

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
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Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 44

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

T ARCADIA, MIDDLE ROAD

MYERS VALLEY CREEK BRIDGE B-61-0294

LOC STR

TREMPEALEAU COUNTY

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
7276-00-76		

STATE PROJECT NUMBER
7276-00-76

END PROJECT
STA. 11+40

STRUCTURE B-61-0294

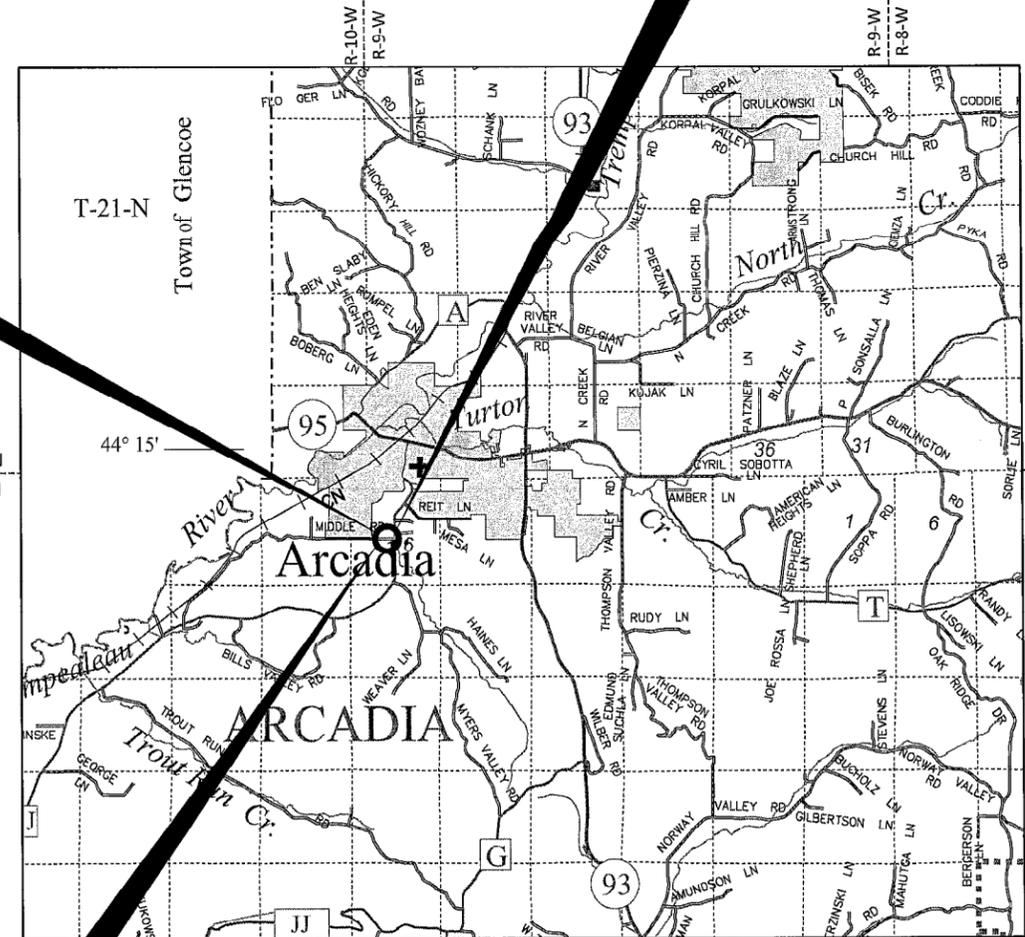
BEGIN PROJECT
STA. 10+00

Y = 392,308.64
X = 806,852.04

LAYOUT
SCALE 0 2 MI

TOTAL NET LENGTH OF CENTERLINE = 0.027 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), TREMPEALEAU COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.



DESIGN DESIGNATION 7276-00-06

A.A.D.T.	2026	=	180
A.A.D.T.	2046	=	270
D.H.V.		=	24
D.D.		=	50/50
T.		=	20.0% (ESTIMATED)
DESIGN SPEED		=	40 MPH
ESALS		=	110,000

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

ACCEPTED FOR
COUNTY of TREMPEALEAU
10-21-25 *Neil Olson*
(Date) (Highway Commissioner)

ORIGINAL PLANS PREPARED BY

JEWELL



STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY	JEWELL ASSOCIATES ENGINEERS, INC
Surveyor	JEWELL ASSOCIATES ENGINEERS, INC
Designer	JEWELL ASSOCIATES ENGINEERS, INC
Project Manager	TOU YANG, P.E.
Regional Examiner	NW REGION
Regional Supervisor	TOU YANG, P.E.

APPROVED FOR THE DEPARTMENT
10/28/2025
DATE: *[Signature]*
(Signature)

E

PROJECT ID: 7276-00-76

COUNTY: TREMPEALEAU

25

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLAN ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

NO TREES OR SHRUBS ARE TO BE REMOVED UNLESS SUCH TREES OR SHRUBS HAVE FIRST BEEN INDICATED FOR REMOVAL BY THE ENGINEER IN THE FIELD.

UNLESS SHOWN OTHERWISE, DISTURBED AREAS SHOWN WITHIN THE RIGHT-OF-WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS ARE TO BE FERTILIZED (TYPE B), SEEDED (USE SEED MIX NO. 20 & SEED MIX NO. 60) AS DIRECTED BY THE ENGINEER.

WHEN THE QUANTITY OF THE ITEM OF BASE AGGREGATE DENSE OR ASPHALTIC SURFACE IS MEASURED FOR PAYMENT BY THE TON, THE DEPTH OR THICKNESS OF THE COURSE SHOWN ON THE PLANS IS APPROXIMATE, AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF MATERIAL AS DIRECTED BY THE ENGINEER IN THE FIELD.

SILT FENCE SHALL BE PLACED AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER IN THE FIELD. SILT FENCE SHALL BE PLACED PRIOR TO CONSTRUCTION AND SHALL BE IN PLACE PRIOR TO STRUCTURE REMOVAL.

WETLANDS ARE PRESENT IN THE PROJECT LIMITS. THE CONTRACTOR SHALL NOT OPERATE EQUIPMENT OR STOCKPILE MATERIALS BEYOND THE EXISTING SLOPE INTERCEPT FROM STA. 9+50 TO STA. 11+90.

THE LOCATION OF ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD PRIOR TO PLACEMENT.

ASPHALTIC SURFACE QUANTITIES WERE CALCULATED USING 112 LB/SY/IN.

CURVE DATA IS BASED ON THE ARC DEFINITIONS.

THE DEPARTMENT OF TRANSPORTATION WILL FURNISH THE CONTRACTOR WITH A MONUMENT TO BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER.

CONTACTS

WISDOT:

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UTILITIES

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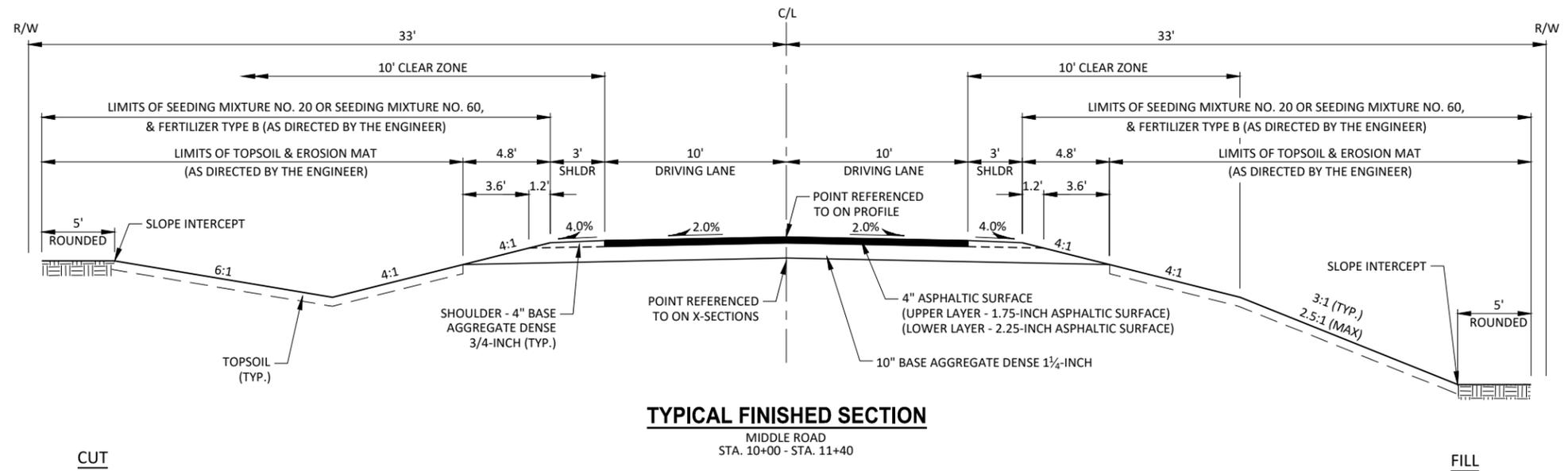
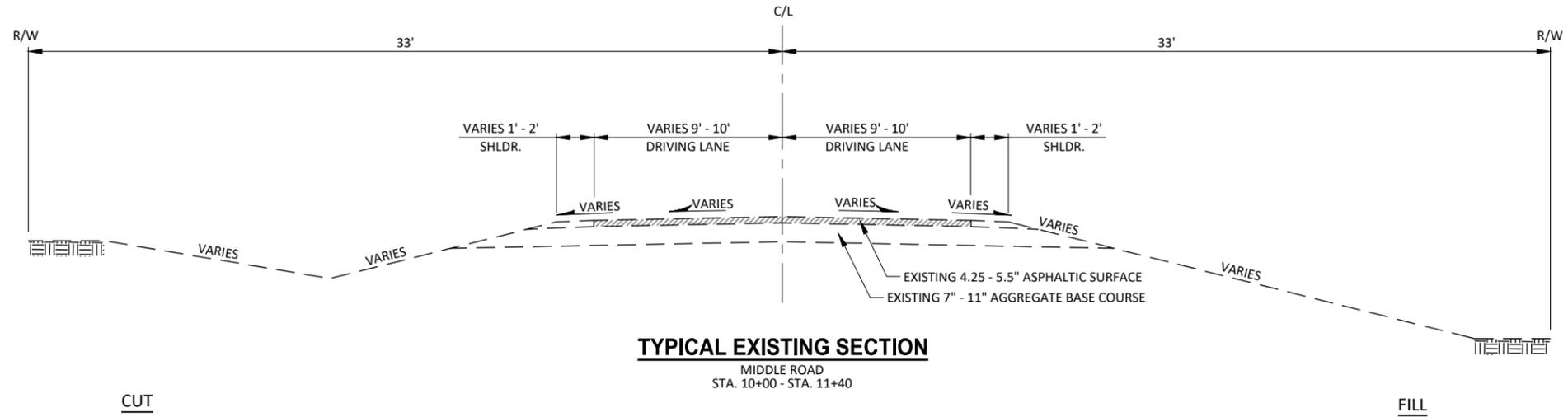


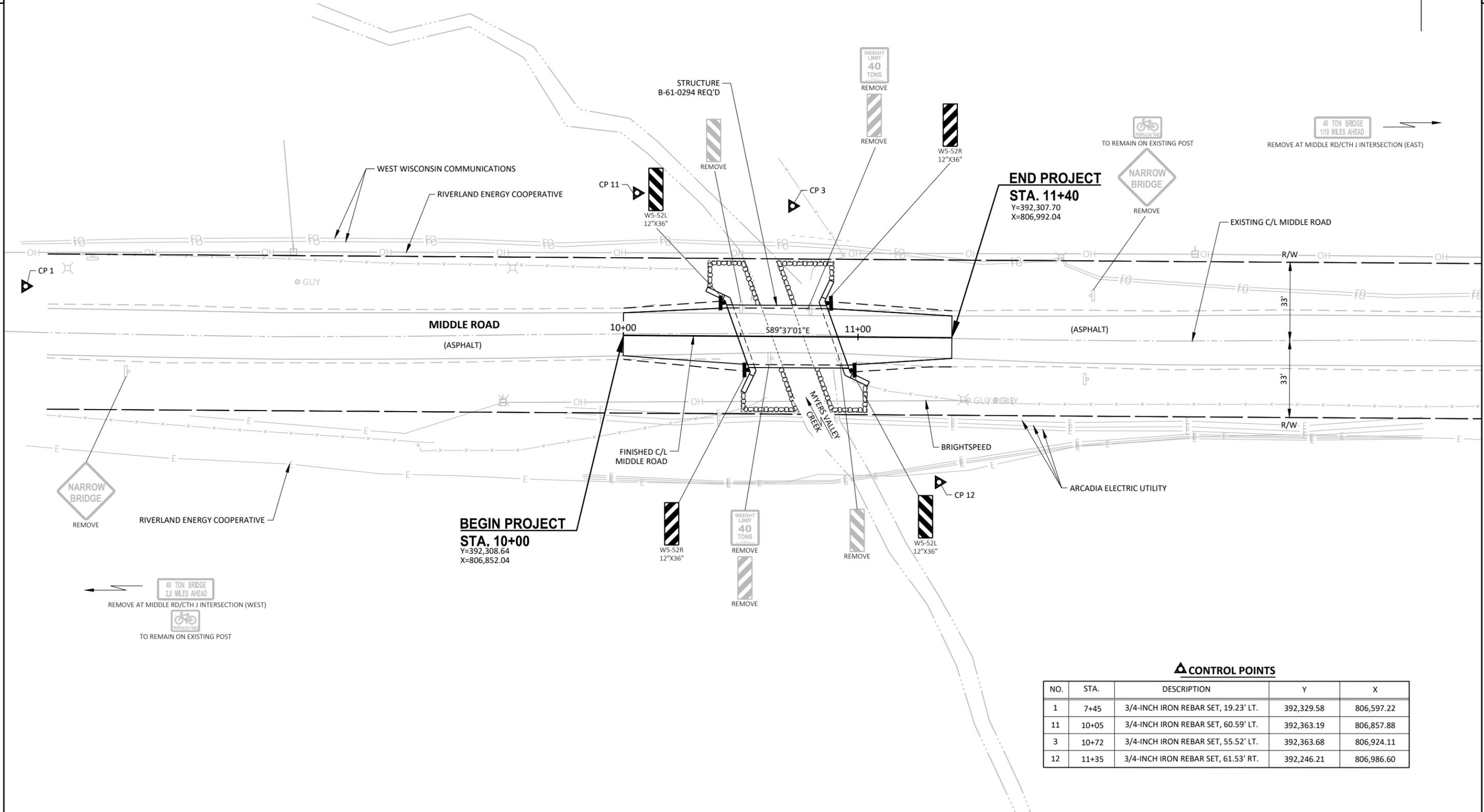
LIST OF STANDARD ABBREVIATIONS

ABUT	Abutment	INV	Invert	SALV	Salvaged
AC	Acre	IP	Iron Pipe or Pin	SAN S	Sanitary Sewer
AGG	Aggregate	IRS	Iron Rod Set	SEC	Section
AH	Ahead	JT	Joint	SHLDR	Shoulder
<	Angle	JCT	Junction	SHR	Shrinkage
ASPH	Asphaltic	LHF	Left-Hand Forward	SW	Sidewalk
AVG	Average	L	Length of Curve	S	South
ADT	Average Daily Traffic	LIN FT or LF	Linear Foot	SQ	Square
BAD	Base Aggregate Dense	LC	Long Chord of Curve	SF or SQ FT	Square Feet
BK	Back	MH	Manhole	SY or SQ YD	Square Yard
BF	Back Face	MB	Mailbox	STD	Standard
BM	Bench Mark	ML or M/L	Match Line	SDD	Standard Detail Drawings
BR	Bridge	N	North	STH	State Trunk Highways
C or C/L	Center Line	Y	North Grid Coordinate	STA	Station
CC	Center to Center	O.A.L.	Overall Length	SS	Storm Sewer
CTH	County Trunk Highway	OD	Outside Diameter	SG	Subgrade
CR	Creek	PLE	Permanent Limited Easement	SE	Superelevation
CR	Crushed		Point	SL or S/L	Survey Line
CY or CU YD	Cubic Yard	PT	Point of Curvature	SV	Septic Vent
CP	Culvert Pipe	PC	Point of Intersection	T	Tangent
C & G	Curb and Gutter	PI	Point of Reverse Curvature	TEL	Telephone
D	Degree of Curve	PRC	Point of Tangency	TEMP	Temporary
DHV	Design Hour Volume	PT	Point On Curve	TI	Temporary Interest
DIA	Diameter	POC	Point on Tangent	TLE	Temporary Limited Easement
E	East	POT	Polyvinyl Chloride	t	Ton
X	East Grid Coordinate	PVC	Portland Cement Concrete	T or TN	Town
ELEC	Electric (al)	PCC	Pound	TRANS	Transition
EL or ELEV	Elevation	LB	Pounds Per Square Inch	TL or T/L	Transit Line
ESALS	Equivalent Single Axle Loads	PSI	Private Entrance	T	Trucks (percent of)
EBS	Excavation Below Subgrade	PE	Radius	TYP	Typical
ESTR	Existing Sign to Remain	R	Railroad	UNCL	Unclassified
FF	Face to Face	RR	Range	UG	Underground Cable
FE	Field Entrance	R	Reference Line	USH	United States Highway
F	Fill	RL or R/L	Reference Point	VAR	Variable
FG	Finished Grade	RP	Reinforced Concrete Culvert	V	Velocity or Design Speed
FL or F/L	Flow Line	RCCP	Pipe	VERT	Vertical
FT	Foot	REQ'D	Required	VC	Vertical Curve
FTG	Footing	RES	Residence or Residential	VOL	Volume
GN	Grid North	RW	Retaining Wall	WM	Water Main
HT	Height	RT	Right	WV	Water Valve
CWT	Hundredweight	RHF	Right-Hand Forward	W	West
HYD	Hydrant	R/W	Right-of-Way	WB	Westbound
INL	Inlet	R	River	YD	Yard
ID	Inside Diameter	RD	Road		
		RDWY	Roadway		

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE	0-2	2-6	6 & OVER									
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA= 0.21 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.19 ACRES





CONTROL POINTS

NO.	STA.	DESCRIPTION	Y	X
1	7+45	3/4-INCH IRON REBAR SET, 19.23' LT.	392,329.58	806,597.22
11	10+05	3/4-INCH IRON REBAR SET, 60.59' LT.	392,363.19	806,857.88
3	10+72	3/4-INCH IRON REBAR SET, 55.52' LT.	392,363.68	806,924.11
12	11+35	3/4-INCH IRON REBAR SET, 61.53' RT.	392,246.21	806,986.60

Estimate Of Quantities

7276-00-76

Line	Item	Item Description	Unit	Total	Qty
0002	203.0250	Removing Structure Over Waterway Remove Debris (structure) 01. P-61-0195	EACH	1.000	1.000
0004	205.0100	Excavation Common	CY	120.000	120.000
0006	205.0506.S	Excavation, Hauling, and Disposal of Creosote Contaminated Soil	TON	55.000	55.000
0008	206.1001	Excavation for Structures Bridges (structure) 01. B-61-0294	EACH	1.000	1.000
0010	210.1500	Backfill Structure Type A	TON	300.000	300.000
0012	213.0100	Finishing Roadway (project) 01. 7276-00-76	EACH	1.000	1.000
0014	305.0110	Base Aggregate Dense 3/4-Inch	TON	20.000	20.000
0016	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	210.000	210.000
0018	455.0605	Tack Coat	GAL	11.000	11.000
0020	465.0105	Asphaltic Surface	TON	55.000	55.000
0022	502.0100	Concrete Masonry Bridges	CY	129.000	129.000
0024	502.3200	Protective Surface Treatment	SY	178.000	178.000
0026	505.0400	Bar Steel Reinforcement HS Structures	LB	4,180.000	4,180.000
0028	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	19,180.000	19,180.000
0030	513.4061	Railing Tubular Type M	LF	90.000	90.000
0032	516.0500	Rubberized Membrane Waterproofing	SY	12.000	12.000
0034	550.2104	Piling CIP Concrete 10 3/4 X 0.25-Inch	LF	1,190.000	1,190.000
0036	606.0300	Riprap Heavy	CY	190.000	190.000
0038	612.0406	Pipe Underdrain Wrapped 6-Inch	LF	140.000	140.000
0040	618.0100	Maintenance and Repair of Haul Roads (project) 01. 7276-00-76	EACH	1.000	1.000
0042	619.1000	Mobilization	EACH	1.000	1.000
0044	624.0100	Water	MGAL	4.000	4.000
0046	625.0100	Topsoil	SY	360.000	360.000
0048	628.1504	Silt Fence	LF	300.000	300.000
0050	628.1520	Silt Fence Maintenance	LF	600.000	600.000
0052	628.1905	Mobilizations Erosion Control	EACH	2.000	2.000
0054	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0056	628.2008	Erosion Mat Urban Class I Type B	SY	360.000	360.000
0058	628.6005	Turbidity Barriers	SY	210.000	210.000
0060	629.0210	Fertilizer Type B	CWT	0.500	0.500
0062	630.0120	Seeding Mixture No. 20	LB	15.000	15.000
0064	630.0160	Seeding Mixture No. 60	LB	1.000	1.000
0066	630.0200	Seeding Temporary	LB	10.000	10.000
0068	630.0500	Seed Water	MGAL	15.000	15.000
0070	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	4.000	4.000
0072	637.2230	Signs Type II Reflective F	SF	12.000	12.000
0074	638.2602	Removing Signs Type II	EACH	10.000	10.000
0076	638.3000	Removing Small Sign Supports	EACH	6.000	6.000
0078	642.5001	Field Office Type B	EACH	1.000	1.000
0080	643.0420	Traffic Control Barricades Type III	DAY	1,360.000	1,360.000
0082	643.0705	Traffic Control Warning Lights Type A	DAY	2,720.000	2,720.000
0084	643.0900	Traffic Control Signs	DAY	1,088.000	1,088.000
0086	643.5000	Traffic Control	EACH	1.000	1.000
0088	645.0111	Geotextile Type DF Schedule A	SY	88.000	88.000
0090	645.0120	Geotextile Type HR	SY	320.000	320.000
0092	650.4500	Construction Staking Subgrade	LF	100.000	100.000
0094	650.5000	Construction Staking Base	LF	100.000	100.000
0096	650.6501	Construction Staking Structure Layout (structure) 01. B-61-0294	EACH	1.000	1.000
0098	650.9911	Construction Staking Supplemental Control (project) 01. 7276-00-76	EACH	1.000	1.000

Estimate Of Quantities

7276-00-76

Line	Item	Item Description	Unit	Total	Qty
0100	650.9920	Construction Staking Slope Stakes	LF	100.000	100.000
0102	690.0150	Sawing Asphalt	LF	36.000	36.000
0104	715.0502	Incentive Strength Concrete Structures	DOL	774.000	774.000
0106	999.2005.S	Maintaining Bird Deterrent System (station) 01. Sta. 10+75	EACH	1.000	1.000

EARTHWORK

STATION - STATION	LOCATION	205.0100 COMMON EXCAVATION (CY) (1)	AVAILABLE MATERIAL (CY) (2)	UNEXPANDED FILL	EXPANDED FILL FACTOR 1.25 (CY)	MASS ORDINATE +/- (CY) (3)	WASTE (CY) (4)
10+00 - 11+40	MAINLINE	120	120	77	96	24	24
TOTALS =		120	120	77	96	24	24

NOTES:

- (1) SALVAGED/UNUSABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.
- (2) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSABLE PAVEMENT MATERIAL
- (3) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.
- (4) FACTORS USED TO COMPUTE ANTICIPATED WASTE AND THE COMPUTED WASTE VOLUME IDENTIFIED ARE FOR GENERAL INFORMATION ONLY.

**EXCAVATION, HAULING, AND DISPOSAL OF
CREOSOTE CONTAMINATED SOIL**

STATION - STATION	LOCATION	205.0506.S (TON)
10+54-10+66	SW WINGWALL EXTENSION, RT.	55
TOTAL =		55

BASE AGGREGATE DENSE

STATION - STATION	LOCATION	305.0110	305.0120	624.0100
		BASE AGGREGATE DENSE 3/4-INCH (TON)	BASE AGGREGATE DENSE 1 1/4-INCH (TON)	WATER (MGAL)
10+00 - 10+54	MAINLINE	10	105	2
10+86 - 11+40	MAINLINE	10	105	2
TOTALS =		20	210	4

ASPHALTIC SURFACE

STATION - STATION	LOCATION	455.0605 TACK COAT (GAL)	465.0105 ASPHALTIC SURFACE (TON)
10+00 - 10+53	MAINLINE	5	27
10+87 - 11+40	MAINLINE	6	28
TOTALS =		11	55

FINISHING ITEMS

STATION - STATION	LOCATION	625.0100	629.0210	630.0120	630.0160	630.0200	630.0500
		TOPSOIL (SY)	FERTILIZER TYPE B (CWT)	SEEDING MIXTURE NO. 20 (LB)	SEEDING MIXTURE NO. 60 (LB)	SEEDING TEMPORARY (LB)	SEED WATER (MGAL)
10+00 - 10+43	MAINLINE, LT.	40	0.1	2	-	1	2
10+00 - 10+54	MAINLINE, RT.	90	0.1	3	0.3	2	4
10+86 - 11+41	MAINLINE, LT.	90	0.1	3	0.3	2	4
10+97 - 11+40	MAINLINE, RT.	70	0.1	2	0.2	2	3
-	UNDISTRIBUTED	70	0.1	5	0.2	3	2
TOTALS =		360	0.5	15	1	10	15

NOTE: USE SEED MIXTURE NO. 60 IN AREAS WITHIN 15-FEET OF EXISTING WETLANDS. DO NOT USE FERTILIZER WITHIN 15-FEET OF EXISTING WETLANDS

SILT FENCE

STATION - STATION	LOCATION	628.1504 SILT FENCE (LF)	628.1520 SILT FENCE MAINTENANCE (LF)
9+96 - 10+50	MAINLINE, LT.	56	112
9+98 - 10+73	MAINLINE, RT.	76	152
10+67 - 11+43	MAINLINE, LT.	79	158
10+93 - 11+42	MAINLINE, RT.	51	102
-	UNDISTRIBUTED	38	76
TOTALS =		300	600

MOBILIZATION EROSION CONTROL

PROJECT	628.1905	628.1910
	MOBILIZATION EROSION CONTROL (EACH)	MOBILIZATION EMERGENCY EROSION CONTROL (EACH)
7276-00-76	2	2
TOTALS =		2

EROSION MAT

STATION - STATION	LOCATION	628.2008 URBAN CLASS I TYPE B (SY)
		10+00 - 10+43
10+00 - 10+54	MAINLINE, RT.	90
10+86 - 11+41	MAINLINE, LT.	90
10+97 - 11+40	MAINLINE, RT.	70
-	UNDISTRIBUTED	70
TOTALS =		360

TURBIDITY BARRIERS

LOCATION	628.6005 TURBIDITY BARRIERS (SY)	
WEST ABUTMENT	83	
EAST ABUTMENT	87	
UNDISTRIBUTED	40	
TOTAL =		210

NOTE: ALL ITEMS CATEGORY 0010 UNLESS OTHERWISE NOTED

PROJECT NO: 7276-00-76

HWY: MIDDLE ROAD

COUNTY: TREMPLEAU

MISCELLANEOUS QUANTITIES

SHEET:

E

PERMANENT SIGNING

APPROX. STATION	LOCATION	SIGN CODE	SIGN DESCRIPTION	ORDER LINES	SIGN SIZE (IN X IN)	637.2230 SIGNS TYPE II REFLECTIVE F (SF)	634.0614 POSTS WOOD 4X6-INCH 14-FT (EACH)	638.2602 REMOVING SIGNS TYPE II (EACH)	638.3000 REMOVING SMALL SIGN SUPPORTS (EACH)
-	MAINLINE, RT	R12-55	XX TON BRIDGE YY MILES AHEAD	40 TON, 2.8 MILES	-	-	-	1	-
7+89	MAINLINE, RT	W5-2	NARROW BRIDGE	-	-	-	-	1	1
10+43	MAINLINE, LT	W5-52L	BRIDGE HASH MARKS	-	12X36	3.00	1	-	-
10+51	MAINLINE, LT	W5-52L	BRIDGE HASH MARKS	-	-	-	-	1	1
10+53	MAINLINE, RT	W5-52R	BRIDGE HASH MARKS	-	12X36	3.00	1	-	-
10+63	MAINLINE, RT	W5-52R	BRIDGE HASH MARKS	-	-	-	-	1	1
10+63	MAINLINE, RT	R12-1	WEIGHT LIMIT	40 TONS	-	-	-	1	-
10+79	MAINLINE, LT	W5-52R	BRIDGE HASH MARKS	-	-	-	-	1	1
10+79	MAINLINE, LT	R12-1	WEIGHT LIMIT	40 TONS	-	-	-	1	-
10+86	MAINLINE, LT	W5-52R	BRIDGE HASH MARKS	-	12X36	3.00	1	-	-
10+91	MAINLINE, RT	W5-52L	BRIDGE HASH MARKS	-	-	-	-	1	1
10+97	MAINLINE, RT	W5-52L	BRIDGE HASH MARKS	-	12X36	3.00	1	-	-
11+99	MAINLINE, LT	W5-2	NARROW BRIDGE	-	-	-	-	1	-
-	MAINLINE, LT	R12-55	XX TON BRIDGE YY MILES AHEAD	40 TON, 1/10 MILES	-	-	-	1	1
PROJECT TOTALS =						12.00	4	10	6

TRAFFIC CONTROL

LOCATION	CALENDAR DAY DURATION	643.0705								COMMENTS
		643.0420		TRAFFIC CONTROL		643.0900		643.5000		
		TRAFFIC CONTROL BARRICADES TYPE III (COUNT)	(DAY)	WARNING LIGHTS TYPE A (COUNT)	(DAY)	TRAFFIC CONTROL SIGNS (COUNT)	(DAY)	TRAFFIC CONTROL (EACH)		
WESTERN INTERSECTION MIDDLE RD/CTH J	68	2	136	4	272	2	136	-		(2 EA) R11-3B
MIDDLE RD/LOCOMOTIVE LANE	68	2	136	4	272	3	204	-		(1 EA) W20-3A; (2 EA) R11-3C
BEGINNING OF PROJECT	68	7	476	14	952	4	272	-		(1 EA) W20-3C; W20-3D; (2 EA) R11-2B
END OF PROJECT	68	7	476	14	952	4	272	-		(1 EA) W20-3C; W20-3D; (2 EA) R11-2B
EASTERN INTERSECTION MIDDLE RD/CTH J	68	2	136	4	272	3	204	-		(1 EA) W20-3A; (2 EA) R11-3C
PROJECT		-	-	-	-	-	-	1		
TOTALS =			1,360		2,720		1,088	1		

CONSTRUCTION STAKING

STATION - STATION	LOCATION	650.4500 SUBGRADE (LF)	650.5000 BASE (LF)	650.6501* STRUCTURE LAYOUT (01. B-61-0294) (EACH)	650.9911 SUPPLEMENTAL CONTROL (01. 7276-00-76) (EACH)	650.9920 SLOPE STAKES (LF)
10+00 - 11+40	MAINLINE	100	100	-	-	100
-	PROJECT	-	-	1	1	-
TOTALS =		100	100	1	1	100

*CATEGORY 0020

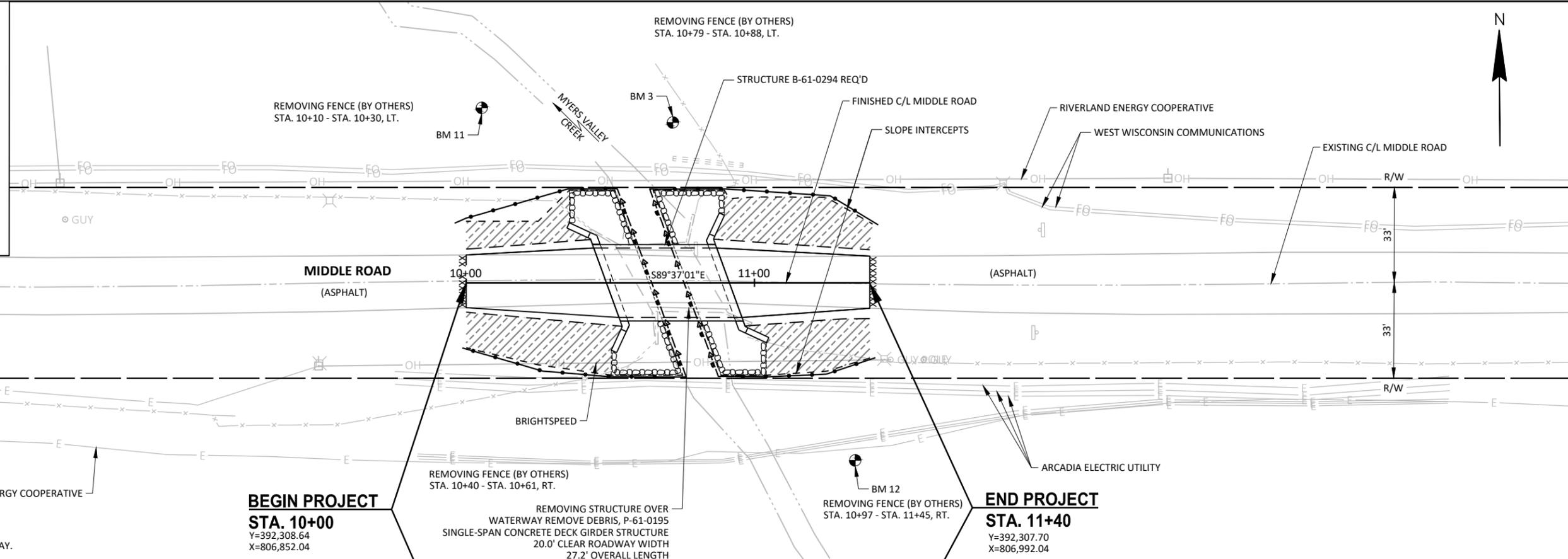
SAWING

STATION - STATION	LOCATION	690.0150 SAWING ASPHALT (LF)
10+00	MAINLINE	18
11+40	MAINLINE	18
TOTALS =		36

NOTE: ALL ITEMS CATEGORY 0010 UNLESS OTHERWISE NOTED

LEGEND

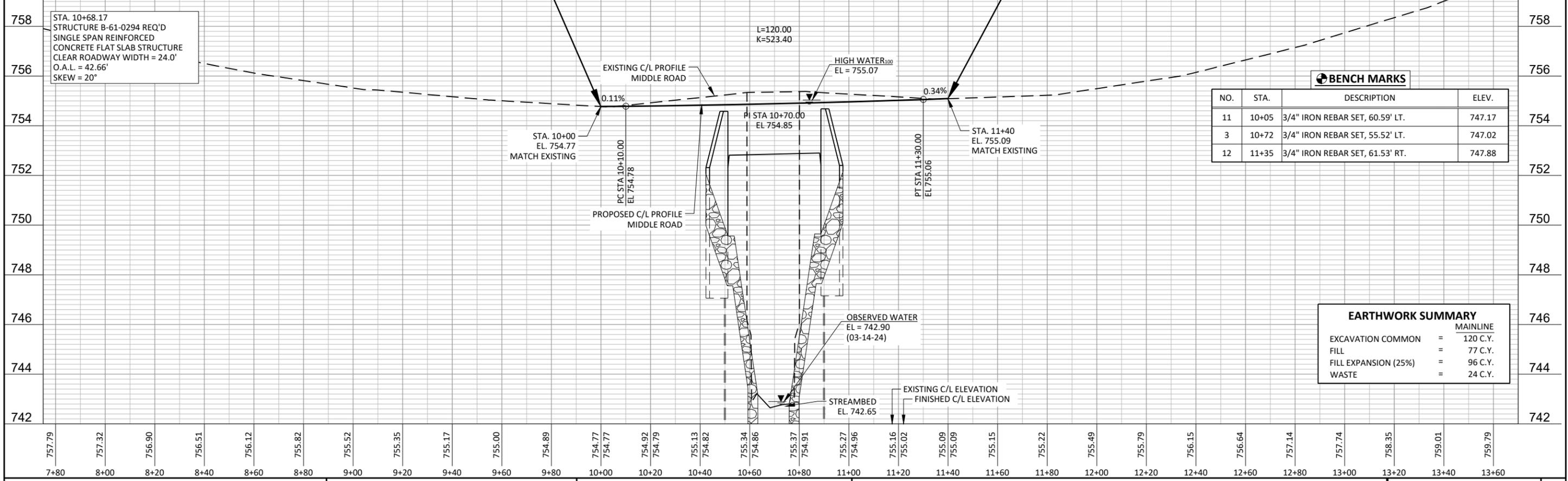
- ← DIRECTION OF FLOW
- XXXXXXXXX SAWING ASPHALT
- SILT FENCE *
- - - - - EDGE OF WATER
- WETLANDS
- ○ ○ ○ ○ HEAVY RIPRAP OVER GEOTEXTILE TYPE HR
- TURBIDITY BARRIER
- ▨ EROSION MAT URBAN CLASS I TYPE B



* NOTE:
INSTALL ALL SILT FENCE BY HAND TO AVOID IMPACTS BEYOND THE EXISTING RIGHT OF WAY.

BEGIN PROJECT
STA. 10+00
Y=392,308.64
X=806,852.04

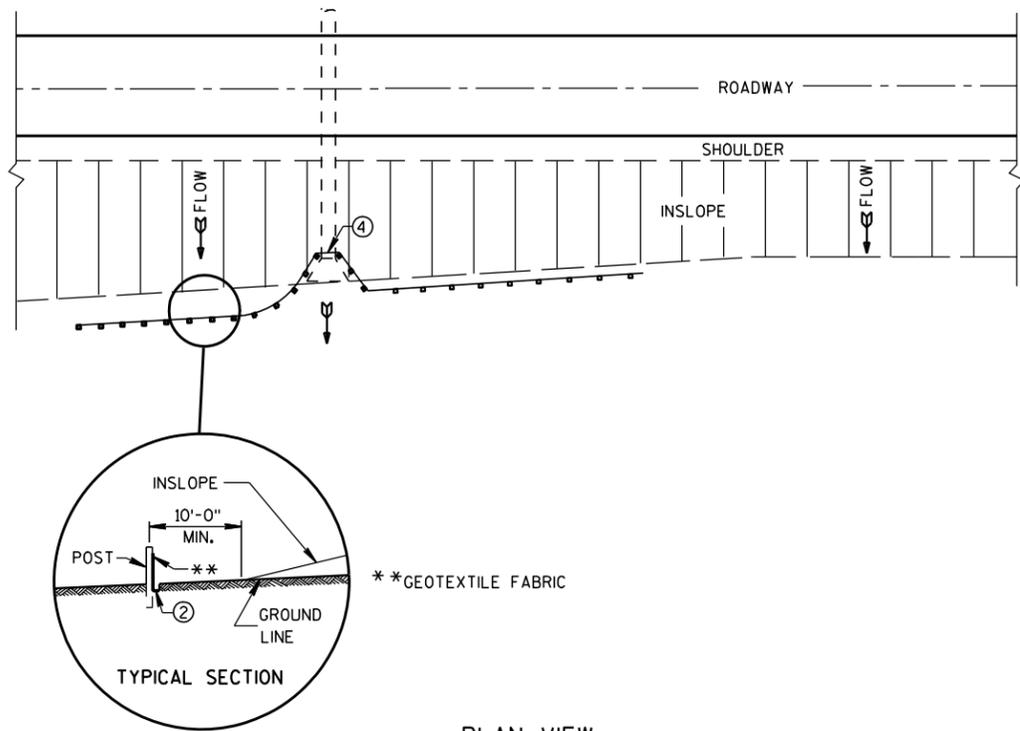
END PROJECT
STA. 11+40
Y=392,307.70
X=806,992.04



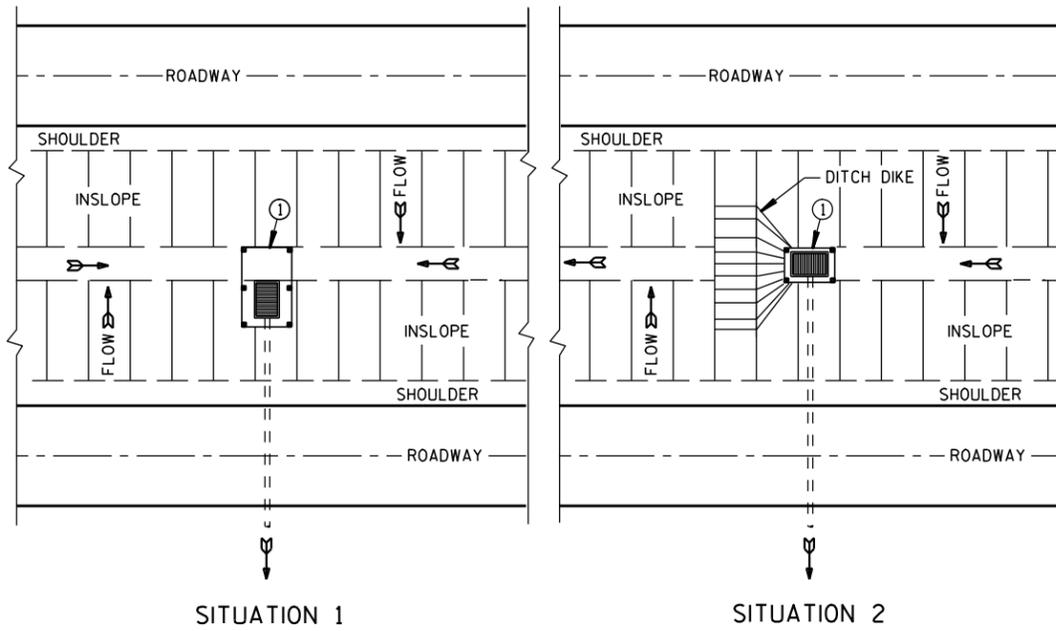
EARTHWORK SUMMARY		
		MAINLINE
EXCAVATION COMMON	=	120 C.Y.
FILL	=	77 C.Y.
FILL EXPANSION (25%)	=	96 C.Y.
WASTE	=	24 C.Y.

Standard Detail Drawing List

08E09-06	SILT FENCE
08E11-02	TURBIDITY BARRIER
12A03-10	NAME PLATE (STRUCTURES)
13C19-03	HMA LONGITUDINAL JOINTS
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C06-12	SIGNING & MARKING FOR TWO LANE BRIDGES
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

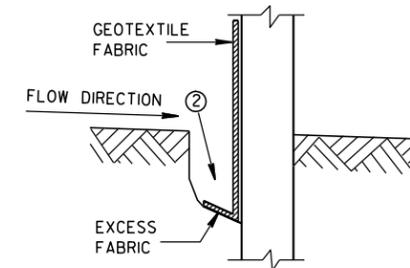


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

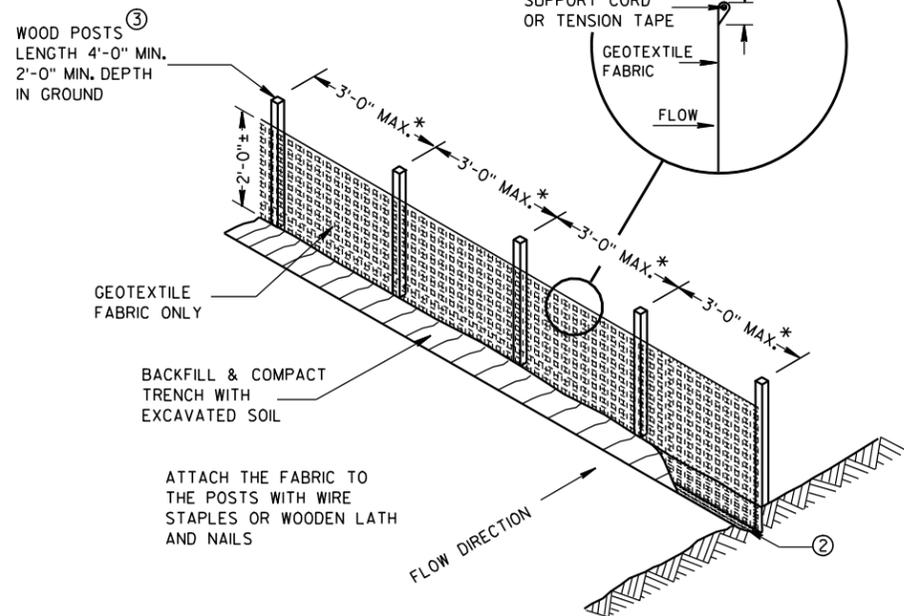
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



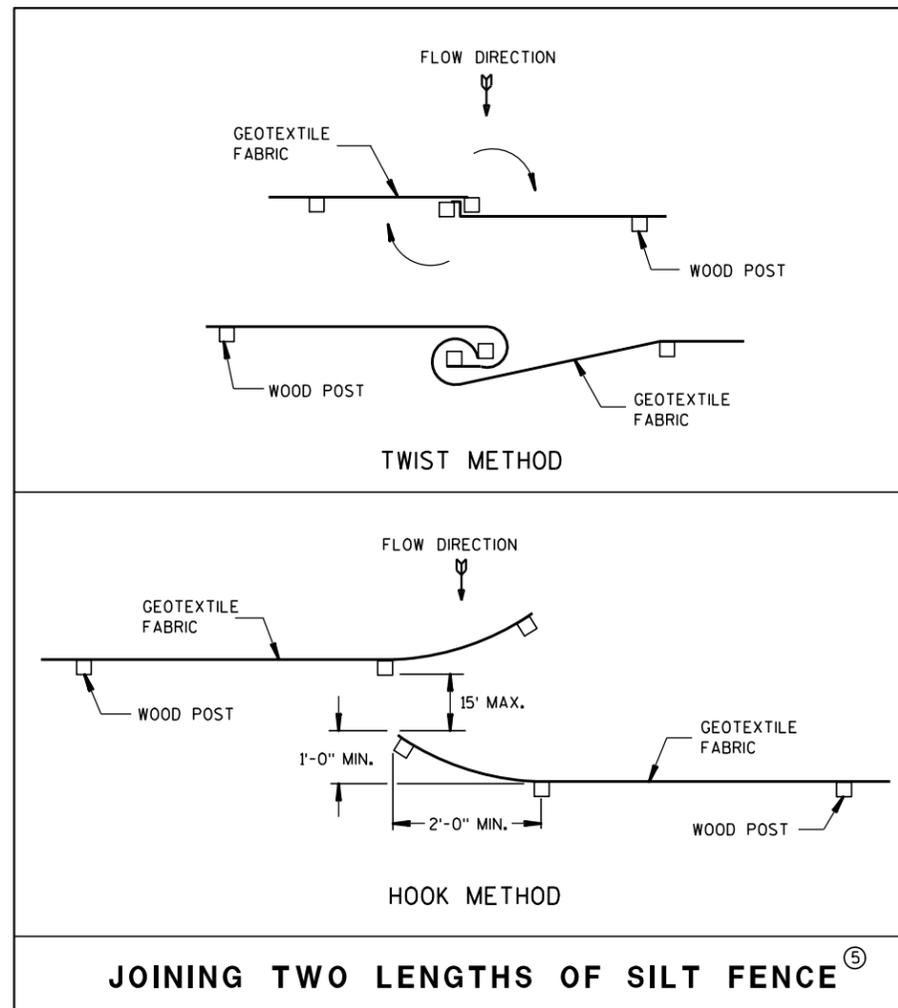
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

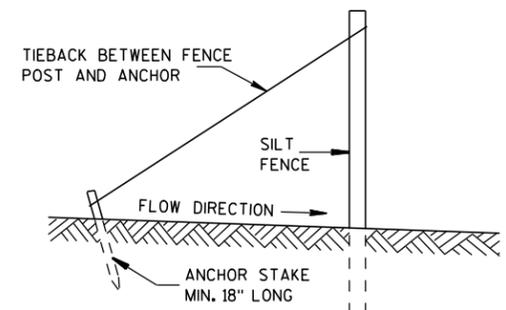


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.



JOINING TWO LENGTHS OF SILT FENCE ⑤



SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

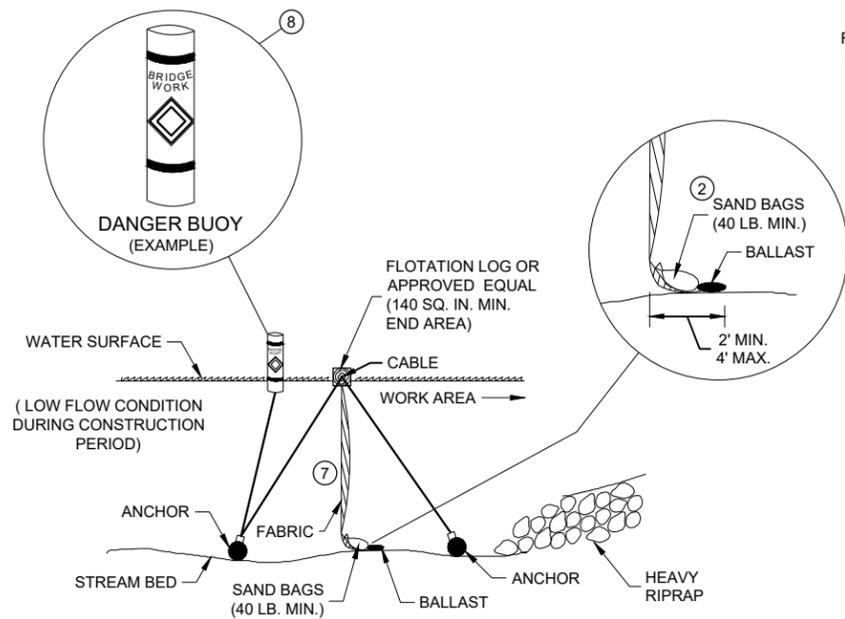
APPROVED

4-29-05

DATE

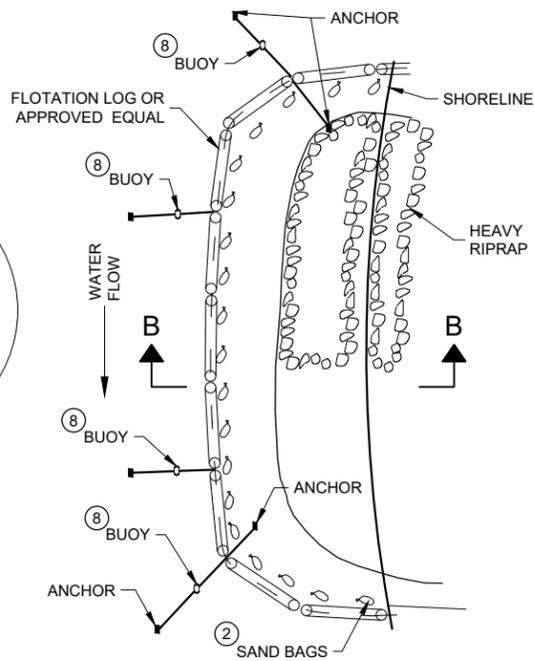
FHWA

/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

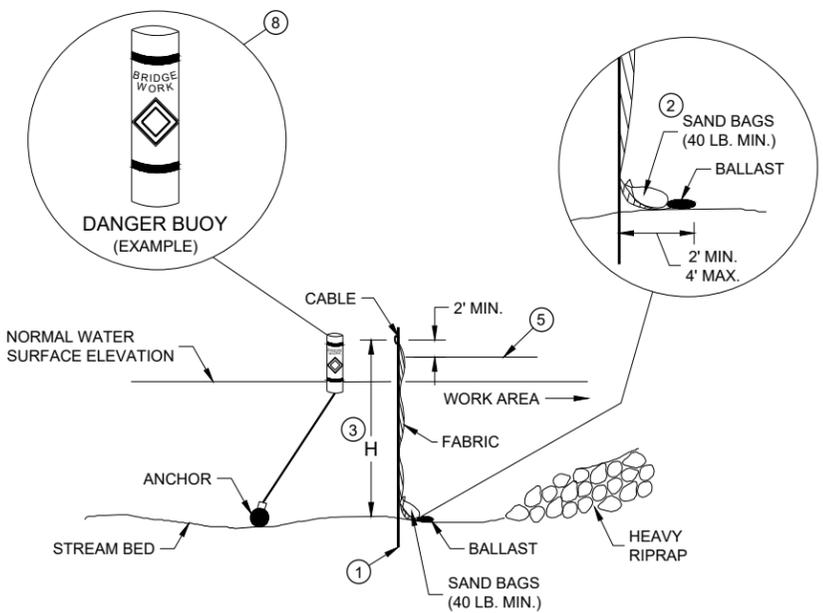


SECTION B - B

**TURBIDITY BARRIER - FLOAT ALTERNATIVE
CAUTION - SEE NOTE 6**

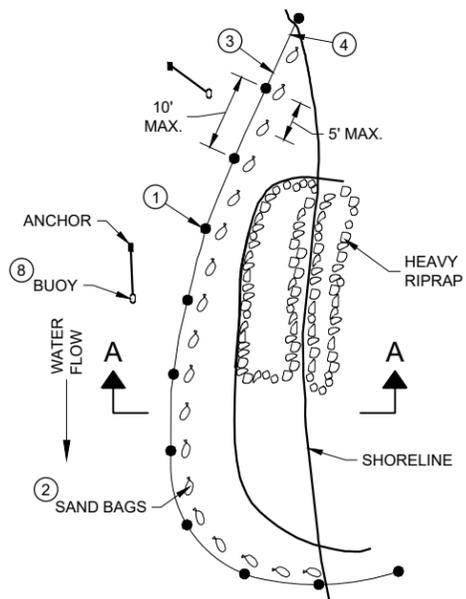


PLAN VIEW



SECTION A - A

TURBIDITY BARRIER - STANDARD POST INSTALLATION



PLAN VIEW

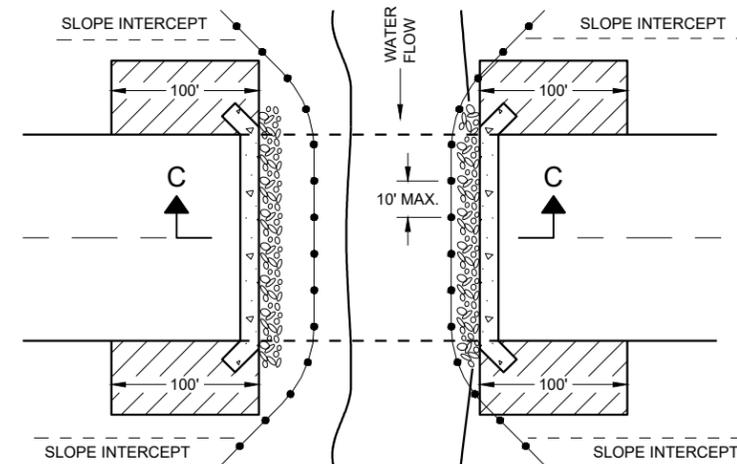
TURBIDITY BARRIER PLACEMENT DETAILS

GENERAL NOTES

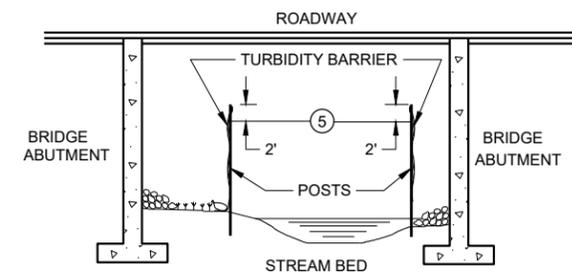
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TURBIDITY BARRIER MAY BE REMOVED AT THE ENGINEERS DISCRETION, WHEN PERMANENT EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

- ① DRIVEN STEEL POSTS, PIPES, OR CHANNELS. LENGTH SHALL BE SUFFICIENT TO SECURELY SUPPORT BARRIER AT HIGH WATER ELEVATIONS.
- ② SAND BAGS TO BE USED AS ADDITIONAL BALLAST WHEN ORDERED BY THE ENGINEER TO MEET ADVERSE FIELD CONDITIONS. SPACE AS APPROPRIATE FOR SITE CONDITIONS.
- ③ WHEN BARRIER HEIGHT "H" EXCEEDS 8 FEET, POST SPACING MAY NEED TO BE DECREASED.
- ④ IN WATERWAYS SUBJECT TO FLUCTUATING WATER ELEVATIONS, PROVISIONS SHOULD BE MADE TO ALLOW THE WATER TO EQUALIZE ON EACH SIDE OF THE BARRIER. THIS MAY BE ACCOMPLISHED BY LEAVING A PORTION OF THE BARRIER OPEN ON THE UPSTREAM END.
- ⑤ ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION PERIOD. MINIMUM BARRIER HEIGHT SHALL BE 2' GREATER THAN EITHER THE Q2 ELEVATION OR THE ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION, WHICHEVER IS GREATER.
- ⑥ FLOAT ALTERNATIVE WILL ONLY BE ALLOWED WITH WRITTEN APPROVAL OF THE ENGINEER, AND IS MEANT FOR LOCATIONS WHERE BEDROCK PREVENTS THE INSTALLATION OF POSTS.
- ⑦ ALLOW SUFFICIENT SLACK VERTICALLY AND HORIZONTALLY SO THAT SEDIMENT BUILD UP WILL NOT SEPARATE OR LOWER THE TURBIDITY BARRIER.
- ⑧ USE AS DIRECTED BY COAST GUARD OR DNR PERMIT WHEN WORKING IN NAVIGABLE WATERWAYS.



PLAN VIEW



SECTION C - C

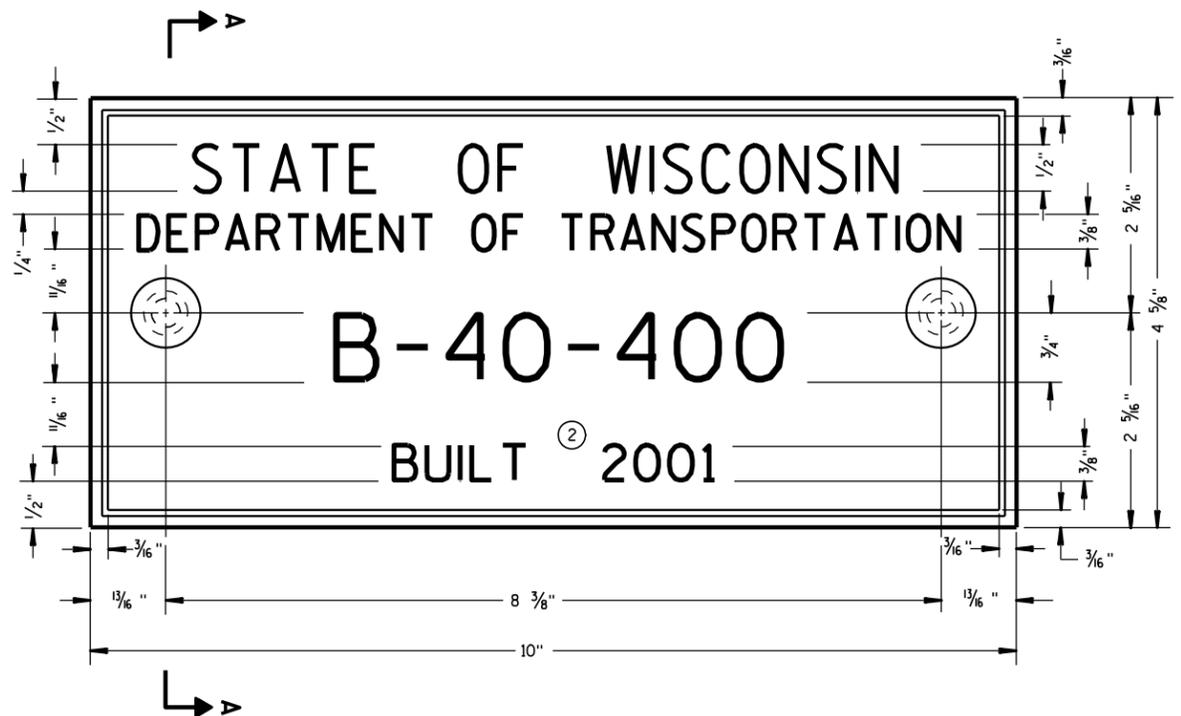
**TURBIDITY BARRIER DETAIL SHOWING
TYPICAL PLACEMENT AT STRUCTURES**

TURBIDITY BARRIER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/4/02 DATE /S/ Beth Cannestra
DATE CHIEF ROADWAY DEVELOPMENT
ENGINEER

FHWA



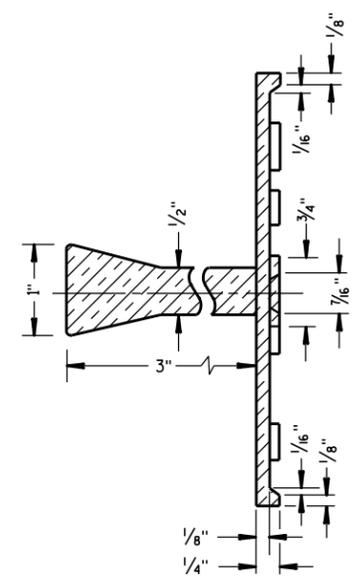
TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)

GENERAL NOTES

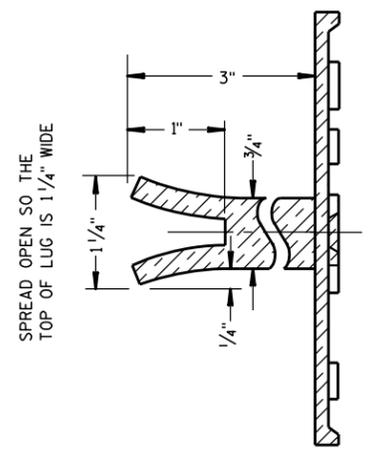
NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

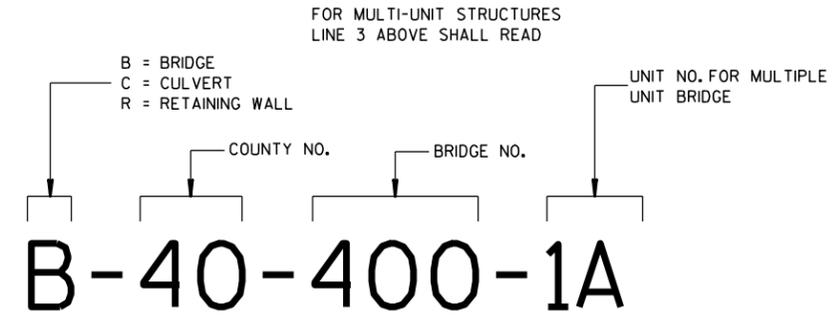
- ① EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- ② REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.



SECTION A-A

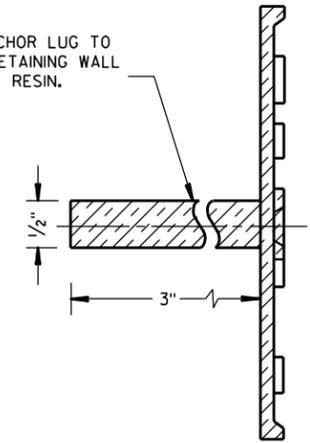


ALTERNATE LUG



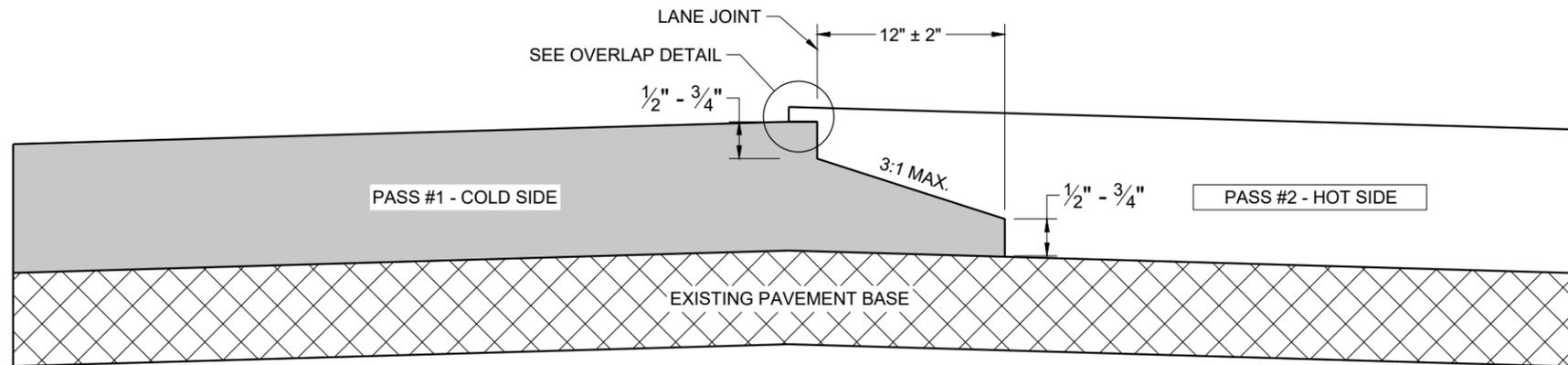
**NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES**

- ① ADHERE ANCHOR LUG TO PRECAST RETAINING WALL WITH EPOXY RESIN.

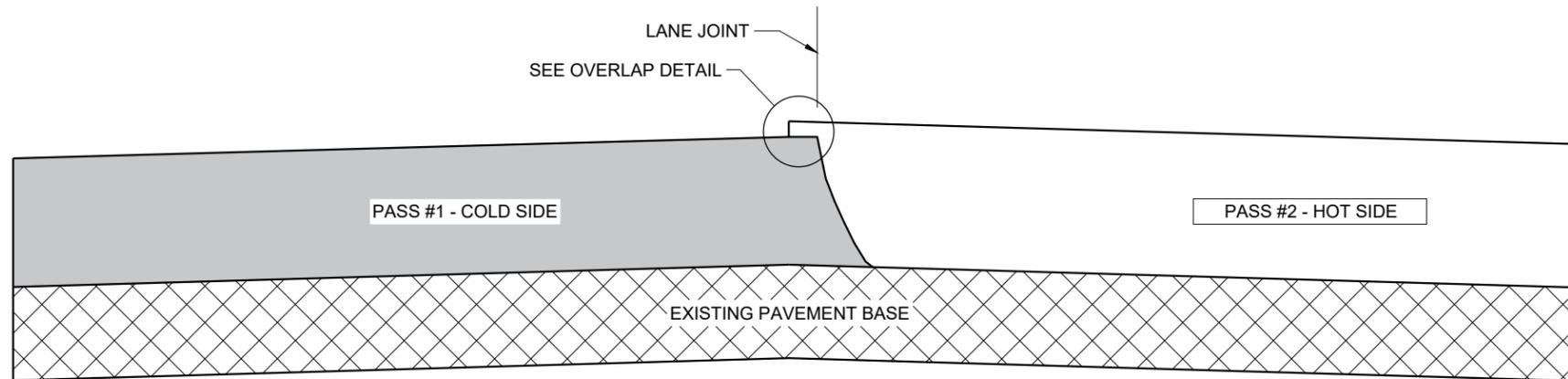


ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

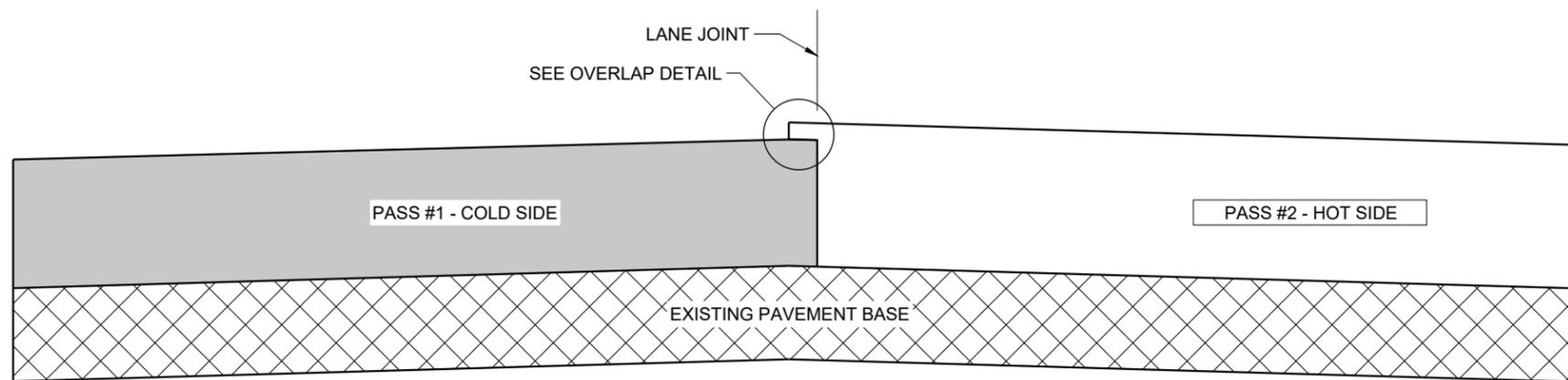
NAME PLATE (STRUCTURES)	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE 3/26/10	/S/ Scot Becker CHIEF STRUCTURAL DEVELOPMENT ENGINEER
FHWA	



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

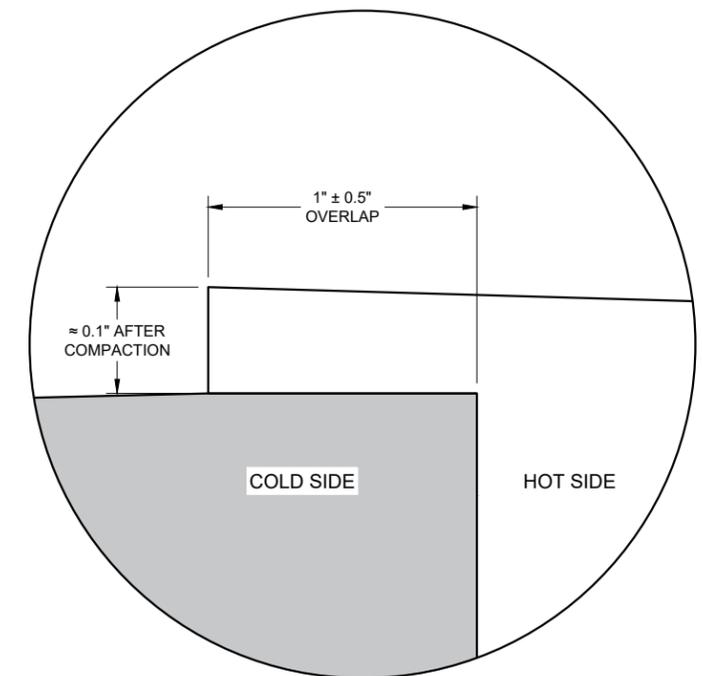
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

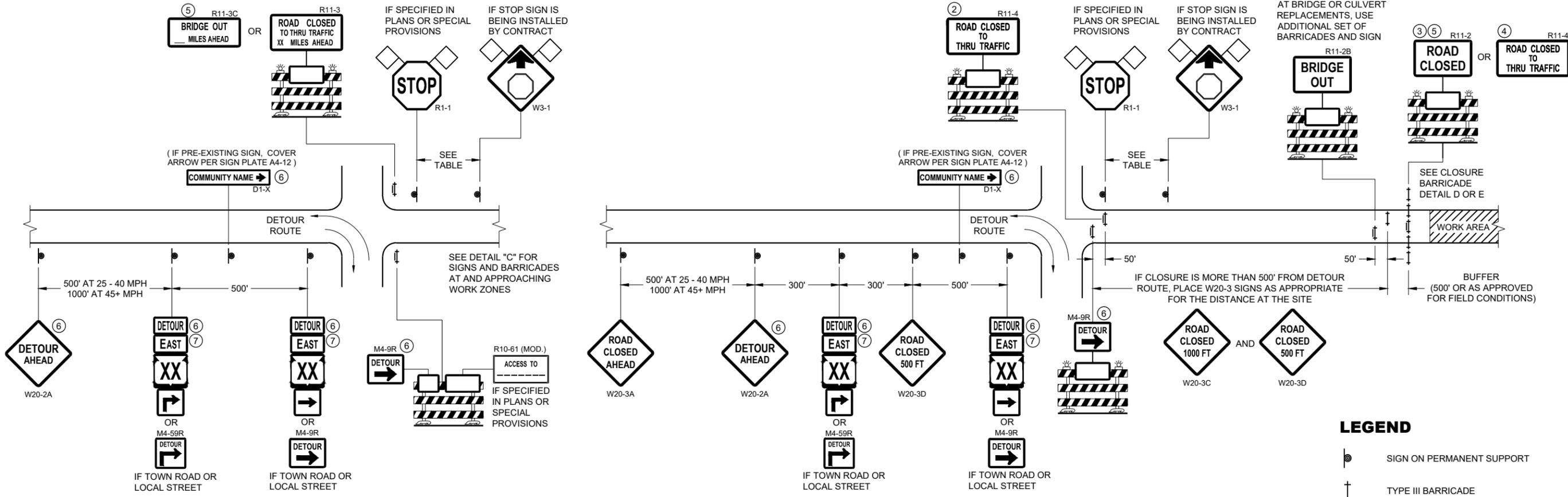
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6

SDD 13C19 - 03

SDD 13C19 - 03

HMA LONGITUDINAL JOINTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

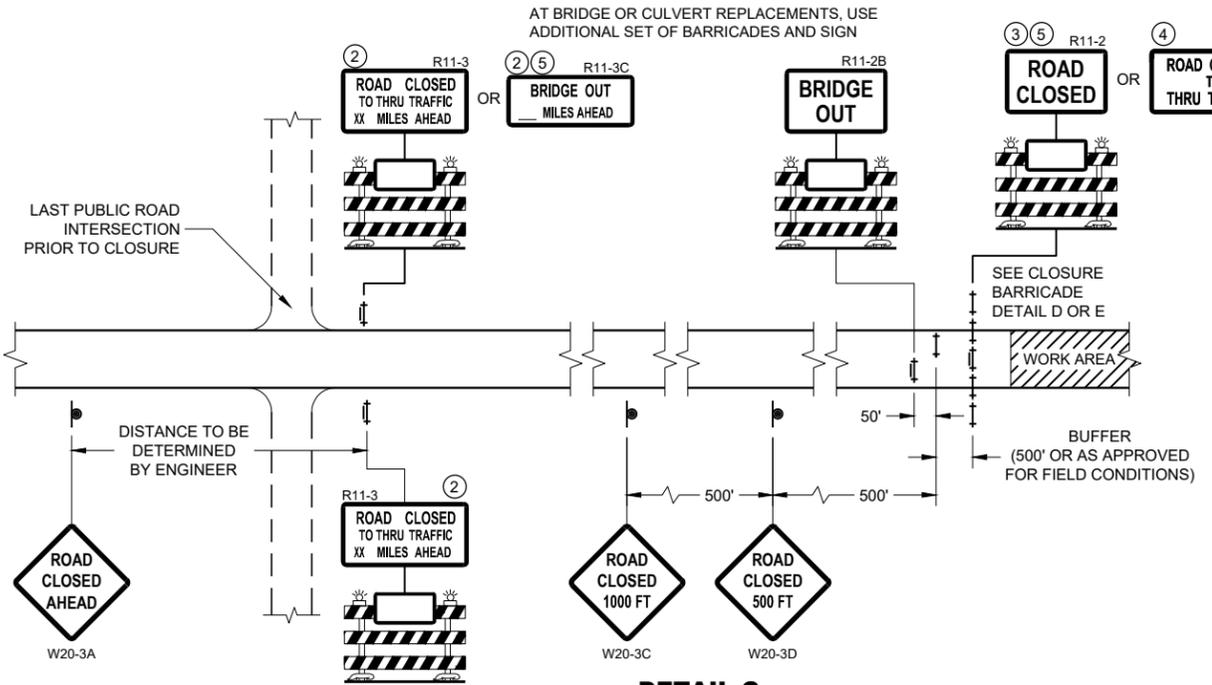
WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



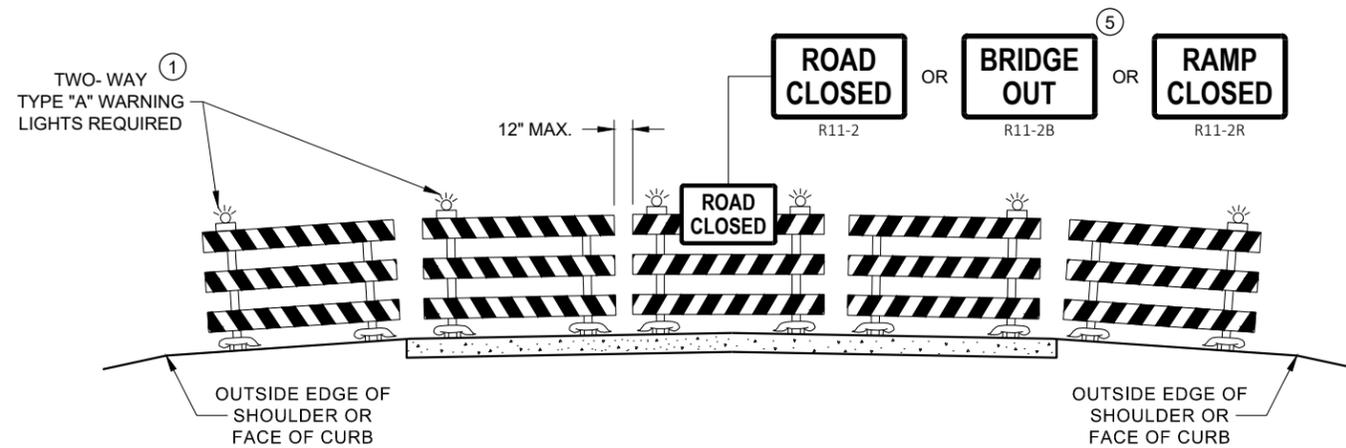
**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

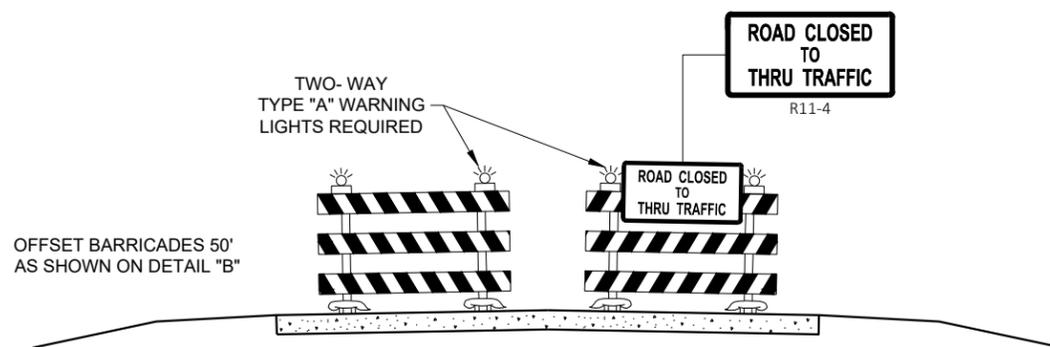
**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER
FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

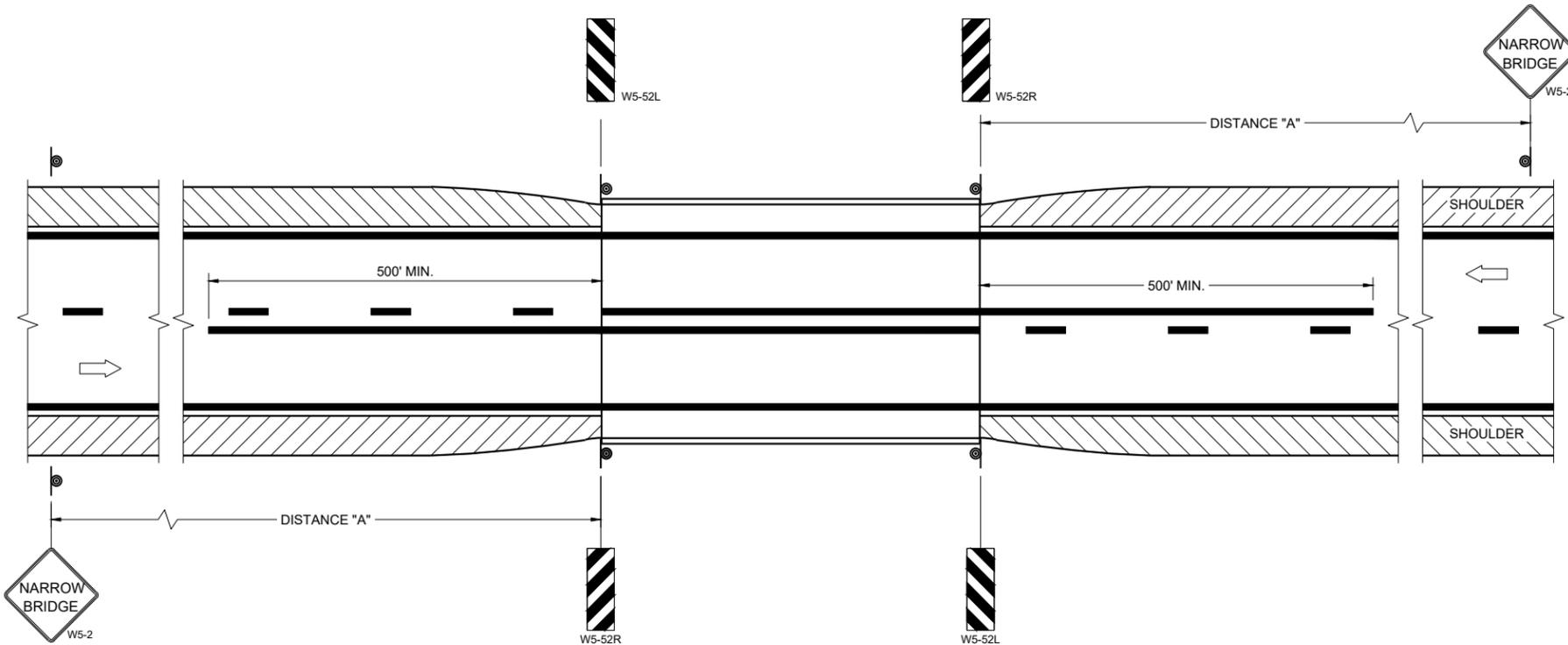
- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

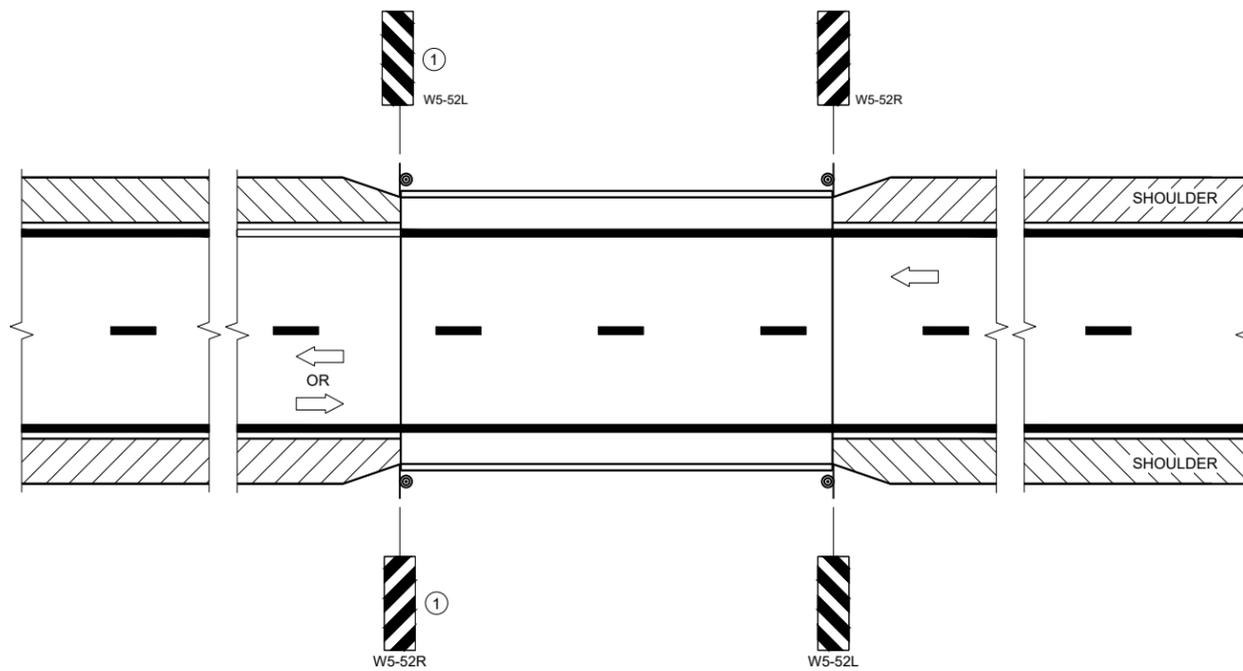
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA



SITUATION 1
 WARRANTING CRITERIA:
 BRIDGE WIDTH IS AT LEAST 16 FEET BUT LESS THAN 24 FEET.



SITUATION 2
 WARRANTING CRITERIA:
 1. BRIDGE WIDTH IS AT LEAST 24 FEET AND
 2. BRIDGE SHOULDER WIDTH IS LESS THAN 6 FEET

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THE DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

LOCATE W5-52 SIGN POST(S) BEHIND GUARDRAIL WHEN PRESENT.

PLACE THE EDGE OF THE W5-52 SIGN IN LINE WITH FACE OF CURB OR PARAPET.

ON BRIDGE ONLY PROJECTS, PLACE 300 FEET OF EDGELINE.

OMIT EDGELINES ON ROADWAYS WITHOUT EXISTING EDGELINES.

① OMIT ON ONE-WAY TRAVELED WAYS.

LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC

DISTANCE TABLE

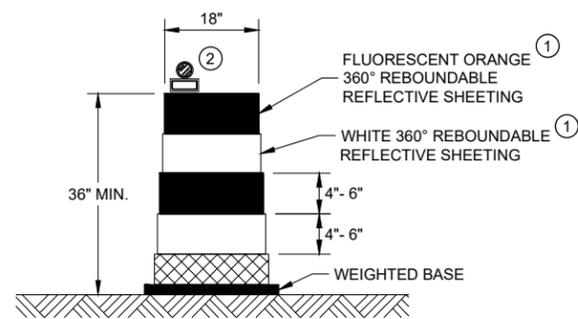
POSTED OR 85TH PERCENTILE SPEED	DISTANCE "A"
25	150'
30	200'
35	250'
40	300'
45	400'
50	550'
55	700'

SIGNING AND MARKING FOR TWO LANE BRIDGES

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

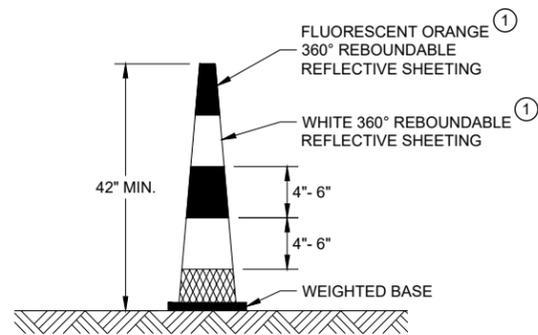
APPROVED
 May 2023 /S/ Jeannie Silver
 DATE Statewide Pavement Marking Engineer

FHWA



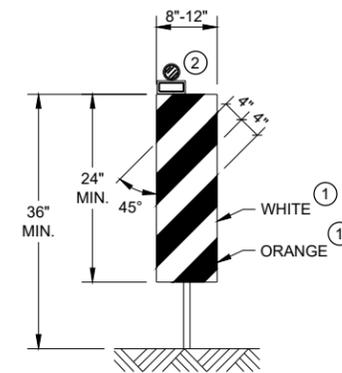
DRUM

BALLAST WIDTHS
RANGE FROM 24"-36"



42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS
BALLAST WIDTHS
RANGE FROM 14"-20"

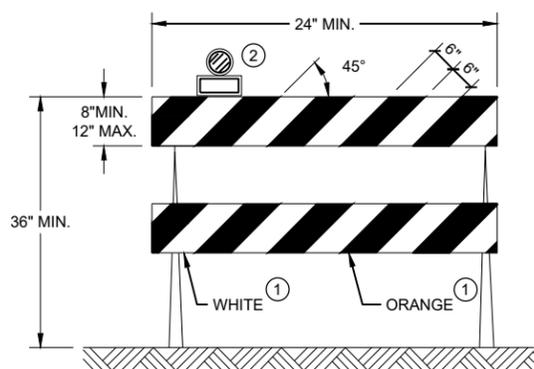


VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.

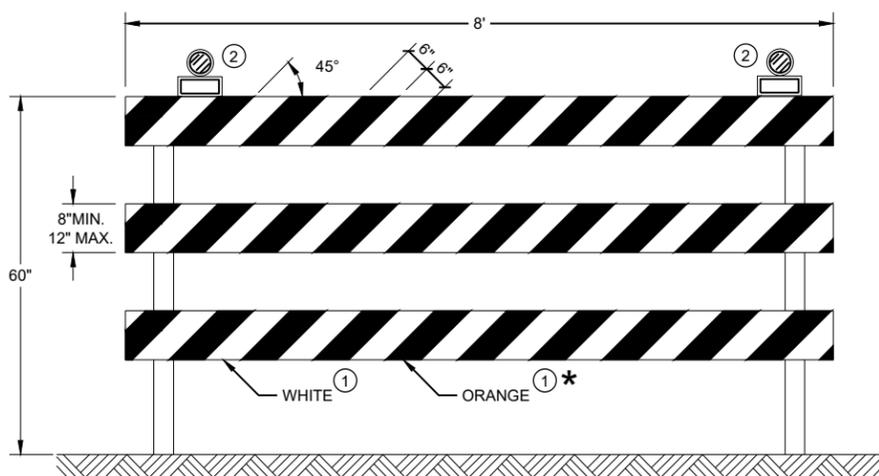
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

**CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS**

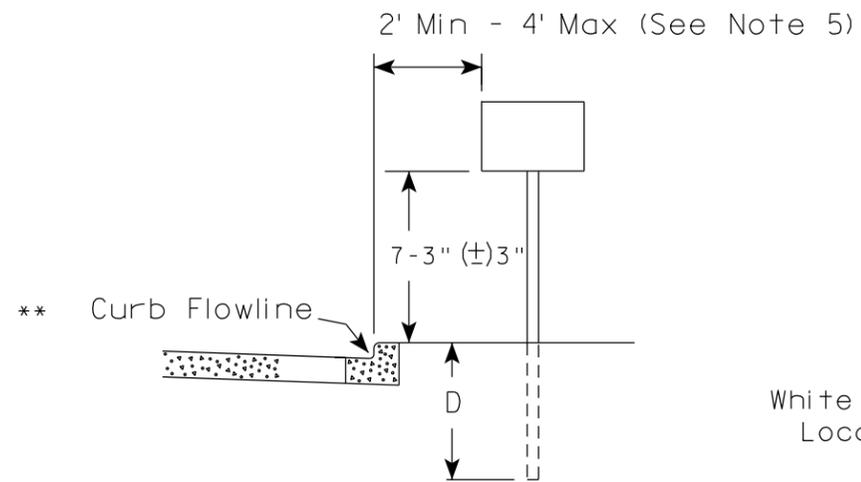
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

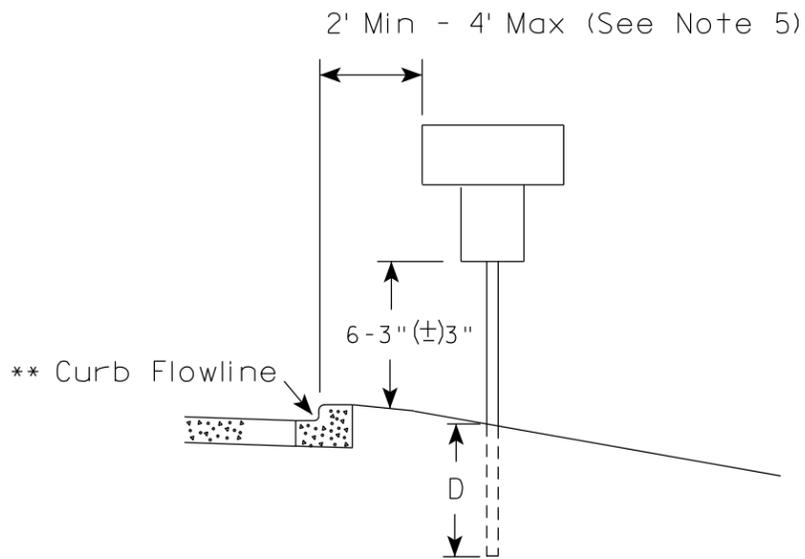
URBAN AREA

RURAL AREA (See Note 2)



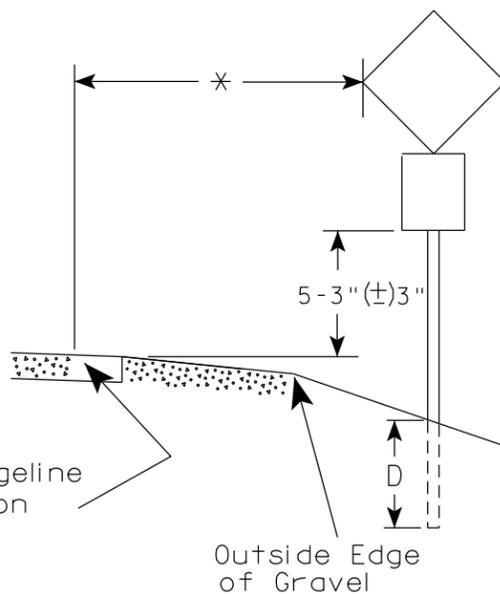
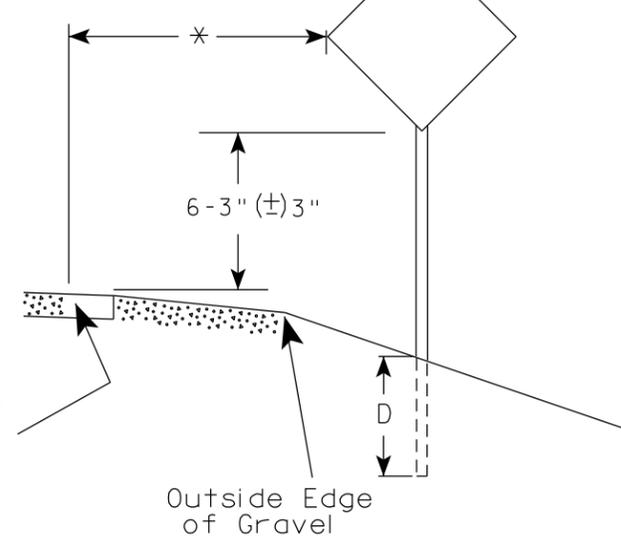
White Edgeline Location

Outside Edge of Gravel



White Edgeline Location

Outside Edge of Gravel



POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".
3. For expressways and freeways, mounting height is 7'- 3" (±) 3" or 6'-3" (±) 3" depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±) 3".
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the Engineer.

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

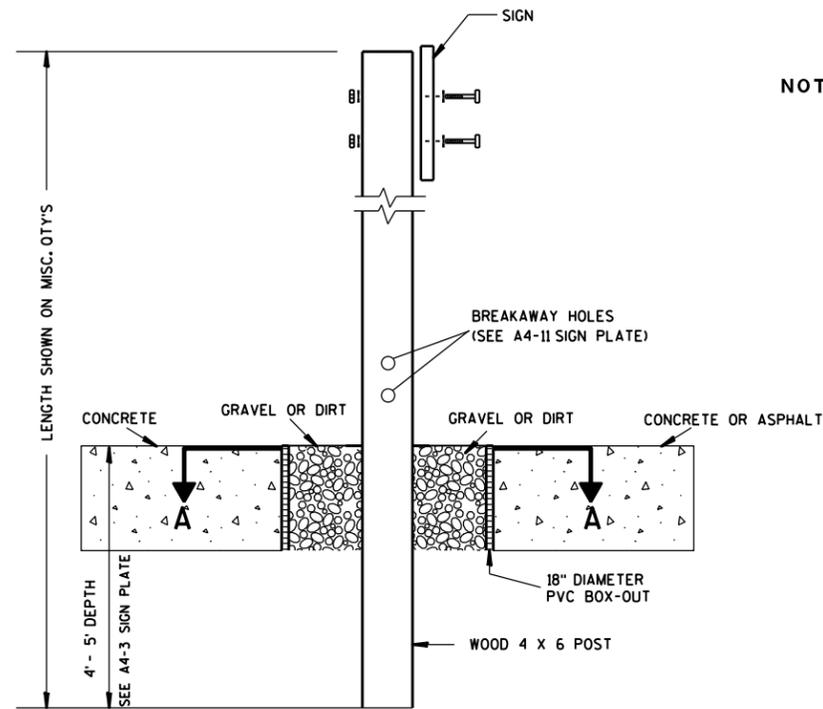
WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R. Raub
for State Traffic Engineer

DATE 12/6/23

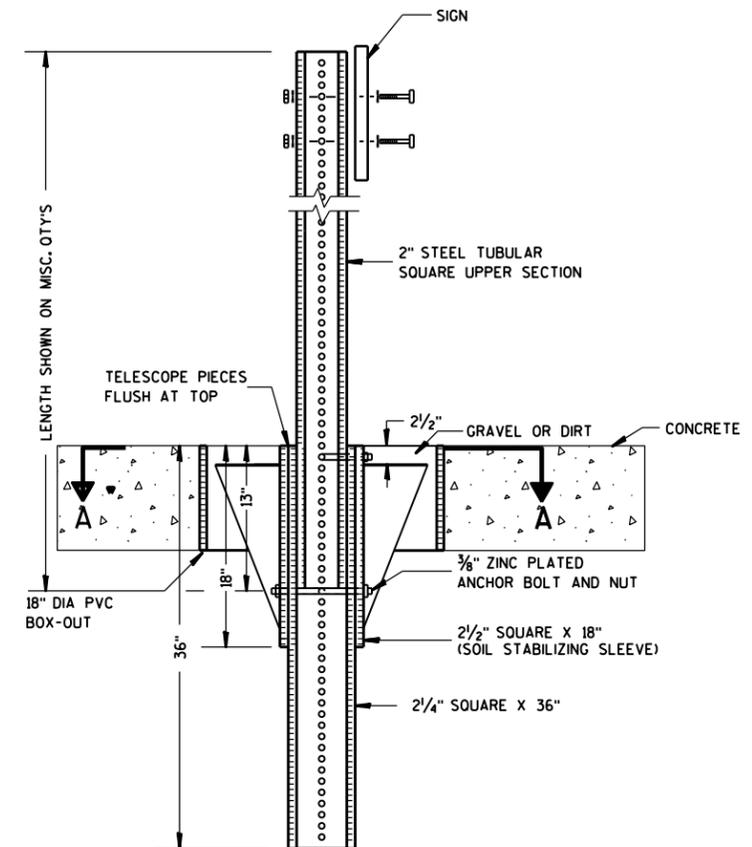
PLATE NO. A4-3.23



ELEVATION VIEW

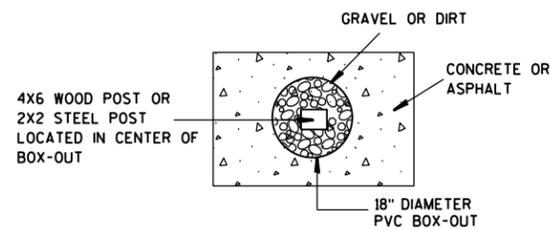
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

**SIGN POST
BOX-OUTS
A4-3B**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

7

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GENERAL NOTES

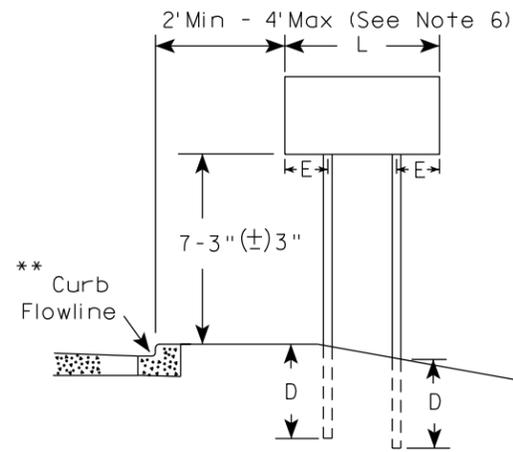
1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) 3" or 6'-3" (±) 3" depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

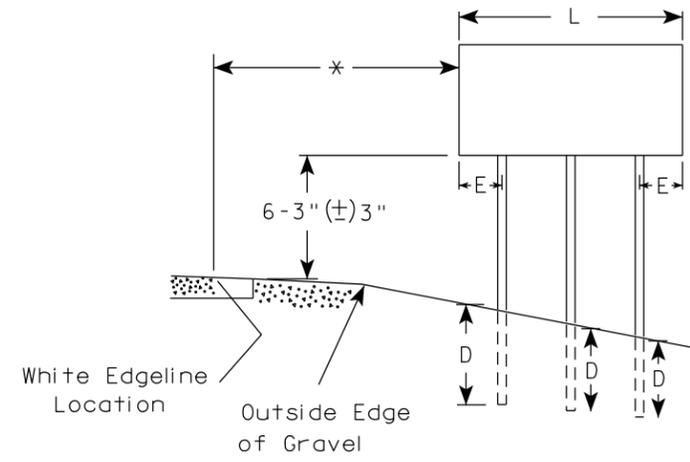
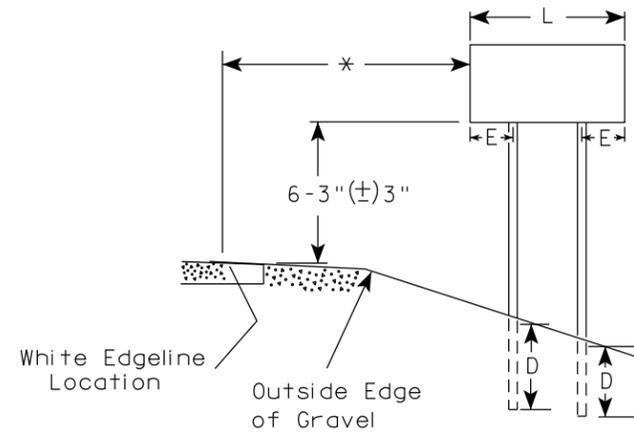
** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

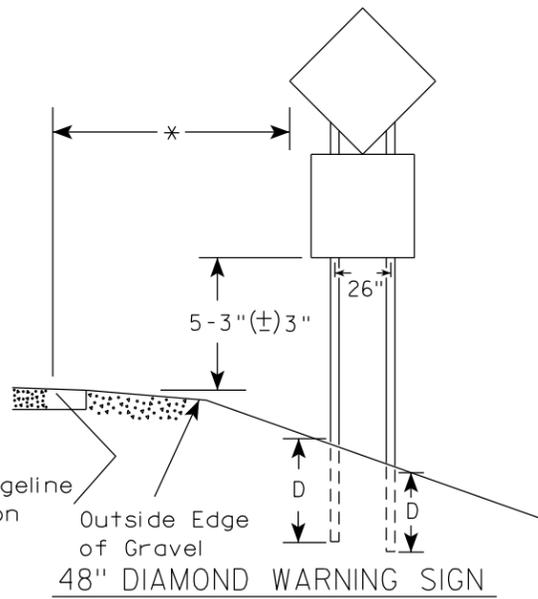
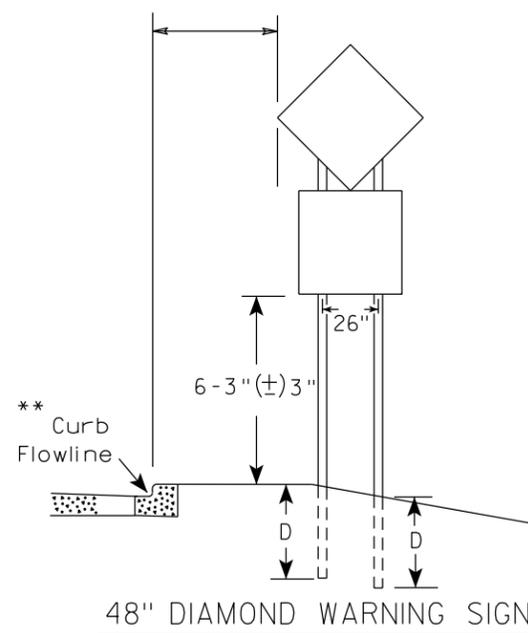
URBAN AREA



RURAL AREA (See Note 3)



URBAN AREA



SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

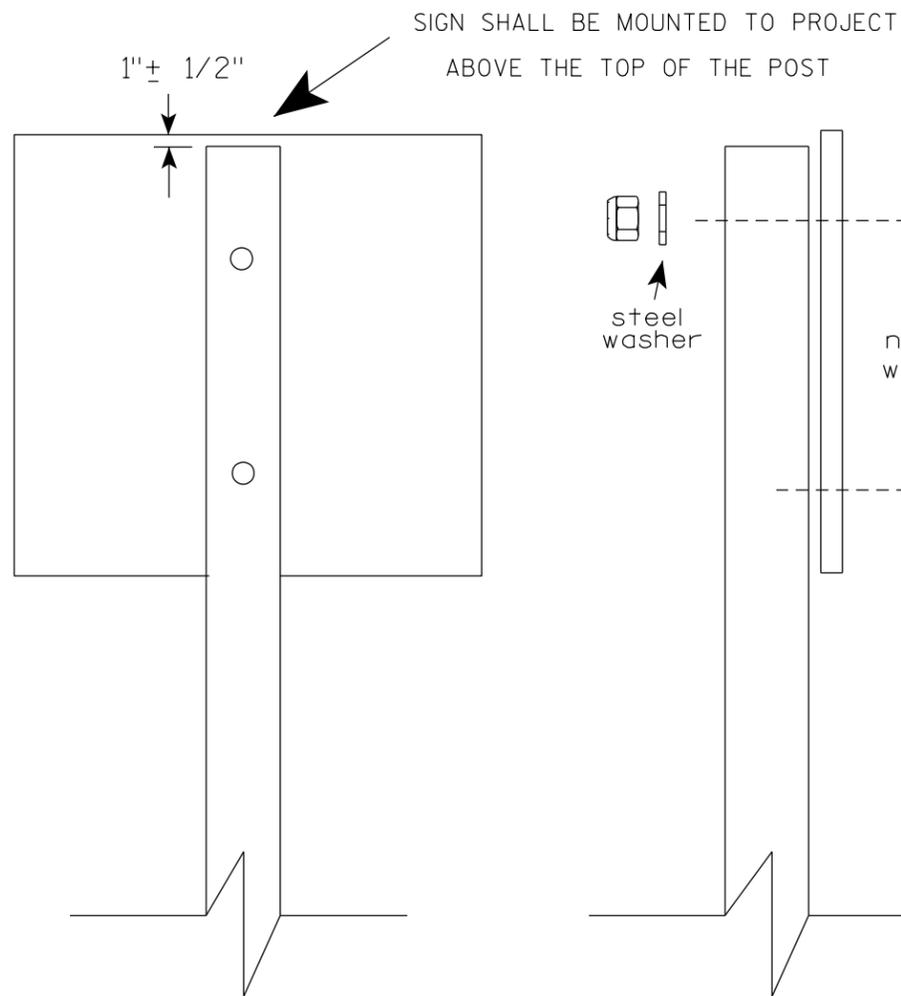
Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-4.16



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS
TO POSTS

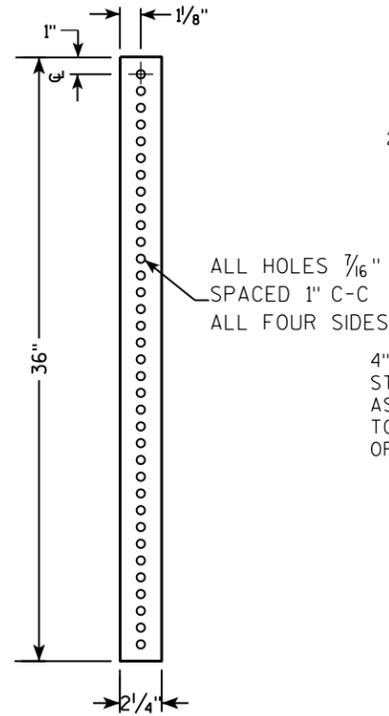
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

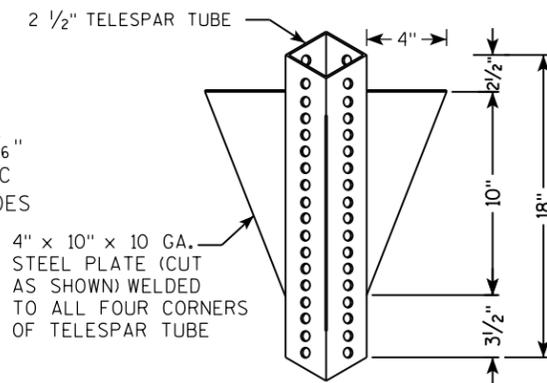
DATE 4/1/2020 PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

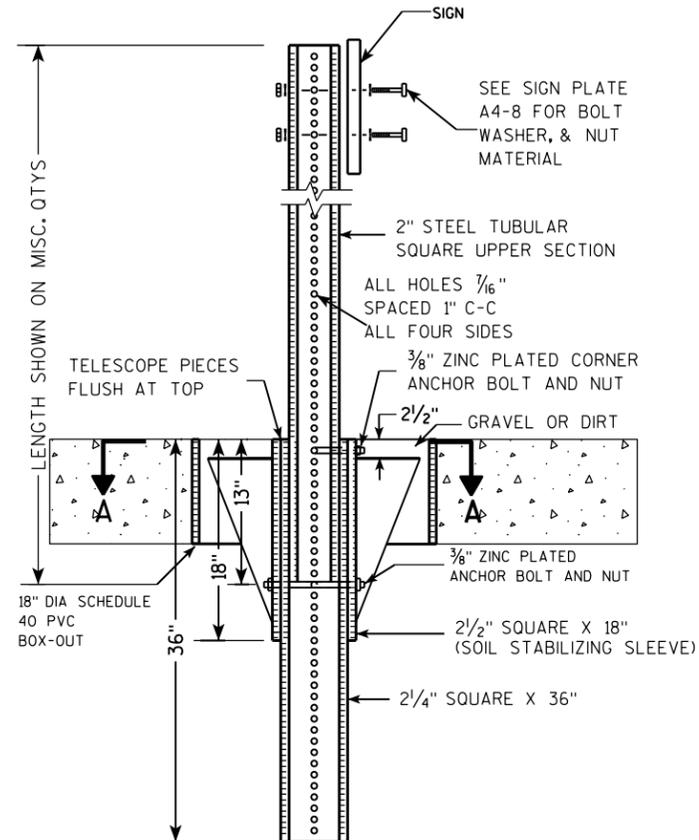
2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



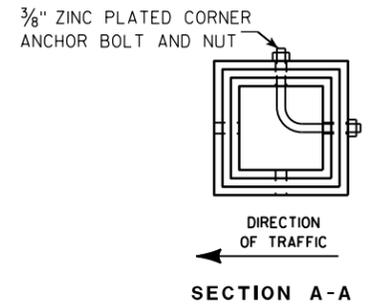
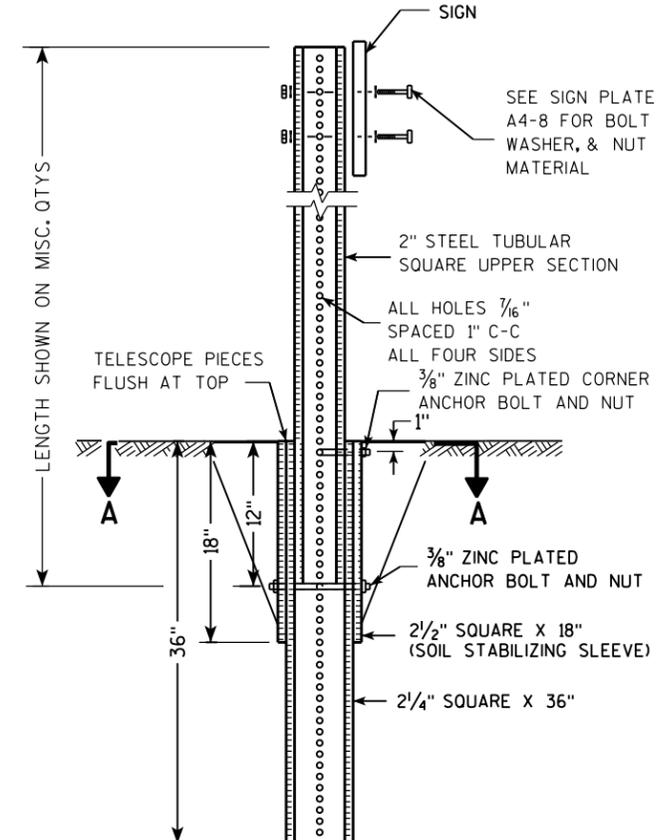
2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

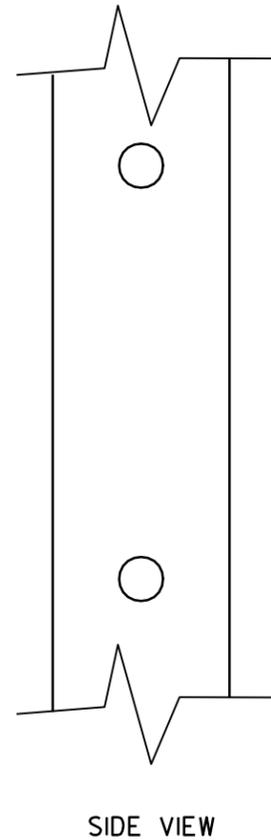
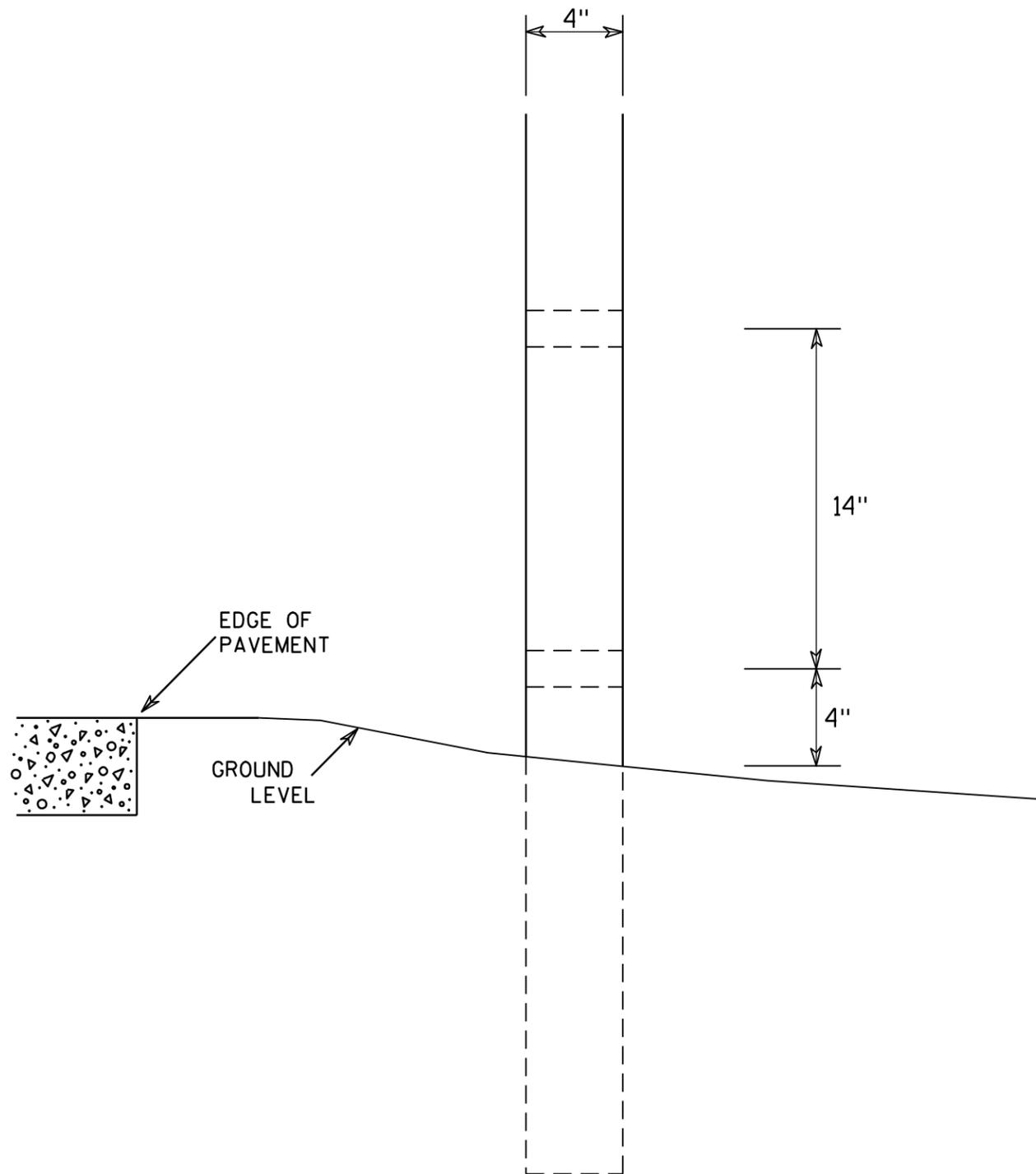
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

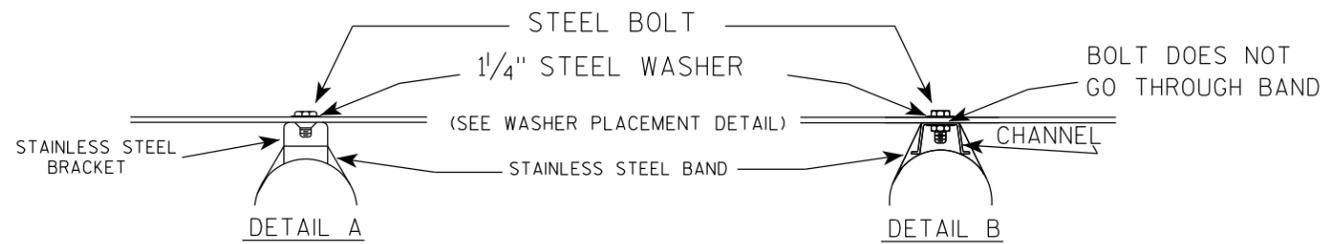
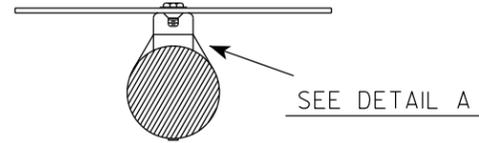
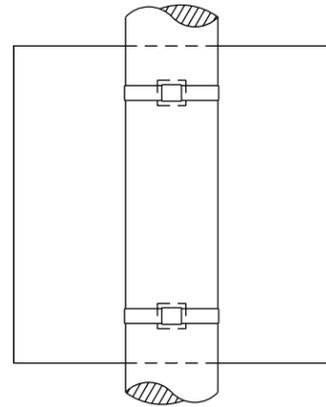
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4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	<i>Chester J Spang</i> for State Traffic Engineer
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

BANDING

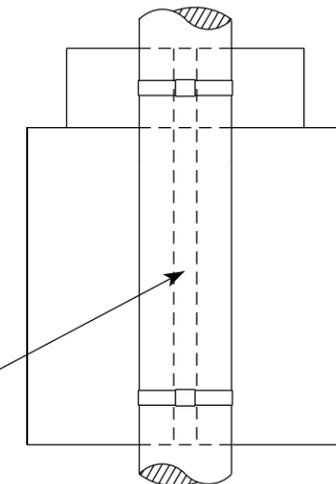
SINGLE SIGN



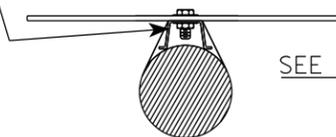
GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be $\frac{3}{4}$ " in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

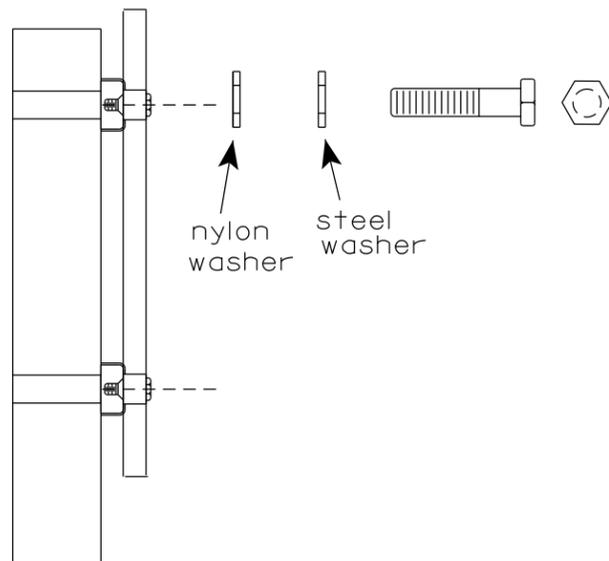
"J" ASSEMBLY



CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET

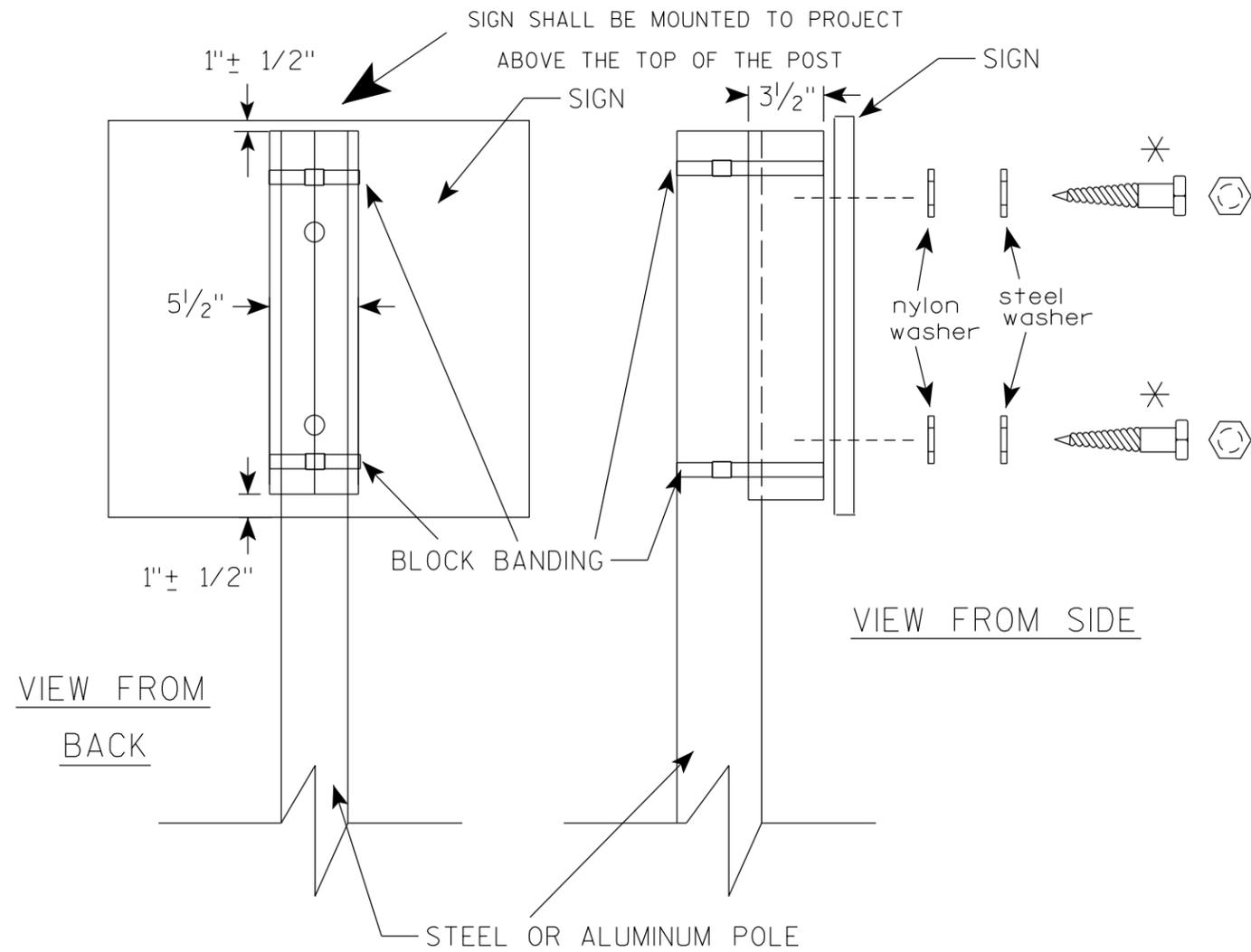


WASHER PLACEMENT



WASHERS (ALL POSTS) -
 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
 1-1/4" O.D. X 3/8" I.D. X .080 NYLON
 FOR ALL TYPE H SIGNS

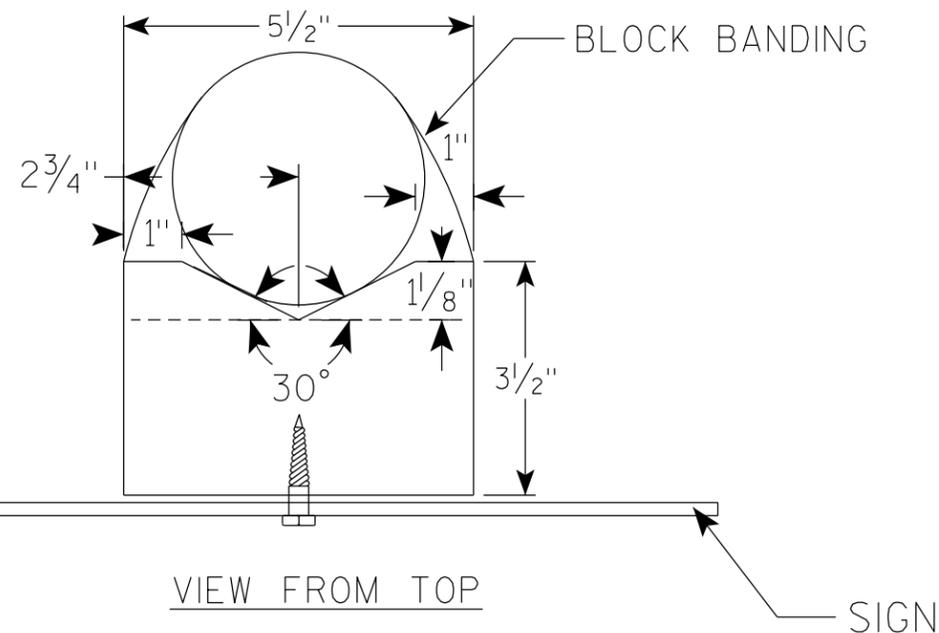
STANDARD SIGN SIGN BANDING DETAILS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> for State Traffic Engineer
DATE 6/10/19	PLATE NO. A5-9.4



GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

* LAG BOLTS SHALL BE 3/8" X 2 1/2"

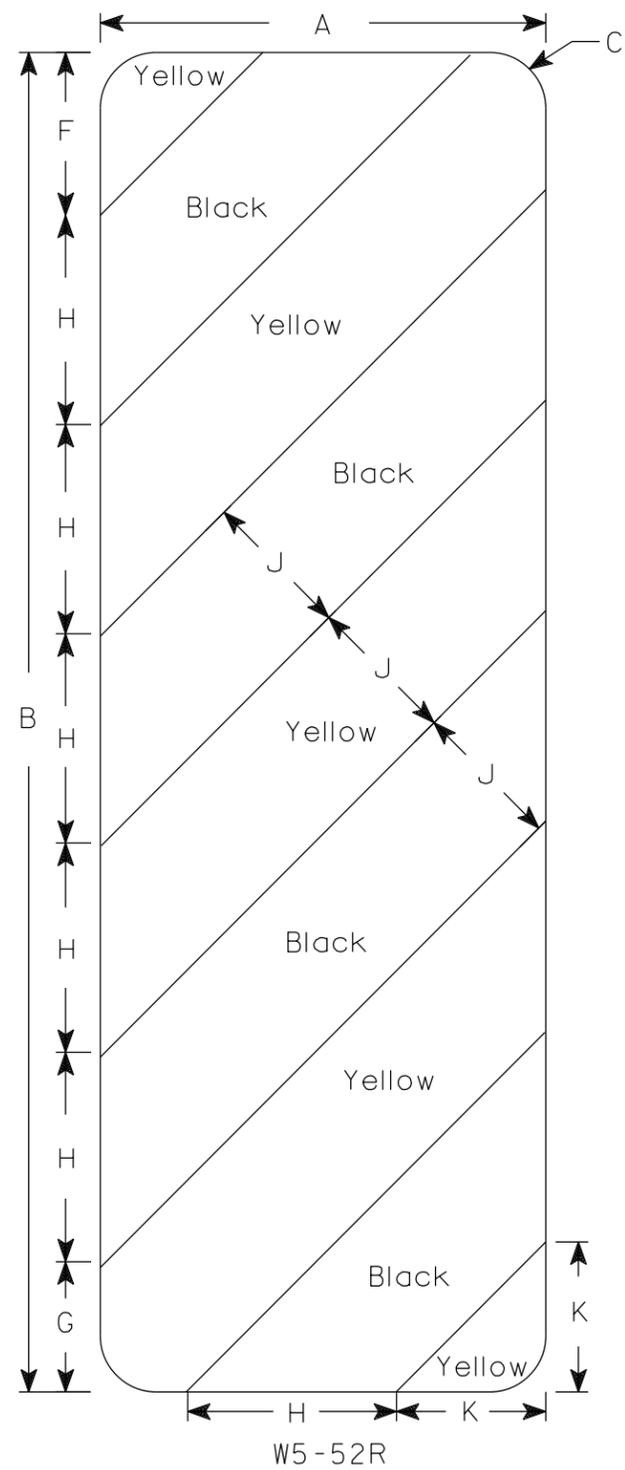
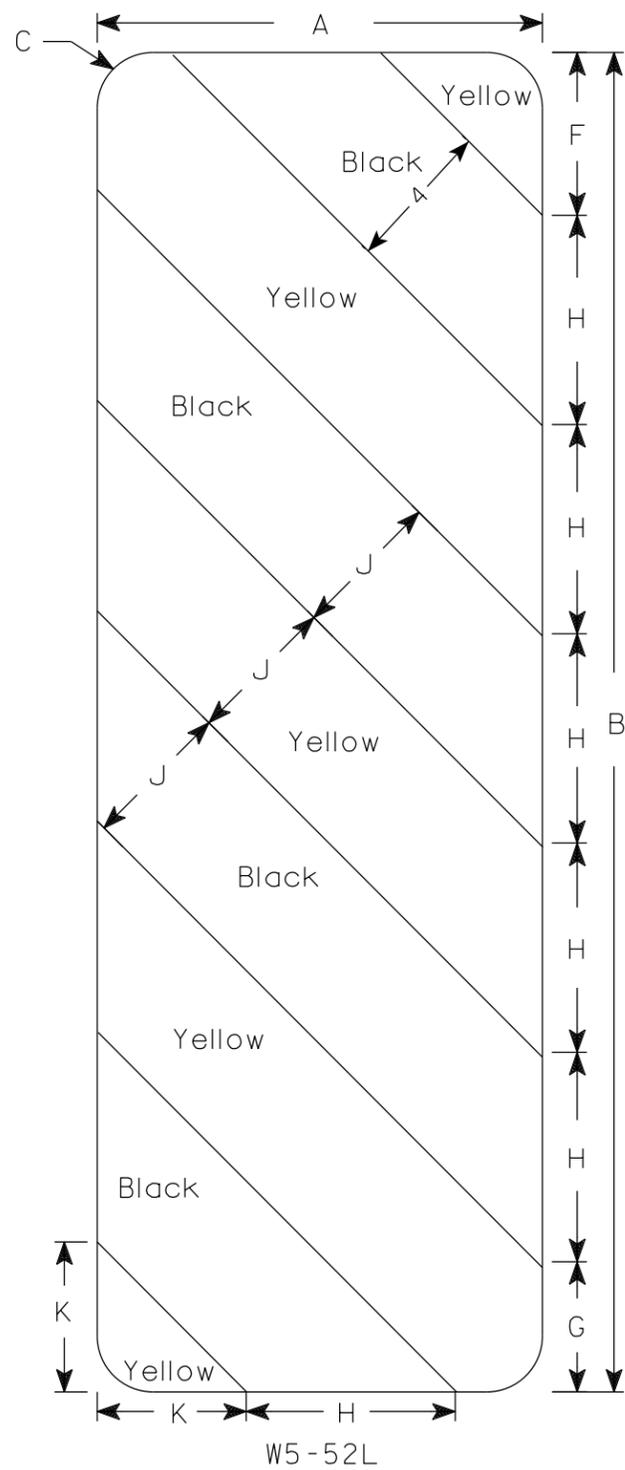


BLOCK BANDING DETAIL
(V-BLOCK OPTION)

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 4/19/2022 PLATE NO. A5-10.3



NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Yellow
Message - Black
3. Alternate colors of stripes as shown.

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SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	36	1 1/2			4 3/8	3 1/2	5 5/8	45°	4	4																3.0
2M	12	36	1 1/2			4 3/8	3 1/2	5 5/8	45°	4	4																3.0
3	18	54	1 1/2			6	5 1/2	8 1/2	45°	6	6 9/16																6.75
4																											
5																											

STANDARD SIGN
W5-52L & W5-52R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/4/2024 PLATE NO. W5-52.10

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BEVEL EXPOSED EDGES OF CONCRETE 3/4" UNLESS OTHERWISE NOTED.

THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES B-61-294 " SHALL BE THE EXISTING GROUNDLINE.

AT THE BACK FACE OF ABUTMENT ALL VOLUME WHICH CANNOT BE PLACED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL TYPE A.

EXCAVATION BELOW THE ABUTMENT AND ABUTMENT BEDDING MATERIALS REQUIRES ENGINEER APPROVAL. GEOTEXTILE SHALL BE SET AT THE BOTTOM OF EXCAVATION AND EXTEND 2'-0" ABOVE BOTTOM OF ABUTMENT.

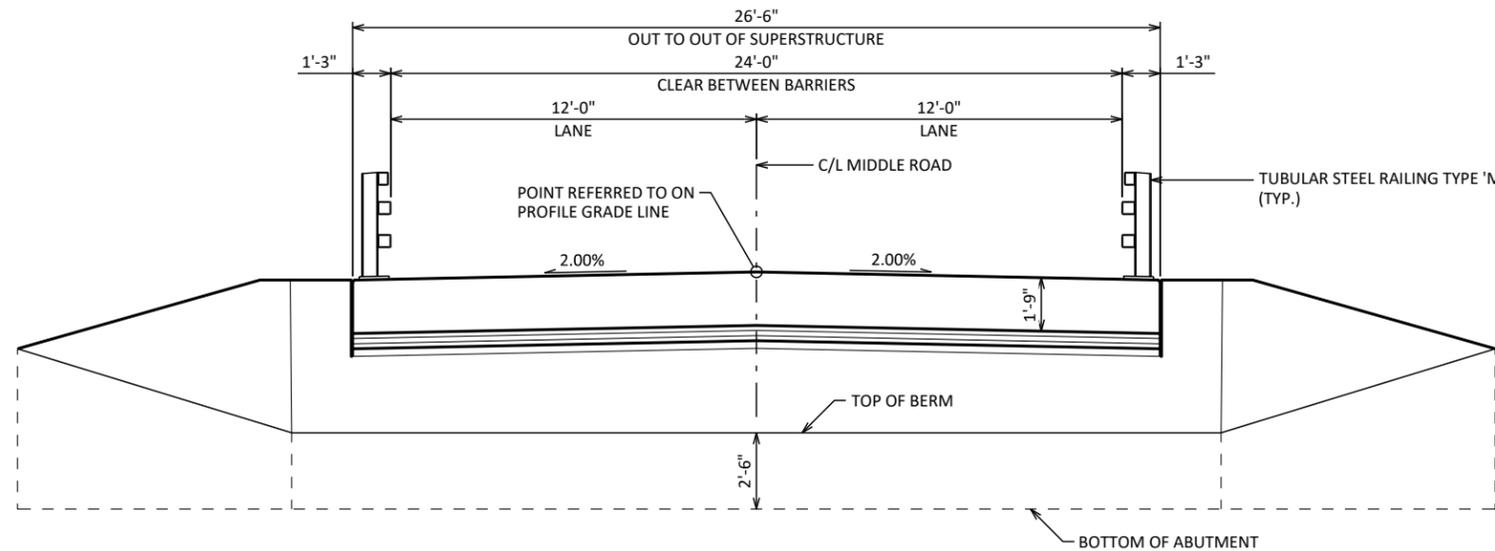
THE QUANTITY FOR BACKFILL STRUCTURE IS CALCULATED BASED ON THE DETAIL SHOWN IN THE PLANS.

THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH HEAVY RIPRAP AND GEOTEXTILE TYPE HR TO THE EXTENT SHOWN ON SHEET 1 AND THE ABUTMENT DETAILS.

AT ABUTMENTS, CONCRETE POURED UNDER WATER WILL BE ALLOWED AND SHALL BE DONE IN ACCORDANCE WITH SECTION 502.3.5.3 OF THE STANDARD SPECIFICATIONS.

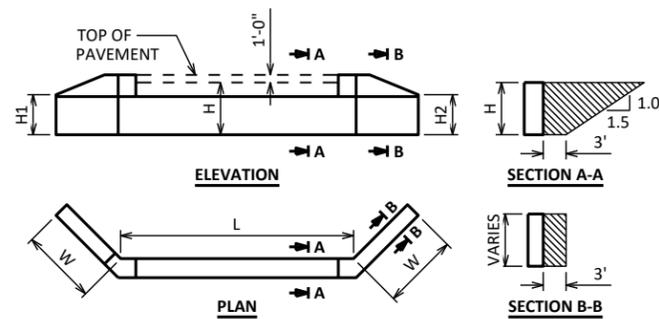
SLAB FALSEWORK SHALL BE SUPPORTED ON PILES OR THE SUBSTRUCTURE UNLESS AN ALTERNATE METHOD IS APPROVED BY THE ENGINEER.

PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO ENTIRE EXPOSED TOP OF SLAB, INCLUDING THE SLAB EDGE AND 1'-0" UNDER THE SLAB, THE TOP AND EXTERIOR EXPOSED FACE OF WINGS AND FRONT FACE OF ABUTMENT TO 1'-0" PAST THE EDGE OF SLAB.



CROSS SECTION THRU ROADWAY

LOOKING UPSTATION
(PILING NOT SHOWN FOR CLARITY)

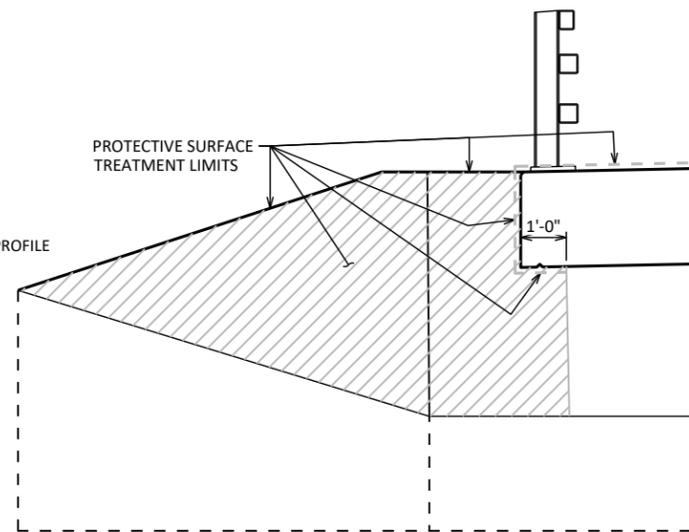
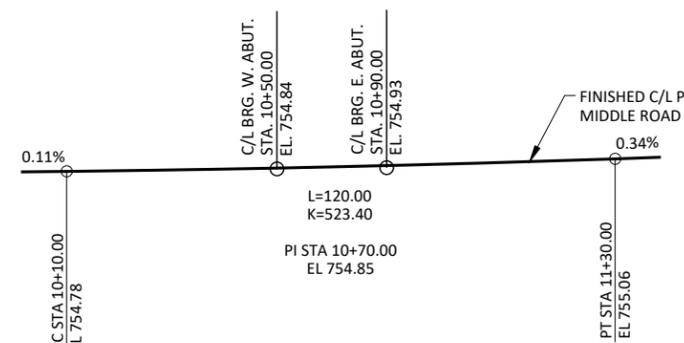


ABUTMENT BACKFILL DIAGRAM

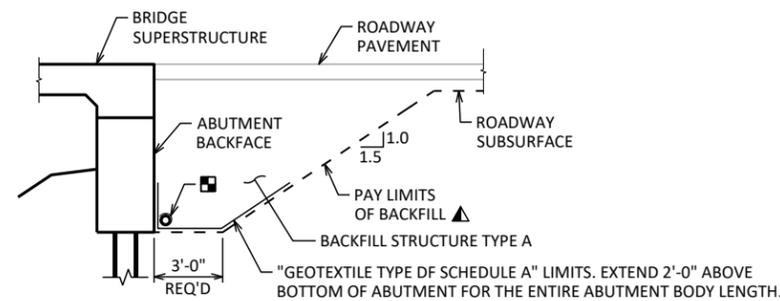
- L = ABUTMENT BODY LENGTH AT BACKFACE (FT)
- H = AVERAGE ABUTMENT FILL HEIGHT (FT)
- H1 = WING 1 HEIGHT AT TIP (FT)
- H2 = WING 2 HEIGHT AT TIP (FT)
- W = WING LENGTH (FT)
- EF = EXPANSION FACTOR (1.20 FOR CY BID ITEMS AND 1.00 FOR TON BID ITEMS)
- $V_{CF} = (L)(3.0')(H) + (L)(0.5)(1.5H)(H) + (3')(0.5)(H1+H2+H+H)(W)$
- $V_{CY} = V_{CF}(EF)/27$
- $V_{TON} = V_{CY}(2.0)$

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	SUPER	WEST ABUT.	EAST ABUT.	TOTALS
203.0250	REMOVING STRUCTURE OVER WATERWAY REMOVE DEBRIS P-61-0195	EACH	---	---	---	1
206.1001	EXCAVATION FOR STRUCTURES BRIDGES B-61-294	EACH	---	---	---	1
210.1500	BACKFILL STRUCTURE TYPE A	TON	---	150	150	300
502.0100	CONCRETE MASONRY BRIDGES	CY	77	26	26	129
502.3200	PROTECTIVE SURFACE TREATMENT	SY	148	15	15	178
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	---	2,090	2,090	4,180
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	16,140	1,520	1,520	19,180
513.4061	RAILING TUBULAR TYPE M	LF	90	---	---	90
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	---	6	6	12
550.2104	PILING CIP CONCRETE 10 3/4 X 0.25-INCH	LF	---	595	595	1,190
606.0300	RIPRAP HEAVY	CY	---	95	95	190
612.0406	PIPE UNDERDRAIN WRAPPED 6-INCH	LF	---	70	70	140
645.0111	GEOTEXTILE TYPE DF SCHEDULE A	SY	---	44	44	88
645.0120	GEOTEXTILE TYPE HR	SY	---	160	160	320
NON-BID ITEMS						
	FILLER	SIZE	---	---	---	1/2", 3/4"
	NAME PLATE					



PROTECTIVE SURFACE TREATMENT DETAILS

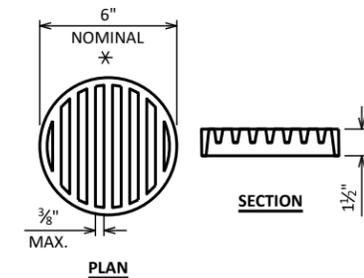


TYPICAL SECTION THRU ABUTMENT

- ▲ BACKFILL PAY LIMITS. BACKFILL BEYOND PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.
- PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN.

BENCH MARKS

NO.	STA.	DESCRIPTION	ELEV.
11	10+05	3/4" IRON REBAR SET, 60.59' LT.	747.17
3	10+72	3/4" IRON REBAR SET, 55.52' LT.	747.02
12	11+35	3/4" IRON REBAR SET, 61.53' RT.	747.88



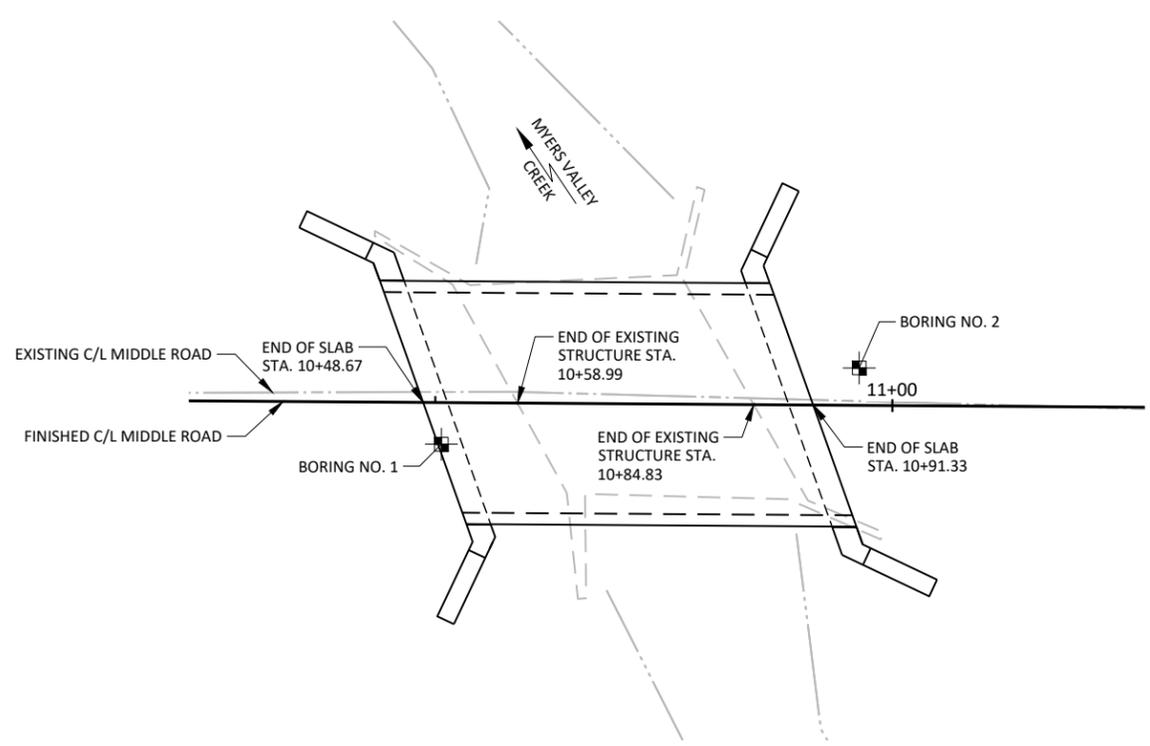
RODENT SHIELD DETAIL

* DIMENSIONS ARE APPROXIMATE. THE GRATE IS SIZED TO FIT INTO A PIPE COUPLING. ORIENT SO SLOTS ARE VERTICAL.

THE RODENT SHIELD, PIPE COUPLING AND SCREWS SHALL BE CONSIDERED INCIDENTAL TO THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6-INCH".

THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE EXPOSED END OF THE PIPE UNDERDRAIN. THE SHIELD SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1-INCH STAINLESS STEEL SHEET METAL SCREWS.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-61-294			
DRAWN BY ZMF		PLANS CK'D RBH	
CROSS SECTION & QUANTITIES			SHEET 2 OF 10



PLAN B-61-294

SOIL BORINGS			
BORING NUMBER	DATE COMPLETED	NORTHING (Y)	EASTING (X)
1	06/20/24	392,304.25	806,902.70
2	06/24/24	392,312.07	806,948.41

BORINGS & REPORT COMPLETED BY: AMERICAN ENGINEERING TESTING
1837 COUNTY HIGHWAY OO
CHIPPEWA FALLS, WI 54729

STATE PROJECT NUMBER
7276-00-76

MATERIAL SYMBOLS

Asphalt	Topsoil	Peat
Concrete	Fill	Gravel
Sand	Clay	Silt
Boulders or Cobbles	Limestone	Bedrock (unknown)
Shale	Sandstone	Igneous/meta

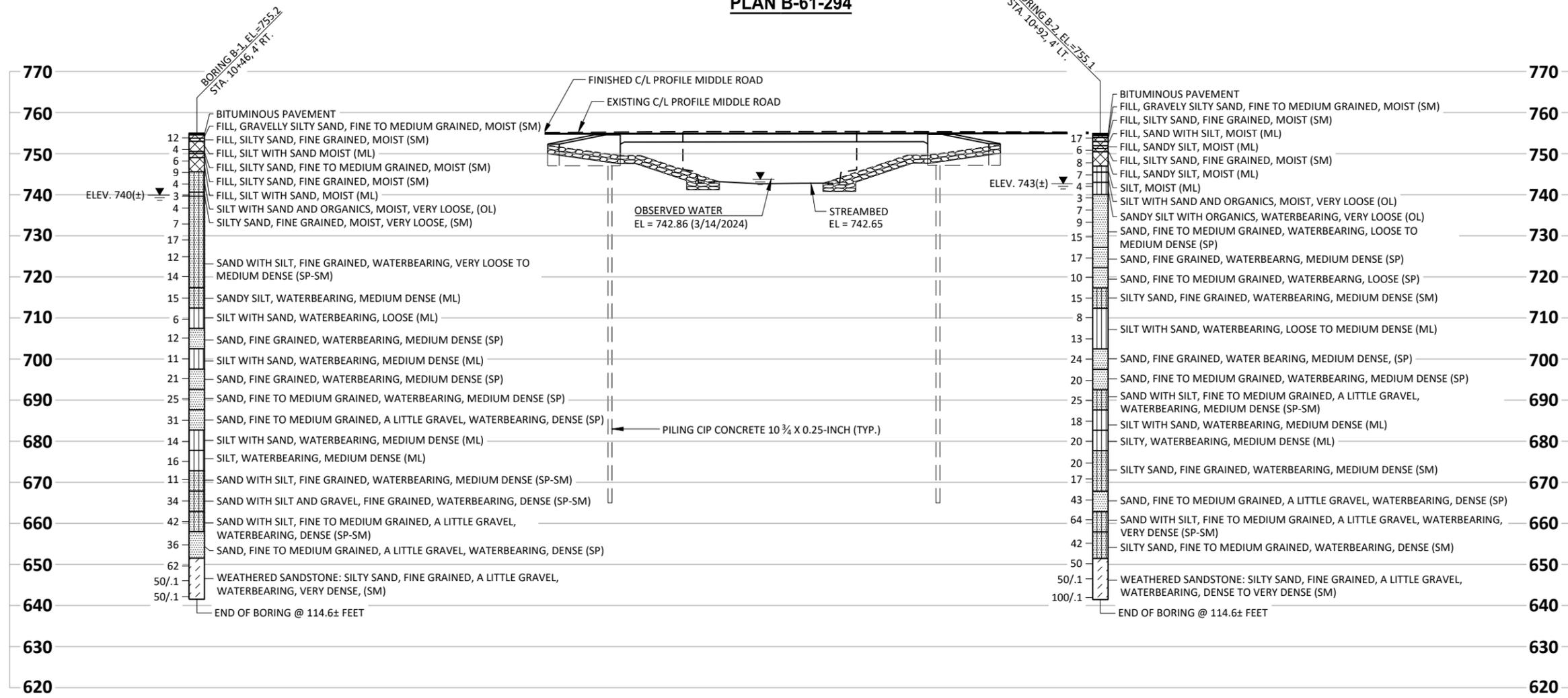
LEGEND OF BORING

(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)
(2) UNLESS OTHERWISE SPECIFIED, THE SPT 'N' VALUE IS BASED ON AASHTO T-206 STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATIONS

- ▽ AT TIME OF DRILLING
- ▽ END OF DRILLING
- ▽ AFTER DRILLING

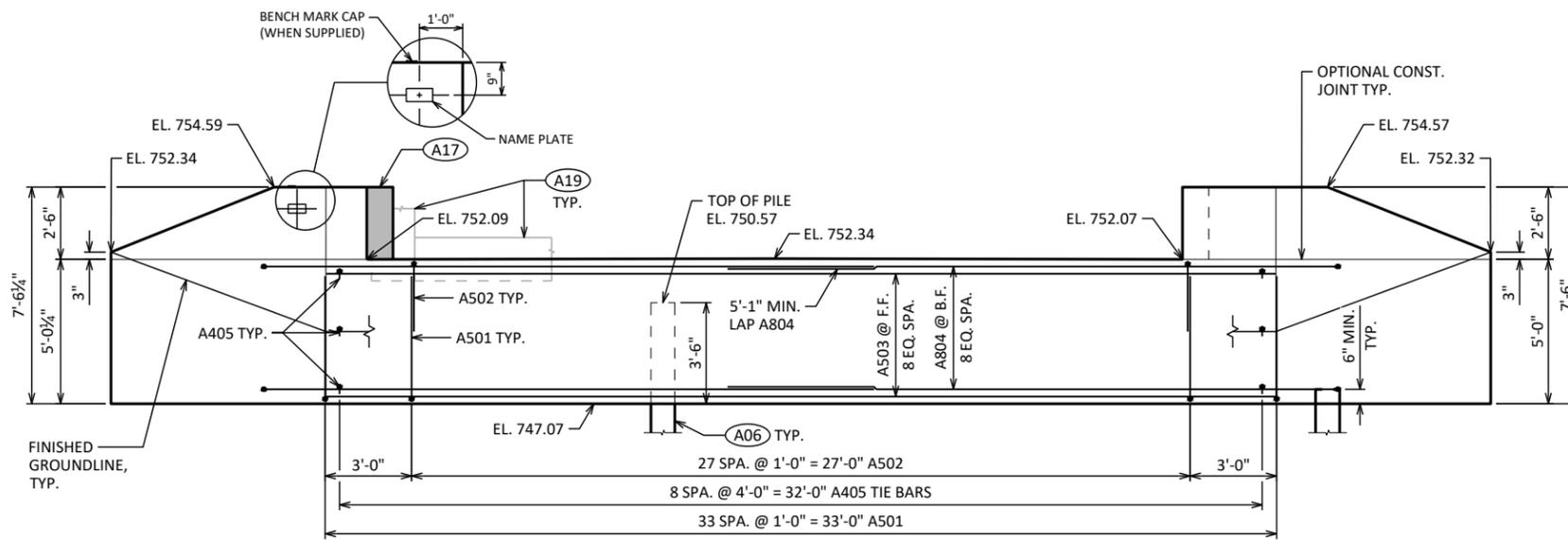
ABBREVIATIONS
F-FINE M-MEDIUM C-COURSE ST-SHELBY TUBE



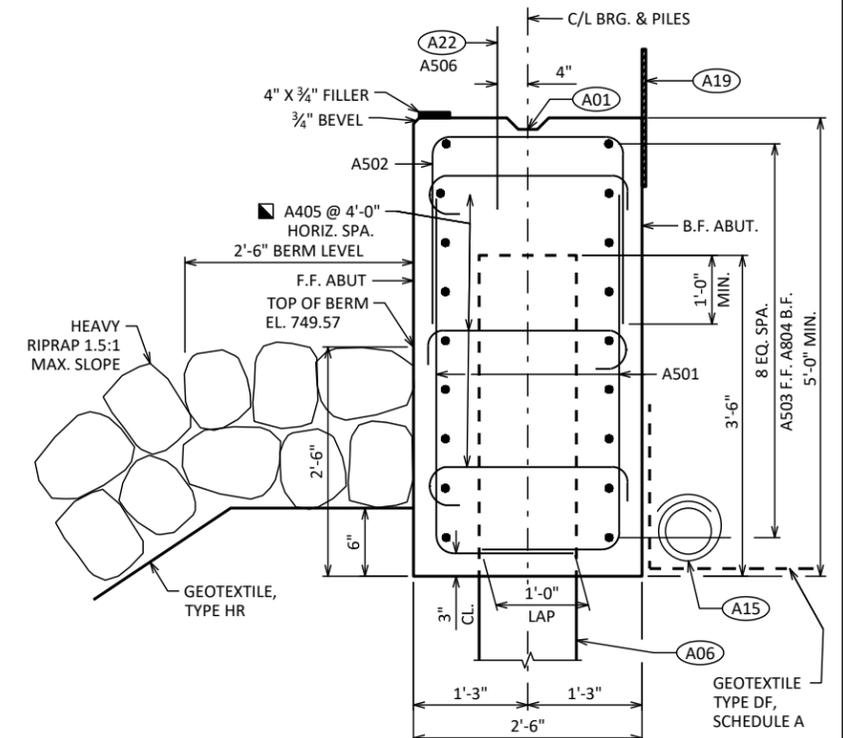
SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.

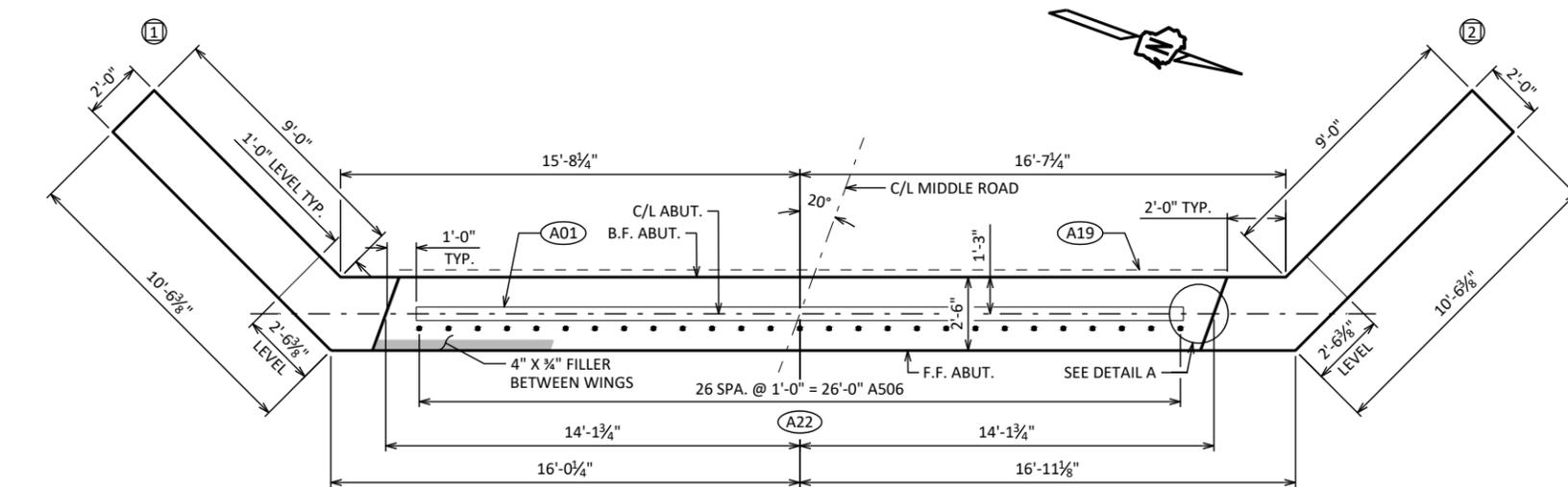
NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-61-294			
DRAWN BY: ZMF		PLANS CK'D: RBH	
SUBSURFACE EXPLORATION		SHEET 3 OF 10	



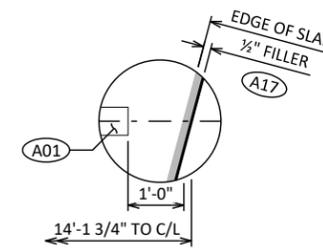
ELEVATION
LOOKING DOWNSTATION



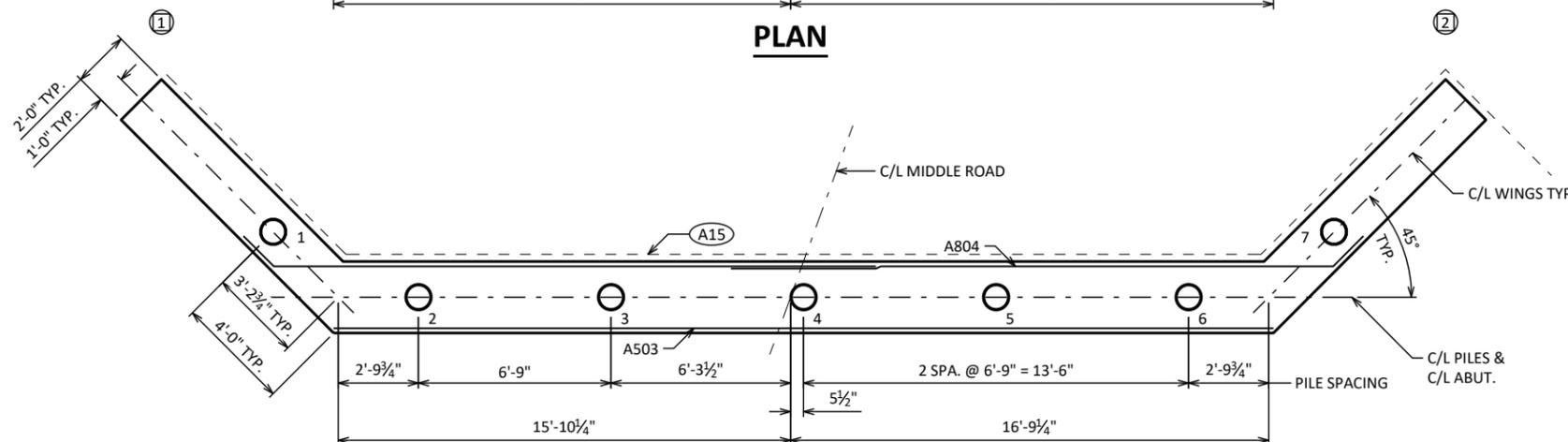
SECTION THRU BODY



PLAN



DETAIL A



PILE PLAN

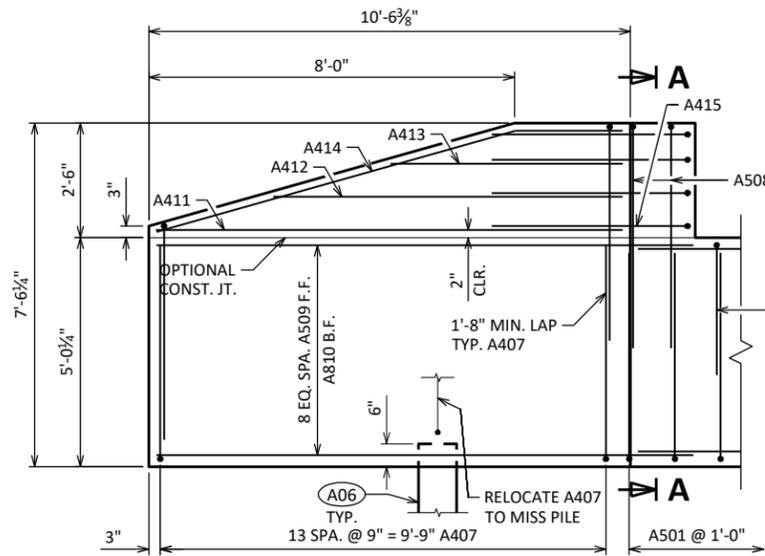
- (A01) CONST. JOINT: KEYWAY FORMED BY A BEVELED 2X6.
- (A06) SUPPORT ABUTMENT ON CIP 10% X 0.25 PILING, ESTIMATED 85 FT LONG WITH A REQUIRED DRIVING RESISTANCE OF 120 TONS PER PILE.
- (A15) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.
- (A17) 1/2" FILLER: SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 3/8" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.
- (A19) 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZ. & VERT. JOINTS AT BACKFACE.
- (A22) A506 BARS SPACED @ 1'-0" CNTRS. MAY BE PLACED AFTER CONCRETE IS POURED BUT BEFORE INITIAL SET HAS TAKEN PLACE. (EMBED 1'-0" INTO CONC.)
- ALTERNATE THE POSITION OF THE 90° AND 180° HOOKS AT EACH VERTICAL LAYER OF TIES.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-61-294			
DRAWN BY		PLANS CK'D	
ZMF		RBH	
WEST ABUTMENT			SHEET 4 OF 10

BILL OF BARS

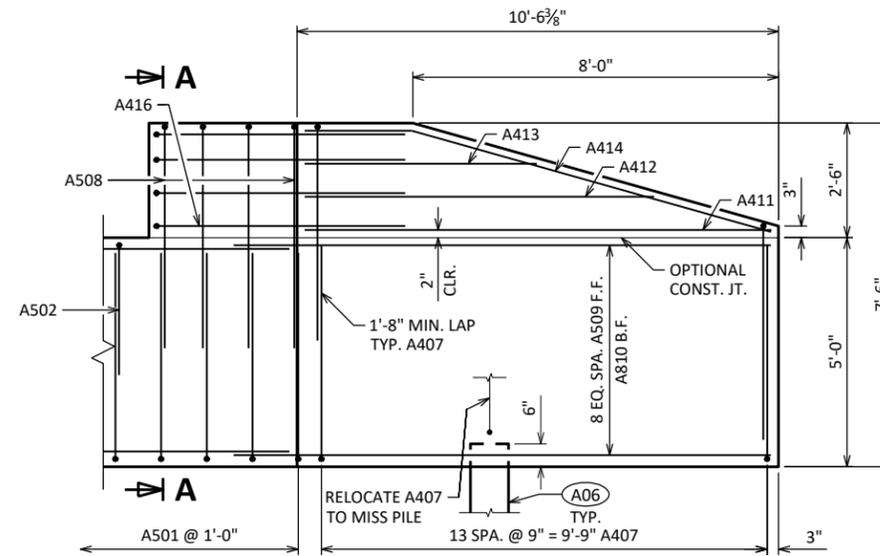
NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BAR MARK	COUNT	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
A501		68	6'-0"	X		ABUT BODY STIRRUPS
A502		28	7'-7"	X		ABUT BODY STIRRUPS - TOP U-BAR
A503		9	32'-11"			ABUT BODY HORIZ. - F.F.
A804		18	22'-5"	X		ABUT BODY HORIZ. - B.F.
A405		27	3'-0"	X		ABUT BODY TIE BARS
A506	X	27	2'-0"			ABUT BODY DOWEL BARS
A407	X	56	10'-10"	X		WING STIRRUPS
A508	X	6	11'-7"	X		WING CORNER STIRRUPS
A509	X	18	11'-9"	X		WING LOWER HORIZ. - F.F.
A810	X	18	13'-3"	X		WING LOWER HORIZ. - B.F.
A411	X	4	10'-1"			WING UPPER HORIZ.
A412	X	4	7'-7"			WING UPPER HORIZ.
A413	X	4	5'-0"			WING UPPER HORIZ.
A414	X	4	9'-8"	X		WING TOP HORIZ.
A415	X	4	7'-6"	X		WING 1 UPPER HORIZ. CORNER
A416	X	4	9'-5"	X		WING 2 UPPER HORIZ. CORNER



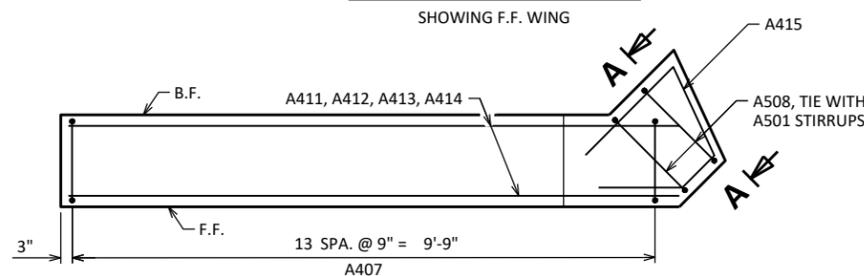
WING 1 ELEVATION

SHOWING F.F. WING



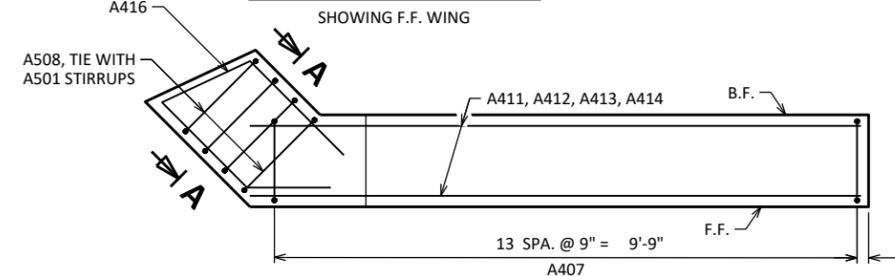
WING 2 ELEVATION

SHOWING F.F. WING



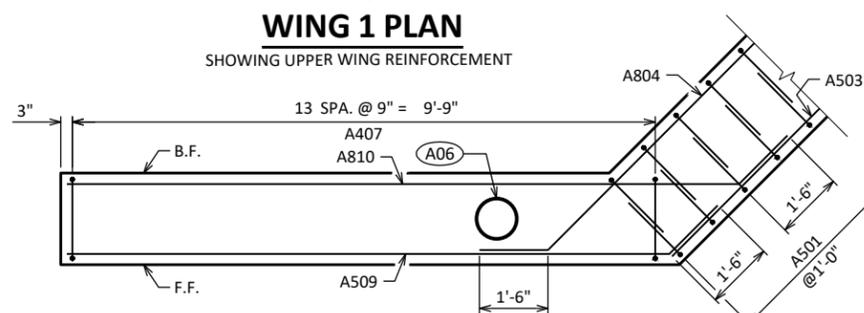
WING 1 PLAN

SHOWING UPPER WING REINFORCEMENT



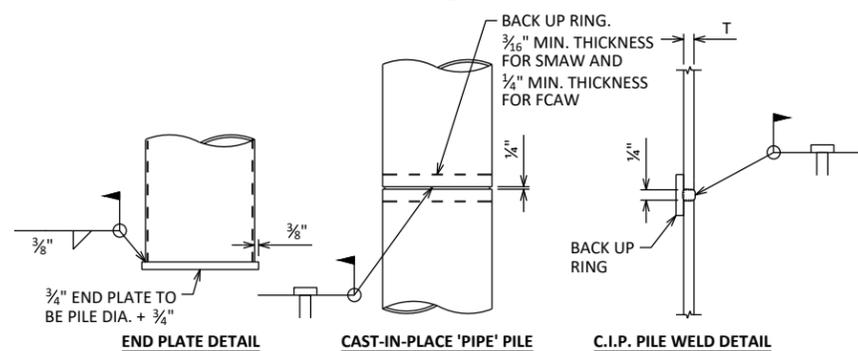
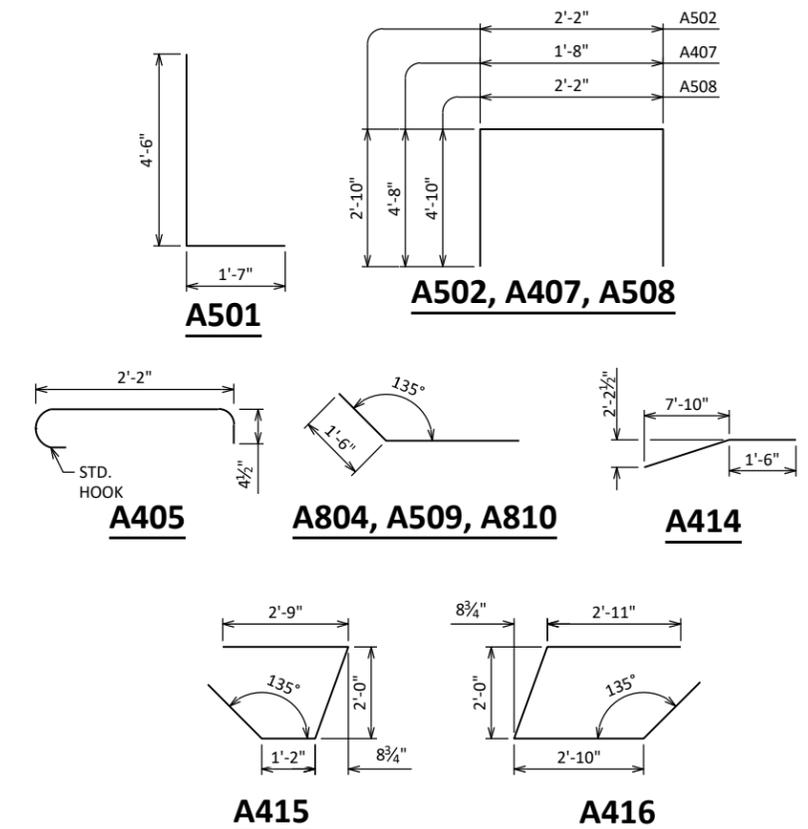
WING 2 PLAN

SHOWING UPPER WING REINFORCEMENT

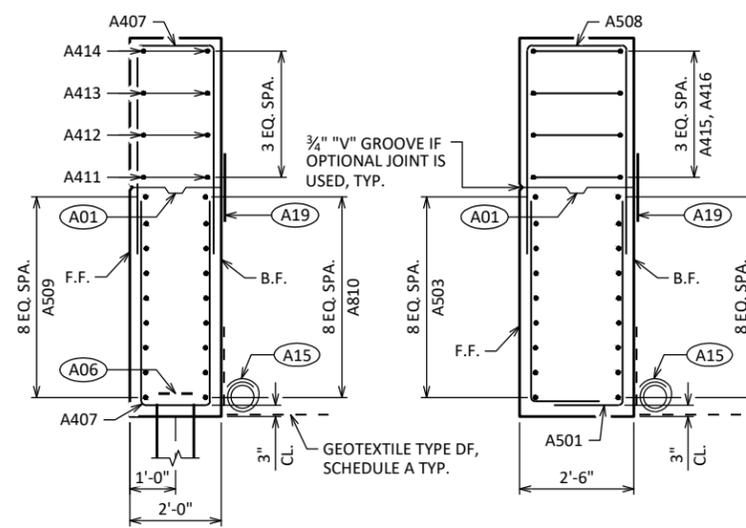


WING 1 PLAN

SHOWING LOWER WING REINFORCEMENT
WING 2 SIMILAR



CIP PILE DETAILS



SECTION THRU WING 1

TYPICAL BOTH WINGS

SECTION A-A

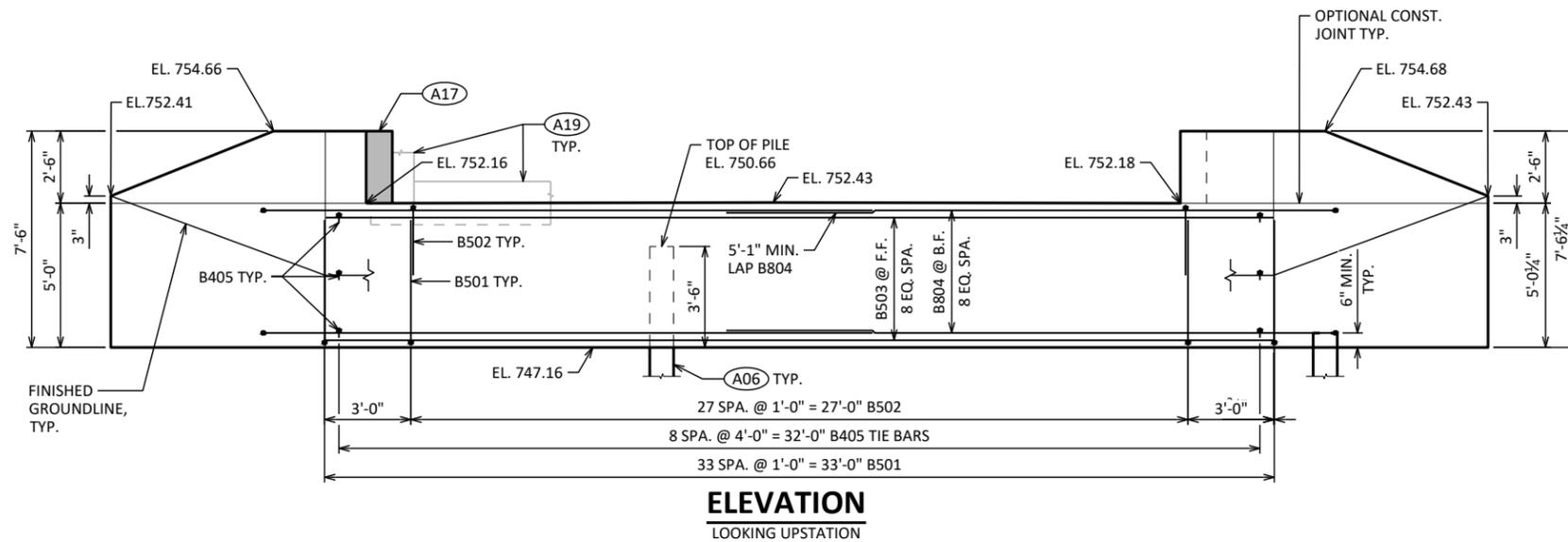
- (A01) OPTIONAL CONST. JOINT: KEYWAY FORMED BY A BEVELED 2X6. PROVIDE 3/4" "V" GROOVE ON F.F. OF WINGWALL IF JOINT IS USED.
- (A06) SUPPORT ABUTMENT ON CIP 10% X 0.25 PILING, ESTIMATED 85 FT LONG WITH A REQUIRED DRIVING RESISTANCE OF 120 TONS PER PILE.
- (A15) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.
- (A19) 18" RUBBERIZED MEMBRANE WATERPROOFING, ONLY IF OPTIONAL CONSTRUCTION JOINT IS USED. COST INCIDENTAL TO BID ITEM "CONCRETE MASONRY STRUCTURES".

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-61-294			
DRAWN BY		ZMF	PLANS CK'D RBH
WEST ABUTMENT DETAILS		SHEET 5 OF 10	

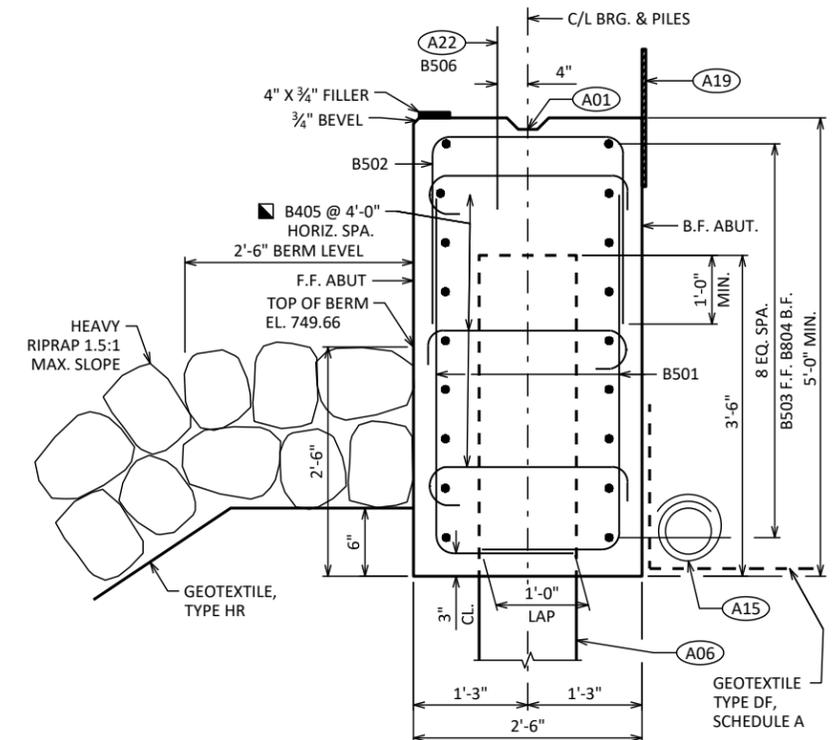
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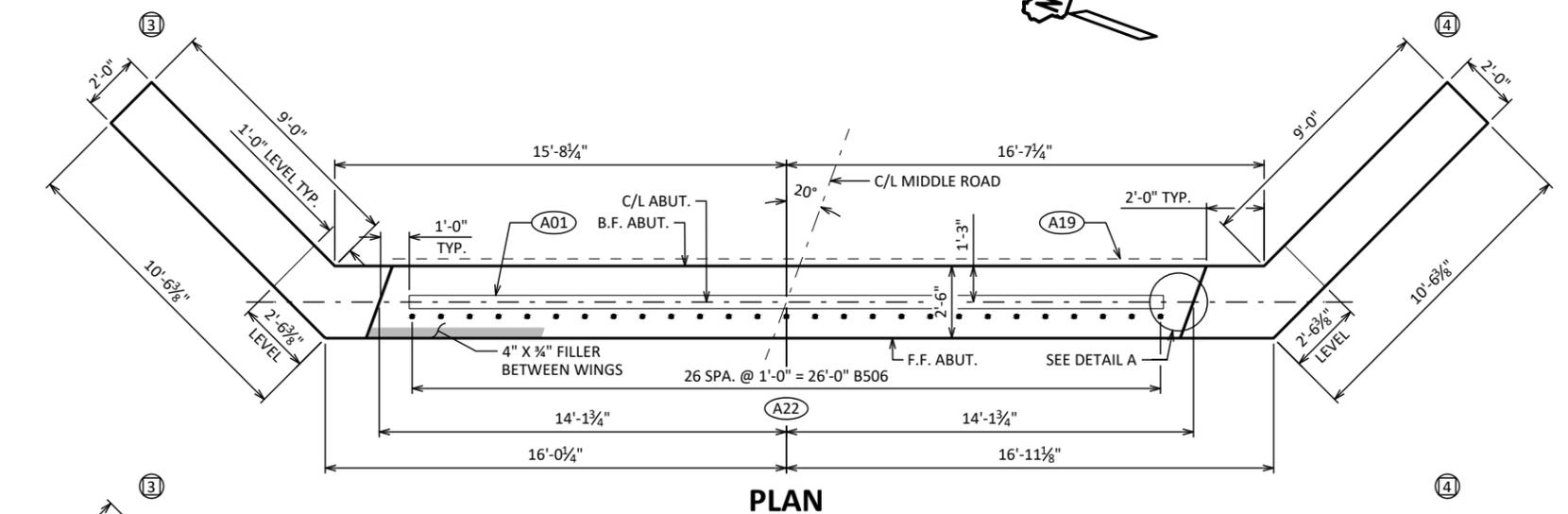
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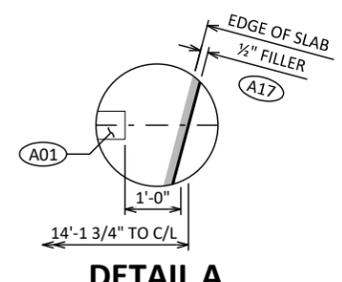
ELEVATION
LOOKING UPSTATION



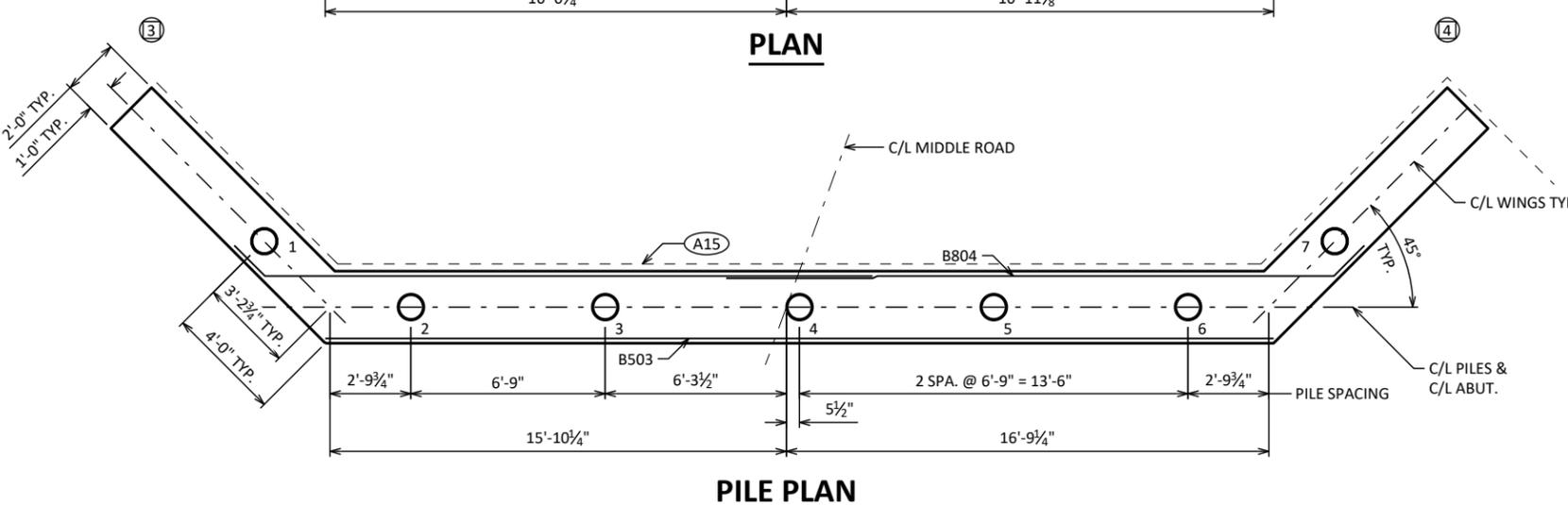
SECTION THRU BODY



PLAN



DETAIL A



PILE PLAN

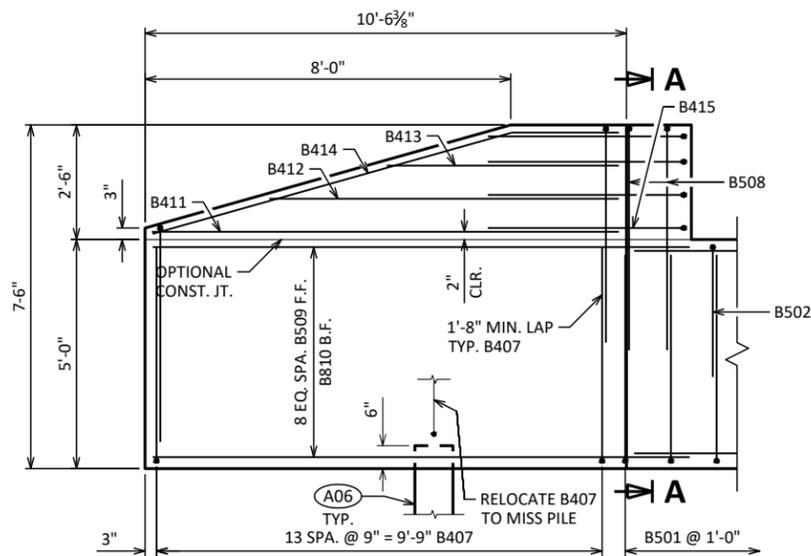
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- ALTERNATE THE POSITION OF THE 90° AND 180° HOOKS AT EACH VERTICAL LAYER OF TIES.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-61-294			
DRAWN BY		PLANS CK'D	
ZMF		RBH	
EAST ABUTMENT			SHEET 6 OF 10

BILL OF BARS

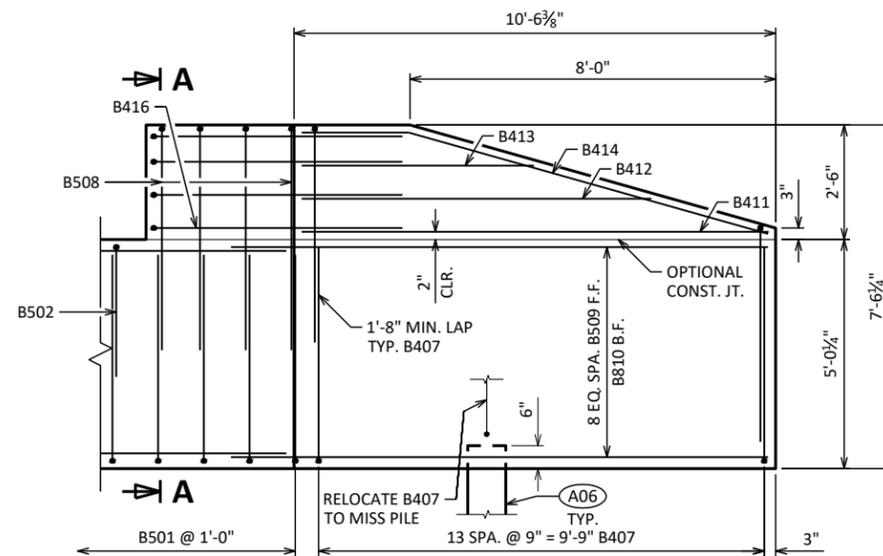
NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BAR MARK	COAT	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
B501		68	6'-0"	X		ABUT BODY STIRRUPS
B502		28	7'-7"	X		ABUT BODY STIRRUPS - TOP U-BAR
B503		9	32'-11"			ABUT BODY HORIZ. - F.F.
B804		18	22'-5"	X		ABUT BODY HORIZ. - B.F.
B405		27	3'-0"	X		ABUT BODY TIE BARS
B506	X	27	2'-0"			ABUT BODY DOWEL BARS
B407	X	56	10'-10"	X		WING STIRRUPS
B508	X	6	11'-7"	X		WING CORNER STIRRUPS
B509	X	18	11'-9"	X		WING LOWER HORIZ. - F.F.
B810	X	18	13'-3"	X		WING LOWER HORIZ. - B.F.
B411	X	4	10'-1"			WING UPPER HORIZ.
B412	X	4	7'-7"			WING UPPER HORIZ.
B413	X	4	5'-0"			WING UPPER HORIZ.
B414	X	4	9'-8"	X		WING TOP HORIZ.
B415	X	4	7'-6"	X		WING 3 UPPER HORIZ. CORNER
B416	X	4	9'-5"	X		WING 4 UPPER HORIZ. CORNER



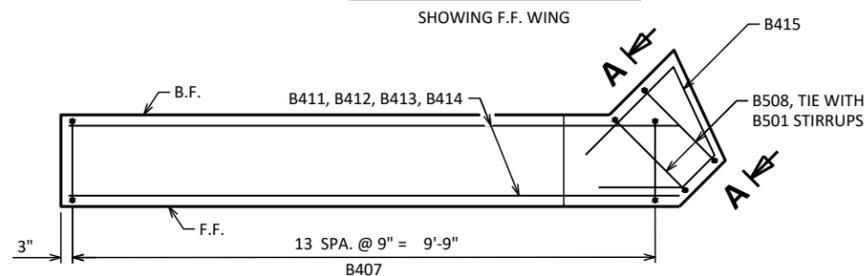
WING 3 ELEVATION

SHOWING F.F. WING



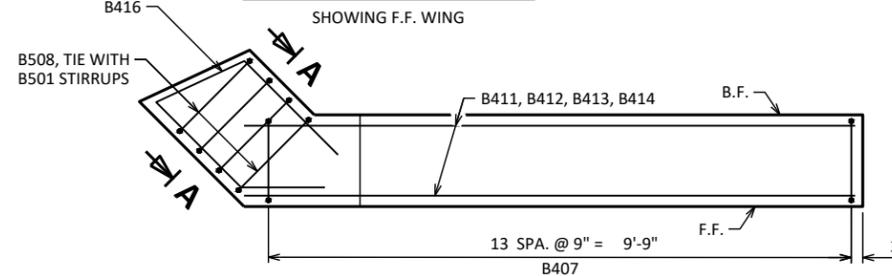
WING 4 ELEVATION

SHOWING F.F. WING



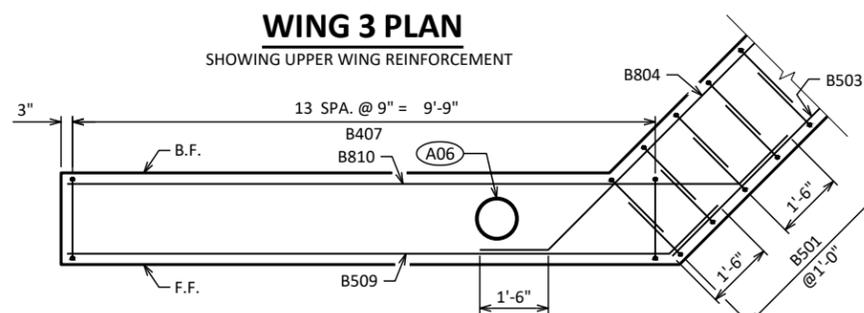
WING 3 PLAN

SHOWING UPPER WING REINFORCEMENT



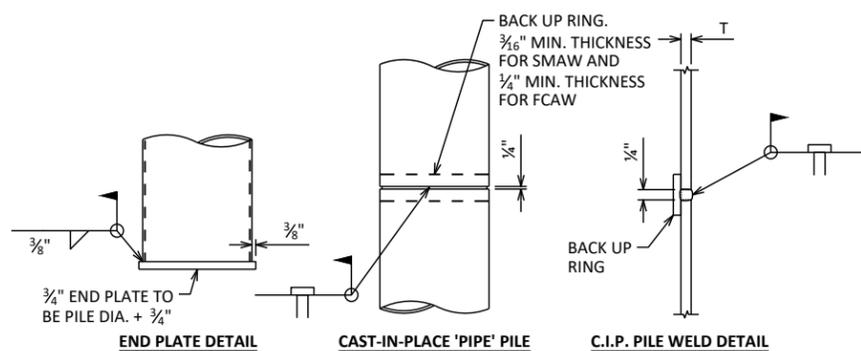
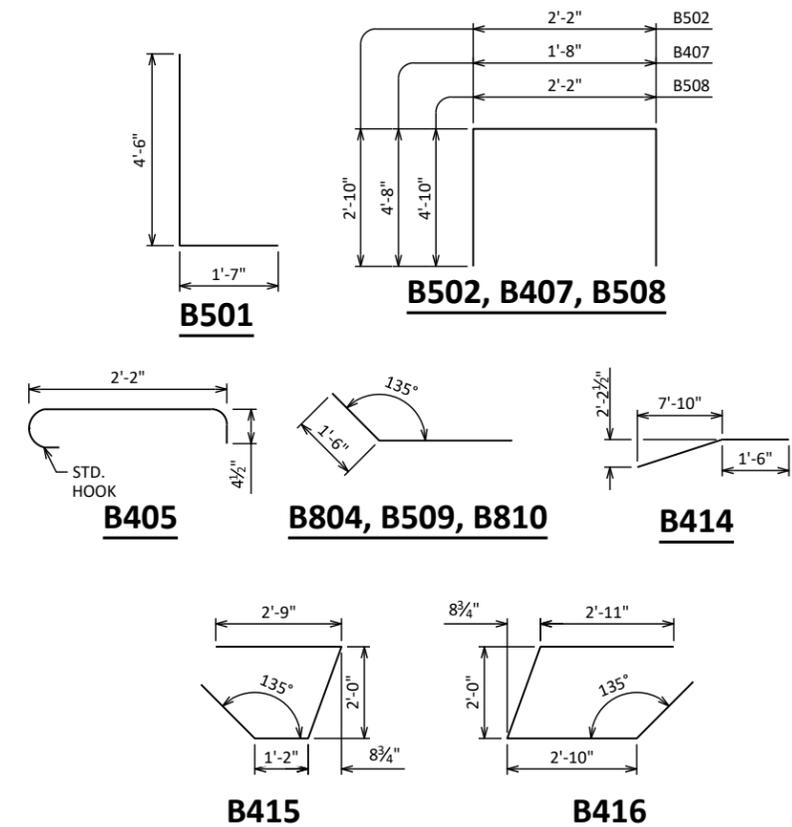
WING 4 PLAN

SHOWING UPPER WING REINFORCEMENT

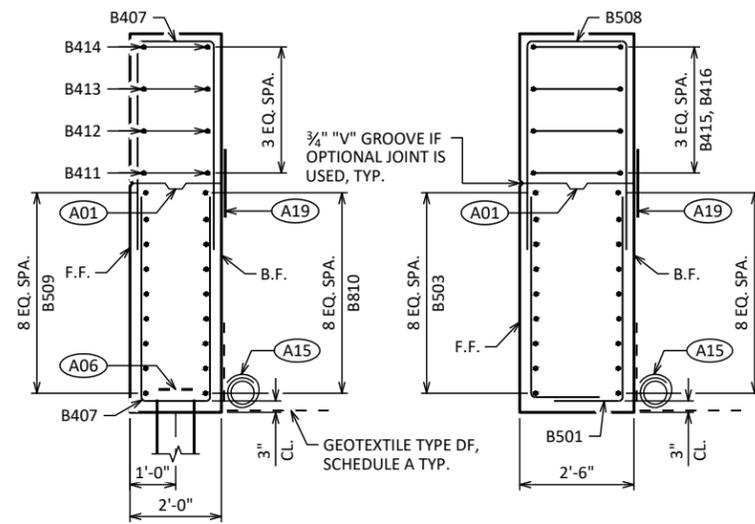


WING 3 PLAN

SHOWING LOWER WING REINFORCEMENT
WING 4 SIMILAR



CIP PILE DETAILS



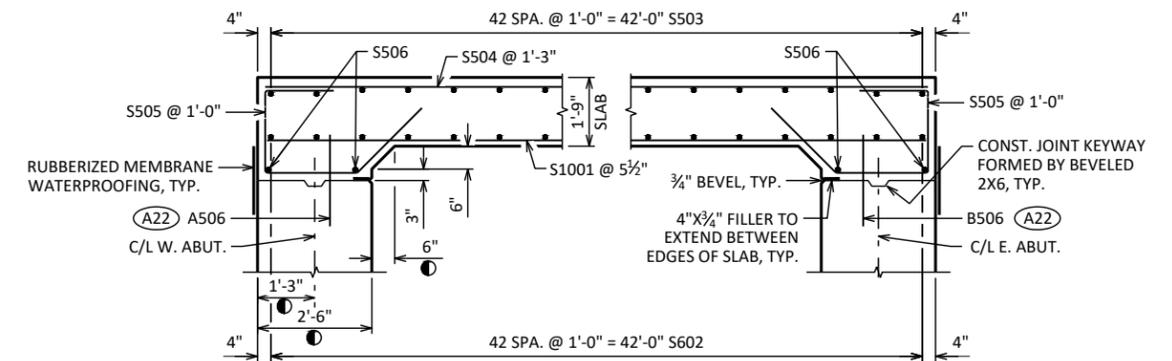
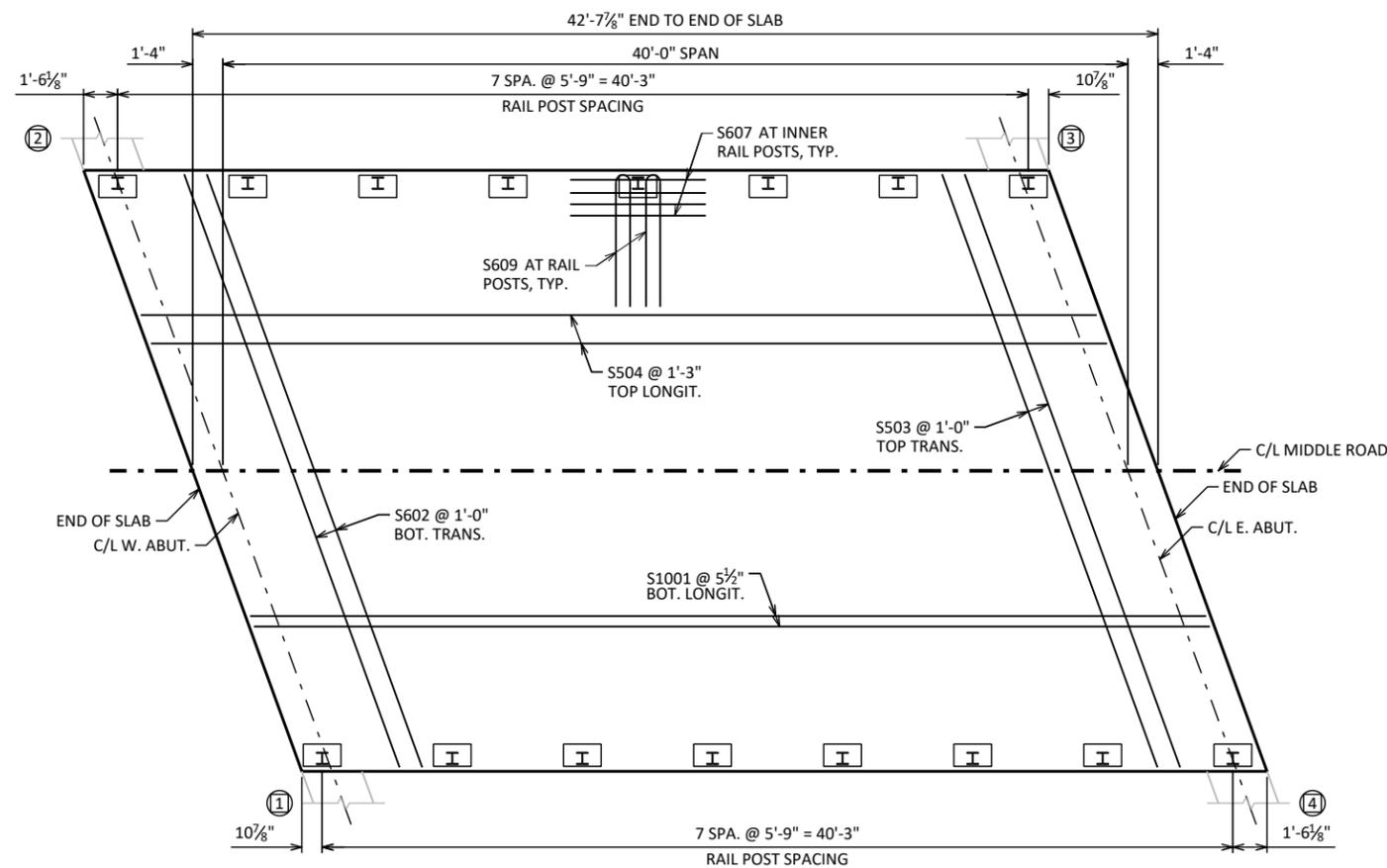
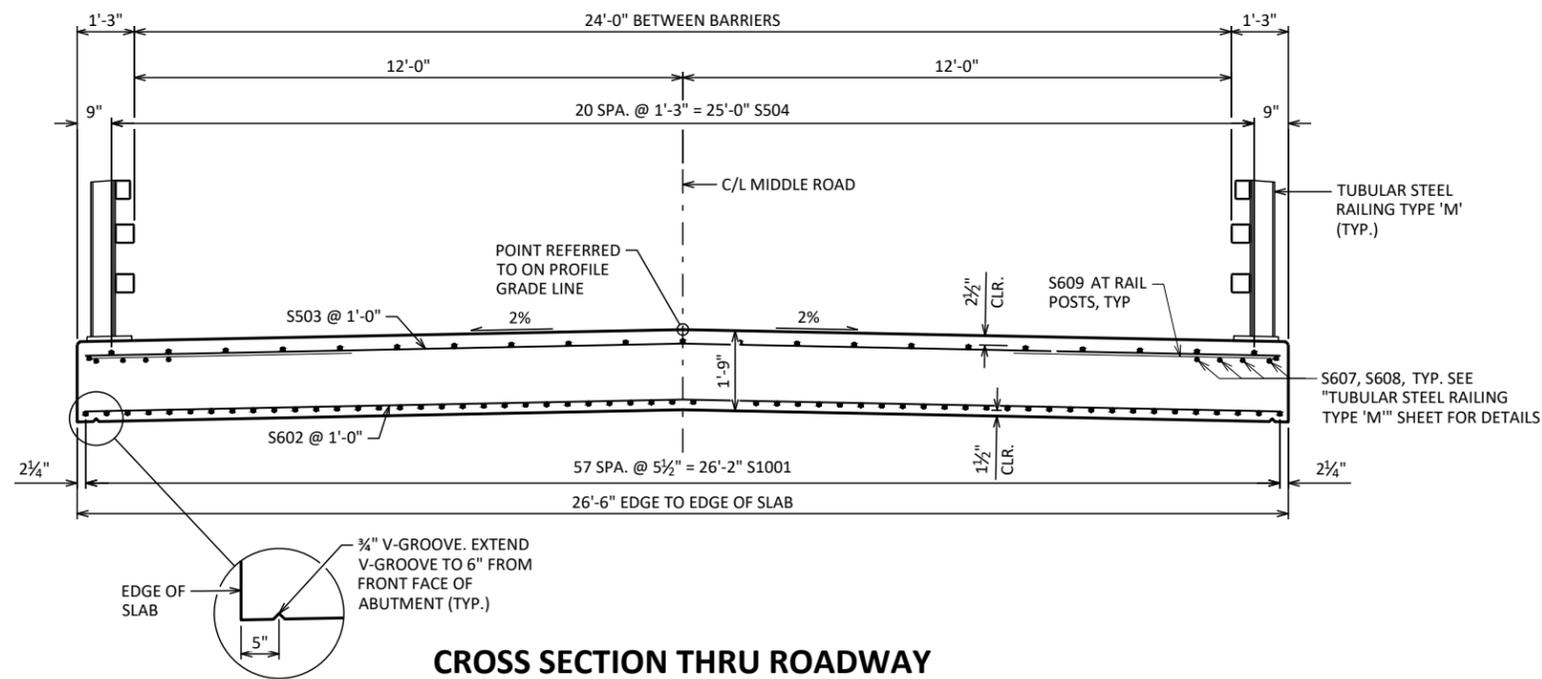
SECTION THRU WING 3

TYPICAL BOTH WINGS

SECTION A-A

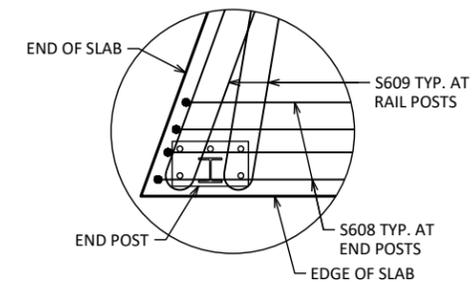
- (A01) OPTIONAL CONST. JOINT: KEYWAY FORMED BY A BEVELED 2X6. PROVIDE 3/4" "V" GROOVE ON F.F. OF WINGWALL IF JOINT IS USED.
- (A06) SUPPORT ABUTMENT ON CIP 10% X 0.25 PILING, ESTIMATED 85 FT LONG WITH A REQUIRED DRIVING RESISTANCE OF 120 TONS PER PILE.
- (A15) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.
- (A19) 18" RUBBERIZED MEMBRANE WATERPROOFING, ONLY IF OPTIONAL CONSTRUCTION JOINT IS USED. COST INCIDENTAL TO BID ITEM "CONCRETE MASONRY STRUCTURES".

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-61-294			
DRAWN BY		ZMF	PLANS CK'D RBH
EAST ABUTMENT DETAILS			SHEET 7 OF 10



LONGITUDINAL SECTION
 DIMENSIONS ARE GIVEN PARALLEL TO ε OF ROADWAY UNLESS OTHERWISE NOTED.

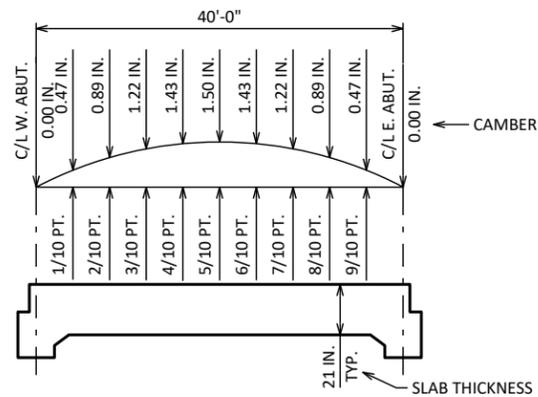
- MEASURED NORMAL TO THE ε OF ABUTMENT. DIMENSIONS ARE TYPICAL FOR BOTH ABUTMENTS.
- (A22) A506, B506 BARS SPACED @ 1'-0" CNTRS. MAY BE PLACED AFTER CONCRETE IS POURED BUT BEFORE INITIAL SET HAS TAKEN PLACE. (EMBED 1'-0" INTO CONC.)



8

8

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-61-294			
DRAWN BY ZMF		PLANS CK'D RBH	
SUPERSTRUCTURE			SHEET 8 OF 10



CAMBER AND SLAB THICKNESS DIAGRAM

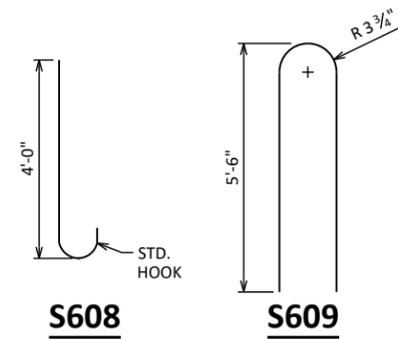
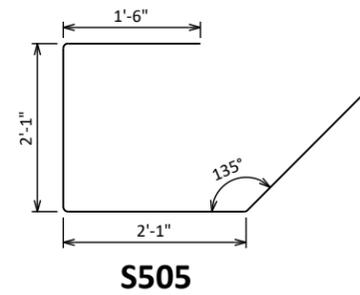
CAMBER SHOWN IS BASED ON 3 TIMES DEAD LOAD DEFLECTIONS. CAMBER SPANS AS SHOWN TO PROVIDE FOR DEAD LOAD DEFLECTION AND FUTURE CREEP. CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT. PARAPETS PLACED ON TOP OF THE SLAB SHALL BE POURED AFTER FALSEWORK HAS BEEN RELEASED, EXCEPT FOR STAGED CONSTRUCTION.

TO DETERMINE FALSEWORK ELEVATION AT EDGE OF SLAB, CROWN OR REFERENCE LINE FOLLOW THIS PROCEDURE:

- LESS TOP OF SLAB ELEVATION AT FINAL GRADE
- PLUS SLAB THICKNESS
- PLUS CAMBER
- PLUS FORM SETTLEMENT/DEFLECTION DUE TO PLACEMENT OF SLAB CONCRETE (TO BE COMPUTED BY THE CONTRACTOR)
- EQUALS TOP OF SLAB FALSEWORK ELEVATION

TOP OF SLAB ELEVATIONS

LOCATION	C/L BRG. W. ABUT.	1/10 PT.	2/10 PT.	3/10 PT.	4/10 PT.	5/10 PT.	6/10 PT.	7/10 PT.	8/10 PT.	9/10 PT.	C/L BRG. E. ABUT.
N. EDGE OF DECK	754.57	754.58	754.59	754.60	754.60	754.61	754.62	754.63	754.64	754.65	754.66
CROWN OR R/L	754.84	754.85	754.86	754.87	754.87	754.88	754.89	754.90	754.91	754.92	754.93
S. EDGE OF DECK	754.59	754.60	754.61	754.62	754.63	754.64	754.64	754.65	754.66	754.68	754.69



BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BAR MARK	COAT	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
S1001	X	58	42'-3"			SLAB BOTTOM LONGITUDINAL
S602	X	43	27'-10"			SLAB BOTTOM TRANSVERSE
S503	X	43	27'-10"			SLAB TOP TRANSVERSE
S504	X	21	42'-3"			SLAB TOP LONGITUDINAL
S505	X	54	7'-5"	X		ABUTMENT DIAPHRAGM STIRRUPS
S506	X	4	27'-10"			ABUTMENT DIAPHRAGM LONGITUDINAL
S607	X	48	6'-0"			SLAB TOP LONGIT. UNDER RAIL POSTS
S608	X	16	4'-8"	X		SLAB TOP LONGIT. UNDER RAIL END POSTS
S609	X	32	11'-3"	X		SLAB TOP HOOKS UNDER RAIL POSTS

SURVEY TOP OF SLAB ELEVATIONS

LOCATION	ABUTMENT	5/10 PT.	ABUTMENT
N. EDGE OF DECK			
C/L			
S. EDGE OF DECK			

PRIOR TO RELEASING SLAB FALSEWORK, TAKE TOP OF SLAB ELEVATIONS AT THE C/L OF ABUTMENTS, THE C/L OF PIERS AND AT 5/10 PTS. TO VERIFY CAMBER. TAKE ELEVATIONS ALONG GUTTER LINES AND CROWN OR R/L. RECORD THE ELEVATIONS IN THE ABOVE TABLE FOR THE "AS BUILT" PLANS.

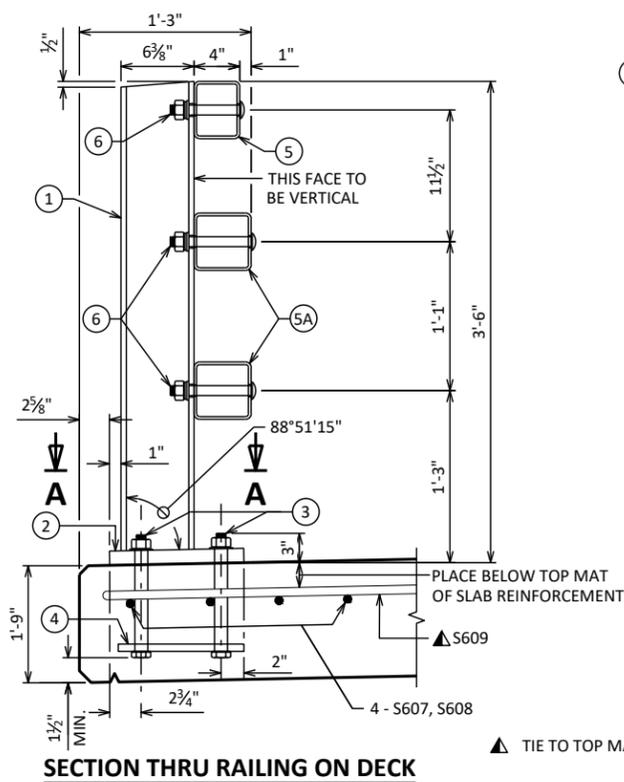
NOTES

FILL IN THE TABLE OF "SURVEY TOP OF SLAB ELEVATIONS" FOR EACH SPAN ON AS BUILT PLANS.

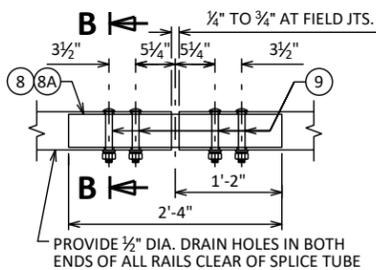
TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 3'-0" CENTERS EACH WAY. BOTTOM LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 4'-0" CENTERS.

ALL SLAB THICKNESS DIMENSIONS ARE MINIMUM. ANY TOLERANCES NECESSARY TO CORRECT CONSTRUCTION DISCREPANCIES ARE TO BE PLUS (+).

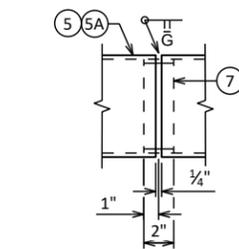
NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-61-294			
DRAWN BY ZMF		PLANS CK'D RBH	
SUPERSTRUCTURE DETAILS			SHEET 9 OF 10



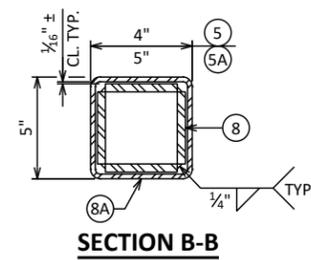
SECTION THRU RAILING ON DECK



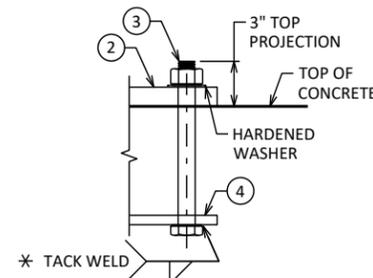
FIELD ERECTION JOINT DETAIL



SHOP RAIL SPLICE DETAIL
LOCATION MUST BE SHOWN ON SHOP DRAWINGS

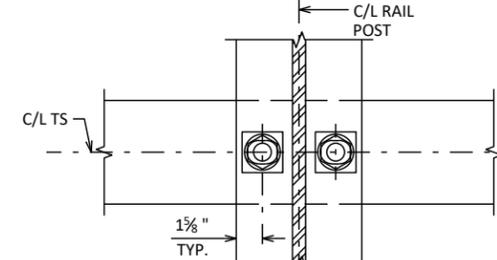


SECTION B-B

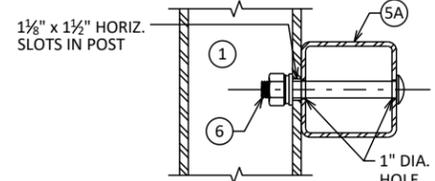


ANCHOR BOLTS

* ANCHOR BOLT ASSEMBLY MAY BE TACK WELDED, EITHER IN THE SHOP, OR IN THE FIELD AFTER THE ANCHOR PLATE IS PLACED.



SECTION THRU POST WEB



SECTION THRU RAIL

NOTE: CONNECTIONS AT LOWER RAILS SHOWN. CONNECTIONS AT TOP RAIL SIMILAR.

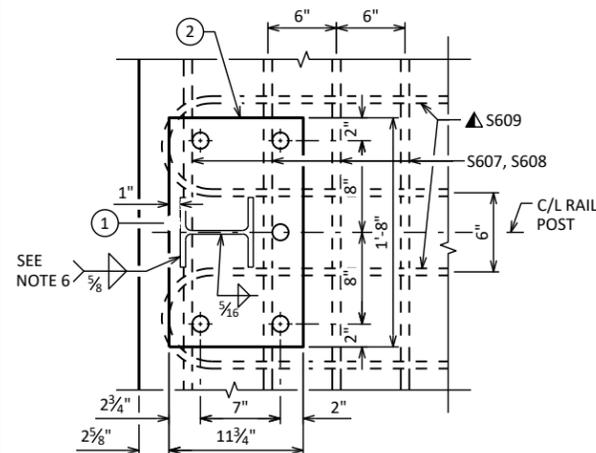
TYPICAL RAIL TO POST CONNECTIONS

LEGEND

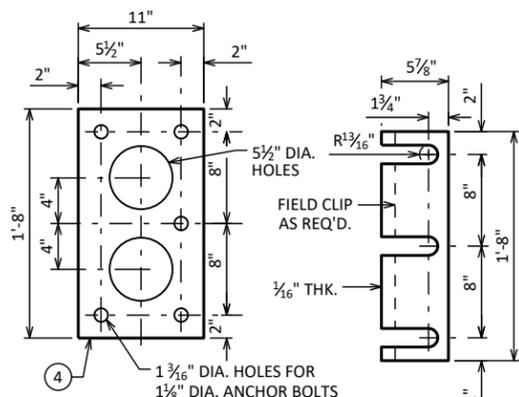
- ① W6 x 25 WITH 1 1/2" x 1 1/2" HORIZ. SLOTS ON EACH SIDE OF POST FOR BOLT NO. 6. CUT BOTTOM OF POST TO MATCH CROSS SLOPE OF ROADWAY. PLACE POST VERTICAL. PLACE POSTS NORMAL TO GRADE LINE.
- ② PLATE 1 1/4" x 11 3/4" x 1'-8" WITH 1 7/16" OVERSIZED HOLES FOR ANCHOR BOLTS NO. 3. WELD TO NO. 1 AS SHOWN.
- ③ ASTM A449 - 1 1/8" DIA. ANCHOR BOLTS WITH NUT AND HARDENED WASHER (ALL GALVANIZED). 5 REQ'D. PER POST. THREAD 3" AND PLACE NORMAL TO PLATE NO. 2. CHAMFER TOP OF BOLTS BEFORE THREADING. USE 1'-9" LONG IN ABUTMENT WINGS. AT POSTS ON CONCRETE SLAB SUPERSTRUCTURES WHERE THE SLAB THICKNESS IS > 16" USE 1'-3" LONG. USE 10 3/4" LONG AT ALL OTHER LOCATIONS. (AN EQUIVALENT THREADED ROD WITH NUTS AND HARDENED WASHERS MAY BE SUBSTITUTED FOR ANCHOR BOLTS IN WINGS IF REQ'D. FOR CONSTRUCTIBILITY.)
- ④ 5/8" x 11" x 1'-8" ANCHOR PLATE (GALVANIZED) WITH 1 3/16" DIA. HOLES FOR ANCHOR BOLTS NO. 3.
- ⑤ TS 5 x 4 x 0.25 STRUCTURAL TUBING. ATTACH TO NO. 1 WITH NO. 6.
- ⑤A TS 5 x 5 x 0.25 STRUCTURAL TUBING. ATTACH TO NO. 1 WITH NO. 6.
- ⑥ 7/8" DIA. A325 SLOTTED ROUND HEAD BOLT WITH NUT, 3/16" x 1 5/8" x 1 5/8" MIN. WASHER, AND LOCK WASHER (2 REQ'D. AT EACH RAIL TO POST LOCATION).
- ⑦ SPLICE SLEEVE FABRICATED FROM 3/4" PLATE. PROVIDE "SLIDING FIT".
- ⑧ 3/8" x 3 5/8" x 2'-4" PLATE. 2 PER RAIL. USED IN NO. 5 & 5A.
- ⑧A 3/8" x 2 5/8" x 2'-4" PLATE USED IN NO. 5, 3/8" x 3 5/8" x 2'-4" PLATE USED IN NO. 5A. 2 PER RAIL.
- ⑨ 7/8" DIA. A325 ROUND HEAD BOLT WITH NUT, WASHER, AND LOCK WASHER. USE 1 5/16" x 1 1/4" LONGIT. SLOTTED HOLES IN PLATE NO. 10A AT FIELD JOINTS AND 1 5/16" x 2 3/4" MIN. LONGIT. SLOTTED HOLES AT EXP. JOINTS IN PLATE NO. 8A. PROVIDE 1 5/16" DIA. ROUND HOLES IN TUBES NO. 5 AND NO. 5A.

GENERAL NOTES

- 1. BID ITEM SHALL BE "RAILING TUBULAR TYPE M" WHICH INCLUDES ALL ITEMS SHOWN.
- 2. RAIL POST AND BASE PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A709 GRADE 50. HOLLOW RAILING STRUCTURAL TUBING SHALL CONFORM TO THE REQUIREMENTS OF ASTM A500 GRADE B OR C WITH A CERTIFIED FY = 50 KSI. ANCHOR PLATES, AND SPLICE TUBE PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A709 GRADE 36.
- 3. THE NUT SECURING THE POST BASE PLATE TO THE CONCRETE SHALL BE TIGHTENED TO A SNUG FIT AND GIVEN AN ADDITIONAL 1/2 TURN.
- 4. RAILS SHALL BE CONTINUOUS OVER A MINIMUM OF THREE (3) POSTS WITHOUT SPLICES WHERE POSSIBLE.
- 5. ENDS OF TUBE SECTIONS SHALL BE SAWED. GRIND SMOOTH EXPOSED EDGES. ALL CUT ENDS SHALL BE TRUE AND SMOOTH.
- 6. WELD IS THE SAME ON BOTH FLANGES. FLANGE WELD DOES NOT REQUIRE MAGNETIC PARTICLE TESTING.
- 7. FILL BOLT SLOT OPENINGS IN POST SHIMS AND PLATE NO. 2 AND CAULK AROUND PERIMETER OF PLATE NO. 2 WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. STEEL POST SHIMS MAY BE USED UNDER POSTS WHERE REQ'D. FOR ALIGNMENT.
- 8. POST BASE PLATES SHALL BE FLAT WITH ALL SURFACES SMOOTH AND FREE FROM WARP AND ALL EDGES SMOOTH, STRAIGHT AND VERTICAL. ALL PLATE CUTS SHALL BE MACHINE OR MACHINE FLAME CUT.
- 9. ALL MATERIAL SHALL BE GALVANIZED AFTER FABRICATION. PRIOR TO GALVANIZING, ALL STEEL RAILING POSTS & STEEL TUBING SHALL BE GIVEN A NO. 6 BLAST CLEANING BY SSPC SPECIFICATIONS.

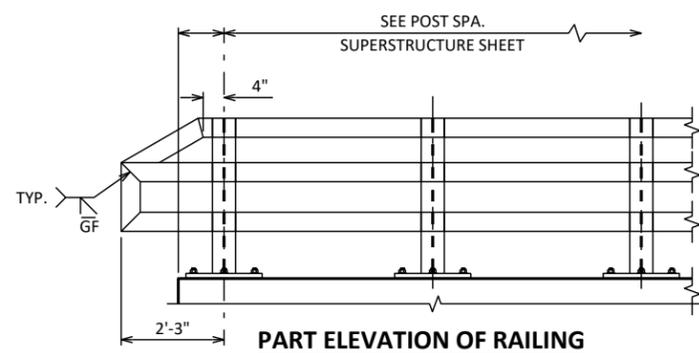


SECTION A-A



ANCHOR PLATE
AT RAIL TO DECK CONNECTION

POST SHIM
DETAIL



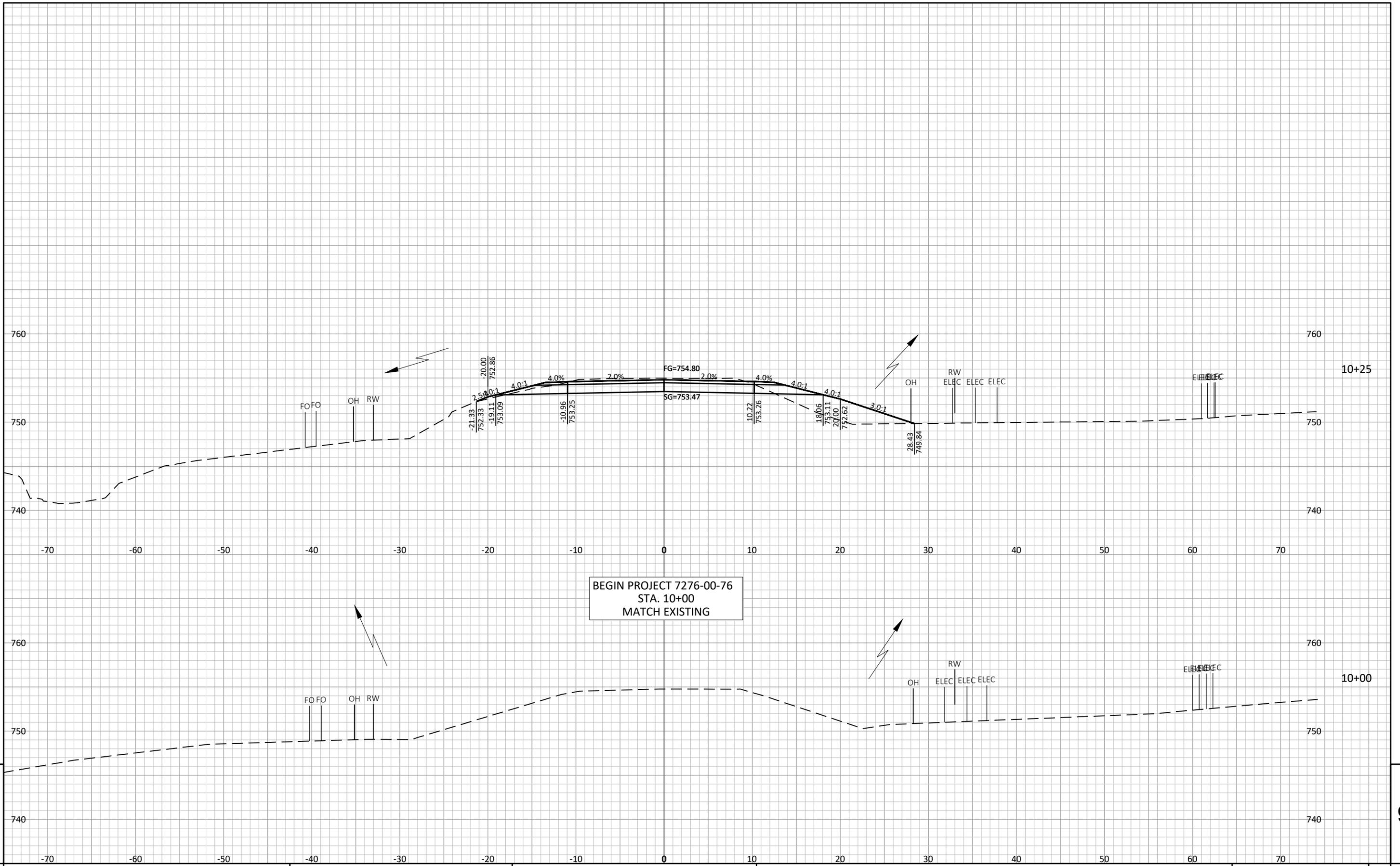
PART ELEVATION OF RAILING

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-61-294			
DRAWN BY		ZMF	PLANS CK'D RBH
TUBULAR STEEL RAILING TYPE 'M'			SHEET 10 OF 10

EARTHWORK - MIDDLE RD

STATION	DISTANCE (LF)	AREA (SF)		INCREMENTAL VOLUME (CY)			CUMULATIVE VOLUME (CY)			
		CUT	FILL	CUT NOTE 1	FILL NOTE 2	FILL (1.25) NOTE 3	CUT (1.00) NOTE 1	FILL NOTE 2	FILL (1.25) NOTE 3	MASS ORDINATE NOTE 4
10+00	0	33	15	0	0	0	0	0	0	0
10+25	25	38	23	33	18	23	33	18	23	11
10+49	24	21	5	26	12	15	59	30	38	22
10+91	0	22	10	0	0	0	59	30	38	22
11+00	9	36	15	10	4	5	69	34	43	27
11+25	25	34	41	32	26	33	101	60	75	26
11+40	15	33	19	19	17	21	120	77	96	24
COLUMN SUBTOTALS =				120	77	96	120	77	96	24

NOTES:	
1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL
2 - FILL (1.25)	(UNEXPANDED FILL)*1.25
3 - MASS ORDINATE	CUT - FILL (1.25)



PROJECT NO: 7276-00-76

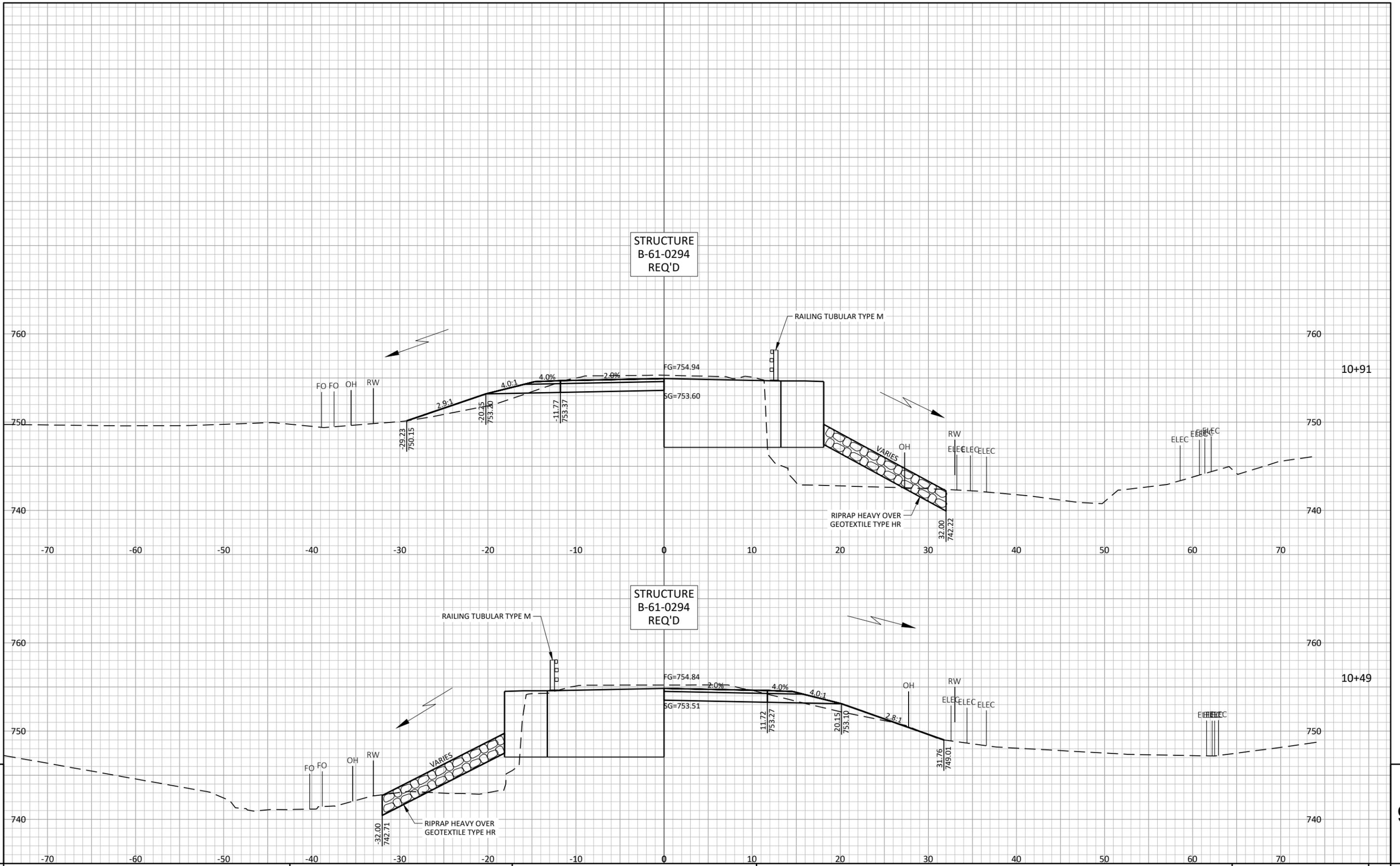
HWY: MIDDLE ROAD

COUNTY: TREMPLEAU

CROSS SECTIONS: MAINLINE

SHEET

E



PROJECT NO: 7276-00-76

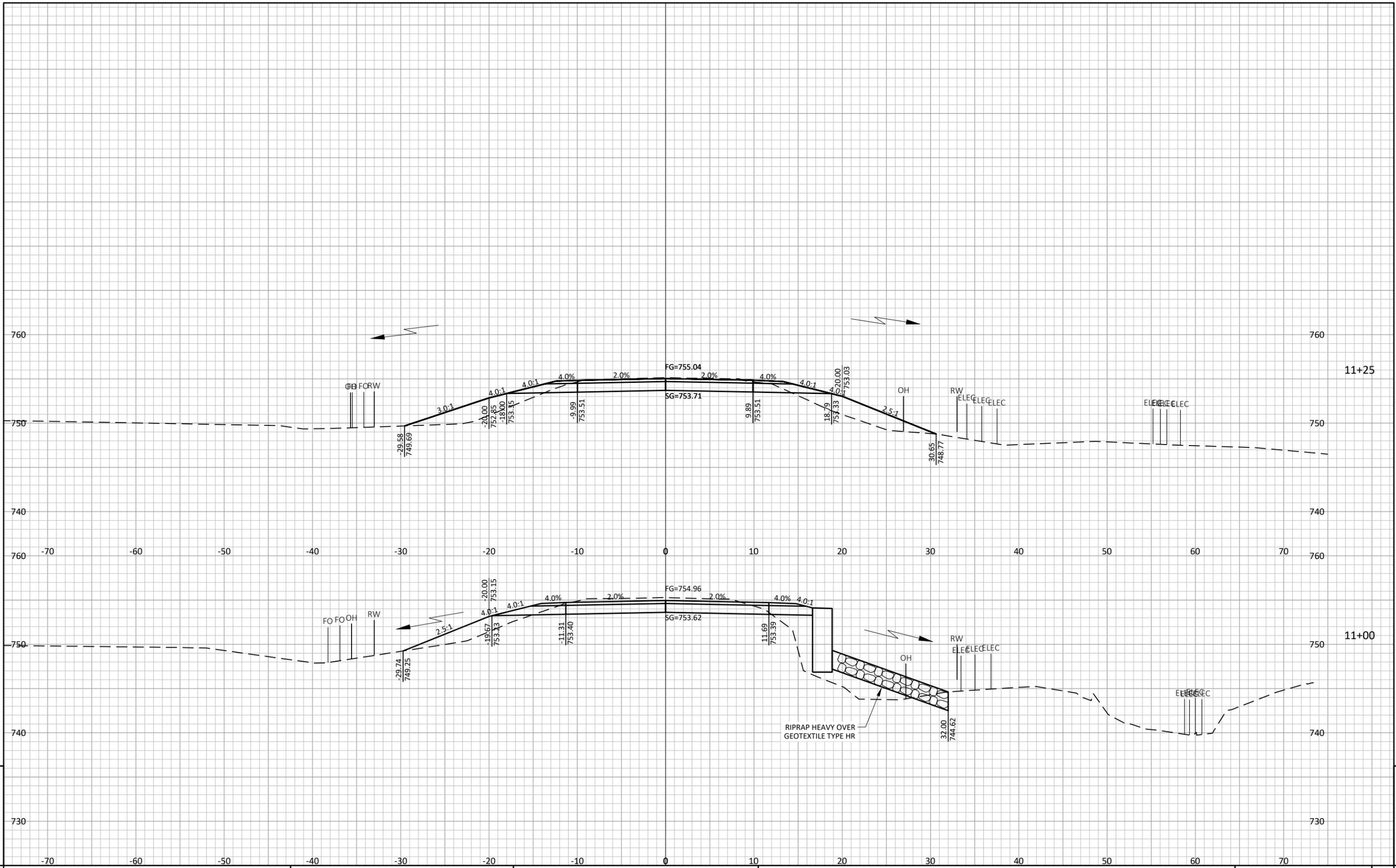
HWY: MIDDLE ROAD

COUNTY: TREMPLEAU

CROSS SECTIONS: MAINLINE

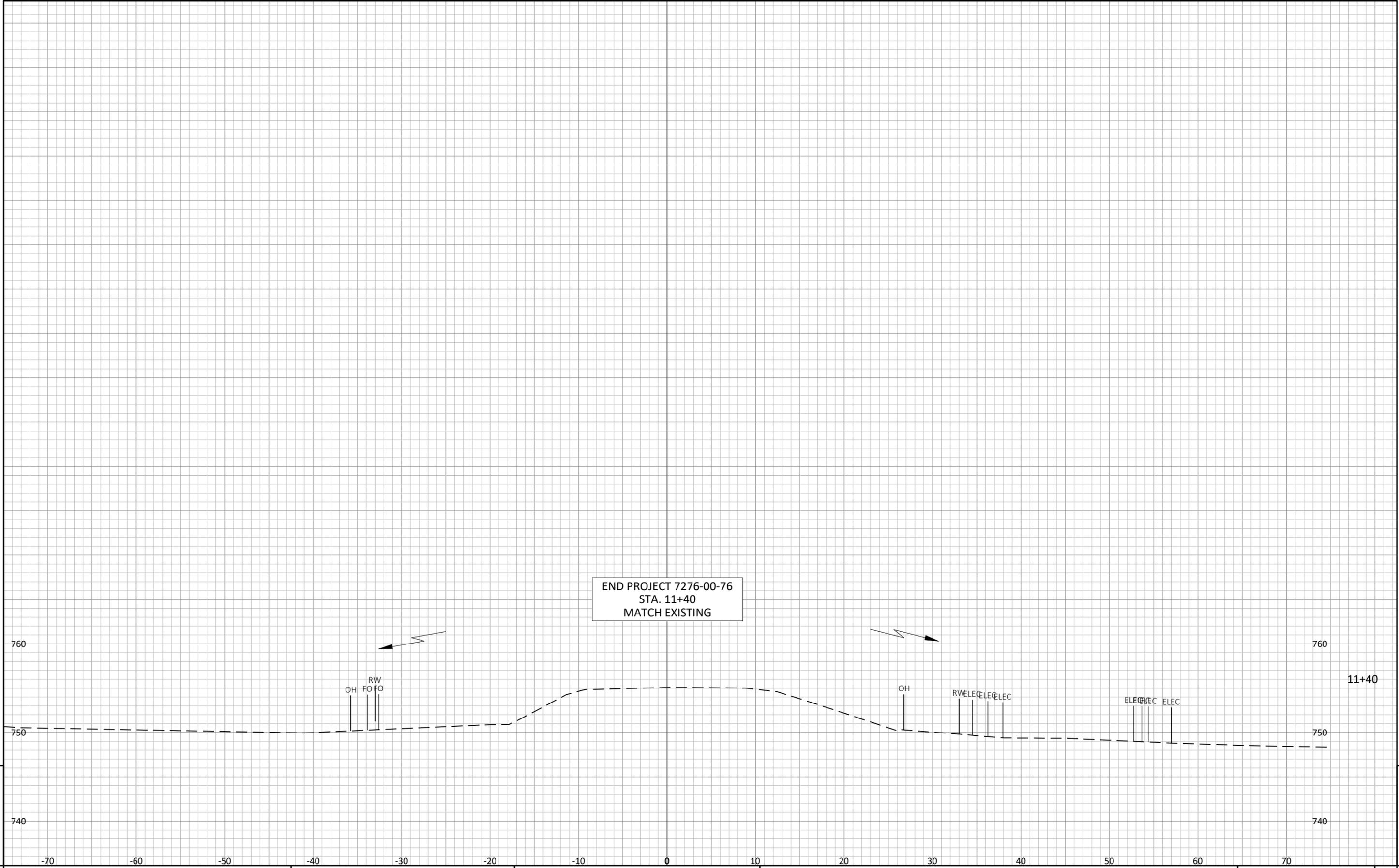
SHEET

E

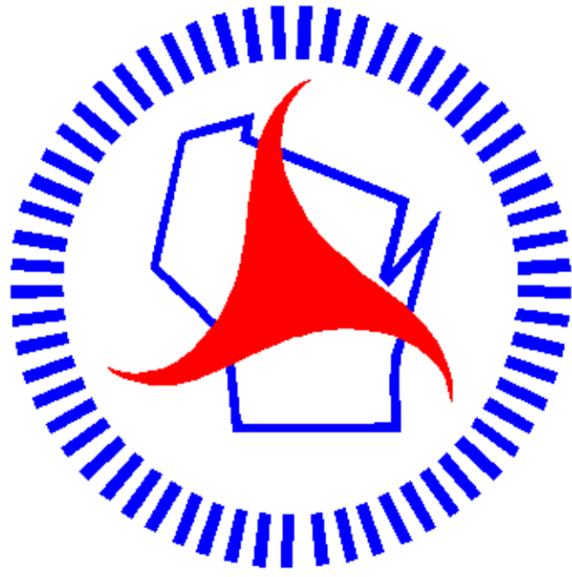


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Notes



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