

MAD

PROJECT ID:
WITH: N/A

6235-01-73

COUNTY:

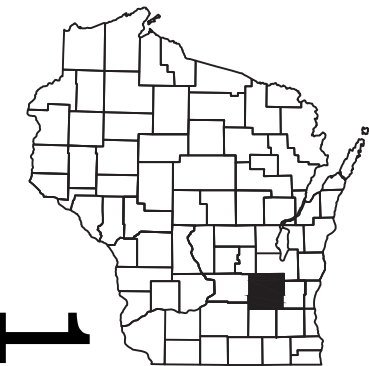
DODGE

JANUARY 2026

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 184



DESIGN DESIGNATION 6235-01-03

A.A.D.T.	2026	=	3,000 - 4,000
A.A.D.T.	2046	=	3,000 - 4,600
D.H.V.		=	418 (2046)
D.D.		=	59/41
T.		=	14.4% - 22.6%
DESIGN SPEED		=	25 M.P.H. - 55 M.P.H.
ESALS		=	730,000

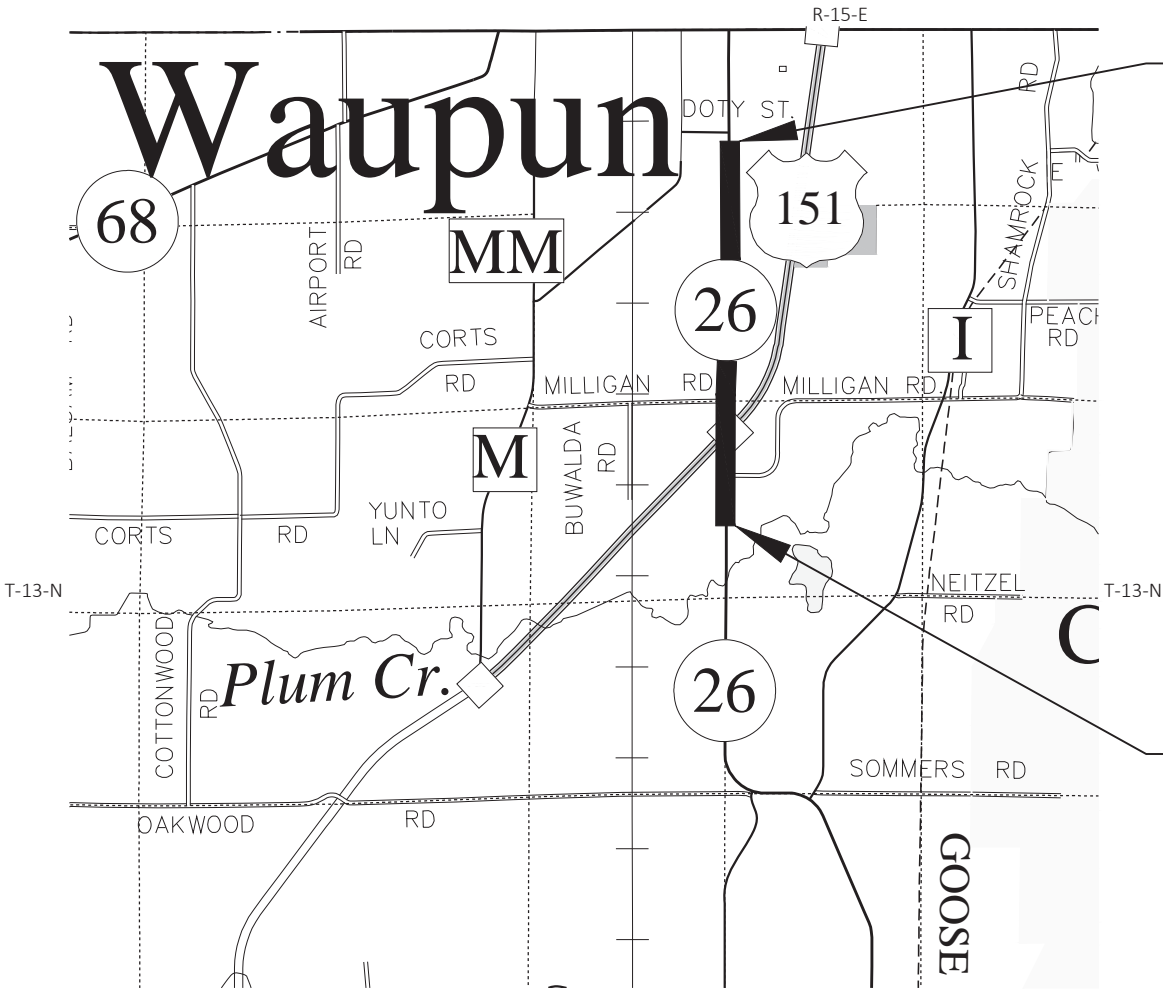
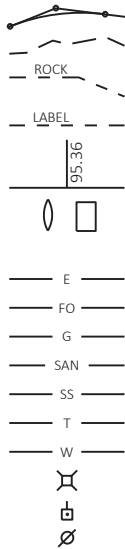
CONVENTIONAL SYMBOLS

PLAN

CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE

GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	



LAYOUT
SCALE 0 1 MI
TOTAL NET LENGTH OF CENTERLINE = 1.933 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), DODGE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 18-WI.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
6235-01-73	WISC 2026142	1

STATE PROJECT NUMBER
6235-01-73

END PROJECT
STA 682+06.64
Y: 784,016.9950
X: 877,305.2109

BEGIN PROJECT
STA 580+00.00
Y: 773,813.7300
X: 877,218.7681

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
PREPARED BY	MSA & STRAND ASSOCIATES
Surveyor	MICKEY DOYLE
Designer	MICHAEL RAMPETSREITER
Project Manager	MICHAEL BANASZAK
Regional Examiner	JUSTIN KUTSCHENREUTER
Regional Supervisor	
APPROVED FOR THE DEPARTMENT	
DATE:	Mike Rampetsreiter

GENERAL NOTES

NO TREES OR SHRUBS THAT ARE NOT MARKED ON THE PLAN TO BE REMOVED SHALL BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER IN THE FIELD.

THE LOCATIONS OF EXISTING UTILITY LOCATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE ARE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN. CONTRACTOR SHALL COORDINATE THEIR CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

HMA PAVEMENT AND WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN.

THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, BIKE, OR PARKING LANE.

APPLY TACK COAT BETWEEN LAYERS OF ASPHALT PAVEMENT AND TO MILLED SURFACES. THE APPLICATION RATE IS 0.05 GAL/SY BETWEEN LAYERS OF NEW HMA PAVEMENT AND 0.07 GAL/SY PLACED ON EXISTING ASPHALT, MILLED SURFACES AND CONCRETE PAVEMENT OR AS DIRECTED BY THE ENGINEER.

SAWCUTS, AS SHOWN ON THE PLANS, ARE SUGGESTED LOCATIONS AND MAY BE ADJUSTED AT THE DISCRETION OF THE ENGINEER TO BETTER SUIT FIELD CONDITIONS.

EXACT SIGN LOCATIONS SHOWN ON THE PLANS ARE APPROXIMATE AND THE LOCATIONS OF SIGNS ARE TO BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE EROSION CONTROL DEVICES AS SHOWN ON THE EROSION CONTROL SHEETS ARE AT SUGGESTED LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE ENGINEER. EROSION CONTROL DEVICES SHALL BE PLACED IN SEQUENCE WITH CONSTRUCTION OPERATIONS OR AS DETERMINED BY THE ENGINEER.

ALL PROPERTY LINES ON PLANS ARE APPROXIMATE.

CURB AND GUTTER PLAN GRADES AND RADII ARE AT THE FLANGE LINE UNLESS OTHERWISE NOTED.

CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY HIS OPERATION OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.

CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS AT ALL TIMES EXCEPT WHEN PAVING OPERATIONS REQUIRE THE DRIVEWAY TO BE CLOSED. ACCESS TO DRIVEWAY SHALL BE RE-ESTABLISHED IMMEDIATELY AFTER PAVEMENT DRIVEWAY AREA IS INSTALLED. ACCESS SHALL BE PROVIDED DURING ALL NON-WORKING HOURS.

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS, SHALL BE FERTILIZED, SEEDED, AND EROSION MAT INSTALLED AS SHOWN ON THE EROSION CONTROL PLAN OR AS DIRECTED BY THE FIELD ENGINEER.

THE LOCATION OF DRIVEWAYS WILL BE DETERMINED BY THE ENGINEER.

ELEVATIONS SHOWN ON THIS PLAN ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 88, NAVD (2012).

ORDER OF SECTION 2 SHEETS

- GENERAL NOTES
- PROJECT OVERVIEW
- TYPICAL SECTION
- CONSTRUCTION DETAILS
- INTERSECTION DETAIL
- REMOVALS
- CURB RAMP DETAILS
- EROSION CONTROL
- PAVEMENT MARKING
- TRAFFIC CONTROL
- TEMPORARY PEDESTRIAN ACCOMMODATION
- DETOUR
- ALIGNMENT DETAILS

STANDARD ABBREVIATIONS

AC	ACRE	LC	LONG CHORD
AGG	AGGREGATE	LS	LUMP SUM
<	ANGLE	M.P.	MARKER POST
AE, AEW	APRON ENDWALL	MGAL	1000 GALLONS
ASPH.	ASPHALTIC	N.C.	NORMAL CROWN
A.D.T.	AVERAGE DAILY TRAFFIC	N	NORTH
A.A.D.T.	ANNUAL AVERAGE DAILY TRAFFIC	NB	NORTHBOUND
B.F.	BACK FACE	NOR	NORMAL
BM	BENCHMARK	NO.	NUMBER
BTWN	BETWEEN	PAVT	PAVEMENT
CTR.	CENTER	P...F.	PERMANENT LIMITED EASEMENT
C/L	CENTER LINE	P.C.	POINT OF CURVATURE
A	CENTRAL ANGLE OR DEFLECTION	P...I.	POINT OF INTERSECTION
C.E.	COMMERCIAL ENTRANCE	P.T.	POINT OF TANGENCY
CONST.	CONSTRUCTION	PCC	PORTLAND CEMENT CONCRETE
CMCP	CORRUGATED METAL CULVERT PIPE	P.E.	PRIVATE ENTRANCE
CMF	CORRUGATED METAL PIPE	PG.	PROFILE GRADE LINE
CO.	COUNTY	P...L.	PROPERTY LINE
CTH	COUNTY TRUNK HIGHWAY	R	RADIUS OR RANGE
CR.	CREEK	R/L	REFERENCE LINE
CABC	CRUSHED AGGREGATE BASE COURSE	R.C.C.P.	REINFORCED CONCRETE CULVERT
CY	CUBIC YARD	REQ'D	REQUIRED
CP	CONTROL POINT OR CULVERT PIPE	RT	RIGHT
C&G	CURB AND GUTTER	R.H.F.	RIGHT HAND FORWARD
D	DEGREE OF CURVE	R/W	RIGHT OF WAY
D.H.V.	DESIGN HOURLY VOLUME	RD.	ROAD
D.A.	DIAMETER	SHLD.	SHOULDER(S)
D.D.	DIRECTIONAL DISTRIBUTION	SHR.	SHRINKAGE
D.SCH.	DISCHARGE	S	SOUTH
D.V.S	DYNAMIC MESSAGE SIGN	SB	SOUTHBOUND
EA	EACH	S.F.	SQUARE FOOT (FEET)
E	EAST	SDD	STANDARD DETAIL DRAWING(S)
EB	EASTBOUND	STH	STATE TRUNK HIGHWAY
ELEC.	ELECTRIC(AL), ELEC. CABLE	STA.	STATION
EL., ELEV.	ELEVATION	S.E.	SUPERELEVATION
ESALS	EQUIVALENT SINGLE AXLE LOADS	S/L	SURVEY LINE
EXC.	EXCAVATION	SYM	SYMMETRICAL
EXIST	EXISTING	T.	PERCENT TRUCKS
F.F.	FACE TO FACE	TEL.	TELEPHONE
FERT.	FERTILIZER	TEMP.	TEMPORARY
F.E.	FIELD ENTRANCE	T...E.	TEMPORARY LIMITED EASEMENT
F/..., F...	FLOW LINE	T.O.C.	TOP OF CURB
GALV.	GALVANIZE	TYP	TYPICAL
H.S.	HIGH STRENGTH	UNCL.	UNCLASSIFIED
CWT	HUNDRED WEIGHT	U.G.	UNDERGROUND (CABLE)
INL	INLET	VAR	VARIABLE
INTER.	INTERSECTION	V.C.	VERTICAL CURVE
IH	INTERSTATE HIGHWAY	V.P.C.	VERTICAL POINT OF CURVATURE
JT.	JOINT	V.P.I.	VERTICAL POINT OF INTERSECTION
LT	LEFT	V.P.T.	VERTICAL POINT OF TANGENCY
L.H.F.	LEFT HAND FORWARD	WL	WEIGHT
L	LENGTH OF CURVE	W	WEST
L.F.	LINEAR FOOT (FEET)	WB	WESTBOUND

DNR CONTACT
DNR LIASON
ERIC HEGGELUND
3911 FISH HATCHERY RD.
FITCHBURG, WI 53711
(608) 228-7927
ERIC.HEGGELUND@WISCONSIN.GOV

WISDOT CONTACT
PROJECT MANAGER
MICHAEL RAMPETSREITER, P.E.
2101 WRIGHT ST.
MADISON, WI 53704
(608) 246-7917
MICHAEL.RAMPETSREITER@DOT.WI.GOV

UTILITIES

ALLIANT ENERGY
ELECTRICITY
JEREMY RENTMEESTER
120 EAST MAPLE AVE.
BEAVER DAM, WI 53916
920-887-6041
JEREMYRENTMEESTER@ALLIANTENERGY.COM

ALLIANT ENERGY
GAS/PETROLEUM
JEREMY RENTMEESTER
120 EAST MAPLE AVE.
BEAVER DAM, WI 53916
920-887-6041
JEREMYRENTMEESTER@ALLIANTENERGY.COM

AT&T WISCONSIN
COMMUNICATION LINE
CHARLES BARTELT
70 E DIVISION ST
FOND DU LAC, WI 54935
920-410-5104
CB1461@ATT.COM

ATC MANAGEMENT, INC.
ELECTRICITY-TRANSMISSION
DOUG VOSBERG
2489 RINDEN RD.
COTTAGE GROVE, WI 53527
608-877-7650
DVOSBERG@ATCLLC.COM

PAETEC COMMUNICATIONS, LLC
COMMUNICATION LINE
LORI KETTER
969 WAUBE LN.
GREEN BAY, WI 54304
920-410-6902

LORI.KETTER@WINDSTREAM.COM
SPECTRUM
COMMUNICATION LINE
JOSHUA WALKOWIAK
165 KNIGHTS WAY
FOND DU LAC, WI 54935
608-419-6214
JOSHUAWALKOWIAK@CHARTER.COM

WAUPUN UTILITIES
ELECTRICITY
OWEN VANDE KOLK
817 S. MADISON ST.
WAUPUN, WI 53963
920-324-7920
OVANDEKOLK@WAUPUNUTILITIES.ORG

WAUPUN UTILITIES
SEWER
MATT GESCHKE
817 S. MADISON ST.
WAUPUN, WI 53963
920-324-7920
MGESCHKE@WAUPUNUTILITIES.ORG

WAUPUN UTILITIES
WATER
MATT GESCHKE
817 S. MADISON ST.
WAUPUN, WI 53963
920-324-7920
MGESCHKE@WAUPUNUTILITIES.ORG

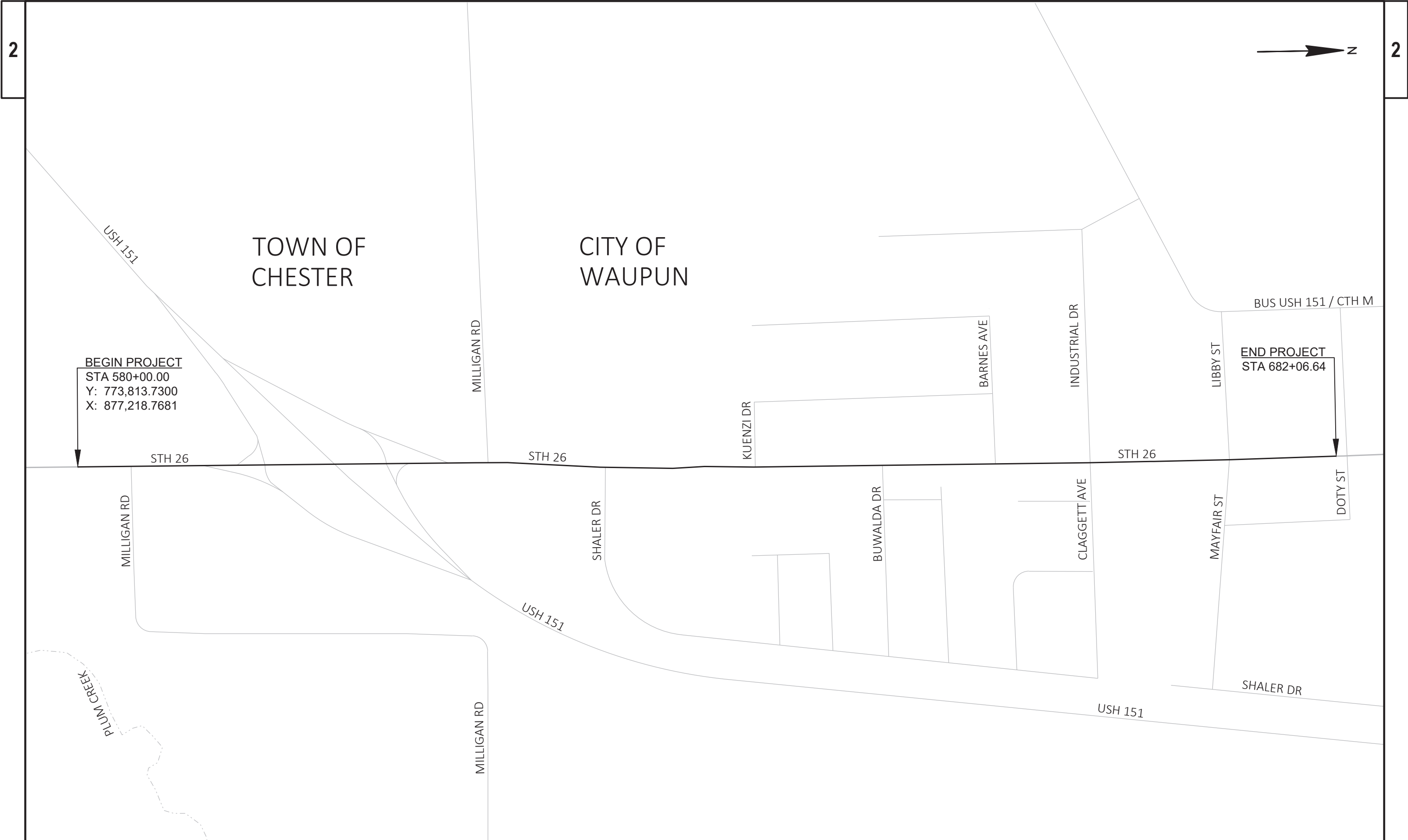


Dial 811 or (800)242-8511
www.DiggersHotline.com

TABLE 2—Asphalt/Concrete Depth Summary

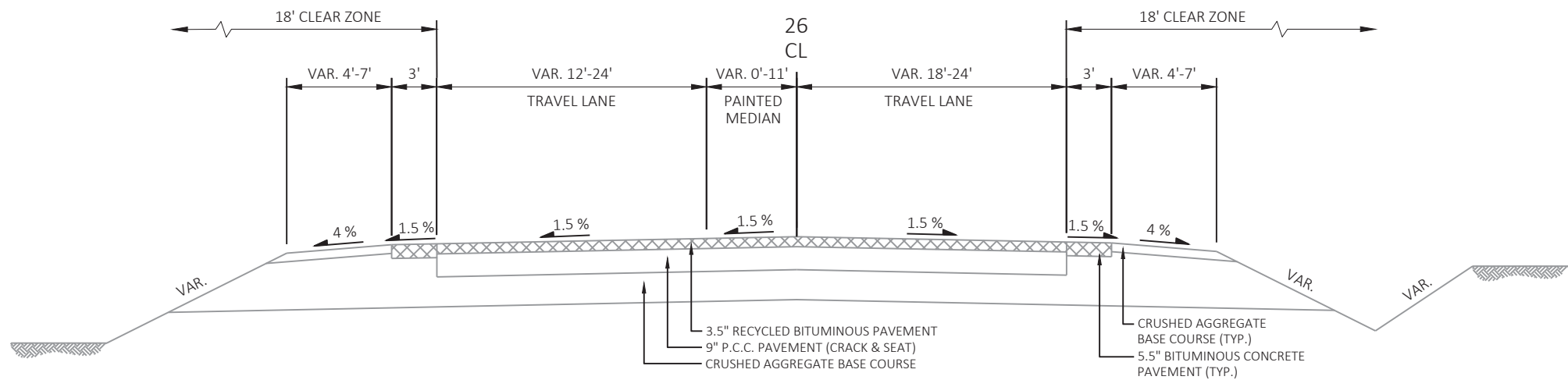
BH/CORE NUMBER	ASPHALT (T), INCHES	CONCRETE (T), INCHES	STATION ¹	OFFSET (FT)	NOTES
BH01	3.0	11.0	595+87	4.0	EAST OF YELLOW LINE, NB LANE
BH02	4.0	7.0	601+09	3.8	EAST OF MEDIAN CURB
BH03	NE ²	NE	606+08	16.0	EAST OF YELLOW LINE, SB ROAD, (MEDIAN BH) 21' S OF ORIGINAL STATION
BH04	6.75	NE	608+77	5.0	EAST OF THE WEST CURB OF NB LANE
BH05	5.25	4.0	635+31	7.0	W OF WHITE LINE, NB LANE. THE CONC SLAB IS ACTUALLY 10.75" BELOW THE ASPHALT.
BH06	5.50	4.0	644+73	4.0	E OF WHITE LINE, SB LANE. THE CONC SLAB IS ACTUALLY 13.50" BELOW THE ASPHALT.
BH07	5.0	5.0	665+34	4.0	E OF WHITE LINE, SB LANE. THE CONC SLAB IS ACTUALLY 16.50" BELOW THE ASPHALT.
BH08	4.5	6.5	679+98	4.0	E OF WHITE LINE, SB LANE. THE CONC SLAB IS ACTUALLY 3.0" BELOW THE ASPHALT.
C01	3.25	9.0	584+36	4.0	EAST OF WHITE LINE, SB LANE
C02	6.0	NE	584+36	5.0	EAST OF YELLOW LINE, SB LANE
C03	3.0	9.0	584+36	2.0	WEST OF WHITE LINE, NB LANE
C04	3.0	9.5	590+07	3.0	WEST OF YELLOW LINE, SB LANE
C05	3.0	8.75	600+08	5.0	EAST OF WHITE LINE, SB LANE
C06	2.75	9.0	604+09	7.0	EAST OF WHITE LINE, SB LANE
C07	3.75	7.25	608+77	1.0	WEST OF WHITE LINE, NB LANE
C08	2.5	9.0	608+77	4.0	EAST OF WHITE LINE, SB LANE
C09	4.25	9.25	609+91	1.0	WEST OF WHITE LINE, NB LANE
C10	3.5	9.0	613+33	5.0	EAST OF WHITE LINE SEPARATING TURN AND DRIVE LANES, SB ROAD
C11	5.25	NE	629+57	3.0	EAST OF YELLOW LINE, SB LANE
C12	5.0	NE	650+10	7.0	EAST OF DOTTED LINE SEPARATING DRIVE AND TURN LANES, SB ROAD
C13	4.75	NE	656+73	3.0	WEST OF WHITE LINE, NB LANE
C14	3.25	NE	671+88	4.0	WEST OF WHITE LINE, NB LANE
C15	4.5	NE	675+37	4.0	EAST OF WHITE LINE, SB LANE

2--NE: NOT ENCOUNTERED.

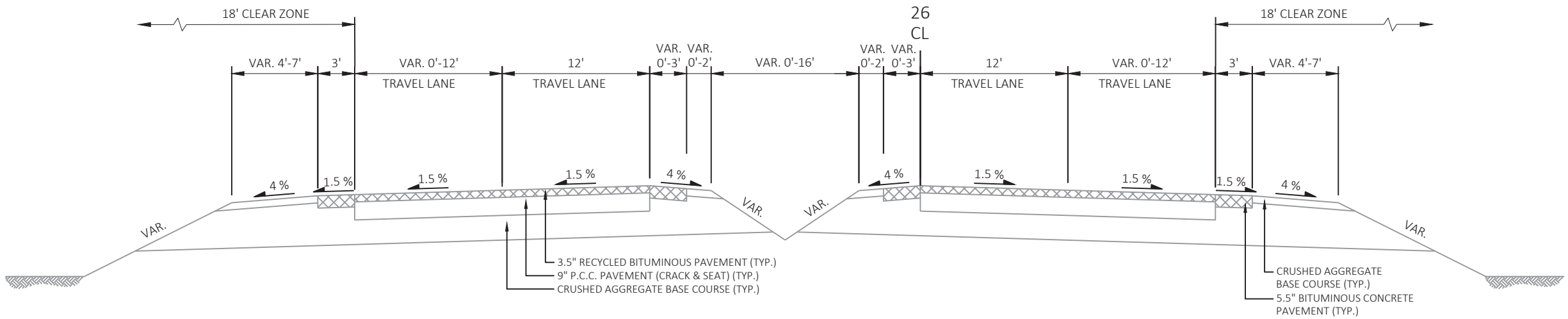


PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	PROJECT OVERVIEW	SHEET	E
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DESIGN NOTES
1. ALL EXISTING PAVEMENT THICKNESSES ARE APPROXIMATE, REMOVALS OF CONCRETE PAVEMENT, BASE, OR OTHER MATERIALS WITHIN PLANNED DEPTHS OF REMOVING ASPHALTIC SURFACING MILLING IS INCIDENTAL..



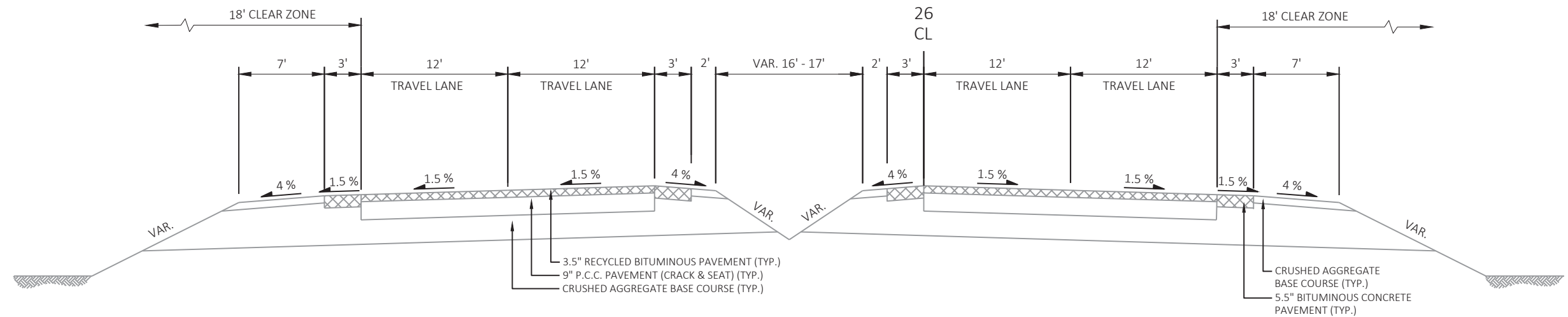
EXISTING TYPICAL SECTION
STA 580+00.00 - 585+72.37



EXISTING TYPICAL SECTION
STA 585+72.37 - 590+68.23

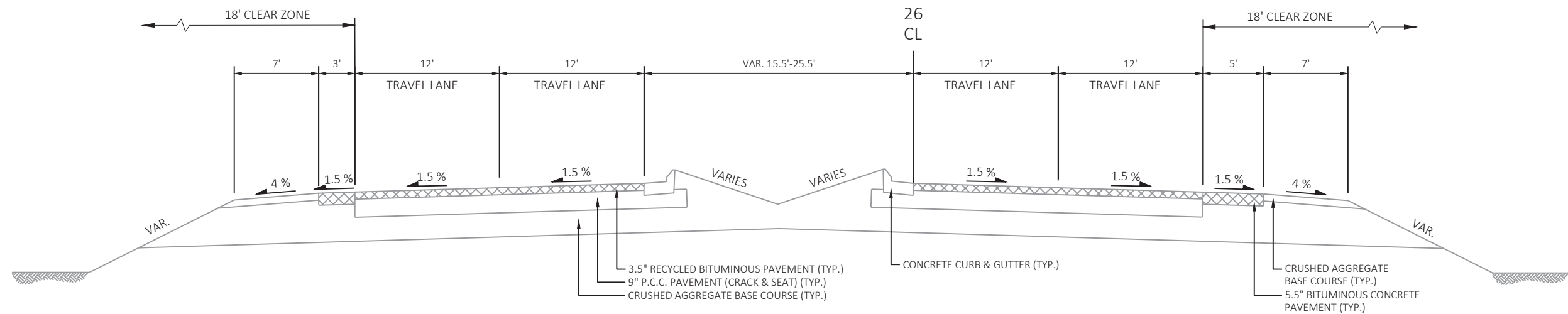
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EXISTING TYPICAL SECTION

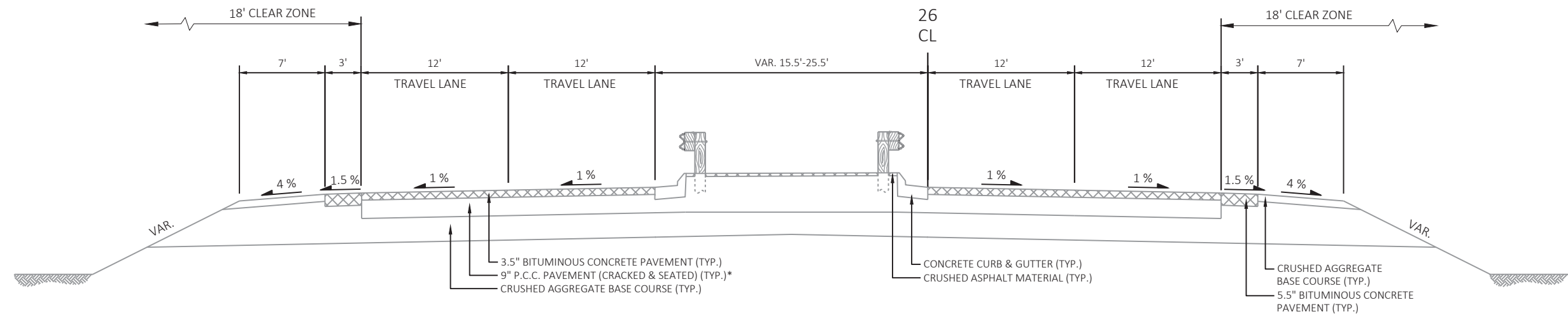
STA 590+68.23 - 595+64.40
STA 609+17.50 - 615+04.06



EXISTING TYPICAL SECTION

STA 595+64.40 - 603+55.00

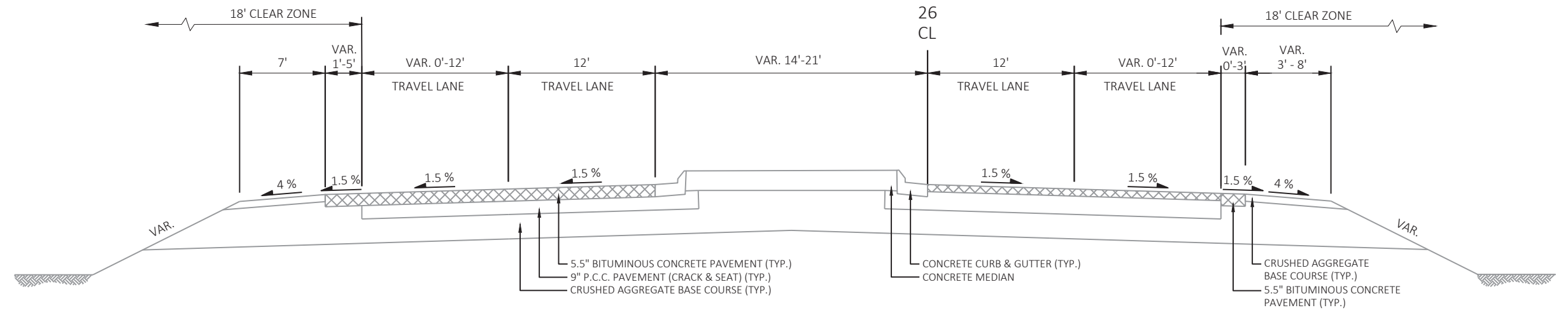
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EXISTING TYPICAL SECTION
STA 603+55.00 - 609+17.50

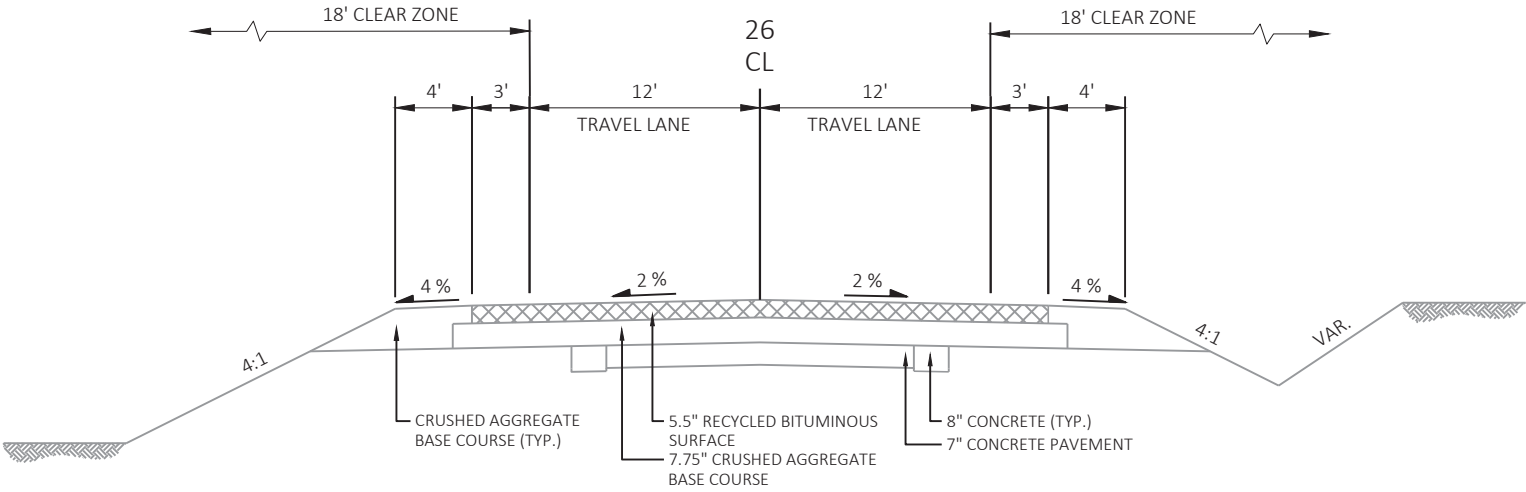
*NOT CRACKED & SEATED
STA 604+95.00 - STA 609+40.00

EXISTING GUARDRAIL
STA 605+81.87 - 608+83.83 LT
STA 604+72.29 - 607+74.51 RT

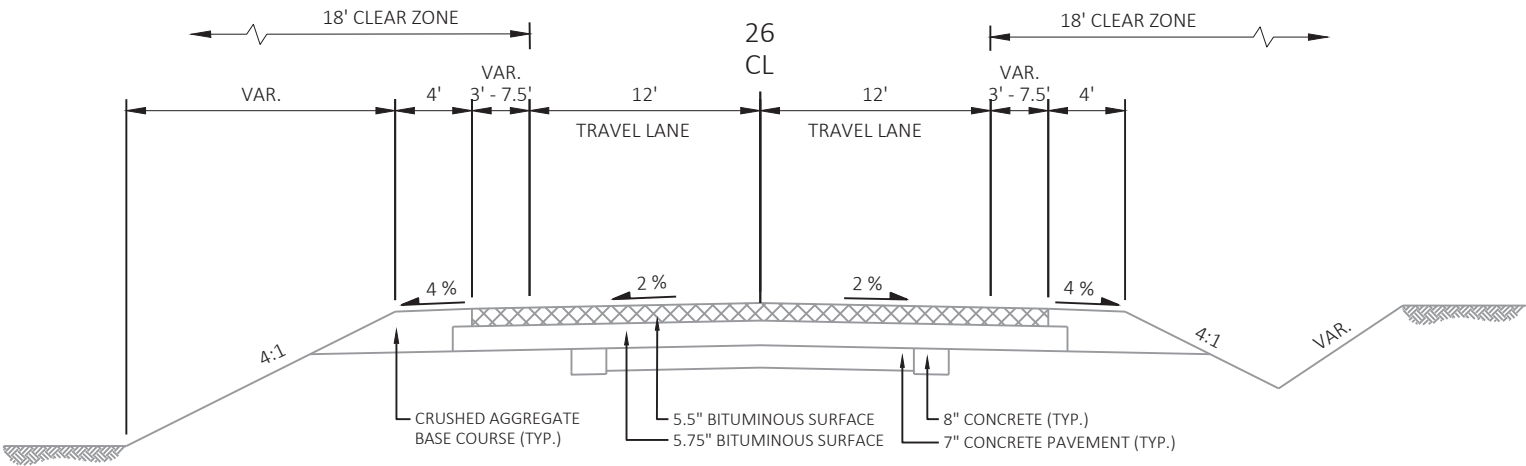


EXISTING TYPICAL SECTION
STA 615+04.06 - 633+60.39

DESIGN NOTES
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EXISTING TYPICAL SECTION
STA 633+60.39 - 682+06.64



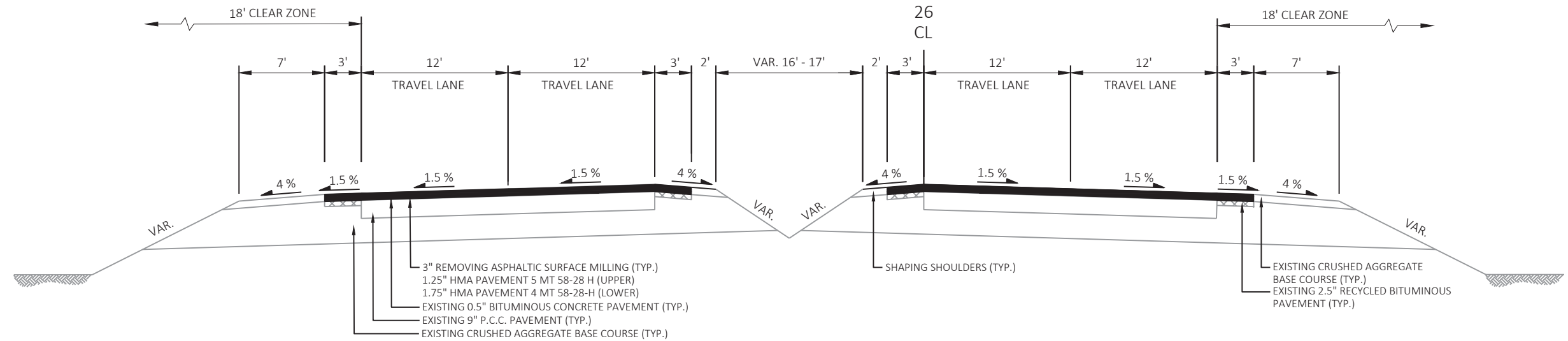
EXISTING TYPICAL SECTION
STA 678+00.00 - 682+06.64

1. ALL EXISTING PAVEMENT THICKNESSES ARE APPROXIMATE, REMOVALS OF CONCRETE PAVEMENT, BASE, OR OTHER MATERIALS WITHIN PLANNED DEPTHS OF REMOVING ASPHALTIC SURFACING MILLING IS INCIDENTAL.
2. TREAT UNDERLYING CONCRETE PAVEMENT WITH BASE PATCHING CONCRETE FROM STA 585+72.37 - STA 590+68.23, NO DOWELS REQUIRED IN AREAS OF CRACK AND SEAT.
3. REMOVING DISTRESSED PAVEMENT MILLING IN AREAS OF REMAINING ASPHALTIC PAVEMENT FROM STA 580+00.00 - STA 585+72.37.
4. REMOVE MINOR AREAS OF DETERIORATED REMAINING ASPHALTIC PAVEMENT AND/OR CONCRETE UNDER PREPARE FOUNDATION FOR ASPHALTIC PAVING FILLING VOIDS WITH ASPHALTIC SURFACE PATCHING AS DETERMINED BY THE ENGINEER IN THE FIELD.
5. ELIMINATE DROP-OFF GREATER THAN 2" PER STANDARD SPEC 104.6.1.2.3 INCIDENTAL TO MILLING AND PAVING OPERATIONS.

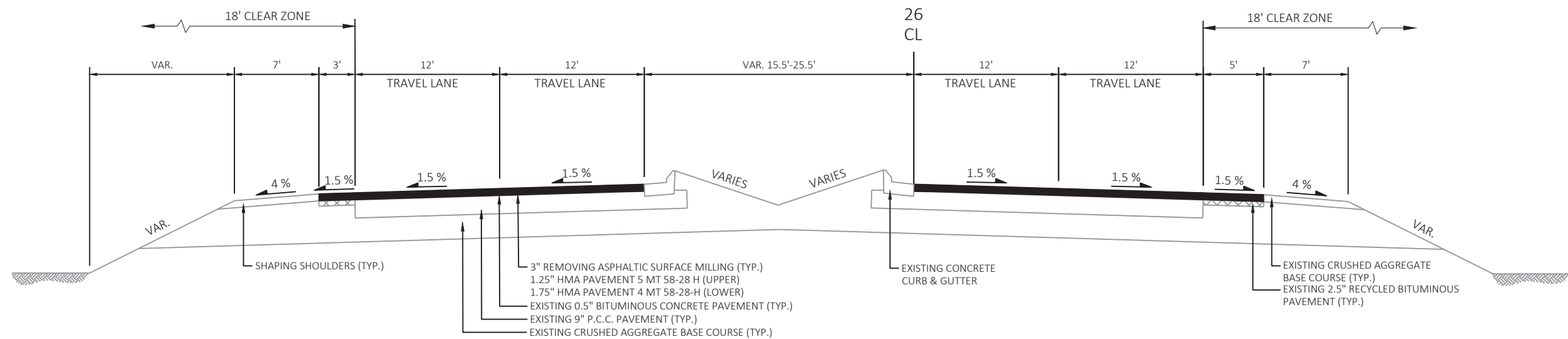


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2. TREAT UNDERLYING CONCRETE PAVEMENT WITH BASE PATCHING CONCRETE, NO DOWELS REQUIRED IN AREAS OF CRACK AND SEAT.
3. REMOVE MINOR AREAS OF DETERIORATED REMAINING ASPHALTIC PAVEMENT AND/OR CONCRETE UNDER PREPARE FOUNDATION FOR ASPHALTIC PAVING FILLING VOIDS WITH ASPHALTIC SURFACE PATCHING AS DETERMINED BY THE ENGINEER IN THE FIELD.
5. ELIMINATE DROP-OFF GREATER THAN 2" PER STANDARD SPEC 104.6.1.2.3 INCIDENTAL TO MILLING AND PAVING OPERATIONS.

**FINISHED TYPICAL SECTION**

STA 590+68.23 - 595+64.40
STA 609+17.50 - 615+04.06

**FINISHED TYPICAL SECTION**

STA 595+64.40 - 603+55.00

PROJECT NO: 6235-01-73

HWY: STH 26

COUNTY: DODGE

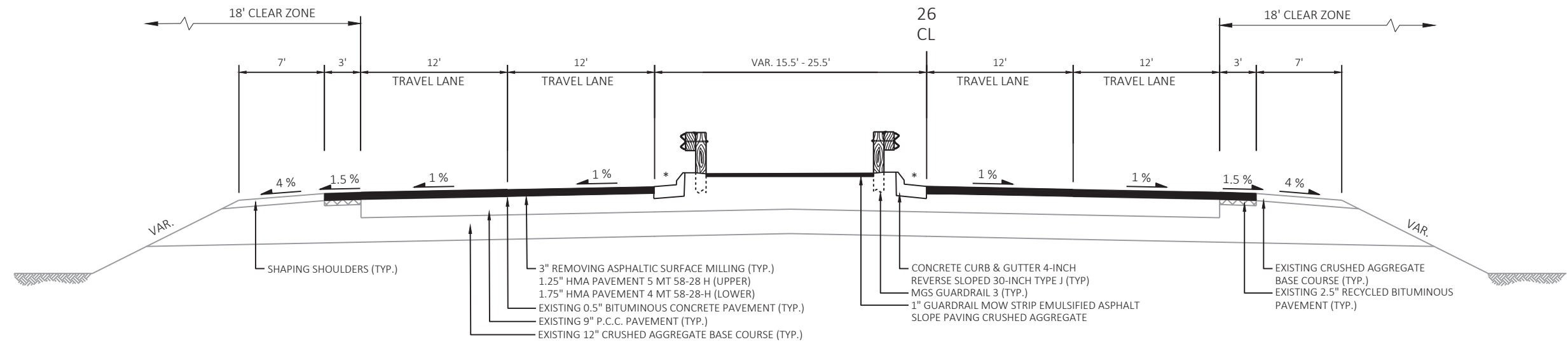
TYPICAL SECTION

SHEET

E

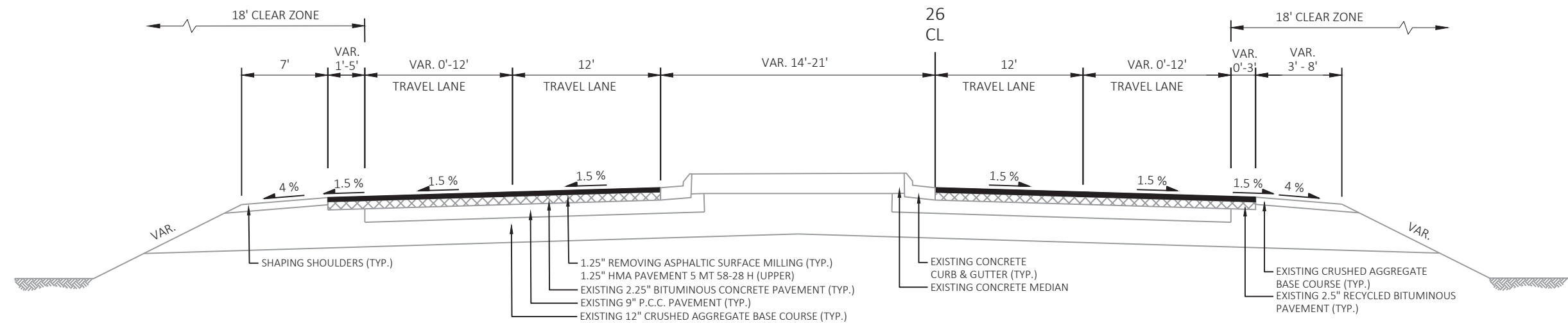
DESIGN NOTES

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2. TREAT UNDERLYING CONCRETE PAVEMENT WITH BASE PATCHING CONCRETE, NO DOWELS REQUIRED IN AREAS OF CRACK AND SEAT. NO CRACK AND SEAT (REQUIRES DOWELING) FROM STA 604+95.00 - STA 609+40.00. NO BASE PATCHING CONCRETE FROM STA 616+92.61 - STA 633+60.39.
3. REMOVING DISTRESSED PAVEMENT MILLING IN AREAS OF REMAINING ASPHALTIC PAVEMENT FROM STA 616+92.61 - STA 633+60.39.
4. REMOVE MINOR AREAS OF DETERIORATED REMAINING ASPHALTIC PAVEMENT AND/OR CONCRETE UNDER PREPARE FOUNDATION FOR ASPHALTIC PAVING FILLING VOIDS WITH ASPHALTIC SURFACE PATCHING AS DETERMINED BY THE ENGINEER IN THE FIELD.
5. SEE GUARDRAIL LAYOUT DETAIL AND CROSS SECTIONS FOR ADDITIONAL INFORMATION TO CONSTRUCT MGS GUARDRAIL 3.
5. ELIMINATE DROP-OFF GREATER THAN 2" PER STANDARD SPEC 104.6.1.2.3 INCIDENTAL TO MILLING AND PAVING OPERATIONS.



FINISHED TYPICAL SECTION
STA 603+55.00 - 609+17.50

* PROPOSED GUTTER CROSS SLOPE IS 4.0%



FINISHED TYPICAL SECTION
STA 615+04.06 - 633+60.39

DO NOT PAVE:
STH 26 NORTHBOUND LEFT TURN LANE
STA 621+73.61 - 625+43.73

PROJECT NO: 6235-01-73

HWY: STH 26

COUNTY: DODGE

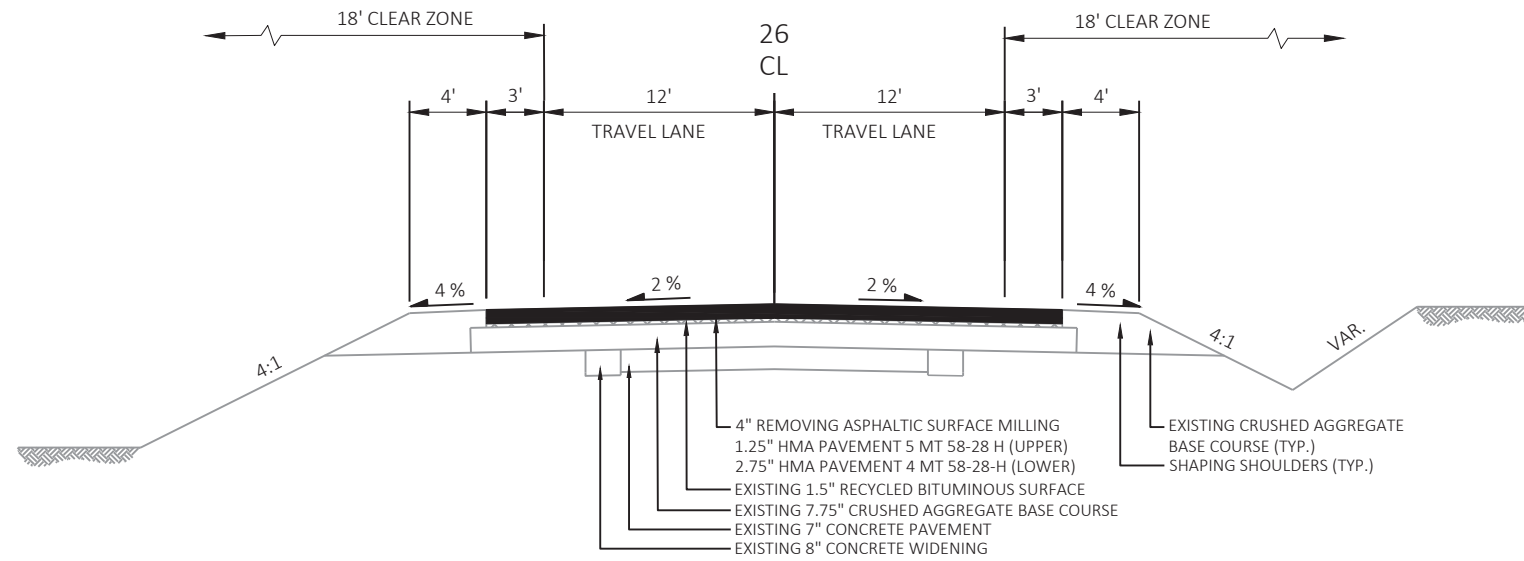
TYPICAL SECTION

SHEET

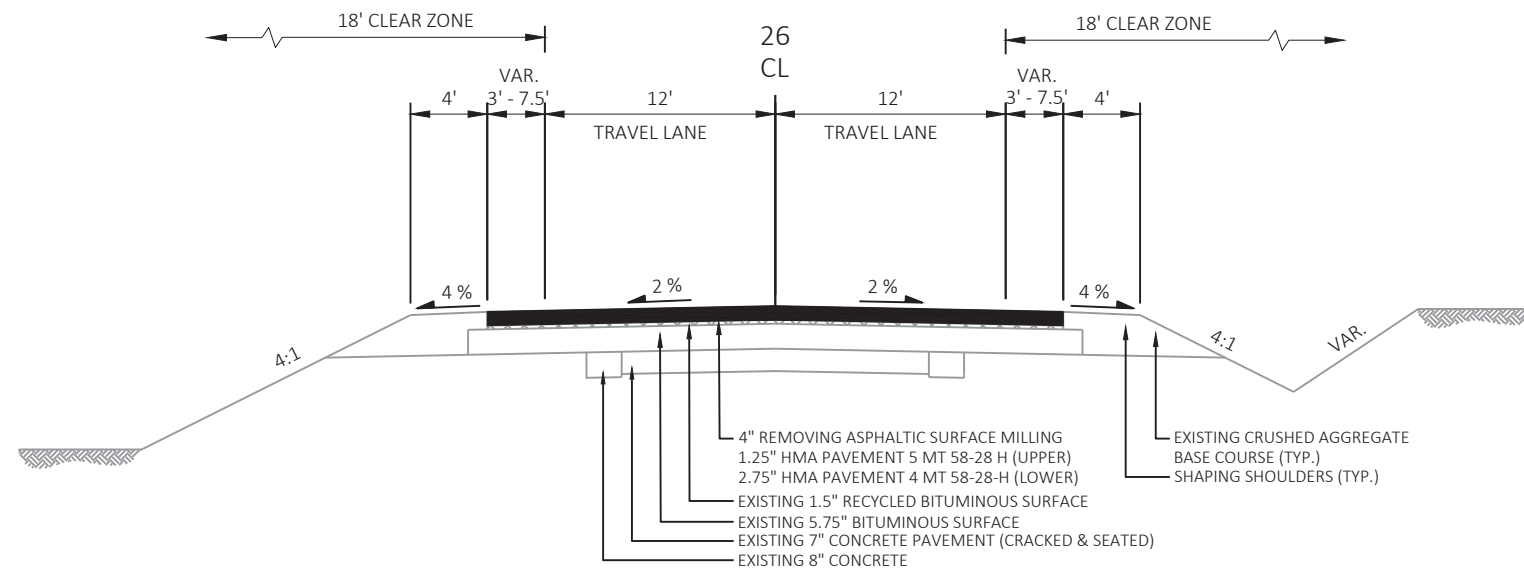
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1. ALL EXISTING PAVEMENT THICKNESSES ARE APPROXIMATE, REMOVALS OF CONCRETE PAVEMENT, BASE, OR OTHER MATERIALS WITHIN PLANNED DEPTHS OF REMOVING ASPHALTIC SURFACING MILLING IS INCIDENTAL.
2. REMOVING DISTRESSED PAVEMENT MILLING IN AREAS OF REMAINING ASPHALTIC PAVEMENT
3. REMOVE MINOR AREAS OF DETERIORATED REMAINING ASPHALTIC PAVEMENT AND/OR CONCRETE PAVEMENT UNDER PREPARE FOUNDATION FOR ASPHALTIC PAVING FILLING VOIDS WITH ASPHALTIC SURFACE PATCHING AS DETERMINED BY THE ENGINEER IN THE FIELD.
5. ELIMINATE DROP-OFF GREATER THAN 2" PER STANDARD SPEC 104.6.1.2.3 INCIDENTAL TO MILLING AND PAVING OPERATIONS.

**FINISHED TYPICAL SECTION**

STA 633+60.39 - 682+06.64

**FINISHED TYPICAL SECTION**

STA 678+00.00 - 682+06.64

PROJECT NO: 6235-01-73

HWY: STH 26

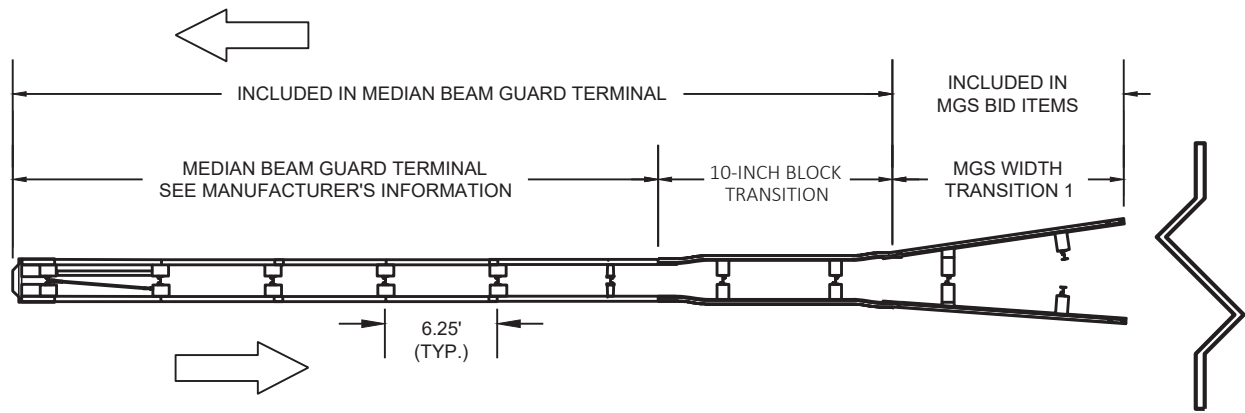
COUNTY: DODGE

TYPICAL SECTION

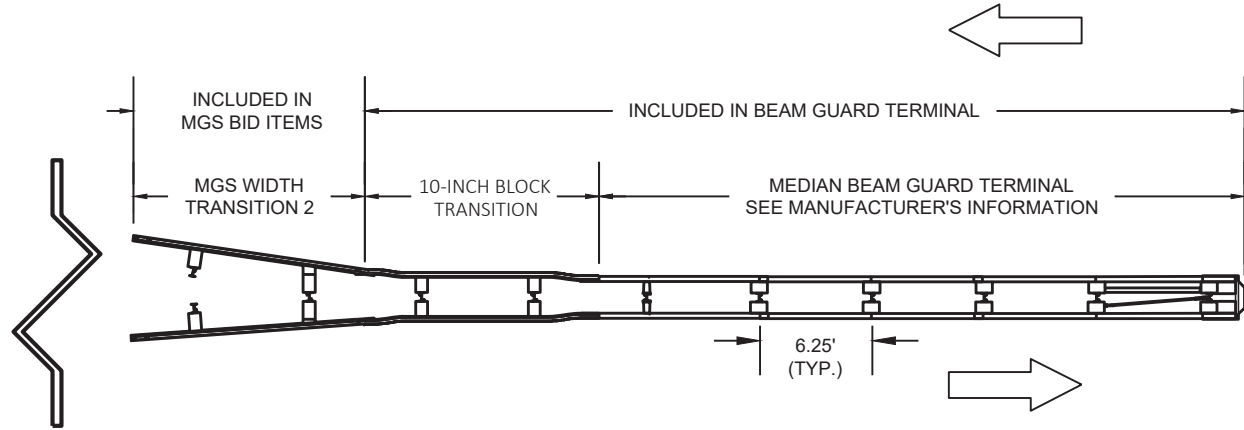
SHEET

E

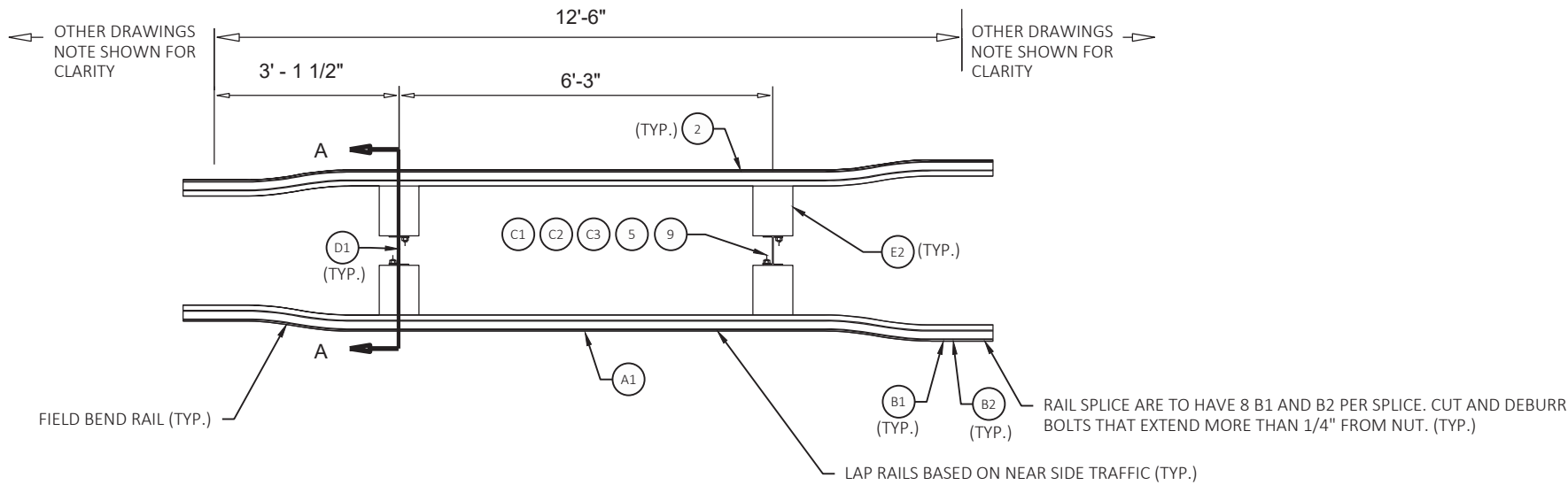
DESIGN NOTES
1. SEE SDD 14B42 AND STANDARD SPECIFICATION 614 FOR MORE INFORMATION.



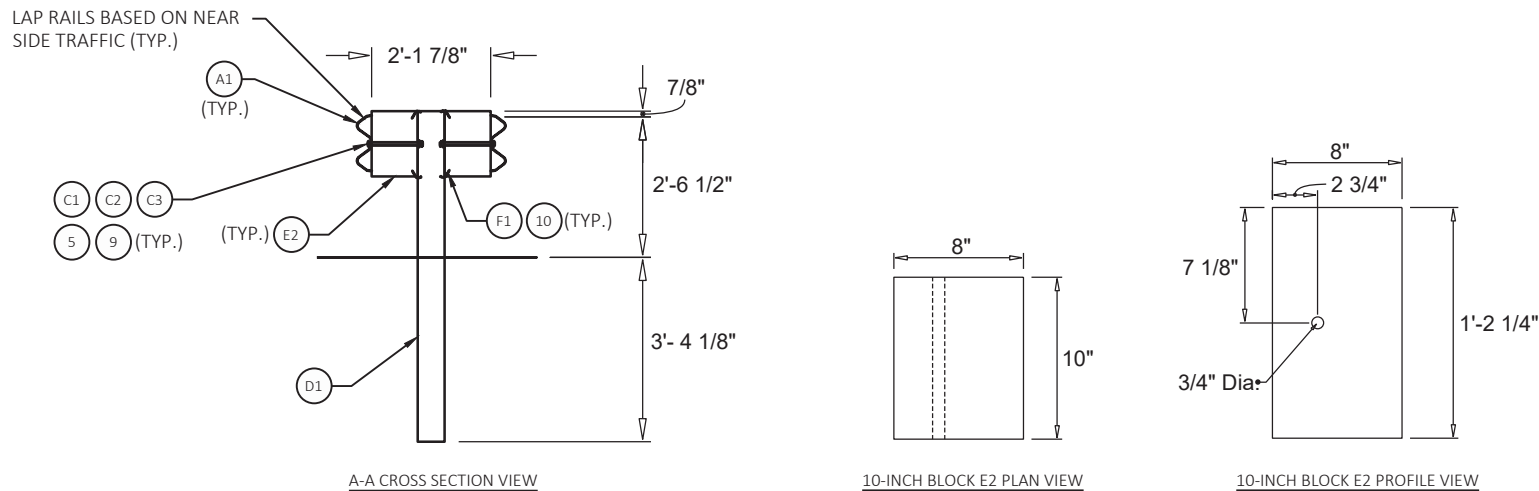
TRANSITION LOCATION 1 PLAN VIEW



TRANSITION LOCATION 2 PLAN VIEW

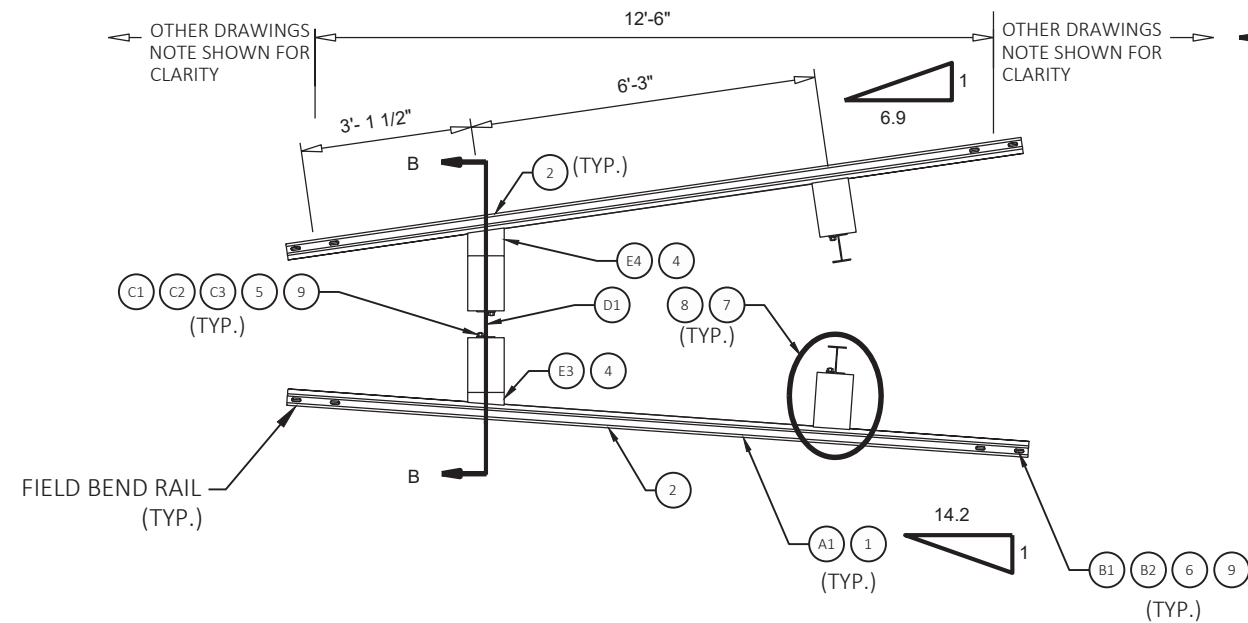


10-INCH BLOCK TRANSITION PLAN VIEW
MIRROR DRAWING FOR TRANSITION LOCATION 2

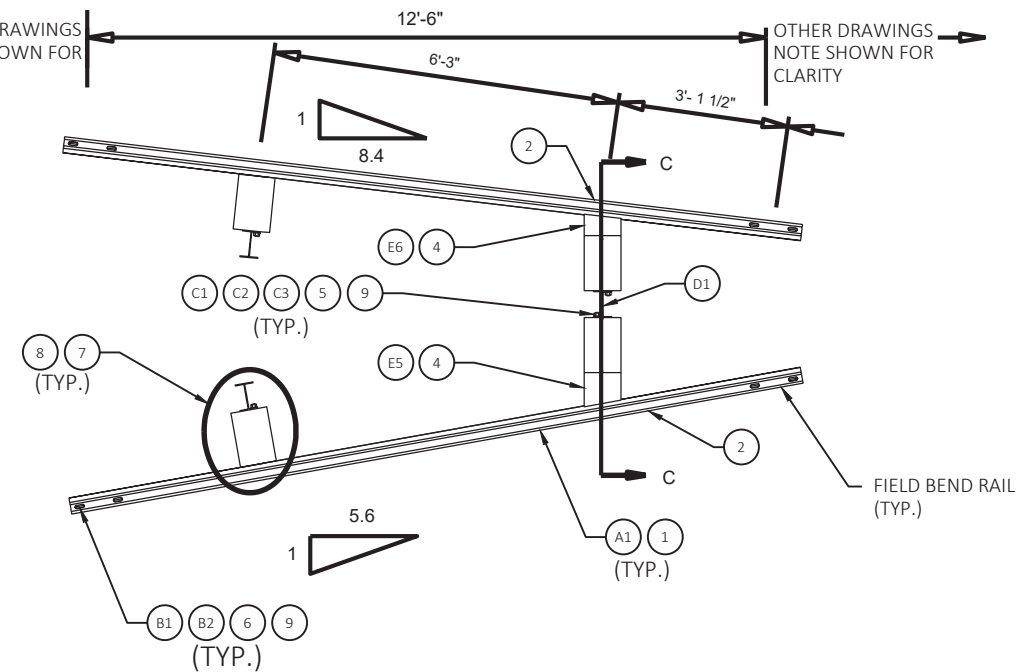


- 2 DRILL OR MODIFIED HOLES IN RAIL TO ACCOMMODATE BOLT PLACEMENT.
- 3 DIMENSIONS ARE ESTIMATED. CONTRACTOR IS TO FIELD VERIFY THESE DIMENSIONS PRIOR TO CUTTING BLOCKS.
- 4 CUT THE BLOCK TO ACCOMMODATE RAIL'S FLARE RATE.
- 5 BOLT RAIL TO BLOCK AND POST USING UPSTREAM FLANGE OF POST.
- 6 RAIL SPLICE ARE TO HAVE 8 B1 AND B2 PER SPLICE.
- 7 STEEL OR WOOD POST ARE ACCEPTABLE AT THIS LOCATION. SEE SDD 14B42.
- 8 WOOD OR PLASTIC BLOCKS ARE ACCEPTABLE AT THIS LOCATION. SEE SDD 14B42.
- 9 CUT AND DEBURR BOLTS THAT EXTEND MORE THAN 1/4" FROM NUT. (TYP.)
- 10 INSTALL 1 NAIL AT EACH CORNER OF THE BLOCK AND BEND RAILS AROUND THE FLANGE OF POSTS

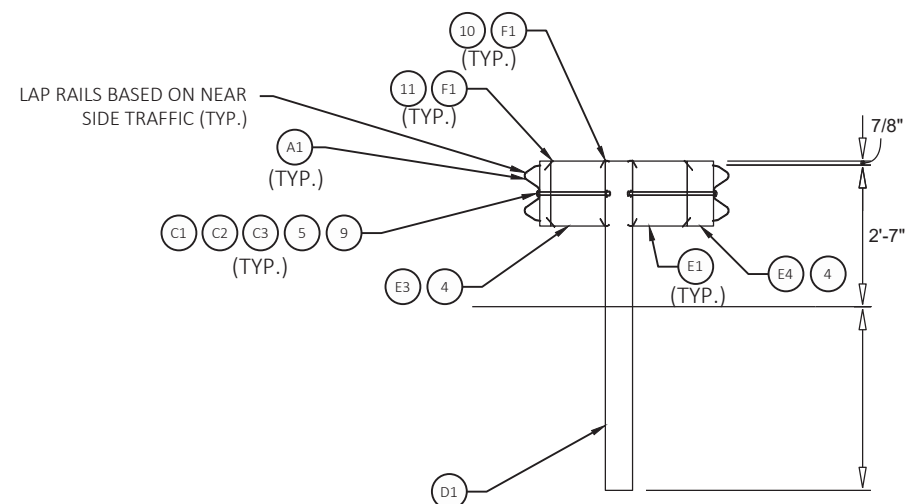
PART NUMBER	DESCRIPTION	MATERIALS	NOTES
A1	BEAM GUARD RAIL	AASHTO M180 CLASS A Type 2 12 GUAGE. SEE APPROVED PRODUCT LIST.	
B1	SPLICE BOLT	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GAL. TO AASHTO M298 CLASS 50 TYPE 1/ ASTM B695 CLASS 50 TYPE 1 UNC AASHTO M180 HEAD 5/8" ASTM A307 Grade B OR SAE J429 GRADE 2 OR ASTM F1554	5/8" Dia.
B2	SPLICE BOLT - NUT	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GAL. TO AASHTO M298 CLASS 50 TYPE 1/ ASTM B695 CLASS 50 TYPE 1 UNC OVER TAP NUTS AS SPECIFIED IN AASHTO 291/ASTM A 563 AASHTO M180 RECESSED HEAVY HEX HEAD 5/8" ASTM A563DH OR SAE J995 GRADE 5	
C1	POST BOLT	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GAL. TO AASHTO M298 CLASS 50 TYPE 1/ ASTM B695 CLASS 50 TYPE 1 UNC AASHTO M180 HEAD 5/8" ASTM A307 GRADE B OR SAE J429 GRADE 2 OR ASTM F1554	5/8" Dia.
C2	POST BOLT - WASHER	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GAL. TO AASHTO M298 CLASS 50 TYPE 1/ ASTM B695 CLASS 50 TYPE 1 F436 TYPE 1 (HARDEN WASHER TYPICAL USED WITH STEEL POSTS) OR ASTM F844 (UNHARDENED WASHER TYPICALLY USED WITH WOOD)	
C3	POST BOLT - NUT	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GAL. TO AASHTO M298 CLASS 50 TYPE 1/ ASTM B695 CLASS 50 TYPE 1 UNC OVER TAP NUTS AS SPECIFIED IN AASHTO 291/ASTM A 563 AASHTO M180 RECESSED HEAVY HEX HEAD ASTM A563DH OR SAE J995 GRADE 5	
D1	STEEL POST	AASHTO M111/ASTM A123, ASTM A6 W6x9 OR W6x8.5 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	
E1	MGS WOOD BLOCK	WISDOT SPEC. 614	
E2	MODIFIED MGS WOOD BLOCK 10-INCH	WISDOT SPEC. 614	
E3	MODIFIED MGS WOOD BLOCK	WISDOT SPEC. 614	
E4			
E5			
E6			
F1	GALVANIZE 16D NAIL	ASTM A153 HOT DIPPED CLASS D DOUBLE HEAD ASTM F1667 TYPE 1 STYLE 12 (16 DOUBLE HEADED)	



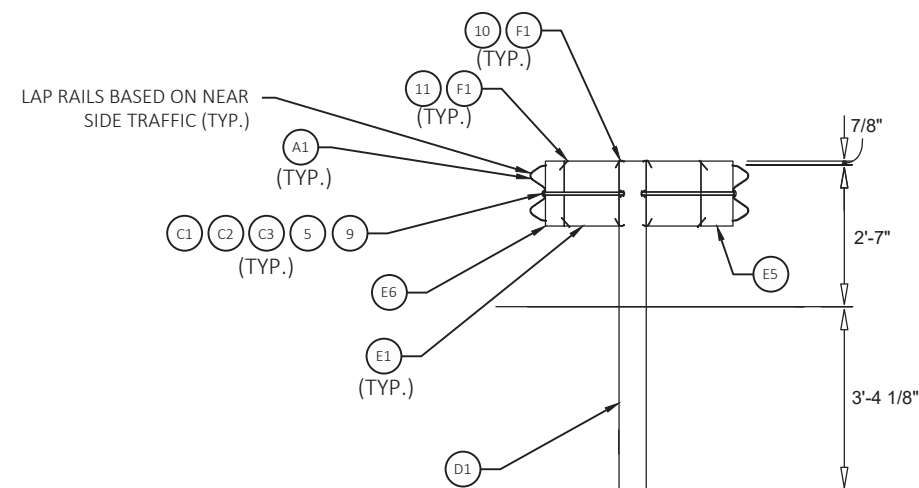
TRANSITION LOCATION 1 PLAN VIEW



TRANSITION LOCATION 2 PLAN VIEW

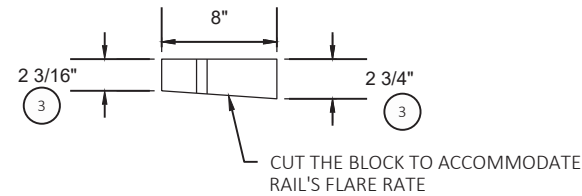


B-B CROSS SECTION VIEW

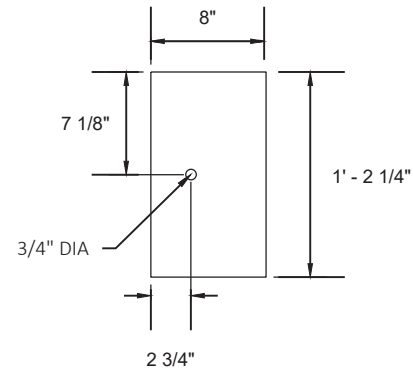


C-C CROSS SECTION VIEW

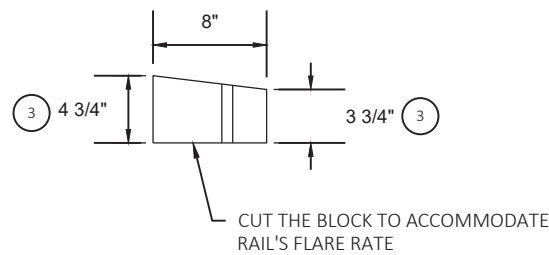
- 2 DRILL OR MODIFIED HOLES IN RAIL TO ACCOMMODATE BOLT PLACEMENT.
- 3 DIMENSIONS ARE ESTIMATED. CONTRACTOR IS TO FIELD VERIFY THESE DIMENSIONS PRIOR TO CUTTING BLOCKS.
- 4 CUT THE BLOCK TO ACCOMMODATE RAIL'S FLARE RATE.
- 5 BOLT RAIL TO BLOCK AND POST USING UPSTREAM FLANGE OF POST.
- 6 RAIL SPLICE ARE TO HAVE 8 B1 AND B2 PER SPLICE.
- 7 STEEL OR WOOD POST ARE ACCEPTABLE AT THIS LOCATION. SEE SDD 14B42.
- 8 WOOD OR PLASTIC BLOCKS ARE ACCEPTABLE AT THIS LOCATION. SEE SDD 14B42.
- 9 CUT AND DEBURR BOLTS THAT EXTEND MORE THAN 1/4" FROM NUT. (TYP.)
- 10 INSTALL 1 NAIL AT EACH CORNER OF THE BLOCK AND BEND RAILS AROUND THE FLANGE OF POSTS



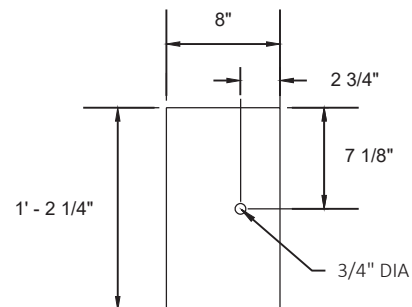
BLOCK E3 PLAN VIEW



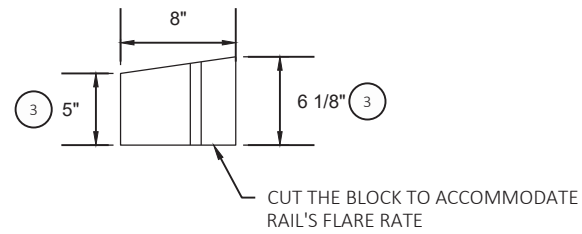
BLOCK E3 PROFILE VIEW



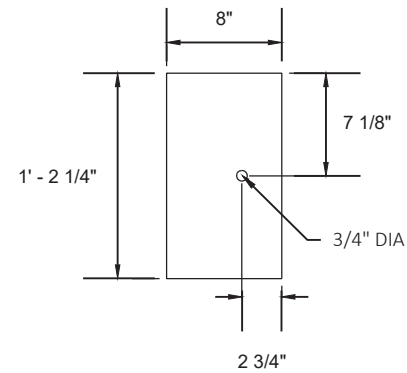
BLOCK E6 PLAN VIEW



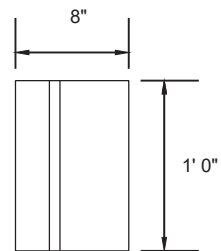
BLOCK E6 PROFILE VIEW



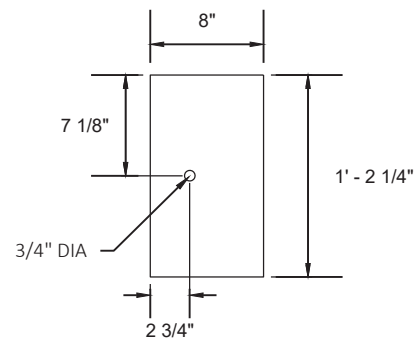
BLOCK E4 PLAN VIEW



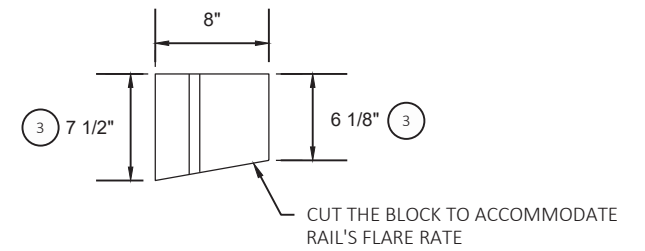
BLOCK E4 PROFILE VIEW



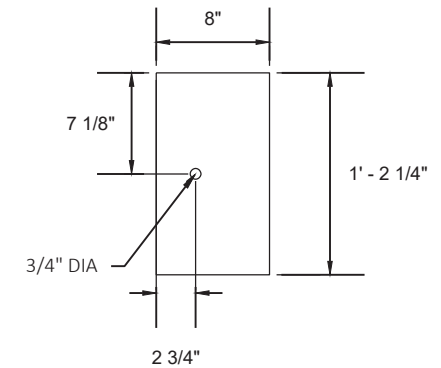
BLOCK E1 PLAN VIEW



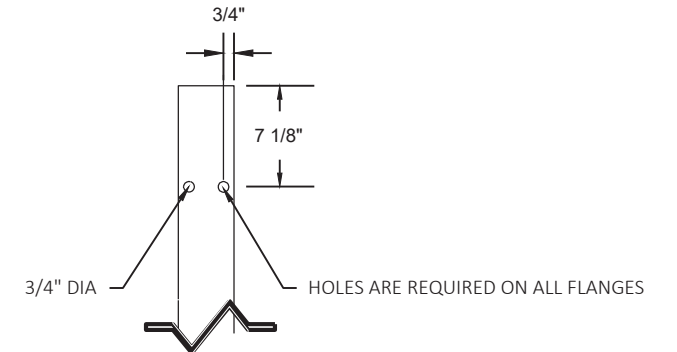
BLOCK E1 PROFILE VIEW



BLOCK E5 PLAN VIEW



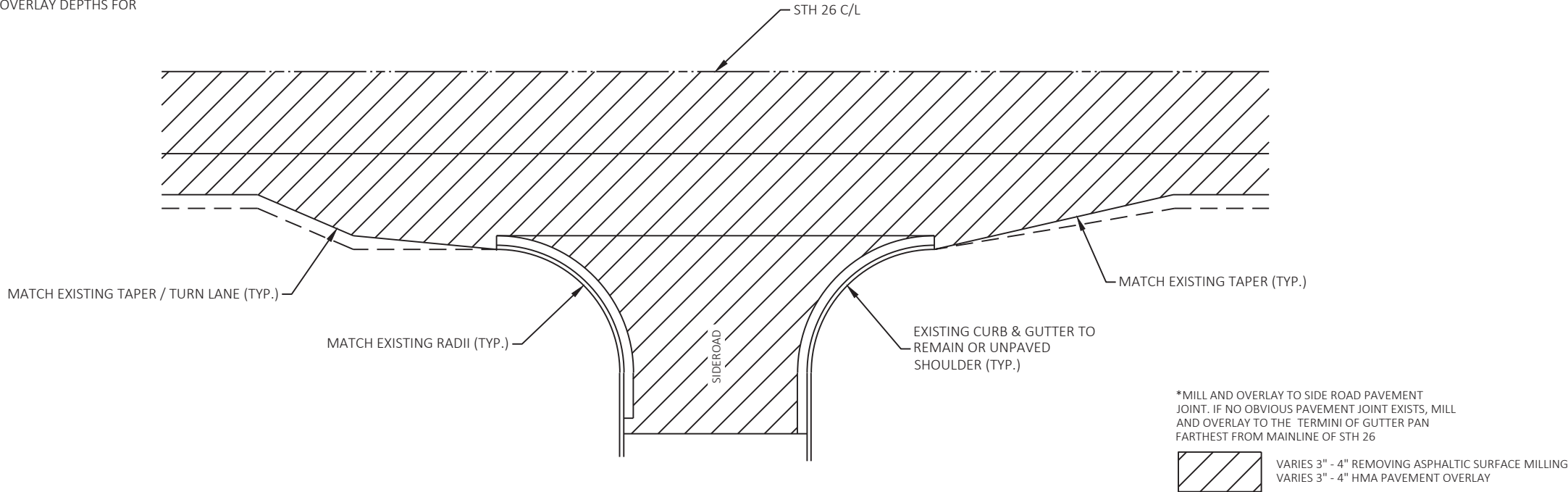
BLOCK E5 PROFILE VIEW



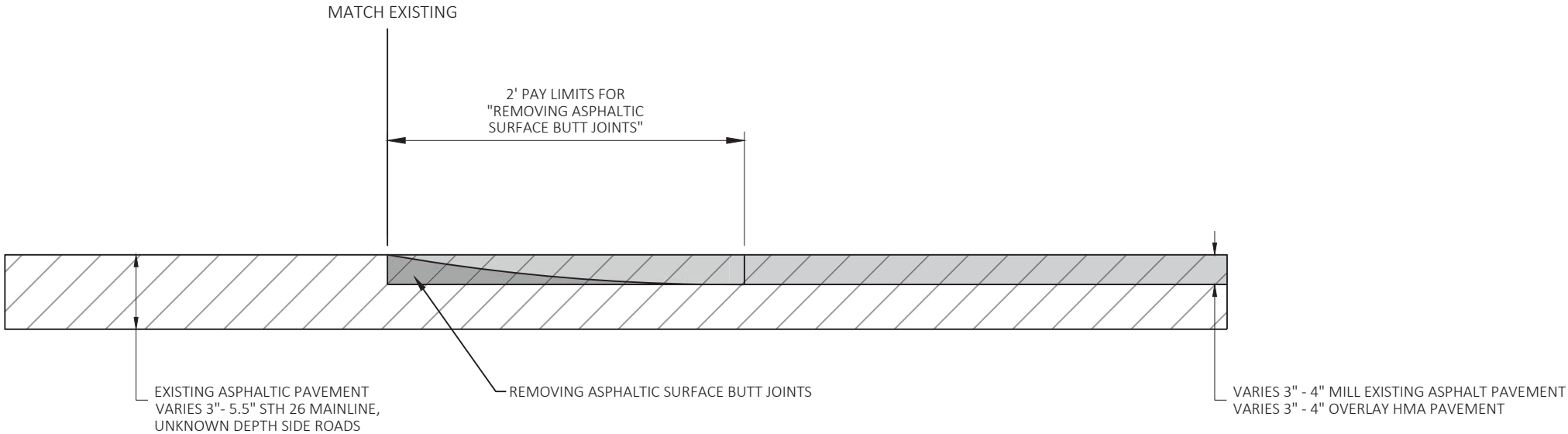
POST D1 DETAIL VIEW

3 DIMENSIONS ARE ESTIMATED. CONTRACTOR IS TO FIELD VERIFY THESE DIMENSIONS PRIOR TO CUTTING BLOCKS.

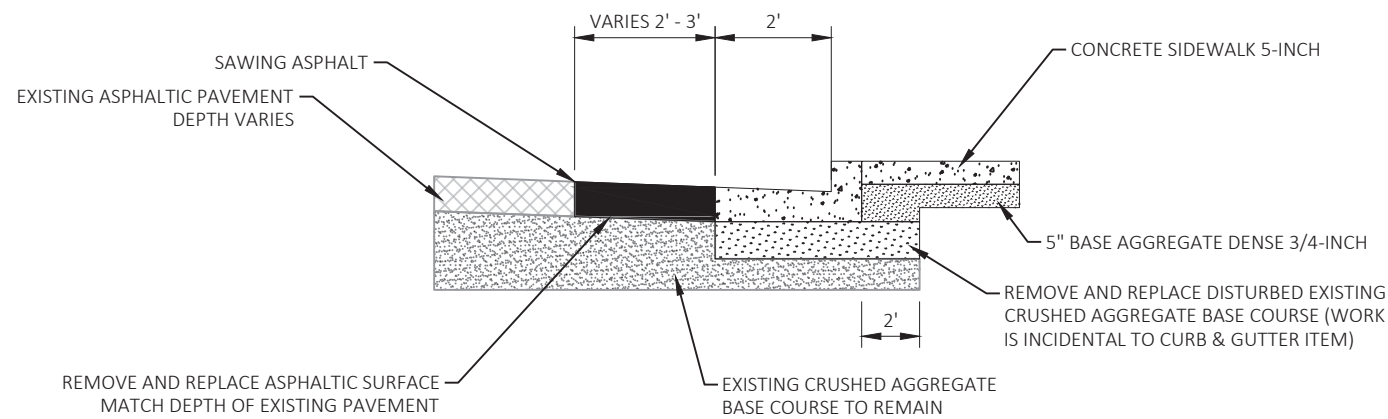
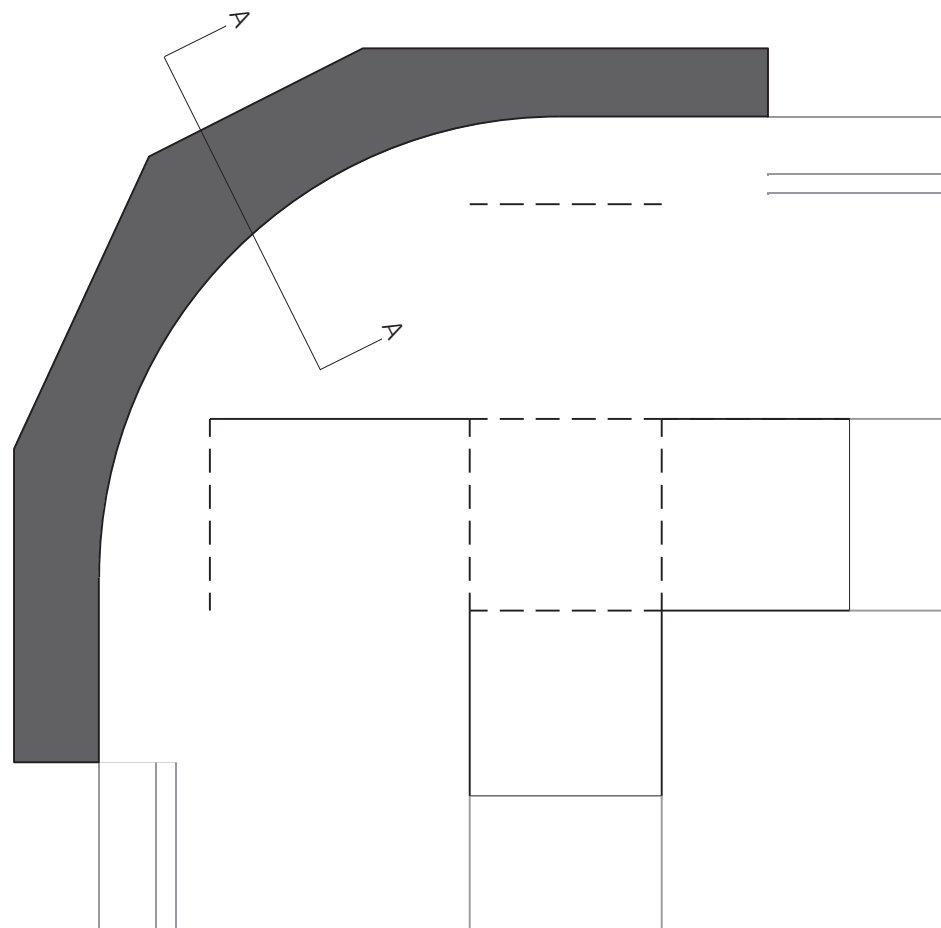
- DESIGN NOTES:
1. ANY SAWCUT USED IN THIS OPERATION IS CONSIDERED INCIDENTAL TO BUTT JOINT ITEM.
 2. SEE FINISHED TYPICAL SECTIONS FOR SPECIFIC MILL & OVERLAY DEPTHS FOR SPECIFIC LOCATIONS.



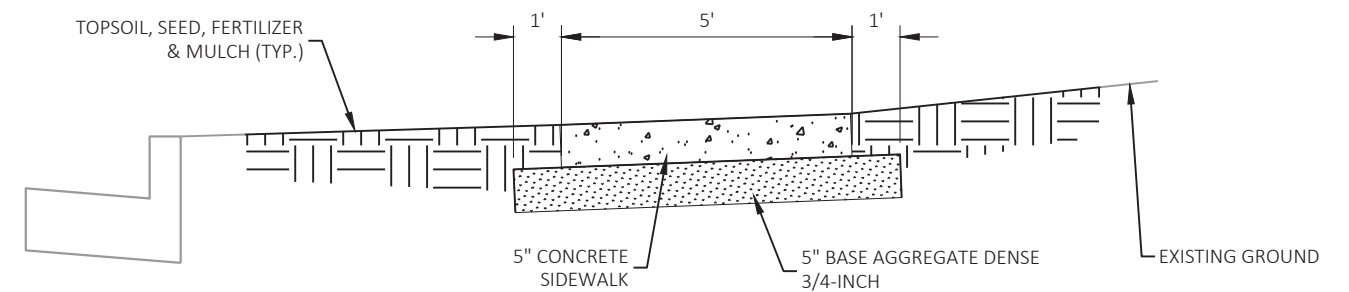
TYPICAL SIDEROAD PAVING LIMITS
MILLIGAN RD, MILLIGAN RD, BARNES ST, KUENZI DR, BUWALDA DR, INDUSTRIAL DR,
CLAGGETT AVE, MAYFAIR ST, LIBBY ST



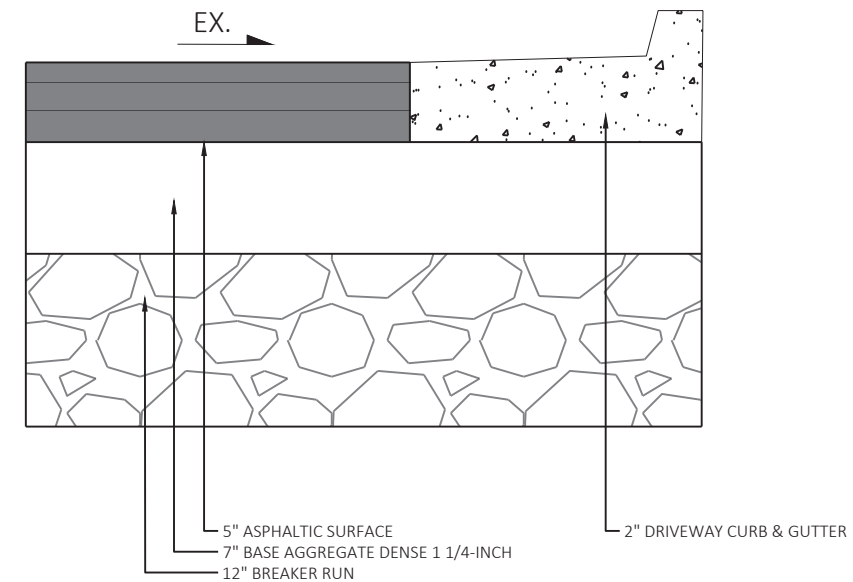
BUTT JOINT TO MATCH EXISTING - STH 26, SIDE ROADS

**SECTION A-A****CURB AND GUTTER REPLACEMENT**
ADJACENT TO ASPHALTIC PAVEMENT**DESIGN NOTES**

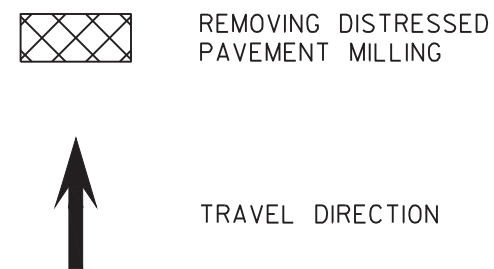
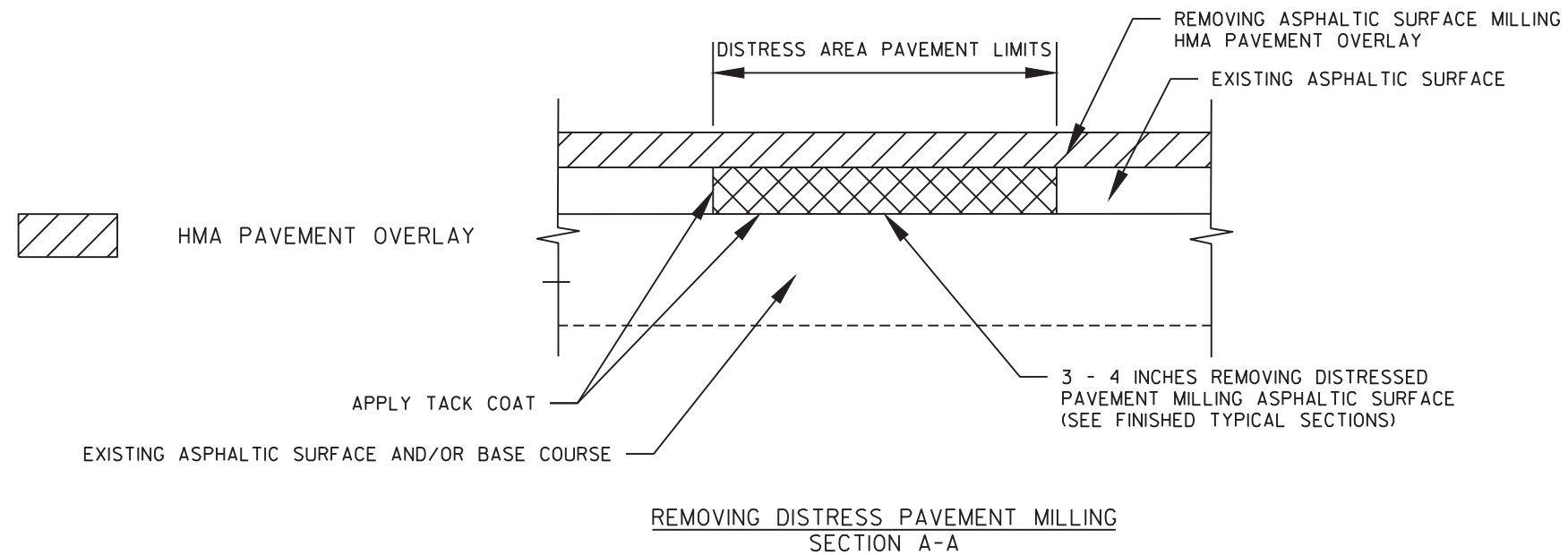
1. SEE CURB RAMP DETAILS FOR GUTTER PAN SLOPES.
2. INSTALL TWO DRILLED TIE BARS AT EACH CONNECTION TO EXISTING CURB AND GUTTER.

**TYPICAL SIDEWALK SECTION****DESIGN NOTES**

1. PAYMENT FOR ALL TOPSOIL, SEED, FERTILIZER, AND MULCH SHALL BE PAID FOR USING STANDARD BID ITEMS.
2. PAYMENT FOR CONCRETE SIDEWALK AND BASE AGGREGATE DENSE SHALL BE PAID FOR USING STANDARD BID ITEMS.

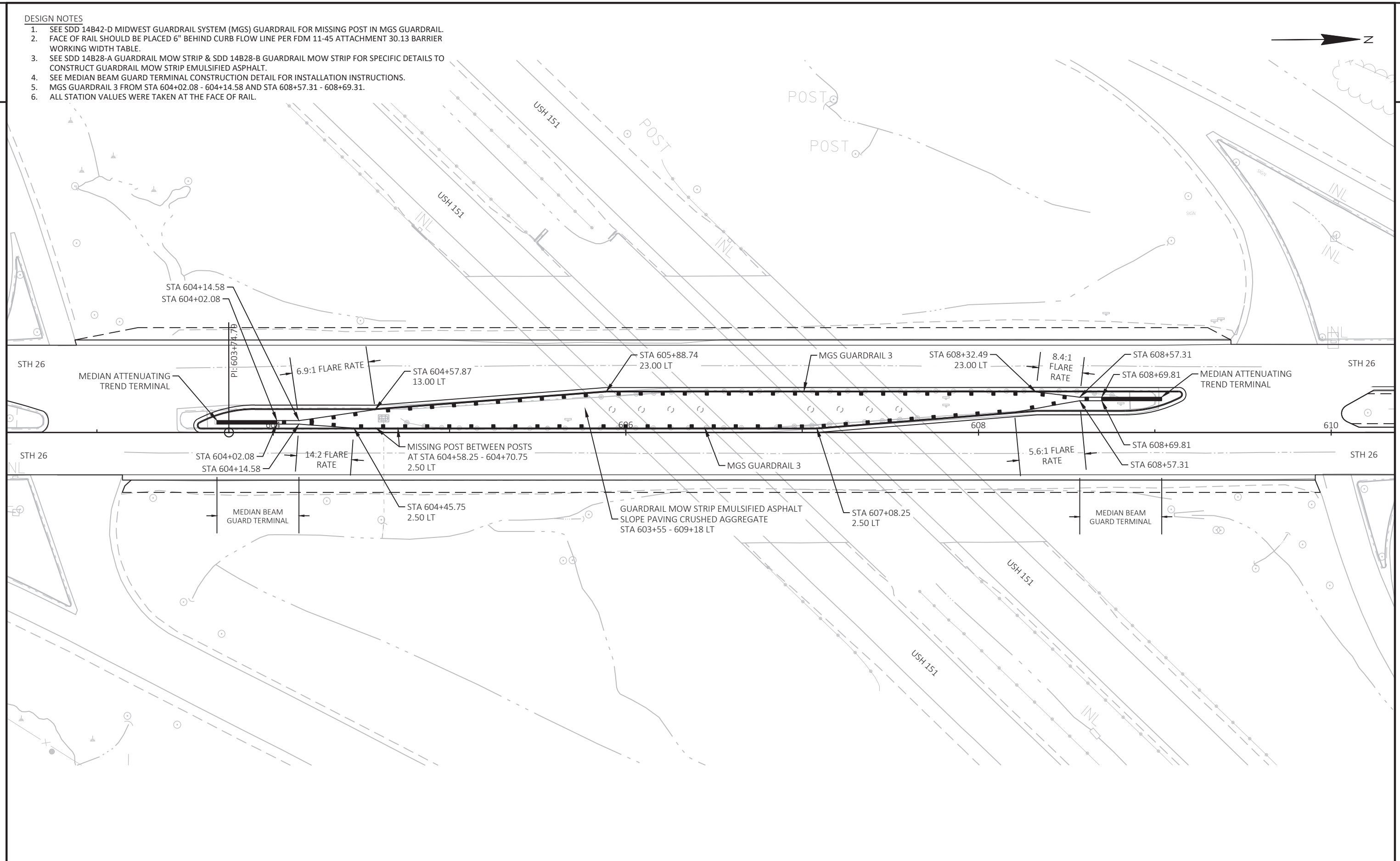
**STH 26 SB LEFT TURN LANE**
MEDIAN ISLAND REPLACEMENT DETAIL**DESIGN NOTES**

1. MATCH EXISTING SLOPE TO MATCH ELEVATIONS OF PROPOSED CURB & GUTTER SHOWN ON INTERSECTION DETAILS AND PAVEMENT GRADES SHEET.
2. THE 5" ASPHALTIC SURFACE CONSTRUCTION OPERATION MUST BE COMPLETED PRIOR TO RESURFACING OPERATIONS. FOLLOW FINISHED TYPICAL SECTION FOR RESURFACING OPERATIONS.



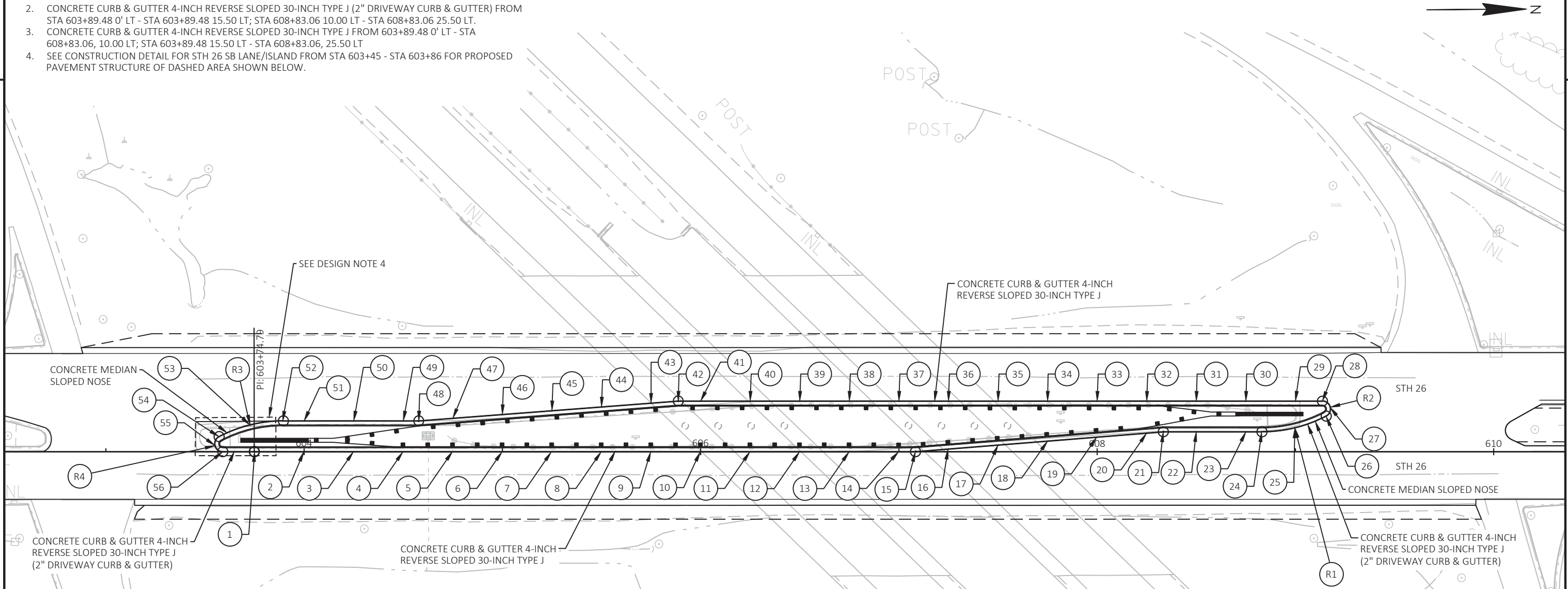
EXACT LOCATION AND LIMITS OF REMOVING DISTRESSED PAVEMENT MILLING TO BE DETERMINED BY THE ENGINEER IN THE FIELD

- DESIGN NOTES
1. SEE SDD 14B42-D MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL FOR MISSING POST IN MGS GUARDRAIL.
 2. FACE OF RAIL SHOULD BE PLACED 6" BEHIND CURB FLOW LINE PER FDM 11-45 ATTACHMENT 30.13 BARRIER WORKING WIDTH TABLE.
 3. SEE SDD 14B28-A GUARDRAIL MOW STRIP & SDD 14B28-B GUARDRAIL MOW STRIP FOR SPECIFIC DETAILS TO CONSTRUCT GUARDRAIL MOW STRIP EMULSIFIED ASPHALT.
 4. SEE MEDIAN BEAM GUARD TERMINAL CONSTRUCTION DETAIL FOR INSTALLATION INSTRUCTIONS.
 5. MGS GUARDRAIL 3 FROM STA 604+02.08 - 604+14.58 AND STA 608+57.31 - 608+69.31.
 6. ALL STATION VALUES WERE TAKEN AT THE FACE OF RAIL.



PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	GUARDRAIL LAYOUT DETAIL	SHEET	E
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- DESIGN NOTES
- 1. EXISTING MEDIAN INLET TO REMAIN
 - 2. CONCRETE CURB & GUTTER 4-INCH REVERSE SLOPED 30-INCH TYPE J (2" DRIVEWAY CURB & GUTTER) FROM STA 603+89.48 0' LT - STA 603+89.48 15.50 LT; STA 608+83.06 10.00 LT - STA 608+83.06 25.50 LT.
 - 3. CONCRETE CURB & GUTTER 4-INCH REVERSE SLOPED 30-INCH TYPE J FROM 603+89.48 0' LT - STA 608+83.06, 10.00 LT; STA 603+89.48 15.50 LT - STA 608+83.06, 25.50 LT
 - 4. SEE CONSTRUCTION DETAIL FOR STH 26 SB LANE/ISLAND FROM STA 603+45 - STA 603+86 FOR PROPOSED PAVEMENT STRUCTURE OF DASHED AREA SHOWN BELOW.

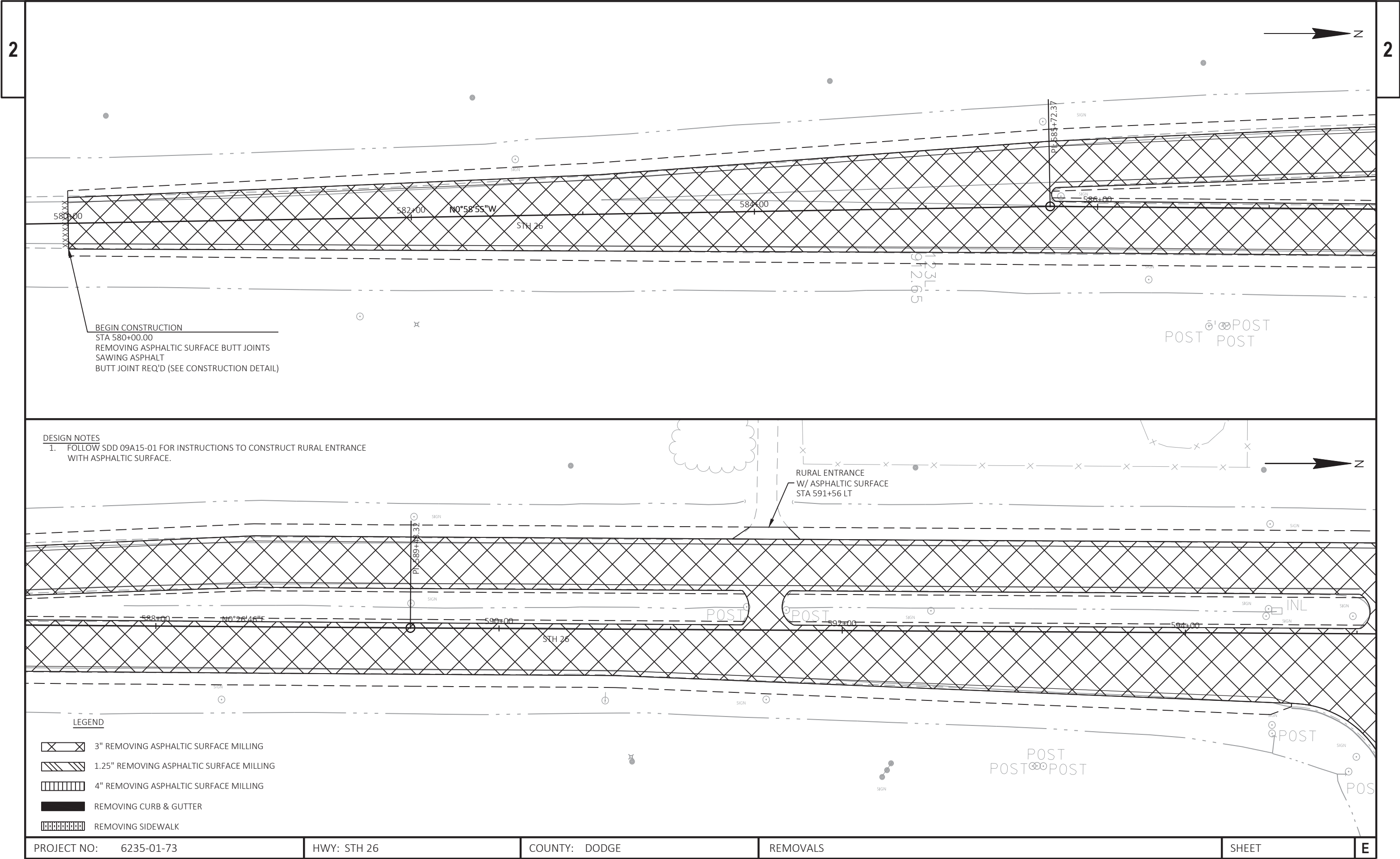


POINT	STA	OFFSET	ELEV.	RADIUS
1	603+75.00	0.00 LT	903.34	N/A
2	604+00.00	0.00 LT	903.36	N/A
3	604+25.00	0.00 LT	903.38	N/A
4	604+50.00	0.00 LT	903.40	N/A
5	604+75.00	0.00 LT	903.42	N/A
6	605+00.00	0.00 LT	903.44	N/A
7	605+25.00	0.00 LT	903.46	N/A
8	605+50.00	0.00 LT	903.48	N/A
9	605+75.00	0.00 LT	903.50	N/A
10	606+00.00	0.00 LT	903.52	N/A
11	606+25.00	0.00 LT	903.54	N/A
12	606+50.00	0.00 LT	903.56	N/A
13	606+75.00	0.00 LT	903.58	N/A
14	607+00.00	0.00 LT	903.60	N/A
15	607+08.35	0.00 LT	903.62	N/A

POINT	STA	OFFSET	ELEV.	RADIUS
16	607+25.00	1.33 LT	903.69	N/A
17	607+50.00	3.33 LT	903.78	N/A
18	607+75.00	5.33 LT	903.87	N/A
19	608+00.00	7.33 LT	903.97	N/A
20	608+25.00	9.33 LT	904.06	N/A
21	608+33.35	10.00 LT	904.09	N/A
22	608+50.00	10.00 LT	904.15	N/A
23	608+75.00	10.00 LT	904.25	N/A
24	608+83.06	10.00 LT	904.27	N/A
R1	608+83.06	80.00 LT	N/A	70
25	608+99.74	12.02 LT	904.26	70
26	609+15.47	17.96 LT	904.25	N/A
R2	609+13.62	21.50 LT	N/A	4
27	609+17.50	22.45 LT	904.27	4
28	609+13.62	25.50 LT	904.27	N/A

POINT	STA	OFFSET	ELEV.	RADIUS
29	609+00.00	25.50 LT	904.18	N/A
30	608+75.00	25.50 LT	904.10	N/A
31	608+50.00	25.50 LT	904.02	N/A
32	608+25.00	25.50 LT	903.95	N/A
33	608+00.00	25.50 LT	903.87	N/A
34	607+75.00	25.50 LT	903.79	N/A
35	607+50.00	25.50 LT	903.71	N/A
36	607+25.00	25.50 LT	903.63	N/A
37	607+00.00	25.50 LT	903.55	N/A
38	606+75.00	25.50 LT	903.47	N/A
39	606+50.00	25.50 LT	903.40	N/A
40	606+25.00	25.50 LT	903.32	N/A
41	606+00.00	25.50 LT	903.26	N/A
42	605+88.64	25.50 LT	903.25	N/A
43	605+75.00	24.46 LT	903.24	N/A

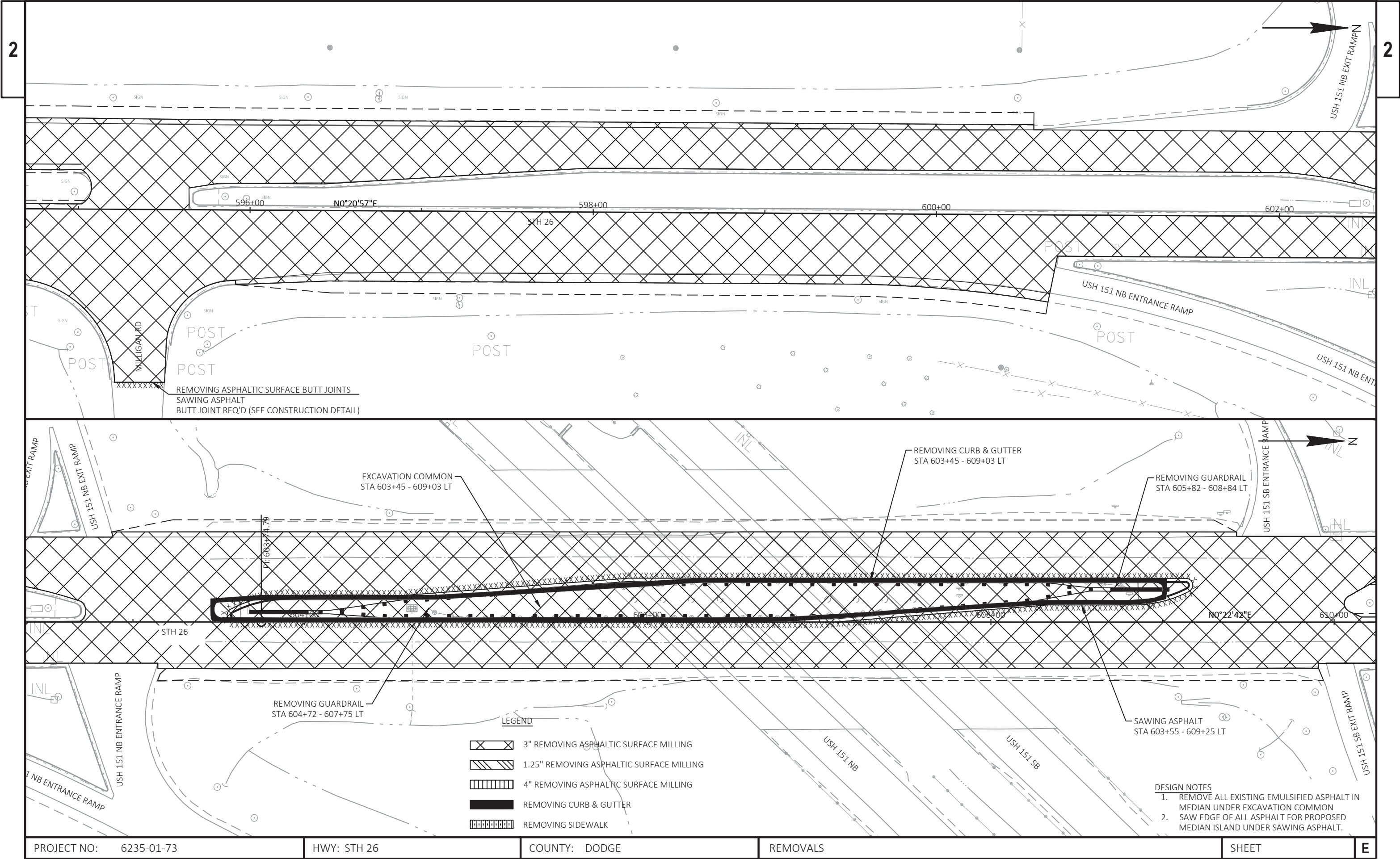
POINT	STA	OFFSET	ELEV.	RADIUS
44	605+50.00	22.55 LT	903.23	N/A
45	605+25.00	20.64 LT	903.21	N/A
46	605+00.00	18.73 LT	903.19	N/A
47	604+75.00	16.82 LT	903.17	N/A
48	604+57.77	15.50 LT	903.15	N/A
49	604+50.00	15.50 LT	903.16	N/A
50	604+25.00	15.50 LT	903.21	N/A
51	604+00.00	15.50 LT	903.25	N/A
52	603+89.48	15.50 LT	903.27	N/A
53	603+72.80	13.48 LT	903.29	N/A
R3	603+89.48	54.50 LT	N/A	70
54	603+57.06	7.55 LT	903.32	N/A
55	603+55.03	3.05 LT	903.33	N/A
R4	603+58.91	4.00 LT	N/A	4
56	603+58.91	0.00 LT	903.34	N/A



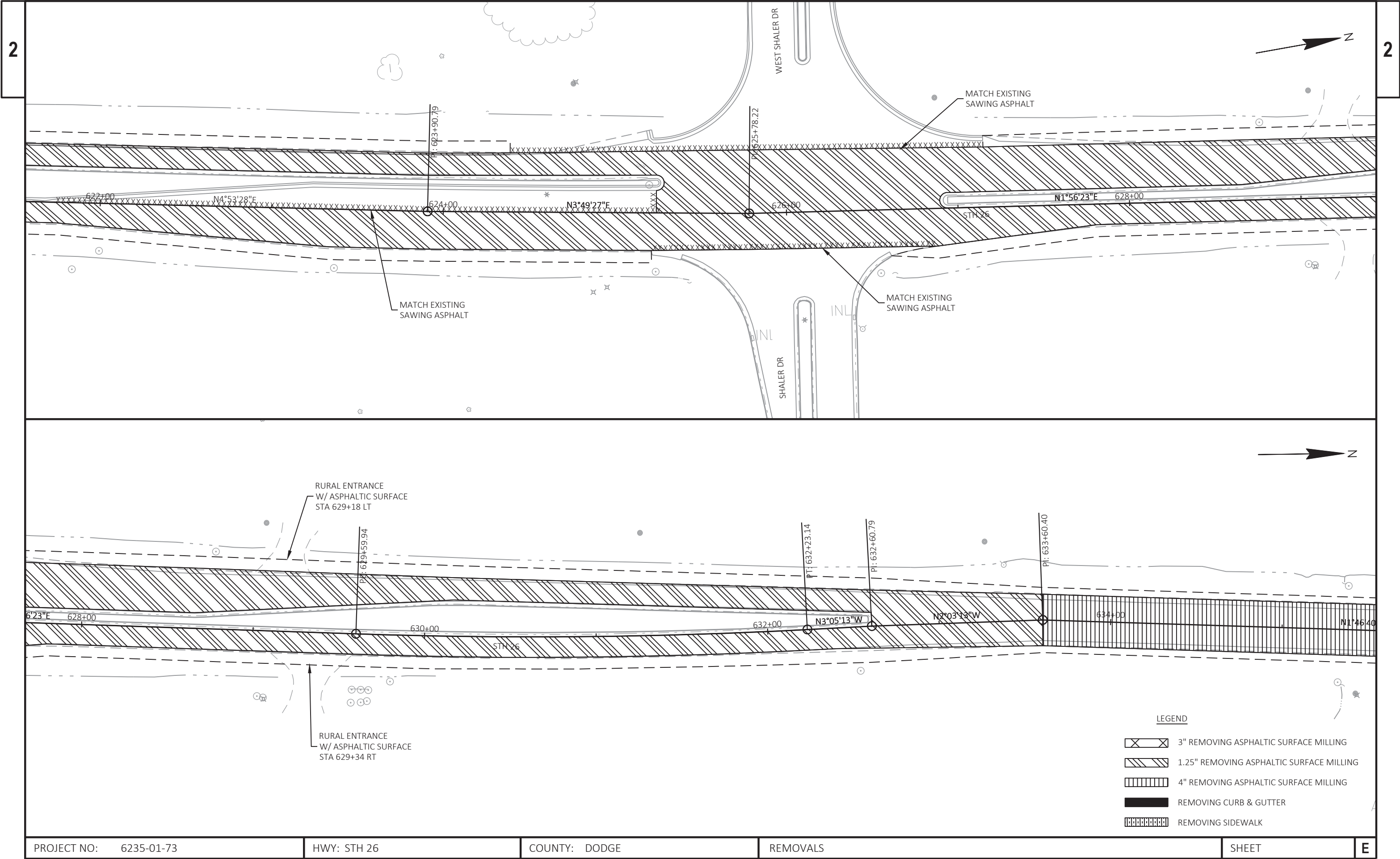
DESIGN NOTES
1. FOLLOW SDD 09A15-01 FOR INSTRUCTIONS TO CONSTRUCT RURAL ENTRANCE WITH ASPHALTIC SURFACE.

- LEGEND
- 3" REMOVING ASPHALTIC SURFACE MILLING
 - 1.25" REMOVING ASPHALTIC SURFACE MILLING
 - 4" REMOVING ASPHALTIC SURFACE MILLING
 - REMOVING CURB & GUTTER
 - REMOVING SIDEWALK

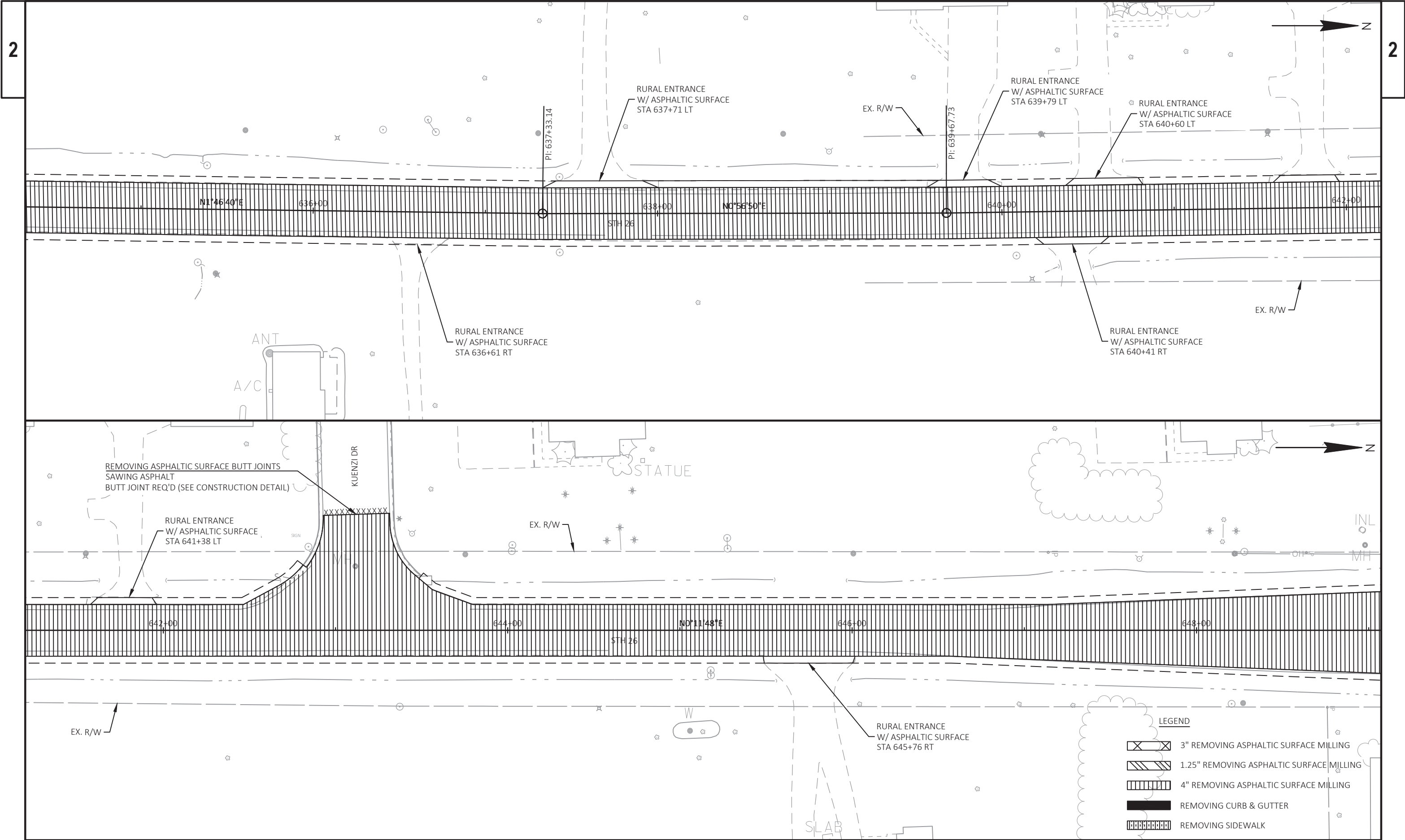
PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	REMOVALS	SHEET	E
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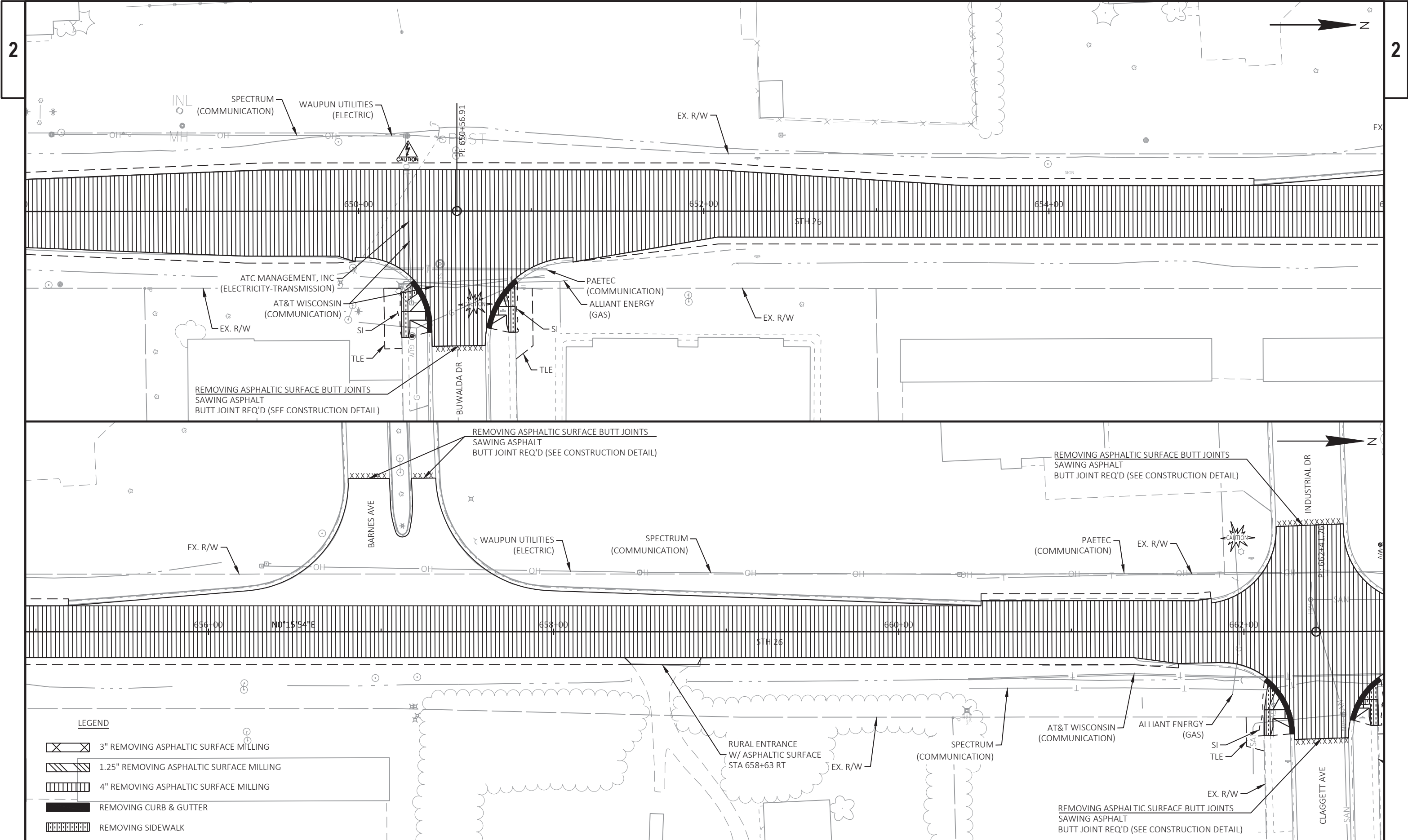


PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	REMOVALS	SHEET	E
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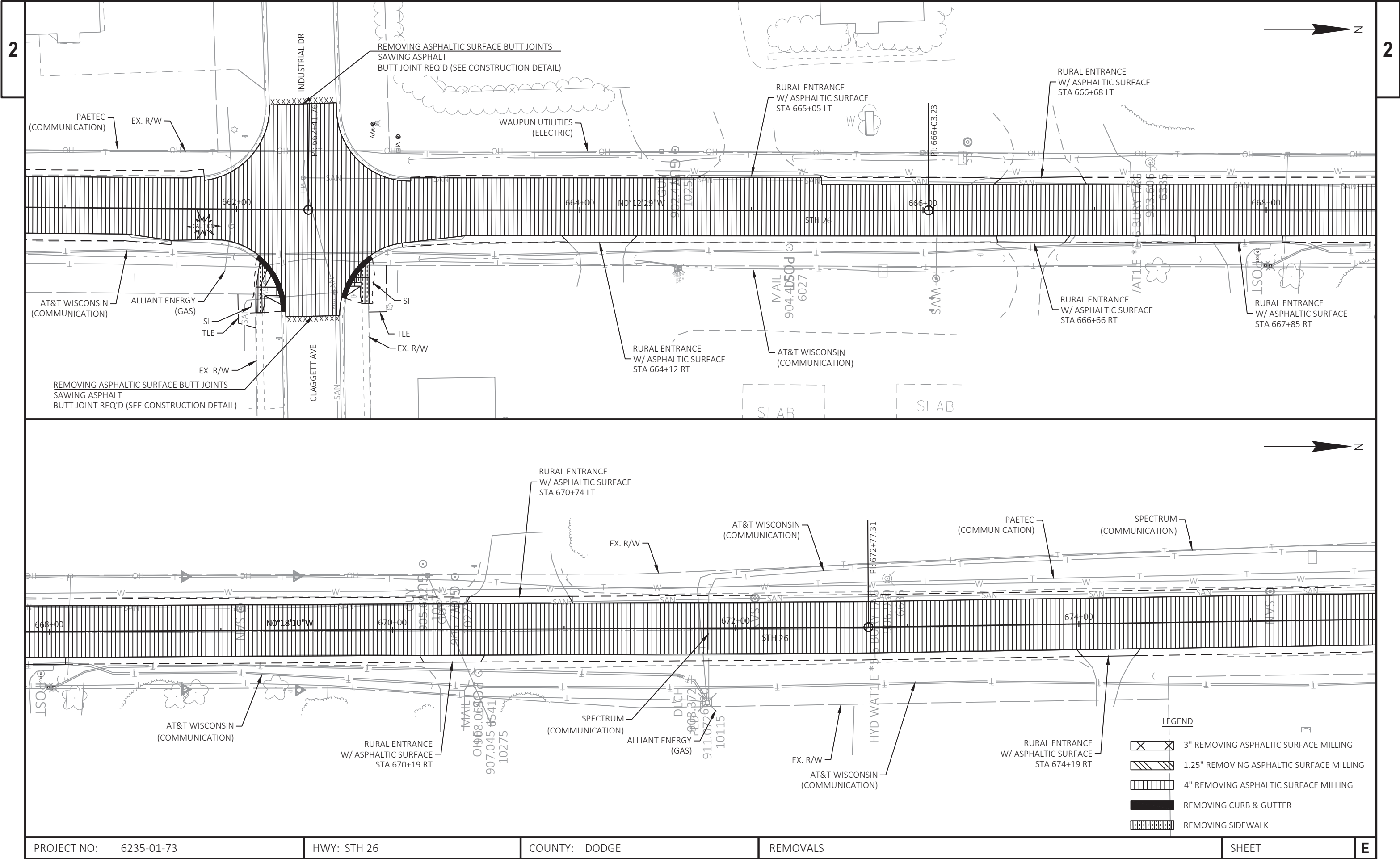


PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	REMOVALS	SHEET	E
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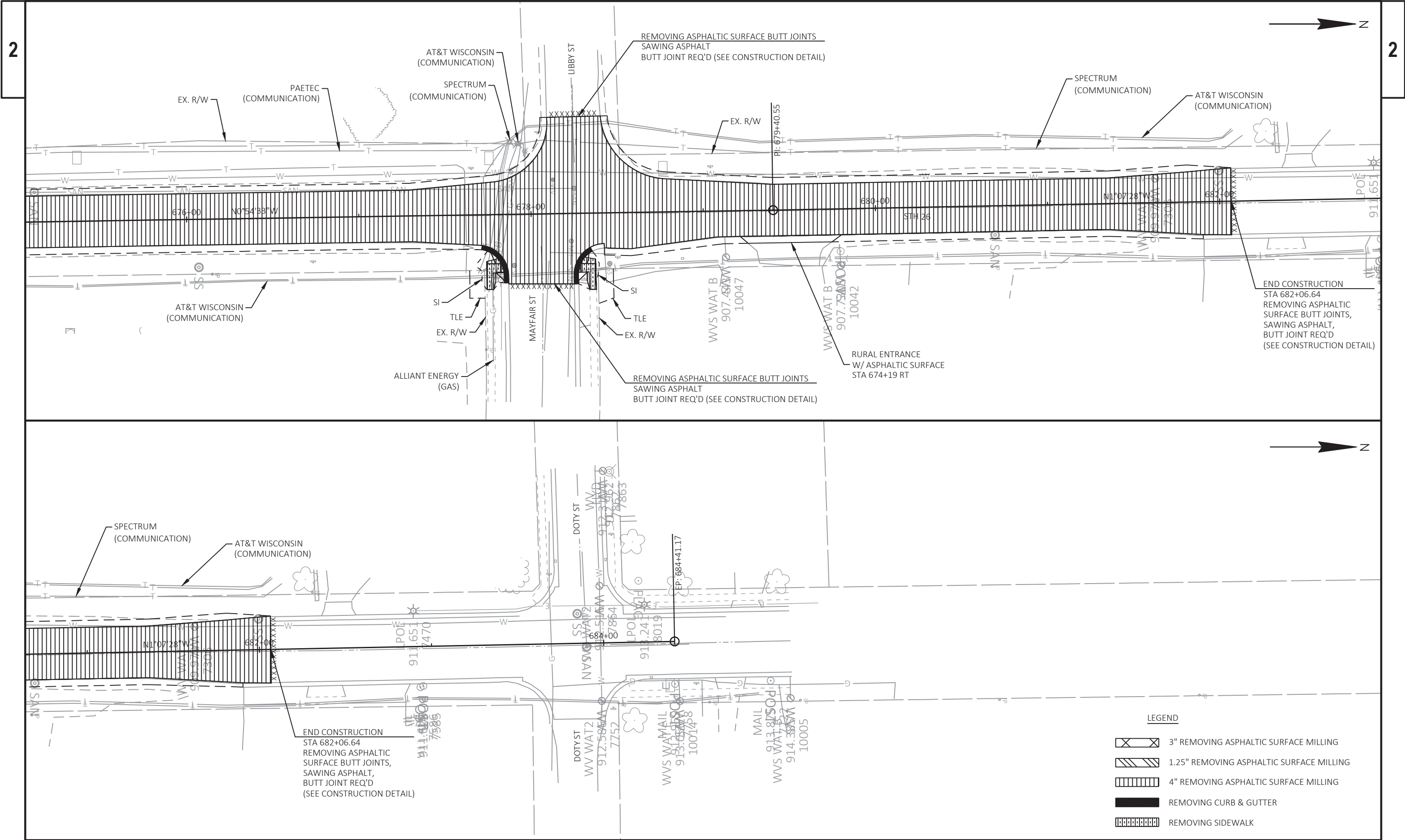




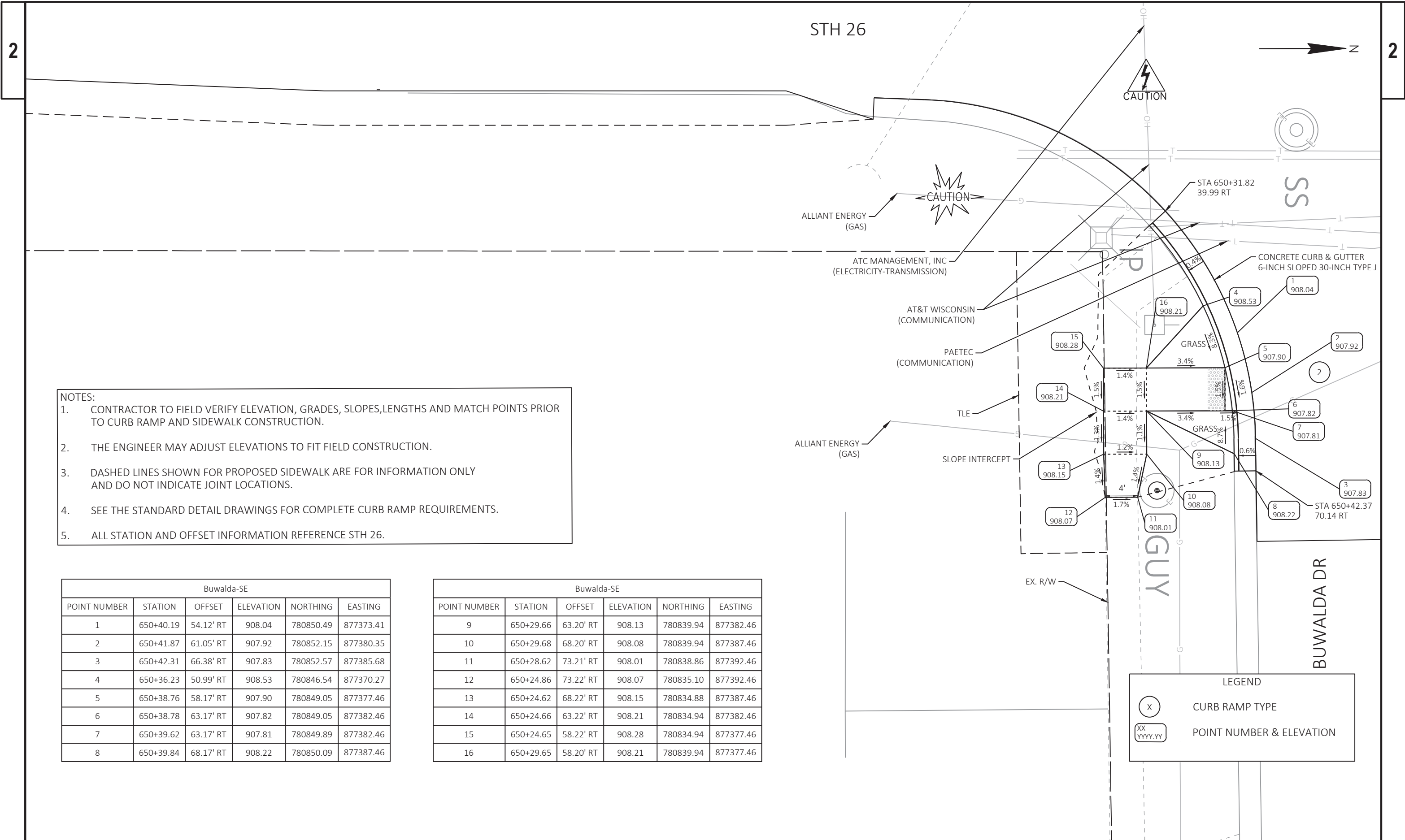
PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	REMOVALS	SHEET	E
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PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	REMOVALS	SHEET	E
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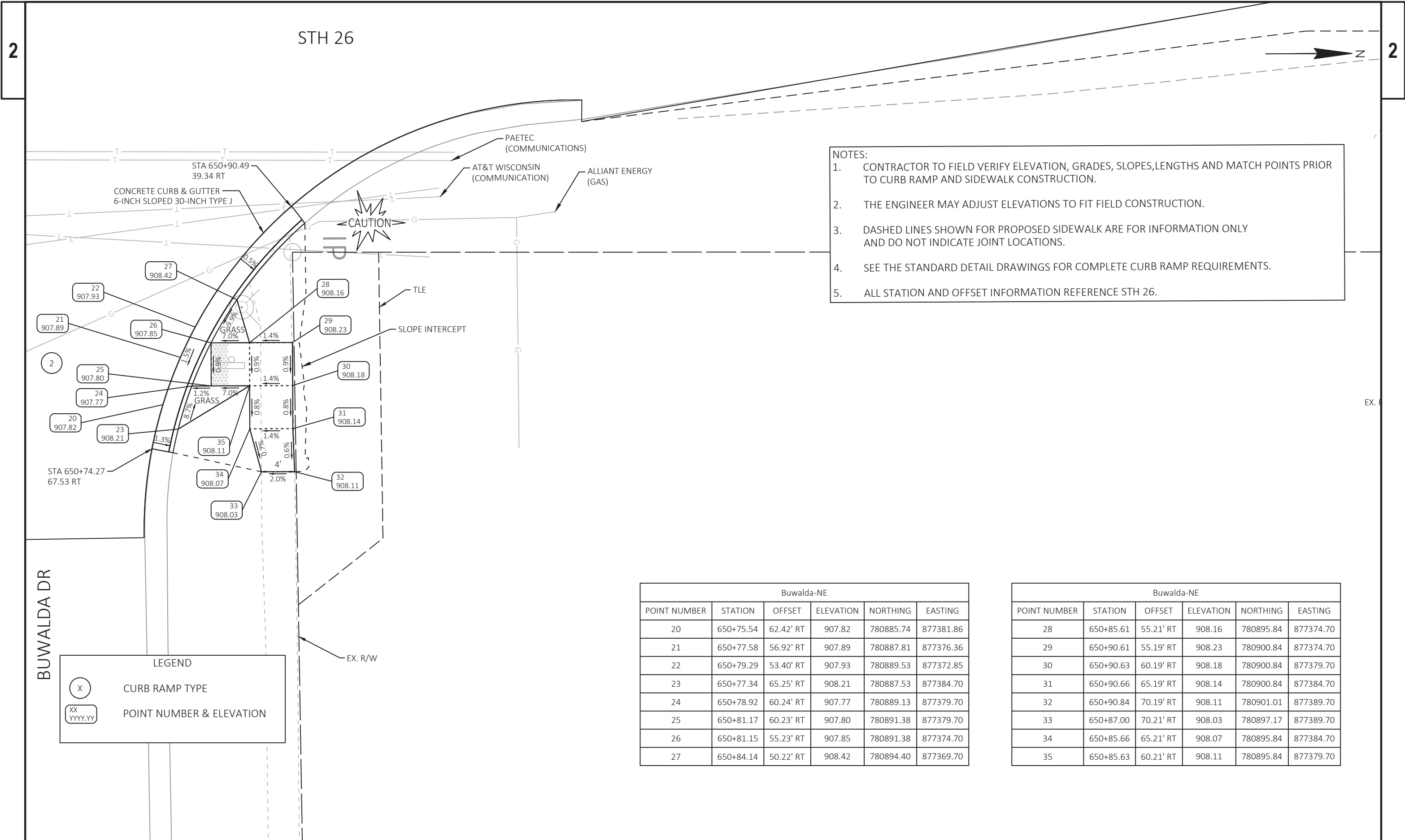
PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	REMOVALS	SHEET	E
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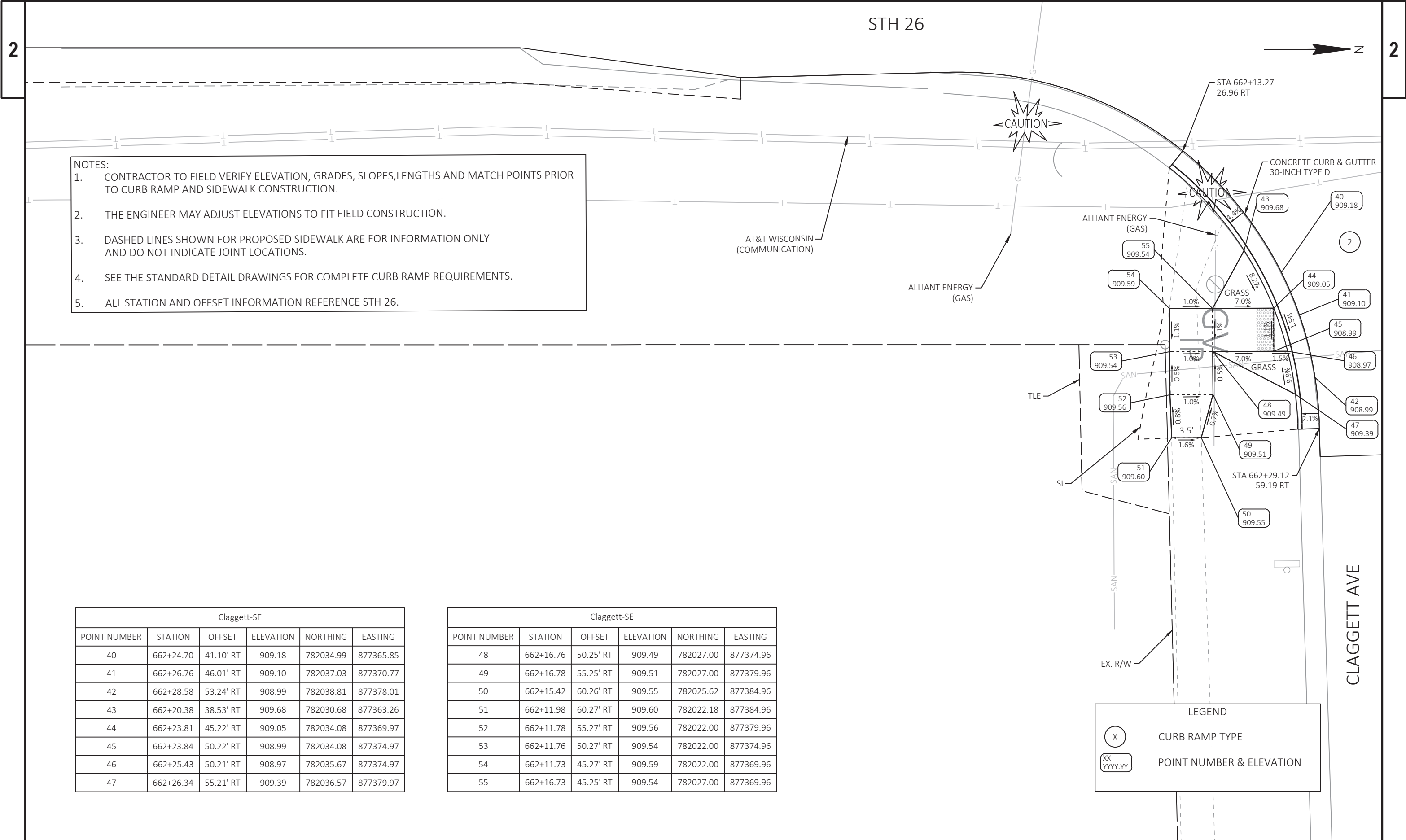


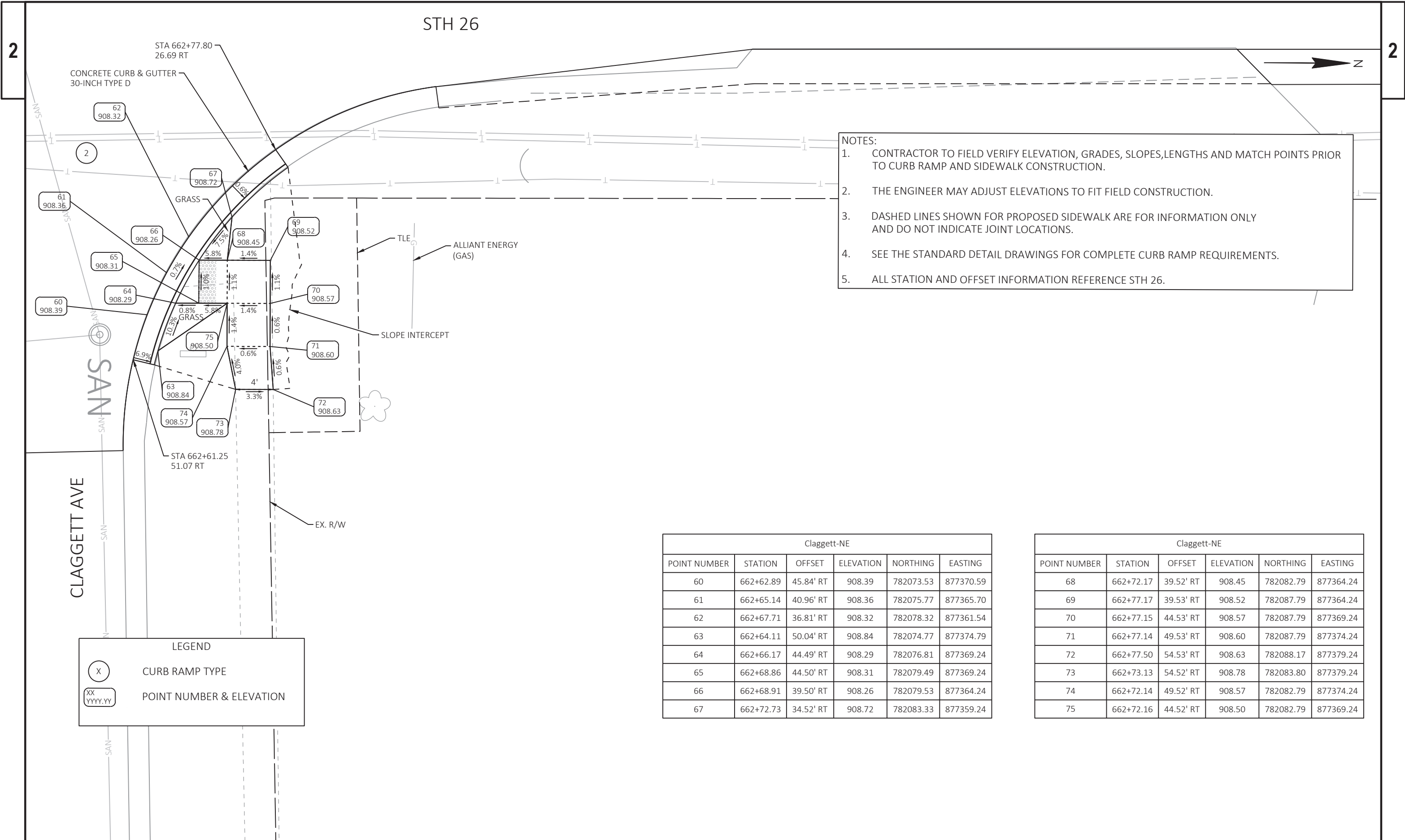
- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION.
 3. DASHED LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
 4. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE STH 26.

Buwalda-SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1	650+40.19	54.12' RT	908.04	780850.49	877373.41
2	650+41.87	61.05' RT	907.92	780852.15	877380.35
3	650+42.31	66.38' RT	907.83	780852.57	877385.68
4	650+36.23	50.99' RT	908.53	780846.54	877370.27
5	650+38.76	58.17' RT	907.90	780849.05	877377.46
6	650+38.78	63.17' RT	907.82	780849.05	877382.46
7	650+39.62	63.17' RT	907.81	780849.89	877382.46
8	650+39.84	68.17' RT	908.22	780850.09	877387.46

Buwalda-SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
9	650+29.66	63.20' RT	908.13	780839.94	877382.46
10	650+29.68	68.20' RT	908.08	780839.94	877387.46
11	650+28.62	73.21' RT	908.01	780838.86	877392.46
12	650+24.86	73.22' RT	908.07	780835.10	877392.46
13	650+24.62	68.22' RT	908.15	780834.88	877387.46
14	650+24.66	63.22' RT	908.21	780834.94	877382.46
15	650+24.65	58.22' RT	908.28	780834.94	877377.46
16	650+29.65	58.20' RT	908.21	780839.94	877377.46



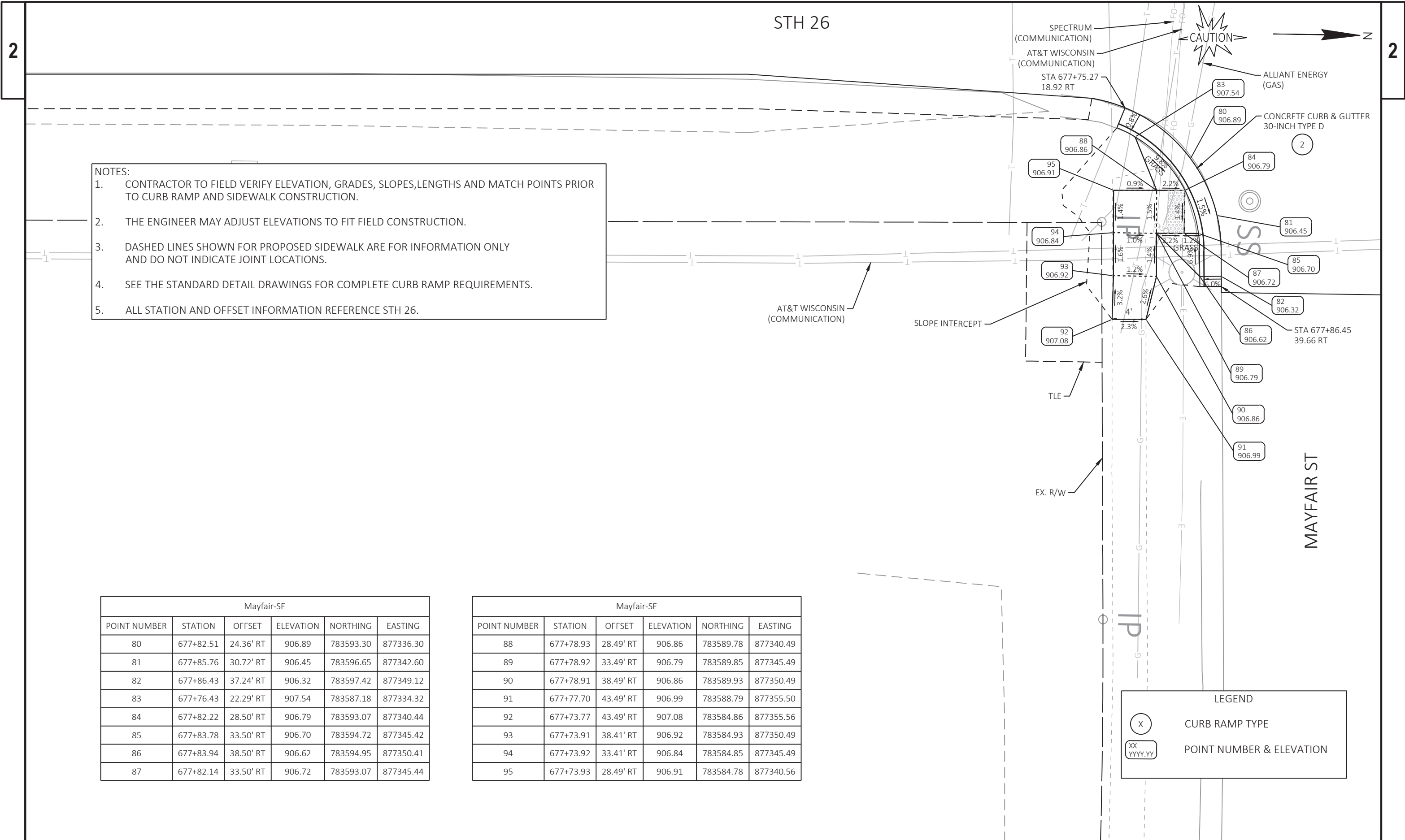




- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION.
 3. DASHED LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
 4. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE STH 26.

Claggett-NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
60	662+62.89	45.84' RT	908.39	782073.53	877370.59
61	662+65.14	40.96' RT	908.36	782075.77	877365.70
62	662+67.71	36.81' RT	908.32	782078.32	877361.54
63	662+64.11	50.04' RT	908.84	782074.77	877374.79
64	662+66.17	44.49' RT	908.29	782076.81	877369.24
65	662+68.86	44.50' RT	908.31	782079.49	877369.24
66	662+68.91	39.50' RT	908.26	782079.53	877364.24
67	662+72.73	34.52' RT	908.72	782083.33	877359.24

Claggett-NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
68	662+72.17	39.52' RT	908.45	782082.79	877364.24
69	662+77.17	39.53' RT	908.52	782087.79	877364.24
70	662+77.15	44.53' RT	908.57	782087.79	877369.24
71	662+77.14	49.53' RT	908.60	782087.79	877374.24
72	662+77.50	54.53' RT	908.63	782088.17	877379.24
73	662+73.13	54.52' RT	908.78	782083.80	877379.24
74	662+72.14	49.52' RT	908.57	782082.79	877374.24
75	662+72.16	44.52' RT	908.50	782082.79	877369.24



- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION.
 3. DASHED LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
 4. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE STH 26.

Mayfair-SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
80	677+82.51	24.36' RT	906.89	783593.30	877336.30
81	677+85.76	30.72' RT	906.45	783596.65	877342.60
82	677+86.43	37.24' RT	906.32	783597.42	877349.12
83	677+76.43	22.29' RT	907.54	783587.18	877334.32
84	677+82.22	28.50' RT	906.79	783593.07	877340.44
85	677+83.78	33.50' RT	906.70	783594.72	877345.42
86	677+83.94	38.50' RT	906.62	783594.95	877350.41
87	677+82.14	33.50' RT	906.72	783593.07	877345.44

Mayfair-SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
88	677+78.93	28.49' RT	906.86	783589.78	877340.49
89	677+78.92	33.49' RT	906.79	783589.85	877345.49
90	677+78.91	38.49' RT	906.86	783589.93	877350.49
91	677+77.70	43.49' RT	906.99	783588.79	877355.50
92	677+73.77	43.49' RT	907.08	783584.86	877355.56
93	677+73.91	38.41' RT	906.92	783584.93	877350.49
94	677+73.92	33.41' RT	906.84	783584.85	877345.49
95	677+73.93	28.49' RT	906.91	783584.78	877340.56

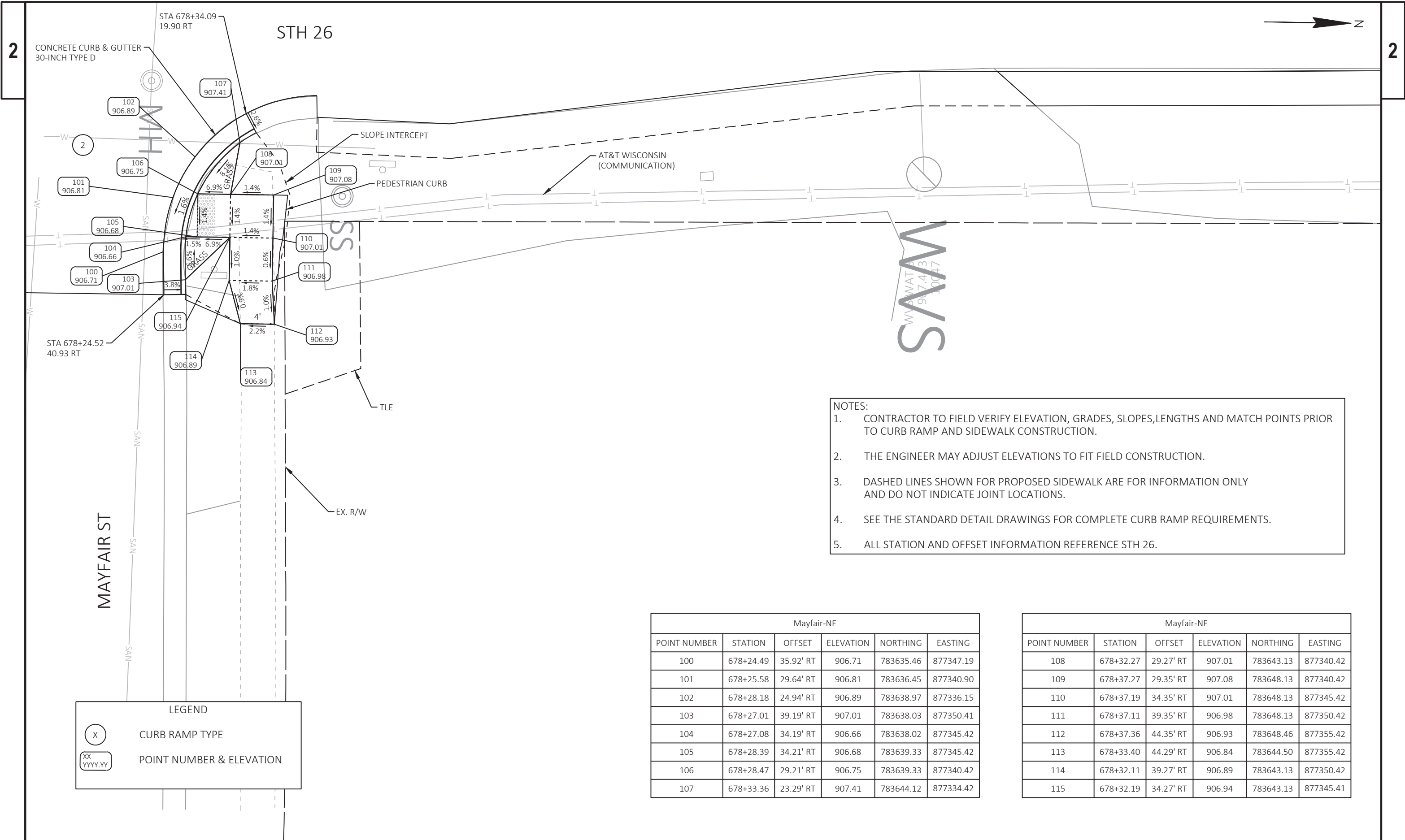
LEGEND

X

CURB RAMP TYPE

XX
YYYY.YY

POINT NUMBER & ELEVATION



PROJECT NO: 6235-01-73

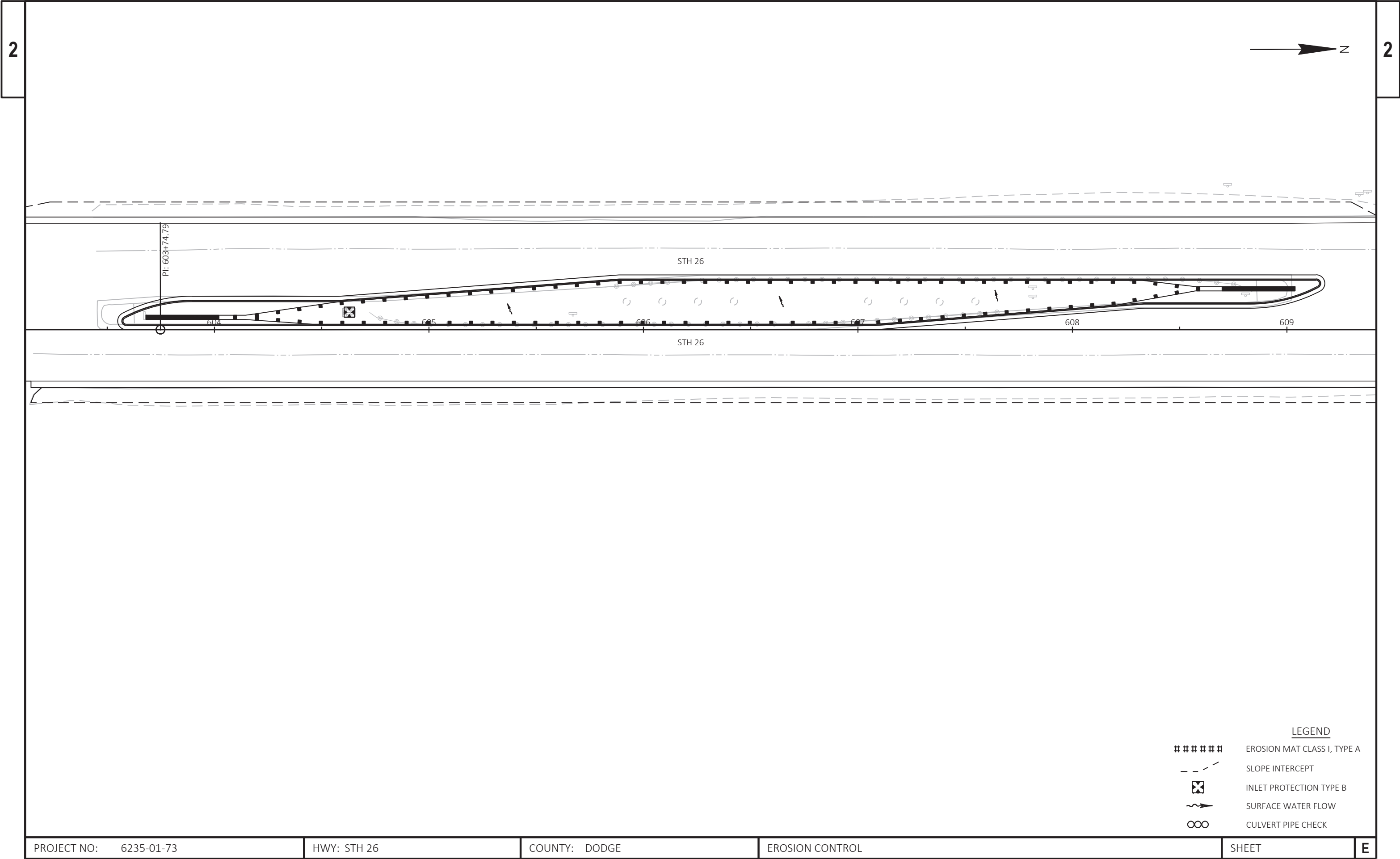
HWY: STH 26

COUNTY: DODGE

CURB RAMP DETAILS - MAYFAIR NE

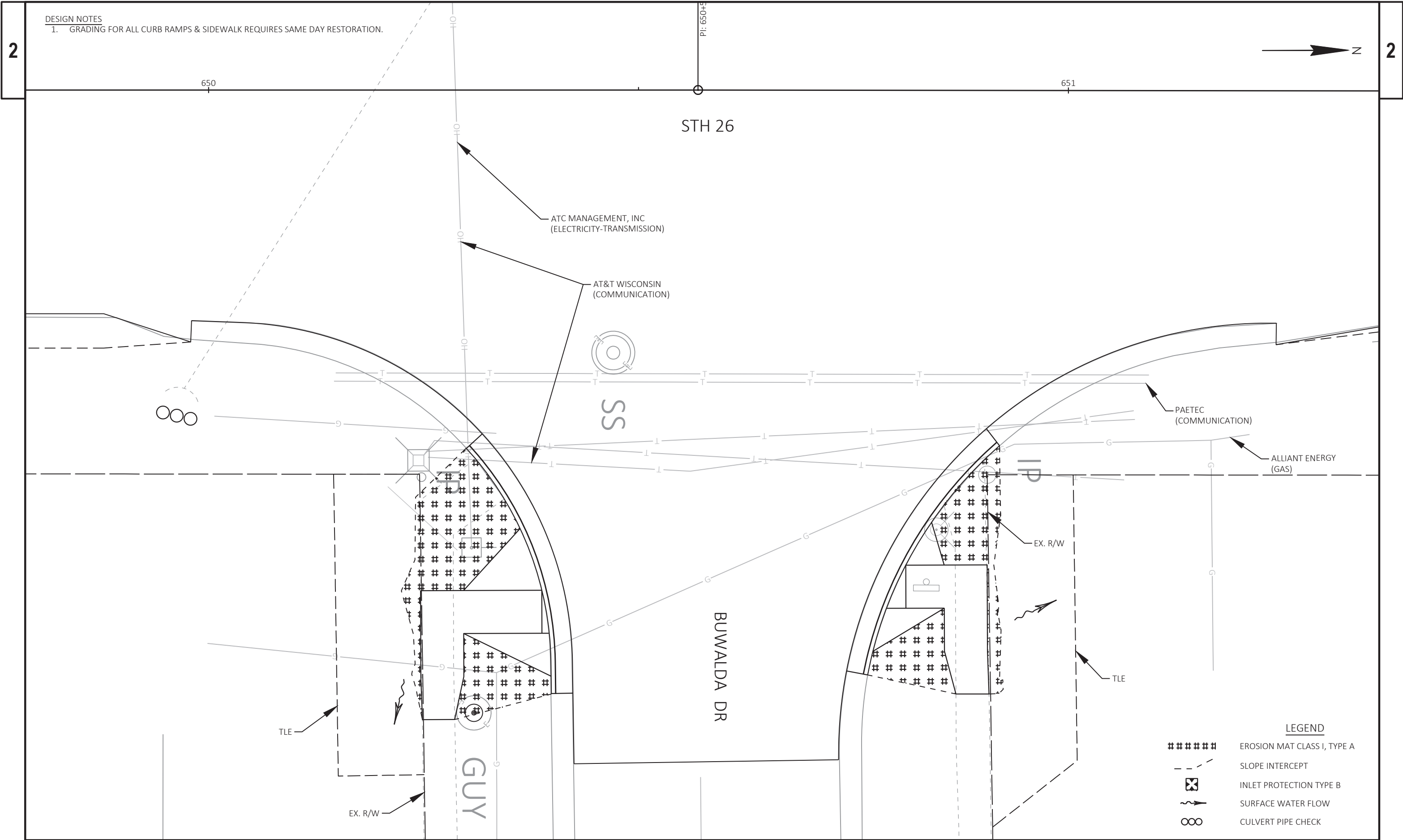
SHEET

E

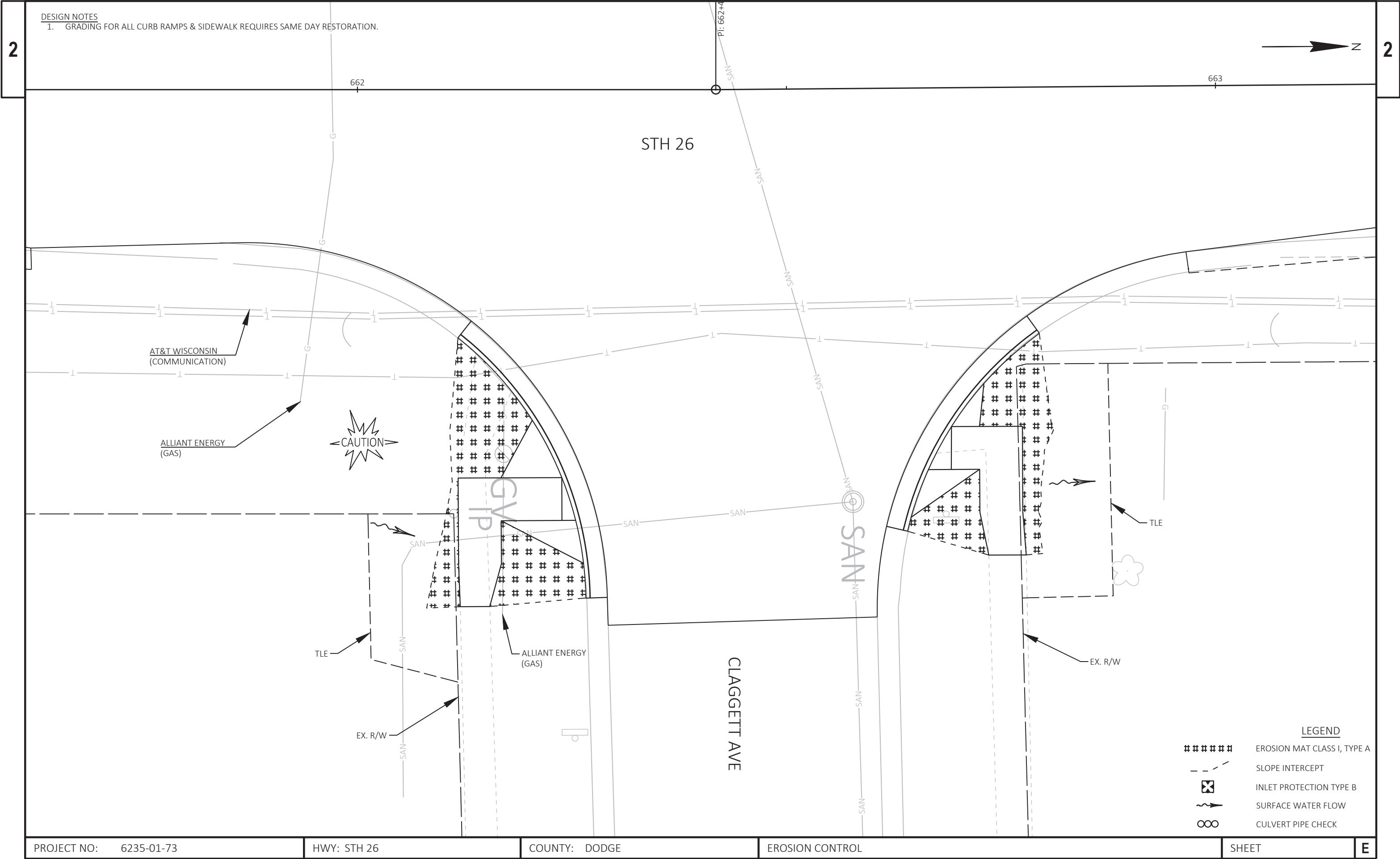


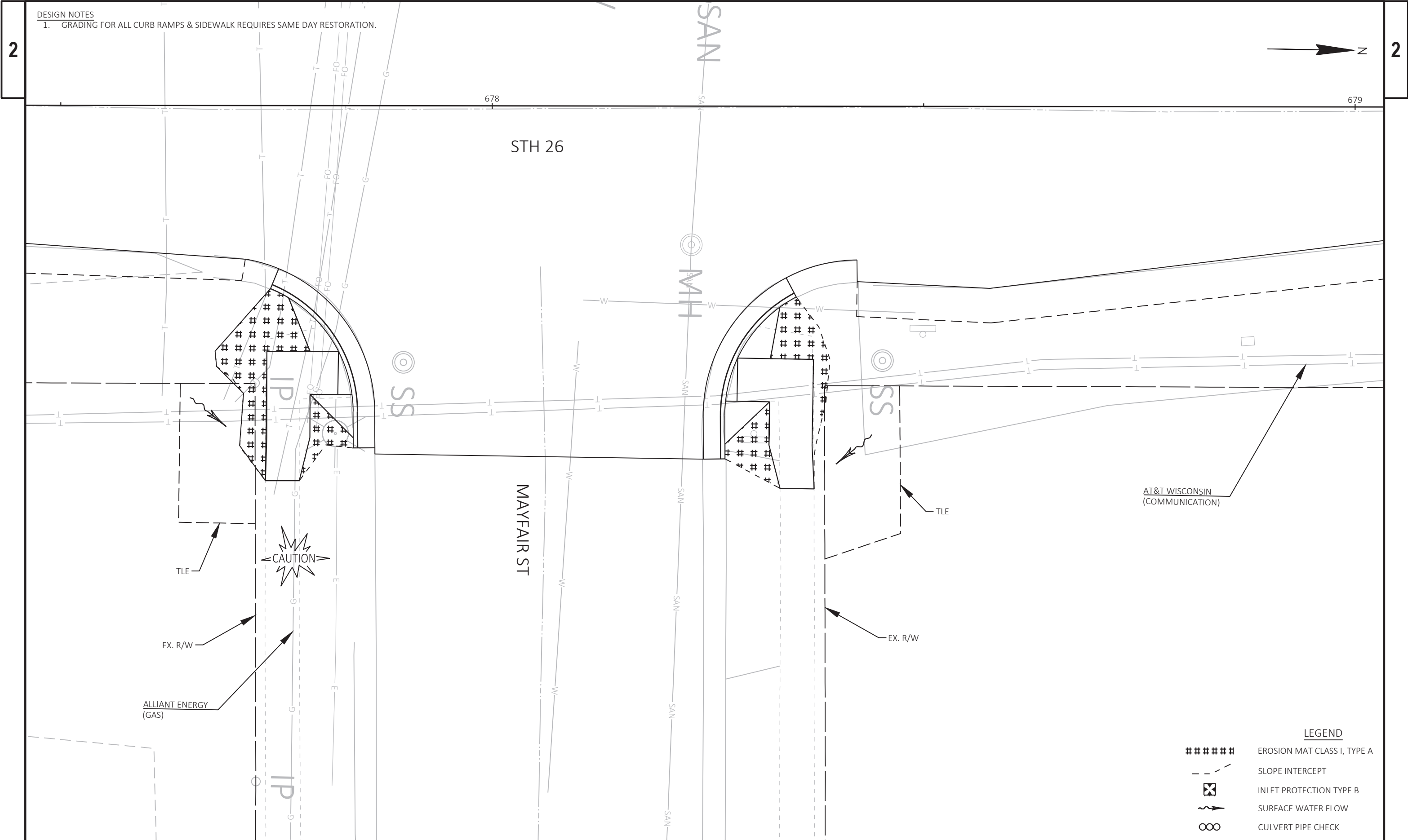
LEGEND	
#####	EROSION MAT CLASS I, TYPE A
- - -	SLOPE INTERCEPT
⊗	INLET PROTECTION TYPE B
~>	SURFACE WATER FLOW
∞	CULVERT PIPE CHECK

PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	EROSION CONTROL	SHEET	E
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DESIGN NOTES
1. GRADING FOR ALL CURB RAMPS & SIDEWALK REQUIRES SAME DAY RESTORATION.



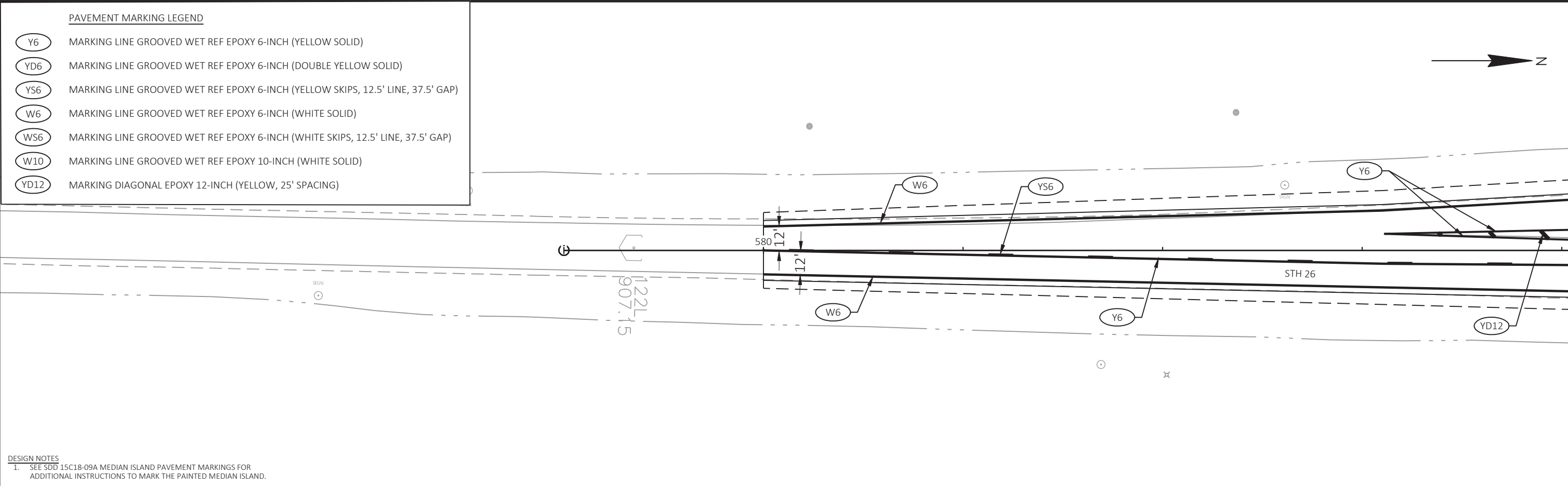


DESIGN NOTES
1. GRADING FOR ALL CURB RAMP & SIDEWALK REQUIRES SAME DAY RESTORATION.

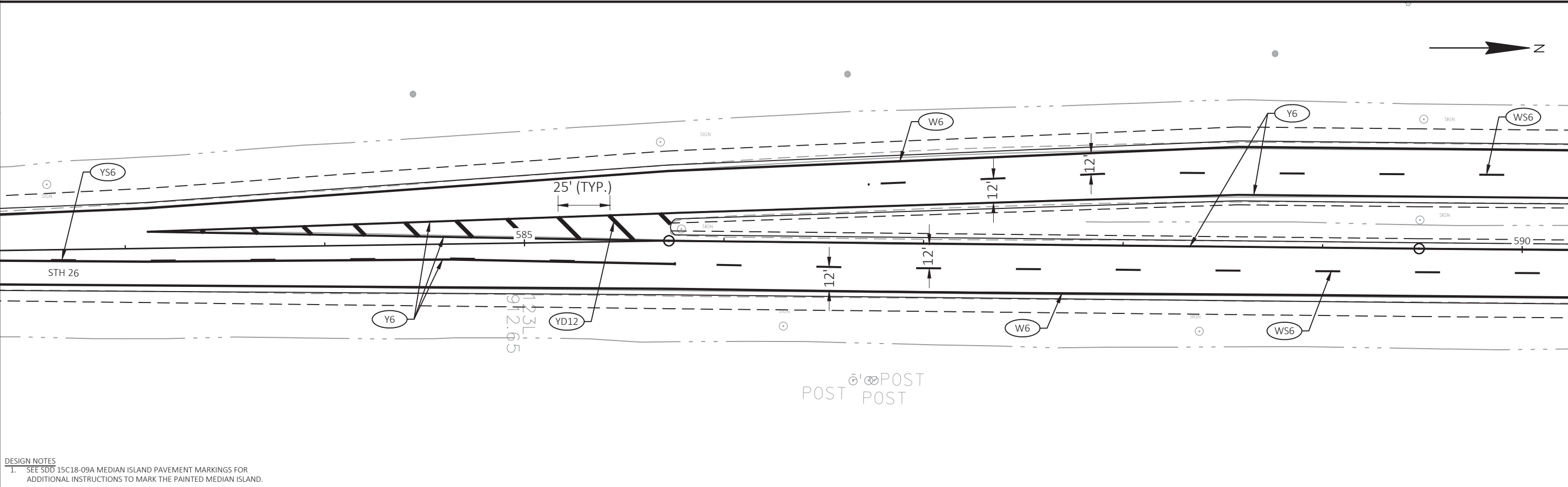
- LEGEND**
- ##### EROSION MAT CLASS I, TYPE A
 - - - SLOPE INTERCEPT
 - ⊗ INLET PROTECTION TYPE B
 - ~> SURFACE WATER FLOW
 - ∞ CULVERT PIPE CHECK

PAVEMENT MARKING LEGEND

- Y6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW SOLID)
- YD6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (DOUBLE YELLOW SOLID)
- YS6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW SKIPS, 12.5' LINE, 37.5' GAP)
- W6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE SOLID)
- WS6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE SKIPS, 12.5' LINE, 37.5' GAP)
- W10 MARKING LINE GROOVED WET REF EPOXY 10-INCH (WHITE SOLID)
- YD12 MARKING DIAGONAL EPOXY 12-INCH (YELLOW, 25' SPACING)



DESIGN NOTES
1. SEE SDD 15C18-09A MEDIAN ISLAND PAVEMENT MARKINGS FOR ADDITIONAL INSTRUCTIONS TO MARK THE PAINTED MEDIAN ISLAND.

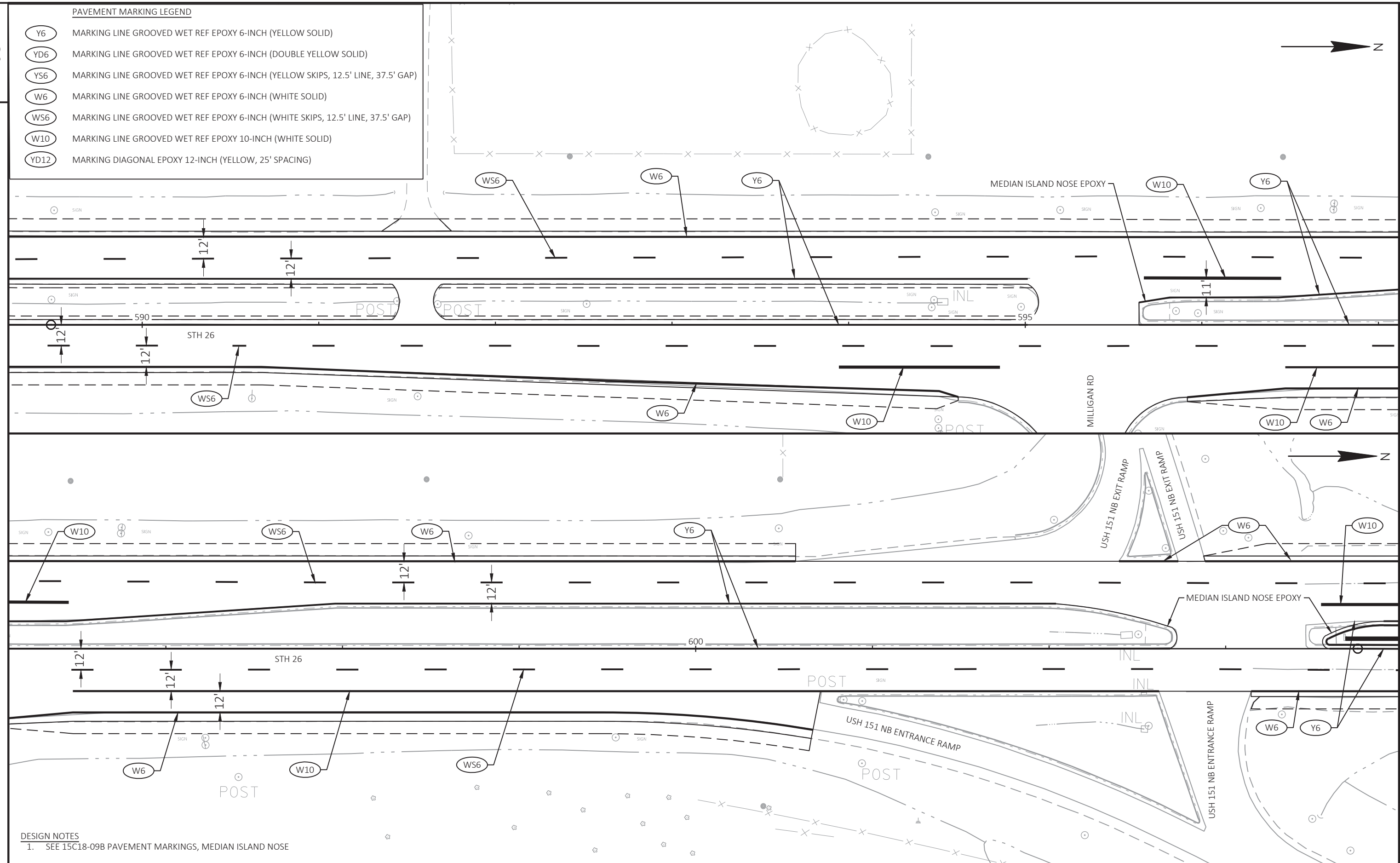


DESIGN NOTES
1. SEE SDD 15C18-09A MEDIAN ISLAND PAVEMENT MARKINGS FOR ADDITIONAL INSTRUCTIONS TO MARK THE PAINTED MEDIAN ISLAND.

PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	PAVEMENT MARKING	SHEET	E
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PAVEMENT MARKING LEGEND

- Y6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW SOLID)
- YD6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (DOUBLE YELLOW SOLID)
- YS6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW SKIPS, 12.5' LINE, 37.5' GAP)
- W6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE SOLID)
- WS6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE SKIPS, 12.5' LINE, 37.5' GAP)
- W10 MARKING LINE GROOVED WET REF EPOXY 10-INCH (WHITE SOLID)
- YD12 MARKING DIAGONAL EPOXY 12-INCH (YELLOW, 25' SPACING)



DESIGN NOTES

- 1. SEE 15C18-09B PAVEMENT MARKINGS, MEDIAN ISLAND NOSE

PROJECT NO: 6235-01-73

HWY: STH 26

COUNTY: DODGE

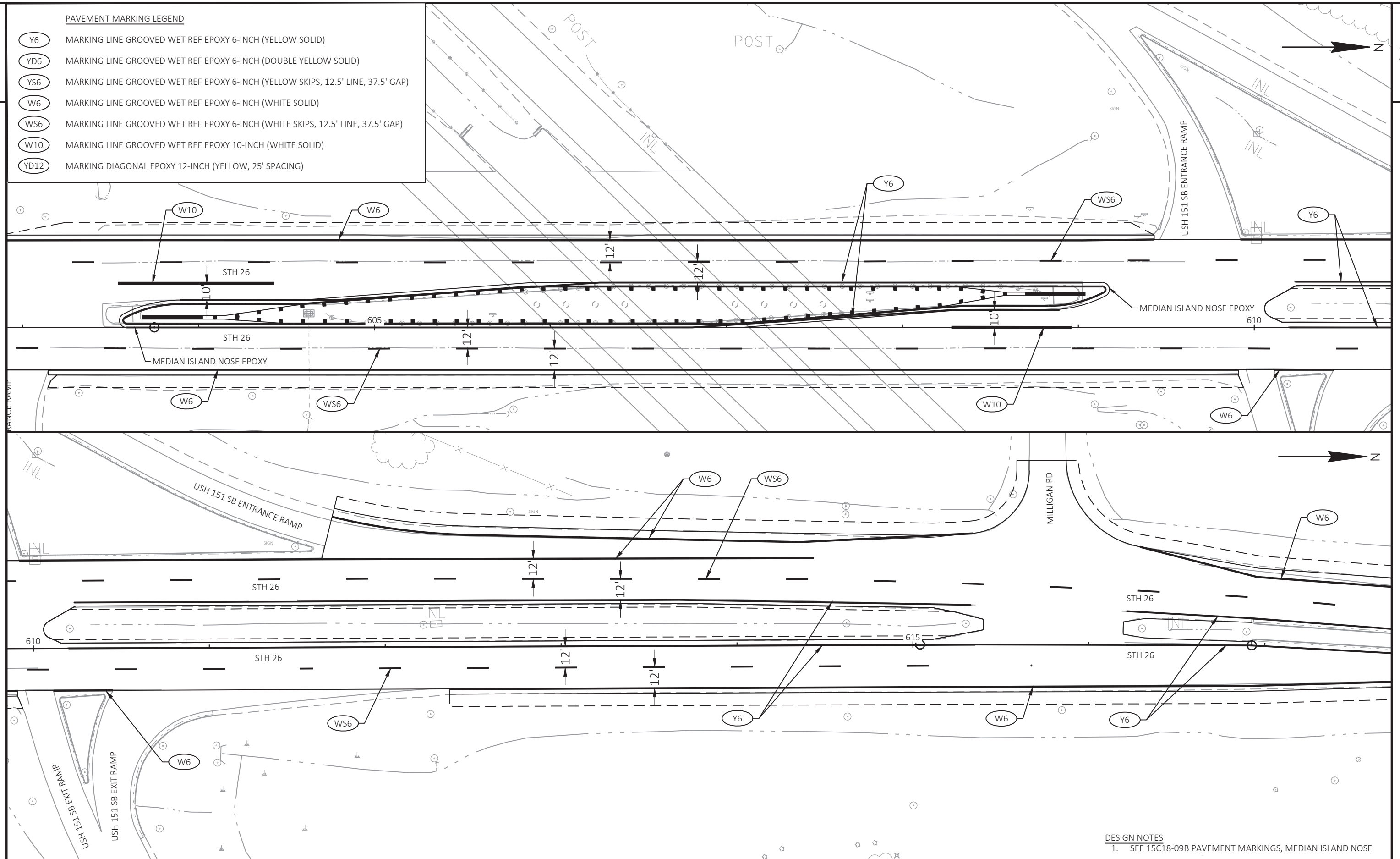
PAVEMENT MARKING

SHEET

E

PAVEMENT MARKING LEGEND

- (Y6) MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW SOLID)
- (YD6) MARKING LINE GROOVED WET REF EPOXY 6-INCH (DOUBLE YELLOW SOLID)
- (YS6) MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW SKIPS, 12.5' LINE, 37.5' GAP)
- (W6) MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE SOLID)
- (WS6) MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE SKIPS, 12.5' LINE, 37.5' GAP)
- (W10) MARKING LINE GROOVED WET REF EPOXY 10-INCH (WHITE SOLID)
- (YD12) MARKING DIAGONAL EPOXY 12-INCH (YELLOW, 25' SPACING)



DESIGN NOTES

1. SEE 15C18-09B PAVEMENT MARKINGS, MEDIAN ISLAND NOSE

PROJECT NO: 6235-01-73

HWY: STH 26

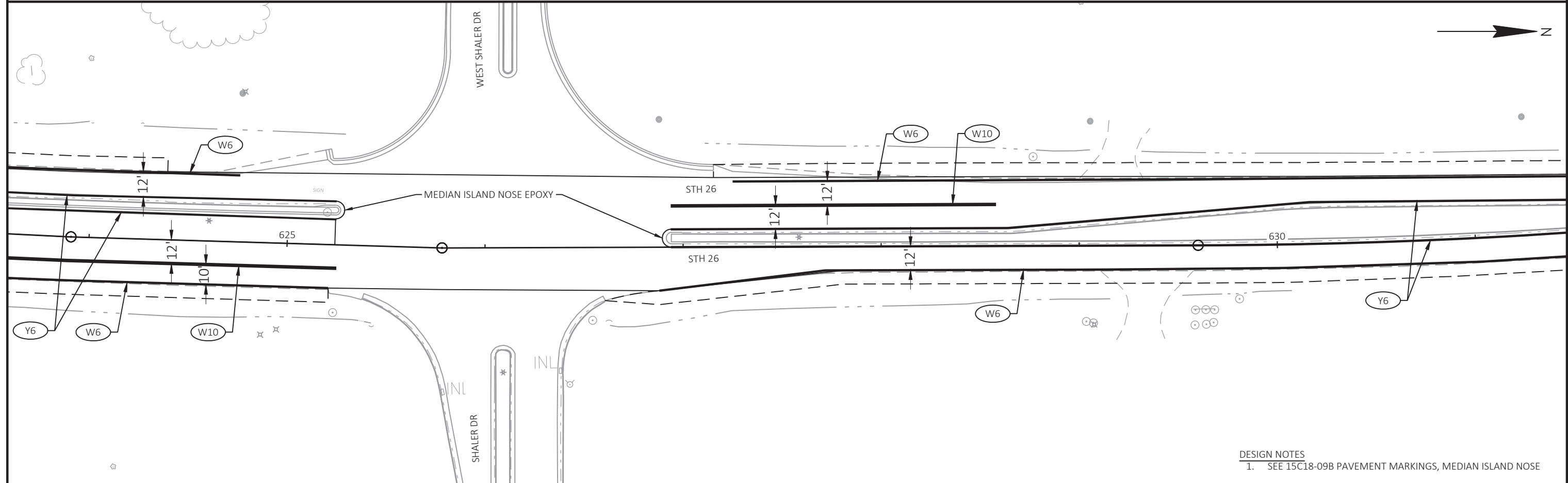
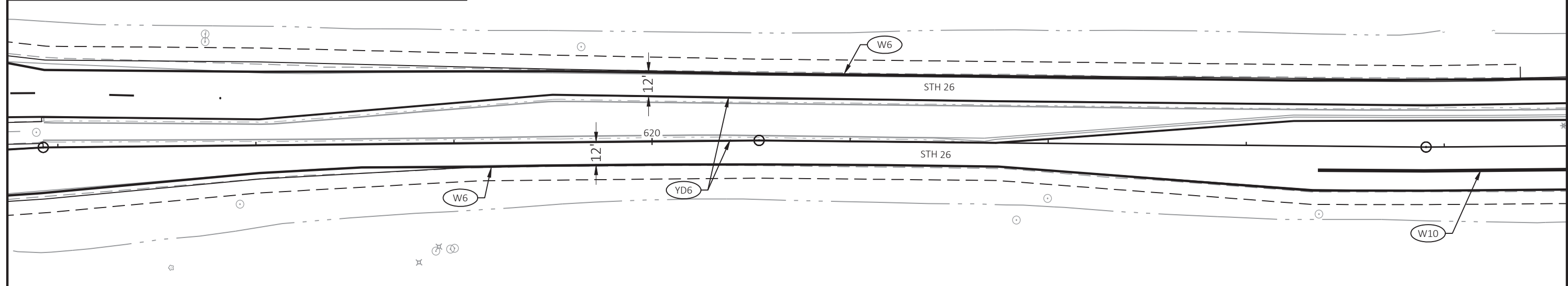
COUNTY: DODGE

PAVEMENT MARKING

SHEET

E

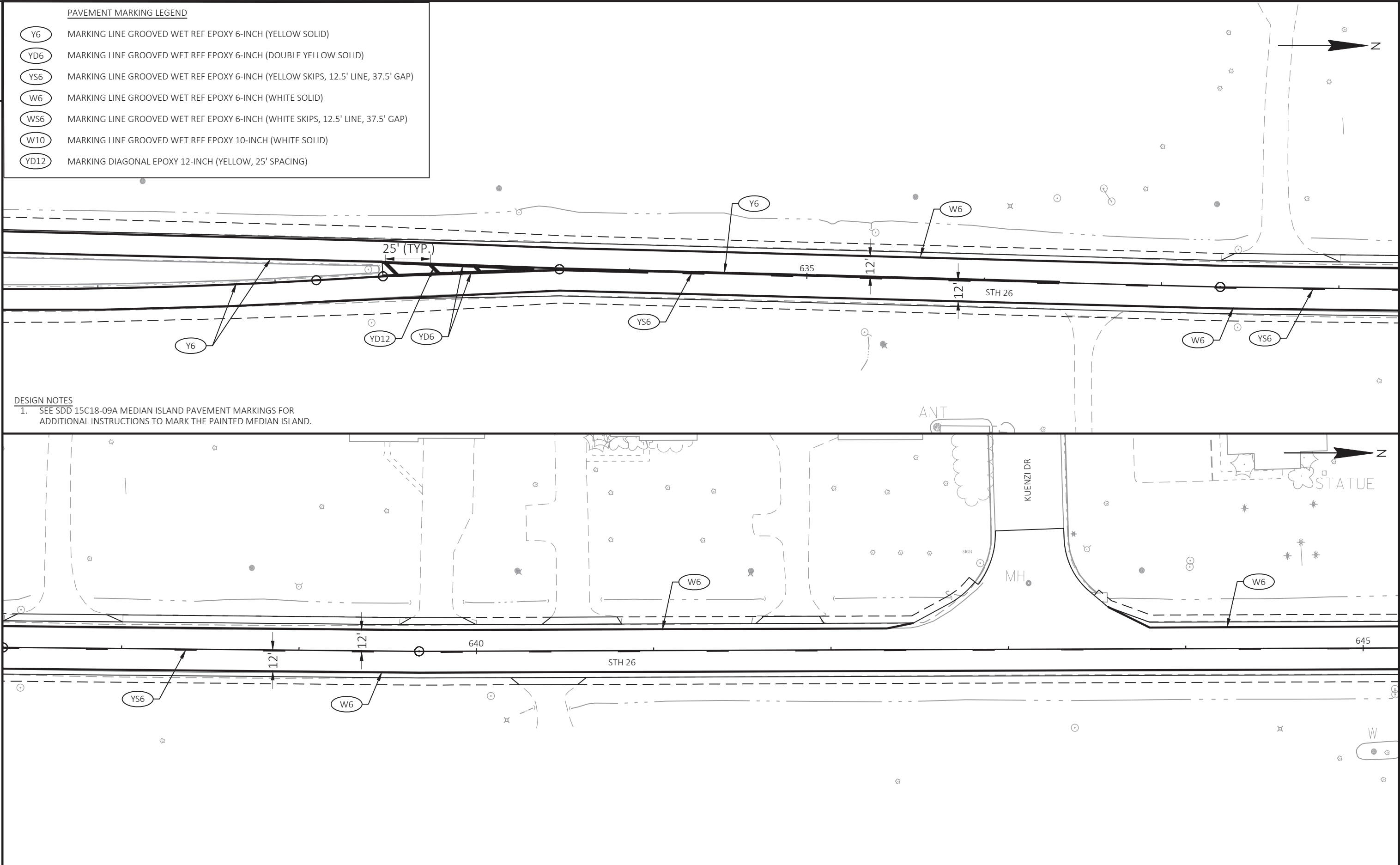
PAVEMENT MARKING LEGEND	
Y6	MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW SOLID)
YD6	MARKING LINE GROOVED WET REF EPOXY 6-INCH (DOUBLE YELLOW SOLID)
YS6	MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW SKIPS, 12.5' LINE, 37.5' GAP)
W6	MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE SOLID)
WS6	MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE SKIPS, 12.5' LINE, 37.5' GAP)
W10	MARKING LINE GROOVED WET REF EPOXY 10-INCH (WHITE SOLID)
YD12	MARKING DIAGONAL EPOXY 12-INCH (YELLOW, 25' SPACING)



PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	PAVEMENT MARKING	SHEET	E
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PAVEMENT MARKING LEGEND

- Y6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW SOLID)
- YD6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (DOUBLE YELLOW SOLID)
- YS6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW SKIPS, 12.5' LINE, 37.5' GAP)
- W6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE SOLID)
- WS6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE SKIPS, 12.5' LINE, 37.5' GAP)
- W10 MARKING LINE GROOVED WET REF EPOXY 10-INCH (WHITE SOLID)
- YD12 MARKING DIAGONAL EPOXY 12-INCH (YELLOW, 25' SPACING)



DESIGN NOTES
1. SEE SDD 15C18-09A MEDIAN ISLAND PAVEMENT MARKINGS FOR ADDITIONAL INSTRUCTIONS TO MARK THE PAINTED MEDIAN ISLAND.

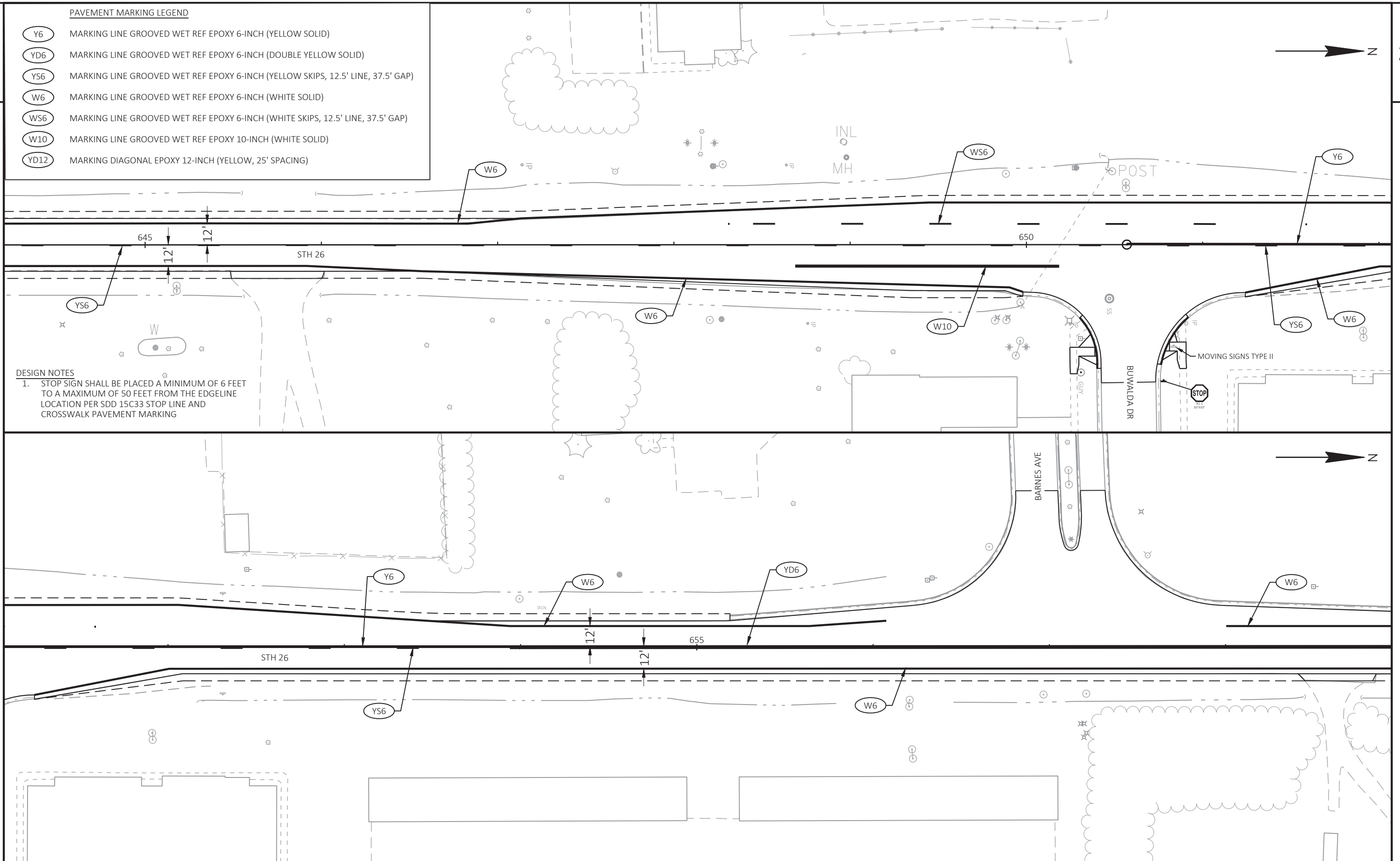
PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	PAVEMENT MARKING	SHEET	E
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PAVEMENT MARKING LEGEND

- Y6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW SOLID)
- YD6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (DOUBLE YELLOW SOLID)
- YS6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW SKIPS, 12.5' LINE, 37.5' GAP)
- W6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE SOLID)
- WS6 MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE SKIPS, 12.5' LINE, 37.5' GAP)
- W10 MARKING LINE GROOVED WET REF EPOXY 10-INCH (WHITE SOLID)
- YD12 MARKING DIAGONAL EPOXY 12-INCH (YELLOW, 25' SPACING)

DESIGN NOTES

- STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION PER SDD 15C33 STOP LINE AND CROSSWALK PAVEMENT MARKING



PROJECT NO: 6235-01-73

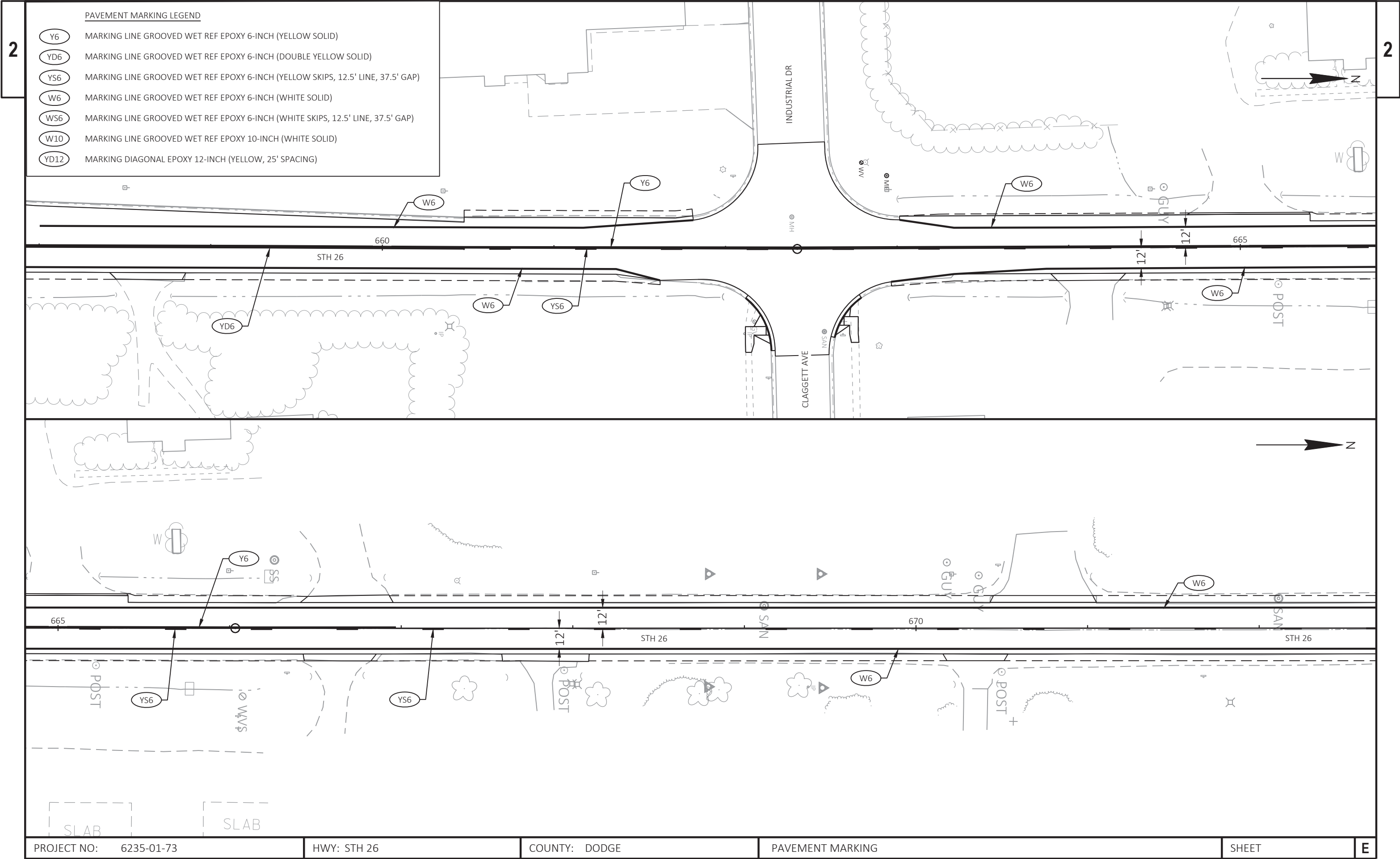
HWY: STH 26

COUNTY: DODGE

PAVEMENT MARKING

SHEET

E



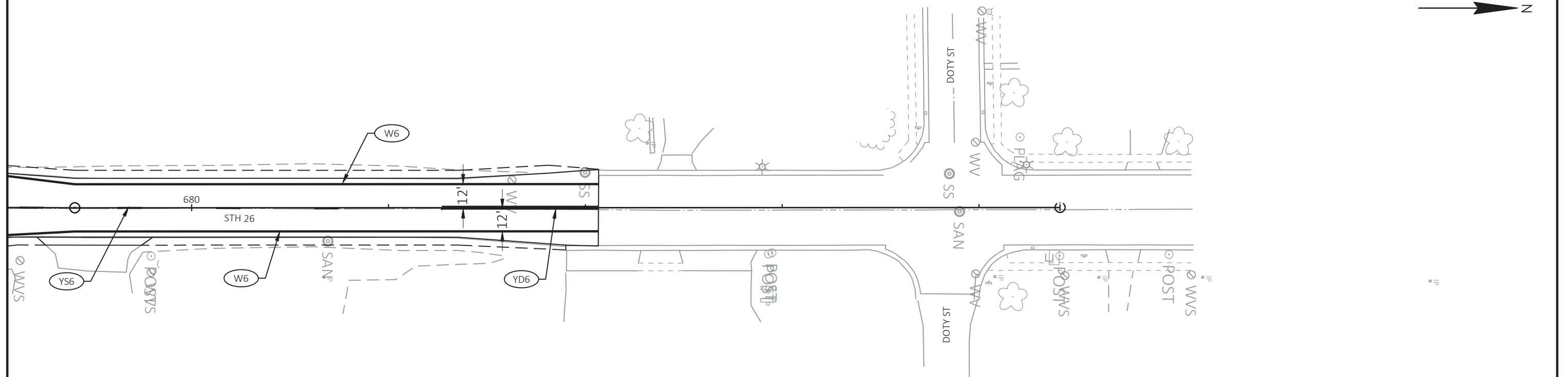
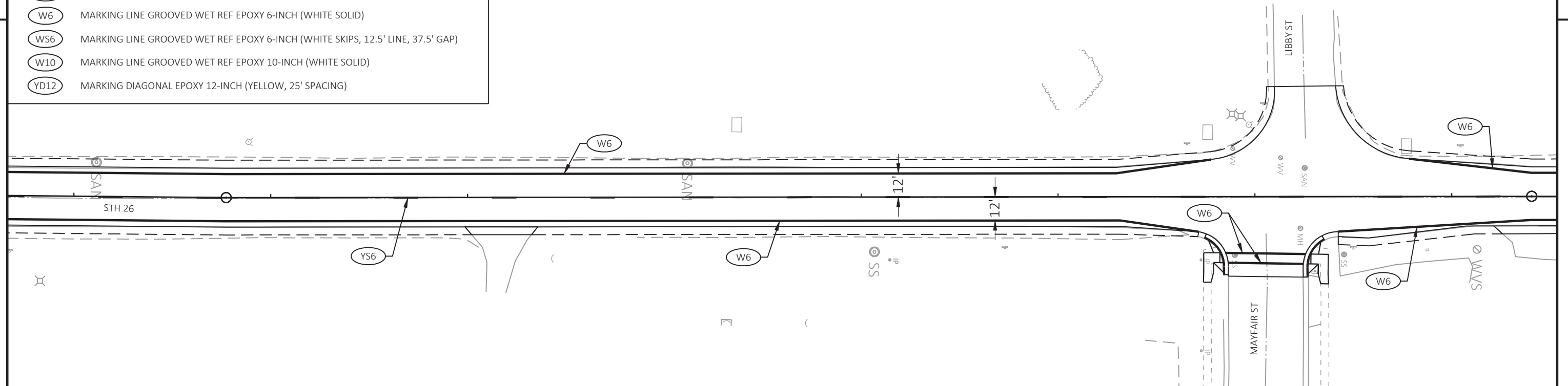
PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	PAVEMENT MARKING	SHEET	E
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2

PAVEMENT MARKING LEGEND

- | | |
|------|---|
| Y6 | MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW SOLID) |
| YD6 | MARKING LINE GROOVED WET REF EPOXY 6-INCH (DOUBLE YELLOW SOLID) |
| YS6 | MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW SKIPS, 12.5' LINE, 37.5' GAP) |
| W6 | MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE SOLID) |
| WS6 | MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE SKIPS, 12.5' LINE, 37.5' GAP) |
| W10 | MARKING LINE GROOVED WET REF EPOXY 10-INCH (WHITE SOLID) |
| YD12 | MARKING DIAGONAL EPOXY 12-INCH (YELLOW, 25' SPACING) |

2



PROJECT NO:	6235-01-73
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HWY: STH 26

COUNTY: DODGE

PAVEMENT MARKING

SHEET

11

STAGE 1A

THE INSIDE TRAVEL LANES WILL BE CLOSED FROM STA 580+00 TO STA 615+04. THE LEFT TURN LANES ON STH 26 AND THE USH 151 RAMPS WILL BE CLOSED. THE USH 151 AND STH 26 INTERCHANGE WILL ALLOW RIGHT TURNS FOR THE USH 151 AND STH 26 INTERCHANGE FOR 24 HOURS TO COMPLETE CONSTRUCTION OPERATIONS FOR BASE PATCHING CONCRETE SHES LOCATED ON THE INSIDE TRAVEL LANES AS SHOWN ON THE PLAN. PCMS MESSAGE WILL BE POSTED 7 DAYS PRIOR TO THE BEGINNING OF STAGE 1A ON USH 151 AND STH 26. THROUGH TRAFFIC WILL BE MAINTAINED ON THE OUTSIDE TRAVEL LANES OF STH 26 DURING CONSTRUCTION OPERATIONS. THE USH 151 AND STH 49 INTERCHANGE WILL BE UTILIZED FOR ALL DETOURS ON STH 26 AND USH 151.

STAGE 1B

THE INSIDE TRAVEL LANES WILL BE CLOSED FROM STA 580+00 TO STA 615+04 FOR BASE PATCHING CONCRETE HES AND MEDIAN CONSTRUCTION OPERATIONS. THE USH 151 AND STH 26 INTERCHANGE RAMPS WILL BE OPEN. THROUGH TRAFFIC WILL BE MAINTAINED ON THE OUTSIDE LANES OF STH 26. THERE WILL BE NO CONCRETE BASE PATCHING OPERATIONS FROM STA 580+00 TO STA 585+72.

STAGE 2A

THE OUTSIDE TRAVEL LANES FROM STA 585+72 TO STA 619+71 WILL BE CLOSED. THE USH 151 AND STH 26 INTERCHANGE RAMPS WILL BE CLOSED FOR 24 HOURS TO COMPLETE CONSTRUCTION OPERATIONS FOR BASE PATCHING CONCRETE SHES LOCATED ON THE OUTSIDE TRAVEL LANES AS SHOWN ON THE PLAN. PCMS MESSAGE WILL BE POSTED 7 DAYS PRIOR TO THE BEGINNING OF STAGE 2A ON USH 151 AND STH 26. THROUGH TRAFFIC WILL BE MAINTAINED ON THE INSIDE TRAVEL LANES OF STH 26 DURING CONSTRUCTION OPERATIONS. THE USH 151 AND STH 49 INTERCHANGE WILL BE UTILIZED FOR ALL DETOURS ON STH 26 AND USH 151.

STAGE 2B

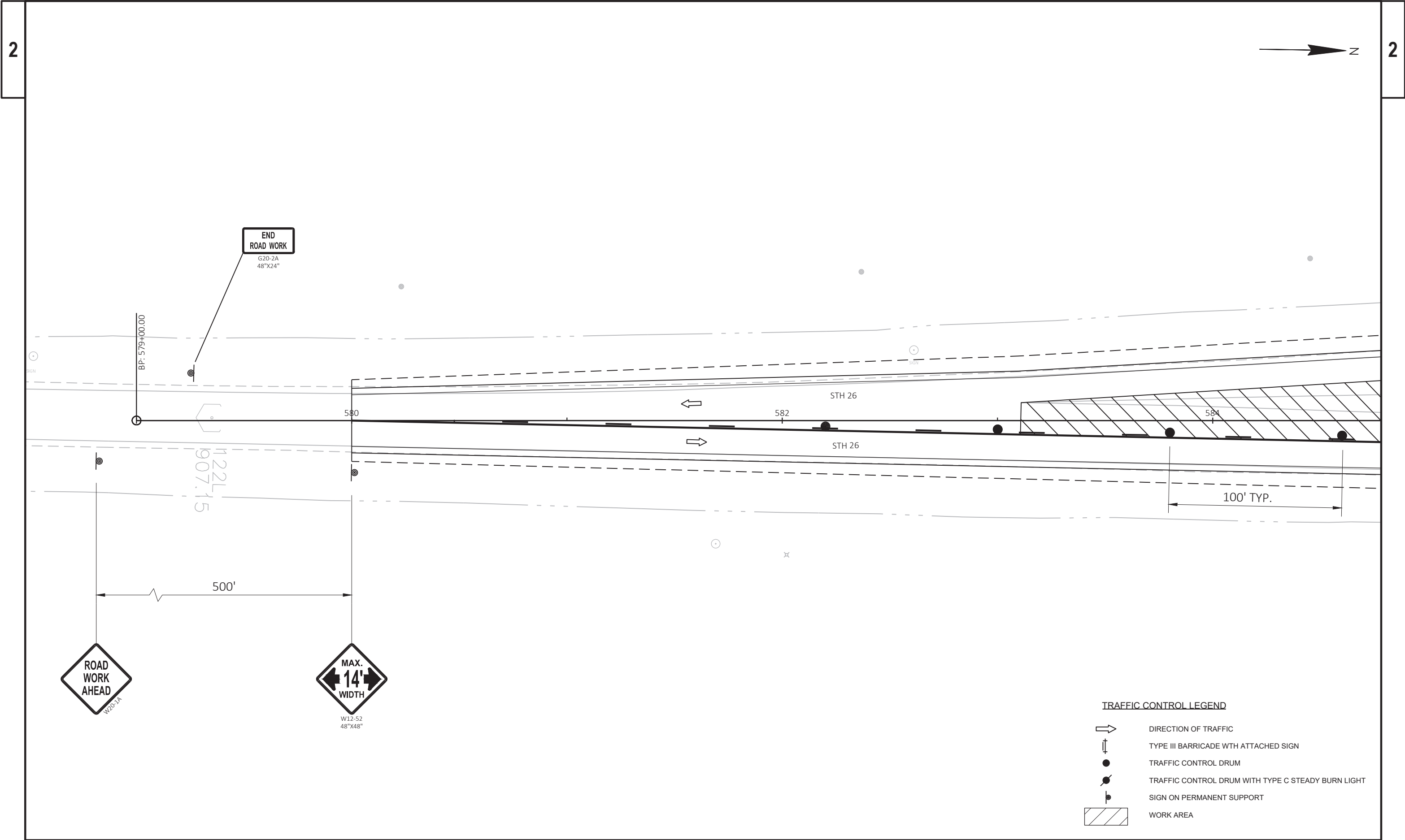
THE OUTSIDE TRAVEL LANES FROM STA 585+72 TO STA 619+71 WILL BE CLOSED FOR BASE PATCHING CONCRETE HES CONSTRUCTION OPERATIONS. THE USH 151 AND STH 26 INTERCHANGE RAMPS WILL BE OPEN. THROUGH TRAFFIC WILL BE MAINTAINED ON THE OUTSIDE LANES OF STH 26. THROUGH TRAFFIC WILL BE MAINTAINED ON THE INSIDE TRAVEL LANES DURING CONCRETE BASE PATCHING OPERATIONS.

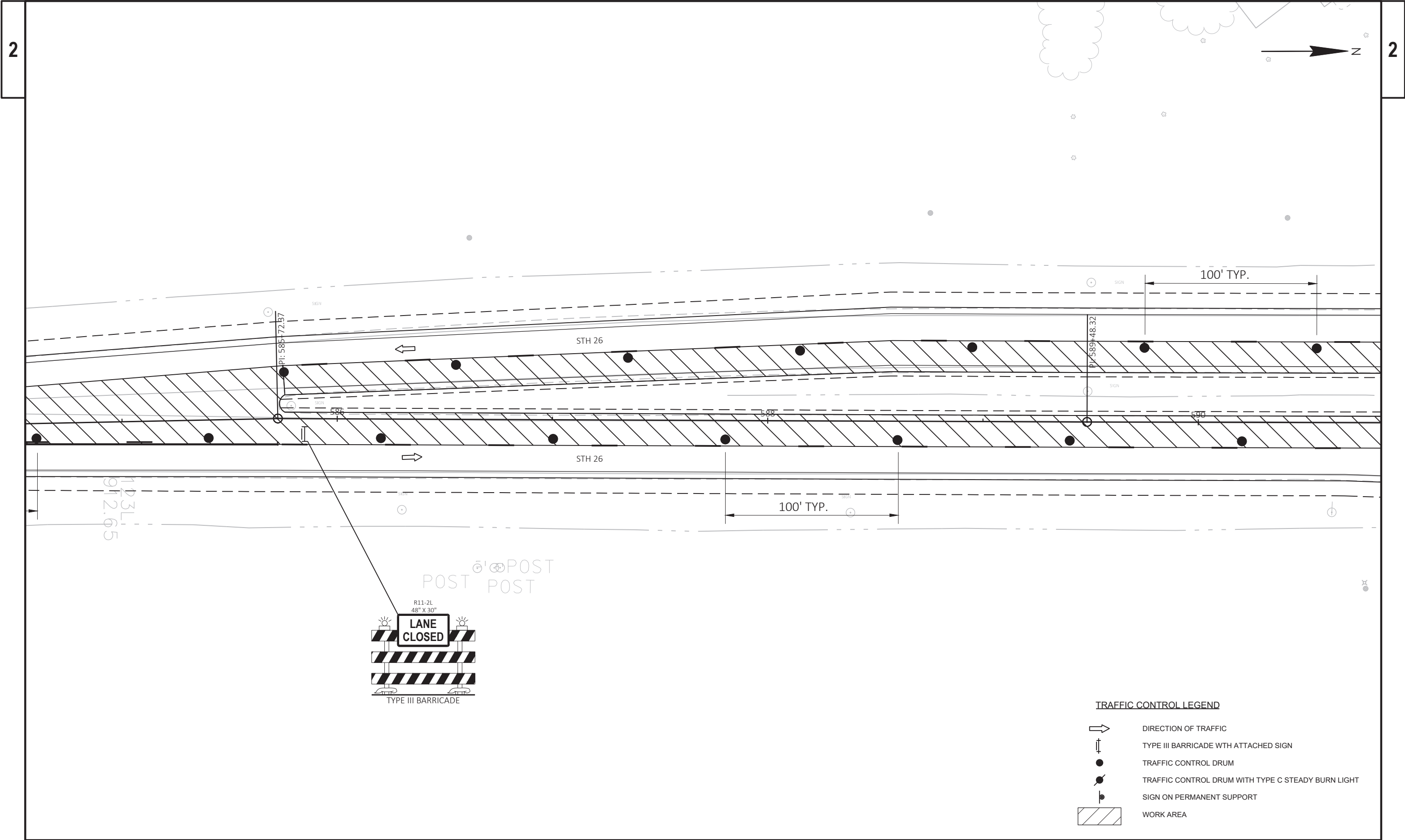
STAGE 3

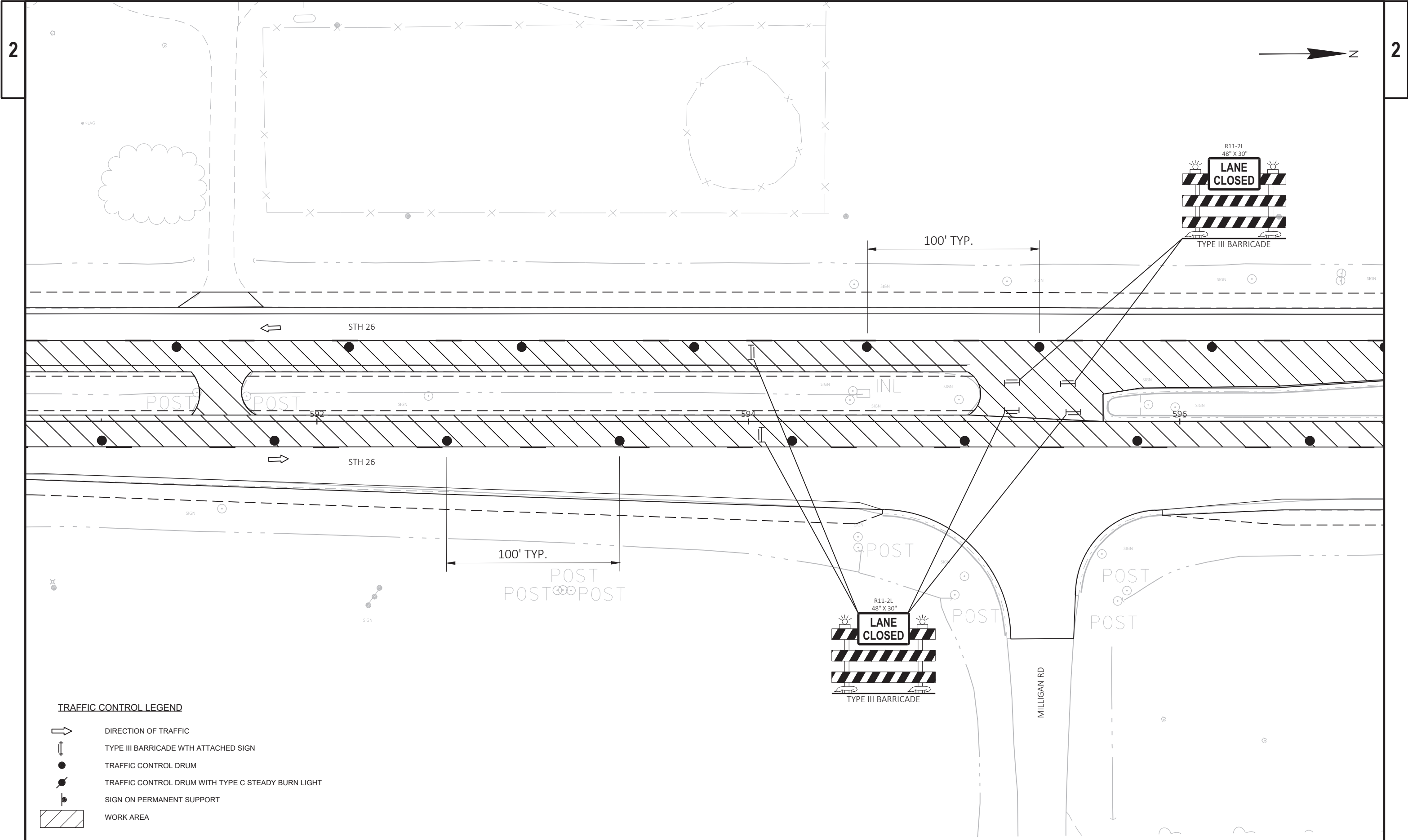
HMA PAVING OPERATIONS WILL BE COMPLETED UNDER FLAGGING FOR THE UNDIVIDED SECTION AND A SINGLE LANE CLOSURE FOR THE DIVIDED SECTION. ALL REMAINING PAVEMENT THAT WAS NOT PAVED IN STAGE 1 FROM STA 580+00 TO STA 585+75 WILL BE PAVED UNDER FLAGGING PER SDD 15C12-A TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION. THERE WILL BE NO CONCRETE BASE PATCHING OPERATIONS WITHIN THIS AREA. ALL CURB RAMPS AND ADJACENT CURB & GUTTER REPLACEMENTS WILL BE COMPLETED PRIOR TO BEGINNING ASPHALT MILLING AND OVERLAYING OPERATIONS IN STAGE 3. FOLLOW SDD 15C12-A TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION FOR UNDIVIDED SECTION OF PROJECT FROM STA 619+71 TO STA 682+07.

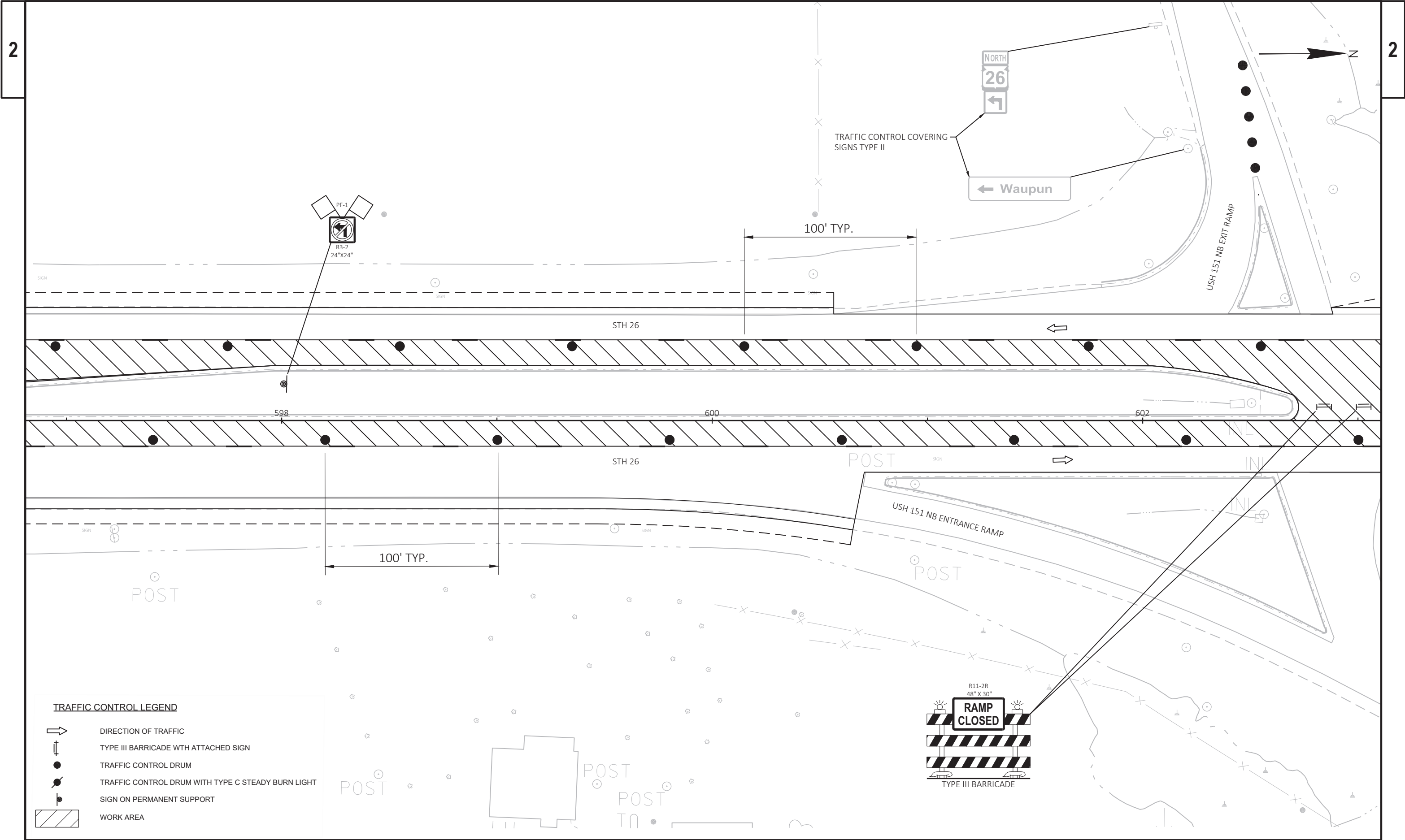
STANDARD DETAIL DRAWINGS

- SDD 15C02 FOR OFF-RAMP & ON-RAMP TRAFFIC CONTROL
- SDD 15C02 ADVANCED WIDTH RESTRICTION SIGNING
- SDD 15C02 TRAFFIC CONTROL FOR ENTRANCE RAMP CLOSURE
- SDD 15C12 TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
- SDD 15D12 TRAFFIC CONTROL LANE CLOSURE
- SDD 15D12 TRAFFIC CONTROL LANE CLOSURE
- SDD 15D16 TRAFFIC CONTROL, EXIT RAMP CLOSURE
- SDD 15D40 TRAFFIC CONTROL, FULL LANE SHIFT, MULTI-LANE DIVIDED 50 MPH AND OVER



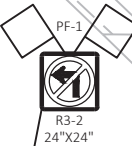
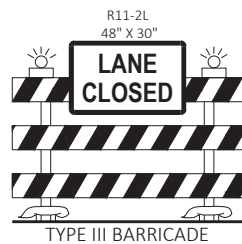






TRAFFIC CONTROL LEGEND

- DIRECTION OF TRAFFIC
- ⏏ TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- SIGN ON PERMANENT SUPPORT
- ▨ WORK AREA

TRAFFIC CONTROL COVERING
SIGNS TYPE II

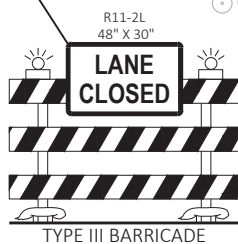
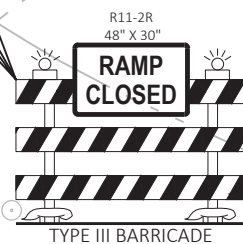
← Fond du Lac

100' TYP.

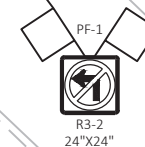
STH 26

STH 26

100' TYP.



← Beaver Dam



USH 151 NB

USH 151 SB

TRAFFIC CONTROL COVERING
SIGNS TYPE II

PROJECT NO: 6235-01-73

HWY: STH 26

COUNTY: DODGE

TRAFFIC CONTROL - STAGE 1

SHEET

E

FILE NAME : N:\PDS\C3D\62350103\SHEETPLAN\025100 TRAFFIC CONTROL\025100-TC-S1.DWG
LAYOUT NAME - 025100-tc-05

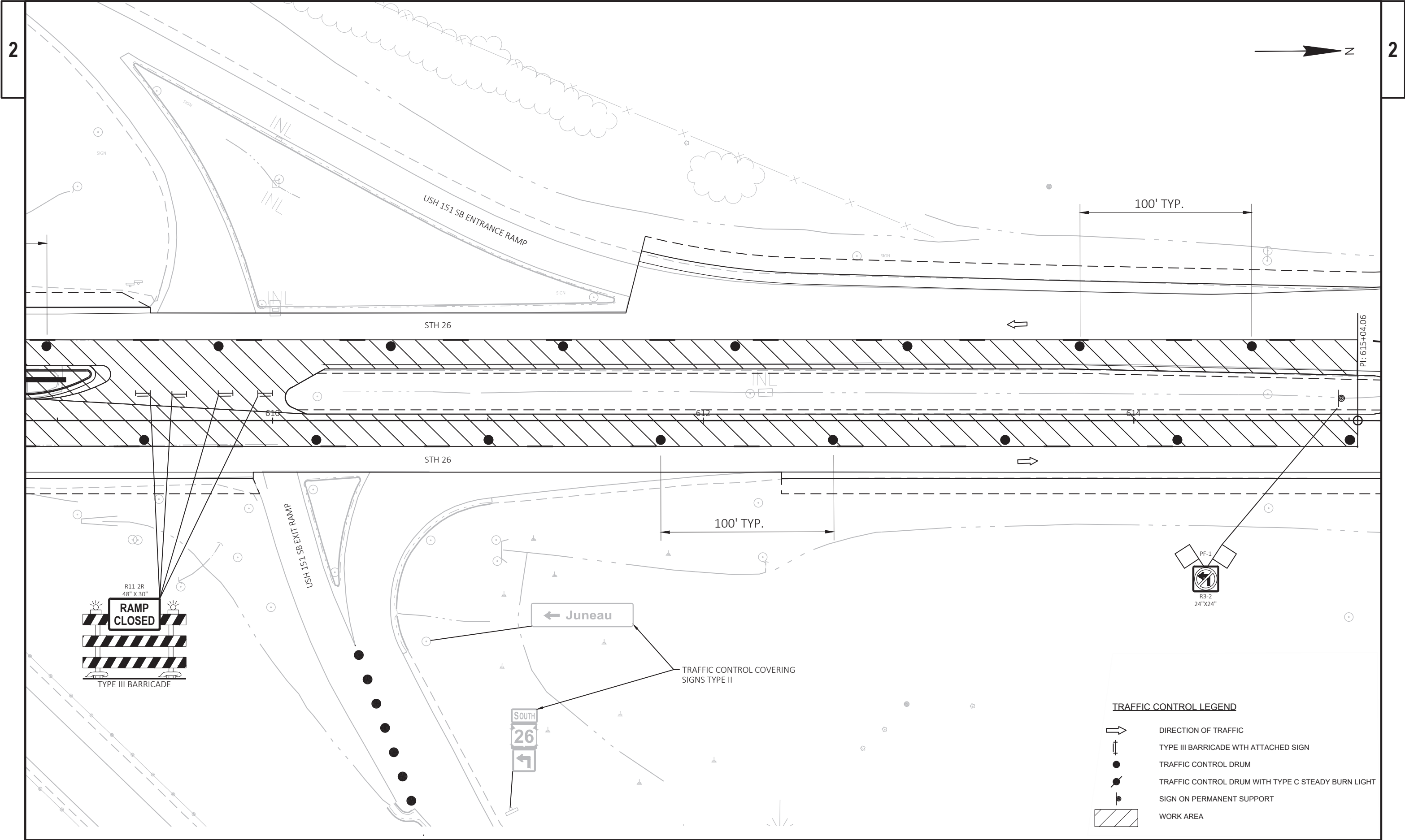
PLOT DATE : 7/2/2025 2:30 PM

PLOT BY : DOYLE, MICKEY R

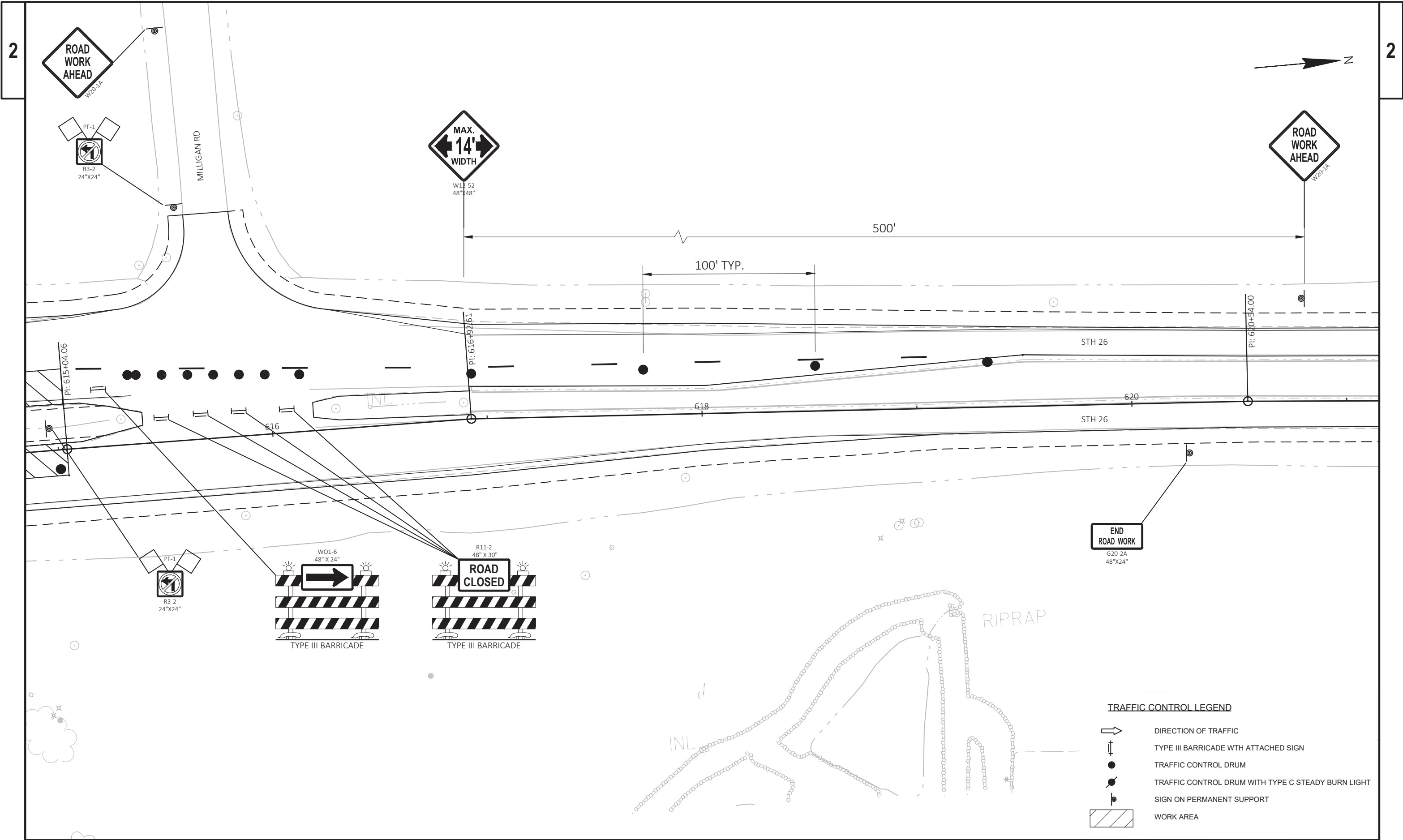
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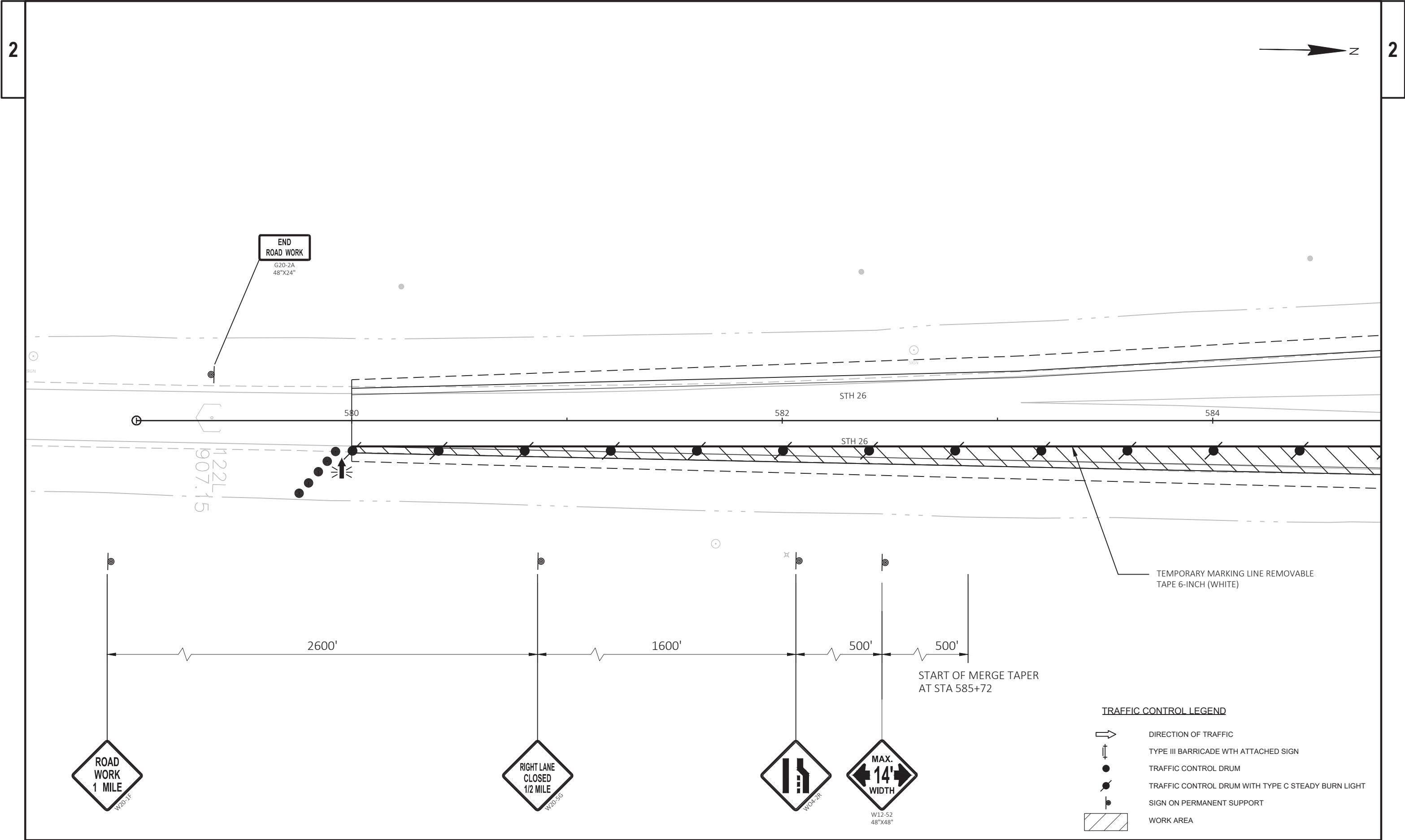
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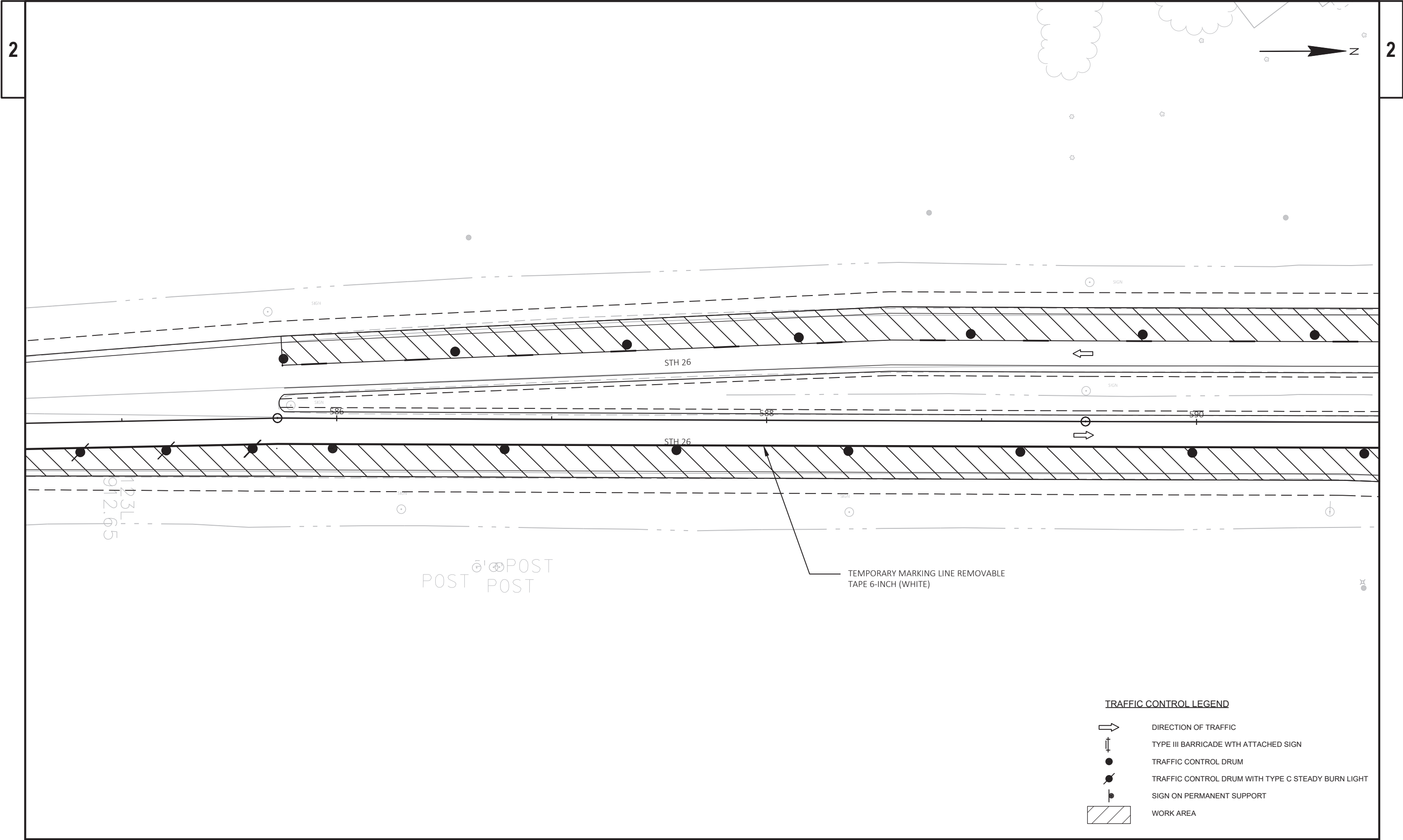
WISDOT/CADDs SHEET 42



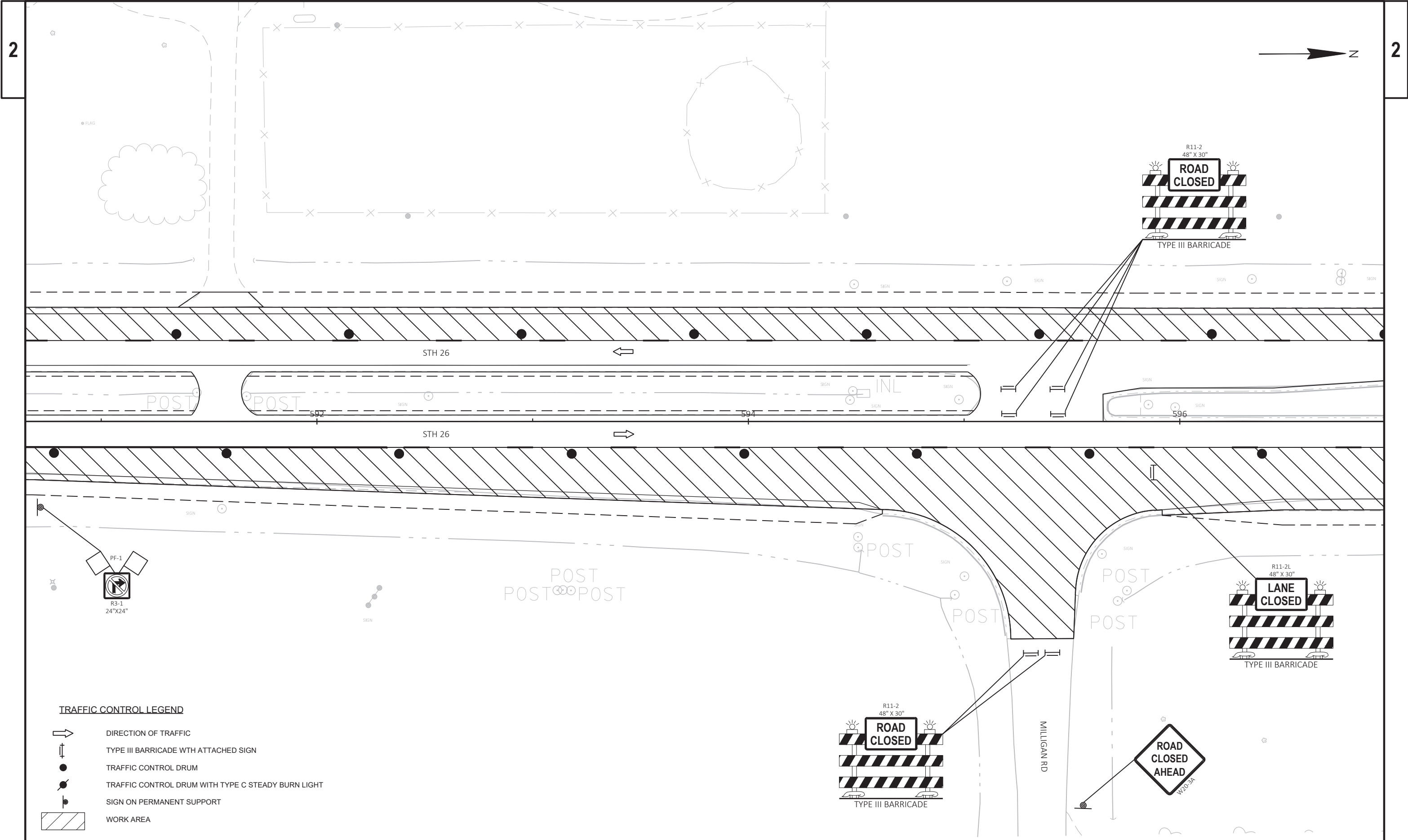
PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	TRAFFIC CONTROL - STAGE 1	SHEET	E
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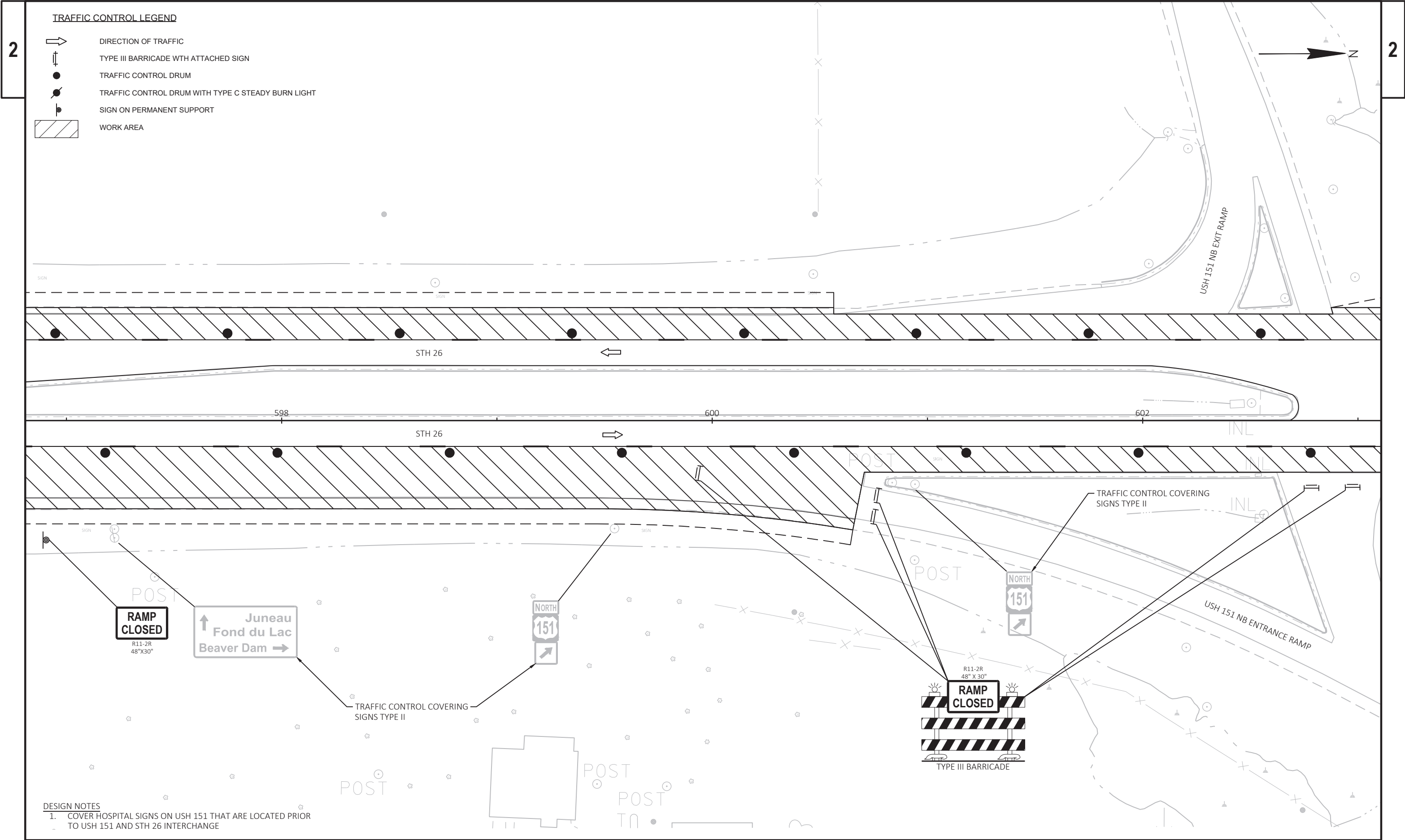




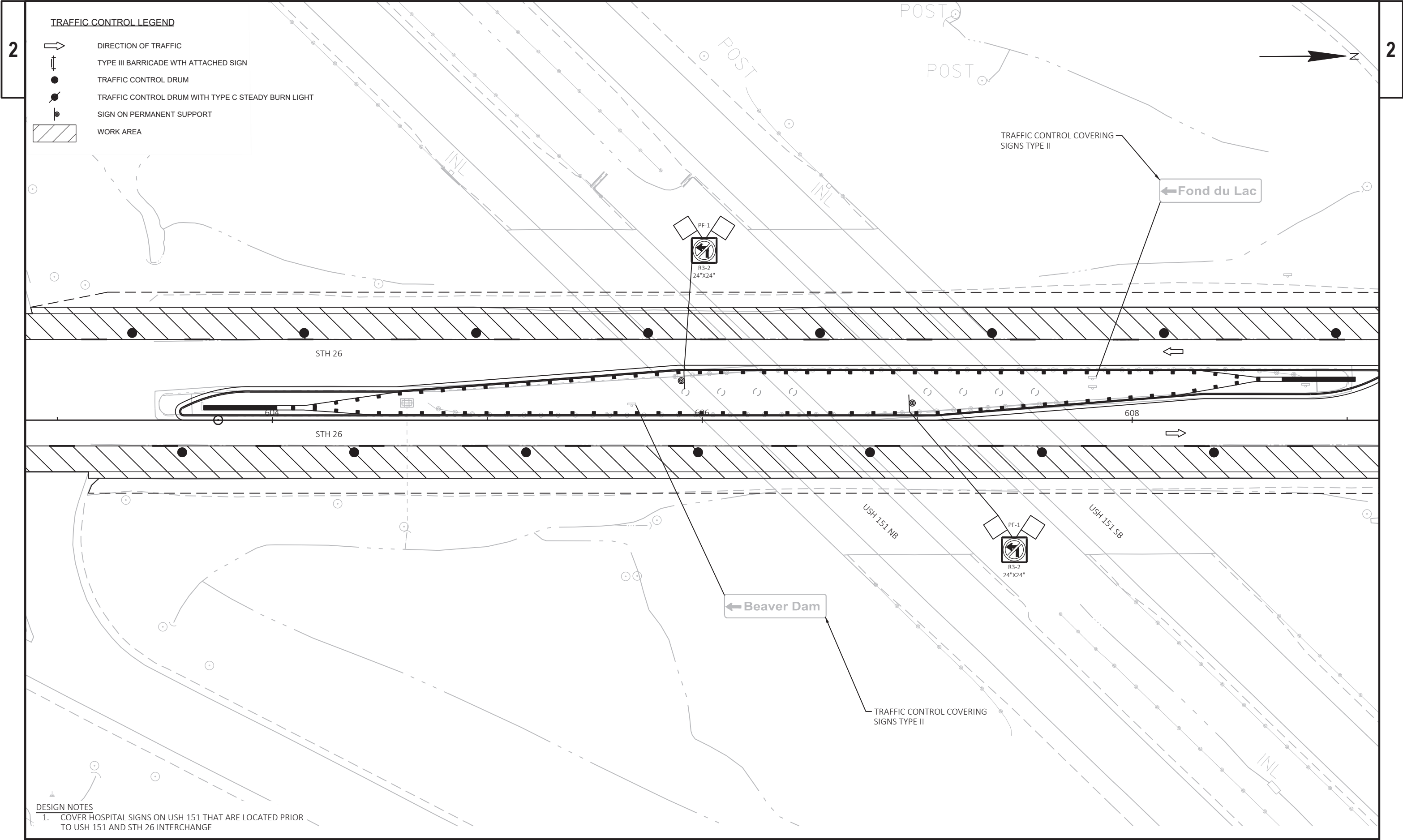


PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	TRAFFIC CONTROL - STAGE 2	SHEET	E
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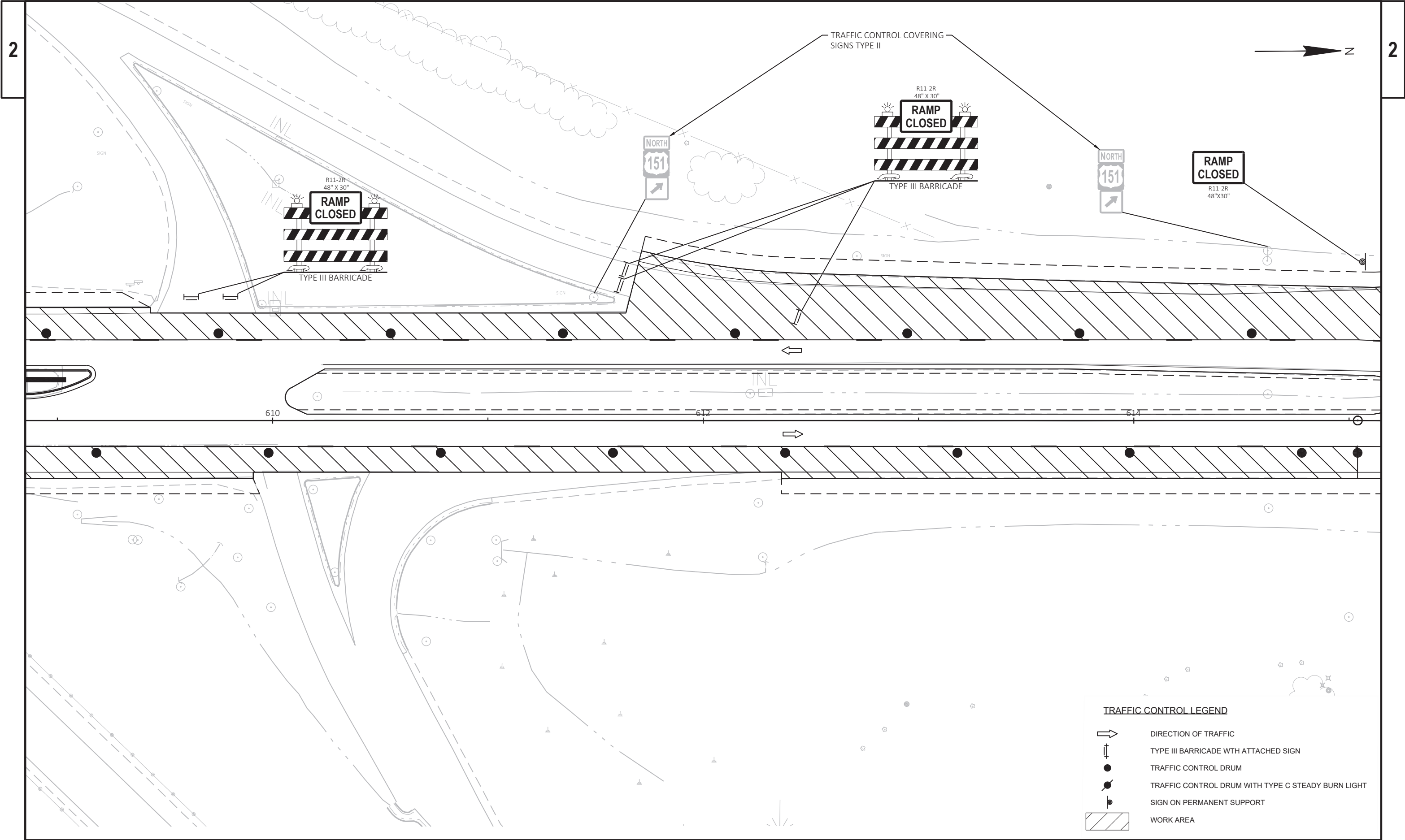


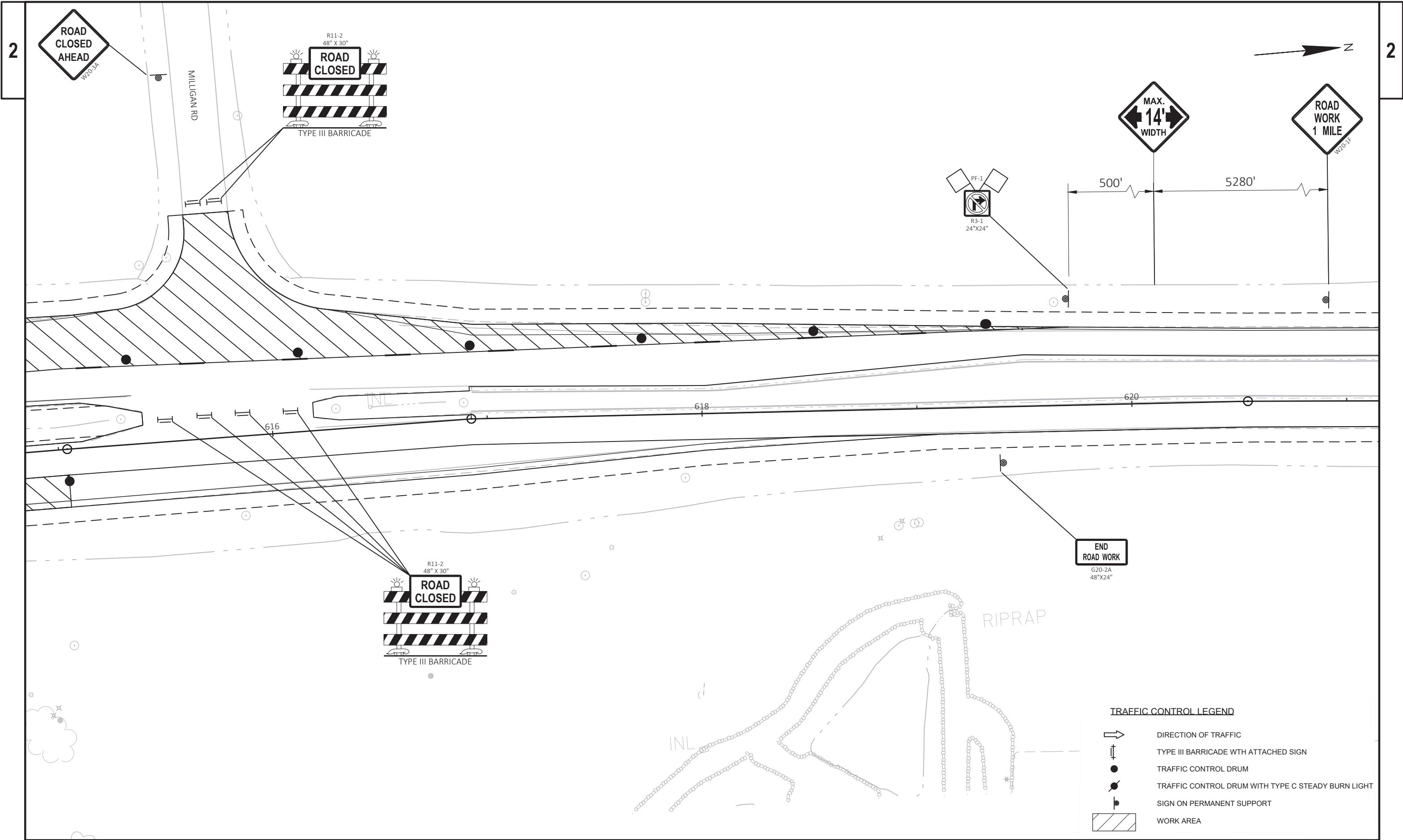
DESIGN NOTES
1. COVER HOSPITAL SIGNS ON USH 151 THAT ARE LOCATED PRIOR TO USH 151 AND STH 26 INTERCHANGE

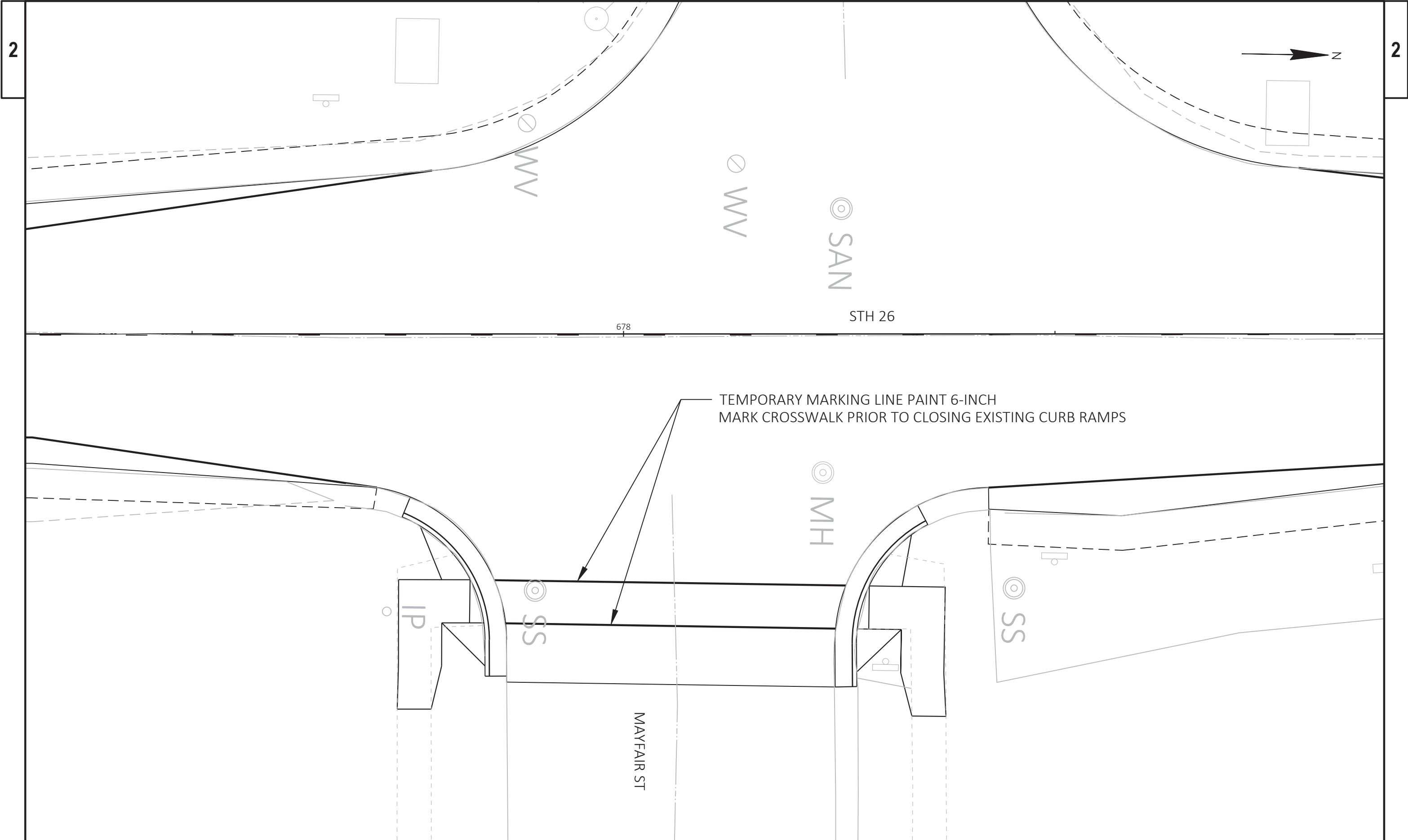


DESIGN NOTES
1. COVER HOSPITAL SIGNS ON USH 151 THAT ARE LOCATED PRIOR TO USH 151 AND STH 26 INTERCHANGE

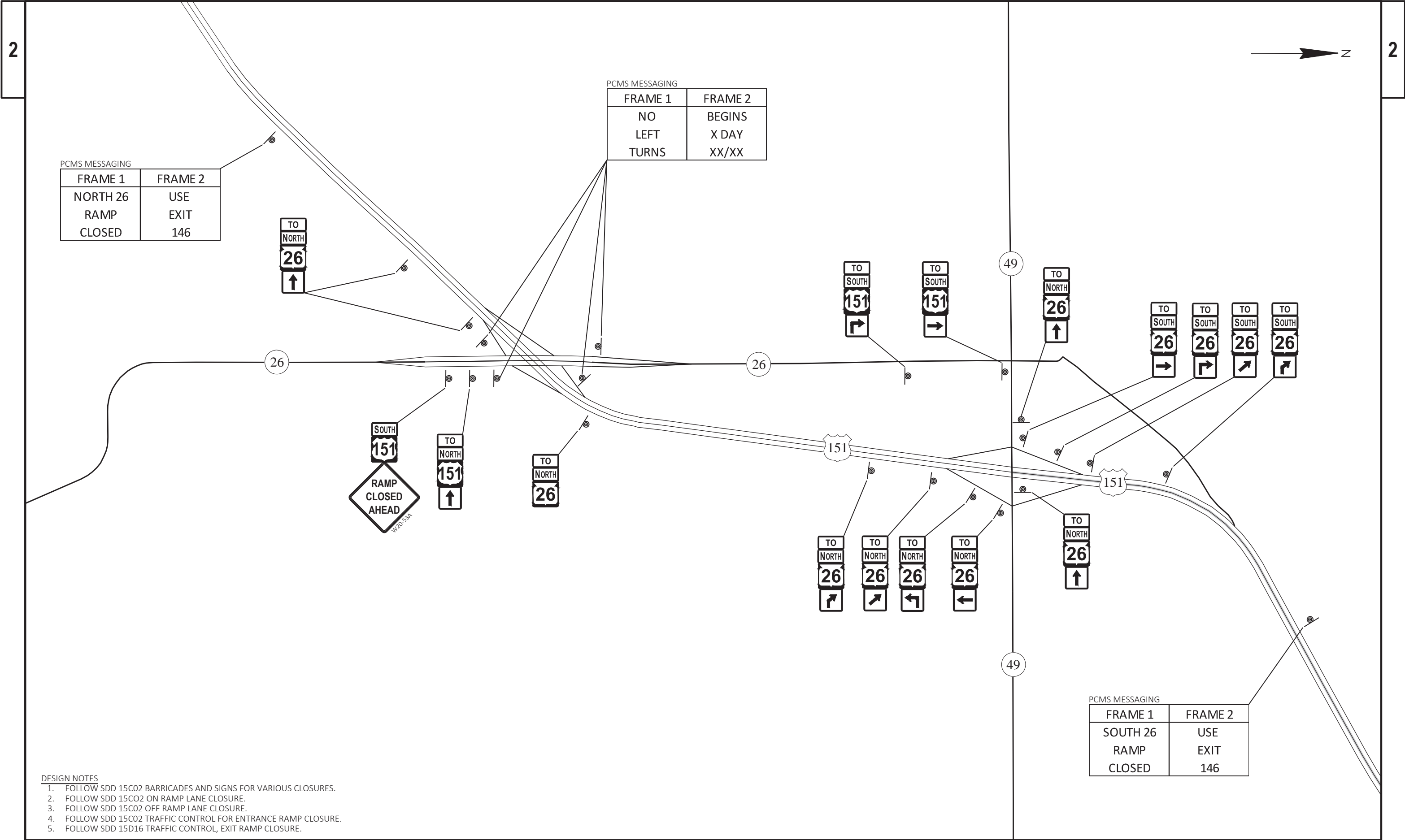
PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	TRAFFIC CONTROL - STAGE 2	SHEET	E
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PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	TEMPORARY PEDESTRIAN ACCOMMODATION	SHEET	E
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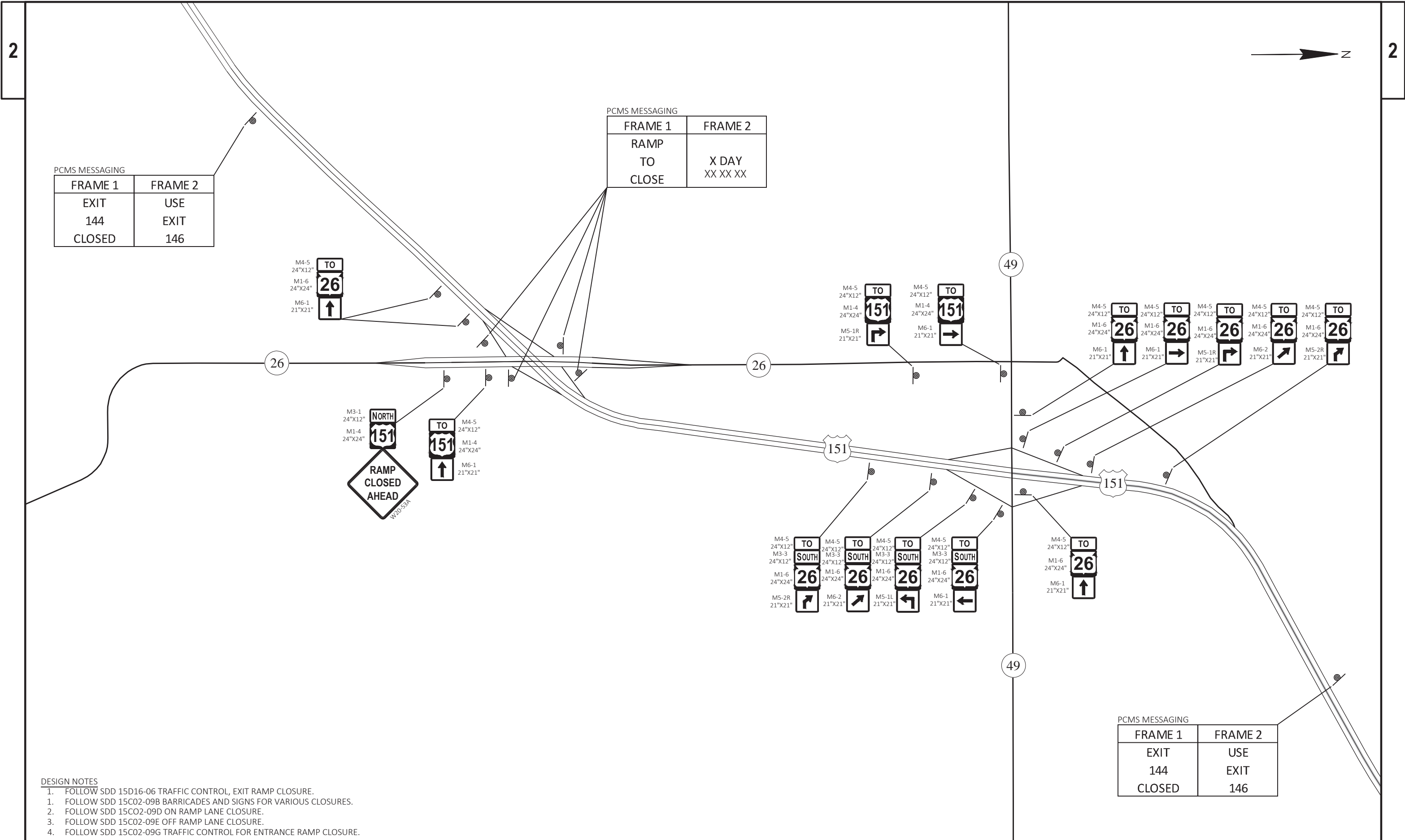


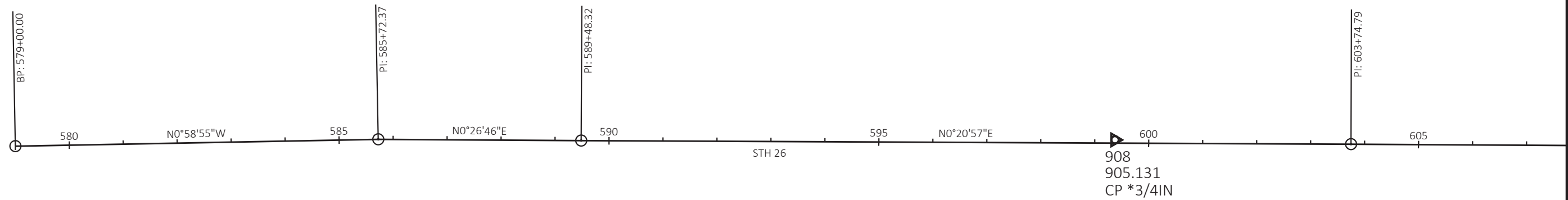
PCMS MESSAGING	
FRAME 1	FRAME 2
NO LEFT TURNS	BEGINS X DAY XX/XX

PCMS MESSAGING	
FRAME 1	FRAME 2
NORTH 26 RAMP CLOSED	USE EXIT 146

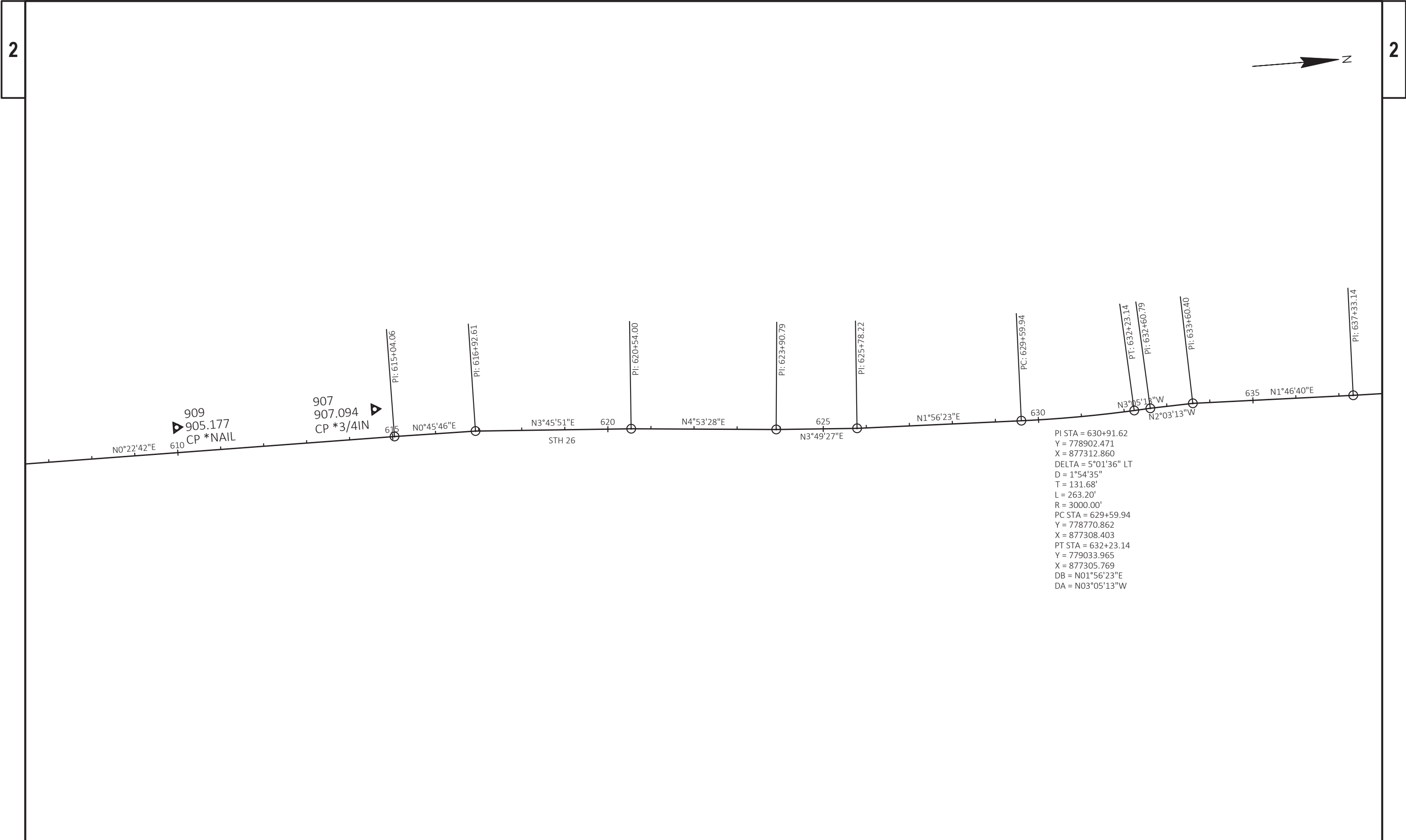
PCMS MESSAGING	
FRAME 1	FRAME 2
SOUTH 26 RAMP CLOSED	USE EXIT 146

- DESIGN NOTES
1. FOLLOW SDD 15C02 BARRICADES AND SIGNS FOR VARIOUS CLOSURES.
 2. FOLLOW SDD 15C02 ON RAMP LANE CLOSURE.
 3. FOLLOW SDD 15C02 OFF RAMP LANE CLOSURE.
 4. FOLLOW SDD 15C02 TRAFFIC CONTROL FOR ENTRANCE RAMP CLOSURE.
 5. FOLLOW SDD 15D16 TRAFFIC CONTROL, EXIT RAMP CLOSURE.

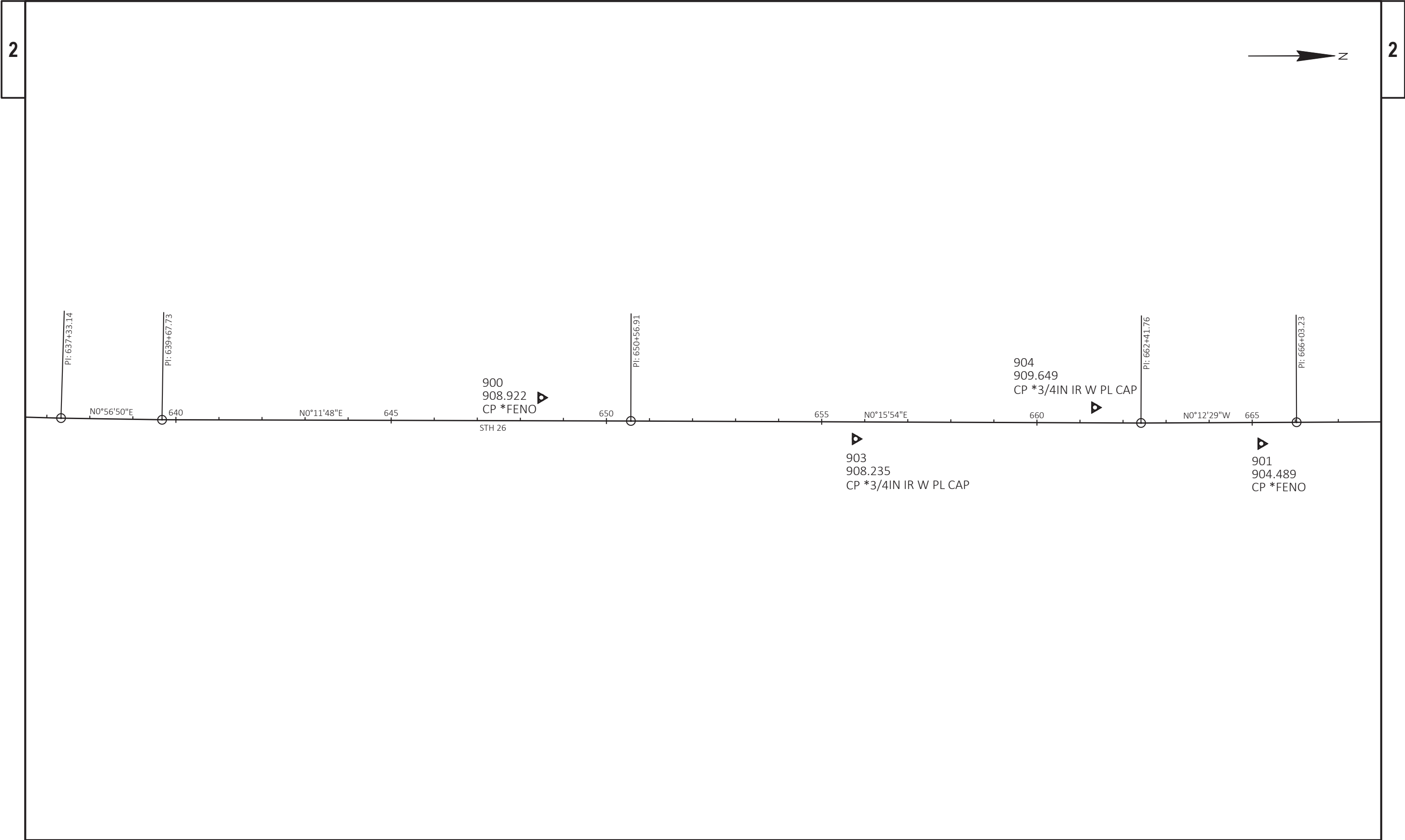




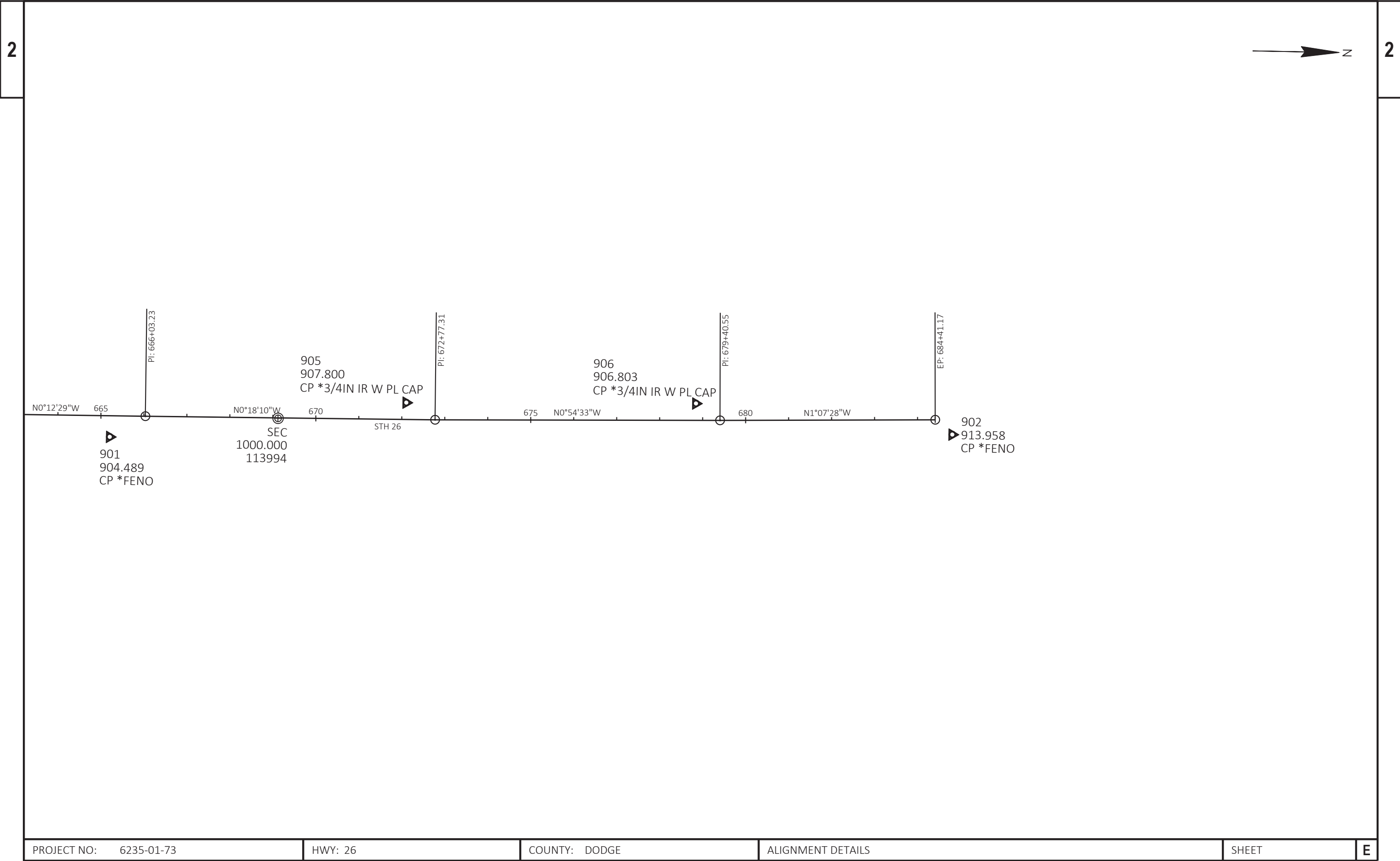
PROJECT NO: 6235-01-73	HWY: 26	COUNTY: DODGE	ALIGNMENT DETAILS	SHEET	E
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PROJECT NO: 6235-01-73	HWY: 26	COUNTY: DODGE	ALIGNMENT DETAILS	SHEET	E
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PROJECT NO: 6235-01-73	HWY: 26	COUNTY: DODGE	ALIGNMENT DETAILS	SHEET	E
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Estimate Of Quantities

6235-01-73

Line	Item	Item Description	Unit	Total	Qty
0002	204.0110	Removing Asphaltic Surface	SY	652.000	652.000
0004	204.0115	Removing Asphaltic Surface Butt Joints	SY	76.000	76.000
0006	204.0120	Removing Asphaltic Surface Milling	SY	51,556.000	51,556.000
0008	204.0150	Removing Curb & Gutter	LF	1,328.000	1,328.000
0010	204.0155	Removing Concrete Sidewalk	SY	76.000	76.000
0012	204.0165	Removing Guardrail	LF	607.000	607.000
0014	205.0100	Excavation Common	CY	44.000	44.000
0016	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 6235-01-73	EACH	1.000	1.000
0018	213.0100	Finishing Roadway (project) 01. 6235-01-73	EACH	1.000	1.000
0020	305.0110	Base Aggregate Dense 3/4-Inch	TON	5.000	5.000
0022	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	10.000	10.000
0024	305.0500	Shaping Shoulders	STA	204.000	204.000
0026	311.0110	Breaker Run	TON	60.000	60.000
0028	390.0100	Removing Pavement for Base Patching	CY	280.000	280.000
0030	390.0305	Base Patching Concrete HES	CY	244.000	244.000
0032	390.0405	Base Patching Concrete SHES	CY	36.000	36.000
0034	416.0620	Drilled Dowel Bars	EACH	888.000	888.000
0036	450.4000	HMA Cold Weather Paving	TON	2,287.000	2,287.000
0038	455.0605	Tack Coat	GAL	5,912.000	5,912.000
0040	460.2000	Incentive Density HMA Pavement	DOL	5,150.000	5,150.000
0042	460.6424	HMA Pavement 4 MT 58-28 H	TON	5,460.000	5,460.000
0044	460.6425	HMA Pavement 5 MT 58-28 H	TON	3,691.000	3,691.000
0046	465.0105	Asphaltic Surface	TON	14.000	14.000
0048	465.0110	Asphaltic Surface Patching	TON	20.000	20.000
0050	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	56.000	56.000
0052	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	119.000	119.000
0054	601.0415	Concrete Curb & Gutter 6-Inch Sloped 30-Inch Type J	LF	67.000	67.000
0056	601.0576	Concrete Curb & Gutter 4-Inch Sloped 30-Inch Type J	LF	1,152.000	1,152.000
0058	601.0600	Concrete Curb Pedestrian	LF	5.000	5.000
0060	602.0410	Concrete Sidewalk 5-Inch	SF	785.000	785.000
0062	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	60.000	60.000
0064	604.0500	Slope Paving Crushed Aggregate	SY	471.000	471.000
0066	611.8115	Adjusting Inlet Covers	EACH	1.000	1.000
0068	614.0397	Guardrail Mow Strip Emulsified Asphalt	SY	437.000	437.000
0070	614.2300	MGS Guardrail 3	LF	887.500	887.500
0072	619.1000	Mobilization	EACH	1.000	1.000
0074	620.0300	Concrete Median Sloped Nose	SF	488.000	488.000
0076	624.0100	Water	MGAL	0.300	0.300
0078	625.0500	Salvaged Topsoil	SY	101.000	101.000
0080	627.0200	Mulching	SY	101.000	101.000
0082	628.1905	Mobilizations Erosion Control	EACH	3.000	3.000
0084	628.1910	Mobilizations Emergency Erosion Control	EACH	3.000	3.000
0086	628.2002	Erosion Mat Class I Type A	SY	103.000	103.000
0088	628.7010	Inlet Protection Type B	EACH	1.000	1.000
0090	628.7020	Inlet Protection Type D	EACH	10.000	10.000
0092	628.7555	Culvert Pipe Checks	EACH	1.000	1.000
0094	629.0210	Fertilizer Type B	CWT	1.000	1.000
0096	630.0130	Seeding Mixture No. 30	LB	2.000	2.000
0098	630.0500	Seed Water	MGAL	3.000	3.000

Estimate Of Quantities

6235-01-73

Line	Item	Item Description	Unit	Total	Qty
0100	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	1.000	1.000
0102	638.2102	Moving Signs Type II	EACH	5.000	5.000
0104	638.4000	Moving Small Sign Supports	EACH	7.000	7.000
0106	642.5001	Field Office Type B	EACH	1.000	1.000
0108	643.0300	Traffic Control Drums	DAY	2,097.000	2,097.000
0110	643.0420	Traffic Control Barricades Type III	DAY	667.000	667.000
0112	643.0705	Traffic Control Warning Lights Type A	DAY	1,246.000	1,246.000
0114	643.0715	Traffic Control Warning Lights Type C	DAY	84.000	84.000
0116	643.0900	Traffic Control Signs	DAY	1,028.000	1,028.000
0118	643.0920	Traffic Control Covering Signs Type II	EACH	14.000	14.000
0120	643.1050	Traffic Control Signs PCMS	DAY	70.000	70.000
0122	643.3180	Temporary Marking Line Removable Tape 6-Inch	LF	11,170.000	11,170.000
0124	643.3305	Temporary Marking Crosswalk Paint 6-inch	LF	80.000	80.000
0126	643.5000	Traffic Control	EACH	1.000	1.000
0128	646.2040	Marking Line Grooved Wet Ref Epoxy 6-Inch	LF	33,143.000	33,143.000
0130	646.4040	Marking Line Grooved Wet Ref Epoxy 10-Inch	LF	833.000	833.000
0132	646.7120	Marking Diagonal Epoxy 12-Inch	LF	126.000	126.000
0134	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	80.000	80.000
0136	646.8220	Marking Island Nose Epoxy	EACH	6.000	6.000
0138	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	1,338.000	1,338.000
0140	650.8000	Construction Staking Resurfacing Reference	LF	10,191.000	10,191.000
0142	650.9000	Construction Staking Curb Ramps	EACH	6.000	6.000
0144	680.0100	Public Land Reference Monument Verify and Reset	EACH	1.000	1.000
0146	690.0150	Sawing Asphalt	LF	2,310.000	2,310.000
0148	690.0250	Sawing Concrete	LF	2,304.000	2,304.000
0150	740.0440	Incentive IRI Ride	DOL	9,210.000	9,210.000
0152	SPV.0060	Special 01. Median Beam Guard Terminal	EACH	2.000	2.000
0154	SPV.0060	Special 02. Property Boundary Monuments	EACH	18.000	18.000
0156	SPV.0060	Special 03. Right of Way Monuments	EACH	4.000	4.000
0158	SPV.0180	Special 01. Removing Distressed Pavement Milling	SY	2,581.000	2,581.000

REMOVAL ITEMS													
					204.0110	204.0115	204.0120	204.0150	204.0155	204.0165	390.0100	SPV.0180. SPECIAL (01.REMOVING DISTRESSED PAVEMENT MILLING)	
CATEGORY	ROADWAY	STATION TO	STATION	LOCATION	REMOVING ASPHALTIC SURFACE SY	REMOVING ASPHALTIC SURFACE BUTT JOINTS SY	REMOVING ASPHALTIC SURFACE MILLING SY	REMOVING CURB & GUTTER LF	REMOVING CONCRETE SIDEWALK SY	REMOVING GUARDRAIL LF	REMOVING PAVEMENT FOR BASE PATCHING CY		REMARKS
STAGE 1B													
0010	STH 26	595+92 -	615+04	LT/RT	253	--	--	1139	--	607	--	--	MEDIAN
STAGE 3													
0010	STH 26	580+00 -	595+92	LT/RT	24	7	10,649	--	--	--	--	533	BEGIN PROJECT TO MILLIGAN RD
0010	STH 26	595+92 -	615+04	LT/RT	--	7	13,877	--	--	--	--	694	MILLIGAN RD TO MILLIGAN RD
0010	STH 26	615+04 -	633+60	LT/RT	53	--	8,071	--	--	--	--	404	MILLIGAN RD TO END DIVIDED SECTION
0010	STH 26	633+60 -	643+80	LT/RT	107	5	3,769	--	--	--	--	189	END DIVIDED SECTION TO KUENZI DR
0010	STH 26	643+80 -	652+08	LT/RT	24	9	3,895	66	26	--	--	195	KUENZI DR TO BUWALDA DR
0010	STH 26	652+08 -	655+19	LT/RT	0	7	1,110	--	--	--	--	56	BUWALDA DR TO BARNES ST
0010	STH 26	655+19 -	663+33	LT/RT	19	9	3,378	68	25	--	--	169	BARNES ST TO CLAGGETT AVE
0010	STH 26	663+33 -	677+31	LT/RT	148	14	4,751	--	--	--	--	238	CLAGGETT AVE TO MAYFAIR ST
0010	STH 26	677+31 -	682+07	LT/RT	24	18	2,056	55	25	--	--	103	MAYFAIR ST TO END PROJECT
0010	STH 26	580+00 -	682+07	PROJECT	--	--	--	--	--	--	280	--	PROJECT
TOTAL 0010					652	76	51,556	1,328	76	607	280	2,581	

EXCAVATION COMMON ITEM				
CATEGORY	ROADWAY	LOCATION	205.0100 EXCAVATION COMMON CY	REMARKS
0010	STH 26	LT	25	STH 26 MEDIAN
0010	STH 26	RT	4	BULWALDA SE QUADRANT
0010	STH 26	RT	3	BULWALDA NE QUADRANT
0010	STH 26	RT	4	CLAGGETT SE QUADRANT
0010	STH 26	RT	3	CLAGGETT NE QUADRANT
0010	STH 26	RT	3	MAYFAIR SE QUADRANT
0010	STH 26	RT	2	MAYFAIR NE QUADRANT
TOTAL 0010			44	

PAVING ITEMS							
					211.0101.01 PREPARE FOUNDATION FOR ASPHALTIC PAVING (PROJECT) (01. 62350173)	213.0100.01 FINISHING ROADWAY (PROJECT) (01. 62350173)	465.0110 ASPHALTIC SURFACE PATCHING
CATEGORY	ROADWAY	STATION TO	STATION	LOCATION	EACH	EACH	TON
0010	STH 26	580+00	-	682+07	LT/RT	1	20
TOTAL 0010					1	1	20

BASE AGGREGATE DENSE ITEMS

						305.0110	305.0120	305.0500	311.0110	624.0100		
						BASE	BASE					
						AGGREGATE	AGGREGATE					
						DENSE 3/4-INCH	DENSE 1 1/4-INCH	SHAPING				
CATEGORY	ROADWAY	STATION	TO	STATION	LOCATION	TON	TON	STAs	BREAKER RUN	WATER	REMARKS	
									TON	MGAL		
0010	STH 26	580+00	-	682+07	LT	--	--	102	--	--	N/A	
0010	STH 26	580+00	-	682+07	RT	--	--	102	--	--	N/A	
0010	STH 26	580+00	-	682+07	LT/RT	5	--	--	--	0.1	UNDISTRIBUTED	
0010	STH 26	603+45	-	603+86	LT	--	10	--	60	0.2	MEDIAN	
TOTAL 0010						5	10	204	60	0.3		

ASPHALTIC PAVEMENT ITEMS

						450.4000	455.0605	460.2000	460.6424	460.6425	465.0105	465.0120	604.0500	614.0397
						HMA COLD WEATHER PAVING TON	TACK COAT GAL	INCENTIVE DENSITY HMA PAVEMENT DOL	HMA PAVEMENT 4 MT 58-28 H TON	HMA PAVEMENT 5 MT 58-28 H TON	ASPHALTIC SURFACE TON	ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES TON	SLOPE PAVING CRUSHED AGGREGATE SY	GUARDRAIL MOW STRIP EMULSIFIED ASPHALT SY
CATEGORY	ROADWAY	STATION	TO	STATION	LOCATION									
STAGE 1B														
0010	STH 26	603+55	-	607+17	LT/RT	--	--	--	--	--	14	--	471	437
STAGE 3														
0010	STH 26	580+00	-	595+92	LT/RT	--	1,285	--	1,049	750	--	--	--	--
0010	STH 26	595+92	-	615+04	LT/RT	--	1,666	--	1,360	972	--	--	--	--
0010	STH 26	615+04	-	633+60	LT/RT	--	581	--	--	581	--	--	--	--
0010	STH 26	633+60	-	643+80	LT/RT	--	453	--	581	264	--	--	--	--
0010	STH 26	643+80	-	652+08	LT/RT	--	468	--	600	273	--	--	--	--
0010	STH 26	652+08	-	655+19	LT/RT	--	134	--	171	78	--	--	--	--
0010	STH 26	655+19	-	663+33	LT/RT	--	507	--	650	296	--	--	--	--
0010	STH 26	663+33	-	677+31	LT/RT	--	571	--	732	333	--	--	--	--
0010	STH 26	677+31	-	682+07	LT/RT	--	247	--	317	144	--	--	--	--
0010	STH 26	580+00	-	682+07	PROJECT	2,287	--	5,150	--	--	--	56	--	--
TOTAL 0010						2,287	5,912	5,150	5,460	3,691	14	56	471	437

BASE PATCHING CONCRETE ITEMS								
CATEGORY	ROADWAY	STATION	TO	STATION	LOCATION	390.0305	390.0405	416.0620
						BASE PATCHING	BASE PATCHING	DRILLED DOWEL
						CONCRETE HES	CONCRETE SHES	BARS
						CY	CY	EACH
REMARKS								
STAGE 1A								
0010	STH 26	603+47	-	603+53	RT	--	2	8
STH 26 NB INSIDE LANE								
0010	STH 26	603+98	-	604+04	RT	--	2	8
STH 26 NB INSIDE LANE								
0010	STH 26	604+16	-	604+22	RT	--	2	8
STH 26 NB INSIDE LANE								
0010	STH 26	604+35	-	604+41	RT	--	2	8
STH 26 NB INSIDE LANE								
0010	STH 26	609+86	-	609+92	RT	--	2	8
STH 26 NB INSIDE LANE								
0010	STH 26	602+09	-	602+15	LT	--	2	8
STH 26 SB INSIDE LANE								
0010	STH 26	602+22	-	602+50	LT	--	2	8
STH 26 SB INSIDE LANE								
0010	STH 26	608+53	-	608+59	LT	--	2	8
STH 26 SB INSIDE LANE								
0010	STH 26	609+17	-	609+23	LT	--	2	8
STH 26 SB INSIDE LANE								
STAGE 1B								
0010	STH 26	587+16	-	587+22	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	594+40	-	594+55	RT	5	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	594+87	-	594+93	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	595+32	-	595+42	RT	4	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	597+35	-	597+41	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	598+65	-	598+71	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	599+78	-	599+84	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	600+00	-	600+06	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	601+14	-	301+20	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	601+32	-	601+38	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	601+53	-	601+59	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	602+62	-	602+68	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	604+90	-	604+96	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	605+14	-	605+20	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	607+68	-	607+74	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	609+66	-	609+72	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	610+19	-	610+25	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	612+17	-	612+37	RT	7	--	--
STH 26 NB INSIDE LANE								
0010	STH 26	612+81	-	612+87	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	615+06	-	615+21	RT	2	--	8
STH 26 NB INSIDE LANE								
0010	STH 26	587+11	-	587+17	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	588+97	-	589+03	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	589+72	-	589+78	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	590+08	-	590+14	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	591+39	-	591+45	LT	2	--	8
STH 26 SB INSIDE LANE								

BASE PATCHING CONCRETE ITEMS								
CATEGORY	ROADWAY	STATION	TO	STATION	LOCATION	390.0305	390.0405	416.0620
						BASE PATCHING	BASE PATCHING	DRILLED DOWEL
						CONCRETE HES	CONCRETE SHES	BARS
						CY	CY	EACH
REMARKS								
0010	STH 26	592+14	-	592+20	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	592+77	-	592+83	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	594+38	-	594+44	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	596+17	-	596+23	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	596+47	-	596+53	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	597+39	-	597+45	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	598+39	-	598+45	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	599+76	-	599+82	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	599+44	-	599+50	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	603+69	-	603+76	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	605+17	-	605+23	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	607+24	-	607+30	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	607+39	-	607+45	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	607+64	-	607+70	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	611+92	-	612+02	LT	4	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	613+17	-	613+23	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	613+59	-	613+65	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	614+39	-	614+45	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	614+86	-	614+92	LT	2	--	8
STH 26 SB INSIDE LANE								
0010	STH 26	615+34	-	615+40	LT	2	--	8
STH 26 SB INSIDE LANE								
						102	18	424

BASE PATCHING CONCRETE ITEMS									
CATEGORY	ROADWAY	STATION	TO	STATION	LOCATION	390.0305	390.0405	416.0620	REMARKS
						BASE PATCHING	BASE PATCHING	DRILLED DOWEL	
						CONCRETE HES	CONCRETE SHES	BARS	
						CY	CY	EACH	
STAGE 2A									
0010	STH 26	602+94	-	603+00	RT	--	2	8	STH 26 NB OUTSIDE LANE
0010	STH 26	603+47	-	603+53	RT	--	2	8	STH 26 NB OUTSIDE LANE
0010	STH 26	603+98	-	604+04	RT	--	2	8	STH 26 NB OUTSIDE LANE
0010	STH 26	604+16	-	604+22	RT	--	2	8	STH 26 NB OUTSIDE LANE
0010	STH 26	604+35	-	604+41	RT	--	2	8	STH 26 NB OUTSIDE LANE
0010	STH 26	609+86	-	609+92	RT	--	2	8	STH 26 NB OUTSIDE LANE
0010	STH 26	602+09	-	602+15	LT	--	2	8	STH 26 SB OUTSIDE LANE
0010	STH 26	608+53	-	608+59	LT	--	2	8	STH 26 SB OUTSIDE LANE
0010	STH 26	609+17	-	609+23	LT	--	2	8	STH 26 SB OUTSIDE LANE
STAGE 2B									
0010	STH 26	587+16	-	587+22	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	590+57	-	590+63	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	591+72	-	592+07	RT	12	--	--	STH 26 NB OUTSIDE LANE
0010	STH 26	592+87	-	593+17	RT	10	--	--	STH 26 NB OUTSIDE LANE
0010	STH 26	594+40	-	594+55	RT	5	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	594+87	-	594+93	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	595+32	-	595+42	RT	4	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	597+35	-	597+41	RT	2	--	8	STH 26 NB OUTSIDE LANE & TURN LANE
0010	STH 26	598+65	-	598+71	RT	2	--	8	STH 26 NB OUTSIDE LANE & TURN LANE
0010	STH 26	599+78	-	599+84	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	600+00	-	600+06	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	600+55	-	600+70	RT	5	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	601+14	-	301+20	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	601+32	-	601+38	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	601+53	-	601+59	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	601+82	-	601+88	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	602+62	-	602+68	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	604+90	-	604+96	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	605+14	-	605+20	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	607+68	-	607+74	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	608+87	-	608+93	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	609+66	-	609+72	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	610+19	-	610+25	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	612+17	-	612+37	RT	7	--	--	STH 26 NB OUTSIDE LANE
0010	STH 26	612+81	-	612+87	RT	2	--	8	STH 26 NB OUTSIDE LANE

BASE PATCHING CONCRETE ITEMS								
CATEGORY	ROADWAY	STATION TO	STATION	LOCATION	390.0305	390.0405	416.0620	REMARKS
					BASE PATCHING CONCRETE HES	BASE PATCHING CONCRETE SHES	DRILLED DOWEL BARS EACH	
0010	STH 26	615+06	- 615+21	RT	2	--	8	STH 26 NB OUTSIDE LANE
0010	STH 26	587+11	- 587+17	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	588+97	- 589+03	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	589+72	- 589+78	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	590+08	- 590+14	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	591+39	- 591+45	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	592+14	- 592+20	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	592+77	- 592+83	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	594+38	- 594+44	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	595+18	- 595+24	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	596+17	- 596+23	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	596+47	- 596+53	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	597+39	- 597+45	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	598+39	- 598+45	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	599+76	- 599+82	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	599+44	- 599+50	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	602+22	- 602+50	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	603+69	- 603+76	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	605+17	- 605+23	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	607+24	- 607+30	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	607+39	- 607+45	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	607+64	- 607+70	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	611+92	- 612+02	LT	4	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	613+17	- 613+23	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	614+39	- 614+45	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	614+86	- 614+92	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	615+34	- 615+40	LT	2	--	8	STH 26 SB OUTSIDE LANE
0010	STH 26	616+67	- 616+85	LT	6	--	--	STH 26 SB TURN LANE
TOTAL 0010					244	36	888	

CONCRETE ITEMS										
CATEGORY	ROADWAY	LOCATION	601.0411	601.0415	601.0576	601.0600	602.0410	602.0505	620.0300	REMARKS
			CONCRETE CURB & GUTTER 30- INCH TYPE D	CONCRETE CURB & GUTTER 6- INCH SLOPED 30- INCH TYPE J	CONCRETE CURB & GUTTER 4- INCH SLOPED 30- INCH TYPE J	CONCRETE CURB PEDESTRIAN	CONCRETE SIDEWALK 5- INCH	DETECTABLE WARNING FIELD YELLOW	CONCRETE MEDIAN SLOPED NOSE	
			LF	LF	LF	LF	SF	SF	SF	
STAGE 1B										
0010	STH 26	LT	--	--	1,152	--	--	--	488	MEDIAN - STH 26 UNDERPASS
STAGE 3										
0010	STH 26	RT	--	33	--	--	169	10	--	BUWALDA DR INTERSECTION - SE QUAD
0010	STH 26	RT	--	34	--	--	144	10	--	BUWALDA DR INTERSECTION - NE QUAD
0010	STH 26	RT	38	--	--	--	151	10	--	CLAGGETT AVE INTERSECTION - SE QUAD
0010	STH 26	RT	31	--	--	--	127	10	--	CLAGGETT AVE INTERSECTION - NE QUAD
0010	STH 26	RT	25	--	--	--	95	10	--	MAYFAIR ST INTERSECTION - SE QUAD
0010	STH 26	RT	25	--	--	5	99	10	--	MAYFAIR ST INTERSECTION - NE QUAD
TOTAL 0010			119	67	1,152	5	785	60	488	

GUARDRAIL ITEMS									
CATEGORY	ROADWAY	STATION	TO	STATION	LOCATION	614.2300	SPV.0060.	REMARKS	
						MGS GUARDRAIL	SPECIAL (01.		
						3	MEDIAN BEAM		
						LF	GUARD	EACH	
							TERMINAL)		
STAGE 1B									
0010	STH 26	604+15	-	608+58	LT	443.75	--		STH 26 NB
0010	STH 26	604+15	-	608+58	LT	443.75	--		STH 26 SB
0010	STH 26	603+68	-	604+15	LT	--	1		MEDIAN BEAM GUARD TERMINAL
0010	STH 26	608+58	-	609+04	LT	--	1		MEDIAN BEAM GUARD TERMINAL
TOTAL 0010						887.5	2		

RESTORATION ITEMS							
CATEGORY	ROADWAY	625.0500 SALVAGED TOPSOIL SY	627.0200 MULCHING SY	629.0210 FERTILIZER TYPE B CWT	630.0130 SEEDING MIXTURE NO. 30 LB	630.0500 SEED WATER MGAL	REMARKS
0010	STH 26	101	101	1	2	3	PROJECT
TOTAL 0010		101	101	1	2	3	

PROJECT ITEMS											
CATEGORY	ROADWAY	STATION TO	STATION	LOCATION	619.1000 MOBILIZATION EACH	642.5001 FIELD OFFICE TYPE B EACH	680.0100 PUBLIC LAND REFERENCE MONUMENT VERIFY AND RESET EACH	740.0440 INCENTIVE IRI RIDE DOL	SPV.0060.02 SPECIAL (PROPERTY BOUNDARY MONUMENTS) EACH	SPV.0060.03 SPECIAL (RIGHT OF WAY MONUMENTS) EACH	
0010	STH 26	580+00	- 682+07	LT/RT	1	1	1	9,210	18	4	
TOTAL 0010					1	1	1	9,210	18	4	

EROSION CONTROL ITEMS									
CATEGORY	ROADWAY	LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH	628.2002 EROSION MAT CLASS I TYPE A SY	628.7010 INLET PROTECTION TYPE B EACH	628.7020 INLET PROTECTION TYPE D EACH	628.7555 CULVERT PIPE CHECKS EACH	REMARKS
STAGE 1B									
0010	STH 26	LT	--	--	--	1	--	--	STH 26 MEDIAN
STAGE 3									
0010	STH 26	RT	--	--	24	--	--	--	BUWALDA DR INTERSECTION - SE QUAD
0010	STH 26	RT	--	--	17	--	--	--	BUWALDA DR INTERSECTION - NE QUAD
0010	STH 26	RT	--	--	23	--	--	--	CLAGGETT AVE INTERSECTION - SE QUAD
0010	STH 26	RT	--	--	17	--	--	--	CLAGGETT AVE INTERSECTION - NE QUAD
0010	STH 26	RT	--	--	13	--	--	--	MAYFAIR ST INTERSECTION - SE QUAD
0010	STH 26	RT	--	--	9	--	--	--	MAYFAIR ST INTERSECTION - NE QUAD
0010	STH 26	LT/RT	3	3	--	--	10	1	UNDISTRIBUTED
TOTAL 0010			3	3	103	1	10	1	

TRAFFIC CONTROL ITEMS											
CATEGORY	ROADWAY	DAYS	643.0300	643.0420	643.0705	643.0715	643.0900	CYCLES EACH	643.0920	643.1050	643.5000
			TRAFFIC CONTROL	TRAFFIC CONTROL	TRAFFIC CONTROL	TRAFFIC CONTROL	TRAFFIC CONTROL		TRAFFIC CONTROL	TRAFFIC CONTROL	
			DRUMS	BARRICADES TYPE	WARNING LIGHTS	WARNING LIGHTS	SIGNS		COVERING SIGNS	SIGNS PCMS	TRAFFIC CONTROL
			DAY	III DAY	TYPE A DAY	TYPE C DAY	DAY		TYPE II EACH	DAY	EACH
STAGE 1											
0010	STH 26	21	1,396	420	924	--	735	1	6	28	--
STAGE 2											
0010	STH 26	7	441	161	322	84	273	1	8	42	--
STAGE 3											
0010	STH 26	2	260	86	0	--	20	--	--	--	--
0010	STH 26	--	--	--	--	--	--	--	--	--	1
TOTAL 0010		30	2,097	667	1,246	84	1,028	2	14	70	1

PAVEMENT MARKING ITEMS												
CATEGORY	ROADWAY	STATION TO	STATION	LOCATION	643.3180 TEMPORARY MARKING LINE REMOVABLE TAPE 6-INCH LF	643.3305 TEMPORARY MARKING CROSSWALK PAINT 6-INCH LF	646.2040 MARKING LINE GROOVED WET REF EPOXY 6- INCH LF	646.4040 MARKING LINE GROOVED WET REF EPOXY 10- INCH LF	646.7120 MARKING DIAGONAL EPOXY 12-INCH LF	646.7420 MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH LF	646.8220 MARKING ISLAND NOSE EPOXY EACH	REMARKS
STAGE 2												
0010	STH 26	585+72 -	592+32	RT	1,476	--	--	--	--	--	--	LANE CLOSURE TEMPORARY MARKING LINE
STAGE 3												
0010	STH 26	633+60 -	682+07	LT/RT	4,847	--	--	--	--	--	--	LONG LINE LOWER HMA LAYER
0010	STH 26	633+60 -	682+07	LT/RT	4,847	--	--	--	--	--	--	LONG LINE MIDDLE HMA LAYER
0010	STH 26	580+00 -	600+57	LT	--	--	2,059	--	--	--	--	WHITE EDGELINE
0010	STH 26	602+39 -	602+74	LT	--	--	34	--	--	--	--	WHITE EDGELINE
0010	STH 26	602+88 -	609+43	LT	--	--	655	--	--	--	--	WHITE EDGELINE
0010	STH 26	609+92 -	614+44	LT	--	--	452	--	--	--	--	WHITE EDGELINE
0010	STH 26	611+70 -	615+34	LT	--	--	366	--	--	--	--	WHITE EDGELINE
0010	STH 26	616+29 -	624+75	LT	--	--	851	--	--	--	--	WHITE EDGELINE
0010	STH 26	627+25 -	642+47	LT	--	--	1,521	--	--	--	--	WHITE EDGELINE
0010	STH 26	643+56 -	656+07	LT	--	--	1,255	--	--	--	--	WHITE EDGELINE
0010	STH 26	658+00 -	661+81	LT	--	--	382	--	--	--	--	WHITE EDGELINE
0010	STH 26	663+02 -	677+78	LT	--	--	1,478	--	--	--	--	WHITE EDGELINE
0010	STH 26	678+78 -	682+07	LT	--	--	329	--	--	--	--	WHITE EDGELINE
0010	STH 26	580+00 -	594+62	RT	--	--	1,463	--	--	--	--	WHITE EDGELINE
0010	STH 26	595+92 -	600+66	RT	--	--	476	--	--	--	--	WHITE EDGELINE
0010	STH 26	596+47 -	602+62	RT	--	--	615	--	--	--	--	WHITE EDGELINE
0010	STH 26	603+14 -	609+91	RT	--	--	677	--	--	--	--	WHITE EDGELINE
0010	STH 26	610+11 -	610+45	RT	--	--	34	--	--	--	--	WHITE EDGELINE
0010	STH 26	612+36 -	625+21	RT	--	--	1,285	--	--	--	--	WHITE EDGELINE
0010	STH 26	626+88 -	649+98	RT	--	--	2,312	--	--	--	--	WHITE EDGELINE
0010	STH 26	651+24 -	661+62	RT	--	--	1,040	--	--	--	--	WHITE EDGELINE
0010	STH 26	662+96 -	677+71	RT	--	--	1,476	--	--	--	--	WHITE EDGELINE
0010	STH 26	678+42 -	682+07	RT	--	--	365	--	--	--	--	WHITE EDGELINE
0010	STH 26	580+00 -	585+76	RT	--	--	576	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	580+00 -	585+76	RT	--	--	144	--	--	--	--	YELLOW CENTERLINE 12.5' LINE, 37.5' SKIPS
0010	STH 26	585+72 -	595+01	CL	--	--	929	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	595+64 -	602+68	CL	--	--	705	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	603+54 -	608+89	CL	--	--	536	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	610+20 -	615+35	CL	--	--	516	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	616+21 -	621+74	CL	--	--	553	--	--	--	--	YELLOW CENTERLINE
					11,170	0	23,084	0	0	0	0	

PAVEMENT MARKING ITEMS													
CATEGORY	ROADWAY	STATION	TO	STATION	LOCATION	643.3180	643.3305	646.2040	646.4040	646.7120	646.7420	646.8220	REMARKS
						TEMPORARY MARKING LINE REMOVABLE TAPE 6-INCH LF	TEMPORARY MARKING CROSSWALK PAINT 6-INCH LF	MARKING LINE GROOVED WET REF EPOXY 6-INCH LF	MARKING LINE GROOVED WET REF EPOXY 10-INCH LF	MARKING DIAGONAL EPOXY 12-INCH LF	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH LF	MARKING ISLAND NOSE EPOXY EACH	
0010	STH 26	626+94	-	633+60	CL	--	--	667	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	583+11	-	585+72	LT	--	--	523	--	105	--	--	YELLOW CENTERLINE PAINTED MEDIAN
0010	STH 26	585+72	-	595+01	LT	--	--	930	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	595+64	-	602+04	LT	--	--	641	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	603+89	-	609+03	LT	--	--	514	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	603+59	-	608+89	LT	--	--	531	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	610+23	-	615+33	LT	--	--	510	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	616+21	-	625+25	LT	--	--	905	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	621+74	-	625+25	LT	--	--	352	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	626+94	-	633+60	LT	--	--	667	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	632+61	-	633+60	LT	--	--	--	--	21	--	--	PAINTED MEDIAN ISLAND
0010	STH 26	633+60	-	634+10	CL	--	--	100	--	--	--	--	DOUBLE YELLOW CENTERLINE
0010	STH 26	634+10	-	636+43	LT	--	--	233	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	634+10	-	653+99	RT	--	--	498	--	--	--	--	YELLOW CENTERLINE 12.5' LINE, 37.5' SKIPS
0010	STH 26	560+57	-	653+94	LT	--	--	337	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	653+94	-	660+48	CL	--	--	1,321	--	--	--	--	DOUBLE YELLOW CENTERLINE
0010	STH 26	660+48	-	666+97	LT	--	--	650	--	--	--	--	YELLOW CENTERLINE
0010	STH 26	666+97	-	681+27	RT	--	--	520	--	--	--	--	YELLOW CENTERLINE 12.5' LINE, 37.5' SKIPS
0010	STH 26	681+27	-	682+07	CL	--	--	160	--	--	--	--	DOUBLE YELLOW CENTERLINE
0010	STH 26	585+76	-	615+67	RT	--	--	--	--	--	--	--	WHITE TRAVEL LANE 12.5' LINE, 12.5' LAG, 25' SKIP
0010	STH 26	586+72	-	617+83	LT	--	--	--	--	--	--	--	WHITE TRAVEL LANE 12.5' LINE, 12.5' LAG, 25' SKIP
0010	STH 26	650+41	-	650+78	RT	--	--	--	--	--	--	--	BUWALDA DR
0010	STH 26	650+42	-	650+76	RT	--	--	--	--	--	--	--	BUWALDA DR
0010	STH 26	662+26	-	662+66	RT	--	--	--	--	--	--	--	CLAGGETT AVE
0010	STH 26	662+28	-	662+63	RT	--	--	--	--	--	--	--	CLAGGETT AVE
0010	STH 26	677+85	-	678+26	RT	--	--	--	--	--	41	--	MAYFAIR ST
0010	STH 26	677+86	-	678+25	RT	--	80	--	--	--	39	--	MAYFAIR ST
0010	STH 26	593+95	-	594+86	RT	--	--	--	92	--	--	--	NB RT LANE
0010	STH 26	595+67	-	596+45	LT	--	--	--	78	--	--	--	SB LT LANE
0010	STH 26	603+54	-	604+43	LT	--	--	--	89	--	--	--	SB LT LANE
0010	STH 26	608+28	-	608+96	RT	--	--	--	69	--	--	--	NB LT LANE
0010	STH 26	623+36	-	625+25	RT	--	--	--	190	--	--	--	NB RT LANE
0010	STH 26	626+94	-	628+58	LT	--	--	--	165	--	--	--	SB LT LANE
0010	STH 26	648+69	-	650+19	RT	--	--	--	150	--	--	--	NB RT LANE
0010	STH 26	580+00	-	682+07	LT	--	--	--	--	--	--	6	PROJECT
TOTAL 0010						11,170	80	33,143	833	126	80	6	

CONSTRUCTION STAKING ITEMS					
		650.5500	650.8000	650.9000	
		CONSTRUCTION	CONSTRUCTION	CONSTRUCTION	
		STAKING CURB	STAKING	STAKING CURB	
		GUTTER AND	RESURFACING	STAKING CURB	
		CURB & GUTTER	REFERENCE	RAMPS	
CATEGORY	ROADWAY	LF	LF	EACH	REMARKS
0010	STH 26	33	--	1	BUWALDA DR INTERSECTION - SE QUAD
0010	STH 26	34	--	1	BUWALDA DR INTERSECTION - NE QUAD
0010	STH 26	38	--	1	CLAGGETT AVE INTERSECTION - SE QUAD
0010	STH 26	31	--	1	CLAGGETT AVE INTERSECTION - NE QUAD
0010	STH 26	25	--	1	MAYFAIR ST INTERSECTION - SE QUAD
0010	STH 26	25	--	1	MAYFAIR ST INTERSECTION - NE QUAD
0010	STH 26	1152	--	--	MEDIAN
0010	STH 26	--	10,191	--	ENTIRE PROJECT
TOTAL 0010		1,338	10,191	6	

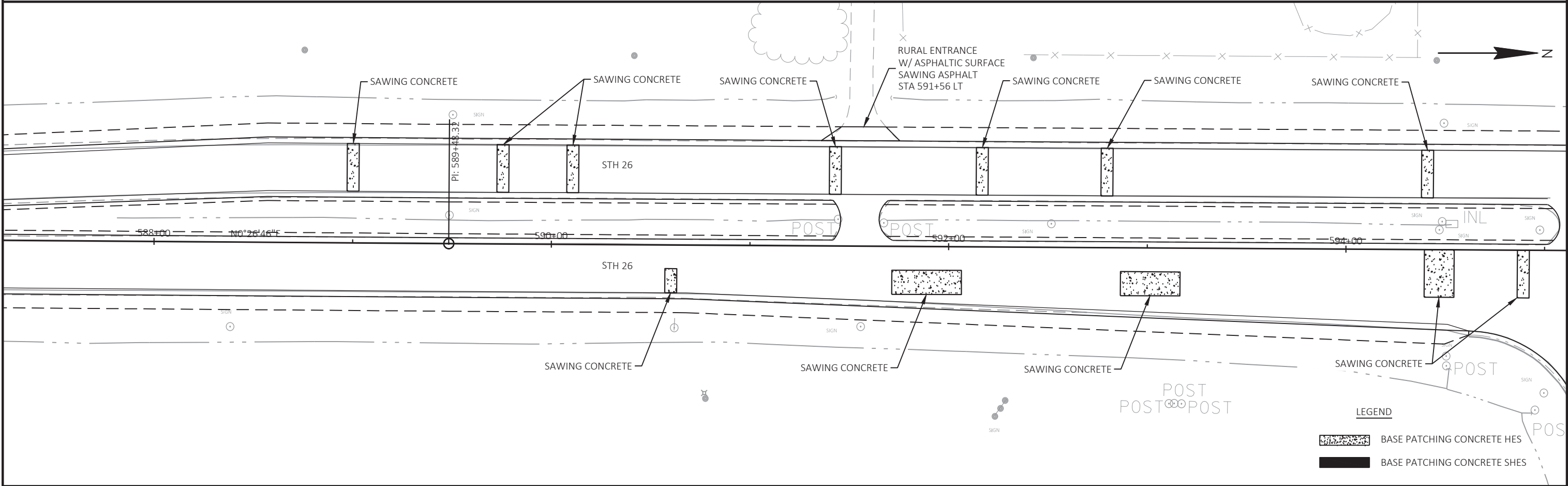
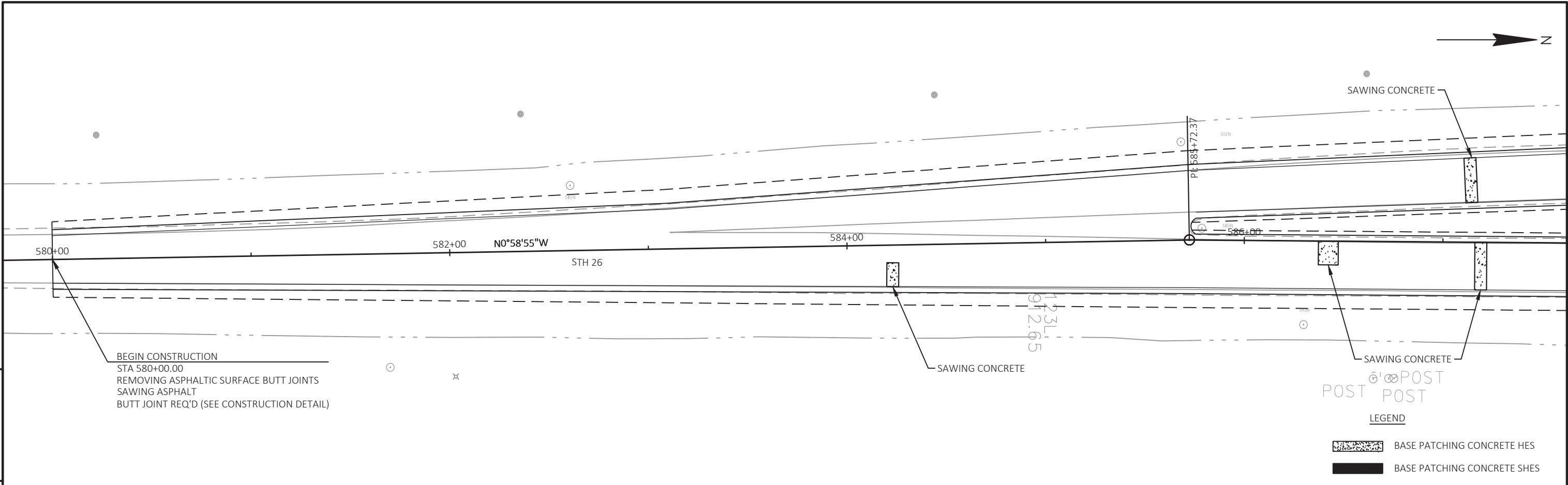
ADJUSTING INLET COVERS ITEM				
			611.8115	
			ADJUSTING	
			INLET COVERS	
CATEGORY	ROADWAY	LOCATION	EACH	REMARKS
0010	STH 26	LT	1	MEDIAN
TOTAL 0010			1	

SIGN ITEMS						
			634.0616	638.2102	638.4000	
			POSTS WOOD			
			4X6-INCH X 16-	MOVING SIGNS	MOVING SMALL	
			FT	TYPE II	SIGN SUPPORTS	
CATEGORY	ROADWAY	LOCATION	EACH	EACH	EACH	REMARKS
0010	STH 26	LT	--	4	6	MEDIAN
0010	STH 26	RT	--	1	1	BUWALDA DR
0010	STH 26	LT/RT	1	--	--	UNDISTRIBUTED
TOTAL 0010			1	5	7	

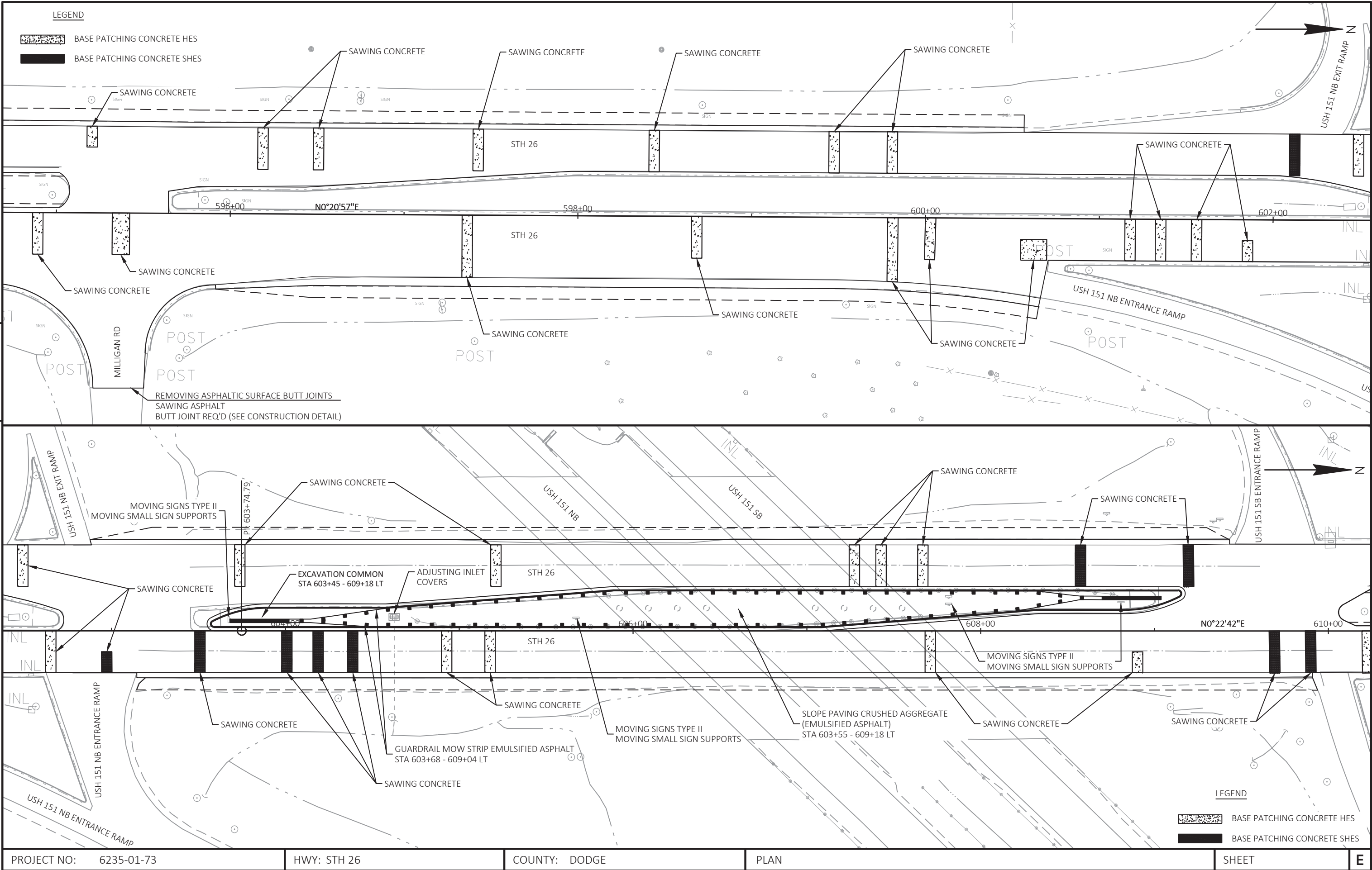
SAWING ITEMS							
					690.0150	690.0250	
					SAWING ASPHALT	SAWING	
						CONCRETE	
CATEGORY	ROADWAY	STATION TO	STATION	LOCATION	LF	LF	REMARKS
0010	STH 26	580+00	- 682+07	PROJECT	2,310	2,304	N/A
TOTAL 0010					2,310	2,304	



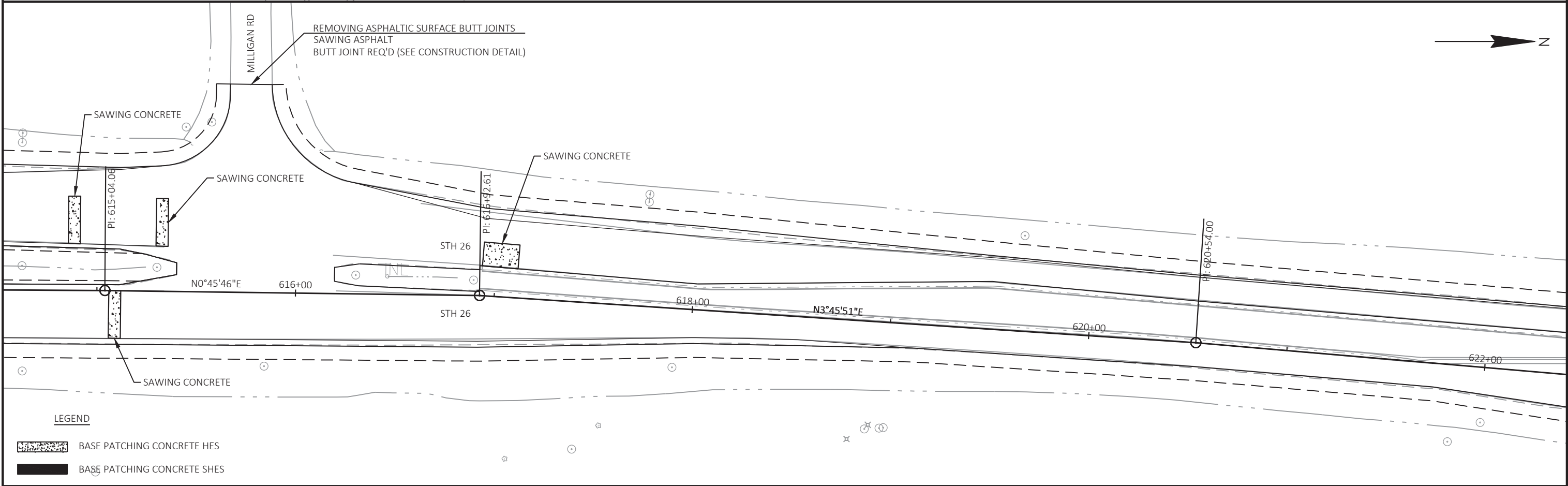
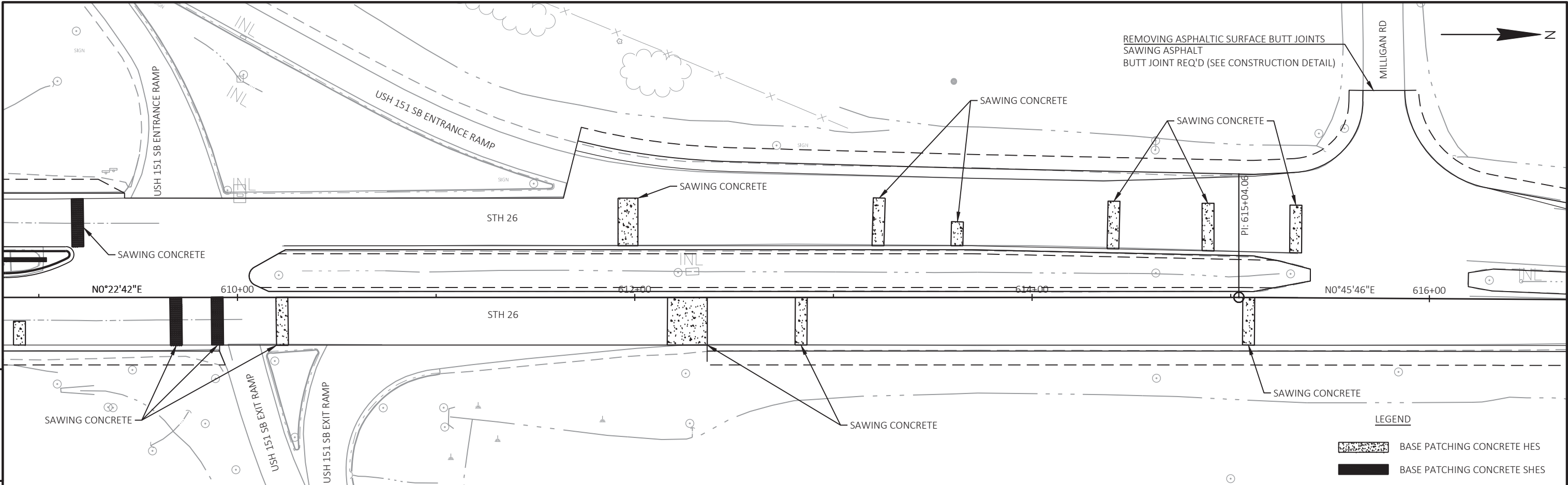




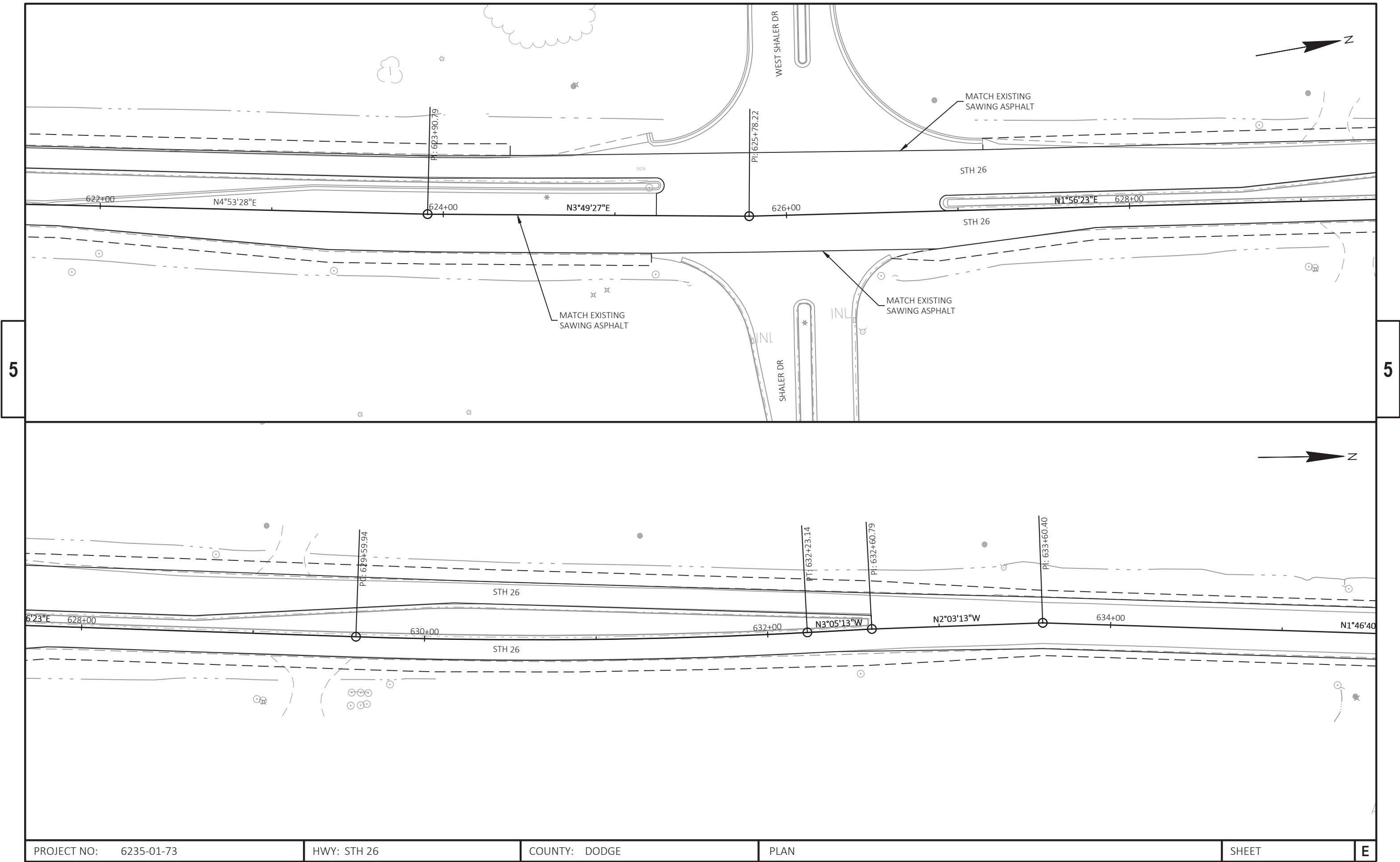
PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	PLAN	SHEET	E
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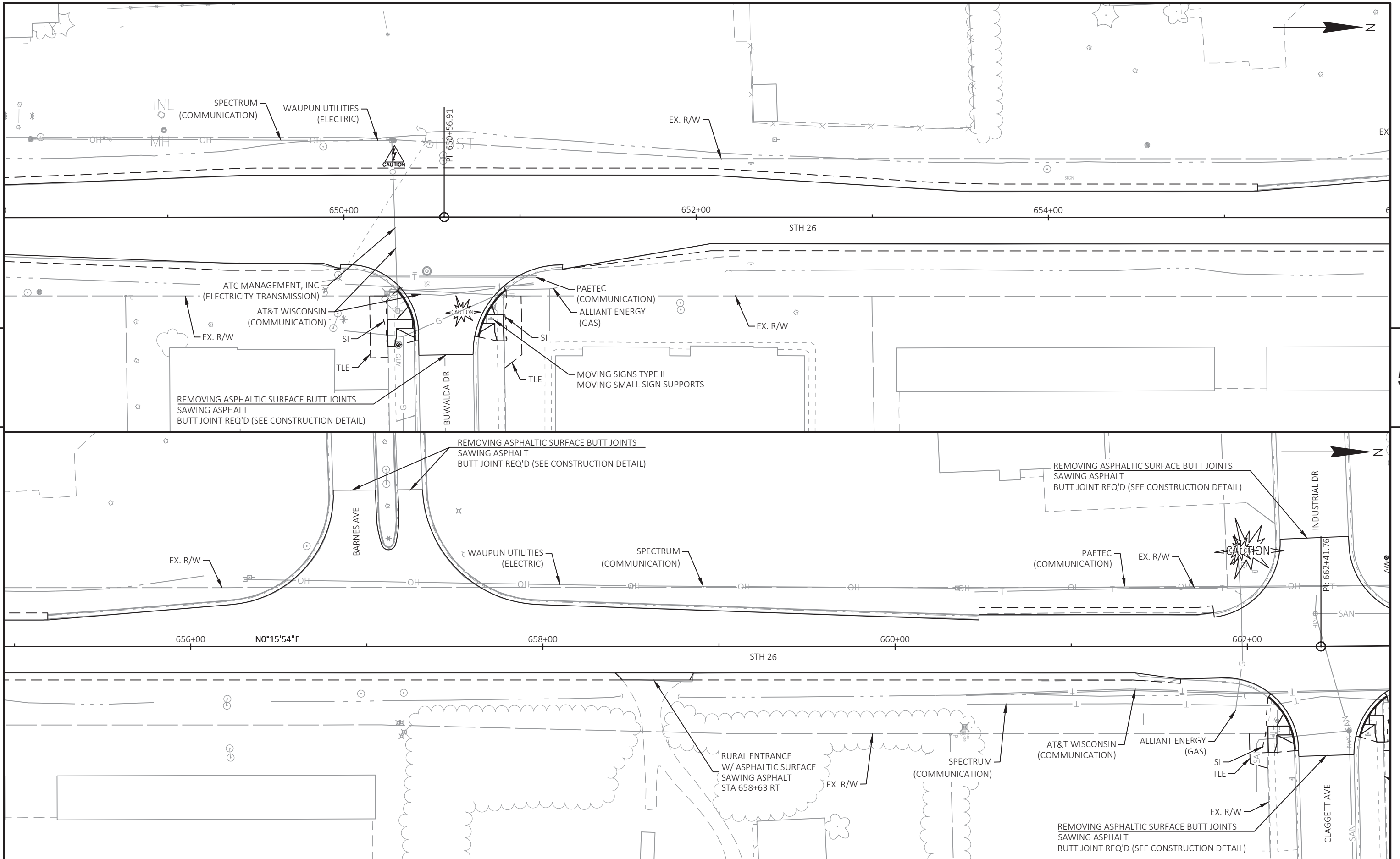
PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	PLAN	SHEET	E
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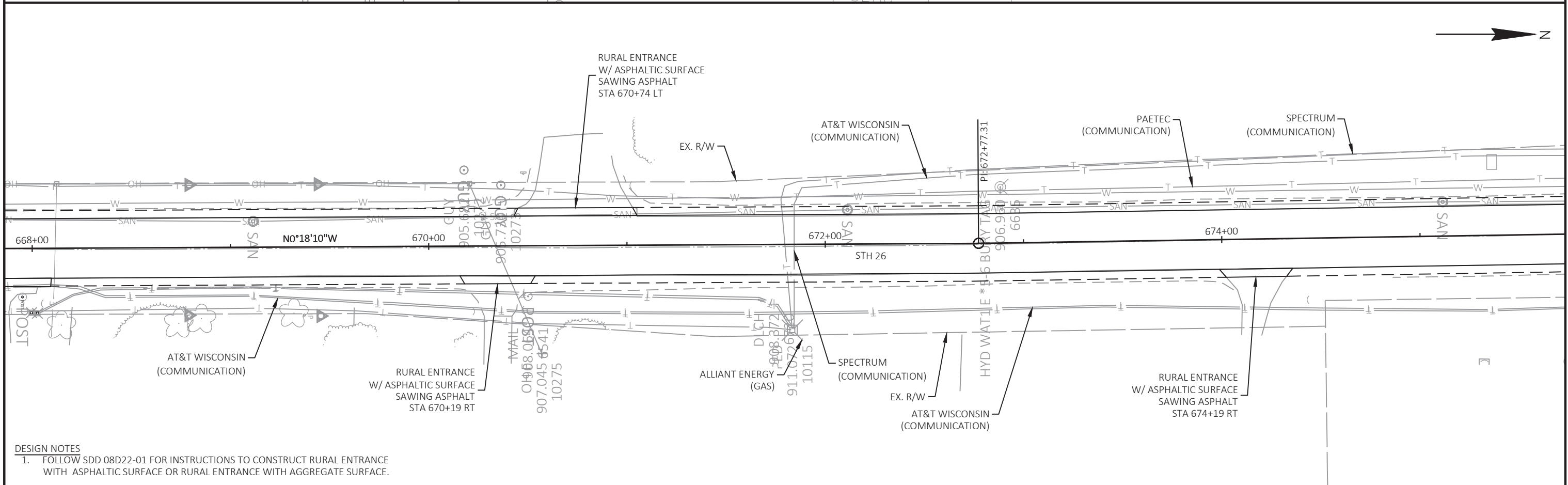
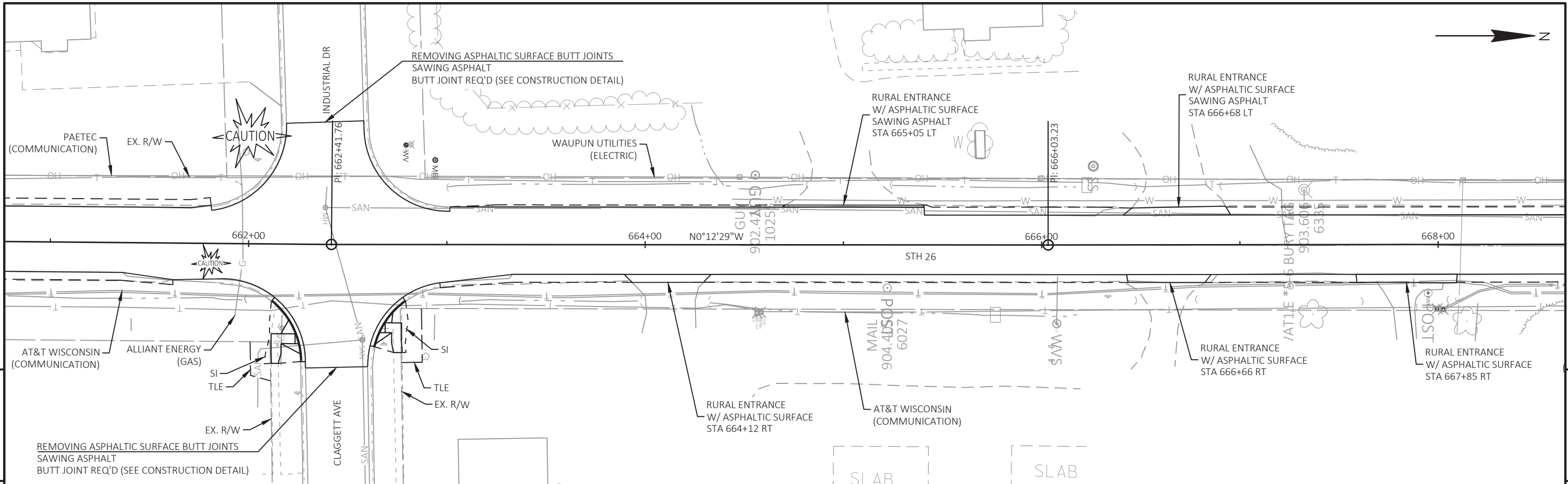
PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	PLAN	SHEET	E
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PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	PLAN	SHEET	E
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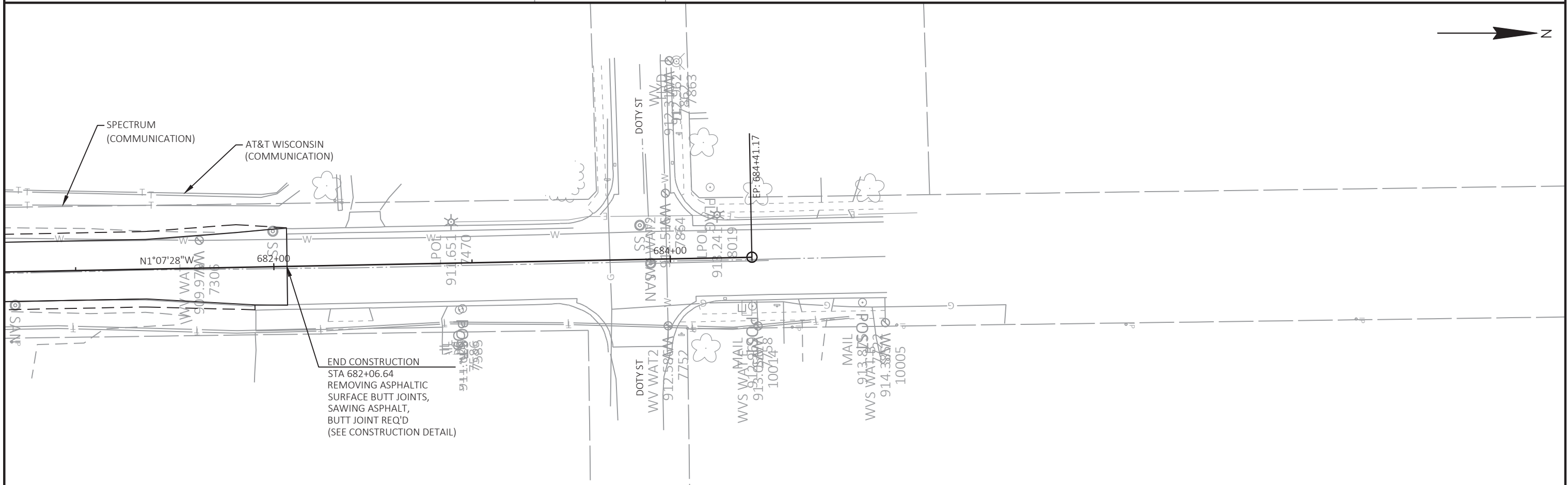
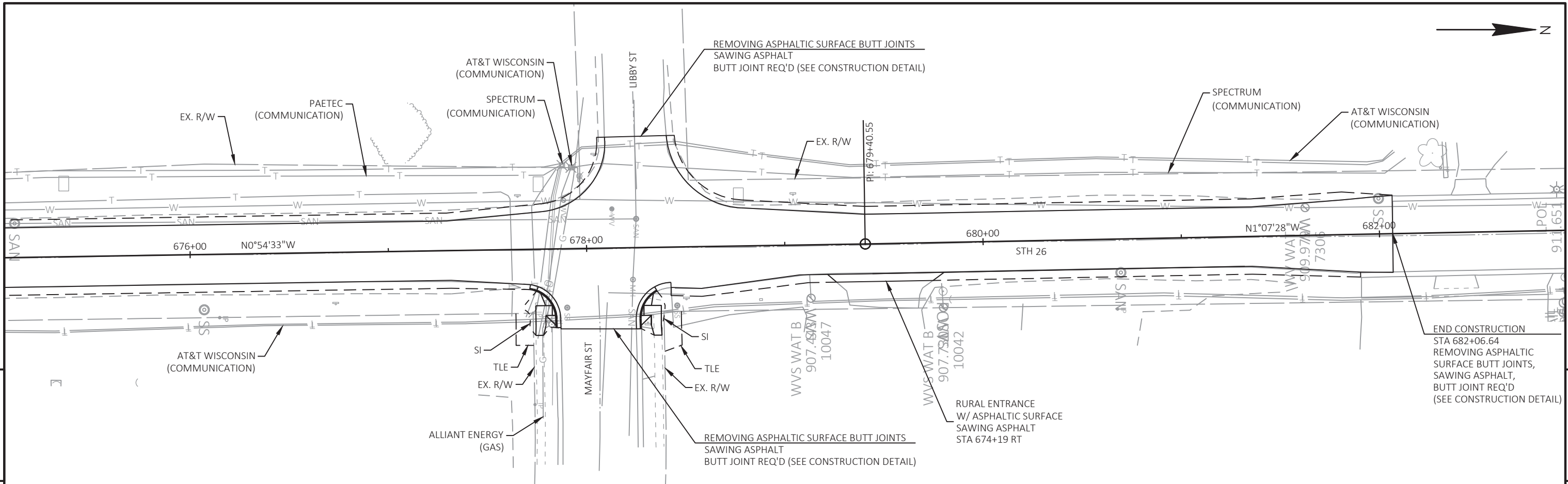


PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	PLAN	SHEET	E
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DESIGN NOTES
1. FOLLOW SDD 08D22-01 FOR INSTRUCTIONS TO CONSTRUCT RURAL ENTRANCE WITH ASPHALTIC SURFACE OR RURAL ENTRANCE WITH AGGREGATE SURFACE.

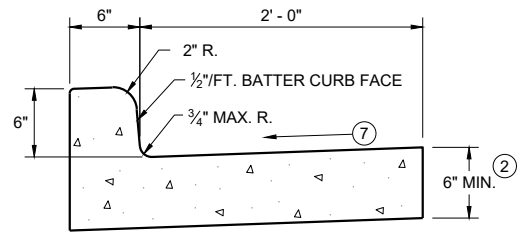
PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	PLAN	SHEET	E
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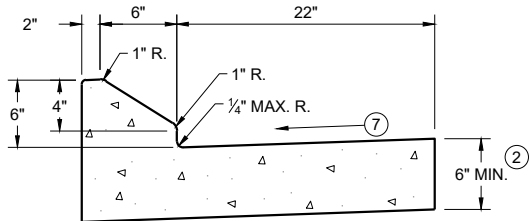
PROJECT NO: 6235-01-73	HWY: STH 26	COUNTY: DODGE	PLAN	SHEET	E
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Standard Detail Drawing List

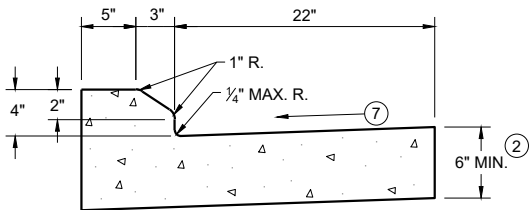
08D01-24A	CONCRETE CURB & GUTTER
08E10-02	INLET PROTECTION TYPE A, B, C AND D
09A15-01	DRIVEWAYS WITHOUT CURB & GUTTER RESURFACING PROJECTS RURAL
11B02-02	CONCRETE MEDIAN NOSE
13C01-19	CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES
13C11-14A	RURAL DOWELED CONCRETE PAVEMENT
13C11-14B	RURAL DOWELED CONCRETE PAVEMENT
13C14-07A	BASE PATCHING CONCRETE
13C14-07B	BASE PATCHING CONCRETE
13C14-07C	BASE PATCHING CONCRETE
13C19-03	HMA LONGITUDINAL JOINTS
14B28-04A	GUARDRAIL MOW STRIP
14B28-04B	GUARDRAIL MOW STRIP
14B29-01	SAFETY EDGE
14B42-07A	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07B	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07C	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07D	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
15C02-10B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-10D	ON RAMP LANE CLOSURE
15C02-10E	OFF RAMP LANE CLOSURE
15C02-10F	ADVANCED WIDTH RESTRICTION SIGNING
15C02-10G	TRAFFIC CONTROL FOR ENTRANCE RAMP CLOSURE
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C08-23B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C08-23C	PAVEMENT MARKING (TURN LANES)
15C08-23D	PAVEMENT MARKING (TURN LANES)
15C08-24A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C08-24B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C08-24C	PAVEMENT MARKING (TURN LANES)
15C08-24D	PAVEMENT MARKING (TURN LANES)
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C18-09A	MEDIAN ISLAND PAVEMENT MARKINGS
15C18-09B	PAVEMENT MARKINGS, MEDIAN ISLAND NOSE
15C33-05	STOP LINE AND CROSSWALK PAVEMENT MARKING
15D12-16A	TRAFFIC CONTROL, LANE CLOSURE
15D16-07	TRAFFIC CONTROL, EXIT RAMP CLOSURE
15D30-10A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10B	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10E	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10G	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10J	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10K	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10L	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11B	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11E	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11G	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11J	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11K	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-11L	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D40-05B	TRAFFIC CONTROL, FULL LANE SHIFT MULTILANE DIVIDED 50 MPH AND OVER
15D50-03A	TRAFFIC CONTROL, ADDED LANE CLOSURE WITHOUT LANE SHIFT



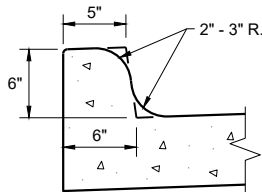
TYPES A^① & D



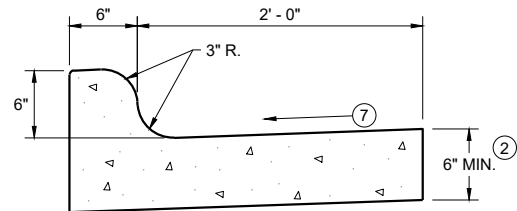
6" SLOPED CURB TYPES G^① & J



4" SLOPED CURB TYPES G^① & J

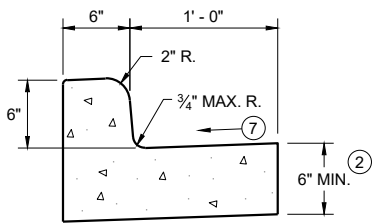


TYPES K^① & L
(OPTIONAL CURB SHAPE)



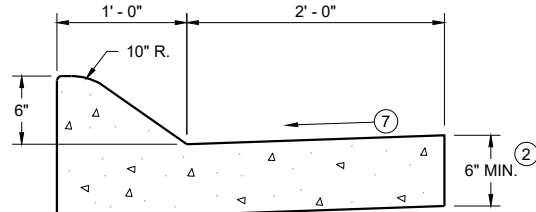
TYPES K^① & L

CONCRETE CURB AND GUTTER 30"

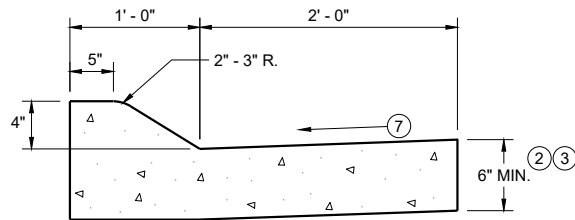


TYPES A^① & D

CONCRETE CURB AND GUTTER 18"

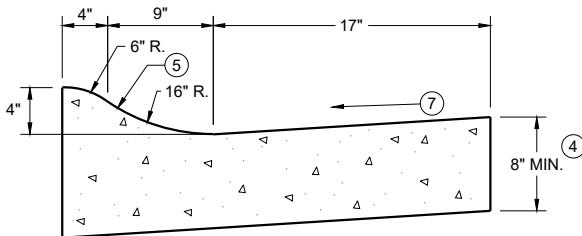


6" SLOPED CURB TYPES A^① & D



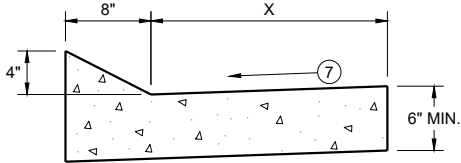
4" SLOPED CURB TYPES A^① & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R^① & T
CONCRETE CURB AND GUTTER 30"

TBT & TBTT	X
30"	22"
36"	28"

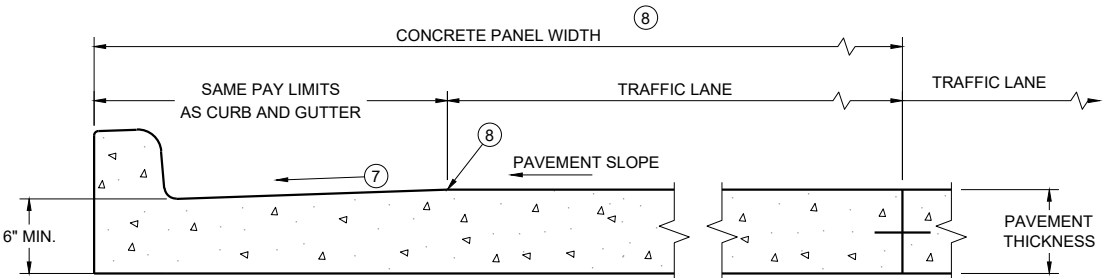


TYPES TBT & TBTT^①

CONCRETE CURB AND GUTTER

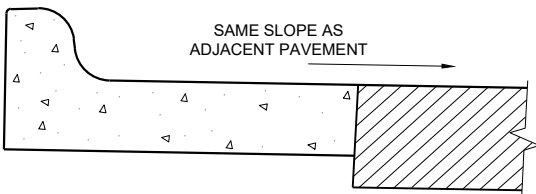
PAVEMENT THICKNESS
AND MAXIMUM CONCRETE
PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'

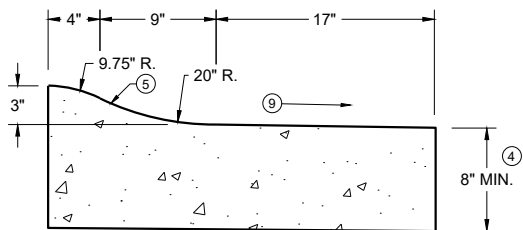


PARTIAL SECTION OF PAVEMENT
WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER^⑥
(TYPICAL FOR ALL CURB & GUTTER TYPES)



3" SLOPED CURB TYPES R^① & T

CONCRETE CURB AND GUTTER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

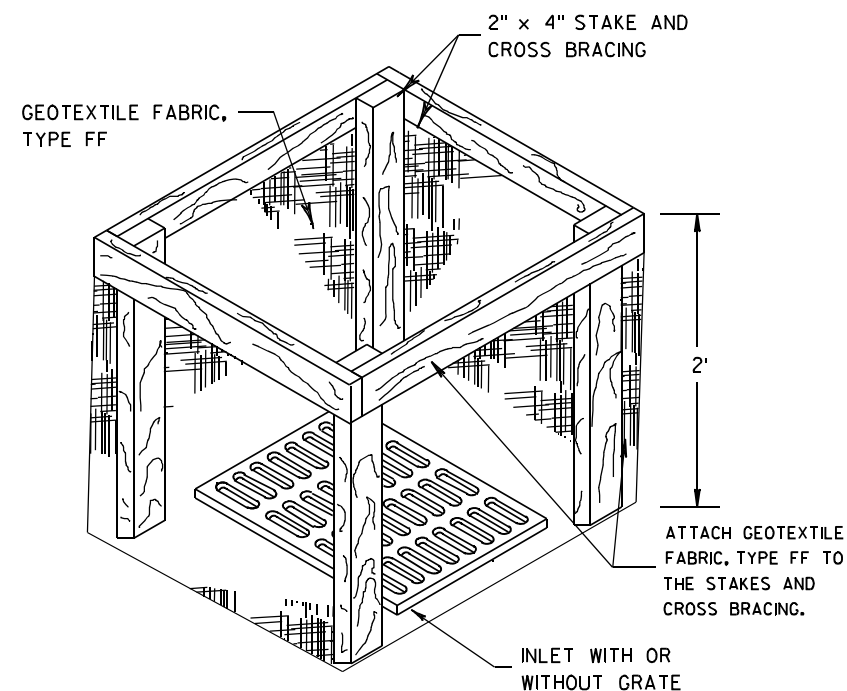
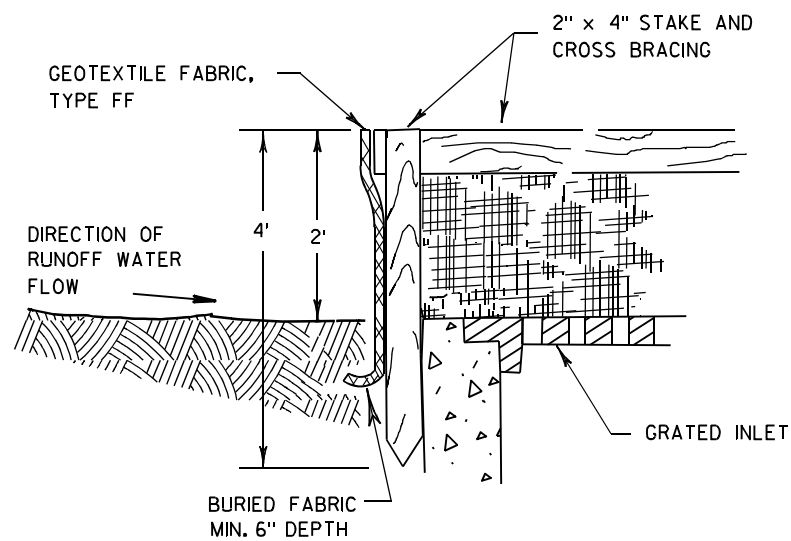
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ SLOPE TO BE REVERSE SLOPE MATCHING THE SLOPE OF THE PAVEMENT AND THE CIRCULATORY ROADWAY



INLET PROTECTION, TYPE A

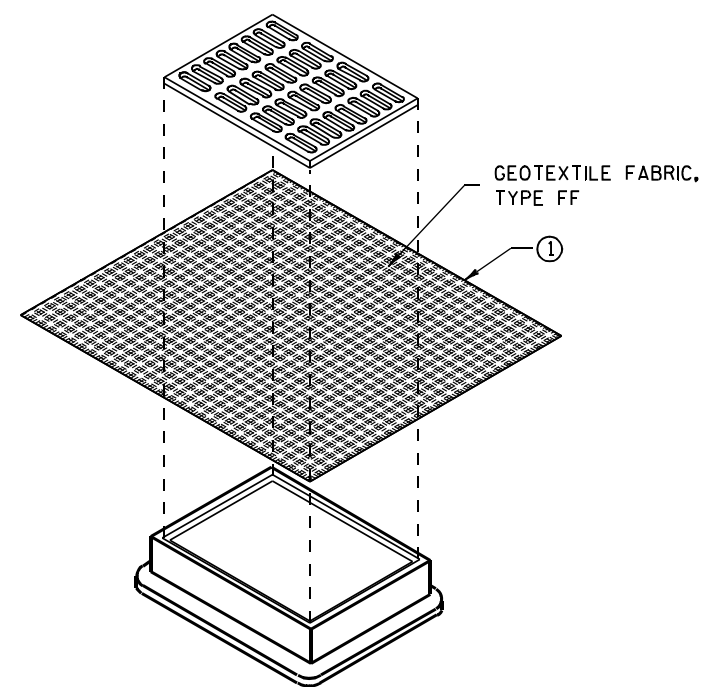
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

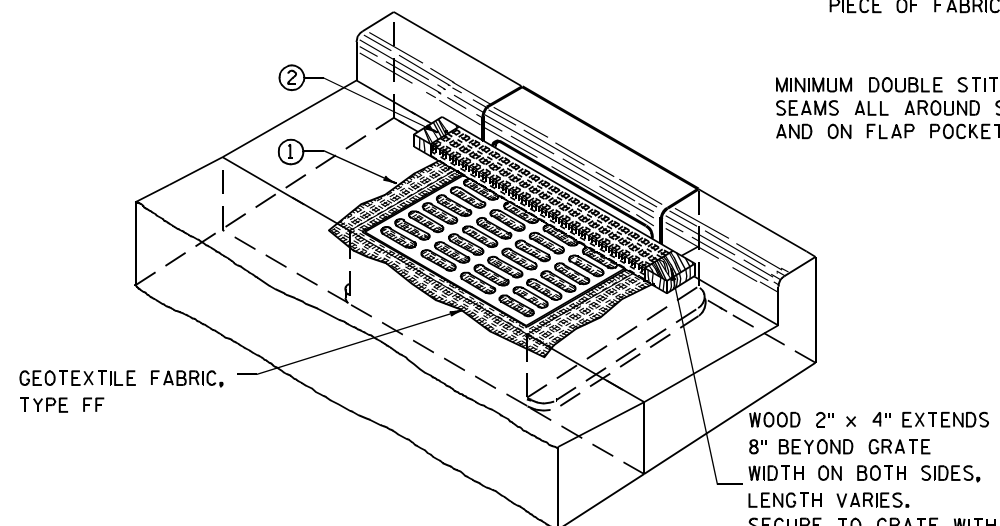
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

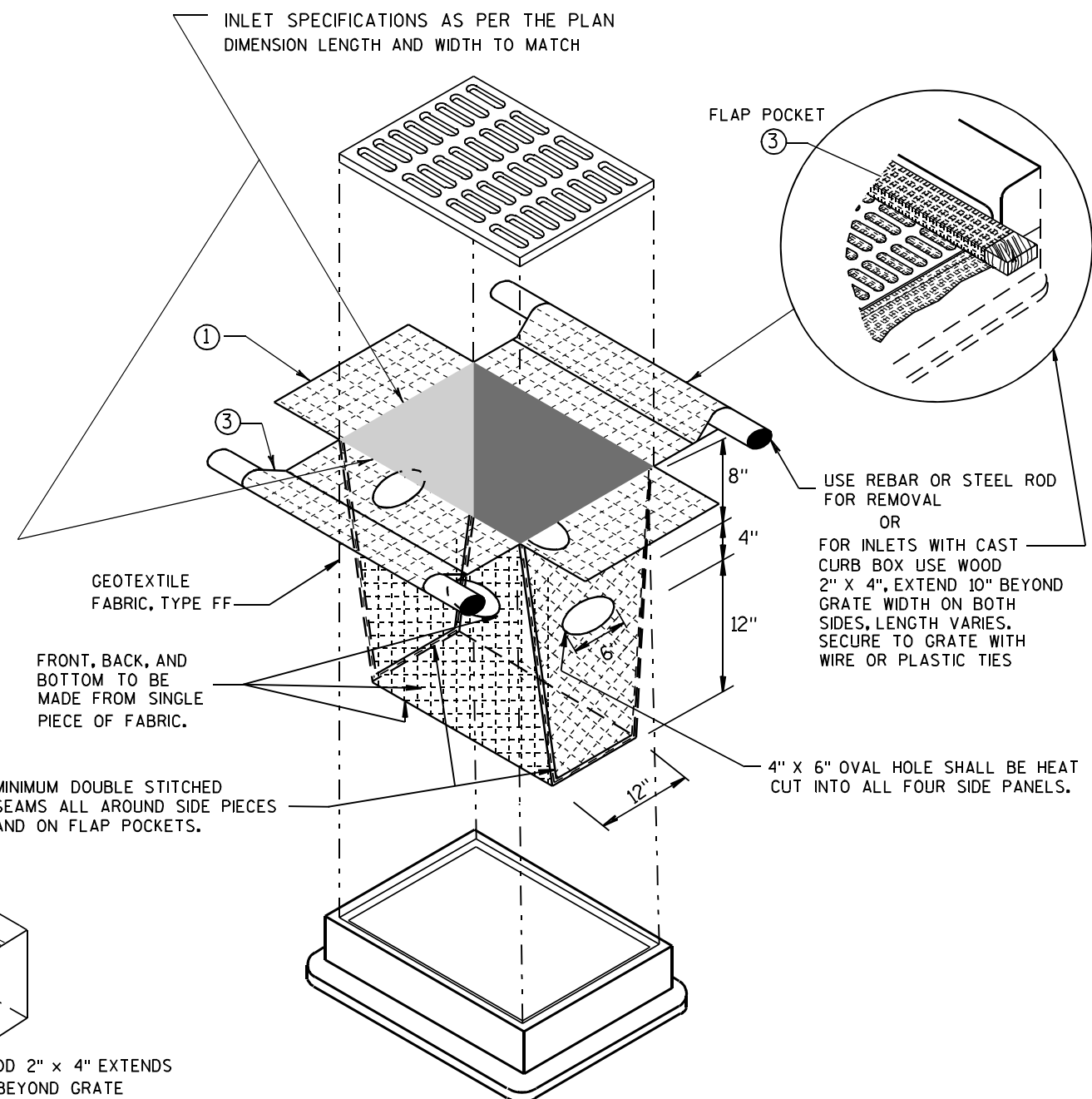
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

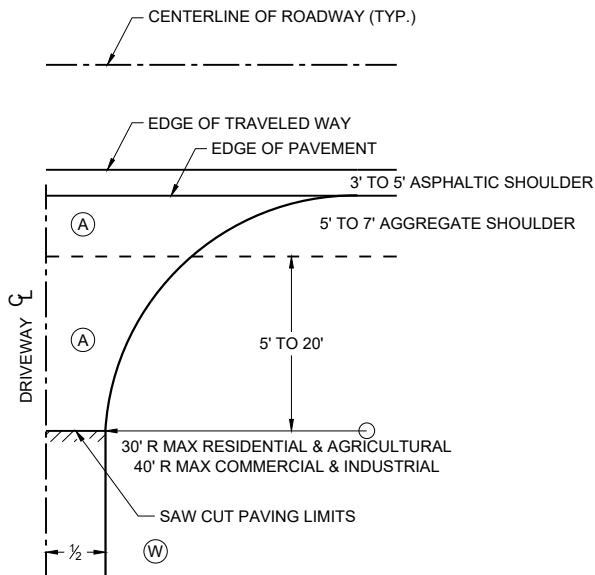
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



INLET PROTECTION, TYPE D

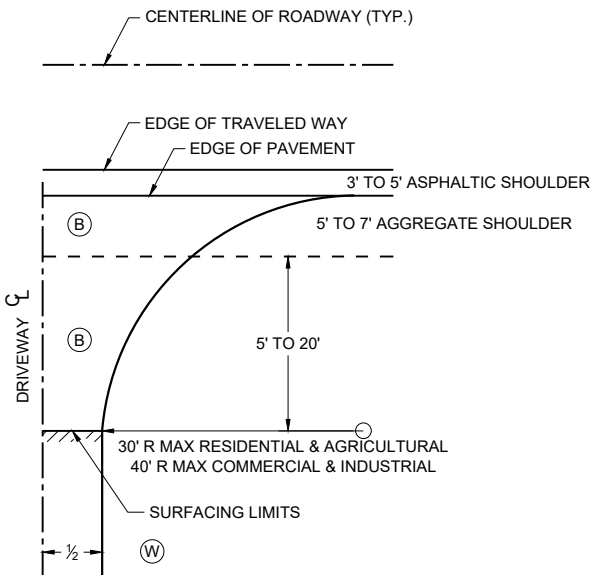
(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

INLET PROTECTION TYPE A, B, C, AND D	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/S/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	

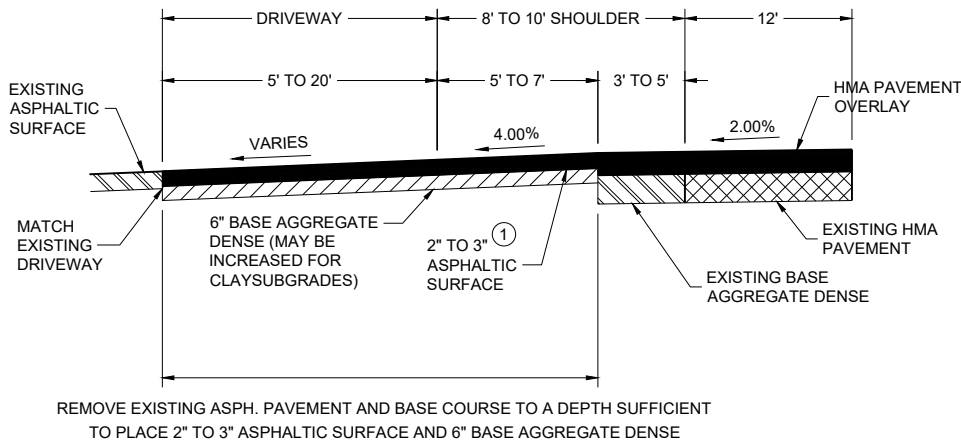


PLAN VIEW
HALF SECTION

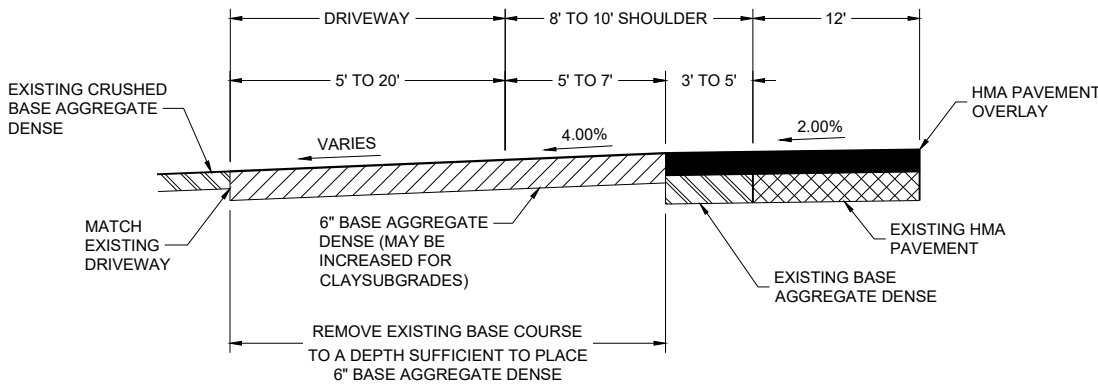
- (A) : PAID FOR AS ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES. (TON)
- (B) : PAID FOR AS BASE AGGREGATE DENSE 1 1/4" (TON)
- (W) : DRIVEWAY WIDTHS (MEASURED AT ROW LINE)
~ COMMERCIAL & INDUSTRIAL 16' MIN. - 35' MAX.
~ RESIDENTIAL & AGRICULTURAL 16' MIN. - 24' MAX. (NONCOMMERCIAL)



PLAN VIEW
HALF SECTION



PROFILE VIEW
RURAL ENTRANCE
WITH ASPHALTIC SURFACE
RESURFACING PROJECTS



PROFILE VIEW
RURAL ENTRANCE
WITH AGGREGATE SURFACE
6" BASE AGGREGATE DENSE
RESURFACING PROJECTS

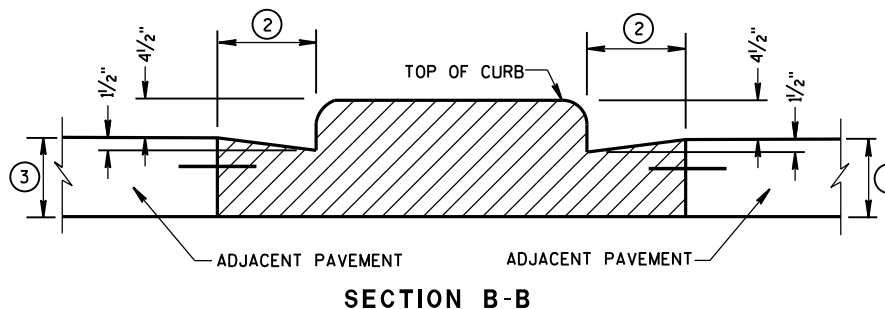
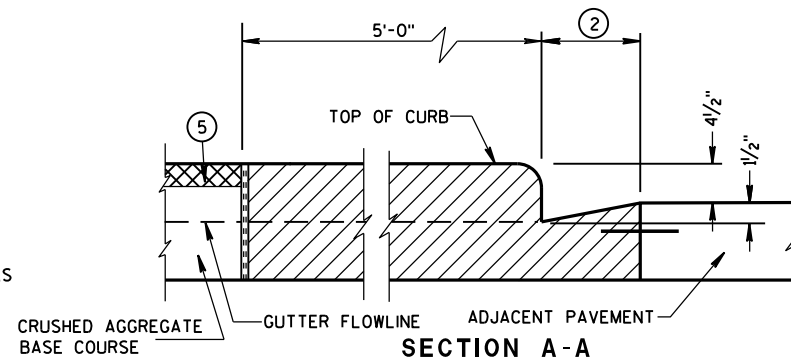
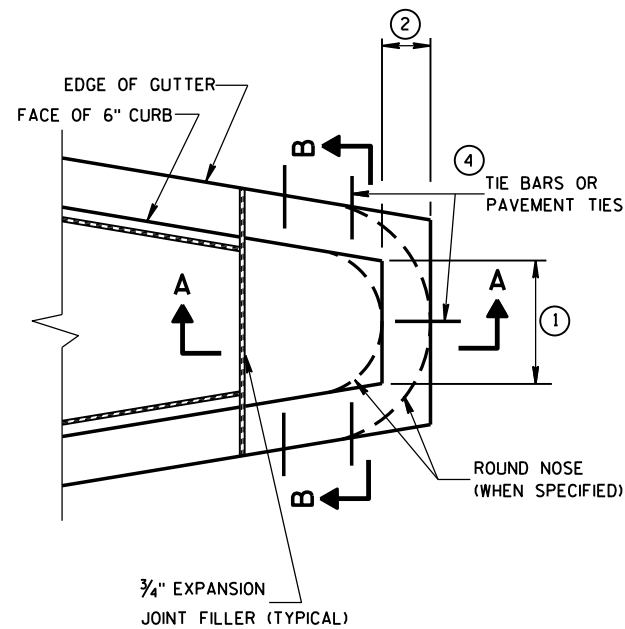
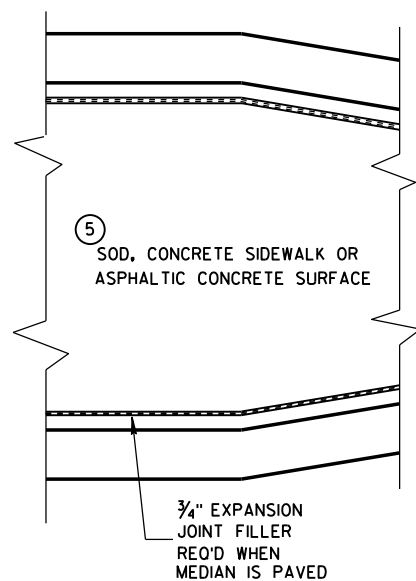
GENERAL NOTES

- ① DESIGN WILL DETERMINE FINAL DRIVEWAY ASPHALTIC THICKNESS BASED ON TYPE OF USAGE AND LOADINGS.

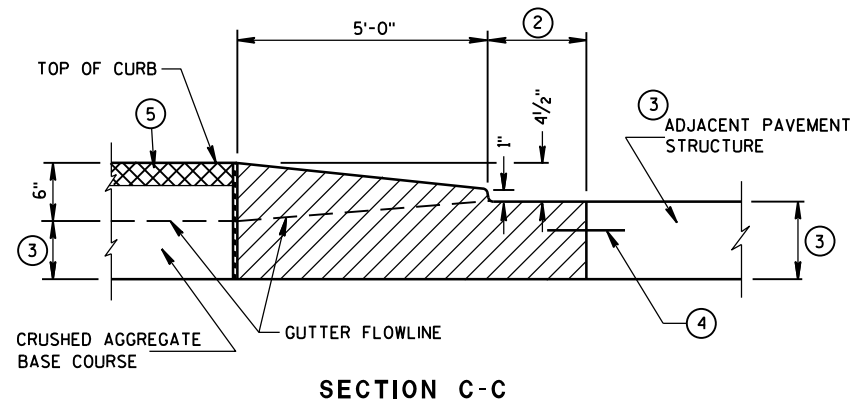
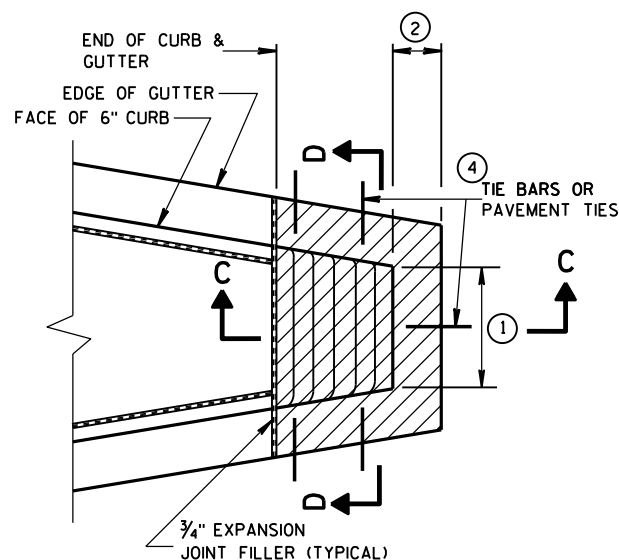
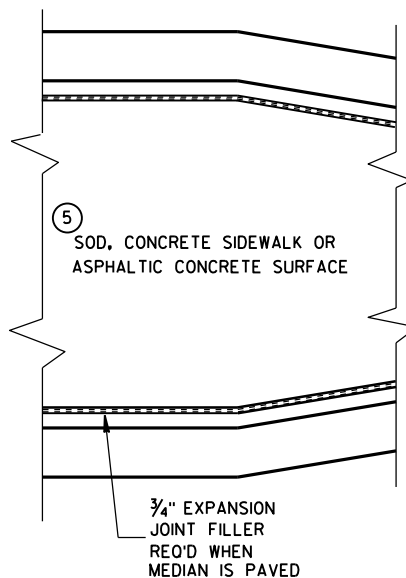
DRIVEWAYS WITHOUT CURB
AND GUTTER RESURFACING
PROJECTS RURAL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

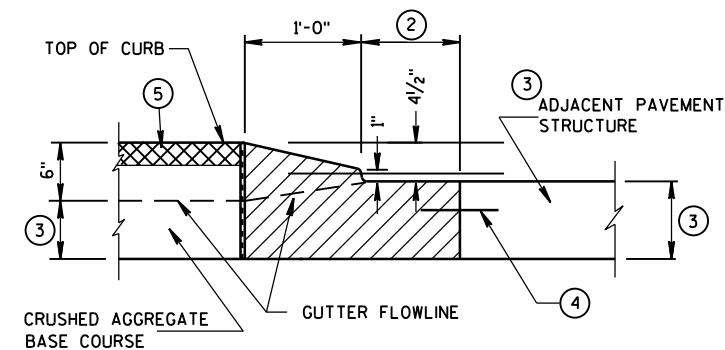
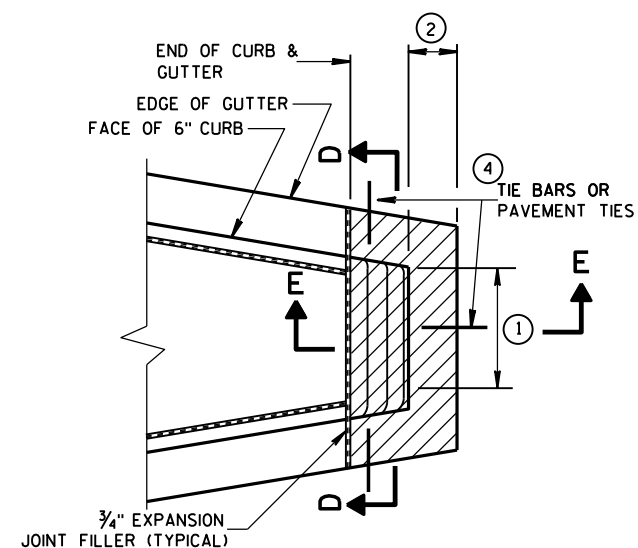
APPROVED
November 2025 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA



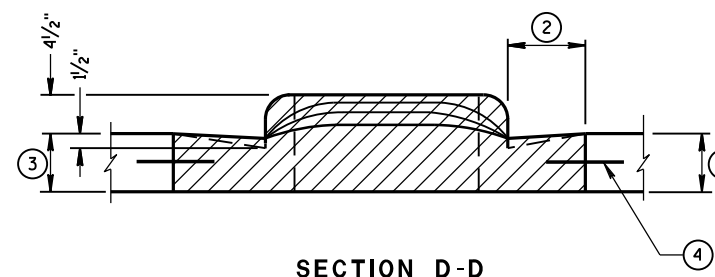
CONCRETE MEDIAN BLUNT NOSE DETAIL



CONCRETE MEDIAN SLOPED NOSE TYPE 1



CONCRETE MEDIAN SLOPED NOSE TYPE 2



GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① SEE PLAN FOR MEDIAN NOSE WIDTH AND RADIUS (FOR ROUND NOSE ALTERNATE).
- ② WIDTH OF GUTTER TO MATCH EXISTING ADJACENT GUTTER OR AS SPECIFIED ELSEWHERE IN THE PLAN.
- ③ DEPTH EQUAL TO ADJACENT PAVEMENT. ADJACENT PAVEMENT STRUCTURE DETAILS ARE SHOWN ON THE PLAN. TYPICAL OPTIONS ARE:
 - (1) NEW OR EXISTING CONCRETE PAVEMENT.
 - (2) ASPHALTIC CONCRETE PAVEMENT OVER NEW OR EXISTING CONCRETE BASE COURSE.
 - (3) ASPHALTIC CONCRETE PAVEMENT OVER CRUSHED AGGREGATE BASE COURSE.

- ④ TIE BARS OR PAVEMENT TIES REQUIRED IN NEW CONCRETE PAVEMENT OR CONCRETE BASE COURSE. TIE BARS SHALL BE NO. 4 X 2'-0" SPACED AT 2'-0" C-C.

PAVEMENT TIES REQUIRED IN EXISTING CONCRETE BASE COURSE. PAVEMENT TIES SHALL BE NO. 6 X 1'-0" SPACED AT 3'-0" C-C INSTALLED ON A HORIZONTAL SKEW OF 6:1. THE DIRECTION OF SKEW SHALL ALTERNATE AFTER EVERY ONE OR TWO BARS.

- ⑤ SURFACE TYPE AND DETAILS ARE SHOWN ELSEWHERE IN THE PLAN.

CONCRETE MEDIAN NOSE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

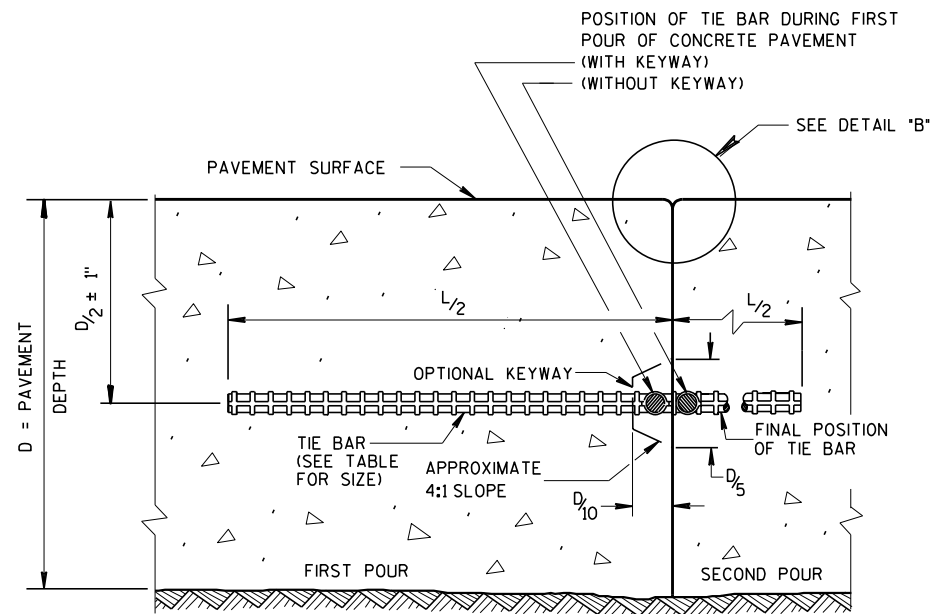
APPROVED

6/8/2006

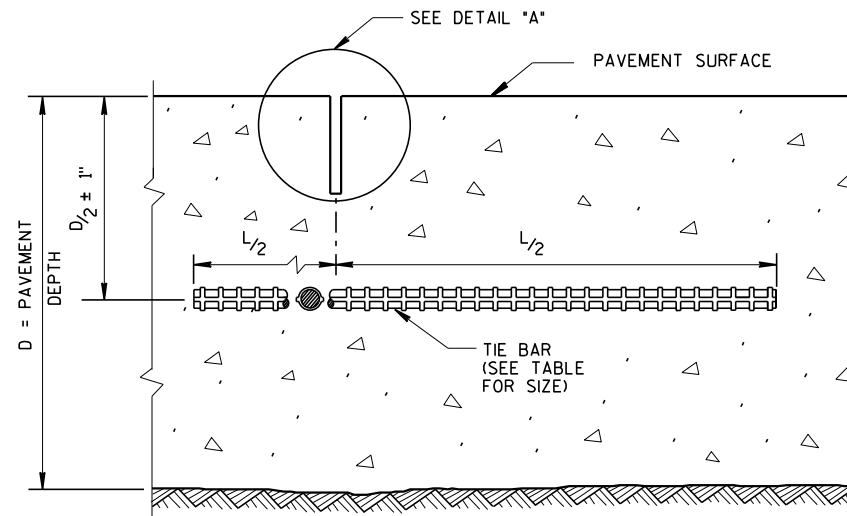
DATE

FHWA

/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER



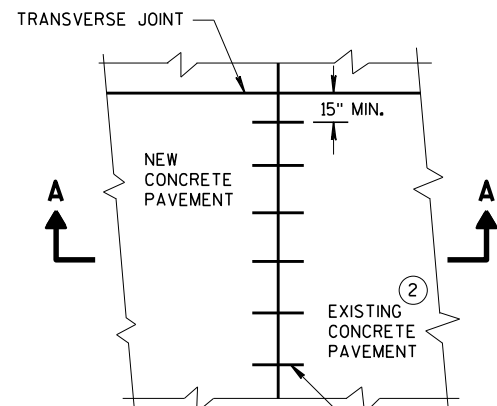
CONSTRUCTION JOINT



SAWED JOINT

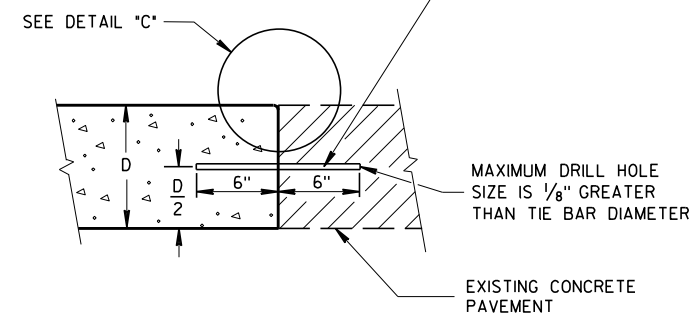
GENERAL NOTES

- CREATE A LONGITUDINAL JOINT FOR PAVEMENT WIDTHS GREATER THAN 15 FEET.
- CORRELATE LONGITUDINAL JOINTS WITH LANE LINES WHEN POSSIBLE.
- 1 ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.
 - 2 PAVEMENT THAT WAS IN PLACE PRIOR TO THE CONTRACT.

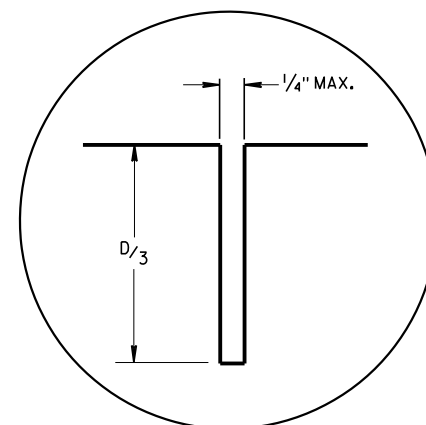


PLAN VIEW

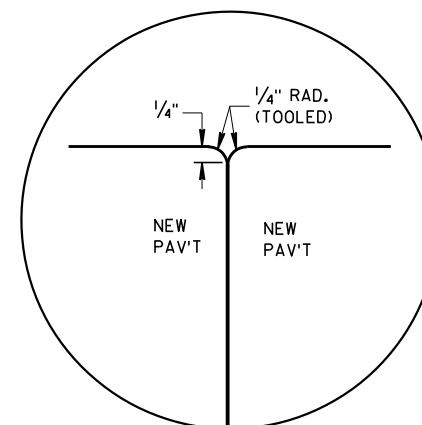
NO. 6 TIE BARS SPACED 30" C-C, INSTALLED PERPENDICULAR TO THE LONGITUDINAL JOINT. ①



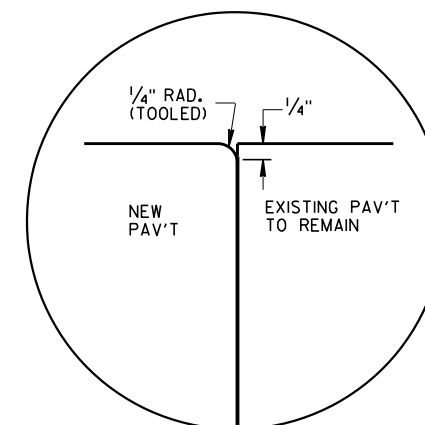
SECTION A-A
LONGITUDINAL CONSTRUCTION JOINT
TIE BARS ANCHORED
INTO EXISTING PAVEMENT



DETAIL "A"



DETAIL "B"



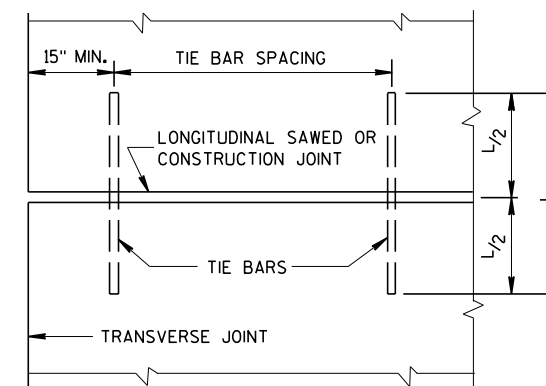
DETAIL "C"

TIE BAR TABLE

PAVEMENT DEPTH (D)	TIE BAR SIZE	TIE BAR LENGTH (L)	MAX. TIE BAR SPACING
< 10 1/2"	NO. 4	30"	36"
≥ 10 1/2"	NO. 5	36"	36"
	NO. 4 *	30"	24" **

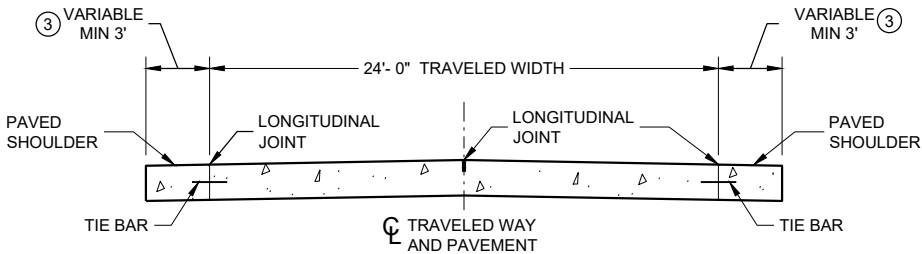
* SUBSTITUTE BENT BARS AT LONGITUDINAL JOINTS WHEN EQUIPMENT LIMITATIONS DURING CONSTRUCTION WARRANT (e.g. AUXILIARY LANES OR TURN LANES)

** CONFORM TO 15" MINIMUM SPACING FROM TRANSVERSE JOINTS; SPACING BETWEEN TIE BARS WILL BE 30" AT TRANSVERSE JOINTS.

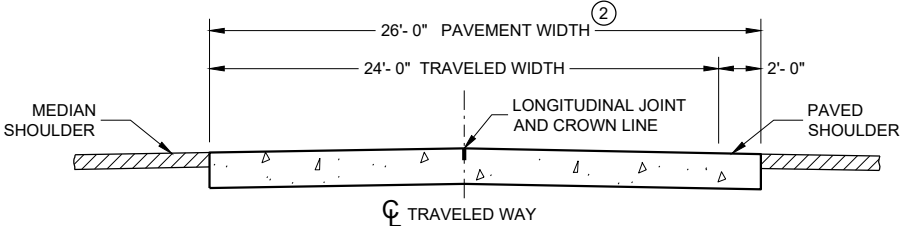


PLAN VIEW
SHOWING LOCATION OF TIE BARS

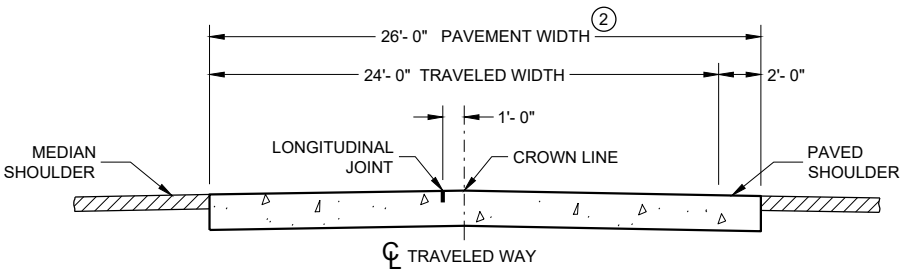
CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED March 2018 DATE	/S/ Peter Kemp, P.E. PAVEMENT SUPERVISOR
FHWA	



SECTION A - A
TWO-LANE TWO-WAY HIGHWAY ①



SECTION B - B



ALTERNATIVE SECTION B - B
DIVIDED HIGHWAY ①

GENERAL NOTES

CONTRACTION JOINTS

CONSTRUCT TRANSVERSE CONTRACTION JOINTS NORMAL TO THE CENTERLINE. SHOW THE LOCATION OF CONTRACTION JOINTS THROUGH INTERSECTIONS ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.

FOR PAVEMENT SLABS OF VARYING WIDTHS, LOCATE THE OUTER MOST DOWEL BAR SO THAT THE CENTER OF THE BAR IS A MINIMUM OF 6 INCHES FROM AND A MAXIMUM OF 18 INCHES FROM THE FREE EDGE OF PAVEMENT.

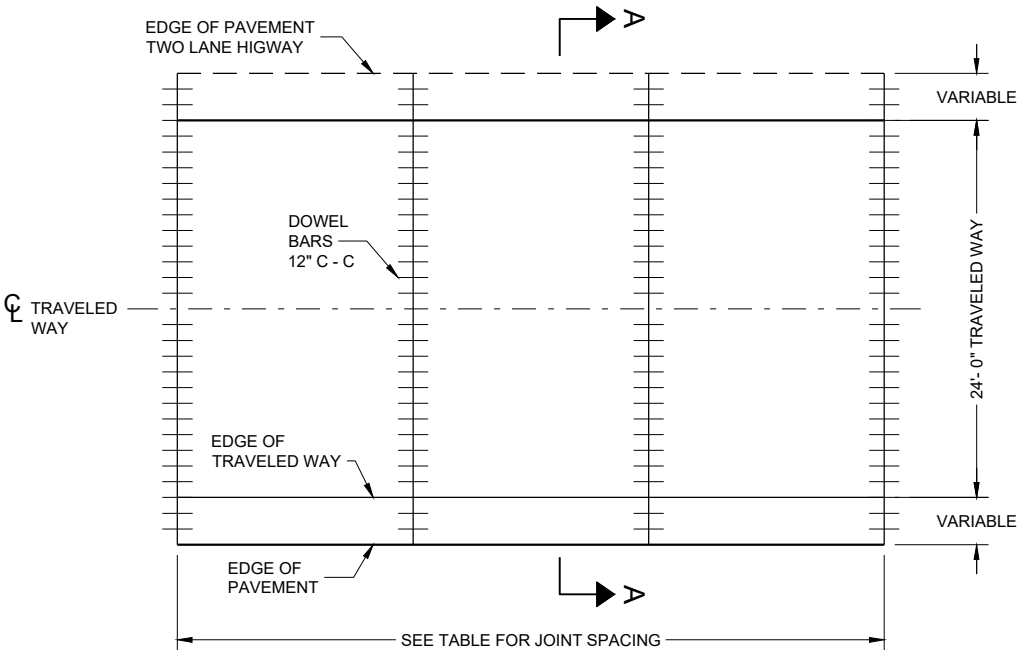
CONSTRUCTION JOINTS

LOCATE CONSTRUCTION JOINTS A MINIMUM OF 6 FEET FROM THE NEAREST CONTRACTION JOINT AND ALIGN PARALLEL TO THE CONTRACTION JOINTS.

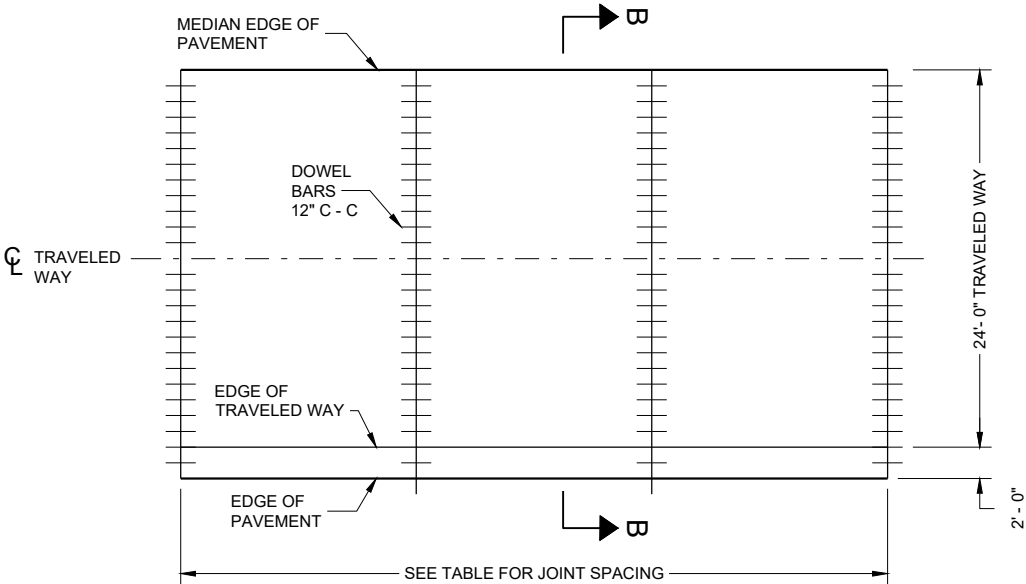
- ① REFER TO TYPICAL CROSS SECTIONS FOR ADDITIONAL DETAILS.
- ② MEASURE THE ENTIRE PAVED WIDTH INCLUDING THE PORTION(S) LABELED "PAVED SHOULDER" AS CONCRETE PAVEMENT.
- ③ SHOULDER WIDTHS LESS THAN 3 FEET SHALL BE PAVED INTEGRAL TO THE MAINLINE CONCRETE PAVEMENT, SEE SECTION B-B.

PAVEMENT DEPTH, DOWEL BAR SIZE AND JOINT SPACING TABLE

PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	CONTRACTION JOINT SPACING
6", 6 ½"	NONE	12'
7", 7 ½"	1"	14'
8" & ABOVE	1 ¼"	15'



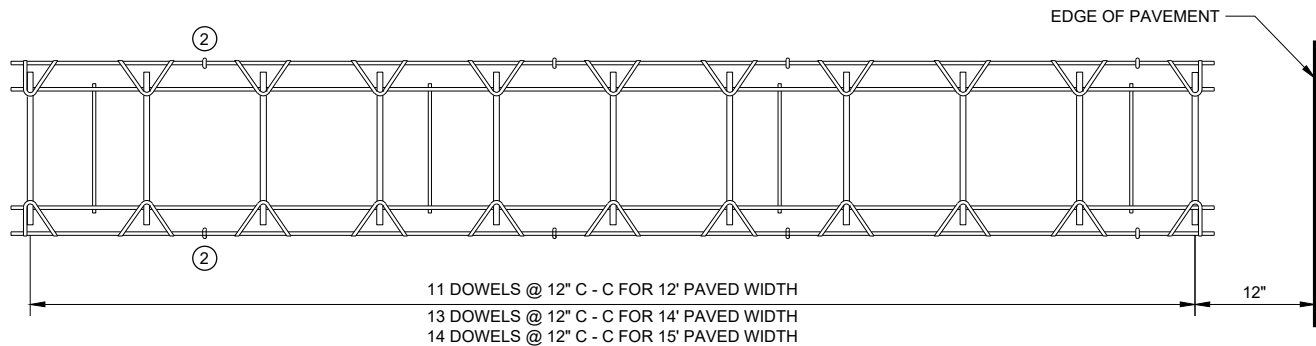
CONTRACTION JOINT LAYOUT FOR TWO-LANE TWO-WAY HIGHWAY



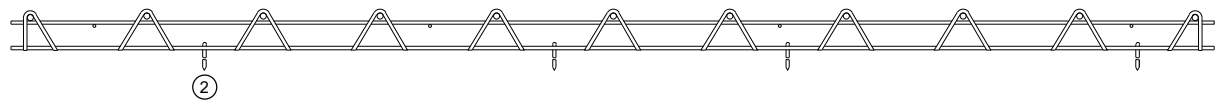
CONTRACTION JOINT LAYOUT FOR DIVIDED HIGHWAY

**RURAL DOWELED
CONCRETE PAVEMENT**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

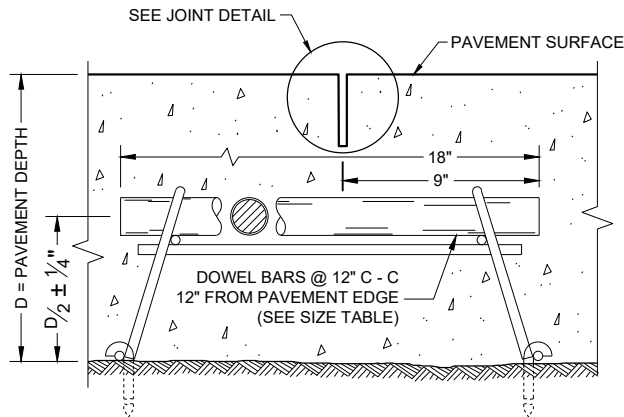


PLAN VIEW

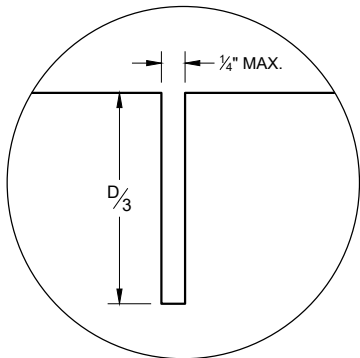


SIDE VIEW
(NORMAL TO CENTERLINE)

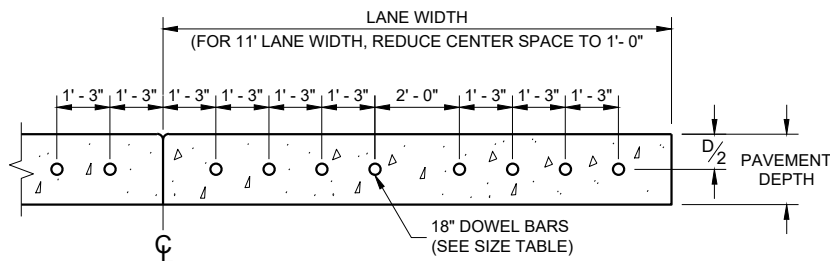
CONTRACTION JOINT DOWEL ASSEMBLY ①



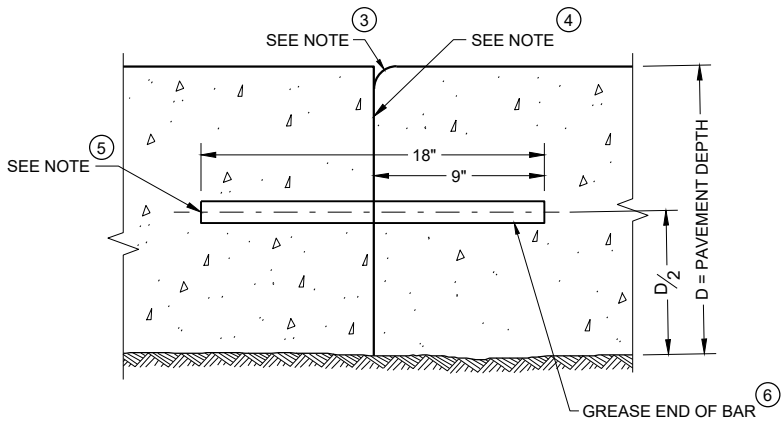
DOWELED CONTRACTION JOINT



JOINT DETAIL



DRILLED DOWEL BAR CONSTRUCTION JOINT ⑦



TRANSVERSE CONSTRUCTION JOINT

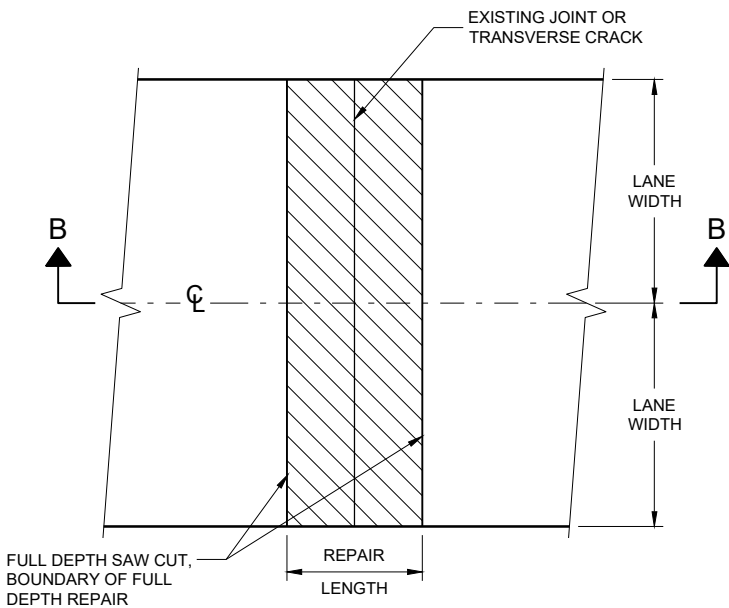
GENERAL NOTES

- ① OBTAIN THE ENGINEER'S APPROVAL FOR THE USE OF ALTERNATIVE DESIGNS OF THE DOWEL ASSEMBLY. USE MECHANICAL DOWEL BAR INSERTERS OR DOWEL ASSEMBLIES WHEN CONSTRUCTION CONTRACTION JOINTS.
- ② SECURE BASKETS WITH ANCHORS TO HOLD DOWEL BARS IN THE CORRECT POSITION AND ALIGNMENT. TYPE, LOCATION, NUMBER AND LENGTH OF ANCHORS ARE DEPENDENT UPON FIELD CONDITIONS.
- ③ FORM OR SAW CONSTRUCTION JOINTS. PROVIDE A $1/4"$ RADIUS AT FORMED JOINTS.
- ④ PROVIDE A SMOOTH VERTICAL FACE FOR THE ENTIRE DEPTH OF THE PAVEMENT WHEN FORMING CONSTRUCTION JOINTS.
- ⑤ INSTALL DOWEL BARS AT CONSTRUCTION JOINTS BY FORMING OR DRILLING. INSTALL FORMED DOWEL BARS 12 INCHES C - C AND 12 INCHES FROM PAVEMENT EDGE. REMOVE EXCESS CONCRETE FROM THE FREE END OF THE DOWEL BAR IF DOWEL BARS ARE FORMED THROUGH A HEADER BOARD. INSTALL DRILLED DOWEL BARS ACCORDING TO THE "DRILLED DOWEL BAR CONSTRUCTION JOINT" DETAIL.
- ⑥ APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.
- ⑦ ANCHOR DOWEL BARS AND TIE BARS INTO DRILLED HOLES WITH AN EPOXY. MAXIMUM DRILLED HOLE SIZE IS $1/8"$ GREATER THAN DOWEL BAR DIAMETER, 9 INCHES IN LENGTH.

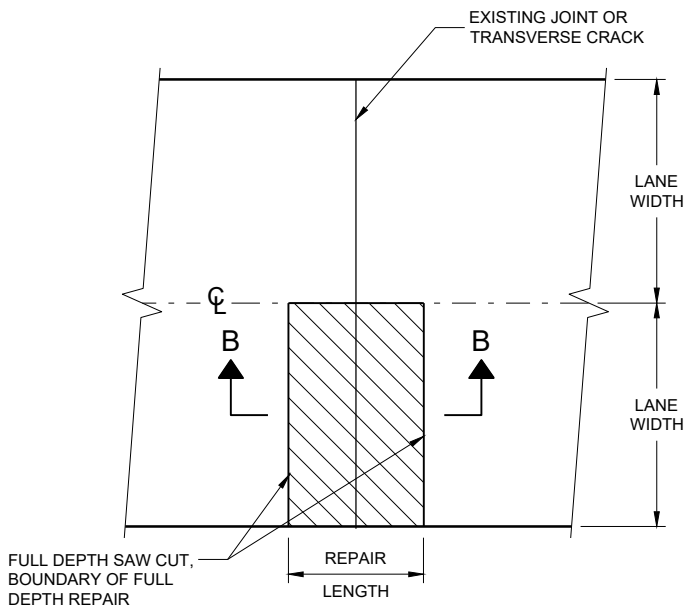
RURAL DOWELED
CONCRETE PAVEMENT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022 /S/ Peter Kemp P.E.
DATE PAVEMENT SUPERVISOR
FHWA

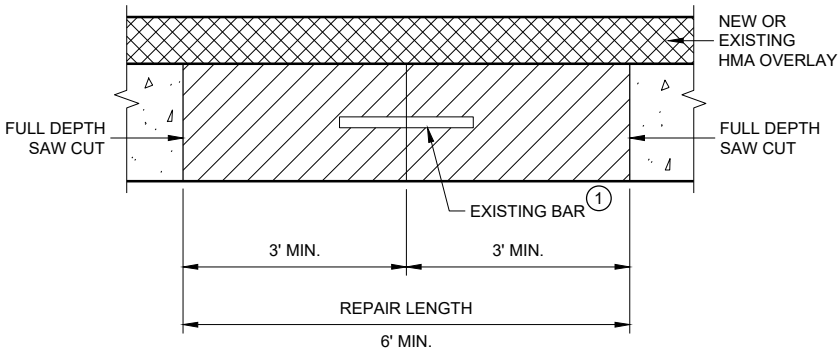


**PLAN VIEW
DOUBLE LANE REPAIR**



**PLAN VIEW
SINGLE LANE REPAIR**

FULL DEPTH CONCRETE PAVEMENT REMOVAL



**SECTION B - B
CONCRETE REMOVAL**

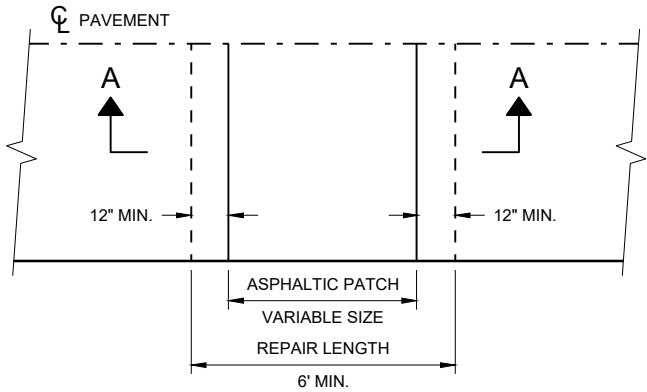
GENERAL NOTES

SAW CUT, DRILL, AND LIFT OUT EXISTING CONCRETE PAVEMENT WITHIN THE BOUNDARIES OF CONCRETE REPAIR AREAS. THE CONTRACTOR MAY MAKE ADDITIONAL SAW CUTS INSIDE THE REPAIR LIMITS TO REDUCE WEIGHT AND SIZE OF CONCRETE PIECES.

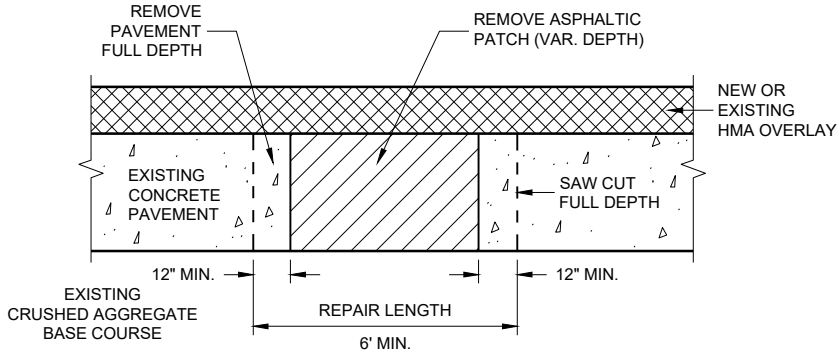
PROVIDE A 6 FOOT MINIMUM DISTANCE FROM BOUNDARIES OF CONCRETE REPAIR AREA TO ADJACENT TRANSVERSE JOINT OR CRACK.

THE LENGTH OF THE REPAIRS MAY VARY FROM THE DIMENSIONS SHOWN IF THE EXISTING CONCRETE PAVEMENT IS NON-DOWELED AND THE PAVEMENT IS TO BE OVERLAID AFTER REPAIRING.

① DOWEL BARS MAY NOT BE PRESENT.



PLAN VIEW

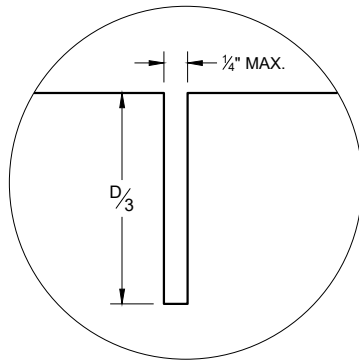


SECTION A - A

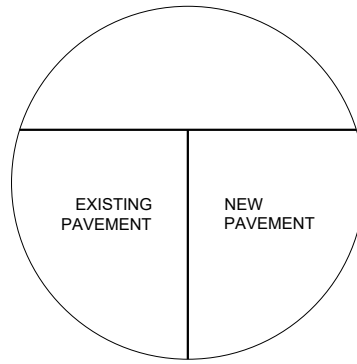
HMA PATCH REMOVAL

BASE PATCHING CONCRETE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

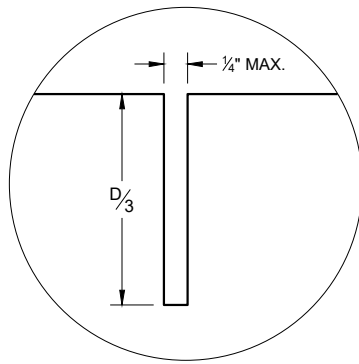


C1

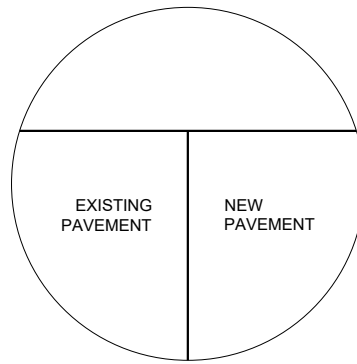


C2

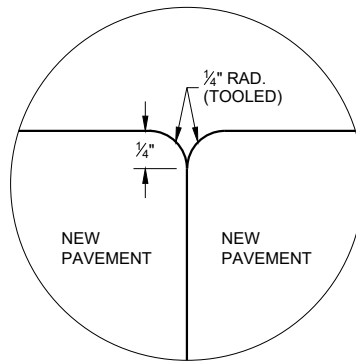
TRANSVERSE JOINTS



L1

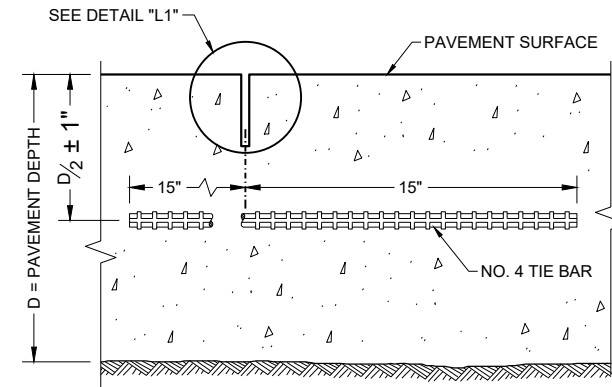


L2

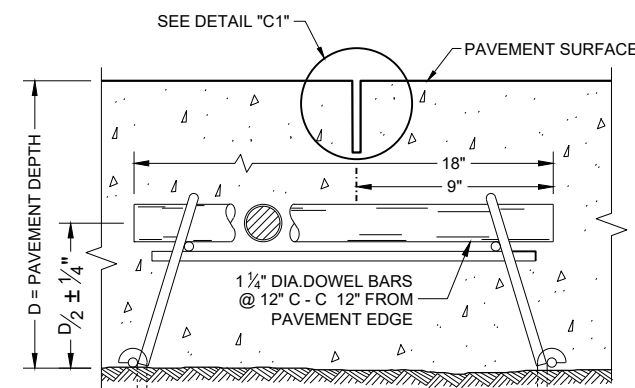


L3

LONGITUDINAL JOINTS



SECTION C - C
SAWED LONGITUDINAL JOINT



SECTION F - F
CONTRACTION JOINT

GENERAL NOTES

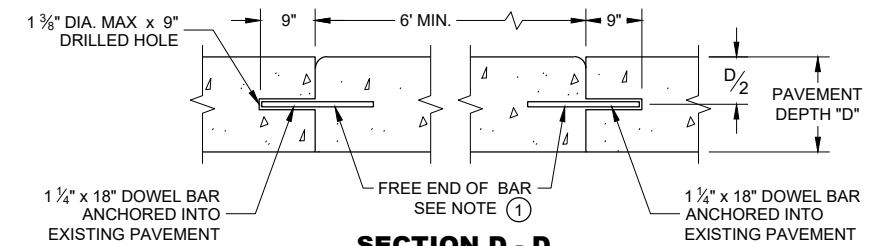
INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.

CONCRETE BASE PATCHES OF EXISTING NON-DOWELED CONCRETE PAVEMENTS DO NOT NEED TO BE DOWELED.

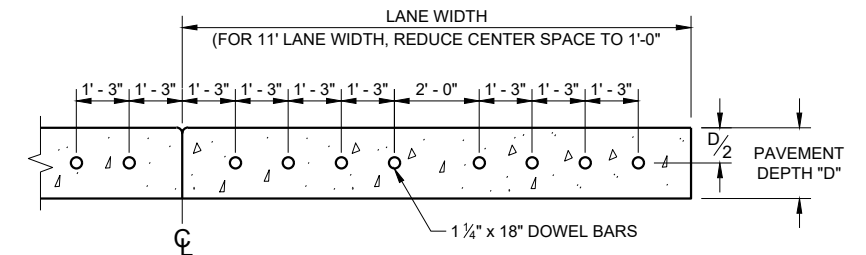
ANCHOR DOWEL BARS AND TIE BARS INTO DRILLED HOLES WITH AN EPOXY.

PROVIDE A MINIMUM DISTANCE OF 15 INCHES FROM ALL TRANSVERSE JOINTS OR EDGES OF REPLACEMENT TO THE CENTER OF THE TIE BAR NEAREST THAT JOINT OR EDGE.

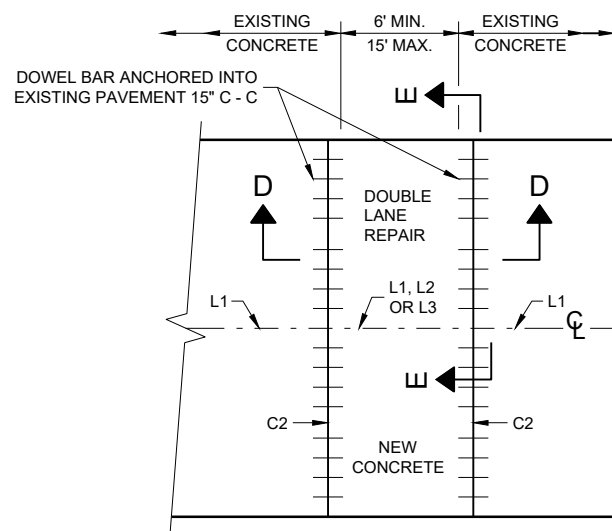
- ① APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.



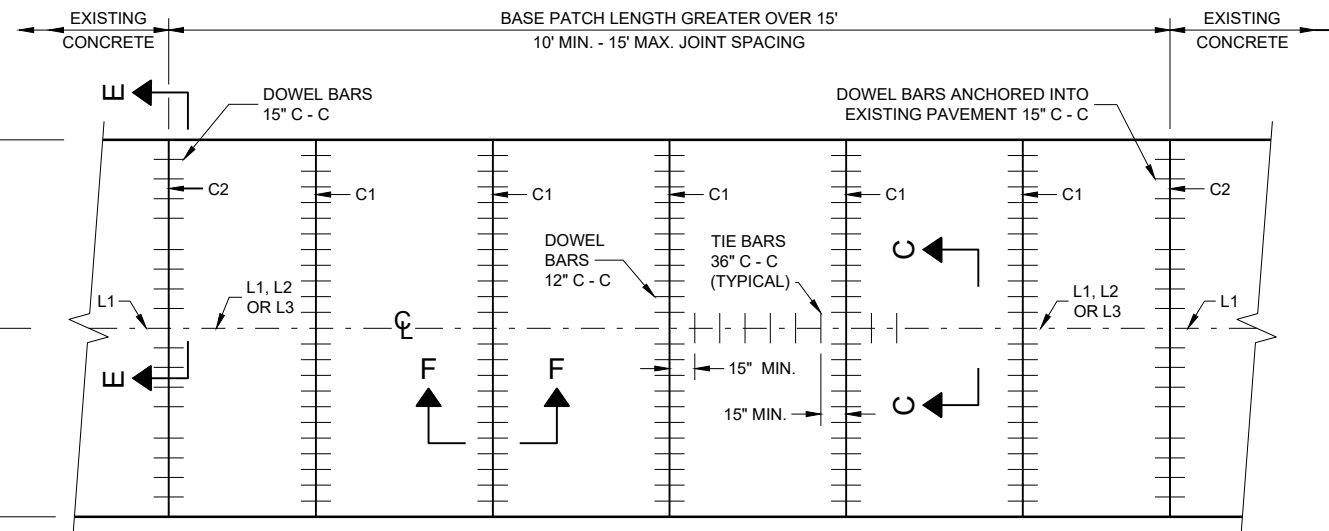
SECTION D - D



SECTION E - E
SPACING OF DOWEL BARS
ANCHORED INTO EXISTING PAVEMENT



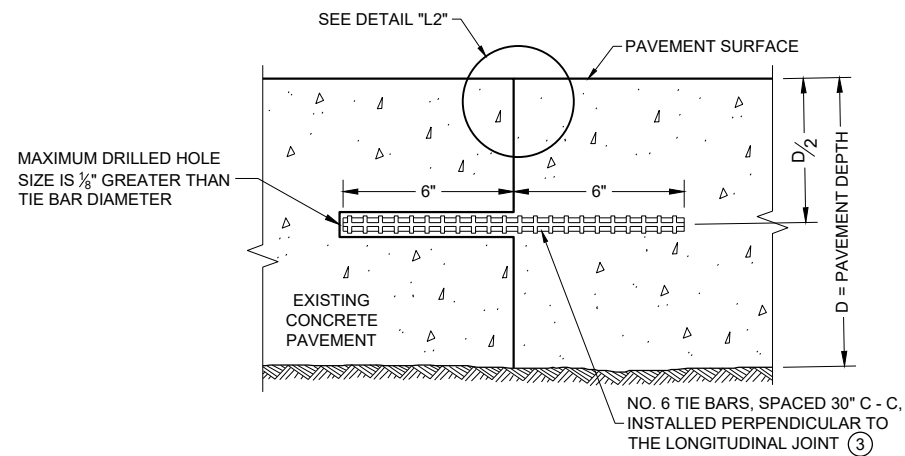
PLAN VIEW
MULTILANE CONCRETE BASE PATCH
15' MAXIMUM LENGTH



PLAN VIEW
MULTILANE CONCRETE BASE PATCH
GREATER THAN 15' IN LENGTH

BASE PATCHING CONCRETE

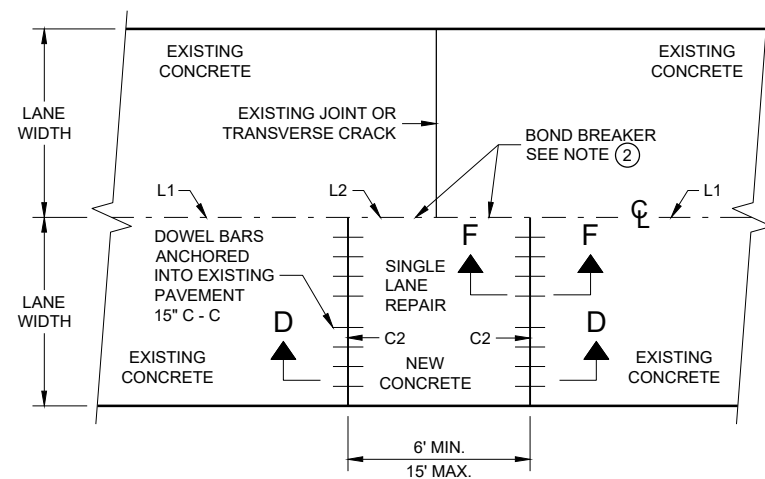
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



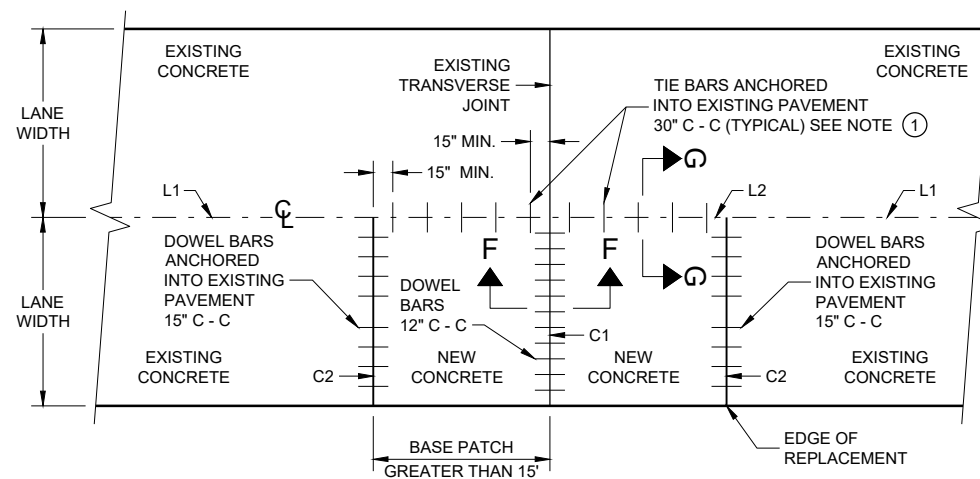
SECTION G - G
TIE BARS ANCHORED INTO EXISTING PAVEMENT

GENERAL NOTES

- ① WITH THE APPROVAL OF THE ENGINEER, FOR SINGLE LANE PAVEMENT REPLACEMENTS LESS THAN 30 FEET IN LENGTH, THE CONTRACTOR MAY INSTALL DRILLED TIE BARS ON 6:1 SKEW HORIZONTALLY, DIRECTION OF SKEW ALTERNATING WITH EACH SUCCESSIVE BAR. DRIVE SKEWED TIE BARS TO A DEPTH OF 6 INCHES IN A HOLE OF SUCH A DIAMETER AS TO PROVIDE A TIGHT DRIVEN FIT.
- ② USE AN ENGINEER APPROVED BOND BREAKER (E.G. RELEASE AGENT, CURING COMPOUND) FOR SINGLE LANE REPAIRS UP TO 15 FEET IN LENGTH.
- ③ ANCHOR TIE BARS INTO DRILLED HOES WITH AN EPOXY.



PLAN VIEW
SINGLE LANE CONCRETE BASE PATCH
15' MAXIMUM LENGTH



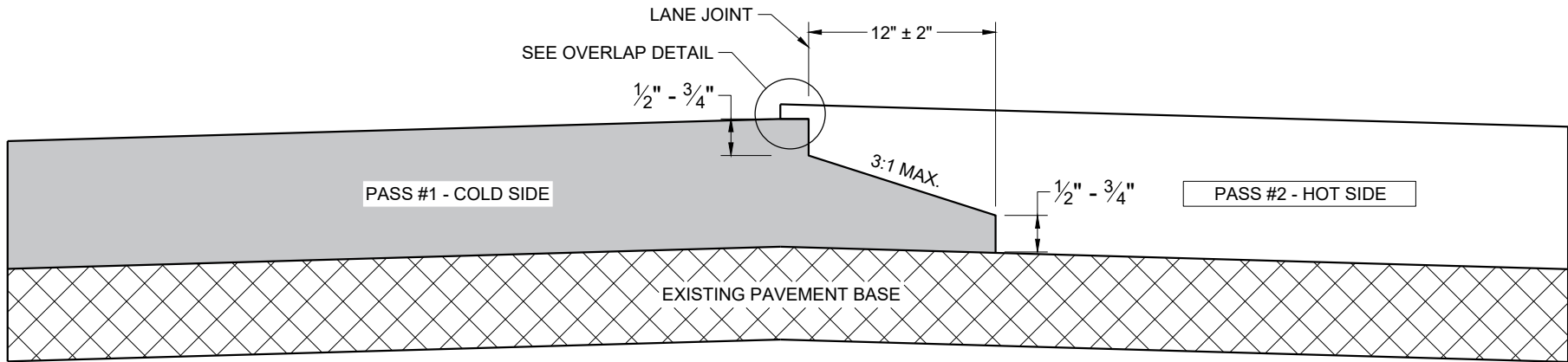
PLAN VIEW
SINGLE LANE CONCRETE BASE PATCH
GREATER THAN 15' LENGTH

BASE PATCHING CONCRETE

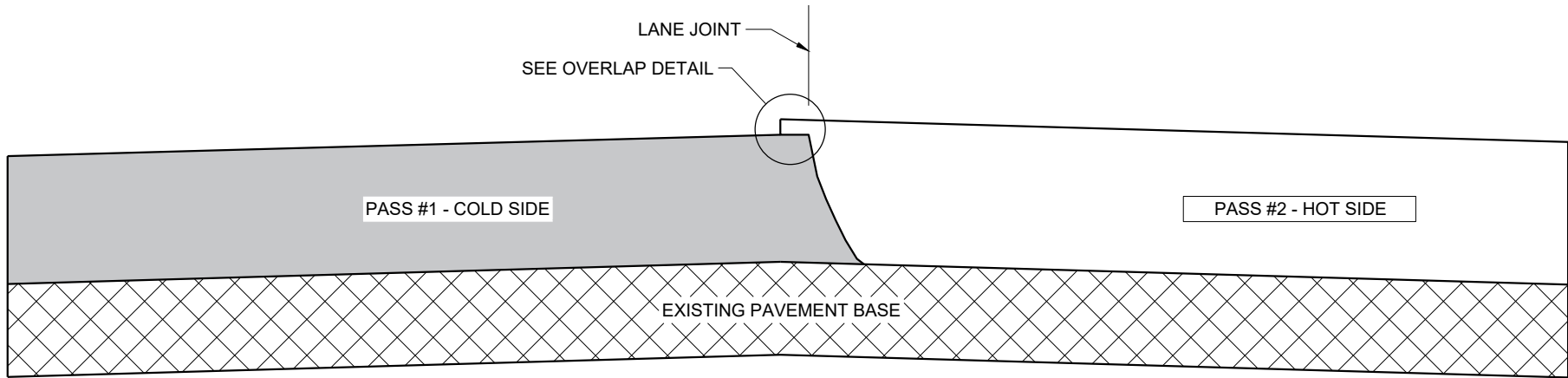
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018 /S/ Peter Kemp, P.E.
DATE PAVEMENT SUPERVISOR

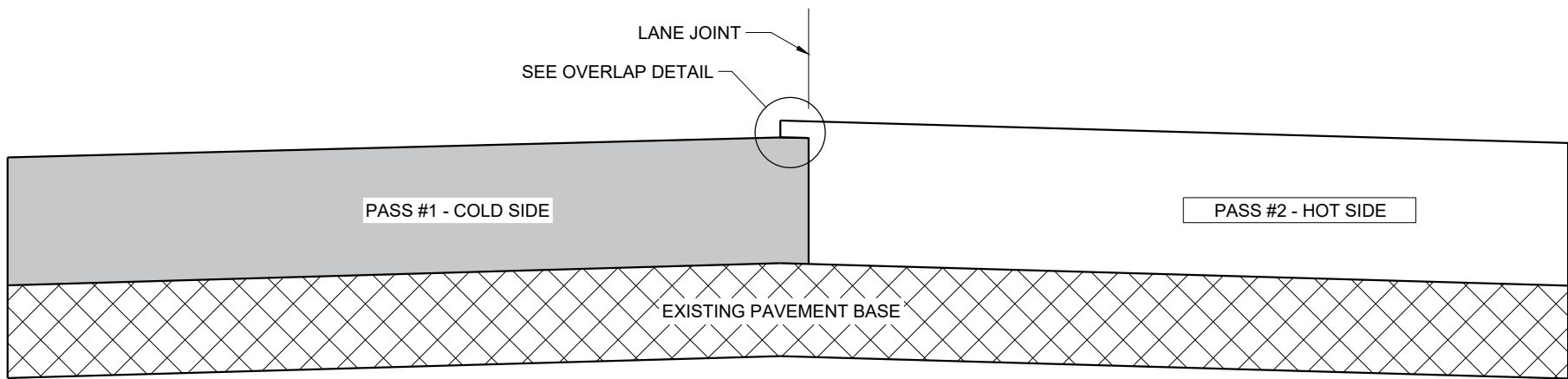
FHWA



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

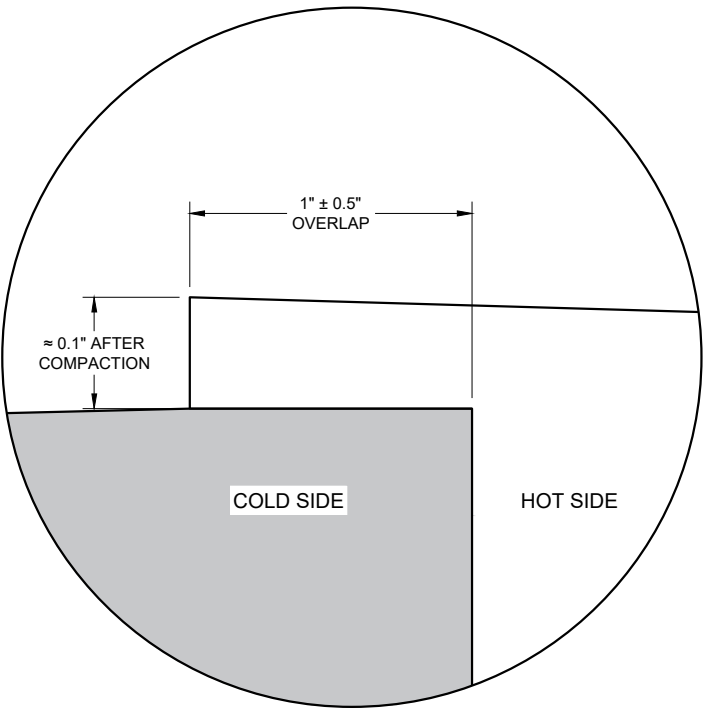
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY 1" ± 0.5" AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



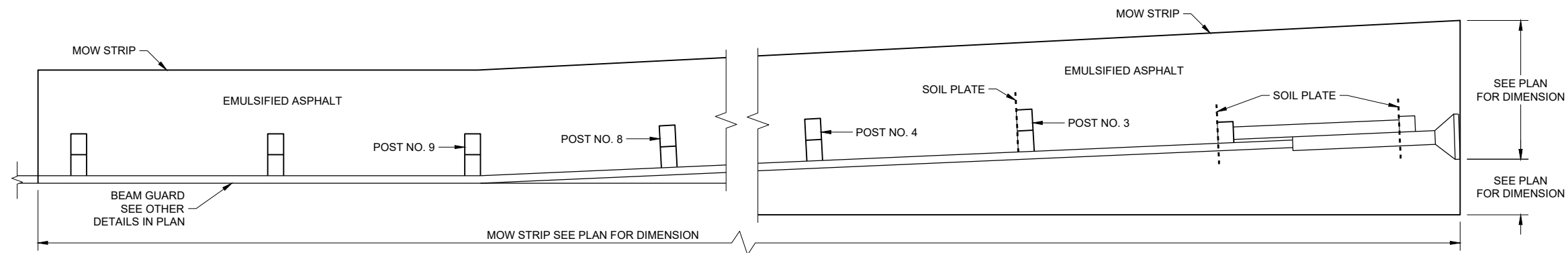
OVERLAP DETAIL (TYPICAL)

HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2020
DATE /S/ Steven Hefel
HMA PAVEMENT ENGINEER

FHWA

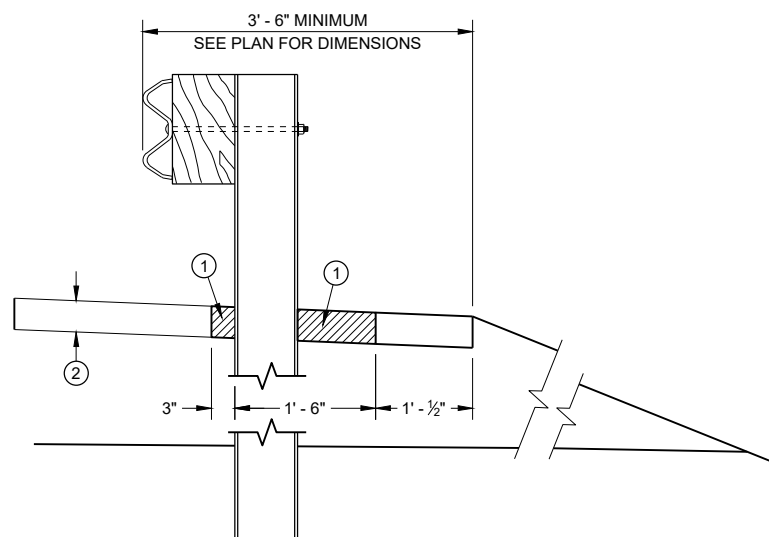


PLAN VIEW
MOW STRIP LAYOUT FOR ENERGY ABSORBING TERMINAL

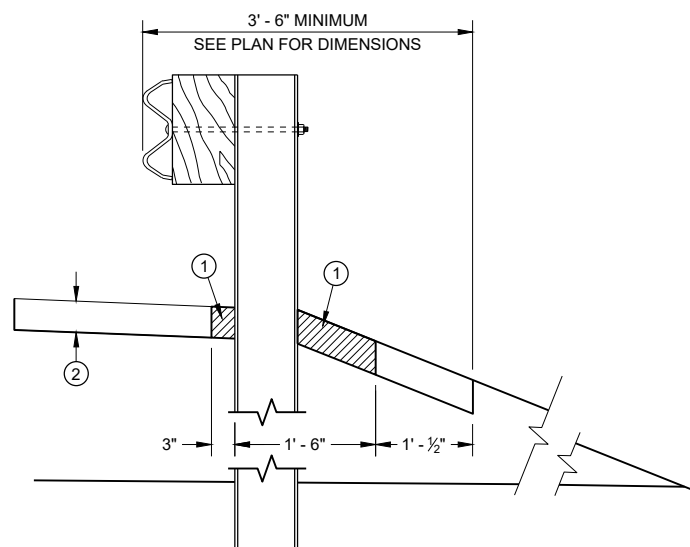
GENERAL NOTES

ONLY USE STEEL POSTS IN CONCRETE AND ASPHALT MOW STRIPS.

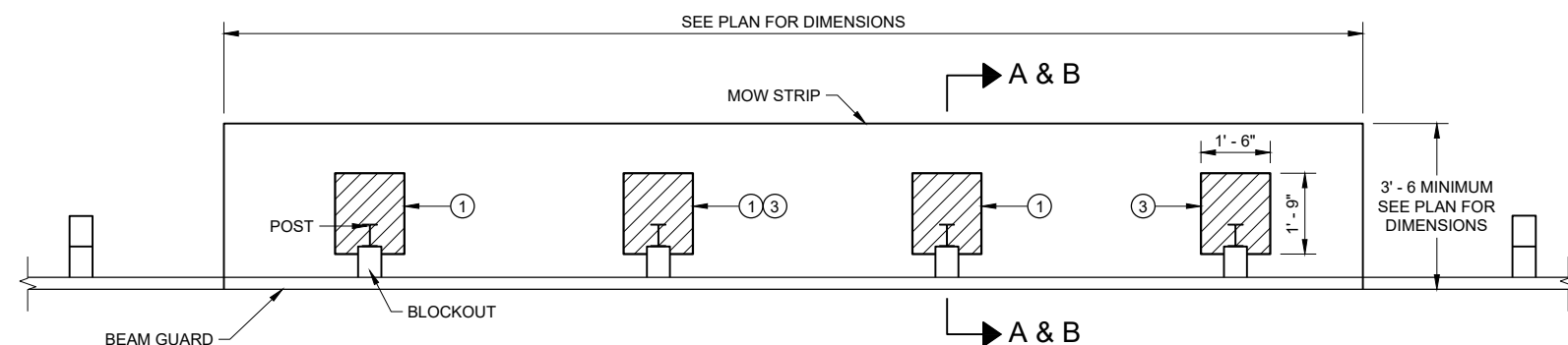
- ① CONTROLLED LOW-STRENGTH BACKFILL OR EMULSIFIED ASPHALT.
- ② DEPTH OF MOW STRIP:
ASPHALT - 4"
CONCRETE - 4"
EMULSIFIED ASPHALT - 1" OR LESS
- ③ FOR EMULSIFIED ASPHALT, MOW STRIP STRIP LEAVE OUTS NOT REQUIRED. (TYPICAL FOR ALL POSTS)



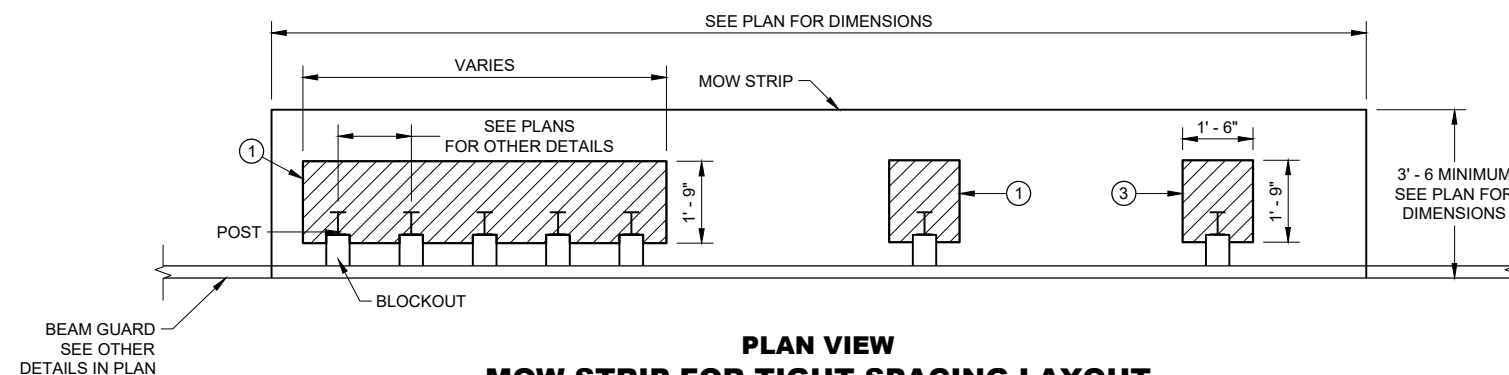
SECTION A - A



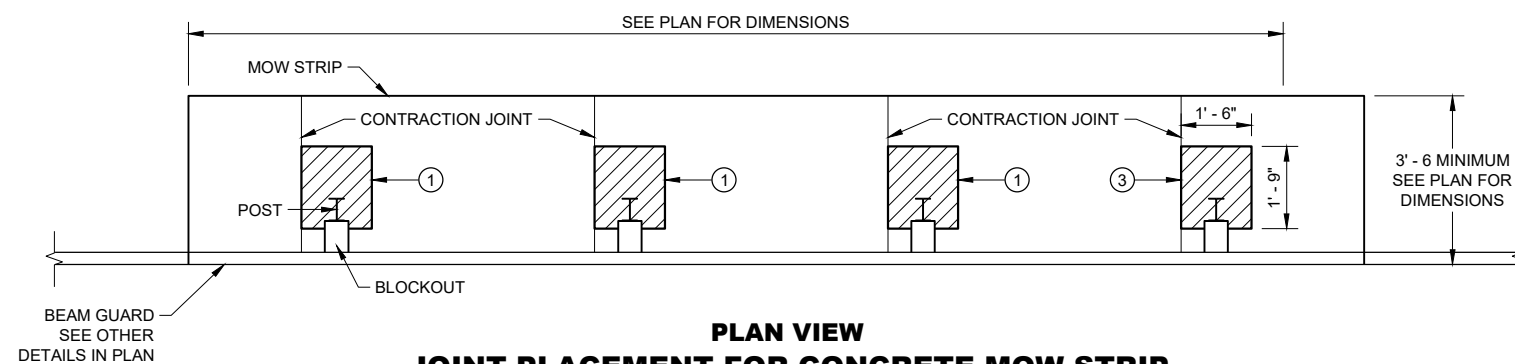
SECTION B - B



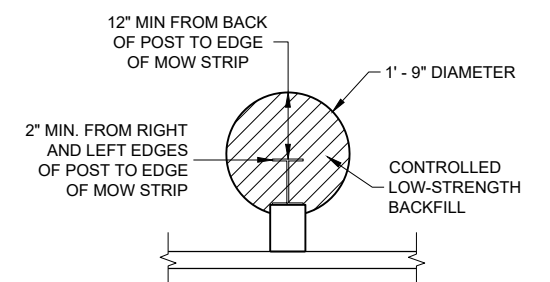
PLAN VIEW
MOW STRIP FOR TYPICAL BLOCKOUT LAYOUT



PLAN VIEW
MOW STRIP FOR TIGHT SPACING LAYOUT



PLAN VIEW
JOINT PLACEMENT FOR CONCRETE MOW STRIP



ALTERNATIVE HMA
MOW STRIP DESIGN

GUARDRAIL MOW STRIP

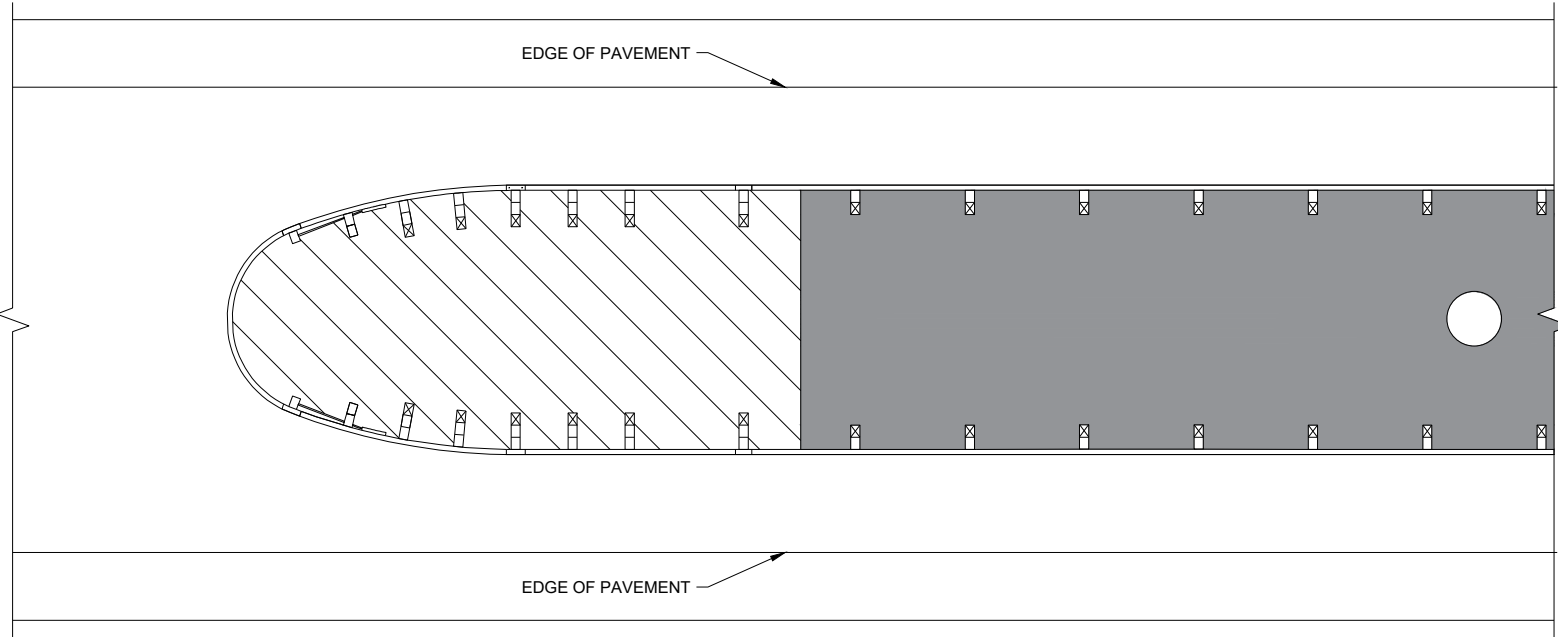
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

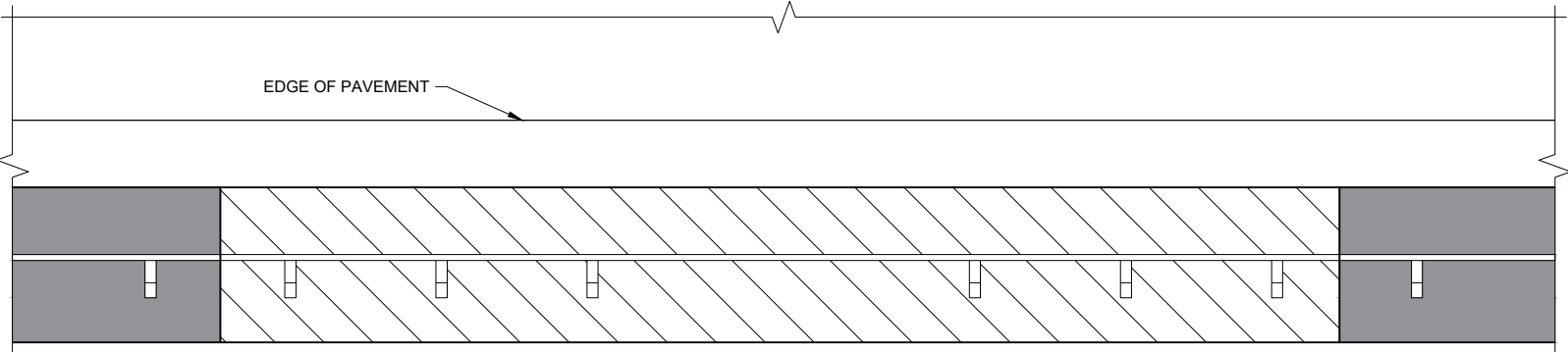
- CONCRETE, ASPHALT, OR EMULSIFIED ASPHALT MOW STRIP (SEE OTHER DETAILS)
- EMULSIFIED ASPHALT MOW STRIP (SEE OTHER DETAILS)

GENERAL NOTES

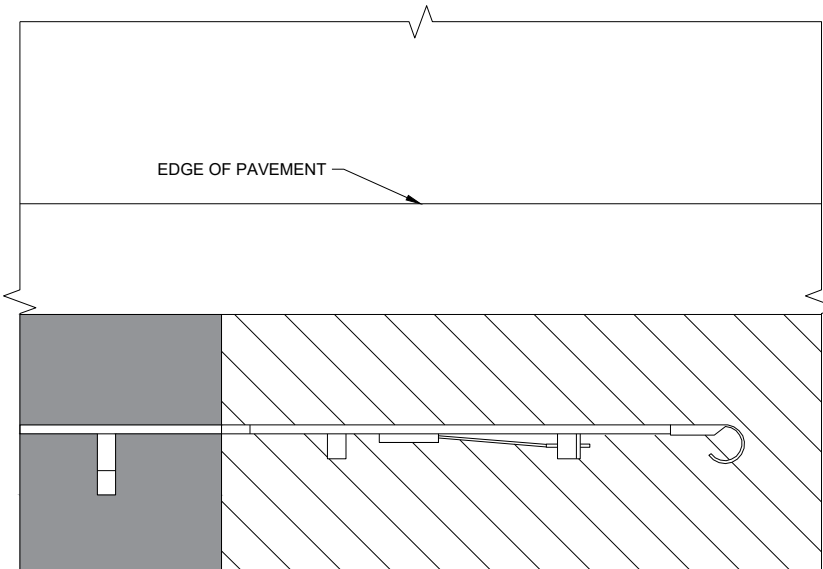
EXISTING THRIE BEAM BULLNOSES MAY HAVE WOOD POSTS. NEW THRIE BEAM BULLNOSE WILL HAVE STEEL POSTS.



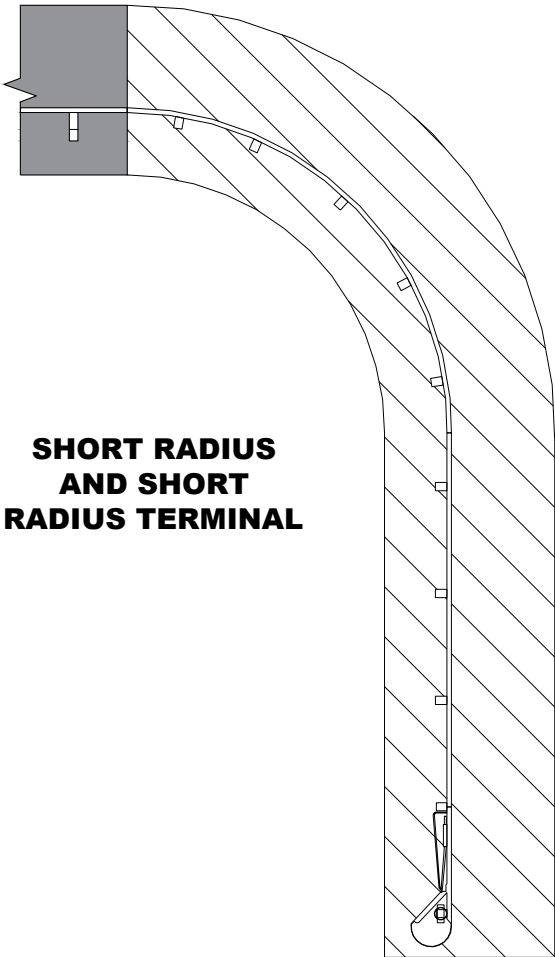
THRIE BEAM BULLNOSE



LONG - SPAN



TYPE 2 TERMINAL



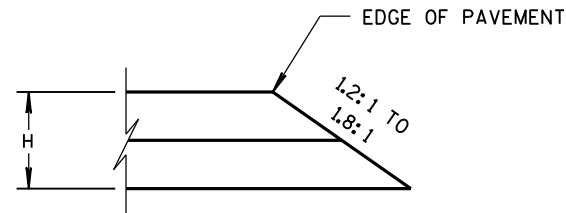
GUARDRAIL MOW STRIP

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

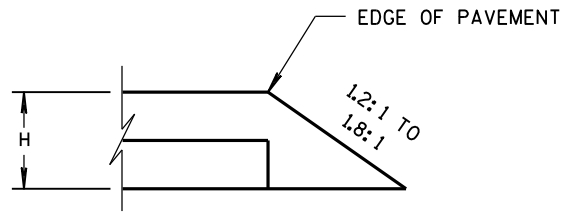
APPROVED
August 2020
DATE

/S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
ENGINEER

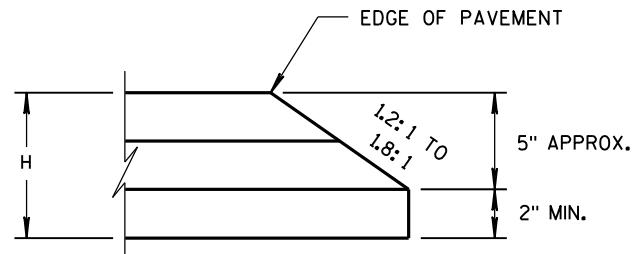
FHWA



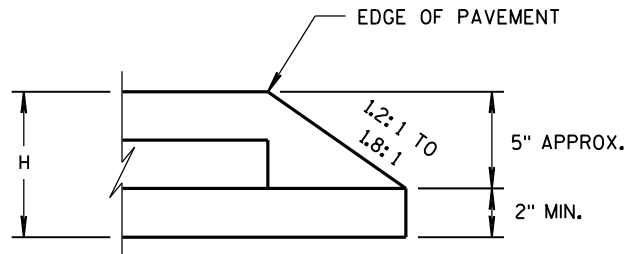
CONSTRUCTED WITH FINAL TWO LAYERS
FOR H 5" OR LESS



CONSTRUCTED WITH FINAL LAYER
FOR H 5" OR LESS

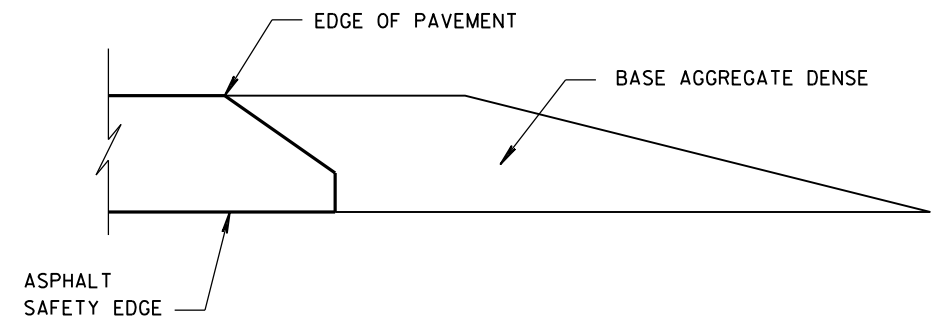


CONSTRUCTED WITH FINAL TWO LAYERS
FOR H GREATER THAN 5"



CONSTRUCTED WITH FINAL LAYER
FOR H GREATER THAN 5"

HMA PAVEMENT AND HMA OVERLAYS



FINISHED SHOULDER AGGREGATE PLACEMENT

SAFETY EDGE_{SM}

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

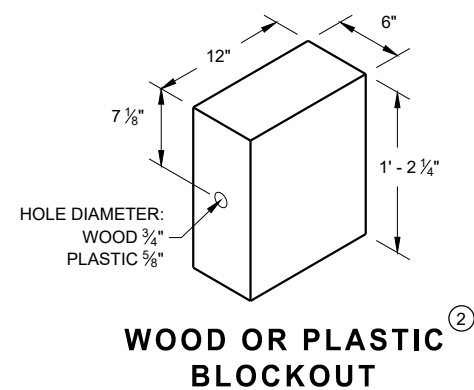
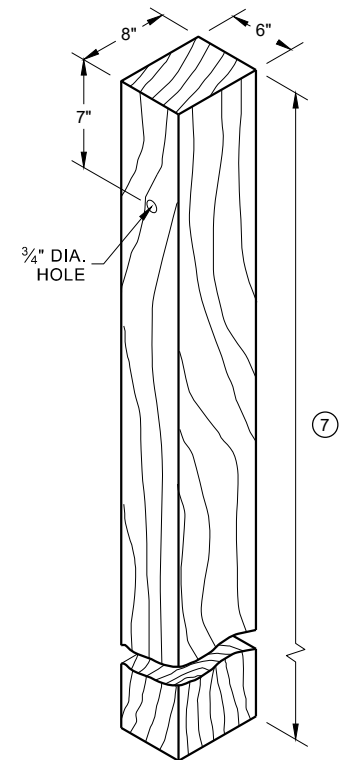
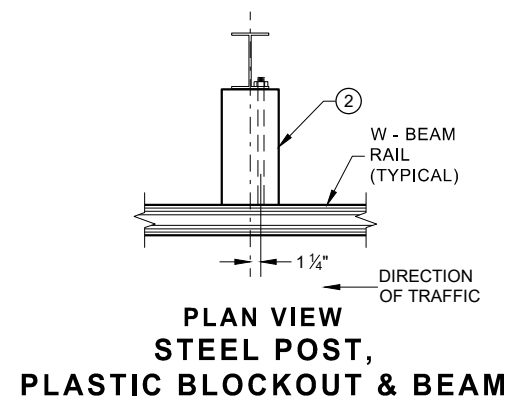
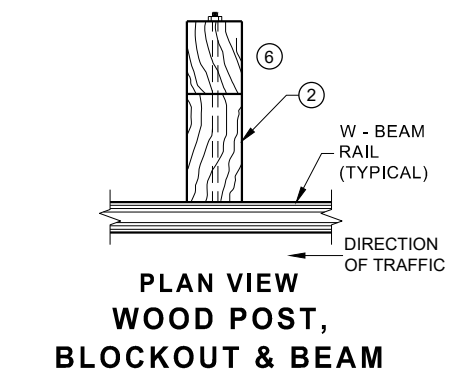
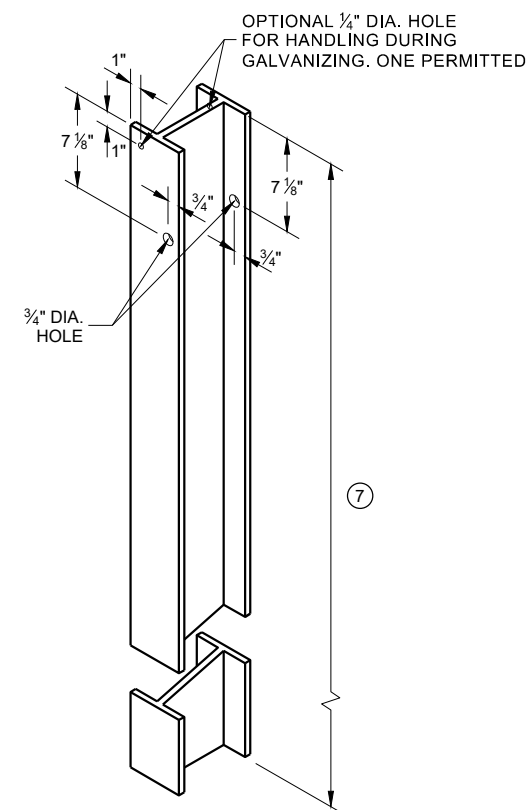
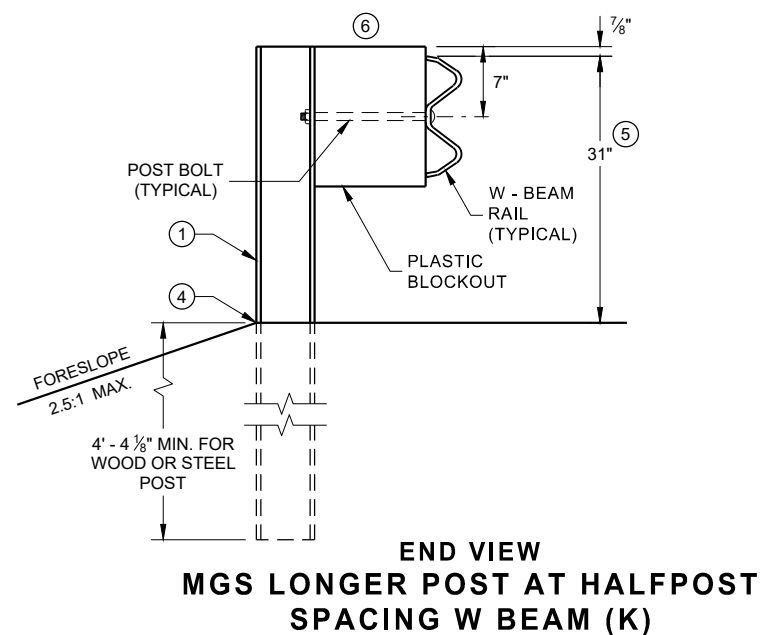
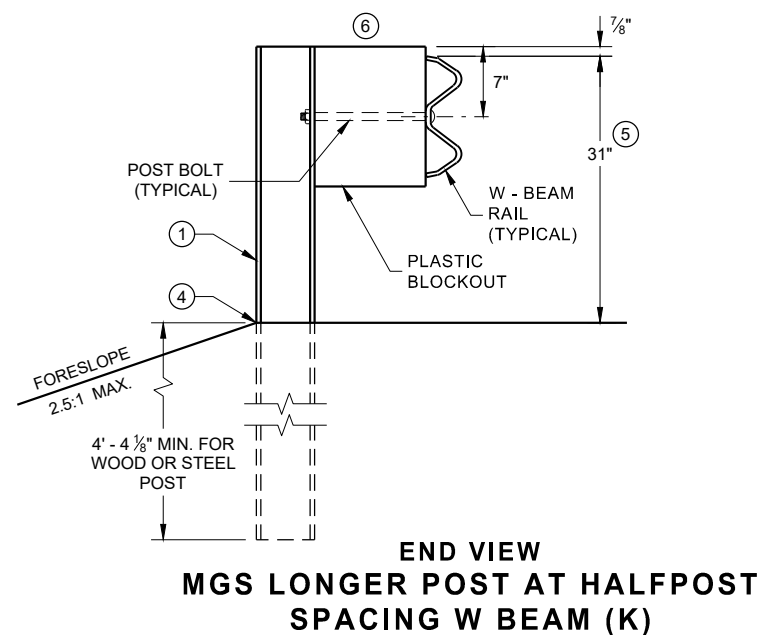
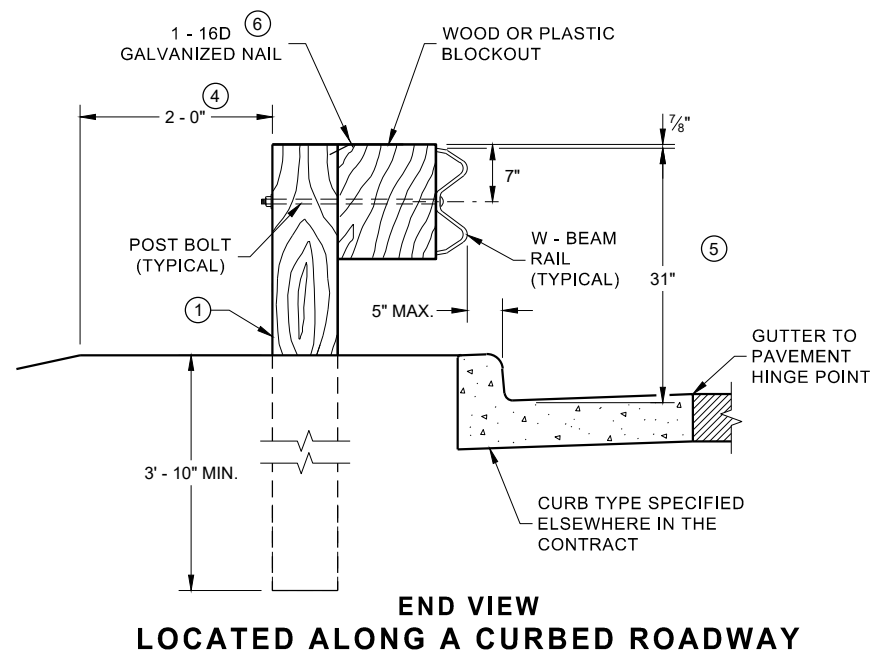
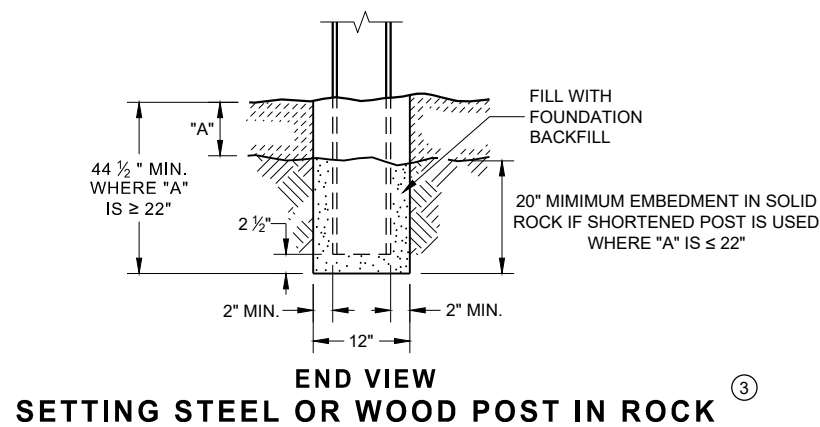
APPROVED

11/30/2012
DATE

FHWA

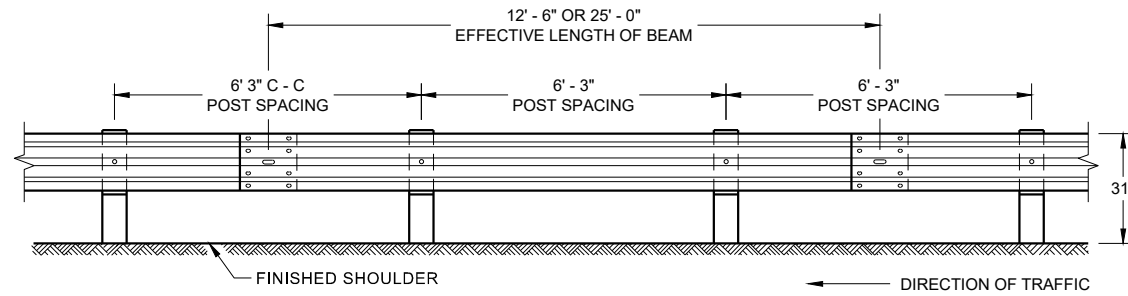
/s/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER

- WOOD OR STEEL POSTS (w6X9 OR w6X8.5) MAY BE USED. DO NOT INTERMIX WOOD AND STEEL POSTS. INSTALL STEEL POSTS WITH HOLES ON APPROACHING TRAFFIC SIDE.
- USE WOOD OR APPROVED PLASTIC BLOCKOUTS. WOOD BLOCKOUTS MAY BE CONSTRUCTED OUT OF TWO OR MORE WOOD BLOCKOUTS. SEE ALTERNATE WOOD BLOCKOUT DETAIL. DIMENSIONS OF APPROVED PLASTIC BLOCKOUTS MAY VARY.
- IF ROCK IS ENCOUNTERED DURING EXCAVATION, PROVIDE A HOLE 12 INCHES IN DIAMETER EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE APPROXIMATELY 2 1/2" INCHES OF GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE. CUT THE POSTS THE TO LENGTH AMD INSTALL. BACKFILL WITH EXCAVATED MATERIAL AND COMPACT. BACKFILL IS TO BE FREE OF LARGE ROCKS.
- WHEN THE DISTANCE FROM BACK OF POST TO SHOULDER HINGE POINT IS LESS THAN 2 FEET INSTALL LONGER POST AT HALF POST SPACING (K).
- FOR NEW MGS INSTALLATION TOP OF W-BEAM RAIL TOLERANCE IS $\pm 1"$. FOR EXISTING MGS INSTALLATION TOP OF W-BEAM IS BETWEEN 27 3/4" TO 32".
- WHEN USING STEEL POST AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.
- TOTAL POST LENGTH FOR TYPE K IS 7' - 0".
TOTAL POST LENGTH FOR OTHER MGS TYPES IS 6' - 0".

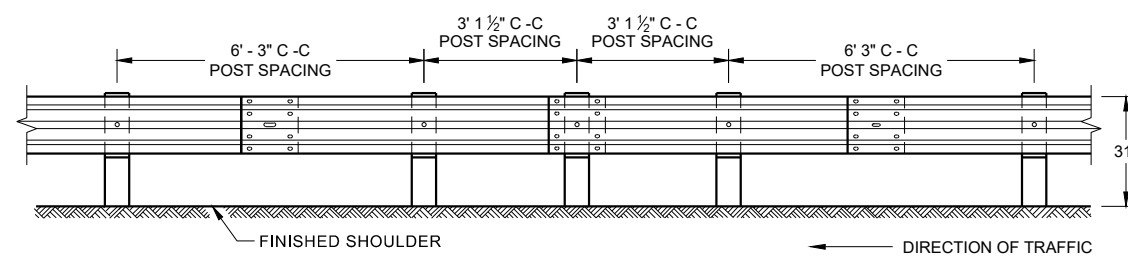


**MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL**

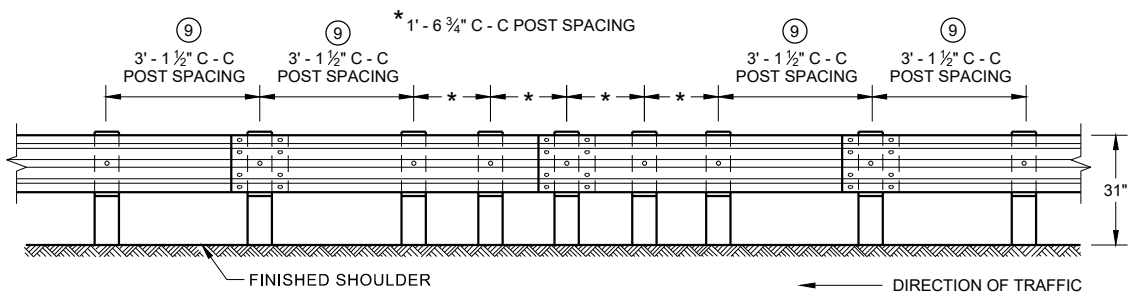
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



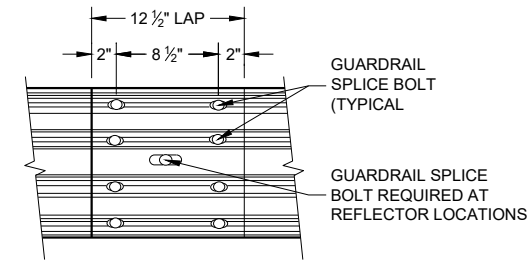
**FRONT VIEW
POST SPACING STANDARD INSTALLATION**



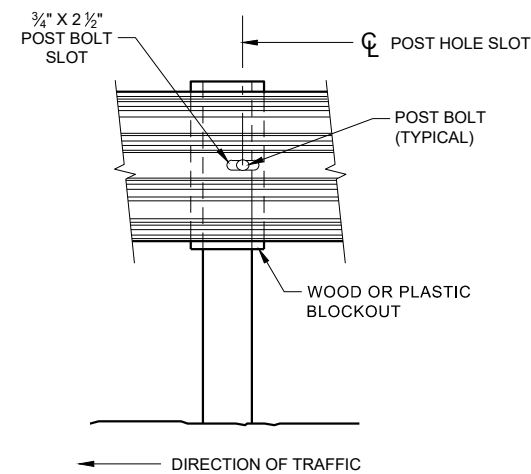
**FRONT VIEW
HALF POST SPACING (HS) AND
HALF POST SPACING WITH LONGER POSTS (K)**



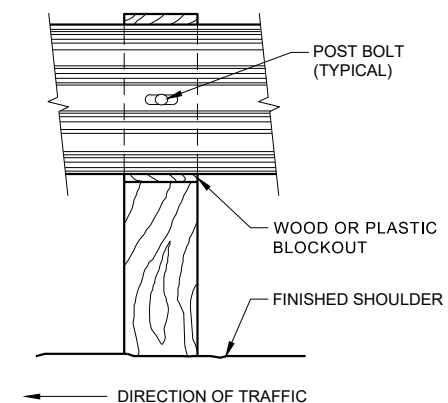
**FRONT VIEW
QUARTER POST SPACING (QS)**



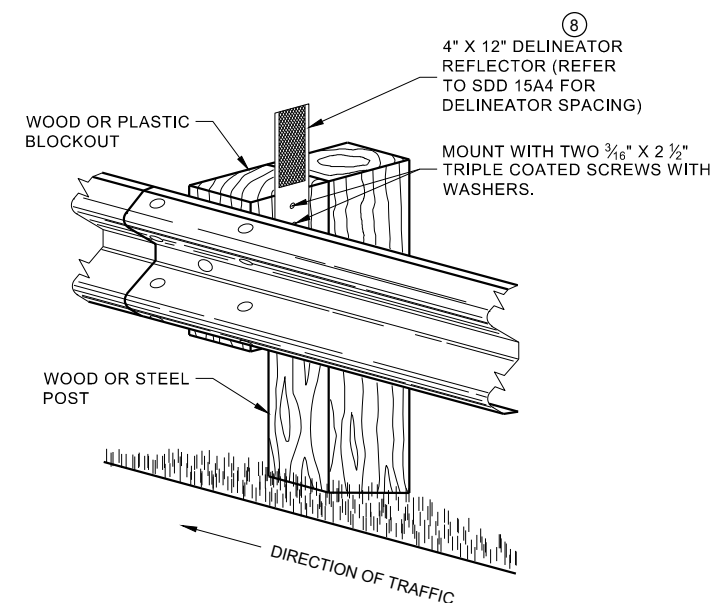
**FRONT VIEW
MID-SPAN BEAM SPLICE**



FRONT VIEW AT STEEL POST



FRONT VIEW AT WOOD POST



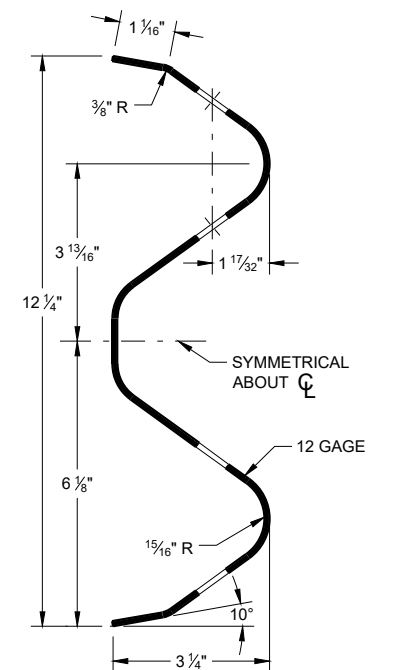
**ONE SIDED REFLECTOR DETAIL
AND TYPICAL INSTALLATION**

GENERAL NOTES

- 8 DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.
- 9 25 FEET OF HALF POST SPACING IS REQUIRED ON APPROACH AND DEPARTURE ENDS OF QUARTER POST SPACING.

POST BOLTS ARE A 3/8" DIAMETER ASTM A307 GUARDRAIL BOLT. A POST BOLT REQUIRES 3/4" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT AND 3/8" DIAMETER F844 FLAT WASHER. POST BOLTS MAY BE LONGER IF MULTIPLE BLOCKOUTS ARE BEING USED.

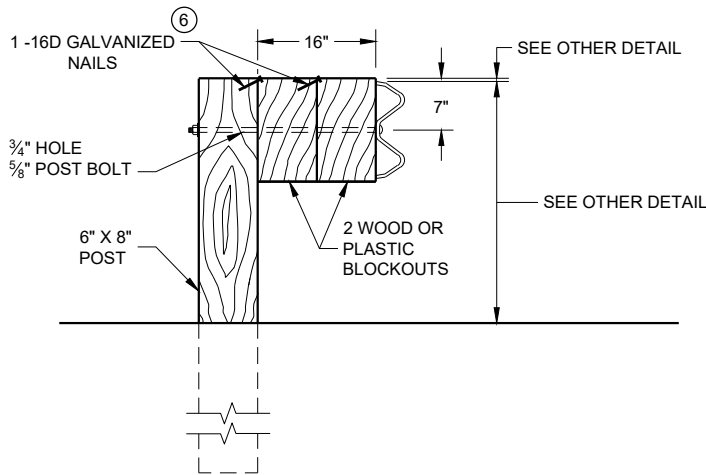
GUARD RAIL SPLICE BOLTS ARE A 3/8" DIAMETER ASTM A307 GUARDRAIL HEAD BOLT. A GUARDRAIL SPLICE BOLT REQUIRES 3/4" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT.



SECTION THRU W-BEAM RAIL

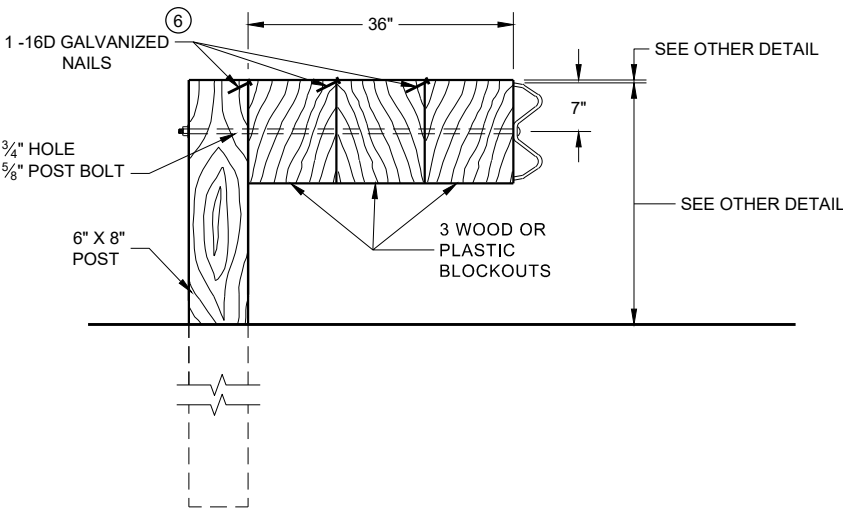
**MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



DETAIL FOR 16" BLOCKOUT DEPTH

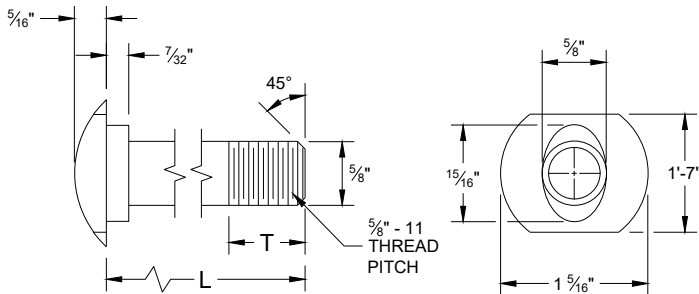
IT IS ACCEPTABLE TO USE BLOCKOUTS UP TO 16" DEEP TO INCREASE THE POST OFFSET TO AVOID UNDERGROUND OBSTACLES. THERE IS NO LIMIT TO THE NUMBER OF POSTS THAT CAN HAVE ADDITIONAL BLOCKOUTS UP TO 16" DEEP.



DETAIL FOR 36" BLOCKOUT DEPTH

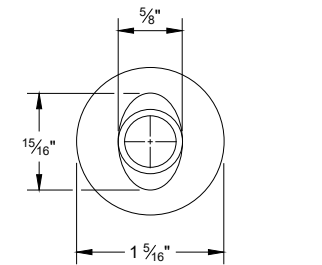
NOTES: UNDER SPECIAL CIRCUMSTANCES, SUCH AS AVOIDING OBSTACLES THAT ARE NOT RELOCATED, IT IS ACCEPTABLE TO INSTALL ADDITIONAL BLOCKOUTS TO OBTAIN UP TO 36" DEPTH FOR ONE OR TWO POSTS IN A SECTION OF GUARDRAIL.
DO NOT USE 16" OR 36" BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.

- NOTE:
- 1. ALL FILLETS SHALL HAVE A MINIMUM RADIUS OF 3/16".
 - 2. IF THE BOLT EXTENDS MORE THAN 1/4" FROM THE NUT THE BOLT SHOULD BE TRIMMED BACK.

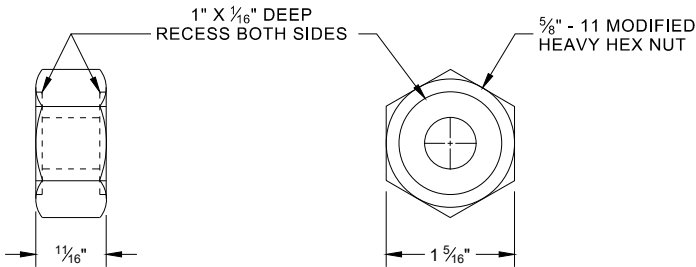


POST BOLT TABLE

L	T (MIN.)
1 1/4"	1 1/8"
2"	1 3/4"
10"	4"
14"	4 1/16"
18"	4"
21"	4 1/16"
25"	4"

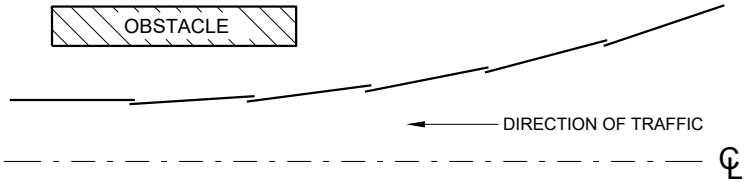


ALTERNATE BOLT HEAD

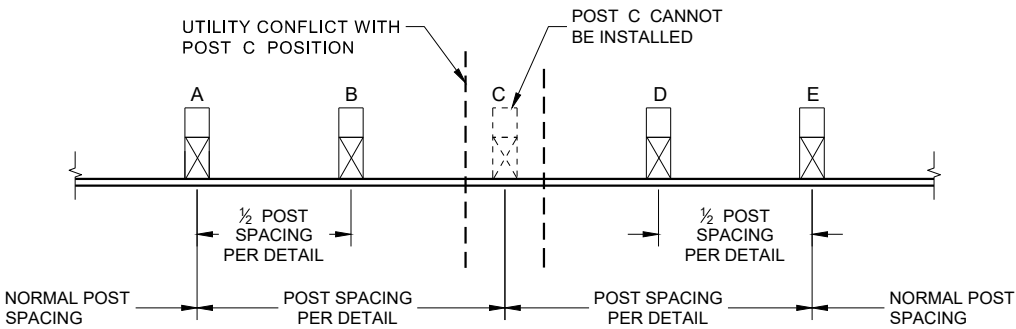


POST BOLT, SPLICE BOLT AND RECESS NUT

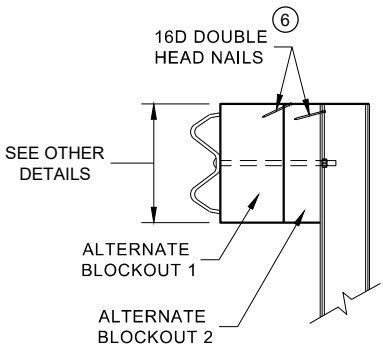
- 6 WHEN USING STEEL POST AD WOOD BLOCKOUTS, INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.



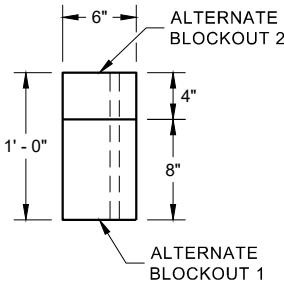
PLAN VIEW
BEAM LAPPING DETAIL



POST DRIVING FOR CONTINUOUS
UNDERGROUND OBSTRUCTION



SIDE VIEW

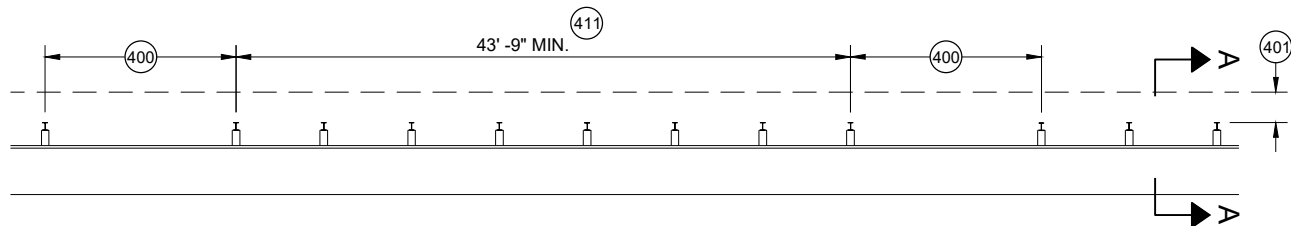


PLAN VIEW

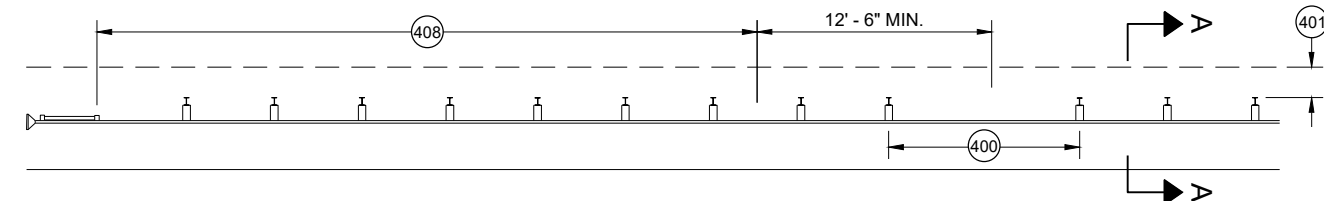
ALTERNATE WOOD
BLOCKOUT DETAIL

MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL

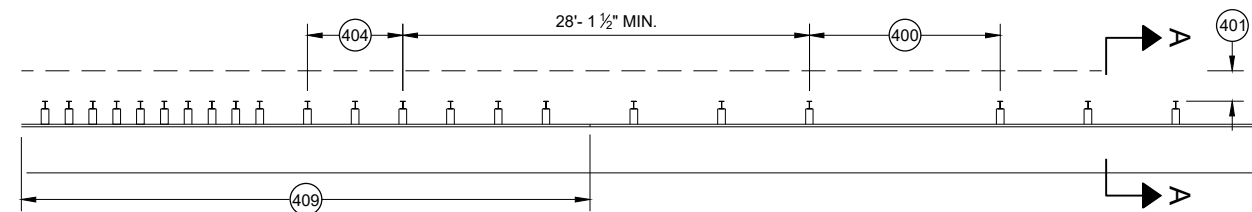
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



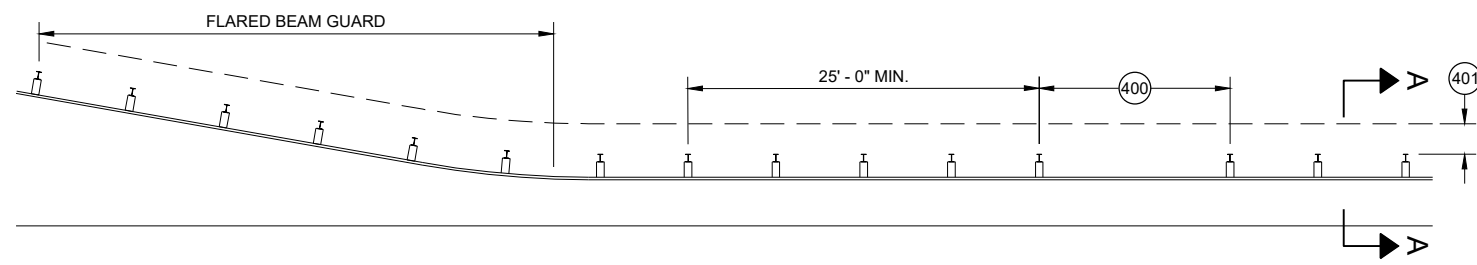
MISSING POST IN MGS GUARDRAIL



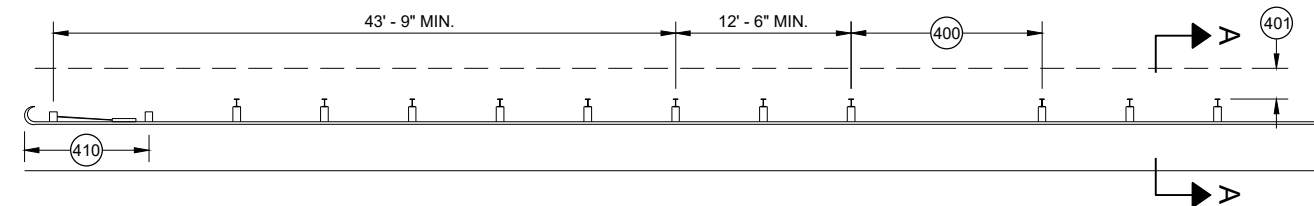
MISSING POST IN MGS GUARDRAIL NEAR EAT



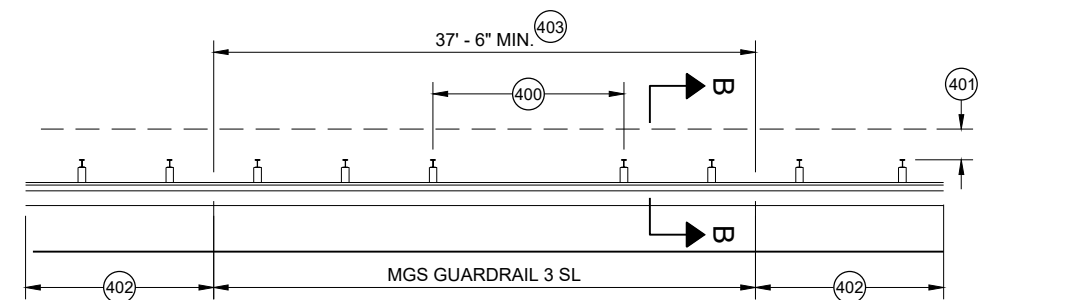
MISSING POST IN MGS GUARDRAIL NEAR AN APPROACH TRANSITION



MISSING POST IN MGS GUARDRAIL NEAR FLARED BEAM GUARD

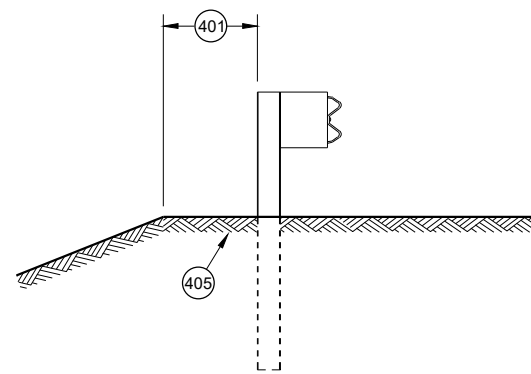


MISSING POST IN MGS GUARDRAIL NEAR A TYPE 2 END TERMINAL

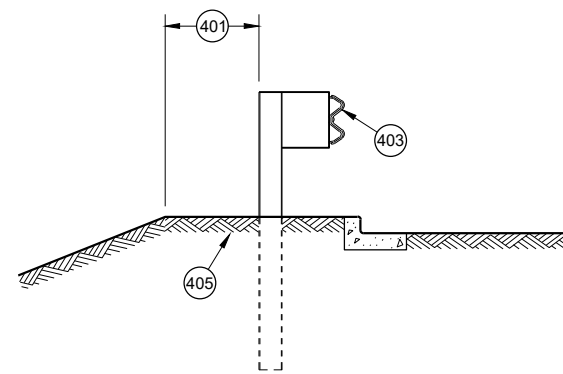


MISSING POST IN SHORT SPAN MGS GUARDRAIL NEAR CURB (SL)

- (400) MAX SPAN 12' - 6"
- (401) 2' MIN.
- (402) MGS GUARDRAIL 3
- (403) NESTING BEAM GUARD
- (404) ASYMMETRIC TRANSITION
- (405) SOIL WELL DRAINED AND COMPACTED
- (406) SEE OTHER DRAWINGS IN THIS SDD
- (407) SEE OTHER DRAWINGS FOR MIN. SPACING BETWEEN SPANS
- (408) SEE SDD 14B44
- (409) SEE SDD 14B45
- (410) SEE SDD 14B47
- (411) MINIMUM DISTANCE BETWEEN MISSING POST SPANS.



SECTION A - A



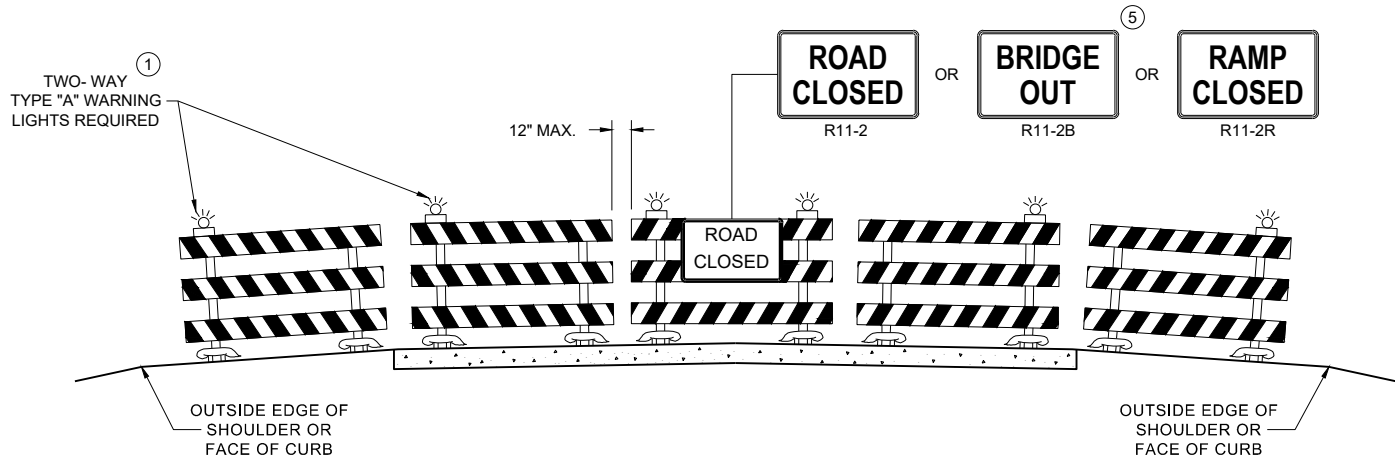
SECTION B - B

**MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL**

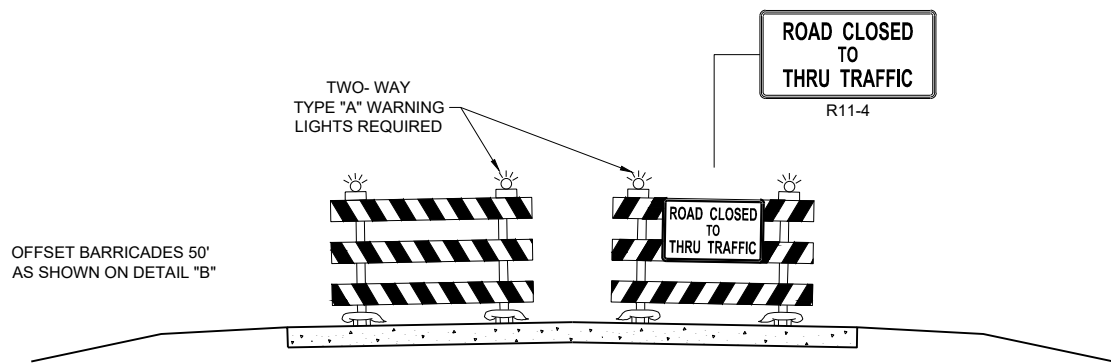
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2021
DATE
/S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR

FHWA



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

PARTIAL NUMBERS ON SIGNS SHALL BE DISPLAYED AS A WHOLE NUMBER(AS NEEDED) FOLLOWED BY A FRACTION. SIGNS SHALL NOT DISPLAY NUMBERS IN DECIMAL FORM.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60 " X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

SIGNS PLACED ON TYPE III BARRICADES THAT ARE SIZES OTHER THAN 48"X30" SHALL HAVE A CORRUGATED POLYPROPYLENE OR POLYETHYLENE PLASTIC SIGN BASE.

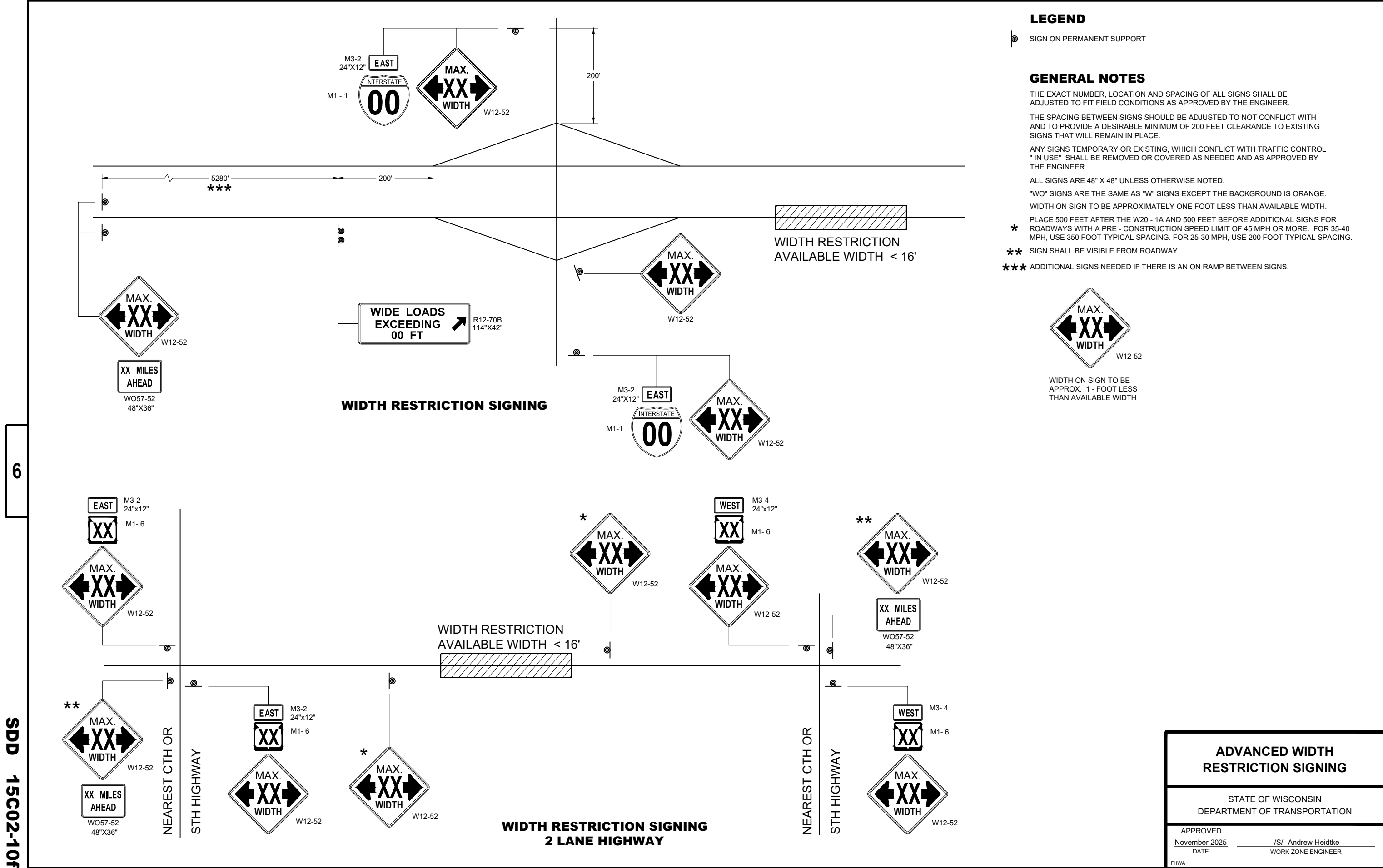
- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES

STATE OF WISCONSIN
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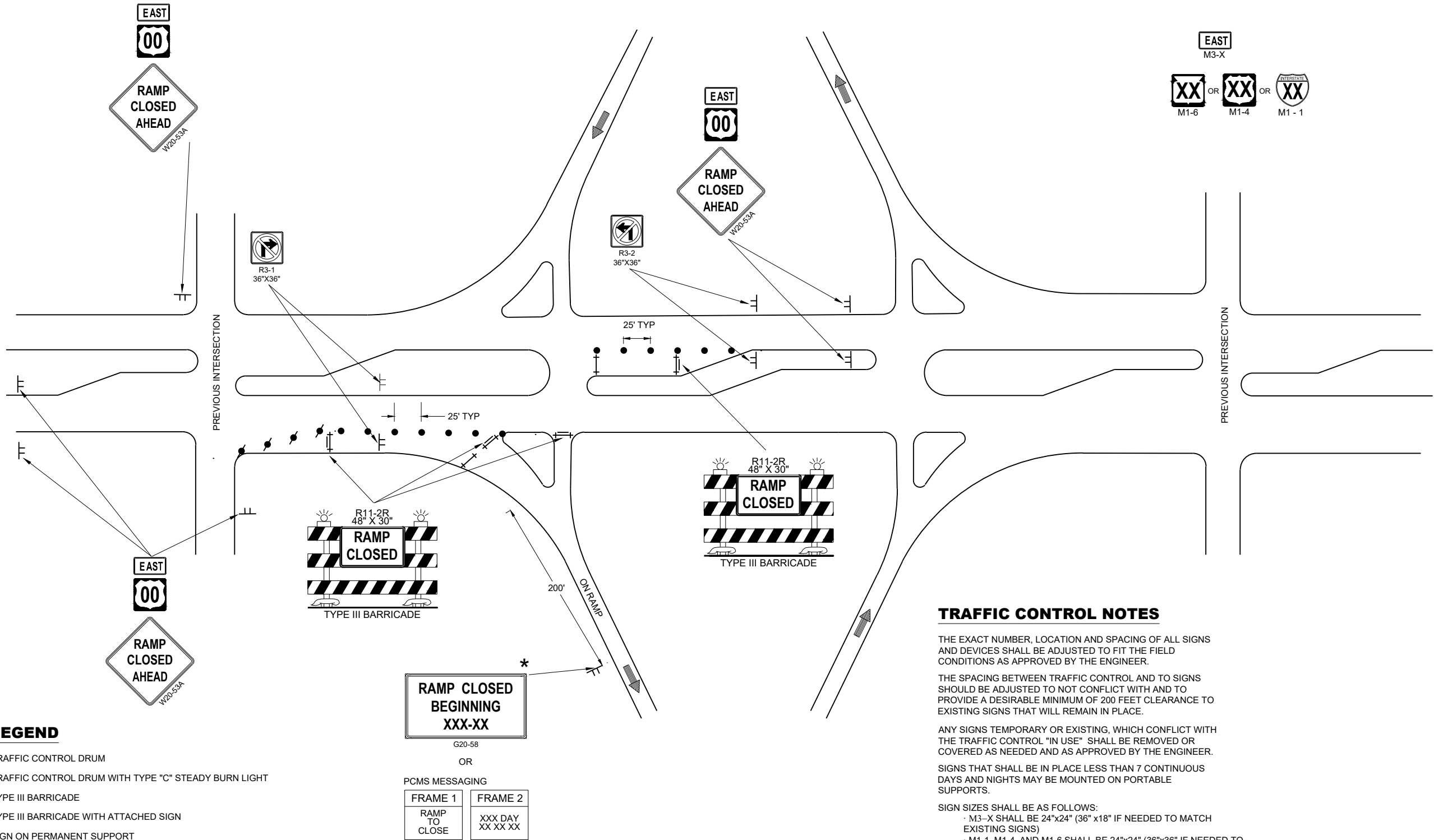
APPROVED
November 2025 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA



LEGEND

- TRAFFIC CONTROL DRUM
- ☼ TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- ↑ TYPE III BARRICADE
- ↑↓ TYPE III BARRICADE WITH ATTACHED SIGN
- ▮ SIGN ON PERMANENT SUPPORT
- ┌ SIGN ON TEMPORARY SUPPORT
- DIRECTION OF TRAFFIC



TRAFFIC CONTROL FOR ENTRANCE RAMP CLOSURE

TRAFFIC CONTROL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT THE FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL AND TO SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

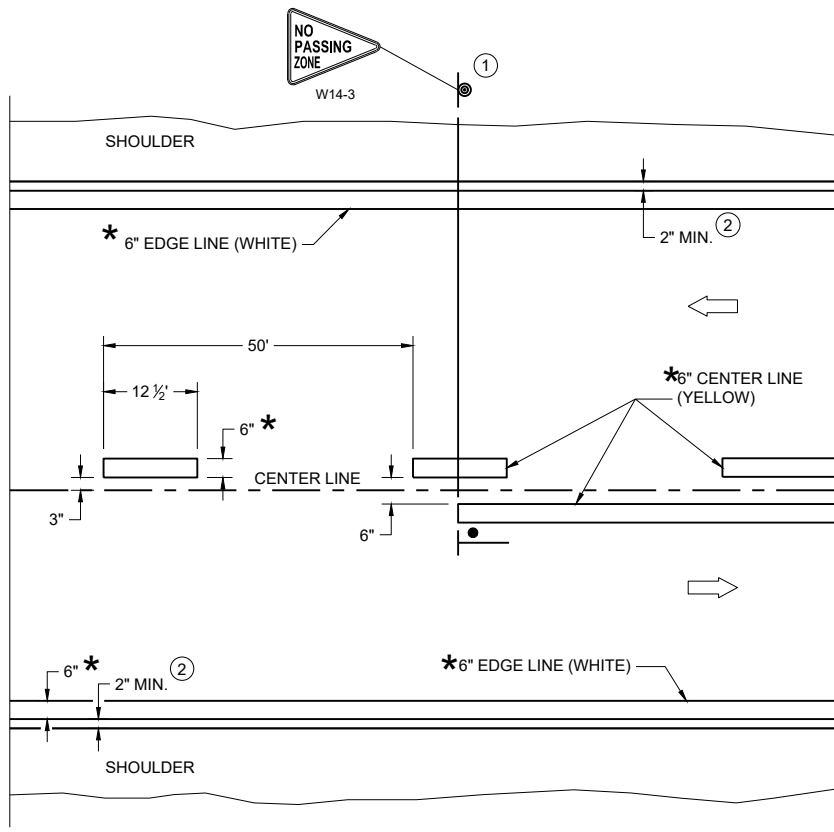
SIGNS THAT SHALL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

SIGN SIZES SHALL BE AS FOLLOWS:

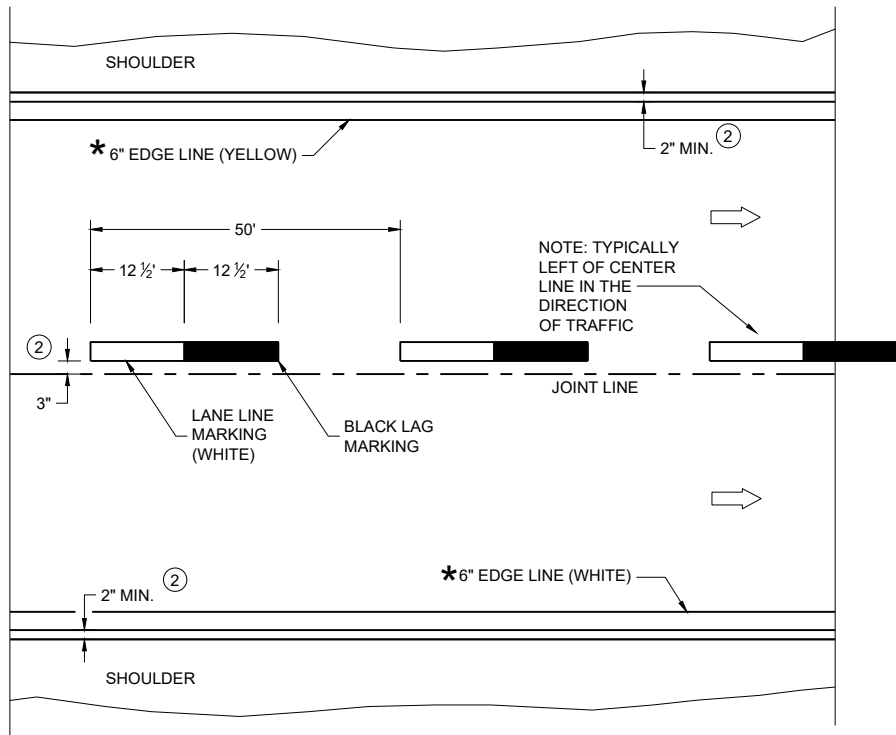
- M3-X SHALL BE 24"x24" (36" x18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-1, M1-4, AND M1-6 SHALL BE 24"x24" (36"x36" IF NEEDED TO MATCH EXISTING SIGNS)
- W20-53A SHALL BE 48"x48"

* PLACE "RAMP CLOSED BEGINNING" SIGN 7 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR LAYOUT.

TRAFFIC CONTROL FOR ENTRANCE RAMP CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2025 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

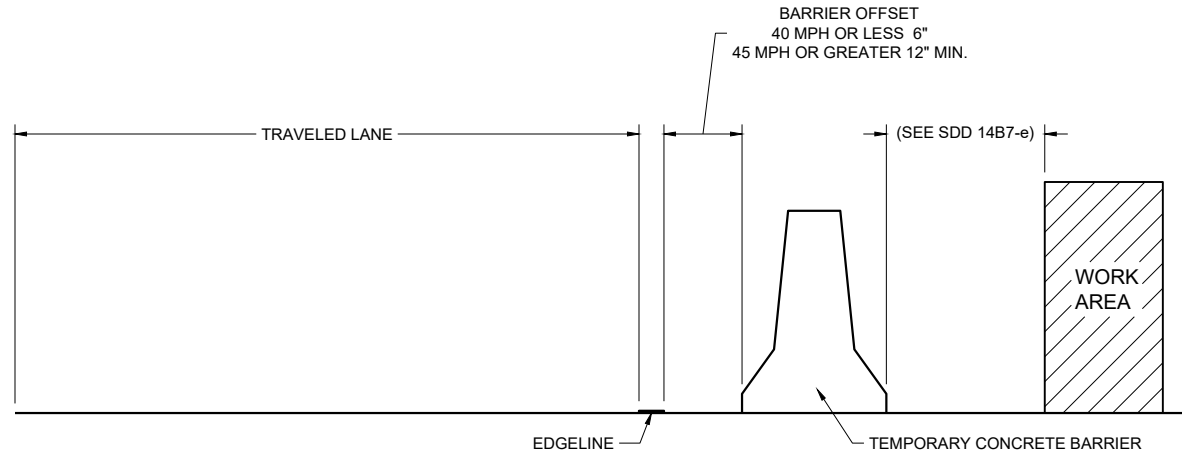
- "T" MARKING
- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Jeannie Silver
DATE Statewide Pavement Marking Engineer

FHWA



TEMPORARY BARRIER OFFSET FROM EDGE LINE

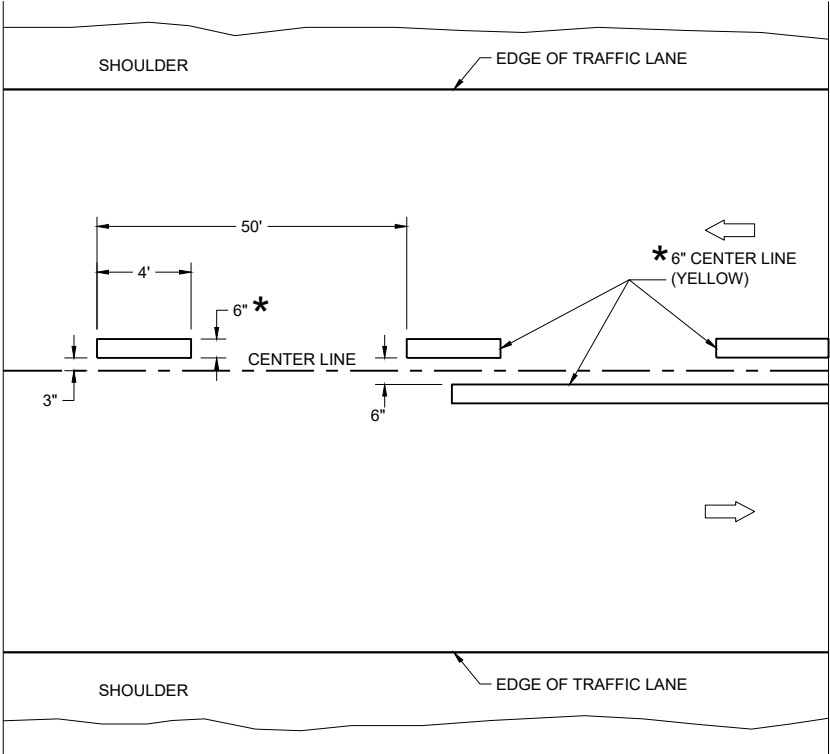
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

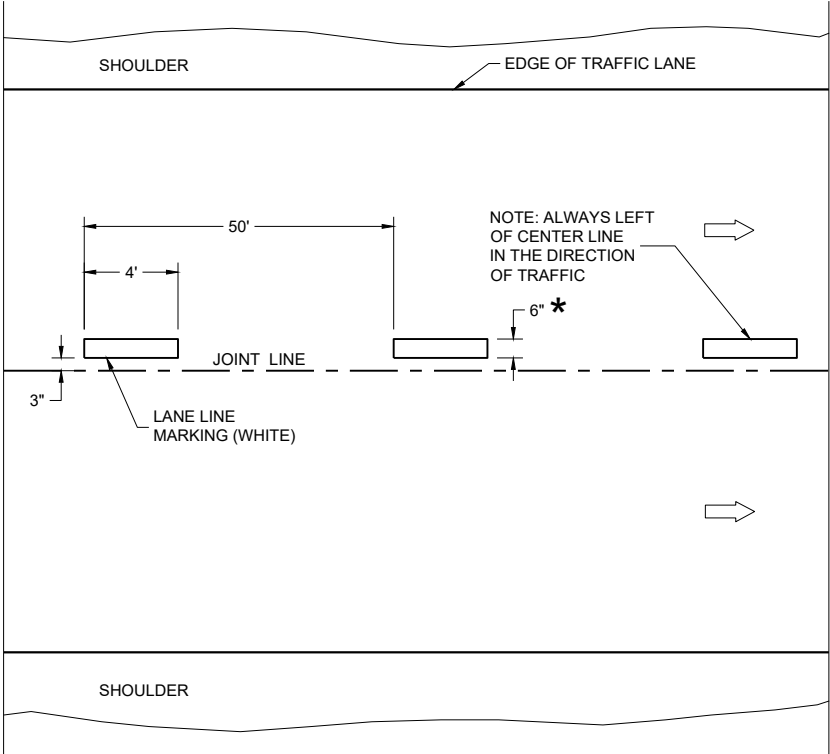
LEGEND

DIRECTION OF TRAFFIC

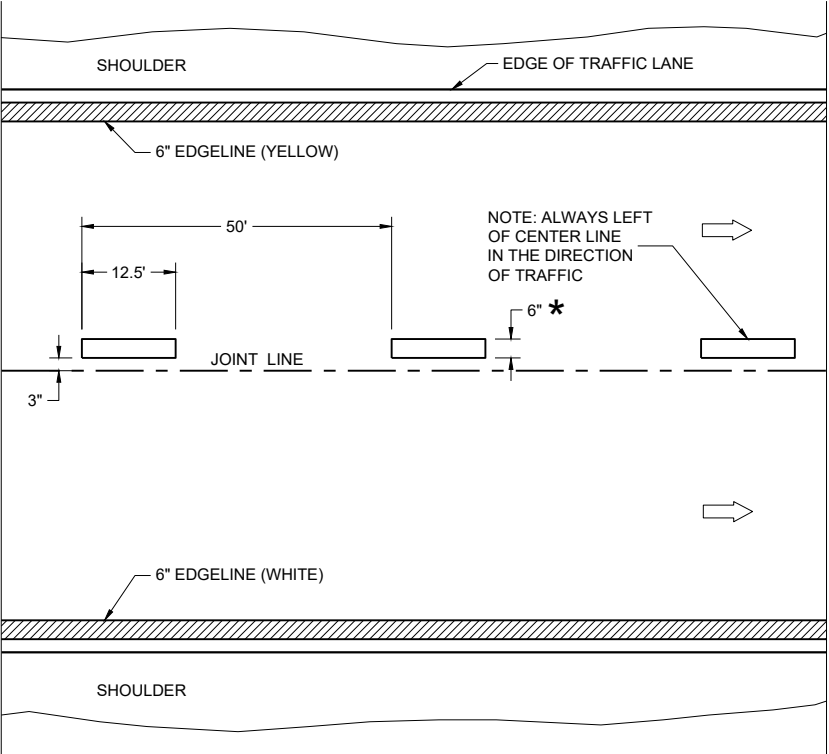
* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



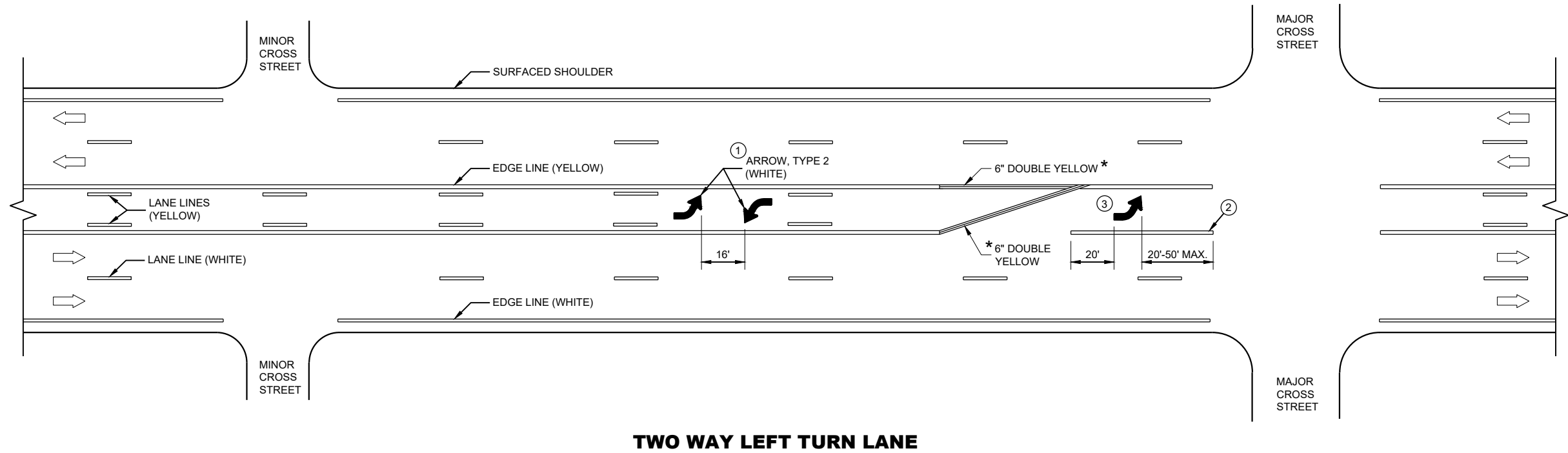
ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Jeannie Silver Statewide Pavement Marking Engineer
FHWA	



GENERAL NOTES

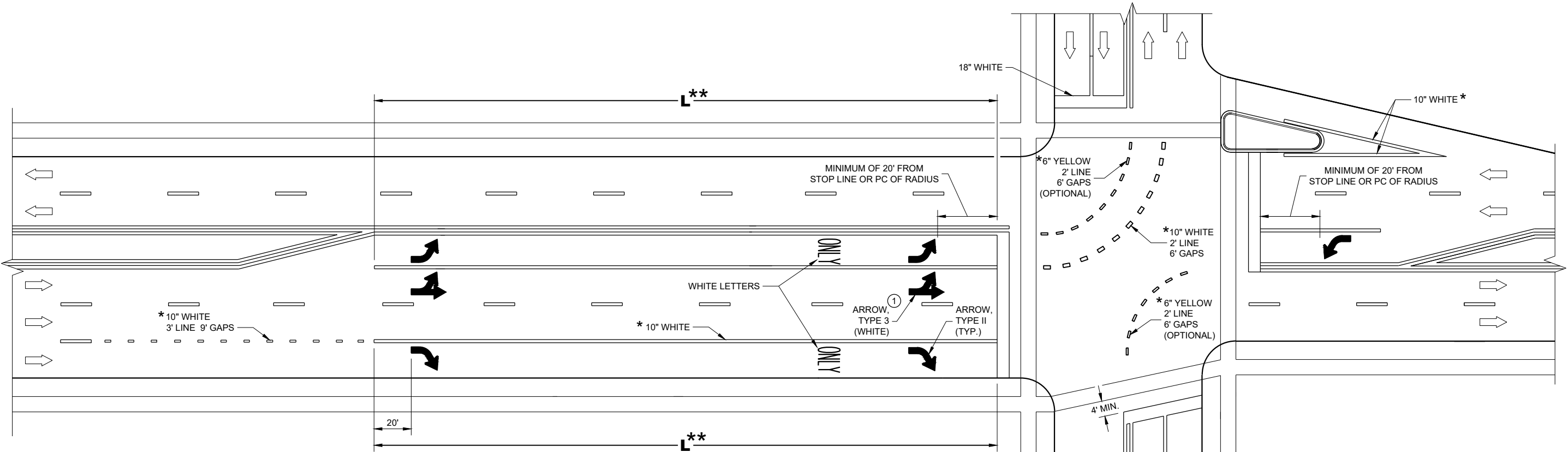
- ① A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ② 10" WHITE
- ③ TURN BAY LENGTH OF LESS THAN 48' DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT.

➡ DIRECTION OF TRAFFIC

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

**PAVEMENT MARKING
(TURN LANES)**

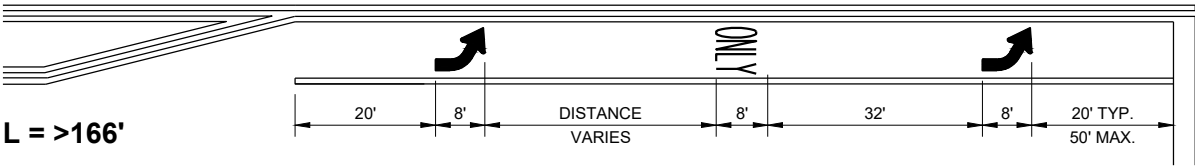
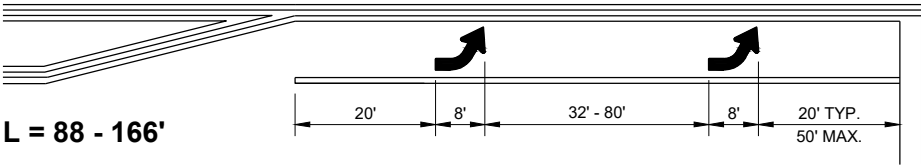
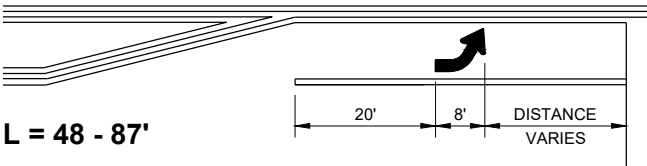
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



** (SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

TURN LANE OPTIONS

LENGTH OF TURN BAY (L) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



GENERAL NOTES

① QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

➡ DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

PAVEMENT MARKING
(TURN LANES)

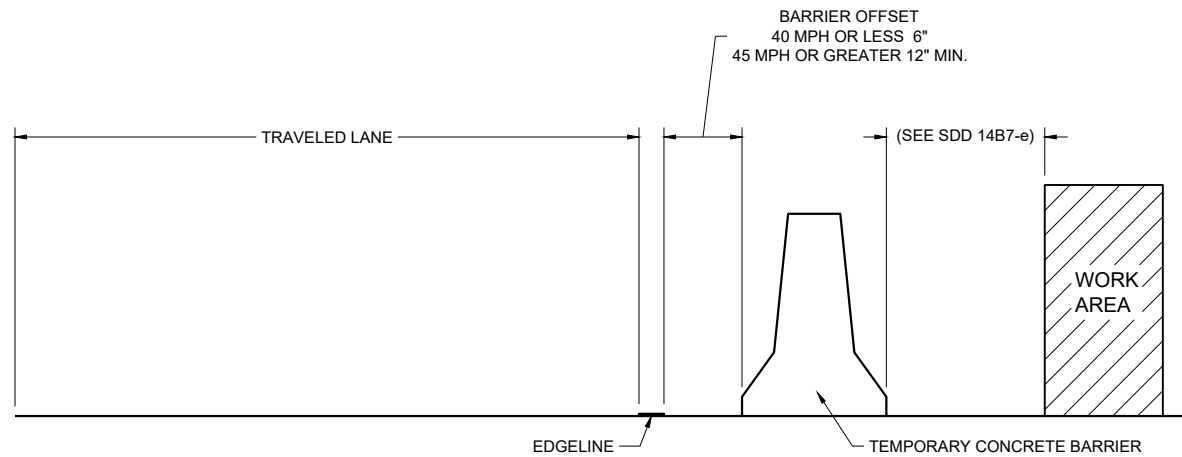
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PERMANENT PAVEMENT MARKING

APPROVED
December 2024
DATE

/S/ Jeannie Silver
Statewide Pavement Marking Engineer



TEMPORARY BARRIER OFFSET FROM EDGE LINE

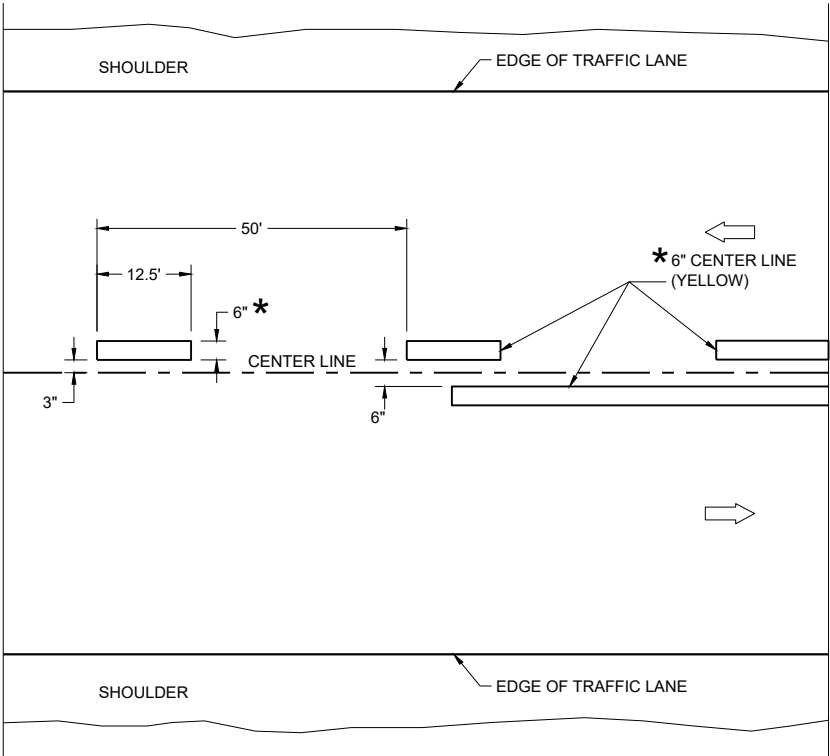
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

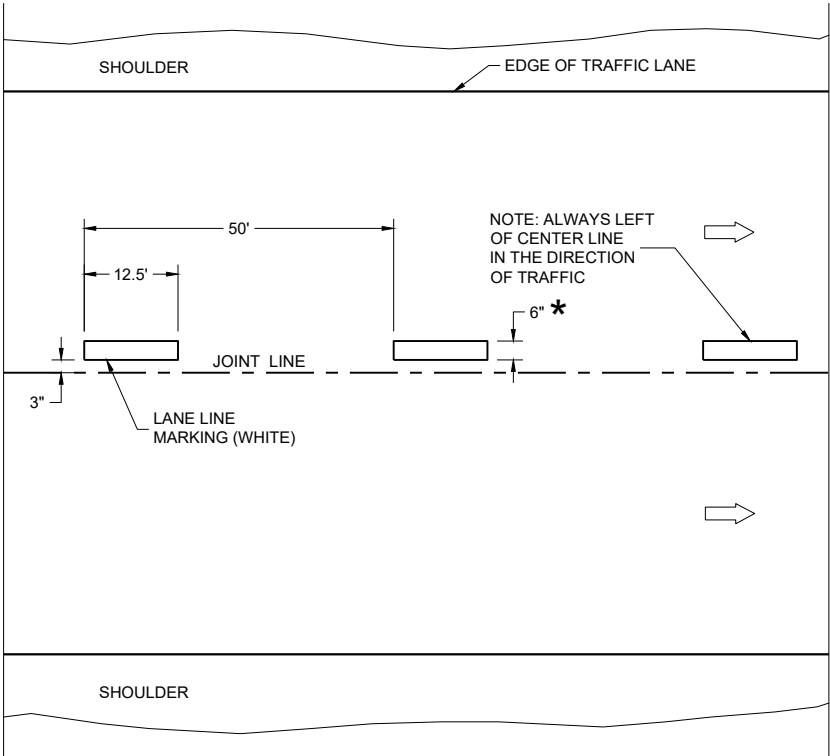
LEGEND

DIRECTION OF TRAFFIC

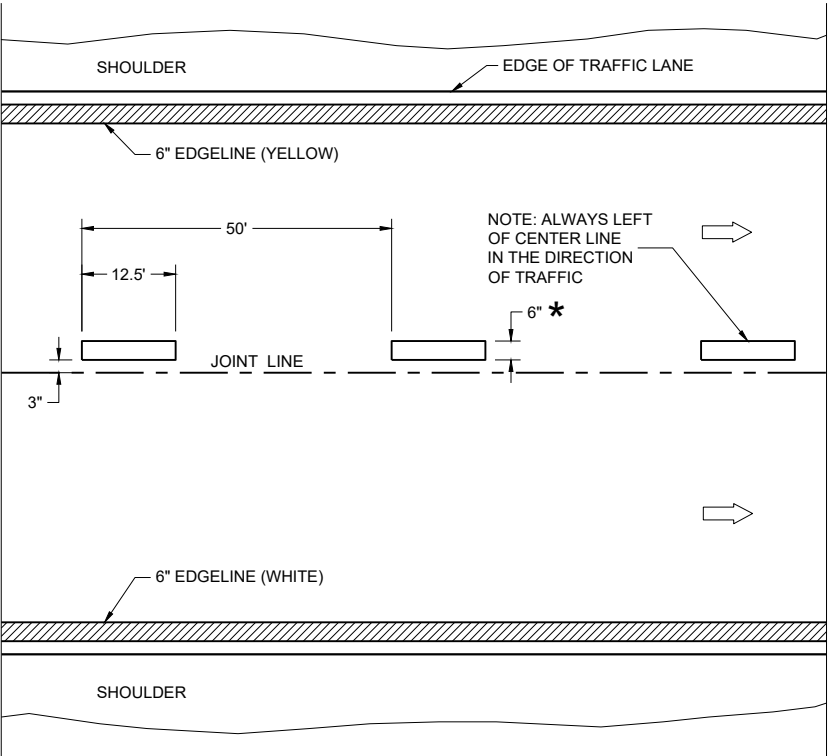
* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



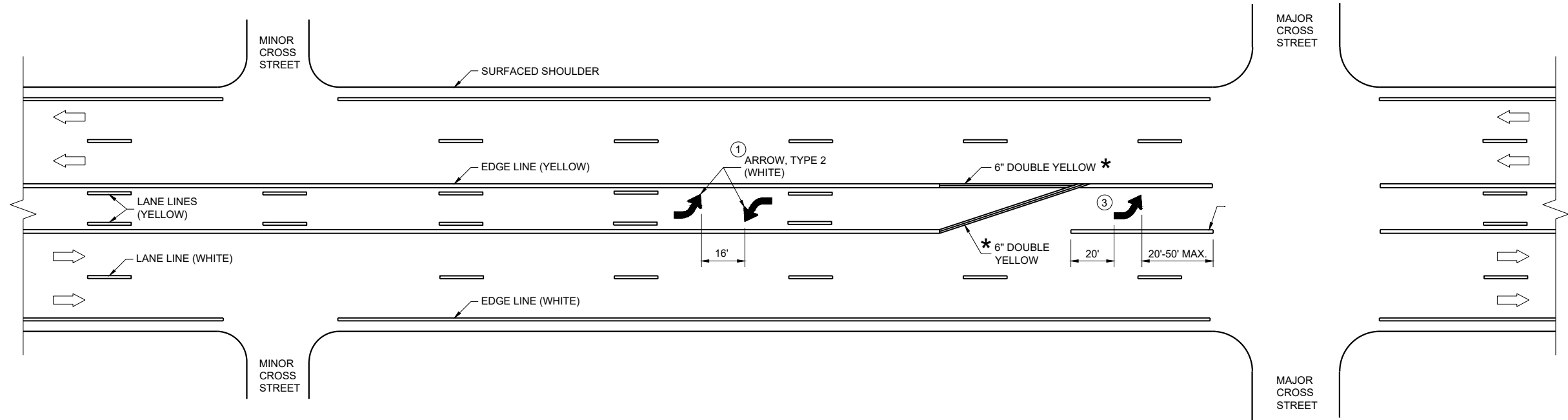
ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED December 2024 DATE	/S/ Jeannie Silver Statewide Pavement Marking Engineer
FHWA	



TWO WAY LEFT TURN LANE

GENERAL NOTES

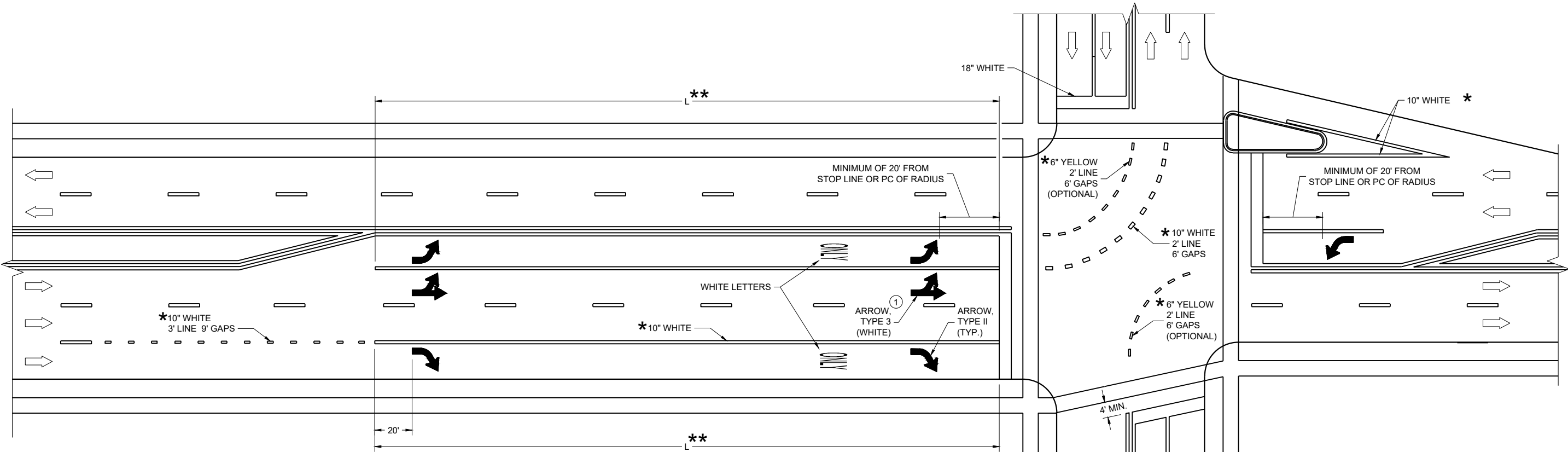
- ① A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ② 10" WHITE
- ③ TURN BAY LENGTH OF LESS THAN 48' DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT.

➡ DIRECTION OF TRAFFIC

*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

PAVEMENT MARKING
(TURN LANES)

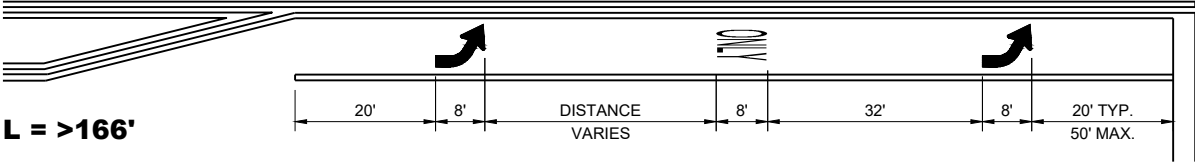
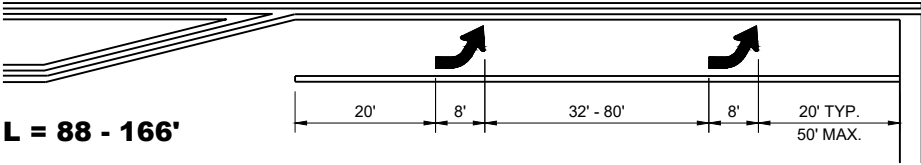
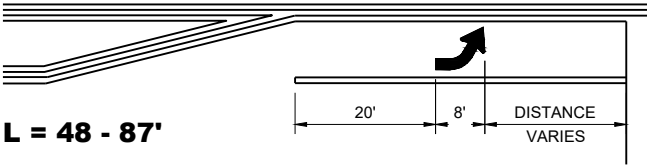
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



** (SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

TURN LANE OPTIONS

LENGTH OF TURN BAY (**L**) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



GENERAL NOTES

① QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

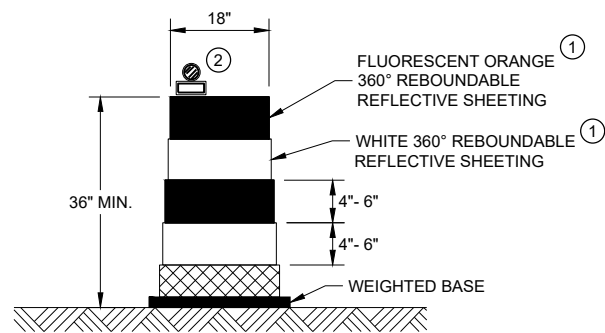
➡ DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

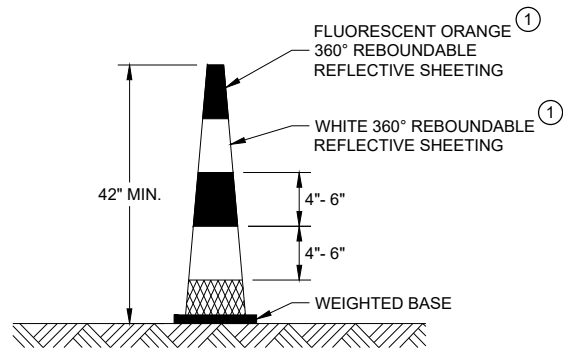
PAVEMENT MARKING
(TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



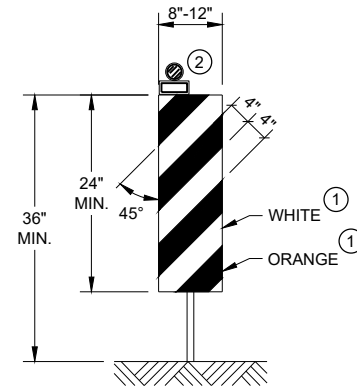
DRUM

BALLAST WIDTHS
RANGE FROM 24"-36"



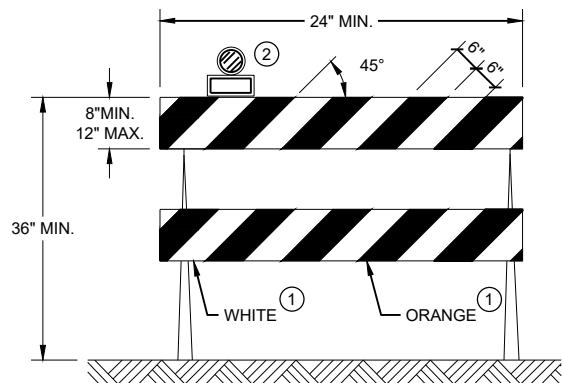
42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS
BALLAST WIDTHS
RANGE FROM 14"-20"



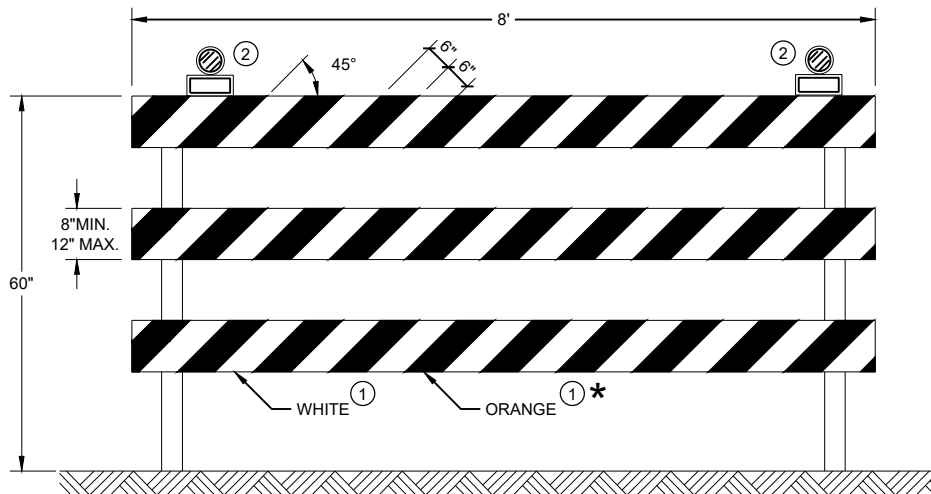
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.






**CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

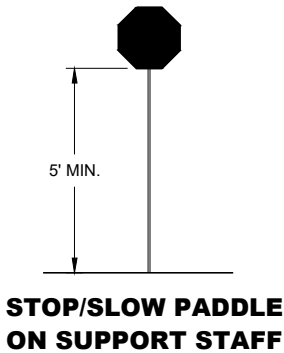
FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

- UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER
- ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.
- INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.
- PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.
- DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

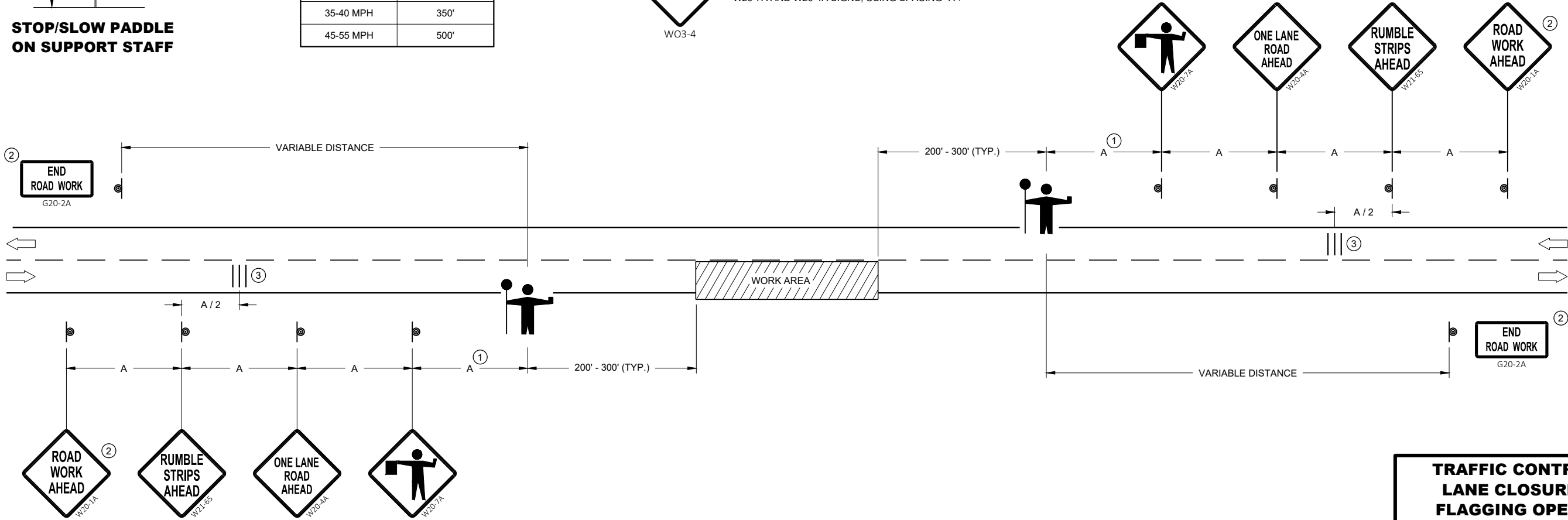


SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

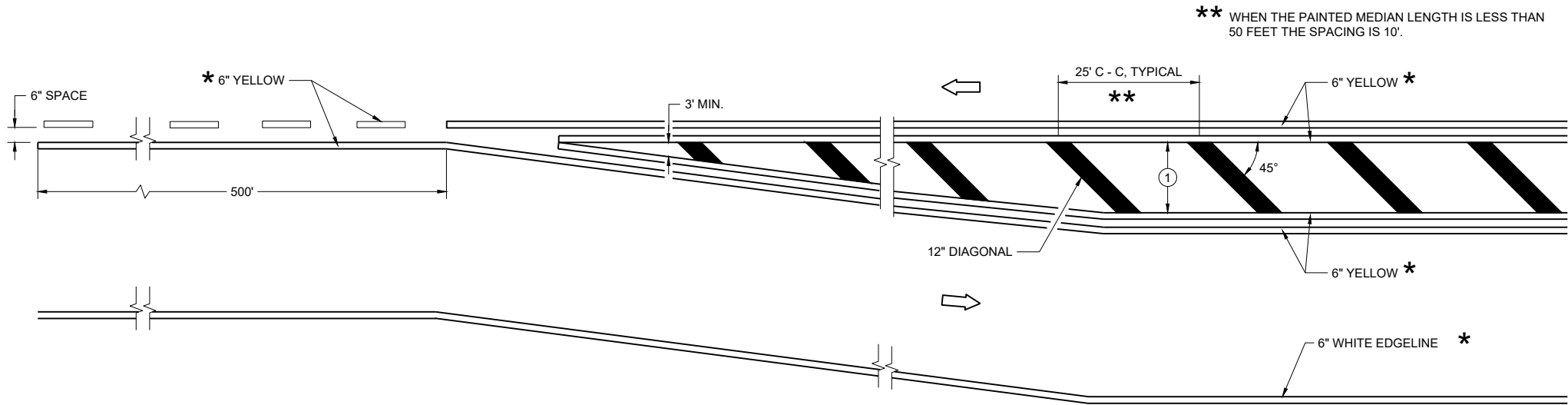
SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



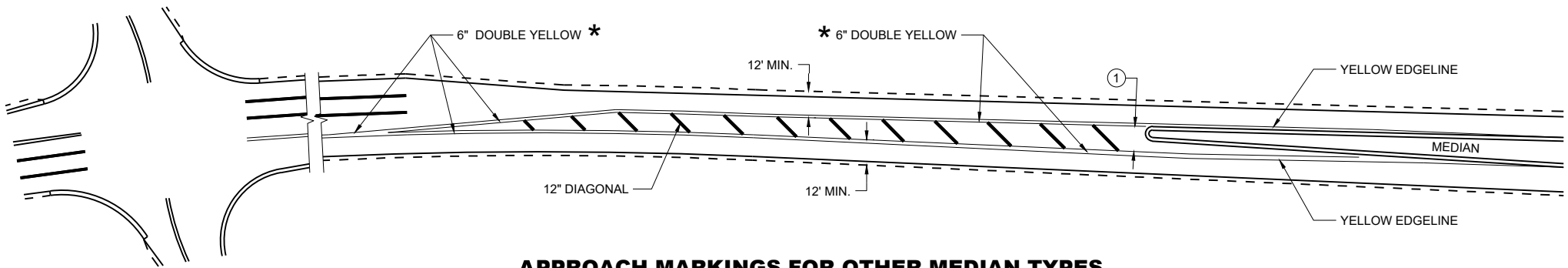
USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



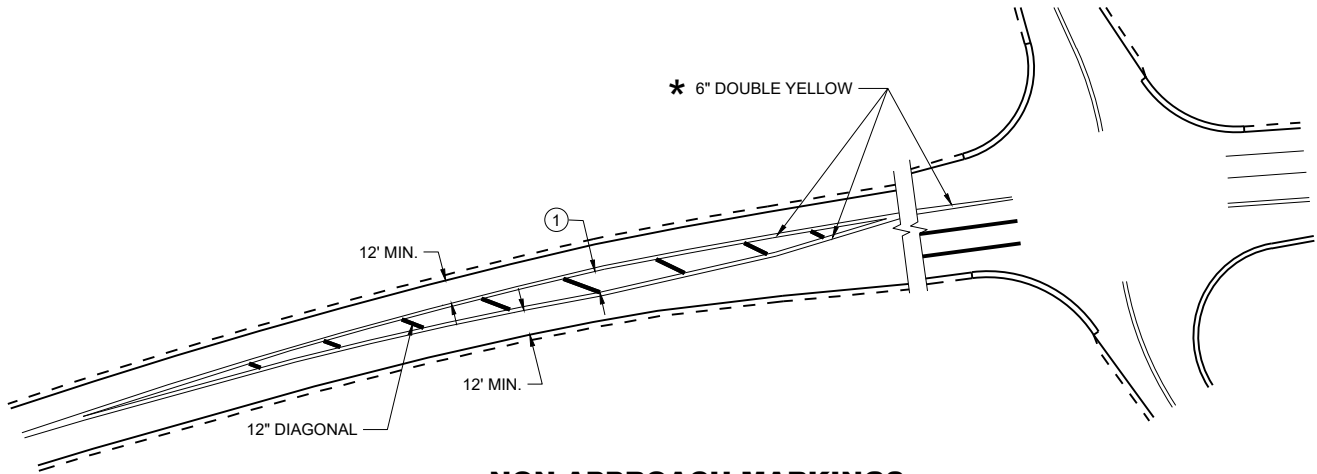
TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



MEDIAN ISLAND DETAIL



APPROACH MARKINGS FOR OTHER MEDIAN TYPES



NON-APPROACH MARKINGS

GENERAL NOTES

- ① DIAGONALS ARE OPTIONAL WHEN PAINTED ISLAND IS LESS THAN 6 FEET AT THE WIDEST POINT. OMIT DIAGONALS IF WIDTH IS LESS THAN 4 FEET.

DIRECTION OF TRAVEL

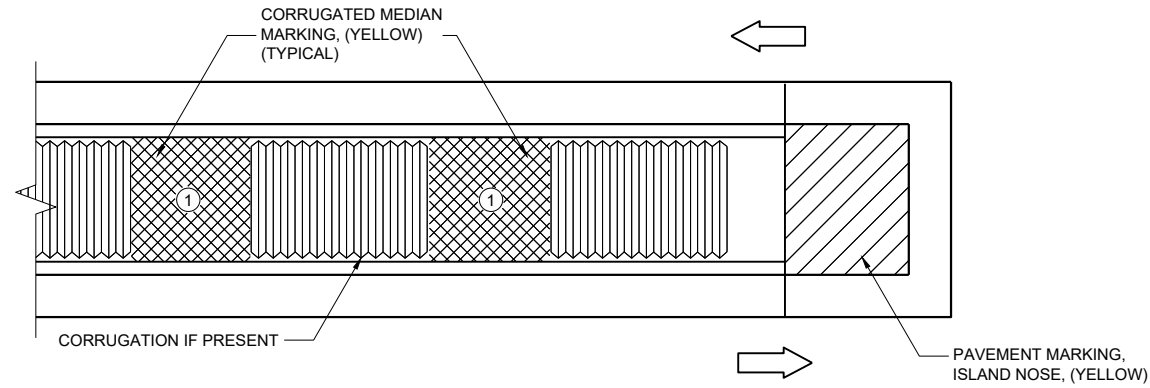
* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

MEDIAN ISLAND
PAVEMENT MARKINGS

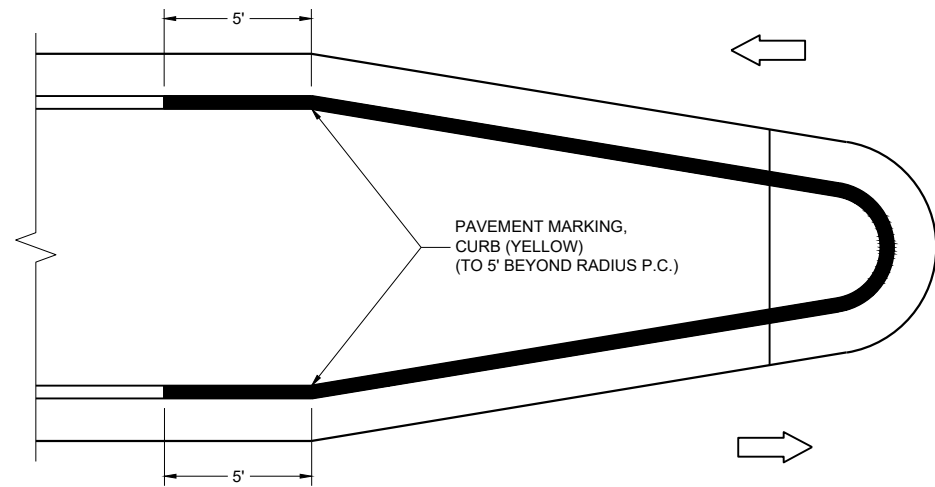
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
August 2024 /S/ Jeannie Silver
DATE Statewide Pavement Marking Engineer

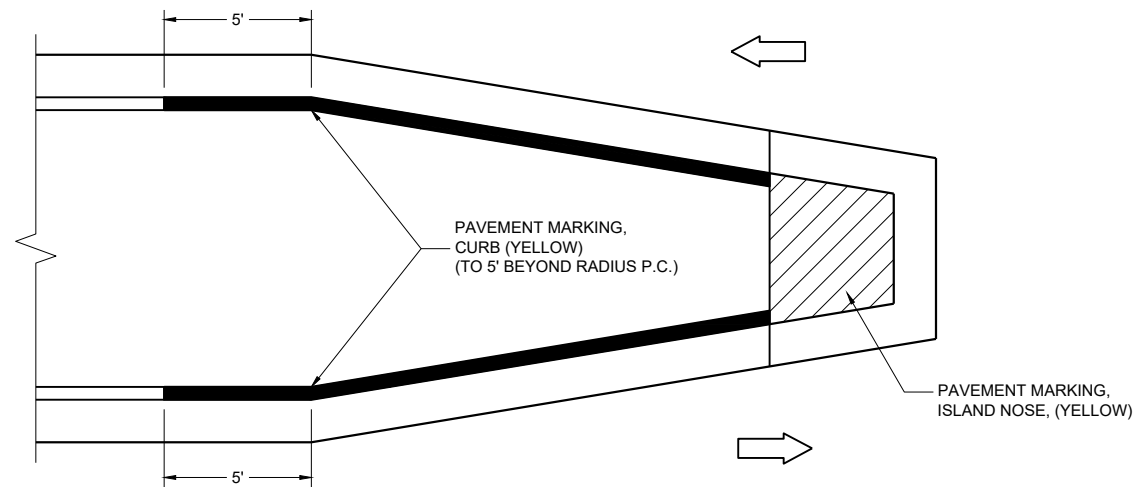
FHWA



MEDIAN ISLAND WITH SQUARE BLUNT NOSE



MEDIAN ISLAND WITH ROUND BLUNT NOSE



MEDIAN ISLAND WITH SLOPED NOSE

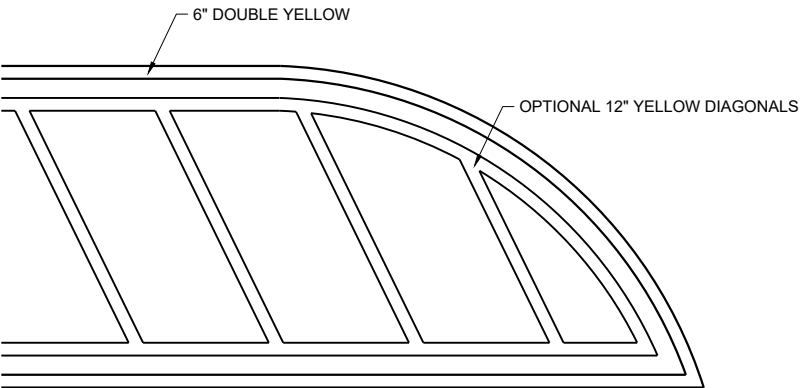
TYPICAL PLACEMENT OF
PAVEMENT MARKING ON MEDIAN ISLANDS

GENERAL NOTES

WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION, YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN. THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.

① APPLY PAVEMENT MARKING TO THE FLAT PORTION OF CORRUGATED MEDIAN.

- ISLAND NOSE MARKING
- CURB MARKING
- CORRUGATED MEDIAN MARKING
- DIRECTION OF TRAVEL



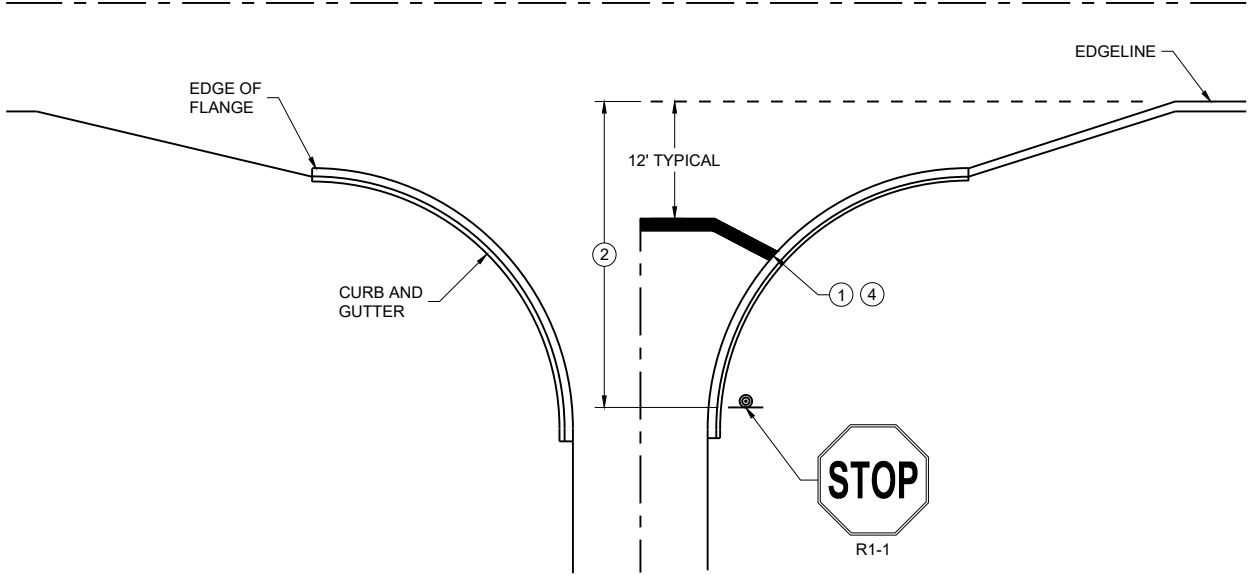
FLUSH MEDIAN ISLAND NOSE

PAVEMENT MARKINGS,
MEDIAN ISLAND NOSE

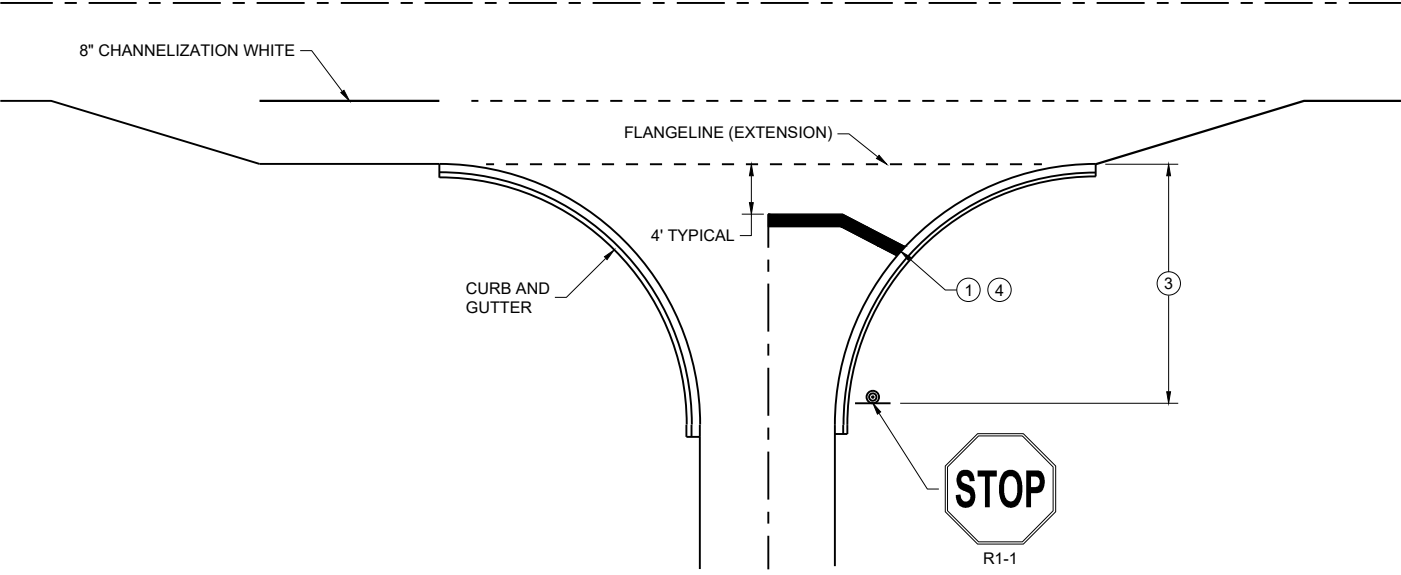
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
August 2024 /S/ Jeannie Silver
DATE Statewide Pavement Marking Engineer

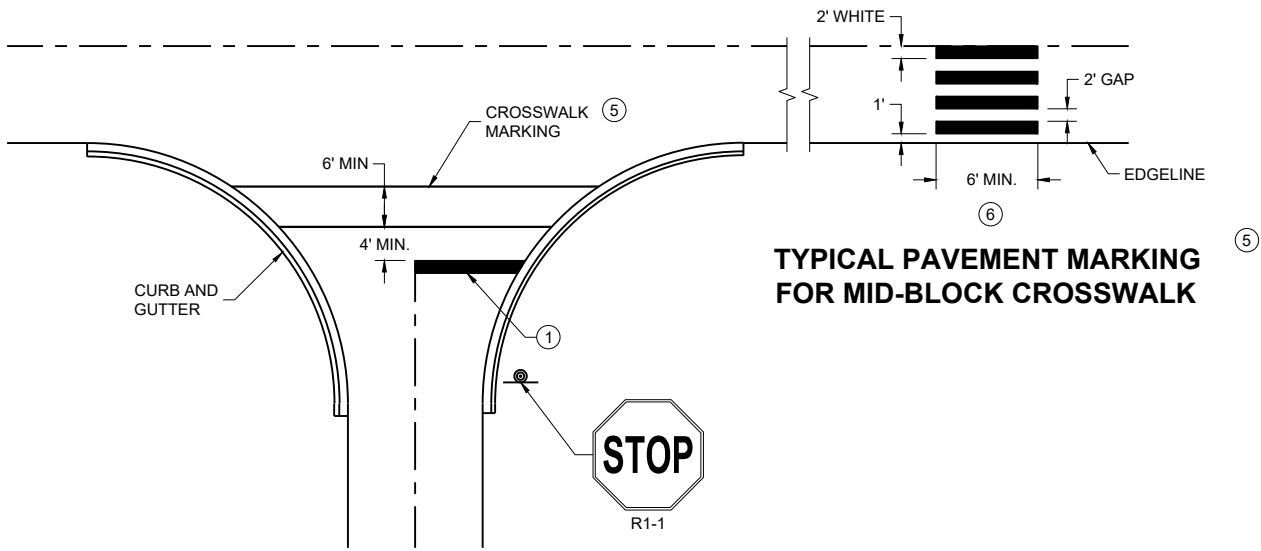
FHWA



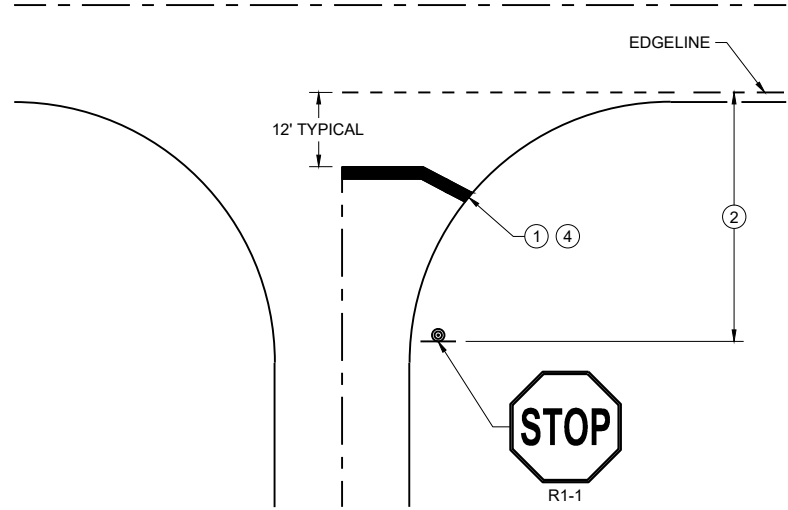
TYPICAL STOP LINE PAVEMENT MARKING
WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDE ROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR
SIDE ROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING
WITHOUT CURB AND GUTTER

GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGE LINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES.
- ⑥ POSTED SPEED LIMITS OF 40 MPH OR GREATER USE A MINIMUM WIDTH OF 8' FOR MIDBLOCK CROSSWALKS

STOP LINE AND CROSSWALK
PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2024 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER
FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

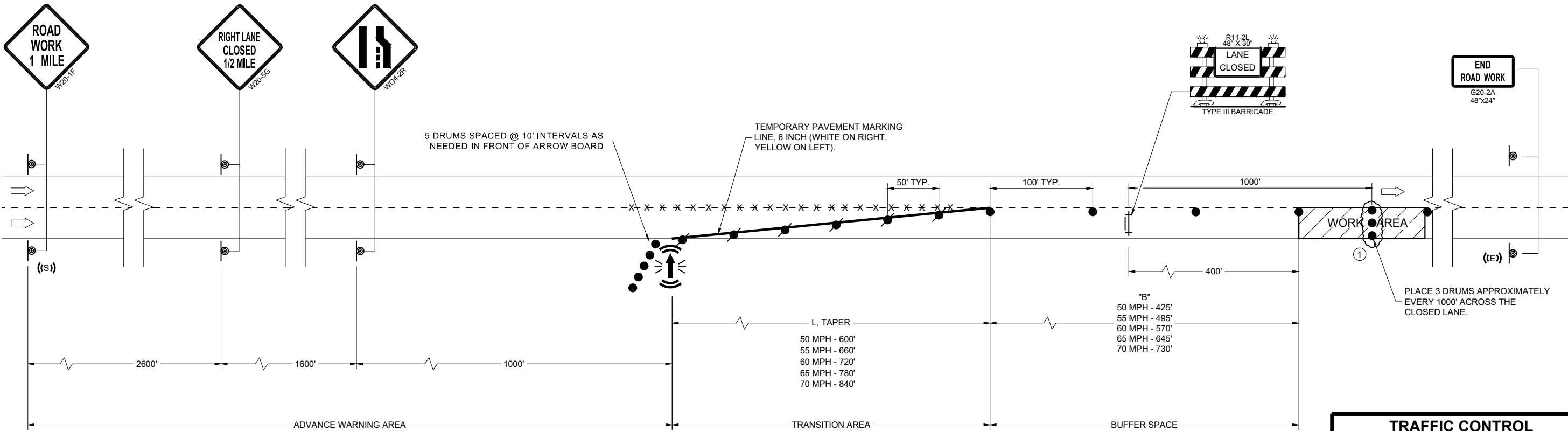
ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

① DRUMS IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKINGS
- DIRECTION OF TRAFFIC
- WORK AREA
- CONNECTED ARROW BOARD
- WZ START LOCATION MARKER
- WZ END LOCATION MARKER

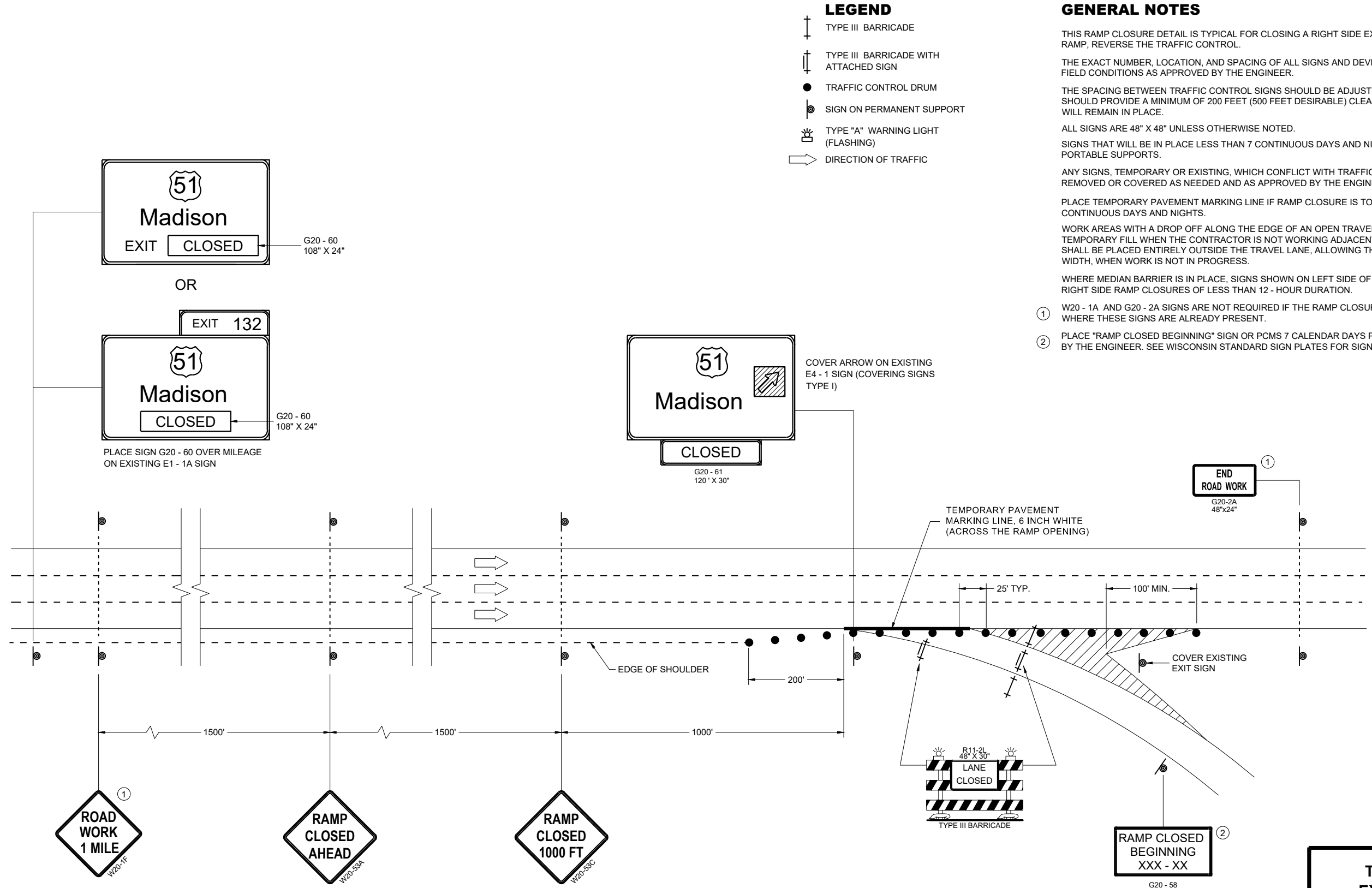


TRAFFIC CONTROL
LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2025 /S/ Andrew Heidtke
DATE ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

FHWA



LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC

GENERAL NOTES

THIS RAMP CLOSURE DETAIL IS TYPICAL FOR CLOSING A RIGHT SIDE EXIT RAMP. FOR A LEFT SIDE EXIT RAMP, REVERSE THE TRAFFIC CONTROL.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS, TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

PLACE TEMPORARY PAVEMENT MARKING LINE IF RAMP CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WORK AREAS WITH A DROP OFF ALONG THE EDGE OF AN OPEN TRAVEL LANE SHALL BE LEVELED WITH TEMPORARY FILL WHEN THE CONTRACTOR IS NOT WORKING ADJACENT TO THE TRAVEL LANE. DRUMS SHALL BE PLACED ENTIRELY OUTSIDE THE TRAVEL LANE, ALLOWING THE FULL UNOBSTRUCTED LANE WIDTH, WHEN WORK IS NOT IN PROGRESS.

WHERE MEDIAN BARRIER IS IN PLACE, SIGNS SHOWN ON LEFT SIDE OF ROADWAY MAY BE OMITTED FOR RIGHT SIDE RAMP CLOSURES OF LESS THAN 12 - HOUR DURATION.

① W20 - 1A AND G20 - 2A SIGNS ARE NOT REQUIRED IF THE RAMP CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

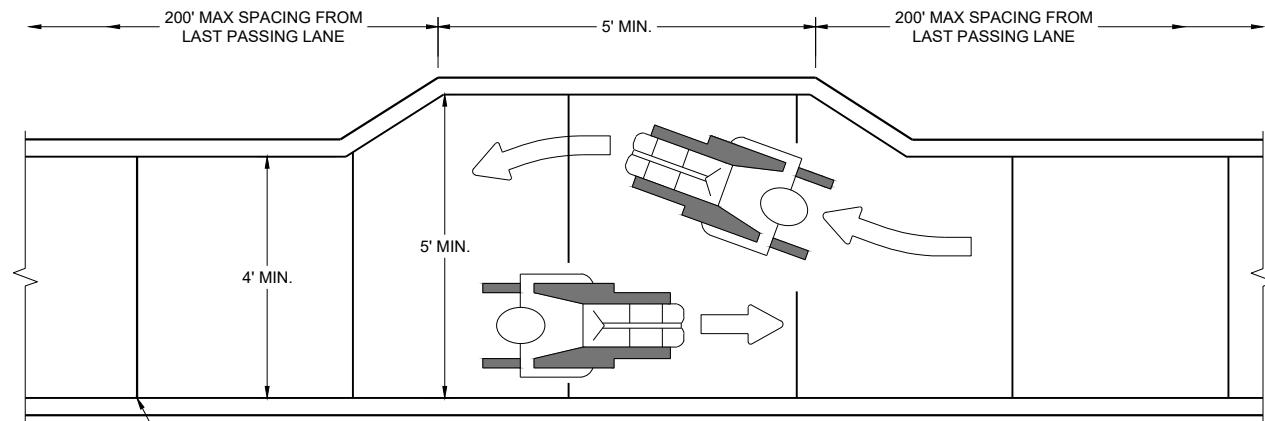
② PLACE "RAMP CLOSED BEGINNING" SIGN OR PCMS 7 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR SIGN LAYOUT.

**TRAFFIC CONTROL,
EXIT RAMP CLOSURE**

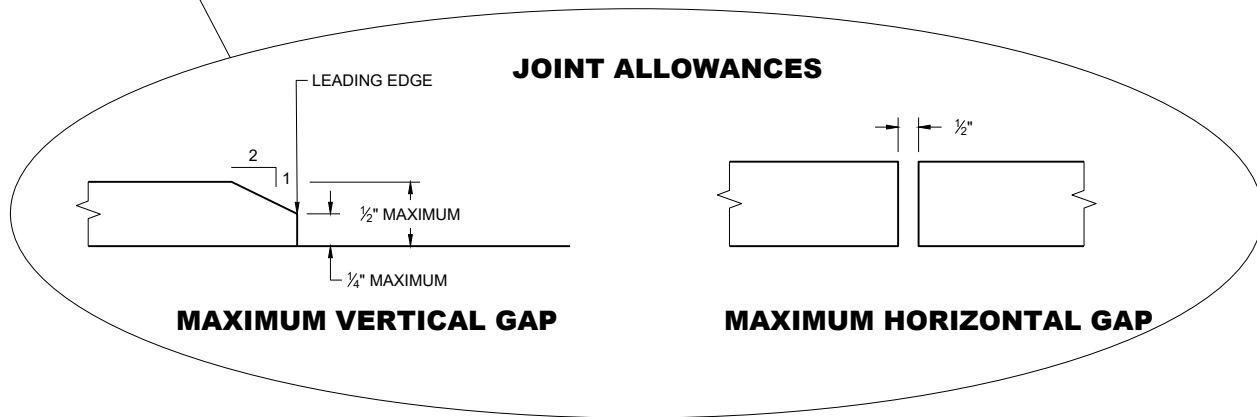
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2025 /S/ Andrew Heidtke
DATE <position>

FHWA

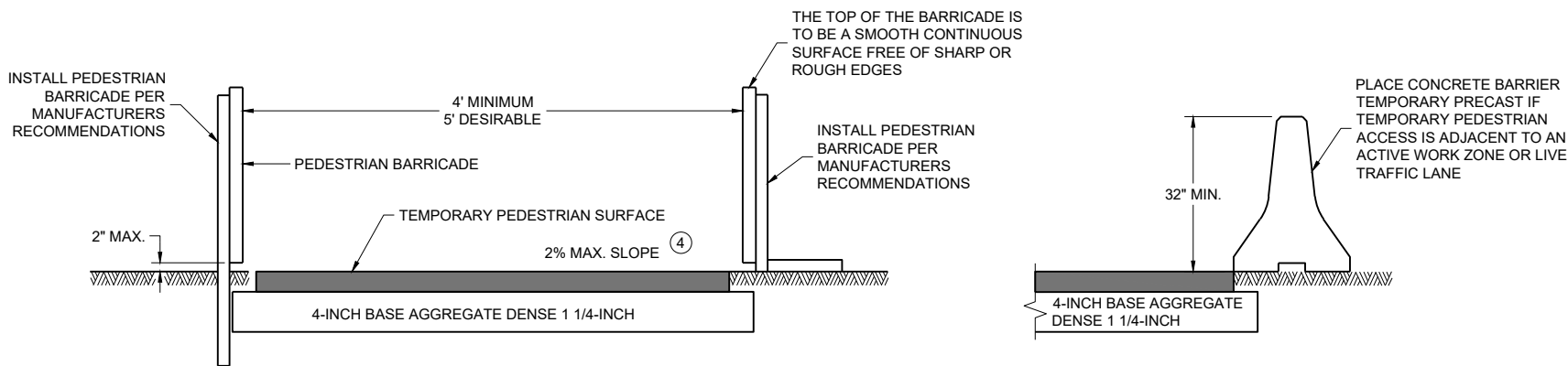


NARROW SIDEWALK PASSING DETAIL



MAXIMUM VERTICAL GAP

MAXIMUM HORIZONTAL GAP



TEMPORARY PEDESTRIAN ACCESS

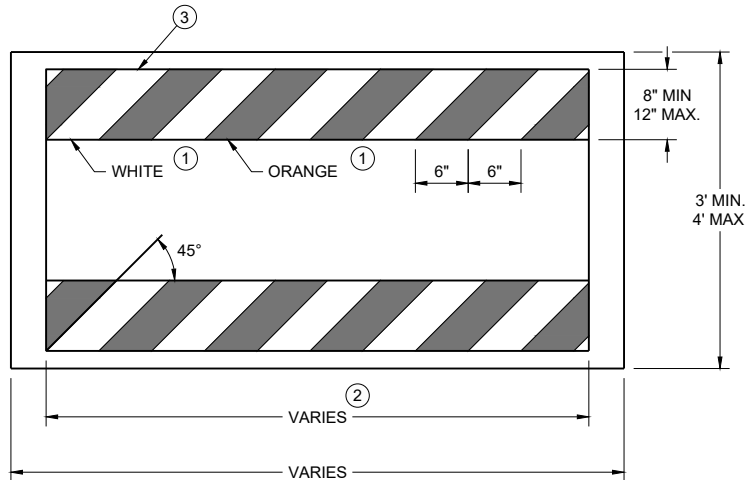
GENERAL NOTES

BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.

★ USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.

- ④ WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.



TEMPORARY PEDESTRIAN BARRICADE *

TRAFFIC CONTROL,
PEDESTRIAN
ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

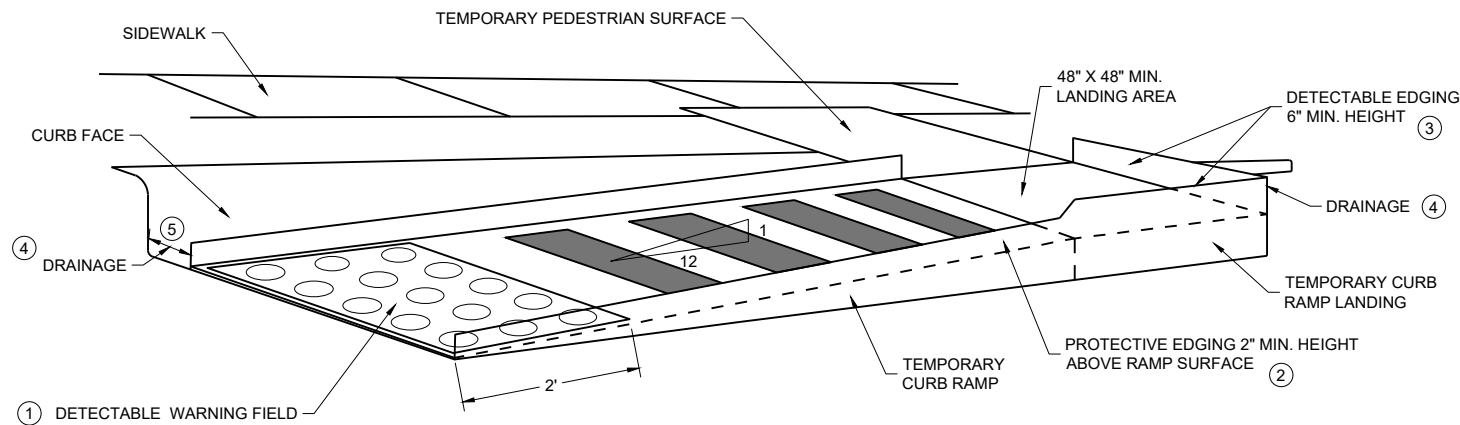
CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

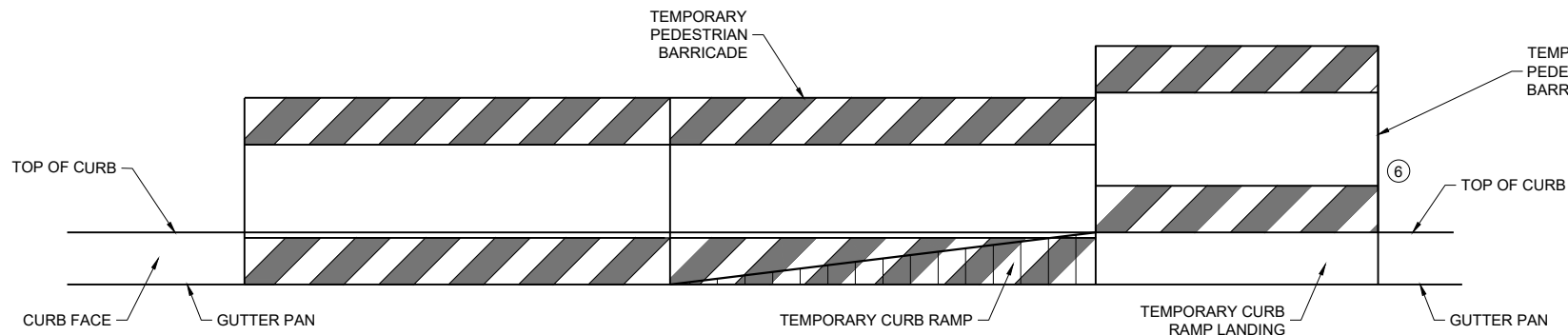
CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP. LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED $\frac{1}{2}$ ". LATERAL EDGES MAY BE VERTICAL UP TO $\frac{1}{4}$ " HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN $\frac{1}{4}$ " AND $\frac{1}{2}$ ".

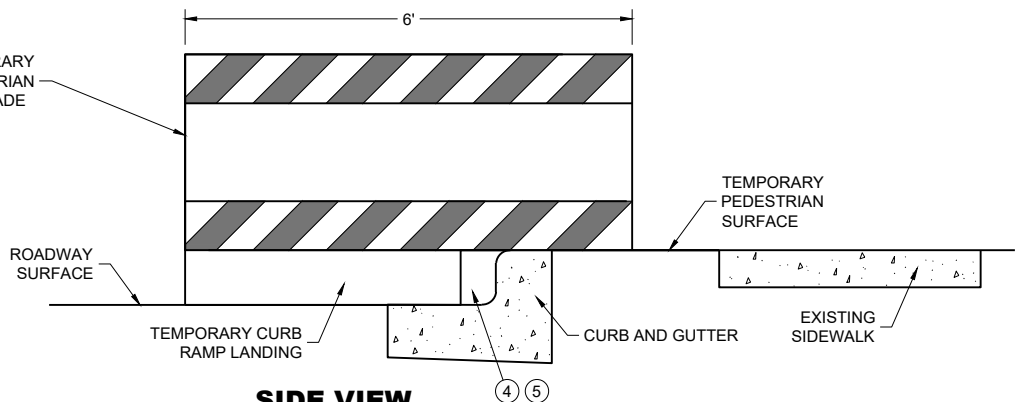
- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ ENSURE CURB RAMP IS OUT OF THE GUTTER PAN.
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



PERSPECTIVE VIEW



FRONT VIEW

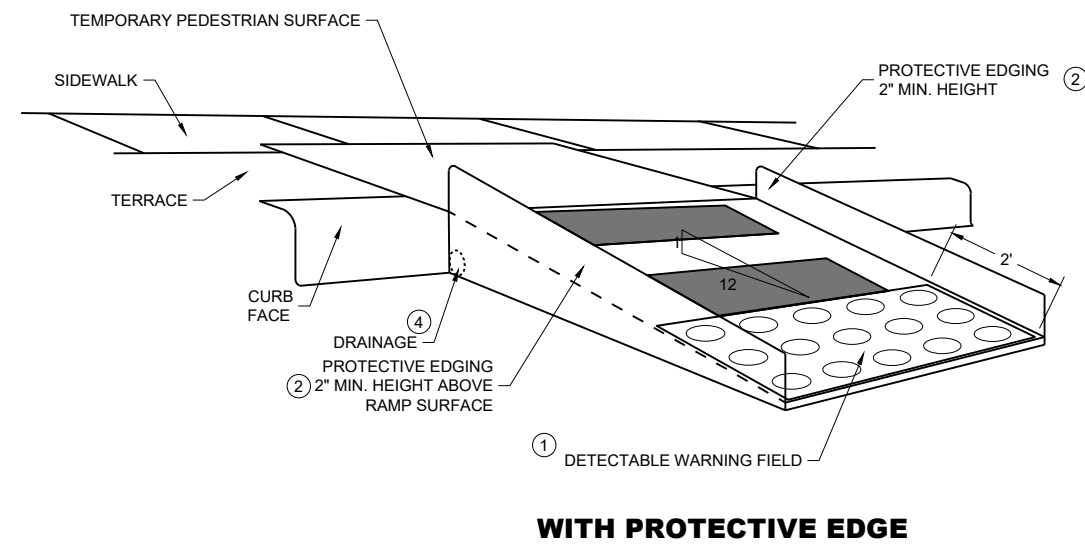


SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

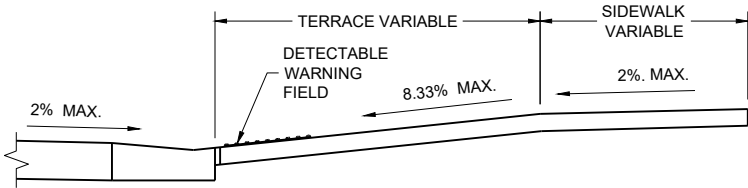
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

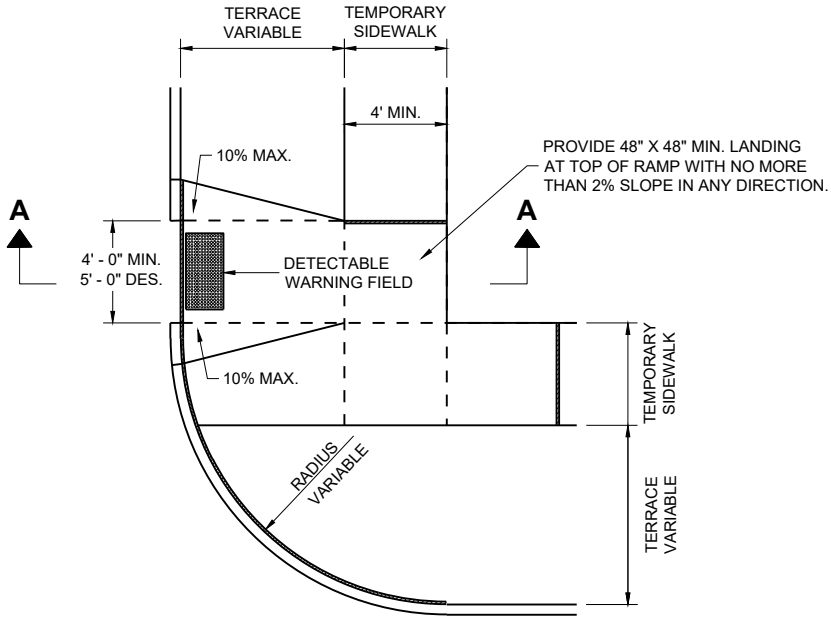


- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

GENERAL NOTES



SECTION A - A



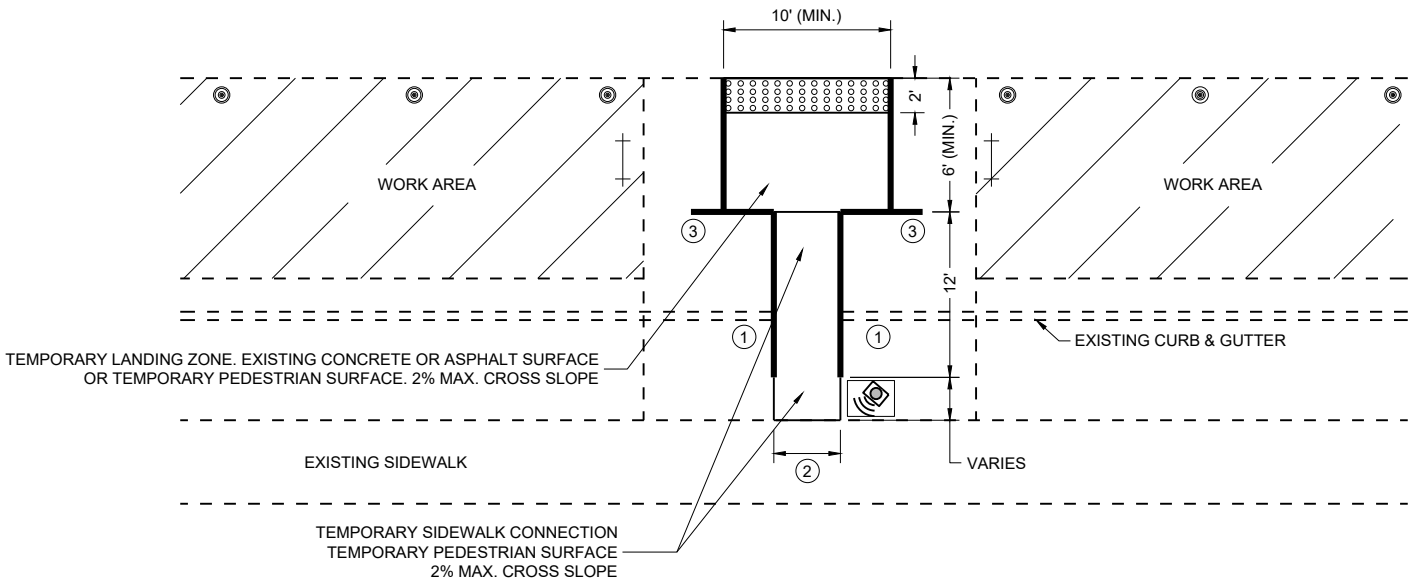
PLAN VIEW
TEMPORARY TYPE 3 RAMP
(OUTSIDE OF CROSSWALK AREA)

TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION

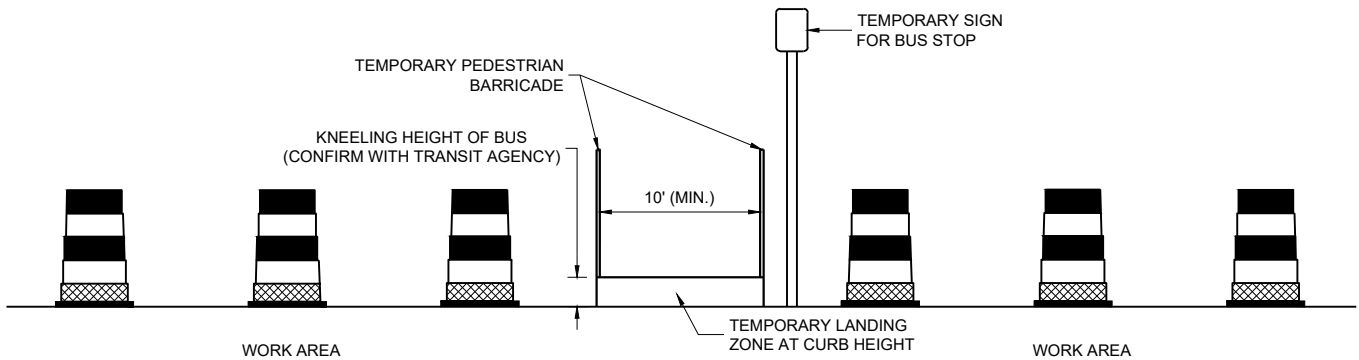
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May 2023 /S/ Andrew Heidtke
DATE <position>

FHWA



PLAN VIEW



PROFILE VIEW
TEMPORARY BUS STOP PAD

GENERAL NOTES








- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.
- PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN ½" WIDTH.
- CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED ½". LATERAL EDGES MAY BE VERTICAL UP TO ¼" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN ¼" AND ½".
- CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

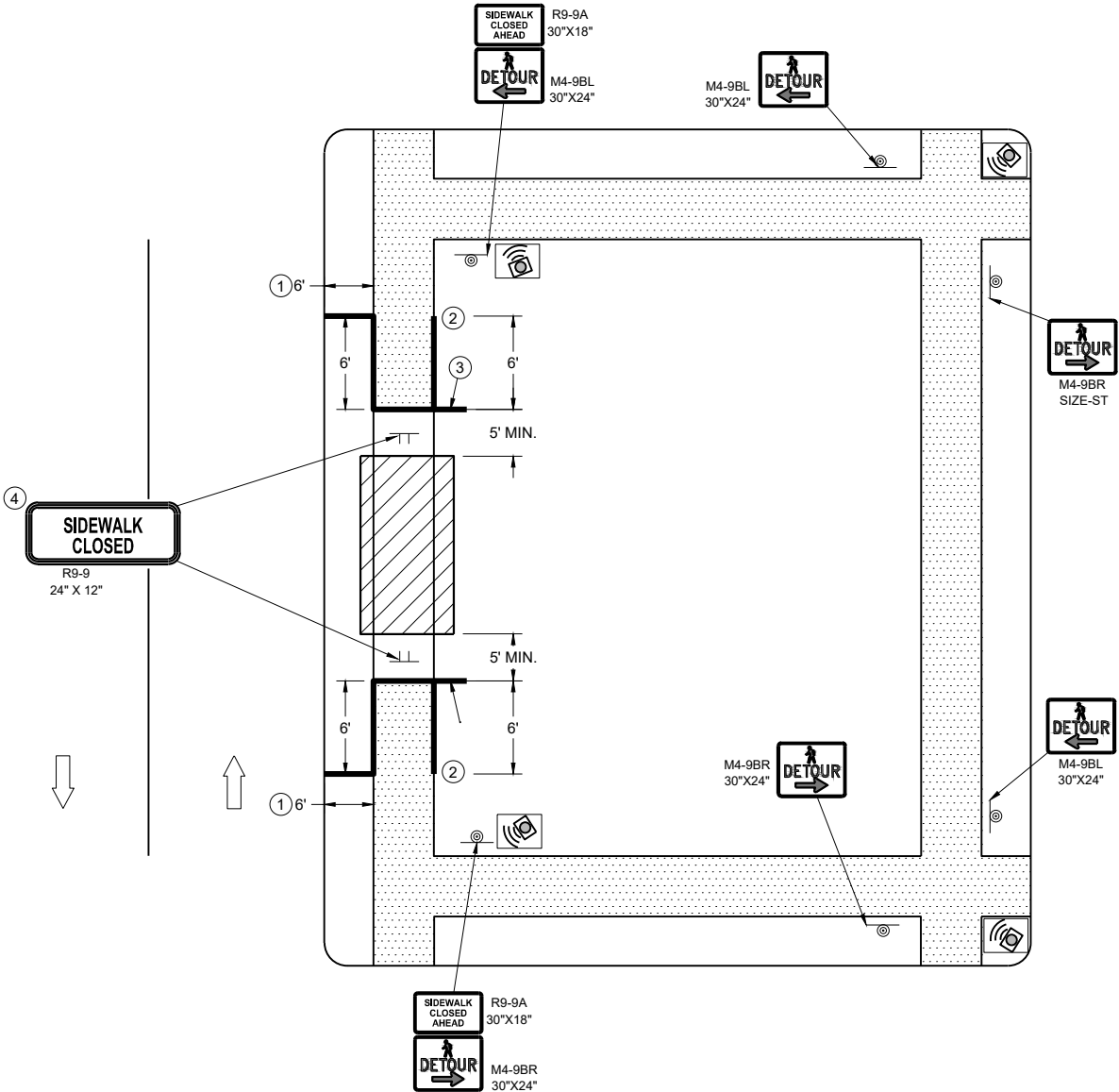
- ① DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ② 5' WIDE MIN. WITH TEMPORARY PEDESTRIAN BARRICADE, 10' WIDE MIN. WITHOUT TEMPORARY PEDESTRIAN BARRICADE.
- ③ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE INTO THIS SPACE.

LEGEND

- ⊙ TRAFFIC CONTROL DRUM
- ⊢ TYPE III BARRICADE
- TEMPORARY PEDESTRIAN BARRICADE
- ⊞ TEMPORARY DETECTABLE WARNING FIELD
- ▨ WORK AREA
- 🔊 TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC
-  TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)



SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

GENERAL NOTES

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICTS WITH DRIVEWAYS AND OTHER EXISTING FEATURES.

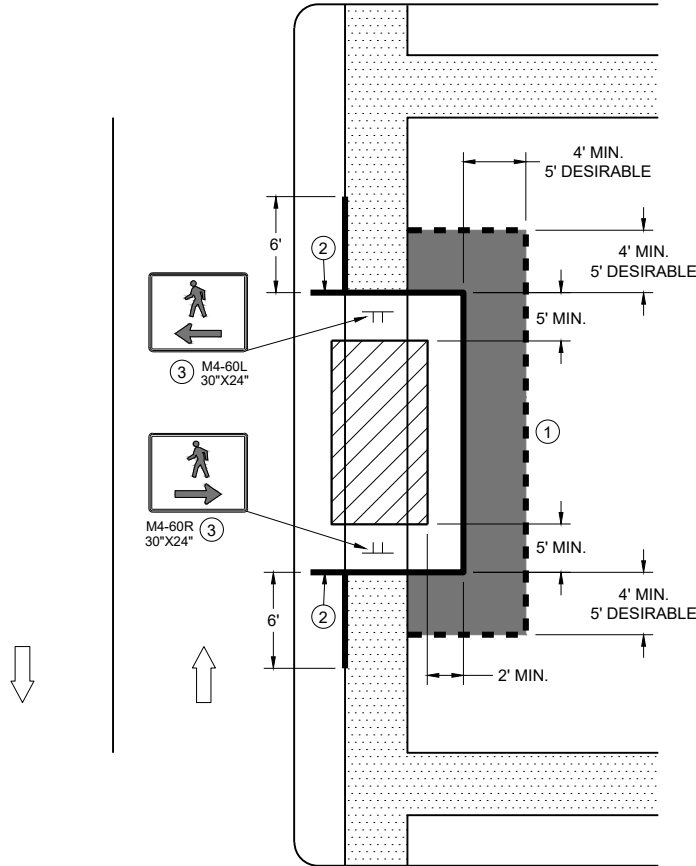
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- WORK AREA
- UNDER PEDESTRIAN TRAFFIC
- TEMPORARY PEDESTRIAN SURFACE
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC










GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- 1 USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
 - 2 IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - 3 MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK BYPASS
SINGLE SIDE

LEGEND

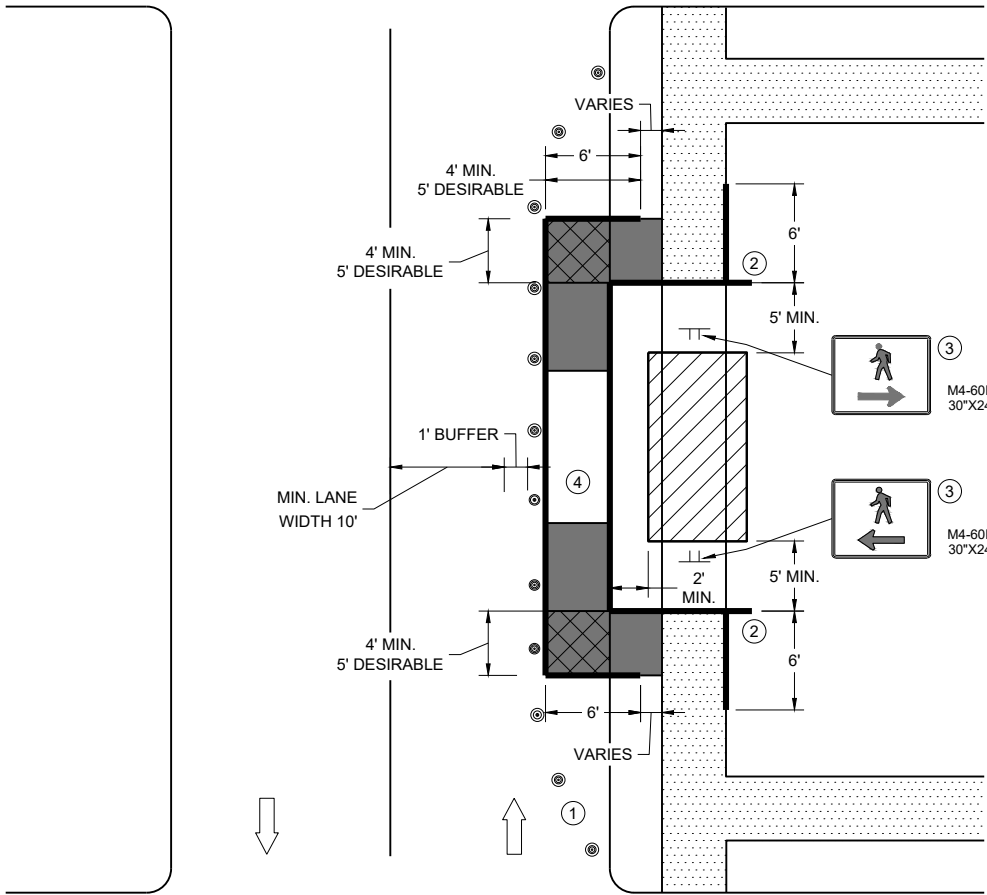
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- UNDER PEDESTRIAN TRAFFIC
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

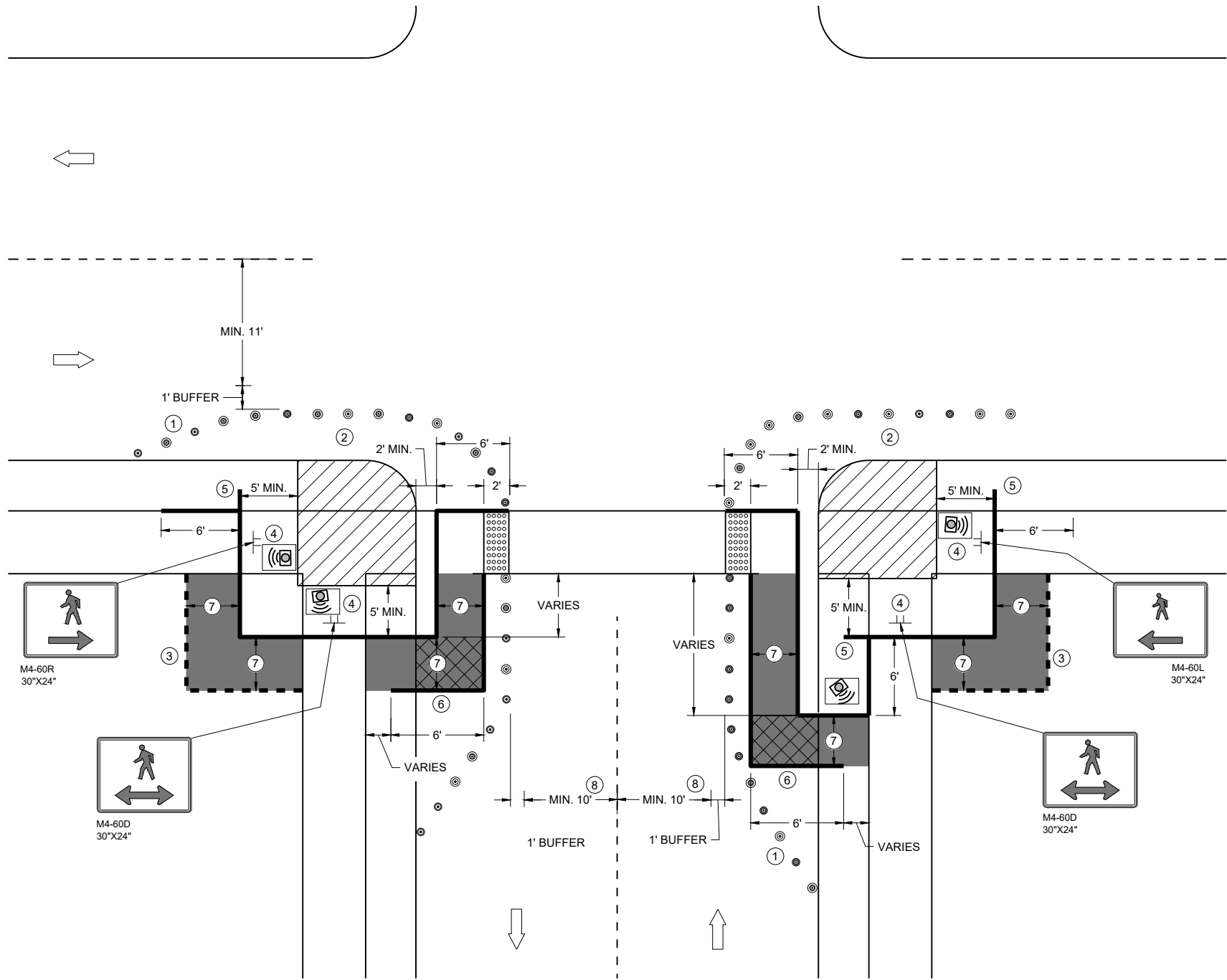
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
- ② PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
- ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
- ④ USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURFACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE. WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.



SIDEWALK BYPASS, SINGLE SIDE

TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



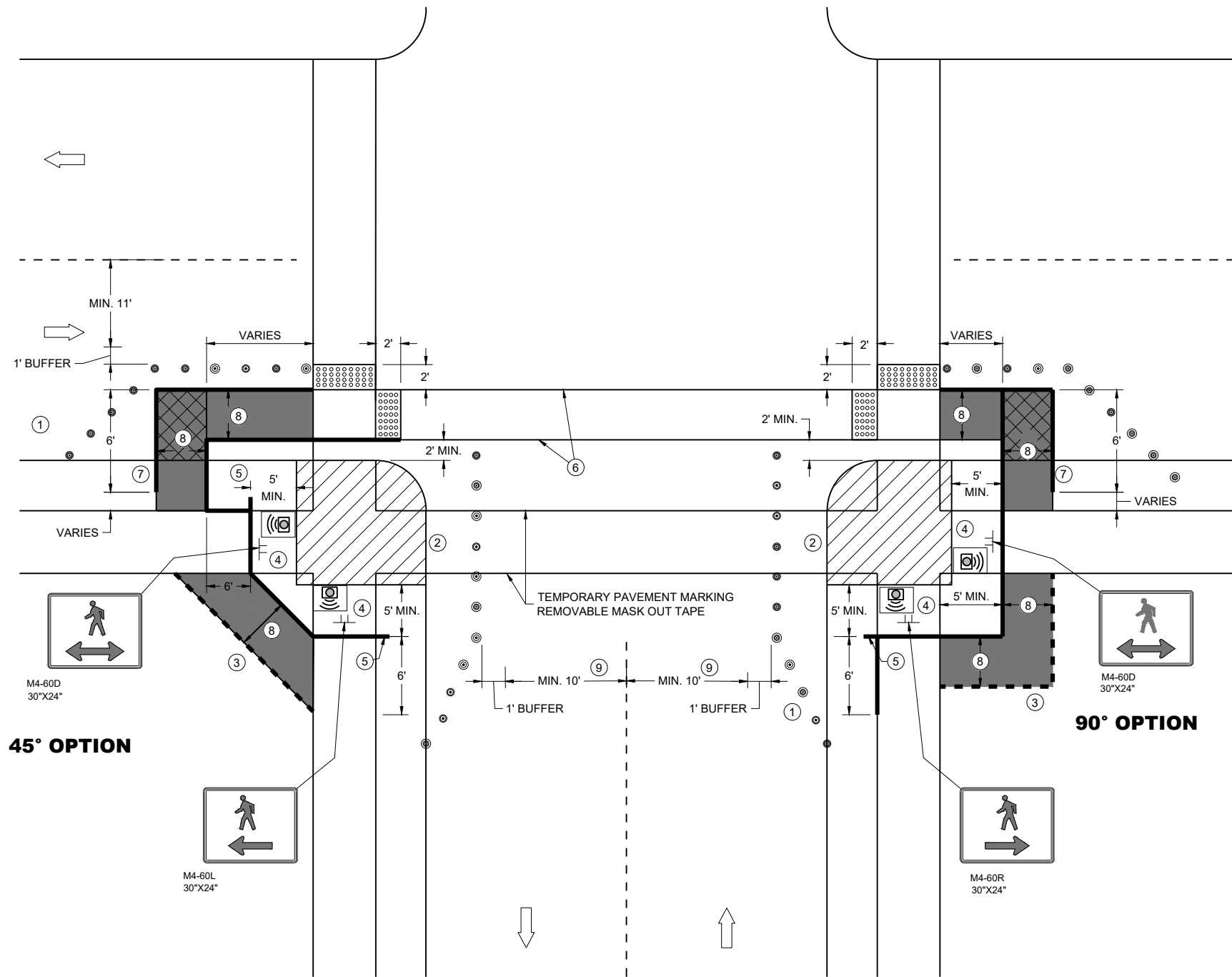
CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE

GENERAL NOTES

- IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.
- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG
- WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
 - ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
 - ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
 - ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
 - ⑥ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
 - ⑦ 4 FEET MINIMUM, 5 FEET DESIRABLE
 - ⑧ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC
- TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)



GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ WHITE 6" TEMPORARY PAVEMENT MARKING
- ⑦ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑧ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑨ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

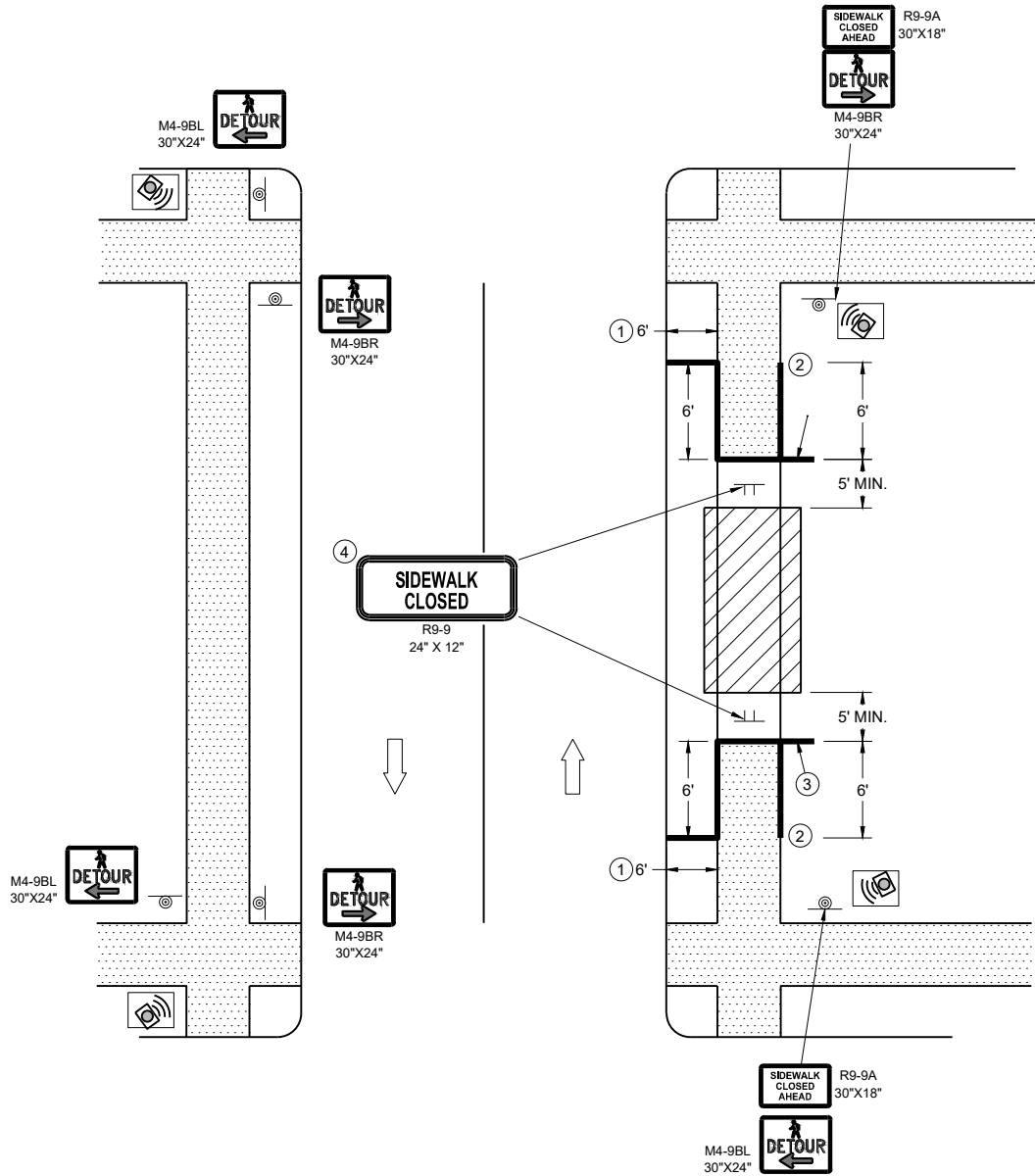
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC
- TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- UNDER PEDESTRIAN TRAFFIC
- WORK AREA
- TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC
- TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)



SIDEWALK DETOUR, SIDEWALK ON BOTH SIDES

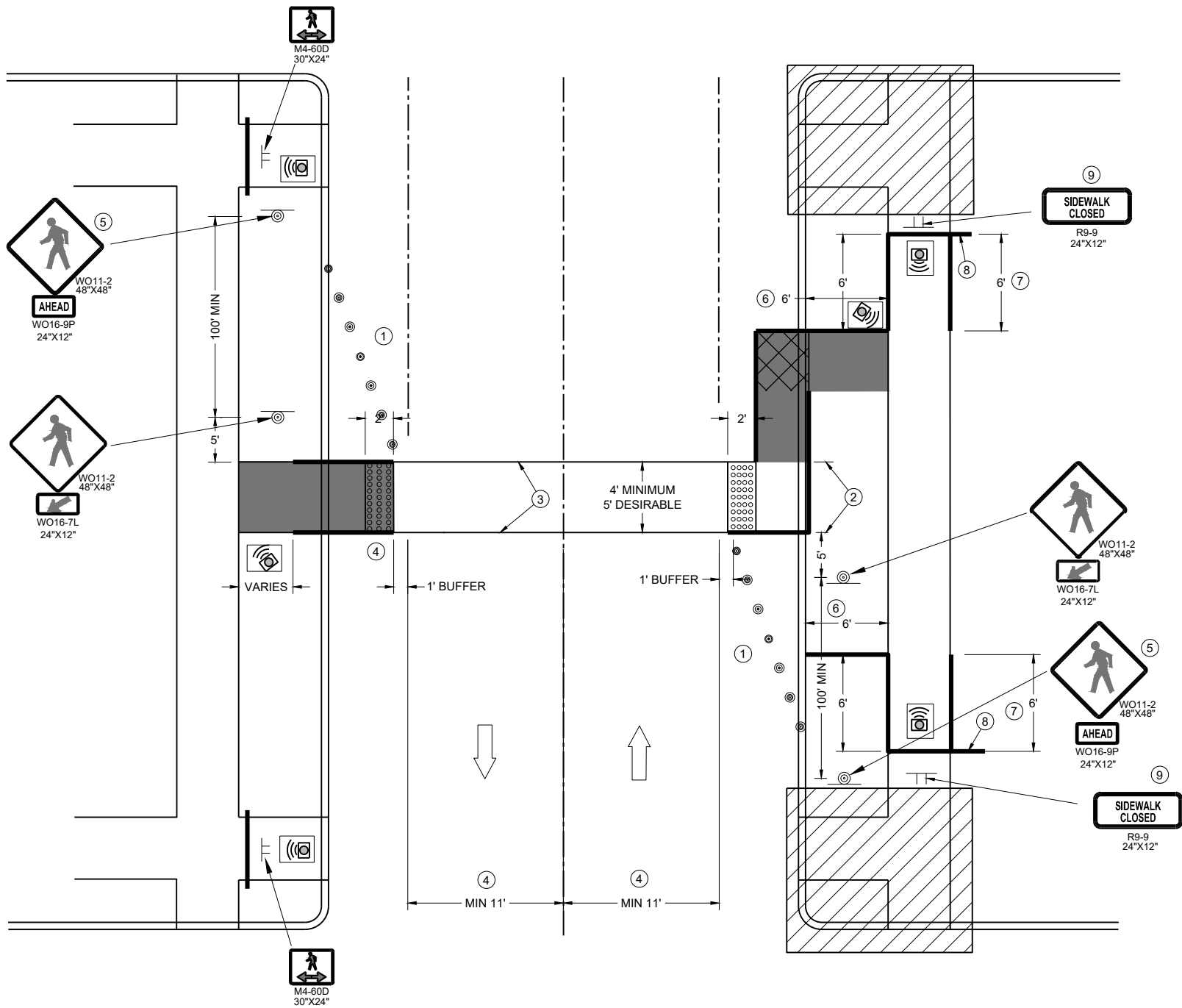
GENERAL NOTES

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICT WITH DRIVEWAYS AND OTHER EXISTING FEATURES.

- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



TEMPORARY PEDESTRIAN CROSSING

GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
SEE OTHER PEDESTRIAN ACCOMMODATION DETAILS FOR SIGNING AND DEVICES FOR DIFFERENT
PEDESTRIAN FACILITIES CLOSURES.

WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE
MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.

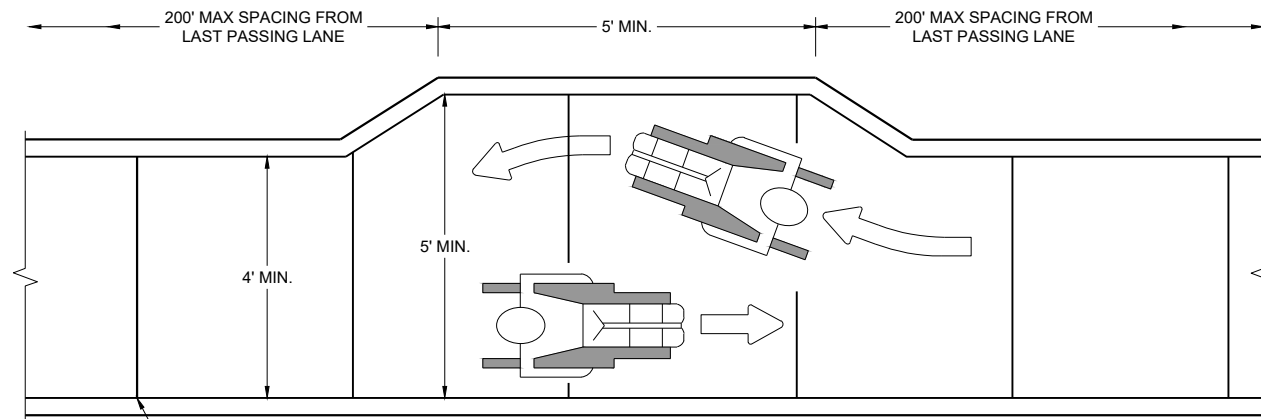
- ① SHOULDER OR LANE CLOSURE ADVANCED WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② 4 FEET MINIMUM, 5 FEET DESIRABLE.
- ③ WHITE 6" TEMPORARY PAVEMENT MARKING.
- ④ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, PERPENDICULAR CURB RAMP MAY NEED TO BE UTILIZED.
- ⑤ IF MINIMUM 100' SPACING FROM THE MID-BLOCK CROSSING CANNOT BE ATTAINED BEFORE THE INTERSECTION, REMOVE THIS SIGN ASSEMBLY.
- ⑥ IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ⑦ PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ⑧ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF THE EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ⑨ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF THE SIGN.

LEGEND

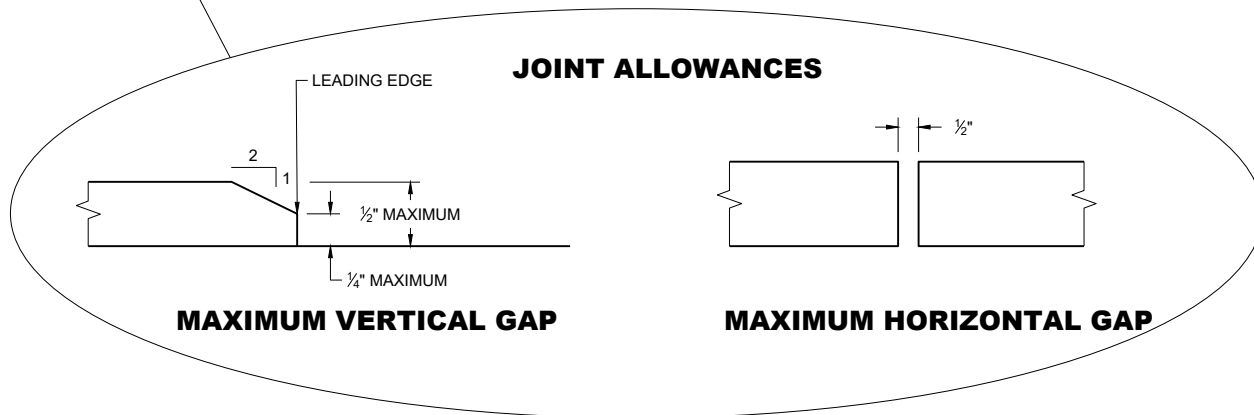
- ⦿ TRAFFIC CONTROL DRUM
- SIGN ON TEMPORARY SUPPORT
- ▬ TEMPORARY CURB RAMP
- ◻ TEMPORARY DETECTABLE WARNING FIELD
- ▬ TEMPORARY PEDESTRIAN SURFACE "A"
- ▬ TEMPORARY PEDESTRIAN SURFACE "B"
- ▨ WORK AREA
- TEMPORARY PEDESTRIAN BARRICADE
- ➡ DIRECTION OF TRAFFIC
- 🔊 TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)

TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

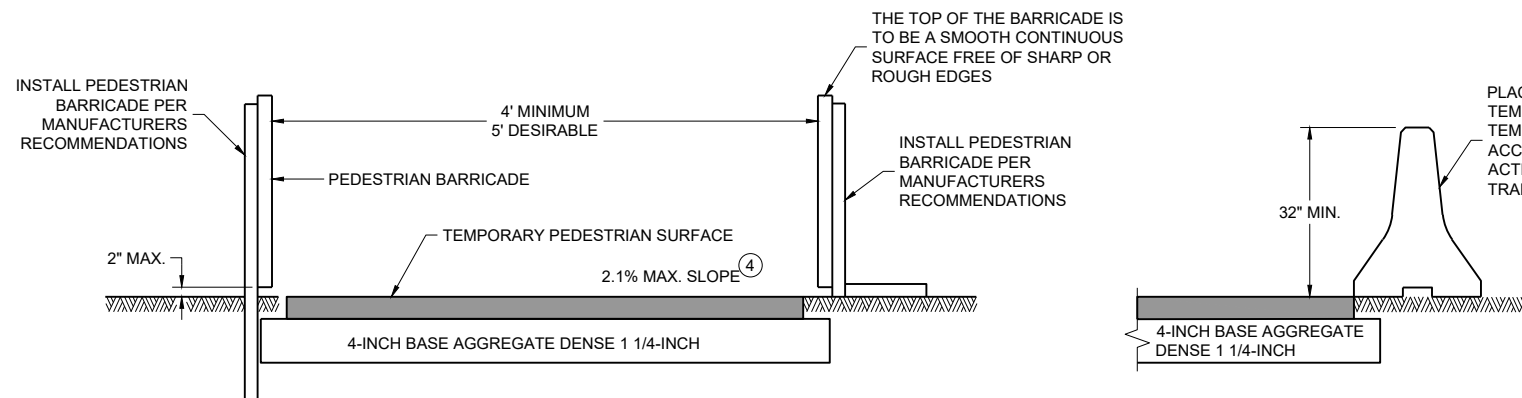


NARROW SIDEWALK PASSING DETAIL



MAXIMUM VERTICAL GAP

MAXIMUM HORIZONTAL GAP



TEMPORARY PEDESTRIAN ACCESS



LEGEND

TEMPORARY PEDESTRIAN BARRICADE



AUDIBLE MESSAGE DEVICE



TEMPORARY SIGN SUPPORT

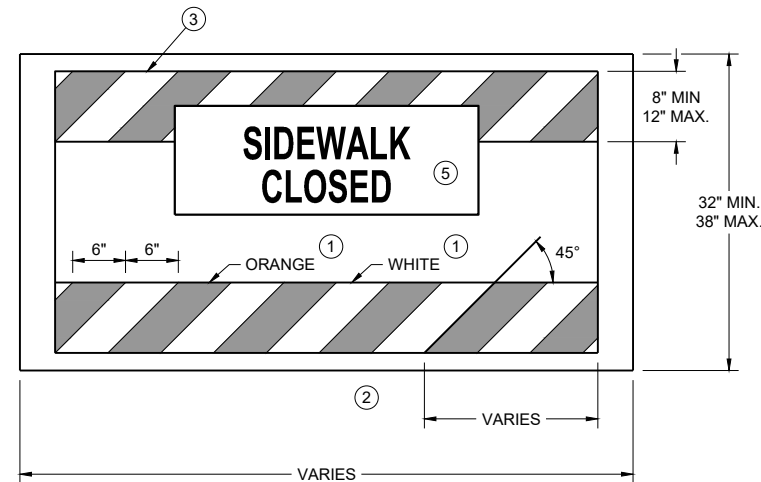


WORK AREA

GENERAL NOTES

BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
 - ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
 - ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
 - ★ USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.
 - ④ WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.
- WHERE SIGNS FOR TEMPORARY PEDESTRIAN ACCOMMODATIONS ARE SHOWN BEING PLACED BEHIND TEMPORARY PEDESTRIAN BARRICADE, THE SIGNS MAY BE MOUNTED ON THE TEMPORARY PEDESTRIAN BARRICADE INSTEAD. A CORRUGATED POLYPROPYLENE OR POLYETHYLENE PLASTIC SIGN BASE SHALL BE USED IF MOUNTED ON THE BARRICADE. THE TOP OF THE SIGN SHALL BE MOUNTED BELOW THE TOP OF THE BARRICADE TO ALLOW A CONTINUOUS HAND-TRAILING EDGE.



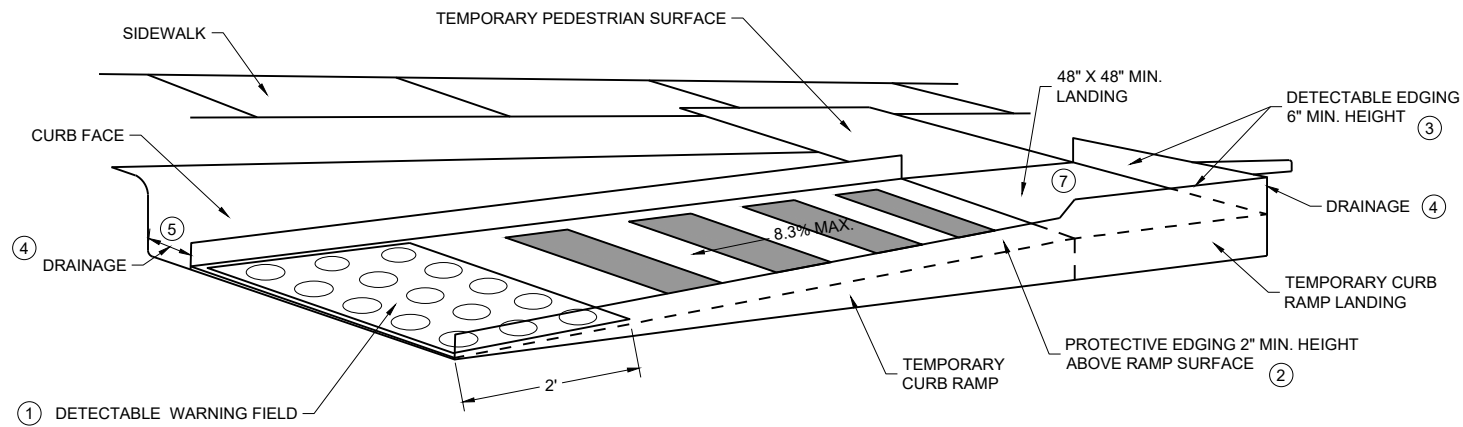
TEMPORARY PEDESTRIAN BARRICADE *



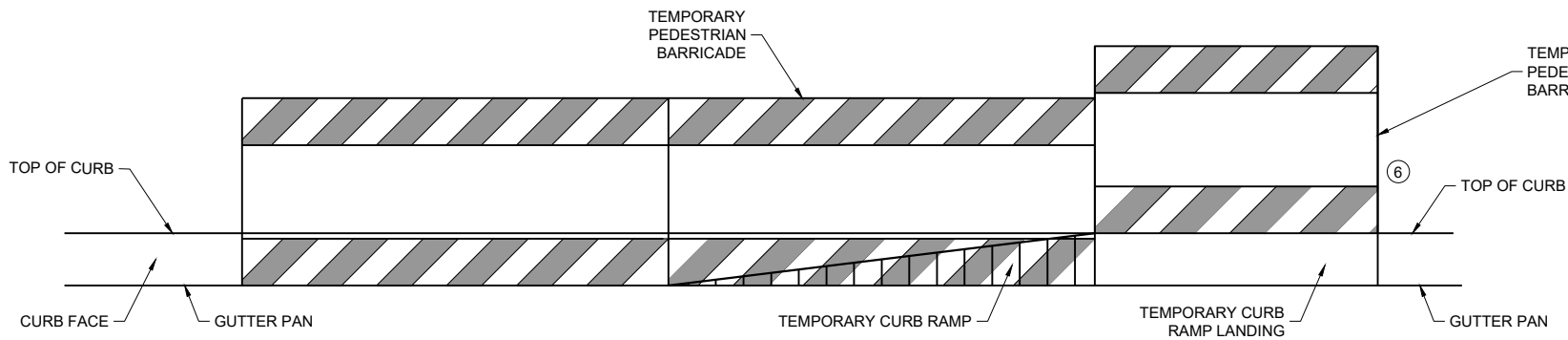
TEMPORARY PEDESTRIAN FLAGGING

TRAFFIC CONTROL,
PEDESTRIAN
ACCOMMODATION

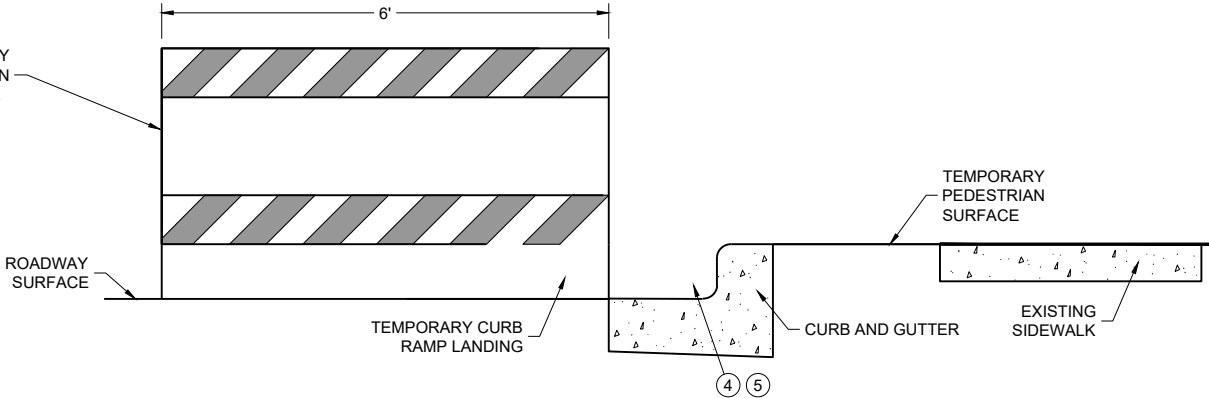
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PERSPECTIVE VIEW



FRONT VIEW



SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:48 (2.1%) MAX. CROSS-SLOPE.

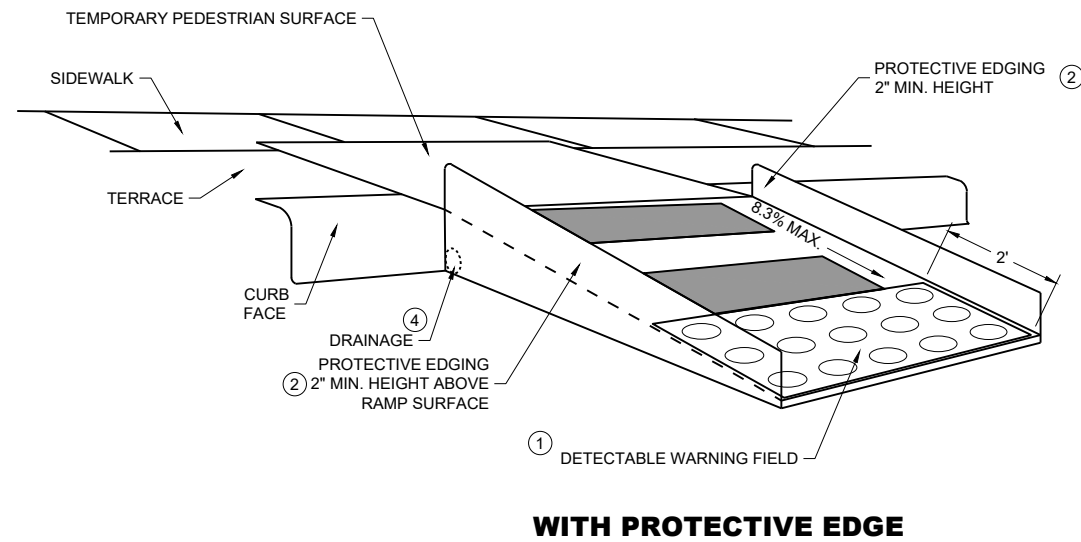
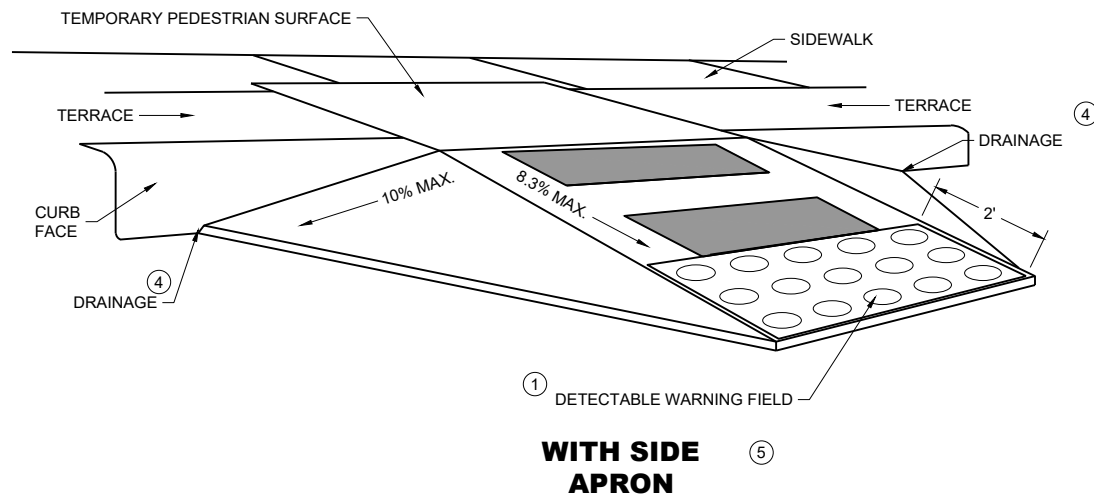
CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP. LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

1. INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
2. PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
3. DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
4. DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
5. ENSURE CURB RAMP IS OUT OF THE GUTTER PAN.
6. IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.
7. LANDING TO BE SLOPED A MAXIMUM OF 2.1% IN ALL DIRECTIONS OF PEDESTRIAN TRAVEL.

TRAFFIC CONTROL,
PEDESTRIAN
ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

**TEMPORARY CURB RAMP PERPENDICULAR TO CURB****GENERAL NOTES**

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:48 (2.1%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN ½" WIDTH.

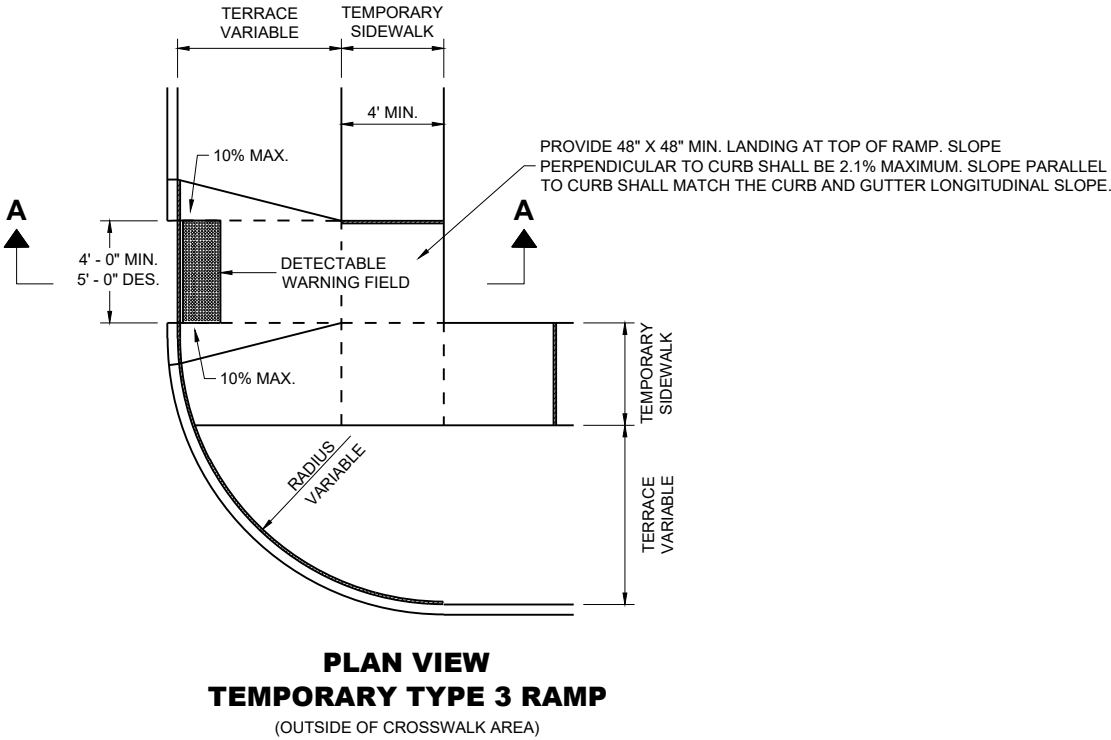
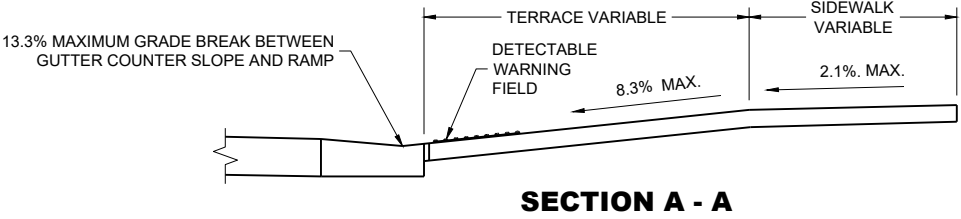
CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED ½". LATERAL EDGES MAY BE VERTICAL UP TO ¼" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN ¼" AND ½".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

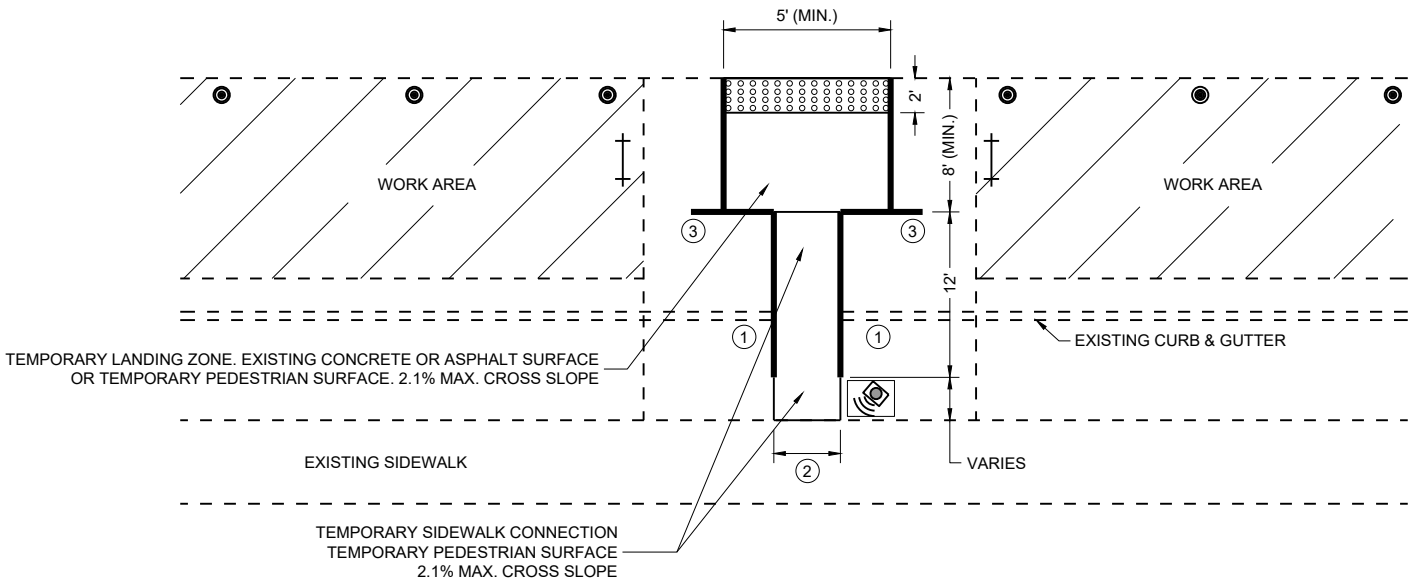
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

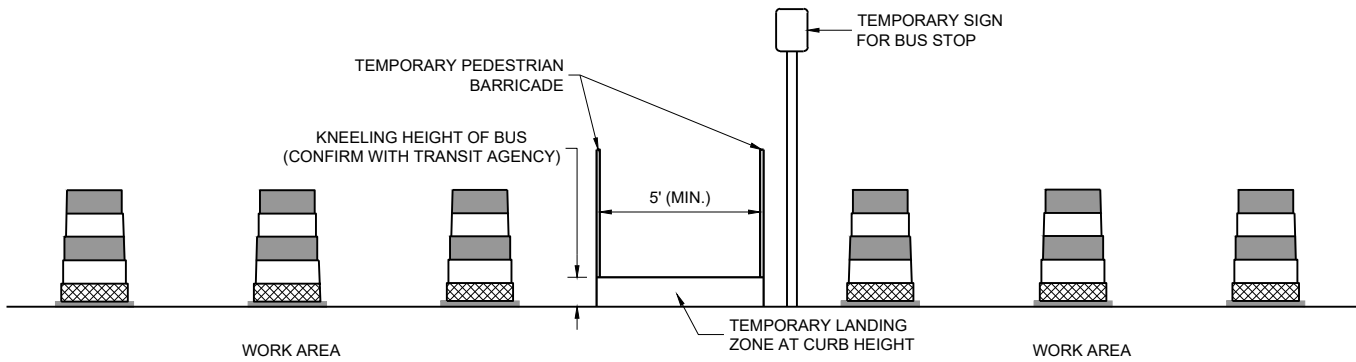


TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PLAN VIEW



PROFILE VIEW
TEMPORARY BUS STOP PAD

GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.

PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.

DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).

LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

CURB RAMPS AND LANDINGS SHALL HAVE A 1:48 (2.1%) MAX. CROSS-SLOPE.

- ① DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ② 5' WIDE MIN. WITH TEMPORARY PEDESTRIAN BARRICADE, 10' WIDE MIN. WITHOUT TEMPORARY PEDESTRIAN BARRICADE.
- ③ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE INTO THIS SPACE.

LEGEND

- TRAFFIC CONTROL DRUM
- ⊢ TYPE III BARRICADE
- TEMPORARY PEDESTRIAN BARRICADE
- ⊞ TEMPORARY DETECTABLE WARNING FIELD
- ▨ WORK AREA
- 🔊 TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)

TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- UNDER PEDESTRIAN TRAFFIC
- WORK AREA
- TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC
- TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)

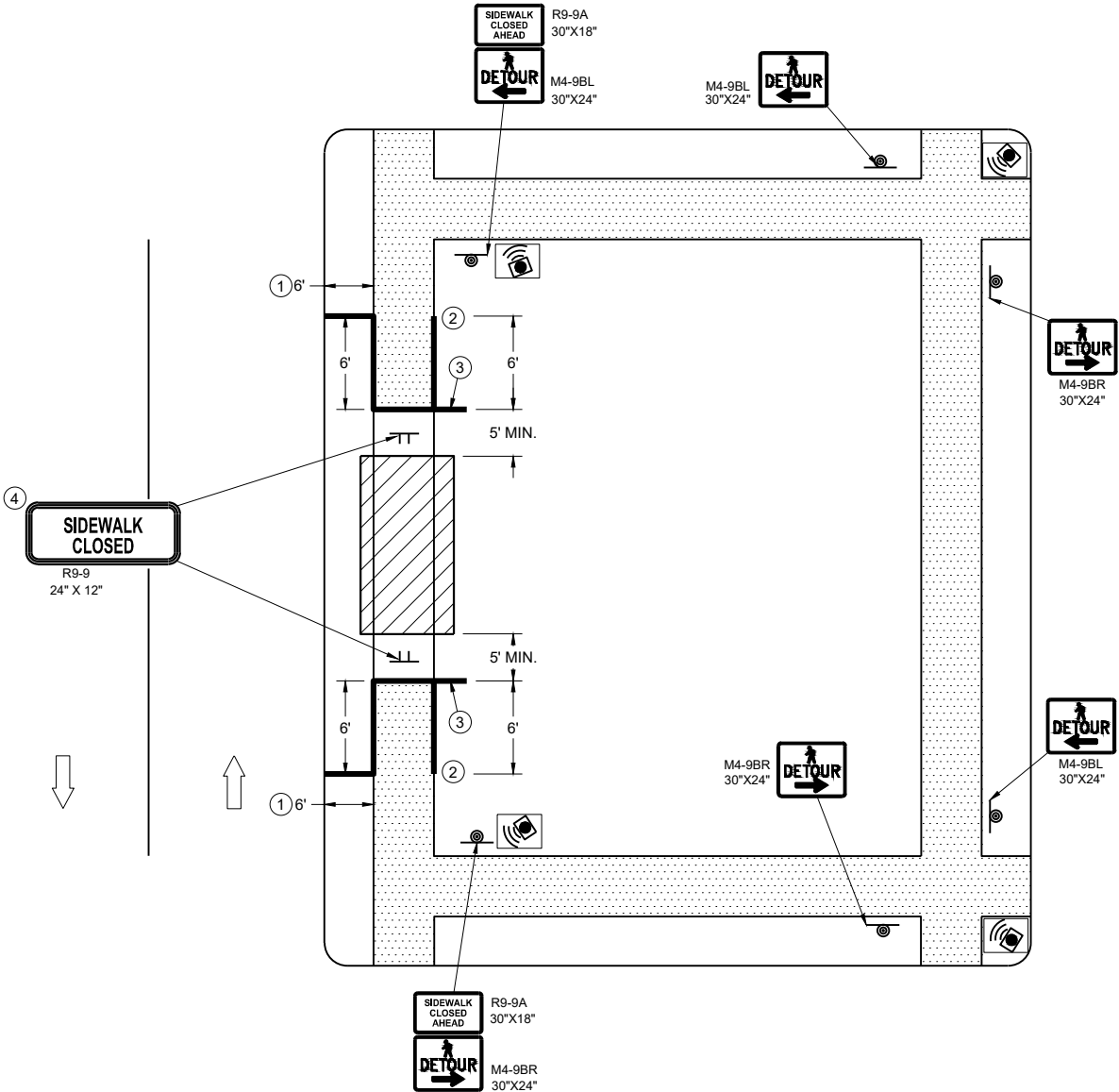
GENERAL NOTES

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.








PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICTS WITH DRIVEWAYS AND OTHER EXISTING FEATURES.

- IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

LEGEND

- SIGN ON TEMPORARY SUPPORT
- WORK AREA
- UNDER PEDESTRIAN TRAFFIC
- TEMPORARY PEDESTRIAN SURFACE
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

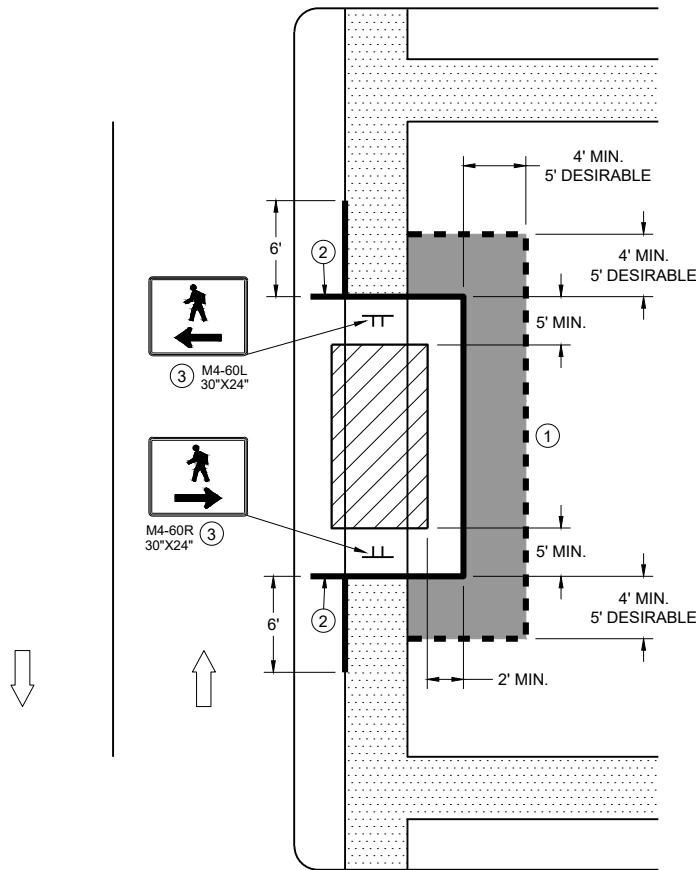
GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

- ① USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ② IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.












SIDEWALK BYPASS
SINGLE SIDE

TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

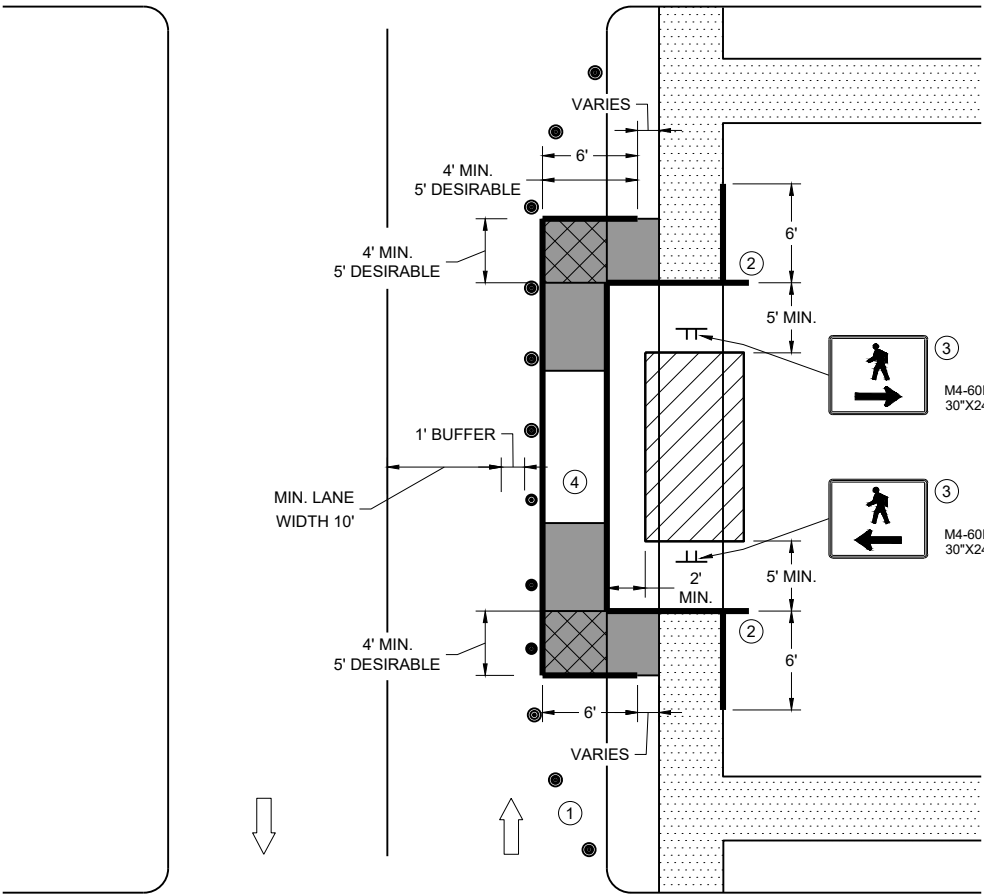
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- UNDER PEDESTRIAN TRAFFIC
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

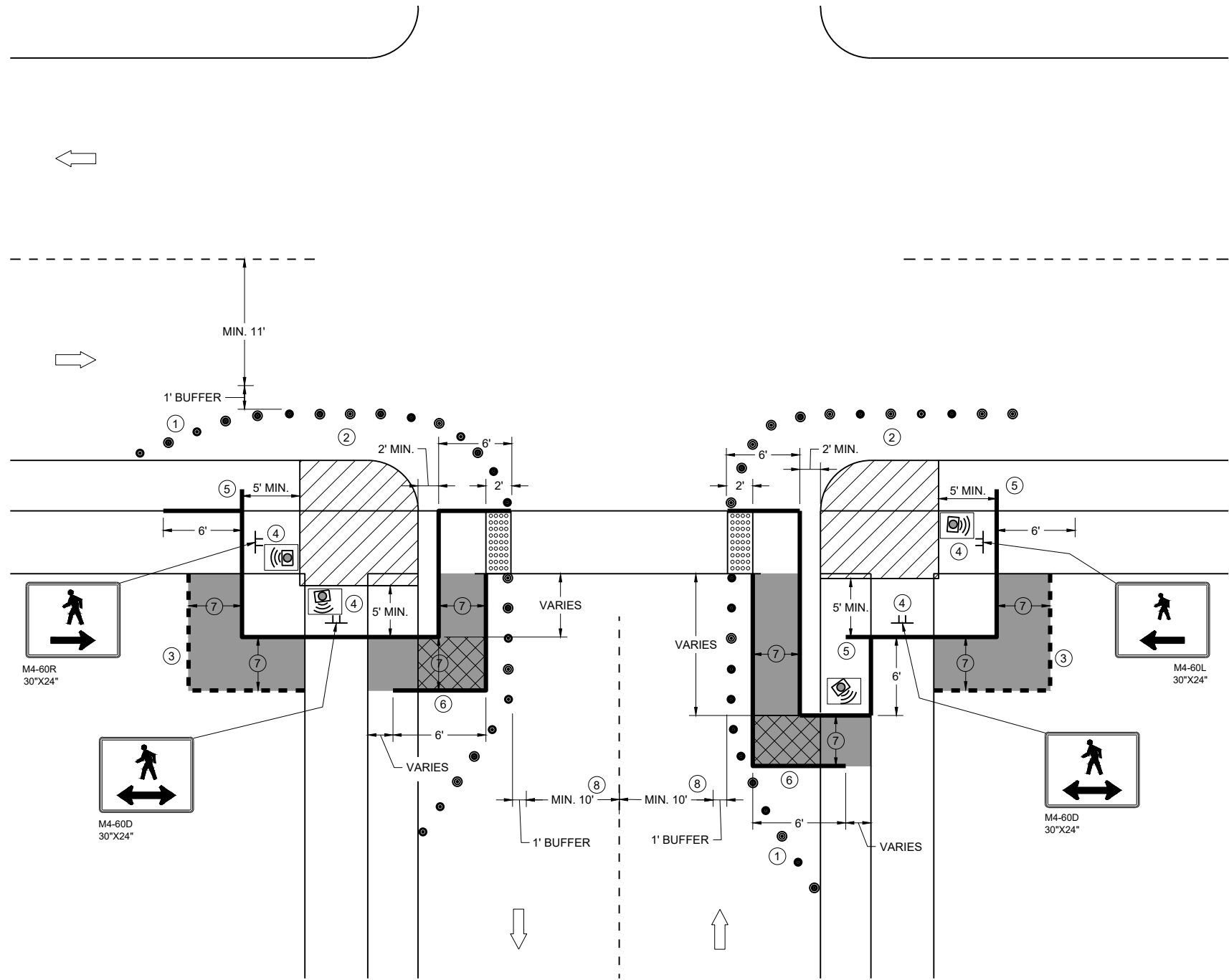
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
- ② PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
- ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
- ④ USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURFACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE. WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.



SIDEWALK BYPASS, SINGLE SIDE

TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



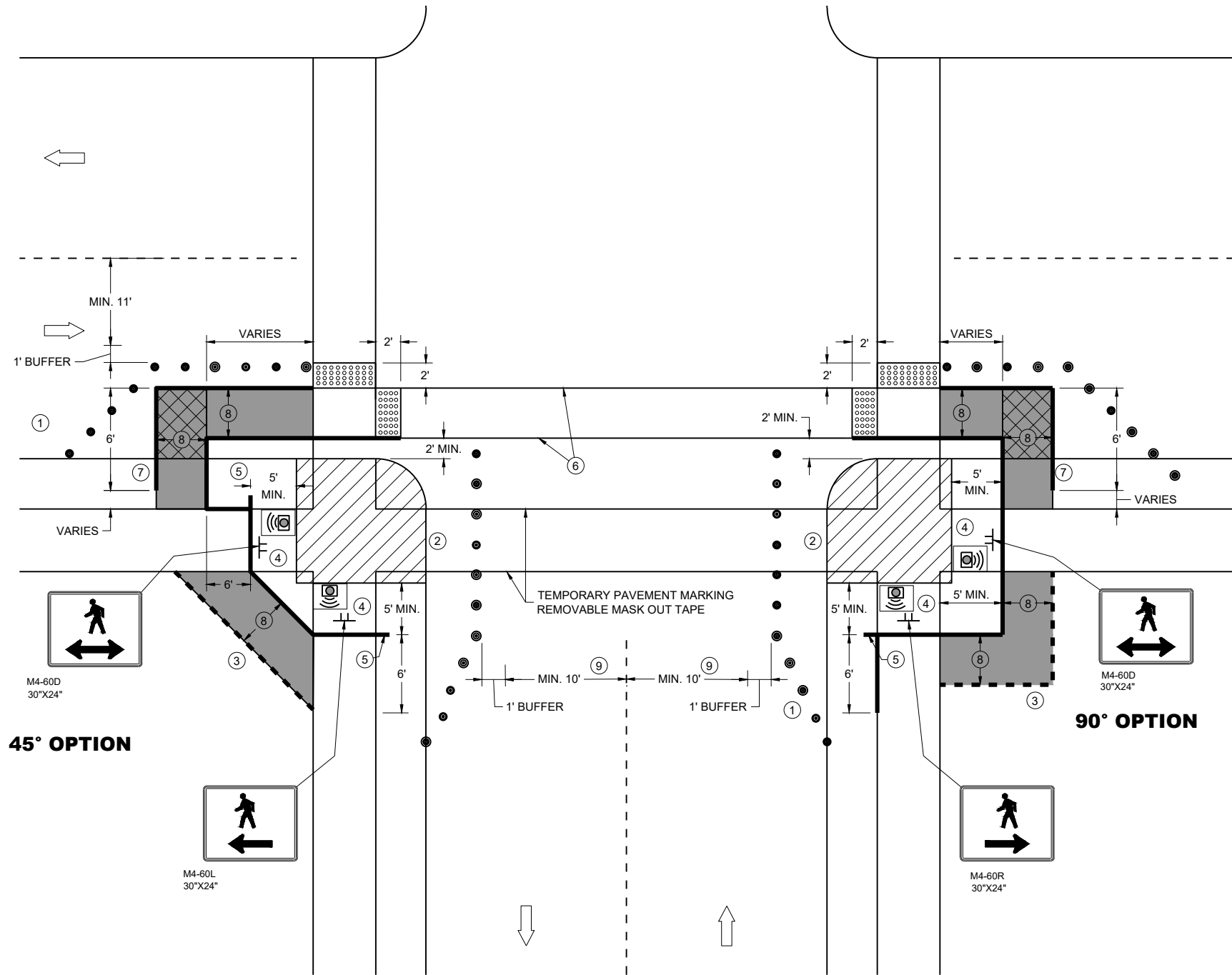
CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE

GENERAL NOTES

- IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.
- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG
- WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
 - ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
 - ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
 - ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
 - ⑥ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
 - ⑦ 4 FEET MINIMUM, 5 FEET DESIRABLE
 - ⑧ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC
- TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)



CURB RAMP PEDESTRIAN TRAFFIC CONTROL

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.

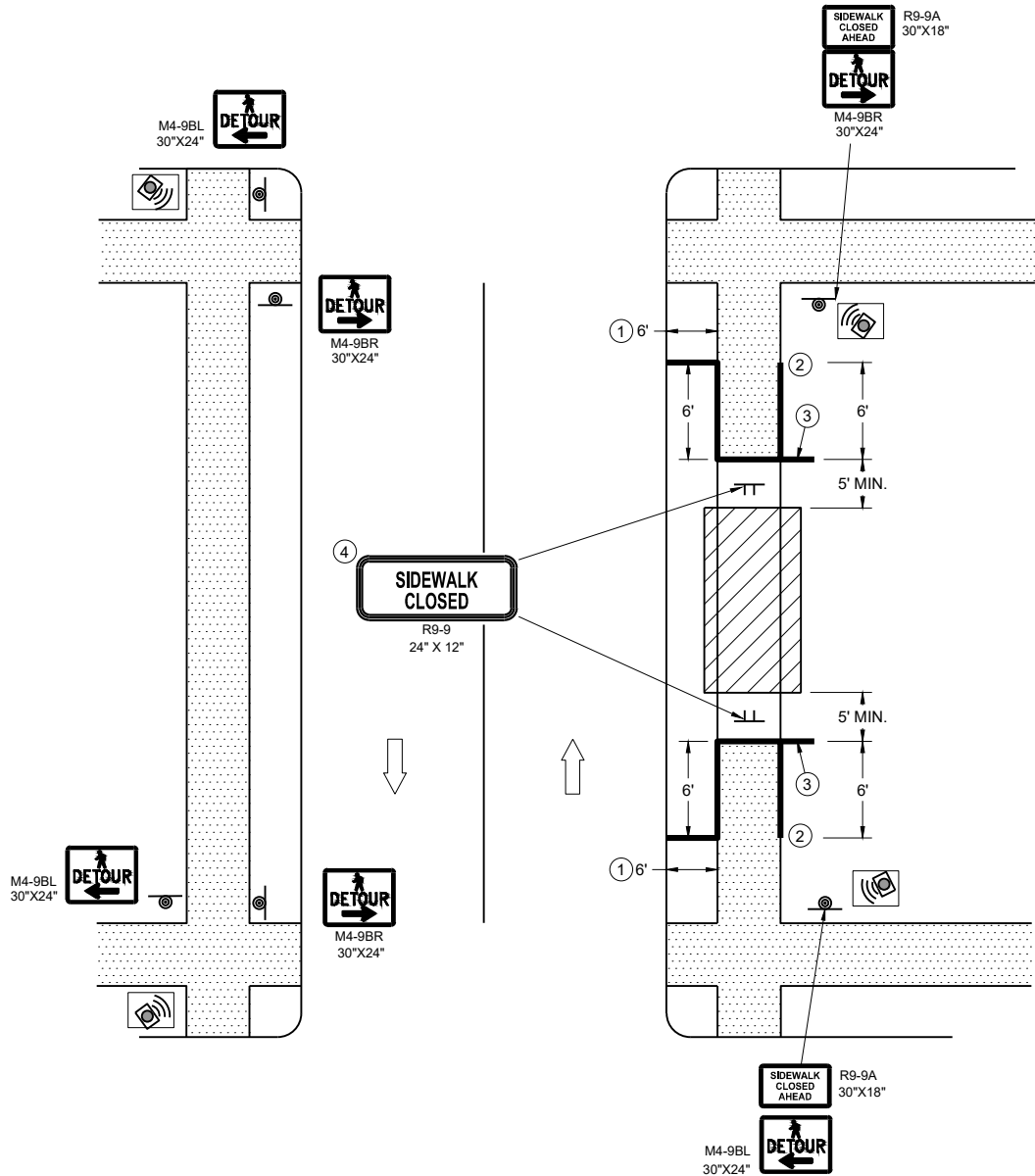
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ WHITE 6" TEMPORARY PAVEMENT MARKING
- ⑦ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑧ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑨ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC
- TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)

LEGEND

- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- UNDER PEDESTRIAN TRAFFIC
- WORK AREA
- TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC
- TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)



SIDEWALK DETOUR, SIDEWALK ON BOTH SIDES

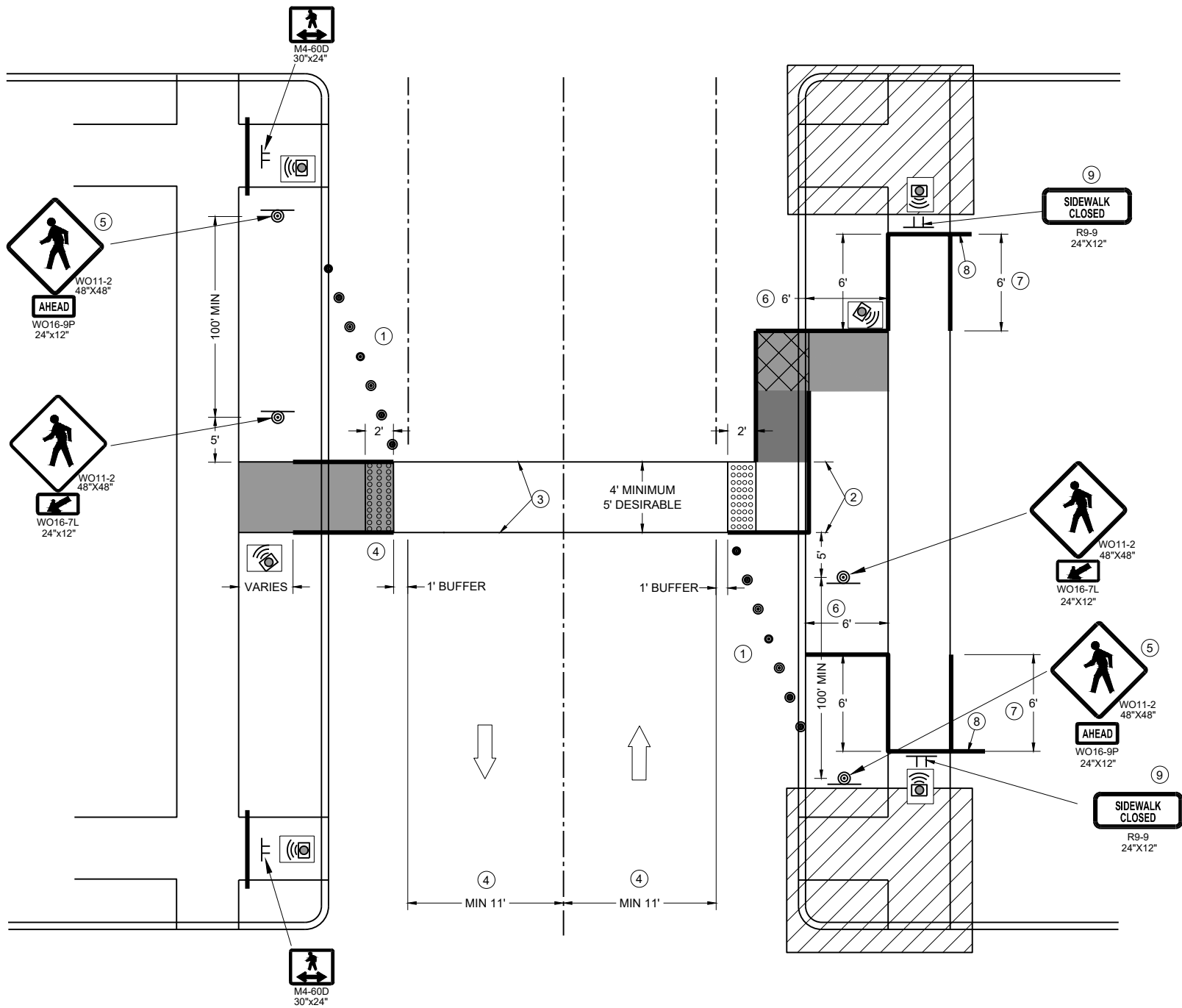
GENERAL NOTES

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICT WITH DRIVEWAYS AND OTHER EXISTING FEATURES.

- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



TEMPORARY PEDESTRIAN CROSSING

GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
SEE OTHER PEDESTRIAN ACCOMMODATION DETAILS FOR SIGNING AND DEVICES FOR DIFFERENT PEDESTRIAN FACILITIES CLOSURES.

WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.










- ① SHOULDER OR LANE CLOSURE ADVANCED WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② 4 FEET MINIMUM, 5 FEET DESIRABLE.
- ③ WHITE 6" TEMPORARY PAVEMENT MARKING.
- ④ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, PERPENDICULAR CURB RAMP MAY NEED TO BE UTILIZED.
- ⑤ IF MINIMUM 100' SPACING FROM THE MID-BLOCK CROSSING CANNOT BE ATTAINED BEFORE THE INTERSECTION, REMOVE THIS SIGN ASSEMBLY.
- ⑥ IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ⑦ PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ⑧ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF THE EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ⑨ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF THE SIGN.

LEGEND

- TRAFFIC CONTROL DRUM
- SIGN ON TEMPORARY SUPPORT
- TEMPORARY CURB RAMP
- TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- WORK AREA
- TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC
- TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2025 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

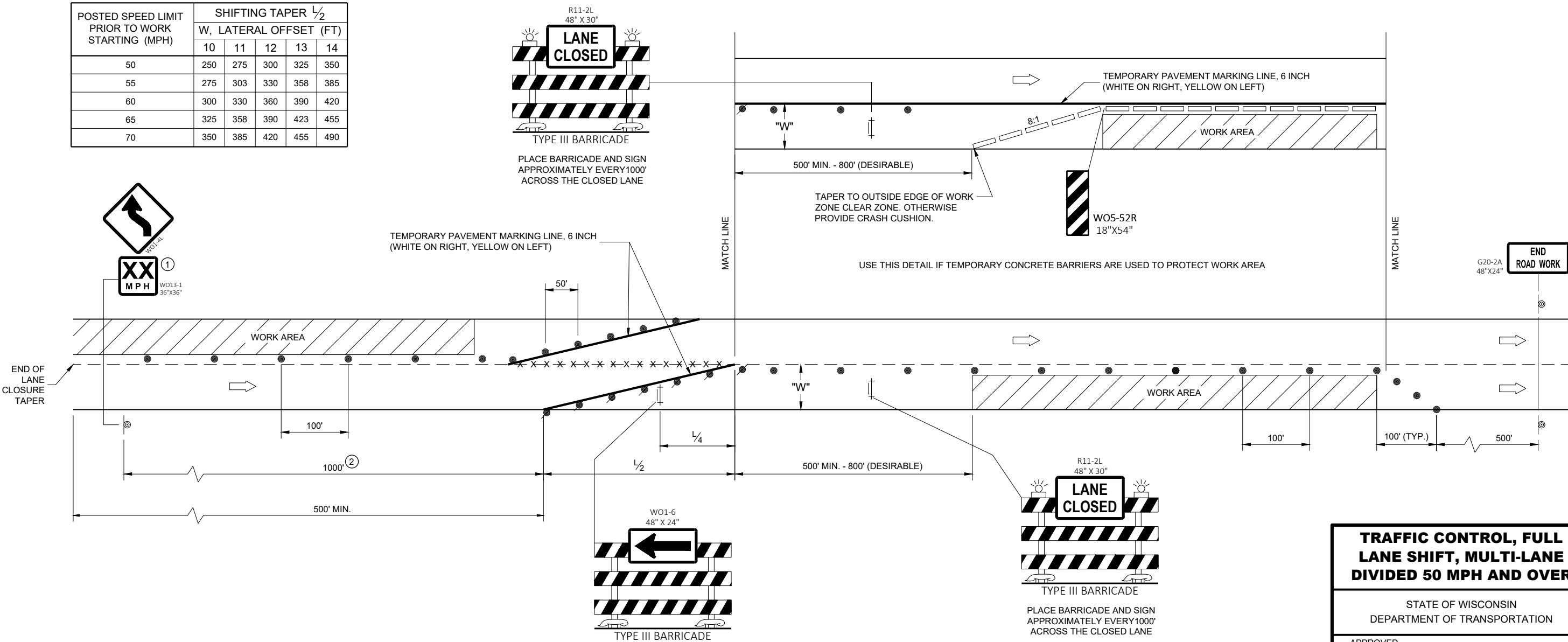
LEGEND

-  TYPE III BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE "A" WARNING LIGHT (FLASHING)
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
-  CONCRETE BARRIER TEMPORARY PRECAST






POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	SHIFTING TAPER $\frac{L}{2}$ W, LATERAL OFFSET (FT)				
	10	11	12	13	14
50	250	275	300	325	350
55	275	303	330	358	385
60	300	330	360	390	420
65	325	358	390	423	455
70	350	385	420	455	490

GENERAL NOTES

- THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.
- THIS LANE CLOSURE IS TYPICAL FOR LANE SHIFT RIGHT - REVERSE FOR SHIFTING LEFT.
- ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.
- "WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.
- ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON ANY "COVERED" OR "DOWNED" SIGNS.
- CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.
- FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINES IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.
- WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.
- IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.
- ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE SHIFT OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE SHIFT MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE $\frac{1}{2}$ THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.
- 1 USE ONLY IF DESIGN SPEED IS 10 MPH BELOW POSTED SPEED.
- 2 IF BEGINNING OF LANE SHIFT IS 1200' OF LESS FROM THE END OF THE LANE CLOSURE TAPER, PLACE THE WO1-4L SIGN 200 FEET AFTER THE END OF THE LANE CLOSURE TAPER.



LEGEND

- 
- TYPE III BARRICADE WITH ATTACHED SIGN
- 
- SIGN ON PERMANENT SUPPORT
- 
- TRAFFIC CONTROL DRUM
- 
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- 
- TYPE "A" WARNING LIGHT (FLASHING)
- 
- DIRECTION OF TRAFFIC
- 
- WORK AREA

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHIFTING TAPER $L/2$					
		W, LATERAL OFFSET (FT)					
		3	4	5	6	7	8
25	200	10	14	17	21	24	28
30	200	15	20	25	30	35	40
35	350	20	27	34	40	47	54
40	350	26	35	44	53	62	70
45	500	45	59	74	89	104	119
50	500	50	66	83	99	116	132
55	500	54	73	91	109	127	145

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"x36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

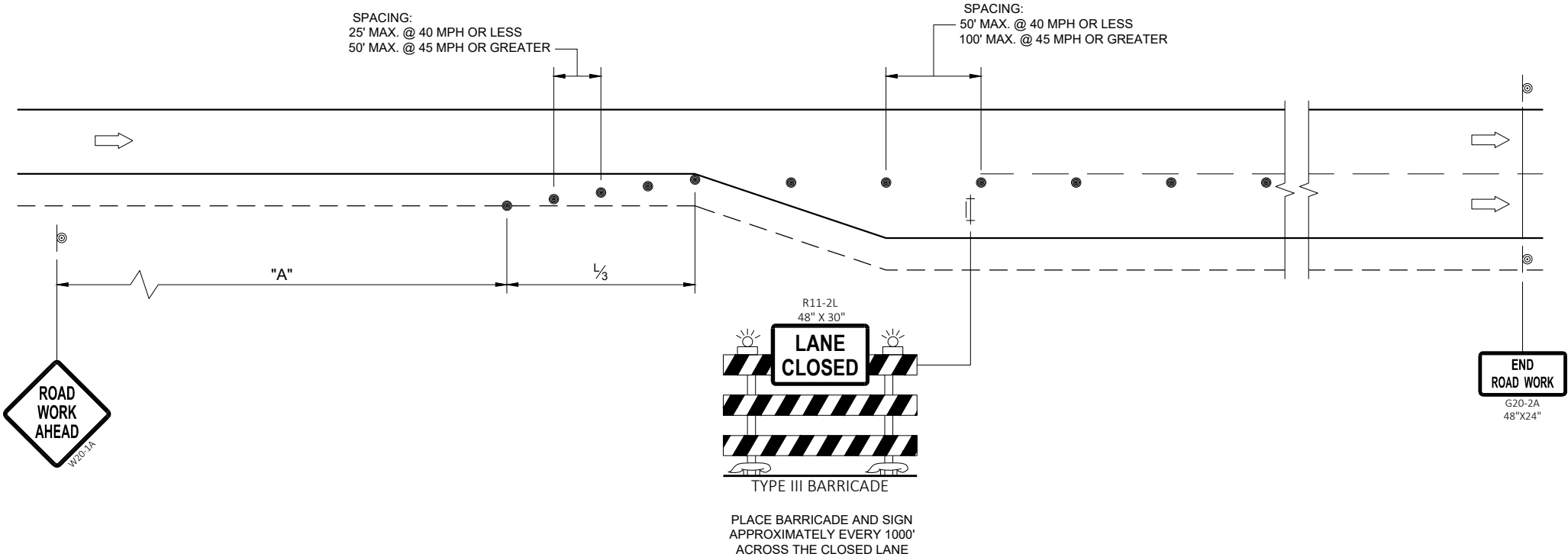
"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION WORK IS LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS.



**TRAFFIC CONTROL
ADDED LANE CLOSURE
WITHOUT LANE SHIFT**

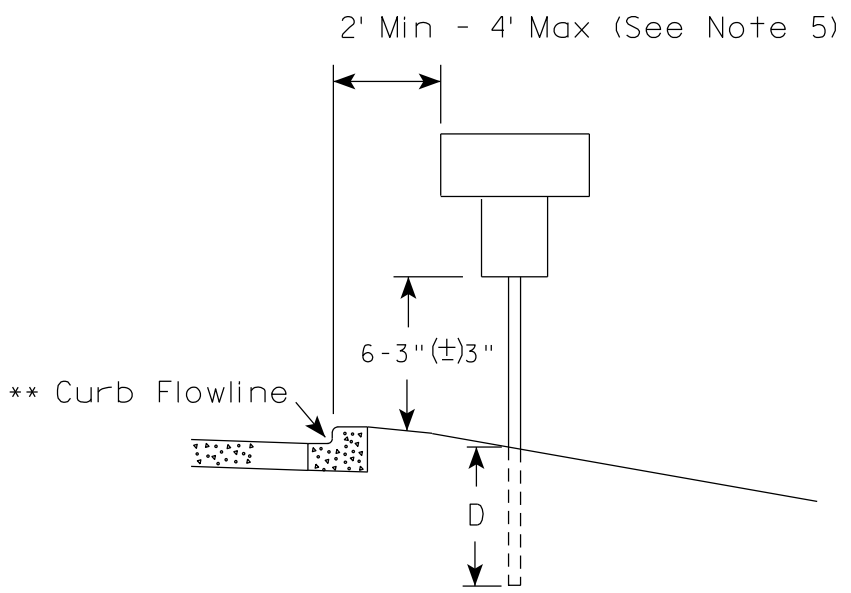
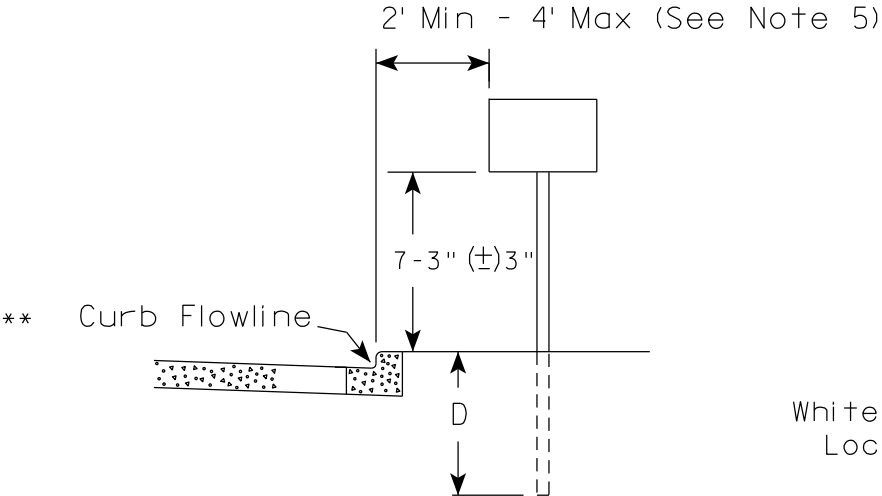
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE

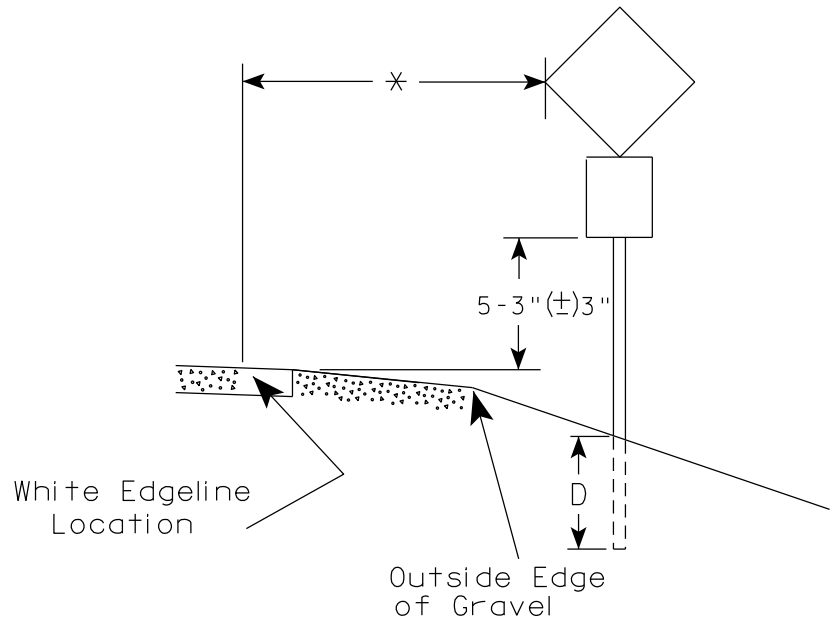
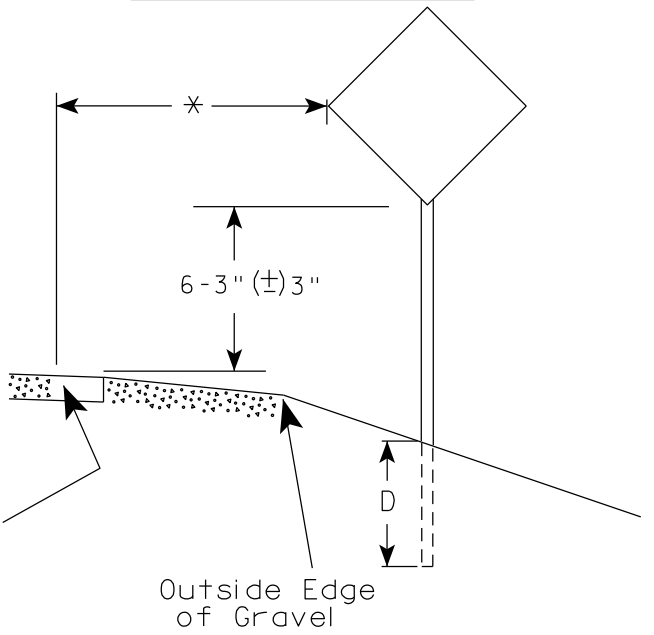
/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

URBAN AREA



RURAL AREA (See Note 2)



POST EMBEDMENT DEPTH	
Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES


1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".
3. For expressways and freeways, mounting height is 7'- 3" (±) 3" or 6'-3" (±) 3" depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±) 3".
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the Engineer.

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED 
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-3.23



ELEVATION VIEW

DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES: 1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST
BOX-OUTS
A4-3B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

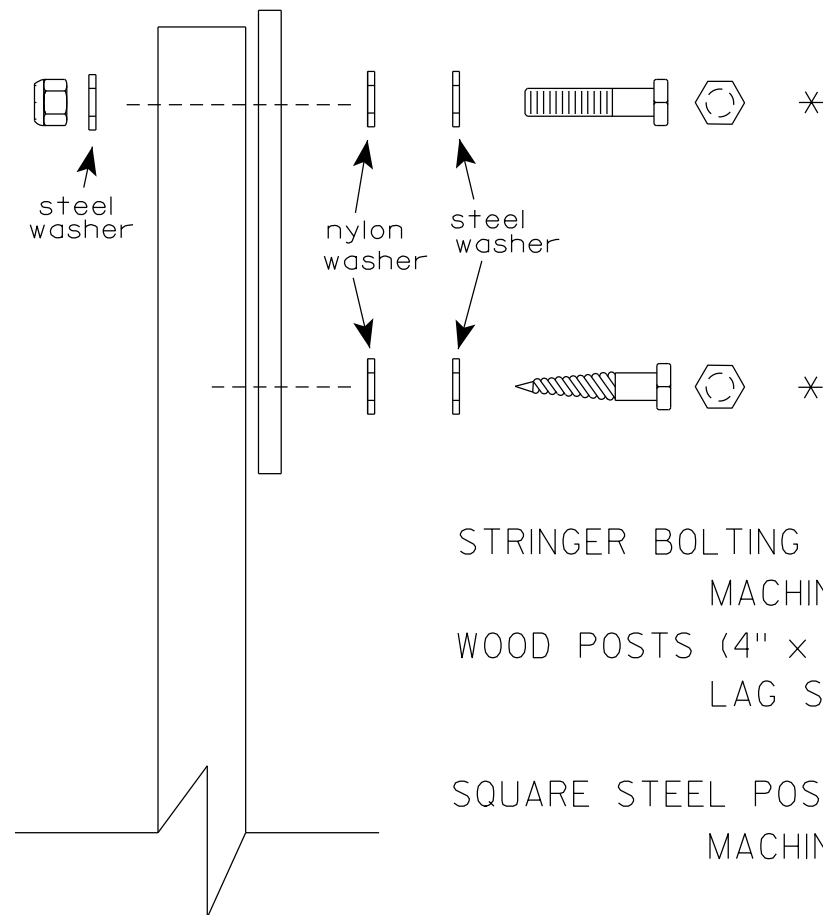
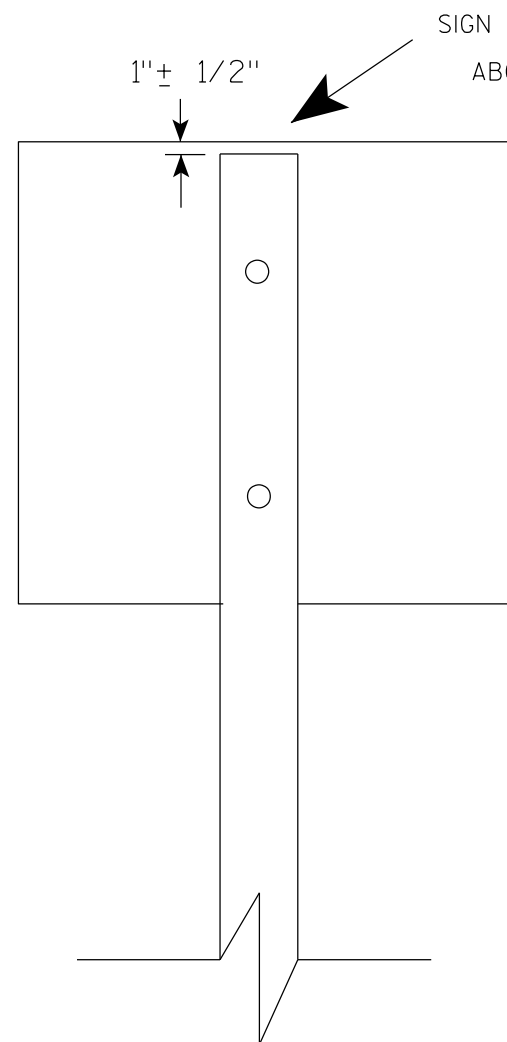
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

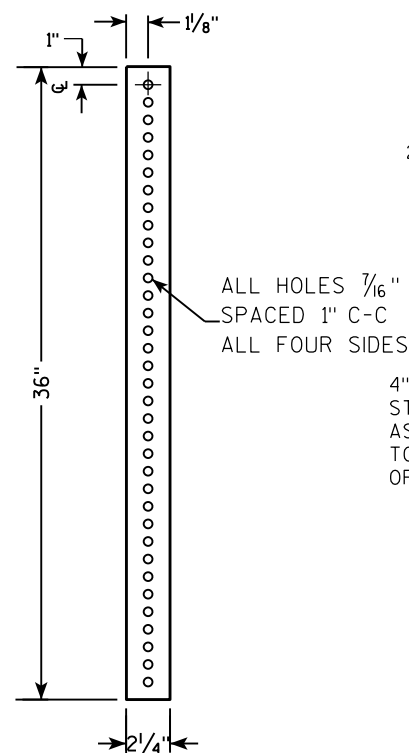
Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS - $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)
 $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
 $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
 - 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON

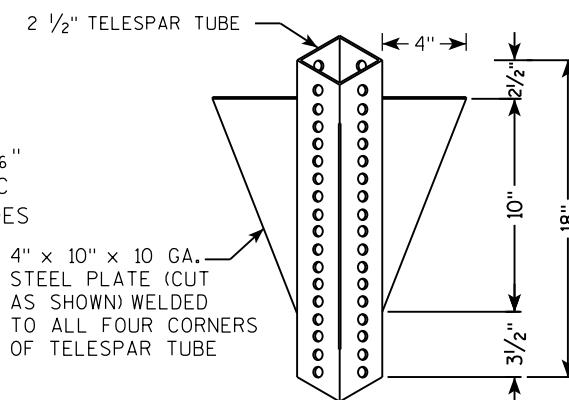
* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 4/1/2020	PLATE NO. A4-8.9

**2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH**



**2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH**



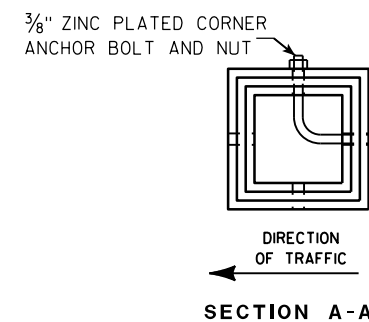
TECHNICAL DRAWING OF A SIGN POST ASSEMBLY.

Labels and Dimensions:

- 18" DIA SCHEDULE 40 PVC BOX-OUT**
- 36"** (Total height of the post section)
- 18"** (Width of the sleeve)
- 13"** (Height of the sleeve)
- 2 1/2" GRAVEL OR DIRT**
- 3/8" ZINC PLATED CORNER ANCHOR BOLT AND NUT**
- 2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)**
- 3/16" ZINC PLATED ANCHOR BOLT AND NUT**
- 2 1/4" SQUARE X 36"**
- 2" STEEL TUBULAR SQUARE UPPER SECTION**
- ALL HOLES 7/16" SPACED 1" C-C ALL FOUR SIDES**
- TELESCOPE PIECES FLUSH AT TOP**
- SIGN**
- SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL**

TECHNICAL DRAWING OF A SIGNPOST ASSEMBLY:

- Dimensions:**
 - Overall height: LENGTH SHOWN ON MISC. Q'TYS
 - Top section height: 36"
 - Section below top: 18"
 - Section below that: 12"
- Components and Labels:**
 - SIGN
 - SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL
 - 2" STEEL TUBULAR SQUARE UPPER SECTION
 - ALL HOLES $\frac{7}{16}$ " SPACED 1" C-C
 - ALL FOUR SIDES
 - $\frac{3}{8}$ " ZINC PLATED CORNER ANCHOR BOLT AND NUT
 - TELESCOPE PIECES FLUSH AT TOP
 - $\frac{3}{8}$ " ZINC PLATED ANCHOR BOLT AND NUT
 - 2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)
 - 2 1/4" SQUARE X 36"



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R. Rauch

for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

PROJECT NO:

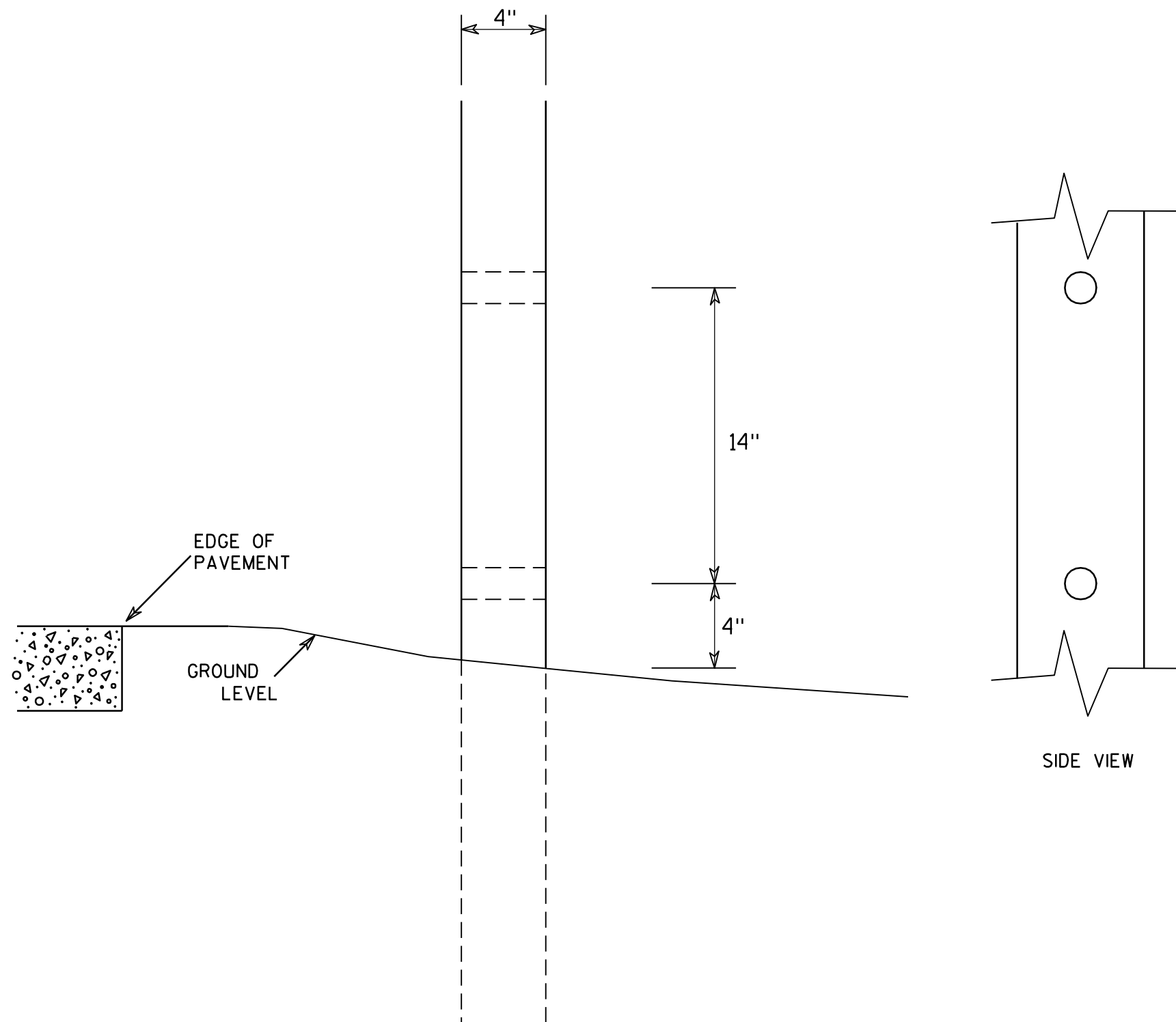
HWY:

COUNTY:

SHEET NO:

T

7

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

**4 X 6 WOOD POST
MODIFICATIONS**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Chester J. Spang
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

PROJECT NO:

HWY:

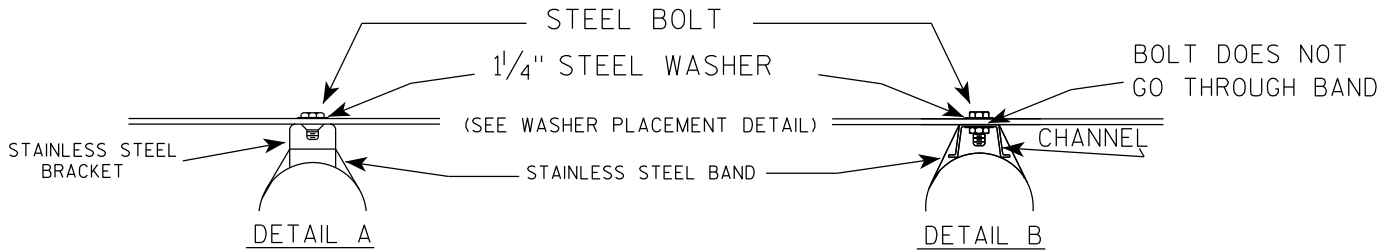
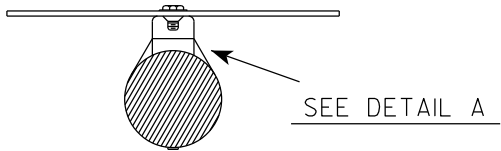
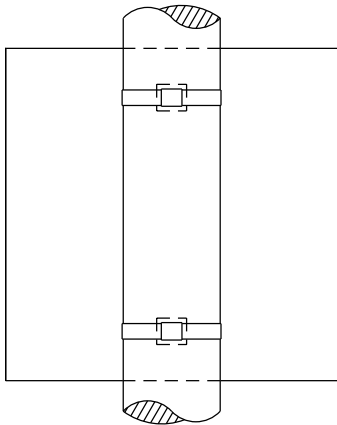
COUNTY:

SHEET NO:

E

BANDING

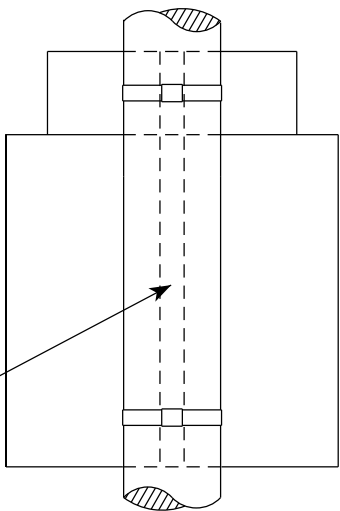
SINGLE SIGN



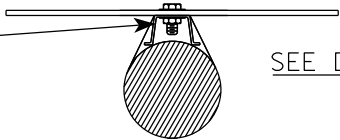
GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be $\frac{3}{4}$ " in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

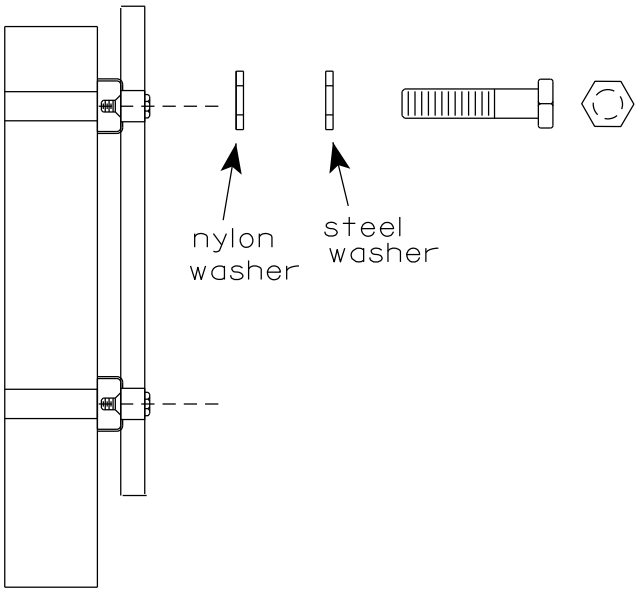
"J" ASSEMBLY



CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET



WASHER PLACEMENT

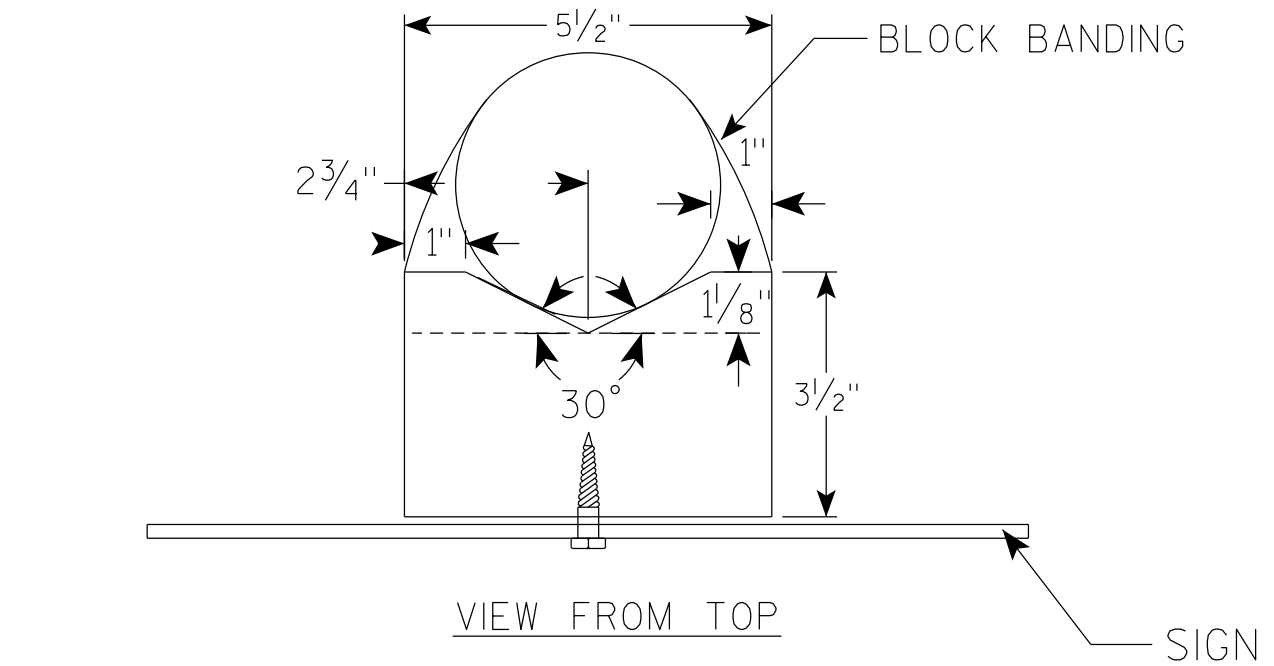
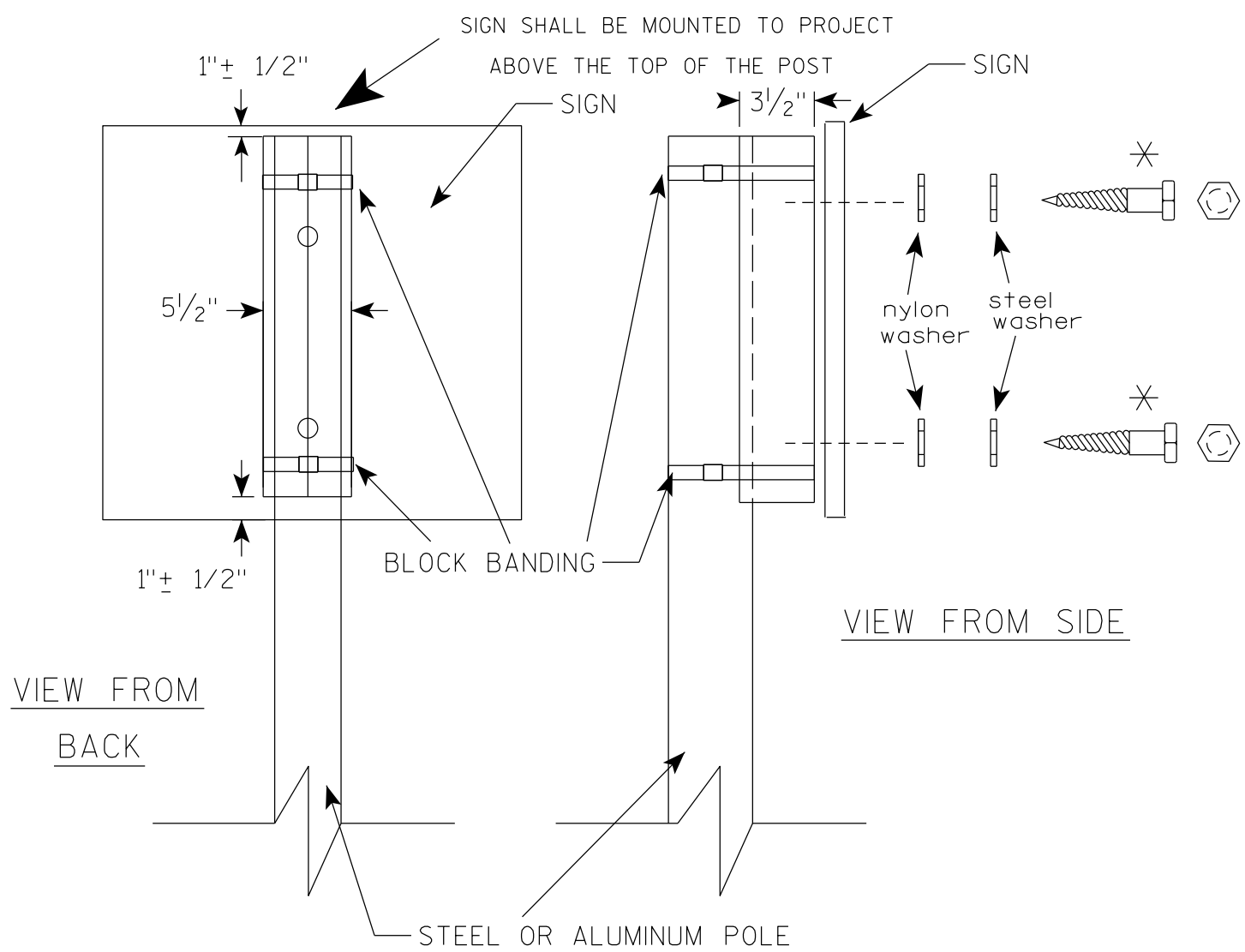


WASHERS (ALL POSTS) -
1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 6/10/19 PLATE NO. A5-9.4



GENERAL NOTES

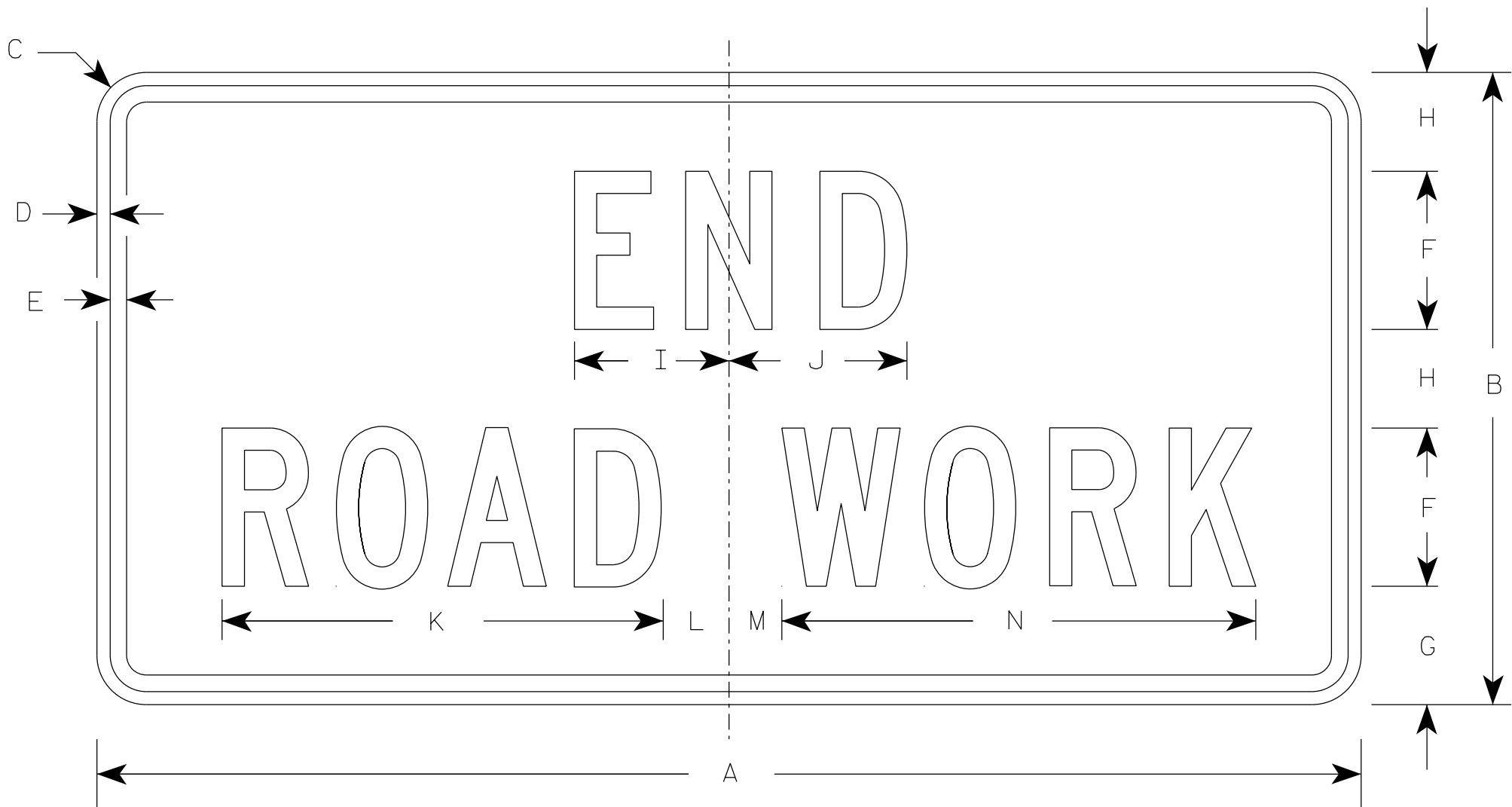
1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, $\frac{3}{4}$ " WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE $\frac{1}{4}$ " O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ "
8. NYLON WASHERS SHALL BE $\frac{1}{4}$ " O.D. X $\frac{3}{8}$ " I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✱ LAG BOLTS SHALL BE $\frac{3}{8}$ " X $2\frac{1}{2}$ "

BLOCK BANDING DETAIL (V-BLOCK OPTION)	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> for State Traffic Engineer
DATE 4/19/2022	PLATE NO. A5-10.3

NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:
Background - Orange
Message - Black
- 3. Message Series - C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



G20-2A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/2	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5
2	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0
2M	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0
3	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0
4	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0
5	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0

STANDARD SIGN

G20-2A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/26/2023 PLATE NO. G20-2A.10

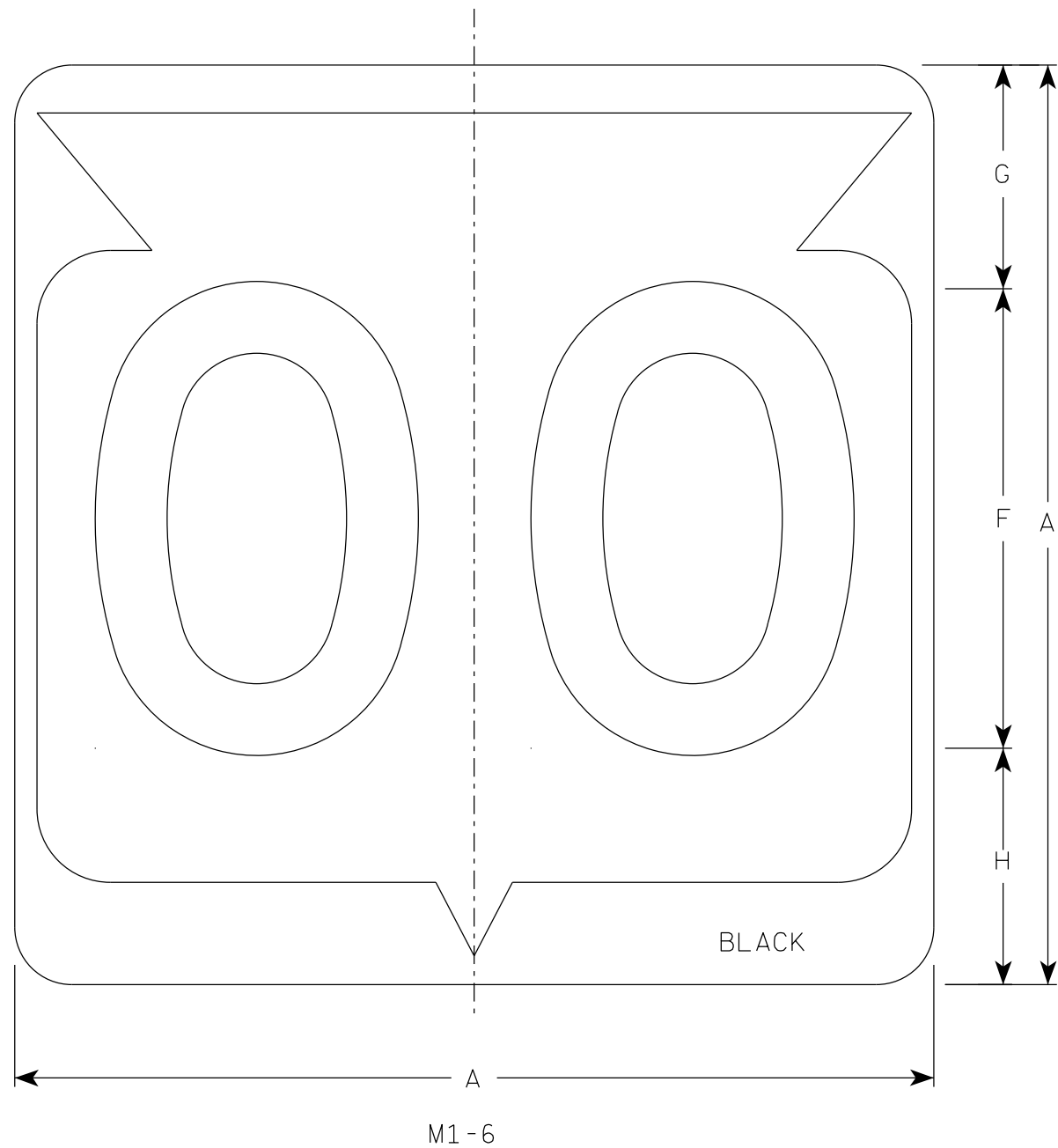
PROJECT NO:

HWY:

COUNTY:

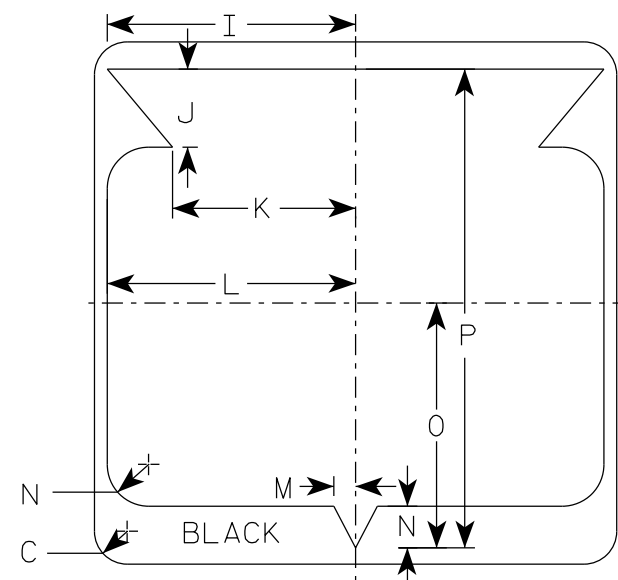
SHEET NO:

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NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D except 3 number signs Series C



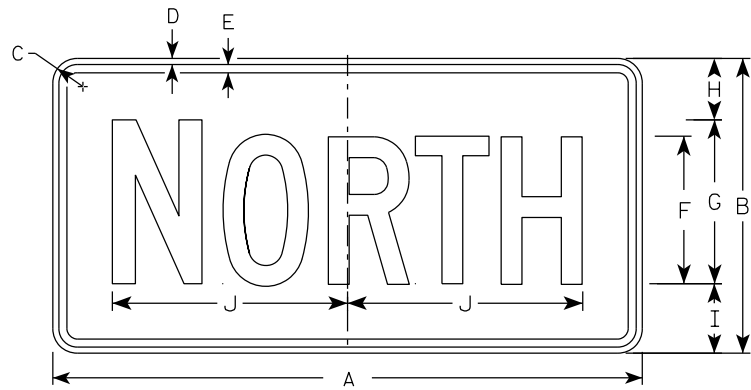
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
2M	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0

STATE ROUTE MARKER
M1-6 FOR ASSEMBLIES

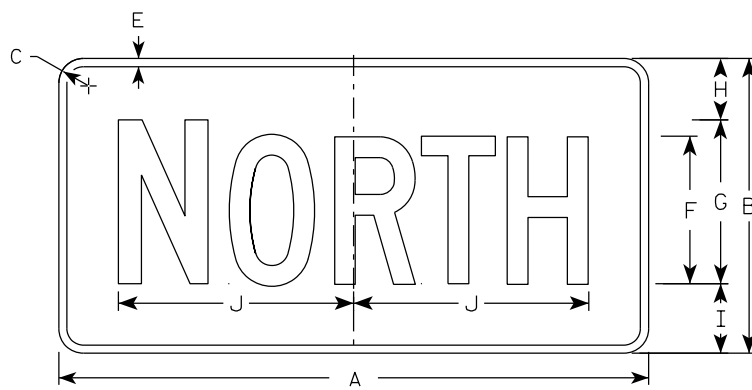
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

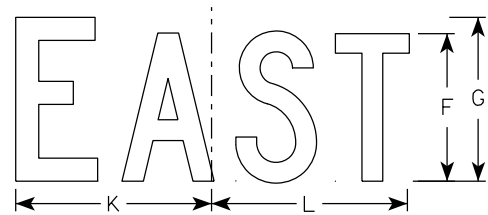
DATE 11/8/2022 PLATE NO. M1-6.11



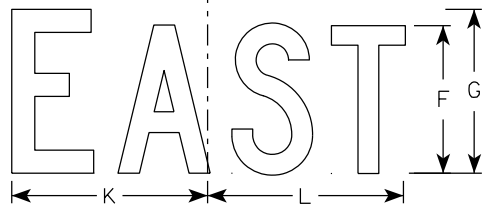
M3-1
MM3-1
MP3-1



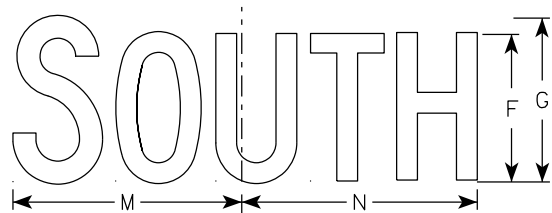
MB3-1
MK3-1
MN3-1



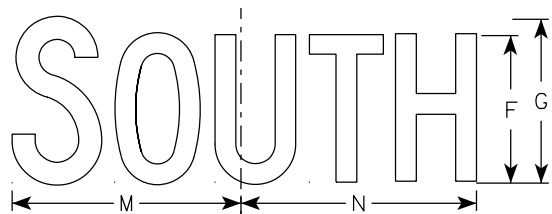
M3-2
MM3-2
MP3-2



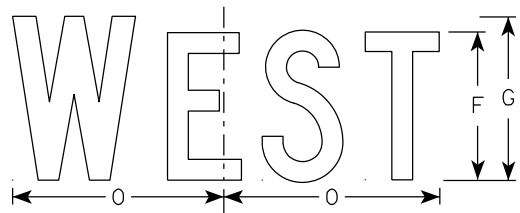
MB3-2
MK3-2
MN3-2



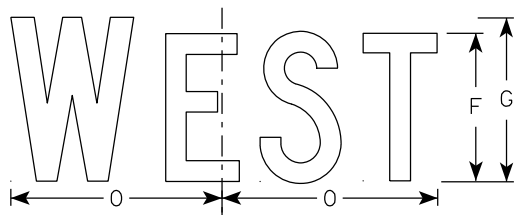
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

NOTES

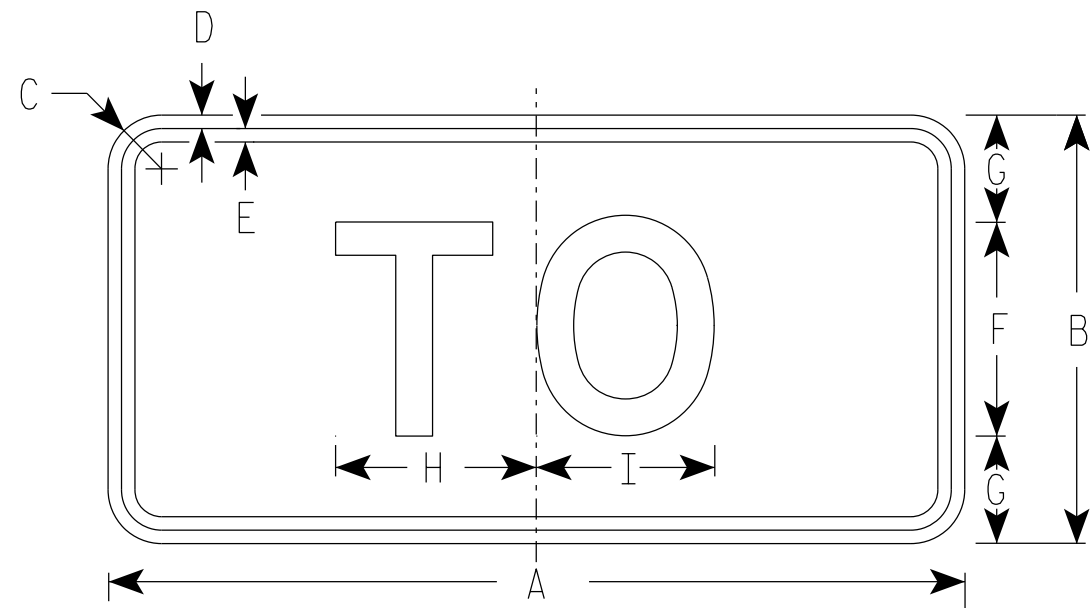
- All Signs Type II - Type H Reflective
- Color:
Background - See note 5
Message - See note 5
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M3-1 thru M3-4 Background - White
Message - Black
MB3-1 thru MB3-4 Background - Blue
Message - White
MK3-1 thru MK3-4 Background - Green
Message - White
MM3-1 thru MM3-4 Background - White
Message - Green
MN3-1 thru MN3-4 Background - Brown
Message - White
MP3-1 thru MP3-4 Background - White
Message - Blue
- Note the first letter of each direction is larger than the remainder of the message.

7

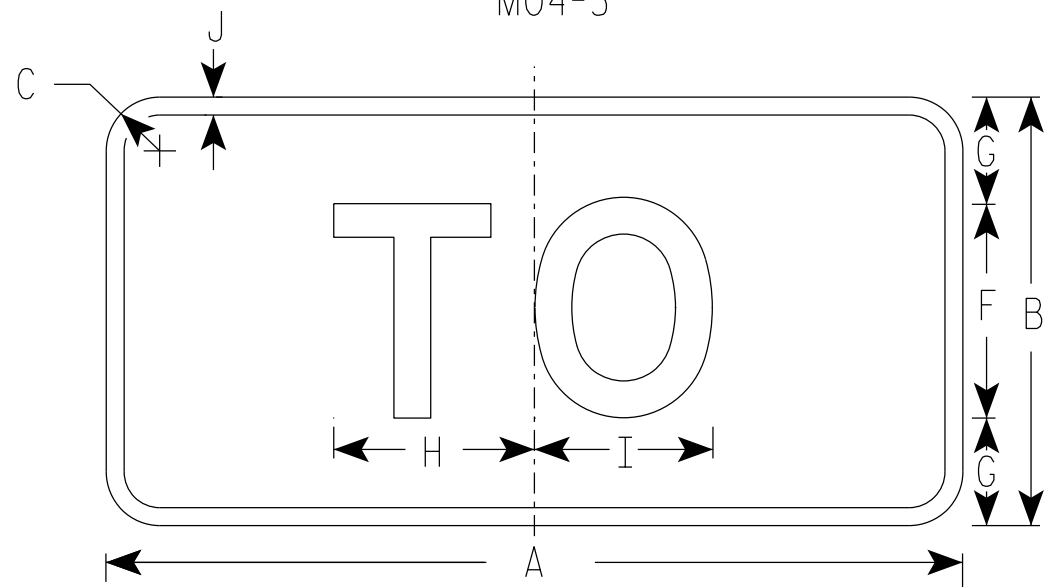
7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4												2.00
2M	24	12	1 1/2	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4												2.00
3	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5
4	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5
5	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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M4-5
MM4-5
MP4-5
M04-5



MB4-5
MK4-5
MN4-5

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - See note 5
Message - See note 5
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-5 Background - White
Message - Black
MB4-5 Background - Blue
Message - White
MK4-5 Background - Green
Message - White
MM4-5 Background - White
Message - Green
MN4-5 Background - Brown
Message - White
MP4-5 Background - White
Message - Blue
M04-5 Background - Orange Type F Reflective
Message - Black

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	3/8	3/8	6	3	5 3/8	5 1/4	1/2																	2.00
2M	24	12	1 1/2	3/8	3/8	6	3	5 3/8	5 1/4	1/2																	2.00
3	36	18	1 1/2	3/8	1/2	9	4 1/2	8 1/4	8 3/8	1/2																	4.5
4	36	18	1 1/2	3/8	1/2	9	4 1/2	8 1/4	8 3/8	1/2																	4.5
5	36	18	1 1/2	3/8	1/2	9	4 1/2	8 1/4	8 3/8	1/2																	4.5

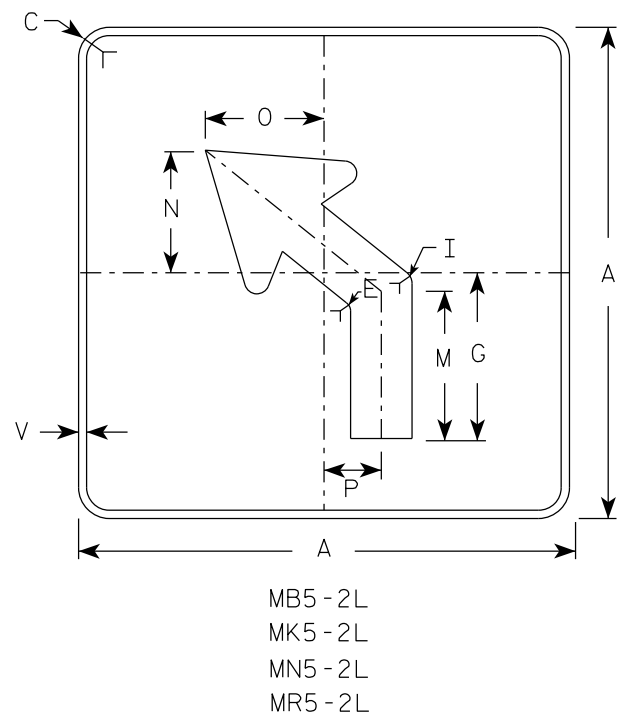
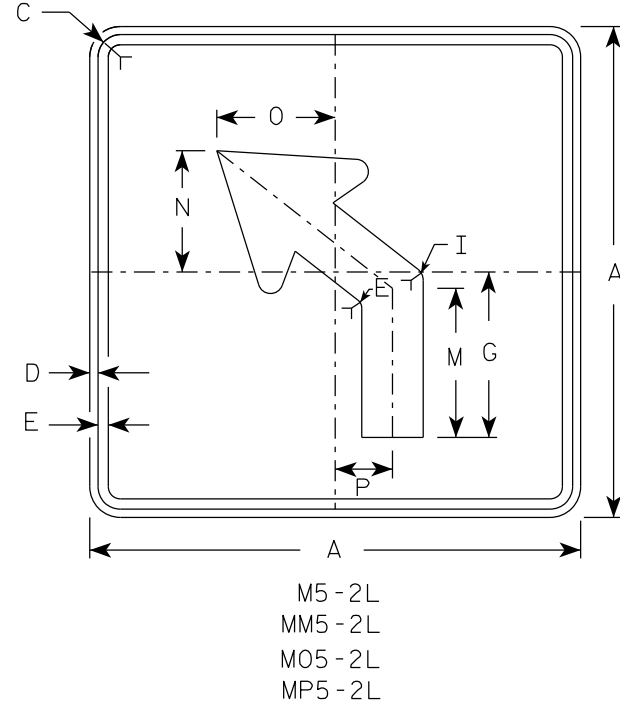
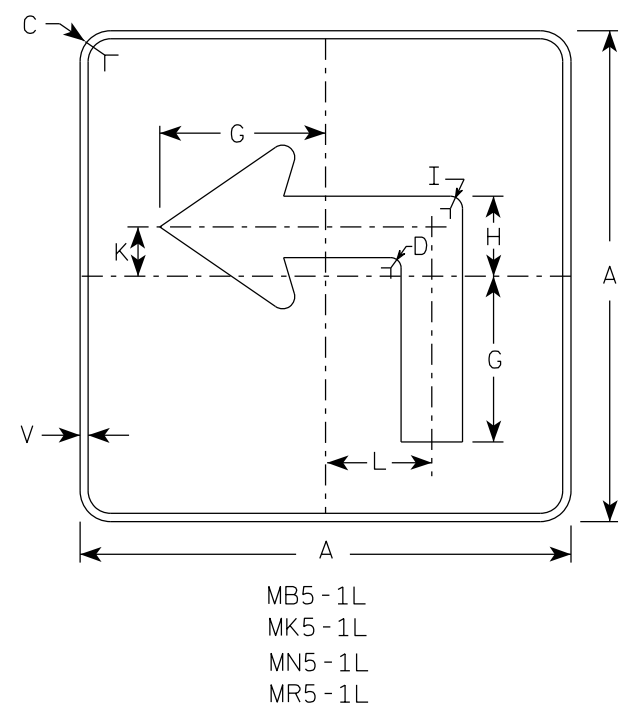
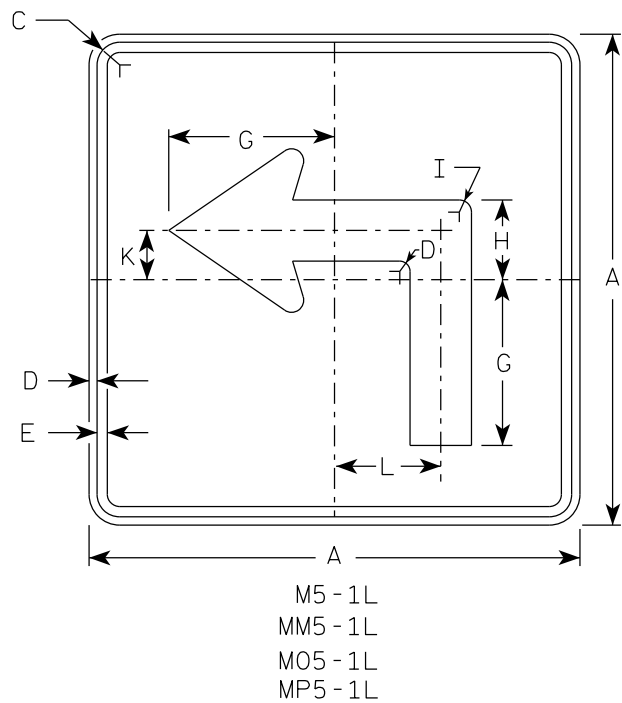
PROJECT NO:		HWY:		COUNTY:		SHEET NO:		E
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STANDARD SIGN
M4-5

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

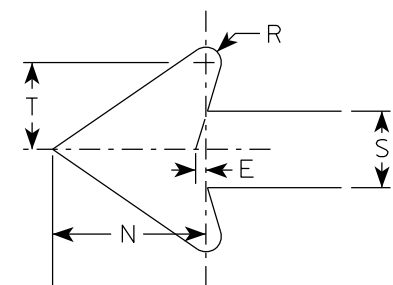
DATE 2/8/2023 PLATE NO. M4-5.11



NOTES

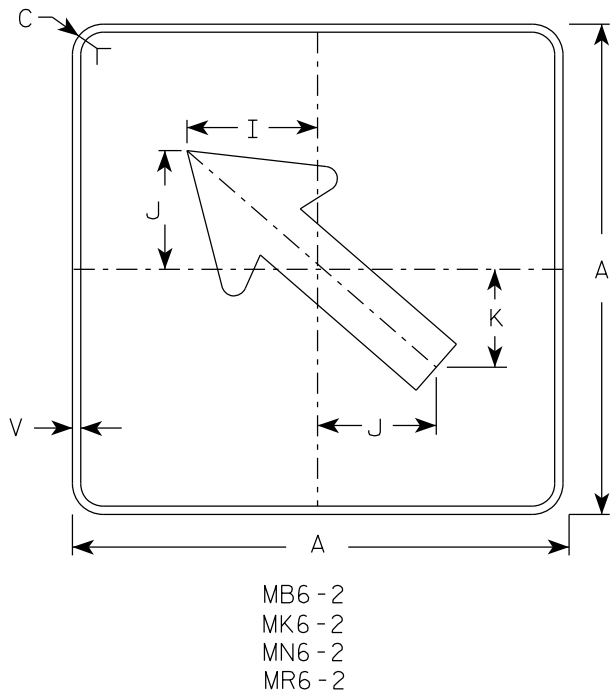
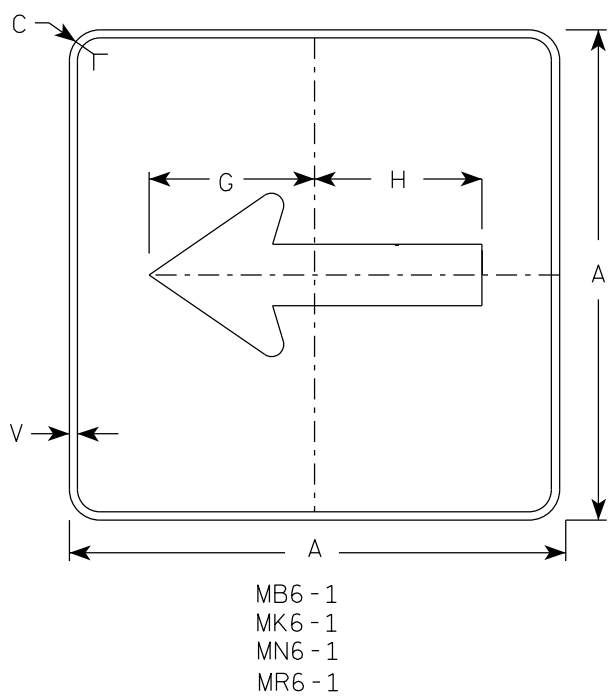
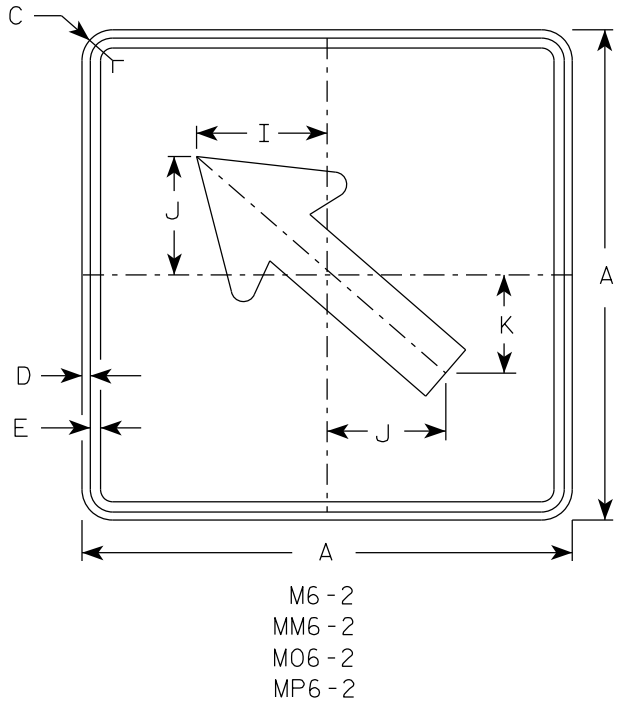
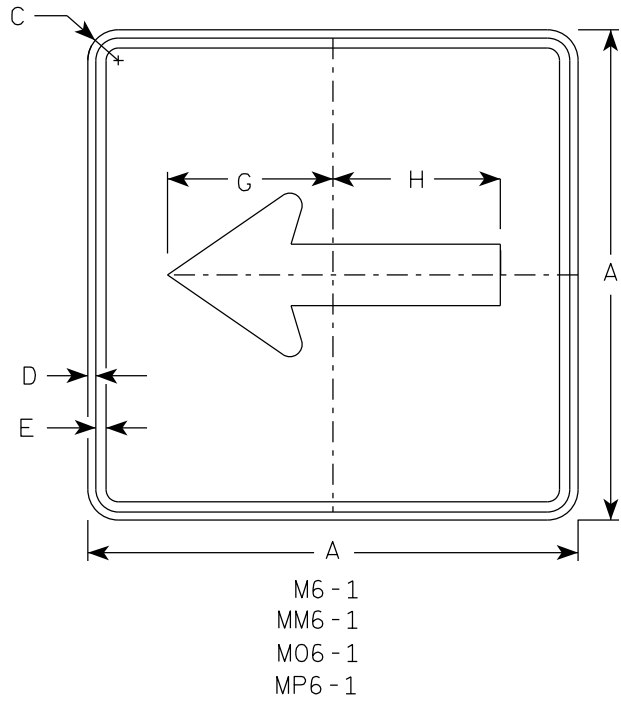
- Signs are Type II - Type H reflective except as shown
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- | | | |
|-----------|-------|---|
| M5-1 and | M5-2 | Background - White |
| | | Message - Black |
| MB5-1 and | MB5-2 | Background - Blue |
| | | Message - White |
| MK5-1 and | MK5-2 | Background - Green |
| | | Message - White |
| MM5-1 and | MM5-2 | Background - White |
| | | Message - Green |
| MN5-1 and | MN5-2 | Background - Brown |
| | | Message - White |
| M05-1 and | M05-2 | Background - Orange - Type F Reflective |
| | | Message - Black |
| MP5-1 and | MP5-2 | Background - White |
| | | Message - Blue |
| MR5-1 and | MR5-2 | Background - Brown |
| | | Message - Yellow |
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

ARROW DETAIL

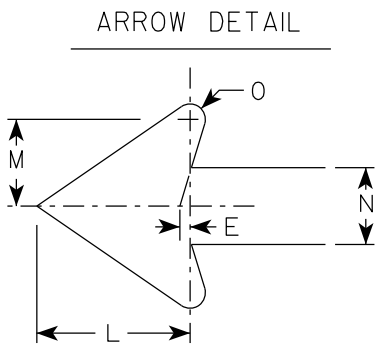


SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	21		1 1/2	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3		1/2					3.06
2M	21		1 1/2	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3		1/2					3.06
3	30		1 7/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4		1/2					6.25
4	30		1 7/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4		1/2					6.25
5	30		1 7/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4		1/2					6.25

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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- NOTES**
- Signs are Type II - Type H Reflective except as Shown
 - Color:
Background - See note 4
Message - See note 4
 - Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
 - M6-1 and M6-2 Background - White
Message - Black
MB6-1 and MB6-2 Background - Blue
Message - White
MK6-1 and MK6-2 Background - Green
Message - White
MM6-1 and MM6-2 Background - White
Message - Green
MN6-1 and MN6-2 Background - Brown
Message - White
M06-1 and M06-2 Background - Orange - Type F Reflective
Message - Black
MP6-1 and MP6-2 Background - White
Message - Blue
MR6-1 and MR6-2 Background - Brown
Message - Yellow



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	
1																												
2S	21		1 1/2	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2							1/2						3.06
2M	21		1 1/2	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2							1/2						3.06
3	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2						6.25
4	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2						6.25
5	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2						6.25

PROJECT NO:

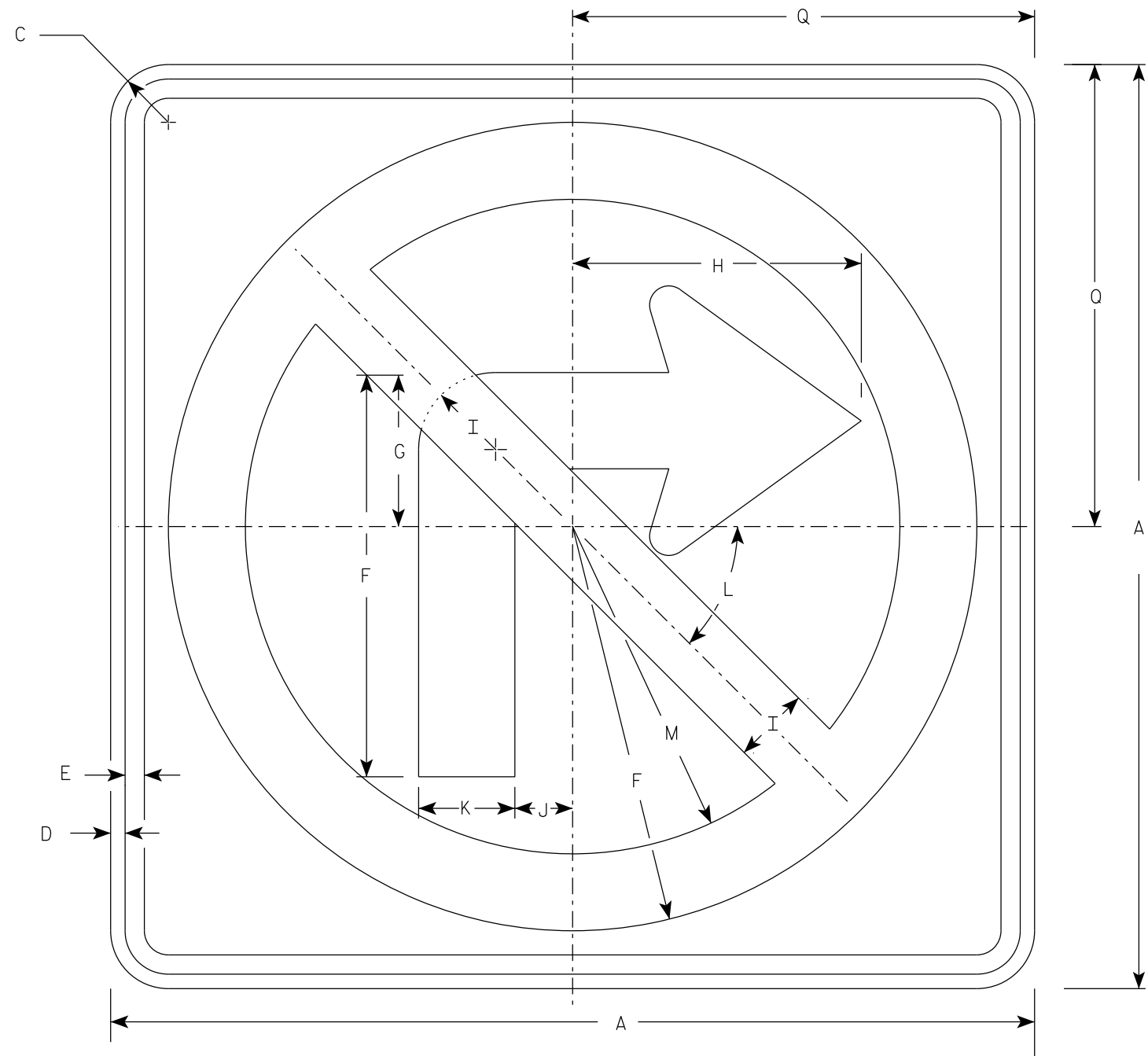
HWY:

COUNTY:

SHEET NO:

E

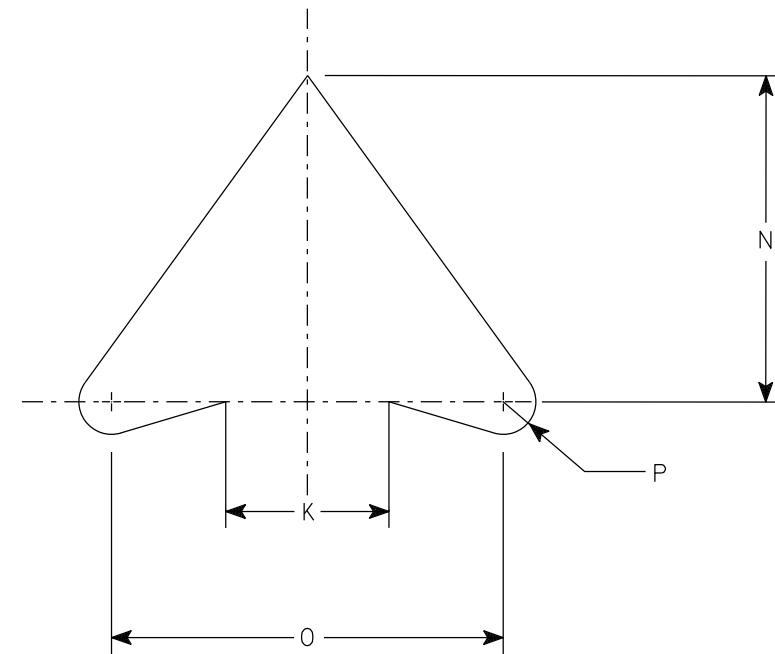
7



R3-1

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - See note 3
3. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.

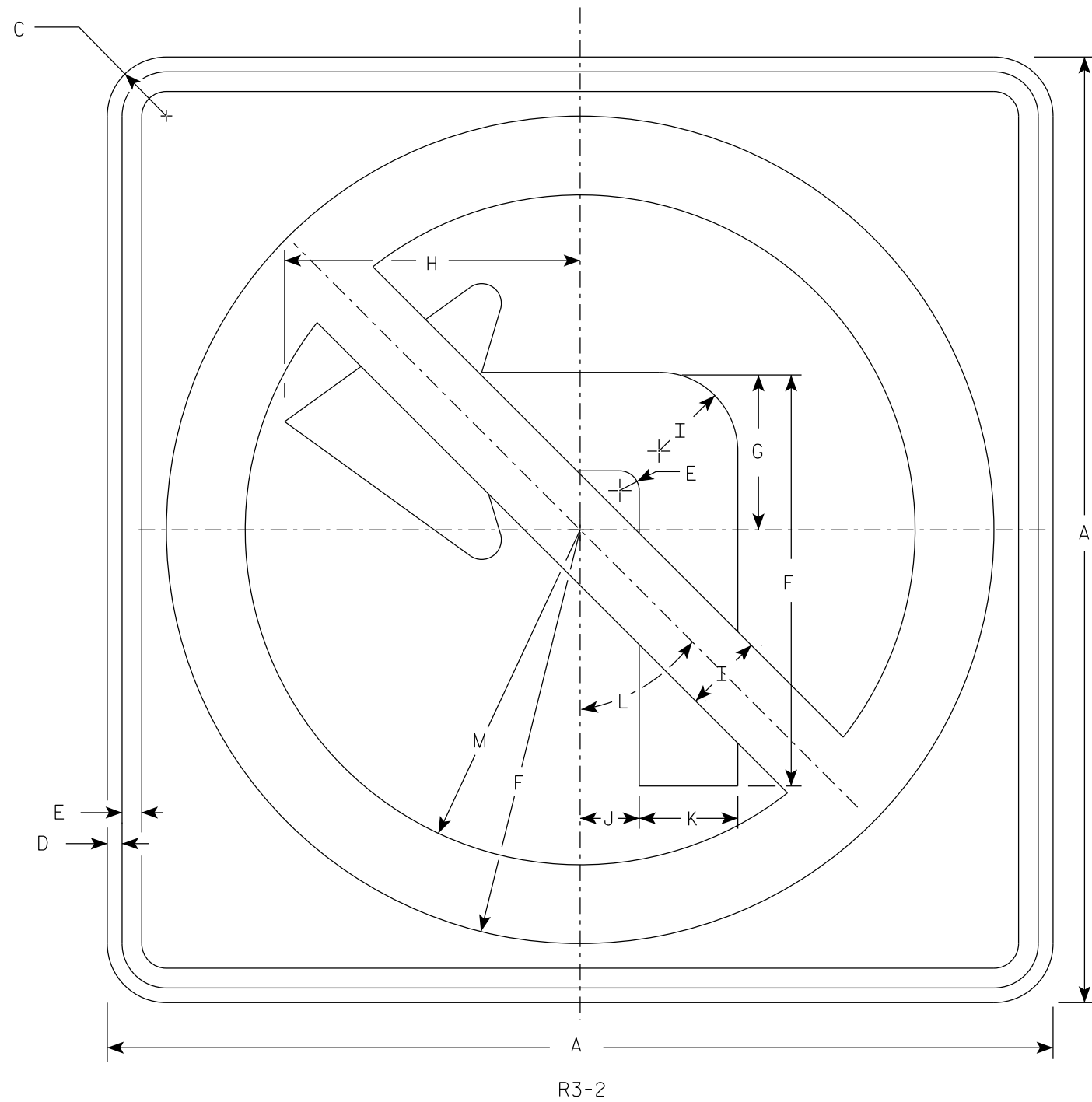


ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/2	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45	8 1/2	5	6	1/2	12										4.0
2S	24		1 1/2	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2	12										4.0
2M	36		2 1/4	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45	12 3/4	7 1/2	9	3/4	18										9.0
3	36		2 1/4	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45	12 3/4	7 1/2	9	3/4	18										9.0
4	36		2 1/4	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4	18										9.0
5	48		2 1/4	3/4	1	21	8	15	4	3	5	45°	17	10	12	1	24										16.0

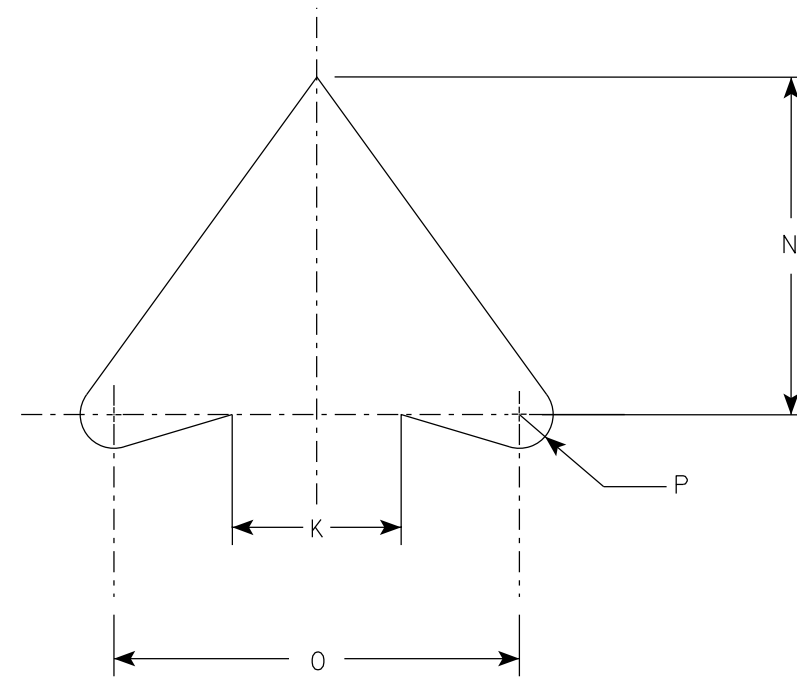
PROJECT NO:	HWY:	COUNTY:	SHEET NO:												E
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7



NOTES

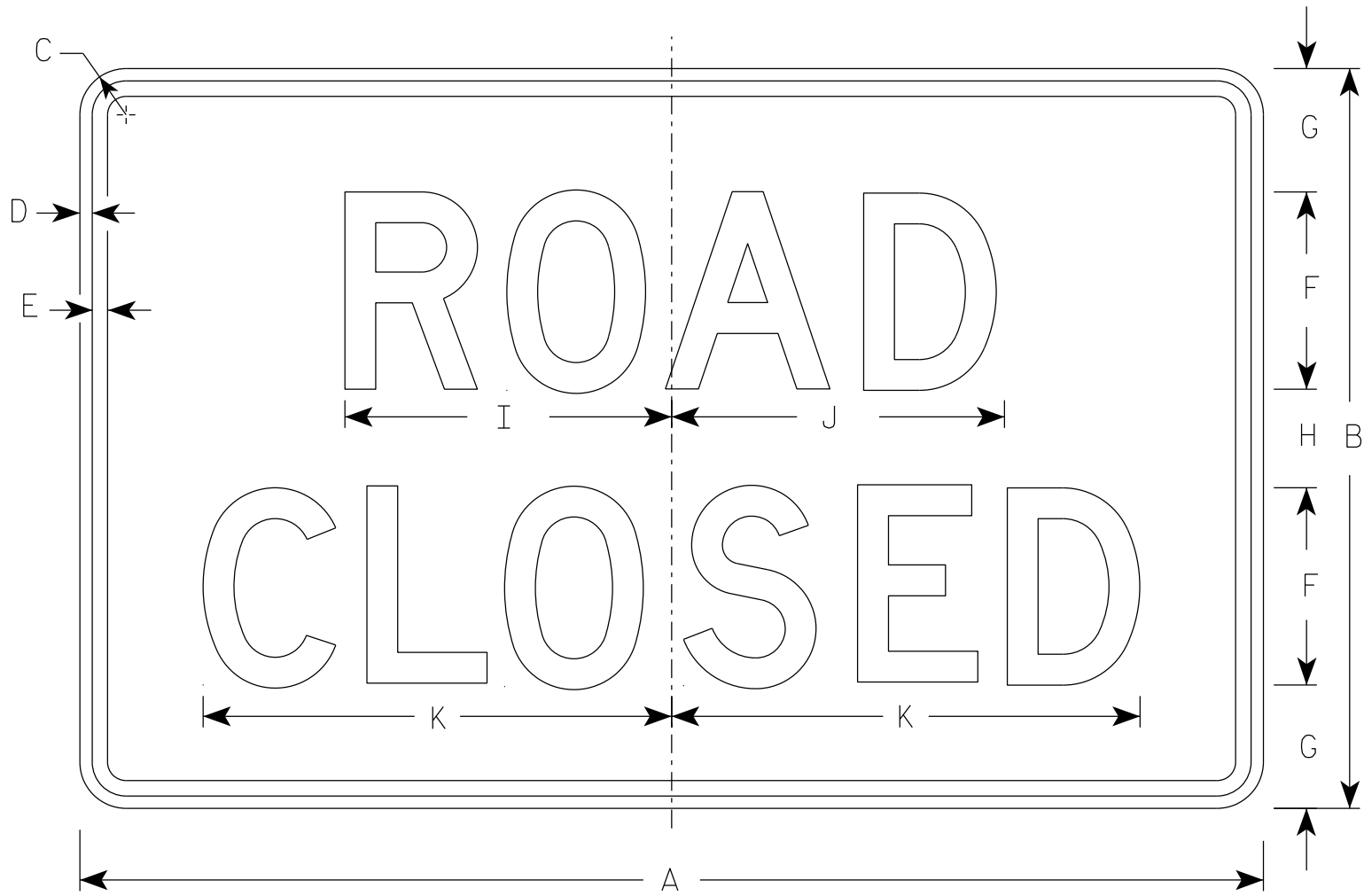
1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - See note 3
3. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

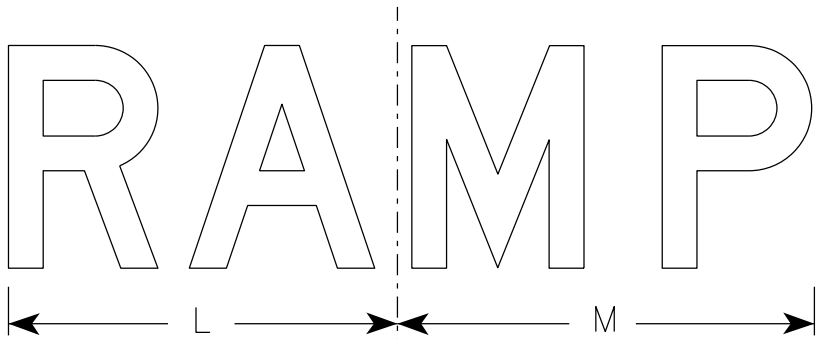
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/2	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2											4.0
2S	24		1 1/2	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2											4.0
2M	36		2 1/4	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
3	36		2 1/4	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
4	36		2 1/4	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
5	48		2 1/4	3/4	1	21	8	15	4	3	5	45°	17	10	12	1											16.0

PROJECT NO:		HWY:		COUNTY:		SHEET NO:		E
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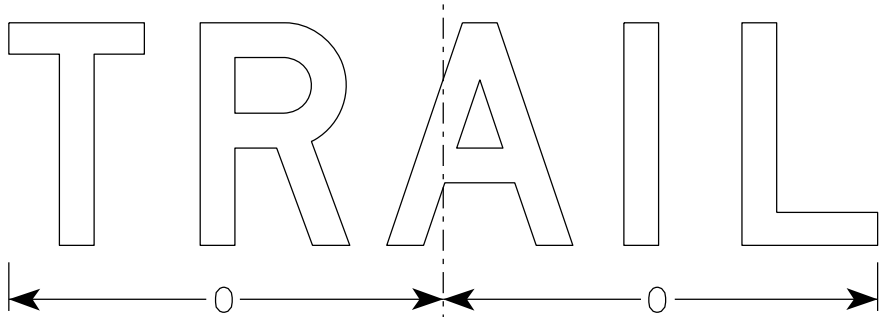


R11-2

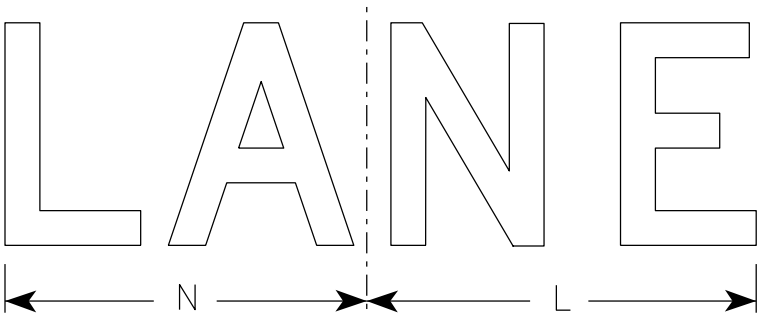
- NOTES
1. Sign is Type II - Type H Reflective
 2. Color:
Background - White
Message - Black
 3. Message Series - D
 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
 5. Modify the message as required.



R11-2R



R11-2T

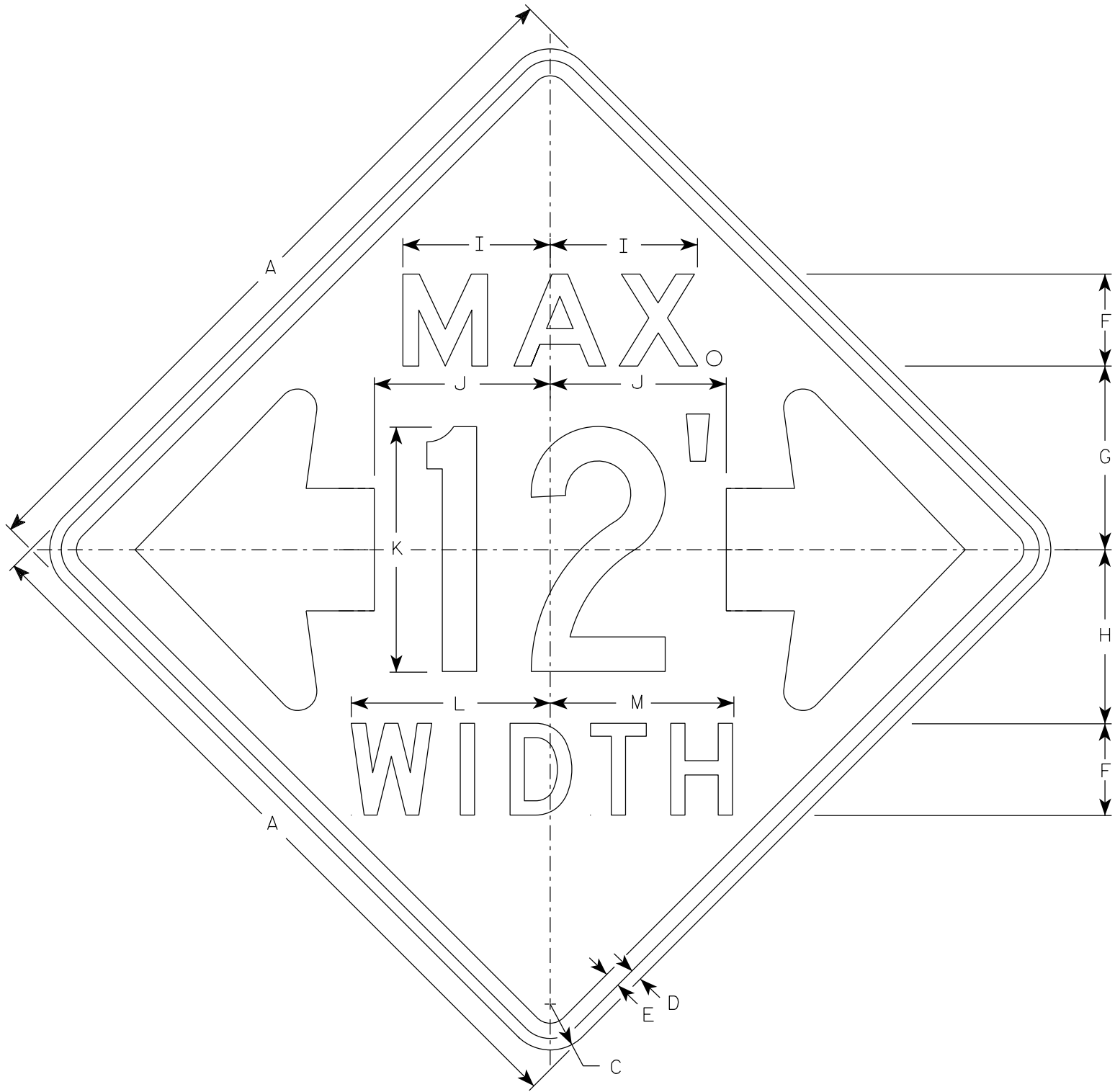


R11-2L

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
2M	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
3	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
4	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
5	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0

STANDARD SIGN R11-2	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 2/5/24	PLATE NO. R11-2.12

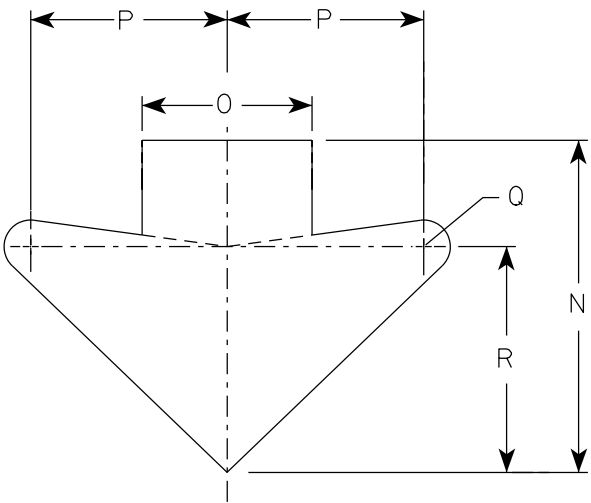
PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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W12-52

NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:
Background - Orange
Message - Black
- 3. Message Series - See note 5
- 4. The top line is series E, the numerals are series C, and the bottom line is series D.
- 5. Substitute appropriate numerals and adjust spacing as required.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48		3	$\frac{3}{4}$	1	6	12	$11\frac{3}{8}$	$9\frac{5}{8}$	$11\frac{1}{2}$	16	13	12	$15\frac{5}{8}$	8	$9\frac{1}{4}$	$1\frac{1}{4}$	$10\frac{5}{8}$								16.0	
2M	48		3	$\frac{3}{4}$	1	6	12	$11\frac{3}{8}$	$9\frac{5}{8}$	$11\frac{1}{2}$	16	13	12	$15\frac{5}{8}$	8	$9\frac{1}{4}$	$1\frac{1}{4}$	$10\frac{5}{8}$								16.0	
3																											
4																											
5																											

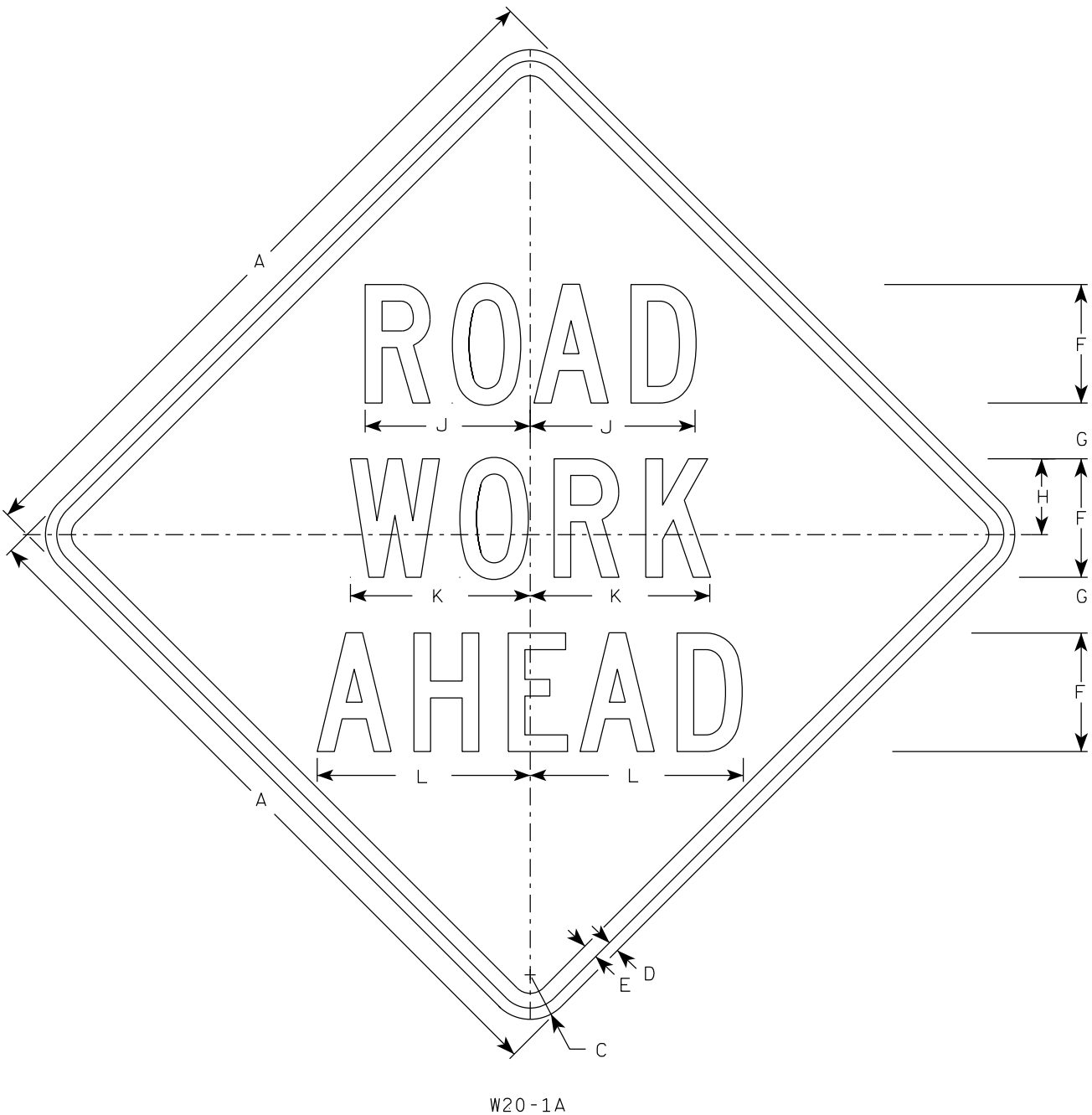
STANDARD SIGN

W12-52

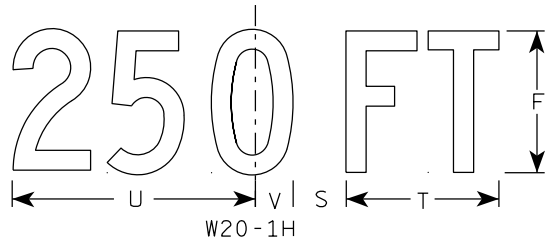
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

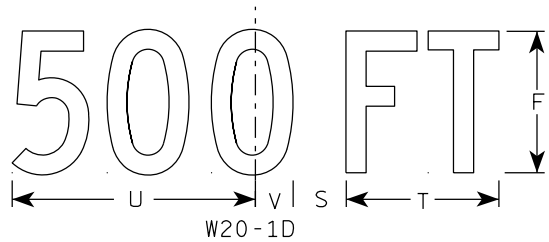
DATE 3/10/2024 PLATE NO. W12-52.8



W20-1A



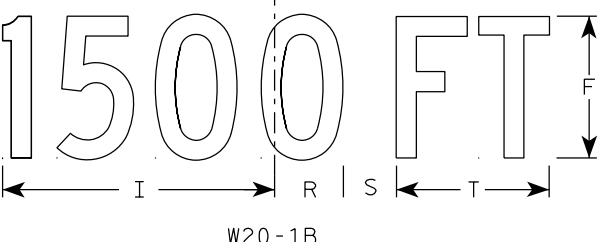
W20-1H



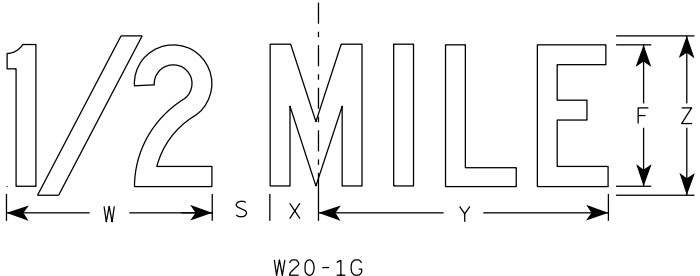
W20-1D



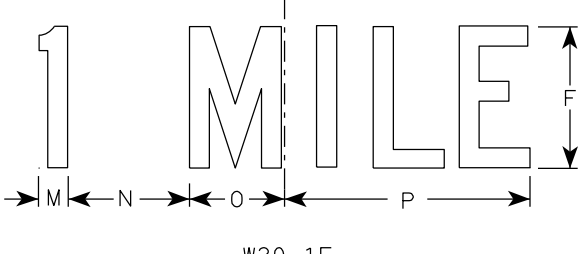
W20-1C



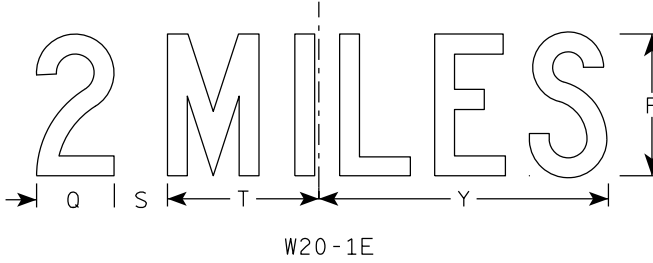
W20-1B



W20-1G



W20-1F



W20-1E

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

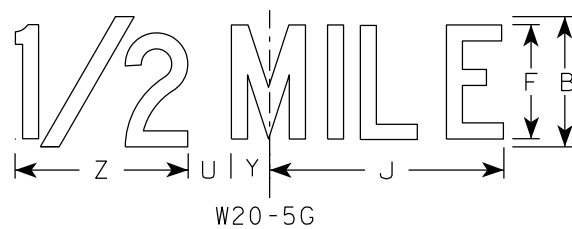
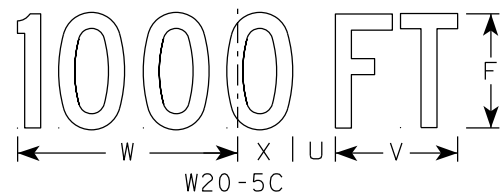
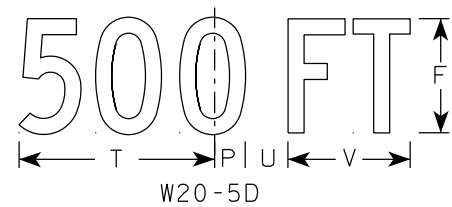
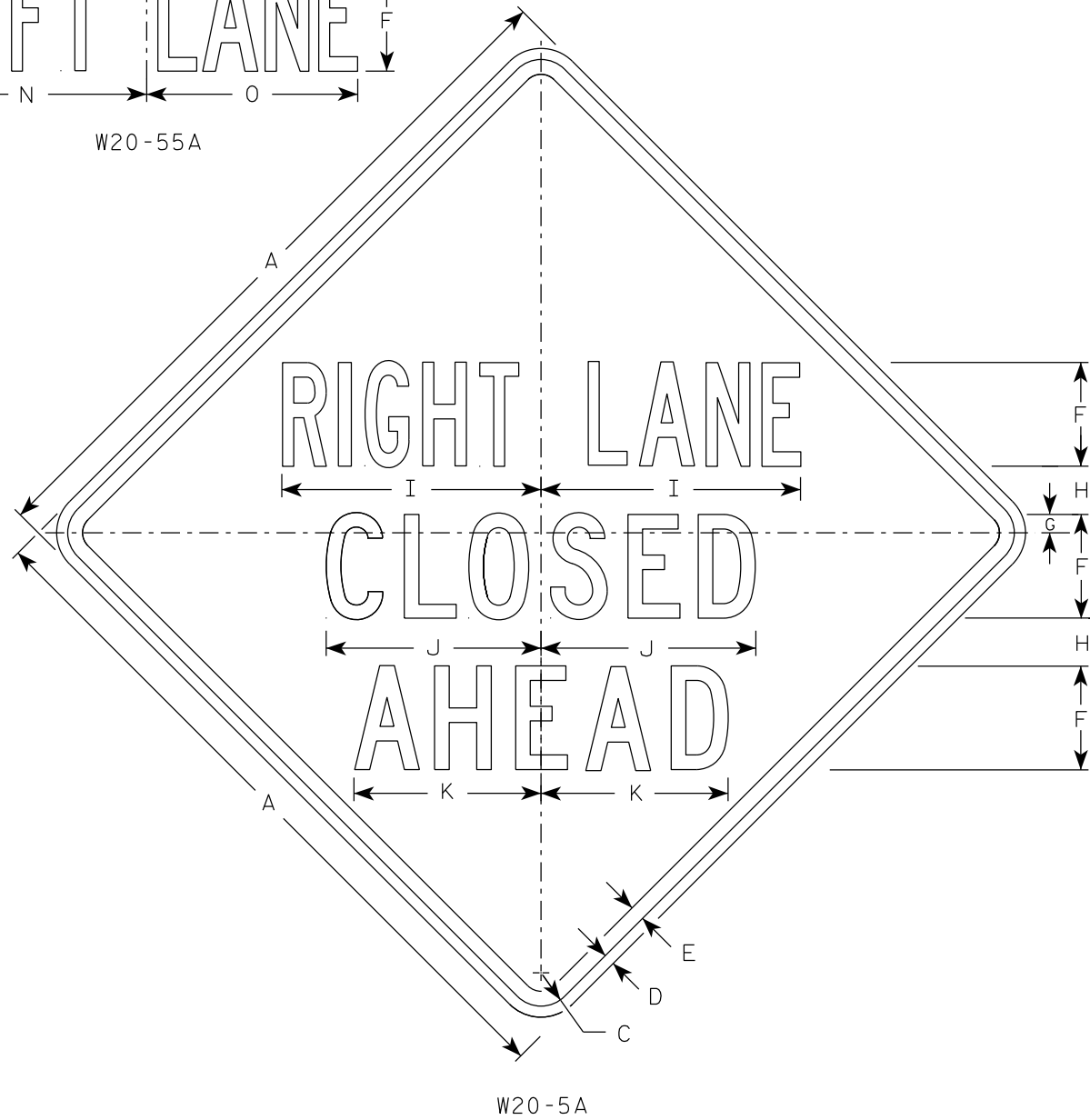
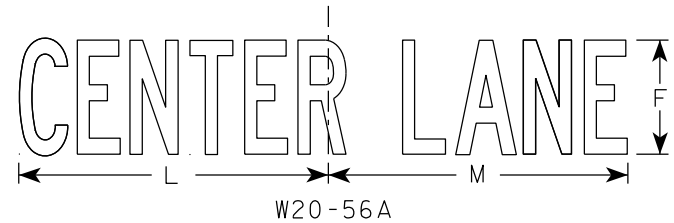
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	v	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9	3 1/4	2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN
W20-1A, B, C, D, E, F, G & H

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/10/2024 PLATE NO. W20-1.12



NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:
 - Background - Orange
 - Message - Black
- 3. Message Series - See Note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. " _____ LANE" is Series B. All other copy is Series C.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	v	W	X	Y	Z	Area sq. ft.
1	36	6	2 1/4	5/8	3/4	5	7/8	2 1/2	13 1/8	10 3/4	9 1/2	14 1/4	13 5/8	12	12	1 3/8	1 1/8	4 1/2	3 1/2	9	1 7/8	5 5/8	10 1/8	2 1/2	1 3/4	8	9.0
2S	48	8	3	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
2M	48	8	3	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
3	48	8	3	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
4	48	8	3	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
5	48	8	3	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0

STANDARD SIGN

W20-5A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/27/24 PLATE NO. W20-5.12

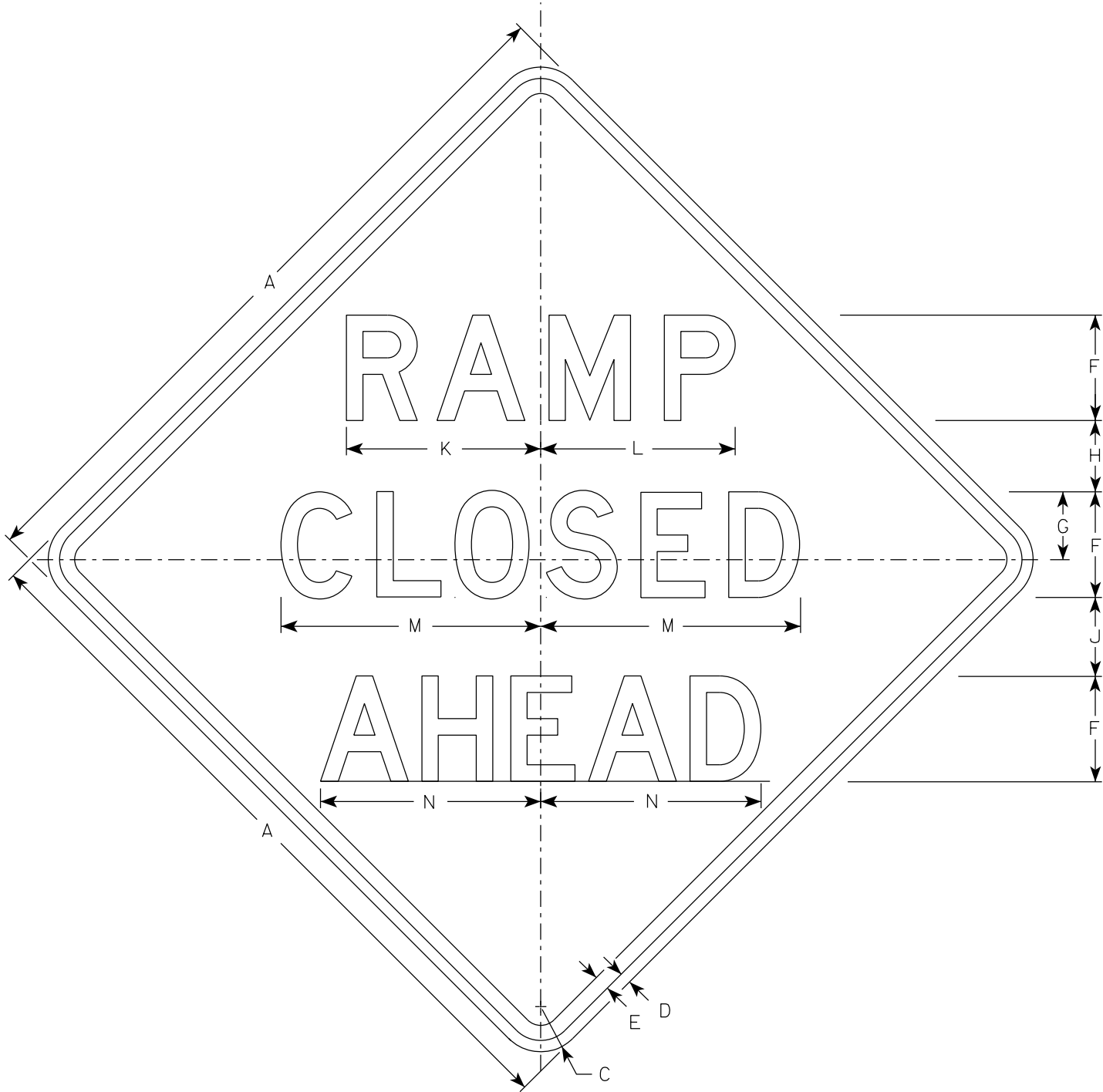
PROJECT NO:

HWY:

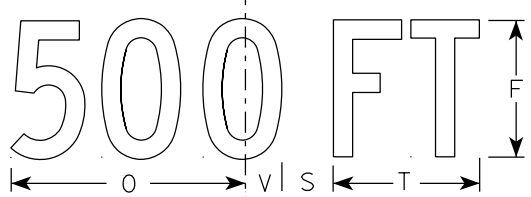
COUNTY:

SHEET NO:

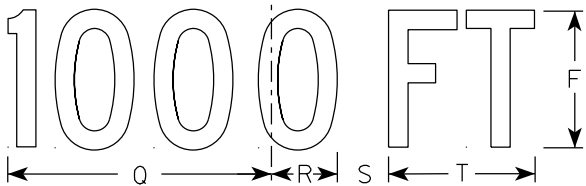
E



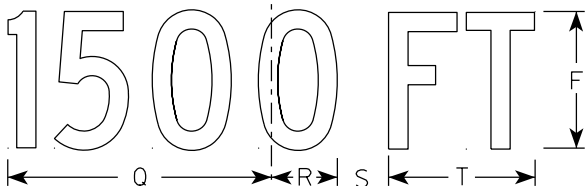
W20-53A



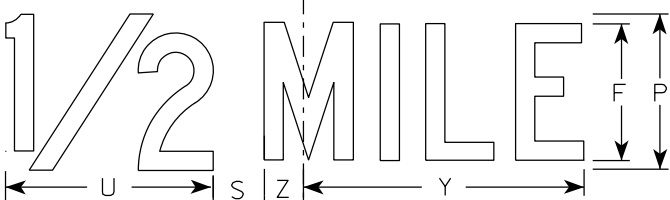
W20-53D



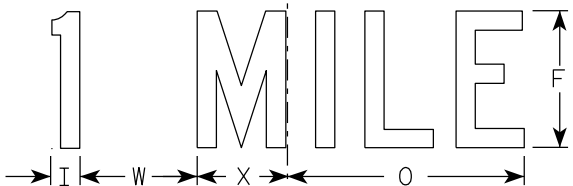
W20-53C



W20-53B



W20-53G



W20-53F

NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:
Background - Orange
Message - Black
- 3. Message Series - see note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Lines 1 and 2 are Series D.
Line 3 is Series D for AHEAD and Series C for all other distances.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	5	3 3/8	3 1/2	1 1/8	4	9 1/4	9 1/4	12 1/2	11	9	6	10 1/8	2 1/2	1 7/8	5 5/8	8	1 3/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	12 7/8	12 7/8	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	12 7/8	12 7/8	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
3	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	12 7/8	12 7/8	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
4	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	12 7/8	12 7/8	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
5	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	12 7/8	12 7/8	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0

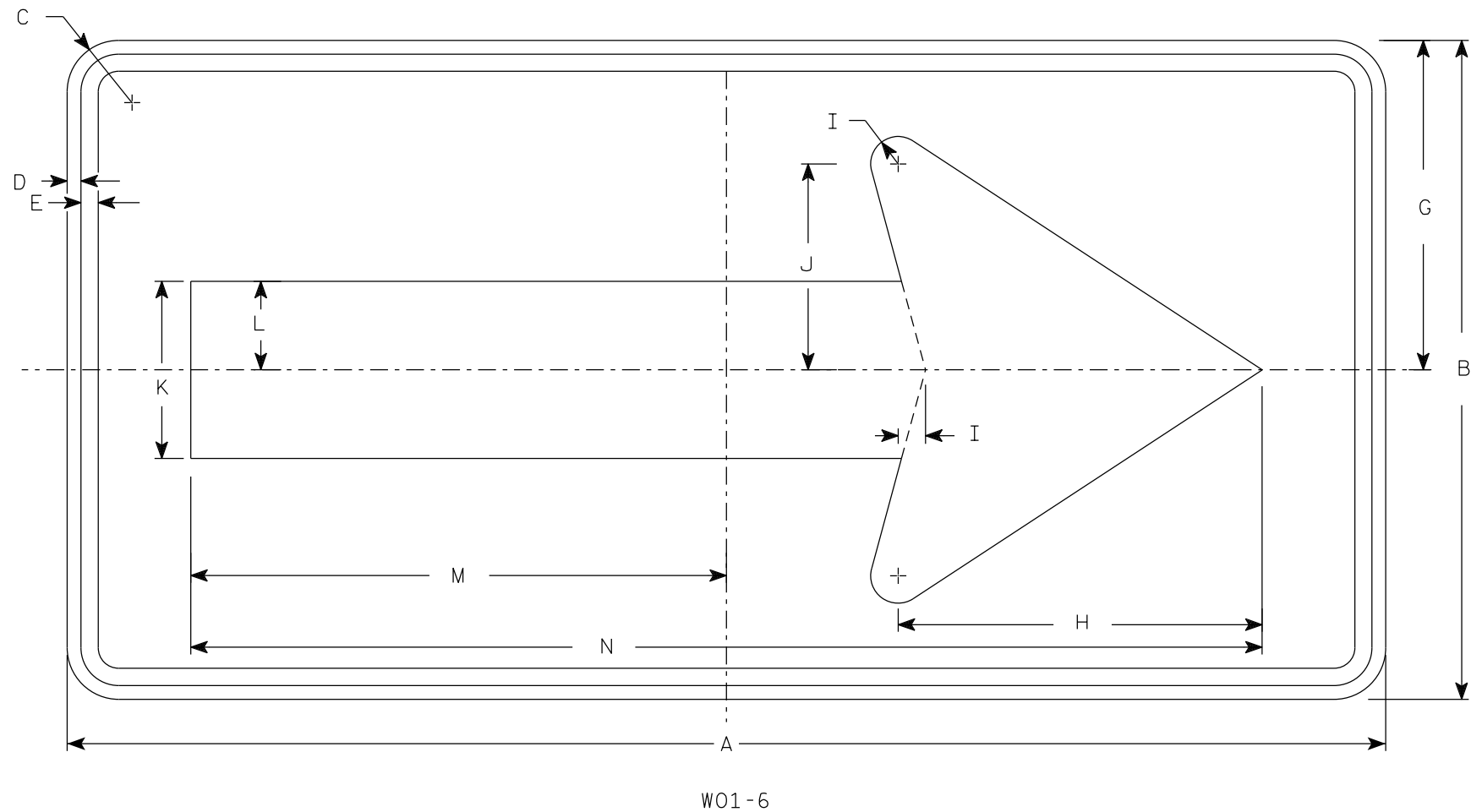
STANDARD SIGN
W20-53A,B,C,D,F,G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 1/10/2024 PLATE NO. W20-53.2

7



NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:
Background - Orange
Message - Black
- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

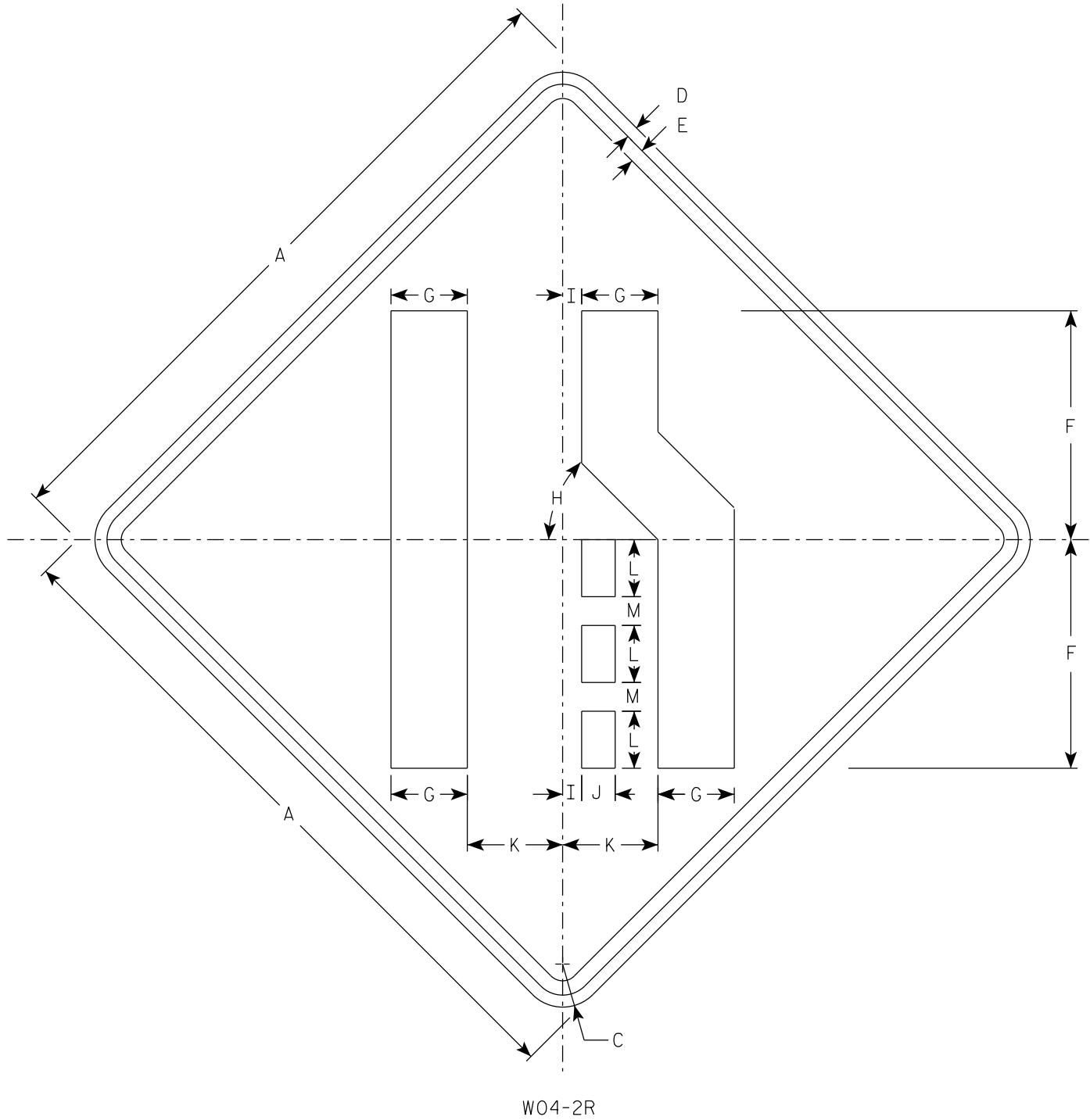
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	24	1 7⁄8	1⁄2	5⁄8		12	13 1⁄4	1	7 1⁄2	6 1⁄2	3 1⁄4	19 1⁄2	39													8.0
2M	48	24	1 7⁄8	1⁄2	5⁄8		12	13 1⁄4	1	7 1⁄2	6 1⁄2	3 1⁄4	19 1⁄2	39													8.0
3	60	30	1 7⁄8	1⁄2	5⁄8		15	16 1⁄4	1 1⁄4	9 1⁄4	8	4	24 3⁄8	48 3⁄4													12.5
4	60	30	1 7⁄8	1⁄2	5⁄8		15	16 1⁄4	1 1⁄4	9 1⁄4	8	4	24 3⁄8	48 3⁄4													12.5
5	60	30	1 7⁄8	1⁄2	5⁄8		15	16 1⁄4	1 1⁄4	9 1⁄4	8	4	24 3⁄8	48 3⁄4													12.5

STANDARD SIGN
W01-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED
Matthew R. Rauch
for State Traffic Engineer

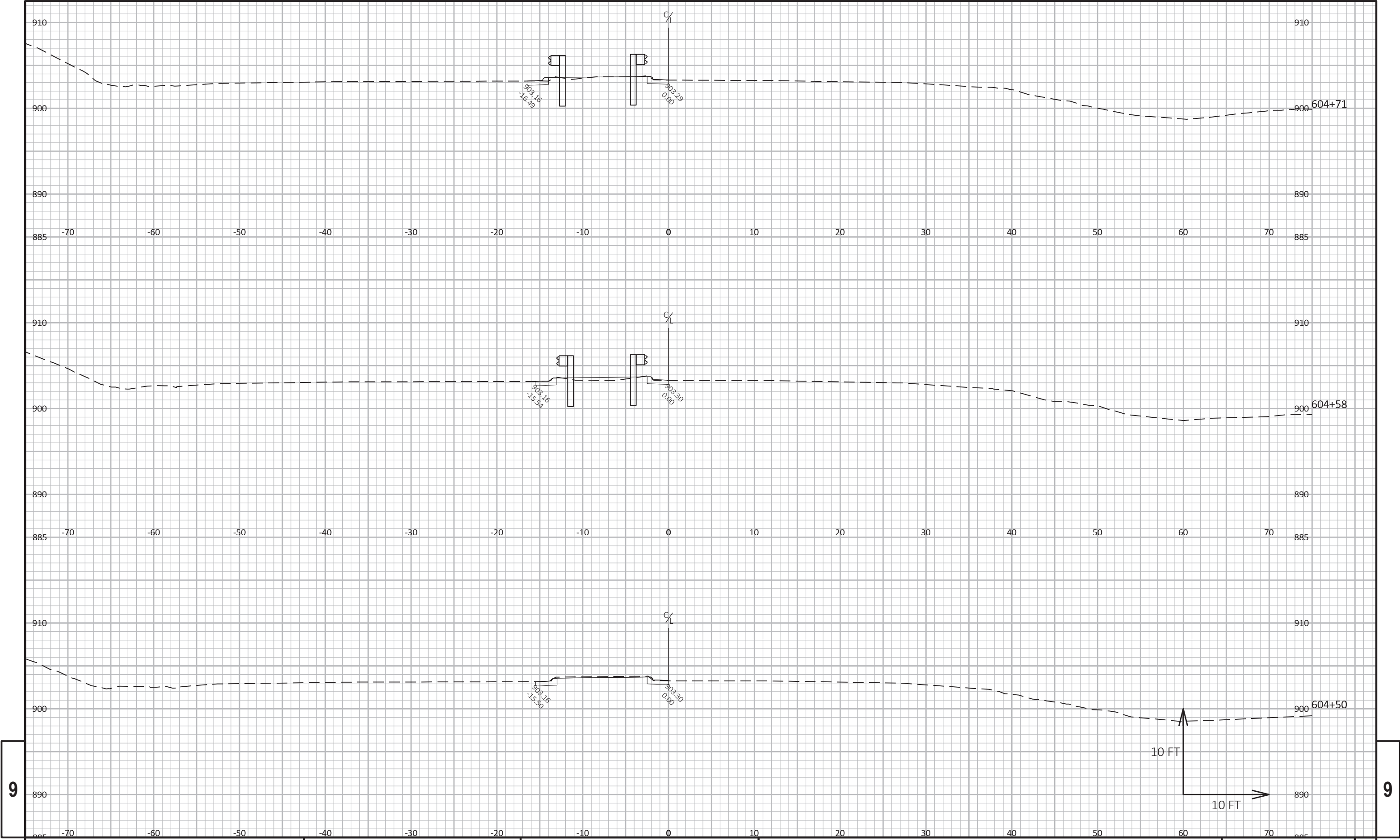
DATE 1/24/2024 PLATE NO. W01-6.2

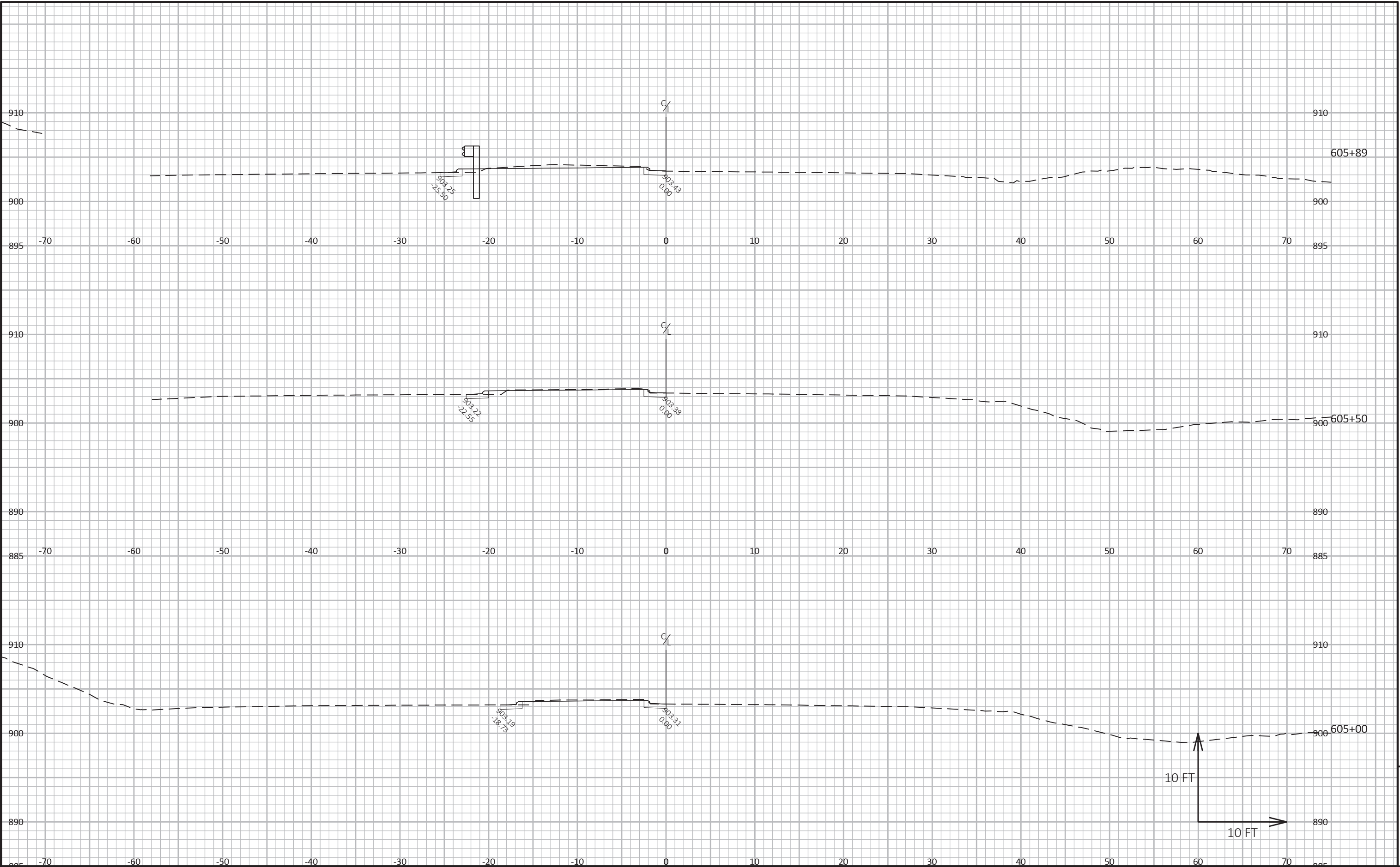


NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W04-2L is the same as W04-2R except the symbol is reversed along the vertical centerline.

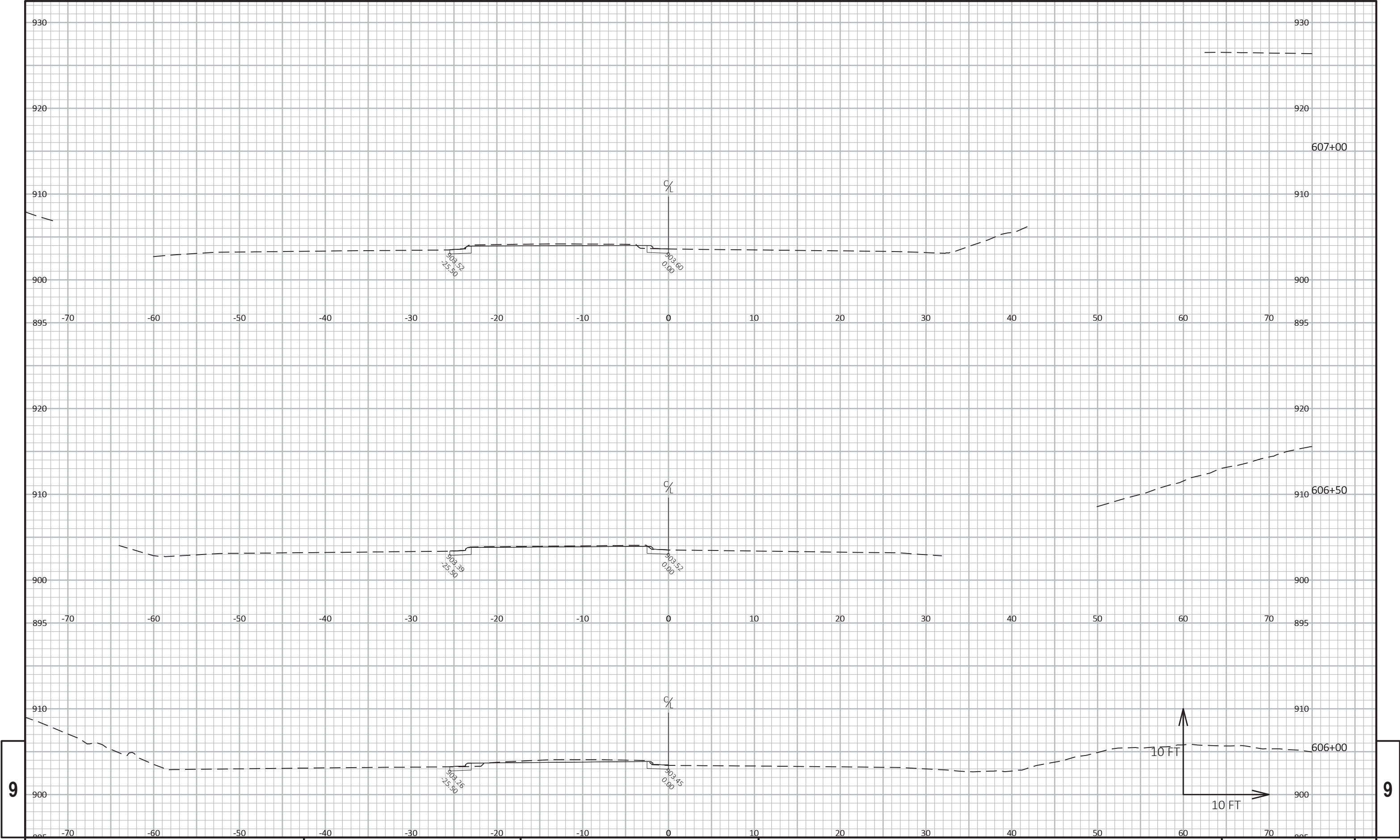
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	12	4	45°	1	1 3/4	5	3	1 1/2														9.0
2S	48		3	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
2M	48		3	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
3	48		3	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
4	48		3	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
5	48		3	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0





9

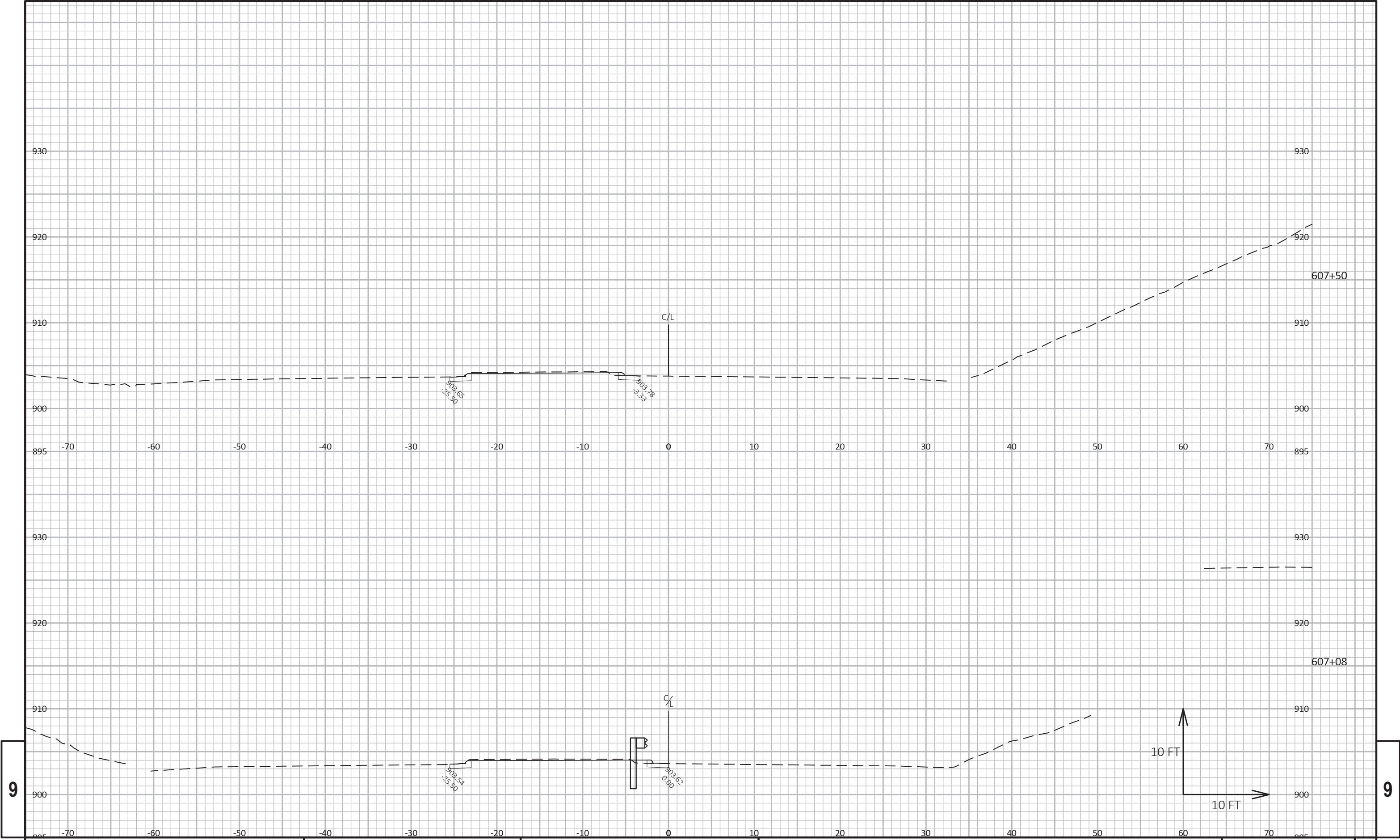
9



9

9

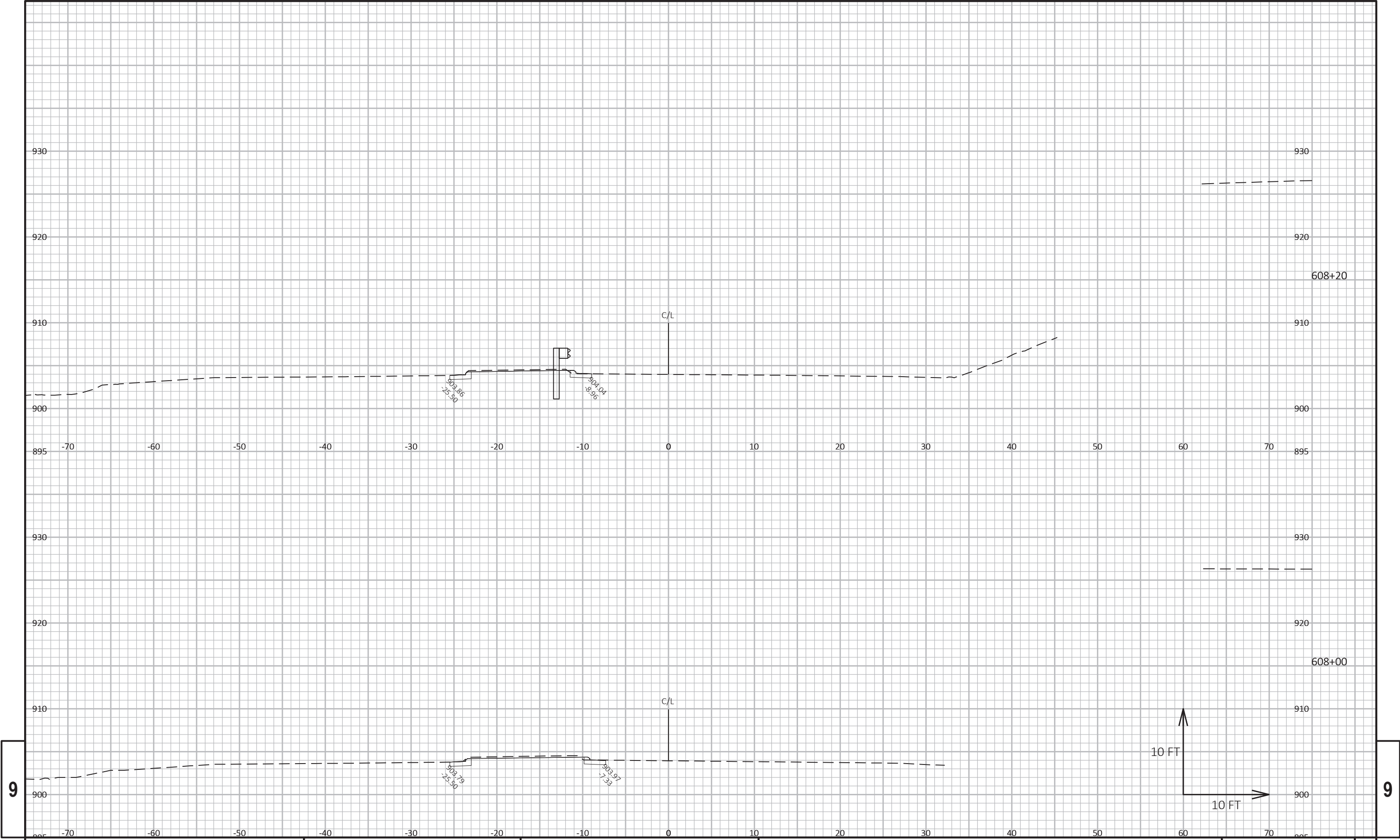
PROJECT NO: XXXX-XX-XX	HWY: XXX	COUNTY: XXX	CROSS SECTIONS: STH 26	SHEET	E
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9

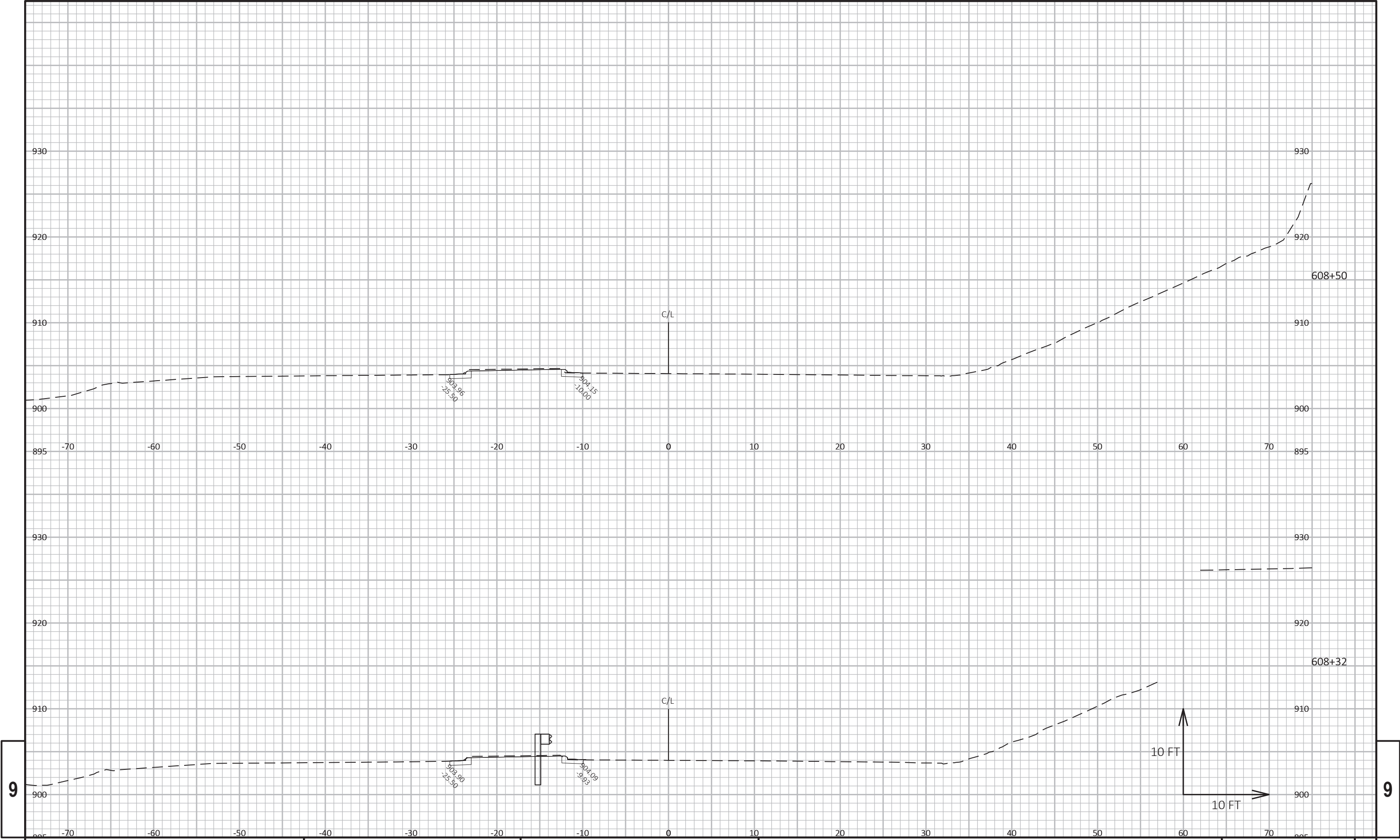
9

PROJECT NO: XXXX-XX-XX	HWY: XXX	COUNTY: XXX	CROSS SECTIONS: STH 26	SHEET	E
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9

9



9

9

PROJECT NO: XXXX-XX-XX			HWY: XXX			COUNTY: XXX			CROSS SECTIONS: STH 26			SHEET			E
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