

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 118



DESIGN DESIGNATION 5770-02-01

A.A.D.T.	2028	=	2500
A.A.D.T.	2048	=	2800
D.H.V.		=	265
D.D.		=	60/40
T.		=	12.6%
DESIGN SPEED		=	30-35 MPH
ESALS		=	630,000

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
MARSH AREA	STORM SEWER
WOODED OR SHRUB AREA	TELEPHONE
	WATER
	UTILITY PEDESTAL
	POWER POLE
	TELEPHONE POLE

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

LONE ROCK - STH 154

WISCONSIN RIVER TO USH 14

STH 130

RICHLAND & SAUK COUNTIES

STATE PROJECT NUMBER
5770-02-71

END PROJECT
STA 116+00

BEGIN PROJECT
STA 45+34.14
Y = 386,726.512
X = 727,508.586



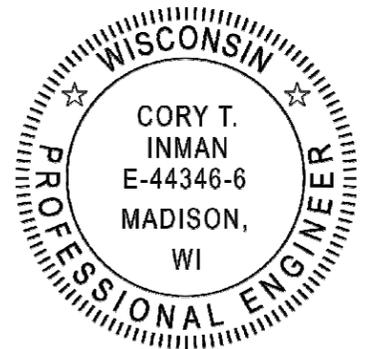
TOTAL NET LENGTH OF CENTERLINE = 1.338 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), RICHLAND COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 18.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5770-02-71		



1600 Aspen Commons, Suite 230
Middleton, WI 53562
608.827.8810 Fax 608.833.3198



10/01/2025 (Date) *Cory T. Inman* (Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY	EMCS, INC.
Surveyor	EMCS, INC.
Designer	NATHANIEL SCHUMAKER
Project Manager	SW REGION
Regional Examiner	DANIEL KLEINERTZ
Regional Supervisor	

APPROVED FOR THE DEPARTMENT
DATE: Nathaniel Schumaker (Signature)

E

PROJECT ID: 5770-02-71

COUNTY: RICHLAND & SAUK

ORDER OF SECTION 2 SHEETS

- GENERAL NOTES
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- CURB RAMP DETAILS
- TRAFFIC CONTROL
- TEMPORARY PEDESTRIAN ACCOMODATIONS
- DETOUR PLAN

GENERAL NOTES

- THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.
- CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY OPERATIONS OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.
- TOPSOIL SHALL BE PLACED 1 INCH BELOW THE TOP OF ADJACENT CONCRETE CURBS OR SIDEWALKS.
- THE QUANTITY OF THE ITEMS FOR EROSION PROTECTION INCLUDES AN UNDISTRIBUTED AMOUNT FOR PROTECTION, CONTROL AND ABATEMENT OF WATER POLLUTION RESULTING FROM SOIL EROSION. THE DISTRIBUTION AND LOCATION OF THESE MATERIALS ARE TO BE DETERMINED BY THE ENGINEER.
- DISTURBED AREAS WITHIN THE RIGHT OF WAY ARE TO BE TOPSOILED (SALVAGED), FERTILIZED, SEEDED, AND MULCHED OR SODDED AS DIRECTED BY THE ENGINEER.
- (SALVAGED) TOPSOIL, MULCH, SEEDING AND FERTILIZER HAS BEEN COMPUTED BY DIRECT MEASUREMENTS ON THE PLANS TO THE SLOPE INTERCEPT.
- DO NOT REMOVE ANY TREES WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- FERTILIZER SHALL NOT BE USED WITHIN 20' OF NAVIGABLE WATERWAYS OR WETLANDS.
- APPLY TACK COAT AT A RATE OF 0.07 GAL/SY TO MILLED PAVEMENT SURFACES AND 0.05 GAL/SY BETWEEN LAYERS OF HMA PAVEMENT.
- THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, PASSING OR PARKING LANE.
- A CONVERSION FACTOR OF 112 LB/SY/IN. IS USED TO ESTIMATE QUANTITIES FOR HMA PAVEMENT.
- ASPHALTIC PAVEMENT SHALL BE CONSTRUCTED WITH THE FOLLOWING LAYERS AND GRADATIONS:

TOTAL LAYER PAVEMENT THICKNESS	LAYERS	HMA PAVEMENT ITEM
3.5"	1.75" (UPPER) 1.75" (LOWER)	4 LT 58-28 S (UPPER) 4 LT 58-28 S (LOWER)



UTILITIES

ELECTRICITY/GAS/PETROLEUM

ALLIANT ENERGY - ELECTRIC AND GAS
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PHONE: (608) 356-0607
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SEWER

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FITCHBURG, WI 53711
PHONE: (608)235-2955
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WISDOT DESIGN

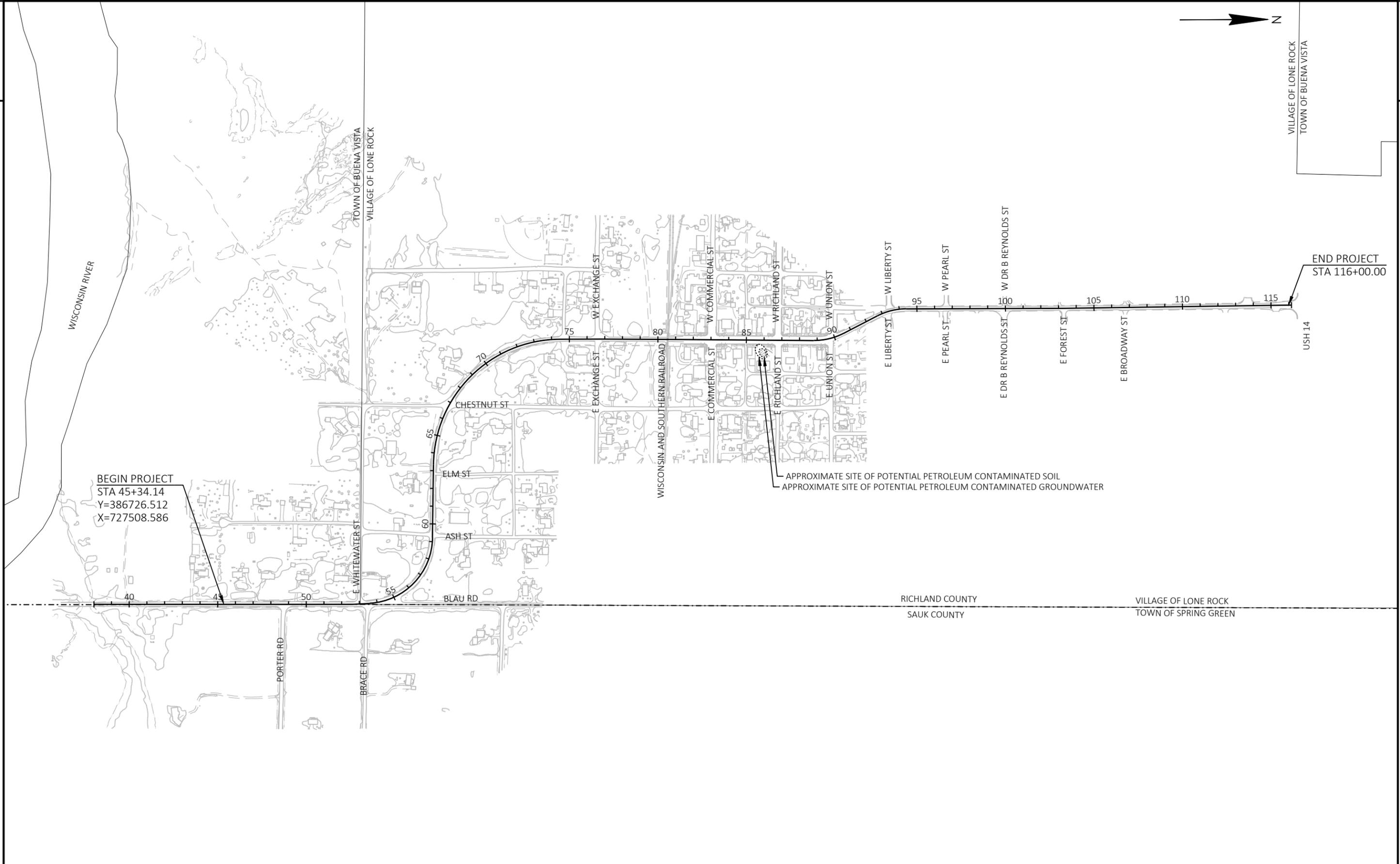
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PHONE: (608)385-1334
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CONSULTANT DESIGN

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EMAIL: CINMAN@EMCSINC.COM

STANDARD ABBREVIATIONS

AGG	AGGREGATE	MGAL	1000 GALLONS
<	ANGLE	N	NORTH
AE, AEW	APRON ENDWALL	NB	NORTHBOUND
ASPH.	ASPHALTIC	NOR.	NORMAL
A.D.T.	AVERAGE DAILY TRAFFIC	PAV'T	PAVEMENT
A.A.D.T.	ANNUAL AVERAGE DAILY TRAFFIC	P.L.E.	PERMANENT LIMITED EASEMENT
C/L	CENTER LINE	P.C.	POINT OF CURVATURE
Δ	CENTRAL ANGLE OR DELTA	P.I.	POINT OF INTERSECTION
CMCP	CORRUGATED METAL CULVERT PIPE	P.T.	POINT OF TANGENCY
CMP	CORRUGATED METAL PIPE	P.E.	PRIVATE ENTRANCE
CO.	COUNTY	R	RADIUS OR RANGE
CTH	COUNTY TRUNK HIGHWAY	R/L	REFERENCE LINE
CR.	CREEK	R.C.C.P.	REINFORCED CONCRETE CULVERT PIPE
CY	CUBIC YARD	REQ'D	REQUIRED
C&G	CURB AND GUTTER	RT	RIGHT
D	DEGREE OF CURVE	R/W	RIGHT OF WAY
D.H.V.	DESIGN HOURLY VOLUME	RD.	ROAD
D.D.	DIRECTIONAL DISTRIBUTION	SHLD.	SHOULDER(S)
EA	EACH	S	SOUTH
E	EAST	SB	SOUTHBOUND
EB	EASTBOUND	S.F.	SQUARE FOOT (FEET)
ELEC.	ELECTRIC(AL), ELEC. CABLE	SDD	STANDARD DETAIL DRAWING(S)
EL., ELEV.	ELEVATION	STH	STATE TRUNK HIGHWAY
ESALS	EQUIVALENT SINGLE AXLE LOADS	STA.	STATION
EXC.	EXCAVATION	S.E.	SUPERELEVATION
EXIST	EXISTING	S.I.	SLOPE INTERCEPT
F.E.	FIELD ENTRANCE	T.	PERCENT TRUCKS
CWT	HUNDRED WEIGHT	TEL.	TELEPHONE
IH	INTERSTATE HIGHWAY	T.L.E.	TEMPORARY LIMITED EASEMENT
LT	LEFT	TYP	TYPICAL
L.	LENGTH OF CURVE	VAR	VARIABLE
L.F.	LINEAR FOOT(FEET)	W	WEST
LC.	LONG CHORD	WB	WESTBOUND
LS	LUMP SUM		



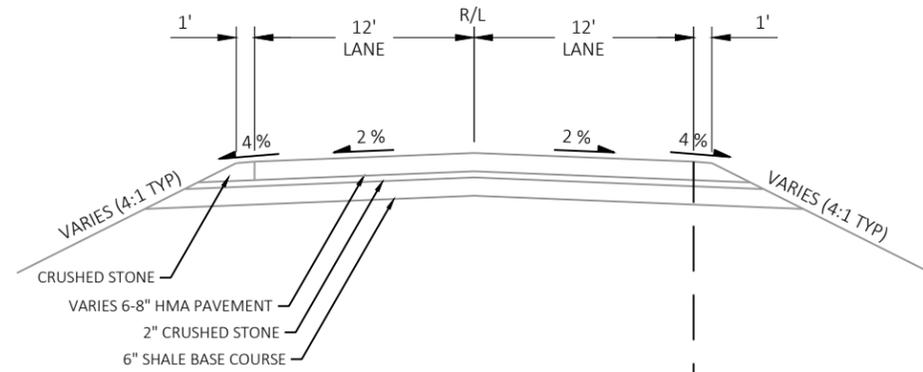
BEGIN PROJECT
 STA 45+34.14
 Y=386726.512
 X=727508.586

APPROXIMATE SITE OF POTENTIAL PETROLEUM CONTAMINATED SOIL
 APPROXIMATE SITE OF POTENTIAL PETROLEUM CONTAMINATED GROUNDWATER

END PROJECT
 STA 116+00.00

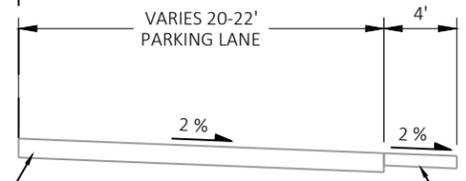
PROJECT NO: 5770-02-71	HWY: STH 130	COUNTY: RICHLAND	PROJECT OVERVIEW	SHEET	E
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CORE #	APPROX STATION	OFFSET	ASPHALTIC PAVEMENT THICKNESS (IN)
1	39+70	9' LT	8
2	51+85	12.5' RT	7.5
3	63+49	8' RT	8
4	76+95	9.5' LT	6
5	89+33	11' RT	6
6	101+65	11.5' LT	6
7	114+42	9.5' LT	6



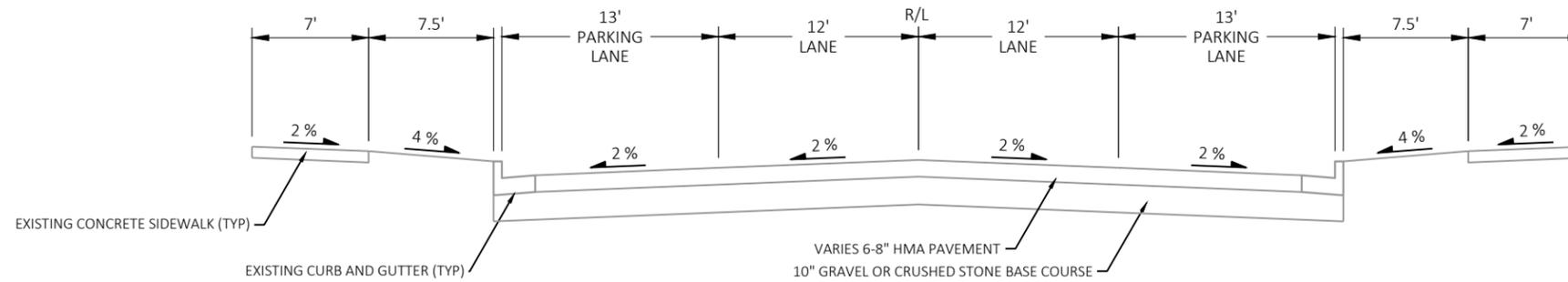
EXISTING TYPICAL SECTION

STA 45+34 - 81+47
STA 89+63 - 116+00



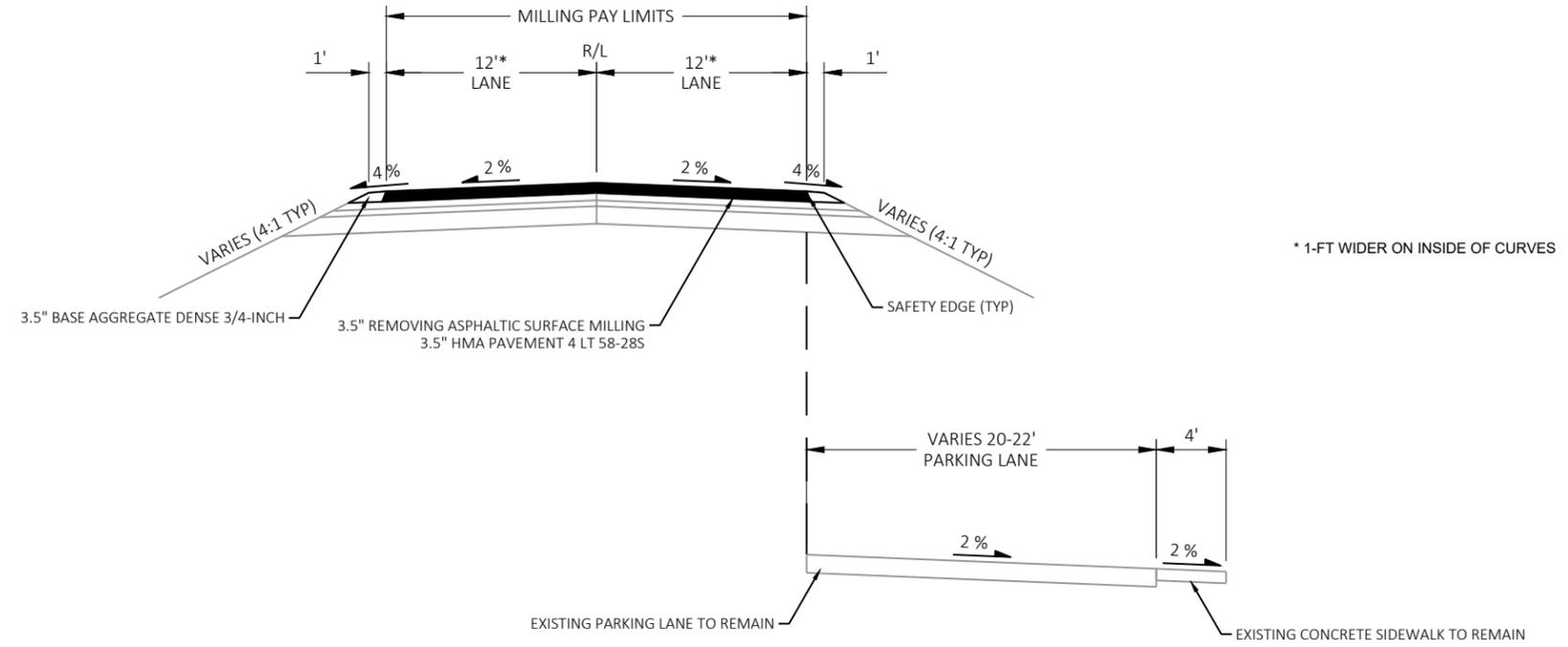
EXISTING TYPICAL SECTION

STA 75+00 - 76+45



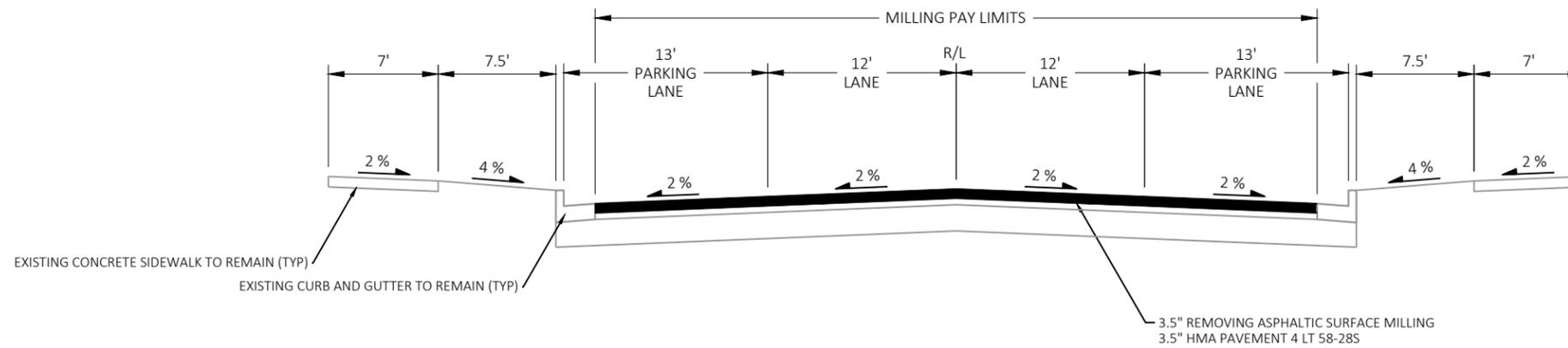
EXISTING TYPICAL SECTION

STA 81+47 - 89+63

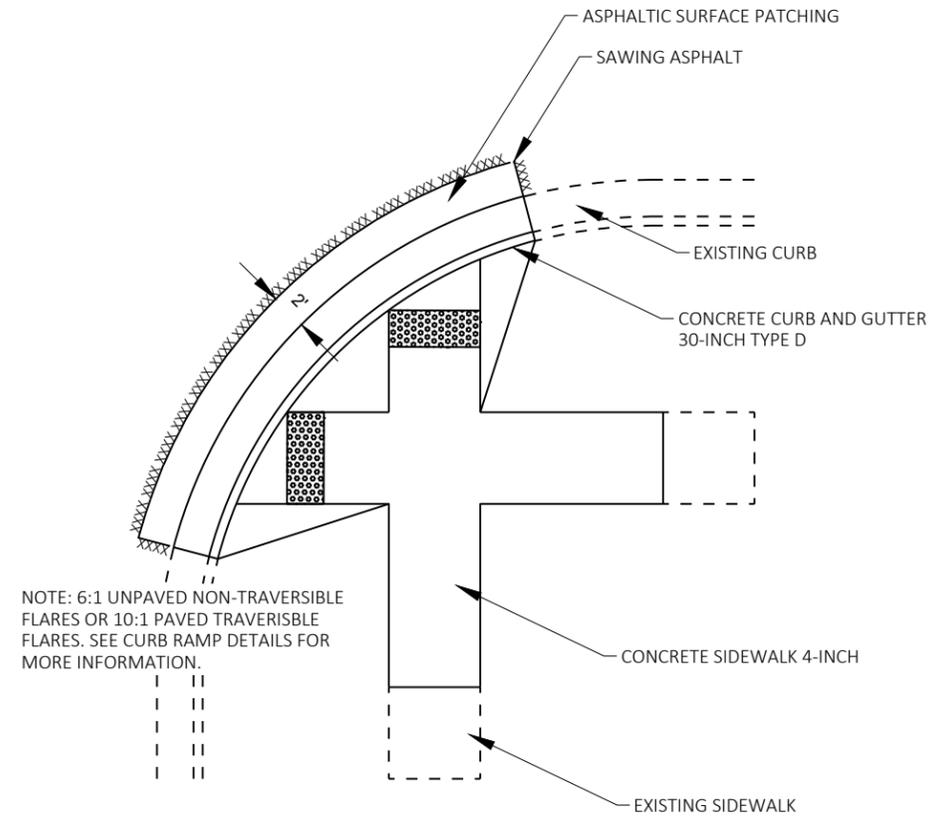


FINISHED TYPICAL SECTION
 STA 45+34 - 81+47
 STA 89+63 - 116+00

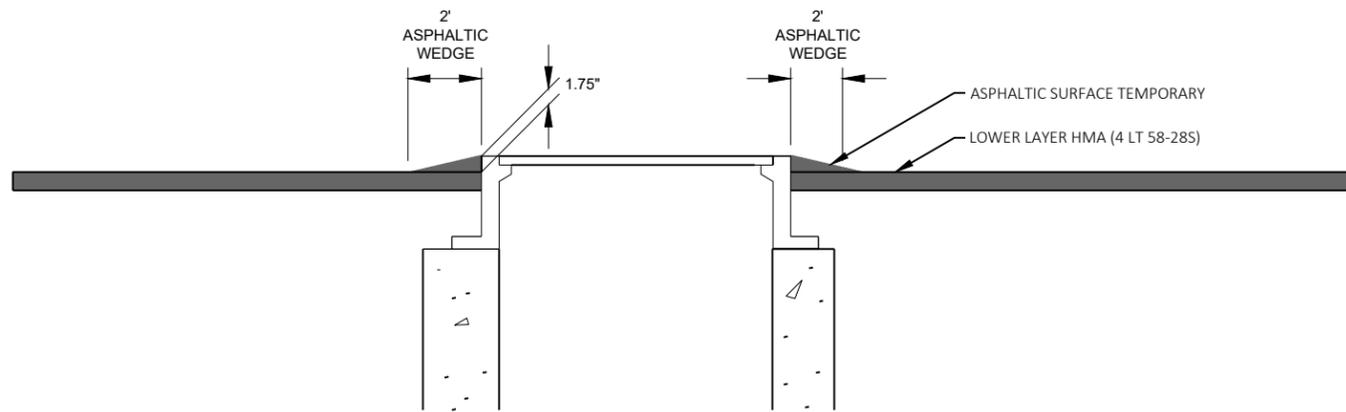
EXISTING TYPICAL SECTION
 STA 75+00 - 76+45



FINISHED TYPICAL SECTION
 STA 81+47 - 89+63



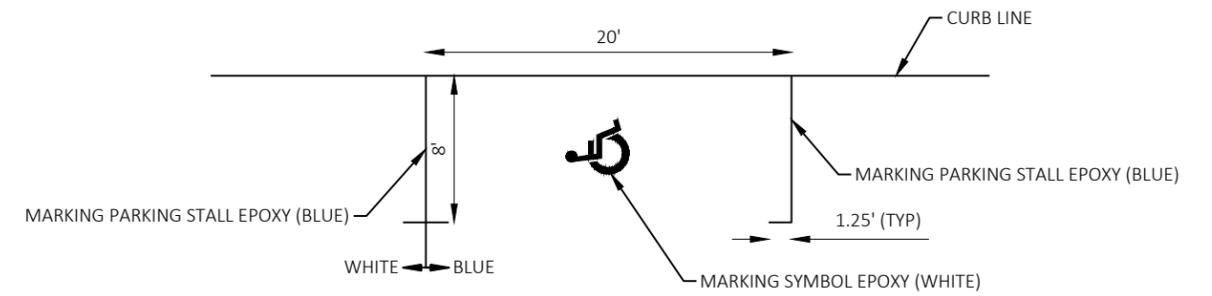
ASPHALT REMOVAL AT CURB RAMPS



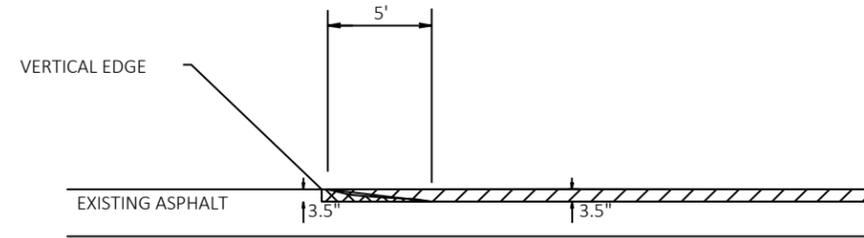
TEMPORARY WEDGES DETAIL (MANHOLES)

NOTES

- PROVIDE ASPHALTIC WEDGES PRIOR TO OPENING LANE TO TRAFFIC
- ADD BARRIER BETWEEN LOWER LIFT PATCH AND WEDGE SO PATCH IS NOT DISTURBED WHEN WEDGE IS PULLED
- REMOVAL OF ASPHALTIC WEDGES WILL BE PAID FOR AS "REMOVING ASPHALTIC SURFACE BUTT JOINTS"



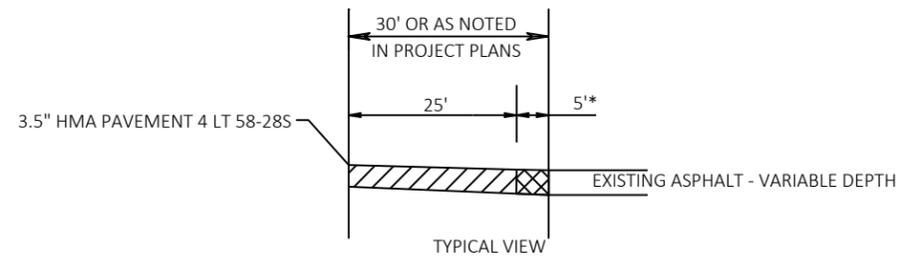
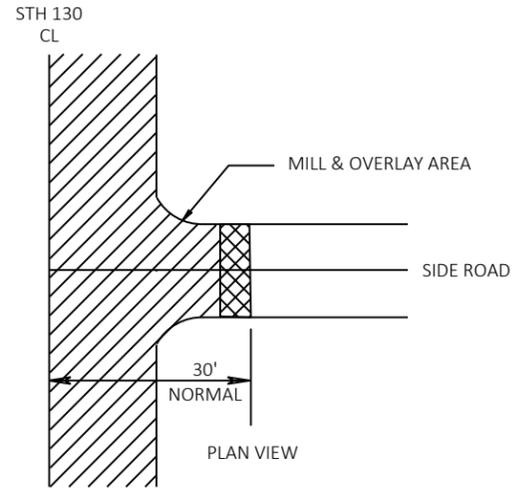
ACCESSIBLE PARKING STALL MARKING



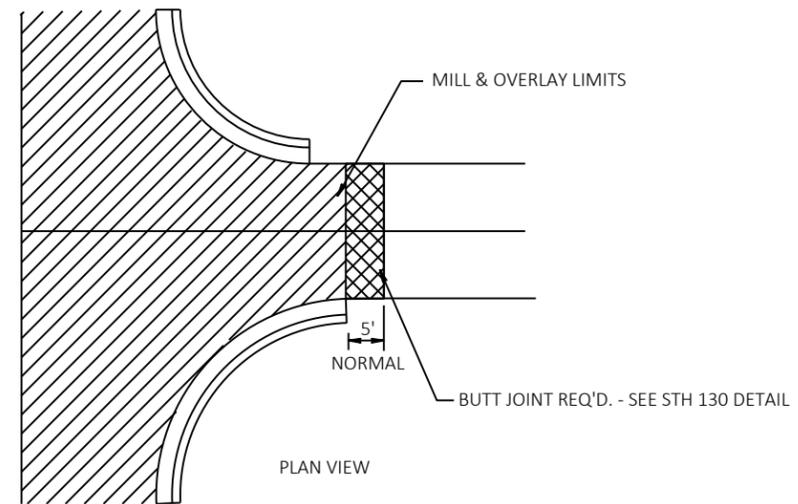
REQUIRED AT BEGINNING AND END OF PROJECT

STH 130 DETAIL

-  REMOVE ASPHALTIC SURFACE WEDGE AT BUTT JOINT TO CREATE VERTICAL EDGE PAID UNDER BID ITEM REMOVING ASPHALTIC SURFACE BUTT JOINTS
-  REMOVING ASPHALTIC SURFACE MILLING



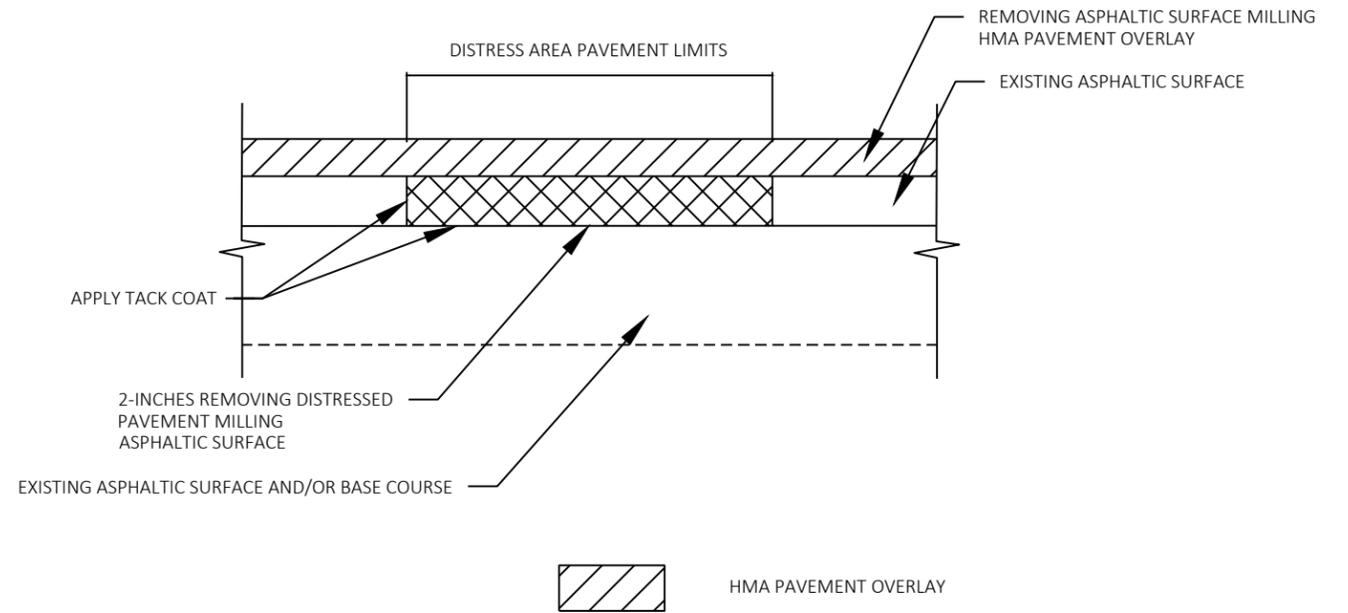
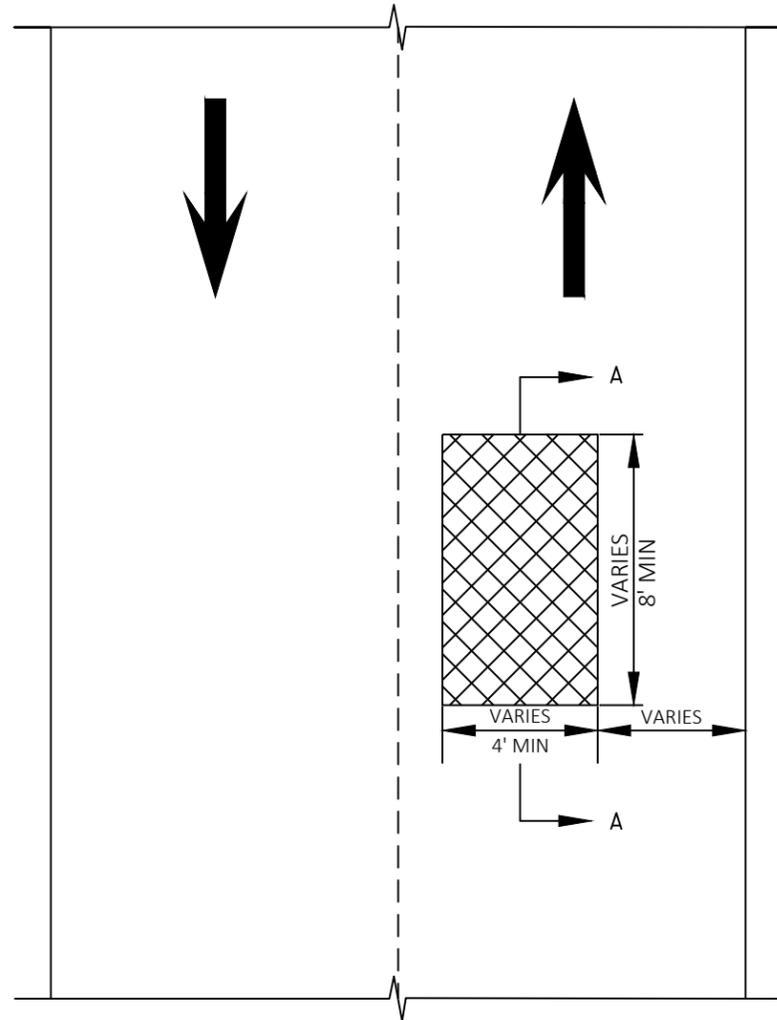
SIDE ROAD DETAIL - NO CURB & GUTTER



SIDE ROAD DETAIL - CURB & GUTTER TO REMAIN

-  REMOVE MATERIAL UNDER ITEM 'REMOVING ASPHALT SURFACE MILLING '
 -  REMOVE MATERIAL UNDER ITEM 'REMOVING ASPHALT SURFACE , BUTT JOINTS' MATERIAL SHALL NOT BE REMOVED UNDER THIS ITEM UNTIL 24 HOURS BEFORE SIDE ROAD PAVING.
- SIDE ROAD PAVEMENT DEPTH SHALL MATCH AT MAINLINE PAVEMENT EDGE AND BE TAPERED TO 2" MINIMUM AT JOINT
- SAWING ASPHALT IS INCIDENTAL TO REMOVING ASPHALT SURFACE BUTT JOINT ITEM
- *REMOVING ASPHALTIC SURFACE BUTT JOINT LENGTH EQUALS 5- FEET ON SIDE ROADS

BUTT JOINT DETAILS



REMOVING DISTRESSED PAVEMENT MILLING

EXACT LOCATION AND LIMITS OF REMOVING DISTRESSED PAVEMENT MILLING TO BE DETERMINED BY THE ENGINEER IN THE FIELD

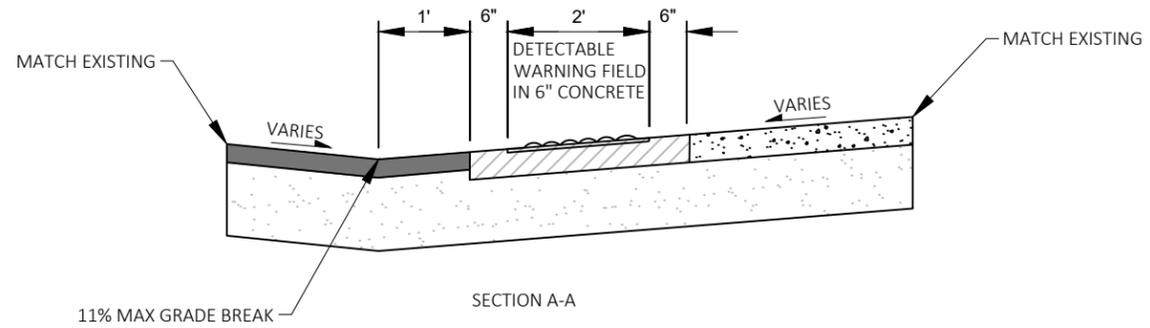
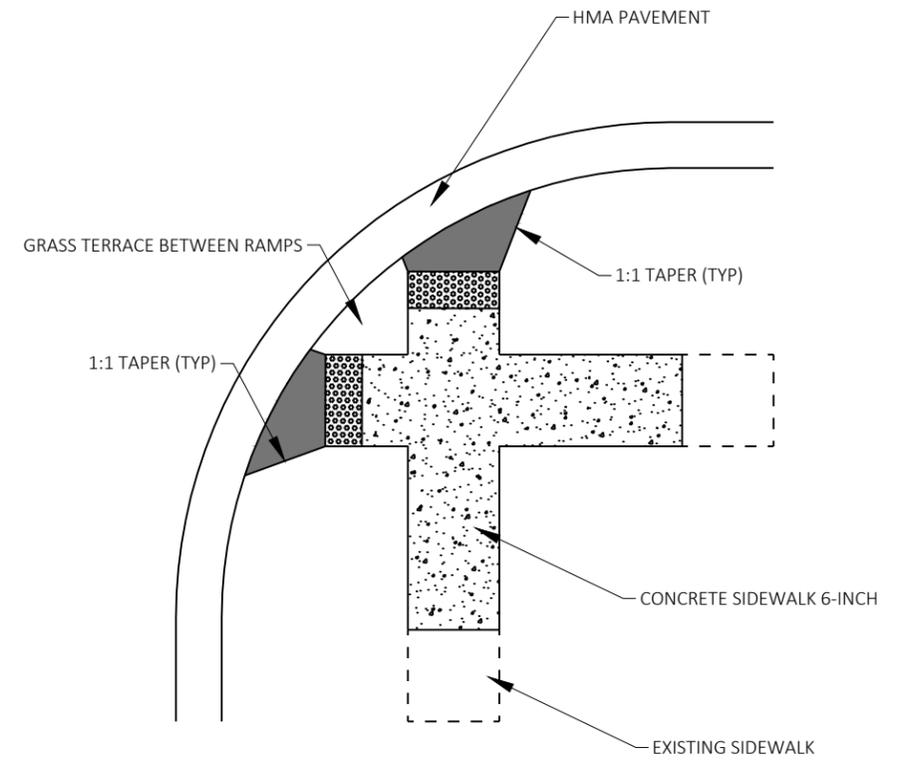
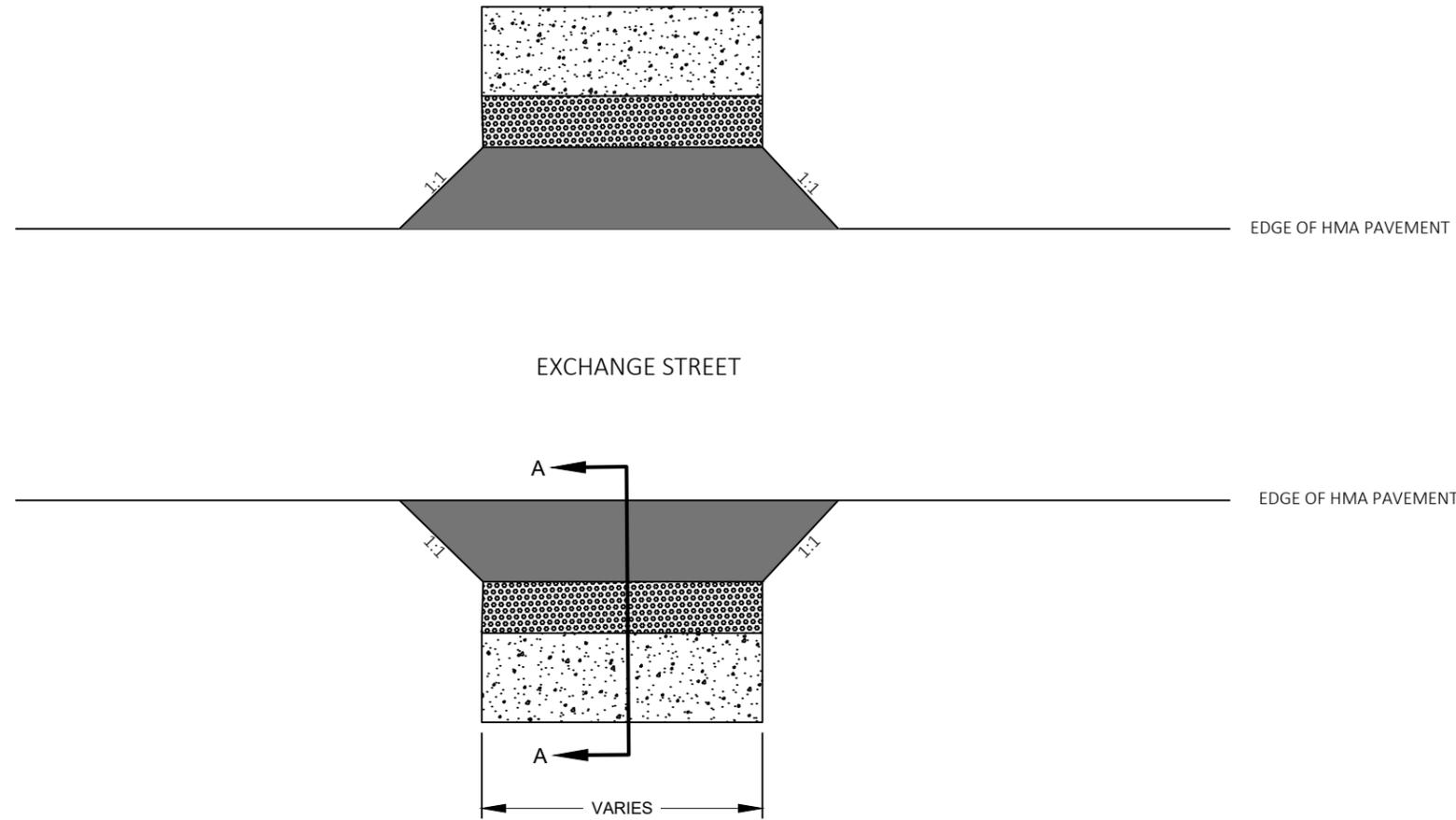


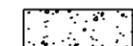
TRAVEL DIRECTION

SECTION A-A

PLAN VIEW

REMOVING DISTRESSED PAVEMENT MILLING

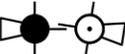


-  3.5" HMA PAVEMENT
-  DETECTABLE WARNING FIELD IN 6" CONCRETE
-  6" CONCRETE SIDEWALK
-  BASE AGGREGATE DENSE

CURB RAMPS - NO CURB AND GUTTER

NOTE: FOR ADDITIONAL INFORMATION NOT SHOWN REFER TO CURB RAMP DETAILS SHEETS.

LEGEND

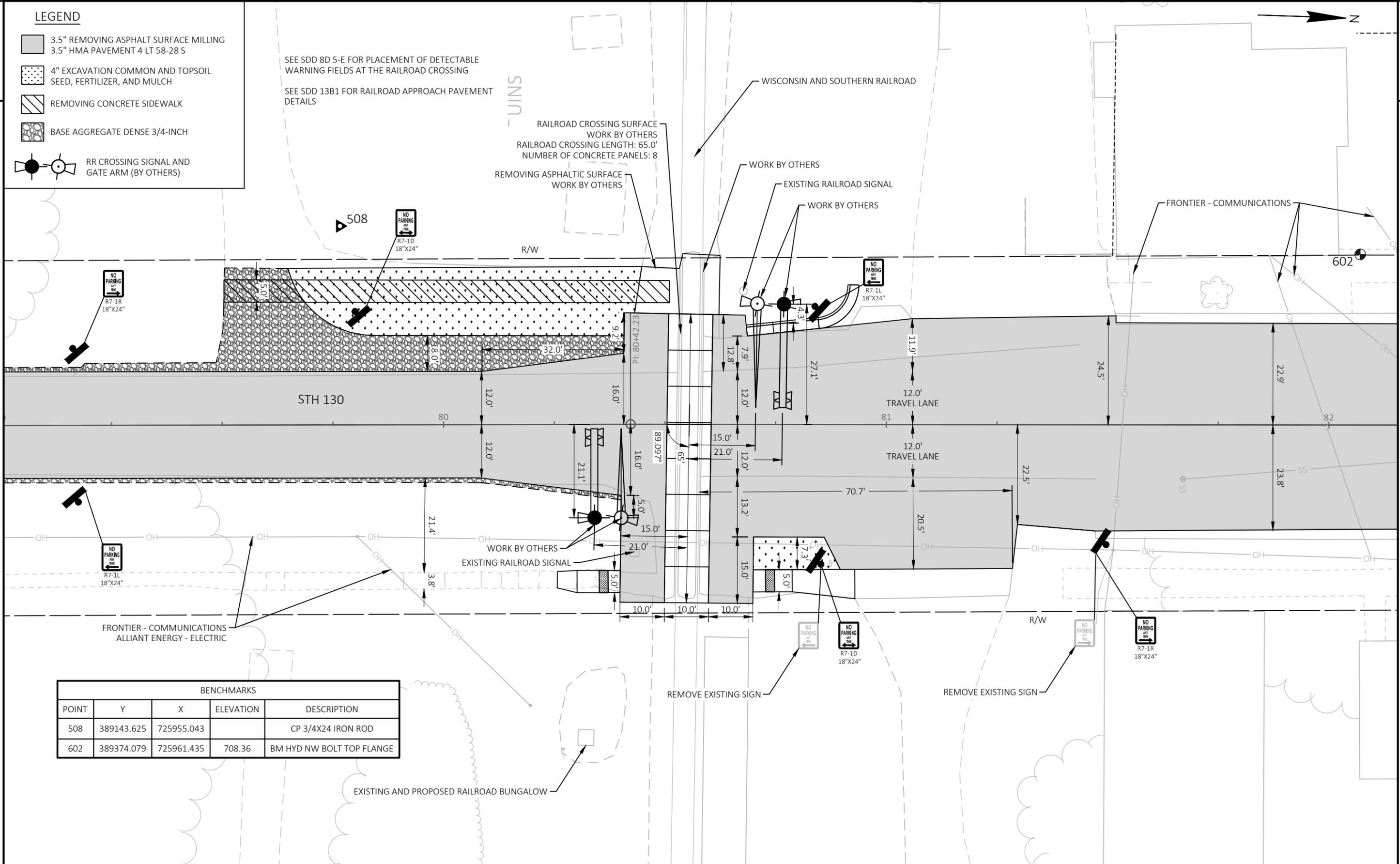
-  3.5" REMOVING ASPHALT SURFACE MILLING
3.5" HMA PAVEMENT 4 LT 58-28 S
-  4" EXCAVATION COMMON AND TOPSOIL
SEED, FERTILIZER, AND MULCH
-  REMOVING CONCRETE SIDEWALK
-  BASE AGGREGATE DENSE 3/4-INCH
-  RR CROSSING SIGNAL AND
GATE ARM (BY OTHERS)

SEE SDD 8D 5-E FOR PLACEMENT OF DETECTABLE
WARNING FIELDS AT THE RAILROAD CROSSING

SEE SDD 13B1 FOR RAILROAD APPROACH PAVEMENT
DETAILS

RAILROAD CROSSING SURFACE
WORK BY OTHERS
RAILROAD CROSSING LENGTH: 65.0'
NUMBER OF CONCRETE PANELS: 8

REMOVING ASPHALTIC SURFACE
WORK BY OTHERS

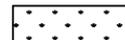


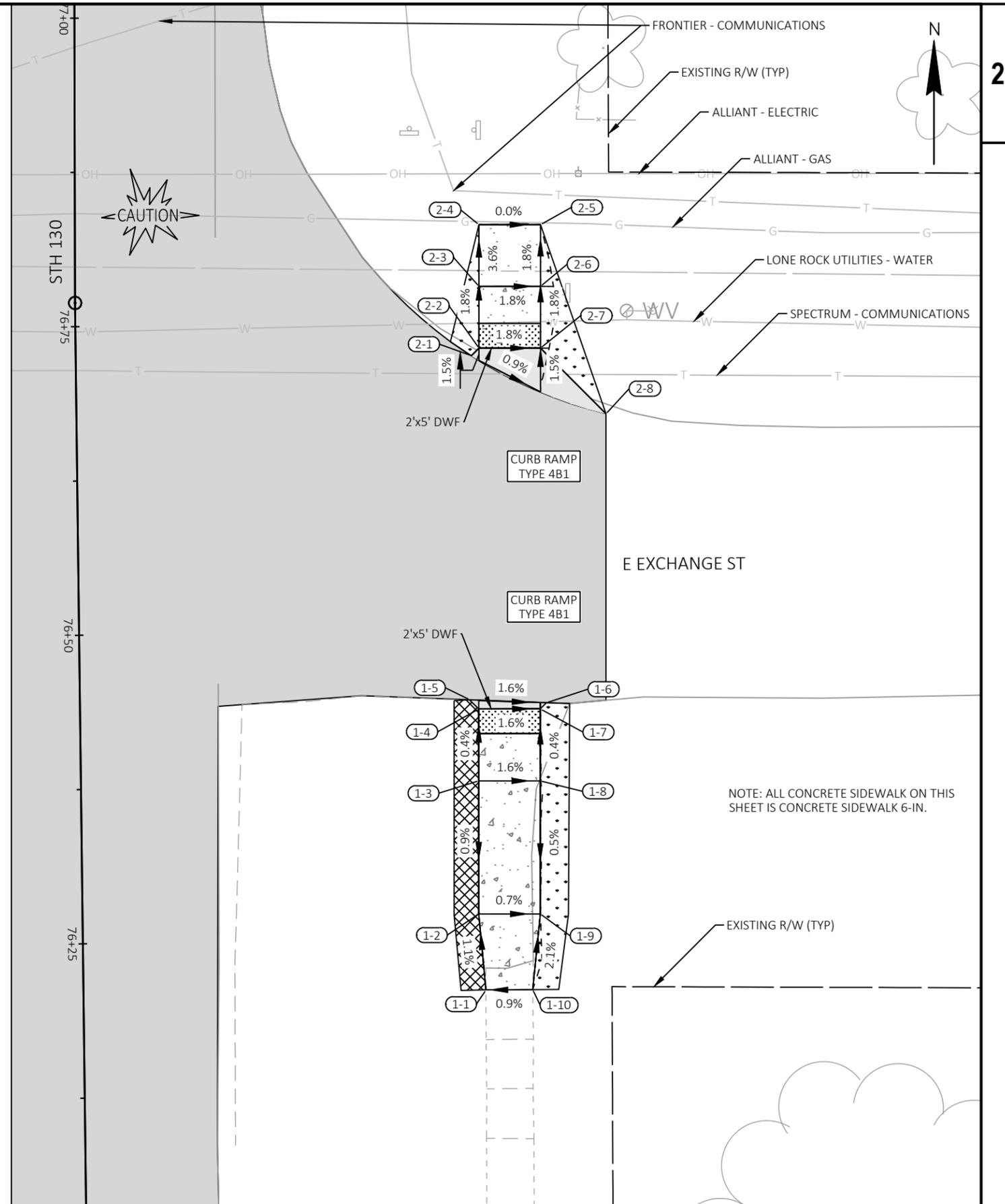
BENCHMARKS				
POINT	Y	X	ELEVATION	DESCRIPTION
508	389143.625	725955.043		CP 3/4X24 IRON ROD
602	389374.079	725961.435	708.36	BM HYD NW BOLT TOP FLANGE

Exchange NE					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
2-1	76+72.26	32.12' RT	705.59*	388839.72	726032.60
2-2	76+72.87	32.75' RT	705.57	388840.33	726033.22
2-3	76+78.21	32.80' RT	705.48	388845.33	726033.22
2-4	76+83.21	32.81' RT	705.30*	388850.33	726033.22
2-5	76+83.20	37.81' RT	705.30*	388850.33	726038.22
2-6	76+78.20	37.80' RT	705.39	388845.33	726038.22
2-7	76+72.81	37.75' RT	705.48	388840.33	726038.22
2-8	76+67.42	43.00' RT	705.48*	388835.01	726043.54

Exchange SE					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
1-1	76+20.87	32.68' RT	705.79*	388788.33	726033.79
1-2	76+27.03	32.16' RT	705.73*	388794.49	726033.20
1-3	76+37.81	32.30' RT	705.83	388805.27	726033.21
1-4	76+43.64	32.38' RT	705.81	388811.10	726033.21
1-5	76+44.39	31.64' RT	705.81*	388811.84	726032.46
1-6	76+44.07	37.87' RT	705.72*	388811.59	726038.69
1-7	76+43.58	37.38' RT	705.73	388811.10	726038.21
1-8	76+37.75	37.30' RT	705.75	388805.27	726038.21
1-9	76+26.97	37.16' RT	705.70*	388794.49	726038.20
1-10	76+20.85	36.47' RT	705.83*	388788.36	726037.58

LEGEND

-  CONCRETE SIDEWALK
-  RAILROAD FLAGGING REQUIRED
-  * MATCH EXISTING ELEV.
-  3.5" HMA 4 LT 58-28 S
-  TOPSOIL, SEEDING, MULCHING, FERTILIZER
-  (L) LANDING
-  ASPHALTIC SURFACE PATCHING
-  PED CONCRETE CURB PEDESTRIAN
-  (EX L) EXISTING LANDING

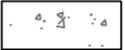
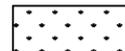


NOTE: ALL CONCRETE SIDEWALK ON THIS SHEET IS CONCRETE SIDEWALK 6-IN.

RR NE					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
4-1	80+69.96	32.73' RT	707.78*	389236.99	726032.46
4-2	80+72.75	32.71' RT	707.74	389239.78	726032.45
4-3	80+78.76	32.68' RT	707.65	389245.78	726032.42
4-4	80+92.90	32.62' RT	707.52*	389259.93	726032.38
4-5	80+92.92	39.26' RT	707.39*	389259.94	726039.01
4-6	80+78.79	37.68' RT	707.60	389245.81	726037.42
4-7	80+72.78	37.71' RT	707.69	389239.80	726037.45
4-8	80+69.89	37.73' RT	707.74*	389236.91	726037.46

RR SE					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
3-1	80+25.64	33.12' RT	707.81*	389192.76	726032.86
3-2	80+30.07	32.92' RT	707.79	389197.19	726032.65
3-3	80+35.07	32.91' RT	707.76	389202.19	726032.63
3-4	80+39.86	32.89' RT	707.73*	389206.98	726032.61
3-5	80+39.77	37.89' RT	707.71*	389206.90	726037.61
3-6	80+35.07	37.91' RT	707.74	389202.20	726037.63
3-7	80+30.09	37.92' RT	707.76	389197.22	726037.65
3-8	80+25.60	36.84' RT	707.73*	389192.73	726036.58

LEGEND

-  CONCRETE SIDEWALK
-  3.5" HMA 4 LT 58-28 S
-  ASPHALTIC SURFACE PATCHING
-  RAILROAD FLAGGING REQUIRED
-  TOPSOIL, SEEDING, MULCHING, FERTILIZER
-  CONCRETE CURB PEDESTRIAN
- * MATCH EXISTING ELEV.
-  LANDING
-  EXISTING LANDING

PROJECT NO: 5770-02-71

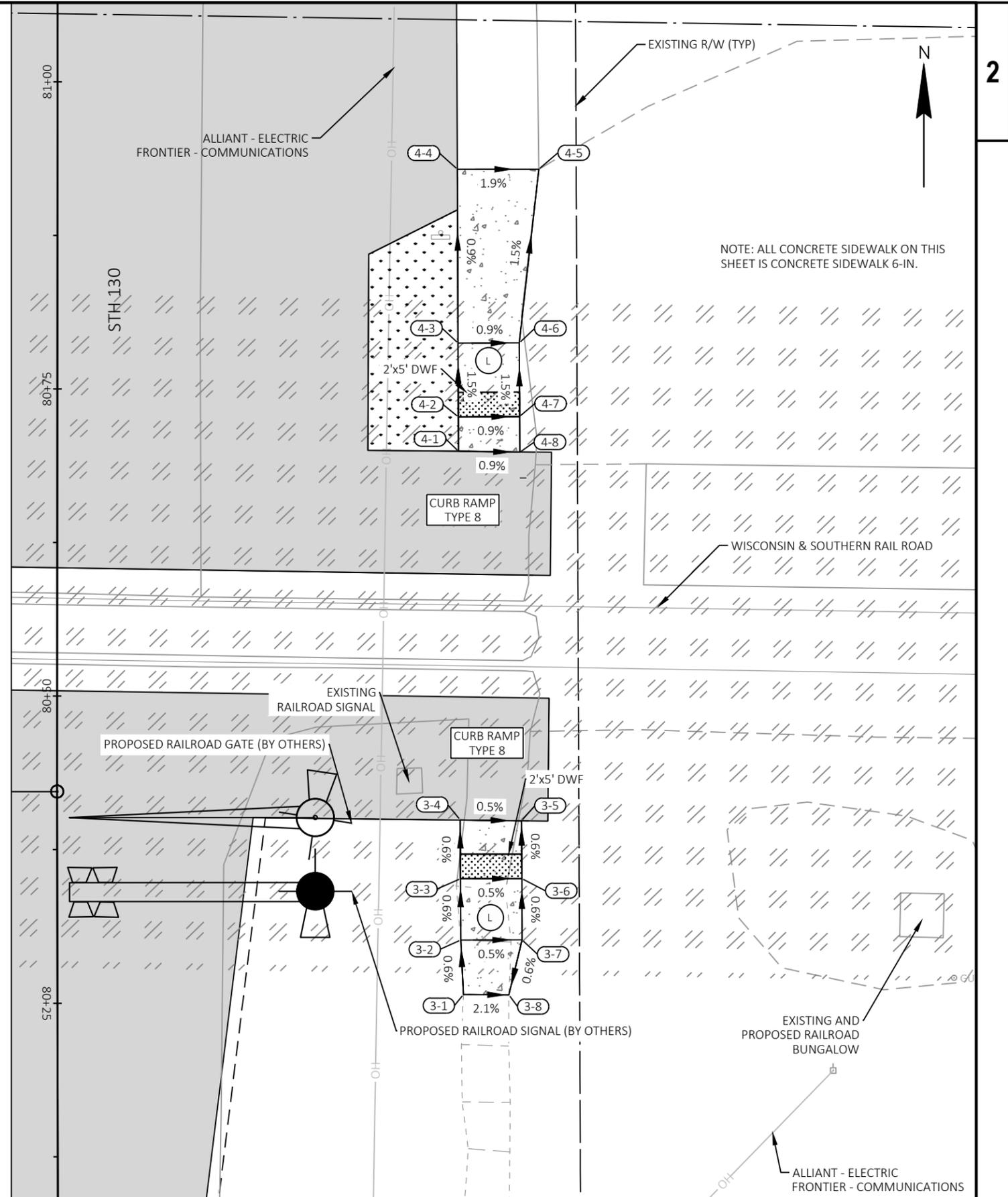
HWY: STH 130

COUNTY: RICHLAND

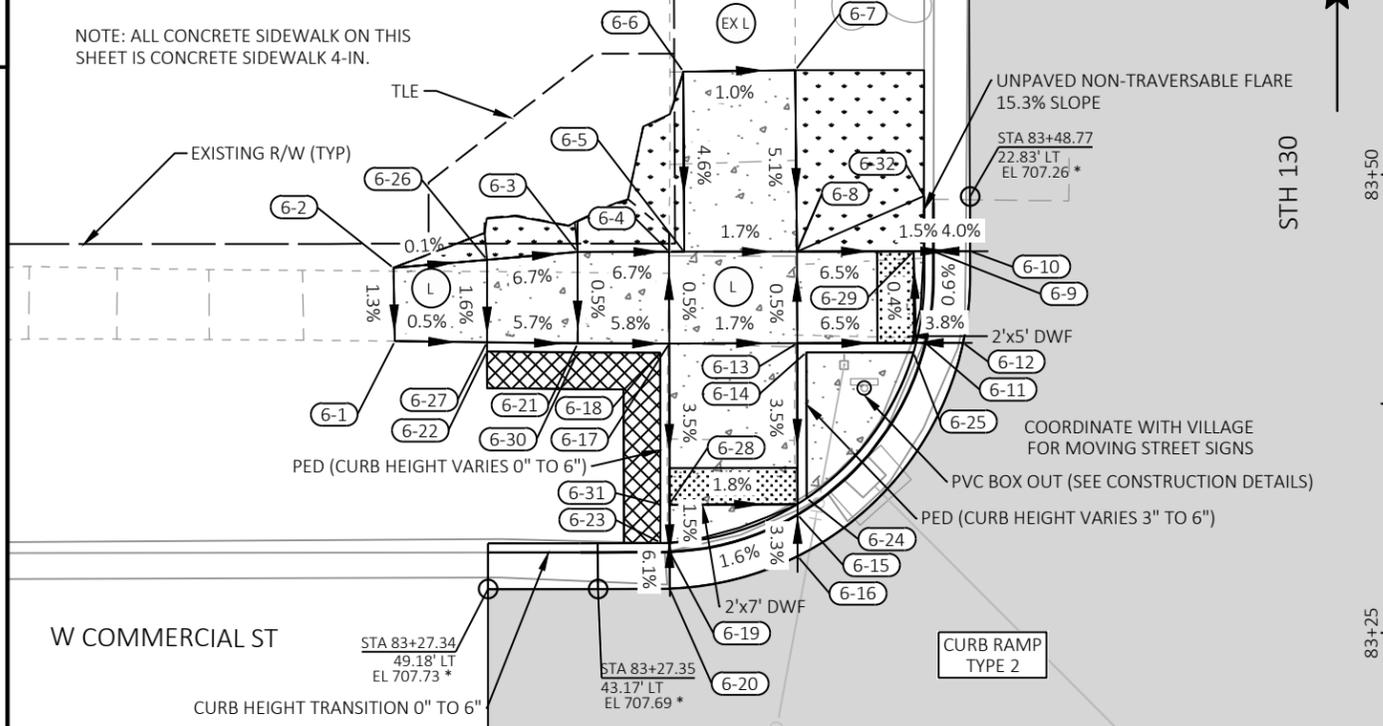
CURB RAMP DETAILS - RAILROAD EAST

SHEET

E



NOTE: ALL CONCRETE SIDEWALK ON THIS SHEET IS CONCRETE SIDEWALK 4-IN.



STH 130
83+50
83+25

W COMMERCIAL ST

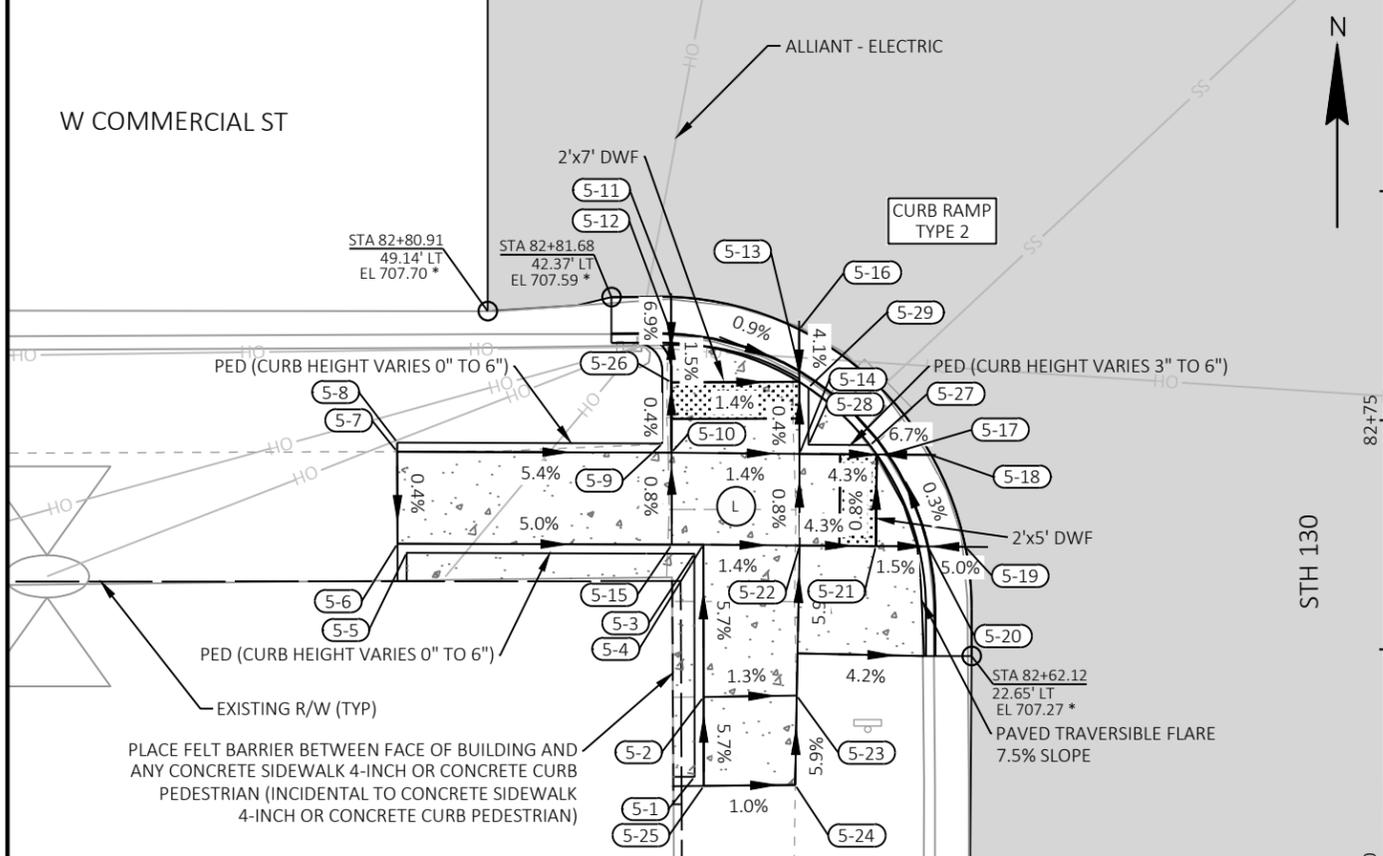
CURB HEIGHT TRANSITION 0" TO 6"

Commercial NW					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
6-1	83+40.86	54.29' LT	708.34*	389507.96	725945.68
6-2	83+44.86	54.37' LT	708.40*	389511.96	725945.61
6-3	83+45.73	44.30' LT	708.06	389512.83	725955.68
6-4	83+45.75	39.30' LT	707.72	389512.84	725960.68
6-5	83+45.75	38.47' LT	707.71	389512.84	725961.51
6-6	83+55.57	38.52' LT	708.16*	389522.66	725961.47
6-7	83+55.67	32.41' LT	708.10*	389522.75	725967.58
6-8	83+45.77	32.30' LT	707.60	389512.86	725967.68
6-9	83+45.79	24.83' LT	707.17	389512.87	725975.15
6-10	83+45.80	22.83' LT	707.25*	389512.87	725977.15
6-11	83+40.79	25.27' LT	707.20	389507.87	725974.71
6-12	83+40.79	23.21' LT	707.32*	389507.87	725976.76
6-13	83+40.77	32.28' LT	707.62	389507.86	725967.69
6-14	83+40.27	31.78' LT	707.85*	389507.35	725968.19
6-15	83+31.41	32.25' LT	707.34	389498.50	725967.71
6-16	83+29.14	32.25' LT	707.42*	389496.23	725967.72

Commercial NW					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
6-17	83+40.75	39.28' LT	707.75	389507.84	725960.69
6-18	83+40.25	39.78' LT	708.01*	389507.34	725960.19
6-19	83+29.37	39.25' LT	707.46	389496.46	725960.72
6-20	83+27.37	39.24' LT	707.58	389494.46	725960.72
6-21	83+40.73	44.28' LT	708.03	389507.83	725955.69
6-22	83+40.30	49.25' LT	708.20*	389507.40	725950.73
6-23	83+29.88	39.75' LT	707.98	389496.98	725960.22
6-24	83+32.32	31.76' LT	707.77	389499.40	725968.21
6-25	83+40.26	25.93' LT	707.70	389507.34	725974.05
6-26	83+45.30	49.25' LT	708.39	389512.40	725950.73
6-27	83+40.80	49.25' LT	708.32	389507.90	725950.72
6-28	83+32.00	39.26' LT	707.48	389499.09	725960.71
6-29	83+45.79	25.92' LT	707.18	389512.87	725974.06
6-30	83+40.23	44.29' LT	708.11*	389507.33	725955.69
6-31	83+31.96	39.76' LT	707.98	389499.05	725960.21
6-32	83+48.80	25.36' LT	707.63*	389515.88	725974.63

Commercial SW					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
5-1	82+55.53	37.80' LT	708.21*	389422.62	725962.10
5-2	82+59.88	37.31' LT	707.93	389426.97	725962.60
5-3	82+67.69	37.83' LT	708.05*	389434.78	725962.08
5-4	82+68.19	37.33' LT	707.46	389435.28	725962.58
5-5	82+67.73	53.55' LT	708.23*	389434.83	725946.36
5-6	82+68.23	54.06' LT	708.23*	389435.33	725945.85
5-7	82+73.24	54.08' LT	708.25*	389440.35	725945.84
5-8	82+73.74	54.08' LT	708.20*	389440.85	725945.84
5-9	82+73.71	39.58' LT	707.92*	389440.80	725960.34
5-10	82+73.20	39.08' LT	707.44	389440.29	725960.84
5-11	82+81.69	39.10' LT	707.54*	389448.78	725960.82
5-12	82+79.68	39.10' LT	707.40	389446.77	725960.82
5-13	82+77.65	32.09' LT	707.33	389444.74	725967.83
5-14	82+73.15	32.08' LT	707.34	389440.23	725967.84
5-15	82+68.19	39.06' LT	707.48	389435.28	725960.85

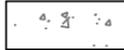
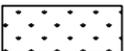
Commercial SW					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
5-16	82+79.92	32.10' LT	707.43*	389447.01	725967.82
5-17	82+73.11	27.24' LT	707.15	389440.19	725972.68
5-18	82+73.09	24.87' LT	707.30*	389440.17	725975.04
5-19	82+68.08	22.99' LT	707.27*	389435.16	725976.92
5-20	82+68.10	25.04' LT	707.17	389435.17	725974.87
5-21	82+68.12	27.89' LT	707.20	389435.20	725972.02
5-22	82+68.15	32.06' LT	707.38	389435.23	725967.85
5-23	82+59.96	32.22' LT	707.87	389427.05	725967.68
5-24	82+55.08	32.29' LT	708.16*	389422.17	725967.61
5-25	82+55.03	37.29' LT	708.21*	389422.12	725962.61
5-26	82+77.04	39.09' LT	707.43	389444.13	725960.83
5-27	82+73.62	28.22' LT	707.63	389440.70	725971.69
5-28	82+73.65	31.58' LT	707.70*	389440.73	725968.34
5-29	82+76.75	31.59' LT	707.75	389443.83	725968.33



STH 130
82+75
82+50

W COMMERCIAL ST

LEGEND

-  CONCRETE SIDEWALK
-  RAILROAD FLAGGING REQUIRED
-  * MATCH EXISTING ELEV.
-  3.5" HMA 4 LT 58-28 S
-  TOPSOIL, SEEDING, MULCHING, FERTILIZER
-  (L) LANDING
-  ASPHALTIC SURFACE PATCHING
-  PED CONCRETE CURB PEDESTRIAN
-  (EX L) EXISTING LANDING

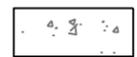
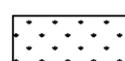
Commercial NE					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
8-1	83+30.56	38.97' RT	706.81	389497.58	726038.94
8-2	83+30.11	39.47' RT	707.29	389497.13	726039.44
8-3	83+32.54	34.02' RT	706.85	389499.57	726033.99
8-4	83+33.51	39.48' RT	707.24	389500.53	726039.45
8-5	83+29.60	38.98' RT	706.81	389496.63	726038.94
8-6	83+27.60	38.98' RT	706.88*	389494.63	726038.94
8-7	83+30.02	34.02' RT	706.77	389497.05	726033.99
8-8	83+27.96	34.03' RT	706.85*	389494.99	726033.99
8-9	83+40.94	25.23' RT	706.79	389507.97	726025.21
8-10	83+40.92	23.20' RT	706.83*	389507.96	726023.18
8-11	83+40.97	33.14' RT	707.20	389507.99	726033.12
8-12	83+45.92	23.16' RT	706.88*	389512.96	726023.14
8-13	83+45.94	25.16' RT	706.80	389512.98	726025.14

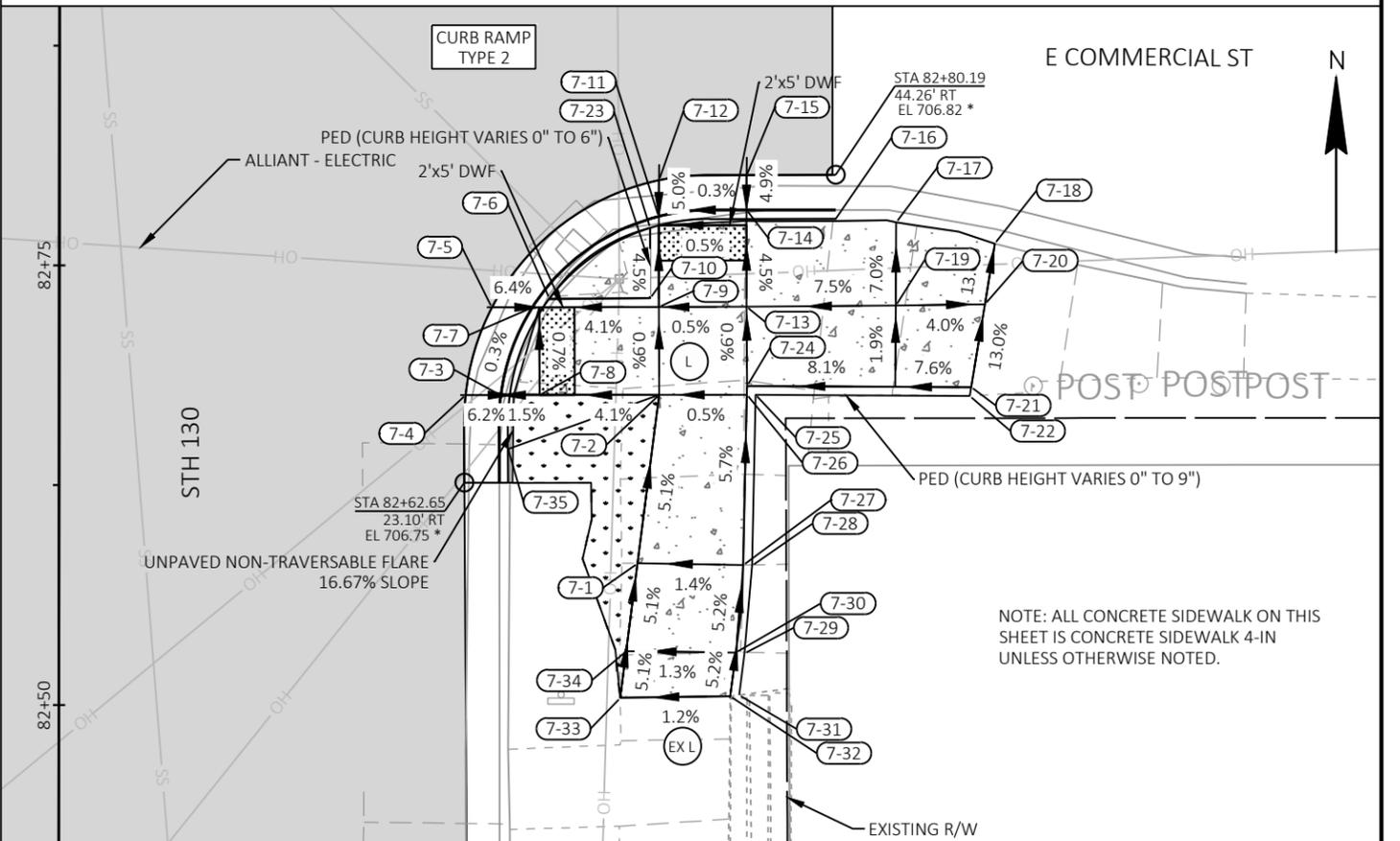
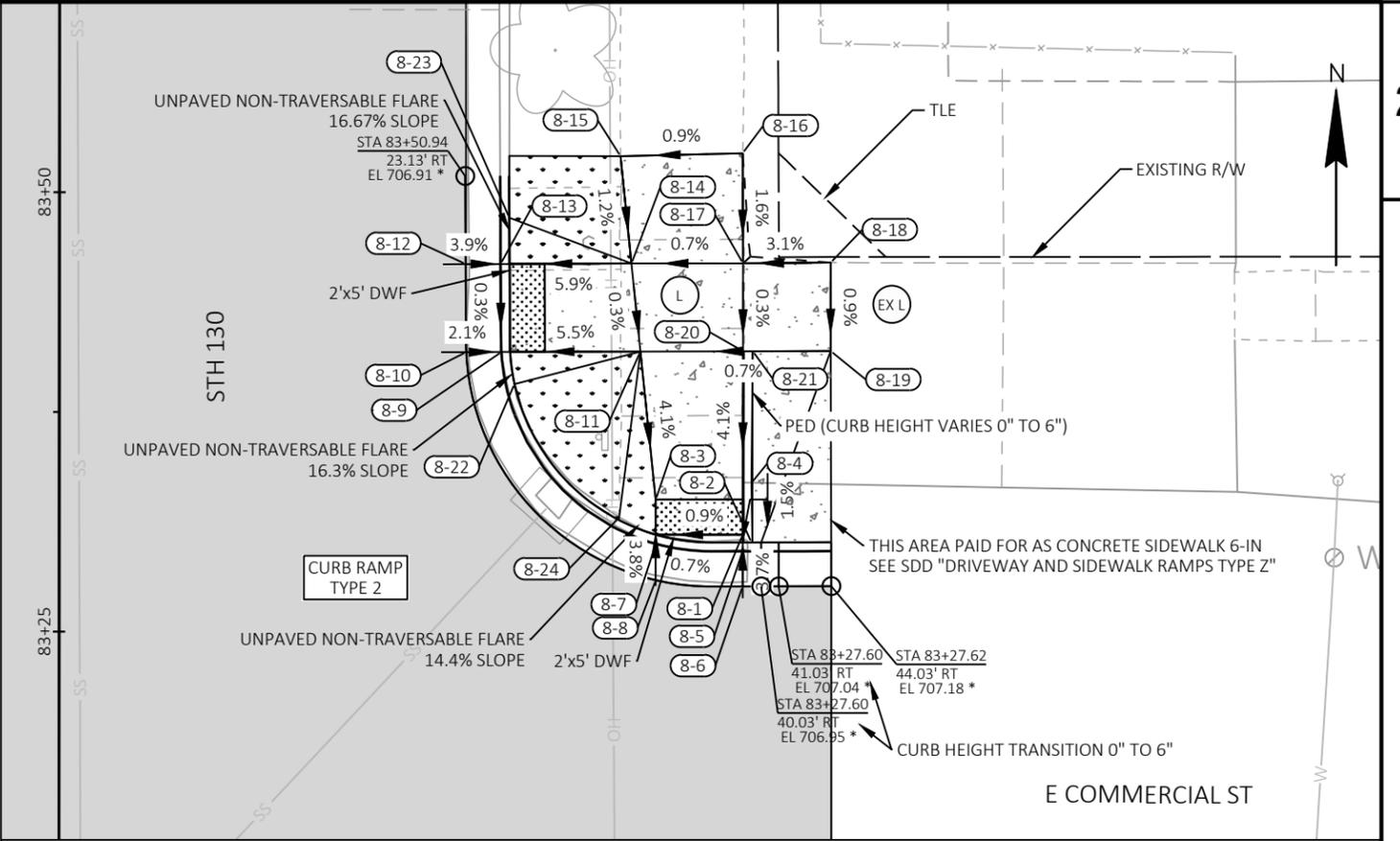
Commercial NE					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
8-14	83+45.97	32.63' RT	707.22	389512.99	726032.61
8-15	83+52.07	31.99' RT	707.29*	389519.10	726031.98
8-16	83+52.24	38.96' RT	707.36*	389519.26	726038.95
8-17	83+45.99	38.98' RT	707.26	389513.01	726038.96
8-18	83+46.03	43.98' RT	707.42*	389513.05	726043.96
8-19	83+41.00	43.99' RT	707.37*	389508.02	726043.97
8-20	83+40.99	38.99' RT	707.24	389508.01	726038.97
8-21	83+40.99	39.54' RT	707.26	389508.01	726039.52
8-22	83+39.11	25.96' RT	707.12	389506.14	726025.93
8-23	83+48.56	25.65' RT	707.19	389515.59	726025.63
8-24	83+31.52	31.93' RT	707.16	389498.55	726031.90

Commercial SE					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
7-1	82+58.08	32.99' RT	707.42	389425.11	726032.89
7-2	82+67.67	34.21' RT	706.92	389434.69	726034.13
7-3	82+67.64	25.18' RT	706.62	389434.68	726025.09
7-4	82+67.64	23.17' RT	706.74	389434.68	726023.08
7-5	82+72.64	24.67' RT	706.75	389439.67	726024.59
7-6	82+73.15	27.95' RT	707.07	389440.18	726027.86
7-7	82+72.63	26.98' RT	706.60	389439.67	726026.89
7-8	82+67.65	27.40' RT	706.64	389434.68	726027.31
7-9	82+72.67	34.20' RT	706.88	389439.69	726034.12
7-10	82+73.16	33.70' RT	707.28*	389440.19	726033.61
7-11	82+77.82	34.18' RT	706.67	389444.84	726034.10
7-12	82+79.87	34.20' RT	706.77	389446.90	726034.12
7-13	82+72.68	39.20' RT	706.90	389439.70	726039.12
7-14	82+78.18	39.20' RT	706.69	389445.20	726039.12
7-15	82+80.15	39.19' RT	706.79	389447.17	726039.12
7-16	82+77.55	44.19' RT	706.99*	389444.57	726044.11
7-17	82+77.50	47.73' RT	707.21*	389444.51	726047.65
7-18	82+76.29	53.35' RT	706.88*	389443.30	726053.27

Commercial SE					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
7-19	82+72.76	47.71' RT	707.54	389439.78	726047.62
7-20	82+72.84	52.78' RT	707.34*	389439.85	726052.70
7-21	82+68.12	52.01' RT	707.96*	389435.13	726051.92
7-22	82+67.62	51.93' RT	708.02*	389434.63	726051.84
7-23	82+77.16	33.69' RT	707.15	389444.18	726033.61
7-24	82+68.18	39.22' RT	706.94	389435.20	726039.14
7-25	82+67.68	39.71' RT	707.77*	389434.70	726039.63
7-26	82+67.68	39.21' RT	706.95	389434.70	726039.13
7-27	82+57.99	39.02' RT	707.50	389425.01	726038.93
7-28	82+57.97	39.52' RT	707.77*	389424.99	726039.43
7-29	82+52.97	39.10' RT	707.86*	389419.99	726039.00
7-30	82+53.02	38.60' RT	707.76	389420.04	726038.50
7-31	82+50.60	38.79' RT	707.89*	389417.62	726038.69
7-32	82+50.53	38.28' RT	707.89*	389417.56	726038.18
7-33	82+50.44	32.01' RT	707.81*	389417.47	726031.91
7-34	82+53.09	32.35' RT	707.68	389420.12	726032.25
7-35	82+64.57	25.60' RT	707.12	389431.61	726025.50

LEGEND

-  CONCRETE SIDEWALK
-  RAILROAD FLAGGING REQUIRED
-  * MATCH EXISTING ELEV.
-  3.5\" HMA 4 LT 58-28 S
-  TOPSOIL, SEEDING, MULCHING, FERTILIZER
-  (L) LANDING
-  ASPHALTIC SURFACE PATCHING
-  PED CONCRETE CURB PEDESTRIAN
-  (EX L) EXISTING LANDING



NOTE: ALL CONCRETE SIDEWALK ON THIS SHEET IS CONCRETE SIDEWALK 4-IN UNLESS OTHERWISE NOTED.

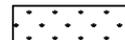
Richland NE					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
12-1	86+99.33	46.10' RT	706.36*	389866.35	726046.39
12-2	86+98.83	46.09' RT	706.32*	389865.85	726046.39
12-3	86+96.92	38.10' RT	706.26*	389863.94	726038.40
12-4	86+98.94	38.10' RT	706.16	389865.97	726038.39
12-5	87+00.85	38.59' RT	706.61*	389867.87	726038.89
12-6	86+99.35	40.08' RT	706.34*	389866.38	726040.38
12-7	87+07.04	38.07' RT	706.42	389874.07	726038.38
12-8	86+99.11	31.10' RT	706.20	389866.14	726031.39
12-9	87+01.48	31.09' RT	706.09	389868.50	726031.39
12-10	87+06.52	30.57' RT	707.02*	389873.55	726030.88
12-11	87+07.02	31.07' RT	706.33	389874.05	726031.38
12-12	87+07.01	26.21' RT	706.11	389874.04	726026.52
12-13	87+07.00	24.00' RT	706.17	389874.04	726024.30
12-14	87+12.00	22.66' RT	706.21*	389879.03	726022.97
12-15	87+12.00	24.67' RT	706.17	389879.04	726024.98
12-16	87+12.02	31.06' RT	706.40	389879.05	726031.37

Richland NE					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
12-17	87+21.68	31.25' RT	706.95*	389888.71	726031.56
12-18	87+21.72	37.99' RT	707.07*	389888.74	726038.31
12-19	87+21.71	38.49' RT	707.07*	389888.73	726038.81
12-20	87+12.04	38.06' RT	706.49	389879.06	726038.37
12-21	87+07.04	38.57' RT	706.44	389874.07	726038.88
12-22	87+02.11	38.09' RT	706.20	389869.13	726038.39
12-23	87+12.01	26.76' RT	706.19	389879.04	726027.07
12-24	87+06.51	27.06' RT	706.59	389873.54	726027.36
12-25	87+02.46	30.59' RT	706.54	389869.49	726030.89
12-26	87+16.64	38.53' RT	707.08*	389883.66	726038.84
12-27	87+12.54	38.55' RT	707.07*	389879.57	726038.86
12-28	87+12.57	46.06' RT	706.97*	389879.58	726046.36
12-29	87+12.07	46.06' RT	706.95*	389879.08	726046.37
12-30	87+07.07	46.07' RT	706.80*	389874.08	726046.38
12-31	87+15.00	25.05' RT	706.61	389882.04	726025.36

Richland SE					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
11-1	86+37.34	39.35' RT	707.29*	389804.36	726039.59
11-2	86+37.34	38.85' RT	707.27*	389804.36	726039.09
11-3	86+37.34	31.22' RT	707.23*	389804.37	726031.46
11-4	86+49.67	31.31' RT	706.41	389816.69	726031.56
11-5	86+49.64	22.56' RT	706.22	389816.68	726022.81
11-6	86+49.65	24.57' RT	706.15	389816.68	726024.82
11-7	86+54.64	23.89' RT	706.23	389821.68	726024.15
11-8	86+54.65	26.11' RT	706.11	389821.68	726026.37
11-9	86+54.67	31.29' RT	706.36	389821.69	726031.55
11-10	86+55.16	30.79' RT	706.82*	389822.19	726031.05
11-11	86+62.71	31.27' RT	706.28*	389829.73	726031.53
11-12	86+60.37	31.25' RT	706.20	389827.39	726031.52
11-13	86+64.79	38.76' RT	706.43*	389831.82	726039.03
11-14	86+62.79	38.77' RT	706.30	389829.82	726039.03

Richland SE					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
11-15	86+59.80	38.78' RT	706.34	389826.82	726039.04
11-16	86+59.42	30.78' RT	706.60	389826.45	726031.04
11-17	86+55.15	26.96' RT	706.59	389822.18	726027.21
11-18	86+62.30	39.27' RT	706.87	389829.33	726039.53
11-19	86+50.22	48.80' RT	707.31*	389817.23	726049.06
11-20	86+49.69	39.31' RT	707.28*	389816.71	726039.56
11-21	86+55.19	38.79' RT	706.46	389822.21	726039.05
11-22	86+50.19	38.80' RT	706.51	389817.21	726039.06
11-23	86+49.65	26.70' RT	706.17	389816.68	726026.95
11-24	86+49.72	48.81' RT	707.31*	389816.73	726049.06
11-25	86+55.22	48.79' RT	707.26*	389822.23	726049.05
11-26	86+55.19	39.29' RT	706.46	389822.21	726039.55
11-27	86+44.65	24.97' RT	706.59	389811.68	726025.22

LEGEND

-  CONCRETE SIDEWALK
-  3.5" HMA 4 LT 58-28 S
-  ASPHALTIC SURFACE PATCHING
-  RAILROAD FLAGGING REQUIRED
-  TOPSOIL, SEEDING, MULCHING, FERTILIZER
-  CONCRETE CURB PEDESTRIAN
- * MATCH EXISTING ELEV.
- (L) LANDING
- (EX L) EXISTING LANDING

PROJECT NO: 5770-02-71

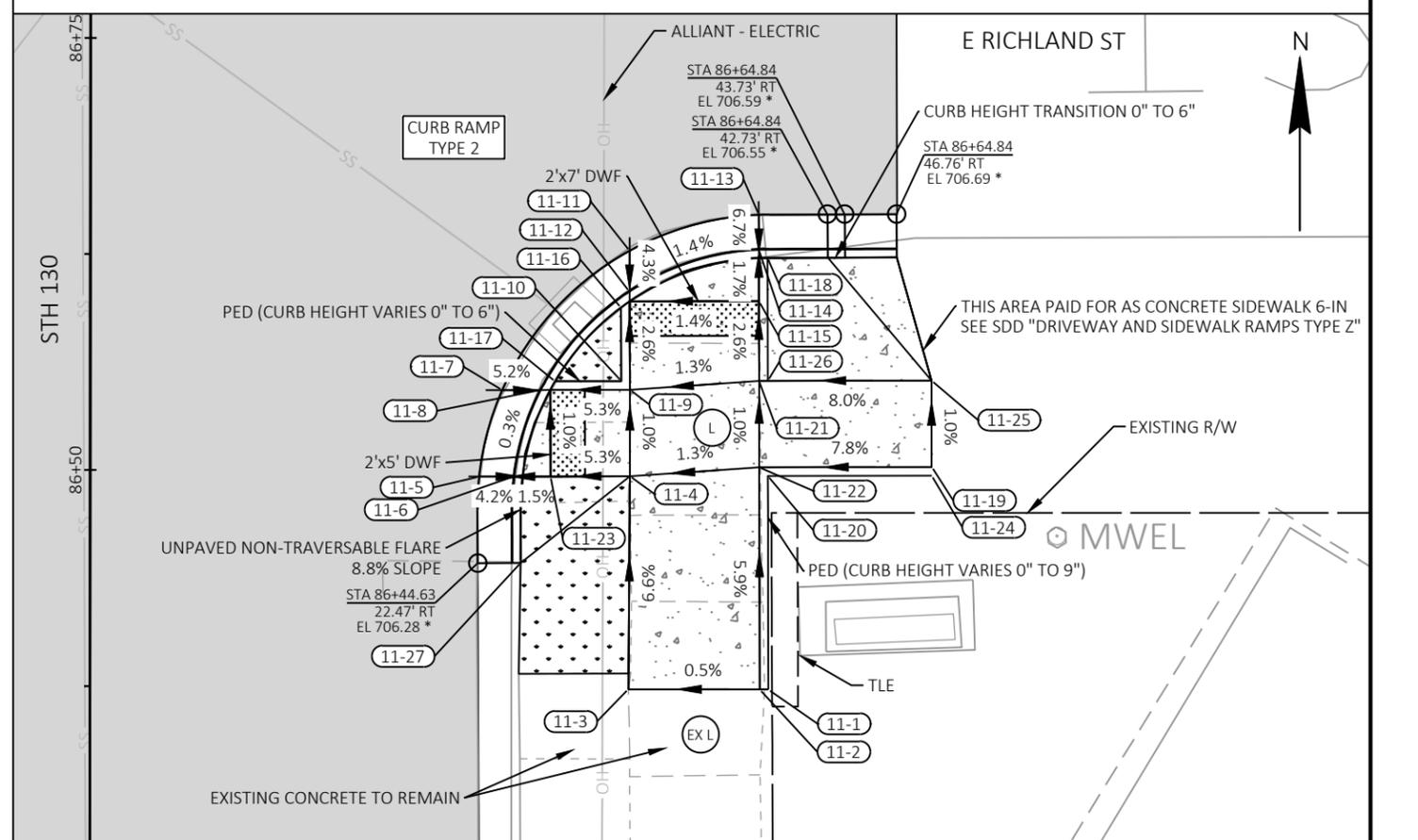
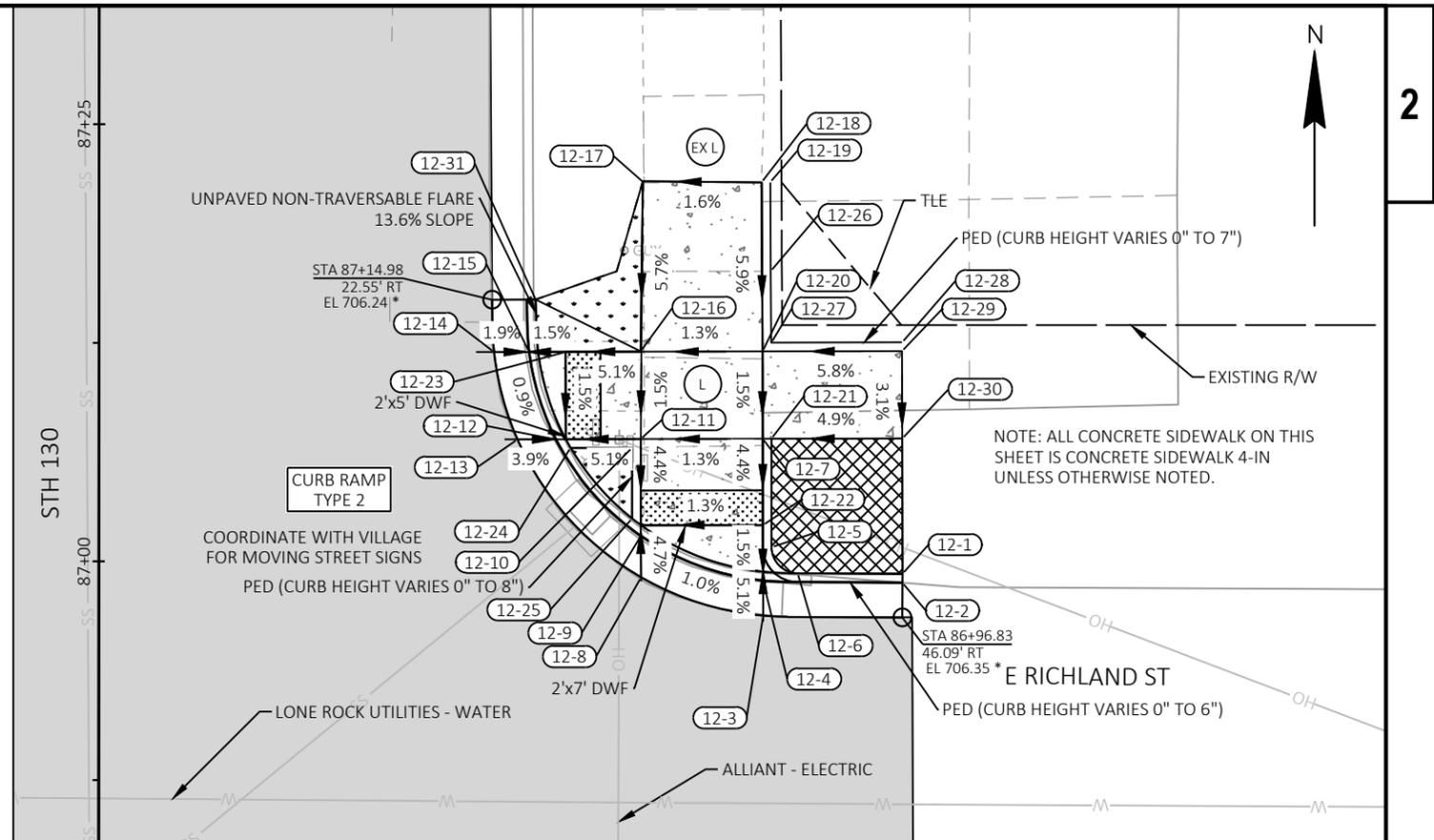
HWY: STH 130

COUNTY: RICHLAND

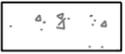
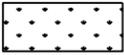
CURB RAMP DETAILS - E RICHLAND ST

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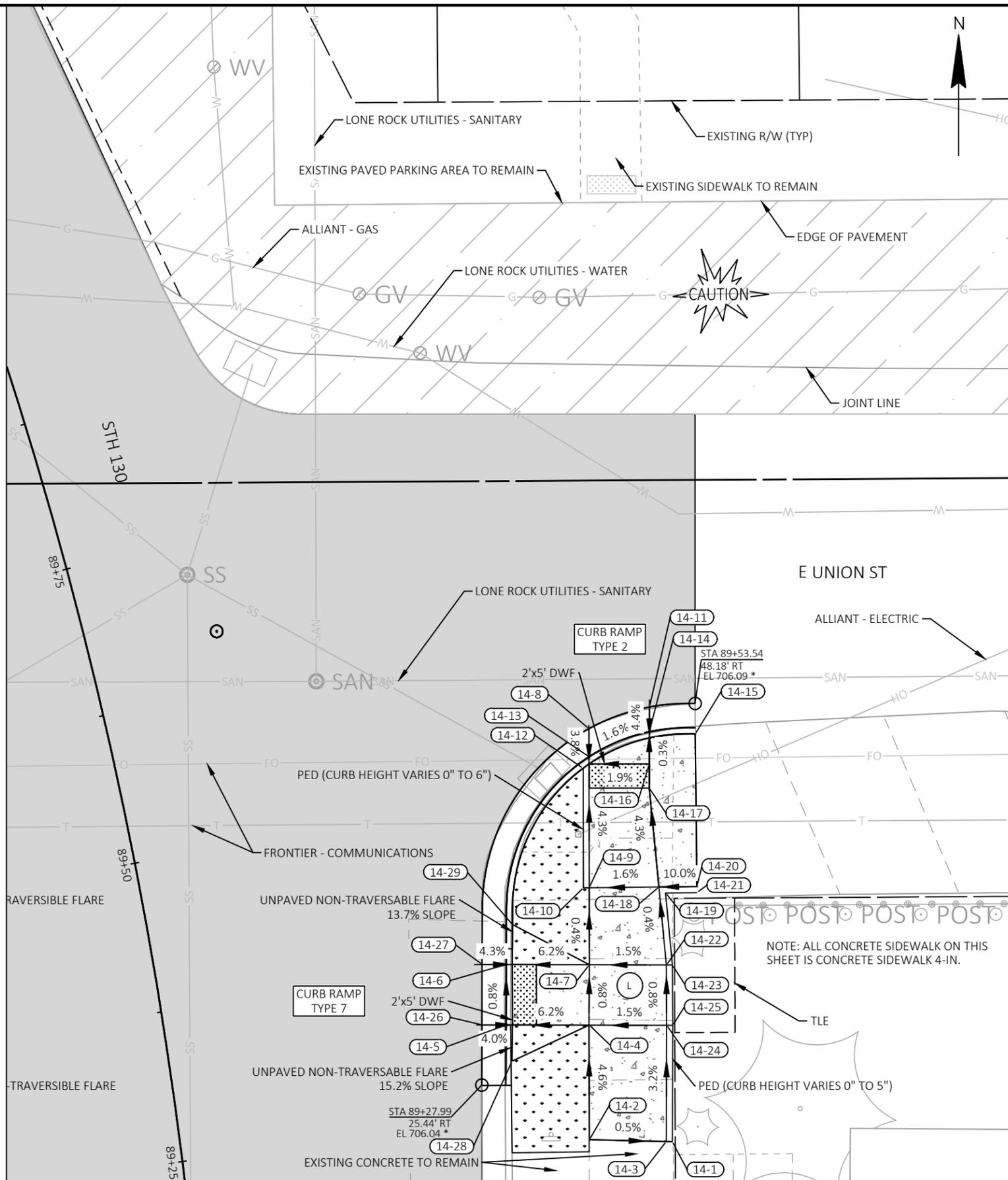


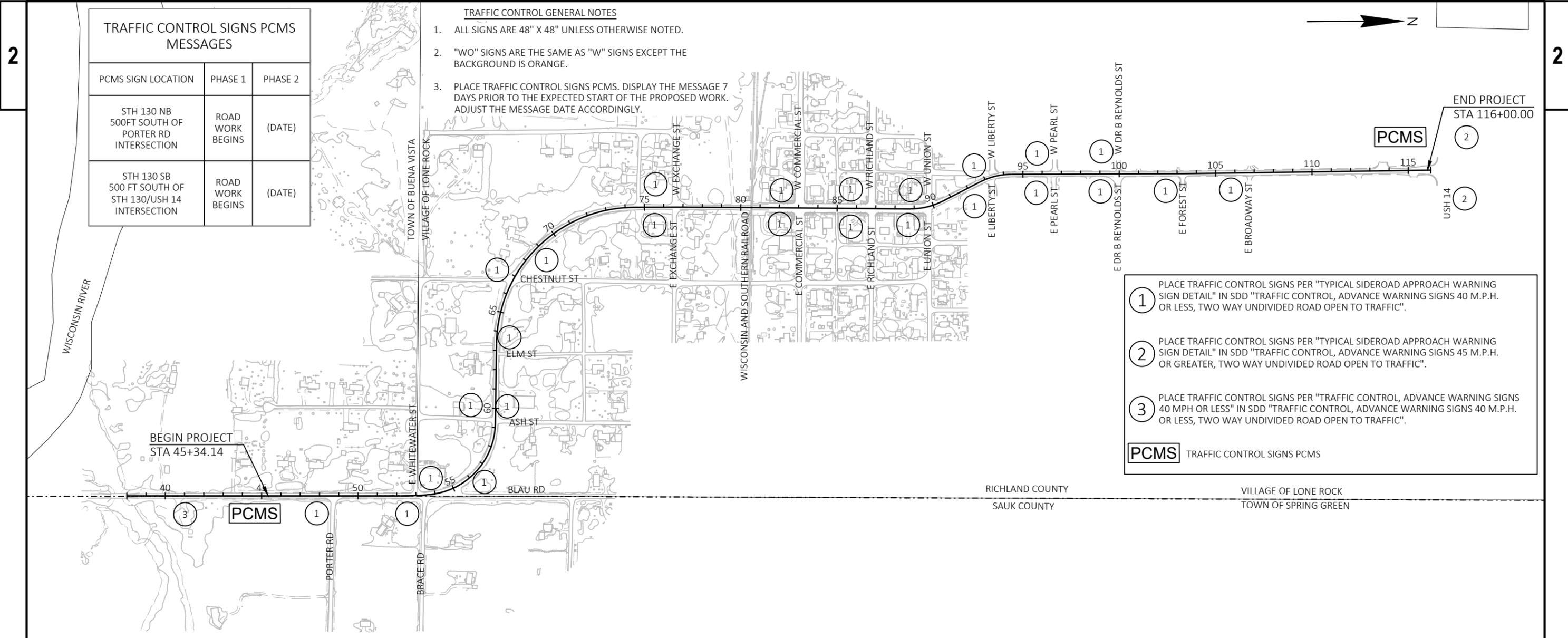
LEGEND

 CONCRETE SIDEWALK	 RAILROAD FLAGGING REQUIRED	* MATCH EXISTING ELEV.
 3.5" HMA 4 LT 58-28 S	 TOPSOIL, SEEDING, MULCHING, FERTILIZER	(L) LANDING
 ASPHALTIC SURFACE PATCHING	PED CONCRETE CURB PEDESTRIAN	(EXL) EXISTING LANDING

Union SE					
POINT	STATION	OFFSET	ELEV	NORTHING	EASTING
14-1	89+21.95	40.55' RT	706.76*	390093.59	726038.36
14-2	89+22.82	33.74' RT	706.79*	390093.75	726031.48
14-3	89+22.01	40.06' RT	706.76*	390093.60	726037.86
14-4	89+31.40	34.95' RT	706.36	390103.23	726031.47
14-5	89+32.32	28.12' RT	705.96	390103.24	726024.56
14-6	89+36.90	28.88' RT	705.92	390108.24	726024.57
14-7	89+35.90	35.69' RT	706.32	390108.23	726031.47
14-8	89+53.31	39.13' RT	705.94	390127.76	726031.43
14-9	89+41.61	36.71' RT	706.29	390114.60	726031.46
14-10	89+41.68	36.23' RT	706.31	390114.59	726030.98
14-11	89+53.97	44.41' RT	706.03*	390129.55	726036.45
14-12	89+50.50	38.01' RT	706.34	390124.49	726030.95
14-13	89+51.26	38.68' RT	705.85	390125.44	726031.45
14-14	89+52.19	44.00' RT	705.94	390127.51	726036.45
14-15	89+51.38	47.69' RT	706.04	390127.34	726040.25

Union SE					
POINT	STATION	OFFSET	ELEV*	NORTHING	EASTING
14-16	89+49.87	43.47' RT	705.94	390124.85	726036.45
14-17	89+48.12	43.09' RT	706.03	390122.85	726036.45
14-18	89+40.79	42.42' RT	706.38	390114.66	726037.24
14-19	89+40.28	42.88' RT	706.71*	390114.17	726037.79
14-20	89+40.36	45.54' RT	706.69*	390114.70	726040.40
14-21	89+39.92	45.47' RT	706.72*	390114.20	726040.40
14-22	89+35.01	42.01' RT	706.41	390108.21	726037.87
14-23	89+34.94	42.51' RT	706.78*	390108.21	726038.37
14-24	89+30.57	41.30' RT	706.45	390103.21	726037.88
14-25	89+30.51	41.80' RT	706.78*	390103.21	726038.38
14-26	89+32.59	26.14' RT	706.04*	390103.25	726022.56
14-27	89+37.19	26.91' RT	706.01*	390108.25	726022.57
14-28	89+29.51	28.19' RT	706.42	390100.25	726025.05
14-29	89+39.85	29.92' RT	706.37	390111.56	726025.08





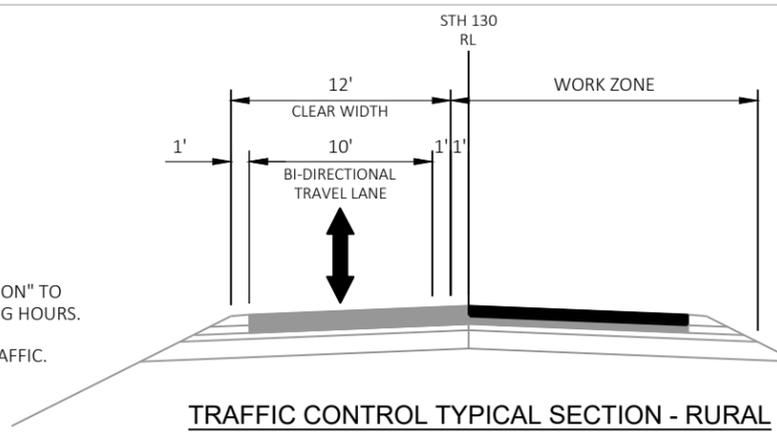
TRAFFIC CONTROL SIGNS PCMS MESSAGES		
PCMS SIGN LOCATION	PHASE 1	PHASE 2
STH 130 NB 500FT SOUTH OF PORTER RD INTERSECTION	ROAD WORK BEGINS	(DATE)
STH 130 SB 500 FT SOUTH OF STH 130/USH 14 INTERSECTION	ROAD WORK BEGINS	(DATE)

- TRAFFIC CONTROL GENERAL NOTES**
1. ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.
 2. "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
 3. PLACE TRAFFIC CONTROL SIGNS PCMS. DISPLAY THE MESSAGE 7 DAYS PRIOR TO THE EXPECTED START OF THE PROPOSED WORK. ADJUST THE MESSAGE DATE ACCORDINGLY.

- PCMS** TRAFFIC CONTROL SIGNS PCMS
1. PLACE TRAFFIC CONTROL SIGNS PER "TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL" IN SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC".
 2. PLACE TRAFFIC CONTROL SIGNS PER "TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL" IN SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC".
 3. PLACE TRAFFIC CONTROL SIGNS PER "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 MPH OR LESS" IN SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC".

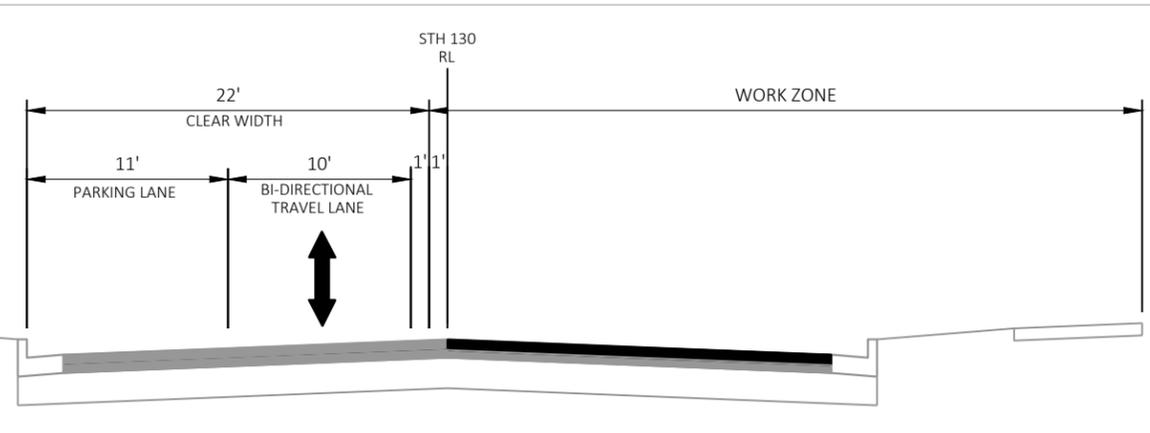
LEGEND
 PAVEMENT CONSTRUCTION AREA

NOTES
 SEE SDD "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION" TO REDUCE TRAFFIC TO ONE LANE BI-DIRECTIONAL TRAFFIC DURING WORKING HOURS.
 WORK ZONE SHOWN INCLUDES WORKING ROOM AND BUFFER TO LIVE TRAFFIC.
 SAME DAY PAVING IS REQUIRED ON ALL MILLED SURFACES.



TRAFFIC CONTROL TYPICAL SECTION - RURAL

STH 130
 TYPICAL STAGING FOR MILL AND OVERLAY OPERATIONS
 (WORK ZONE FOR STH 130 NB SHOWN, MIRROR TYPICAL SECTION FOR STH 130 SB WORK ZONE)



TRAFFIC CONTROL TYPICAL SECTION - URBAN

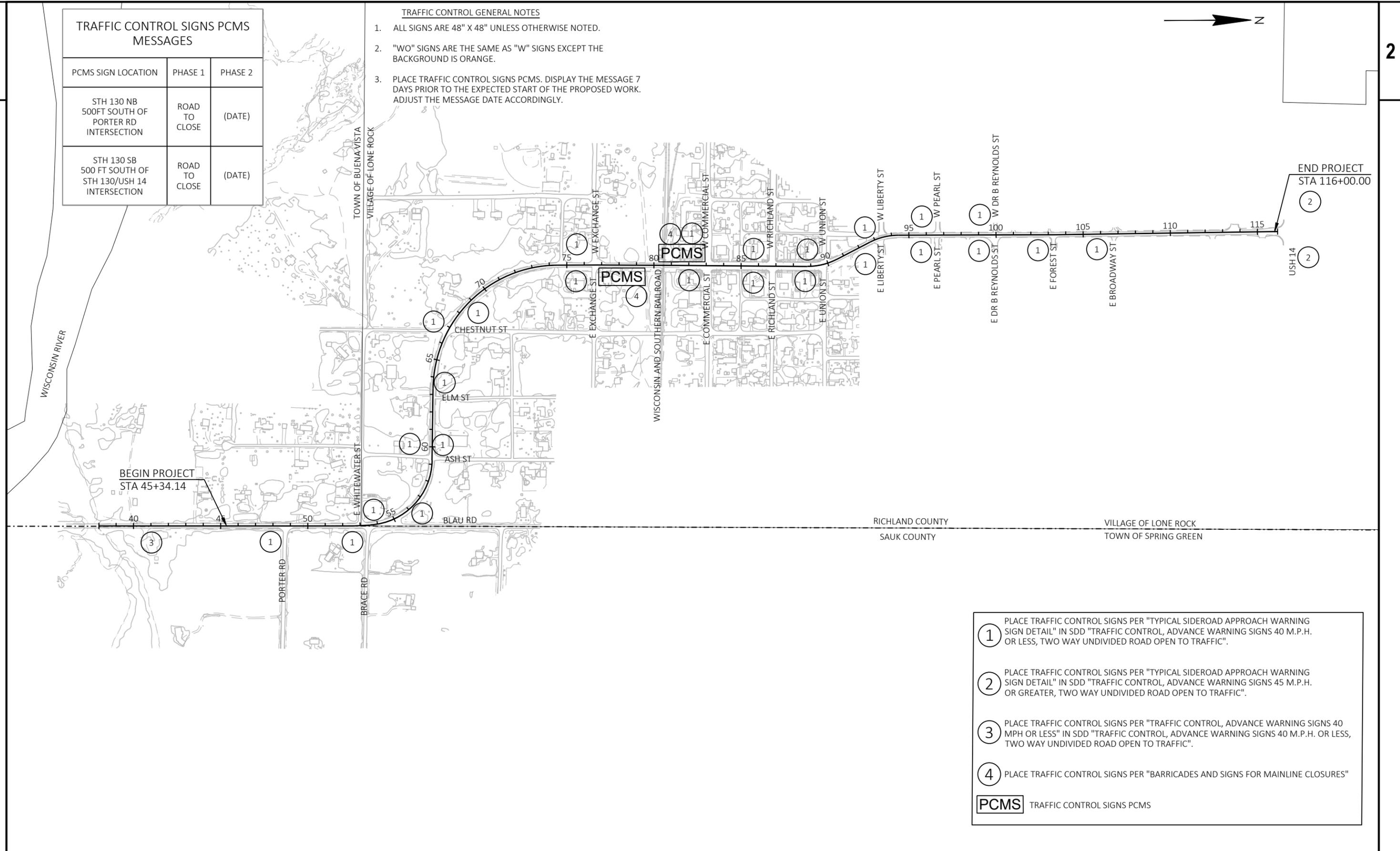
STH 130
 TYPICAL STAGING FOR MILL AND OVERLAY OPERATIONS
 (WORK ZONE FOR STH 130 NB SHOWN, MIRROR TYPICAL SECTION FOR STH 130 SB WORK ZONE)

TRAFFIC CONTROL SIGNS PCMS MESSAGES

PCMS SIGN LOCATION	PHASE 1	PHASE 2
STH 130 NB 500FT SOUTH OF PORTER RD INTERSECTION	ROAD TO CLOSE	(DATE)
STH 130 SB 500 FT SOUTH OF STH 130/USH 14 INTERSECTION	ROAD TO CLOSE	(DATE)

TRAFFIC CONTROL GENERAL NOTES

1. ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.
2. "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
3. PLACE TRAFFIC CONTROL SIGNS PCMS. DISPLAY THE MESSAGE 7 DAYS PRIOR TO THE EXPECTED START OF THE PROPOSED WORK. ADJUST THE MESSAGE DATE ACCORDINGLY.



① PLACE TRAFFIC CONTROL SIGNS PER "TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL" IN SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC".

② PLACE TRAFFIC CONTROL SIGNS PER "TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL" IN SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC".

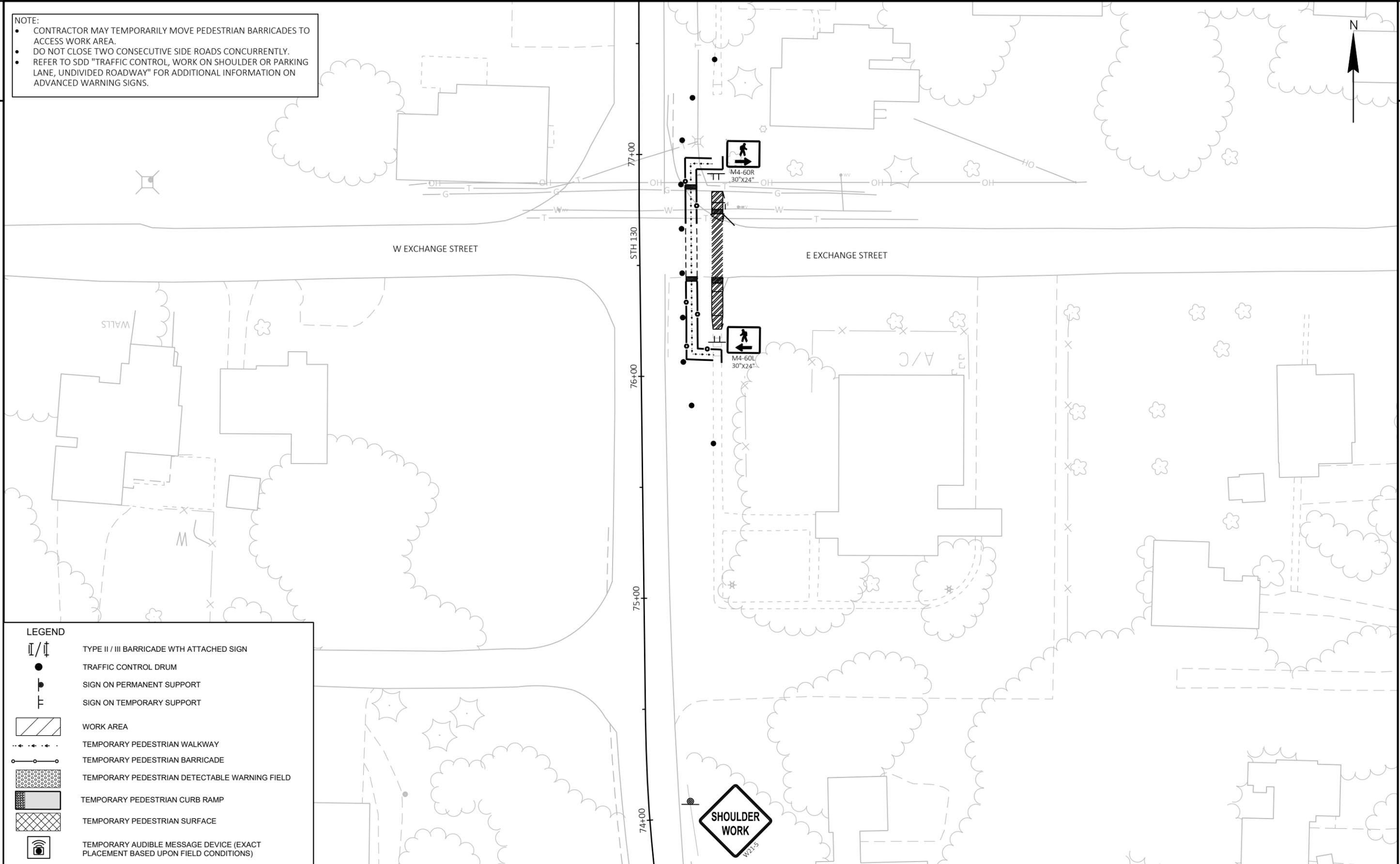
③ PLACE TRAFFIC CONTROL SIGNS PER "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 MPH OR LESS" IN SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC".

④ PLACE TRAFFIC CONTROL SIGNS PER "BARRICADES AND SIGNS FOR MAINLINE CLOSURES"

PCMS TRAFFIC CONTROL SIGNS PCMS

NOTE:

- CONTRACTOR MAY TEMPORARILY MOVE PEDESTRIAN BARRICADES TO ACCESS WORK AREA.
- DO NOT CLOSE TWO CONSECUTIVE SIDE ROADS CONCURRENTLY.
- REFER TO SDD "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY" FOR ADDITIONAL INFORMATION ON ADVANCED WARNING SIGNS.

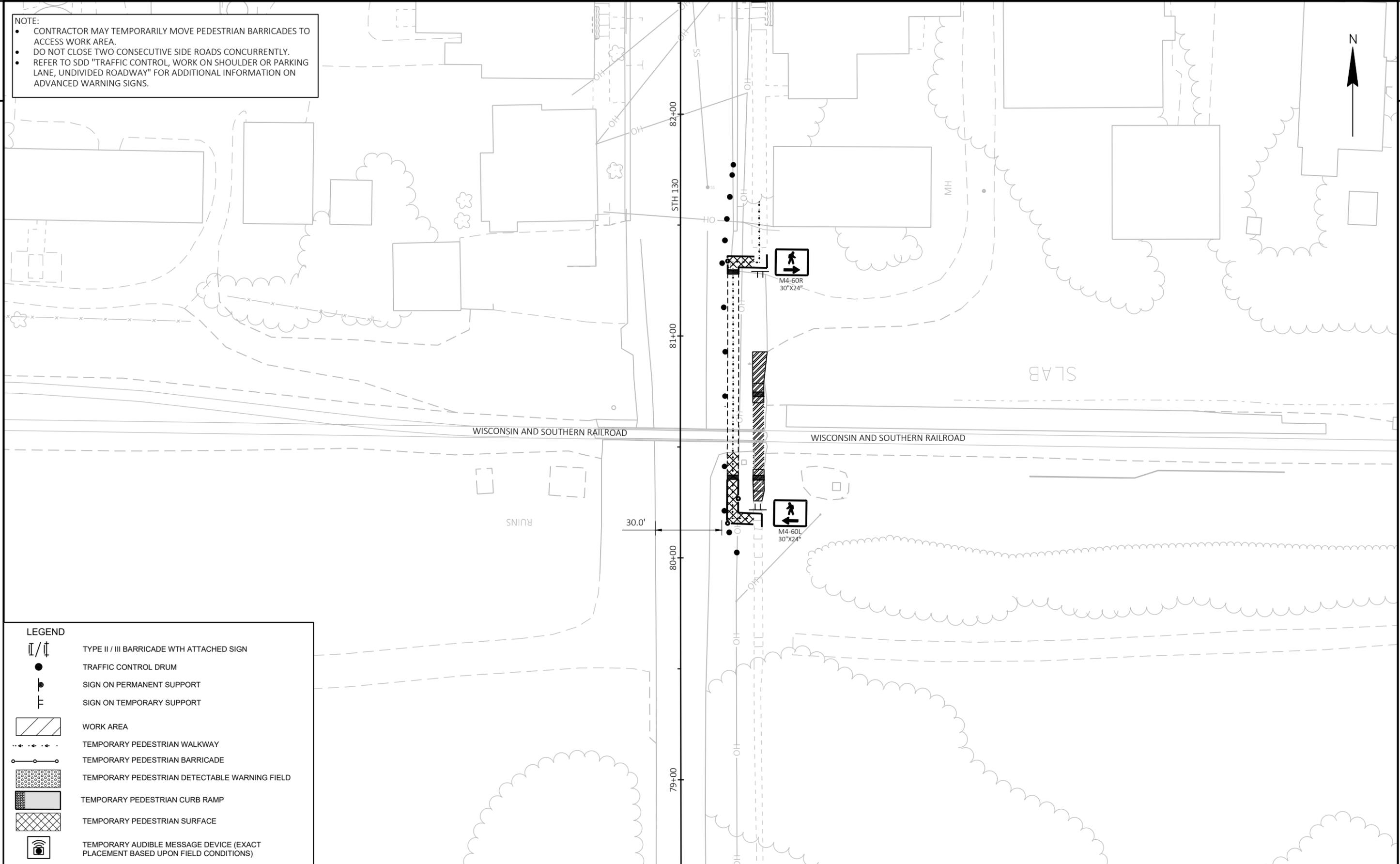


LEGEND

- TYPE II / III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- WORK AREA
- TEMPORARY PEDESTRIAN WALKWAY
- TEMPORARY PEDESTRIAN BARRICADE
- TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE
- TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)

NOTE:

- CONTRACTOR MAY TEMPORARILY MOVE PEDESTRIAN BARRICADES TO ACCESS WORK AREA.
- DO NOT CLOSE TWO CONSECUTIVE SIDE ROADS CONCURRENTLY.
- REFER TO SDD "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY" FOR ADDITIONAL INFORMATION ON ADVANCED WARNING SIGNS.

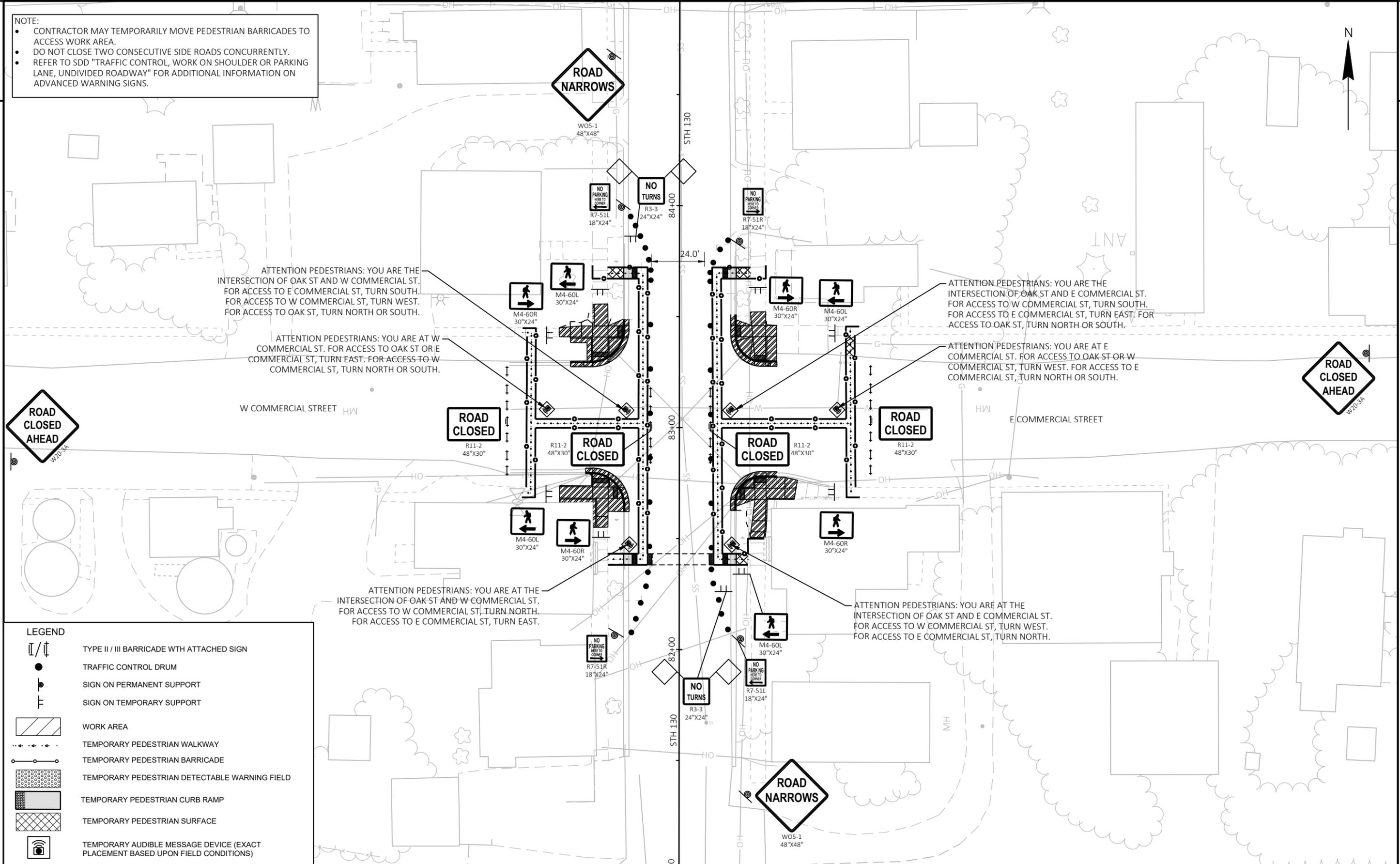


LEGEND

-  TYPE II / III BARRICADE WITH ATTACHED SIGN
-  TRAFFIC CONTROL DRUM
-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  WORK AREA
-  TEMPORARY PEDESTRIAN WALKWAY
-  TEMPORARY PEDESTRIAN BARRICADE
-  TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE
-  TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)

NOTE:

- CONTRACTOR MAY TEMPORARILY MOVE PEDESTRIAN BARRICADES TO ACCESS WORK AREA.
- DO NOT CLOSE TWO CONSECUTIVE SIDE ROADS CONCURRENTLY.
- REFER TO SDD "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY" FOR ADDITIONAL INFORMATION ON ADVANCED WARNING SIGNS.



ATTENTION PEDESTRIANS: YOU ARE THE INTERSECTION OF OAK ST AND W COMMERCIAL ST. FOR ACCESS TO E COMMERCIAL ST, TURN SOUTH. FOR ACCESS TO W COMMERCIAL ST, TURN WEST. FOR ACCESS TO OAK ST, TURN NORTH OR SOUTH.

ATTENTION PEDESTRIANS: YOU ARE AT W COMMERCIAL ST. FOR ACCESS TO OAK ST OR E COMMERCIAL ST, TURN EAST. FOR ACCESS TO W COMMERCIAL ST, TURN NORTH OR SOUTH.

ATTENTION PEDESTRIANS: YOU ARE THE INTERSECTION OF OAK ST AND E COMMERCIAL ST. FOR ACCESS TO W COMMERCIAL ST, TURN SOUTH. FOR ACCESS TO E COMMERCIAL ST, TURN EAST. FOR ACCESS TO OAK ST, TURN NORTH OR SOUTH.

ATTENTION PEDESTRIANS: YOU ARE AT E COMMERCIAL ST. FOR ACCESS TO OAK ST OR W COMMERCIAL ST, TURN WEST. FOR ACCESS TO E COMMERCIAL ST, TURN NORTH OR SOUTH.

ATTENTION PEDESTRIANS: YOU ARE AT THE INTERSECTION OF OAK ST AND W COMMERCIAL ST. FOR ACCESS TO W COMMERCIAL ST, TURN NORTH. FOR ACCESS TO E COMMERCIAL ST, TURN EAST.

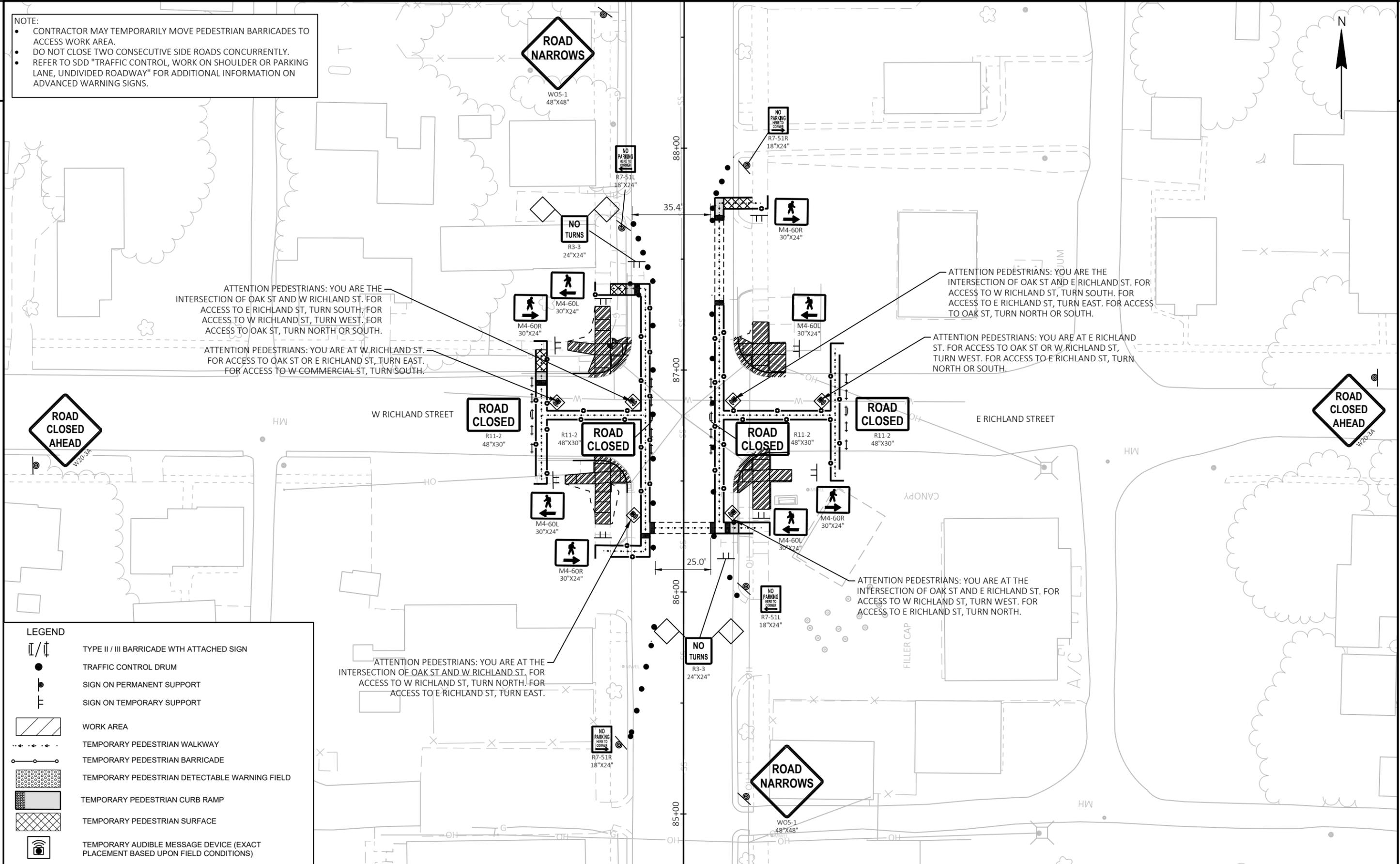
ATTENTION PEDESTRIANS: YOU ARE AT THE INTERSECTION OF OAK ST AND E COMMERCIAL ST. FOR ACCESS TO W COMMERCIAL ST, TURN WEST. FOR ACCESS TO E COMMERCIAL ST, TURN NORTH.

LEGEND

- TYPE II / III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- WORK AREA
- TEMPORARY PEDESTRIAN WALKWAY
- TEMPORARY PEDESTRIAN BARRICADE
- TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE
- TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)

NOTE:

- CONTRACTOR MAY TEMPORARILY MOVE PEDESTRIAN BARRICADES TO ACCESS WORK AREA.
- DO NOT CLOSE TWO CONSECUTIVE SIDE ROADS CONCURRENTLY.
- REFER TO SDD "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY" FOR ADDITIONAL INFORMATION ON ADVANCED WARNING SIGNS.

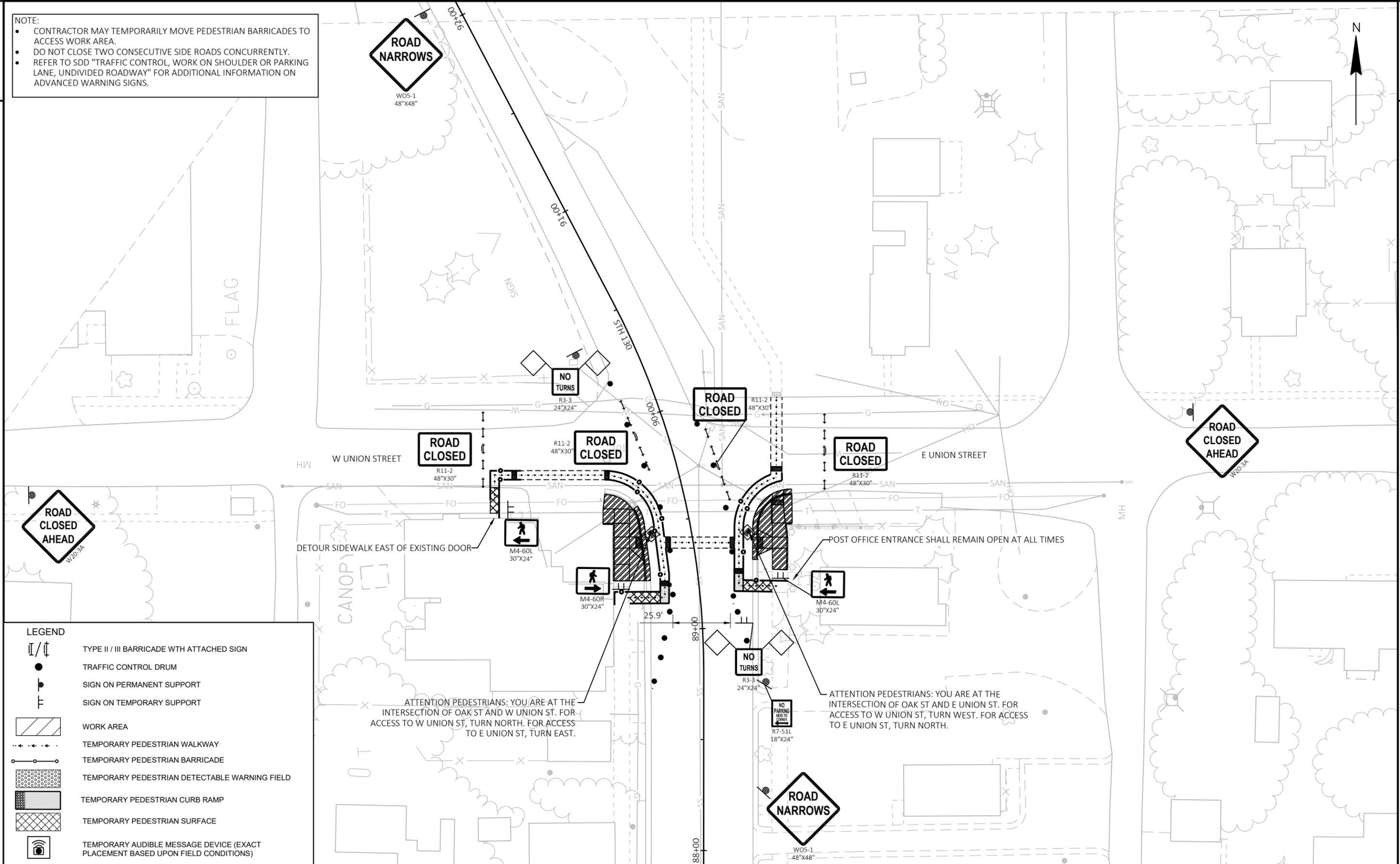


LEGEND

- TYPE II / III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- WORK AREA
- TEMPORARY PEDESTRIAN WALKWAY
- TEMPORARY PEDESTRIAN BARRICADE
- TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE
- TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)

NOTE:

- CONTRACTOR MAY TEMPORARILY MOVE PEDESTRIAN BARRICADES TO ACCESS WORK AREA.
- DO NOT CLOSE TWO CONSECUTIVE SIDE ROADS CONCURRENTLY.
- REFER TO SDD "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY" FOR ADDITIONAL INFORMATION ON ADVANCED WARNING SIGNS.



LEGEND

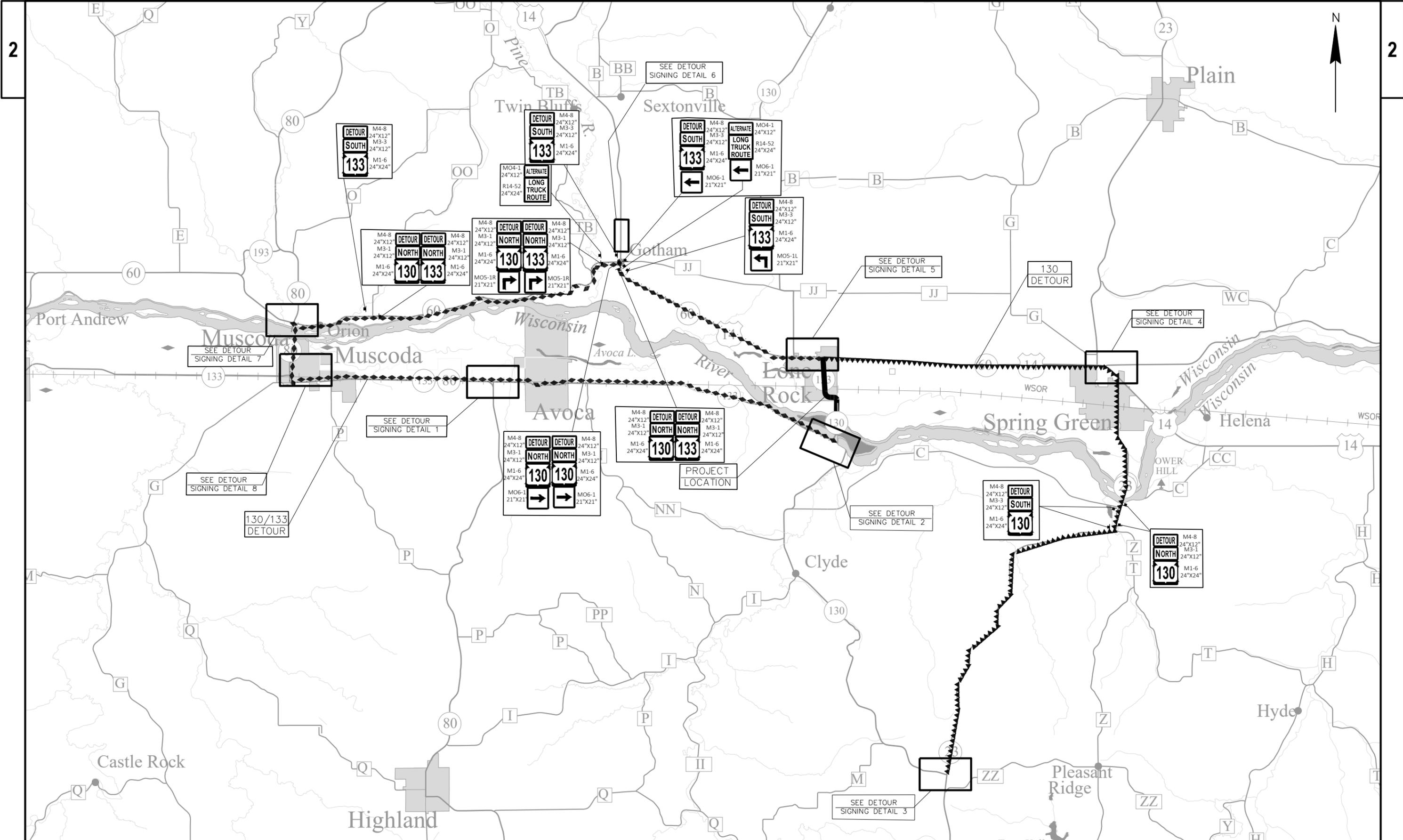
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- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- WORK AREA
- TEMPORARY PEDESTRIAN WALKWAY
- TEMPORARY PEDESTRIAN BARRICADE
- TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE
- TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)

ATTENTION PEDESTRIANS: YOU ARE AT THE INTERSECTION OF OAK ST AND W UNION ST. FOR ACCESS TO W UNION ST, TURN NORTH. FOR ACCESS TO E UNION ST, TURN EAST.

ATTENTION PEDESTRIANS: YOU ARE AT THE INTERSECTION OF OAK ST AND E UNION ST. FOR ACCESS TO W UNION ST, TURN WEST. FOR ACCESS TO E UNION ST, TURN NORTH.

POST OFFICE ENTRANCE SHALL REMAIN OPEN AT ALL TIMES

DETOUR SIDEWALK EAST OF EXISTING DOOR



PROJECT NO: 5770-02-71

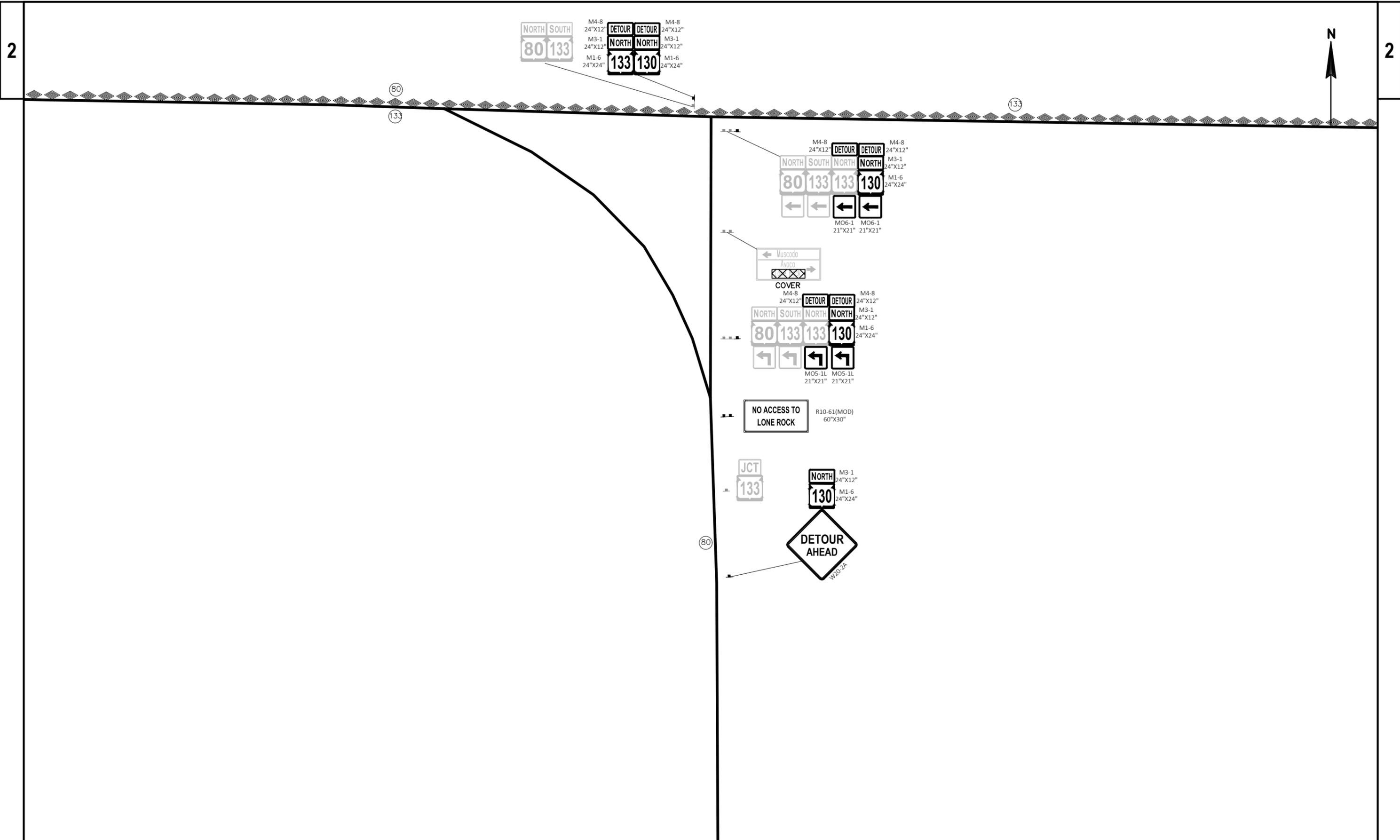
HWY: STH 130

COUNTY: RICHLAND

DETOUR OVERVIEW

SHEET

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PROJECT NO: 5770-02-71

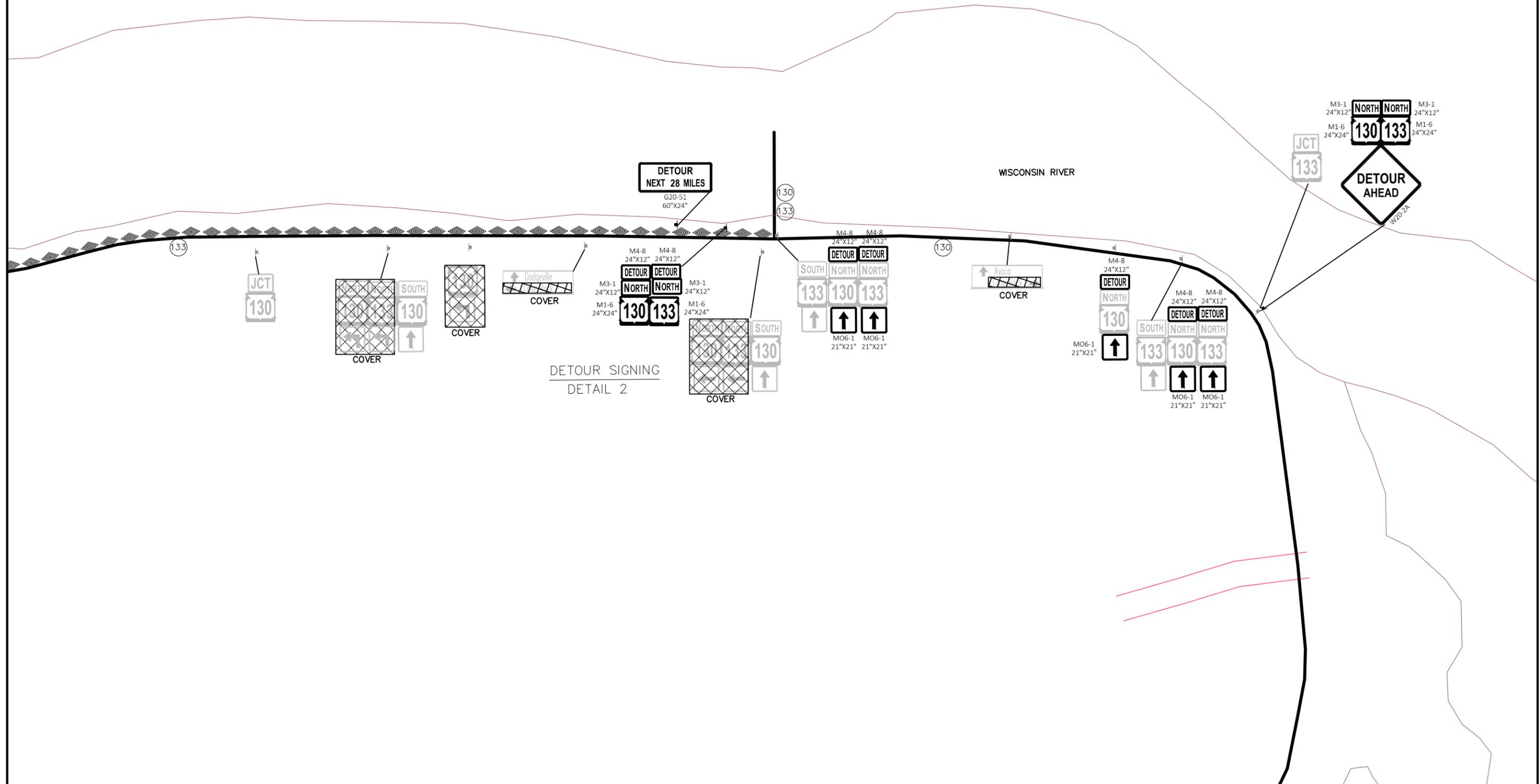
HWY: STH 130

COUNTY: RICHLAND

DETOUR SIGNING DETAIL 1

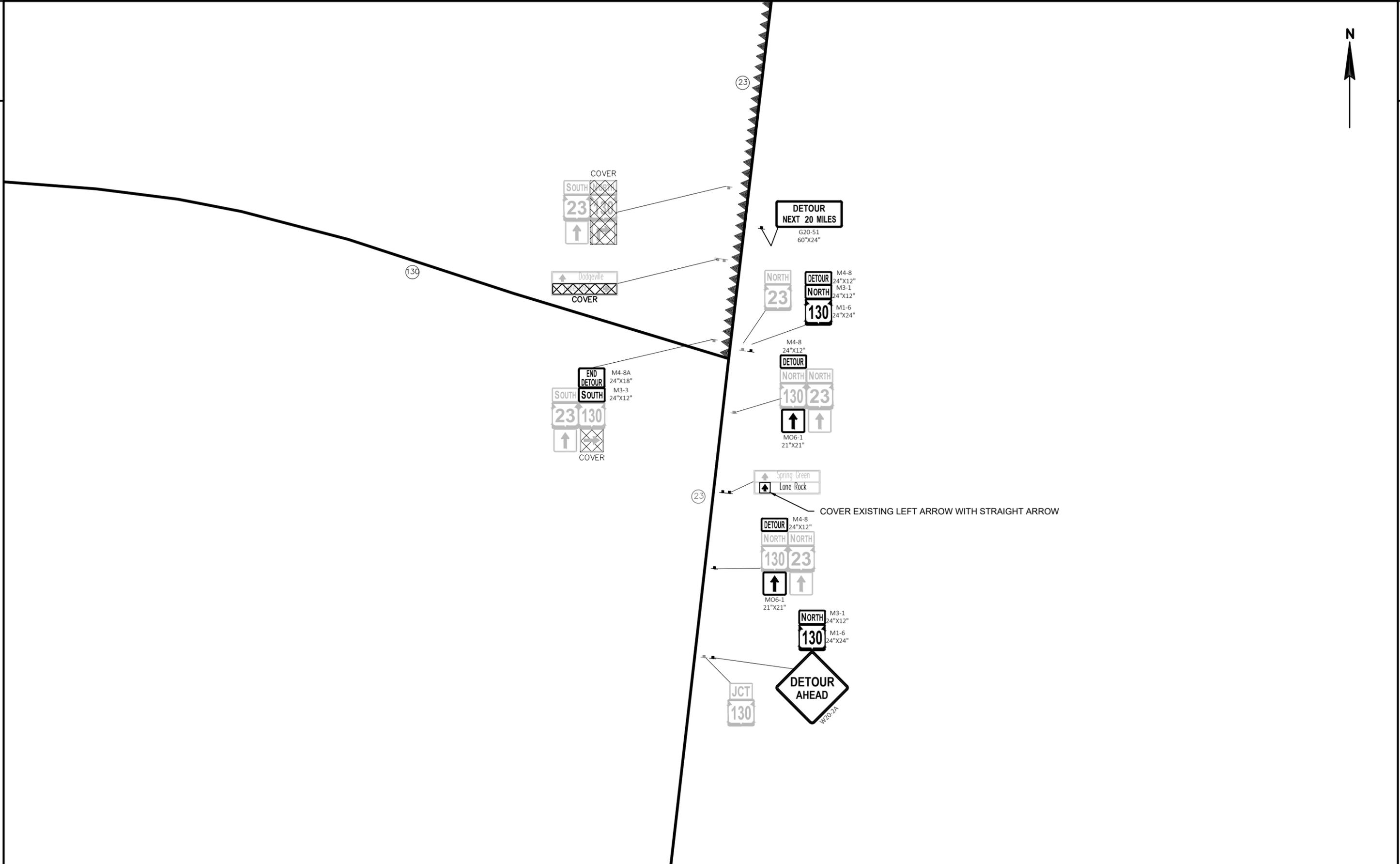
SHEET

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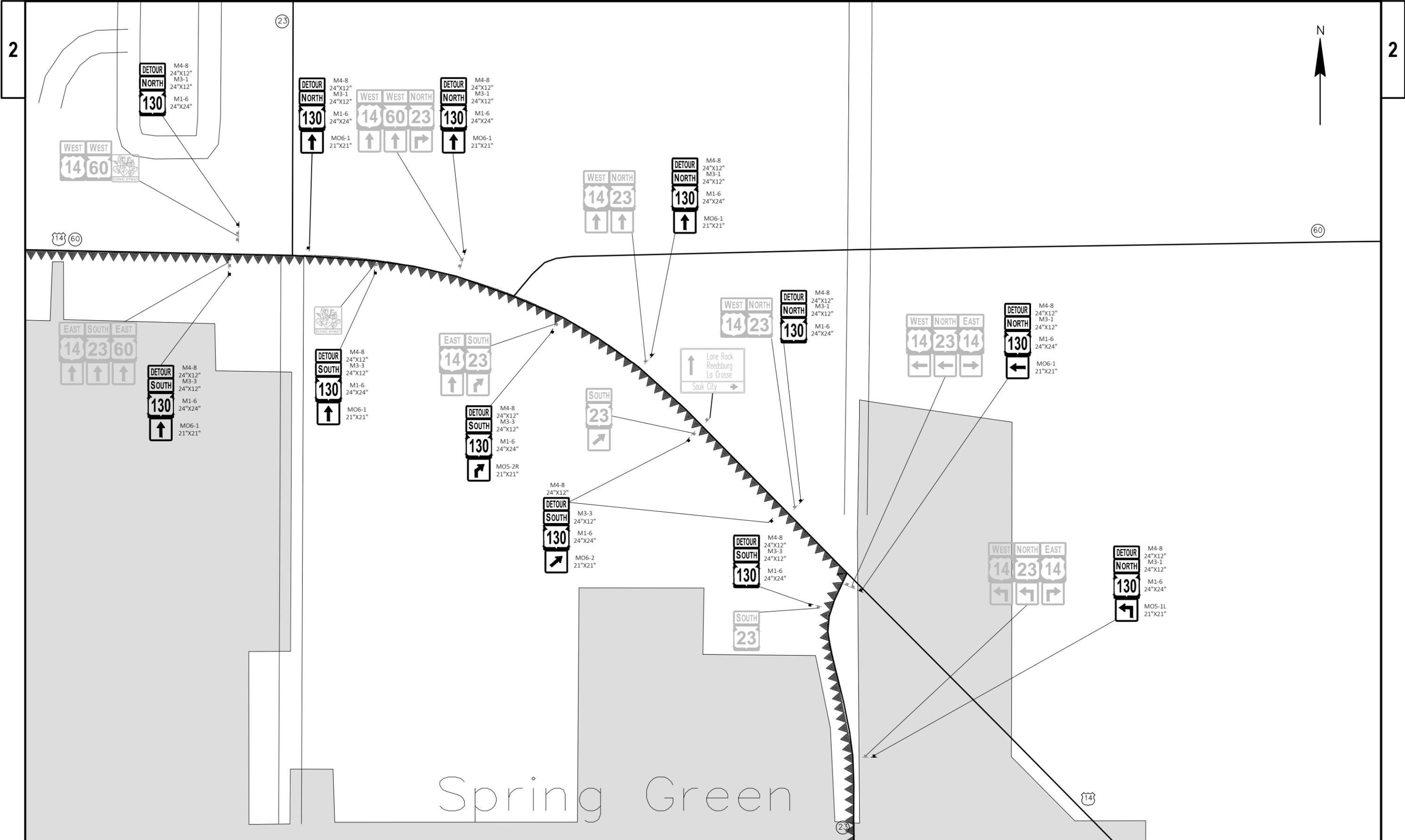


DETOUR SIGNING
DETAIL 2

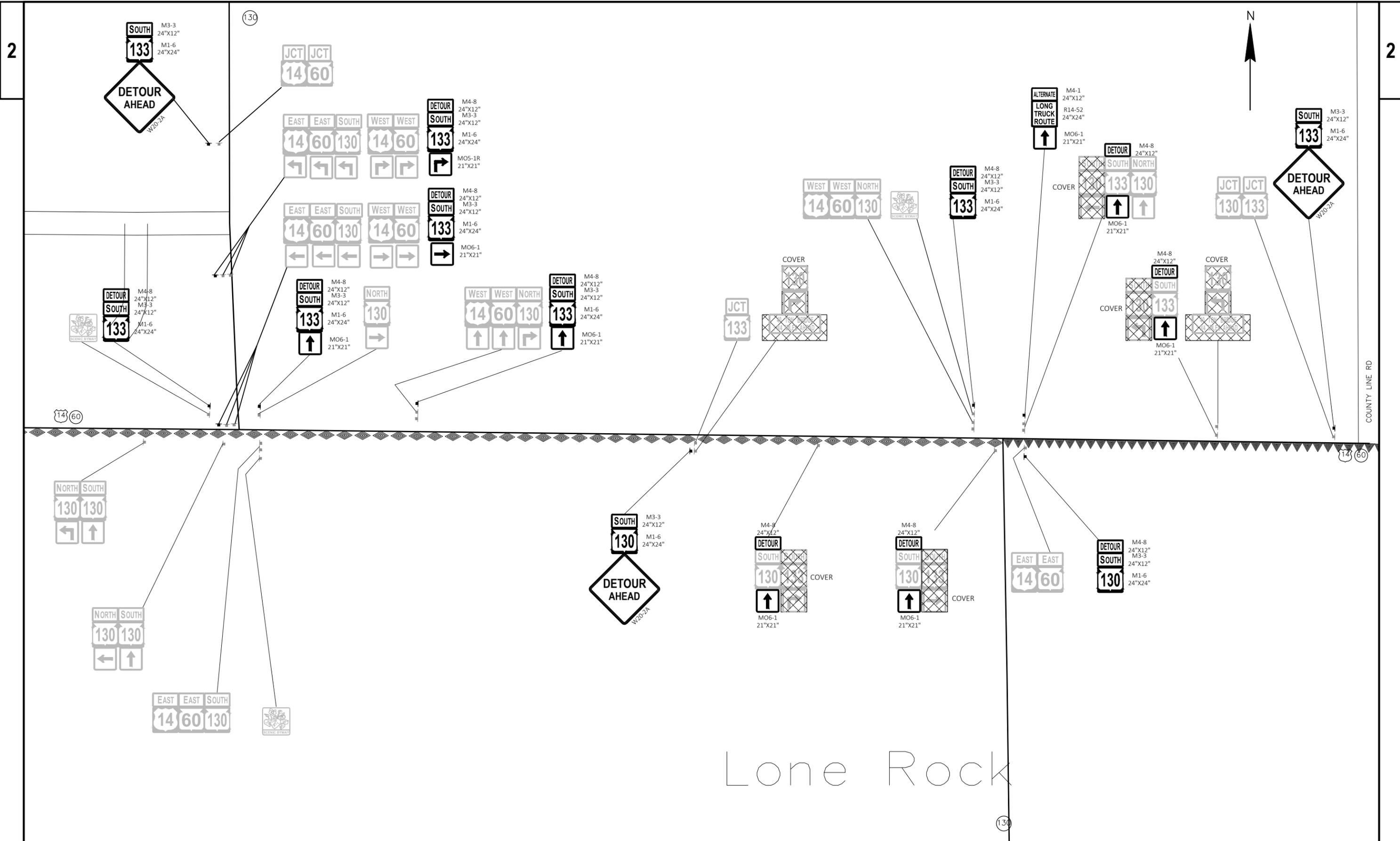
PROJECT NO: 5770-02-71	HWY: STH 130	COUNTY: RICHLAND	DETOUR SIGNING DETAIL 2	SHEET	E
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PROJECT NO: 5770-02-71	HWY: STH 130	COUNTY: RICHLAND	DETOUR SIGNING DETAIL 3	SHEET	E
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PROJECT NO: 5770-02-71	HWY: STH 130	COUNTY: RICHLAND	DETOUR SIGNING DETAIL 4	SHEET	E
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PROJECT NO: 5770-02-71

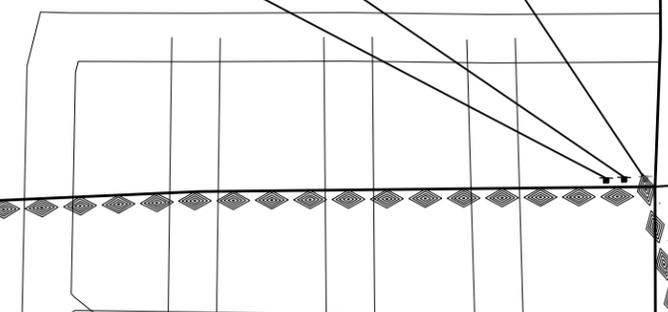
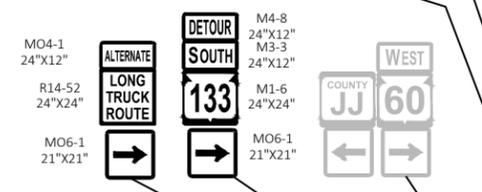
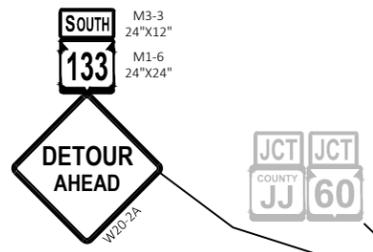
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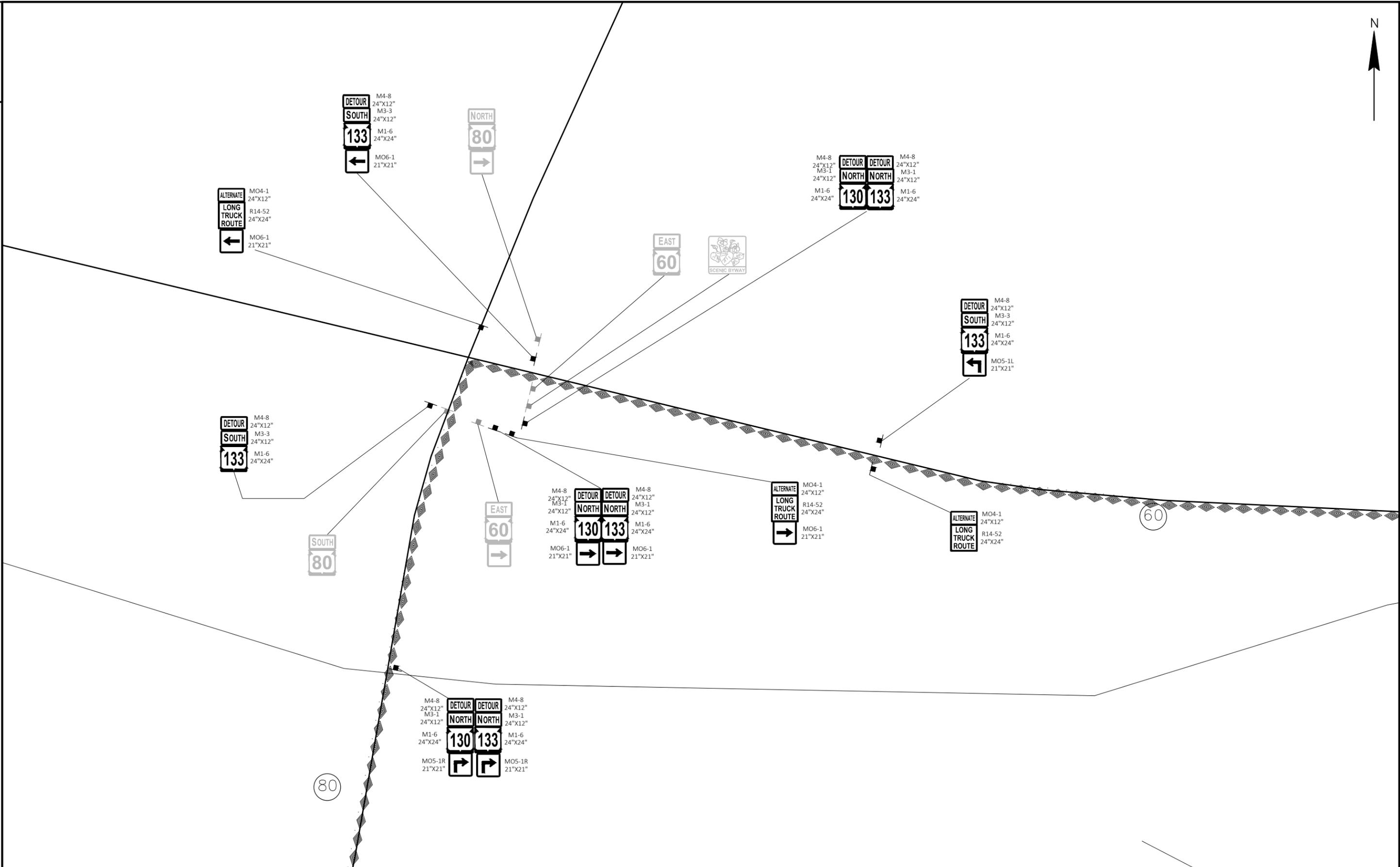
COUNTY: RICHLAND

DETOUR SIGNING DETAIL 5

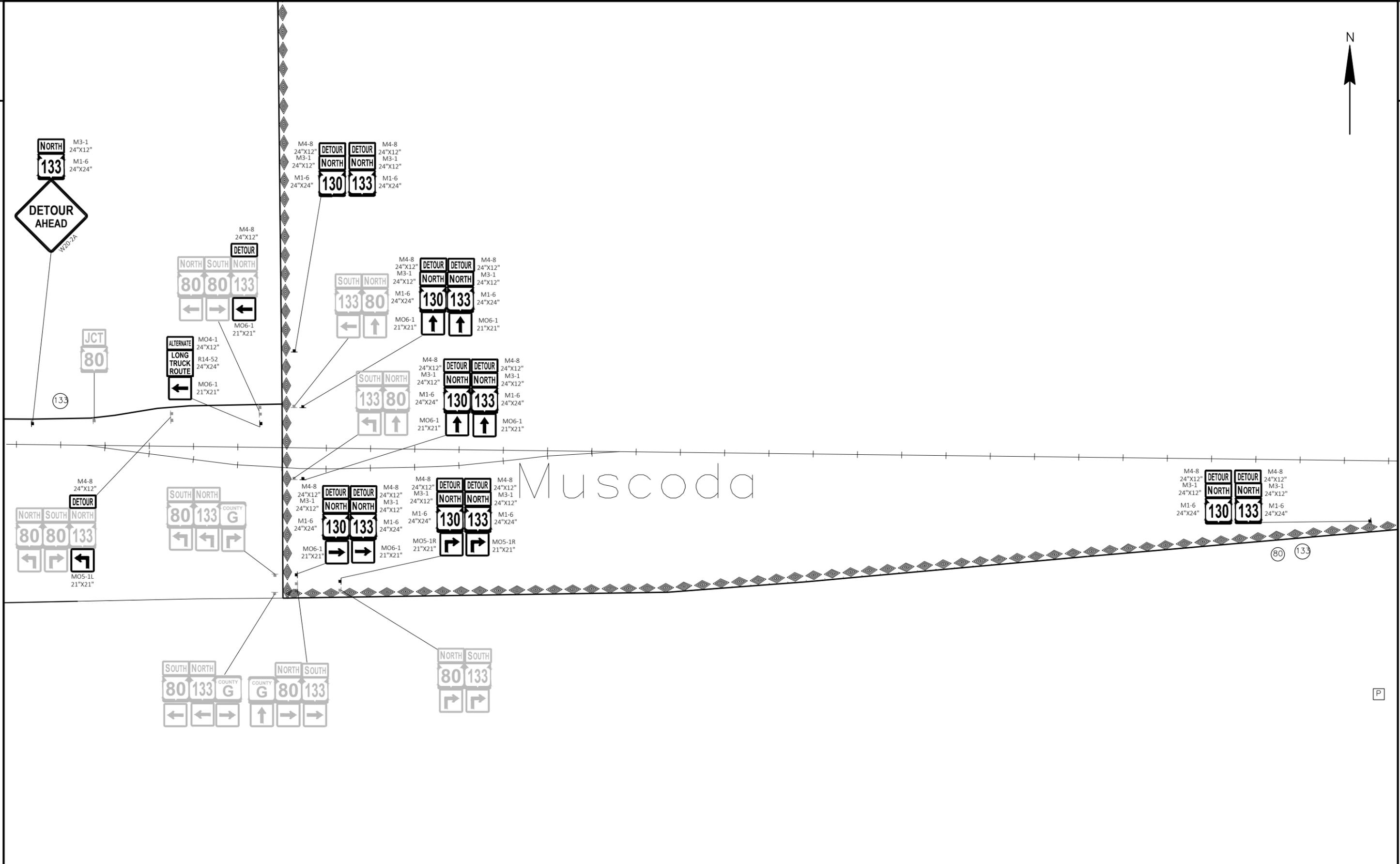
SHEET

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PROJECT NO: 5770-02-71	HWY: STH 130	COUNTY: RICHLAND	DETOUR SIGNING DETAIL 7	SHEET	E
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PROJECT NO: 5770-02-71	HWY: STH 130	COUNTY: RICHLAND	DETOUR SIGNING DETAIL 8	SHEET	E
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Estimate Of Quantities

5770-02-71

Line	Item	Item Description	Unit	Total	Qty
0002	204.0110	Removing Asphaltic Surface	SY	154.000	154.000
0004	204.0115	Removing Asphaltic Surface Butt Joints	SY	635.000	635.000
0006	204.0120	Removing Asphaltic Surface Milling	SY	23,870.000	23,870.000
0008	204.0150	Removing Curb & Gutter	LF	337.000	337.000
0010	204.0155	Removing Concrete Sidewalk	SY	413.000	413.000
0012	205.0100	Excavation Common	CY	11.000	11.000
0014	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 5770-02-71	EACH	1.000	1.000
0016	213.0100	Finishing Roadway (project) 01. 5770-02-71	EACH	1.000	1.000
0018	305.0110	Base Aggregate Dense 3/4-Inch	TON	148.000	148.000
0020	455.0605	Tack Coat	GAL	1,716.000	1,716.000
0022	460.2000	Incentive Density HMA Pavement	DOL	3,080.000	3,080.000
0024	460.5224	HMA Pavement 4 LT 58-28 S	TON	4,802.000	4,802.000
0026	465.0105	Asphaltic Surface	TON	140.000	140.000
0028	465.0110	Asphaltic Surface Patching	TON	25.000	25.000
0030	465.0125	Asphaltic Surface Temporary	TON	1.000	1.000
0032	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	367.000	367.000
0034	601.0600	Concrete Curb Pedestrian	LF	331.000	331.000
0036	602.0405	Concrete Sidewalk 4-Inch	SF	2,936.000	2,936.000
0038	602.0415	Concrete Sidewalk 6-Inch	SF	396.000	396.000
0040	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	254.000	254.000
0042	611.8110	Adjusting Manhole Covers	EACH	4.000	4.000
0044	611.8115	Adjusting Inlet Covers	EACH	10.000	10.000
0046	618.0100	Maintenance and Repair of Haul Roads (project) 01. 5770-02-71	EACH	1.000	1.000
0048	619.1000	Mobilization	EACH	1.000	1.000
0050	624.0100	Water	MGAL	3.000	3.000
0052	625.0100	Topsoil	SY	285.000	285.000
0054	627.0200	Mulching	SY	285.000	285.000
0056	628.1504	Silt Fence	LF	103.000	103.000
0058	628.1905	Mobilizations Erosion Control	EACH	3.000	3.000
0060	628.1910	Mobilizations Emergency Erosion Control	EACH	3.000	3.000
0062	628.7015	Inlet Protection Type C	EACH	11.000	11.000
0064	629.0205	Fertilizer Type A	CWT	0.200	0.200
0066	630.0140	Seeding Mixture No. 40	LB	2.000	2.000
0068	630.0500	Seed Water	MGAL	7.000	7.000
0070	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	12.000	12.000
0072	637.2210	Signs Type II Reflective H	SF	21.000	21.000
0074	638.2102	Moving Signs Type II	EACH	4.000	4.000
0076	638.2602	Removing Signs Type II	EACH	2.000	2.000
0078	642.5201	Field Office Type C	EACH	1.000	1.000
0080	643.0300	Traffic Control Drums	DAY	809.000	809.000
0082	643.0420	Traffic Control Barricades Type III	DAY	885.000	885.000
0084	643.0705	Traffic Control Warning Lights Type A	DAY	560.000	560.000
0086	643.0900	Traffic Control Signs	DAY	4,367.000	4,367.000
0088	643.0920	Traffic Control Covering Signs Type II	EACH	41.000	41.000
0090	643.1050	Traffic Control Signs PCMS	DAY	28.000	28.000
0092	643.3165	Temporary Marking Line Paint 6-Inch	LF	24,474.000	24,474.000
0094	643.5000	Traffic Control	EACH	1.000	1.000
0096	644.1430	Temporary Pedestrian Surface Plate	SF	800.000	800.000
0098	644.1601	Temporary Pedestrian Curb Ramp	DAY	77.000	77.000

Estimate Of Quantities

5770-02-71

Line	Item	Item Description	Unit	Total	Qty
0100	644.1605	Temporary Pedestrian Detectable Warning Field	SF	165.000	165.000
0102	644.1810	Temporary Pedestrian Barricade	LF	3,170.000	3,170.000
0104	644.1900.S	Temporary Audible Message Devices	DAY	108.000	108.000
0106	646.2040	Marking Line Grooved Wet Ref Epoxy 6-Inch	LF	24,729.000	24,729.000
0108	646.5220	Marking Symbol Epoxy	EACH	2.000	2.000
0110	646.5320	Marking Railroad Crossing Epoxy	EACH	2.000	2.000
0112	646.6120	Marking Stop Line Epoxy 18-Inch	LF	22.000	22.000
0114	646.7120	Marking Diagonal Epoxy 12-Inch	LF	88.000	88.000
0116	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	920.000	920.000
0118	646.8320	Marking Parking Stall Epoxy	LF	498.000	498.000
0120	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	698.000	698.000
0122	650.8000	Construction Staking Resurfacing Reference	LF	7,066.000	7,066.000
0124	650.9000	Construction Staking Curb Ramps	EACH	14.000	14.000
0126	650.9500	Construction Staking Sidewalk (project) 01. 5770-02-71	EACH	1.000	1.000
0128	650.9911	Construction Staking Supplemental Control (project) 01. 5770-02-71	EACH	1.000	1.000
0130	680.0100	Public Land Reference Monument Verify and Reset	EACH	1.000	1.000
0132	690.0150	Sawing Asphalt	LF	606.000	606.000
0134	690.0250	Sawing Concrete	LF	84.000	84.000
0136	740.0440	Incentive IRI Ride	DOL	2,677.000	2,677.000
0138	SPV.0060	Special 01. Adjusting Sanitary Manhole Covers	EACH	1.000	1.000
0140	SPV.0060	Special 02. Adjusting Water Valve Boxes	EACH	1.000	1.000
0142	SPV.0180	Special 01. Removing Distressed Pavement Milling	SY	698.000	698.000

REMOVAL ITEMS

Table with columns: CATEGORY, LOCATION, STATION, TO STATION, ALIGNMENT, SY, SY, SY. Includes sub-headers 204.0115, 204.0120, SPV.0180.01. Rows include 0010 and 0020 categories with various stationing and alignment details.

REMOVAL ITEMS - SIDEWALK

Table with columns: CATEGORY, LOCATION, STATION, TO STATION, ALIGNMENT, SY, LF, SY, CY. Includes sub-headers 204.0110, 204.0150, 204.0155, 205.0100. Rows include 0010 and 0020 categories with various stationing and alignment details.

BASE AGGREGATE ITEMS

Table with columns: CATEGORY, LOCATION, STATION TO STATION, ALIGNMENT, TON, COMMENTS. Includes sub-headers 305.0110, BASE AGGREGATE DENSE 3/4-INCH. Rows include 0010 category with various stationing and alignment details.

ASPHALTIC ITEMS

Table with columns: CATEGORY, LOCATION, STATION, TO STATION, ALIGNMENT, EACH, GAL, TON, TON, TON, TON. Includes sub-headers 211.0101, 455.0605, 460.5224, 465.0105, 465.0110, 465.0125. Rows include 0010 and 0020 categories with various stationing and alignment details.

CONCRETE CURB AND GUTTER

Table with columns: CATEGORY, LOCATION, STATION, TO STATION, ALIGNMENT, LF, LF. Includes sub-headers 601.0411, 601.0600, CONCRETE CURB & GUTTER 30-INCH TYPE D, CONCRETE CURB PEDESTRIAN. Rows include 0010 category with various stationing and alignment details.

CONCRETE SIDEWALK

Table with columns: CATEGORY, LOCATION, STATION, TO STATION, ALIGNMENT, SF, SF. Includes sub-headers 602.0405, 602.0415, CONCRETE SIDEWALK 4-INCH, CONCRETE SIDEWALK 6-INCH. Rows include 0010 category with various stationing and alignment details.

CURB RAMP DETECTABLE WARNING FIELDS

CATEGORY	LOCATION	STATION	TO STATION	ALIGNMENT	602.0505 CURB RAMP DETECTABLE WARNING FIELD YELLOW SF
0010	SE EXCHANGE ST	76+20	- 76+45	STH 130	10
	NE EXCHANGE ST	76+65	- 76+85	STH 130	10
	SE RAILROAD	80+30	- 80+50	STH 130	10
	NE RAILROAD	80+55	- 80+95	STH 130	10
	SW COMMERCIAL ST	82+55	- 82+85	STH 130	24
	SE COMMERCIAL ST	82+50	- 82+85	STH 130	20
	NW COMMERCIAL ST	83+25	- 83+55	STH 130	24
	NE COMMERCIAL ST	83+25	- 83+55	STH 130	20
	SW RICHLAND ST	86+30	- 86+55	STH 130	24
	SE RICHLAND ST	86+35	- 86+55	STH 130	24
	NW RICHLAND ST	86+95	- 87+30	STH 130	24
	NE RICHLAND ST	86+95	- 87+25	STH 130	24
	SW UNION ST	89+25	- 89+50	STH 130	10
	SE UNION ST	89+20	- 89+50	STH 130	20
CATEGORY TOTAL					254
PROJECT TOTAL					254

ADJUSTING MANHOLE AND INLET COVERS

CATEGORY	LOCATION	STATION TO	STATION	ALIGNMENT	611.8110 ADJUSTING MANHOLE COVERS EACH	611.8115 ADJUSTING INLET COVERS EACH	SPV.0060.01 ADJUSTING SANITARY MANHOLE COVERS EACH	SPV.0060.02 ADJUSTING WATER VALVE BOXES EACH
0010	BEGIN TO RAILROAD	45+34	- 80+52	STH 130	-	-	-	-
	RAILROAD TO UNION ST	80+52	- 89+95	STH 130	-	10	-	-
	UNION ST TO END	89+95	- 116+15	STH 130	-	-	-	-
CATEGORY TOTAL					--	10	--	--
0030	RAILROAD TO UNION ST	80+52	- 89+95	STH 130	4	-	1	1
CATEGORY TOTAL					4	--	1	1
PROJECT TOTAL					4	10	1	1

WATER

CATEGORY	LOCATION	624.0100 WATER MGAL
0010	PROJECT-BASE COMPACTION	3.0
	PROJECT-COMMON EXCAVATION	0.0
	PROJECT-BORROW	--
PROJECT TOTAL		3.0

RESTORATION

CATEGORY	LOCATION	STATION	TO STATION	ALIGNMENT	625.0100 TOPSOIL SY	627.0200 MULCHING SY	628.1504 SILT FENCE LF	629.0205 FERTILIZER TYPE A CWT	630.0140 SEEDING MIXTURE NO. 40 LB	630.0500 SEED WATER MGAL
0010	SE EXCHANGE ST	76+20	- 76+45	STH 130	6	6	--	--	--	--
	NE EXCHANGE ST	76+65	- 76+85	STH 130	4	4	--	--	--	--
	SW RAILROAD	79+50	- 80+50	STH 130	125	125	94	0.08	0.9	3
	NE RAILROAD	80+55	- 80+95	STH 130	15	15	--	0.01	0.1	--
	SE COMMERCIAL ST	82+50	- 82+85	STH 130	7	7	--	--	0.1	--
	NW COMMERCIAL ST	83+25	- 83+55	STH 130	12	12	--	0.01	0.1	--
	NE COMMERCIAL ST	83+25	- 83+55	STH 130	11	11	--	0.01	0.1	--
	SW RICHLAND ST	86+30	- 86+55	STH 130	7	7	--	--	--	--
	SE RICHLAND ST	86+35	- 86+55	STH 130	9	9	--	0.01	0.1	--
	NW RICHLAND ST	86+95	- 87+30	STH 130	27	27	--	0.02	0.2	1
	NE RICHLAND ST	86+95	- 87+25	STH 130	4	4	--	--	--	--
	SW UNION ST	89+25	- 89+50	STH 130	16	16	--	0.01	0.1	--
	SE UNION ST	89+20	- 89+50	STH 130	16	16	--	0.01	0.1	--
	UNDISTRIBUTED			STH 130	26	26	9	0.04	0.2	3
CATEGORY TOTAL					285	285	103	0.20	2.0	7
PROJECT TOTAL					285	285	103	0.20	2.0	7

MOBILIZATION EROSION CONTROL

CATEGORY	LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH
0010	STH 130	3	3
CATEGORY TOTAL		3	3
PROJECT TOTAL		3	3

INLET PROTECTION

CATEGORY	LOCATION	STATION	TO STATION	ALIGNMENT	628.7015 INLET PROTECTION TYPE C EACH
0010	BEGIN TO RAILROAD	45+34	- 80+52	STH 130	--
	RAILROAD TO UNION ST	80+52	- 89+95	STH 130	11
	UNION ST TO END	89+95	- 116+15	STH 130	--
CATEGORY TOTAL					11
PROJECT TOTAL					11

CATEGORY	SIGN NUMBER	SIGN CODE	SIGN SIZE	SIGN TYPE	SIGN DIMENSION W X H IN X IN	DESCRIPTION	634.0614 POSTS WOOD 4X6-INCH X 14-FT EACH	637.2210 SIGNS TYPE II REFLECTIVE H SF	638.2102 MOVING SIGNS TYPE II EACH	638.2602 REMOVING SIGNS TYPE II EACH	SIGN MOUNTED ON SAME POST AS SIGN NO.	COMMENTS
0010	201	W10-1				RAILROAD CROSSING	1	--	1	--	--	USE EXISTING SIGN
	202	R7-1L	2S	II	18 X 24	NO PARKING ANY TIME (LEFT ARROW)	1	3.0	--	--	--	
	203	R7-1R	2S	II	18 X 24	NO PARKING ANY TIME (RIGHT ARROW)	1	3.0	--	--	--	
	204	R7-1D	2S	II	18 X 24	NO PARKING ANY TIME (DOUBLE ARROWS)	1	3.0	--	--	--	
	205	R7-1L	2S	II	18 X 24	NO PARKING ANY TIME (LEFT ARROW)	1	3.0	--	--	--	
	206	R7-1D	2S	II	18 X 24	NO PARKING ANY TIME (DOUBLE ARROWS)	1	3.0	--	--	--	
	206-R	R7-1L				NO PARKING ANY TIME (LEFT ARROW)	--	--	--	1	--	
	207	R7-1R	2S	II	18 X 24	NO PARKING ANY TIME (RIGHT ARROW)	1	3.0	--	--	--	
	207-R	R7-1R				NO PARKING ANY TIME (RIGHT ARROW)	--	--	--	1	--	
	208	R7-8A	2S	II	12 X 18	RESERVED PARKING (DISABLED) THIS SPACE	1	1.5	--	--	--	
	209	R1-1				STOP	1	--	1	--	--	USE EXISTING SIGN
	210	R1-1				STOP	1	--	1	--	--	USE EXISTING SIGN
	211	R1-1				STOP	1	--	1	--	--	USE EXISTING SIGN
	212	R7-8A	2S	II	12 X 18	RESERVED PARKING (DISABLED) THIS SPACE	1	1.5	--	--	--	
CATEGORY TOTAL							12	21	4	2		
PROJECT TOTAL							12	21	4	2		

TYPE II SIGNS AND SUPPORTS

TRAFFIC CONTROL ITEMS - DETOUR

643.0900* 643.0920

CATEGORY	LOCATION	DURATION		CYCLES NO.	TRAFFIC CONTROL SIGNS		TRAFFIC CONTROL COVERING SIGNS TYPE II		COMMENTS
		DAY	NO.		NO.	DAY	NO.	EACH	
0010	STH 130	7	1	1	59	413	--	--	DETOUR STH 130-DETOUR
	STH 130	7	1	1	56	392	23	23	DETOUR STH 130-DETAIL 1, 2, AND 3
	STH 130	7	1	1	49	343	--	--	DETOUR STH 130-DETAIL 4
	STH 130	7	1	1	45	315	18	18	DETOUR STH 130-DETAIL 5
	STH 130	7	1	1	14	98	--	--	DETOUR STH 130-DETAIL 6
	STH 130	7	1	1	95	665	--	--	DETOUR STH 130-DETAIL 7 AND 8
	SUBTOTAL					2,226		41	

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

TRAFFIC CONTROL ITEMS - PEDESTRIAN

CATEGORY	STAGE	LOCATION	DURATION		TRAFFIC CONTROL DRUMS		TRAFFIC CONTROL BARRICADES TYPE III		TRAFFIC CONTROL SIGNS		TEMPORARY PEDESTRIAN SURFACE PLATE		TEMPORARY PEDESTRIAN CURB RAMP		TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD		TEMPORARY PEDESTRIAN BARRICADE		TEMPORARY AUDIBLE MESSAGE DEVICES	
			DAY	NO.	NO.	DAY	NO.	DAY	NO.	DAY	SF	NO.	DAY	SF	LF	NO.	DAY			
0010	1	EXCHANGE ST	7	1	10	70	--	--	3	21	--	--	--	--	20		179	--	--	
		RAILROAD	7	1	13	91	--	--	2	14	262	--	--	--	20		106	--	--	
		COMMERCIAL ST	7	1	34	238	24	168	22	154	131	4	28	20		1,170	6	42		
		RICHLAND ST	7	1	30	210	20	140	22	154	150	4	28	40		1,042	6	42		
		UNION ST	7	1	18	126	20	140	14	98	184	2	14	50		385	2	14		
		STAGE 1 SUBTOTAL				735		448		441	727		70	150		2,882		98		
		UNDISTRIBUTED				74		45		44	73		7	15		288		10		
		SUBTOTAL				809		493		485	800		77	165		3,170		108		

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

TRAFFIC CONTROL ITEMS

CATEGORY	LOCATION	DURATION		CYCLES NO.	TRAFFIC CONTROL BARRICADES TYPE III		TRAFFIC CONTROL WARNING LIGHTS TYPE A		TRAFFIC CONTROL SIGNS		TRAFFIC CONTROL SIGNS PCMS		TRAFFIC CONTROL		COMMENTS
		DAYS	NO.		NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY	EACH		
0010	BEGIN/END PROJECT	7	2	--	--	--	--	--	--	2	28	--	--		ADVANCE WARNING PCMS
	STH 130	28	1	14	392	20	560	15	420	--	--	--	--		TRAFFIC CONTROL STH 130 MAINLINE CLOSURE
	STH 130	16	1	--	--	--	--	3	48	--	--	--	--		TRAFFIC CONTROL STH 130 MILL AND OVERLAY
	SIDERoads	28	1	--	--	--	--	27	756	--	--	--	--		TRAFFIC CONTROL STH 130 MAINLINE CLOSURE
	SIDERoads	16	1	--	--	--	--	27	432	--	--	--	--		TRAFFIC CONTROL STH 130 MILL AND OVERLAY
	PROJECT 5770-02-71	--	--	--	--	--	--	--	--	--	--	--	--	1	
	SUBTOTAL				392		560		1,656		28		1		

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

LONG LINE PAVEMENT MARKING ITEMS

CATEGORY	LOCATION	STATION TO	STATION	ALIGNMENT	643.3165		646.2040				646.5320	646.6120	646.7120	646.7420
					TEMPORARY MARKING LINE PAINT 6-INCH		MARKING LINE GROOVED WET REF EPOXY 6-INCH				MARKING RAILROAD CROSSING EPOXY	MARKING STOP LINE EPOXY 18-INCH	MARKING DIAGONAL EPOXY 12-INCH	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH
					SOLID YELLOW LF	12.5' LINE 37.5' SKIP YELLOW LF	SOLID YELLOW LF	12.5' LINE 37.5' SKIP YELLOW LF	SOLID WHITE LF	3' LINE 9' SKIP WHITE LF	EACH	LF	LF	LF
0010	BEGIN TO RAILROAD	45+34	- 80+52	STH 130	14,074	--	7,037	--	6,146	75	1	--	--	53
	RAILROAD TO UNION ST	80+52	- 89+95	STH 130	2,934	--	1,467	--	1,468	--	1	--	88	867
	UNION ST TO END	89+95	- 116+15	STH 130	6,672	794	3,336	397	4,767	36	--	22	--	--
<u>CATEGORY TOTAL</u>					<u>23,680</u>	<u>794</u>	<u>11,840</u>	<u>397</u>	<u>12,381</u>	<u>111</u>	<u>2</u>	<u>22</u>	<u>88</u>	<u>920</u>
PROJECT TOTAL					24,474			24,729			2	22	88	920

MISCELLANEOUS PAVEMENT MARKING ITEMS

CATEGORY	LOCATION	STATION TO	STATION	ALIGNMENT	EACH	646.5220		646.8320
						MARKING SYMBOL EPOXY		MARKING PARKING STALL EPOXY
						WHITE LF	BLUE LF	
0010	RAILROAD TO COMMERCIAL EAST	80+50	- 83+00	STH 130	--	62	--	
	RAILROAD TO COMMERCIAL WEST	80+50	- 83+00	STH 130	1	40	20	
	COMMERCIAL TO RICHLAND EAST	83+00	- 86+75	STH 130	--	102	--	
	COMMERCIAL TO RICHLAND WEST	83+00	- 86+75	STH 130	--	110	--	
	RICHLAND TO UNION EAST	86+75	- 89+75	STH 130	1	54	20	
	RICHLAND TO UNION WEST	86+75	- 89+75	STH 130	--	90	--	
<u>CATEGORY TOTAL</u>					<u>2</u>	<u>458</u>	<u>40</u>	
PROJECT TOTAL					2	458	40	

STAKING ITEMS

CATEGORY	LOCATION	STATION TO	STATION	ALIGNMENT	EACH	650.5500	650.8000	650.9000	950.9500	650.9910.01
						CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER LF	CONSTRUCTION STAKING RESURFACING REFERENCE LF	CONSTRUCTION STAKING CURB RAMPS EACH	CONSTRUCTION STAKING SIDEWALK (PROJECT) (5700-02-71) EACH	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (5700-02-71) EACH
						LF	LF	EACH	EACH	EACH
0010	STH 130	45+34	- 116+00			698	7,066	14	1	1
<u>CATEGORY TOTAL</u>						<u>698</u>	<u>7,066</u>	<u>14</u>	<u>1</u>	<u>1</u>
PROJECT TOTAL						698	7,066	14	1	1

SAWING

CATEGORY	LOCATION	STATION	TO STATION	ALIGNMENT	690.0150	690.0250
					SAWING ASPHALT LF	SAWING CONCRETE LF
0010	SE EXCHANGE ST	76+20	- 76+45	STH 130	26	--
	SW COMMERCIAL ST	82+55	- 82+85	STH 130	39	7
	SE COMMERCIAL ST	82+50	- 82+85	STH 130	40	36
	NW COMMERCIAL ST	83+25	- 83+55	STH 130	68	--
	NE COMMERCIAL ST	83+25	- 83+55	STH 130	51	13
	SW RICHLAND ST	86+30	- 86+55	STH 130	72	--
	SE RICHLAND ST	86+35	- 86+55	STH 130	79	--
	NW RICHLAND ST	86+95	- 87+30	STH 130	45	--
	NE RICHLAND ST	86+95	- 87+25	STH 130	53	21
	SW UNION ST	89+25	- 89+50	STH 130	71	7
	SE UNION ST	89+20	- 89+50	STH 130	62	--
<u>CATEGORY TOTAL</u>					<u>606</u>	<u>84</u>
PROJECT TOTAL					606	84

REFERENCE MONUMENTS

CATEGORY	LOCATION	STATION	680.0100
			PUBLIC LAND REFERENCE MONUMENT VERIFY AND RESET EACH
0010	STH 130	53+15.84	1
<u>PROJECT TOTAL</u>			<u>1</u>

NOTES:
THIS EXHIBIT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSES ONLY.
REFER TO THE CONVEYANCE DOCUMENT FOR PARCEL RELATED DETAILS.

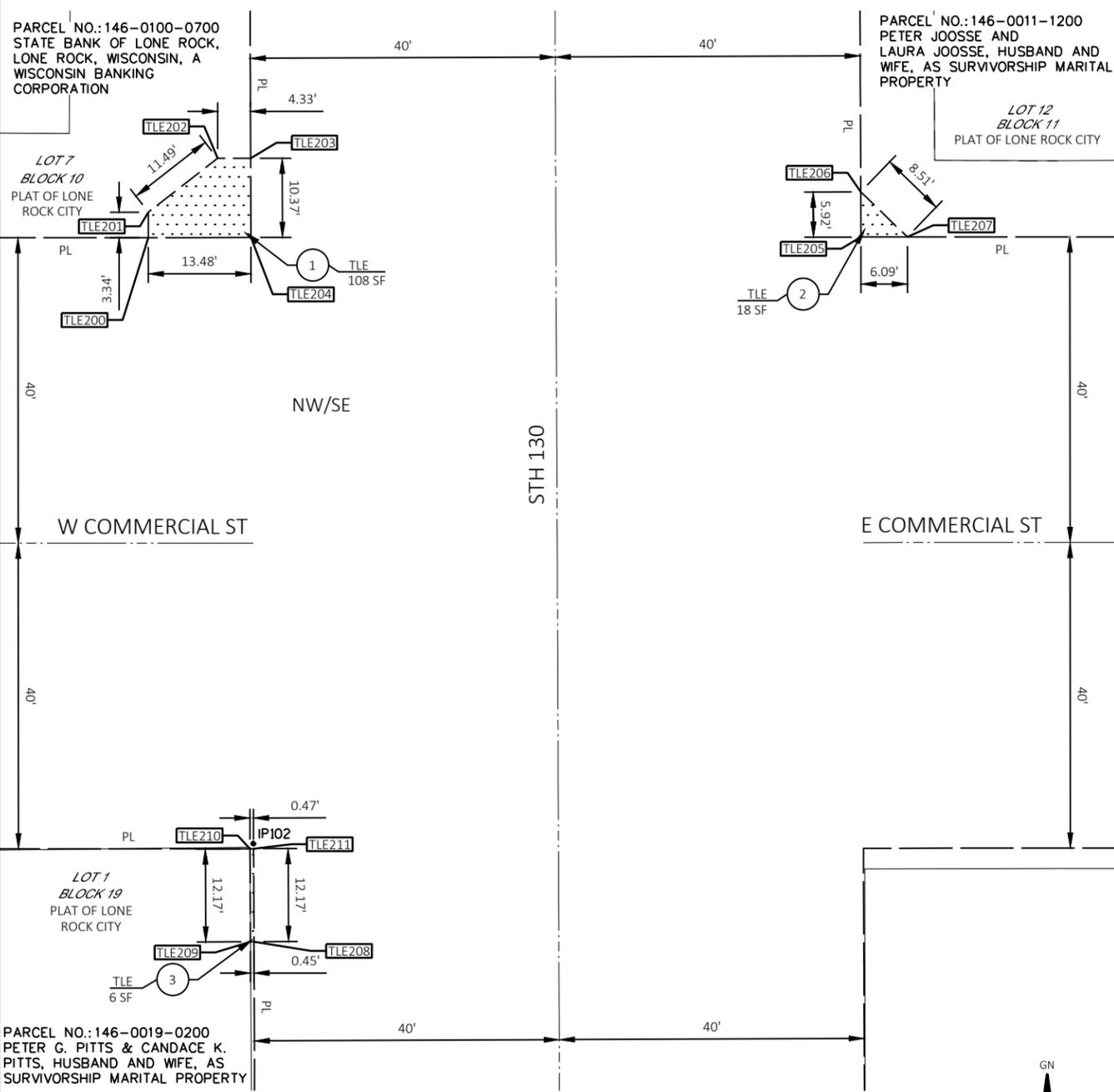
ALL TLES ARE FOR SLOPE GRADING PURPOSES.

R/W PROJECT NUMBER: 5770-02-21 SHEET NUMBER: 1

TLE EXHIBIT
LONE ROCK - STH 154
WISCONSIN RIVER TO USH 14
RICHLAND COUNTY

THAT PART OF LOT 1 OF BLOCK 19, LOT 7 OF BLOCK 10 AND LOT 12 OF BLOCK 11 OF THE PLAT OF LONE ROCK CITY LOCATED IN PART OF THE NW1/4 OF THE SE1/4 OF SECTION 12, TOWNSHIP 8 NORTH, RANGE 2 EAST, VILLAGE OF LONE ROCK, RICHLAND COUNTY, WISCONSIN.

EXISTING MONUMENTS			
POINT	Y (NORTHING)	X (EASTING)	DESCRIPTION
IP102	389433.962	725961.307	CHISELED X



SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE S.F.
1	STATE BANK OF LONE ROCK, LONE ROCK, WISCONSIN, A WISCONSIN BANKING CORPORATION	TLE	108
2	PETER JOOSSE AND LAURA JOOSSE, HUSBAND AND WIFE, AS SURVIVORSHIP MARITAL PROPERTY	TLE	18
3	PETER G. PITTS & CANDACE K. PITTS, HUSBAND AND WIFE, AS SURVIVORSHIP MARITAL PROPERTY	TLE	6

REVISED: 3-3-2026

SCALE, FEET

0 10 20

GN

THIS MAP IS APPROVED FOR THE DEPARTMENT OF TRANSPORTATION
SOUTHWEST - LA CROSSE REGION OFFICE

SIGNATURE: *Cory Schlagel* DATE: 9/2/2022

PRINT NAME: CORY SCHLAGEL

NOTES:
THIS EXHIBIT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSES ONLY.
REFER TO THE CONVEYANCE DOCUMENT FOR PARCEL RELATED DETAILS.

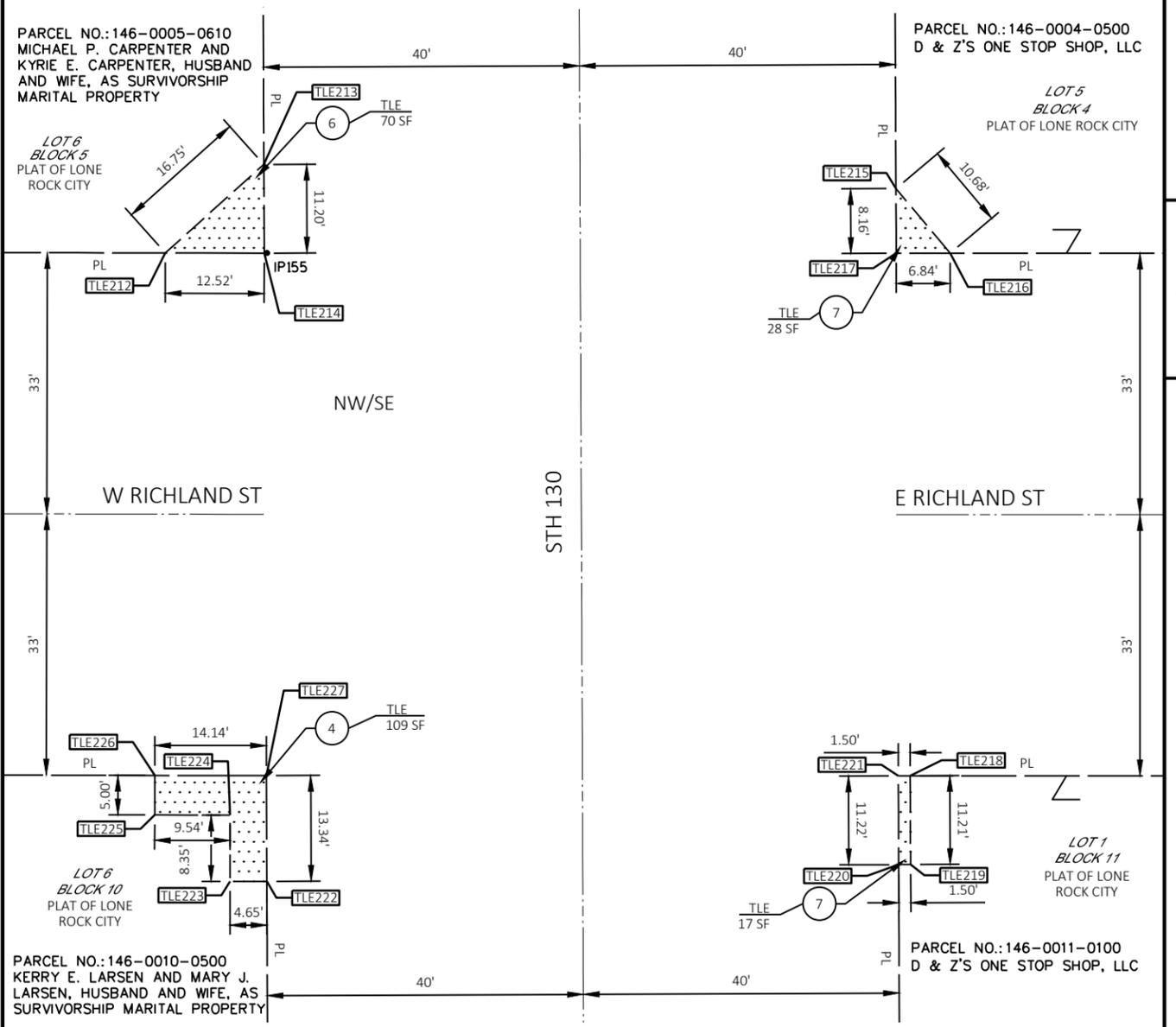
ALL TLES ARE FOR SLOPE GRADING PURPOSES.

R/W PROJECT NUMBER: 5770-02-21 SHEET NUMBER: 2

TLE ACQUISITION EXHIBIT
LONE ROCK - STH 154
WISCONSIN RIVER TO USH 14
RICHLAND COUNTY

THAT PART OF LOT 6 OF BLOCK 10, LOT 6 OF BLOCK 5, LOT 5 OF BLOCK 4 AND LOT 1 OF BLOCK 11 OF THE PLAT OF LONE ROCK CITY LOCATED IN PART OF THE NW1/4 OF THE SE1/4 OF SECTION 12, TOWNSHIP 8 NORTH, RANGE 2 EAST, VILLAGE OF LONE ROCK, RICHLAND COUNTY, WISCONSIN.

EXISTING MONUMENTS			
POINT	Y (NORTHING)	X (EASTING)	DESCRIPTION
IP155	389880.591	725959.820	1.25" O.D. IRON PIPE



EXISTING MONUMENTS			
POINT	Y (NORTHING)	X (EASTING)	DESCRIPTION
IP155	389880.591	725959.820	1.25" O.D. IRON PIPE

SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE S.F.
4	KERRY E. LARSEN AND MARY J. LARSEN, HUSBAND AND WIFE, AS SURVIVORSHIP MARITAL PROPERTY	TLE	109
6	MICHAEL P. CARPENTER AND KYRIE E. CARPENTER, HUSBAND AND WIFE, AS SURVIVORSHIP MARITAL PROPERTY	TLE	70
7	D & Z'S ONE STOP SHOP, LLC	TLE	45

REVISED: 3-3-2026

SCALE, FEET

0 10 20

GN

THIS MAP IS APPROVED FOR THE DEPARTMENT OF TRANSPORTATION
SOUTHWEST - LA CROSSE REGION OFFICE

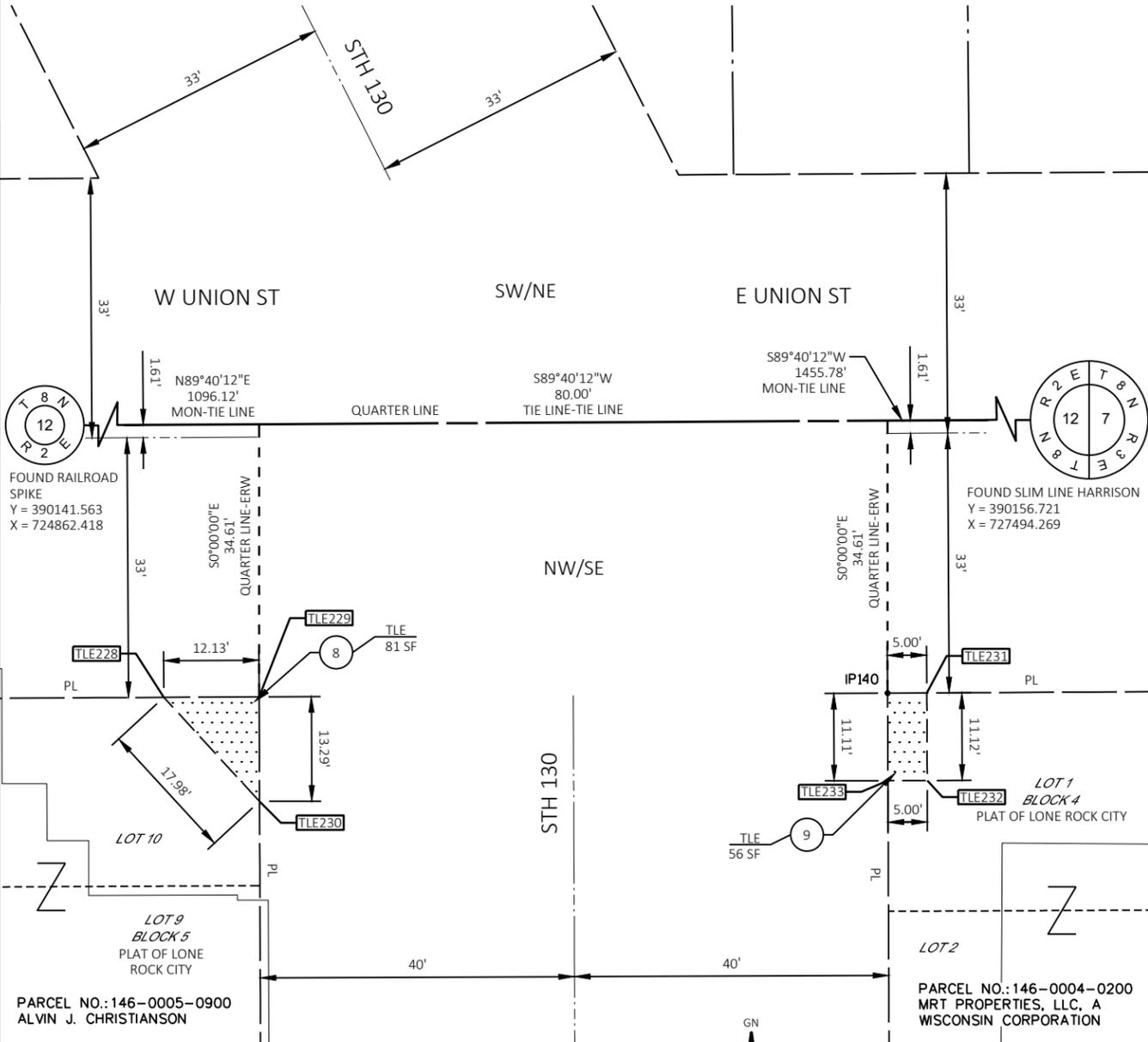
SIGNATURE: *Cory Schlagel* DATE: 9/2/2022

PRINT NAME: CORY SCHLAGEL

NOTES:
 THIS EXHIBIT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSES ONLY.
 REFER TO THE CONVEYANCE DOCUMENT FOR PARCEL RELATED DETAILS.
 ALL TLES ARE FOR SLOPE GRADING PURPOSES.

R/W PROJECT NUMBER: 5770-02-21 SHEET NUMBER: 3
 TLE ACQUISITION EXHIBIT
 LONE ROCK - STH 154
 WISCONSIN RIVER TO USH 14
 STH 130 RICHLAND COUNTY
 THAT PART OF LOT 10 OF BLOCK 5 AND LOT 1 OF BLOCK 4 OF THE PLAT OF LONE
 ROCK CITY LOCATED IN PART OF THE NW1/4 OF THE SE1/4 OF SECTION 12,
 TOWNSHIP 8 NORTH, RANGE 2 EAST, VILLAGE OF LONE ROCK, RICHLAND COUNTY,
 WISCONSIN.

EXISTING MONUMENTS			
POINT	Y (NORTHING)	X (EASTING)	DESCRIPTION
IP140	390113.722	726038.517	1.25" O.D. IRON PIPE



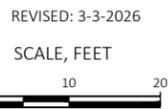
SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE S.F.
8	ALVIN J. CHRISTIANSON	TLE	81
9	MRT PROPERTIES, LLC, A WISCONSIN CORPORATION	TLE	56

THIS MAP IS APPROVED FOR THE DEPARTMENT OF TRANSPORTATION
 SOUTHWEST - LA CROSSE REGION OFFICE

SIGNATURE: *Cory Schlagel* DATE: 9/2/2022
 PRINT NAME: CORY SCHLAGEL



LEGEND

- 1 MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW)
- 2 MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE)
- 3 MARKING PARKING STALL EPOXY (WHITE UNLESS OTHERWISE NOTED)
- 4 MARKING RAILROAD CROSSING EPOXY
- 5 MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- 6 MARKING STOP LINE EPOXY 18-INCH (WHITE)
- 7 MARKING DIAGONAL EPOXY 12-INCH (WHITE)
- 3.5" REMOVING ASPHALTIC SURFACE MILLING 3.5" HMA PAVEMENT 4 LT 58-28 S
- REMOVING ASPHALTIC SURFACE BUTT JOINTS 3.5" HMA PAVEMENT 4 LT 58-28 S
- INLET PROTECTION TYPE C
- SILT FENCE
- SIGN ON PERMANENT SUPPORT

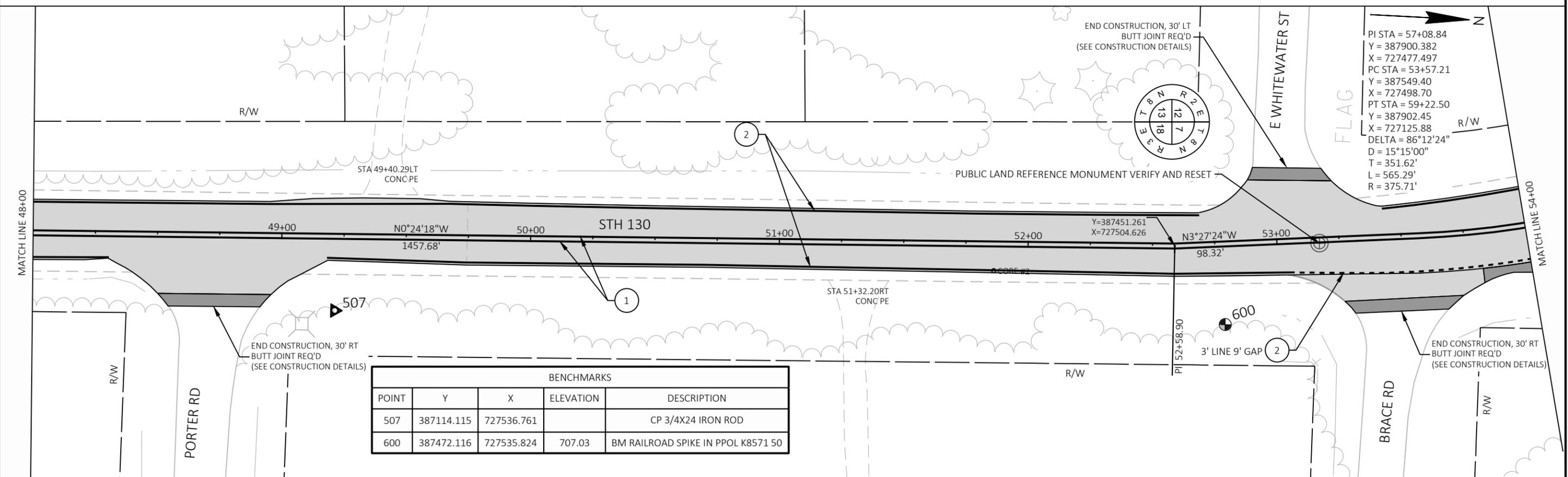
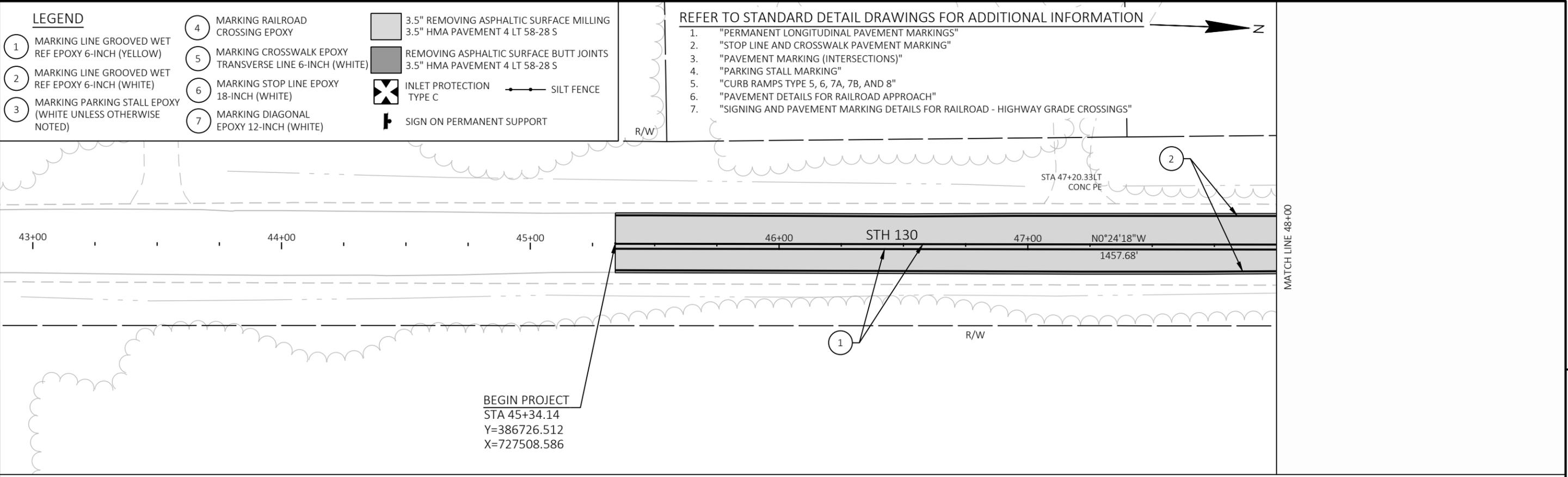
REFER TO STANDARD DETAIL DRAWINGS FOR ADDITIONAL INFORMATION

1. "PERMANENT LONGITUDINAL PAVEMENT MARKINGS"
2. "STOP LINE AND CROSSWALK PAVEMENT MARKING"
3. "PAVEMENT MARKING (INTERSECTIONS)"
4. "PARKING STALL MARKING"
5. "CURB RAMPS TYPE 5, 6, 7A, 7B, AND 8"
6. "PAVEMENT DETAILS FOR RAILROAD APPROACH"
7. "SIGNING AND PAVEMENT MARKING DETAILS FOR RAILROAD - HIGHWAY GRADE CROSSINGS"



5

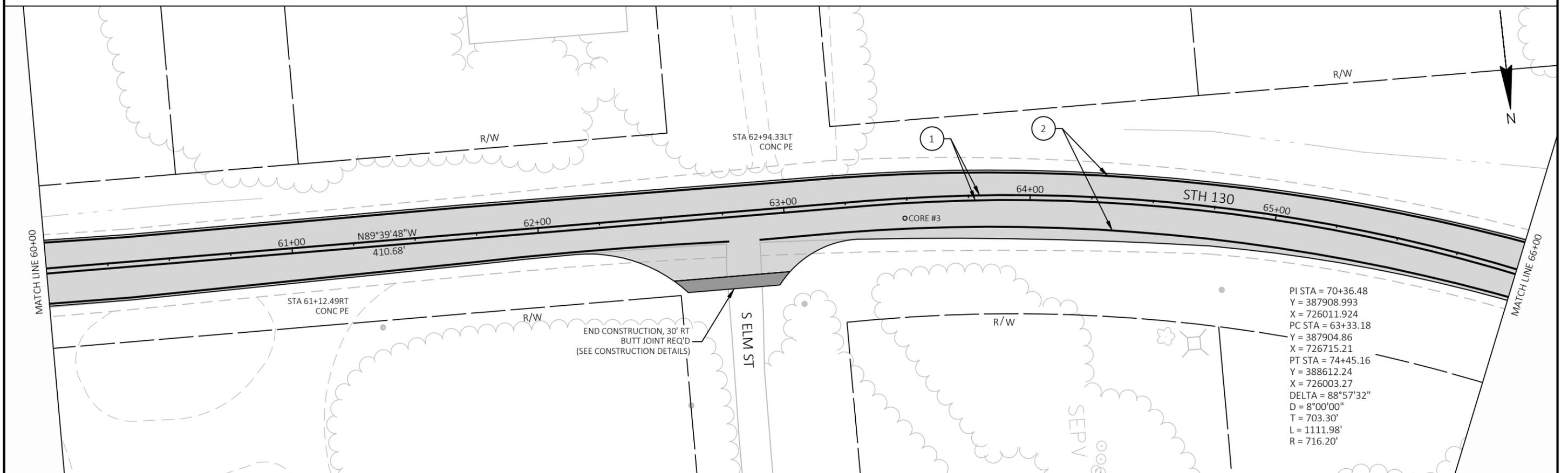
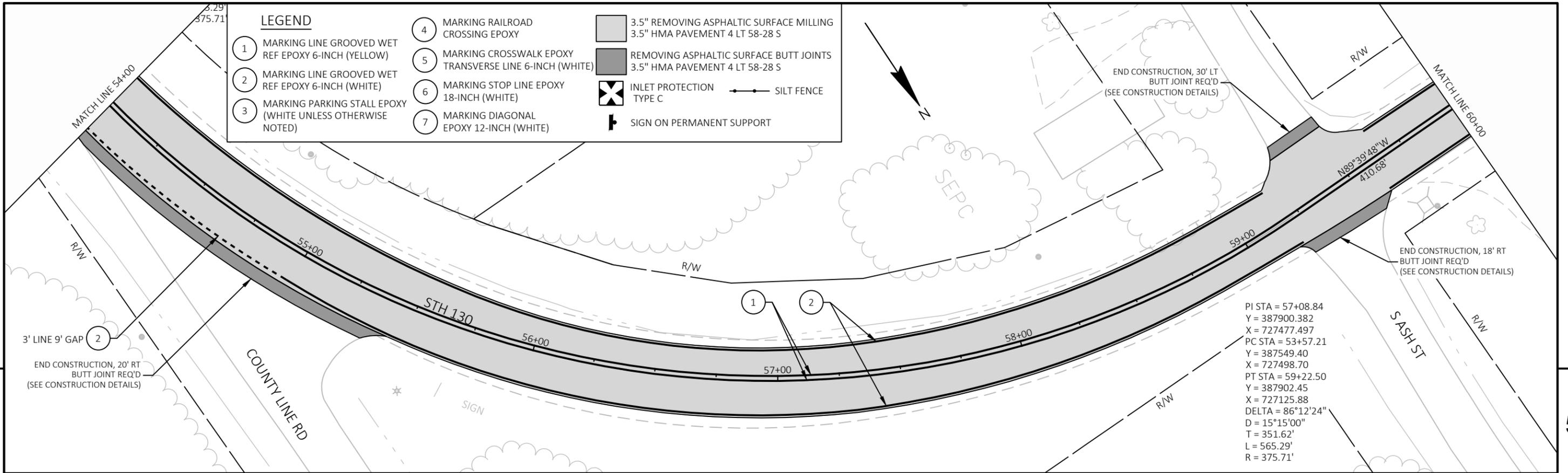
5



BENCHMARKS				
POINT	Y	X	ELEVATION	DESCRIPTION
507	387114.115	727536.761		CP 3/4X24 IRON ROD
600	387472.116	727535.824	707.03	BM RAILROAD SPIKE IN PPOL K8571 50

LEGEND

- | | | |
|--|--|---|
| ① MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW) | ④ MARKING RAILROAD CROSSING EPOXY | 3.5" REMOVING ASPHALTIC SURFACE MILLING
3.5" HMA PAVEMENT 4 LT 58-28 S |
| ② MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE) | ⑤ MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE) | REMOVING ASPHALTIC SURFACE BUTT JOINTS
3.5" HMA PAVEMENT 4 LT 58-28 S |
| ③ MARKING PARKING STALL EPOXY (WHITE UNLESS OTHERWISE NOTED) | ⑥ MARKING STOP LINE EPOXY 18-INCH (WHITE) | INLET PROTECTION TYPE C |
| | ⑦ MARKING DIAGONAL EPOXY 12-INCH (WHITE) | SILT FENCE |
| | | SIGN ON PERMANENT SUPPORT |



PROJECT NO: 5770-02-71

HWY: STH 130

COUNTY: RICHLAND

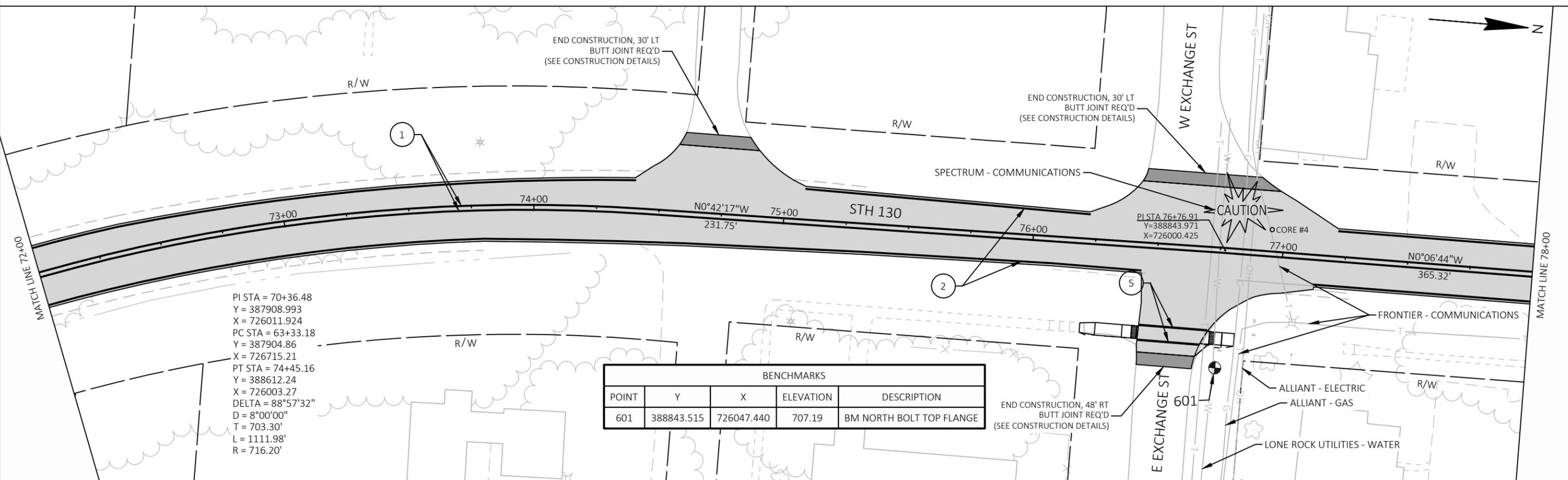
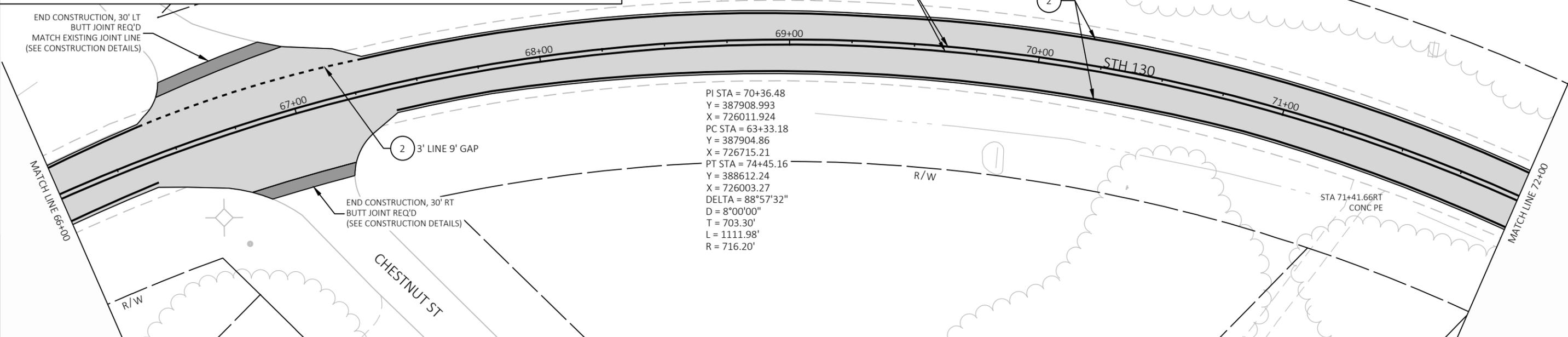
PLAN DETAILS

SHEET

E

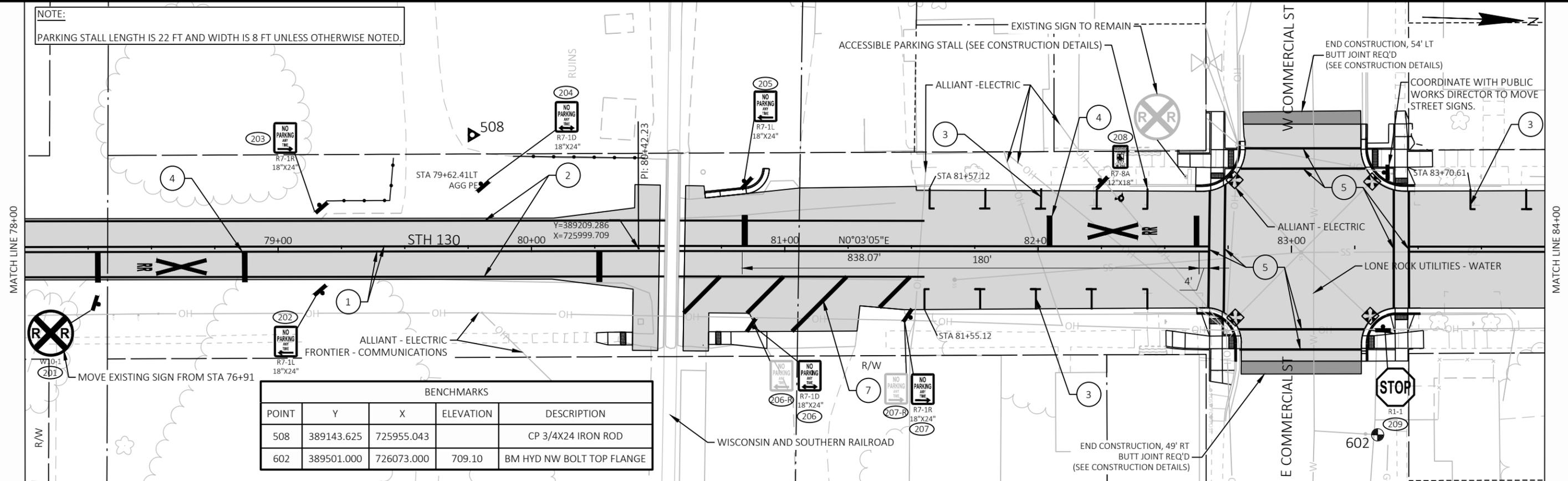
LEGEND

- 1 MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW)
- 2 MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE)
- 3 MARKING PARKING STALL EPOXY (WHITE UNLESS OTHERWISE NOTED)
- 4 MARKING RAILROAD CROSSING EPOXY
- 5 MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- 6 MARKING STOP LINE EPOXY 18-INCH (WHITE)
- 7 MARKING DIAGONAL EPOXY 12-INCH (WHITE)
- 3.5" REMOVING ASPHALTIC SURFACE MILLING 3.5" HMA PAVEMENT 4 LT 58-28 S
- REMOVING ASPHALTIC SURFACE BUTT JOINTS 3.5" HMA PAVEMENT 4 LT 58-28 S
- INLET PROTECTION TYPE C
- SILT FENCE
- SIGN ON PERMANENT SUPPORT

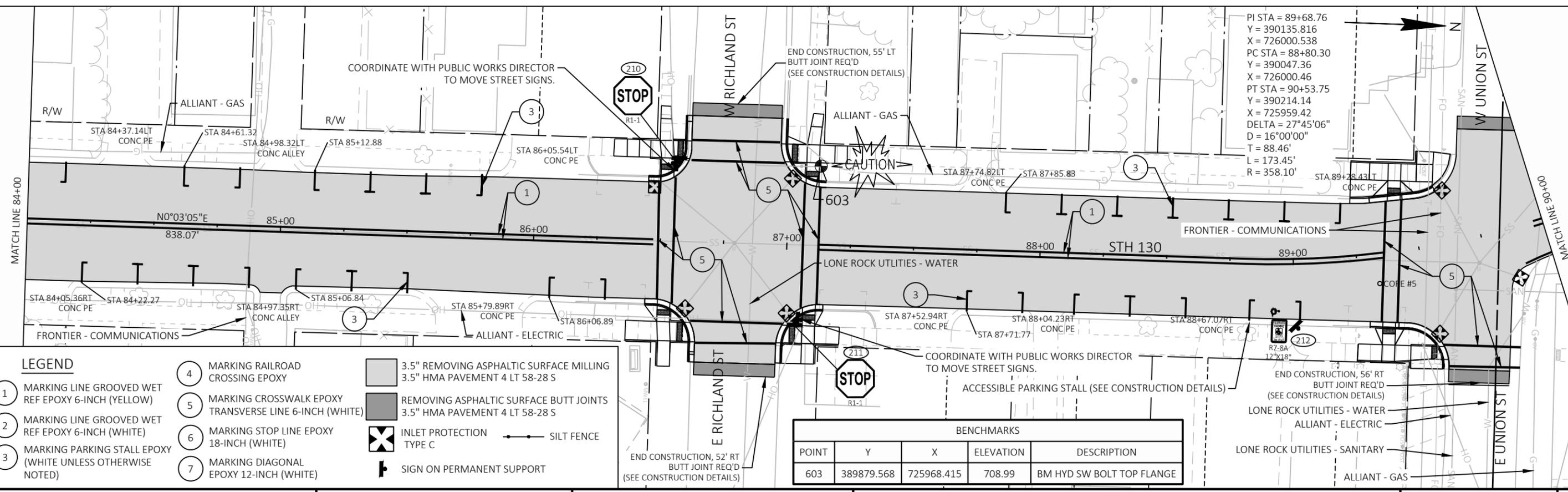


BENCHMARKS				
POINT	Y	X	ELEVATION	DESCRIPTION
601	388843.515	726047.440	707.19	BM NORTH BOLT TOP FLANGE

NOTE:
PARKING STALL LENGTH IS 22 FT AND WIDTH IS 8 FT UNLESS OTHERWISE NOTED.



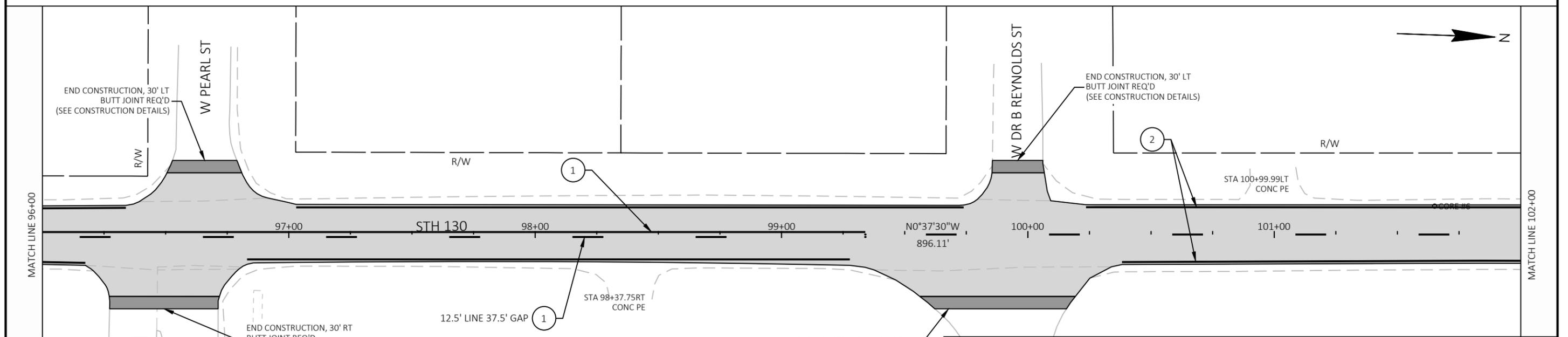
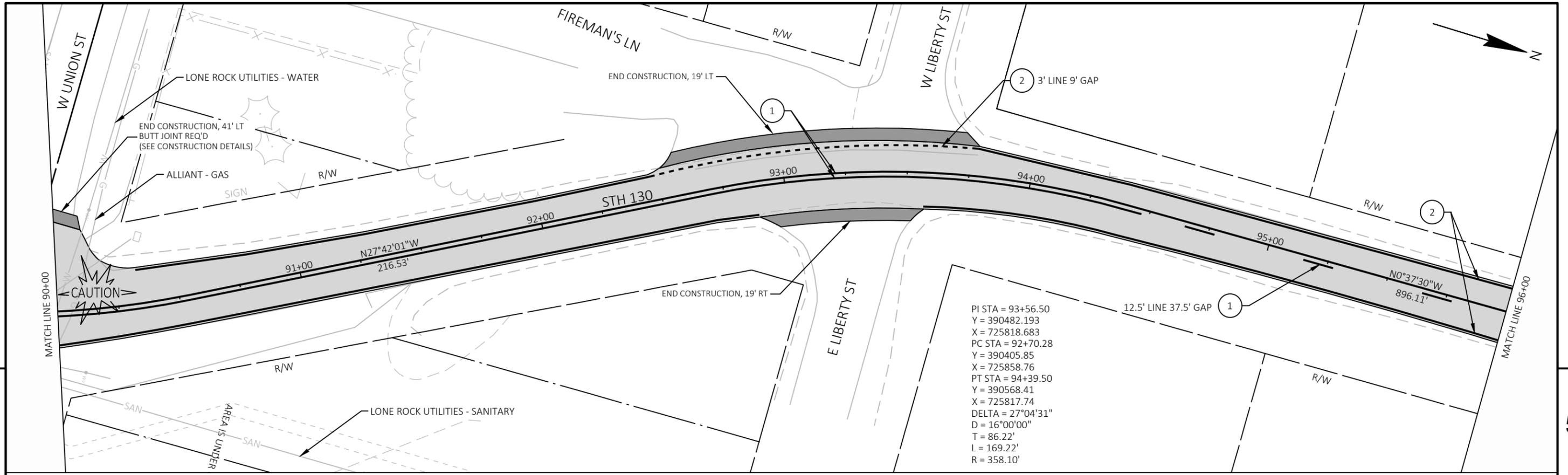
BENCHMARKS				
POINT	Y	X	ELEVATION	DESCRIPTION
508	389143.625	725955.043		CP 3/4X24 IRON ROD
602	389501.000	726073.000	709.10	BM HYD NW BOLT TOP FLANGE



PI STA = 89+68.76
Y = 390135.816
X = 726000.538
PC STA = 88+80.30
Y = 390047.36
X = 726000.46
PT STA = 90+53.75
Y = 390214.14
X = 725959.42
DELTA = 27°45'06"
D = 16°00'00"
T = 88.46'
L = 173.45'
R = 358.10'

LEGEND	
1	MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW)
2	MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE)
3	MARKING PARKING STALL EPOXY (WHITE UNLESS OTHERWISE NOTED)
4	MARKING RAILROAD CROSSING EPOXY
5	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
6	MARKING STOP LINE EPOXY 18-INCH (WHITE)
7	MARKING DIAGONAL EPOXY 12-INCH (WHITE)
	3.5" REMOVING ASPHALTIC SURFACE MILLING 3.5" HMA PAVEMENT 4 LT 58-28 S
	REMOVING ASPHALTIC SURFACE BUTT JOINTS 3.5" HMA PAVEMENT 4 LT 58-28 S
	INLET PROTECTION TYPE C
	SILT FENCE
	SIGN ON PERMANENT SUPPORT

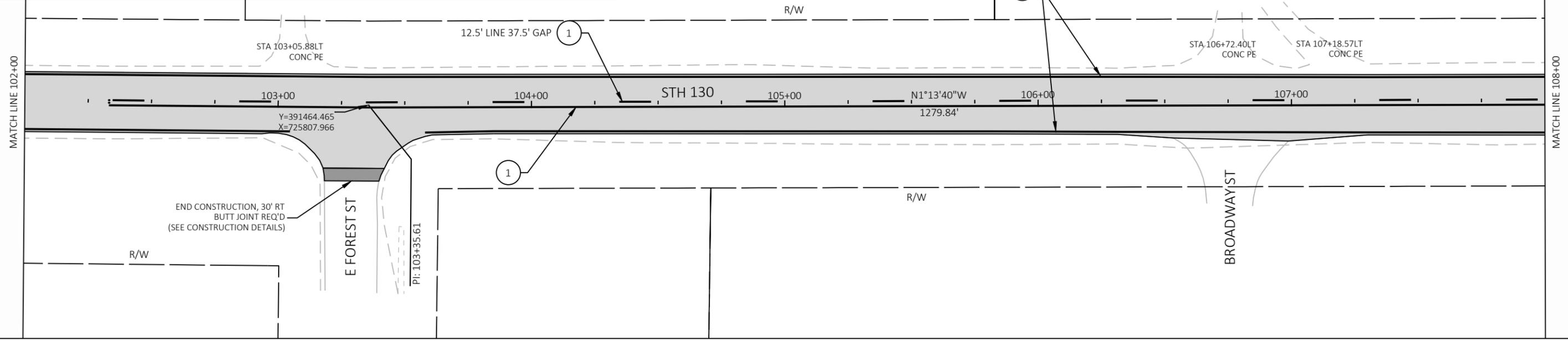
BENCHMARKS				
POINT	Y	X	ELEVATION	DESCRIPTION
603	389879.568	725968.415	708.99	BM HYD SW BOLT TOP FLANGE



LEGEND	
①	MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW)
②	MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE)
③	MARKING PARKING STALL EPOXY (WHITE UNLESS OTHERWISE NOTED)
④	MARKING RAILROAD CROSSING EPOXY
⑤	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
⑥	MARKING STOP LINE EPOXY 18-INCH (WHITE)
⑦	MARKING DIAGONAL EPOXY 12-INCH (WHITE)
[Grey Box]	3.5" REMOVING ASPHALTIC SURFACE MILLING 3.5" HMA PAVEMENT 4 LT 58-28 S
[Dark Grey Box]	REMOVING ASPHALTIC SURFACE BUTT JOINTS 3.5" HMA PAVEMENT 4 LT 58-28 S
[X Symbol]	INLET PROTECTION TYPE C
[T Symbol]	SIGN ON PERMANENT SUPPORT
[Dashed Line]	SILT FENCE

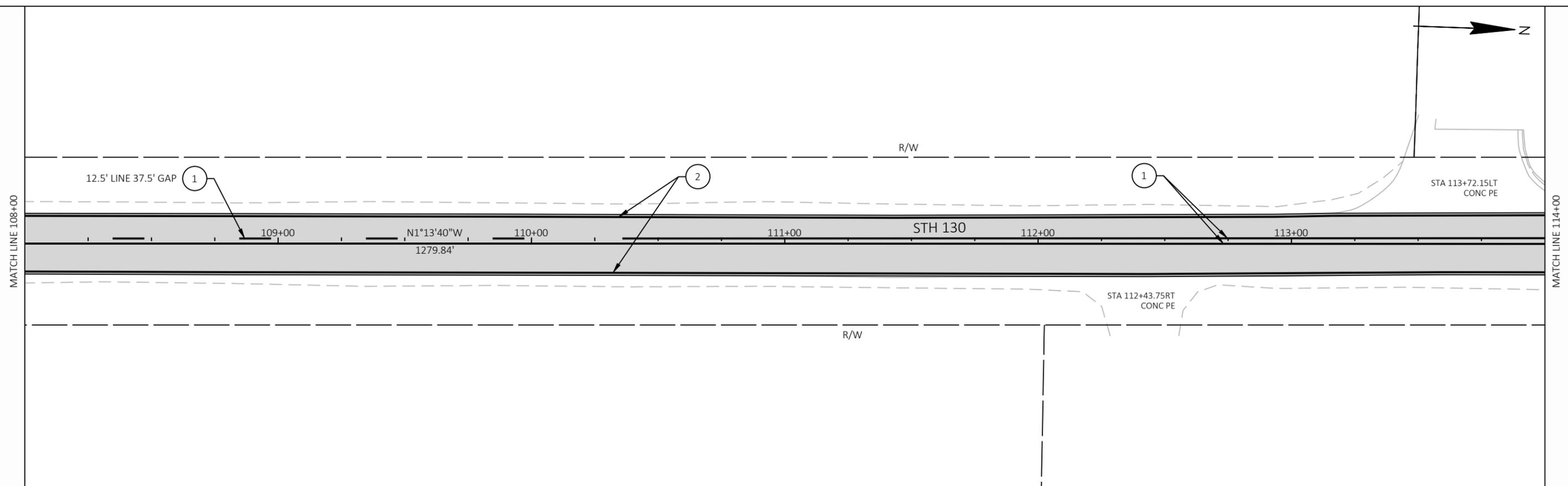
LEGEND

- ① MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW)
- ② MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE)
- ③ MARKING PARKING STALL EPOXY (WHITE UNLESS OTHERWISE NOTED)
- ④ MARKING RAILROAD CROSSING EPOXY
- ⑤ MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- ⑥ MARKING STOP LINE EPOXY 18-INCH (WHITE)
- ⑦ MARKING DIAGONAL EPOXY 12-INCH (WHITE)
- 3.5" REMOVING ASPHALTIC SURFACE MILLING 3.5" HMA PAVEMENT 4 LT 58-28 S
- REMOVING ASPHALTIC SURFACE BUTT JOINTS 3.5" HMA PAVEMENT 4 LT 58-28 S
- INLET PROTECTION TYPE C
- SILT FENCE
- SIGN ON PERMANENT SUPPORT



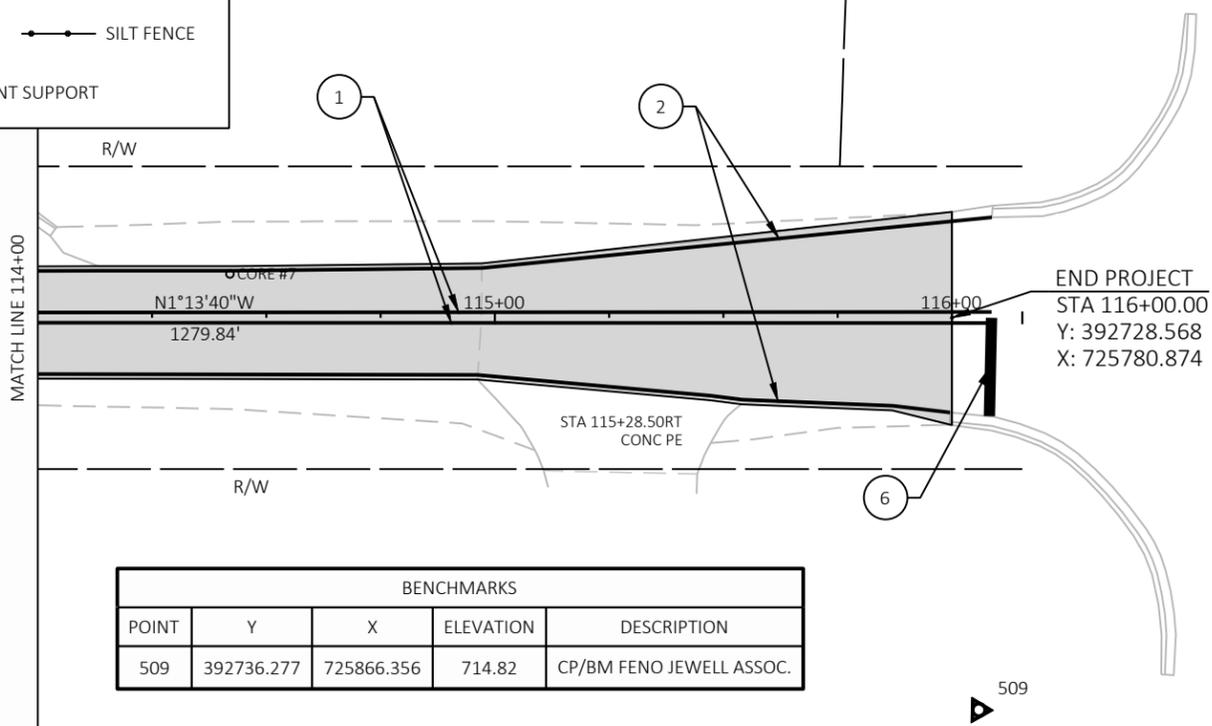
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LEGEND

- ① MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW)
- ② MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE)
- ③ MARKING PARKING STALL EPOXY (WHITE UNLESS OTHERWISE NOTED)
- ④ MARKING RAILROAD CROSSING EPOXY
- ⑤ MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- ⑥ MARKING STOP LINE EPOXY 18-INCH (WHITE)
- ⑦ MARKING DIAGONAL EPOXY 12-INCH (WHITE)
- 3.5" REMOVING ASPHALTIC SURFACE MILLING
3.5" HMA PAVEMENT 4 LT 58-28 S
- REMOVING ASPHALTIC SURFACE BUTT JOINTS
3.5" HMA PAVEMENT 4 LT 58-28 S
- INLET PROTECTION TYPE C
- SILT FENCE
- SIGN ON PERMANENT SUPPORT



END PROJECT
 STA 116+00.00
 Y: 392728.568
 X: 725780.874

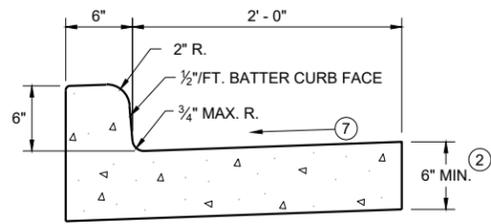
BENCHMARKS				
POINT	Y	X	ELEVATION	DESCRIPTION
509	392736.277	725866.356	714.82	CP/BM FENO JEWELL ASSOC.

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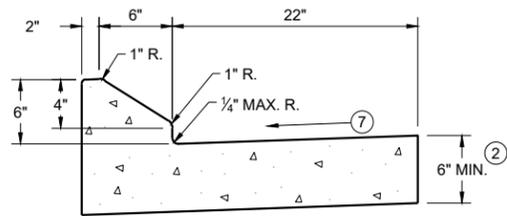
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Standard Detail Drawing List

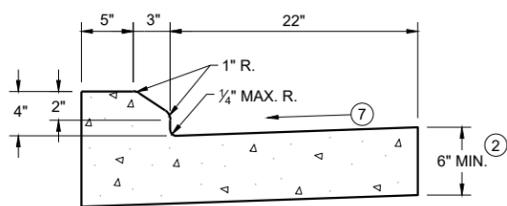
08D01-24A	CONCRETE CURB & GUTTER
08D01-24B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-22B	CURB RAMPS TYPES 2 AND 3
08D05-22D	CURB RAMPS TYPES 4B AND 4B1
08D05-22E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-22G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D19-05	DRIVEWAY AND SIDEWALK RAMPS TYPE Z
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
13B01-11A	PAVEMENT DETAILS FOR RAILROAD APPROACH
13B01-11B	TYPICAL SECTIONS FOR RAILWAY APPROACH
13C19-03	HMA LONGITUDINAL JOINTS
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-09C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C02-09H	MODIFIED ROUTE ASSEMBLY FOR DETOUR SIGNING
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C07-16A	PAVEMENT MARKING SYMBOLS
15C08-24A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C08-24B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C09-13A	SIGNING AND PAVEMENT MARKING DETAILS FOR RAILROAD-HIGHWAY GRADE CROSSINGS
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C19-10A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C33-05	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-06A	PAVEMENT MARKING (INTERSECTIONS)
15C36-01	PARKING STALL MARKING
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D30-11A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11B	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11J	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11L	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION



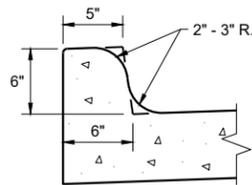
TYPES A^① & D



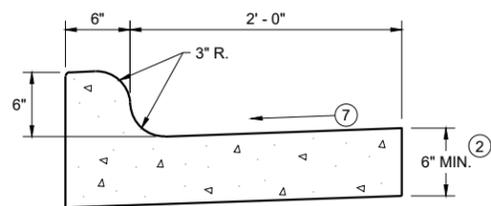
6" SLOPED CURB TYPES G^① & J



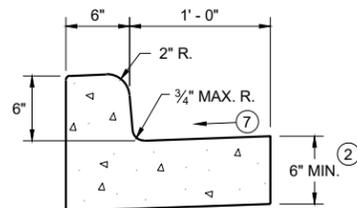
4" SLOPED CURB TYPES G^① & J



TYPES K^① & L
(OPTIONAL CURB SHAPE)

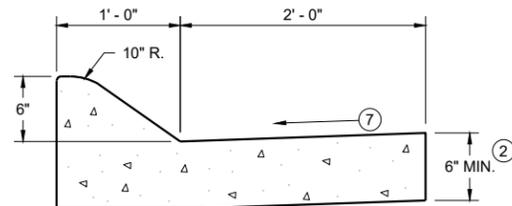


TYPES K^① & L
CONCRETE CURB AND GUTTER 30"

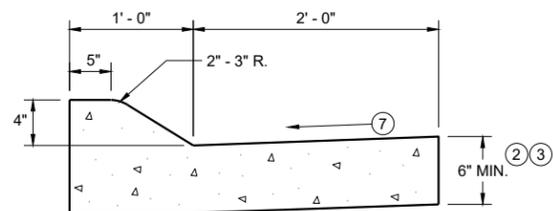


TYPES A^① & D

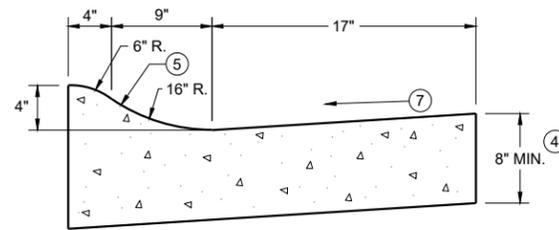
CONCRETE CURB AND GUTTER 18"



6" SLOPED CURB TYPES A^① & D

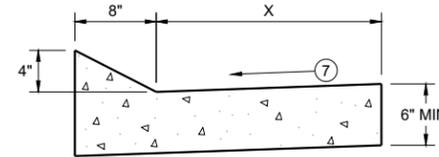


4" SLOPED CURB TYPES A^① & D
CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R^① & T
CONCRETE CURB AND GUTTER 30"

TBT & TBTT	X
30"	22"
36"	28"

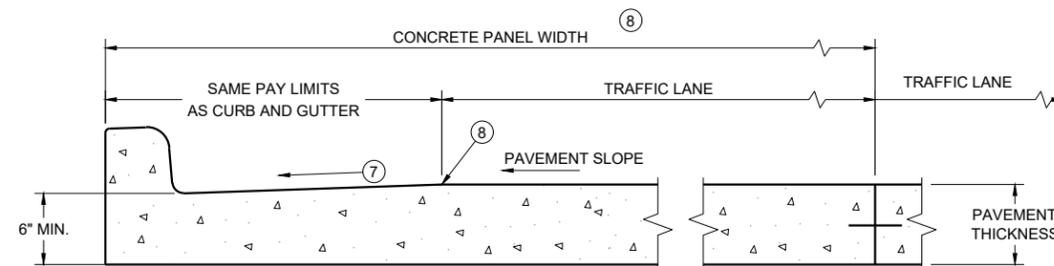


TYPES TBT & TBTT^①

CONCRETE CURB AND GUTTER

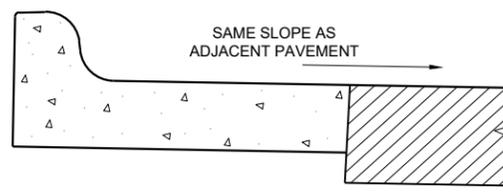
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER^⑥
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

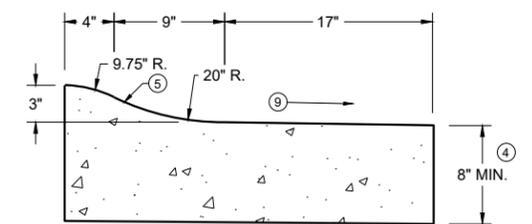
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

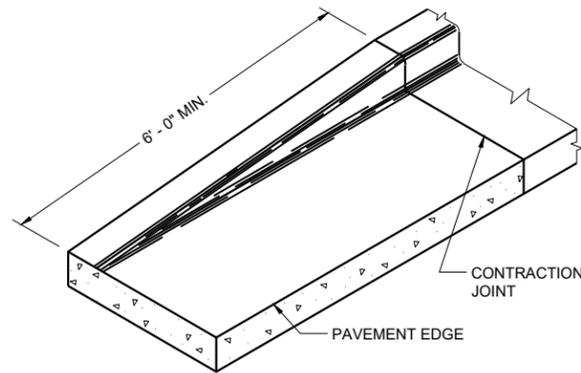
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ SLOPE TO BE REVERSE SLOPE MATCHING THE SLOPE OF THE PAVEMENT AND THE CIRCULATORY ROADWAY



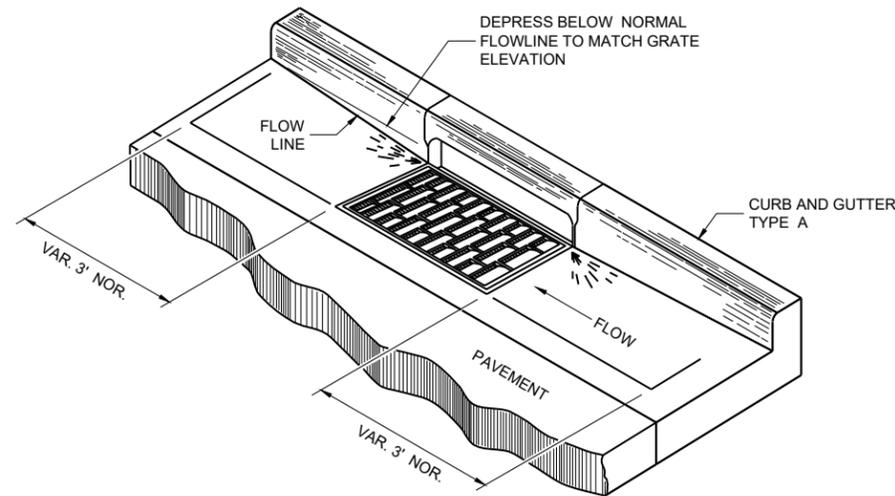
3" SLOPED CURB TYPES R^① & T

CONCRETE CURB AND GUTTER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

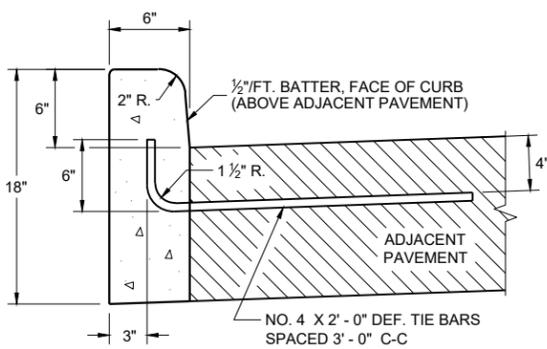


END SECTION CURB AND GUTTER

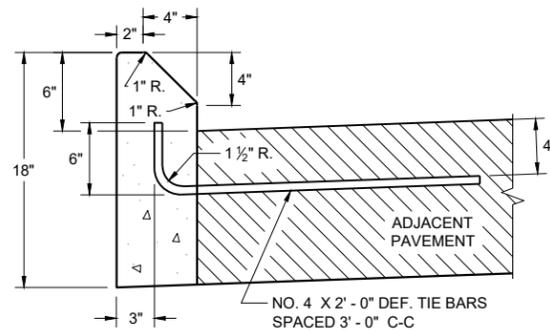


DETAIL OF CURB AND GUTTER AT INLETS

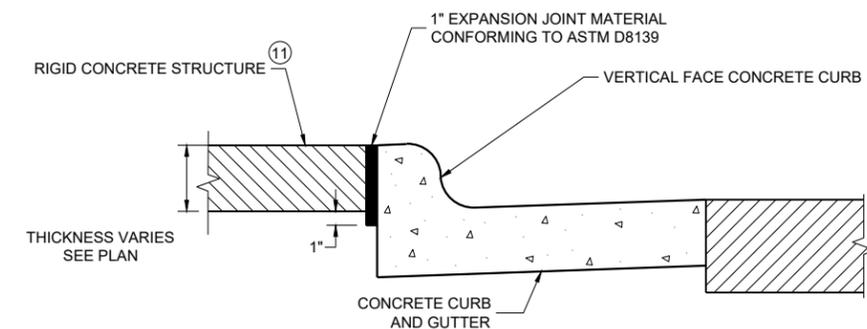
(TYPICAL H INLET COVER SHOWN)



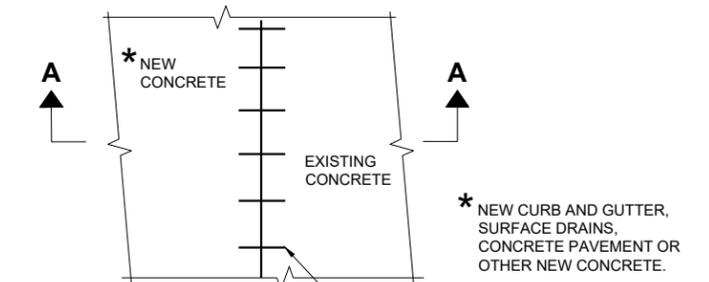
TYPES A^① & D



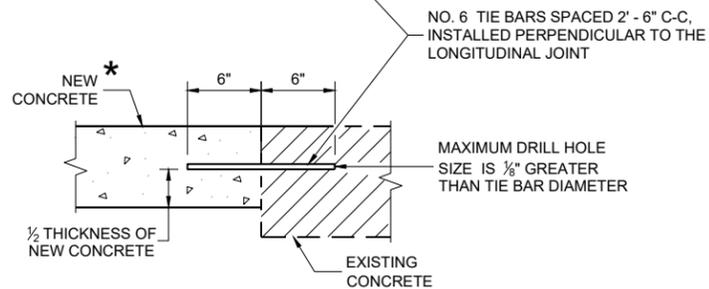
**TYPES G^① & J
CONCRETE CURB**



EXPANSION JOINT DETAIL FOR VERTICAL CURB ABUTTING A RIGID STRUCTURE^⑪



PLAN VIEW



**SECTION A - A
TIE BARS DRILLED INTO EXISTING PAVEMENT**

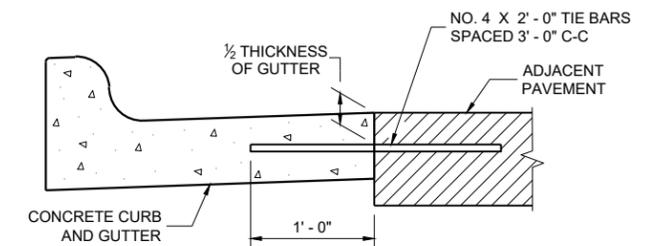
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

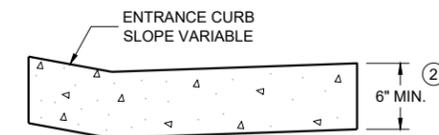
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑩ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.
- ⑪ PLACE 1" THICK EXPANSION JOINT MATERIAL BETWEEN VERTICAL FACE CURB TYPES EXTENDING FROM THE TOP OF CURB TO 1 INCH BELOW THE ADJOINING CONCRETE SURFACE. RIGID CONCRETE STRUCTURES INCLUDE RAISED CONCRETE MEDIANS, CONCRETE SAFETY ISLANDS, SPLITTER ISLANDS, OR LOCATIONS IDENTIFIED ON THE PLANS.

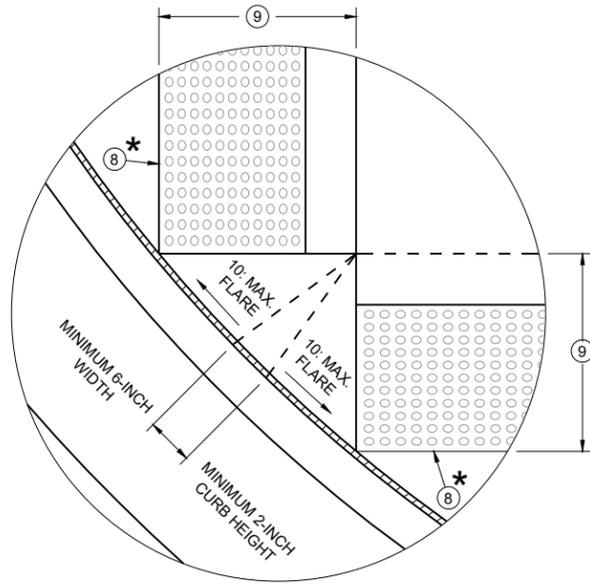


TYPICAL TIE BAR LOCATION^①

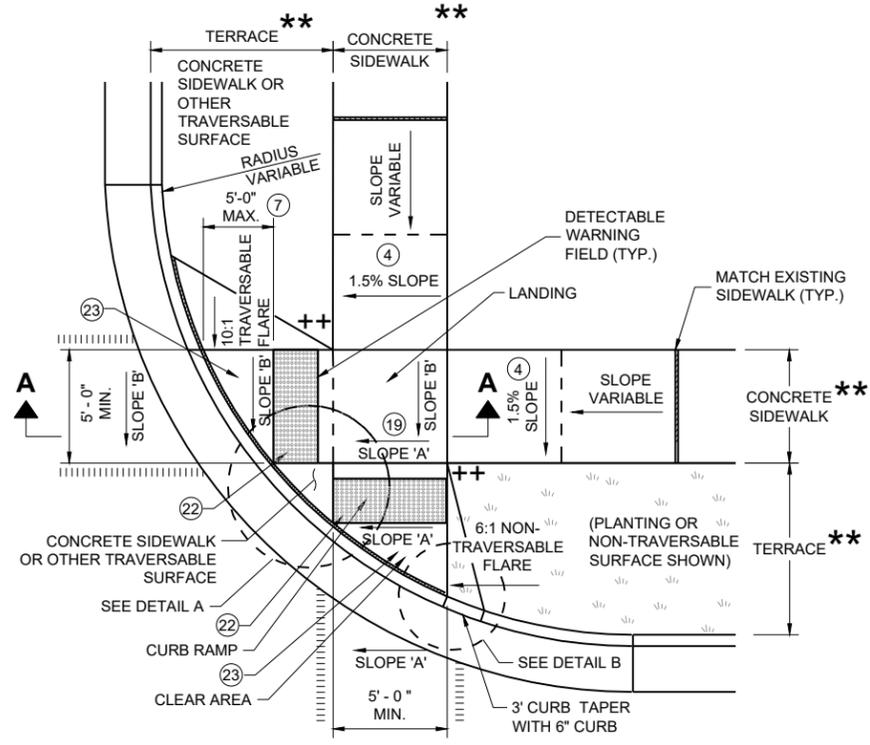


**DRIVEWAY ENTRANCE CURB^⑩
(WHEN DIRECTED BY THE ENGINEER)**

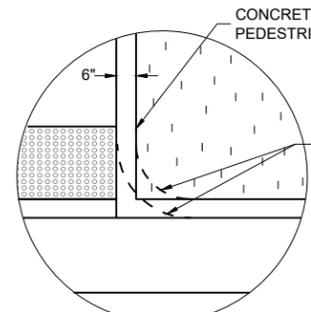
CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2025 DATE	/s/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR



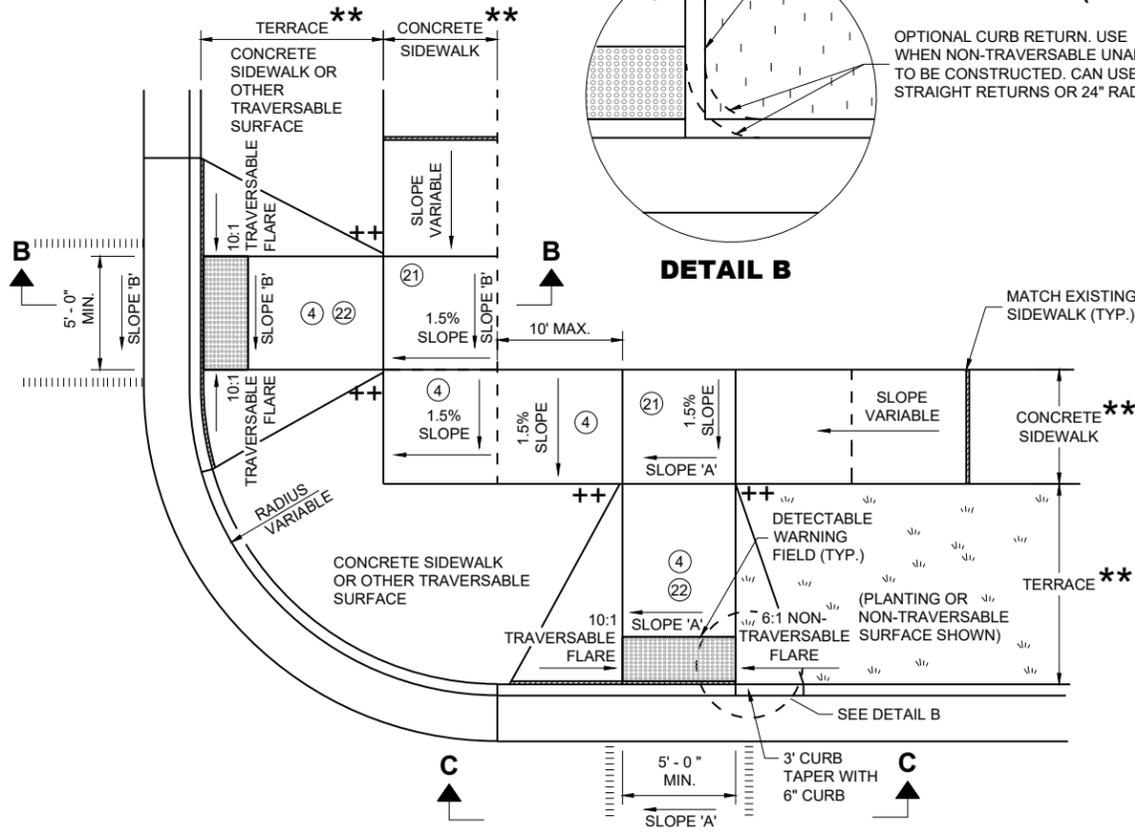
DETAIL A



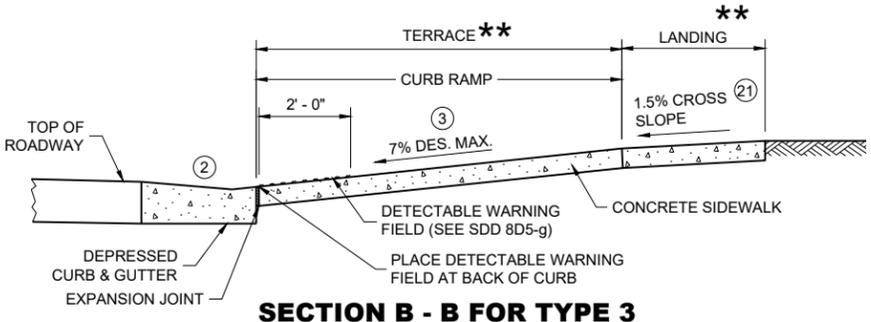
**PLAN VIEW
CURB RAMP TYPE 2
(CENTER OF CORNER RADIUS)**



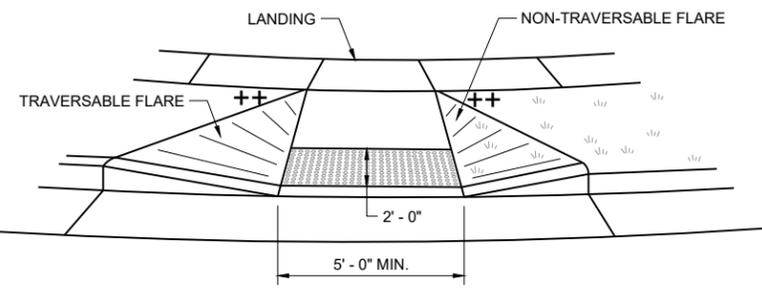
DETAIL B



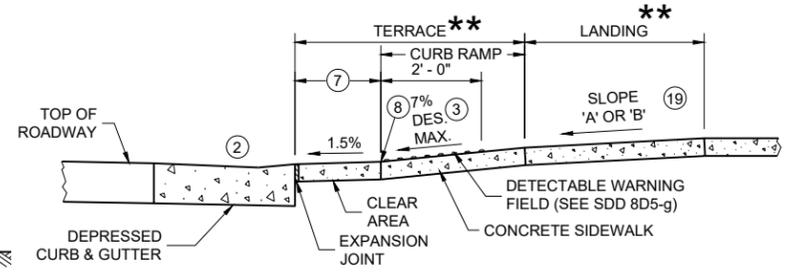
**PLAN VIEW
CURB RAMP TYPE 3
(OUTSIDE OF CROSSWALK AREA)**



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3



SECTION A - A FOR TYPE 2

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)
- * MAXIMUM 2.1% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS
- ++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF CURB RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER COUNTER SLOPE AND THE CURB RAMP SLOPE IS DESIRABLY 11% OR LESS AND SHALL NOT EXCEED 13.3%. TYPICAL GUTTER COUNTER SLOPE IS 4% BUT MAY BE MODIFIED TO FIT FIELD CONDITIONS. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5%, DESIRABLY 7% OR LESS, AND SHALL NOT EXCEED A MAXIMUM OF 8.3%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
 - ③ MAXIMUM 8.3% CURB RAMP SLOPE IS ALLOWABLE WITH GUTTER COUNTER SLOPE OF 5% MAXIMUM AND A 13.3% MAXIMUM GRADE CHANGE.
 - ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2.1% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
 - ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-4.
 - ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
 - ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% DESIGN MAXIMUM SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% DESIGN MAXIMUM SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.
 - ⑰ A MAXIMUM 2-INCH CONCRETE BORDER IS PERMITTED ALONG ALL SIDES OF THE DETECTABLE WARNING FIELD SURFACE.
 - ⑲ WHERE A LANDING SERVES TWO CURB RAMPS, THE LANDING SLOPE SHALL NOT EXCEED THE CROSS SLOPE AT THE BOTTOM OF THE RAMP OR WITHIN THE CROSSWALK PARALLEL TO THE DIRECTION OF TRAVEL.
 - ⑳ PROVIDE A LANDING WITH A SLOPE PARALLEL TO ROADWAY THAT MATCHES SLOPE AT THE BOTTOM OF THE ADJACENT RAMP. SLOPE PERPENDICULAR TO ROADWAY SHALL BE 2.1% MAXIMUM. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
 - ㉑ THE ENTIRE RAMP SHALL BE A PLANAR SURFACE. DO NOT WARP THE RUNNING SLOPE OR CROSS SLOPE OF THE RAMP. WARPING OF THE SIDEWALK CROSS SLOPE SHALL TAKE PLACE BETWEEN THE LANDING AND MATCH POINT.
 - ㉒ THE CLEAR AREA BETWEEN THE BOTTOM OF RAMP AND BACK OF CURB SHALL BE SLOPED SO THAT WATER DRAINS OUT OF ONE SIDE OR BOTH SIDES OF THE CURB OPENING.

6

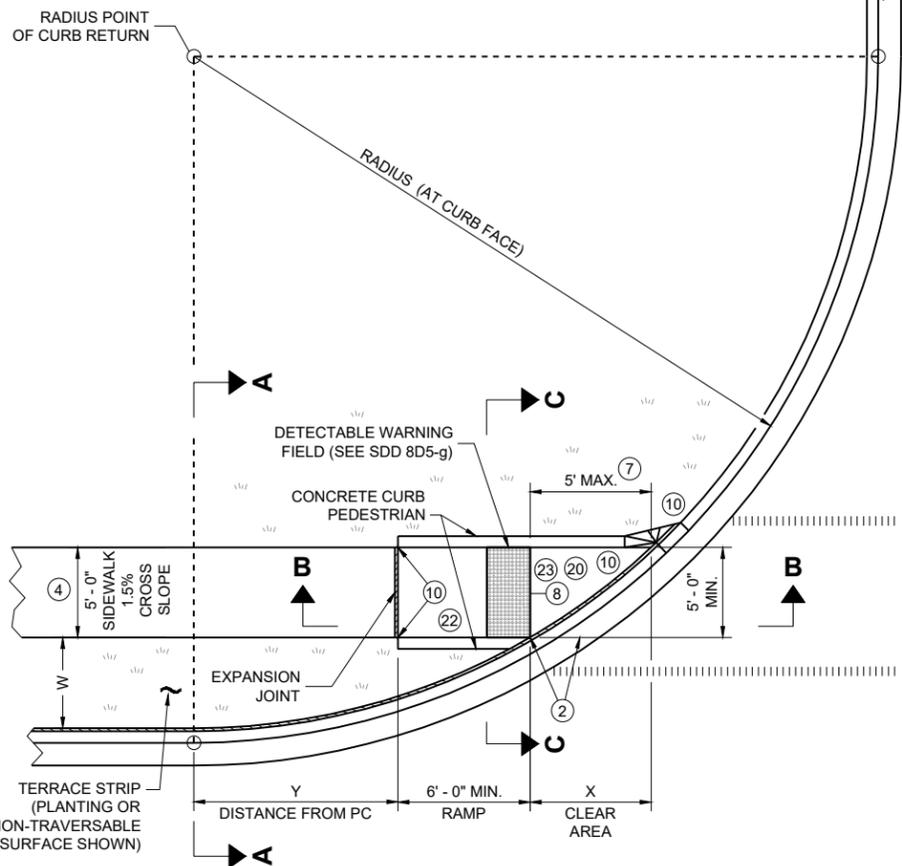
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SDD 08D05-22b

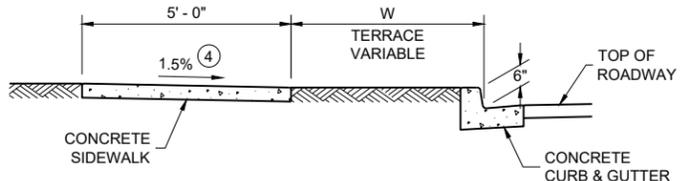
SDD 08D05-22b

**CURB RAMPS
TYPE 2 AND 3**

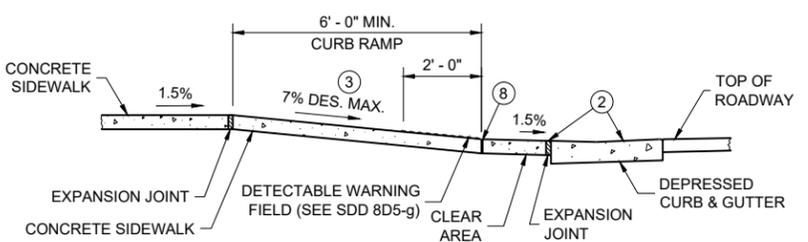
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**PLAN VIEW
CURB RAMP TYPE 4B**



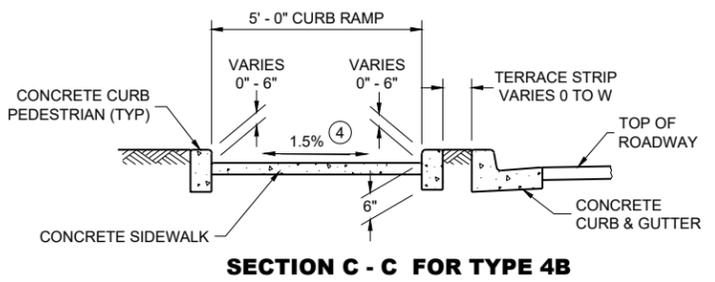
SECTION A - A FOR TYPE 4B



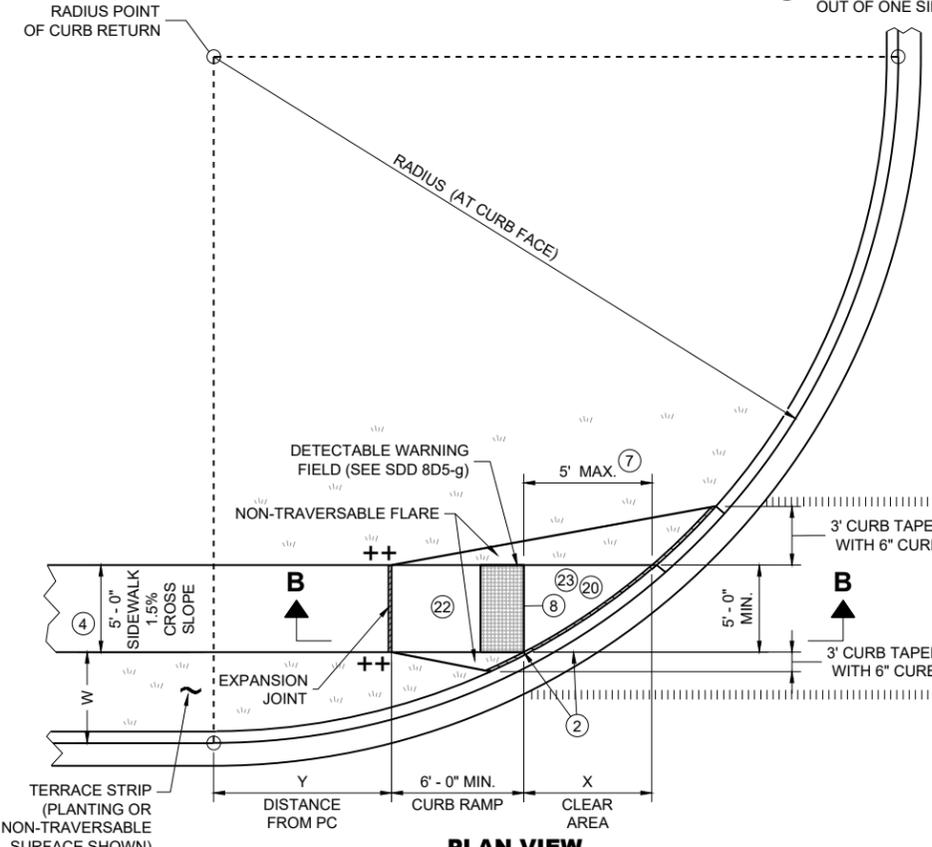
**SECTION B - B FOR
TYPE 4B AND TYPE 4B1**

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET			4' - 11 1/2"	5' - 1 3/4"	4' - 3 1/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET									4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET															4' - 10 3/4"	19' - 8 1/4"

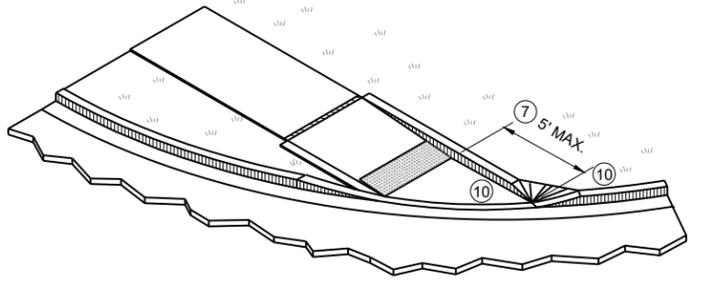
INTERMEDIATE RADII CAN BE INTERPOLATED
DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH



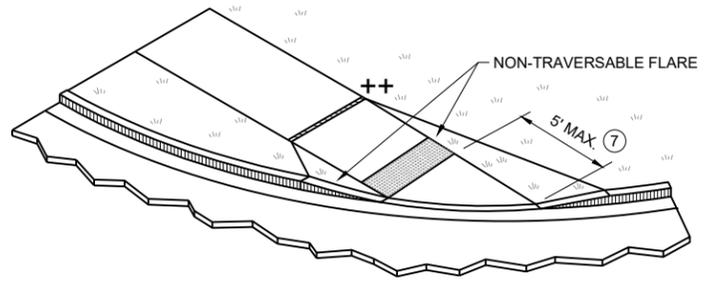
SECTION C - C FOR TYPE 4B



**PLAN VIEW
CURB RAMP TYPE 4B1**



ISOMETRIC VIEW FOR TYPE 4B



ISOMETRIC VIEW FOR TYPE 4B1

- LEGEND**
- 1/2" EXPANSION JOINT SIDEWALK
 - CONTRACTION JOINT SIDEWALK
 - PAVEMENT MARKING CROSSWALK (WHITE)
 - MAXIMUM 2.1% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK
 - CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF CURB RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- 2 GRADE CHANGE BETWEEN GUTTER COUNTER SLOPE AND THE CURB RAMP SLOPE IS DESIRABLY 11% OR LESS AND SHALL NOT EXCEED 13.3%. TYPICAL GUTTER COUNTER SLOPE IS 4% BUT MAY BE MODIFIED TO FIT FIELD CONDITIONS. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5%, DESIRABLY 7% OR LESS, AND SHALL NOT EXCEED A MAXIMUM OF 8.3%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 MAXIMUM 8.3% CURB RAMP SLOPE IS ALLOWABLE WITH GUTTER COUNTER SLOPE OF 5% MAXIMUM AND A 13.3% MAXIMUM GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2.1% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 7 WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- 8 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 10 INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- 17 A MAXIMUM 2-INCH CONCRETE BORDER IS PERMITTED ALONG ALL SIDES OF THE DETECTABLE WARNING FIELD SURFACE.
- 20 MAXIMUM 1.5% DESIGN MAXIMUM AND 2.1% PROWAG MAXIMUM RUNNING SLOPE ON CLEAR AREA. CROSS SLOPE OF CLEAR AREA SHALL MATCH THE CROSS SLOPE OF THE ADJACENT CROSSWALK.
- 22 THE ENTIRE RAMP SHALL BE A PLANAR SURFACE. DO NOT WARP THE RUNNING SLOPE OR CROSS SLOPE OF THE RAMP. WARPING OF THE SIDEWALK CROSS SLOPE SHALL TAKE PLACE BETWEEN THE LANDING AND MATCH POINT.
- 23 THE CLEAR AREA BETWEEN THE BOTTOM OF RAMP AND BACK OF CURB SHALL BE SLOPED SO THAT WATER DRAINS OUT OF ONE SIDE OR BOTH SIDES OF THE CURB OPENING.

6

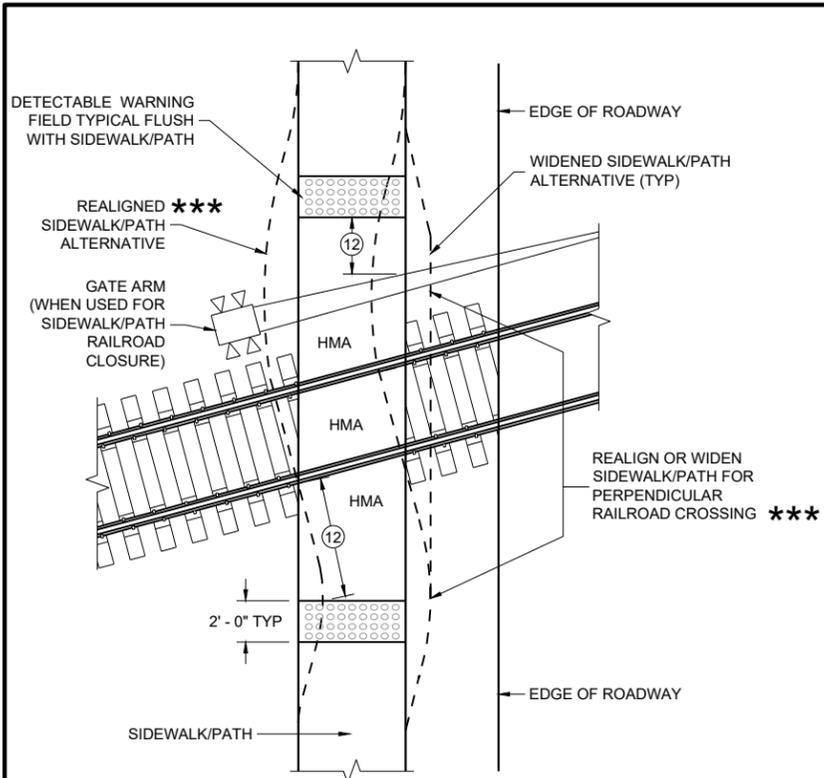
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SDD 08D05-22d

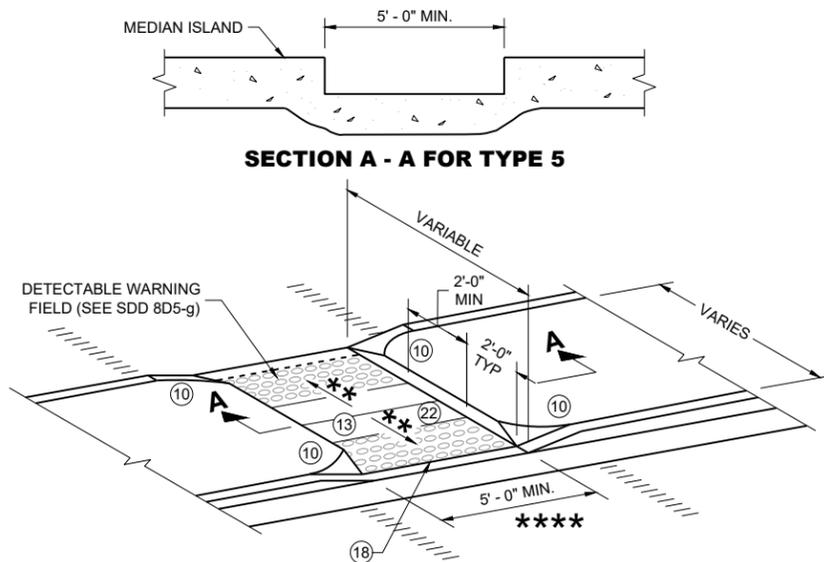
SDD 08D05-22d

**CURB RAMPS
TYPE 4B AND 4B1**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



CURB RAMP TYPE 8
DETECTABLE WARNINGS
FOR SIDEWALKS OR SHARED USE PATHS
AT RAILROAD CROSSINGS



SECTION A - A FOR TYPE 5

CURB RAMP TYPE 5
MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING

GENERAL NOTES

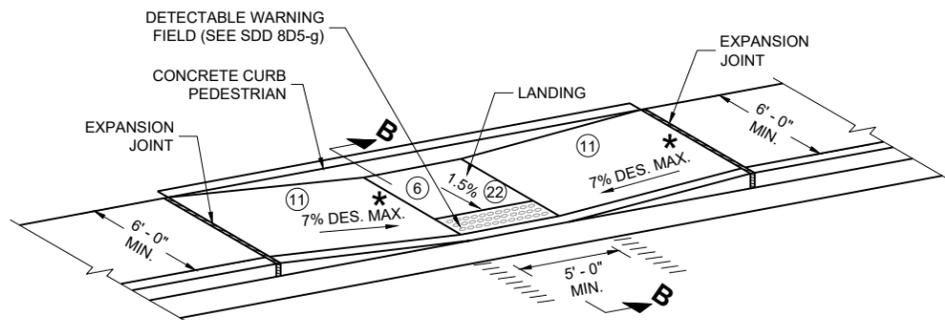
AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF CURB RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

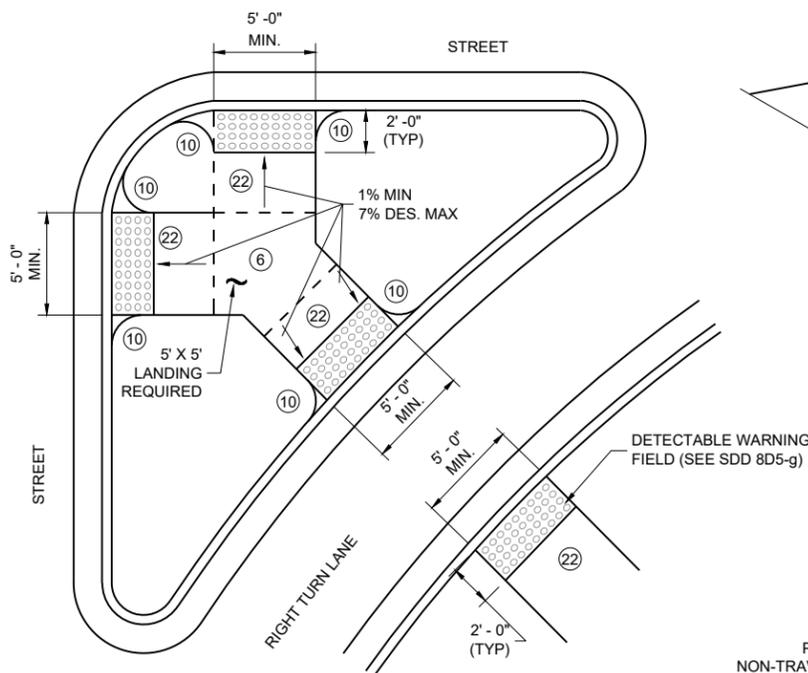
SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2.1%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- ② GRADE CHANGE BETWEEN GUTTER COUNTER SLOPE AND THE CURB RAMP SLOPE IS DESIRABLY 11% OR LESS AND SHALL NOT EXCEED 13.3%. TYPICAL GUTTER COUNTER SLOPE IS 4% BUT MAY BE MODIFIED TO FIT FIELD CONDITIONS. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5%, DESIRABLY 7% OR LESS, AND SHALL NOT EXCEED A MAXIMUM OF 8.3%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.3% CURB RAMP SLOPE IS ALLOWABLE WITH GUTTER COUNTER SLOPE OF 5% MAXIMUM AND A 13.3% MAXIMUM GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2.1% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A 5 FOOT BY 5 FOOT LANDING. SLOPE PERPENDICULAR TO CURB SHALL BE 2.1% MAXIMUM. SLOPE PARALLEL TO CURB SHALL MATCH THE CURB AND GUTTER LONGITUDINAL SLOPE.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- ⑪ SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- ⑫ THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK/PATH. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD TRACK IS 15 FEET MAXIMUM AND 12 FEET MINIMUM, 15 FEET TYPICAL FROM THE NEAREST RAIL.
- ⑬ DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STEEL-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.
- ⑰ A MAXIMUM 2-INCH CONCRETE BORDER IS PERMITTED ALONG ALL SIDES OF THE DETECTABLE WARNING FIELD SURFACE.
- ⑱ WHEN THE DISTANCE BETWEEN THE BACK OF CURBS IS LESS THAN 6 FEET BUT THE FACE OF CURB TO FACE OF CURB DISTANCE IS 6 FEET OR GREATER THEN THE DETECTABLE WARNING FIELDS MAY BE MOVED SO THAT THE EDGE OF THE WARNING FIELD IS PLACED AT THE GUTTER FLOWLINE. MAINTAIN A MINIMUM OF TWO FEET BETWEEN DETECTABLE WARNING FIELD PANELS.
- ⑳ THE ENTIRE RAMP SHALL BE A PLANAR SURFACE. DO NOT WARP THE RUNNING SLOPE OR CROSS SLOPE OF THE RAMP. WARPING OF THE SIDEWALK CROSS SLOPE SHALL TAKE PLACE BETWEEN THE LANDING AND MATCH POINT.

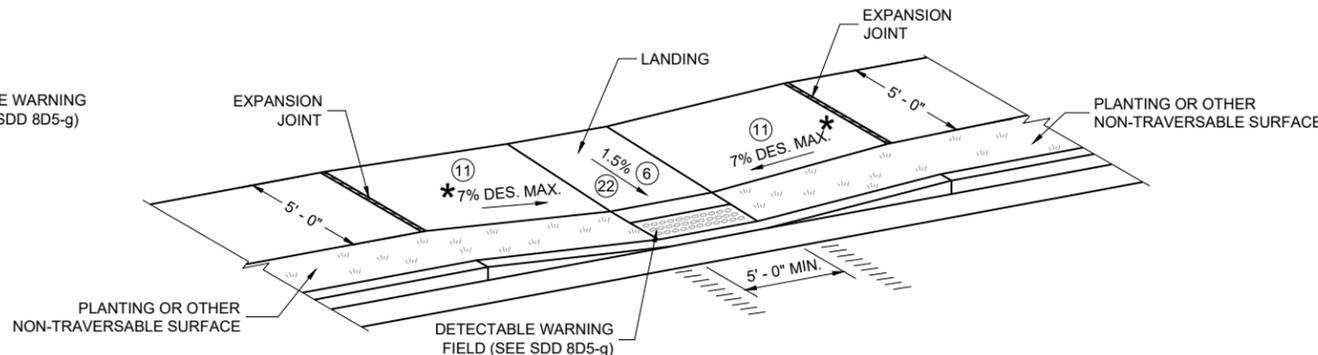


CURB RAMP TYPE 7A
FOR INTERSECTIONS AND
MID BLOCK CROSSINGS



CURB RAMP TYPE 6
DETECTABLE WARNING AT ISLANDS

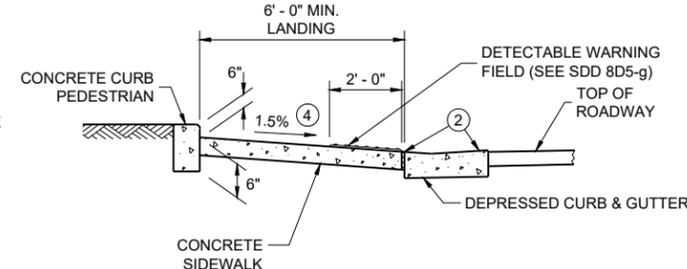
REFER TO GENERAL NOTES ② AND ③
 FOR ALL ISLAND CURB RAMPS



CURB RAMP TYPE 7B
FOR INTERSECTIONS AND
MID BLOCK CROSSINGS

LEGEND

- ½" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT FIELD LOCATED
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)
- * MAXIMUM 8.3%
- ** 1% MINIMUM (PROVIDE DRAINAGE)
- *** DETAILS TO BE DETERMINED BY ENGINEER
- **** FOR SHARED USE PATHS, WIDTH MUST BE AS WIDE AS THE CROSSWALK



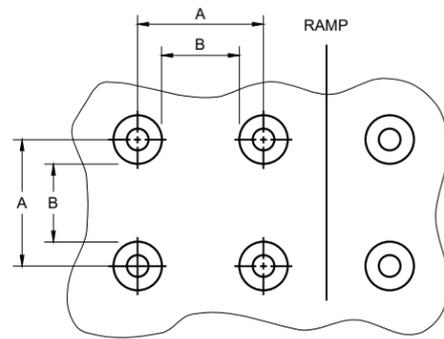
SECTION B - B FOR TYPE 7A

CURB RAMPS
TYPE 5, 6, 7A, 7B & 8

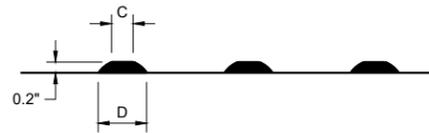
STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

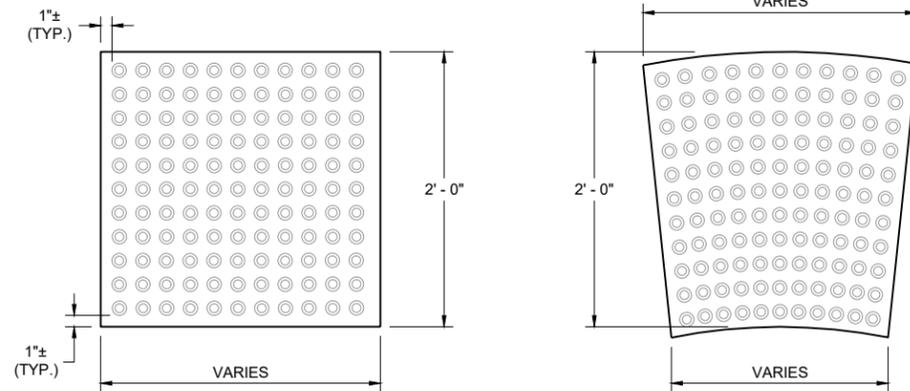


PLAN VIEW



ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**

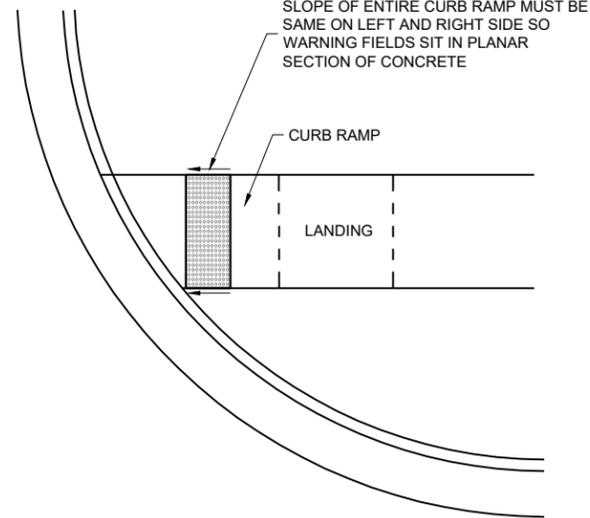


**RECTANGULAR
PLATES**

**RADIAL
PLATES**

PLAN VIEW

DETECTABLE WARNING FIELDS (TYPICAL)

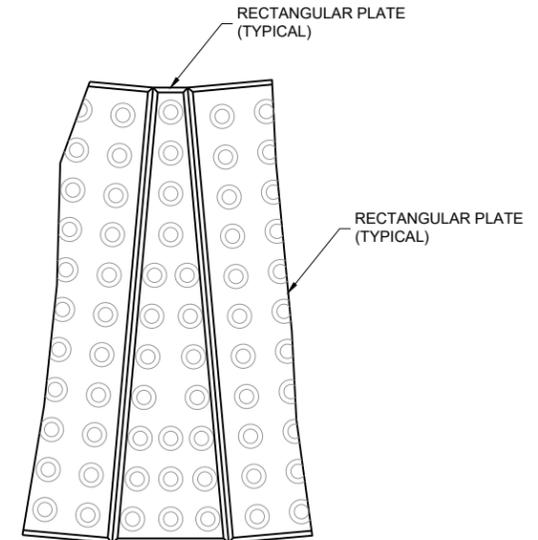


**DETECTABLE WARNING FIELD
PLANAR INSTALLATION**

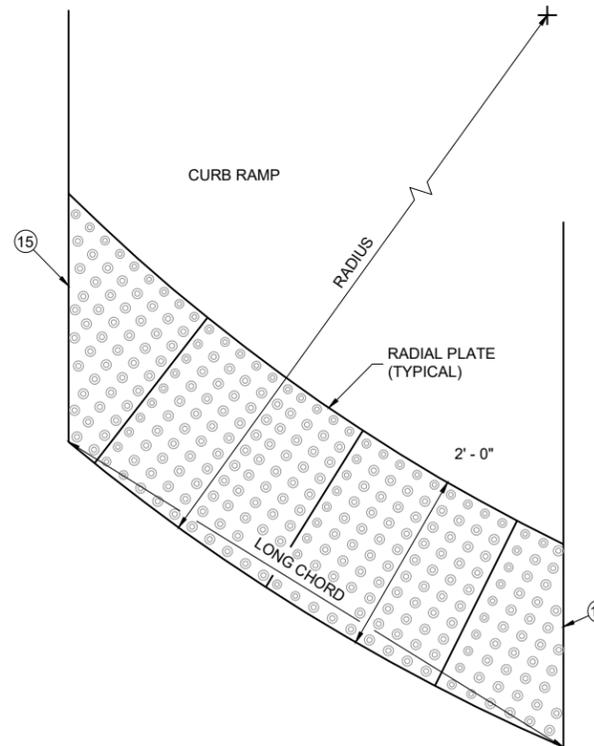
GENERAL NOTES

- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.
- PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.
- REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.
- DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.



**PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL**



**PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES**

**CURB RAMPS
RECTANGULAR AND RADIAL
DETECTABLE WARNING PLATES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2025 /S/ Rodney Taylor
DATE <position>

FHWA

GENERAL NOTES

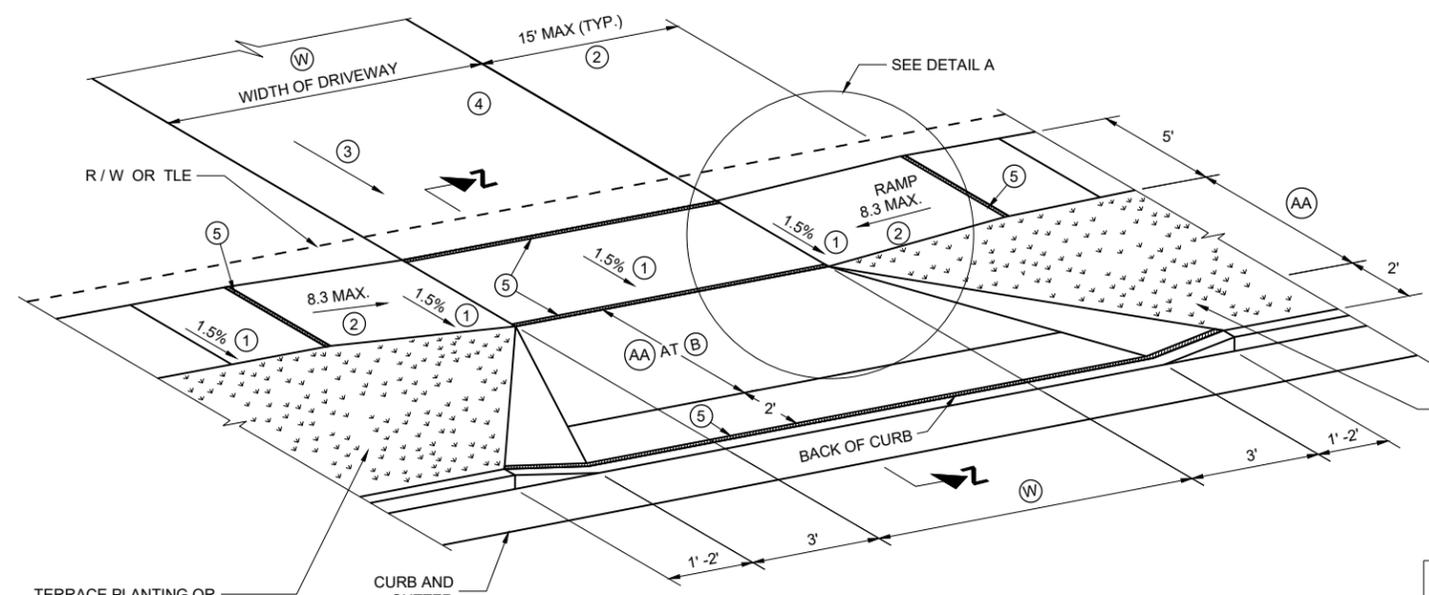
PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.

(W) IS SHOWN ON PLAN AND PROFILE SHEETS.

OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.

- ① CONSTRUCTION TOLERANCE OF 0.5%± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2.1%.
- ② THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY.
- ③ DRIVEWAY SLOPES: DESIRABLE MAXIMUM
10.5% UP AWAY FROM SIDEWALK (SAG)
8.5% DOWN AWAY FROM SIDEWALK (CREST)
ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG
- ④ DRIVEWAY TYPES
* 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE 2-INCH
* TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE 6-INCH BASE
* AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES.)
- ⑤ 1/2" EXPANSION JOINT FILLER.
- ⑥ DETECTABLE WARNING FIELDS ARE REQUIRED WHEN A PEDESTRIAN CIRCULATION ROUTE CROSSES A DRIVEWAY THAT IS TRAFFIC SIGNAL, STOP, OR YIELD CONTROLLED. DETECTABLE WARNING FIELDS TO BE 2 FT DEEP AND EXTEND THE WIDTH OF THE PEDESTRIAN CIRCULATION ROUTE.

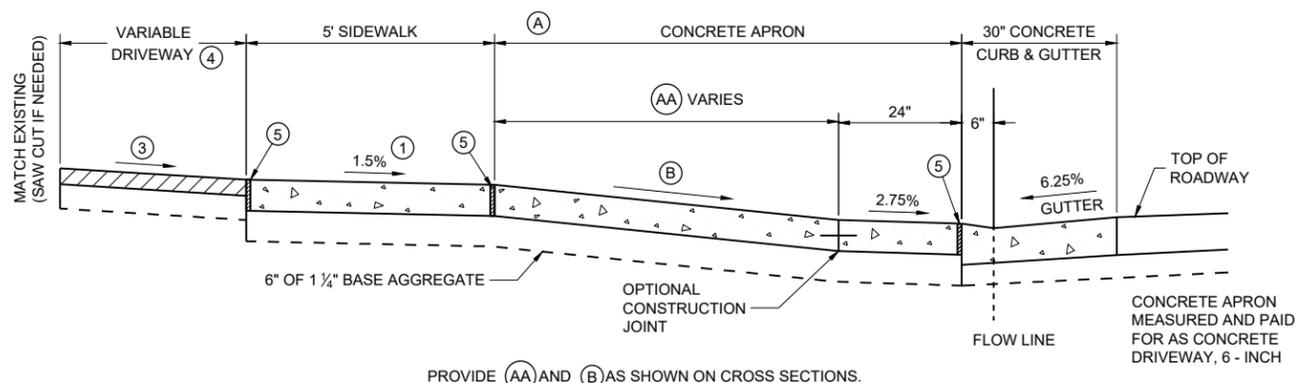
(W): 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
16' MIN. - 35' MAX. COMMERCIAL (CE)



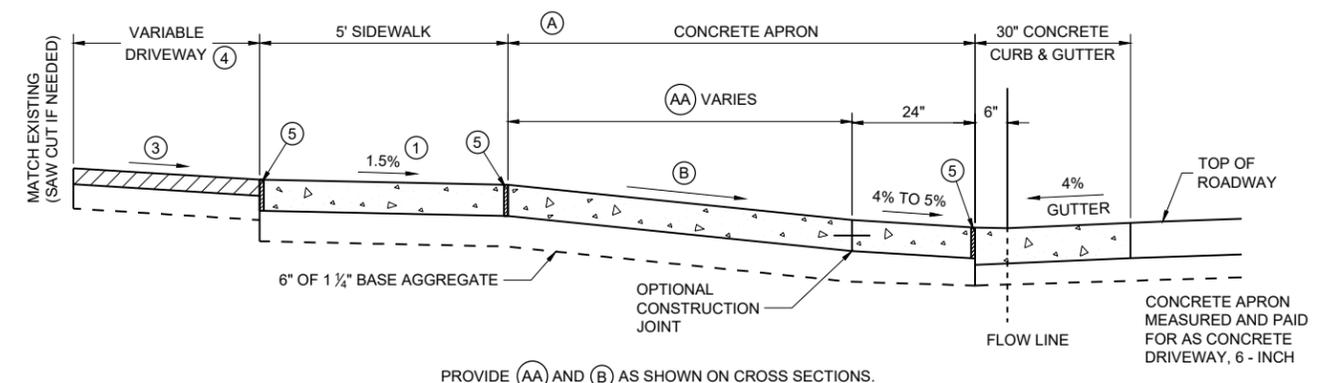
**TYPE Z
SIDEWALK WITH WIDER TERRACE
TERRACE VARIES 7 TO 12 FEET**

TABLE Z

(AA) FEET	(B) % 6.25% GUTTER	(B) % 4% GUTTER
4.5'	11.5%	9% TO 11.5%
5.5'	9% TO 11.5%	8% TO 11.5%
6.5'	8% TO 11.5%	6% TO 11.5%
7.5'	7% TO 11.5%	6% TO 11.5%
8.5'	6% TO 11.5%	5% TO 11.5%
9.5'	5% TO 11.5%	4% TO 11.5%



6.25% GUTTER SLOPE



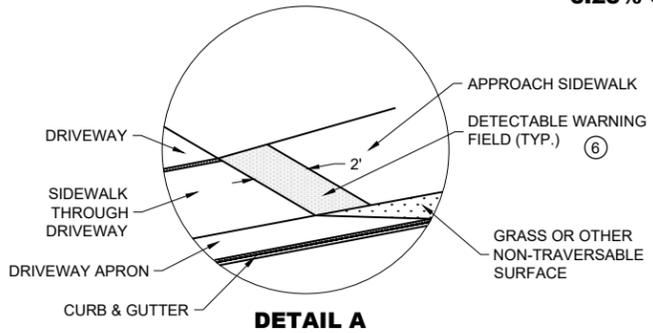
4% GUTTER SLOPE

PROVIDE (AA) AND (B) AS SHOWN ON CROSS SECTIONS.

PROVIDE (AA) AND (B) AS SHOWN ON CROSS SECTIONS.

NOTE: SIDEWALK MAY BE DEPRESSED IN DRIVEWAY AREAS FOR (B) VALUES NOT SHOWN IN TABLE Z.
SIDEWALK WITHIN THE LIMITS OF THE DRIVEWAY PAID FOR AS CONCRETE DRIVEWAY 6-INCH.

**SECTION Z - Z
DRIVEWAY DETAIL WITH CONCRETE CURB AND GUTTER
(URBAN AND SUBURBAN)**

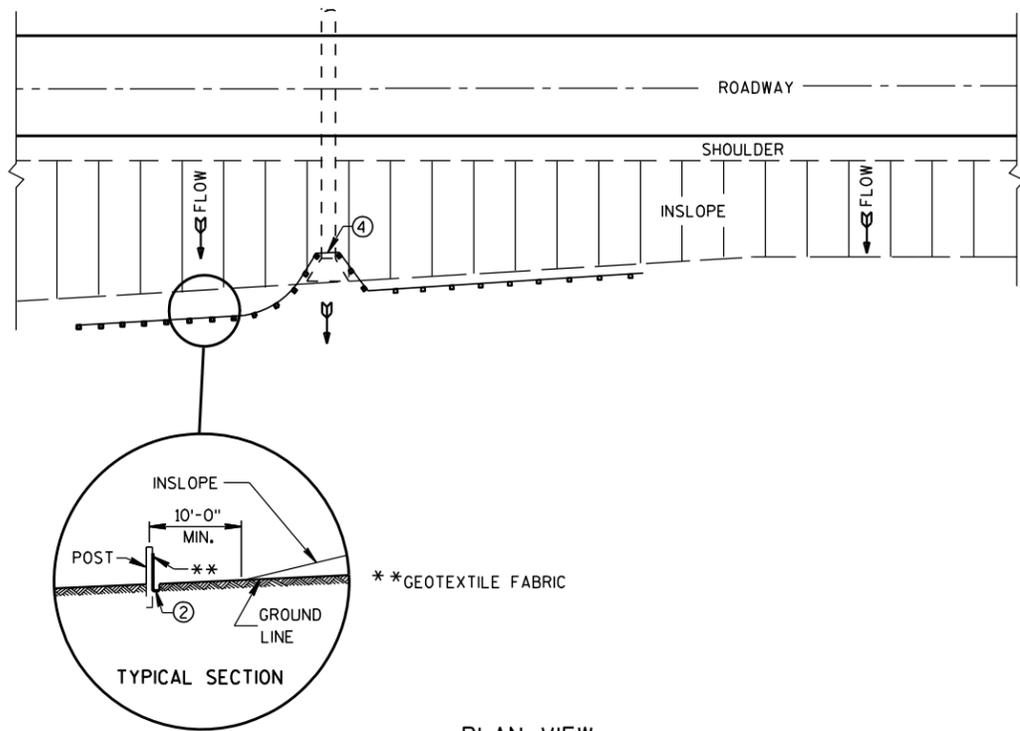


DETAIL A

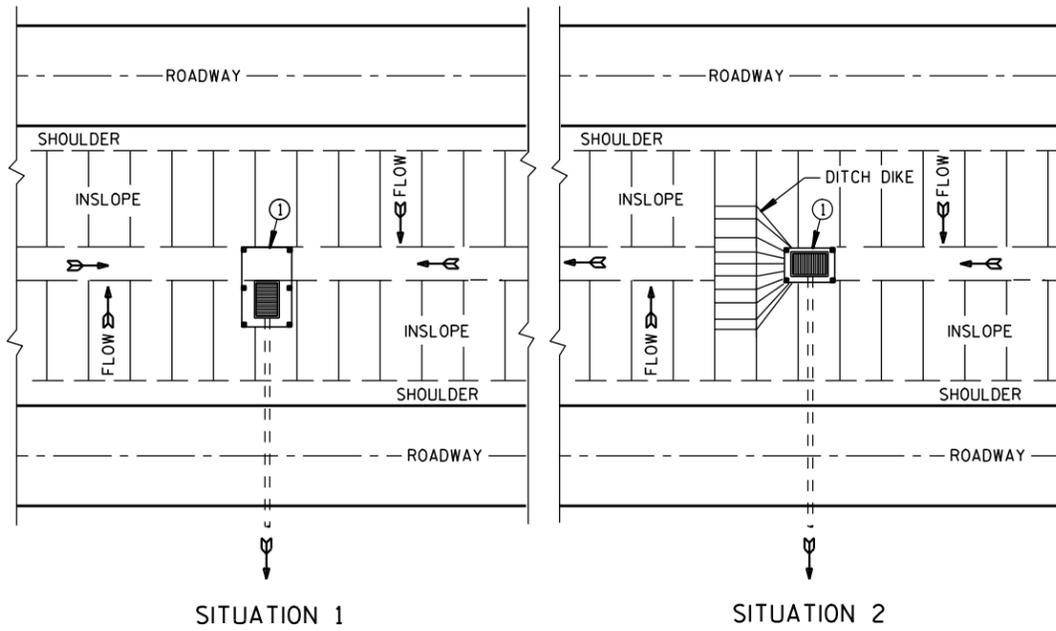
**DRIVEWAY AND
SIDEWALK RAMPS
TYPE Z**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2023 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

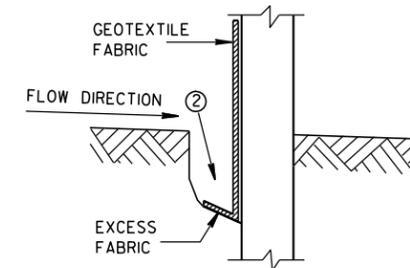


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

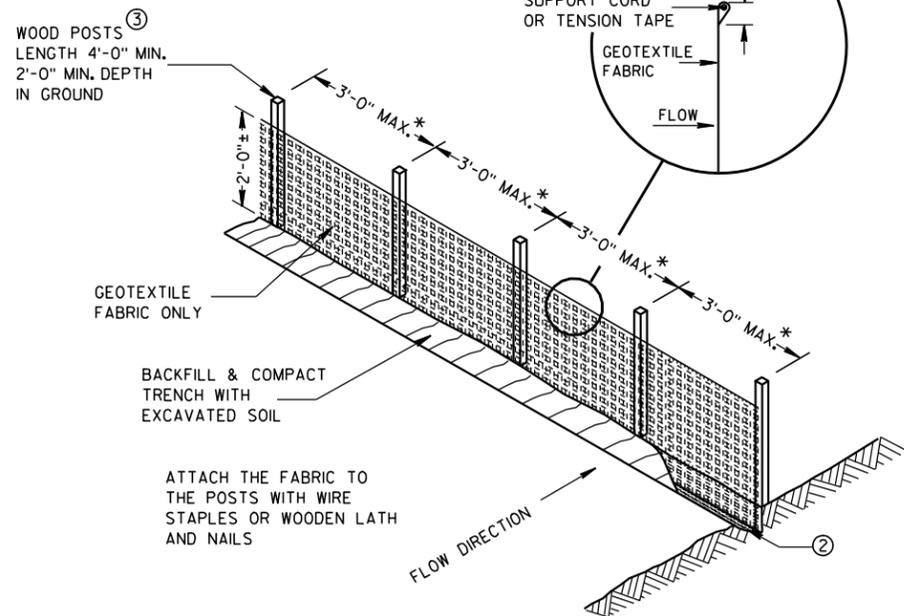
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



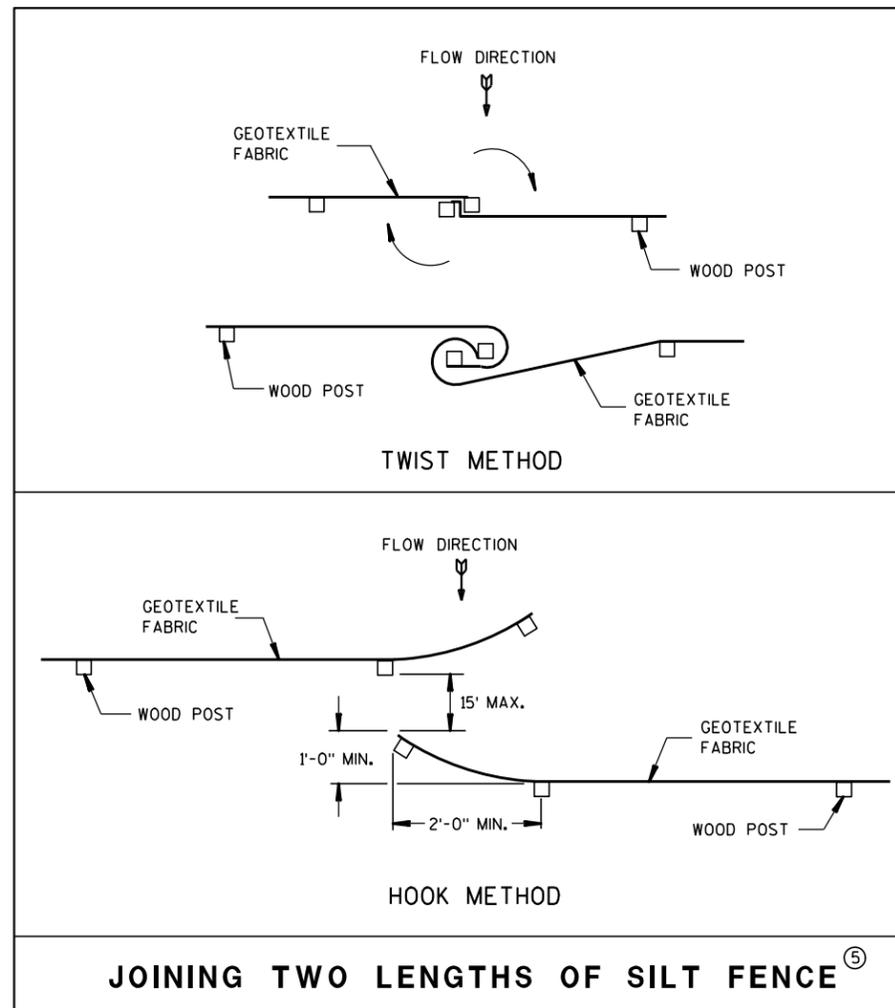
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

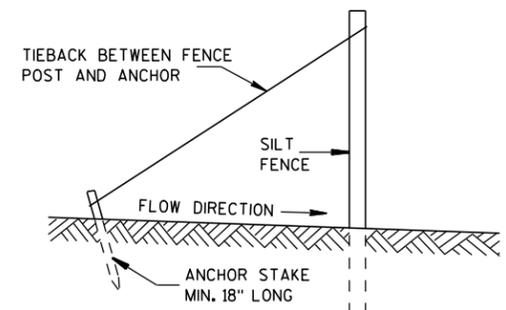


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.



JOINING TWO LENGTHS OF SILT FENCE ⑤



SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

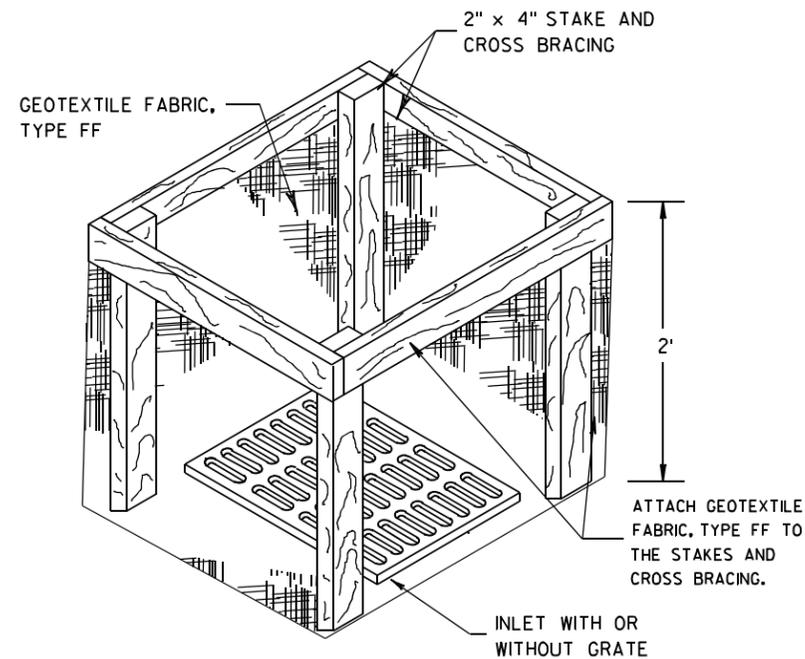
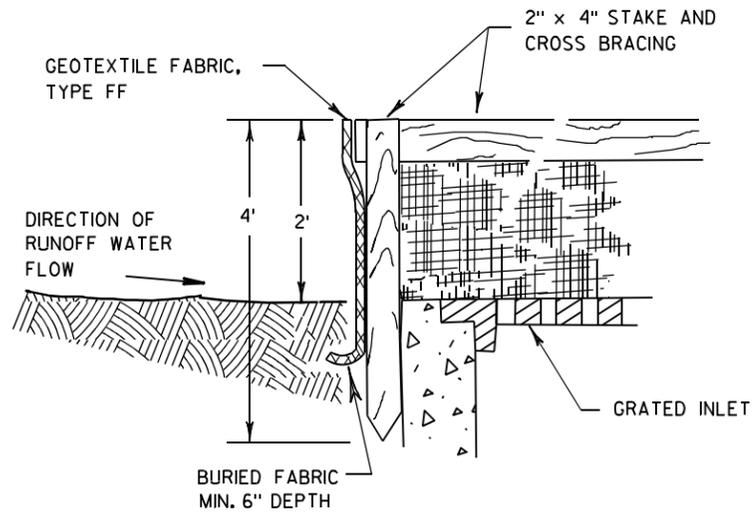
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

4-29-05
DATE

FHWA

/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER



INLET PROTECTION, TYPE A

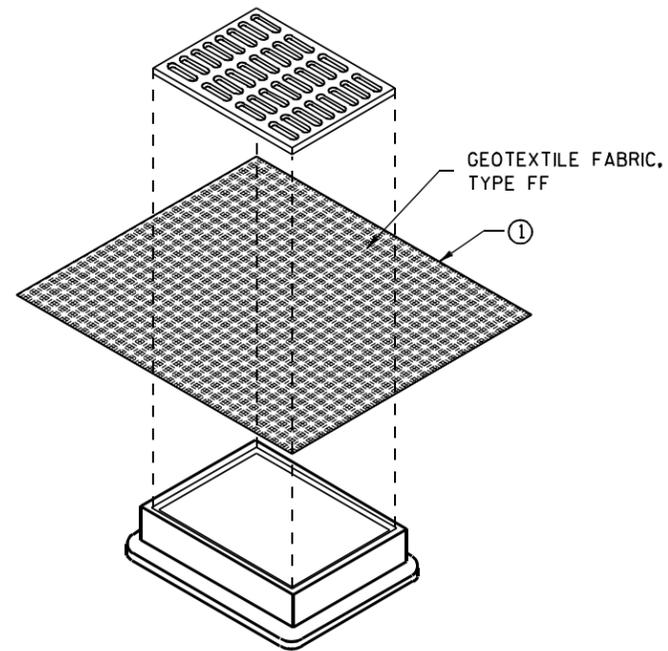
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

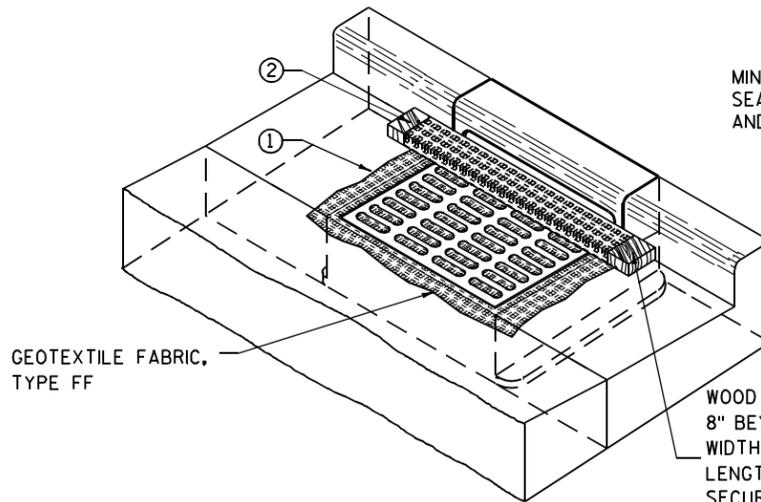
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

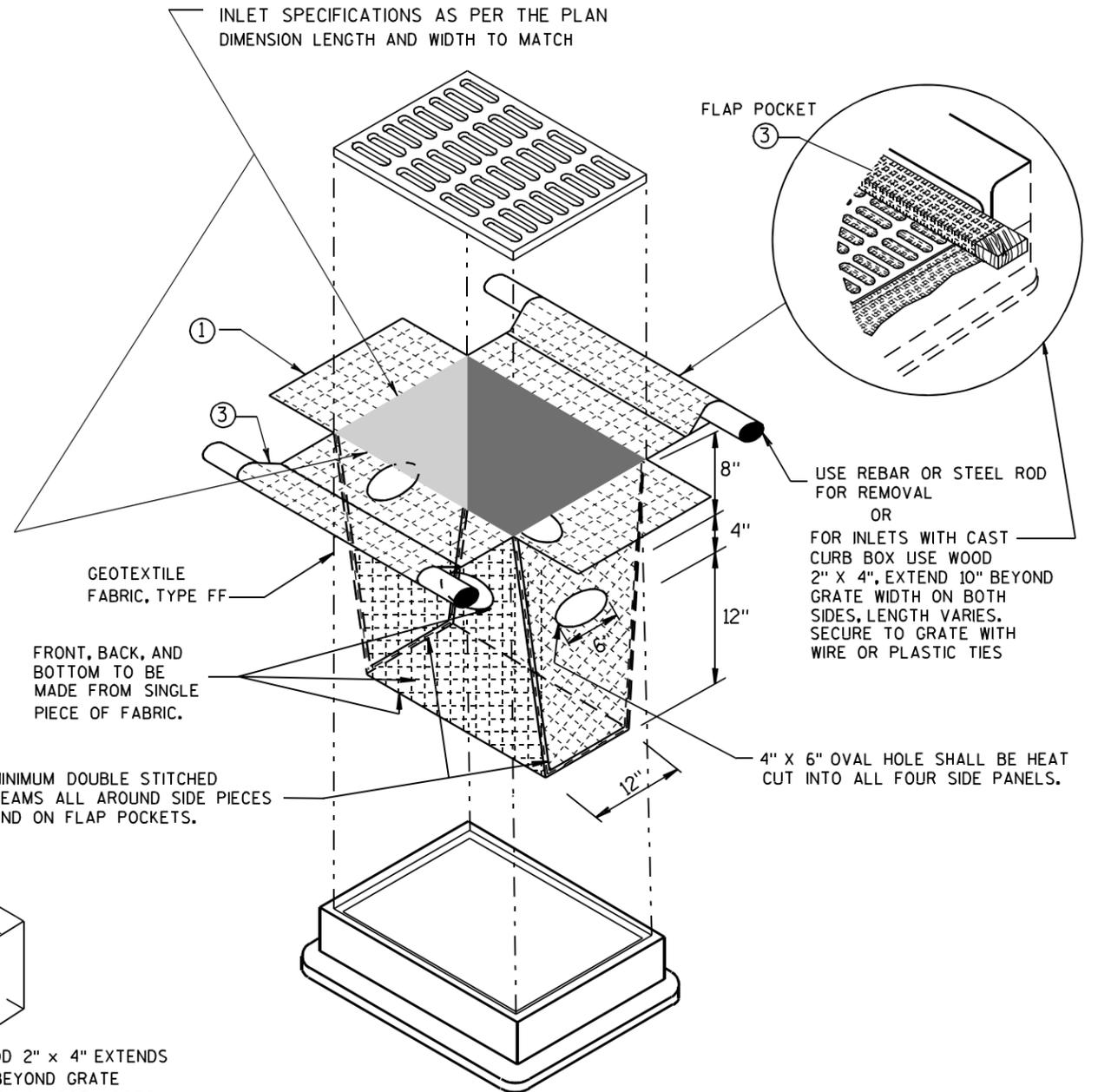
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



INLET PROTECTION, TYPE D

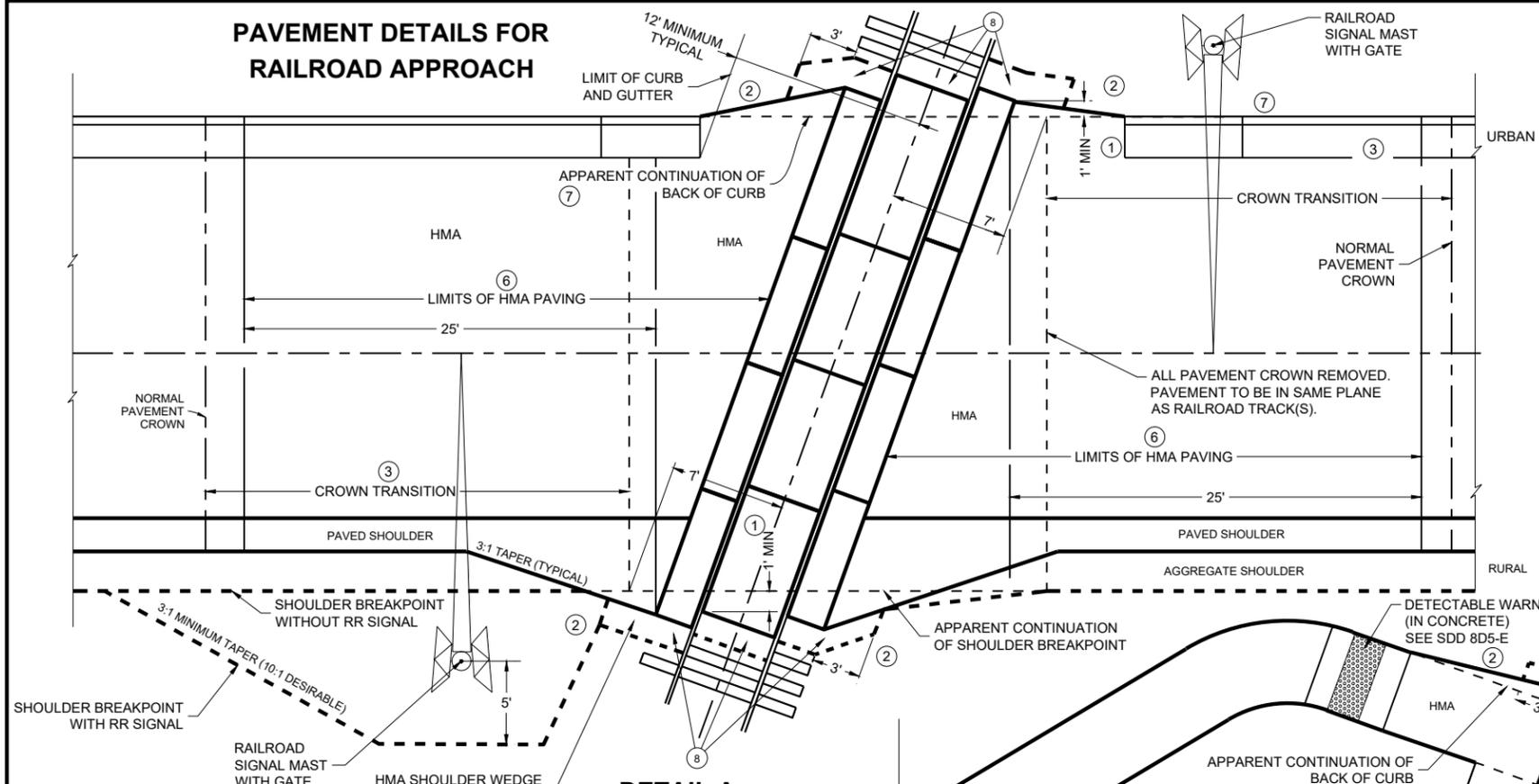
(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

**INLET PROTECTION
TYPE A, B, C, AND D**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02 /S/ Beth Connestra
DATE
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

PAVEMENT DETAILS FOR RAILROAD APPROACH



**DETAIL A
RAILROAD APPROACH**

GENERAL NOTES

PLANS AND SECTIONS ARE TYPICAL. DIMENSIONS VARY PER PROJECT.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, PROJECT PLANS, AND THE APPLICABLE SPECIAL PROVISIONS.

CROSSING SURFACE MATERIAL, RAILS, TIES, BALLAST, AND CROSSING DRAINAGE SYSTEM BY OTHERS UNLESS DIRECTED OTHERWISE. IF THE FINAL GRADES DON'T MATCH TO THE PLAN GRADES THEN GRADE ADJUSTMENTS WILL BE NECESSARY. CONFIRM NEW GRADES WITH PROJECT ENGINEER.

HMA PAVEMENT APPROACHES, HMA PAVEMENT CROSSING SURFACES, AND HMA FLANGWAY/FIELD FILLERS TO BE REPLACED BY ROADWAY CONTRACTOR UNLESS DIRECTED OTHERWISE BY THE PLANS, SPECIAL PROVISIONS, RAILROAD ENGINEER, OR PROJECT ENGINEER.

HMA PAVEMENT SHALL BE ROLLED PARALLEL TO THE TRACK.

WHEN THERE IS A SIDEWALK OR SHARED-USE PATH, ADD DETECTABLE WARNING FIELDS PER CURRENT STANDARD DETAIL DRAWING 8D5-E.

THE CROSSING SHALL NOT BE OPENED TO ANY TYPE OF TRAFFIC UNTIL IT IS FULLY PAVED AND COOLED SUFFICIENTLY UNLESS OTHERWISE APPROVED BY THE RAILROAD ENGINEER AND THE PROJECT ENGINEER.

NO NON-RUBBER TIRED OR TRACKED EQUIPMENT SHALL CROSS OR SIT ON THE CROSSING SURFACE WITHOUT PROTECTING THE CROSSING SURFACE WITH A METHOD APPROVED BY THE RAILROAD ENGINEER AND PROJECT ENGINEER.

PLACE BASE AGGREGATE DENSELY AROUND SIGNAL BASE. COORDINATE WITH THE RAILROAD ENGINEER.

HMA SHOULDER WEDGE TO END OF CROSSING
DETECTABLE WARNING FIELD (IN CONCRETE) SEE SDD 8D5-E

15' MIN TYPICAL TO TRACK CENTERLINE
CONCRETE OR HMA PAVEMENT

DETECTABLE WARNING FIELD (IN CONCRETE) SEE SDD 8D5-E

RAILROAD SIGNAL MAST WITH GATE

CANTILEVERED RAILROAD SIGNAL MAST

LIMIT OF CURB AND GUTTER

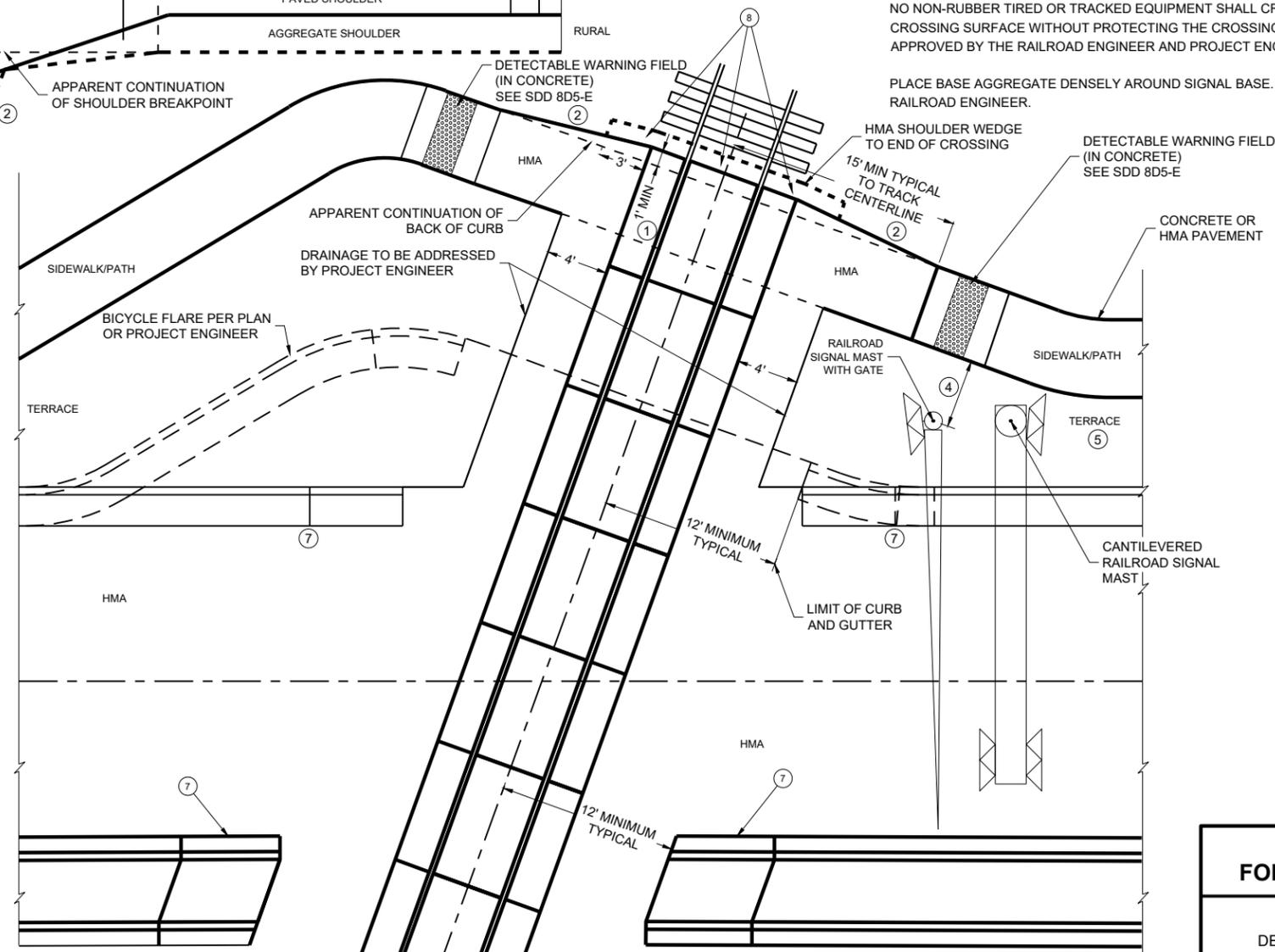
12' MINIMUM TYPICAL

6

6

GENERAL NOTES CONTINUED

- ① 1' MINIMUM CROSSING SURFACE COVERAGE PAST THE APPARENT CONTINUATION OF SHOULDER BREAKPOINT, BACK OF CURB, OR OUTSIDE EDGE OF SIDEWALK/PATH. INDIVIDUAL RAILROADS MAY HAVE DIFFERENT MINIMUM STANDARDS.
- ② HMA FLARE FROM OUTSIDE EDGE OF SIDEWALK/PATH, BACK OF CURB, OR AGGREGATE SHOULDER BREAKPOINT TO THE END OF CROSSING SURFACE MATERIAL.
- ③ CROWN TRANSITION LENGTH SHOWN ELSEWHERE IN THE PLAN.
- ④ NEAR EDGE OF PATH TO THE CENTER OF SIGNAL OR GATE MAST SHOULD BE A MINIMUM OF 5'-0". FOR SIDEWALK, THE NEAR EDGE SHOULD BE A MINIMUM OF 3'-0" TO THE CENTER OF SIGNAL OR GATE. NEAR EDGE OF SIDEWALK TO A NON-GATED MAST OR CANTILEVER SHOULD BE A MINIMUM OF 2'-6". SEE PLAN FOR RAILROAD SIGNAL AND GATE LOCATION IF THEY ARE NOT ALREADY INSTALLED.
- ⑤ TERRACE WIDTH VARIES. SEE PLAN FOR RAILROAD SIGNAL AND GATE LOCATIONS. PER PLAN OR PROJECT ENGINEER THE TERRACE AND SIDEWALK/PATH GRADES SHALL BE TRANSITIONED TO MATCH THE GRADE OF THE TRACK. FIELD FIT TO AVOID PONDING.
- ⑥ 25' MINIMUM HMA PAVING MEASURED PARALLEL TO THE ROAD OR 10' MINIMUM MEASURED PERPENDICULAR TO THE TRACK FROM THE EDGE OF THE CROSSING SURFACE, WHICHEVER IS GREATER.
- ⑦ REFERENCE SDD 8-D-01 END SECTION CURB AND GUTTER. MEDIAN END NEAR THE TRACK SHOULD BE PARALLEL TO THE TRACK. 6'-0" TAPER FOR A MEDIAN SHOULD BE REDUCED TO GET FULL HEIGHT CURB WHERE THE GATE COMES DOWN. DESIGN OPTION TO POUR MEDIAN TAPER IN ONE PIECE. BUILD PER PLAN UNLESS OTHERWISE APPROVED BY THE RAILROAD ENGINEER AND THE PROJECT ENGINEER.
- ⑧ IF METAL END PLATES ARE NOT INSTALLED BY THE RAILROAD THEN HMA PAVEMENT WEDGE SHALL BE PLACED AT THE END OF THE LAST PANEL TAPERED TO BACK EDGE OF NEXT TIE AND THOROUGHLY COMPACTED. SEE DETAIL G.



**DETAIL B
MEDIAN AND SIDEWALK/SHARED-USE PATH APPROACH**

PAVEMENT DETAILS FOR RAILROAD APPROACH

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

May 2023
DATE

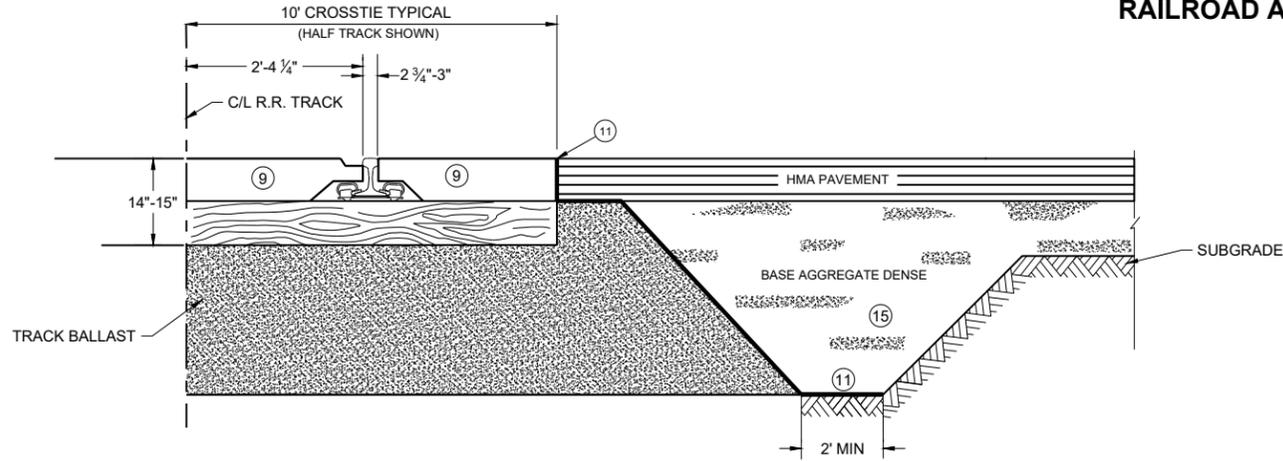
/s/ Kristen Sommers
STATE RAILROAD ENGINEERING
AND SAFETY SUPERVISOR

FHWA

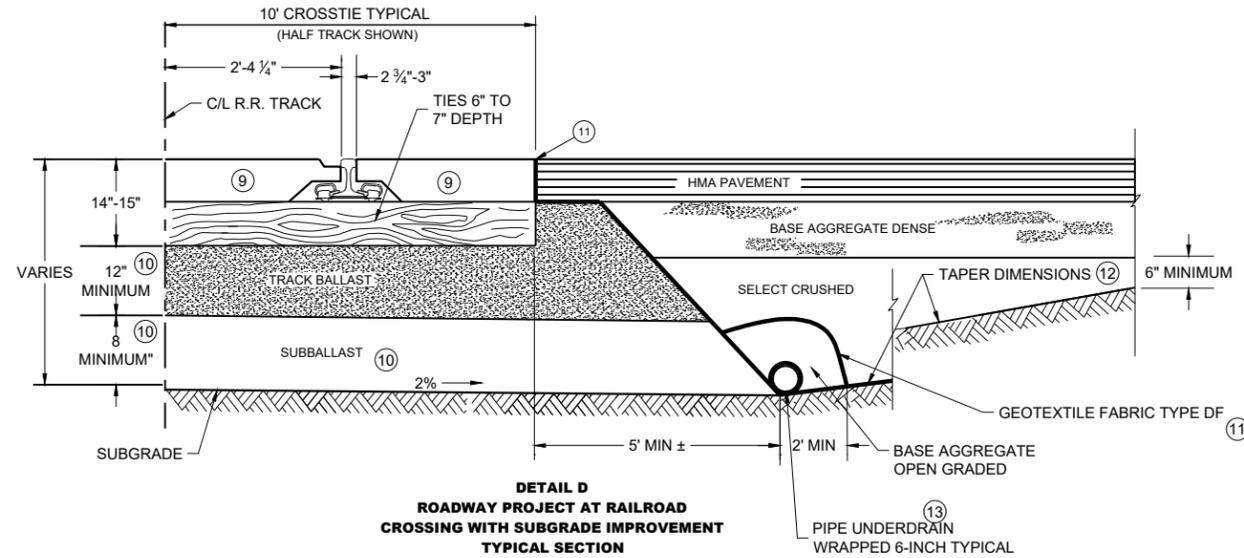
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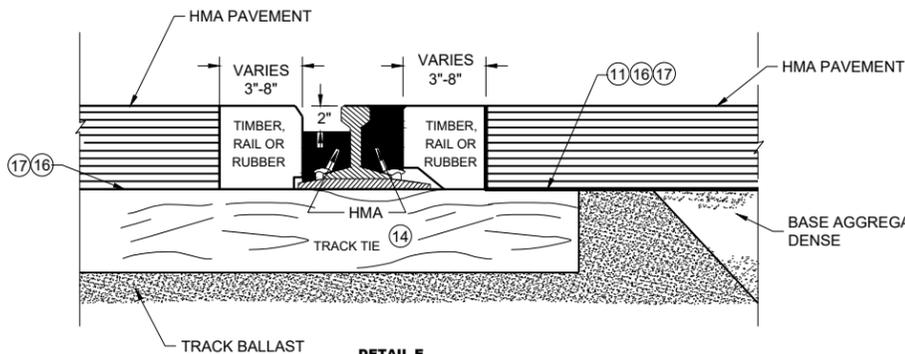
TYPICAL SECTIONS FOR RAILROAD APPROACH



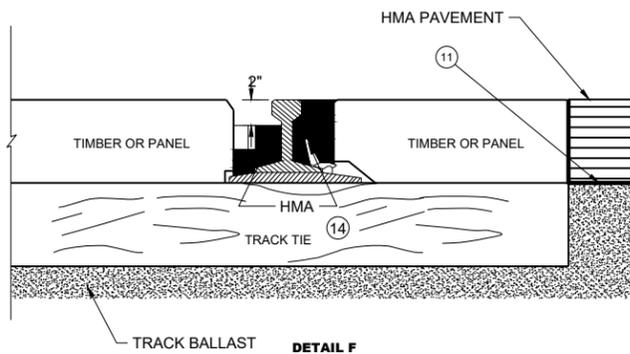
**DETAIL C
ROADWAY PROJECT AT RAILROAD
CROSSING WITHOUT SUBGRADE IMPROVEMENT
TYPICAL SECTION**



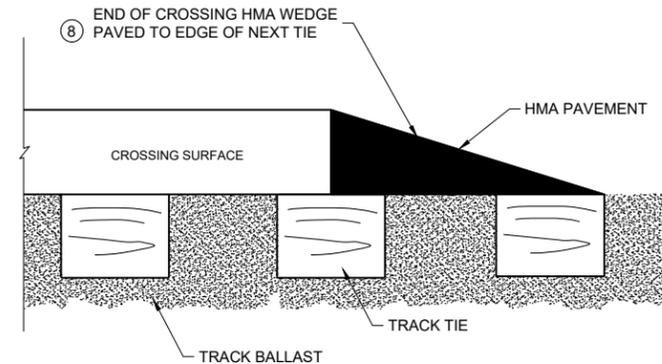
**DETAIL D
ROADWAY PROJECT AT RAILROAD
CROSSING WITH SUBGRADE IMPROVEMENT
TYPICAL SECTION**



**DETAIL E
TIMBER, RAIL OR
RUBBER SECTION
HMA FLANGEWAY
AND FIELD FILLERS**



**DETAIL F
PANEL SECTION
HMA FLANGEWAY
AND FIELD FILLERS**



**DETAIL G
END OF CROSSING HMA WEDGE**

GENERAL NOTES

- 8 IF METAL END PLATES ARE NOT INSTALLED BY THE RAILROAD THEN HMA PAVEMENT WEDGE SHALL BE PLACED AT THE END OF THE LAST PANEL TAPERED TO BACK EDGE OF NEXT TIE AND THOROUGHLY COMPACTED. SEE DETAIL A AND B.
- 9 MATCH THE CROSSING TYPE THAT IS INSTALLED UNLESS OTHERWISE DIRECTED BY PROJECT ENGINEER.
- 10 TRACK BALLAST AND SUBBALLAST REQUIRED 12" AND 8" MINIMUM DEPTHS RESPECTIVELY. DIMENSION FROM BOTTOM OF TRACK TIE TO HIGH SIDE OF 2% SLOPE. THE 2% SLOPE IS REQUIRED ON RAILROAD SUBBALLAST. SEE PLAN FOR CROWN, MATERIAL THICKNESS, AND SLOPE DIRECTION. SUBBALLAST CAN BE HMA, 1 1/2" BASE AGGREGATE DENSE, SELECT CRUSHED, OR A COMBINATION OF THEM.
- 11 GEOTEXTILE FABRIC TYPE SAS PLACED IN ORDER TO PROVIDE STABILIZATION AND SEPARATION ON TOP OF THE TRACK BALLAST WHERE IT IS UNDER HMA PAVEMENT, BASE AGGREGATE DENSE OR SELECT CRUSHED MATERIAL AND THE FIELD SIDE BALLAST CRIBS. GEOTEXTILE FABRIC TYPE DF PLACED IN ORDER TO PROVIDE STABILIZATION AND SEPARATION UNDER AND AROUND THE PIPE UNDERDRAIN. PLACING GEOTEXTILE FABRIC OR GEOGRID UNDER THE SUBBALLAST IS OPTIONAL.
- 12 TAPER DIMENSIONS PROVIDED BY PLAN OR BY PROJECT ENGINEER.
- 13 IF SHOWN ON THE PLAN, TYPICAL 6-INCH PERFORATED PVC SCHEDULE 80 PIPE UNDERDRAIN TO BE PLACED ALONG THE TOE OF SLOPE, GRADED TO DRAIN AND DAYLIGHT OR INTO STORM SEWER. BASE AGGREGATE OPEN GRADED OVER PIPE UNDERDRAIN AND THEN WRAPPED IN GEOTEXTILE FABRIC TYPE DF SCHEDULE A IN ORDER TO STABILIZE AND SEPARATE FROM SELECT CRUSHED.
- 14 HMA FLANGEWAY AND FIELD FILLERS ARE TO BE PLACED AND THOROUGHLY HAND COMPACTED BY THE CONTRACTOR, WHEN NOT PROVIDED BY OTHERS AS PART OF THE CROSSING SURFACE MATERIAL. IF THE CROSSING SURFACE IS NOT BEING REPLACED, THEN REMOVE AND REPLACE THE HMA FLANGEWAY AND FIELD FILLERS AS DIRECTED BY THE RAILROAD OR PROJECT ENGINEER.
- 15 GRADE TO MATCH EXISTING OR PROPOSED TYPICAL SECTION OF ROADWAY. SEE PLAN OR PROJECT ENGINEER FOR MORE DETAIL. IF NOT NOTED OTHERWISE IN THE PLAN, BACKFILL ANY REMOVED BASE AND SUBGRADE WITH BASE AGGREGATE DENSE.
- 16 IF THE CROSSING IS NOT BEING REPLACED, REMOVE AND REPLACE HMA AS DIRECTED BY RAILROAD AND PROJECT ENGINEER. CARE MUST BE TAKEN TO NOT DAMAGE CROSSING PANELS, TIES, RAIL, PLATES AND SPIKES.
- 17 PLACE HMA FULL DEPTH. AGGREGATE IS NOT TO BE PLACED BETWEEN THE RAILROAD TIES AND THE HMA PAVEMENT.

TYPICAL SECTIONS FOR RAILWAY APPROACH

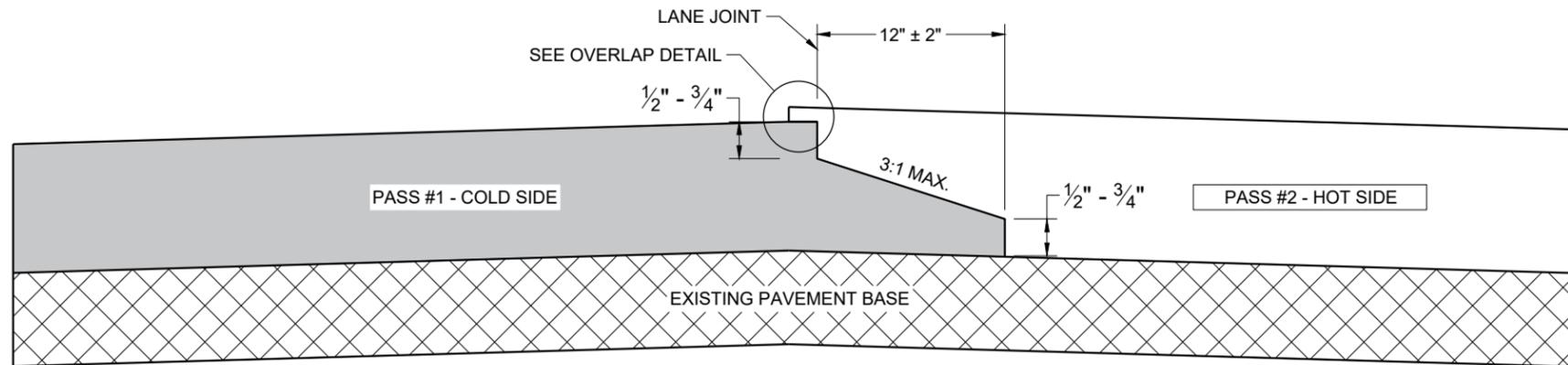
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

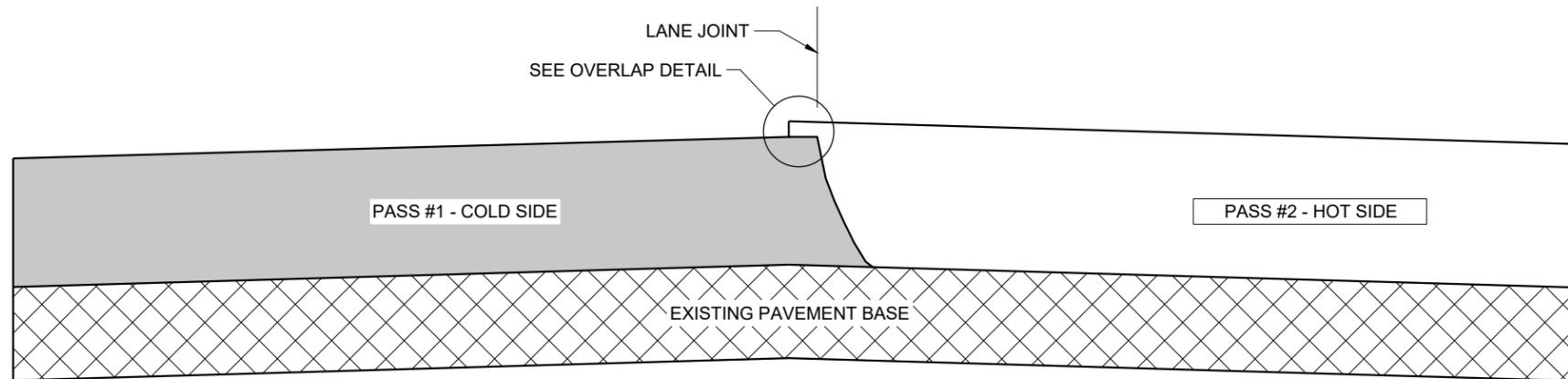
May 2023
DATE

/s/ Kristen Sommers
STATE RAILROAD ENGINEERING
AND SAFETY SUPERVISOR

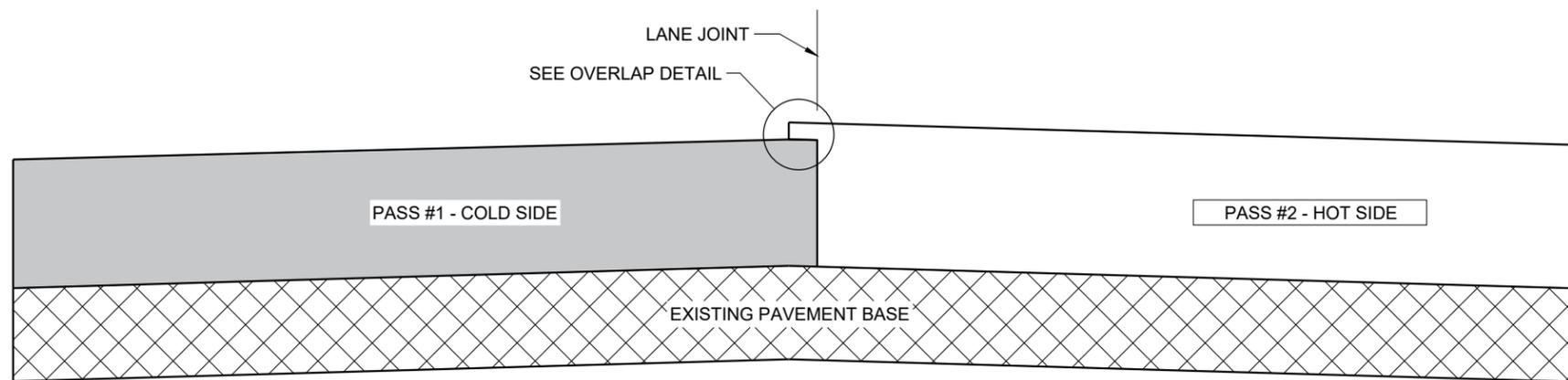
FHWA



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

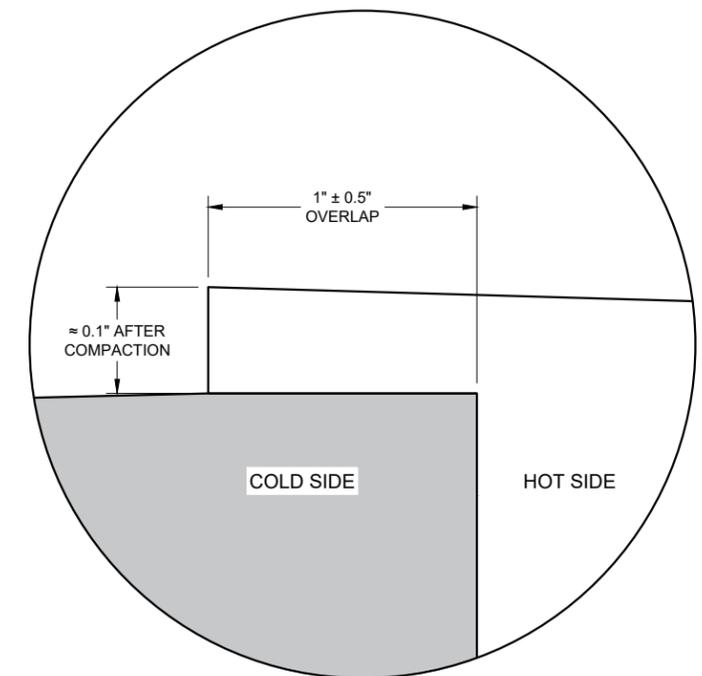
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY $0.1"$ AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO $2"$ FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

6

6

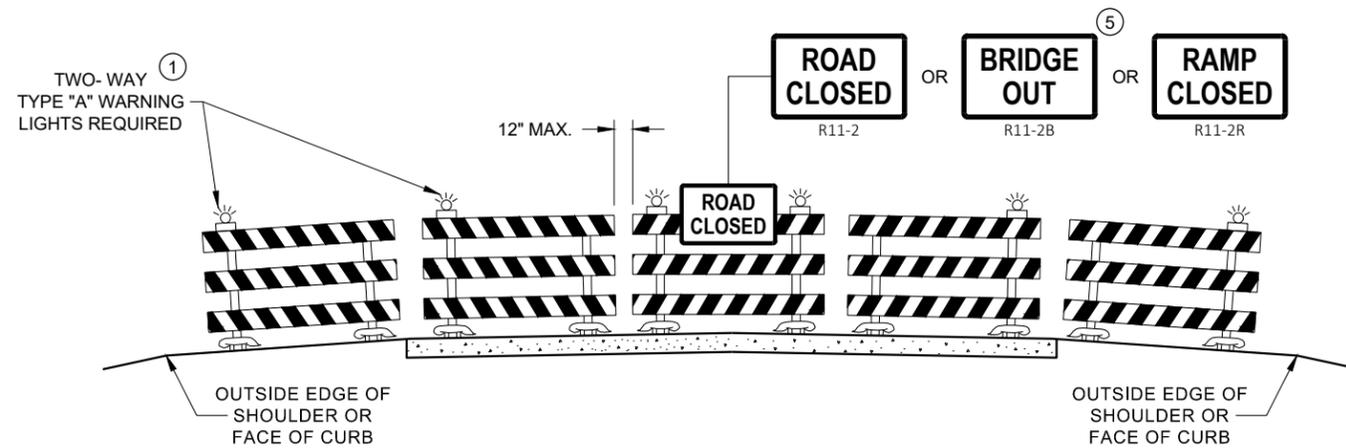
SDD 13C19 - 03

SDD 13C19 - 03

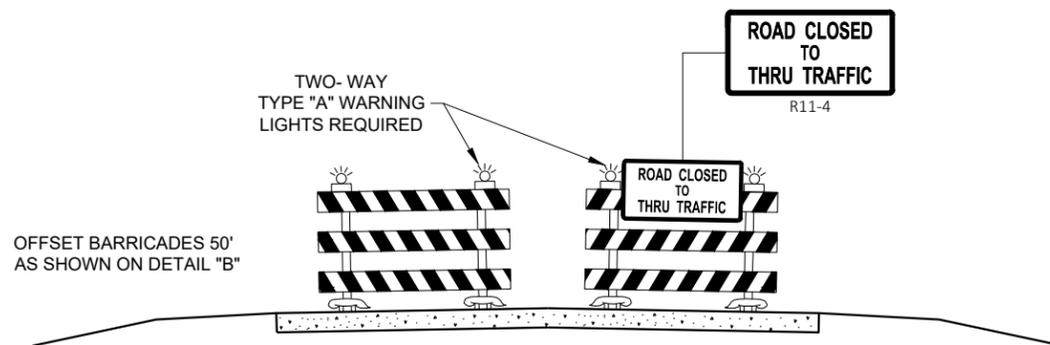
HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2020 DATE /S/ Steven Hefel
HMA PAVEMENT ENGINEER
FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

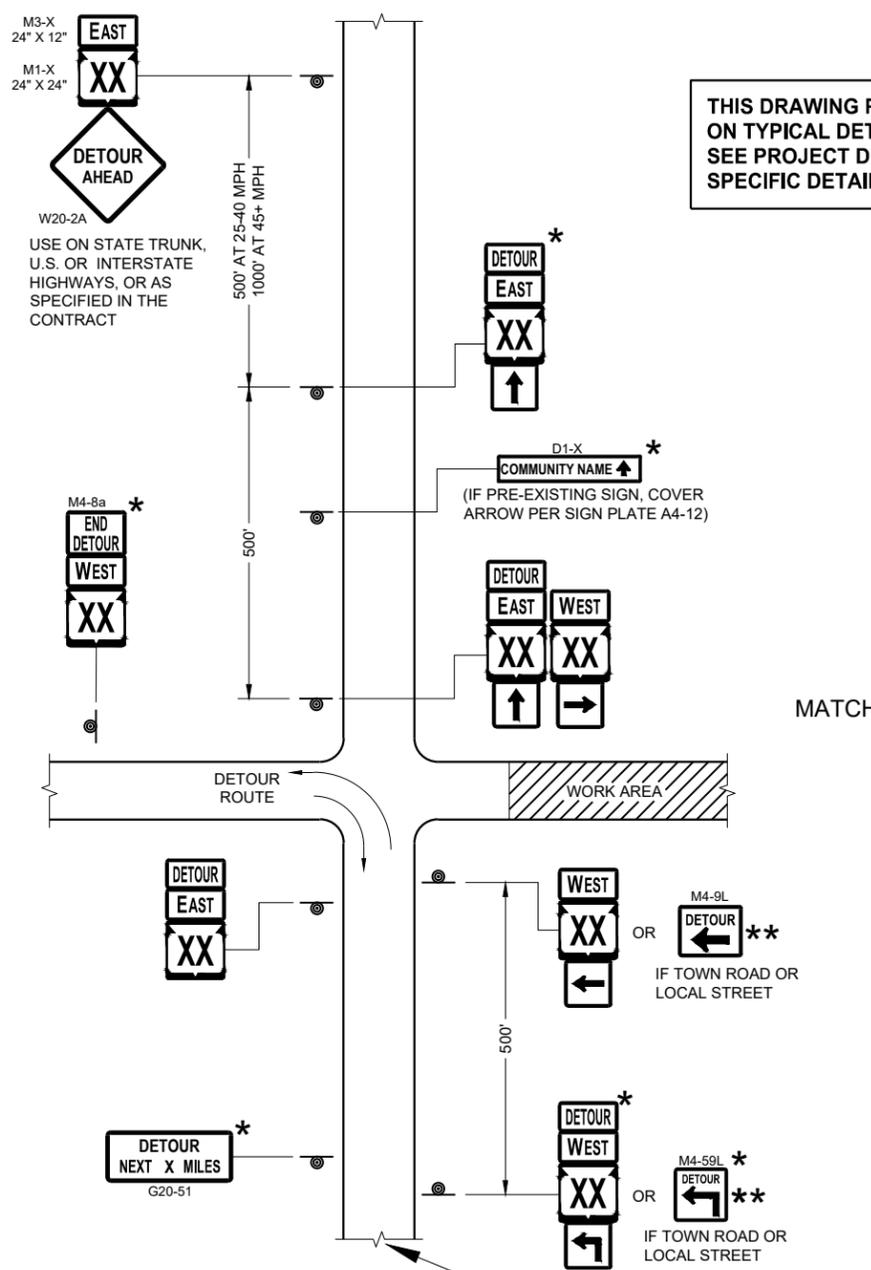
- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA



THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1
- M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

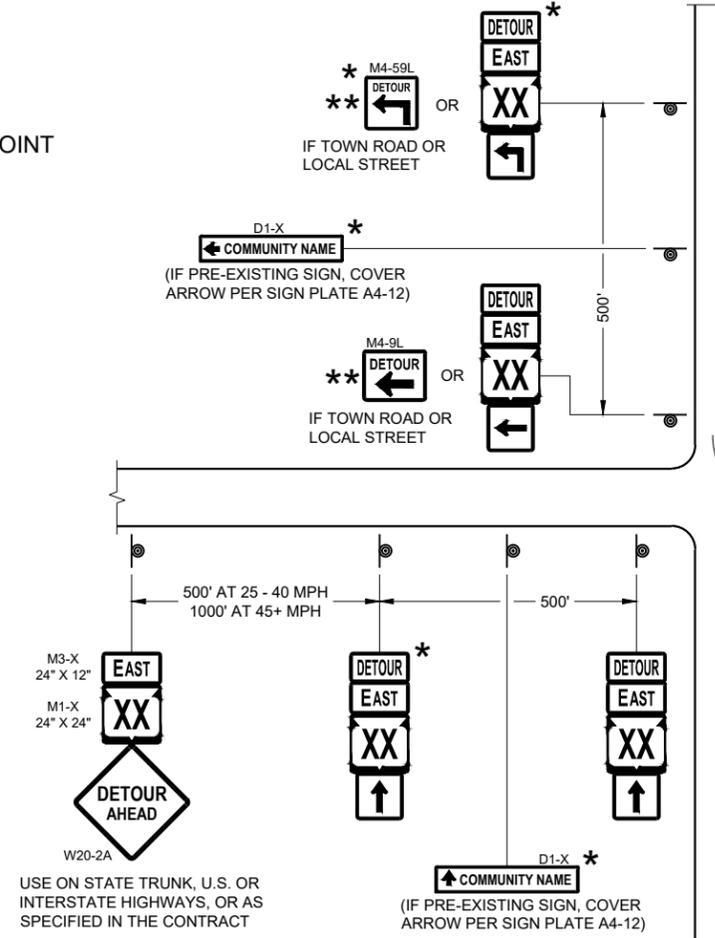
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

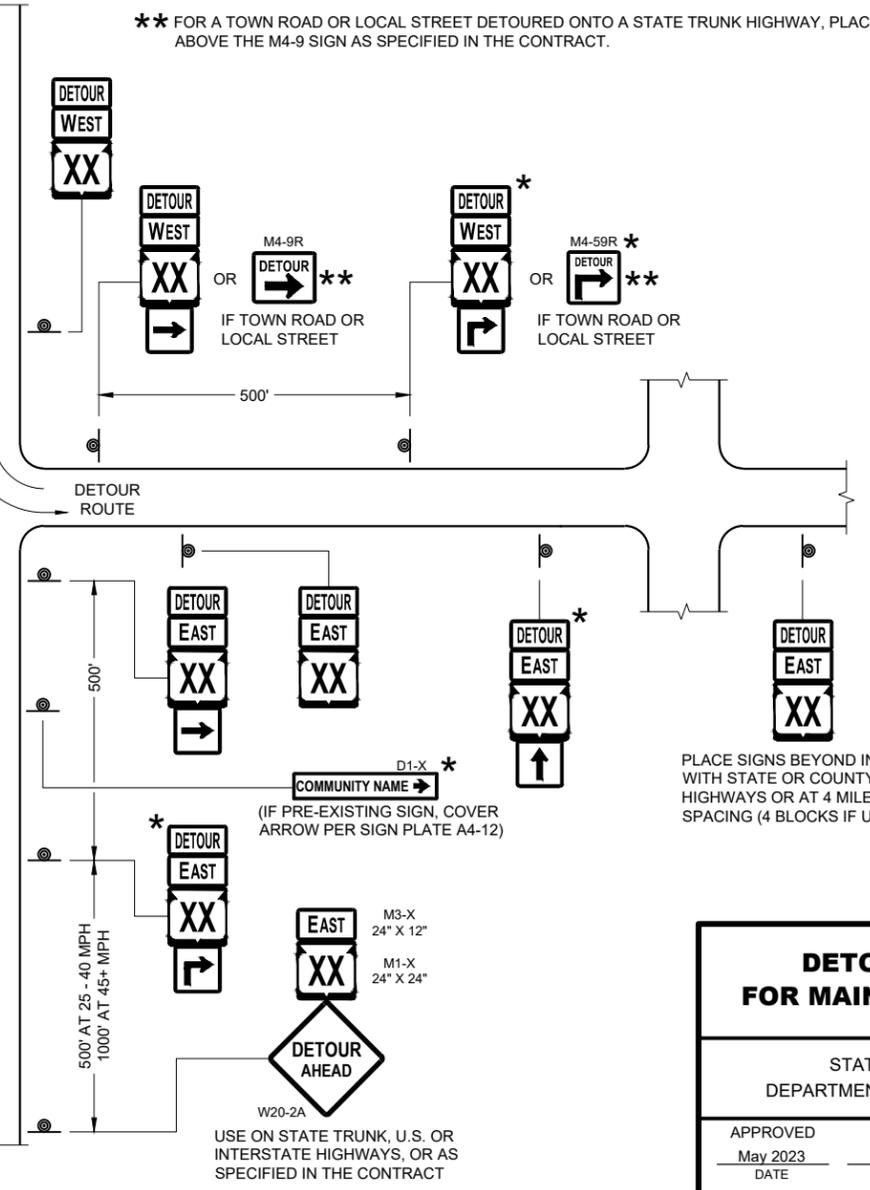
- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-9R SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

- * OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- ** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

MATCH POINT



**DETAIL F
DETOUR SIGNING**



**DETOUR SIGNING
FOR MAINLINE CLOSURES**

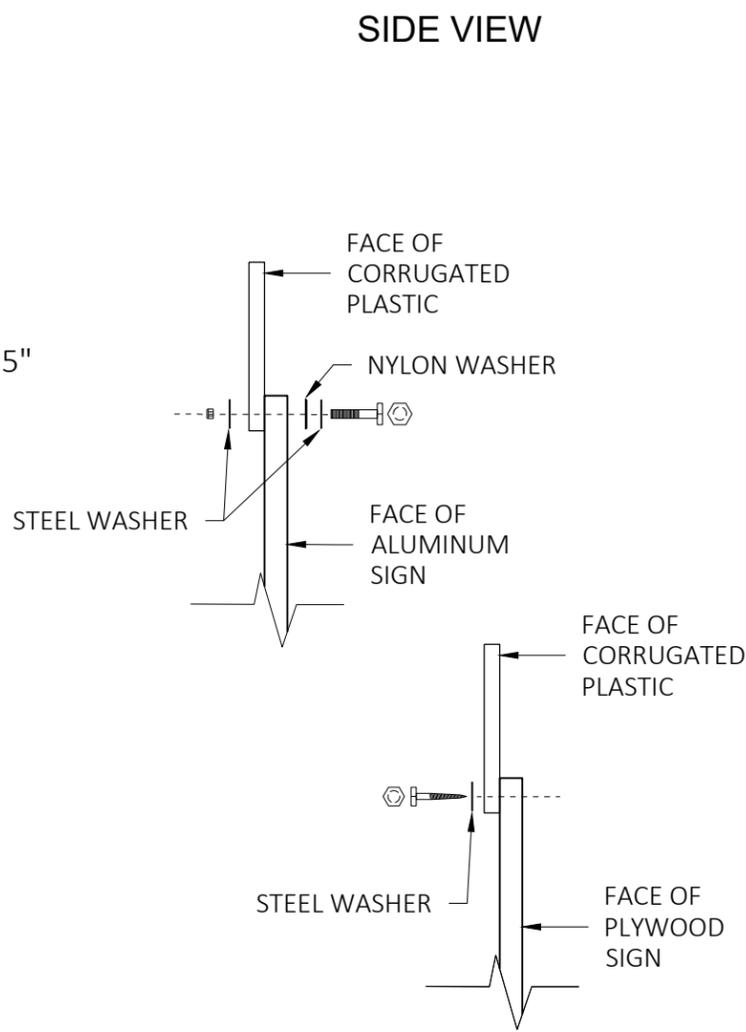
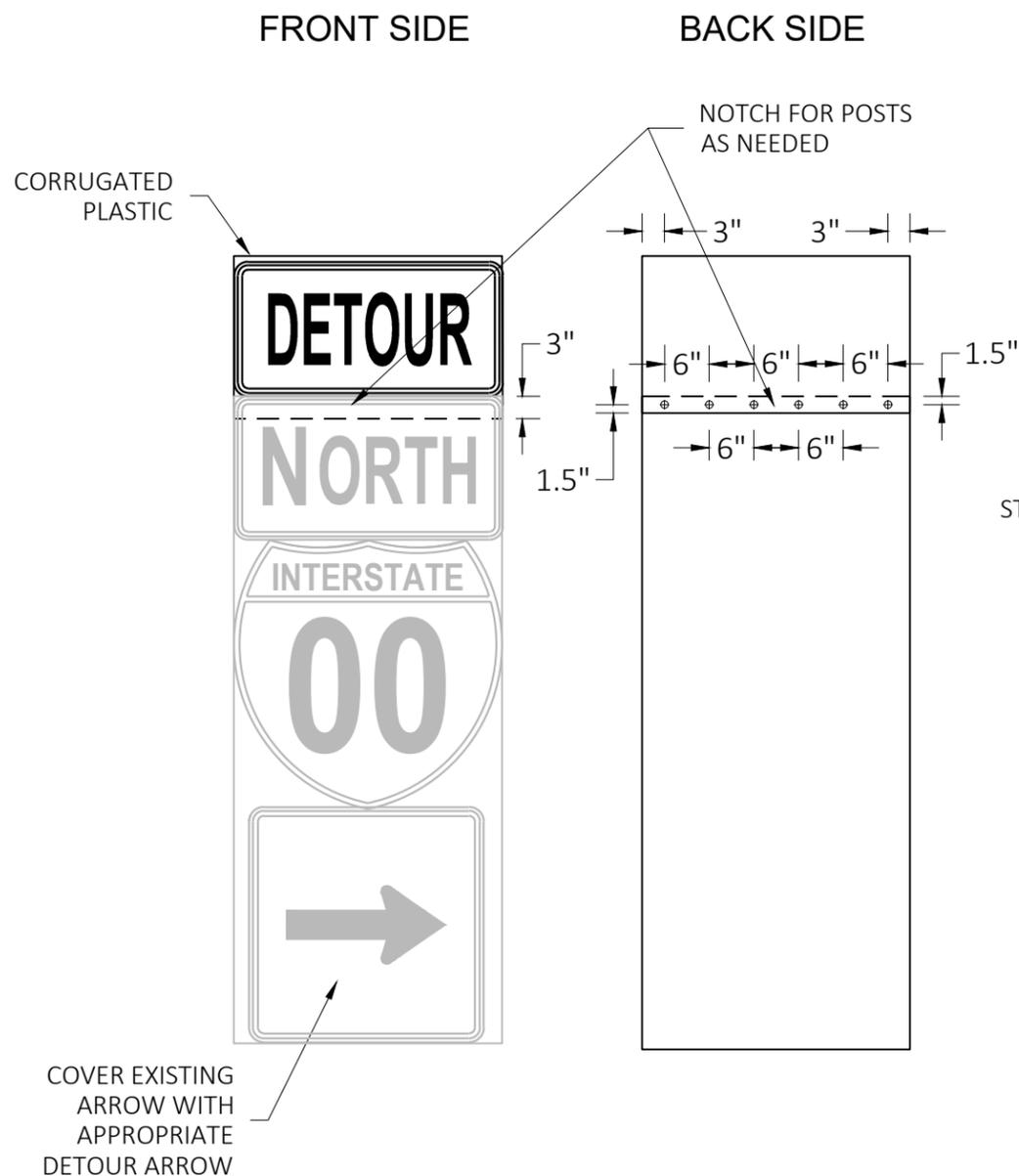
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

PLACE SIGNS BEYOND INTERSECTIONS WITH STATE OR COUNTY TRUNK HIGHWAYS OR AT 4 MILE MAXIMUM SPACING (4 BLOCKS IF URBAN AREA)



GENERAL NOTES

CELLS OF CORRUGATED PLASTIC SHALL BE VERTICALLY ORIENTED.

PROVIDE A 0.4-INCH THICK BASE CORRUGATED PLASTIC WITH A 0.035-INCH WALL THICKNESS AND 0.4-INCH CELL SIZE.

FOR 36" WIDE SIGNS: USE 6 FASTENERS AS SHOWN.

FOR 24" WIDE SIGNS: USE 4 FASTENERS WITH EDGE SPACING AS SHOWN AND 6" SPACING BETWEEN FASTENERS.

METAL WASHERS, NUTS, BOLTS AND LAGS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

- A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3.
- B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

PLYWOOD SIGNS:

LAG SCREWS - 5/16" x 1"

ALUMINUM SIGNS:

MACHINE BOLTS - 5/16" x 1-1/4" LENGTH W/NUTS

WASHERS:

1-1/4" O.D. x 3/8" I.D. x 1/16" STEEL

1-1/4" O.D. x 3/8" I.D. x .080 NYLON

MODIFIED ROUTE ASSEMBLY FOR DETOUR SIGNING

MODIFIED ROUTE ASSEMBLY FOR DETOUR SIGNING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Andrew Heidtke ROADWAY STANDARDS DEVELOPMENT ENGINEER
<small>FHWA</small>	

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

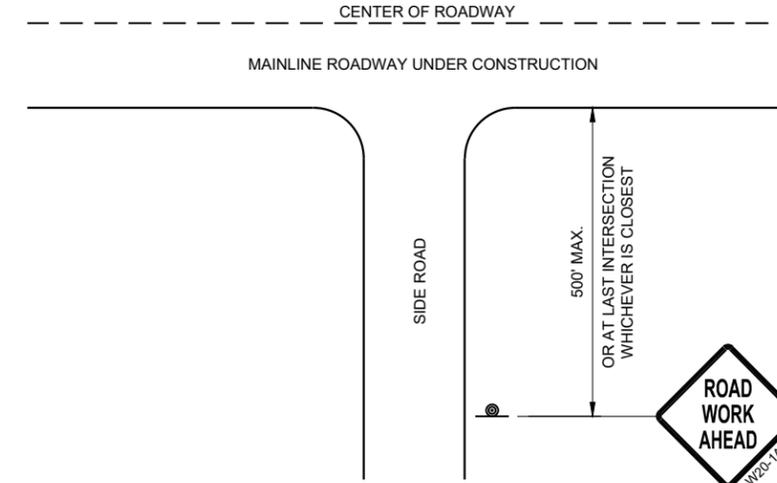
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

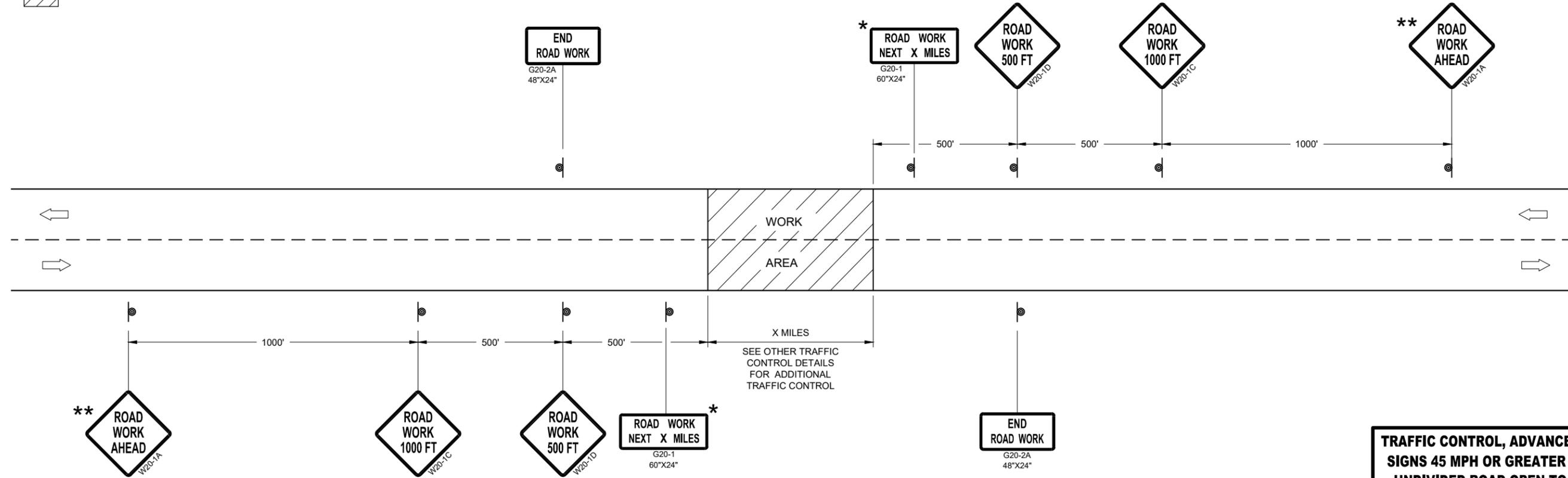
- * OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS
- ** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 MPH OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE July 2018 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

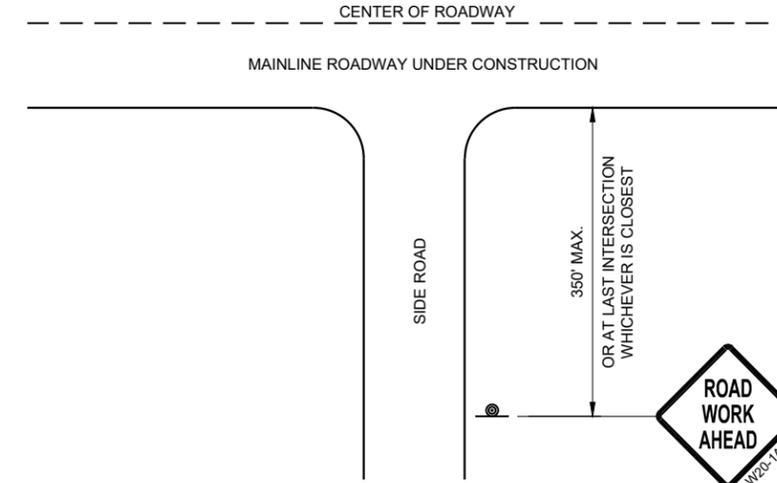
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

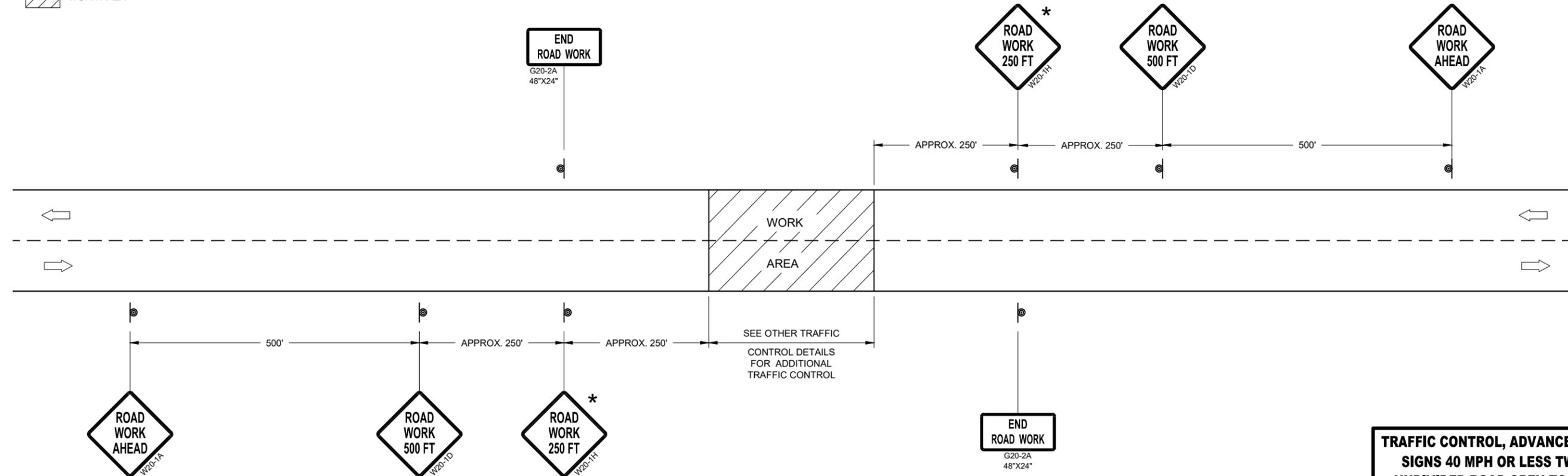
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



**TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL**

LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

**TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 40 MPH OR LESS TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC**

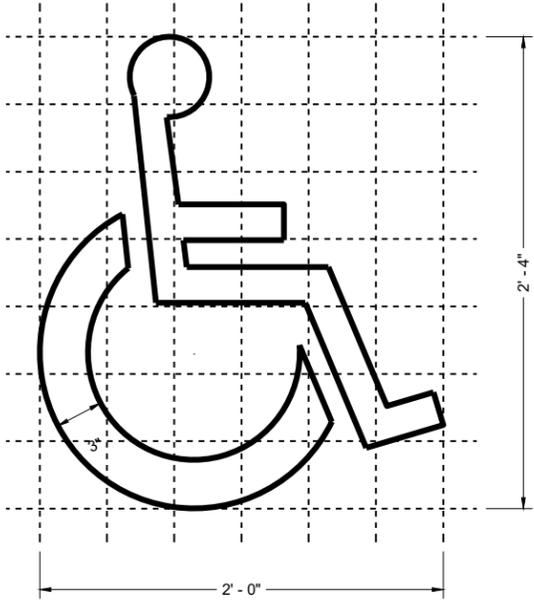
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

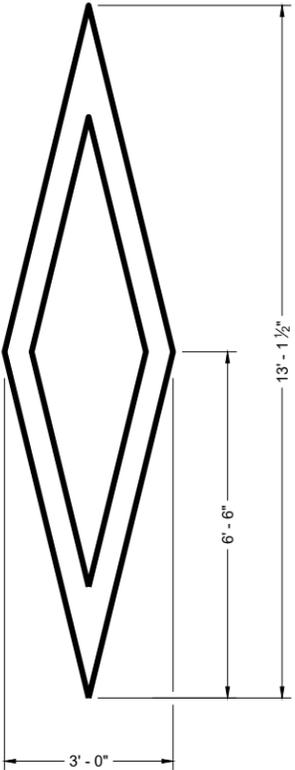
FHWA

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



HANDICAP SYMBOL



PREFERENTIAL LANE SYMBOL

PAVEMENT MARKING SYMBOLS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2024 /S/ Jeannie Silver
DATE STATE SIGNING AND MARKING ENGINEER

FHWA

GENERAL NOTES

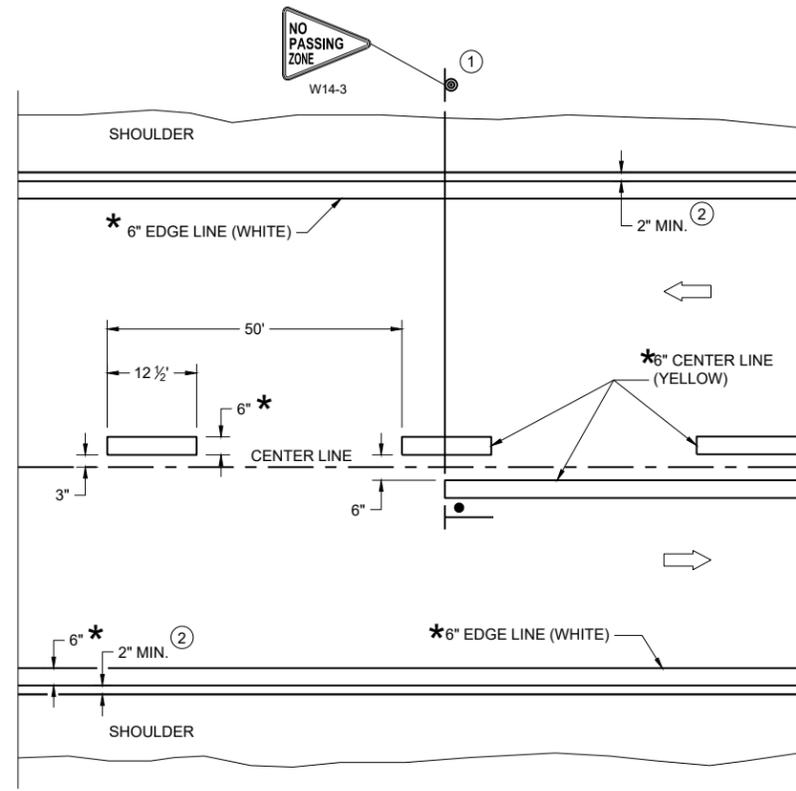
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

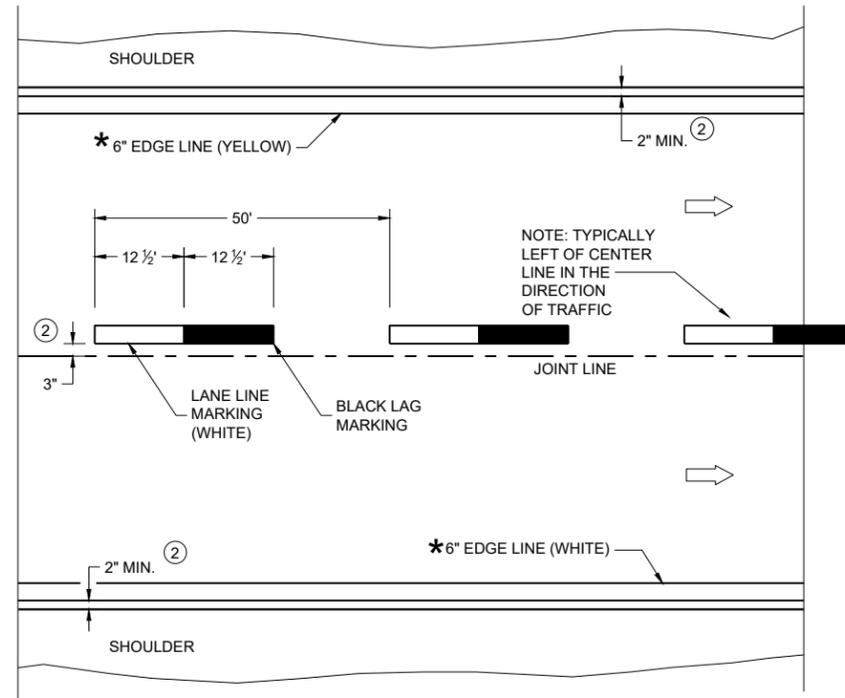
LEGEND

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

6

6

SDD 15C08-24a

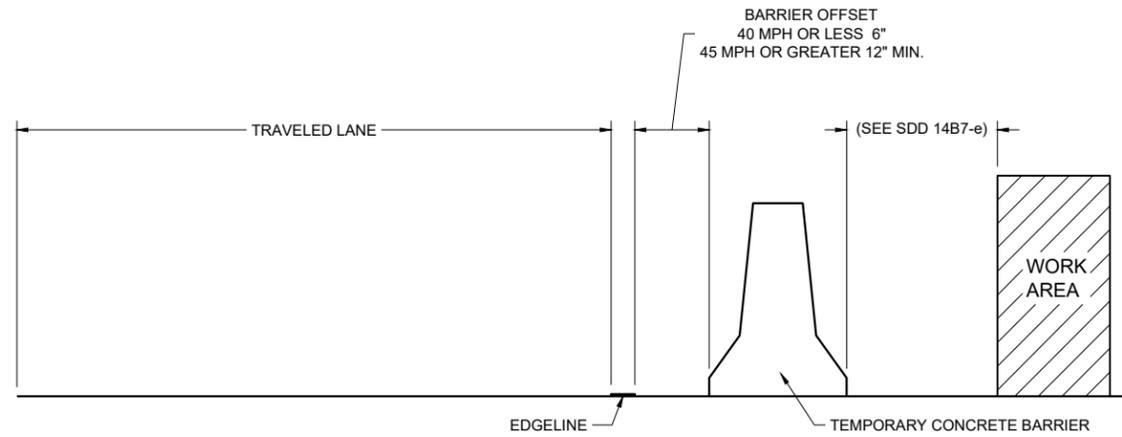
SDD 15C08-24a

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2024 /S/ Jeannie Silver
DATE Statewide Pavement Marking Engineer

FHWA



TEMPORARY BARRIER OFFSET FROM EDGELINE

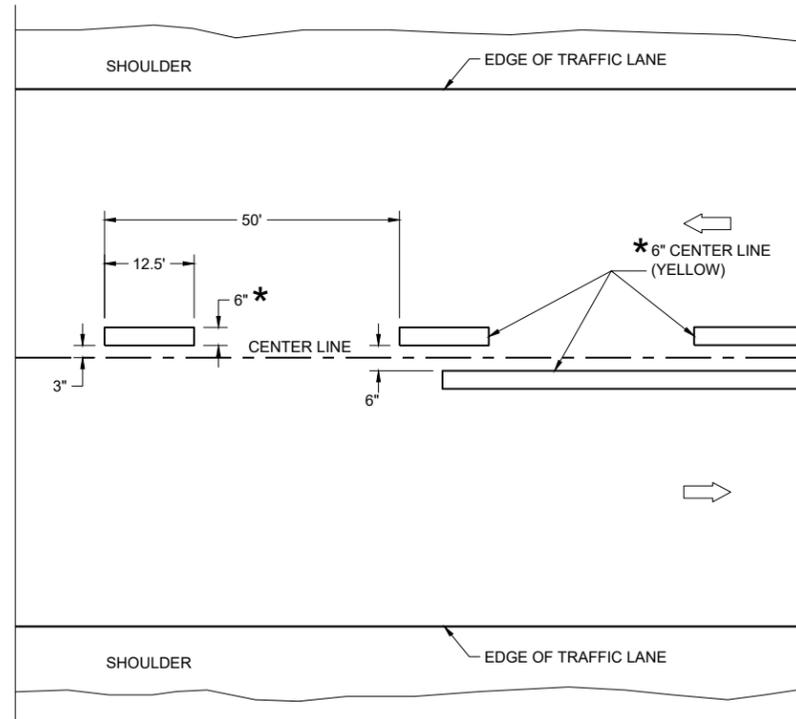
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

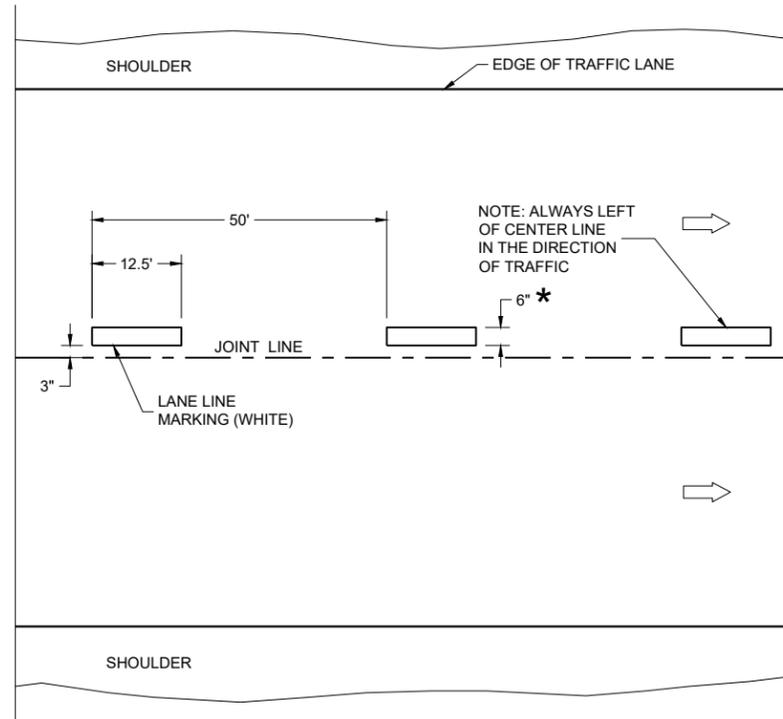
LEGEND

➡ DIRECTION OF TRAFFIC

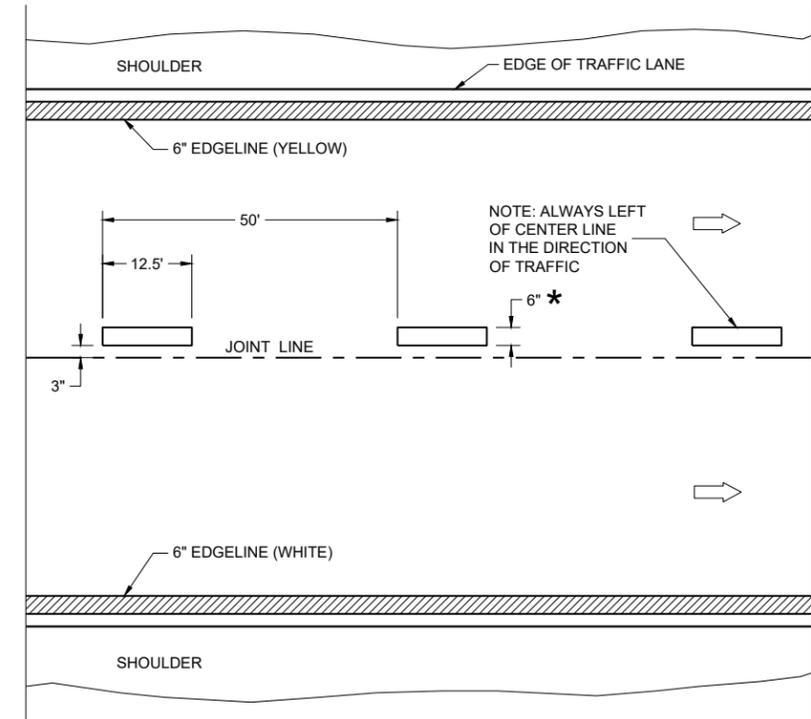
*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

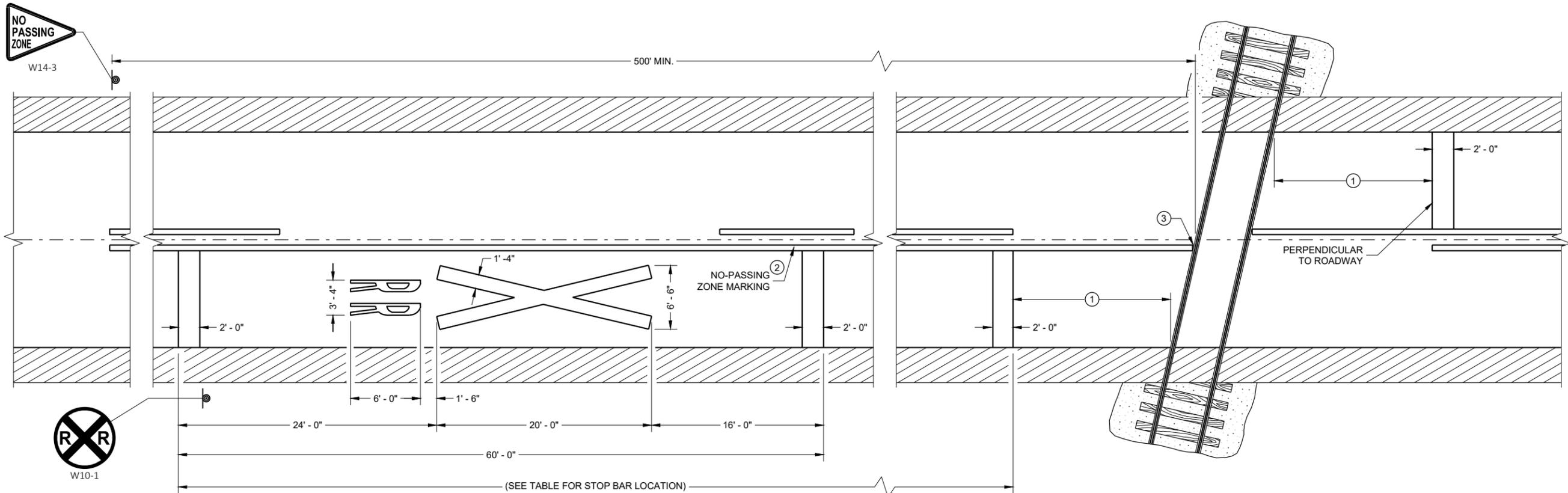
TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2024 /S/ Jeannie Silver
DATE Statewide Pavement Marking Engineer

FHWA



PAVEMENT MARKING

LEGEND

⊙ SIGN ON PERMANENT SUPPORT

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

ON MULTI-LANE ROADS THE TRANSVERSE BANDS SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.

CENTER OR LANE LINES AND NO-PASSING ZONE MARKINGS SHOWN ON THIS DRAWING ARE REQUIRED AND PAID FOR UNDER OTHER ITEMS IN THE CONTRACT.

TRACE EXISTING SYMBOL WHERE EXISTING SYMBOLS ARE PLACED.

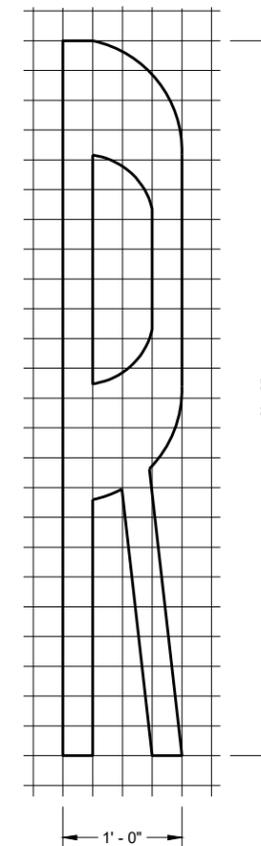
- ① PLACE STOP BAR APPROXIMATELY 8 FEET IN ADVANCE OF THE GATE (IF PRESENT), BUT NO CLOSER THAN 15 FEET IN ADVANCE OF THE NEAREST RAIL. FIELD-FIT STOP BAR TO MAXIMIZE VIEW OF APPROACHING TRAIN.
- ② 500' MINIMUM. MARKING LIMITS MAY BE EXTENDED AS DIRECTED BY THE ENGINEER TO MEET ADJACENT NO-PASSING ZONE MARKINGS.
- ③ FOR MULTIPLE TRACK CROSSINGS, THE BARRIER LINE SHALL EXTEND TO THE NEAR RAIL OF THE FURTHEST TRACK IN THE DIRECTION OF HIGHWAY TRAVEL.

DISTANCE TABLE

TABLE BASED UPON 2C-4 WISCONSIN SUPPLEMENT OF MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

POSTED SPEED (M.P.H.)	DIMENSION RANGE (FEET)
25	150* - 250'
30	200* - 300'
35	250* - 450'
40	300* - 500'
45	400* - 650'
50	550* - 800'
55	750* - 1000'
60	1000* - 1250'
65	1000* - 1250'

* THE MINIMUM DISTANCES IN THE TABLE ARE DESIRABLE AND SHOULD BE USED. THE DISTANCES MAY BE INCREASED UP TO THE MAXIMUM TO ALLOW FOR FIELD CONDITIONS SUCH AS THE CLOSED PROXIMITY OF DRIVEWAYS, BRIDGES, SIDE ROADS OR OTHER FEATURES THAT WOULD PROHIBIT THE MINIMUM DISTANCES FROM BEING USED.

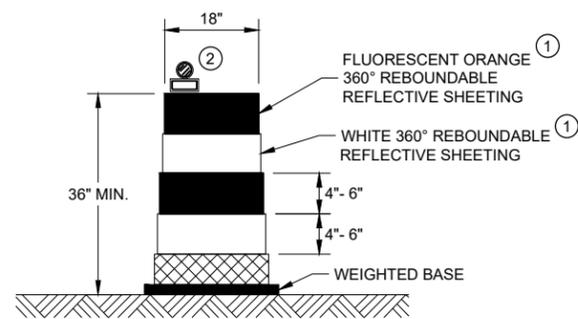


SIGNING AND PAVEMENT MARKING DETAILS FOR RAILROAD - HIGHWAY GRADE CROSSINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

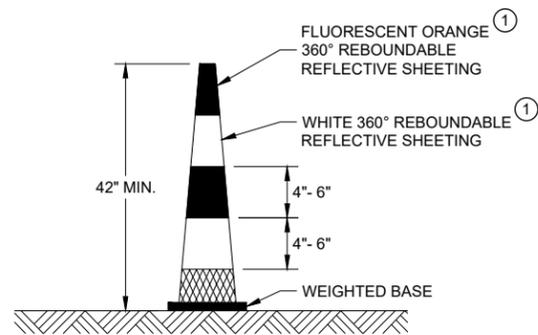
APPROVED
May 2023 DATE /S/ Matthew R. Rauch
STATE SIGNING AND MARKING ENGINEER

FHWA



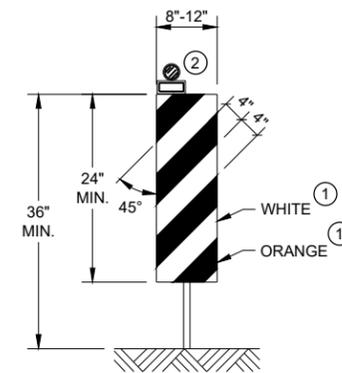
DRUM

BALLAST WIDTHS
RANGE FROM 24"-36"



42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS
BALLAST WIDTHS
RANGE FROM 14"-20"

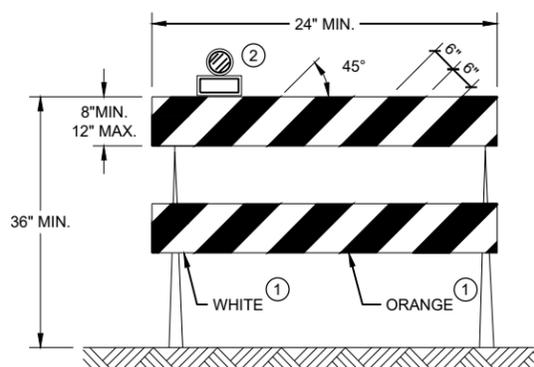


VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.

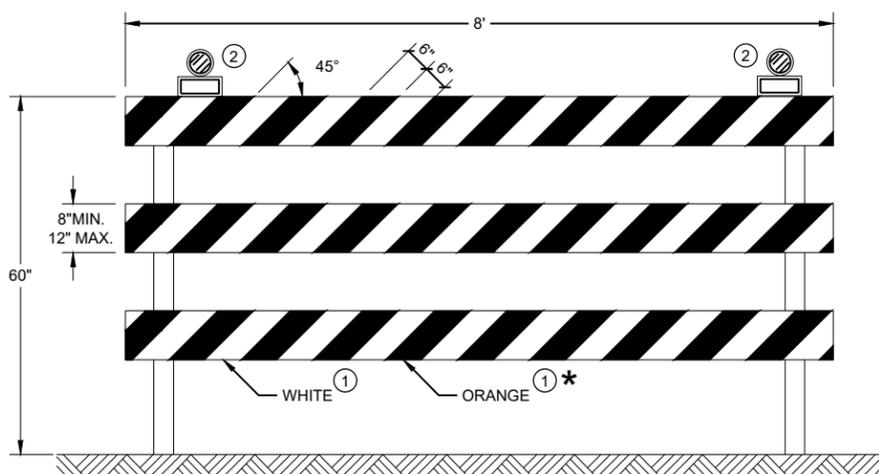
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

**CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
 November 2022 /S/ Andrew Heidtke
 DATE WORK ZONE ENGINEER
 FHWA

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

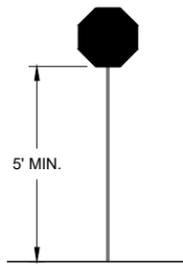
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



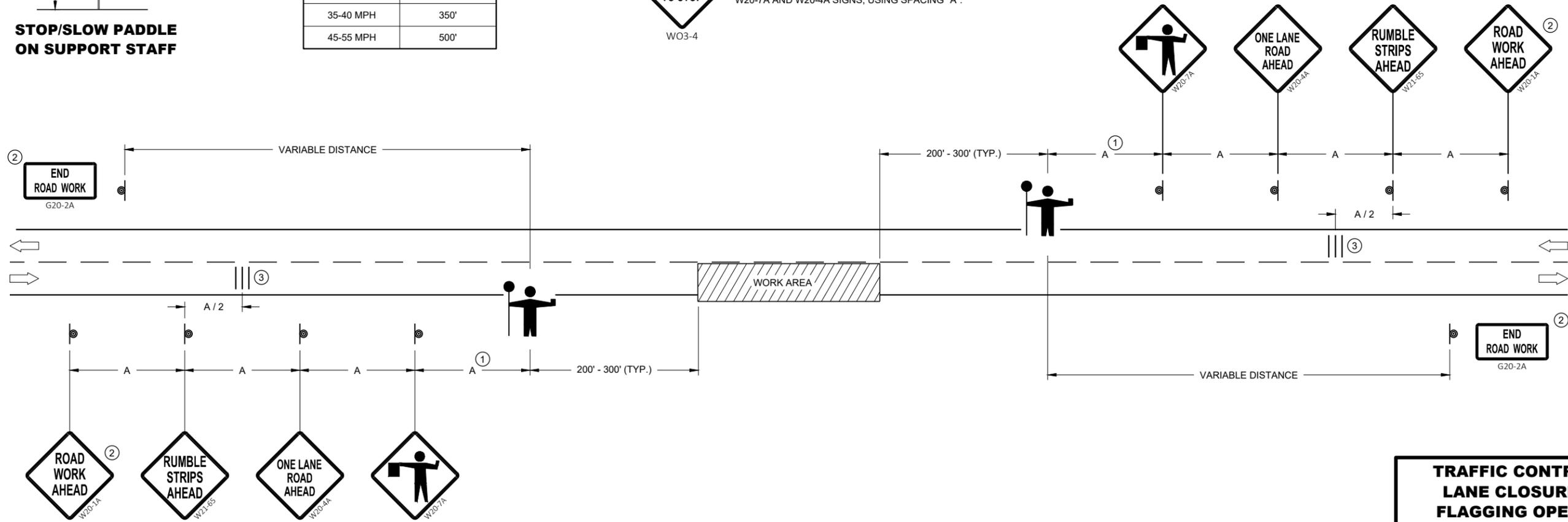
STOP/SLOW PADDLE ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



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SDD 15C12 - 09a

SDD 15C12 - 09a

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE May 2022 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

LEGEND

- V1** LEAD VEHICLE
- V2** MARKING VEHICLE
- V3** SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH WORKERS SHALL NOT PERFORM WORK FROM ANY SHADOW OR PROTECTION VEHICLES.

UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

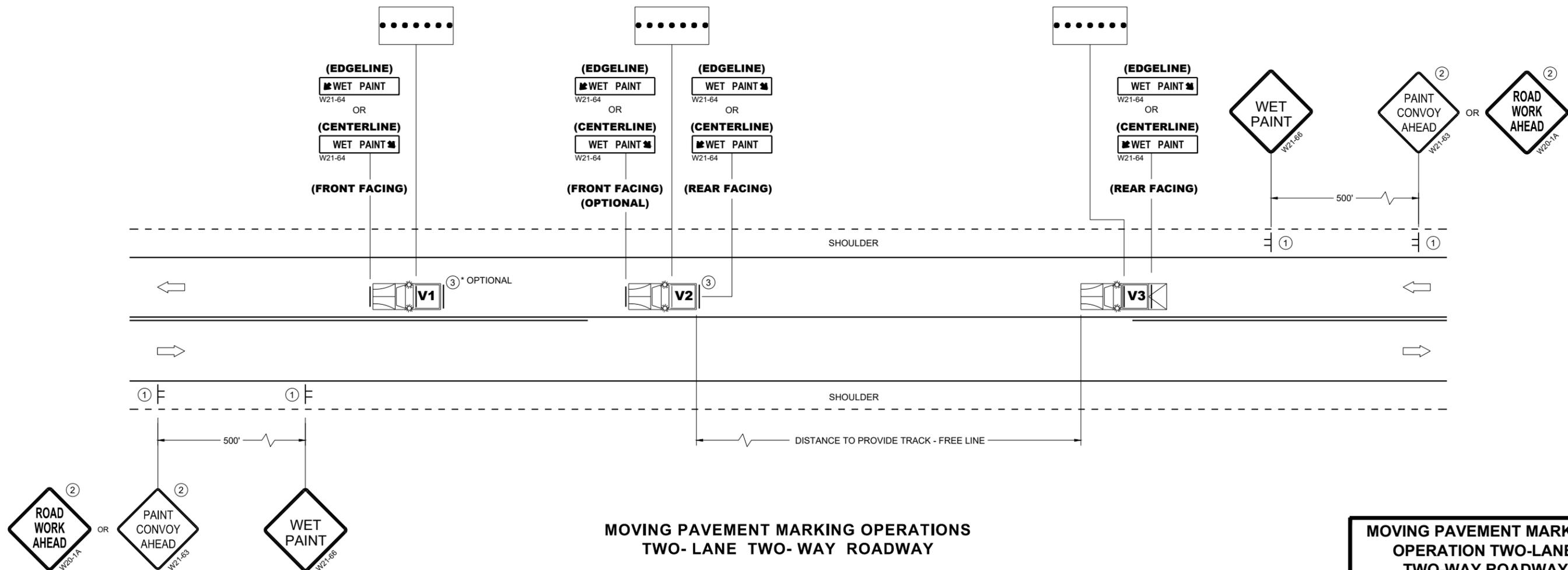
CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING .

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES AND AFTER EVERY MAJOR INTERSECTION.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.
- ③ V1 AND V2 CAN BE SWITCHED SO THAT THE MARKER IS THE LEAD VEHICLE.

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**MOVING PAVEMENT MARKING OPERATIONS
TWO-LANE TWO-WAY ROADWAY**

**MOVING PAVEMENT MARKING
OPERATION TWO-LANE
TWO-WAY ROADWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2025 /S/ Andrew Heidtke
DATE STATE ELECTRICAL ENGINEER

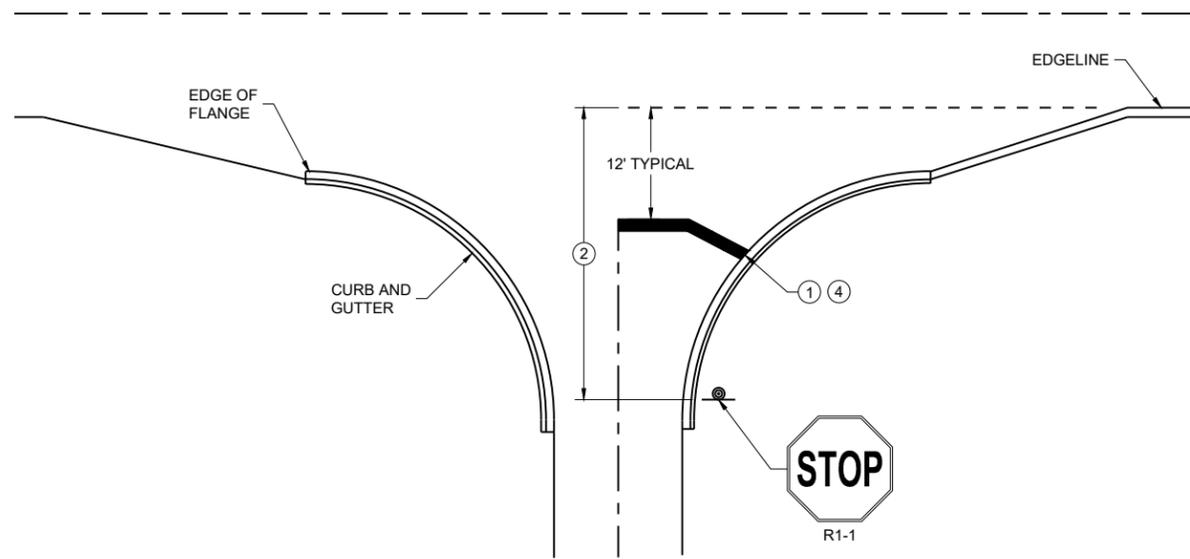
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SDD 15C19-11a

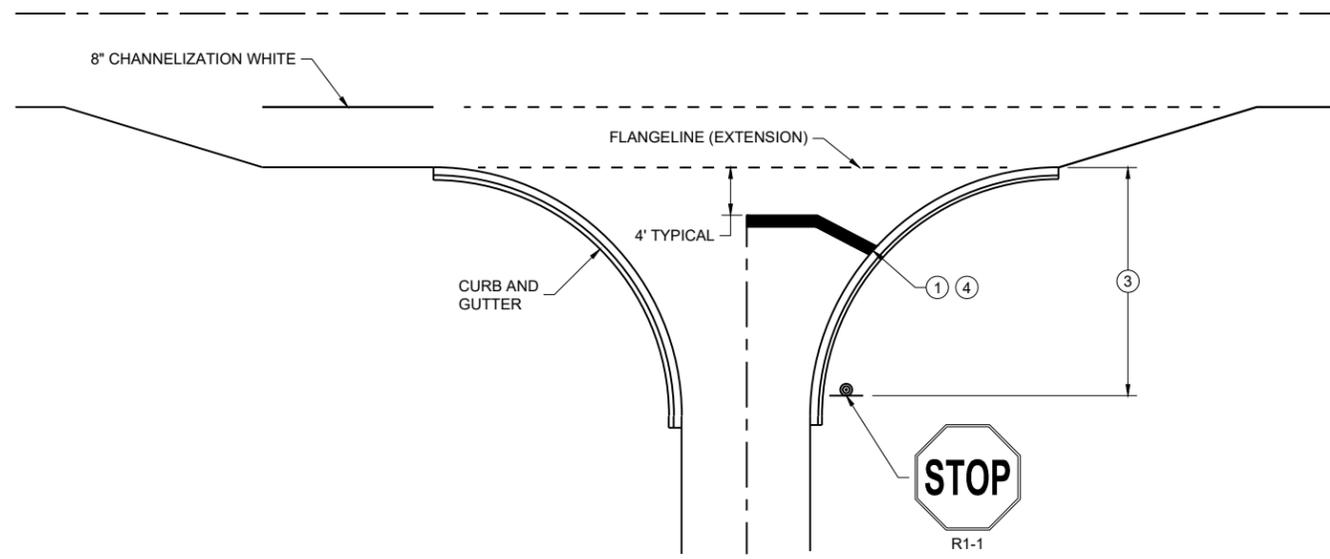
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

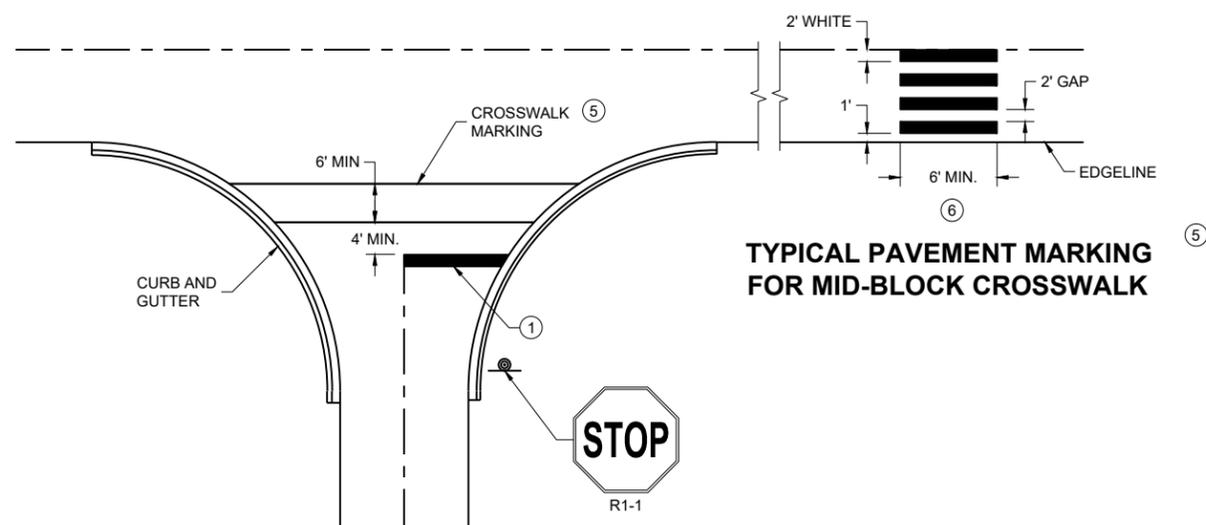
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGE LINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES.
- ⑥ POSTED SPEED LIMITS OF 40 MPH OR GREATER USE A MINIMUM WIDTH OF 8' FOR MIDBLOCK CROSSWALKS



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

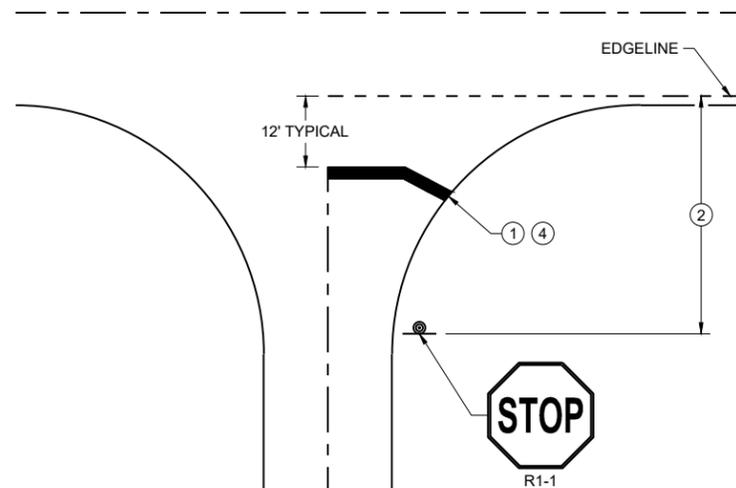


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDE ROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDE ROADS WITH CROSSWALK MARKING

TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

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SDD 15C33-05

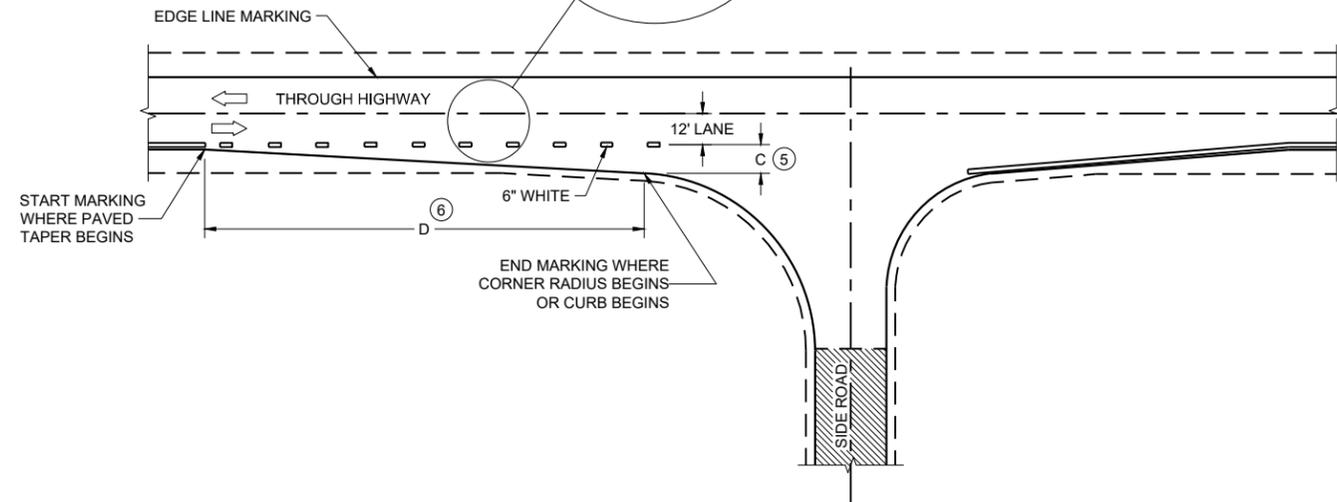
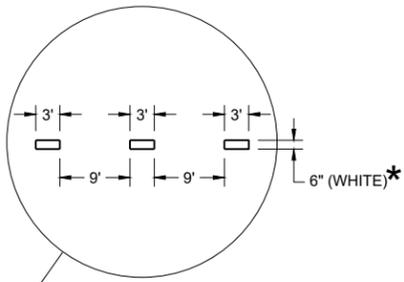
SDD 15C33-05

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2024 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER

FHWA



MINOR INTERSECTION

*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

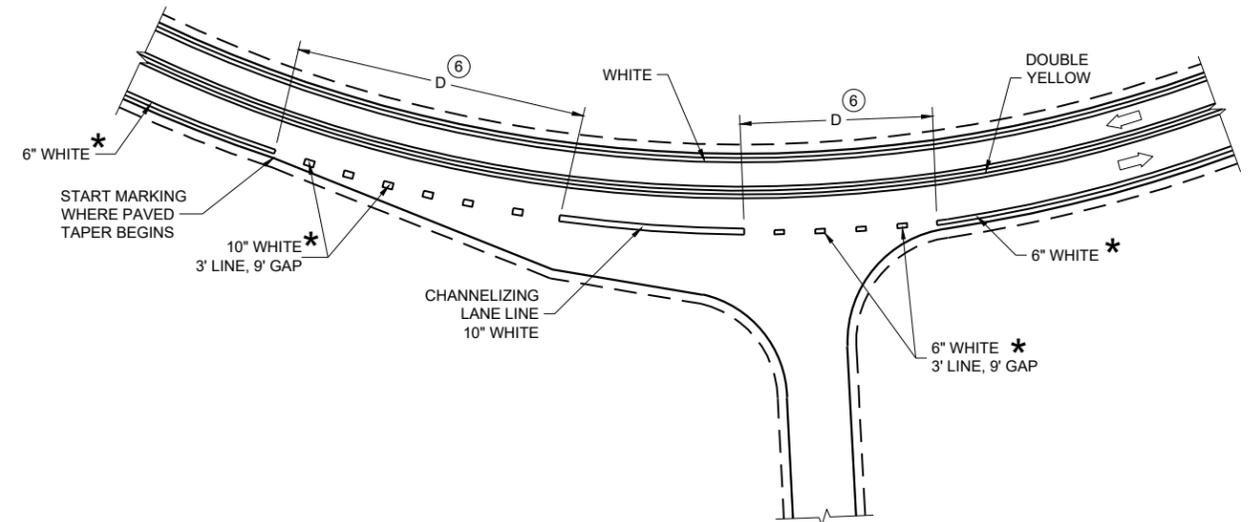
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

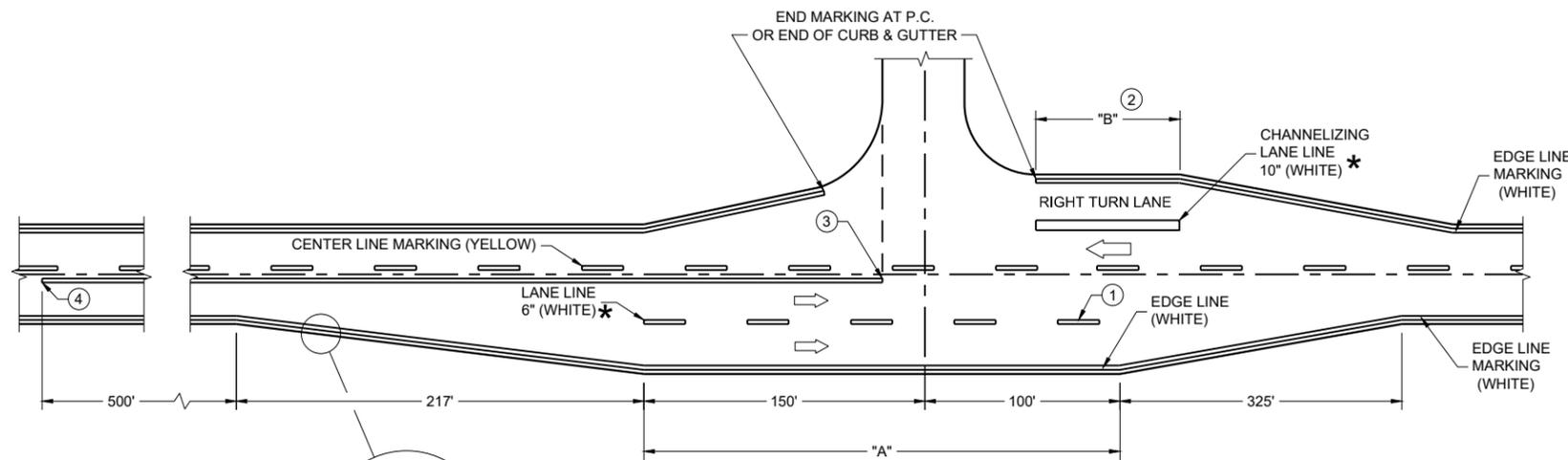
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
- ⑤ WHEN DISTANCE "C" IS LESS THAN 4 FEET, OMIT DOTTED EXTENSION.
- ⑥ WHEN DISTANCE "D" IS LESS THAN 50 FEET, OMIT DOTTED EXTENSION.

LEGEND

➡ DIRECTION OF TRAVEL

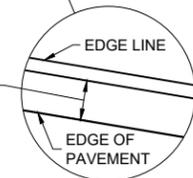


INTERSECTION ON OUTSIDE OF CURVE



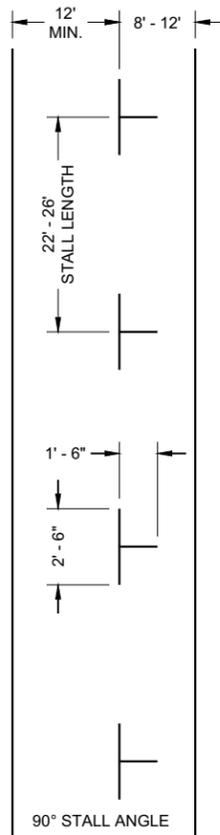
**MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**

BYPASS LANE PAVED SHOULDER WIDTH (AS SHOWN ELSEWHERE IN PLANS) - PLUS 2 INCHES

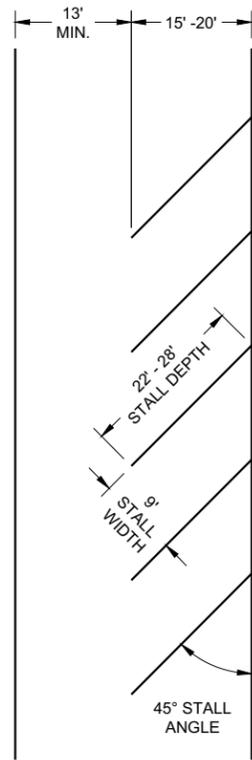


**PAVEMENT MARKING
(INTERSECTIONS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

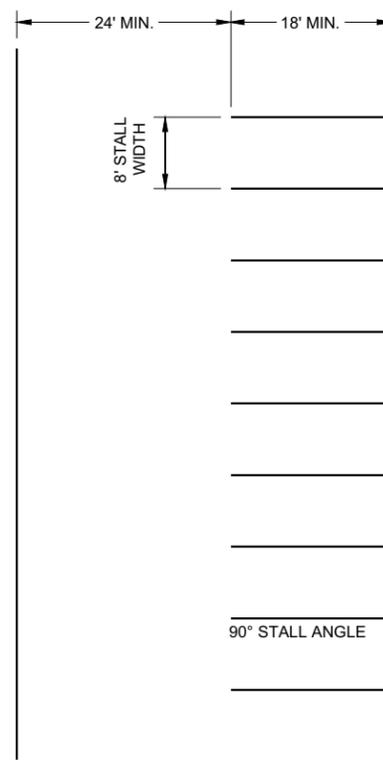
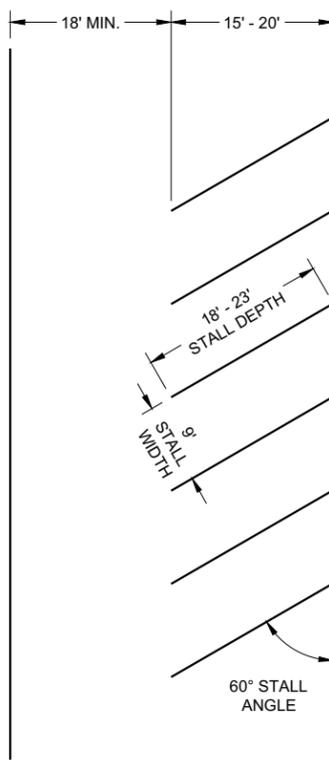


PARALLEL PARKING



ANGLED PARKING

(ANGLED PARKING IS NOT ALLOWED ON STATE HIGHWAYS UNLESS A DESIGN JUSTIFICATION HAS BEEN COMPLETED.)



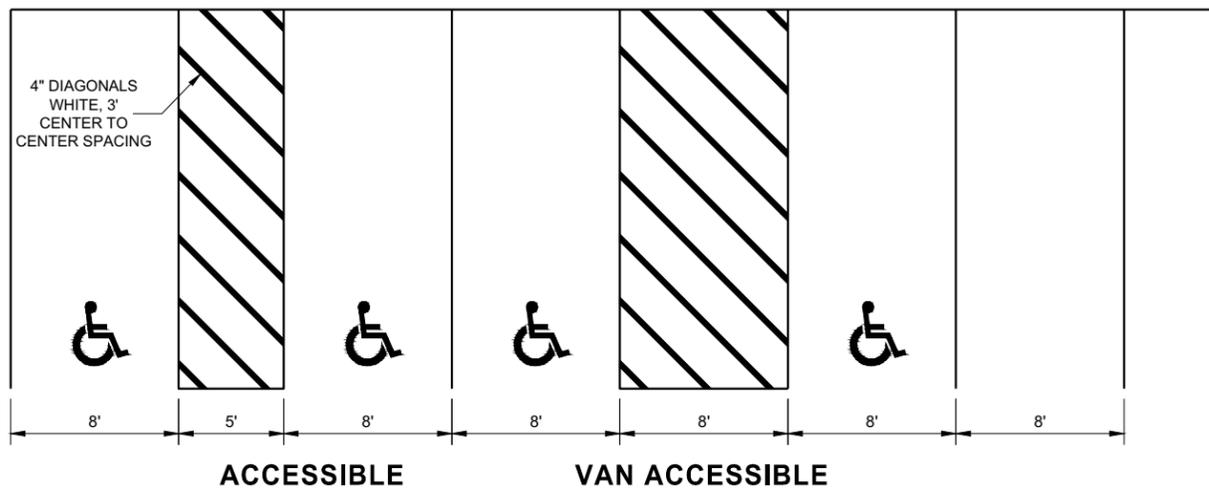
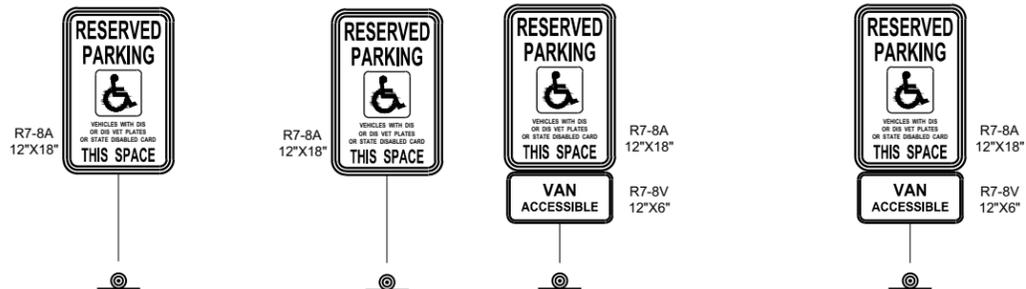
PARKING LOTS

GENERAL NOTES

ALL LINES 4" WHITE (UNLESS OTHERWISE NOTED)
 LAST PARKING STALL IS A MINIMUM OF 15' FROM THE CROSSWALK.

LEGEND

⊙ SIGN ON PERMANENT SUPPORT



PARKING STALL MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED August 2019 DATE	/S/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER
<small>FHWA</small>	

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  DIRECTION OF TRAFFIC
-  WORK ZONE

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

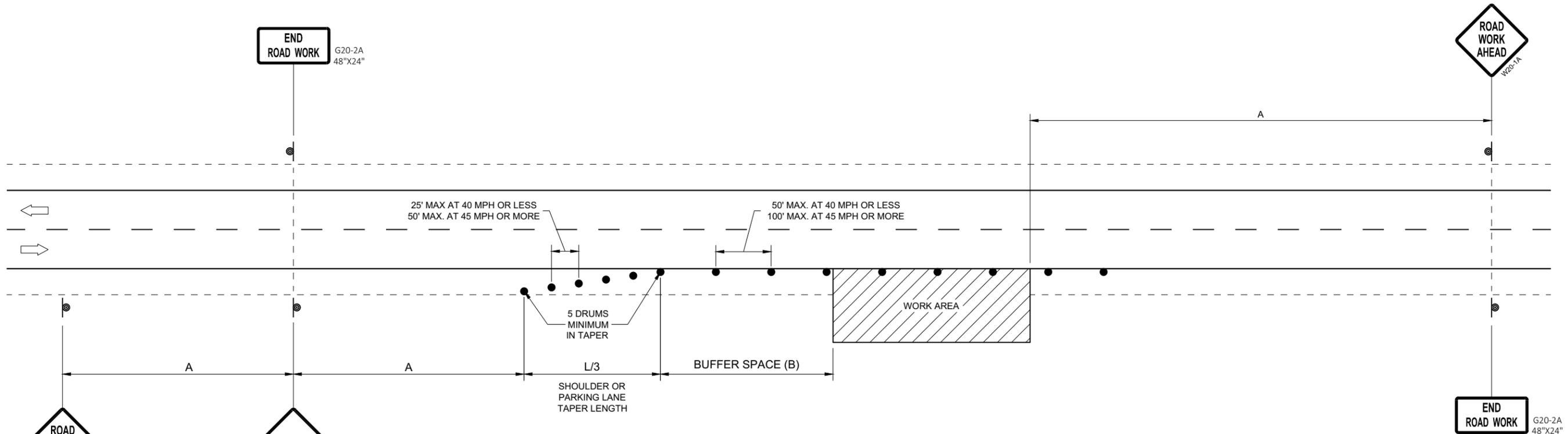
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

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OR
IF TRAFFIC CONTROL DEVICES
ENCROACH ONTO TRAVELED WAY, USE

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHOULDER TAPER L / 3 W, LATERAL OFFSET (FT)						BUFFER SPACE (B) FEET
		3	4	5	6	7	8	
25	200'	10	14	17	21	24	28	55
30	200'	15	20	25	30	35	40	85
35	350'	20	27	34	40	47	54	120
40	350'	26	35	44	53	62	70	170
45	500'	45	59	74	89	104	119	220
50	500'	50	66	83	99	116	132	280
55	500'	54	73	91	109	127	145	335'

**TRAFFIC CONTROL, WORK ON
SHOULDER OR PARKING LANE,
UNDIVIDED ROADWAY**

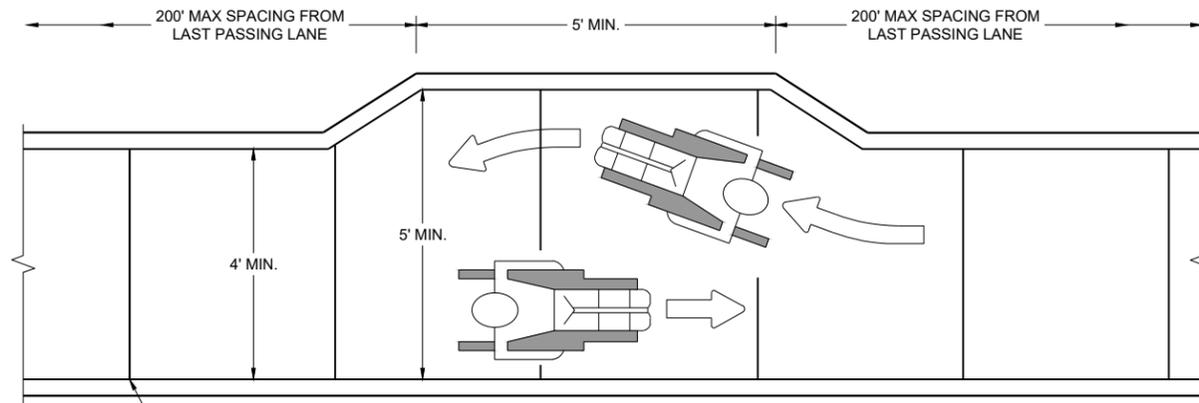
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2020 /S/ Andrew Heidtke
DATE STATEWIDE WORK ZONE TRAFFIC
SAFETY ENGINEER

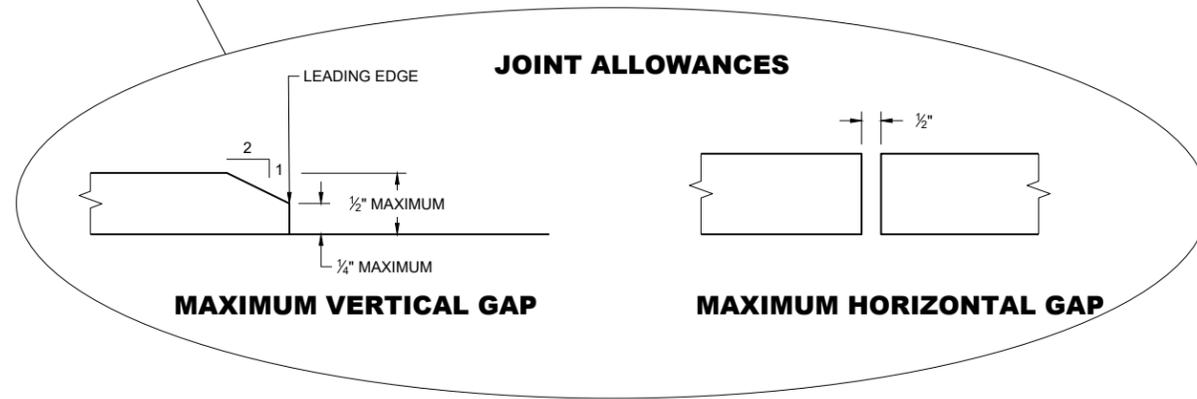
FHWA

SDD 15D28 - 04

SDD 15D28 - 04



NARROW SIDEWALK PASSING DETAIL

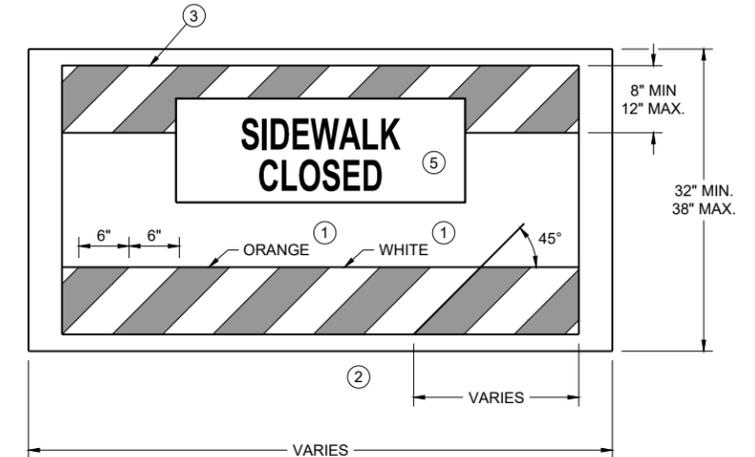


MAXIMUM VERTICAL GAP

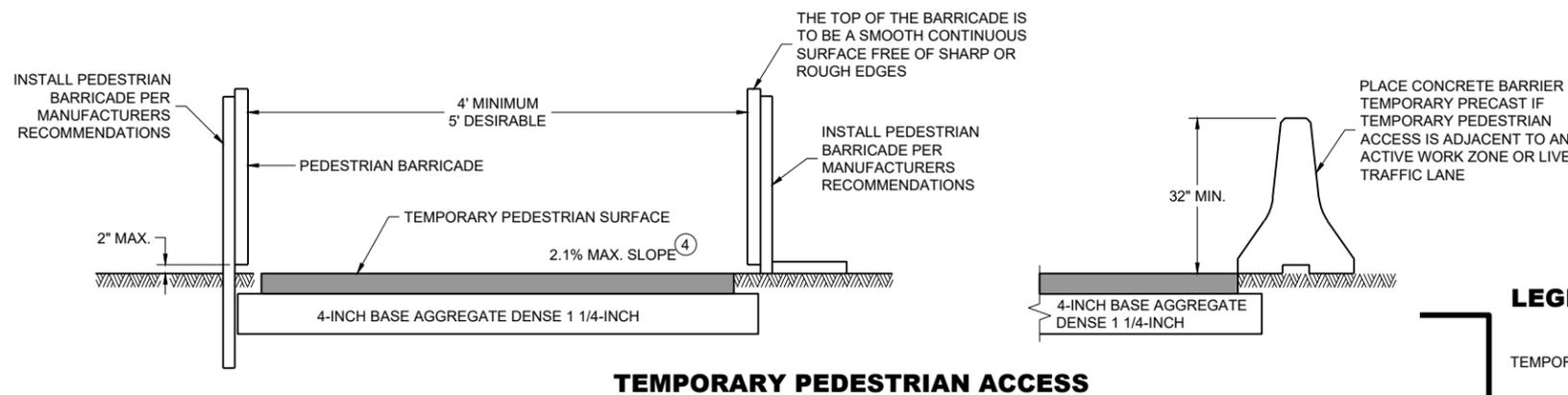
MAXIMUM HORIZONTAL GAP

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- ★ USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.
- ④ WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.
- ⑤ WHERE SIGNS FOR TEMPORARY PEDESTRIAN ACCOMMODATIONS ARE SHOWN BEING PLACED BEHIND TEMPORARY PEDESTRIAN BARRICADE, THE SIGNS MAY BE MOUNTED ON THE TEMPORARY PEDESTRIAN BARRICADE INSTEAD. A CORRUGATED POLYPROPYLENE OR POLYETHYLENE PLASTIC SIGN BASE SHALL BE USED IF MOUNTED ON THE BARRICADE. THE TOP OF THE SIGN SHALL BE MOUNTED BELOW THE TOP OF THE BARRICADE TO ALLOW A CONTINUOUS HAND-TRAILING EDGE.



TEMPORARY PEDESTRIAN BARRICADE *



TEMPORARY PEDESTRIAN ACCESS



TEMPORARY PEDESTRIAN FLAGGING

- LEGEND**
- TEMPORARY PEDESTRIAN BARRICADE
 - AUDIBLE MESSAGE DEVICE
 - TEMPORARY SIGN SUPPORT
 - WORK AREA

**TRAFFIC CONTROL,
PEDESTRIAN
ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

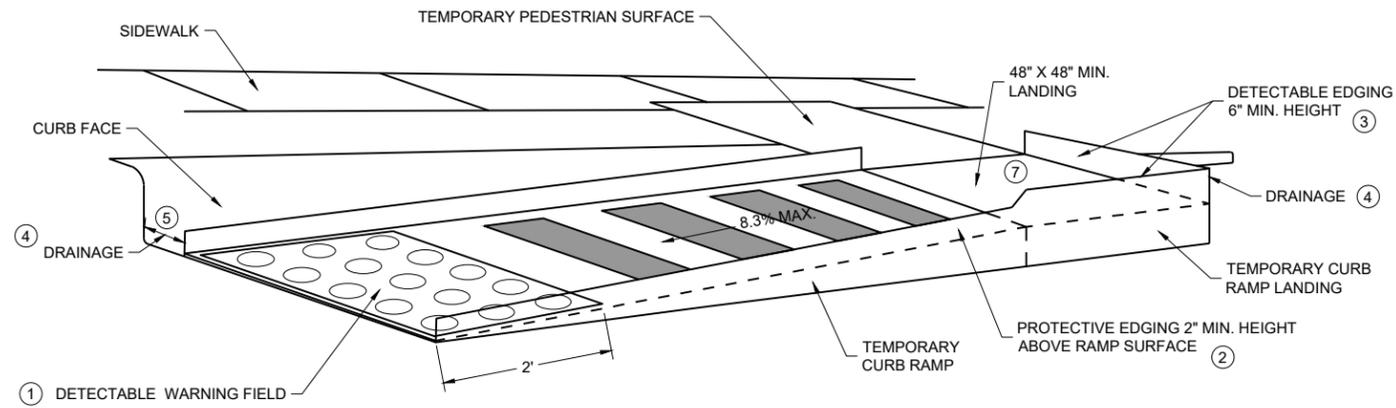
CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:48 (2.1%) MAX. CROSS-SLOPE.

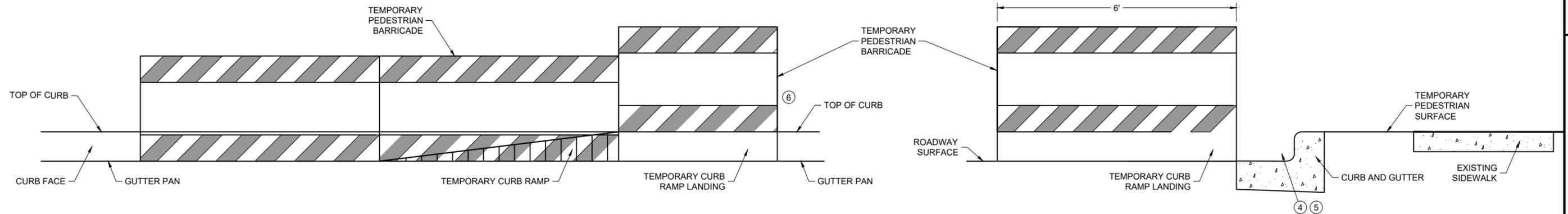
CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP. LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ ENSURE CURB RAMP IS OUT OF THE GUTTER PAN.
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.
- ⑦ LANDING TO BE SLOPED A MAXIMUM OF 2.1% IN ALL DIRECTIONS OF PEDESTRIAN TRAVEL.



PERSPECTIVE VIEW



FRONT VIEW

SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

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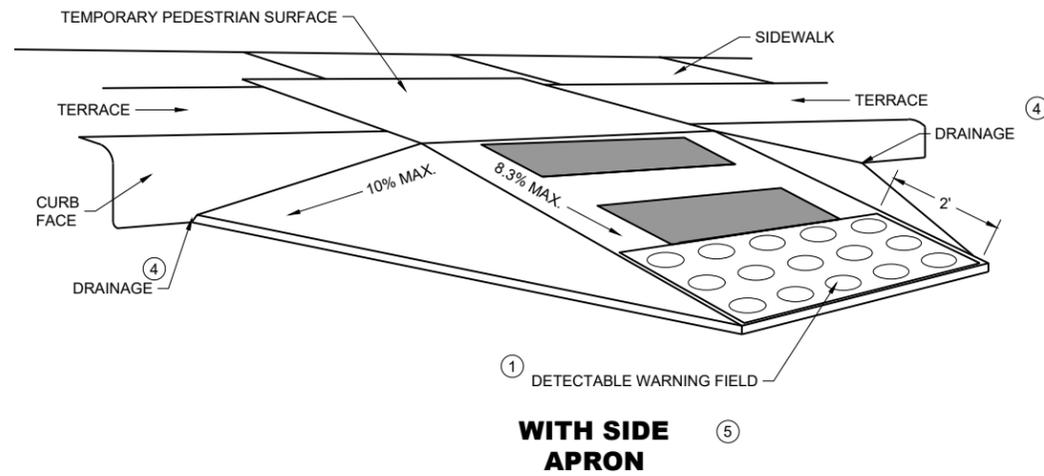
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SDD 15D30-12b

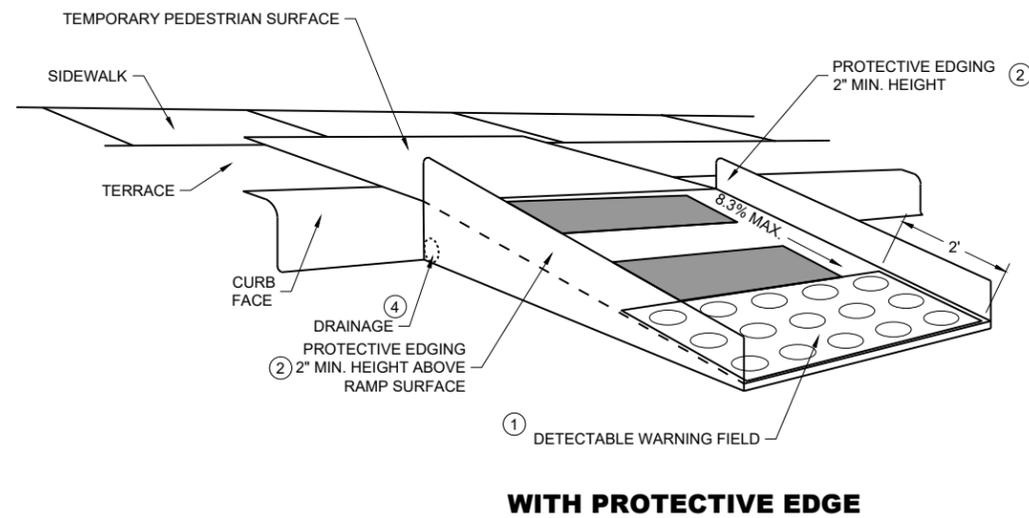
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**TRAFFIC CONTROL,
PEDESTRIAN
ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



WITH SIDE APRON



WITH PROTECTIVE EDGE

TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMP SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMP AND LANDINGS SHALL HAVE A 1:48 (2.1%) MAX. CROSS-SLOPE.

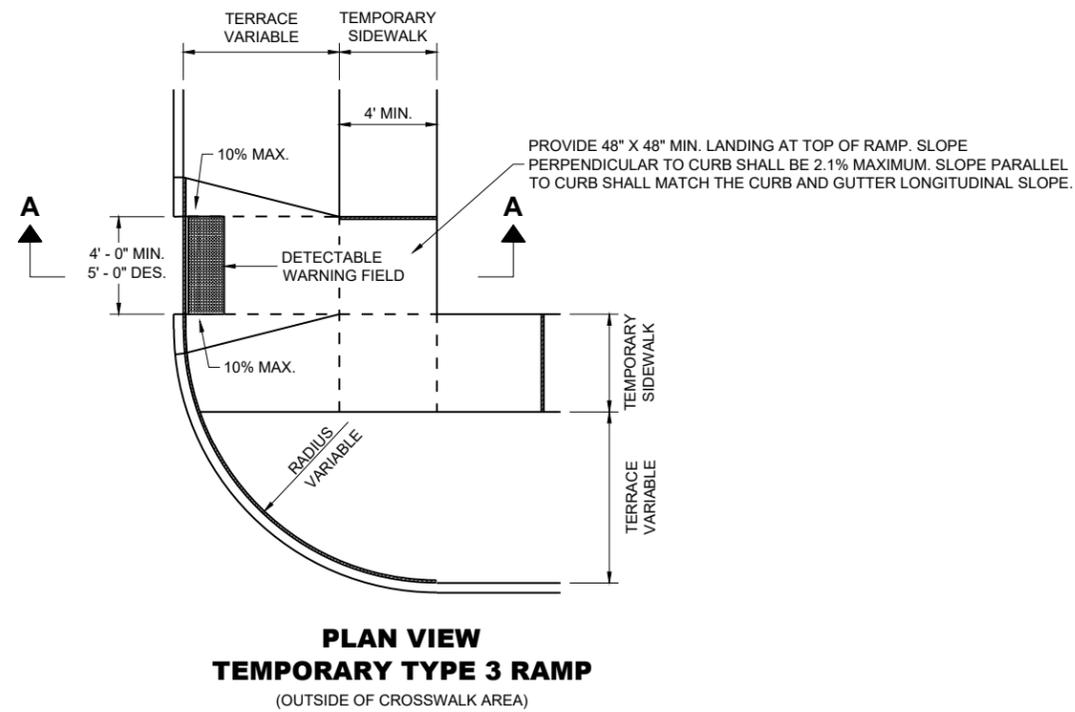
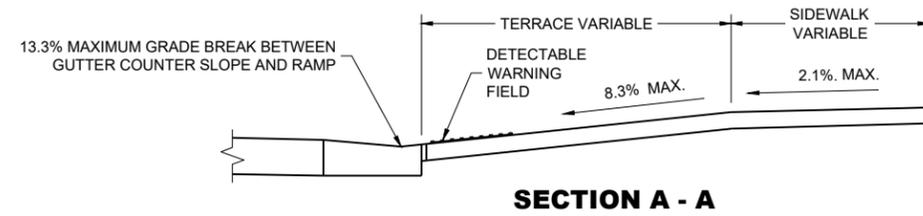
CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMP OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

GENERAL NOTES



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SDD 15D30-12d

SDD 15D30-12d

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

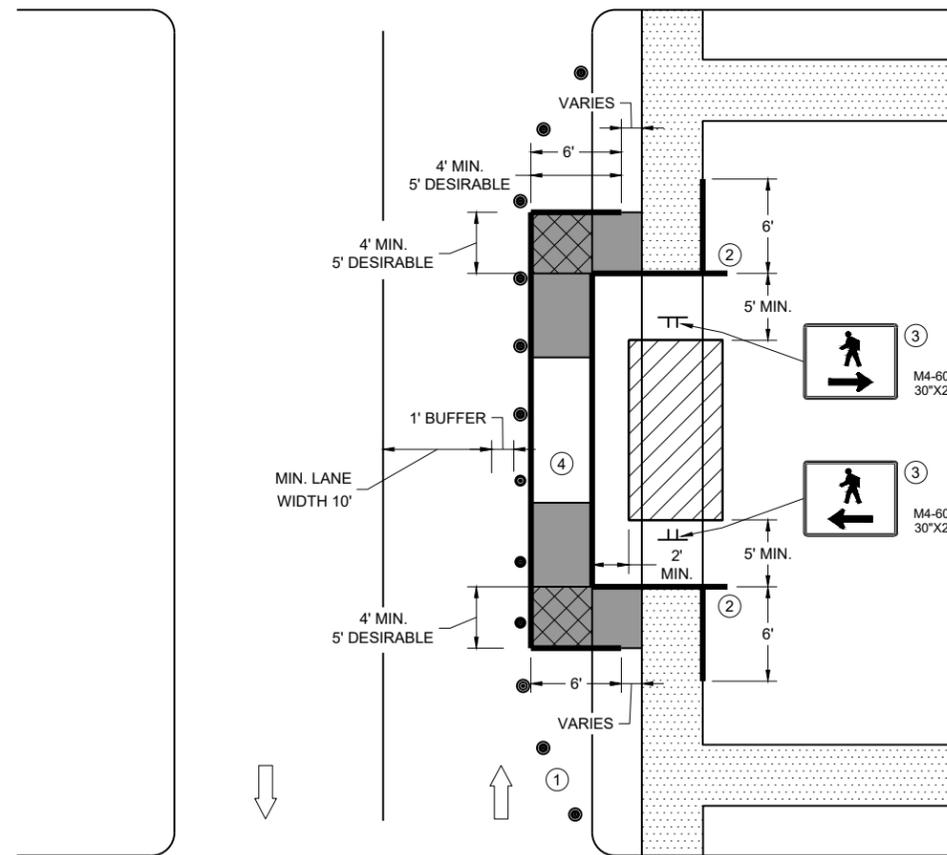
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
- ② PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
- ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
- ④ USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURFACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE. WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.



SIDEWALK BYPASS, SINGLE SIDE

6

6

SDD 15D30-12h

SDD 15D30-12h

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

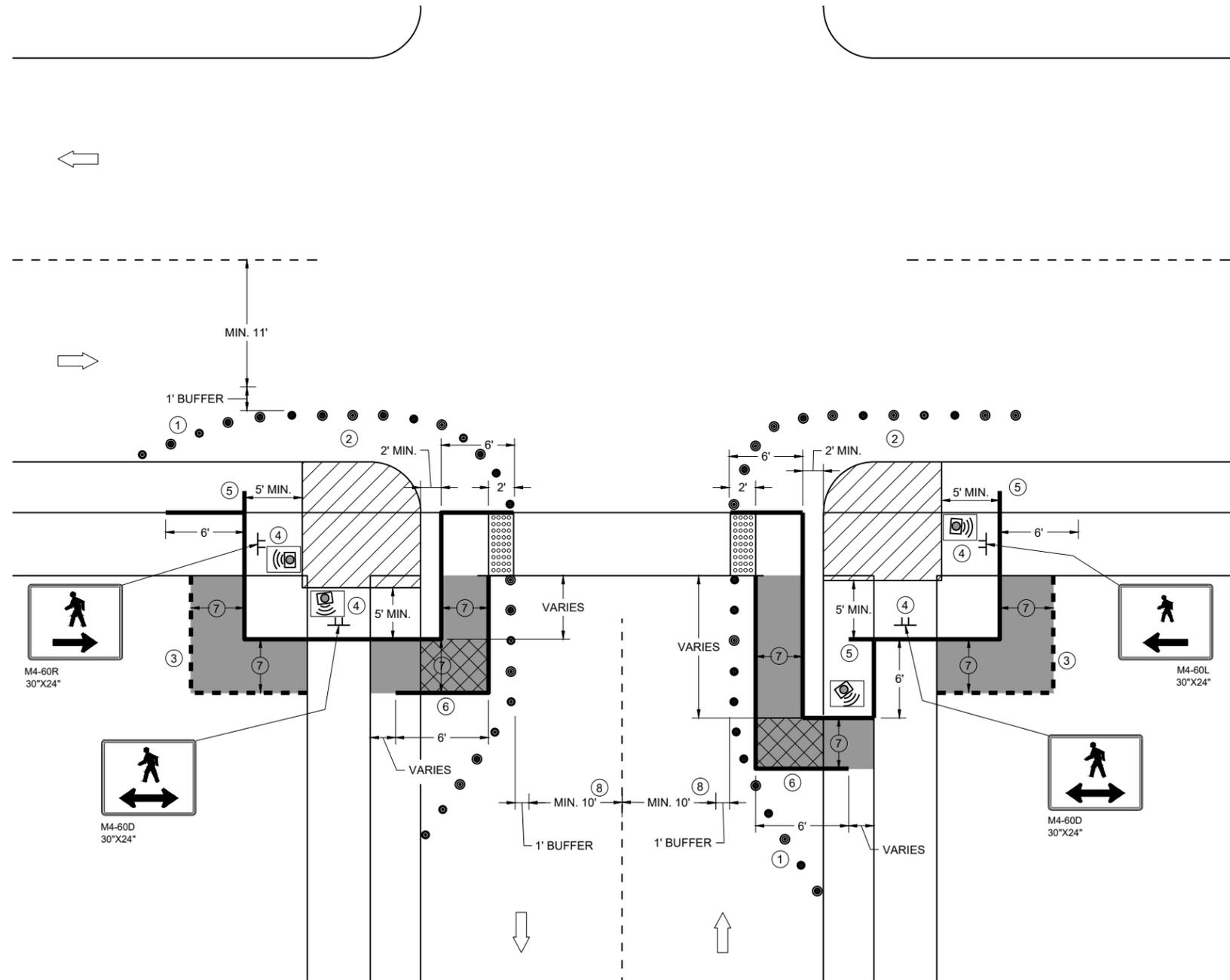
WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑦ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑧ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC
-  TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)



**CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE**

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

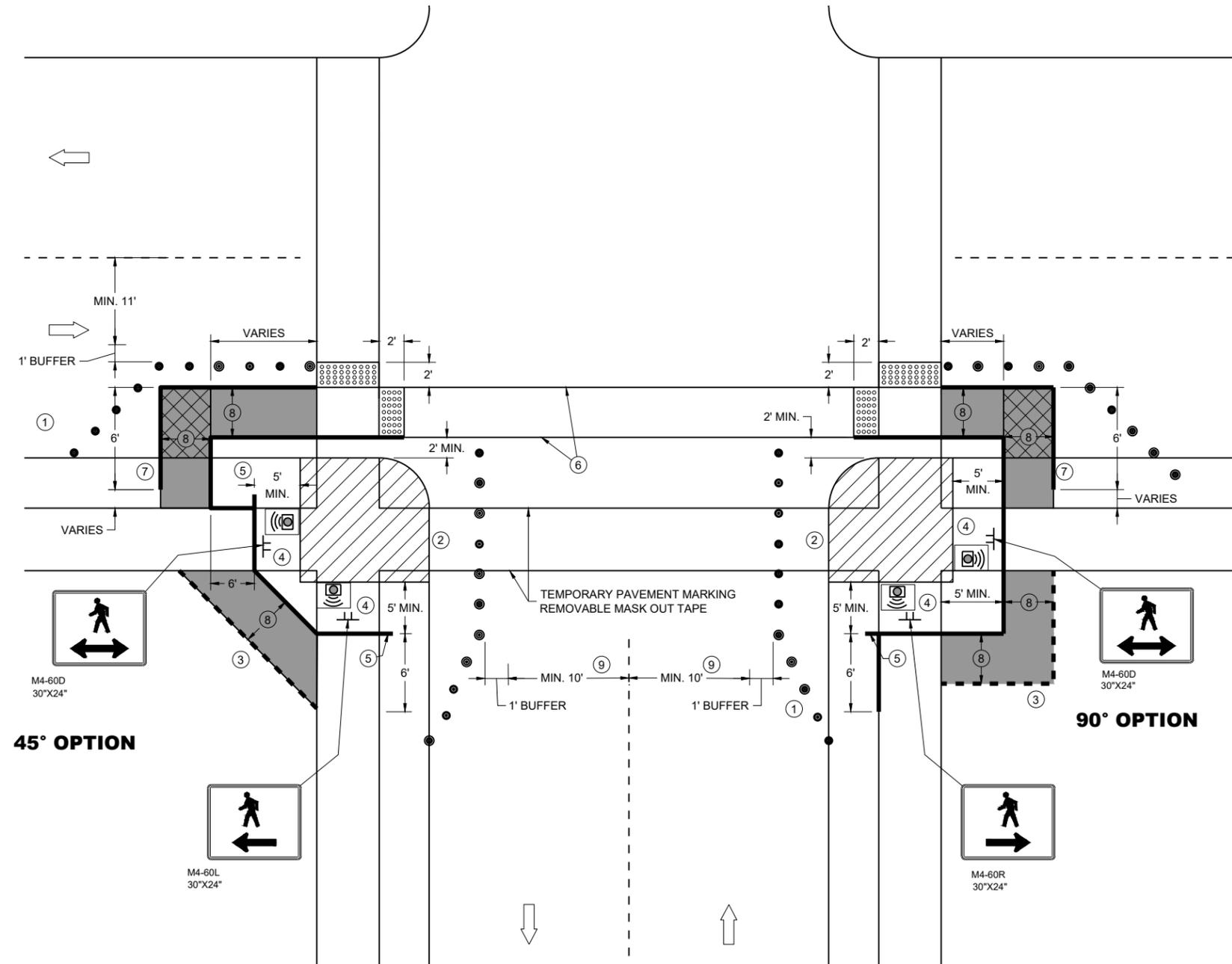
WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ WHITE 6" TEMPORARY PAVEMENT MARKING
- ⑦ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑧ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑨ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC
-  TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)



45° OPTION

90° OPTION

CURB RAMP PEDESTRIAN TRAFFIC CONTROL

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

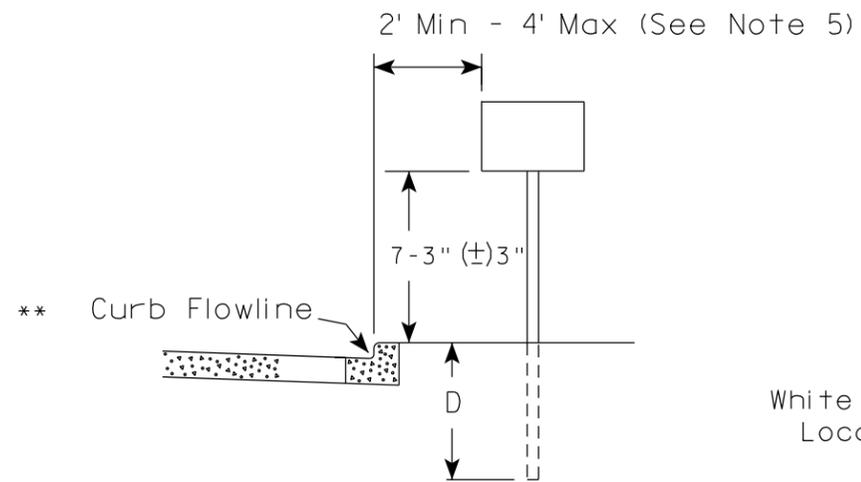
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SDD 15D30-12j

SDD 15D30-12j

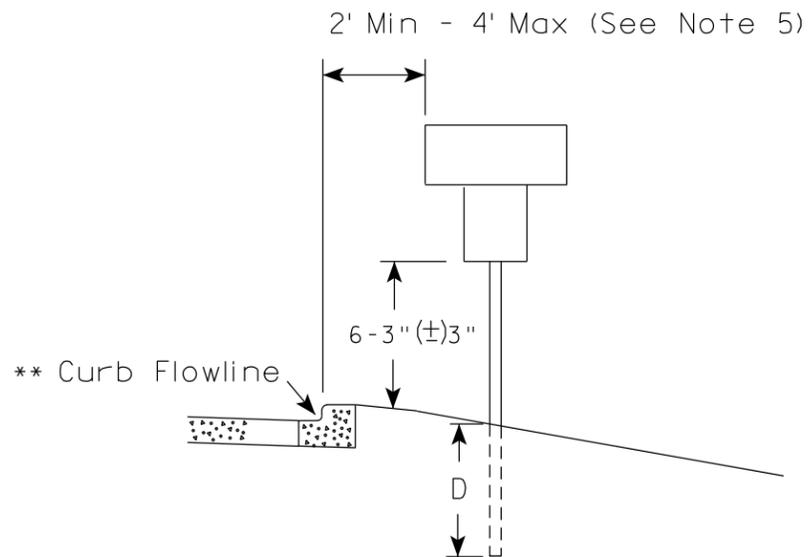
URBAN AREA

RURAL AREA (See Note 2)



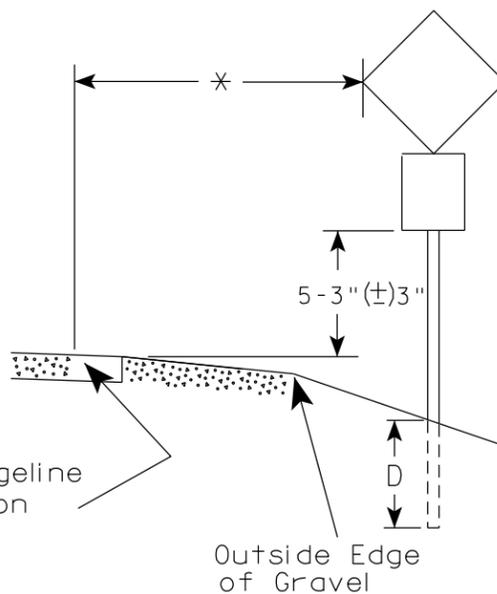
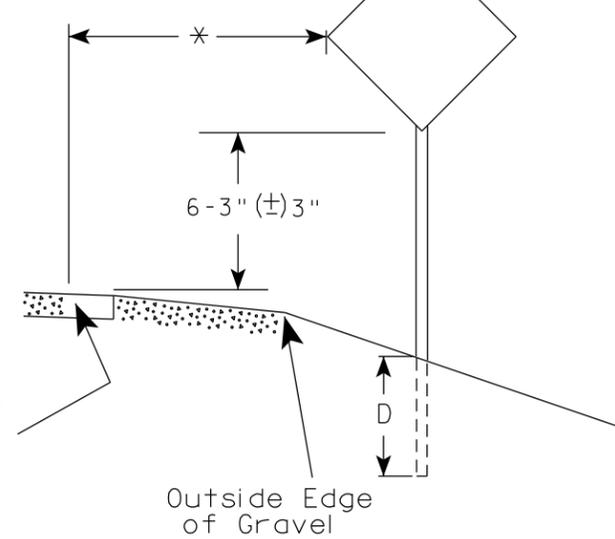
White Edgeline Location

Outside Edge of Gravel



White Edgeline Location

Outside Edge of Gravel



POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (± 3)". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (± 3)".
3. For expressways and freeways, mounting height is 7'- 3" (± 3)" or 6'-3" (± 3)" depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (± 3)".
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. Folding signs shall be mounted at a height of 5'-3" (± 3)" or as directed by the Engineer.

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

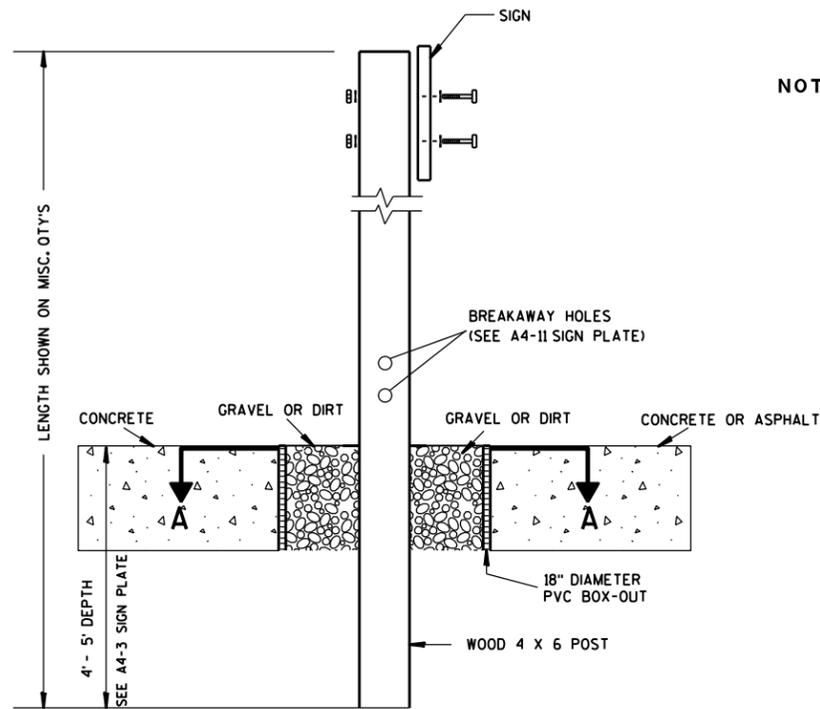
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Raub*
for State Traffic Engineer

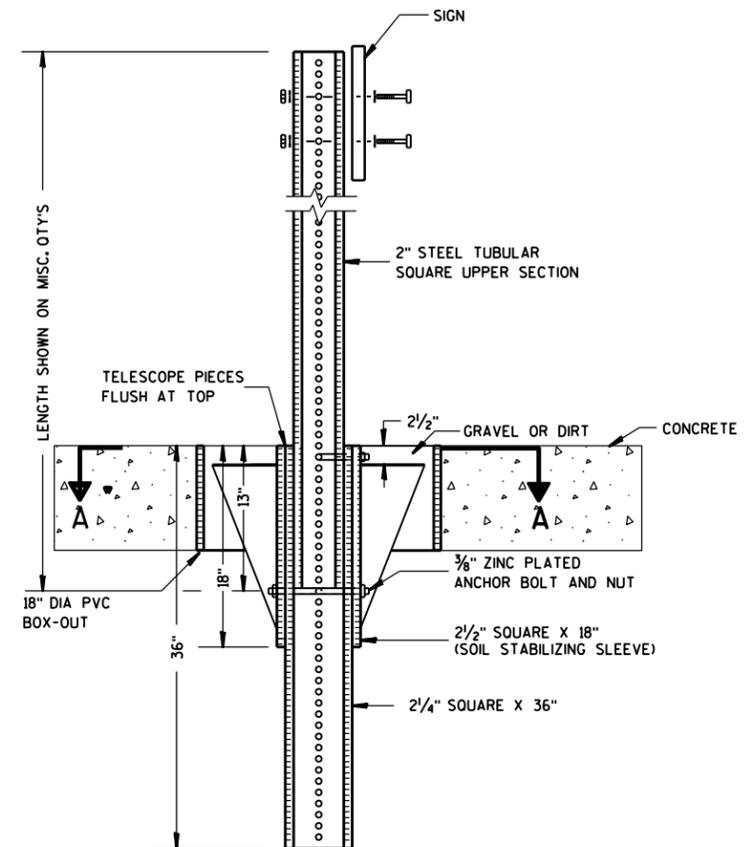
DATE 12/6/23 PLATE NO. A4-3.23



ELEVATION VIEW

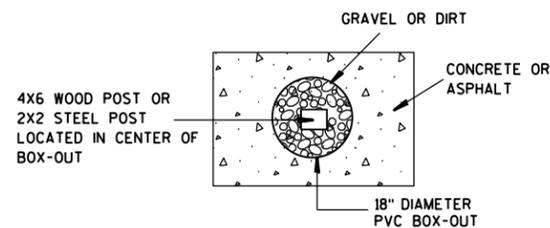
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B	
<small>WISCONSIN DEPT OF TRANSPORTATION</small>	
APPROVED <i>Matthew R. Rauch</i> <small>for State Traffic Engineer</small>	
<small>DATE 1/27/14</small>	<small>PLATE NO. A4-3B.1</small>

GENERAL NOTES

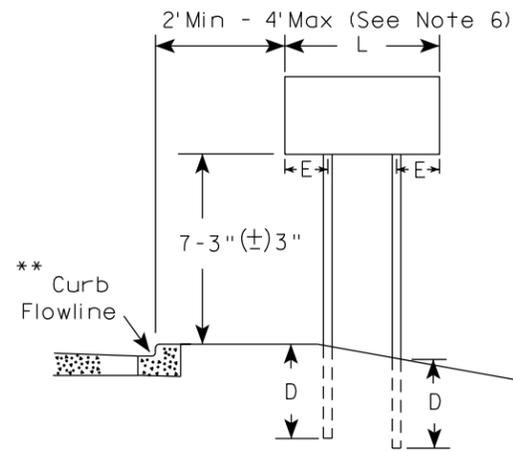
- For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
- See tables below for required number of posts.
- For expressways and freeways, mounting height is 7'-3" (\pm 3") or 6'-3" (\pm 3") depending upon existence of sub-sign.
- The (\pm) tolerance for mounting height is 3 inches.
- J-Assemblies are considered to be one sign for mounting height.
- Offset distance shall be consistent with existing signs or consistent throughout length of project.
- Folding signs shall be mounted at a height of 5'-3" (\pm 3") or as directed by the engineer.
- The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (\pm 3"). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (\pm 3").

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

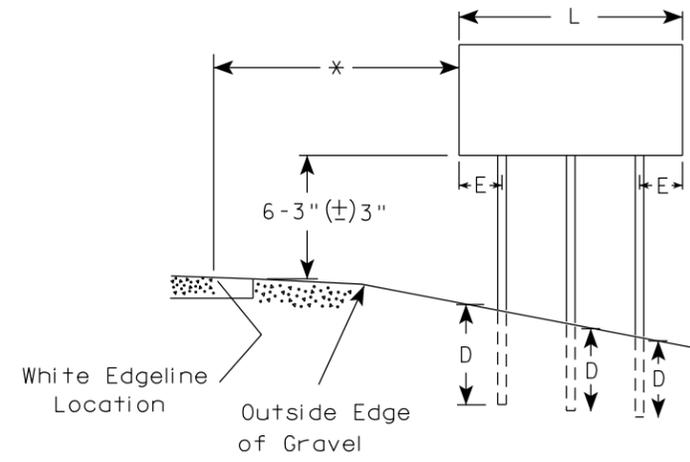
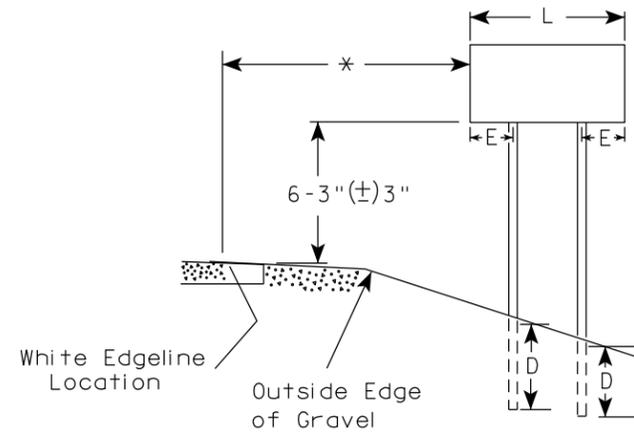
** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

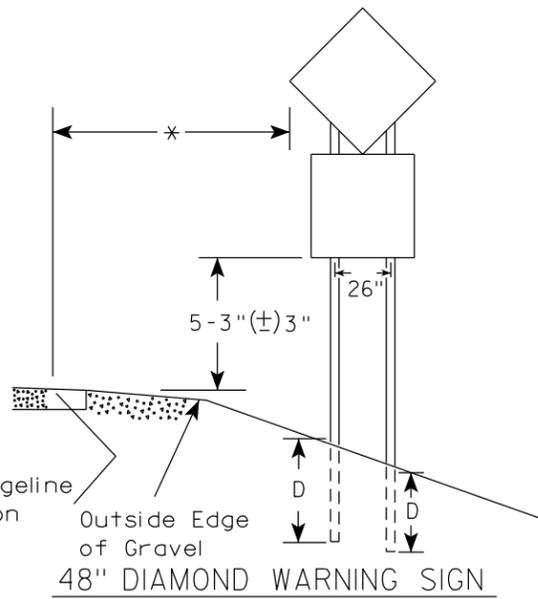
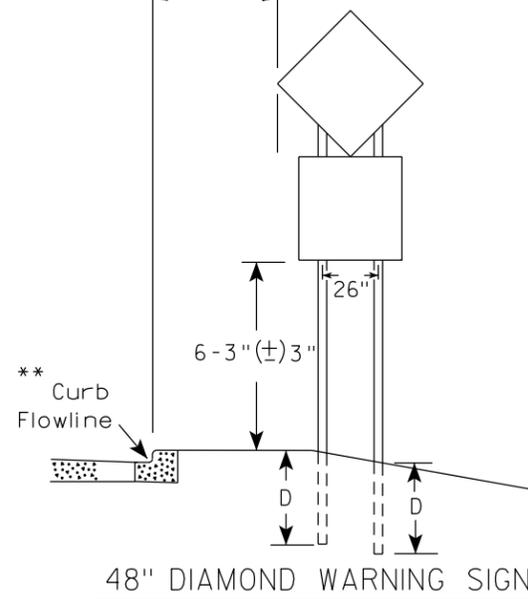
URBAN AREA



RURAL AREA (See Note 3)



URBAN AREA



SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

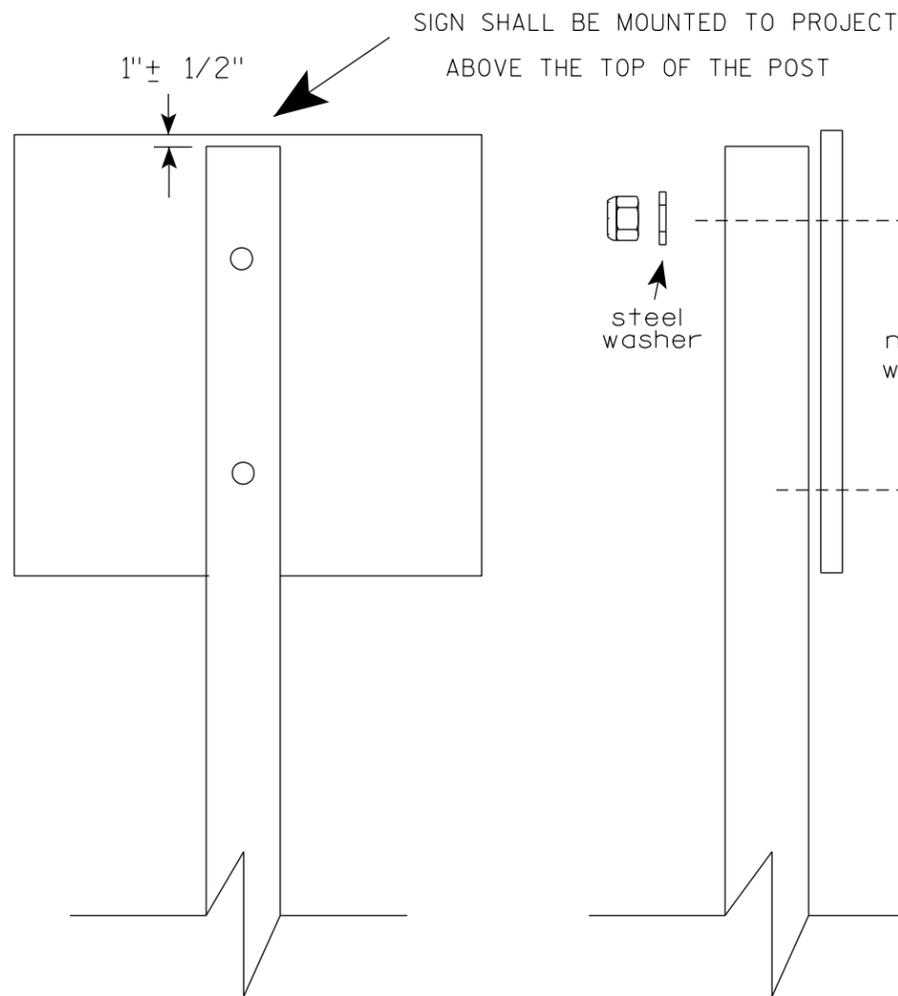
SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION
 APPROVED *Matthew R Rauch*
 For State Traffic Engineer
 DATE 12/6/23 PLATE NO. A4-4.16



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

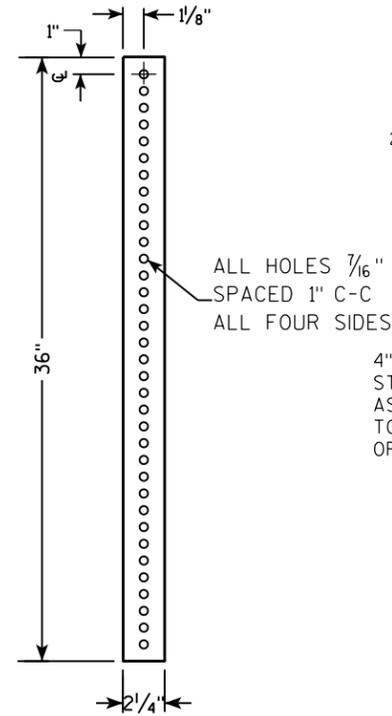
- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
- 1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

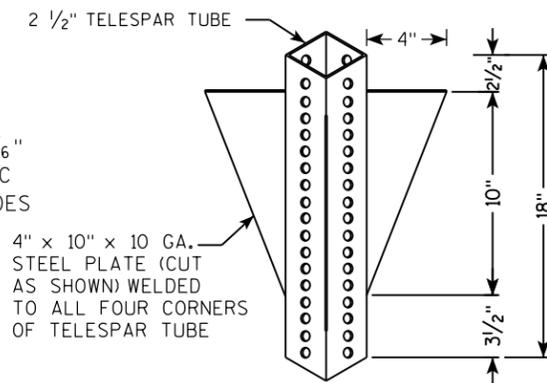
ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R Rauch</i> For State Traffic Engineer
DATE 4/1/2020	PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

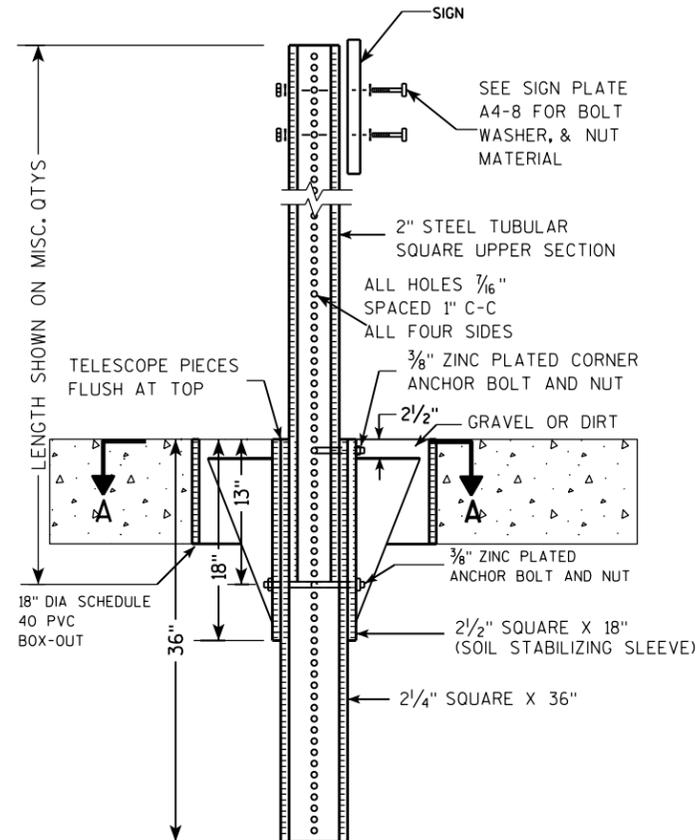
2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



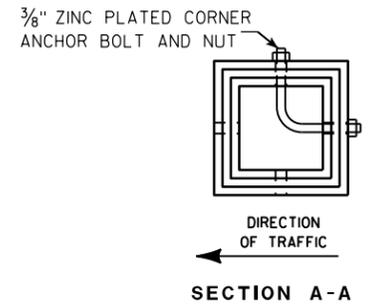
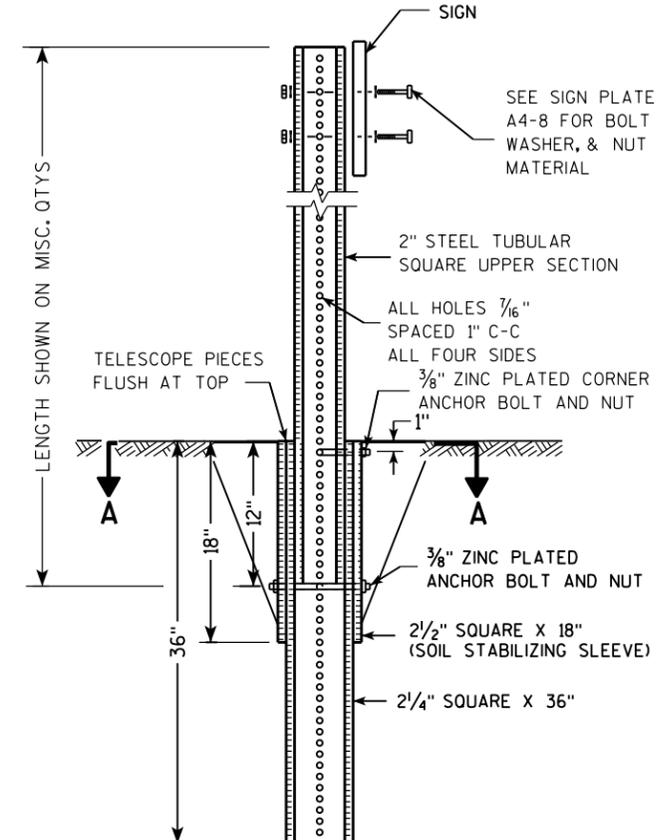
2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

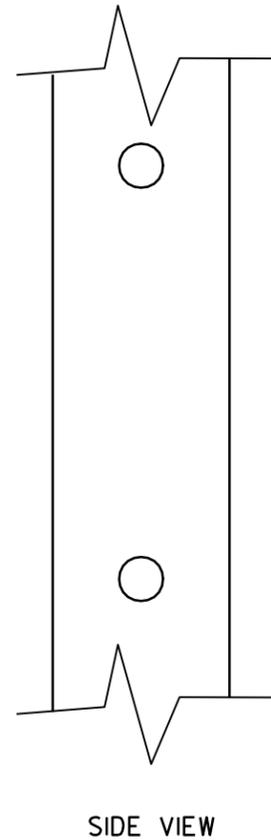
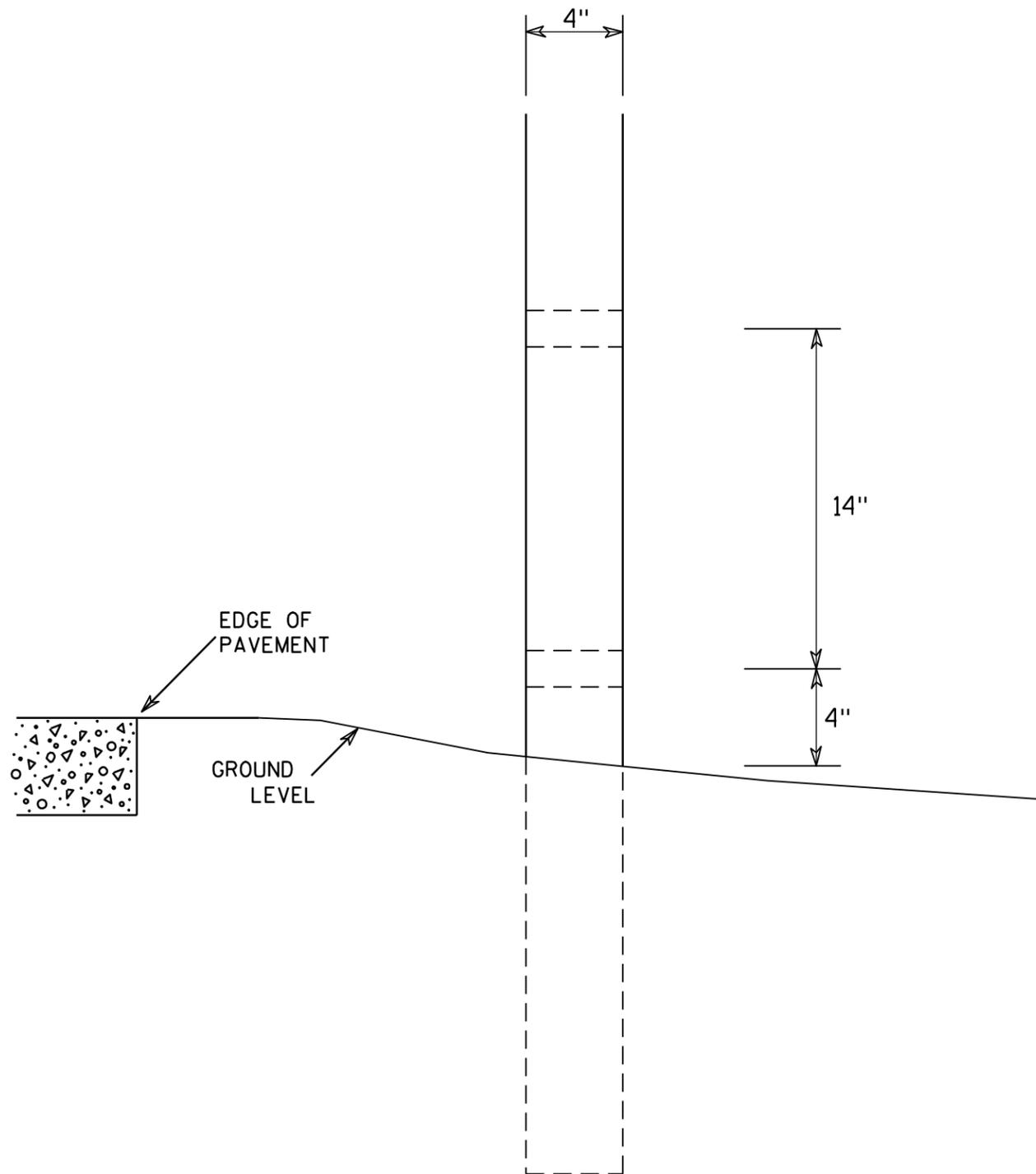
Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

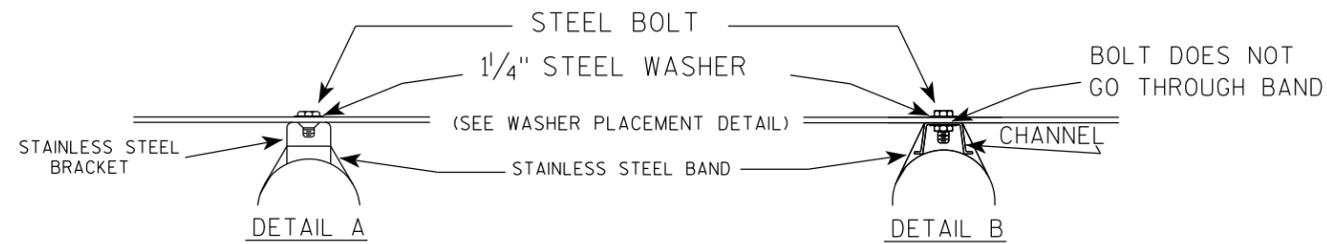
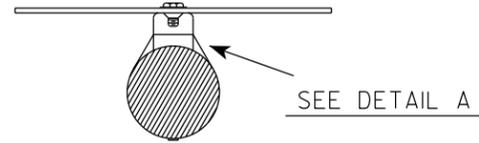
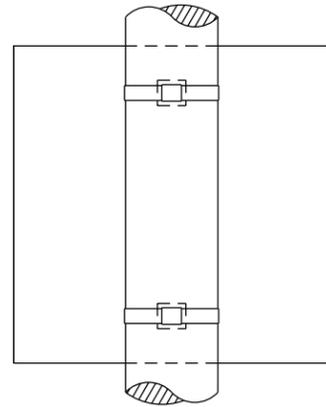
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7

4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	 <small>for State Traffic Engineer</small>
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

BANDING

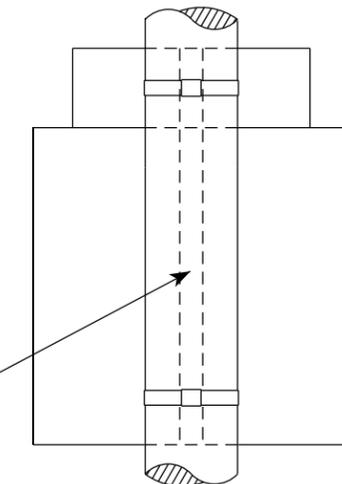
SINGLE SIGN



GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

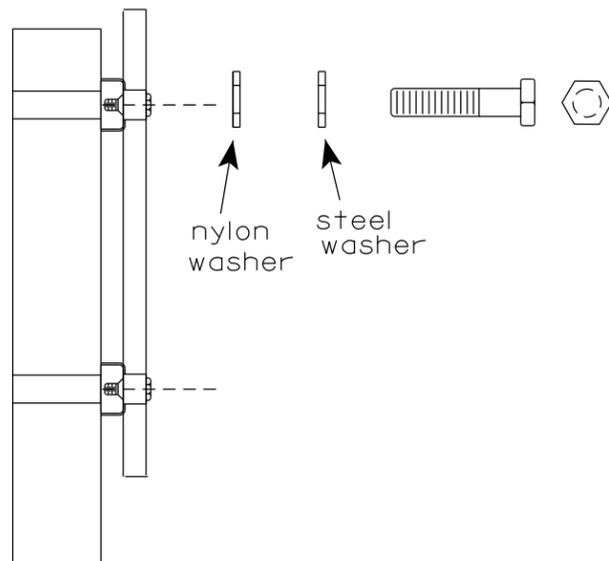
"J" ASSEMBLY



CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET

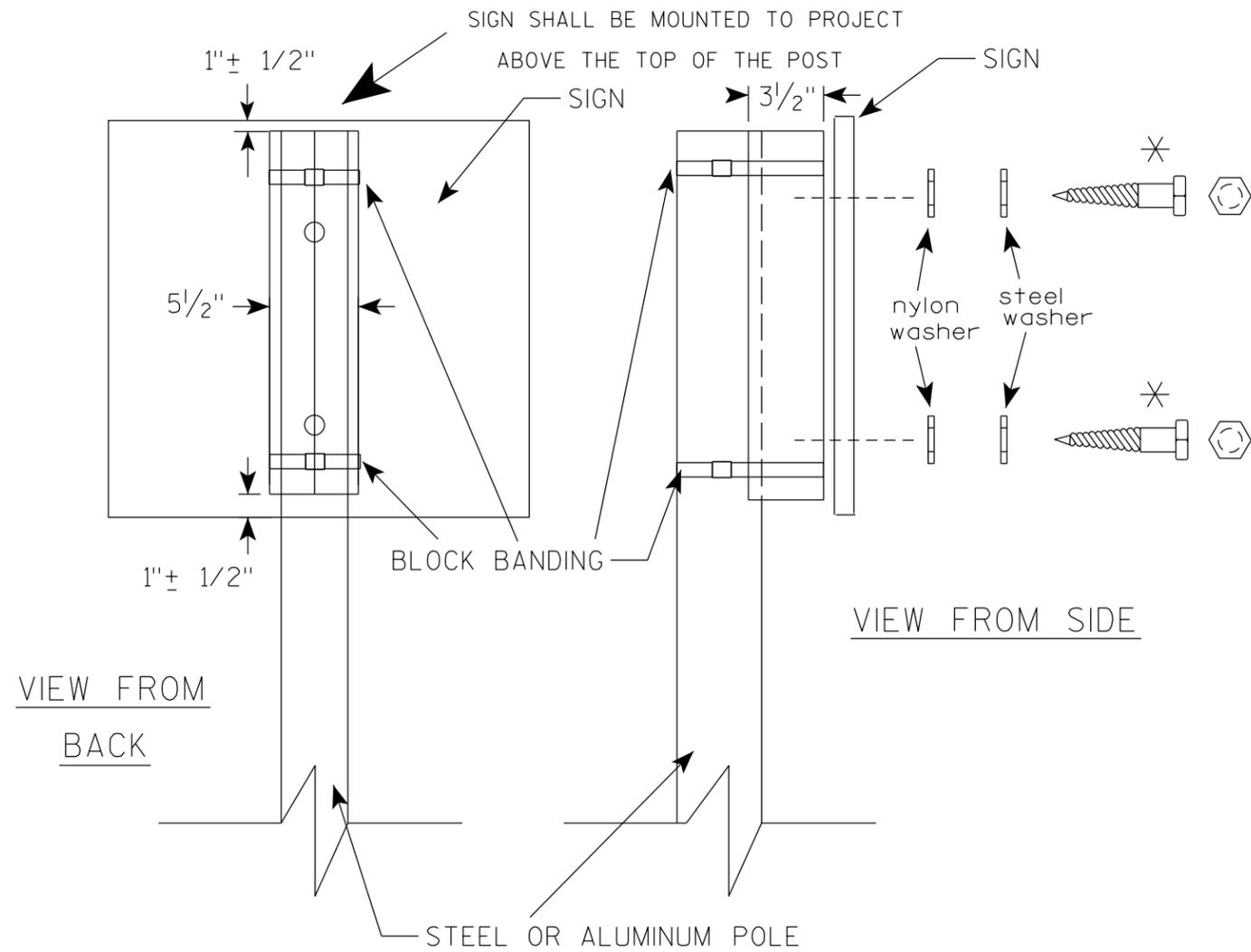


WASHER PLACEMENT



WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

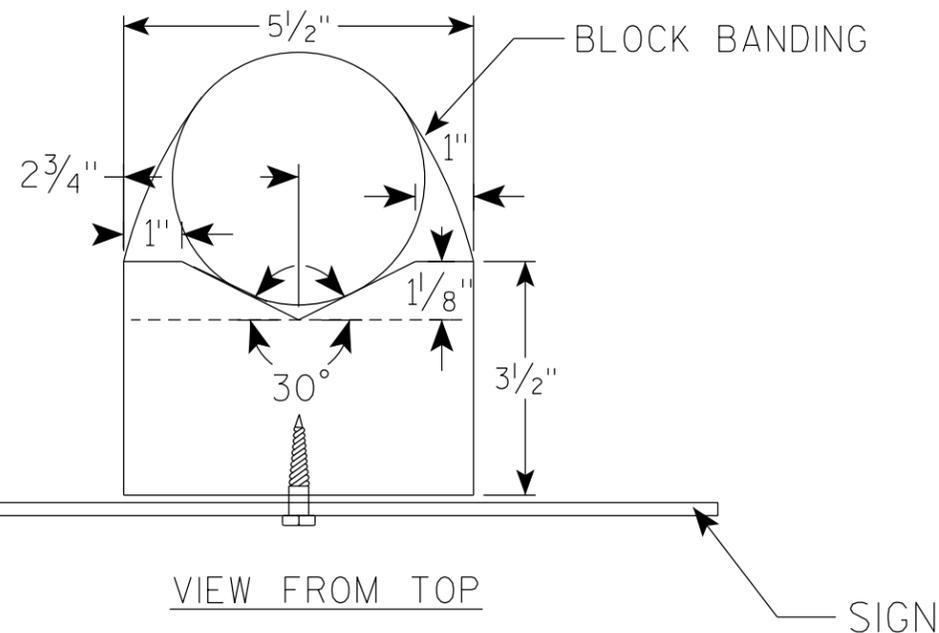
STANDARD SIGN
SIGN BANDING DETAILS
WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 6/10/19 PLATE NO. A5-9.4



GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✱ LAG BOLTS SHALL BE 3/8" X 2 1/2"



BLOCK BANDING DETAIL
(V-BLOCK OPTION)

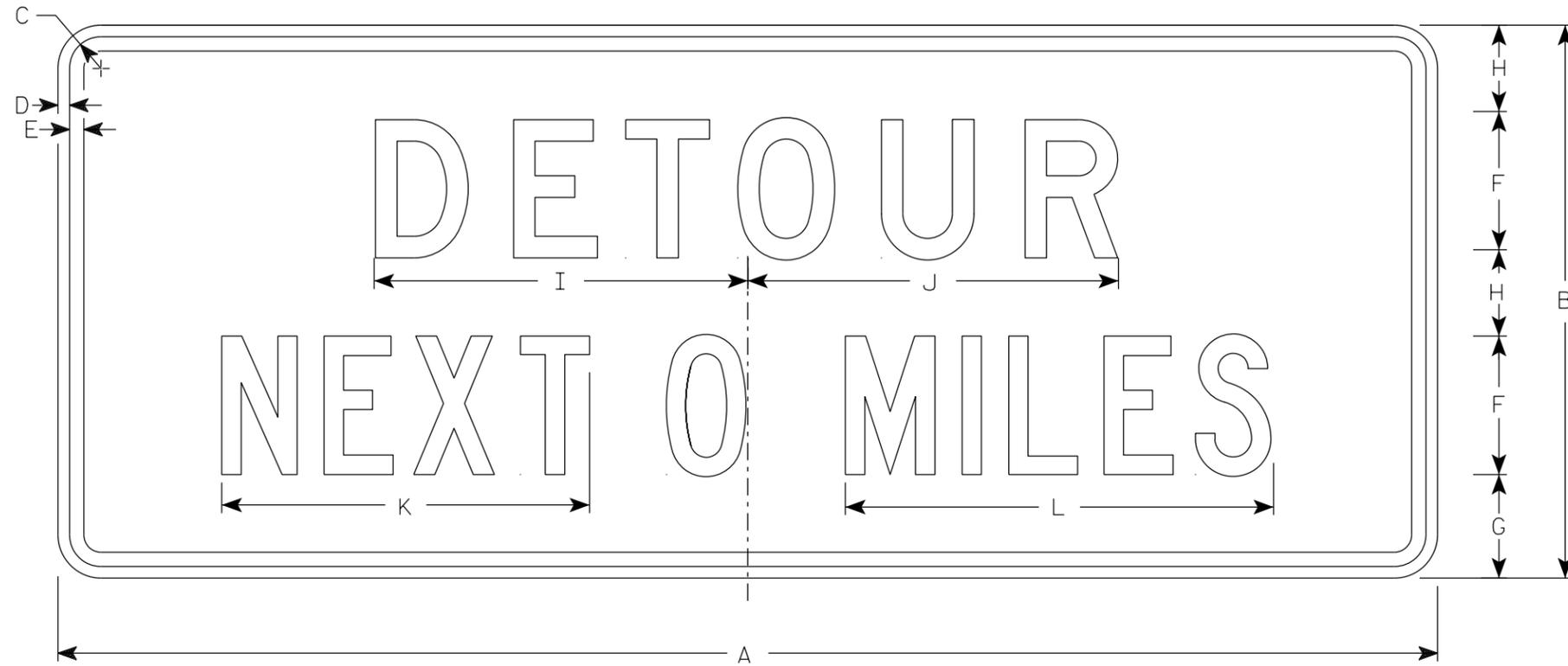
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 4/19/2022 PLATE NO. A5-10.3

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - Line 1 is D and Line 2 is C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Round distance to nearest whole Mile and substitute appropriate numerals and optically adjust spacing to achieve proper balance



G20-51

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	60	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	16 1/4	16 1/8	16	18 5/8															10.0
2M	60	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	16 1/4	16 1/8	16	18 5/8															10.0
3	60	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	16 1/4	16 1/8	16	18 5/8															10.0
4	60	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	16 1/4	16 1/8	16	18 5/8															10.0
5	60	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	16 1/4	16 1/8	16	18 5/8															10.0

STANDARD SIGN
G20-51

WISCONSIN DEPT OF TRANSPORTATION

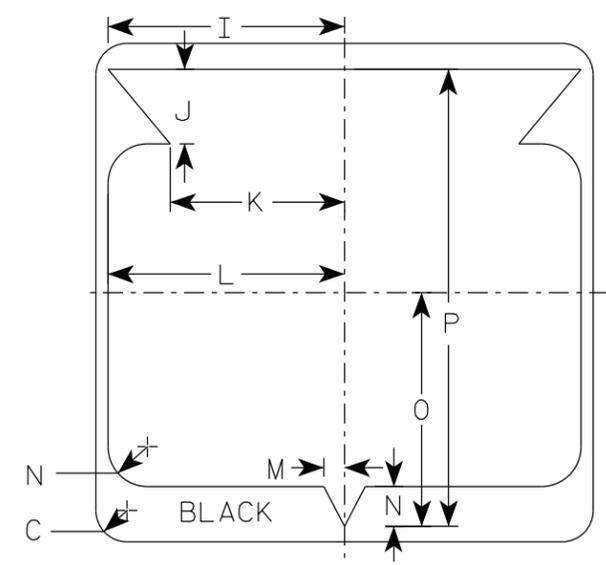
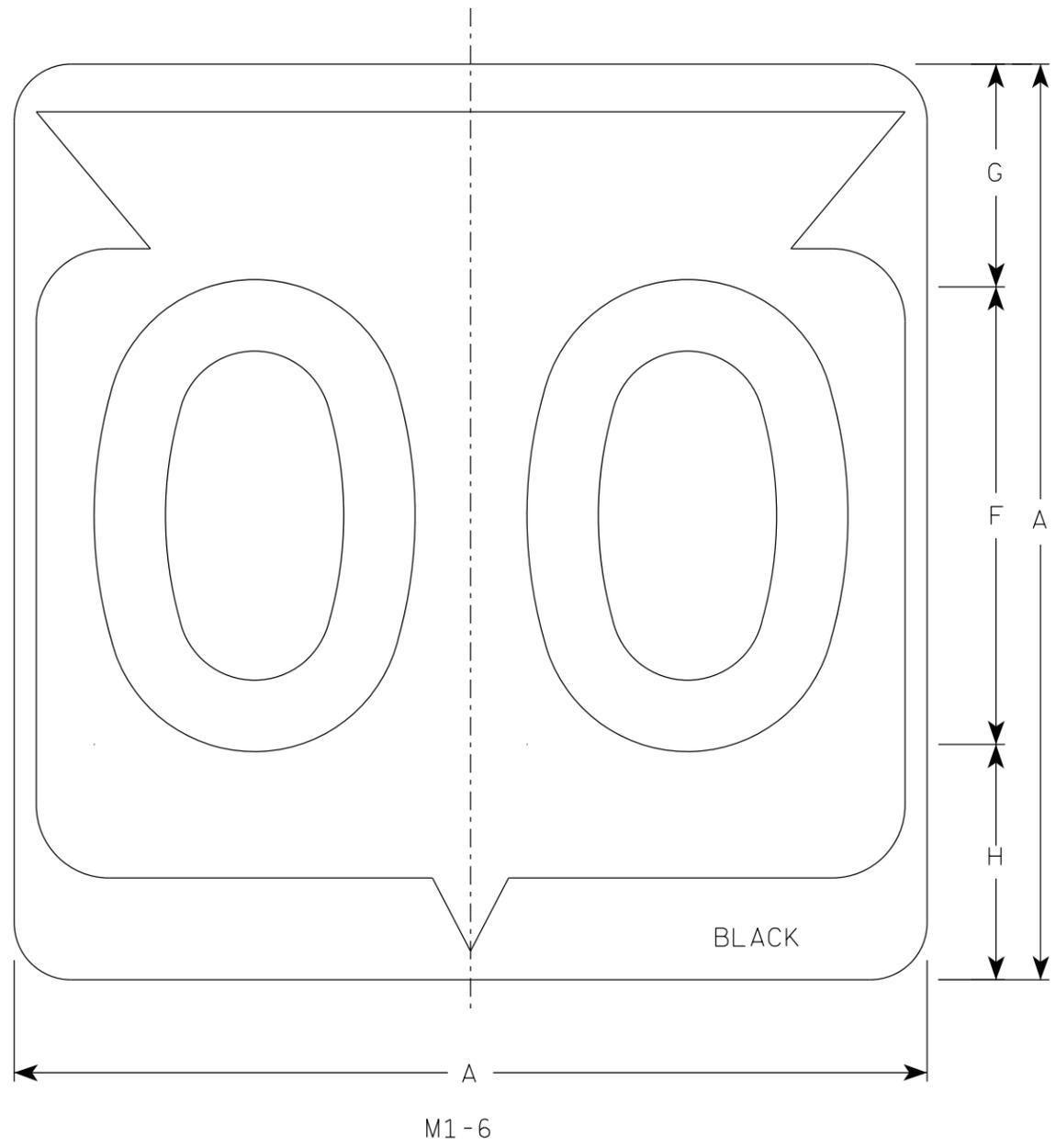
APPROVED *Matthew R. Rauch*
State Traffic Engineer

DATE 1/26/2023 PLATE NO. G20-51.3

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D except 3 number signs Series C



7

7

M1-6

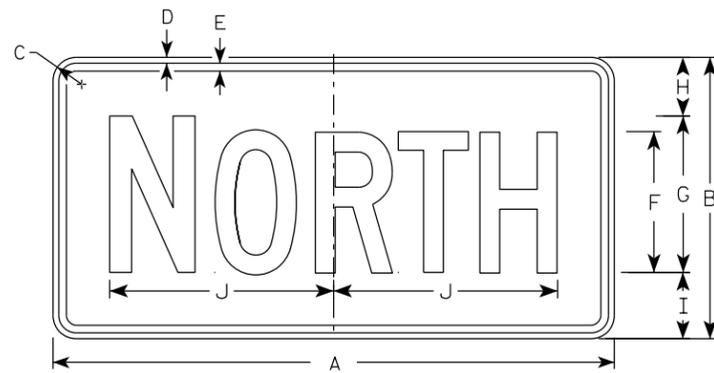
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
2M	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33										9.0	
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33										9.0	
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33										9.0	

STATE ROUTE MARKER
M1-6 FOR ASSEMBLIES

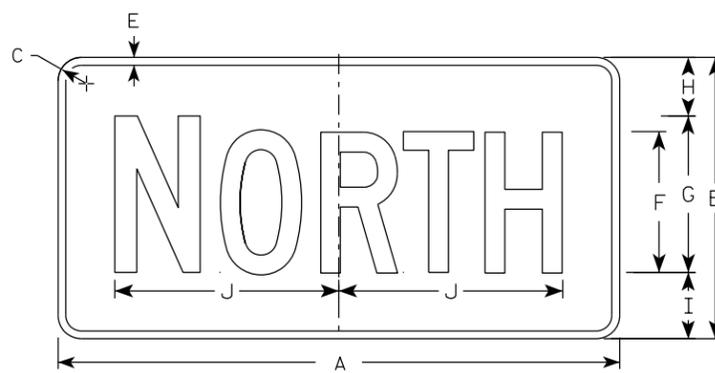
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Raub*
for State Traffic Engineer

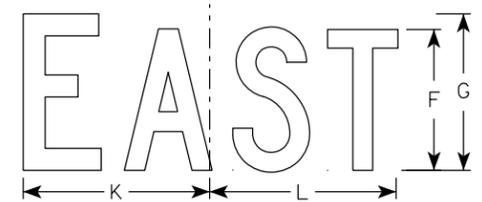
DATE 11/8/2022 PLATE NO. M1-6.11



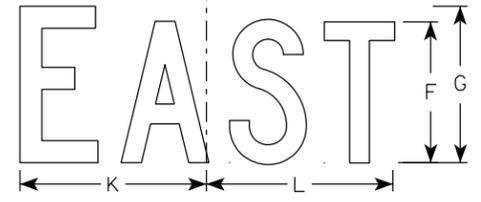
M3-1
MM3-1
MP3-1



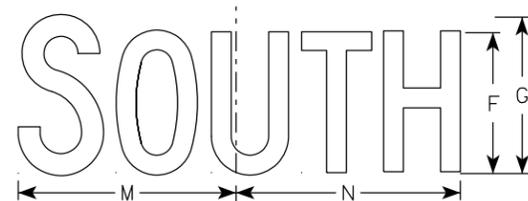
MB3-1
MK3-1
MN3-1



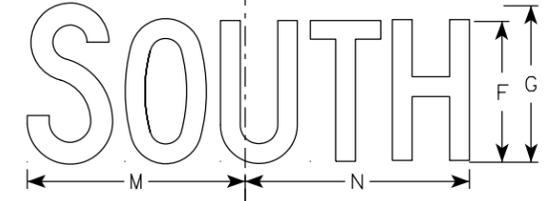
M3-2
MM3-2
MP3-2



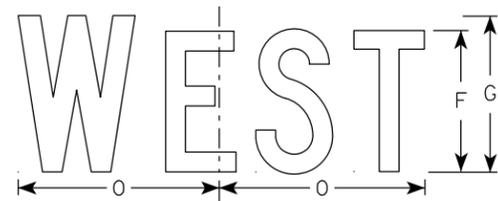
MB3-2
MK3-2
MN3-2



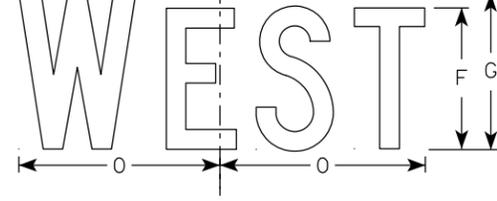
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

NOTES

- All Signs Type II - Type H Reflective
- Color:
Background - See note 5
Message - See note 5
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M3-1 thru M3-4 Background - White
Message - Black
MB3-1 thru MB3-4 Background - Blue
Message - White
MK3-1 thru MK3-4 Background - Green
Message - White
MM3-1 thru MM3-4 Background - White
Message - Green
MN3-1 thru MN3-4 Background - Brown
Message - White
MP3-1 thru MP3-4 Background - White
Message - Blue
- Note the first letter of each direction is larger than the remainder of the message.

7

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SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4												2.00
2M	24	12	1 1/2	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4												2.00
3	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5
4	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5
5	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5

STANDARD SIGNS
M3-1 THRU M3-4
SERIES

WISCONSIN DEPT OF TRANSPORTATION

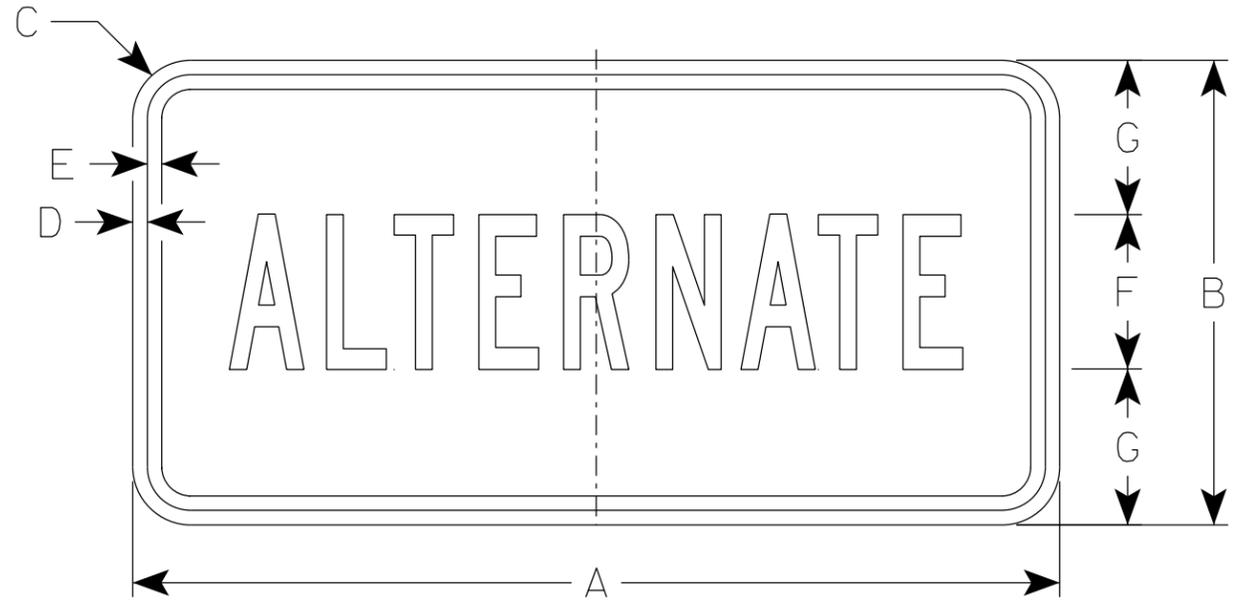
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/8/2023 PLATE NO. M3-1.15

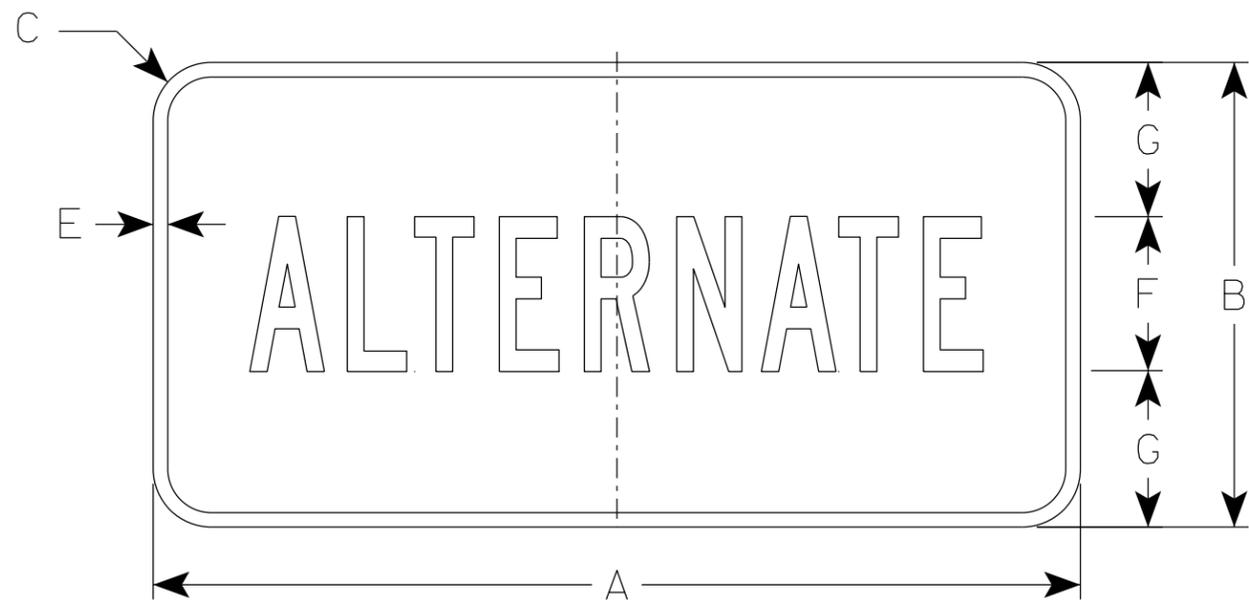
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective except as Shown
2. Color:
Background - See Note 5
Message - See note 5
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-1 Background - White
Message - Black
MB4-1 Background - Blue
Message - White
M04-1 Background - Orange - Type F Reflective
Message - Black



M4 - 1
M04 - 1



MB4 - 1

7

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SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	3/8	3/8	4	4	9 3/4	9 1/2																		2.00
2M	24	12	1 1/2	3/8	3/8	4	4	9 3/4	9 1/2																		2.00
3	36	18	1 1/2	3/8	1/2	7	5 1/2	16 3/8	16 1/2																		4.5
4	36	18	1 1/2	3/8	1/2	7	5 1/2	16 3/8	16 1/2																		4.5
5	36	18	1 1/2	3/8	1/2	7	5 1/2	16 3/8	16 1/2																		4.5

STANDARD SIGN
M4 - 1

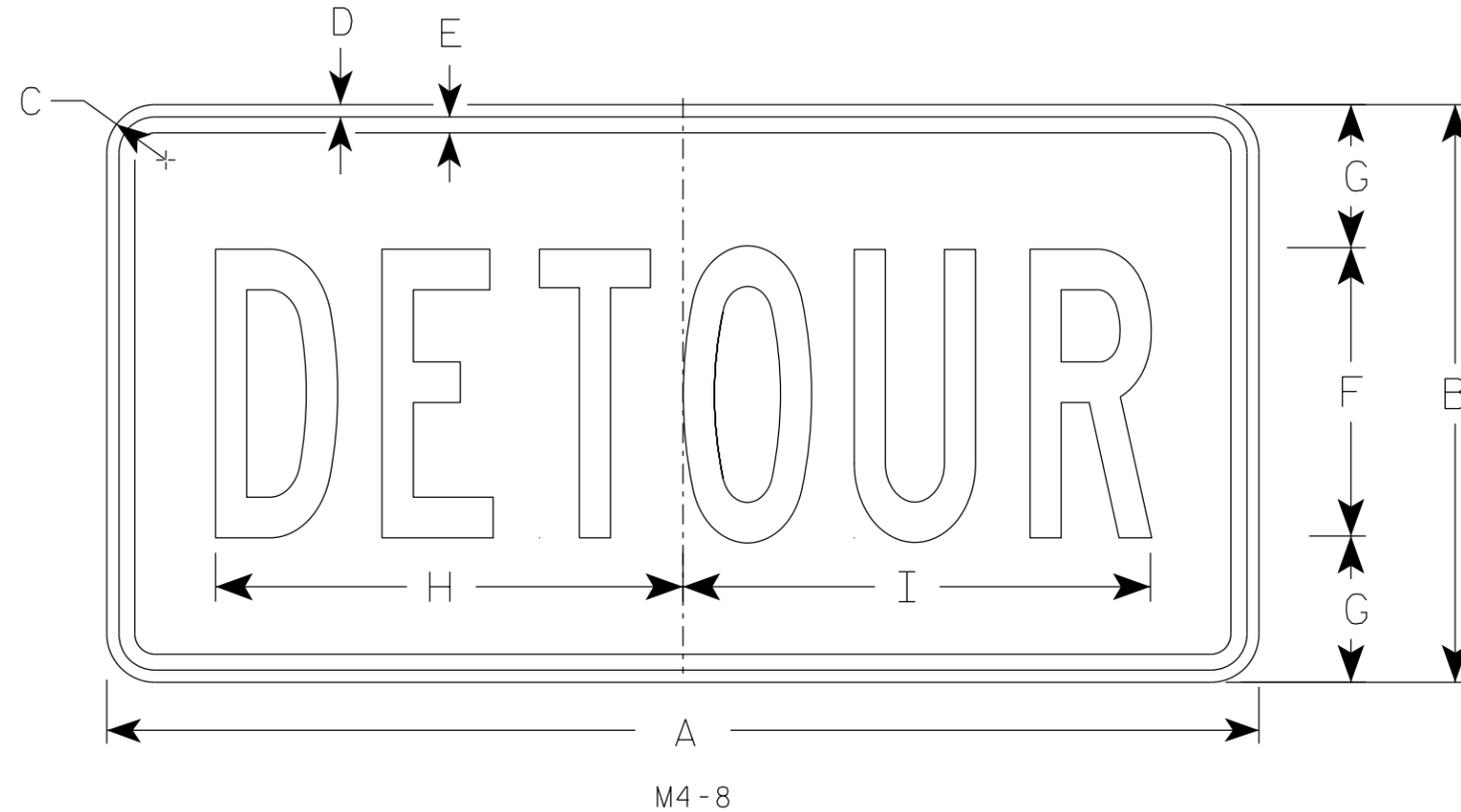
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/8/2023 PLATE NO. M4-1.10

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

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SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/2	3/8	3/8	6	3	10	10 1/4																		2.0
2M	24	12	1 1/2	3/8	3/8	6	3	10	10 1/4																		2.0
3	36	18	1 1/2	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
4	36	18	1 1/2	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
5	36	18	1 1/2	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5

STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

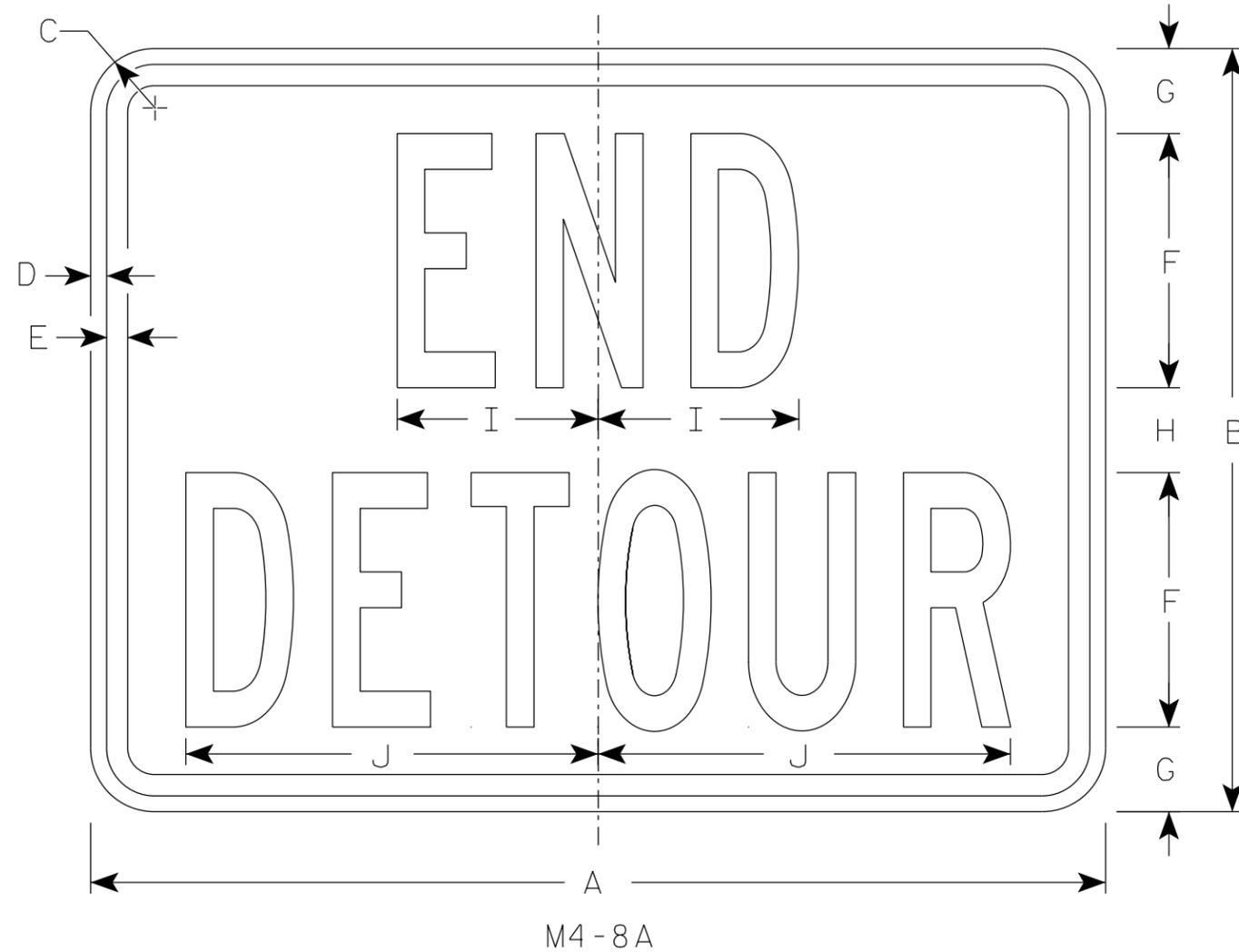
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/9/2023 PLATE NO. M4-8.4

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



M4-8A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 1/2	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
2M	24	18	1 1/2	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
3	30	24	1 1/2	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
4	30	24	1 1/2	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
5	30	24	1 1/2	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0

STANDARD SIGN
M4-8A

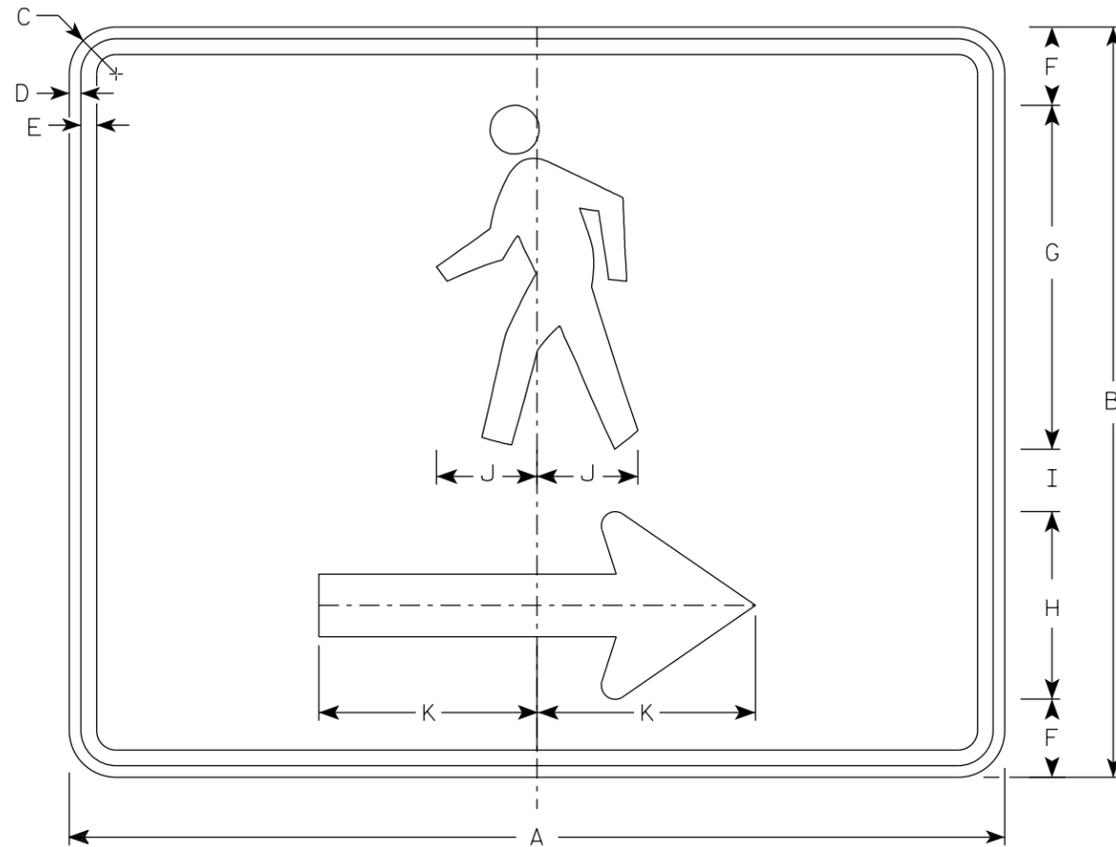
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

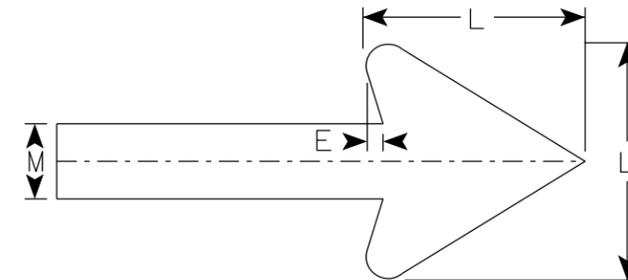
DATE 2/9/2023 PLATE NO. M4-8A.4

NOTES

1. Sign is Type II- Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M4-60L is the same as M4-60R except the arrow is reversed.



M4-60R



Arrow Detail

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	24	1 1/2	3/8	1/2	2 1/2	11	6	2	3 1/4	7	6	2														5.00
2M	30	24	1 1/2	3/8	1/2	2 1/2	11	6	2	3 1/4	7	6	2														5.00
3																											
4																											
5																											

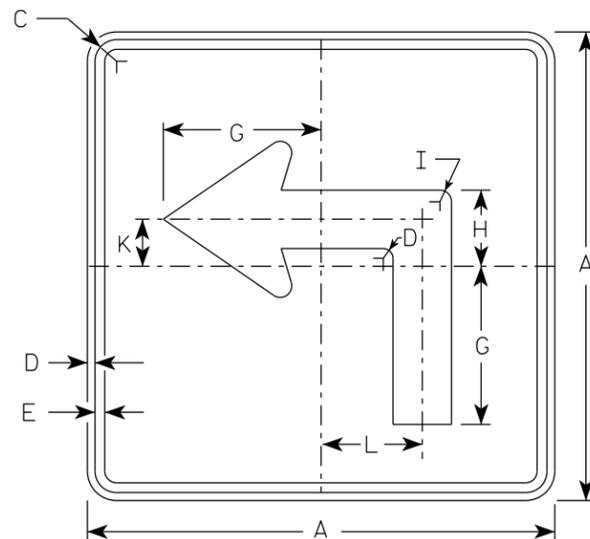
STANDARD SIGN
M4-60 L&R

WISCONSIN DEPT OF TRANSPORTATION

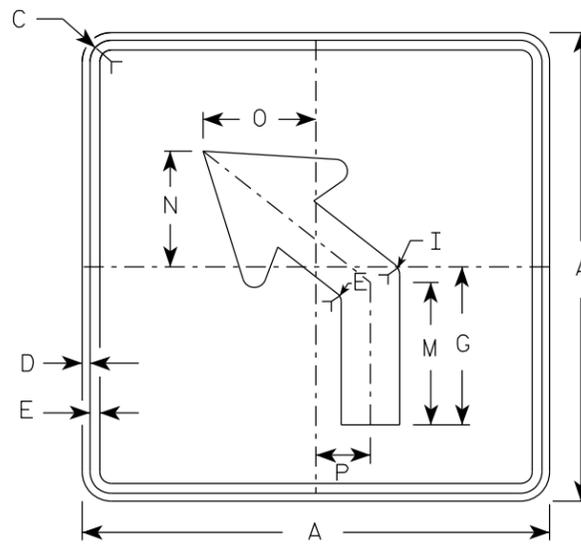
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 2/14/2023 PLATE NO. M4-60.2

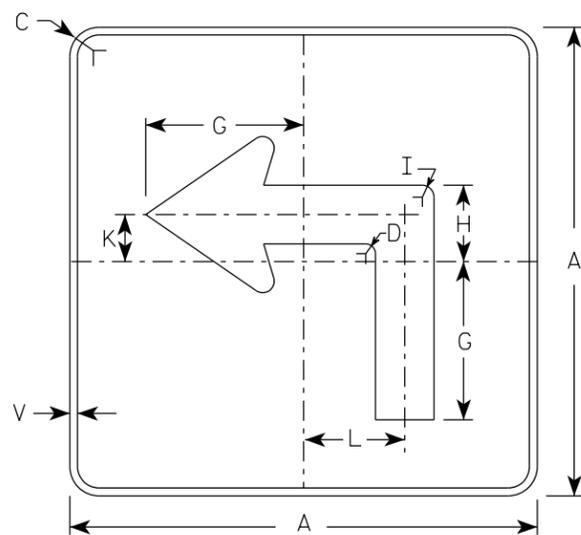
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**



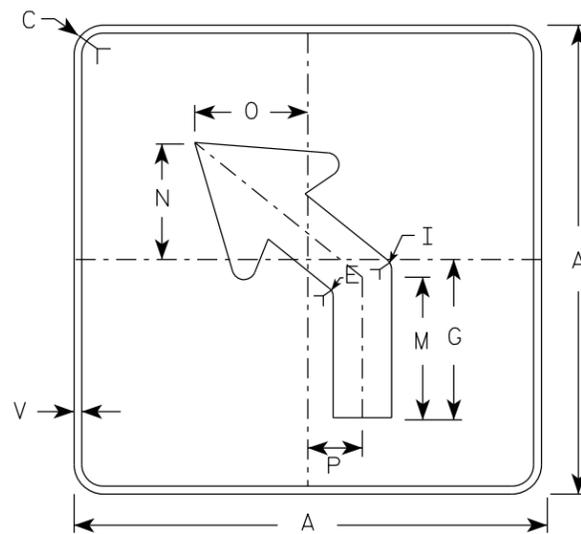
M5-1L
MM5-1L
M05-1L
MP5-1L



M5-2L
MM5-2L
M05-2L
MP5-2L

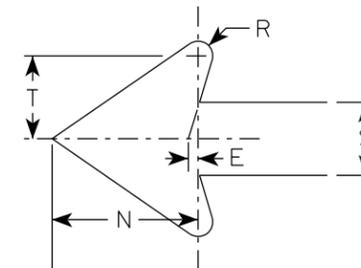


MB5-1L
MK5-1L
MN5-1L
MR5-1L



MB5-2L
MK5-2L
MN5-2L
MR5-2L

ARROW DETAIL



NOTES

- Signs are Type II - Type H reflective except as shown
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- | | |
|-----------------|---|
| M5-1 and M5-2 | Background - White |
| | Message - Black |
| MB5-1 and MB5-2 | Background - Blue |
| | Message - White |
| MK5-1 and MK5-2 | Background - Green |
| | Message - White |
| MM5-1 and MM5-2 | Background - White |
| | Message - Green |
| MN5-1 and MN5-2 | Background - Brown |
| | Message - White |
| M05-1 and M05-2 | Background - Orange - Type F Reflective |
| | Message - Black |
| MP5-1 and MP5-2 | Background - White |
| | Message - Blue |
| MR5-1 and MR5-2 | Background - Brown |
| | Message - Yellow |
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	21		1 1/2	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3		1/2					3.06
2M	21		1 1/2	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3		1/2					3.06
3	30		1 7/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4		1/2					6.25
4	30		1 7/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4		1/2					6.25
5	30		1 7/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4		1/2					6.25

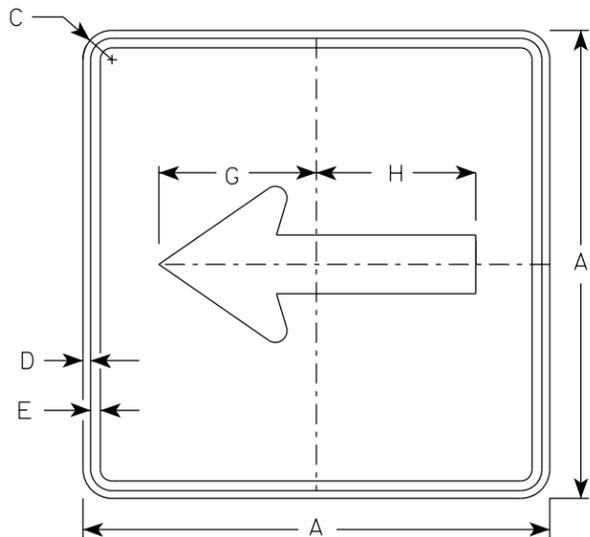
STANDARD SIGN
M5-1 & M5-2

WISCONSIN DEPT OF TRANSPORTATION

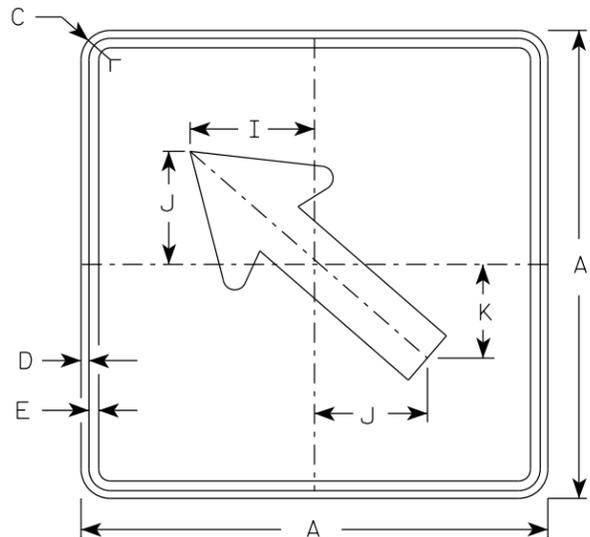
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/13/2023 PLATE NO. M5-1.15

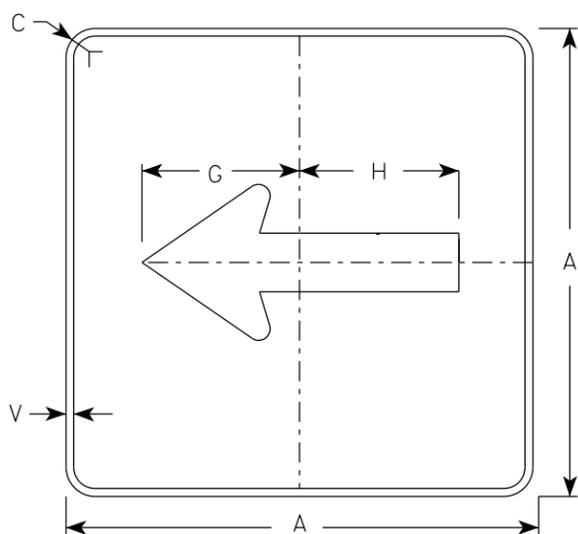
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



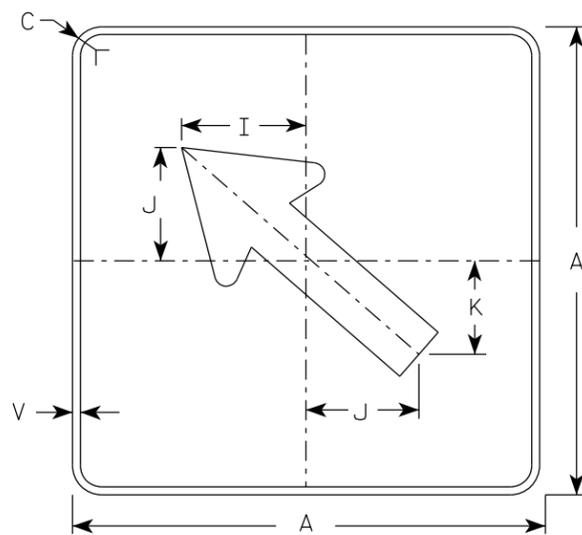
M6-1
MM6-1
M06-1
MP6-1



M6-2
MM6-2
M06-2
MP6-2



MB6-1
MK6-1
MN6-1
MR6-1

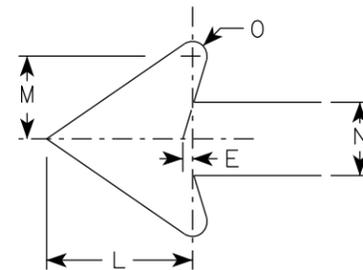


MB6-2
MK6-2
MN6-2
MR6-2

NOTES

- Signs are Type II - Type H Reflective except as Shown
- Color:
 - Background - See note 4
 - Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White
Message - Black
 MB6-1 and MB6-2 Background - Blue
Message - White
 MK6-1 and MK6-2 Background - Green
Message - White
 MM6-1 and MM6-2 Background - White
Message - Green
 MN6-1 and MN6-2 Background - Brown
Message - White
 M06-1 and M06-2 Background - Orange - Type F Reflective
Message - Black
 MP6-1 and MP6-2 Background - White
Message - Blue
 MR6-1 and MR6-2 Background - Brown
Message - Yellow

ARROW DETAIL



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	21		1 1/2	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2							1/2					3.06
2M	21		1 1/2	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2							1/2					3.06
3	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2					6.25
4	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2					6.25
5	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2					6.25

STANDARD SIGN
M6-1 & M6-2
SERIES

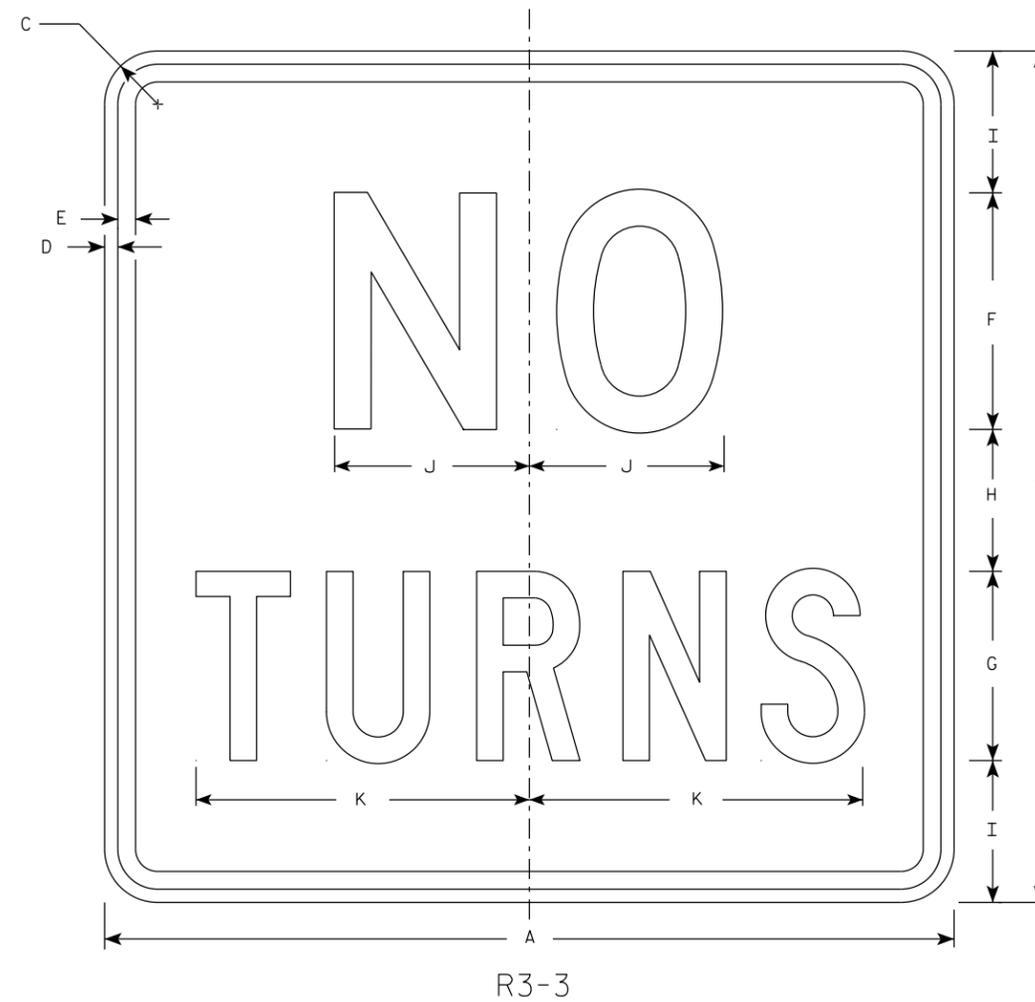
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 2/13/2023 PLATE NO. M6-1.16

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - See Note 4
4. Line 1 is Series D and Line 2 is Series C



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24		1 1/2	3/8	1/2	6	5	4	4 1/2	5 1/8	8 3/4																4.0
2M	36		2 1/4	5/8	3/4	10	8	6	6	8 1/4	14 1/8																9.0
3	36		2 1/4	5/8	3/4	10	8	6	6	8 1/4	14 1/8																9.0
4	36		2 1/4	5/8	3/4	10	8	6	6	8 1/4	14 1/8																9.0
5	48		2 1/4	3/4	1	12	10	8	9	10 1/4	17 5/8																16.0

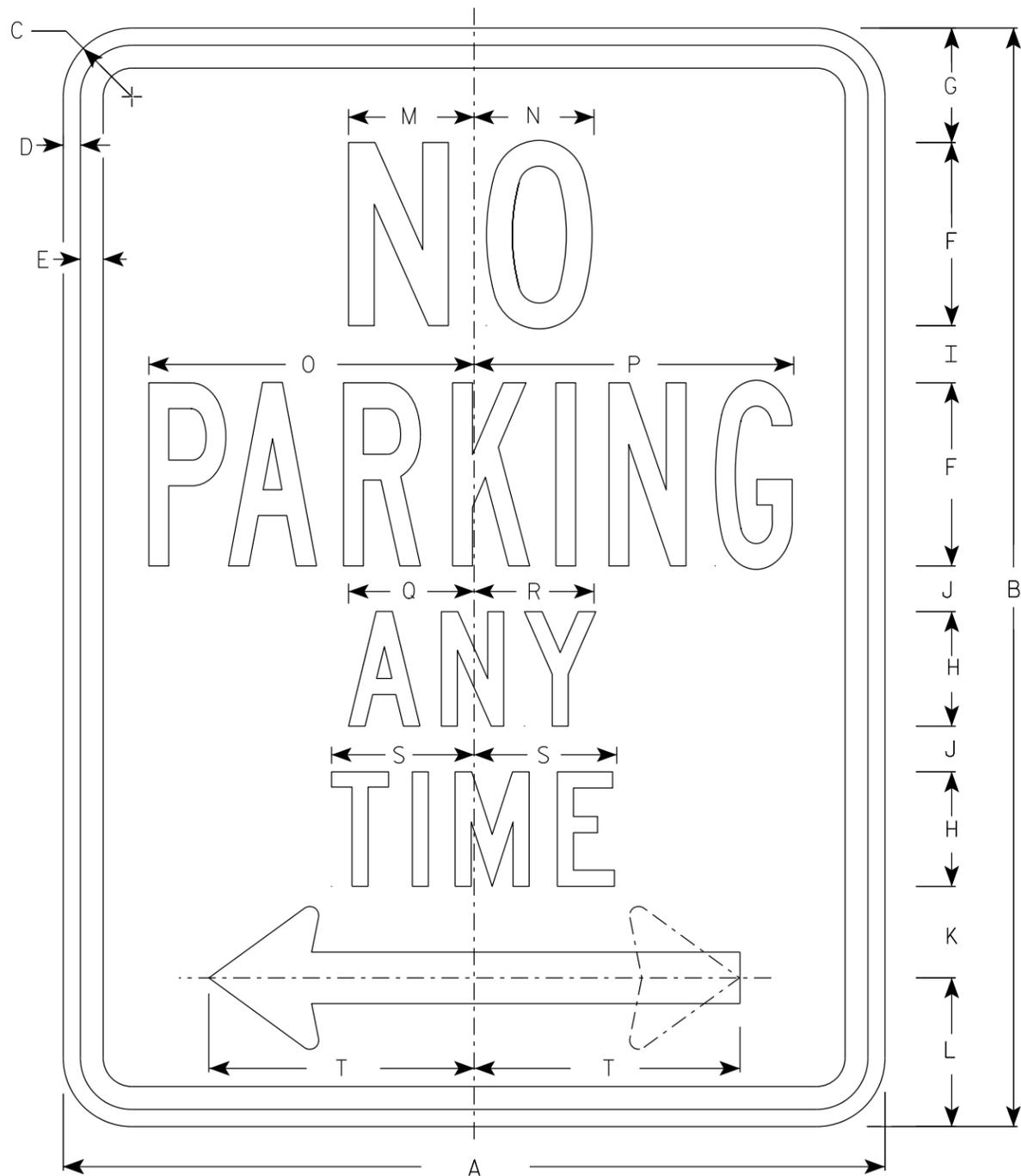
STANDARD SIGN
R3-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/13/23 PLATE NO. R3-3.11

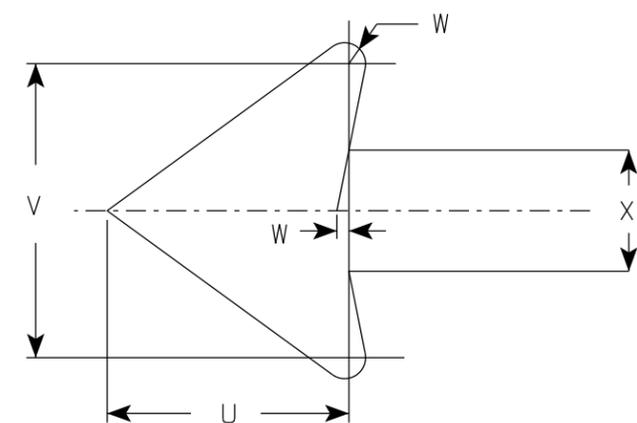
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Red
3. Message Series - See Note 4
4. Lines 1, 3 and 4 are series C, line 2 is series B.
5. R7-1D (double arrow)
R7-1L (left arrow)
R7-1R (right arrow)

ARROW DETAIL



7

7

R7-1

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/2	3/8	3/8	3	1 7/8	2	7/8	5/8	1 1/2	2 1/2	2	2	4 7/8	4 7/8	2 1/4	2 1/8	2 1/2	3 7/8	1 1/2	1 3/4	1/8	3/4			1.5
2S	18	24	1 1/2	3/8	1/2	4	2 1/2	2 1/2	1 1/4	1	2	3 1/4	2 3/4	2 5/8	7 1/8	7	2 3/4	2 5/8	3 1/8	5 7/8	2 1/4	2 5/8	1/4	1 1/8			3.0
2M	24	30	1 1/2	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
3	24	30	1 1/2	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
4																											
5																											

STANDARD SIGN
R7-1

WISCONSIN DEPT OF TRANSPORTATION

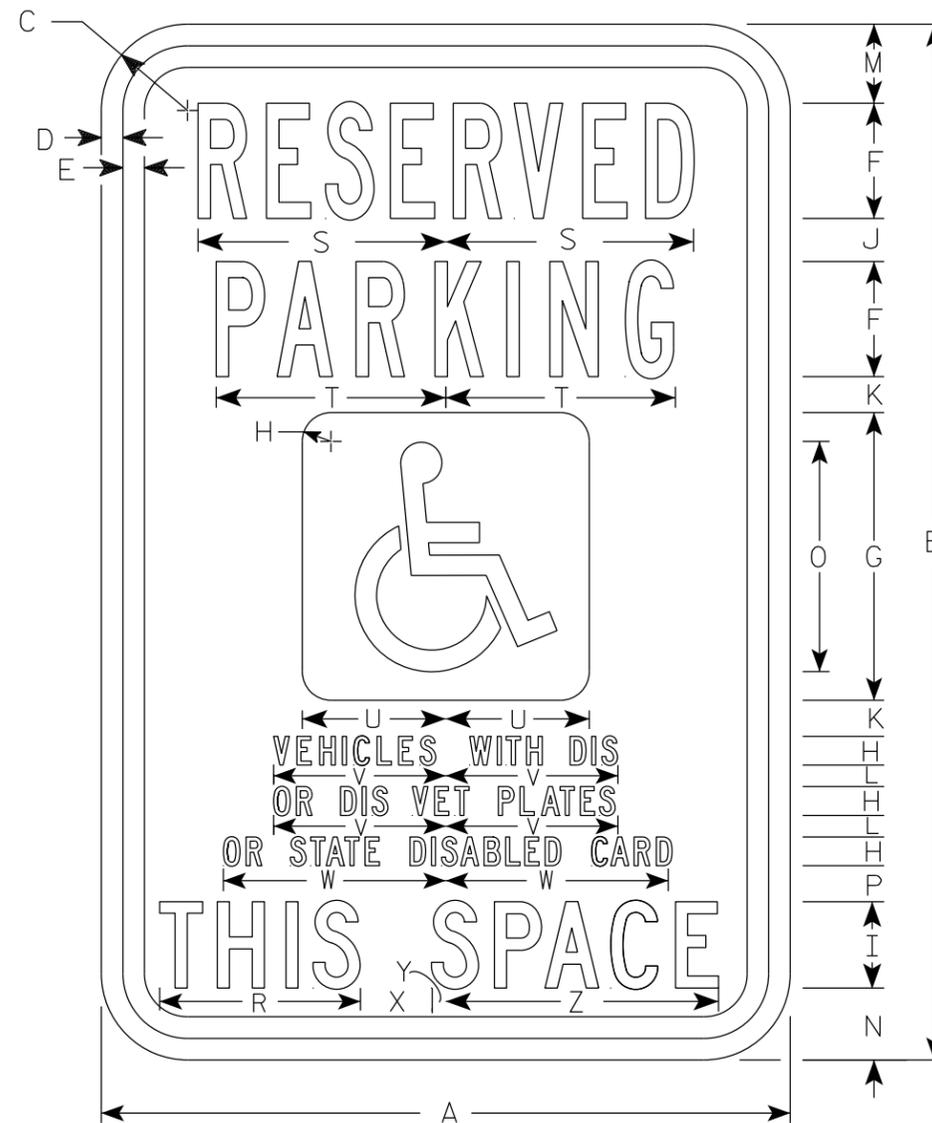
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/26/23 PLATE NO. R7-1.11

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

- Sign is Type II - Type H Reflective
- Color:
 - Background - Sign is white Type H Reflective; paraplegic background is blue.
 - Message - Legend and border are green; paraplegic symbol is white
- Message Series - Lines 1 & 2 are Series B
Lines 3, 4, 5 & 6 are Series C



R7-8A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	18	1 1/2	3/8	3/8	2	5	1/2	1 1/2	3/4	5/8	3/8	1 3/8	1 1/4	4	5/8		3 1/2	4 3/8	4	2 1/2	3	3 7/8	1 1/4	1/4	4 3/4	1.5
2M	18	24	1 1/2	3/8	1/2	3	6	3/4	2	7/8	5/8	1/2	1 7/8	2	5	3/4		4 5/8	6 1/2	5 3/8	3	4 1/2	5 7/8	1 1/2	1/4	6 3/8	3.0
3	18	24	1 1/2	3/8	1/2	3	6	3/4	2	7/8	5/8	1/2	1 7/8	2	5	3/4		4 5/8	6 1/2	5 3/8	3	4 1/2	5 7/8	1 1/2	1/4	6 3/8	3.0
4																											
5																											

STANDARD SIGN
R7-8A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 11/9/23 PLATE NO. R7-8A.7

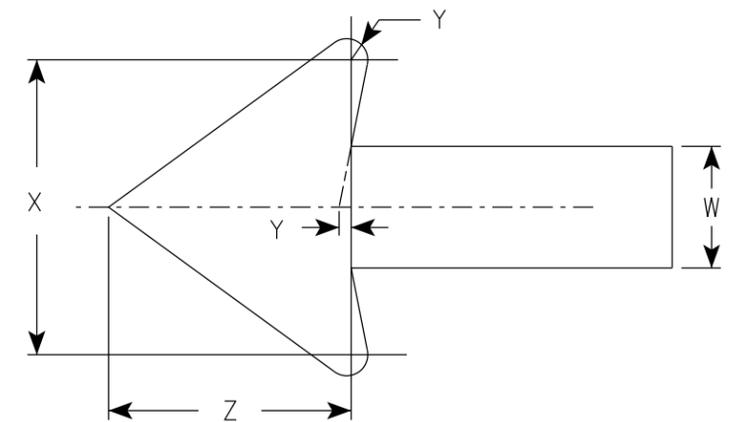
7

7



NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Red
3. Message Series - Lines 1, 3 and 4 are Series C.
Line 2 is Series B.
4. R7-51D (double arrow)
R7-51R (right arrow)
R7-51L (left arrow)



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/2	3/8	3/8	3	1 7/8	2	7/8	5/8	1 1/2	2 1/2	2	2	4 7/8	4 7/8	4 7/8	5/8	1 3/4	2 1/2	4 3/8	3 7/8	3/4	1 3/4	1/8	1 1/2	1.5
2S	18	24	1 1/2	3/8	1/2	4	2 1/2	2 1/2	1 1/4	1	2	3 1/4	2 3/4	2 5/8	7 1/8	7	5 3/4	1 1/8	1 1/2	3 1/8	5 1/2	5 7/8	1 1/8	2 5/8	1/4	2 1/4	3.0
2M	24	30	1 1/2	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	7 1/8	1 1/4	2	3 3/4	6 1/2	7 3/4	1 1/2	3 1/2	1/4	3	5.0
3	24	30	1 1/2	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	7 1/8	1 1/4	2	3 3/4	6 1/2	7 3/4	1 1/2	3 1/2	1/4	3	5.0
4																											
5																											

STANDARD SIGN
R7-51

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

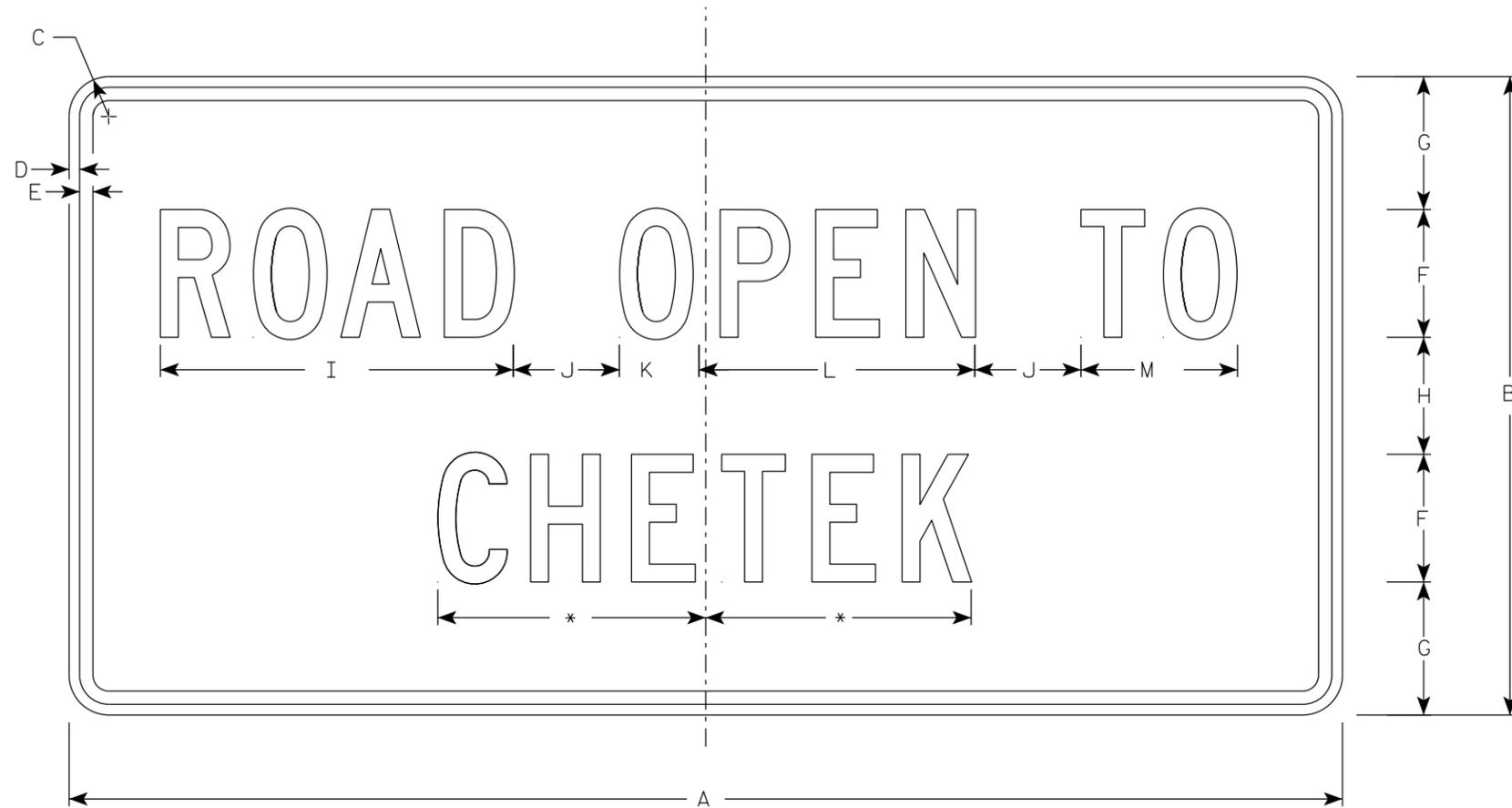
DATE 11/9/23 PLATE NO. R7-51.7

7

7

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate message and optically balance.



R10-61

*See note 5

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	24	1 1/2	1/2	5/8	4	5 1/2	5	10 3/4	2	2 1/8	8 3/8	4 5/8														6.0
2S	60	30	1 7/8	1/2	5/8	6	6 1/4	5 1/2	16 5/8	5	3 3/4	13	7 3/8														12.5
2M	60	30	1 7/8	1/2	5/8	6	6 1/4	5 1/2	16 5/8	5	3 3/4	13	7 3/8														12.5
3																											
4																											
5																											

STANDARD SIGN
R10-61

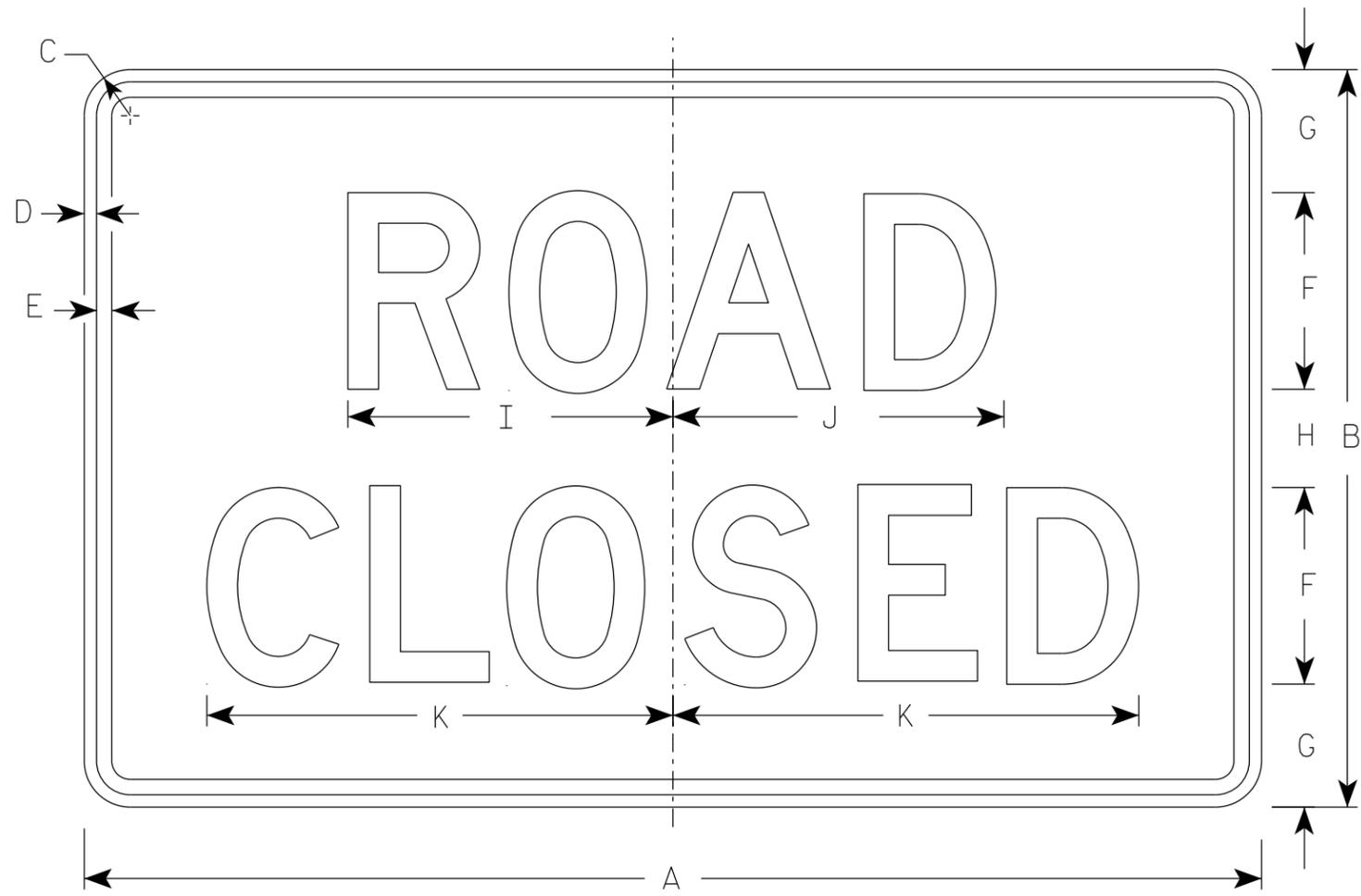
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

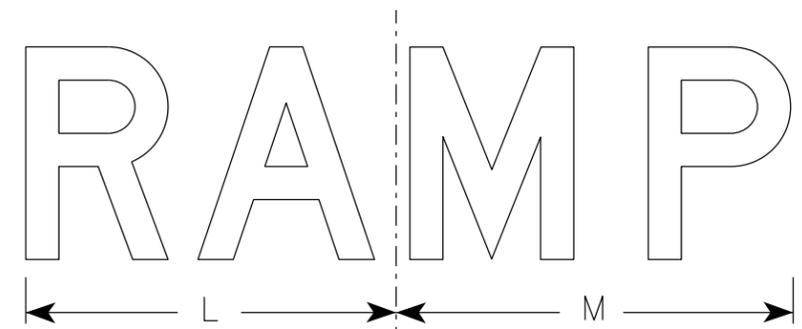
DATE 2/5/24 PLATE NO. R10-61.6

7

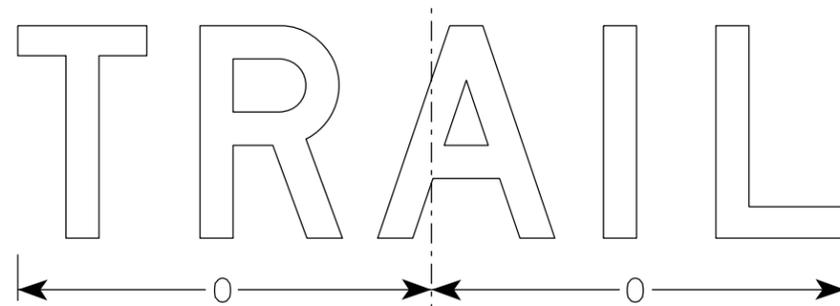
7



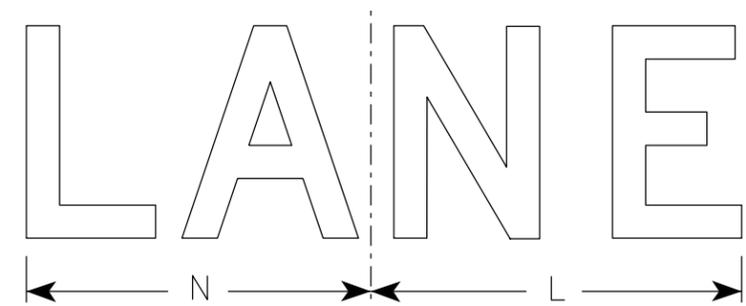
R11-2



R11-2R



R11-2T



R11-2L

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Modify the message as required.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
2M	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
3	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
4	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
5	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0

STANDARD SIGN
R11-2

WISCONSIN DEPT OF TRANSPORTATION

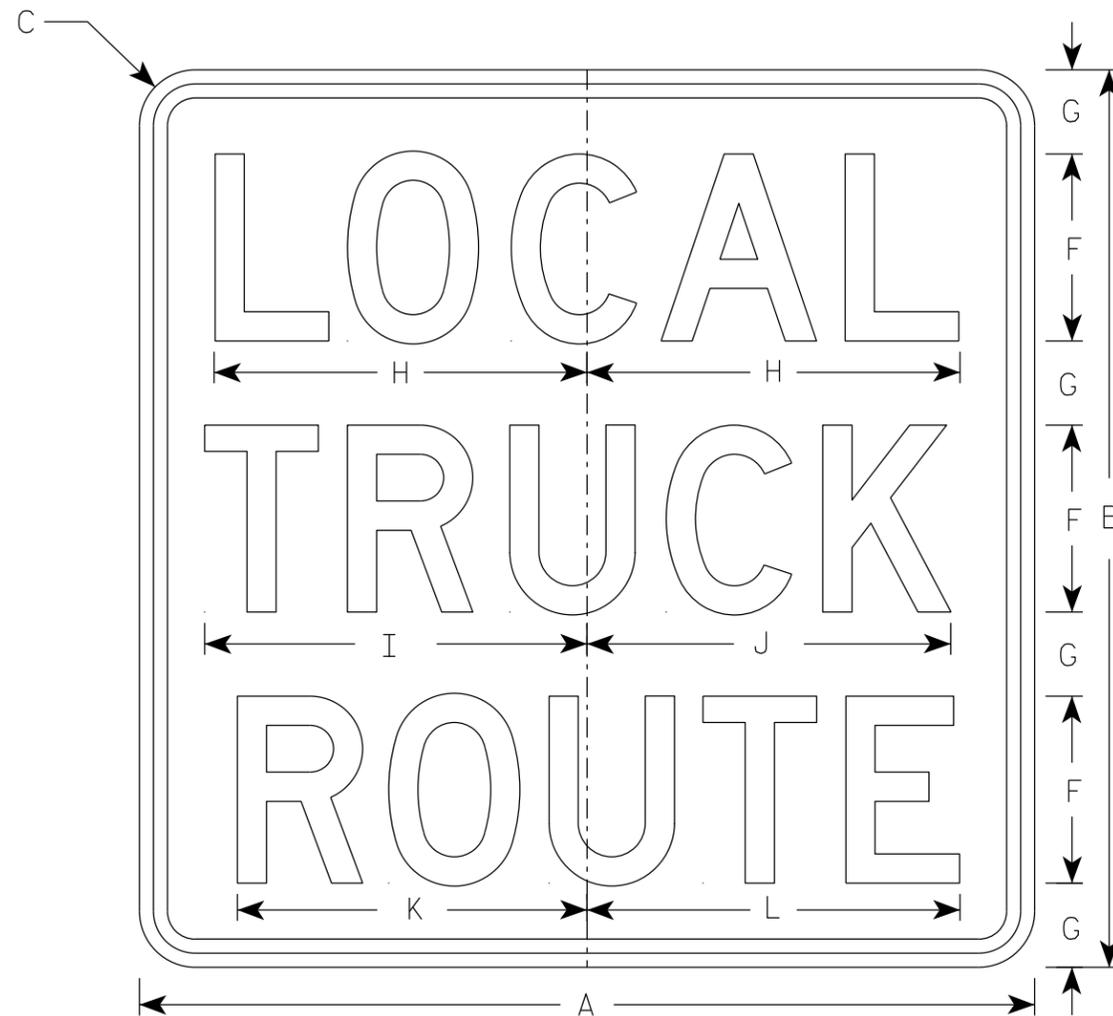
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 2/5/24 PLATE NO. R11-2.12

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
 - Background - White
 - Message - Black
3. Message Series - D



R14-52

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	24	1 1/2	3/8	3/8	5	2 1/4	10	10 1/4	9 3/4	9 3/8	10															4.00
2M	24	24	1 1/2	3/8	3/8	5	2 1/4	10	10 1/4	9 3/4	9 3/8	10															4.00
3	30	30	1 7/8	3/8	3/8	6	3	12 1/2	13	12 3/4	11 3/4	12 1/2															6.25
4	42	42	1 7/8	1/2	5/8	8	4 1/2	16 3/4	17 3/8	17 1/4	16 3/4	17															12.25
5																											

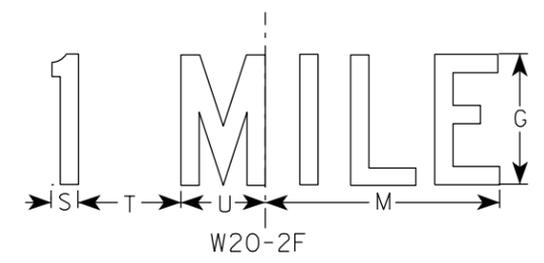
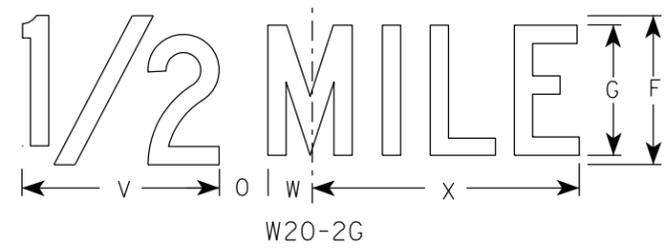
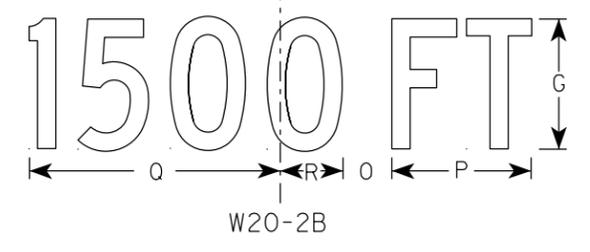
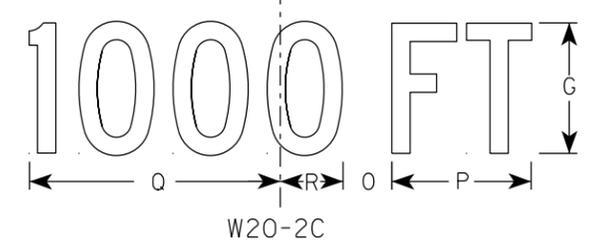
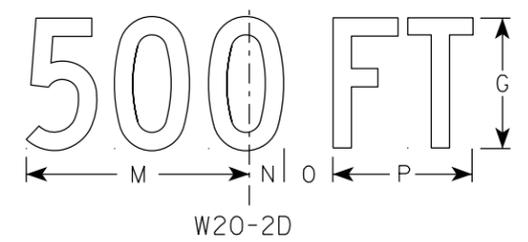
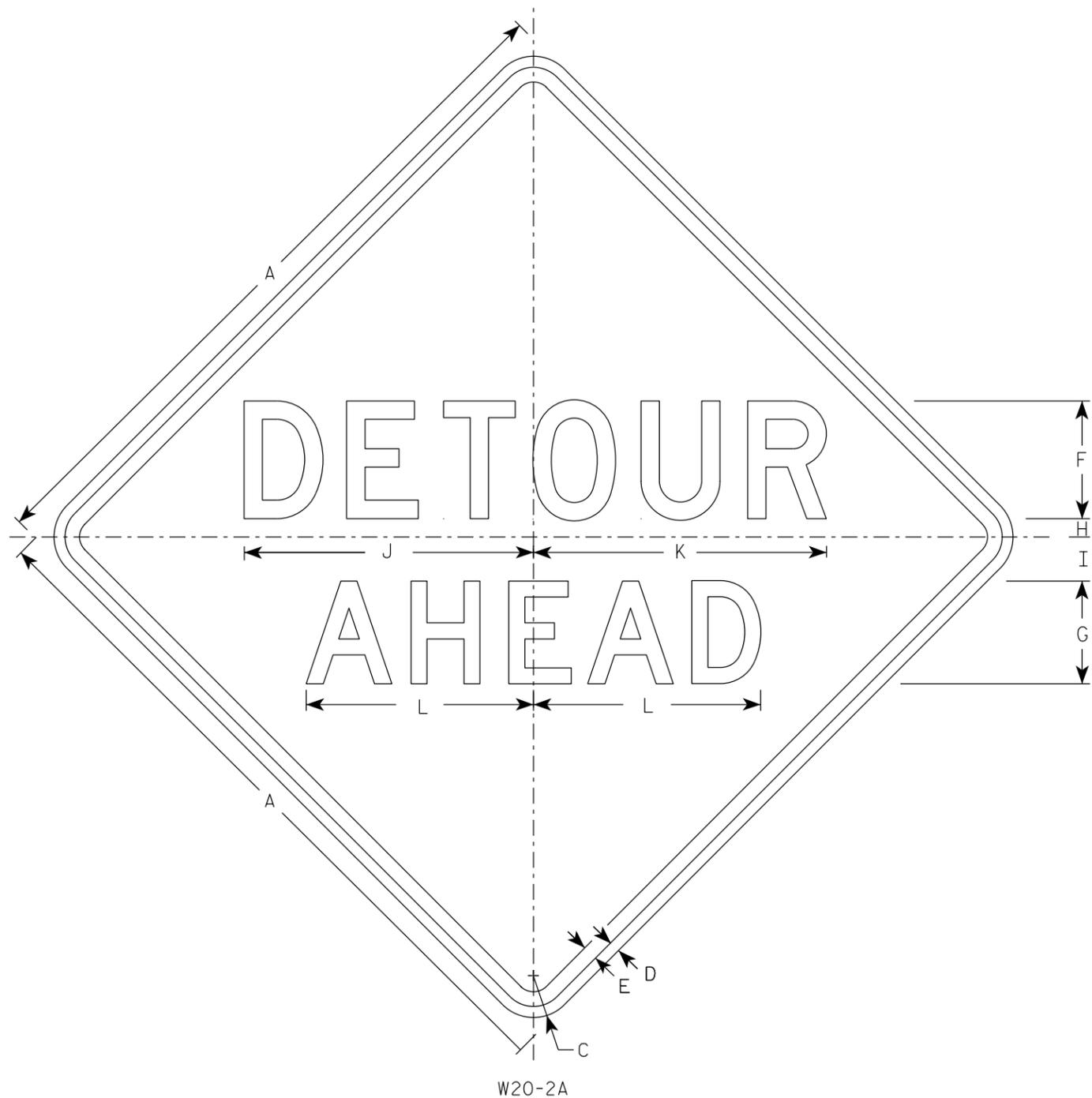
STANDARD SIGN
R14-52

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 2/9/24 PLATE NO. R14-52.4

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
Line 2 is Series D for AHEAD and Series C for all other distances.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
2M	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
3	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
4	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
5	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0

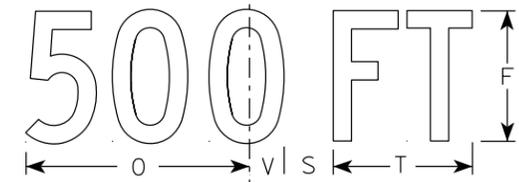
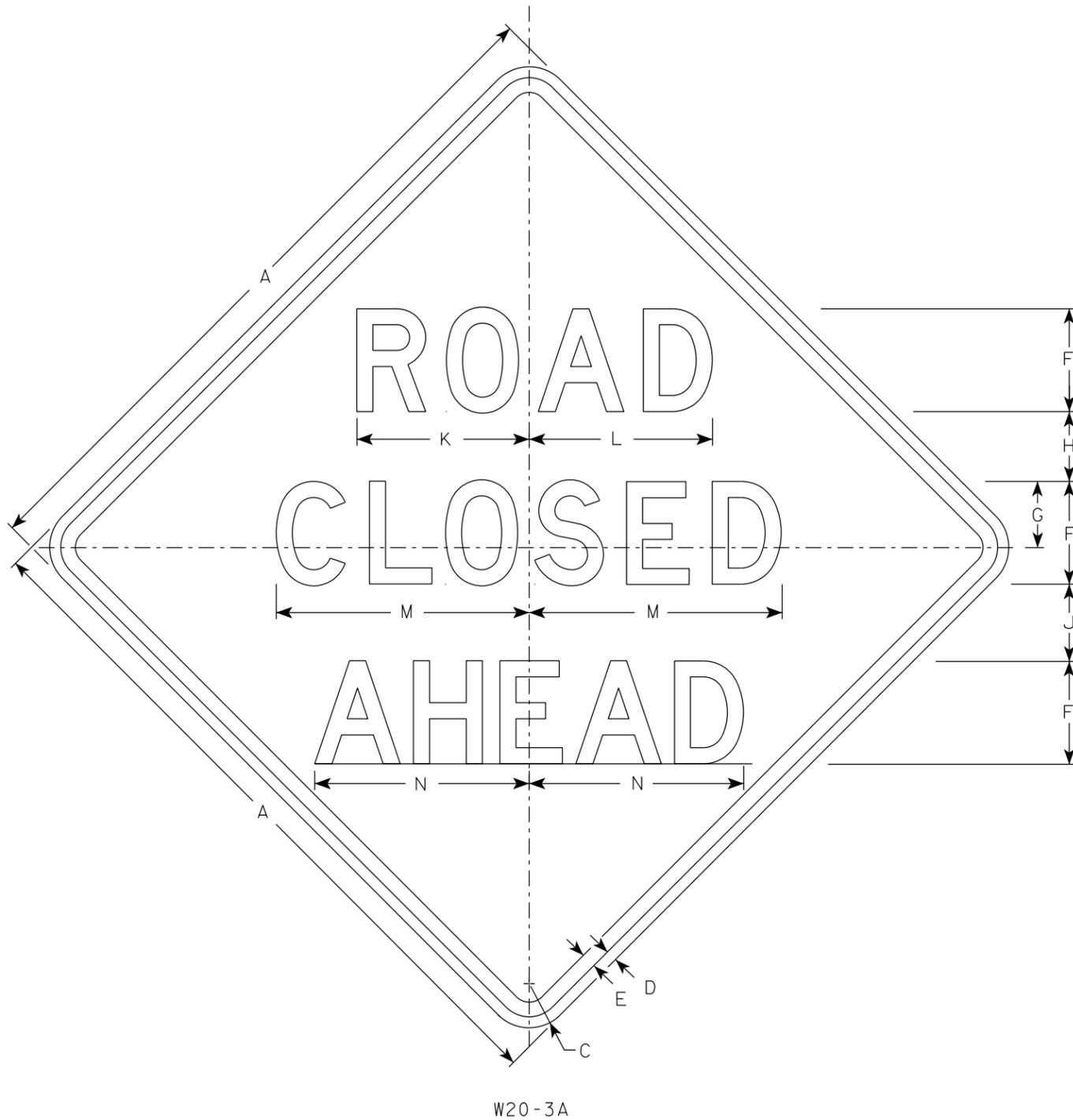
STANDARD SIGN
W20-2A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

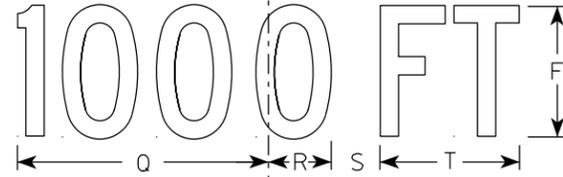
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 1/10/2024 PLATE NO. W20-2.7

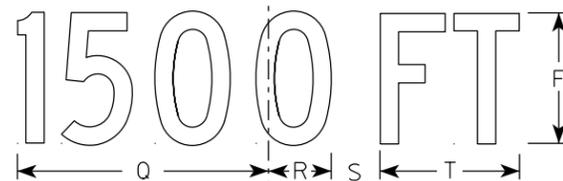
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



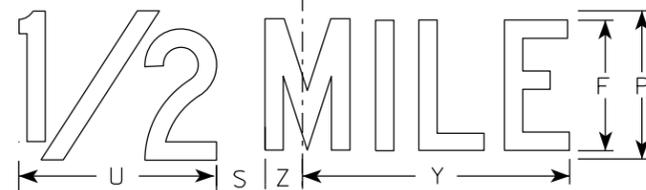
W20-3D



W20-3C



W20-3B



W20-3G



W20-3F

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.
Line 3 is Series D for AHEAD and Series C for all other distances.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	5	3 3/8	3 1/2	1 1/8	4	8 3/8	8 7/8	12 1/2	11	9	6	10 1/8	2 1/2	1 7/8	5 5/8	8	1 3/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
3	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
4	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
5	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

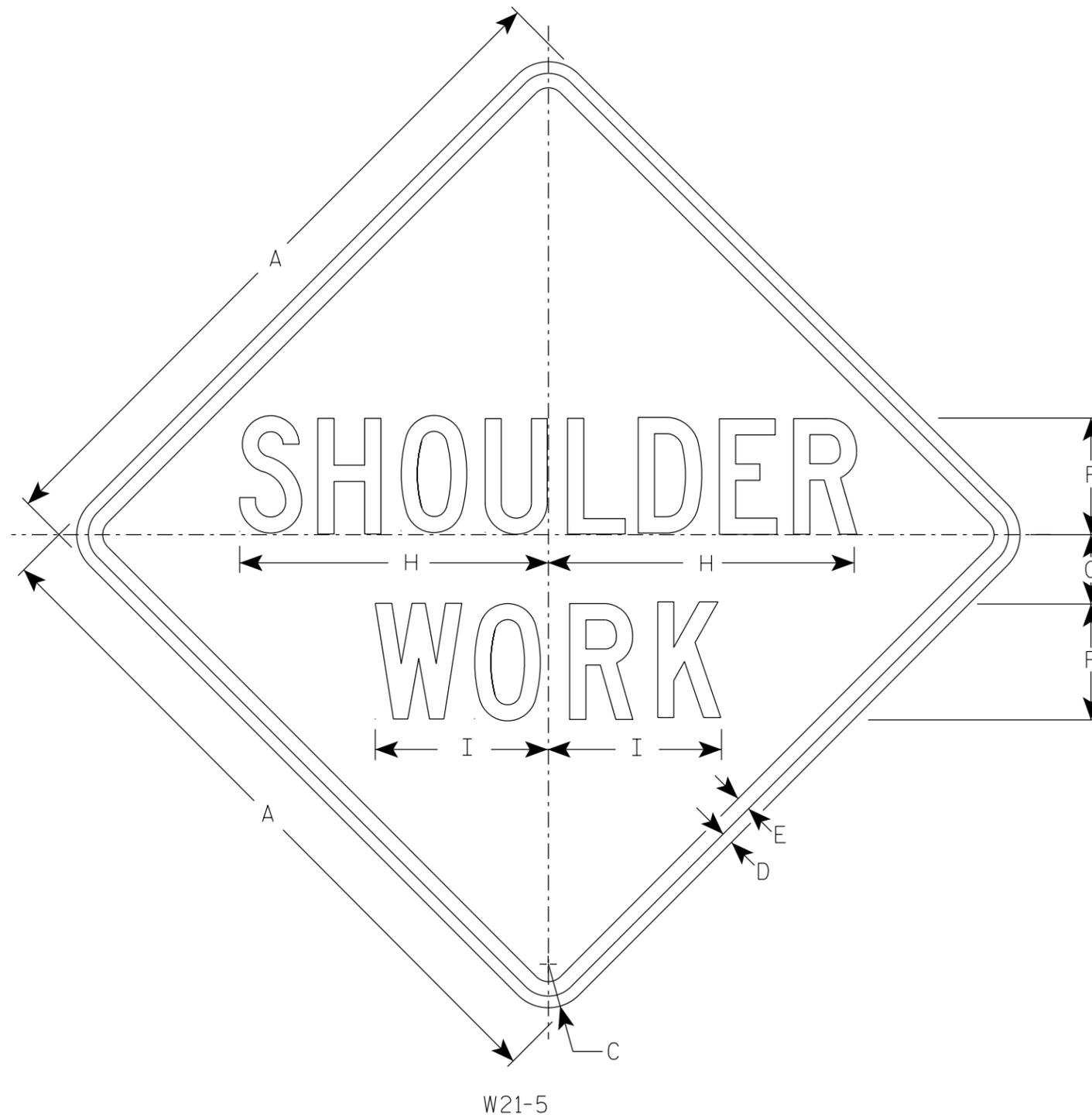
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 1/10/2024 PLATE NO. W20-3.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - C



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	6	3 1/2	16	9																		9.0
2S	48		3	3/4	1	8	5	21 3/8	11 1/4																		16.0
2M	48		3	3/4	1	8	5	21 3/8	11 1/4																		16.0
3	48		3	3/4	1	8	5	21 3/8	11 1/4																		16.0
4	48		3	3/4	1	8	5	21 3/8	11 1/4																		16.0
5	48		3	3/4	1	8	5	21 3/8	11 1/4																		16.0

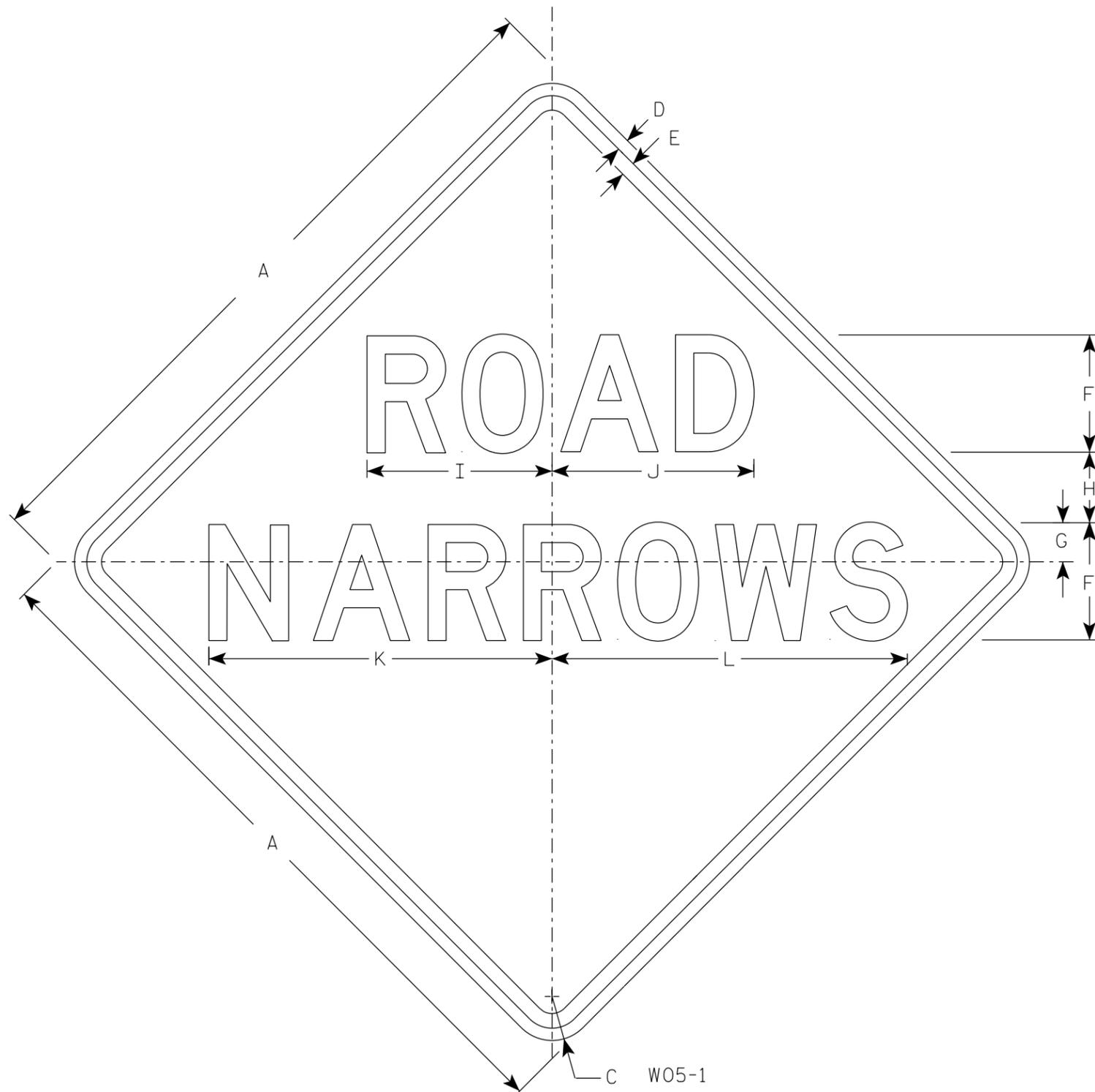
STANDARD SIGN
W21-5

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 1/11/2024 PLATE NO. W21-5.7

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**



NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	6	2	3 1/2	9 1/2	10 3/8	17 5/8	18 1/4															9.0
2S	48		3	3/4	1	8	3	4	12 3/4	13 3/4	23 1/2	24 3/8															16.0
2M	48		3	3/4	1	8	3	4	12 3/4	13 3/4	23 1/2	24 3/8															16.0
3	48		3	3/4	1	8	3	4	12 3/4	13 3/4	23 1/2	24 3/8															16.0
4	48		3	3/4	1	8	3	4	12 3/4	13 3/4	23 1/2	24 3/8															16.0
5	48		3	3/4	1	8	3	4	12 3/4	13 3/4	23 1/2	24 3/8															16.0

C W05-1

STANDARD SIGN
W05-1

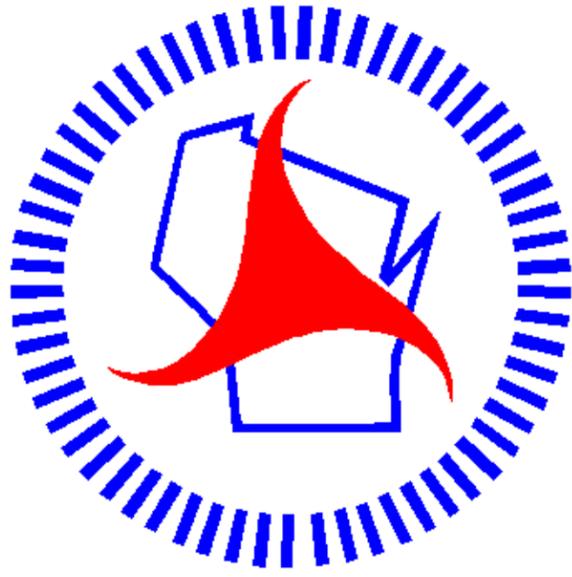
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/29/2024 PLATE NO. W05-1.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

Notes



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