LAX

PROJECT ID: WITH: N/A

5405-00-70

COUNTY: **VERNON**

MARCH 2026

ORDER OF SHEETS

TOTAL SHEETS = 42

N

DESIGN DESIGNATION 5405-00-00

A.A.D.T.	2026	=	94
A.A.D.T.	2046	=	99
D.H.V.		=	14
D.D.		=	62/38
T.		=	7.7%
DESIGN SPEED		=	30 MPH
ESALS		=	7,300

CONVENTIONAL SYMBOLS

PLAN

- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE (300'EB')
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE

- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES

- ELECTRIC (E)
- FIBER OPTIC (FO)
- GAS (G)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

TOWN OF STERLING, PRAIRIE ROAD

RUSH CREEK BRIDGE B-62-0272

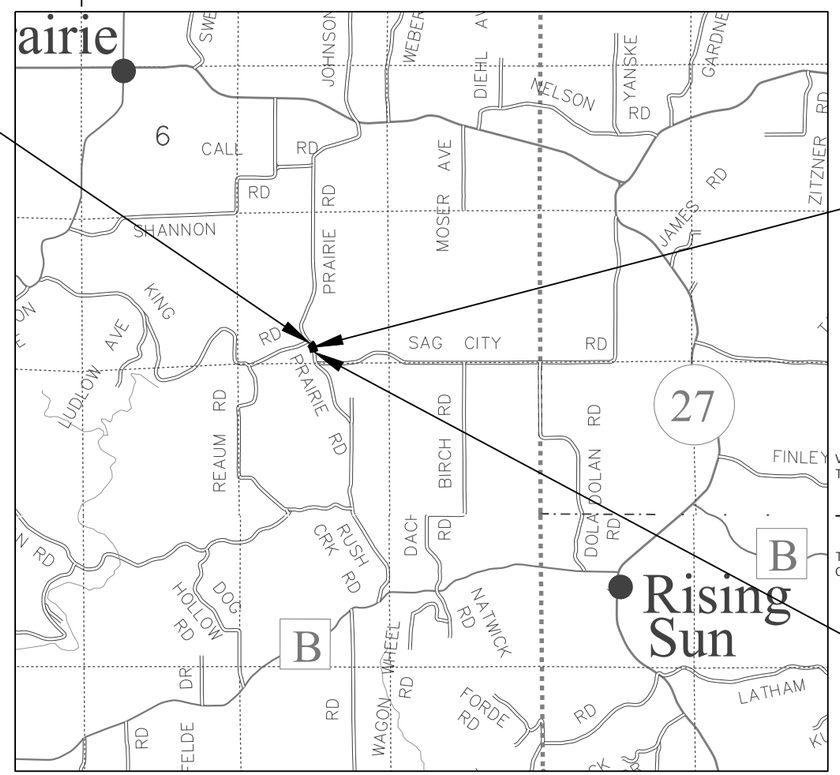
LOC STR

VERNON COUNTY

STATE PROJECT NUMBER
5405-00-70

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5405-00-70		

R-6-W ◄► R-5-W



LAYOUT

SCALE 0 1 MI

TOTAL NET LENGTH OF CENTERLINE = 0.0271 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), VERNON COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 18.

ACCEPTED FOR

VERNON COUNTY

10/20/20 (Date) — *Phil Hewitt* Hwy Comm (Signature and Title of Official)

ORIGINAL PLANS PREPARED BY

WESTBROOK
Associated Engineers, Inc.
619 EAST HOXIE STREET
P.O. BOX 429
SPRING GREEN, WISCONSIN 53588
PHONE (608) 588-7866
FAX (608) 588-7954

WISCONSIN PROFESSIONAL ENGINEER
AARON B. PALMER
E-35695
RICHLAND CENTER, WI

DATE: 10/17/25 (Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	WESTBROOK ASSOCIATED ENGINEERS, INC.
Designer	WESTBROOK ASSOCIATED ENGINEERS, INC.
Project Manager	LORRAINE BETZEL, P.E.
Regional Examiner	SW REGION
Regional Supervisor	KYLE HEMP, P.E.

APPROVED FOR THE DEPARTMENT

DATE: 10/21/25 *Lorraine Betzel* (Signature)

E

STANDARD ABBREVIATIONS

ABUT	ABUTMENT	LS	LUMP SUM
AC	ACRE	MGAL	ONE THOUSAND GALLONS
AGG	AGGREGATE	ML OR M/L	MATCH LINE
AH	AHEAD	NOM	NOMINAL
∠	ANGLE	NC	NORMAL CROWN
AADT	ANNUAL AVERAGE DAILY TRAFFIC	NB	NORTHBOUND
ASPH	ASPHALTIC	NO	NUMBER
BK	BACK	OD	OUTSIDE DIAMETER
BAD	BASE AGGREGATE DENSE	PAVT	PAVEMENT
BL OR B/L	BASE LINE	PC	POINT OF CURVATURE
BM	BENCH MARK	PI	POINT OF INTERSECTION
CL OR C/L	CENTER LINE	PT	POINT OF TANGENCY
Δ	CENTRAL ANGLE OR DELTA	PCC	PORTLAND CEMENT CONCRETE
CONC	CONCRETE	LB	POUND
CONST	CONSTRUCTION	PSI	POUNDS PER SQUARE INCH
CP	CONTROL POINT	PE	PRIVATE ENTRANCE
CO	COUNTY	PROJ	PROJECT
CTH	COUNTY TRUCK HIGHWAY	PL	PROPERTY LINE
CY	CUBIC YARD	R	RADIUS
D	DEGREE OF CURVE	RL OR R/L	REFERENCE LINE
DHV	DESIGN HOUR VOLUME	REQD	REQUIRED
DIA	DIAMETER	RT	RIGHT
DD	DIRECTIONAL DISTRIBUTION	RHF	RIGHT HAND FORWARD
DWY	DRIVEWAY	R/W	RIGHT OF WAY
EA	EACH	RD	ROAD
EB	EASTBOUND	RDWY	ROADWAY
EL OR ELEV	ELEVATION	SHLDR	SHOULDER
EMB	EMBANKMENT	SB	SOUTHBOUND
ESALS	EQUIVALENT SINGLE AXLE LOADS	SPECS	SPECIFICATIONS
EXC	EXCAVATION	SF	SQUARE FEET
EXIST	EXISTING	SY	SQUARE YARD
FERT	FERTILIZER	SDD	STANDARD DETAIL DRAWINGS
FE	FIELD ENTRANCE	STH	STATE TRUNK HIGHWAY
FL OR F/L	FLOW LINE	STA	STATION
FT	FOOT	SE	SUPERELEVATION
HES	HIGH EARLY STRENGTH	SL OR S/L	SURVEY LINE
CWT	HUNDRED WEIGHT	TEMP	TEMPORARY
IN DIA	INCH DIAMETER	T	TRUCKS (PERCENT OF)
ID	INSIDE DIAMETER	TYP	TYPICAL
INTERS	INTERSECTION	USH	UNITED STATES HIGHWAY
IH	INTERSTATE HIGHWAY	VAR	VARIABLE
INV	INVERT	VC	VERTICAL CURVE
JT	JOINT	VPC	VERTICAL POINT OF CURVATURE
LT	LEFT	VPI	VERTICAL POINT OF INTERSECTION
LHF	LEFT HAND FORWARD	VPT	VERTICAL POINT OF TANGENCY
L	LENGTH OF CURVE	W	WEST
LF	LINEAR FOOT	WB	WESTBOUND
LC	LONG CHORD OF CURVE		

WISCONSIN DNR LIAISON

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3550 MORMON COULEE RD
LA CROSSE, WI 54601
PHONE: (608) 406-7880
EMAIL: KAREN.KALVELAGE@WISCONSIN.GOV

DESIGN PROJECT MANAGER

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DESIGN CONSULTANT

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PHONE: (608) 588-7866
EMAIL: APALMER@WESTBROOKENG.COM

COUNTY HIGHWAY COMMISSIONER

PHIL HEWITT
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1335 RAILROAD AVE
VIROQUA, WI 54665
PHONE: (608) 637-5452
EMAIL: PHIL.HEWITT@VERNONCOUNTYWI.GOV

UTILITIES CONTACTS

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COMMUNICATIONS
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WESTBY, WI 54667
PHONE: (608) 632-0607
EMAIL: SFREDERICK@VERNONCOM.COOP

VERNON ELECTRIC COOPERATIVE
ELECTRIC
COLE CARY
110 SAUGSTAD RD
WESTBY, WI 54667
PHONE: (608) 634-7472
EMAIL: CCARY@VERNONELECTRIC.ORG

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY OPERATIONS, OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.

HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LBS/SY/IN.

APPLY TACK COAT BETWEEN LAYERS OF HMA PAVEMENT AT A RATE OF 0.05 GAL/SY.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

RIGHT OF WAY INFORMATION SHOWN ON THE PLANS IS APPROXIMATE.

THE CONTRACTOR IS TO WORK WITH UTMOST CARE AND PROTECT ALL SURVEY MARKERS. REMOVAL OF ANY SURVEY MARKER IS TO BE WITH THE APPROVAL OF THE ENGINEER.

WHEN THE QUANTITY OF THE ITEMS OF BASE AGGREGATE, SUBBASE OR HMA PAVEMENT IS MEASURED FOR PAYMENT BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE LAYERS SHOWN ON THE PLAN IS APPROXIMATE, AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL PREPARE AN EROSION CONTROL IMPLEMENTATION PLAN (ECIP) AND SUBMIT THE PLAN TO WISDOT AND WDNR FOR REVIEW AT LEAST 14 DAYS PRIOR TO THE PRECONSTRUCTION CONFERENCE.

EROSION CONTROL FEATURES, AS SHOWN IN THE PLANS, ARE AT APPROXIMATE LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE CONTRACTOR'S ECIP AND APPROVED BY THE ENGINEER. MAINTAIN EROSION CONTROL MEASURES UNTIL SUCH A TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NECESSARY.

BIODEGRADABLE NON-NETTED MATTING SHALL BE USED ALONG STREAM CORRIDORS.

SLOPES 2.5:1 OR STEEPER REQUIRE EROSION MAT.

CURVE DATA IS BASED ON THE ARC DEFINITION.

THE PROPOSED SHOULDER WIDTH SHOWN IN THE TYPICAL SECTIONS ARE MINIMUM WIDTH. PERPETUATE EXISTING SHOULDERS THAT ARE WIDER THAN WHAT IS SHOWN IN THE TYPICAL SECTIONS.

THE CONTRACTOR'S PAVING OPERATION SHALL BE CONSISTENT WITH THE TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING LANE.

THE 4-INCH ASPHALTIC SURFACE SHALL BE CONSTRUCTED USING ONE (1) 2.25-INCH LOWER LAYER AND ONE (1) 1.75-INCH UPPER LAYER. THE PREFERRED LOWER LAYER IS 2.25-INCHES OF 3 LT 58-28 S. THE PREFERRED UPPER LAYER IS 1.75-INCHES OF 4 LT 58-28 S.

SAWCUTS, AS SHOWN ON THE PLANS, ARE SUGGESTED LOCATIONS AND MAY BE ADJUSTED AT THE DISCRETION OF THE ENGINEER TO BETTER SUIT FIELD CONDITIONS.

THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS AT ALL TIMES EXCEPT WHEN PAVING OPERATIONS REQUIRE THE DRIVEWAY TO BE CLOSED. ACCESS TO DRIVEWAYS SHALL BE RE-ESTABLISHED IMMEDIATELY AFTER OPERATIONS ARE COMPLETED. ACCESS SHALL BE PROVIDED DURING ALL NON-WORKING HOURS.

TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

BEARINGS SHOWN ON THE PLAN ARE TRUE BEARINGS.

DO NOT DRIVE OR STORE EQUIPMENT, OR STORE CONSTRUCTION MATERIALS IN ENVIRONMENTALLY SENSITIVE AREAS, WETLANDS OR WATERWAYS.

RUNOFF COEFFICIENT TABLE

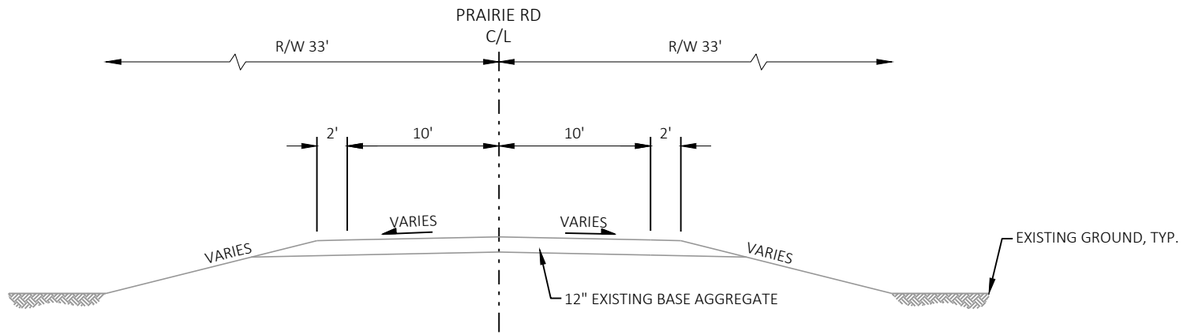
	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS:	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIPTURF:	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPETURF:			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT:	.70 - .95											
CONCRETE:	.80 - .95											
BRICK:	.70 - .80											
DRIVES, WALKS:	.75 - .85											
ROOFS:	.75 - .95											
GRAVEL ROADS, SHOULDERS:	.40 - .60											

TOTAL PROJECT AREA = 0.217 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.124 ACRES

DIGGERS HOTLINE
Dial 811 or (800)242-8511
www.DiggersHotline.com

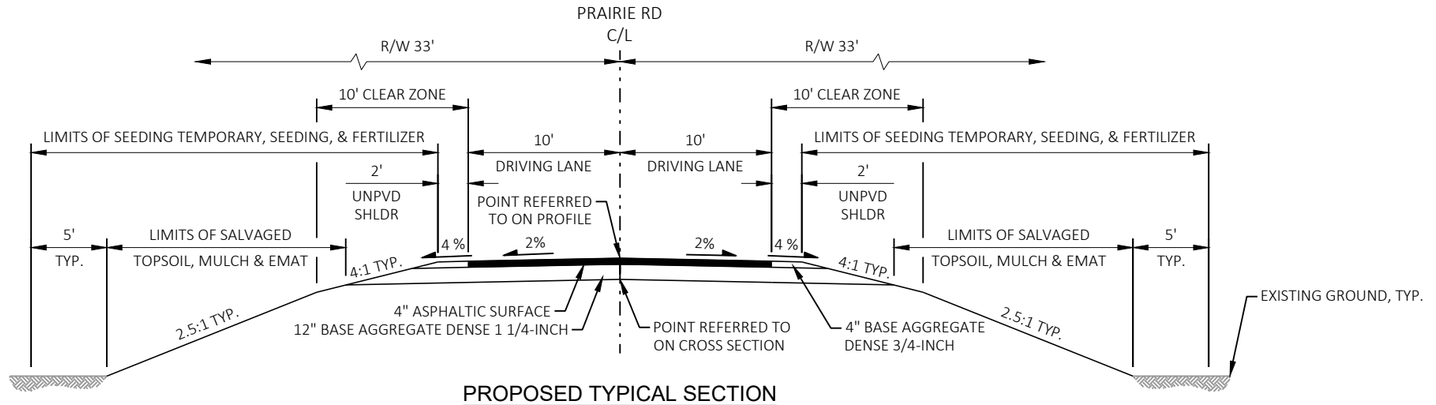
ORDER OF SECTION 2 DETAIL SHEETS

2



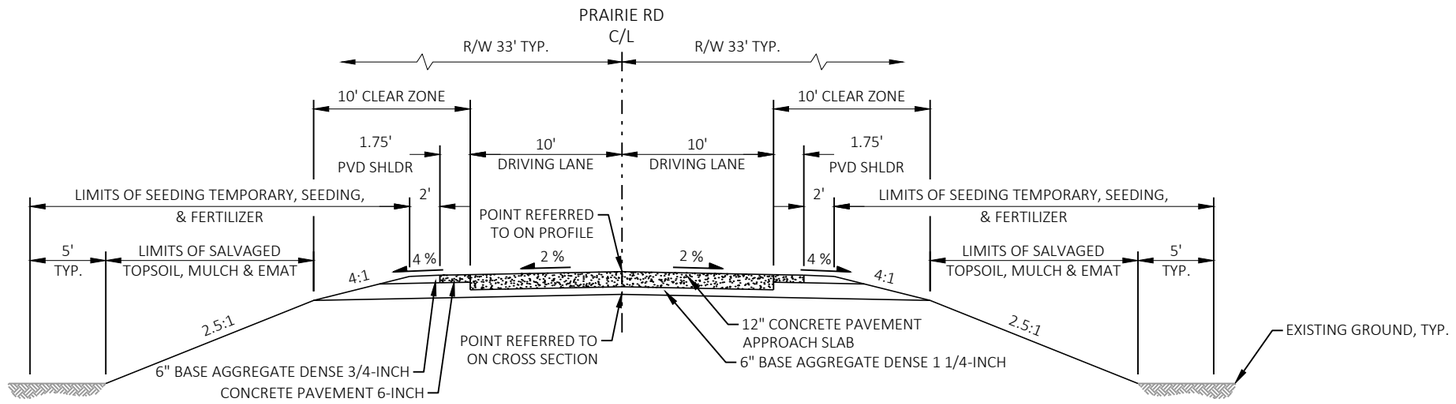
EXISTING TYPICAL SECTION

STA. 11+78.86 - STA. 13+21.76



PROPOSED TYPICAL SECTION

STA. 11+78.86 - STA. 12+09.21
STA 12+92.08 - STA 13+21.76



CONCRETE APPROACH SLAB TYPICAL SECTION

STA 12+09.21 - STA 12+29.63
STA 12+70.98 - STA 12+92.08

PROJECT NO: 5405-00-70	HWY: PRAIRIE RD	COUNTY: VERNON	TYPICAL SECTIONS	SHEET 3	E

LEGEND

EXISTING SIGN

PROPOSED SIGN

N

REMOVE 2R

W5-52L 12"X36" 2

W5-52R 12"X36" 5

REMOVE 5R

WEIGHT LIMIT 20 TONS / WATCH FOR SIGN AHEAD REMOVE 6R

NARROW BRIDGE REMOVE 8R

BARN

BP: 10+00.00

PI: 11+11.01

PC: 11+65.53

PT: 12+26.65

PC: 12+80.23

EP: 14+48.70

11+00

12+00

13+00

14+00

GUY

FO

OH

1R NARROW BRIDGE REMOVE

3R WEIGHT LIMIT 20 TONS / WATCH FOR SIGN AHEAD REMOVE

4R REMOVE

4 W5-52R 12"X36"

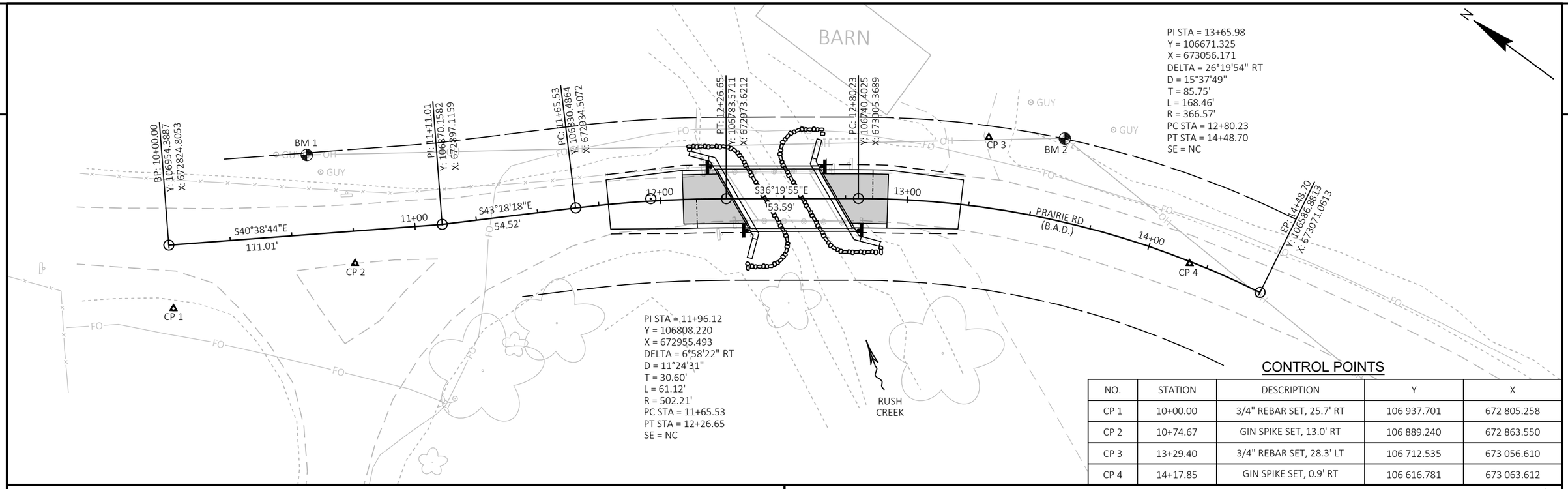
7 W5-52L 12"X36"

7R REMOVE

NOTE:
CONSTRUCT IN ACCORDANCE WITH SDD "SIGNING AND MARKING FOR TWO LANE BRIDGES"

PROJECT NO: 5405-00-70	HWY: PRAIRIE RD	COUNTY: VERNON	PERMANENT SIGNING	SHEET 4	E

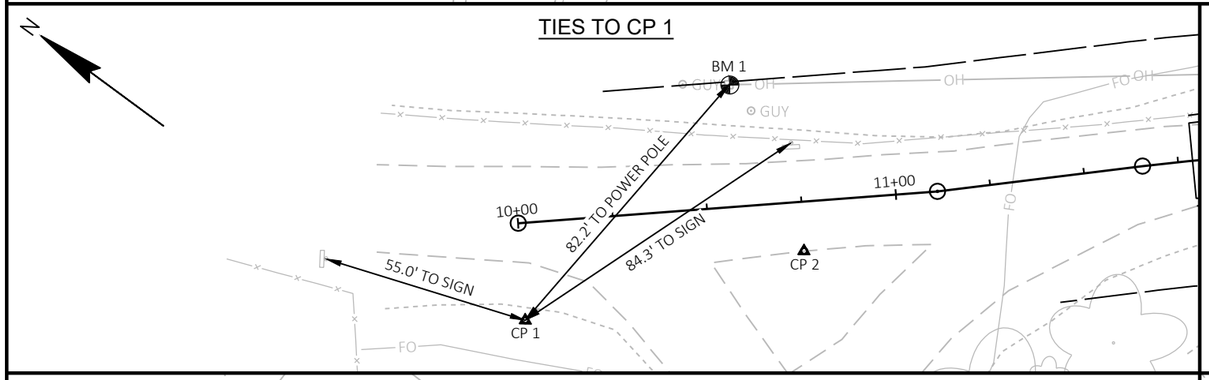
FILE NAME : G:\00-PROJECT FILES\2022\22317 PRAIRIE RD, RUSH CREEK BRIDGE, VERNON COUNTY\0-CAD\SHEETS\023201_PS.DWG
LAYOUT NAME - 023201_ps
PLOT DATE : 10/16/2025 8:55 AM PLOT BY : KYLE MAYBERRY PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 42



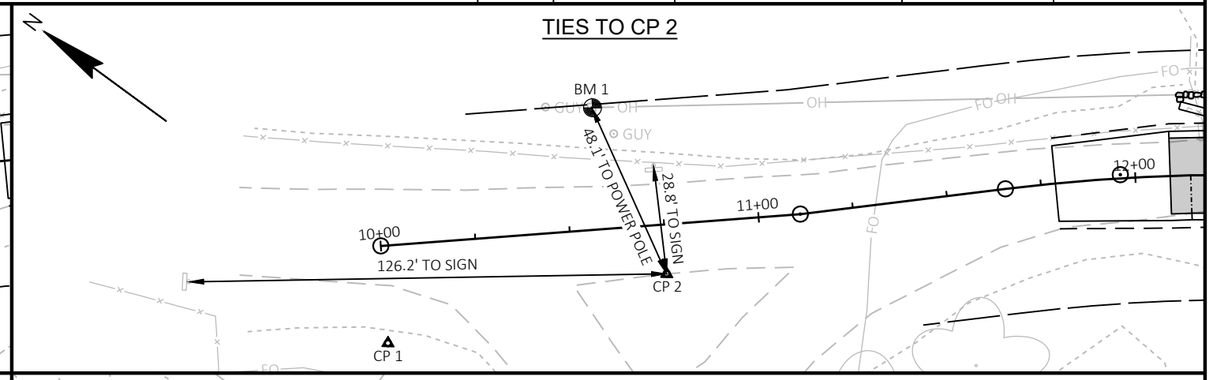
CONTROL POINTS

NO.	STATION	DESCRIPTION	Y	X
CP 1	10+00.00	3/4" REBAR SET, 25.7' RT	106 937.701	672 805.258
CP 2	10+74.67	GIN SPIKE SET, 13.0' RT	106 889.240	672 863.550
CP 3	13+29.40	3/4" REBAR SET, 28.3' LT	106 712.535	673 056.610
CP 4	14+17.85	GIN SPIKE SET, 0.9' RT	106 616.781	673 063.612

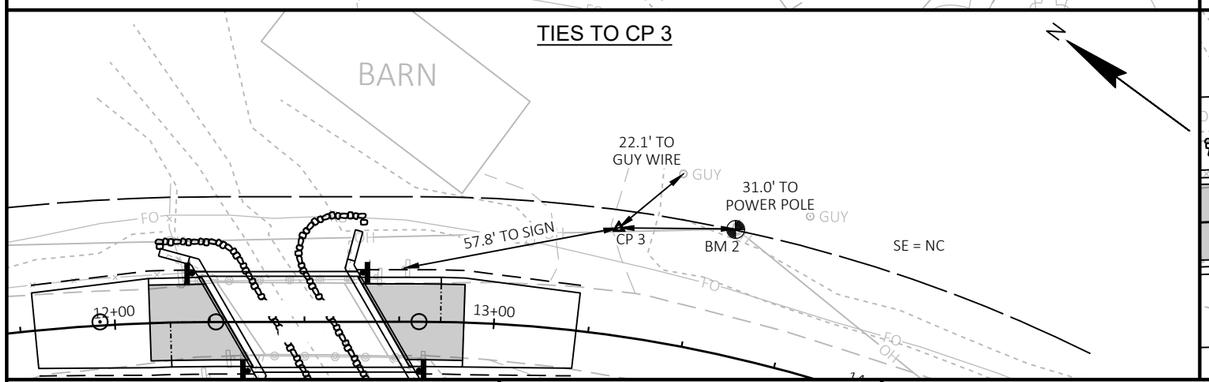
TIES TO CP 1



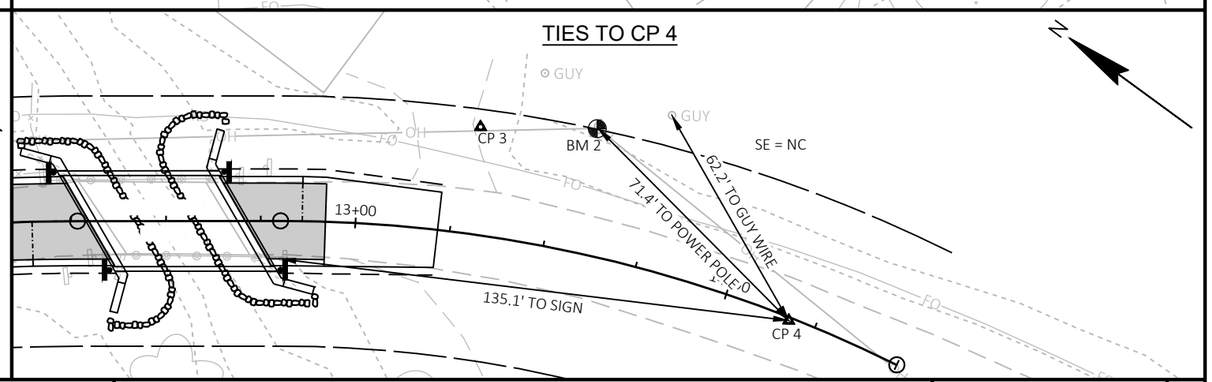
TIES TO CP 2



TIES TO CP 3



TIES TO CP 4



PROJECT NO: 5405-00-70	HWY: PRAIRIE RD	COUNTY: VERNON	ALIGNMENT DETAILS AND CONTROL POINTS	SHEET 5	E

FILE NAME : G:\00-PROJECT FILES\2022\22317 PRAIRIE RD, RUSH CREEK BRIDGE, VERNON COUNTY\0-CAD\SHEETS\027201_AD.DWG
LAYOUT NAME - 027201_ad
PLOT DATE : 10/16/2025 8:58 AM PLOT BY : KYLE MAYBERRY PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 42

Estimate Of Quantities

5405-00-70

Line	Item	Item Description	Unit	Total	Qty
0002	203.0250	Removing Structure Over Waterway Remove Debris (structure) 01. P-62-329	EACH	1.000	1.000
0004	205.0100	Excavation Common	CY	135.000	135.000
0006	206.1001	Excavation for Structures Bridges (structure) 01. B-62-0272	EACH	1.000	1.000
0008	210.1500	Backfill Structure Type A	TON	300.000	300.000
0010	213.0100	Finishing Roadway (project) 01. 5405-00-70	EACH	1.000	1.000
0012	305.0110	Base Aggregate Dense 3/4-Inch	TON	20.000	20.000
0014	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	220.000	220.000
0016	415.0060	Concrete Pavement 6-Inch	SY	14.000	14.000
0018	415.0410	Concrete Pavement Approach Slab	SY	93.000	93.000
0020	455.0605	Tack Coat	GAL	8.000	8.000
0022	465.0105	Asphaltic Surface	TON	34.000	34.000
0024	502.0100	Concrete Masonry Bridges	CY	149.000	149.000
0026	502.3200	Protective Surface Treatment	SY	121.000	121.000
0028	502.3210	Pigmented Surface Sealer	SY	42.000	42.000
0030	505.0400	Bar Steel Reinforcement HS Structures	LB	4,600.000	4,600.000
0032	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	22,240.000	22,240.000
0034	516.0500	Rubberized Membrane Waterproofing	SY	12.000	12.000
0036	550.0020	Pre-Boring Rock or Consolidated Materials	LF	147.000	147.000
0038	550.1100	Piling Steel HP 10-Inch X 42 Lb	LF	210.000	210.000
0040	606.0300	Riprap Heavy	CY	130.000	130.000
0042	612.0406	Pipe Underdrain Wrapped 6-Inch	LF	160.000	160.000
0044	618.0100	Maintenance and Repair of Haul Roads (project) 01. 5405-00-70	EACH	1.000	1.000
0046	619.1000	Mobilization	EACH	1.000	1.000
0048	624.0100	Water	MGAL	2.400	2.400
0050	625.0500	Salvaged Topsoil	SY	90.000	90.000
0052	627.0200	Mulching	SY	65.000	65.000
0054	628.1504	Silt Fence	LF	360.000	360.000
0056	628.1520	Silt Fence Maintenance	LF	580.000	580.000
0058	628.1905	Mobilizations Erosion Control	EACH	4.000	4.000
0060	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0062	628.2008	Erosion Mat Urban Class I Type B	SY	25.000	25.000
0064	628.6005	Turbidity Barriers	SY	180.000	180.000
0066	629.0210	Fertilizer Type B	CWT	0.250	0.250
0068	630.0130	Seeding Mixture No. 30	LB	16.000	16.000
0070	630.0200	Seeding Temporary	LB	10.000	10.000
0072	630.0500	Seed Water	MGAL	6.500	6.500
0074	634.0612	Posts Wood 4x6-Inch X 12-FT	EACH	4.000	4.000
0076	637.2230	Signs Type II Reflective F	SF	12.000	12.000
0078	638.2602	Removing Signs Type II	EACH	13.000	13.000
0080	638.3000	Removing Small Sign Supports	EACH	10.000	10.000
0082	642.5001	Field Office Type B	EACH	1.000	1.000
0084	643.0420	Traffic Control Barricades Type III	DAY	1,633.000	1,633.000
0086	643.0705	Traffic Control Warning Lights Type A	DAY	3,266.000	3,266.000
0088	643.0900	Traffic Control Signs	DAY	1,278.000	1,278.000
0090	643.5000	Traffic Control	EACH	1.000	1.000
0092	645.0111	Geotextile Type DF Schedule A	SY	68.000	68.000
0094	645.0120	Geotextile Type HR	SY	200.000	200.000
0096	650.4500	Construction Staking Subgrade	LF	100.000	100.000
0098	650.5000	Construction Staking Base	LF	100.000	100.000

Estimate Of Quantities

5405-00-70

Line	Item	Item Description	Unit	Total	Qty
0100	650.6501	Construction Staking Structure Layout (structure) 01. B-62-0272	EACH	1.000	1.000
0102	650.9911	Construction Staking Supplemental Control (project) 01. 5405-00-70	EACH	1.000	1.000
0104	650.9920	Construction Staking Slope Stakes	LF	100.000	100.000
0106	715.0502	Incentive Strength Concrete Structures	DOL	894.000	894.000
0108	715.0720	Incentive Compressive Strength Concrete Pavement	DOL	500.000	500.000

3

DIVISION	FROM/TO STATION	205.0100 EXCAVATION COMMON (1) CUT (2)	SALVAGED/UNUSABLE PAVEMENT MATERIAL (3)	AVAILABLE MATERIAL (4)	UNEXPANDED FILL	EXPANDED FILL (5) FACTOR 1.25	MASS ORDINATE +/- (6)	WASTE (7)
DIVISION 1								
WEST APPROACH	11+78.86/12+35.21	61	---	61	39	49	12	12
DIVISION 1 SUBTOTAL		61	---	61	39	49	12	12
DIVISION 2								
EAST APPROACH	12+65.41/13+21.76	74	---	74	5	6	68	68
DIVISION 2 SUBTOTAL		74	---	74	5	6	68	68
GRAND TOTAL		135	---	135	44	55	80	80

NOTES:
(1) EXCAVATION COMMON IS THE SUM OF THE CUT AND EBS EXCAVATION COLUMNS. ITEM NUMBER 205.0100
(2) SALVAGED/UNSUABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.
(3) SALVAGED/UNUSABLE PAVEMENT MATERIAL
(4) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSABLE PAVEMENT MATERIAL
(5) EXPANDED FILL FACTOR = 1.25
(6) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.
(7) FACTORS USED TO COMPUTE ANTICIPATED WASTE AND THE COMPUTED WASTE VOLUME IDENTIFIED ARE FOR GENERAL INFORMATION ONLY.

BASE AGGREGATE DENSE

STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	624.0100 WATER MGAL
11+79	-	12+29	WEST APPROACH	10	110	1.2
12+72	-	13+22	EAST APPROACH	10	110	1.2
			TOTAL	20	220	2.4

CONCRETE PAVEMENT

STATION	TO	STATION	LOCATION	415.0060 CONCRETE PAVEMENT 6-INCH SY	415.0410 CONCRETE PAVEMENT APPROACH SLAB SY
12+09	-	12+29	WEST APPROACH	7	46
12+71	-	12+92	EAST APPROACH	7	47
			TOTAL	14	93

ASPHALTIC SURFACE

STATION	TO	STATION	LOCATION	455.0605 TACK COAT GAL	465.0105 ASPHALTIC SURFACE TON
11+79	-	12+29	WEST APPROACH	4	17
12+72	-	13+22	EAST APPROACH	4	17
			TOTAL	8	34

MOBILIZATIONS EROSION CONTROL

LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH
PROJECT	4	2
TOTAL	4	2

SILT FENCE

STATION	TO	STATION	LOCATION	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF
11+79	-	12+29	WEST APPROACH, LT	66	132
11+79	-	12+29	WEST APPROACH, RT	92	184
12+72	-	13+22	EAST APPROACH, LT	81	162
12+72	-	13+22	EAST APPROACH, RT	51	102
			UNDISTRIBUTED	70	---
			TOTAL	360	580

TURBIDITY BARRIERS

LOCATION	628.6005 SY
WEST APPROACH	68
EAST APPROACH	76
UNDISTRIBUTED	36
TOTAL	180

FINISHING ITEMS

STATION	TO	STATION	LOCATION	625.0500 SALVAGED TOPSOIL SY	627.0200 MULCHING SY	628.2008 EROSION MAT URBAN CLASS I TYPE B SY	629.0210 FERTILIZER TYPE B CWT	630.0130 SEEDING MIXTURE NO. 30 LB	630.0200 SEEDING TEMPORARY LB	630.0500 SEED WATER MGAL
11+79	-	12+29	WEST APPROACH, LT	27	16	11	0.05	4	2	1.6
11+79	-	12+29	WEST APPROACH, RT	16	16	---	0.04	3	2	1.1
12+72	-	13+22	EAST APPROACH, LT	18	10	9	0.06	4	3	1.8
12+72	-	13+22	EAST APPROACH, RT	9	9	---	0.02	2	1	0.6
			UNDISTRIBUTED	20	14	5	0.08	3	2	1.4
			TOTAL	90	65	25	0.25	16	10	6.5

TRAFFIC CONTROL

LOCATION	DURATION DAY	643.0420 TRAFFIC CONTROL BARRICADES TYPE III NO.	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAY	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A NO.	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	643.0900 TRAFFIC CONTROL SIGNS NO.	643.0900 TRAFFIC CONTROL SIGNS DAY	643.5000 TRAFFIC CONTROL EACH
WEST APPROACH	71	9	639	18	1,278	7	497	---
EAST APPROACH	71	9	639	18	1,278	7	497	---
UNDISTRIBUTED	71	5	355	10	710	4	284	---
PROJECT	---	---	---	---	---	---	---	1
TOTAL		23	1,633	46	3,266	18	1,278	1

PERMANENT SIGNING

STATION	LOCATION	SIGN NUMBER	SIGN CODE	634.0612 POSTS WOOD 4X6-INCH X 12-FT EACH	637.2230 SIGNS TYPE II REFLECTIVE F SF	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	REMARKS
---	WEST APPROACH, LT	1R	R12-55	---	---	1	1	20 TON BRIDGE 1.2 MILES AHEAD
---	WEST APPROACH, LT	2R	W5-2	---	---	1	1	NARROW BRIDGE
12+18	WEST APPROACH, LT	3R	R12-1	---	---	2	1	WEIGHT LIMIT 20 TONS & WATCH FOR SIGN AHEAD
12+21	WEST APPROACH, LT	4	W5-52L	1	3	---	---	BRIDGE HASH MARKS
12+25	WEST APPROACH, RT	4R	W5-52L	---	---	1	1	BRIDGE HASH MARKS
12+31	WEST APPROACH, RT	5R	W5-52R	---	---	1	1	BRIDGE HASH MARKS
12+35	WEST APPROACH, RT	5	W5-52R	1	3	---	---	BRIDGE HASH MARKS
12+65	EAST APPROACH, LT	6	W5-52R	1	3	---	---	BRIDGE HASH MARKS
12+69	EAST APPROACH, LT	6R	W5-52R	---	---	1	1	BRIDGE HASH MARKS
12+76	EAST APPROACH, LT	7R	R12-1	---	---	2	1	WEIGHT LIMIT 20 TONS & SIGN
12+80	EAST APPROACH, RT	8R	W5-52L	---	---	1	1	BRIDGE HASH MARKS
12+80	EAST APPROACH, RT	8	W5-52L	1	3	---	---	BRIDGE HASH MARKS
---	EAST APPROACH, RT	9R	W5-2	---	---	1	1	NARROW BRIDGE
---	EAST APPROACH, RT	10R	R12-55	---	---	2	1	20 TON BRIDGE 1 MILES AHEAD & WATCH FOR SIGN AHEAD
			TOTAL	4	12	13	10	

CONSTRUCTION STAKING

STATION	TO	STATION	LOCATION	650.4500 CONSTRUCTION STAKING SUBGRADE LF	650.5000 CONSTRUCTION STAKING BASE LF	650.6501.01 CONSTRUCTION STAKING STRUCTURE LAYOUT (STRUCTURE) (01. B-62-0272) EACH	650.9911.01 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (01. 5405-00-70) EACH	650.9920 CONSTRUCTION STAKING SLOPE STAKES LF
11+79	-	12+29	WEST APPROACH	50	50	---	---	50
12+72	-	13+22	EAST APPROACH	50	50	---	---	50
			PROJECT	---	---	1	1	---
			TOTAL	100	100	1*	1	100

*CATEGORY 0020

ALL ITEMS CATEGORY 0010 UNLESS NOTED OTHERWISE

PROJECT NO: 5405-00-70	HWY: PRAIRIE RD	COUNTY: VERNON	MISCELLANEOUS QUANTITIES	SHEET 8	E

LEGEND

- SLOPE INTERCEPT
- SURFACE WATER FLOW
- STREAM EDGE
- SILT FENCE
- TURBIDITY BARRIER
- EXISTING SIGN
- PROPOSED SIGN
- CONCRETE PAVEMENT APPROACH SLABS
- JOINT

ARMAND BRINGE TRUST

BARN

SLOPE INTERCEPT, TYP.

BP: 10+00.00

EXISTING R/W

BM 1

PI: 11+11.01

PC: 11+65.53

PT: 12+26.65

PC: 12+80.23

P.E. (B.A.D.)

CP 3

BM 2

GUY

VERNON ELECTRIC COOP

VERNON COMMUNICATIONS COOP

EP: 14+48.70

12+00

13+00

14+00

11+00

PRAIRIE RD (B.A.D.)

CP 2

CP 4

CP 1

KING RD (B.A.D.)

CONCRETE PAVEMENT APPROACH SLAB, TYP.

RUSH CREEK

P.E. (B.A.D.)

EXISTING R/W

VERNON COMMUNICATIONS COOP

BENCH MARKS

NO.	STATION	DESCRIPTION	ELEV.
BM 1	10+58.45	RAILROAD SPIKE IN POWER POLE, 32.13' LT	1008.620
BM 2	13+57.47	RAILROAD SPIKE IN POWER POLE, 33.10' LT	1017.081

BEGIN PROJECT
STA. 11+78.86
Y: 106 820.664
X: 672 943.522
MATCH EXISTING

ARMAND BRINGE TRUST

END PROJECT
STA. 13+21.76
MATCH EXISTING

9+50.00 10+00.00 11+00.00 12+00.00 13+00.00 14+00.00 15+00.00 15+50.00

1020 1015 1010 1005 1004

STA 11+78.86
EL 1014.33

VPC STA 12+02.10
VPC EL 1015.32

VPI STA 12+32.12
VPI EL 1016.60

VPT STA 12+62.14
VPT EL 1016.44

STA 13+21.76
EL 1016.11

PROPOSED ℄ PROFILE

-0.55%

4.27%

HIGH WATER$_{100}$
EL. 1015.30

EXISTING ℄ PROFILE

HP STA 12+55.32
HP EL 1016.46

VCL = 60.04

K = 12.46

APPROX. STREAMBED EL. = 1004.03
DRY RUN
OBSERVED WATER EL. - 1004.03
(12-13-2022)

EXISTING ℄ ELEVATION

PROPOSED ℄ ELEVATION

1010.94 1010.67 1011.52 1013.11 1015.20 1015.23 1004.47 1016.45 1016.22 1016.23 1016.05 1016.34

STA. 12+50.30
REMOVE EXISTING STRUCTURE P-62-0329
34.3-FT OVERALL LENGTH AND
20.0-FT CLEAR ROADWAY WIDTH
SINGLE SPAN STEEL DECK GIRDER STRUCTURE

STA. 12+50.30
STRUCTURE B-62-0272 REQUIRED
42.9-FT OVERALL LENGTH AND
24.0-FT CLEAR ROADWAY WIDTH
SINGLE SPAN REINFORCED CONCRETE
FLAT SLAB STRUCTURE

PROJECT NO: 5405-00-70	HWY: PRAIRIE RD	COUNTY: VERNON	PLAN & PROFILE	SHEET 9	E

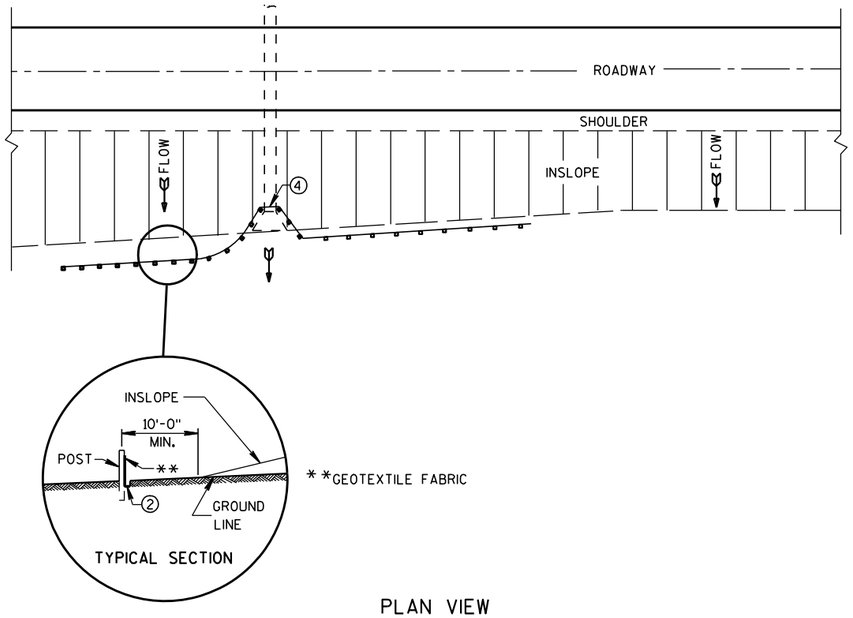
FILE NAME : G:\00-PROJECT FILES\2022\22317 PRAIRIE RD, RUSH CREEK BRIDGE, VERNON COUNTY\0-CAD\SHEETS\050101_PP.DWG
LAYOUT NAME - 050101_pp

PLOT DATE : 10/17/2025 9:59 AM PLOT BY : KYLE MAYBERRY PLOT NAME : PLOT SCALE : 1 IN:40 FT

Standard Detail Drawing List

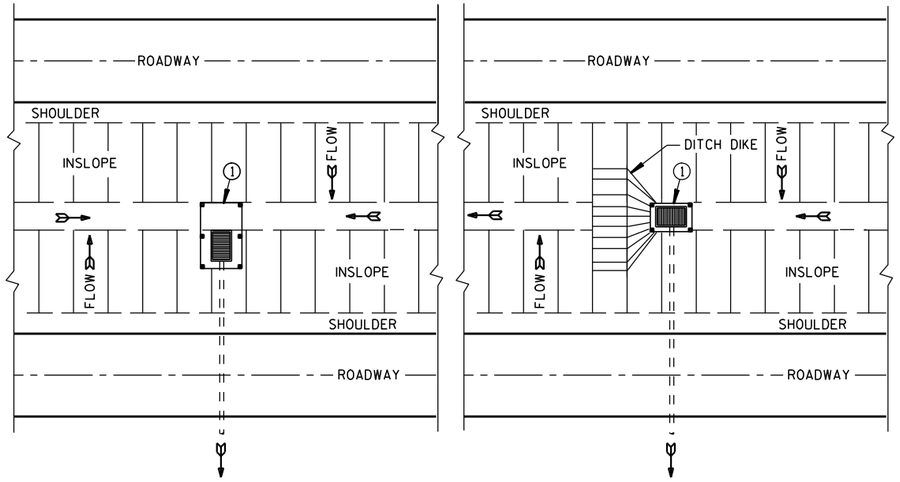
08E09-06	SILT FENCE
08E11-02	TURBIDITY BARRIER
12A03-10	NAME PLATE (STRUCTURES)
13B02-09A	CONCRETE PAVEMENT APPROACH SLAB
13C19-03	HMA LONGITUDINAL JOINTS
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C06-12	SIGNING & MARKING FOR TWO LANE BRIDGES
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS

6



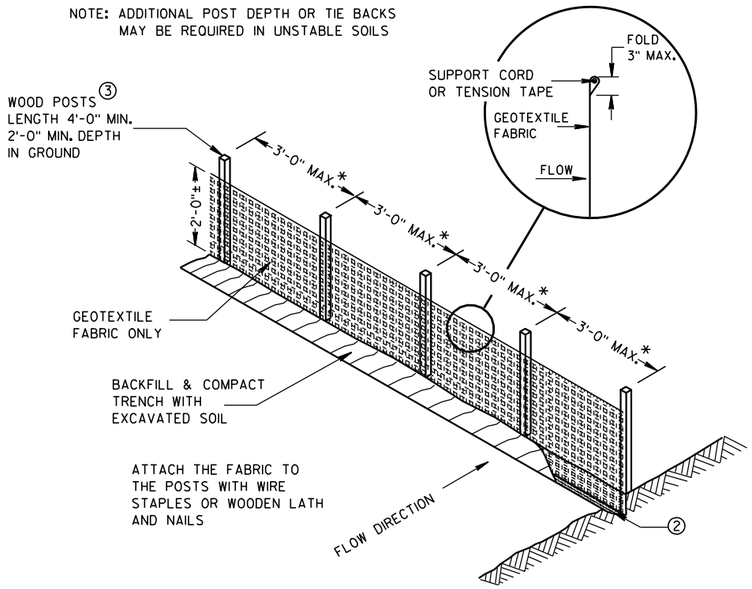
PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



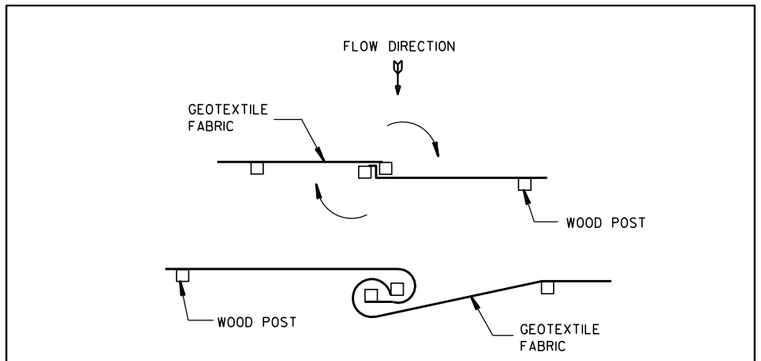
PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS



*NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.

SILT FENCE

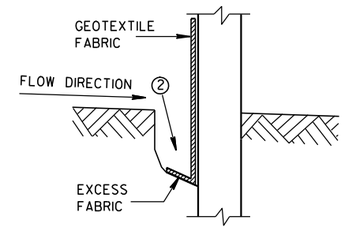


JOINING TWO LENGTHS OF SILT FENCE 5

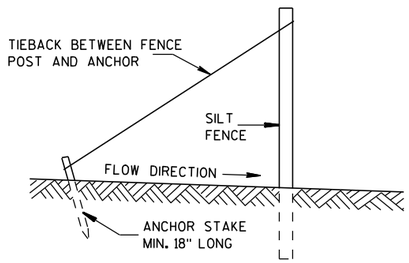
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

1. HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
2. FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
3. WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.
4. SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
5. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



TRENCH DETAIL



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

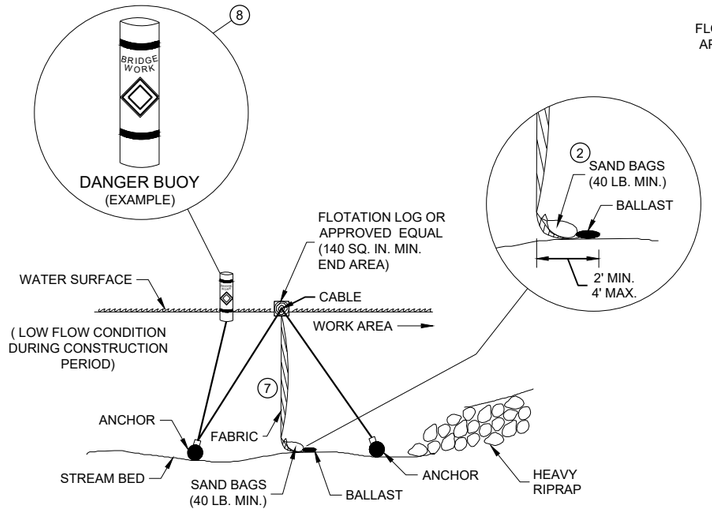
APPROVED
4-29-05 DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6

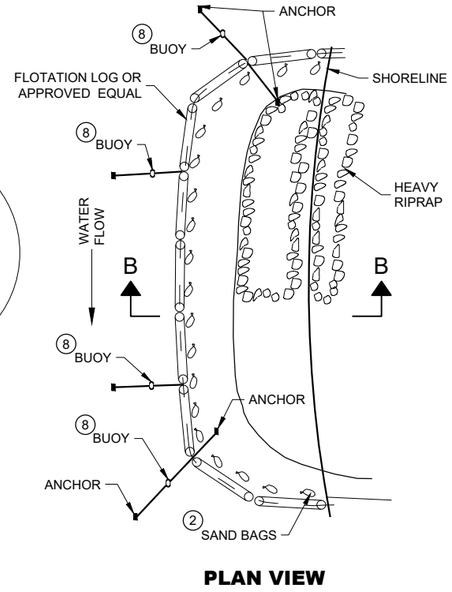
S.D.D. 8 E 9-6

6

S.D.D. 8 E 9-6

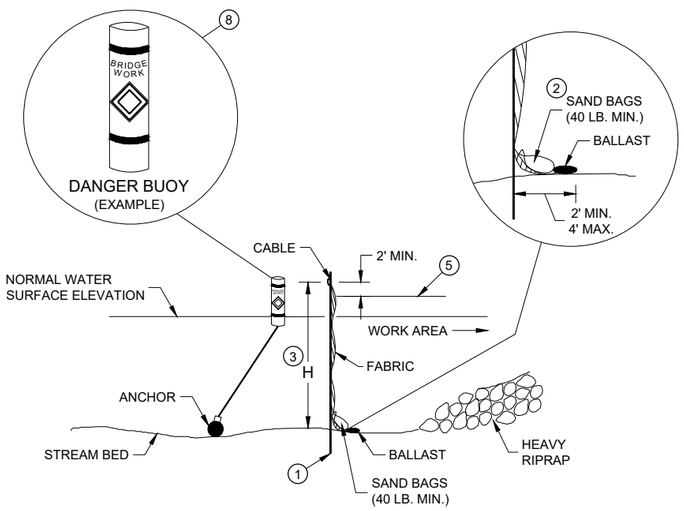


SECTION B - B

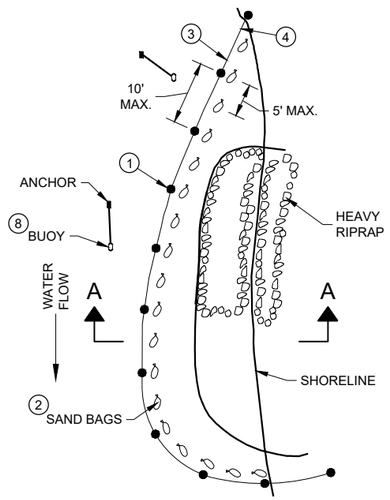


PLAN VIEW

TURBIDITY BARRIER - FLOAT ALTERNATIVE
CAUTION - SEE NOTE 6



SECTION A - A



PLAN VIEW

TURBIDITY BARRIER - STANDARD POST INSTALLATION

TURBIDITY BARRIER PLACEMENT DETAILS

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TURBIDITY BARRIER MAY BE REMOVED AT THE ENGINEERS DISCRETION, WHEN PERMANENT EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

(1) DRIVEN STEEL POSTS, PIPES, OR CHANNELS. LENGTH SHALL BE SUFFICIENT TO SECURELY SUPPORT BARRIER AT HIGH WATER ELEVATIONS.

(2) SAND BAGS TO BE USED AS ADDITIONAL BALLAST WHEN ORDERED BY THE ENGINEER TO MEET ADVERSE FIELD CONDITIONS. SPACE AS APPROPRIATE FOR SITE CONDITIONS.

(3) WHEN BARRIER HEIGHT "H" EXCEEDS 8 FEET, POST SPACING MAY NEED TO BE DECREASED.

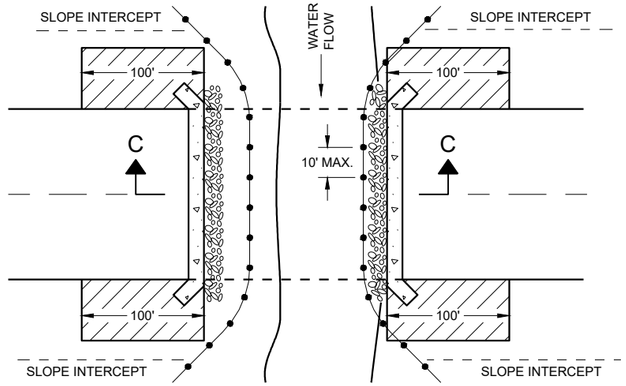
(4) IN WATERWAYS SUBJECT TO FLUCTUATING WATER ELEVATIONS, PROVISIONS SHOULD BE MADE TO ALLOW THE WATER TO EQUALIZE ON EACH SIDE OF THE BARRIER. THIS MAY BE ACCOMPLISHED BY LEAVING A PORTION OF THE BARRIER OPEN ON THE UPSTREAM END.

(5) ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION PERIOD. MINIMUM BARRIER HEIGHT SHALL BE 2' GREATER THAN EITHER THE Q2 ELEVATION OR THE ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION, WHICHEVER IS GREATER.

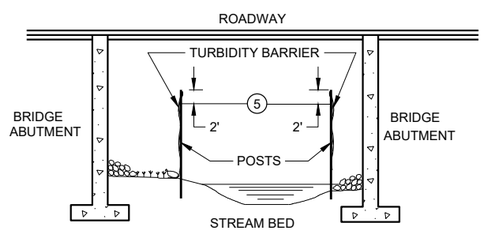
(6) FLOAT ALTERNATIVE WILL ONLY BE ALLOWED WITH WRITTEN APPROVAL OF THE ENGINEER, AND IS MEANT FOR LOCATIONS WHERE BEDROCK PREVENTS THE INSTALLATION OF POSTS.

(7) ALLOW SUFFICIENT SLACK VERTICALLY AND HORIZONTALLY SO THAT SEDIMENT BUILD UP WILL NOT SEPARATE OR LOWER THE TURBIDITY BARRIER.

(8) USE AS DIRECTED BY COAST GUARD OR DNR PERMIT WHEN WORKING IN NAVIGABLE WATERWAYS.



PLAN VIEW



SECTION C - C

TURBIDITY BARRIER DETAIL SHOWING TYPICAL PLACEMENT AT STRUCTURES

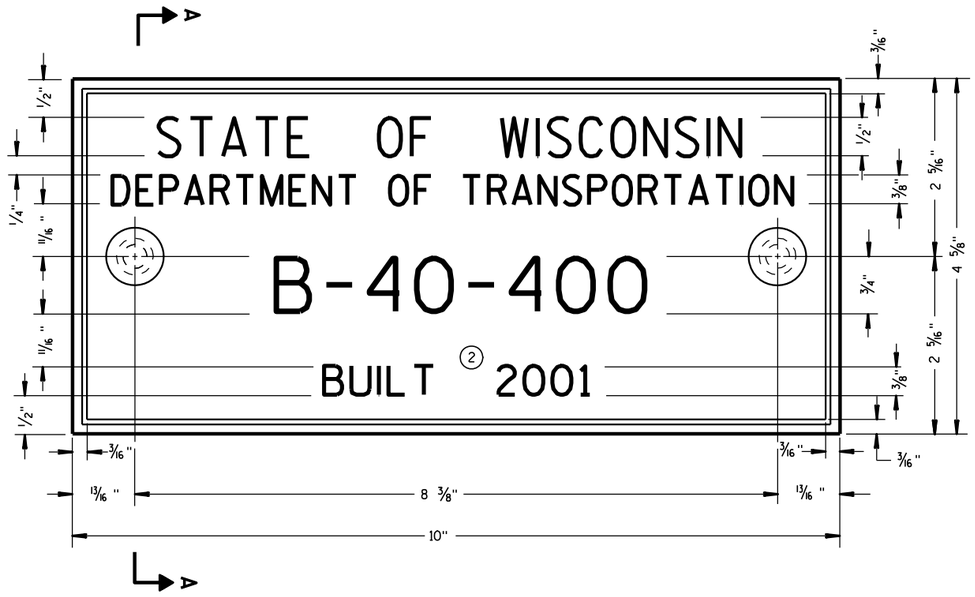
TURBIDITY BARRIER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/4/02
DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA

6
SDD 08E11 - 02



TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)

FOR MULTI-UNIT STRUCTURES
LINE 3 ABOVE SHALL READ

B = BRIDGE
C = CULVERT
R = RETAINING WALL

COUNTY NO.

BRIDGE NO.

UNIT NO. FOR MULTIPLE UNIT BRIDGE

B-40-400-1A

NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES

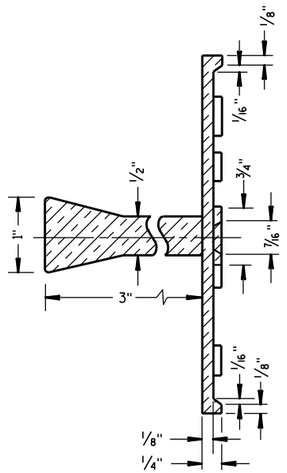
GENERAL NOTES

NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

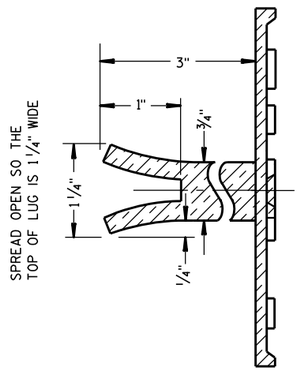
THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

(1) EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.

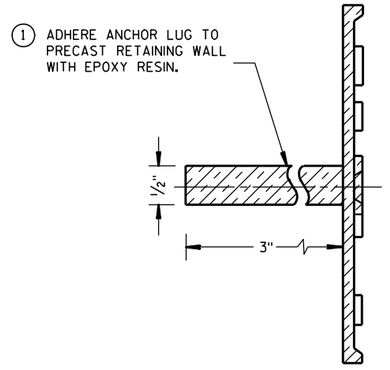
(2) REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.



SECTION A-A



ALTERNATE LUG



ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

NAME PLATE (STRUCTURES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

3/26/10 DATE

/S/ Scot Beck -- CHIEF STRUCTURAL DEVELOP ENGINEER

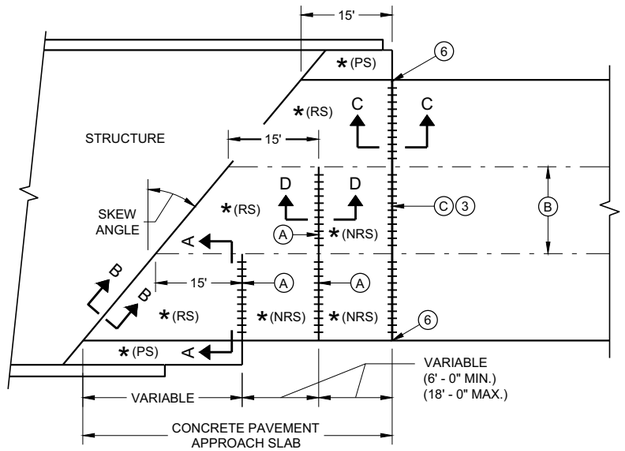
FHWA

6

S.D.D. 12 A 3-10

6

S.D.D. 12 A 3-10



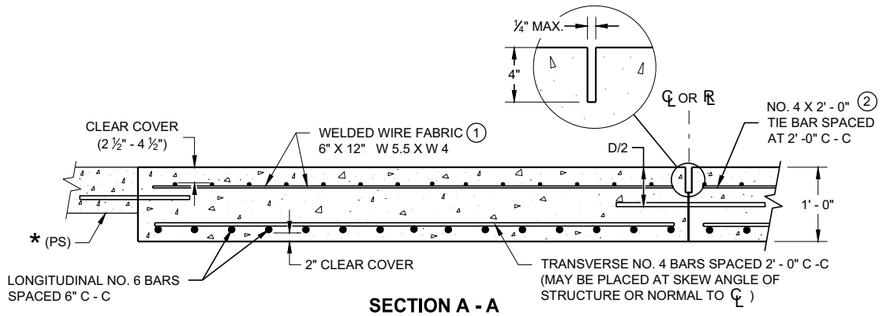
SKEWED APPROACH
(PAVEMENT MORE THAN TWO LANES)

* (RS) = REINFORCED CONCRETE SLAB

* (PS) = PAVED CONCRETE SHOULDER OR CONCRETE DRAINAGE SLAB

* (NRS) - NON - REINFORCED CONCRETE SLAB

*** STANDARD DOWEL BAR DIAMETER (SEE SDD 13C11 AND SDD 13C13)



SECTION A - A
REINFORCEMENT POSITIONING DETAIL

GENERAL NOTES

THE CONTRACTOR MAY SPLICE NO. 6 BARS IN THE APPROACH SLAB FOR SKEWED STRUCTURES ONLY. STAGGER SPLICES WITH A MAXIMUM OF ONE SPLICE PER BAR. THE LENGTH OF LAP IS 20 INCHES.

TACK WELD DOWEL BARS TO THE BASKETS ON ALTERNATE ENDS.

(1) THE CONTRACTOR MAY USE NO. 4 BARS SPACED AT 2' - 0 " C - C IN BOTH THE LONGITUDINAL AND TRANSVERSE DIRECTIONS FOR TOP REINFORCEMENT AS AN ALTERNATIVE TO THE WELDED WIRE FABRIC.

(2) THE CONTRACTOR MAY OMIT THE BARS BETWEEN REINFORCED SLABS WHERE SLAB REINFORCEMENT BARS EXTEND ACROSS THE CENTERLINE OR REFERENCE LINE.

(3) DO NOT CONSTRUCT AN EXPANSION JOINT OR INSTALL DOWEL BARS WHEN ABUTTING AN HMA PAVEMENT.

(4) USE A JOINT SEALANT CONFORMING TO STANDARD SPECIFICATION 415.2.6.

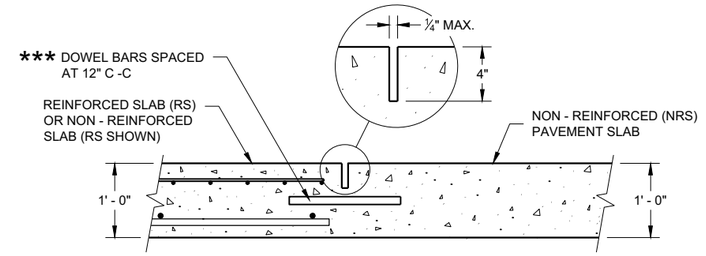
(5) PLACE EXPANSION CAP ON THE END OF THE DOWEL THAT IS NOT TACK WELDED TO THE BASKET. DO NOT FORCE DOWEL BAR PAST THE DOWEL STOP.

(6) EXTEND EXPANSION JOINT THROUGH ANY ADJACENT TIED CONCRETE.

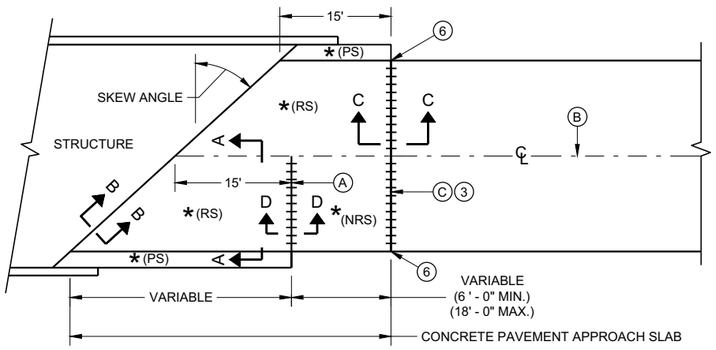
(A) STANDARD CONTRACTION JOINT NORMAL TO ℄ OR ℞.

(B) STANDARD LONGITUDINAL JOINT WITH TIE BARS.

(C) 1 ½" EXPANSION JOINT WITH DOWEL BARS NORMAL TO ℄ OR ℞ .

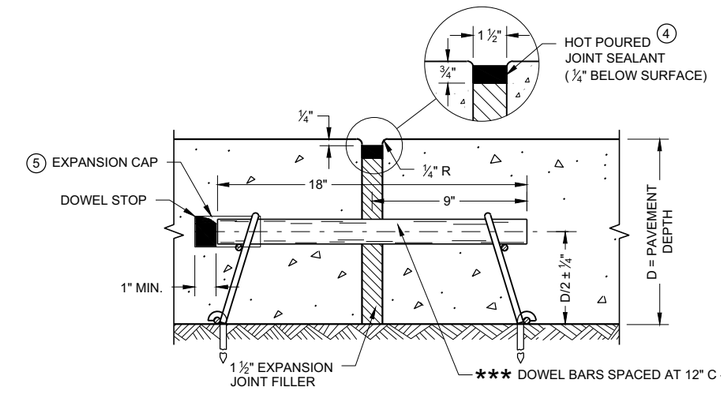


SECTION D - D
CONTRACTION JOINT

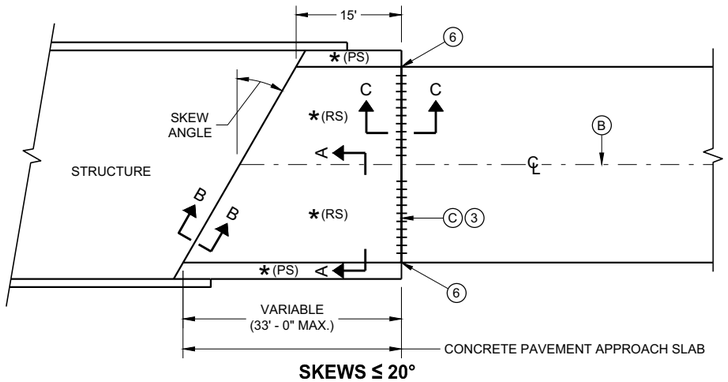


SKEWS > 20°
(PAVEMENT WIDTH ≤ 30')

SECTION B - B
BEND DETAIL
BOTTOM REINFORCEMENT

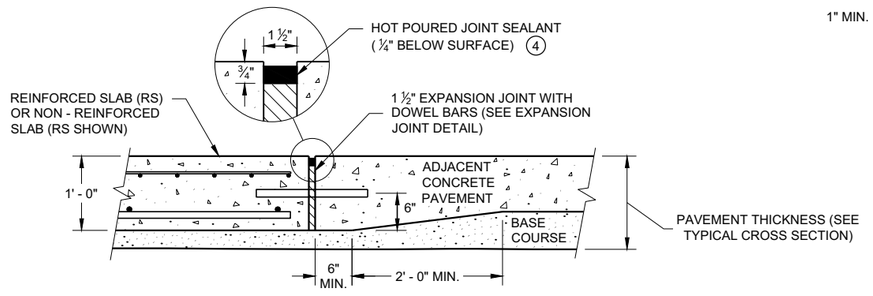


EXPANSION JOINT DETAIL



SKEWS ≤ 20°
(PAVEMENT WIDTH ≤ 30')

APPROACH SLAB AND ADJACENT PAVEMENT



SECTION C - C
TRANSITION DETAIL
APPROACH SLAB TO ADJACENT PAVEMENT

CONCRETE PAVEMENT APPROACH SLAB

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

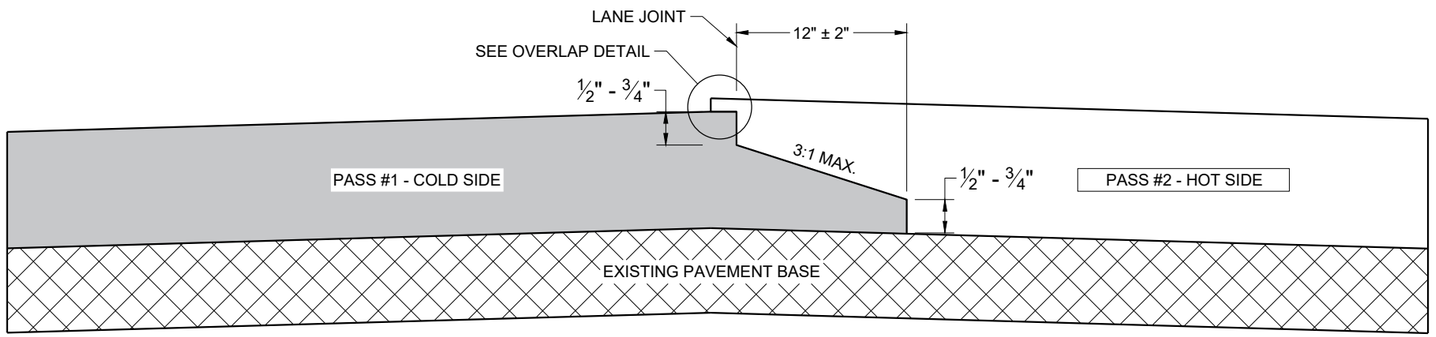
APPROVED
November 2018
DATE

/S/ Peter Kemp, P.E.
PAVEMENT SUPERVIS

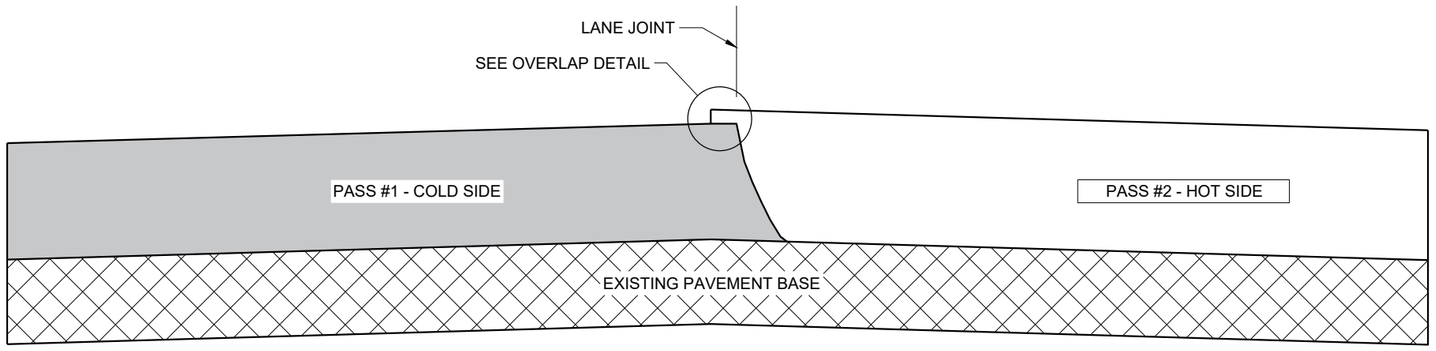
FHWA

6

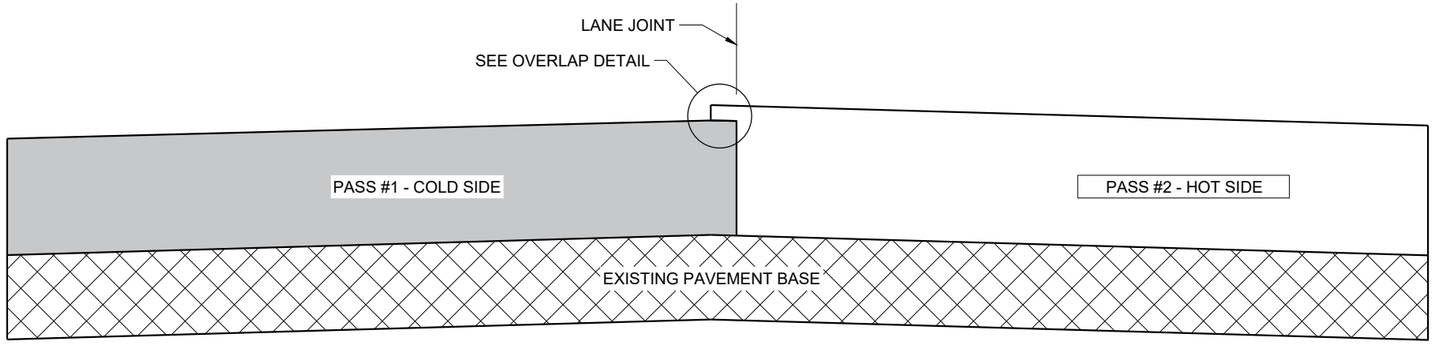
SDD 13B02 - 09a



TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)

GENERAL NOTES

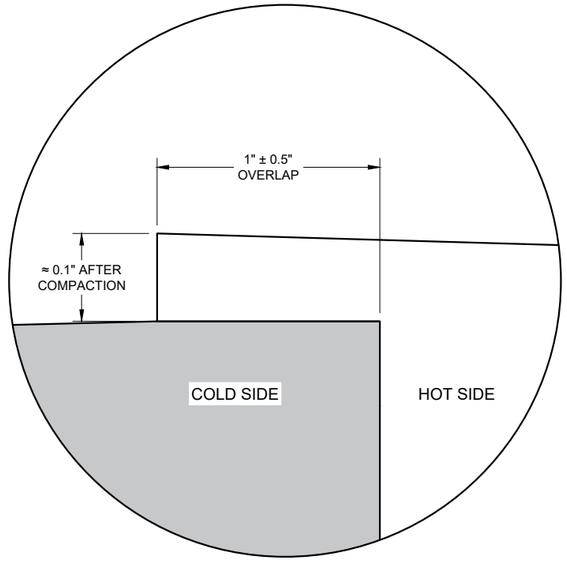
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY 1" ± 0.5" AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

6

6

SDD 13C19 - 03

SDD 13C19 - 03

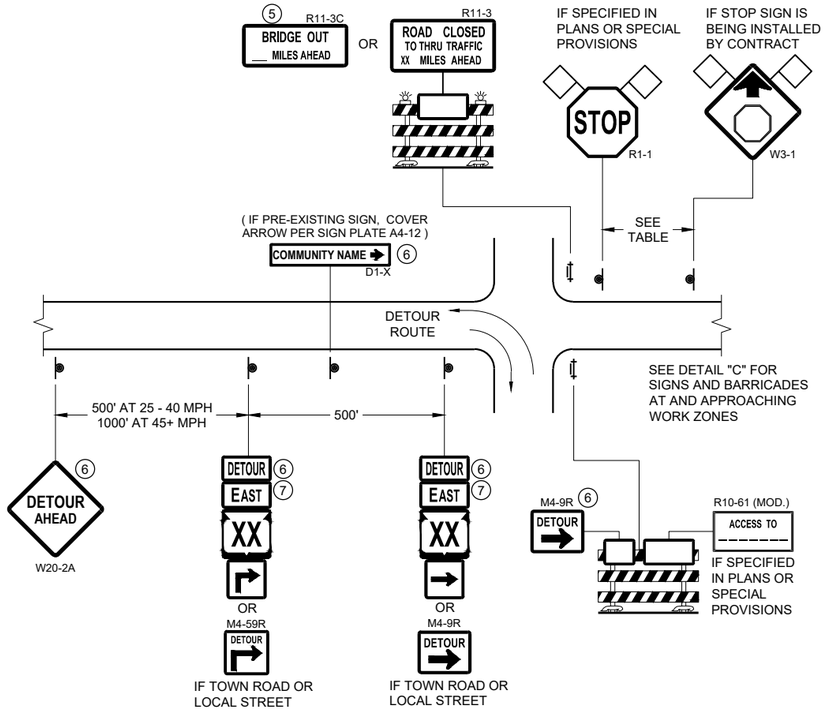
HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2020
DATE

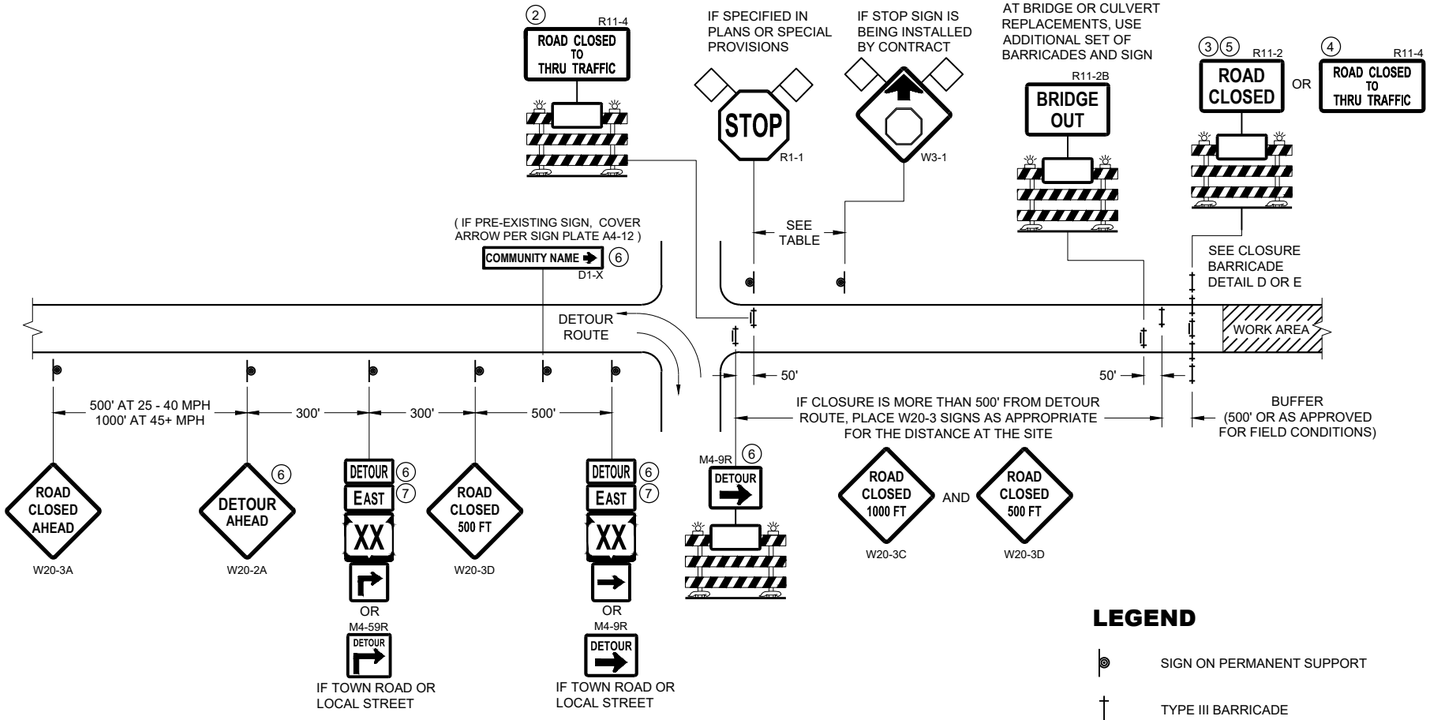
/S/ Steven Hefel
HMA PAVEMENT ENGIN

FHWA



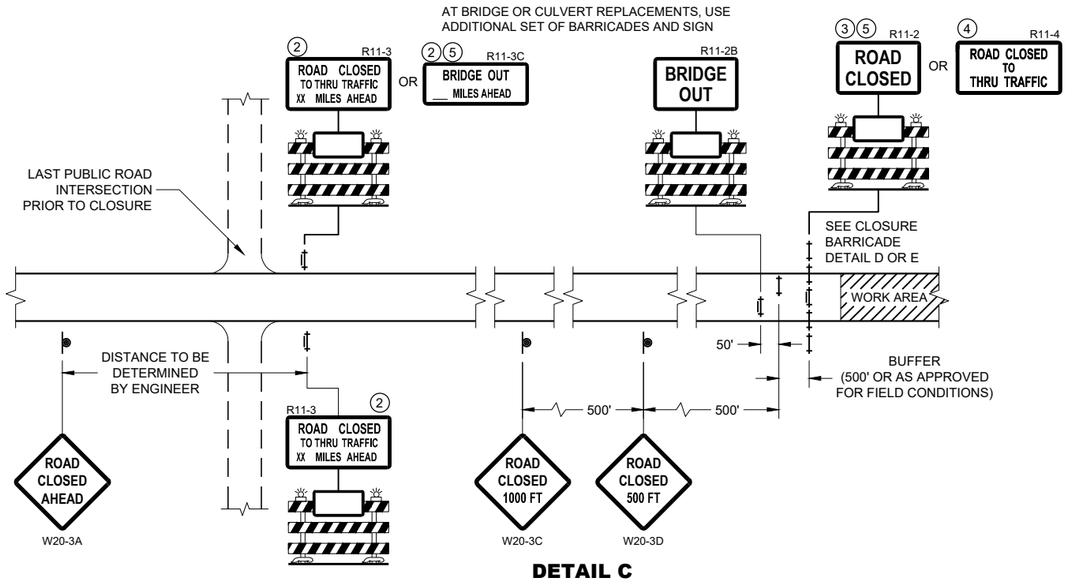
DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN OR EQUAL TO ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

DETOUR M4 - 8
EAST M3 - X
XX M1 - 4 OR XX M1 - 6 OR COUNTY X M1 - 5A
M05 - 1 OR M06 - 1

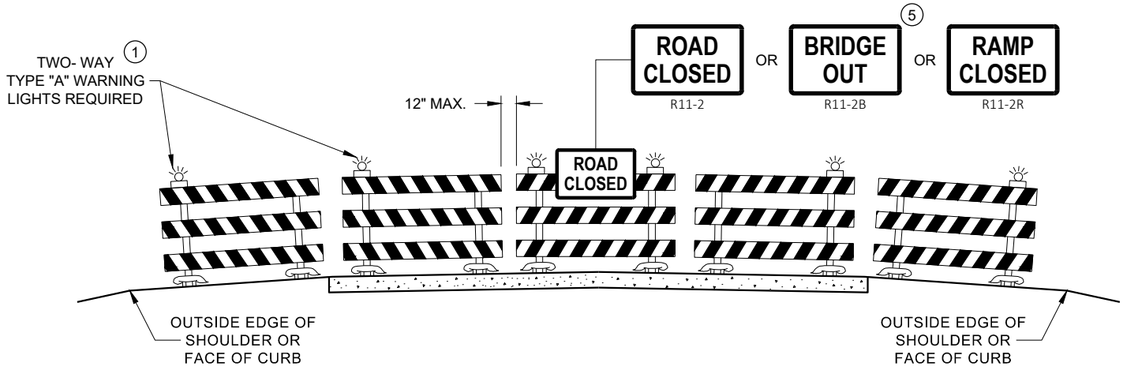
BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

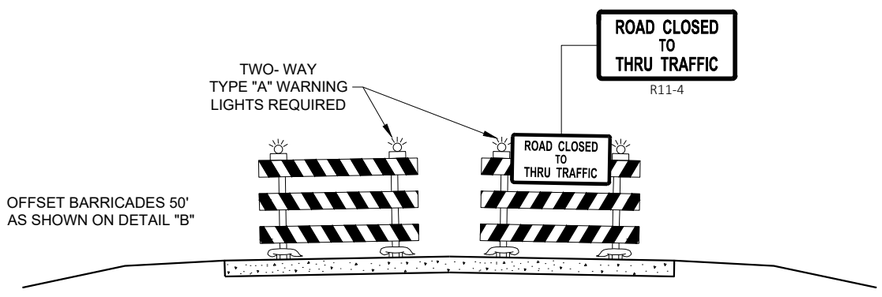
APPROVED
May 2023
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

SDD 15C02 - 09a



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60 " X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING.

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR VARIOUS CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEE
FHWA

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SDD 15C02 - 09b

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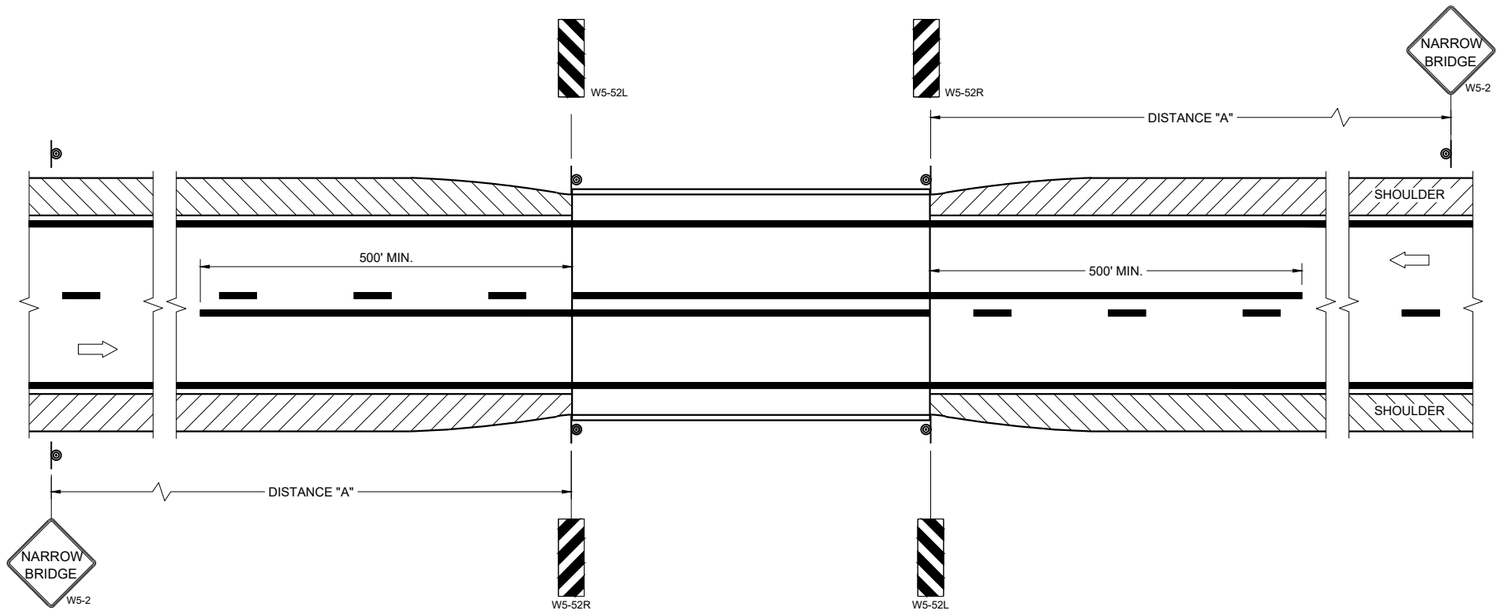
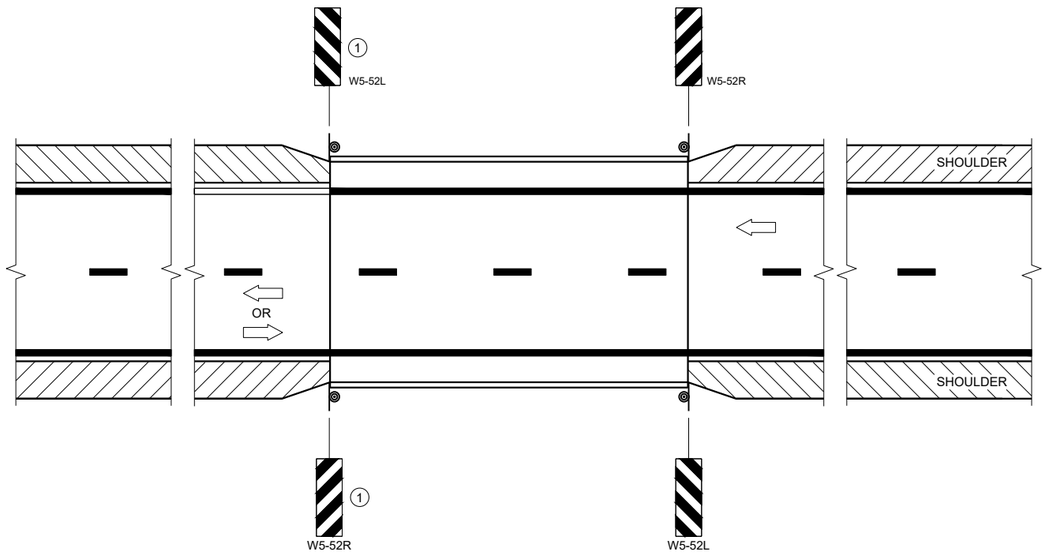
SDD 15C02 - 09b



SITUATION 1

WARRANTING CRITERIA:
BRIDGE WIDTH IS AT LEAST 16 FEET BUT LESS THAN 24 FEET.



SITUATION 2

WARRANTING CRITERIA:
1. BRIDGE WIDTH IS AT LEAST 24 FEET AND
2. BRIDGE SHOULDER WIDTH IS LESS THAN 6 FEET

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THE DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

LOCATE W5-52 SIGN POST(S) BEHIND GUARDRAIL WHEN PRESENT.

PLACE THE EDGE OF THE W5-52 SIGN IN LINE WITH FACE OF CURB OR PARAPET.

ON BRIDGE ONLY PROJECTS, PLACE 300 FEET OF EDGELINE.

OMIT EDGELINES ON ROADWAYS WITHOUT EXISTING EDGELINES.

(1) OMIT ON ONE-WAY TRAVELED WAYS.

LEGEND

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC

DISTANCE TABLE

POSTED OR 85TH PERCENTILE SPEED	DISTANCE "A"
25	150'
30	200'
35	250'
40	300'
45	400'
50	550'
55	700'

SIGNING AND MARKING FOR TWO LANE BRIDGES

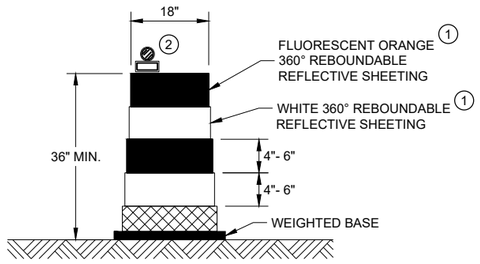
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE
/S/ Jeannie Silver
Statewide Pavement Marking Engineer
FHWA

6
SDD 15C06-12

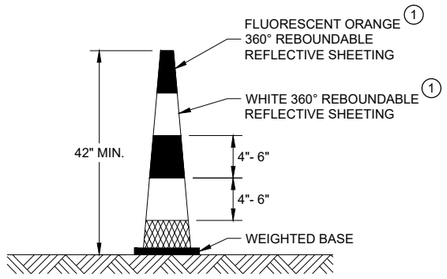
GENERAL NOTES

1. REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
2. LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



DRUM

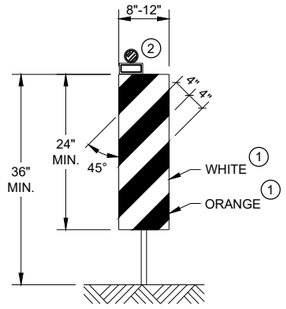
BALLAST WIDTHS
RANGE FROM 24"-36"



42" CONE

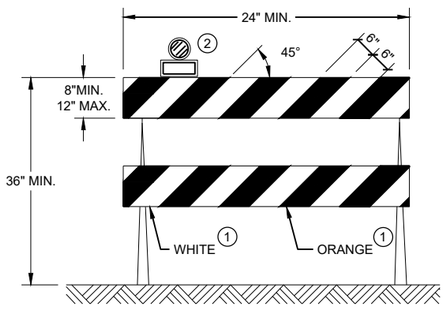
DO NOT USE IN TAPERS
½ SPACING OF DRUMS

BALLAST WIDTHS
RANGE FROM 14"-20"



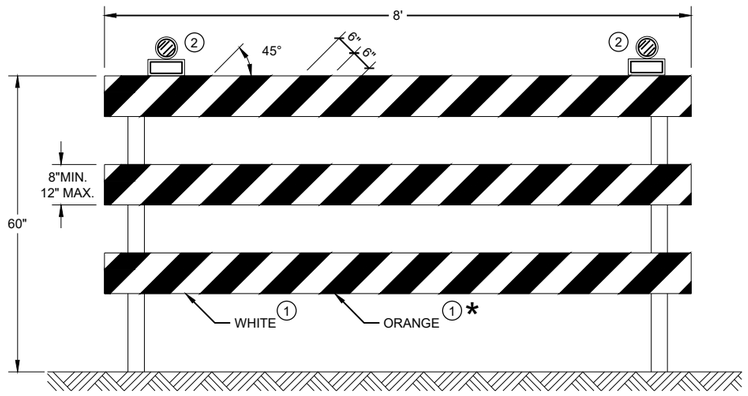
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

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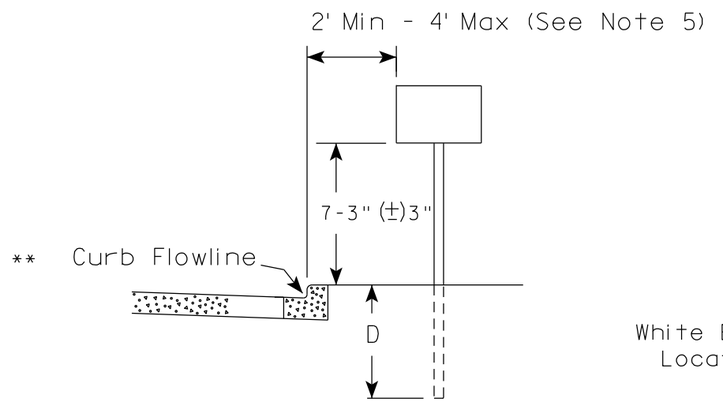
SDD 15C11 - 10b

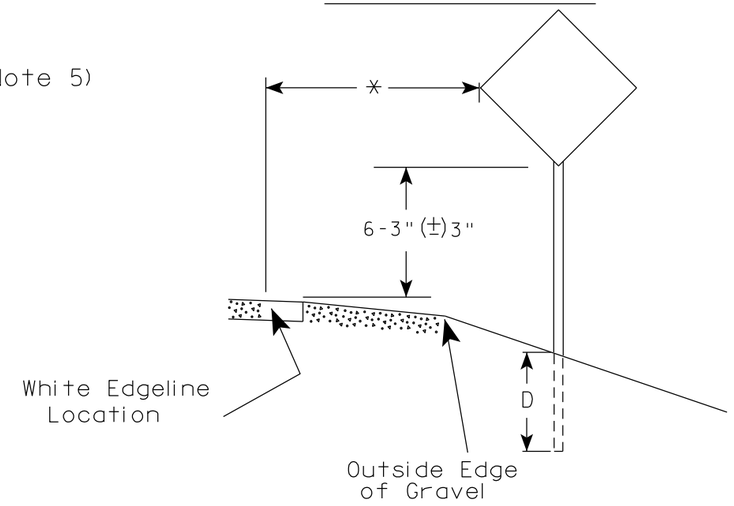
6

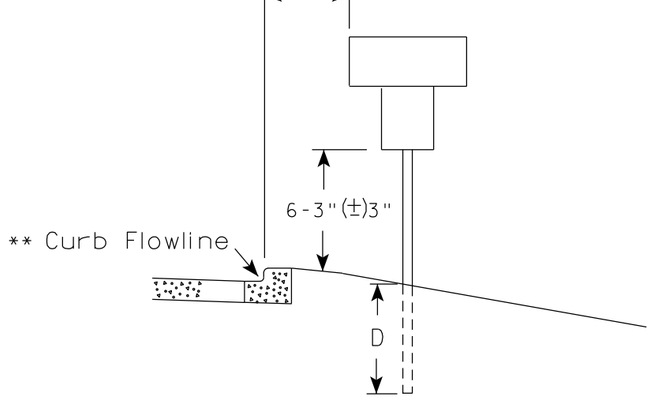
SDD 15C11 - 10b

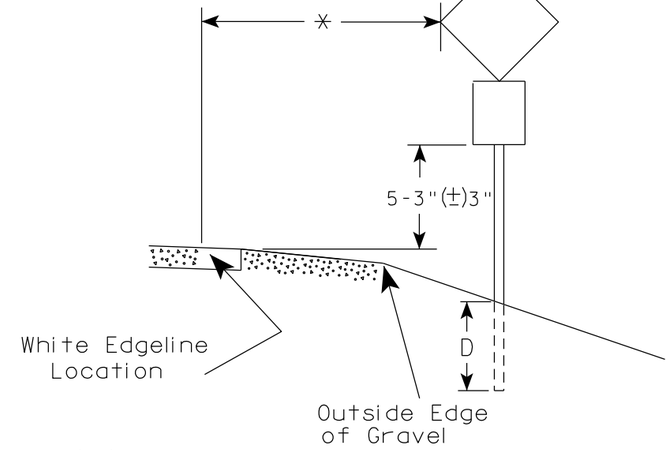
URBAN AREA



RURAL AREA (See Note 2)







** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
   The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".
3. For expressways and freeways, mounting height is 7'- 3" (±) 3" or 6'-3" (±) 3" depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±) 3".
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directd by the Engineer.

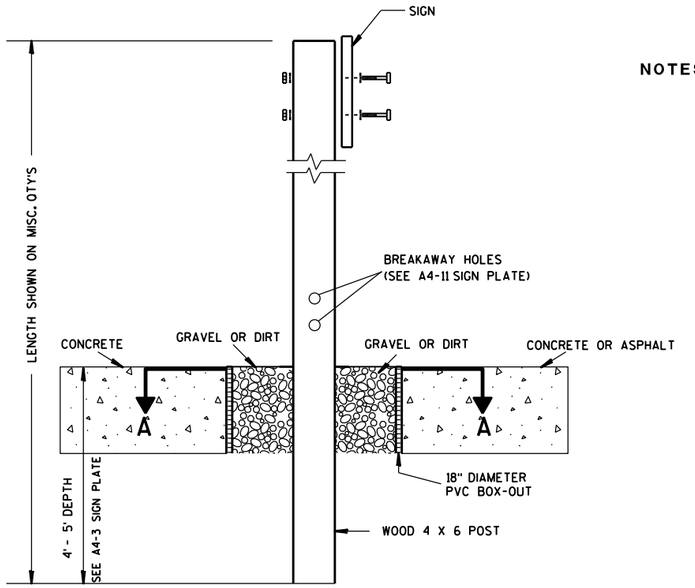
TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-3.23

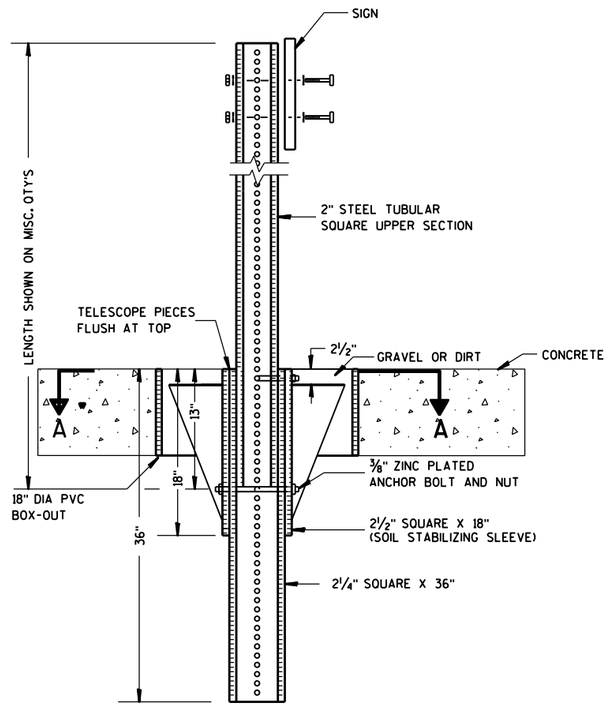
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 20	E



ELEVATION VIEW

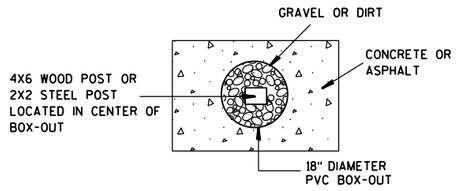
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

NOTES:
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

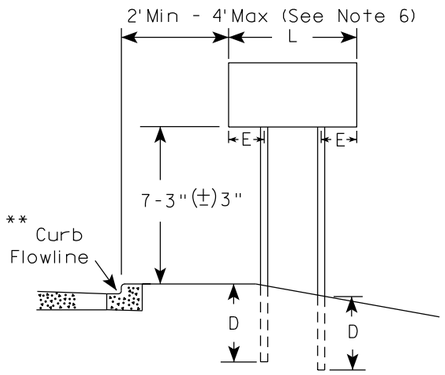
WISCONSIN DEPT OF TRANSPORTATION

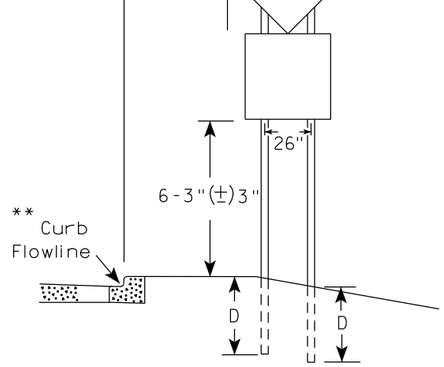
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

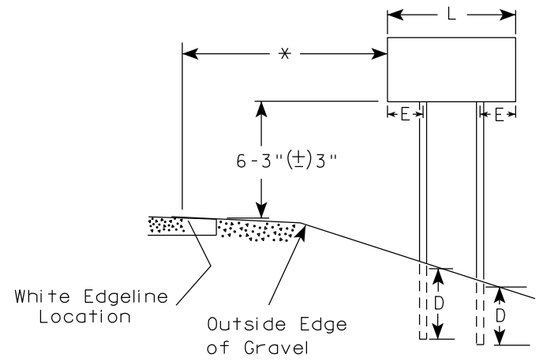
URBAN AREA

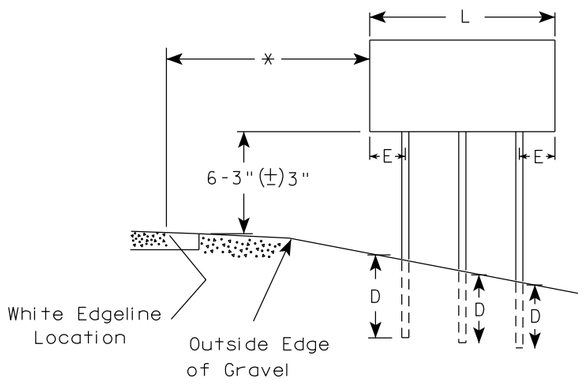


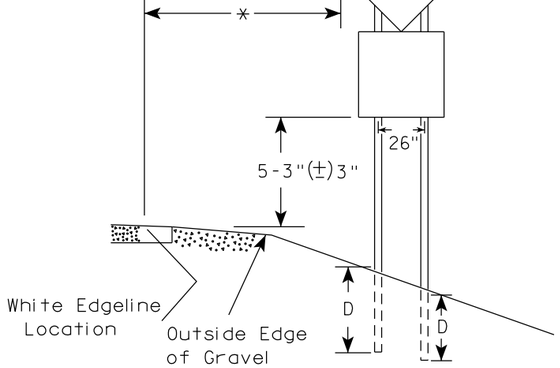


48" DIAMOND WARNING SIGN

RURAL AREA (See Note 3)







48" DIAMOND WARNING SIGN

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)		
	L	E
***	Greater than 48" Less than 60"	12"
	60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) 3" or 6'-3" (±) 3" depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±) 3".

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

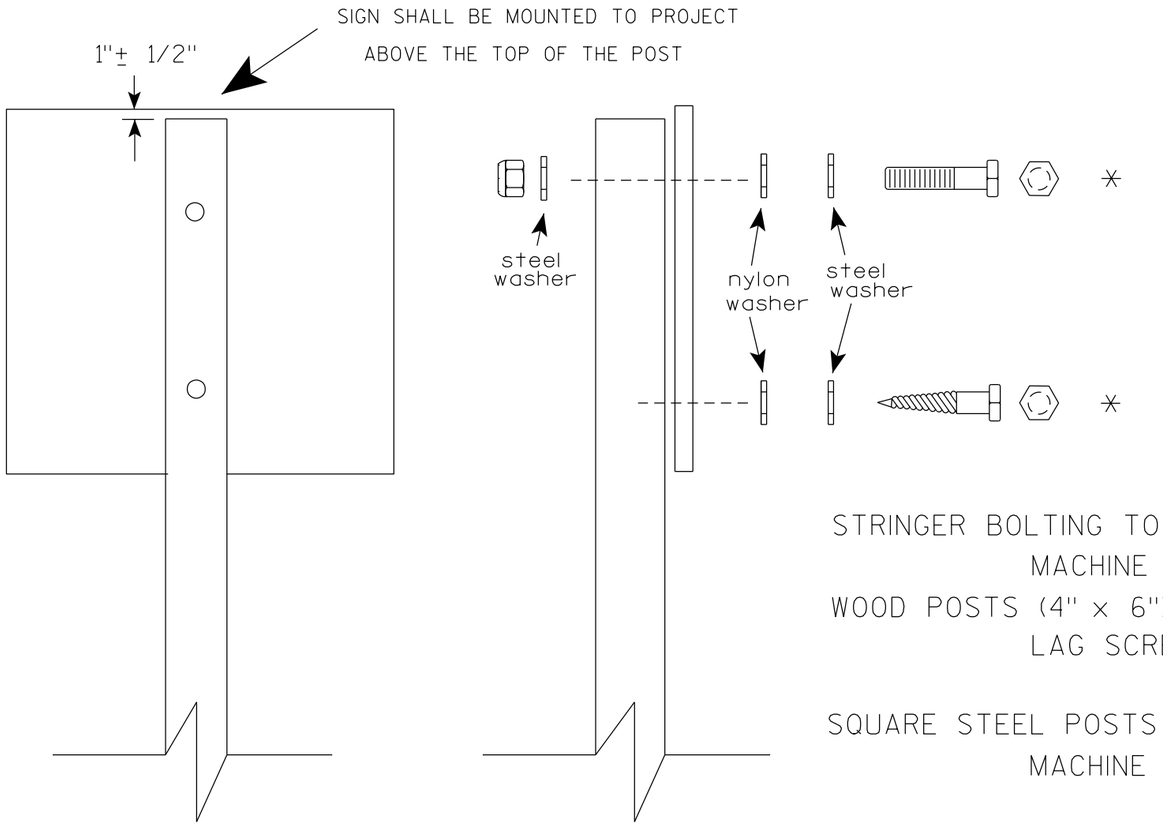
TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-4.16

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 22	E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3

b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

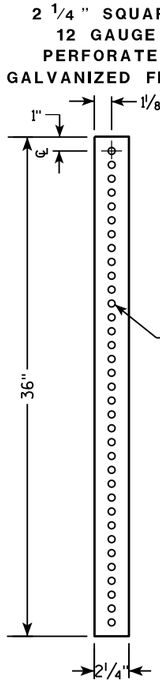
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

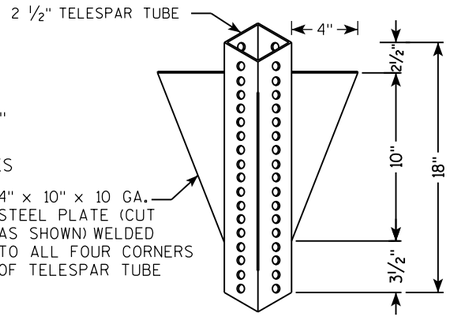
DATE 4/1/2020 PLATE NO. A4-8.9

TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM

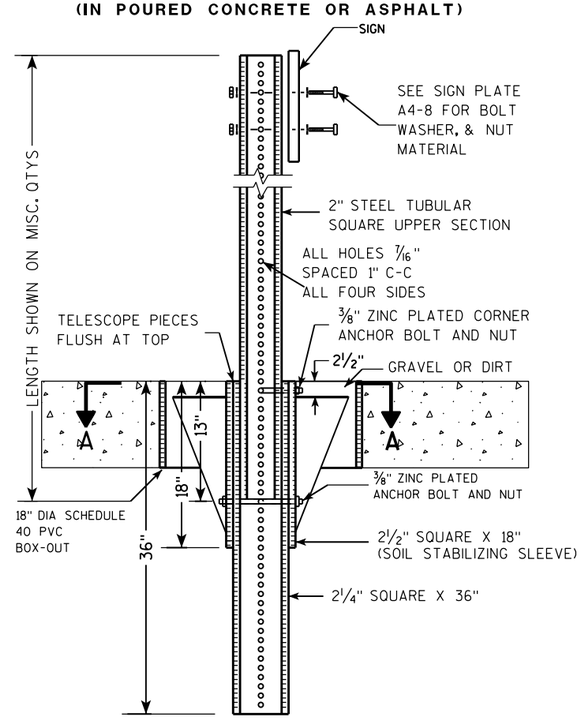
2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



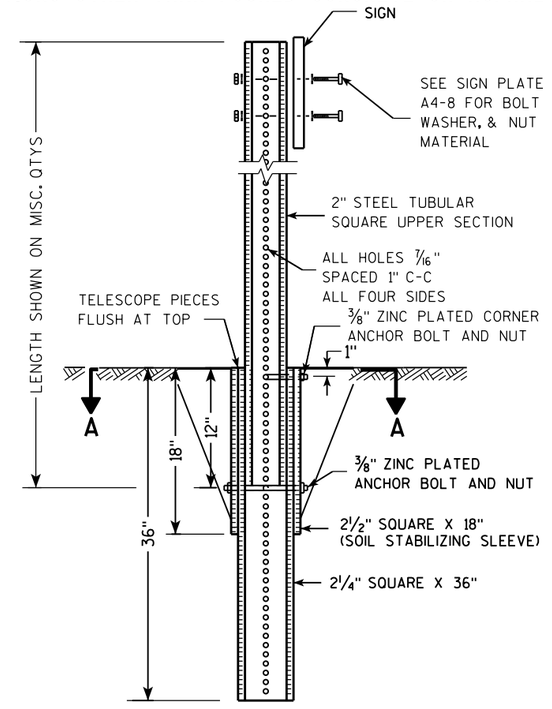
2 1/2 " SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH

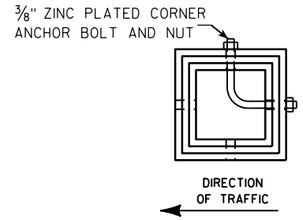


DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)





SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

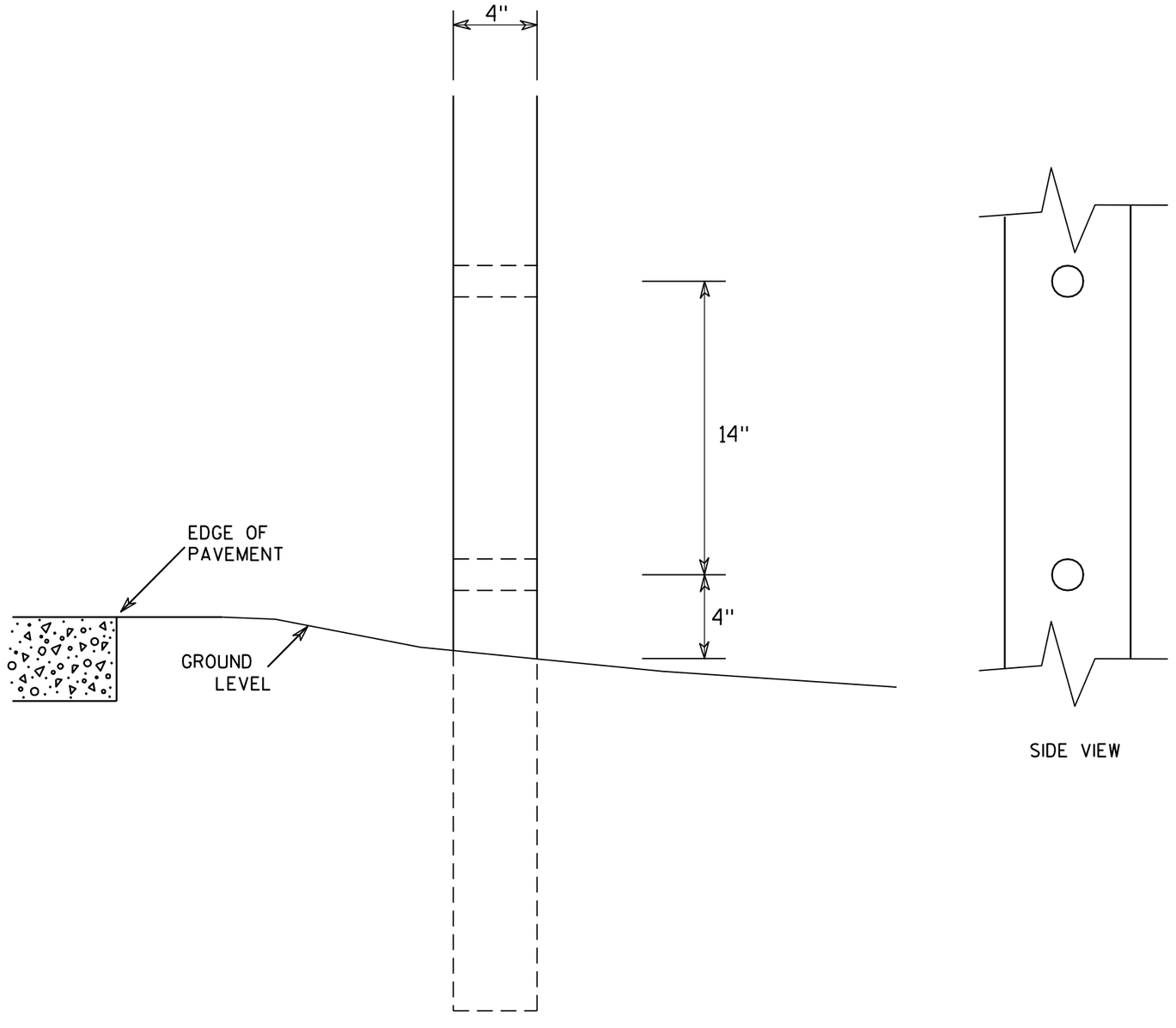
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/05/15 PLATE A4-9.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE : 05-FEB-2015 17:09 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

7

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

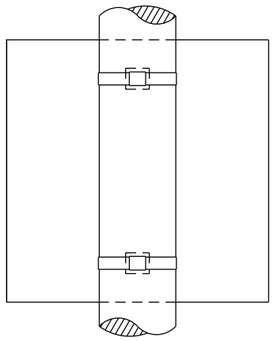
APPROVED Chester J Spang for State Traffic Engineer

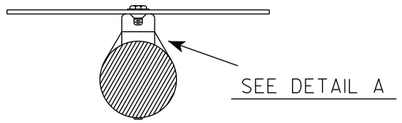
DATE 3/27/97 PLATE NO. A4-11.2

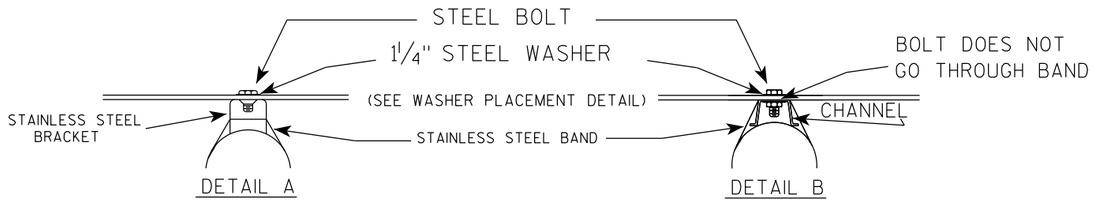
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 25	E

BANDING

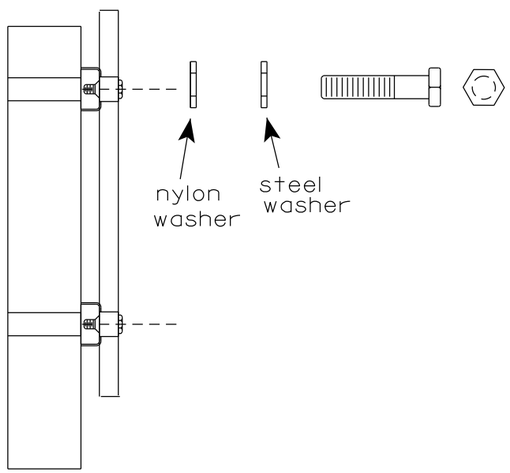
SINGLE SIGN







WASHER PLACEMENT

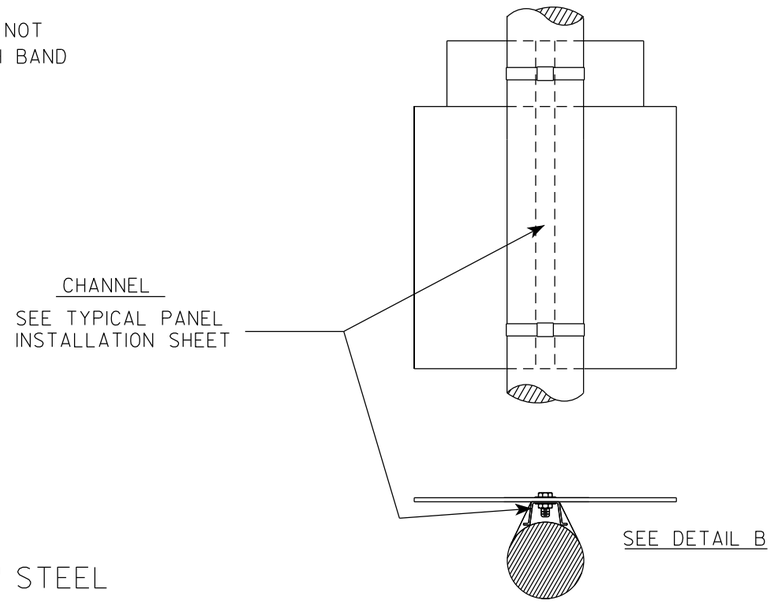


WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be ¾" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

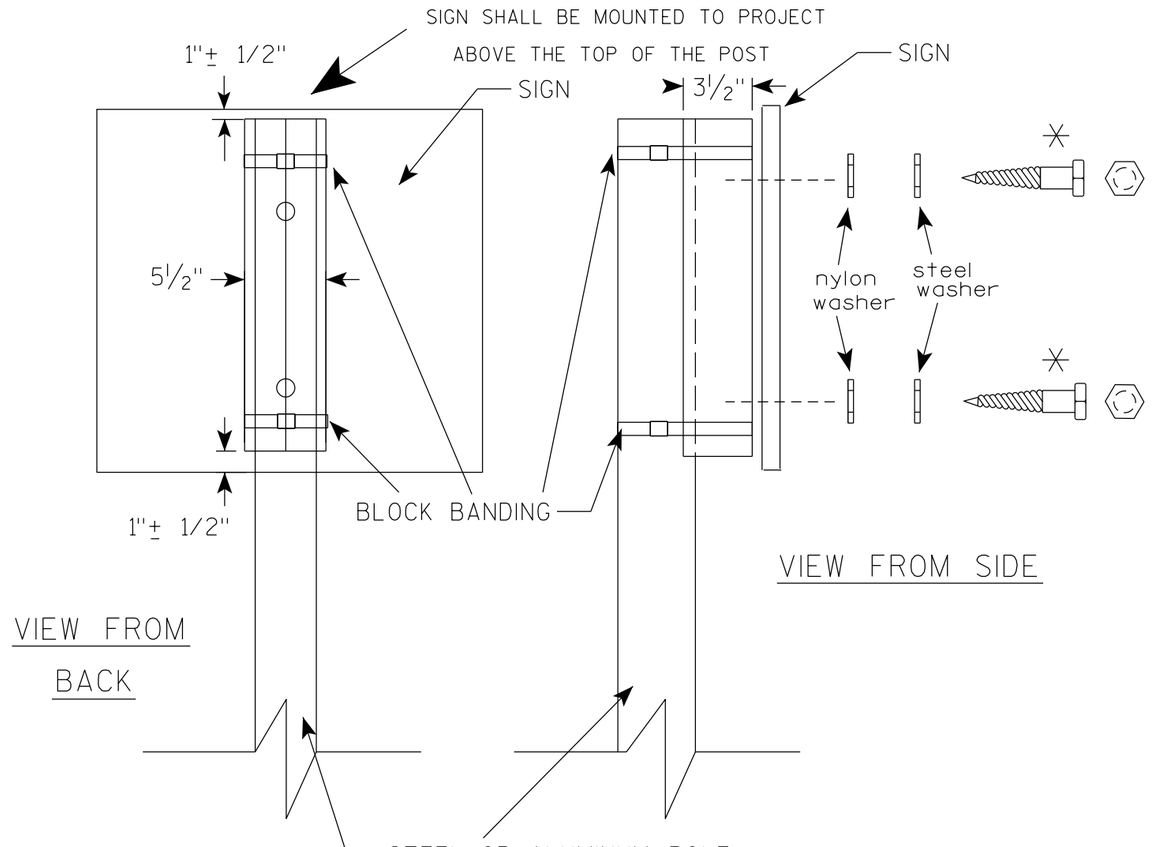
DATE 6/10/19 PLATE NO. A5-9.4

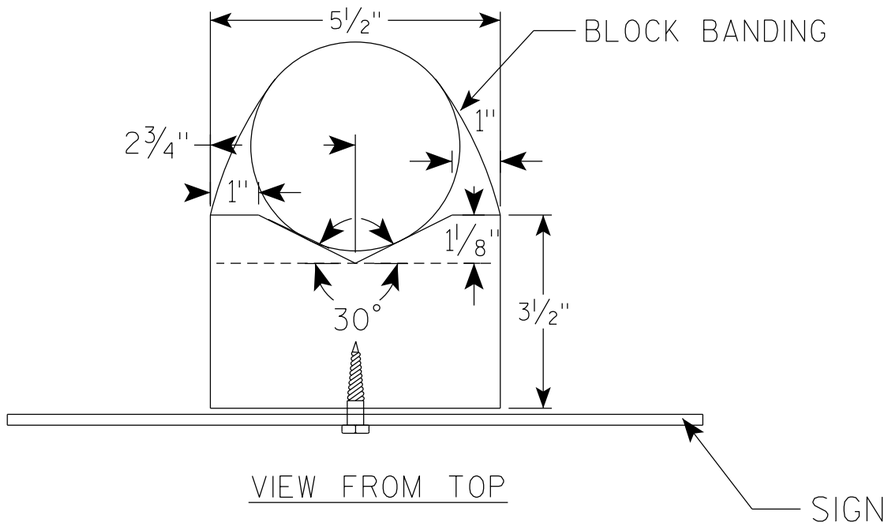
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 26	E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A59.dgn PLOT DATE : 10-JUN 2019 4:10 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : $$.....plotscale.....$$ WISDOT/CADDS SHEET 42

7

7





GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WisDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, ¾" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORNALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✳ LAG BOLTS SHALL BE ⅜" X 2½"

BLOCK BANDING DETAIL (V-BLOCK OPTION)

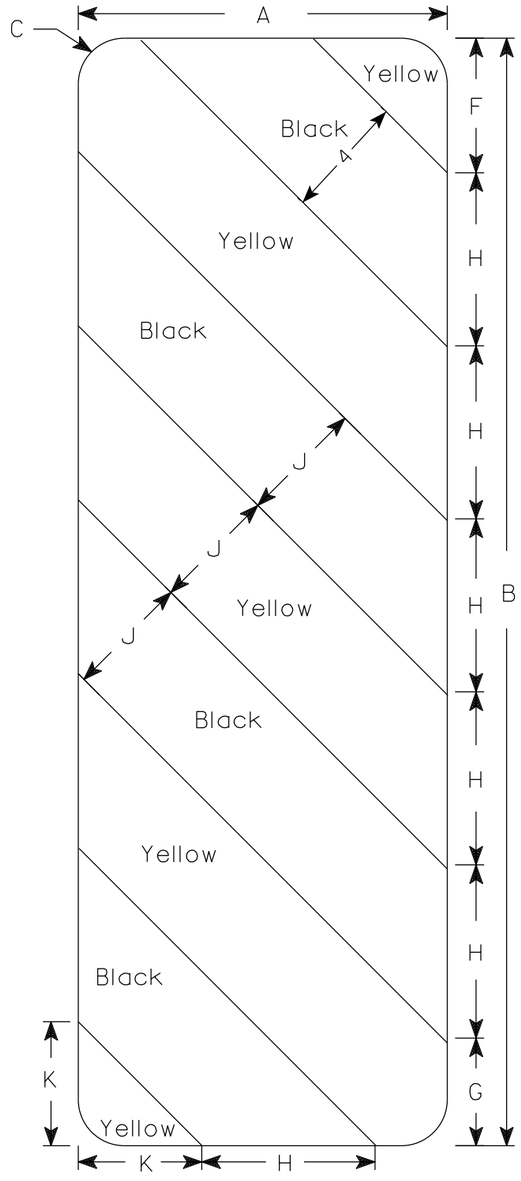
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

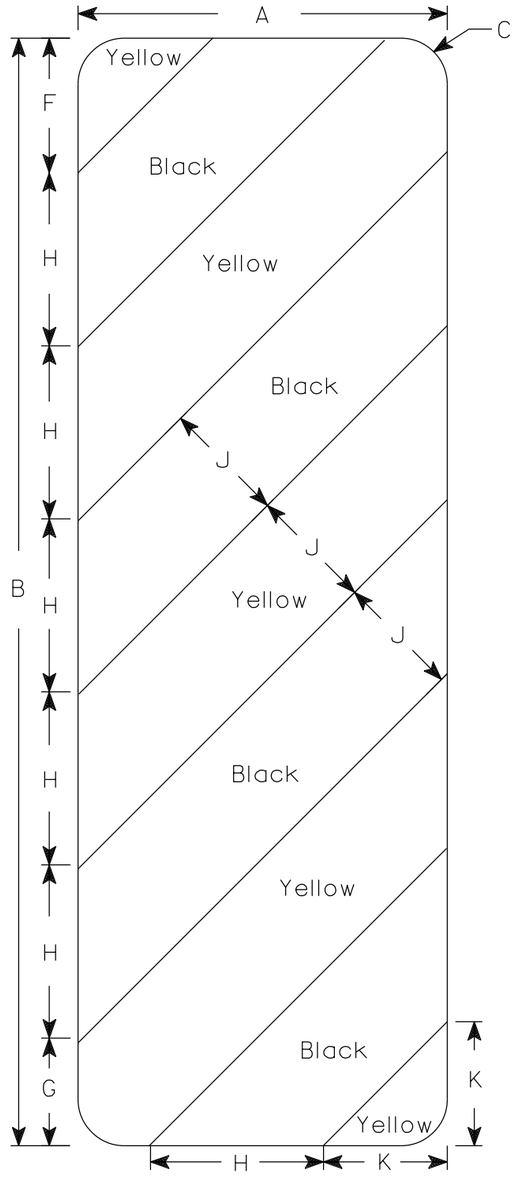
DATE 4/19/2022 PLATE NO. A5-10.3

PROJECT NO:

SHEET NO: 27 E



W5-52L



W5-52R

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Yellow
   Message - Black
3. Alternate colors of stripes as shown.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	36	1 1/2			4 3/8	3 1/2	5 5/8	45°	4	4																3.0
2M	12	36	1 1/2			4 3/8	3 1/2	5 5/8	45°	4	4																3.0
3	18	54	1 1/2			6	5 1/2	8 1/2	45°	6	6 9/16																6.75
4																											
5																											

STANDARD SIGN

W5-52L & W5-52R

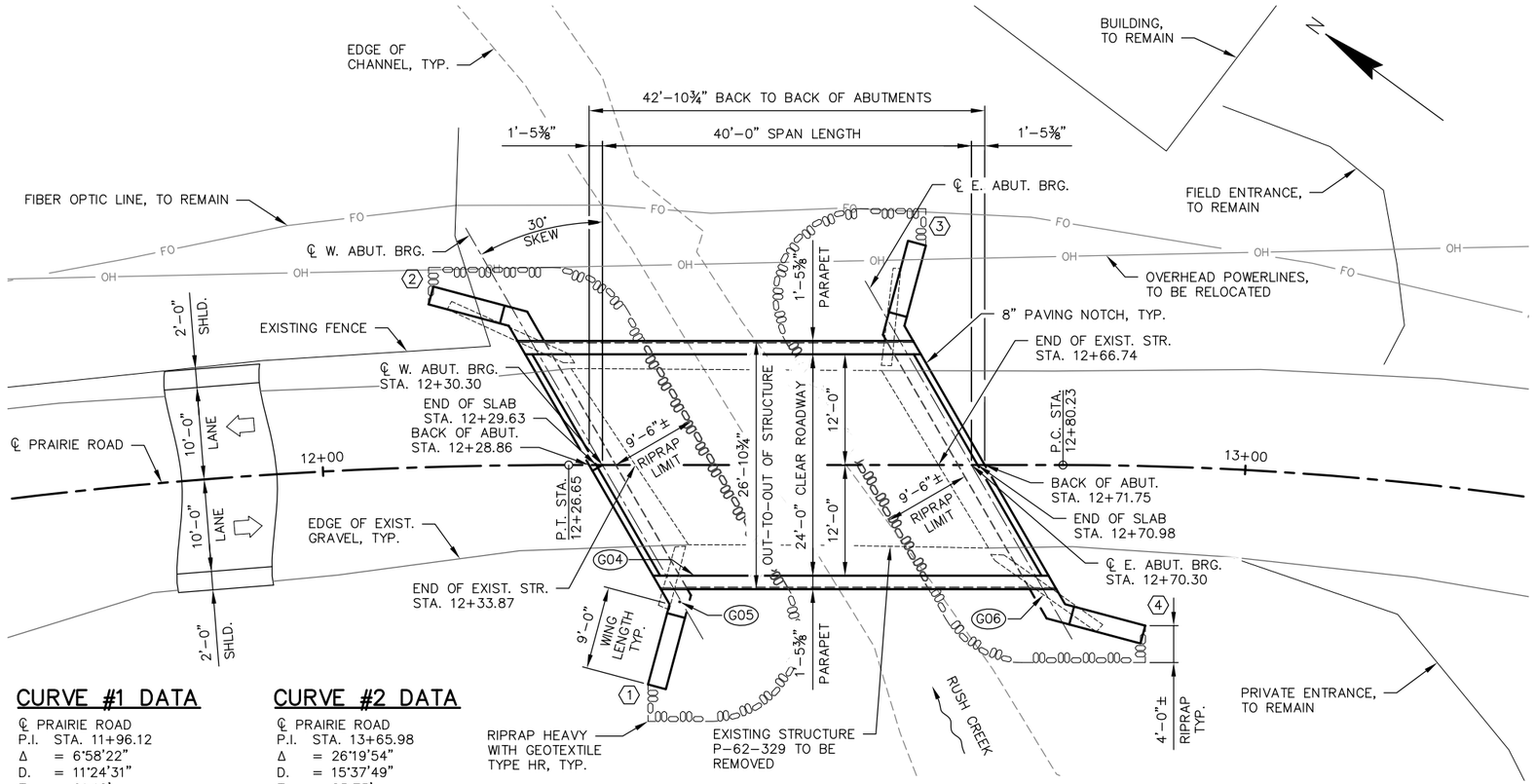
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 3/4/2024 PLATE NO. W5-52.10

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate_W552.dgn PLOT DATE : 4-MARCH 2024 11:57 PLOT BY : dotc4c PLOT NAME : PLOT SCALE : $$.....plotscale.....$$ WISDOT/CADDS SHEET 42



CURVE #1 DATA

℄ PRAIRIE ROAD
P.I. STA. 11+96.12
Δ = 6°58'22"
D. = 11°24'31"
T. = 61.12'
R. = 502.21'
L. = 61.12'
P.C. STA. 11+65.53
P.T. STA. 12+26.65

CURVE #2 DATA

℄ PRAIRIE ROAD
P.I. STA. 13+65.98
Δ = 26°19'54"
D. = 15°37'49"
T. = 85.75'
R. = 366.57'
L. = 168.46'
P.C. STA. 12+80.23
P.T. STA. 14+48.70

PLAN B-62-272

(SINGLE SPAN CONCRETE FLAT SLAB BRIDGE)

NOTES

EXCAVATION AS INDICATED IN THE HATCH AREAS, TO BE INCLUDED IN THE BID ITEM "EXCAVATION FOR STRUCTURES BRIDGES B-62-272".

(G01) BACKFILL PAY LIMITS. BACKFILL BEYOND BACKFILL PAY LIMITS SHALL BE INCLUDED WITH BID ITEM "EXCAVATION FOR STRUCTURES BRIDGES B-62-272". LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.

(G02) "GEOTEXTILE TYPE DF SCHEDULE A" LIMITS. EXTEND 2'-0" ABOVE BOTTOM OF ABUTMENT FOR THE ENTIRE ABUTMENT BODY LENGTH.

(G03) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN AS DETAILED IN "WINGS 1 & 3 DETAILS" SHEET.

(G04) NAME PLATE REQUIRED NEAR WING 1. FOR LOCATION SEE "PARAPET & SUPERSTRUCTURE REINFORCEMENT" SHEET.

(G05) BENCHMARK CAP (WHEN SUPPLIED) NEAR WING 1. FOR LOCATION SEE "ABUTMENTS" SHEET.

(G06) REMOVE ALL PORTIONS OF EXISTING STRUCTURE IN CONFLICT WITH NEW ABUTMENT CONCRETE AND PILES. TYPICAL NEAR ALL 4 WING LOCATIONS. ALL REMOVAL TO BE INCLUDED WITH BID ITEM "REMOVING STRUCTURE OVER WATERWAY REMOVE DEBRIS P-62-329."

(G07) THE WATERWAY AT THIS LOCATION HAS A LIMITED AMOUNT OF BASE FLOW AND FLOWS PRIMARILY WITH RAIN EVENTS.

⬡ INDICATES WING NUMBER

LIST OF DRAWINGS

STATE PROJECT NUMBER

5405-00-70

DESIGN DATA

LIVE LOAD:

DESIGN LOADING — HL-93
INVENTORY RATING FACTOR — RF=1.16
OPERATING RATING FACTOR — RF=1.51
WISCONSIN STANDARD PERMIT VEHICLE RATING (WIS.-SPV): — 250 KIPS

STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 POUNDS PER SQUARE FOOT.

MATERIAL PROPERTIES:

CONCRETE MASONRY, SLAB — $f'c$ = 4,000 P.S.I.
ALL OTHER — $f'c$ = 3,500 P.S.I.
HIGH-STRENGTH BAR STEEL REINFORCEMENT — fy = 60,000 P.S.I.

FOUNDATION DATA:

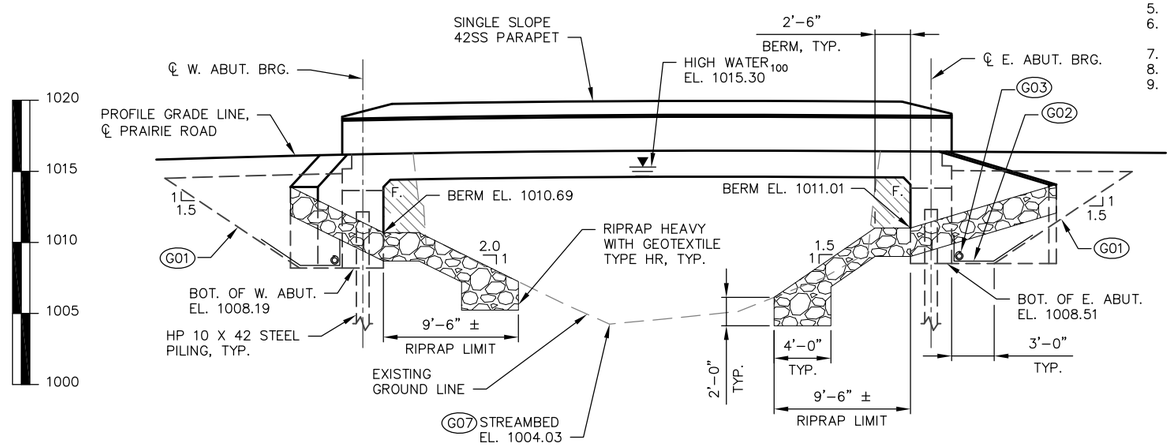
ABUTMENTS TO BE SUPPORTED ON HP 10 X 42 STEEL PILING SEATED IN PREBORED HOLES CORED 3 FEET MINIMUM INTO ROCK. PILE DRIVING IS NOT REQUIRED. THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS 140 TONS MULTIPLIED BY A RESISTANCE FACTOR OF 0.5. ESTIMATED 10 FT PILE LENGTHS AT W. ABUT. AND 20 FT PILE LENGTHS AT E. ABUT.

HYDRAULIC DATA:

Q_{100} — 1,600 C.F.S.
DRAINAGE AREA — 6.22 SQ. MI.
BRIDGE WATER AREA — 225 SQ. FT.
BRIDGE VELOCITY — 7.11 F.P.S.
HIGH WATER$_{100}$ EL. — 1015.30 FT.
ROADWAY OVERTOPPING — N/A
SCOUR CRITICAL CODE — 5
Q_2 — 317 C.F.S.
Q_2 ELEVATION — 1010.21 FT.
Q_2 VELOCITY — 3.36 F.P.S.

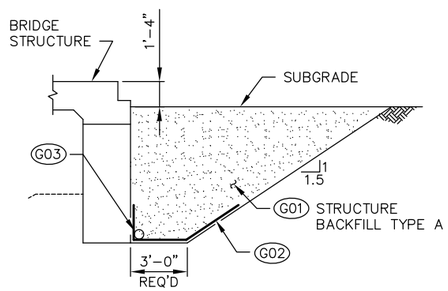
TRAFFIC DATA:

PRAIRIE ROAD
A.A.D.T. (2026) — 94
A.A.D.T. (2046) — 99
DESIGN SPEED — 30 M.P.H.



ELEVATION

(NORMAL TO RUSH CREEK, LOOKING NORTH)



ABUTMENT BACKFILL DETAIL

(TYPICAL AT BOTH ABUTMENTS)

BENCH MARKS

NO.	STATION/OFFSET	DESCRIPTION	ELEVATION
BM #1	10+58.45, 32.13' LT.	RAILROAD SPIKE, POWER POLE	1008.62
BM #2	13+57.47, 33.10' LT.	RAILROAD SPIKE, POWER POLE	1017.08

HORIZONTAL DATUM AND ADJUSTMENT: NAD 83 (2011)
VERTICAL DATUM AND ADJUSTMENT: NAVD 88 (2012)
COORDINATE REFERENCE SYSTEM: WISCRS, VERNON CO.

WISCONSIN
ANDREW C. KNUTSON
E-34662
SPRING GREEN, WI
8-22-25
PROFESSIONAL ENGINEER

BRIDGE OFFICE CONTACT
AARON BONK, P.E.
(608) 261-0261

CONSULTANT CONTACT
ANDY KNUTSON, P.E., S.E.
(608) 588-7866

NO.	DATE	REVISION	BY

WESTBROOK Associated Engineers, Inc.
619 EAST HOXIE STREET
P.O. BOX 429
SPRING GREEN, WI 53588
PHONE (608) 588-7866
FAX (608) 588-7954

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

ACCEPTED JLR 12/02/25
CHIEF STRUCTURES DESIGN ENGINEER — DATE

STRUCTURE B-62-272

PRAIRIE ROAD OVER RUSH CREEK

COUNTY: VERNON
TOWN/~~CITY/VILLAGE~~: STERLING
DESIGN SPEC.: AASHTO LRFD DESIGN SPEC.

DESIGNED BY	DESIGN CK'D.	DRAWN BY	PLANS CK'D.
JDO	CDS	JDO	ACK

GENERAL PLAN

SHEET 1 OF 9

29

I.D. 5405-00-70

PLOT DATE: Aug 22, 2025

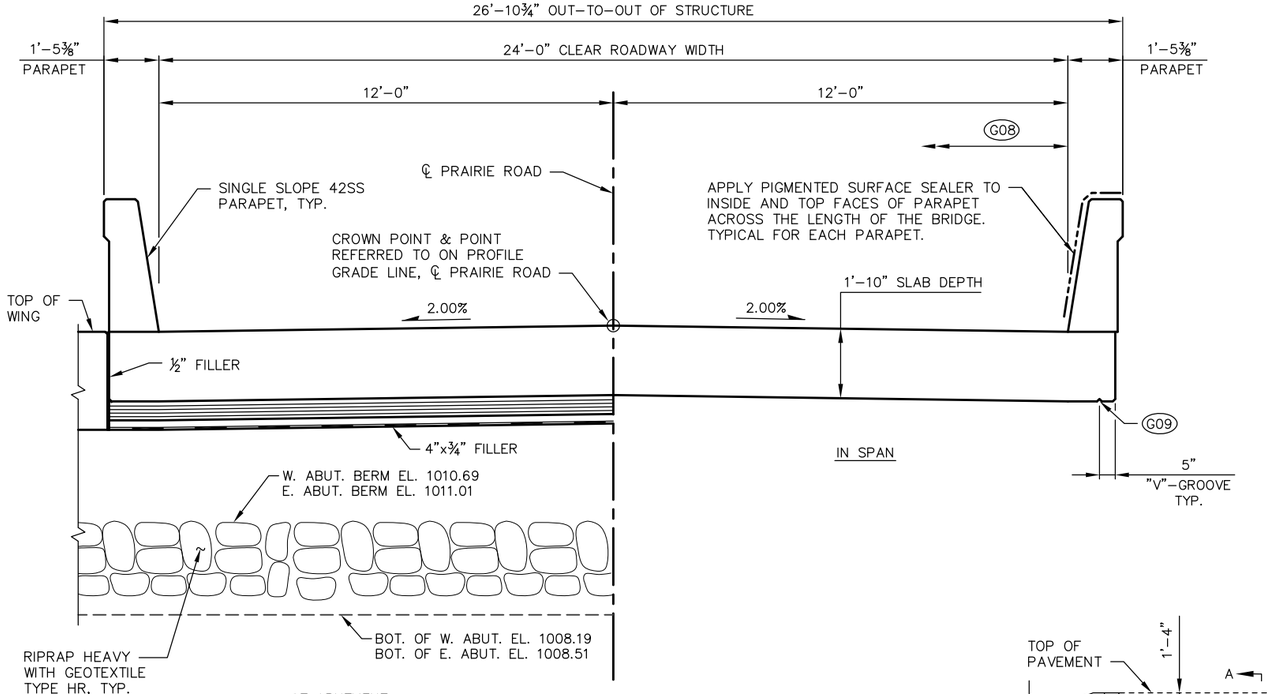
FILE: B620272_01_gen.dwg
PLOT SCALE:

8

8

STATE PROJECT NUMBER

5405-00-70



AT ABUTMENT
(PILING NOT SHOWN FOR CLARITY)

CROSS SECTION THRU ROADWAY

(LOOKING EAST)

NOTES

(G08) COAT WITH "PROTECTIVE SURFACE TREATMENT" AS PER THE STANDARD SPECIFICATIONS. PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO THE TOP SURFACE OF SLAB BETWEEN THE PARAPETS. ALSO APPLY PROTECTIVE SURFACE TREATMENT TO THE VERTICAL AND HORIZONTAL SURFACES OF THE PAVING NOTCHES.

(G09) ¾" V-GROOVE REQ'D. EXTEND V-GROOVE TO 6" FROM FRONT FACE OF ABUTMENT.

TOTAL ESTIMATED QUANTITIES

ITEM NO.	BID ITEMS	UNIT	W. ABUT.	E. ABUT.	SUPER.	TOTALS
203.0250	REMOVING STRUCTURE OVER WATERWAY REMOVE DEBRIS P-62-329	EACH	---	---	---	1
206.1001	EXCAVATION FOR STRUCTURES BRIDGES B-62-272	EACH	---	---	---	1
210.1500	BACKFILL STRUCTURE TYPE A	TON	150	150	---	300
502.0100	CONCRETE MASONRY BRIDGES	CY	28.4	28.1	92.4	149
502.3200	PROTECTIVE SURFACE TREATMENT	SY	---	---	121	121
502.3210	PIGMENTED SURFACE SEALER	SY	---	---	42	42
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	2360	2240	---	4600
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	1435	1435	19370	22240
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	6	6	---	12
550.0020	PRE-BORING ROCK OR CONSOLIDATED MATERIALS	LF	42	105	---	147
550.1100	PILING STEEL HP 10-INCH X 42 LB	LF	70	140	---	210
606.0300	RIPRAP HEAVY	CY	60	70	---	130
612.0406	PIPE UNDERDRAIN WRAPPED 6-INCH	LF	80	80	---	160
645.0111	GEOTEXTILE TYPE DF SCHEDULE A	SY	34	34	---	68
645.0120	GEOTEXTILE TYPE HR	SY	100	100	---	200
(NON-BID ITEM)	FILLER	SIZE				½" & ¾"

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BEVEL EXPOSED EDGES OF CONCRETE ¾" UNLESS OTHERWISE NOTED.

THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH RIPRAP HEAVY AND GEOTEXTILE TYPE HR TO THE EXTENT SHOWN ON THE "GENERAL PLAN" SHEET AND THE ABUTMENT SHEETS.

THE BACKFILL QUANTITIES ARE BASED ON THE PAY LIMITS SHOWN ON THE PLANS AND MAY NOT REFLECT ACTUAL PLACED QUANTITIES. "BACKFILL STRUCTURE TYPE A" REQUIRED DIRECTLY BEHIND ABUTMENTS AND ABUTMENT WING FOR 3 FEET. BACKFILL PLACED BEYOND PAY LIMITS OR EXCEEDING PLAN QUANTITIES SHALL BE INCLUDED WITH "EXCAVATION FOR STRUCTURES BRIDGES B-62-272".

EXCAVATION BELOW THE ABUTMENT AND ABUTMENT BEDDING MATERIALS REQUIRES ENGINEER APPROVAL. GEOTEXTILE SHALL BE SET AT THE BOTTOM OF EXCAVATION AND EXTEND 2'-0" ABOVE BOTTOM OF ABUTMENT.

THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES BRIDGES B-62-272" SHALL BE THE EXISTING GROUND LINE.

AT ABUTMENTS, CONCRETE POURED UNDER WATER WILL BE ALLOWED AND SHALL BE DONE IN ACCORDANCE WITH SECTION 502.3.5.3 OF THE STANDARD SPECIFICATIONS.

SLAB FALSEWORK SHALL BE SUPPORTED ON PILES OR THE SUBSTRUCTURE, UNLESS AN ALTERNATE METHOD IS APPROVED BY THE ENGINEER.

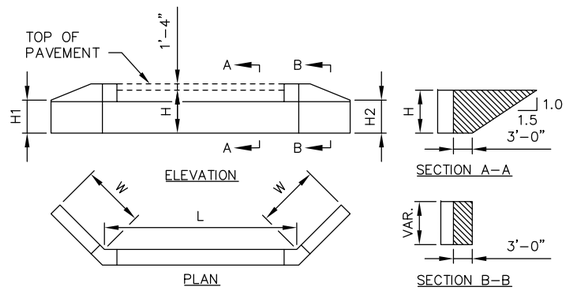
AT THE BACKFACE OF ABUTMENT ALL VOLUME WHICH CANNOT BE PLACED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL TYPE A.

DO NOT PLACE FILL ABOVE 3'-0" FROM BOTTOM OF ABUTMENT UNTIL SUPERSTRUCTURE IS IN PLACE.

SHALLOW BEDROCK (LESS THAN 10-FT BELOW ABUTMENT) WAS ENCOUNTERED IN THE BORING FOR THE WEST AND EAST ABUTMENT. A MINIMUM OF 3-FEET OF PRE-BORE AT THE ABUTMENT INTO SUITABLE BEDROCK IS REQUIRED IF THE MINIMUM OF 10-FEET OF PILE PENETRATION INTO NATURAL GROUND CANNOT BE ACHIEVED. THE CONTRACTOR AND THE CONSTRUCTION ENGINEER SHOULD ANTICIPATE VARIABLE PILE PENETRATION.

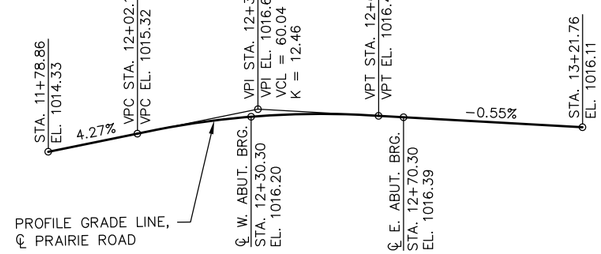
PILES PLACED IN PREBORED HOLES CORED INTO ROCK DO NOT REQUIRE DRIVING. PILES SHALL BE "FIRMLY SEATED" ON ROCK AFTER PLACEMENT IN PREBORED HOLES.

THE EXISTING STRUCTURE (P-62-329) IS A SINGLE SPAN STEEL GIRDER CONCRETE DECK BRIDGE WITH AN OVERALL LENGTH OF 35-FT AND A DECK WIDTH OF 20-FT AND IS TO BE REMOVED PER BID ITEM "REMOVING STRUCTURE OVER WATERWAY REMOVE DEBRIS P-62-329".

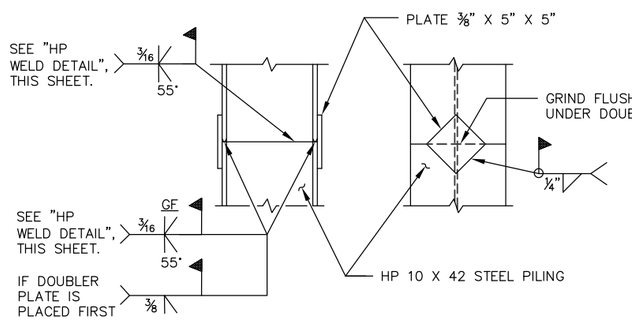


ABUTMENT BACKFILL DIAGRAM

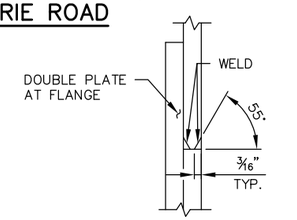
L = ABUTMENT BODY LENGTH AT BACKFACE (FT)
H = AVERAGE ABUTMENT FILL HEIGHT (FT)
H1 = WING 1 HEIGHT AT TIP (FT)
H2 = WING 2 HEIGHT AT TIP (FT)
W = WING LENGTH (FT)
EF = EXPANSION FACTOR (1.20 FOR CY BID ITEMS AND 1.00 FOR TON BID ITEMS)
V_{CF} = (L)(3.0')(H) + (L)(0.5)(1.5H)(H) + (3')(0.5)(H1+H2+H+H)(W)
V_{CY} = V_{CF}(EF)/27
V_{TON} = V_{CY}(2.0)



PROFILE GRADE LINE, ℄ PRAIRIE ROAD



PILE SPLICE DETAILS



HP WELD DETAIL

(FLANGE SHOWN, WEB SIMILAR)

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-62-272

DRAWN BY JDO | PLANS CK'D ACK

CROSS SECTION, GENERAL NOTES & QUANTITIES

SHEET 2 OF 9

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I.D. 5405-00-70

PLOT DATE: Aug 12, 2025

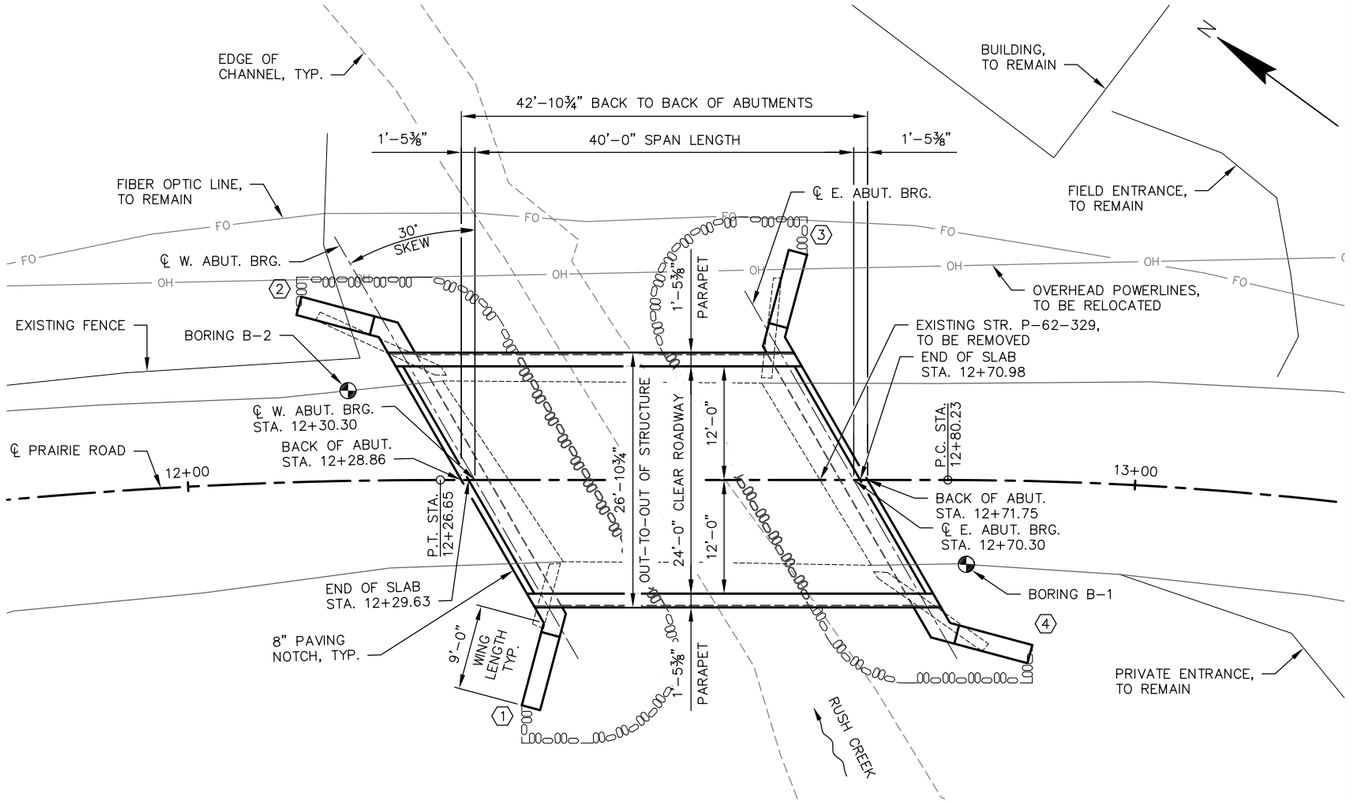
FILE: B620272_02_qty.dwg
PLOT SCALE:

B-62-272 BORINGS

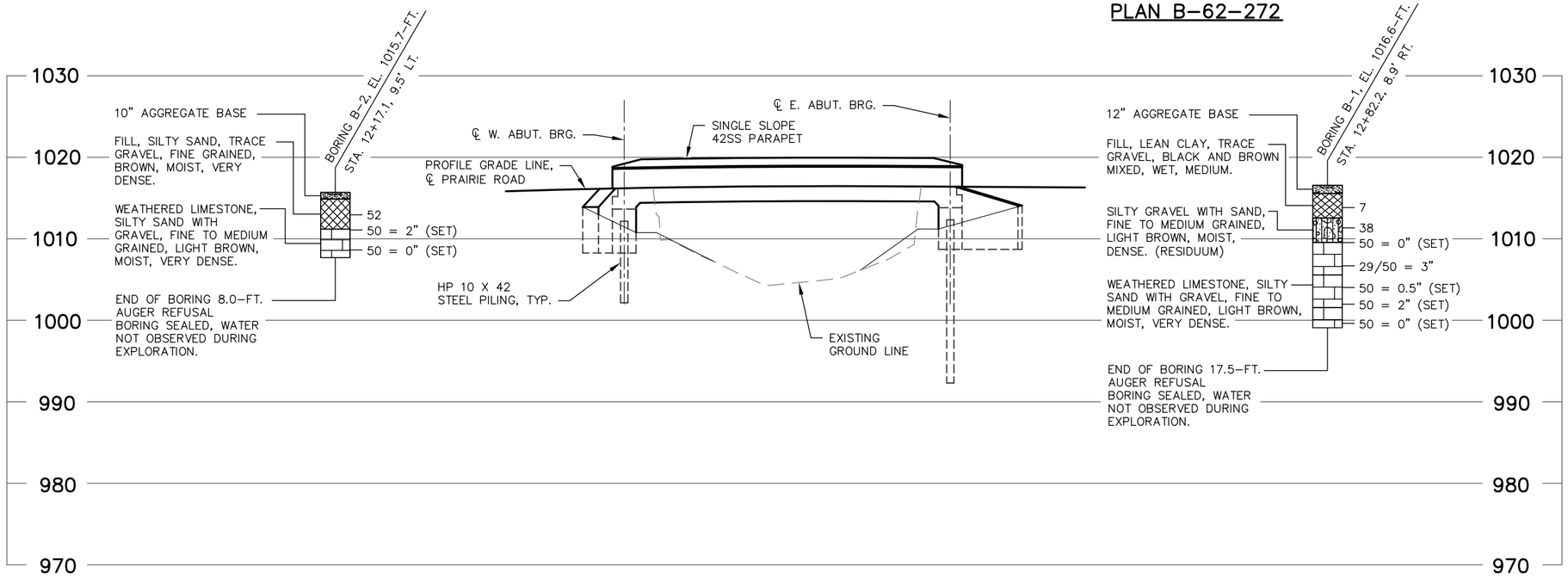
BORING #	DATE COMPLETED	NORTHING (Y)	EASTING (X)
BORING B-1	12/28/2022	106733.6	672999.3
BORING B-2	12/28/2022	106797.0	672975.4
BORINGS COMPLETED BY: CHOSEN VALLEY TESTING, INC.			
SUBSURFACE INVESTIGATION REPORT: CHOSEN VALLEY TESTING, INC.			
ALL COORDINATES REFERENCED TO WISCRS, VERNON COUNTY			

NOTE

⬡ INDICATES WING NUMBER



PLAN B-62-272



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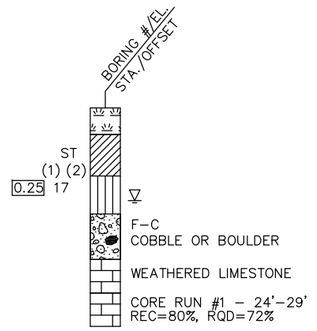
MATERIAL SYMBOLS

ASPHALT	TOPSOIL	PEAT
CONCRETE	FILL	GRAVEL
SAND	CLAY	SILT
BOULDERS OR COBBLES	LIMESTONE	BEDROCK (UNKNOWN)
SHALE	SANDSTONE	IGNEOUS/ META

LEGEND OF BORING



(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)

(2) UNLESS OTHERWISE SPECIFIED, THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION

- ▽ AT TIME OF DRILLING
- ▼ END OF DRILLING
- ▼ AFTER DRILLING

ABBREVIATIONS

F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.

NO.	DATE	REVISION	BY

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SUBSURFACE EXPLORATION

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PLOT SCALE:

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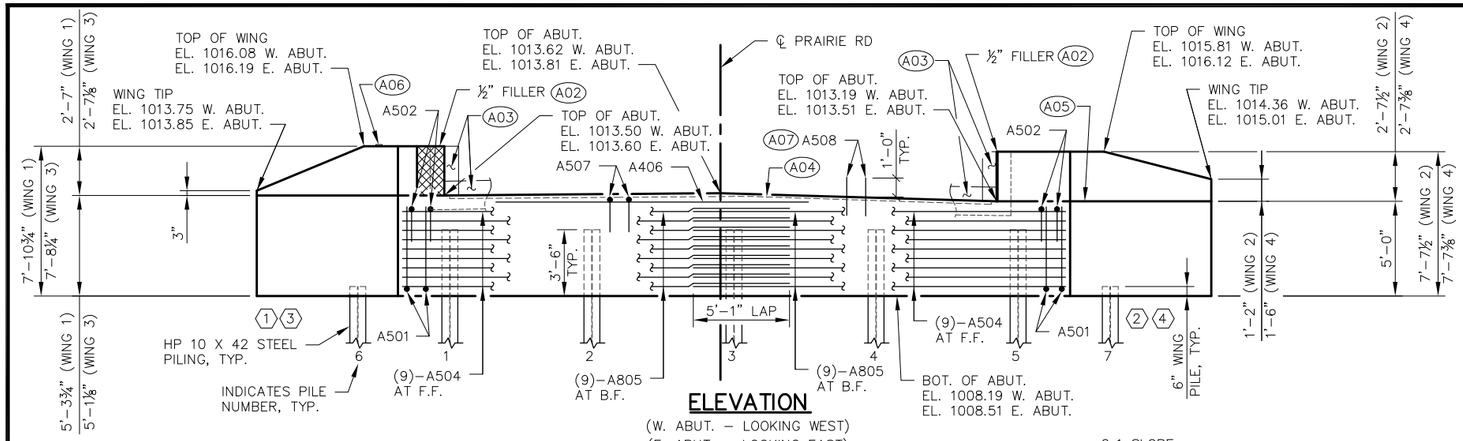
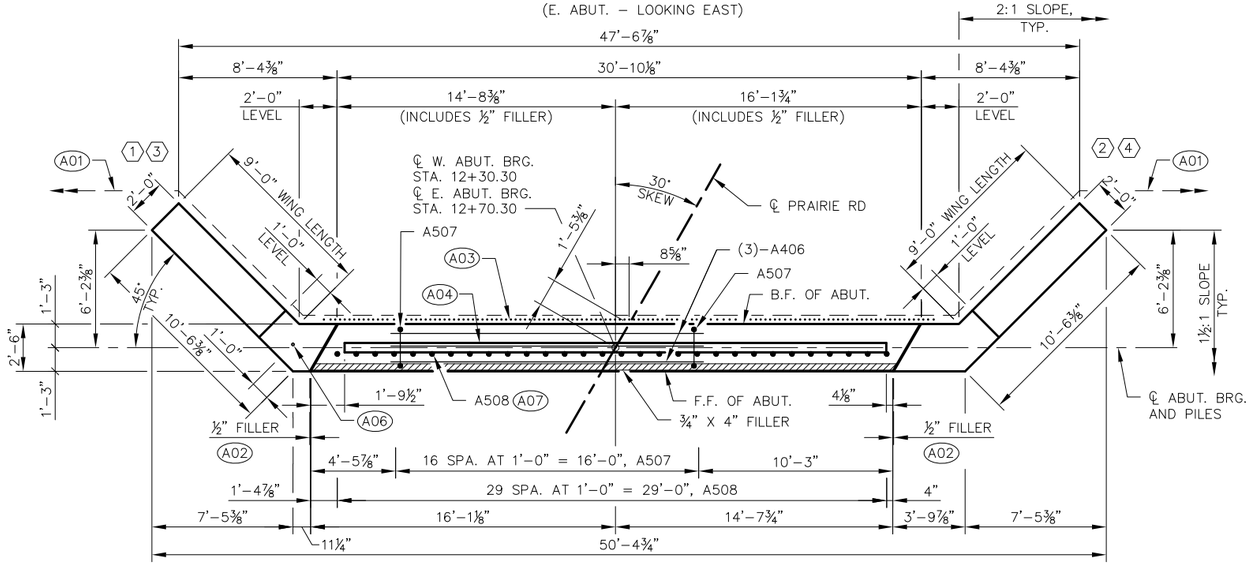
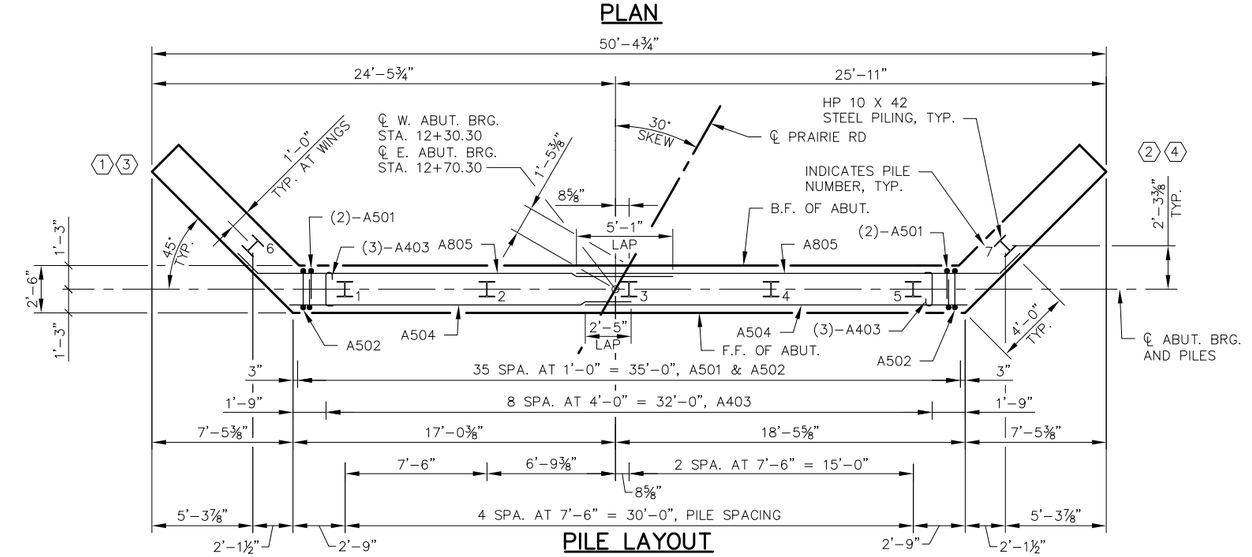


ELEVATION

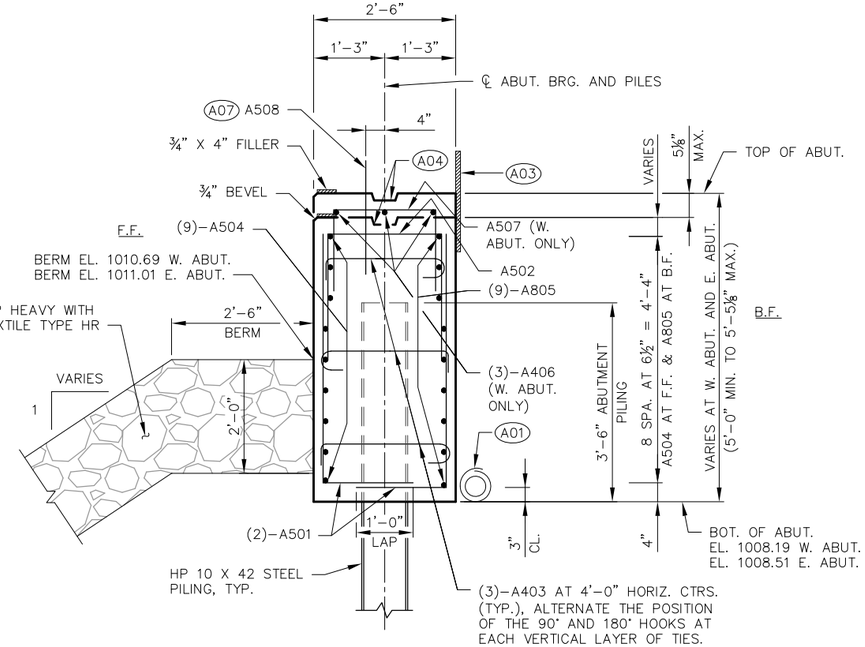
(W. ABUT. – LOOKING WEST)
(E. ABUT. – LOOKING EAST)



PLAN



PILE LAYOUT



TYPICAL SECTION THRU ABUTMENT

NOTES

DO NOT PLACE FILL ABOVE 3'-0" FROM THE BOTTOM OF THE ABUTMENT UNTIL SUPERSTRUCTURE IS IN PLACE.

WEST AND EAST ABUTMENTS TO BE SUPPORTED ON HP 10 X 42 STEEL PILING SEATED IN PREBORED HOLES CORED 3 FEET MINIMUM INTO ROCK. PILE DRIVING IS NOT REQUIRED. THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS 140 TONS MULTIPLIED BY A RESISTANCE FACTOR OF 0.5. ESTIMATED 10 FT PILE LENGTHS AT WEST ABUTMENT AND 20 FT PILE LENGTHS AT EAST ABUTMENT.

SEE "CROSS SECTION, GENERAL NOTES & QUANTITIES" SHEET FOR HP 10 X 42 STEEL PILING SPLICE DETAILS.

(A01) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN AS DETAILED ON "WINGS 1 & 3 DETAILS" SHEET. RODENT SHIELD SHALL BE INCLUDED WITH THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6-INCH".

(A02) SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF ½" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD ⅛" BELOW SURFACE OF CONCRETE). ½" FILLER TO EXTEND FROM BRIDGE SEAT TO TOP OF WING.

(A03) 18" RUBBERIZED MEMBRANE WATERPROOFING (R.M.W.), SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACK FACE.

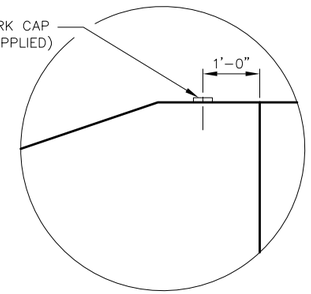
(A04) KEYED CONST. JT. FORMED BY BEVELED 2 X 6

(A05) OPTIONAL KEYED CONST. JT. FORMED BY BEVELED 2 X 6, TYP.

(A06) BENCHMARK CAP (WHEN SUPPLIED) AT WING 1 ONLY. SEE "BENCHMARK CAP DETAIL", THIS SHEET.

(A07) A508 BARS MAY BE PLACED AFTER CONCRETE HAS BEEN POURED BUT BEFORE INITIAL SET HAS TAKEN PLACE. EMBED 1'-0" INTO ABUTMENT BODY.

⬡ INDICATES WING NUMBER



BENCHMARK CAP DETAIL

F.F. – FRONT FACE
B.F. – BACK FACE

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
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ABUTMENTS

SHEET 4 OF 9

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FILE: B620272_04-06_abut.dwg
PLOT SCALE:

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STATE PROJECT NUMBER

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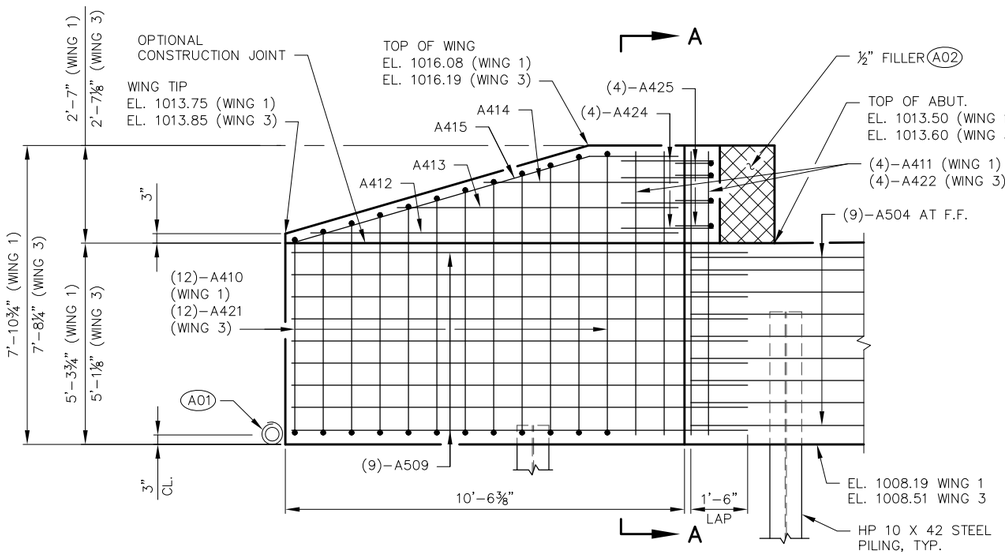
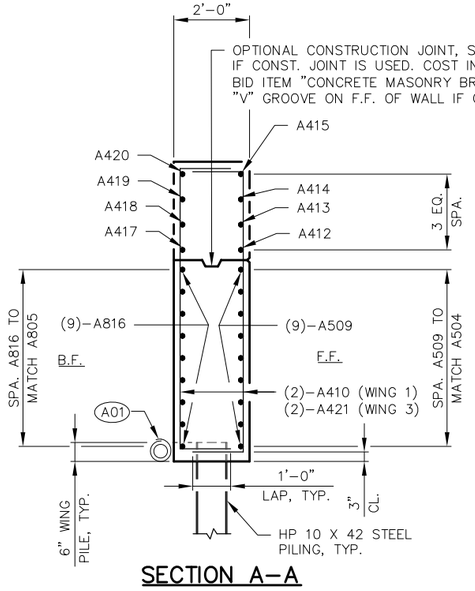


F.F. ELEVATION – WINGS 1 & 3



SECTION A–A

NOTES

DO NOT PLACE FILL ABOVE 3'–0" FROM THE BOTTOM OF THE ABUTMENT UNTIL SUPERSTRUCTURE IS IN PLACE.

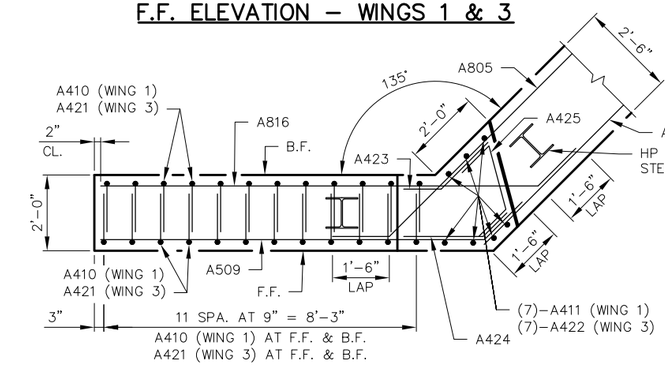
WEST AND EAST ABUTMENTS TO BE SUPPORTED ON HP 10 X 42 STEEL PILING SEATED IN PREBORED HOLES CORED 3 FEET MINIMUM INTO ROCK. PILE DRIVING IS NOT REQUIRED. THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS 140 TONS MULTIPLIED BY A RESISTANCE FACTOR OF 0.5. ESTIMATED 10 FT PILE LENGTHS AT WEST ABUTMENT AND 20 FT PILE LENGTHS AT EAST ABUTMENT.

SEE "CROSS SECTION, GENERAL NOTES & QUANTITIES" SHEET FOR HP 10 X 42 STEEL PILING SPLICE DETAILS.

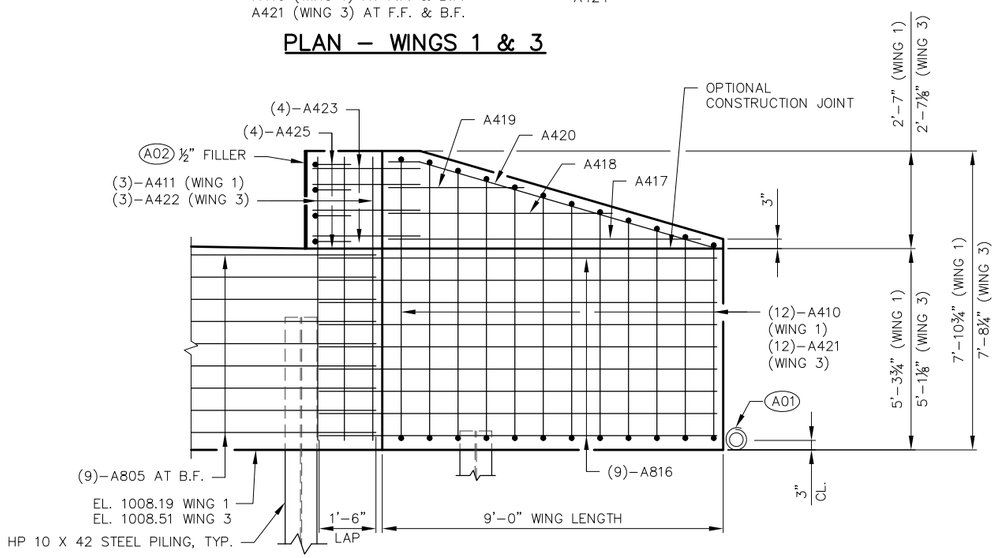
(A01) PIPE UNDERDRAIN WRAPPED (6–INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN AS DETAILED ON THIS SHEET. RODENT SHIELD SHALL BE INCLUDED WITH THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6–INCH".

(A02) SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF ½" FILLER WITH NON–STAINING GRAY NON–BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD ⅛" BELOW SURFACE OF CONCRETE.) ½" FILLER TO EXTEND FROM BRIDGE SEAT TO TOP OF WING.

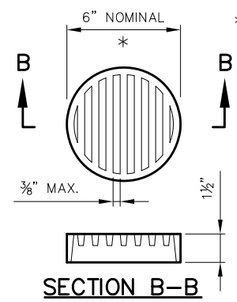
F.F. – FRONT FACE
B.F. – BACK FACE



PLAN – WINGS 1 & 3



B.F. ELEVATION – WINGS 1 & 3



SECTION B–B

* DIMENSIONS ARE APPROXIMATE. THE GRATE IS SIZED TO FIT INTO A PIPE COUPLING. ORIENT SHIELD SO SLOTS ARE VERTICAL.

THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALLY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE EXPOSED END OF THE PIPE UNDERDRAIN. THE SHIELD SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1–INCH STAINLESS STEEL SHEET METAL SCREWS.

THE RODENT SHIELD, PIPE COUPLING AND SCREWS SHALL BE INCLUDED WITH THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6–INCH".

RODENT SHIELD DETAIL

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STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-62-272

DRAWN BY JDO | PLANS CK'D ACK

WINGS 1 & 3 DETAILS

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PLOT SCALE:

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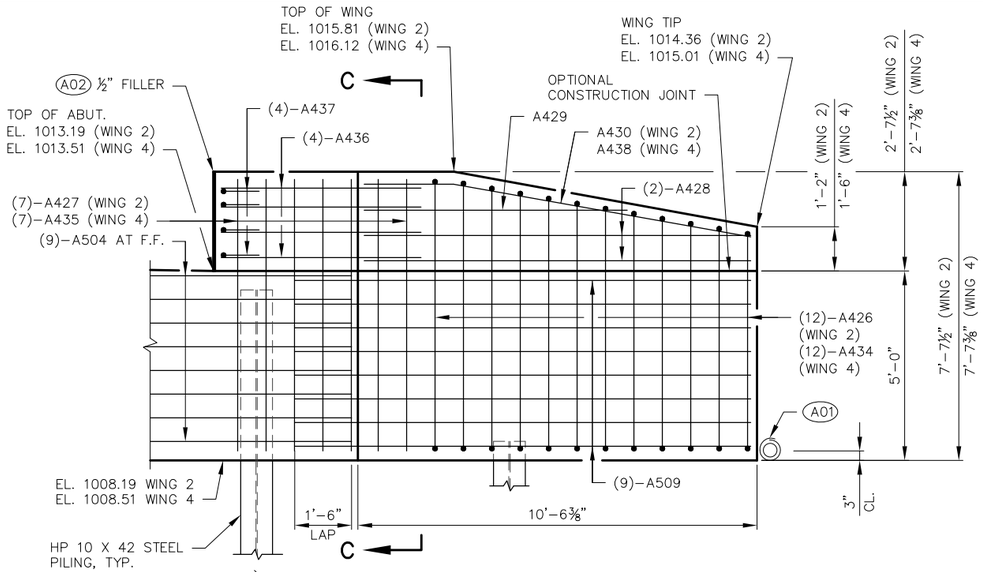
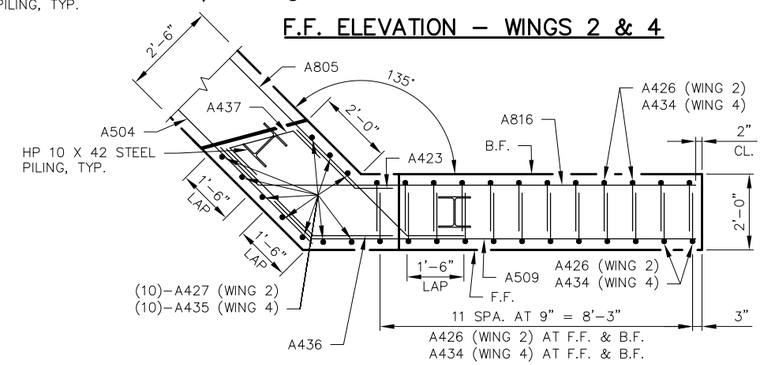
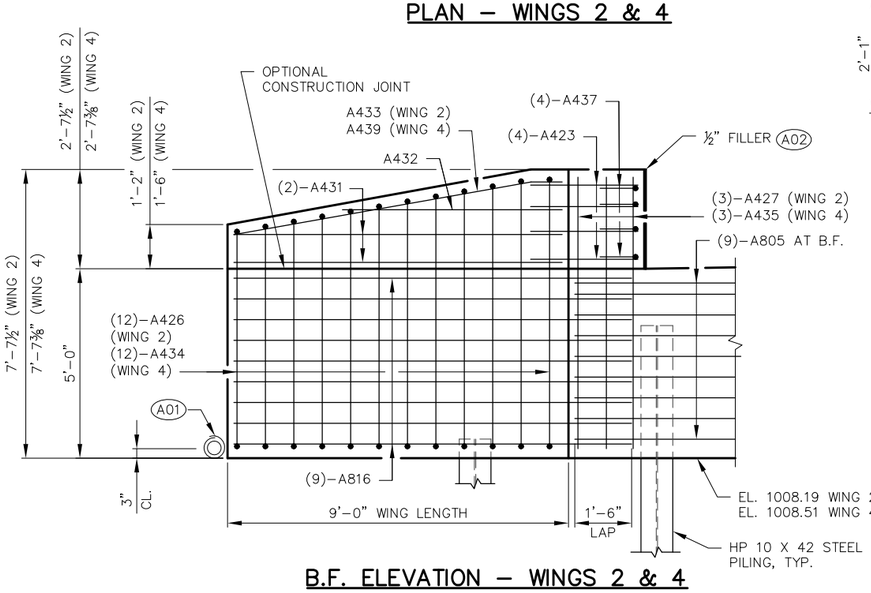


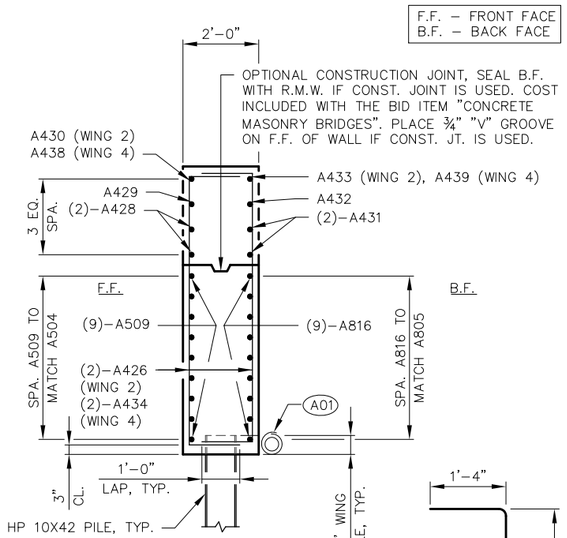
F.F. ELEVATION – WINGS 2 & 4



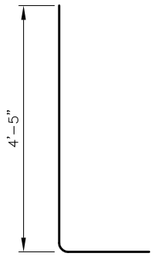
PLAN – WINGS 2 & 4



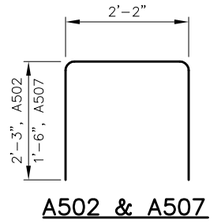
B.F. ELEVATION – WINGS 2 & 4



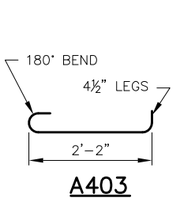
SECTION C–C



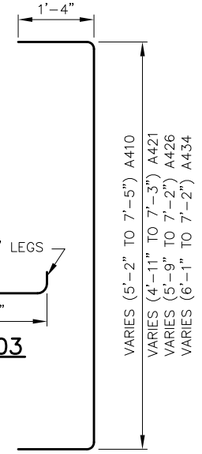
A501



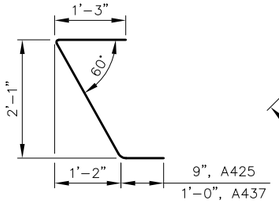
A502 & A507



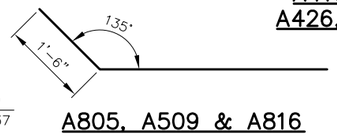
A403



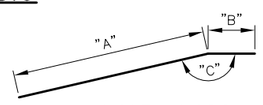
A410, A421, A426, & A434



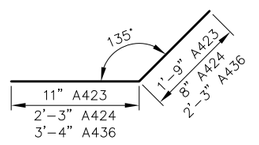
A425 & A437



A805, A509 & A816



A415, A420, A430, A433, A438, & A439



A423, A424, & A436

NOTES

DO NOT PLACE FILL ABOVE 3'–0" FROM THE BOTTOM OF THE ABUTMENT UNTIL SUPERSTRUCTURE IS IN PLACE.

WEST AND EAST ABUTMENTS TO BE SUPPORTED ON HP 10 X 42 STEEL PILING SEATED IN PREBORED HOLES CORED 3 FEET MINIMUM INTO ROCK. PILE DRIVING IS NOT REQUIRED. THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS 140 TONS MULTIPLIED BY A RESISTANCE FACTOR OF 0.5. ESTIMATED 10 FT PILE LENGTHS AT WEST ABUTMENT AND 20 FT PILE LENGTHS AT EAST ABUTMENT.

SEE "CROSS SECTION, GENERAL NOTES & QUANTITIES" SHEET FOR HP 10 X 42 STEEL PILING SPLICE DETAILS.

(A01) PIPE UNDERDRAIN WRAPPED (6–INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN AS DETAILED ON "WINGS 1 & 3 DETAILS" SHEET. RODENT SHIELD SHALL BE INCLUDED WITH THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6–INCH".

(A02) SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF ½" FILLER WITH NON–STAINING GRAY NON–BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD ⅛" BELOW SURFACE OF CONCRETE.) ½" FILLER TO EXTEND FROM BRIDGE SEAT TO TOP OF WING.

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5405–00–70

BILL OF BARS
BOTH ABUTMENTS

COATED = 2,870 LBS.
UNCOATED = 4,600 LBS.

MARK	COATED	NUMBER W. ABUT.	NUMBER E. ABUT.	LENGTH	BENT	BAR SERIES	LOCATION	
A501		72	72	5'–11"	X		BODY – STIRRUP – F.F. & B.F.	VERT.
A502		36	36	6'–5"	X		BODY – STIRRUP – TOP	VERT.
A403		27	27	3'–1"	X		BODY – TIES	HORIZ.
A504		18	18	18'–11"			BODY – F.F.	HORIZ.
A805		18	18	23'–10"	X		BODY – B.F.	HORIZ.
A406		3	––	16'–6"			BODY – TOP – ADDITIONAL REINF.	HORIZ.
A507		17	––	4'–11"	X		BODY – STIRRUP – TOP – ADD. REINF.	VERT.
A508	X	30	30	2'–0"			BODY – TOP DOWELS	VERT.
A509	X	18	18	11'–9"	X		WINGS 1 THRU 4 – F.F.	HORIZ.
A410	X	24	––	8'–10"	X	⚠	WING 1 – STIRRUP – F.F. & B.F.	VERT.
A411	X	7	––	7'–5"			WING 1 – F.F. & B.F.	VERT.
A412	X	1	1	9'–8"			WINGS 1 & 3 – F.F.	HORIZ.
A413	X	1	1	7'–5"			WINGS 1 & 3 – F.F.	HORIZ.
A414	X	1	1	5'–1"			WINGS 1 & 3 – F.F.	HORIZ.
A415	X	1	1	10'–6"	X		WINGS 1 & 3 – F.F. – TOP	HORIZ.
A816	X	18	18	13'–3"	X		WINGS 1 THRU 4 – B.F.	HORIZ.
A417	X	1	1	8'–2"			WINGS 1 & 3 – B.F.	HORIZ.
A418	X	1	1	5'–10"			WINGS 1 & 3 – B.F.	HORIZ.
A419	X	1	1	3'–6"			WINGS 1 & 3 – B.F.	HORIZ.
A420	X	1	1	8'–11"	X		WINGS 1 & 3 – B.F. – TOP	HORIZ.
A421	X	––	24	8'–7"	X	⚠	WING 3 – F.F. & B.F.	VERT.
A422	X	––	7	7'–3"			WING 3 – F.F. & B.F.	VERT.
A423	X	8	8	2'–8"	X		WINGS 1 THRU 4 – B.F. CORNER	HORIZ.
A424	X	4	4	2'–11"	X		WINGS 1 & 3 – F.F. CORNER	HORIZ.
A425	X	4	4	4'–2"	X		WINGS 1 & 3 – TOP CORNER	HORIZ.
A426	X	24	––	9'–0"	X	⚠	WING 2 – STIRRUP – F.F. & B.F.	VERT.
A427	X	10	––	7'–2"			WING 2 – F.F. & B.F.	VERT.
A428	X	2	2	10'–2"			WINGS 2 & 4 – F.F.	HORIZ.
A429	X	1	1	6'–11"			WINGS 2 & 4 – F.F.	HORIZ.
A430	X	1	––	10'–3"	X		WING 2 – F.F. – TOP	HORIZ.
A431	X	2	2	8'–8"			WINGS 2 & 4 – B.F.	HORIZ.
A432	X	1	1	5'–5"			WINGS 2 & 4 – B.F.	HORIZ.
A433	X	1	––	8'–8"	X		WING 2 – B.F. – TOP	HORIZ.
A434	X	––	24	9'–2"	X	⚠	WING 4 – STIRRUP – F.F. & B.F.	VERT.
A435	X	––	10	7'–2"			WING 4 – F.F. & B.F.	VERT.
A436	X	4	4	5'–7"	X		WINGS 2 & 4 – F.F. CORNER	HORIZ.
A437	X	4	4	4'–5"	X		WINGS 2 & 4 – TOP CORNER	HORIZ.
A438	X	––	1	10'–3"	X		WING 4 – F.F. – TOP	HORIZ.
A439	X	––	1	8'–8"	X		WING 4 – B.F. – TOP	HORIZ.

THE FIRST DIGIT OF A BAR MARK SIGNIFIES THE BAR SIZE.

ALL BAR BEND DIMENSIONS ARE OUT TO OUT OF BAR.

BUNDLE EACH ABUTMENT BARS SEPARATELY.

⚠ LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE "BAR SERIES TABLE" FOR ACTUAL LENGTHS.

BAR SERIES TABLE

MARK	NO. REQ'D	LENGTH
A410	2 SERIES OF 12	7'–8" TO 9'–11"
A421	2 SERIES OF 12	7'–5" TO 9'–9"
A426	2 SERIES OF 12	8'–3" TO 9'–8"
A434	2 SERIES OF 12	8'–7" TO 9'–8"

BUNDLE AND TAG EACH SERIES SEPARATELY.

BAR BEND DIMENSIONS

MARK	"A"	"B"	"C"
A415	8'–2"	2'–4"	163°
A420	8'–2"	0'–9"	163°
A430	7'–11"	2'–4"	170°
A433	7'–11"	0'–9"	170°
A438	7'–11"	2'–4"	172°
A439	7'–11"	0'–9"	172°

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STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B–62–272

DRAWN BY JDO | PLANS CK'D ACK

WINGS 2 & 4 DETAILS & ABUT. REINFORCEMENT

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PLOT SCALE:

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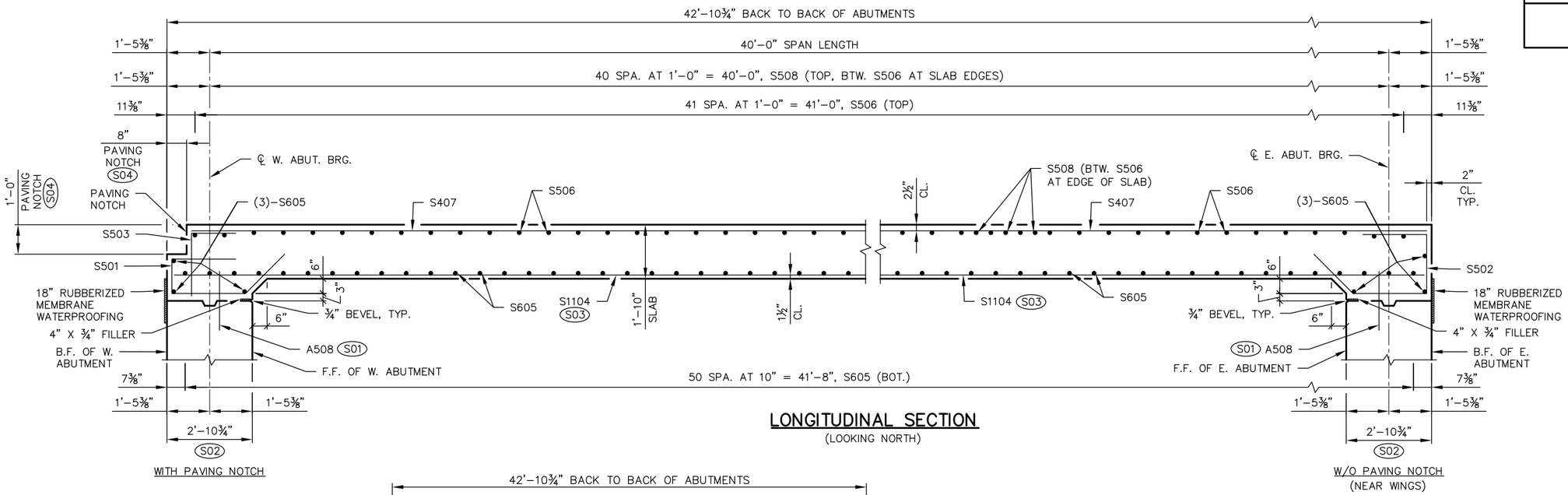
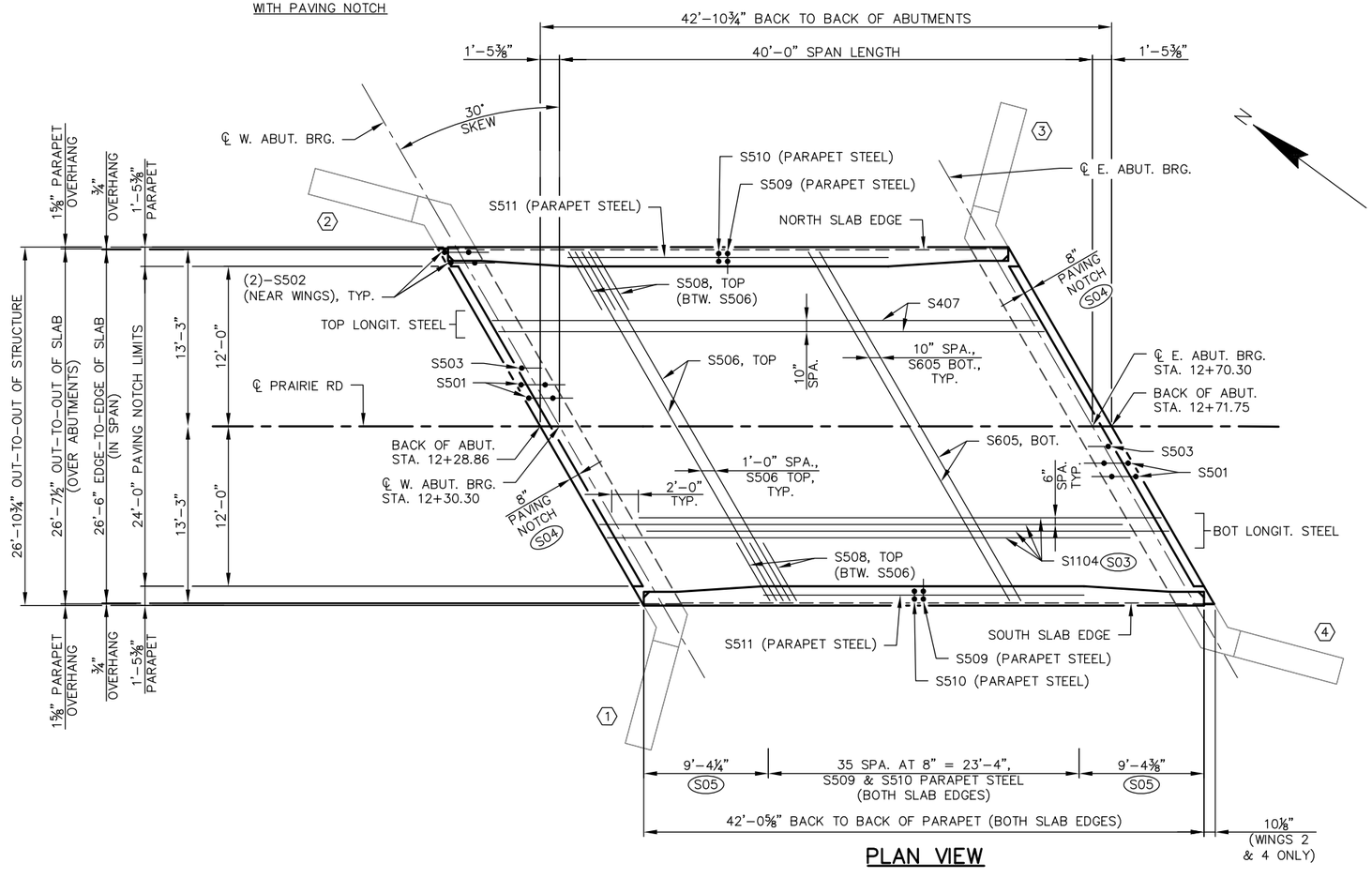
5405–00–70



LONGITUDINAL SECTION
(LOOKING NORTH)

WITH PAVING NOTCH

W/O PAVING NOTCH
(NEAR WINGS)



PLAN VIEW

NOTES

TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 3'–0" CENTERS EACH WAY.

BOTTOM LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 4'–0" CENTERS.

ALL SLAB THICKNESS DIMENSIONS ARE MINIMUM. ANY TOLERANCES NECESSARY TO CORRECT CONSTRUCTION DISCREPANCIES ARE TO BE PLUS (+).

PARAPETS TO BE CAST ON THE SLAB AFTER FALSEWORK HAS BEEN RELEASED.

(S01) SEE "ABUTMENTS" SHEET FOR PLACEMENT OF A508 BARS

(S02) DIMENSION IS TAKEN PARALLEL TO ℄ PRAIRIE RD

(S03) EXTEND ONE END OF THE S1104 BAR TO 2" CLEAR OF ONE BACK FACE OF ABUTMENT. ALTERNATE BETWEEN WEST AND EAST ABUTMENTS ACROSS ENTIRE SLAB.

(S04) APPLY PROTECTIVE SURFACE TREATMENT TO THE VERTICAL AND HORIZONTAL SURFACES OF THE PAVING NOTCHES.

(S05) PARAPET STEEL IN TRANSITION ZONE. SEE DETAILS ON "PARAPET & SUPERSTRUCTURE REINFORCEMENT" SHEET.

⬡ INDICATES WING NUMBER

F.F. – FRONT FACE
B.F. – BACK FACE

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STRUCTURE B–62–272

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SUPERSTRUCTURE

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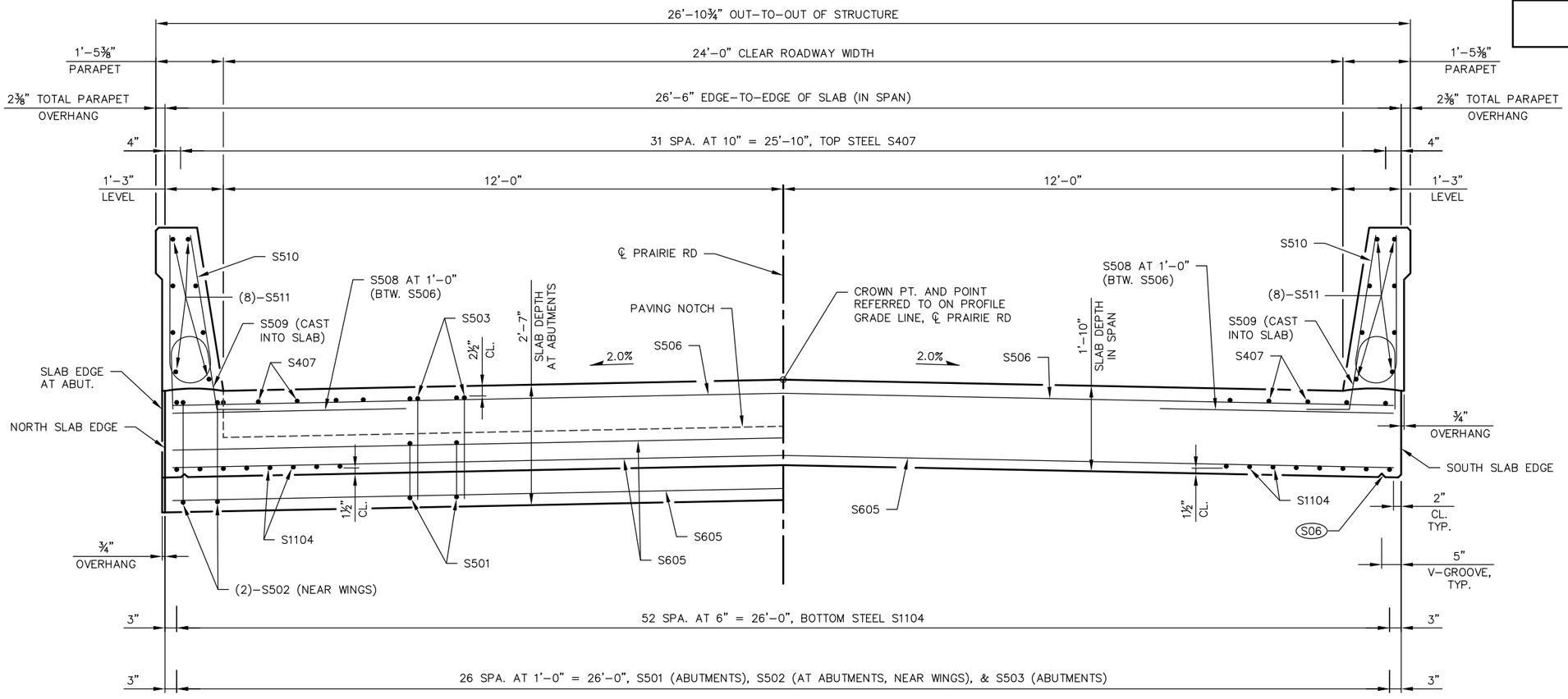
PLOT DATE: Aug 12, 2025

FILE: B620272_07_09_super.dwg
PLOT SCALE:

STATE PROJECT NUMBER

5405–00–70

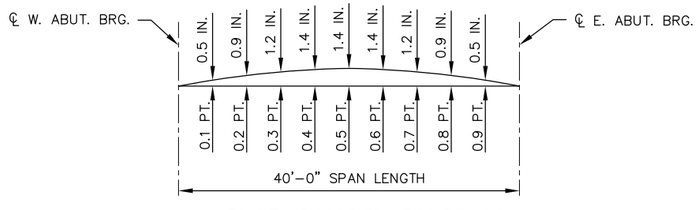


AT ABUTMENTS

IN SPAN

CROSS SECTION THRU ROADWAY

(LOOKING EAST)



SLAB CAMBER DIAGRAM

TO DETERMINE FALSEWORK ELEVATION AT EDGE OF SLAB, CROWN OR REFERENCE LINE FOLLOW THIS PROCEDURE:

	TOP OF SLAB ELEVATION AT FINAL GRADE
LESS	SLAB THICKNESS
PLUS	CAMBER
PLUS	FORM SETTLEMENT/DEFLECTION DUE TO PLACEMENT OF SLAB CONCRETE (TO BE COMPUTED BY THE CONTRACTOR)
EQUALS	TOP OF SLAB FALSEWORK ELEVATION.

TOP OF SLAB ELEVATIONS

SPAN PT	INSIDE FACE OF NORTH PARAPET	℄ PRAIRIE RD	INSIDE FACE OF SOUTH PARAPET
℄ W. ABUT.	1015.81	1016.20	1016.08
0.1	1015.90	1016.28	1016.14
0.2	1015.99	1016.34	1016.18
0.3	1016.06	1016.39	1016.20
0.4	1016.11	1016.42	1016.22
0.5	1016.16	1016.45	1016.22
0.6	1016.19	1016.46	1016.20
0.7	1016.21	1016.45	1016.18
0.8	1016.22	1016.44	1016.16
0.9	1016.21	1016.42	1016.14
℄ E. ABUT.	1016.19	1016.39	1016.12

SURVEY TOP OF SLAB ELEVATIONS

	℄ W. ABUT. BRG.	5/10 PT.	℄ E. ABUT. BRG.
NORTH SLAB EDGE			
℄ PRAIRIE RD			
SOUTH SLAB EDGE			

PRIOR TO RELEASING SLAB FALSEWORK, TAKE TOP OF SLAB ELEVATIONS AT THE ℄ OF ABUTMENTS AND AT 5/10 POINTS TO VERIFY CAMBER. TAKE ELEVATIONS ALONG EDGE OF SLAB AND REFERENCE LINE. RECORD THE ELEVATIONS IN THE ABOVE TABLE FOR THE "AS BUILT" PLANS.

NOTES

CAMBER SPAN AS SHOWN TO PROVIDE FOR DEAD LOAD DEFLECTION AND FUTURE CREEP. CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT.

(S06) ¾" V–GROOVE. EXTEND V–GROOVE TO 6" FROM FRONT FACE OF ABUTMENT BODY. V–GROOVES ARE REQUIRED.

NO.	DATE	REVISION	BY

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DEPARTMENT OF TRANSPORTATION

STRUCTURE B–62–272

DRAWN BY JDO | PLANS CK'D ACK

SUPERSTRUCTURE DETAILS

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PLOT DATE: Aug 12, 2025

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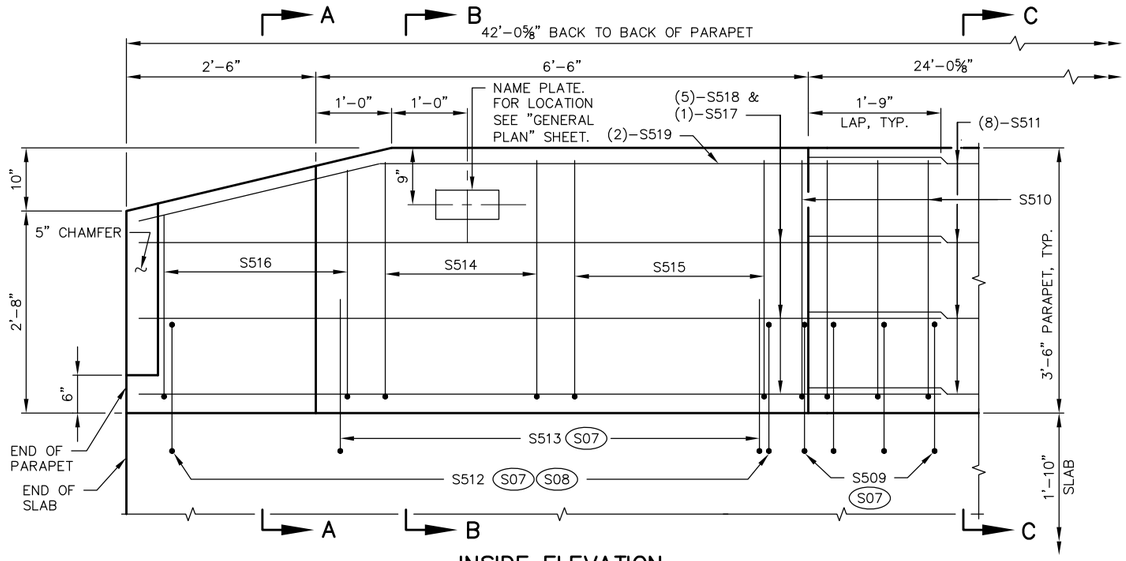
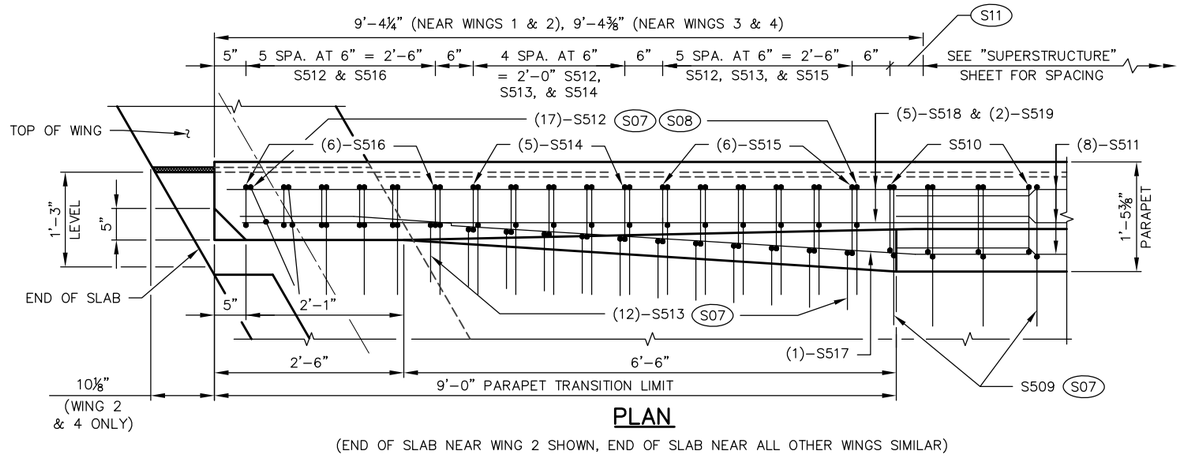
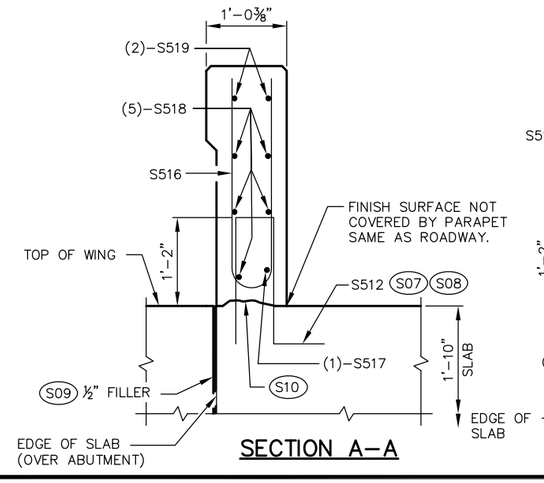


INSIDE ELEVATION

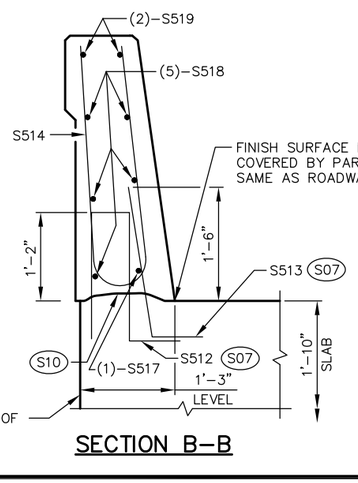


PLAN

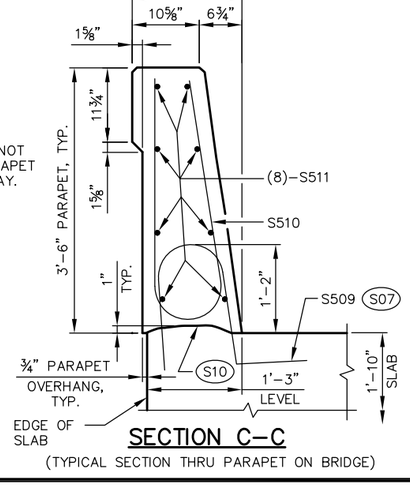
(END OF SLAB NEAR WING 2 SHOWN, END OF SLAB NEAR ALL OTHER WINGS SIMILAR)



SECTION A–A

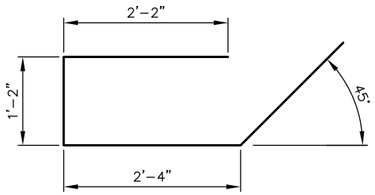


SECTION B–B

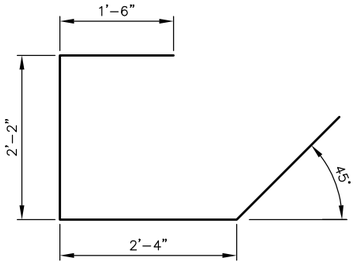


SECTION C–C

(TYPICAL SECTION THRU PARAPET ON BRIDGE)



S501



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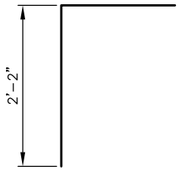
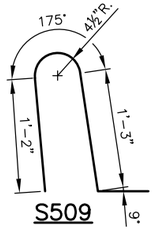


S503



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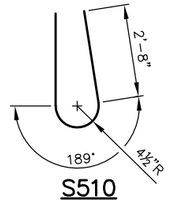

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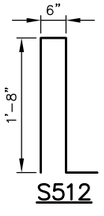
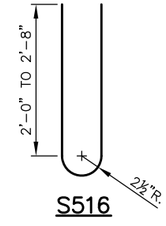


S512

S513



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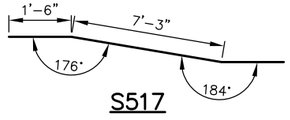

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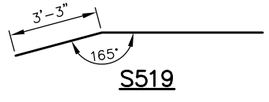
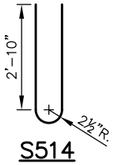


S519

BAR SERIES TABLE

MARK	NO. REQD.	LENGTH
S516	4 SERIES OF 6	4'–9" TO 6'–1"



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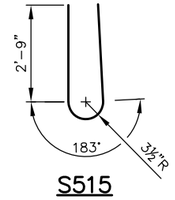

S515

STATE PROJECT NUMBER

5405–00–70

BILL OF BARS SUPERSTRUCTURE

COATED = 19,370 LBS.

MARK	NUMBER COATED	NUMBER UNCOATED	LENGTH	BENT	BAR SERIES	LOCATION	
S501	46		7'–5"	X		SLAB AT ABUTMENT – TIES	LONGIT.
S502	8		7'–9"	X		SLAB AT ABUTMENT WINGS – TIES	LONGIT.
S503	46		3'–7"	X		SLAB AT ABUTMENT – TIES W/S501	LONGIT.
S1104	53		39'–3"			SLAB – BOTTOM	LONGIT.
S605	57		30'–2"			SLAB – BOTTOM & OVER ABUTMENTS	TRANS.
S506	42		30'–2"			SLAB – TOP	TRANS.
S407	32		40'–11"			SLAB – TOP	LONGIT.
S508	82		5'–0"			SLAB – TOP BTW. S506	TRANS.
S509	76		4'–5"	X		PARAPET – STIRRUP	VERT.
S510	76		6'–8"	X		PARAPET – STIRRUP	VERT.
S511	16		24'–1"			PARAPET – HORIZ.	LONGIT.
S512	68		4'–4"	X		PARAPET – TRANSITION – STIRRUP	VERT.
S513	48		2'–9"	X		PARAPET – TRANSITION – STIRRUP	VERT.
S514	20		6'–5"	X		PARAPET – TRANSITION – STIRRUP	VERT.
S515	24		6'–6"	X		PARAPET – TRANSITION – STIRRUP	VERT.
S516	24		5'–5"	X	⚠	PARAPET – TRANSITION – STIRRUP	VERT.
S517	4		10'–8"	X		PARAPET – TRANSITION – HORIZ.	LONGIT.
S518	20		10'–7"			PARAPET – TRANSITION – HORIZ.	LONGIT.
S519	8		10'–9"	X		PARAPET – TRANSITION – HORIZ.	LONGIT.

THE FIRST OR FIRST TWO DIGITS OF A FOUR DIGIT BAR MARK SIGNIFIES THE BAR SIZE.

ALL BAR BEND DIMENSIONS ARE OUT TO OUT OF BAR.

⚠ LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.

NOTES

(S07) S509, S512, AND S513 BARS TO BE TIED TO SUPERSTRUCTURE SLAB BEFORE CONCRETE IS POURED.

(S08) ADJUST ORIENTATION OF S512 NEAR END OF SLAB TO ENSURE CLEAR COVER AT PAVING NOTCH.

(S09) SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF ½" FILLER WITH NON–STAINING GRAY NON–BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD ⅛" BELOW SURFACE OF CONCRETE).

(S10) CONST. JOINT – STRIKE OFF AS SHOWN AND LEAVE ROUGH.

(S11) 5¼" NEAR WINGS 1 AND 2. 5⅜" NEAR WINGS 3 AND 4.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B–62–272

DRAWN BY JDO | PLANS CK'D ACK

PARAPET & SUPERSTRUCTURE REINFORCEMENT

SHEET 9 OF 9

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PLOT DATE: Aug 12, 2025

FILE: B620272_07_09_super.dwg
PLOT SCALE:

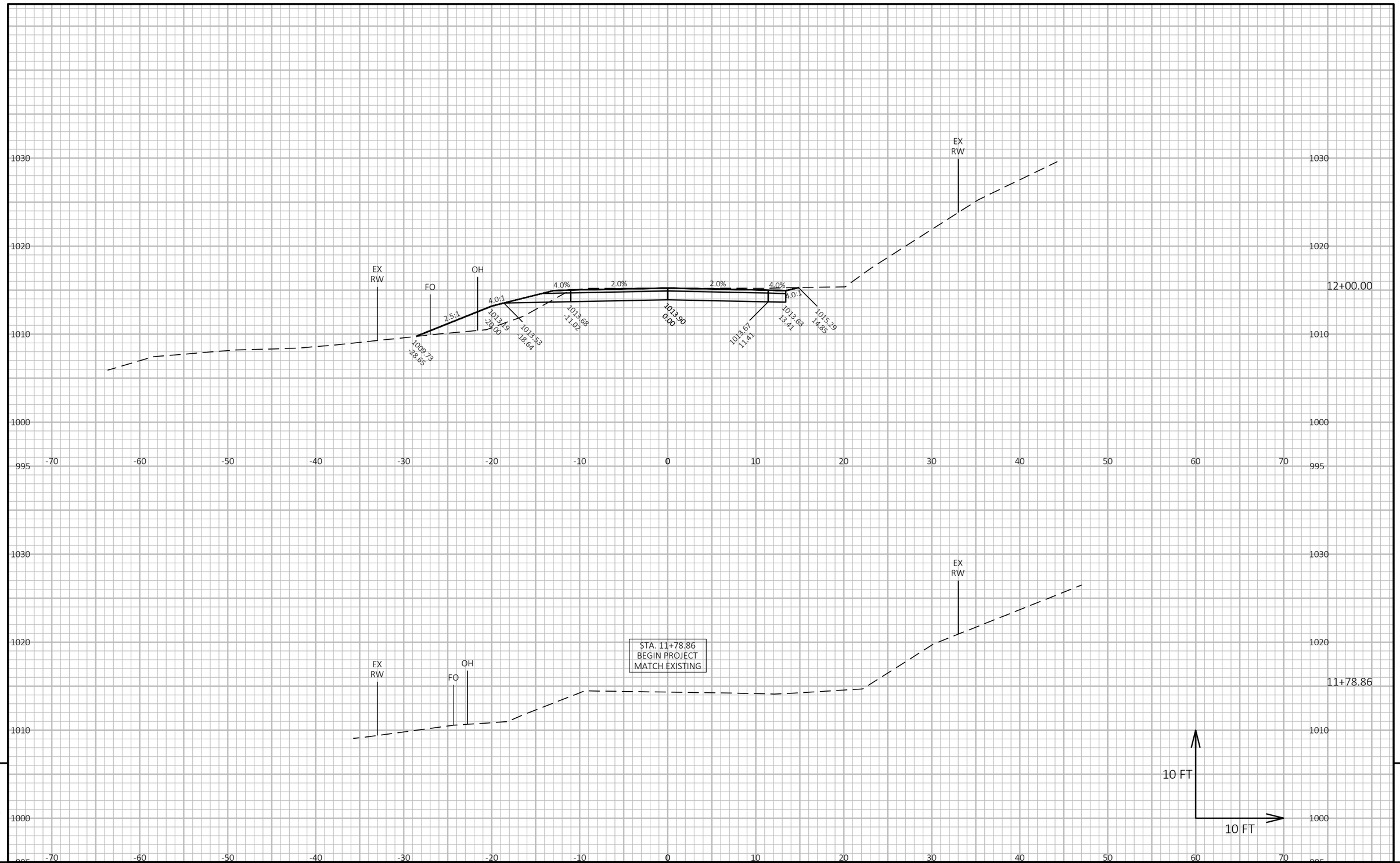
WEST APPROACH

STATION	DISTANCE	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)		
		CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
					NOTE 1	NOTE 2	NOTE 3	NOTE 1		NOTE 4
11+78.86	0.00	34.35	0.00	7.95	0	0	0	0	0	0
12+00.00	21.14	37.86	0.00	21.45	28	0	12	28	15	13
12+12.00	12.00	37.13	0.00	34.11	17	0	12	45	30	15
12+35.21	23.21	0.16	0.00	1.08	16	0	15	61	49	12
STRUCTURE B-62-0272										
DIVISION 1 TOTAL					61	0	39			

EAST APPROACH

STATION	DISTANCE	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)		
		CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
					NOTE 1	NOTE 2	NOTE 3	NOTE 1		NOTE 4
STRUCTURE B-62-0272										
12+65.41	0.00	3.90	0.00	9.06	0	0	0	0	0	0
12+89.57	24.16	46.00	0.00	2.22	22	0	5	22	6	16
13+00.00	10.43	41.66	0.00	0.00	17	0	0	39	6	33
13+21.76	21.76	44.23	0.00	0.00	35	0	0	74	6	68
DIVISION 2 TOTAL					74	0	5			
PROJECT TOTAL					135	0	44			

NOTES:	
1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL
2 - SALVAGED/UNUSABLE PAVEMENT MATERIAL	THIS DOES NOT SHOW UP IN CROSS SECTIONS
3 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME
4 - MASS ORDINATE	[(CUT) - (FILL * FILL FACTOR) - (SALVAGED/UNUSABLE PAVEMENT MATERIAL)]
	PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS QUANTITY INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.

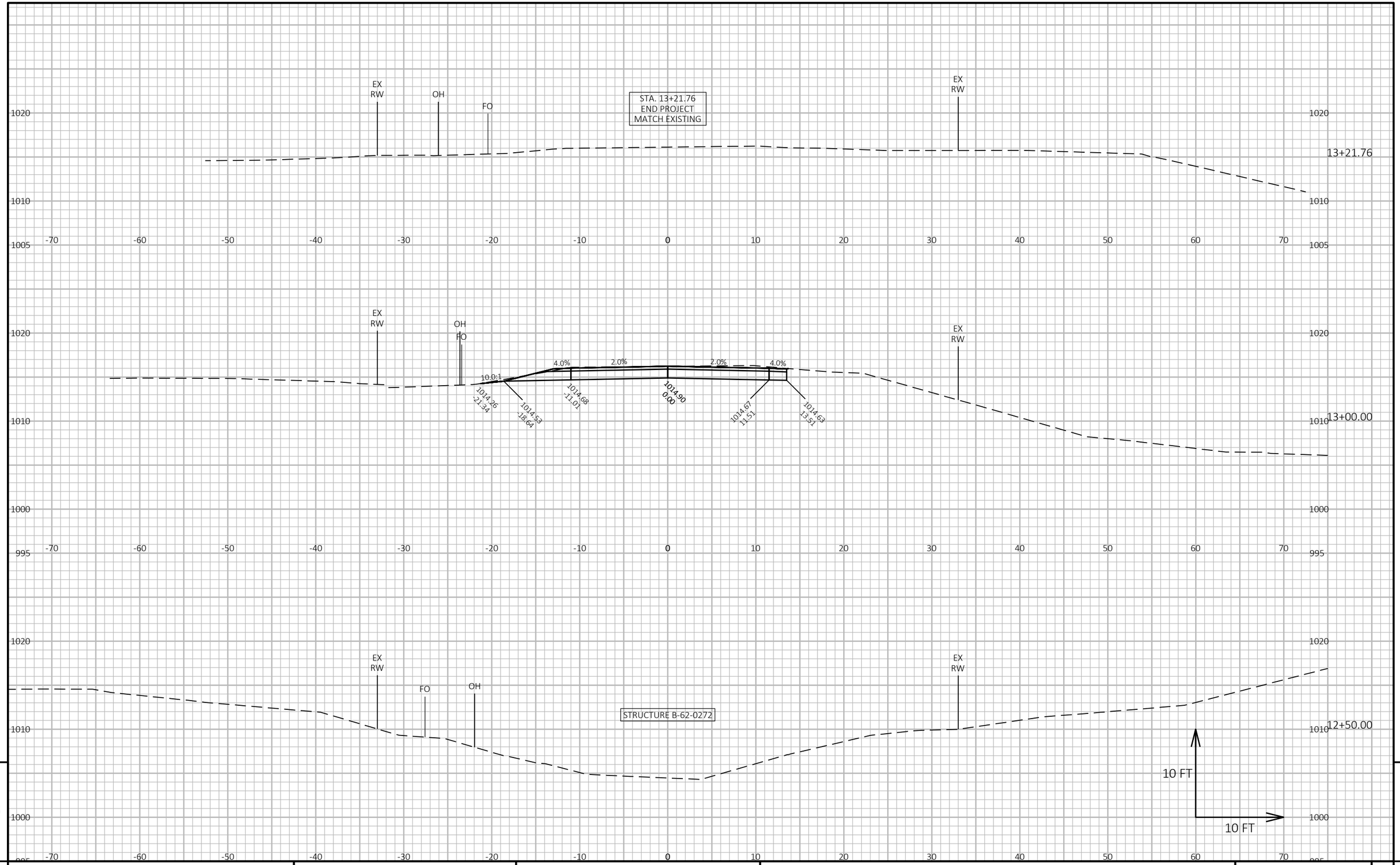


PROJECT NO: 5405-00-70	HWY: PRAIRIE RD	COUNTY: VERNON	CROSS SECTIONS	SHEET 39	E

FILE NAME : G:\00-PROJECT FILES\2022\22317 PRAIRIE RD, RUSH CREEK BRIDGE, VERNON COUNTY\0-CAD\DSGN\CRDR\CRDR-LCL-PRAIRIERD.DWG
LAYOUT NAME - 090201_xs

PLOT DATE : 11/4/2024 8:45 AM PLOT BY : GAVIN WIPPERFURTH PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



9

9

PROJECT NO: 5405-00-70	HWY: PRAIRIE RD	COUNTY: VERNON	CROSS SECTIONS	SHEET 40	E

FILE NAME : G:\00-PROJECT FILES\2022\22317 PRAIRIE RD, RUSH CREEK BRIDGE, VERNON COUNTY\0-CAD\DSGN\CRDR\CRDR-LCL-PRAIRIERD.DWG
LAYOUT NAME - 090202_xs

PLOT DATE : 11/4/2024 8:45 AM PLOT BY : GAVIN WIPPERFURTH PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

Notes

Wisconsin Department of Transportation

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through innovation and exceptional service.

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