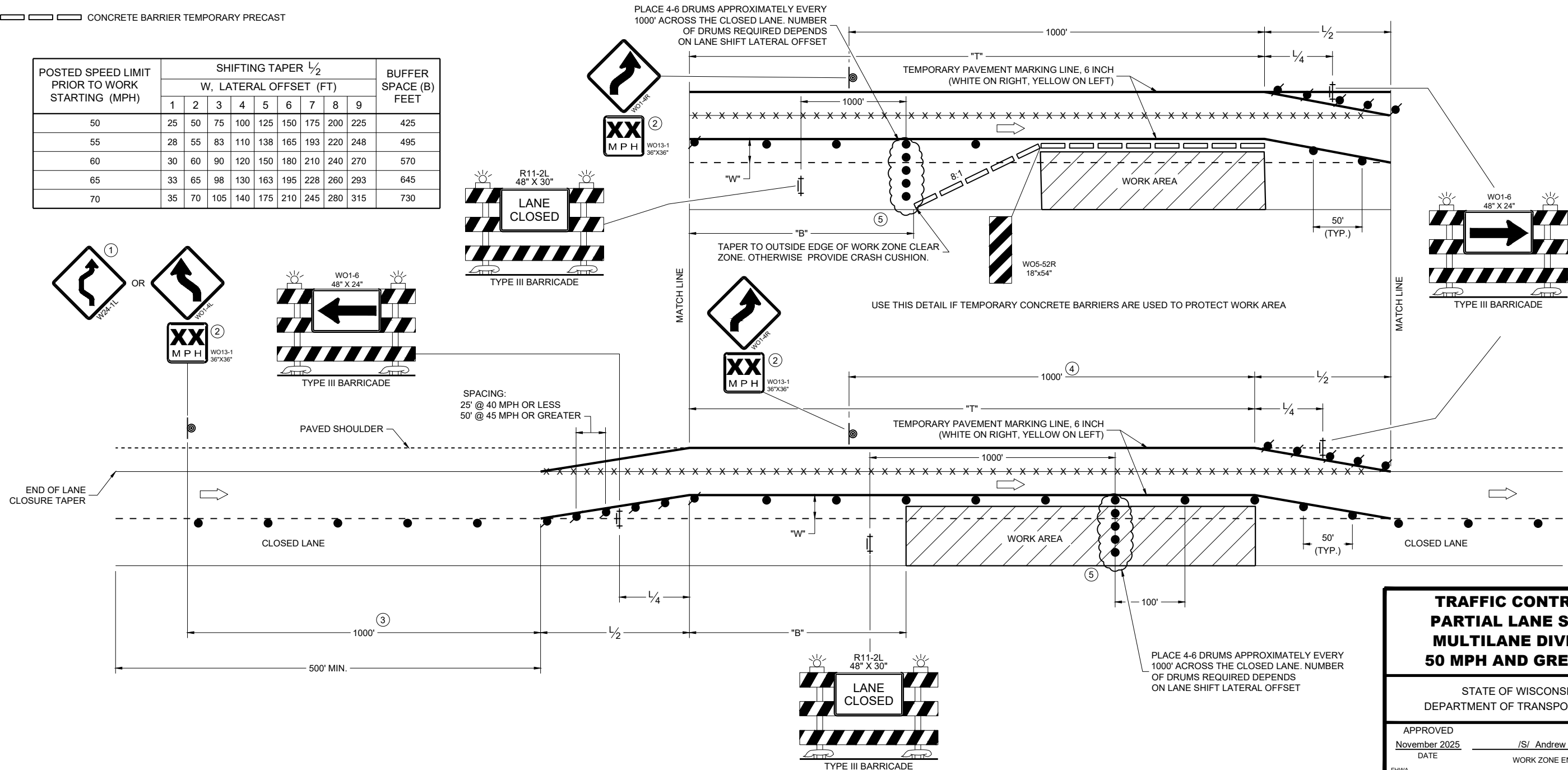
WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE SHIFT OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE SHIFT MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

- ① USE ONLY WHEN T<600', OMIT WO1-4R.
- ② IF NEEDED, USE ONLY IF DESIGN SPEED IS 10 MPH BELOW POSTED SPEED.
- ③ IF THE BEGINNING OF LANE SHIFT TAPER IS 1200 FEET OR LESS FROM END OF LANE CLOSURE TAPER, PLACE THE WO1-4L SIGN 200 FEET AFTER THE END OF THE LANE CLOSURE TAPER.
- ④ IF THE BEGINNING OF THE SECOND LANE SHIFT TAPER IS 1200 FEET OR LESS FROM END OF THE FIRST LANE CLOSURE TAPER, PLACE THE WO1-4L SIGN 200 FEET AFTER THE END OF THE FIRST LANE CLOSURE TAPER.
- ⑤ DRUMS IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	SHIFTING TAPER 1/2									BUFFER SPACE (B) FEET
	W, LATERAL OFFSET (FT)									
	1	2	3	4	5	6	7	8	9	
50	25	50	75	100	125	150	175	200	225	425
55	28	55	83	110	138	165	193	220	248	495
60	30	60	90	120	150	180	210	240	270	570
65	33	65	98	130	163	195	228	260	293	645
70	35	70	105	140	175	210	245	280	315	730



**TRAFFIC CONTROL,
PARTIAL LANE SHIFT
MULTILANE DIVIDED
50 MPH AND GREATER**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2025 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

6

6

SDD 15D40-07d

SDD 15D40-07d

LEGEND








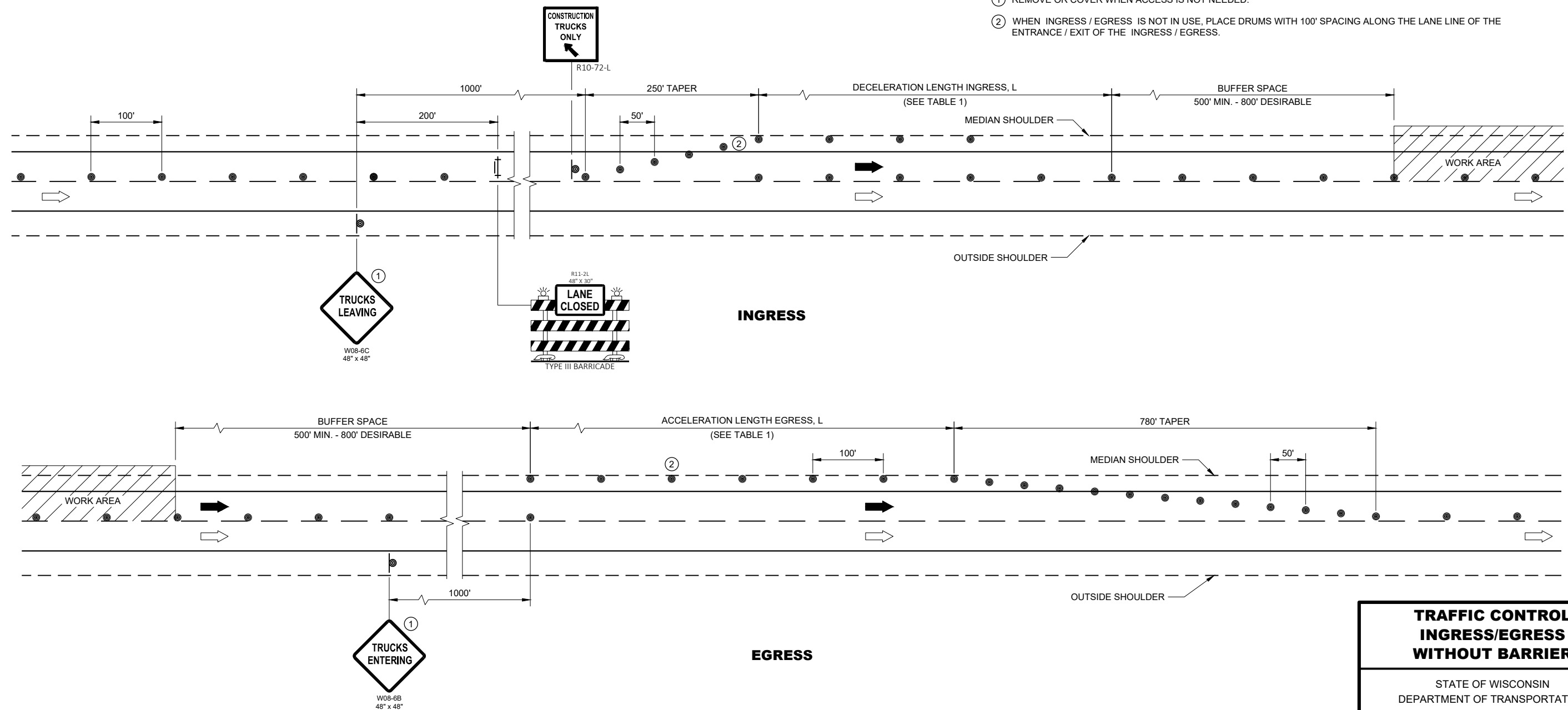
-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  DIRECTION OF TRAFFIC
-  CONSTRUCTION TRAFFIC
-  WORK AREA

TABLE 1

S (MPH)	INGRESS, L	EGRESS, L
50	435'	720'
55	480'	960'
60	530'	1200'
65	570'	1410'
70	615'	1620'

LEGEND

- ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.
- "WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.
- ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.
- THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- WORK ZONE INGRESS/EGRESS LOCATIONS SHALL BE APPROVED BY THE ENGINEER. LOCATIONS FOR WORK ZONE ACCESS TO/FROM THE FREEWAY SHALL NOT BE USED FOR INGRESS AND EGRESS AT THE SAME TIME.
- THIS ACCESS DETAIL IS TYPICAL FOR LEFT LANE ACCESS, FOR RIGHT LANE ACCESS, REVERSE THE TRAFFIC CONTROL
- TEMPORARY SUPPORTS MAY BE USED IF PLACED BEHIND TEMPORARY BARRIER WALL
- TRUCKS SHALL USE FLASHING YELLOW BEACON WHEN ENTERING AND EXITING LIVE TRAFFIC.
- ① REMOVE OR COVER WHEN ACCESS IS NOT NEEDED.
 - ② WHEN INGRESS / EGRESS IS NOT IN USE, PLACE DRUMS WITH 100' SPACING ALONG THE LANE LINE OF THE ENTRANCE / EXIT OF THE INGRESS / EGRESS.



6








6

SDD 15D47-03b

SDD 15D47-03b

TRAFFIC CONTROL INGRESS/EGRESS WITHOUT BARRIER	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  REMOVING PAVEMENT MARKINGS
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  CONNECTED ARROW BOARD
-  WZ START LOCATION MARKER
-  WZ END LOCATION MARKER

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS, INCLUDING PCMS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON "COVERED" OR "DOWNED" SIGNS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.

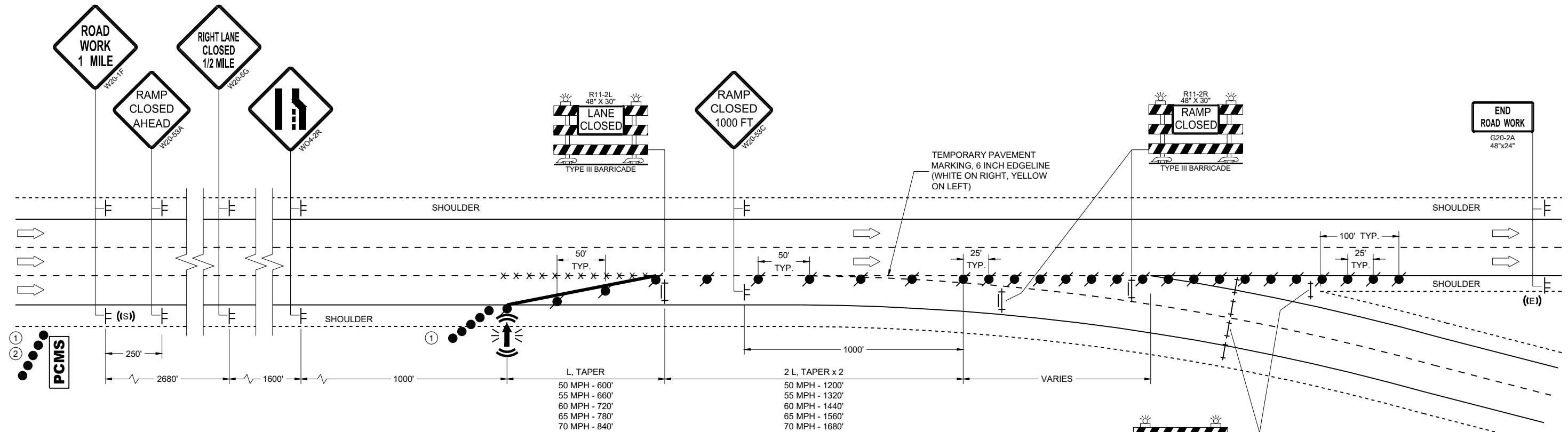
REMOVE ALL ARROW BOARDS AND DEVICES BEYOND THE SHOULDER WHEN THE WORK IS NOT IN PROGRESS.

REFER TO SDD 15C2 FOR SETTING DETOUR SIGNAGE.

ADD FREEWAY SHIELD AND DIRECTION IF APPLICABLE. COVER EXISTING RAMP AND LANE SIGNS IF APPLICABLE.

① 5 DRUMS SPACED AT 10 FOOT INTERVALS.

② DURING THE PRE-WARNING PERIOD, PLACE THE PCMS AT THE SYSTEM RAMP THAT IS TO BE CLOSED. DURING CLOSURE PERIOD, PLACE PCMS OR FMS AT LAST AVAILABLE EXIT IN ADVANCE OF SYSTEM RAMP CLOSURE. IF DYNAMIC MESSAGE SIGNS (DMS) ARE AVAILABLE, COORDINATE POTENTIAL SIGN MESSAGES WITH THE TRAFFIC MANAGEMENT CENTER (TMC).



RAMP CLOSED BEGINNING XXX-XX
G20-58

PCMS MESSAGING	
ONE WEEK IN ADVANCE OF CLOSURE:	
FRAME 1	FRAME 2
RAMP TO CLOSE	XXXDAY XX XX XX
DURING CLOSURE:	
FRAME 1	FRAME 2
HWY XX RAMP CLOSED	USE ALT ROUTE

SEE SDD 15C2 (SHEET "b"), DETAIL "D" "BARRICADES AND SIGNS FOR MAINLINE CLOSURES" PLACE BARRICADES TO FULLY SEAL OFF ROAD

**TRAFFIC CONTROL,
SYSTEM RAMP CLOSURE**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2025 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

6

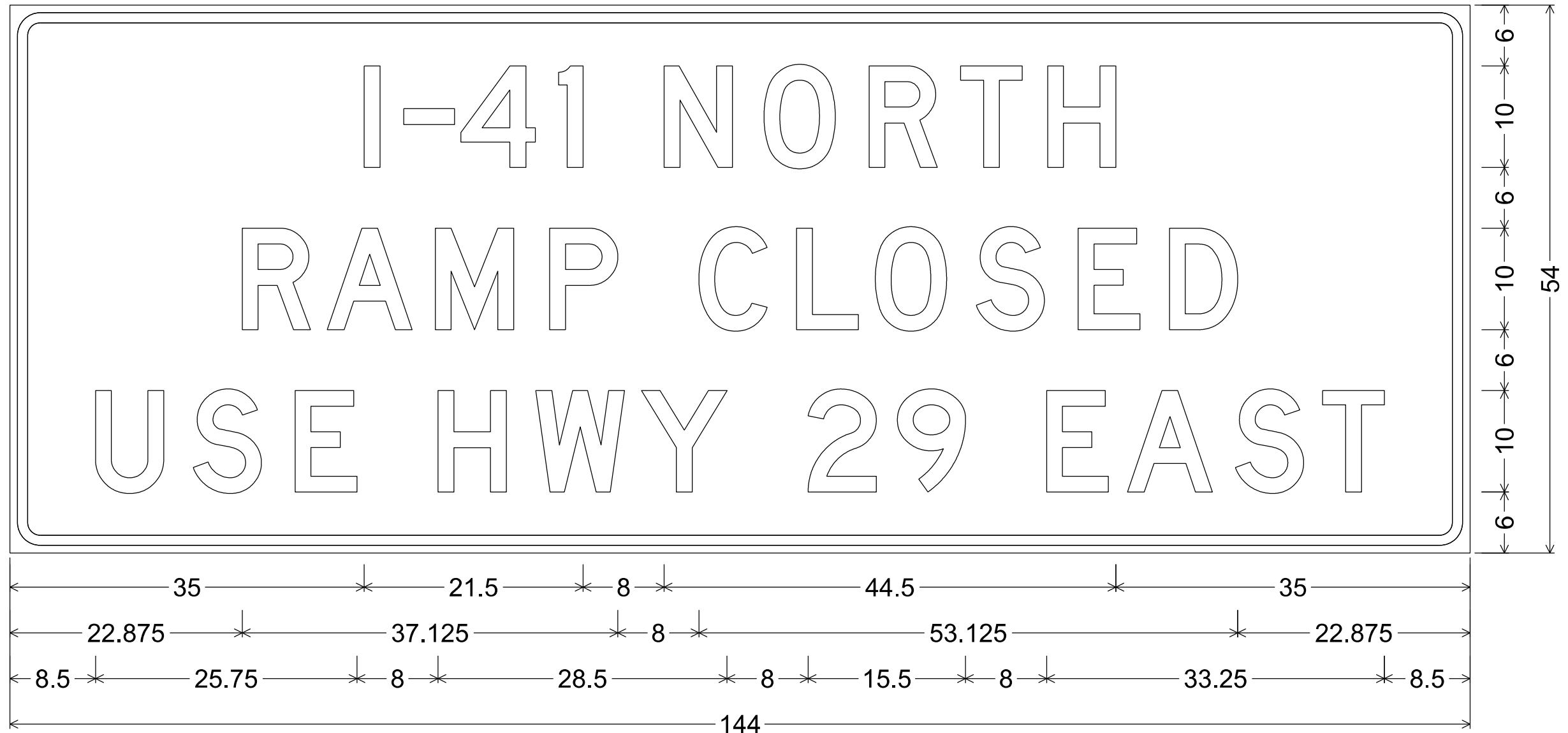
6

SDD 15D49-05

SDD 15D49-05

NOTES

1. Fixed Message Sign Type II - Type F Reflective
2. Color:
 Background - Orange
 Message - Black
3. Message Series - D



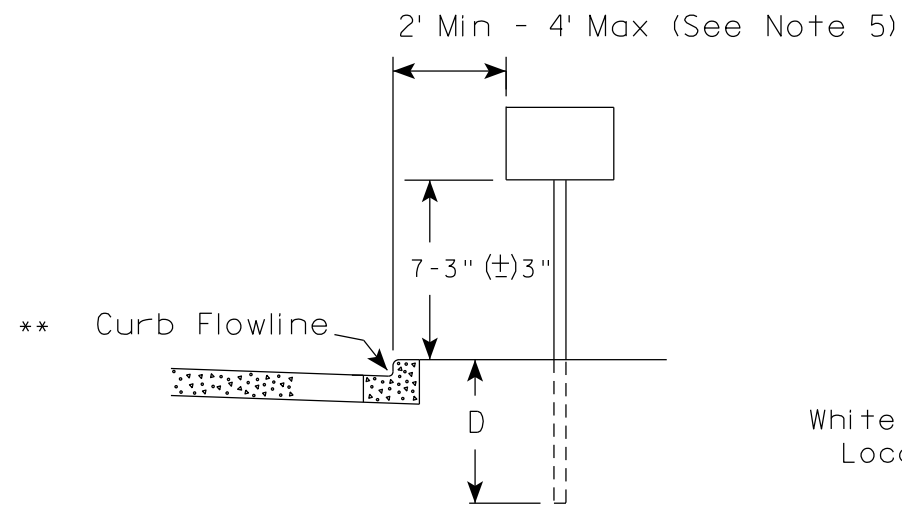
3.000" Radius, 1.000" Border, 0.750" Indent

7

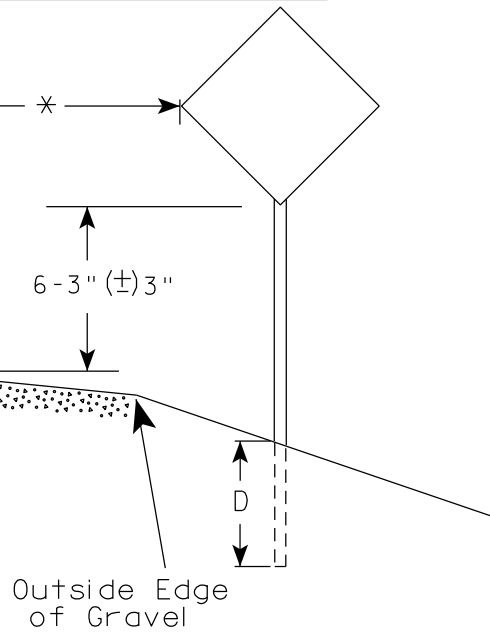
7

URBAN AREA

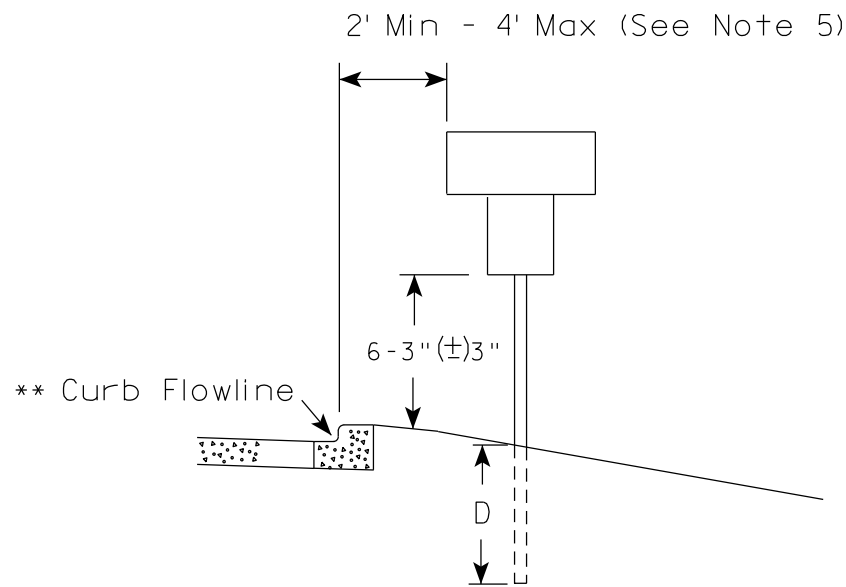
RURAL AREA (See Note 2)



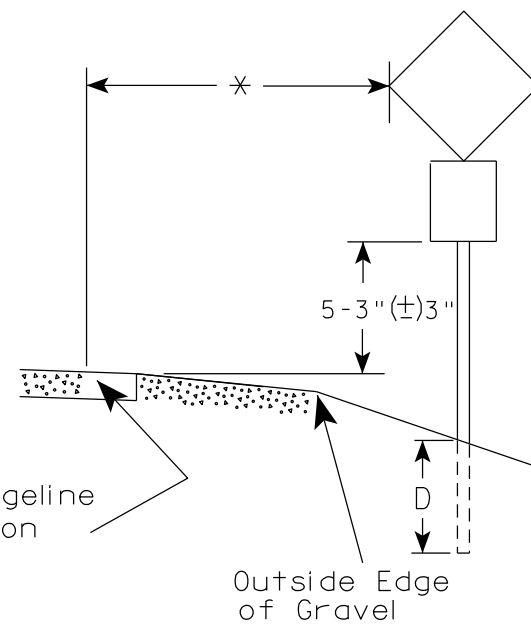
White Edgeline Location



Outside Edge of Gravel



White Edgeline Location



Outside Edge of Gravel

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".
3. For expressways and freeways, mounting height is 7'- 3" (±) 3" or 6'-3" (±) 3" depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±) 3".
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the Engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

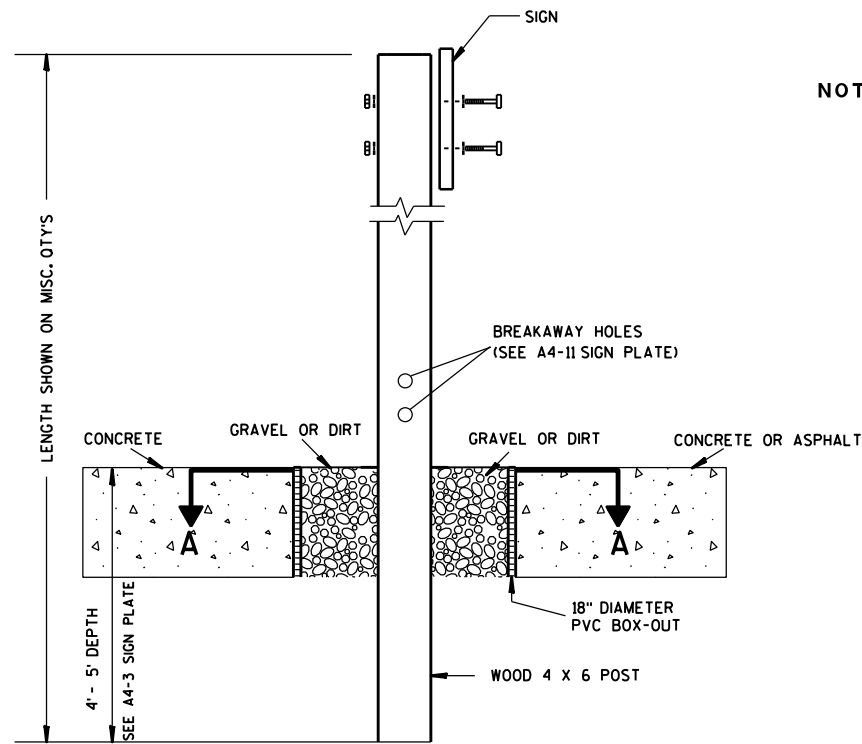
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Raub*
for State Traffic Engineer

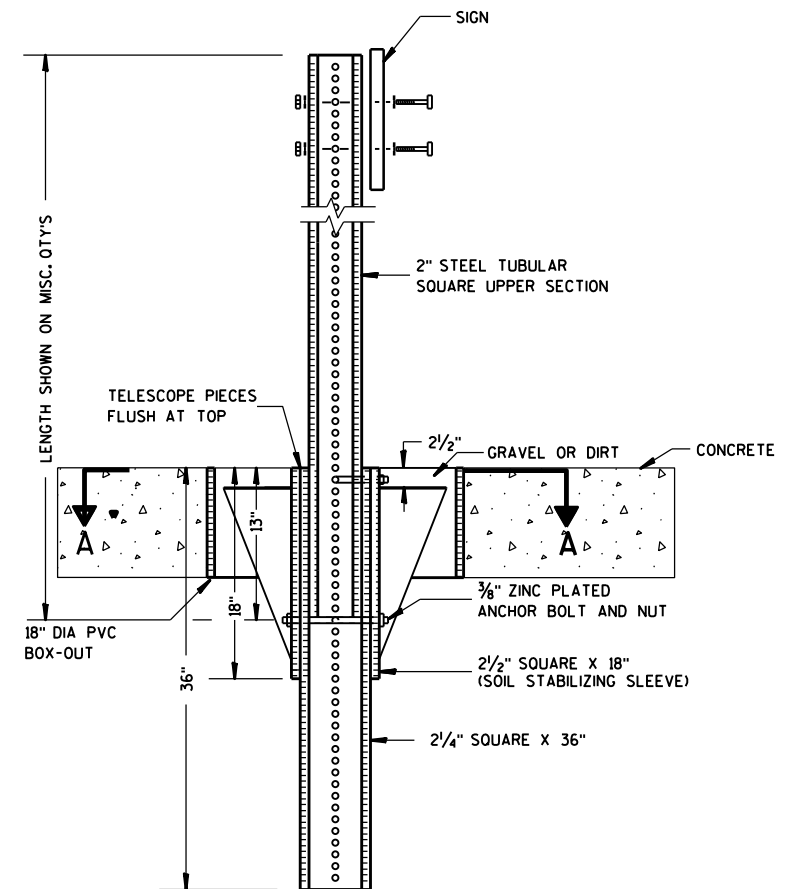
DATE 12/6/23 PLATE NO. A4-3.23



ELEVATION VIEW

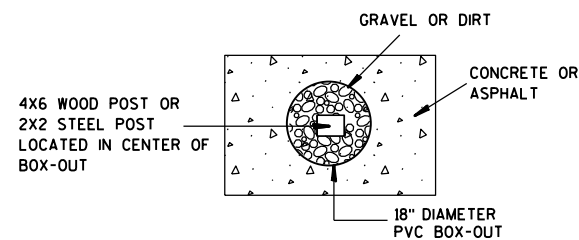
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B	
<small>WISCONSIN DEPT OF TRANSPORTATION</small>	
APPROVED <i>Matthew R. Rauch</i> <small>for State Traffic Engineer</small>	
<small>DATE 1/27/14</small>	<small>PLATE NO. A4-3B.1</small>

GENERAL NOTES

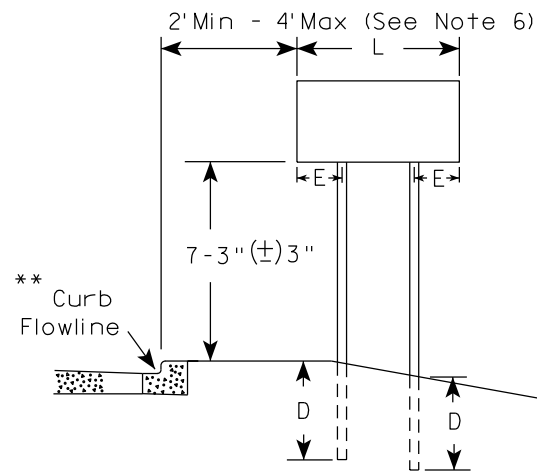
1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) 3" or 6'-3" (±) 3" depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

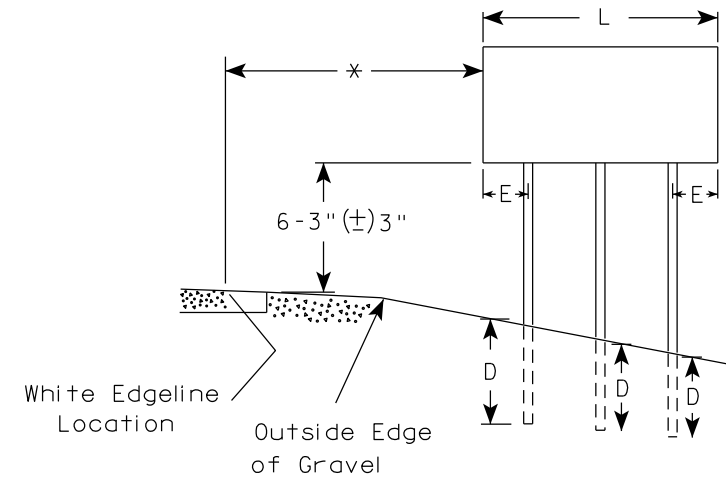
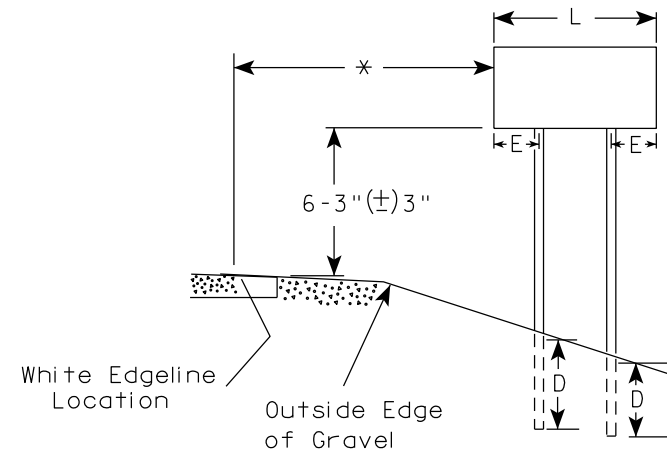
** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

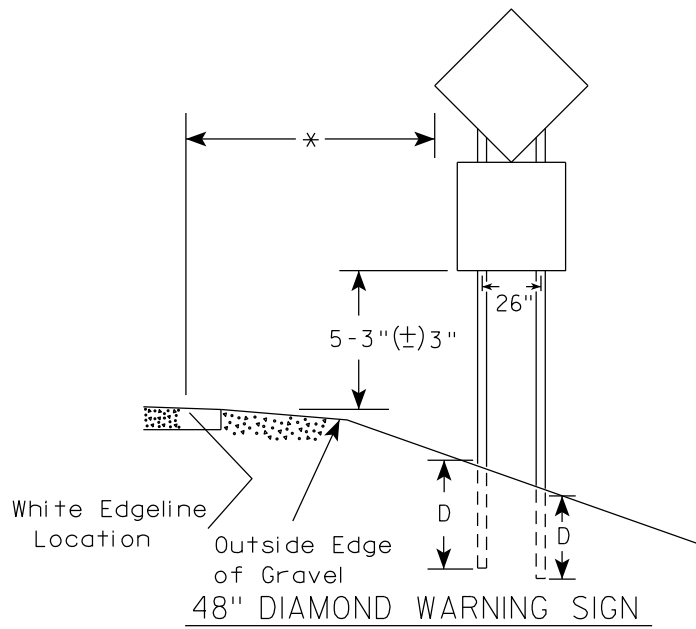
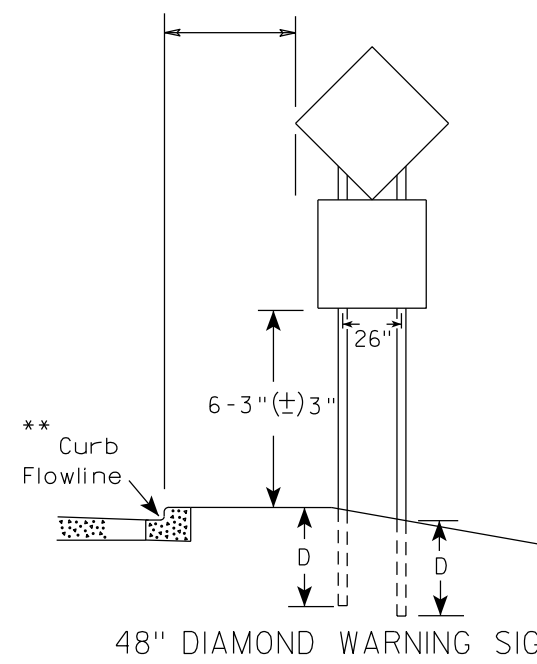
URBAN AREA



RURAL AREA (See Note 3)



URBAN AREA



SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION
 APPROVED *Matthew R Rauch*
 For State Traffic Engineer
 DATE 12/6/23 PLATE NO. A4-4.16

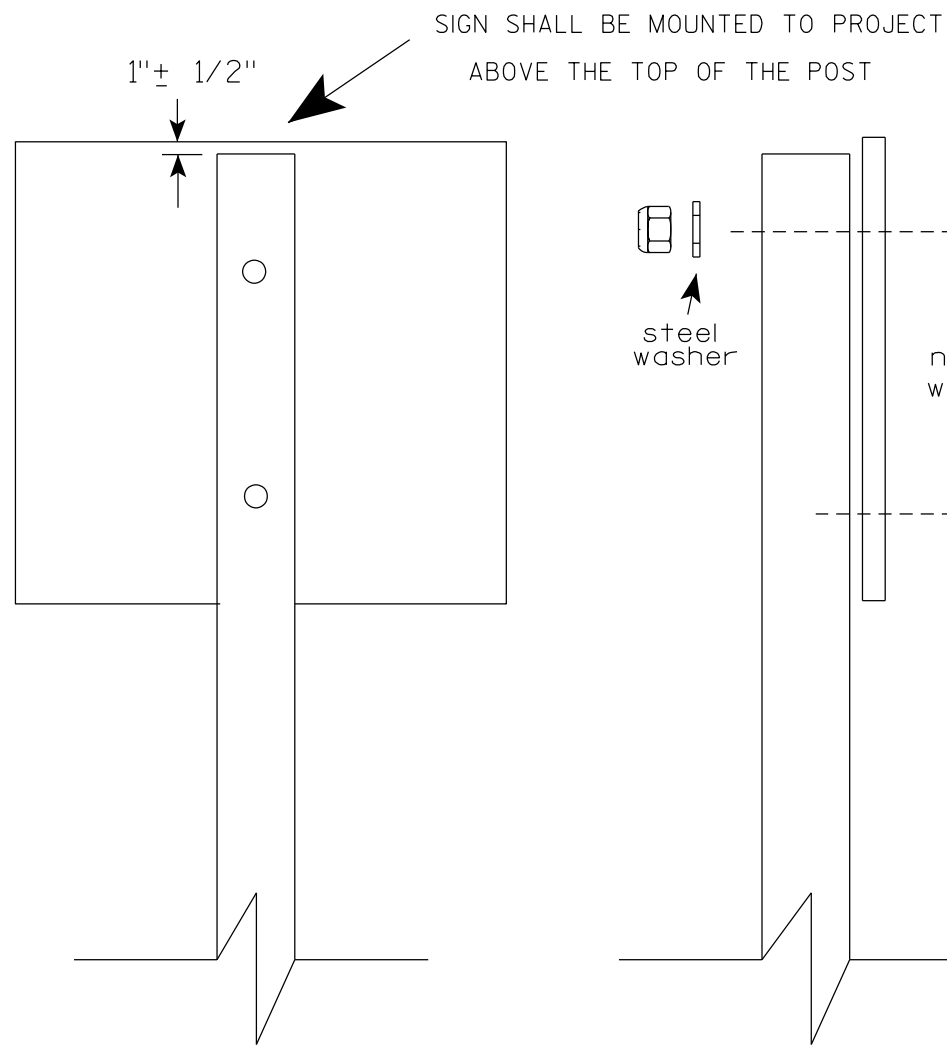
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

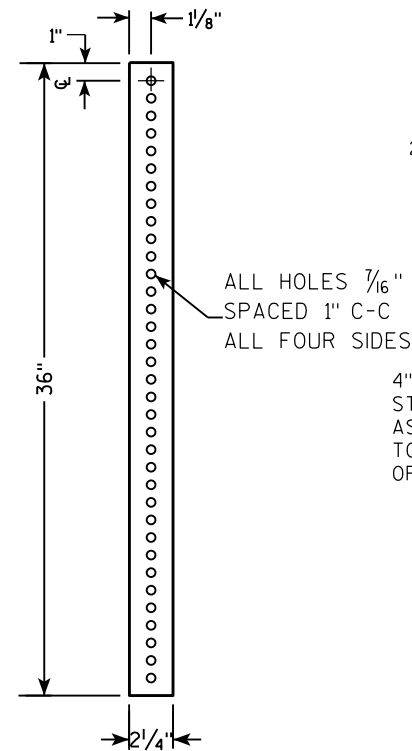
- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
- 1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

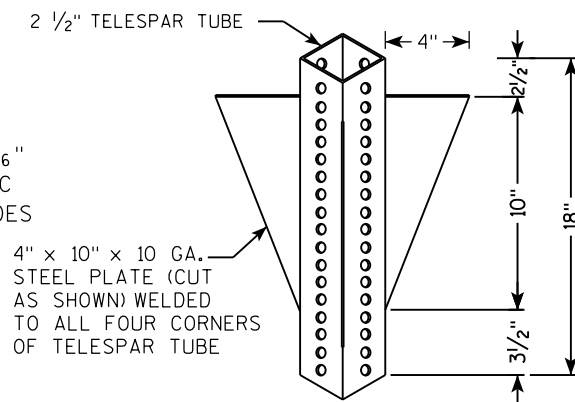
ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R Rauch</i> For State Traffic Engineer
DATE 4/1/2020	PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

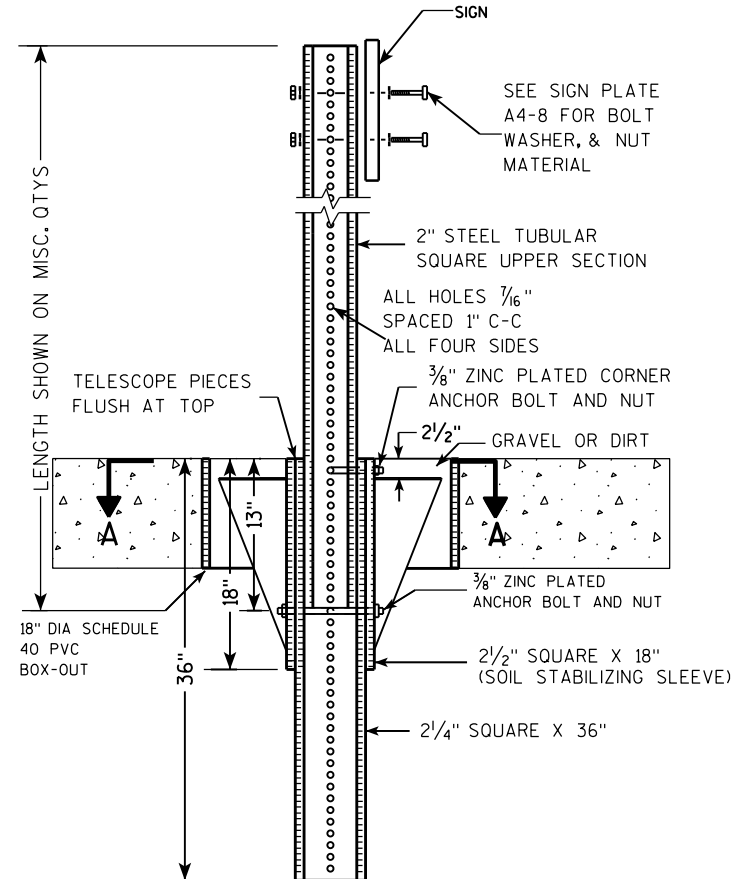
2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



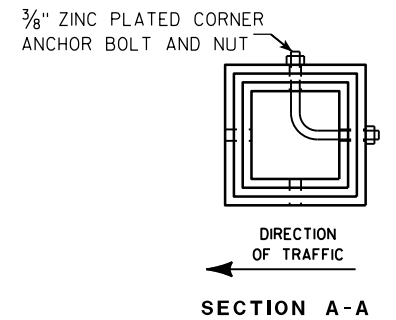
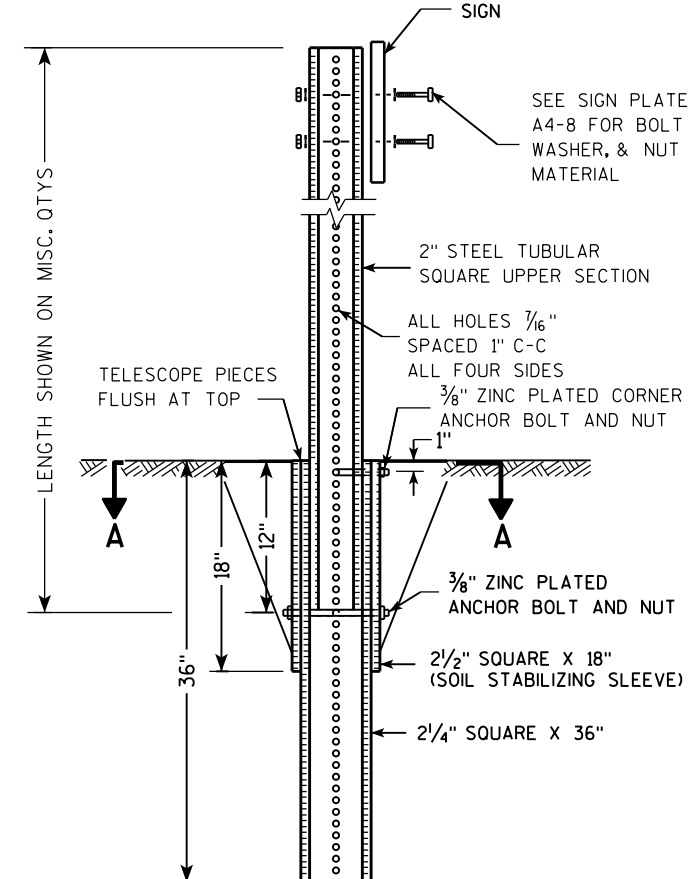
2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

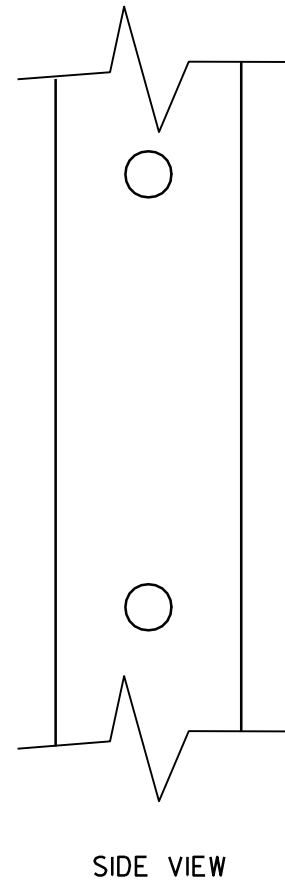
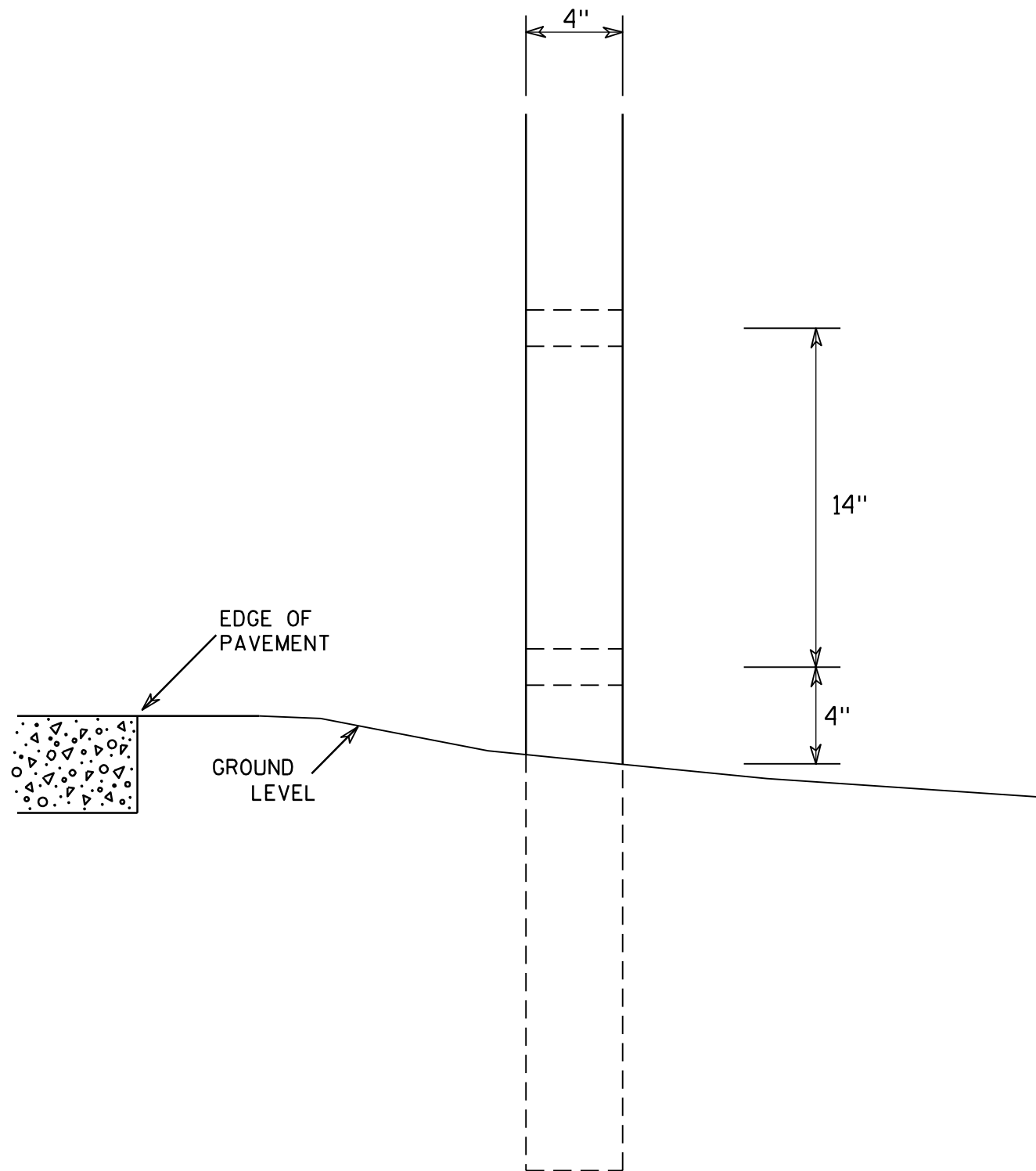
Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9




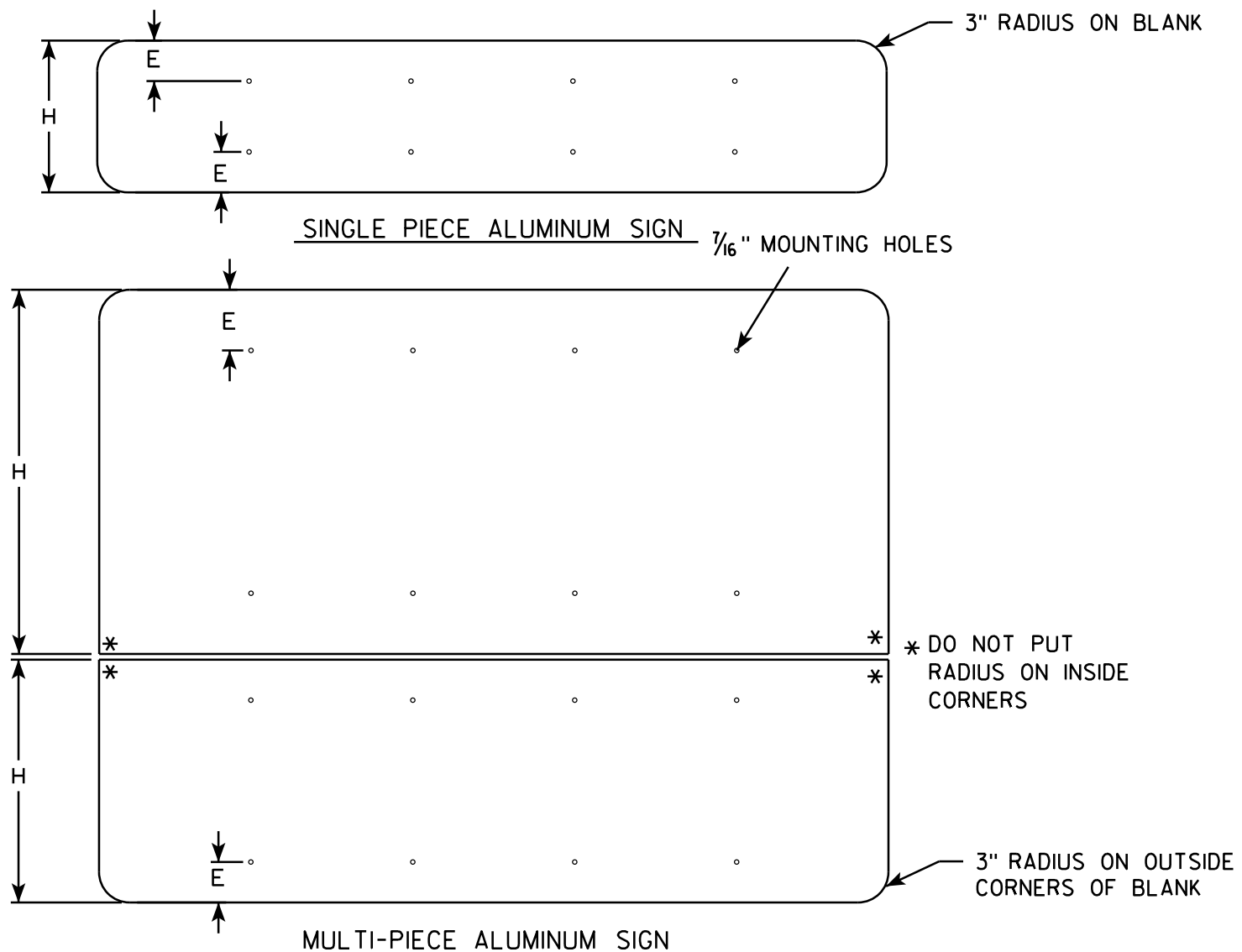
GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

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4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	 <small>for State Traffic Engineer</small>
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>



GENERAL NOTES

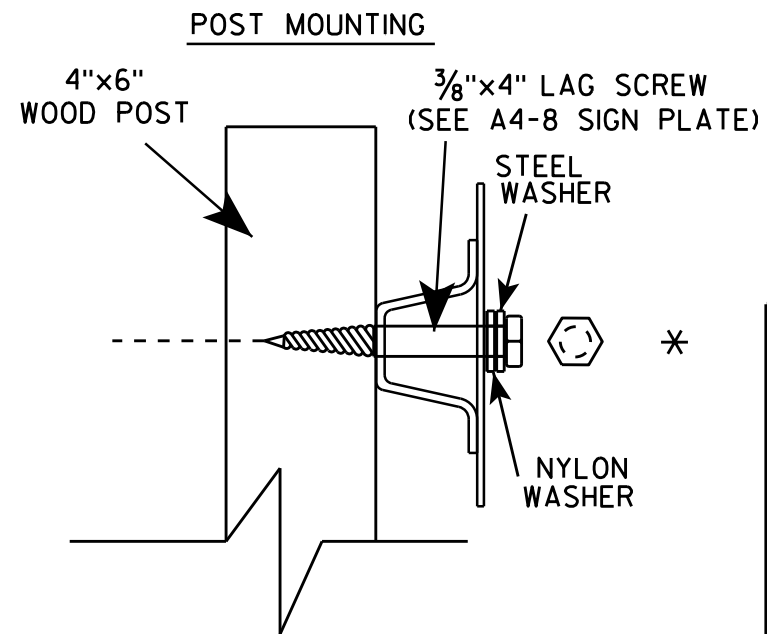
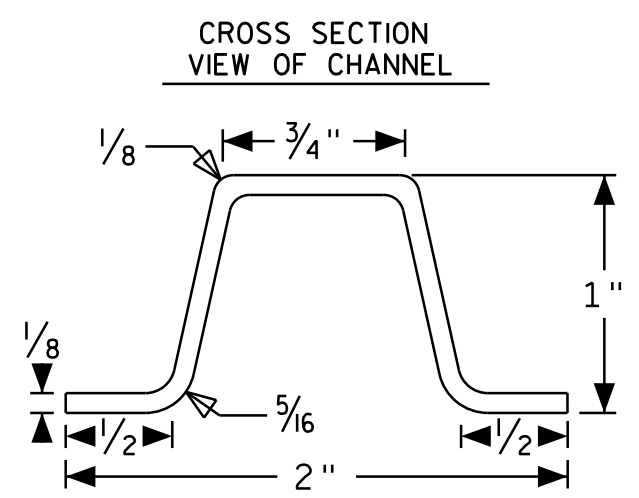
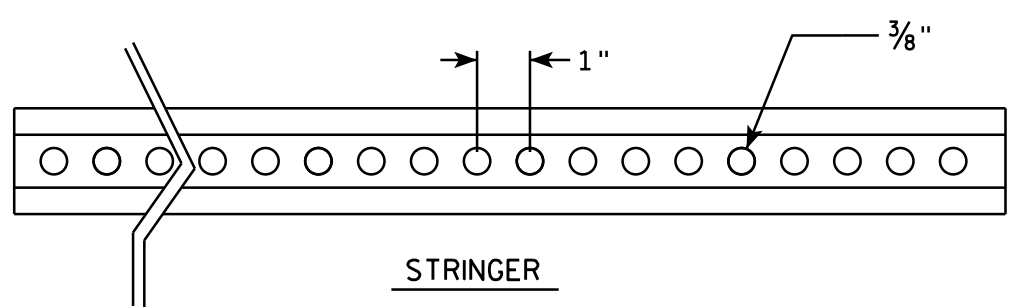
- ALL SIGNS OVER 60" IN WIDTH SHALL HAVE A 3" RADIUS ON THE OUTSIDE CORNERS OF THE ALUMINUM BLANK.
- MOUNTING HOLES SHALL BE $\frac{7}{16}$ " DIAMETER.
- SEE CHART FOR HOLE SPACING REQUIREMENTS
- FOR SIGN PANELS WITH DIMENSION (H) 36" AND OVER, DIMENSION E SHALL BE 6"
- FOR SIGN PANELS WITH DIMENSION (H) UNDER 36", DIMENSION E SHALL BE 4"
- SIGN STRINGER MATERIAL SHALL CONSIST OF STEEL CHANNEL POST SECTIONS, WEIGHING 1.12 LBS/FT IN ACCORDANCE WITH SECTION 633.2.1 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION.
- SEE SIGN PLATE A4-8 FOR SIGN STRINGER BOLTING REQUIREMENTS.

SIGN WIDTH	STRINGER WIDTH	POSTS	HOLE SPACING	MOUNTING HOLES
78"	72"	2	16"	15" 31" 47" 63"
84"	72"	2	17"	16 $\frac{1}{2}$ " 33 $\frac{1}{2}$ " 50 $\frac{1}{2}$ " 67 $\frac{1}{2}$ "
90"	72"	2	18"	18" 36" 54" 72"
96"	90"	2	19"	19 $\frac{1}{2}$ " 38 $\frac{1}{2}$ " 57 $\frac{1}{2}$ " 76 $\frac{1}{2}$ "
102"	90"	2	20"	21" 41" 61" 81"
108"	90"	2	21"	22 $\frac{1}{2}$ " 43 $\frac{1}{2}$ " 64 $\frac{1}{2}$ " 85 $\frac{1}{2}$ "
114"	108"	3	15"	12" 27" 42" 57" 72" 87" 102"
120"	108"	3	16"	12" 28" 44" 60" 76" 92" 108"
126"	108"	3	17"	12" 29" 46" 63" 80" 97" 114"
132"	126"	3	18"	12" 30" 48" 66" 84" 102" 120"
138"	126"	3	19"	12" 31" 50" 69" 88" 107" 126"
144"	126"	3	20"	12" 32" 52" 72" 92" 112" 132"

* DO NOT PUT RADIUS ON INSIDE CORNERS

7

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SIGN STRINGER MOUNTING REQUIREMENTS

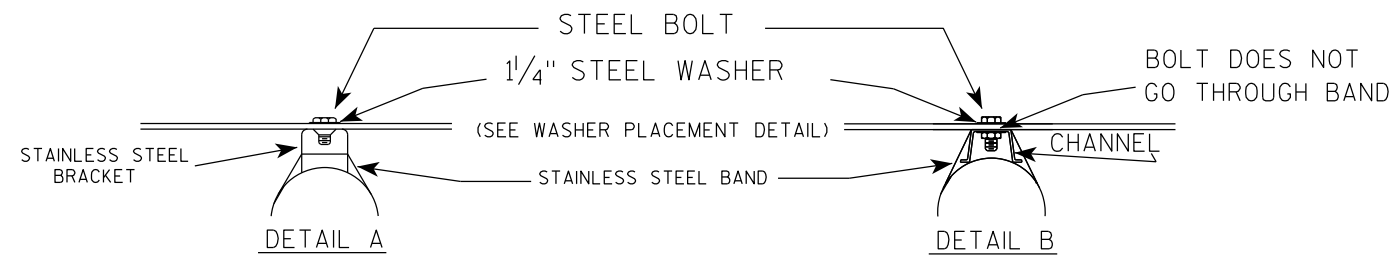
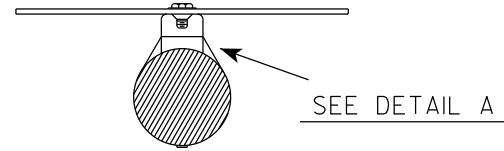
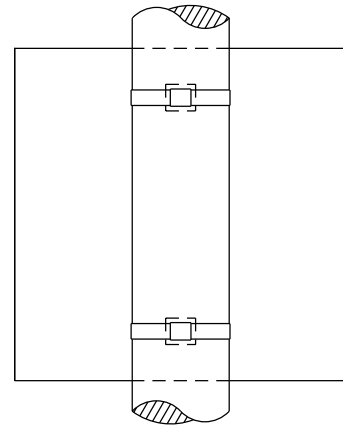
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 4/26/16 PLATE NO. A4-18.1

BANDING

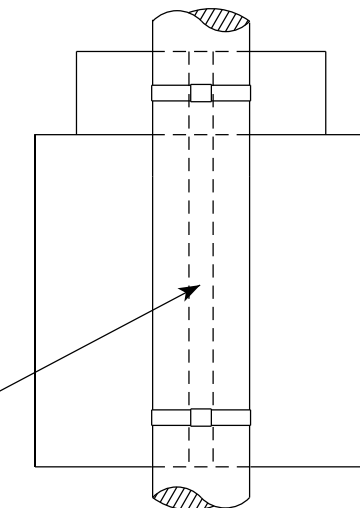
SINGLE SIGN



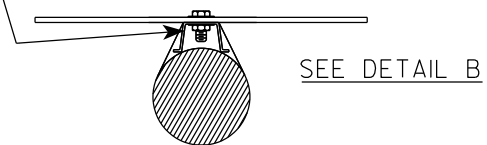
GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

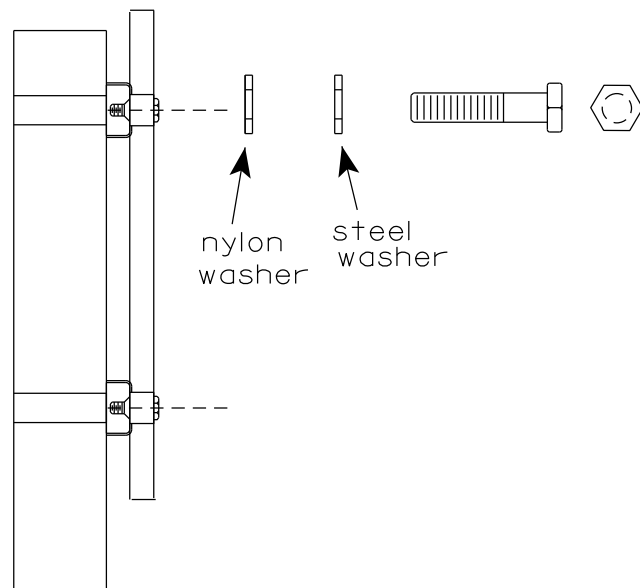
"J" ASSEMBLY



CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET



WASHER PLACEMENT



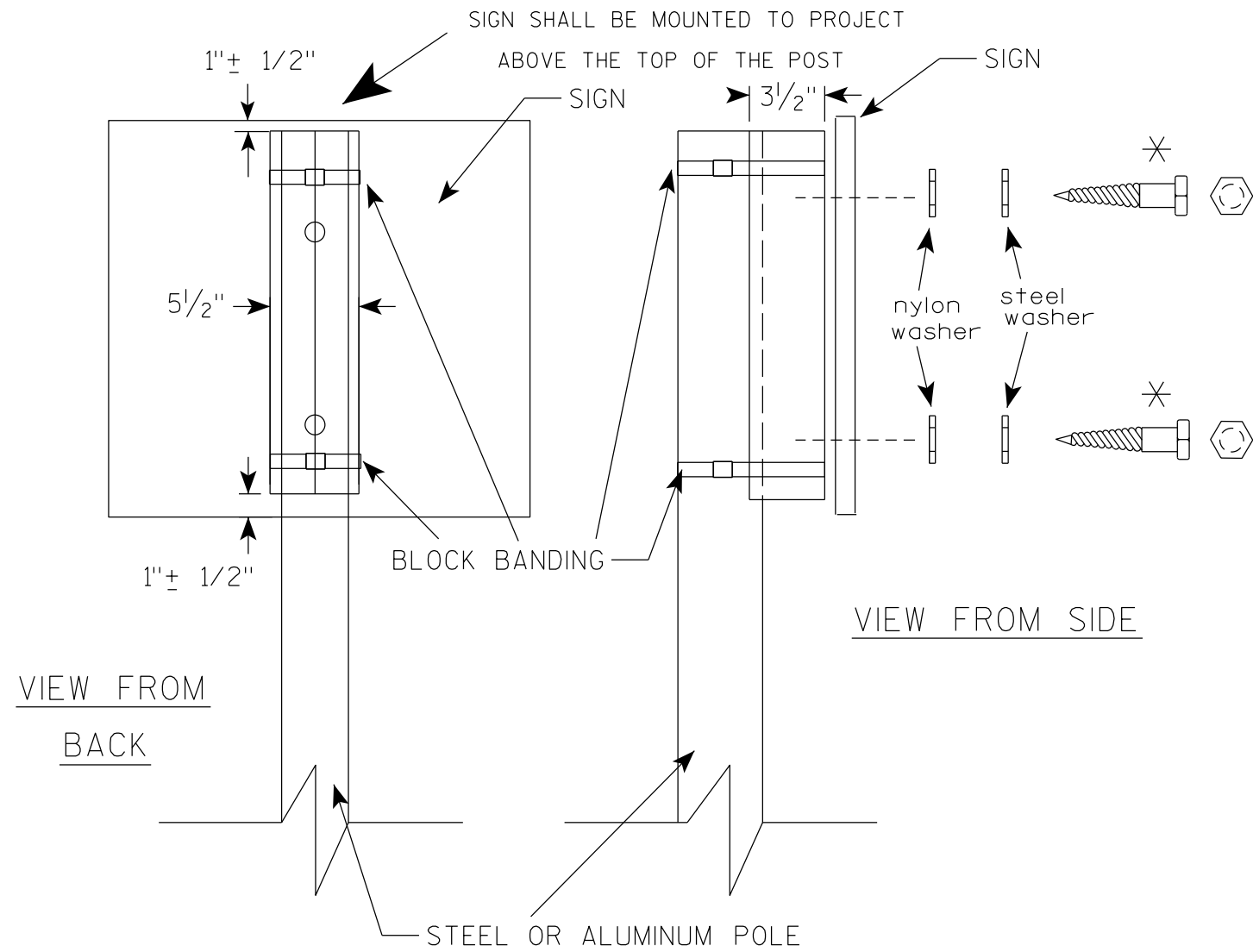
WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

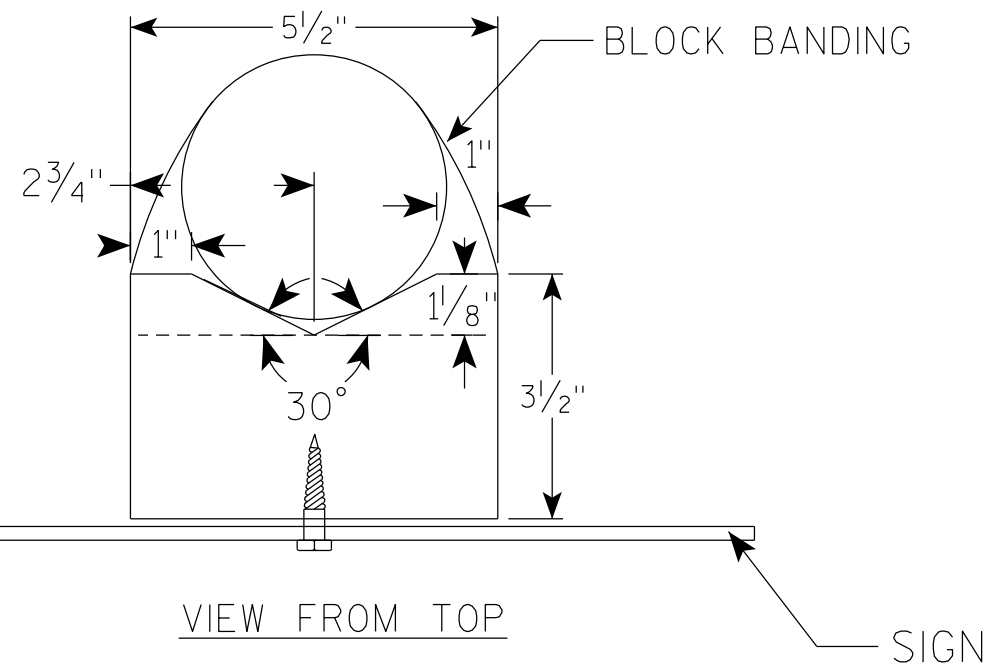
DATE 6/10/19 PLATE NO. A5-9.4



GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

* LAG BOLTS SHALL BE 3/8" X 2 1/2"



BLOCK BANDING DETAIL
(V-BLOCK OPTION)

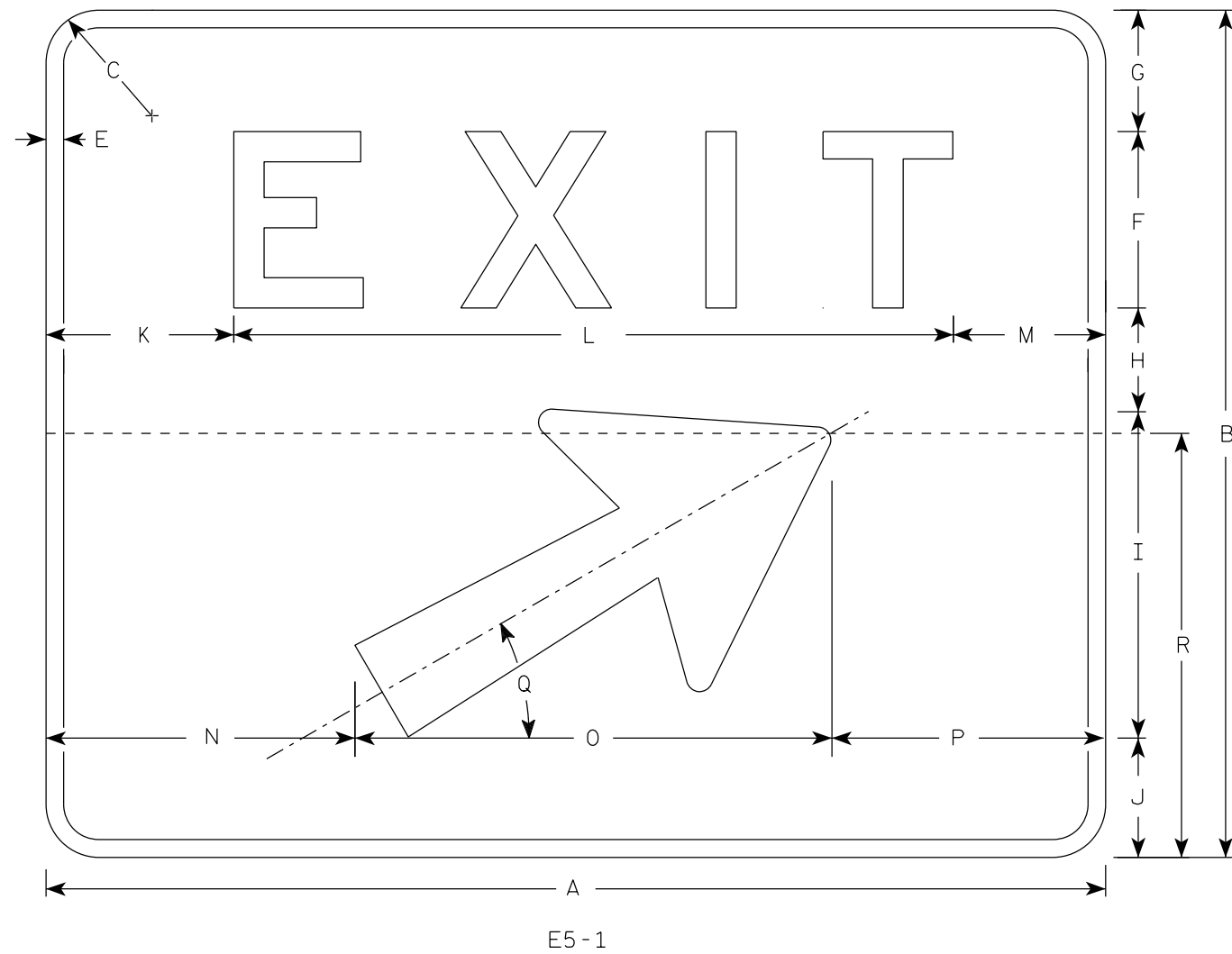
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 4/19/2022 PLATE NO. A5-10.3

NOTES

1. Sign is Type II - Type H reflective
2. Color:
Background - Green
Message - White
3. Message Series - E
4. Sign shall be split into two separate pieces as shown on the detail by the dashed line (-----) for sizes 4 & 5.
5. Arrow is Type "A" from sign plate A1-1.
6. As per the Standard Spec's, this sign shall not have a vertical joint.
7. Size 3 E5-1 shall only be used in a Work Zone application with a Temporary Sign Support



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S																											
2M																											
3	48	48	3		1	10	6 7/8	5 7/8	18 1/2	6 3/4	8 1/2	31 1/8	8 3/8	11 1/2	27	9 1/2	30°	30									16.0
4	60	48	3		1	10	6 7/8	5 7/8	18 1/2	6 3/4	10 5/8	40 3/4	8 5/8	17 1/2	27	15 1/2	30°	30									20.0
5	72	60	3		1	12	9 3/4	10	18 1/2	9 3/4	13 1/4	48 1/2	10 1/4	23 1/2	27	21 1/2	30°	30									30.0

STANDARD SIGN
E5-1

WISCONSIN DEPT OF TRANSPORTATION

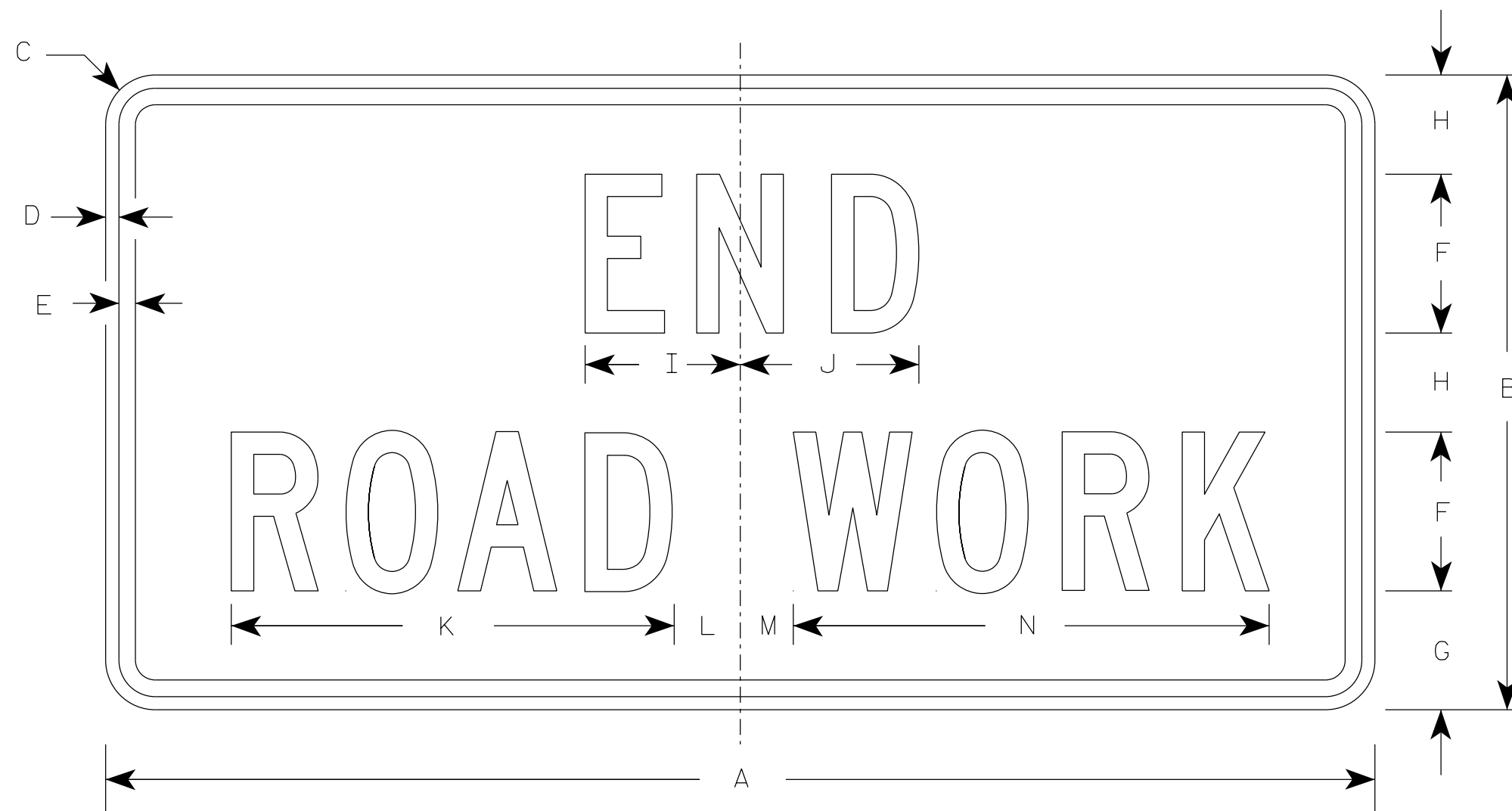
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 1/26/2023 PLATE NO. E5-1.12

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



G20-2A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/2	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5
2	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0
2M	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0
3	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0
4	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0
5	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0

STANDARD SIGN
G20-2A

WISCONSIN DEPT OF TRANSPORTATION

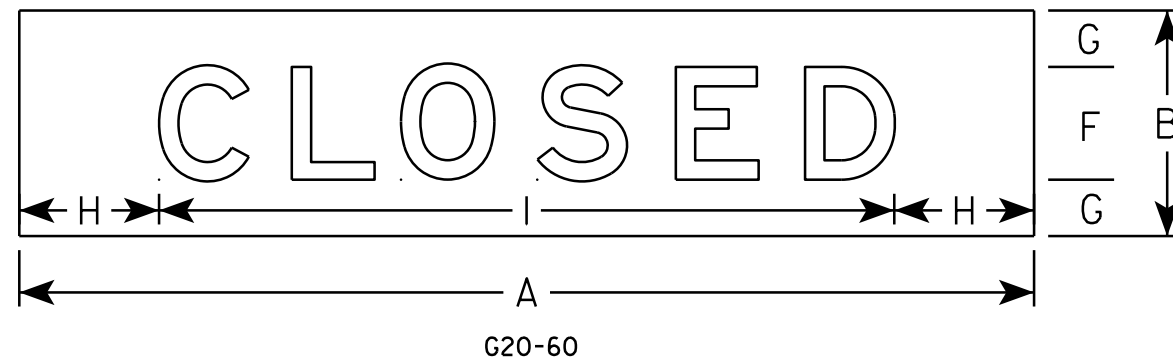
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 1/26/2023 PLATE NO. G20-2A.10

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II- Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - E
4. Material shall be .040 aluminum



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2																											
3																											
4	108	24				12	6	14 7/8	78 1/4																		18.0
5																											

STANDARD SIGN
G20-60

WISCONSIN DEPT OF TRANSPORTATION

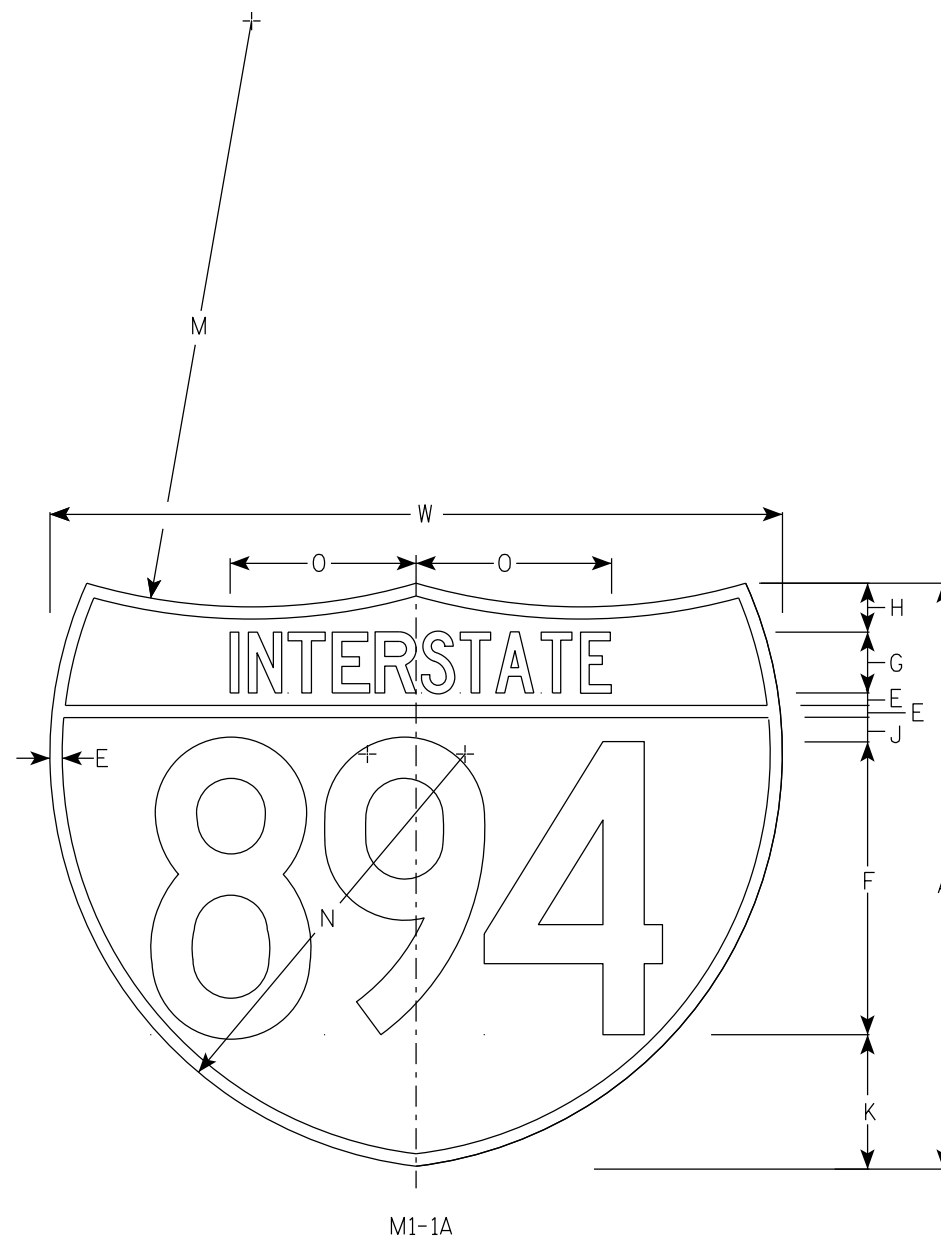
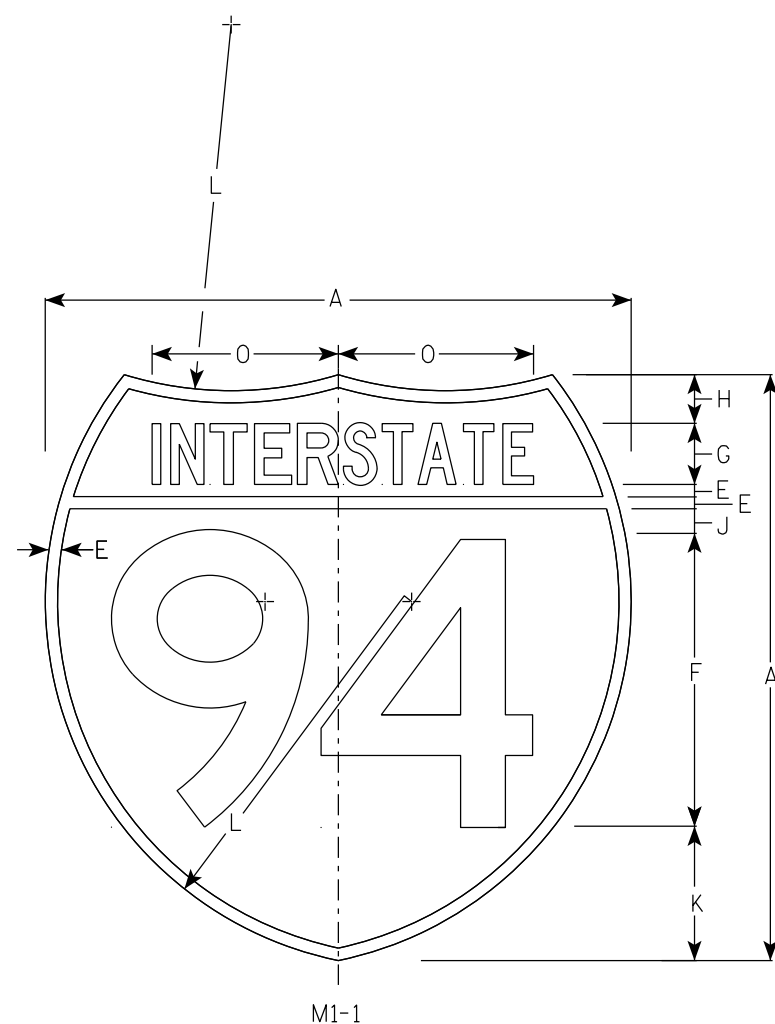
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/13/15 PLATE NO. G20-60.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - Top Red - Bottom Blue
Message - White
3. Message Series - See note 5
4. Substitute appropriate numerals & adjust spacing as per plate A10-1.
5. M1-1 - Numerals - D
Interstate - C
M1-1A - All copy - C



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	M1-1 Area sq. ft.	M1-1A Area sq. ft.
1																											
2	24				1/2	12	2 1/2	2		1	5 1/2	15	24	17	7 7/8								30			3.13	3.91
2M	24				1/2	12	2 1/2	2		1	5 1/2	15	24	17	7 7/8								30			3.13	3.91
3	36				3/4	18	3 3/4	3		1 1/2	8 1/4	22 1/2	36	25 1/2	11 3/4								45			7.03	8.79
4	36				3/4	18	3 3/4	3		1 1/2	8 1/4	22 1/2	36	25 1/2	11 3/4								45			7.03	8.79
5	36				3/4	18	3 3/4	3		1 1/2	8 1/4	22 1/2	36	25 1/2	11 3/4								45			7.03	8.79

INTERSTATE ROUTE MARKER
M1-1 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 11/7/2022 PLATE NO. M1-1.9

PROJECT NO:

HWY:

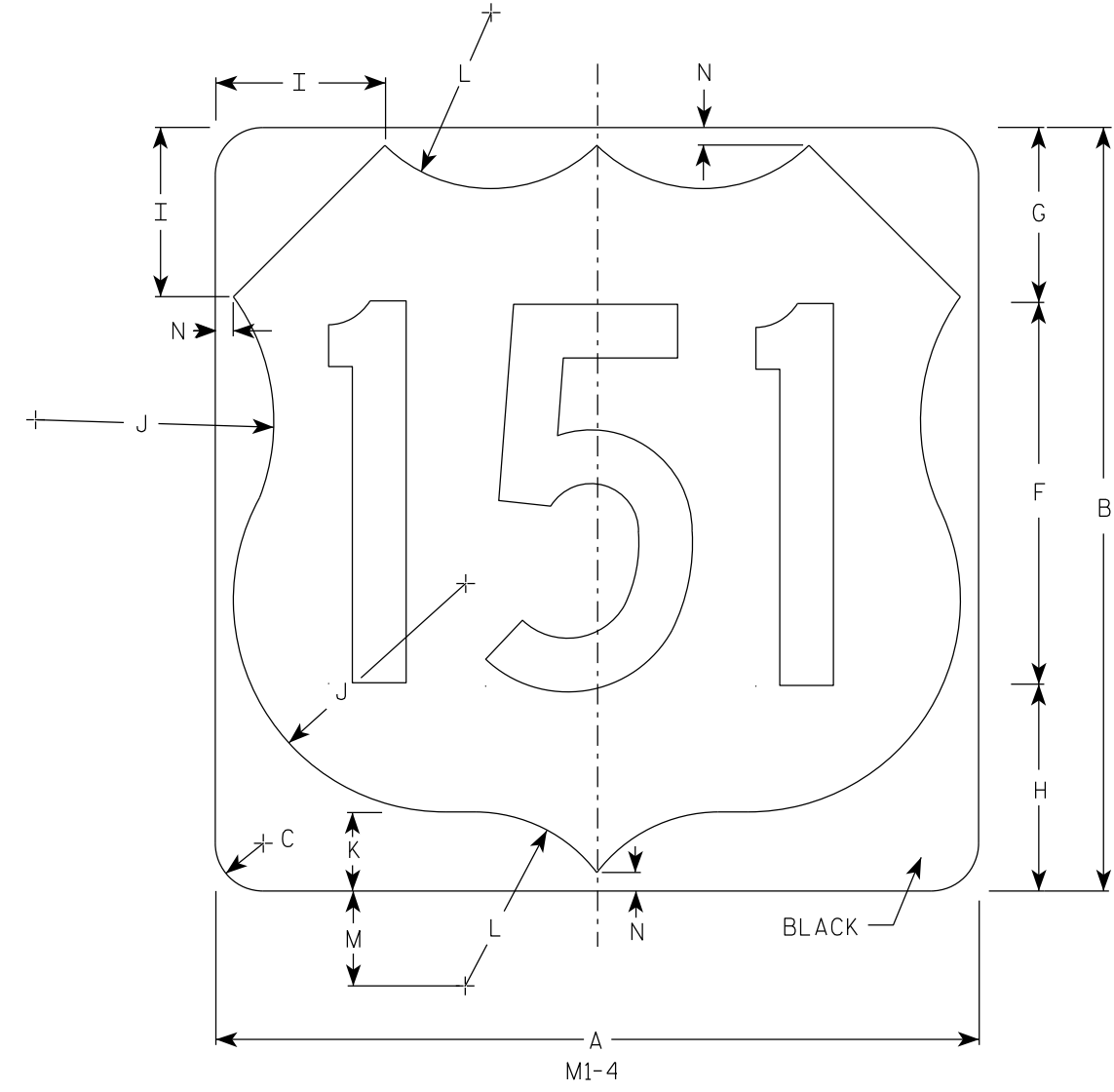
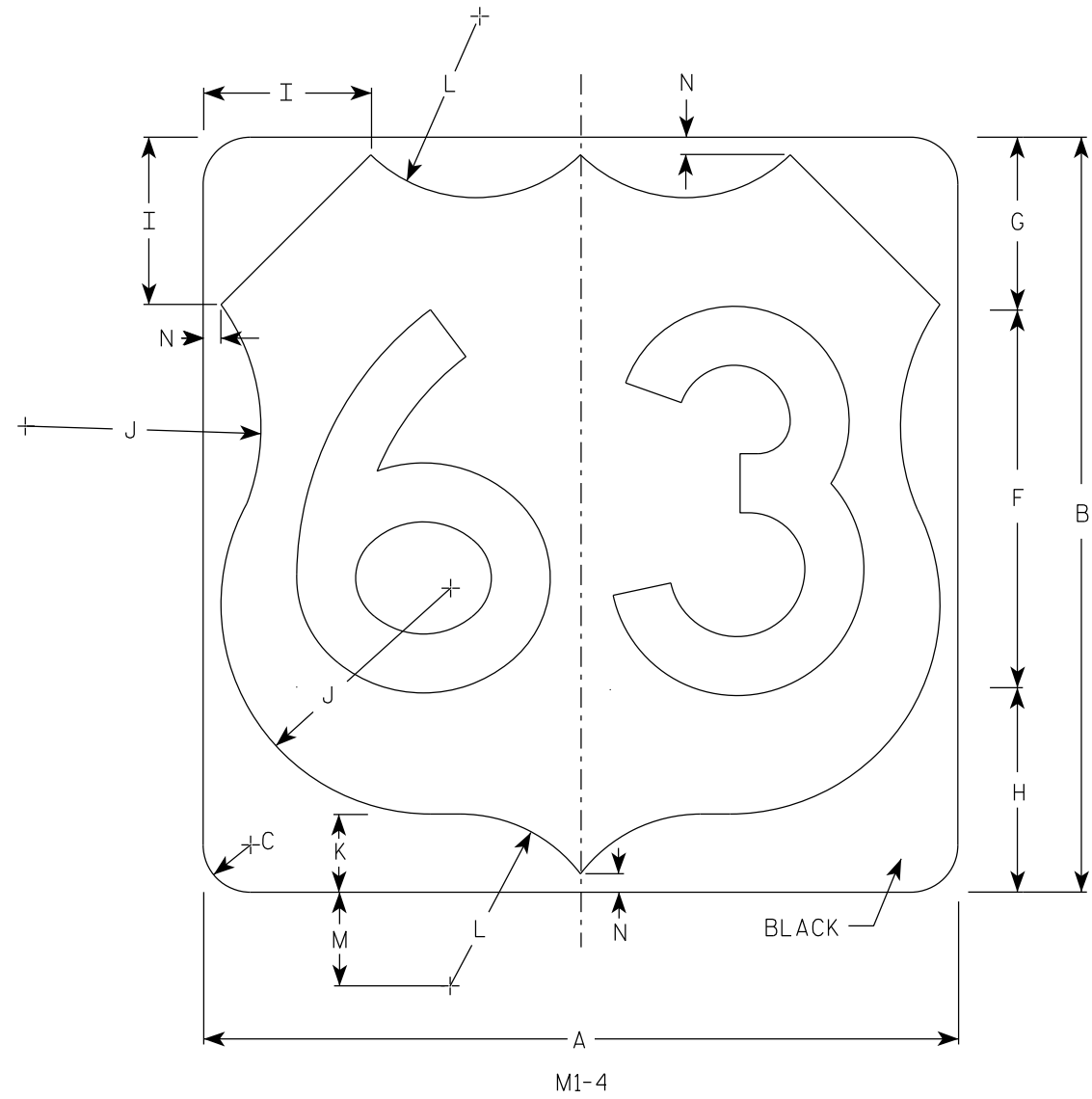
COUNTY:

SHEET NO:

E

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D except 3 number signs Series C



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	24	1 1/2			12	5 1/2	6 1/2	5	7 1/2	2 1/2	5 1/2	3	1/2													4.0
2M	24	24	1 1/2			12	5 1/2	6 1/2	5	7 1/2	2 1/2	5 1/2	3	1/2													4.0
3	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0
4	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0
5	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0

USH MARKER
M1-4 FOR ASSEMBLIES

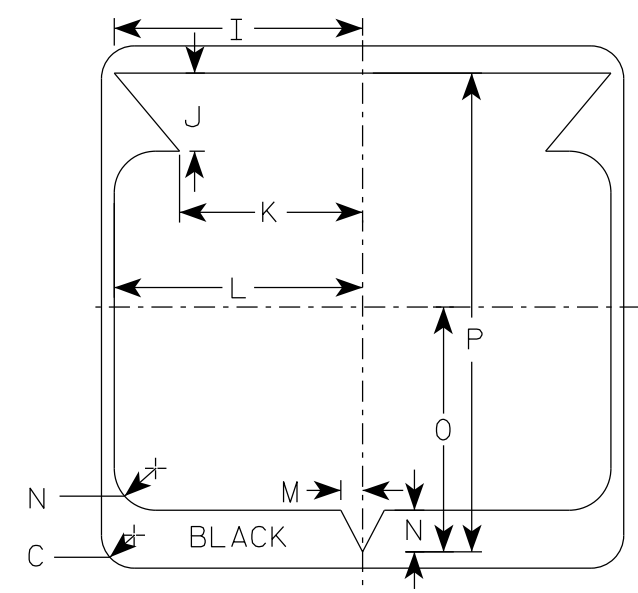
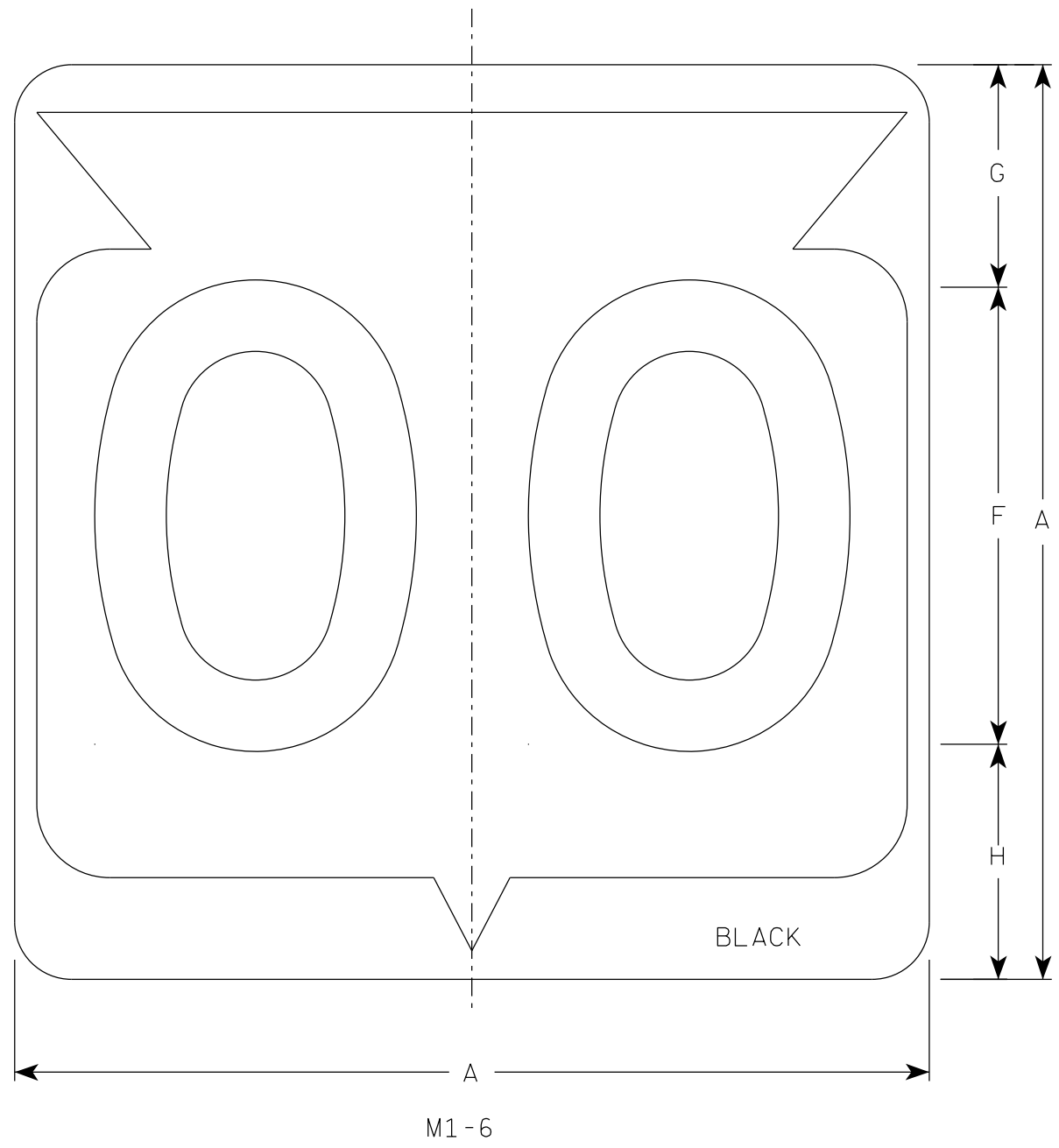
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Raub*
for State Traffic Engineer

DATE 12/20/22 PLATE NO. M1-4.11

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D except 3 number signs Series C



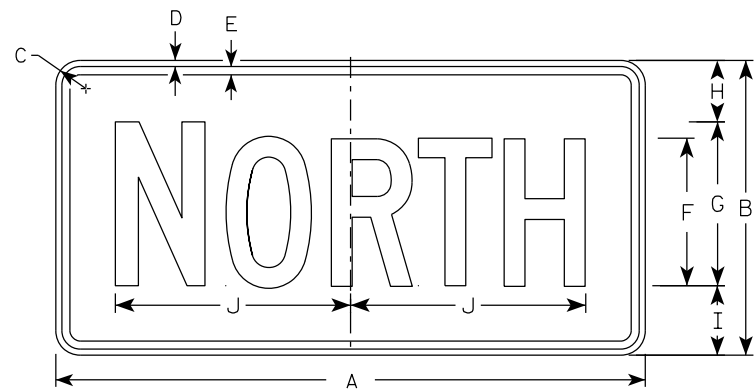
7

7

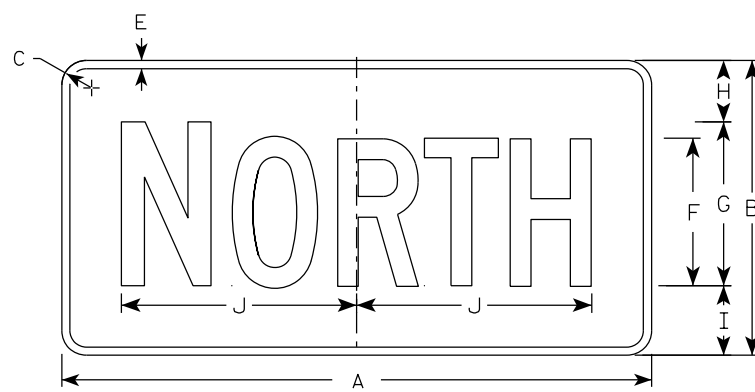
M1-6

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
2M	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0

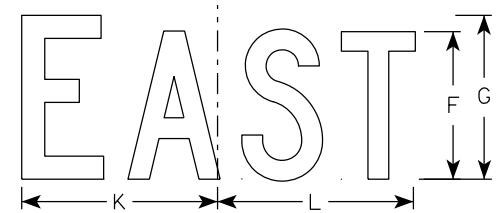
STATE ROUTE MARKER
M1-6 FOR ASSEMBLIES
WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Raub*
for State Traffic Engineer
DATE 11/8/2022 PLATE NO. M1-6.11



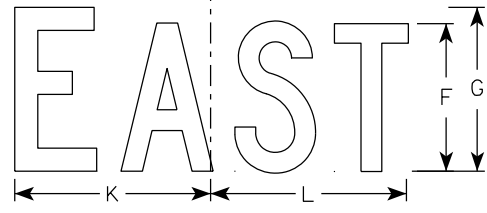
M3-1
MM3-1
MP3-1



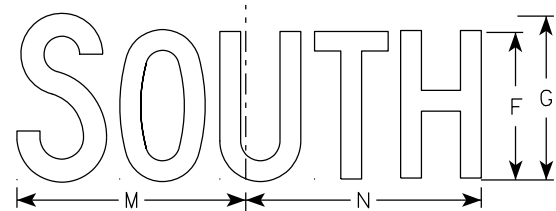
MB3-1
MK3-1
MN3-1



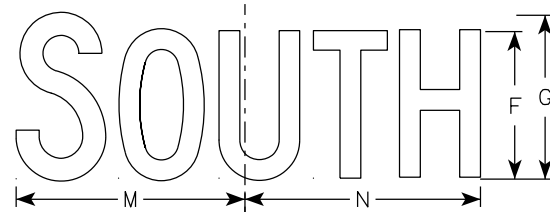
M3-2
MM3-2
MP3-2



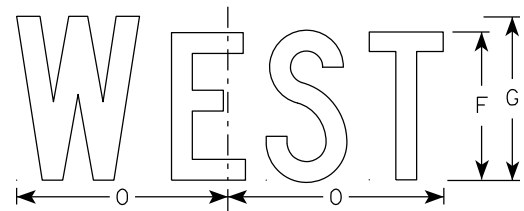
MB3-2
MK3-2
MN3-2



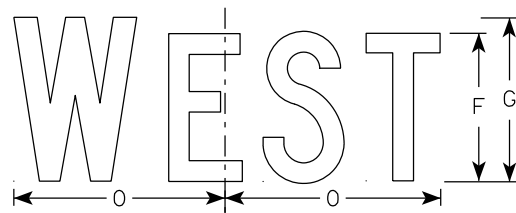
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

NOTES

- All Signs Type II - Type H Reflective
- Color:
 - Background - See note 5
 - Message - See note 5
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M3-1 thru M3-4 Background - White
 Message - Black
 MB3-1 thru MB3-4 Background - Blue
 Message - White
 MK3-1 thru MK3-4 Background - Green
 Message - White
 MM3-1 thru MM3-4 Background - White
 Message - Green
 MN3-1 thru MN3-4 Background - Brown
 Message - White
 MP3-1 thru MP3-4 Background - White
 Message - Blue
- Note the first letter of each direction is larger than the remainder of the message.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4												2.00
2M	24	12	1 1/2	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4												2.00
3	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5
4	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5
5	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5

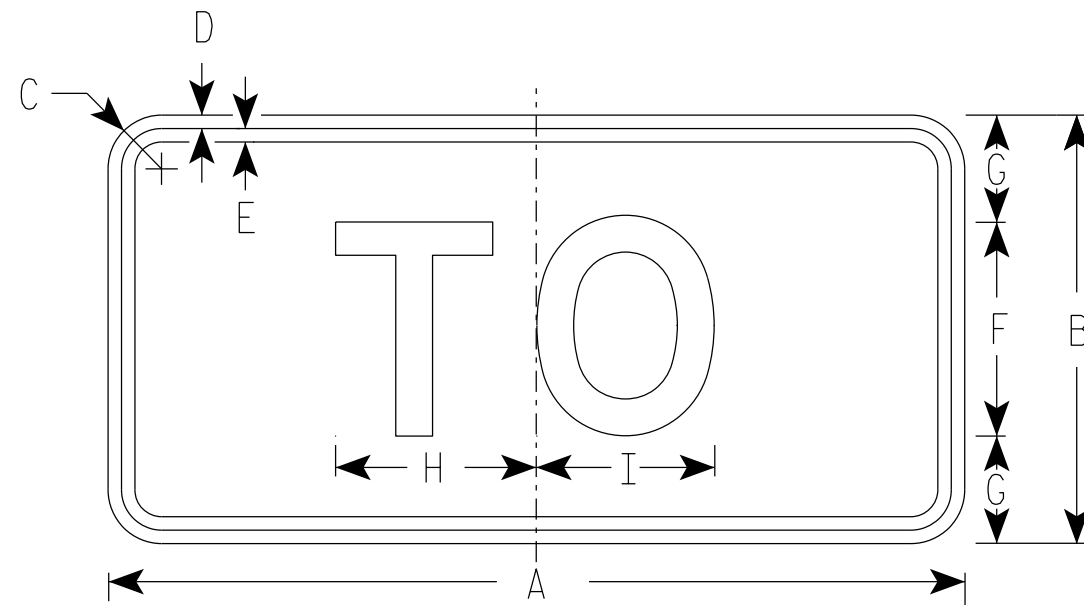
STANDARD SIGNS
M3-1 THRU M3-4
SERIES

WISCONSIN DEPT OF TRANSPORTATION

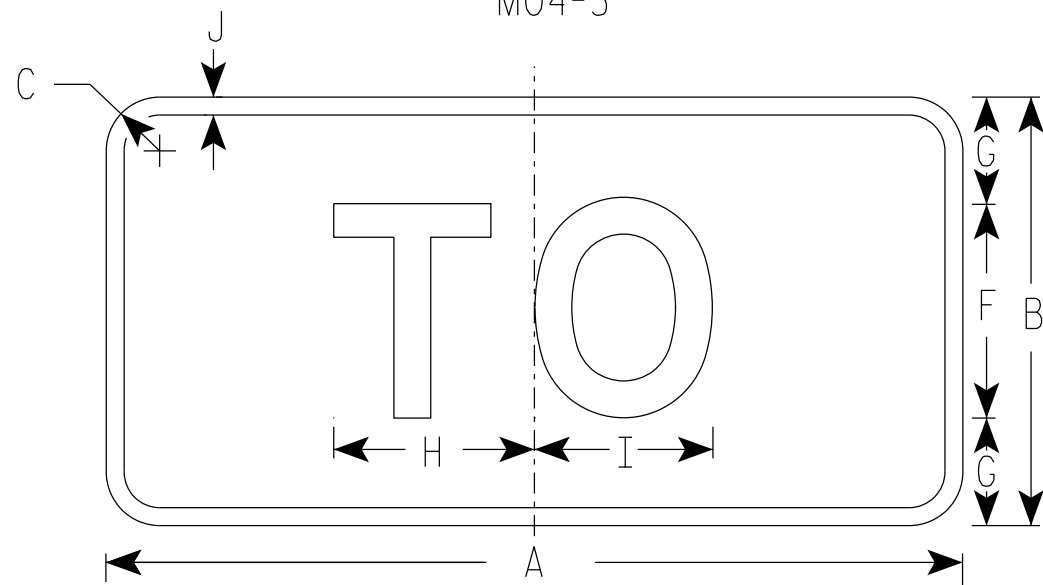
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/8/2023 PLATE NO. M3-1.15

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



M4-5
MM4-5
MP4-5
M04-5



MB4-5
MK4-5
MN4-5

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - See note 5
Message - See note 5
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-5 Background - White
Message - Black
MB4-5 Background - Blue
Message - White
MK4-5 Background - Green
Message - White
MM4-5 Background - White
Message - Green
MN4-5 Background - Brown
Message - White
MP4-5 Background - White
Message - Blue
M04-5 Background - Orange Type F Reflective
Message - Black

7

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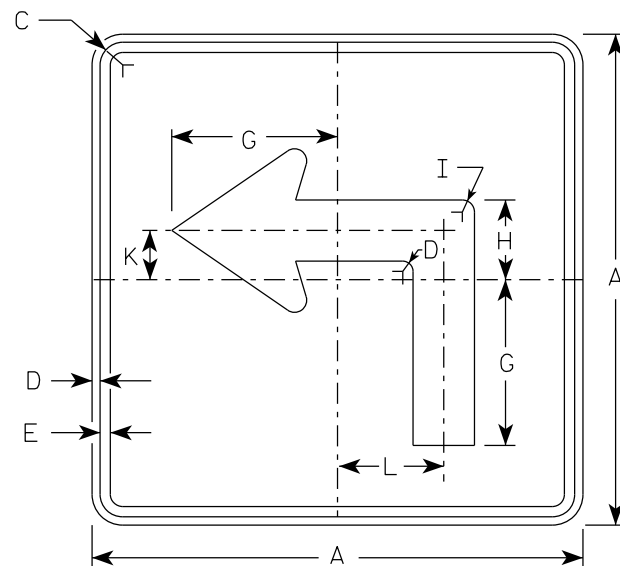
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	3/8	3/8	6	3	5 3/8	5 1/4	1/2																	2.00
2M	24	12	1 1/2	3/8	3/8	6	3	5 3/8	5 1/4	1/2																	2.00
3	36	18	1 1/2	3/8	1/2	9	4 1/2	8 1/4	8 3/8	1/2																	4.5
4	36	18	1 1/2	3/8	1/2	9	4 1/2	8 1/4	8 3/8	1/2																	4.5
5	36	18	1 1/2	3/8	1/2	9	4 1/2	8 1/4	8 3/8	1/2																	4.5

STANDARD SIGN
M4-5

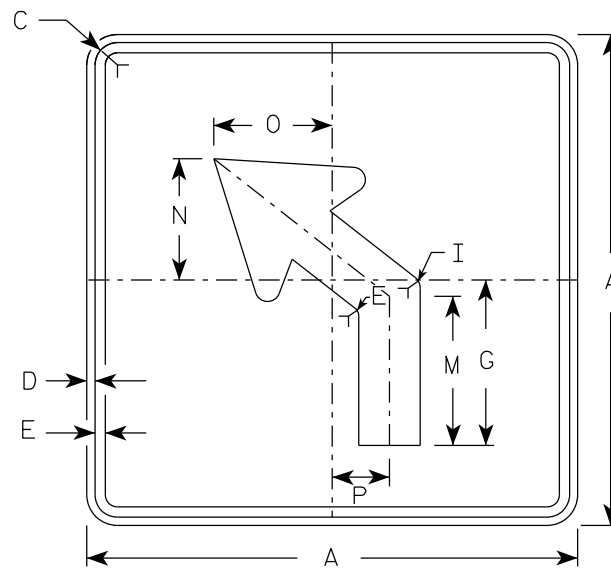
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

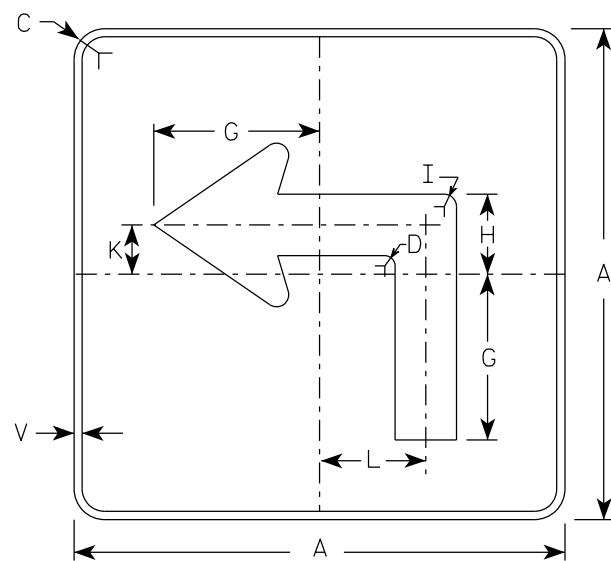
DATE 2/8/2023 PLATE NO. M4-5.11



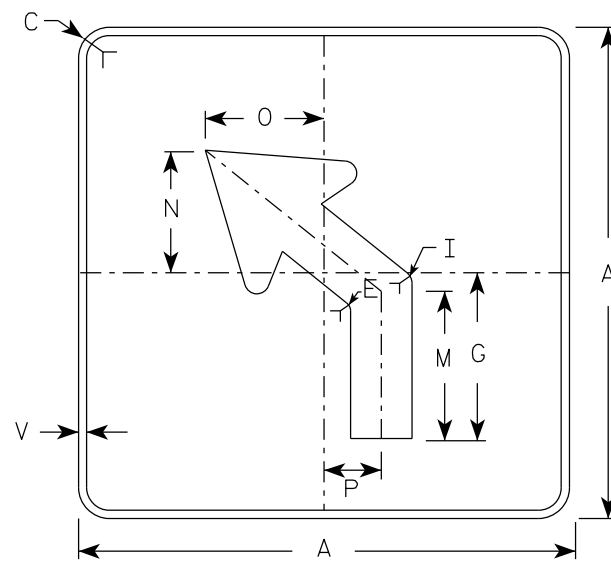
M5-1L
MM5-1L
M05-1L
MP5-1L



M5-2L
MM5-2L
M05-2L
MP5-2L

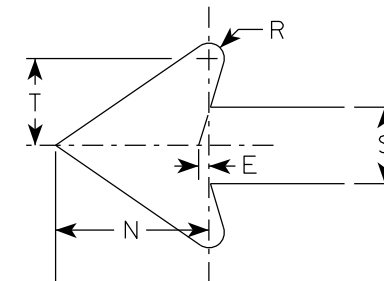


MB5-1L
MK5-1L
MN5-1L
MR5-1L



MB5-2L
MK5-2L
MN5-2L
MR5-2L

ARROW DETAIL



NOTES

- Signs are Type II - Type H reflective except as shown
- Color:
 - Background - See note 4
 - Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- | | |
|-----------------|---|
| M5-1 and M5-2 | Background - White |
| | Message - Black |
| MB5-1 and MB5-2 | Background - Blue |
| | Message - White |
| MK5-1 and MK5-2 | Background - Green |
| | Message - White |
| MM5-1 and MM5-2 | Background - White |
| | Message - Green |
| MN5-1 and MN5-2 | Background - Brown |
| | Message - White |
| M05-1 and M05-2 | Background - Orange - Type F Reflective |
| | Message - Black |
| MP5-1 and MP5-2 | Background - White |
| | Message - Blue |
| MR5-1 and MR5-2 | Background - Brown |
| | Message - Yellow |
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

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SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	21		1 1/2	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3		1/2					3.06
2M	21		1 1/2	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3		1/2					3.06
3	30		1 7/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4		1/2					6.25
4	30		1 7/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4		1/2					6.25
5	30		1 7/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4		1/2					6.25

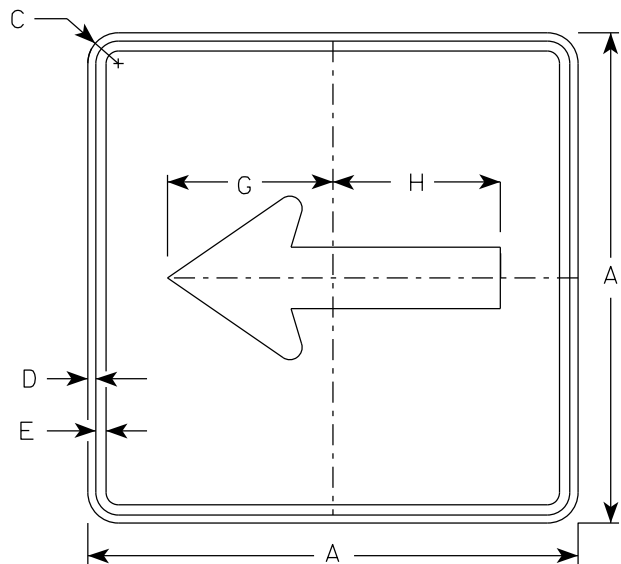
STANDARD SIGN
M5-1 & M5-2

WISCONSIN DEPT OF TRANSPORTATION

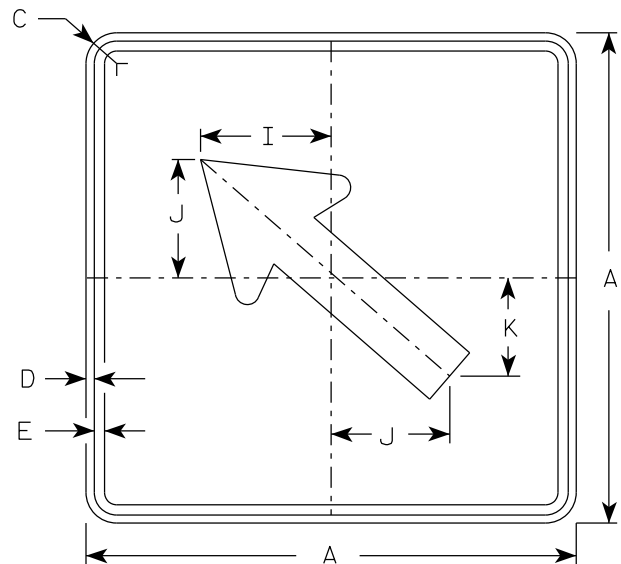
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/13/2023 PLATE NO. M5-1.15

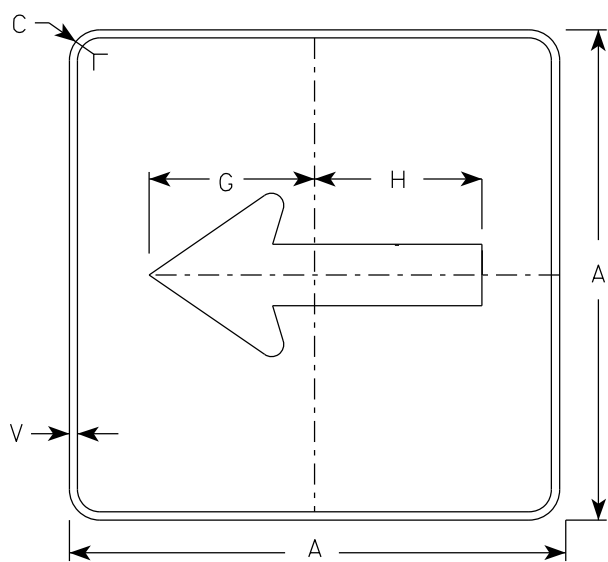
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



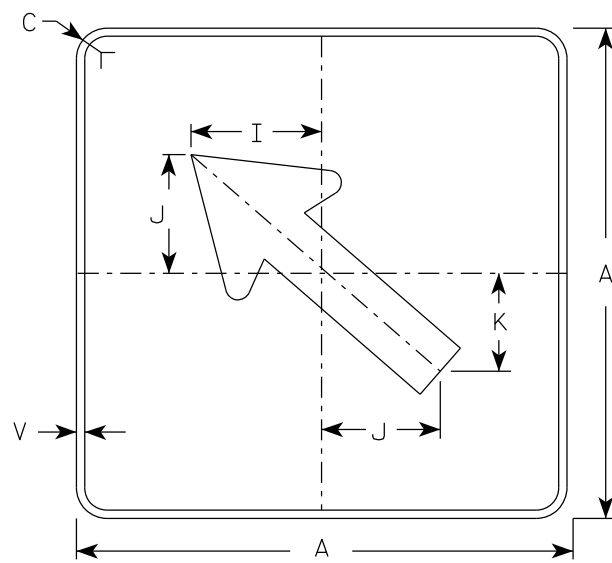
M6-1
MM6-1
M06-1
MP6-1



M6-2
MM6-2
M06-2
MP6-2



MB6-1
MK6-1
MN6-1
MR6-1

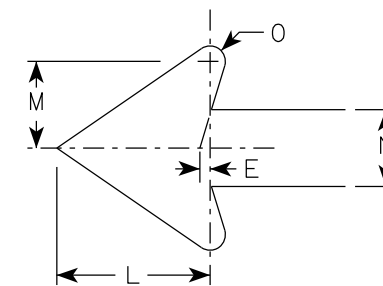


MB6-2
MK6-2
MN6-2
MR6-2

NOTES

- Signs are Type II - Type H Reflective except as Shown
- Color:
 - Background - See note 4
 - Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White
Message - Black
MB6-1 and MB6-2 Background - Blue
Message - White
MK6-1 and MK6-2 Background - Green
Message - White
MM6-1 and MM6-2 Background - White
Message - Green
MN6-1 and MN6-2 Background - Brown
Message - White
M06-1 and M06-2 Background - Orange - Type F Reflective
Message - Black
MP6-1 and MP6-2 Background - White
Message - Blue
MR6-1 and MR6-2 Background - Brown
Message - Yellow

ARROW DETAIL



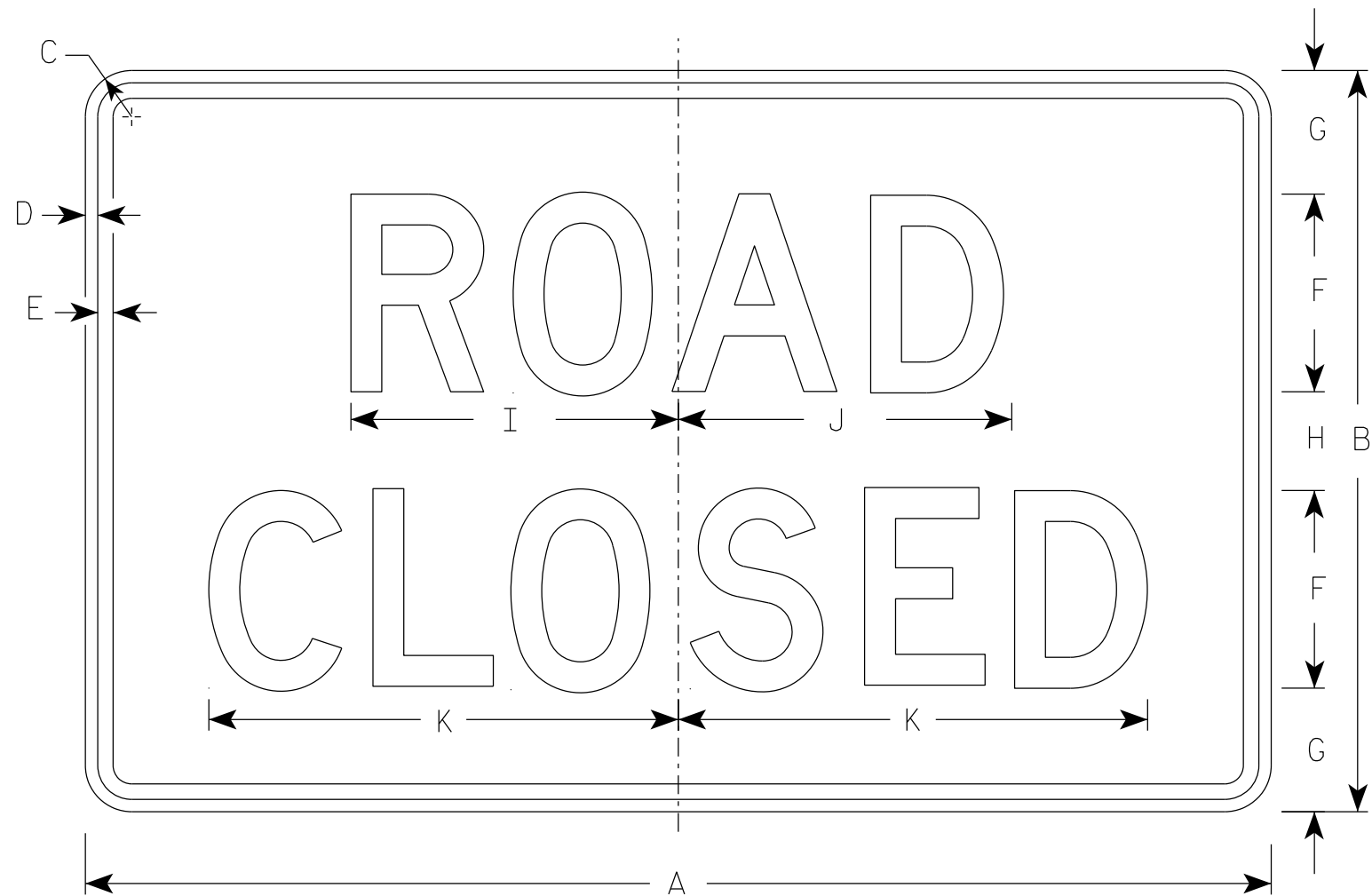
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	21		1 1/2	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2							1/2					3.06
2M	21		1 1/2	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2							1/2					3.06
3	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2					6.25
4	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2					6.25
5	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2					6.25

STANDARD SIGN
M6-1 & M6-2
SERIES

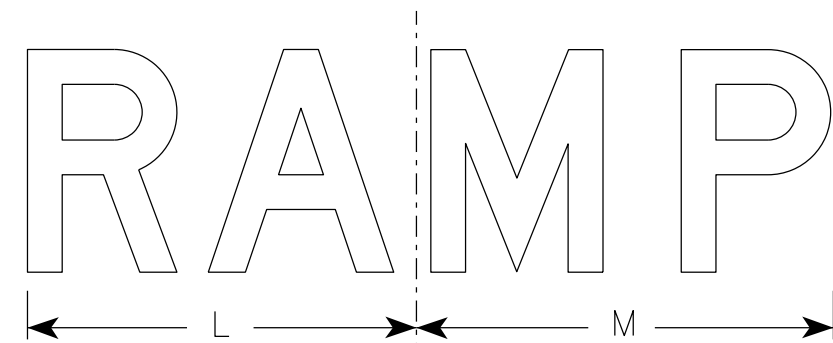
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

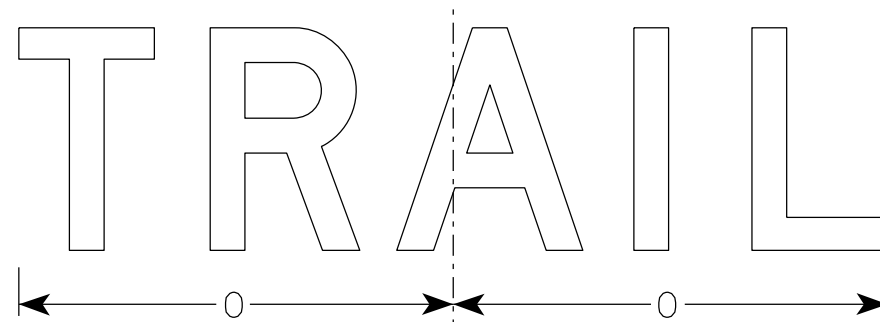
DATE 2/13/2023 PLATE NO. M6-1.16



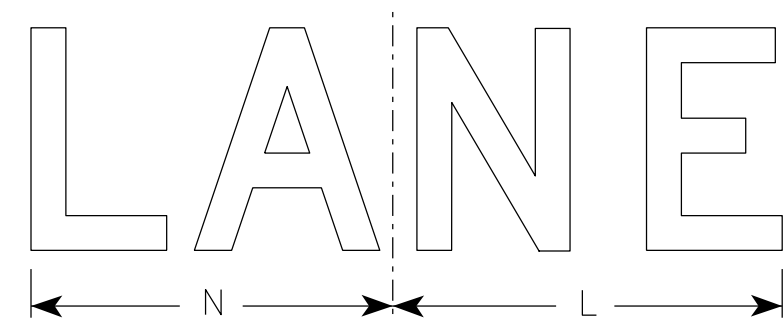
R11-2



R11-2R



R11-2T



R11-2L

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Modify the message as required.

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SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
2M	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
3	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
4	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
5	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0

STANDARD SIGN
R11-2

WISCONSIN DEPT OF TRANSPORTATION

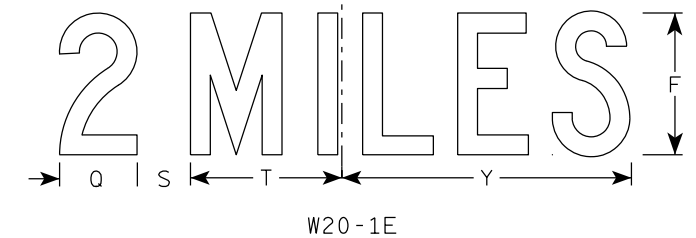
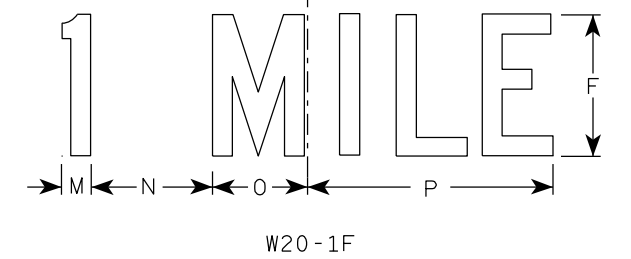
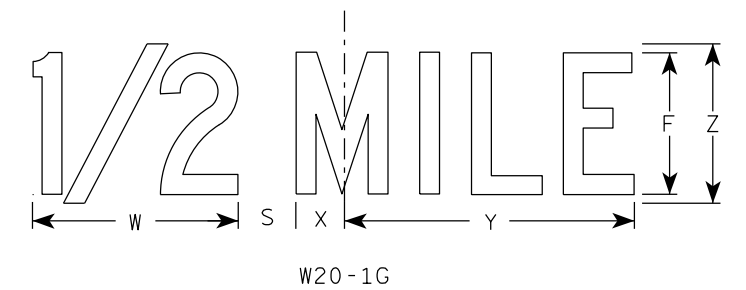
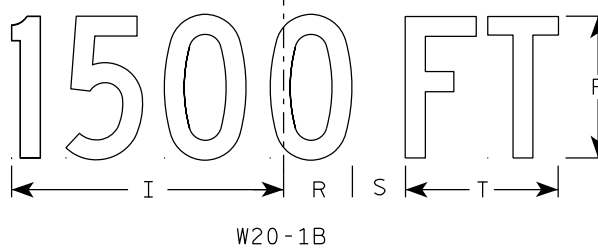
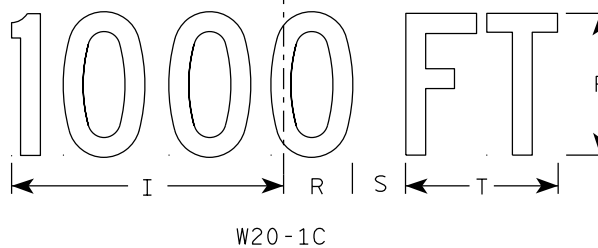
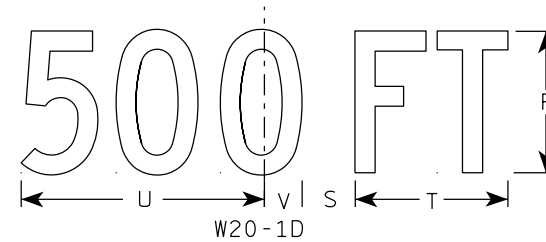
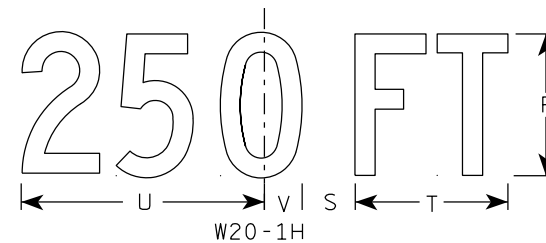
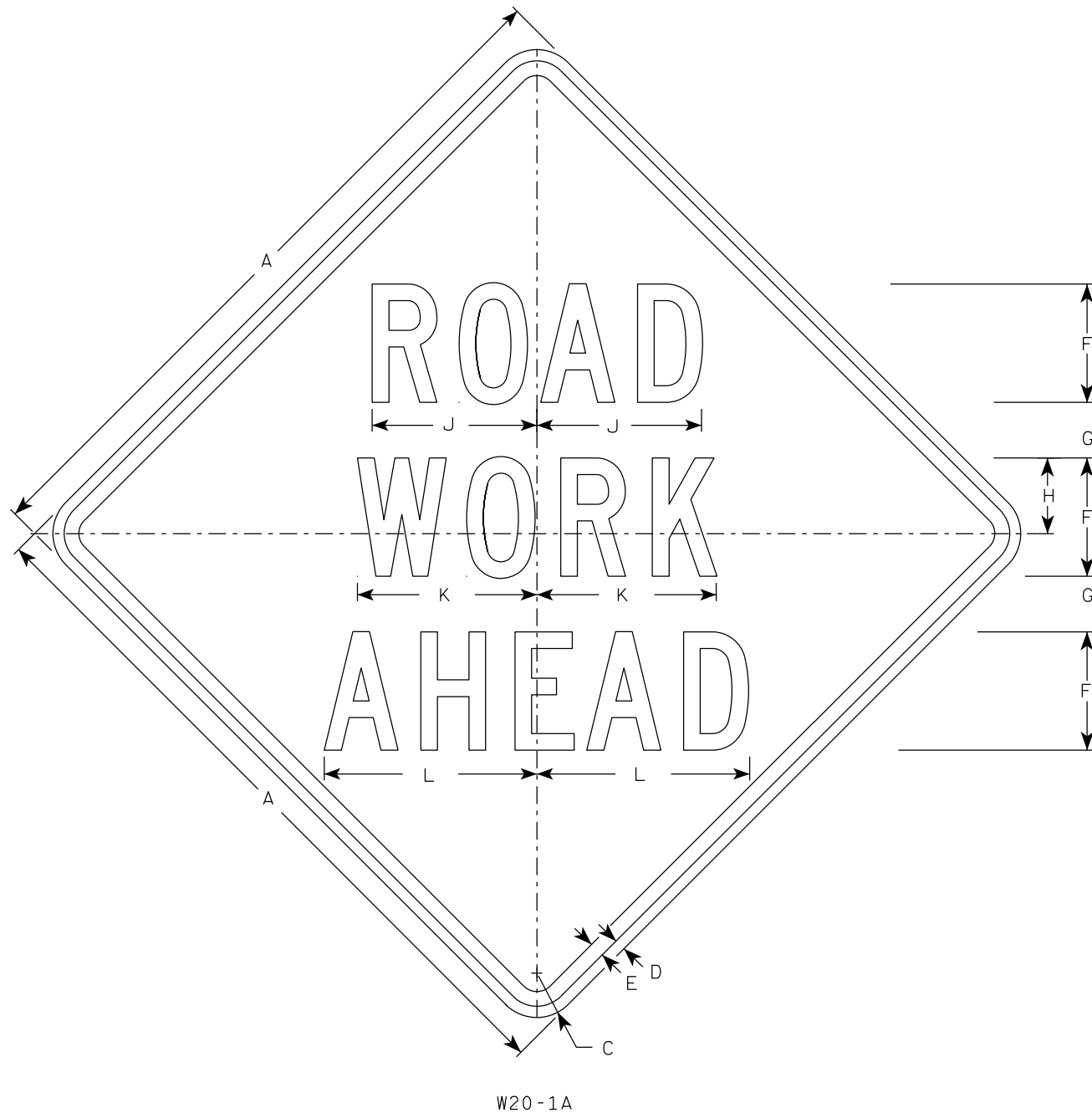
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 2/5/24 PLATE NO. R11-2.12

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



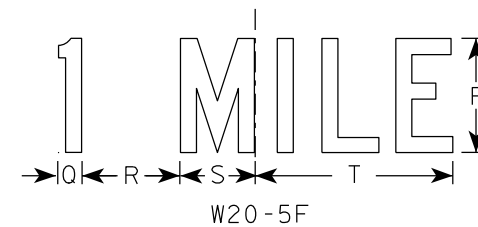
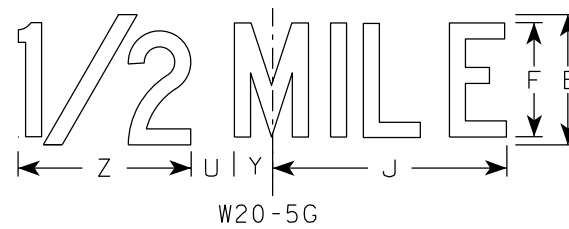
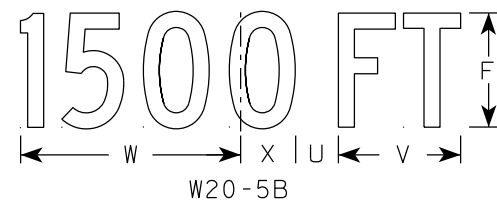
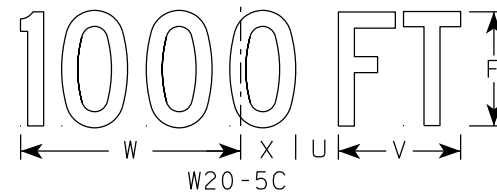
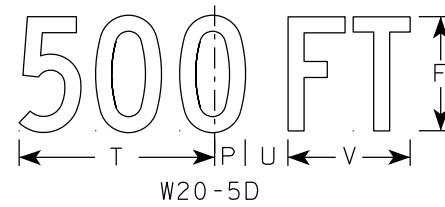
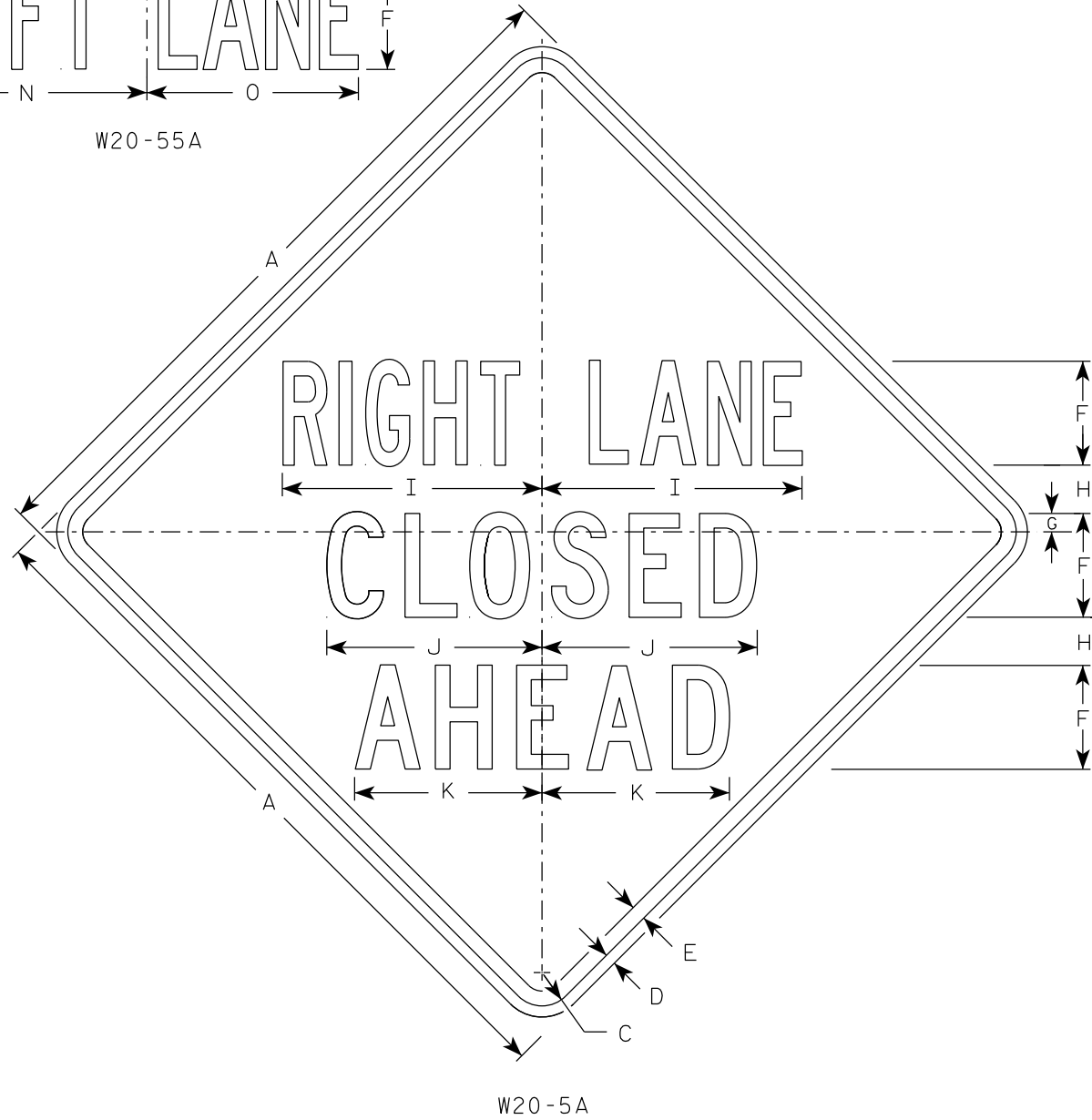
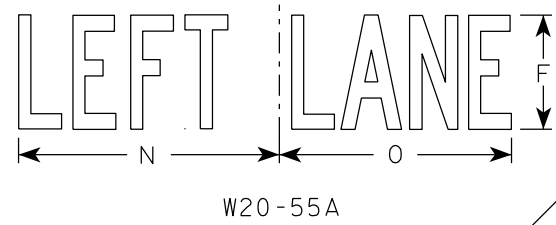
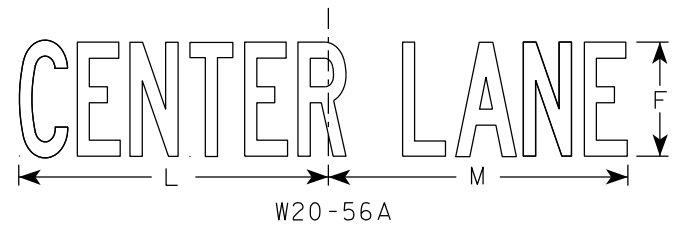
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9	3 1/4	2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN
W20-1A, B, C, D, E, F, G & H

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 1/10/2024 PLATE NO. W20-1.12



NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. " _____ LANE" is Series B.
All other copy is Series C.

7

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SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	6	2 1/4	5/8	3/4	5	7/8	2 1/2	13 1/8	10 3/4	9 1/2	14 1/4	13 5/8	12	12	1 3/8	1 1/8	4 1/2	3 1/2	9	1 7/8	5 5/8	10 1/8	2 1/2	1 3/4	8	9.0
2S	48	8	3	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
2M	48	8	3	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
3	48	8	3	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
4	48	8	3	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
5	48	8	3	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0

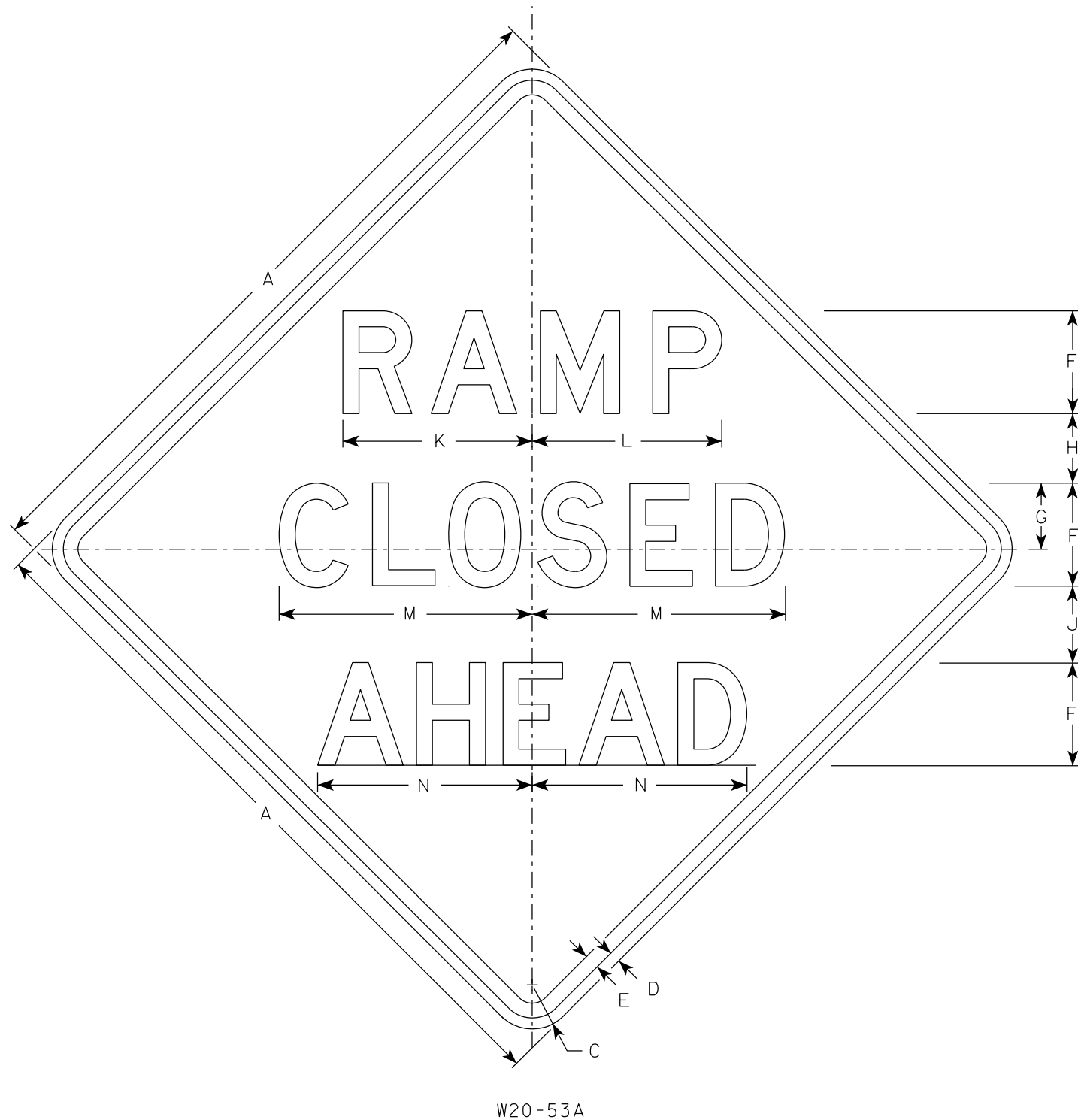
STANDARD SIGN
W20-5A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

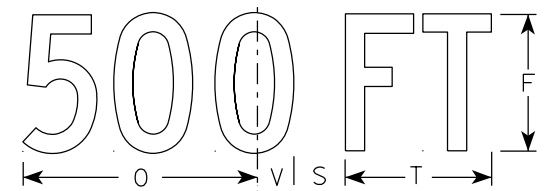
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/27/24 PLATE NO. W20-5.12

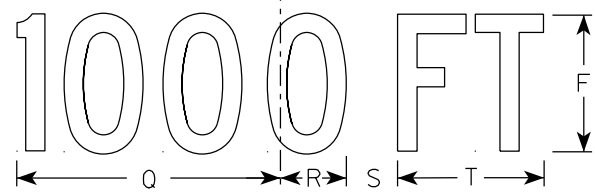
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



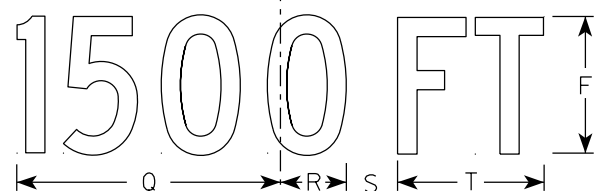
W20-53A



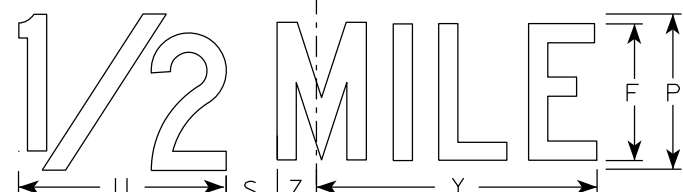
W20-53D



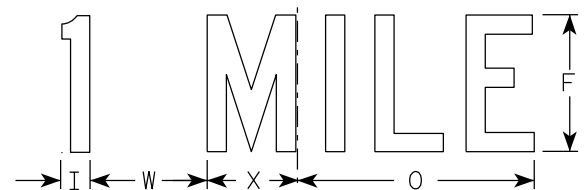
W20-53C



W20-53B



W20-53G



W20-53F

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.
Line 3 is Series D for AHEAD and Series C for all other distances.

7

7

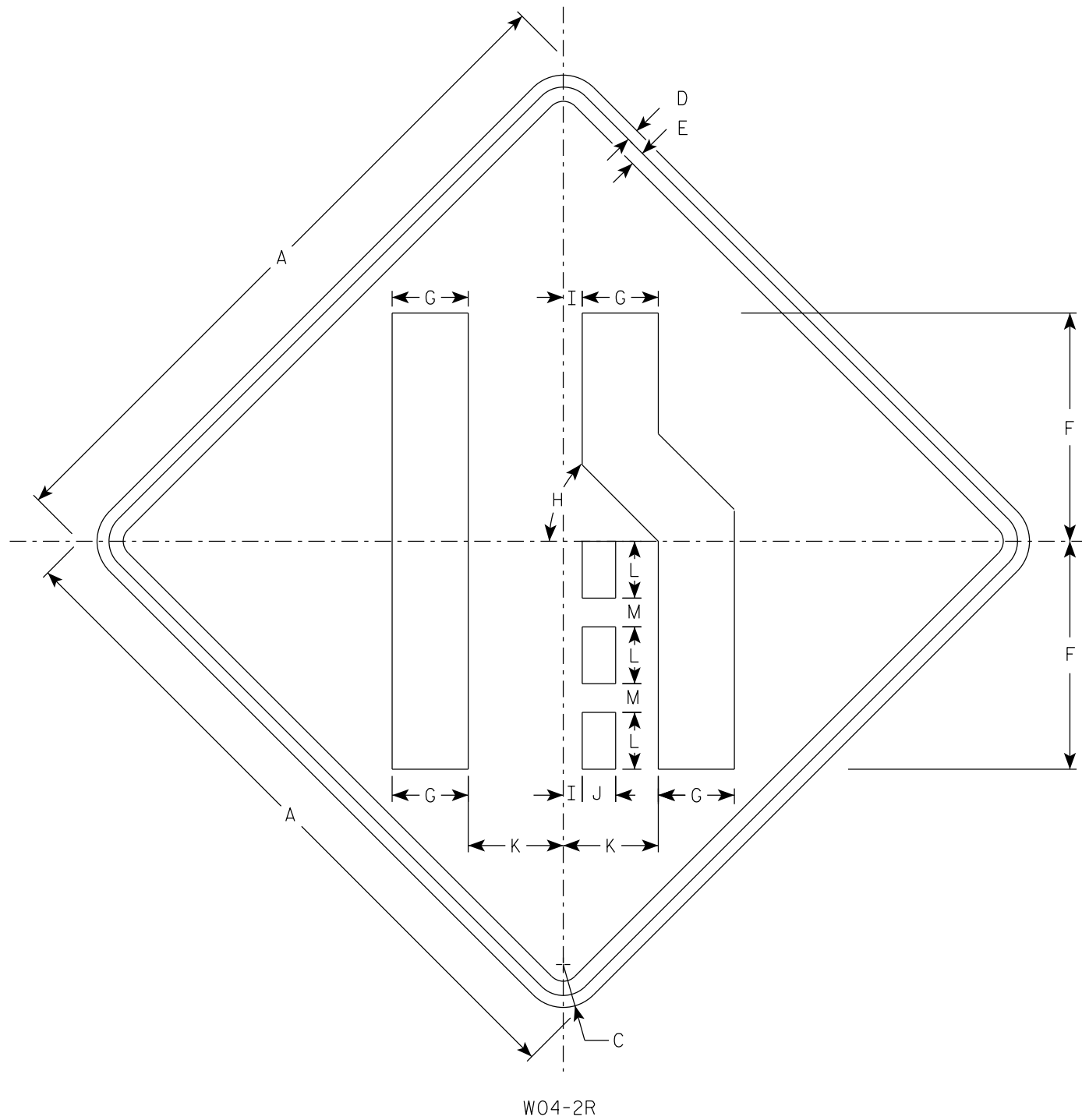
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	5	3 3/8	3 1/2	1 1/8	4	9 1/4	9 1/4	12 1/2	11	9	6	10 1/8	2 1/2	1 7/8	5 5/8	8	1 3/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	12 7/8	12 7/8	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	12 7/8	12 7/8	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
3	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	12 7/8	12 7/8	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
4	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	12 7/8	12 7/8	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
5	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	12 7/8	12 7/8	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-53A,B,C,D,F,G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 1/10/2024 PLATE NO. W20-53.2



NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W04-2L is the same as W04-2R except the symbols are reversed along the vertical centerline.

7

7

W04-2R

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	12	4	45°	1	1 3/4	5	3	1 1/2														9.0
2S	48		3	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
2M	48		3	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
3	48		3	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
4	48		3	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
5	48		3	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0

STANDARD SIGN
W04-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 1/25/2024 PLATE NO. W04-2.2

F = FIXED BEARING
 GL = GUIDED LONGITUDINAL UNIDIRECTIONAL BEARING
 [Symbol] EXISTING FLOOR DRAIN
 * END TO END OF PPC OVERLAY (SEE 'SECTION THRU ABUTMENT' FOR DETAILS ON THE LIMITS)

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HL-93
 INVENTORY RATING: RF = 0.90
 OPERATING RATING: RF = 1.17
 WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV): 250 (KIPS)

MATERIAL PROPERTIES:

CONCRETE MASONRY:
 RAPID SET DECK REPAIR $f'_c = 3,000$ PSI (AT 3 HOURS)

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.

3/4" INCH OF CONCRETE SHALL BE REMOVED FROM THE ENTIRE BRIDGE DECK UNDER THE BID ITEM "CLEANING DECKS".

AREAS OF "PREPARATION DECKS TYPE 1" SHALL BE DEFINED BY A SAW CUT.

PREPARATION DECKS TYPE 1 AND PREPARATION DECKS TYPE 2 REPAIR AREAS ARE BASED ON THE PLANS AND AS DETERMINED BY THE ENGINEER. DECK PREPARATION REPAIRS SHALL BE FILLED WITH "RAPID SET DECK REPAIR". POLYESTER POLYMER CONCRETE PATCHES MAY BE SUBSTITUTED AT NO EXTRA COST.

DECK REPAIRS SHALL BE FILLED PRIOR TO OVERLAY PLACEMENT IF DECK REPAIRS ARE FILLED WITH "RAPID SET DECK REPAIR".

SHOT BLASTING, OVERLAY PRIME COAT, DECK SURFACE PREPARATIONS, AND TRANSITIONAL AREAS ARE INCLUDED IN THE BID ITEM "POLYESTER POLYMER CONCRETE OVERLAY".

OVERLAY CONSTRUCTION JOINTS SHALL BE APPROVED BY THE ENGINEER. AVOID PLACING LONGITUDINAL JOINTS NEAR WHEEL PATHS. WHEN REQUIRED, PLACE LONGITUDINAL JOINTS AT LANE LINES OR IN THE MIDDLE OF THE LANE. WHEEL PATHS DURING TEMPORARY TRAFFIC STAGING NEED NOT BE CONSIDERED.

IF MINIMUM OVERLAY THICKNESS IS EXCEEDED BY MORE THAN 1/2", CONTACT THE STRUCTURES DESIGN SECTION.

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	1
509.0302	PREPARATION DECKS TYPE 2	SY	1
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	10
509.0500	CLEANING DECKS	SY	7,811
509.9015.S	REMOVING POLYMER OVERLAY STRUCTURE B-05-658	SY	7,811
SPV.0035	RAPID SET DECK REPAIR	CY	1
SPV.0180	POLYESTER POLYMER CONCRETE OVERLAY	SY	7,811

TRAFFIC DATA

RAMP FEN: ADT = 39,000 (2035) R.D.S. = 70 MPH
IH 41: ADT = 38,400 (2035) R.D.S. = 70 MPH

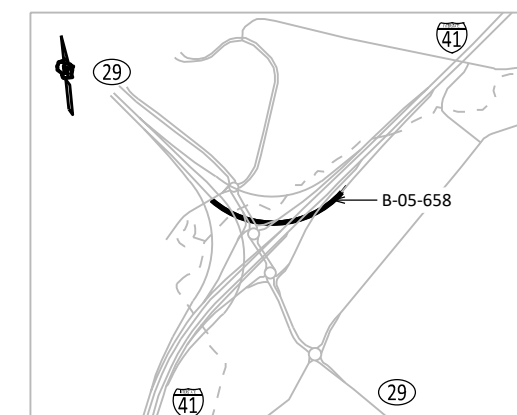
STRUCTURE DESIGN CONTACTS:

STACIE HAUSERMAN 608-261-6109
 KYLE BUSCH 608-267-0465

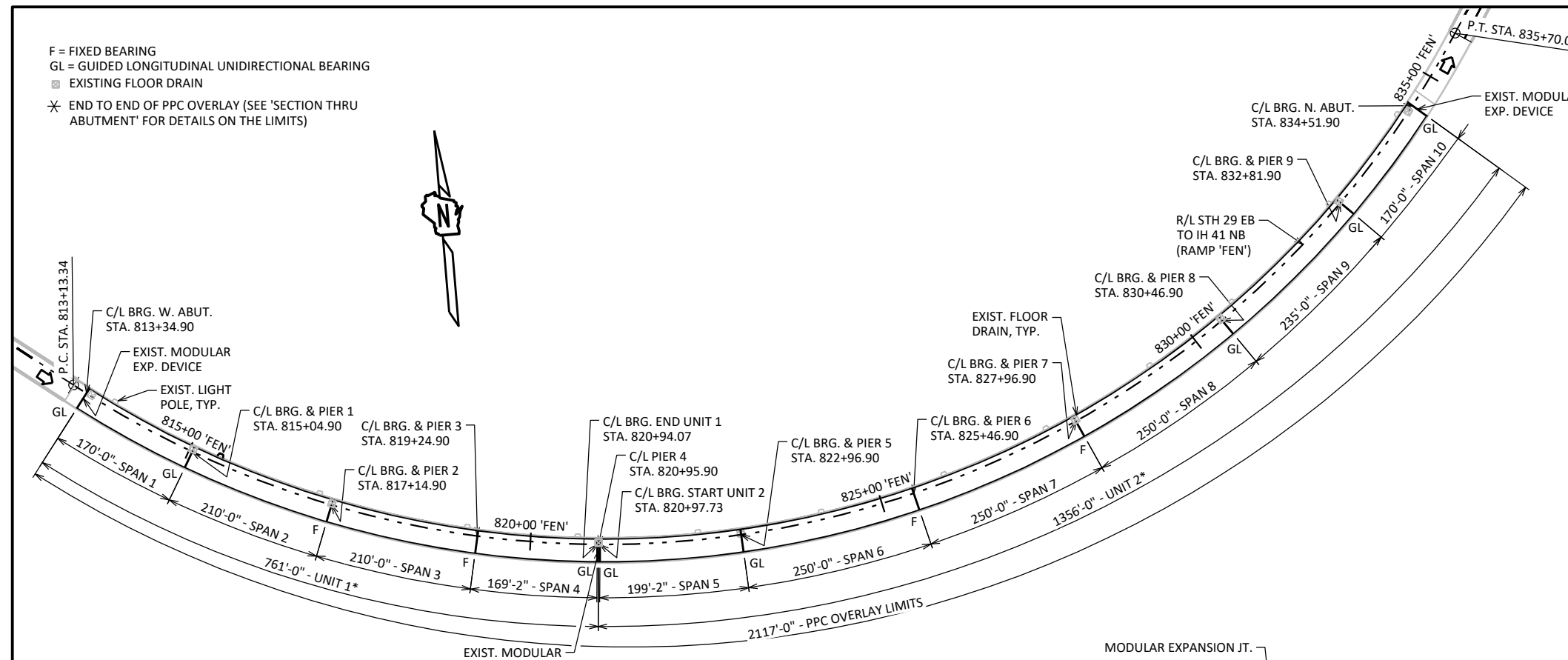
CURVE DATA

EB STH 29 TO IH 41 NB RAMP

P.I. = 828+03.10
 N = 575662.88
 E = 83655.58
 $\Delta = 95^\circ 25' 27.06''$
 $D = 4^\circ 13' 42.49''$
 T = 1489.76'
 L = 2256.71'
 R = 1355.00'
 S.E. = VARIES
 P.C. = 813+13.34
 P.T. = 835+70.05

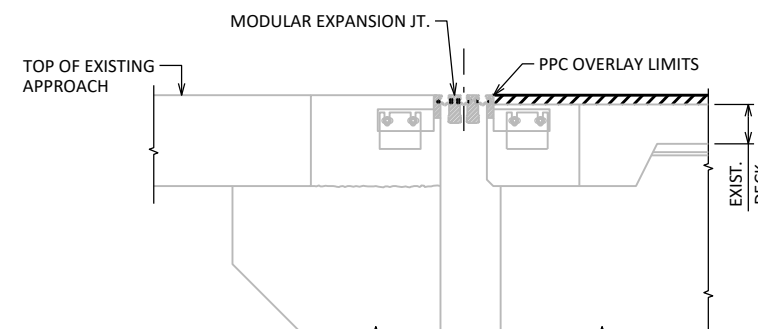


PROJECT LOCATION



PLAN

PPC OVERLAY
 10-SPAN CONT. STEEL BOX GIRDER BRIDGE



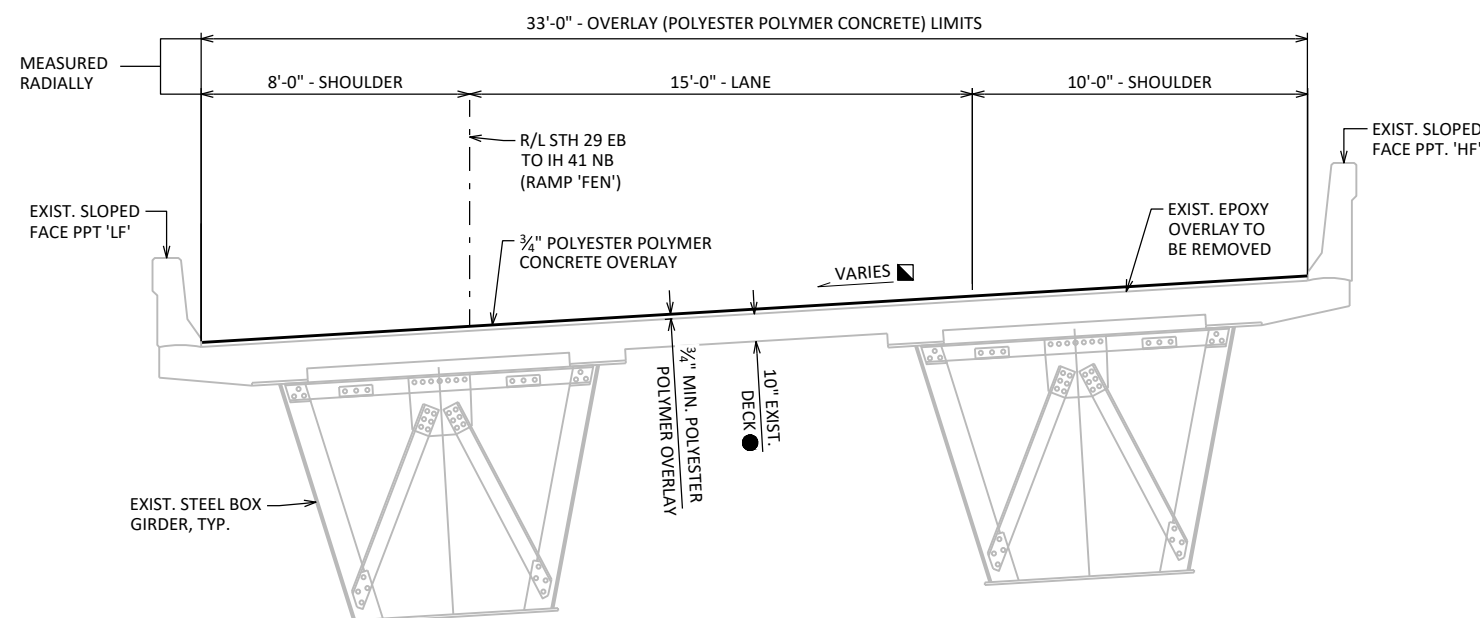
SECTION THRU ABUT.

WEST ABUTMENT JOINT SHOWN, PPC LIMITS AT PIER 4 AND NORTH ABUTMENT ARE SIMILAR

SUPERELEVATION TABLE

DESCRIPTION	STATION	SUPER ELEVATION
ADVERSE CROWN	812+46.47	2.0%
C/L BRG. W. ABUT.	813+34.90	4.67%
BEGIN FULL SUPERELEVATION	813+79.34	6.0%
C/L BRG. N. ABUT.	834+51.90	6.0%
END FULL SUPER	835+16.72	6.0%

POSITIVE SUPERELEVATION MEANS THE RIGHT EDGE OF THE DECK IS HIGHER WHEN LOOKING UPSTATION.



TYPICAL SECTION

(LOOKING UPSTATION)

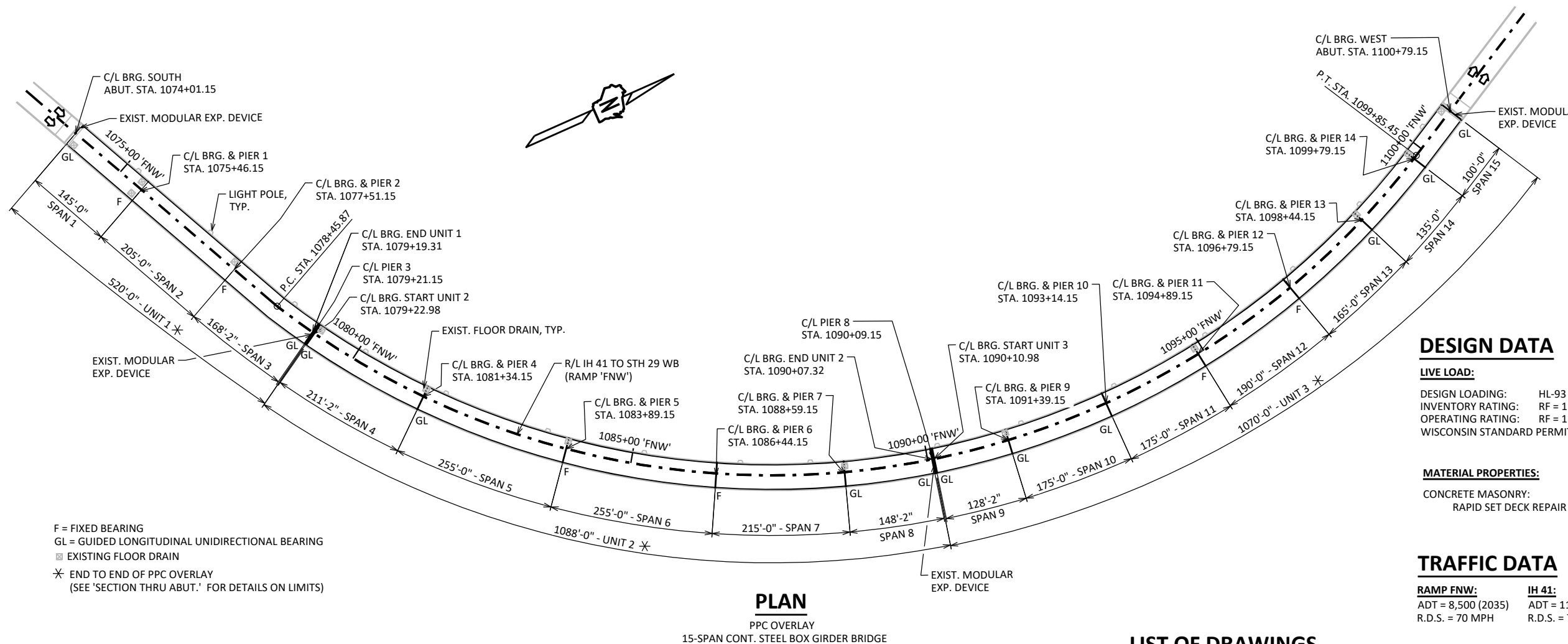
● REMOVE 3/4" OF EXISTING DECK PER BID ITEM 'CLEANING DECKS'

NO.	DATE	REVISION	BY
 ACCEPTED KHB 1/28/26 CHIEF STRUCTURES DESIGN ENGINEER DATE			
STRUCTURE B-05-658			
STH 29/32 TO IH 41 NB RAMP			
COUNTY	BROWN	VILLAGE	HOWARD
DESIGN SPEC.	RAHABILITATION N/A		
DESIGNED BY	SEH	DESIGNED CK'D	RRH
DRAWN BY	SEH	PLANS CK'D	DLM
POLYESTER POLYMER CONCRETE OVERLAY			SHEET 1 OF 1

8

8

SCALE =



F = FIXED BEARING
 GL = GUIDED LONGITUDINAL UNIDIRECTIONAL BEARING
 [Symbol] EXISTING FLOOR DRAIN
 * END TO END OF PPC OVERLAY
 (SEE 'SECTION THRU ABUT.' FOR DETAILS ON LIMITS)

PLAN

PPC OVERLAY
 15-SPAN CONT. STEEL BOX GIRDER BRIDGE

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HL-93
 INVENTORY RATING: RF = 1.01
 OPERATING RATING: RF = 1.30
 WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV): 250 (KIPS)

MATERIAL PROPERTIES:

CONCRETE MASONRY:
 RAPID SET DECK REPAIR $f'_c = 3,000$ PSI (AT 3 HOURS)

TRAFFIC DATA

RAMP FNW: ADT = 8,500 (2035)
 R.D.S. = 70 MPH
IH 41: ADT = 11,700 (2035)
 R.D.S. = 70 MPH

CURVE DATA

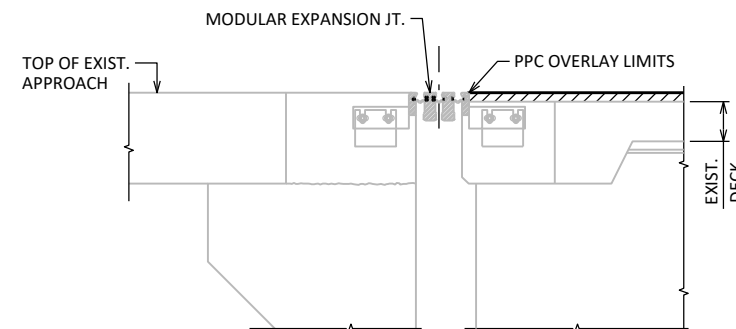
IH 41 NB TO STH 29 WB RAMP
 P.I. = 1092+12.07
 N = 575867.66
 E = 83739.36
 $\Delta = 90^\circ 28' 17.69''$
 D = 4° 13' 42.49"
 T = 1366.20'
 L = 2139.58'
 R = 1355.00'
 S.E. = VARIES
 P.C. = 1078+45.87
 P.T. = 1099+85.45

LIST OF DRAWINGS

- POLYESTER POLYMER CONCRETE OVERLAY
- CROSS SECTION & QUANTITIES

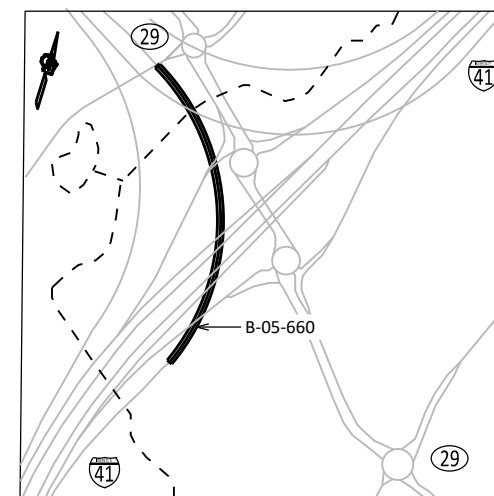
GENERAL NOTES

- DRAWINGS SHALL NOT BE SCALED.
- DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
- 3/4" INCH OF CONCRETE SHALL BE REMOVED FROM THE ENTIRE BRIDGE DECK UNDER THE BID ITEM "CLEANING DECKS".
- AREAS OF "PREPARATION DECKS TYPE 1" SHALL BE DEFINED BY A SAW CUT.
- PREPARATION DECKS TYPE 1 AND PREPARATION DECKS TYPE 2 AREAS ARE BASED ON THE PLANS AND AS DETERMINED BY THE ENGINEER. DECK PREPARATION DECK REPAIRS SHALL BE FILLED WITH "RAPID SET DECK REPAIR". POLYESTER POLYMER CONCRETE PATCHES MAY BE SUBSTITUTED AT NO EXTRA COST.
- DECK REPAIRS SHALL BE FILLED PRIOR TO OVERLAY PLACEMENT IF DECK REPAIRS ARE FILLED WITH "RAPID SET DECK REPAIR".
- SHOT BLASTING, OVERLAY PRIME COAT, DECK SURFACE PREPARATIONS, AND TRANSITIONAL AREAS ARE INCLUDED IN THE BID ITEM "POLYESTER POLYMER CONCRETE OVERLAY".
- OVERLAY CONSTRUCTION JOINTS SHALL BE APPROVED BY THE ENGINEER. AVOID PLACING LONGITUDINAL JOINTS NEAR WHEEL PATHS. WHEN REQUIRED, PLACE LONGITUDINAL JOINTS AT LANE LINES OR IN THE MIDDLE OF THE LANE. WHEEL PATHS DURING TEMPORARY TRAFFIC STAGING NEED NOT BE CONSIDERED.
- IF MINIMUM OVERLAY THICKNESS IS EXCEEDED BY MORE THAN 1/2", CONTACT THE STRUCTURES DESIGN SECTION.



SECTION THRU ABUT.

SOUTH ABUTMENT JOINT SHOWN, PPC LIMITS AT PIER 3, PIER 8 AND WEST ABUTMENT ARE SIMILAR



PROJECT LOCATION

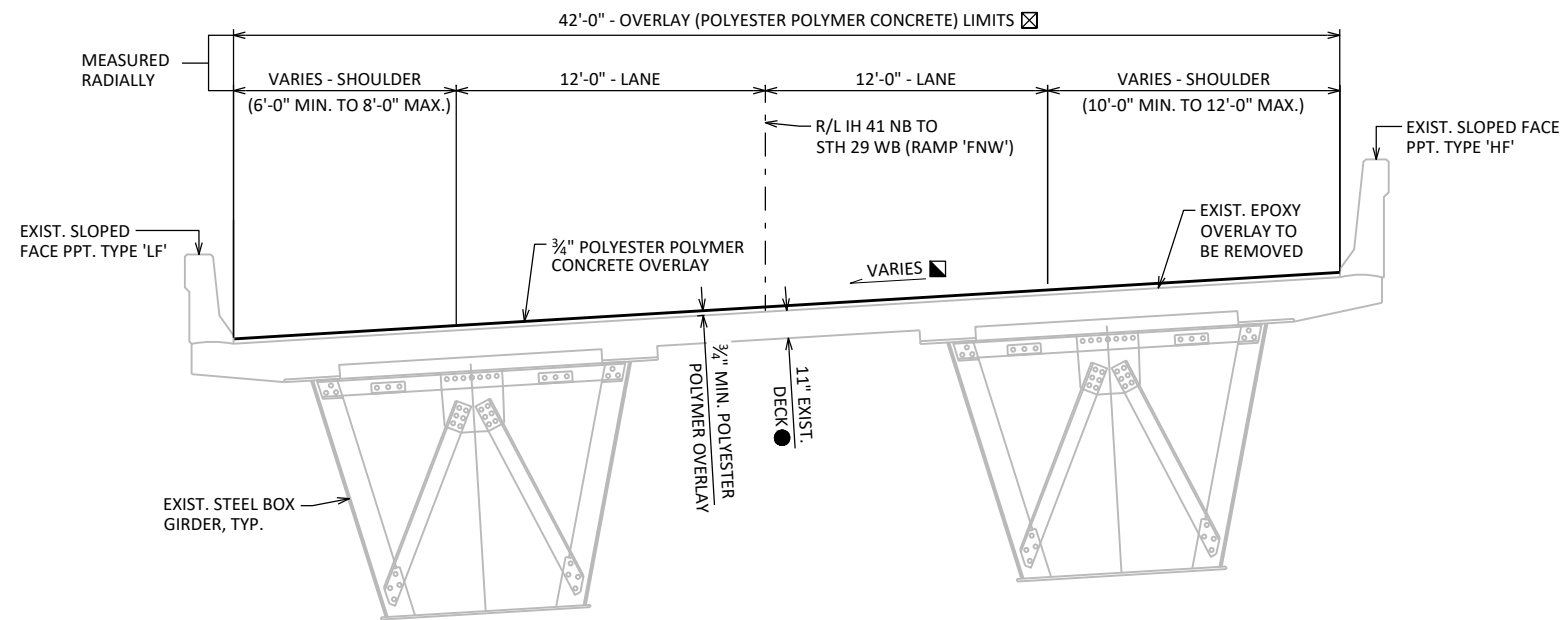
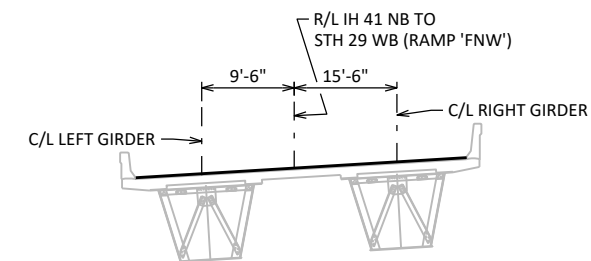
STRUCTURE DESIGN CONTACTS:
 STACIE HAUSSERMAN 608-261-6109
 KYLE BUSCH 608-267-0465

NO.	DATE	REVISION	BY
ACCEPTED		DATE	
[Signature]		1/28/26	
STRUCTURE B-05-660			
IH 41 NB TO STH 29 WB			
COUNTY	BROWN	VILLAGE	HOWARD
DESIGN SPEC.	REHABILITATION N/A		
DESIGNED BY	DESIGNED CK'D	RRH BY	PLANS CK'D
SEH	CK'D	RRH	SEH
POLYESTER POLYMER CONCRETE OVERLAY			SHEET 1 OF 2

▣ SUPERELEVATION TABLE

DESCRIPTION	STATION	SLOPE LEFT OF THE R/L	SLOPE RIGHT OF THE R/L
C/L BRG. SOUTH ABUT.	1074+01.15	3.1%	-3.1%
ADVERSE CROWN	1074+30.34	2.0%	-2.0%
NORMAL CROWN	1075+37.00	-2.0%	-2.0%
NORMAL CROWN	1076+85.34	-2.0%	-2.0%
ADVERSE CROWN	1077+92.00	-2.0%	2.0%
PC	1078+45.87	-4.0%	4.0%
BEGIN FULL SUPERELEVATION	1078+98.67	-6.0%	6.0%
END FULL SUPERELEVATION	1099+45.82	-6.0%	6.0%
ADVERSE CROWN	1100+52.49	-2.0%	2.0%
C/L BRG. WEST ABUT.	1100+79.15	-2.0%	1.0%

POSITIVE SUPERELEVATION MEANS THE EDGE OF THE DECK IS HIGHER THAN AT THE R/L WHEN LOOKING UPSTATION.



TYPICAL SECTION
(LOOKING UPSTATION)

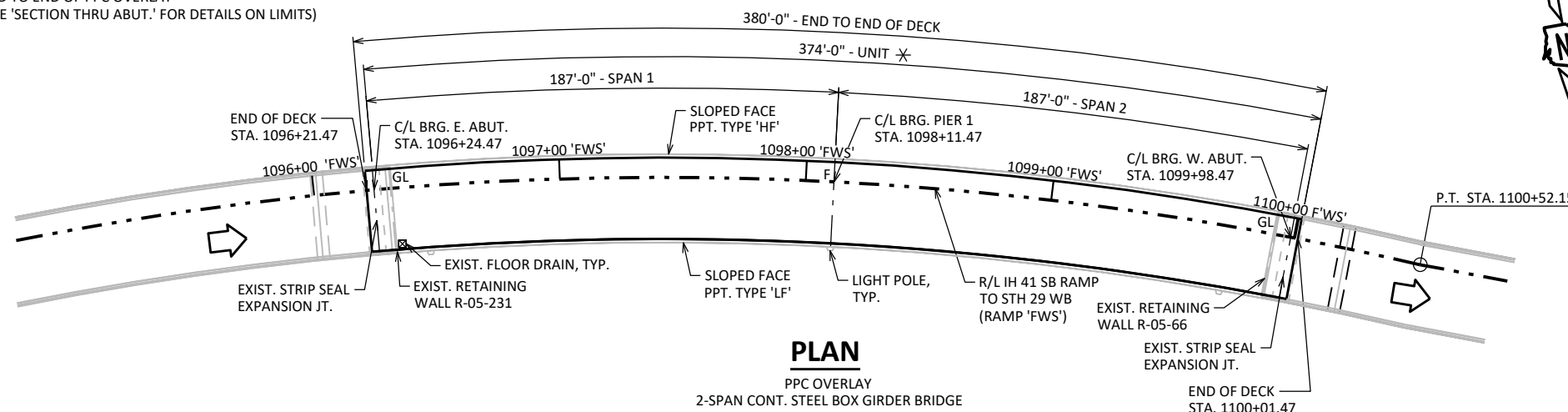
- REMOVE 3/4" OF EXISTING DECK PER BID ITEM 'CLEANING DECKS'
- ▣ SEE ROADWAY PLANS FOR STAGING DETAILS

TOTAL ESTIMATED QUANTITIES

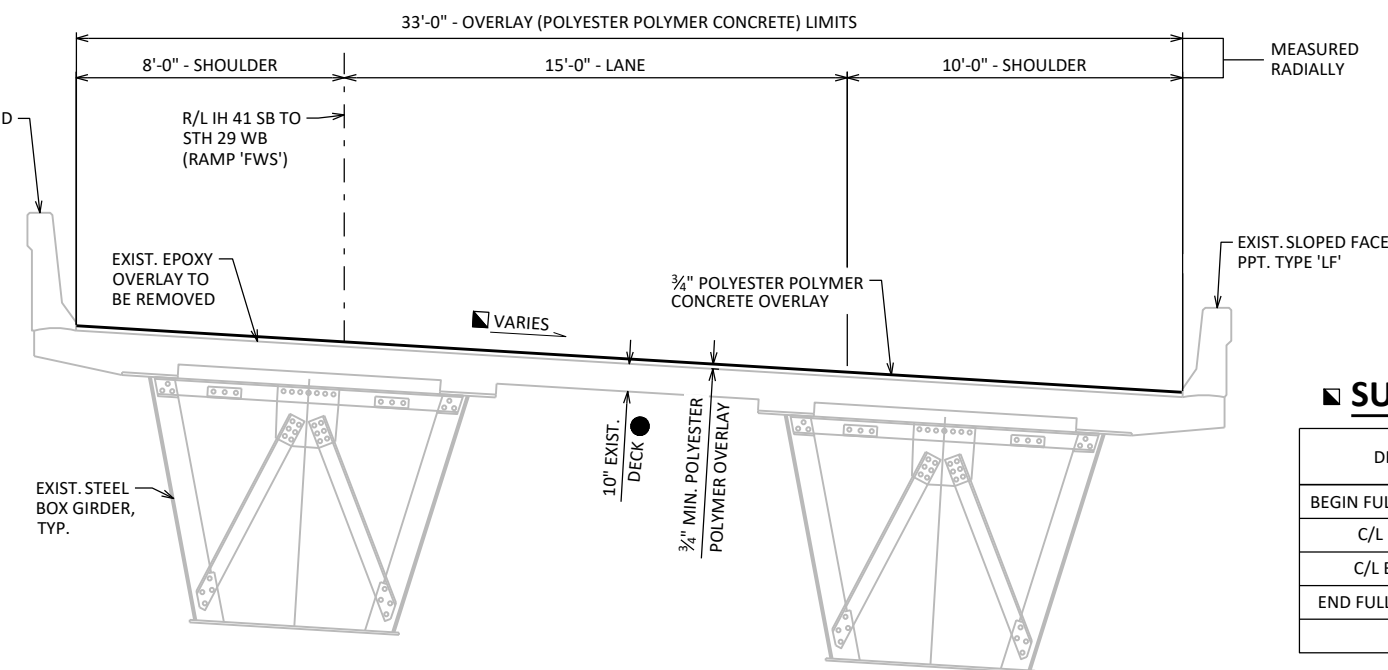
BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	1
509.0302	PREPARATION DECKS TYPE 2	SY	1
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	10
509.0500	CLEANING DECKS	SY	12,500
509.9015.S	REMOVING POLYMER OVERLAY STRUCTURE B-05-660	SY	12,500
SPV.0035	RAPID SET DECK REPAIR	CY	1
SPV.0180	POLYESTER POLYMER CONCRETE OVERLAY	SY	12,500

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE B-05-660			
DRAWN BY		SEH	PLANS CK'D DLM
CROSS SECTION & QUANTITIES			SHEET 2

F = FIXED BEARING
 GL = GUIDED LONGITUDINAL UNIDIRECTIONAL BEARING
 ☒ EXISTING FLOOR DRAIN
 ✱ END TO END OF PPC OVERLAY
 (SEE 'SECTION THRU ABUT.' FOR DETAILS ON LIMITS)



PLAN
 PPC OVERLAY
 2-SPAN CONT. STEEL BOX GIRDER BRIDGE



TYPICAL SECTION
 (LOOKING UPSTATION)

■ SUPERELEVATION TABLE

DESCRIPTION	STATION	SUPER ELEVATION
BEGIN FULL SUPERELEVATION	1083+00.43	-6.0%
C/L BRG. E. ABUT.	1096+24.47	-6.0%
C/L BRG. W. ABUT.	1099+08.47	-6.0%
END FULL SUPERELEVATION	1099+98.82	-6.0%
PT	1100+52.15	-4.0%

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HL-93
 INVENTORY RATING: RF = 0.89
 OPERATING RATING: RF = 1.16
 WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV): 250 (KIPS)

MATERIAL PROPERTIES:

CONCRETE MASONRY:
 RAPID SET DECK REPAIR $f'_c = 3,000$ PSI (AT 3 HOURS)

TRAFFIC DATA

RAMP 'FWS':
 ADTT = 4,200 (2035)
 R.D.S. = 70 MPH

SHAWANO:
 ADTT = 15,500 (2022)
 R.D.S. = 40 MPH

DOUSMAN:
 ADTT = 14,500 (2035)
 R.D.S. = 40 MPH

CURVE DATA

IH 41 SB RAMP TO STH 29 WB:
 P.I. = 1093+11.97
 N = 576333.17
 E = 83559.79
 $\Delta = 76^\circ 19' 33.09''$
 D = 4° 13' 42.49"
 T = 1064.86'
 L = 1805.05'
 R = 1355.00'
 S.E. = VARIES
 P.C. = 1082+47.10
 P.T. = 1100+52.15

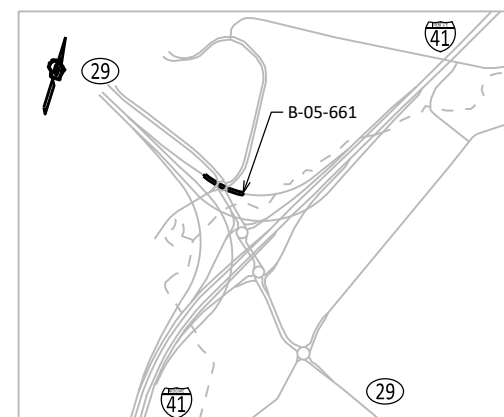
GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.
 DIMENSIONS AND STATIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
 3/4" -INCH OF CONCRETE SHALL BE REMOVED FROM THE ENTIRE BRIDGE DECK UNDER THE BID ITEM "CLEANING DECKS".
 AREAS OF "PREPARATION DECKS TYPE 1" SHALL BE DEFINED BY A SAW CUT.
 PREPARATION DECKS TYPE 1 AND PREPARATION DECKS TYPE 2 AREAS ARE BASED ON THE PLANS AND AS DETERMINED BY THE ENGINEER. DECK PREPARATION REPAIRS SHALL BE FILLED WITH "RAPID SET DECK REPAIR". POLYESTER POLYMER CONCRETE PATCHES MAY BE SUBSTITUTED AT NO EXTRA COST.
 DECK REPAIRS SHALL BE FILLED PRIOR TO OVERLAY PLACEMENT IF DECK REPAIRS ARE FILLED WITH "RAPID SET DECK REPAIR".
 SHOT BLASTING, OVERLAY PRIME COAT, DECK SURFACE PREPARATIONS, AND TRANSITIONAL AREAS ARE INCLUDED IN THE BID ITEM "POLYESTER POLYMER CONCRETE OVERLAY".
 OVERLAY CONSTRUCTION JOINTS SHALL BE APPROVED BY THE ENGINEER. AVOID PLACING LONGITUDINAL JOINTS NEAR WHEEL PATHS. WHEN REQUIRED, PLACE LONGITUDINAL JOINTS AT LANE LINES OR IN THE MIDDLE OF THE LANE. WHEEL PATHS DURING TEMPORARY TRAFFIC STAGING NEED NOT BE CONSIDERED.
 IF MINIMUM OVERLAY THICKNESS IS EXCEEDED BY MORE THAN 1/2", CONTACT THE STRUCTURES DESIGN SECTION.

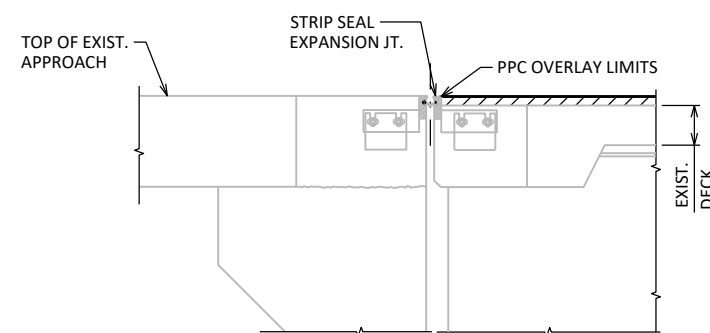
LIST OF DRAWINGS

1. POLYESTER POLYMER CONCRETE OVERLAY

STRUCTURE DESIGN CONTACTS:
 STACIE HAUSSEMAN 608-261-6109
 KYLE BUSCH 608-267-0465



PROJECT LOCATION



SECTION THRU ABUT.
 WEST ABUTMENT JOINT SHOWN, PPC LIMITS AT EAST ABUTMENT IS SIMILAR

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	1
509.0302	PREPARATION DECKS TYPE 2	SY	1
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	10
509.0500	CLEANING DECKS	SY	1,386
509.9015.S	REMOVING POLYMER OVERLAY STRUCTURE B-05-661	SY	1,386
SPV.0035	RAPID SET DECK REPAIR	CY	1
SPV.0180	POLYESTER POLYMER CONCRETE OVERLAY	SY	1,386

● REMOVE 3/4" OF EXISTING DECK PER BID ITEM 'CLEANING DECKS'

NO.	DATE	REVISION	BY
 ACCEPTED <i>[Signature]</i> KHB 1/28/26 CHIEF STRUCTURES DESIGN ENGINEER DATE			
STRUCTURE B-05-661			
IH 41 SB RAMP TO STH 29 WB (RAMP 'FSW')			
COUNTY	BROWN	VILLAGE	HOWARD
DESIGN SPEC.	REHABILITATION N/A		
DESIGNED BY	DESIGNED CK'D	RRH BY	PLANS CK'D
SEH	CK'D	RRH	SEH
POLYESTER POLYMER CONCRETE OVERLAY			SHEET 1 OF 1

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HL-93
 INVENTORY RATING: RF = 0.98
 OPERATING RATING: RF = 1.27
 WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV): 250 (KIPS)

MATERIAL PROPERTIES:

CONCRETE MASONRY:
 RAPID SET DECK REPAIR $f'_c = 3,000$ PSI (AT 3 HOURS)

TRAFFIC DATA

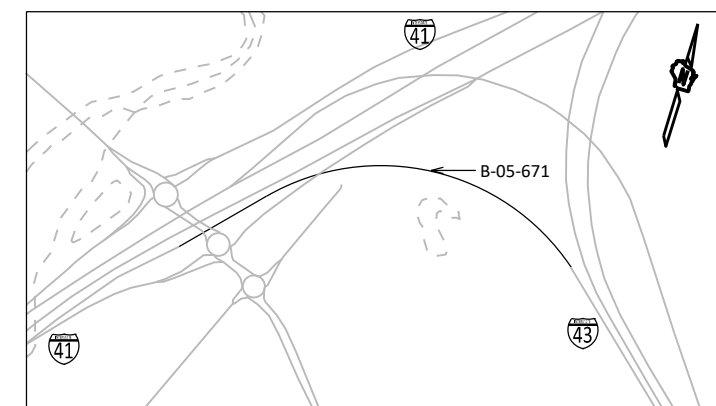
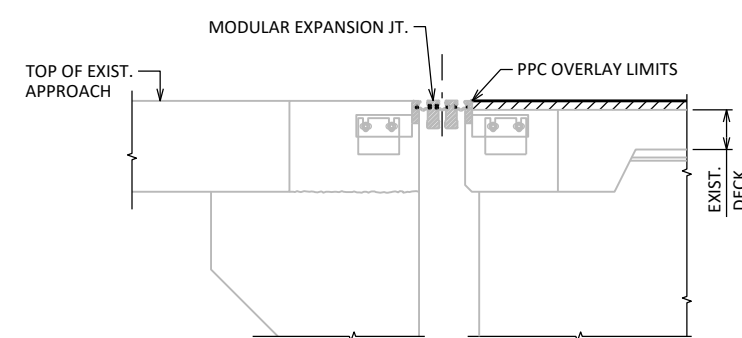
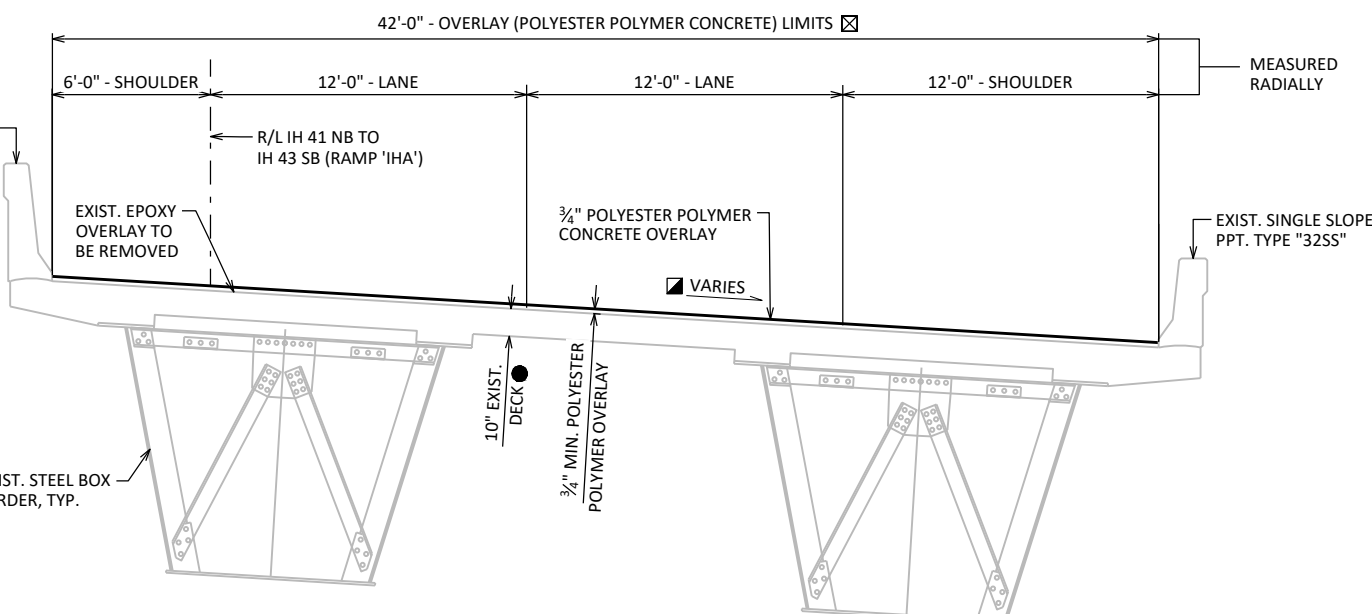
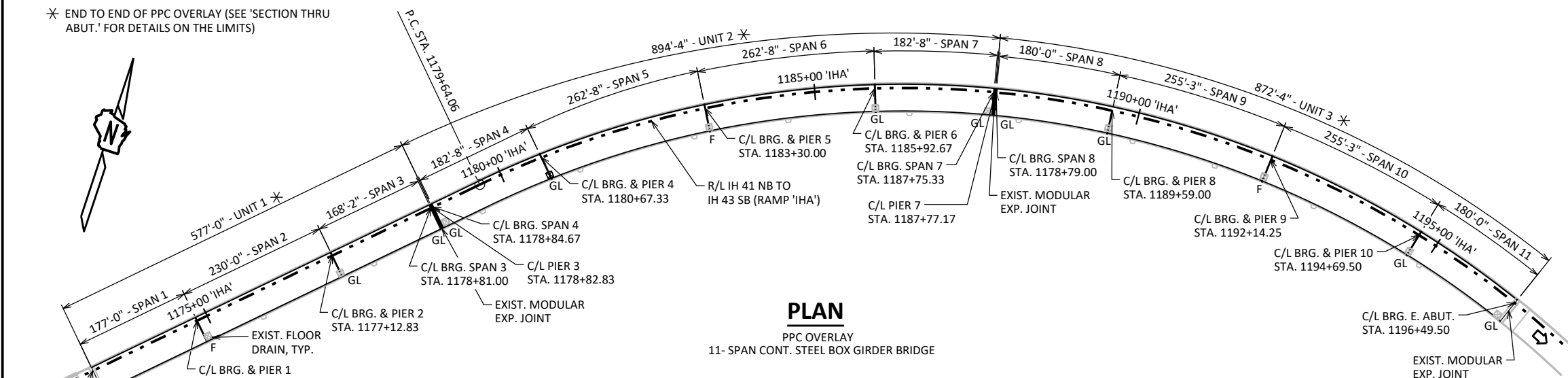
RAMP IHA:
 ADT = 11,700 (2035)
 R.D.S. = 70 MPH

VELP AVE./IH 141:
 ADT = 15,300 (2035)
 R.D.S. = 35 MPH

CURVE DATA

IH 41 NB RAMP TO IH 43 SB
 P.I. = 1193+68.84
 N = 583,934.76
 E = 89,660.93
 $\Delta = 86^\circ 14' 42''$
 D = 3° 49' 11"
 T = 1404.78'
 L = 2257.89'
 R = 1500.00'
 S.E. = VARIES
 P.C. = 1179+64.06
 P.T. = 1202+21.95

F = FIXED BEARING
 GL = GUIDED LONGITUDINAL UNIDIRECTIONAL BEARING
 ■ EXISTING FLOOR DRAIN
 ✱ END TO END OF PPC OVERLAY (SEE 'SECTION THRU ABUT.' FOR DETAILS ON THE LIMITS)



LIST OF DRAWINGS:

- 1. POLYESTER POLYMER CONCRETE OVERLAY

STRUCTURE DESIGN CONTACTS:
 STACIE HAUSSERMAN 608-261-6109
 KYLE BUSCH 608-267-0465

GENERAL NOTES

- DRAWINGS SHALL NOT BE SCALED.
- DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
- 3/4 - INCH OF CONCRETE SHALL BE REMOVED FROM THE ENTIRE BRIDGE DECK UNDER THE BID ITEM "CLEANING DECKS".
- AREAS OF "PREPARATION DECKS TYPE 1" SHALL BE DEFINED BY A SAW CUT.
- PREPARATION DECKS TYPE 1 AND PREPARATION DECKS TYPE 2 REPAIR AREAS ARE BASED ON THE PLANS AND AS DETERMINED BY THE ENGINEER. DECK PREPARATION REPAIRS SHALL BE FILLED WITH "RAPID SET DECK REPAIR". POLYESTER POLYMER CONCRETE PATCHES MAY BE SUBSTITUTED AT NO EXTRA COST.
- DECK REPAIRS SHALL BE FILLED PRIOR TO OVERLAY PLACEMENT IF DECK REPAIRS ARE FILLED WITH "RAPID SET DECK REPAIR".
- SHOT BLASTING, OVERLAY PRIME COAT, DECK SURFACE PREPARATIONS, AND TRANSITIONAL AREAS ARE INCLUDED IN THE BID ITEM "POLYESTER POLYMER CONCRETE OVERLAY".
- OVERLAY CONSTRUCTION JOINTS SHALL BE APPROVED BY THE ENGINEER. AVOID PLACING LONGITUDINAL JOINTS NEAR WHEEL PATHS. WHEN REQUIRED, PLACE LONGITUDINAL JOINTS AT LANE LINES OR IN THE MIDDLE OF THE LANE. WHEEL PATHS DURING TEMPORARY TRAFFIC STAGING NEED NOT BE CONSIDERED.
- IF MINIMUM OVERLAY THICKNESS IS EXCEEDED BY MORE THAN 1/2", CONTACT THE STRUCTURES DESIGN SECTION.

- REMOVE 3/4" OF EXISTING DECK PER BID ITEM "CLEANING DECKS"
- ☒ SEE ROADWAY PLANS FOR STAGING DETAILS

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	1
509.0302	PREPARATION DECKS TYPE 2	SY	1
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	10
509.0500	CLEANING DECKS	SY	10,859
509.9015.S	REMOVING POLYMER OVERLAY STRUCTURE B-05-671	SY	10,859
SPV.0035	RAPID SET DECK REPAIR	CY	1
SPV.0180	POLYESTER POLYMER CONCRETE OVERLAY	SY	10,859

☒ SUPERELEVATION TABLE

DESCRIPTION	STATION	SUPER ELEVATION
C/L BRG. SOUTH ABUT.	1173+05.83	-2.0%
BEGIN TRANSITION	1178+84.06	-2.0%
P.T.	1179+64.06	-4.0%
BEGIN FULL SUPERELEVATION	1180+43.00	-6.0%
C/L BRG. EAST ABUT.	1196+49.50	-6.0%

POSITIVE SUPERELEVATION MEANS THE RIGHT EDGE OF THE DECK IS HIGHER WHEN LOOKING UPSTATION.

NO.	DATE	REVISION	BY

BUREAU OF STRUCTURES
 ACCEPTED: *[Signature]* KHB 1/28/26
 CHIEF STRUCTURES DESIGN ENGINEER DATE

STRUCTURE B-05-671
 IH 41 NB RAMP TO IH 43 SB

COUNTY: BROWN VILLAGE: HOWARD

DESIGN SPEC. REHABILITATION N/A
 DESIGNED BY: SEH CK'D: RRH DRAWN BY: SEH PLANS CK'D: DLM

POLYESTER POLYMER CONCRETE OVERLAY SHEET 1 OF 1

LEGEND

- F - FIXED BEARING
- E - ELASTOMERIC EXPANSION BEARING
- GL - GUIDED LONGITUDINAL UNIDIRECTIONAL BEARING
- ☒ - EXIST. FLOOR DRAIN

STATE PROJECT NUMBER

1130-73-71

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HL-93
 INVENTORY RATING: RF = 0.90
 OPERATING RATING: RF = 1.17
 WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV): 250 (KIPS)

MATERIAL PROPERTIES:

CONCRETE MASONRY:
 RAPID SET DECK REPAIR — $f'_c = 3,000$ PSI (AT 3 HOURS)

TRAFFIC DATA

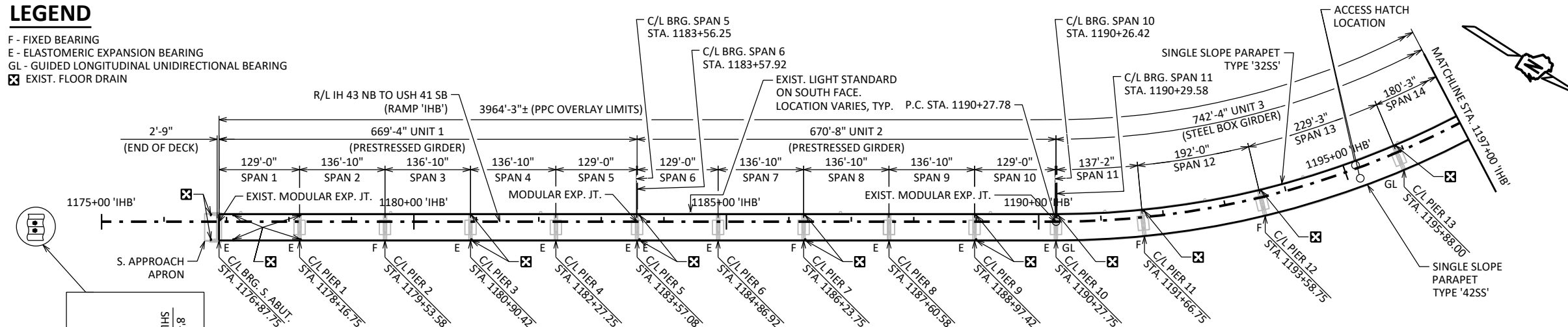
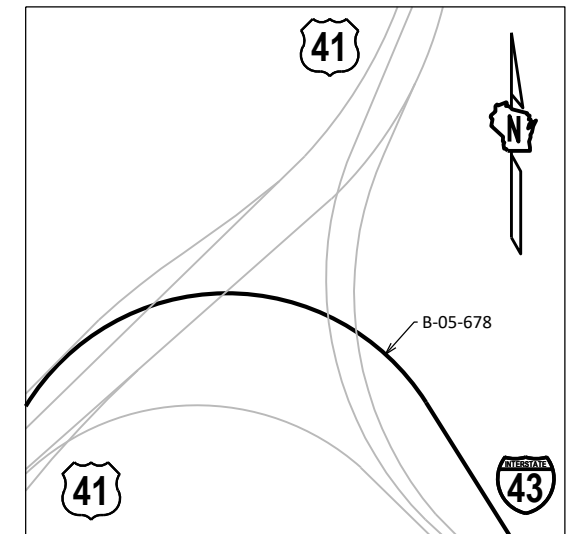
IH 43 NB TO USH 41 SB (RAMP 'IHB')

ADT = 13,000 (2035)
 R.D.S. = 70 MPH

USH 41

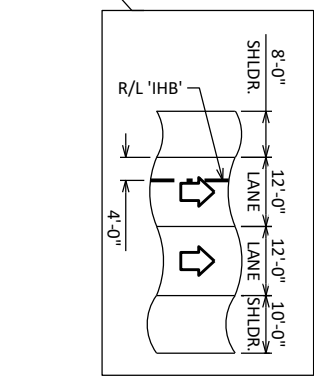
ADT = 56,600 (2035)
 R.D.S. = 70 MPH

LOCATION MAP

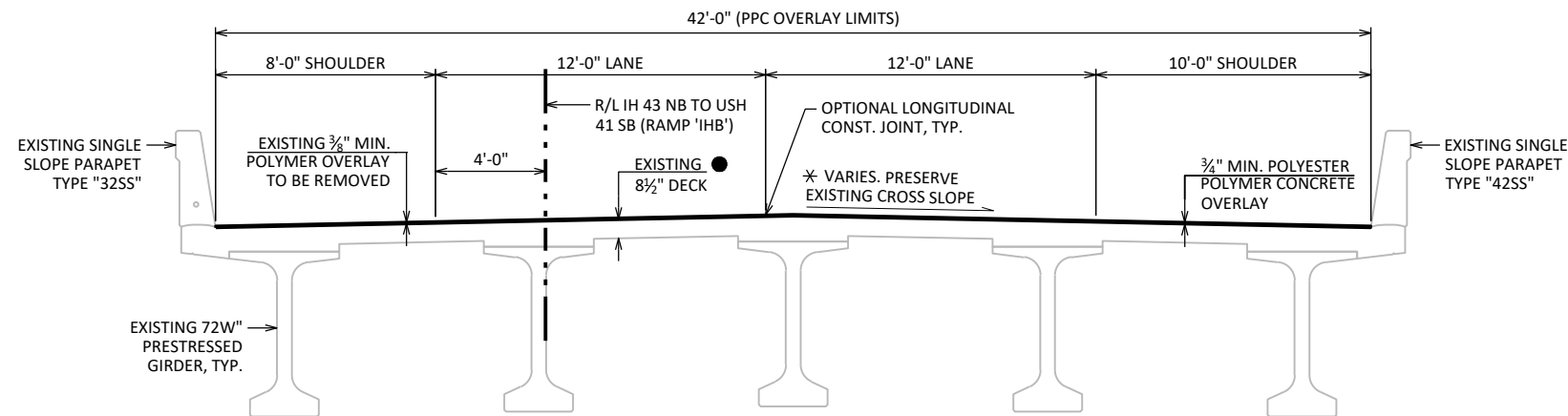


PLAN

PPC OVERLAY
 10-SPAN 72W" PRESTRESSED GIRDER AND
 13-SPAN CONTINUOUS STEEL BOX GIRDER BRIDGE



* SEE 'DECK SLOPE TRANSITION' DETAIL ON 'POLYESTER POLYMER CONCRETE OVERLAY 2 OF 2' SHEET.



CROSS SECTION THRU ROADWAY - UNITS 1 THRU 2

LOOKING UPSTATION

● REMOVE 3/4" OF EXISTING DECK PER BID ITEM "CLEANING DECKS"

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.

3/4" -INCH OF CONCRETE SHALL BE REMOVED FROM THE ENTIRE BRIDGE DECK UNDER THE BID ITEM "CLEANING DECKS".

AREAS OF "PREPARATION DECKS TYPE 1" SHALL BE DEFINED BY A SAW CUT.

PREPARATION DECKS TYPE 1 AND PREPARATION DECKS TYPE 2 AREAS ARE BASED ON THE PLANS AND AS DETERMINED BY THE ENGINEER. DECK PREPARATION SHALL BE FILLED WITH "RAPID SET DECK REPAIR". POLYESTER POLYMER CONCRETE PATCHES MAY BE SUBSTITUTED AT NO EXTRA COST.

DECK REPAIRS SHALL BE FILLED PRIOR TO OVERLAY PLACEMENT IF DECK REPAIRS ARE FILLED WITH "RAPID SET DECK REPAIR".

SHOT BLASTING, OVERLAY PRIME COAT, AND DECK SURFACE PREPARATIONS ARE INCLUDED IN THE BID ITEM "POLYESTER POLYMER CONCRETE OVERLAY".

OVERLAY CONSTRUCTION JOINTS SHALL BE APPROVED BY THE ENGINEER. AVOID PLACING LONGITUDINAL JOINTS NEAR WHEEL PATHS. WHEN REQUIRED, PLACE LONGITUDINAL JOINTS AT LANE LINES OR IN THE MIDDLE OF THE LANE. WHEEL PATHS DURING TEMPORARY TRAFFIC STAGING NEED NOT BE CONSIDERED. SEE ROADWAY PLANS FOR STAGING.

IF MINIMUM OVERLAY THICKNESS IS EXCEEDED BY MORE THAN 1/2", CONTACT THE STRUCTURES DESIGN SECTION.

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	1
509.0302	PREPARATION DECKS TYPE 2	SY	1
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	10
509.0500	CLEANING DECKS	SY	18,580
509.9015.S	REMOVING POLYMER OVERLAY STRUCTURE B-05-678	SY	18,580
SPV.0035	RAPID SET DECK REPAIR	CY	1
SPV.0180	POLYESTER POLYMER CONCRETE OVERLAY	SY	18,580

STRUCTURE DESIGN CONTACTS:

RYAN R. HARRIS 608-266-2963
 KYLE H. BUSCH 608-267-0465

NO.	DATE	REVISION	BY
 BUREAU OF STRUCTURES ACCEPTED: <i>[Signature]</i> KHB 1/28/26 CHIEF STRUCTURES DESIGN ENGINEER DATE			

STRUCTURE B-05-678			
IH 43 NB TO USH 41 SB (RAMP 'IHB')			
COUNTY	BROWN	VILLAGE	HOWARD
DESIGN SPEC.	REHABILITATION N/A		
DESIGNED BY	RRH	DESIGNED CK'D	SEH
DRAWN BY	RRH	PLANS CK'D	DLM
POLYESTER POLYMER CONCRETE OVERLAY			SHEET 1 OF 2
1 OF 2			

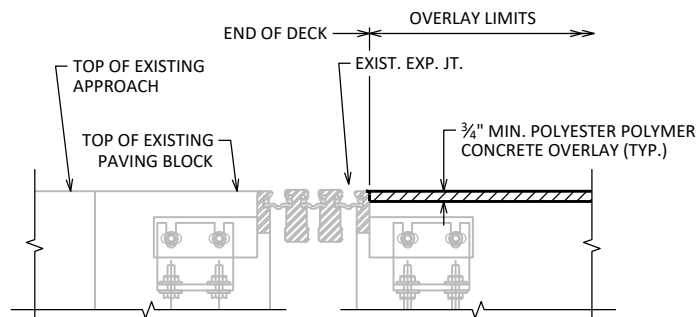
HORIZONTAL CURVE DATA

IH 43 NB TO USH 41 NB RAMP 'IHB'

PC = 1190+27.78
 PI = 1211+36.20
 PT = 1217+84.77
 R = 1400.00'
 DELTA = 112°49'53"
 D = 4°5'33"
 L = 2756.99'

LIST OF DRAWINGS:

1. POLYESTER POLYMER CONCRETE OVERLAY 1 OF 2
2. POLYESTER POLYMER CONCRETE OVERLAY 2 OF 2



SECTION THRU ABUTMENT

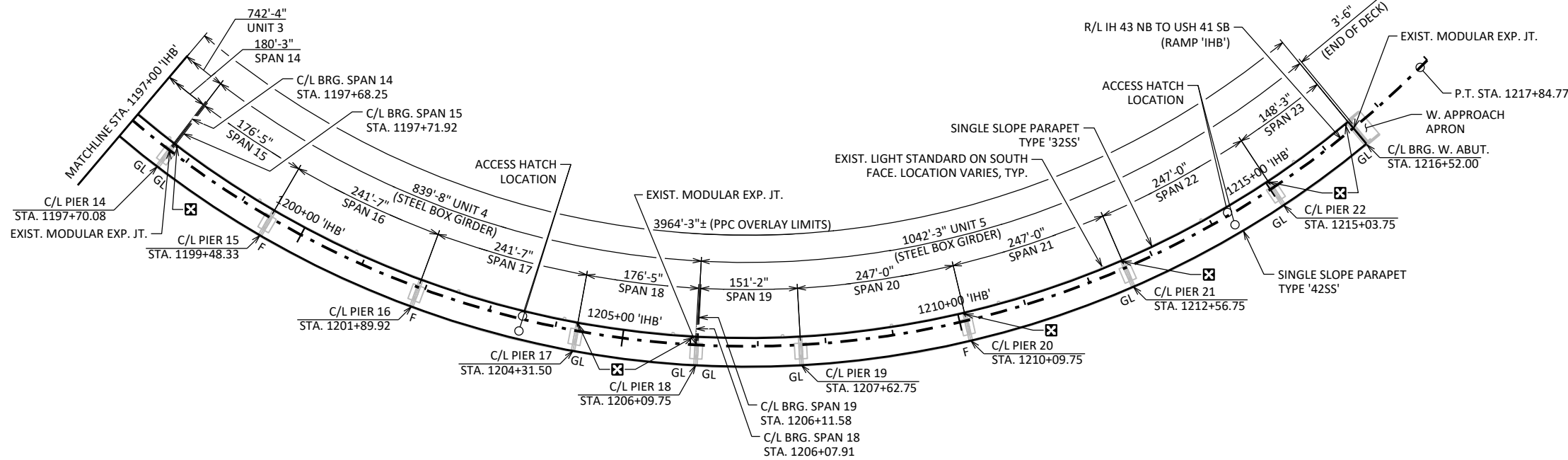
SOUTH ABUTMENT SHOWN
 WEST ABUTMENT AND INTERMEDIATE EXPANSION JOINTS SIMILAR

LEGEND

- F - FIXED BEARING
- E - ELASTOMERIC EXPANSION BEARING
- GL - GUIDED LONGITUDINAL UNIDIRECTIONAL BEARING
- ☒ EXIST. FLOOR DRAIN

STATE PROJECT NUMBER

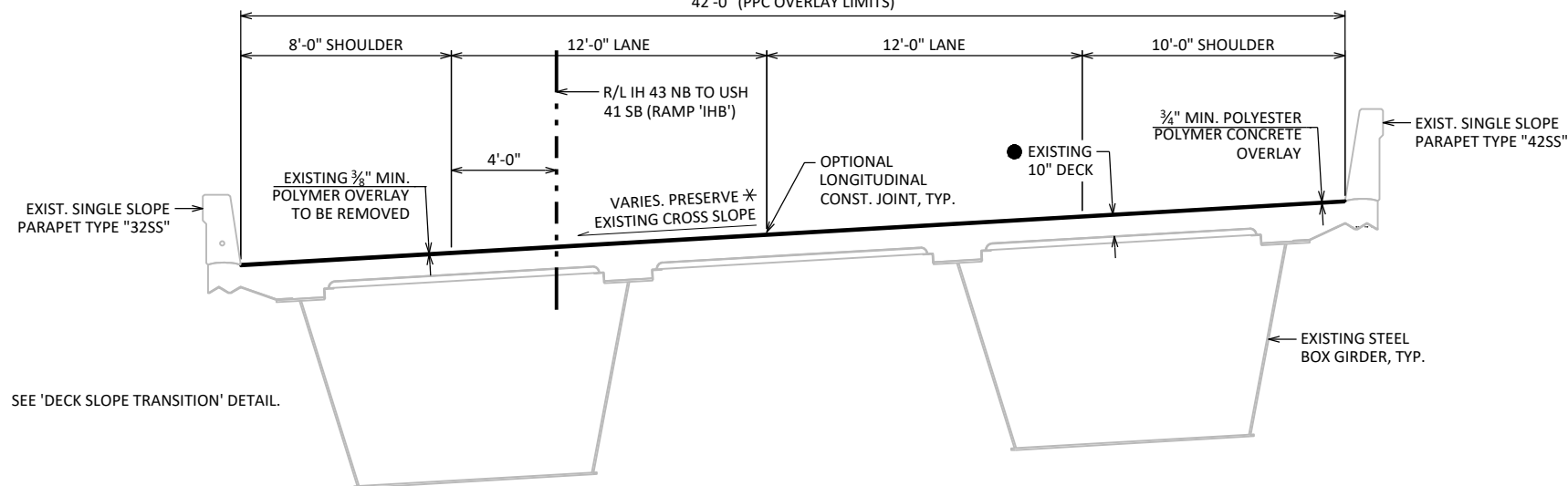
1130-73-71



PLAN

PPC OVERLAY
10-SPAN 72W" PRESTRESSED GIRDER AND
13-SPAN CONTINUOUS STEEL BOX GIRDER BRIDGE

42'-0" (PPC OVERLAY LIMITS)

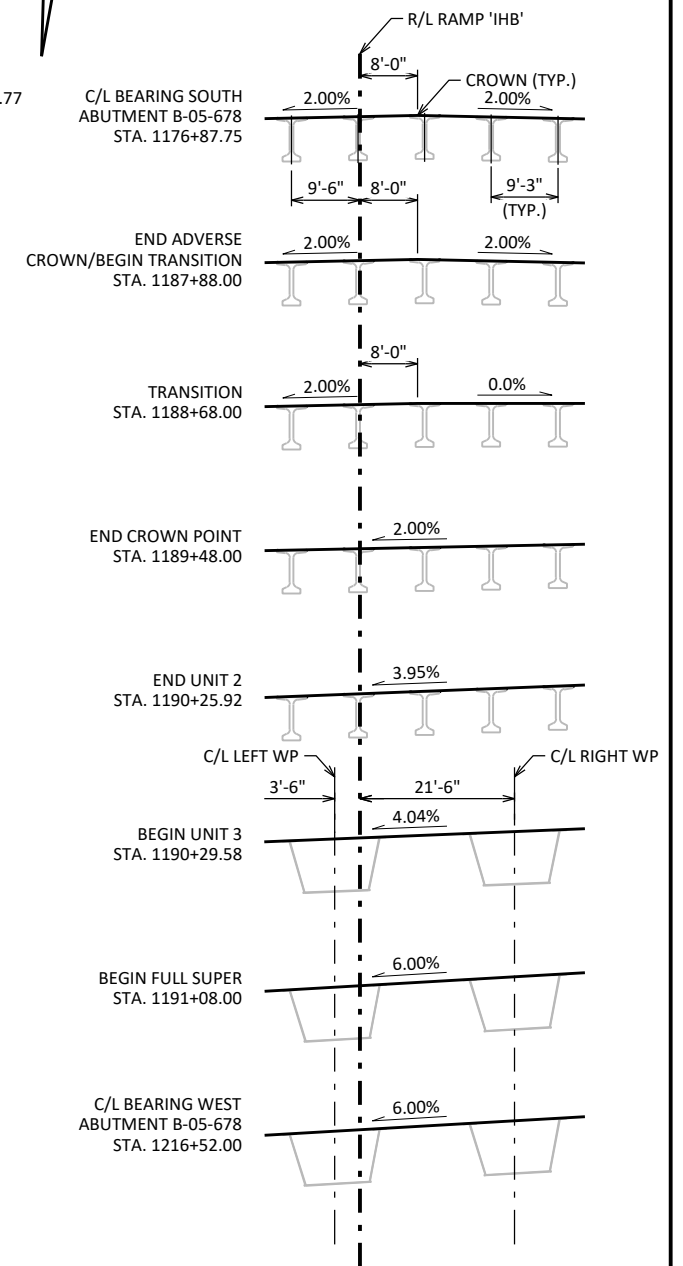


**CROSS SECTION THRU
ROADWAY - UNITS 3 THRU 5**

LOOKING UPSTATION

● REMOVE 3/4" OF EXISTING DECK PER BID ITEM "CLEANING DECKS"

* SEE 'DECK SLOPE TRANSITION' DETAIL.



DECK SLOPE TRANSITION

LOOKING UPSTATION

8

8

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE B-05-678			
DRAWN BY		RRH	PLANS CK'D DLM
POLYESTER POLYMER CONCRETE OVERLAY			SHEET 2
2 OF 2			

LEGEND

F - FIXED BEARING
GL - GUIDED LONGITUDINAL UNIDIRECTIONAL BEARING

STATE PROJECT NUMBER

1130-73-71

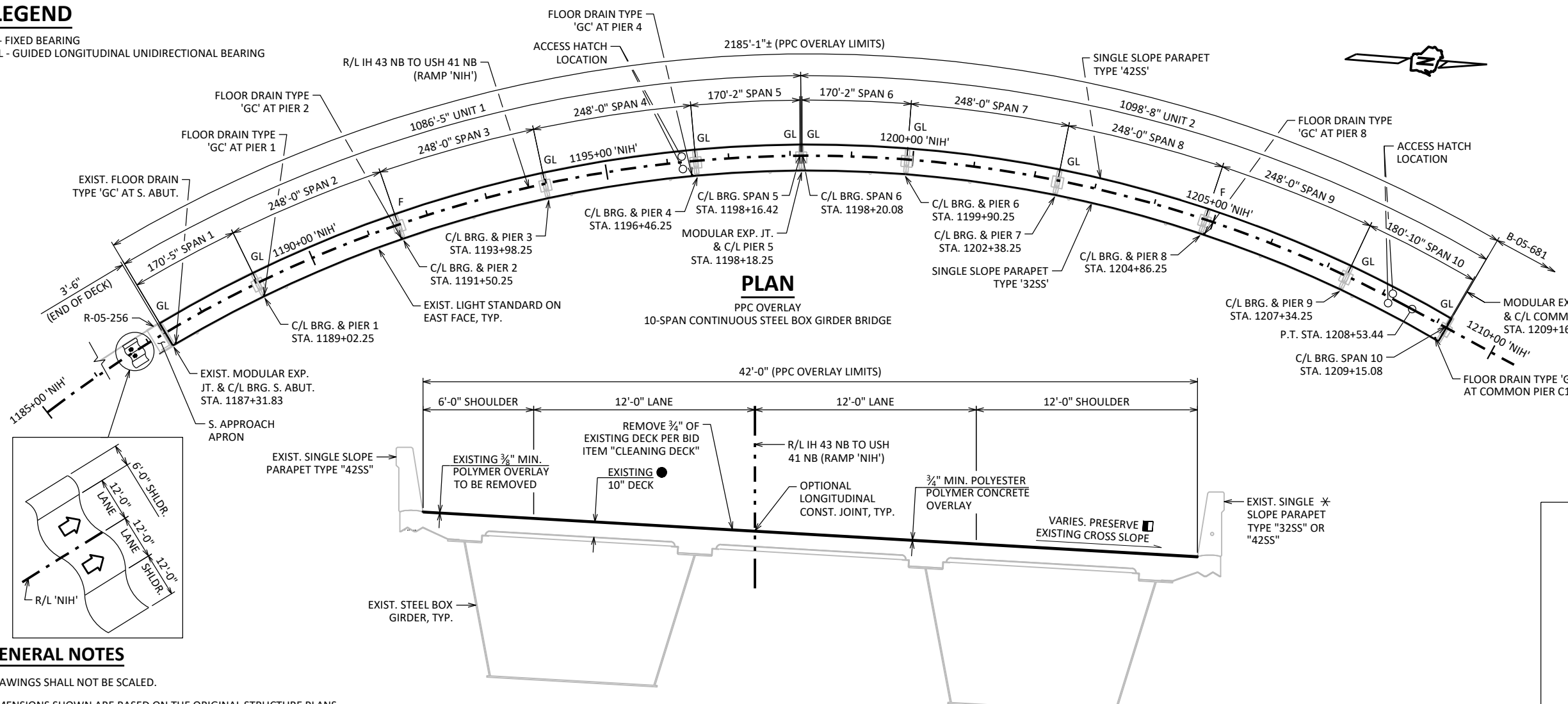
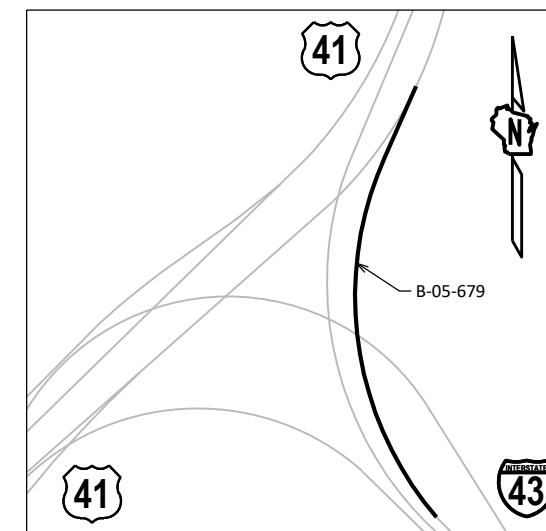
DESIGN DATA

LIVE LOAD:
DESIGN LOADING: HL-93
INVENTORY RATING: RF = 1.02
OPERATING RATING: RF = 1.32
WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV): 250 (KIPS)
MATERIAL PROPERTIES:
CONCRETE MASONRY: RAPID SET DECK REPAIR — $f'_c = 3,000$ PSI (AT 3 HOURS)

TRAFFIC DATA

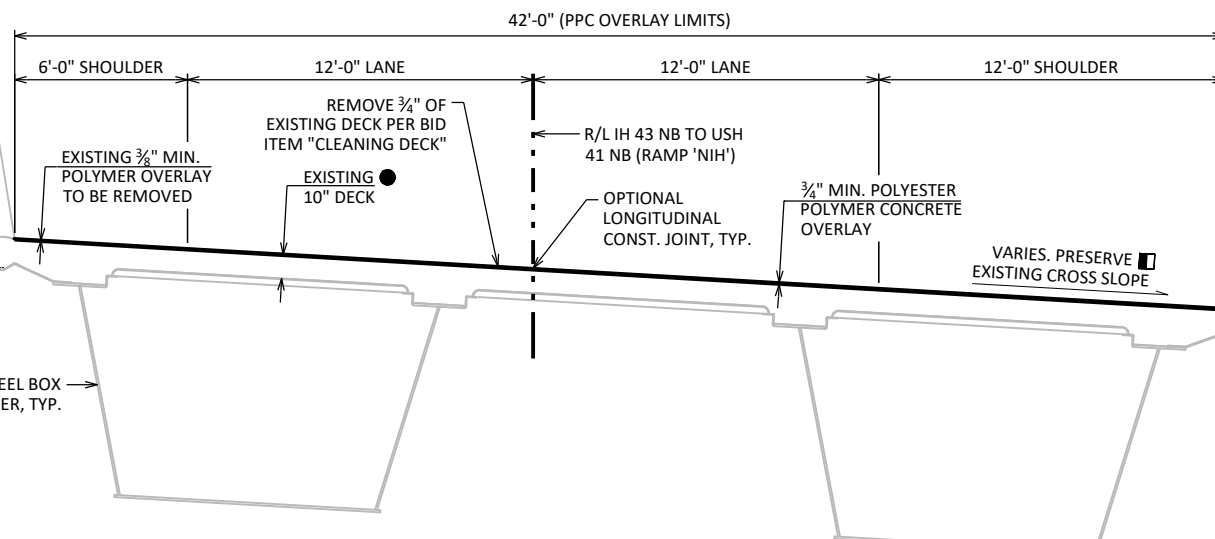
IH 43 NB TO USH 41 NB (RAMP 'NIH')
ADT = 9,700 (2035)
R.D.S. = 70 MPH

LOCATION MAP



PLAN

PPC OVERLAY
10-SPAN CONTINUOUS STEEL BOX GIRDER BRIDGE



CROSS SECTION THRU ROADWAY

LOOKING UPSTATION
* NOTE: PARAPET TRANSITION FROM STA. 1209+01.17 TO STA. 1209+11.17
● REMOVE 3/4" OF EXISTING DECK PER BID ITEM "CLEANING DECKS"

DECK SLOPE TRANSITION

LOCATION	DESCRIPTION
C/L BEARING SOUTH ABUTMENT B-05-679 STA. 1187+31.83	6.0% SUPERELEVATION
STA. 1207+63	END FULL 6.0% SUPERELEVATION
PT STA. 1208+53	4.0%
C/L BEARING UNIT 2 PIER C1 STA. 1209+15.08	2.62%

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
3/4" -INCH OF CONCRETE SHALL BE REMOVED FROM THE ENTIRE BRIDGE DECK UNDER THE BID ITEM "CLEANING DECKS".
AREAS OF "PREPARATION DECKS TYPE 1" SHALL BE DEFINED BY A SAW CUT.
PREPARATION DECKS TYPE 1 AND PREPARATION DECKS TYPE 2 AREAS ARE BASED ON THE PLANS AND AS DETERMINED BY THE ENGINEER. DECK PREPARATION SHALL BE FILLED WITH "RAPID SET DECK REPAIR". POLYESTER POLYMER CONCRETE PATCHES MAY BE SUBSTITUTED AT NO EXTRA COST.
DECK REPAIRS SHALL BE FILLED PRIOR TO OVERLAY PLACEMENT IF DECK REPAIRS ARE FILLED WITH "RAPID SET DECK REPAIR".
SHOT BLASTING, OVERLAY PRIME COAT, AND DECK SURFACE PREPARATIONS ARE INCLUDED IN THE BID ITEM "POLYESTER POLYMER CONCRETE OVERLAY".
OVERLAY CONSTRUCTION JOINTS SHALL BE APPROVED BY THE ENGINEER. AVOID PLACING LONGITUDINAL JOINTS NEAR WHEEL PATHS. WHEN REQUIRED, PLACE LONGITUDINAL JOINTS AT LANE LINES OR IN THE MIDDLE OF THE LANE. WHEEL PATHS DURING TEMPORARY TRAFFIC STAGING NEED NOT BE CONSIDERED. SEE ROADWAY PLANS FOR STAGING.
IF MINIMUM OVERLAY THICKNESS IS EXCEEDED BY MORE THAN 1/2", CONTACT THE STRUCTURES DESIGN SECTION.

TOTAL ESTIMATED QUANTITIES

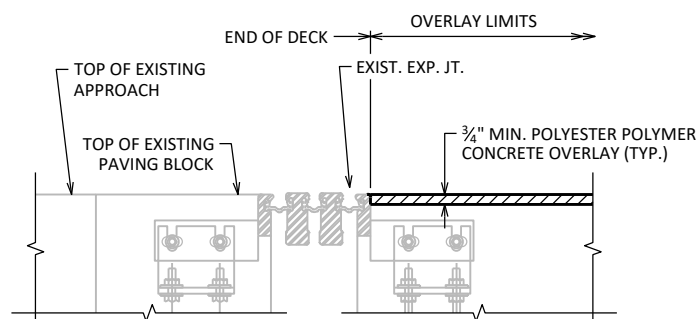
BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	1
509.0302	PREPARATION DECKS TYPE 2	SY	1
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	10
509.0500	CLEANING DECKS	SY	10,200
509.9015.S	REMOVING POLYMER OVERLAY STRUCTURE B-05-679	SY	10,200
SPV.0035	RAPID SET DECK REPAIR	CY	1
SPV.0180	POLYESTER POLYMER CONCRETE OVERLAY	SY	10,200

LIST OF DRAWINGS:

- POLYESTER POLYMER CONCRETE OVERLAY

HORIZONTAL CURVE DATA

IH 43 NB TO USH 41 SB RAMP 'IHB'
PC = 1183+49.38
PI = 1197+84.44
PT = 1208+53.44
R = 2050.00'
DELTA = 69°59'11"
D = 5°35'23"
L = 2504.07'



SECTION THRU ABUTMENT

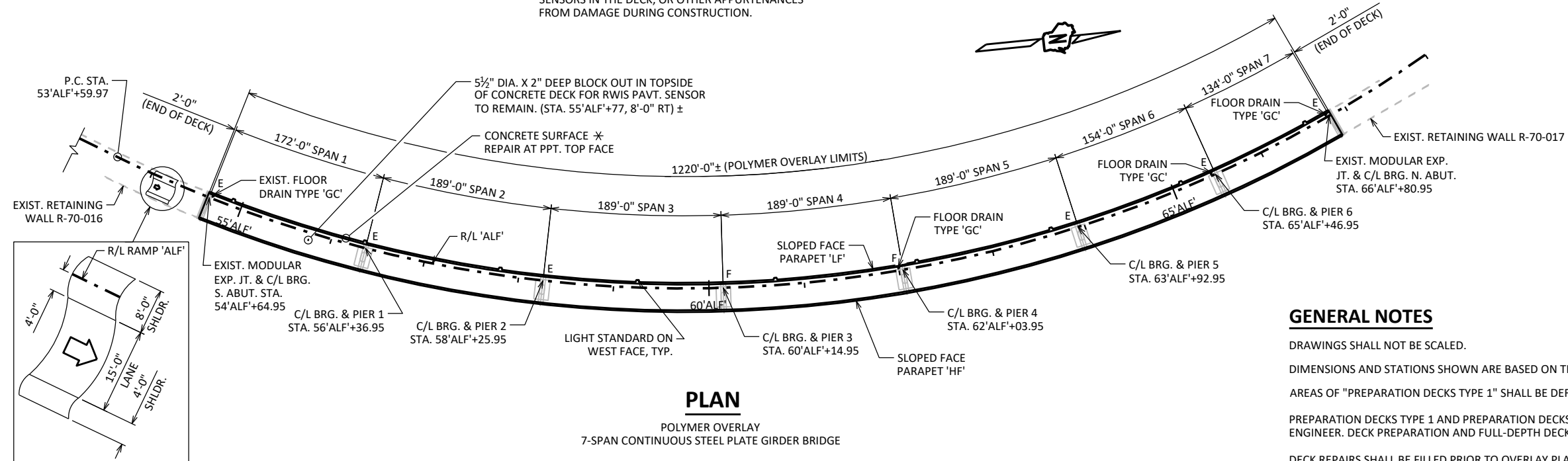
SOUTH ABUTMENT SHOWN
NORTH ABUTMENT AND INTERMEDIATE EXPANSION JOINTS SIMILAR

STRUCTURE DESIGN CONTACTS:

RYAN R. HARRIS 608-266-2963
KYLE H. BUSCH 608-267-0465

NO.	DATE	REVISION	BY
 BUREAU OF STRUCTURES ACCEPTED <i>[Signature]</i> 1/28/26 CHIEF STRUCTURES DESIGN ENGINEER DATE			
STRUCTURE B-05-679			
IH 43 NB TO USH 41 NB (RAMP 'NIH')			
COUNTY	BROWN	VILLAGE	HOWARD
DESIGN SPEC.	REHABILITATION N/A		
DESIGNED BY	DESIGNED CK'D	DRAWN SEH BY	PLANS CK'D
RRH	CK'D	RRH	DLM
POLYESTER POLYMER CONCRETE OVERLAY			SHEET 1 OF 1

NOTE: PROTECT DRAINS, EXPANSION JOINTS, ACCESS HATCHES, SENSORS IN THE DECK, OR OTHER APPURTENANCES FROM DAMAGE DURING CONSTRUCTION.



PLAN
POLYMER OVERLAY
7-SPAN CONTINUOUS STEEL PLATE GIRDER BRIDGE

DESIGN DATA

LIVE LOAD:
 DESIGN LOADING: HL-93
 INVENTORY RATING: RF = 1.07
 OPERATING RATING: RF = 1.39
 WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV): 250 (KIPS)

MATERIAL PROPERTIES:
 CONCRETE MASONRY:
 RAPID SET DECK REPAIR — $f'_c = 3,000$ PSI (AT 3 HOURS)
 CONCRETE SURFACE REPAIR — $f'_c = 4,000$ PSI

TRAFFIC DATA

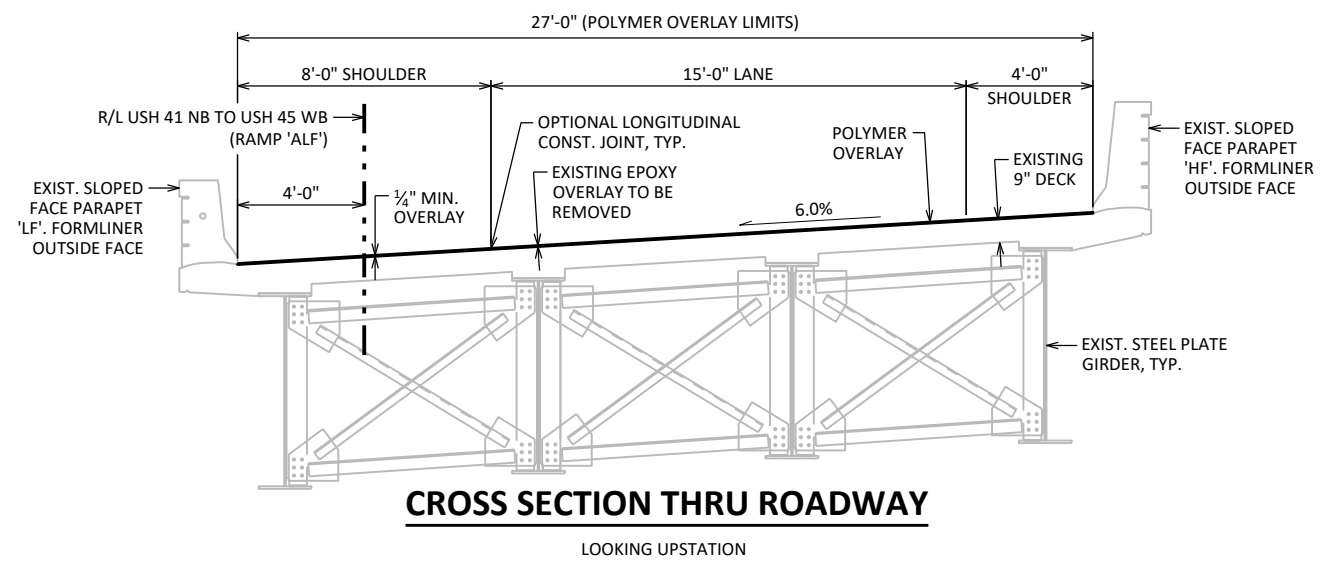
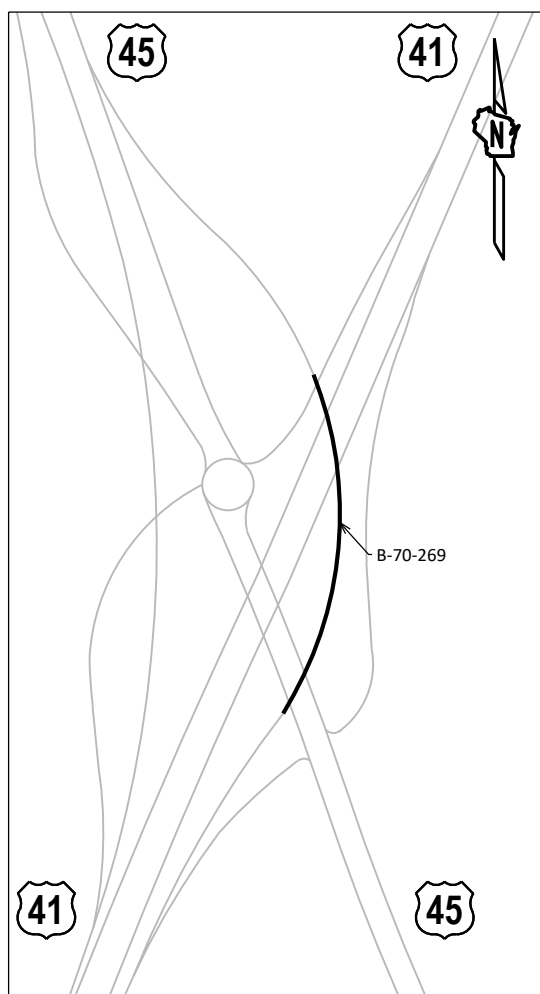
USH 41 NB TO USH 45 NB (RAMP 'ALF')
 ADT = 7,800 (2035)
 R.D.S. = 70 MPH

USH 41/USH 45
 ADT = 59,900 (2035)
 R.D.S. = 70 MPH

GENERAL NOTES

- DRAWINGS SHALL NOT BE SCALED.
- DIMENSIONS AND STATIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
- AREAS OF "PREPARATION DECKS TYPE 1" SHALL BE DEFINED BY A SAW CUT.
- PREPARATION DECKS TYPE 1 AND PREPARATION DECKS TYPE 2 AREAS ARE BASED ON THE PLANS AND AS DETERMINED BY THE ENGINEER. DECK PREPARATION AND FULL-DEPTH DECK REPAIRS SHALL BE FILLED WITH "RAPID SET DECK REPAIR".
- DECK REPAIRS SHALL BE FILLED PRIOR TO OVERLAY PLACEMENT.
- SHOT BLASTING, OVERLAY PRIME COAT, DECK SURFACE PREPARATIONS, AND TRANSITIONAL AREAS ARE INCLUDED IN THE BID ITEM "POLYMER OVERLAY".
- * CONCRETE SURFACE REPAIR ANTICIPATED AT TOP OF WEST PARAPET. EXTENTS AND LOCATIONS SHALL BE DETERMINED BY THE FIELD ENGINEER.

LOCATION MAP



CROSS SECTION THRU ROADWAY
LOOKING UPSTATION

HORIZONTAL CURVE DATA

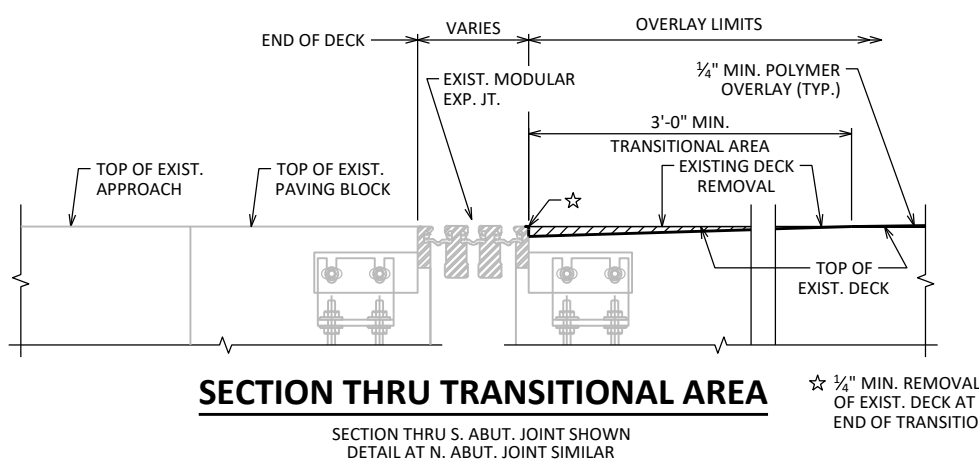
USH 41 NB TO USH 45 WB RAMP 'ALF'
 PC = 53'ALF'+59.97
 PI = 64'ALF'+89.67
 PT = 72'ALF'+41.30
 R = 1350.00'
 DELTA = 79°50'45.9"
 D = 4°14'38.9"
 L = 1881.33'

LIST OF DRAWINGS:

- 1. POLYMER OVERLAY

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	1
509.0302	PREPARATION DECKS TYPE 2	SY	1
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	10
509.1500	CONCRETE SURFACE REPAIR	SF	1
509.5100.S	POLYMER OVERLAY	SY	3685
509.9015.S	REMOVING POLYMER OVERLAY STRUCTURE B-70-269	SY	3685
SPV.0035	RAPID SET DECK REPAIR	CY	1



SECTION THRU TRANSITIONAL AREA

SECTION THRU S. ABUT. JOINT SHOWN
 DETAIL AT N. ABUT. JOINT SIMILAR

STRUCTURE DESIGN CONTACTS:
 RYAN R. HARRIS 608-266-2963
 KYLE H. BUSCH 608-267-0465

NO.	DATE	REVISION	BY

BUREAU OF STRUCTURES
 ACCEPTED *[Signature]* 1/28/26
 CHIEF STRUCTURES DESIGN ENGINEER DATE

STRUCTURE B-70-269
 R/L USH 41 NB TO USH 45 WB (RAMP 'ALF')

COUNTY WINNEBAGO CITY OSHKOSH

DESIGN SPEC. REHABILITATION N/A
 DESIGNED BY RRH CK'D SEH DRAWN BY RRH PLANS CK'D DLM

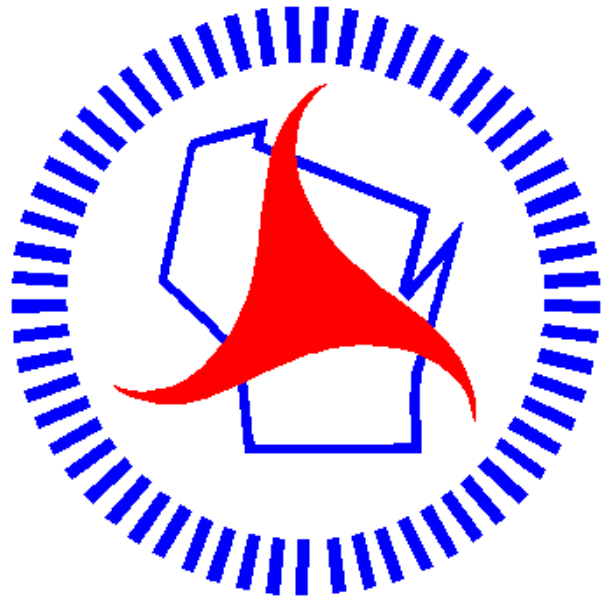
POLYMER OVERLAY

SHEET 1 OF 1

8

8

SCALE =



Wisconsin Department of Transportation

Dedicated people creating transportation solutions through innovation and exceptional service.

<http://www.dot.wisconsin.gov>