

LAX
PROJECT ID:
WITH: N/A

5790-02-72

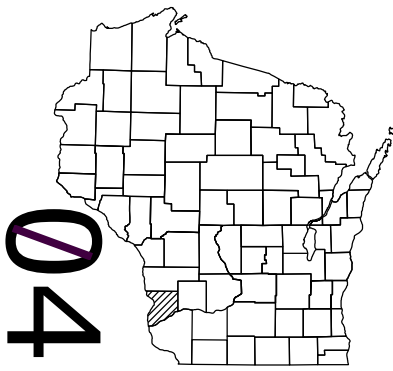
COUNTY:
CRAWFORD

AUGUST 2025

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 406



DESIGN DESIGNATION 5790-02-72

A.A.D.T.	2027	=	890
A.A.D.T.	2047	=	890
D.H.V.		=	89
D.D.		=	50/50
T.		=	10.1%
DESIGN SPEED		=	55 MPH
ESALS		=	190,000

CONVENTIONAL SYMBOLS

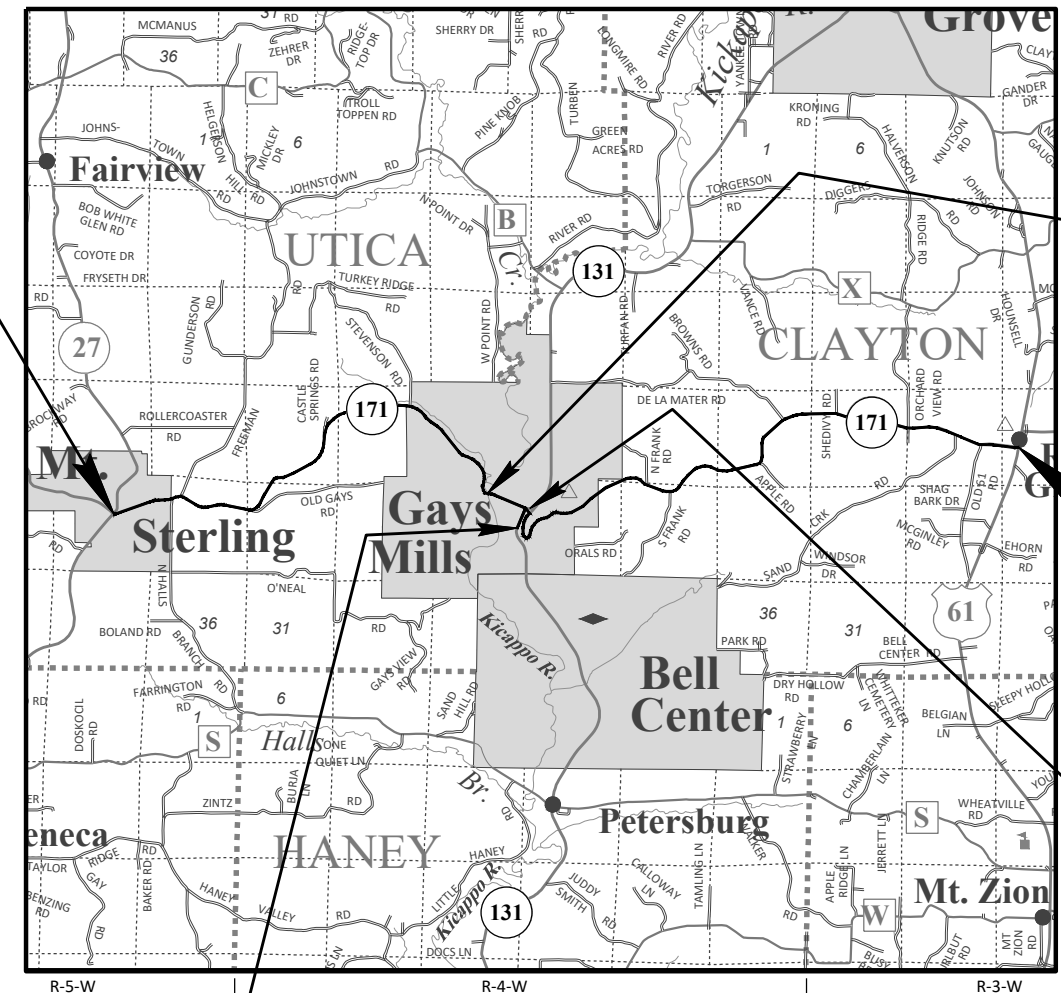
PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
HIGH VOLTAGE	
MARSH AREA	
WOODED OR SHRUB AREA	



PROFILE	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
PLAN OF PROPOSED IMPROVEMENT
FERRYVILLE - ROLLING GROUND
STH 27 TO USH 61
STH 171
CRAWFORD COUNTY

STATE PROJECT NUMBER
5790-02-72



BEGIN PROJECT
STA 2+24
Y=218,257.120
X=375,885.609

EXCEPTION TO NET C/L LENGTH
STA 264+62 - STA 268+90
STRUCTURE B-12-137
STA 264+66
Y=219,612.663
X=396,243.616

END PROJECT
STA 617+79
Y=222,506.717
X=425,443.655

END CONSTRUCTION
STH 131
STA 394+75'C
Y=218,787.290
X=398,569.750

BEGIN CONSTRUCTION
STH 131
STA 382+98'C
Y=217,857.197
X=398,036.438

SCALE 0 2 MI
TOTAL NET LENGTH OF CENTERLINE = 11.769 MI

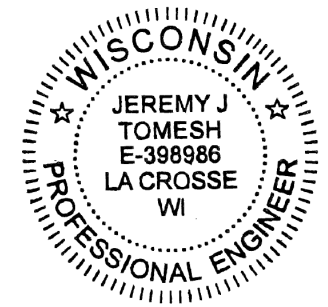
HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), CRAWFORD COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5790-02-72		

ORIGINAL PLANS PREPARED BY

Short Elliott Hendrickson Inc.
329 Jay Street, Suite 301
La Crosse, WI 54601-4007
608.782.3161 main | 888.908.8166 fax
www.sehinc.com



4/23/25
(Date) *Jeremy J. Tomesh*
(Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	SEH
Designer	SEH
Project Manager	JERED LEX
Regional Examiner	SW REGION
Regional Supervisor	BRIAN MEYER

APPROVED FOR THE DEPARTMENT

DATE: _____
Jered Lex
(Signature)

E

WISDOT CONTACT:

WISCONSIN DEPT OF TRANSPORTATION
SOUTHWEST REGION
3550 MORMON COULEE ROAD
LA CROSSE, WI 54601
TELEPHONE: 608.785.9956
ATTENTION: JERED LEX, P.E.
EMAIL: JERED.LEX@DOT.WI.GOV

DESIGN CONTACT:

SEH INC.
329 JAY STREET, SUITE 301
LA CROSSE, WI 54601
TELEPHONE: 608.498.4947
ATTENTION: JEREMY TOMESH, P.E.
PROJECT MANAGER
EMAIL: JTOMESH@SEHINC.COM

DNR AREA LIAISON:

WI DEPT OF NATURAL RESOURCES
DNR SERVICE CENTER
3550 MORMON COULEE RD
LA CROSSE, WI 54601
TELEPHONE: 608.785.9115
ATTENTION: KAREN KALVELAGE
EMAIL: KAREN.KALVELAGE@WISCONSIN.GOV

UTILITY CONTACT LIST:

ATC MANAGEMENT INC - ELECTRICITY
ATTENTION: DOUG VOSBERG
2489 RINDEN ROAD
COTTAGE GROVE, WI 53527
PHONE: 608.877.7650
EMAIL: DVOSBERG@ATCLLC.COM

ALLIANT ENERGY - ELECTRICITY
ATTENTION: AL MUNN
2200 E CAMPION BLVD
PRAIRIE DU CHIEN, WI 53821
PHONE: 608.326.9481
EMAIL: ALLANMUMM@ALLIANTENERGY.COM

BRIGHTSPEED OF WESTERN WISCONSIN LLC-COMMUNICATION
ATTENTION: DOUG MCGOWAN
135 N BONSON ST
PLATTEVILLE, WI 53818
PHONE: 608.342.4316
EMAIL: DOUG.MCGOWAN1@BRIGHTSPEED.COM

CENTURYLINK (LUMEN) - COMMUNICATION
ATTENTION: KYLE SCHLAMPP
20 S WILSON AVE
RICE LAKE, WI 54868
PHONE: 715.475.2029
EMAIL: KYLE.SCHLAMPP@LUMEN.COM

DAIRYLAND POWER COOPERATIVE - ELECTRICITY
ATTENTION: MICHAEL LYDON
3200 EAST AVE S
LA CROSSE, WI 54602
PHONE: 608.787.1381
EMAIL: MICHAEL.LYDON@DAIRYLANDPOWER.COM

GAYS MILLS WATER & SEWER UTILITY - WATER
ATTENTION: JIM CHELLEVOLD
16381 STATE HWY 131, SUITE 1
GAYS MILLS, WI 54631
PHONE: 608.735.4341
EMAIL: JCHELLEVOLD@GAYSMILLS.ORG

GAYS MILLS WATER & SEWER UTILITY - SEWER
ATTENTION: JIM CHELLEVOLD
16381 STATE HWY 131, SUITE 1
GAYS MILLS, WI 54631
PHONE: 608.735.4341
EMAIL: JCHELLEVOLD@GAYSMILLS.ORG

MADISON GAS AND ELECTRIC - GAS
ATTENTION: ROGER AHLES
623 RAILROAD ST
MADISON, WI 53701
PHONE: 608.252.5682
EMAIL: RAHLES@MGE.COM

MEDIACOM WISCONSIN LLC - COMMUNICATION
ATTENTION: CRAIG EGGERT
1240 HIGHWAY 52 SOUTH
CHATFIELD, MN 55923
PHONE: 563.419.5160
EMAIL: CEGGERT@MEDIACOMCC.COM

MT STERLING WATERWORKS - SEWER
ATTENTION: DAVE JONES
17013 STH 27
FERRYVILLE, WI 54628
PHONE: 608.306.1023
EMAIL: DJONES@CO.CRAWFORD.WI.GOV

MT STERLING WATERWORKS - WATER
ATTENTION: DAVE JONES
17013 STH 27
FERRYVILLE, WI 54628
PHONE: 608.306.1023
EMAIL: DJONES@CO.CRAWFORD.WI.GOV

RICHLAND GRANT TELEPHONE COOPERATIVE - COMMUNICATION
ATTENTION: JOSH LIEN
202 N EAST ST
BLUE RIVER, WI 53518
PHONE: 608.537.2461
EMAIL: JOSHL@RGTC.COOP

SCENIC RIVERS ENERGY COOPERATIVE - COMMUNICATION
ATTENTION: PHIL SCHNEIDER
206 CTY K
LANCASTER, WI 53813
PHONE: 608.723.2121
EMAIL: PSCHNEIDER@SREC.NET

VALLEY RIDGE CLEAN WATER COMMISSION - SEWER
ATTENTION: CORY CALE
22439 STH 35
LYNXVILLE, WI 54626
PHONE: 608.874.4698
EMAIL: VRCWC@CENTURYTEL.NET



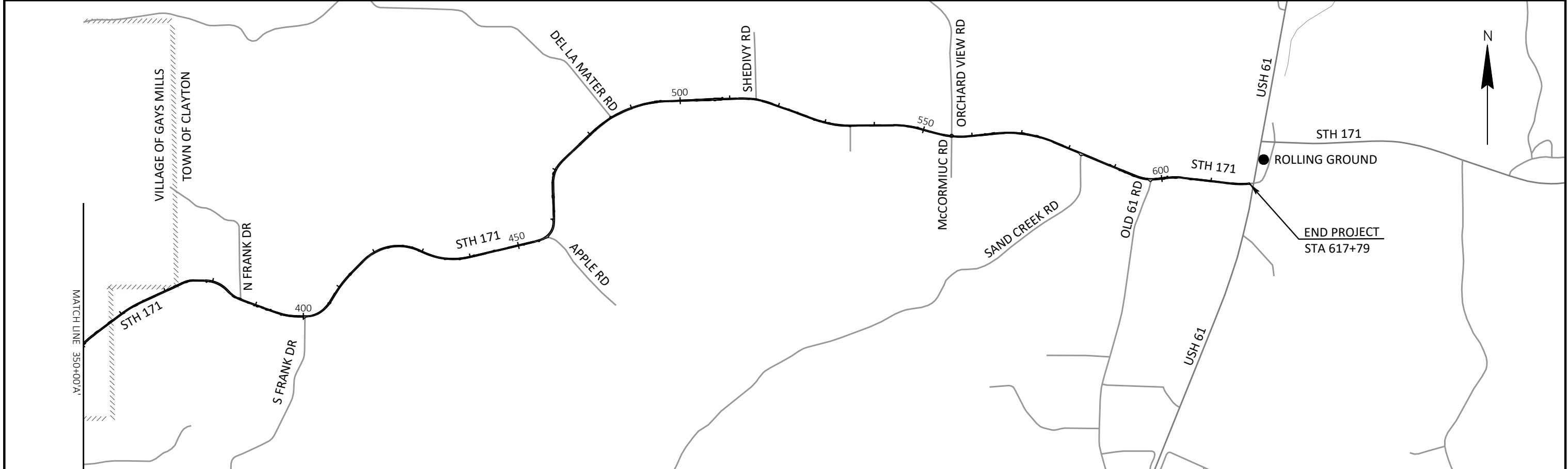
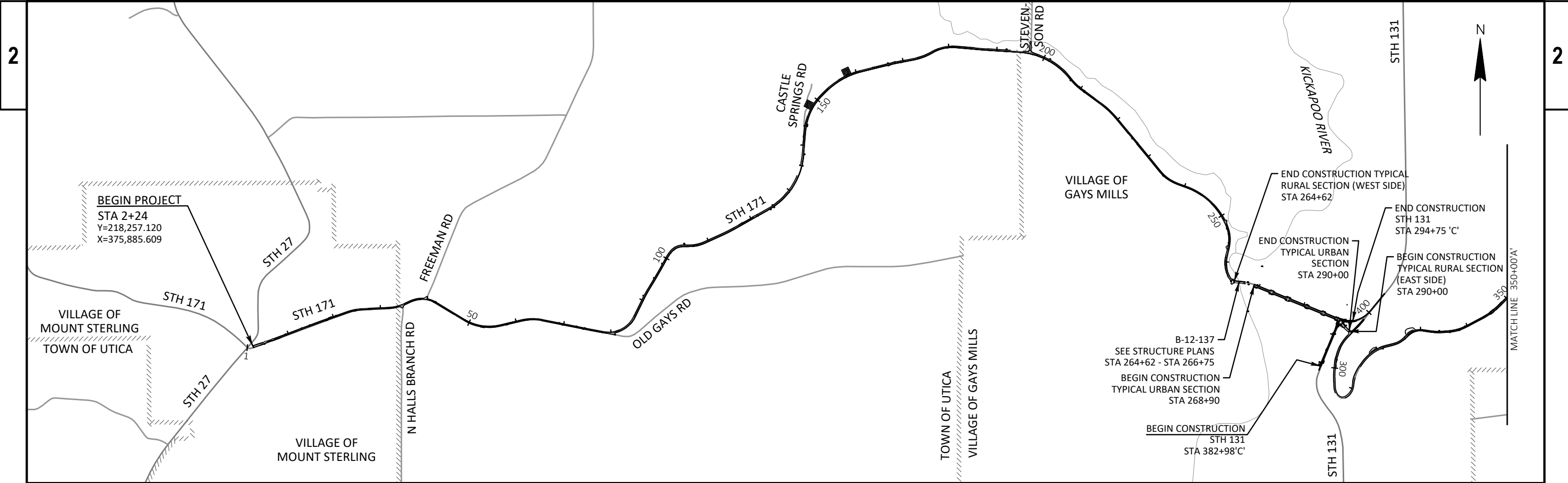
GENERAL NOTES:

1. REFERENCE LINES SHOWN ON THE PLANS ARE APPROXIMATE IN RELATION TO EXISTING CENTERLINE. CONTRACTOR SHALL VERIFY THAT PLAN OFFSETS ARE ACCURATE IN RELATION TO THE EXISTING CENTERLINE.
2. WHEN THE QUANTITY OF BASE AGGREGATE OR HMA PAVEMENT IS MEASURED FOR PAYMENT BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE LAYER SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.
3. THE LOCATION OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS IS APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.
4. NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.
5. THE EXACT LOCATION OF THE EROSION CONTROL DEVICES SHALL BE DETERMINED IN THE FIELD.
6. DISTURBED AREAS WITHIN THE RIGHT-OF-WAY, EXCEPT THE AREAS WITHIN THE FINISHED SUBGRADE SHOULDER POINTS, ARE TO BE 4-INCH SALVAGED TOPSOIL, FERTILIZED AND SEEDED.
7. ALL PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND SHALL BE RESTORED IN-KIND. LIMITS TO BE DETERMINED BY ENGINEER
8. WHEN PORTION OF EXISTING ASPHALTIC SURFACES ARE TO BE REMOVED TO ACCOMMODATE NEW CONSTRUCTION, THE LINE OF SUCH REMOVAL SHALL BE NEATLY DELINEATED WITH A SAW CUT JOINT THOUGH THE ASPHALTIC SURFACE SO THAT REMOVAL OF THE ASPHALT SHALL BE ACCOMPLISHED WITHOUT DAMAGE TO REMAINING PORTION, THE LOCATION OF SAW JOINT AND THE AMOUNT REMOVED AT SIDE ROADS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER
9. BEARINGS SHOWN ON THE PLAN ARE REFERENCED TO THE EXISTING ROADWAY CENTERLINE AND ARE ASSUMED.
10. THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, PASSING OR PARKING LANE.
11. SHOULDER TAPER LOCATIONS SHALL MATCH EXISTING.
12. A CONVERSION FACTOR OF 2.1 TONS/CY IS USED TO ESTIMATE QUANTITIES FOR BASE AGGREGATE DENSE 3/4-INCH.
13. A CONVERSION FACTOR OF 112 LBS/IN/SY IS USED TO ESTIMATE QUANTITIES FOR HMA PAVEMENT.
14. APPLY TACK COAT AT A RATE OF 0.05 GAL/SY BETWEEN LAYERS OF HMA PAVEMENT.
15. HMA PAVEMENT SHALL BE PLACED IN LIFTS AS FOLLOWS:
 - 3.50-INCH = 1.75" - 4 LT 58-28 S (UPPER LAYER, RURAL STH 171)
1.75" - 4 LT 58-28 S (LOWER LAYER, RURAL STH 171)
 - 2.25-INCH = 1.25" - 5 MT 58-28 S (UPPER LAYER, URBAN STH 171)
1.00" - HMA PAVEMENT INTERLAYER (LOWER LAYER, URBAN STH 171)
 - 3.50-INCH = 1.75" - 4 LT 58-28 S (UPPER LAYER, STH 131)
1.75" - 4 LT 58-28 S (LOWER LAYER, STH 131)
 - 4.25-INCH = 1.25" - 5 MT 58-28 S (UPPER LAYER, STH 171/131 INTERSECTION)
3.00" - 4 LT 58-28 S (LOWER LAYER, STH 171/131 INTERSECTION)

RUNOFF COEFFICIENT TABLE

LAND USE:	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE-TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = 92.6 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 4.5 ACRES



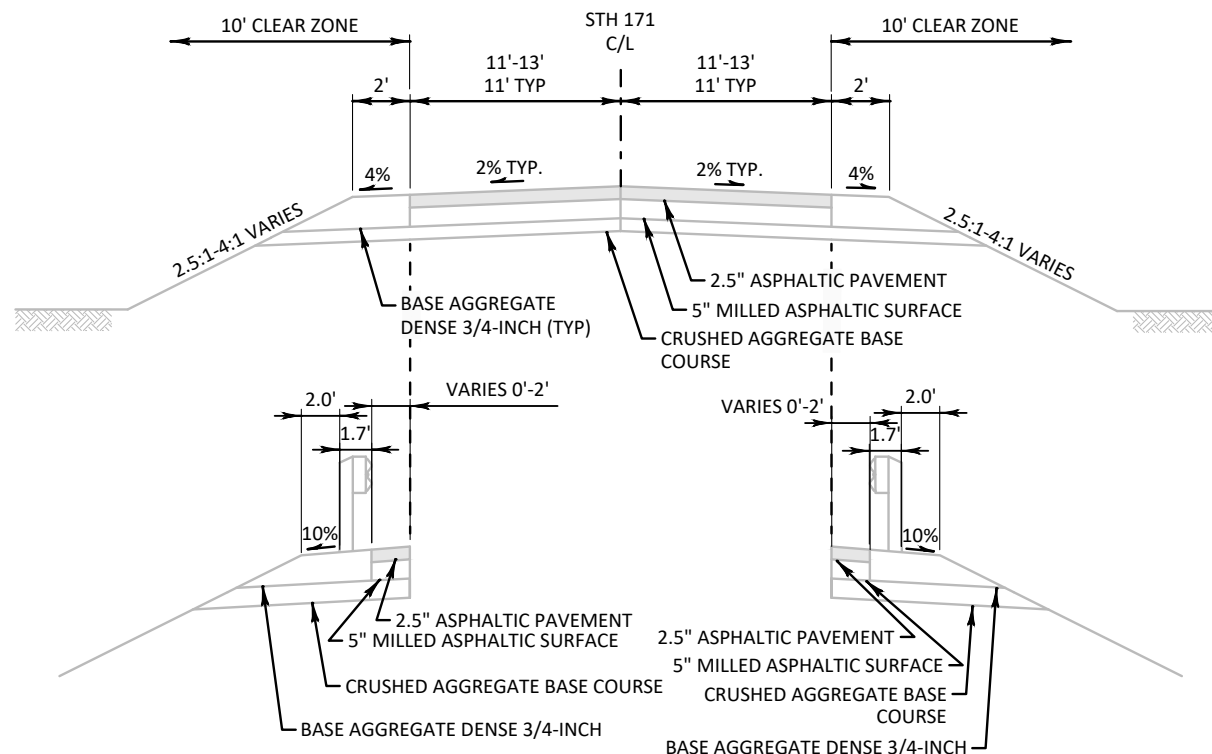
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PROJECT OVERVIEW	SHEET	E
------------------------	--------------	------------------	------------------	-------	----------

EXISTING GUARD RAIL LOCATIONS
LEFT

- STA 52+65 - STA 55+85
- STA 81+57 - STA 117+54
- STA 121+21 - STA 125+75
- STA 129+09 - STA 140+44
- STA 142+26 - STA 167+95
- STA 170+00 - STA 196+70
- STA 196+94 - STA 204+88
- STA 234+43 - STA 236+24
- STA 257+16 - STA 260+73
- STA 320+98 - STA 321+90
- STA 323+32 - STA 326+35

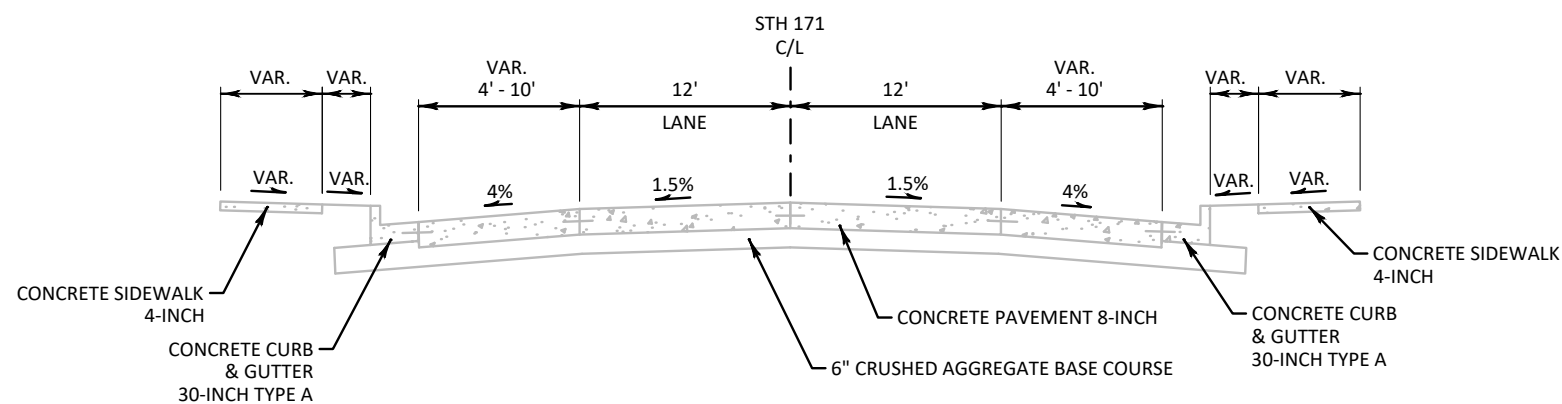
EXISTING GUARD RAIL LOCATIONS
RIGHT

- STA 298+80 - STA 320+95



TYPICAL EXISTING SECTION - STH 171 RURAL

STH 171
STA 2+24 - STA 263+25
STA 291+30 - STA 617+79

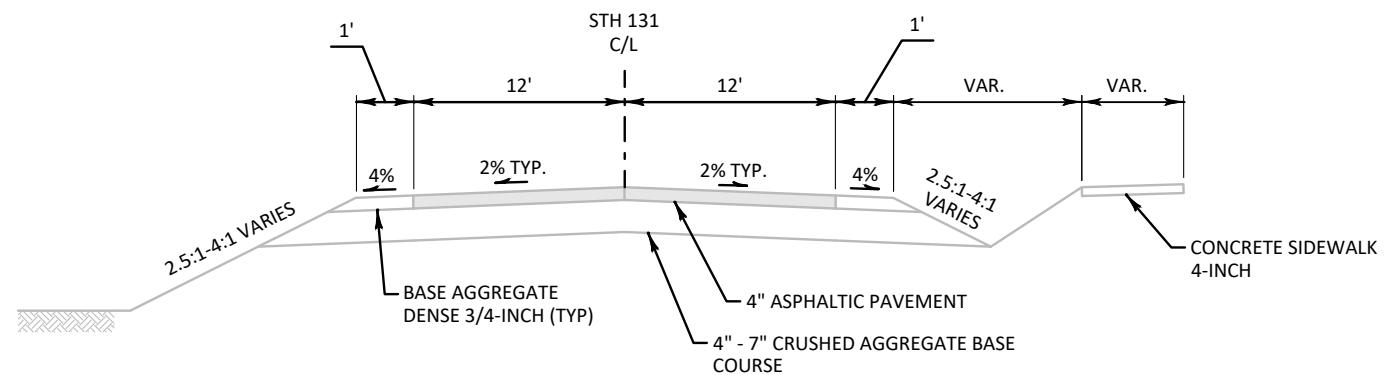


TYPICAL EXISTING SECTION - STH 131 URBAN

STH 131
STA 392+41 C - STA 394+75 C

TYPICAL EXISTING SECTION - STH 171 URBAN

STH 171
STA 263+25 - STA 264+66
STA 268+90 - STA 291+30



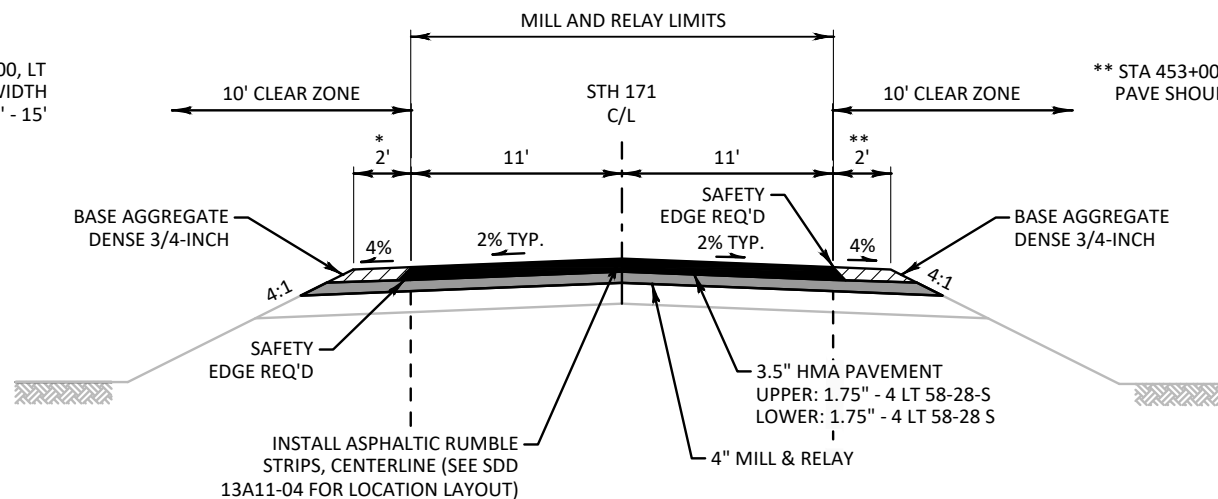
TYPICAL EXISTING SECTION - STH 131 RURAL

STH 131
 STA 382+99 C - STA 392+41 C

* STA 302+00 - STA 311+00, LT
PAVE SHOULDER FULL WIDTH
AND WIDEN FROM 13' - 15'

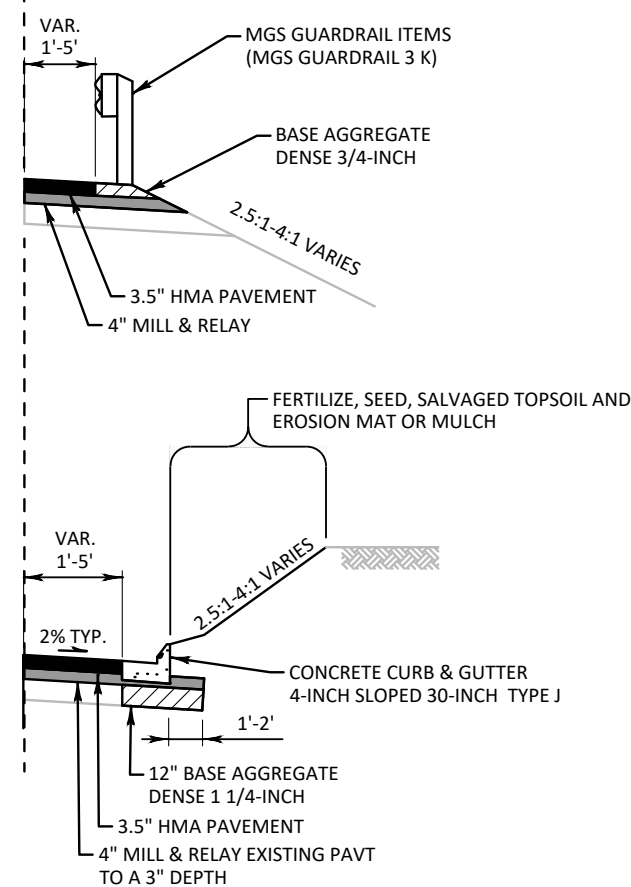
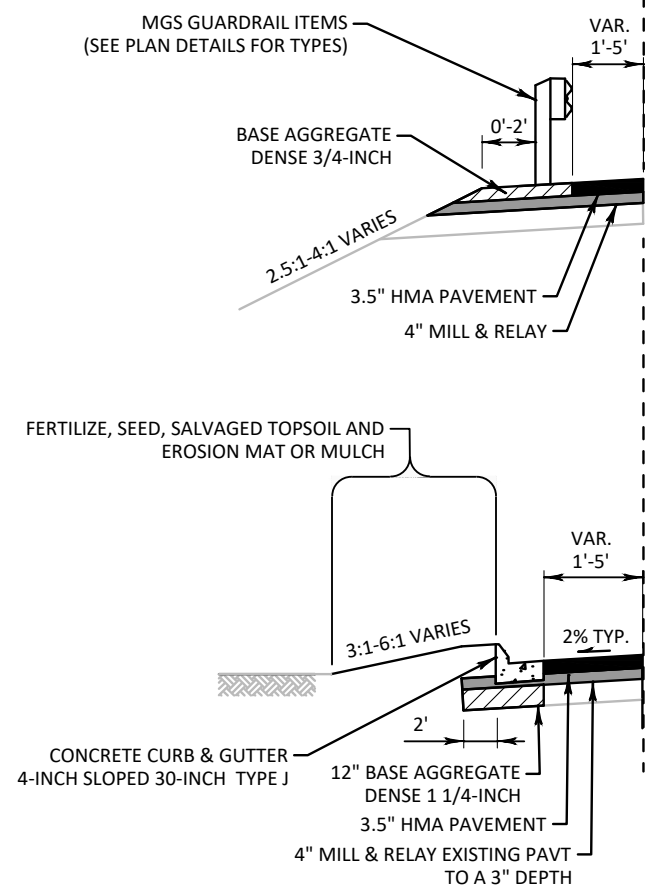
** STA 453+00 - STA 460+00, RT
PAVE SHOULDER FULL WIDTH

- FINISHED CL PROFILE:**
- HMA PAVEMENT TO BE 1 1/2" TO 2 1/2" ABOVE EXISTING PROFILE
 - MILL & RELAY PROFILE TO BE 1" - 2" BELOW EXISTING PROFILE.
 - CONTRACTOR REQUIRED TO RE-GRADE SUPER ELEVATIONS BY MOVING MATERIAL AS NEEDED FROM LOW OR HIGH SIDE OF SUPER TO MEET DESIRED SUPER ELEVATION RATE AS DIRECTED BY THE ENGINEER. MAXIMUM OF 3" FINISH GRADE CHANGE ON HIGH SIDE OF SUPER ALLOWED.
 - MAINTAIN EXISTING SIDE SLOPES AS BEST AS PRACTICAL.



- PROPOSED GUARD RAIL LOCATIONS LEFT**
- STA 52+64 - STA 55+87
 - STA 81+58 - STA 117+54
 - STA 120+69 - STA 126+28
 - STA 129+07 - STA 140+46
 - STA 142+06 - STA 167+95
 - STA 170+04 - STA 196+70
 - STA 196+92 - STA 204+97
 - STA 233+55 - STA 236+28
 - STA 257+17 - STA 260+77
 - STA 321+17 - STA 326+55

- PROPOSED CURB AND GUTTER LOCATIONS LEFT**
- STA 2+24 - STA 4+50
 - STA 334+04 - STA 349+59
 - STA 365+36 - STA 373+84
 - STA 405+02 - STA 408+12
 - STA 455+48 - STA 459+67
 - STA 464+85 - STA 470+24
 - STA 503+19 - STA 508+29
 - STA 591+84 - STA 600+32

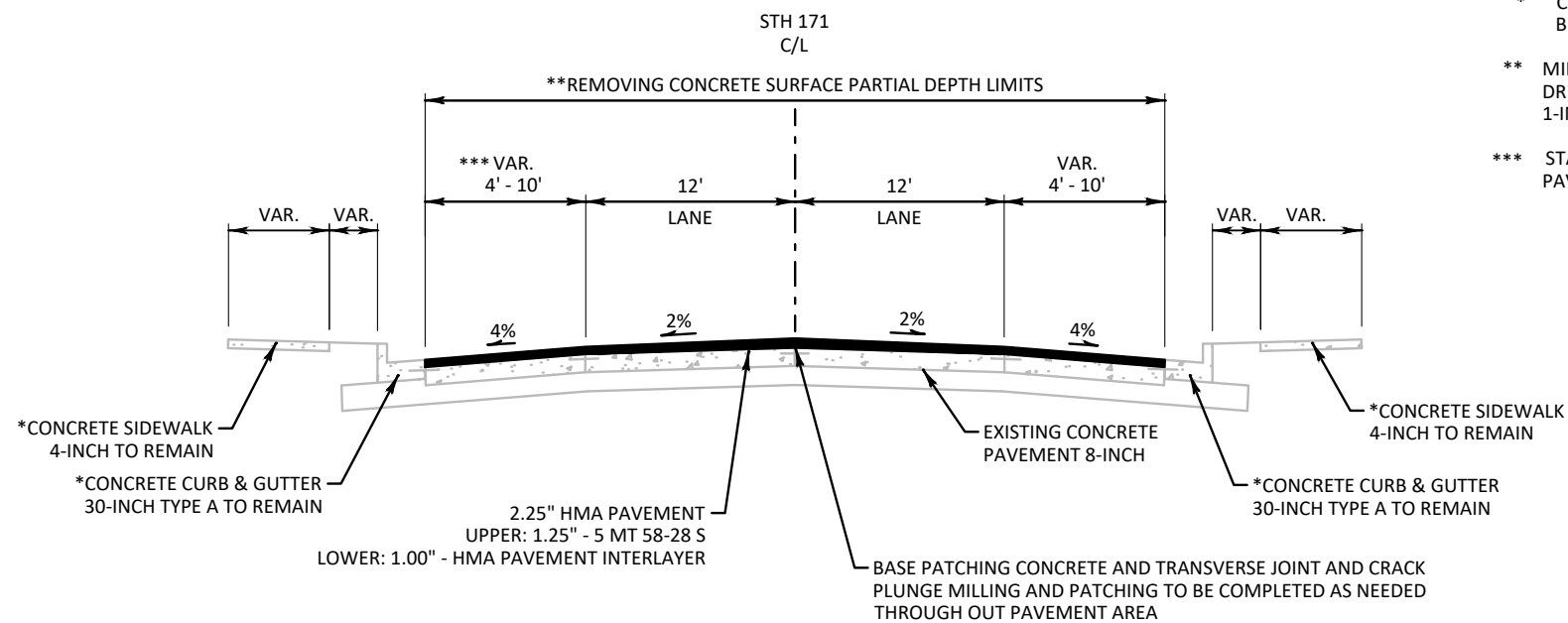


- PROPOSED GUARD RAIL LOCATIONS RIGHT**
- STA 298+54 - STA 321+04

- PROPOSED CURB AND GUTTER LOCATIONS RIGHT**
- STA 3+65 - STA 4+50
 - STA 84+60 - STA 137+96
 - STA 144+49 - STA 207+15
 - STA 250+70 - STA 252+72
 - STA 258+62 - STA 263+26
 - STA 321+26 - STA 334+09
 - STA 349+40 - STA 352+90
 - STA 378+92 - STA 384+19
 - STA 395+86 - STA 400+10
 - STA 503+19 - STA 508+30

TYPICAL PROPOSED SECTION - STH 171 RURAL

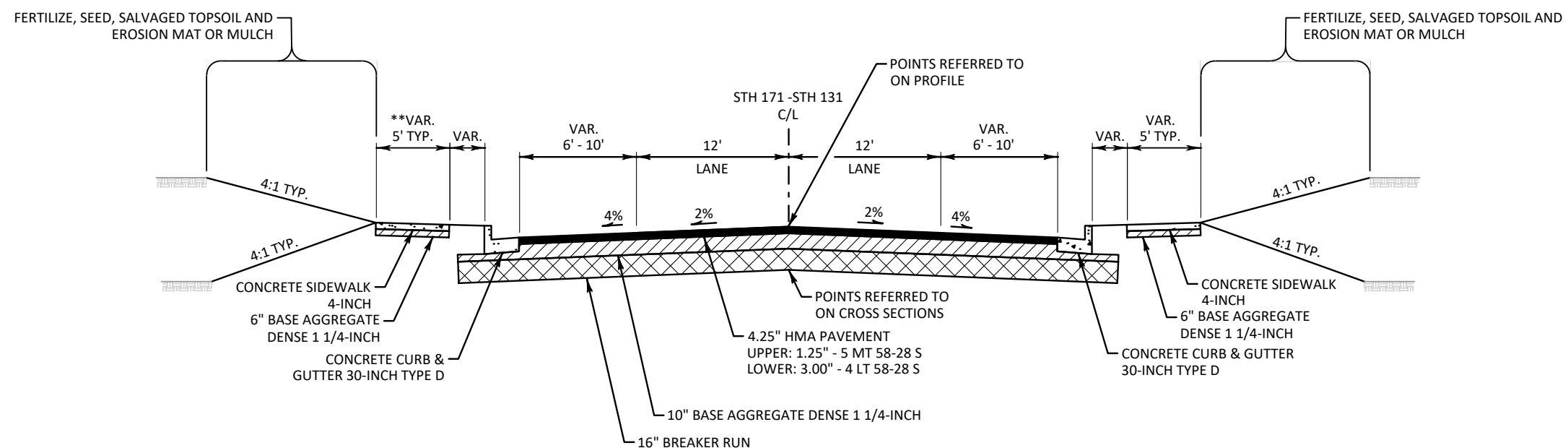
- STH 171
- STA 2+24 - STA 264+62
 - STA 291+30 - STA 343+00
 - STA 349+40 - STA 617+79
 - STA 800+22'B' - STA 803+57'C'



- * CURB RAMPS AND CORRESPONDING CURB & GUTTER AND SIDEWALK WILL BE REPLACED ACCORDING TO PLAN, WITHIN THE VILLAGE OF GAYS MILLS.
- ** MILL AND REMOVE 1-INCH OF EXISTING CONCRETE PAVEMENT IN THE 12-FT DRIVING LANES AT 2% SLOPE. IN PARKING LANES, VARY MILL DEPTH FROM 1-INCH AT THE EDGE OF DRIVING LANE TO 2.25-INCH AT EDGE OF GUTTER.
- *** STA 290+00 - STA 291+30, LT: REMOVE EXISTING ASPHALTIC SHOULDER AND PAVE LOWER LAYER 3.00" - 4 LT 58-28 S.

TYPICAL PROPOSED SECTION - STH 171 URBAN

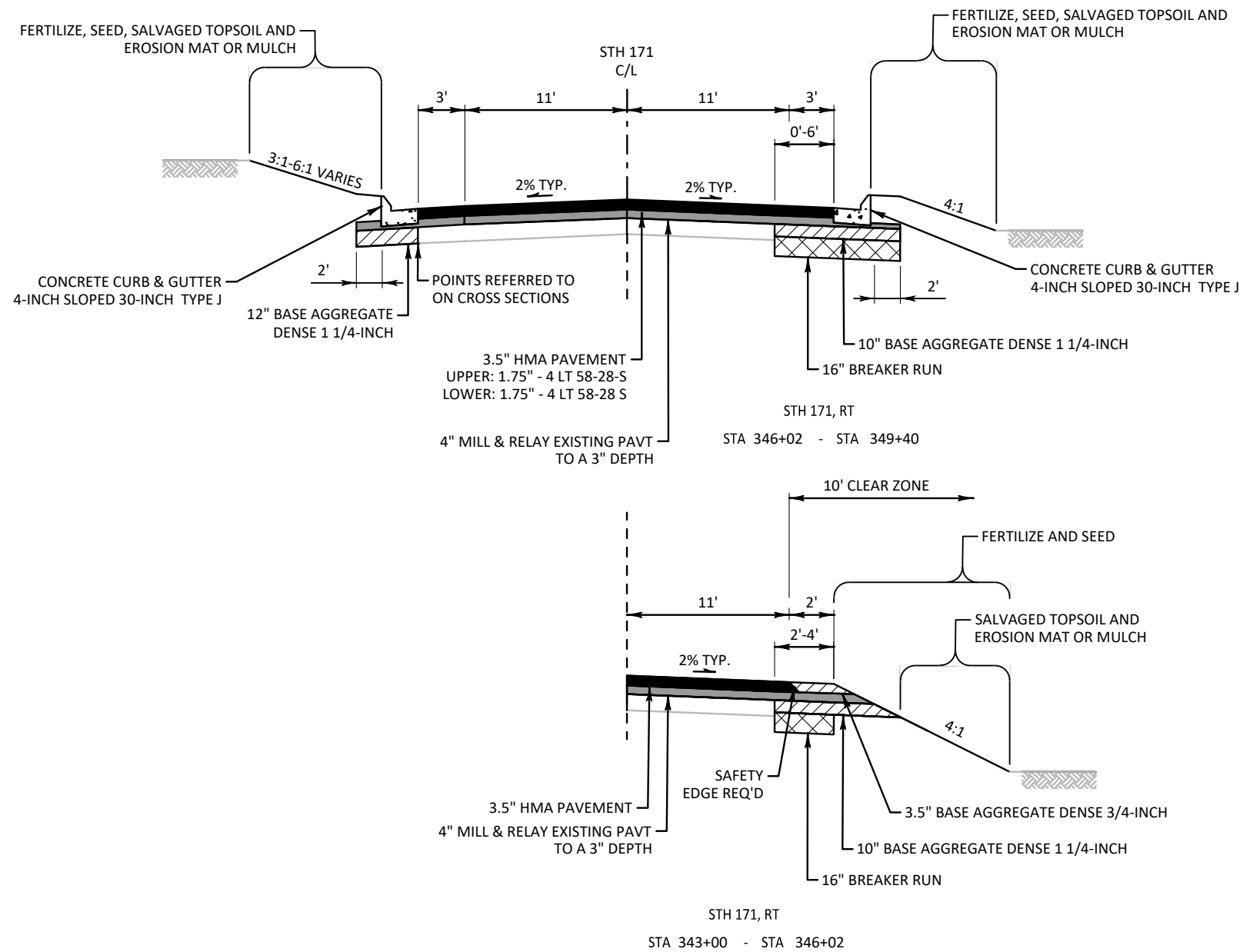
STH 171
 STA 268+90 - STA 287+02
 STA 290+00 - STA 291+30



TYPICAL PROPOSED SECTION - STH 171 & STH 131 INTERSECTION URBAN

STH 171 & STH 131
 STA 287+02 - STA 290+00
 STA 392+15 C - STA 394+75 C

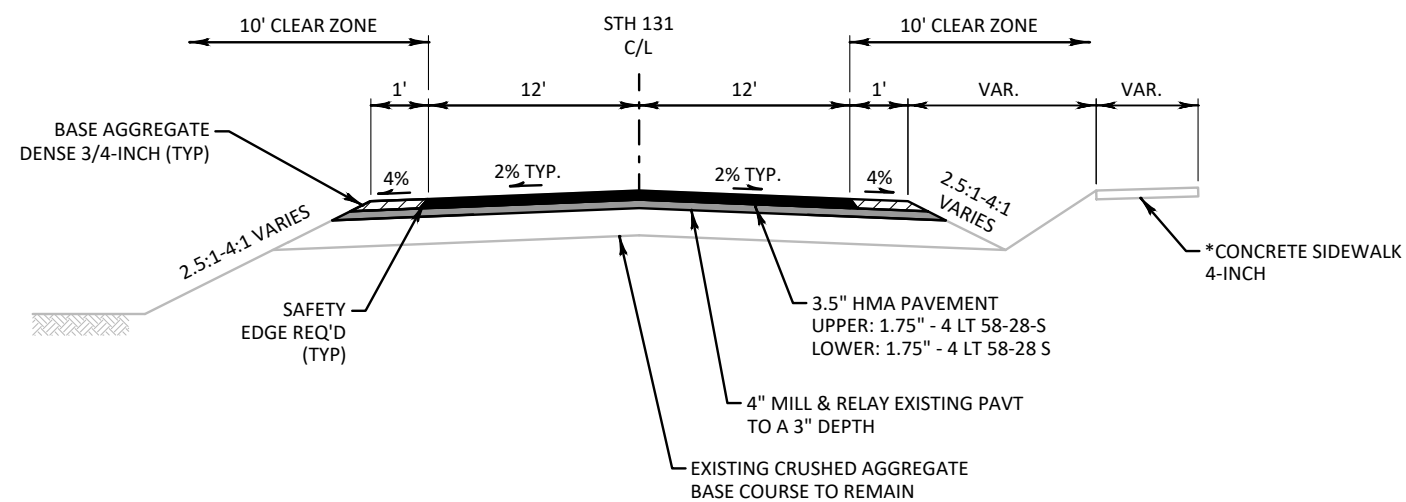
** EXISTING RETAINING WALL PRESENT AT BACK OF SIDEWALK FROM STA 393+50 - STA 394+27, LT



TYPICAL PROPOSED SECTION - STH 171 RURAL

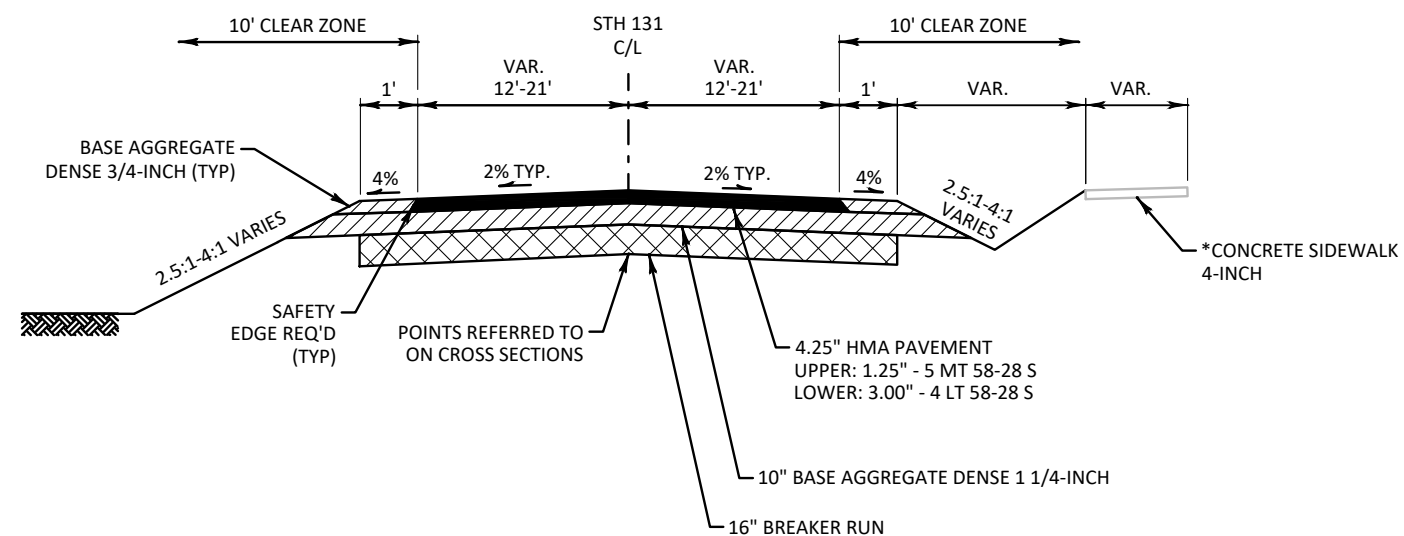
STH 171
STA 343+00 - STA 349+40

* CURB RAMPS AND CORRESPONDING CURB & GUTTER AND SIDEWALK WILL BE REPLACED ACCORDING TO PLAN, WITHIN THE VILLAGE OF GAYS MILLS.



TYPICAL PROPOSED SECTION - STH 131 RURAL

STH 131
STA 382+98 C - STA 391+23 C

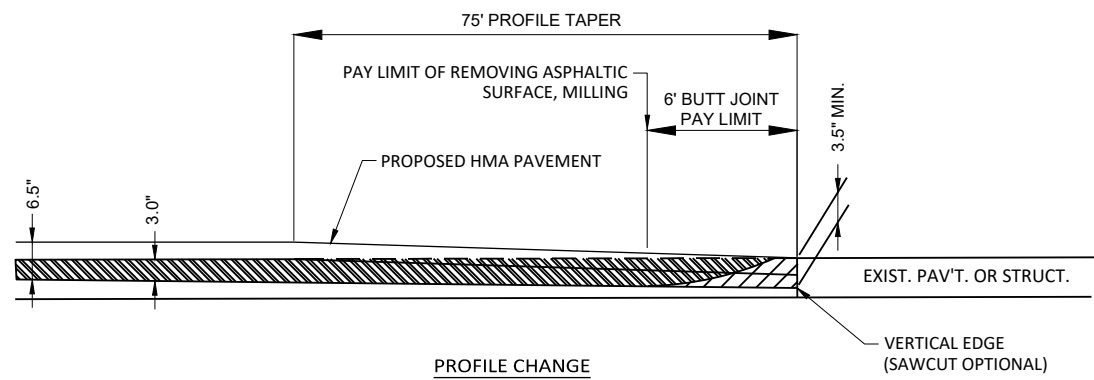




TYPICAL PROPOSED SECTION - STH 131 RURAL

STH 131
STA 391+23 C - STA 392+15 C

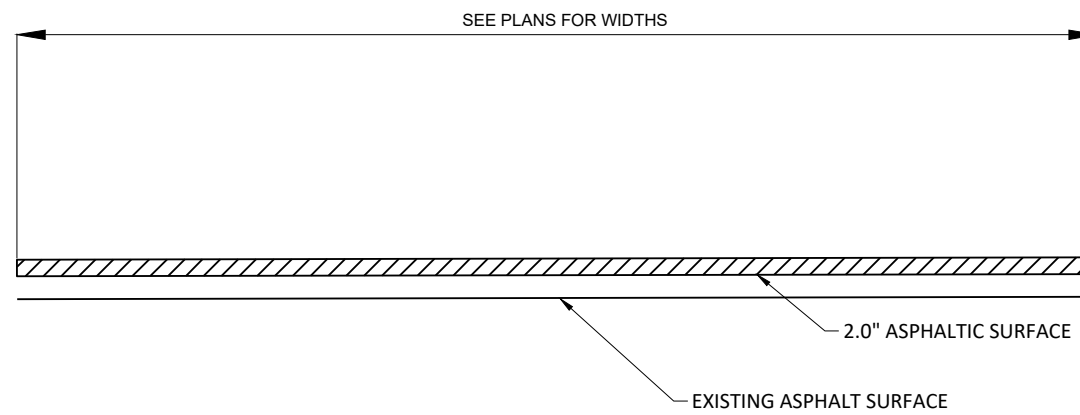
SOIL BORING LOG					
BORING NUMBER	STATION	OFFSET	NORTHING	EASTING	HMA LAYER THICKNESS (INCHES)
1	8+74.93	-5.347'	218485.3195	376495.3984	5
2	25+47.58	-10.391'	219039.3216	378073.9068	4.5
3	39+92.88	-6.696'	219247.7238	379488.9414	8
4	54+04.49	-10.098'	218680.4379	380759.667	8
5	66+86.67	0.710'	218780.9248	382021.4242	7.5
6	79+34.73	5.391'	218542.0411	383246.8916	5.5
7	91+51.66	1.265'	219357.4821	384037.5597	8.5
8	106+36.75	0.837'	220351.9174	384987.5351	5.5
9	119+72.51	-12.998'	220909.5878	386191.755	7.5
10	132+41.09	-11.527'	221710.2626	387135.2898	7
11	145+14.18	-16.494'	222952.5014	387360.6358	8.5
12	158+44.15	-13.176'	223903.2385	388251.18	8
13	172+04.58	3.271'	224215.2714	389575.8294	8
14	184+85.62	3.613'	224443.3461	390807.0446	11.5
15	198+24.08	-1.500'	224314.9785	392136.7776	8
16	212+37.43	5.747'	223465.4053	393245.4298	7.5
17	226+39.68	-13.753'	222486.9282	394240.049	7.5
18	238+07.41	-15.636'	221664.48	395041.0777	10
19	251+02.89	-9.019'	220890.9811	396043.5243	7.5
20	263+72.27	-12.871'	219671.9973	396176.9092	5
21	303+63.08	8.044'	217454.7744	398323.0272	6.5
22	299+21.94	14.670'	217897.4117	398328.6142	5.5
23	316+74.64	5.299'	217995.4834	398969.0727	6.5
24	331+70.80	6.032'	218575.3228	400281.1335	6.5
25	342+82.04	-10.800'	218831.1327	401333.4965	7.5
26	357+74.34	-14.358'	219756.0837	402495.4595	5.5
27	370+06.01	-8.980'	220349.4211	403572.2791	6
28	383+08.10	-11.548'	220397.1364	404798.6425	5
29	396+32.48	4.817'	219854.8161	405986.9122	5.5
30	405+19.29	-0.821'	220021.9912	406817.3434	6.5
31	420+94.81	-2.361'	221168.1576	407871.8425	6
32	435+85.52	2.747'	220987.6707	409310.4004	5
33	448+17.26	-13.046'	221233.3042	410513.5465	5.5
34	460+76.69	-10.946'	221869.7129	411397.2717	7.5
35	474+57.54	-9.419'	223133.3387	411738.7335	6
36	487+93.20	-14.422'	223968.055	412777.4171	8.5
37	501+27.88	6.883'	224179.0309	414086.1743	6.25
38	515+85.44	2.046'	224189.0707	415540.0923	6.5
39	527+04.13	0.892'	223829.9243	416598.3925	7.5

SOIL BORING LOG					
BORING NUMBER	STATION	OFFSET	NORTHING	EASTING	HMA LAYER THICKNESS (INCHES)
40	544+87.72	6.219'	223677.29	418361.93	9
41	560+62.74	-10.240'	223485.72	419910.12	5.5
42	575+68.08	-10.580'	223395.17	421400.08	5.5
43	589+21.47	-7.786'	222873.95	422649.53	7
44	601+46.66	-16.048'	222645.02	423817.65	8
45	615+33.86	4.244'	222498.01	425198.69	7



-  MILL AND RELAY (3.5-INCHES TO A 4.0-INCH DEPTH)
-  REMOVING ASPHALTIC SURFACE BUTT JOINT

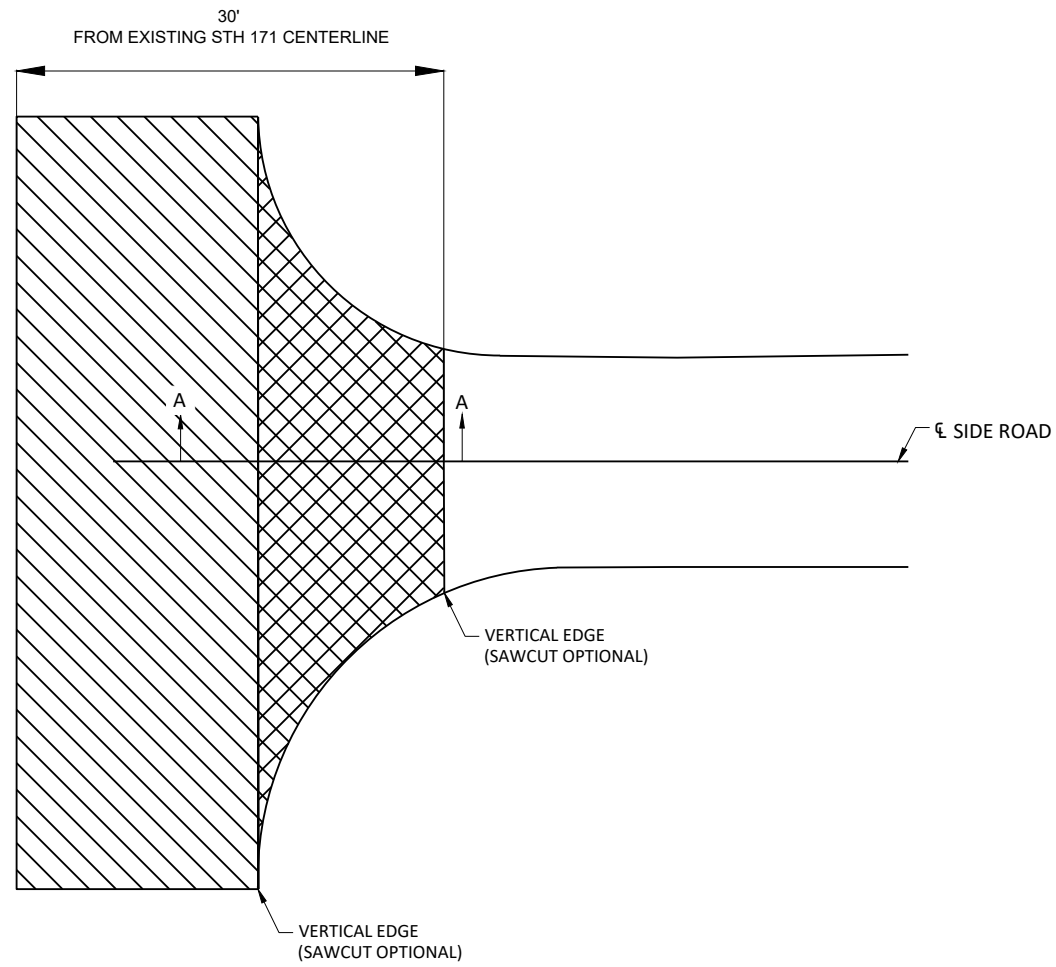
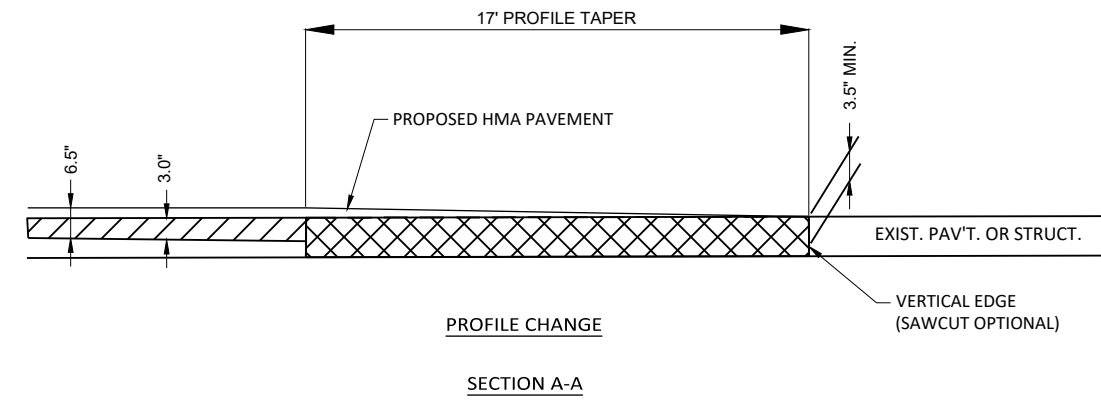
BUTT JOINT DETAIL FOR ASPHALTIC PAVEMENTS





-  ASPHALTIC SURFACE (2.0-INCHES) OVERLAY

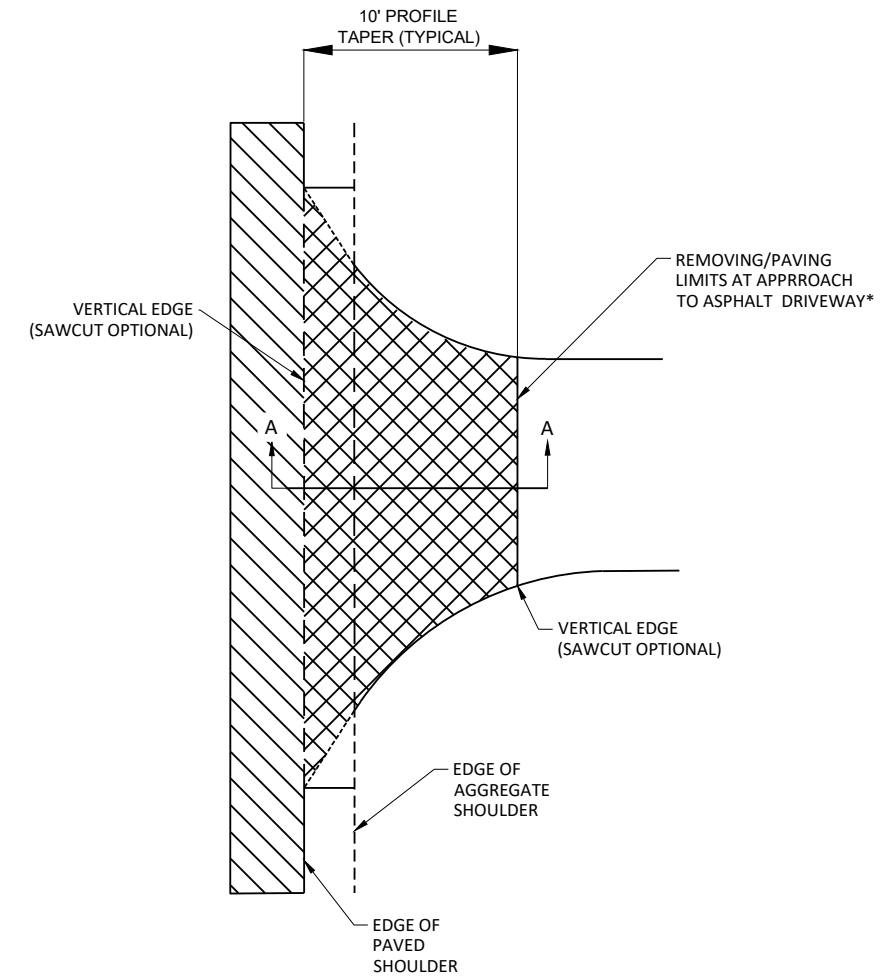
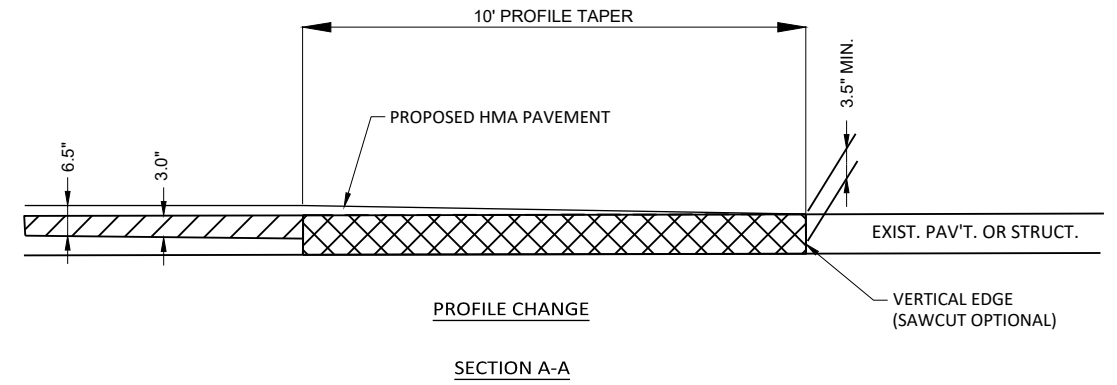
RURAL SCENIC LOOKOUT DETAIL



STA 317+90 - STA 319+80, LT
 STA 326+65 - STA 328+85, LT



-  MILL AND RELAY
-  REMOVING ASPHALTIC SURFACE

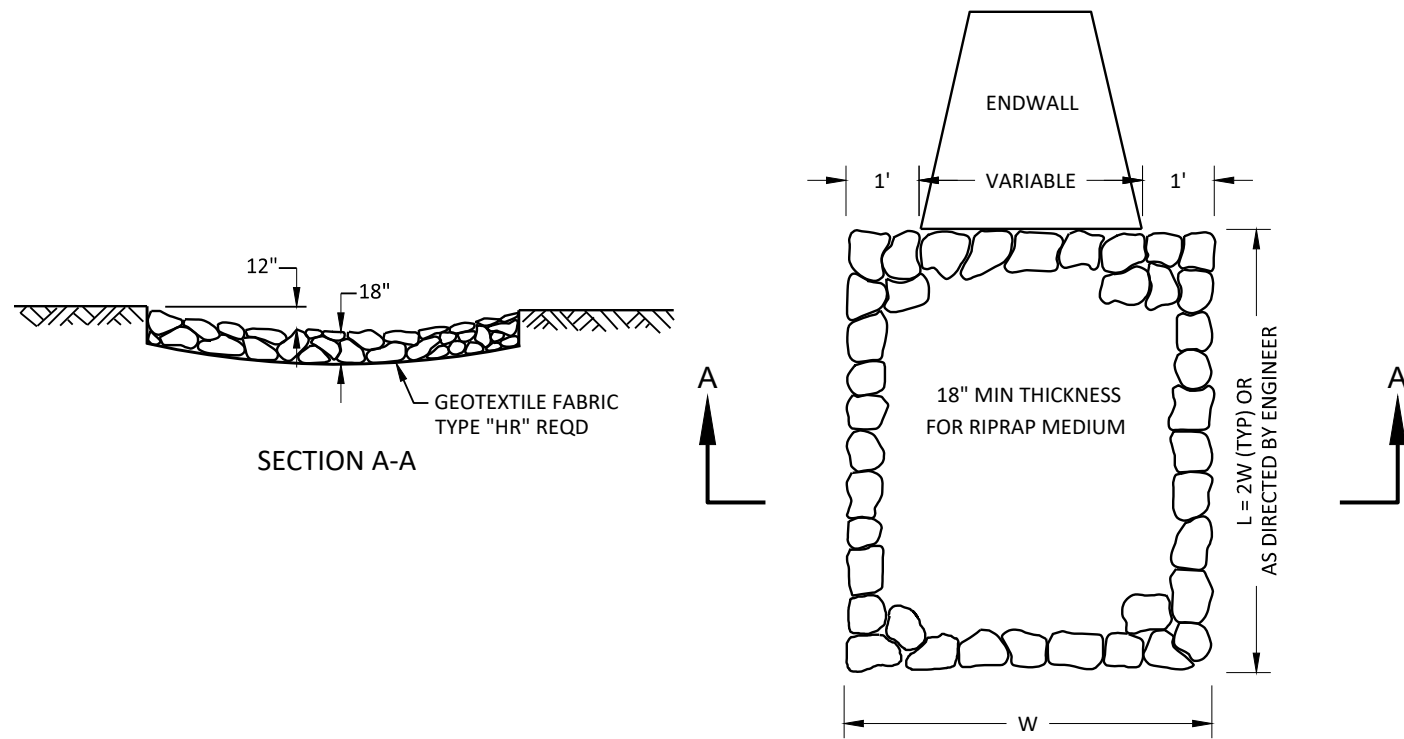
RURAL ASPHALTIC SURFACE SIDE ROAD DETAIL



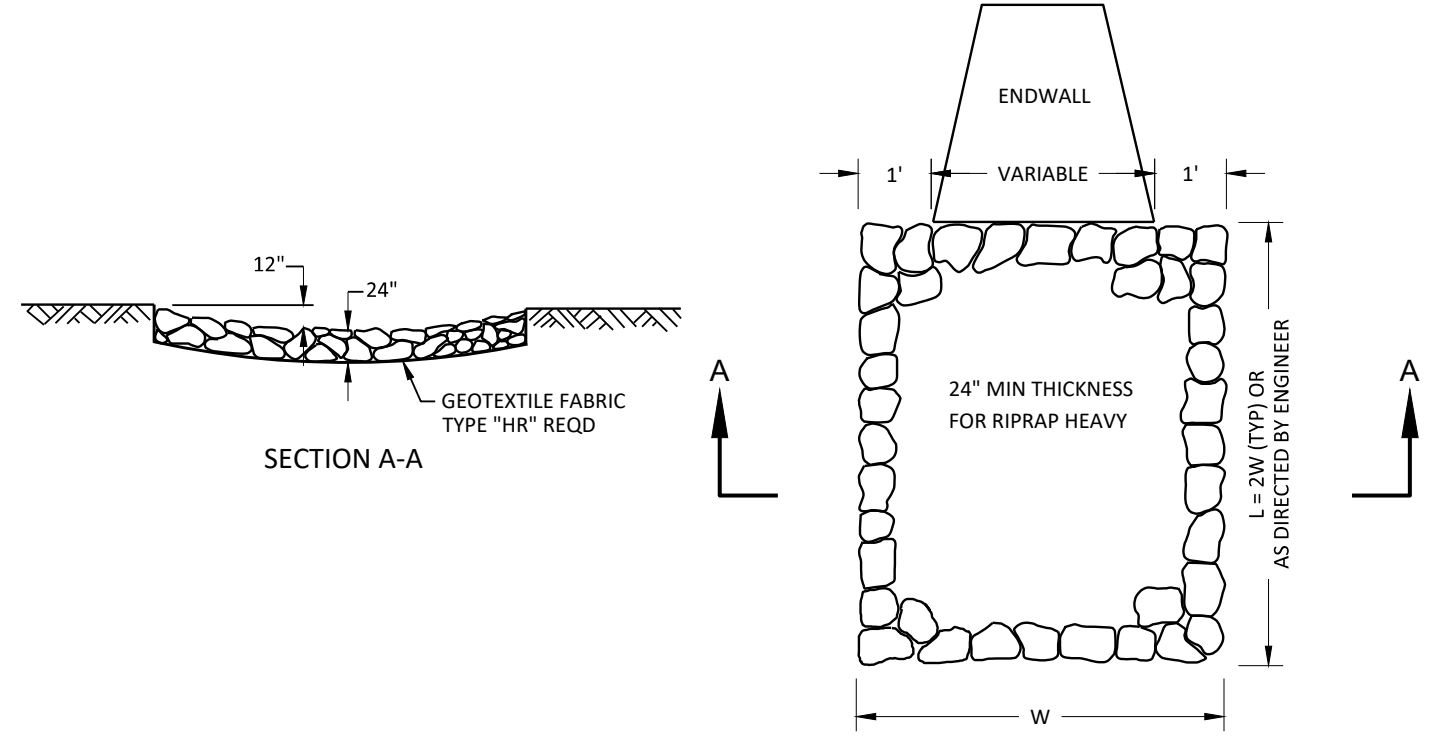
-  MILL AND RELAY
-  REMOVING ASPHALTIC SURFACE, MATCH ADJACENT PAVEMENT THICKNESS ALONG JOINTS

*LIMITS OF BASE AGGREGATE DENSE 3/4-INCH AT AGGREGATE DRIVEWAYS AND FIELD ENTRANCES AS DETERMINED BY THE ENGINEER.

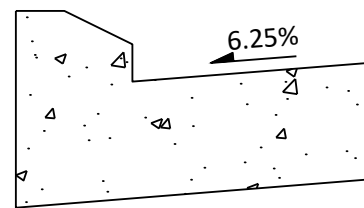
PRIVATE ENTRANCE DETAIL



RIPRAP MEDIUM TREATMENT AT CULVERTS

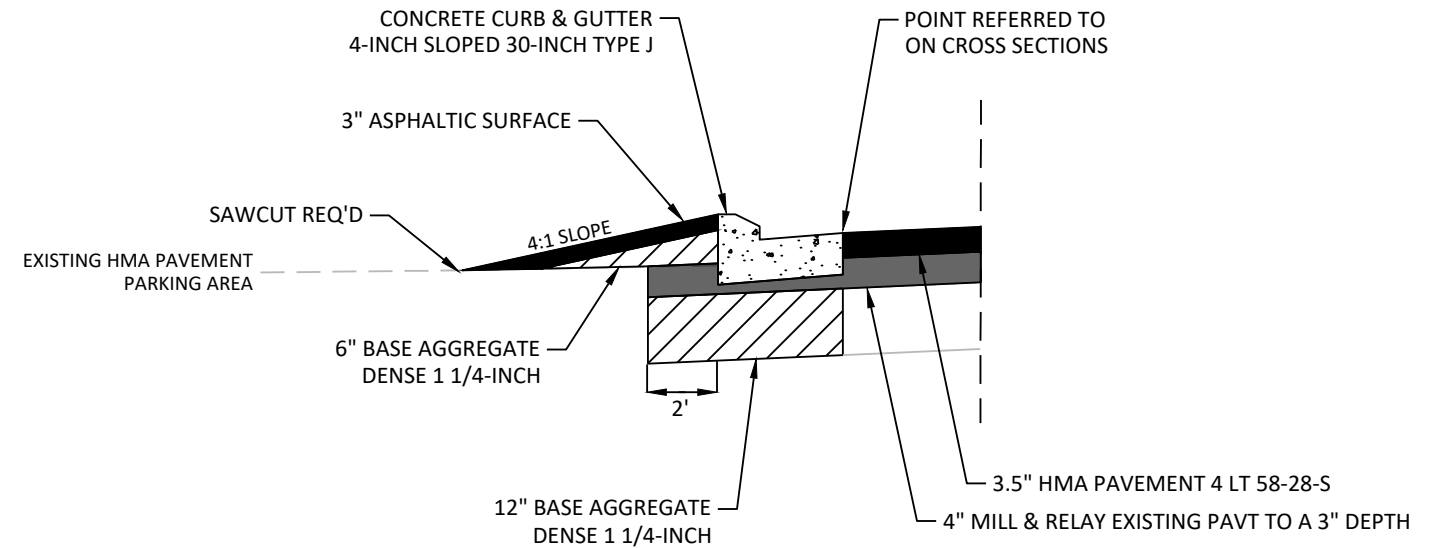


RIPRAP HEAVY TREATMENT AT CULVERTS



CONCRETE CURB & GUTTER 4-INCH SLOPED 30-INCH TYPE J

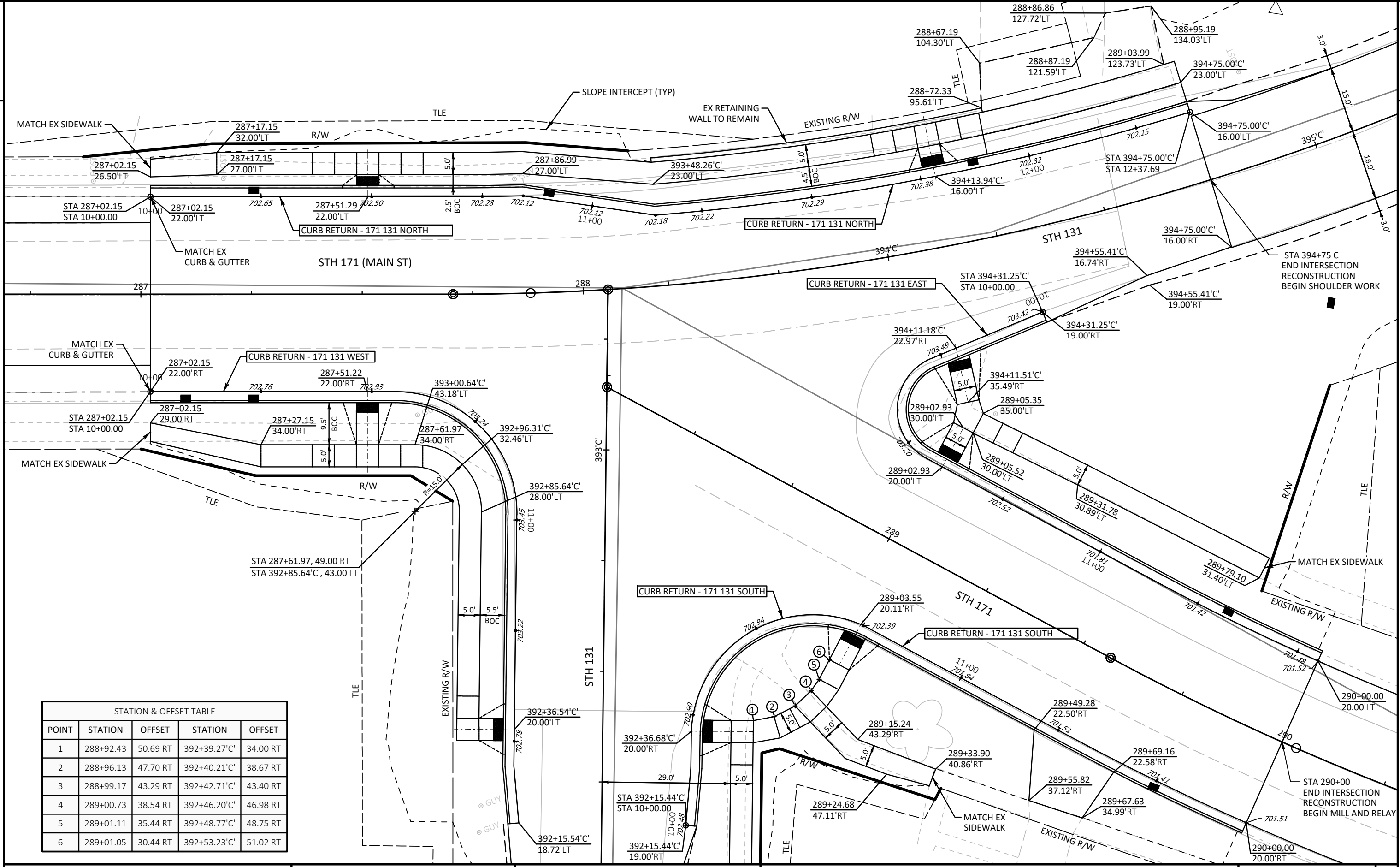
USE 6.25% GUTTER SLOPE AT ALL 4-INCH SLOPED 30-INCH TYPE J CURB & GUTTER



CURB & GUTTER AND PAVED PARKING AREAS AT ORCHARDS MATCH-IN DETAIL

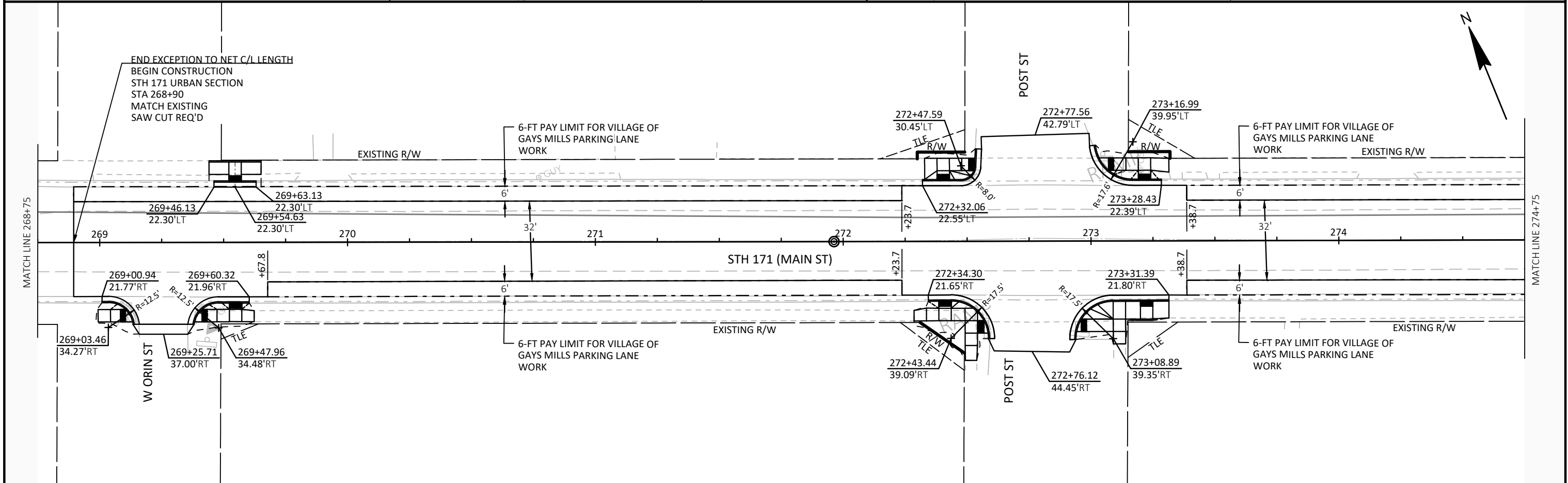
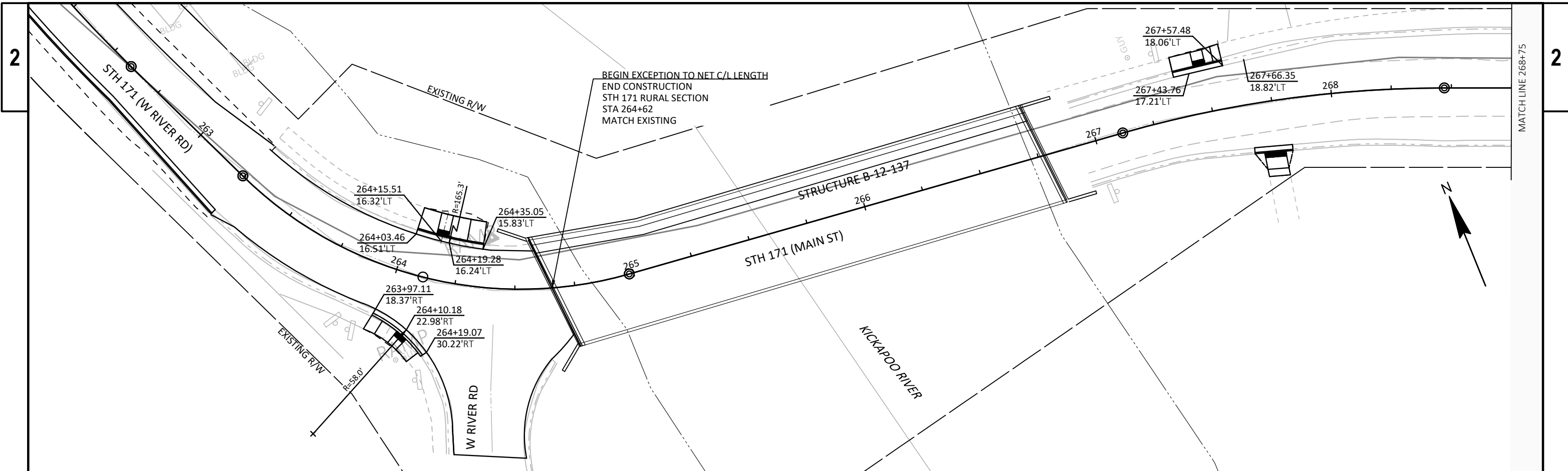
STA 346+43 - STA 348+72, LT
STA 505+82 - STA 506+79, LT
STA 507+39 - STA 507+92, LT

CONSTRUCT PAVEMENT BEHIND CURB & GUTTER TO MATCH-IN TO EXISTING PAVED PARKING AREAS.

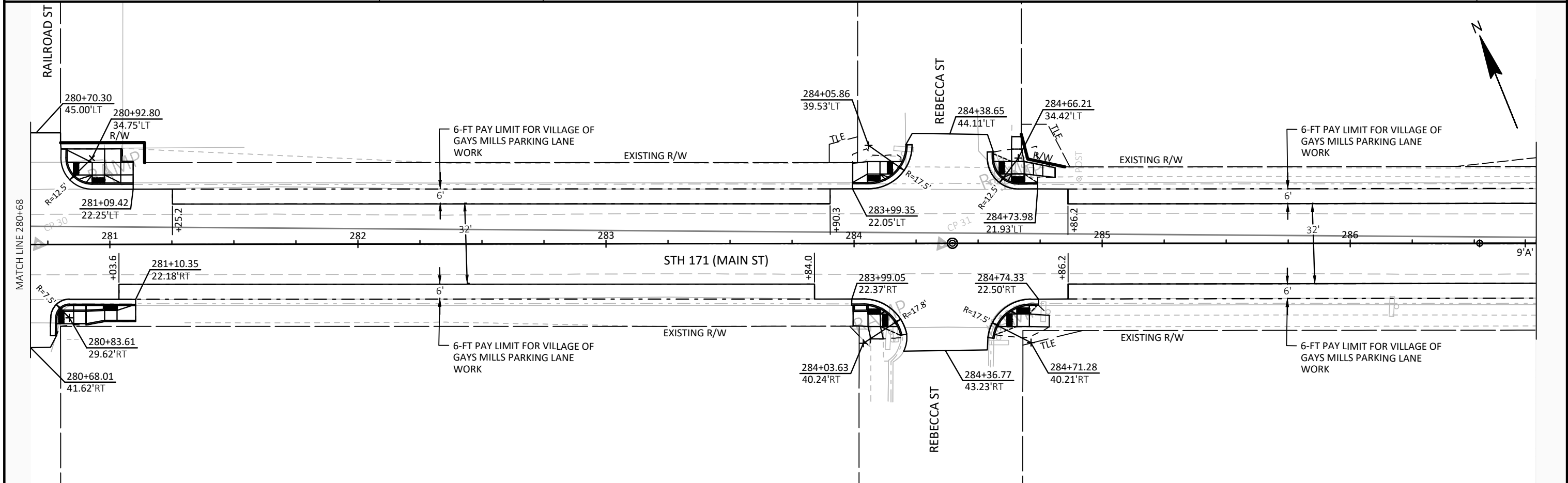
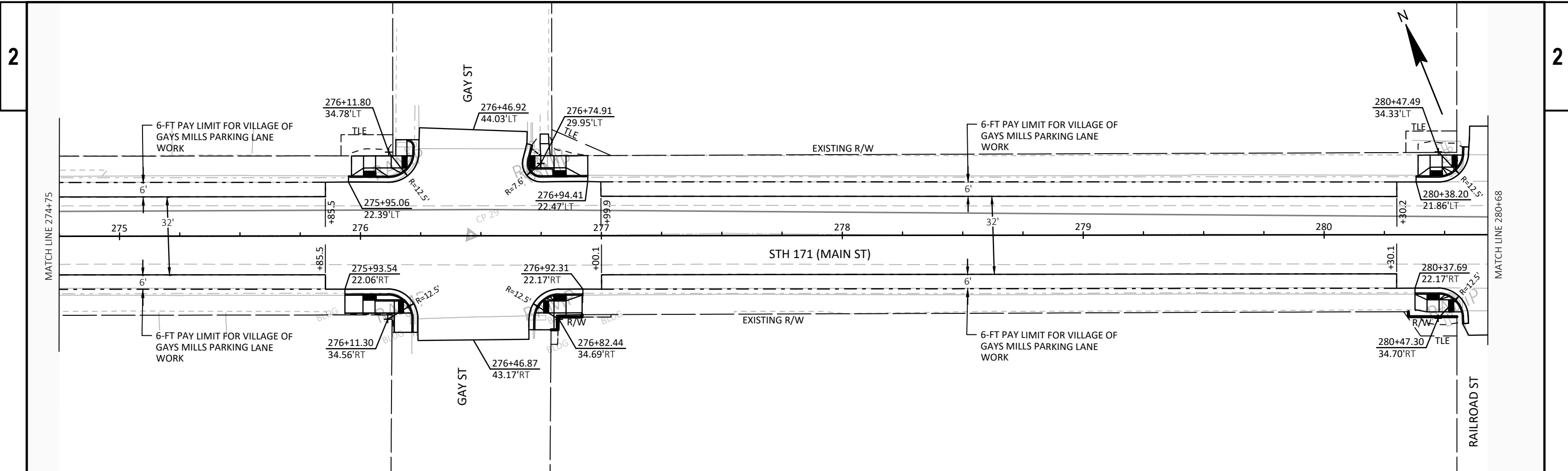


STATION & OFFSET TABLE

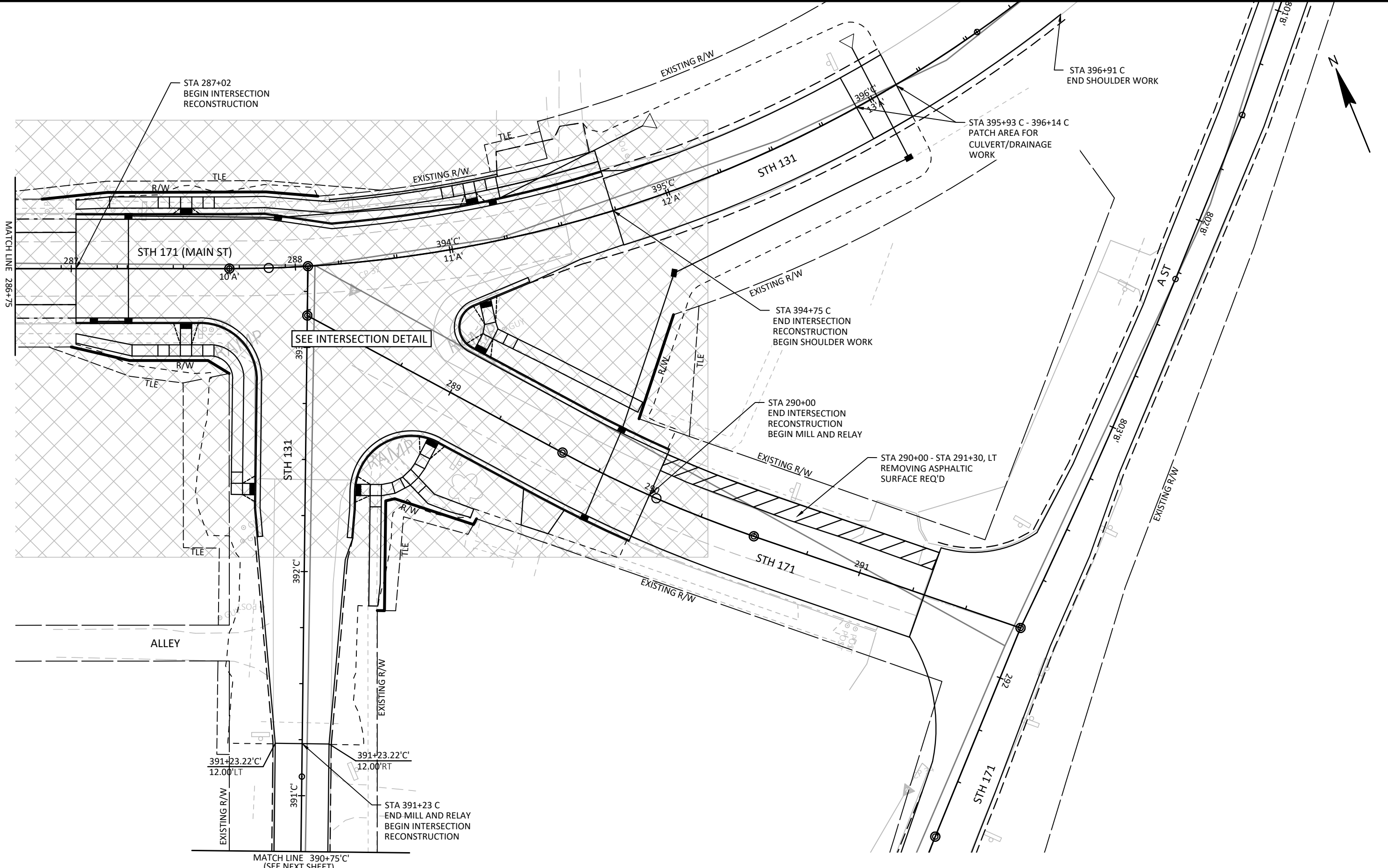
POINT	STATION	OFFSET	STATION	OFFSET
1	288+92.43	50.69 RT	392+39.27'C	34.00 RT
2	288+96.13	47.70 RT	392+40.21'C	38.67 RT
3	288+99.17	43.29 RT	392+42.71'C	43.40 RT
4	289+00.73	38.54 RT	392+46.20'C	46.98 RT
5	289+01.11	35.44 RT	392+48.77'C	48.75 RT
6	289+01.05	30.44 RT	392+53.23'C	51.02 RT



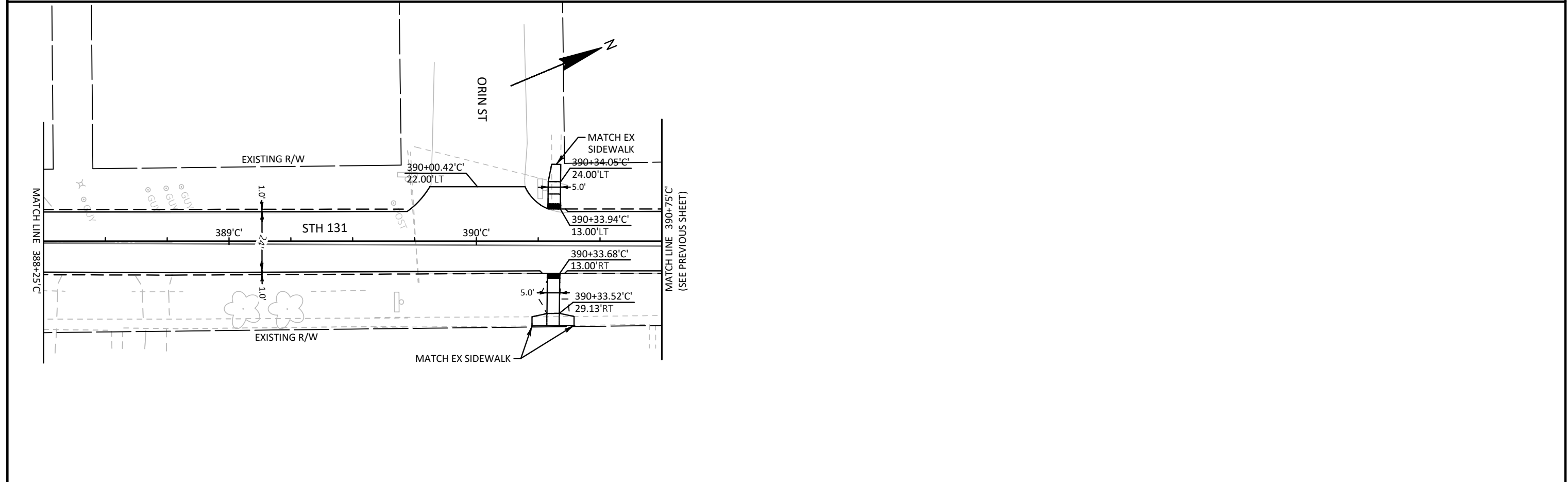
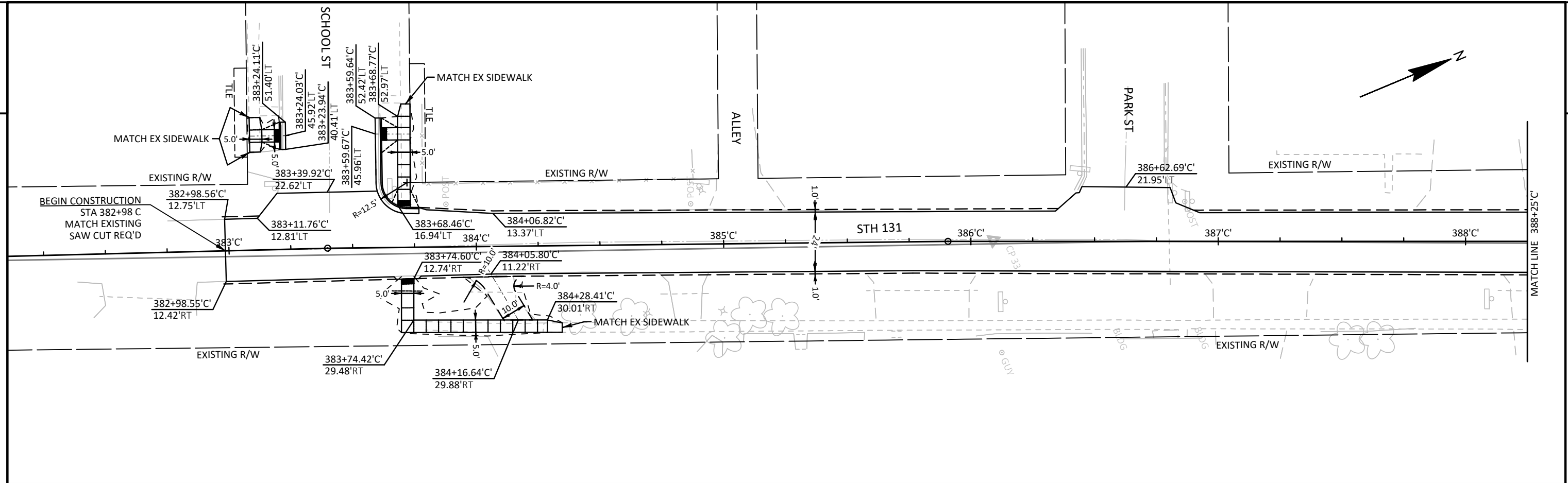
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PLAN DETAILS	SHEET	E
------------------------	--------------	------------------	--------------	-------	---



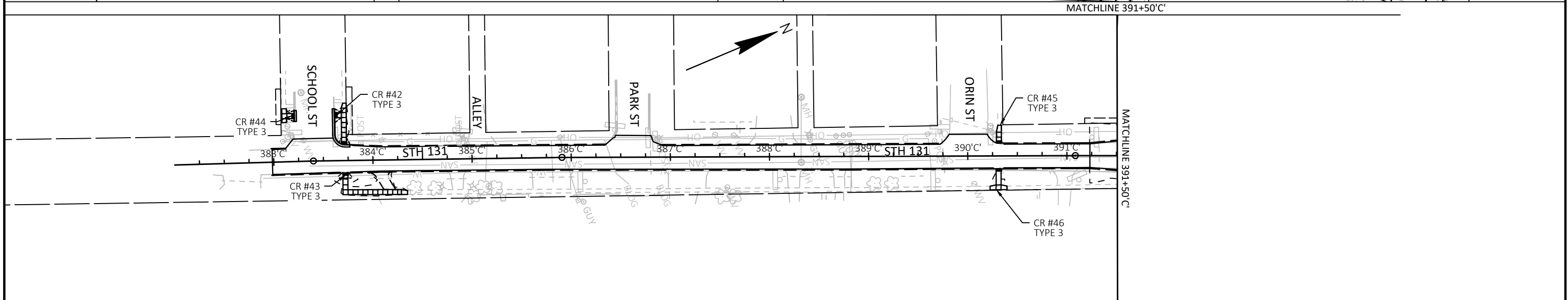
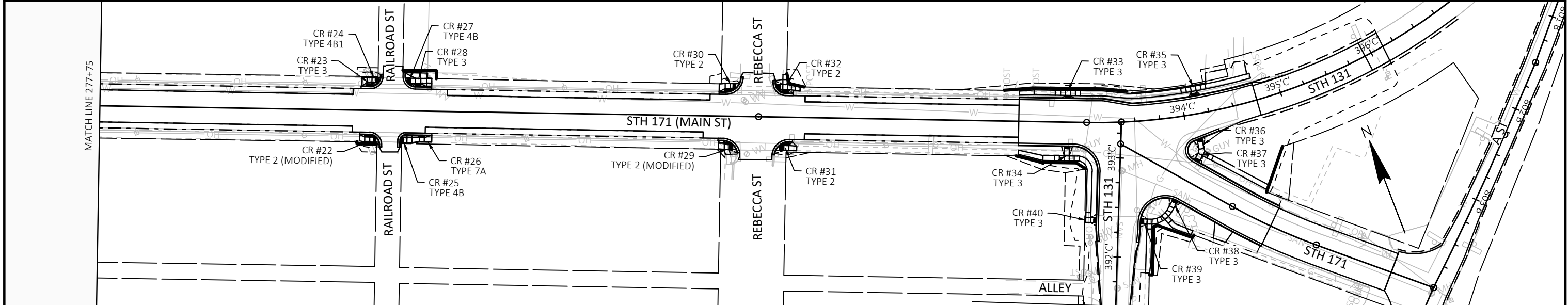
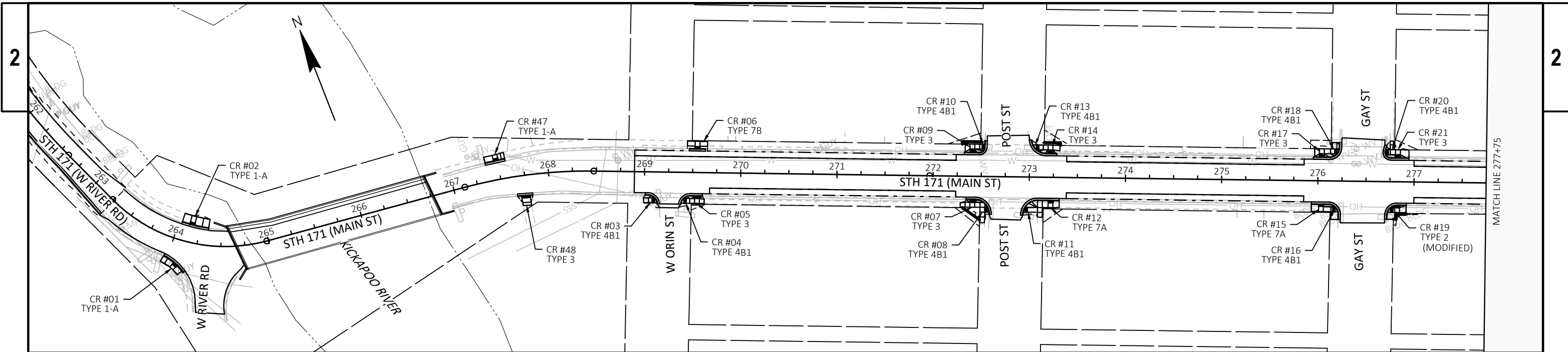
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PLAN DETAILS	SHEET	E
------------------------	--------------	------------------	--------------	-------	---



PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PLAN DETAILS	SHEET	E
------------------------	--------------	------------------	--------------	-------	---



PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PLAN DETAILS	SHEET	E
------------------------	--------------	------------------	--------------	-------	---



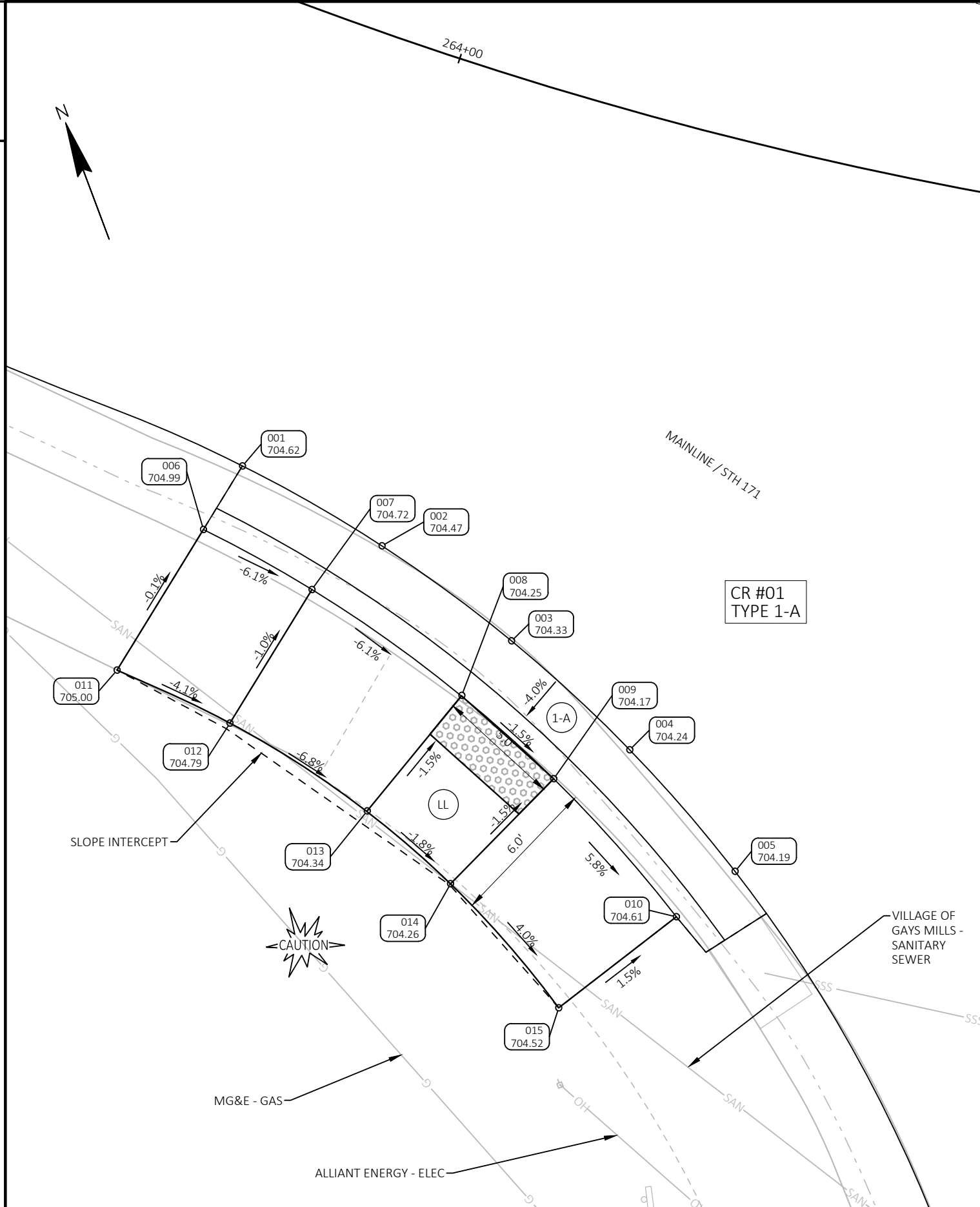
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CURB RAMP DETAILS - OVERVIEW	SHEET	E
------------------------	--------------	------------------	------------------------------	-------	---

- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

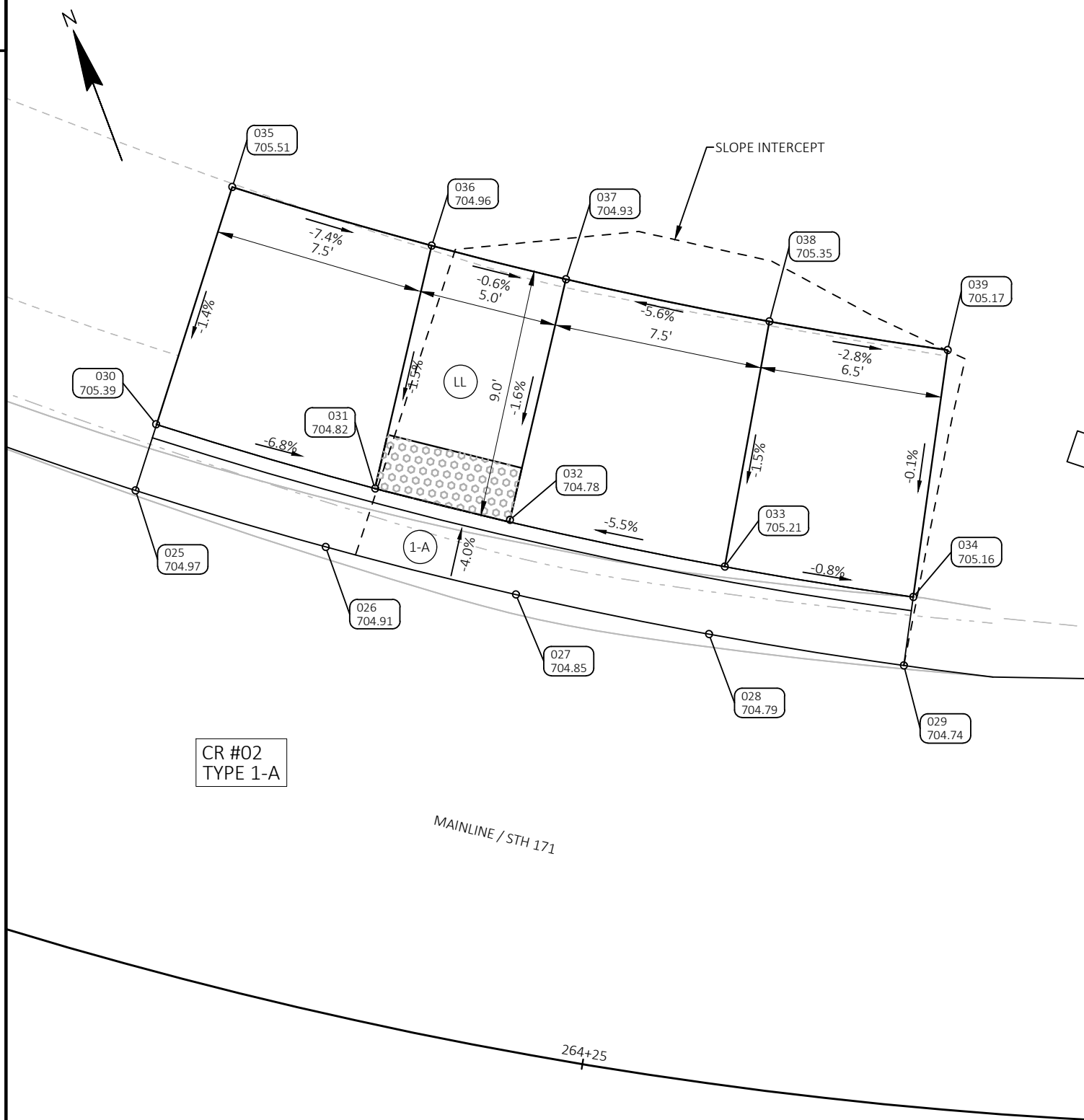
R RIVER RD SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
001	263+97.10	18.38' RT	704.62	219632.17	396172.32
002	264+02.80	19.69' RT	704.47	219627.06	396176.32
003	264+08.21	21.91' RT	704.33	219621.54	396179.72
004	264+13.22	24.97' RT	704.24	219615.67	396182.49
005	264+17.72	28.81' RT	704.19	219609.53	396184.59
006	263+96.53	21.32' RT	704.99	219630.39	396169.90
007	264+00.88	22.22' RT	704.72	219626.49	396173.04
008	264+07.04	24.56' RT	704.25	219620.26	396177.04
009	264+10.89	26.85' RT	704.17	219615.75	396179.21
010	264+16.09	31.11' RT	704.61	219608.73	396181.70
011	263+95.33	27.83' RT	705.00	219626.44	396164.53
012	263+99.60	28.37' RT	704.79	219622.75	396167.95
013	264+04.99	30.09' RT	704.34	219617.38	396171.75
014	264+08.39	32.01' RT	704.26	219613.39	396173.76
015	264+12.94	35.76' RT	704.52	219607.11	396175.92



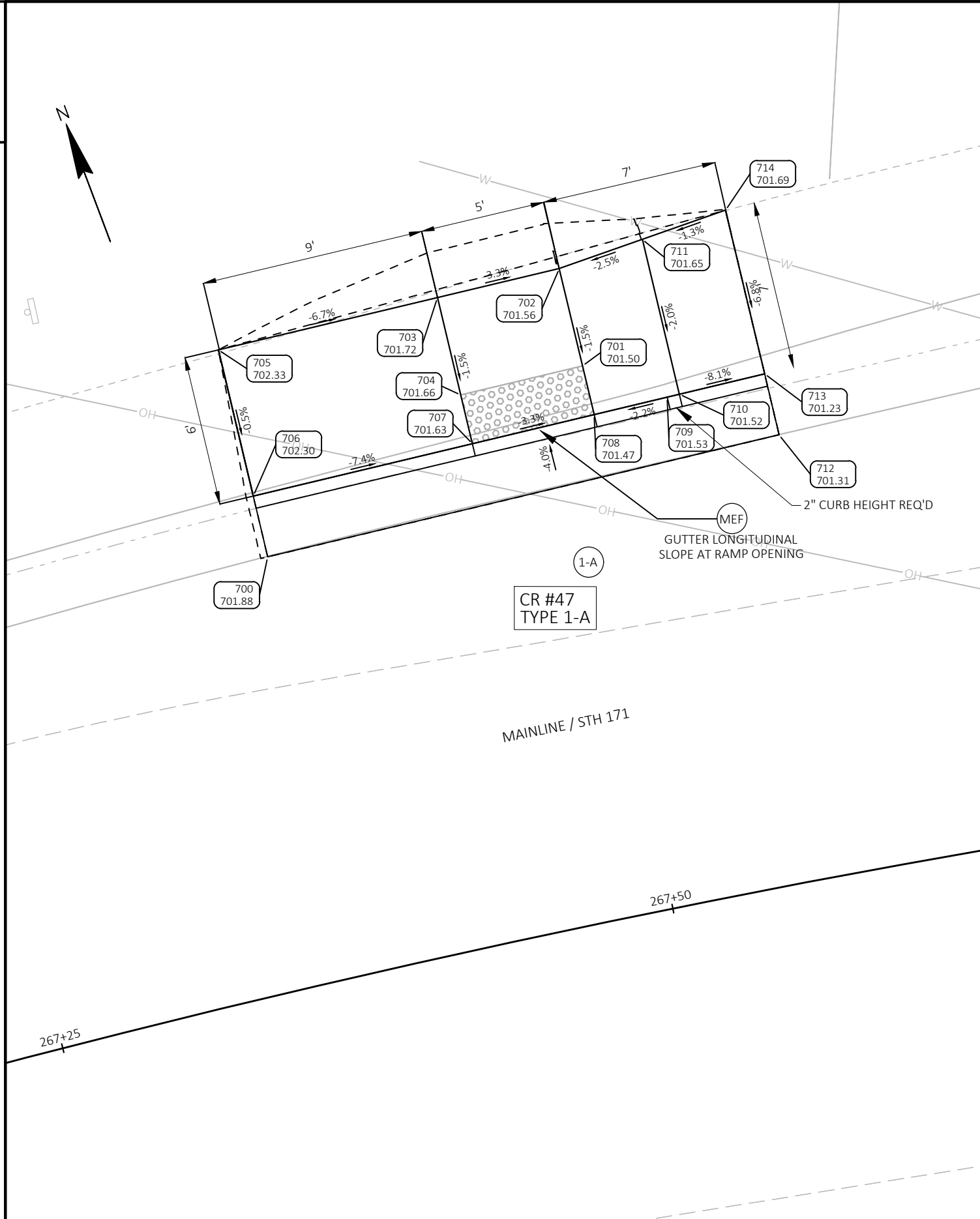
- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

- (X) CURB RAMP TYPE
- (PED) CONCRETE PEDESTRIAN CURB
- (LL) LEVEL LANDING
- (G) GRADED FLARE
- (P) PAVED FLARE
- (ID) POINT ID/ELEVATION



W RIVER RD NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
025	264+03.46	16.51' LT	704.97	219654.71	396199.70
026	264+11.42	16.39' LT	704.91	219650.23	396205.30
027	264+19.27	16.24' LT	704.85	219646.06	396211.01
028	264+27.16	16.05' LT	704.79	219642.11	396216.93
029	264+35.05	15.83' LT	704.74	219638.41	396223.03
030	264+03.48	19.01' LT	705.39	219656.64	396201.29
031	264+12.79	18.87' LT	704.82	219651.52	396207.74
032	264+18.43	18.83' LT	704.78	219648.63	396211.82
033	264+27.38	18.54' LT	705.21	219644.15	396218.38
034	264+35.14	18.32' LT	705.16	219640.57	396224.27
035	264+03.64	28.01' LT	705.51	219663.54	396207.06
036	264+12.68	27.87' LT	704.96	219658.87	396212.94
037	264+18.69	27.75' LT	704.93	219655.92	396216.97
038	264+27.70	27.54' LT	705.35	219651.75	396223.20
039	264+35.51	27.32' LT	705.17	219648.37	396228.78

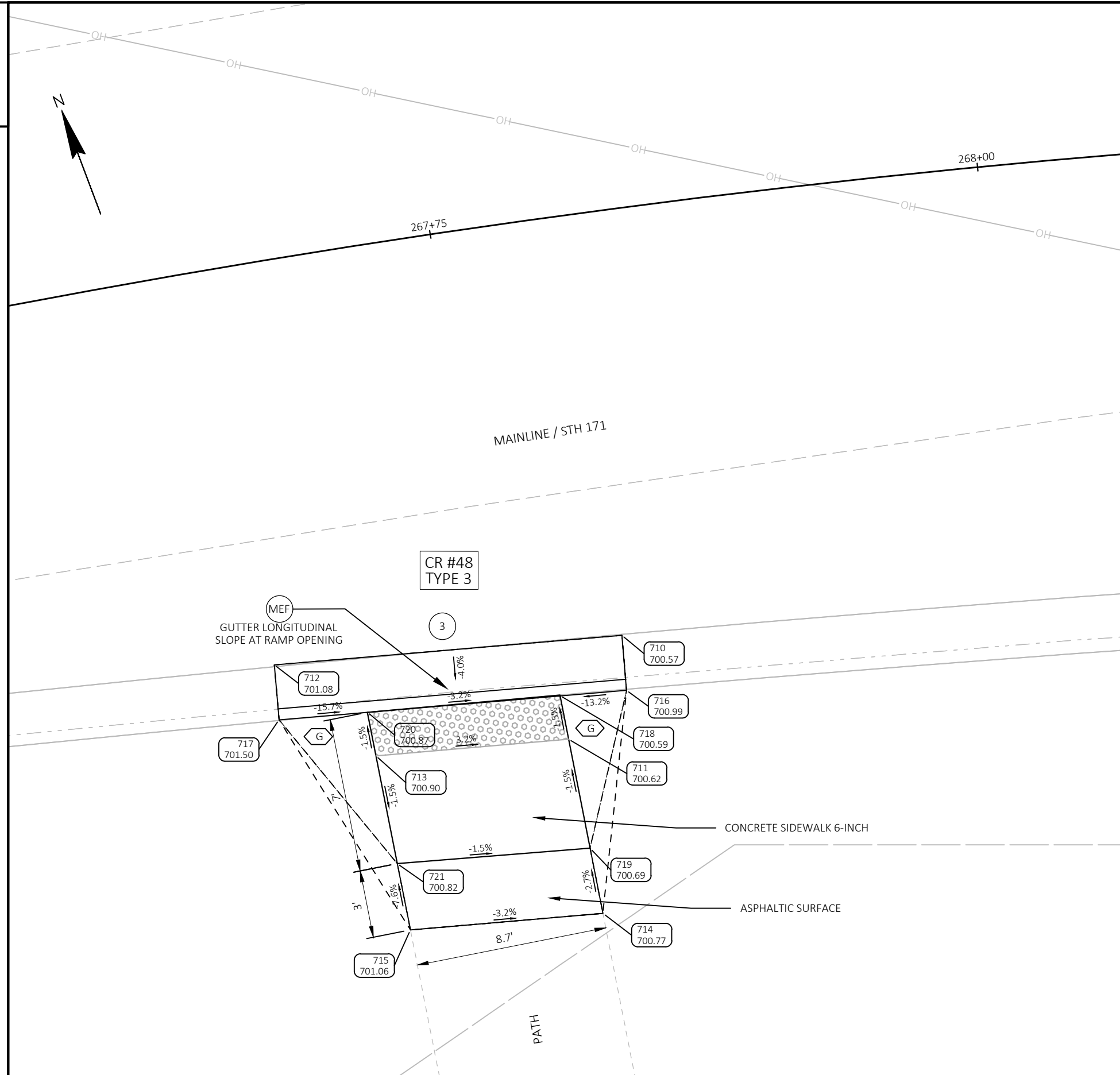


- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

ROBB PARK NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
700	267+37.36	17.14' LT	701.88	219596.54	396516.00
701	267+50.72	21.96' LT	701.50	219598.88	396530.52
702	267+50.58	25.96' LT	701.56	219602.84	396531.12
703	267+45.84	25.80' LT	701.72	219603.59	396526.18
704	267+45.94	21.80' LT	701.66	219599.64	396525.57
705	267+37.29	25.64' LT	702.33	219604.95	396517.28
706	267+37.34	19.64' LT	702.30	219599.01	396516.38
707	267+45.99	19.80' LT	701.63	219597.66	396525.27
708	267+50.79	19.96' LT	701.47	219596.90	396530.21
709	267+53.67	20.08' LT	701.53	219596.45	396533.18
710	267+54.15	20.10' LT	701.52	219596.38	396533.67
711	267+53.89	26.46' LT	701.65	219602.67	396534.63
712	267+57.62	17.77' LT	701.31	219593.38	396536.75
713	267+57.51	20.27' LT	701.23	219595.85	396537.14
714	267+57.18	26.98' LT	701.69	219602.50	396538.15



- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

ROBB PARK S					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
710	267+81.15	19.22' RT	700.57	219552.07	396551.35
711	267+77.97	23.56' RT	700.62	219548.61	396547.34
712	267+64.75	18.16' RT	701.08	219556.75	396536.25
713	267+68.79	22.98' RT	700.90	219551.20	396538.97
714	267+78.42	31.59' RT	700.77	219540.71	396545.81
715	267+69.08	31.03' RT	701.06	219543.30	396537.44
716	267+81.02	21.72' RT	700.99	219549.68	396550.61
717	267+64.53	20.65' RT	701.50	219554.36	396535.51
718	267+77.86	21.55' RT	700.59	219550.59	396547.73
719	267+78.25	28.58' RT	700.69	219543.67	396546.38
720	267+68.71	20.97' RT	700.87	219553.17	396539.35
721	267+68.97	27.96' RT	700.82	219546.31	396538.02



269+00

269+25

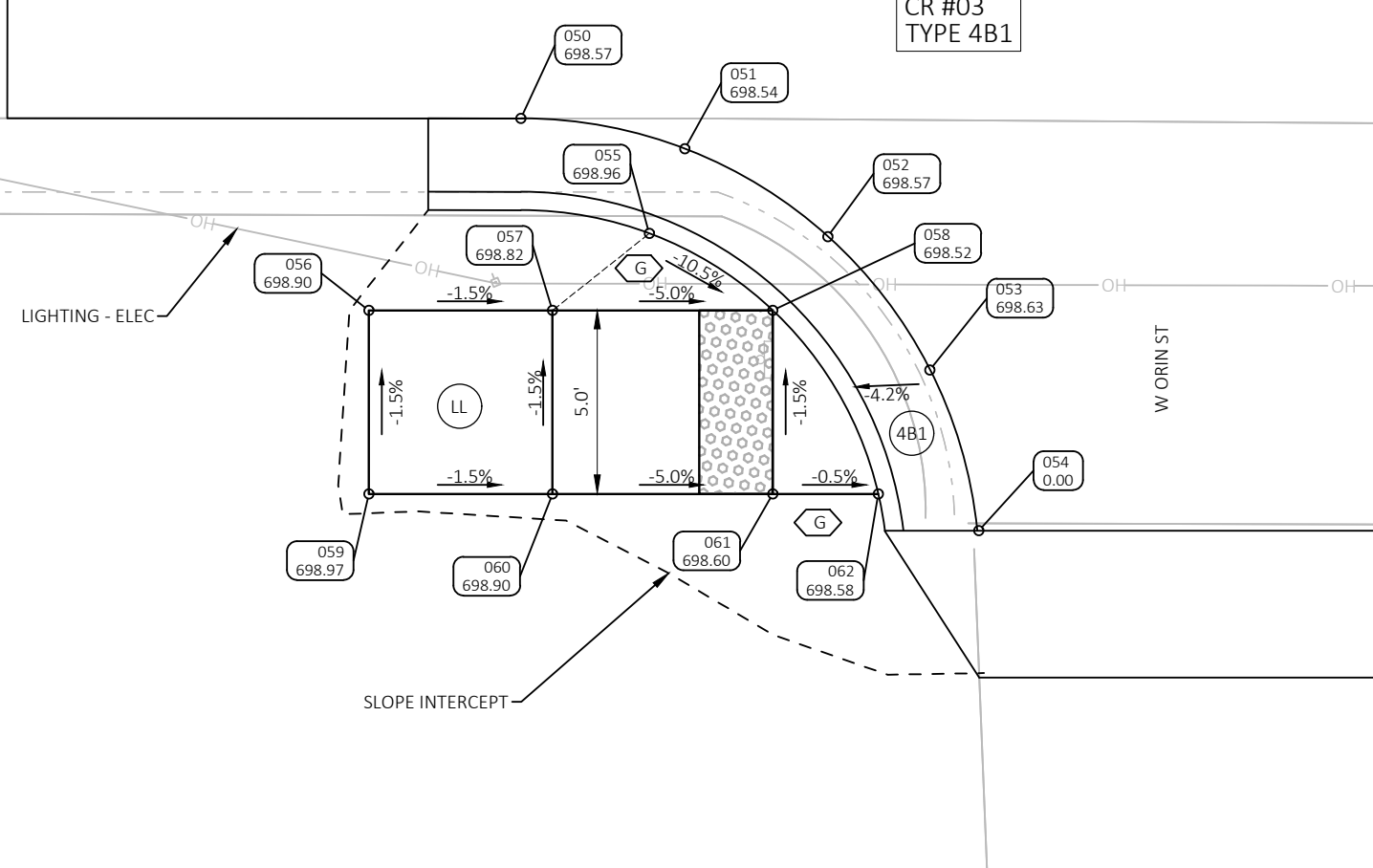
- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

W ORIN ST SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
050	269+03.47	21.77' RT	698.57	219508.70	396662.74
051	269+07.93	22.60' RT	698.54	219506.25	396666.56
052	269+11.82	24.99' RT	698.57	219502.57	396669.27
053	269+14.60	28.62' RT	698.63	219498.16	396670.47
054	269+15.93	33.00' RT	0.00	219493.60	396670.06
055	269+06.96	24.90' RT	698.96	219504.48	396664.79
056	268+99.33	27.00' RT	698.90	219505.41	396656.93
057	269+04.33	27.00' RT	698.82	219503.53	396661.57
058	269+10.33	27.00' RT	698.52	219501.27	396667.12
059	268+99.33	32.00' RT	698.97	219500.78	396655.05
060	269+04.33	32.00' RT	698.90	219498.90	396659.68
061	269+10.33	32.00' RT	698.60	219496.64	396665.24
062	269+13.20	32.00' RT	698.58	219495.56	396667.90

CR #03
TYPE 4B1



269+25 269+50



- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

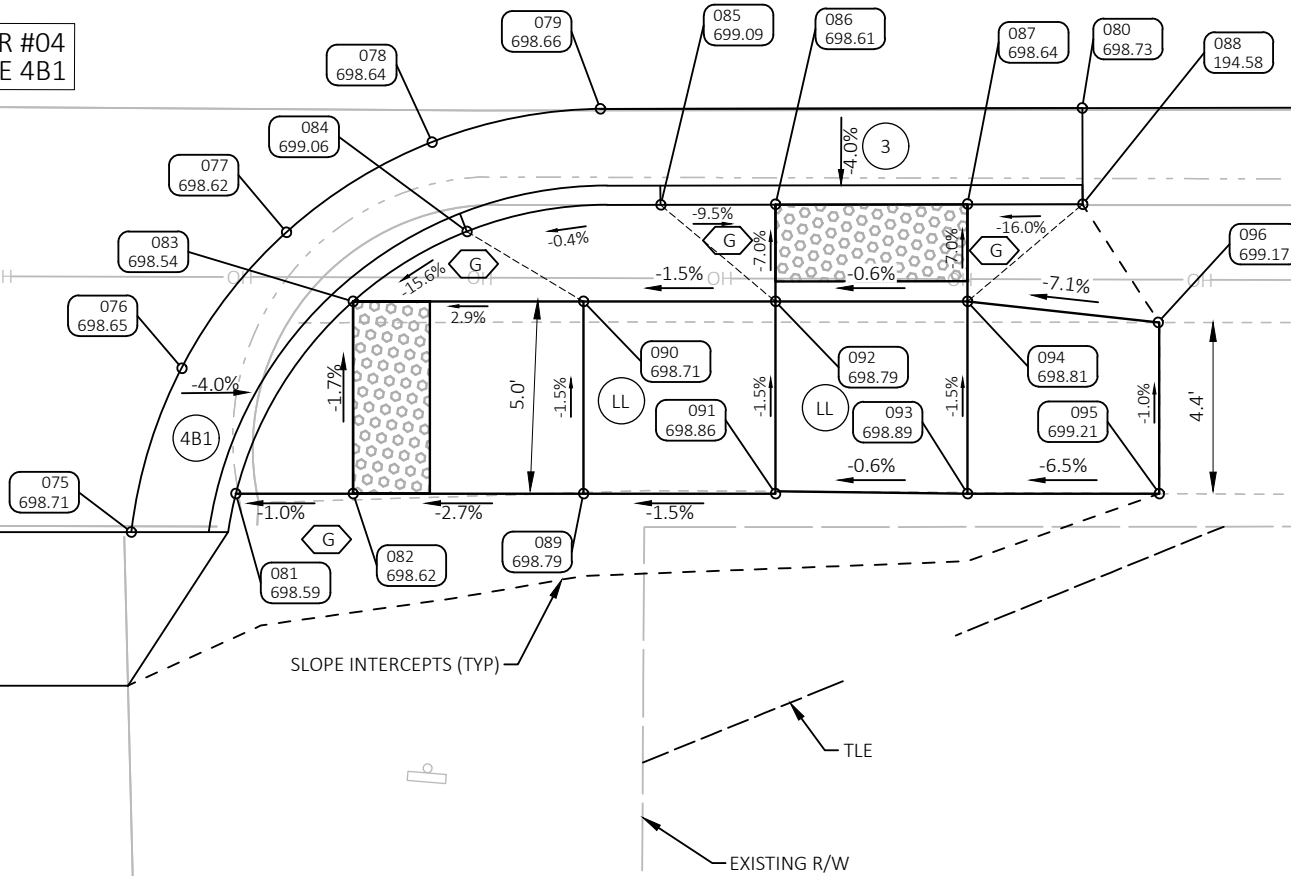
MAINLINE / STH 171

CR #05
TYPE 3

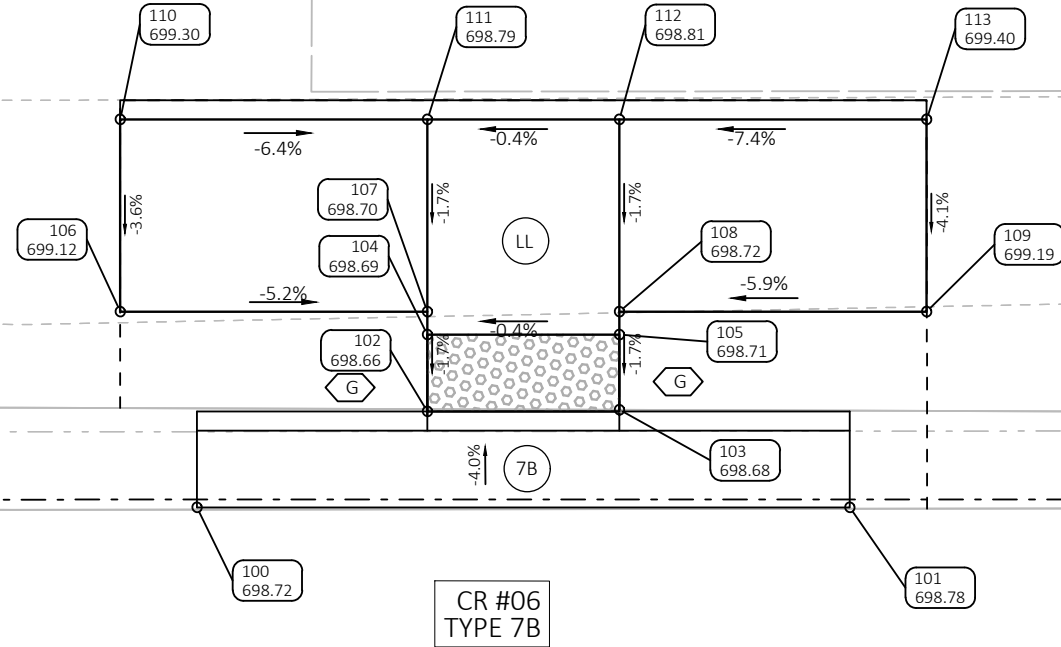
CR #04
TYPE 4B1

LIGHTING
ELEC

WORIN ST



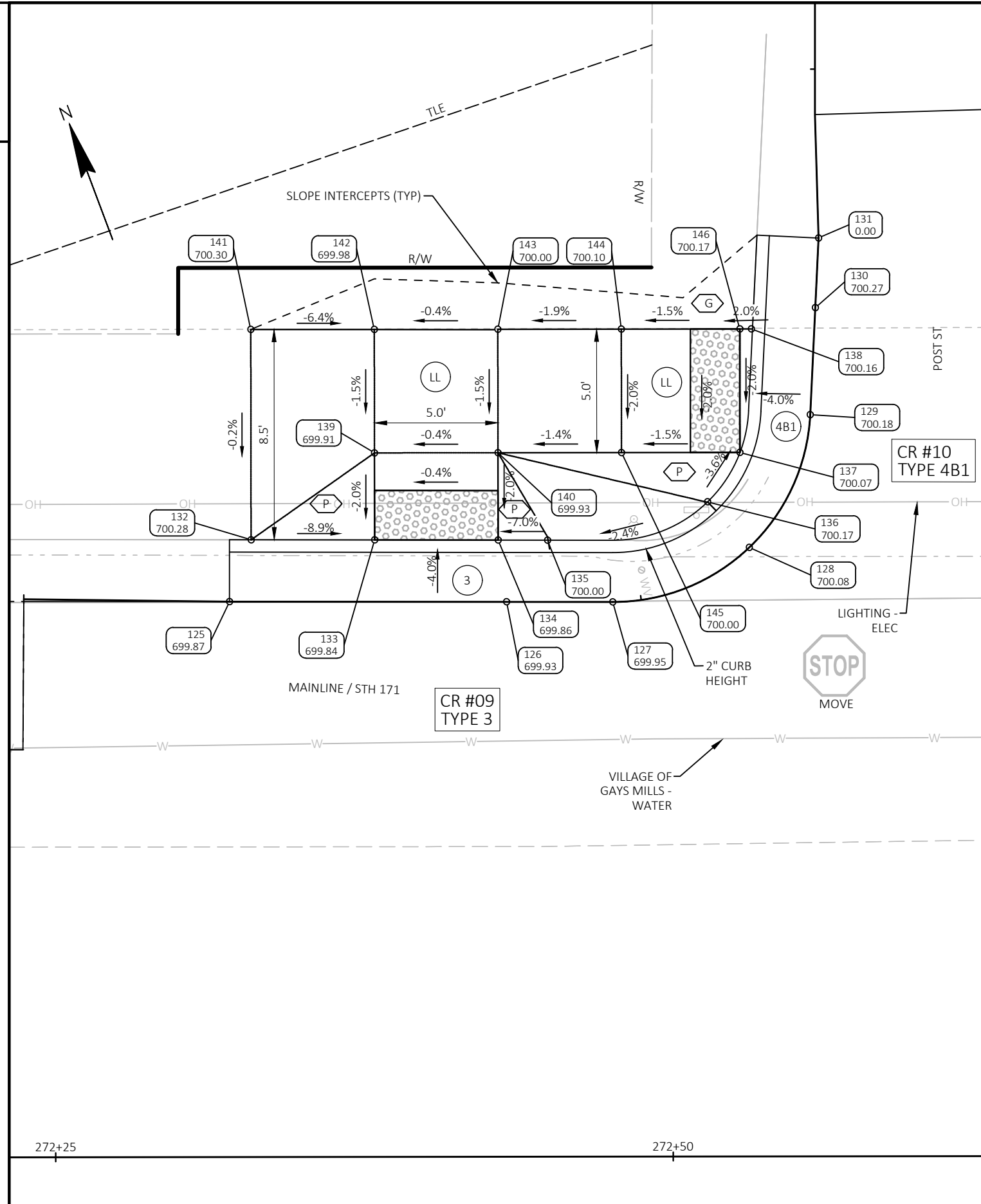
W ORIN ST SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
075	269+35.55	33.00' RT	698.71	219486.22	396688.23
076	269+36.87	28.74' RT	698.65	219489.68	396691.06
077	269+39.59	25.20' RT	698.62	219491.93	396694.91
078	269+43.38	22.85' RT	698.64	219492.68	396699.31
079	269+47.76	21.98' RT	698.66	219491.83	396703.70
080	269+60.32	21.96' RT	698.73	219487.13	396715.33
081	269+38.27	32.00' RT	698.59	219486.13	396691.13
082	269+41.32	32.00' RT	698.62	219484.98	396693.96
083	269+41.32	27.00' RT	698.54	219489.61	396695.84
084	269+44.29	25.18' RT	699.06	219490.18	396699.28
085	269+49.34	24.48' RT	699.09	219488.93	396704.22
086	269+52.32	24.47' RT	698.61	219487.81	396706.99
087	269+57.32	24.46' RT	698.64	219485.94	396711.62
088	269+60.32	24.47' RT	194.58	219484.81	396714.40
089	269+47.32	32.00' RT	698.79	219482.72	396699.52
090	269+47.32	27.00' RT	698.71	219487.35	396701.40
091	269+52.32	32.00' RT	698.86	219480.84	396704.15
092	269+52.32	27.00' RT	698.79	219485.47	396706.03
093	269+57.32	32.00' RT	698.89	219478.96	396708.79
094	269+57.32	27.00' RT	698.81	219483.59	396710.67
095	269+62.32	32.00' RT	699.21	219477.08	396713.42
096	269+62.29	27.54' RT	699.17	219481.22	396715.07



- NOTES:
- CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 - THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 - SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 - ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND	
(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

STH 171 (NORTH ORIN ST)					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
100	269+46.13	22.30' LT	698.72	219533.48	396718.84
101	269+63.13	22.30' LT	698.78	219527.08	396734.59
102	269+52.13	24.80' LT	698.66	219533.54	396725.34
103	269+57.13	24.84' LT	698.68	219531.70	396729.99
104	269+52.13	26.80' LT	698.69	219535.39	396726.09
105	269+57.13	26.80' LT	698.71	219533.51	396730.73
106	269+44.13	27.40' LT	699.12	219538.96	396718.91
107	269+52.13	27.40' LT	698.70	219535.95	396726.32
108	269+57.13	27.40' LT	698.72	219534.07	396730.95
109	269+65.13	27.40' LT	699.19	219531.06	396738.37
110	269+44.13	32.40' LT	699.30	219543.59	396720.79
111	269+52.13	32.40' LT	698.79	219540.58	396728.20
112	269+57.13	32.40' LT	698.81	219538.70	396732.83
113	269+65.13	32.40' LT	699.40	219535.69	396740.25



- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

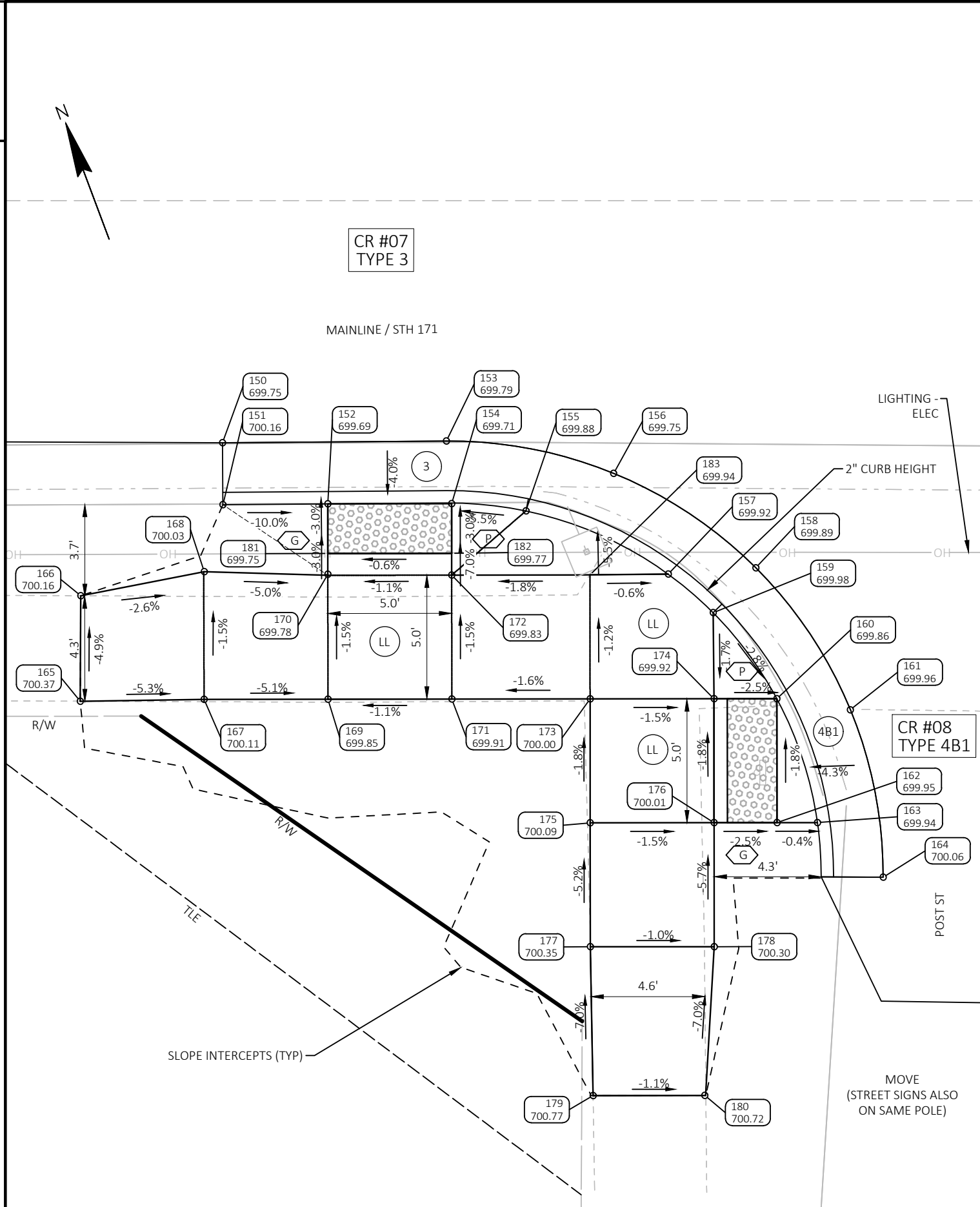
POST ST NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
125	272+32.07	22.49' LT	699.87	219426.12	396983.83
126	272+43.28	22.46' LT	699.93	219421.88	396994.22
127	272+47.58	22.45' LT	699.95	219420.26	396998.19
128	272+53.10	24.65' LT	700.08	219420.23	397004.15
129	272+55.58	30.03' LT	700.18	219424.28	397008.46
130	272+55.79	34.36' LT	700.27	219428.21	397010.28
131	272+55.93	37.18' LT	0.00	219430.78	397011.46
132	272+32.94	24.99' LT	700.28	219428.10	396985.58
133	272+37.94	24.97' LT	699.84	219426.22	396990.21
134	272+42.94	24.96' LT	699.86	219424.33	396994.84
135	272+44.94	24.96' LT	700.00	219423.58	396996.69
136	272+51.42	26.50' LT	700.17	219422.57	397003.28
137	272+52.73	28.50' LT	700.07	219423.93	397005.24
138	272+53.20	33.50' LT	700.16	219428.39	397007.55
139	272+37.94	28.50' LT	699.91	219429.48	396991.53
140	272+42.94	28.50' LT	699.93	219427.61	396996.17
141	272+32.94	33.50' LT	700.30	219435.99	396988.77
142	272+37.94	33.50' LT	699.98	219434.12	396993.41
143	272+42.94	33.50' LT	700.00	219432.24	396998.04
144	272+47.94	33.50' LT	700.10	219430.37	397002.68
145	272+47.94	28.50' LT	700.00	219425.73	397000.80
146	272+52.73	33.50' LT	700.17	219428.57	397007.12

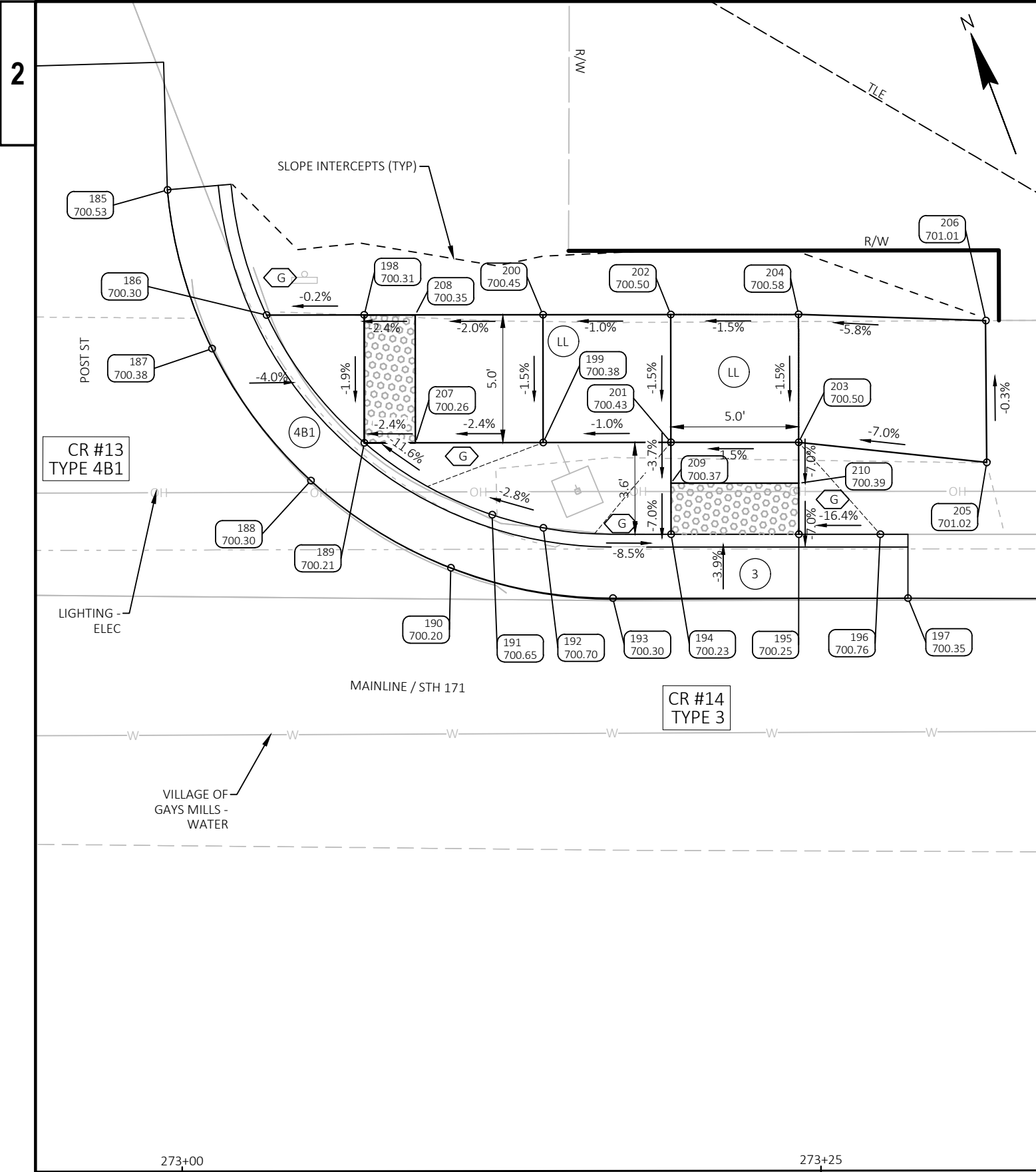
- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

- (X) CURB RAMP TYPE
- (PED) CONCRETE PEDESTRIAN CURB
- (LL) LEVEL LANDING
- (G) GRADED FLARE
- (P) PAVED FLARE
- (ID ELEV) POINT ID/ELEVATION

POST ST SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
150	272+34.30	21.65' RT	699.75	219384.37	396969.34
151	272+34.31	24.15' RT	700.16	219382.04	396968.42
152	272+38.54	24.12' RT	699.69	219380.48	396972.35
153	272+43.33	21.59' RT	699.79	219381.03	396977.74
154	272+43.54	24.12' RT	699.71	219378.61	396976.98
155	272+46.54	24.41' RT	699.88	219377.21	396979.65
156	272+50.08	22.91' RT	699.75	219377.28	396983.50
157	272+52.28	26.97' RT	699.92	219372.68	396984.02
158	272+55.82	26.72' RT	699.89	219371.59	396987.39
159	272+54.09	28.53' RT	699.98	219370.56	396985.11
160	272+56.66	32.00' RT	699.86	219366.38	396986.19
161	272+59.63	32.45' RT	699.96	219364.85	396988.77
162	272+56.66	37.00' RT	699.95	219361.74	396984.31
163	272+58.30	37.00' RT	699.94	219361.13	396985.83
164	272+60.94	39.21' RT	700.06	219358.09	396987.45
165	272+28.54	32.07' RT	700.37	219376.87	396960.10
166	272+28.57	27.81' RT	700.16	219380.80	396961.72
167	272+33.54	32.00' RT	700.11	219375.05	396964.76
168	272+33.55	26.85' RT	700.03	219379.83	396966.70
169	272+38.54	32.00' RT	699.85	219373.18	396969.39
170	272+38.54	26.96' RT	699.78	219377.85	396971.28
171	272+43.54	32.00' RT	699.91	219371.30	396974.03
172	272+43.54	27.00' RT	699.83	219375.94	396975.90
173	272+49.12	32.00' RT	700.00	219369.21	396979.20
174	272+54.12	32.00' RT	699.92	219367.33	396983.84
175	272+49.12	37.00' RT	700.09	219364.57	396977.33
176	272+54.12	37.00' RT	700.01	219362.70	396981.96
177	272+49.12	42.00' RT	700.35	219359.94	396975.45
178	272+54.12	42.00' RT	700.30	219358.06	396980.08
179	272+49.22	48.00' RT	700.77	219354.34	396973.29
180	272+53.73	48.00' RT	700.72	219352.65	396977.47
181	272+38.54	26.12' RT	699.75	219378.63	396971.60
182	272+43.54	26.12' RT	699.77	219376.75	396976.23
183	272+49.12	27.00' RT	699.94	219373.84	396981.08





- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

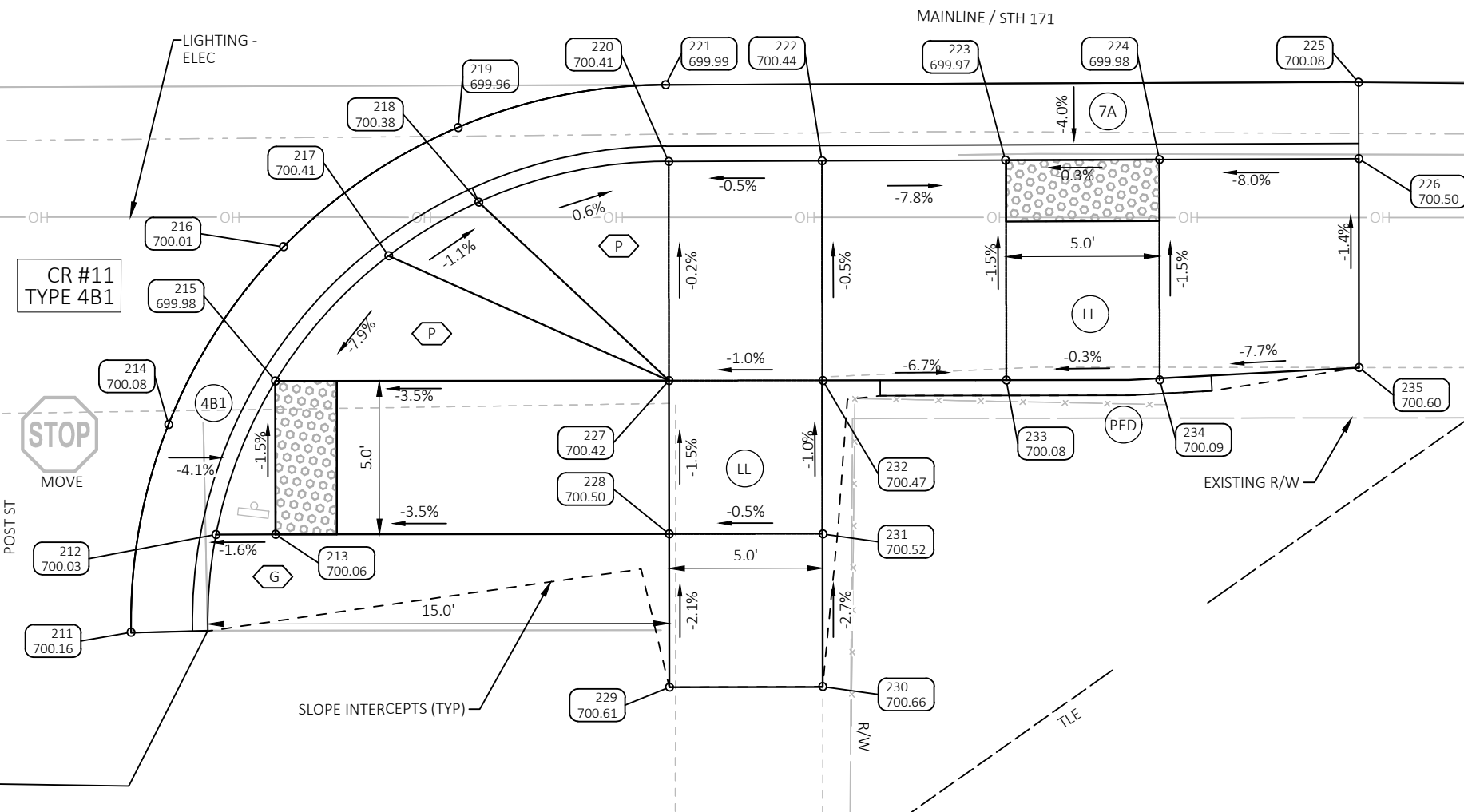
LEGEND

- (X) CURB RAMP TYPE
- (PED) CONCRETE PEDESTRIAN CURB
- (LL) LEVEL LANDING
- (G) GRADED FLARE
- (P) PAVED FLARE
- (ID ELEV) POINT ID/ELEVATION

POST ST NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
185	272+99.46	38.40' LT	700.53	219415.58	397052.28
186	273+03.34	33.50' LT	700.30	219409.59	397054.03
187	273+01.20	32.19' LT	700.38	219409.17	397051.56
188	273+05.06	27.02' LT	700.30	219402.93	397053.20
189	273+07.16	28.50' LT	700.21	219403.52	397055.70
190	273+10.54	23.60' LT	700.20	219397.71	397056.99
191	273+12.16	25.68' LT	700.65	219399.02	397059.27
192	273+14.16	25.16' LT	700.70	219397.79	397060.93
193	273+16.88	22.41' LT	700.30	219394.22	397062.42
194	273+19.16	24.90' LT	700.23	219395.68	397065.47
195	273+24.16	24.90' LT	700.25	219393.80	397070.10
196	273+27.34	24.90' LT	700.76	219392.61	397073.06
197	273+28.43	22.39' LT	700.35	219389.88	397073.12
198	273+07.16	33.50' LT	700.31	219408.15	397057.57
199	273+14.16	28.50' LT	700.38	219400.89	397062.18
200	273+14.16	33.50' LT	700.45	219405.53	397064.06
201	273+19.16	28.50' LT	700.43	219399.02	397066.82
202	273+19.16	33.50' LT	700.50	219403.65	397068.69
203	273+24.16	28.50' LT	700.50	219397.14	397071.45
204	273+24.16	33.50' LT	700.58	219401.77	397073.33
205	273+31.51	27.70' LT	701.02	219393.64	397077.97
206	273+31.49	33.25' LT	701.01	219398.79	397080.03
207	273+09.16	28.50' LT	700.26	219402.77	397057.55
208	273+09.16	33.50' LT	700.35	219407.40	397059.42
209	273+19.16	26.90' LT	700.37	219397.54	397066.22
210	273+24.16	26.90' LT	700.39	219395.66	397070.85



273+00 273+25

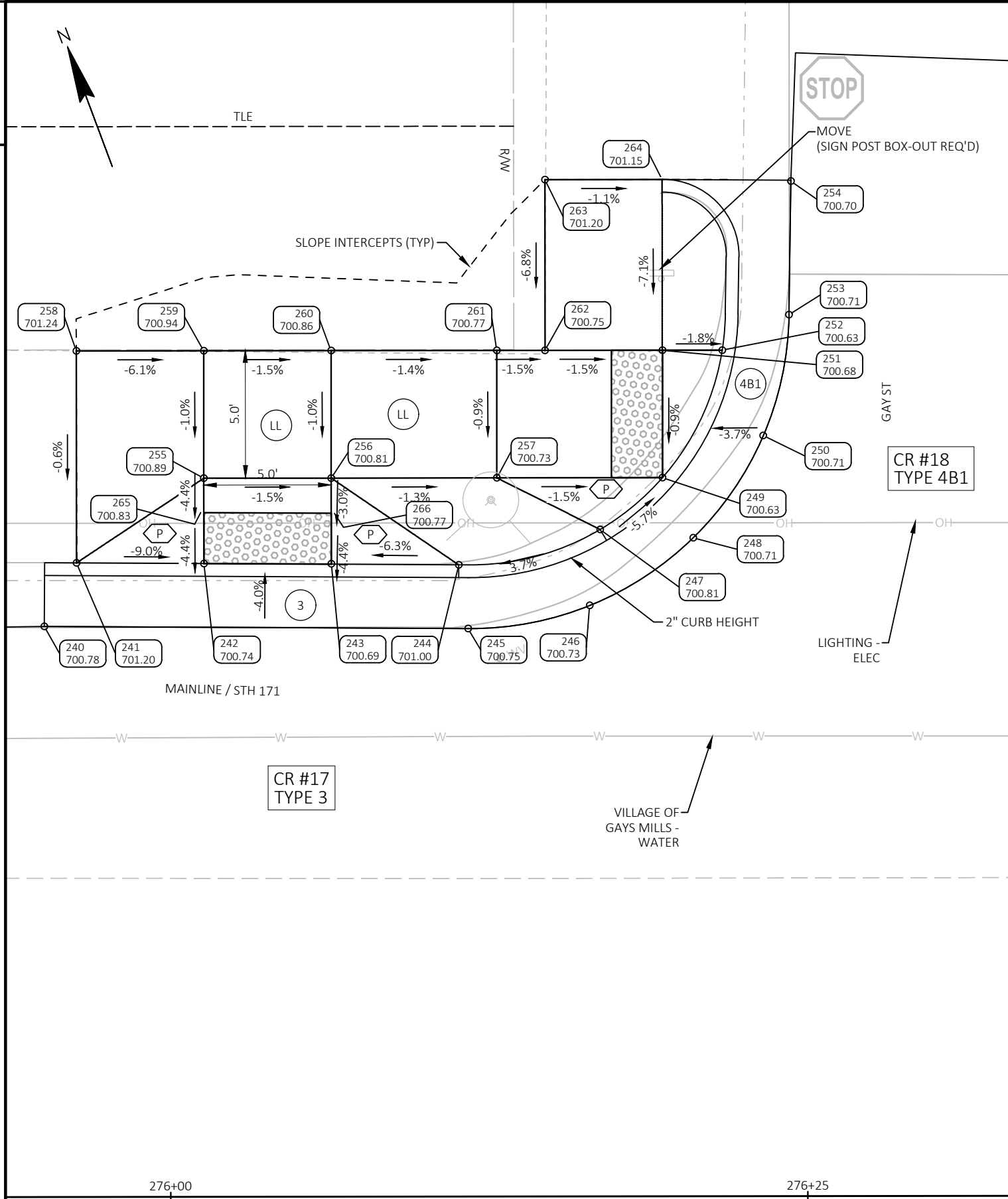


- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

POST ST SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
211	272+91.34	39.68' RT	700.16	219346.25	397015.45
212	272+94.11	36.50' RT	700.03	219348.16	397019.22
213	272+96.06	36.50' RT	700.06	219347.43	397021.02
214	272+92.58	32.91' RT	700.08	219352.06	397019.14
215	272+96.06	31.50' RT	699.98	219352.06	397022.90
216	272+96.33	27.12' RT	700.01	219356.02	397024.79
217	272+99.76	27.42' RT	700.41	219354.46	397027.85
218	273+02.69	25.67' RT	700.38	219354.98	397031.23
219	273+02.03	23.24' RT	699.96	219357.48	397031.53
220	273+08.89	24.36' RT	700.41	219353.87	397037.47
221	273+08.79	21.86' RT	699.99	219356.23	397038.32
222	273+13.89	24.34' RT	700.44	219352.01	397042.11
223	273+19.87	24.33' RT	699.97	219349.78	397047.66
224	273+24.89	24.31' RT	699.98	219347.91	397052.32
225	273+31.39	21.80' RT	700.08	219347.80	397059.29
226	273+31.39	24.29' RT	700.50	219345.49	397058.35
227	273+08.89	31.50' RT	700.42	219347.25	397034.79
228	273+08.89	36.50' RT	700.50	219342.61	397032.92
229	273+08.89	41.50' RT	700.61	219337.98	397031.04
230	273+13.89	41.49' RT	700.66	219336.12	397035.68
231	273+13.91	36.51' RT	700.52	219340.73	397037.56
232	273+13.91	31.51' RT	700.47	219345.36	397039.45
233	273+19.89	31.50' RT	700.08	219343.12	397044.99
234	273+24.89	31.50' RT	700.09	219341.25	397049.62
235	273+31.39	31.10' RT	700.60	219339.18	397055.80



- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
ID ELEV	POINT ID/ELEVATION

GAY ST NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
240	275+95.06	22.39' LT	700.78	219289.85	397320.28
241	275+96.33	24.88' LT	701.20	219291.69	397322.39
242	276+01.33	24.85' LT	700.74	219289.78	397327.01
243	276+06.33	24.84' LT	700.69	219287.89	397331.64
244	276+11.33	24.79' LT	701.00	219285.97	397336.26
245	276+11.69	22.29' LT	700.75	219283.52	397335.66
246	276+16.47	23.19' LT	700.73	219282.56	397340.42
247	276+16.88	26.17' LT	700.81	219285.17	397341.93
248	276+20.53	25.84' LT	700.71	219283.49	397345.19
249	276+19.33	28.20' LT	700.63	219286.13	397344.96
250	276+23.28	29.85' LT	700.71	219286.18	397349.24
251	276+19.33	33.20' LT	700.68	219290.77	397346.83
252	276+21.68	33.20' LT	700.63	219289.89	397349.01
253	276+24.30	34.59' LT	700.71	219290.19	397351.96
254	276+24.38	39.83' LT	700.70	219295.02	397354.00
255	276+01.33	28.20' LT	700.89	219292.89	397328.27
256	276+06.33	28.20' LT	700.81	219291.01	397332.91
257	276+12.83	28.20' LT	700.73	219288.57	397338.93
258	275+96.33	33.20' LT	701.24	219299.40	397325.51
259	276+01.33	33.20' LT	700.94	219297.52	397330.15
260	276+06.33	33.20' LT	700.86	219295.64	397334.78
261	276+12.83	33.20' LT	700.77	219293.21	397340.81
262	276+14.73	33.20' LT	700.75	219292.49	397342.56
263	276+14.73	39.90' LT	701.20	219298.70	397345.08
264	276+19.33	39.90' LT	701.15	219296.98	397349.34
265	276+01.33	26.85' LT	700.83	219291.63	397327.76
266	276+06.33	26.82' LT	700.77	219289.73	397332.39



276+00

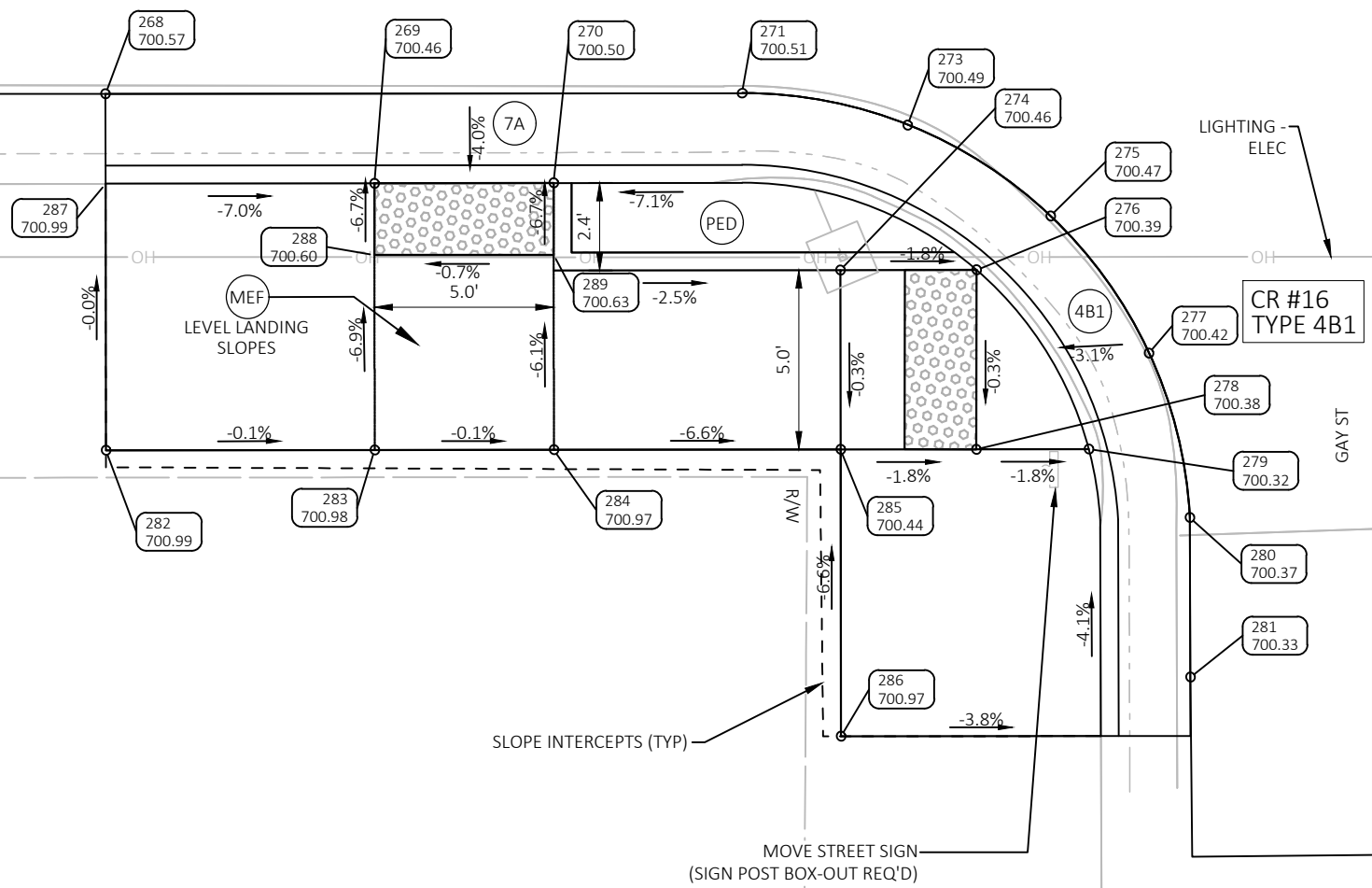
276+25

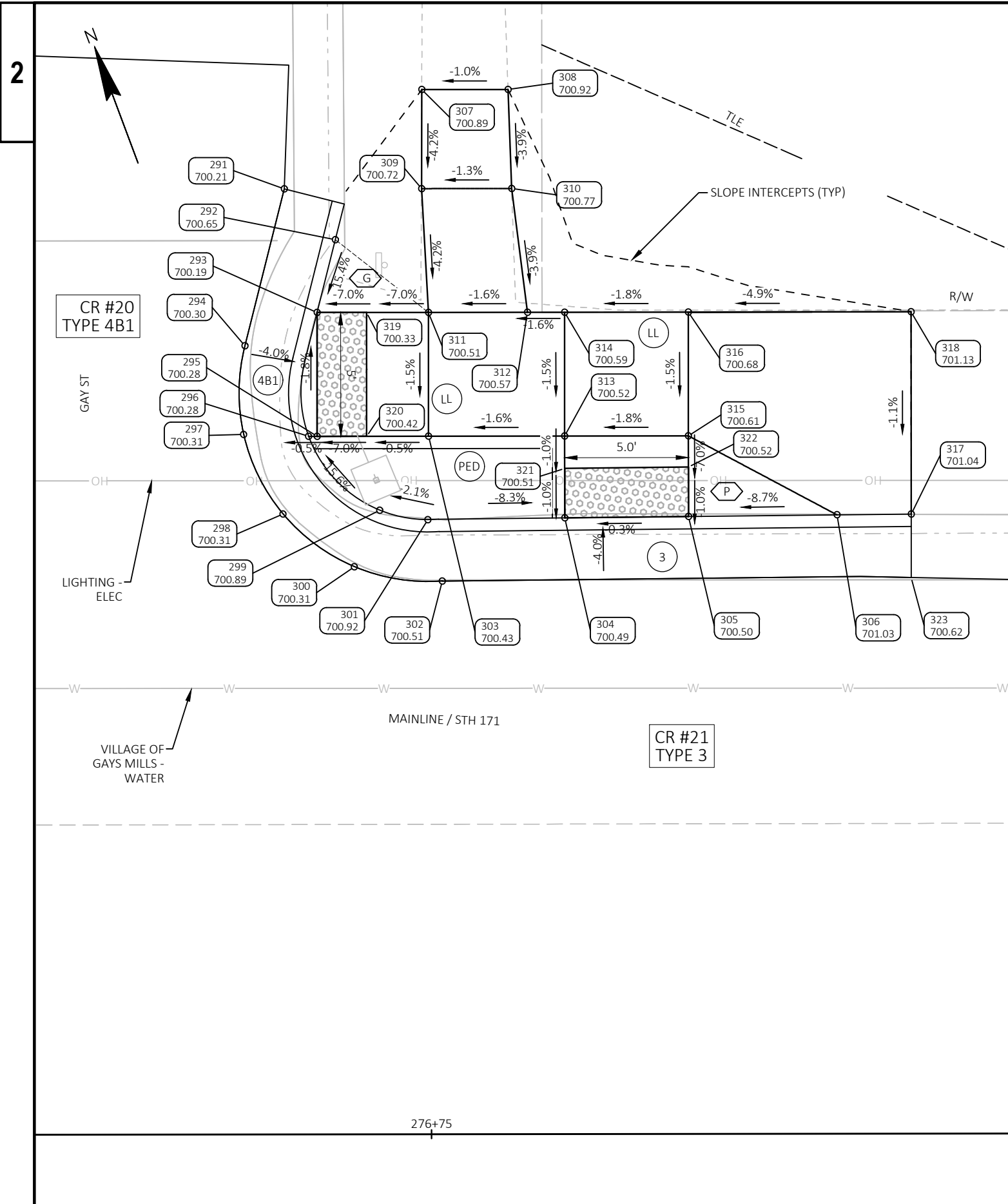
- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

GAY ST SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
268	275+93.54	22.06' RT	700.57	219249.22	397302.20
269	276+01.04	24.57' RT	700.46	219244.08	397308.21
270	276+06.04	24.56' RT	700.50	219242.21	397312.85
271	276+11.30	22.06' RT	700.51	219242.55	397318.66
273	276+15.92	22.96' RT	700.49	219239.99	397322.61
274	276+14.04	27.00' RT	700.46	219236.95	397319.35
275	276+19.91	25.50' RT	700.47	219236.14	397325.35
276	276+17.84	27.00' RT	700.39	219235.52	397322.87
277	276+22.65	29.32' RT	700.42	219231.57	397326.45
278	276+17.83	32.00' RT	700.38	219230.89	397320.98
279	276+20.97	32.00' RT	700.32	219229.71	397323.89
280	276+23.78	33.91' RT	700.37	219226.89	397325.78
281	276+23.79	38.36' RT	700.33	219222.76	397324.12
282	275+93.54	32.00' RT	700.99	219240.00	397298.47
283	276+01.04	32.00' RT	700.98	219237.19	397305.42
284	276+06.04	32.00' RT	700.97	219235.31	397310.06
285	276+14.04	32.00' RT	700.44	219232.31	397317.47
286	276+14.04	40.00' RT	700.97	219224.90	397314.47
287	275+93.54	24.57' RT	700.99	219246.89	397301.26
288	276+01.04	26.56' RT	700.60	219242.23	397307.46





- NOTES:
- CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 - THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 - SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 - ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

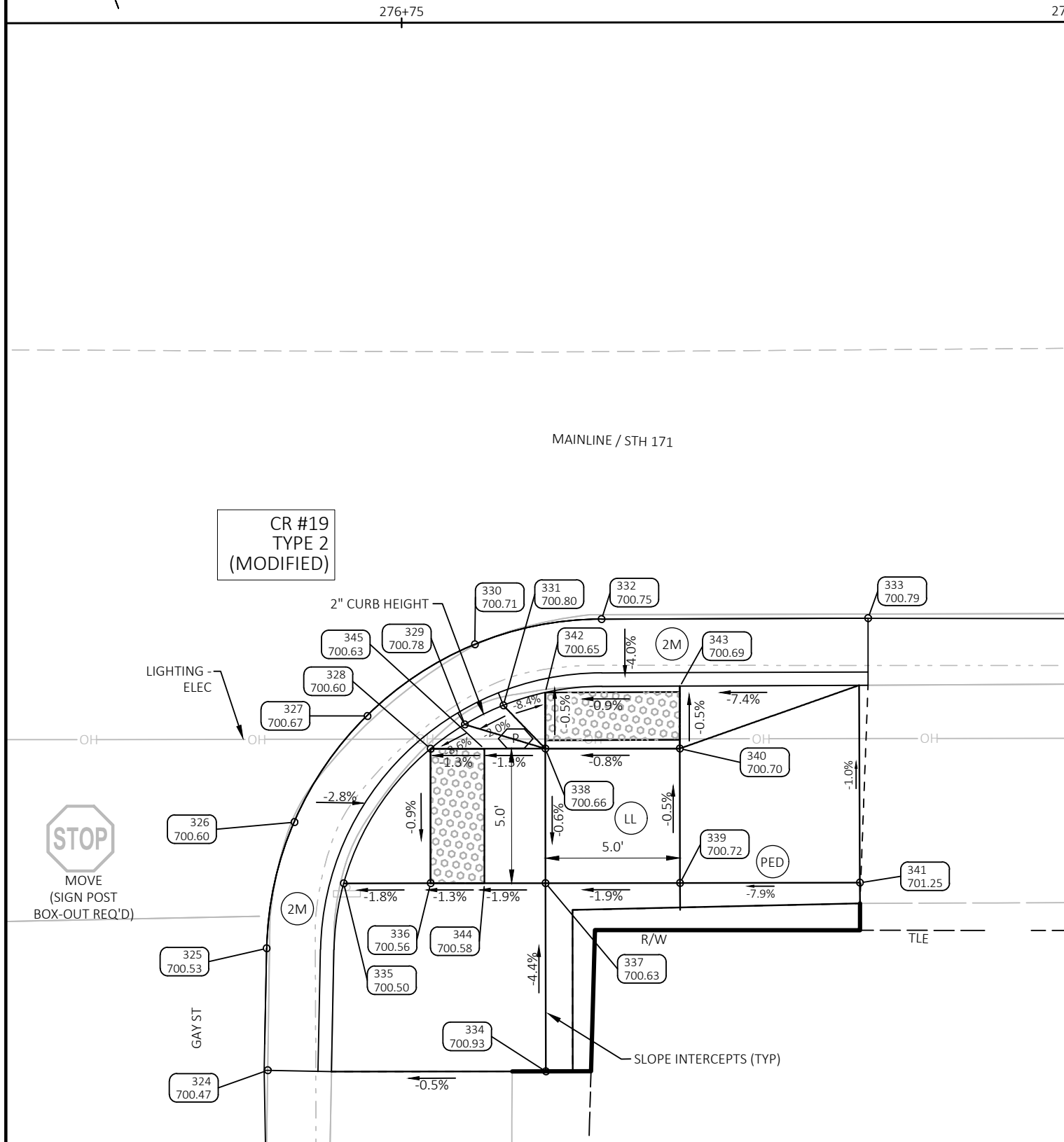
POINT NUMBER	GAY ST NE				POINT NUMBER	GAY ST NE				EASTING
	STATION	OFFSET	ELEVATION	NORTHING		STATION	OFFSET	ELEVATION	NORTHING	
291	276+69.09	38.20' LT	700.21	219276.73	397391.83	276+80.41	26.89' LT	700.51	219262.01	397401.08
292	276+71.15	36.14' LT	700.65	219274.05	397395.97	276+85.41	26.94' LT	700.52	219260.18	397405.74
293	276+70.41	33.20' LT	700.19	219271.61	397394.18	276+94.41	22.47' LT	700.62	219252.66	397412.40
294	276+67.49	31.86' LT	700.30	219271.46	397390.98					
295	276+70.41	28.20' LT	700.28	219266.97	397392.31					
296	276+70.06	28.20' LT	700.28	219267.10	397391.98					
297	276+67.44	28.28' LT	700.31	219268.16	397389.58					
298	276+69.02	25.06' LT	700.31	219264.58	397389.84					
299	276+72.93	25.21' LT	700.89	219263.25	397393.52					
300	276+71.90	22.93' LT	700.31	219261.52	397391.71					
301	276+74.88	24.82' LT	700.92	219262.16	397395.18					
302	276+75.46	22.35' LT	700.51	219259.65	397394.79					
303	276+74.91	28.20' LT	700.43	219265.28	397396.48					
304	276+80.41	24.89' LT	700.49	219260.15	397400.33					
305	276+85.41	24.94' LT	700.50	219258.32	397404.99					
306	276+91.41	25.00' LT	701.03	219256.13	397410.57					
307	276+74.65	42.20' LT	700.89	219278.36	397401.49					
308	276+78.14	42.20' LT	700.92	219277.05	397404.72					
309	276+74.64	38.20' LT	700.72	219274.65	397399.98					
310	276+78.28	38.20' LT	700.77	219273.29	397403.36					
311	276+74.91	33.20' LT	700.51	219269.92	397398.35					
312	276+78.91	33.20' LT	700.57	219268.42	397402.06					
313	276+80.41	28.20' LT	700.52	219263.22	397401.58					
314	276+80.41	33.20' LT	700.59	219267.85	397403.45					
315	276+85.41	28.20' LT	700.61	219261.34	397406.21					
316	276+85.41	33.20' LT	700.68	219265.98	397408.09					
317	276+94.41	25.03' LT	701.04	219255.03	397413.36					
318	276+94.41	33.20' LT	701.13	219262.60	397416.43					
319	276+72.41	33.20' LT	700.33	219270.86	397396.04					
320	276+72.41	28.20' LT	700.42	219266.22	397394.16					



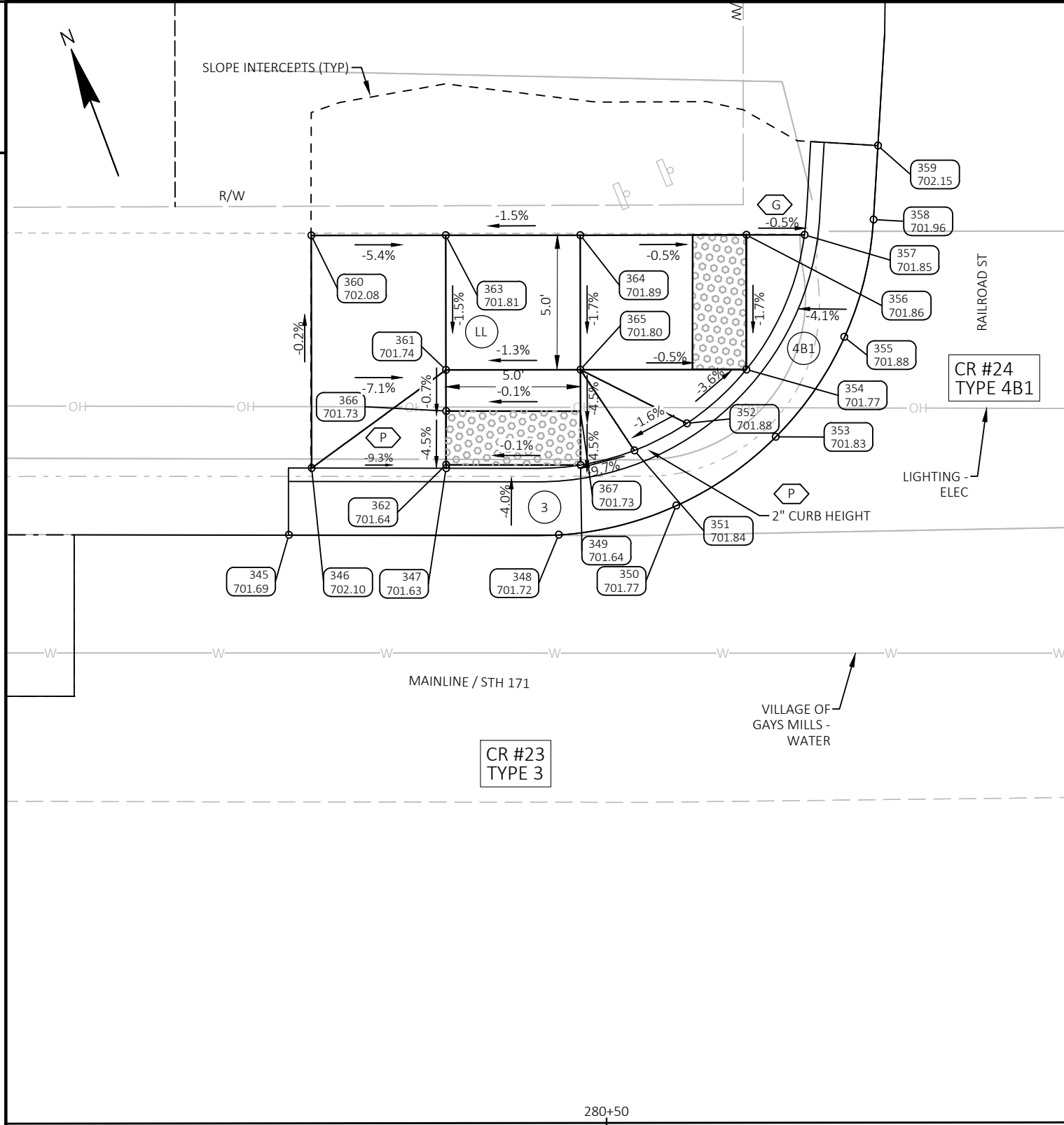
- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION



GAY ST SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
324	276+69.97	38.95' RT	700.47	219204.89	397366.71
325	276+69.94	34.41' RT	700.53	219209.11	397368.38
326	276+70.98	29.72' RT	700.60	219213.07	397371.10
327	276+73.70	25.78' RT	700.67	219215.70	397375.11
328	276+76.04	27.00' RT	700.60	219213.69	397376.82
329	276+77.32	26.10' RT	700.78	219214.05	397378.34
330	276+77.70	23.12' RT	700.71	219216.66	397379.81
331	276+78.75	25.39' RT	700.80	219214.16	397379.93
332	276+82.41	22.19' RT	700.75	219215.76	397384.52
333	276+92.31	22.17' RT	700.79	219212.07	397393.71
334	276+80.31	39.00' RT	700.93	219200.96	397376.27
335	276+72.81	32.00' RT	700.50	219210.27	397371.94
336	276+76.04	32.00' RT	700.56	219209.05	397374.94
337	276+80.31	32.00' RT	700.63	219207.45	397378.90
338	276+80.31	27.00' RT	700.66	219212.09	397380.78
339	276+85.31	32.00' RT	700.72	219205.58	397383.53
340	276+85.31	27.00' RT	700.70	219210.21	397385.41
341	276+92.00	32.00' RT	701.25	219203.07	397389.73
342	276+80.31	24.92' RT	700.65	219214.02	397381.56
343	276+85.31	24.70' RT	700.69	219212.34	397386.27



- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

RAILROAD ST NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
345	280+38.21	21.87' LT	701.69	219123.13	397730.87
346	280+39.06	24.36' LT	702.10	219125.11	397732.59
347	280+44.06	24.34' LT	701.63	219123.22	397737.22
348	280+48.25	21.87' LT	701.72	219119.36	397740.17
349	280+49.06	24.46' LT	701.64	219121.46	397741.90
350	280+52.62	22.94' LT	701.77	219118.72	397744.63
351	280+51.06	24.99' LT	701.84	219121.20	397743.96
352	280+53.02	26.00' LT	701.88	219121.40	397746.14
353	280+56.32	25.50' LT	701.83	219119.70	397749.01
354	280+55.23	28.00' LT	701.77	219122.43	397748.94
355	280+58.87	29.21' LT	701.88	219122.18	397752.77
356	280+55.24	33.00' LT	701.86	219127.06	397750.83
357	280+57.40	33.00' LT	701.85	219126.25	397752.83
358	280+59.96	33.57' LT	701.96	219125.81	397755.42
359	280+60.13	36.32' LT	702.15	219128.30	397756.61
360	280+39.06	33.00' LT	702.08	219133.13	397735.84
361	280+44.06	28.00' LT	701.74	219126.62	397738.60
362	280+44.06	24.47' LT	701.64	219123.34	397737.27
363	280+44.06	33.00' LT	701.81	219131.25	397740.47
364	280+49.06	33.00' LT	701.89	219129.37	397745.11
365	280+49.06	28.00' LT	701.80	219124.74	397743.23
366	280+44.07	26.47' LT	701.73	219125.20	397738.03
367	280+49.06	26.46' LT	701.73	219123.31	397742.65

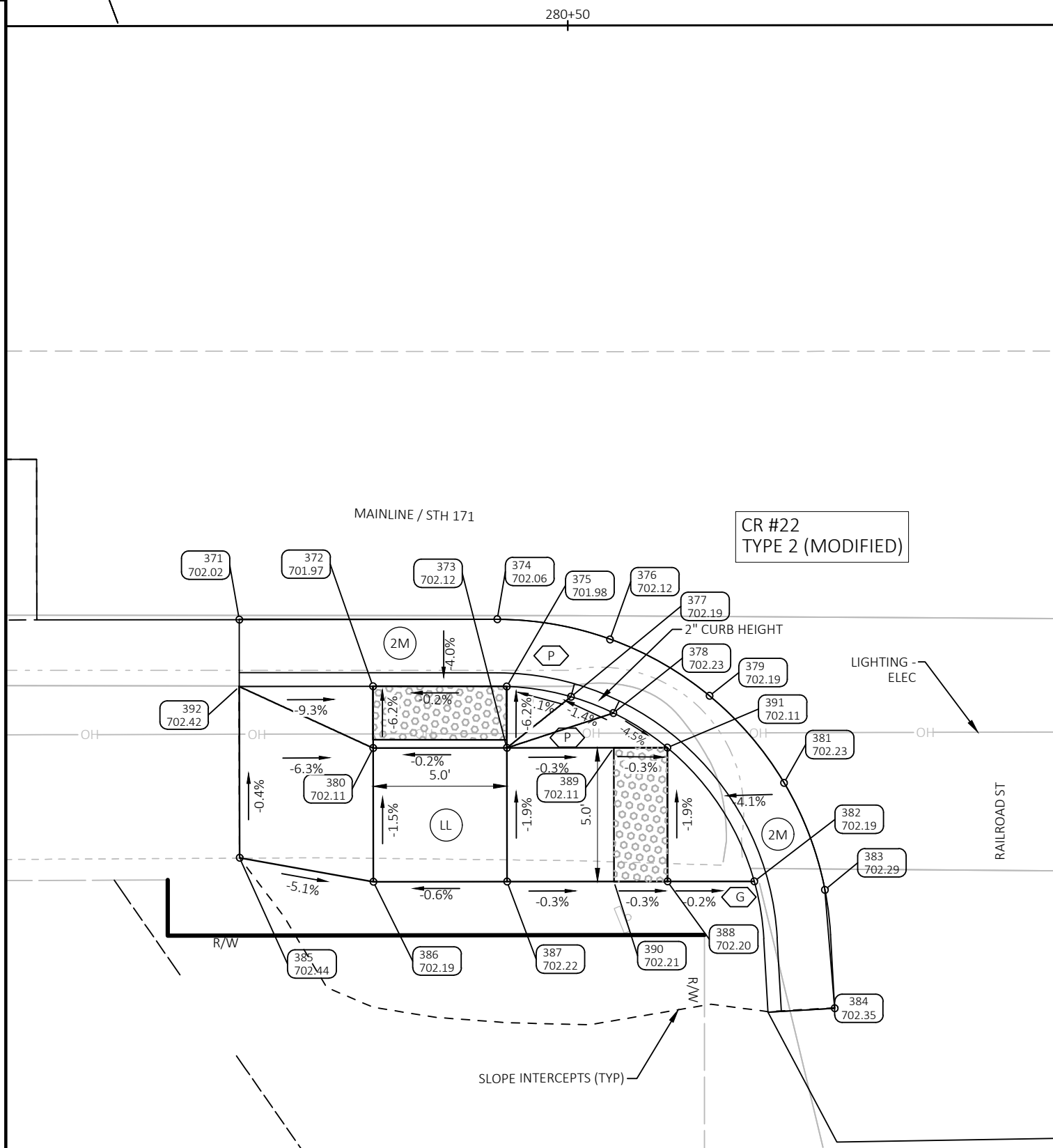


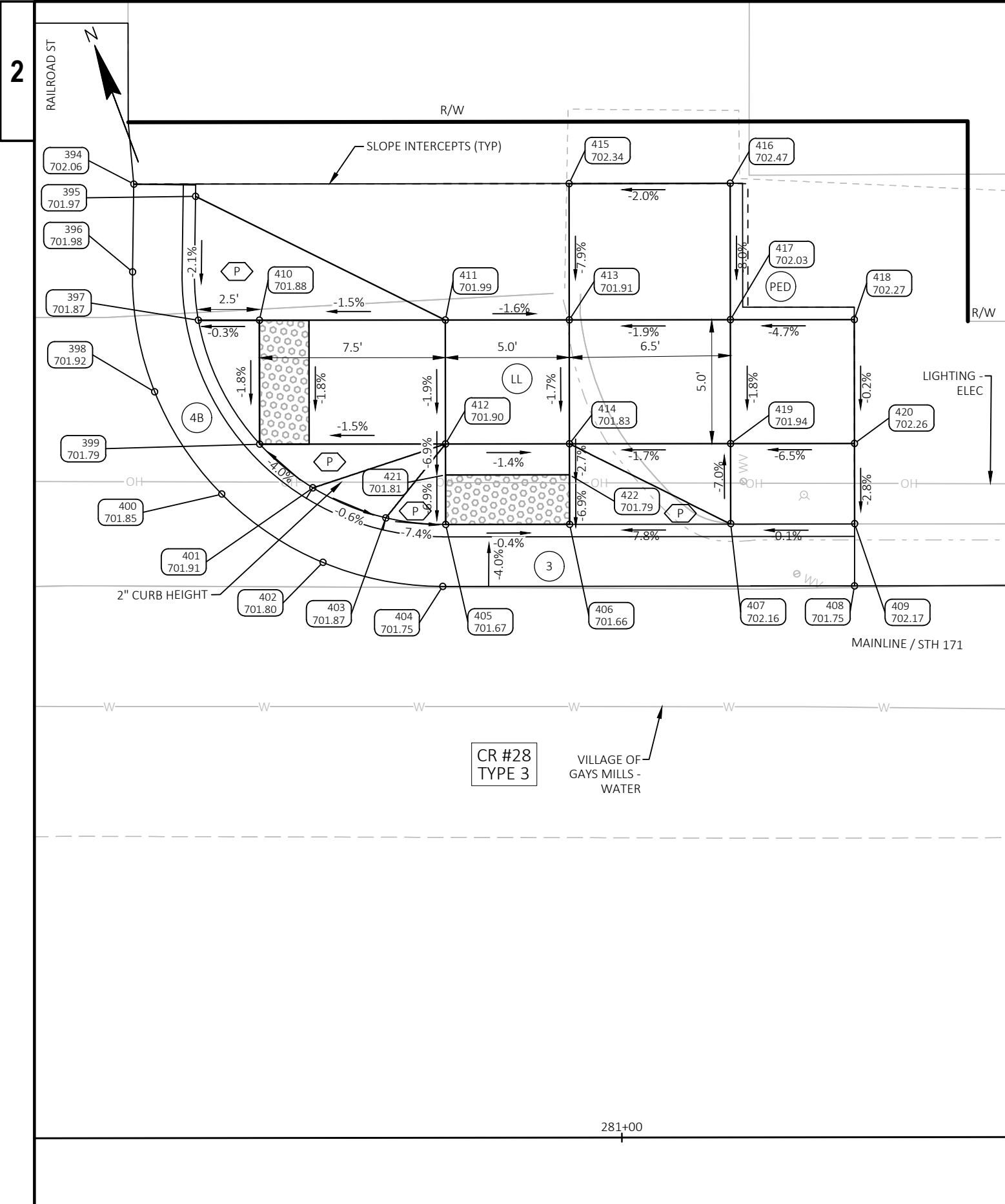
- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

RAILROAD ST SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
371	280+37.69	22.20' RT	702.02	219082.48	397713.86
372	280+42.69	24.70' RT	701.97	219078.28	397717.55
373	280+47.69	27.00' RT	702.12	219074.27	397721.32
374	280+47.33	22.20' RT	702.06	219078.86	397722.80
375	280+47.69	24.71' RT	701.98	219076.40	397722.18
376	280+51.55	22.95' RT	702.12	219076.58	397726.42
377	280+50.09	25.09' RT	702.19	219075.14	397724.27
378	280+51.67	25.70' RT	702.23	219073.98	397725.51
379	280+55.27	25.06' RT	702.19	219073.22	397729.08
380	280+42.69	27.00' RT	702.11	219076.15	397716.69
381	280+58.05	28.32' RT	702.23	219069.16	397730.43
382	280+56.93	32.00' RT	702.19	219066.17	397728.02
383	280+59.57	32.32' RT	702.29	219064.88	397730.35
384	280+59.94	36.75' RT	702.35	219060.64	397729.02
385	280+37.69	31.10' RT	702.44	219074.23	397710.52
386	280+42.69	32.00' RT	702.19	219071.51	397714.81
387	280+47.69	32.00' RT	702.22	219069.64	397719.45
388	280+53.69	32.00' RT	702.20	219067.39	397725.01
389	280+51.69	27.00' RT	702.11	219072.77	397725.03
390	280+51.69	32.00' RT	702.21	219068.14	397723.16
391	280+53.69	27.00' RT	702.11	219072.02	397726.89
392	280+37.69	24.70' RT	702.42	219080.15	397712.92





- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

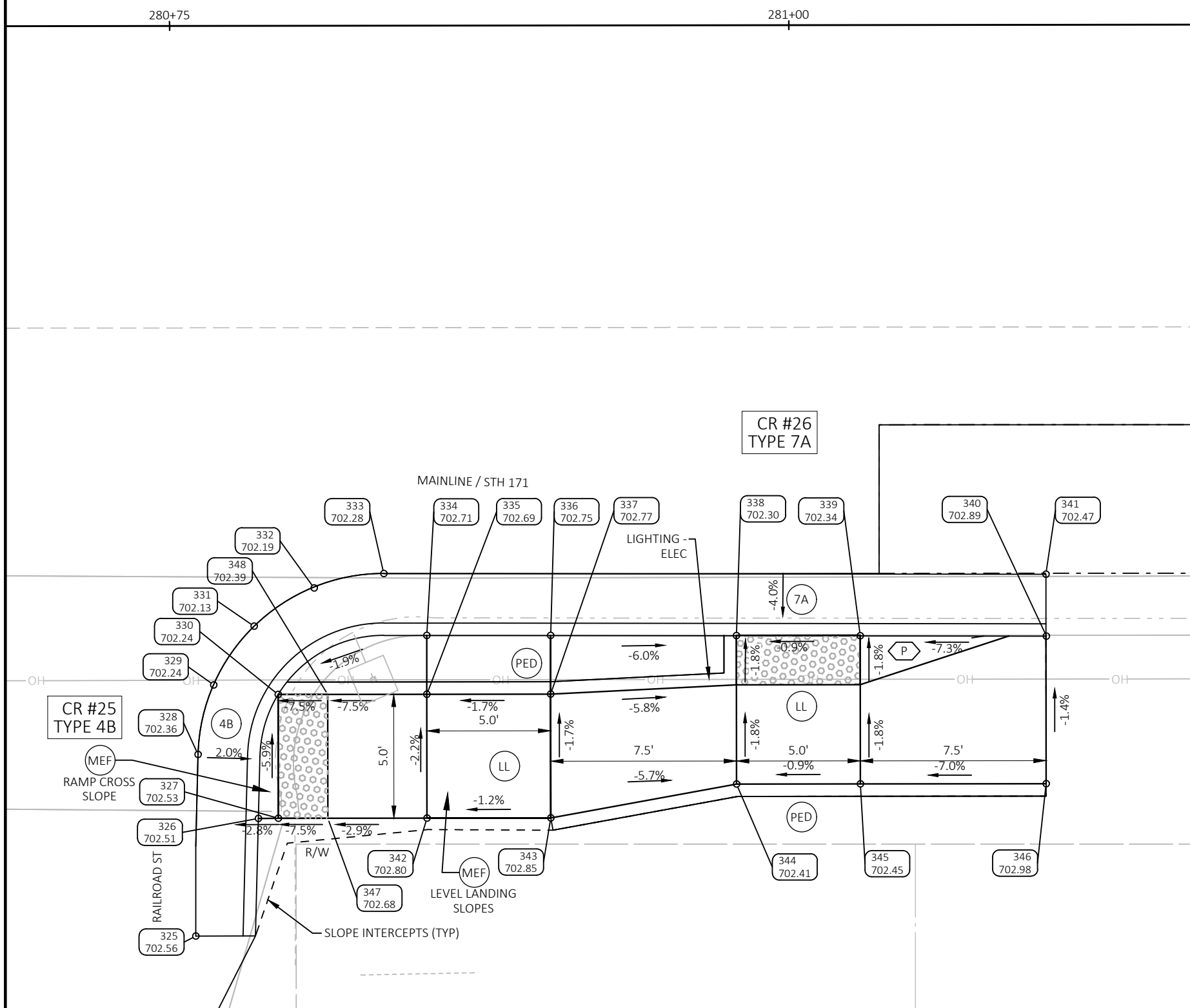
RAILROAD ST NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
394	280+80.36	38.50' LT	702.06	219122.73	397776.18
395	280+82.85	38.00' LT	701.97	219121.34	397778.30
396	280+80.30	34.96' LT	701.98	219119.47	397774.79
397	280+82.95	33.00' LT	701.87	219116.66	397776.52
398	280+81.19	30.12' LT	701.92	219114.65	397773.80
399	280+85.42	28.00' LT	701.79	219111.10	397776.93
400	280+83.88	25.99' LT	701.85	219109.81	397774.75
401	280+87.56	26.23' LT	701.91	219108.66	397778.25
402	280+87.97	23.22' LT	701.80	219105.72	397777.50
403	280+90.50	25.02' LT	701.87	219106.43	397780.53
404	280+92.80	22.25' LT	701.75	219103.00	397781.61
405	280+92.92	24.75' LT	701.67	219105.28	397782.66
406	280+97.92	24.75' LT	701.66	219103.40	397787.30
407	281+04.42	24.77' LT	702.16	219100.98	397793.33
408	281+09.41	22.25' LT	701.75	219096.77	397797.01
409	281+09.42	24.77' LT	702.17	219099.10	397797.96
410	280+85.42	33.00' LT	701.88	219115.74	397778.81
411	280+92.92	33.00' LT	701.99	219112.92	397785.76
412	280+92.92	28.00' LT	701.90	219108.29	397783.88
413	280+97.92	33.00' LT	701.91	219111.05	397790.39
414	280+97.92	28.00' LT	701.83	219106.41	397788.52
415	280+97.92	38.50' LT	702.34	219116.15	397792.46
416	281+04.42	38.50' LT	702.47	219113.71	397798.48
417	281+04.42	33.00' LT	702.03	219108.61	397796.42
418	281+09.42	33.00' LT	702.27	219106.73	397801.05
419	281+04.42	28.00' LT	701.94	219103.97	397794.54
420	281+09.42	28.00' LT	702.26	219102.10	397799.18
421	280+92.92	26.75' LT	701.81	219107.13	397783.41
422	280+97.92	26.75' LT	701.79	219105.25	397788.05



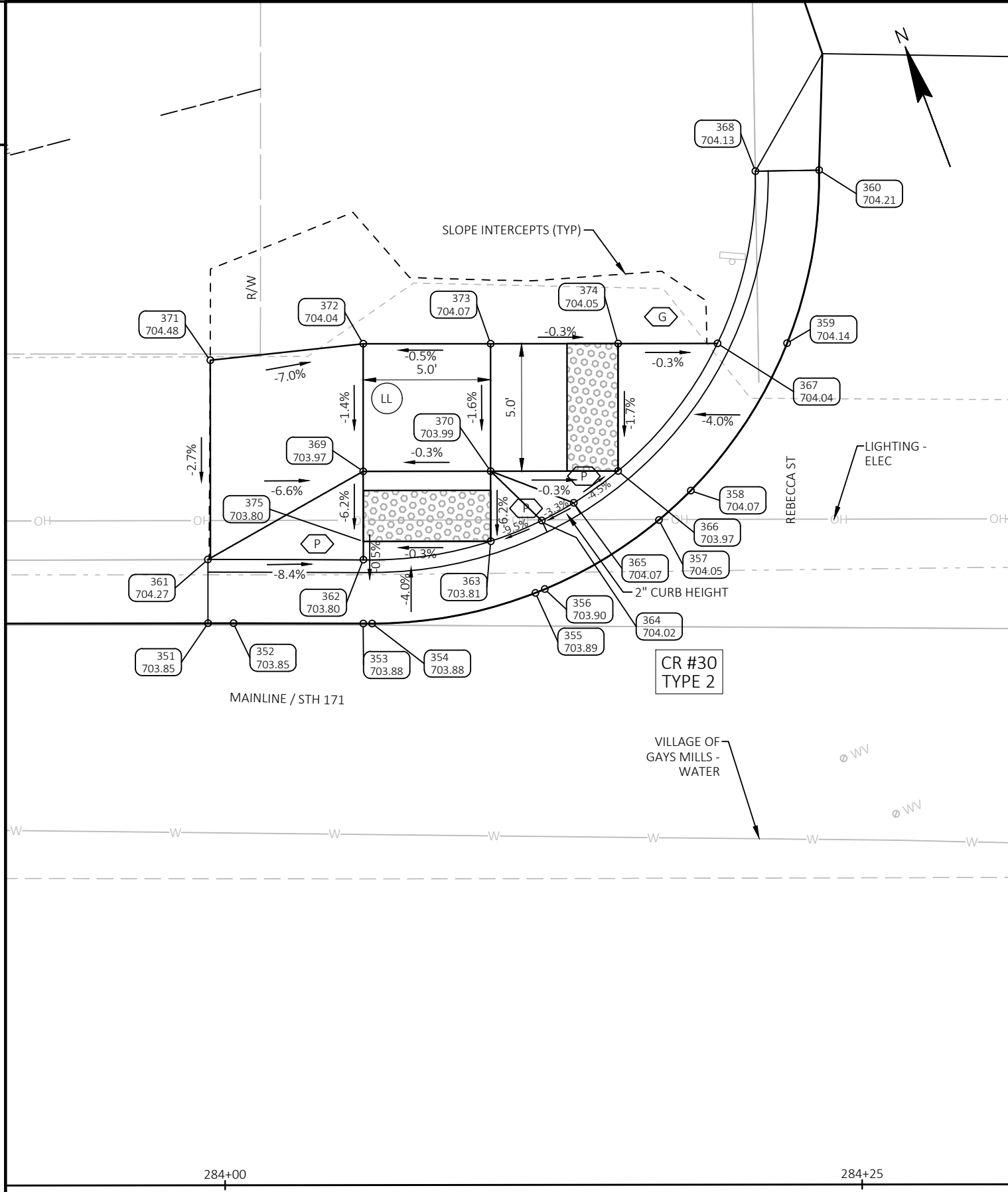
- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION



RAILROAD ST SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
325	280+76.01	36.75' RT	702.56	219054.61	397743.92
326	280+78.55	32.00' RT	702.51	219058.06	397748.05
327	280+79.35	32.00' RT	702.53	219057.76	397748.80
328	280+76.12	29.42' RT	702.36	219061.36	397746.77
329	280+76.75	26.61' RT	702.24	219063.73	397748.41
330	280+79.35	27.00' RT	702.24	219062.39	397750.68
331	280+78.38	24.24' RT	702.13	219065.31	397750.81
332	280+80.81	22.70' RT	702.19	219065.83	397753.64
333	280+83.62	22.12' RT	702.28	219065.32	397756.46
334	280+85.36	24.62' RT	702.71	219062.35	397757.13
335	280+85.35	27.00' RT	702.69	219060.14	397756.24
336	280+90.35	24.63' RT	702.75	219060.46	397761.76
337	280+90.35	27.00' RT	702.77	219058.27	397760.87
338	280+97.85	24.65' RT	702.30	219057.63	397768.71
339	281+02.85	24.66' RT	702.34	219055.74	397773.34
340	281+10.37	24.68' RT	702.89	219052.91	397780.30
341	281+10.35	22.18' RT	702.47	219055.23	397781.22
342	280+85.35	32.00' RT	702.80	219055.51	397754.36
343	280+90.35	32.00' RT	702.85	219053.63	397759.00
344	280+97.85	30.64' RT	702.41	219052.08	397766.46
345	281+02.85	30.64' RT	702.45	219050.20	397771.10
346	281+10.35	30.64' RT	702.98	219047.39	397778.05
347	280+81.35	32.00' RT	702.68	219057.01	397750.66
348	280+81.35	27.00' RT	702.39	219061.64	397752.53



- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

REBECCA ST NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
351	283+99.35	22.05' LT	703.85	218987.81	398065.71
352	284+00.35	22.05' LT	703.85	218987.44	398066.63
353	284+05.44	22.03' LT	703.88	218985.51	398071.34
354	284+05.79	22.03' LT	703.88	218985.38	398071.67
355	284+12.21	23.22' LT	703.89	218984.08	398078.06
356	284+12.57	23.37' LT	703.90	218984.08	398078.45
357	284+17.05	26.08' LT	704.05	218984.91	398083.62
358	284+18.30	27.23' LT	704.07	218985.51	398085.21
359	284+22.09	33.00' LT	704.14	218989.44	398090.89
360	284+23.35	39.79' LT	704.21	218995.26	398094.61
361	283+99.35	24.55' LT	704.27	218990.13	398066.64
362	284+05.45	24.53' LT	703.80	218987.83	398072.29
363	284+10.45	25.25' LT	703.81	218986.62	398077.20
364	284+12.45	26.06' LT	704.02	218986.62	398079.35
365	284+13.71	26.75' LT	704.07	218986.79	398080.78
366	284+15.45	28.00' LT	703.97	218987.29	398082.86
367	284+19.36	33.00' LT	704.04	218990.46	398088.36
368	284+20.85	39.76' LT	704.13	218996.16	398092.28
369	284+05.45	28.00' LT	703.97	218991.04	398073.59
370	284+10.45	28.00' LT	703.99	218989.17	398078.23
371	283+99.45	32.36' LT	704.48	218997.34	398069.67
372	284+05.45	33.00' LT	704.04	218995.68	398075.47
373	284+10.45	33.00' LT	704.07	218993.80	398080.10
374	284+15.45	33.00' LT	704.05	218991.93	398084.74
375	284+05.45	25.25' LT	703.80	218988.49	398072.56



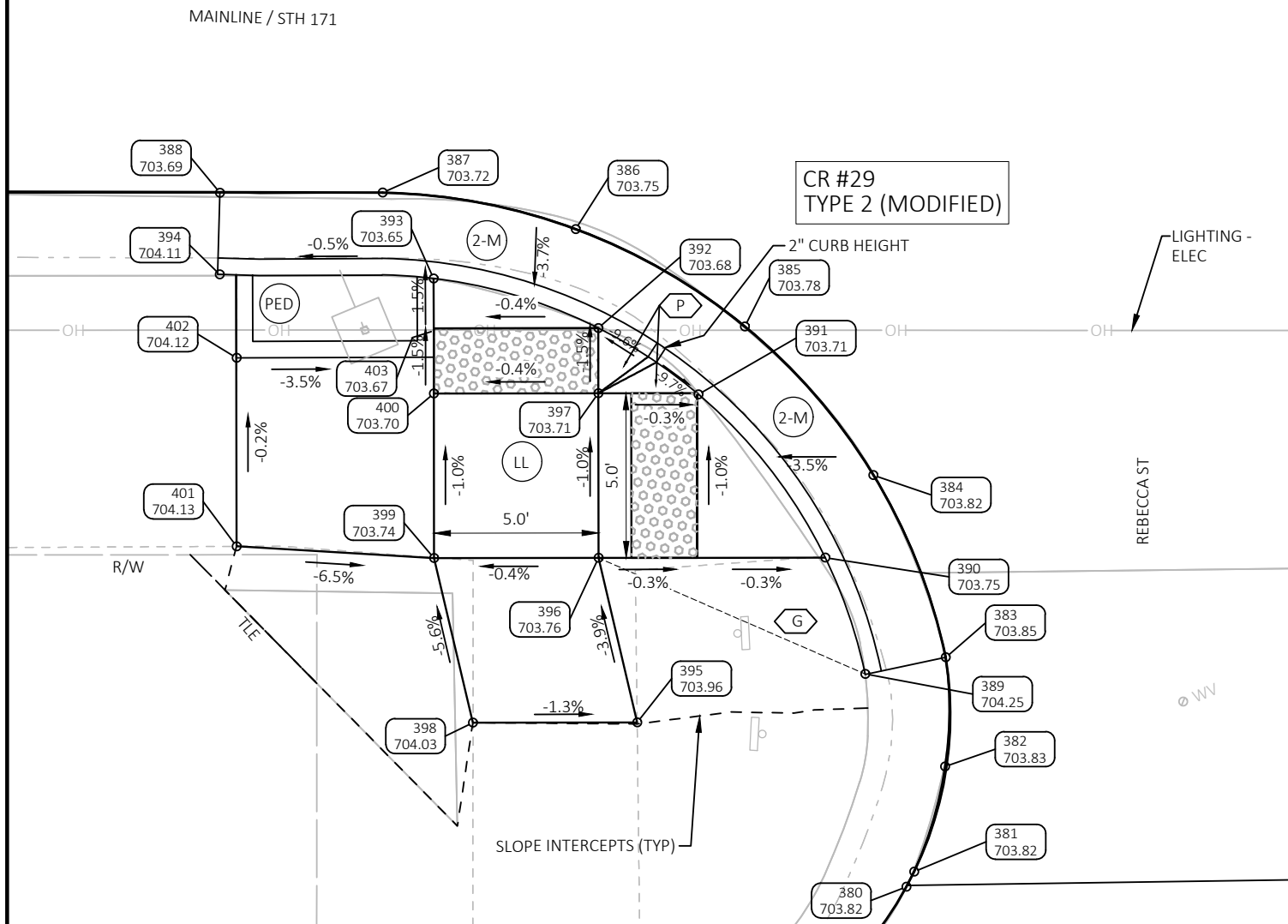
284+00

284+25

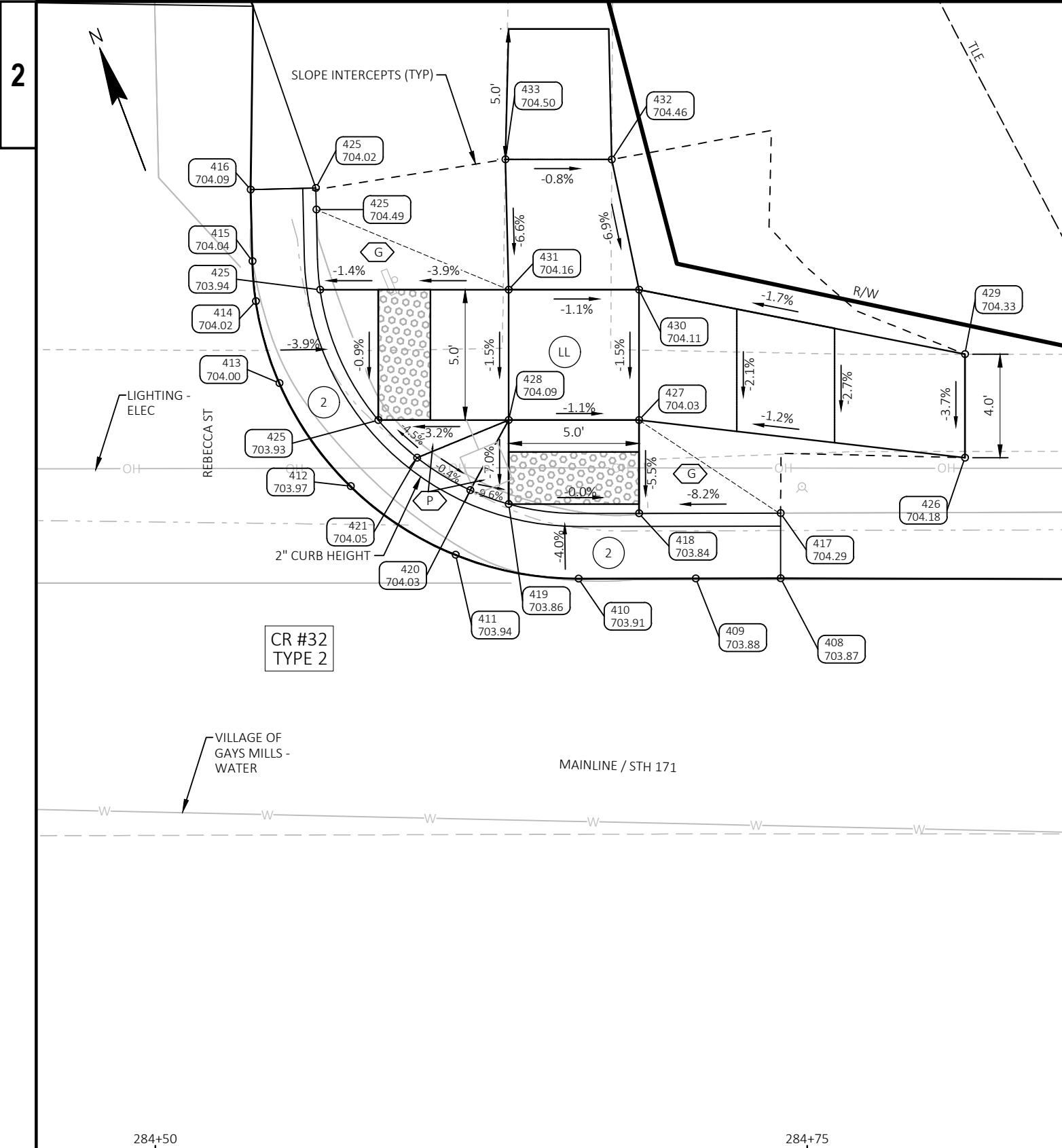
- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION



REBECCA ST SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
380	284+19.88	43.50' RT	703.82	218919.35	398060.15
381	284+20.12	43.04' RT	703.82	218919.69	398060.54
382	284+21.06	39.85' RT	703.83	218922.29	398062.60
383	284+21.09	36.54' RT	703.85	218925.36	398063.87
384	284+18.89	31.00' RT	703.82	218931.32	398063.92
385	284+14.99	26.48' RT	703.78	218936.97	398062.00
386	284+09.85	23.52' RT	703.75	218941.64	398058.35
387	284+03.99	22.40' RT	703.72	218944.88	398053.33
388	283+99.05	22.39' RT	703.69	218946.73	398048.75
389	284+18.64	37.04' RT	704.25	218925.81	398061.42
390	284+17.43	33.50' RT	703.75	218929.54	398061.62
391	284+13.58	28.53' RT	703.71	218935.59	398059.92
392	284+10.54	26.52' RT	703.68	218938.60	398057.85
393	284+05.54	25.01' RT	703.65	218941.88	398053.79
394	283+99.04	24.87' RT	704.11	218944.44	398047.81
395	284+11.71	38.50' RT	703.96	218927.06	398054.44
396	284+10.54	33.50' RT	703.76	218932.13	398055.24
397	284+10.54	28.50' RT	703.71	218936.76	398057.11
398	284+06.71	38.50' RT	704.03	218928.93	398049.81
399	284+05.54	33.50' RT	703.74	218934.00	398050.60
400	284+05.54	28.50' RT	703.70	218938.64	398052.48
401	283+99.54	33.13' RT	704.13	218936.59	398045.18
402	283+99.55	27.41' RT	704.12	218941.89	398047.33
403	284+05.54	26.52' RT	703.67	218940.47	398053.22



- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

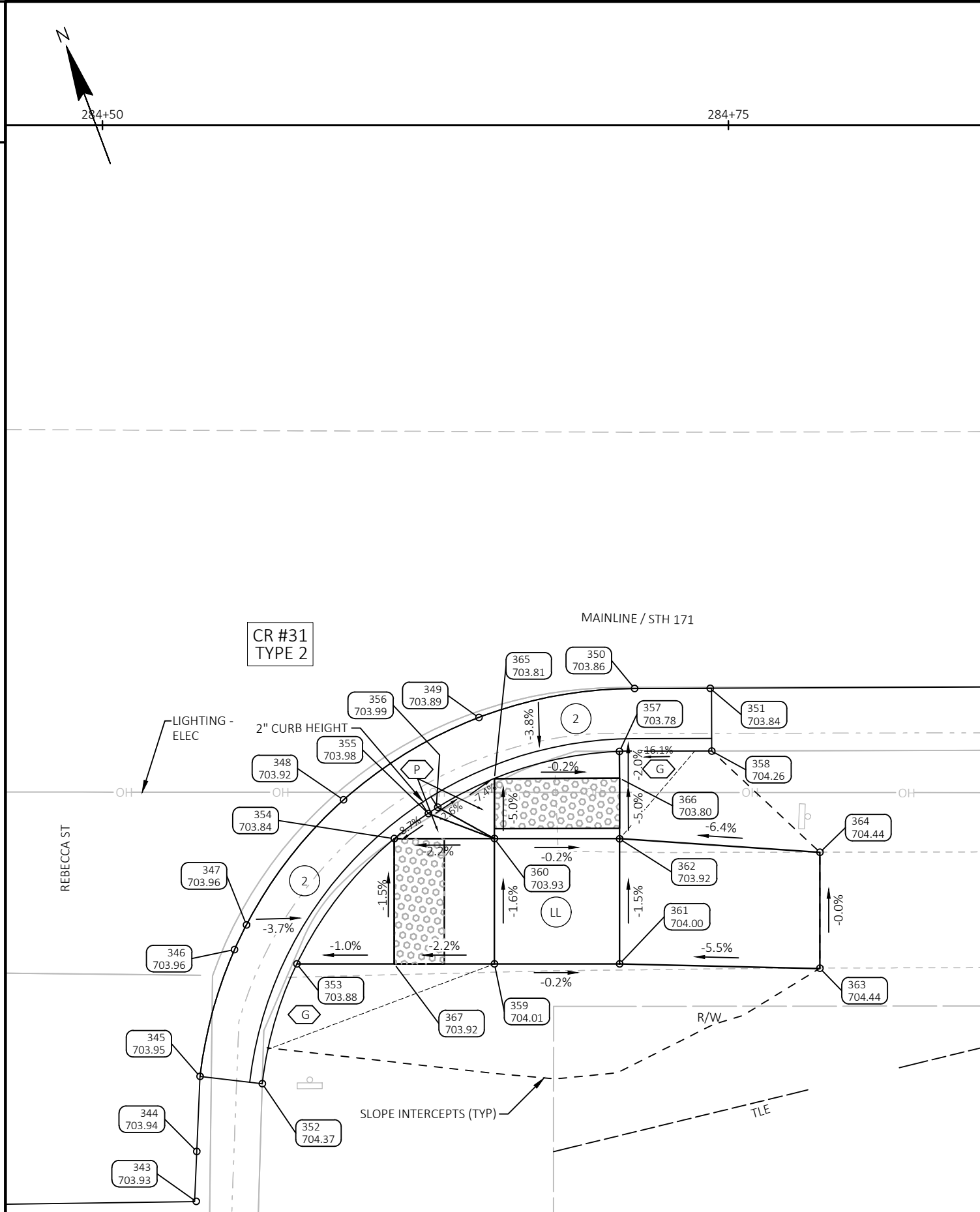
REBECCA ST NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
408	284+73.98	21.93' LT	703.87	218959.66	398134.85
409	284+70.72	21.93' LT	703.88	218960.88	398131.83
410	284+66.23	21.92' LT	703.91	218962.57	398127.66
411	284+61.51	22.84' LT	703.94	218965.19	398123.64
412	284+57.49	25.47' LT	703.97	218969.14	398120.90
413	284+54.76	29.42' LT	704.00	218973.83	398119.86
414	284+53.85	32.56' LT	704.02	218977.09	398120.20
415	284+53.72	34.10' LT	704.04	218978.56	398120.65
416	284+53.65	36.84' LT	704.09	218981.13	398121.62
417	284+73.98	24.43' LT	704.29	218961.98	398135.79
418	284+68.55	24.42' LT	703.84	218964.02	398130.75
419	284+63.55	24.78' LT	703.86	218966.23	398126.26
420	284+62.08	25.32' LT	704.03	218967.28	398125.09
421	284+60.04	26.56' LT	704.05	218969.20	398123.67
425	284+58.55	28.00' LT	703.93	218971.09	398122.83
425	284+56.32	33.00' LT	703.94	218976.57	398122.65
425	284+56.16	36.90' LT	704.02	218980.24	398123.97
425	284+56.17	36.08' LT	704.49	218979.47	398123.67
426	284+81.05	26.56' LT	704.18	218961.29	398143.14
427	284+68.55	28.00' LT	704.03	218967.33	398132.10
428	284+63.55	28.00' LT	704.09	218969.21	398127.47
429	284+81.05	30.53' LT	704.33	218964.97	398144.63
430	284+68.55	33.00' LT	704.11	218971.96	398133.98
431	284+63.55	33.00' LT	704.16	218973.84	398129.35
432	284+67.50	37.99' LT	704.46	218976.98	398134.89
433	284+63.42	38.00' LT	704.50	218978.52	398131.11



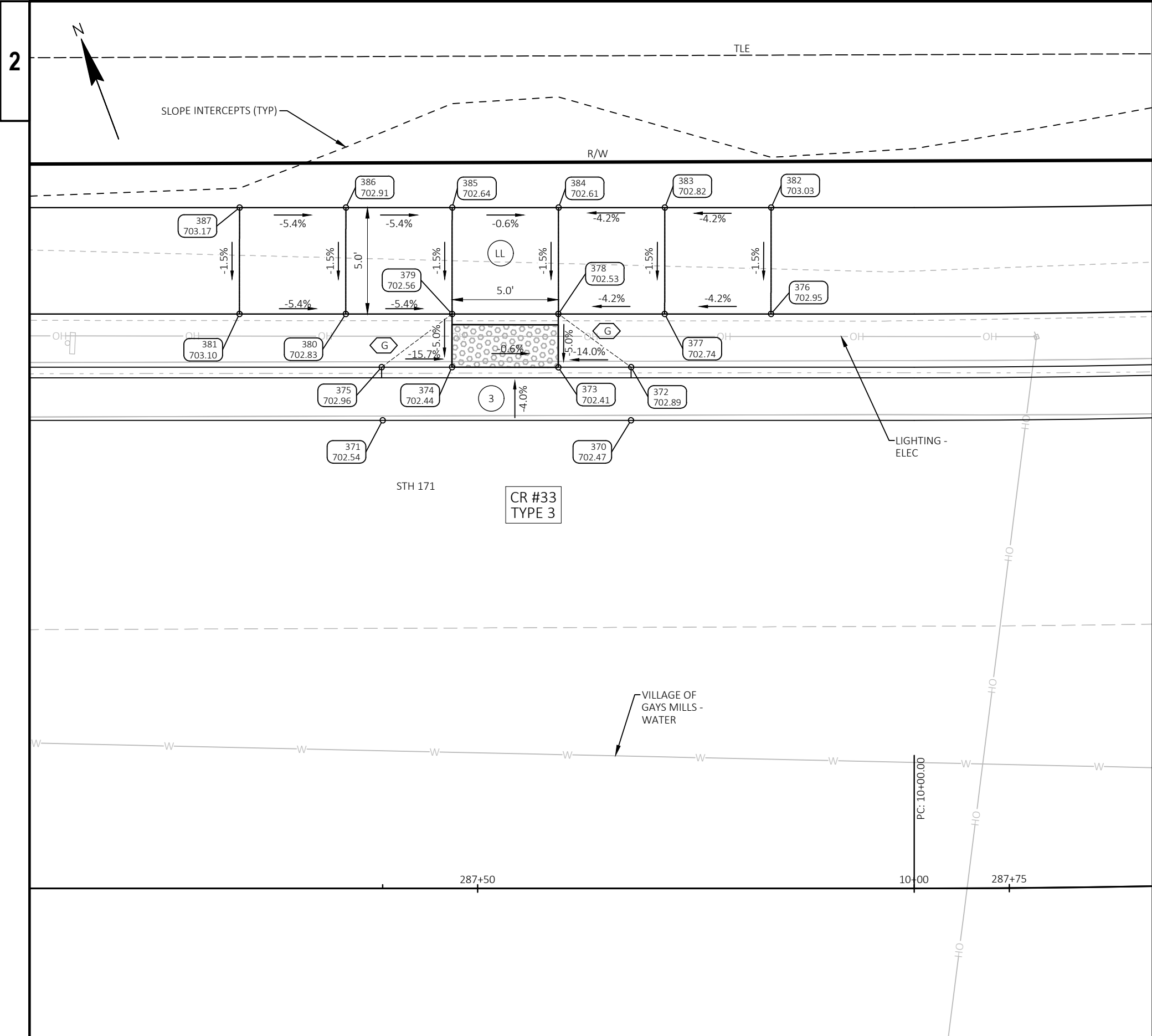
- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION



REBECCA ST SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
343	284+53.74	42.99' RT	703.93	218907.13	398091.67
344	284+53.77	40.99' RT	703.94	218908.97	398092.44
345	284+53.90	38.00' RT	703.95	218911.70	398093.69
346	284+55.27	32.93' RT	703.96	218915.87	398096.88
347	284+55.75	31.95' RT	703.96	218916.60	398097.69
348	284+59.63	26.95' RT	703.92	218919.78	398103.16
349	284+65.03	23.66' RT	703.89	218920.79	398109.40
350	284+71.25	22.50' RT	703.86	218919.52	398115.60
351	284+74.27	22.50' RT	703.84	218918.39	398118.40
352	284+56.38	38.28' RT	704.37	218910.50	398095.89
353	284+57.77	33.50' RT	703.88	218914.41	398098.97
354	284+61.66	28.50' RT	703.84	218917.58	398104.45
355	284+63.01	27.50' RT	703.98	218918.00	398106.09
356	284+63.39	27.25' RT	703.99	218918.08	398106.53
357	284+70.65	25.02' RT	703.78	218917.42	398114.10
358	284+74.33	25.00' RT	704.26	218916.05	398117.51
359	284+65.66	33.50' RT	704.01	218911.44	398106.28
360	284+65.66	28.50' RT	703.93	218916.07	398108.16
361	284+70.66	33.50' RT	704.00	218909.56	398110.91
362	284+70.66	28.51' RT	703.92	218914.18	398112.79
363	284+78.66	33.68' RT	704.44	218906.38	398118.26
364	284+78.66	29.05' RT	704.44	218910.67	398120.00
365	284+65.66	26.10' RT	703.81	218918.29	398109.06
366	284+70.65	26.09' RT	703.80	218916.43	398113.70
367	284+61.66	33.50' RT	703.92	218912.94	398102.57

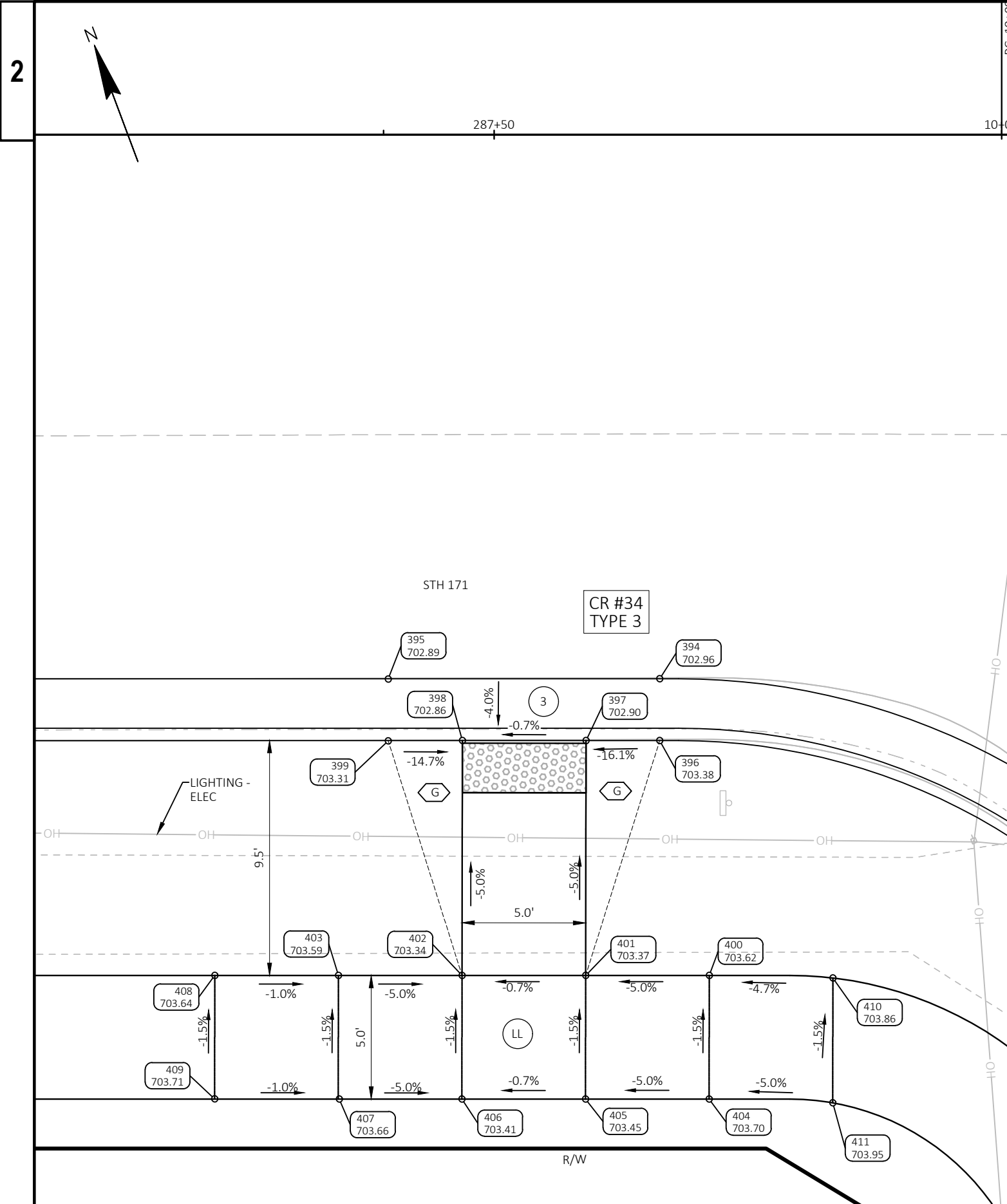


- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

STH 171 STA 287+50 N					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
370	9+86.68	22.00' LT	702.47	218853.16	398397.29
371	9+75.00	22.00' LT	702.54	218857.56	398386.47
372	9+86.68	24.50' LT	702.89	218855.48	398398.23
373	9+83.26	24.49' LT	702.41	218856.76	398395.06
374	9+78.26	24.50' LT	702.44	218858.65	398390.43
375	9+74.96	24.50' LT	702.96	218859.89	398387.37
376	9+93.26	27.00' LT	702.95	218855.32	398405.27
377	9+88.26	27.00' LT	702.74	218857.20	398400.64
378	9+83.26	27.00' LT	702.53	218859.08	398396.01
379	9+78.26	27.00' LT	702.56	218860.96	398391.37
380	9+73.26	27.00' LT	702.83	218862.84	398386.74
381	9+68.26	27.00' LT	703.10	218864.72	398382.11
382	9+93.27	32.00' LT	703.03	218859.95	398407.16
383	9+88.27	32.00' LT	702.82	218861.83	398402.53
384	9+83.27	32.00' LT	702.61	218863.71	398397.89
385	9+78.27	32.00' LT	702.64	218865.59	398393.26
386	9+73.27	32.00' LT	702.91	218867.47	398388.63
387	9+68.27	32.00' LT	703.17	218869.35	398384.00



- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

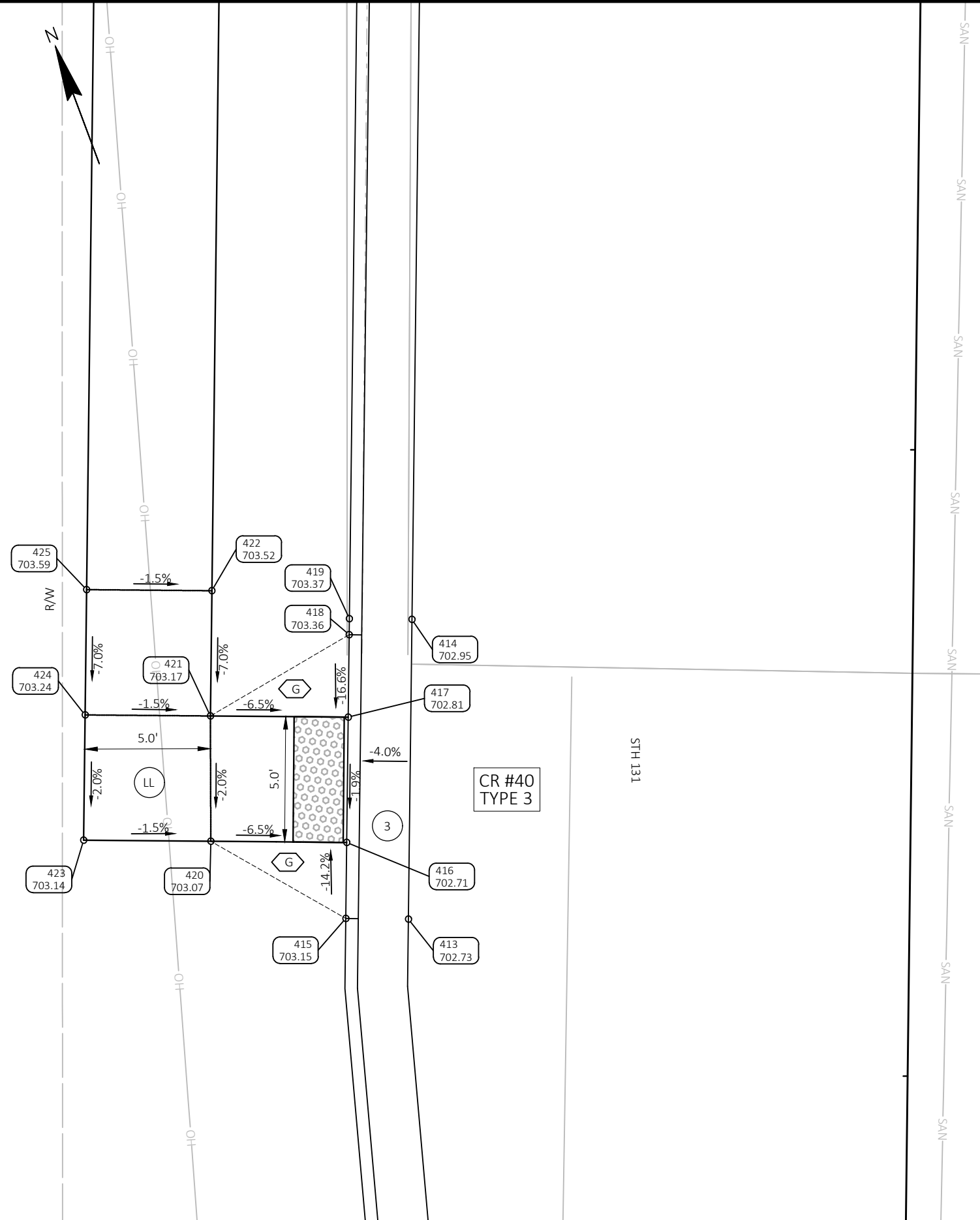
(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

STH 171 STA 287+50 S					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
394	9+86.17	22.00' RT	702.96	218812.59	398380.26
395	9+75.18	22.01' RT	702.89	218816.71	398370.08
396	9+86.17	24.50' RT	703.38	218810.27	398379.32
397	9+83.19	24.50' RT	702.90	218811.39	398376.56
398	9+78.19	24.50' RT	702.86	218813.27	398371.93
399	9+75.19	24.51' RT	703.31	218814.39	398369.14
400	9+88.17	34.00' RT	703.62	218800.71	398377.61
401	9+83.17	34.00' RT	703.37	218802.60	398372.97
402	9+78.17	34.00' RT	703.34	218804.48	398368.34
403	9+73.17	34.00' RT	703.59	218806.36	398363.71
404	9+88.17	39.00' RT	703.70	218796.08	398375.72
405	9+83.17	39.00' RT	703.45	218797.97	398371.08
406	9+78.17	39.00' RT	703.41	218799.85	398366.45
407	9+73.21	39.00' RT	703.66	218801.71	398361.86
408	9+68.17	34.00' RT	703.64	218808.24	398359.08
409	9+68.17	38.99' RT	703.71	218803.62	398357.19
410	9+93.17	34.10' RT	703.86	218798.74	398382.20
411	9+93.17	39.15' RT	703.95	218794.06	398380.29

- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

- (X) CURB RAMP TYPE
- (PED) CONCRETE PEDESTRIAN CURB
- (LL) LEVEL LANDING
- (G) GRADED FLARE
- (P) PAVED FLARE
- (ID ELEV) POINT ID/ELEVATION



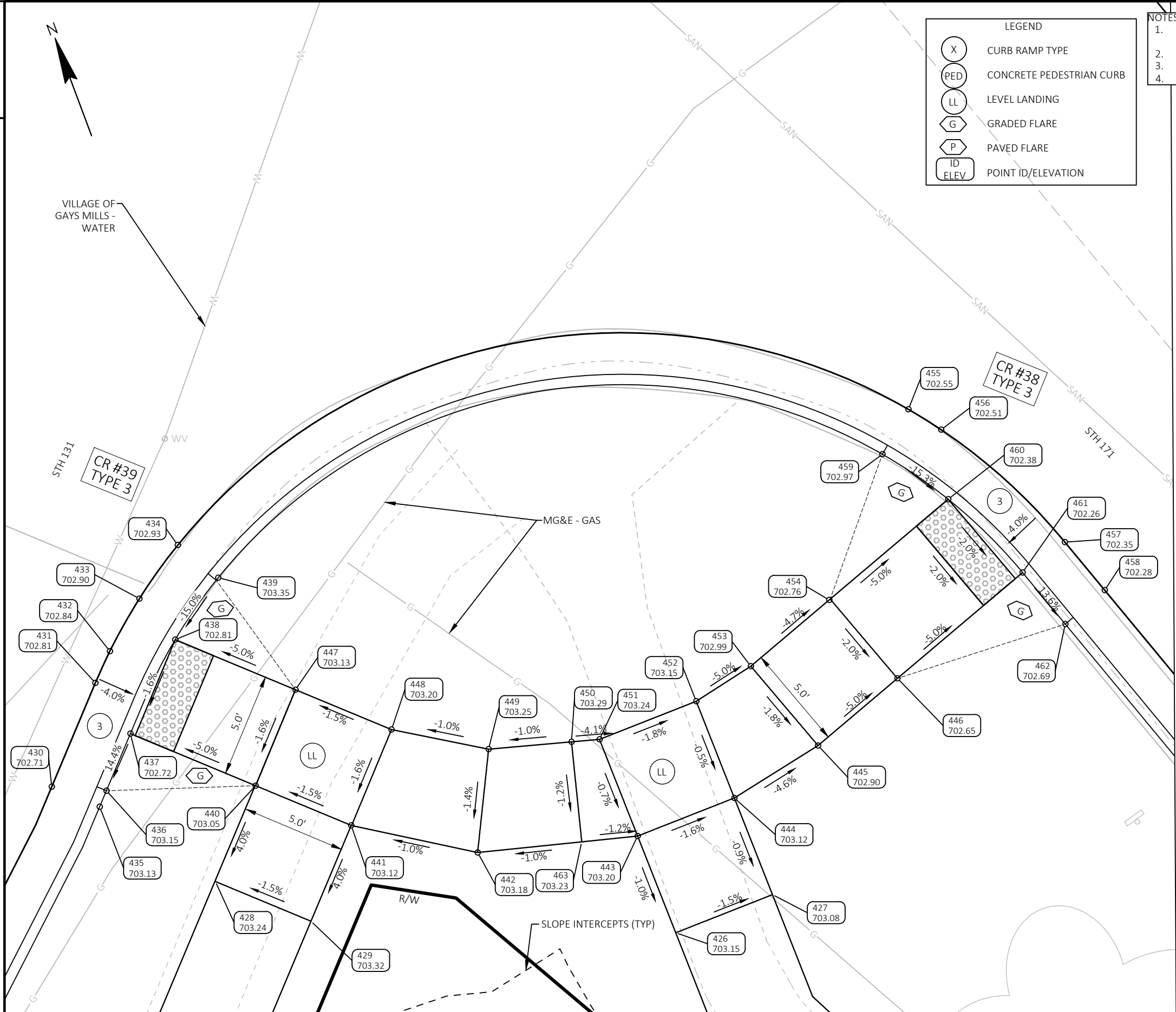
STH 131 - MAIN ST SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
413	392+31.03	20.00' LT	702.73	218726.33	398375.03
414	392+42.99	20.00' LT	702.95	218737.36	398379.67
415	392+31.03	22.50' LT	703.15	218727.30	398372.73
416	392+34.06	22.50' LT	702.71	218730.10	398373.90
417	392+39.06	22.50' LT	702.81	218734.71	398375.84
418	392+42.35	22.50' LT	703.36	218737.74	398377.11
419	392+42.99	22.50' LT	703.37	218738.33	398377.36
420	392+34.04	27.92' LT	703.07	218732.18	398368.90
421	392+39.04	28.00' LT	703.17	218736.82	398370.76
422	392+44.04	28.00' LT	703.52	218741.43	398372.70
423	392+34.02	33.00' LT	703.14	218734.13	398364.21
424	392+39.02	33.00' LT	703.24	218738.74	398366.15
425	392+44.02	33.00' LT	703.59	218743.35	398368.08



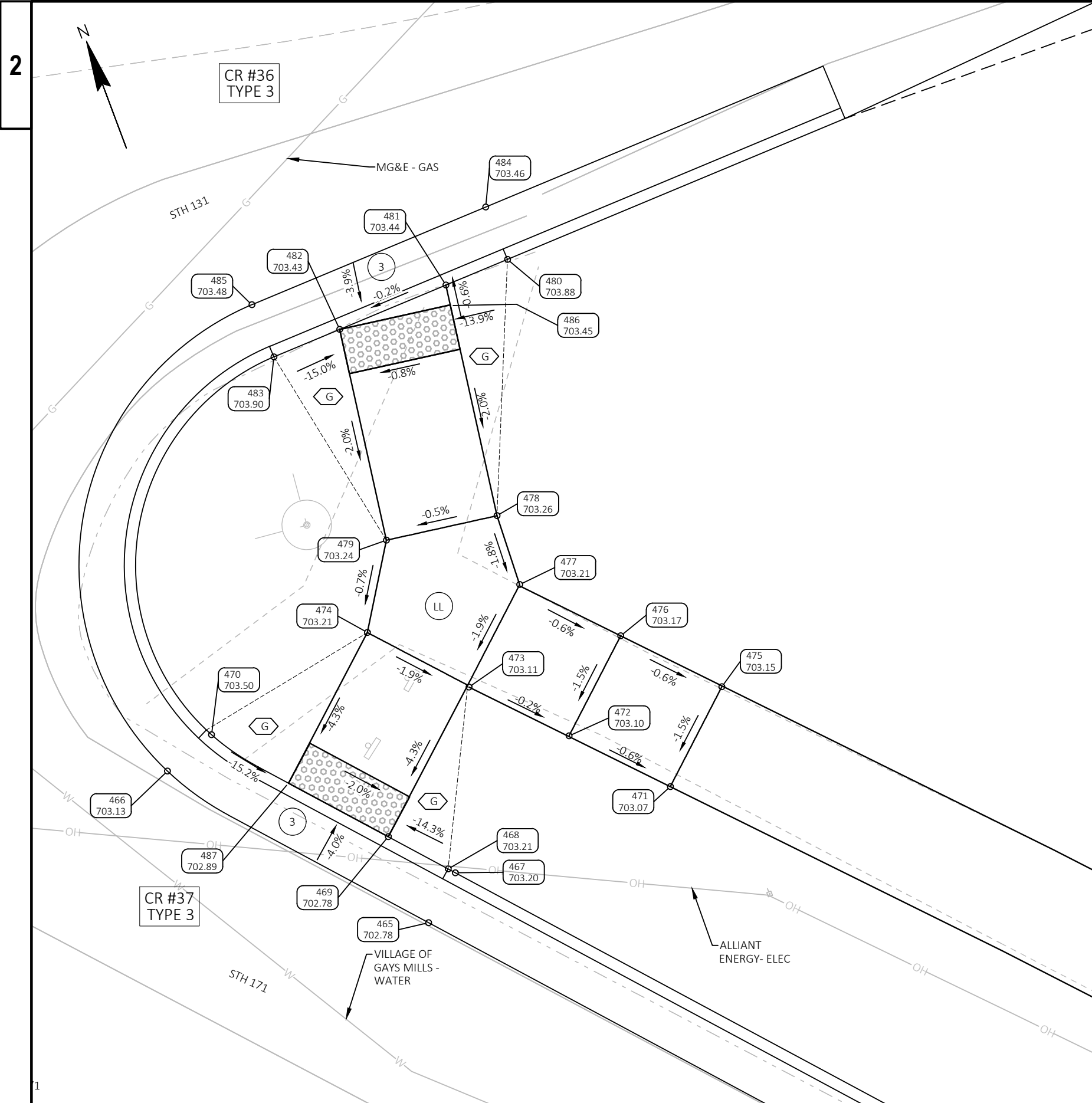
LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L



STH 171 - MAIN ST S					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
426	392+35.55	50.39' RT	703.15	218703.24	398441.68
427	392+39.04	53.97' RT	703.08	218705.07	398446.33
428	392+29.25	29.02' RT	703.24	218705.70	398419.54
429	392+29.27	34.00' RT	703.32	218703.79	398424.14
430	392+30.41	20.00' RT	702.71	218710.26	398411.67
431	392+35.82	20.00' RT	702.81	218715.26	398413.77
432	392+37.50	20.05' RT	702.84	218716.78	398414.47
433	392+40.38	20.38' RT	702.90	218719.31	398415.88
434	392+43.47	21.09' RT	702.93	218721.89	398417.73
435	392+30.41	22.50' RT	703.13	218709.30	398413.97
436	392+31.24	22.50' RT	703.15	218710.07	398414.30
437	392+34.22	22.50' RT	702.72	218712.82	398415.45
438	392+39.23	22.73' RT	702.81	218717.34	398417.61
439	392+42.78	23.49' RT	703.35	218720.32	398419.68
440	392+34.25	29.02' RT	703.05	218710.31	398421.47
441	392+34.27	34.00' RT	703.12	218708.40	398426.07
442	392+35.42	40.11' RT	703.18	218707.10	398432.15
443	392+39.13	46.90' RT	703.20	218707.89	398439.85
444	392+42.62	50.48' RT	703.12	218709.72	398444.50
445	392+46.51	53.21' RT	702.90	218712.24	398448.52
446	392+50.96	55.48' RT	702.65	218715.47	398452.34
447	392+39.25	29.00' RT	703.13	218714.93	398423.39
448	392+39.27	34.00' RT	703.20	218713.01	398428.01
449	392+40.21	38.67' RT	703.25	218712.07	398432.68
450	392+42.08	42.21' RT	703.29	218712.42	398436.67
451	392+42.71	43.40' RT	703.24	218712.54	398438.01
452	392+46.20	46.98' RT	703.15	218714.37	398442.66
453	392+48.77	48.75' RT	702.99	218716.06	398445.29
454	392+53.17	50.99' RT	702.76	218719.25	398449.06
455	392+63.10	50.95' RT	702.55	218728.42	398452.87
456	392+62.81	52.78' RT	702.51	218727.44	398454.44
457	392+60.13	60.36' RT	702.35	218722.03	398460.40
458	392+58.74	63.02' RT	702.28	218719.72	398462.31
459	392+60.62	50.63' RT	702.97	218726.26	398451.62
460	392+59.85	54.39' RT	702.38	218724.09	398454.78
461	392+57.99	59.05' RT	702.26	218720.57	398458.36
462	392+56.50	61.91' RT	702.69	218718.09	398460.42
463	392+37.83	44.53' RT	703.23	218707.61	398437.16

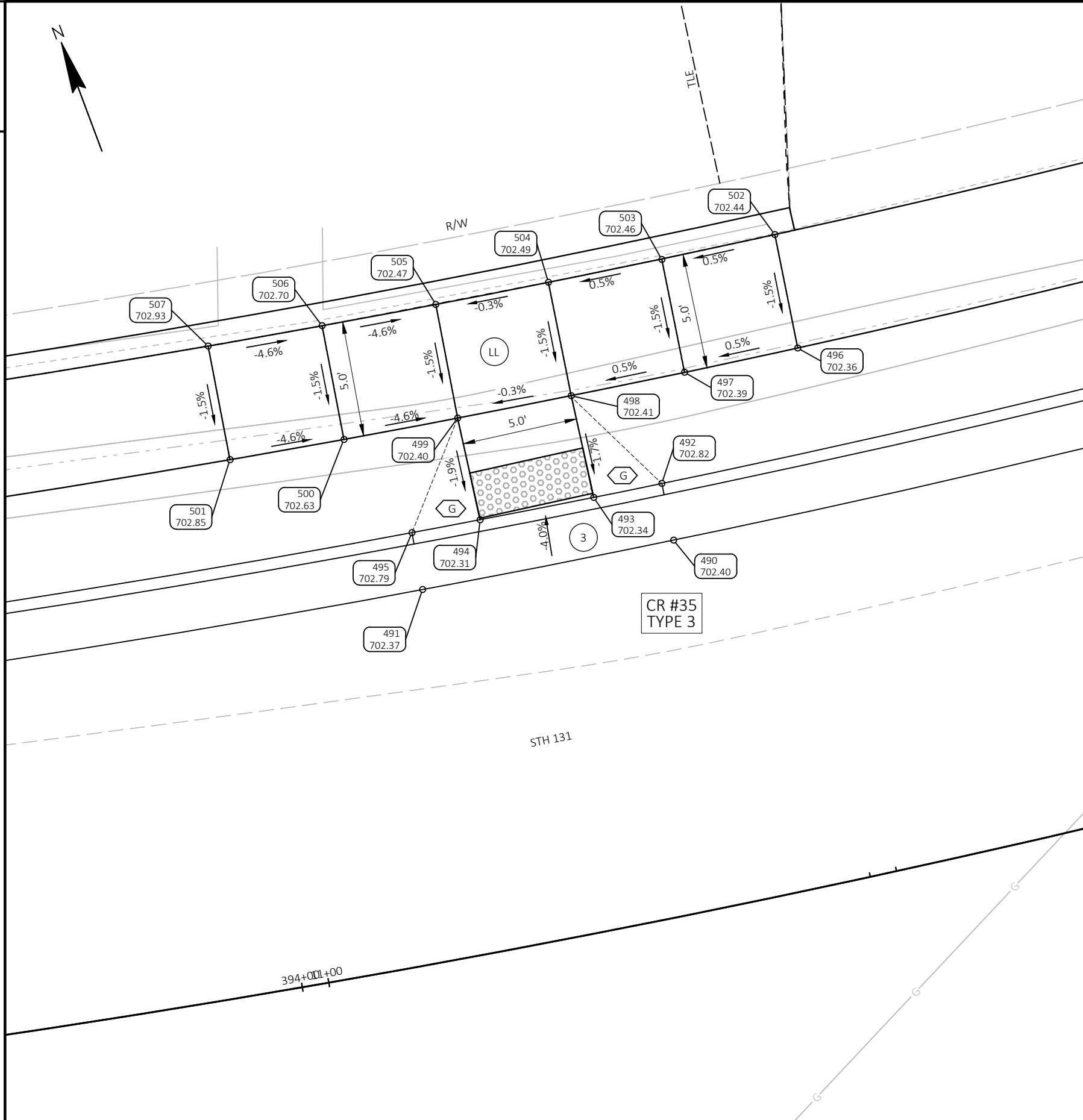


- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

STA 171 - MAIN ST SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
465	11+06.81	52.50' RT	702.78	218745.46	398492.93
466	10+97.41	43.89' RT	703.13	218755.99	398484.77
467	11+08.25	50.55' RT	703.20	218747.05	398494.86
468	11+07.99	50.33' RT	703.21	218747.33	398494.63
469	11+05.83	48.45' RT	702.78	218749.64	398492.71
470	10+99.45	42.64' RT	703.50	218756.75	398487.18
471	11+17.50	48.65' RT	703.07	218747.01	398505.08
472	11+13.86	45.57' RT	703.10	218750.76	398501.78
473	11+10.21	42.61' RT	703.11	218754.42	398498.50
474	11+06.52	39.41' RT	703.21	218758.34	398495.25
475	11+20.39	44.80' RT	703.15	218750.23	398508.85
476	11+16.75	41.69' RT	703.17	218754.00	398505.56
477	11+13.08	38.60' RT	703.21	218757.77	398502.28
478	11+12.71	35.42' RT	703.26	218760.96	398502.49
479	11+08.00	35.56' RT	703.24	218761.80	398497.56
480	11+15.24	24.41' RT	703.88	218771.28	398507.18
481	11+12.47	25.00' RT	703.44	218771.25	398504.23
482	11+07.71	26.04' RT	703.43	218771.20	398499.15
483	11+04.76	26.70' RT	703.90	218771.16	398496.01
484	11+14.77	21.96' RT	703.46	218773.77	398507.15
485	11+04.24	24.26' RT	703.48	218773.66	398495.97
486	11+12.49	25.90' RT	703.45	218770.36	398504.08
487	11+02.22	45.33' RT	702.89	218753.49	398489.52

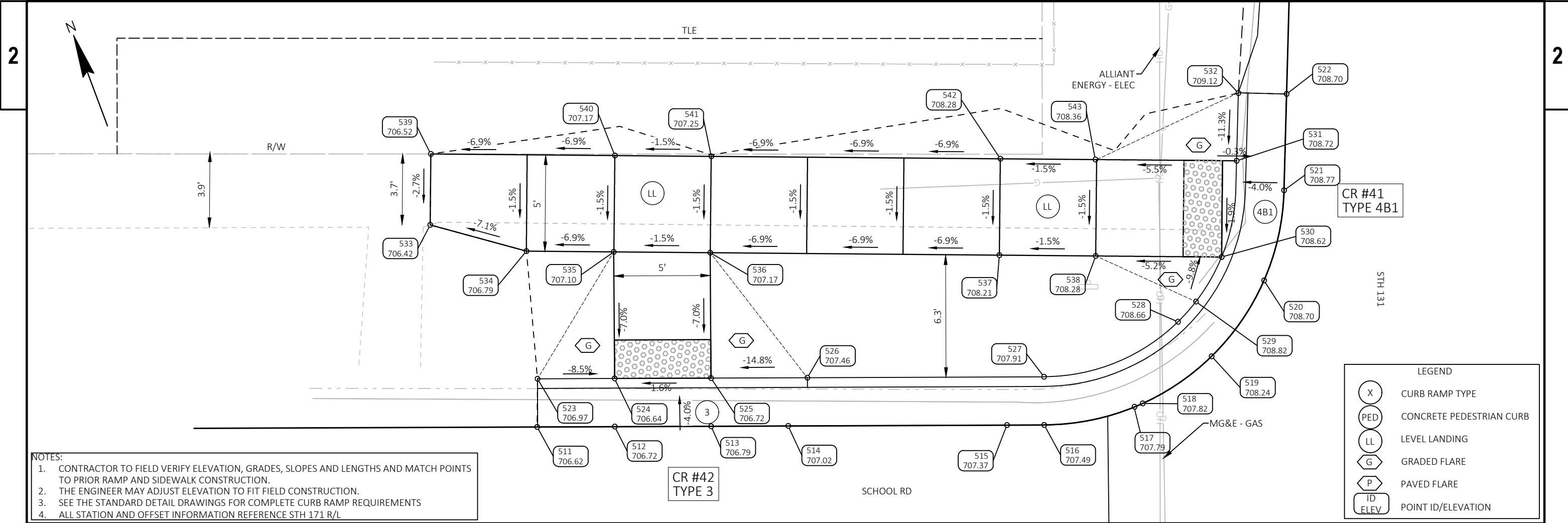


- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND

(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

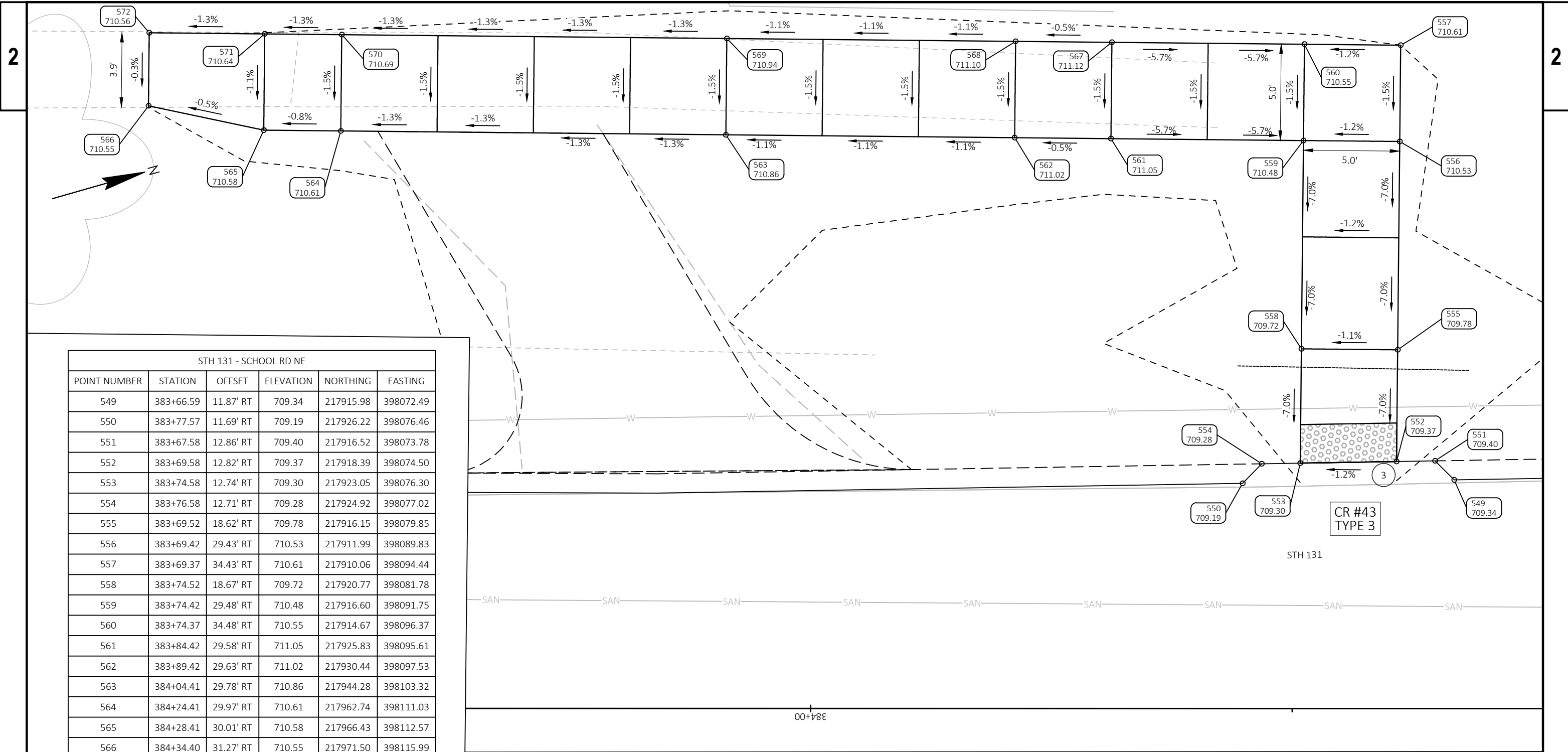
STH 131 STA 11+15					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
490	11+18.47	16.00' LT	702.40	218810.41	398517.78
491	11+07.10	16.00' LT	702.37	218812.51	398506.93
492	11+18.47	18.50' LT	702.82	218812.87	398518.23
493	11+15.37	18.50' LT	702.34	218813.42	398515.28
494	11+10.20	18.50' LT	702.31	218814.37	398510.37
495	11+07.10	18.50' LT	702.79	218814.96	398507.43
496	11+25.69	23.00' LT	702.36	218816.08	398525.88
497	11+20.49	23.00' LT	702.39	218816.95	398520.95
498	11+15.28	23.00' LT	702.41	218817.86	398516.03
499	11+10.07	23.00' LT	702.40	218818.81	398511.12
500	11+04.86	23.00' LT	702.63	218819.80	398506.22
501	10+99.66	23.00' LT	702.85	218820.84	398501.33
502	11+25.78	28.00' LT	702.44	218821.00	398526.80
503	11+20.53	27.98' LT	702.46	218821.84	398521.88
504	11+15.28	28.00' LT	702.49	218822.77	398516.96
505	11+10.07	28.00' LT	702.47	218823.71	398512.10
506	11+04.82	28.00' LT	702.70	218824.71	398507.20
507	10+99.56	28.00' LT	702.93	218825.75	398502.31



NOTES:
 1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

LEGEND	
(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

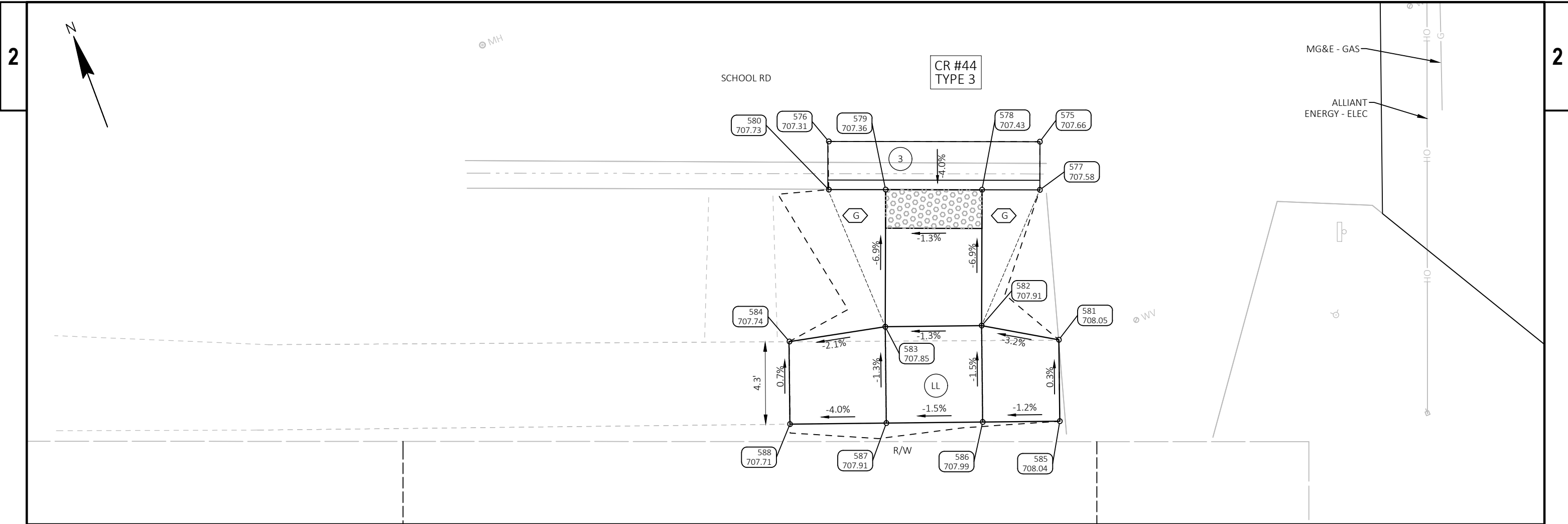
SCHOOL RD NW						SCHOOL RD NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING	POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
511	383+59.64	52.45' LT	706.62	217933.75	398010.28	528	383+65.10	19.20' LT	708.66	217926.30	398043.14
512	383+59.66	48.42' LT	706.72	217932.25	398014.02	529	383+66.15	18.27' LT	708.82	217926.92	398044.40
513	383+59.68	43.42' LT	706.79	217930.39	398018.66	530	383+68.46	16.94' LT	708.62	217928.56	398046.50
514	383+59.70	39.42' LT	707.02	217928.90	398022.37	531	383+73.46	16.16' LT	708.72	217932.89	398049.11
515	383+59.74	28.08' LT	707.37	217924.67	398032.90	532	383+76.97	16.07' LT	709.12	217936.11	398050.52
516	383+59.75	26.14' LT	707.49	217923.95	398034.70	533	383+70.13	58.00' LT	706.42	217945.55	398009.09
517	383+60.69	21.46' LT	707.79	217923.06	398039.40	534	383+68.77	53.01' LT	706.79	217942.41	398013.20
518	383+60.87	21.04' LT	707.82	217923.06	398039.85	535	383+68.73	48.48' LT	707.10	217940.67	398017.39
519	383+63.32	17.46' LT	708.24	217923.98	398044.09	536	383+68.69	43.47' LT	707.17	217938.75	398022.01
520	383+67.25	14.74' LT	708.70	217926.61	398048.09	537	383+68.56	28.47' LT	708.21	217932.99	398035.86
521	383+71.92	13.70' LT	708.77	217930.54	398050.81	538	383+68.52	23.47' LT	708.28	217931.07	398040.48
522	383+76.92	13.57' LT	708.70	217935.13	398052.81	539	383+73.80	57.97' LT	706.52	217948.95	398010.50
523	383+62.14	52.43' LT	706.97	217936.06	398011.24	540	383+73.73	48.42' LT	707.17	217945.28	398019.32
524	383+62.16	48.43' LT	706.64	217934.57	398014.96	541	383+73.69	43.42' LT	707.25	217943.36	398023.94
525	383+62.18	43.43' LT	706.72	217932.71	398019.60	542	383+73.56	28.42' LT	708.28	217937.60	398037.79
526	383+62.20	38.43' LT	707.46	217930.85	398024.24	543	383+73.52	23.49' LT	708.36	217935.71	398042.34
527	383+62.25	26.15' LT	707.91	217926.27	398035.63						



STH 131 - SCHOOL RD NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
549	383+66.59	11.87' RT	709.34	217915.98	398072.49
550	383+77.57	11.69' RT	709.19	217926.22	398076.46
551	383+67.58	12.86' RT	709.40	217916.52	398073.78
552	383+69.58	12.82' RT	709.37	217918.39	398074.50
553	383+74.58	12.74' RT	709.30	217923.05	398076.30
554	383+76.58	12.71' RT	709.28	217924.92	398077.02
555	383+69.52	18.62' RT	709.78	217916.15	398079.85
556	383+69.42	29.43' RT	710.53	217911.99	398089.83
557	383+69.37	34.43' RT	710.61	217910.06	398094.44
558	383+74.52	18.67' RT	709.72	217920.77	398081.78
559	383+74.42	29.48' RT	710.48	217916.60	398091.75
560	383+74.37	34.48' RT	710.55	217914.67	398096.37
561	383+84.42	29.58' RT	711.05	217925.83	398095.61
562	383+89.42	29.63' RT	711.02	217930.44	398097.53
563	384+04.41	29.78' RT	710.86	217944.28	398103.32
564	384+24.41	29.97' RT	710.61	217962.74	398111.03
565	384+28.41	30.01' RT	710.58	217966.43	398112.57
566	384+34.40	31.27' RT	710.55	217971.50	398115.99
567	383+84.37	34.58' RT	711.12	217923.90	398100.22
568	383+89.37	34.63' RT	711.10	217928.51	398102.15
569	384+04.37	34.77' RT	710.94	217942.35	398107.93
570	384+24.36	34.97' RT	710.69	217960.81	398115.64
571	384+28.36	35.01' RT	710.64	217964.50	398117.18
572	384+34.36	35.07' RT	710.56	217970.04	398119.49

LEGEND	
(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

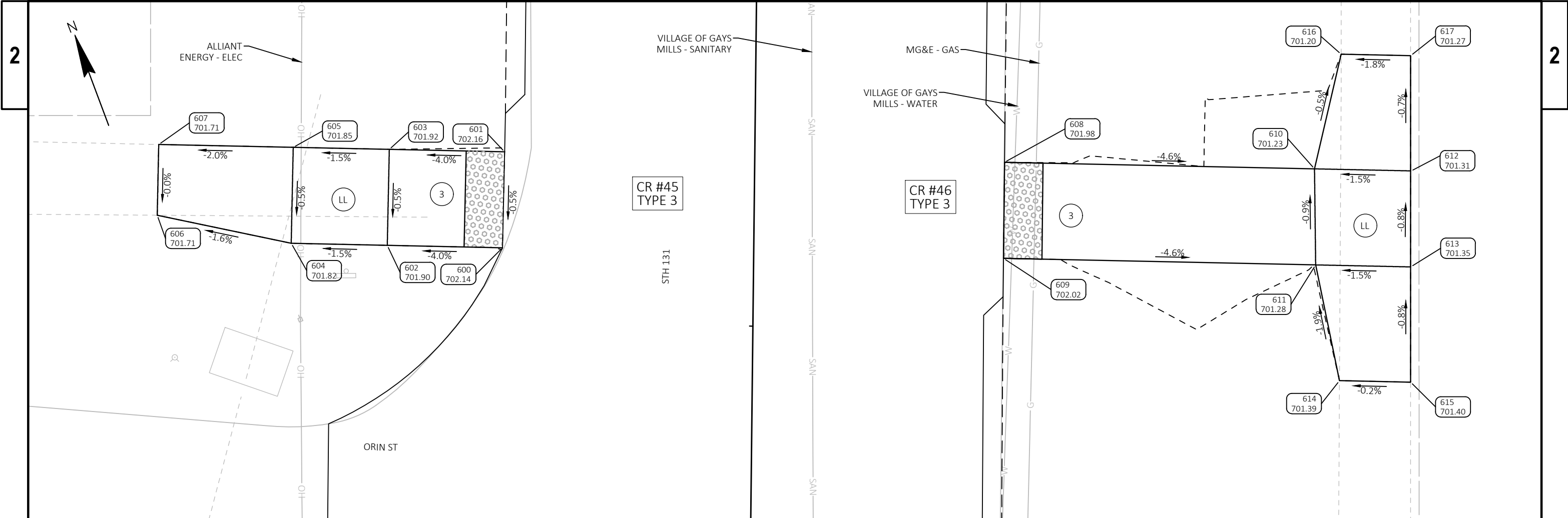
- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L



SCHOOL RD SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
575	383+23.94	40.41' LT	707.66	217895.52	398008.00
576	383+24.11	51.36' LT	707.31	217899.65	397997.86
577	383+21.44	40.45' LT	707.58	217893.20	398007.06
578	383+21.49	43.44' LT	707.43	217894.34	398004.28
579	383+21.57	48.44' LT	707.36	217896.22	397999.65
580	383+21.61	51.40' LT	707.73	217897.34	397996.92
581	383+13.65	39.57' LT	708.05	217885.63	398005.05
582	383+14.44	43.57' LT	707.91	217887.81	398001.61
583	383+14.45	48.57' LT	707.85	217889.64	397996.96
584	383+13.76	53.55' LT	707.74	217890.80	397992.06
585	383+09.43	39.58' LT	708.04	217881.69	398003.50
586	383+09.44	43.58' LT	707.99	217883.16	397999.78
587	383+09.46	48.58' LT	707.91	217884.99	397995.13
588	383+09.47	53.58' LT	707.71	217886.82	397990.47

LEGEND	
(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 3. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

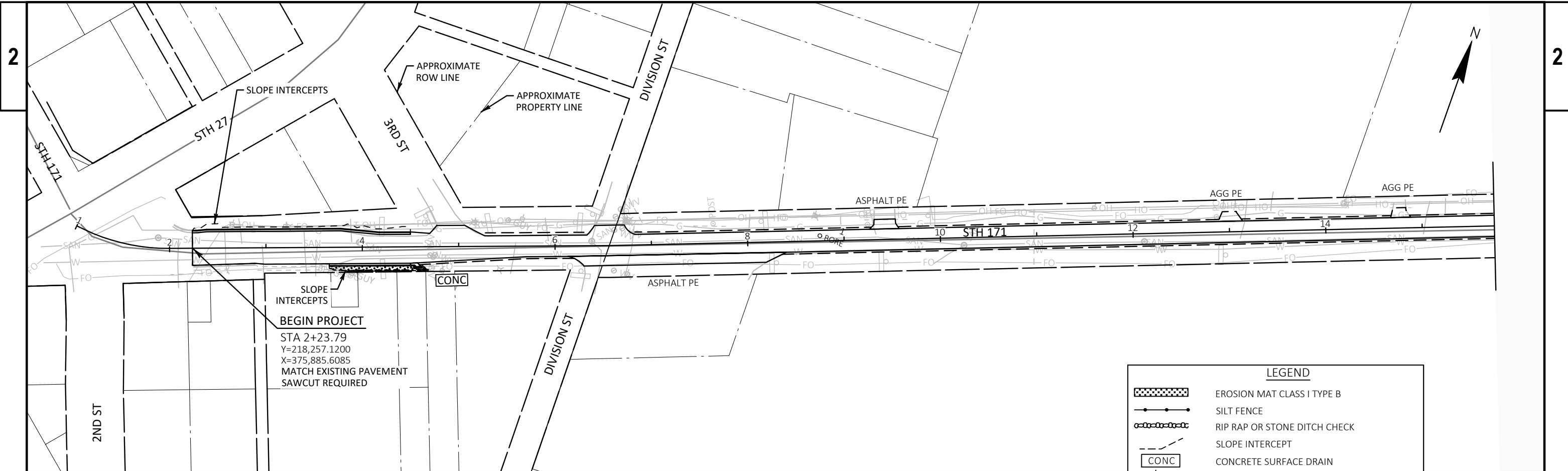


ORIN-131 NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
600	390+28.94	13.07' LT	702.14	218537.30	398303.20
601	390+33.94	13.02' LT	702.16	218541.89	398305.18
602	390+29.00	19.07' LT	701.90	218539.67	398297.69
603	390+34.00	19.02' LT	701.92	218544.26	398299.67
604	390+29.05	24.07' LT	701.82	218541.65	398293.10
605	390+34.05	24.02' LT	701.85	218546.24	398295.08
606	390+30.43	31.06' LT	701.71	218545.63	398287.19
607	390+34.12	31.02' LT	701.71	218549.01	398288.65

ORIN-131 NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
608	390+33.68	13.00' RT	701.98	218531.59	398329.08
609	390+28.68	13.00' RT	702.02	218526.98	398327.14
610	390+33.52	29.13' RT	701.23	218525.21	398343.89
611	390+28.52	29.25' RT	701.28	218520.55	398342.06
612	390+33.47	34.13' RT	701.31	218523.23	398348.48
613	390+28.47	34.19' RT	701.35	218518.59	398346.60
614	390+22.50	30.55' RT	701.39	218514.50	398340.95
615	390+22.47	34.26' RT	701.40	218513.03	398344.35
616	390+39.50	30.45' RT	701.20	218530.22	398347.42
617	390+39.47	34.07' RT	701.27	218528.79	398350.74

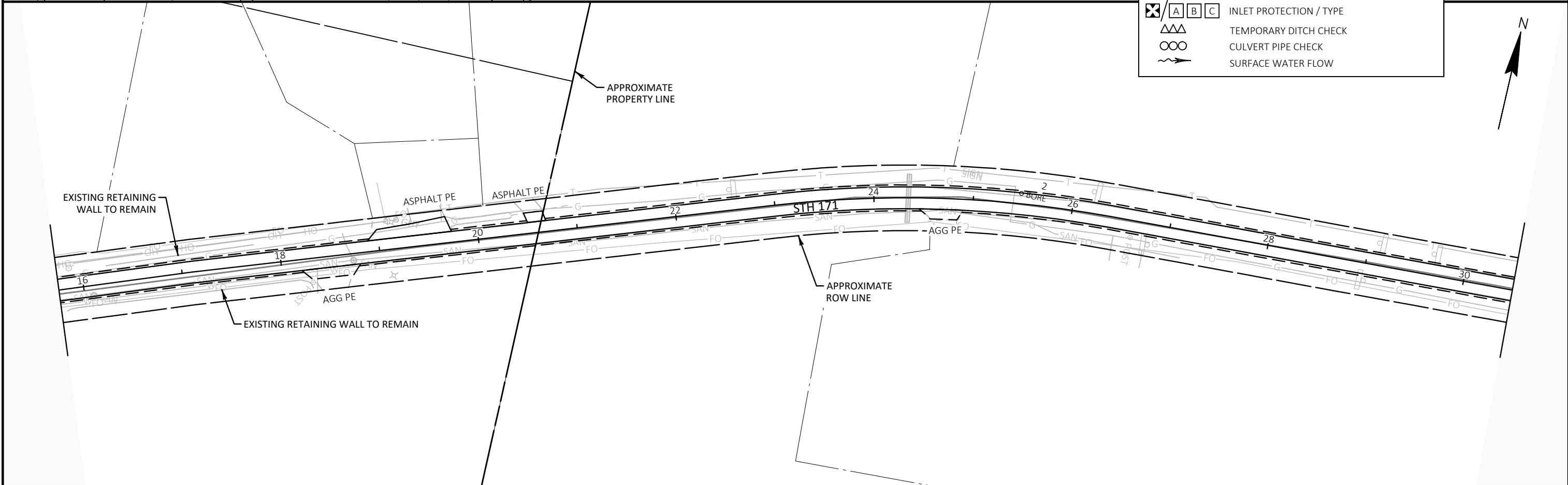
LEGEND	
(X)	CURB RAMP TYPE
(PED)	CONCRETE PEDESTRIAN CURB
(LL)	LEVEL LANDING
(G)	GRADED FLARE
(P)	PAVED FLARE
(ID ELEV)	POINT ID/ELEVATION

- NOTES:
- CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES AND LENGTHS AND MATCH POINTS TO PRIOR RAMP AND SIDEWALK CONSTRUCTION.
 - THE ENGINEER MAY ADJUST ELEVATION TO FIT FIELD CONSTRUCTION.
 - SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
 - ALL STATION AND OFFSET INFORMATION REFERENCE STH 171 R/L

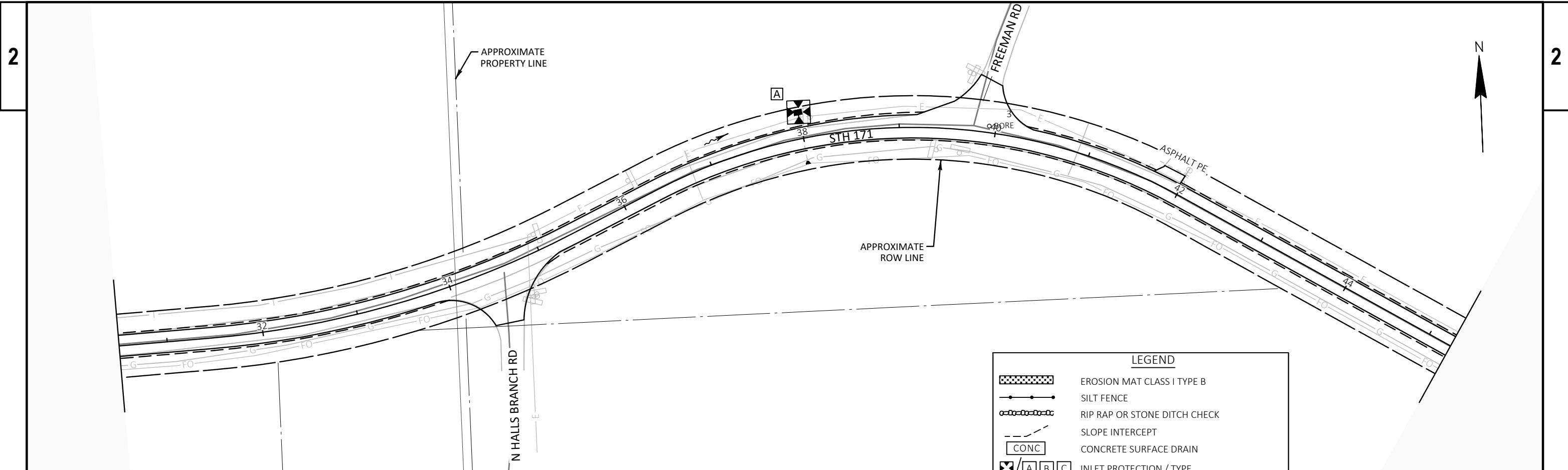


BEGIN PROJECT
 STA 2+23.79
 Y=218,257.1200
 X=375,885.6085
 MATCH EXISTING PAVEMENT
 SAWCUT REQUIRED

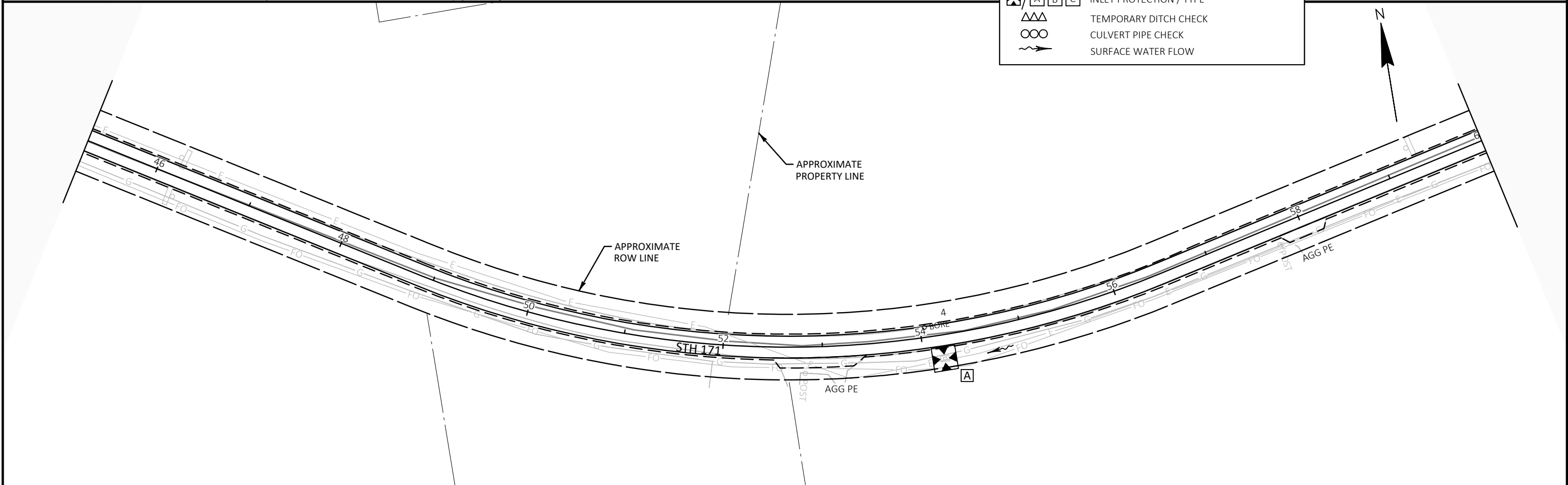
LEGEND	
	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW

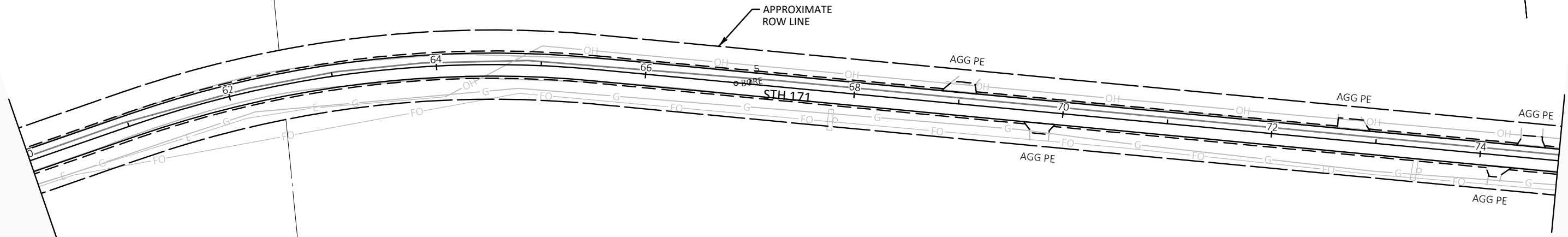


PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	EROSION CONTROL	SHEET	E
------------------------	--------------	------------------	-----------------	-------	----------



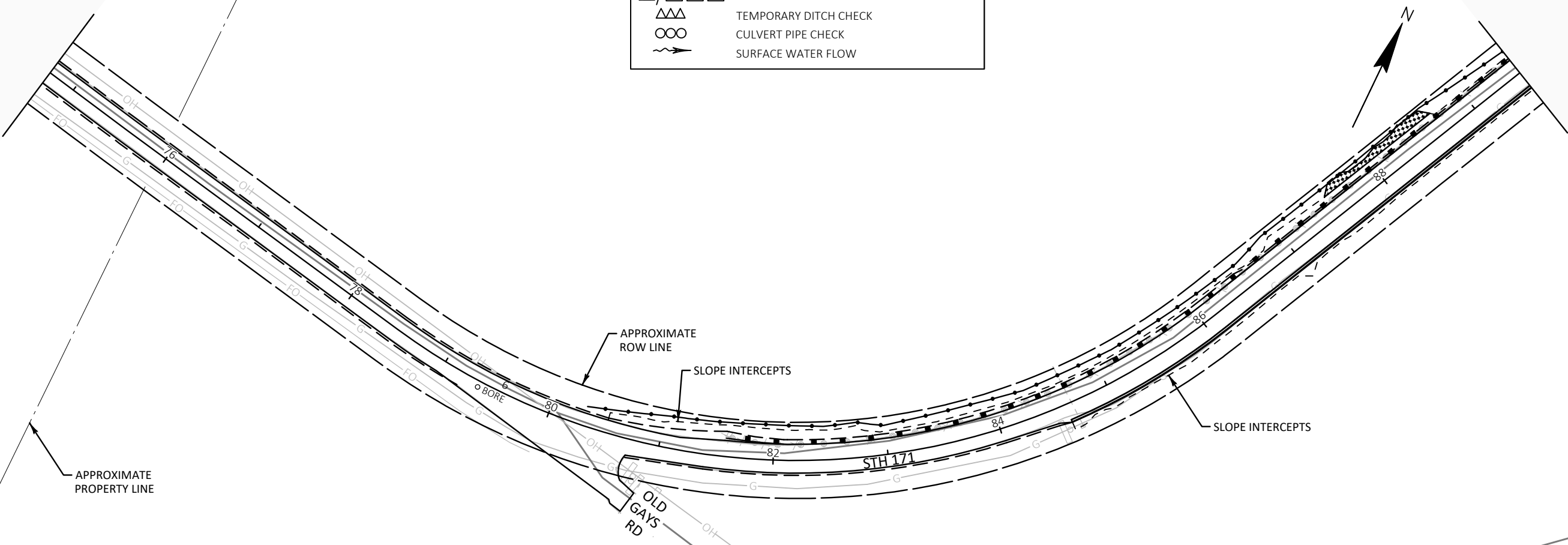
LEGEND	
	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW

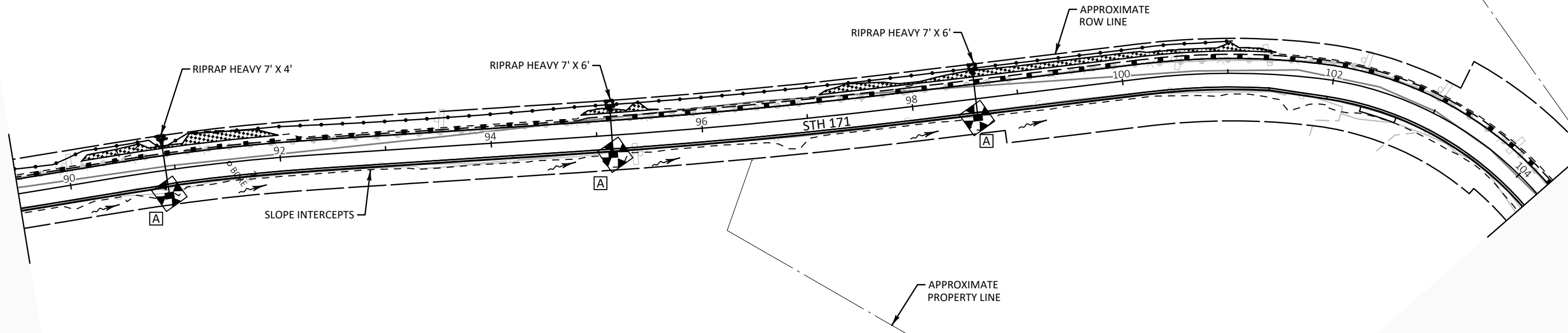




LEGEND

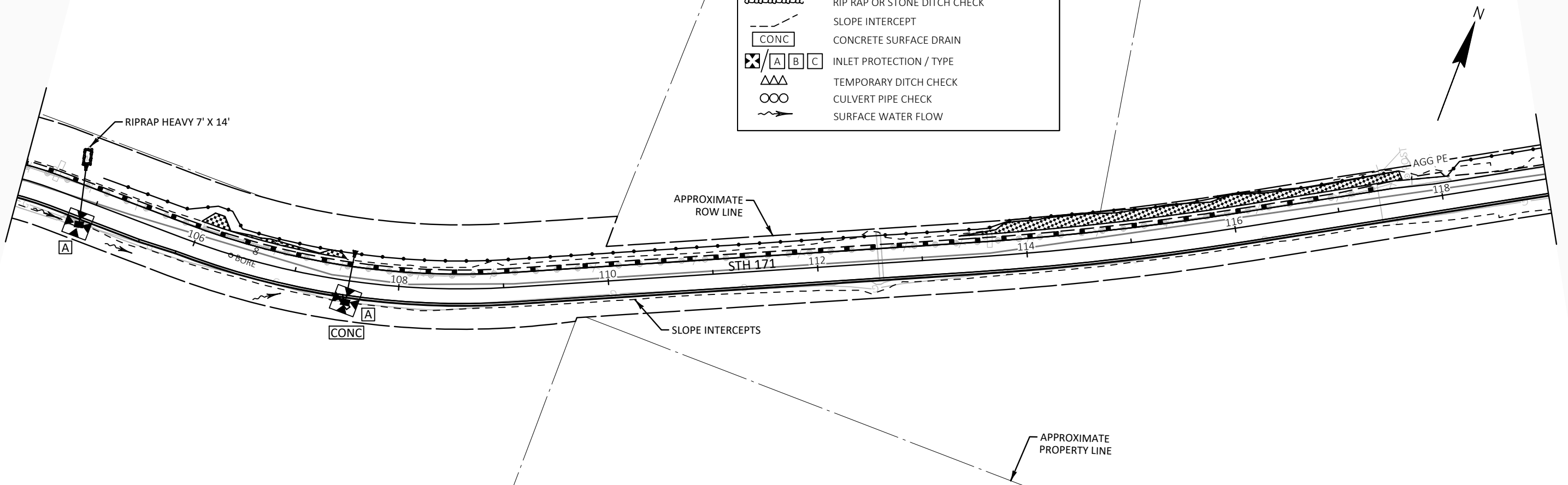
	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW

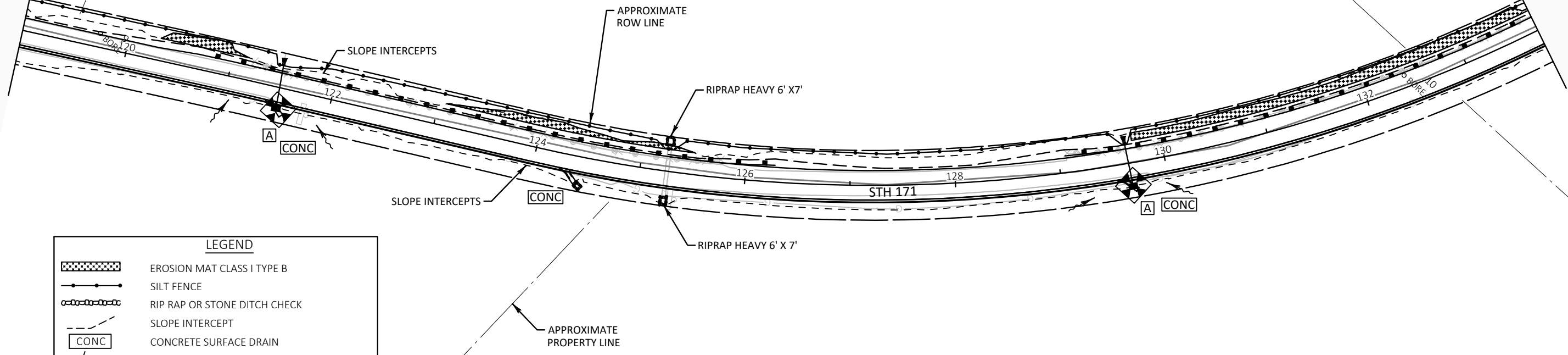




LEGEND

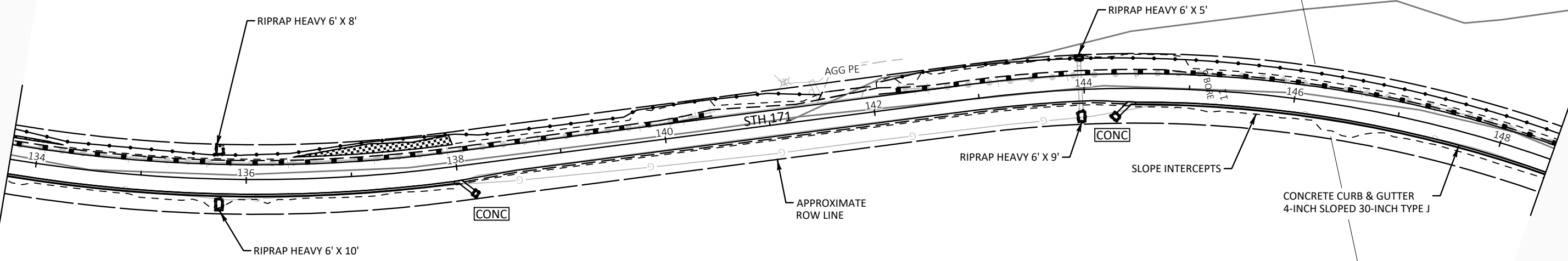
	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW

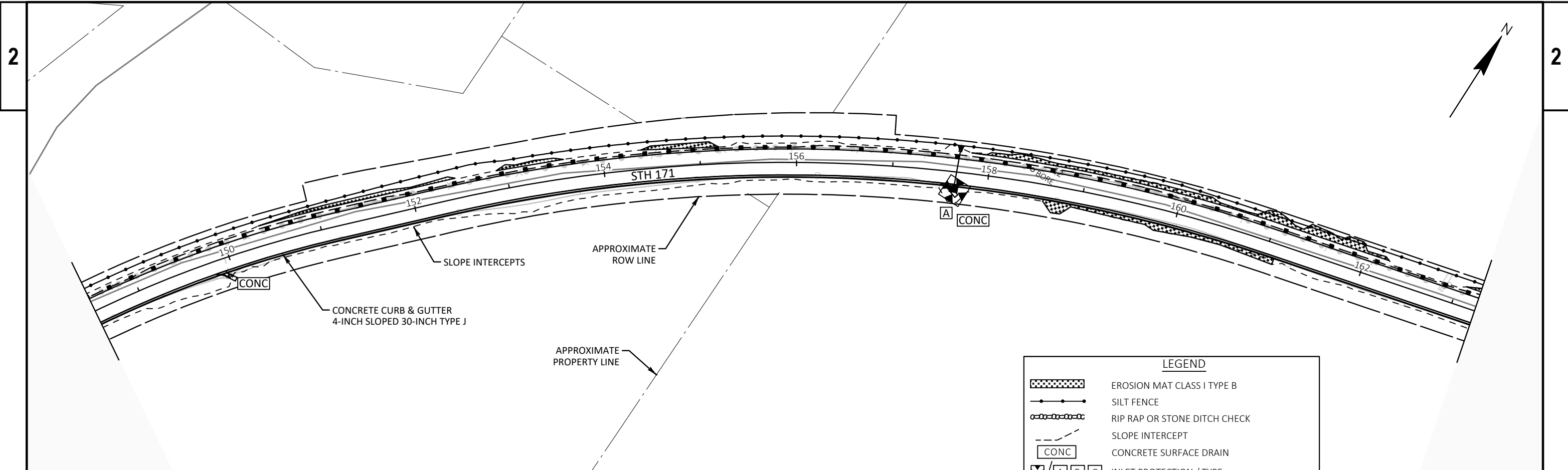




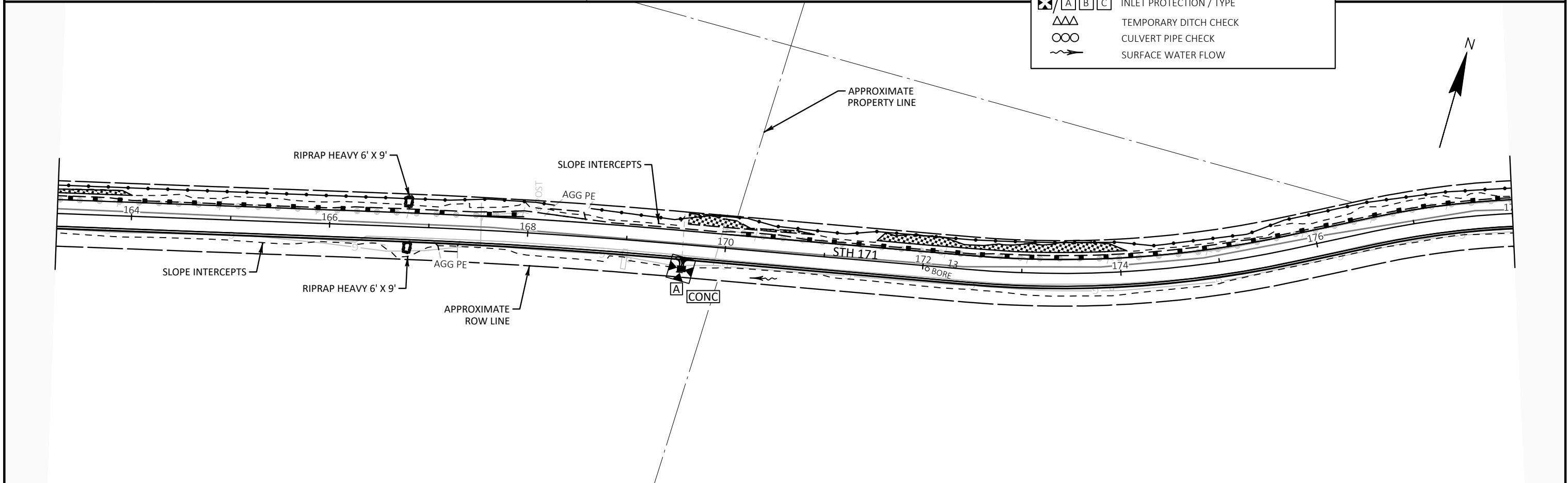
LEGEND

	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW

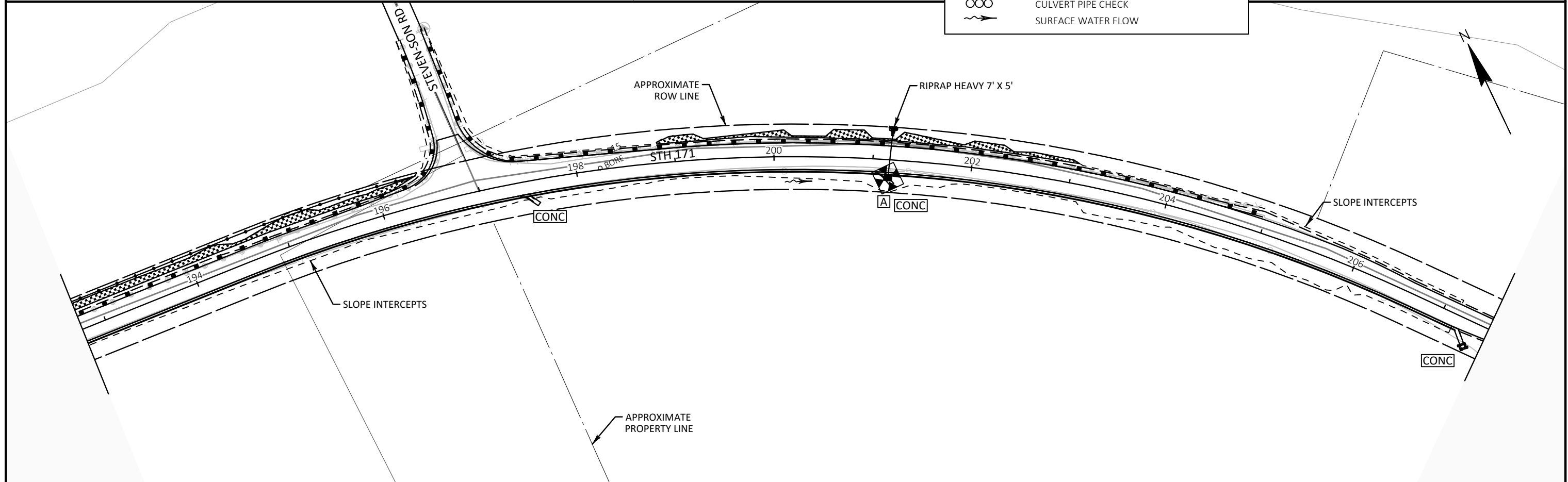
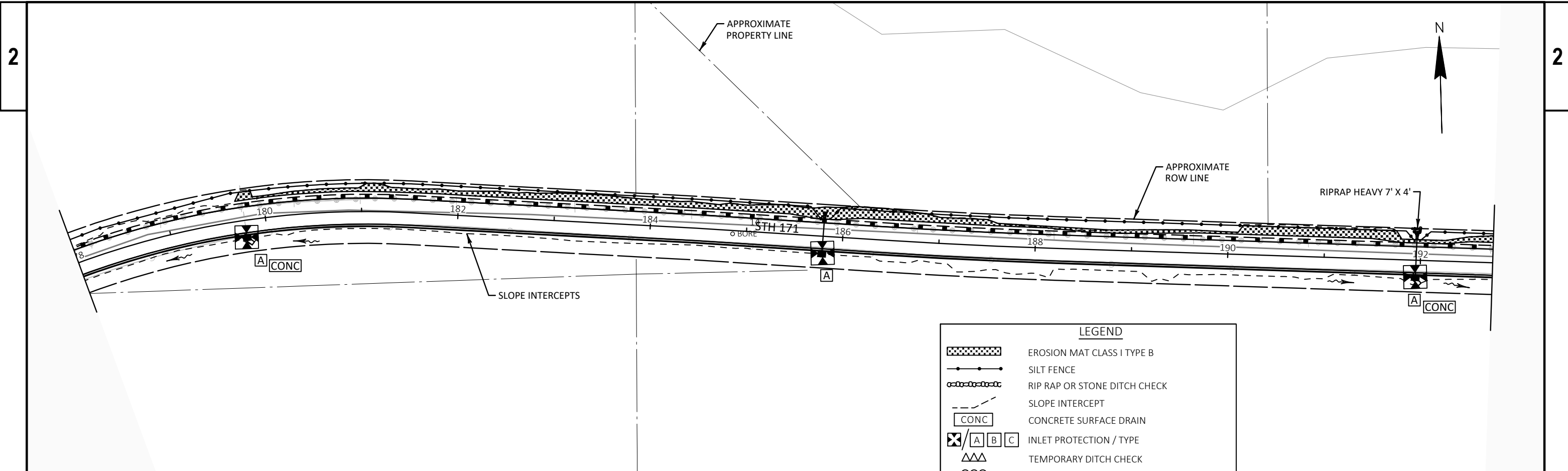


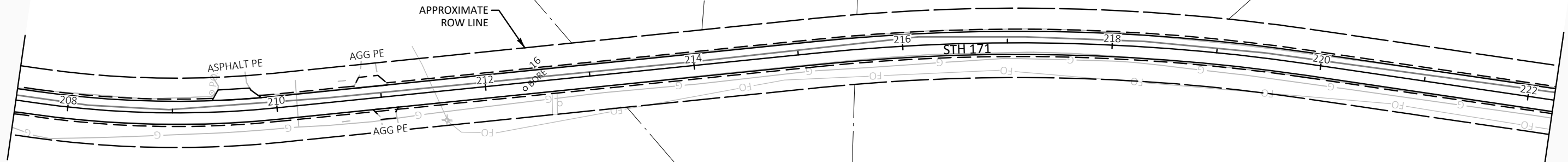


LEGEND	
	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW

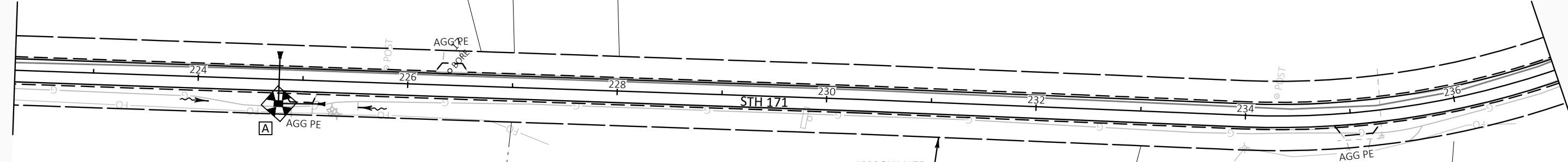


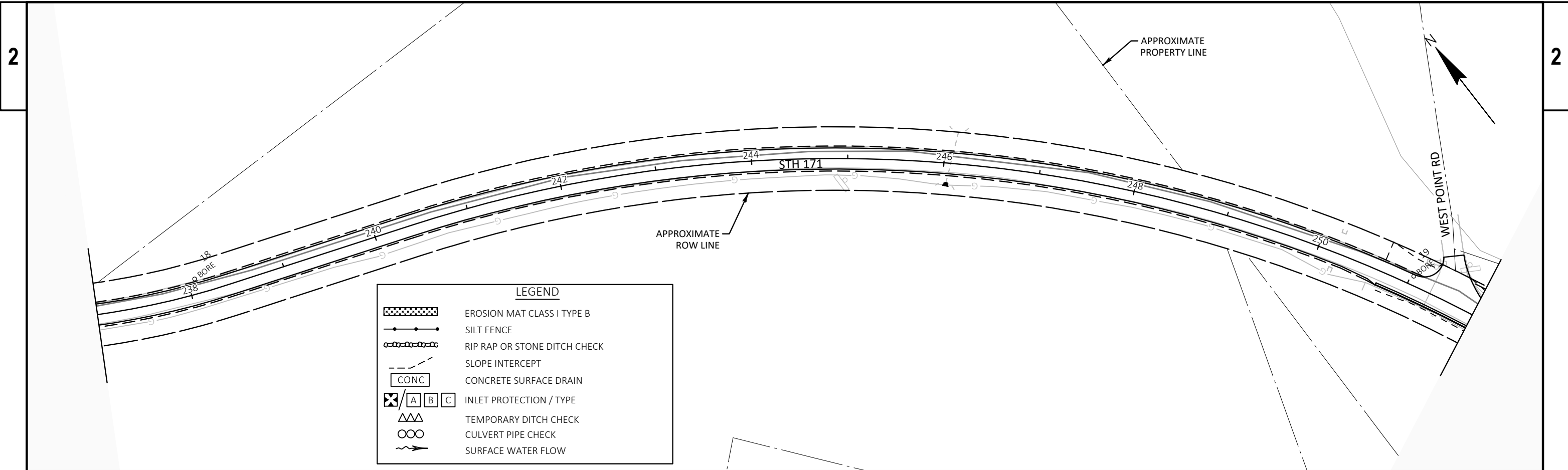
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	EROSION CONTROL	SHEET	E
------------------------	--------------	------------------	-----------------	-------	----------



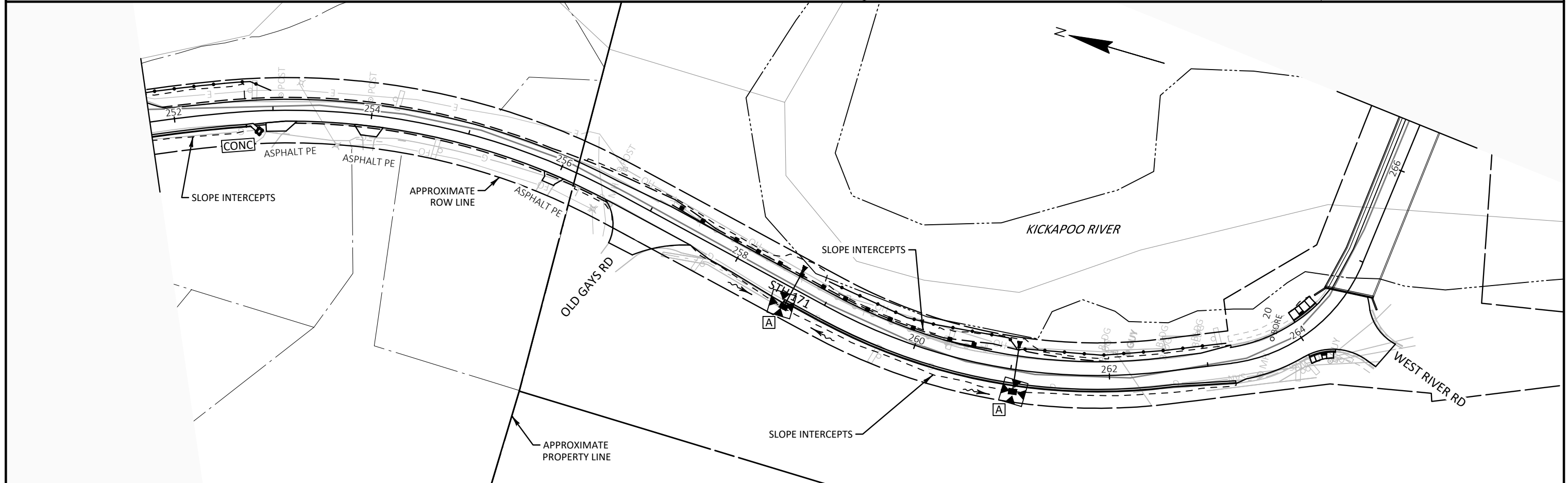


LEGEND	
	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW

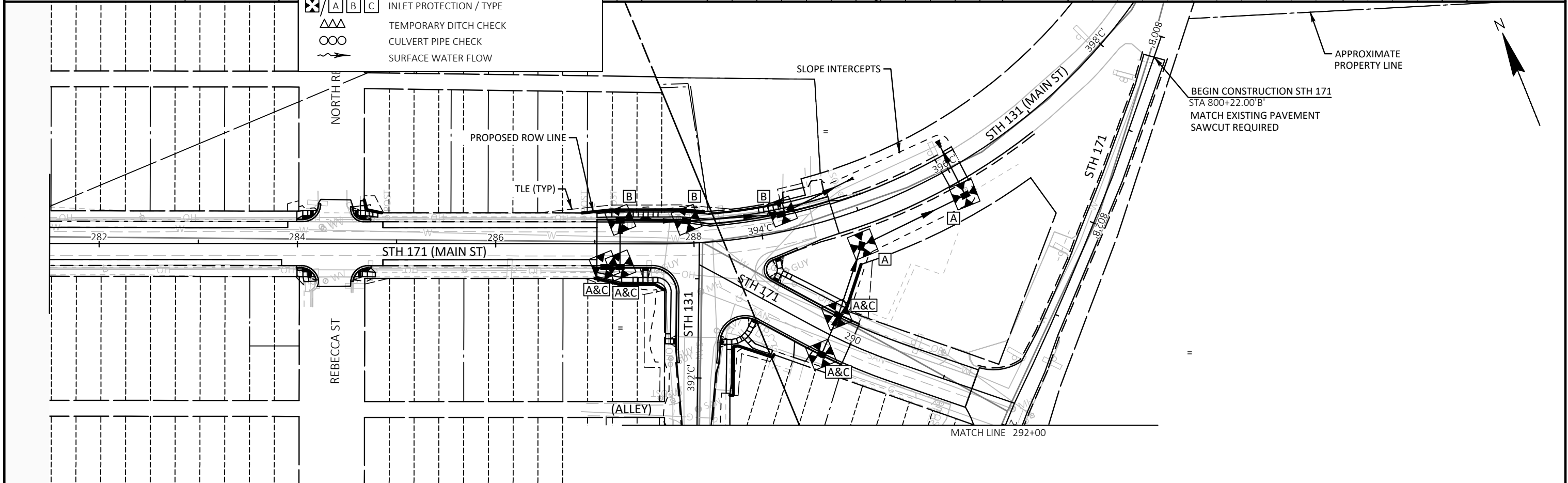
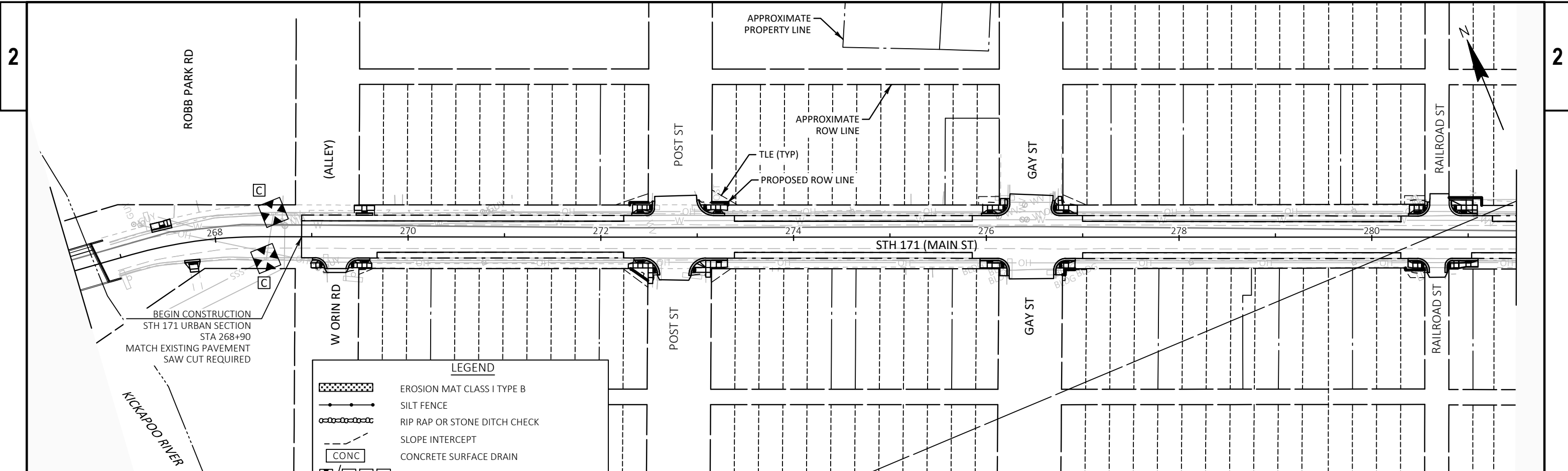


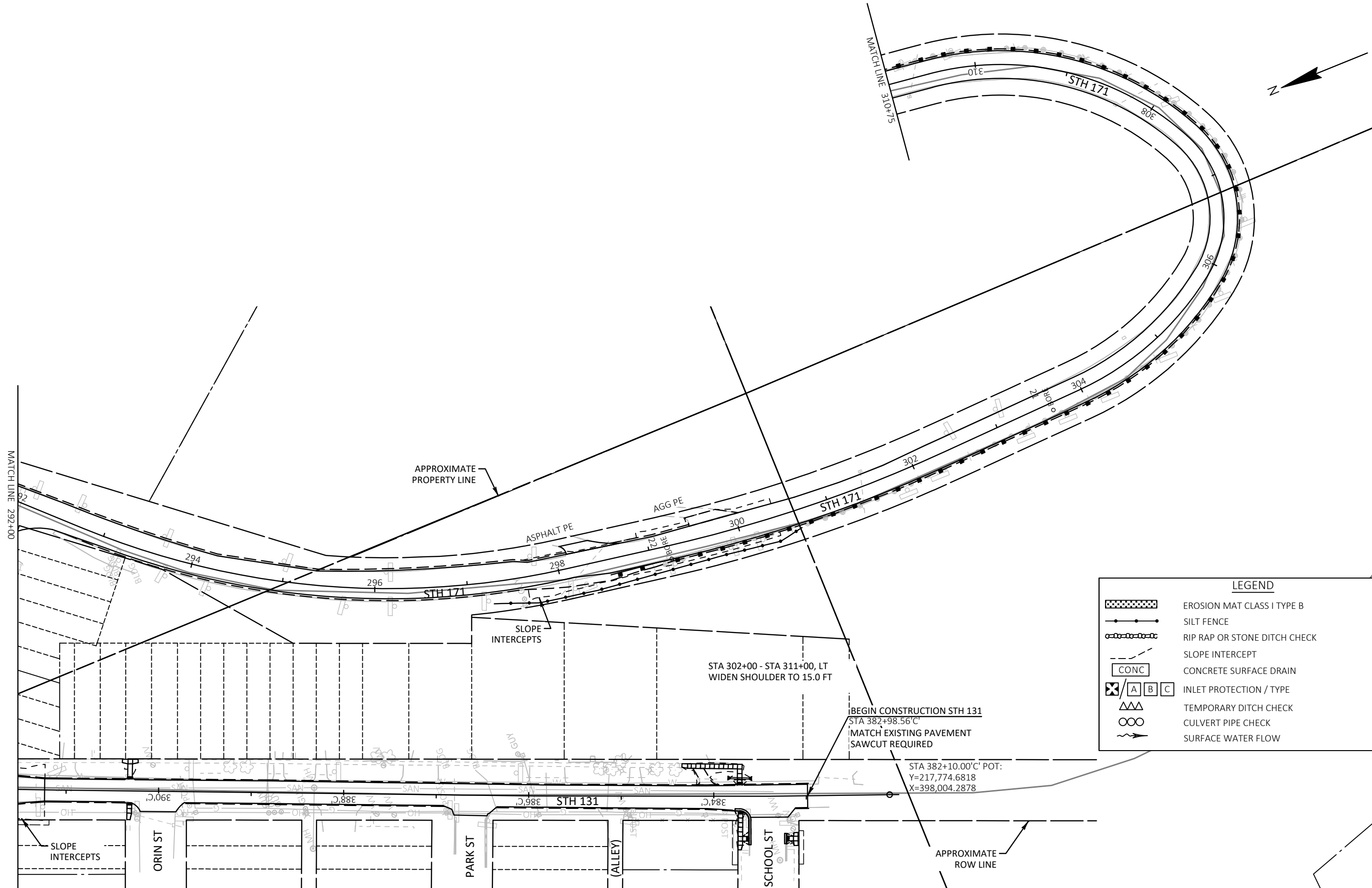


LEGEND	
	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW

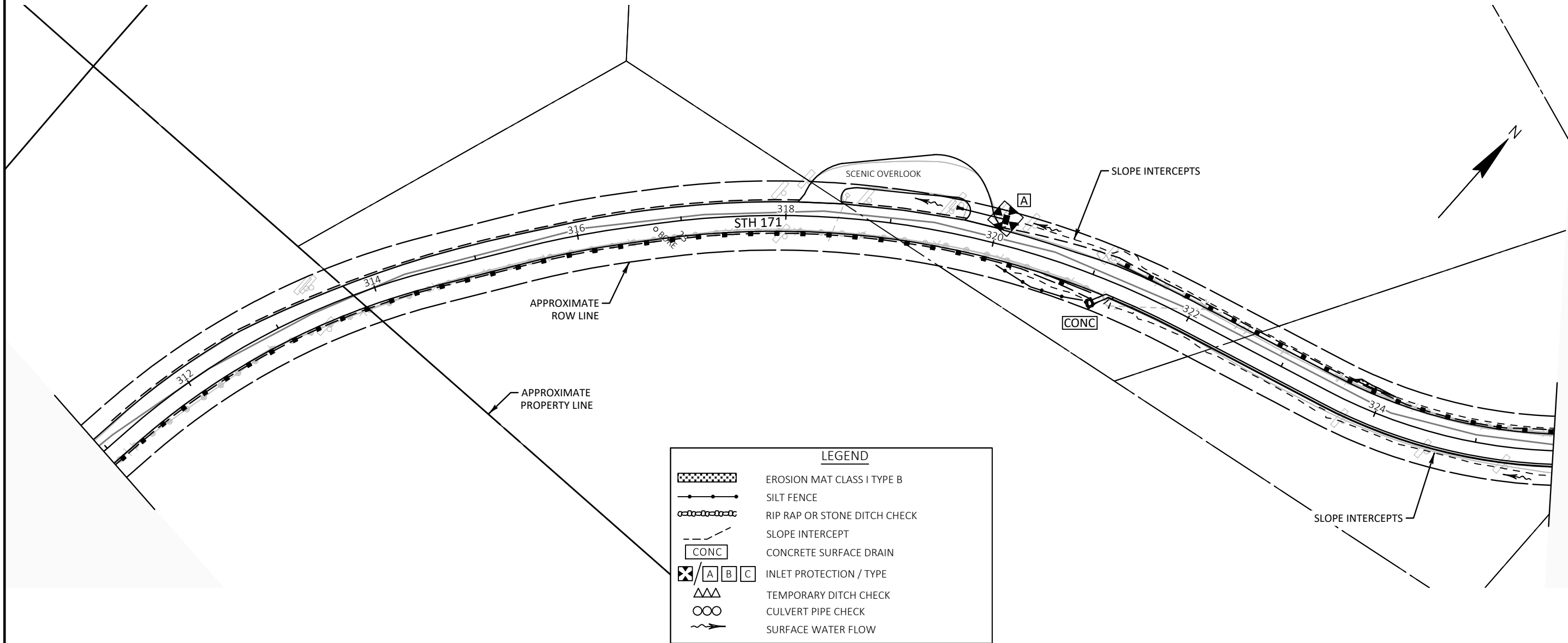


PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	EROSION CONTROL	SHEET	E
------------------------	--------------	------------------	-----------------	-------	----------



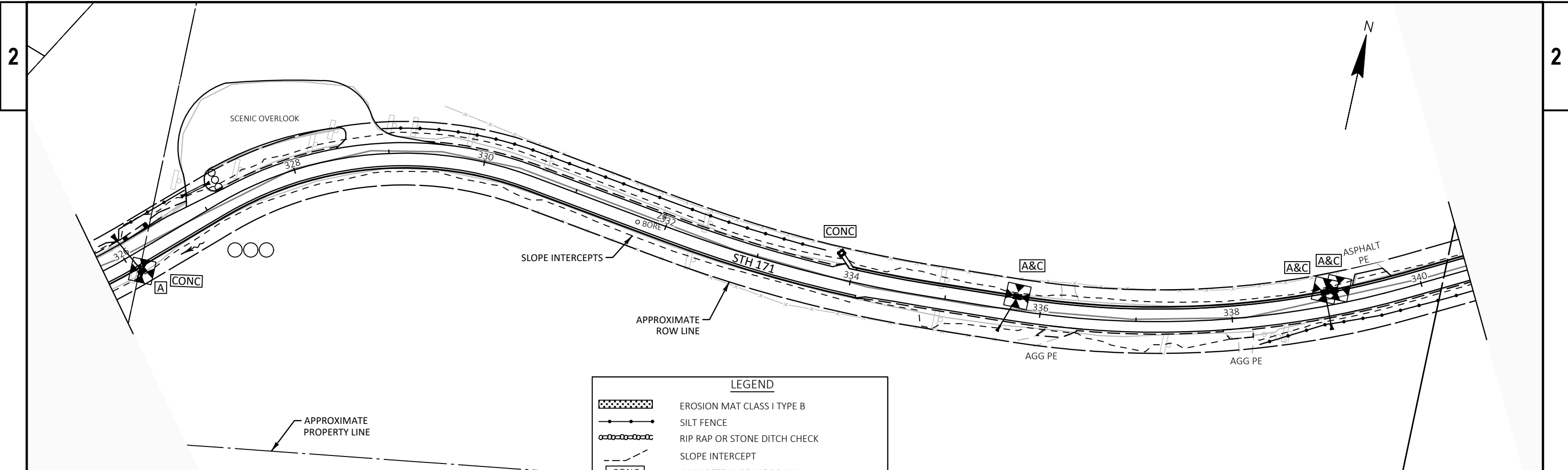


LEGEND	
	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW



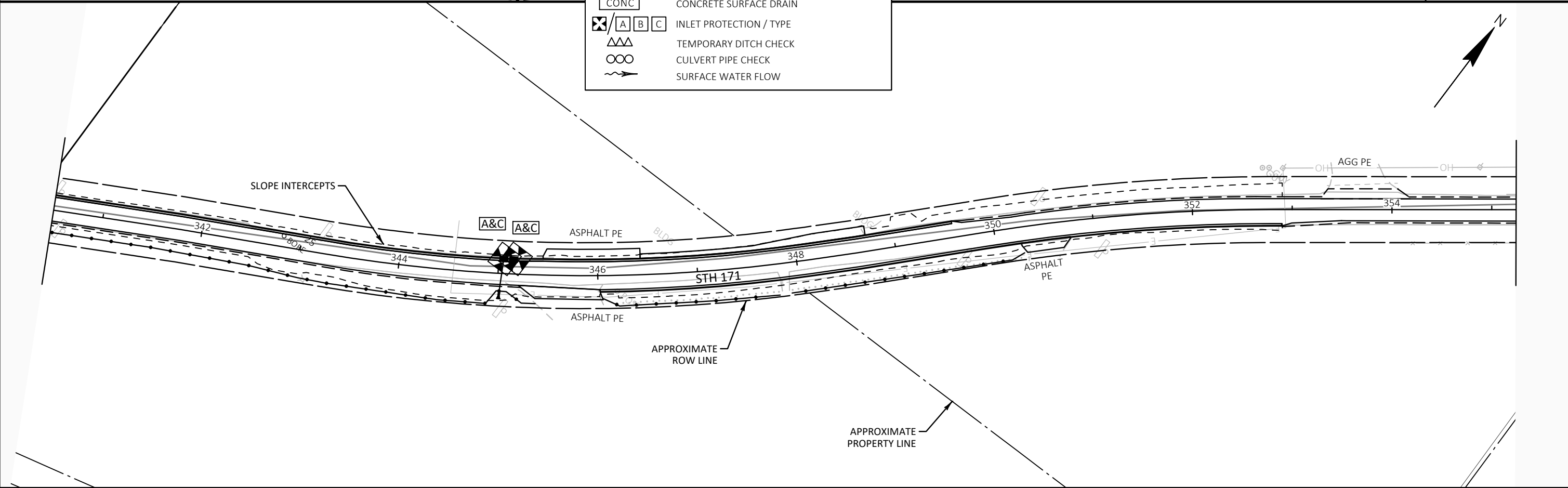
LEGEND

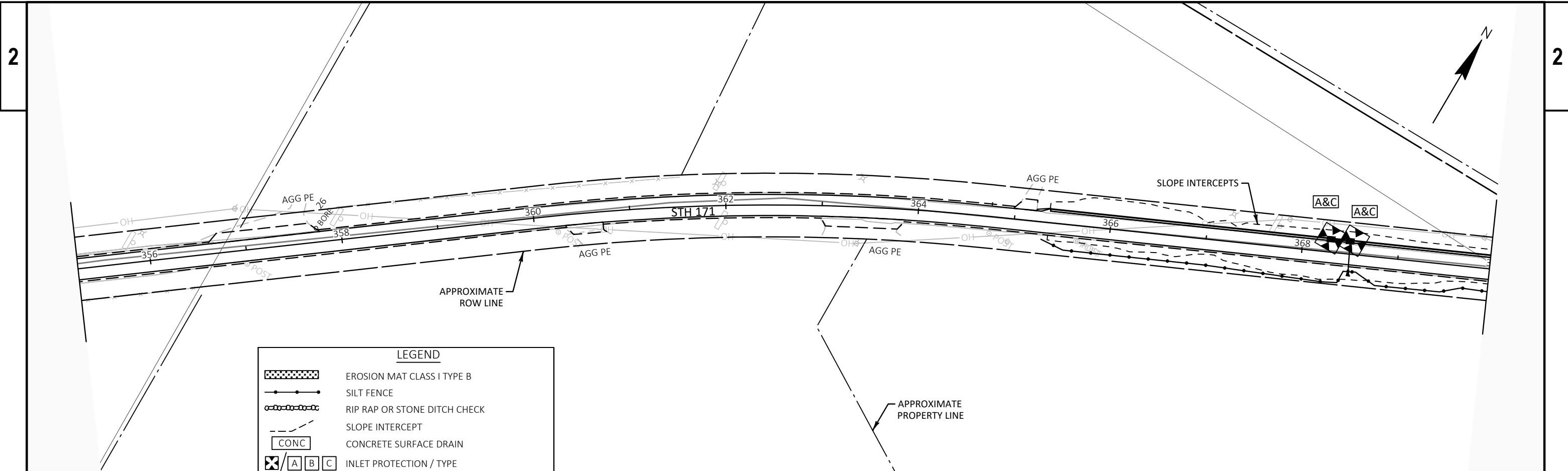
	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW



LEGEND

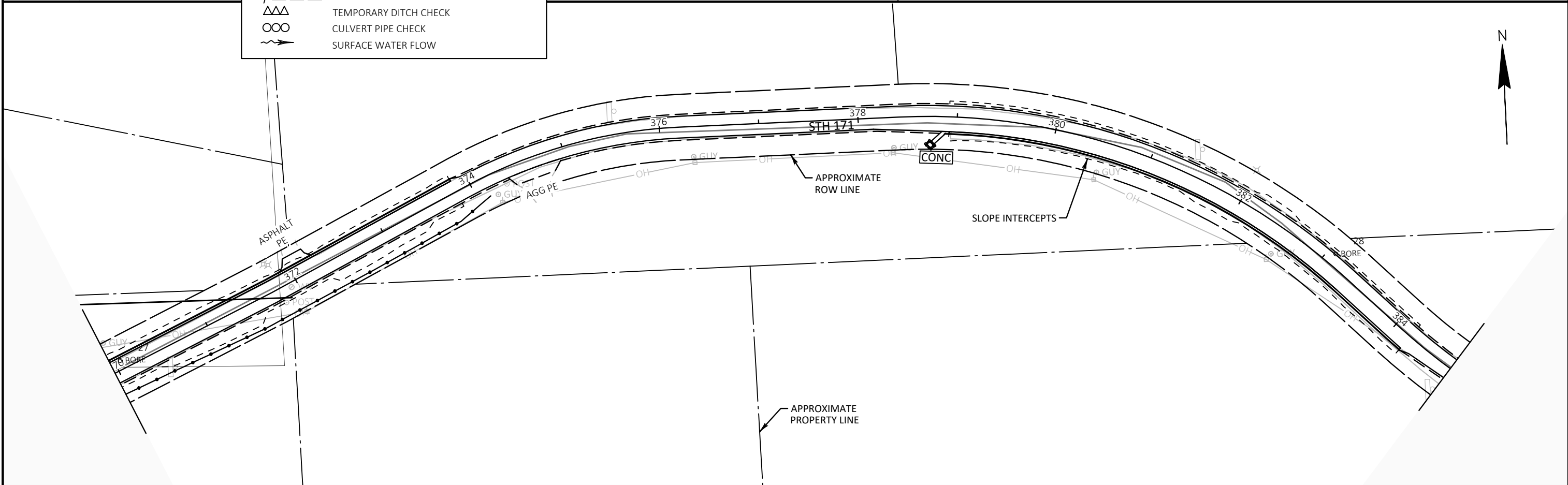
	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW



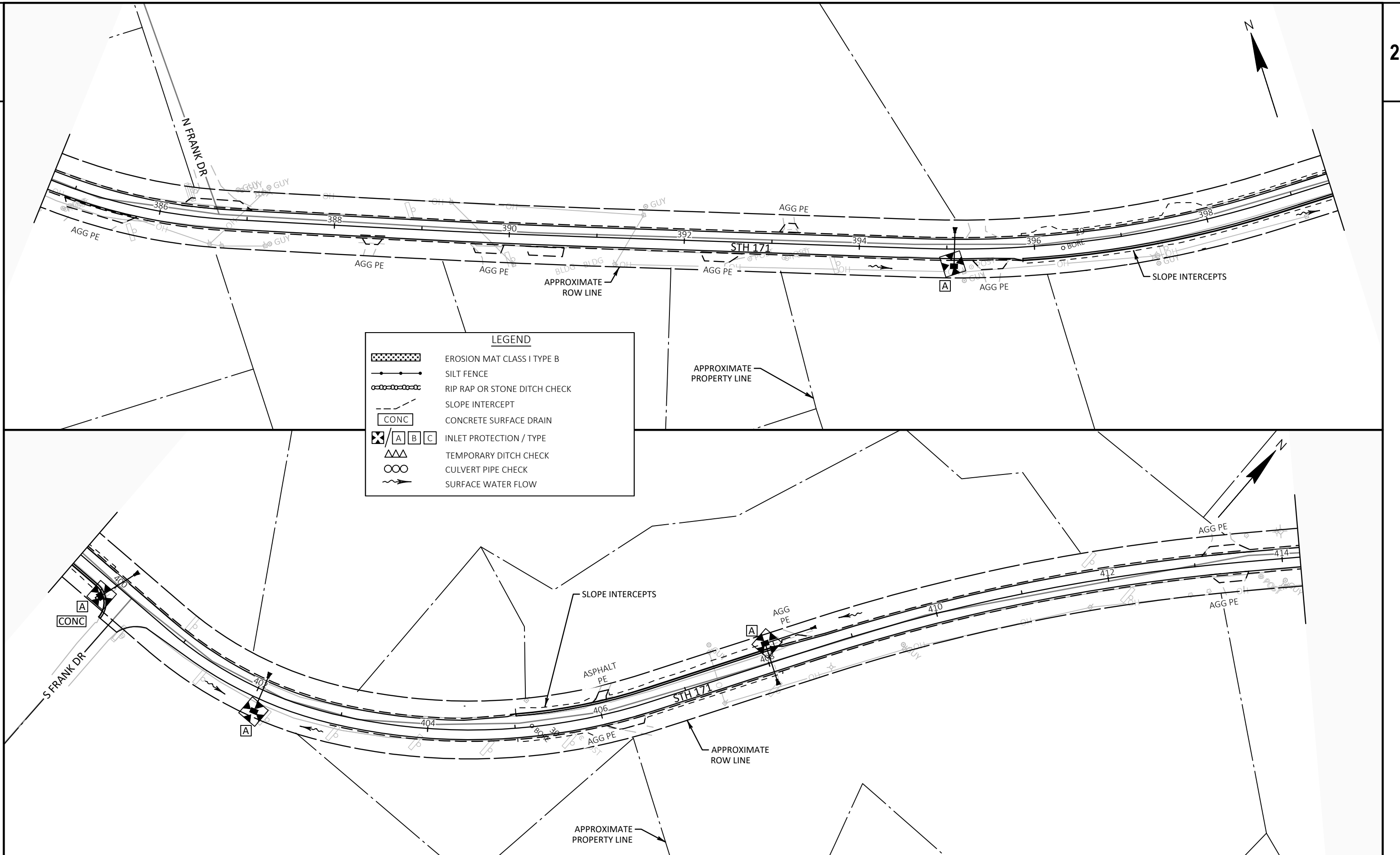


LEGEND

	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW

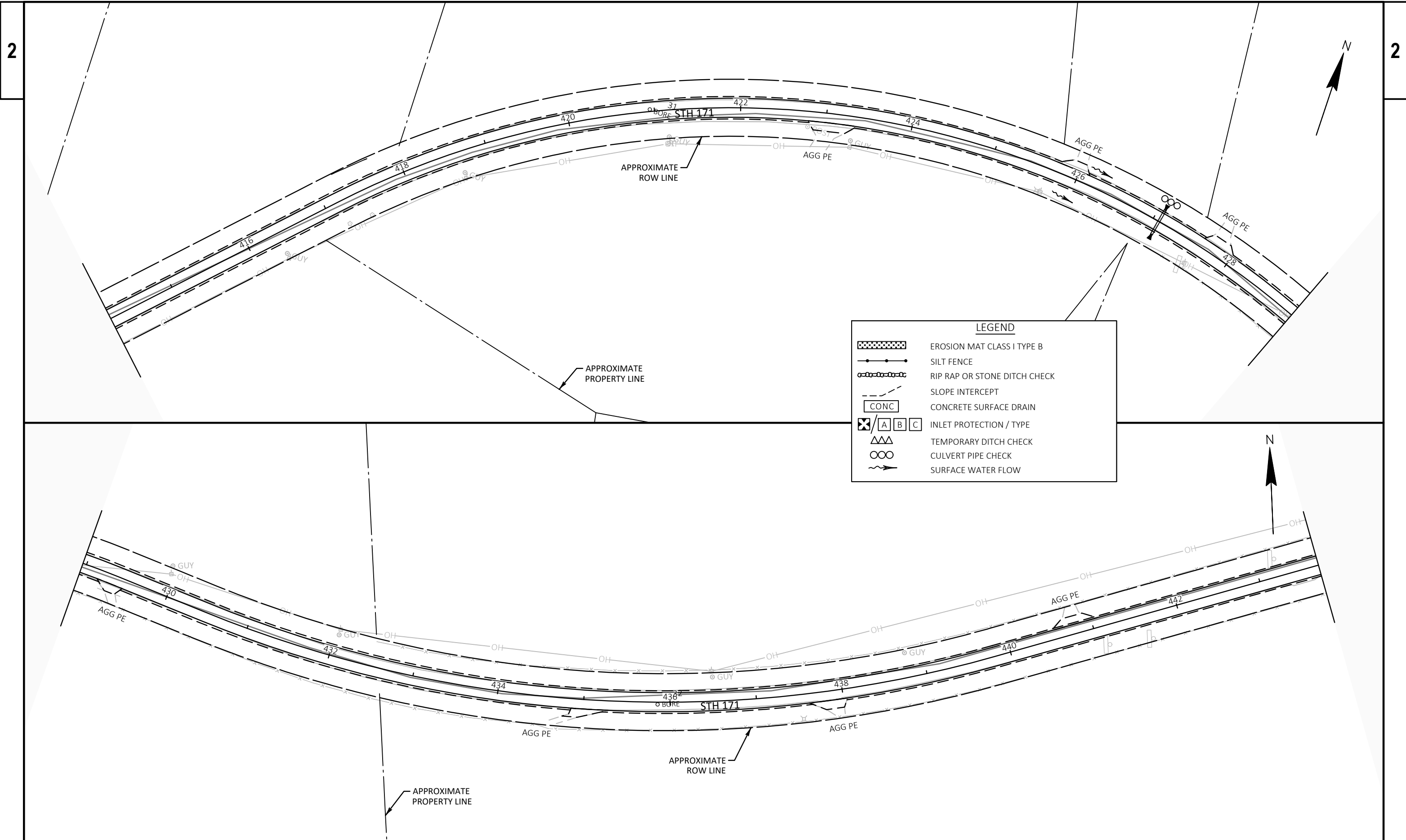


PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	EROSION CONTROL	SHEET E
------------------------	--------------	------------------	-----------------	----------------



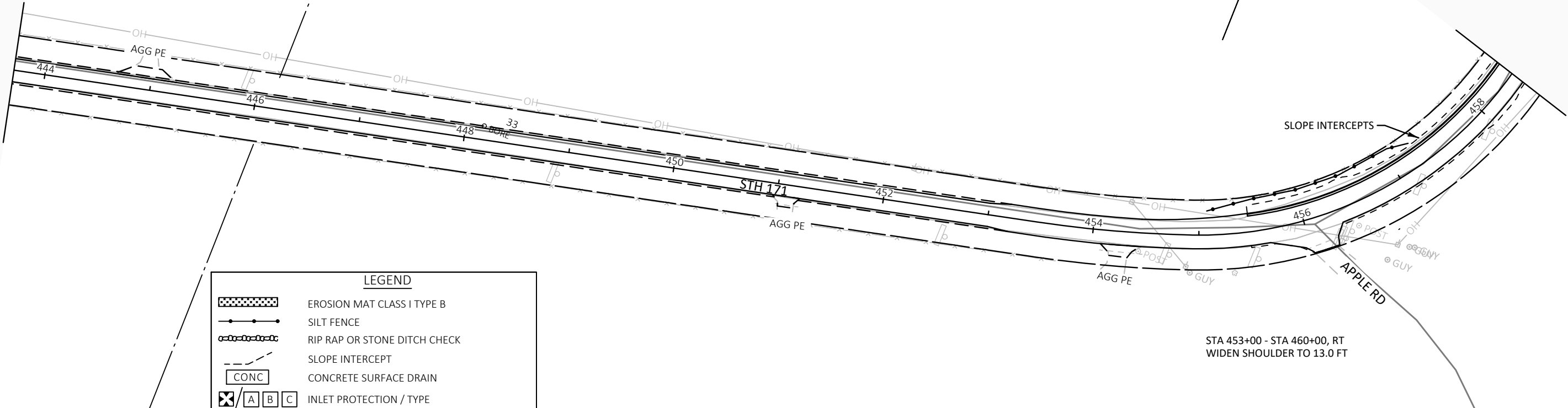
LEGEND

	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW



LEGEND

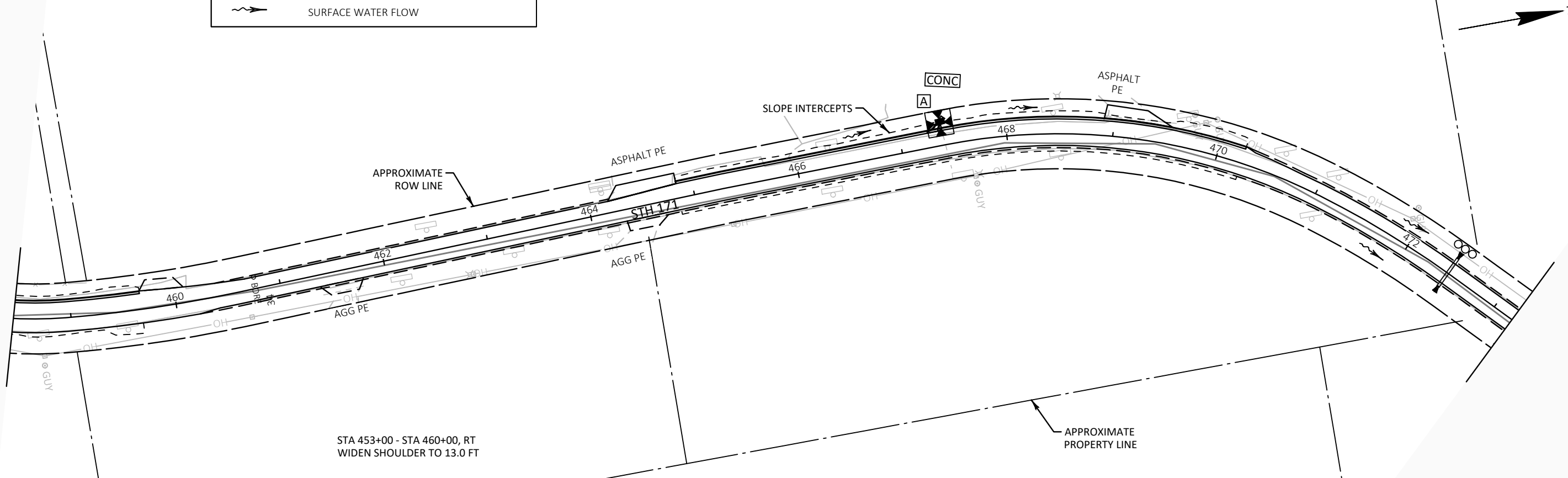
	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW



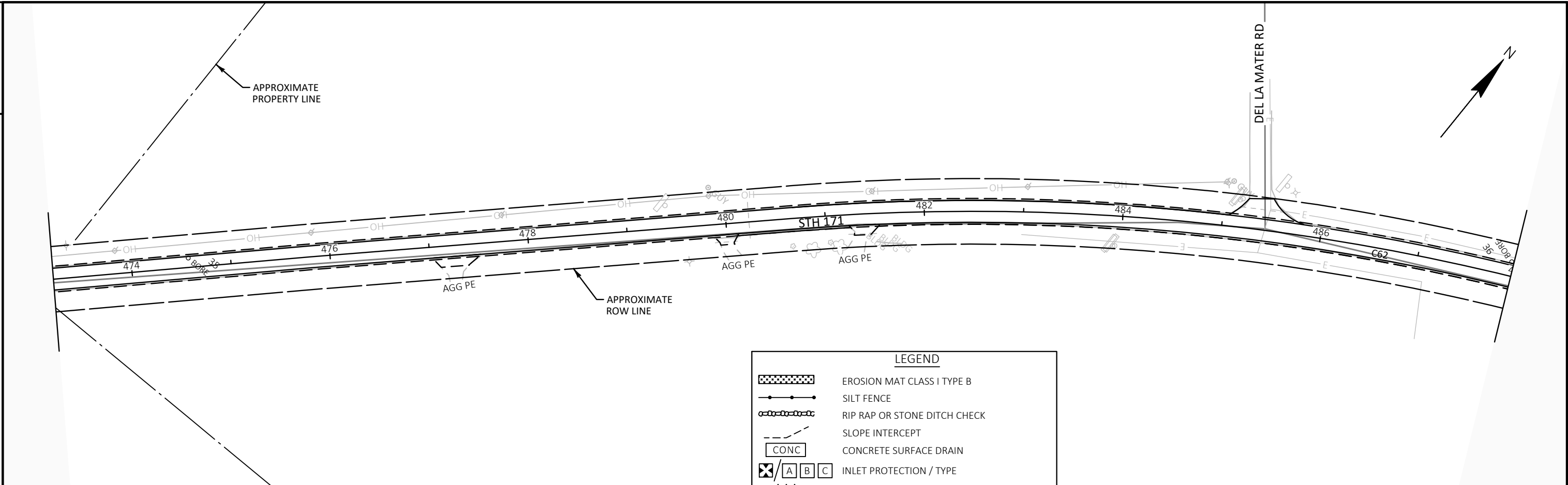
LEGEND

	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW

STA 453+00 - STA 460+00, RT
WIDEN SHOULDER TO 13.0 FT

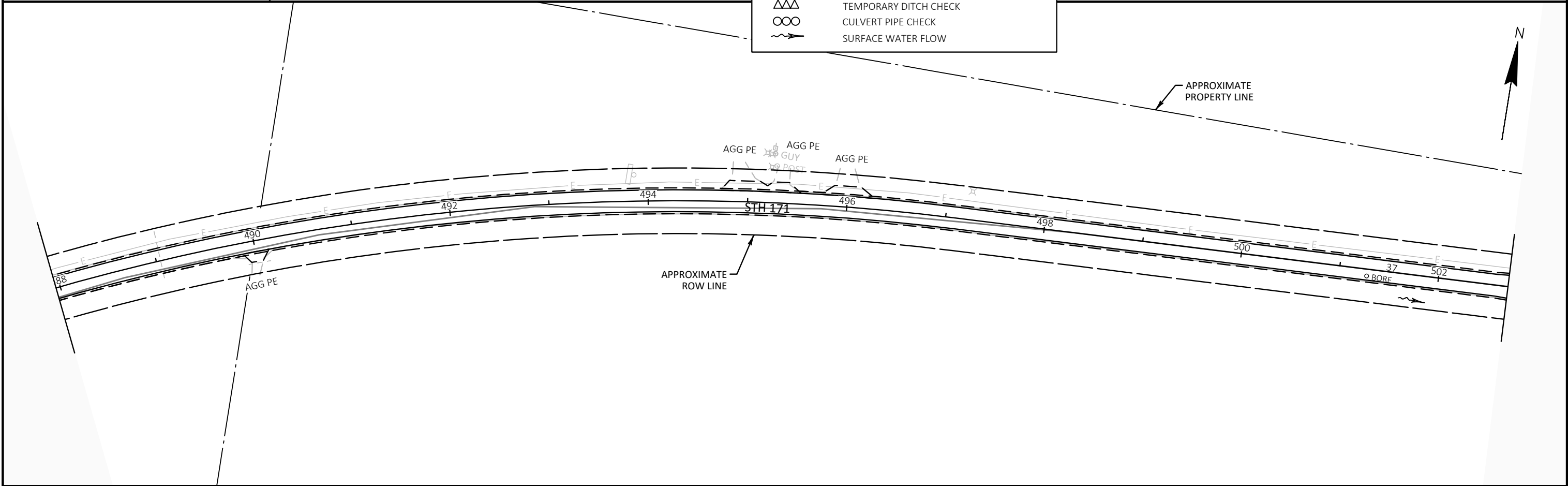


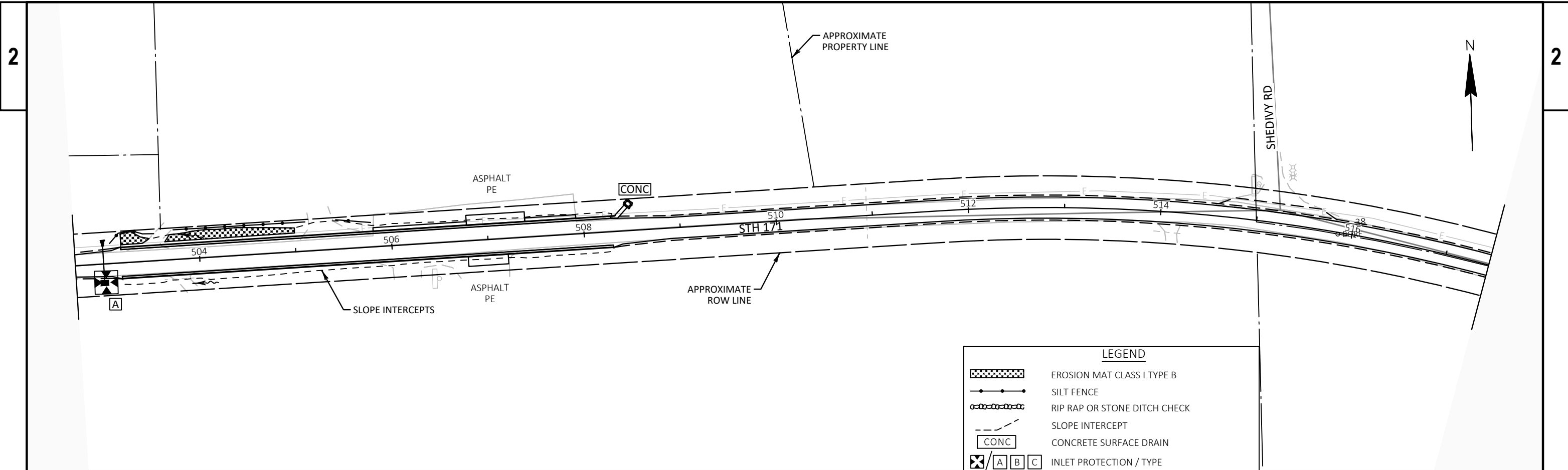
STA 453+00 - STA 460+00, RT
WIDEN SHOULDER TO 13.0 FT



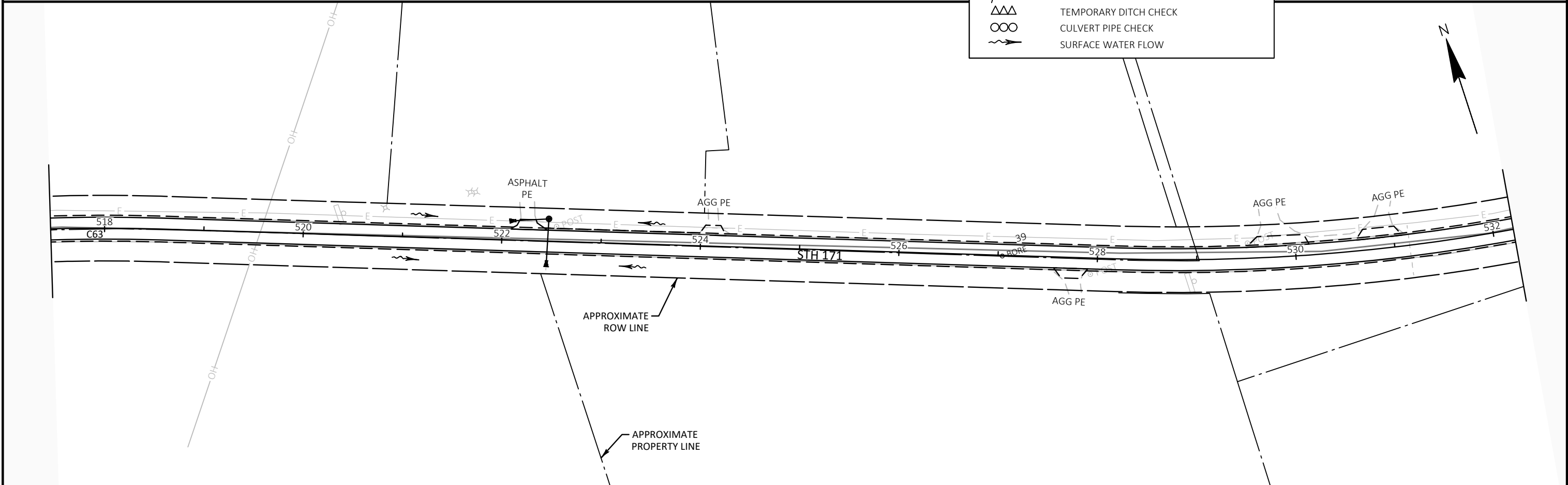
LEGEND

	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW

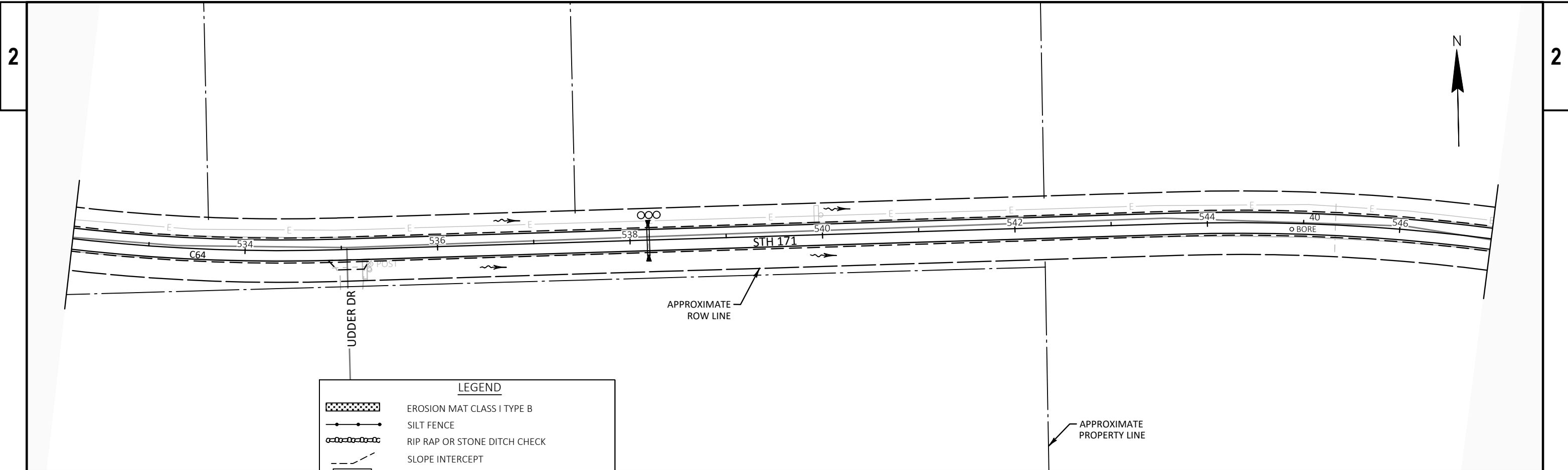




LEGEND	
	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW

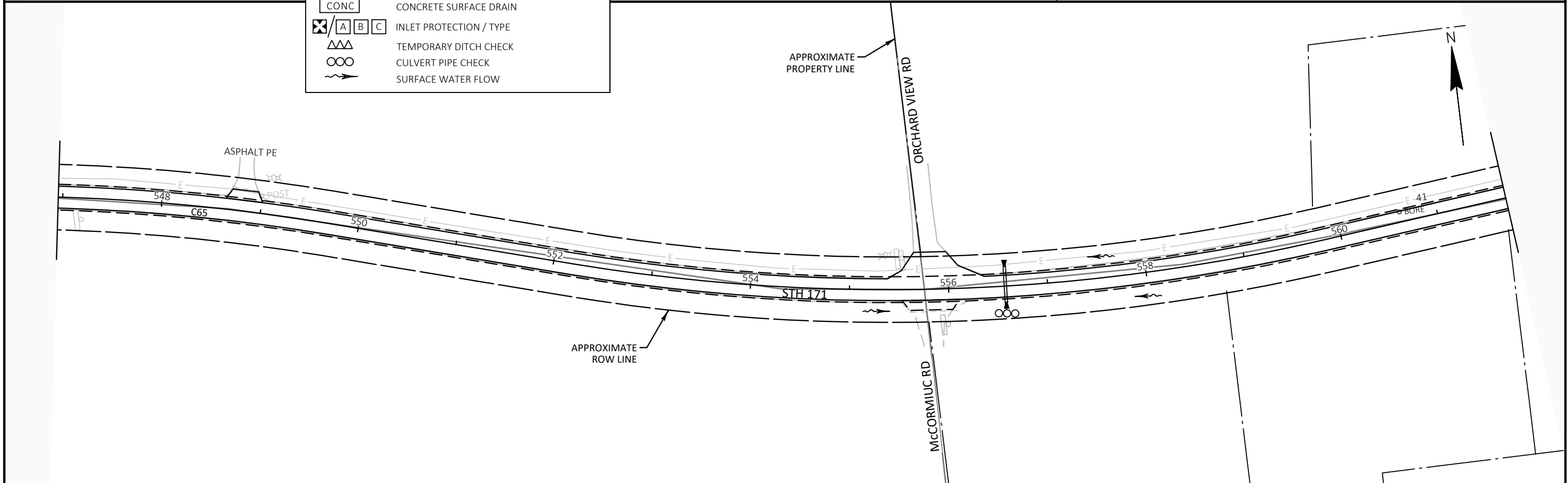


PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	EROSION CONTROL	SHEET	E
------------------------	--------------	------------------	-----------------	-------	---

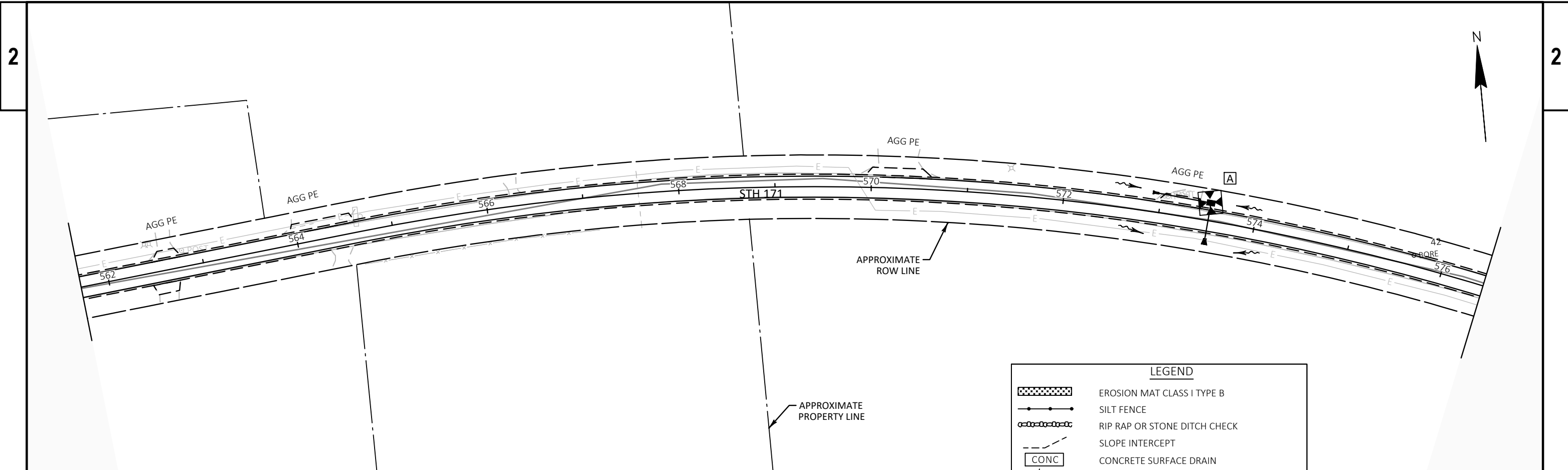


LEGEND

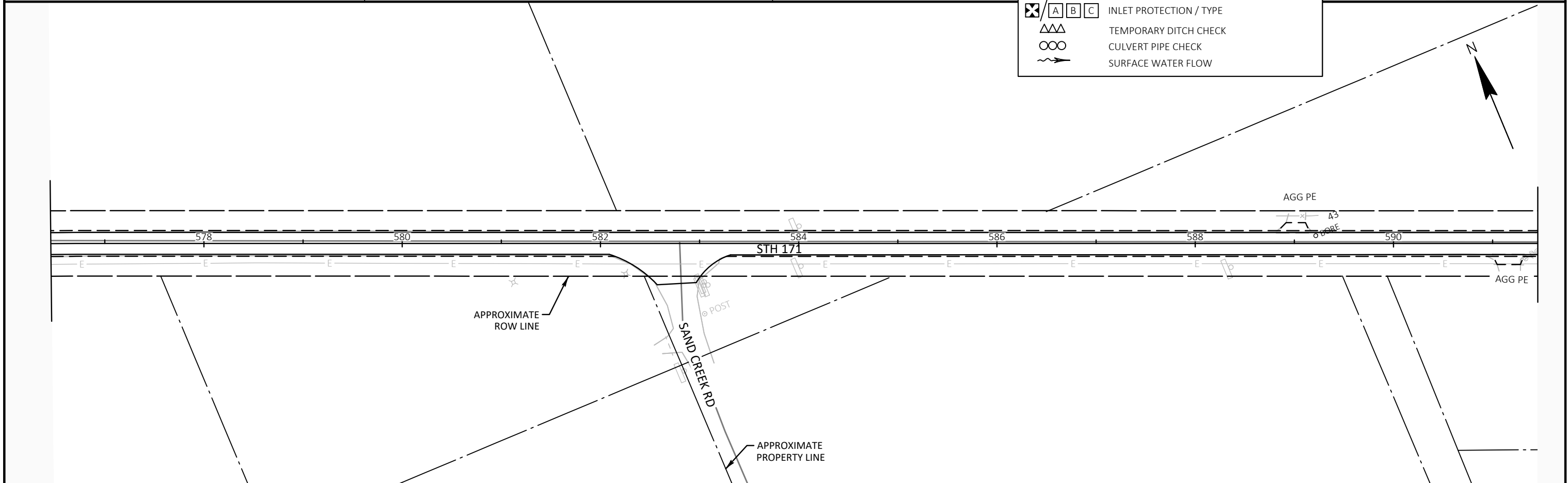
	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW



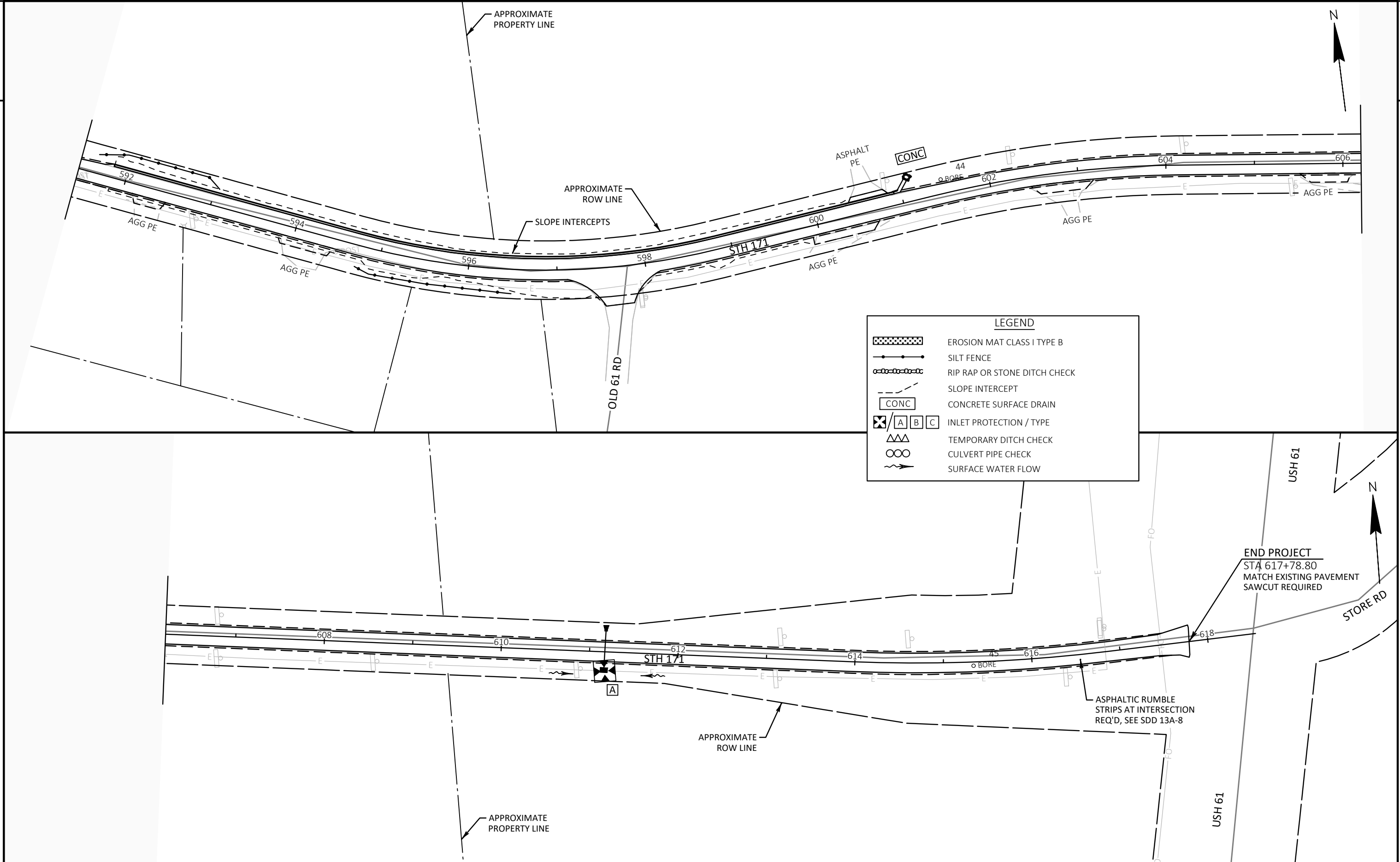
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	EROSION CONTROL	SHEET E
------------------------	--------------	------------------	-----------------	----------------



LEGEND	
	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW



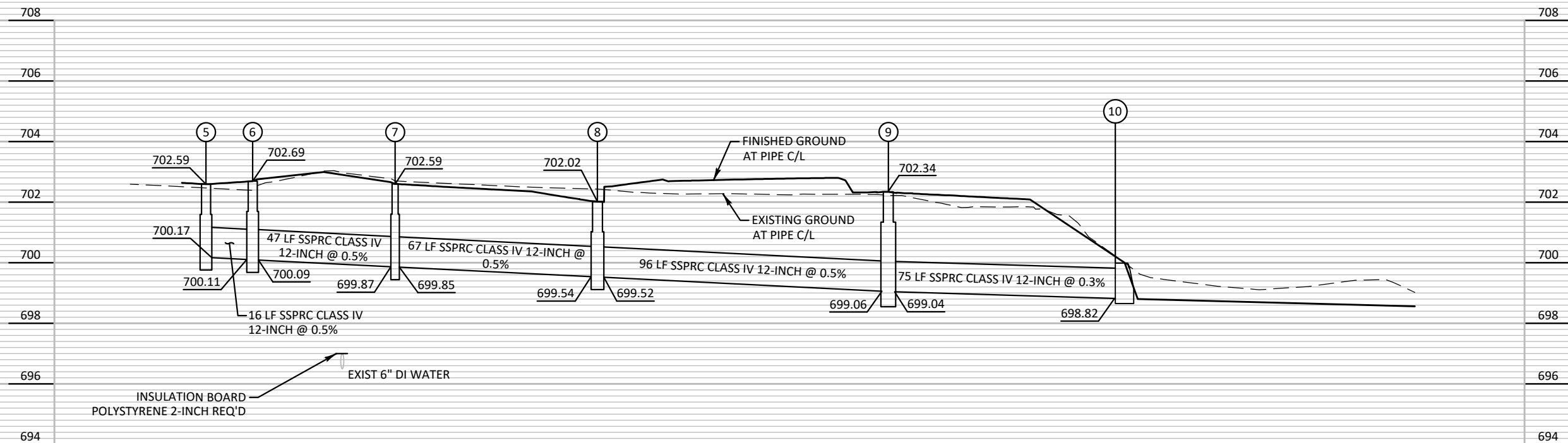
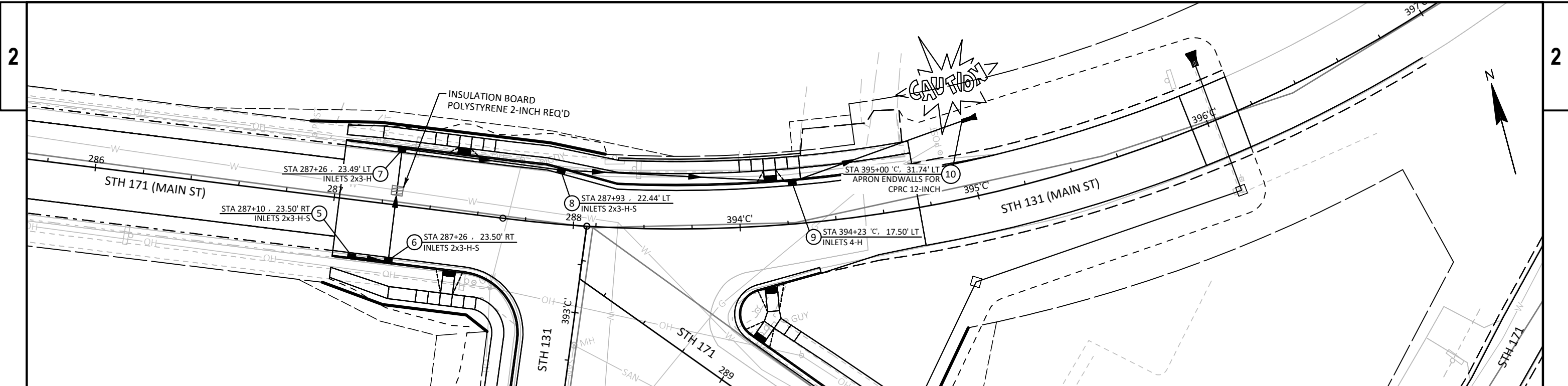
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	EROSION CONTROL	SHEET	E
------------------------	--------------	------------------	-----------------	-------	----------

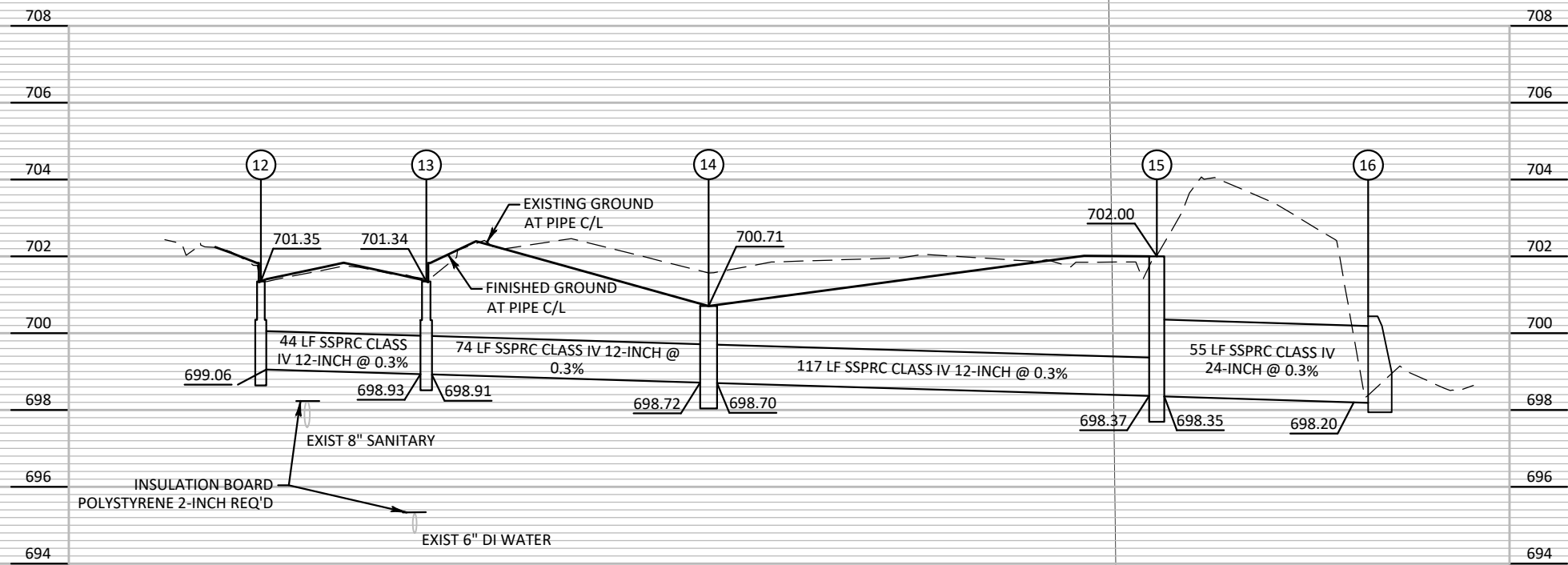
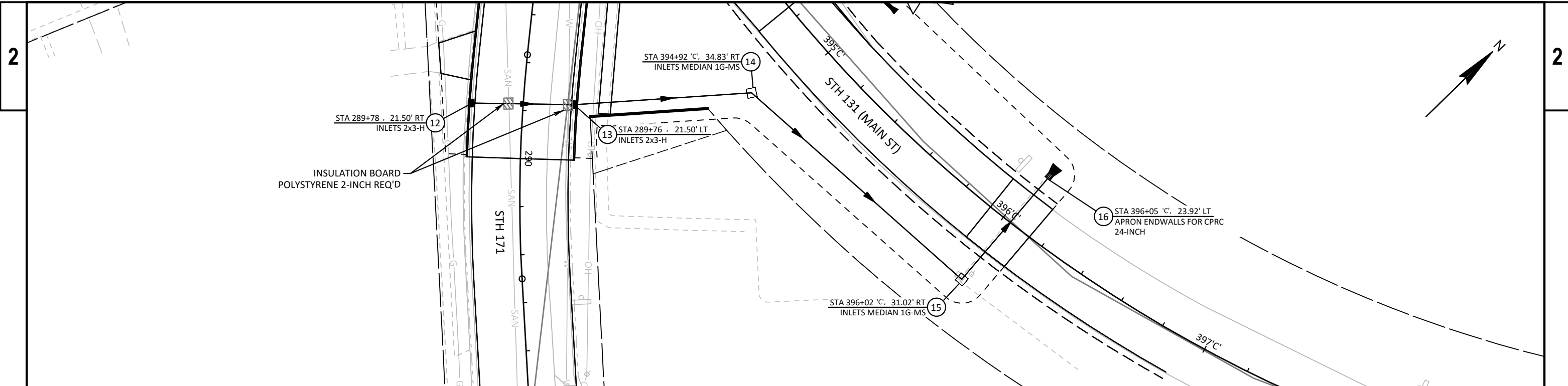


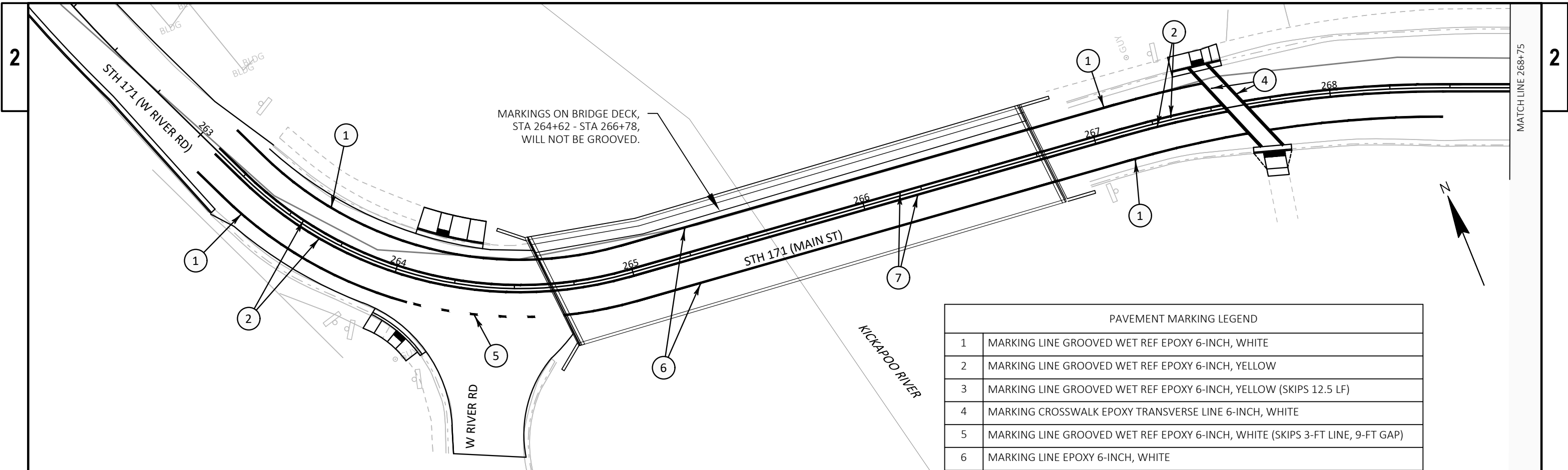
LEGEND

	EROSION MAT CLASS I TYPE B
	SILT FENCE
	RIP RAP OR STONE DITCH CHECK
	SLOPE INTERCEPT
	CONCRETE SURFACE DRAIN
	INLET PROTECTION / TYPE
	TEMPORARY DITCH CHECK
	CULVERT PIPE CHECK
	SURFACE WATER FLOW

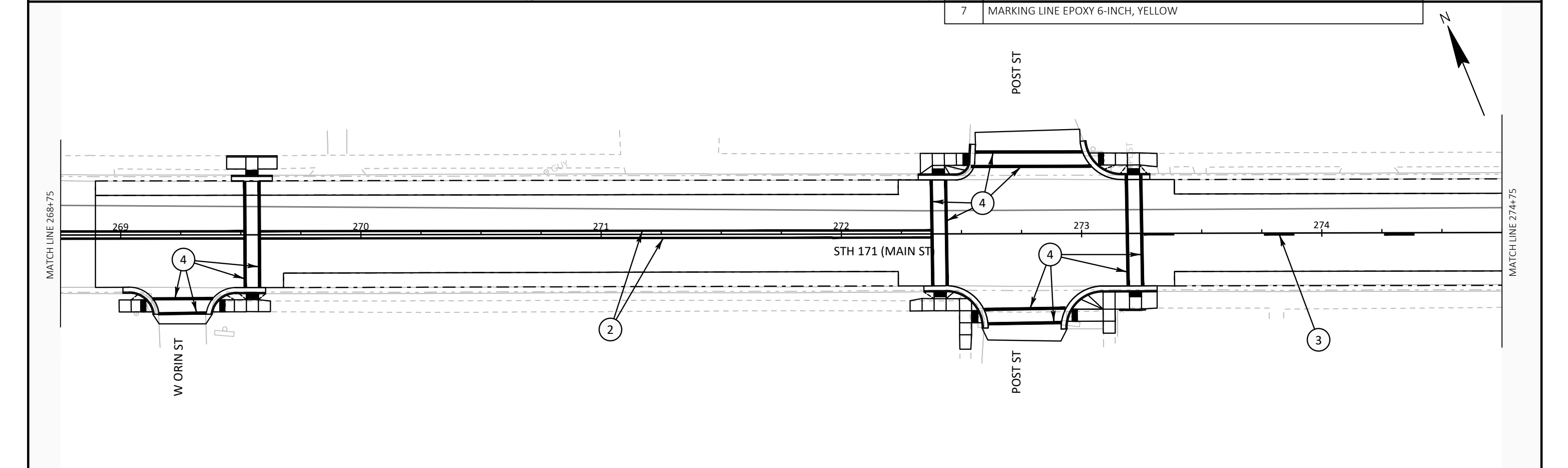
END PROJECT
 STA 617+78.80
 MATCH EXISTING PAVEMENT
 SAWCUT REQUIRED

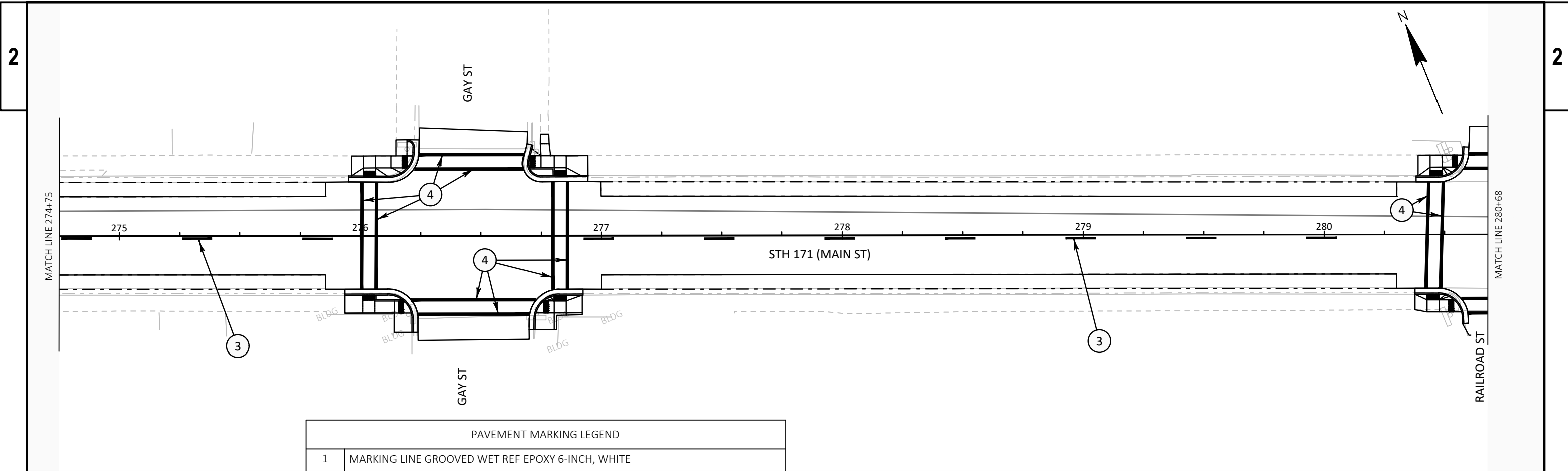




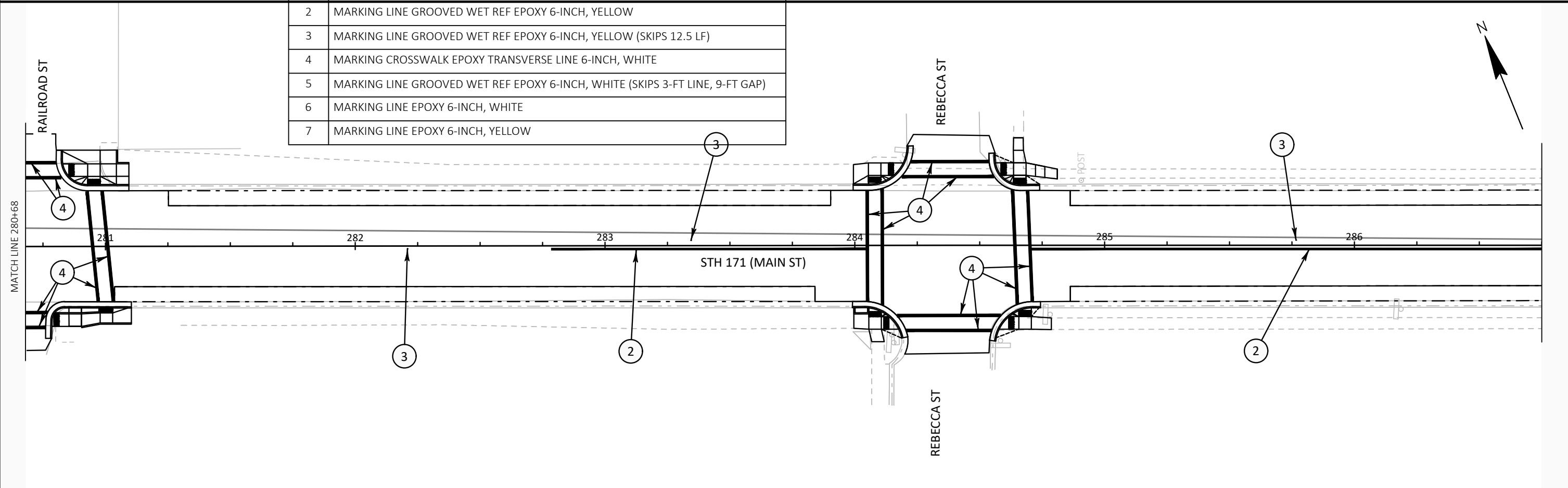


PAVEMENT MARKING LEGEND	
1	MARKING LINE GROOVED WET REF EPOXY 6-INCH, WHITE
2	MARKING LINE GROOVED WET REF EPOXY 6-INCH, YELLOW
3	MARKING LINE GROOVED WET REF EPOXY 6-INCH, YELLOW (SKIPS 12.5 LF)
4	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH, WHITE
5	MARKING LINE GROOVED WET REF EPOXY 6-INCH, WHITE (SKIPS 3-FT LINE, 9-FT GAP)
6	MARKING LINE EPOXY 6-INCH, WHITE
7	MARKING LINE EPOXY 6-INCH, YELLOW

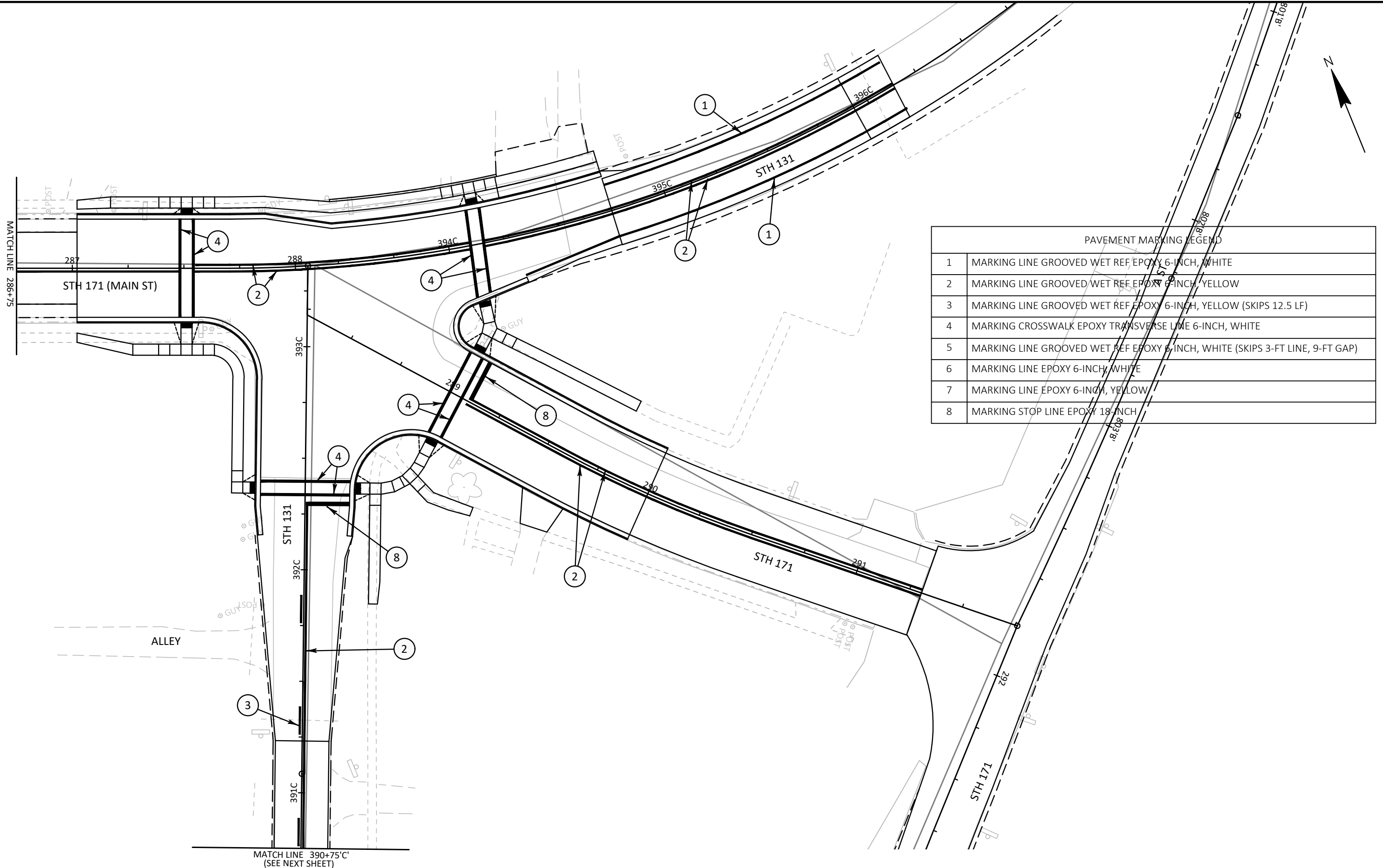




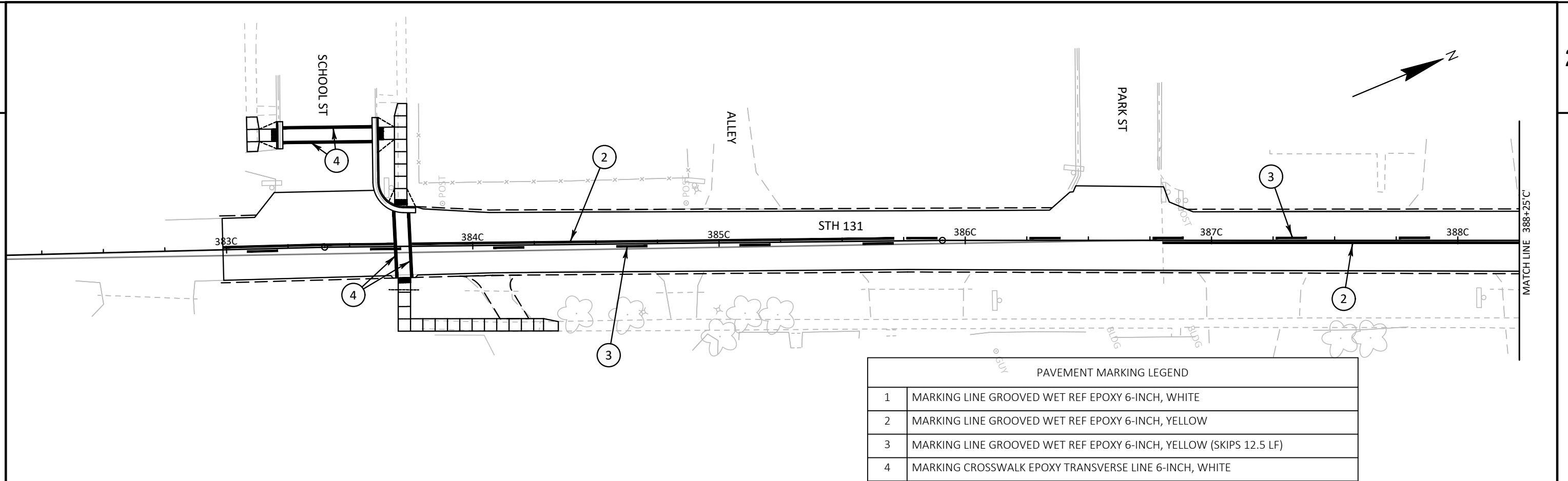
PAVEMENT MARKING LEGEND	
1	MARKING LINE GROOVED WET REF EPOXY 6-INCH, WHITE
2	MARKING LINE GROOVED WET REF EPOXY 6-INCH, YELLOW
3	MARKING LINE GROOVED WET REF EPOXY 6-INCH, YELLOW (SKIPS 12.5 LF)
4	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH, WHITE
5	MARKING LINE GROOVED WET REF EPOXY 6-INCH, WHITE (SKIPS 3-FT LINE, 9-FT GAP)
6	MARKING LINE EPOXY 6-INCH, WHITE
7	MARKING LINE EPOXY 6-INCH, YELLOW



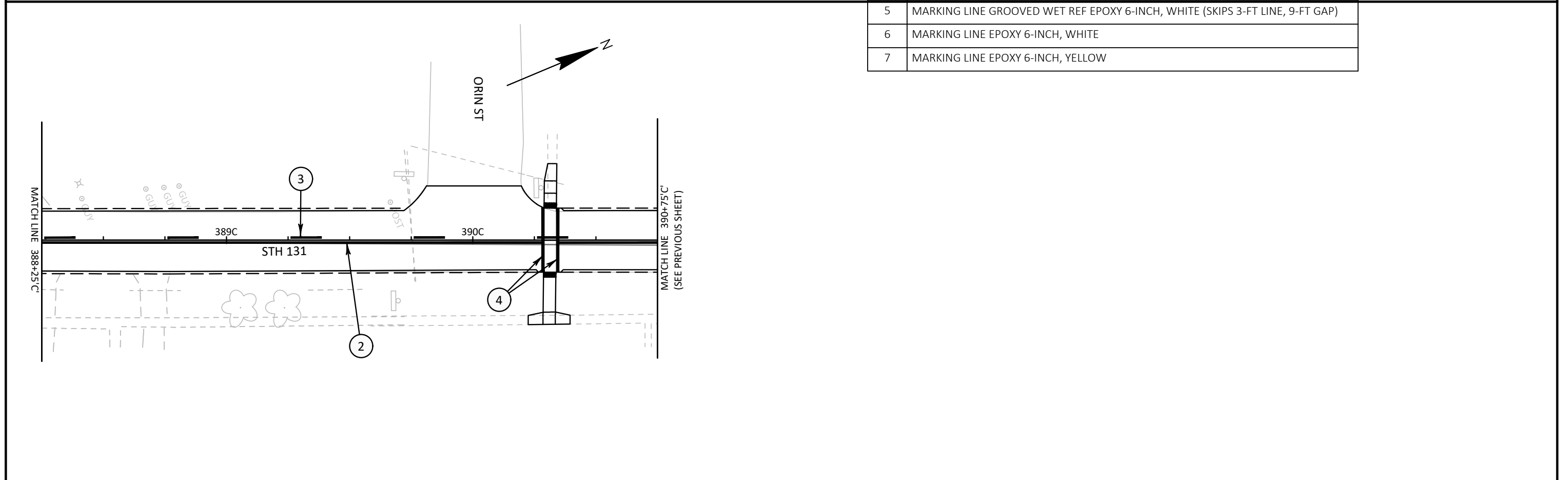
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PAVEMENT MARKING PLAN	SHEET	E
------------------------	--------------	------------------	-----------------------	-------	----------



PAVEMENT MARKING LEGEND	
1	MARKING LINE GROOVED WET REF EPOXY 6-INCH, WHITE
2	MARKING LINE GROOVED WET REF EPOXY 6-INCH, YELLOW
3	MARKING LINE GROOVED WET REF EPOXY 6-INCH, YELLOW (SKIPS 12.5 LF)
4	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH, WHITE
5	MARKING LINE GROOVED WET REF EPOXY 6-INCH, WHITE (SKIPS 3-FT LINE, 9-FT GAP)
6	MARKING LINE EPOXY 6-INCH, WHITE
7	MARKING LINE EPOXY 6-INCH, YELLOW
8	MARKING STOP LINE EPOXY 18-INCH

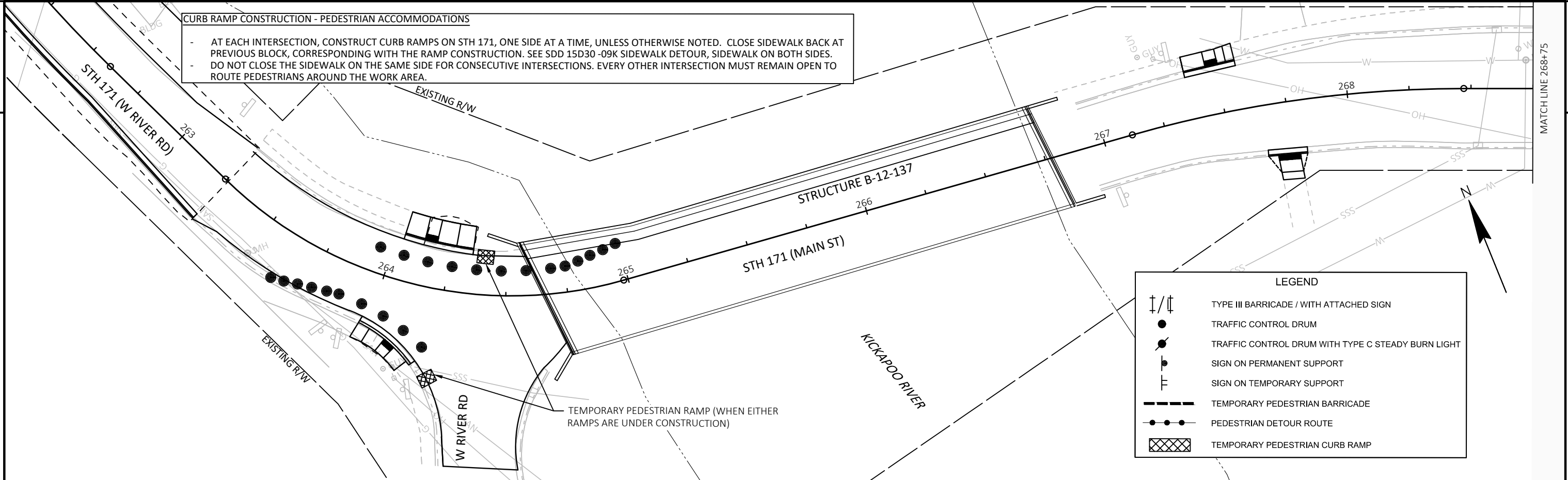


PAVEMENT MARKING LEGEND	
1	MARKING LINE GROOVED WET REF EPOXY 6-INCH, WHITE
2	MARKING LINE GROOVED WET REF EPOXY 6-INCH, YELLOW
3	MARKING LINE GROOVED WET REF EPOXY 6-INCH, YELLOW (SKIPS 12.5 LF)
4	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH, WHITE
5	MARKING LINE GROOVED WET REF EPOXY 6-INCH, WHITE (SKIPS 3-FT LINE, 9-FT GAP)
6	MARKING LINE EPOXY 6-INCH, WHITE
7	MARKING LINE EPOXY 6-INCH, YELLOW



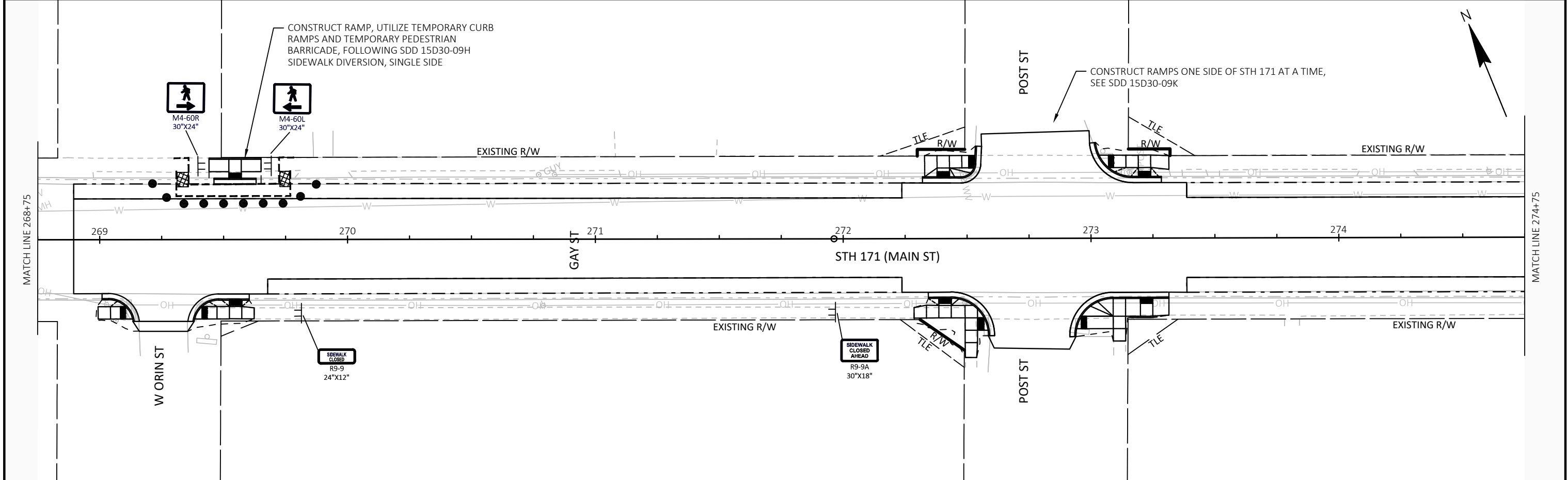
CURB RAMP CONSTRUCTION - PEDESTRIAN ACCOMMODATIONS

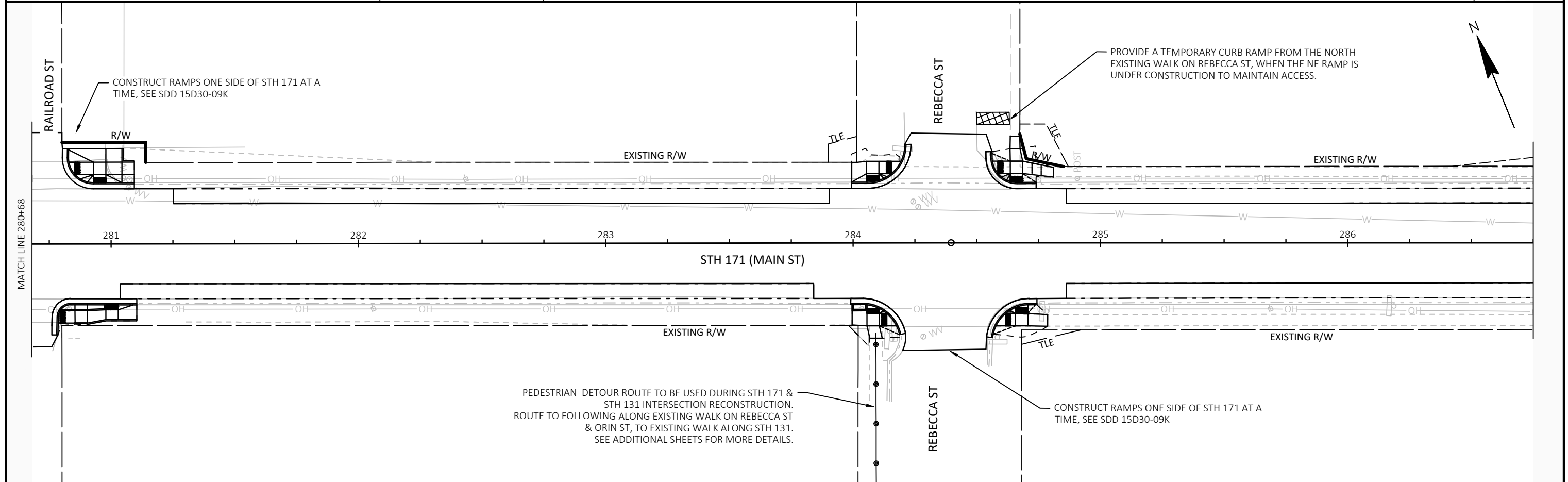
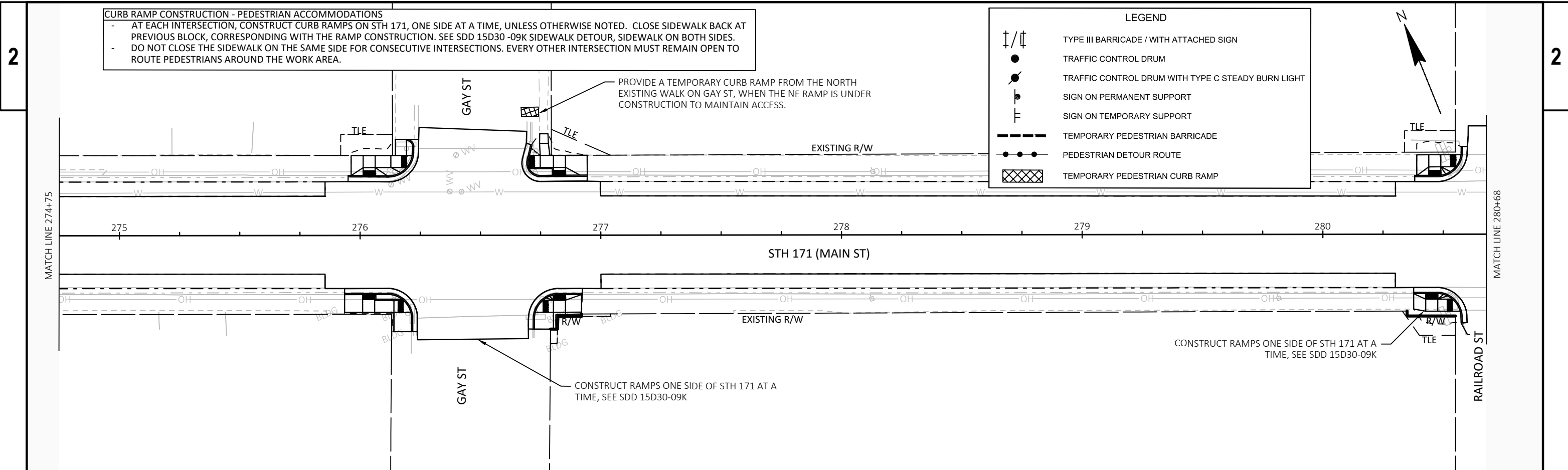
- AT EACH INTERSECTION, CONSTRUCT CURB RAMP ON STH 171, ONE SIDE AT A TIME, UNLESS OTHERWISE NOTED. CLOSE SIDEWALK BACK AT PREVIOUS BLOCK, CORRESPONDING WITH THE RAMP CONSTRUCTION. SEE SDD 15D30-09K SIDEWALK DETOUR, SIDEWALK ON BOTH SIDES. DO NOT CLOSE THE SIDEWALK ON THE SAME SIDE FOR CONSECUTIVE INTERSECTIONS. EVERY OTHER INTERSECTION MUST REMAIN OPEN TO ROUTE PEDESTRIANS AROUND THE WORK AREA.



LEGEND

- TYPE III BARRICADE / WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- TEMPORARY PEDESTRIAN BARRICADE
- PEDESTRIAN DETOUR ROUTE
- TEMPORARY PEDESTRIAN CURB RAMP





PROJECT NO: 5790-02-72

HWY: STH 171

COUNTY: CRAWFORD

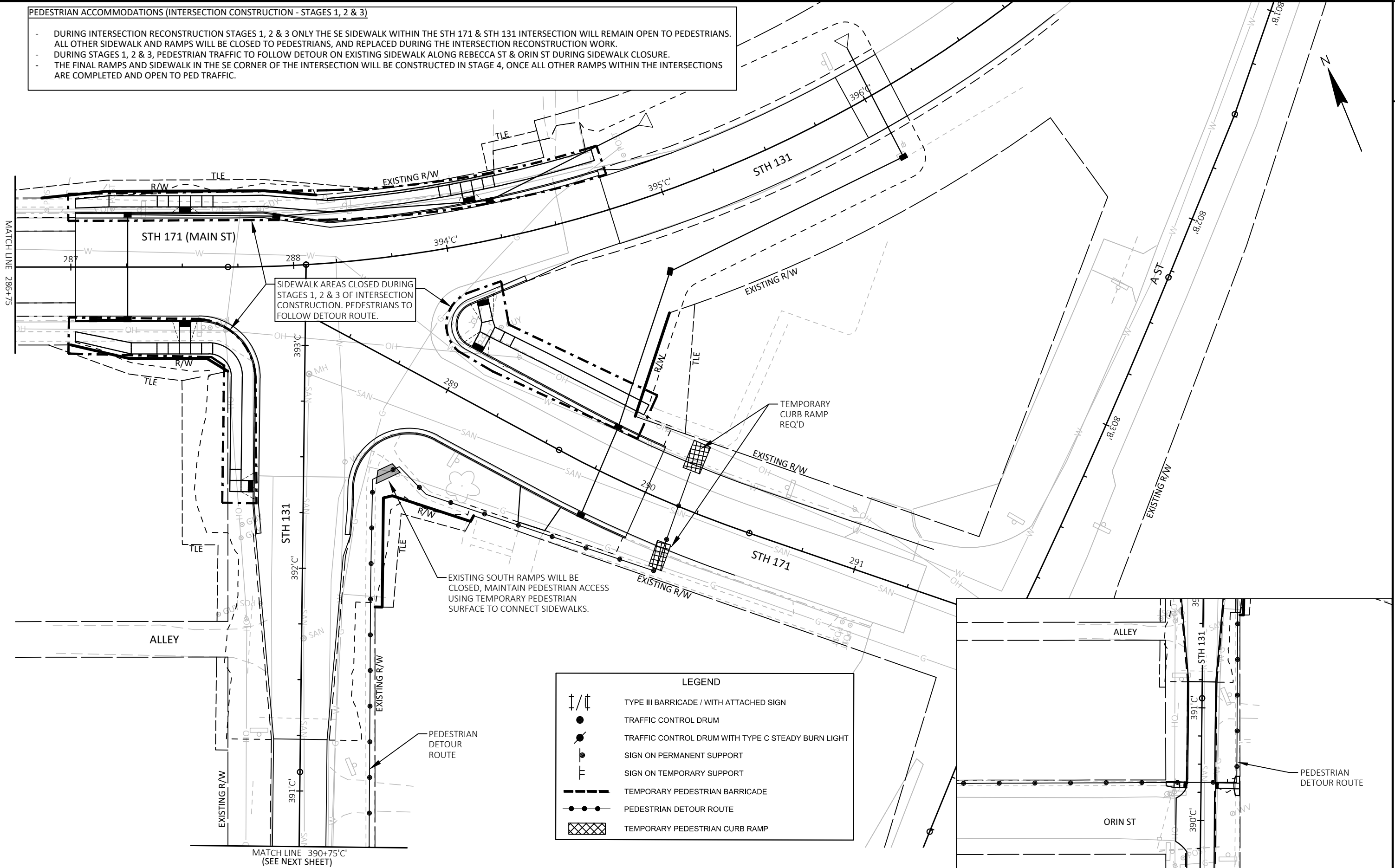
PEDESTRIAN ACCOMMODATION

SHEET

E

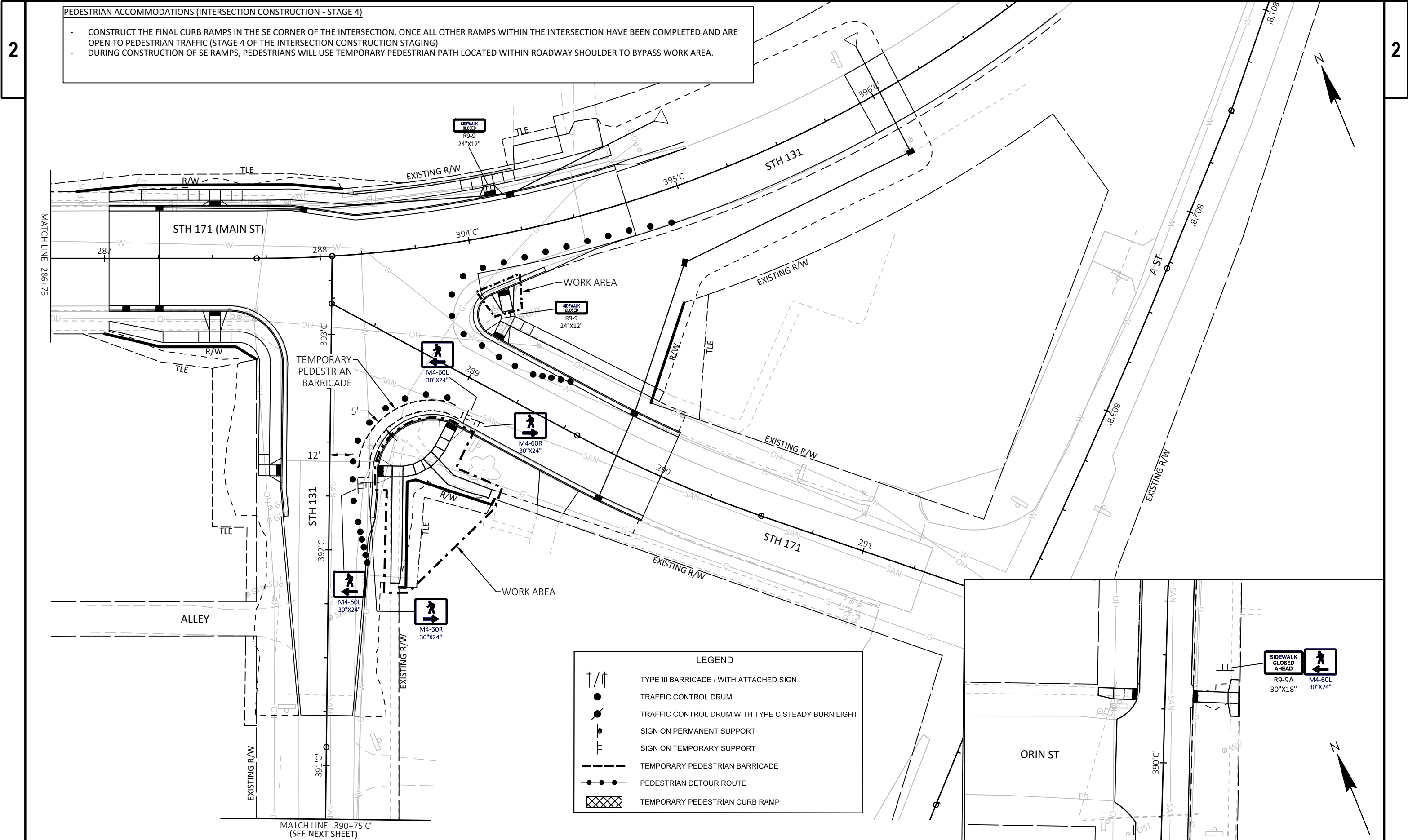
PEDESTRIAN ACCOMMODATIONS (INTERSECTION CONSTRUCTION - STAGES 1, 2 & 3)

- DURING INTERSECTION RECONSTRUCTION STAGES 1, 2 & 3 ONLY THE SE SIDEWALK WITHIN THE STH 171 & STH 131 INTERSECTION WILL REMAIN OPEN TO PEDESTRIANS. ALL OTHER SIDEWALK AND RAMPS WILL BE CLOSED TO PEDESTRIANS, AND REPLACED DURING THE INTERSECTION RECONSTRUCTION WORK.
- DURING STAGES 1, 2 & 3, PEDESTRIAN TRAFFIC TO FOLLOW DETOUR ON EXISTING SIDEWALK ALONG REBECCA ST & ORIN ST DURING SIDEWALK CLOSURE.
- THE FINAL RAMPS AND SIDEWALK IN THE SE CORNER OF THE INTERSECTION WILL BE CONSTRUCTED IN STAGE 4, ONCE ALL OTHER RAMPS WITHIN THE INTERSECTIONS ARE COMPLETED AND OPEN TO PED TRAFFIC.



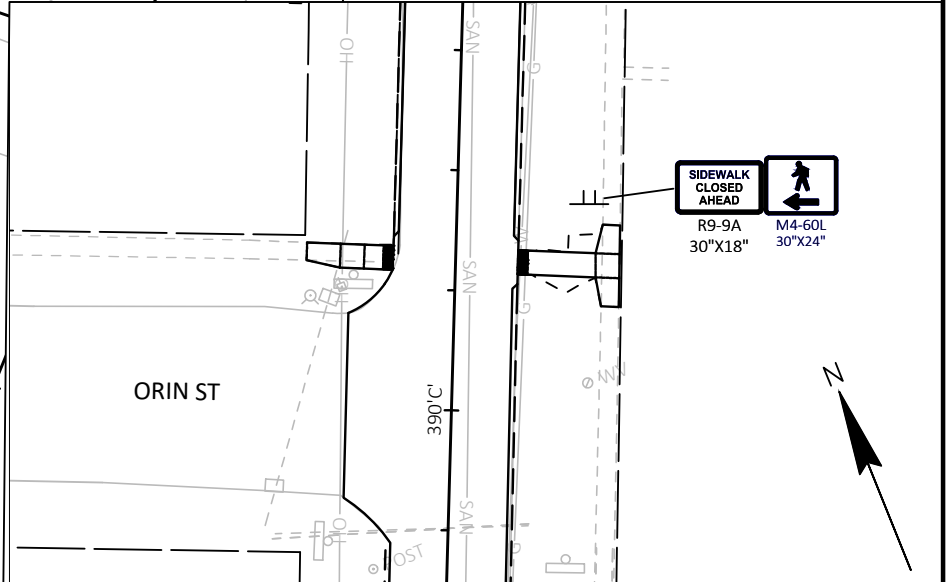
PEDESTRIAN ACCOMMODATIONS (INTERSECTION CONSTRUCTION - STAGE 4)

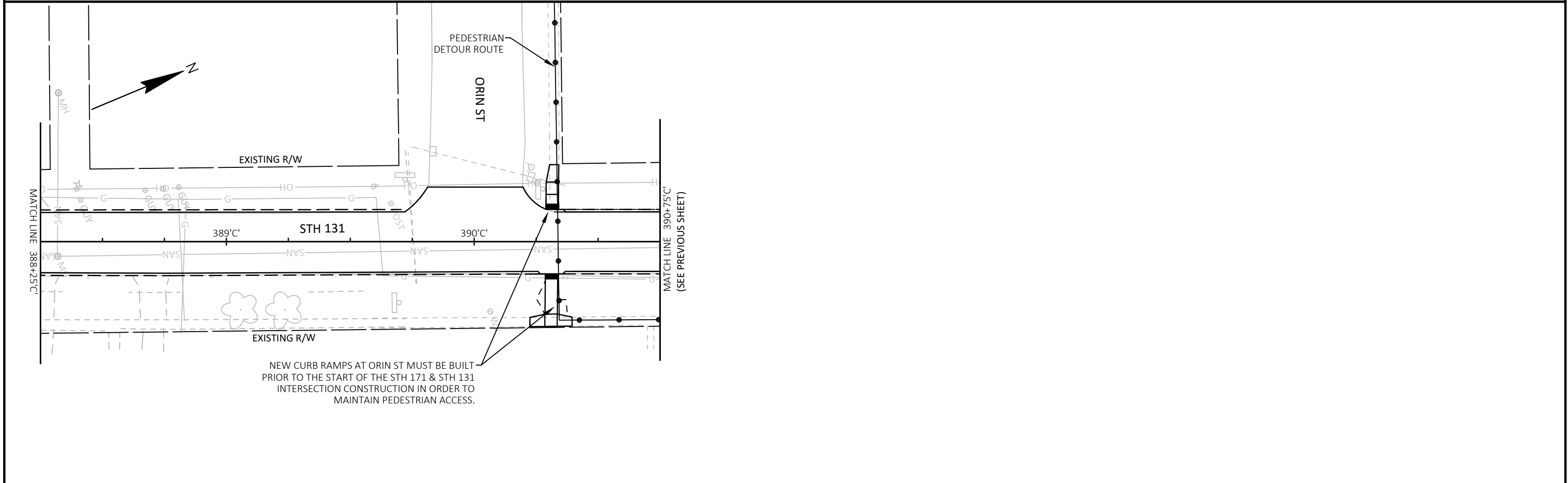
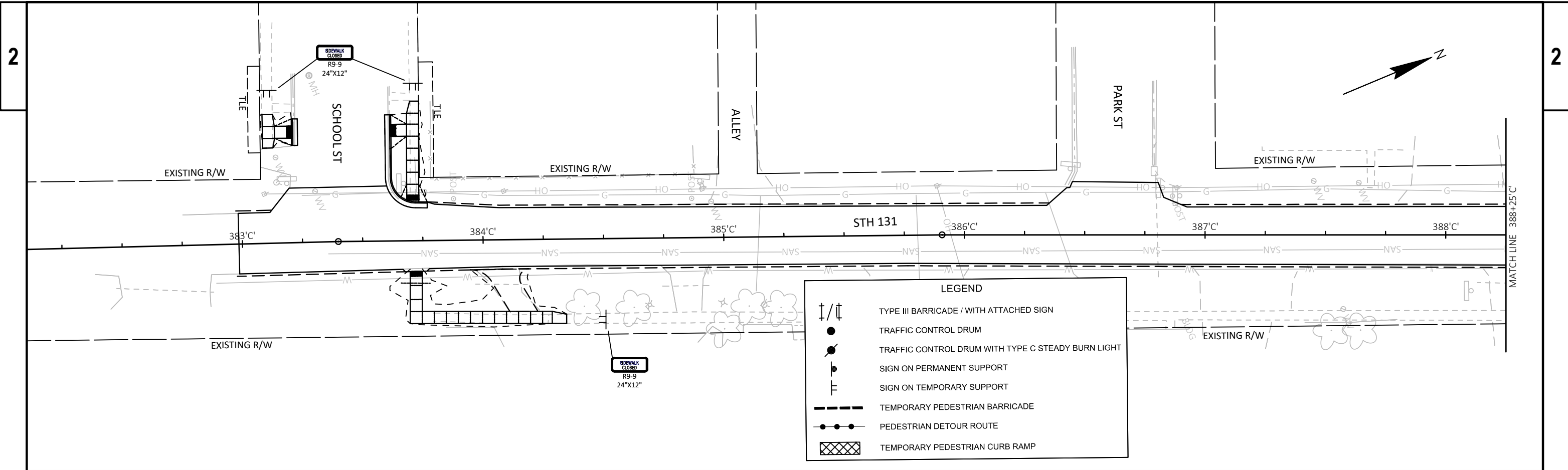
- CONSTRUCT THE FINAL CURB RAMP IN THE SE CORNER OF THE INTERSECTION, ONCE ALL OTHER RAMPS WITHIN THE INTERSECTION HAVE BEEN COMPLETED AND ARE OPEN TO PEDESTRIAN TRAFFIC (STAGE 4 OF THE INTERSECTION CONSTRUCTION STAGING)
- DURING CONSTRUCTION OF SE RAMPS, PEDESTRIANS WILL USE TEMPORARY PEDESTRIAN PATH LOCATED WITHIN ROADWAY SHOULDER TO BYPASS WORK AREA.



LEGEND

	TYPE III BARRICADE / WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	TEMPORARY PEDESTRIAN BARRICADE
	PEDESTRIAN DETOUR ROUTE
	TEMPORARY PEDESTRIAN CURB RAMP





PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PEDESTRIAN ACCOMMODATION	SHEET	E
------------------------	--------------	------------------	--------------------------	-------	---

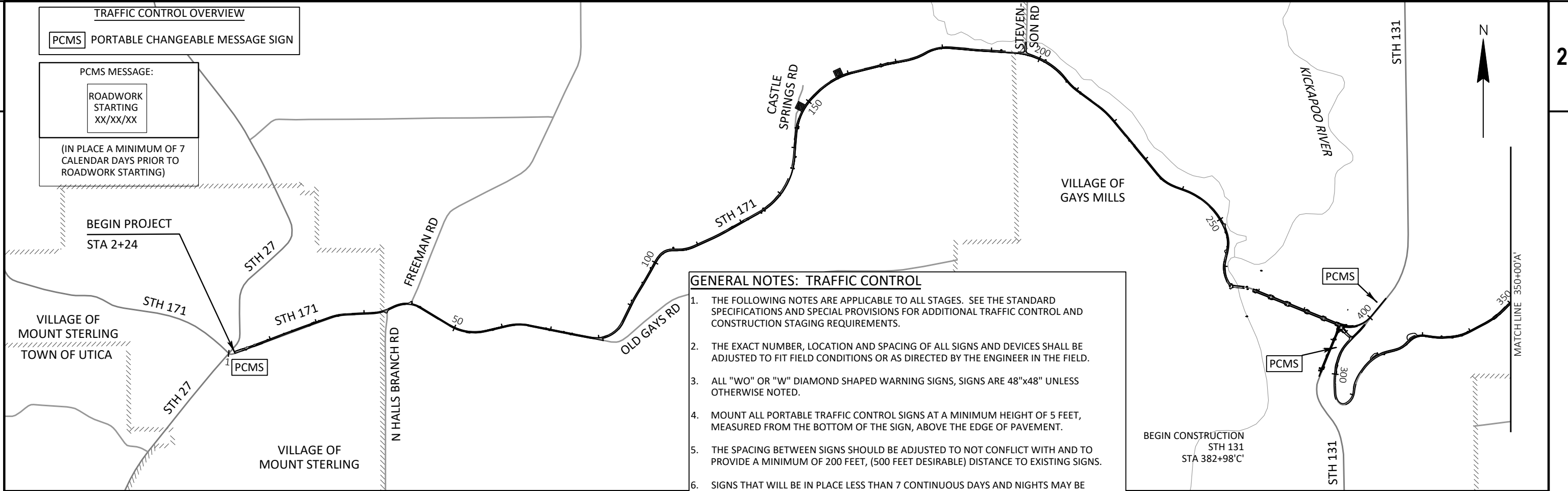
TRAFFIC CONTROL OVERVIEW

PCMS PORTABLE CHANGEABLE MESSAGE SIGN

PCMS MESSAGE:

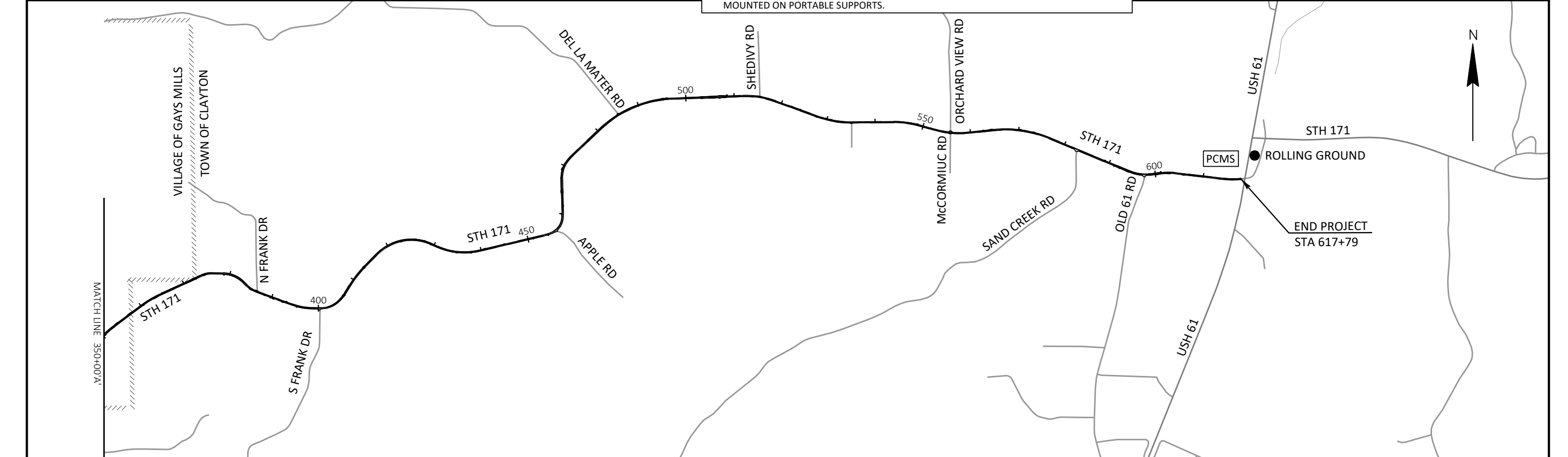
ROADWORK STARTING
XX/XX/XX

(IN PLACE A MINIMUM OF 7 CALENDAR DAYS PRIOR TO ROADWORK STARTING)



- GENERAL NOTES: TRAFFIC CONTROL**
1. THE FOLLOWING NOTES ARE APPLICABLE TO ALL STAGES. SEE THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR ADDITIONAL TRAFFIC CONTROL AND CONSTRUCTION STAGING REQUIREMENTS.
 2. THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER IN THE FIELD.
 3. ALL "WO" OR "W" DIAMOND SHAPED WARNING SIGNS, SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.
 4. MOUNT ALL PORTABLE TRAFFIC CONTROL SIGNS AT A MINIMUM HEIGHT OF 5 FEET, MEASURED FROM THE BOTTOM OF THE SIGN, ABOVE THE EDGE OF PAVEMENT.
 5. THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.
 6. SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

BEGIN CONSTRUCTION
STH 131
STA 382+98'C



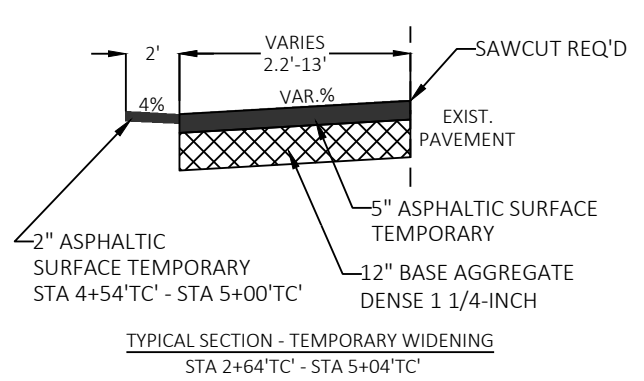
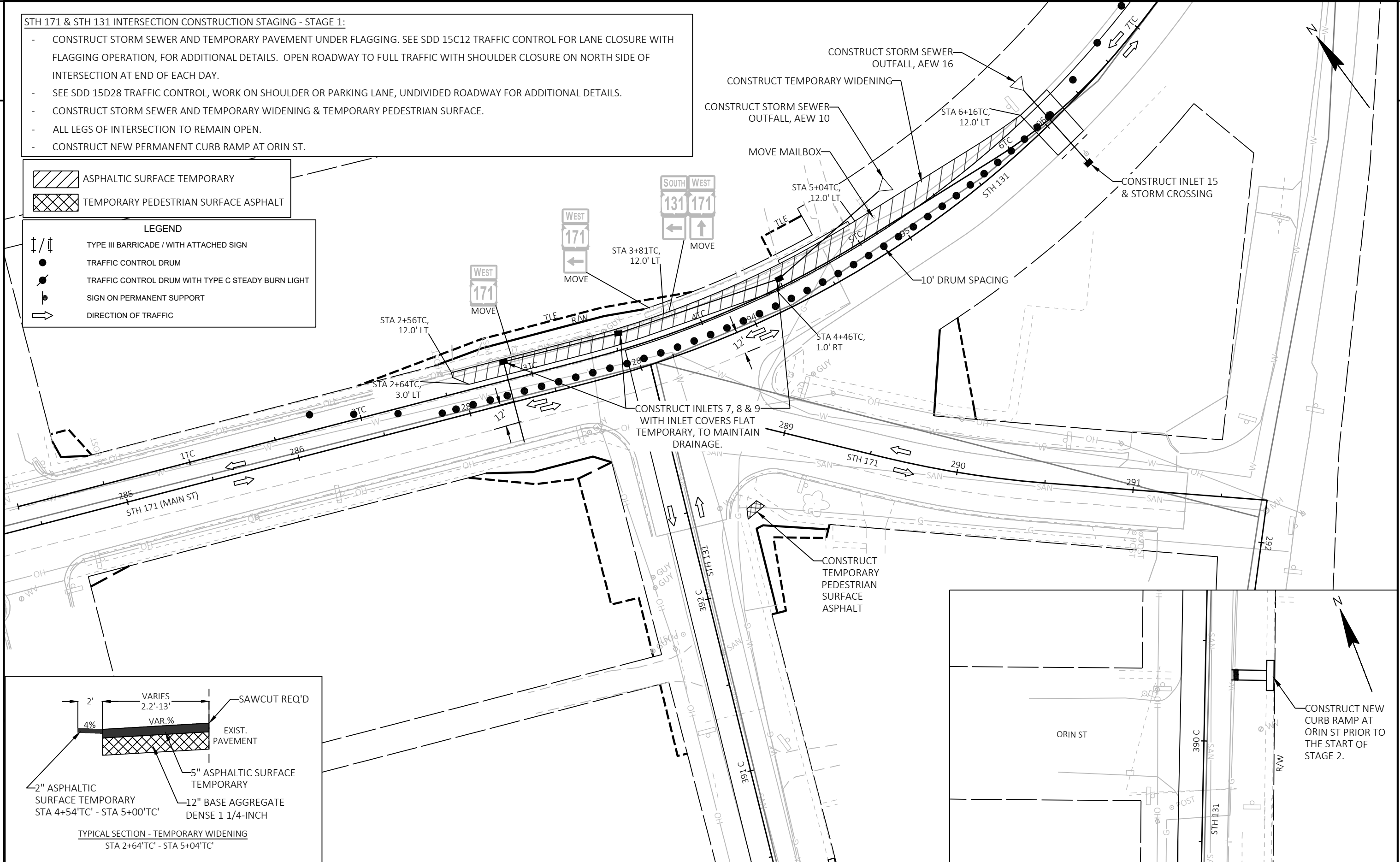
STH 171 & STH 131 INTERSECTION CONSTRUCTION STAGING - STAGE 1:

- CONSTRUCT STORM SEWER AND TEMPORARY PAVEMENT UNDER FLAGGING. SEE SDD 15C12 TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION, FOR ADDITIONAL DETAILS. OPEN ROADWAY TO FULL TRAFFIC WITH SHOULDER CLOSURE ON NORTH SIDE OF INTERSECTION AT END OF EACH DAY.
- SEE SDD 15D28 TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY FOR ADDITIONAL DETAILS.
- CONSTRUCT STORM SEWER AND TEMPORARY WIDENING & TEMPORARY PEDESTRIAN SURFACE.
- ALL LEGS OF INTERSECTION TO REMAIN OPEN.
- CONSTRUCT NEW PERMANENT CURB RAMP AT ORIN ST.

- ASPHALTIC SURFACE TEMPORARY
- TEMPORARY PEDESTRIAN SURFACE ASPHALT

LEGEND

- TYPE III BARRICADE / WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC



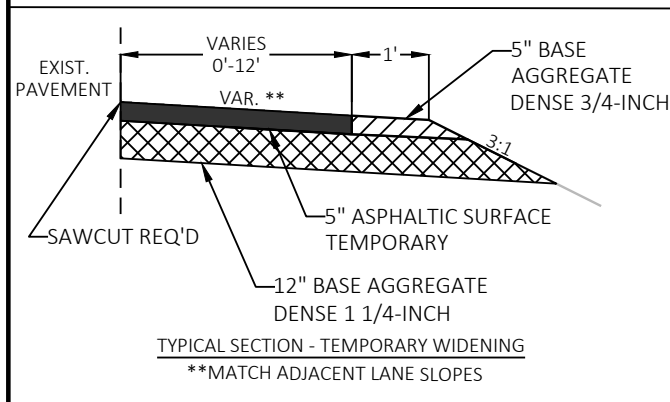
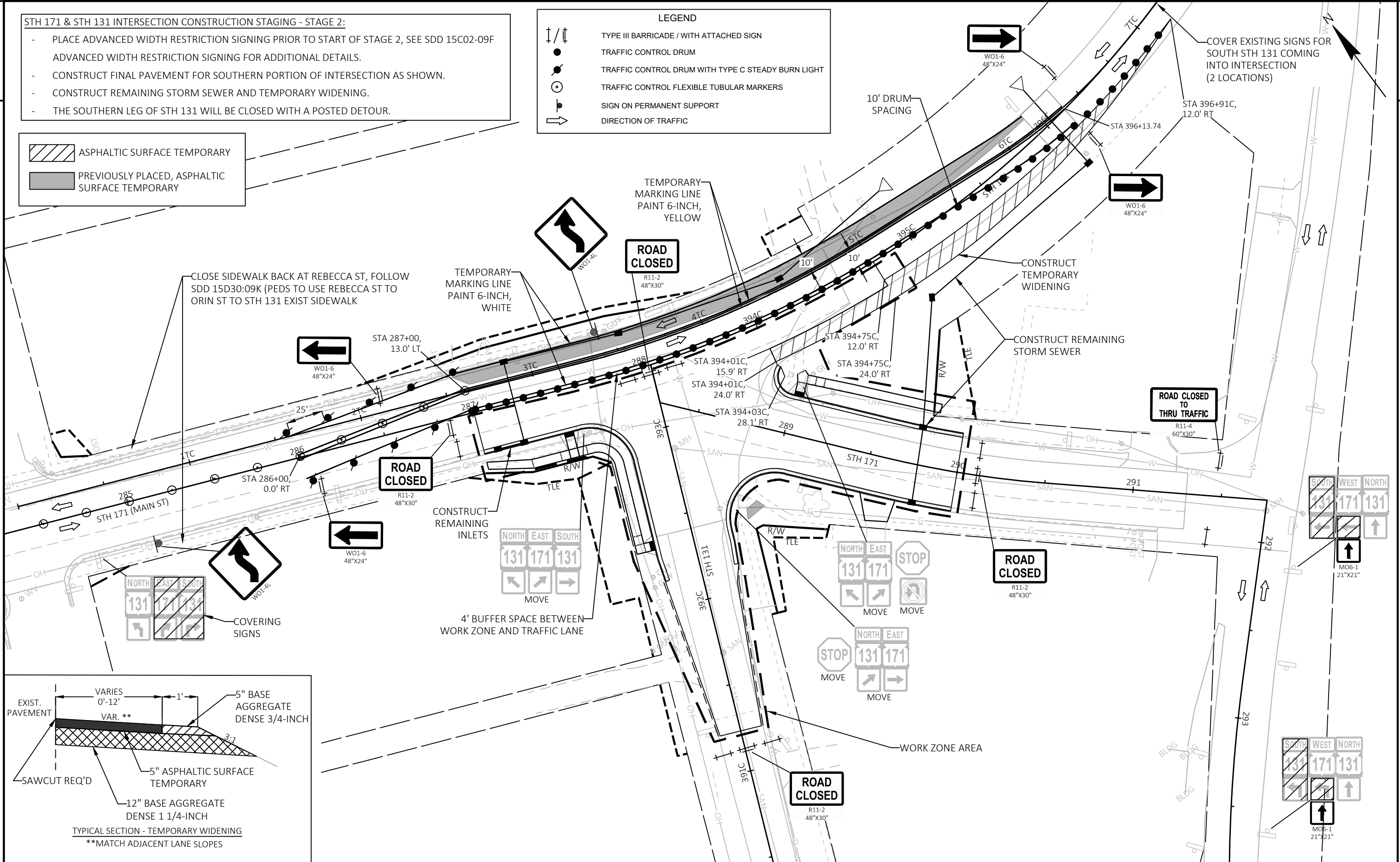
STH 171 & STH 131 INTERSECTION CONSTRUCTION STAGING - STAGE 2:

- PLACE ADVANCED WIDTH RESTRICTION SIGNING PRIOR TO START OF STAGE 2, SEE SDD 15C02-09F ADVANCED WIDTH RESTRICTION SIGNING FOR ADDITIONAL DETAILS.
- CONSTRUCT FINAL PAVEMENT FOR SOUTHERN PORTION OF INTERSECTION AS SHOWN.
- CONSTRUCT REMAINING STORM SEWER AND TEMPORARY WIDENING.
- THE SOUTHERN LEG OF STH 131 WILL BE CLOSED WITH A POSTED DETOUR.

- ASPHALTIC SURFACE TEMPORARY
- PREVIOUSLY PLACED, ASPHALTIC SURFACE TEMPORARY

LEGEND

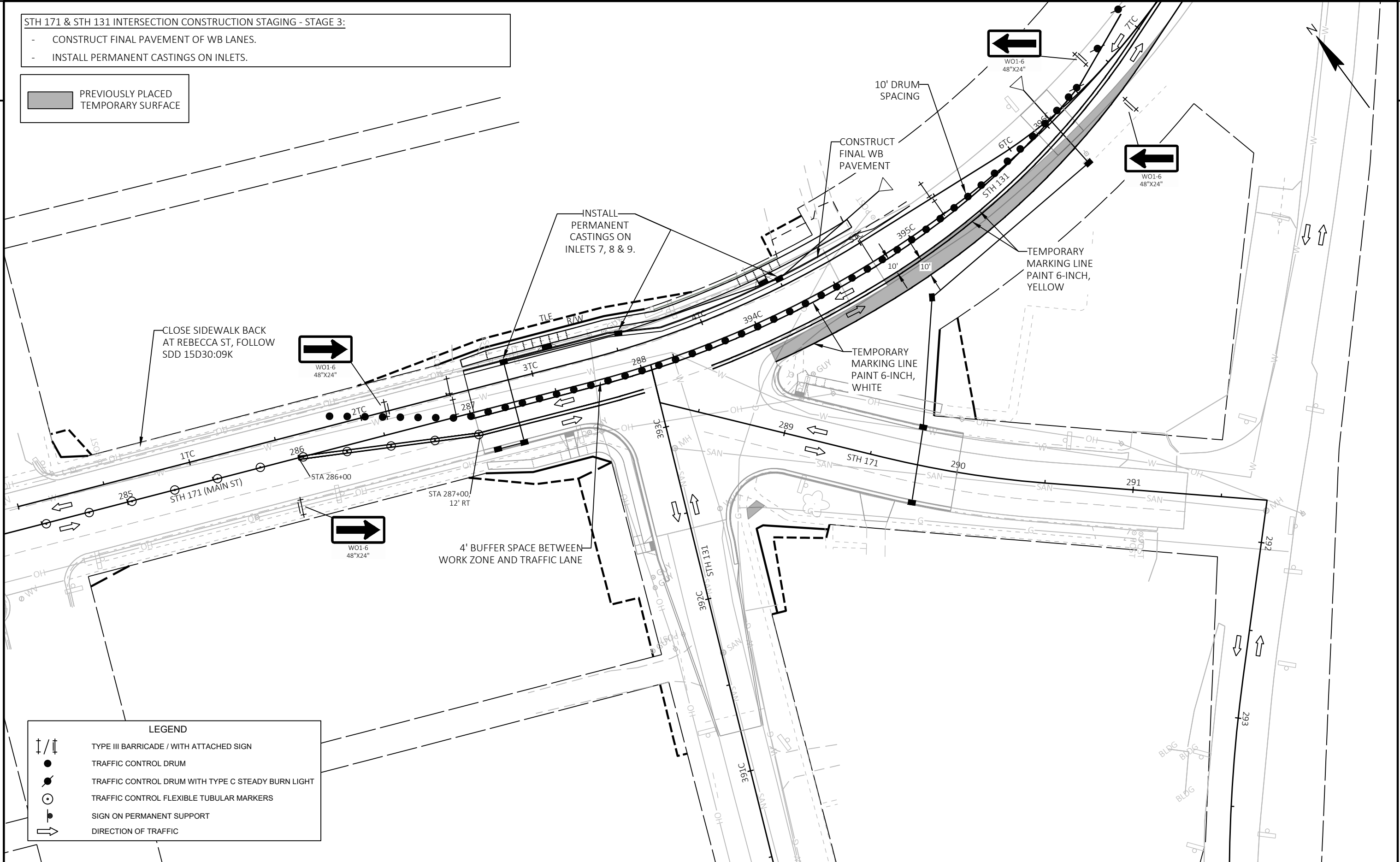
- TYPE III BARRICADE / WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- TRAFFIC CONTROL FLEXIBLE TUBULAR MARKERS
- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC



STH 171 & STH 131 INTERSECTION CONSTRUCTION STAGING - STAGE 3:

- CONSTRUCT FINAL PAVEMENT OF WB LANES.
- INSTALL PERMANENT CASTINGS ON INLETS.

PREVIOUSLY PLACED TEMPORARY SURFACE

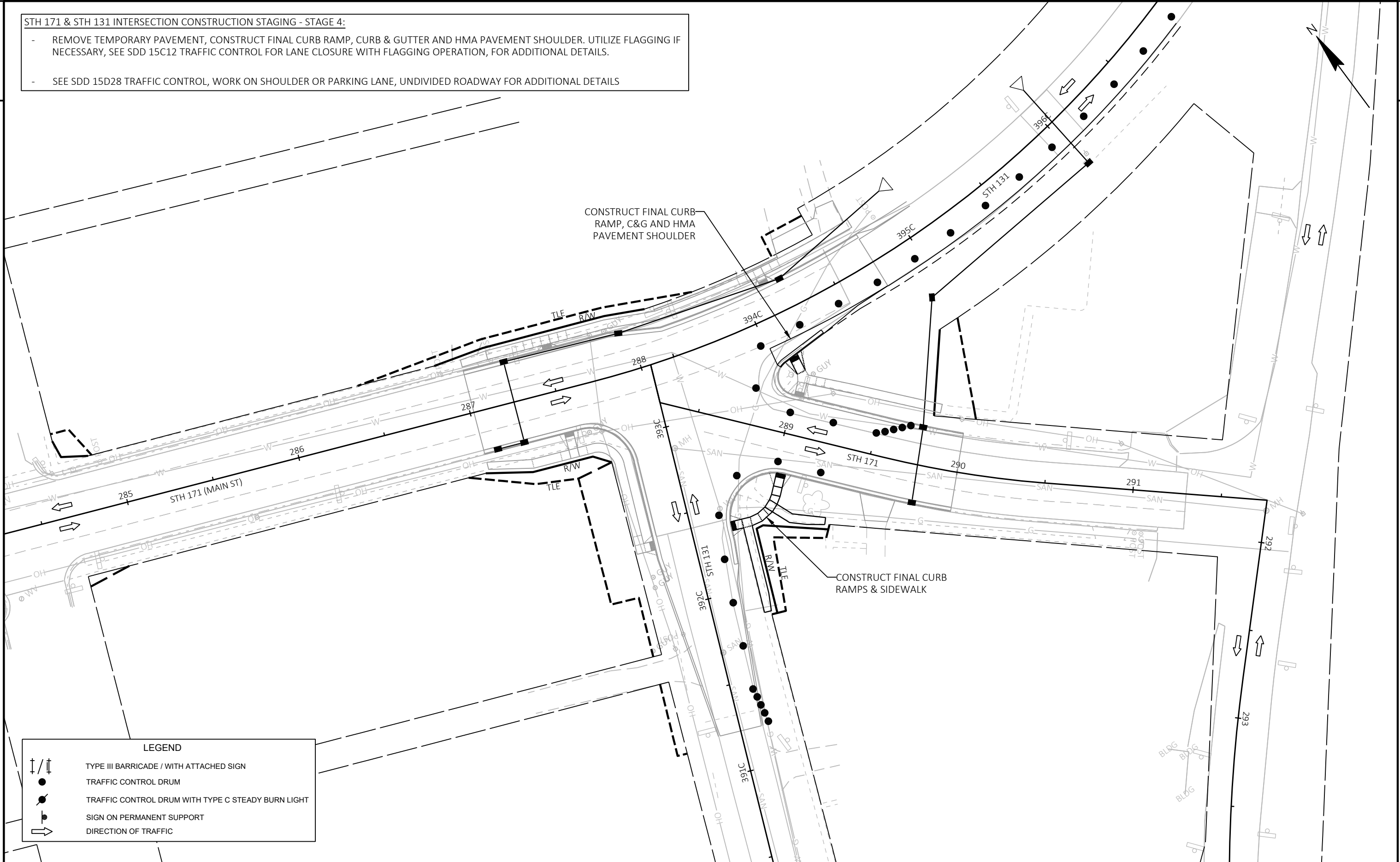


LEGEND

	TYPE III BARRICADE / WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	TRAFFIC CONTROL FLEXIBLE TUBULAR MARKERS
	SIGN ON PERMANENT SUPPORT
	DIRECTION OF TRAFFIC

STH 171 & STH 131 INTERSECTION CONSTRUCTION STAGING - STAGE 4:

- REMOVE TEMPORARY PAVEMENT, CONSTRUCT FINAL CURB RAMP, CURB & GUTTER AND HMA PAVEMENT SHOULDER. UTILIZE FLAGGING IF NECESSARY, SEE SDD 15C12 TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION, FOR ADDITIONAL DETAILS.
- SEE SDD 15D28 TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY FOR ADDITIONAL DETAILS



LEGEND	
	TYPE III BARRICADE / WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	SIGN ON PERMANENT SUPPORT
	DIRECTION OF TRAFFIC

PROJECT NO: 5790-02-72

HWY: STH 171

COUNTY: CRAWFORD

TRAFFIC CONTROL - STAGE 4

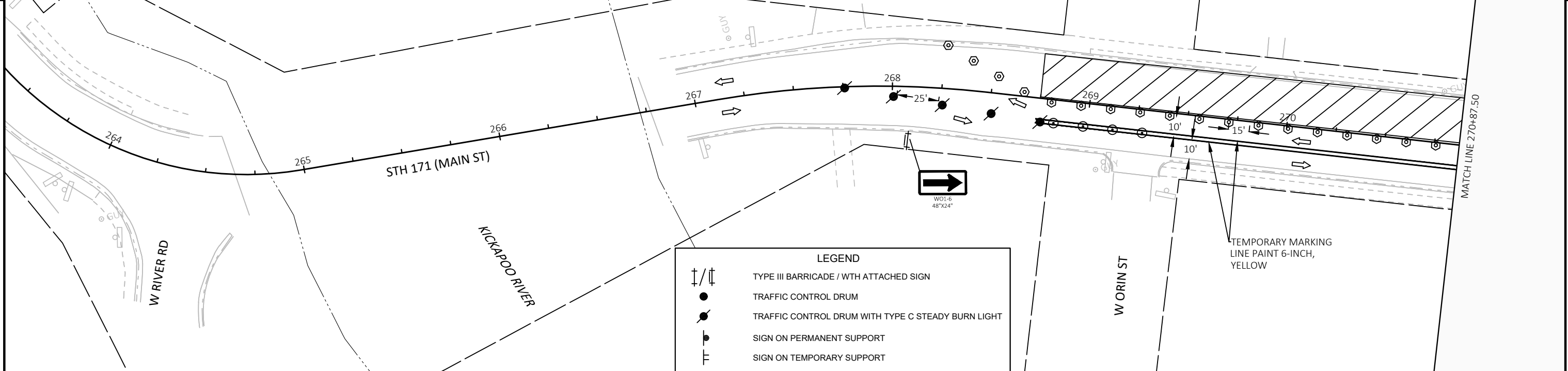
SHEET

E

2 **STAGED CONSTRUCTION - BASE PATCHING CONCRETE: STAGE 1**

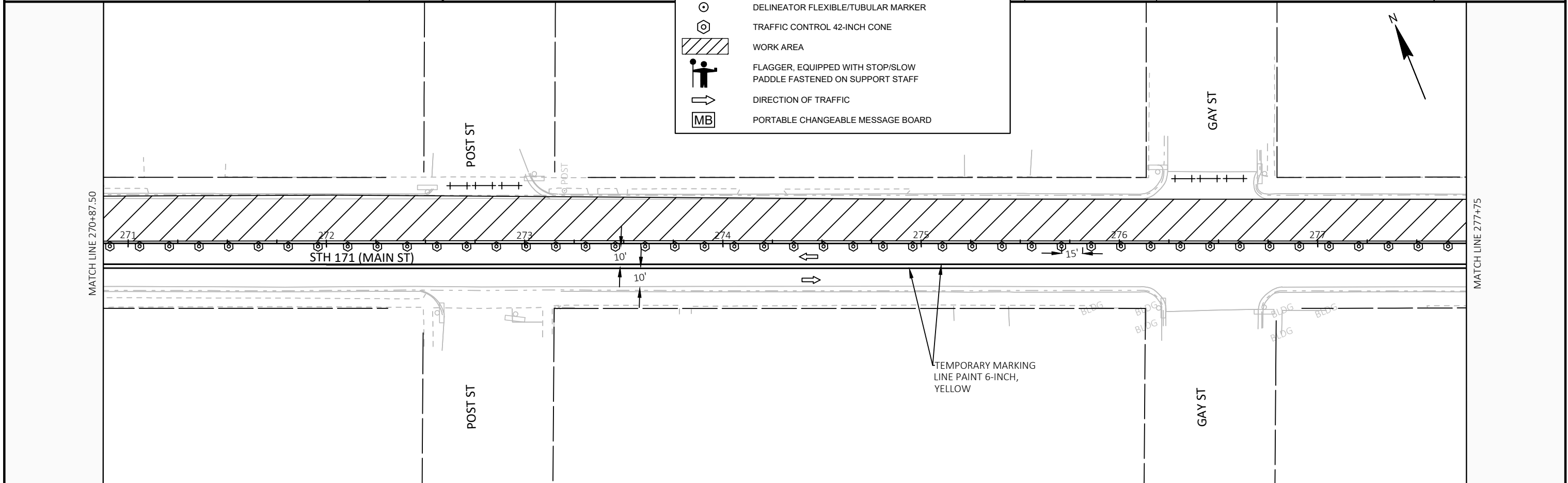
- PLACE ADVANCED WIDTH RESTRICTION SIGNING PRIOR TO START OF WORK. SEE SDD 15C02-09F ADVANCED WIDTH RESTRICTION SIGNING FOR ADDITIONAL DETAILS.
- PERFORM BASE PATCHING CONCRETE WITHIN THE VILLAGE ONE TRAVEL DIRECTION AT A TIME
- CLOSE TRAVEL LANE & SHOULDER WITHIN WORK ZONE, TWO-WAY TRAFFIC TO RUN IN OPPOSITE LANE & SHOULDER
- UTILIZE FLAGGING OPERATIONS WHEN/IF NECESSARY

2



LEGEND

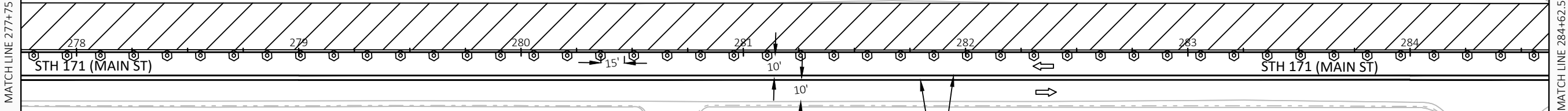
	TYPE III BARRICADE / WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	DELINEATOR FLEXIBLE/TUBULAR MARKER
	TRAFFIC CONTROL 42-INCH CONE
	WORK AREA
	FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
	DIRECTION OF TRAFFIC
	PORTABLE CHANGEABLE MESSAGE BOARD



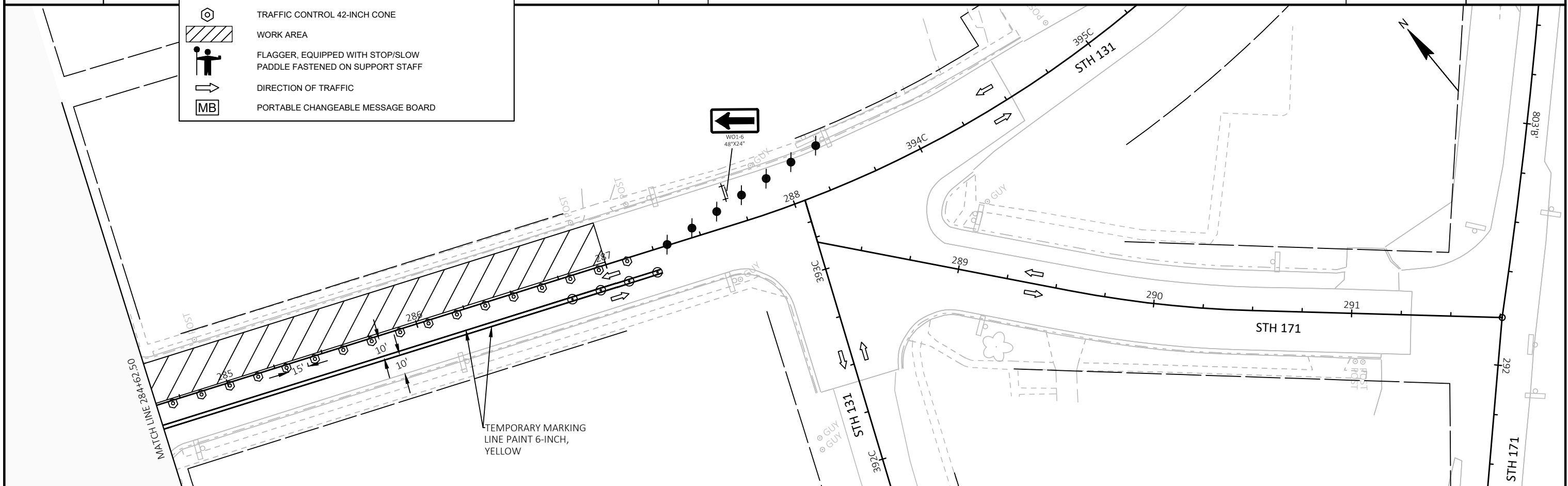
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD STAGING PLAN - BASE PATCHING SHEET **E**

STAGED CONSTRUCTION - BASE PATCHING CONCRETE: STAGE 1

- PERFORM BASE PATCHING CONCRETE WITHIN THE VILLAGE ONE TRAVEL DIRECTION AT A TIME
- CLOSE TRAVEL LANE & SHOULDER WITHIN WORK ZONE, TWO-WAY TRAFFIC TO RUN IN OPPOSITE LANE & SHOULDER
- UTILIZE FLAGGING OPERATIONS WHEN/IF NECESSARY

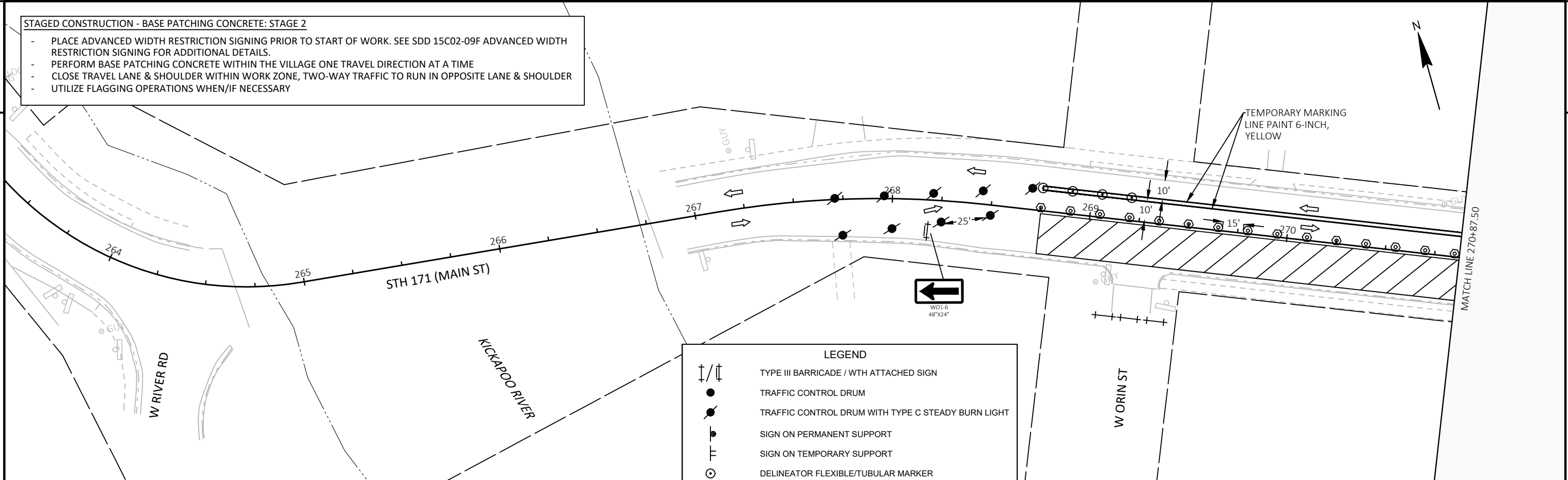


LEGEND	
	TYPE III BARRICADE / WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	DELINEATOR FLEXIBLE/TUBULAR MARKER
	TRAFFIC CONTROL 42-INCH CONE
	WORK AREA
	FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
	DIRECTION OF TRAFFIC
	PORTABLE CHANGEABLE MESSAGE BOARD



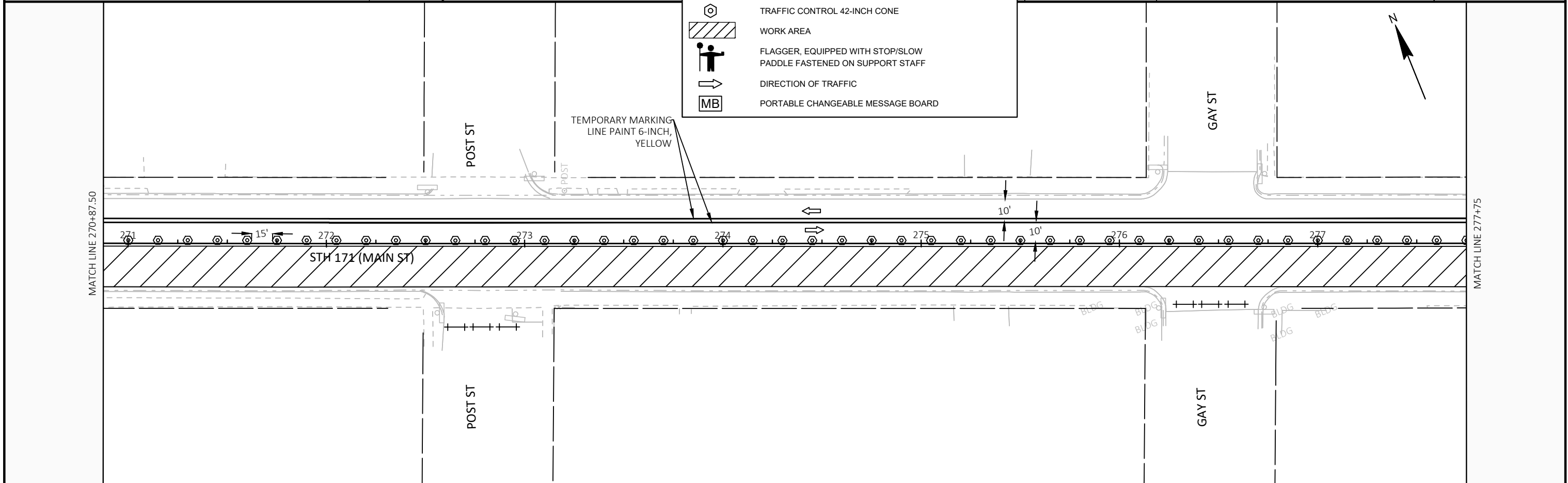
STAGED CONSTRUCTION - BASE PATCHING CONCRETE: STAGE 2

- PLACE ADVANCED WIDTH RESTRICTION SIGNING PRIOR TO START OF WORK. SEE SDD 15C02-09F ADVANCED WIDTH RESTRICTION SIGNING FOR ADDITIONAL DETAILS.
- PERFORM BASE PATCHING CONCRETE WITHIN THE VILLAGE ONE TRAVEL DIRECTION AT A TIME
- CLOSE TRAVEL LANE & SHOULDER WITHIN WORK ZONE, TWO-WAY TRAFFIC TO RUN IN OPPOSITE LANE & SHOULDER
- UTILIZE FLAGGING OPERATIONS WHEN/IF NECESSARY



LEGEND

- TYPE III BARRICADE / WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- DELINEATOR FLEXIBLE/TUBULAR MARKER
- TRAFFIC CONTROL 42-INCH CONE
- WORK AREA
- FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
- DIRECTION OF TRAFFIC
- PORTABLE CHANGEABLE MESSAGE BOARD

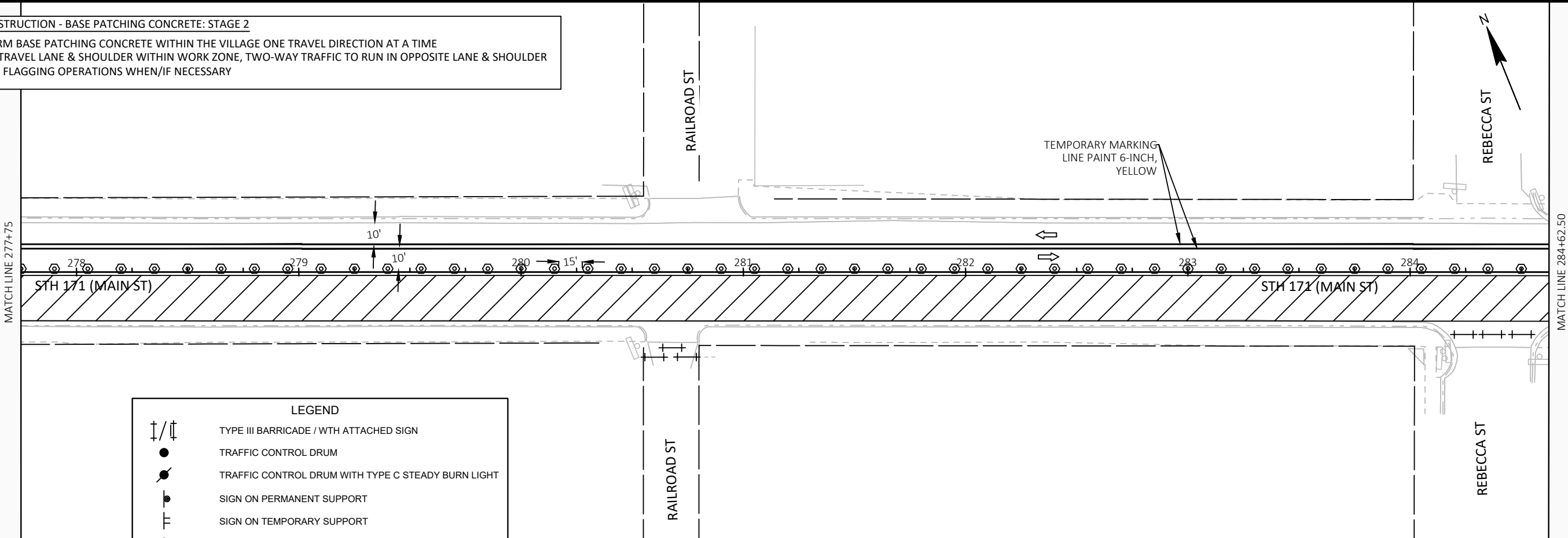


2

STAGED CONSTRUCTION - BASE PATCHING CONCRETE: STAGE 2

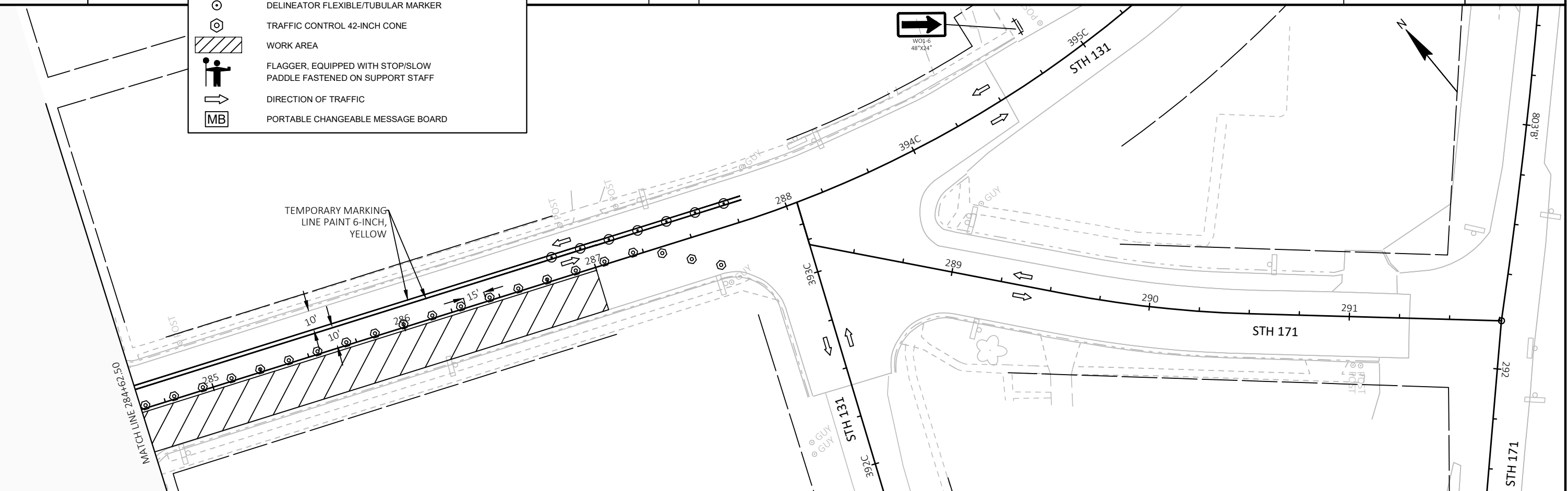
- PERFORM BASE PATCHING CONCRETE WITHIN THE VILLAGE ONE TRAVEL DIRECTION AT A TIME
- CLOSE TRAVEL LANE & SHOULDER WITHIN WORK ZONE, TWO-WAY TRAFFIC TO RUN IN OPPOSITE LANE & SHOULDER
- UTILIZE FLAGGING OPERATIONS WHEN/IF NECESSARY

2

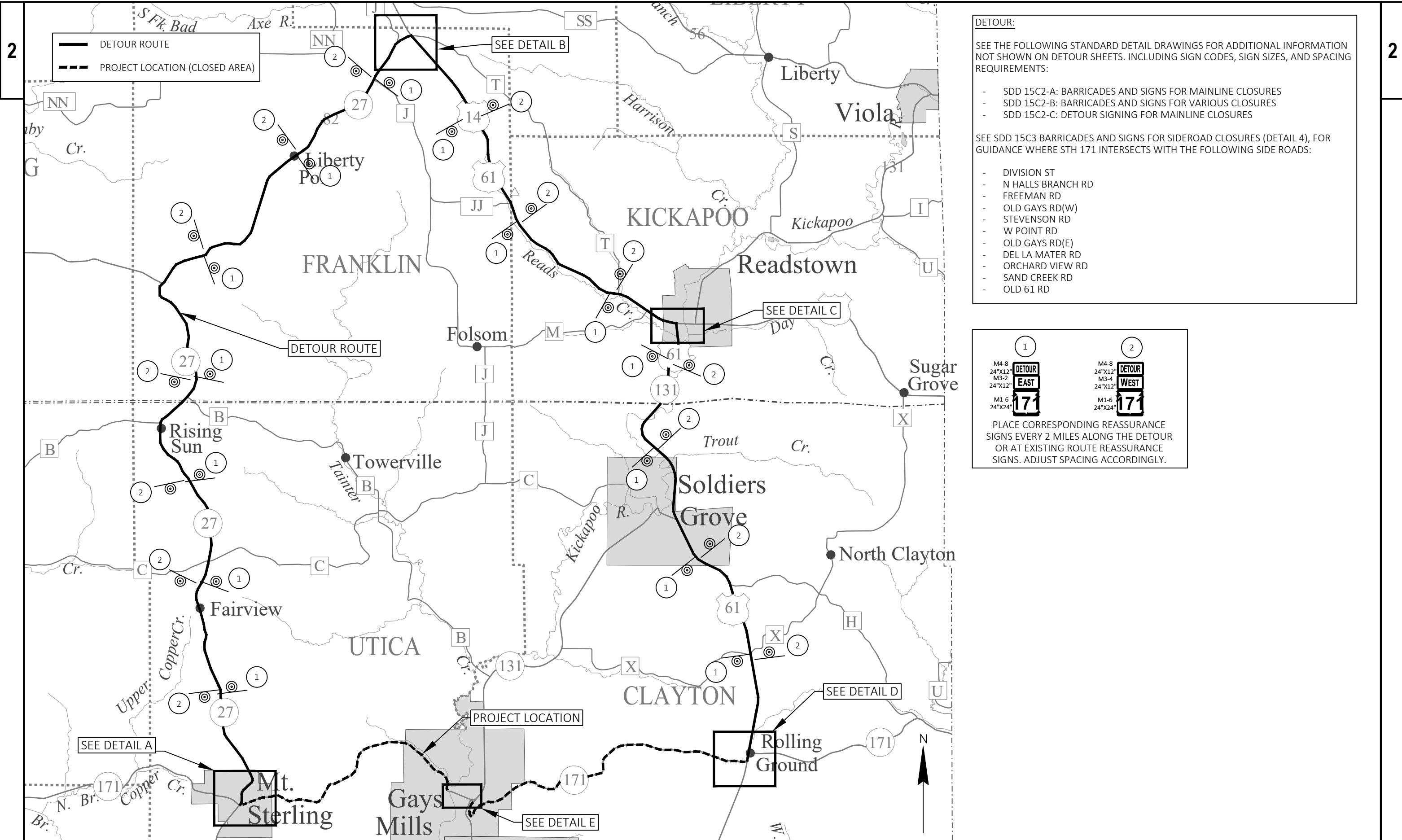


LEGEND

- TYPE III BARRICADE / WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- DELINEATOR FLEXIBLE/TUBULAR MARKER
- TRAFFIC CONTROL 42-INCH CONE
- WORK AREA
- FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
- DIRECTION OF TRAFFIC
- PORTABLE CHANGEABLE MESSAGE BOARD



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD STAGING PLAN - BASE PATCHING SHEET E







DETOUR:
 SEE THE FOLLOWING STANDARD DETAIL DRAWINGS FOR ADDITIONAL INFORMATION NOT SHOWN ON DETOUR SHEETS. INCLUDING SIGN CODES, SIGN SIZES, AND SPACING REQUIREMENTS:

- SDD 15C2-A: BARRICADES AND SIGNS FOR MAINLINE CLOSURES
- SDD 15C2-B: BARRICADES AND SIGNS FOR VARIOUS CLOSURES
- SDD 15C2-C: DETOUR SIGNING FOR MAINLINE CLOSURES

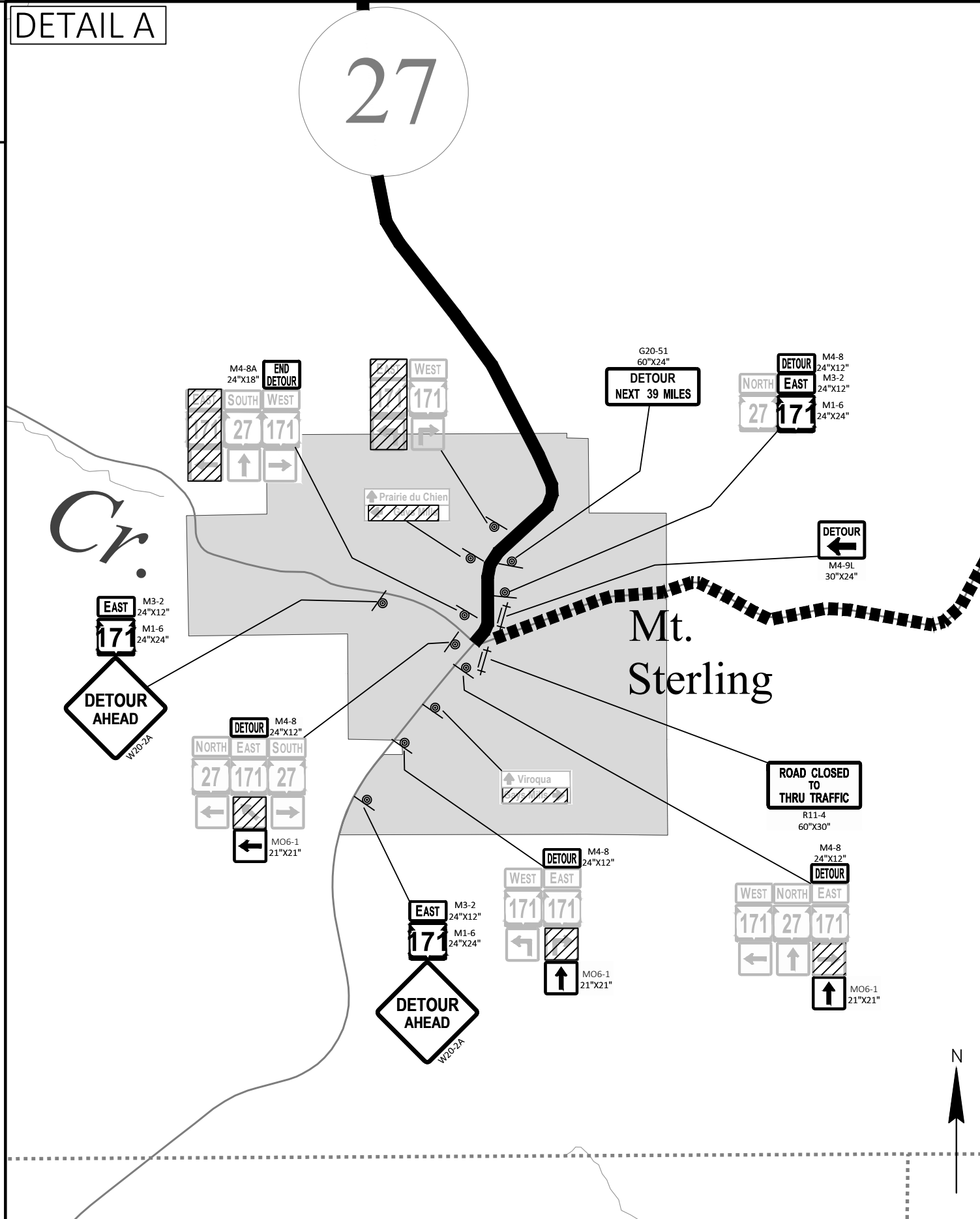
SEE SDD 15C3 BARRICADES AND SIGNS FOR SIDEROAD CLOSURES (DETAIL 4), FOR GUIDANCE WHERE STH 171 INTERSECTS WITH THE FOLLOWING SIDE ROADS:

- DIVISION ST
- N HALLS BRANCH RD
- FREEMAN RD
- OLD GAYS RD(W)
- STEVENSON RD
- W POINT RD
- OLD GAYS RD(E)
- DEL LA MATER RD
- ORCHARD VIEW RD
- SAND CREEK RD
- OLD 61 RD

1	2
<small>M4-8 24"x12"</small> <small>M3-2 24"x12"</small>  <small>M1-6 24"x24"</small> 	<small>M4-8 24"x12"</small> <small>M3-4 24"x12"</small>  <small>M1-6 24"x24"</small> 
PLACE CORRESPONDING REASSURANCE SIGNS EVERY 2 MILES ALONG THE DETOUR OR AT EXISTING ROUTE REASSURANCE SIGNS. ADJUST SPACING ACCORDINGLY.	

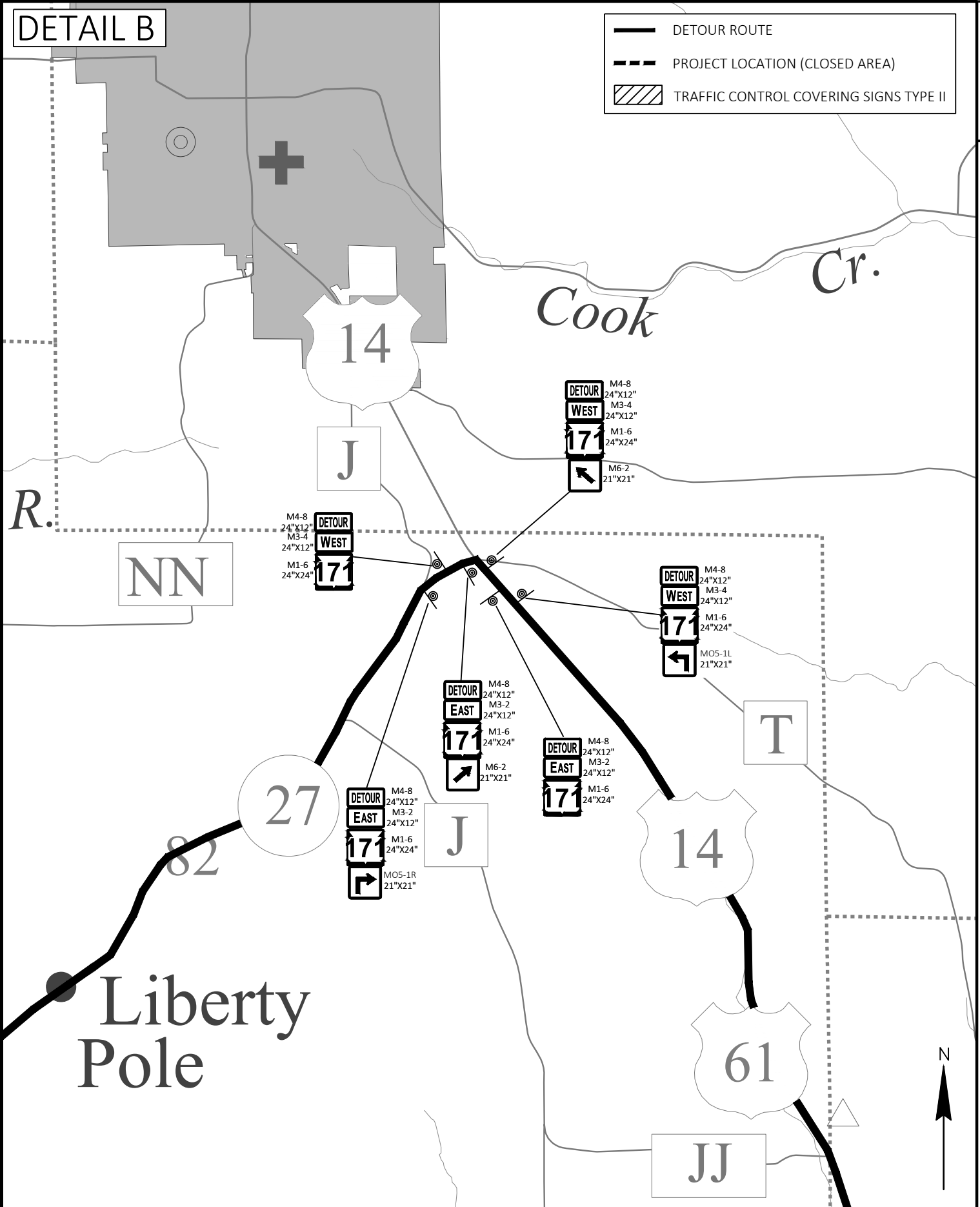
DETAIL A

2



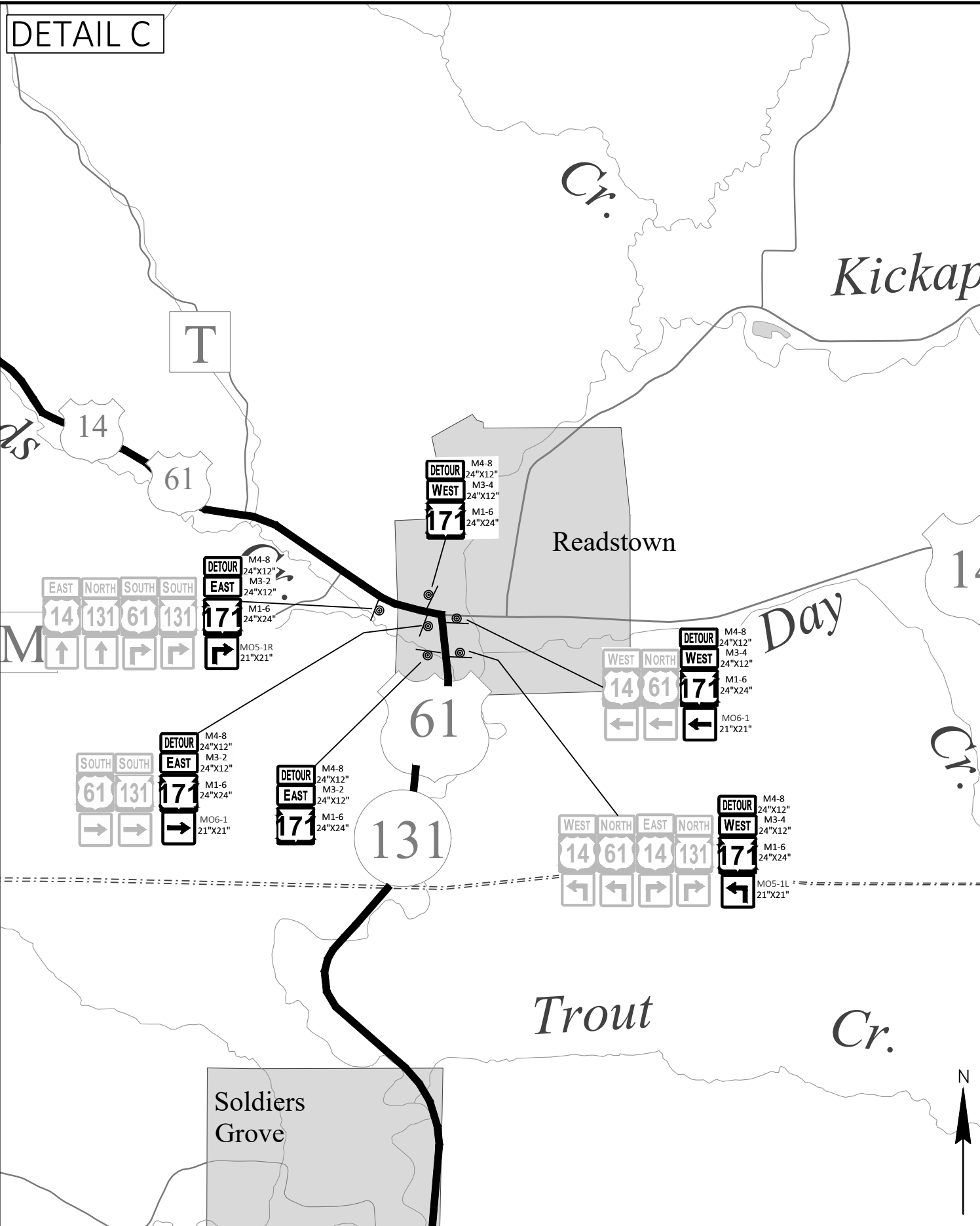
DETAIL B

2

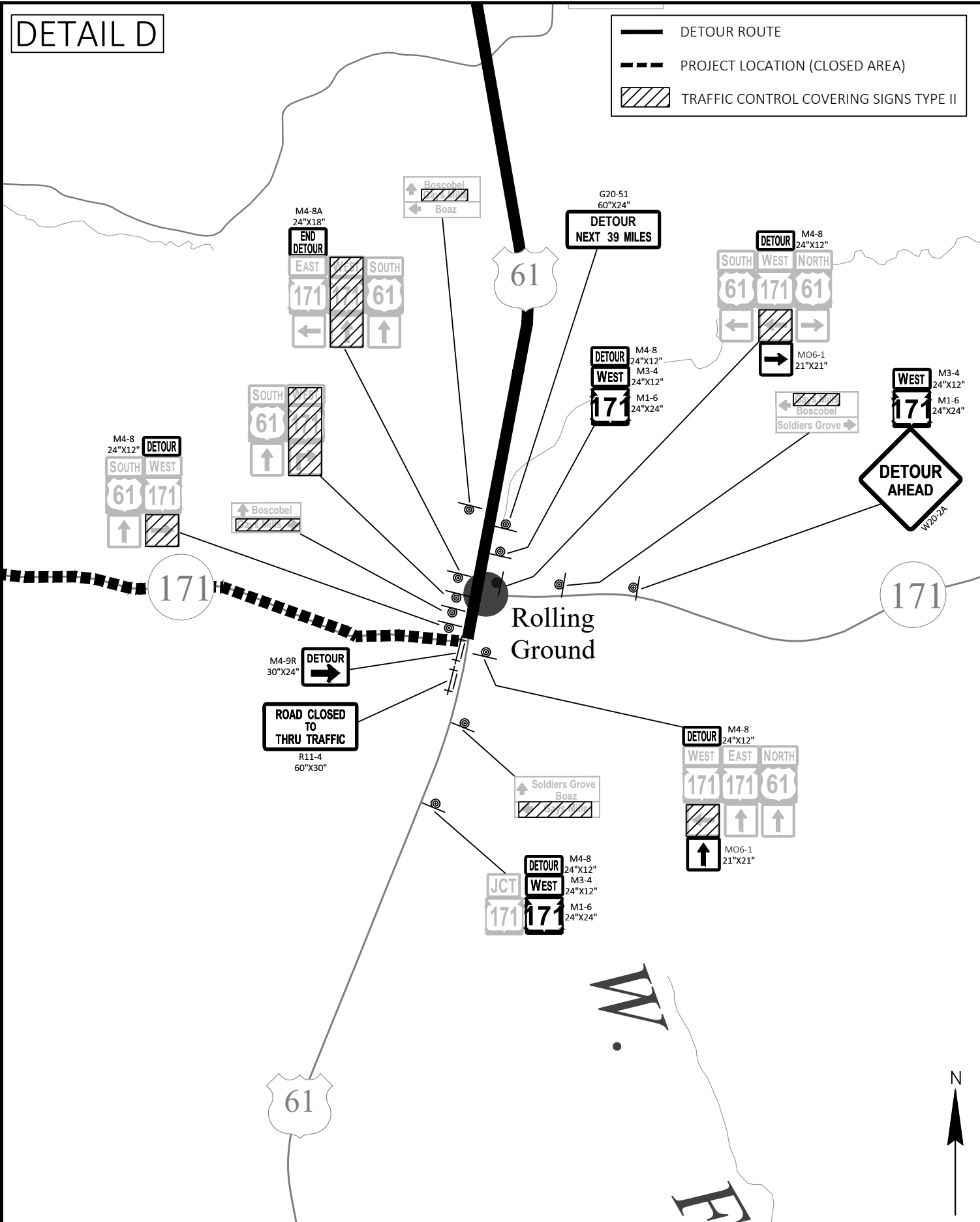


——— DETOUR ROUTE
 - - - PROJECT LOCATION (CLOSED AREA)
 TRAFFIC CONTROL COVERING SIGNS TYPE II

DETAIL C



DETAIL D



——— DETOUR ROUTE
 - - - PROJECT LOCATION (CLOSED AREA)
 ▨ TRAFFIC CONTROL COVERING SIGNS TYPE II

PROJECT NO: 5790-02-72

HWY: STH 171

COUNTY: CRAWFORD

DETOUR PLAN - STH 171




SHEET

E

DETAIL E

2

2

 DETOUR ROUTE
 PROJECT LOCATION (CLOSED AREA)
 TRAFFIC CONTROL COVERING SIGNS TYPE II

Gays Mills

171

131

171

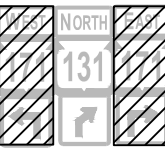

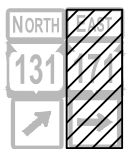

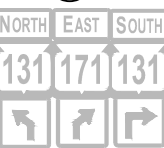
131

ROAD CLOSED TO THRU TRAFFIC
R11-4
60"x30"

ROAD OPEN TO W RIVER RD
R10-61
60"x30"

ROAD CLOSED TO THRU TRAFFIC
R11-4
60"x30"

- 5
- 4
- 3
- 2
- 1

1 
 2 
 3 
 4 
 5 





LEGEND

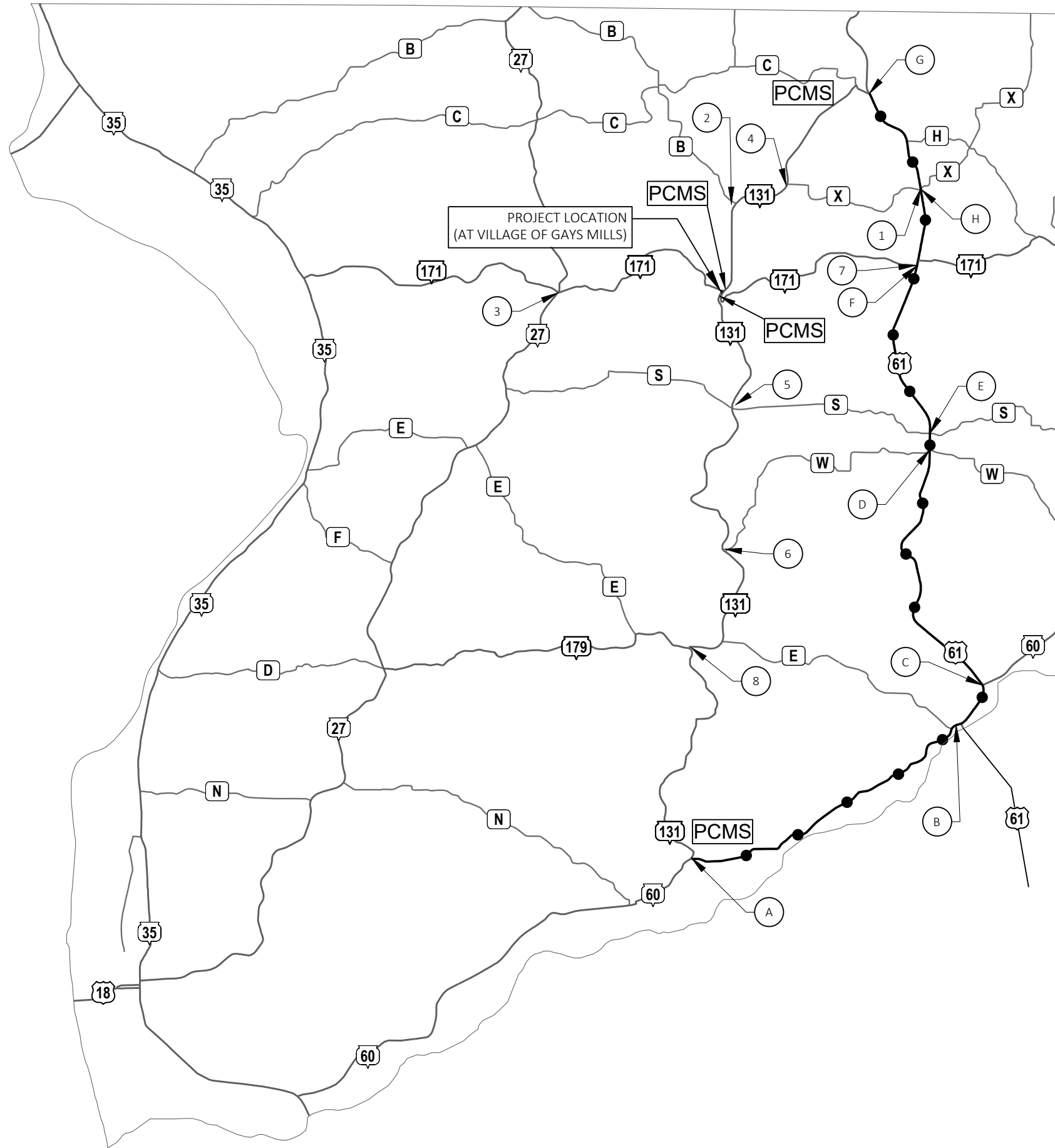
- DETOUR ROUTE (STH 131)
- PROJECT WORK ZONE
- DETOUR DETAIL
- SIGN LOCATION

PCMS MESSAGES

MESSAGE 1
ROAD
TO
CLOSE

MESSAGE 2
XXXXDAY
XX/XX

PCMS TO BE PLACED 7 DAYS PRIOR TO START OF DETOUR

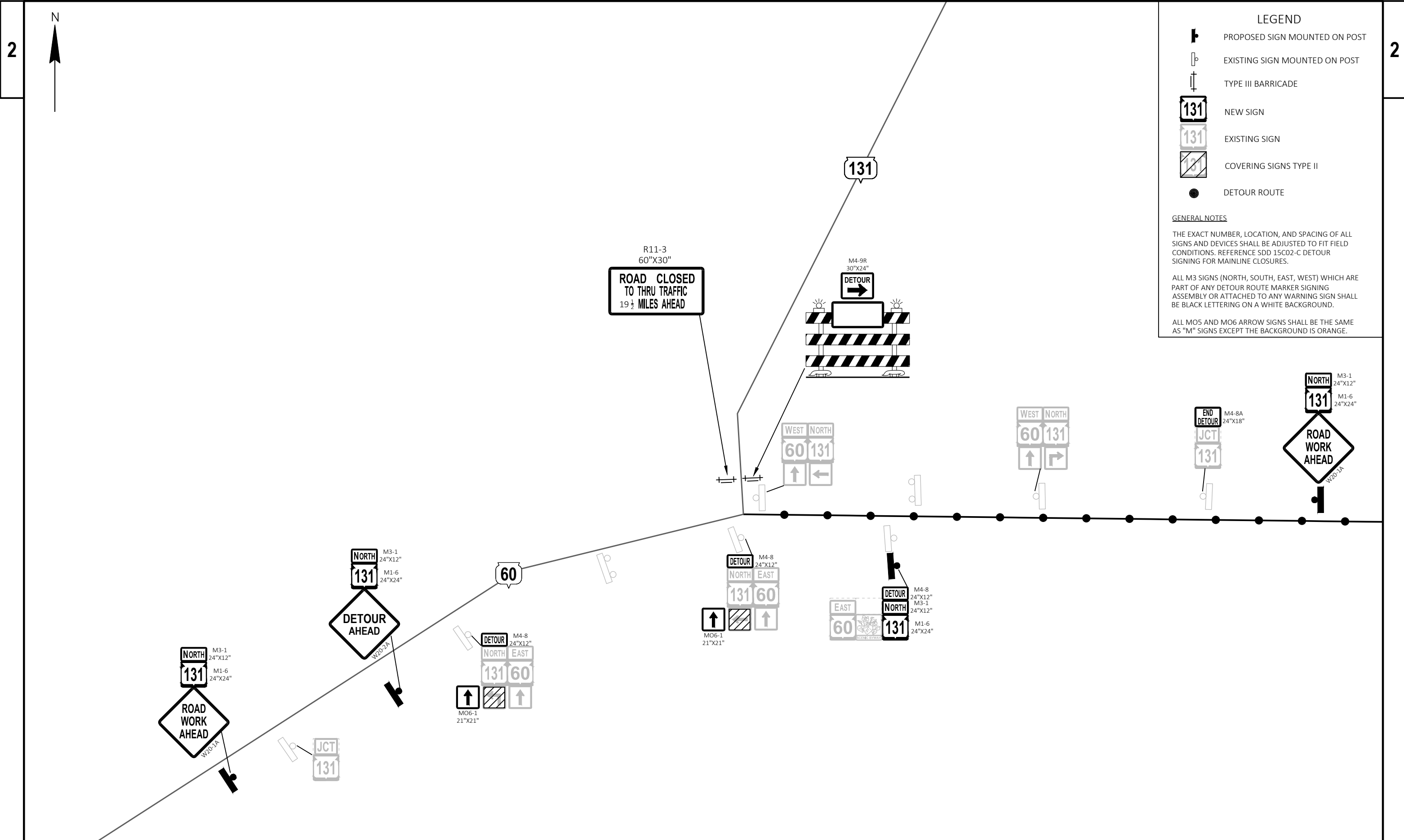







SIGN LOCATION DETAIL

DETAIL NUMBER	DETAIL LOCATION	MILES	DIRECTION/CODE
1	(USH 61 & CTH X)	4 1/2	NORTH/M3-1
2	(CTH B & STH 131)	2 1/2	SOUTH/M3-3
3	(STH 131 & CTH X)	4	SOUTH/M3-3
4	(STH 27 & CTH S)	6 1/2	NORTH/M3-1
5	(STH 131 & CTH S)	3 1/2	NORTH/M3-1
6	(STH 131 & CTH W)	8 1/2	NORTH/M3-1
7	(STH 61 & STH 171)	6	SOUTH/M3-3
8	(STH 131 & STH 179)	12	NORTH/M3-1

REASSURANCE SIGNS DETAIL

PLACE REASSURANCE SIGNS ALONG THE DETOUR ROUTE, AT 4 MILE MAXIMUM INTERVALS



- LEGEND**
-  PROPOSED SIGN MOUNTED ON POST
 -  EXISTING SIGN MOUNTED ON POST
 -  TYPE III BARRICADE
 -  NEW SIGN
 -  EXISTING SIGN
 -  COVERING SIGNS TYPE II
 -  DETOUR ROUTE








GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS. REFERENCE SDD 15C02-C DETOUR SIGNING FOR MAINLINE CLOSURES.

ALL M3 SIGNS (NORTH, SOUTH, EAST, WEST) WHICH ARE PART OF ANY DETOUR ROUTE MARKER SIGNING ASSEMBLY OR ATTACHED TO ANY WARNING SIGN SHALL BE BLACK LETTERING ON A WHITE BACKGROUND.

ALL MO5 AND MO6 ARROW SIGNS SHALL BE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

LEGEND

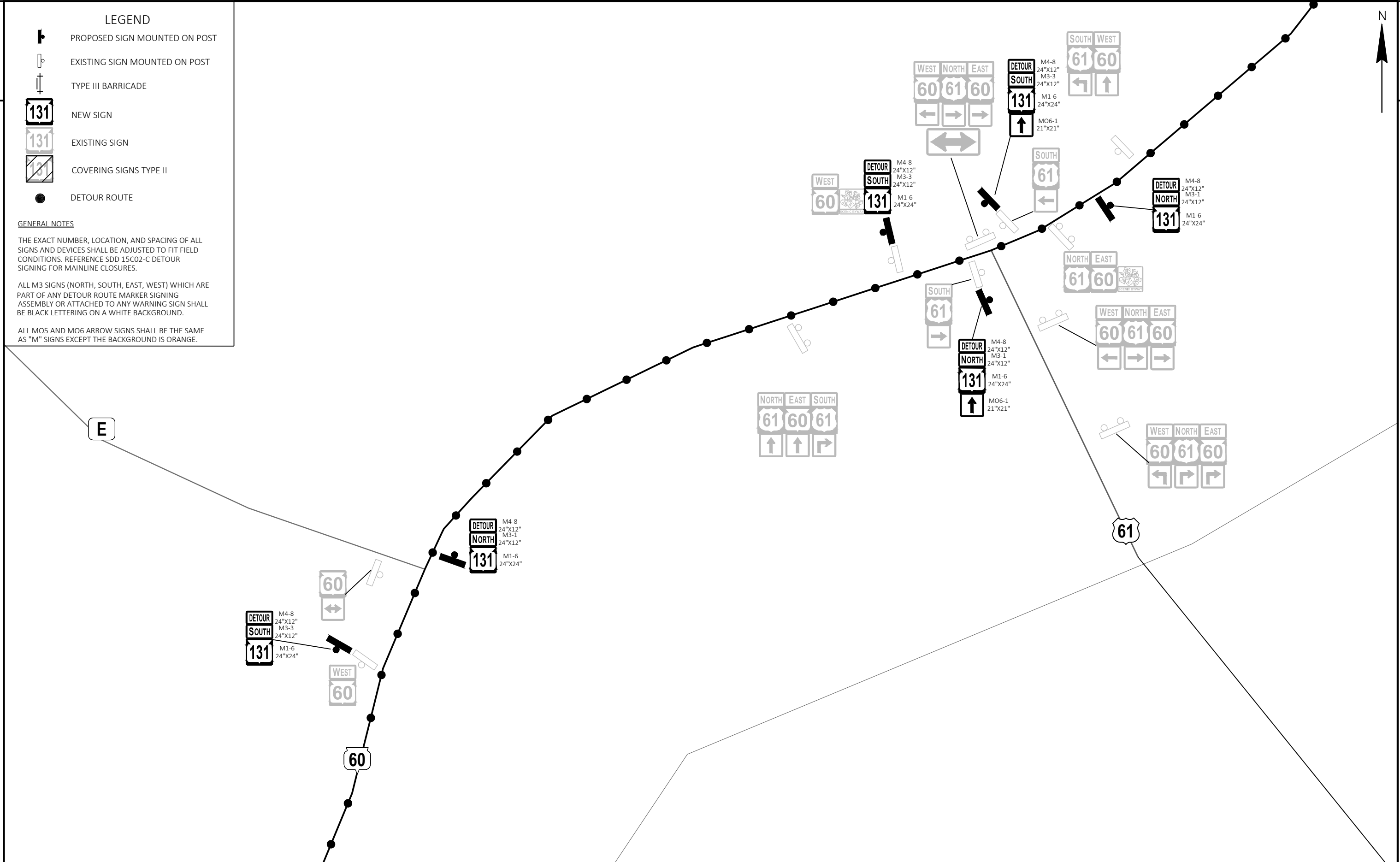
-  PROPOSED SIGN MOUNTED ON POST
-  EXISTING SIGN MOUNTED ON POST
-  TYPE III BARRICADE
-  NEW SIGN
-  EXISTING SIGN
-  COVERING SIGNS TYPE II
-  DETOUR ROUTE

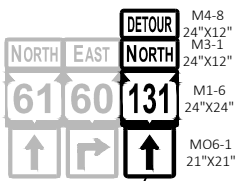
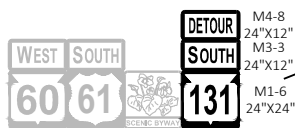
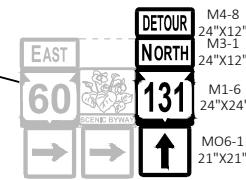
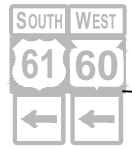
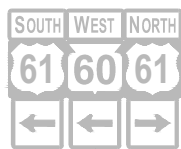
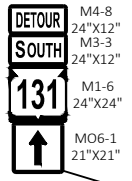
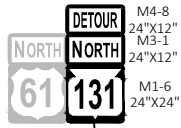
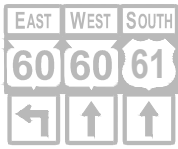
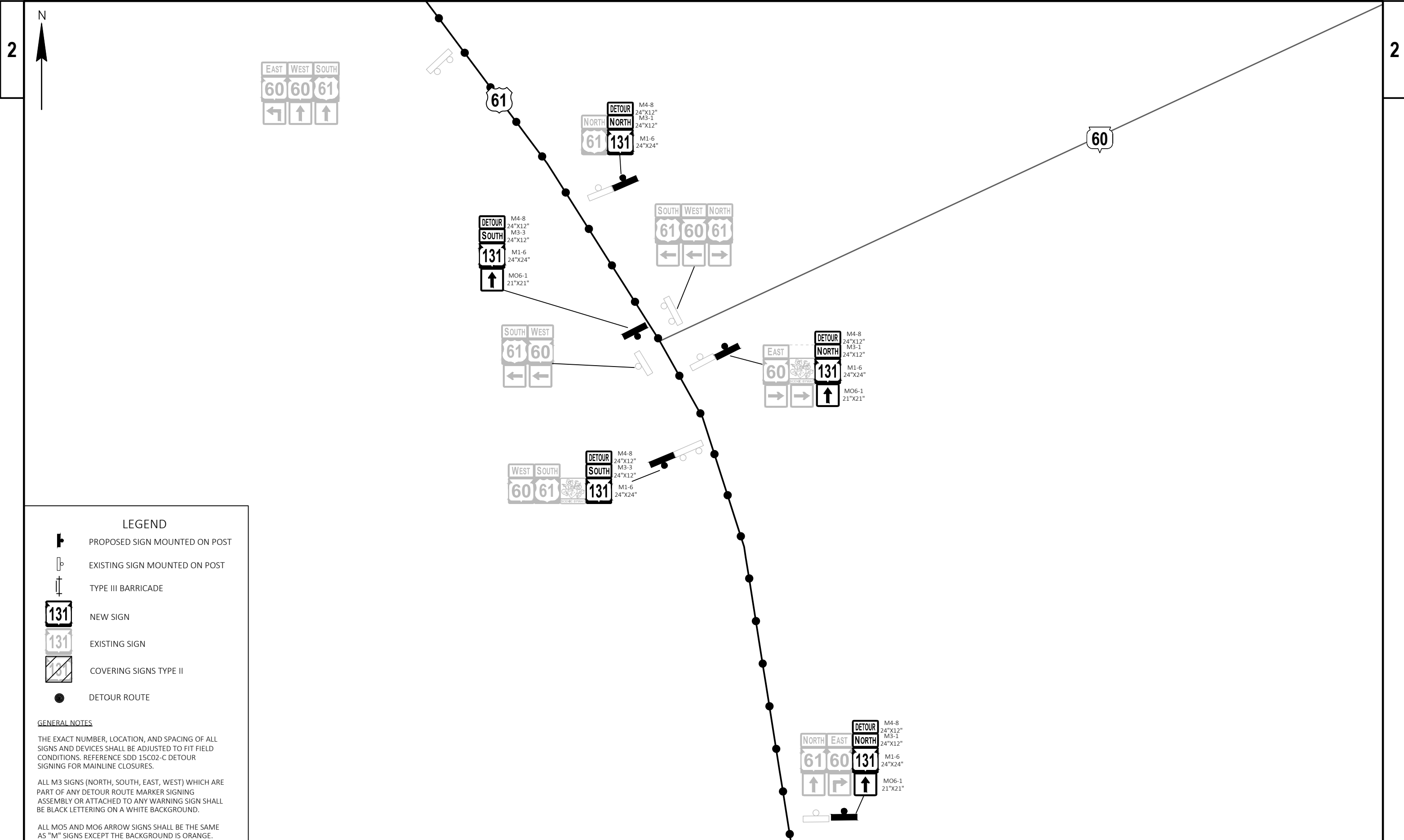
GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS. REFERENCE SDD 15C02-C DETOUR SIGNING FOR MAINLINE CLOSURES.








ALL M3 SIGNS (NORTH, SOUTH, EAST, WEST) WHICH ARE PART OF ANY DETOUR ROUTE MARKER SIGNING ASSEMBLY OR ATTACHED TO ANY WARNING SIGN SHALL BE BLACK LETTERING ON A WHITE BACKGROUND.

ALL M05 AND M06 ARROW SIGNS SHALL BE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.





LEGEND

-  PROPOSED SIGN MOUNTED ON POST
-  EXISTING SIGN MOUNTED ON POST
-  TYPE III BARRICADE
-  NEW SIGN
-  EXISTING SIGN
-  COVERING SIGNS TYPE II
-  DETOUR ROUTE

GENERAL NOTES








THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS. REFERENCE SDD 15C02-C DETOUR SIGNING FOR MAINLINE CLOSURES.

ALL M3 SIGNS (NORTH, SOUTH, EAST, WEST) WHICH ARE PART OF ANY DETOUR ROUTE MARKER SIGNING ASSEMBLY OR ATTACHED TO ANY WARNING SIGN SHALL BE BLACK LETTERING ON A WHITE BACKGROUND.

ALL M05 AND M06 ARROW SIGNS SHALL BE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.



LEGEND

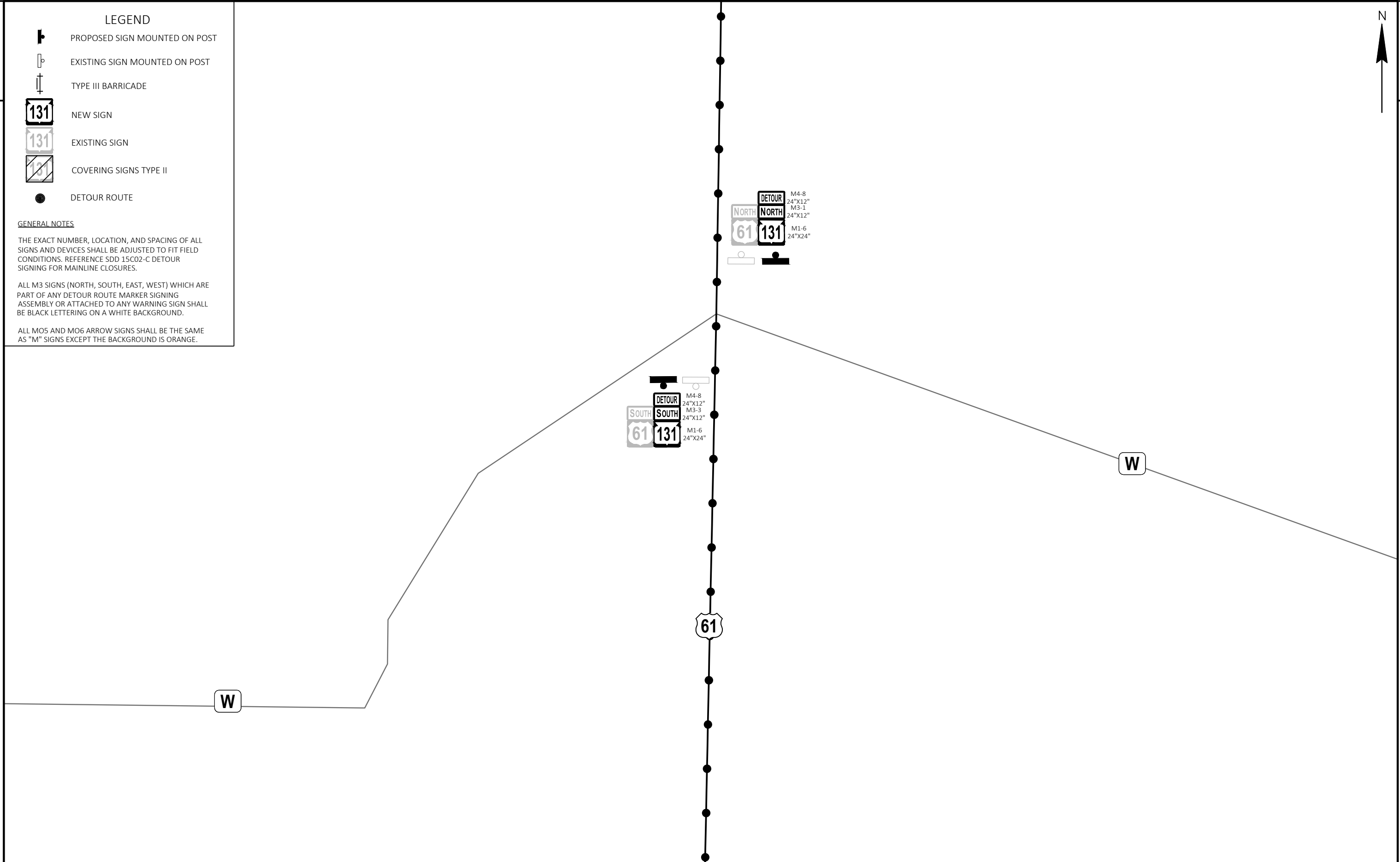
-  PROPOSED SIGN MOUNTED ON POST
-  EXISTING SIGN MOUNTED ON POST
-  TYPE III BARRICADE
-  NEW SIGN
-  EXISTING SIGN
-  COVERING SIGNS TYPE II
-  DETOUR ROUTE

GENERAL NOTES

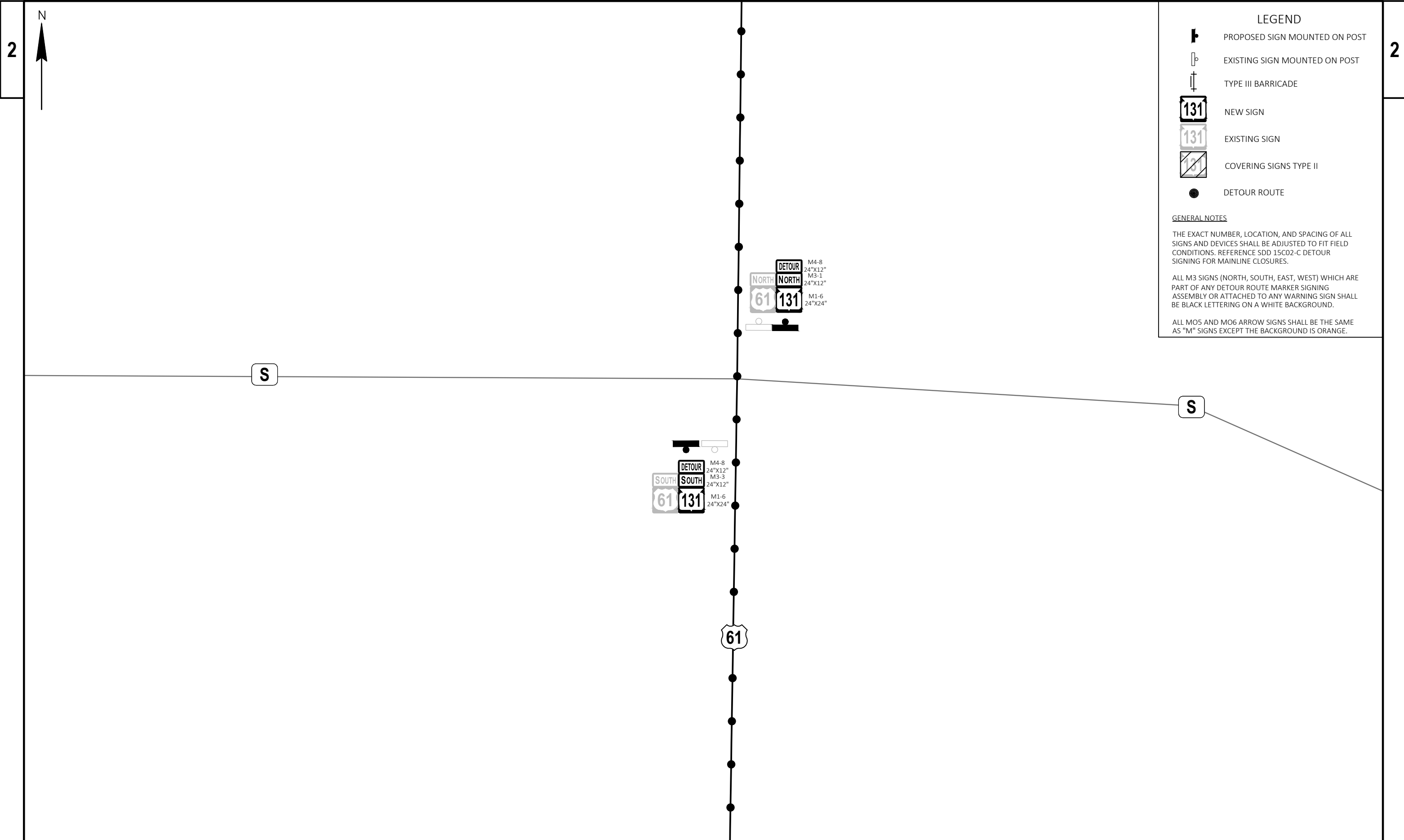
THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS. REFERENCE SDD 15C02-C DETOUR SIGNING FOR MAINLINE CLOSURES.

ALL M3 SIGNS (NORTH, SOUTH, EAST, WEST) WHICH ARE PART OF ANY DETOUR ROUTE MARKER SIGNING ASSEMBLY OR ATTACHED TO ANY WARNING SIGN SHALL BE BLACK LETTERING ON A WHITE BACKGROUND.








ALL M05 AND M06 ARROW SIGNS SHALL BE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.



PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	DETOUR DETAIL D	SHEET	E
------------------------	--------------	------------------	-----------------	-------	---



LEGEND

-  PROPOSED SIGN MOUNTED ON POST
-  EXISTING SIGN MOUNTED ON POST
-  TYPE III BARRICADE
-  NEW SIGN
-  EXISTING SIGN
-  COVERING SIGNS TYPE II
-  DETOUR ROUTE

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS. REFERENCE SDD 15C02-C DETOUR SIGNING FOR MAINLINE CLOSURES.

ALL M3 SIGNS (NORTH, SOUTH, EAST, WEST) WHICH ARE PART OF ANY DETOUR ROUTE MARKER SIGNING ASSEMBLY OR ATTACHED TO ANY WARNING SIGN SHALL BE BLACK LETTERING ON A WHITE BACKGROUND.

ALL M05 AND M06 ARROW SIGNS SHALL BE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

PROJECT NO: 5790-02-72

HWY: STH 171

COUNTY: CRAWFORD








DETOUR DETAIL E

SHEET

E



LEGEND

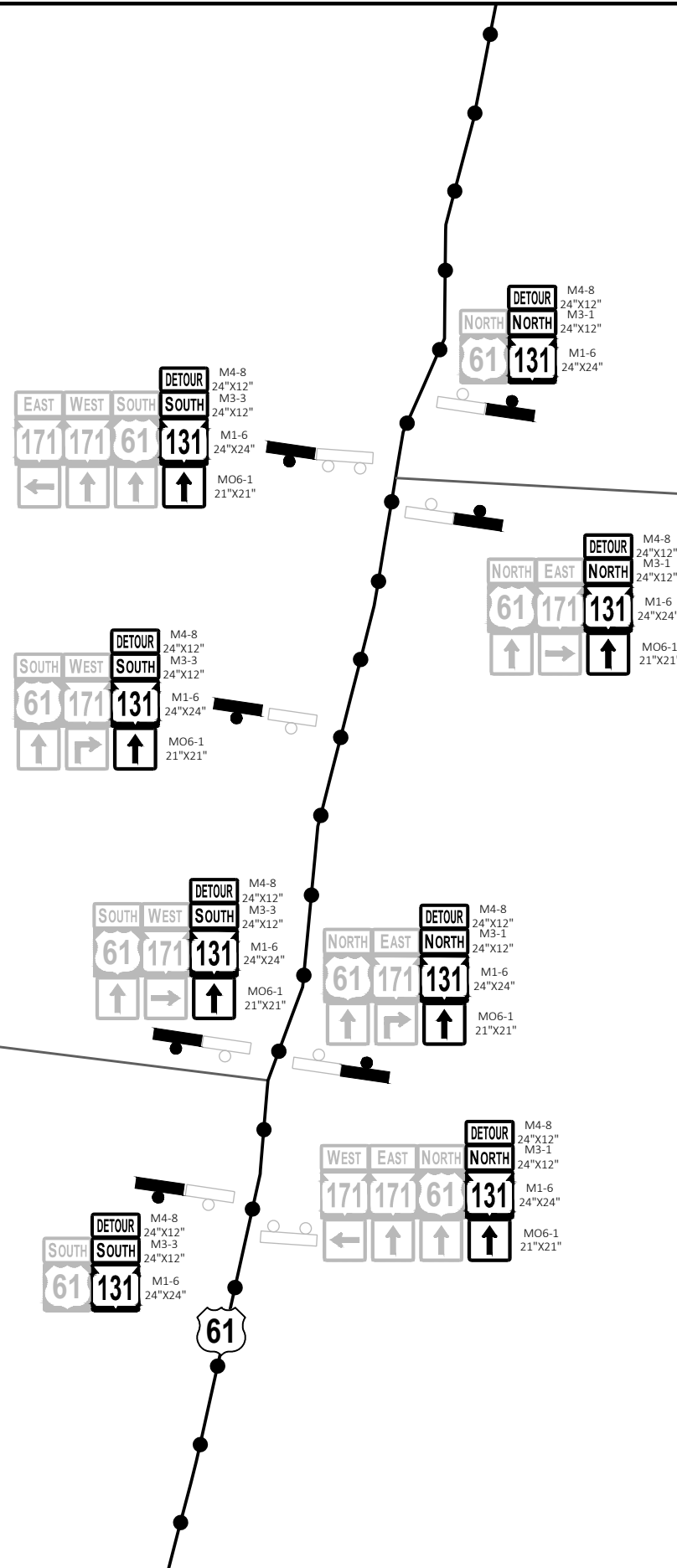
-  PROPOSED SIGN MOUNTED ON POST
-  EXISTING SIGN MOUNTED ON POST
-  TYPE III BARRICADE
-  NEW SIGN
-  EXISTING SIGN
-  COVERING SIGNS TYPE II
-  DETOUR ROUTE

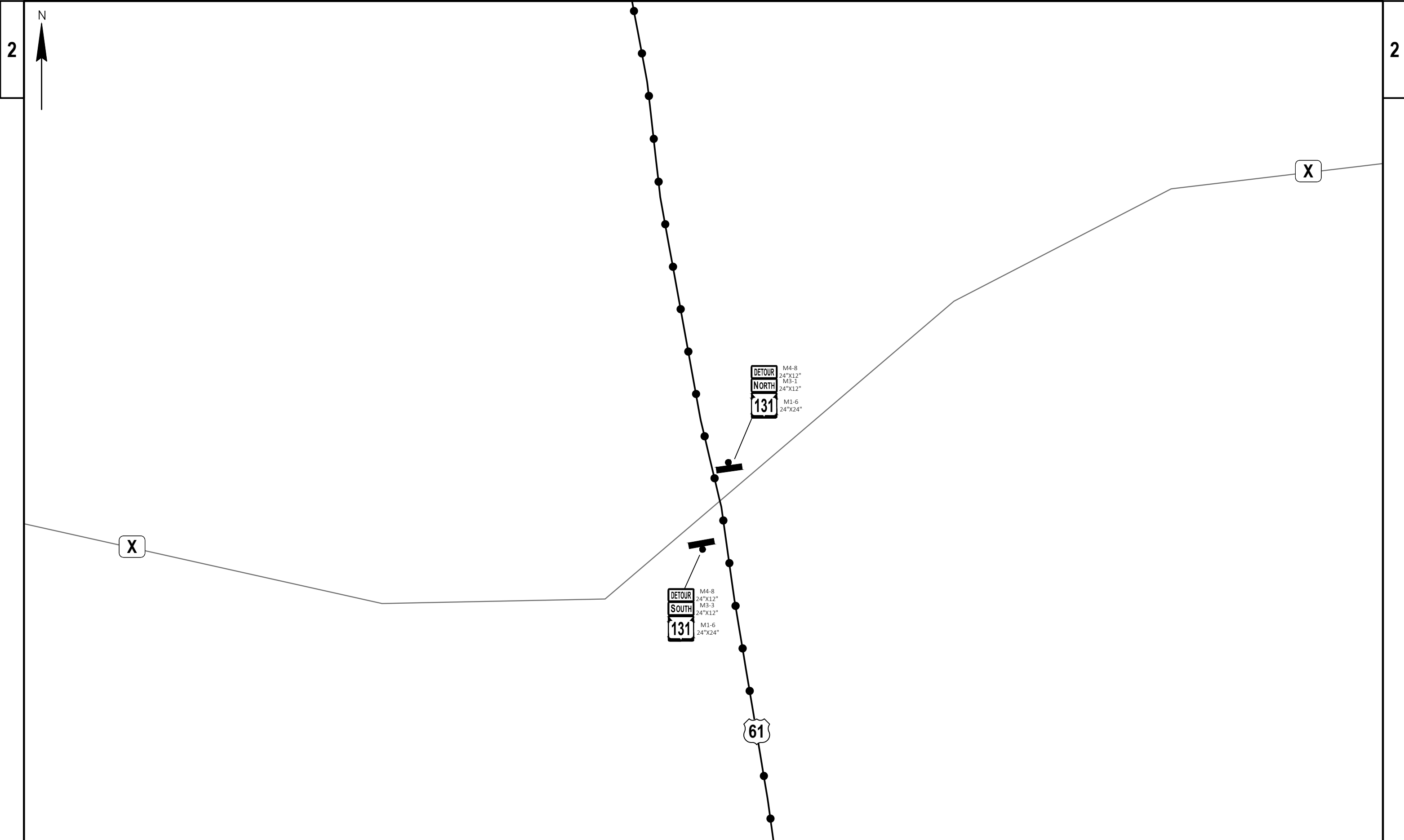
GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS. REFERENCE SDD 15C02-C DETOUR SIGNING FOR MAINLINE CLOSURES.

ALL M3 SIGNS (NORTH, SOUTH, EAST, WEST) WHICH ARE PART OF ANY DETOUR ROUTE MARKER SIGNING ASSEMBLY OR ATTACHED TO ANY WARNING SIGN SHALL BE BLACK LETTERING ON A WHITE BACKGROUND.

ALL MO5 AND MO6 ARROW SIGNS SHALL BE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.





2

2

PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	DETOUR	SHEET	E
------------------------	--------------	------------------	--------	-------	---

FILE NAME : \\SP-PZ1.SEHINC.COM\PROJECTS\1\UZ\W\WITSW\162034\5-FINAL-DSGN\51-DRAWINGS\10-CIVIL\C3D\57900202\SHEETS\PLAN\027001_DT.DWG
LAYOUT NAME - Detail H

PLOT DATE : 7/23/2024 10:02 AM

PLOT BY : CLARISSA ERBS

PLOT NAME :

PLOT SCALE : 1 IN:100 FT

WISDOT/CADD SHEET 42

SE REGION	STATION	DESCRIPTION	LEFT OUTSIDE SHOULDER	LEFT OUTSIDE LANE	RIGHT OUTSIDE LANE	RIGHT OUTSIDE SHOULDER
	1+00.00'	BEGIN ALIGNMENT	-4.00%	-2.00%	-2.00%	-4.00%
2	21+46.36'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
2	21+46.36'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
2	21+97.36'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
2	22+48.36'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
2	22+99.36'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
2	23+50.36'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
2	25+88.06'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
2	26+39.06'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
2	26+90.06'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
2	27+41.06'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
2	27+92.06'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
2	27+92.06'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
3	29+93.14'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
3	29+93.14'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
3	30+44.14'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
3	30+95.14'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
3	31+46.14'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
3	31+97.14'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
3	34+87.98'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
4	36+90.13'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
4	41+53.65'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
4	42+04.65'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
4	42+55.65'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
4	43+06.65'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
4	43+57.65'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
4	43+57.65'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
5	47+33.69'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
5	47+33.69'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
5	47+84.69'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
5	48+35.69'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
5	48+86.69'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
5	49+37.69'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
5	56+00.32'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
5	56+51.32'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
5	57+02.32'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
5	57+53.32'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
5	58+04.32'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
5	58+04.32'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
6	59+51.17'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
6	59+51.17'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
6	60+02.17'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
6	60+53.17'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
6	61+04.17'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
6	61+55.17'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
6	65+01.78'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%

SE REGION	STATION	DESCRIPTION	LEFT OUTSIDE SHOULDER	LEFT OUTSIDE LANE	RIGHT OUTSIDE LANE	RIGHT OUTSIDE SHOULDER
6	65+52.78'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
6	66+03.78'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
6	66+54.78'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
6	67+05.78'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
6	67+05.78'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
7	76+96.89'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
7	76+96.89'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
7	77+47.89'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
7	77+98.89'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
7	78+49.89'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
7	79+00.89'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
7	85+62.35'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
7	86+13.35'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
7	86+64.35'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
7	87+15.35'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
7	87+66.35'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
7	87+66.35'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
8	89+11.11'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
8	89+11.11'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
8	89+62.31'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
8	90+13.51'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
8	90+64.71'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
8	90+90.31'	BEGIN FULL SUPER	5.00%	5.00%	-5.00%	-5.00%
8	91+91.78'	END FULL SUPER	5.00%	5.00%	-5.00%	-5.00%
8	92+17.38'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
8	92+68.58'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
8	93+19.78'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
8	93+70.98'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
8	93+70.98'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
9	93+90.98'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
9	93+90.98'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
9	94+41.82'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
9	94+92.65'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
9	95+02.82'	BEGIN FULL SUPER	-4.00%	-2.40%	2.40%	2.40%
9	97+78.48'	END FULL SUPER	-4.00%	-2.40%	2.40%	2.40%
10	100+70.75'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
10	101+34.40'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
11	102+36.39'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
11	104+07.55'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
12	106+38.14'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
12	108+57.08'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
12	109+08.08'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
12	109+59.08'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
12	110+10.08'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
12	110+61.08'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
12	110+61.08'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%

SUPERELEVATION DATA IS FOR INFORMATION PURPOSES ONLY AND IS ASSUMED TO NOT MATCH EXISTING CONDITIONS. PAVING OPERATIONS SHALL UTILIZE MAXIMUM ALLOWABLE SLOPE CORRECTION TO MATCH SUPERELEVATION DATA TO THE GREATEST EXTENT POSSIBLE.

SE REGION	STATION	DESCRIPTION	LEFT OUTSIDE SHOULDER	LEFT OUTSIDE LANE	RIGHT OUTSIDE LANE	RIGHT OUTSIDE SHOULDER
13	112+26.21'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
13	112+26.21'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
13	112+77.21'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
13	113+28.21'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
13	113+79.21'	BEGIN FULL SUPER	-4.00%	-4.00%	4.00%	4.00%
13	115+77.39'	END FULL SUPER	-4.00%	-4.00%	4.00%	4.00%
13	116+28.39'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
13	116+79.39'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
13	117+30.39'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
13	117+30.39'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
14	122+45.74'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
14	122+45.74'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
14	122+96.97'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
14	123+48.20'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
14	123+99.42'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
14	124+42.97'	BEGIN FULL SUPER	-5.70%	-5.70%	5.70%	5.70%
14	137+47.48'	END FULL SUPER	-5.70%	-5.70%	5.70%	5.70%
14	137+91.02'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
14	138+42.25'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
14	138+93.48'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
14	139+44.70'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
14	139+44.70'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
15	141+41.54'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
15	141+41.54'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
15	141+92.54'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
15	142+43.54'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
15	142+94.54'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
15	143+45.54'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
15	150+62.60'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
16	152+77.96'	BEGIN FULL SUPER	5.50%	5.50%	-5.50%	-5.50%
16	160+57.50'	END FULL SUPER	5.50%	5.50%	-5.50%	-5.50%
16	160+95.68'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
16	161+46.59'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
16	161+97.50'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
16	162+48.41'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
16	162+48.41'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
17	165+71.15'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
17	165+71.15'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
17	166+21.98'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
17	166+72.82'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
17	166+82.98'	BEGIN FULL SUPER	2.40%	2.40%	-2.40%	-4.00%
17	169+24.56'	END FULL SUPER	2.40%	2.40%	-2.40%	-4.00%
17	169+34.73'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
17	169+85.56'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
17	170+36.40'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
17	170+36.40'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%

SE REGION	STATION	DESCRIPTION	LEFT OUTSIDE SHOULDER	LEFT OUTSIDE LANE	RIGHT OUTSIDE LANE	RIGHT OUTSIDE SHOULDER
18	170+92.03'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
18	170+92.03'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
18	171+43.03'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
18	171+94.03'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
18	172+45.03'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
18	172+96.03'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
18	175+21.95'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
19	177+08.96'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
19	180+98.98'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
19	181+49.98'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
19	182+00.98'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
19	182+51.98'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
19	183+02.98'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
19	183+02.98'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
20	185+50.71'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
20	185+50.71'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
20	186+01.54'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
20	186+52.37'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
20	186+62.54'	BEGIN FULL SUPER	-4.00%	-2.40%	2.40%	2.40%
20	188+00.34'	END FULL SUPER	-4.00%	-2.40%	2.40%	2.40%
20	188+10.50'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
20	188+61.34'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
20	189+12.17'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
20	189+12.17'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
21	193+10.49'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
21	193+10.49'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
21	193+61.56'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
21	194+12.63'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
21	194+63.70'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
21	195+04.56'	BEGIN FULL SUPER	5.60%	5.60%	-5.60%	-5.60%
21	206+14.94'	END FULL SUPER	5.60%	5.60%	-5.60%	-5.60%
22	208+19.71'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
22	209+49.24'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
22	210+00.24'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
22	210+51.24'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
22	211+02.24'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
22	211+53.24'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
22	211+53.24'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
23	213+81.06'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
23	213+81.06'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
23	214+32.26'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
23	214+83.46'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
23	215+34.66'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
23	215+60.26'	BEGIN FULL SUPER	5.00%	5.00%	-5.00%	-5.00%
23	219+45.29'	END FULL SUPER	5.00%	5.00%	-5.00%	-5.00%
23	219+70.89'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
23	220+22.09'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
23	220+73.29'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
23	221+24.49'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
23	221+24.49'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%

SE REGION	STATION	DESCRIPTION	LEFT OUTSIDE SHOULDER	LEFT OUTSIDE LANE	RIGHT OUTSIDE LANE	RIGHT OUTSIDE SHOULDER
24	232+44.54'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
24	232+44.54'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
24	232+95.54'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
24	233+46.54'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
24	233+97.54'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
24	234+48.54'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
24	237+91.38'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
25	240+86.52'	BEGIN FULL SUPER	5.70%	5.70%	-5.70%	-5.70%
25	252+04.73'	END FULL SUPER	5.70%	5.70%	-5.70%	-5.70%
26	253+04.39'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
26	256+12.07'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
26	256+63.07'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
26	257+14.07'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
26	257+65.07'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
26	258+16.07'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
26	258+16.07'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
27	258+25.11'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
27	258+25.11'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
27	258+59.47'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
27	258+93.83'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
27	259+26.47'	BEGIN FULL SUPER	-4.00%	-3.90%	3.90%	3.90%
27	262+37.12'	END FULL SUPER	-4.00%	-3.90%	3.90%	3.90%
28	263+58.75'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
28	264+63.84'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
28	264+98.18'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
28	265+32.51'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
28	265+66.84'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
28	266+01.18'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
28	266+01.18'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
29	266+30.75'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
29	266+30.75'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
29	266+64.90'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
29	266+99.04'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
29	267+33.19'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
29	267+34.90'	BEGIN FULL SUPER	4.10%	4.10%	-4.10%	-4.10%
29	268+23.73'	END FULL SUPER	4.10%	4.10%	-4.10%	-4.10%
29	268+25.44'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
29	268+59.58'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
29	268+93.73'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
29	269+27.87'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
29	269+27.87'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
30	286+91.50'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
30	286+91.50'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
30	287+25.86'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
30	287+60.22'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
30	287+83.26'	END FULL SUPER	-4.00%	-3.90%	3.90%	3.90%
30	287+92.86'	BEGIN FULL SUPER	-4.00%	-3.90%	3.90%	3.90%
31	289+79.14'	BEGIN FULL SUPER	-4.00%	-3.90%	3.90%	3.90%
31	290+27.73'	END FULL SUPER	-4.00%	-3.90%	3.90%	3.90%

SE REGION	STATION	DESCRIPTION	LEFT OUTSIDE SHOULDER	LEFT OUTSIDE LANE	RIGHT OUTSIDE LANE	RIGHT OUTSIDE SHOULDER
32	293+27.52'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
32	297+56.20'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
33	299+60.29'	BEGIN FULL SUPER	-5.50%	-5.50%	5.50%	5.50%
33	301+91.62'	END FULL SUPER	-5.50%	-5.50%	5.50%	5.50%
34	304+48.42'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
34	305+35.88'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
35	306+37.87'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
35	307+00.66'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
36	308+02.65'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
36	309+99.78'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
37	311+76.69'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
37	314+06.15'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
38	315+97.02'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
38	315+97.15'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
39	317+29.10'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
39	321+46.48'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
40	324+01.46'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
40	325+55.49'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
41	327+62.65'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
41	330+00.67'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
42	332+53.69'	BEGIN FULL SUPER	-5.70%	-5.70%	5.70%	5.70%
42	334+35.00'	END FULL SUPER	-5.70%	-5.70%	5.70%	5.70%
43	336+22.32'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
43	339+34.83'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
43	339+85.83'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
43	340+36.83'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
43	340+87.83'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
43	341+38.83'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
43	341+38.83'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
44	341+78.78'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
44	341+78.78'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
44	342+29.69'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
44	342+80.60'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
44	343+31.51'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
44	343+69.69'	BEGIN FULL SUPER	-5.50%	-5.50%	5.50%	5.50%
44	347+65.46'	END FULL SUPER	-5.50%	-5.50%	5.50%	5.50%
45	350+35.30'	BEGIN FULL SUPER	5.20%	5.20%	-5.20%	-5.20%
45	352+34.22'	END FULL SUPER	5.20%	5.20%	-5.20%	-5.20%
45	352+64.91'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
45	353+16.06'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
45	353+67.22'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
45	354+18.37'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
45	354+18.37'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%

SE REGION	STATION	DESCRIPTION	LEFT OUTSIDE SHOULDER	LEFT OUTSIDE LANE	RIGHT OUTSIDE LANE	RIGHT OUTSIDE SHOULDER
46	358+72.18'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
46	358+72.18'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
46	359+23.25'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
46	359+74.31'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
46	360+25.37'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
46	360+43.25'	BEGIN FULL SUPER	4.70%	4.70%	-4.70%	-4.70%
46	364+35.75'	END FULL SUPER	4.70%	4.70%	-4.70%	-4.70%
46	364+53.62'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
46	365+04.69'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
46	365+55.75'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
46	366+06.81'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
46	366+06.81'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
47	372+39.38'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
47	372+39.38'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
47	372+90.38'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
47	373+41.38'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
47	373+92.38'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
47	374+43.38'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
47	375+68.87'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
48	378+92.33'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
48	382+81.69'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
49	384+53.22'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
49	386+30.26'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
49	386+81.26'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
49	387+32.26'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
49	387+83.26'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
49	388+34.26'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
49	388+34.26'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
50	393+18.84'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
50	393+18.84'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
50	393+69.84'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
50	394+20.84'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
50	394+71.84'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
50	395+22.84'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
50	398+28.74'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
51	401+26.90'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
51	402+39.06'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
52	403+41.05'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
52	406+08.76'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
53	408+96.57'	BEGIN FULL SUPER	4.70%	4.70%	-4.70%	-4.70%
53	413+11.77'	END FULL SUPER	4.70%	4.70%	-4.70%	-4.70%
53	413+29.64'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
53	413+80.70'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
53	414+31.77'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
53	414+82.83'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
53	414+82.83'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%

SE REGION	STATION	DESCRIPTION	LEFT OUTSIDE SHOULDER	LEFT OUTSIDE LANE	RIGHT OUTSIDE LANE	RIGHT OUTSIDE SHOULDER
54	415+61.98'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
54	415+61.98'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
54	416+12.98'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
54	416+63.98'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
54	417+14.98'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
54	417+65.98'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
54	428+88.55'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
55	430+97.01'	BEGIN FULL SUPER	-5.70%	-5.70%	5.70%	5.70%
55	439+32.68'	END FULL SUPER	-5.70%	-5.70%	5.70%	5.70%
55	439+76.23'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
55	440+27.45'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
55	440+78.68'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
55	441+29.91'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
55	441+29.91'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
56	451+93.85'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
56	451+93.85'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
56	452+44.85'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
56	452+95.85'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
56	453+97.85'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
56	454+43.37'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
57	455+45.36'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
57	458+43.91'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
58	459+45.24'	BEGIN FULL SUPER	-5.90%	-5.90%	5.90%	5.90%
58	460+08.55'	END FULL SUPER	-5.90%	-5.90%	5.90%	5.90%
58	460+57.18'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
58	461+08.37'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
58	461+59.55'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
58	462+10.74'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
58	462+10.74'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
59	465+88.28'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
59	465+88.28'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
59	466+39.28'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
59	466+90.28'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
59	467+41.28'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
59	467+92.28'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
59	470+53.37'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
60	471+55.35'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
60	471+97.83'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
60	472+99.83'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
60	473+50.83'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
60	474+01.83'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
60	474+01.83'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%

SE REGION	STATION	DESCRIPTION	LEFT OUTSIDE SHOULDER	LEFT OUTSIDE LANE	RIGHT OUTSIDE LANE	RIGHT OUTSIDE SHOULDER
61	479+38.86'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
61	479+38.86'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
61	479+89.73'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
61	480+40.60'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
61	480+91.47'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
61	481+06.73'	BEGIN FULL SUPER	4.60%	4.60%	-4.60%	-4.60%
61	496+71.79'	END FULL SUPER	4.60%	4.60%	-4.60%	-4.60%
61	496+87.05'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
61	497+37.92'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
61	497+88.79'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
61	498+39.65'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
61	498+39.65'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
62	510+26.12'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
62	510+26.12'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
62	510+77.28'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
62	511+28.43'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
62	511+79.59'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
62	512+10.28'	BEGIN FULL SUPER	5.20%	5.20%	-5.20%	-5.20%
62	518+11.65'	END FULL SUPER	5.20%	5.20%	-5.20%	-5.20%
62	518+42.34'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
62	518+93.49'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
62	519+44.65'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
62	519+95.80'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
62	519+95.80'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
63	526+82.65'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
63	526+82.65'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
63	527+33.85'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
63	527+85.05'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
63	528+36.25'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
63	528+61.85'	BEGIN FULL SUPER	-5.00%	-5.00%	5.00%	5.00%
63	534+54.45'	END FULL SUPER	-5.00%	-5.00%	5.00%	5.00%
63	534+80.05'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
63	535+31.25'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
63	535+82.45'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
63	536+33.65'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
63	536+33.65'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
64	542+36.75'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
64	542+36.75'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
64	542+88.00'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
64	543+39.25'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
64	543+90.50'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
64	544+11.00'	BEGIN FULL SUPER	4.80%	4.80%	-4.80%	-4.80%
64	549+28.88'	END FULL SUPER	4.80%	4.80%	-4.80%	-4.80%
65	552+62.09'	BEGIN FULL SUPER	-5.00%	-5.00%	5.00%	5.00%
65	559+39.14'	END FULL SUPER	-5.00%	-5.00%	5.00%	5.00%
65	559+64.74'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
65	560+15.94'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
65	560+67.14'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
65	561+18.34'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
65	561+18.34'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%

SE REGION	STATION	DESCRIPTION	LEFT OUTSIDE SHOULDER	LEFT OUTSIDE LANE	RIGHT OUTSIDE LANE	RIGHT OUTSIDE SHOULDER
66	563+41.13'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
66	563+41.13'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
66	563+92.04'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
66	564+42.95'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
66	564+93.86'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
66	565+04.04'	BEGIN FULL SUPER	4.40%	4.40%	-4.40%	-4.40%
66	576+33.52'	END FULL SUPER	4.40%	4.40%	-4.40%	-4.40%
66	576+43.70'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
66	576+94.61'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
66	577+45.52'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
66	577+96.42'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
66	577+96.42'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
67	593+17.13'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
67	593+17.13'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
67	593+68.13'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
67	594+19.13'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
67	594+70.13'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
67	595+21.13'	BEGIN FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
67	598+32.37'	END FULL SUPER	-6.00%	-6.00%	6.00%	6.00%
68	601+84.63'	BEGIN FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
68	603+60.92'	END FULL SUPER	6.00%	6.00%	-6.00%	-6.00%
68	604+11.92'	LOW SHOULDER MATCH	4.00%	4.00%	-4.00%	-4.00%
68	604+62.92'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
68	605+13.92'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
68	605+64.92'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
68	605+64.92'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
69	612+56.89'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
69	612+56.89'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
69	613+08.04'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
69	613+59.19'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
69	614+10.35'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
69	614+41.04'	BEGIN FULL SUPER	-5.20%	-5.20%	5.20%	5.20%
69	616+42.98'	END FULL SUPER	-5.20%	-5.20%	5.20%	5.20%
69	616+73.67'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
69	617+24.83'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
69	617+75.98'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
69	618+27.14'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
69	618+27.14'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
69	618+54.31'	END ALIGNMENT	-4.00%	-2.00%	-2.00%	-4.00%

Estimate Of Quantities

5790-02-72

Line	Item	Item Description	Unit	Total	Qty
0002	203.0100	Removing Small Pipe Culverts	EACH	32.000	32.000
0004	204.0100	Removing Concrete Pavement	SY	1,998.000	1,998.000
0006	204.0109.S	Removing Concrete Surface Partial Depth	SF	89,846.000	89,846.000
0008	204.0110	Removing Asphaltic Surface	SY	261.000	261.000
0010	204.0115	Removing Asphaltic Surface Butt Joints	SY	690.000	690.000
0012	204.0120	Removing Asphaltic Surface Milling	SY	764.000	764.000
0014	204.0150	Removing Curb & Gutter	LF	1,375.000	1,375.000
0016	204.0155	Removing Concrete Sidewalk	SY	689.000	689.000
0018	204.0165	Removing Guardrail	LF	14,279.000	14,279.000
0020	204.0190	Removing Surface Drains	EACH	1.000	1.000
0022	204.0220	Removing Inlets	EACH	11.000	11.000
0024	204.0270	Abandoning Culvert Pipes	EACH	1.000	1.000
0026	205.0100	Excavation Common	CY	3,286.000	3,286.000
0028	205.0200	Excavation Rock	CY	354.000	354.000
0030	208.0100	Borrow	CY	1,310.000	1,310.000
0032	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 5790-02-72	EACH	1.000	1.000
0034	305.0110	Base Aggregate Dense 3/4-Inch	TON	5,531.000	5,531.000
0036	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	13,743.000	13,743.000
0038	311.0110	Breaker Run	TON	2,201.000	2,201.000
0040	330.0100	Mill and Relay	SY	170,564.000	170,564.000
0042	374.1010.S	QMP Mill and Relay Compaction	SY	170,439.000	170,439.000
0044	390.0100	Removing Pavement for Base Patching	CY	199.000	199.000
0046	390.0305	Base Patching Concrete HES	CY	199.000	199.000
0048	416.0610	Drilled Tie Bars	EACH	671.000	671.000
0050	416.0620	Drilled Dowel Bars	EACH	1,295.000	1,295.000
0052	455.0605	Tack Coat	GAL	10,022.000	10,022.000
0054	460.0105.S	HMA Percent Within Limits (PWL) Test Strip Volumetrics	EACH	2.000	2.000
0056	460.0110.S	HMA Percent Within Limits (PWL) Test Strip Density	EACH	2.000	2.000
0058	460.4210.S	HMA Pavement Interlayer	TON	571.000	571.000
0060	460.5224	HMA Pavement 4 LT 58-28 S	TON	32,843.000	32,843.000
0062	460.6225	HMA Pavement 5 MT 58-28 S	TON	910.000	910.000
0064	465.0105	Asphaltic Surface	TON	254.000	254.000
0066	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	217.000	217.000
0068	465.0125	Asphaltic Surface Temporary	TON	173.000	173.000
0070	465.0560	Asphaltic Rumble Strips, Centerline	LF	48,830.000	48,830.000
0072	509.5100.S	Polymer Overlay	SY	858.000	858.000
0074	509.9015.S	Removing Polymer Overlay (structure) 01. B-12-137	SY	858.000	858.000
0076	520.8000	Concrete Collars for Pipe	EACH	4.000	4.000
0078	520.8700	Cleaning Culvert Pipes	EACH	3.000	3.000
0080	521.1018	Apron Endwalls for Culvert Pipe Steel 18-Inch	EACH	2.000	2.000
0082	521.1024	Apron Endwalls for Culvert Pipe Steel 24-Inch	EACH	1.000	1.000
0084	521.3118	Culvert Pipe Corrugated Steel 18-Inch	LF	26.000	26.000
0086	522.0418	Culvert Pipe Reinforced Concrete Class IV 18-Inch	LF	20.000	20.000
0088	522.0424	Culvert Pipe Reinforced Concrete Class IV 24-Inch	LF	130.000	130.000
0090	522.1012	Apron Endwalls for Culvert Pipe Reinforced Concrete 12-Inch	EACH	1.000	1.000
0092	522.1015	Apron Endwalls for Culvert Pipe Reinforced Concrete 15-Inch	EACH	1.000	1.000
0094	522.1018	Apron Endwalls for Culvert Pipe Reinforced Concrete 18-Inch	EACH	7.000	7.000
0096	522.1024	Apron Endwalls for Culvert Pipe Reinforced Concrete 24-Inch	EACH	30.000	30.000
0098	522.1030	Apron Endwalls for Culvert Pipe Reinforced Concrete 30-Inch	EACH	3.000	3.000

Estimate Of Quantities

5790-02-72

Line	Item	Item Description	Unit	Total	Qty
0100	601.0409	Concrete Curb & Gutter 30-Inch Type A	LF	656.000	656.000
0102	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	779.000	779.000
0104	601.0555	Concrete Curb & Gutter 6-Inch Sloped 36-Inch Type A	LF	25.000	25.000
0106	601.0576	Concrete Curb & Gutter 4-Inch Sloped 30-Inch Type J	LF	20,786.000	20,786.000
0108	601.0600	Concrete Curb Pedestrian	LF	101.000	101.000
0110	602.0405	Concrete Sidewalk 4-Inch	SF	7,752.000	7,752.000
0112	602.0415	Concrete Sidewalk 6-Inch	SF	420.000	420.000
0114	602.0515	Curb Ramp Detectable Warning Field Natural Patina	SF	547.500	547.500
0116	602.0810	Concrete Driveway 6-Inch	SY	24.000	24.000
0118	602.3010	Concrete Surface Drains	CY	24.000	24.000
0120	606.0200	Riprap Medium	CY	48.000	48.000
0122	606.0300	Riprap Heavy	CY	398.000	398.000
0124	608.0005	Storm Sewer Rock Excavation	CY	48.000	48.000
0126	608.0412	Storm Sewer Pipe Reinforced Concrete Class IV 12-Inch	LF	587.000	587.000
0128	608.0415	Storm Sewer Pipe Reinforced Concrete Class IV 15-Inch	LF	35.000	35.000
0130	608.0418	Storm Sewer Pipe Reinforced Concrete Class IV 18-Inch	LF	289.000	289.000
0132	608.0424	Storm Sewer Pipe Reinforced Concrete Class IV 24-Inch	LF	943.000	943.000
0134	608.0430	Storm Sewer Pipe Reinforced Concrete Class IV 30-Inch	LF	122.000	122.000
0136	611.0612	Inlet Covers Type C	EACH	1.000	1.000
0138	611.0624	Inlet Covers Type H	EACH	4.000	4.000
0140	611.0630	Inlet Covers Type HM-GJ	EACH	5.000	5.000
0142	611.0633	Inlet Covers Type HM-GJ-S	EACH	2.000	2.000
0144	611.0639	Inlet Covers Type H-S	EACH	3.000	3.000
0146	611.0642	Inlet Covers Type MS	EACH	51.000	51.000
0148	611.0645	Inlet Covers Type MS-A	EACH	2.000	2.000
0150	611.2005	Manholes 5-FT Diameter	EACH	1.000	1.000
0152	611.3004	Inlets 4-FT Diameter	EACH	1.000	1.000
0154	611.3230	Inlets 2x3-FT	EACH	13.000	13.000
0156	611.3901	Inlets Median 1 Grate	EACH	5.000	5.000
0158	611.3902	Inlets Median 2 Grate	EACH	24.000	24.000
0160	611.8120.S	Cover Plates Temporary	EACH	11.000	11.000
0162	612.0902.S	Insulation Board Polystyrene (inch) 01. 2-Inch	SY	9.000	9.000
0164	614.0397	Guardrail Mow Strip Emulsified Asphalt	SY	1,627.000	1,627.000
0166	614.2300	MGS Guardrail 3	LF	9,974.500	9,974.500
0168	614.2330	MGS Guardrail 3 K	LF	3,712.500	3,712.500
0170	614.2350	MGS Guardrail Short Radius	LF	152.000	152.000
0172	614.2500	MGS Thrie Beam Transition	LF	79.000	79.000
0174	614.2610	MGS Guardrail Terminal EAT	EACH	16.000	16.000
0176	618.0100	Maintenance and Repair of Haul Roads (project) 01. 5790-02-72	EACH	1.000	1.000
0178	619.1000	Mobilization	EACH	1.000	1.000
0180	624.0100	Water	MGAL	1,104.000	1,104.000
0182	625.0500	Salvaged Topsoil	SY	6,208.000	6,208.000
0184	627.0200	Mulching	SY	1,374.000	1,374.000
0186	628.1504	Silt Fence	LF	19,497.000	19,497.000
0188	628.1520	Silt Fence Maintenance	LF	38,993.000	38,993.000
0190	628.1905	Mobilizations Erosion Control	EACH	3.000	3.000
0192	628.1910	Mobilizations Emergency Erosion Control	EACH	4.000	4.000
0194	628.2004	Erosion Mat Class I Type B	SY	4,834.000	4,834.000
0196	628.7005	Inlet Protection Type A	EACH	52.000	52.000

Estimate Of Quantities

5790-02-72

Line	Item	Item Description	Unit	Total	Qty
0198	628.7010	Inlet Protection Type B	EACH	4.000	4.000
0200	628.7015	Inlet Protection Type C	EACH	17.000	17.000
0202	628.7504	Temporary Ditch Checks	LF	200.000	200.000
0204	628.7555	Culvert Pipe Checks	EACH	24.000	24.000
0206	629.0210	Fertilizer Type B	CWT	4.000	4.000
0208	630.0120	Seeding Mixture No. 20	LB	88.000	88.000
0210	630.0140	Seeding Mixture No. 40	LB	27.000	27.000
0212	630.0200	Seeding Temporary	LB	170.000	170.000
0214	630.0500	Seed Water	MGAL	143.000	143.000
0216	634.0612	Posts Wood 4x6-Inch X 12-FT	EACH	21.000	21.000
0218	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	23.000	23.000
0220	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	3.000	3.000
0222	634.0618	Posts Wood 4x6-Inch X 18-FT	EACH	14.000	14.000
0224	637.2230	Signs Type II Reflective F	SF	77.250	77.250
0226	638.2102	Moving Signs Type II	EACH	53.000	53.000
0228	638.3000	Removing Small Sign Supports	EACH	54.000	54.000
0230	642.5001	Field Office Type B	EACH	1.000	1.000
0232	643.0300	Traffic Control Drums	DAY	2,478.000	2,478.000
0234	643.0420	Traffic Control Barricades Type III	DAY	1,003.000	1,003.000
0236	643.0500	Traffic Control Flexible Tubular Marker Posts	EACH	30.000	30.000
0238	643.0600	Traffic Control Flexible Tubular Marker Bases	EACH	30.000	30.000
0240	643.0705	Traffic Control Warning Lights Type A	DAY	1,564.000	1,564.000
0242	643.0715	Traffic Control Warning Lights Type C	DAY	222.000	222.000
0244	643.0900	Traffic Control Signs	DAY	17,853.000	17,853.000
0246	643.0920	Traffic Control Covering Signs Type II	EACH	34.000	34.000
0248	643.1050	Traffic Control Signs PCMS	DAY	56.000	56.000
0250	643.1070	Traffic Control Cones 42-Inch	DAY	1,270.000	1,270.000
0252	643.3165	Temporary Marking Line Paint 6-Inch	LF	373,634.000	373,634.000
0254	643.5000	Traffic Control	EACH	1.000	1.000
0256	644.1410	Temporary Pedestrian Surface Asphalt	SF	118.000	118.000
0258	644.1430	Temporary Pedestrian Surface Plate	SF	50.000	50.000
0260	644.1601	Temporary Pedestrian Curb Ramp	DAY	8.000	8.000
0262	644.1810	Temporary Pedestrian Barricade	LF	645.000	645.000
0264	645.0130	Geotextile Type R	SY	288.000	288.000
0266	646.2020	Marking Line Epoxy 6-Inch	LF	854.000	854.000
0268	646.2040	Marking Line Grooved Wet Ref Epoxy 6-Inch	LF	233,519.000	233,519.000
0270	646.6120	Marking Stop Line Epoxy 18-Inch	LF	38.000	38.000
0272	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	2,012.000	2,012.000
0274	648.0100	Locating No-Passing Zones	MI	11.660	11.660
0276	650.4000	Construction Staking Storm Sewer	EACH	80.000	80.000
0278	650.4500	Construction Staking Subgrade	LF	502.000	502.000
0280	650.5000	Construction Staking Base	LF	650.000	650.000
0282	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	1,736.000	1,736.000
0284	650.6000	Construction Staking Pipe Culverts	EACH	1.000	1.000
0286	650.8000	Construction Staking Resurfacing Reference	LF	61,830.000	61,830.000
0288	650.9000	Construction Staking Curb Ramps	EACH	52.000	52.000
0290	650.9500	Construction Staking Sidewalk (project) 01. 5790-02-72	EACH	1.000	1.000
0292	650.9911	Construction Staking Supplemental Control (project) 01. 5790-02-72	EACH	1.000	1.000
0294	690.0150	Sawing Asphalt	LF	2,575.000	2,575.000

Estimate Of Quantities

5790-02-72

Line	Item	Item Description	Unit	Total	Qty
0296	690.0250	Sawing Concrete	LF	3,507.000	3,507.000
0298	740.0440	Incentive IRI Ride	DOL	23,400.000	23,400.000
0300	SPV.0055	Special 01. HMA Pavement PWL QMP, Core Only Project; Incentive Density PWL HMA Pavement	DOL	33,540.000	33,540.000
0302	SPV.0055	Special 02. Incentive Air Voids HMA Pavement	DOL	33,540.000	33,540.000
0304	SPV.0055	Special 03. Incentive Density HMA Pavement Longitudinal Joints	DOL	24,930.000	24,930.000
0306	SPV.0060	Special 01. Inlet Covers Flat Temporary	EACH	3.000	3.000
0308	SPV.0060	Special 02. Adjusting Sanitary Manhole Covers	EACH	11.000	11.000
0310	SPV.0060	Special 03. Adjust Water Valve Box	EACH	18.000	18.000
0312	SPV.0090	Special 01. Grading, Shaping, and Finishing for Concrete Curb and Gutter	LF	19,309.000	19,309.000
0314	SPV.0090	Special 02. Transverse Joint and Crack Plunge Milling and Patching	LF	624.000	624.000

3

3

		REMOVING ASPHALT ITEMS					
CATEGORY	STATION	204.0115 REMOVING ASPHALTIC SURFACE BUTT JOINTS	204.0110 REMOVING ASPHALTIC SURFACE	204.0120 REMOVING ASPHALTIC SURFACE MILLING	330.0100 MILL AND RELAY	*624.0100 WATER	NOTES
		SY	SY	SY	SY	MGAL	
0010	STA 2+23 - STA 264+62	-	-	-	74444	298	MAINLINE
	STA 29000 STA 61779	-	-	-	92836	372	MAINLINE
	STA 80022_80357	-	-	-	851	4	
	STA 223	23	-	-	-	-	STH 171 EGINNING
	STA 495 LT	25	-	-	-	-	3RD ST
	STA 660 LT	23	-	-	-	-	DIVISION ST
	STA 3445 RT	22	-	-	-	-	N HALLS RANCH RD
	STA 3986 LT	19	-	-	-	-	REEMAN RD
	STA 8079 RT	14	-	-	-	-	OLD GAS RD
	STA 19682 LT	16	-	-	-	-	STEVENSON RD
	STA 25134 LT	16	-	-	-	-	W POINT RD
	STA 25692 RT	20	-	-	-	-	OLD GAS RD
	STA 26442 RT	21	-	-	-	-	W RIVER RD
	STA 26462	28	-	-	-	-	WEST SIDE END
	STA 27274 LT	30	16	-	-	-	POST ST NORTH SIDE
	STA 27274 RT	21	24	-	-	-	POST ST SOUTH SIDE
	STA 27645 LT	30	10	-	-	-	GA ST NORTH SIDE
	STA 27645 RT	31	17	-	-	-	GA ST SOUTH SIDE
	STA 28075 LT	24	24	-	-	-	RAILROAD ST NORTH SIDE
	STA 28065 RT	12	10	-	-	-	RAILROAD ST SOUTH SIDE
	STA 28435 LT	18	25	-	-	-	REECCA ST NORTH SIDE
	STA 28434 RT	23	12	-	-	-	REECCA ST SOUTH SIDE
	STA 29000	30	-	-	-	-	EAST SIDE EGIN
	STA 29000 STA 29130 LT	-	123	-	-	-	ASPHALT SHOULDER PAVEMENT
	STA 29157 LT	19	-	-	-	-	STH 171 MATCH IN
	STA 39997 RT	17	-	-	-	-	S RAN DR
	STA 48514 LT	20	-	-	-	-	DEL LA MATER RD
	STA 58254 RT	30	-	-	-	-	SAND CREE RD
	STA 59744 RT	24	-	-	-	-	OLD 61 RD
	STA 61755	24	-	-	-	-	STH 171 END
	STA 725 RT	-	-	232	-	-	DRIVEWA
	STA 942 LT	-	-	28	-	-	DRIVEWA
	STA 1950 LT	-	-	54	-	-	DRIVEWA
	STA 2058 LT	-	-	21	-	-	DRIVEWA
	STA 4185 LT	-	-	30	-	-	DRIVEWA
	STA 20960 LT	-	-	43	-	-	DRIVEWA
	STA 25300 RT	-	-	29	-	-	DRIVEWA
	STA 25400 RT	-	-	26	-	-	DRIVEWA
	STA 25600 RT	-	-	18	-	-	DRIVEWA
	STA 29810 LT	-	-	46	-	-	DRIVEWA
	STA 34570 LT	-	-	126	-	-	DRIVEWA
	STA 52200 LT	-	-	27	-	-	DRIVEWA
	STA 54858 LT	-	-	35	-	-	DRIVEWA
	STA 60037 LT	-	-	49	-	-	DRIVEWA
	STA 38298 C STA 39241 C	17	-	-	2307	10	STH 131
	STA 38341 C LT	31	-	-	-	-	SCHOOL ST
	STA 38662 C LT	27	-	-	-	-	PAR ST
	STA 39000 C LT	35	-	-	-	-	ORIN ST
	SUTOTAL CATEGOR 0010	690	261	764	170438	684	
	PROJECT TOTALS	690	261	764	170564	684	
	ADDITIONAL UANTITIES SHOWN ELSEWHERE						

PROJECT NO 57900272

HW STH 171

COUNT CRAWORD

MISCELLANEOUS UANTITIES

SHEET

E

3

3

REMOVING CONCRETE ITEMS							
CATEGOR	STATION	LOCATION	2040100	2040109S	2040150	2040155	COMMENT
			REMOVING CONCRETE PAVEMENT	REMOVING CONCRETE SURACE PARTIAL DEPTH	REMOVING CUR GUTTER	REMOVING CONCRETE SIDEWAL	
			S	S	L	S	
0010	STA 223 STA 289	RT	-	-	66	-	MT STERLING IN RONT O POST OICE
	STA 26887 STA 28702	MAINLINE	-	84626	-	-	GAS MILLS DOWNTOWN
	STA 29000 STA 29130	MAINLINE	-	5220	-	-	
	STA 26409	RT	-	-	27	17	CUR RAMP WEST O RIDGE
	STA 26424	LT	-	-	29	28	CUR RAMP WEST O RIDGE
	STA 26757	LT	-	-	21	15	RO PAR CROSSING N
	STA 26773	RT	-	-	16	4	RO PAR CROSSING S
	STA 26909	RT	-	-	17	-	CUR RAMP W ORIN ST SW UAD
	STA 26940	RT	-	-	27	13	CUR RAMP W ORIN ST SE UAD
	STA 26955	LT	-	-	17	13	CUR RAMP W ORIN ST NE UAD
	STA 27245	LT	-	-	23	19	CUR RAMPS POST ST NW UAD
	STA 27245	RT	-	-	34	23	CUR RAMPS POST ST SW UAD
	STA 27310	RT	-	-	17	27	CUR RAMPS POST ST SE UAD
	STA 27310	LT	-	-	31	17	CUR RAMPS POST ST NE UAD
	STA 27610	LT	-	-	38	26	CUR RAMPS GA ST NW UAD
	STA 27610	RT	-	-	40	30	CUR RAMPS GA ST SW UAD
	STA 27675	LT	-	-	30	23	CUR RAMPS GA ST SE UAD
	STA 27675	RT	-	-	34	24	CUR RAMPS GA ST NE UAD
	STA 28048	LT	-	-	27	17	CUR RAMPS RAILROAD ST NW UAD
	STA 28048	RT	-	-	24	13	CUR RAMPS RAILROAD ST SW UAD
	STA 28080	RT	-	-	42	26	CUR RAMP RAILROAD ST SE UAD
	STA 28100	LT	-	-	29	19	CUR RAMP RAILROAD ST NE UAD
	STA 28408	LT	-	-	10	19	CUR RAMPS REECCA ST NW UAD
	STA 28408	RT	-	-	26	17	CUR RAMPS REECCA ST SW UAD
	STA 28462	LT	-	-	25	11	CUR RAMPS REECCA ST SE UAD
	STA 28462	RT	-	-	25	15	CUR RAMPS REECCA ST NE UAD
	STA 28702	LT	-	-	-	99	CUR RAMP SIDEWAL NORTH SIDE TILL END O WAL AT EAST
	STA 28702 STA 28780	RT	-	-	-	38	CUR RAMP STH 171 SIDEWAL
	STA 28884	RT	-	-	-	56	CUR RAMP STH 171 STH 131 SOUTH CORNER
	STA 28885	LT	-	-	-	56	CUR RAMPS STH 171 STH 131 EAST CORNER
	STA 28702 STA 29000	-	1998	-	653	-	STH 171 STH 131 INTERSECT RECONSTRUCT AREA
	STA 38320 C	LT	-	-	11	7	CUR RAMP SOUTH SIDE SCHOOL ST
	STA 38371 C	LT	-	-	36	17	CUR RAMP NORTH SIDE SCHOOL ST
	STA 38372 C	RT	-	-	-	-	CUR RAMP RT SIDE STH 131 SIDEWAL
	SUTOTAL CATEGOR 0010		1998	89846	1375	689	
	PROJECT TOTALS		1998	89846	1375	689	

REMOVING GUARDRAIL				
CATEGOR	STATION	LOCATION	2040165 REMOVING GUARDRAIL	
			L	
0010	STA 8156 STA 11754	LT	3596	
	STA 12121 STA 12575	LT	454	
	STA 12909 STA 14045	LT	1127	
	STA 14226 STA 16795	LT	2589	
	STA 17000 STA 19667	LT	2700	
	STA 19695 STA 20488	LT	820	
	STA 25716 STA 26073	LT	354	
	STA 29879 STA 32088	RT	2251	
	STA 32094 STA 32186	LT	94	
	STA 32329 STA 326312	LT	294	
	PROJECT TOTALS		14279	

3

REMOVING PIPES AND STORM SEWER ITEMS						
CATEGOR	STATION	2030100 REMOVING SMALL PIPE CULVERTS EACH	2040190 REMOVING SURFACE DRAINS EACH	2040220 REMOVING INLETS EACH	2040270 AANDONING CULVERT PIPES EACH	COMMENT
0010	STA 9091	1	-	-	-	CP 6
	STA 9514	1	-	1	-	CP 7
	STA 9862 15 RT	1	-	1	-	CP 8
	STA 10485	1	-	-	-	CP 9 24INCH
	STA 10753	1	-	1	-	CP 10 24INCH
	STA 12150	1	-	-	-	CP 12
	STA 12965	1	-	-	-	CP 14 24INCH
	STA 15767	1	-	1	-	CP 18 30INCH
	STA 17390	-	-	-	1	CP 21 24 SIE
	STA 17972 138 RT	-	-	1	-	CP 22
	STA 19700 15 RT	-	1	-	-	AREA DRAIN
	STA 18580	1	-	1	-	CP 23
	STA 19196	1	-	1	-	CP 24
	STA 20117	1	-	1	-	CP 26 24INCH
	STA 22477	1	-	-	-	CP 28
	STA 25865	1	-	-	-	CP 33 24INCH
	STA 26105	1	-	1	-	CP 34
	STA 31961 LT	1	-	-	-	CP 40 18INCH
	STA 32606	1	-	1	-	CP 40A
	STA 32681 LT	1	-	-	-	CP 41 18INCH
	STA 33568	1	-	-	-	CP 42
	STA 34482	1	-	-	-	CP 44 24INCH
	STA 36829	1	-	-	-	CP 45 24INCH
	STA 39488	1	-	-	-	CP 46 24INCH
	STA 40000	1	-	-	-	CP 47
	STA 40182	1	-	-	-	CP 48 24INCH
	STA 40802	1	-	-	-	CP 49
	STA 42687	1	-	-	-	CP 50 24INCH
	STA 47225	1	-	-	-	CP 52 24INCH
	STA 50289	1	-	-	-	CP 54 24INCH
	STA 52246	1	-	-	-	CP 56
	STA 53798	1	-	-	-	CP 58 24INCH
	STA 55637	1	-	1	-	CP 60 24INCH
	STA 57352	1	-	-	-	CP 62
	STA 61117	1	-	-	-	CP 63
PROJECT TOTALS		32	1	11	1	

BASE AGGREGATE DENSE (1 OF 3)

CATEGORY	STATION	LOCATION	305.0110	305.0120	311.0110		REMARKS
			BASE	BASE	BREAKER	*624.0100	
			AGGREGATE	AGGREGATE	TON	MGAL	
			DENSE	DENSE	3/4-INCH	1 1/4-INCH	
0010	STA 2+23 - STA 263+25	VARIES	312	-	-	5	GUARDRAIL AREAS
	STA 2+23 - STA 263+25	MAINLINE	1537	-	-	24	SHOULDERS
	STA 263+25 - STA 382+89'C	VARIES	-	415	-	7	CURB RAMPS
	STA 291+12 - STA 617+55	VARIES	68	-	-	2	GUARDRAIL AREAS
	STA 291+12 - STA 617+55	MAINLINE	3053	-	-	46	SHOULDERS
STH 131		SHOULDERS	63	-	-	1	STH 131
STH 171 & STH 131 INTERSECTION		-	-	1529	2201	56	STH 171 & STH 131 INTERSECTION
STH 171 & STH 131 INTERSECTION		-	17	311	-	5	INTER. CONSTRUCTION - STAGE 1
STH 171 & STH 131 INTERSECTION		-	14	236	-	4	INTER. CONSTRUCTION - STAGE 2
STA 391+23 C - STA 392+15 C		MAINLINE	4	278	-	5	STH 131 RECONSTRUCT MATCH IN
STA 2+24 - STA 4+50, LT		C&G	-	177	-	3	MT STERLING, 17' TO FLAG
STA 3+65 - STA 4+50, RT		C&G	-	61	-	1	MT STERLING, 17' TO FLAG
STA 84+60 - STA 91+00, RT		C&G	-	356	-	6	14' TO FLAG
STA 91+00 - STA 101+40, RT		C&G	-	501	-	8	13' TO FLAG
STA 101+40 - STA 105+86, RT		C&G	-	314	-	5	16' TO FLAG
STA 105+86 - STA 137+96, RT		C&G	-	1784	-	27	14' TO FLAG
STA 144+48 - STA 207+15, RT		C&G	-	3018	-	46	13' TO FLAG
STA 250+70 - STA 252+72, RT		C&G	-	98	-	2	13' TO FLAG
STA 258+62 - STA 263+26, RT		C&G	-	258	-	4	14' TO FLAG
STA 321+26 - STA 334+09, RT		C&G	-	618	-	10	13' TO FLAG
STA 334+03 - STA 349+59, LT		C&G	-	750	-	12	13' TO FLAG
STA 346+02 - STA 350+75, RT		C&G	-	263	-	4	14' TO FLAG
STA 350+75 - STA 352+89, RT		C&G	-	104	-	2	14' TO FLAG
STA 365+36 - STA 373+84, LT		C&G	-	346	-	6	12' TO FLAG
STA 378+92 - STA 384+19, RT		C&G	-	332	-	5	15' TO FLAG
STA 395+86 - STA 400+10, RT		C&G	-	205	-	4	13' TO FLAG
STA 405+02 - STA 408+12, LT		C&G	-	173	-	3	14' TO FLAG
STA 455+47 - STA 459+67, LT		C&G	-	265	-	4	15' TO FLAG
STA 464+84 - STA 470+24, LT		C&G	-	300	-	5	14' TO FLAG
STA 503+18 - STA 508+29, LT		C&G	-	284	-	5	14' TO FLAG
STA 503+18 - STA 508+30, RT		C&G	-	285	-	5	14' TO FLAG
STA 591+83 - STA 600+32, LT		C&G	-	409	-	7	13' TO FLAG
STA 13+00		LT	3	-	-	1	AGGREGATE DW
STA 14+77		LT	2.5	-	-	1	AGGREGATE DW
STA 18+50		RT	7	-	-	1	AGGREGATE DW
STA 24+72		RT	5	-	-	1	AGGREGATE DW
STA 52+95		RT	11	-	-	1	AGGREGATE DW
STA 58+02		RT	6	-	-	1	AGGREGATE DW
STA 69+00		LT	4	-	-	1	AGGREGATE DW
STA 69+79		RT	3	-	-	1	AGGREGATE DW
STA 72+74		LT	4	-	-	1	AGGREGATE DW
STA 74+14		RT	2.5	-	-	1	AGGREGATE DW
STA 74+46		LT	3	-	-	1	AGGREGATE DW
STA 117+88		LT	4.5	-	-	1	AGGREGATE DW
STA 141+72		LT	6	-	-	1	AGGREGATE DW
STA 167+20		RT	3	-	-	1	AGGREGATE DW

TABLE CONTINUED

3

BASE AGGREGATE DENSE (2 OF 3)

305.0110 305.0120
 BASE BASE
 AGGREGATE AGGREGATE 311.0110
 DENSE DENSE BREAKER *624.0100
3/4-INCH 1 1/4-INCH RUN WATER

CATEGORY	STATION	LOCATION	TON	TON	TON	MGAL	REMARKS
0010	STA 168+35	LT	6	-	-	1	AGGREGATE DW
	STA 210+90	LT	3.5	-	-	1	AGGREGATE DW
	STA 211+04	RT	2.5	-	-	1	AGGREGATE DW
	STA 225+00	RT	4	-	-	1	AGGREGATE DW
	STA 226+40	LT	3.5	-	-	1	AGGREGATE DW
	STA 235+00	RT	5	-	-	1	AGGREGATE DW
	STA 299+50	LT	6	-	-	1	AGGREGATE DW
	STA 335+93	RT	4	-	-	1	AGGREGATE DW
	STA 337+84	RT	4	-	-	1	AGGREGATE DW
	STA 353+47	LT	10.5	-	-	1	AGGREGATE DW
	STA 357+00	LT	14	-	-	1	AGGREGATE DW
	STA 360+31	RT	5	-	-	1	AGGREGATE DW
	STA 363+18	RT	11	-	-	1	AGGREGATE DW
	STA 364+88	LT	3	-	-	1	AGGREGATE DW
	STA 371+83	LT	4	-	-	1	AGGREGATE DW
	STA 374+40	RT	7	-	-	1	AGGREGATE DW
	STA 385+05	RT	9.5	-	-	1	AGGREGATE DW
	STA 386+38	LT	9.5	-	-	1	AGGREGATE DW
	STA 388+18	RT	3	-	-	1	AGGREGATE DW
	STA 389+52	RT	4.5	-	-	1	AGGREGATE DW
	STA 390+18	RT	5.5	-	-	1	AGGREGATE DW
	STA 392+15	RT	5	-	-	1	AGGREGATE DW
	STA 392+95	LT	2.5	-	-	1	AGGREGATE DW
	STA 395+25	RT	5.5	-	-	1	AGGREGATE DW
	STA 405+80	LT	2	-	-	1	AGGREGATE DW
	STA 406+00	RT	5.5	-	-	1	AGGREGATE DW
	STA 408+11	LT	4.5	-	-	1	AGGREGATE DW
	STA 413+11	LT	6	-	-	1	AGGREGATE DW
	STA 413+15	RT	5	-	-	1	AGGREGATE DW
	STA 422+81	RT	6.5	-	-	1	AGGREGATE DW
	STA 425+72	LT	4	-	-	1	AGGREGATE DW
	STA 427+57	LT	4.5	-	-	1	AGGREGATE DW
	STA 429+10	RT	3	-	-	1	AGGREGATE DW
	STA 434+76	RT	4	-	-	1	AGGREGATE DW
	STA 437+62	RT	4.5	-	-	1	AGGREGATE DW
	STA 440+55	LT	5.5	-	-	1	AGGREGATE DW
	STA 444+72	LT	5.5	-	-	1	AGGREGATE DW
	STA 450+84	RT	2.5	-	-	1	AGGREGATE DW
	STA 454+02	RT	3.5	-	-	1	AGGREGATE DW
	STA 459+88	LT	3.5	-	-	1	AGGREGATE DW
	STA 461+32	RT	4.5	-	-	1	AGGREGATE DW
	STA 464+23	RT	4.5	-	-	1	AGGREGATE DW
	STA 469+00	LT	11	-	-	1	AGGREGATE DW
	STA 477+00	RT	5.5	-	-	1	AGGREGATE DW
	STA 479+75	RT	2.5	-	-	1	AGGREGATE DW
	STA 481+15	RT	3	-	-	1	AGGREGATE DW

TABLE CONTINUED

BASE AGGREGATE DENSE (3 OF 3)

305.0110 305.0120
 BASE BASE
 AGGREGATE AGGREGATE 311.0110
 DENSE DENSE BREAKER *624.0100
3/4-INCH 1 1/4-INCH RUN WATER

CATEGORY	STATION	LOCATION	TON	TON	TON	MGAL	REMARKS
0010	STA 489+77	RT	3	-	-	1	AGGREGATE DW
	STA 494+90	LT	9	-	-	1	AGGREGATE DW
	STA 495+78	LT	5.5	-	-	1	AGGREGATE DW
	STA 515+00	LT	14	-	-	1	AGGREGATE DW
	STA 523+88	LT	3	-	-	1	AGGREGATE DW
	STA 527+50	RT	3.5	-	-	1	AGGREGATE DW
	STA 529+60	LT	7.5	-	-	1	AGGREGATE DW
	STA 530+60	LT	5.5	-	-	1	AGGREGATE DW
	STA 534+83	RT	5	-	-	1	AGGREGATE DW
	STA 555+58	RT	7	-	-	1	AGGREGATE DW
	STA 562+39	LT	3.5	-	-	1	AGGREGATE DW
	STA 564+00	LT	8.5	-	-	1	AGGREGATE DW
	STA 570+00	LT	8.5	-	-	1	AGGREGATE DW
	STA 573+00	LT	5.5	-	-	1	AGGREGATE DW
	STA 588+78	LT	3.5	-	-	1	AGGREGATE DW
	STA 590+92	RT	4	-	-	1	AGGREGATE DW
	STA 592+06	RT	4.5	-	-	1	AGGREGATE DW
	STA 593+89	RT	8	-	-	1	AGGREGATE DW
	STA 600+00	RT	10	-	-	1	AGGREGATE DW
	STA 602+58	RT	8.5	-	-	1	AGGREGATE DW
	STA 605+57	RT	6.5	-	-	1	AGGREGATE DW
	STA 385+09 C	LT	4	-	-	1	AGGREGATE ALLEYWAY
	STA 387+18 C	RT	5.5	-	-	1	AGGREGATE DW
	STA 388+05 C	LT	6	-	-	1	AGGREGATE DW
	STA 388+23 C	RT	2	-	-	1	AGGREGATE DW
	STA 388+69 C	RT	2	-	-	1	AGGREGATE DW
	STA 390+97 C	RT	3	-	-	1	AGGREGATE DW
	STA 391+66 C	LT	2.5	-	-	1	AGGREGATE ALLEYWAY
	STA 346+43 - STA 348+72	LT	-	53	-	1	PAVT PATCH MATCH-IN BEHIND C&G
	STA 505+82 - STA 506+79	LT	-	13	-	1	PAVT PATCH MATCH-IN BEHIND C&G
	STA 507+39 - STA 507+92	LT	-	7	-	1	PAVT PATCH MATCH-IN BEHIND C&G
SUBTOTAL CATEGORY 0010			5531	13743	2201	420	

PROJECT TOTAL

5531 13743 2201 420

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

CONCRETE SURFACE DRAINS

602.3010
 CONCRETE 606.0200 645.0130
 SURFACE RIPRAP GEOTEXTILE
DRAINS MEDIUM TYPE R

CATEGORY	STATION - STATION	CY	CY	SY	REMARKS
0010	STA 4+50, RT	1	2	12	CSD, END OF C&G RUN
	STA 107+52, RT	1	2	12	CSD, ANGLE FLUME AT 45 DEG. DRAINS TO INLET
	STA 121+50, RT	1	2	12	CSD, ANGLE FLUME AT 45 DEG. DRAINS TO INLET
	STA 124+31, RT	1	2	12	CSD
	STA 129+63, RT	1	2	12	CSD, ANGLE FLUME AT 45 DEG. DRAINS TO INLET
	STA 137+95, RT	1	2	12	CSD, END OF C&G RUN
	STA 144+48, RT	1	2	12	CSD, END OF C&G RUN
	STA 149+85, RT	1	2	12	CSD, ANGLE FLUME AT 45 DEG. DRAINS TO INLET
	STA 157+66, RT	1	2	12	CSD, ANGLE FLUME AT 45 DEG. DRAINS TO INLET
	STA 169+57, RT	1	2	12	CSD, DRAINS TO INLET
	STA 179+76, RT	1	2	12	CSD, ANGLE FLUME AT 45 DEG. DRAINS TO INLET
	STA 191+95, RT	1	2	12	CSD, ANGLE FLUME AT 45 DEG. DRAINS TO INLET
	STA 197+46, RT	1	2	12	CSD, ANGLE FLUME AT 45 DEG.
	STA 201+15, RT	1	2	12	CSD, ANGLE FLUME AT 45 DEG. DRAINS TO INLET
	STA 207+15, RT	1	2	12	CSD, END OF C&G RUN
	STA 252+76, RT	1	2	12	CSD, END OF C&G RUN
	STA 321+02, RT	1	2	12	CSD, END OF C&G RUN
	STA 326+08, RT	1	2	12	CSD, ANGLE FLUME AT 45 DEG. DRAINS TO INLET
	STA 334+00, LT	1	2	12	CSD, END OF C&G RUN
	STA 378+92, RT	1	2	12	CSD, END OF C&G RUN
	STA 399+93, RT	1	2	12	CSD, ANGLE FLUME, DRAINS TO INLET
	STA 467+40, LT	1	2	12	CSD, ANGLE FLUME AT 45 DEG. DRAINS TO INLET
	STA 508+29, LT	1	2	12	CSD, END OF C&G RUN
	STA 600+92, LT	1	2	12	CSD, END OF C&G RUN
SUBTOTAL CATEGORY 0010		24	48	288	
PROJECT TOTAL		24	48	288	

3

3

3

3

<u>ASPHALT PAVEMENT</u>											
CATEGORY	LOCATION	LOWER LAYER THICKNESS IN	UPPER LAYER THICKNESS IN	455.0605 TACK COAT GAL	460.5224	460.6225	460.4210.5	465.0105	465.0125	SPV.0090.02	REMARKS
					4 LT 58-28.5	5 MT 58-28.5	INTERLAYER	ASPHALTIC SURFACE	ASPHALTIC SURFACE	TRANSVERSE JOINT AND CRACK PLUNGE	
					TON	TON	TON	TON	TON	LF	
0010	STA 2+24 - 264+62	1.75	1.75	-	7316	-	-	-	-	-	RURAL - LOWER LAYER
	STA 2+24 - 264+62	1.75	1.75	3732	7316	-	-	-	-	-	RURAL - UPPER LAYER
	STA 268+90 - STA 287+02	1.00	1.25	555	-	-	444	-	-	-	URBAN - LOWER LAYER
	STA 268+90 - STA 287+02	1.00	1.25	397	-	555	-	-	-	-	URBAN - UPPER LAYER
	STA 268+90 - STA 287+02	-	-	-	-	-	-	-	-	624	URBAN - MAINLINE
	STA 290+00 - STA 291+30	1.00	1.25	32	-	-	26	-	-	-	URBAN - LOWER LAYER
	STA 290+00 - STA 291+30	1.00	1.25	23	-	32	-	-	-	-	URBAN - UPPER LAYER
	STA 290+00 - STA 291+30, LT	3.00	1.25	6	21	9	-	-	-	-	URBAN - MAINLINE/SHOULDER IMPROVEMENT
	STH 171 & STH 131 INTERSECTION	3.00	1.25	-	382	-	-	-	-	-	STH 171 & STH 131 INTERS. - LOWER
	STH 171 & STH 131 INTERSECTION	3.00	1.25	114	-	159	-	-	-	-	STH 171 & STH 131 INTERS. - UPPER
	STH 171 & STH 131 INTERSECTION	0.00	5.00	-	-	-	-	-	100	-	INTER. CONSTRUCTION - STAGE 1
	STH 171 & STH 131 INTERSECTION	0.00	5.00	-	-	-	-	-	73	-	INTER. CONSTRUCTION - STAGE 2
	STA 102+31 - STA 102+68, RT	-	3.00	-	-	-	-	3	-	-	QUARRY ENTRANCE
	STA 290+00 - 617+79	1.75	1.75	-	8553	-	-	-	-	-	RURAL - LOWER LAYER
	STA 290+00 - 617+79	1.75	1.75	4364	8553	-	-	-	-	-	RURAL - UPPER LAYER
	STA 346+43 - STA 348+72, LT	-	3.00	-	-	-	-	26	-	-	PAVT PATCH MATCH-IN BEHIND C&G
	STA 505+82 - STA 506+79, LT	-	3.00	-	-	-	-	6	-	-	PAVT PATCH MATCH-IN BEHIND C&G
	STA 507+39 - STA 507+92, LT	-	3.00	-	-	-	-	3	-	-	PAVT PATCH MATCH-IN BEHIND C&G
	STA 382+98 C - STA 391+23 C	1.75	1.75	167	234	-	-	-	-	-	STH 131 - LOWER LAYER
	STA 382+98 C - STA 391+23 C	1.75	1.75	119	234	-	-	-	-	-	STH 131 - UPPER LAYER
	STA 391+23 C - STA 392+15 C	3.00	1.25	23	56	-	-	-	-	-	STH 131 - RECONSTRUCT MATCH IN LOWER
	STA 391+23 C - STA 392+15 C	3.00	1.25	17	-	23	-	-	-	-	STH 131 - RECONSTRUCT MATCH IN UPPER
	STA 800+22'B' - 803+57'B'	1.75	1.75	60	83	-	-	-	-	-	RURAL - LOWER LAYER
	STA 800+22'B' - 803+57'B'	1.75	1.75	43	83	-	-	-	-	-	RURAL - UPPER LAYER
	STA 396+03 C	3.00	1.25	-	12	-	-	-	-	-	CULVERT WORK PATCH AREA-LOWER
	STA 396+03 C	3.00	1.25	4	-	5	-	-	-	-	CULVERT WORK PATCH AREA-UPPER
	OVERLOOK AREAS	-	1.75	149	-	-	-	209	-	-	OVERLOOK AREAS
	PROJECT	-	-	-	-	-	-	-	-	-	PROJECT
	STA 346+19 - STA 348+47, LT	-	3.00	-	-	-	-	4	-	-	PATCH AREA FOR C&G
	STA 505+58 - STA 506+55, LT	-	3.00	-	-	-	-	2	-	-	PATCH AREA FOR C&G
	STA 507+16 - STA 507+69, LT	-	3.00	-	-	-	-	1	-	-	PATCH AREA FOR C&G
	SUBTOTAL CATEGORY 0010			9805	32843	783	470	254	173	624	
0040	STA 268+90 - STA 287+02, LT & RT	1.00	1.25	127	-	-	101	-	-	-	PARKING LANE- LOWER LAYER
	STA 268+90 - STA 287+02, LT & RT	1.00	1.25	90	-	127	-	-	-	-	PARKING LANE- UPPER LAYER
	STA 268+90 - STA 287+02, LT & RT	-	-	-	-	-	-	-	-	-	PARKING LANES
	SUBTOTAL CATEGORY 0040			217	0	127	101	0	0	0	
	PROJECT TOTALS			10022	32843	910	571	254	173	624	

ASPHALTIC RUMBLE STRIPS

465.0560
 ASPHALTIC
 RUMBLE STRIPS
 CENTERLINE

CATEGORY	LOCATION	LF	NOTES
0010	STA 22+50 - 260+30	20980	GAP AT COMMERCIAL DRIVEWAYS AND
	STA 295+00 - 617+50	27850	INTERSECTIONS, SEE SDD 13A11-4
PROJECT TOTALS		48830	

3

3

ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES

465.0120
 UPPER ASPHALTIC SURFACE
 LAYER DRIVEWAYS AND
 THICKNESS FIELD ENTRANCES

CATEGORY	LOCATION	IN	TON	REMARKS
0010	STA 7+25, RT	3.00	39	DWY, RT
	STA 9+42, LT	3.00	5	DWY, LT
	STA 19+50, LT	3.00	9	DWY, LT
	STA 20+58, LT	3.00	3	DWY, LT
	STA 41+85, LT	3.00	5	DWY, LT
	STA 209+60, LT	3.00	7	DWY, LT
	STA 253+00, RT	3.00	5	DWY, RT
	STA 254+00, RT	3.00	4	DWY, RT
	STA 256+00, RT	3.00	3	DWY, RT
	STA 298+10, LT	3.00	8	DWY, LT
	STA 339+25, LT	3.00	7	DWY, LT
	STA 345+40, RT	3.00	16	DWY, RT
	STA 345+70, LT	3.00	18	DWY, LT
	STA 350+26, RT	3.00	8	DWY, RT
	STA 405+78, LT	3.00	2	DWY, LT
	STA 464+30, LT	3.00	13	DWY, LT
	STA 469+00, LT	3.00	13	DWY, LT
	STA 506+75, RT	3.00	8	DWY, RT
	STA 506+83, LT	3.00	11	DWY, LT
	STA 522+00, LT	3.00	4	DWY, LT
	STA 548+58, LT	3.00	6	DWY, LT
	STA 600+37, LT	3.00	8	DWY, LT
	STA 384+84 C	3.00	8	DWY, RT
	STA 385+80 C	3.00	3	DWY, RT
	STA 390+97 C	3.00	4	DWY, LT
PROJECT TOTALS			217	

CULVERT PIPE ITEMS

CATEGORY	INLET			OUTLET			SLOPE (%)	CP #	520.8700 CLEANING CULVERT PIPES EACH	521.3118* CULVERT PIPE CORRUGATED STEEL 18-INCH LF	521.1018 APRON ENDWALLS FOR CULVERT PIPE STEEL 18-INCH EACH	521.1024 APRON ENDWALLS FOR CULVERT PIPE STEEL 24-INCH EACH	522.0418 CULVERT REINFORCED CONCRETE CLASS IV 18-INCH LF	522.0424 CULVERT REINFORCED CONCRETE CLASS IV 24-INCH LF	522.1018* APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 18-INCH EACH	522.1024* APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 24-INCH EACH	NOTES	
	STATION	OFFSET	ELEV. (FT)	STATION	OFFSET	ELEV. (FT)												
0010	125+25	24.5' RT	-	125+25	25.3' LT	-	-	13	-	-	-	-	-	-	-	-	-	
	135+75	19.1' RT	-	135+75	23.7' LT	-	-	15	-	-	-	-	-	-	-	-	-	
	143+95	20.7' RT	-	143+95	28.3' LT	-	-	16	-	-	-	-	-	-	-	-	-	
	149+95	16.4' RT	-	149+95	29.1' LT	-	-	17	-	-	-	-	-	-	-	-	-	
	166+78	16.4' RT	-	166+78	20.7' LT	-	-	19	-	-	-	-	-	-	-	-	-	
	246+04	20.5' RT	-	246+12	33.2' LT	-	-	30	-	-	-	-	-	-	-	-	-	
	327+10	23.9' LT	1028.24	326+90	25.4' LT	1026.64	8.00%	41	-	-	-	-	20	-	2	-	-	
	427+07	15.0' LT	1143.93	427+07	17.1' RT	1143.29	2.00%	50	-	-	-	-	-	32	-	2	-	
	472+46	16.8' LT	1133.04	472+46	15.2' RT	1132.40	2.00%	52	-	-	-	-	-	32	-	2	-	
	531+15	-	-	-	-	-	-	57	-	-	-	-	-	-	-	-	-	
	538+20	14.4' LT	1217.59	538+19	15.6' RT	1217.29	1.00%	58	-	-	-	-	-	30	-	2	-	
	556+57	16.2' RT	1202.28	556+57	20.0' LT	1201.67	1.71%	60	-	-	-	-	-	36	-	2	-	
	383+62' C	19.0' RT	707.55	383+82' C	19.5' RT	707.25	1.45%	61	-	22	2	-	-	-	-	-	-	PIPE THICKNESS=0.064'
PROJECT TOTALS									3	22	2	1	20	130	2	8		

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

3

3

CONCRETE ITEMS

CATEGORY	STATION - STATION	390.0100	390.0305	416.0610	416.0620	601.0409	601.0411	601.0555	601.0576	601.0600	602.0405	602.0415	602.0810	*690.0250	REMARKS	
		REMOVING	BASE	DRILLED	DRILLED	CONCRETE	CONCRETE	CONCRETE	CONCRETE	CONCRETE	CONCRETE	CONCRETE	CONCRETE	CONCRETE		CONCRETE
		PAVEMENT FOR	PATCHING	TIE	DOWEL	CURB & GUTTER	CURB & GUTTER	CURB & GUTTER	CURB & GUTTER	CURB	SIDEWALK	SIDEWALK	DRIVEWAY	SAWING		
		CY	CY	EACH	EACH	LF	LF	LF	LF	LF	SF	SF	SY	LF		
0010	STH 171 - URBAN	199	199	444	1295	-	-	-	-	-	-	-	-	2368	MAINLINE - GAYS MILLS, 10% OF TOTAL AREA	
	STA 264+09, RT	-	-	0	-	-	-	25	-	-	146	-	-	21	CURB RAMP, WEST OF BRIDGE	
	STA 264+23, LT	-	-	10	-	29	-	-	-	-	246	-	-	23	CURB RAMP, WEST OF BRIDGE	
	STA 267+48, LT	-	-	7	-	21	-	-	-	-	129	-	-	18	ROBB PARK CROSSING N	
	STA 267+73, RT	-	-	6	-	16	-	-	-	-	-	32	-	5	ROBB PARK CROSSING S	
	STA 269+09, RT	-	-	6	-	17	-	-	-	-	64	-	-	22	CURB RAMP, W ORIN ST - SW QUAD	
	STA 269+40, RT	-	-	9	-	27	-	-	-	-	63	-	-	25	CURB RAMP, W ORIN ST - SE QUAD	
	STA 269+55, LT	-	-	6	-	17	-	-	-	21	118	-	-	33	CURB RAMP, W ORIN ST - NE QUAD	
	STA 272+45, LT	-	-	10	-	30	-	-	-	-	153	-	-	41	CURB RAMPS, POST ST - NW QUAD	
	STA 272+45, RT	-	-	12	-	34	-	-	-	-	265	-	-	46	CURB RAMPS, POST ST - SW QUAD	
	STA 273+10, RT	-	-	16	-	47	-	-	-	11	346	-	-	62	CURB RAMPS, POST ST - SE QUAD	
	STA 273+10, LT	-	-	12	-	34	-	-	-	-	185	-	-	43	CURB RAMPS, POST ST - NE QUAD	
	STA 276+10, LT	-	-	13	-	38	-	-	-	-	240	-	-	59	CURB RAMPS, GAY ST - NW QUAD	
	STA 276+10, RT	-	-	13	-	39	-	-	-	15	243	-	-	60	CURB RAMPS, GAY ST - SW QUAD	
	STA 276+75, LT	-	-	12	-	34	-	-	-	14	198	-	-	47	CURB RAMPS, GAY ST - NE QUAD	
	STA 276+75, RT	-	-	10	-	30	-	-	-	-	212	-	-	56	CURB RAMPS, GAY ST - SE QUAD	
	STA 280+48, LT	-	-	10	-	28	-	-	-	-	139	-	-	42	CURB RAMPS, RAILROAD ST - NW QUAD	
	STA 280+48, RT	-	-	10	-	28	-	-	-	-	120	-	-	37	CURB RAMPS, RAILROAD ST - SW QUAD	
	STA 280+80, RT	-	-	14	-	42	-	-	-	40	207	-	-	74	CURB RAMP, RAILROAD ST - SE QUAD	
	STA 281+00, LT	-	-	13	-	37	-	-	-	-	256	-	-	57	CURB RAMP, RAILROAD ST - NE QUAD	
	STA 284+08, LT	-	-	11	-	31	-	-	-	-	135	-	-	42	CURB RAMPS, REBECCA ST - NW QUAD	
	STA 284+08, RT	-	-	9	-	26	-	-	-	-	147	-	-	46	CURB RAMPS, REBECCA ST - SW QUAD	
	STA 284+62, LT	-	-	9	-	25	-	-	-	-	101	-	-	36	CURB RAMPS, REBECCA ST - SE QUAD	
	STA 284+62, RT	-	-	9	-	26	-	-	-	-	164	-	-	36	CURB RAMPS, REBECCA ST - NE QUAD	
	STA 287+49, LT	-	-	-	-	-	237	-	-	-	1199	-	-	-	CURB RAMP & SW - NORTH SIDE OF INTERSECTION	
	STA 287+49, RT	-	-	-	-	-	165	-	-	-	721	-	-	-	CURB RAMP & SW - SW CORNER INTERSECTION	
	STA 288+85, RT	-	-	-	-	-	168	-	-	-	612	-	-	-	CURB RAMP & SW - SE CORNER INTERSECTION	
	STA 288+84, LT	-	-	-	-	-	150	-	-	-	504	-	-	-	CURB RAMP & SW - EAST CORNER INTERSECTION	
	STA 383+20 C, LT	-	-	-	-	-	11	-	-	-	102	-	-	-	CURB RAMP, SOUTH SIDE, SCHOOL ST	
	STA 383+71 C, LT	-	-	-	-	-	48	-	-	-	238	-	-	-	CURB RAMP, NORTH SIDE, SCHOOL ST	
	STA 383+72 C, RT	-	-	-	-	-	-	-	-	-	414	-	-	-	CURB RAMP & SW, RT SIDE, STH 131	
	STA 390+31 C, LT	-	-	-	-	-	-	-	-	-	85	-	-	4	CURB RAMP - STH 131/ORIN ST	
	STA 390+31 C, RT	-	-	-	-	-	-	-	-	-	-	158	-	8	CURB RAMP - STH 131/ORIN ST	
	STA 394+50 C, LT	-	-	-	-	-	-	-	-	-	-	230	24	-		
	STA 2+24 - STA 4+50, LT	-	-	-	-	-	-	-	-	-	-	-	-	-	MT STERLING, 17' TO FLAG	
	STA 3+65 - STA 4+50, RT	-	-	-	-	-	-	-	-	-	-	-	-	-	MT STERLING, 17' TO FLAG	
	STA 84+60 - STA 91+00, RT	-	-	-	-	-	-	-	640	-	-	-	-	-	14' TO FLAG	
	STA 91+00 - STA 101+40, RT	-	-	-	-	-	-	-	1040	-	-	-	-	-	13' TO FLAG	
	STA 101+40 - STA 105+86, RT	-	-	-	-	-	-	-	446	-	-	-	-	-	16' TO FLAG	
	STA 105+86 - STA 137+96, RT	-	-	-	-	-	-	-	3210	-	-	-	-	-	14' TO FLAG	
	STA 144+49 - STA 207+15, RT	-	-	-	-	-	-	-	6266	-	-	-	-	-	13' TO FLAG	
	STA 250+70 - STA 252+72, RT	-	-	-	-	-	-	-	202	-	-	-	-	-	13' TO FLAG	
	STA 258+62 - STA 263+26, RT	-	-	-	-	-	-	-	464	-	-	-	-	-	14' TO FLAG	
	STA 321+26 - STA 334+09, RT	-	-	-	-	-	-	-	1283	-	-	-	-	-	13' TO FLAG	

TABLE CONTINUED

3

3

CONCRETE ITEMS

CATEGORY	STATION - STATION	CONCRETE ITEMS														REMARKS
		390.0100 REMOVING PAVEMENT FOR BASE PATCHING	390.0305 BASE CONCRETE	416.0610 DRILLED TIE BARS	416.0620 DRILLED DOWEL BARS	601.0409 CONCRETE 30-INCH TYPE A	601.0411 CONCRETE 30-INCH TYPE D	601.0555 CONCRETE CURB & GUTTER 6-INCH SLOPED	601.0576 CONCRETE CURB & GUTTER 4-INCH SLOPED	601.0600 CONCRETE CURB PEDESTRIAN	602.0405 CONCRETE SIDEWALK 4-INCH	602.0415 CONCRETE SIDEWALK 6-INCH	602.0810 CONCRETE DRIVEWAY 6-INCH	*690.0250 SAWING CONCRETE		
		CY	CY	EACH	EACH	LF	LF	LF	LF	LF	SF	SF	SY	LF		
	STA 334+04 - STA 349+59, LT	-	-	-	-	-	-	-	1555	-	-	-	-	-		
	STA 346+02 - STA 349+40, RT	-	-	-	-	-	-	-	338	-	-	-	-	-		
	STA 349+40 - STA 352+90, RT	-	-	-	-	-	-	-	350	-	-	-	-	-		
	STA 365+36 - STA 373+84, LT	-	-	-	-	-	-	-	848	-	-	-	-	-		
	STA 378+92 - STA 384+19, RT	-	-	-	-	-	-	-	527	-	-	-	-	-		
	STA 395+86 - STA 400+10, RT	-	-	-	-	-	-	-	424	-	-	-	-	-		
	STA 405+02 - STA 408+12, LT	-	-	-	-	-	-	-	310	-	-	-	-	-		
	STA 455+47 - STA 459+67, LT	-	-	-	-	-	-	-	420	-	-	-	-	-		
	STA 464+84 - STA 470+24, LT	-	-	-	-	-	-	-	540	-	-	-	-	-		
	STA 503+19 - STA 508+29, LT	-	-	-	-	-	-	-	510	-	-	-	-	-		
	STA 503+19 - STA 508+30, RT	-	-	-	-	-	-	-	511	-	-	-	-	-		
	STA 591+84 - STA 600+92, LT	-	-	-	-	-	-	-	902	-	-	-	-	-		
	SUBTOTAL CATEGORY 0010	199	199	671	1295	656	779	25	20786	101	7752	420	24	3311		
	PROJECT TOTAL	199	199	671	1295	656	779	25	20786	101	7752	420	24	3311		

*ADDITIONAL QUANTITIES SHOWHN ELSEWHERE IN PLANS

WARNING FIELD

602.0515

DETECTABLE

WARNING FIELD

NATURAL PATINA

CATEGORY	LOCATION	SF	NOTES
0010	STH 171-GAYS MILLS	530	53 LOCATIONS, 10 SF EACH
	STA 267+57	17.5	TRAIL CROSSING
PROJECT TOTALS		547.5	

STORM SEWER PIPE ITEMS

CAT	INLET		OUTFALL		CP #/ FROM-TO	PIPE THICKNESS (IN)	520.8000 CONCRETE COLLARS FOR PIPE EACH	521.3118* CULVERT PIPE CORRUGATED STEEL 18-INCH LF	522.1012 APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 12-INCH EACH	522.1015 APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 15-INCH EACH	522.1018* APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 18-INCH EACH	522.1024* APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 24-INCH EACH	INLET ELEVATION	DISCHARGE ELEVATION	SLOPE %
	STATION	OFFSET	STATION	OFFSET											
0010	37+99	27.5' LT	37+99	23.5' LT	3	0.064	1	4	-	-	-	-	1201.22	1200.98	5.87%
	90+91	24.26' RT	90+91	23.1' LT	6	-	-	-	-	-	-	-	1084.87	1084.39	1.02%
	95+14	18.2' RT	95+14	20.8' LT	7	-	-	-	-	-	-	-	1055.63	1055.24	1.00%
	98+60	18.1' RT	98+60	21.1' LT	8	-	-	-	-	-	-	-	1037.55	1031.77	1.99%
	104+91	23.1' RT	104+79	27.4' LT	9	-	-	-	-	-	-	-	989.86	988.82	2.00%
	107+51	20.8' RT	107+54	23.2' LT	10	-	-	-	-	-	-	-	975.95	973.30	6.03%
	121+51	20.0' RT	121+49	21.9' LT	12	-	-	-	-	-	-	-	912.70	911.87	1.98%
	129+66	22.7' RT	129+65	19.8' LT	14	-	-	-	-	-	-	-	891.73	890.38	2.01%
	169+58	24.6' RT	169+57	19.5' RT	20	-	1	-	-	-	-	-	817.50	817.37	2.62%
	179+78	18.6' RT	179+78	12.6' RT	22	-	1	-	-	-	-	-	823.48	823.36	2.01%
	185+79	18.9' RT	185+80	19.1' LT	23	-	-	-	-	-	-	-	822.36	821.60	2.00%
	191+95	18.6' RT	191+96	22.4' LT	24	-	-	-	-	-	-	-	794.50	793.63	2.13%
	201+16	18.6' RT	201+18	24.4' LT	26	-	-	-	-	-	-	-	773.86	773.00	2.00%
	224+77	23.1' RT	224+77	20.1' LT	28	-	-	-	-	-	-	-	731.99	731.12	2.02%
	**258+64	20.8' RT	258+64	19.1' LT	33	-	-	-	-	-	-	-	699.00	698.14	2.15%
	**261+05	22.4' RT	261+05	22.6' LT	34	-	-	-	-	-	-	-	700.00	699.10	2.00%
	394+23'C'	17.5' LT	395+00'C'	31.74' LT	9 TO 10	-	-	-	1	-	-	-	699.04	698.82	0.30%
	396+02'C'	31.0' RT	396+05'C'	23.9' LT	15 TO 16	-	-	-	-	-	-	-	698.35	698.20	0.30%
	320+06	24.3' LT	319+65	24.7' LT	40	-	-	-	-	-	1	-	957.00	956.00	2.38%
	326+09	19.9' RT	326+03	21.6' LT	40A	-	-	-	-	-	-	-	1017.50	1016.66	2.00%
	335+74	14.5' RT	335+61	18.3' RT	42	-	-	-	-	1	-	-	1076.90	1076.77	0.51%
	339+00	14.5' LT	339+00	19.0' RT	43	-	-	-	-	-	1	-	1097.73	1097.05	2.03%
	345+05	14.5' LT	345+02	19.5' RT	44	-	-	-	-	-	1	-	1114.75	1114.00	2.20%
	368+49	13.5' LT	368+50	16.5' RT	45	-	-	-	-	-	1	-	1145.33	1144.73	1.99%
	395+08	19.6' RT	395+09	18.5' LT	46	-	-	-	-	-	-	-	1142.37	1141.94	1.00%
	399+93	21.5' RT	400+07	22.0' LT	47	-	-	-	-	-	-	-	1136.86	1135.95	2.00%
	402+06	26.2' RT	402+05	15.0' RT	48	-	-	-	-	-	-	-	1134.71	1133.89	2.00%
	408+03	23.0' LT	408+03	20.2' RT	49	-	-	-	-	-	-	-	1127.86	1127.00	1.99%
	467+40	22.4' LT	467+41	11.6' LT	51	-	1	-	-	-	-	-	1148.41	1148.19	2.05%
	503+00	19.7' RT	503+00	18.7' LT	54	-	-	-	-	-	-	-	1184.37	1183.60	2.00%
	522+47	22.3' LT	522+46	19.7' RT	56	-	-	-	-	-	-	-	1205.82	1205.41	0.98%
	572+97	20.0' LT	573+52	18.6' LT	62A	-	-	-	-	-	1	-	1196.23	1195.00	2.23%
PROJECT TOTALS							4	4	1	1	5	22			

*ADDITIONAL QUANTITIES SHOWN IN SF WHERE

**JOINT TIES ARE REQUIRED ON ALL SECTIONS OF PIPE AND ENDWALLS INSTALLED

3

3

STORM SEWER PIPE ITEMS

CAT	INLET		OUTFALL		CP #/ FROM-TO	522.1030	608.0412	608.0415	608.0418	608.0424	608.0430	612.0902.5 INSULATION BOARD POLYSTYRENE 2-INCH	INLET ELEVATION	DISCHARGE ELEVATION	SLOPE %
	STATION	OFFSET	STATION	OFFSET		APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 30-INCH EACH	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 12-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 15-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 18-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 24-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 30-INCH LF				
0010	90+91	24.26' RT	90+91	23.1' LT	6	-	-	-	-	47	-	-	1084.87	1084.39	1.02%
	95+14	18.2' RT	95+14	20.8' LT	7	-	-	-	-	39	-	-	1055.63	1055.24	1.00%
	98+60	18.1' RT	98+60	21.1' LT	8	-	-	-	-	39	-	-	1032.55	1031.77	1.99%
	104+91	23.1' RT	104+79	27.4' LT	9	-	-	-	-	52	-	-	989.86	988.82	2.00%
	107+51	20.8' RT	107+54	23.2' LT	10	-	-	-	-	44	-	-	975.95	973.30	6.03%
	121+51	20.0' RT	121+49	21.9' LT	12	-	-	-	-	42	-	-	912.70	911.87	1.98%
	129+66	22.2' RT	129+65	19.8' LT	14	-	-	-	-	42	-	-	891.23	890.38	2.01%
	157+65	18.6' RT	157+68	21.4' LT	18	1	-	-	-	-	40	-	842.15	841.35	2.00%
	169+58	24.6' RT	169+57	19.5' RT	20	-	-	-	-	5	-	-	817.50	817.37	2.62%
	179+78	18.6' RT	179+78	12.6' RT	22	-	-	-	-	6	-	-	823.48	823.36	2.01%
	185+79	18.9' RT	185+80	19.1' LT	23	-	-	-	-	38	-	-	822.36	821.60	2.00%
	191+95	18.6' RT	191+96	22.4' LT	24	-	-	-	-	41	-	-	794.50	793.63	2.13%
	201+16	18.6' RT	201+18	24.4' LT	26	-	-	-	-	43	-	-	773.86	773.00	2.00%
	224+77	23.1' RT	224+77	20.1' LT	28	-	-	-	-	43	-	-	731.99	731.12	2.02%
	225+15	22.0' RT	224+77	23.1' RT	28A	-	-	-	39	-	-	-	734.50	732.24	5.92%
	258+64	20.8' RT	258+64	19.1' LT	33	-	-	-	-	40	-	-	699.00	698.14	2.15%
	261+05	22.4' RT	261+05	22.6' LT	34	-	-	-	-	45	-	-	700.00	699.10	2.00%
	287+10	23.50' RT	287+26	23.5' RT	5 TO 6	-	16	-	-	-	-	3	700.17	700.11	0.50%
	287+26	23.5' RT	287+26	23.49' LT	6 TO 7	-	47	-	-	-	-	-	700.09	699.87	0.50%
	287+26	23.49' LT	287+93	22.44' LT	7 TO 8	-	67	-	-	-	-	-	699.85	699.54	0.50%
	287+93	22.44' LT	394+23'C	17.5' LT	8 TO 9	-	96	-	-	-	-	-	699.52	699.06	0.50%
	394+23'C	17.5' LT	395+00'C	31.74' LT	9 TO 10	-	75	-	-	-	-	-	699.04	698.82	0.30%
	289+78	21.5' RT	289+76	21.5' RT	12 TO 13	-	44	-	-	-	-	6	699.06	698.93	0.30%
	289+76	21.5' RT	394+92'C	34.8' RT	13 TO 14	-	74	-	-	-	-	-	698.93	698.72	0.30%
	394+92'C	34.8' RT	396+02'C	31.0' RT	14 TO 15	-	117	-	-	-	-	-	698.70	698.37	0.30%
	396+02'C	31.0' RT	396+05'C	23.9' LT	15 TO 16	-	-	-	-	52	-	-	698.35	698.20	0.30%
	320+06	24.3' LT	319+65	24.7' LT	40	-	-	-	42	-	-	-	957.00	956.00	2.38%
	326+09	19.9' RT	326+03	21.6' LT	40A	-	-	-	-	42	-	-	1017.50	1016.66	2.00%
	335+74	14.5' RT	335+61	18.3' RT	42	-	-	35	-	-	-	-	1076.90	1076.72	0.51%
	339+00	14.5' LT	339+00	19.0' RT	43	-	-	-	34	-	-	-	1097.73	1097.05	2.03%
	339+10	14.4' LT	339+00	14.5' LT	43B	-	10	-	-	-	-	-	1098.43	1098.23	2.03%
	345+05	14.5' LT	345+02	19.5' RT	44	-	-	-	34	-	-	-	1114.75	1114.00	2.20%
	345+15	14.5' LT	345+05	14.5' LT	44B	-	10	-	-	-	-	-	1115.45	1115.25	2.02%
	368+29	13.5' LT	368+49	13.5' LT	45A	-	20	-	-	-	-	-	1146.23	1145.83	2.00%
	368+49	13.5' LT	368+50	16.5' RT	45	-	-	-	30	-	-	-	1145.33	1144.73	1.99%
	395+08	19.6' RT	395+09	18.5' LT	46	-	-	-	-	38	-	-	1142.32	1141.94	1.00%
	399+93	21.5' RT	400+07	22.0' LT	47	-	-	-	-	46	-	-	1136.86	1135.95	2.00%
	402+06	26.2' RT	402+05	15.0' RT	48	-	-	-	-	41	-	-	1134.71	1133.89	2.00%
	408+03	23.0' LT	408+03	20.2' RT	49	-	-	-	-	44	-	-	1127.86	1127.00	1.99%
	408+58	23.0' LT	408+03	23.0' LT	49A	-	-	-	55	-	-	-	1130.11	1127.96	3.92%
	467+40	22.4' LT	467+41	11.6' LT	51	-	11	-	-	-	-	-	1148.41	1148.19	2.05%
	503+00	19.7' RT	503+00	18.7' LT	54	-	-	-	-	38	-	-	1184.37	1183.60	2.00%
	522+14	19.6' LT	522+47	22.3' LT	56A	-	-	-	-	34	-	-	1208.00	1205.82	6.54%
	522+47	22.3' LT	522+46	19.7' RT	56	-	-	-	-	42	-	-	1205.82	1205.41	0.98%
	572+97	20.0' LT	573+52	18.6' LT	62A	-	-	-	55	-	-	-	1196.23	1195.00	2.23%
	573+52	18.6' LT	573+52	19.0' RT	62	1	-	-	-	-	38	-	1194.00	1193.20	2.13%
	611+17	20.6' RT	611+18	23.6' LT	63	1	-	-	-	-	44	-	1167.55	1167.08	1.06%
PROJECT TOTALS						3	587	35	289	943	122	9			

PROJECT NO: 5790-02-72

HWY: STH 171

COUNTY: CRAWFORD

MISCELLANEOUS QUANTITIES

SHEET

E

3

3

ADJUSTING UTILITIES

611.8120.5 SPV.0060.02 SPV.0060.03

COVER ADJUSTING ADJUST

PLATES SANITARY MANHOLE WATER VALVE

TEMPORARY COVERS BOX

CATEGORY	STATION	OFFSET	EACH	EACH	EACH	COMMENT	
0030	3+97	4.8' LT	1	1	-	MT STERLING	
	4+70	8.9' RT	-	-	1	MT STERLING	
	4+73	10.9' RT	-	-	1	MT STERLING	
	4+75	8.9' RT	-	-	1	MT STERLING	
	6+38	4.7' LT	1	1	-	MT STERLING	
	6+62	15.8' LT	-	-	1	MT STERLING	
	6+70	17.5' LT	-	-	1	MT STERLING	
	10+24	7.9' RT	1	1	-	MT STERLING	
	12+12	8.4' RT	1	1	-	MT STERLING	
	16+09	10.3' RT	1	1	-	MT STERLING	
	18+73	6.9' RT	1	1	-	MT STERLING	
	SUBTOTAL CAT 0030			6	6	5	
	0040	263+49	17.3' RT	1	1	-	GAYS MILLS
		272+49	23.7' LT	-	-	1	GAYS MILLS
276+13		20.9' LT	-	-	1	GAYS MILLS	
276+37		18.1' LT	-	-	1	GAYS MILLS	
276+40		33.2' LT	-	-	1	GAYS MILLS	
276+42		17.9' LT	-	-	1	GAYS MILLS	
281+07		23.1' LT	-	-	1	GAYS MILLS	
284+24		16.6' LT	-	-	1	GAYS MILLS	
284+26		14.1' LT	-	-	1	GAYS MILLS	
284+29		38.0' RT	-	-	1	GAYS MILLS	
288+93		31.9' LT	-	-	1	GAYS MILLS	
291+66		12.4' LT	-	-	1	GAYS MILLS	
291+82		CL	1	1	-	GAYS MILLS	
383+30 C		20.9' LT	-	-	1	GAYS MILLS	
388+32 C		5.8' RT	1	1	-	GAYS MILLS	
391+68 C		1' RT	1	1	-	GAYS MILLS	
392+48 C		18.2' RT	-	-	1	GAYS MILLS	
392+87 C		2.0' RT	1	1	-	GAYS MILLS	
SUBTOTAL CAT 0040			5	5	13		
PROJECT TOTALS			11	11	18		

STORM SEWER STRUCTURE ITEMS

CATEGORY	** STATION	** OFFSET	CP #/ INLET	608.0005	611.0612	611.0624	611.0630	611.0633	611.0639	611.0642	611.0645	611.2005	611.3004		611.3901	611.3902	SPV.0060.01	RIM ELEV	INVERT ELEV	***DEPTH FT
				STORM SEWER ROCK EXCAVATION CY	INLET COVERS TYPE C EACH	INLET COVERS TYPE H EACH	INLET COVERS TYPE HM-GJ EACH	INLET COVERS TYPE HM-GJS EACH	INLET COVERS TYPE H-S EACH	INLET COVERS TYPE MS EACH	INLET COVERS TYPE MS-A EACH	MANHOLES 5-FT DIAMETER EACH	INLETS 4-FT DIAMETER EACH	611.3230 INLETS 2 X 3-FT EACH	INLETS MEDIAN 1 GRATE EACH	INLETS MEDIAN 2 GRATE EACH	INLET COVERS FLAT TEMPORARY EACH			
0010	37+99	27.5' LT	3	-	-	-	-	-	-	1	-	-	-	-	1	-	-	1204.00	1201.22	2.78
	90+91	24.26' RT	6	-	-	-	-	-	-	2	-	-	-	-	-	1	-	1087.90	1084.87	3.03
	95+14	18.2' RT	7	6	-	-	-	-	-	2	-	-	-	-	-	1	-	1059.30	1055.63	3.67
	98+60	18.1' RT	8	6	-	-	-	-	-	2	-	-	-	-	-	1	-	1035.90	1032.55	3.35
	104+91	23.1' RT	9	-	-	-	-	-	-	2	-	-	-	-	-	1	-	993.60	989.86	3.74
	107+51	20.8' RT	10	6	-	-	-	-	-	2	-	-	-	-	-	1	-	979.95	975.95	4.00
	121+51	20.0' RT	12	-	-	-	-	-	-	2	-	-	-	-	-	1	-	916.70	912.70	4.00
	129+66	22.2' RT	14	-	-	-	-	-	-	2	-	-	-	-	-	1	-	894.74	891.23	3.51
	157+65	18.6' RT	18	6	-	-	-	-	-	2	-	-	-	-	-	1	-	845.80	842.15	3.65
	169+58	24.6' RT	20	-	-	-	-	-	-	2	-	-	-	-	-	1	-	820.25	817.50	2.75
	179+78	18.6' RT	22	-	-	-	-	-	-	2	-	-	-	-	-	1	-	826.00	823.48	2.52
	185+79	18.9' RT	23	6	-	-	-	-	-	2	-	-	-	-	-	1	-	826.40	822.36	4.04
	191+95	18.6' RT	24	-	-	-	-	-	-	2	-	-	-	-	-	1	-	798.50	794.50	4.00
	201+16	18.6' RT	26	6	-	-	-	-	-	2	-	-	-	-	-	1	-	776.55	773.86	2.69
	224+77	23.1' RT	28	-	-	-	-	-	-	2	-	-	-	-	-	1	-	734.49	731.99	2.50
	258+64	20.8' RT	33	6	-	-	-	-	-	2	-	-	-	-	-	1	-	702.75	699.00	3.75
	261+05	22.4' RT	34	6	-	-	-	-	-	2	-	-	-	-	-	1	-	704.40	700.00	4.40
	287+10	23.50' RT	5	-	-	-	-	-	1	-	-	-	-	1	-	-	-	702.59	700.17	1.42
	287+26	23.50' RT	6	-	-	-	-	-	1	-	-	-	-	1	-	-	-	702.69	700.09	1.60
	287+26	23.49' LT	7	-	-	1	-	-	-	-	-	-	-	1	-	-	1	702.59	699.85	1.74
	287+93	22.44' LT	8	-	-	-	-	-	1	-	-	-	-	1	-	-	1	702.02	699.52	1.50
	394+23 C	17.50' LT	9	-	-	1	-	-	-	-	-	-	1	-	-	-	1	702.34	699.04	2.30
	289+78	21.50' RT	12	-	-	1	-	-	-	-	-	-	-	1	-	-	-	701.35	699.06	1.29
	289+76	21.50' LT	13	-	-	1	-	-	-	-	-	-	-	1	-	-	-	701.34	698.93	1.41
	394+92 C	34.83' RT	14	-	-	-	-	-	-	1	-	-	-	-	1	-	-	700.71	698.70	2.01
	396+02 C	31.02' RT	15	-	-	-	-	-	-	1	-	-	-	-	1	-	-	702.00	698.35	3.65
	320+06	24.3' LT	40	-	-	-	-	-	-	1	-	-	-	-	1	-	-	959.95	957.00	2.95
	326+09	19.9' RT	40A	-	-	-	-	-	-	2	-	-	-	-	-	1	-	1021.50	1017.50	4.00
	335+74	14.5' RT	42	-	-	-	1	-	-	-	-	-	-	1	-	-	-	1079.25	1076.90	1.35
	339+00	14.5' LT	43	-	-	-	1	-	-	-	-	-	-	1	-	-	-	1100.73	1097.73	2.00
	339+10	14.4' LT	43B	-	-	-	1	-	-	-	-	-	-	1	-	-	-	1100.93	1098.43	1.50
	345+05	14.5' LT	44	-	-	-	-	1	-	-	-	-	-	1	-	-	-	1118.75	1114.75	3.00
	345+15	14.5' LT	44B	-	-	-	1	-	-	-	-	-	-	1	-	-	-	1118.80	1115.45	2.35
	368+29	13.5' LT	45A	-	-	-	1	-	-	-	-	-	-	1	-	-	-	1148.40	1146.23	1.17
	368+49	13.5' LT	45	-	-	-	-	1	-	-	-	-	-	1	-	-	-	1148.33	1145.33	2.00
	395+08	19.6' RT	46	-	-	-	-	-	-	-	2	-	-	-	-	1	-	1144.8	1142.32	2.50
	399+93	21.5' RT	47	-	-	-	-	-	-	2	-	-	-	-	-	1	-	1141.00	1136.86	4.14
	402+06	26.2' RT	48	-	-	-	-	-	-	2	-	-	-	-	-	1	-	1138.25	1134.71	3.54
	408+03	23.0' LT	49	-	-	-	-	-	-	2	-	-	-	-	-	1	-	1131.30	1127.86	3.44
	467+40	22.4' LT	51	-	-	-	-	-	-	1	-	-	-	-	1	-	-	1150.41	1148.41	2.00
	503+00	19.7' RT	54	-	-	-	-	-	-	2	-	-	-	-	-	1	-	1186.87	1184.37	2.50
	522+47	22.3' LT	56	-	1	-	-	-	-	-	-	1	-	-	-	-	-	1209.50	1205.82	2.43
	573+52	18.6' LT	62	-	-	-	-	-	-	2	-	-	-	-	-	1	-	1197.04	1194.00	3.04
	611+17	20.6' RT	63	-	-	-	-	-	-	2	-	-	-	-	-	1	-	1170.59	1167.55	3.04
PROJECT TOTALS				48	1	4	5	2	3	51	2	1	1	13	5	24	3			

** STATION & OFFSET ARE TO CENTER OF STRUCTURE
 *** DEPTH = RIM ELEV - COVER HT - 6 INCH ADJ RING HT - INVERT ELEV - PIPE THICKNESS
 FOR MEDIAN INLETS DEPTH = RIM ELEV - INVERT ELEV - PIPE THICKNESS

3

3

3

BEAM GUARD

CATEGORY	STATION	LOCATION	614.2300	614.2330	614.2350	614.2500	614.2610	NOTES
			MGS	MGS	MGS GUARDRAIL	MGS THRIE BEAM	MGS GUARDRAIL	
			<u>GUARDRAIL 3</u>	<u>GUARDRAIL 3 K</u>	<u>SHORT RADIUS</u>	<u>TRANSITION</u>	<u>TERMINAL EAT</u>	
			LF	LF	LF	LF	EACH	
0010	STA 81+57 - STA 117+54	LT	2650	837.5	-	-	2	
	STA 120+69 - STA 126+28	LT	-	450	-	-	2	
	STA 129+06 - STA 140+47	LT	1025	-	-	-	2	
	STA 142+06 - STA 167+95	LT	2500	-	-	-	2	
	STA 170+03 - STA 196+70	LT	2612.5	-	67.5	39.5	1	
	STA 196+93 - STA 204+97	LT	762	-	84.5	39.5	1	
	STA 257+16 - STA 260+77	LT	-	250	-	-	2	
	STA 298+54 - STA 321+05	RT	-	2175	-	-	2	
	STA 321+17 - STA 326+56	LT	425	-	-	-	2	
PROJECT TOTALS			9974.5	3712.5	152	79	16	

GUARDRAIL MOW STRIP EMULSIFIED ASPHALT

CATEGORY	STATION	LOCATION	614.0397
			SY
0010	STA 81+57 - STA 86+46	LT	178
	STA 105+87 - STA 109+08	LT	125
	STA 113+45 - STA 117+54	LT	160
	STA 123+94 - STA 126+28	LT	91
	STA 129+06 - STA 137+96	LT	347
	STA 172+45 - STA 175+73	LT	128
	STA 257+16 - STA 260+77	LT	98
	STA 311+26 - STA 321+05	LT	381
	STA 323+50 - STA 326+55	LT	119
PROJECT TOTALS			1627

EROSION CONTROL MOBILIZATION

CATEGORY	LOCATION	628.1905	628.1910
		MOBILIZATION	EMERGENCY MOBILIZATIONS
		<u>EROSION CONTROL</u>	<u>EROSION CONTROL</u>
		(EACH)	(EACH)
0010	PROJECT	3	4
PROJECT TOTALS		3	4

3

EROSION CONTROL AND FINISHING ITEMS

CATEGORY	LOCATION	628.2004																	SPV.0090.01 GRADING, SHAPING, AND FINISHING FOR CONCRETE CURB AND GUTTER	NOTES
		606.0300	625.0500	628.1504		628.1520	EROSION	628.7005	628.7010	628.7015	628.7504	628.7555	629.0210	630.0120	630.0140	630.0200	630.0500			
		RIPRAP	SALVAGED	MULCHING	SILT	FENCE	MAT	INLET	INLET	INLET	TEMPORARY	CULVERT	FERTILIZER	SEEDING	SEEDING	SEEDING	SEED			
		HEAVY	TOPSOIL			MAINTENANCE	CLASS I	PROTECTION	PROTECTION	PROTECTION	DITCH	PIPE	(CWT)	NO. 20	NO. 40	TEMPORARY	WATER			
		CY	(SY)	SY	LF	LF	SY	EACH	EACH	EACH	LF	EACH	(LB)	(LB)	(LB)	(MGAL)	LF			
0010	VARIES	-	-	-	-	-	-	41	3	13	-	-	-	-	-	-	-	-		
	VARIES	-	3867	-	-	-	3867	-	-	-	-	-	2.5	70	-	105	87	-		
	VARIES	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	19309		
	STH 171-GAYS MILLS	-	197	197	-	-	-	-	-	-	-	-	0.2	-	4	6	5	-		
	171 & 131 INTER.	-	765	765	-	-	-	-	-	-	-	-	0.5	-	14	21	18	-		
	SCHOOL ST	-	137	137	-	-	-	-	-	-	-	-	0.1	-	3	4	4	-		
	90+91	23	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-		
	95+14	23	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-		
	98+60	23	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-		
	104+91	23	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-		
	125+25	45	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-		
	135+75	45	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-		
	143+95	45	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-		
	166+78	45	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-		
	191+95	23	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-		
	201+16	23	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-		
	327+00, LT	-	-	-	-	-	-	-	-	-	-	3	-	-	-	-	-	-		
	427+07, LT	-	-	-	-	-	-	-	-	-	-	4	-	-	-	-	-	-		
	472+46, LT	-	-	-	-	-	-	-	-	-	-	4	-	-	-	-	-	-		
	538+20, LT	-	-	-	-	-	-	-	-	-	-	4	-	-	-	-	-	-		
	556+57, RT	-	-	-	-	-	-	-	-	-	-	4	-	-	-	-	-	-		
	3+84 - 4+55, RT	-	-	-	72	144	-	-	-	-	-	-	-	-	-	-	-	-		
	80+30 - 101+05, LT	-	-	-	2062	4124	-	-	-	-	-	-	-	-	-	-	-	-		
	104+97 - 117+64, LT	-	-	-	1259	2518	-	-	-	-	-	-	-	-	-	-	-	-		
	118+07 - 141+51, LT	-	-	-	2349	4698	-	-	-	-	-	-	-	-	-	-	-	-		
	142+08 - 168+25, LT	-	-	-	2652	5304	-	-	-	-	-	-	-	-	-	-	-	-		
	168+61 - 196+42, LT	-	-	-	2812	5624	-	-	-	-	-	-	-	-	-	-	-	-		
	251+62 - 253+00, LT	-	-	-	141	282	-	-	-	-	-	-	-	-	-	-	-	-		
	258+92 - 263+22, LT	-	-	-	418	836	-	-	-	-	-	-	-	-	-	-	-	-		
	297+26 - 300+66, RT	-	-	-	346	692	-	-	-	-	-	-	-	-	-	-	-	-		
	320+07 - 321+00, RT	-	-	-	91	182	-	-	-	-	-	-	-	-	-	-	-	-		
	325+81 - 326+90, LT	-	-	-	113	226	-	-	-	-	-	-	-	-	-	-	-	-		
	328+93 - 333+68, LT	-	-	-	481	962	-	-	-	-	-	-	-	-	-	-	-	-		
	338+24 - 345+40, RT	-	-	-	731	1462	-	-	-	-	-	-	-	-	-	-	-	-		
	346+05 - 347+80, RT	-	-	-	180	360	-	-	-	-	-	-	-	-	-	-	-	-		
	347+91 - 350+28, RT	-	-	-	238	476	-	-	-	-	-	-	-	-	-	-	-	-		
	365+28 - 374+14, RT	-	-	-	899	1798	-	-	-	-	-	-	-	-	-	-	-	-		
	455+07 - 457+25, LT	-	-	-	202	404	-	-	-	-	-	-	-	-	-	-	-	-		
	503+07 - 503+55, LT	-	-	-	49	98	-	-	-	-	-	-	-	-	-	-	-	-		
	503+74 - 505+12, LT	-	-	-	138	276	-	-	-	-	-	-	-	-	-	-	-	-		
	505+43 - 505+80, LT	-	-	-	37	74	-	-	-	-	-	-	-	-	-	-	-	-		
	591+63 - 593+05, LT	-	-	-	146	292	-	-	-	-	-	-	-	-	-	-	-	-		
	594+74 - 596+50, RT	-	-	-	181	362	-	-	-	-	-	-	-	-	-	-	-	-		
	UNDISTRIBUTED	80	1242	275	3900	7799	967	11	1	4	200	5	0.9	18	6	34	29	-		
	PROJECT TOTAL	398	6208	1374	19497	38993	4834	52	4	17	200	24	4	88	27	170	143	19309		

3

3

PROJECT NO	57900272	HW	STH 171	COUNT	CRAWORD	MISCELLANEOUS UANTITIES	SHEET	E
------------	----------	----	---------	-------	---------	-------------------------	-------	---

3

3

CATEGORY	LOCATION	SIGN CODE	SIZE	SIGNS							NOTES
				634.0612	634.0614	634.0616	634.0618	637.2230	638.3000		
				POSTS WOOD 4X6-INCH	POSTS WOOD 4X6-INCH	POSTS WOOD 4X6-INCH	POSTS WOOD 4X6-INCH	SIGNS TYPE II	638.2102 MOVING	REMOVING SMALL SIGN	
		<u>12-FT</u>	<u>14-FT</u>	<u>16-FT</u>	<u>18-FT</u>	<u>REFLECTIVE F</u>	<u>SIGNS TYPE II</u>	<u>SUPPORTS</u>			
			EACH	EACH	EACH	EACH	SF	EACH	EACH		
0010	68+00, RT	W11-2 30" X 30"	-	1	-	-	6.25	-	-	PED CROSSING WARNING AT ORCHARD	
		W16-9P 24" X 12"	-	-	-	-	2.00	-	-		
	75+00, LT	W11-2 30" X 30"	-	1	-	-	6.25	-	-	PED CROSSING WARNING AT ORCHARD	
		W16-9P 24" X 12"	-	-	-	-	2.00	-	-		
	90+00, LT	-	-	-	-	1	-	1	1	CURVE, SPEED WARNING	
	93+49, LT	-	-	-	-	1	-	1	1	SNOWMOBILE TRAIL	
	100+50, LT	-	-	1	-	-	-	1	1	CHEVRON	
	101+41, LT	-	-	1	-	-	-	1	1	CHEVRON	
	102+24, LT	-	-	1	-	-	-	1	1	CHEVRON	
	102+96, LT	-	-	1	-	-	-	1	1	CHEVRON	
	103+75, LT	-	-	1	-	-	-	1	1	CHEVRON	
	104+51, LT	-	-	1	-	-	-	1	1	CHEVRON	
	113+60, LT	-	-	-	-	1	-	1	1	CURVE, SPEED WARNING	
	161+92, LT	-	-	-	-	1	-	1	1	CURVE WARNING	
	185+00, LT	-	-	-	-	1	-	1	1	CURVE, SPEED WARNING	
	196+64, LT	-	-	-	1	-	-	1	1	STOP SIGN	
	257+22, LT	-	-	-	-	1	-	1	1	CURVE, SPEED WARNING	
	268+29, LT	-	-	-	1	-	-	1	1	SPEED SIGN	
	272+51, LT	-	-	-	1	-	-	1	1	STOP SIGN	
	272+55, RT	-	-	-	1	-	-	1	1	STREET SIGNS/ATV ROUTE	
	272+95, RT	-	-	-	1	-	-	1	1	STOP SIGN	
	276+19, LT	-	-	-	1	-	-	1	1	STOP SIGN, BOX-OUT REQ'D	
	276+20, RT	-	-	-	1	-	-	1	1	STREET SIGN, BOX-OUT REQ'D	
	276+73, RT	-	-	-	1	-	-	1	1	STOP SIGN, BOX-OUT REQ'D	
	284+60, LT	-	-	-	1	-	-	1	1	STREET SIGN	
	301+85, RT	-	-	-	-	1	-	1	1	CURVE, SPEED WARNING	
	303+75, RT	-	-	-	2	-	-	1	2	DESTINATION SIGN	
304+00-312+00		-	-	14	-	-	-	14	14	CHEVRONS	
	310+75, RT	-	-	-	-	1	-	1	1	CURVE, SPEED WARNING	
	313+44, RT	-	-	-	-	1	-	1	1	CURVE, SPEED WARNING	
	317+98, RT	-	-	-	-	1	-	1	1	HISTORICAL MARKER	
	321+12, LT	-	-	1	-	-	-	1	1	CHEVRON	
	287+31, LT	-	-	-	1	-	-	1	1	INTERSEC. CONSTRUCT-STAGE 1, ROUTE MARKER	
	344+00, RT	W11-2 30" X 30"	-	1	-	-	6.25	-	-	PED CROSSING WARNING AT ORCHARD	
		W16-9P 24" X 12"	-	-	-	-	2.00	-	-		
	352+00, LT	W11-2 30" X 30"	-	1	-	-	6.25	-	-	PED CROSSING WARNING AT ORCHARD	
		W16-9P 24" X 12"	-	-	-	-	2.00	-	-		
	393+47 C, LT	-	-	-	-	1	-	1	1	INTERSEC. CONSTRUCT-STAGE 1, DIRECTION ASSEMBLY	

TABLE CONTINUED

PROJECT NO	57900272	HW	STH 171	COUNT	CRAWORD	MISCELLANEOUS UANTITIES	SHEET	E
------------	----------	----	---------	-------	---------	-------------------------	-------	---

3

3

CATEGORY	LOCATION	SIGN CODE	SIZE	SIGNS				REFLECTIVE F SF	SIGNS TYPE II EACH	SUPPORTS EACH	NOTES
				634.0612 POSTS WOOD 4X6-INCH 12-FT EACH	634.0614 POSTS WOOD 4X6-INCH 14-FT EACH	634.0616 POSTS WOOD 4X6-INCH 16-FT EACH	634.0618 POSTS WOOD 4X6-INCH 18-FT EACH				
	393+56 C, LT	-	-	-	-	-	1	-	1	1	INTERSEC. CONSTRUCT- STAGE 1, DIRECTION ASSEMBLY
	287+60, RT	-	-	-	-	-	2	-	1	2	INTERSEC. CONSTRUCT- STAGE 2, DIRECTION ASSEMBLY
	289+03, LT	-	-	-	1	-	1	-	3	2	INTERSEC. CONSTRUCT- STAGE 2, DIRECTION ASSEMBLY, STOP SIGN & U-TURN
	392+45 C, RT	-	-	-	1	-	1	-	2	2	INTERSEC. CONSTRUCT- STAGE 2, STOP SIGN & DIRECTION ASSEMBLY
	450+00, RT	W1-1L	36" X 36"	-	-	-	1	9	-	-	CURVE WARNING SIGN
	450+00, RT	W13-1	18" X 18"	-	-	-	-	2.25	-	-	MPH (30 MPH)
	463+00, RT	W11-2	30" X 30"	-	1	-	-	6.25	-	-	PED CROSSING WARNING AT ORCHARD
		W16-9P	24" X 12"	-	-	-	-	2.00	-	-	
	471+00, LT	W11-2	30" X 30"	-	1	-	-	6.25	-	-	PED CROSSING WARNING AT ORCHARD
		W16-9P	24" X 12"	-	-	-	-	2.00	-	-	
	502+00, RT	W11-2	30" X 30"	-	1	-	-	6.25	-	-	PED CROSSING WARNING AT ORCHARD
		W16-9P	24" X 12"	-	-	-	-	2.00	-	-	
	510+00, LT	W11-2	30" X 30"	-	1	-	-	6.25	-	-	PED CROSSING WARNING AT ORCHARD
		W16-9P	24" X 12"	-	-	-	-	2.00	-	-	
	UNDISTRIBUTED	-	-	-	3	-	-	-	3	3	NO PASSING ZONE SIGNS
	PROJECT TOTALS			21	25	3	14	77.25	53	54	

TRAFFIC CONTROL

CATEGORY	PROJECT LOCATION	DAYS	*643.0300		643.0420		643.0500		643.0600		643.0705		643.0715		643.0900		643.0920		643.1070		643.1050		NOTES	
			APPROX. SERVICE PERIOD	TRAFFIC CONTROL DRUMS	TRAFFIC CONTROL TYPE III	TRAFFIC CONTROL BARRICADES	TRAFFIC CONTROL FLEXIBLE TUBULAR MARKER POSTS	TRAFFIC CONTROL FLEXIBLE TUBULAR MARKER BASES	TRAFFIC CONTROL WARNING LIGHTS TYPE A	TRAFFIC CONTROL WARNING LIGHTS TYPE C	TRAFFIC CONTROL SIGNS	TRAFFIC CONTROL COVERING SIGNS TYPE II	TRAFFIC CONTROL CONES 42-INCH	TRAFFIC CONTROL SIGNS PCMS	TRAFFIC CONTROL									
0010	PRE-WARNING	7	0	0	0	0	-	-	0	0	0	0	0	0	0	0	0	0	0	0	4	28	-	PRIOR TO CONSTRUCTION
	ENTIRE PROJECT AREA	152	0	0	0	0	-	-	0	0	0	0	39	5928	0	0	0	0	0	0	0	0	1	SDD 15C04, ADVANCED WARNING
	MILLED SURFACE	60	0	0	0	0	-	-	0	0	0	0	37	2220	0	0	0	0	0	0	0	0	-	SDD 15D44, MILLED SURFACES
	DETOUR	28	0	0	19	532	-	-	38	1064	0	0	201	5628	1	25	0	0	0	0	0	0	-	STH 171 - CULVERT WORK - DETOUR
	269+00 - 287+00 - WB	5	12	60	14	70	8	8	4	20	12	60	20	100	0	0	127	635	0	0	0	0	-	BASE PATCHING CONCRETE AREA, GAYS MILLS - WB LANE
	269+00 - 287+00 - EB	5	8	40	17	85	11	11	4	20	8	40	20	100	0	0	127	635	0	0	0	0	-	BASE PATCHING CONCRETE AREA, GAYS MILLS - EB LANE
	INTER. RECONSTRUCTION - STAGE 1	5	49	245	0	0	-	-	0	0	0	0	5	25	0	0	0	0	0	0	0	0	-	STH 171 & STH 131 INTERSECTION
	INTER. RECONSTRUCTION - STAGE 2	14	53	742	14	196	5	5	18	252	7	98	29	406	0	0	0	0	0	0	0	0	-	STH 171 & STH 131 INTERSECTION
	284+79, RT	14	0	0	0	0	-	-	0	0	0	0	0	0	1	1	0	0	0	0	0	0	-	ROUTE ASSEMBLY, STH 131
	292+00, LT	14	0	0	0	0	-	-	0	0	0	0	0	0	1	2	0	0	0	0	0	0	-	ROUTE ASSEMBLY, STH 131
	293+63, LT	14	0	0	0	0	-	-	0	0	0	0	0	0	1	2	0	0	0	0	0	0	-	ROUTE ASSEMBLY, STH 131
	398+00 C, LT	14	0	0	0	0	-	-	0	0	0	0	0	0	1	1	0	0	0	0	0	0	-	ROUTE ASSEMBLY, STH 131
	399+75 C, LT	14	0	0	0	0	-	-	0	0	0	0	0	0	1	1	0	0	0	0	0	0	-	ROUTE ASSEMBLY, STH 131
	INTER. RECONSTRUCTION - STAGE 2	14	0	0	4	56	-	-	8	112	0	0	196	2744	1	2	0	0	4	28	0	0	-	STH 171 & STH 131 - DETOUR SIGNING
	INTER. RECONSTRUCTION - STAGE 3	8	57	456	8	64	6	6	12	96	3	24	4	32	0	0	0	0	0	0	0	0	-	STH 171 & STH 131 INTERSECTION
	INTER. RECONSTRUCTION - STAGE 4	5	35	175	0	0	-	-	0	0	0	0	5	25	0	0	0	0	0	0	0	0	-	STH 171 & STH 131 INTERSECTION
PROJECT TOTALS			1718	1003	30	30	1564	222	17208	34	1270	56	1											

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

3

3

TEMPORARY PEDESTRIAN

CATEGORY	STATION	DAYS	*643.0300		*643.0900		644.1410	644.1430	644.1601	644.1810	LF	NOTES
			QTY.	DAYS	QTY.	DAYS	TEMPORARY PEDESTRIAN SURFACE ASPHALT	TEMPORARY PEDESTRIAN SURFACE PLATE	TEMPORARY PEDESTRIAN CURB RAMP	TEMPORARY PEDESTRIAN BARRICADE		
PERIOD	DRUMS	SIGNS	ASPHALT	PLATE	CURB RAMP	BARRICADE						
APPROX. SERVICE PERIOD	TRAFFIC CONTROL DRUMS	TRAFFIC CONTROL SIGNS	SURFACE ASPHALT	SURFACE PLATE	TEMPORARY PEDESTRIAN CURB RAMP	TEMPORARY PEDESTRIAN BARRICADE	SF	SF	QTY	DAY	LF	NOTES
0010	269+27, RT	4	16	64	5	20	-	-	0	0	48	W ORIN ST - SOUTH SIDE
	272+75, RT	4	16	64	10	40	-	-	0	0	48	POST ST - SOUTH SIDE
	276+47, RT	4	16	64	10	40	-	-	0	0	48	GAY ST - SOUTH SIDE
	280+68, RT	4	16	64	10	40	-	-	0	0	48	RAILROAD ST - SOUTH SIDE
	284+37, RT	4	16	64	10	40	-	-	0	0	48	REBECCA ST - SOUTH SIDE
	269+55, LT	4	16	64	2	8	-	50	2	8	135	W ORIN ST - NORTH SIDE
	272+76, LT	4	16	64	10	40	-	-	0	0	48	POST ST - NORTH SIDE
	276+47, LT	4	16	64	10	40	-	-	0	0	48	GAY ST - NORTH SIDE
	280+76, LT	4	16	64	10	40	-	-	0	0	48	RAILROAD ST - NORTH SIDE
	284+36, LT	4	16	64	10	40	-	-	0	0	48	REBECCA ST - NORTH SIDE
	STH 171 & STH 131 INTER.	22	0	0	8	176	118	-	0	0	30	WEST & SOUTH RAMPS
	STH 171 & STH 131 INTER.	27	0	0	3	81	-	-	0	0	12	NORTH SIDE RAMPS
	383+20 C, LT	4	8	32	1	4	-	-	0	0	12	SCHOOL ST - SOUTH
	383+65 C, LT	4	12	48	1	4	-	-	0	0	12	SCHOOL ST - NORTH
	383+72 C, RT	4	10	40	8	32	-	-	0	0	12	SCHOOL ST - EAST
PROJECT TOTALS				760		645	118	50		8	645	

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

3

MARKING LINE ITEMS

CATEGORY	STATION - STATION	CL LENGTH	LOCATION	646.2040					646.7420		NOTES
				646.2020		MARKING LINE		646.6120		MARKING	
				MARKING LINE		GROOVED WET REF		MARKING		CROSSWALK	
				EPOXY 6-INCH		EPOXY 6-INCH		STOP LINE		EPOXY	
		SOLID	SOLID	SOLID	SOLID	SKIPS	EPOXY	TRANSVERSE			
		WHITE	YELLOW	WHITE	YELLOW	YELLOW	18-INCH	LINE 6-INCH			
		LF	LF	LF	LF	LF	LF	LF			
0010	STA 2+23 - STA 50+00	4,777	CL	-	-	-	8,420	284	-	-	
	STA 2+23 - STA 50+00		LT/RT	-	-	9155	-	-	-	-	
	STA 50+00 - STA 100+00	5,000	CL	-	-	-	8,883	280	-	-	
	STA 50+00 - STA 100+00		LT/RT	-	-	9825	-	-	-	-	
	STA 100+00 - STA 150+00	5,000	CL	-	-	-	8,151	463	-	-	
	STA 100+00 - STA 150+00		LT/RT	-	-	10000	-	-	-	-	
	STA 150+00 - STA 200+00	5,000	CL	-	-	-	10,000	-	-	-	
	STA 150+00 - STA 200+00		LT/RT	-	-	9906	-	-	-	-	
	STA 200+00 - STA 250+00	5,000	CL	-	-	-	7,910	473	-	-	
	STA 200+00 - STA 250+00		LT/RT	-	-	10000	-	-	-	-	
	STA 250+00 - STA 264+62	1,462	CL	-	-	-	2,925	-	-	-	
	STA 250+00 - STA 264+62		LT/RT	-	-	2698	-	-	-	-	
	STA 264+62 - STA 266+78	216	CL	-	427	-	-	-	-	BRIDGE DECK	
	STA 264+62 - STA 266+78		LT/RT	427	-	-	-	-	-	-	
	STA 266+78 - STA 291+30	2,455	CL	-	-	-	2,199	325	-	URBAN - GAYS MILLS	
	STA 266+78 - STA 291+30		LT/RT	-	-	247	-	-	-	-	
	VARIES	-	-	-	-	-	-	-	2,012	26 CROSSINGS	
	STA 382+98 C - STA 396+14 C	1,316	CL	-	-	-	1,211	238	-	STH 131	
	STA 382+98 C - STA 396+14 C		LT/RT	-	-	322	-	-	-	-	
	289+11, LT	-	-	-	-	-	-	-	19	STOP LINE	
	392+29 C, RT	-	-	-	-	-	-	-	19	STOP LINE	
	STA 291+88 - STA 350+00	5,812	CL	-	-	-	11,623	-	-	-	
	STA 291+88 - STA 350+00		LT/RT	-	-	11722	-	-	-	-	
	STA 350+00 - STA 400+00	5,000	CL	-	-	-	10,000	-	-	-	
	STA 350+00 - STA 400+00		LT/RT	-	-	9964	-	-	-	-	
	STA 400+00 - STA 450+00	5,000	CL	-	-	-	10,000	-	-	-	
	STA 400+00 - STA 450+00		LT/RT	-	-	9967	-	-	-	-	
	STA 450+00 - STA 500+00	5,000	CL	-	-	-	10,000	-	-	-	
	STA 450+00 - STA 500+00		LT/RT	-	-	9845	-	-	-	-	
	STA 500+00 - STA 550+00	5,000	CL	-	-	-	10,000	-	-	-	
	STA 500+00 - STA 550+00		LT/RT	-	-	10000	-	-	-	-	
	STA 550+00 - STA 600+00	5,000	CL	-	-	-	8,179	434	-	-	
	STA 550+00 - STA 600+00		LT/RT	-	-	9679	-	-	-	-	
	STA 600+00 - STA 617+55	1,755	CL	-	-	-	3,511	-	-	-	
	STA 600+00 - STA 617+55		LT/RT	-	-	3511	-	-	-	-	
	STA 800+75 - STA 810+58	983	CL	-	-	-	922	247	-	STH 131	
	STA 800+75 - STA 810+58		LT/RT	-	-	-	-	-	-	-	
	SUBTOTAL CATEGORY 0010			427	427		233,519	38	2,012		
	PROJECT TOTALS			854			233519	38	2,012		

TEMPORARY MARKING ITEMS

CATEGORY	STATION - STATION	LOCATION	643.3165			REMARKS
			TEMPORARY MARKING LINE			
			PAINT 6-INCH			
			SOLID	SOLID	SKIPS	
			WHITE	YELLOW	YELLOW	
			LF	LF	LF	
0010	*STA 2+23 - STA 50+00	CL	-	8,420	91	4' SKIPS 50' C-C
	*STA 50+00 - STA 100+00	CL	-	8,883	90	4' SKIPS 50' C-C
	*STA 100+00 - STA 150+00	CL	-	8,151	148	4' SKIPS 50' C-C
	*STA 150+00 - STA 200+00	CL	-	10,000	-	-
	*STA 200+00 - STA 250+00	CL	-	7,910	152	4' SKIPS 50' C-C
	*STA 250+00 - STA 264+62	CL	-	2,925	-	-
	*STA 268+87 - STA 291+13	CL	-	1,421	143	URBAN - GAYS MILLS
	*STA 291+88 - STA 350+00	CL	-	11,623	-	-
	*STA 350+00 - STA 400+00	CL	-	10,000	-	-
	*STA 400+00 - STA 450+00	CL	-	10,000	-	-
	*STA 450+00 - STA 500+00	CL	-	10,000	-	-
	*STA 500+00 - STA 550+00	CL	-	10,000	-	-
	*STA 550+00 - STA 600+00	CL	-	8,179	139	4' SKIPS 50' C-C
	*STA 600+00 - STA 617+55	CL	-	3,511	-	-
	*STA 383+00 C - STA 390+75 C	CL	-	922	79	STH 131
	STA 267+00 - STA 287+00	CL	-	8,020	-	DOUBLE YELLOW, BASE PATCHING AREA, APPLIED TWICE
	STA 269+00 - STA 287+00	CL	-	3,705	-	BASE PATCHING CONCRETE AREA, GAYS MILLS - WB LANE
	STA 269+00 - STA 287+00	CL	-	3,809	-	BASE PATCHING CONCRETE AREA, GAYS MILLS - EB LANE
	INTER. RECONSTRUCTION - STAGE 1	-	866	884	-	STH 171 & STH 131 INTERSECTION
	INTER. RECONSTRUCTION - STAGE 2	-	805	1,144	-	STH 171 & STH 131 INTERSECTION
	PROJECT TOTALS			373,634		

* TOTAL QUANTITY IS APPLIED 3 TIMES OVER CONSTRUCTION TIMEFRAME OF JOB (MILLED SURFACE & 1ST & 2ND LIF)

LOCATING NO PASSING ZONES

CATEGORY	STATION	648.0100
		LOCATING
		NO-PASSING ZONES
MI		
0010	PROJECT	11.66
PROJECT TOTALS		11.66

CONSTRUCTION STAKING

CATEGORY	STATION - STATION	650.4000	650.4500	650.5000	650.5500	650.6000	650.8000	650.9000	650.9500	650.9911	REMARKS
		CONSTRUCTION STAKING SEWER EACH	CONSTRUCTION STAKING SUBGRADE LF	CONSTRUCTION STAKING BASE LF	CONSTRUCTION STAKING CURB GUTTER AND LF	CONSTRUCTION STAKING PIPE CULVERTS EACH	CONSTRUCTION STAKING RESURFACING REFERENCE LF	CONSTRUCTION STAKING CURB RAMPS EACH	CONSTRUCTION STAKING SIDEWALK (5790-02-72) EACH	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (5790-02-72) EACH	
0010	STA 2+24 - 264+62	34	-	-	-	1	26238	-	-	-	
	STA 268+90 - STA 287+02	-	-	-	1398	-	1812	52	1	1	CURB RAMPS
	STA 292+00 - 617+79	35	-	-	-	-	32579	-	-	-	
	STA 346+02 - STA 349+14, RT	-	-	-	338	-	-	-	-	-	
	STH 171 & STH 131 INTER.	11	502	650	-	-	825	-	-	-	INTER. RECONSTRUCTION
	STA 800+22'B' - 803+98'B'	-	-	-	-	-	376	-	-	-	
PROJECT TOTALS		80	502	650	1736	1	61830	52	1	1	

SAWCUT
690.0150 *690.0250
SAWING SAWING
ASPHALT CONCRETE

CATEGORY	STATION	LOCATION	LF	LF	REMARKS
0010	STA 2+23	CL	36	3	BEGIN PROJECT
	STA 2+23 - STA 4+05	LT	182	-	MT STERLING PARKING AREA
	STA 4+94	LT	30	-	3RD STREET
	STA 6+57	LT	31	-	DIVISION ST
	STA 6+29 - STA 8+19	RT, DW	190	-	COMMERCIAL DW
	STA 9+42	LT, DW	23	-	DRIVEWAY
	STA 19+50	LT, DW	41	-	DRIVEWAY
	STA 20+58	LT, DW	19	-	DRIVEWAY
	STA 34+50	RT	30	-	N HALLS BRANCH RD
	STA 39+80	LT	26	-	FREEMAN RD
	STA 41+85	LT, DW	24	-	DRIVEWAY
	STA 80+60	RT	20	-	OLD GAYS RD
	STA 196+85	LT	23	-	STEVENSON RD
	STA 209+60	LT, DW	31	-	DRIVEWAY
	STA 251+55	LT	21	-	W POINT RD
	STA 253+00	RT, DW	20	-	DRIVEWAY
	STA 254+00	RT, DW	17	-	DRIVEWAY
	STA 256+00	RT, DW	13	-	DRIVEWAY
	STA 257+00	LT	27	-	OLD GAYS RD
	STA 264+37	RT	31	-	W RIVER RD
	STA 267+73	RT	9	-	ROBB PARK CROSSING S (ASPH PATH)
	STA 268+87	CL	-	46	BEGIN URBAN SECTION
	STA 269+25	RT	22	-	W ORIN ST
	STA 272+74	LT	45	-	POST ST - NORTH SIDE
	STA 272+74	RT	34	-	POST ST - SOUTH SIDE
	STA 276+45	LT	44	-	GAY ST - NORTH SIDE
	STA 276+45	RT	48	-	GAY ST - SOUTH SIDE
	STA 280+75	LT	36	-	RAILROAD ST - NORTH SIDE
	STA 280+65	RT	16	-	RAILROAD ST - SOUTH SIDE
	STA 284+35	LT	27	-	REBECCA ST - NORTH SIDE
	STA 284+34	RT	34	-	REBECCA ST - SOUTH SIDE

TABLE CONTINUED

SAWCUT

690.0150 *690.0250
SAWING SAWING
ASPHALT CONCRETE

CATEGORY	STATION	LOCATION	LF	LF	REMARKS
0010	STA 287+02	-	-	58	MAINLINE-RECONSTRUCT LIMIT
	STA 394+75 C	-	35	-	END URBAN SECTION, INTERS. W/ STH 131, NORTH
	STA 290+00	-	-	45	END RECONSTRUCT AREA LIMIT
	STA 289+34	RT	-	4	SW MATCH IN - INTERSECTION SOUTH CORNER
	STA 289+70	LT	-	5	SW MATCH IN - INTERSECTION EAST CORNER
	STA 289+62	RT	-	13	DRIVEWAY
	STA 291+75	LT	22	-	END OF CONSTRUCTION LIMITS
	STA 298+10	LT, DW	32	-	DRIVEWAY
	STA 318+10	LT	43	-	OVERLOOK ENTRANCE
	STA 319+62	LT	38	-	OVERLOOK ENTRANCE
	STA 326+76	LT	32	-	OVERLOOK ENTRANCE
	STA 328+52	LT	58	-	OVERLOOK ENTRANCE
	STA 345+70	LT, DW	114	-	COMMERCIAL DW
	STA 346+18 - STA 348+47	LT	227	-	C&G PATCH
	STA 400+00	RT	26	-	S FRANK DR
	STA 485+15	LT	25	-	DEL LA MATER RD
	STA 505+58 - STA 506+55	LT	97	-	C&G PATCH
	STA 506+83	LT, DW	81	-	DRIVEWAY
	STA 507+15 - STA 507+69	LT	54	-	C&G PATCH
	STA 522+00	LT, DW	16	-	DRIVEWAY
	STA 548+58	LT, DW	27	-	DRIVEWAY
	STA 555+60	LT	33	-	ORCHARD VIEW RD
	STA 582+50	RT	40	-	SAND CREEK RD
	STA 597+42	RT	32	-	OLD 61 RD
	STA 600+37	LT, DW	37	-	DRIVEWAY
	STA 617+55	CL	37	-	END PROJECT
	STA 382+98 C	CL	26	-	STH 131 MATCH IN
	STA 383+20 C	LT	13	7	CURB RAMP, SOUTH SIDE, SCHOOL ST
	STA 383+71 C	LT	63	7	CURB RAMP, NORTH SIDE, SCHOOL ST
	STA 383+72 C	RT	-	4	CURB RAMP, RT SIDE, STH 131
	STA 383+41 C	LT	42	-	SCHOOL ST
	STA 384+84 C	RT	13	-	DRIVEWAY
	STA 385+80 C	RT	35	-	DRIVEWAY
	STA 386+62 C	LT	36	-	PARK ST
	STA 390+00 C	LT	49	-	ORIN ST
	STA 390+97 C	LT	18	-	DRIVEWAY
	STA 391+23 C	-	24	-	RECONSTRUCT LIMITS
	STA 391+85 C	RT	-	4	SOUTH SIDEWALK MATCH IN
SUBTOTAL CATEGORY 0010			2575	196	

PROJECT TOTALS 2575 196

*ADDITIONAL QUANTITIES SHOWHN ELSEWHERE IN PLANS

3

3

PWL MIXTURE USE TABLE

LOCATION	STATION - STATION	MIXTURE USE	UNDERLYING SURFACE	BID ITEM	TONS	THICKNESS	MIXTURE ACCEPTANCE	DENSITY ACCEPTANCE
11-FOOT DRIVING LANE	2+24 - 264+62	LOWER LAYER	MILL & RELAY ASPHALT	4 LT 58-28 S	6285	1.75"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT SPV.0055.02	INCENTIVE DENSITY PWL HMA PAVEMENT SPV.0055.01
	290+00 - 617+79				7853			
	382+98 C - 391+23 C				198			
	STA 800+22'B' - 803+57'B'				80			
12-FOOT DRIVING LANE	268+90 - 287+02	LOWER LAYER	CONCRETE	HMA INTERLAYER	271	1.00"	QMP AS PER SS 460.	INCENTIVE DENSITY HMA PAVEMENT 460.2000
	287+02 - 290+00		BASE AGGREGATE	4 LT 58-28 S	134	3.00"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT SPV.0055.02	INCENTIVE DENSITY PWL HMA PAVEMENT SPV.0055.01
	STH 171 & STH 131 INTERSECTION		BASE AGGREGATE	4 LT 58-28 S	104	3.00"		
	391+23 C - 392+15 C		BASE AGGREGATE	4 LT 58-28 S	42	3.00"		
11-FOOT DRIVING LANE	2+24 - 264+62	UPPER LAYER	4 LT 58-28 S	4 LT 58-28 S	6285	1.75"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT SPV.0055.02	INCENTIVE DENSITY PWL HMA PAVEMENT SPV.0055.01
	290+00 - 617+79				7853			
	382+98 C - 391+23 C				198			
	STA 800+22'B' - 803+57'B'				80			
12-FOOT DRIVING LANE	268+90 - 287+02	UPPER LAYER	HMA INTERLAYER	5 MT 58-28 S	338	1.25"	QMP AS PER SS 460.	INCENTIVE DENSITY HMA PAVEMENT 460.2000
	287+02 - 290+00		4 LT 58-28 S		56			
	STH 171 & STH 131 INTERSECTION		4 LT 58-28 S		44			
	391+23 C - 392+15 C		4 LT 58-28 S		17			
VARIABLE WIDTH SHOULDER	2+24 - 264+62	LOWER LAYER	MILL & RELAY ASPHALT	4 LT 58-28 S	823	1.75"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT SPV.0055.02	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
	268+90 - 287+02		CONCRETE	HMA INTERLAYER	173	1.00"	QMP AS PER SS 460.	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
	287+02 - 290+00		BASE AGGREGATE	4 LT 58-28 S	248	3.00"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT SPV.0055.02	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
	290+00 - 617+79		MILL & RELAY ASPHALT	4 LT 58-28 S	700	1.75"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT SPV.0055.02	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
	STH 171 & STH 131 INTERSECTION		BASE AGGREGATE	4 LT 58-28 S	278	3.00"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT SPV.0055.02	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
	391+23 C - 392+15 C		BASE AGGREGATE	4 LT 58-28 S	14	3.00"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT SPV.0055.02	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
VARIABLE WIDTH SHOULDER	2+24 - 264+62	UPPER LAYER	4 LT 58-28 S	4 LT 58-28 S	823	1.75"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
	268+90 - 287+02		HMA INTERLAYER	5 MT 58-28 S	217	1.25"	QMP AS PER SS 460.	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
	287+02 - 290+00		4 LT 58-28 S	5 MT 58-28 S	103	1.25"	QMP AS PER SS 460.	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
	290+00 - 617+79		4 LT 58-28 S	4 LT 58-28 S	700	1.75"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT SPV.0055.02	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
	STH 171 & STH 131 INTERSECTION		4 LT 58-28 S	5 MT 58-28 S	115	1.25"	QMP AS PER SS 460.	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
	391+23 C - 392+15 C		4 LT 58-28 S	5 MT 58-28 S	6	1.25"	QMP AS PER SS 460.	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
SIDE ROADS	2+24 - 617+79	UPPER LAYER	MILL & RELAY ASPHALT	4 LT 58-28 S	196	3.00"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT SPV.0055.02	INCENTIVE DENSITY HMA PAVEMENT 460.2000

PROJECT NO 57900272

HW STH 171

COUNT CRAWFORD

MISCELLANEOUS QUANTITIES

SHEET

E

EARTHWORK SUMMARY (PROJECT ID 5790-02-72)

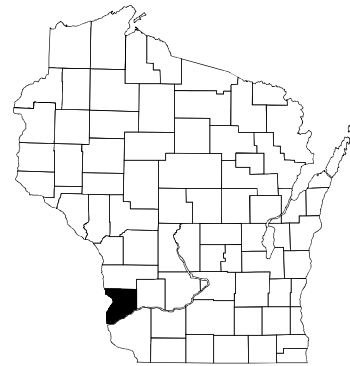
CATEGORY 0010

SUBSTAGE	FROM/TO STATION	LOCATION	205.0100 COMMON EXCAVATION (1)	SALVAGED / UNUSABLE PAVEMENT MATERIAL (3)	AVAILABLE MATERIAL (4)	205.0200 EXCAVATION ROCK	UNEXPANDED FILL	EXPANDED FILL (5)	MASS ORDINATE +/- (6)	208.0100 BORROW	COMMENT:
			CUT (2)					FACTOR 1.25			
1	2+55'TC' - 6+17'TC'	STH 131 TEMP	209	64	145	0	16	20	125	0	
	84+60 - 137+96	STH 171	9	0	9	48	529	661	-652	652	
	139+96 - 142+56	STH 171	0	0	0	0	18	23	-23	23	
	145+00 - 207+15	STH 171	138	0	138	288	400	500	-362	362	
	234+43 - 236+28	STH 171	1	0	1	0	6	8	-7	7	
	250+70 - 263+26	STH 171	10	0	10	18	221	276	-266	266	
STAGE 1 SUBTOTALS			1,085	64	1,021	354	1,345	1,681	-660	1,310	
2	320+53 352+89	STH 171	643	0	643	0	99	124	519	0	
	365+36 - 373+84	STH 171	2	0	2	0	38	48	-46	0	
	379+00 - 384+19	STH 171	1	0	1	0	3	4	-3	0	
	395+86 - 408+03	STH 171	36	0	36	0	0	0	36	0	
	455+48 - 459+67	STH 171	1	0	1	0	3	4	-3	0	
	591+84 - 600+32	STH 171	35	0	35	0	12	15	20	0	
STAGE 2 SUBTOTALS			1,478	466	1,012	0	148	185	827	0	
2	288+99 - 289+97	STH 171	367	108	259	0	32	40	219	0	
	391+23 - 392+85	STH 131	365	104	261	0	47	59	202	0	
	9+32'A' - 11+74'A'	STH 171	427	119	308	0	16	20	288	0	
	INTERSECTION	STH 171 / STH 131	188	120	68	0	45	56	12	0	(7)
	10+34'TC2' - 13+28'TC2'	STH 131 TEMP	131	15	116	0	8	10	106	0	(7)
STAGE 2 SUBTOTALS			1,478	466	1,012	0	148	185	827	0	
3	9+32'A' - 13+13'A'	STH 171 / STH 131	643	135	508	0	16	20	488	0	
STAGE 3 SUBTOTALS			643	135	508	0	16	20	488	0	
4	11+00'A' - 11+74'A'	STH 131	80	9	71	0	0	0	71	0	
STAGE 4 SUBTOTALS			80	9	71	0	0	0	71	0	
PROJECT TOTALS			3,286			354			726	1,310	

NOTES:

- (1) COMMON EXCAVATION IS THE SUM OF THE CUT . ITEM NUMBER 205.0100
- (2) SALVAGED/UNUSABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.
- (3) SALVAGED/UNUSABLE PAVEMENT MATERIAL
- (4) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSABLE PAVEMENT MATERIAL
- (5) EXPANDED FILL FACTOR = 1.25
- DEPENDENT ON SELECTIONS:
- (6) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE STAGE. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE STAGE. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE STAGE.
- (7) THE CUT AND FILL VALUES WERE OBTAINED FROM A SURFACE TO SURFACE VOLUMN COMPUTATION.

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION TRANSPORTATION PROJECT PLAT TITLE SHEET 5790-02-22 FERRYVILLE - ROLLING GROUND STH 27 TO USH 61 STH 171 CRAWFORD COUNTY



CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	●
QUARTER LINE	---	SECTION CORNER MONUMENT	□	NON-MONUMENTED R/W POINT	○
SIXTEENTH LINE	---	GEODETIC SURVEY MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	IP ○
NEW REFERENCE LINE	---	SIXTEENTH CORNER MONUMENT		OFF-PREMISE SIGN	
NEW R/W LINE	---	SIGN			
EXISTING R/W OR HE LINE	---				
PROPERTY LINE	---				
LOT, TIE & OTHER MINOR LINES	---				
SLOPE INTERCEPT	---				
CORPORATE LIMITS	---				
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)	---				
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---				
TEMPORARY LIMITED EASEMENT AREA	---				
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---				
TRANSMISSION STRUCTURES	---				
BUILDING		TO BE REMOVED		PARCEL NUMBER (25)	UTILITY NUMBER (40)
BRIDGE		CULVERT		PARALLEL OFFSETS	

CONVENTIONAL ABBREVIATIONS

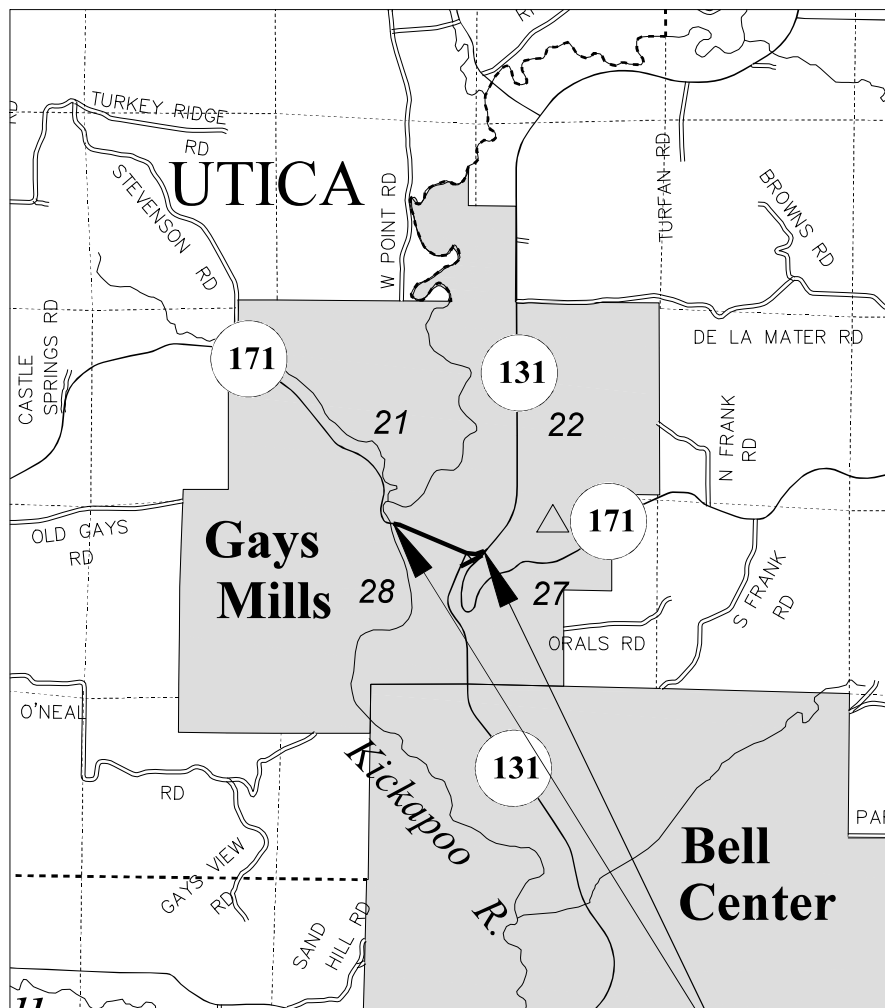
ACCESS RIGHTS	AR	POINT OF COMPOUND CURVE	PCC
ACRES	AC	POINT OF INTERSECTION	PI
AHEAD	AH	PROPERTY LINE	PL
ALUMINUM	ALUM	RECORDED AS (100')	
AND OTHERS	ET AL	REEL / IMAGE	R/I
BACK	BK	REFERENCE LINE	R/L
BLOCK	BLK	REMAINING	REM
CENTERLINE	C/L	RESTRICTIVE DEVELOPMENT	RDE
CERTIFIED SURVEY MAP	CSM	EASEMENT	
CONCRETE	CONC	RIGHT	RT
COUNTY	CO	RIGHT OF WAY	R/W
COUNTY TRUNK HIGHWAY	CTH	SECTION	SEC
DISTANCE	DIST	SEPTIC VENT	SEPV
CORNER	COR	SQUARE FEET	SF
DOCUMENT NUMBER	DOC	STATE TRUNK HIGHWAY	STH
EASEMENT	EASE	STATION	STA
EXISTING	EX	TELEPHONE PEDESTAL	TP
GAS VALVE	GV	TEMPORARY LIMITED EASEMENT	TLE
GRID NORTH	GN		
HIGHWAY EASEMENT	HE	TRANSPORTATION PROJECT PLAT	TPP
IDENTIFICATION	ID	UNITED STATES HIGHWAY	USH
LAND CONTRACT	LC	VOLUME	V
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY NUMBER	NGS		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL UTILITY SYMBOLS

WATER	---
GAS	---
TELEPHONE	---
OVERHEAD TRANSMISSION LINES	---
ELECTRIC	---
CABLE TELEVISION	---
FIBER OPTIC	---
SANITARY SEWER	---
STORM SEWER	---
ELECTRIC TOWER	⊠



R 4 W

LAYOUT SCALE 0 0.5 MI.

PROJECT LOCATION

THE NOTES, CONVENTIONAL SIGNS, AND ABBREVIATIONS ARE ASSOCIATED WITH EACH TRANSPORTATION PROJECT PLAT FOR PROJECT 5790-02-22.

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), CRAWFORD COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, CENTERLINE OF EXISTING PAVEMENTS AND/OR EXISTING OCCUPATIONAL LINES.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

PROPERTY LINES SHOWN ON THIS PLAT FOR PROPERTIES BEING IMPACTED ARE DRAWN FROM DATA DERIVED FROM FILED/RECORDED MAPS AND DOCUMENTS OF PUBLIC RECORD. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN THE SW REGION - LA CROSSE.

PARCEL AND UTILITY IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE TPP DETAIL PAGES.

INFORMATION FOR THE BASIS OF EXISTING HIGHWAY RIGHT-OF-WAY POINTS OF REFERENCE AND ACCESS CONTROL ARE LISTED ON THE TPP DETAIL PAGES.

PROJECT NUMBER 5790-02-22-4.01
SHEET 2 OF 2
AMENDMENT NO:

TRANSPORTATION PROJECT PLAT NO: 5790-02-22-4.01

THAT PART OF LOTS 11 AND 12, BLOCK 15, PART OF LOTS 1, 2 AND 12, BLOCK 14, PART OF LOT 12 BLOCK 13, PART OF LOT 10 AND 13, BLOCK 4, PART OF LOTS 1, 11 AND 12, BLOCK 16, PART OF LOT 1, BLOCK 17, ALL IN ASSESSOR'S PLAT OF MAIN VILLAGE GAYS MILLS, AND PART OF LOTS 9 AND 10, BLOCK 3, RAILROAD ADDITION, AND PART OF LOT 1, CSM 826, V 6, P 120, DOC 263278, ALL LOCATED IN PART OF GOV LOT 1, SECTION 28, ALSO THAT PART OF LOT 4, BLOCK 4, RAILROAD ADDITION, LOCATED IN PART OF GOV LOT 4, SECTION 28, ALL IN TOWNSHIP 10 NORTH, RANGE 4 WEST, VILLAGE OF GAYS MILLS, CRAWFORD COUNTY, WISCONSIN.

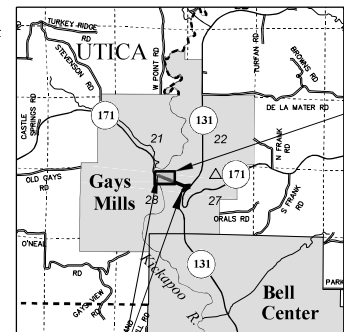
RELOCATION ORDER STH 171 FERRYVILLE - ROLLING GROUND, STH 27 TO USH 61, CRAWFORD COUNTY.

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE NAMED PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

- THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
- THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

LOCATION SKETCH NOT TO SCALE



PROJECT LOCATION

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), CRAWFORD COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN THE SW REGION - LA CROSSE.

SIXTEENTH LINES WERE ESTABLISHED BY SECTION BREAKDOWN OF OBSERVED PLS MONUMENTS.

SCHEDULE OF LANDS & INTERESTS REQUIRED

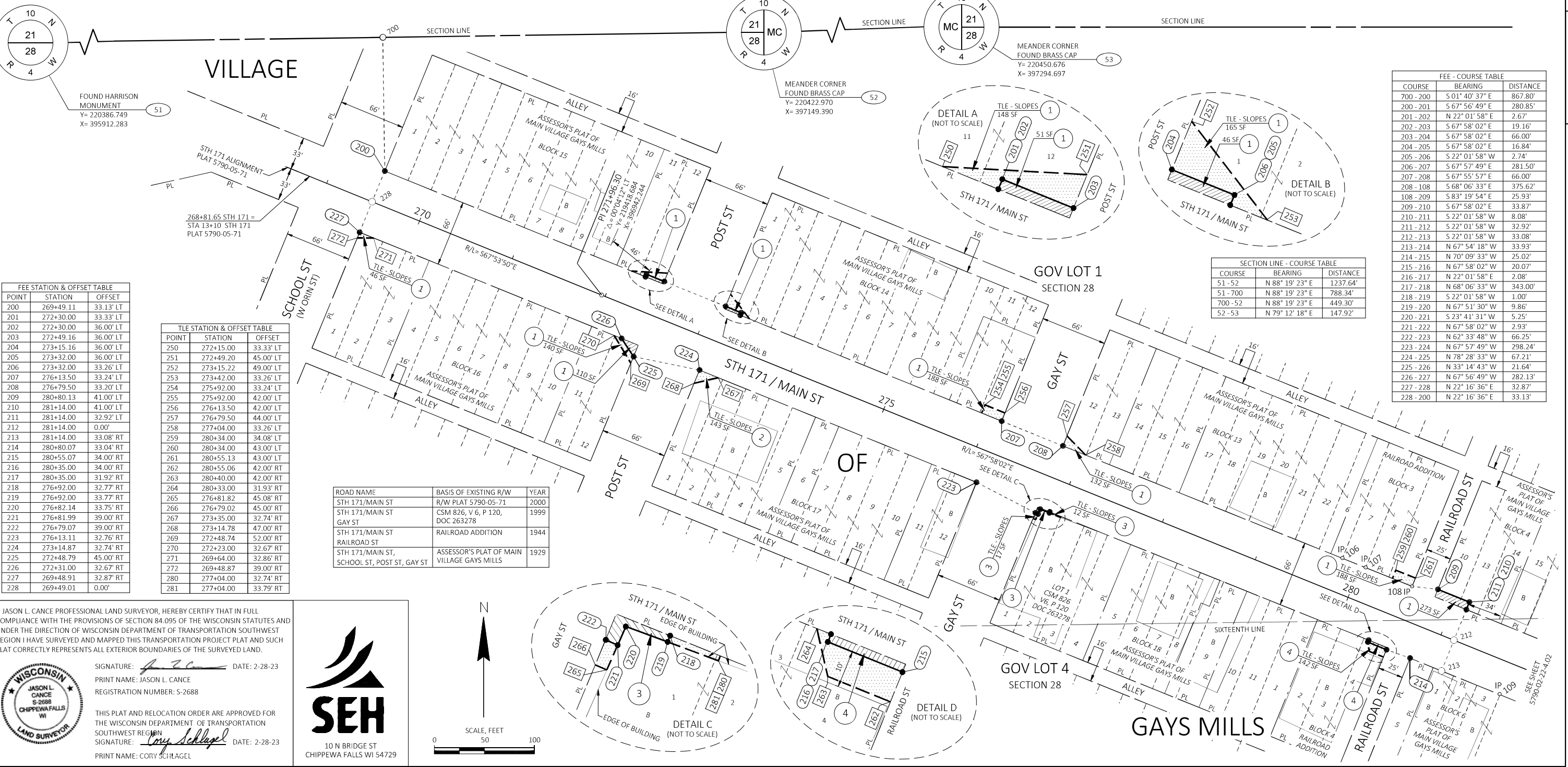
PARCEL NUMBER	OWNER(S)	INTERESTS REQUIRED	R/W SQUARE FEET REQUIRED			TLE SQUARE FEET
			NEW	EXISTING	TOTAL	
1	VILLAGE OF GAYS MILLS	FEE / TLE	480	-	480	1007
2	RICHARD M. DUDGEON	TLE	-	-	-	143
3	JAMES D. AND SHARON L. CHELLEVOLD	FEE / TLE	28	-	28	29
4	JAMES AND ALLISON SHOWN	FEE / TLE	42	-	42	142

RECOVERED MONUMENTS					
POINT	Y	X	DESCRIPTION	STATION	OFFSET
106	219156.419	397681.593	1" IRON PIPE	279+80.04	34.24' LT
107	219146.970	397704.806	1" IRON PIPE	280+05.11	34.19' LT
108	219128.147	397751.148	1" IRON PIPE	280+55.12	34.13' LT
109	219020.562	397838.432	1" REBAR	281+76.39	32.86' RT

350867
RECORDED 03/01/2023 12:13 PM
WELISSA C. HAGEM
REGISTER OF DEEDS OFFICE
CRAWFORD COUNTY, WI
RECORDING FEE 25.00

PAGES: 2
VOLUME: 3PL PAGE: 162
The above recording information verifies that this document has been electronically recorded and returned to the submitter.

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 5790-02-22-4.01
SHEET 1 OF 2



FEE STATION & OFFSET TABLE		
POINT	STATION	OFFSET
200	269+49.11	33.13' LT
201	272+30.00	33.33' LT
202	272+30.00	36.00' LT
203	272+49.16	36.00' LT
204	273+15.16	36.00' LT
205	273+32.00	36.00' LT
206	273+32.00	33.26' LT
207	276+13.50	33.24' LT
208	276+79.50	33.20' LT
209	280+80.13	41.00' LT
210	281+14.00	41.00' LT
211	281+14.00	32.92' LT
212	281+14.00	0.00'
213	281+14.00	33.08' RT
214	280+80.07	33.04' RT
215	280+55.07	34.00' RT
216	280+35.00	34.00' RT
217	280+35.00	31.92' RT
218	276+92.00	32.77' RT
219	276+92.00	33.77' RT
220	276+82.14	33.75' RT
221	276+81.99	39.00' RT
222	276+79.07	39.00' RT
223	276+13.11	32.76' RT
224	273+14.87	32.74' RT
225	272+48.79	45.00' RT
226	272+31.00	32.67' RT
227	269+48.91	32.87' RT
228	269+49.01	0.00'

TLE STATION & OFFSET TABLE		
POINT	STATION	OFFSET
250	272+15.00	33.33' LT
251	272+49.20	45.00' LT
252	273+15.22	49.00' LT
253	273+42.00	33.26' LT
254	275+92.00	33.24' LT
255	275+92.00	42.00' LT
256	276+13.50	42.00' LT
257	276+79.50	44.00' LT
258	277+04.00	33.26' LT
259	280+34.00	34.08' LT
260	280+34.00	43.00' LT
261	280+55.13	43.00' LT
262	280+55.06	42.00' RT
263	280+40.00	42.00' RT
264	280+33.00	31.93' RT
265	276+81.82	45.08' RT
266	276+79.02	45.00' RT
267	273+35.00	32.74' RT
268	273+14.78	47.00' RT
269	272+48.74	52.00' RT
270	272+23.00	32.67' RT
271	269+64.00	32.86' RT
272	269+48.87	39.00' RT
280	277+04.00	32.74' RT
281	277+04.00	33.79' RT

ROAD NAME	BASIS OF EXISTING R/W	YEAR
STH 171/MAIN ST	R/W PLAT 5790-05-71	2000
STH 171/MAIN ST	CSM 826, V 6, P 120, DOC 263278	1999
GAY ST	DOC 263278	
STH 171/MAIN ST	RAILROAD ADDITION	1944
RAILROAD ST		
STH 171/MAIN ST, SCHOOL ST, POST ST, GAY ST	ASSESSOR'S PLAT OF MAIN VILLAGE GAYS MILLS	1929

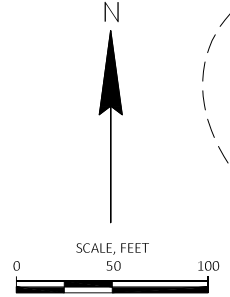
SECTION LINE - COURSE TABLE		
COURSE	BEARING	DISTANCE
51-52	N 88° 19' 23" E	1237.64'
51-700	N 88° 19' 23" E	788.34'
700-52	N 88° 19' 23" E	449.30'
52-53	N 79° 12' 18" E	147.92'

FEE - COURSE TABLE		
COURSE	BEARING	DISTANCE
700-200	S 01° 40' 37" E	867.80'
200-201	S 67° 56' 49" E	280.85'
201-202	N 22° 01' 58" E	2.67'
202-203	S 67° 58' 02" E	19.16'
203-204	S 67° 58' 02" E	66.00'
204-205	S 22° 01' 58" W	16.84'
205-206	S 22° 01' 58" W	2.74'
206-207	S 67° 57' 49" E	281.50'
207-208	S 67° 55' 57" E	66.00'
208-108	S 68° 06' 33" E	375.62'
108-209	S 83° 19' 54" E	25.93'
209-210	S 67° 58' 02" E	33.87'
210-211	S 22° 01' 58" W	8.08'
211-212	S 22° 01' 58" W	32.92'
212-213	S 22° 01' 58" W	33.08'
213-214	N 67° 54' 18" W	33.93'
214-215	N 70° 09' 33" W	25.02'
215-216	N 67° 58' 02" W	20.07'
216-217	N 22° 01' 58" E	2.08'
217-218	N 68° 06' 33" W	343.00'
218-219	S 22° 01' 58" W	1.00'
219-220	N 67° 51' 30" W	9.86'
220-221	S 23° 41' 31" W	5.25'
221-222	N 67° 58' 02" W	2.93'
222-223	N 62° 33' 48" W	66.25'
223-224	N 67° 57' 49" W	298.24'
224-225	N 78° 28' 33" W	67.21'
225-226	N 33° 14' 43" W	21.64'
226-227	N 67° 56' 49" W	282.13'
227-228	N 22° 16' 36" E	32.87'
228-200	N 22° 16' 36" E	33.13'

I, JASON L. CANCE PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF WISCONSIN DEPARTMENT OF TRANSPORTATION SOUTHWEST REGION I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.



SIGNATURE: *Jason L. Cance* DATE: 2-28-23
 PRINT NAME: JASON L. CANCE
 REGISTRATION NUMBER: S-2688
 THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION SOUTHWEST REGION
 SIGNATURE: *Cory Schlagel* DATE: 2-28-23
 PRINT NAME: CORY SCHLAGEL



TRANSPORTATION PROJECT PLAT NO: 5790-02-22-4.02 AMENDMENT NO: 2

AMENDS PARCEL NO. 13 OF TRANSPORTATION PROJECT PLAT NO: 5790-02-22-4.02 RECORDED AS DOCUMENT NO. 350947

THAT PART OF LOT 24, BLOCK 4, PART OF LOT 12 AND LOTS 19 THRU 24, BLOCK 5, PART OF LOT 12, BLOCK 6, PART OF LOT 1 AND LOTS 9 THRU 12, AND LOT 24, BLOCK 7, ALL IN ASSessor'S PLAT OF MAIN VILLAGE GAYS MILLS, PART OF LOTS 9 AND 10, SHERWOOD ADDITION TO THE VILLAGE OF GAYS MILLS, ALL LOCATED IN PART OF GOV LOT 4, ALL IN SECTION 28, ALSO PART OF LOT 19 AND OUTLOT 18, ASSessor'S PLAT OF MAIN VILLAGE GAYS MILLS, AND PART OF LOT 9, SHERWOOD ADDITION TO THE VILLAGE OF GAYS MILLS, ALL LOCATED IN PART OF THE SW 1/4 OF THE NW 1/4, SECTION 27, ALL IN TOWNSHIP 10 NORTH, RANGE 4 WEST, VILLAGE OF GAYS MILLS, CRAWFORD COUNTY, WISCONSIN.

RELOCATION ORDER STH 171 FERRYVILLE - ROLLING GROUND, STH 27 TO USH 61, CRAWFORD COUNTY.

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE NAMED PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:
 1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
 2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED IN THE OFFICE OF THE REGISTER OF DEEDS IN CRAWFORD COUNTY AS SHEET 2 OF 2 DOCUMENT NUMBER 350867.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), CRAWFORD COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 1/2" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN THE SW REGION - LA CROSSE.

SIXTEENTH LINES WERE ESTABLISHED BY SECTION BREAKDOWN OF OBSERVED PLSS MONUMENTS.

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO TRANSFER OF LAND INTERESTS TO THE DEPARTMENT.

SCHEDULE OF LANDS & INTERESTS REQUIRED

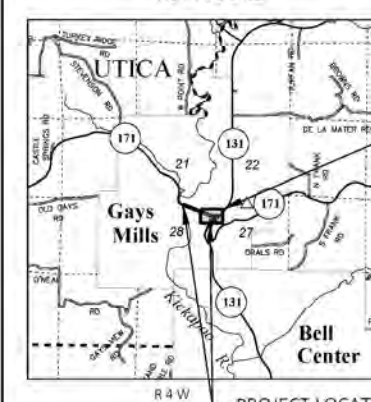
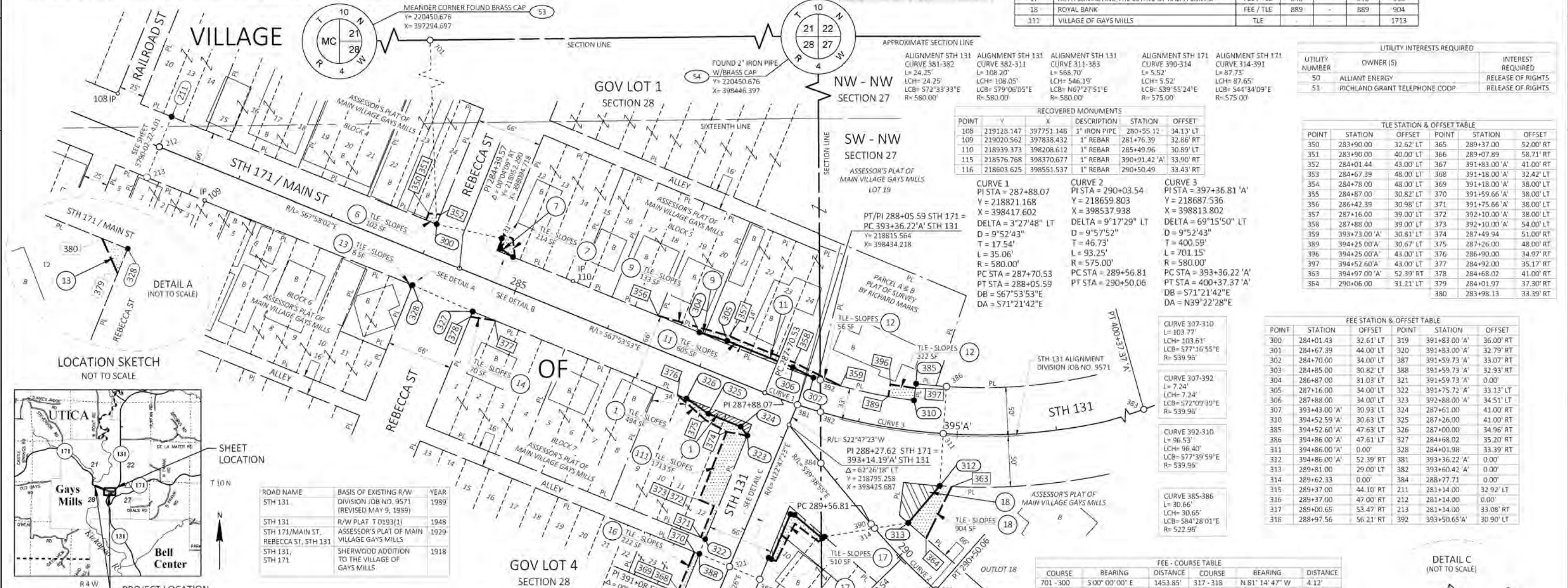
PARCEL NUMBER	OWNER(S)	INTERESTS REQUIRED	R/W SQUARE FEET REQUIRED			TLE SQUARE FEET
			NEW	EXISTING	TOTAL	
1	VILLAGE OF GAYS MILLS	FEE / TLE	377	-	377	494
6	ALAN AND SHERRY BAHR	TLE	-	-	-	102
7	RICK E. HADLEY	FEE / TLE	45	-	45	214
9	DANNY E. AND BARBARA J. EITSERT	FEE / TLE	1	-	1	133
11	PAUL E. DILLEY	FEE / TLE	292	-	292	605
12	CORTNEY DUCHARME	TLE	-	-	-	378
13	CLETUS J. AND JEAN ANN PROCHASKA	TLE	-	-	-	8
14	DANNY W. AND ANGELA F. WARDWELL	TLE	-	-	-	70
16	CRAIG L. ANDERSON	TLE	-	-	-	222
17	RUTH LOMAS AND THE ESTATE OF RALPH LOMAS	FEE / TLE	340	-	340	510
18	ROYAL BANK	FEE / TLE	889	-	889	904
111	VILLAGE OF GAYS MILLS	TLE	-	-	-	1713

351581
 RECORDED 06/16/2023 01:22 PM
 MELISSA C NAGEL
 REGISTER OF DEEDS OFFICE
 CRAWFORD COUNTY, WI
 RECORDING FEE 25.00
 PAGES: 1
 VOLUME: 3PL PAGE: 176
 The above recording information verifies that this document has been electronically recorded and returned to the submitter.

RESERVED FOR REGISTER OF DEEDS
 PROJECT NUMBER 5790-02-22-4.02
 AMENDMENT NO. 2

UTILITY EASEMENT TABLE			
UTILITY NUMBER	PARCEL	RECORDING INFORMATION	NAME
50	18	V. 159 P. 275, DOC. 132862 V. 159 P. 272, DOC. 132850 V. 348 P. 303, DOC. 136047	ALLIANT ENERGY
51	17	DOC. 330572 DOC. 330575 V. 146, PG. 625, DOC. 112491	RICHLAND GRANT TELEPHONE COOP

SECTION LINE - COURSE TABLES		
COURSE	BEARING	DISTANCE
53-701	N 90° 00' 00" E	776.89'
701-54	N 90° 00' 00" E	374.81'
53-54	N 90° 00' 00" E	1151.70'
54-392	S 00° 22' 50" E	1610.08'
392-382	S 00° 22' 50" E	32.32'
382-384	S 00° 22' 50" E	51.62'
384-55	S 00° 22' 50" E	1000.97'
54-55	S 00° 22' 50" E	2694.99'



ROAD NAME	BASIS OF EXISTING R/W	YEAR
STH 131	DIVISION JOB NO. 9571 (REVISED MAY 9, 1989)	1989
STH 131	R/W PLAT T 0193(1)	1948
STH 171/MAIN ST, REBECCA ST, STH 131	ASSessor'S PLAT OF MAIN VILLAGE GAYS MILLS	1929
STH 131, STH 171	SHERWOOD ADDITION TO THE VILLAGE OF GAYS MILLS	1918

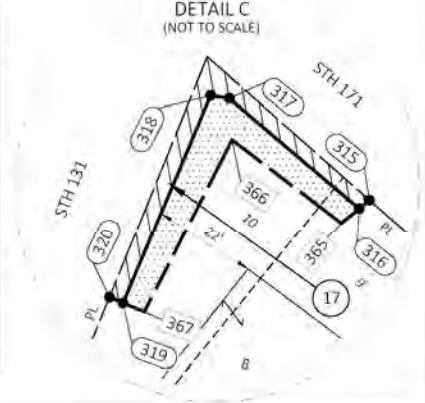
I, JASON L. CANCE PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF WISCONSIN DEPARTMENT OF TRANSPORTATION SOUTHWEST REGION I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: *J. Cance* DATE: 6-15-23
 PRINT NAME: JASON L. CANCE
 REGISTRATION NUMBER: S-2688

THIS PLAT AND RELOCATION ORDER ARE REQUIRED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION SOUTHWEST REGION
 SIGNATURE: *Cory Schlagel* DATE: 6-15-23
 PRINT NAME: CORY SCHLAGEL



FEE - COURSE TABLE			
COURSE	BEARING	DISTANCE	COURSE
701-300	S 00° 00' 00" E	1453.85'	317-318
300-301	S 77° 43' 49" E	66.99'	318-319
301-302	S 07° 28' 55" W	10.33'	319-320
302-303	S 55° 55' 16" E	15.33'	320-387
303-304	S 67° 57' 28" E	202.00'	387-321
304-305	S 73° 45' 00" E	29.15'	321-388
305-306	S 68° 05' 53" E	70.97'	388-322
306-307	S 63° 13' 11" E	23.21'	322-323
307-308	S 07° 12' 45" E	17.00'	323-324
308-311	S 03° 50' 32" W	47.61'	324-326
311-312	S 03° 50' 32" W	52.39'	326-327
312-313	S 40° 24' 59" W	48.94'	327-328
313-314	S 80° 59' 22" W	34.23'	328-313
314-315	S 80° 12' 28" W	50.88'	313-211
315-316	S 50° 21' 05" W	2.90'	211-300
316-317	N 49° 44' 31" W	36.93'	S 67° 54' 18" E



NOTES:
THIS EXHIBIT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSES ONLY.
REFER TO THE CONVEYANCE DOCUMENT FOR PARCEL RELATED DETAILS.

PURPOSE OF TLE IS FOR GRADING.

R/W PROJECT NUMBER: 5790-02-22

EXHIBIT NUMBER: 1

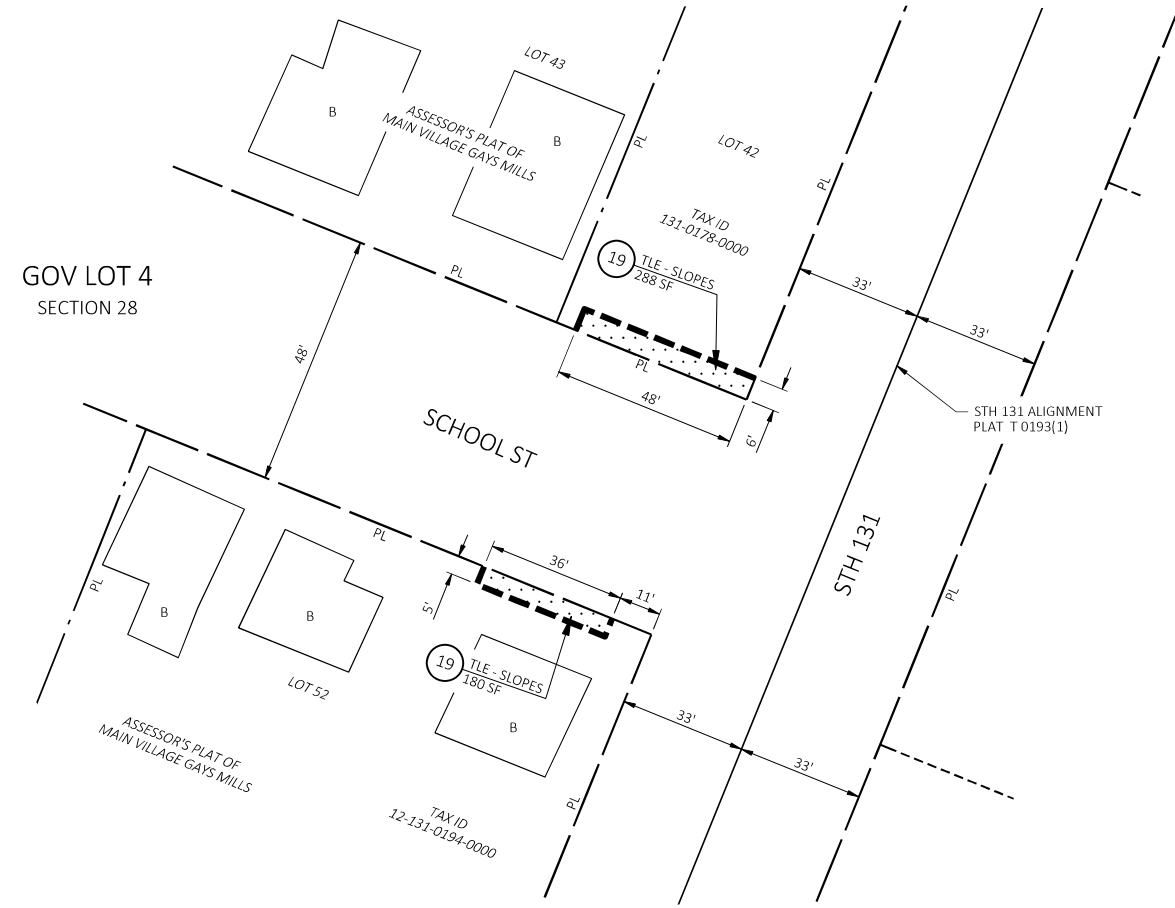
TLE ACQUISITION EXHIBIT
FERRYVILLE - ROLLING GROUND
STH 27 TO USH 61

STH 171

CRAWFORD COUNTY

THAT PART OF LOT 42, AND PART OF LOT 52, ASSESSOR'S PLAT OF MAIN VILLAGE GAYS MILLS
LOCATED IN PART OF GOVERNMENT LOT 4, SECTION 28, T10N, R4W, VILLAGE OF GAYS MILLS,
CRAWFORD COUNTY, WISCONSIN.

4



SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE S.F.
19	KEVIN J. AND PATRICIA J. MURRAY	TLE	468

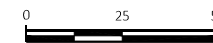
UTILITY INTERESTS REQUIRED

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
N/A	N/A	N/A

N

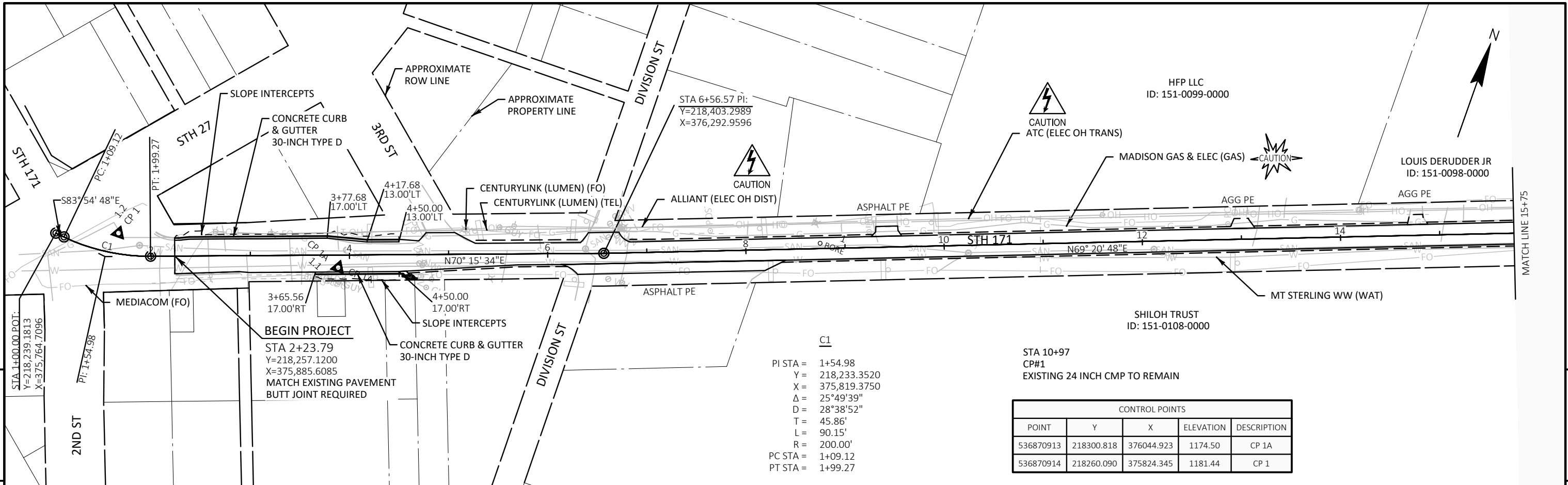


SCALE, FEET



THIS EXHIBIT IS APPROVED FOR THE DEPARTMENT OF TRANSPORTATION, SOUTHWEST REGION - LA CROSSE, WISCONSIN.

SIGNATURE: *Cory Schlagel* DATE: 2-23-23
PRINT NAME: CORY SCHLAGEL



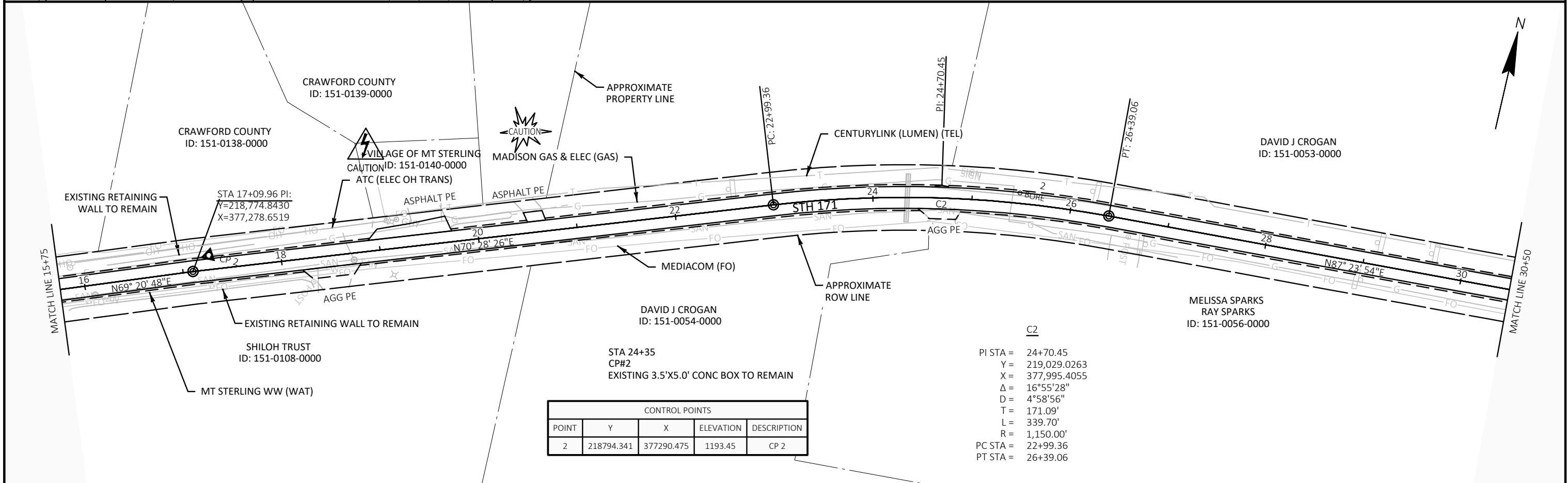
BEGIN PROJECT
 STA 2+23.79
 Y=218,257.1200
 X=375,885.6085
 MATCH EXISTING PAVEMENT
 BUTT JOINT REQUIRED

C1
 PI STA = 1+54.98
 Y = 218,233.3520
 X = 375,819.3750
 Δ = 25°49'39"
 D = 28°38'52"
 T = 45.86'
 L = 90.15'
 R = 200.00'
 PC STA = 1+09.12
 PT STA = 1+99.27

CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
536870913	218300.818	376044.923	1174.50	CP 1A
536870914	218260.090	375824.345	1181.44	CP 1

5

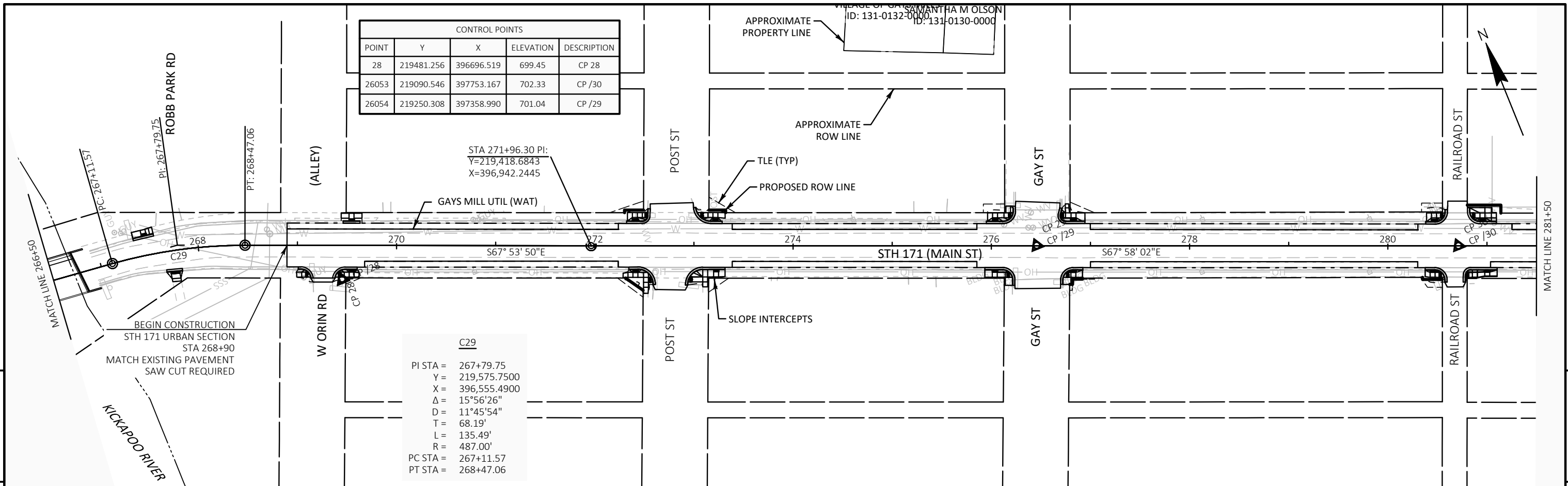
5



STA 24+35
 CP#2
 EXISTING 3.5'X5.0' CONC BOX TO REMAIN

C2
 PI STA = 24+70.45
 Y = 219,029.0263
 X = 377,995.4055
 Δ = 16°55'28"
 D = 4°58'56"
 T = 171.09'
 L = 339.70'
 R = 1,150.00'
 PC STA = 22+99.36
 PT STA = 26+39.06

CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
2	218794.341	377290.475	1193.45	CP 2



CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
28	219481.256	396696.519	699.45	CP /28
26053	219090.546	397753.167	702.33	CP /30
26054	219250.308	397358.990	701.04	CP /29

STA 271+96.30 PI:
Y=219,418.6843
X=396,942.2445

C29
PI STA = 267+79.75
Y = 219,575.7500
X = 396,555.4900
Δ = 15°56'26"
D = 11°45'54"
T = 68.19'
L = 135.49'
R = 487.00'
PC STA = 267+11.57
PT STA = 268+47.06

BEGIN CONSTRUCTION
STH 171 URBAN SECTION
STA 268+90
MATCH EXISTING PAVEMENT
SAW CUT REQUIRED

KICKAPOO RIVER

CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
26049	218797.942	398449.143	702.47	CP /32
26052	218953.653	398090.658	704.20	CP /31

STA 284+39.57 PI:
Y=218,952.2901
X=398,094.7175

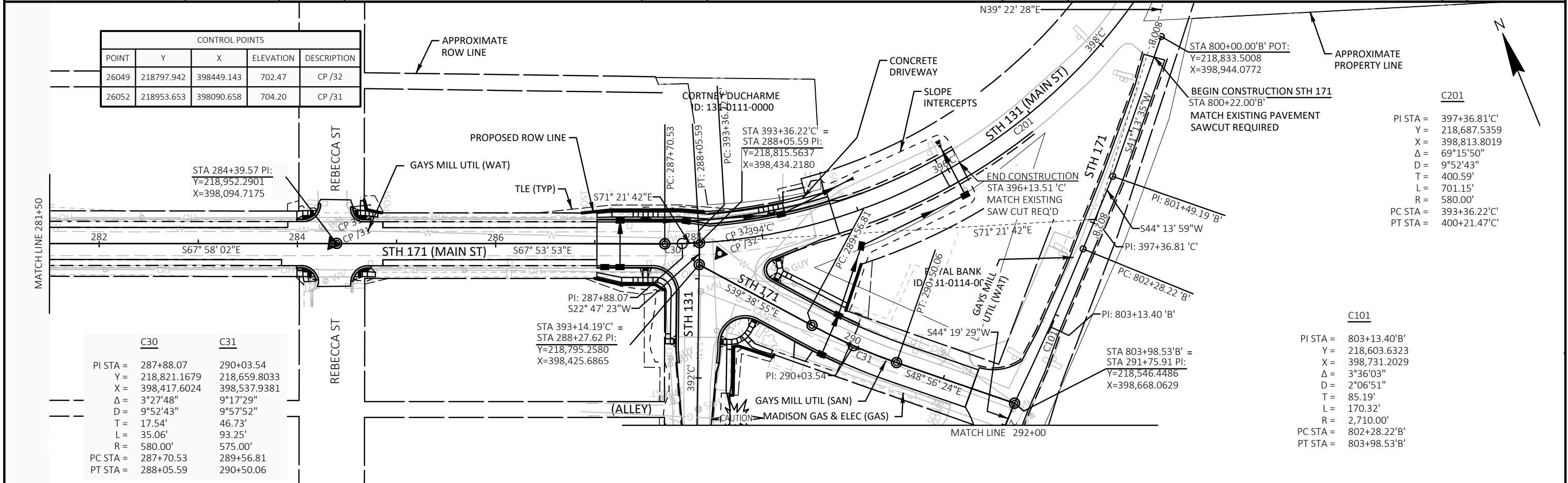
C30
PI STA = 287+88.07
Y = 218,821.1679
X = 398,417.6024
Δ = 3°27'48"
D = 9°52'43"
T = 17.54'
L = 35.06'
R = 580.00'
PC STA = 287+70.53
PT STA = 288+05.59

C31
PI STA = 290+03.54
Y = 218,659.8033
X = 398,537.9381
Δ = 9°17'29"
D = 9°57'52"
T = 46.73'
L = 93.25'
R = 575.00'
PC STA = 289+56.81
PT STA = 290+50.06

PI: 287+88.07
S22° 47' 23"W
STA 393+14.19'C' =
STA 288+27.62 PI:
Y=218,795.2580
X=398,425.6865

C201
PI STA = 397+36.81'C'
Y = 218,687.5359
X = 398,813.8019
Δ = 69°15'50"
D = 9°52'43"
T = 400.59'
L = 701.15'
R = 580.00'
PC STA = 393+36.22'C'
PT STA = 400+21.47'C'

C101
PI STA = 803+13.40'B'
Y = 218,603.6323
X = 398,731.2029
Δ = 3°36'03"
D = 2°06'51"
T = 85.19'
L = 170.32'
R = 2,710.00'
PC STA = 802+28.22'B'
PT STA = 803+98.53'B'



PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PLAN	SHEET	E
------------------------	--------------	------------------	------	-------	---

5

5

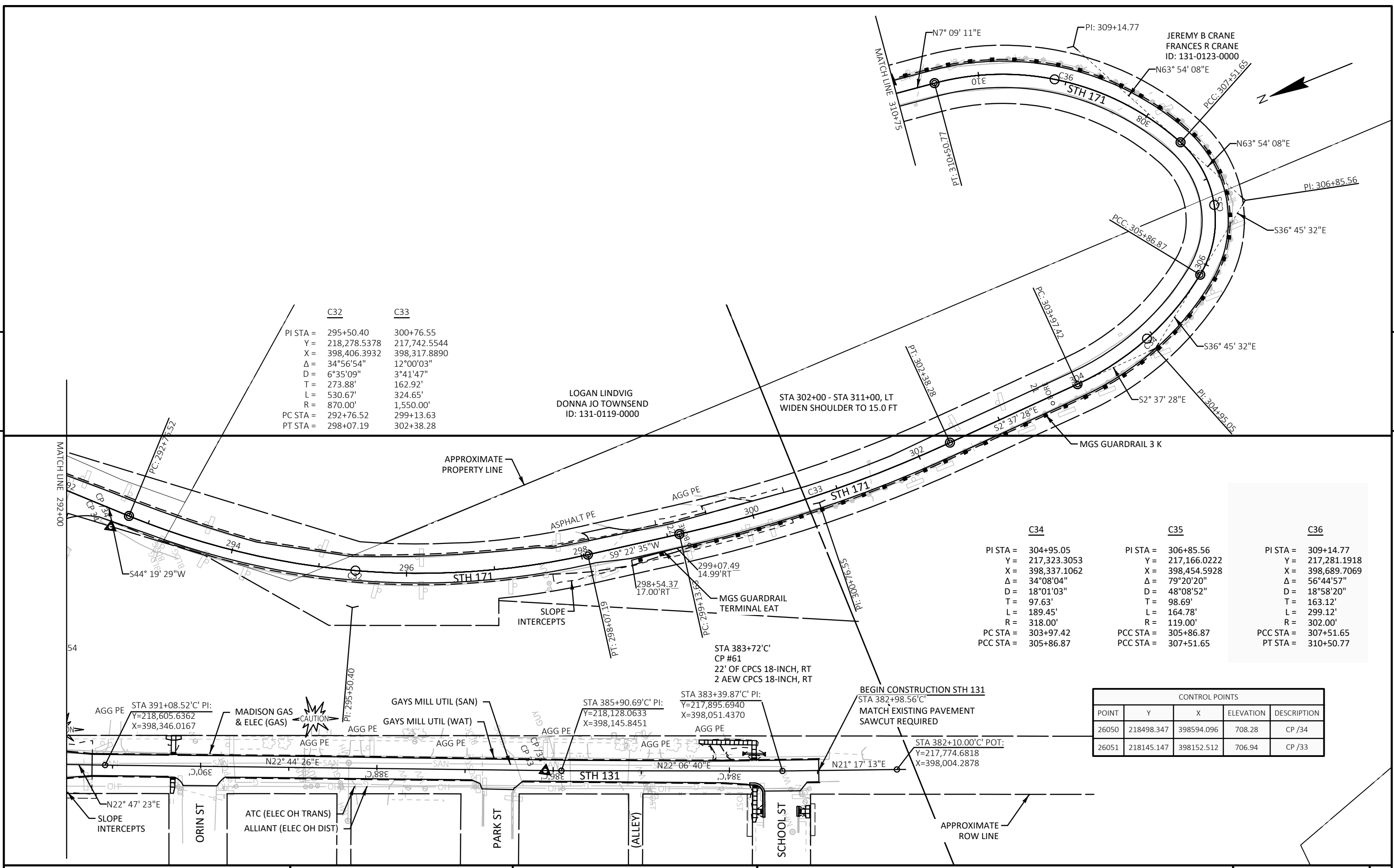
	C32	C33
PI STA =	295+50.40	300+76.55
Y =	218,278.5378	217,742.5544
X =	398,406.3932	398,317.8890
Δ =	34°56'54"	12°00'03"
D =	6°35'09"	3°41'47"
T =	273.88'	162.92'
L =	530.67'	324.65'
R =	870.00'	1,550.00'
PC STA =	292+76.52	299+13.63
PT STA =	298+07.19	302+38.28

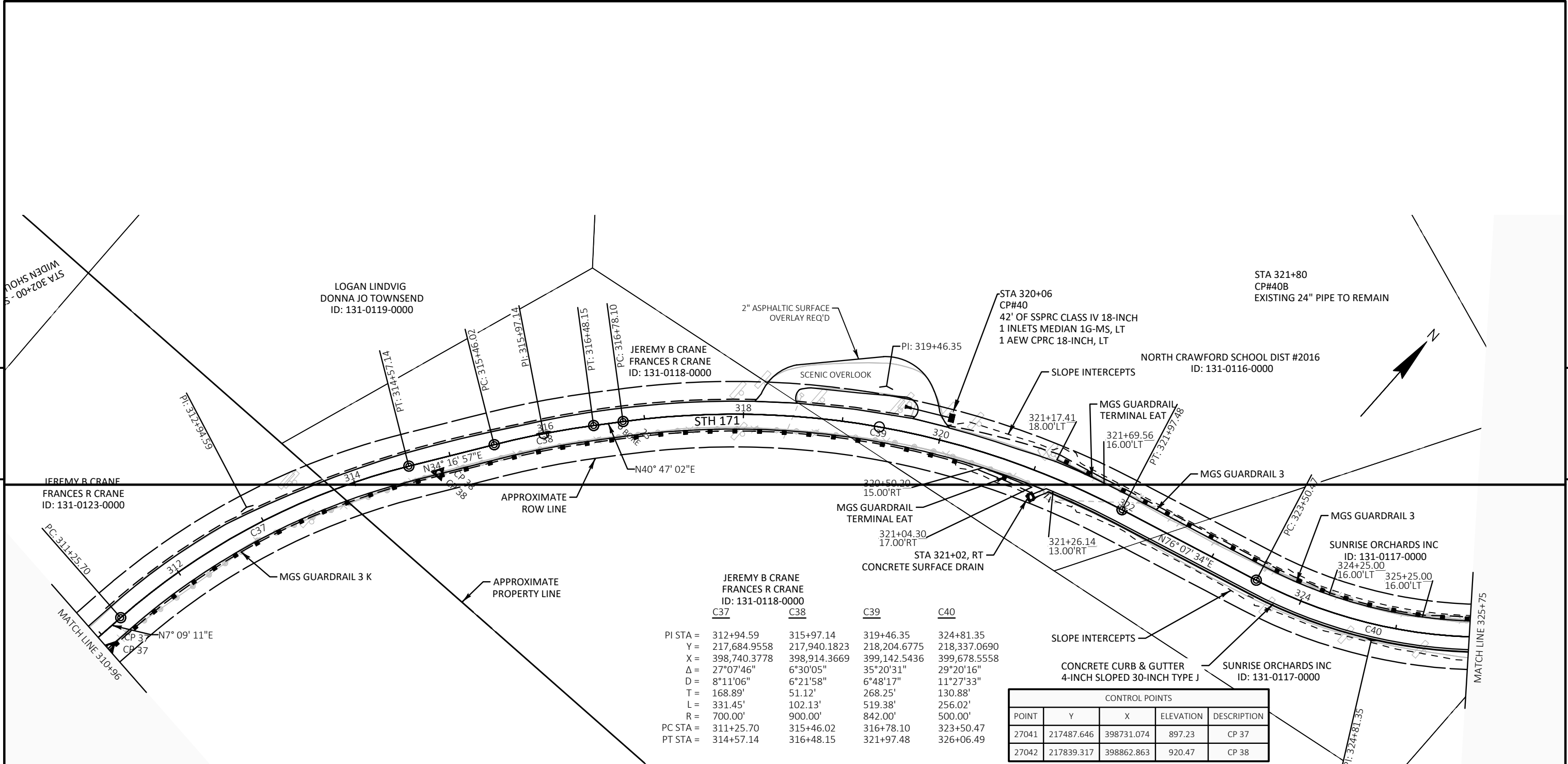
LOGAN LINDVIG
DONNA JO TOWNSEND
ID: 131-0119-0000

STA 302+00 - STA 311+00, LT
WIDEN SHOULDER TO 15.0 FT

	C34	C35	C36
PI STA =	304+95.05	306+85.56	309+14.77
Y =	217,323.3053	217,166.0222	217,281.1918
X =	398,337.1062	398,454.5928	398,689.7069
Δ =	34°08'04"	79°20'20"	56°44'57"
D =	18°01'03"	48°08'52"	18°58'20"
T =	97.63'	98.69'	163.12'
L =	189.45'	164.78'	299.12'
R =	318.00'	119.00'	302.00'
PC STA =	303+97.42	PCC STA = 305+86.87	PCC STA = 307+51.65
PCC STA =	305+86.87	PCC STA = 307+51.65	PT STA = 310+50.77

CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
26050	218498.347	398594.096	708.28	CP /34
26051	218145.147	398152.512	706.94	CP /33



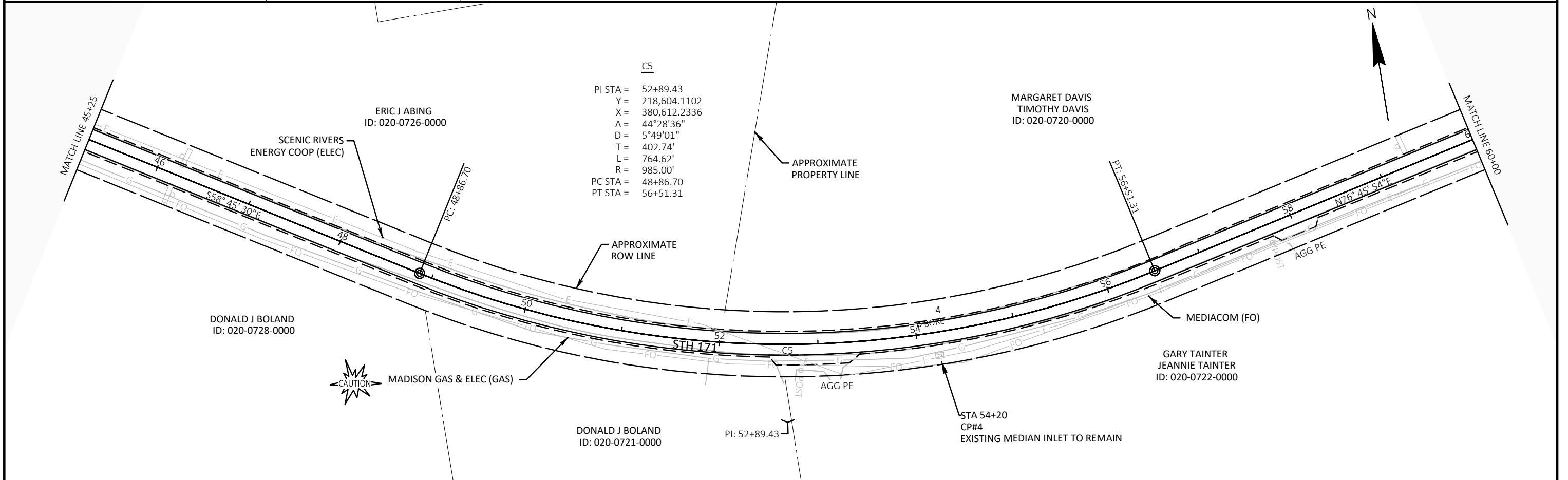
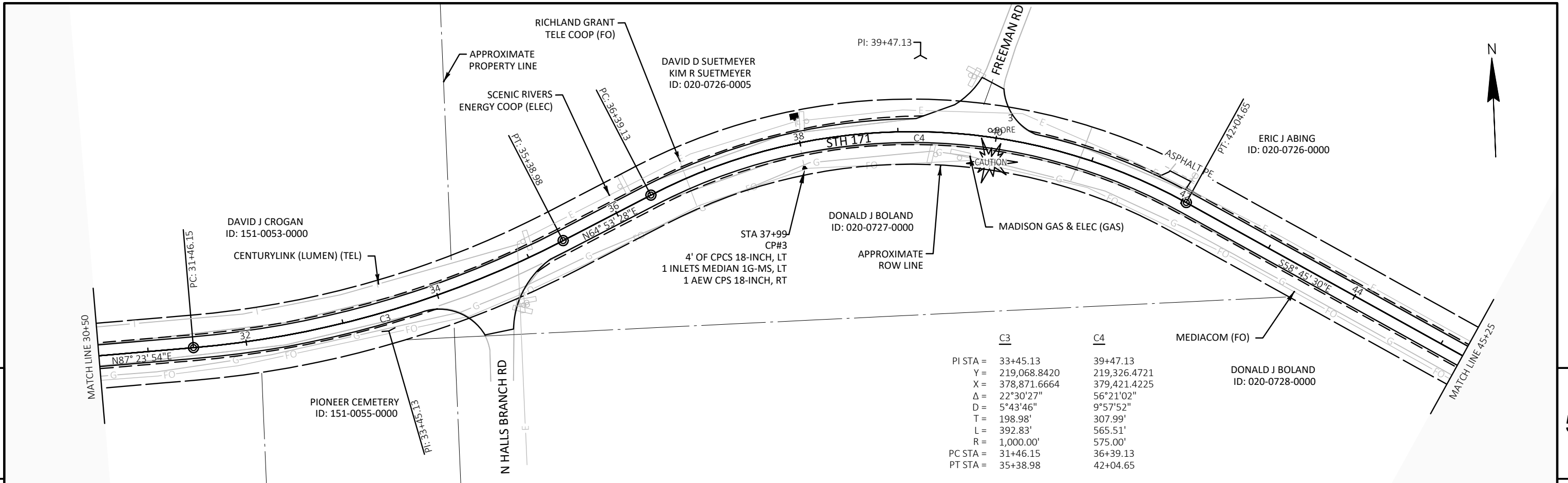


JEREMY B CRANE
FRANCES R CRANE
ID: 131-0118-0000

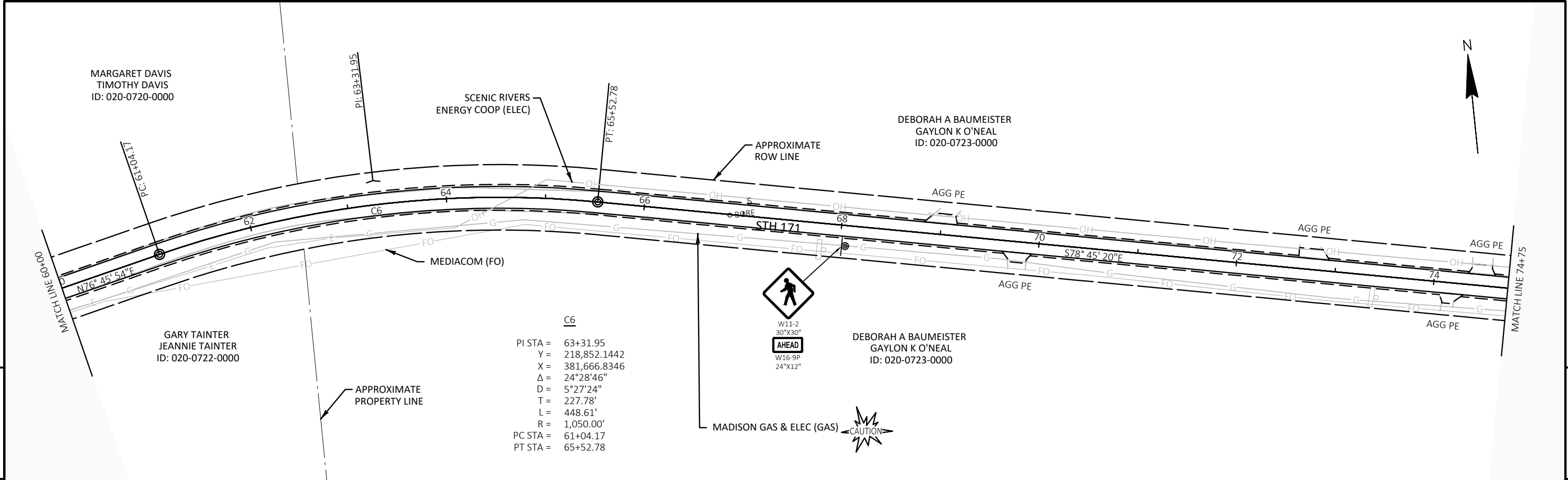
	C37	C38	C39	C40
PI STA =	312+94.59	315+97.14	319+46.35	324+81.35
Y =	217,684.9558	217,940.1823	218,204.6775	218,337.0690
X =	398,740.3778	398,914.3669	399,142.5436	399,678.5558
Δ =	27°07'46"	6°30'05"	35°20'31"	29°20'16"
D =	8°11'06"	6°21'58"	6°48'17"	11°27'33"
T =	168.89'	51.12'	268.25'	130.88'
L =	331.45'	102.13'	519.38'	256.02'
R =	700.00'	900.00'	842.00'	500.00'
PC STA =	311+25.70	315+46.02	316+78.10	323+50.47
PT STA =	314+57.14	316+48.15	321+97.48	326+06.49

CONTROL POINTS

POINT	Y	X	ELEVATION	DESCRIPTION
27041	217487.646	398731.074	897.23	CP 37
27042	217839.317	398862.863	920.47	CP 38



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD PLAN SHEET E

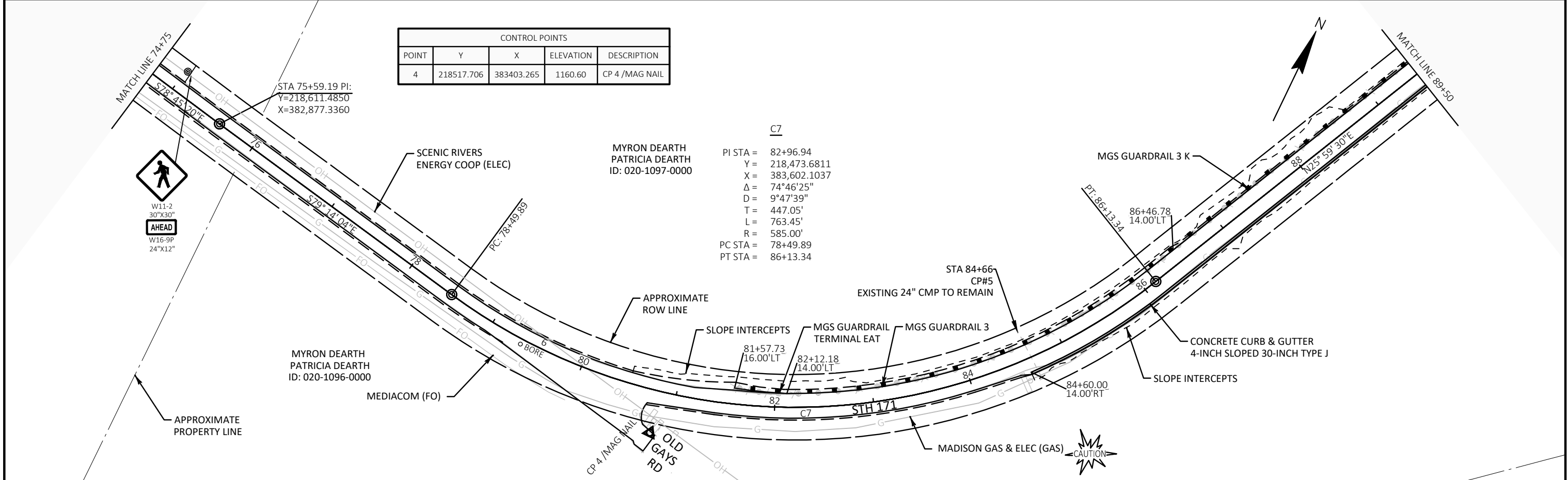


C6

PI STA =	63+31.95
Y =	218,852.1442
X =	381,666.8346
Δ =	24°28'46"
D =	5'27'24"
T =	227.78'
L =	448.61'
R =	1,050.00'
PC STA =	61+04.17
PT STA =	65+52.78

CONTROL POINTS

POINT	Y	X	ELEVATION	DESCRIPTION
4	218517.706	383403.265	1160.60	CP 4 /MAG NAIL



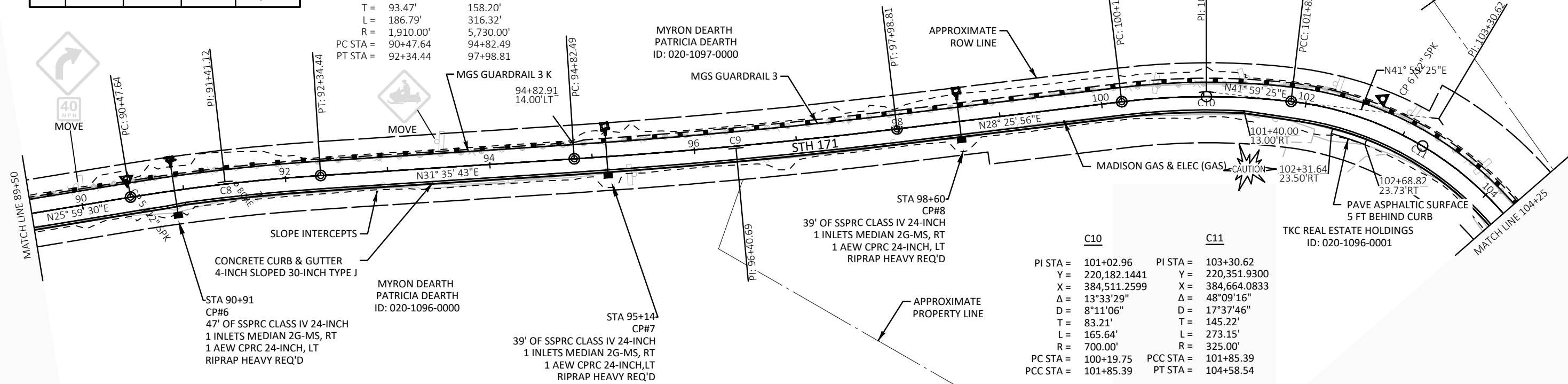
C7

PI STA =	82+96.94
Y =	218,473.6811
X =	383,602.1037
Δ =	74°46'25"
D =	9°47'39"
T =	447.05'
L =	763.45'
R =	585.00'
PC STA =	78+49.89
PT STA =	86+13.34

CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
5	219272.628	383972.215	1092.60	CP 5 /12" SPK
6	220318.445	384616.618	1011.87	CP 6 /12" SPK

C8
 PI STA = 91+41.12
 Y = 219,349.9098
 X = 384,029.3130
 $\Delta = 5^{\circ}36'12''$
 D = 2^{\circ}59'59"
 T = 93.47'
 L = 186.79'
 R = 1,910.00'
 PC STA = 90+47.64
 PT STA = 92+34.44

C9
 PI STA = 96+40.69
 Y = 219,775.5577
 X = 384,291.1240
 $\Delta = 3^{\circ}09'47''$
 D = 1^{\circ}00'00"
 T = 158.20'
 L = 316.32'
 R = 5,730.00'
 PC STA = 94+82.49
 PT STA = 97+98.81

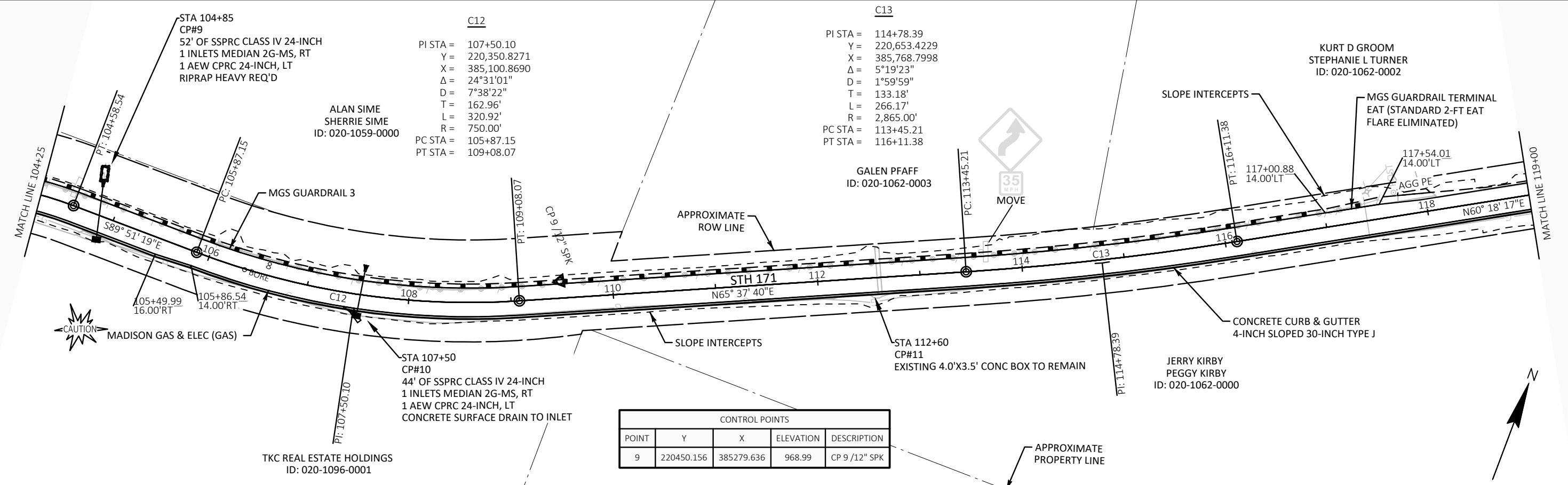


C10
 PI STA = 101+02.96
 Y = 220,182.1441
 X = 384,511.2599
 $\Delta = 13^{\circ}33'29''$
 D = 8^{\circ}11'06"
 T = 83.21'
 L = 165.64'
 R = 700.00'
 PC STA = 100+19.75
 PCC STA = 101+85.39

C11
 PI STA = 103+30.62
 Y = 220,351.9300
 X = 384,664.0833
 $\Delta = 48^{\circ}09'16''$
 D = 17^{\circ}37'46"
 T = 145.22'
 L = 273.15'
 R = 325.00'
 PCC STA = 101+85.39
 PT STA = 104+58.54

5

5



C12
 PI STA = 107+50.10
 Y = 220,350.8271
 X = 385,100.8690
 $\Delta = 24^{\circ}31'01''$
 D = 7^{\circ}38'22"
 T = 162.96'
 L = 320.92'
 R = 750.00'
 PC STA = 105+87.15
 PT STA = 109+08.07

C13
 PI STA = 114+78.39
 Y = 220,653.4229
 X = 385,768.7998
 $\Delta = 5^{\circ}19'23''$
 D = 1^{\circ}59'59"
 T = 133.18'
 L = 266.17'
 R = 2,865.00'
 PC STA = 113+45.21
 PT STA = 116+11.38

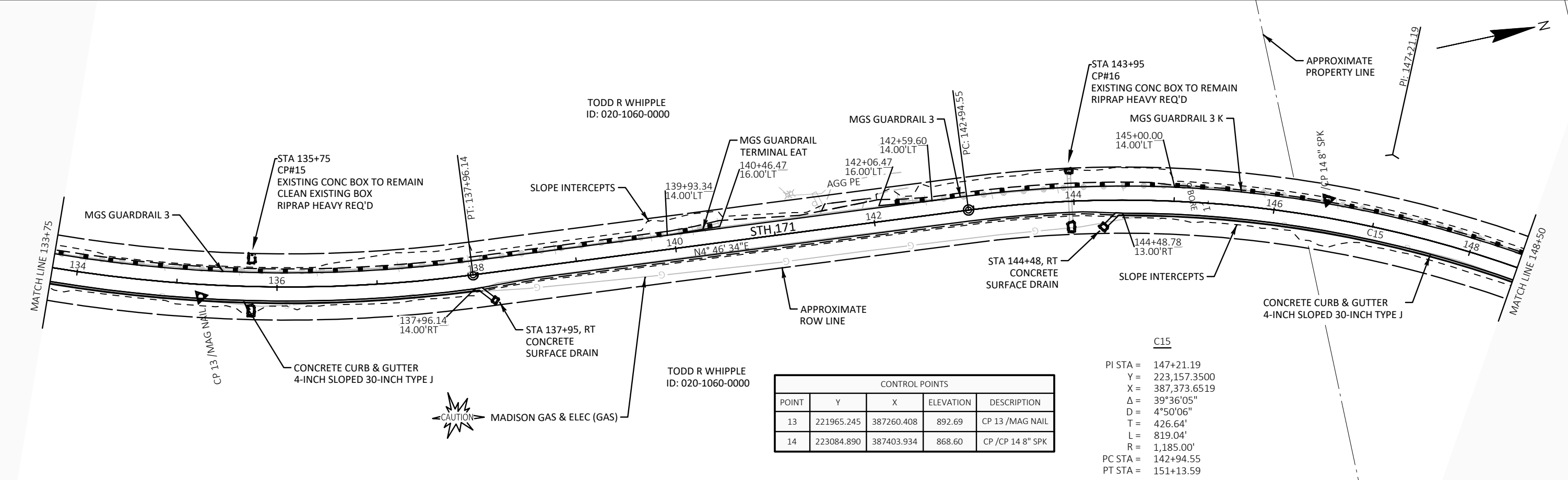
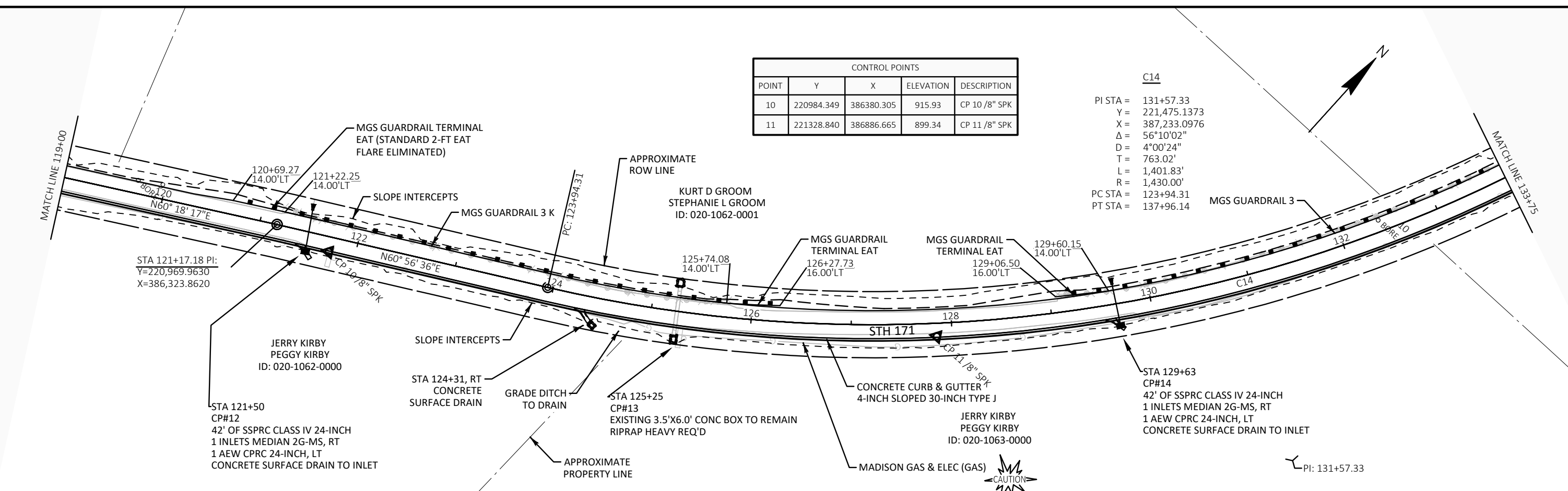
CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
9	220450.156	385279.636	968.99	CP 9 /12" SPK

CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
10	220984.349	386380.305	915.93	CP 10 /8" SPK
11	221328.840	386886.665	899.34	CP 11 /8" SPK

C14
 PI STA = 131+57.33
 Y = 221,475.1373
 X = 387,233.0976
 $\Delta = 56^{\circ}10'02''$
 $D = 4^{\circ}00'24''$
 $T = 763.02'$
 $L = 1,401.83'$
 $R = 1,430.00'$
 PC STA = 123+94.31
 PT STA = 137+96.14

5

5



CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
13	221965.245	387260.408	892.69	CP 13 /MAG NAIL
14	223084.890	387403.934	868.60	CP /CP 14 8" SPK

C15
 PI STA = 147+21.19
 Y = 223,157.3500
 X = 387,373.6519
 $\Delta = 39^{\circ}36'05''$
 $D = 4^{\circ}50'06''$
 $T = 426.64'$
 $L = 819.04'$
 $R = 1,185.00'$
 PC STA = 142+94.55
 PT STA = 151+13.59

PROJECT NO: 5790-02-72

HWY: STH 171

COUNTY: CRAWFORD

PLAN

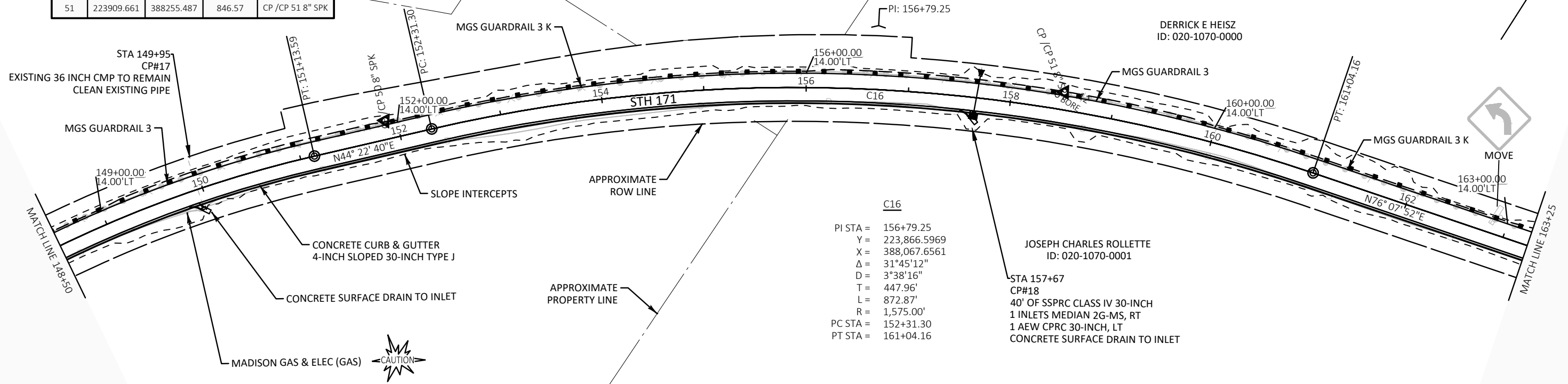
SHEET

E

CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
50	223528.612	387711.550	863.06	CP /CP 50 8" SPK
51	223909.661	388255.487	846.57	CP /CP 51 8" SPK

DERRICK E HEISZ
ID: 020-1051-0000

DERRICK E HEISZ
ID: 020-1070-0000



C16

PI STA = 156+79.25
Y = 223,866.5969
X = 388,067.6561
Δ = 31°45'12"
D = 3°38'16"
T = 447.96'
L = 872.87'
R = 1,575.00'
PC STA = 152+31.30
PT STA = 161+04.16

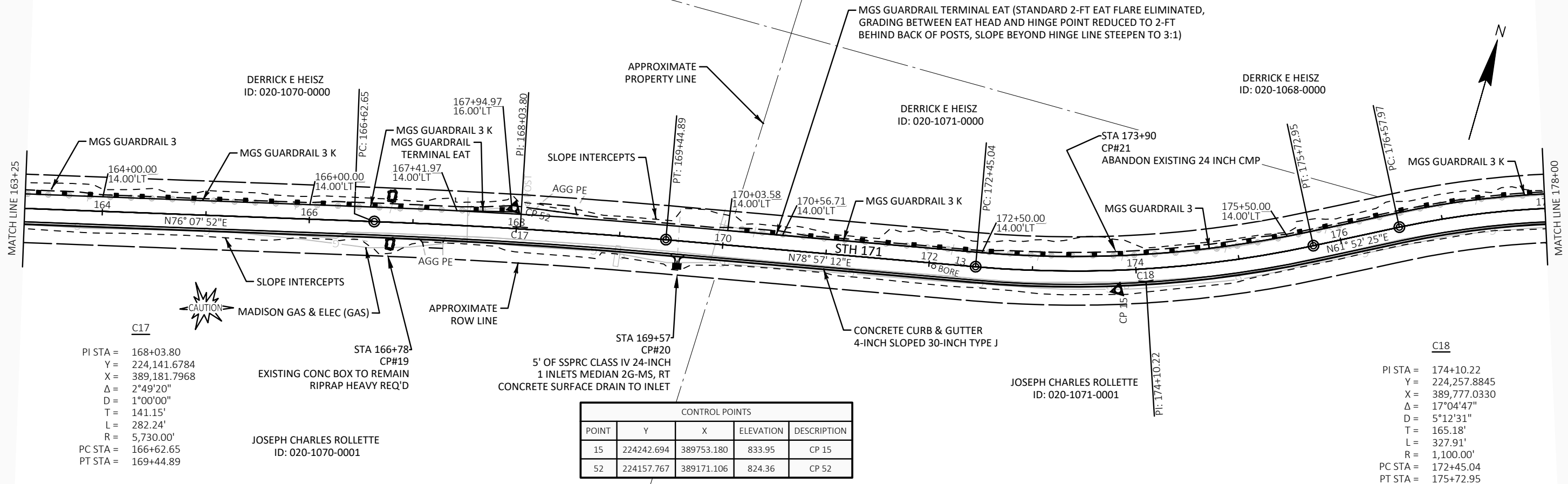
JOSEPH CHARLES ROLLETTE
ID: 020-1070-0001

STA 157+67
CP#18
40' OF SSPRC CLASS IV 30-INCH
1 INLETS MEDIAN 2G-MS, RT
1 AEW CPRC 30-INCH, LT
CONCRETE SURFACE DRAIN TO INLET

5

5

MGS GUARDRAIL TERMINAL EAT (STANDARD 2-FT EAT FLARE ELIMINATED, GRADING BETWEEN EAT HEAD AND HINGE POINT REDUCED TO 2-FT BEHIND BACK OF POSTS, SLOPE BEYOND HINGE LINE STEEPEN TO 3:1)



C17

PI STA = 168+03.80
Y = 224,141.6784
X = 389,181.7968
Δ = 2°49'20"
D = 1°00'00"
T = 141.15'
L = 282.24'
R = 5,730.00'
PC STA = 166+62.65
PT STA = 169+44.89

JOSEPH CHARLES ROLLETTE
ID: 020-1070-0001

STA 169+57
CP#20
5' OF SSPRC CLASS IV 24-INCH
1 INLETS MEDIAN 2G-MS, RT
CONCRETE SURFACE DRAIN TO INLET

CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
15	224242.694	389753.180	833.95	CP 15
52	224157.767	389171.106	824.36	CP 52

C18

PI STA = 174+10.22
Y = 224,257.8845
X = 389,777.0330
Δ = 17°04'47"
D = 5°12'31"
T = 165.18'
L = 327.91'
R = 1,100.00'
PC STA = 172+45.04
PT STA = 175+72.95

JOSEPH CHARLES ROLLETTE
ID: 020-1071-0001

PROJECT NO: 5790-02-72

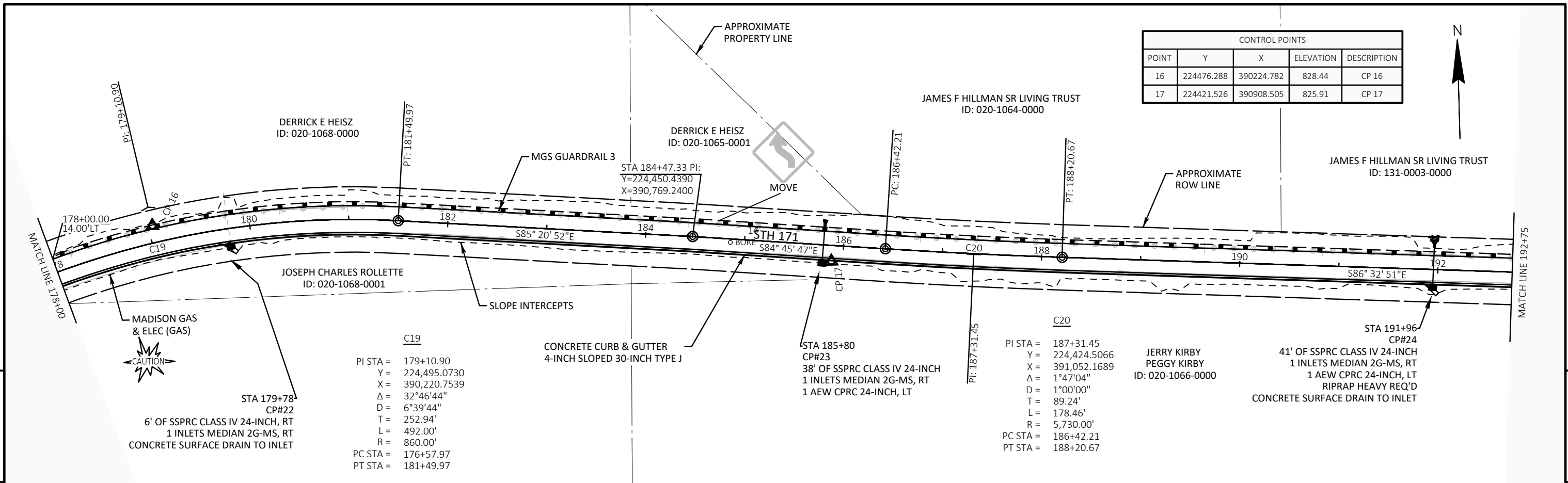
HWY: STH 171

COUNTY: CRAWFORD

PLAN

SHEET

E



CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
16	224476.288	390224.782	828.44	CP 16
17	224421.526	390908.505	825.91	CP 17

C19

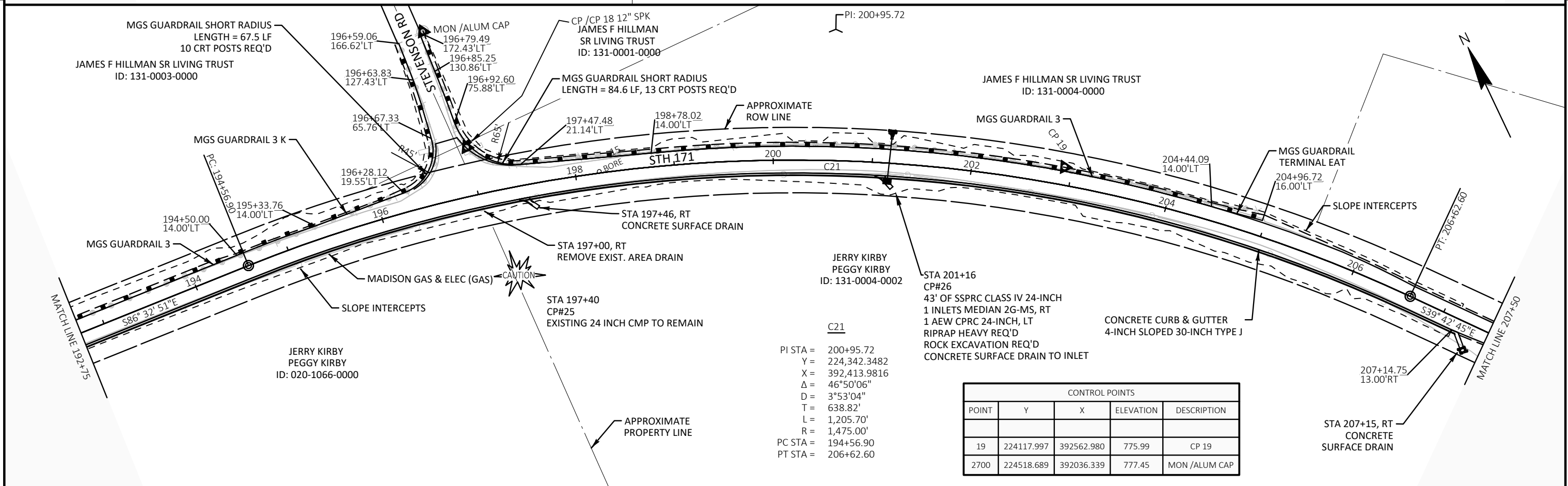
PI STA = 179+10.90
 Y = 224,495.0730
 X = 390,220.7539
 Δ = 32°46'44"
 D = 6°39'44"
 T = 252.94'
 L = 492.00'
 R = 860.00'
 PC STA = 176+57.97
 PT STA = 181+49.97

C20

PI STA = 187+31.45
 Y = 224,424.5066
 X = 391,052.1689
 Δ = 1°47'04"
 D = 1°00'00"
 T = 89.24'
 L = 178.46'
 R = 5,730.00'
 PC STA = 186+42.21
 PT STA = 188+20.67

5

5



CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
19	224117.997	392562.980	775.99	CP 19
2700	224518.689	392036.339	777.45	MON /ALUM CAP

C21

PI STA = 200+95.72
 Y = 224,342.3482
 X = 392,413.9816
 Δ = 46°50'06"
 D = 3°53'04"
 T = 638.82'
 L = 1,205.70'
 R = 1,475.00'
 PC STA = 194+56.90
 PT STA = 206+62.60

C22
 PI STA = 208+85.05
 Y = 223,679.8045
 X = 392,964.2804
 $\Delta = 13^\circ 53' 25''$
 $D = 5^\circ 59' 58''$
 T = 116.33'
 L = 231.52'
 R = 955.00'
 PC STA = 207+68.71
 PT STA = 210+00.24

JUSTIN J SPROSTY
 TYSON J SPROSTY
 ID: 131-0004-0001

JUSTIN J SPROSTY
 TYSON J SPROSTY
 ID: 131-0014-0001

JAMES F HILLMAN
 SR LIVING TRUST
 ID: 131-0014-0000

JOHN R JOHNSON LF EST
 JOSEPH M JOHNSON
 ID: 131-0016-0000

C23
 PI STA = 217+53.97
 Y = 223,163.5263
 X = 393,664.6169
 $\Delta = 14^\circ 06' 35''$
 $D = 2^\circ 59' 59''$
 T = 236.37'
 L = 470.36'
 R = 1,910.00'
 PC STA = 215+17.60
 PT STA = 219+87.95

AMY MARTIN
 DANIEL MARTIN
 ID: 131-0020-0000

CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
20	223538.244	393120.956	752.75	CP 20
21	223226.670	393603.973	754.07	CP 21

CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
22	222036.217	394573.292	750.67	CP 22

C24
 PI STA = 236+25.60
 Y = 221,714.6911
 X = 394,853.2201
 $\Delta = 31^\circ 07' 10''$
 $D = 6^\circ 59' 45''$
 T = 228.05'
 L = 444.83'
 R = 819.00'
 PC STA = 233+97.55
 PT STA = 238+42.38

JOHN R JOHNSON LF EST
 JOSEPH M JOHNSON
 ID: 131-0018-0000

STA 230+32.95 PI:
 Y=222,174.6960
 X=394,479.5500

STA 235+28
 CP#29
 EXISTING 36 INCH CMP TO REMAIN

SARAH A EWING
 ID: 131-0023-0001

STA 224+77
 CP#28
 43' OF SSPRC CLASS IV 24-INCH
 1 INLETS MEDIAN 2G-MS-A, RT
 1 AEW CPRC 24-INCH, LT
 39' OF SSPRC CLASS IV 18-INCH
 1 AEW CPRC 18-INCH, RT

RICHLAND GRANT
 TELE COOP (FO)

AMY MARTIN
 DANIEL MARTIN
 ID: 131-0020-0000

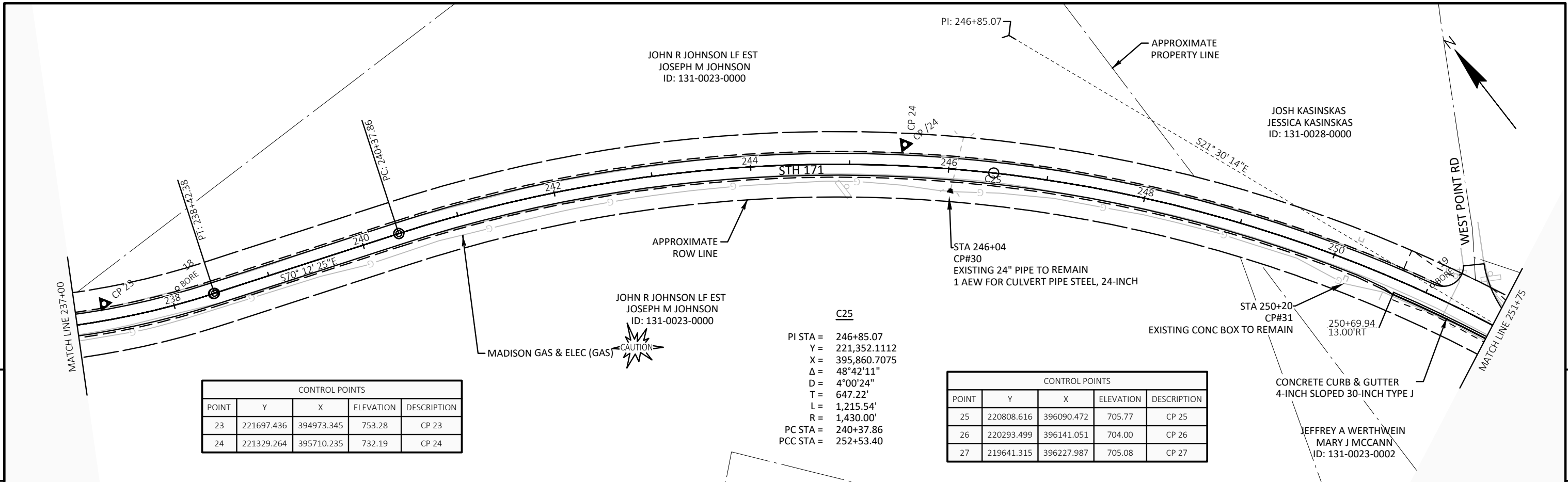
MADISON GAS & ELEC (GAS)

JOHN R JOHNSON LF EST
 JAMES M JOHNSON
 ID: 131-0019-0000

RICHLAND GRANT
 TELE COOP (FO)

5

5



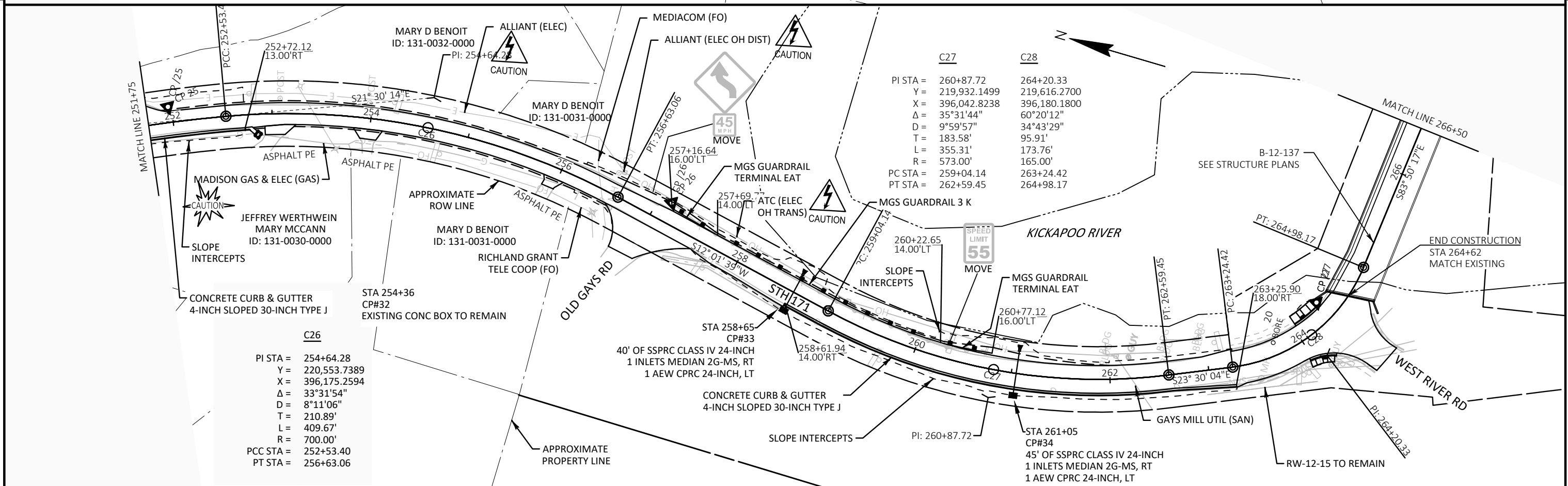
CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
23	221697.436	394973.345	753.28	CP 23
24	221329.264	395710.235	732.19	CP 24

C25
 PI STA = 246+85.07
 Y = 221,352.1112
 X = 395,860.7075
 Δ = 48°42'11"
 D = 4°00'24"
 T = 647.22'
 L = 1,215.54'
 R = 1,430.00'
 PC STA = 240+37.86
 PCC STA = 252+53.40

CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
25	220808.616	396090.472	705.77	CP 25
26	220293.499	396141.051	704.00	CP 26
27	219641.315	396227.987	705.08	CP 27

5

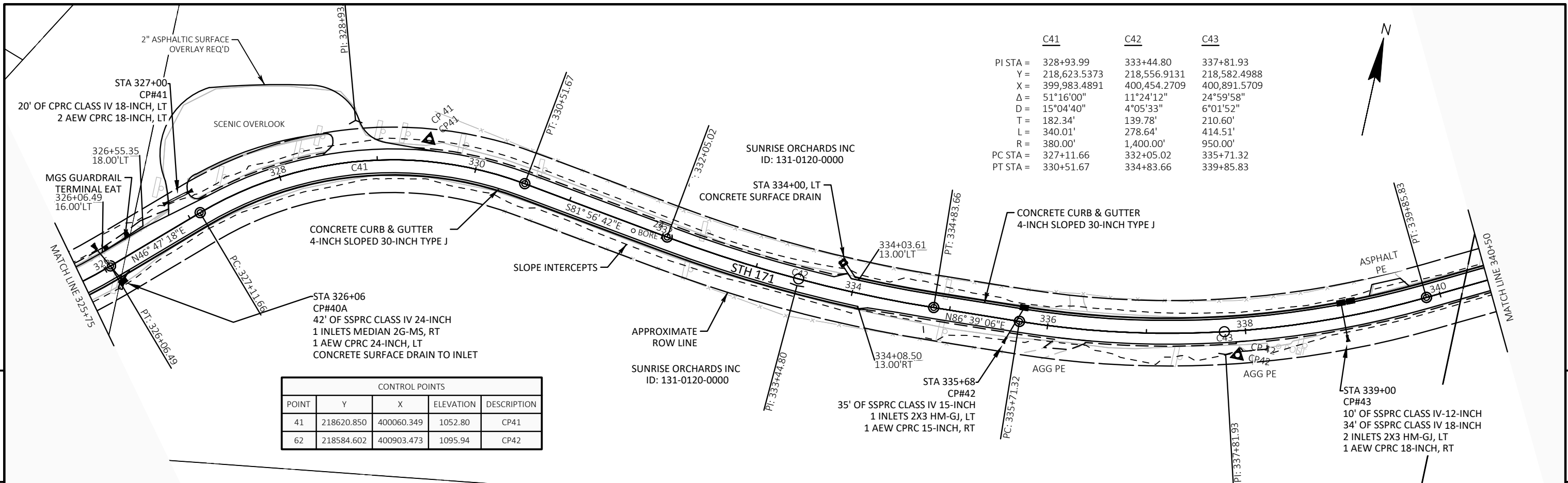
5



C26
 PI STA = 254+64.28
 Y = 220,553.7389
 X = 396,175.2594
 Δ = 33°31'54"
 D = 8°11'06"
 T = 210.89'
 L = 409.67'
 R = 700.00'
 PCC STA = 252+53.40
 PT STA = 256+63.06

C27
 PI STA = 260+87.72
 Y = 219,932.1499
 X = 396,042.8238
 Δ = 35°31'44"
 D = 9°59'57"
 T = 183.58'
 L = 355.31'
 R = 573.00'
 PC STA = 259+04.14
 PT STA = 262+59.45

C28
 PI STA = 264+20.33
 Y = 219,616.2700
 X = 396,180.1800
 Δ = 60°20'12"
 D = 34°43'29"
 T = 95.91'
 L = 173.76'
 R = 165.00'
 PC STA = 263+24.42
 PT STA = 264+98.17

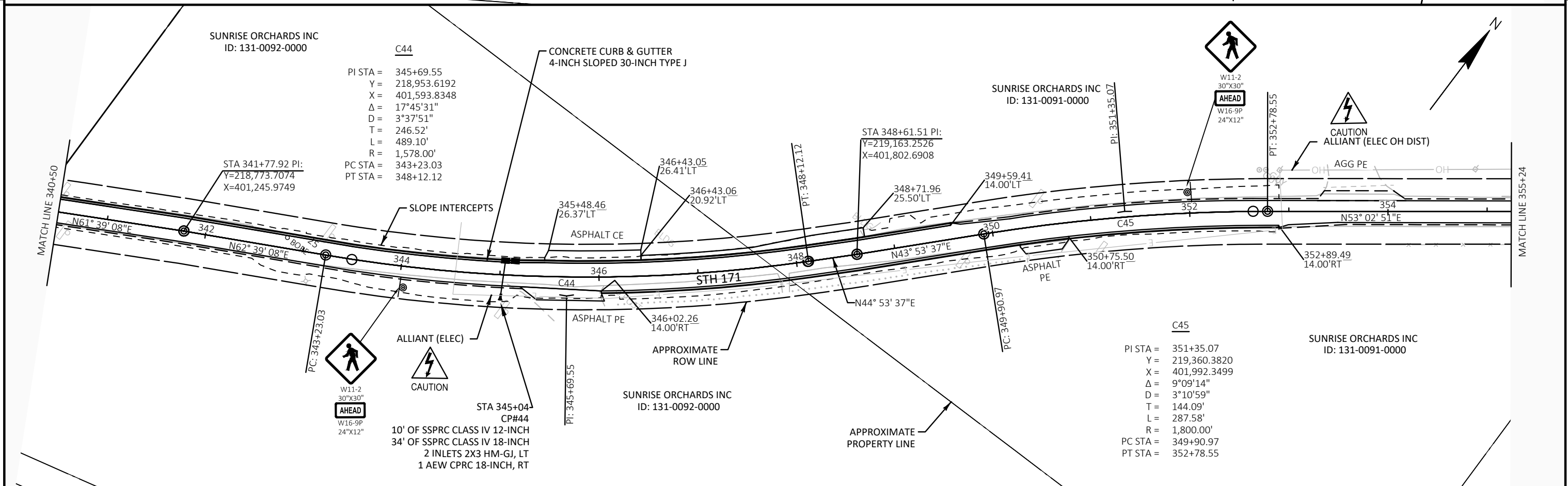


	C41	C42	C43
PI STA =	328+93.99	333+44.80	337+81.93
Y =	218,623.5373	218,556.9131	218,582.4988
X =	399,983.4891	400,454.2709	400,891.5709
Δ =	51°16'00"	11°24'12"	24°59'58"
D =	15°04'40"	4°05'33"	6°01'52"
T =	182.34'	139.78'	210.60'
L =	340.01'	278.64'	414.51'
R =	380.00'	1,400.00'	950.00'
PC STA =	327+11.66	332+05.02	335+71.32
PT STA =	330+51.67	334+83.66	339+85.83

CONTROL POINTS				
POINT	Y	X	ELEVATION	DESCRIPTION
41	218620.850	400060.349	1052.80	CP41
62	218584.602	400903.473	1095.94	CP42

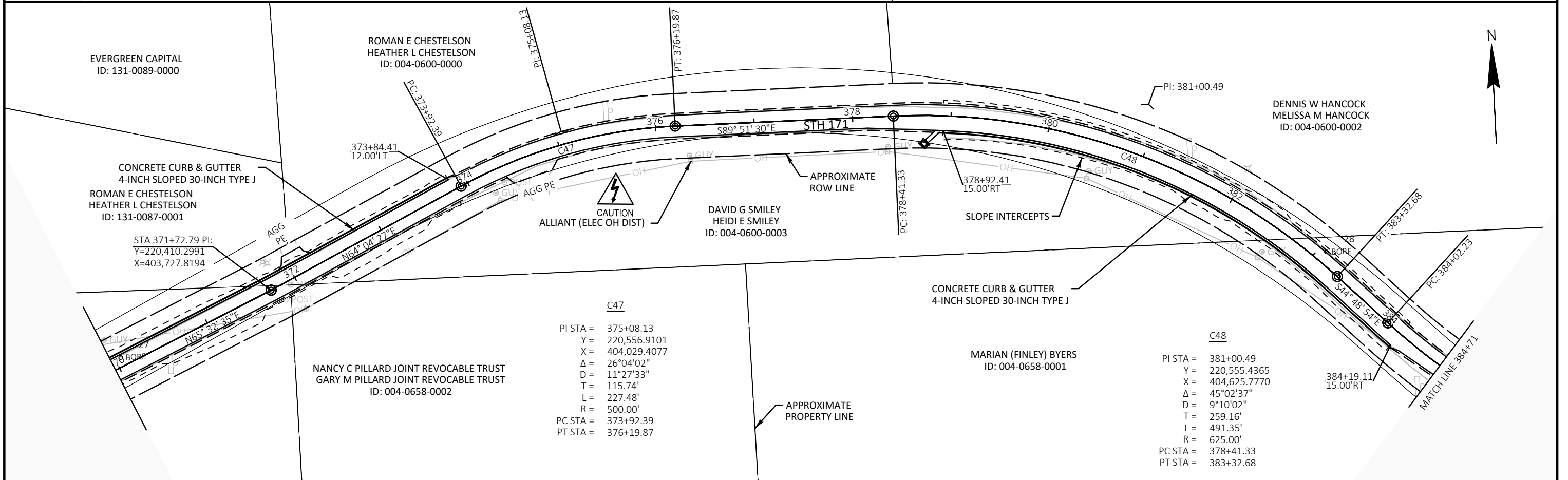
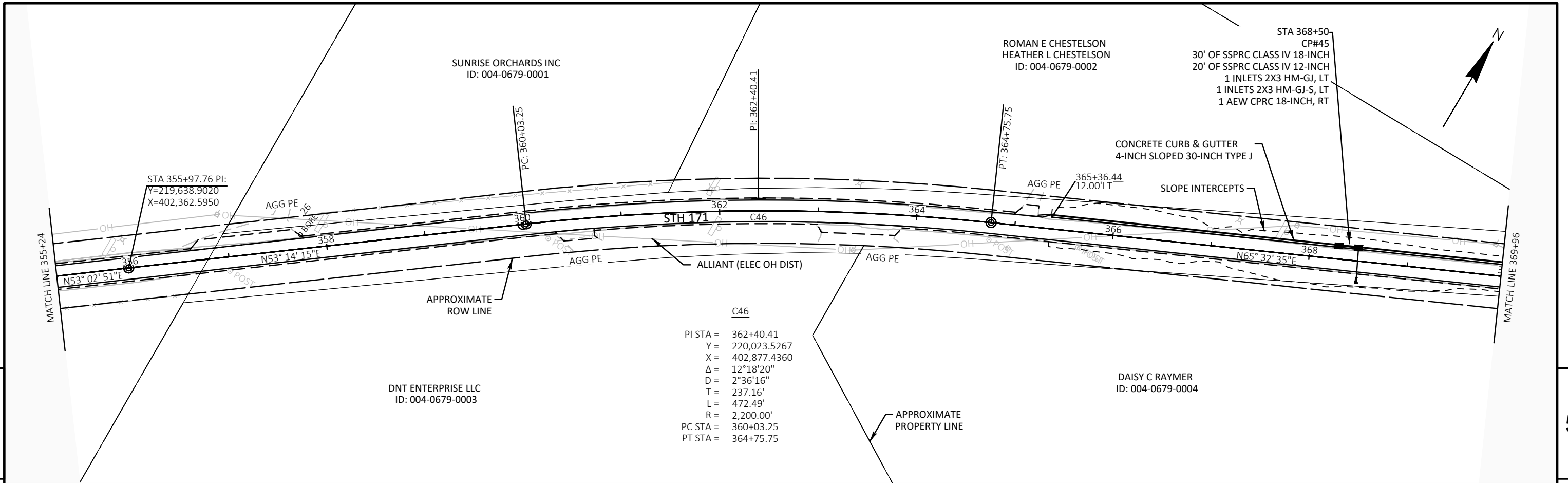
5

5



	C44
PI STA =	345+69.55
Y =	218,953.6192
X =	401,593.8348
Δ =	17°45'31"
D =	3°37'51"
T =	246.52'
L =	489.10'
R =	1,578.00'
PC STA =	343+23.03
PT STA =	348+12.12

	C45
PI STA =	351+35.07
Y =	219,360.3820
X =	401,992.3499
Δ =	9°09'14"
D =	3°10'59"
T =	144.09'
L =	287.58'
R =	1,800.00'
PC STA =	349+90.97
PT STA =	352+78.55



PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PLAN	SHEET	E
------------------------	--------------	------------------	------	-------	---

	C49	C50
PI STA =	385+43.92	389+83.01
Y =	220,221.7369	220,065.7891
X =	404,957.3272	405,372.4540
Δ =	24°35'45"	1°06'49"
D =	8°48'53"	0°30'00"
T =	141.70'	111.38'
L =	279.03'	222.76'
R =	650.00'	11,460.00'
PC STA =	384+02.23	388+71.63
PT STA =	386+81.26	390+94.39

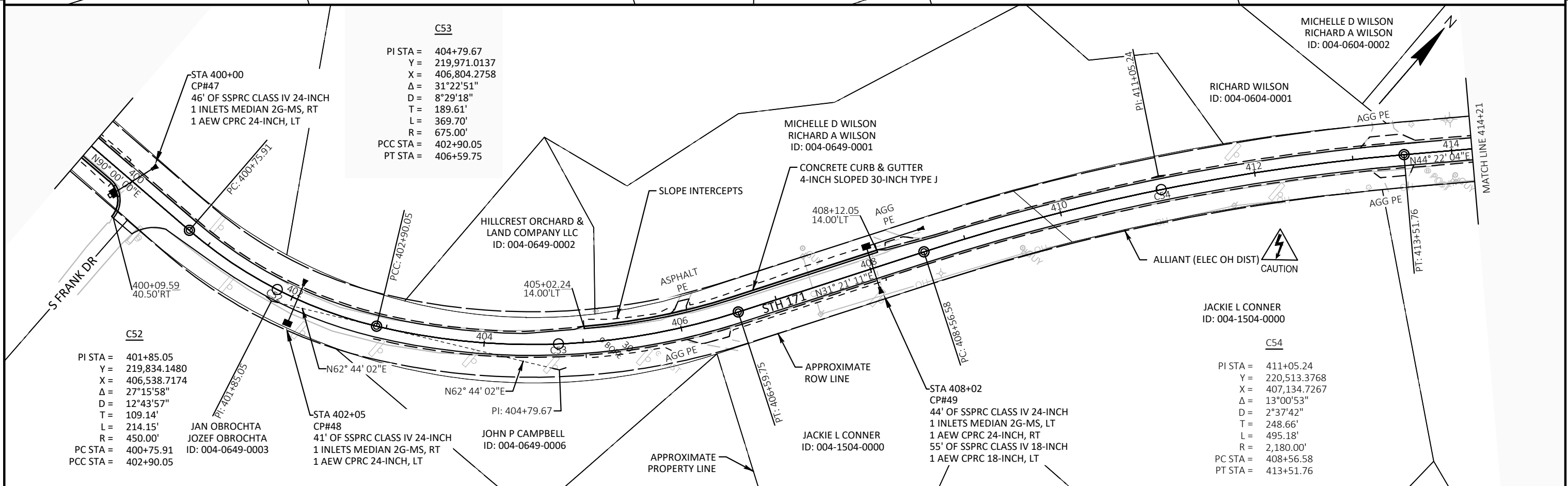
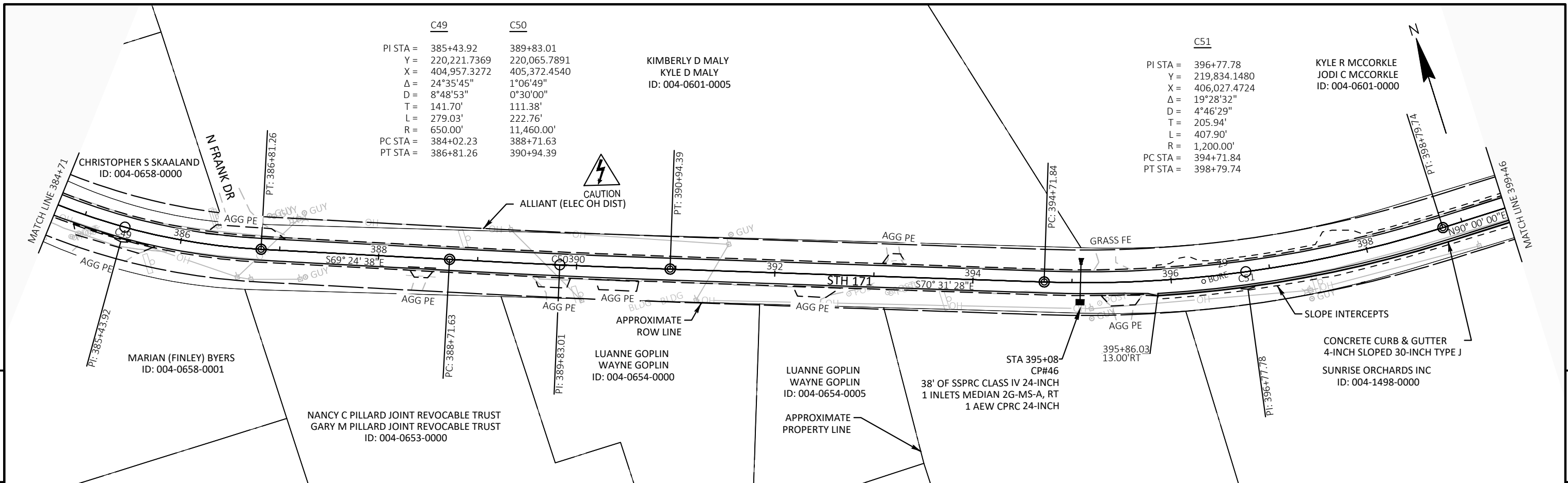
KIMBERLY D MALY
KYLE D MALY
ID: 004-0601-0005

	C51
PI STA =	396+77.78
Y =	219,834.1480
X =	406,027.4724
Δ =	19°28'32"
D =	4°46'29"
T =	205.94'
L =	407.90'
R =	1,200.00'
PC STA =	394+71.84
PT STA =	398+79.74

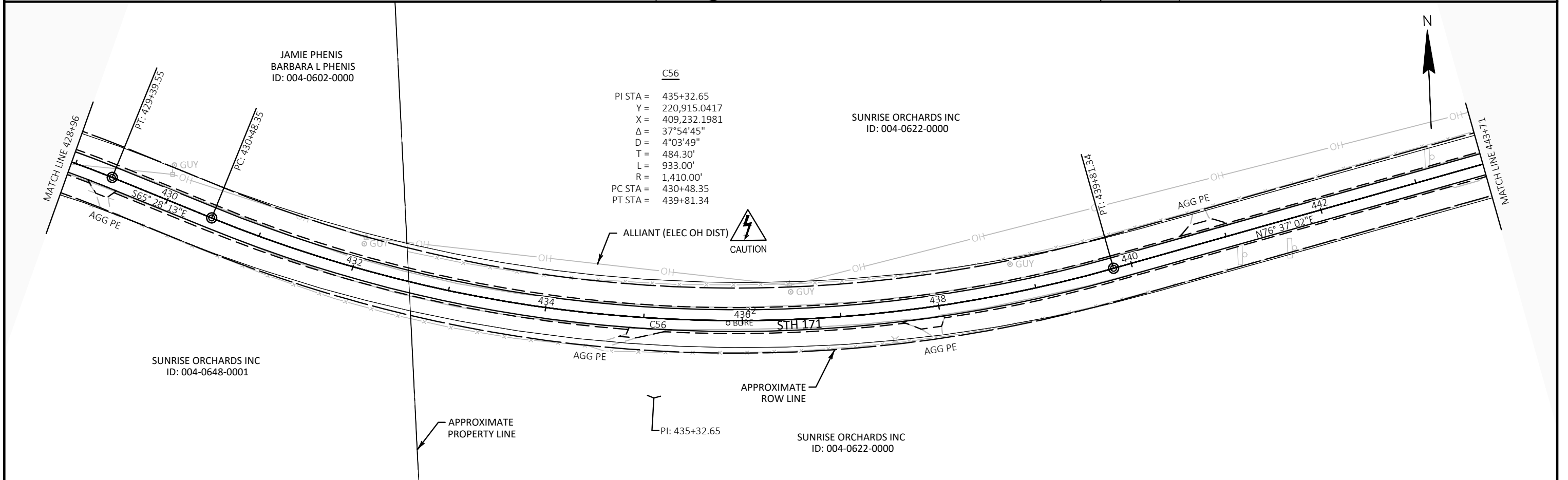
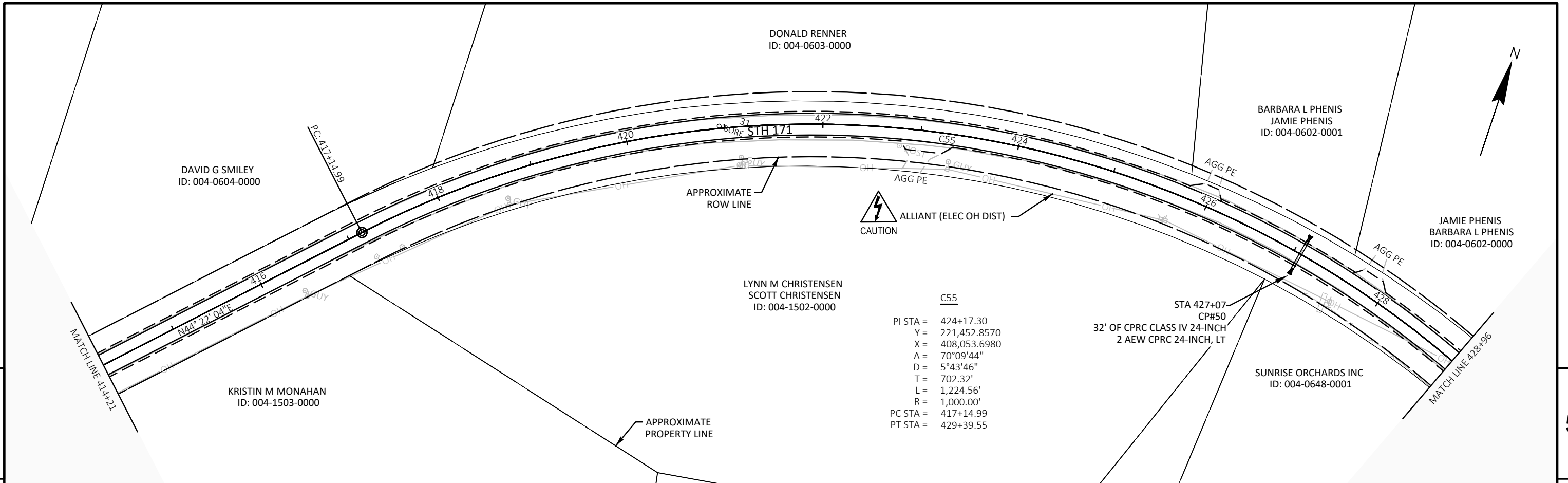
KYLE R MCCORKLE
JODI C MCCORKLE
ID: 004-0601-0000

	C53
PI STA =	404+79.67
Y =	219,971.0137
X =	406,804.2758
Δ =	31°22'51"
D =	8°29'18"
T =	189.61'
L =	369.70'
R =	675.00'
PCC STA =	402+90.05
PT STA =	406+59.75

	C54
PI STA =	411+05.24
Y =	220,513.3768
X =	407,134.7267
Δ =	13°00'53"
D =	2°37'42"
T =	248.66'
L =	495.18'
R =	2,180.00'
PC STA =	408+56.58
PT STA =	413+51.76

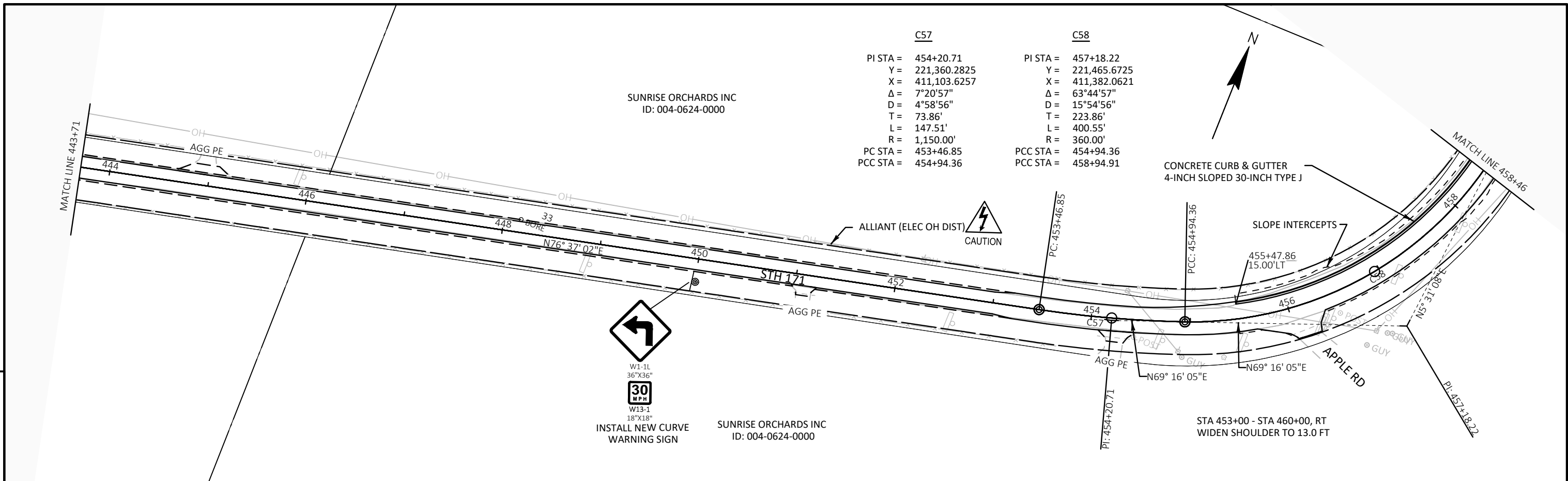


PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PLAN	SHEET	E
------------------------	--------------	------------------	------	-------	---



PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PLAN	SHEET	E
------------------------	--------------	------------------	------	-------	---

5



C57

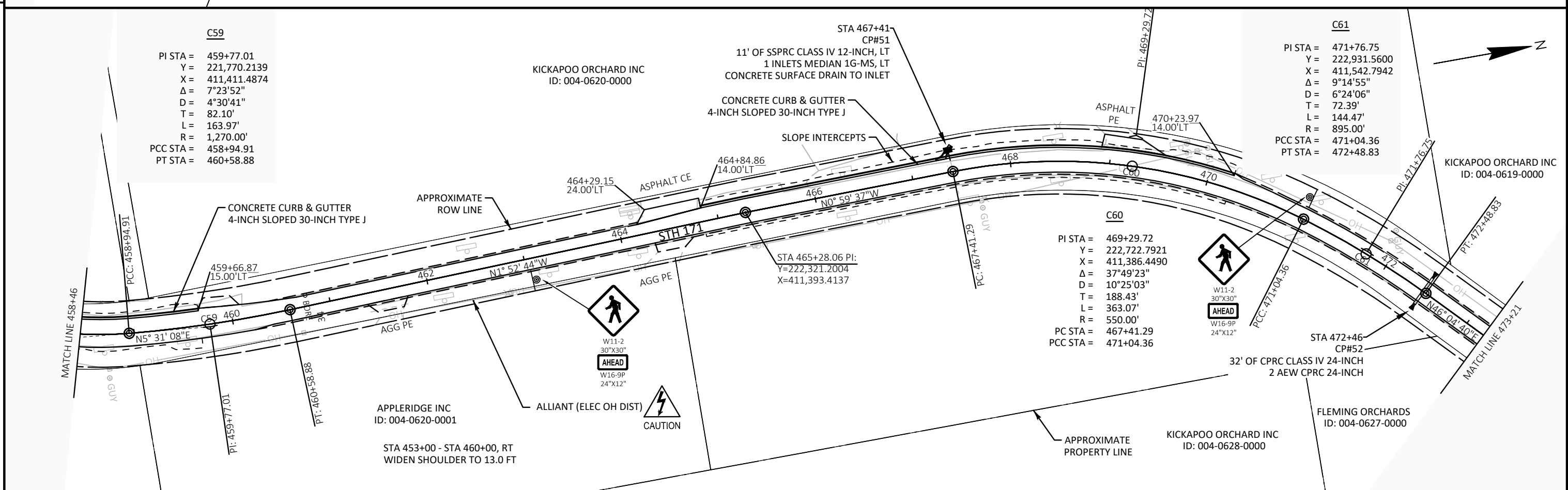
PI STA = 454+20.71
 Y = 221,360.2825
 X = 411,103.6257
 Δ = 7°20'57"
 D = 4°58'56"
 T = 73.86'
 L = 147.51'
 R = 1,150.00'
 PC STA = 453+46.85
 PCC STA = 454+94.36

C58

PI STA = 457+18.22
 Y = 221,465.6725
 X = 411,382.0621
 Δ = 63°44'57"
 D = 15°54'56"
 T = 223.86'
 L = 400.55'
 R = 360.00'
 PCC STA = 454+94.36
 PCC STA = 458+94.91

5

5



C59

PI STA = 459+77.01
 Y = 221,770.2139
 X = 411,411.4874
 Δ = 7°23'52"
 D = 4°30'41"
 T = 82.10'
 L = 163.97'
 R = 1,270.00'
 PCC STA = 458+94.91
 PT STA = 460+58.88

C61

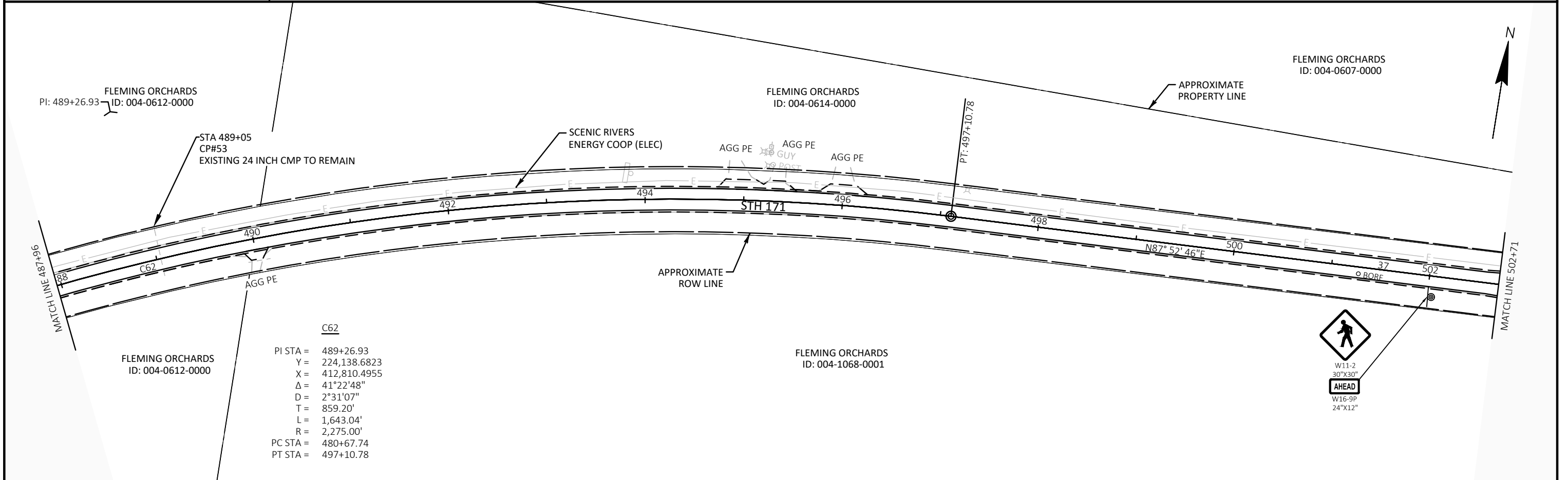
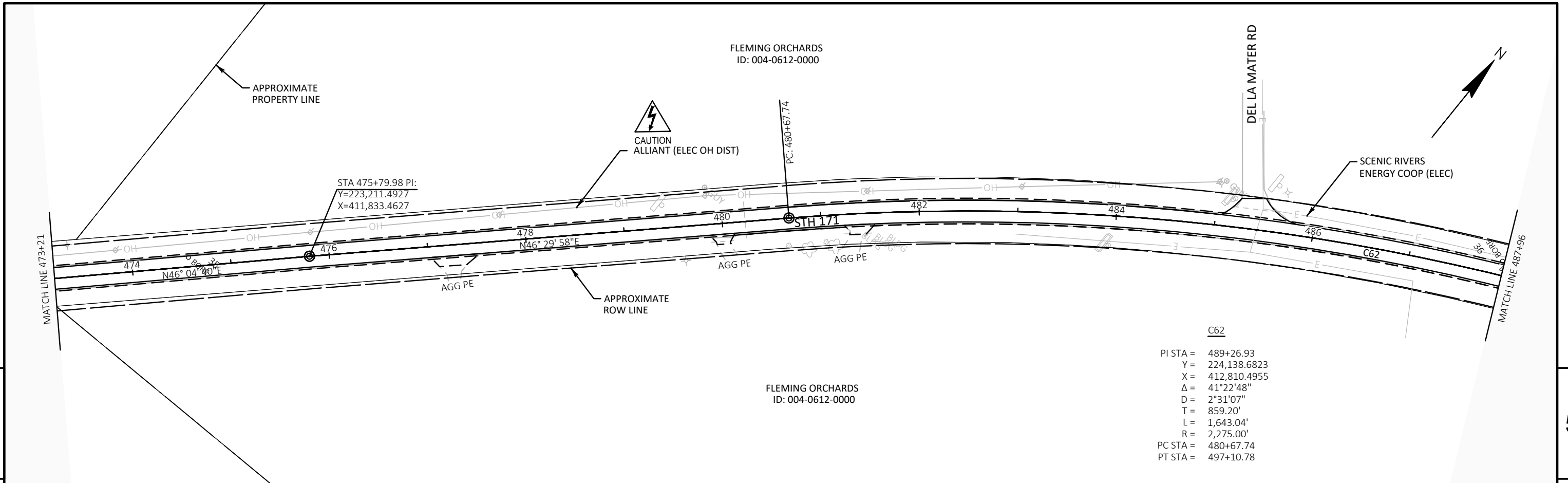
PI STA = 471+76.75
 Y = 222,931.5600
 X = 411,542.7942
 Δ = 9°14'55"
 D = 6°24'06"
 T = 72.39'
 L = 144.47'
 R = 895.00'
 PCC STA = 471+04.36
 PT STA = 472+48.83

C60

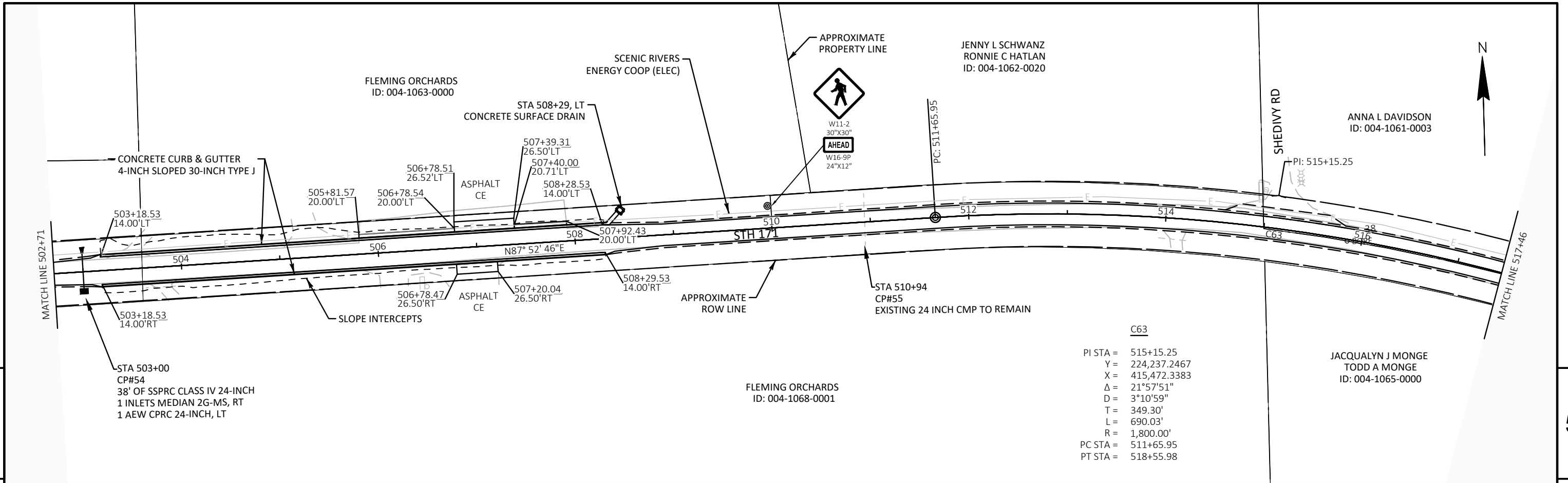
PI STA = 469+29.72
 Y = 222,722.7921
 X = 411,386.4490
 Δ = 37°49'23"
 D = 10°25'03"
 T = 188.43'
 L = 363.07'
 R = 550.00'
 PC STA = 467+41.29
 PCC STA = 471+04.36

5

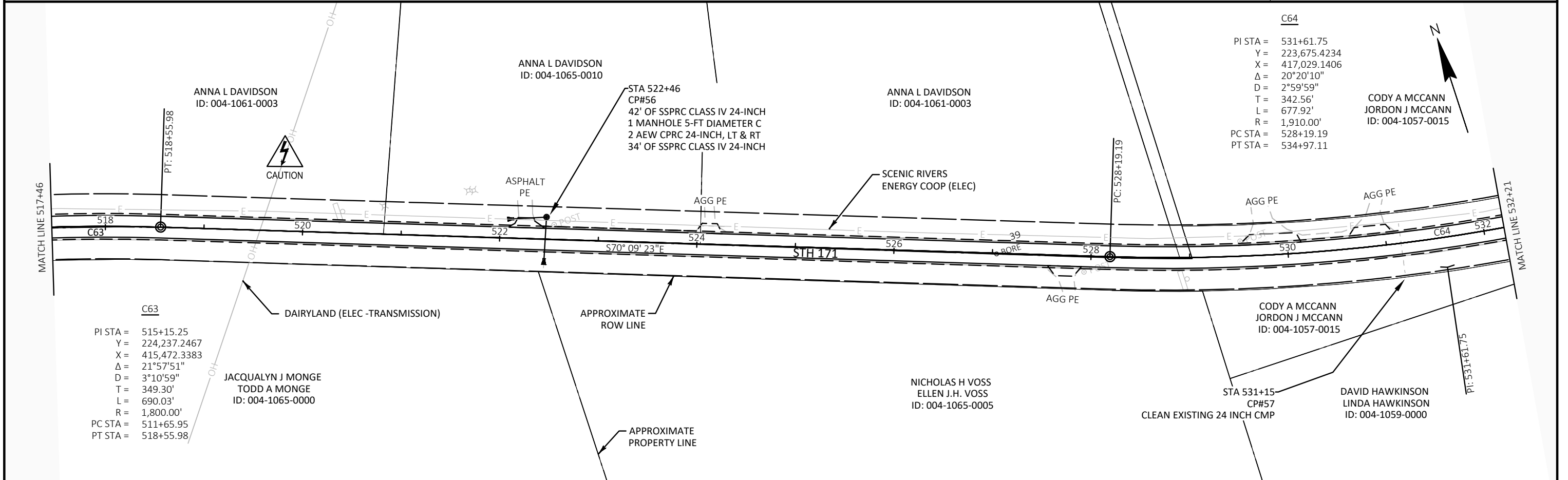
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD PLAN SHEET E



PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PLAN	SHEET	E
------------------------	--------------	------------------	------	-------	---



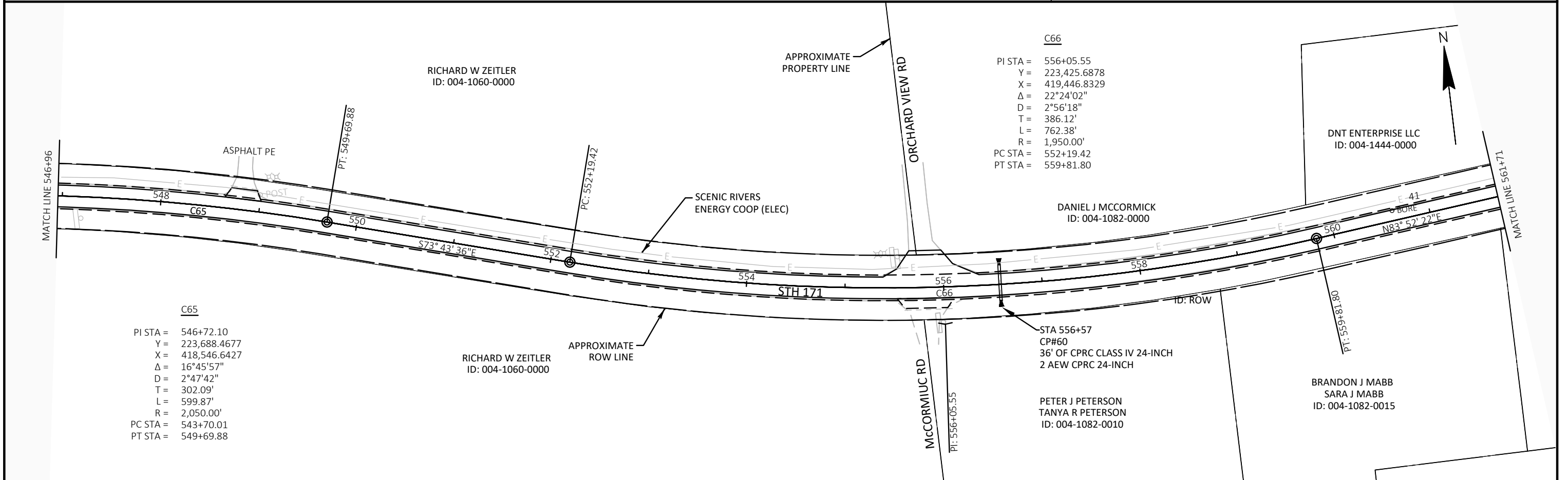
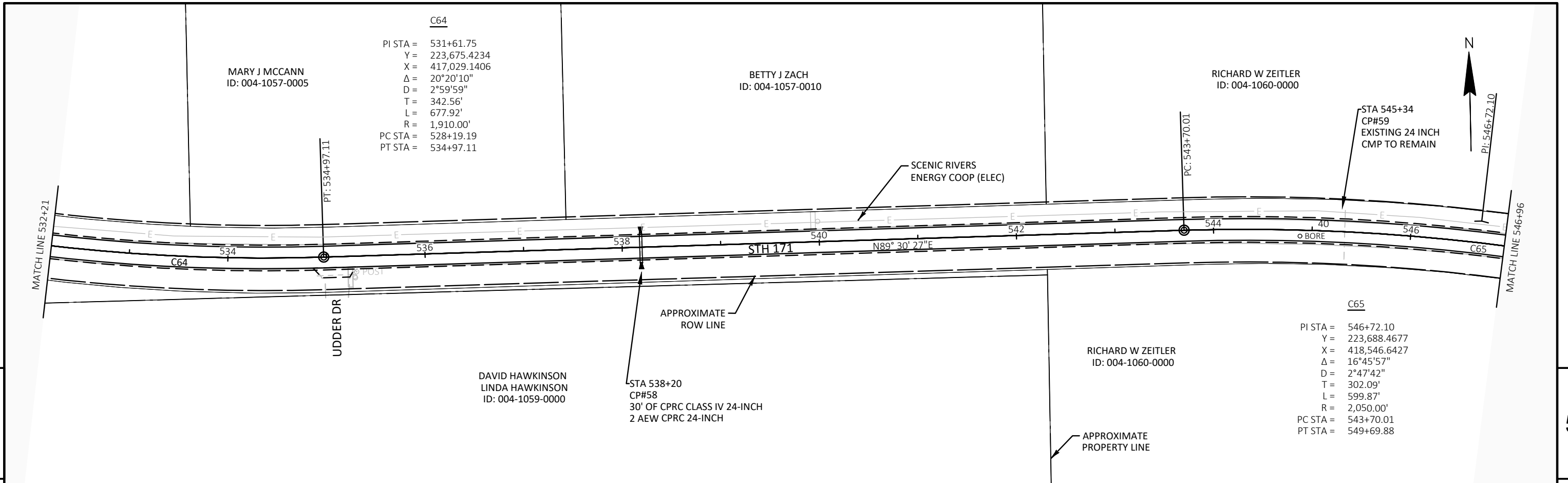
C63
 PI STA = 515+15.25
 Y = 224,237.2467
 X = 415,472.3383
 Δ = 21°57'51"
 D = 3°10'59"
 T = 349.30'
 L = 690.03'
 R = 1,800.00'
 PC STA = 511+65.95
 PT STA = 518+55.98



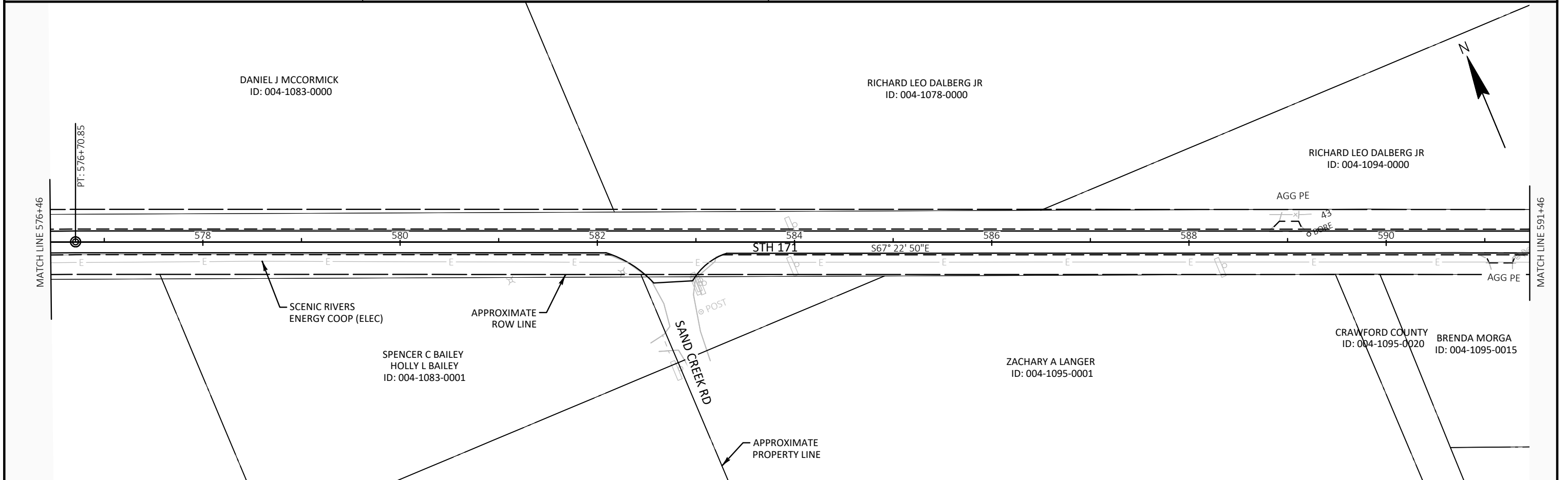
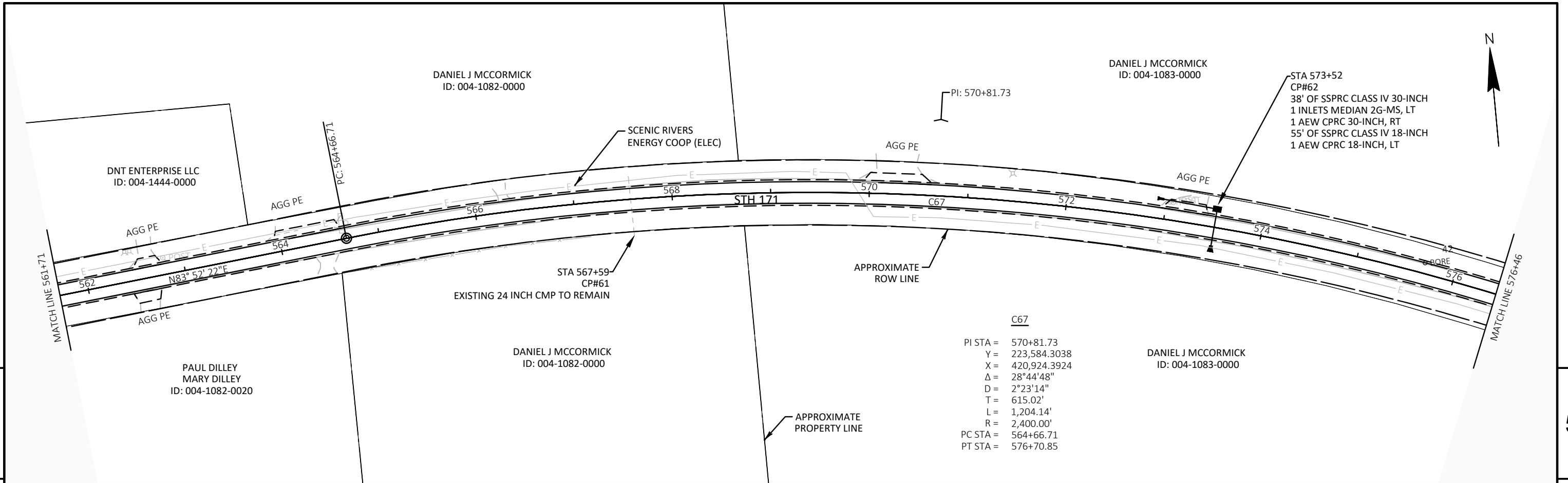
C64
 PI STA = 531+61.75
 Y = 223,675.4234
 X = 417,029.1406
 Δ = 20°20'10"
 D = 2°59'59"
 T = 342.56'
 L = 677.92'
 R = 1,910.00'
 PC STA = 528+19.19
 PT STA = 534+97.11

C63
 PI STA = 515+15.25
 Y = 224,237.2467
 X = 415,472.3383
 Δ = 21°57'51"
 D = 3°10'59"
 T = 349.30'
 L = 690.03'
 R = 1,800.00'
 PC STA = 511+65.95
 PT STA = 518+55.98

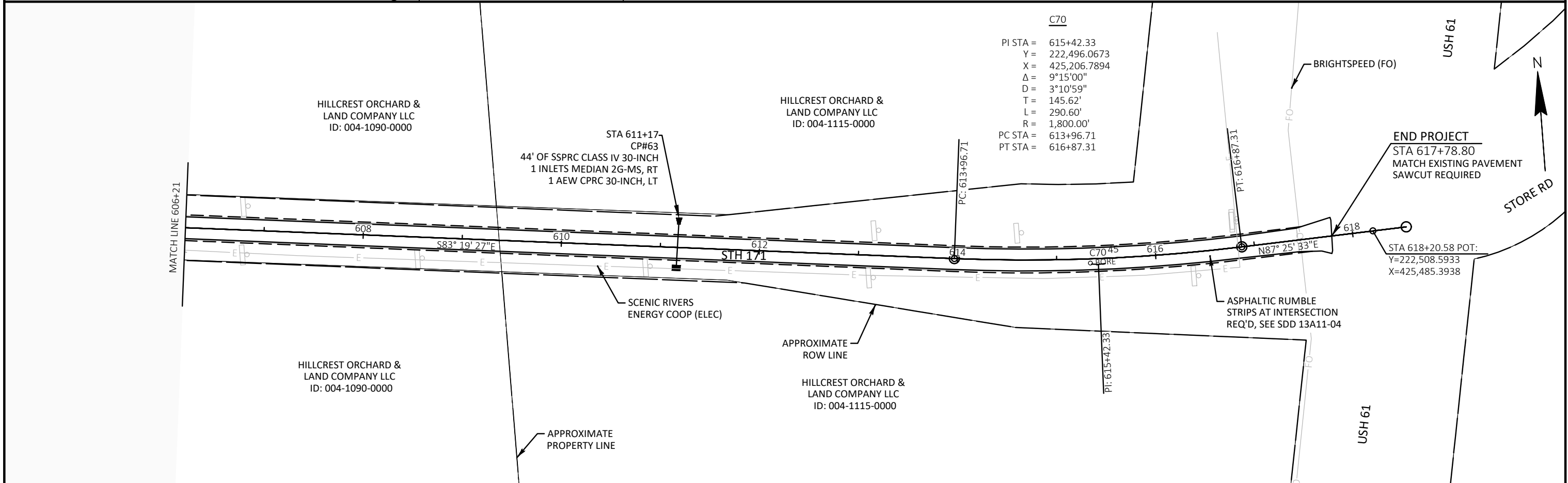
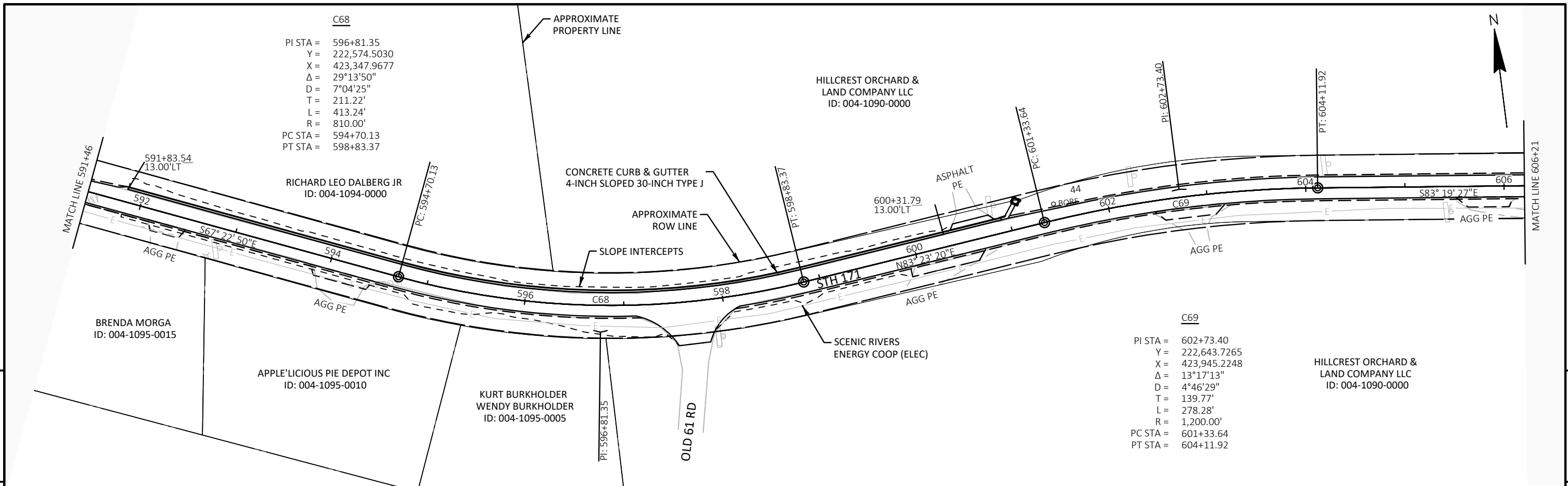
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PLAN	SHEET	E
------------------------	--------------	------------------	------	-------	---



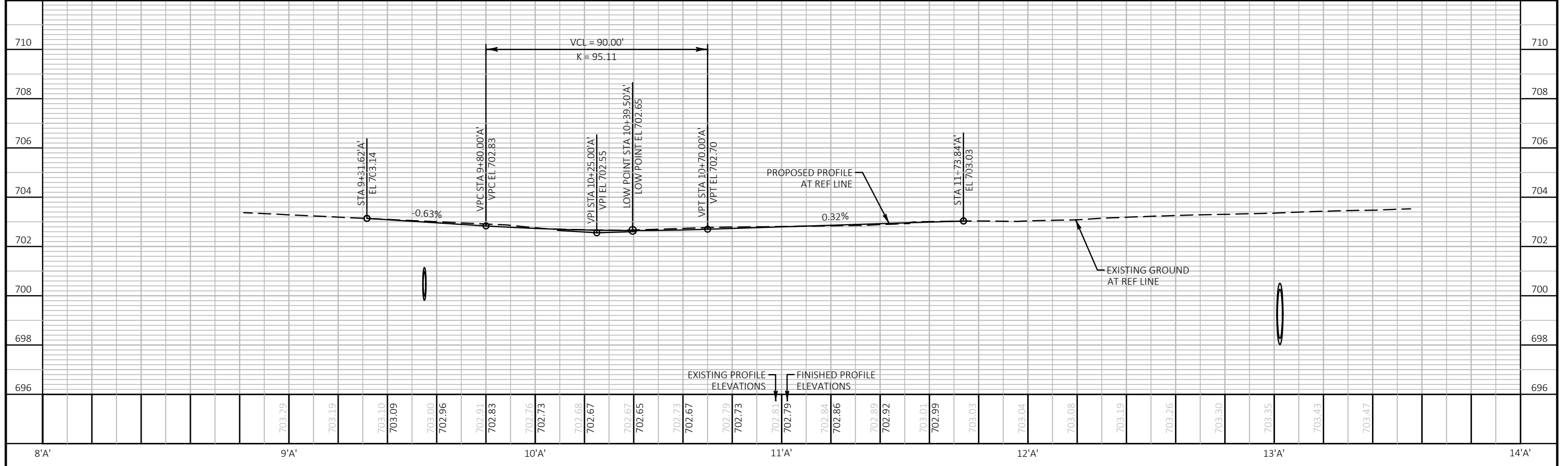
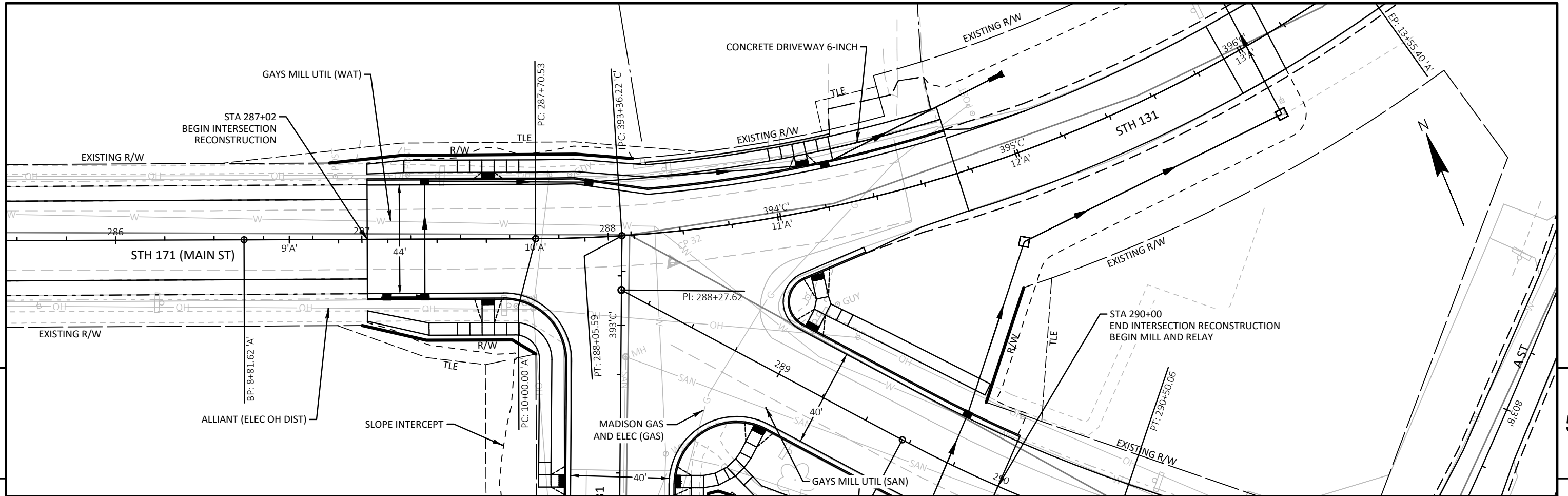
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PLAN	SHEET	E
------------------------	--------------	------------------	------	-------	---



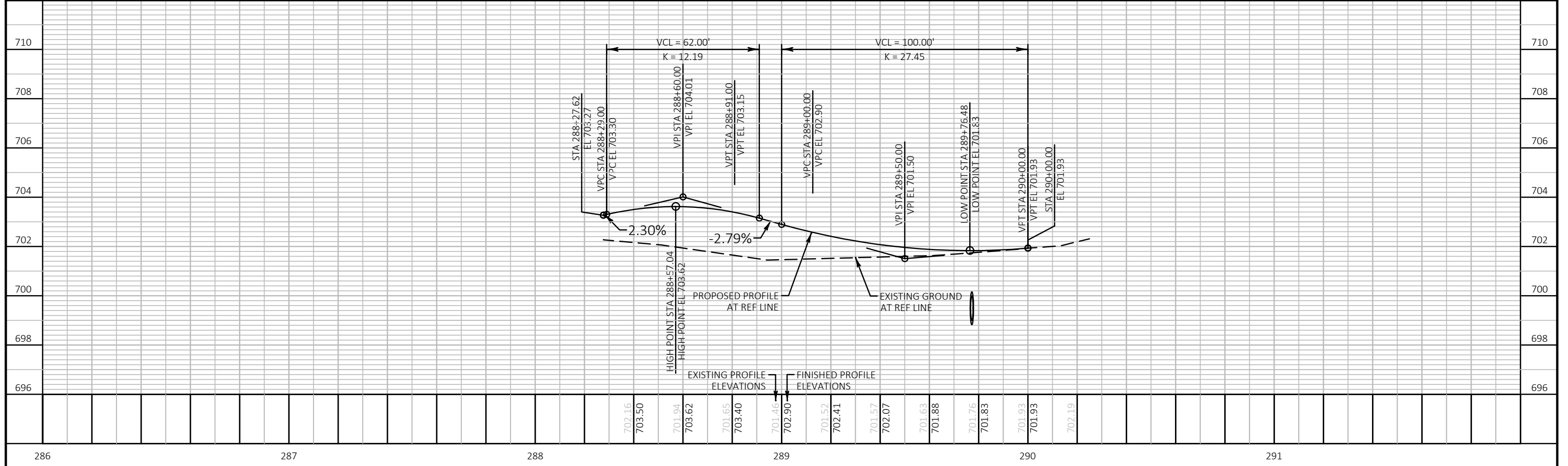
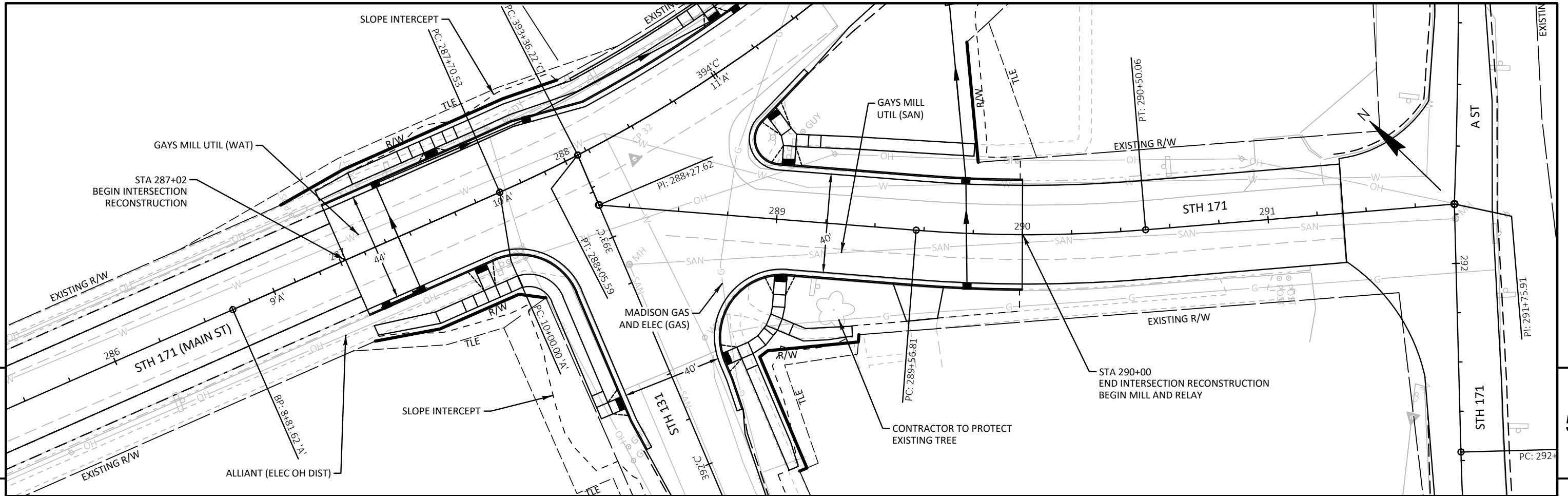
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PLAN	SHEET	E
------------------------	--------------	------------------	------	-------	---



PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PLAN	SHEET	E
------------------------	--------------	------------------	------	-------	---

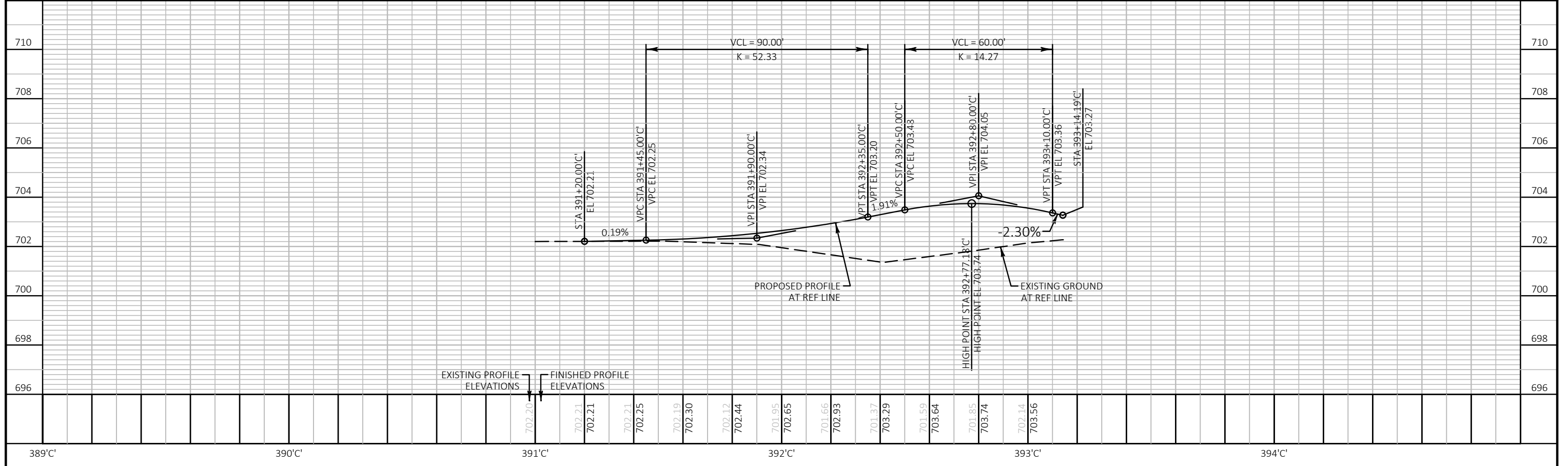
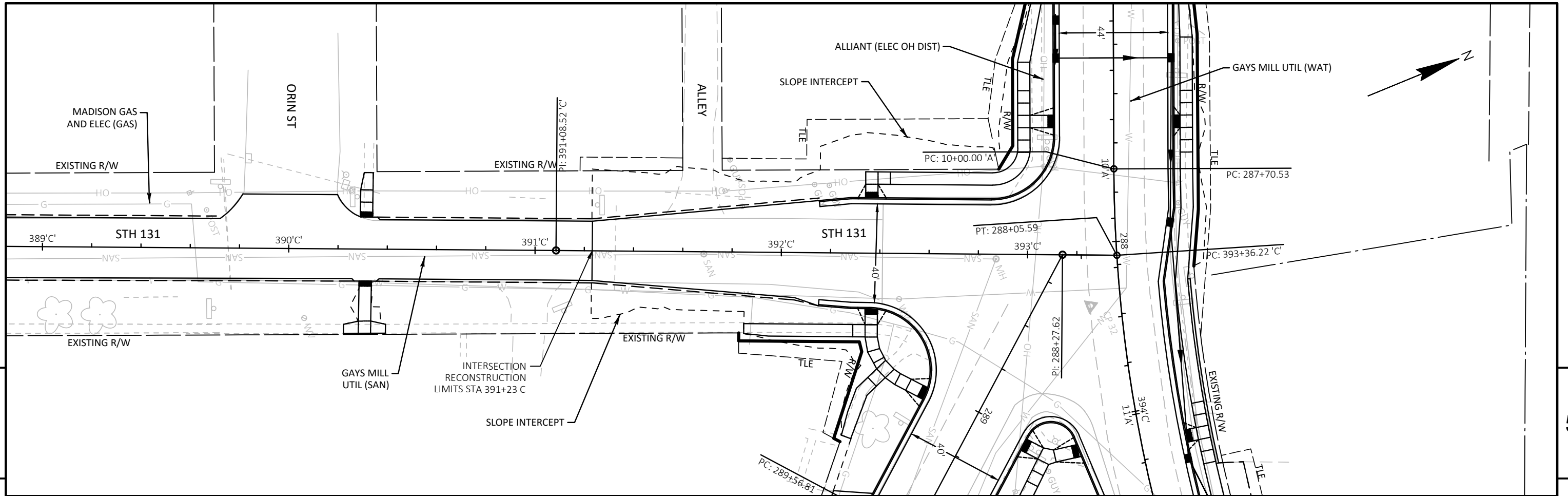


PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	PLAN AND PROFILE: MAIN STREET (STH 171/131)	SHEET E
------------------------	--------------	------------------	---	----------------



286	287	288	289	290	291
-----	-----	-----	-----	-----	-----

PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD PLAN AND PROFILE: STH 171 SHEET: E



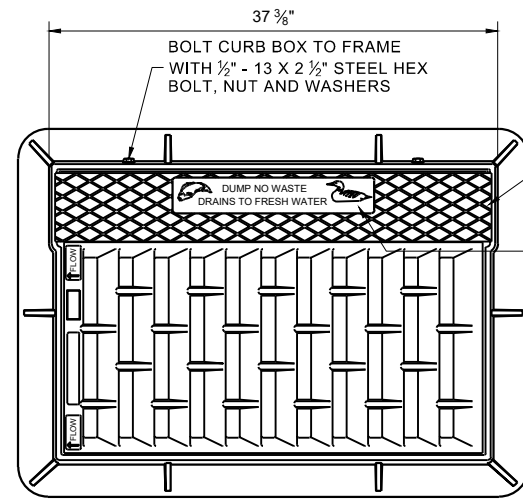
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD PLAN AND PROFILE: STH 131 SHEET: E

Standard Detail Drawing List

08A05-21A	INLET COVERS TYPE A, H, A-S, H-S & Z
08A05-21B	INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM
08A05-21C	INLET COVERS TYPE F, HM, HM-S, S, T, HM-GJ & HM-GJ-S
08B09-04	MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT, 8-FT, 9-FT, 10-FT DIAMETER
08C06-03	INLETS 3-FT AND 4-FT DIAMETER
08C07-03	INLETS 2X2-FT, 2X2.5-FT, 2X3-FT, 2.5X3-FT & 2X3.5-FT
08C08-03	INLETS MEDIAN 1 AND 2 GRATE
08D01-23A	CONCRETE CURB & GUTTER
08D01-23B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D04-07	CONCRETE SURFACE DRAINS & ASPHALTIC FLUMES
08D05-21A	CURB RAMPS TYPES 1 AND 1-A
08D05-21B	CURB RAMPS TYPES 2 AND 3
08D05-21C	CURB RAMPS TYPES 4A AND 4A1
08D05-21D	CURB RAMPS TYPE 4B AND 4B1
08D05-21E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-21F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-21G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D20-01	DRIVEWAYS WITH CURB & GUTTER RETURNS
08D21-01	DRIVEWAYS WITHOUT CURB & GUTTER
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E15-01	CULVERT PIPE CHECK
08F01-11	APRON ENDWALLS FOR CULVERT PIPE
08F04-08	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
13A11-04A	CENTERLINE RUMBLE STRIPS - ASPHALT
13C13-11	URBAN DOWELED CONCRETE PAVEMENT
13C14-07A	BASE PATCHING CONCRETE
13C14-07B	BASE PATCHING CONCRETE
13C14-07C	BASE PATCHING CONCRETE
13C19-03	HMA LONGITUDINAL JOINTS
14B28-04A	GUARDRAIL MOW STRIP
14B28-04B	GUARDRAIL MOW STRIP
14B29-01	SAFETY EDGE
14B42-07A	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07B	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07C	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07D	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B44-04A	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-04B	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-04C	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B53-02A	SHORT RADIUS BEAM GUARD (MGS) SHORT RADIUS TERMINAL (MGS)
14B53-02B	SHORT RADIUS BEAM GUARD (MGS) SHORT RADIUS TERMINAL (MGS)
14B53-02C	SHORT RADIUS BEAM GUARD (MGS) SHORT RADIUS TERMINAL (MGS)
14B53-02D	SHORT RADIUS BEAM GUARD (MGS) SHORT RADIUS TERMINAL (MGS)
14B53-02E	SHORT RADIUS BEAM GUARD (MGS) SHORT RADIUS TERMINAL (MGS)
14B53-02F	SHORT RADIUS BEAM GUARD (MGS) SHORT RADIUS TERMINAL (MGS)
14B53-02G	SHORT RADIUS BEAM GUARD (MGS) SHORT RADIUS TERMINAL (MGS)
14B53-02H	SHORT RADIUS BEAM GUARD (MGS) SHORT RADIUS TERMINAL (MGS)
14B53-02I	SHORT RADIUS BEAM GUARD (MGS) SHORT RADIUS TERMINAL (MGS)
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-09C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C08-23B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C11-10A	CHANNELIZING DEVICES FLEXIBLE TUBULAR MARKER POST
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C19-09A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C33-05	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-06A	PAVEMENT MARKING (INTERSECTIONS)
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D30-10A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10B	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

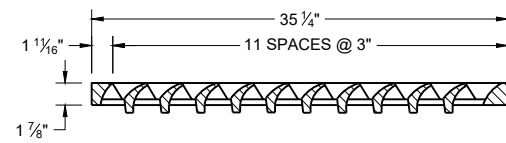
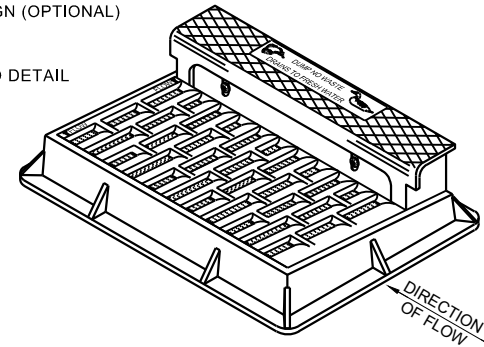
Standard Detail Drawing List

15D30-10E	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10G	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10J	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10K	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-10L	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D44-02	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES
15D51-01	TRAFFIC CONTROL, MOBILE OPERATIONS ON AN UNDIVIDED ROADWAY

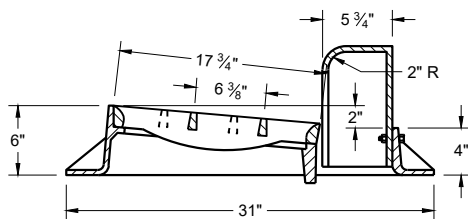
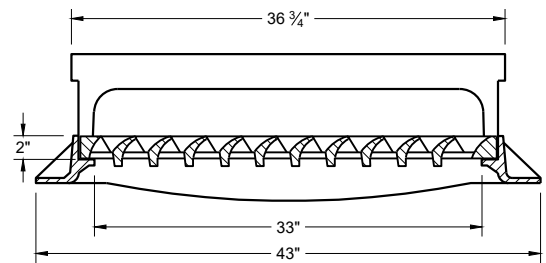
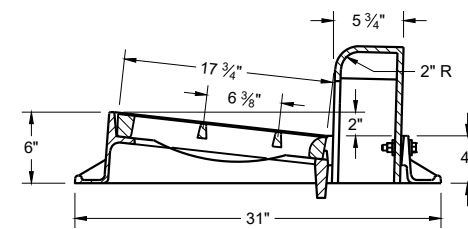
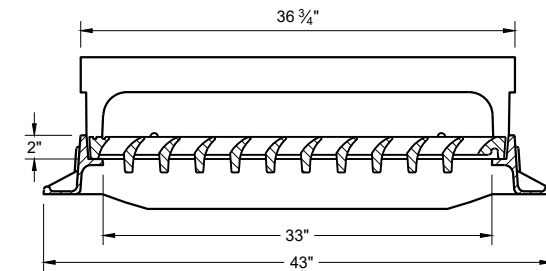


NOTE: EITHER CASTING IS ACCEPTABLE

TYPE "C" CHECKERED TOP DESIGN (OPTIONAL)
SEE LOGO DETAIL



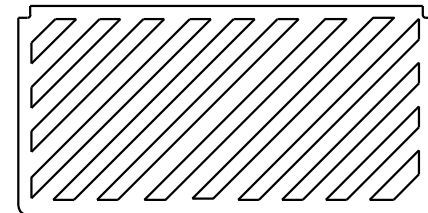
NOTE: CURB BOX HEIGHT ADJUSTABLE 6" - 9"



TYPE "H"

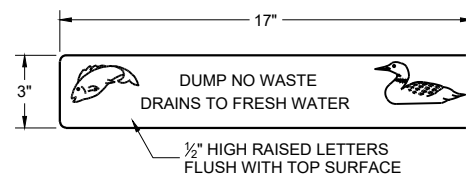
NOTE: EITHER CASTING IS ACCEPTABLE

1 1/8" DIAGONAL BARS WITH 1 5/8" OPENINGS



SPECIAL GRATE FOR TYPE "H" COVER

(MEASURES 35" X 17 3/4" X 2")
(NOTED AS TYPE H-S ON DRAINAGE TABLE)



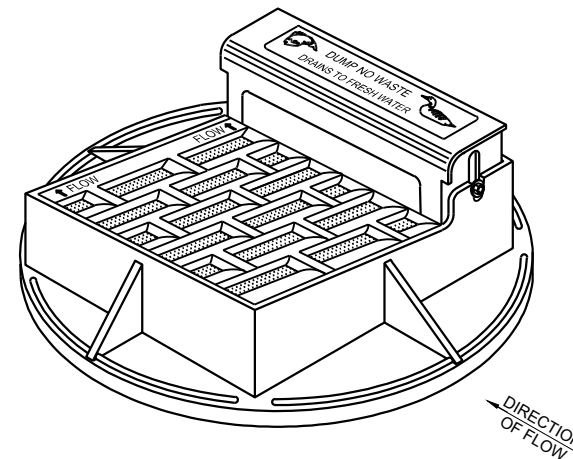
LOGO DETAIL

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

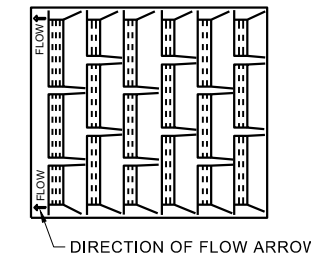
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

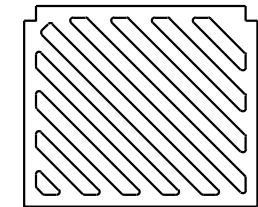


NOTE: CURB BOX HEIGHT ADJUSTABLE 6" - 9"

NOTE: EITHER CASTING IS ACCEPTABLE

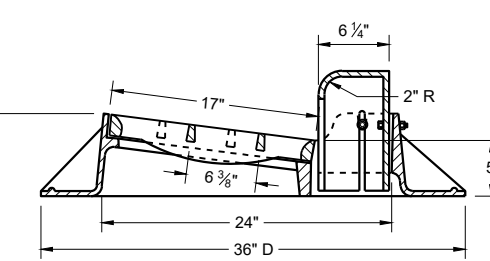
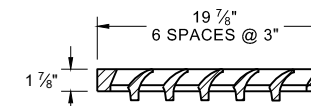
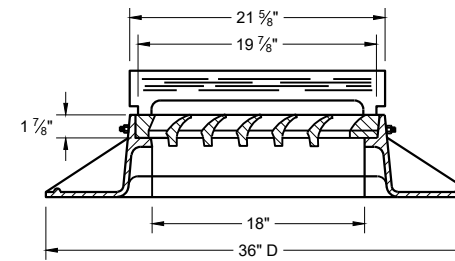


1" DIAGONAL BARS WITH 1 1/2" OPENINGS

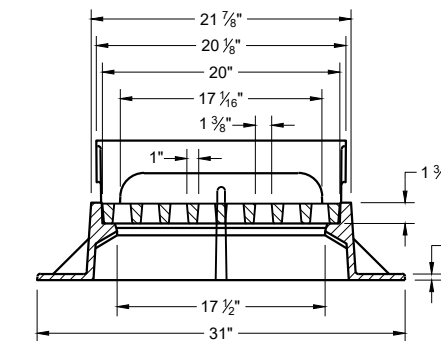
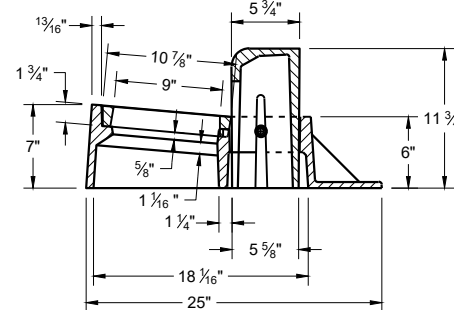


SPECIAL GRATE FOR TYPE "A" COVER

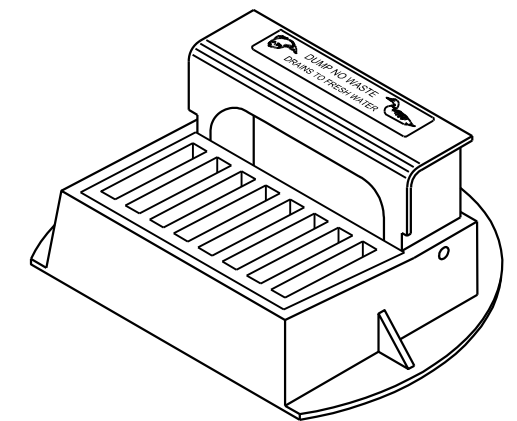
(MEASURES 19 3/4" X 17" X 1 7/8")
(NOTED AS TYPE A-S ON DRAINAGE TABLE)



TYPE "A"



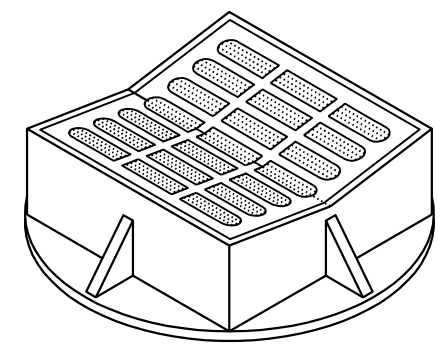
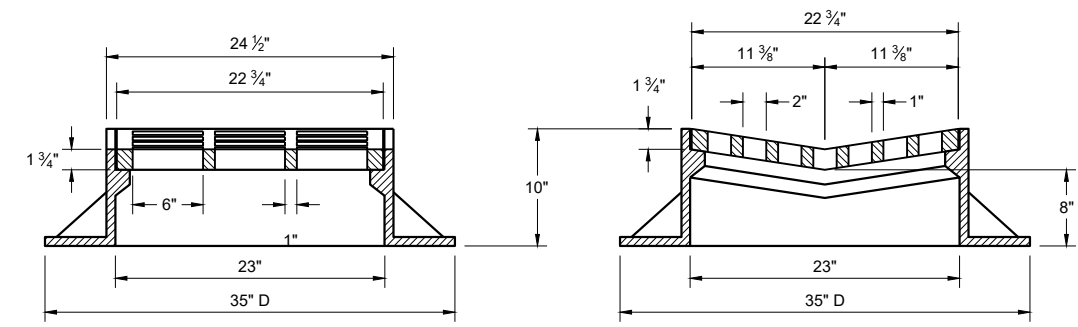
TYPE "Z"



INLET COVERS TYPES A, H, A-S, H-S AND Z

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

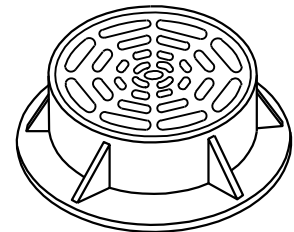
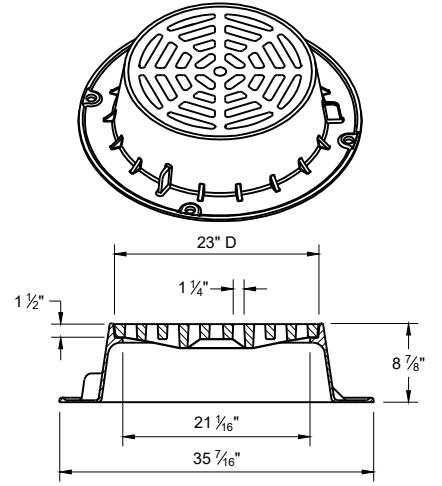
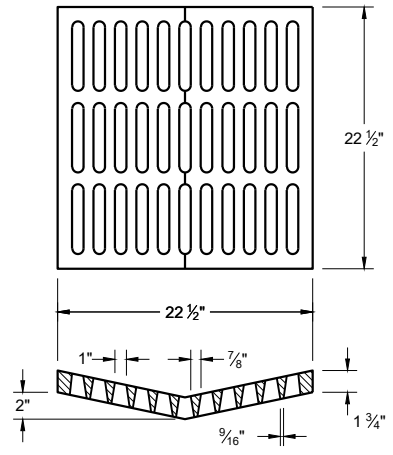
APPROVED
December 2023 DATE /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA



TYPE "B"

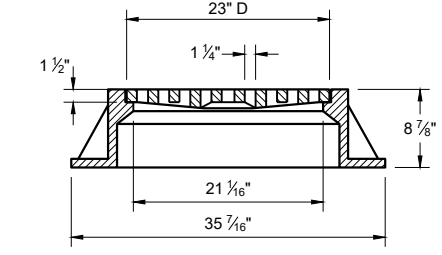
ALTERNATIVE GRATE FOR TYPE "B" COVER

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS POSSIBLE
NOTED AS TYPE B - A ON THE DRAINAGE TABLE



TYPE "C"

NOTE: EITHER CASTING IS ACCEPTABLE

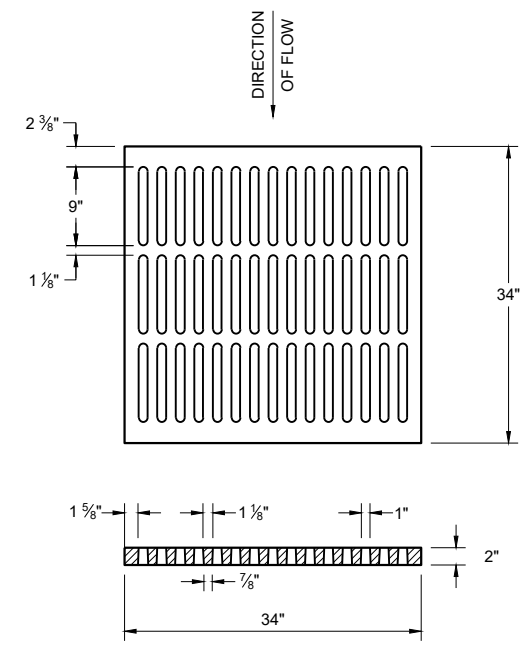


GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

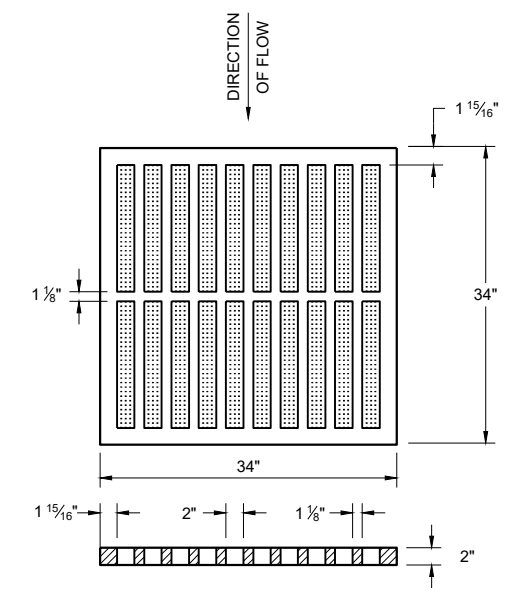
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



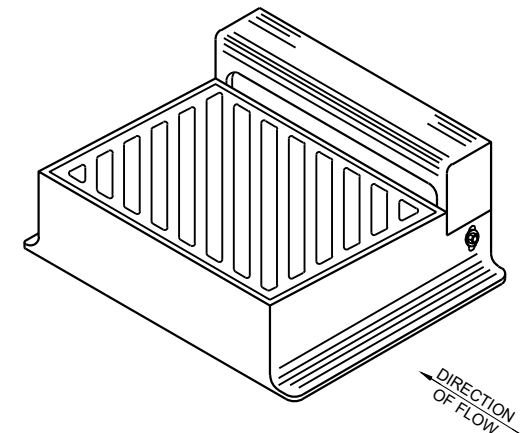
ALTERNATIVE TYPE "MS"

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS PERMITTED
NOTED AS TYPE MS-A ON THE DRAINAGE TABLE

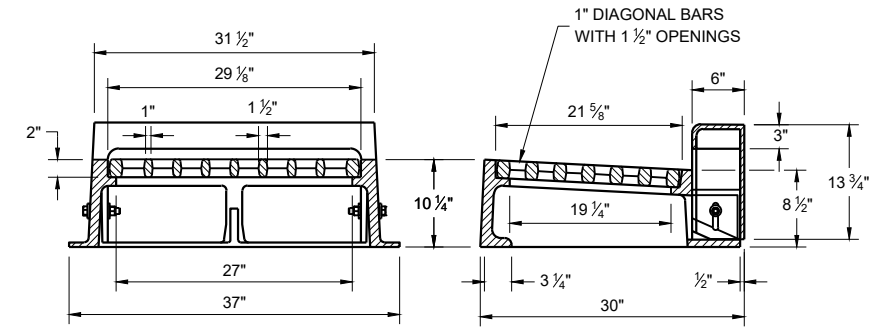


TYPE "MS"

USE ON FREEWAYS AND EXPRESSWAYS
NOTED AS TYPE MS ON THE DRAINAGE TABLE



DIAGONAL SLOTS SHALL BE ORIENTED TO THE DIRECTION OF FLOW AS ILLUSTRATED. GRATES ARE MANUFACTURED TO BE REVERSIBLE.



TYPE "WM"

NOTE: CURB BOX HEIGHT ADJUSTABLE 6" - 9"

6

6

SDD 08A05-21b

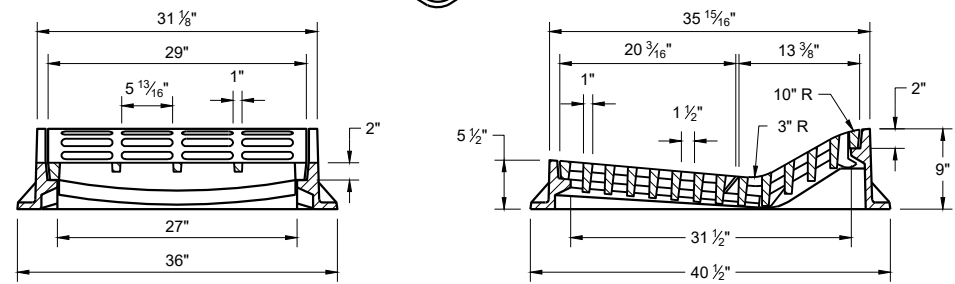
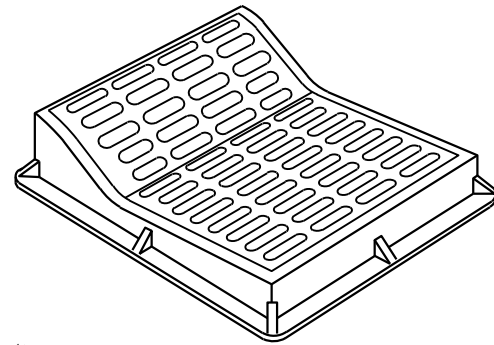
SDD 08A05-21b

INLET COVERS TYPES B, B-A, C, MS, MS-A AND WM	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED December 2023 DATE	/s/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	

GENERAL NOTES

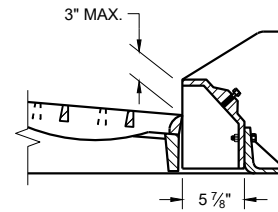
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.



TYPE "F"

USE WITH TYPES "A" AND "D" CONCRETE CURB AND GUTTER, 36"

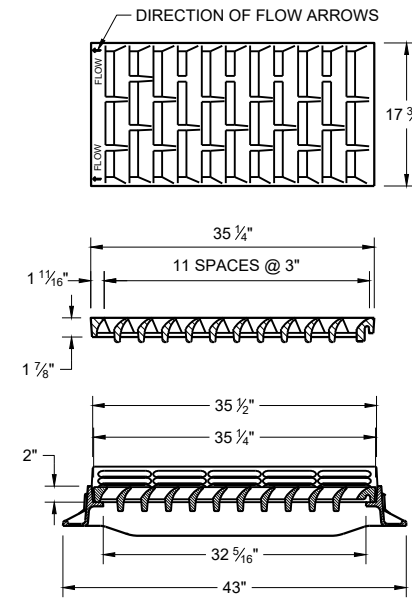


ALTERNATIVE CURB BOX FOR TYPE "HM" COVER

USE WITH TYPES "G" AND "J" CONCRETE CURB AND GUTTER, 30 INCH NOTED AS TYP "HM-GJ" ON DRAINAGE TABLE

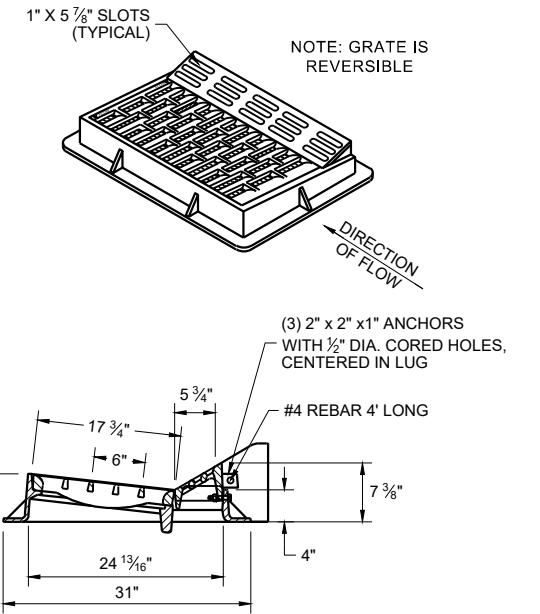
NOTE: SPECIAL GRATE FOR THE TYPE "H" COVER MAY ALSO BE USED FOR THE TYPE "HM-GJ" COVER.

NOTED AS TYPE HM-GJ-S ON THE DRAINAGE TABLE.



TYPE "HM"

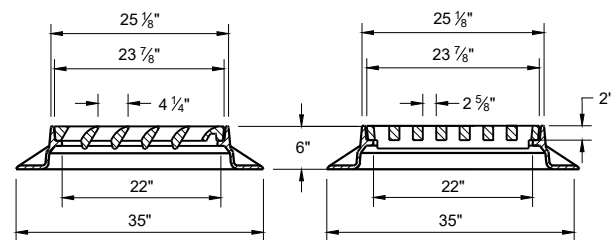
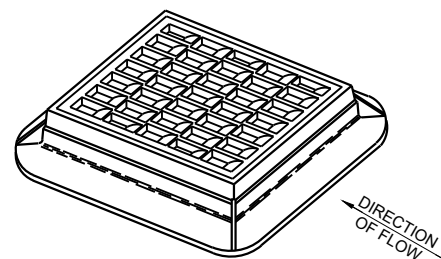
USE WITH TYPES "A" AND "D" CONCRETE CURB AND GUTTER, 36"



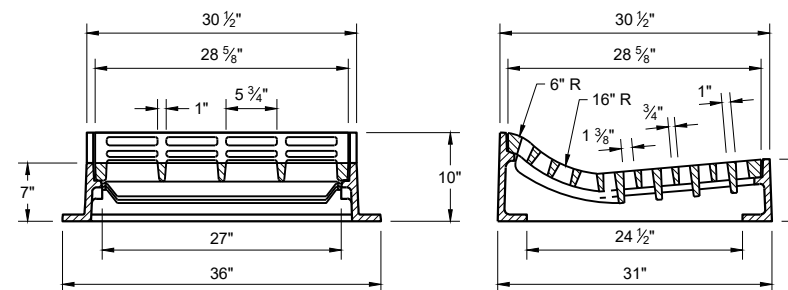
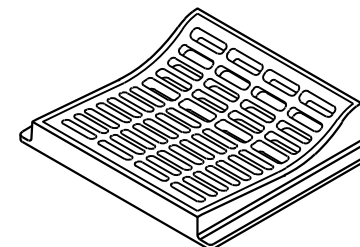
NOTE: SPECIAL GRATE FOR THE TYPE "H" COVER MAY ALSO BE USED FOR THE TYPE "HM-GJ" COVER.

NOTED AS TYPE HM-GJ-S ON THE DRAINAGE TABLE.

6



TYPE "S"



TYPE "T"

USE WITH TYPES "R" AND "T" CONCRETE CURB AND GUTTER, 36"

**INLET COVERS
TYPES F, HM, HM-S, S, T,
HM-GJ AND HM-GJ-S**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

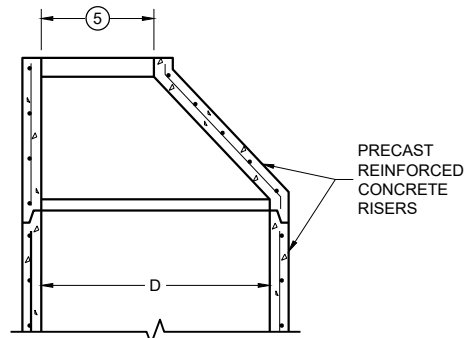
APPROVED
December 2023 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR

SDD 08A05-21c

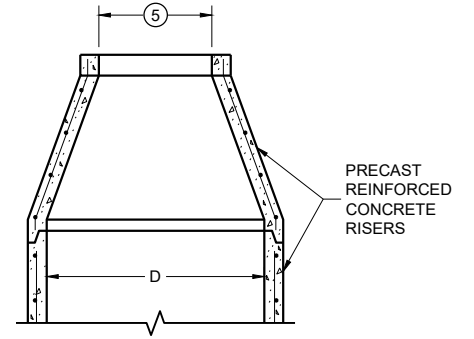
SDD 08A05-21c



**PLAN VIEW
CIRCULAR OPENING**



OPTIONAL PRECAST REINFORCED CONCRETE ECCENTRIC TOP



OPTIONAL PRECAST REINFORCED CONCRETE CONCENTRIC TOP

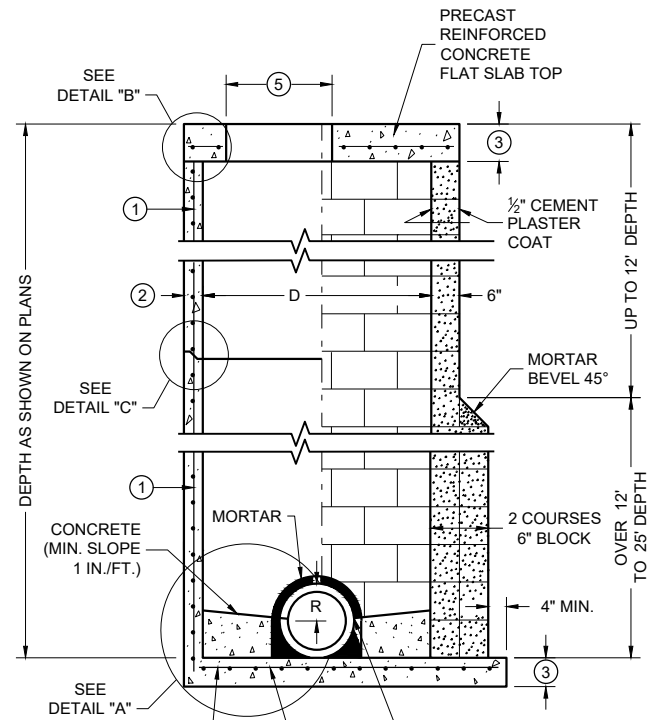
MANHOLE COVER OPENING MATRIX

MANHOLE COVER TYPE OPENING SIZE (FT.)	C	ALL JS	K	L	M
2 DIA.	X	X		X	
3 DIA.			X		X

PIPE MATRIX

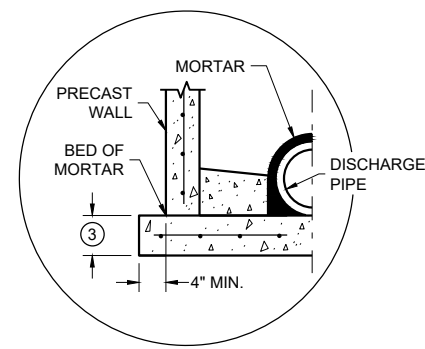
MANHOLE SIZE (DIA.)	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES		MINIMUM WALL THICKNESS (IN)	MINIMUM PRECAST FLAT SLAB TOP AND BASE THICKNESS
	180° SEPARATION (IN)	90° SEPARATION (IN)		
3-FT	15	12	4	6
4-FT	24	18	4	6
5-FT	36	24	5	8
6-FT	42	36	6	8
7-FT	48	36/42 *	7	8
8-FT	60	42	8	8
9-FT	66	54	9	10
10-FT	72	60	10	10

*A 36" PIPE AND A 42" PIPE CAN BE PLACED WITHIN 90 DEGREES. SEE MINIMUM HORIZONTAL PIPE SEPARATION DETAIL.

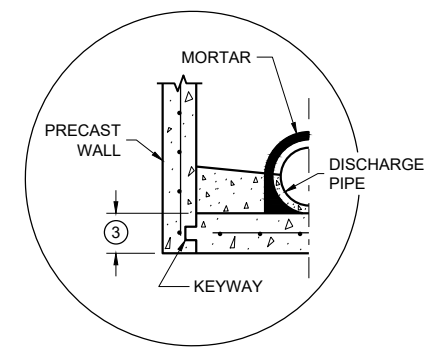


SECTION A - A

PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE
CONCRETE BLOCK WITH CAST IN PLACE OR PRECAST REINFORCED CONCRETE BASE ①

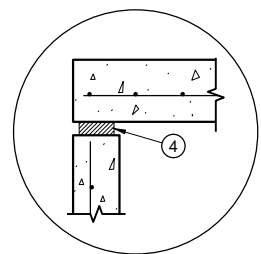


SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

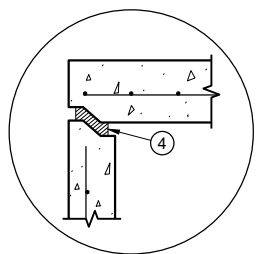


PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION

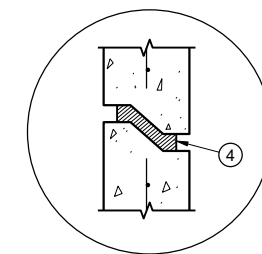
DETAIL "A"



TOP WITH PLAIN END JOINT



TOP WITH TONGUE AND GROOVE JOINT



RISER WITH TONGUE AND GROOVE JOINT

DETAIL "B"

DETAIL "C"

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST MANHOLE UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONCRETE CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED CONCRETE FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES. CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH: 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF 1/2 INCH AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST MANHOLE UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

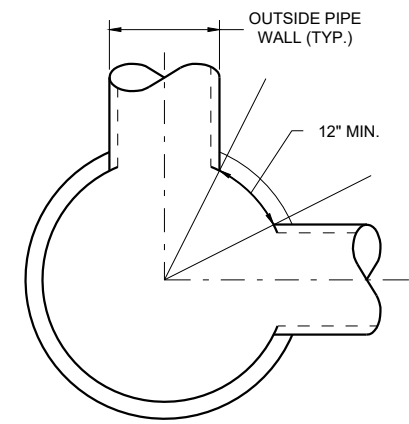
PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

CONCRETE BLOCK WILL NOT BE PERMITTED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "D".

- ① FOR PRECAST MANHOLES AND REINFORCED CONCRETE BASES PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.
- ② SEE PIPE MATRIX TABLE FOR MINIMUM WALL THICKNESS FOR PRECAST MANHOLES
- ③ SEE PIPE MATRIX TABLE FOR MINIMUM THICKNESS OF PRECAST FLAT SLAB TOPS AND BASES.
- ④ JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 OR RUBBER GASKETS CONFORMING TO ASTM C443.
- ⑤ SEE MANHOLE COVER OPENING MATRIX.



**MINIMUM HORIZONTAL PIPE SEPARATION
DETAIL "D"**

**MANHOLES, 3-FT, 4-FT
5-FT, 6-FT, 7-FT, 8-FT, 9-FT
AND 10-FT DIAMETER**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2023 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR
FHWA

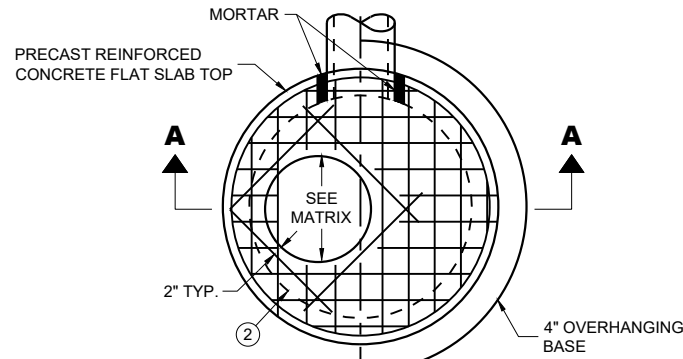
6

6

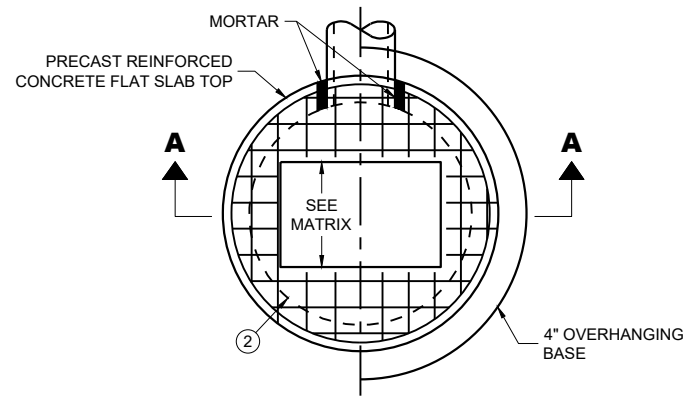
SDD 08B09-04

SDD 08B09-04

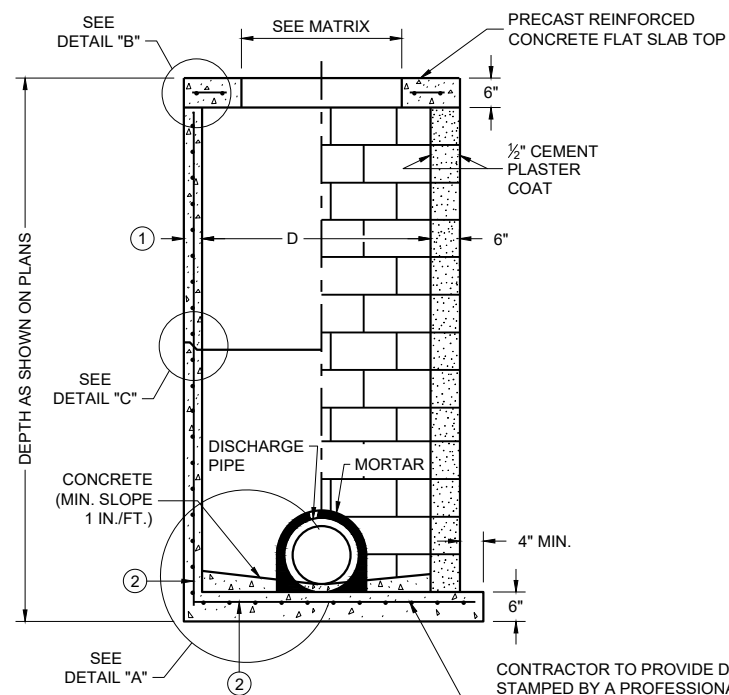
MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT, 8-FT, 9-FT AND 10-FT DIAMETER



PLAN VIEW CIRCULAR OPENING



PLAN VIEW RECTANGULAR OPENING



SECTION A - A

PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

CONCRETE BLOCK WITH CAST IN PLACE OR PRECAST REINFORCED CONCRETE BASE

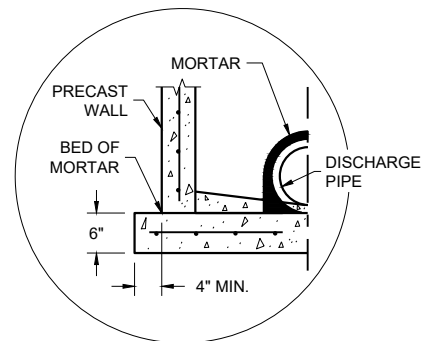
CIRCULAR INLETS WITH FLAT TOP

CATCH BASIN COVER OPENING MATRIX

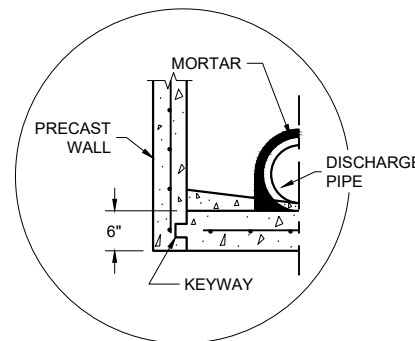
INLET SIZE	INLET COVER TYPE OPENING SIZE (FT.)	ALL A'S	ALL B'S	BW	C	F	ALL H'S	S	T	V	WM	Z
		3-FT	2 DIA.				X					
	2 X 2	X	X					X		X		
4-FT	2 DIA.				X							X
	2 X 2	X	X					X	X	X		
	2 X 2.5			X			X					
	2 X 3						X					
	2.5 X 3					X						

PIPE MATRIX

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18

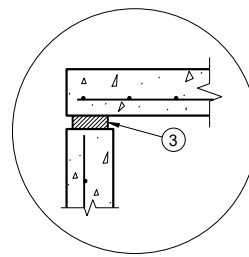


SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

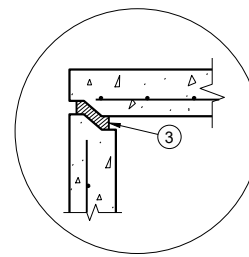


PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION

DETAIL "A"

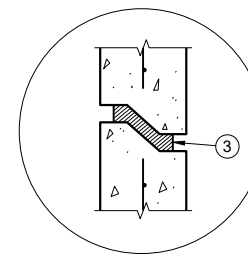


TOP WITH PLAIN END JOINT



TOP WITH TONGUE AND GROOVE JOINT

DETAIL "B"



RISER WITH TONGUE AND GROOVE JOINT

DETAIL "C"

INLETS 3-FT AND 4-FT DIAMETER

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

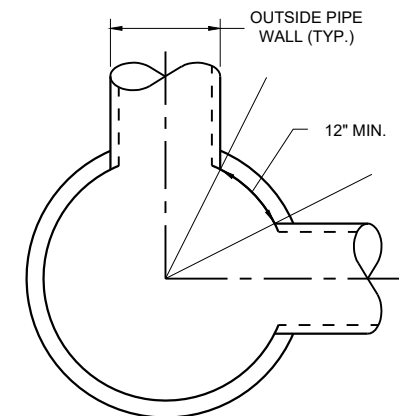
ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "D".

- ① MINIMUM WALL THICKNESS SHALL BE 4 INCHES FOR 3-FT DIAMETER AND 5 INCHES FOR 4-FT DIAMETER PRECAST INLETS.
- ② FOR PRECAST INLETS AND REINFORCED CONCRETE BASES PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.
- ③ JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 OR RUBBER GASKETS CONFORMING TO ASTM C443.

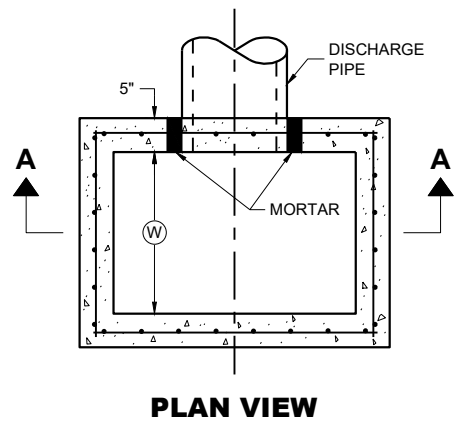


**MINIMUM HORIZONTAL PIPE SEPARATION
DETAIL "D"**

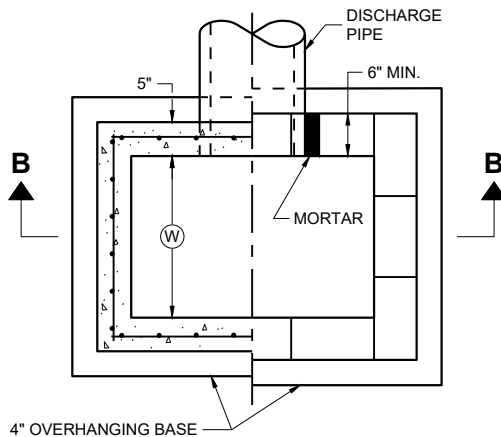
INLETS 3-FT AND 4-FT DIAMETER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2023 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR

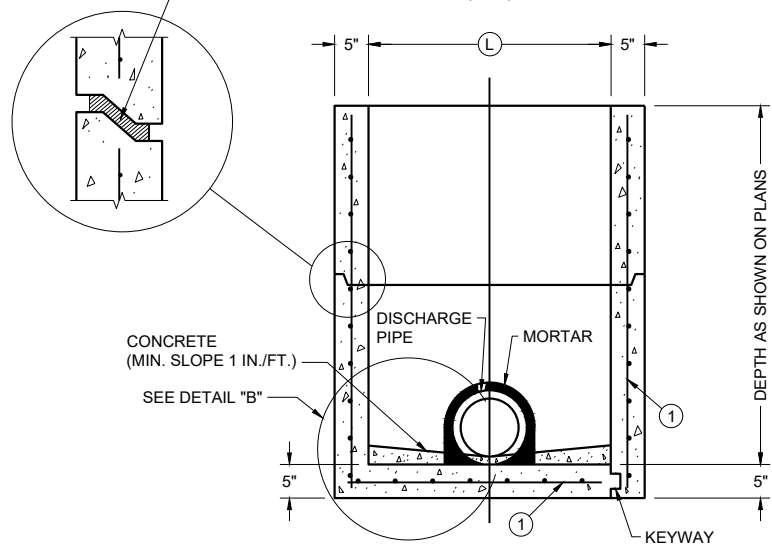


PLAN VIEW



PLAN VIEW

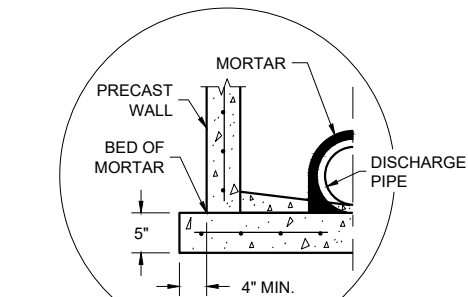
RISER JOINT TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP.)



PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

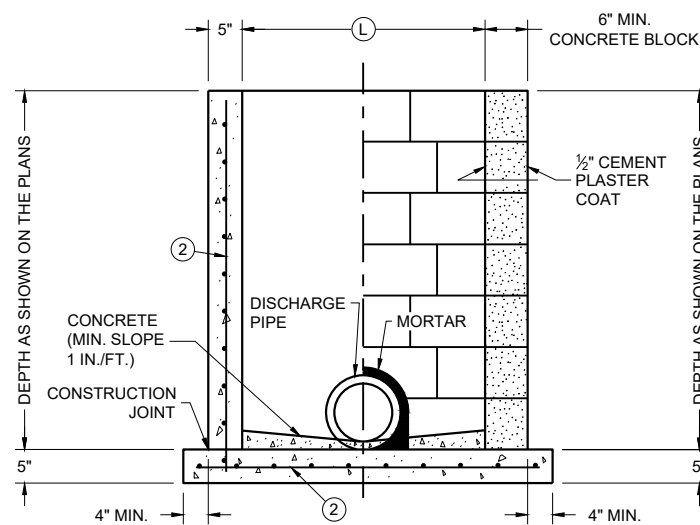
SECTION A - A

PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

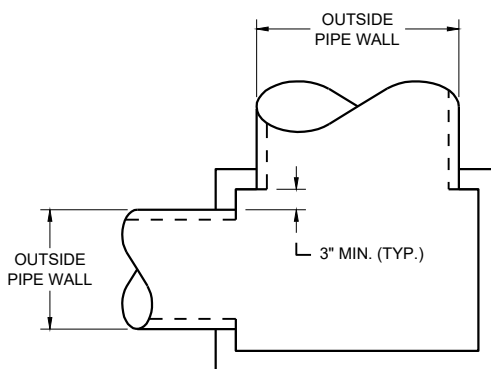
DETAIL "B"



CAST IN PLACE REINFORCED CONCRETE

SECTION B - B

CONCRETE BLOCK WITH CAST IN PLACE OR PRECAST REINFORCED CONCRETE BASE



DETAIL "A"

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3" CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

- ① FOR PRECAST INLETS AND REINFORCED CONCRETE BASES PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913
- ② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

CATCH BASIN COVER MATRIX

INLET SIZE	WIDTH (W) (FT.)	LENGTH (L) (FT.)	INLET COVER TYPE												
			ALL A'S	ALL B'S	BW	F	ALL H'S	S	T	V	WM	V V-B			
2 X 2-FT	2	2	X	X				X							
2 X 2.5-FT	2	2.5			X			X	X	X	X				
2 X 3-FT	2	3					X								
2.5 X 3-FT	2.5	3				X									
2 X 3.5-FT	2	3.5													X

PIPE MATRIX

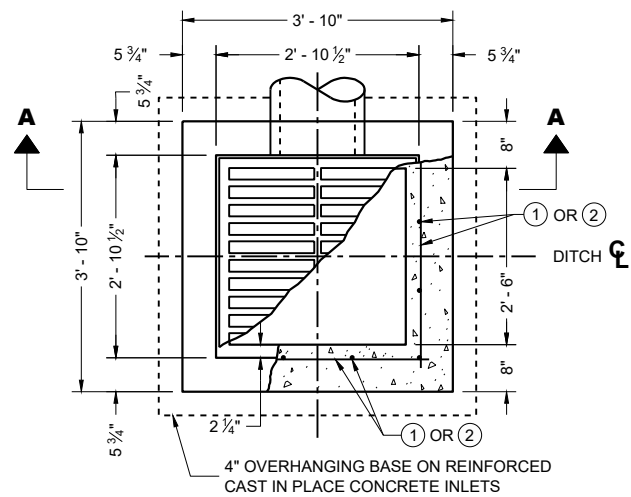
CATCH BASIN SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	WIDTH (IN)	LENGTH (IN)
2 X 2-FT	12	12
2 X 2.5-FT	12	18
2 X 3-FT	12	24
2.5 X 3-FT	18	24
2 X 3.5-FT	12	30

INLETS 2 X 2-FT, 2 X 2.5-FT, 2 X 3-FT, 2.5 X 3-FT AND 2 X 3.5-FT

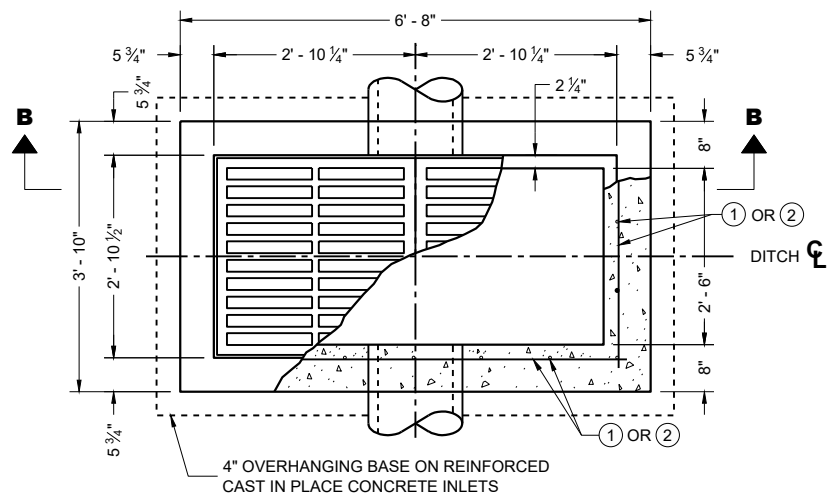
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2023 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA

INLETS 2 X 2-FT, 2 X 2.5-FT, 2 X 3-FT, 2.5 X 3-FT AND 2X3.5-FT



PLAN VIEW



PLAN VIEW

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR INLETS WHICH MAY INCLUDE PRECAST REINFORCED CONCRETE INLETS, SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL MEDIAN INLETS ARE DESIGNATED ON THE PLANS AS "INLETS, 1G-MS", ETC. THE FIRST NUMBER AND LETTER DESIGNATE THE TYPE OF STRUCTURE, AND THE FOLLOWING LETTERS DESIGNATE THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

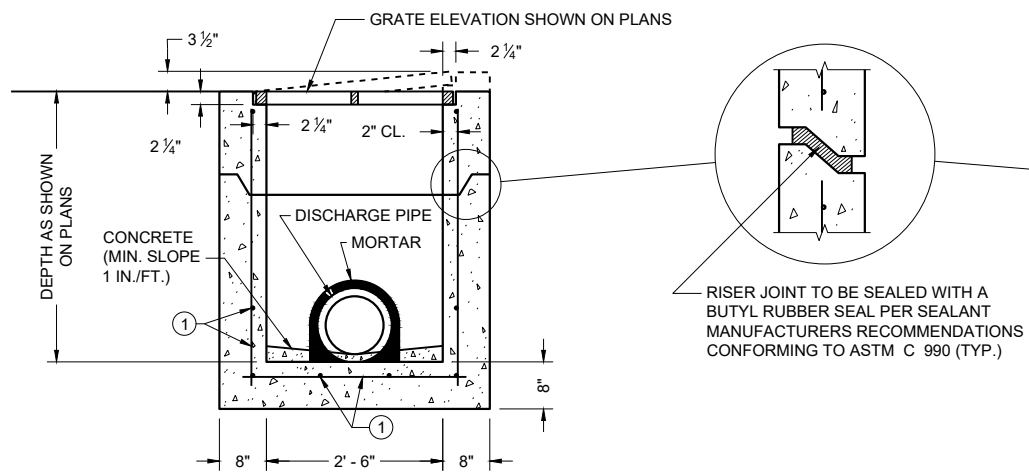
ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

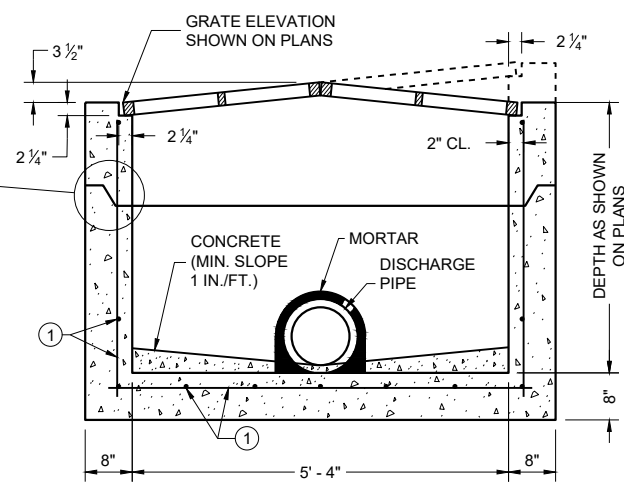
PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3" CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

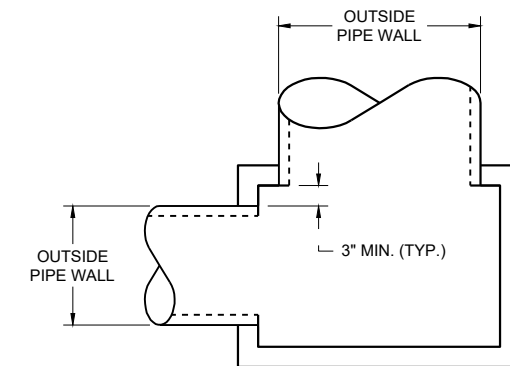
- ① FOR PRECAST INLETS AND REINFORCED CONCRETE BASES PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.
- ② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.



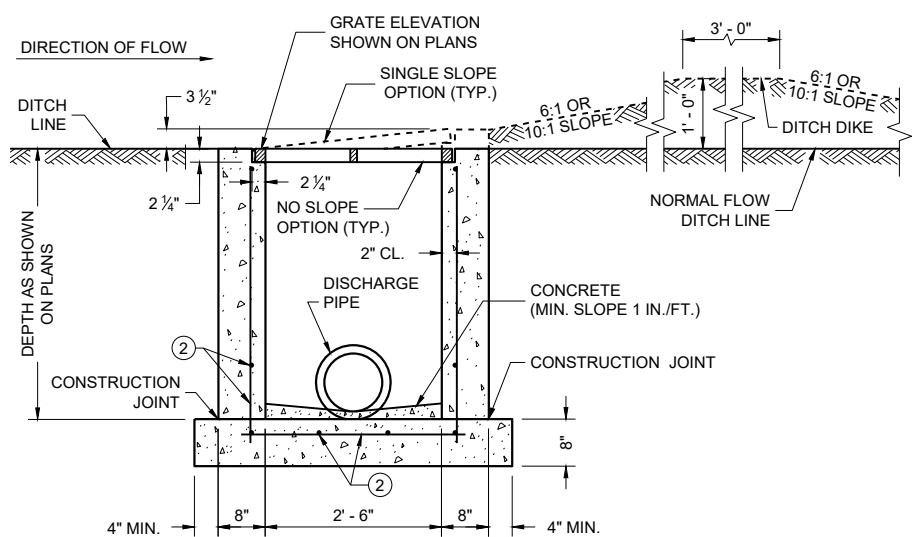
PRECAST REINFORCED CONCRETE SECTION A - A



PRECAST REINFORCED CONCRETE SECTION B - B

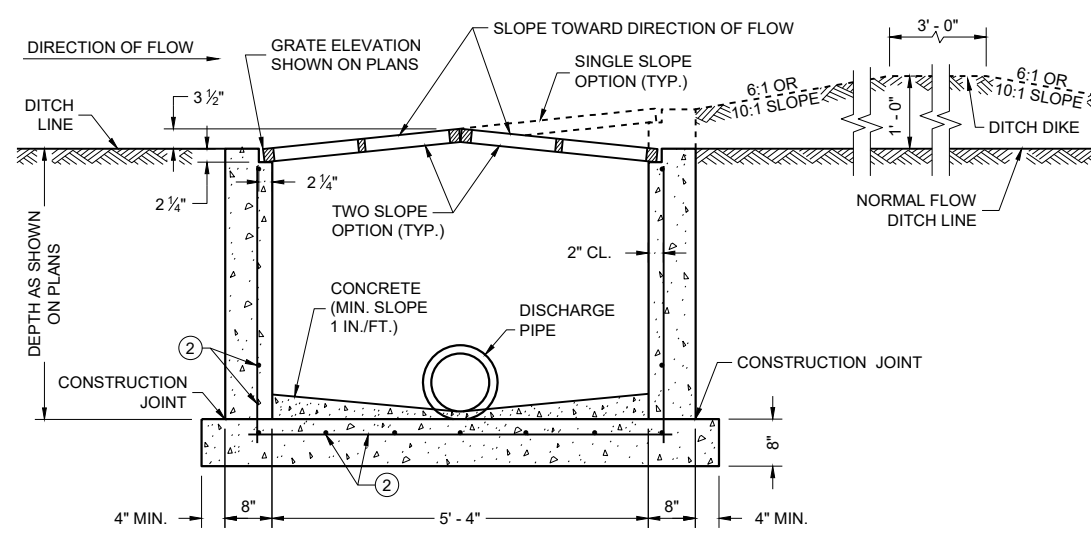


DETAIL "A"



REINFORCED CAST IN PLACE CONCRETE SECTION A - A

INLETS MEDIAN 1 GRATE



REINFORCED CAST IN PLACE CONCRETE SECTION B - B

INLETS MEDIAN 2 GRATE

PIPE MATRIX

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER	
	WIDTH (IN)	LENGTH (IN)
1 GRATE	18	18
2 GRATE	18	42

**INLETS
MEDIAN 1 AND 2 GRATE**

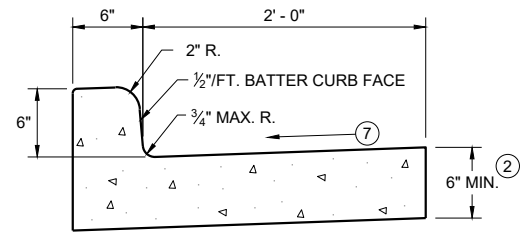
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

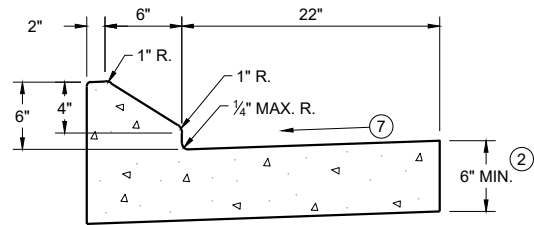
December 2023
DATE

/s/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR

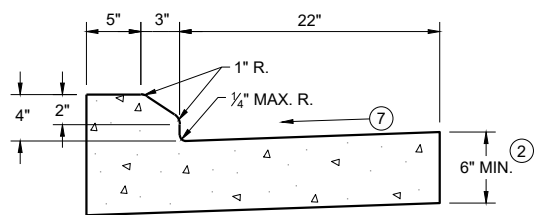
FHWA



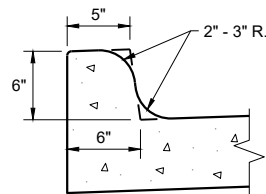
TYPES A^① & D



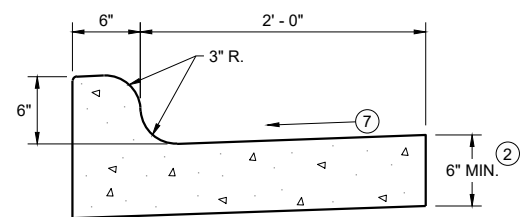
6" SLOPED CURB TYPES G^① & J



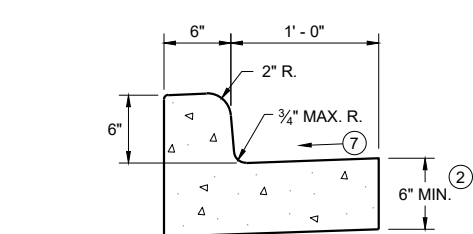
4" SLOPED CURB TYPES G^① & J



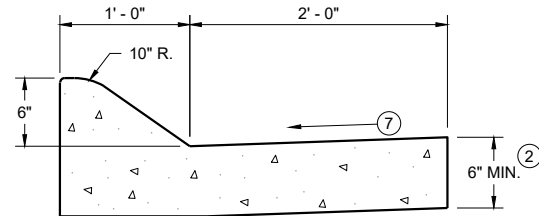
TYPES K^① & L
(OPTIONAL CURB SHAPE)



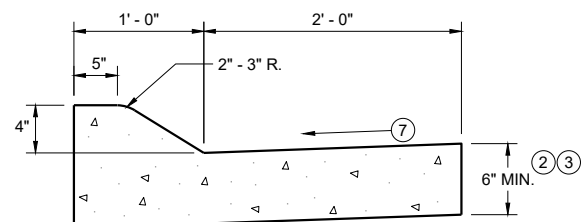
TYPES K^① & L
CONCRETE CURB AND GUTTER 30"



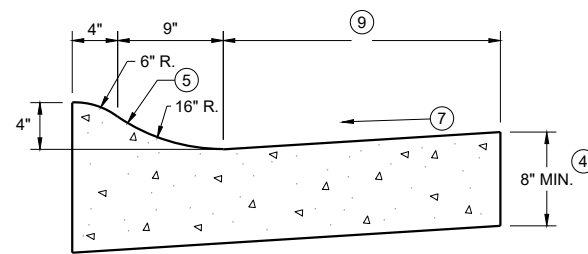
TYPES A^① & D
CONCRETE CURB AND GUTTER 18"



6" SLOPED CURB TYPES A^① & D

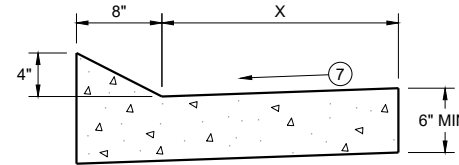


4" SLOPED CURB TYPES A^① & D
CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R^① & T

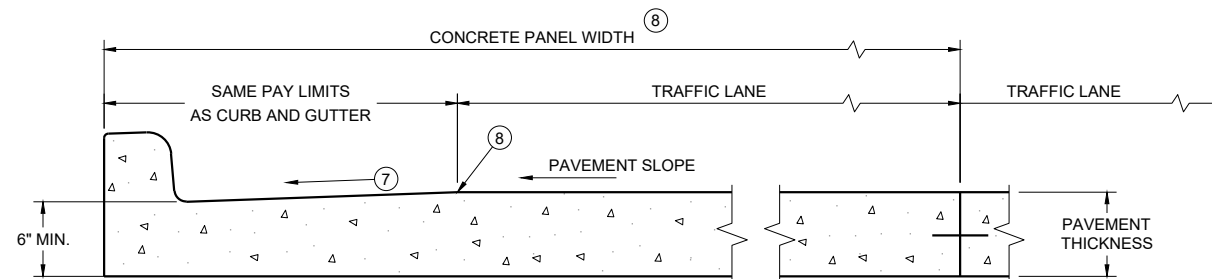
TBT & TBTT	X
30"	22"
36"	28"



TYPES TBT & TBTT^①
CONCRETE CURB AND GUTTER

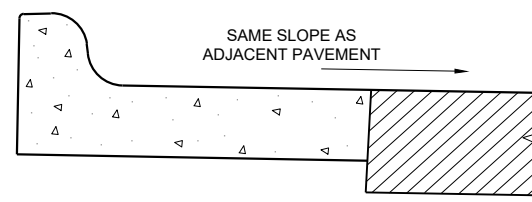
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER^⑥
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

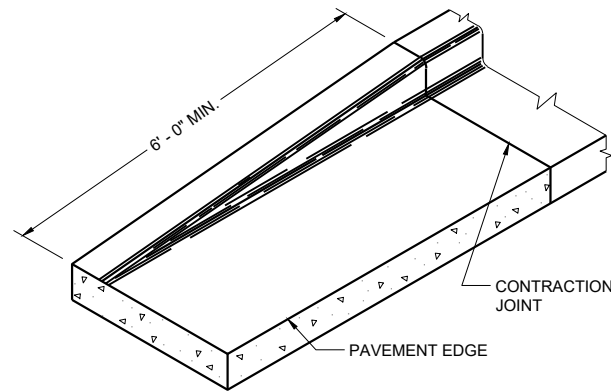
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

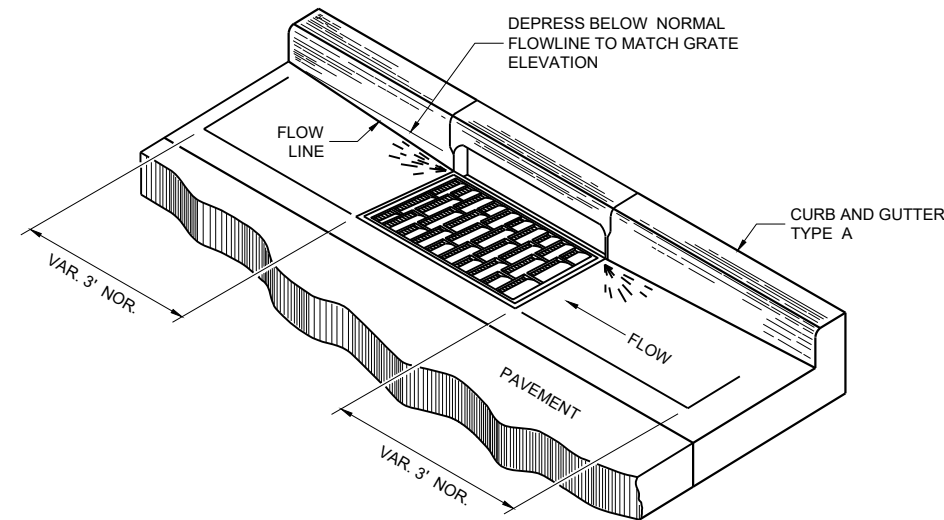
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWS.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES

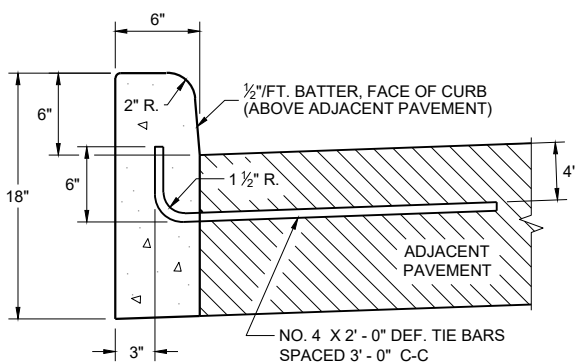


END SECTION CURB AND GUTTER

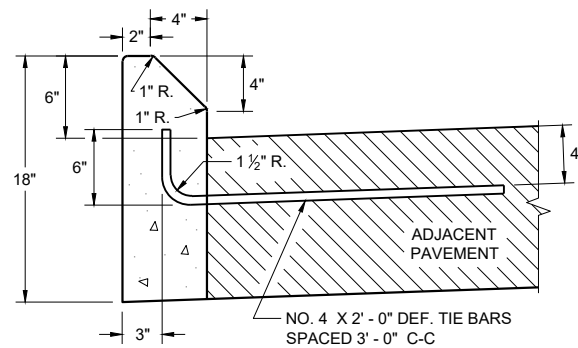


DETAIL OF CURB AND GUTTER AT INLETS

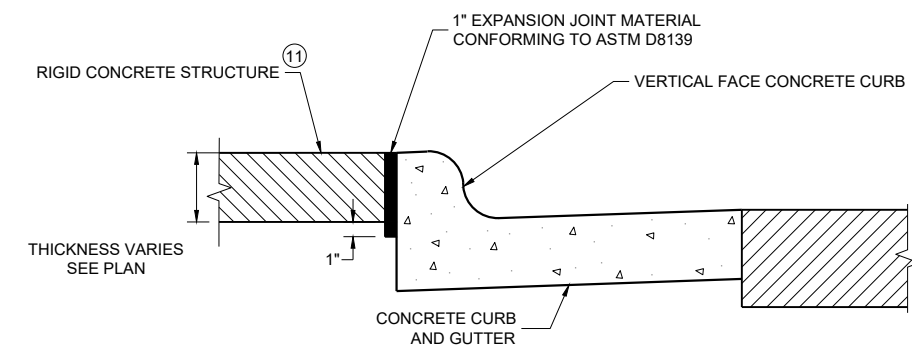
(TYPICAL H INLET COVER SHOWN)



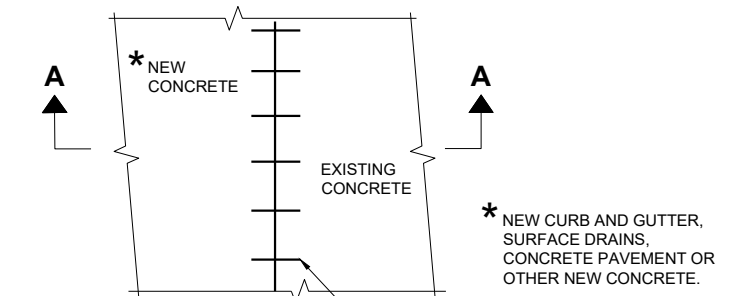
TYPES A^① & D



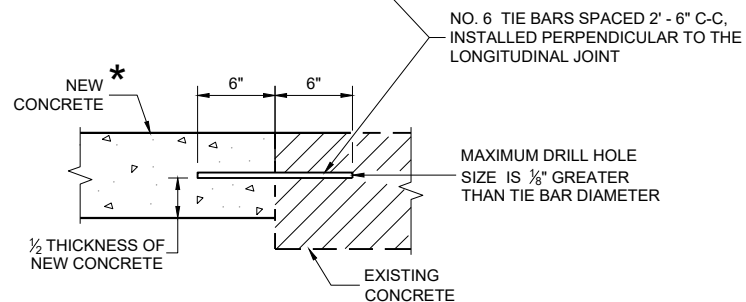
**TYPES G^① & J
CONCRETE CURB**



EXPANSION JOINT DETAIL FOR VERTICAL CURB ABUTTING A RIGID STRUCTURE^⑪



PLAN VIEW



**SECTION A - A
TIE BARS DRILLED INTO EXISTING PAVEMENT**

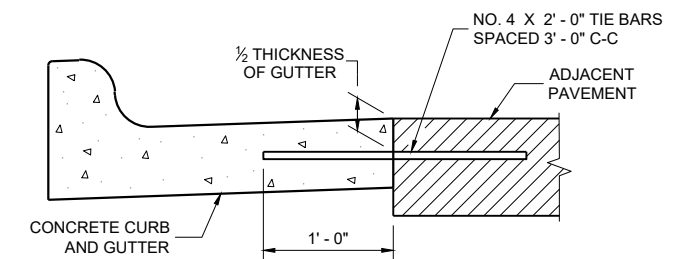
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

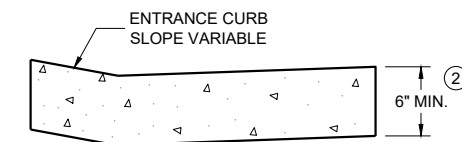
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑩ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.
- ⑪ PLACE 1" THICK EXPANSION JOINT MATERIAL BETWEEN VERTICAL FACE CURB TYPES EXTENDING FROM THE TOP OF CURB TO 1 INCH BELOW THE ADJOINING CONCRETE SURFACE. RIGID CONCRETE STRUCTURES INCLUDE RAISED CONCRETE MEDIANS, CONCRETE SAFETY ISLANDS, SPLITTER ISLANDS, OR LOCATIONS IDENTIFIED ON THE PLANS.



TYPICAL TIE BAR LOCATION^①



**DRIVEWAY ENTRANCE CURB^⑩
(WHEN DIRECTED BY THE ENGINEER)**

6

6

SDD 08D01-23b

SDD 08D01-23b

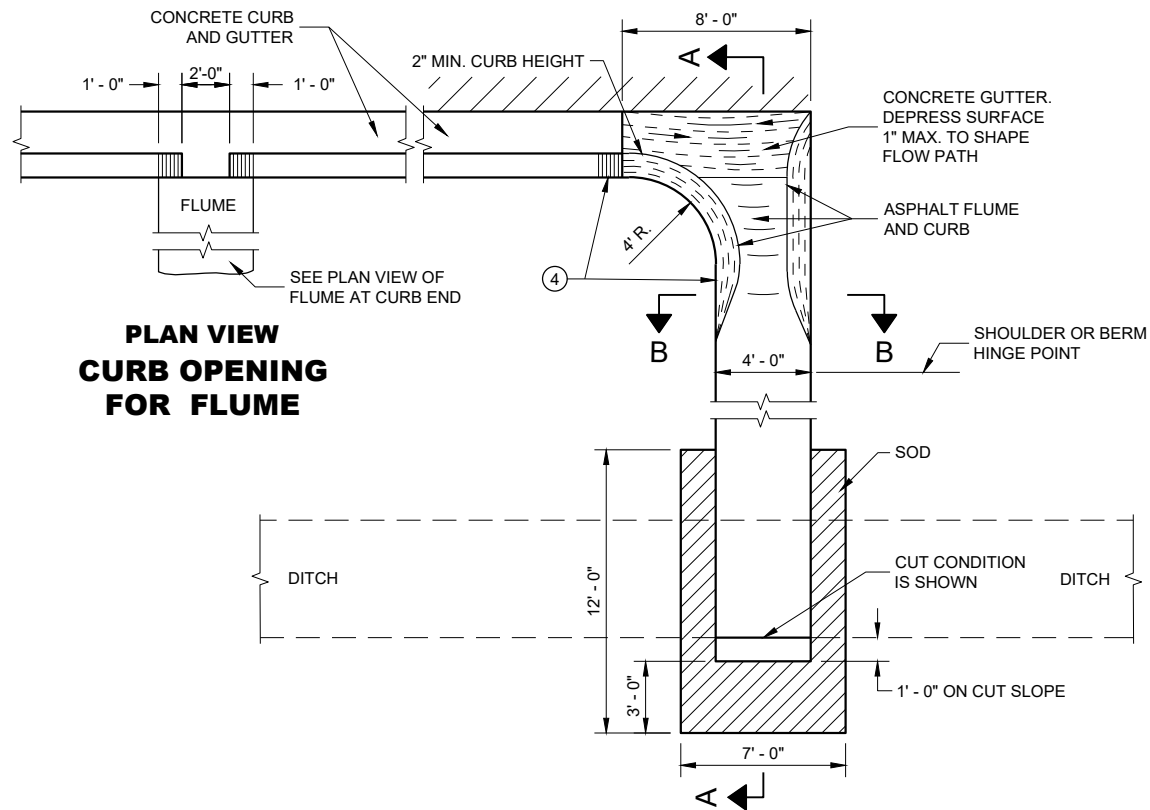
CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR

NOTE: TAPER CURB ENDS TO GUTTER IN 1' - 0"

ASPHALTIC FLUME



**PLAN VIEW
CURB OPENING
FOR FLUME**

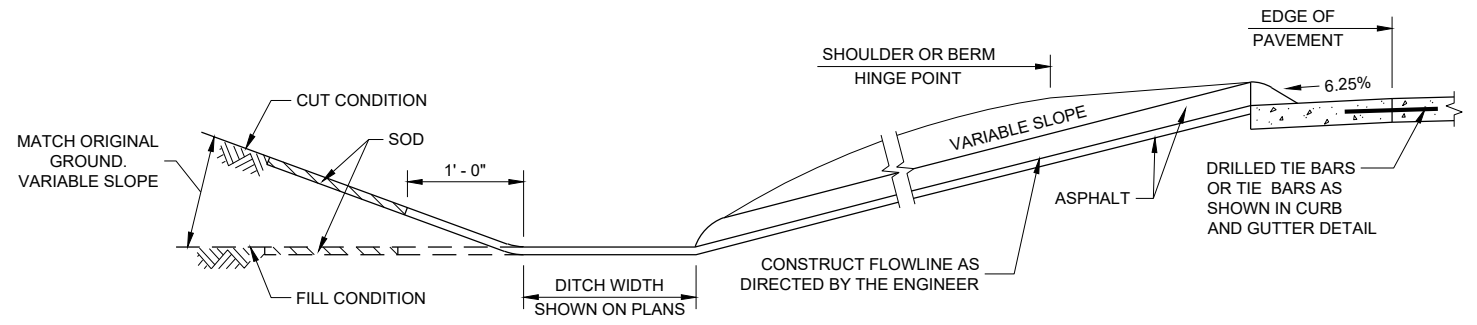
**PLAN VIEW
FLUME AT CURB END**

GENERAL NOTES

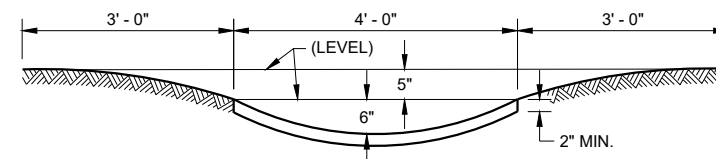
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

4" X 4" - W3.0 X W3.0 CONCRETE REINFORCEMENT SHALL BE IN ACCORDANCE WITH AASHTO SPECIFICATION M55.

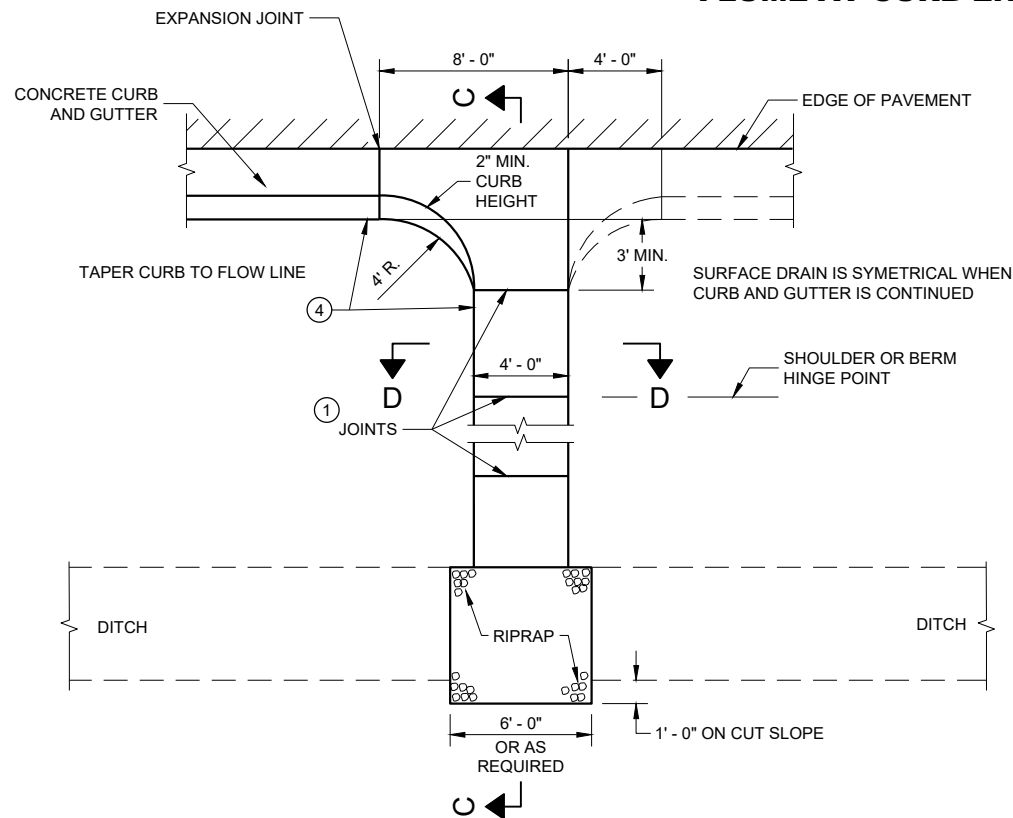
- ① JOINTS SHALL BE 1/8" TO 1/4" WIDE BY 1 1/2" DEEP AND SPACED AT UNIFORM INTERVALS OF APPROXIMATELY 4 FEET.
- ② GEOTEXTILE TYPE "R" SHALL UNDERLAY THE FULL LENGTH AND WIDTH OF THE CONCRETE SURFACE DRAIN AND RIPRAP.
- ③ CONCRETE SURFACE DRAIN WITHOUT CURB AND GUTTER MAY BE USED ON BACKSLOPES WHEN SPECIFIED.
- ④ ANGLE OF FLUME IN RELATION TO BACK OF CURB TO BE CONSTRUCTED PER THE PLAN DETAILS OR AS DIRECTED BY THE ENGINEER. ANGLE OF FLUME MAY BE OTHER THAN 90 DEGREES AS SHOWN.



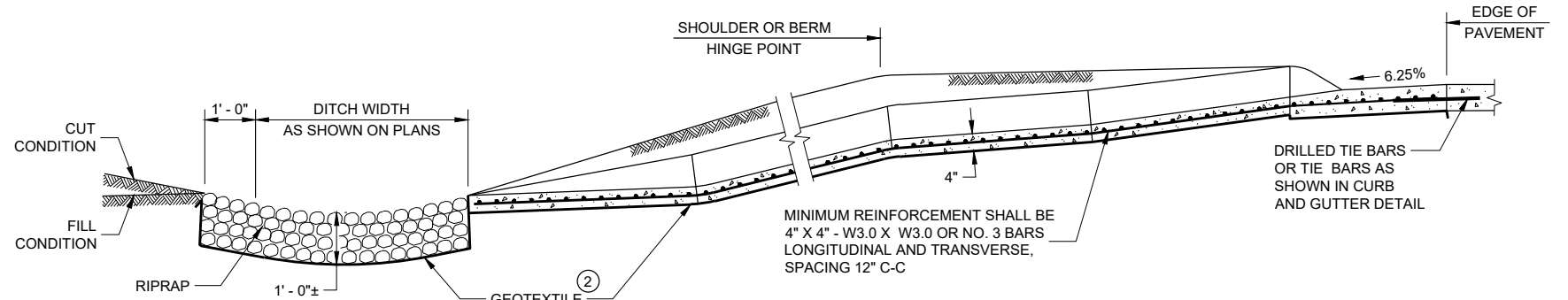
SECTION A - A



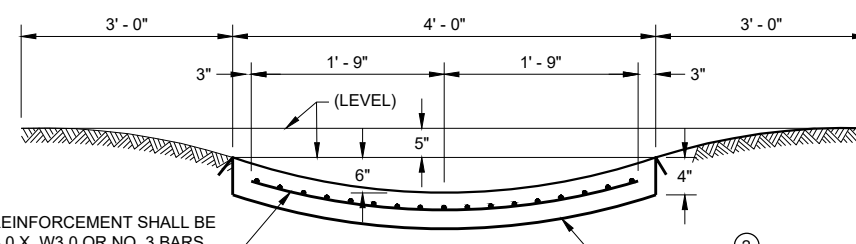
SECTION B - B



**PLAN VIEW
CONCRETE SURFACE DRAIN**



SECTION C - C



SECTION D - D

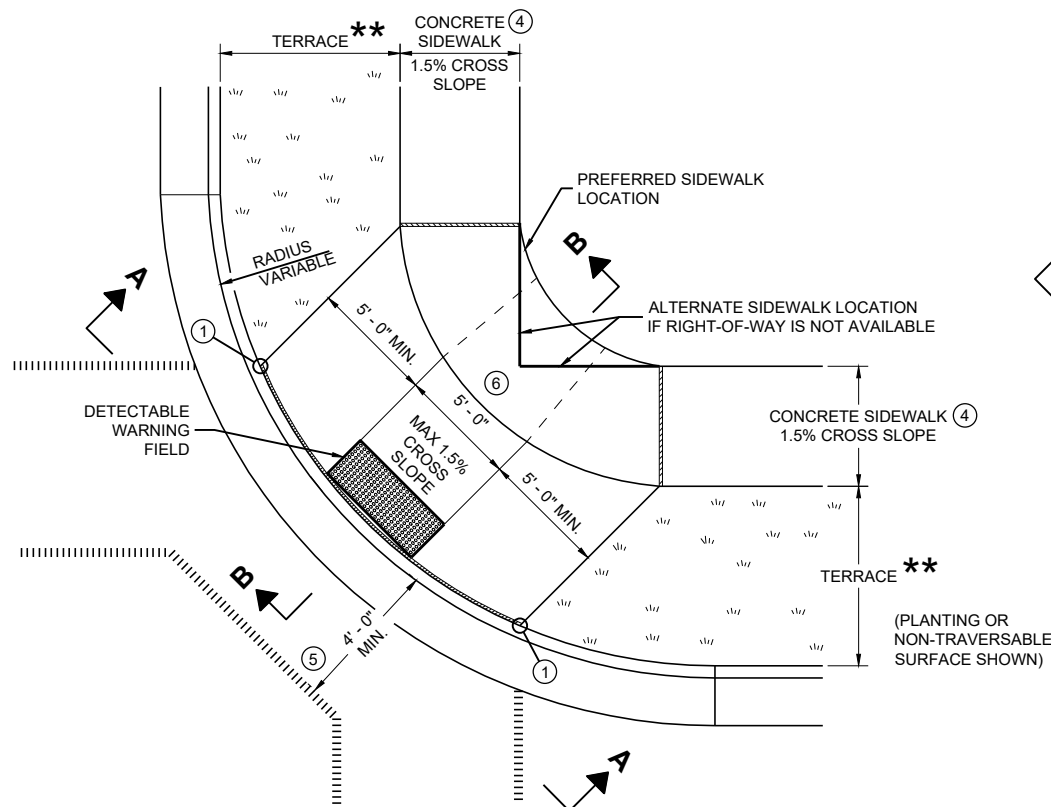
MINIMUM REINFORCEMENT SHALL BE 4" X 4" - W3.0 X W3.0 OR NO. 3 BARS LONGITUDINAL AND TRANSVERSE, SPACING 12" C-C

CONCRETE SURFACE DRAINS AND ASPHALTIC FLUMES

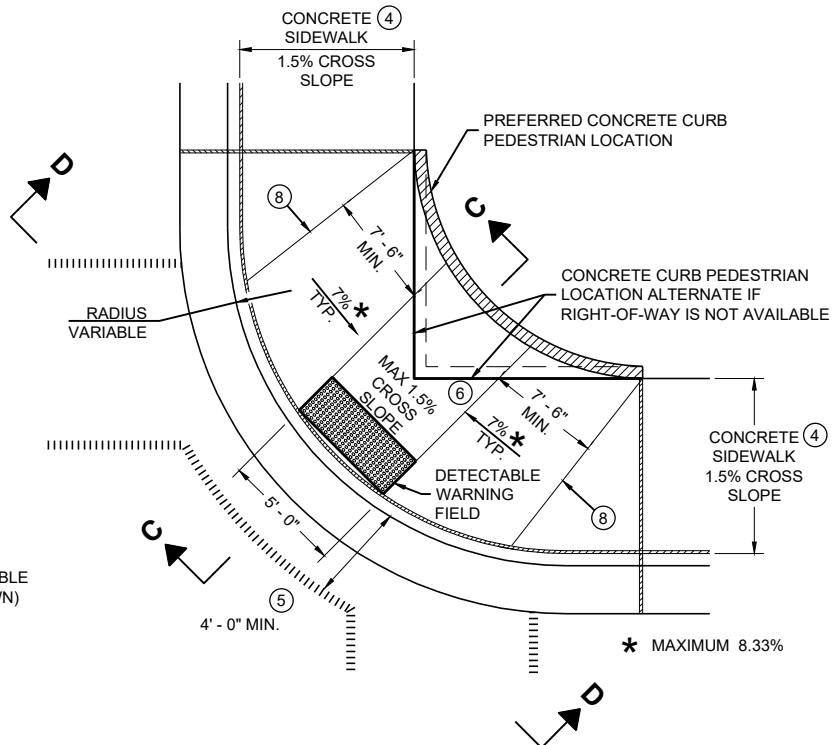
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

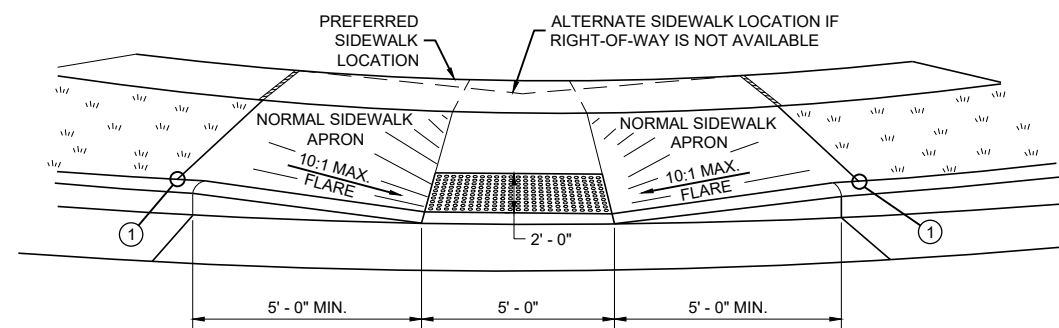
FHWA



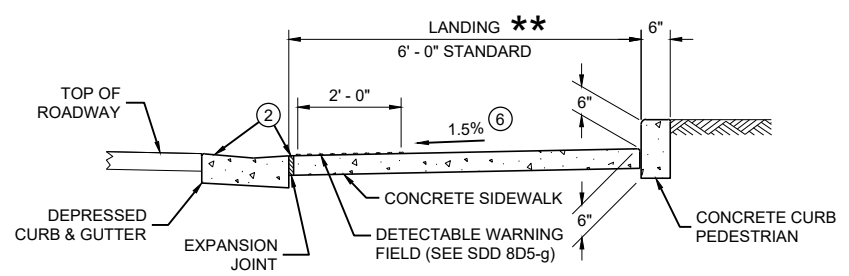
**PLAN VIEW
CURB RAMP TYPE 1
(CENTER OF CORNER RADIUS)**



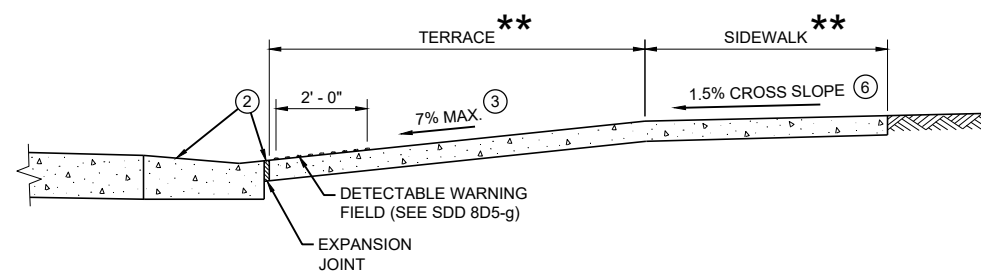
**PLAN VIEW
CURB RAMP TYPE 1 - A
(NO TERRACE)**



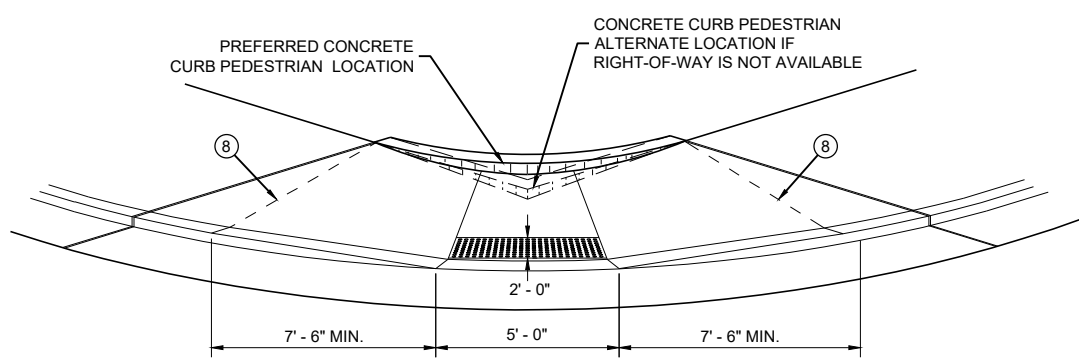
VIEW A - A FOR TYPE 1



SECTION C - C FOR TYPE 1 - A



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.
- TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.
- DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.
- SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA. 4 FOOT WIDTH IS MEASURED FROM THE FLANGE LINE
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 1 AND 1-A**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

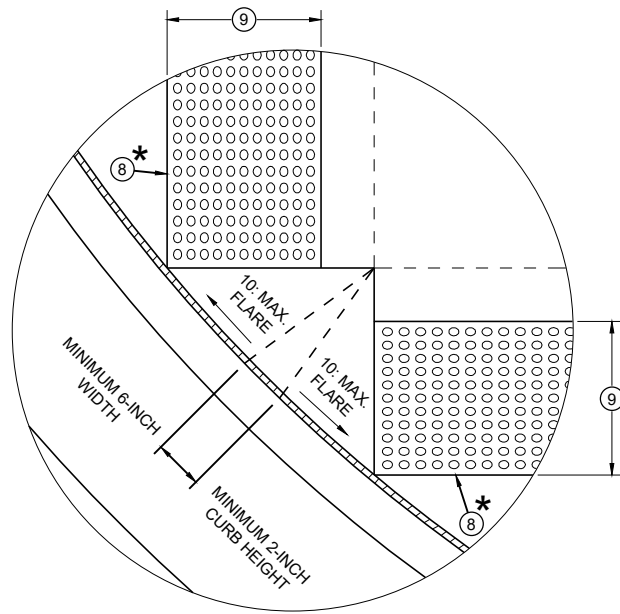
6

6

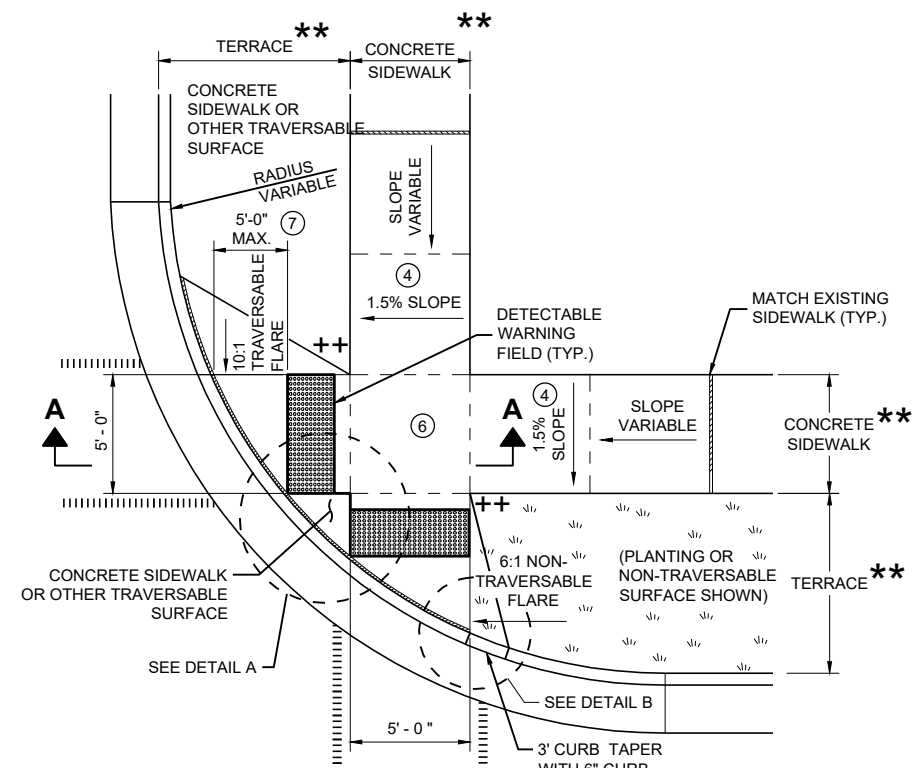
SDD 08D05-21a

SDD 08D05-21a

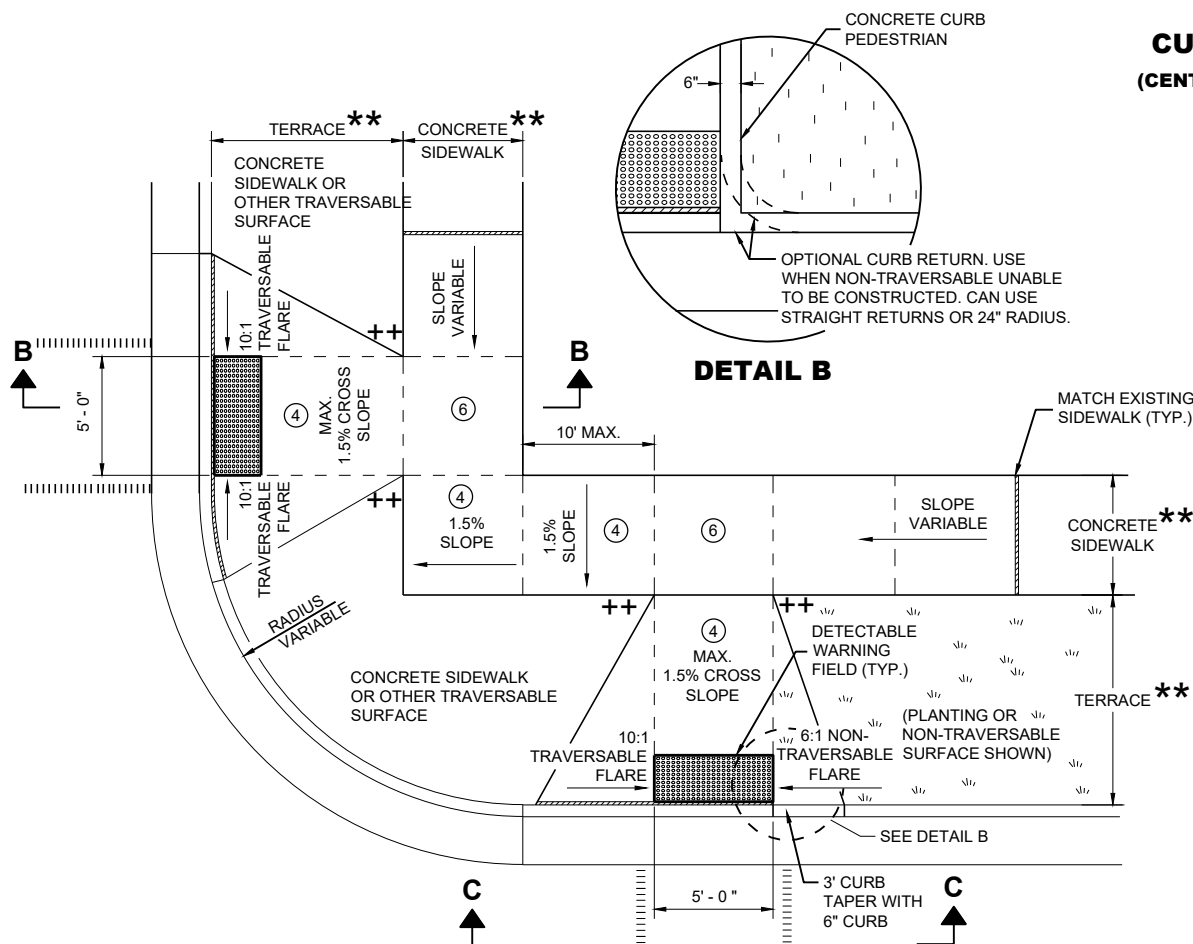
** WIDTH SHOWN ELSEWHERE IN THE PLANS



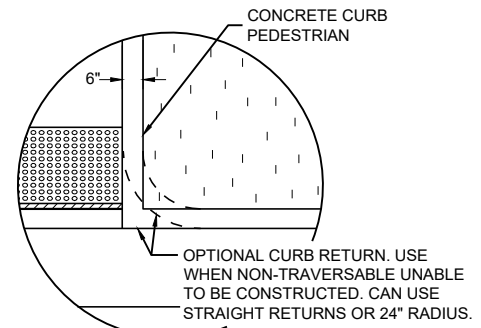
DETAIL A



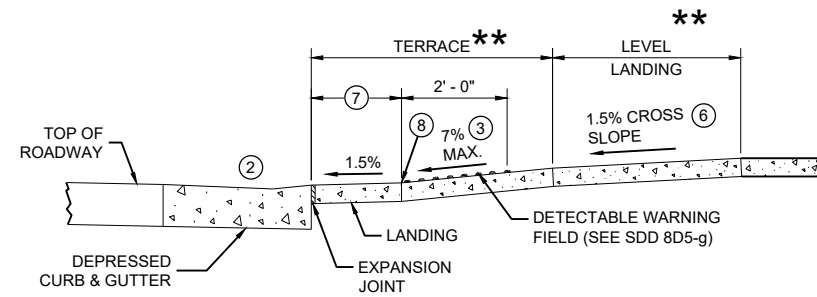
**PLAN VIEW
CURB RAMP TYPE 2
(CENTER OF CORNER RADIUS)**



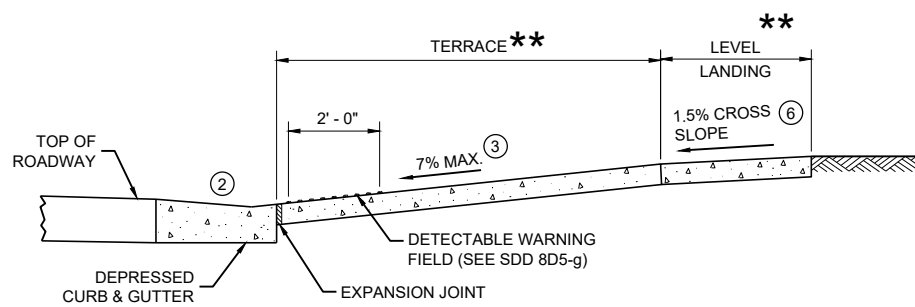
**PLAN VIEW
CURB RAMP TYPE 3
(OUTSIDE OF CROSSWALK AREA)**



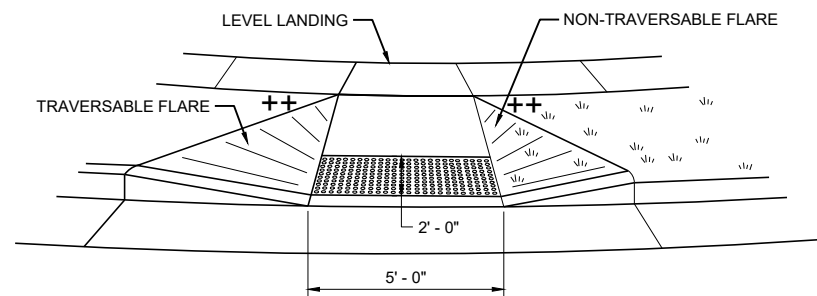
DETAIL B



SECTION A - A FOR TYPE 2



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

- * MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS
- ++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - - CONTRACTION JOINT SIDEWALK
- |||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 2 AND 3**

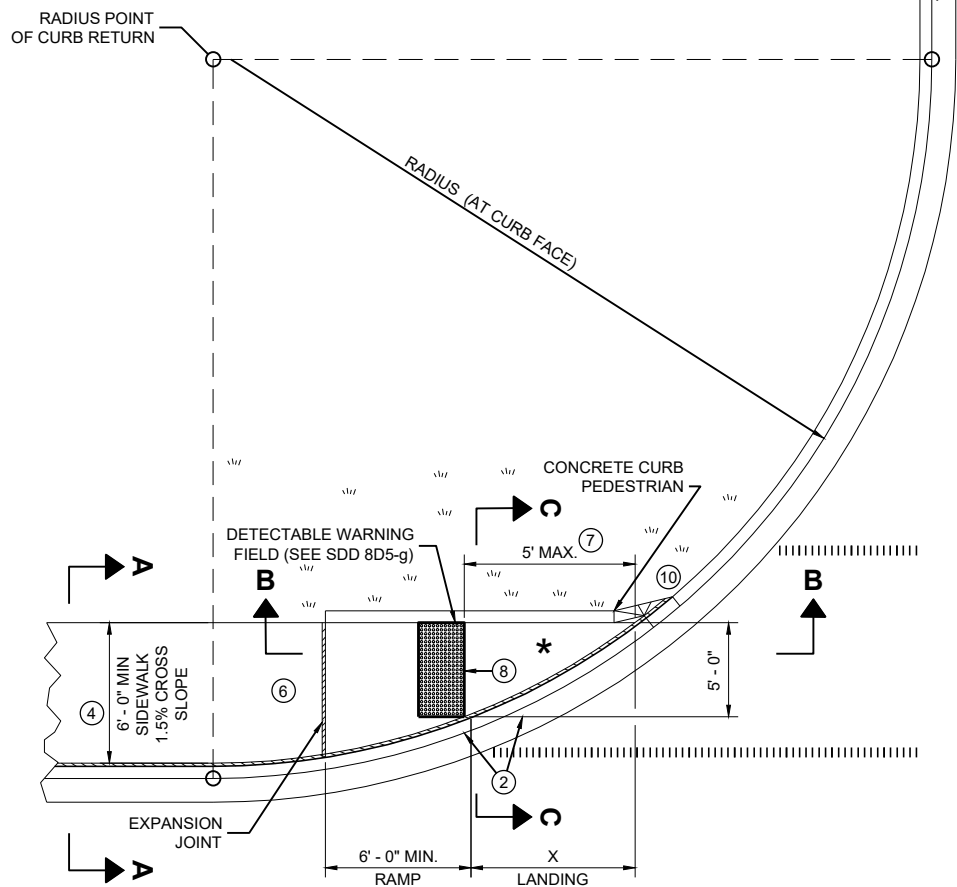
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

6

SDD 08D05-21b

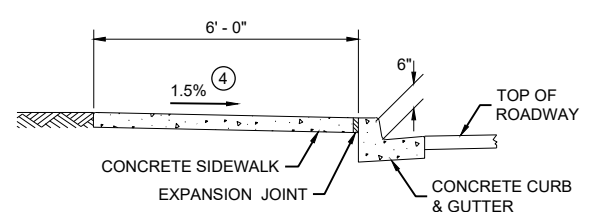
SDD 08D05-21b



**PLAN VIEW
CURB RAMP TYPE 4A**

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"

INTERMEDIATE RADII CAN BE INTERPOLATED



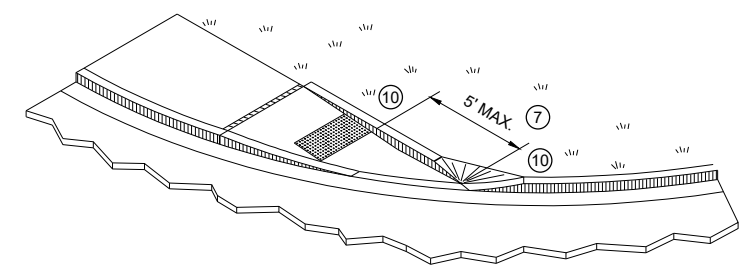
SECTION A - A FOR TYPE 4A

GENERAL NOTES

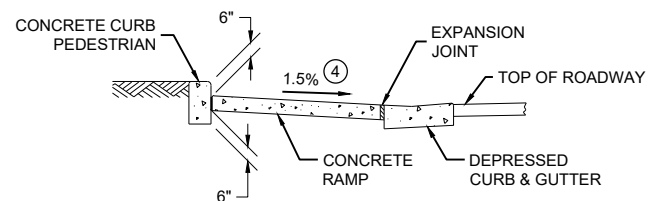
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

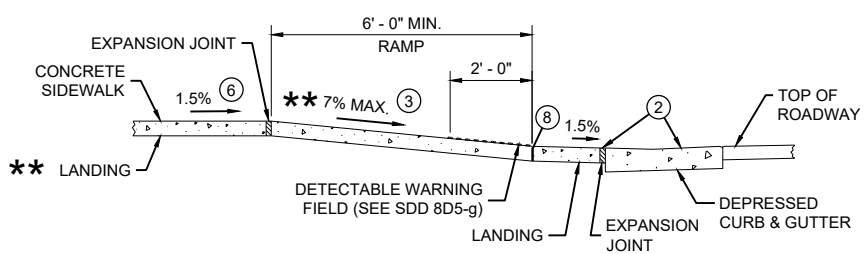


ISOMETRIC VIEW FOR TYPE 4A



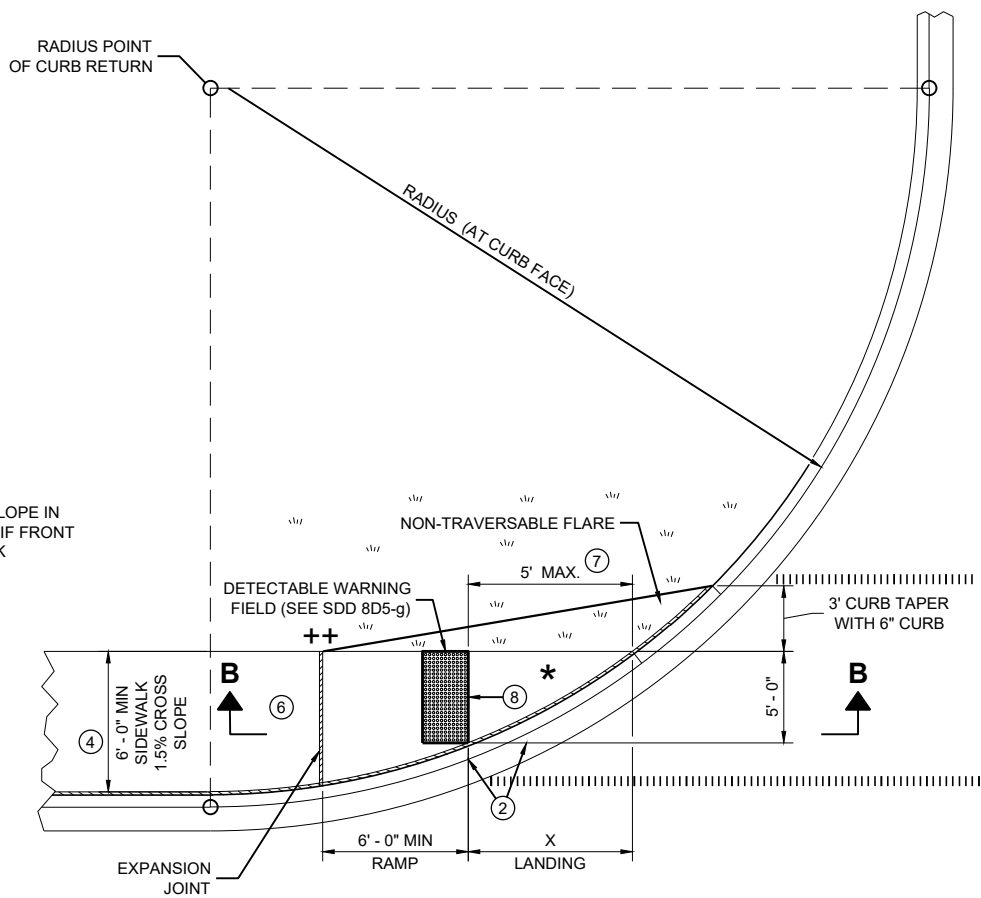
SECTION C - C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK



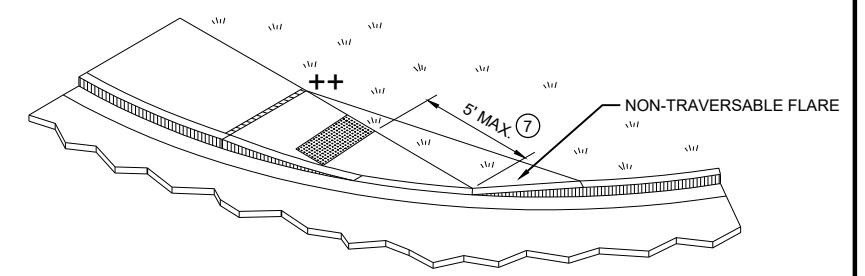
**SECTION B - B FOR
TYPE 4A AND TYPE 4A1**

** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



**PLAN VIEW
CURB RAMP TYPE 4A1**

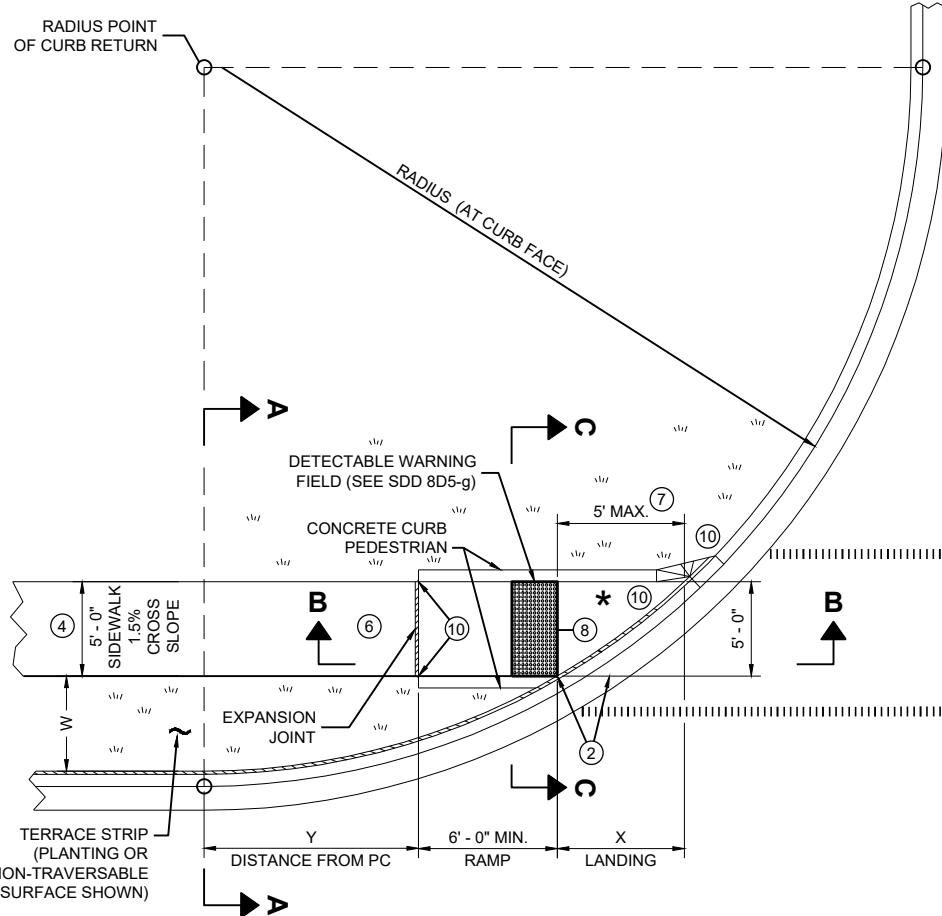
++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



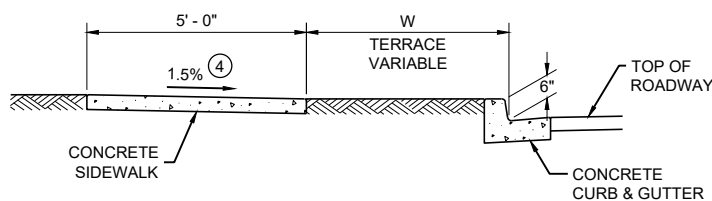
ISOMETRIC VIEW FOR TYPE 4A1

**CURB RAMPS
TYPE 4A AND 4A1**

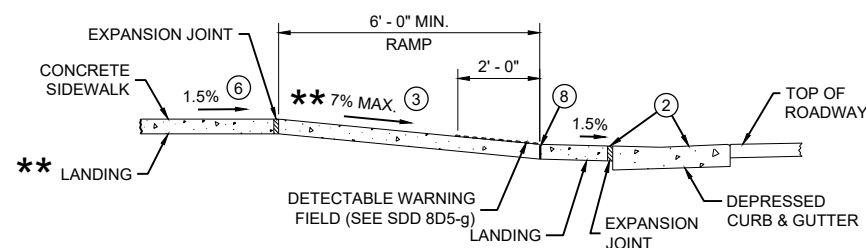
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**PLAN VIEW
CURB RAMP TYPE 4B**



SECTION A - A FOR TYPE 4B

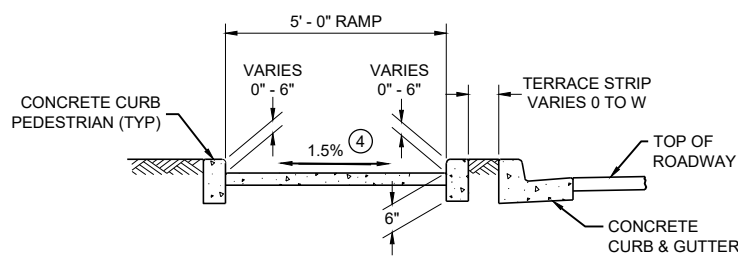


**SECTION B - B FOR
TYPE 4B AND TYPE 4B1**

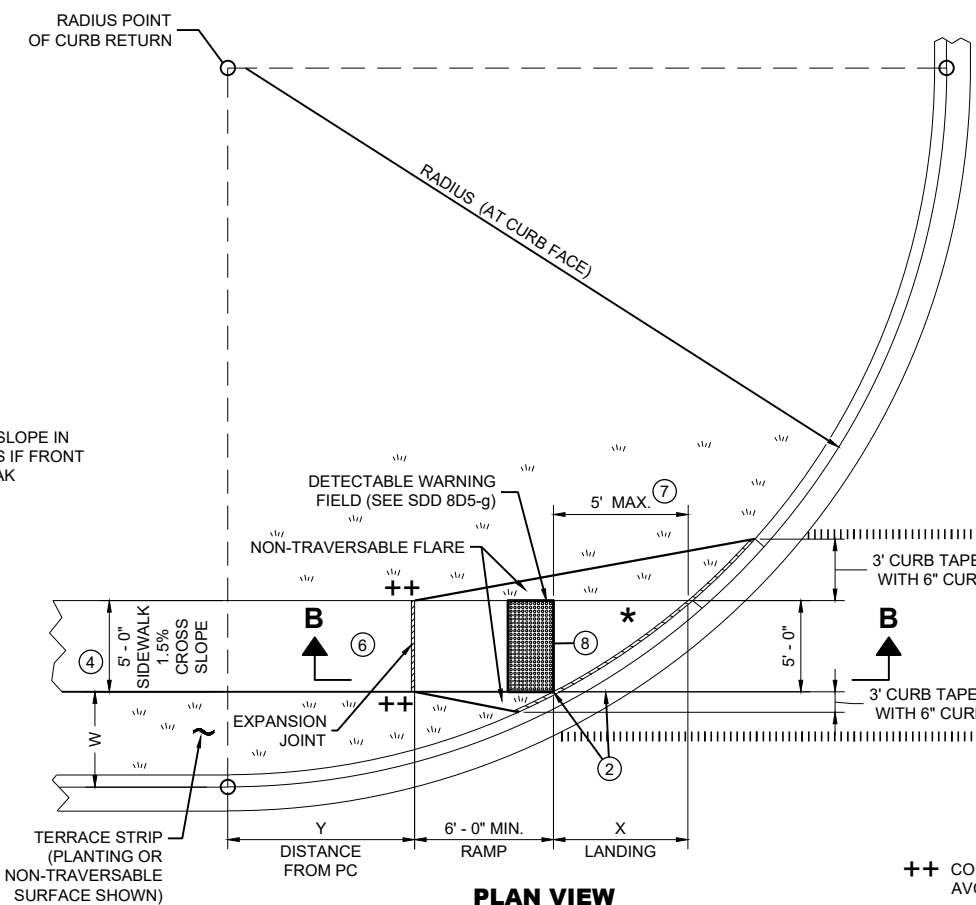
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET			4' - 11 1/2"	5' - 1 3/4"	4' - 3 1/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET									4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET															4' - 10 3/4"	19' - 8 1/4"

INTERMEDIATE RADII CAN BE INTERPOLATED
DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH

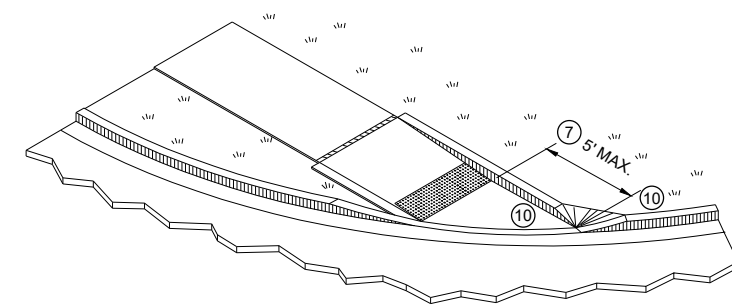


SECTION C - C FOR TYPE 4B

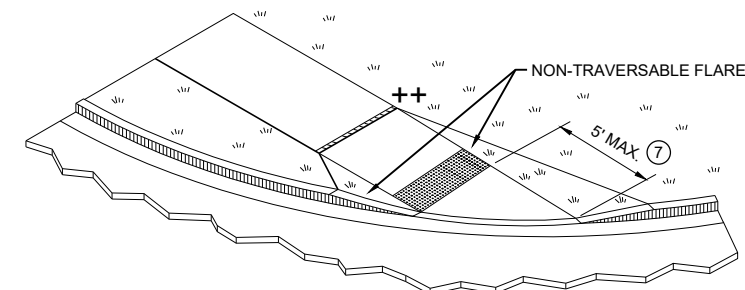


**PLAN VIEW
CURB RAMP TYPE 4B1**

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



ISOMETRIC VIEW FOR TYPE 4B



ISOMETRIC VIEW FOR TYPE 4B1

LEGEND

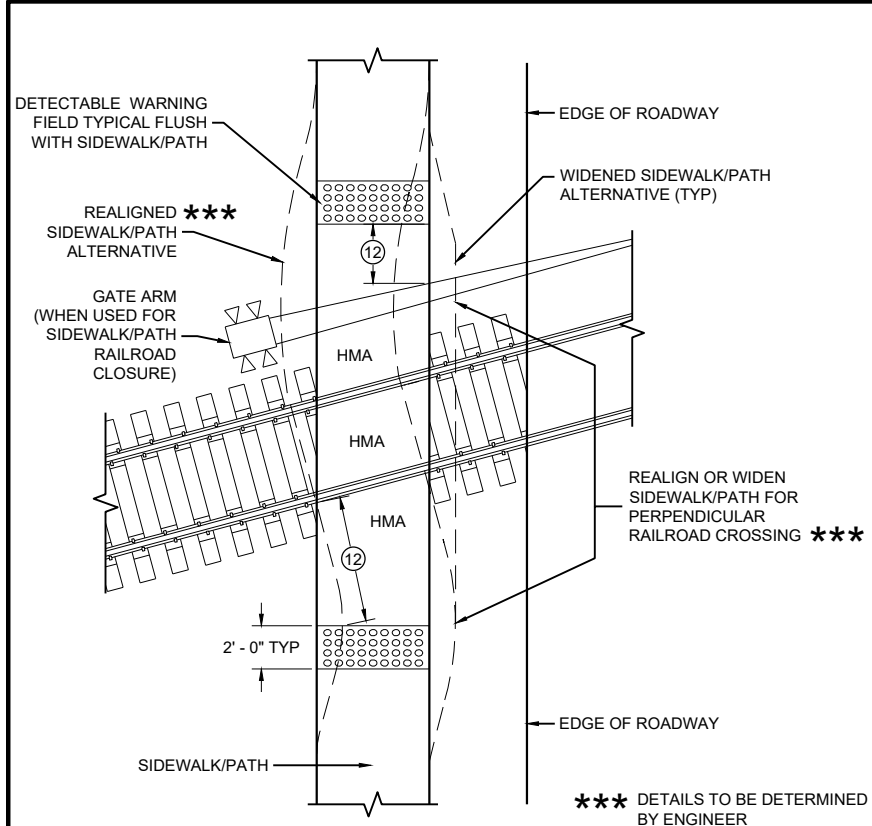
- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- (2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- (3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- (4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- (6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- (7) WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- (8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- (10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

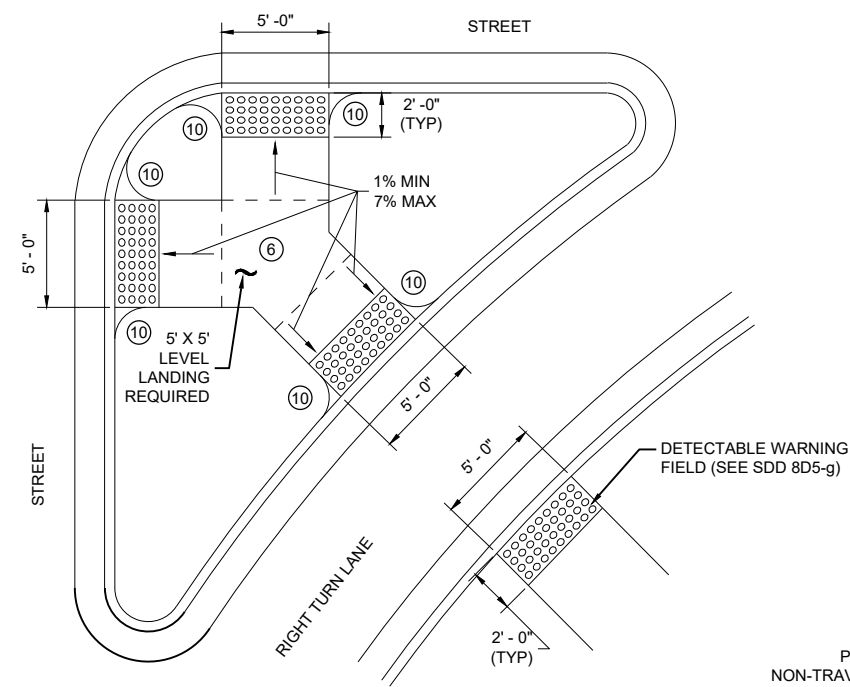
**CURB RAMPS
TYPE 4B AND 4B1**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



CURB RAMP TYPE 8

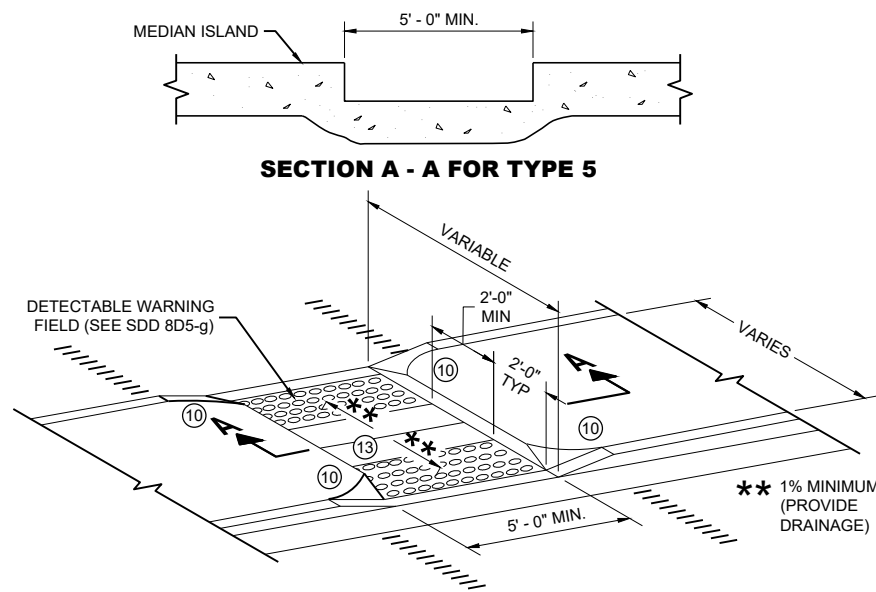
DETECTABLE WARNINGS FOR SIDEWALKS OR SHARED USE PATHS AT RAILROAD CROSSINGS



CURB RAMP TYPE 6

DETECTABLE WARNING AT ISLANDS

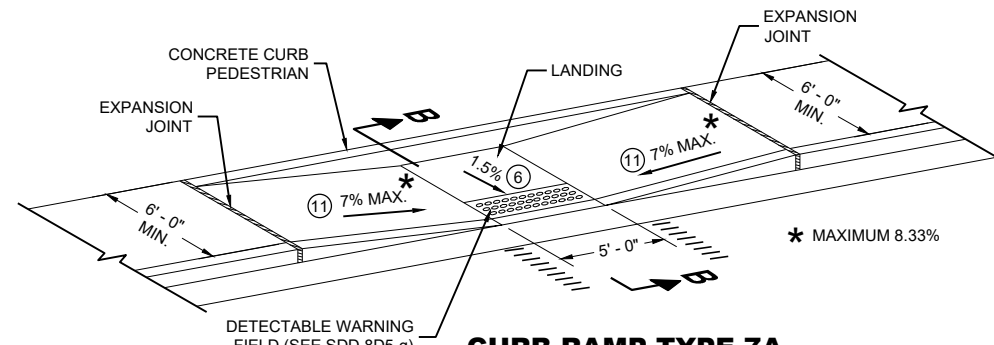
REFER TO GENERAL NOTES ② AND ③ FOR ALL ISLAND CURB RAMPS



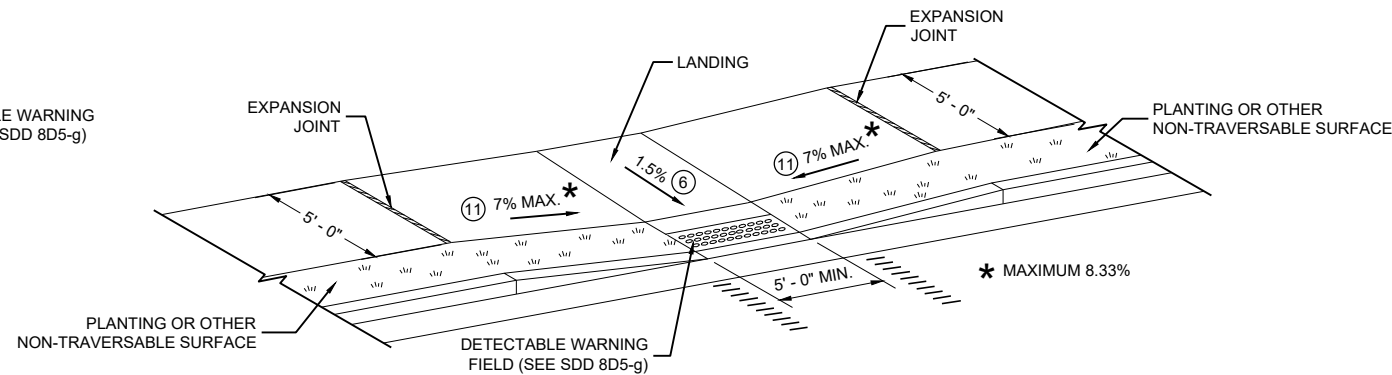
SECTION A - A FOR TYPE 5

CURB RAMP TYPE 5

MEDIAN ISLAND NON-ELEVATED PEDESTRIAN CROSSING



CURB RAMP TYPE 7A FOR INTERSECTIONS AND MID BLOCK CROSSINGS



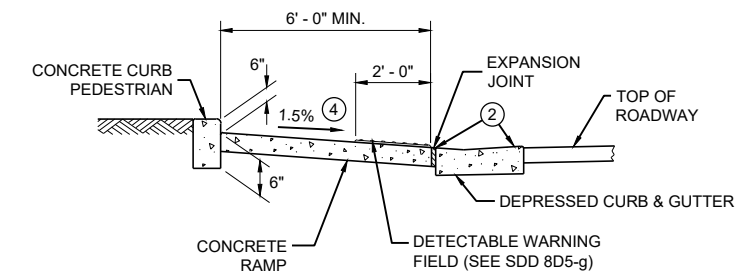
CURB RAMP TYPE 7B FOR INTERSECTIONS AND MID BLOCK CROSSINGS

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- ⑪ SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- ⑫ THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK/PATH. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD TRACK IS 15 FEET MAXIMUM AND 12 FEET MINIMUM, 15 FEET TYPICAL FROM THE NEAREST RAIL.
- ⑬ DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

LEGEND

- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

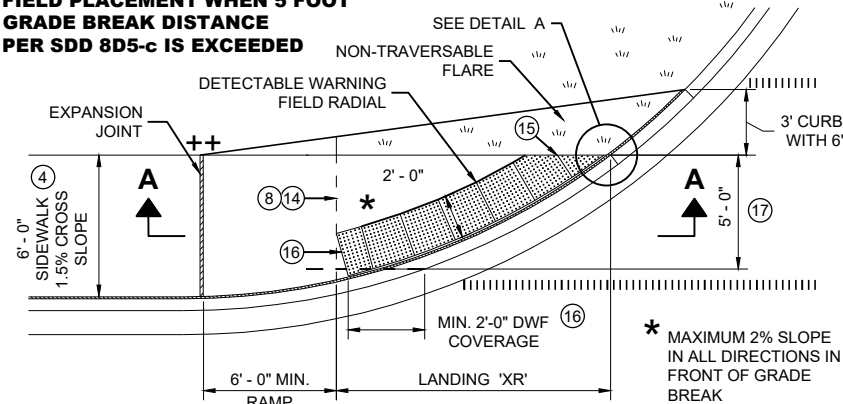


SECTION B - B FOR TYPE 7A

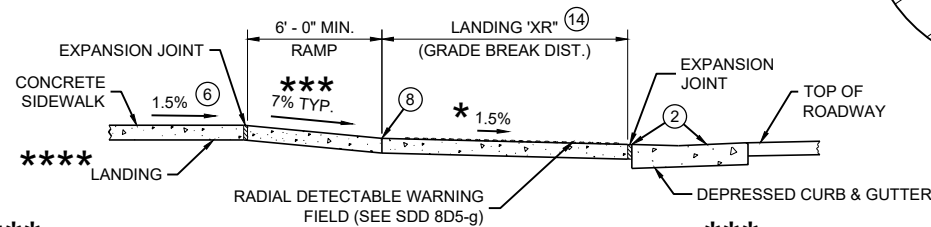
CURB RAMPS TYPE 5, 6, 7A, 7B & 8

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

**RADIAL DETECTABLE WARNING
FIELD PLACEMENT WHEN 5 FOOT
GRADE BREAK DISTANCE
PER SDD 8D5-c IS EXCEEDED**



**PLAN VIEW
CURB RAMP TYPE 4A1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**



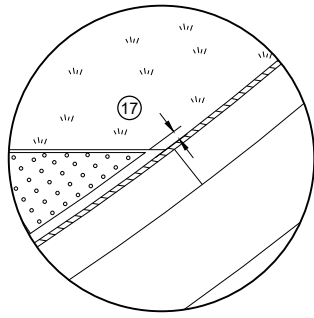
SECTION A - A FOR TYPE 4A1

**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

LEGEND

- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT SIDEWALK
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

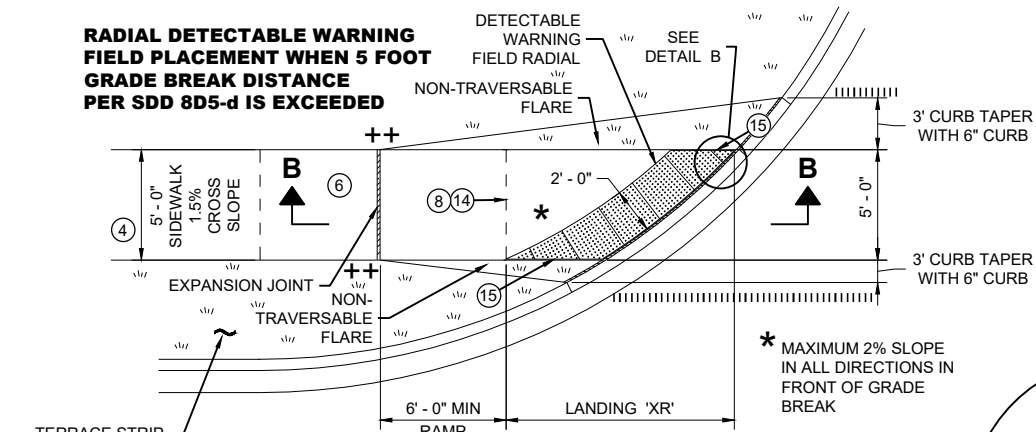


DETAIL A

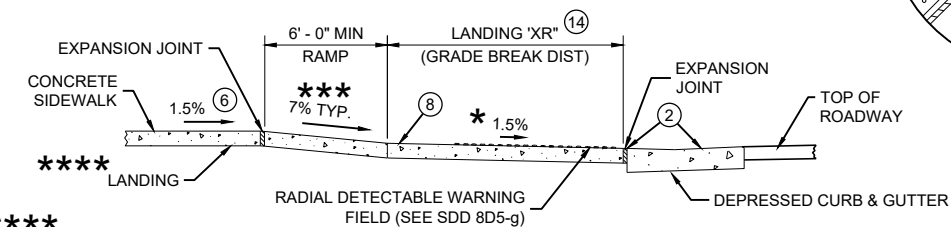
GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B CURB RAMPS ARE NOT SHOWN.
- REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- (2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- (3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- (4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- (6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- (8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- (14) CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- (15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/2" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- (16) USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- (17) A MAXIMUM 3 INCH CONCRETE BORDER WIDTH IS ALLOWABLE IN FRONT OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

**RADIAL DETECTABLE WARNING
FIELD PLACEMENT WHEN 5 FOOT
GRADE BREAK DISTANCE
PER SDD 8D5-d IS EXCEEDED**



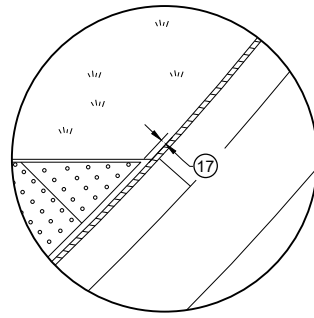
**PLAN VIEW
CURB RAMP TYPE 4B1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**



SECTION B - B FOR TYPE 4B1

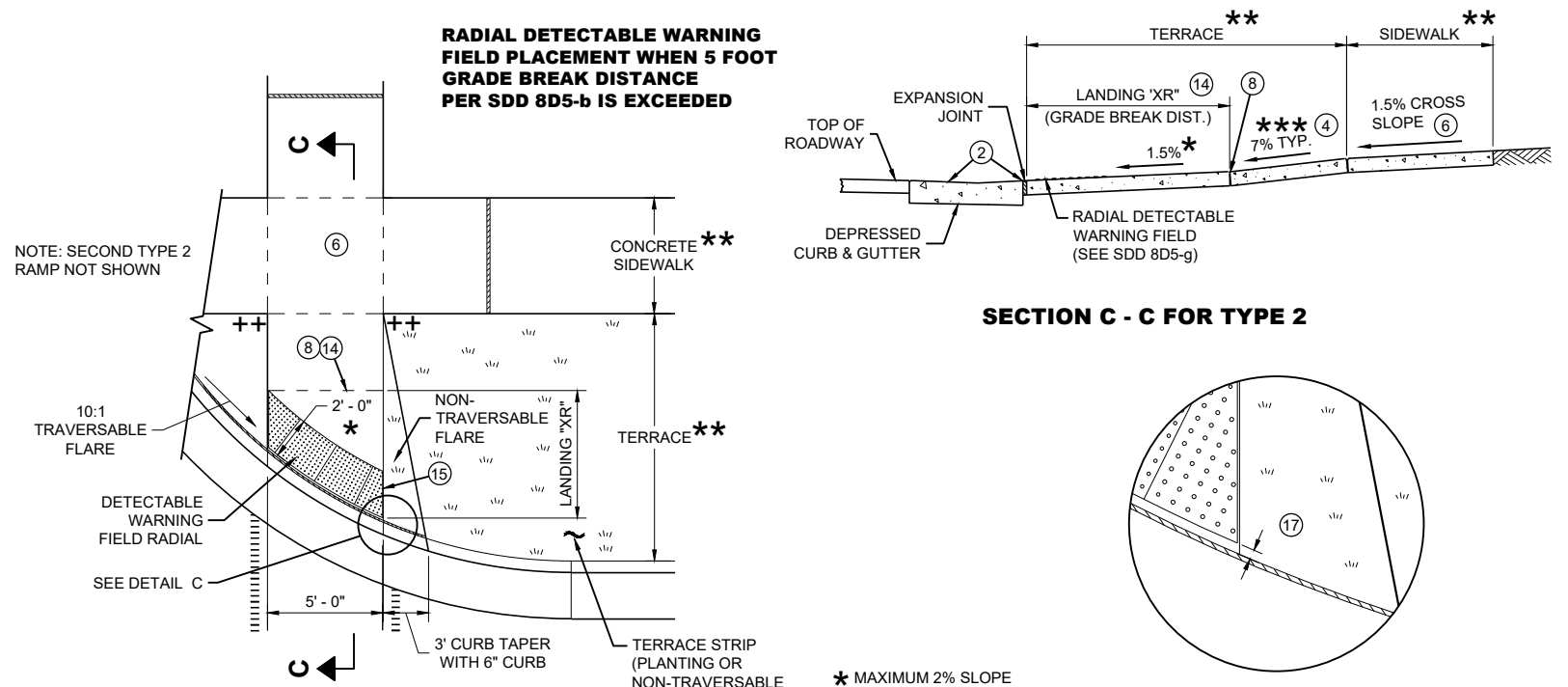
**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%



DETAIL B

**RADIAL DETECTABLE WARNING
FIELD PLACEMENT WHEN 5 FOOT
GRADE BREAK DISTANCE
PER SDD 8D5-b IS EXCEEDED**



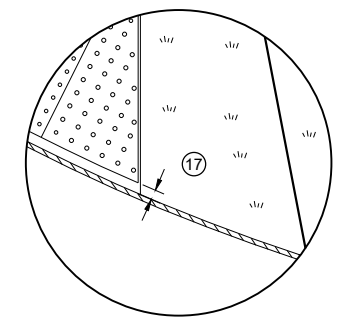
**PLAN VIEW
CURB RAMP TYPE 2
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)
(ON LINE WITH SIDEWALK)**

* MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

** WIDTH SHOWN ELSEWHERE IN THE PLANS

*** MAXIMUM 8.33%

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



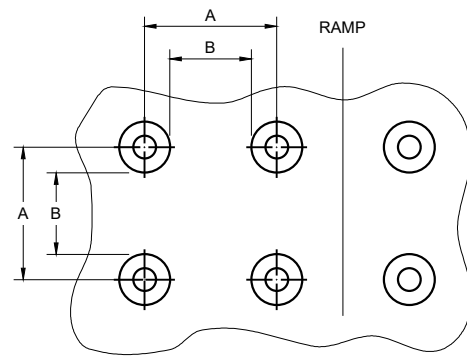
DETAIL C

**CURB RAMPS
RADIAL DETECTABLE WARNING**

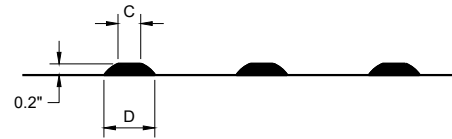
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

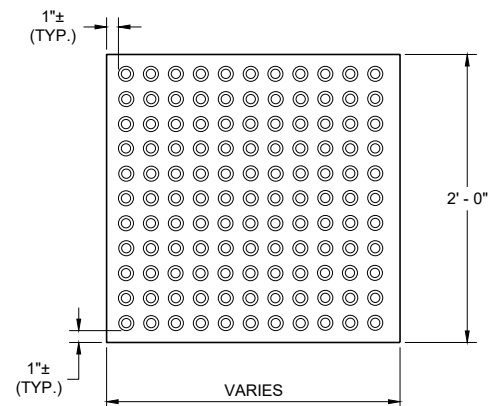


PLAN VIEW

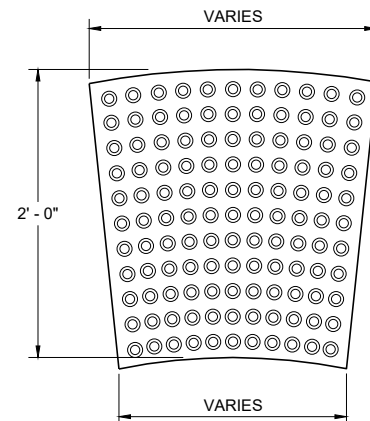


ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**

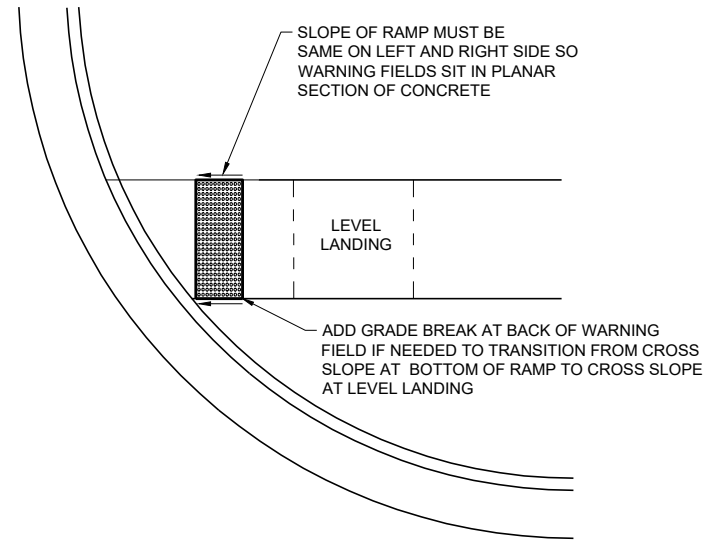


**RECTANGULAR
PLATES**



**RADIAL
PLATES**

**PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)**

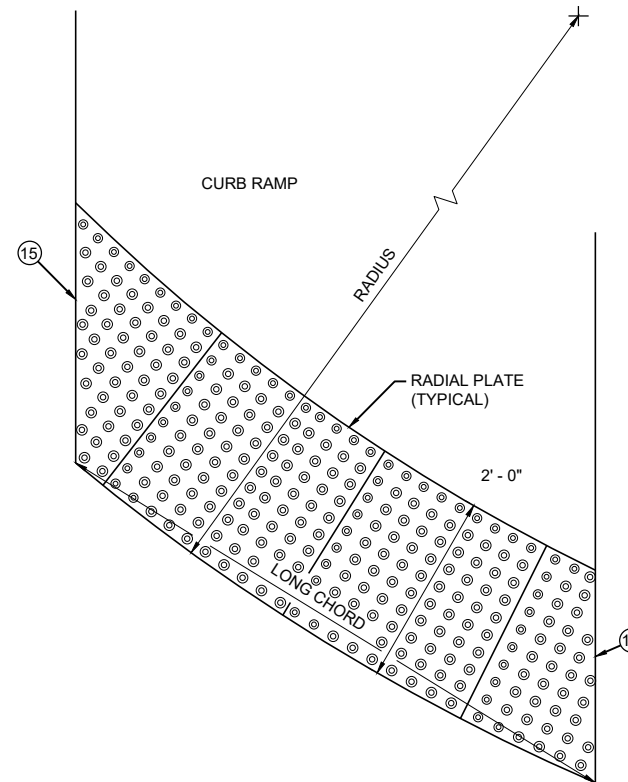


**DETECTABLE WARNING FIELD
PLANAR INSTALLATION**

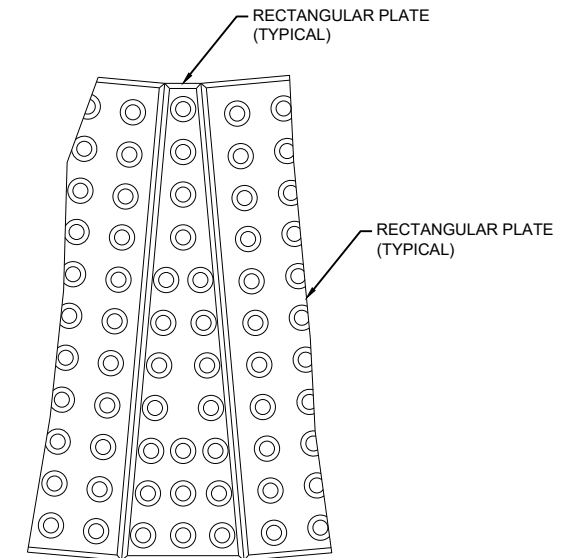
GENERAL NOTES

- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.
- PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.
- REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.
- DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.



**PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES**

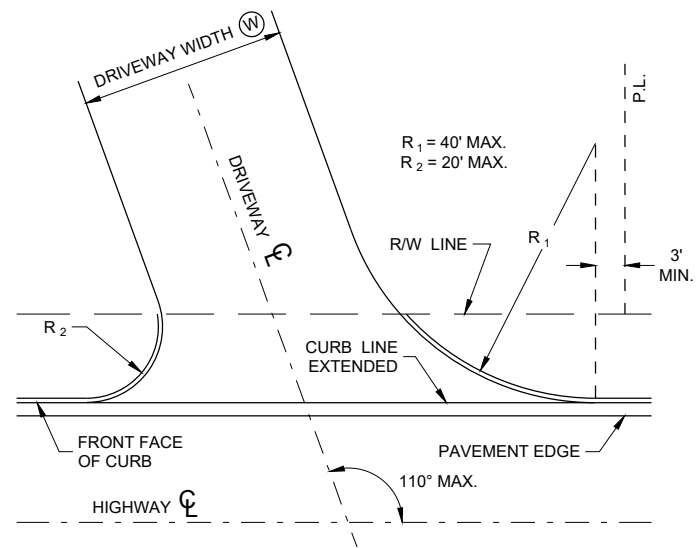
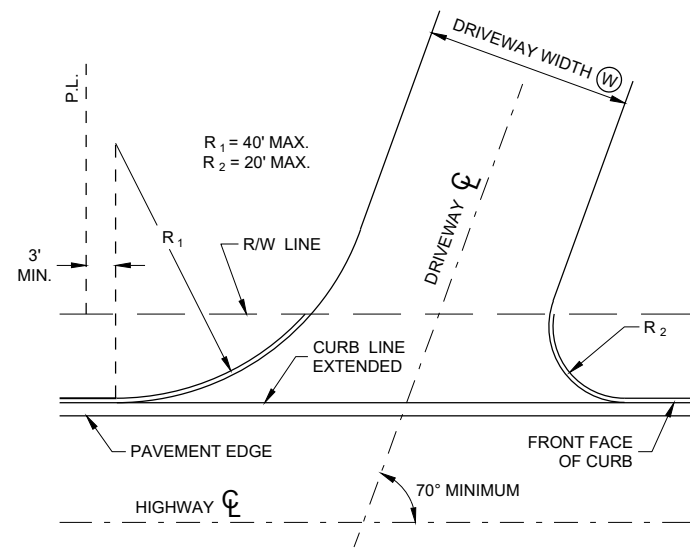


**PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL**

**CURB RAMPS
RECTANGULAR AND RADIAL
DETECTABLE WARNING PLATES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2023 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR



**SKewed DRIVEWAY DETAILS
(COMMERCIAL AND NON-COMMERCIAL)
SIDEWALK NOT SHOWN**

GENERAL NOTES

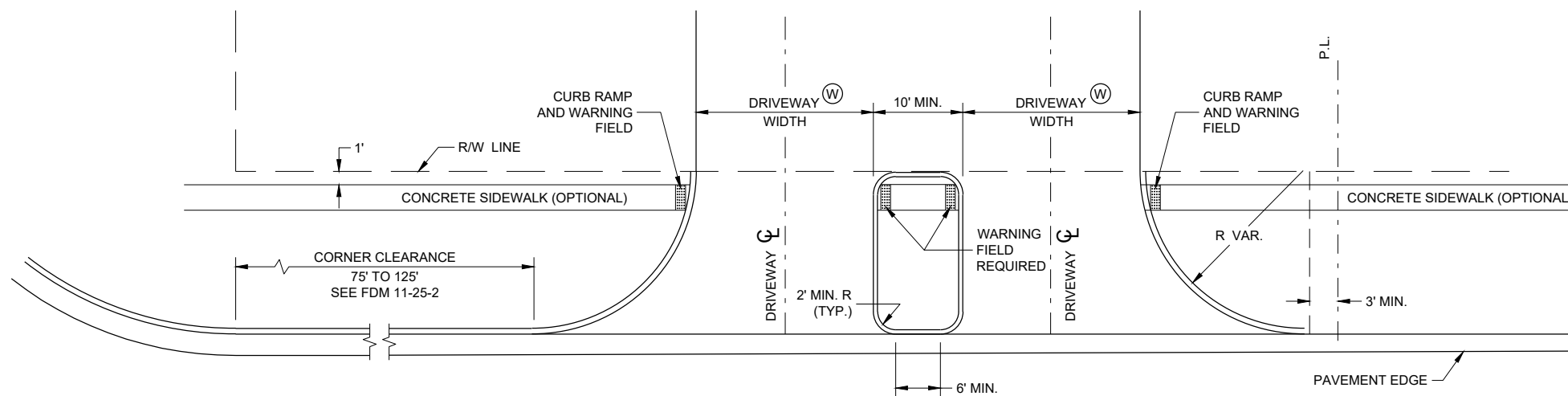
A MAXIMUM RADIUS OF 10 FEET SHALL BE USED FOR NON-COMMERCIAL PRIVATE ENTRANCES. RADII FOR COMMERCIAL DRIVEWAYS SHALL BE DETERMINED BY THE ENGINEER BASED ON TRAFFIC AND DRIVEWAY PERMIT RESTRICTIONS.

THE MINIMUM ANGLE OF INTERSECTION BETWEEN THE DRIVEWAY AND HIGHWAY CENTERLINES SHALL BE 70°.

ALL CURVILINEAR PRIVATE ENTRANCE OUTLINES SHALL BE CONTAINED WITHIN THE HIGHWAY R/W.

NO DRIVEWAY SHALL BE BUILT WITHIN 3 FEET OF THE PROPERTY LINE EXCEPT FOR EXISTING JOINT DRIVEWAY SHARED BY TWO OWNERS.

Ⓜ : 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
16' MIN. - 35' MAX. COMMERCIAL (CE)



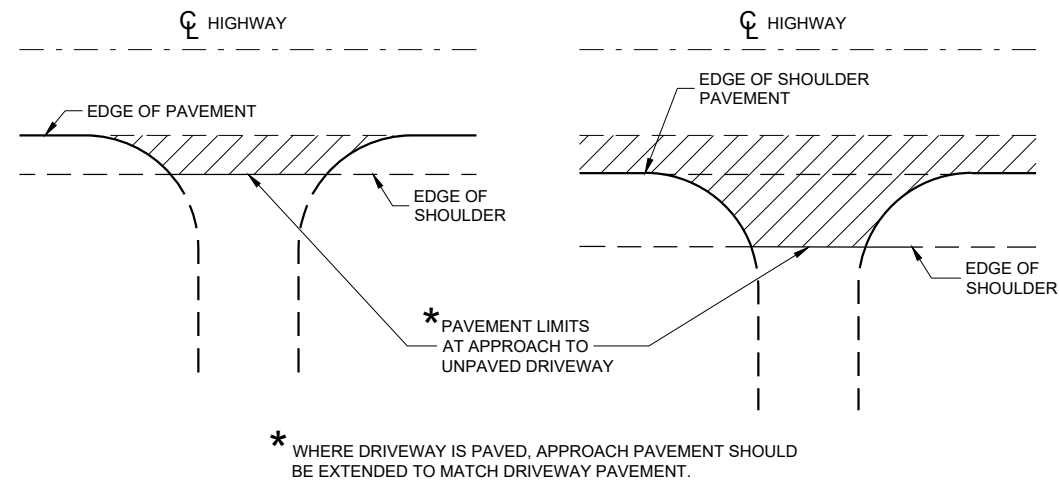
**DRIVEWAY LOCATION AND SPACING DETAILS
SIDEWALK SHOWN**

**DRIVEWAYS WITH
CURB AND GUTTER
RETURNS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
00-00-00 DATE /S/ <AUTHOR>
ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA



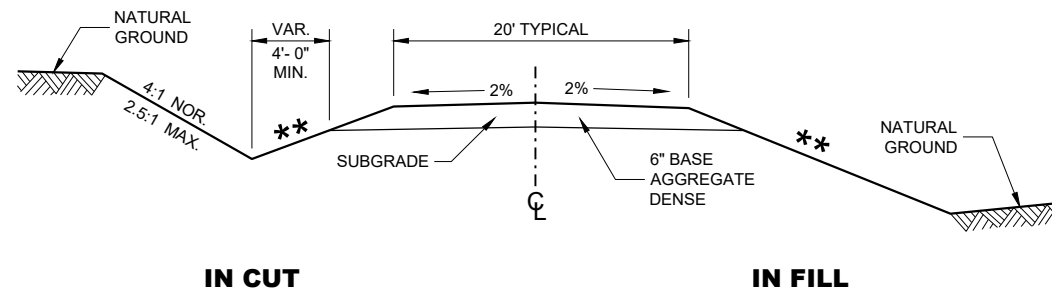
PLAN VIEW

(UNPAVED SHOULDER ON HIGHWAY)

PLAN VIEW

(PAVED SHOULDER ON HIGHWAY)

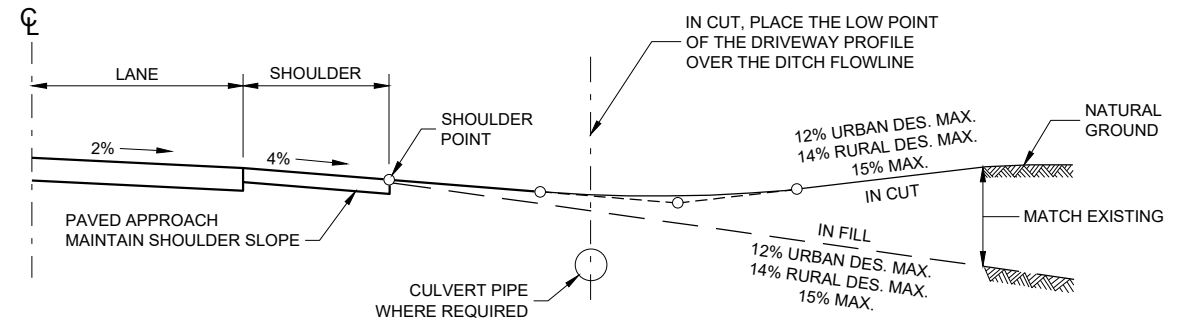
**RURAL DRIVEWAY INTERSECTION DETAIL
(NO CURB AND GUTTER OR SIDEWALK)**



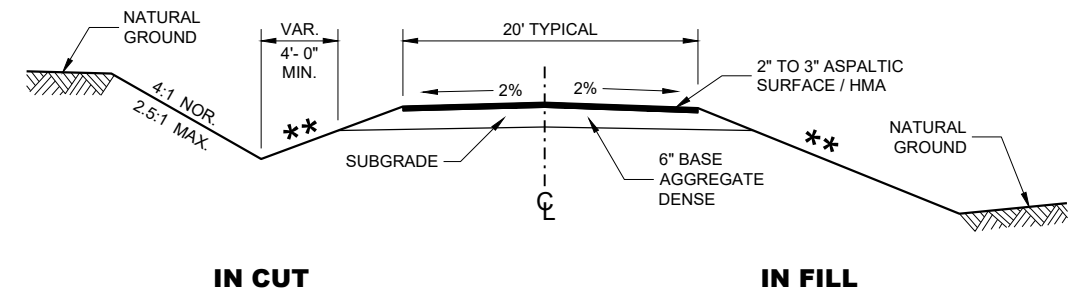
**TYPICAL CROSS SECTION FOR
PRIVATE DRIVE OR FIELD ENTRANCE
AGGREGATE SURFACE**

** SLOPE CAN VARY WITH SPEED. SEE 11-45-30.6.2

POSTED SPEED MPH	MAX. SLOPE
<35	4:1
≥ 35 TO < 60	6:1
≥60	10:1



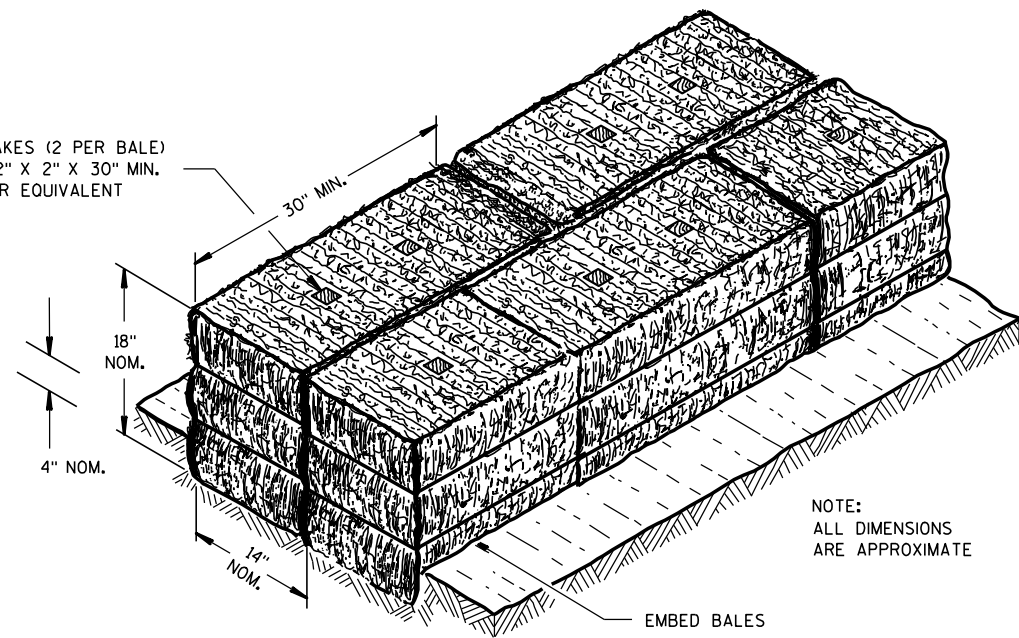
TYPICAL DRIVEWAY PROFILES



**TYPICAL CROSS SECTION FOR
PRIVATE DRIVE OR FIELD ENTRANCE
ASPHALTIC SURFACE**

DRIVEWAYS WITHOUT CURB AND GUTTER	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED December 2017 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	

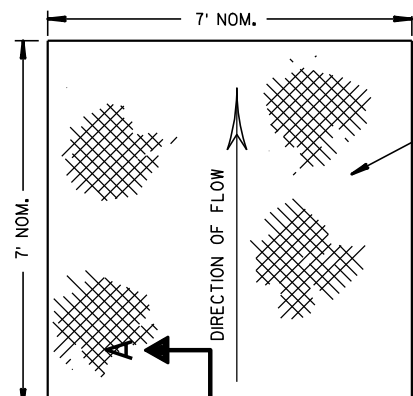
WOOD STAKES (2 PER BALE)
NOMINAL 2" X 2" X 30" MIN.
LENGTH OR EQUIVALENT



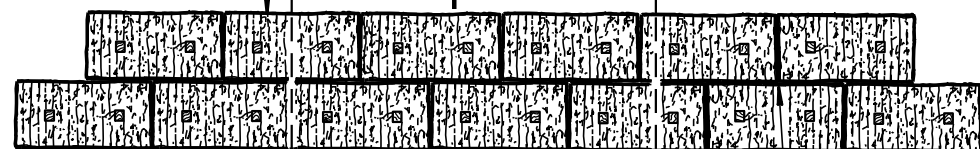
NOTE:
ALL DIMENSIONS
ARE APPROXIMATE

EMBED BALES

SECTION A-A



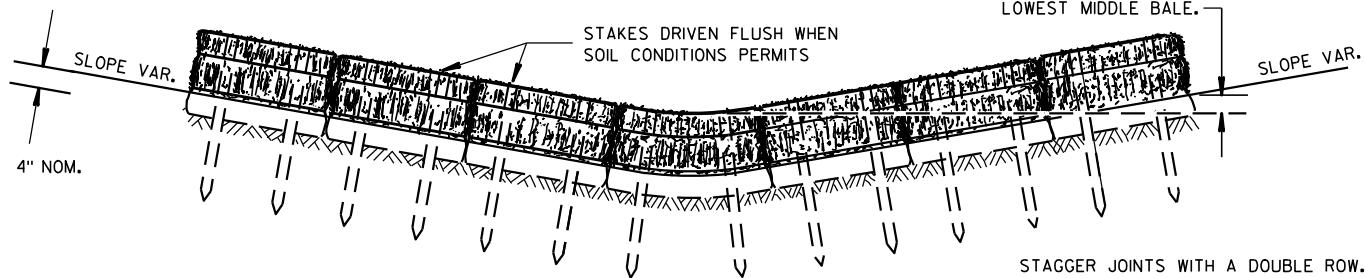
FOR SCOUR PROTECTION USE:
EROSION MAT FOR CHANNEL LINING.
LAP MAT UNDER UPSTREAM BALES
AND SECURE FABRIC WITH WOOD STAKES,
AT 3-FOOT INTERVALS.



STAGGER JOINTS BETWEEN ADJACENT
ROWS OF BALES.

PLAN VIEW

BOTTOM ELEVATION OF END BALE SHALL
BE EQUAL TO OR GREATER THAN TOP OF
LOWEST MIDDLE BALE.



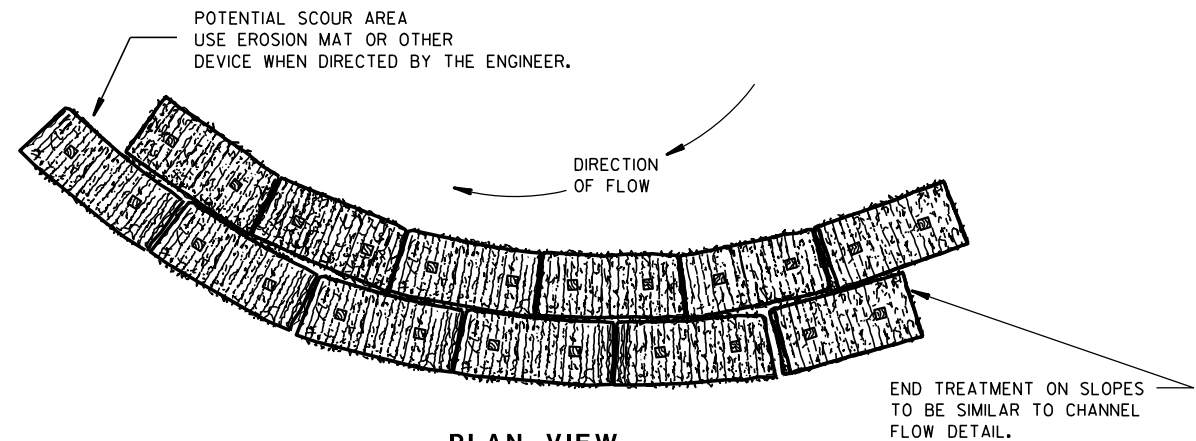
FRONT ELEVATION

TEMPORARY DITCH CHECK USING EROSION BALES ①

GENERAL NOTES

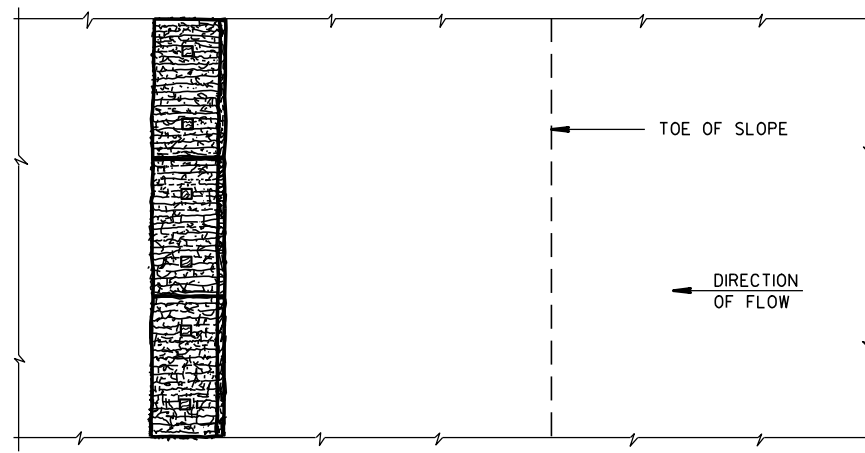
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

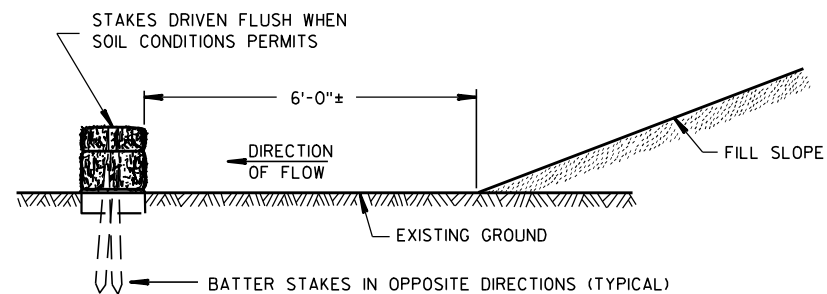


PLAN VIEW

WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW



FRONT ELEVATION

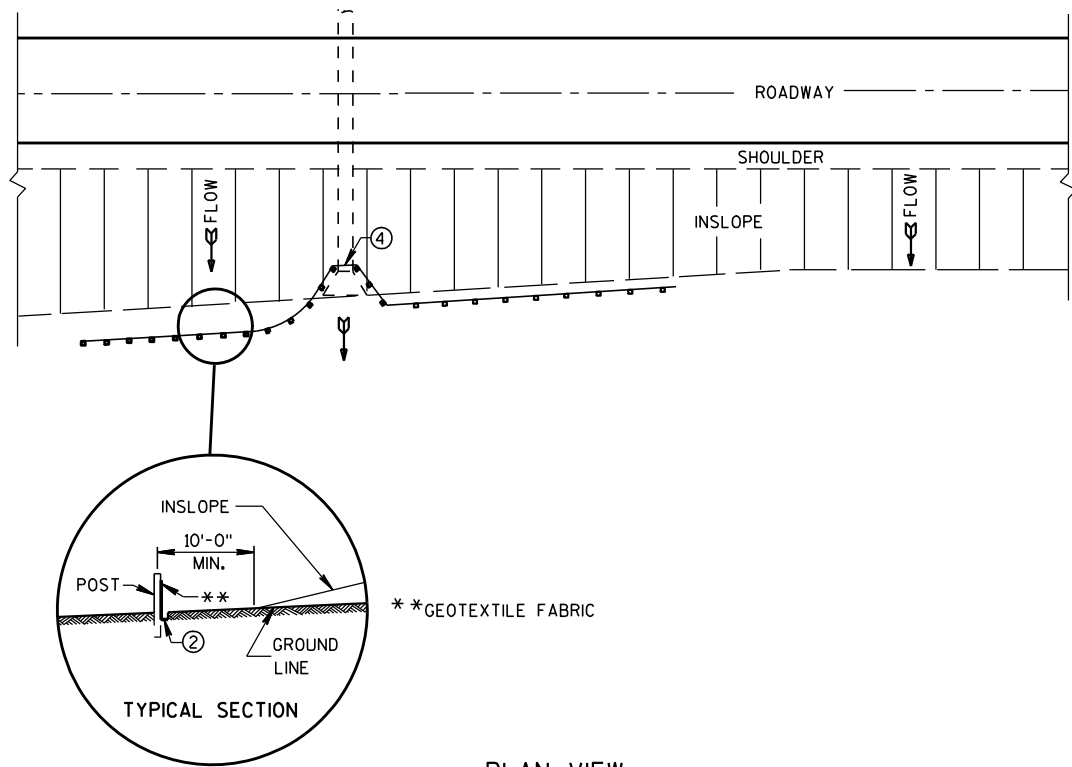
WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

EROSION BALES FOR SHEET FLOW

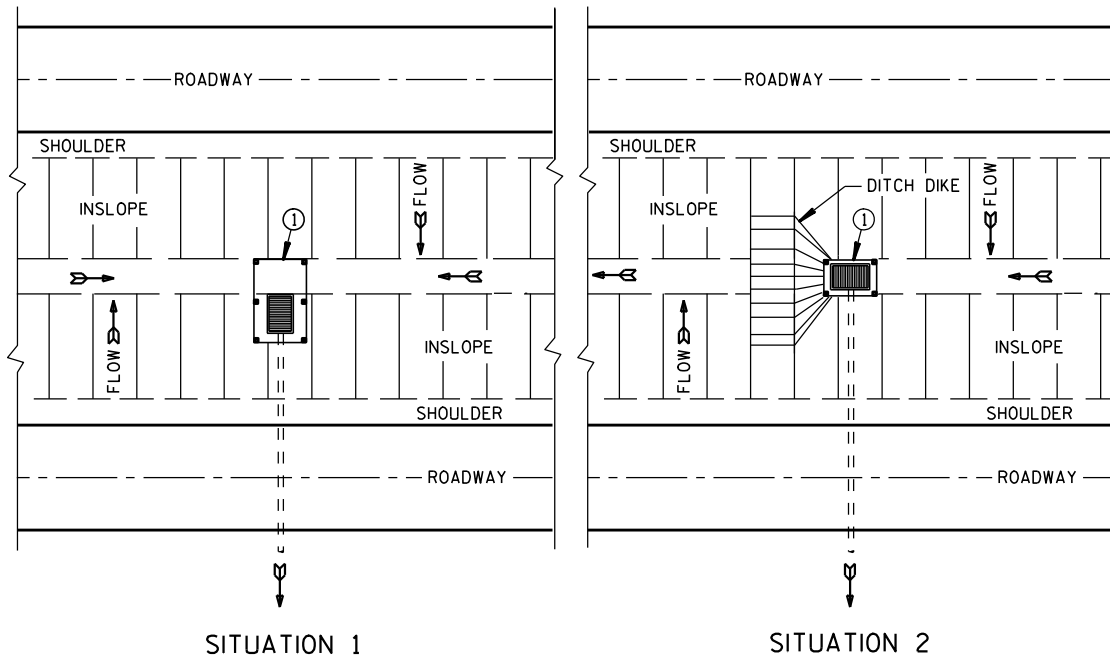
TYPICAL INSTALLATIONS OF
EROSION BALES / TEMPORARY
DITCH CHECKS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
 6/04/02 /S/ Beth Canestra
 DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
 FHWA



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

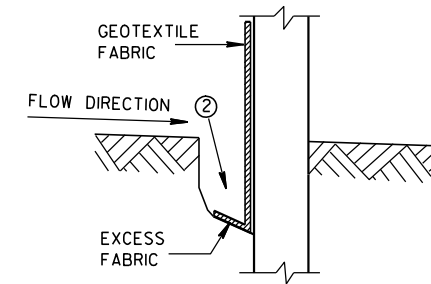


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

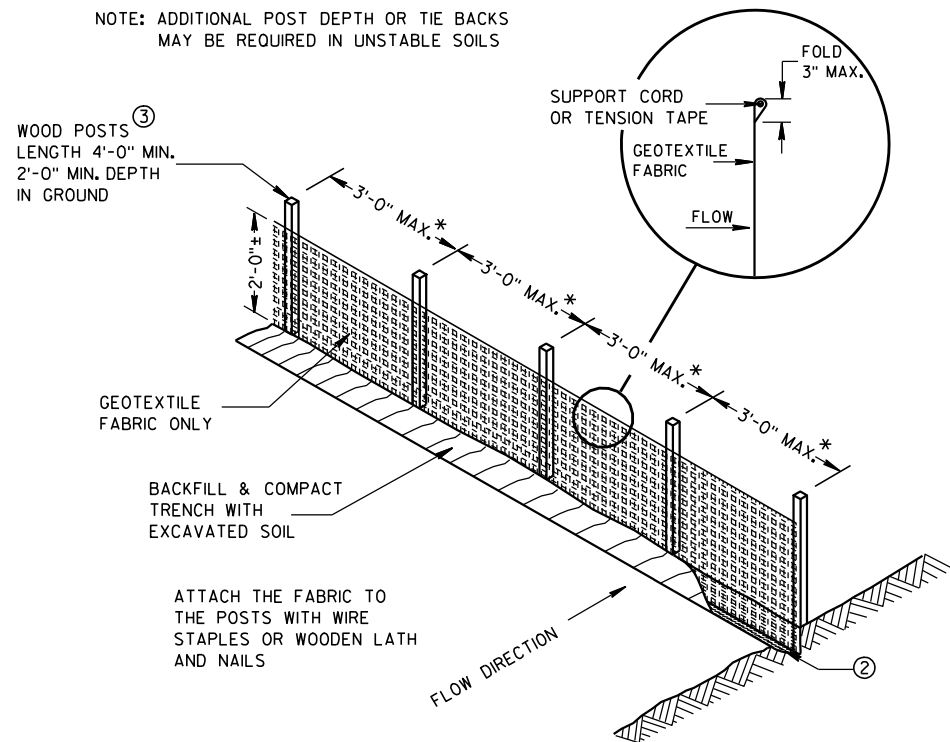
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



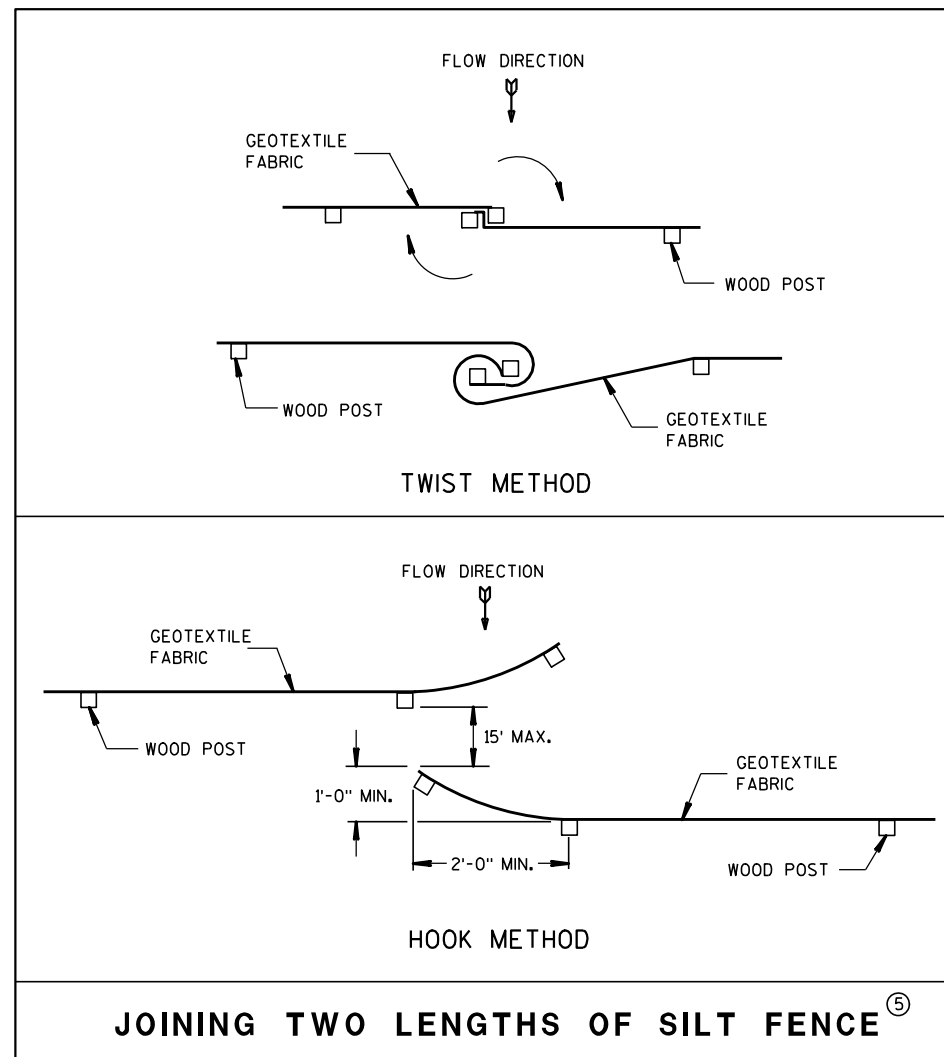
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

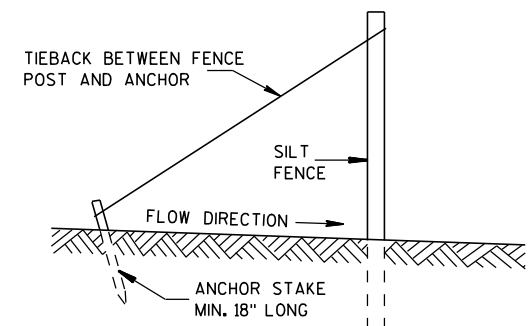


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.



JOINING TWO LENGTHS OF SILT FENCE ⑤

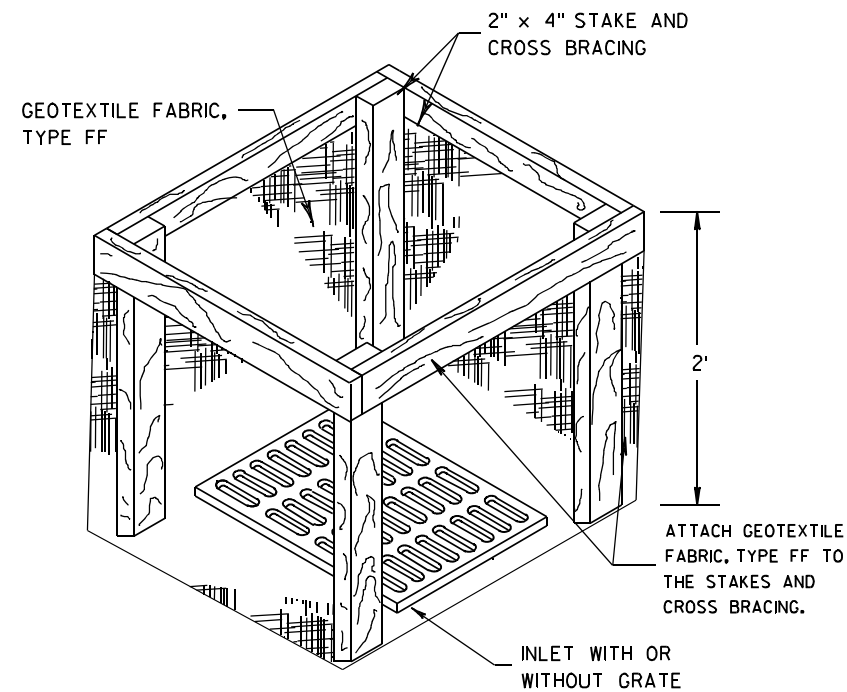
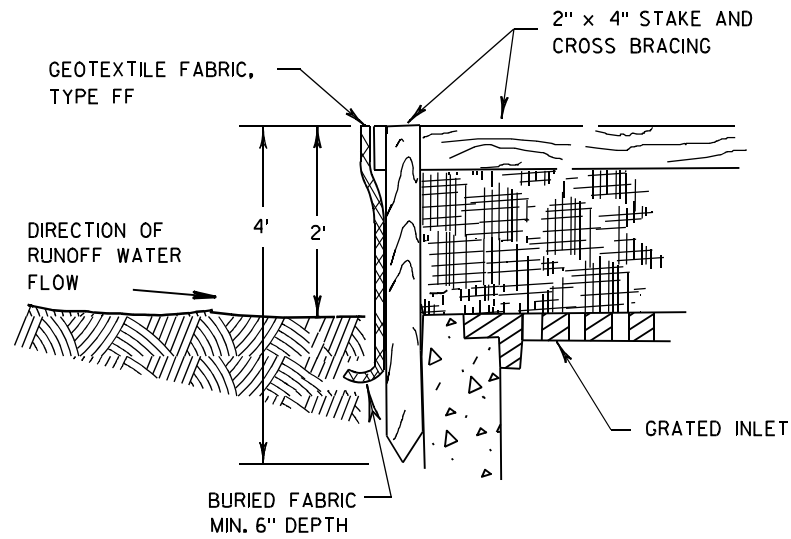


SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 /S/ Beth Canestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



INLET PROTECTION, TYPE A

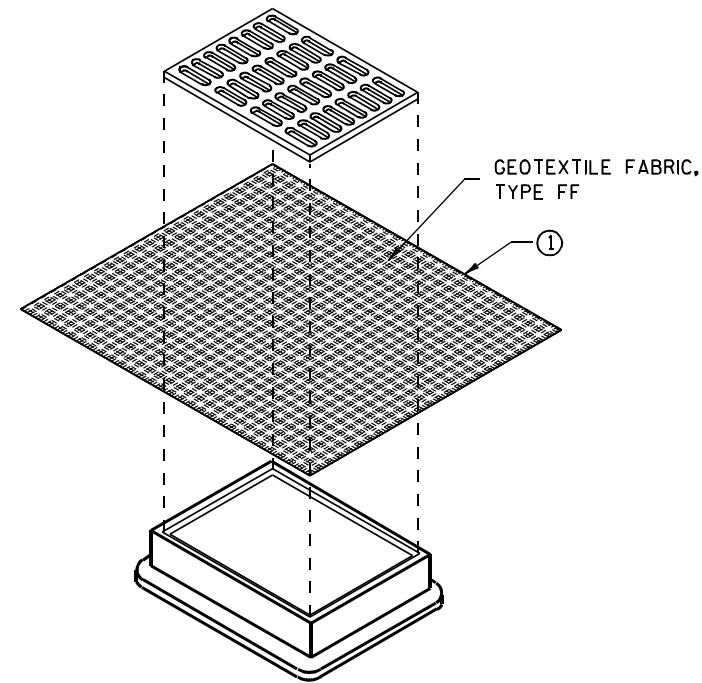
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

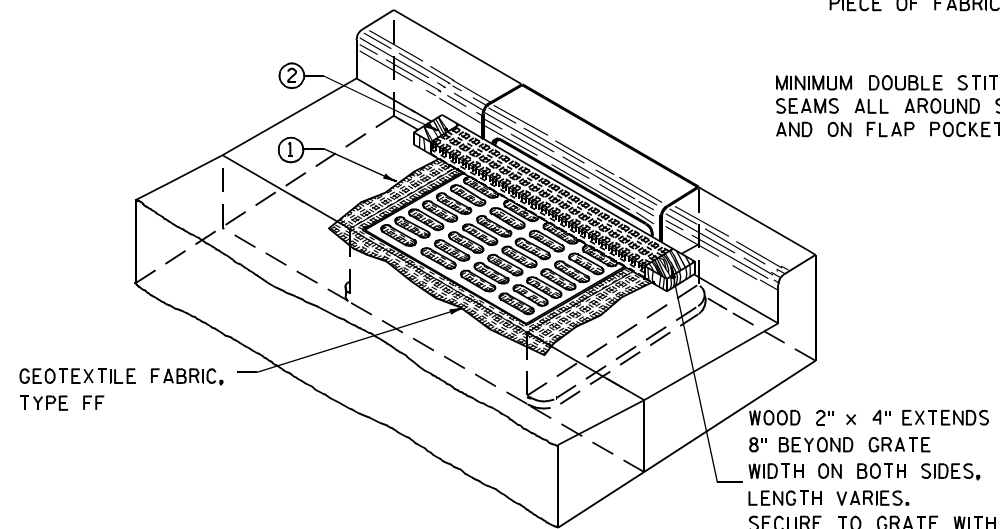
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

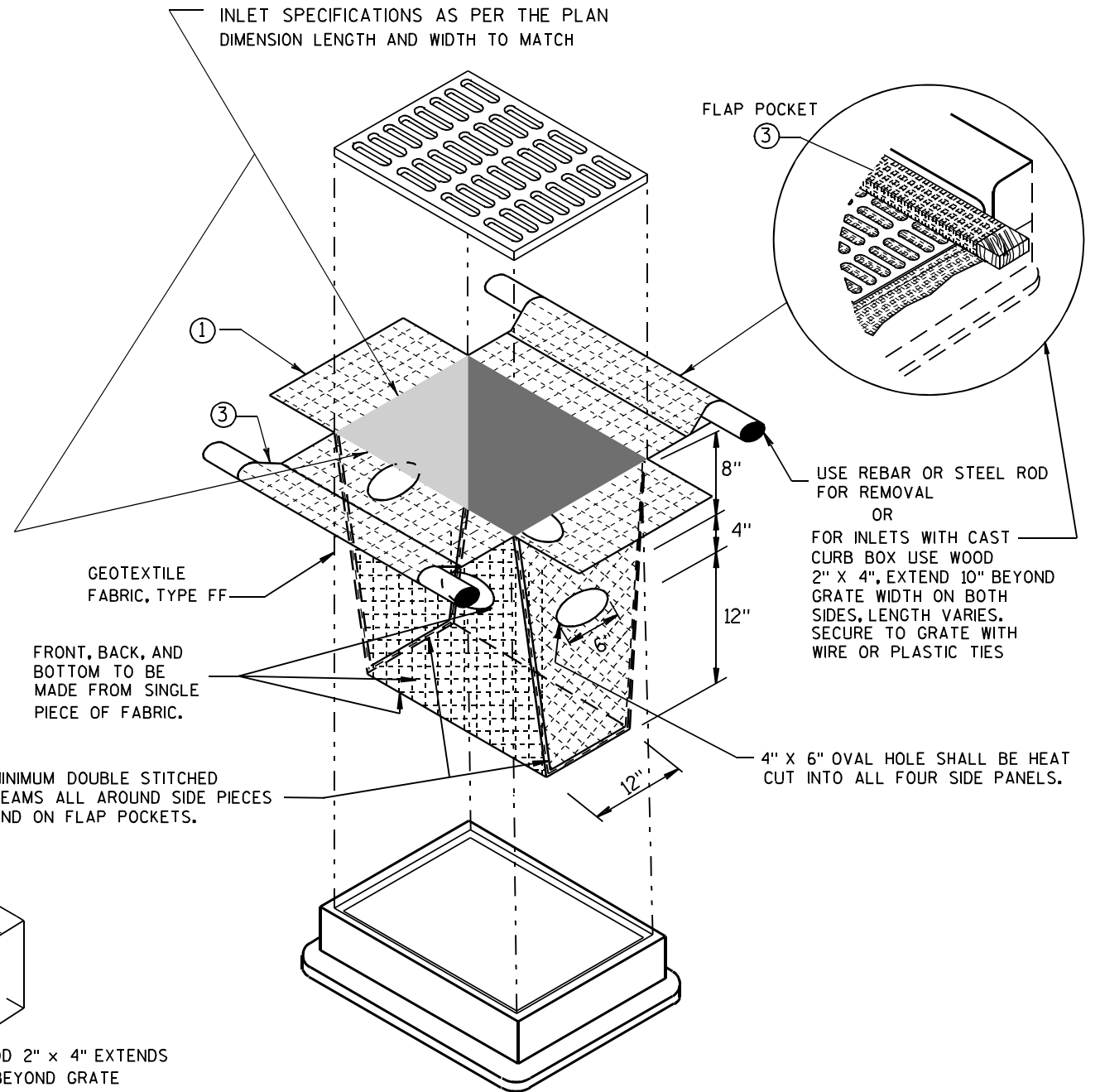
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



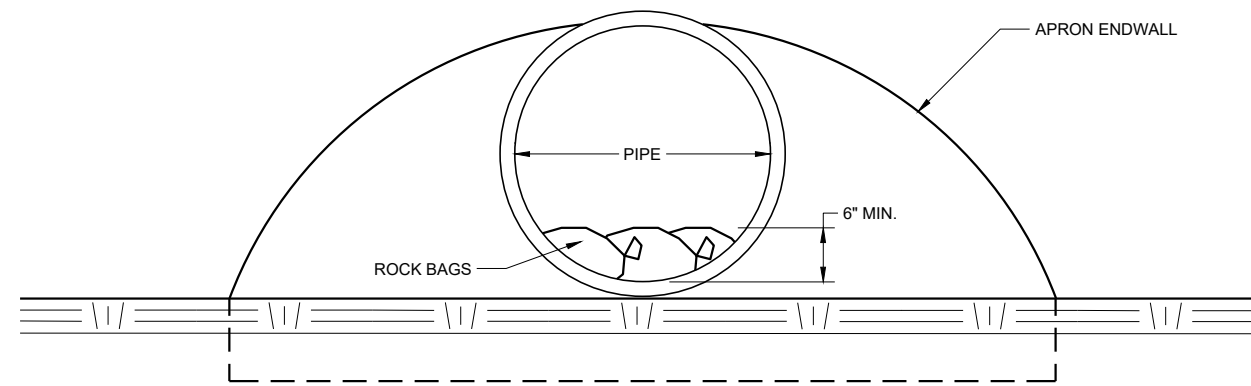
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

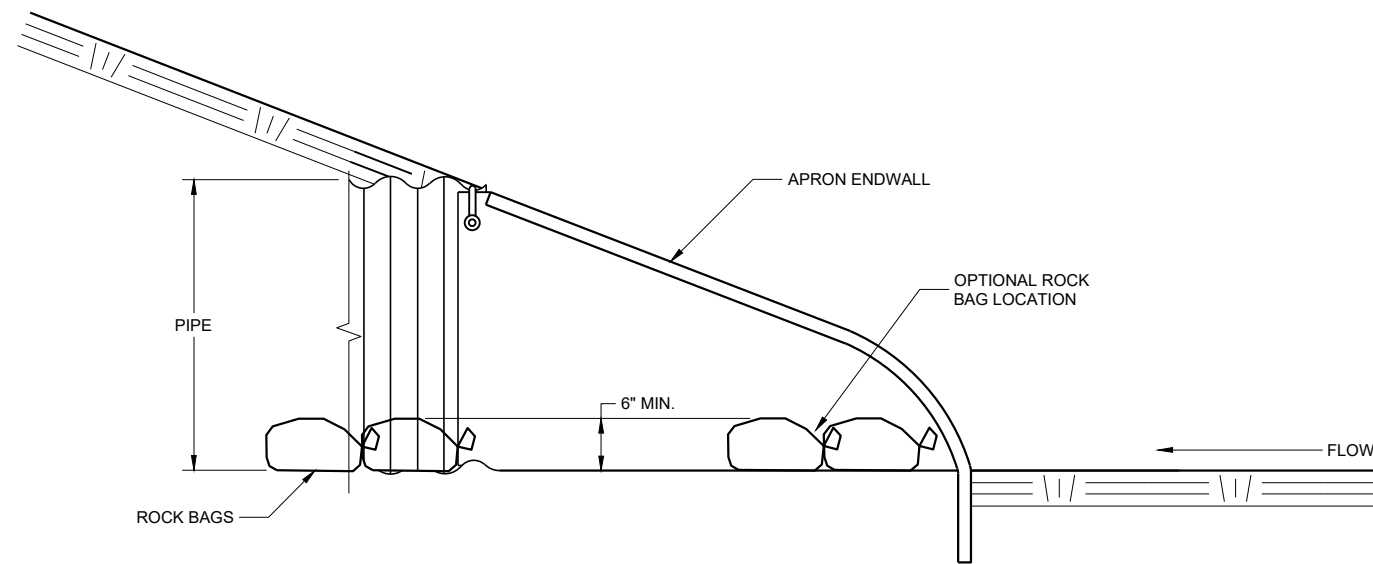
**INLET PROTECTION
TYPE A, B, C, AND D**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02 /S/ Beth Connestra
DATE
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



END VIEW



SIDE VIEW

CULVERT PIPE CHECK
(INSTALL ON INLET END ONLY)

6

6

SDD 08E15 - 01

SDD 08E15 - 01

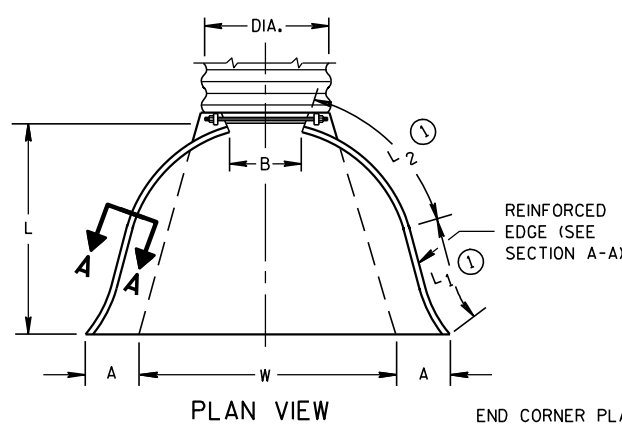
CULVERT PIPE CHECK	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2019 DATE	/S/ Daniel Schave EROSION CONTROL ENGINEER
<small>FHWA</small>	

METAL APRON ENDWALLS											
PIPE DIA. (IN.)	MIN. THICK. (Inches)		DIMENSIONS (Inches)							APPROX. SLOPE	BODY
	STEEL	ALUM.	A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1	L2	W (±2")		
12	.064	.060	6	6	6	21	12	17 1/2	24	2 1/2 to 1	1 Pc.
15	.064	.060	7	8	6	26	14	21 3/4	30	2 1/2 to 1	1 Pc.
18	.064	.060	8	10	6	31	15	28 1/4	36	2 1/2 to 1	1 Pc.
21	.064	.060	9	12	6	36	18	29 5/8	42	2 1/2 to 1	1 Pc.
24	.064	.075	10	13	6	41	18	37 1/4	48	2 1/2 to 1	1 Pc.
30	.079	.075	12	16	8	51	18	52 1/4	60	2 1/2 to 1	1 Pc.
36	.079	.105	14	19	9	60	24	59 3/4	72	2 1/2 to 1	2 Pc.
42	.109	.105	16	22	11	69	24	75 5/8	84	2 1/2 to 1	2 Pc.
48	.109	.105	18	27	12	78	24	81	90	2 1/4 to 1	3 Pc.
54	.109	.105	18	30	12	84	30	85 1/2	102	2 1/4 to 1	3 Pc.
60	.109x	.105x	18	33	12	87	—	—	114	2 to 1	3 Pc.
66	.109x	.105x	18	36	12	87	—	—	120	2 to 1	3 Pc.
72	.109x	.105x	18	39	12	87	—	—	126	2 to 1	3 Pc.
78	.109x	.105x	18	42	12	87	—	—	132	1 1/2 to 1	3 Pc.
84	.109x	.105x	18	45	12	87	—	—	138	1 1/2 to 1	3 Pc.
90	.109x	.105x	18	37	12	87	—	—	144	1 1/2 to 1	3 Pc.
96	.109x	.105x	18	35	12	87	—	—	150	1 1/2 to 1	3 Pc.

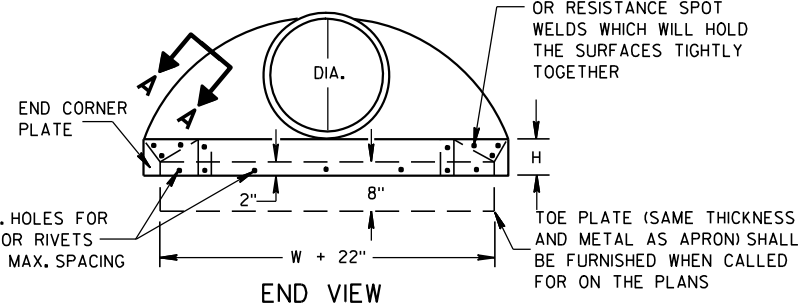
* EXCEPT CENTER PANEL SEE GENERAL NOTES

REINFORCED CONCRETE APRON ENDWALLS									
PIPE DIA. (IN.)	DIMENSIONS (Inches)							APPROX. SLOPE	
	T	A	B	C	D	E	G		
12	2	4	24	48 1/8	72 1/8	24	2	3 to 1	
15	2 1/4	6	27	46	73	30	2 1/4	3 to 1	
18	2 1/2	9	27	46	73	36	2 1/2	3 to 1	
21	2 3/4	9	36	37 1/2	73 1/2	42	2 3/4	3 to 1	
24	3	9 1/2	43 1/2	30	73 1/2	48	3	3 to 1	
27	3 1/4	10 1/2	49 1/2	24	73 1/2	54	3 1/4	3 to 1	
30	3 1/2	12	54	19 3/4	73 1/2	60	3 1/2	3 to 1	
36	4	15	63	34 3/4	97 3/4	72	4	3 to 1	
42	4 1/2	21	63	35	98	78	4 1/2	3 to 1	
48	5	24	72	26	98	84	5	3 to 1	
54	5 1/2	27	65	33 1/4-35	98 1/4-100	90	5 1/2	2 1/2 to 1	
60	6	30-35	60	39	99	96	5	2 to 1	
66	6 1/2	24-30	72-78	21-27	99	102	5 1/2	2 to 1	
72	7	24-36	78	21	99	108	6	2 to 1	
78	7 1/2	24-36	78	21	99	114	6 1/2	2 to 1	
84	8	36	90 1/2	21	111 1/2	120	6 1/2	1 1/2 to 1	
90	8 1/2	41	87 1/2	24	111 1/2	132	6 1/2	1 1/2 to 1	

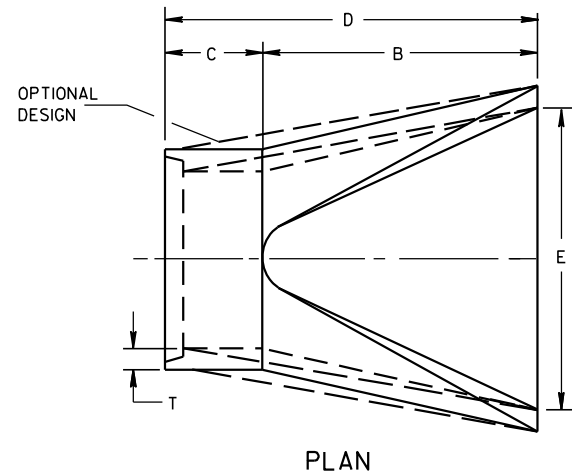
* MINIMUM
** MAXIMUM



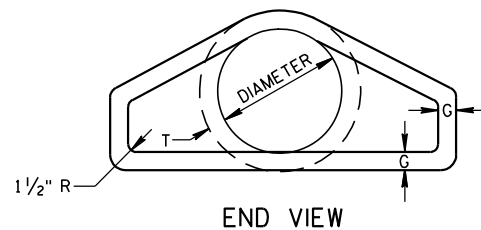
END CORNER PLATES MAY BE FASTENED TO APRON PROPER BY BOLTS, RIVETS, OR RESISTANCE SPOT WELDS WHICH WILL HOLD THE SURFACES TIGHTLY TOGETHER



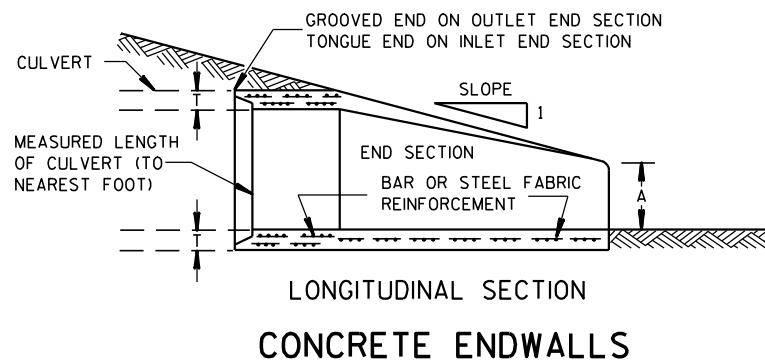
SIDE ELEVATION
METAL ENDWALLS



PLAN

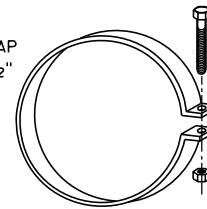


END VIEW

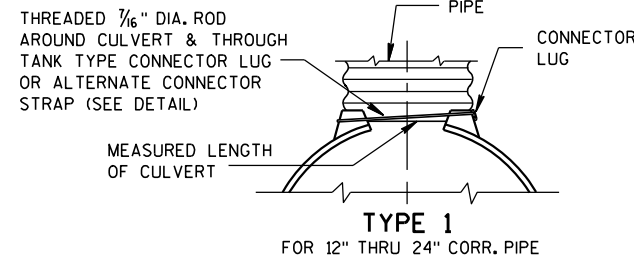


LONGITUDINAL SECTION
CONCRETE ENDWALLS

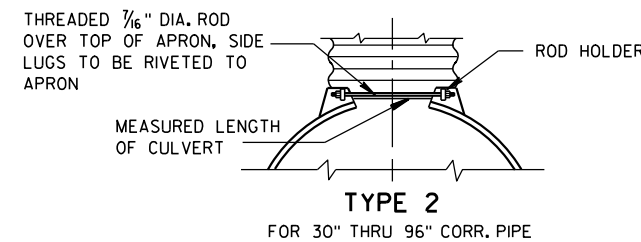
1" WIDE, 12 GA. (0.109" THICK) GALVANIZED STRAP WITH STANDARD 6" X 1/2" BAND BOLT AND NUT



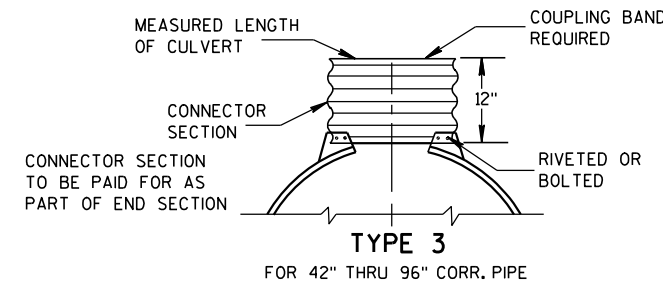
ALTERNATE FOR TYPE 1 CONNECTION
END SECTION CONNECTOR STRAP



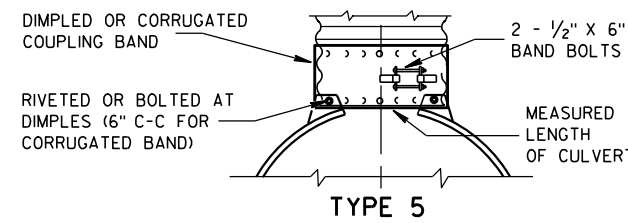
TYPE 1
FOR 12" THRU 24" CORR. PIPE



TYPE 2
FOR 30" THRU 96" CORR. PIPE



TYPE 3
FOR 42" THRU 96" CORR. PIPE



TYPE 5
ALTERNATE FOR:
ALL SIZES CORRUGATED CIRCULAR PIPE

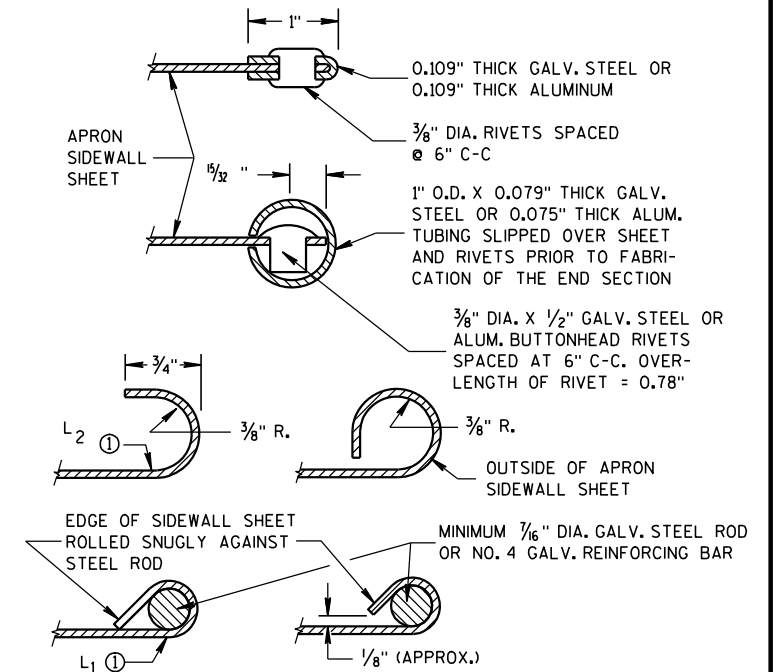
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL, AND CORRUGATED BAND FITS INSIDE ENDWALL. DIMPLED BAND MAY BE USED WITH HELICALLY CORRUGATED PIPE.

FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5 AS APPLICABLE.

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VICE VERSA. GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

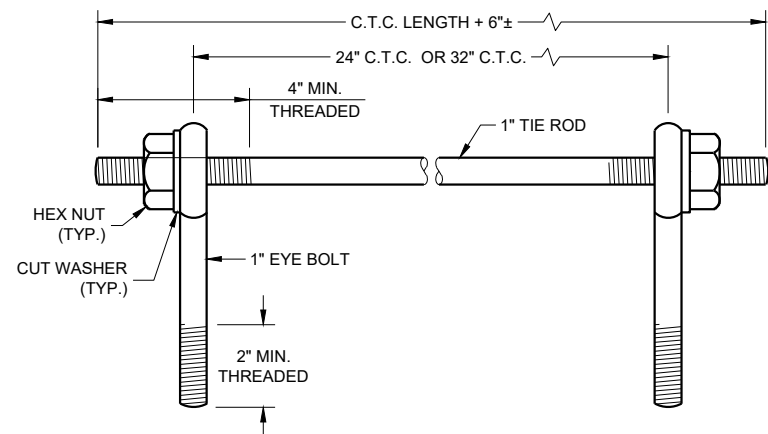
WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

① FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

APRON ENDWALLS FOR
CULVERT PIPE

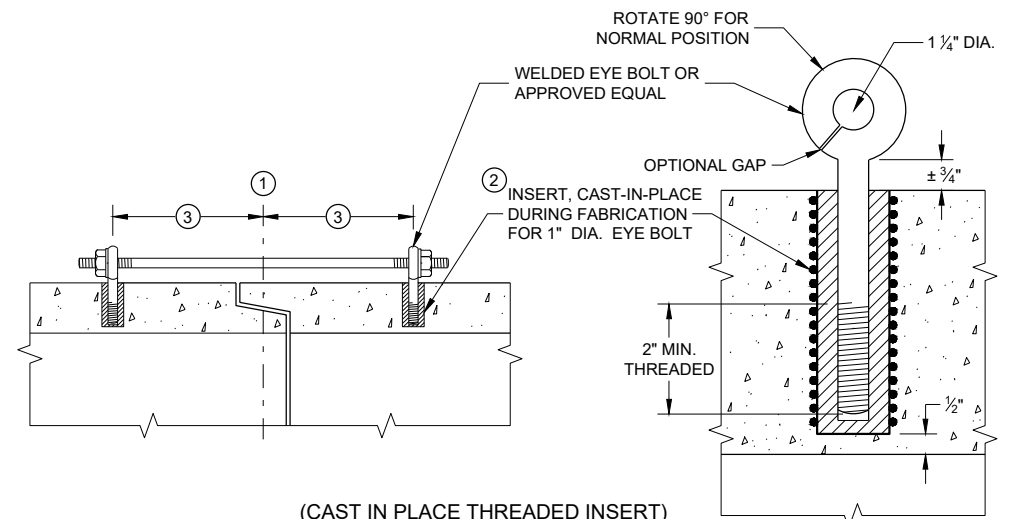
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/30/94 DATE /S/ Rory L. Rhinesmith
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



EYE BOLTS AND TIE ROD

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)



(CAST IN PLACE THREADED INSERT)
LONGITUDINAL SECTIONS

GENERAL NOTES

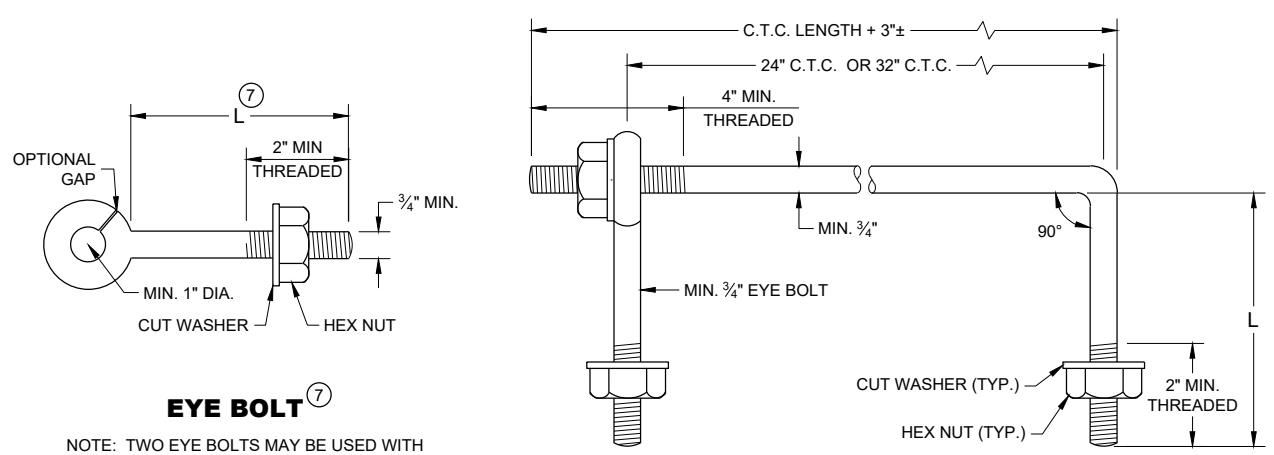
DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

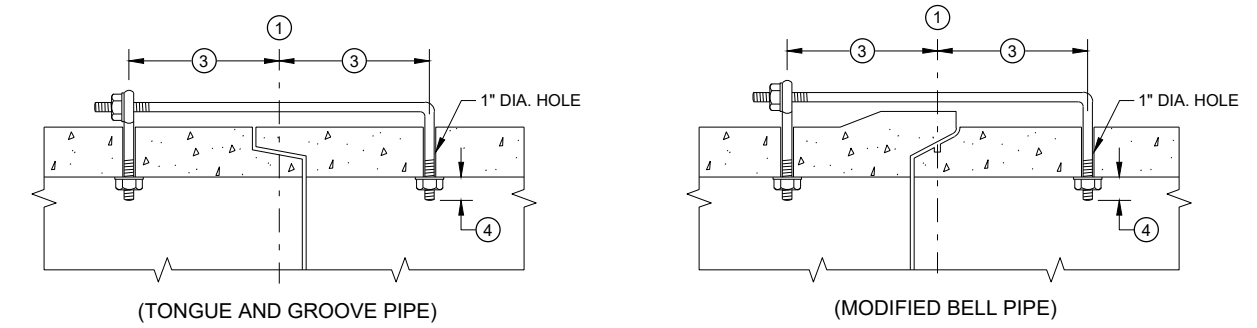
- ① CENTER LINE OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.
- ② THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.
- ③ HOLES SHALL BE CAST-IN-PLACE OR DRILLED PER THE APPLICABLE DETAIL, AND EQUAL DISTANCE FROM THE CENTERLINE OF THE JOINT.
- ④ BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.
- ⑤ OPENING TO BE ROD DIAMETER PLUS 1 INCH.
- ⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN 1/2 INCH OF THE INNER SURFACE OF THE PIPE.
- ⑦ EYE BOLT LENGTH DETERMINED BY WALL THICKNESS, BELL THICKNESS AND BOLT PROJECTION INSIDE PIPE.



EYE BOLT AND TIE ROD

EYE BOLT

NOTE: TWO EYE BOLTS MAY BE USED WITH A 30" OR 38" LONG THREADED ROD IN LIEU OF THE 90° BENT TIE ROD.



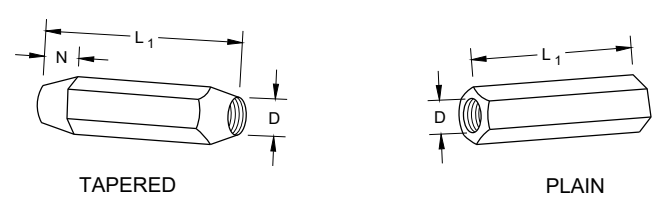
LONGITUDINAL SECTION
(JOINT TIES FOR 18" TO 66" DIA. CONCRETE PIPE)

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)

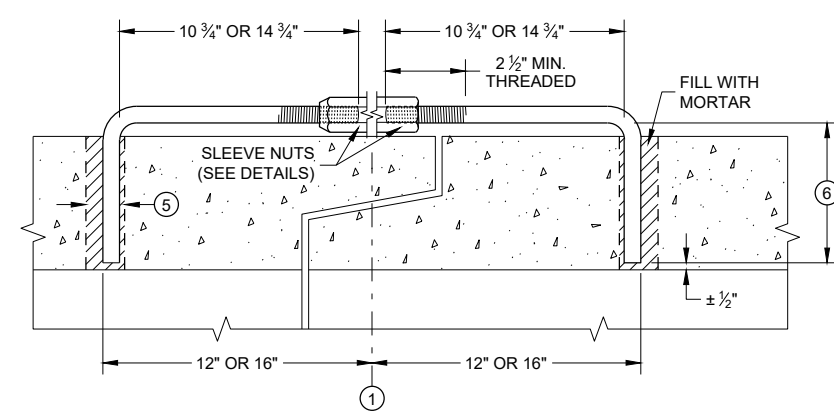
ADJUSTABLE TIE ROD TABLE

PIPE DIAMETER	TIE ROD DIAMETER	D	L ₁	N
12 - 60	5/8	5/8	5	1/2
66 - 84	3/4	3/4	5	1/2
90 - 144	1	1	7	1 7/16

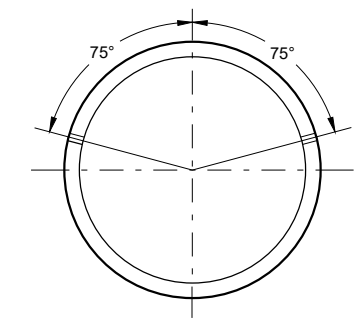
DIMENSIONS SHOWN ARE IN INCHES



RIGHT AND LEFT THREADS SLEEVE NUTS

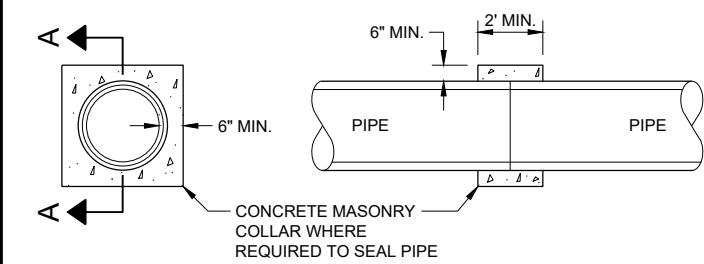


LONGITUDINAL SECTION
ADJUSTABLE TIE ROD (ALTERNATE NO. 3)



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION



SECTION A - A
CONCRETE COLLAR DETAIL

JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

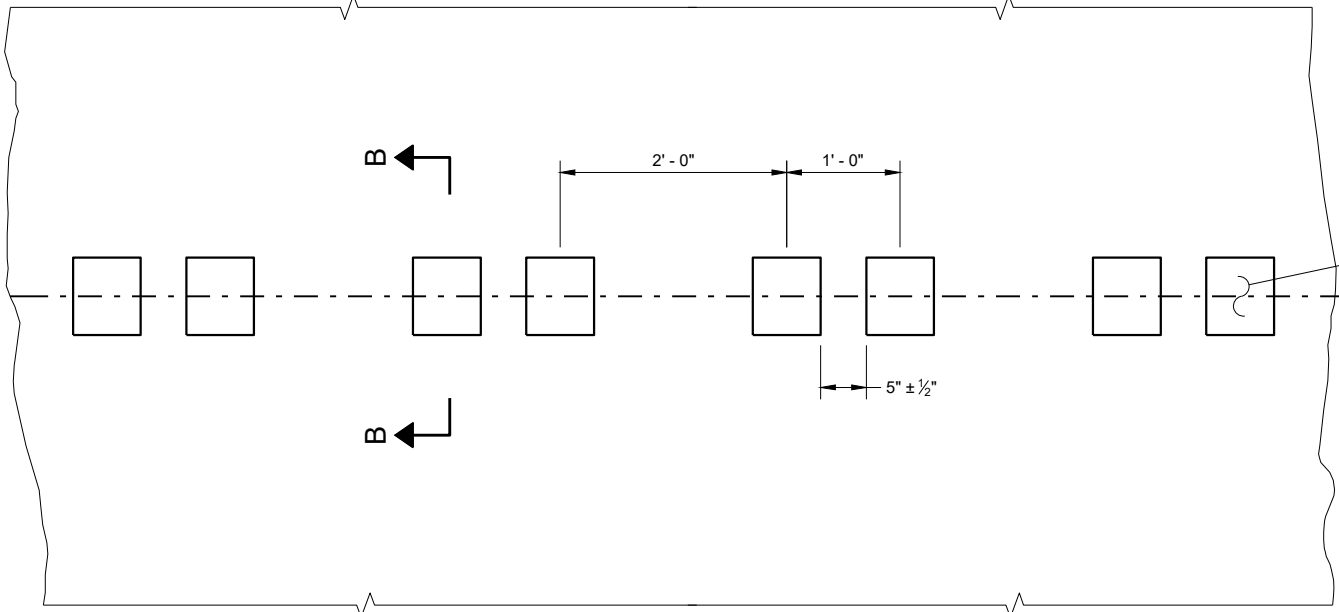
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

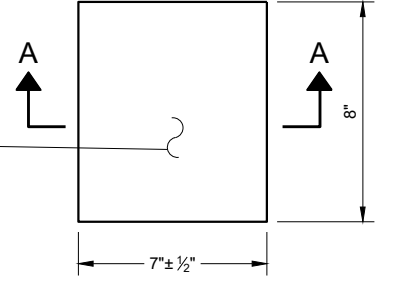
GENERAL NOTES

DO NOT MILL SHOULDER GROOVES THROUGH INTERSECTIONS, MARKED CROSSWALKS, NON-MOTORIZED PATH CROSSINGS, ETC. REFER TO SDD 13A11 SHEETS "d" AND "e".

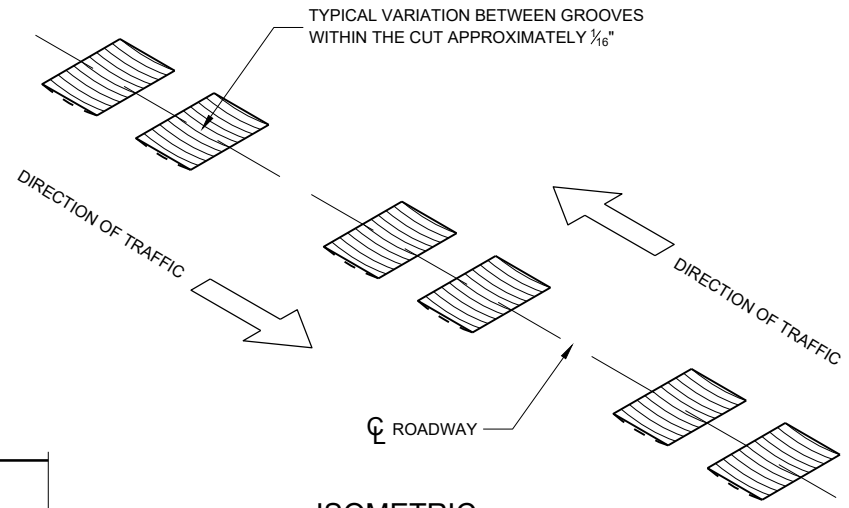
CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS WHEN DIRECTED BY THE ENGINEER.



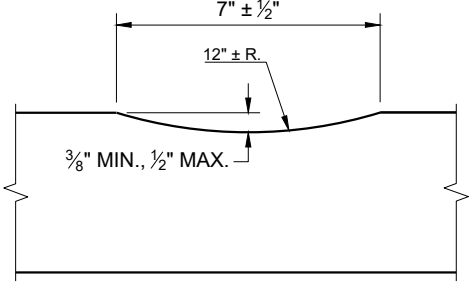
PLAN DETAIL VIEW



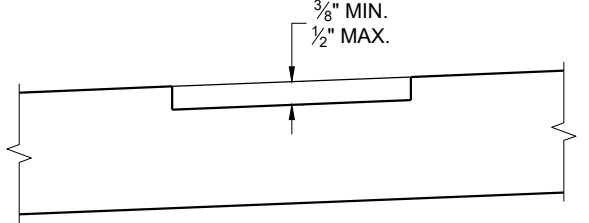
PLAN VIEW (SINGLE GROOVE)



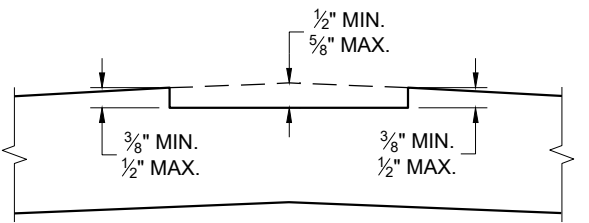
ISOMETRIC



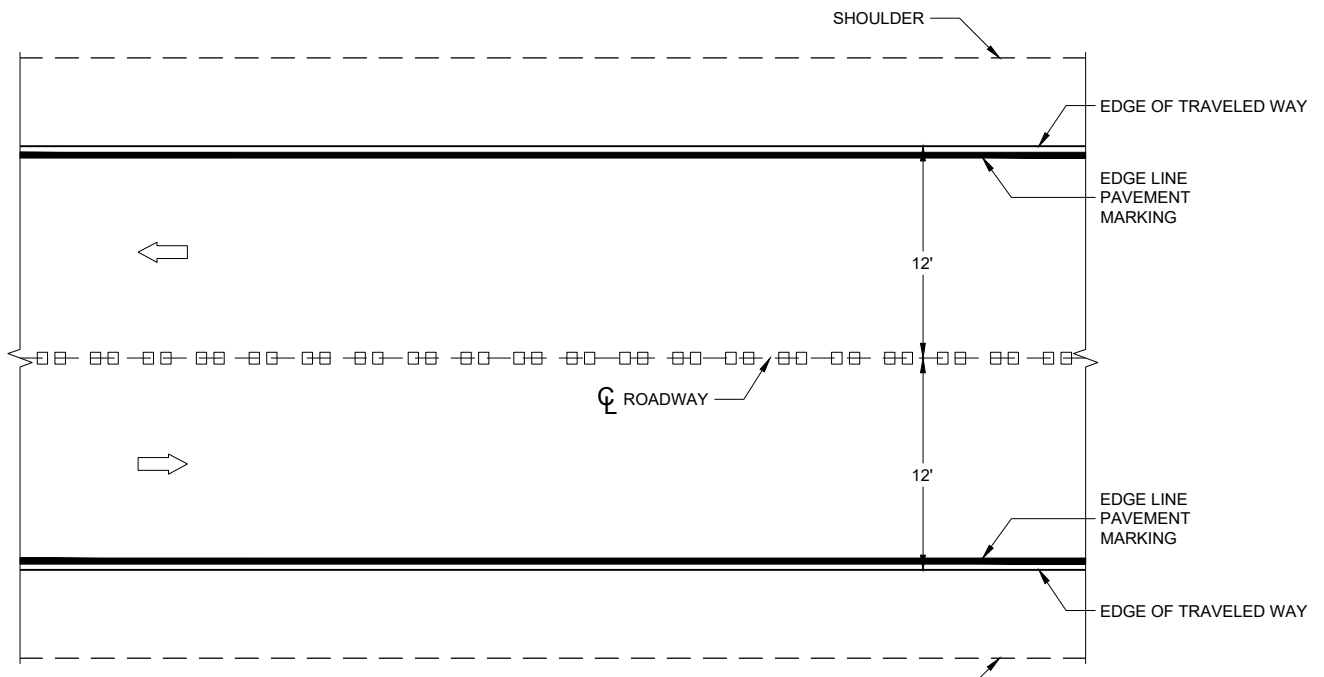
SECTION A - A



SECTION B - B SUPERELEVATED ROADWAY



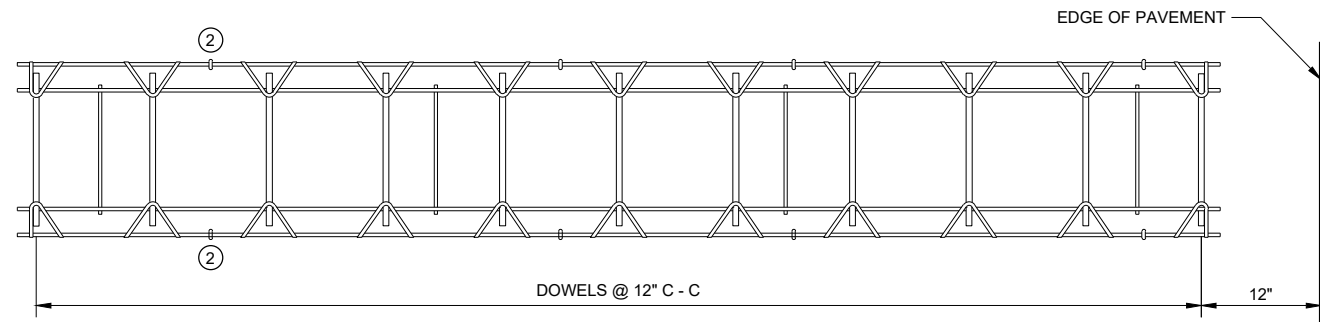
SECTION B - B CROWNED ROADWAY



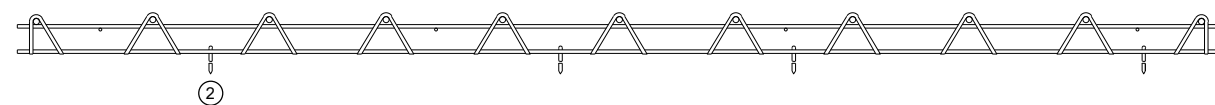
PLAN VIEW

CENTERLINE RUMBLE STRIPS - ASPHALT

CENTERLINE RUMBLE STRIPS - ASPHALT
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

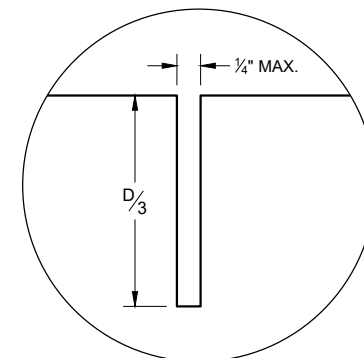


PLAN VIEW



SIDE VIEW

CONTRACTION JOINT DOWEL ASSEMBLY ①



JOINT DETAIL

GENERAL NOTES

CONTRACTION JOINTS

CONSTRUCT TRANSVERSE CONTRACTION JOINTS NORMAL TO THE CENTERLINE. SHOW THE LOCATION OF CONTRACTION JOINTS THROUGH INTERSECTIONS ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

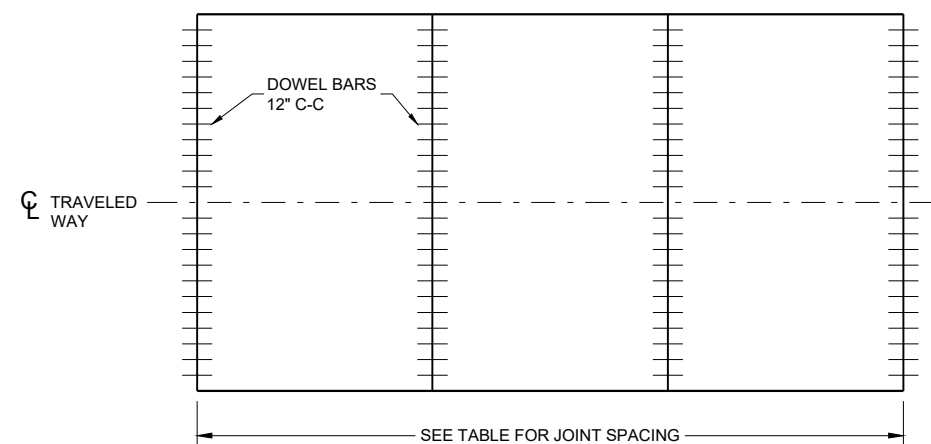
INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.

FOR PAVEMENT SLABS OF VARYING WIDTHS, LOCATE THE OUTER MOST DOWEL BAR SO THAT THE CENTER OF THE BAR IS A MINIMUM OF 6 INCHES FROM AND A MAXIMUM OF 18 INCHES FROM THE FREE EDGE OF PAVEMENT.

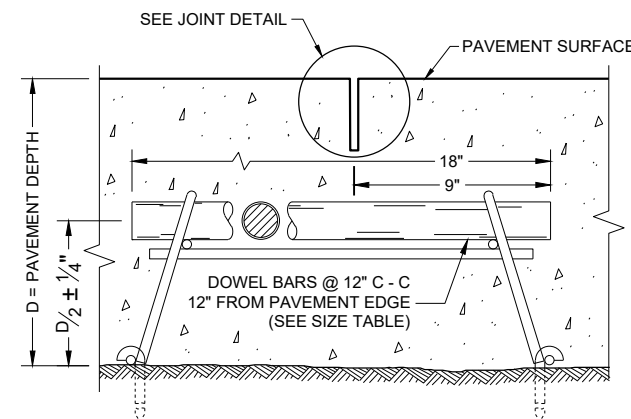
CONSTRUCTION JOINTS

LOCATE CONSTRUCTION JOINTS A MINIMUM OF 6 FEET FROM THE NEAREST CONTRACTION JOINT AND ALIGN PARALLEL TO THE CONTRACTION JOINTS.

- ① OBTAIN THE ENGINEER'S APPROVAL FOR THE USE OF ALTERNATIVE DESIGNS OF THE DOWEL ASSEMBLY. USE MECHANICAL DOWEL BAR INSERTERS OR DOWEL ASSEMBLIES WHEN CONSTRUCTION CONTRACTION JOINTS.
- ② SECURE BASKETS WITH ANCHORS TO HOLD DOWEL BARS IN THE CORRECT POSITION AND ALIGNMENT. TYPE, LOCATION, NUMBER AND LENGTH OF ANCHORS ARE DEPENDENT UPON FIELD CONDITIONS.
- ③ FORM OR SAW CONSTRUCTION JOINTS. PROVIDE A 1/4" RADIUS AT FORMED JOINTS.
- ④ PROVIDE A SMOOTH VERTICAL FACE FOR THE ENTIRE DEPTH OF THE PAVEMENT WHEN FORMING CONSTRUCTION JOINTS.
- ⑤ INSTALL DOWEL BARS AT CONSTRUCTION JOINTS BY FORMING OR DRILLING. INSTALL FORMED DOWEL BARS 12 INCHES C - C AND 12 INCHES FROM PAVEMENT EDGE. REMOVE EXCESS CONCRETE FROM THE FREE END OF THE DOWEL BAR IF DOWEL BARS ARE FORMED THROUGH A HEADER BOARD. INSTALL DRILLED DOWEL BARS ACCORDING TO THE "DRILLED DOWEL BAR CONSTRUCTION JOINT" DETAIL.
- ⑥ APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.
- ⑦ ANCHOR DOWEL BARS AND TIE BARS INTO DRILLED HOLES WITH AN EPOXY. MAXIMUM DRILLED HOLE SIZE IS 1/8" GREATER THAN DOWEL BAR DIAMETER, 9 INCHES IN LENGTH.



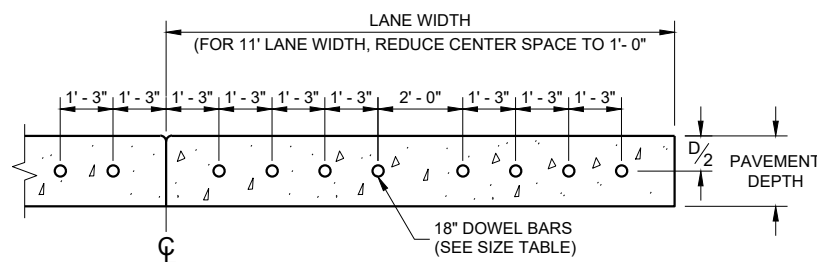
CONTRACTION JOINT LOCATIONS



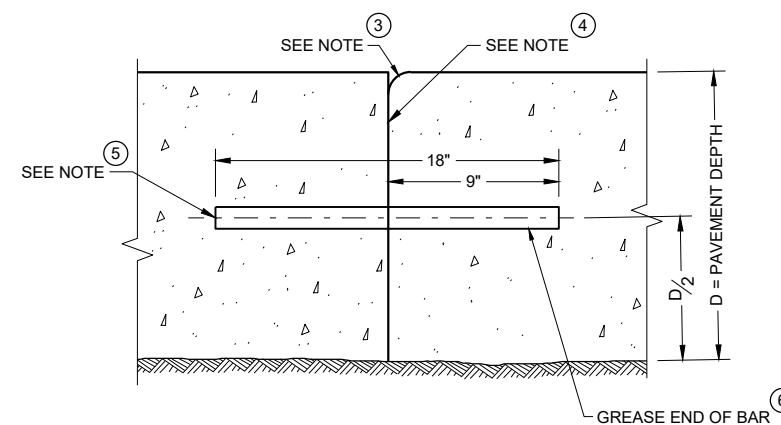
DOWELED CONTRACTION JOINT

PAVEMENT DEPTH, DOWEL BAR SIZE AND JOINT SPACING TABLE

PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	CONTRACTION JOINT SPACING
6", 6 1/2"	NONE	12'
7", 7 1/2"	1"	14'
8" & ABOVE	1 1/4"	15'



DRILLED DOWEL BAR CONSTRUCTION JOINT ⑦

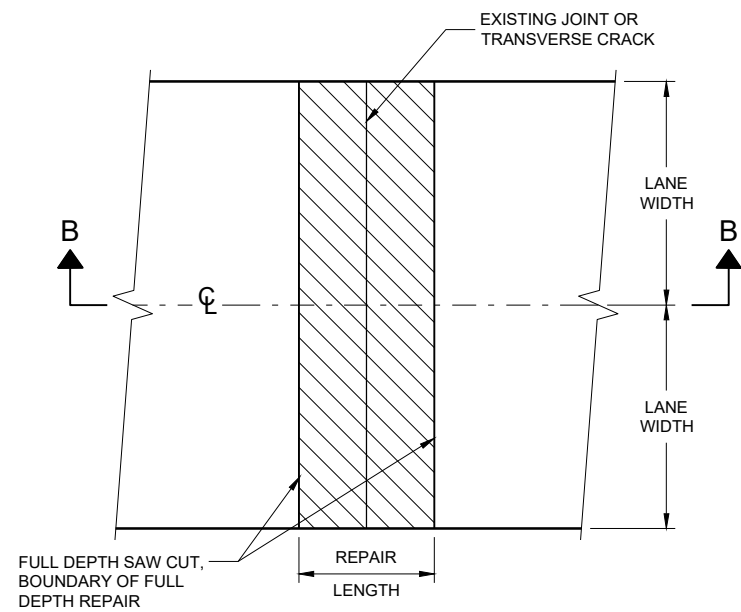


TRANSVERSE CONSTRUCTION JOINT

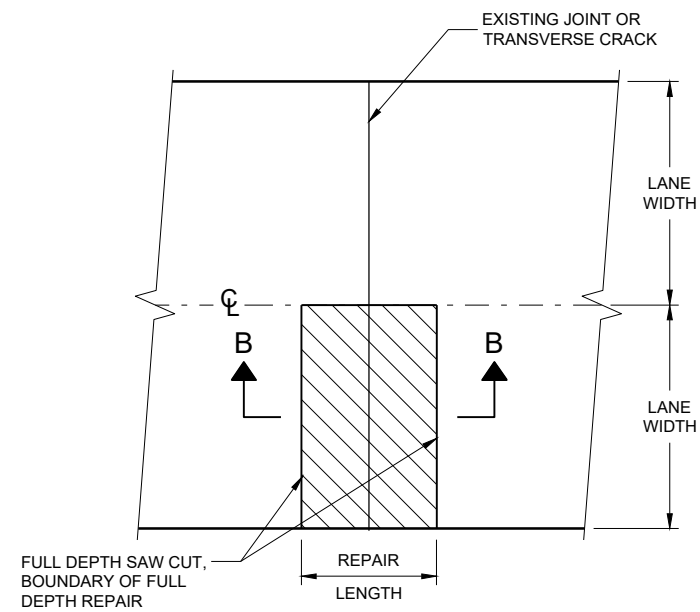
**URBAN DOWELED
CONCRETE PAVEMENT**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022 /S/ Peter Kemp P.E.
DATE PAVEMENT SUPERVISOR
FHWA

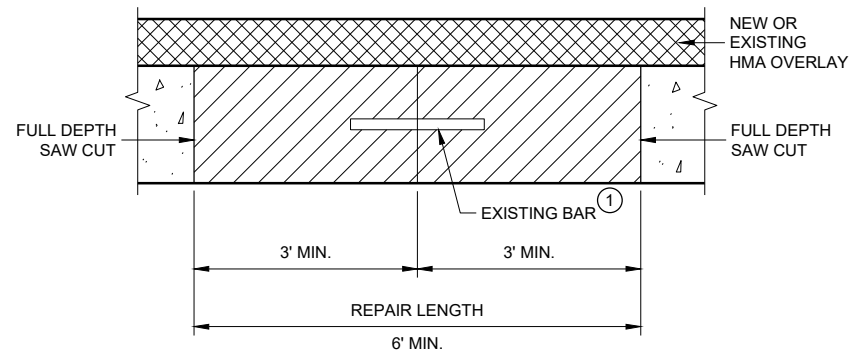


**PLAN VIEW
DOUBLE LANE REPAIR**



**PLAN VIEW
SINGLE LANE REPAIR**

FULL DEPTH CONCRETE PAVEMENT REMOVAL



**SECTION B - B
CONCRETE REMOVAL**

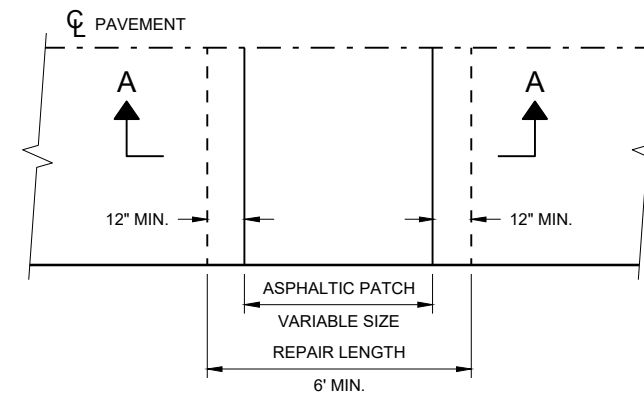
GENERAL NOTES

SAW CUT, DRILL, AND LIFT OUT EXISTING CONCRETE PAVEMENT WITHIN THE BOUNDARIES OF CONCRETE REPAIR AREAS. THE CONTRACTOR MAY MAKE ADDITIONAL SAW CUTS INSIDE THE REPAIR LIMITS TO REDUCE WEIGHT AND SIZE OF CONCRETE PIECES.

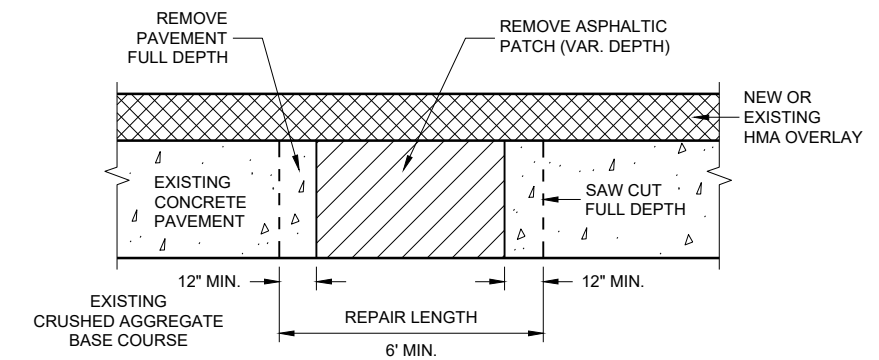
PROVIDE A 6 FOOT MINIMUM DISTANCE FROM BOUNDARIES OF CONCRETE REPAIR AREA TO ADJACENT TRANSVERSE JOINT OR CRACK.

THE LENGTH OF THE REPAIRS MAY VARY FROM THE DIMENSIONS SHOWN IF THE EXISTING CONCRETE PAVEMENT IS NON-DOWELED AND THE PAVEMENT IS TO BE OVERLAID AFTER REPAIRING.

① DOWEL BARS MAY NOT BE PRESENT.



PLAN VIEW

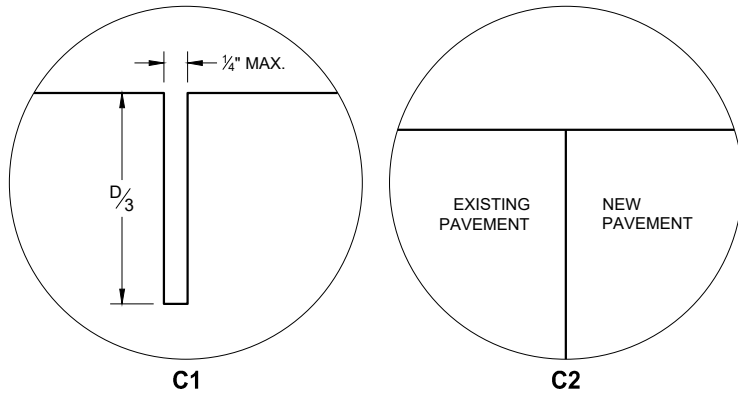


SECTION A - A

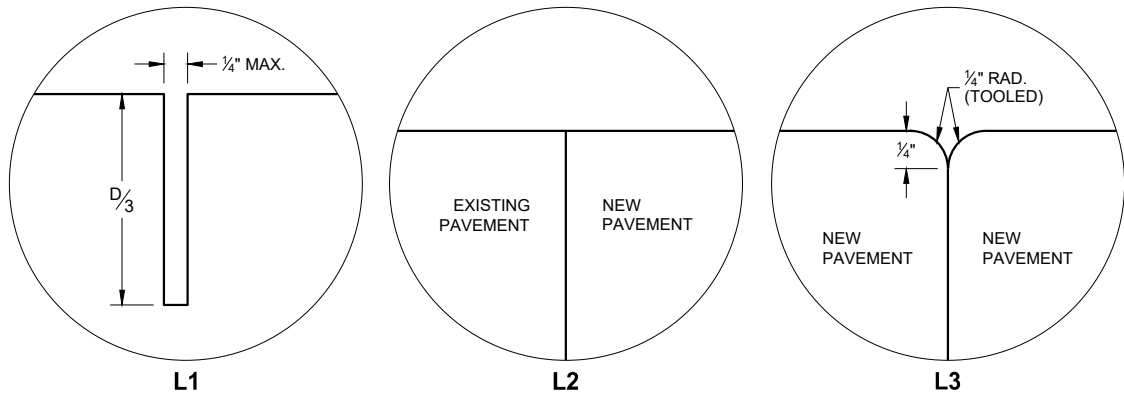
HMA PATCH REMOVAL

BASE PATCHING CONCRETE

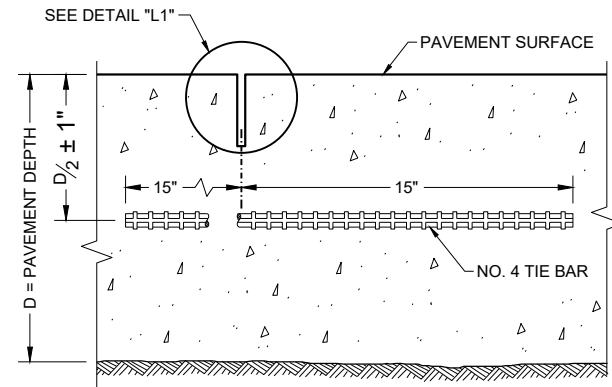
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



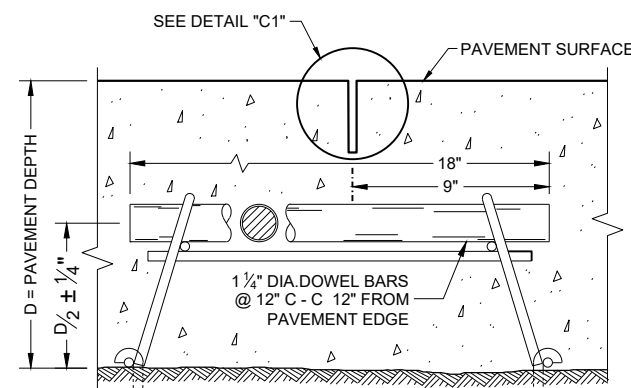
TRANSVERSE JOINTS



LONGITUDINAL JOINTS



**SECTION C - C
SAWED LONGITUDINAL JOINT**



**SECTION F - F
CONTRACTION JOINT**

GENERAL NOTES

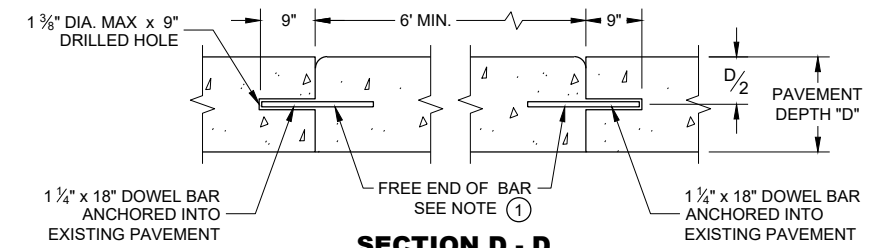
INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.

CONCRETE BASE PATCHES OF EXISTING NON-DOWELED CONCRETE PAVEMENTS DO NOT NEED TO BE DOWELED.

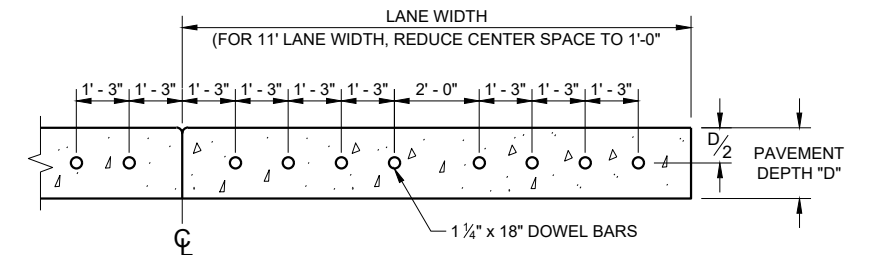
ANCHOR DOWEL BARS AND TIE BARS INTO DRILLED HOLES WITH AN EPOXY.

PROVIDE A MINIMUM DISTANCE OF 15 INCHES FROM ALL TRANSVERSE JOINTS OR EDGES OF REPLACEMENT TO THE CENTER OF THE TIE BAR NEAREST THAT JOINT OR EDGE.

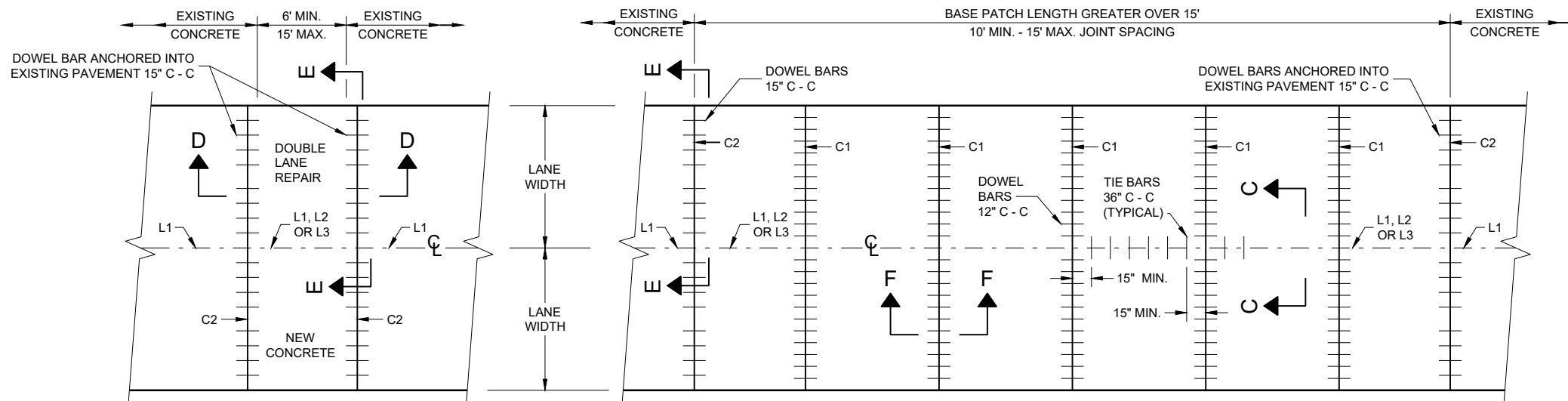
- ① APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.



SECTION D - D



**SECTION E - E
SPACING OF DOWEL BARS
ANCHORED INTO EXISTING PAVEMENT**

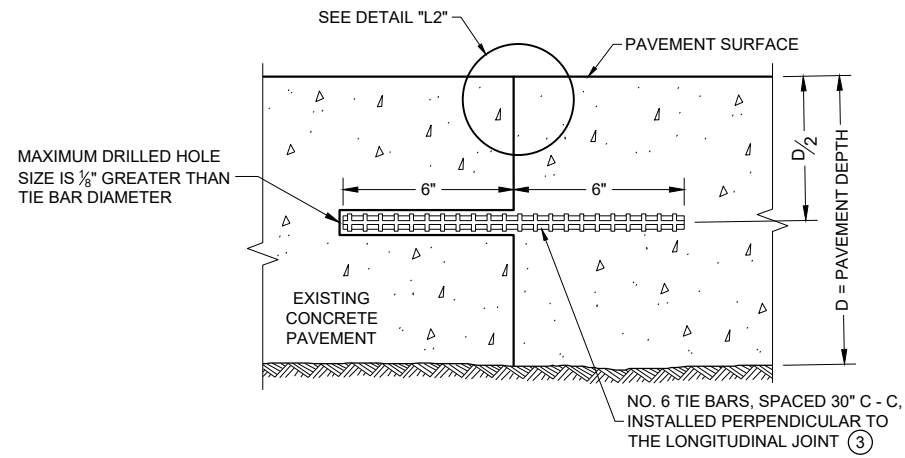


**PLAN VIEW
MULTILANE CONCRETE BASE PATCH
15' MAXIMUM LENGTH**

**PLAN VIEW
MULTILANE CONCRETE BASE PATCH
GREATER THAN 15' IN LENGTH**

BASE PATCHING CONCRETE

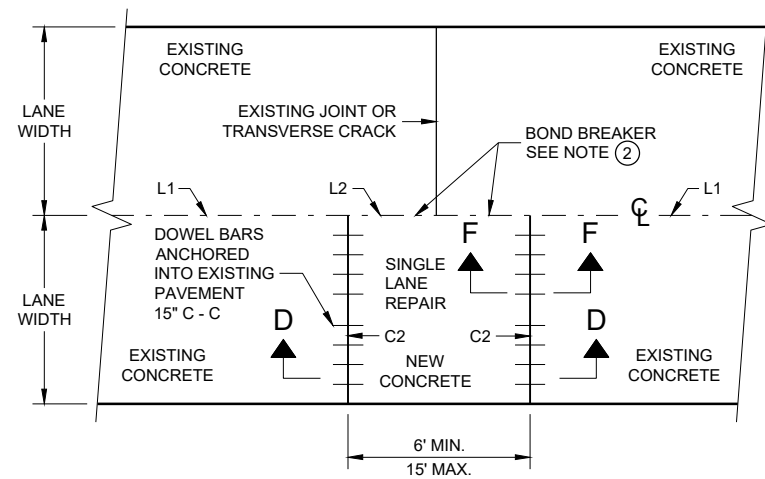
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



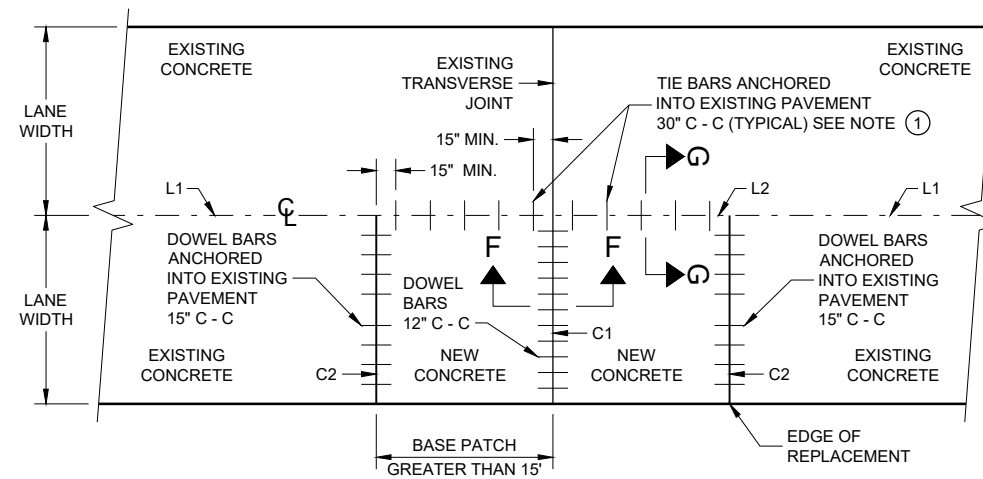
SECTION G - G
TIE BARS ANCHORED INTO EXISTING PAVEMENT

GENERAL NOTES

- ① WITH THE APPROVAL OF THE ENGINEER, FOR SINGLE LANE PAVEMENT REPLACEMENTS LESS THAN 30 FEET IN LENGTH, THE CONTRACTOR MAY INSTALL DRILLED TIE BARS ON 6:1 SKEW HORIZONTALLY, DIRECTION OF SKEW ALTERNATING WITH EACH SUCCESSIVE BAR. DRIVE SKEWED TIE BARS TO A DEPTH OF 6 INCHES IN A HOLE OF SUCH A DIAMETER AS TO PROVIDE A TIGHT DRIVEN FIT.
- ② USE AN ENGINEER APPROVED BOND BREAKER (E.G. RELEASE AGENT, CURING COMPOUND) FOR SINGLE LANE REPAIRS UP TO 15 FEET IN LENGTH.
- ③ ANCHOR TIE BARS INTO DRILLED HOES WITH AN EPOXY.



PLAN VIEW
SINGLE LANE CONCRETE BASE PATCH
15' MAXIMUM LENGTH



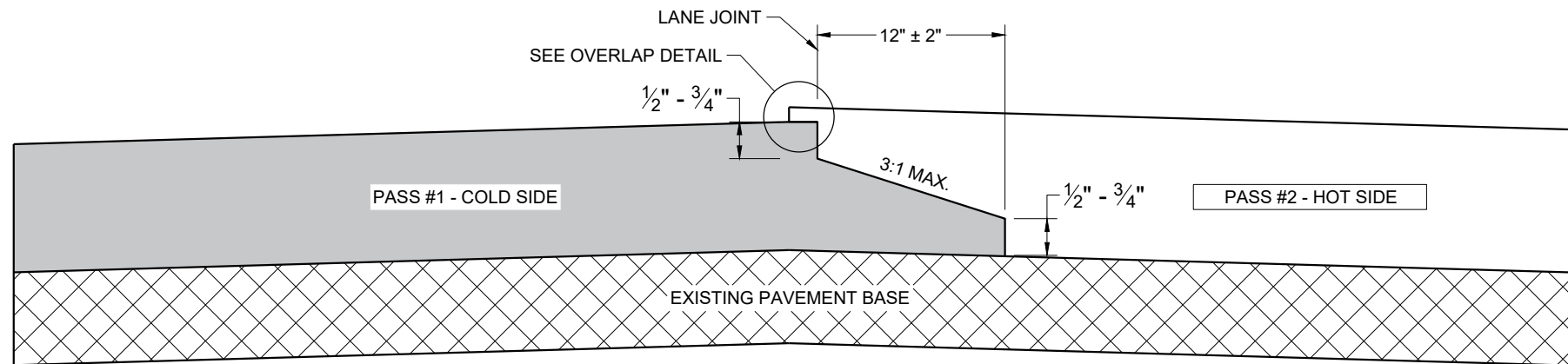
PLAN VIEW
SINGLE LANE CONCRETE BASE PATCH
GREATER THAN 15' LENGTH

BASE PATCHING CONCRETE

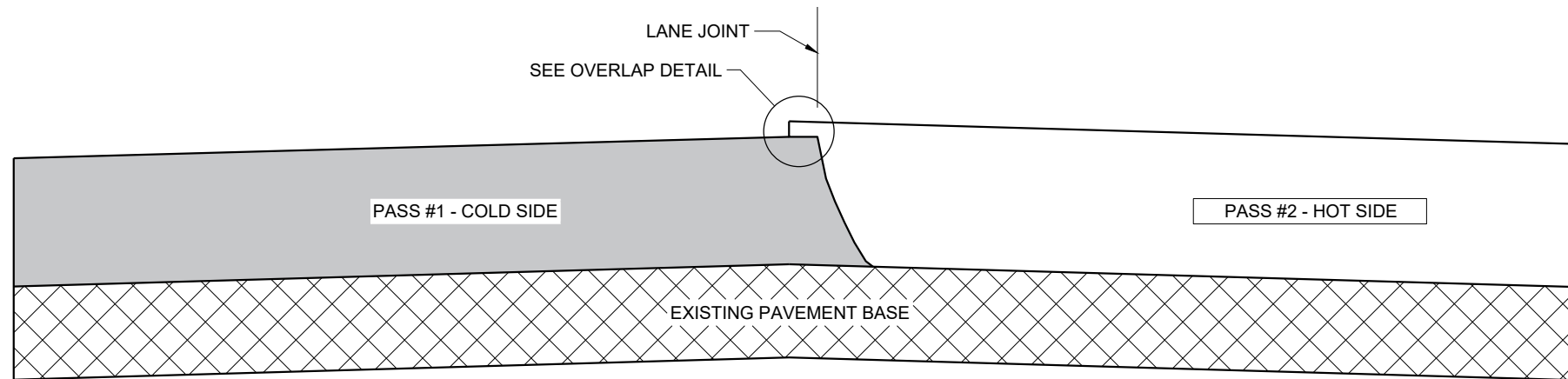
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018 /S/ Peter Kemp, P.E.
DATE PAVEMENT SUPERVISOR

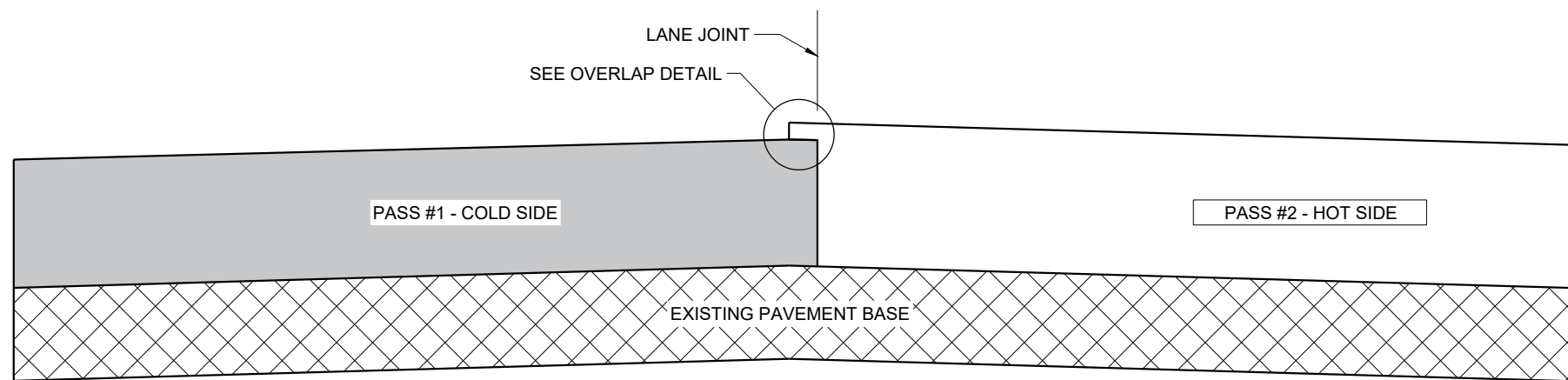
FHWA



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

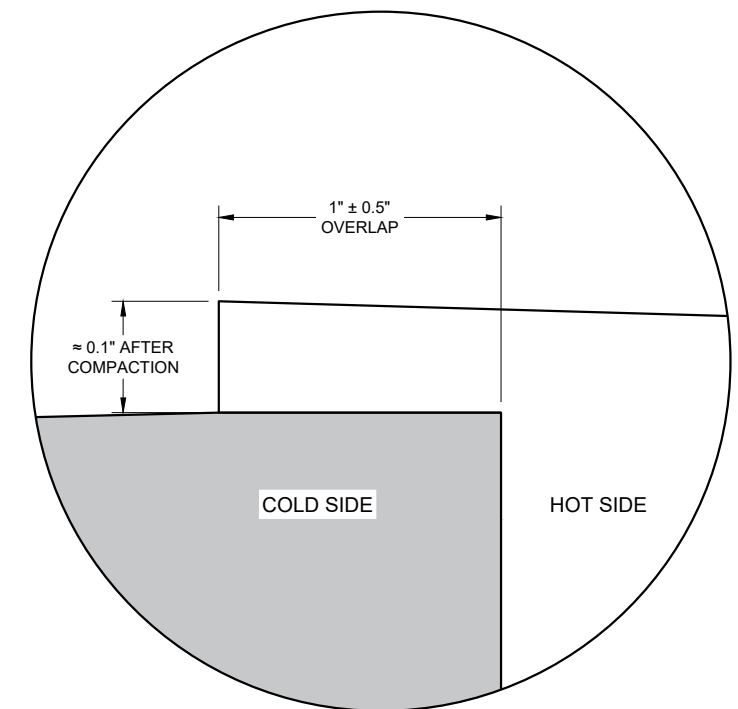
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

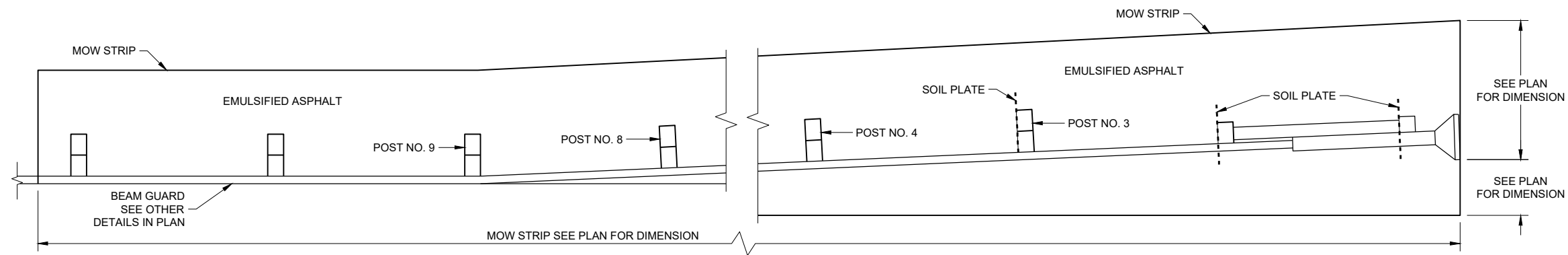
6

6

SDD 13C19 - 03

SDD 13C19 - 03

HMA LONGITUDINAL JOINTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	

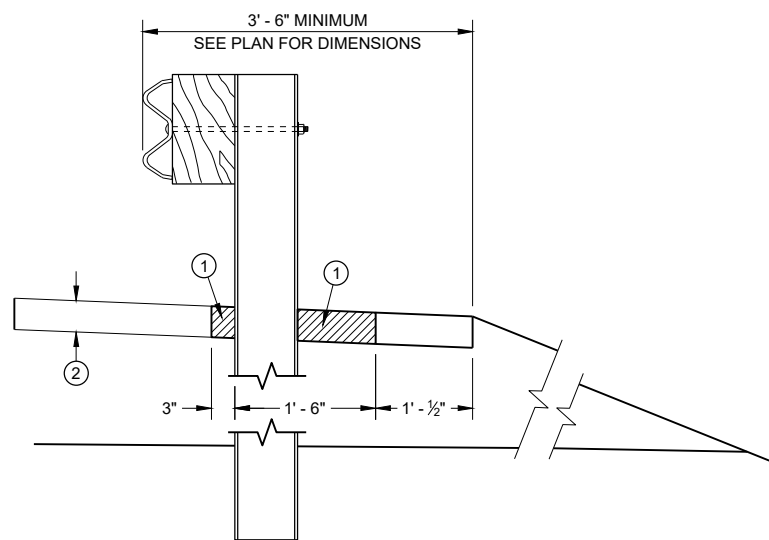


PLAN VIEW
MOW STRIP LAYOUT FOR ENERGY ABSORBING TERMINAL

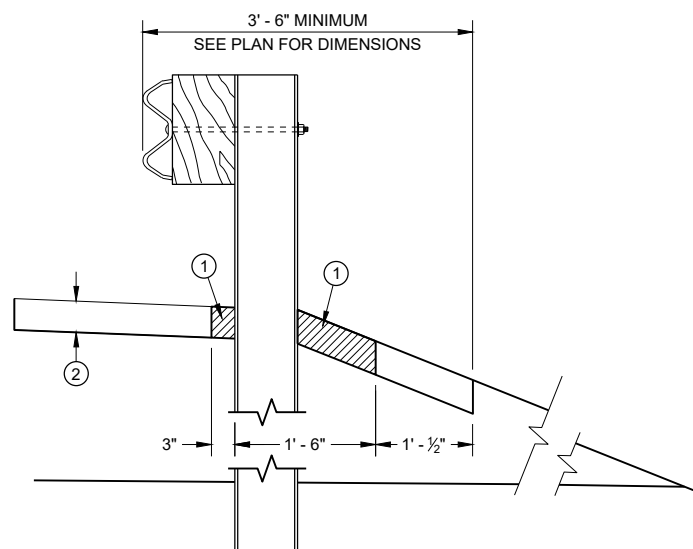
GENERAL NOTES

ONLY USE STEEL POSTS IN CONCRETE AND ASPHALT MOW STRIPS.

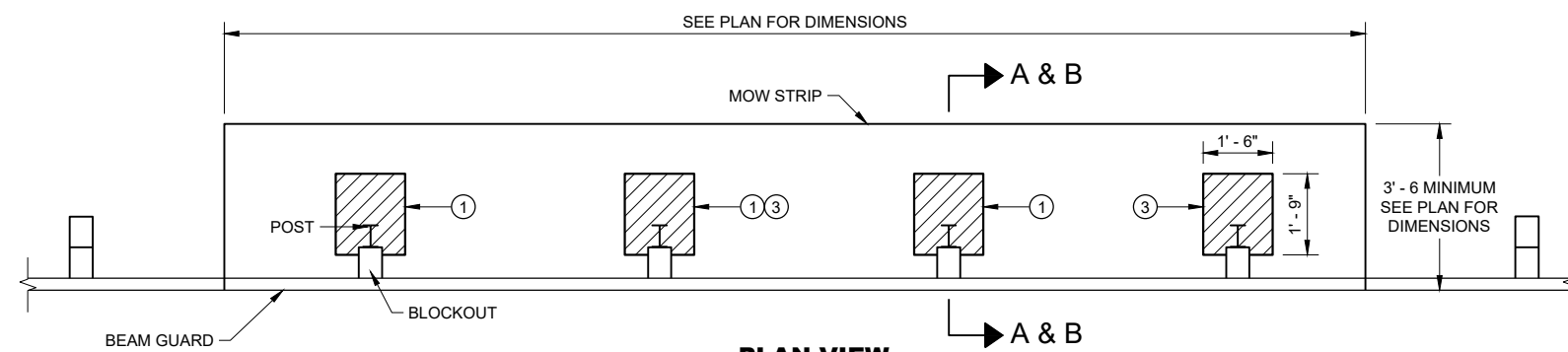
- ① CONTROLLED LOW-STRENGTH BACKFILL OR EMULSIFIED ASPHALT.
- ② DEPTH OF MOW STRIP:
ASPHALT - 4"
CONCRETE - 4"
EMULSIFIED ASPHALT - 1" OR LESS
- ③ FOR EMULSIFIED ASPHALT, MOW STRIP STRIP LEAVE OUTS NOT REQUIRED. (TYPICAL FOR ALL POSTS)



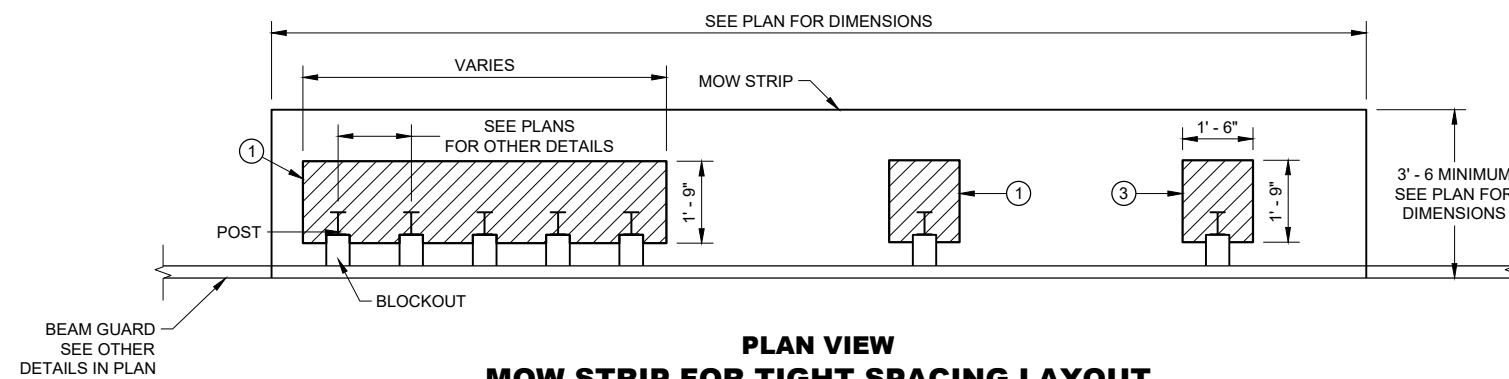
SECTION A - A



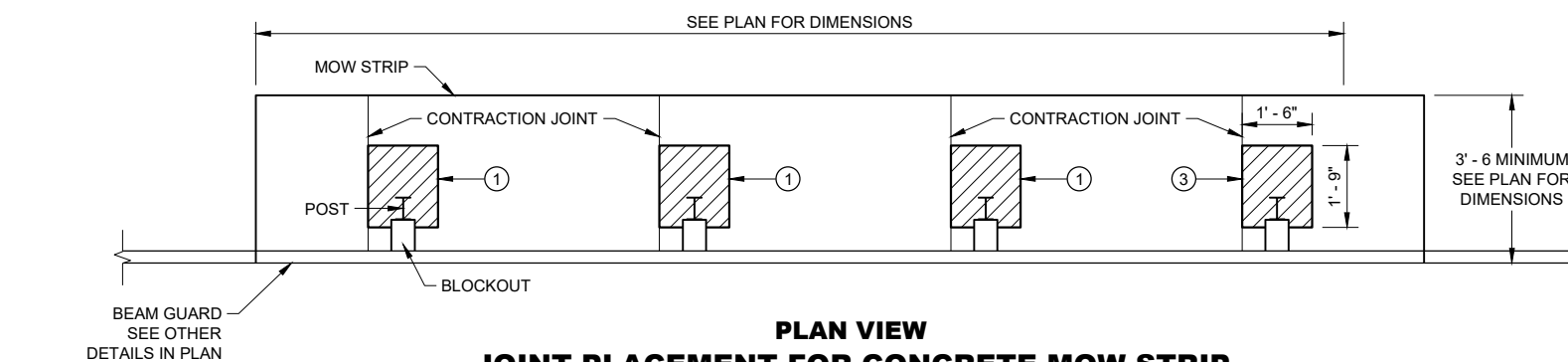
SECTION B - B



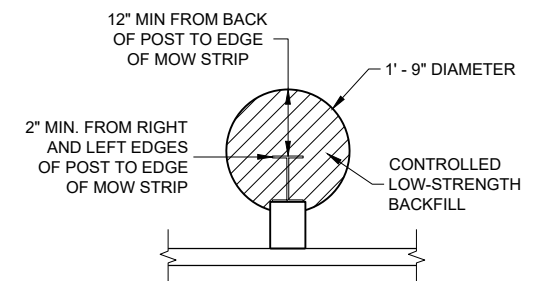
PLAN VIEW
MOW STRIP FOR TYPICAL BLOCKOUT LAYOUT



PLAN VIEW
MOW STRIP FOR TIGHT SPACING LAYOUT



PLAN VIEW
JOINT PLACEMENT FOR CONCRETE MOW STRIP



ALTERNATIVE HMA
MOW STRIP DESIGN

GUARDRAIL MOW STRIP

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

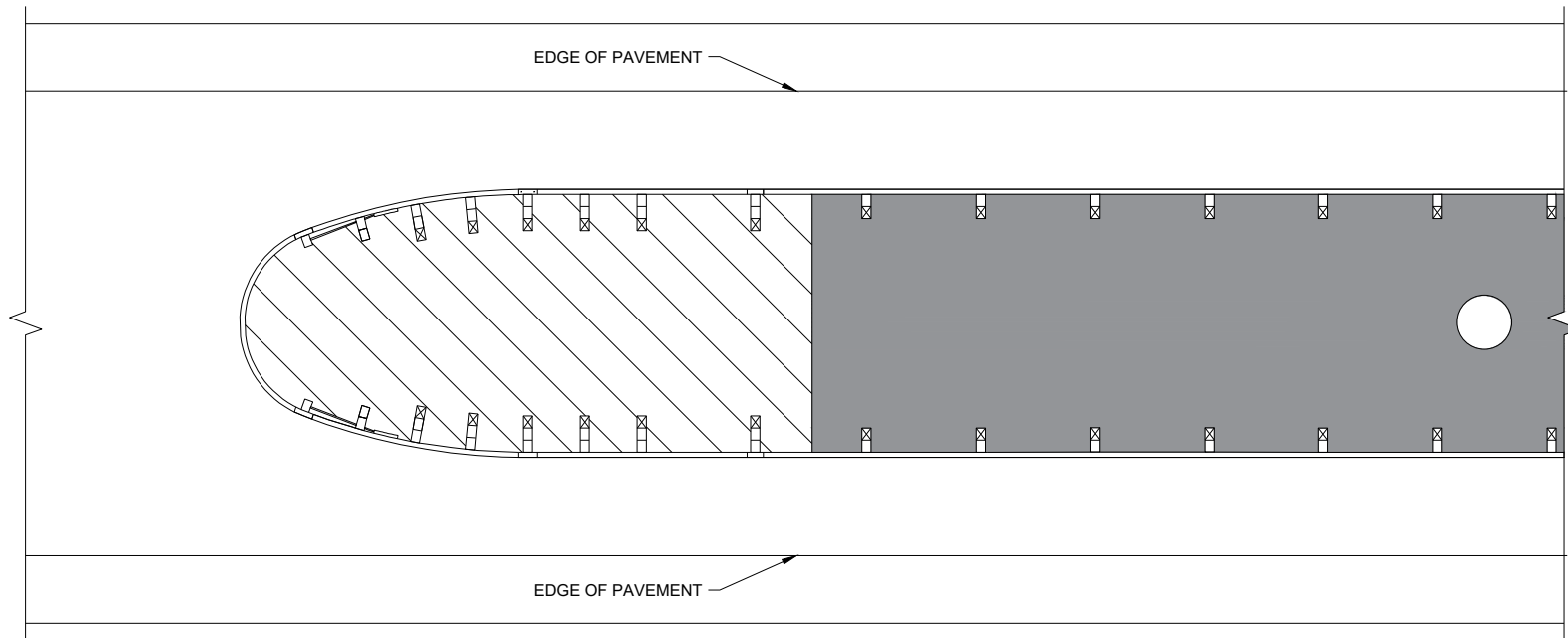
LEGEND

 CONCRETE, ASPHALT, OR EMULSIFIED ASPHALT MOW STRIP (SEE OTHER DETAILS)

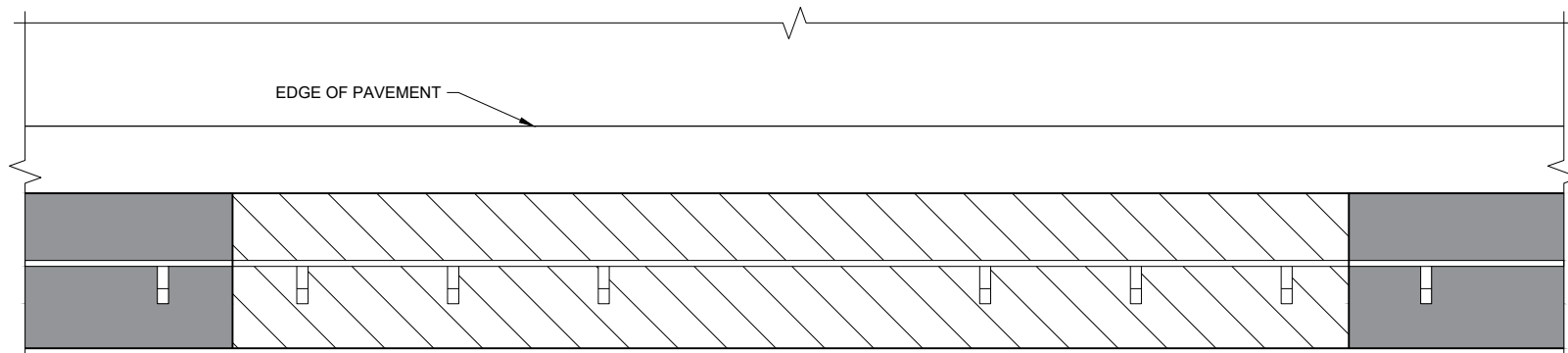
 EMULSIFIED ASPHALT MOW STRIP (SEE OTHER DETAILS)

GENERAL NOTES

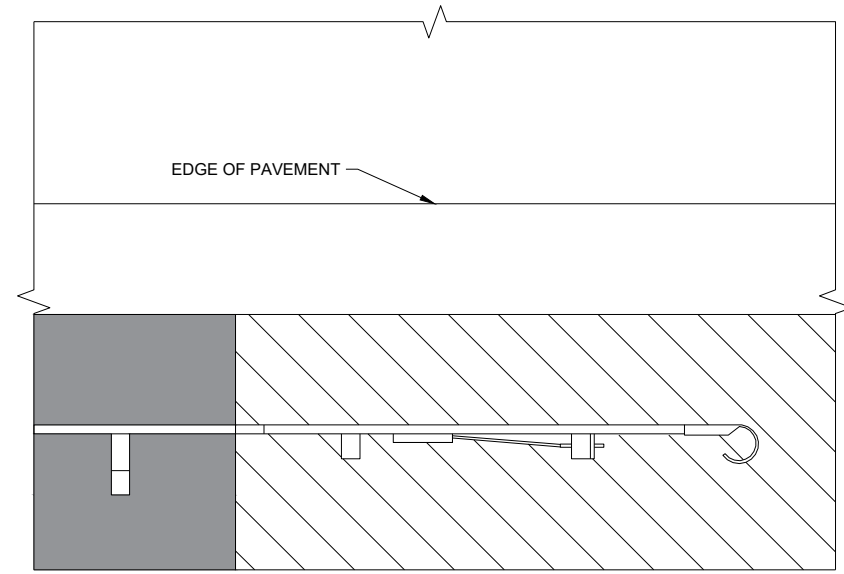
EXISTING THRIE BEAM BULLNOSES MAY HAVE WOOD POSTS. NEW THRIE BEAM BULLNOSE WILL HAVE STEEL POSTS.



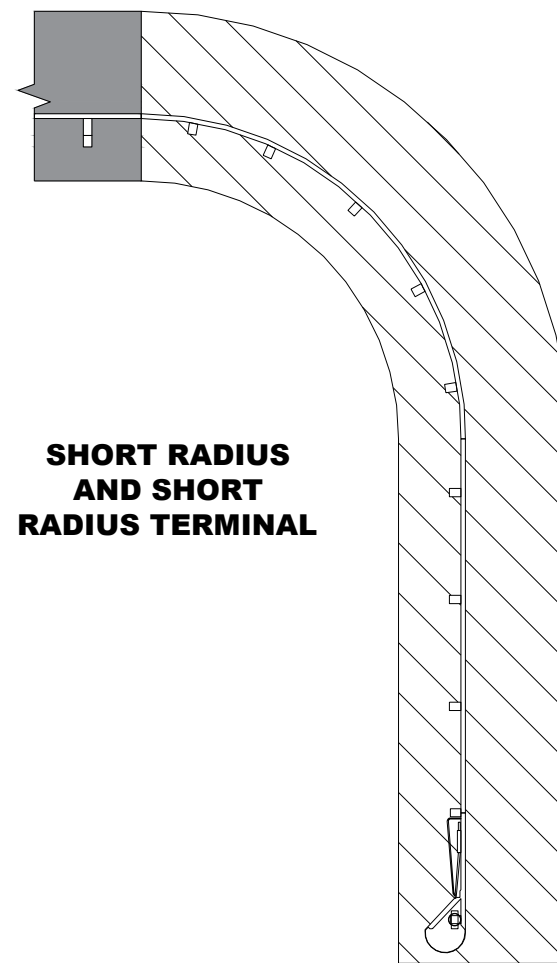
THRIE BEAM BULLNOSE



LONG - SPAN



TYPE 2 TERMINAL



**SHORT RADIUS
AND SHORT
RADIUS TERMINAL**

6

6

SDD 14B28 - 04b

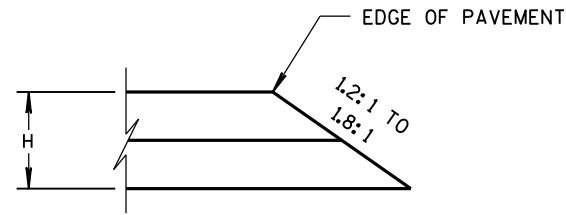
SDD 14B28 - 04b

GUARDRAIL MOW STRIP

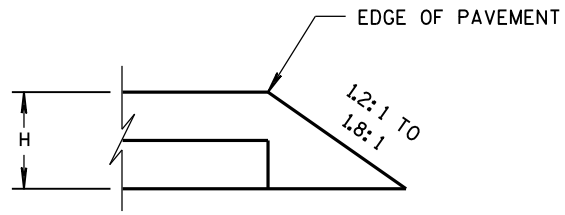
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
August 2020 DATE /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
ENGINEER

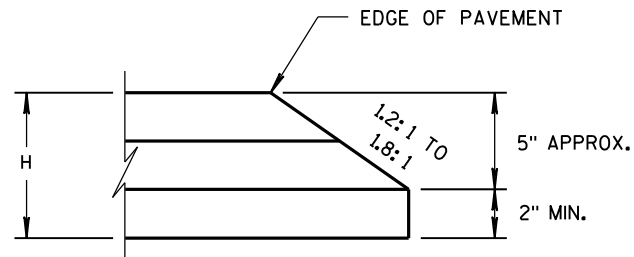
FHWA



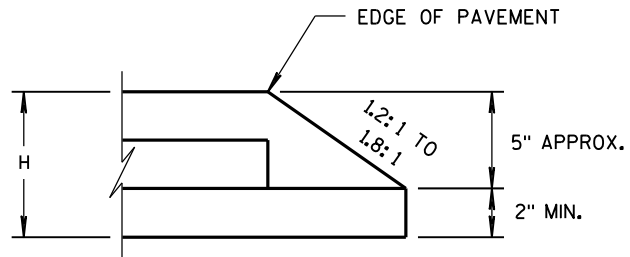
CONSTRUCTED WITH FINAL TWO LAYERS
FOR H 5" OR LESS



CONSTRUCTED WITH FINAL LAYER
FOR H 5" OR LESS

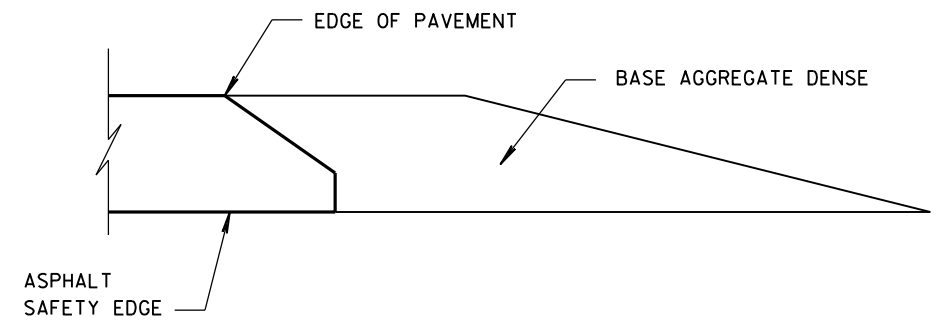


CONSTRUCTED WITH FINAL TWO LAYERS
FOR H GREATER THAN 5"



CONSTRUCTED WITH FINAL LAYER
FOR H GREATER THAN 5"

HMA PAVEMENT AND HMA OVERLAYS



FINISHED SHOULDER AGGREGATE PLACEMENT

6

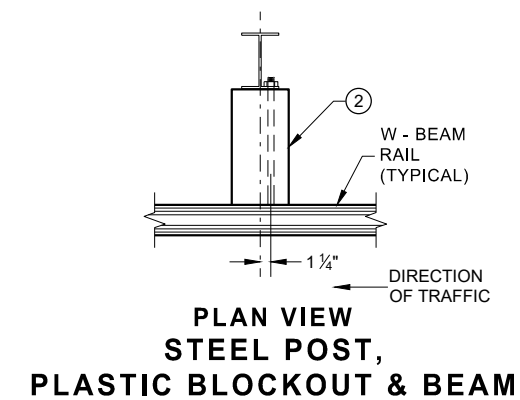
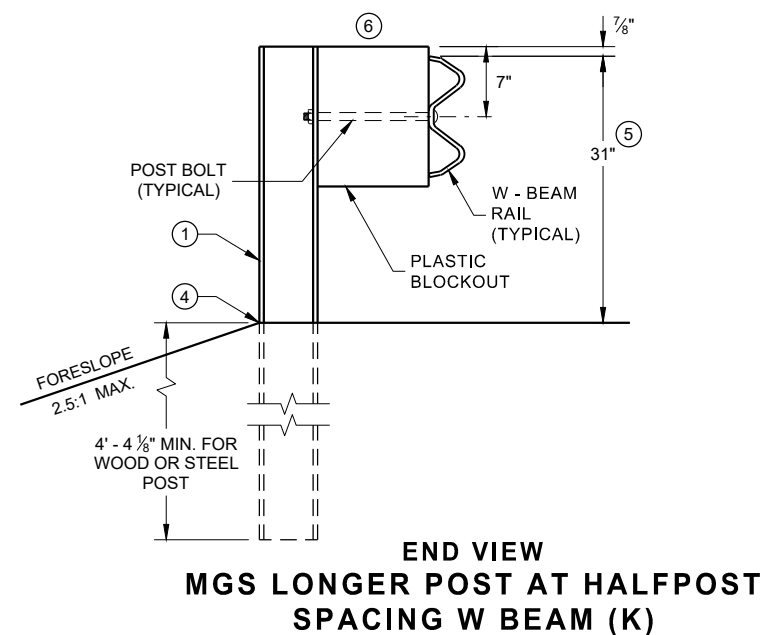
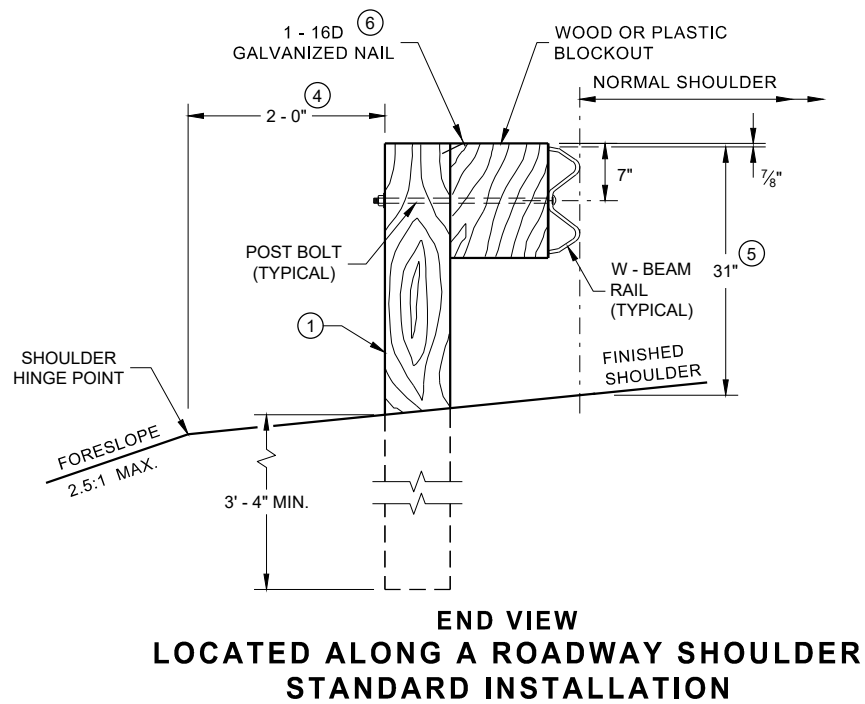
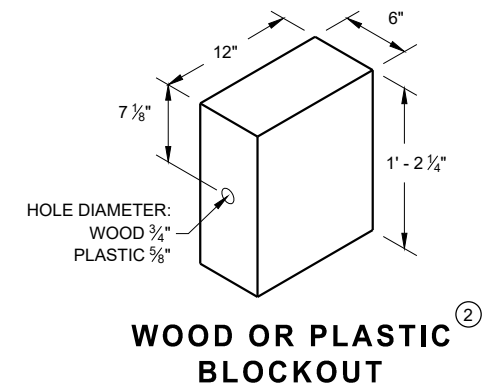
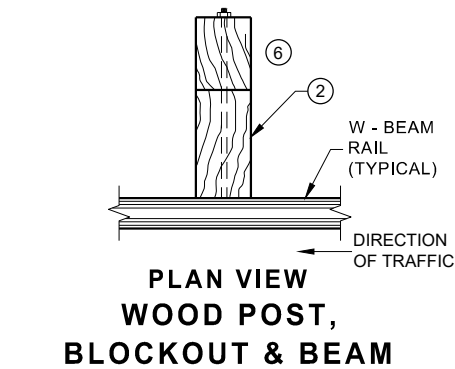
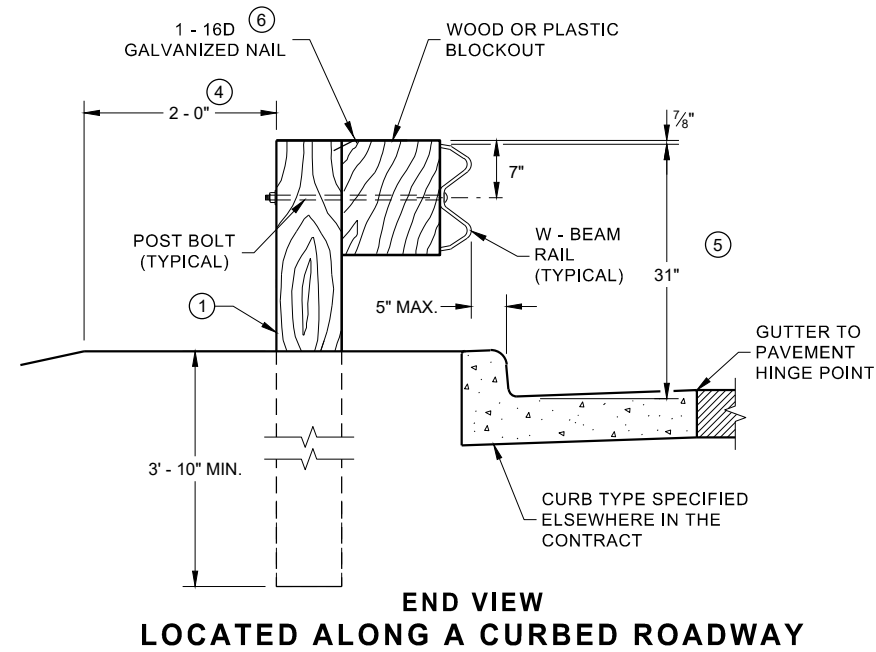
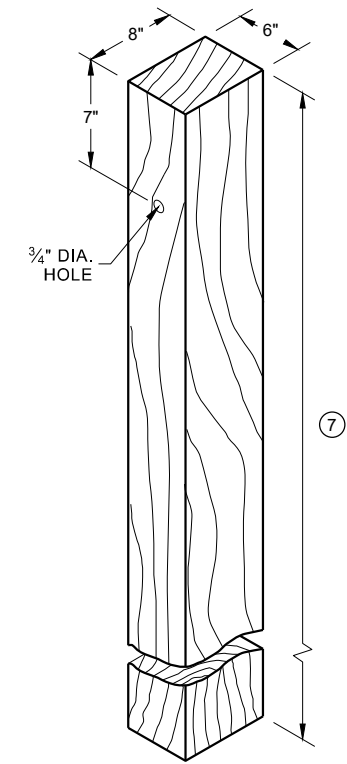
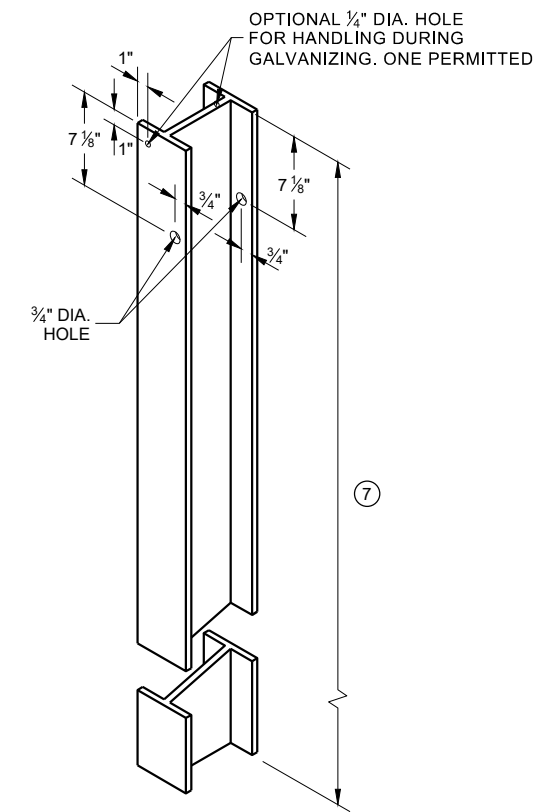
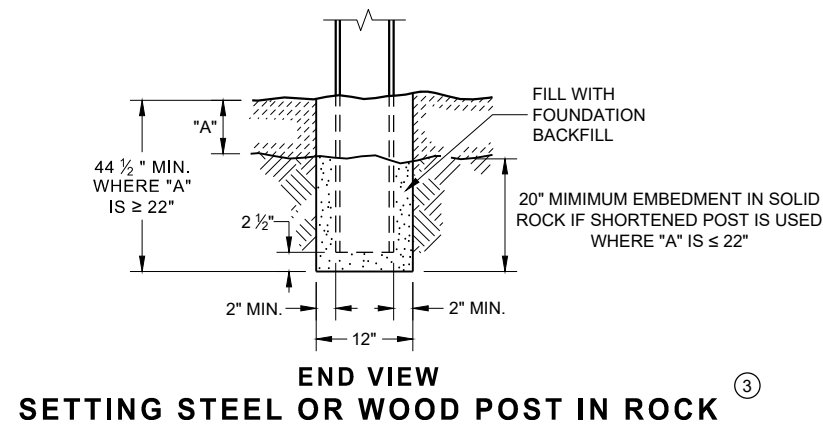
6

S.D.D. 14 B 29-1

S.D.D. 14 B 29-1

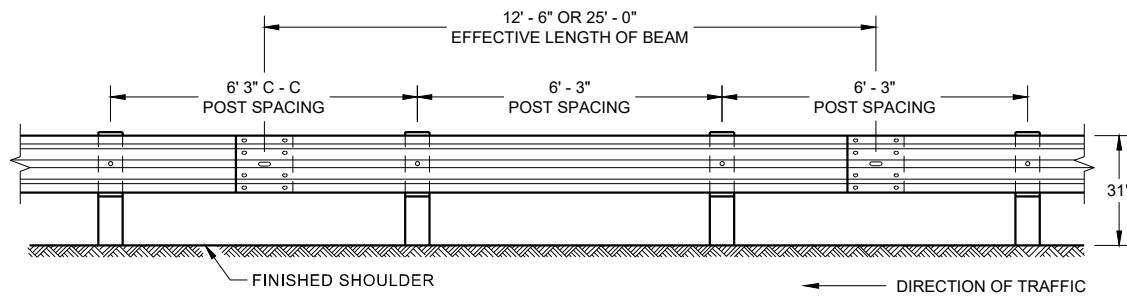
SAFETY EDGE _{SM}	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE	/s/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	

- ① WOOD OR STEEL POSTS (w6X9 OR w6X8.5) MAY BE USED. DO NOT INTERMIX WOOD AND STEEL POSTS. INSTALL STEEL POSTS WITH HOLES ON APPROACHING TRAFFIC SIDE.
- ② USE WOOD OR APPROVED PLASTIC BLOCKOUTS. WOOD BLOCKOUTS MAY BE CONSTRUCTED OUT OF TWO OR MORE WOOD BLOCKOUTS. SEE ALTERNATE WOOD BLOCKOUT DETAIL. DIMENSIONS OF APPROVED PLASTIC BLOCKOUTS MAY VARY.
- ③ IF ROCK IS ENCOUNTERED DURING EXCAVATION, PROVIDE A HOLE 12 INCHES IN DIAMETER EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE APPROXIMATELY 2 1/2" INCHES OF GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE. CUT THE POSTS THE TO LENGTH AND INSTALL. BACKFILL WITH EXCAVATED MATERIAL AND COMPACT. BACKFILL IS TO BE FREE OF LARGE ROCKS.
- ④ WHEN THE DISTANCE FROM BACK OF POST TO SHOULDER HINGE POINT IS LESS THAN 2 FEET INSTALL LONGER POST AT HALF POST SPACING (K).
- ⑤ FOR NEW MGS INSTALLATION TOP OF W-BEAM RAIL TOLERANCE IS $\pm 1"$. FOR EXISTING MGS INSTALLATION TOP OF W-BEAM IS BETWEEN 27 3/4" TO 32".
- ⑥ WHEN USING STEEL POST AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.
- ⑦ TOTAL POST LENGTH FOR TYPE K IS 7' - 0". TOTAL POST LENGTH FOR OTHER MGS TYPES IS 6' - 0".

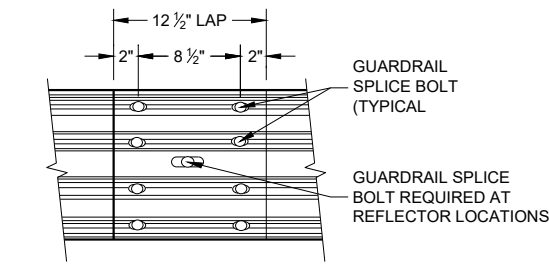


**MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



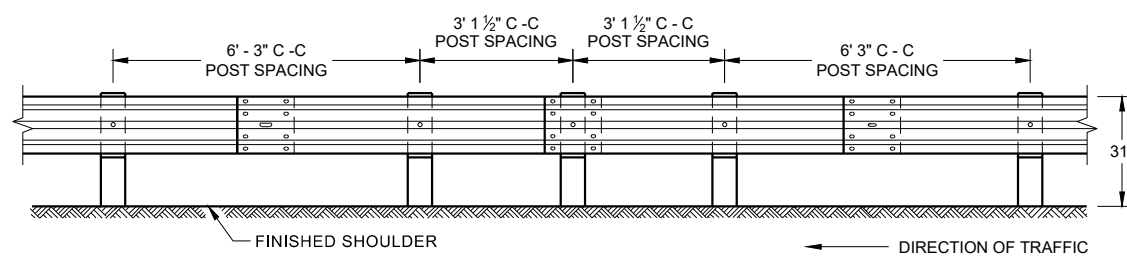
**FRONT VIEW
POST SPACING STANDARD INSTALLATION**



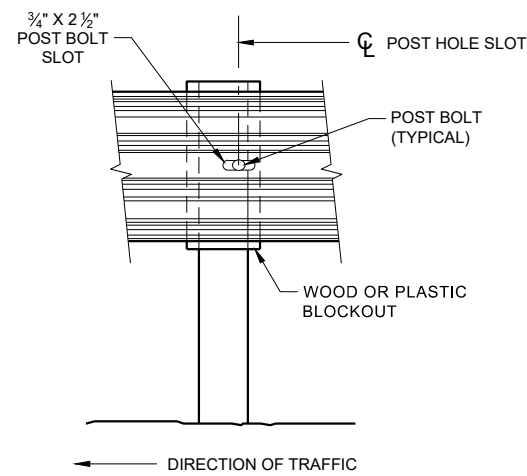
**FRONT VIEW
MID-SPAN BEAM SPLICE**

GENERAL NOTES

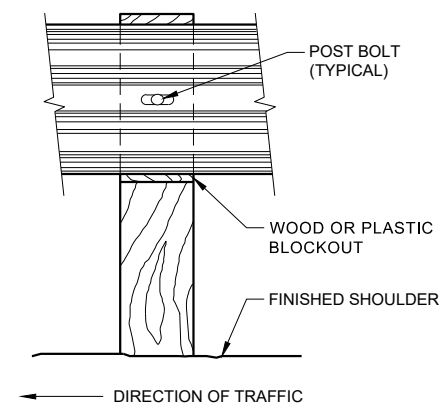
- ⑧ DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.
 - ⑨ 25 FEET OF HALF POST SPACING IS REQUIRED ON APPROACH AND DEPARTURE ENDS OF QUARTER POST SPACING.
- POST BOLTS ARE A 3/8" DIAMETER ASTM A307 GUARDRAIL BOLT. A POST BOLT REQUIRES 3/4" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT AND 3/8" DIAMETER F844 FLAT WASHER. POST BOLTS MAY BE LONGER IF MULTIPLE BLOCKOUTS ARE BEING USED.
- GUARD RAIL SPLICE BOLTS ARE A 3/8" DIAMETER ASTM A307 GUARDRAIL HEAD BOLT. A GUARDRAIL SPLICE BOLT REQUIRES 3/8" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT.



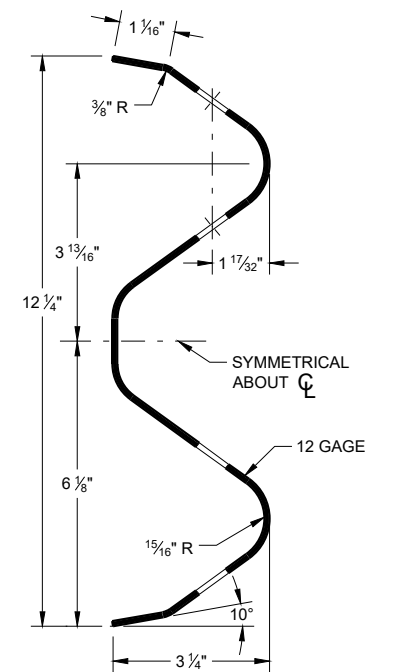
**FRONT VIEW
HALF POST SPACING (HS) AND
HALF POST SPACING WITH LONGER POSTS (K)**



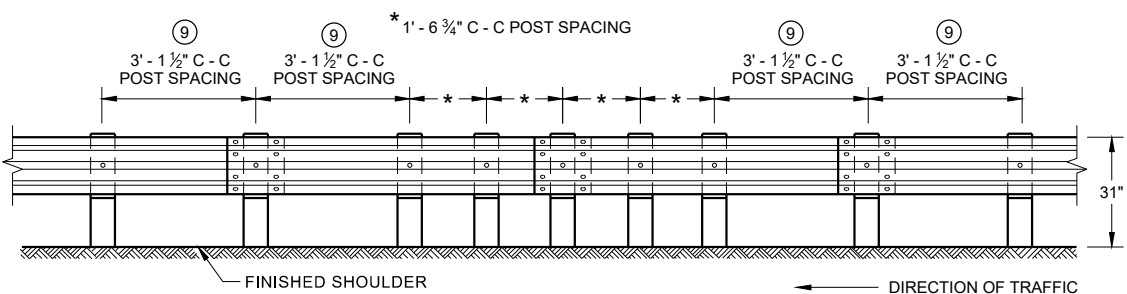
FRONT VIEW AT STEEL POST



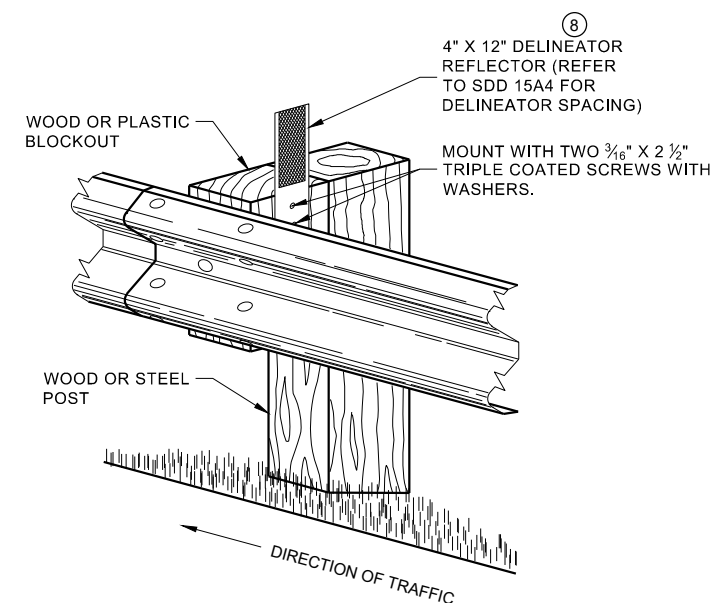
FRONT VIEW AT WOOD POST



SECTION THRU W-BEAM RAIL



**FRONT VIEW
QUARTER POST SPACING (QS)**



**ONE SIDED REFLECTOR DETAIL
AND TYPICAL INSTALLATION**

**MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL**

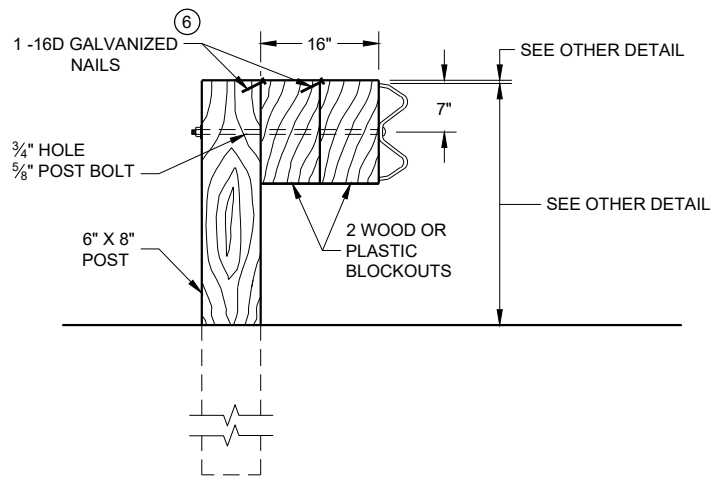
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

6

SDD 14B42 - 07b

SDD 14B42 - 07b

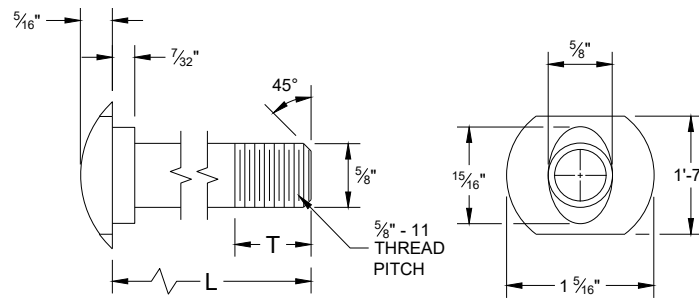


DETAIL FOR 16" BLOCKOUT DEPTH

IT IS ACCEPTABLE TO USE BLOCKOUTS UP TO 16" DEEP TO INCREASE THE POST OFFSET TO AVOID UNDERGROUND OBSTACLES. THERE IS NO LIMIT TO THE NUMBER OF POSTS THAT CAN HAVE ADDITIONAL BLOCKOUTS UP TO 16" DEEP.

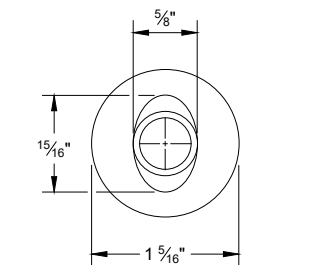
NOTE:

1. ALL FILLETS SHALL HAVE A MINIMUM RADIUS OF 3/16".
2. IF THE BOLT EXTENDS MORE THAN 1/4" FROM THE NUT THE BOLT SHOULD BE TRIMMED BACK.

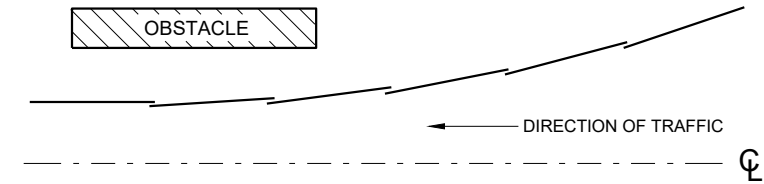


POST BOLT TABLE

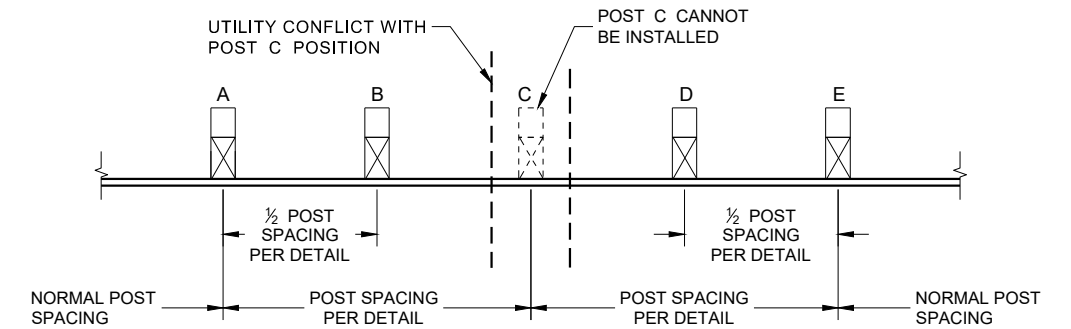
L	T (MIN.)
1 1/4"	1 1/8"
2"	1 3/4"
10"	4"
14"	4 1/16"
18"	4"
21"	4 1/16"
25"	4"



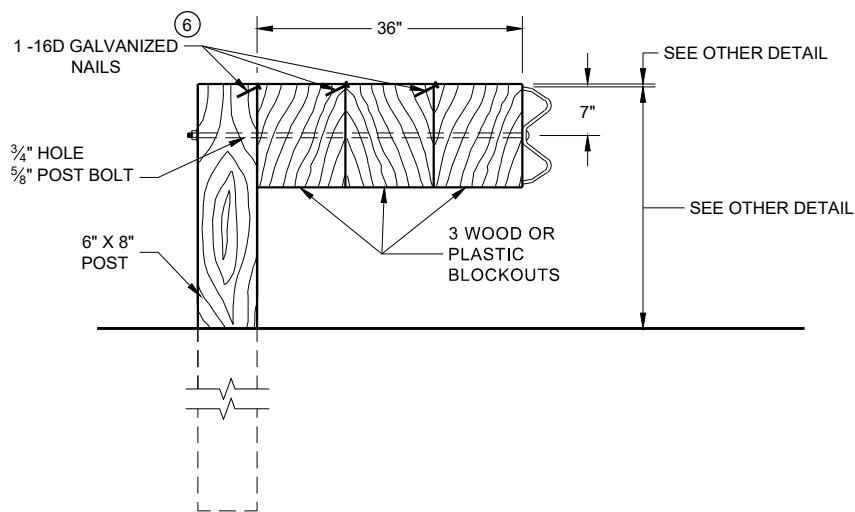
ALTERNATE BOLT HEAD



**PLAN VIEW
BEAM LAPPING DETAIL**

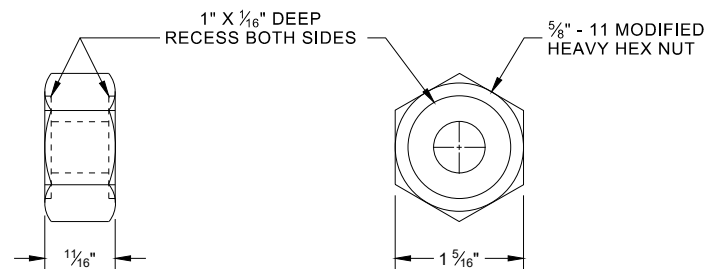


**POST DRIVING FOR CONTINUOUS
UNDERGROUND OBSTRUCTION**

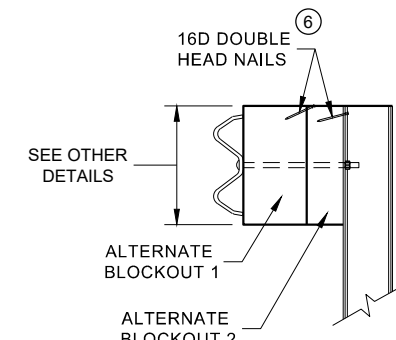


DETAIL FOR 36" BLOCKOUT DEPTH

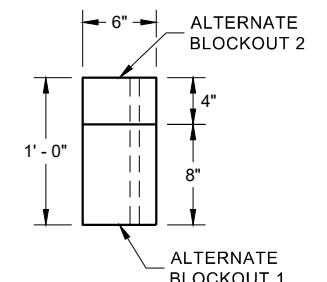
NOTES: UNDER SPECIAL CIRCUMSTANCES, SUCH AS AVOIDING OBSTACLES THAT ARE NOT RELOCATED, IT IS ACCEPTABLE TO INSTALL ADDITIONAL BLOCKOUTS TO OBTAIN UP TO 36" DEPTH FOR ONE OR TWO POSTS IN A SECTION OF GUARDRAIL.
DO NOT USE 16" OR 36" BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.



**POST BOLT, SPLICE BOLT
AND RECESS NUT**



SIDE VIEW



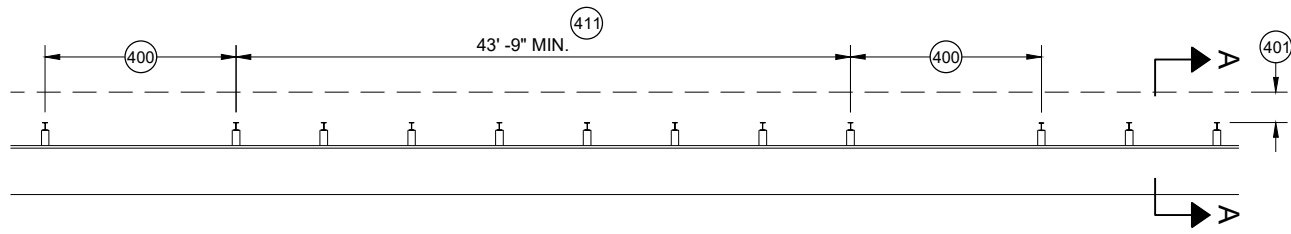
PLAN VIEW

**ALTERNATE WOOD
BLOCKOUT DETAIL**

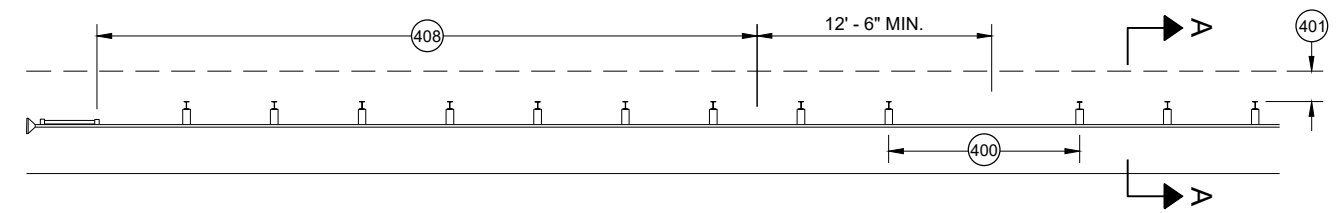
6 WHEN USING STEEL POST AND WOOD BLOCKOUTS, INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

**MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL**

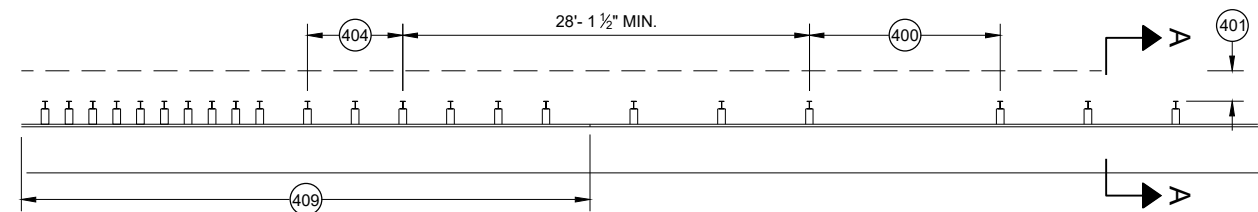
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



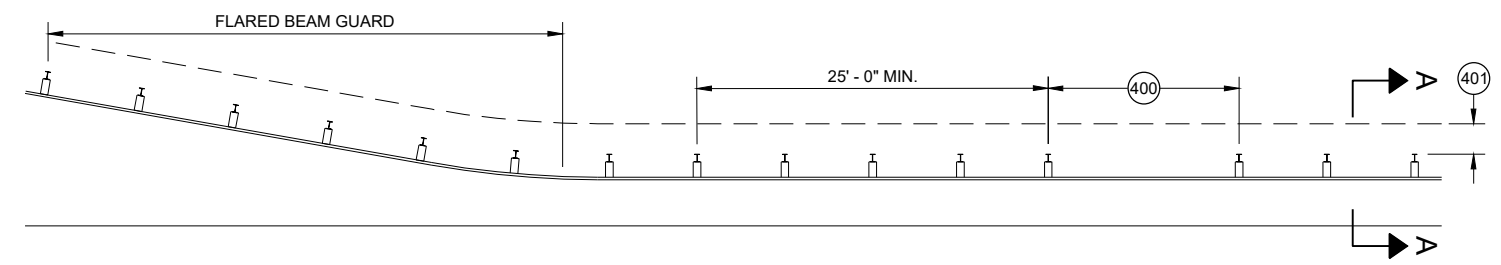
MISSING POST IN MGS GUARDRAIL



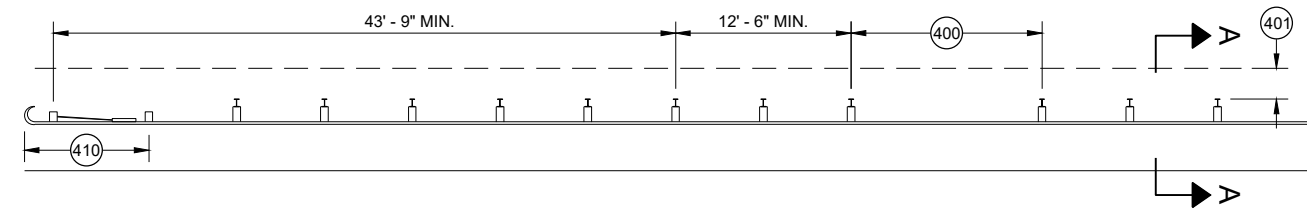
MISSING POST IN MGS GUARDRAIL NEAR EAT



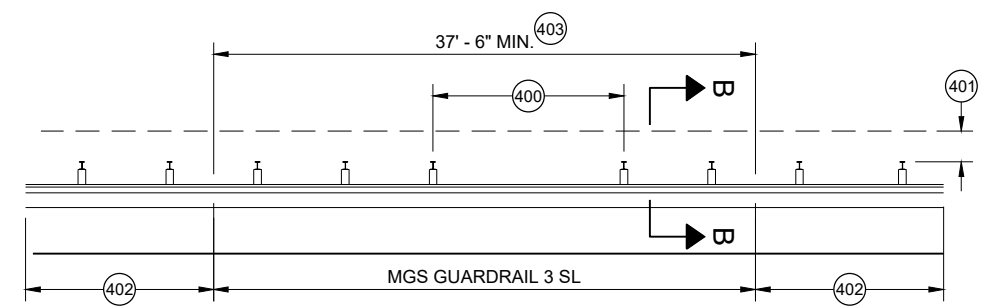
MISSING POST IN MGS GUARDRAIL NEAR AN APPROACH TRANSITION



MISSING POST IN MGS GUARDRAIL NEAR FLARED BEAM GUARD

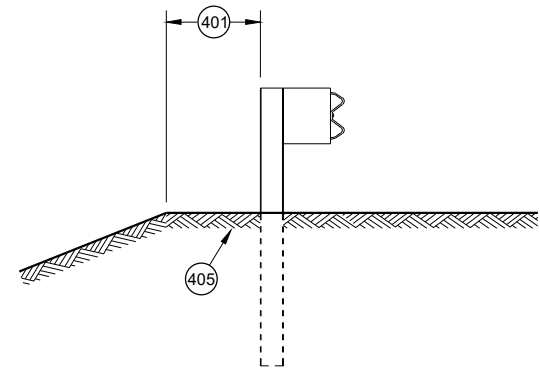


MISSING POST IN MGS GUARDRAIL NEAR A TYPE 2 END TERMINAL

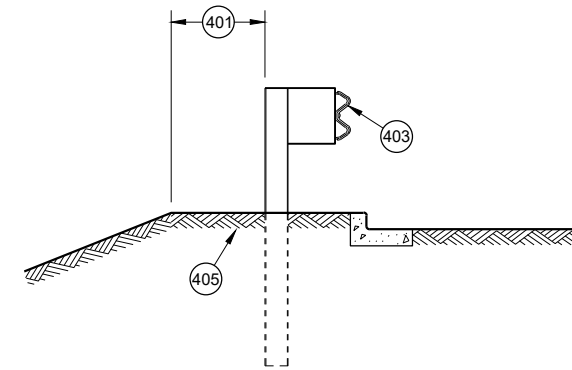


MISSING POST IN SHORT SPAN MGS GUARDRAIL NEAR CURB (SL)

- ④00 MAX SPAN 12' - 6"
- ④01 2' MIN.
- ④02 MGS GUARDRAIL 3
- ④03 NESTING BEAM GUARD
- ④04 ASYMMETRIC TRANSITION
- ④05 SOIL WELL DRAINED AND COMPACTED
- ④06 SEE OTHER DRAWINGS IN THIS SDD
- ④07 SEE OTHER DRAWINGS FOR MIN. SPACING BETWEEN SPANS
- ④08 SEE SDD 14B44
- ④09 SEE SDD 14B45
- ④10 SEE SDD 14B47
- ④11 MINIMUM DISTANCE BETWEEN MISSING POST SPANS.



SECTION A - A



SECTION B - B

MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2021 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
<small>FHWA</small>	

GENERAL NOTES

- (A) THE SLOPE IN THE AREA BOUNDED BY THE GRADELINE, THE HINGE POINT LINE AND THE CLEAR ZONE LIMITS (CZL) SHALL BE 4:1 OR FLATTER.
 - (B) AFTER FINAL ASSEMBLY, RECHECK CABLE TO BE SURE IT IS TAUT AND HAS NOT RELAXED
 - (C) DIFFERENT MANUFACTURERS REQUIRE DIFFERENT PERFORATED W - BEAM RAIL END PANELS. SEE MANUFACTURER'S INFORMATION.
 - (D) ATTACH ALUMINUM SHEET TO E.A.T. HEAD USING 4 STAINLESS STEEL SELF - TAPPING SCREWS. ONE SCREW PER CORNER.
 - (E) HARDWARE MAY VARY BETWEEN MANUFACTURER. SEE MANUFACTURER'S DRAWING FOR INFORMATION.
- DIMENSIONS MAY VARY, MANUFACTURER'S INFORMATION.

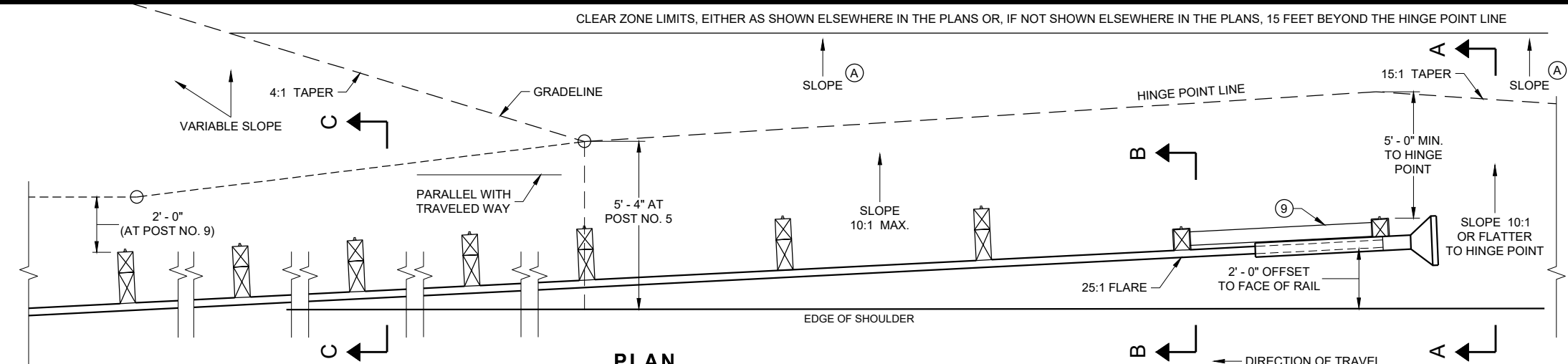
SEE SDD 14B42 FOR MORE INFORMATION.

* DO NOT ATTACH BLOCKOUTS TO POST 1 AND 2.

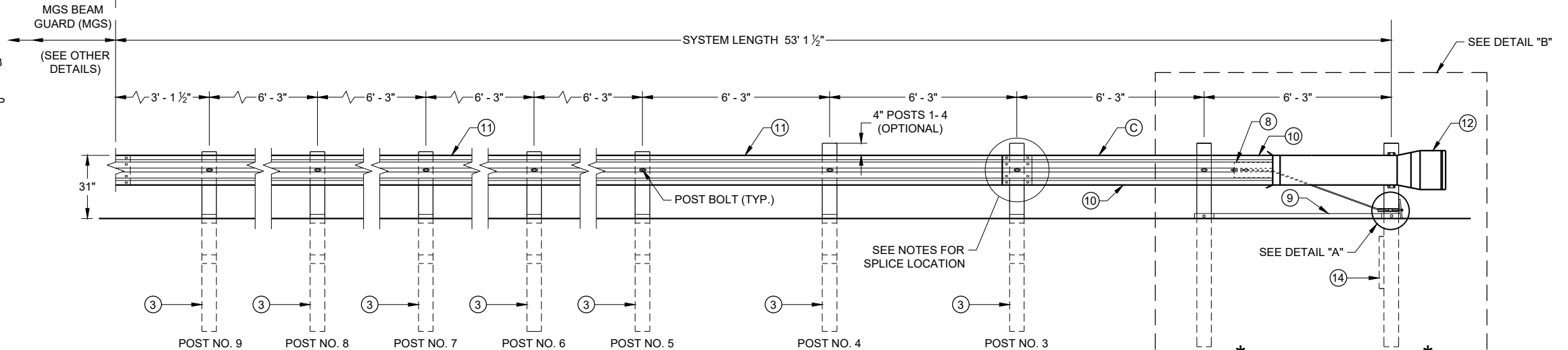
DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.

SEE MANUFACTURER'S DRAWING FOR SPLICE LOCATION, HARDWARE DIMENSIONS AND INSTALLATION INSTRUCTIONS.

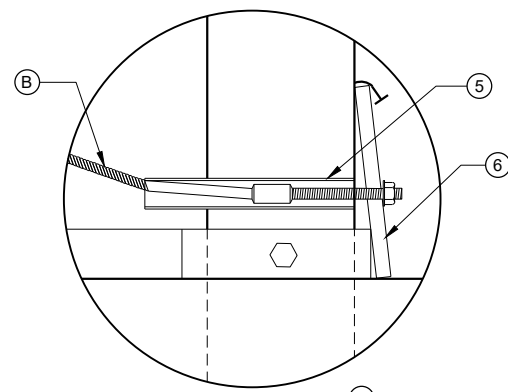
THE CENTER OF THE UPPER 3 1/2" DIAMETER HOLE ON POST NUMBER 3 THROUGH POST 9 IS TO BE FLUSH WITH THE GROUND LINE UP TO A MAXIMUM OF 2" ABOVE GROUND LINE. WOOD BLOCKS ON POSTS NUMBERED 3 THROUGH 9 MAY BE ADJUSTED UP TO 3" ABOVE THE TOP OF POST.



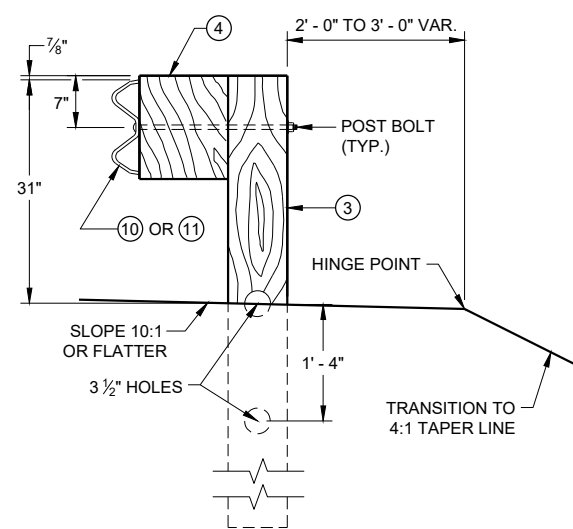
PLAN



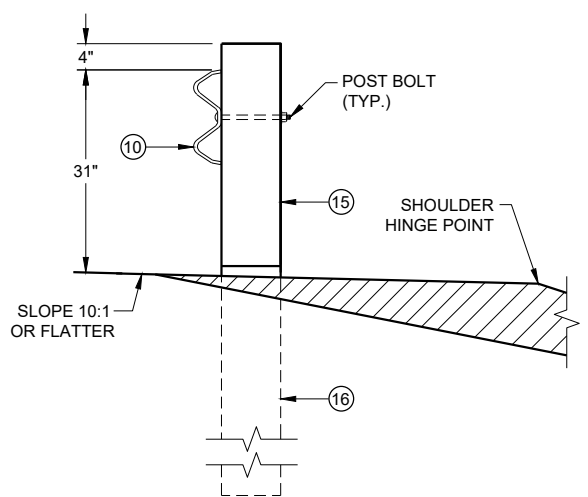
ELEVATION



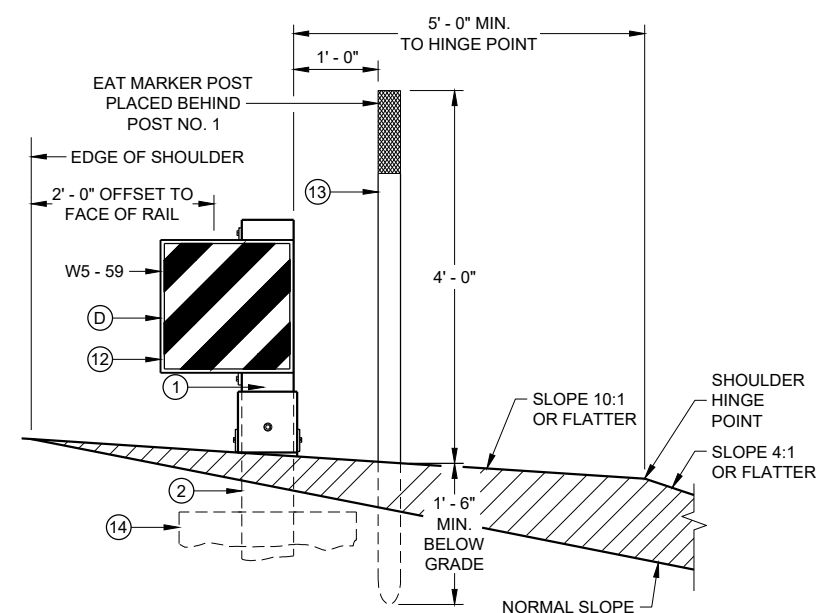
DETAIL "A"



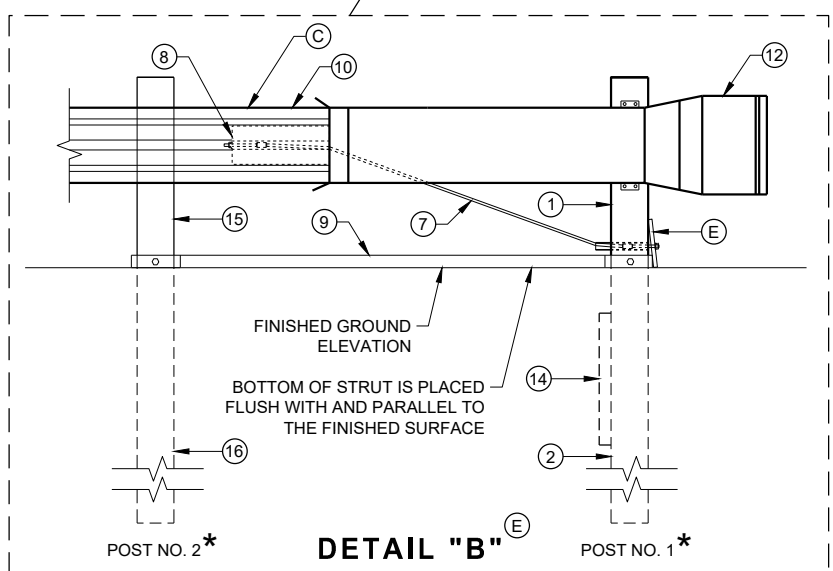
**SECTION C - C
TYPICAL AT POST NOS. 3 - 9**



**SECTION B - B
TYPICAL AT POST NO. 2***



**SECTION A - A
TYPICAL AT POST NO. 1***



DETAIL "B"

**MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL
(MGS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

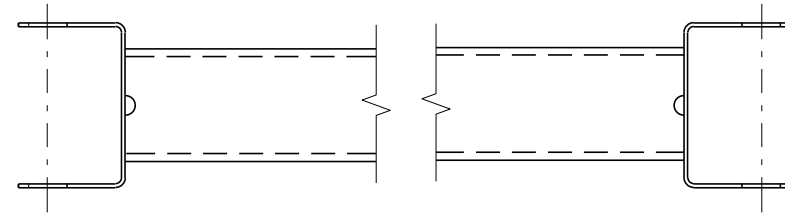
6

SDD 14B44 - 04a

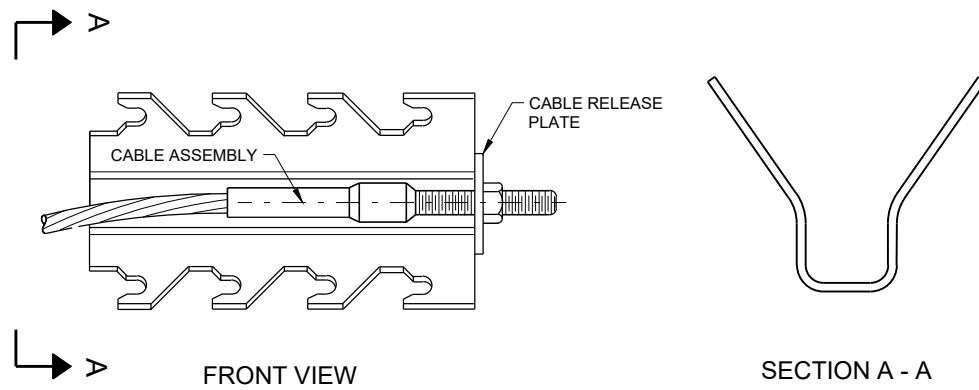
SDD 14B44 - 04a

BILL OF MATERIALS

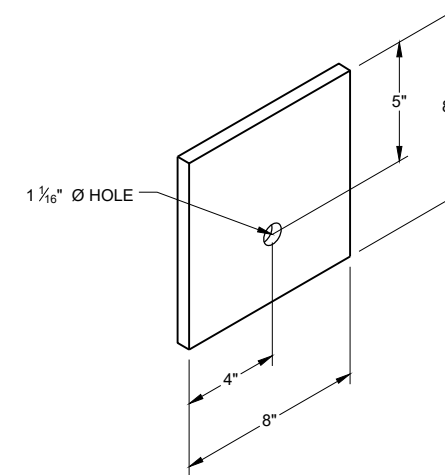
PART NO.	DESCRIPTION MATERIALS PROVIDED BY MGS EAT MANUFACTURER. SEE MANUFACTURER'S DETAILS FOR MORE INFORMATION.
①	UPPER POST NO. 1 6" X 6" TUBE
②	LOWER POST NO. 1
③	WOOD CRT
④	WOOD BLOCKOUT
⑤	PIPE SLEEVE
⑥	BEARING PLATE
⑦	BCT CABLE ASSEMBLY
⑧	ANCHOR CABLE BOX
⑨	GROUND STRUT
⑩	PERFORATED W-BEAM RAIL END PANEL, 12'-6" LONG.
⑪	STANDARD W-BEAM RAIL. MULTIPLE SECTIONS REQUIRED. SECTIONS VARY IN LENGTH.
⑫	IMPACT HEAD
⑬	EAT MARKER POST - YELLOW (SEE APPROVED PRODUCTS LIST)
⑭	SOIL PLATE
⑮	UPPER POST NO. 2
⑯	LOWER POST NO. 2



GENERIC GROUND STRUT ⑨ ⑤



GENERIC ANCHOR CABLE BOX ⑨ ⑤



BEARING PLATE ⑥ ⑤

6

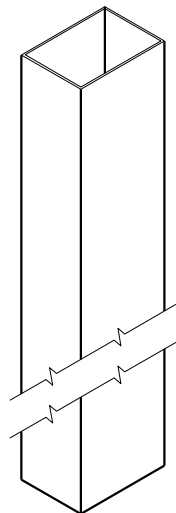
6

SDD 14B44 - 04b

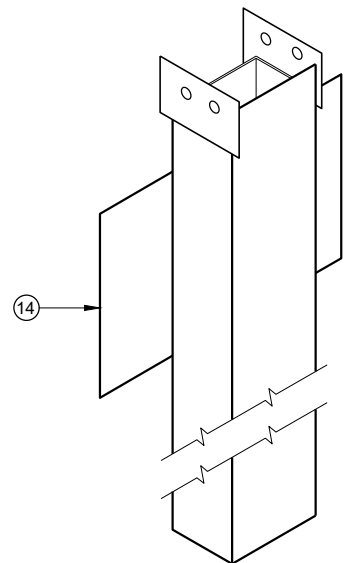
SDD 14B44 - 04b

**MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL
(MGS)**

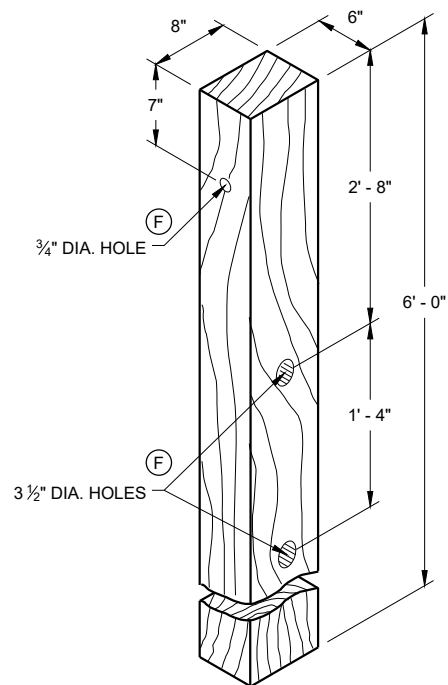
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



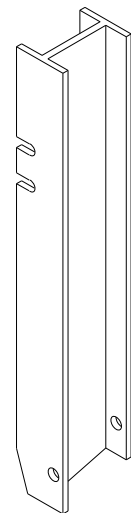
UPPER POST NO. 1 ⁽¹⁾ (E)



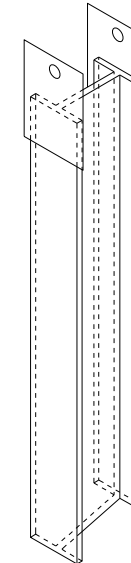
LOWER POST NO. 1 ⁽²⁾ (E)



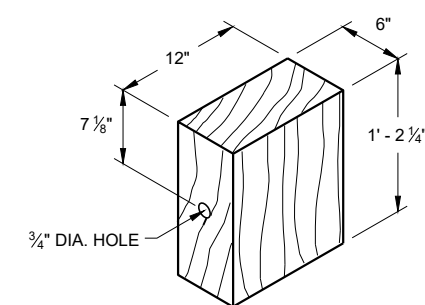
WOOD CRT POST ⁽³⁾ (E)
POSTS NUMBER 3-9



UPPER POST NO. 2 ⁽¹⁵⁾ (E)

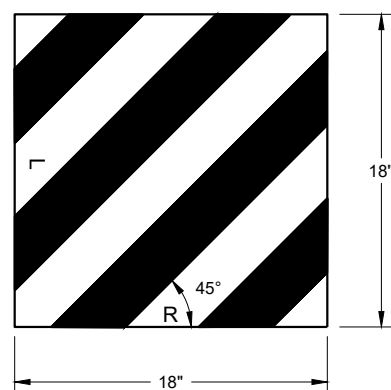


LOWER POST NO. 2 ⁽¹⁶⁾ (E)

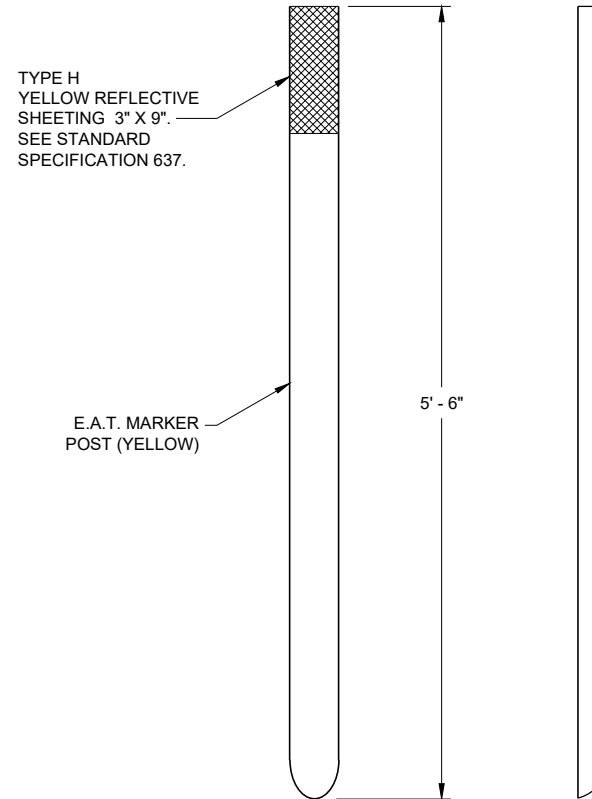


WOOD BLOCKOUT ⁽⁴⁾
REQ'D. AT ALL POSTS EXCEPT POST NO'S 1 & 2

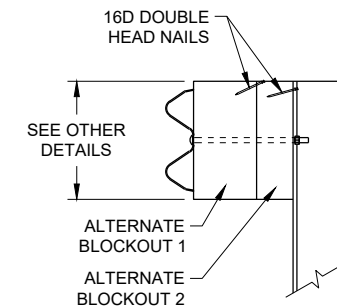
6



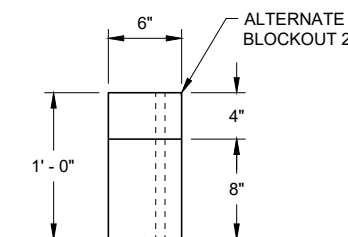
W5 - 59
REFLECTIVE SHEETING DETAIL ^(E)



FRONT VIEW SIDE VIEW
E.A.T. MARKER POST ⁽¹³⁾



SIDE VIEW



TOP VIEW

ALTERNATE WOOD BLOCKOUT DETAIL

6

SDD 14B44 - 04c

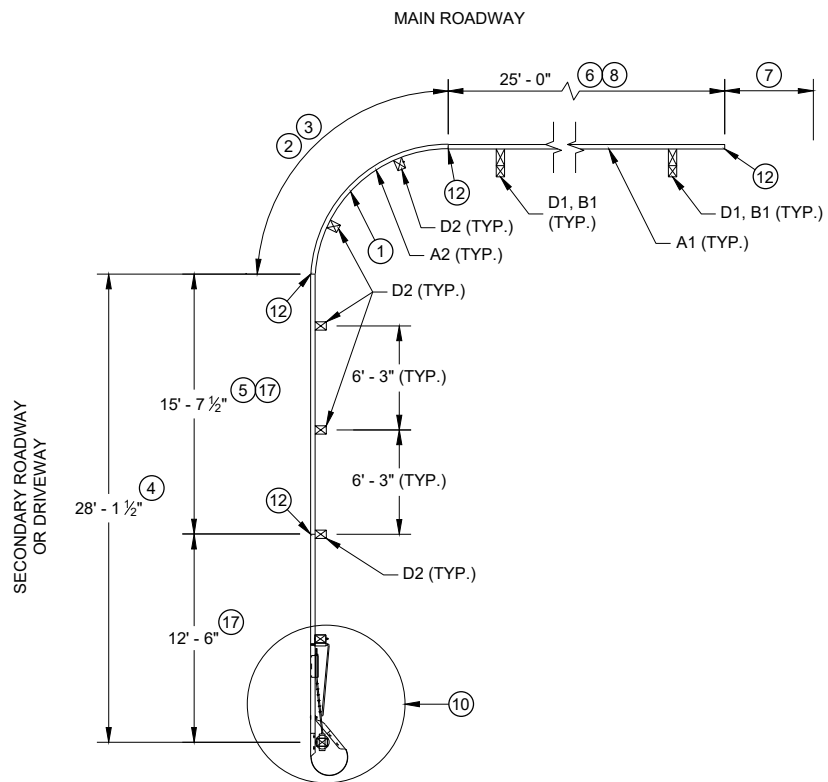
SDD 14B44 - 04c

**MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL
(MGS)**

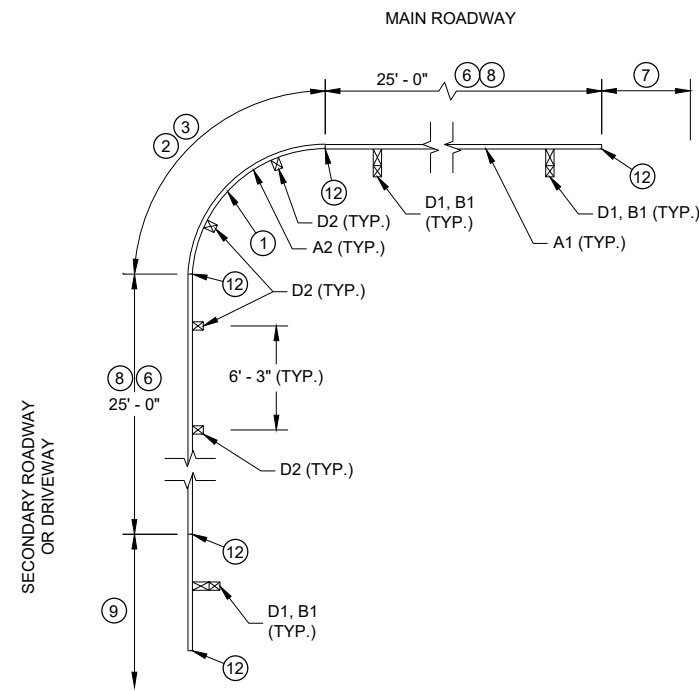
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018 DATE /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR

FHWA



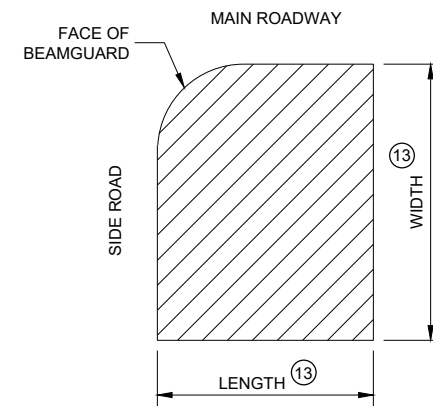
PLAN VIEW
SHORT RADIUS BEAM GUARD WITH
SHORT RADIUS TERMINAL ON
SECONDARY ROAD OR DRIVEWAY



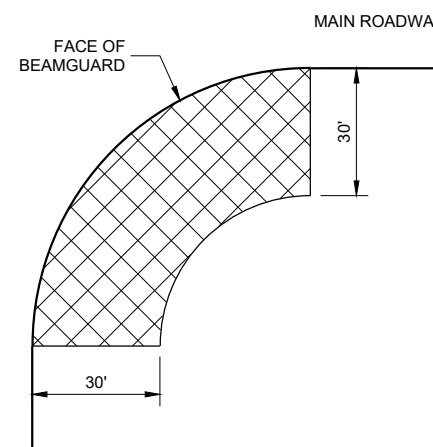
PLAN VIEW
SHORT RADIUS BEAM GUARD WITH
EAT, ADDITIONAL BEAM GUARD
OR
TRANSITION TO RIGID BARRIER ON
SECONDARY ROAD OR DRIVEWAY

TABLE FOR RADIUS OF 32' AND LESS

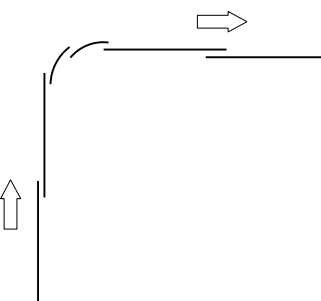
RADIUS (FT)	LENGTH (FT)	WIDTH (FT)
8	25	15
16	30	15
24	40	20
32	50	30



AREA FREE OF FIXED
OBJECTS FOR RADIUS
32' AND LESS



AREA FREE OF FIXED
OBJECTS FOR RADIUS
GREATER THAN 32'



LAP SPLICE DETAIL

GENERAL NOTES

- SEE PLANS FOR OTHER BARRIER SYSTEM AND LOCATION SPECIFICS.
- SEE SDD 14B42 FOR MORE INFORMATION ON BEAM GUARD INSTALLATION, PARTS, MATERIALS, AND INSTALLATION INFORMATION.
- GALVANIZE PARTS AFTER FABRICATION.
- WELDING TO FOLLOW CURRENT REQUIREMENTS OF THE AMERICAN WELDING SOCIETY STRUCTURAL WELDING CODE ANSI / AWS D1.1.
- UNLESS NOTED OTHERWISE, ALL PLATES ARE FLAT AND FREE OF WARP.
- UNLESS NOTED OTHERWISE, ALL EDGES ARE SMOOTH, STRAIGHT AND VERTICAL.
- ALL CUTS AND HOLES, EXCEPT IN BEAM GUARD RAIL ARE TO BE MACHINED OR MACHINE FLAME CUT.
- UNLESS NOTED OTHERWISE, CUT OR PROVIDE BOLTS THAT ARE 1/4" TO 1/2" BEYOND THE NUT.
- DRAWINGS ARE NOT TO SCALE.

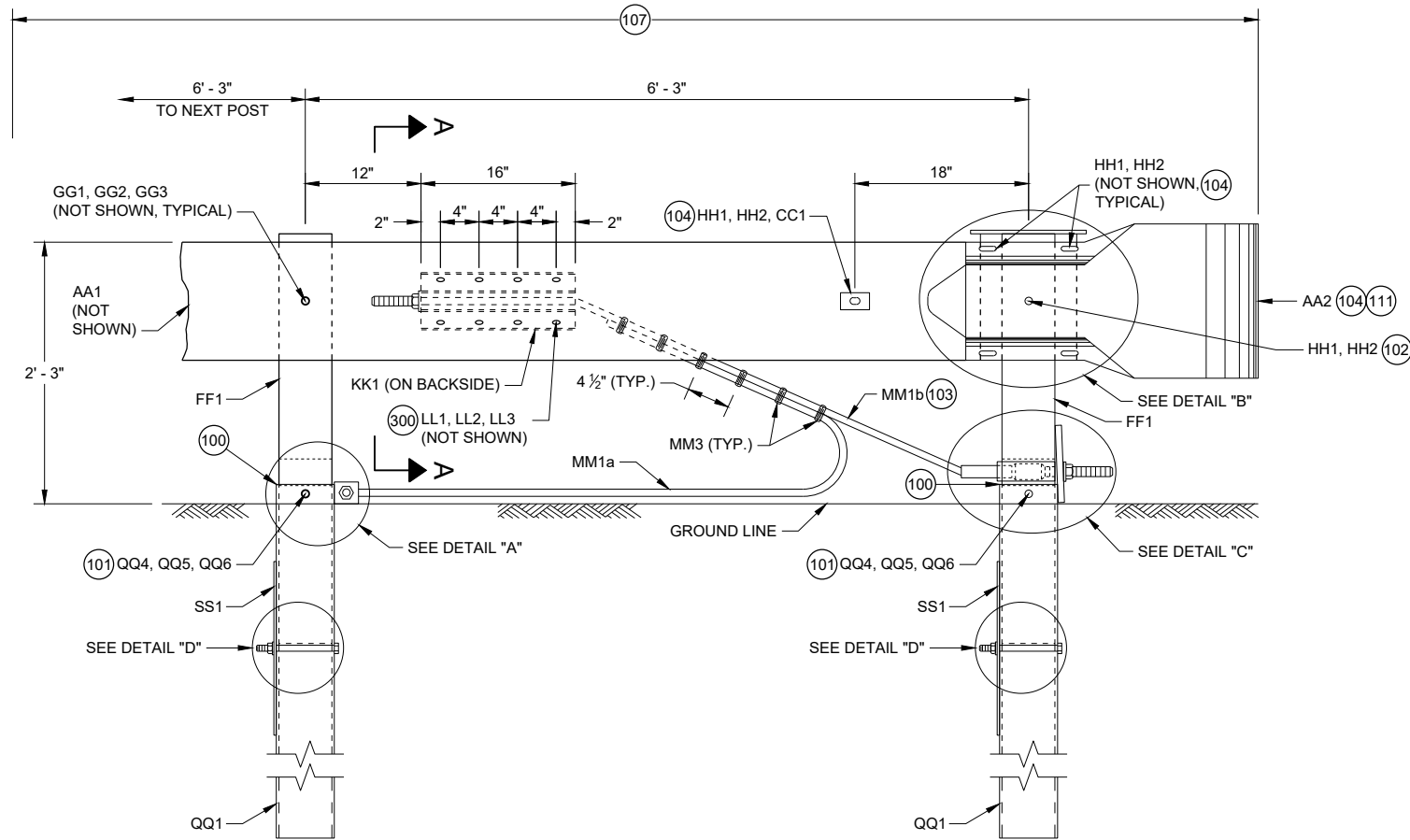
- ① RADIUS MEASURE FROM INSIDE OF RAIL. LENGTH OF BEAM GUARD SHORT RADIUS GUARD MEASURED ALONG TRAFFIC SIDE OF RAIL. RADIUS BETWEEN 8 FEET TO 150 FEET. SEE PLAN FOR REQUIRED RADIUS. BEAM GUARD RAIL IN RADIUS IS SHOP BENT. ODD RAIL LENGTH OR FIELD CUTS MAY BE REQUIRED.
- ② CONTROLLED RELEASE TERMINAL (CRT) POSTS ARE USED IN THE RADIUS. CONTROLLED RELEASE TERMINAL (CRT) POSTS ARE SPACED 6' - 3". SEE PLAN FOR NUMBER OF CONTROLLED RELEASE (CRT) POSTS.
- ③ WITHIN RADIUS BEAM GUARD RAILS ARE NOT BOLTED TO POSTS. BEAM GUARD RAIL IS RESTED ON TOP OF LAG SCREW.
- ④ MINIMUM LENGTH OF BEAM GUARD ALONG SIDE ROAD OR DRIVEWAY TO INSTALL SHORT RADIUS TERMINAL. BEAM GUARD IS PAID WITH BEAM GUARD ITEM.
- ⑤ ODD LENGTH OF BEAM GUARD REQUIRED TO INSTALL SHORT RADIUS TERMINAL.
- ⑥ MINIMUM AMOUNT OF BEAM GUARD TO BE INSTALLED PRIOR TO TRANSITION TO RIGID BARRIER, ADDITIONAL BEAM GUARD, OR EAT. BEAM GUARD PAID FOR WITH BEAM GUARD ITEM. SEE PLANS FOR MORE DETAIL.
- ⑦ BEAM GUARD, EAT, OR TRANSITION TO RIGID BARRIER. SEE PLAN.
- ⑧ TOP OF BEAM GUARD BY THE RADIUS IS 27". HEIGHT OF BEAM GUARD IS 31" BY TRANSITION TO RIGID BARRIER, ADDITIONAL BEAM GUARD OR EAT.
- ⑨ ADDITIONAL BEAM GUARD, EAT OR TRANSITION TO RIGID BARRIER. BEAM GUARD SHOWN. SEE PLAN FOR DETAILS.
- ⑩ SHORT RADIUS TERMINAL (SEE OTHER DETAILS).
- ⑪ HEIGHT VARIES. SEE NOTE ⑧ AND ⑧.
- ⑫ BEAM GUARD RAIL SPLICE LOCATION. SPLICE LOCATION REQUIRES PART F1 AND F2. SEE SDD 14B42 FOR DETAILS.
- ⑬ SEE TABLE FOR VALUES.
- ⑭ MAXIMUM HEIGHT FOR CENTER OF HOLE IS 3/4" ABOVE FINISHED GROUND ±1".
- ⑮ DRILL POST 1 5/8" DIA. PILOT HOLE. DO NOT HAMMER LAG SCREW INTO POST.
- ⑯ SMALL SIGNS ON BREAKAWAY HARDWARE ARE ACCEPTABLE.
- ⑰ TOP OF RAIL HEIGHT IS 27" WHEN USING A SHORT RADIUS TERMINAL (CRT).

6

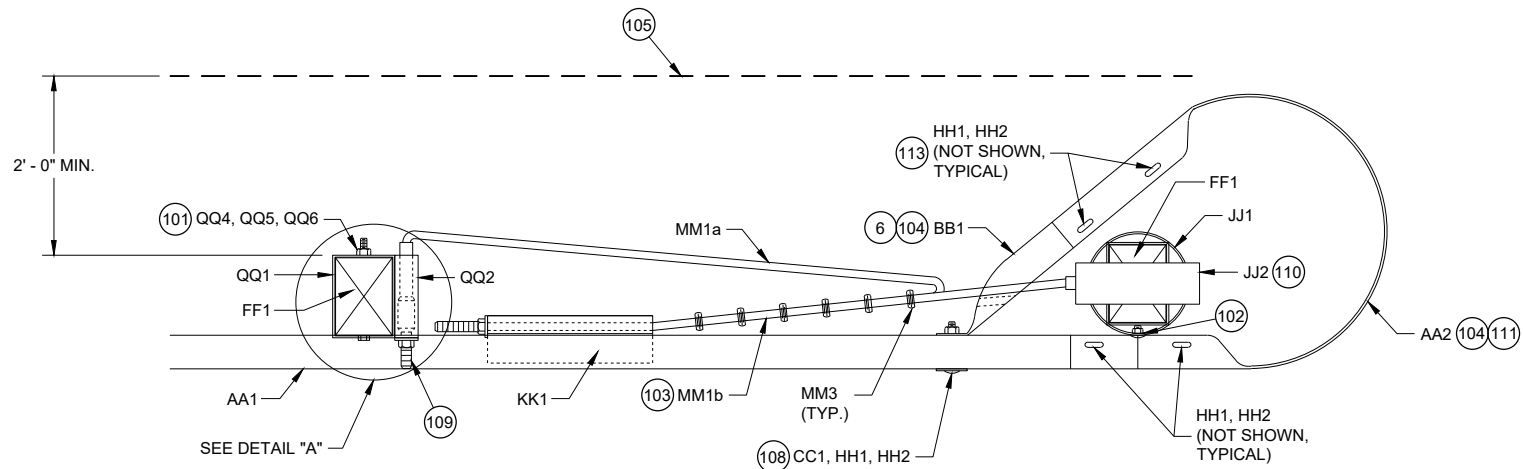
6

SHORT RADIUS BEAM GUARD (MGS) SHORT RADIUS TERMINAL (MGS)

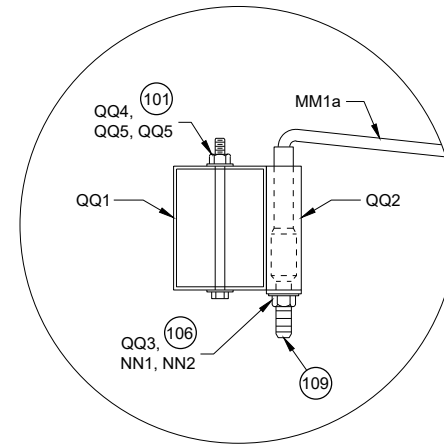
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



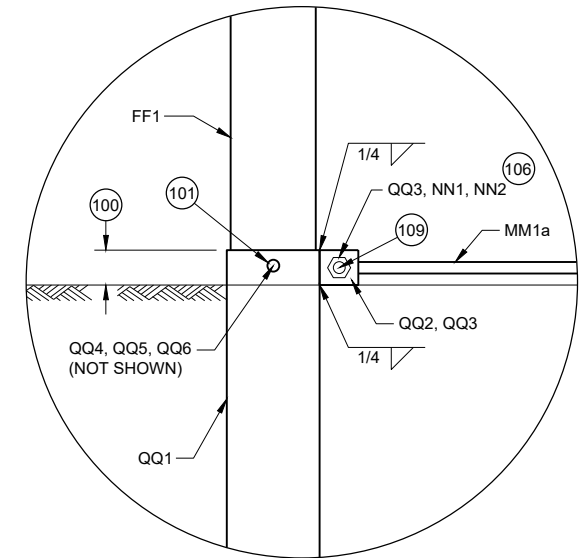
**PROFILE VIEW
SHORT RADIUS TERMINAL**



**TOP VIEW
SHORT RADIUS TERMINAL**



**TOP VIEW
DETAIL "A"
(WOOD BREAKAWAY AND BEAM
GUARD RAIL POSTS NOT SHOWN)**



**PROFILE VIEW
DETAIL "A"**

GENERAL NOTES

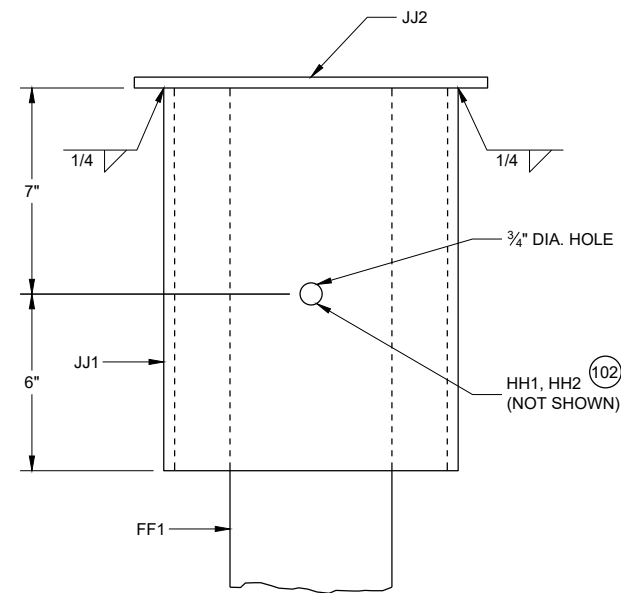
- 100 TOP OF FOUNDATION TUBE 2 INCHES MAXIMUM ABOVE FINISHED GROUND.
- 101 WASHERS REQUIRED BETWEEN BOLT HEAD AND FOUNDATION TUBE AND BETWEEN NUT AND FOUNDATION TUBE.
- 102 SPLICE BOLT AND NUT CONNECTS BEAM GUARD RAIL, W-BEAM SECTION BUFFER, AND STEEL PIPE ASSEMBLY. NO WASHER REQUIRED. SEE DETAIL "B".
- 103 CABLE IS TAUT.
- 104 ADJUST AA2 AND BB1 TO FIT.
- 105 BREAK POINT OF SHOULDER.
- 106 TACK WELD CABLE CONNECTOR TUBE PLATE TO CABLE CONNECTION TUBE. SEE DETAIL "A" PROFILE VIEW.
- 107 PAY LIMIT FOR BEAM GUARD.
- 108 SQUARE WASHER BETWEEN HEAD OF BOLT AND TRAFFIC FACE OF BEAM GUARD. ROUND WASHER REQUIRED BETWEEN NUT AND BB1.
- 109 CUT OR PROVIDE THREADED STUD THAT IS FLUSH WITH FACE OF BEAM GUARD RAIL KK1 (PLUS OR MINUS 1/2" TOLERANCE). DEBURR AFTER CUTTING.
- 110 SEE STEEL PIPE ASSEMBLY DETAILS.
- 111 ATTACH UU2 WITH UU3. SHOP APPLY UU1 TO UU2.
- 112 FOUR (4) HH1 AND HH2 REQUIRED TO ATTACH AA1 TO AA2.
- 113 FOUR (4) HH1 AND HH2 REQUIRED TO ATTACH AA2 TO BB1.

**SHORT RADIUS BEAM
GUARD (MGS) SHORT
RADIUS TERMINAL (MGS)**

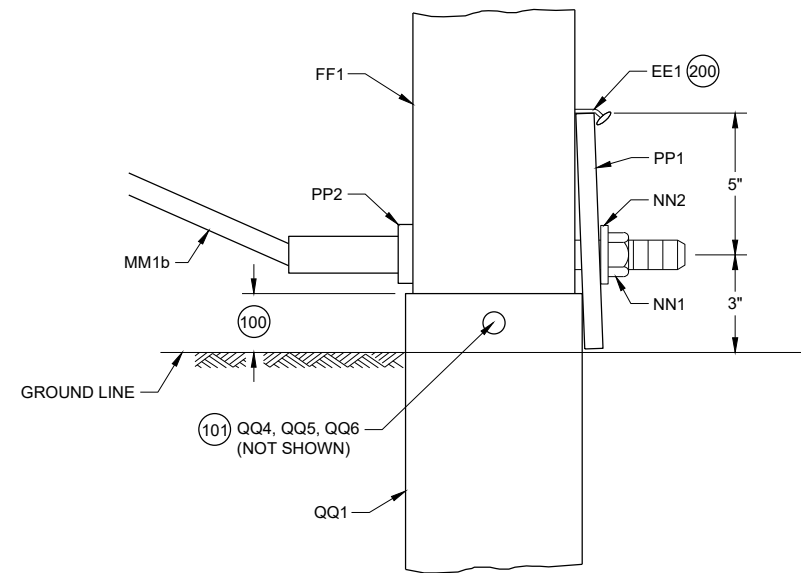
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

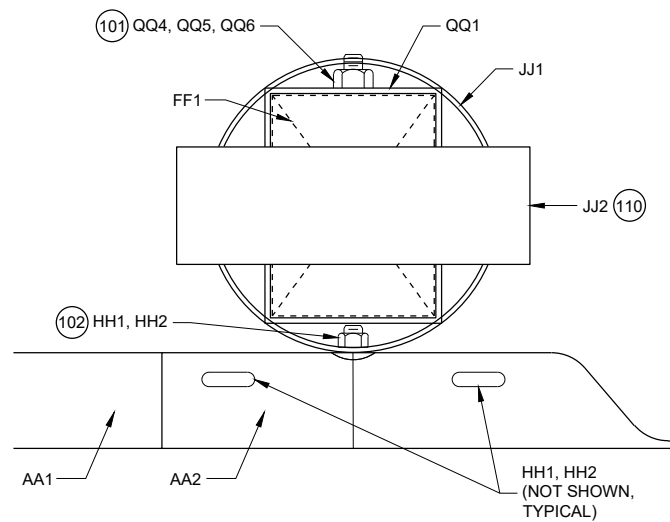
(200) TWO (2) NAILS SPACED 4 INCHES CENTER TO CENTER.



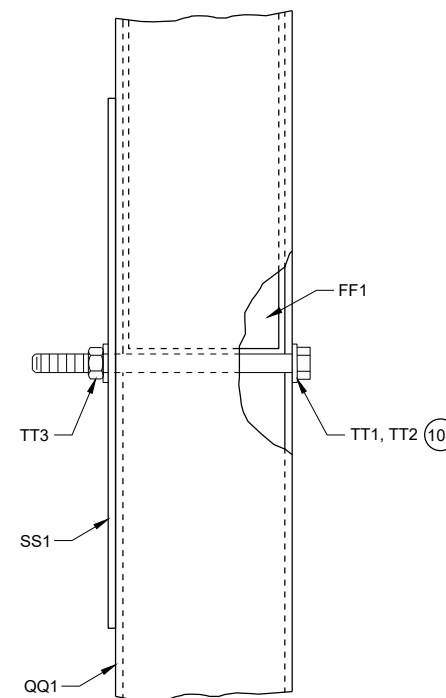
**PROFILE VIEW
DETAIL "B"
STEEL PIPE ASSEMBLY
(BEAM GUARD AND W BEAM
END SECTION NOT SHOWN)**



**PROFILE VIEW
DETAIL "C"**



**PLAN VIEW
DETAIL "B"
STEEL PIPE ASSEMBLY**



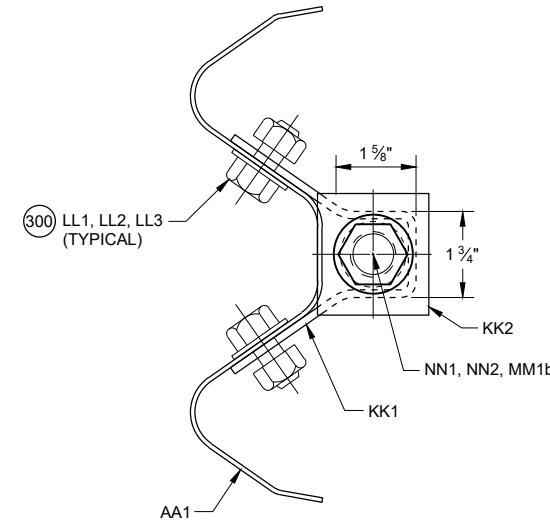
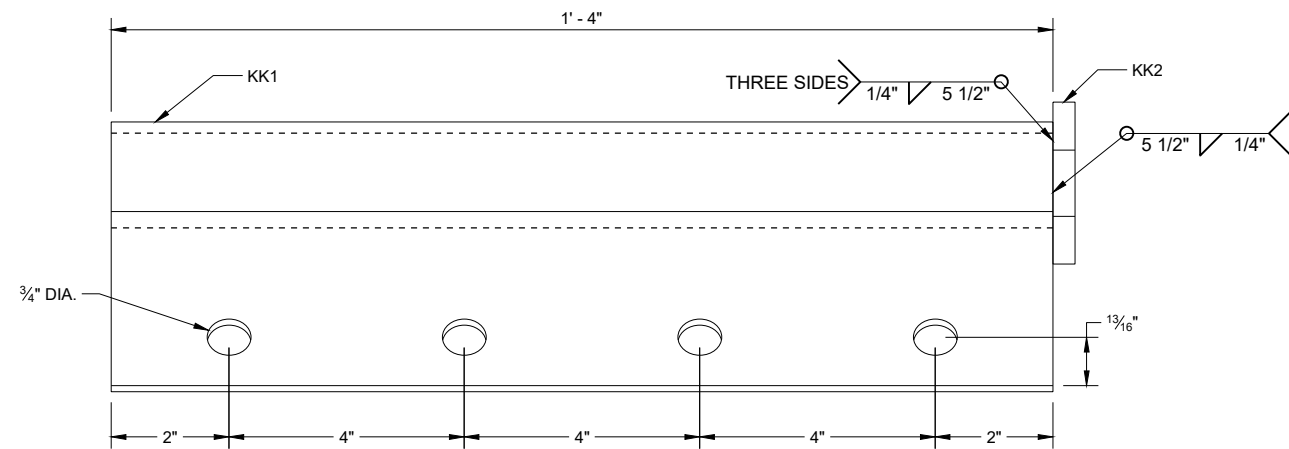
**PROFILE VIEW
DETAIL "D"**

**SHORT RADIUS BEAM
GUARD (MGS) SHORT
RADIUS TERMINAL (MGS)**

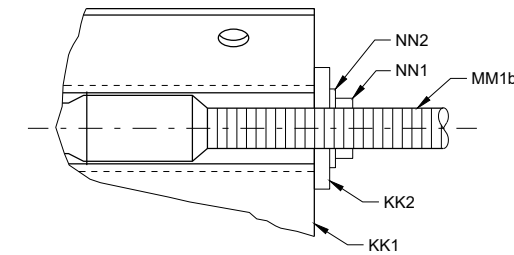
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

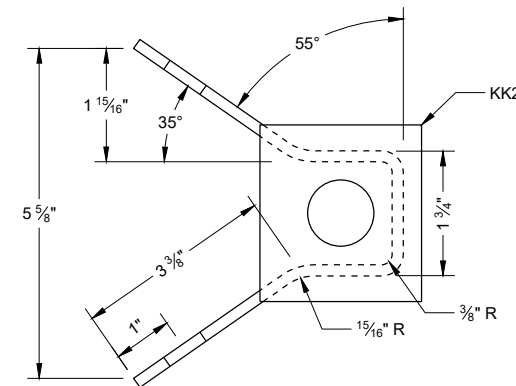
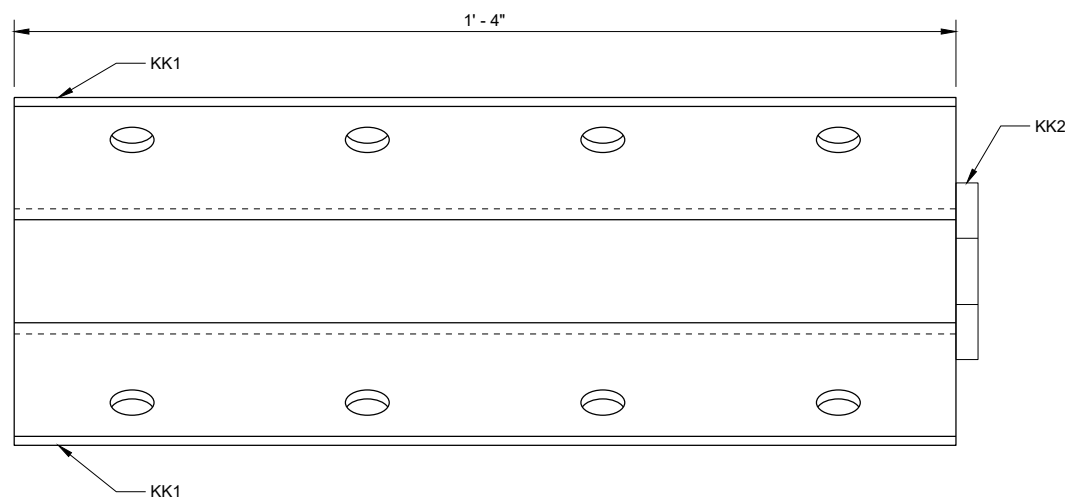
300 WASHERS REQUIRED BETWEEN BOLT HEAD AND BEAM GUARD RAIL AND BETWEEN NUT AND ANCHOR BRACKET. EIGHT (8) LL1 AND LL3 REQUIRED. SIXTEEN (16) LL2 REQUIRED.



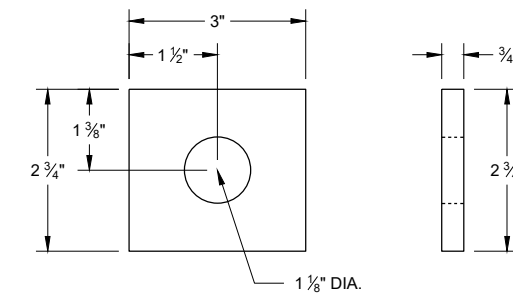
SECTION A - A



6



ANCHOR BRACKET BEARING PLATE (KK2)



ANCHOR BRACKET (KK1, KK2)

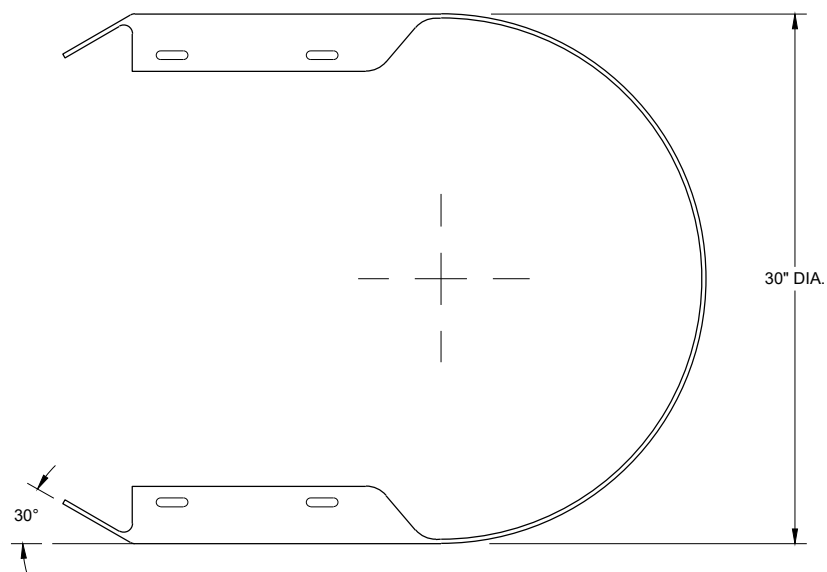
**SHORT RADIUS BEAM
GUARD (MGS) SHORT
RADIUS TERMINAL (MGS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

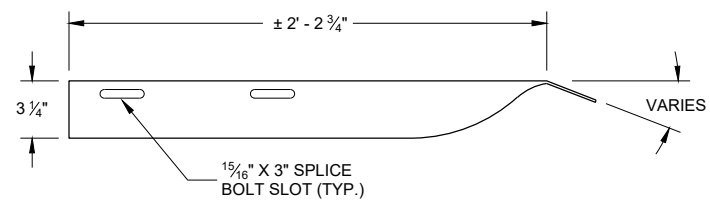
SDD 14B53 - 02d

SDD 14B53 - 02d

6



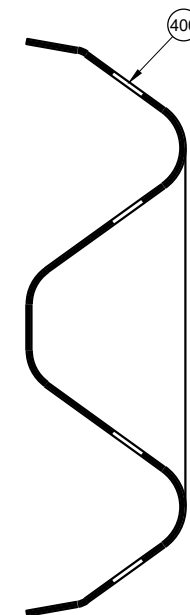
TOP VIEW



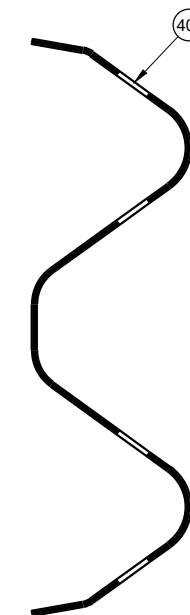
TOP VIEW

GENERAL NOTES

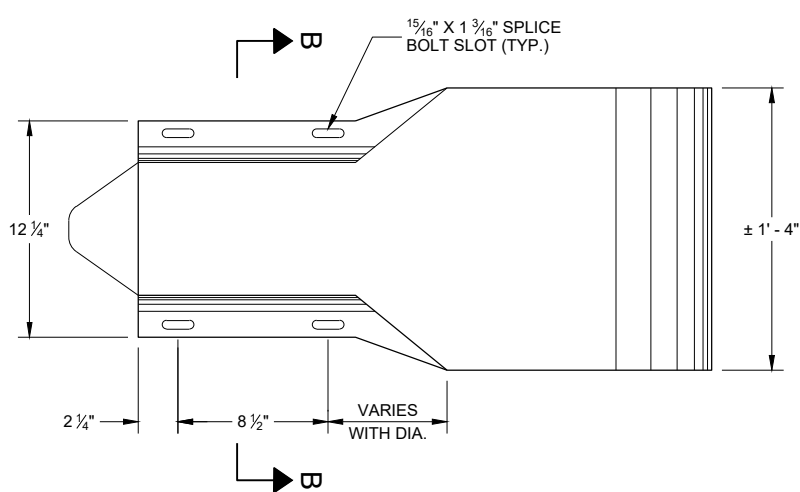
- (400) CROSS SECTION OF PART IS TO FIT OVER AA1 .
- (401) CROSS SECTION OF PART IS TO FIT OVER OR UNDER AA1 .



SECTION B - B

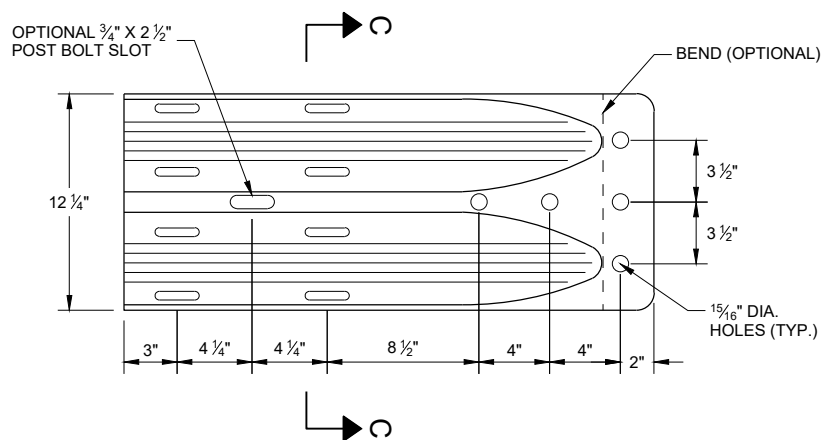


SECTION C - C



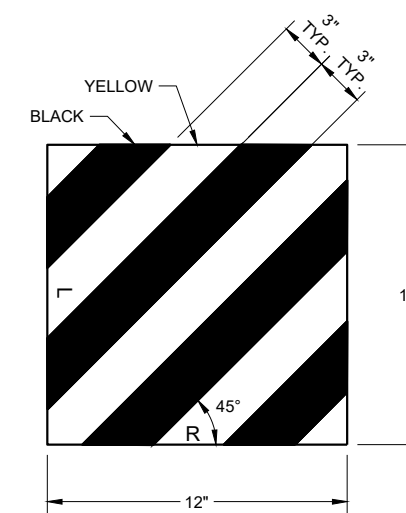
PROFILE VIEW

**W BEAM
END SECTION BUFFER (AA2)**



PROFILE VIEW

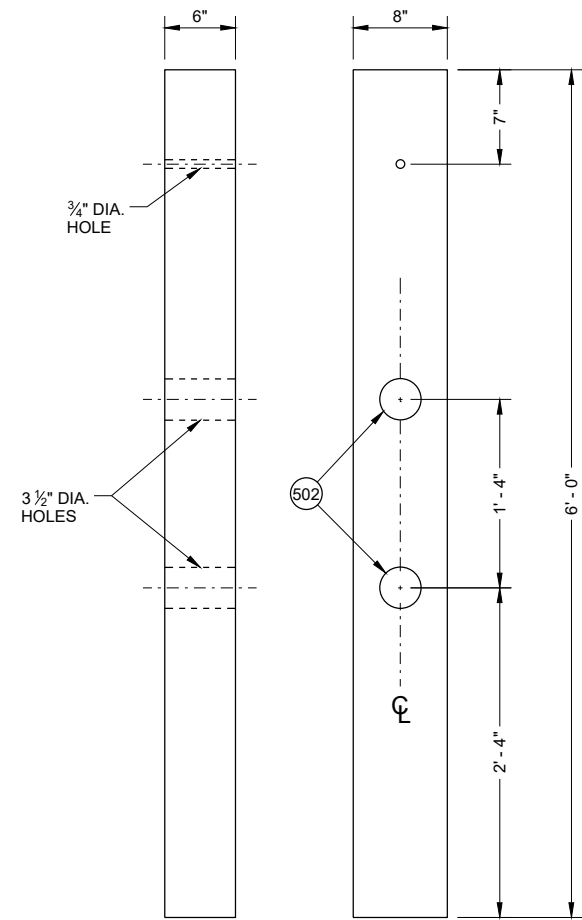
**W BEAM
TERMINAL CONNECTOR (BB1)**



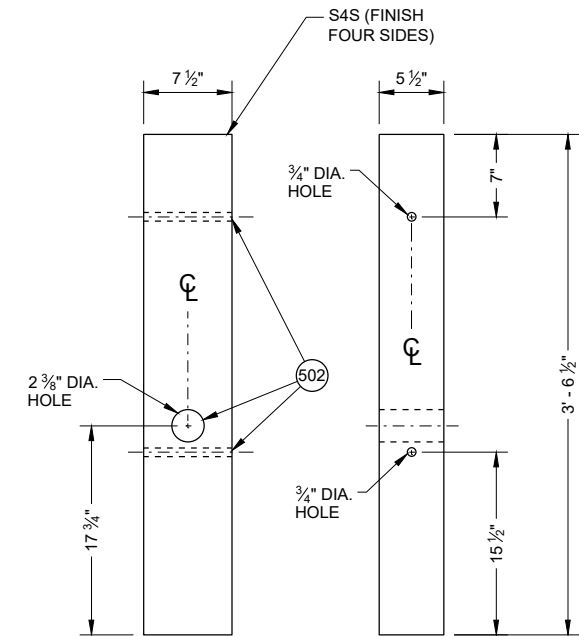
REFLECTIVE SHEETING (UU1, UU2)

**SHORT RADIUS BEAM
GUARD (MGS) SHORT
RADIUS TERMINAL (MGS)**

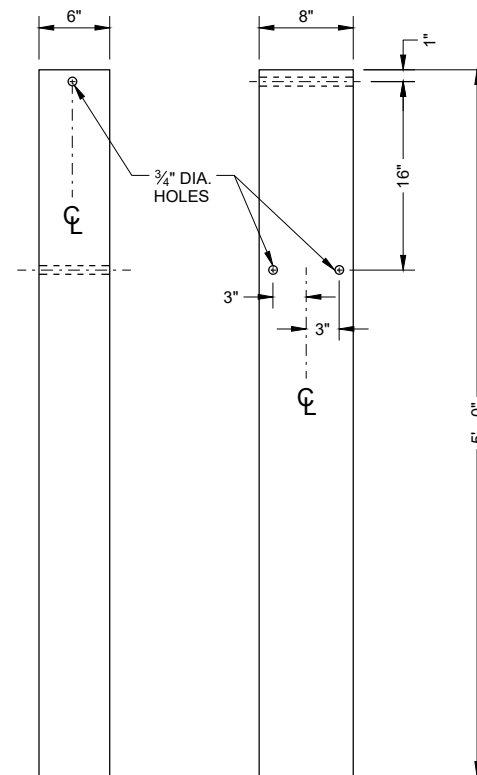
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



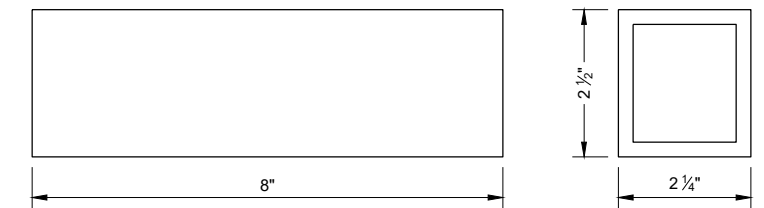
**FRONT VIEW SIDE VIEW
CONTROLLED RELEASE
POST (CRT) (DD2)**



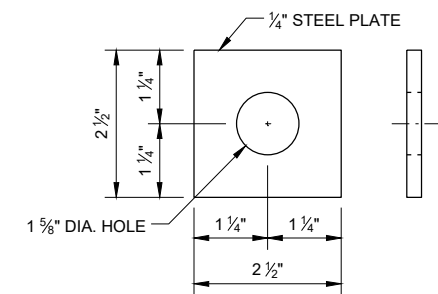
**FRONT VIEW SIDE VIEW
WOOD BREAKAWAY POST (FF1)**



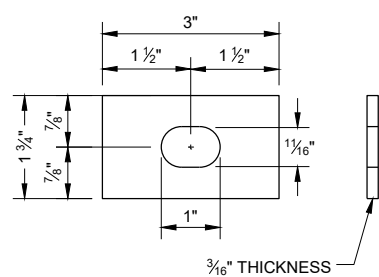
**FRONT VIEW SIDE VIEW
FOUNDATION TUBE (QQ1)**



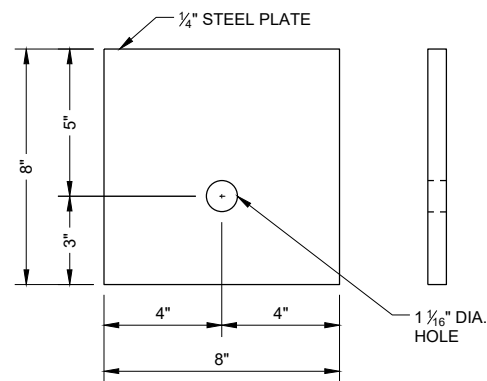
**FOUNDATION TUBE -
ANCHOR CABLE TUBE (QQ2)**



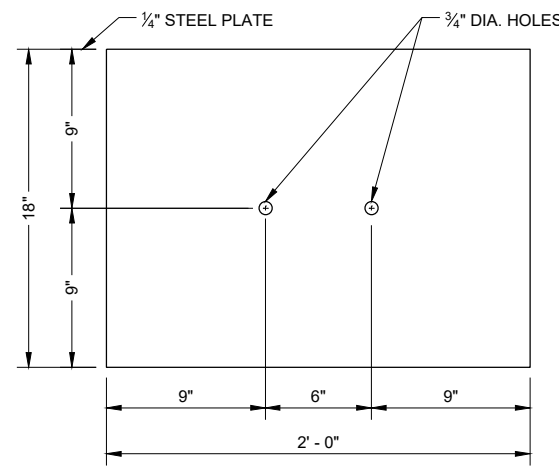
**ANCHOR CABLE TUBE
END PLATE (QQ3)**



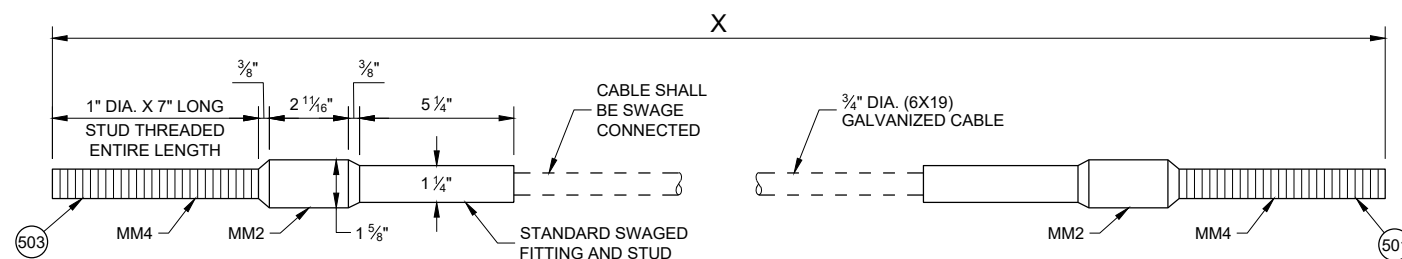
**RECTANGULAR PLATE
WASHER (CC1)**



BEARING PLATE (PP1)



SOIL PLATE (SS1)



CABLE ASSEMBLY (MM1a, MM1b)

"X" LENGTH

MM1b	9' - 0"
MM1b	6' - 8"

GENERAL NOTES

- (500) SEE DETAIL "D" FOR LOCATION AND ATTACHMENT OF SS1.
- (501) FOR MM1a THREADED STUD ONLY REQUIRED ON ONE END. SWAGED FITTING REQUIRED.
- (502) LOCATE HOLES ON THE CENTERLINE OF THE SIDE OF THE POST.
- (503) MM1a MAY HAVE ONE THREADED STUD 4 INCHES LONG. SEE NOTE (109).

**SHORT RADIUS BEAM
GUARD (MGS) SHORT
RADIUS TERMINAL (MGS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

BILL OF MATERIALS - SHORT RADIUS BEAM GUARD (MGS)

PART	DESCRIPTION	MATERIALS SPECIFICATIONS	NOTES
A1	BEAM GUARD RAIL	AASHTO M180, CLASS A, TYPE 2	
		APPROVED PRODUCER	
A2	BEAM GUARD RAIL - SHOP BENT	INDICATE ON BACK OF RAIL THE RADIUS THAT RAIL WAS BENT TO. SHOP BEND RADIUS IS TO THE NEAREST FOOT. FOLLOW AASHTO M180 ON HOW TO MARK RADIUS INFORMATION.	
		AASHTO M180, CLASS A, TYPE 2	
		APPROVED PRODUCER	
B1	BLOCK - WOOD	WISDOT SPEC. 614	SEE SDD 14B42
C1	NAIL	ASTM A153 HOT DIP CLASS D	
		ASTM F1667 TYPE 1 STYLE 12 (16 DOUBLE HEAD)	
D1	POST-STRONG POST-WOOD	WISDOT SPEC. 614	SEE SDD 14B42
D2	POST-CRT-WOOD	WISDOT SPEC. 614	
E1	POST BOLT	ASTM A307 GRADE A OR SAE J429 GRADE 2	5/8" DIA. SEE SDD 14B42 FOR BOLT GEOMETRY
		AASHTO M180	
		GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329 OR GALV. MECHANICALLY TO AASHTO M298 CLASS 50, TYPE 1 / ASTM B695 CLASS 50, TYPE 1	
		UNC	
E2	POST BOLT - WASHER	ASTM F436 TYPE 1 (HARDEN TYPICALLY USED WITH STEEL) OR ASTM F844 (UNHARDENED TYPICALLY WITH WOOD)	5/8" DIA.
		GALV. AASHTO M111 / ASTM A 123 OR GALV. HOT DIP. TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329	
E3	POST BOLT - NUT	AASHTO M180 DOUBLE RECESSED HEAVY HEX HEAD	5/8" DIA. SEE SDD 14B42 FOR BOLT GEOMETRY
		GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329 OR GALV. MECHANICALLY TO AASHTO M298 CLASS 50, TYPE 1 / ASTM B695 CLASS 50, TYPE 1	
		UNC	
		OVER TAPPED NUTS OVER-SIZE AS SPECIFIED IN AASHTO 291 / ASTM A 563	
		ASTM A563 GRADE A HEAVY HEX HEAD	
F1	SPLICE BOLT	GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329 OR GALV. MECHANICALLY TO AASHTO M298 CLASS 50, TYPE 1 / ASTM B695 CLASS 50, TYPE 1	5/8" DIA. SEE SDD 14B42 FOR BOLT GEOMETRY
		ASTM A307 GRADE A OR SAE J429 GRADE 2	
		UNC	
		AASHTO M180	

PART	DESCRIPTION	MATERIALS SPECIFICATIONS	NOTES
F2	SPLICE BOLT - NUT	ASTM A563 GRADE A	5/8" DIA. SEE SDD 14B42 FOR BOLT GEOMETRY
		AASHTO M180 DOUBLE RECESSED HEAVY HEX HEAD	
		GALV. HOT DIP TO AASHTO M232 CLASS C/ASTM A153 CLASS C/ASTM F2329 OR GALV. MECHANICALLY TO AASHTO M298 CLASS 50, TYPE 1/ASTM B695 CLASS 50, TYPE 1	
		OVER TAPPED NUTS OVER-SIZE AS SPECIFIED IN AASHTO 291 / ASTM A 563	
		UNC	
G1	LAG SCREW	ASTM A308 GRADE A ASTM A153 CLASS D	1/2" DIA. 6" LONG
H1	DELINEATOR - BEAM GUARD		SEE SDD 14B42 FOR MORE INFORMATION
H2	DELINEATION - SHEETING	YELLOW OR WHITE	
		WISDOT SPEC 637 TYPE SH	
		APPROVED PRODUCT LIST	
J1	FOUNDATION BACKFILL	STANDARD SPEC. 614	
AA1	BEAM GUARD RAIL - PUNCHED	AASHTO M180, CLASS A, TYPE 2	
		APPROVED PRODUCER	
AA2	BEAM GUARD RAIL - END SECTION BUFFER	AASHTO M180, CLASS A, TYPE 2	
		APPROVED PRODUCER	
BB1	BEAM GUARD RAIL - TERMINAL CONNECTOR MODIFIED	AASHTO M180, CLASS A, TYPE 2	
		APPROVED PRODUCER	
CC1	SHORT RADIUS - SQUARE WASHER	AASHTO M180	
		GALV. AASHTO M111 / ASTM A123	
EE1	NAIL	ASTM A153 HOT DIP CLASS D	
		ASTM F1667 TYPE 1 STYLE 12 (16 DOUBLE HEADED)	
FF1	POST - BCT - WOOD	S4S FINISH ON 4 SIDES	
		WISDOT SPEC. 614	
GG1	POST BOLT	ASTM A307 GRADE A OR SAE J429 GRADE 2	5/8" DIA. SEE SDD 14B42 FOR BOLT GEOMETRY
		AASHTO M180	
		GALV. HOT DIP TO AASHTO M232 CLASS C/ASTM A153 CLASS C/ASTM F2329 OR GALV. MECHANICALLY TO AASHTO M298 CLASS 50, TYPE 1/ASTM B695 CLASS 50, TYPE 1	
		UNC	
GG2	POST BOLT - WASHER	ASTM F436 TYPE 1 (HARDEN TYPICALLY USED WITH STEEL) OR ASTM F844 (UNHARDENED TYPICALLY WITH WOOD)	5/8" DIA.
		GALV. AASHTO M111 / ASTM A 123 OR GALV. HOT DIP. TO AASHTO M232 CLASS C/ASTM A153 CLASS C / ASTM F2329	

6

6

SDD 14B53 - 02g

SDD 14B53 - 02g

SHORT RADIUS BEAM GUARD (MGS) SHORT RADIUS TERMINAL (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

BILL OF MATERIALS - SHORT RADIUS BEAM GUARD (MGS)

PART	DESCRIPTION	MATERIALS SPECIFICATIONS	NOTES
GG3	POST BOLT - NUT	ASTM A563 GRADE A	3/8" DIA. SEE 14B42 FOR GEOMETRY
		AASHTO M180 DOUBLE RECESSED HEAVY HEX HEAD	
		GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329 OR GALV. MECHANICALLY TO AASHTO M298 CLASS 50, TYPE 1 / ASTM B695 CLASS 50, TYPE 1	
		UNC	
		OVER TAPPED NUTS OVER-SIZE AS SPECIFIED IN AASHTO 291 / ASTM A 563	
ASTM A563 GRADE A HEAVY HEX HEAD			
HH1	SPLICE BOLT	GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329 OR GALV. MECHANICALLY TO AASHTO M298 CLASS 50, TYPE 1 / ASTM B695 CLASS 50, TYPE 1	3/8" DIA. SEE SDD 14B42 FOR BOLT GEOMETRY
		ASTM A307 GRADE A OR SAE J429 GRADE 2	
		UNC	
		AASHTO M180 HEAD GEOMETRY	
HH2	SPLICE BOLT - NUT	ASTM A563 GRADE A	3/8" DIA. SEE SDD 14B42 FOR BOLT GEOMETRY
		AASHTO M180 DOUBLE RECESSED HEAVY HEX HEAD	
		GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329 OR GALV. MECHANICALLY TO AASHTO M298 CLASS 50, TYPE 1 / ASTM B695 CLASS 50, TYPE 1	
		OVER TAPPED NUTS OVER-SIZE AS SPECIFIED IN AASHTO 291 / ASTM A 563	
		UNC	
JJ1	PIPE - STEEL	ASTM A53 GALVANIZED GRADE B SCHEDULE 40	10" O.D.
JJ2	TOP PLATE	ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX STRENGTH 50 KSI OR ASTM A709 MAX STRENGTH 50 KSI OR ASTM A992 MAX STRENGTH 50 KSI	DIMENSIONS 3/8" X 4" X 1' - 0"
		GALV. AASHTO M111 / ASTM A123	
KK1	ANCHOR BRACKET	ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX STRENGTH 50 KSI OR ASTM A709 MAX STRENGTH 50 KSI OR ASTM A992 MAX STRENGTH 50 KSI	
		GALV. AASHTO M111 / ASTM A123	
KK2	ANCHOR BRACKET - BEARING PLATE	ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX STRENGTH 50 KSI OR ASTM A709 MAX STRENGTH 50 KSI OR ASTM A992 MAX STRENGTH 50 KSI	
		GALV. AASHTO M111 / ASTM A123	
LL1	ANCHOR BRACKET - BOLT	ASTM A307 GRADE B HEAVY HEX HEAD OR SAE J429 GRADE 2 HEAVY HEX HEAD	3/8" DIA.
		GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329 OR GALV. MECHANICALLY TO AASHTO M298 CLASS 50, TYPE 1 / ASTM B695 CLASS 50, TYPE 1	
		UNC	

PART	DESCRIPTION	MATERIALS SPECIFICATIONS	NOTES
LL2	ANCHOR BRACKET - WASHER	ASTM F436 TYPE 1 (HARDEN WASHER ONLY)	3/8" DIA.
		GALV. AASHTO M111 / ASTM A123 OR GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329	
LL3	ANCHOR BRACKET - NUT	ASTM A563 GRADE A	3/8" DIA.
		GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329 OR GALV. MECHANICALLY TO AASHTO M298 CLASS 50, TYPE 1 / ASTM B695 CLASS 50, TYPE 1	
		OVER TAPPED NUTS OVER-SIZE AS SPECIFIED IN AASHTO 291 / ASTM A 563	
		UNC	
MM1a	ANCHOR CABLE	AASHTO M30 / ASTM A741 INDEPENDENT WIRE CORE (IWRC) OR WIRE STRAND CORE (WCS), IMPROVED PLOW STEEL (IPS), 6X19, TYPE II OR IIc CLASS C ZINC COATED	
MM1b	ANCHOR CABLE	AASHTO M30 / ASTM A741 INDEPENDENT WIRE CORE (IWRC) OR WIRE STRAND CORE (WCS), IMPROVED PLOW STEEL (IPS), 6X19, TYPE II OR IIc CLASS C ZINC COATED	
MM2	ANCHOR CABLE - SWAGE FITTING	ASTM A576 GRADE 1035	
		SWAGE FITTINGS ARE TO BE FACTORY SWEDGED. WITH A BREAKING STRENGTH 40,000 LBS.	
		GALV. AASHTO M111 / ASTM A123	
		ASME B30.26 FORGED, CAST, OR DIE STAMPED WITH THE FOLLOWING INTO CONNECTION: NAME OF MANUFACTURER OR TRADEMARK OF CONNECTION'S MANUFACTURER, SIZE OR RATED LOAD, GRADE.	
MM3	WIRE ROPE CABLE CLAMPS	FF-C-450D TYPE 1 CLASS 1	3/4"
		ASTM A153 HOT DIP CLASS D	
MM4	ANCHOR CABLE - SWAGE FITTING - STUD	ASTM F3125 GRADE A325 TYPE 1 OR SAE GRADE 5 OR ASTM A449 TYPE 1 HEAVY HEX HEAD	
		GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329 OR GALV. MECHANICALLY TO AASHTO M298 CLASS 50, TYPE 1 / ASTM B695 CLASS 50, TYPE 1	
		UNC	
NN1	ANCHOR CABLE - NUT	ASTM A563 GRADE A	1" DIA.
		AASHTO M180 DOUBLE RECESSED HEAVY HEX HEAD	
		GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329 OR GALV. MECHANICALLY TO AASHTO M298 CLASS 50, TYPE 1 / ASTM B695 CLASS 50, TYPE 1	
		OVER TAPPED NUTS OVER-SIZE AS SPECIFIED IN AASHTO 291 / ASTM A 563	
NN2	ANCHOR CABLE - NUT - WASHER	UNC	1" DIA.
		ASTM F436 TYPE 1 (HARDEN WASHER ONLY)	
		GALV. AASHTO M111 / ASTM A123 OR GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329	

6

6

SDD 14B53 - 02h

SDD 14B53 - 02h

**SHORT RADIUS BEAM
GUARD (MGS) SHORT
RADIUS TERMINAL (MGS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

BILL OF MATERIALS - SHORT RADIUS BEAM GUARD (MGS)

PART	DESCRIPTION	MATERIALS SPECIFICATIONS	NOTES
PP1	BEARING PLATE AT POST	ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX STRENGTH 50 KSI OR ASTM A709 MAX STRENGTH 50 KSI OR ASTM A992 MAX STRENGTH 50 KSI	
		GALV. AASHTO M111 / ASTM A123	
PP2	PIPE - STEEL	ASTM A53 GALVANIZED GRADE B SCHEDULE 40	2" DIA. x 6" LONG
QQ1	FOUNDATION TUBE	ASTM A500 GRADE B	8" X 6" X 3/8"
		GALV. AASHTO M111 / ASTM A123	
QQ2	SHORT RADIUS - FOUNDATION TUBE - ANCHOR CABLE - TUBE	ASTM A500 GRADE B	DIMENSIONS 2 1/2" X 2 1/4" X 1/4" X 8"
		GALV. AASHTO M111 / ASTM A123	
QQ3	SHORT RADIUS - SOIL TUBE - ANCHOR CABLE - TUBE - END PLATE	ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX STRENGTH 50 KSI OR ASTM A709 MAX STRENGTH 50 KSI OR ASTM A992 MAX STRENGTH 50 KSI	DIMENSIONS 2 1/2" X 2 1/2" X 1/4"
		GALV. AASHTO M111 / ASTM A123	
QQ4	GROUND STRUT AND YOKE - BOLT	GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329 OR GALV. MECHANICALLY TO AASHTO M298 CLASS 50, TYPE 1 / ASTM B695 CLASS 50, TYPE 1	5/8 DIA.
		ASTM A307 GRADE B HEAVY HEX HEAD OR SAE J429 GRADE 2 HEAVY HEX HEAD	
		UNC	
QQ5	GROUND PLATE AND YOKE - WASHER	ASTM F436 TYPE 1 (HARDEN WASHER ONLY)	5/8 DIA.
		GALV. AASHTO M111 / ASTM A123 OR GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329	
QQ6	GROUND STRUT AND YOKE - NUT	HEAVY HEX	5/8 DIA.
		UNC	
		ASTM A563 GRADE A	
		OVER TAPPED NUTS AS SPECIFIED IN AASHTO 291 / ASTM A 563	
		GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329 OR GALV. MECHANICALLY TO AASHTO M298 CLASS 50, TYPE 1 / ASTM B695 CLASS 50, TYPE 1	

PART	DESCRIPTION	MATERIALS SPECIFICATIONS	NOTES
SS1	SOIL PLATE	ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX STRENGTH 50 KSI OR ASTM A709 MAX STRENGTH 50 KSI OR ASTM A992 MAX STRENGTH 50 KSI	
		GALV. AASHTO M111 / A123	
TT1	SOIL PLATE - BOLT	ASTM A307 GRADE B HEAVY HEX HEAD OR SAE J429 GRADE 2 HEAVY HEX HEAD	5/8 DIA.
		GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329 OR GALV. MECHANICALLY TO AASHTO M298 CLASS 50, TYPE 1 / ASTM B695 CLASS 50, TYPE 1	
		UNC	
TT2	SOIL PLATE - WASHER	ASTM F436 TYPE 1 (HARDEN WASHER ONLY)	5/8 DIA.
		GALV. AASHTO M111 / ASTM A123 OR GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329	
TT3	SOIL PLATE - NUT	GALV. HOT DIP TO AASHTO M232 CLASS C / ASTM A153 CLASS C / ASTM F2329 OR GALV. MECHANICALLY TO AASHTO M298 CLASS 50, TYPE 1 / ASTM B695 CLASS 50, TYPE 1	5/8 DIA.
UU1	OBJECT MARKER - SHEETING	MUTCD / WISDOT OBJECT MARKER TYPE 3	PATTERN AND COLOR FOR SHEETING. SHEETING TYPE FOR MARKER.
		WISDOT SPEC 637 TYPE F	
		APPROVED PRODUCT LIST	
UU2	OBJECT MARKER - ALUMINUM PLATE	WISDOT SPEC 637 ALUMINUM PLATE	MATERIAL AND THICKNESS OF MATERIALS
UU3	OBJECT MARKER - SCREWS	STAINLESS SELF-TAPPING SCREWS	
VV1	FOUNDATION BACKFILL	WISDOT SPEC 614	

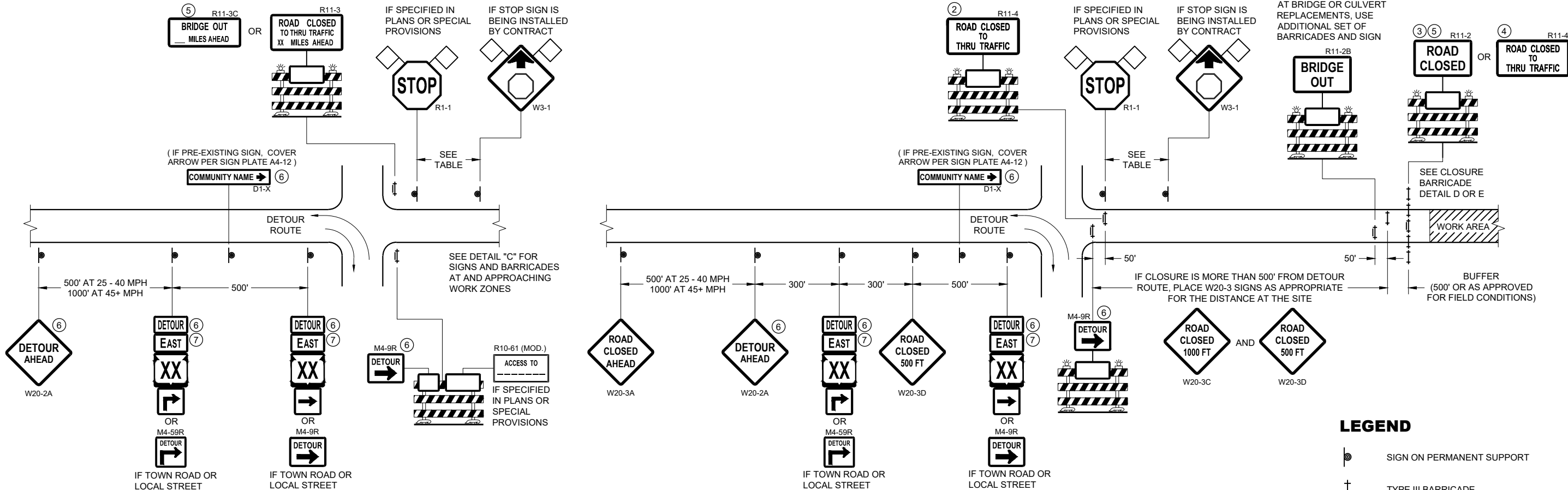
6

6

SDD 14B53 - 02i

SDD 14B53 - 02i

SHORT RADIUS BEAM GUARD (MGS) SHORT RADIUS TERMINAL (MGS)	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2022 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

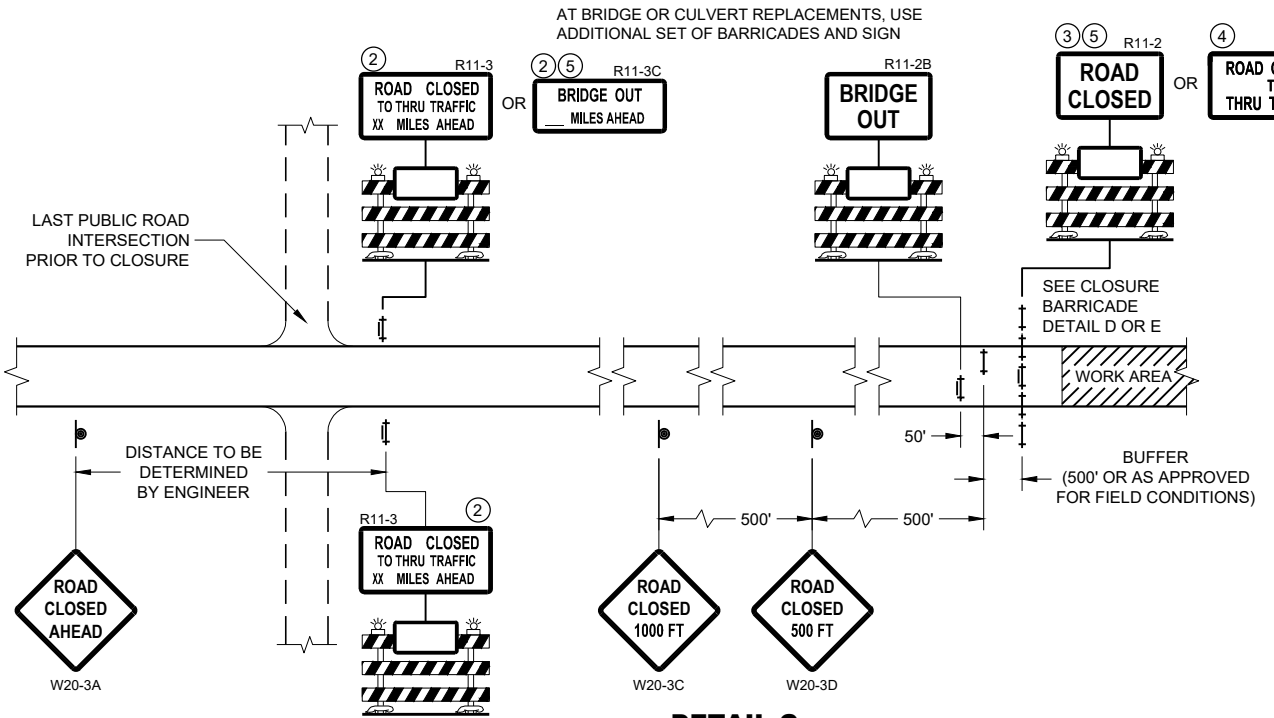
WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



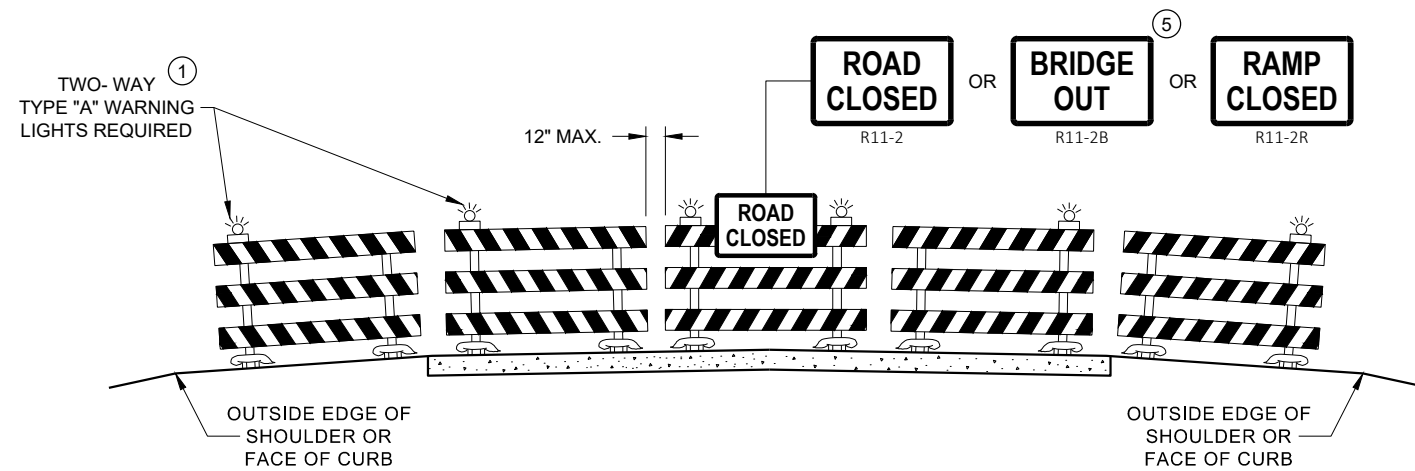
**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

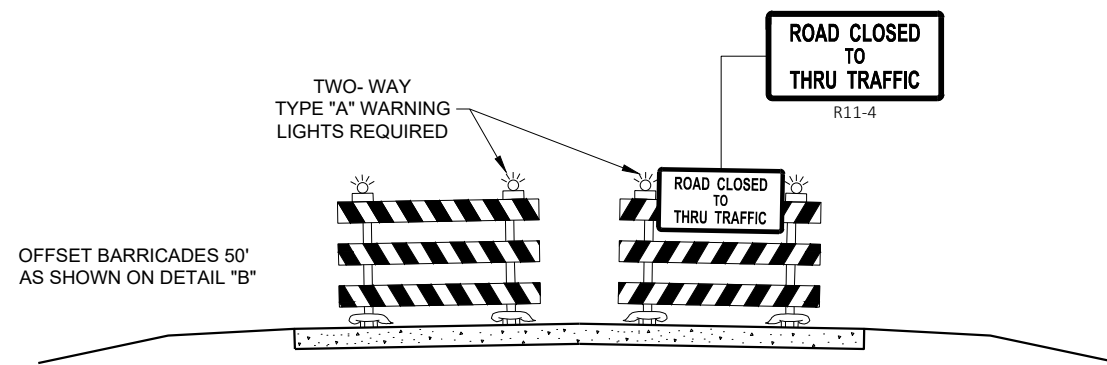
**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

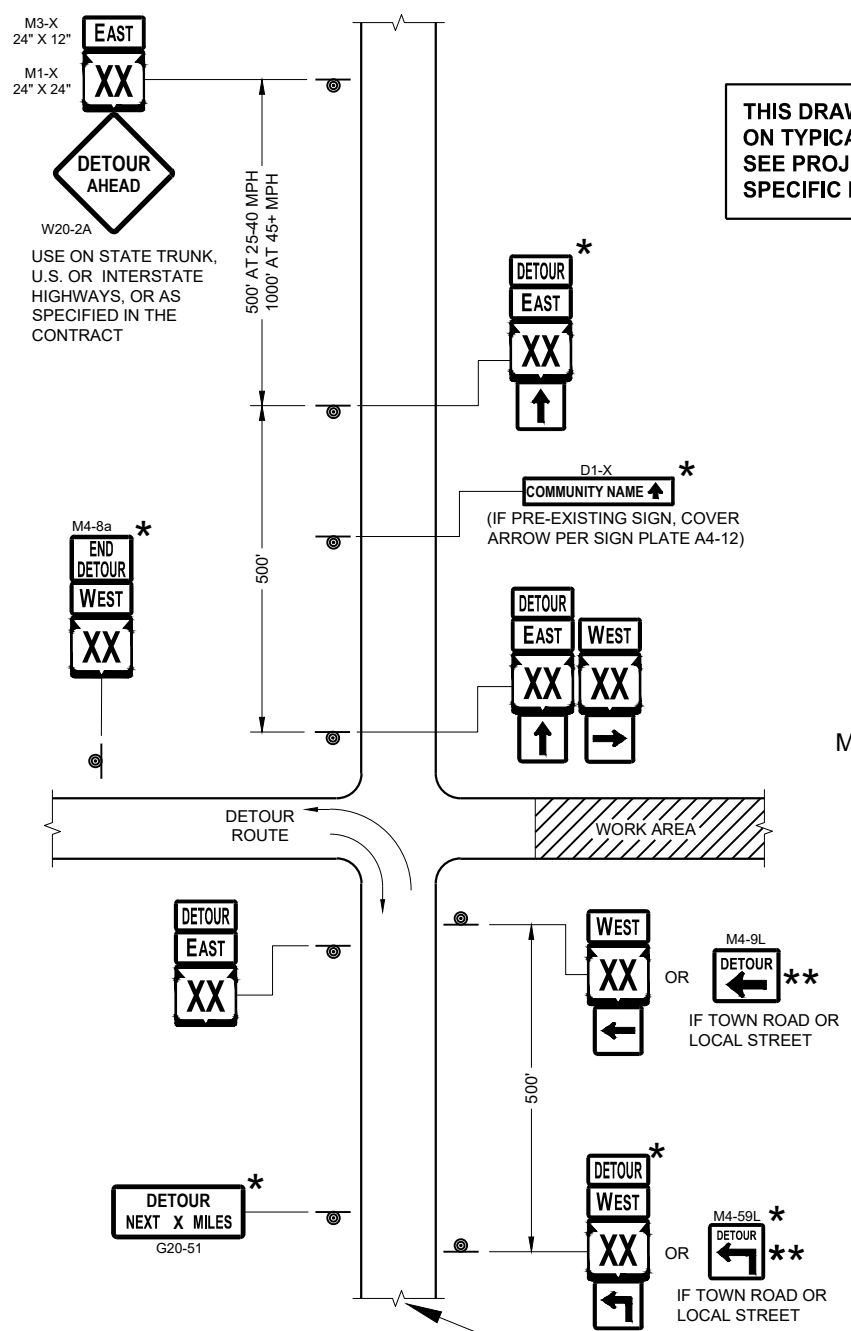
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER



THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1
- M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

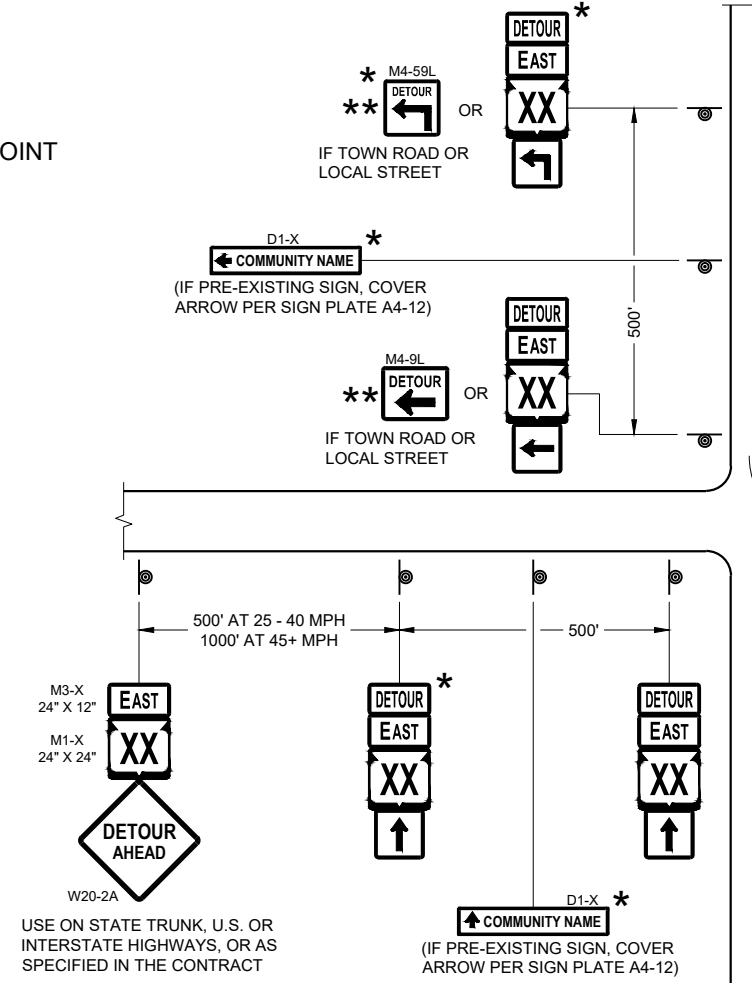
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

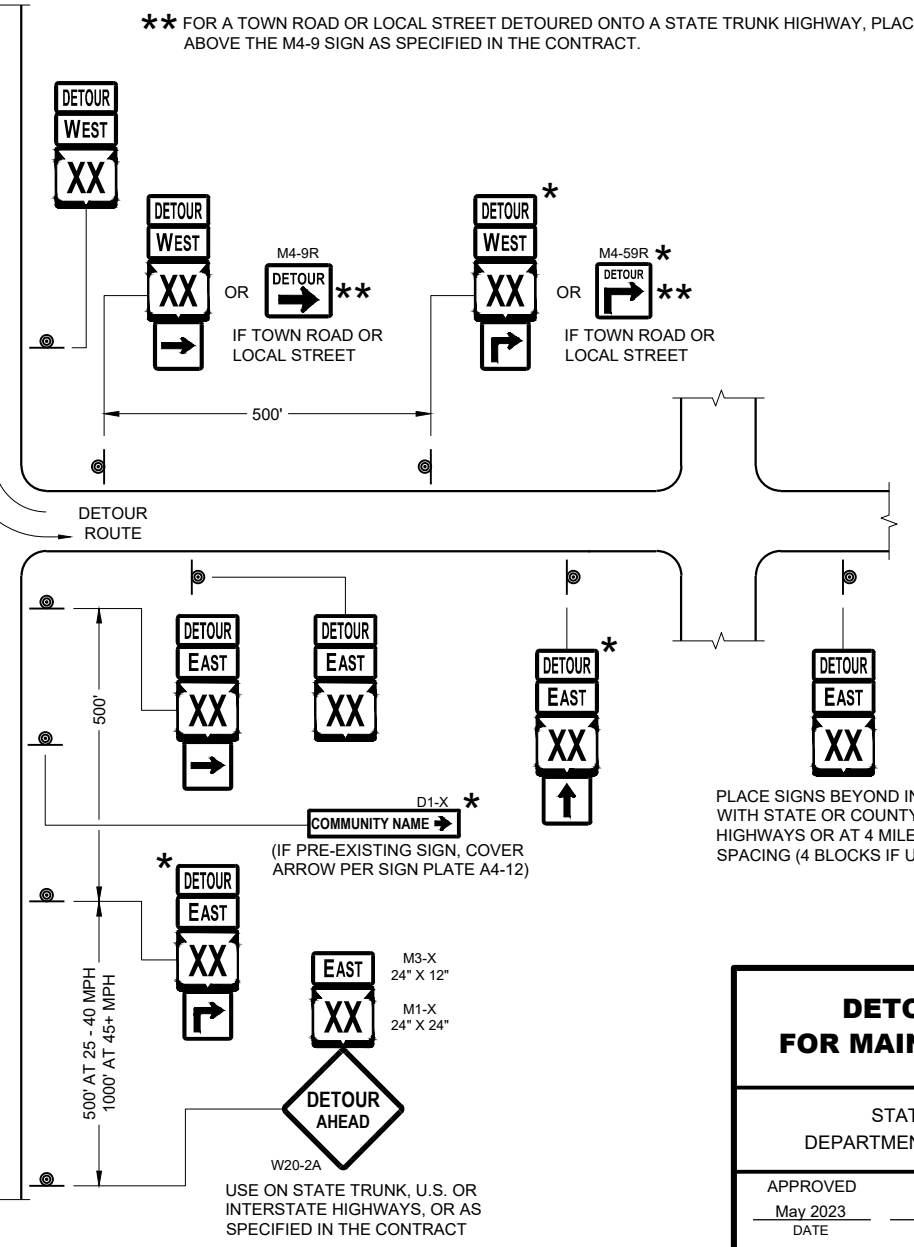
- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-59 SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

- * OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- ** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

MATCH POINT



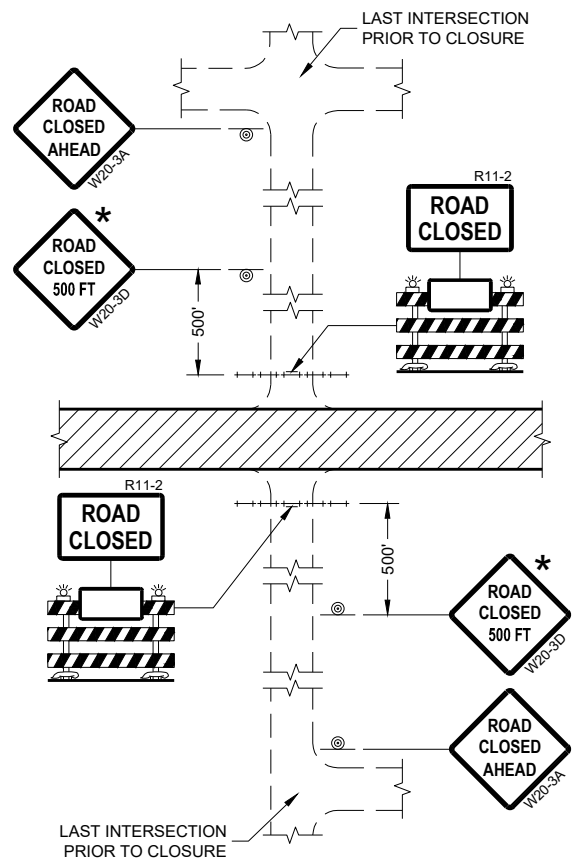
**DETAIL F
DETOUR SIGNING**



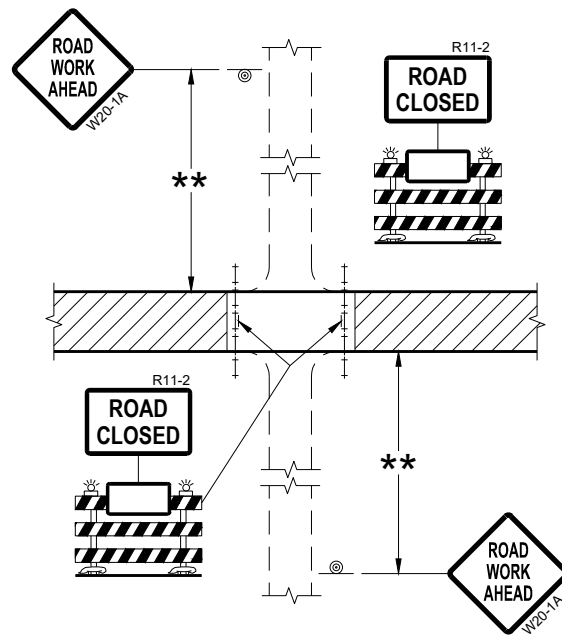
PLACE SIGNS BEYOND INTERSECTIONS WITH STATE OR COUNTY TRUNK HIGHWAYS OR AT 4 MILE MAXIMUM SPACING (4 BLOCKS IF URBAN AREA)

SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

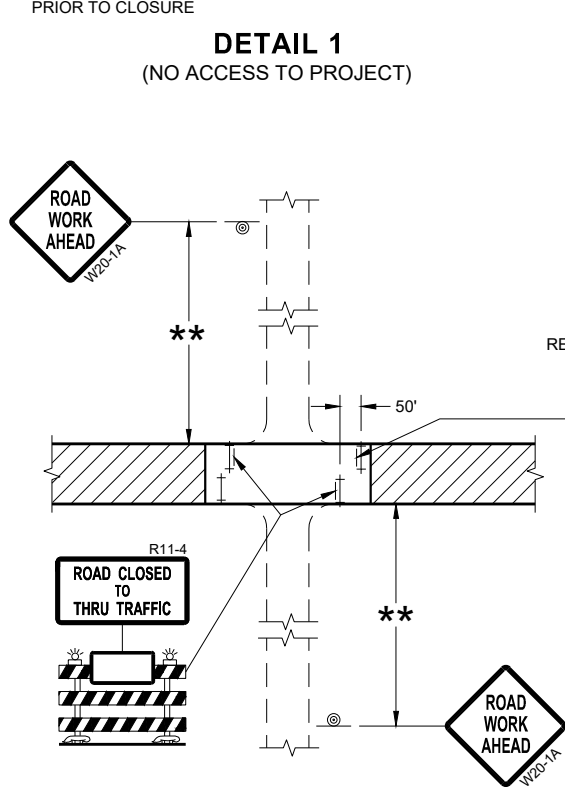
DETOUR SIGNING FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



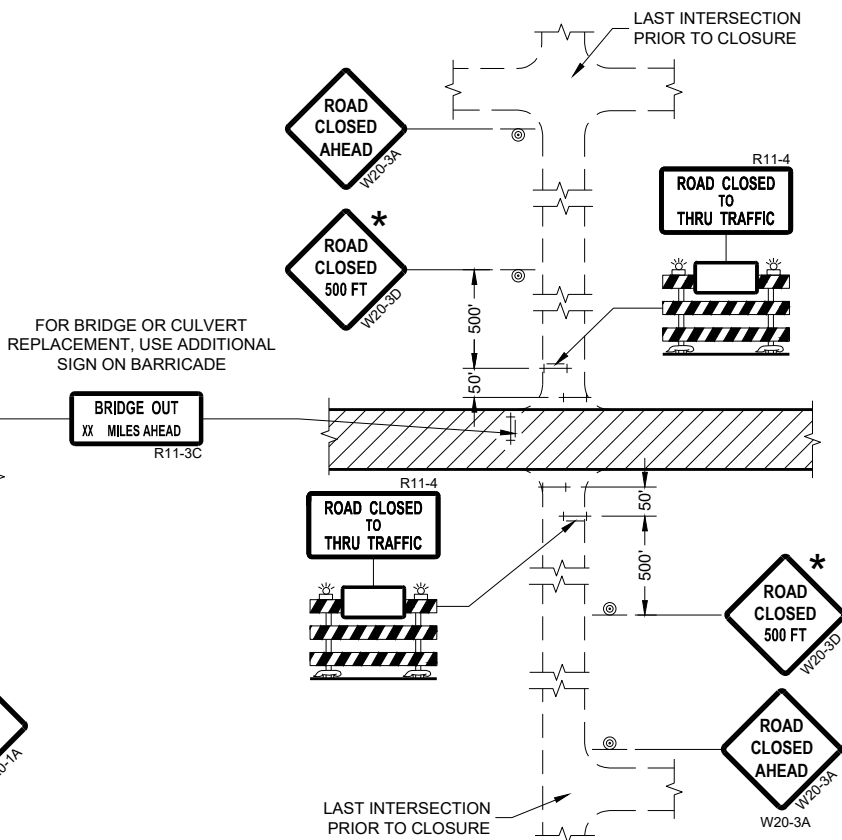
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

- * OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- ** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION




APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

GENERAL NOTES

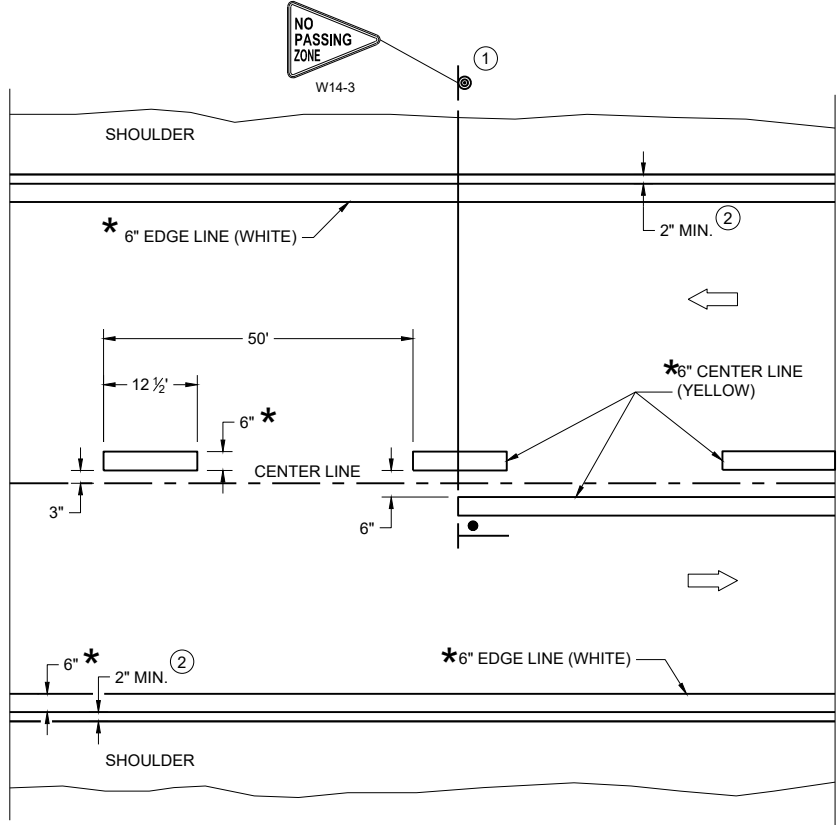
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

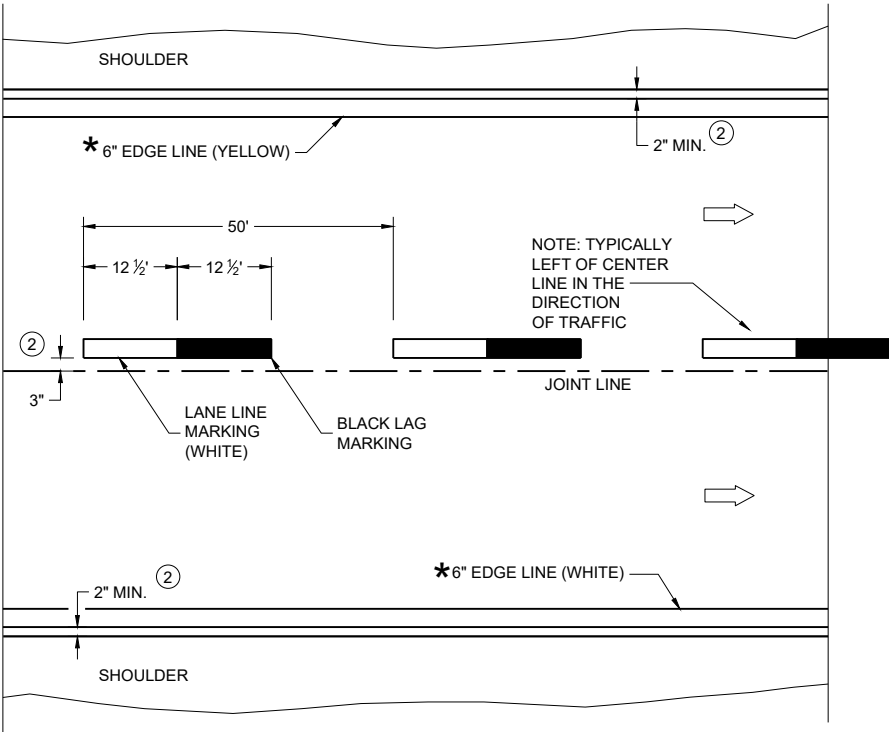
LEGEND

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



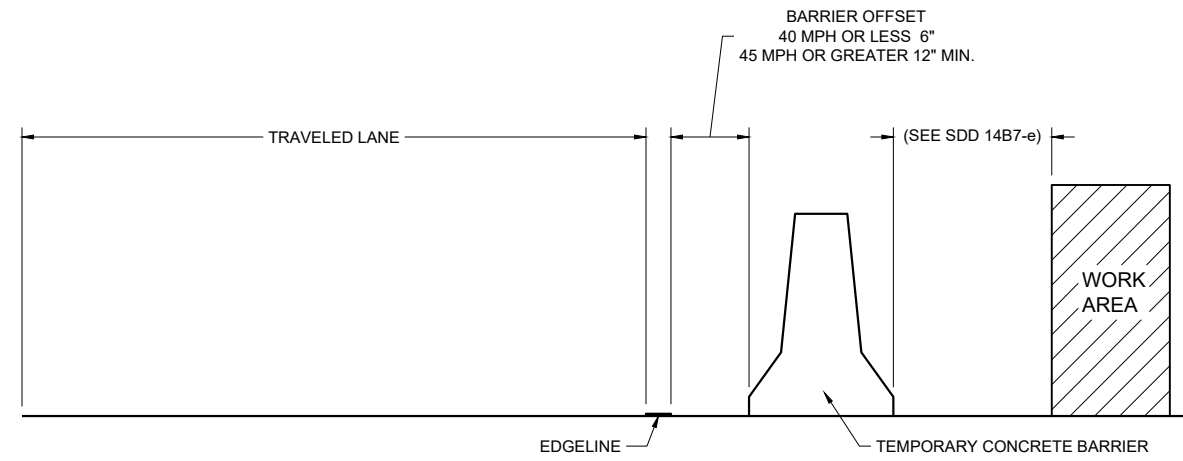
ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Jeannie Silver
DATE Statewide Pavement Marking Engineer



TEMPORARY BARRIER OFFSET FROM EDGELINE

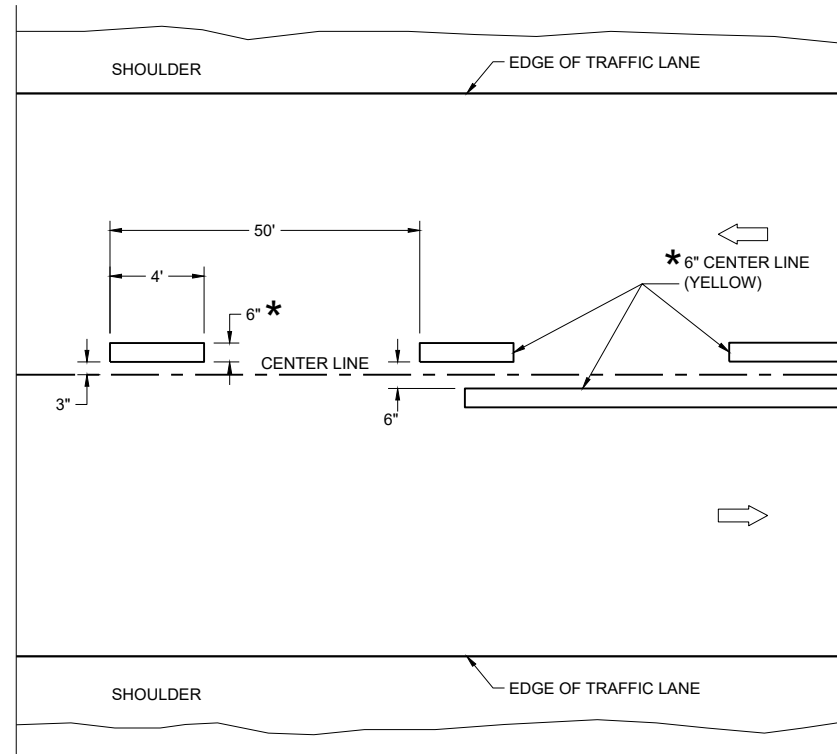
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

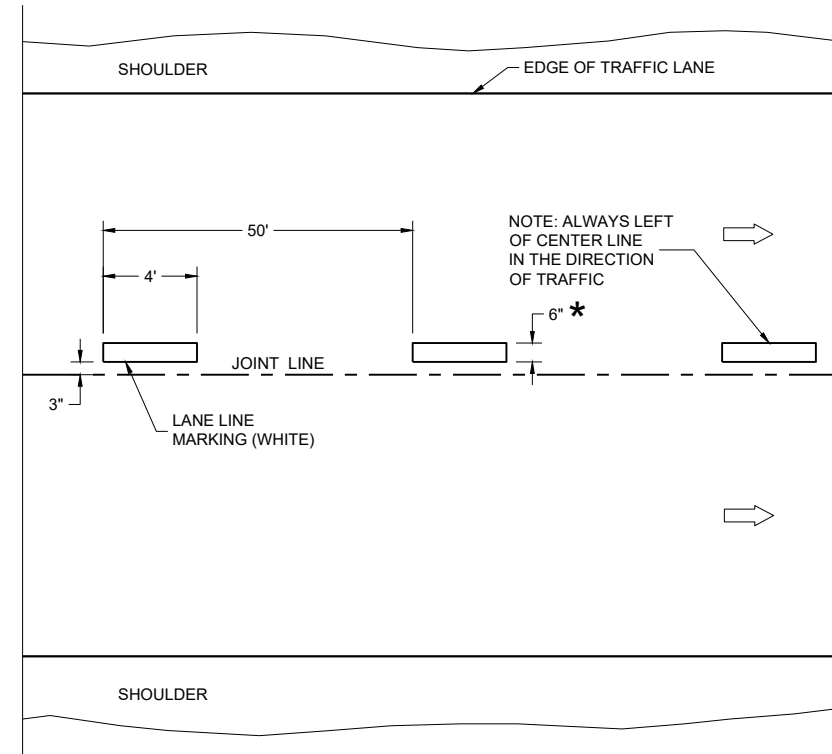
LEGEND

➡ DIRECTION OF TRAFFIC

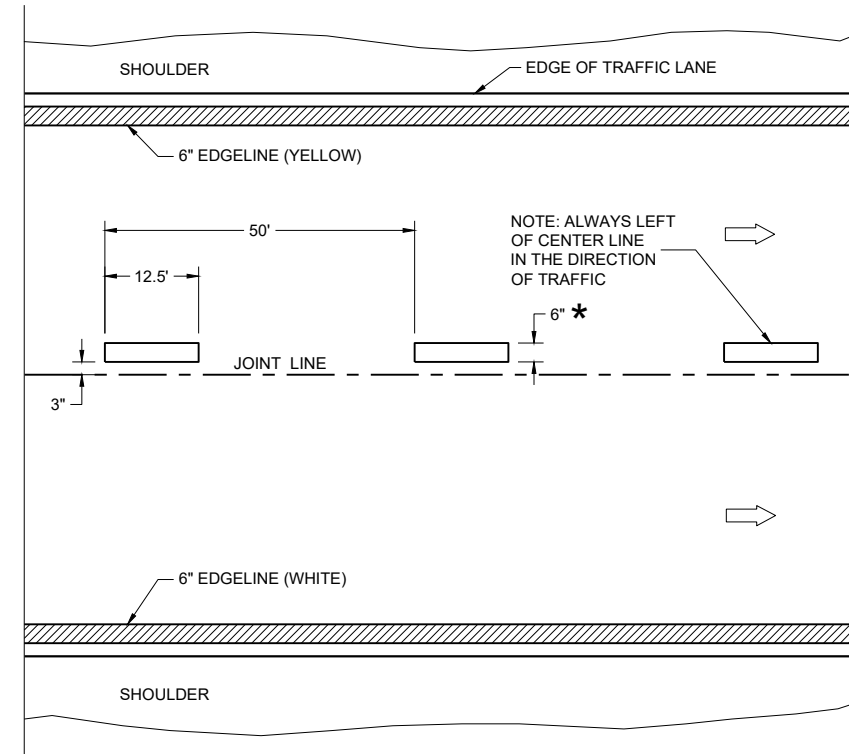
*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

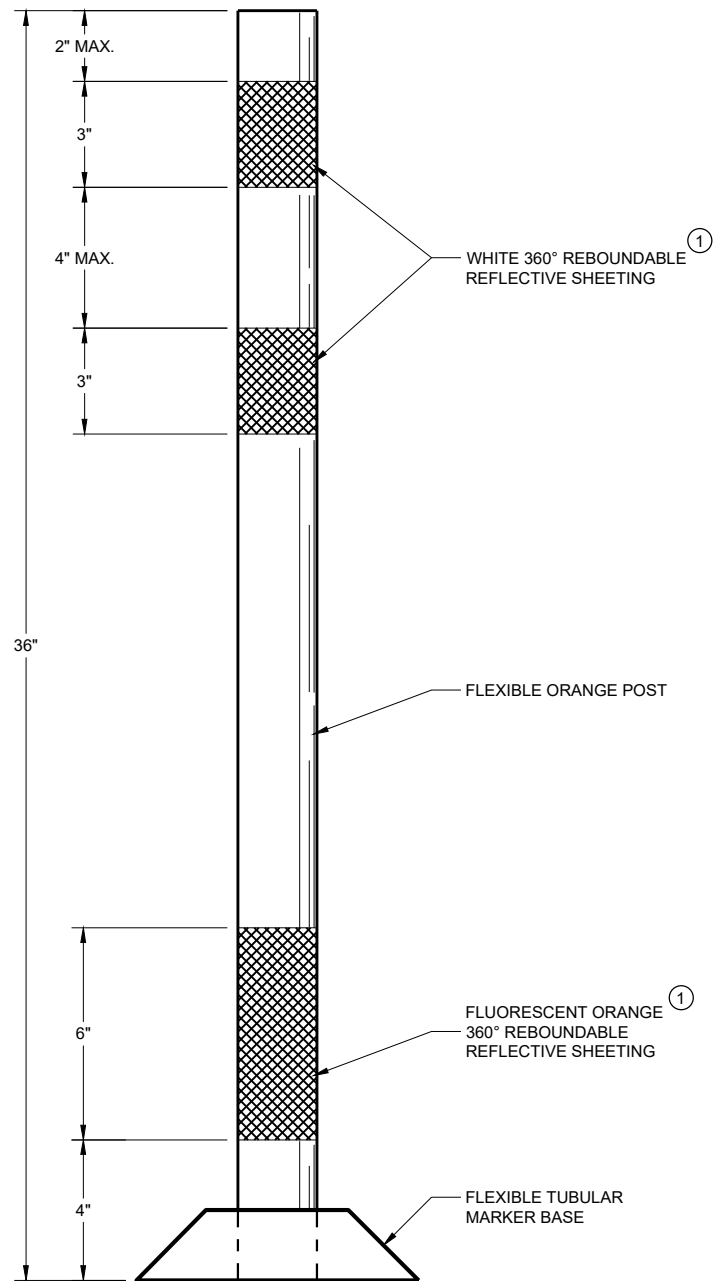
TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Jeannie Silver
DATE Statewide Pavement Marking Engineer

FHWA



FLEXIBLE TUBULAR
MARKER POST
WORK ZONE

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

SURFACE MOUNTED BASES SHALL BE FURNISHED IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS TO BE COMPATIBLE WITH FLEXIBLE TUBULAR MARKER POSTS TO A SIZE AND SHAPE THAT WILL PROVIDE A STABLE POST FOUNDATION WHEN SECURED TO THE PAVEMENT.

THE ASPHALTIC ADHESIVE OR BUTYL PAD FURNISHED SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS, UNLESS DIRECTED BY THE ENGINEER TO USE BOLTS.

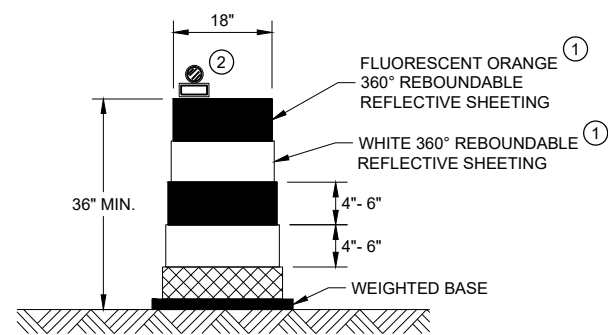
① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.

**CHANNELIZING DEVICES
FLEXIBLE TUBULAR
MARKER POST**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

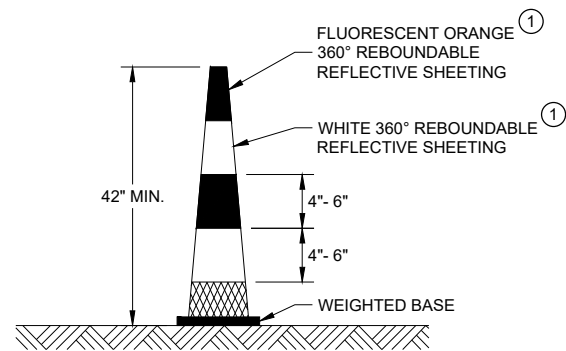
APPROVED
November 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA



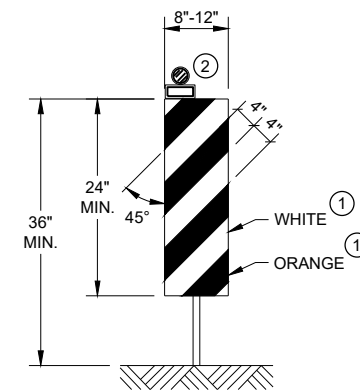
DRUM

BALLAST WIDTHS
RANGE FROM 24"-36"



42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS
BALLAST WIDTHS
RANGE FROM 14"-20"

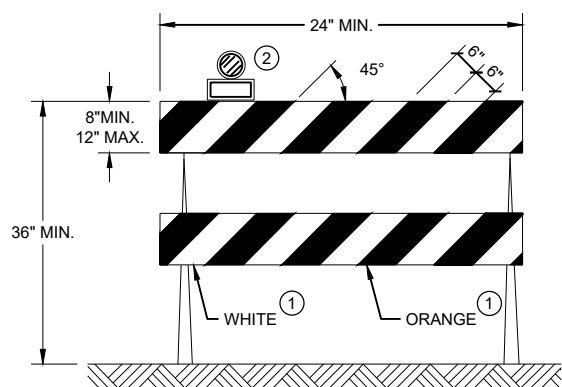


VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.

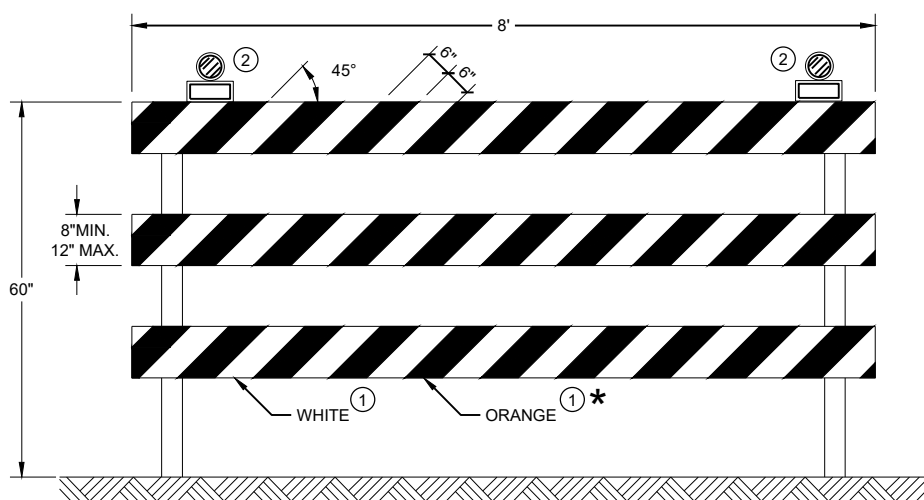
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.






* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

**CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
 November 2022 /S/ Andrew Heidtke
 DATE WORK ZONE ENGINEER
 FHWA

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

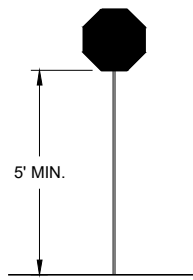
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



STOP/SLOW PADDLE ON SUPPORT STAFF

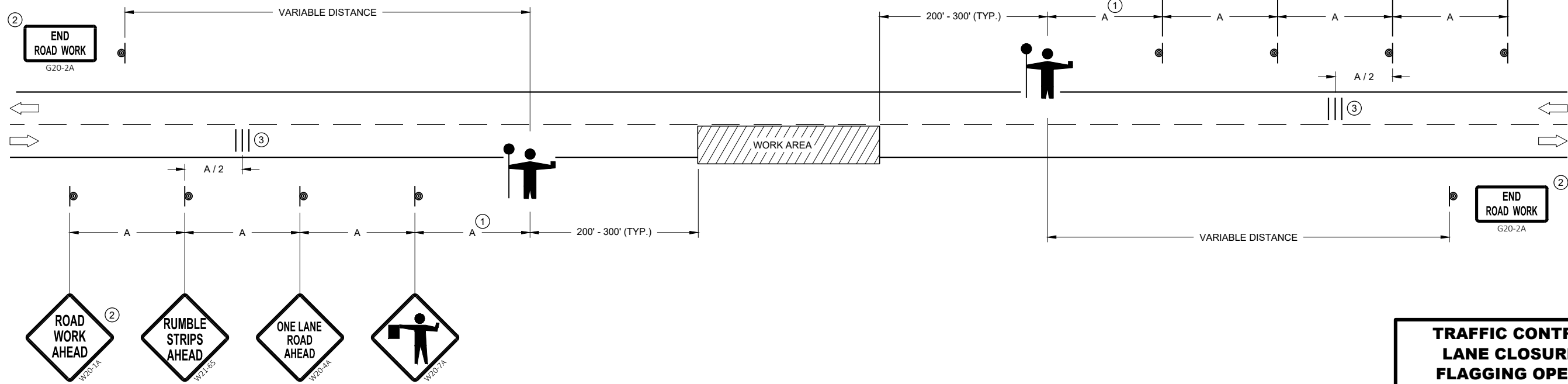
SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



W03-4

USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".






TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: May 2022 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

LEGEND

- V1** LEAD VEHICLE
- V2** MARKING VEHICLE
- V3** SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH

UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

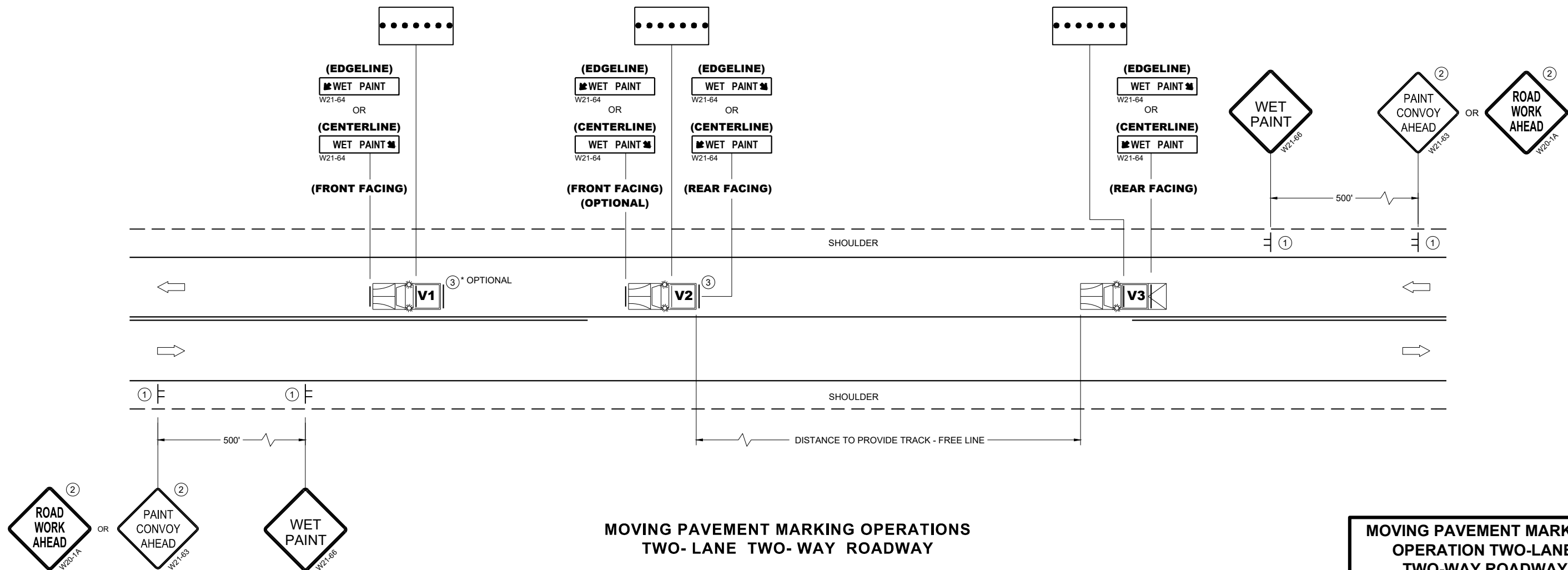
CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING .

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES AND AFTER EVERY MAJOR INTERSECTION.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.
- ③ V1 AND V2 CAN BE SWITCHED SO THAT THE MARKER IS THE LEAD VEHICLE.

6

6



**MOVING PAVEMENT MARKING OPERATIONS
TWO-LANE TWO-WAY ROADWAY**

**MOVING PAVEMENT MARKING
OPERATION TWO-LANE
TWO-WAY ROADWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2024 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

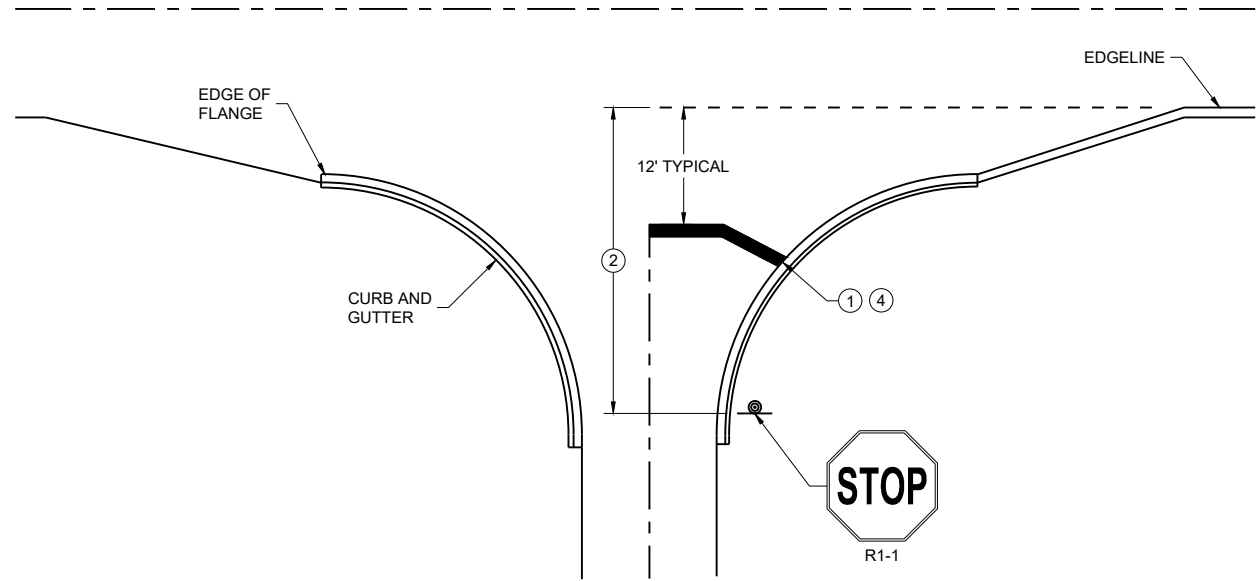
SDD 15C19-9a

SDD 15C19-9a

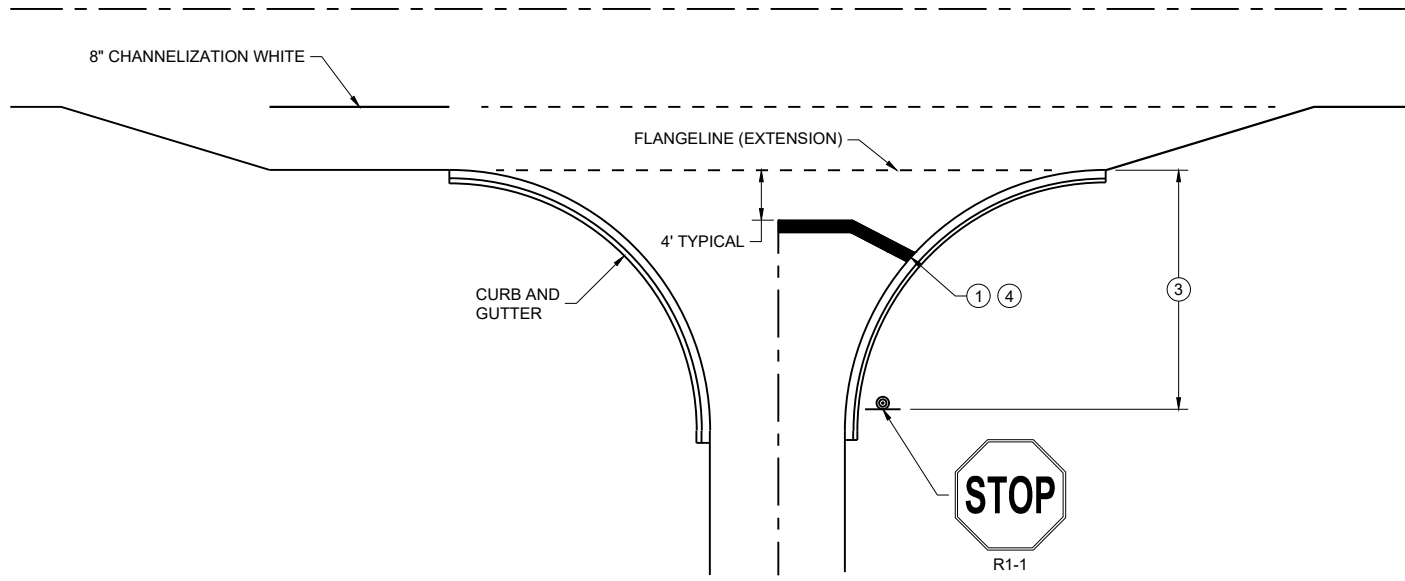
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

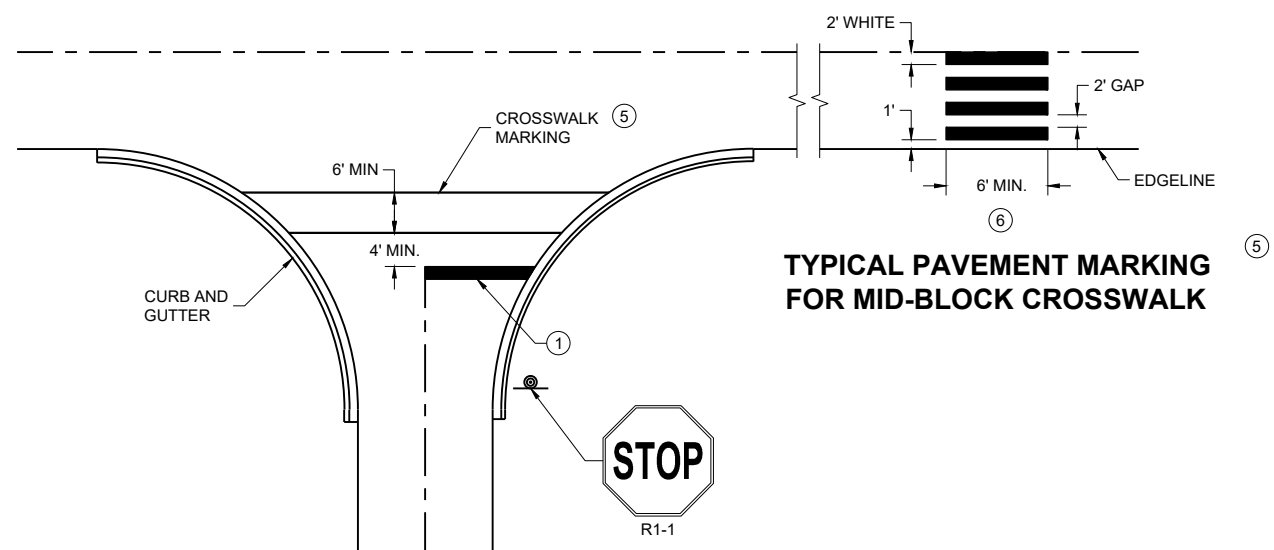
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGE LINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES.
- ⑥ POSTED SPEED LIMITS OF 40 MPH OR GREATER USE A MINIMUM WIDTH OF 8' FOR MIDBLOCK CROSSWALKS



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

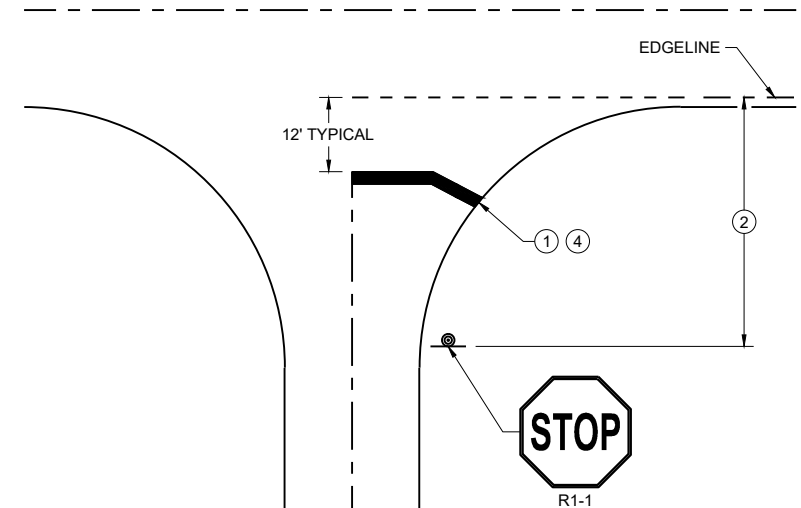


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDE ROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDE ROADS WITH CROSSWALK MARKING

TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

6

6

SDD 15C33-05

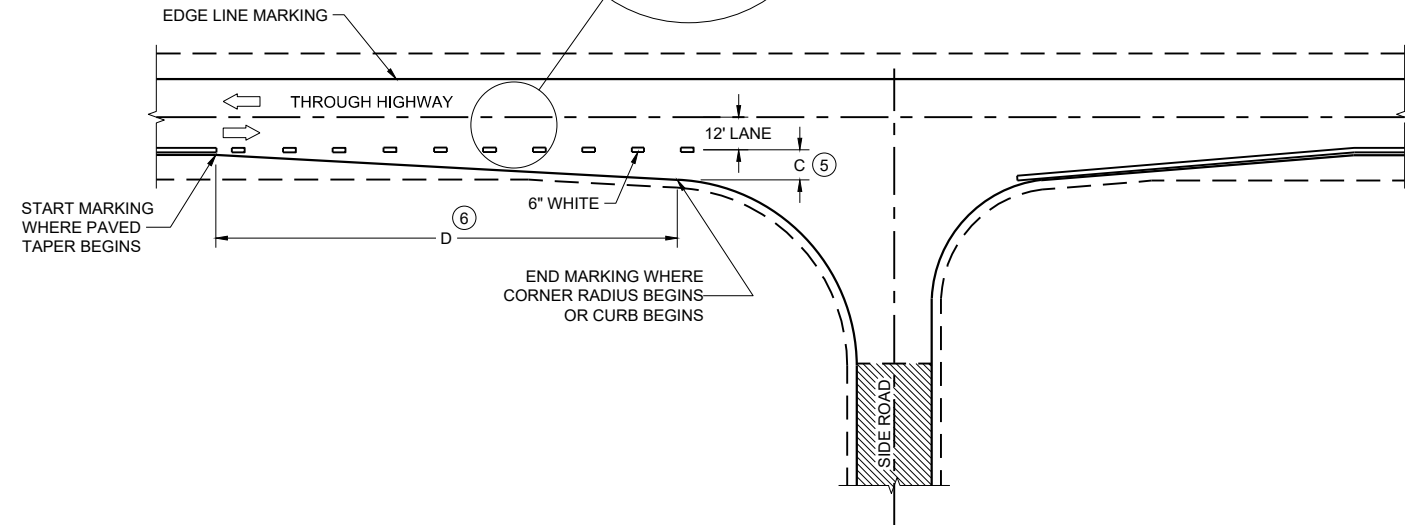
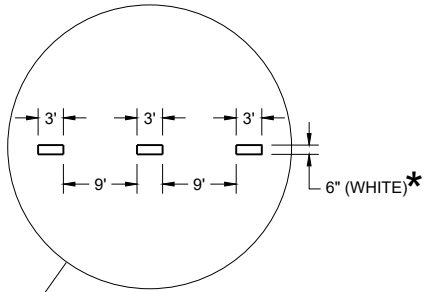
SDD 15C33-05

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2024 /s/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER

FHWA



MINOR INTERSECTION

*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

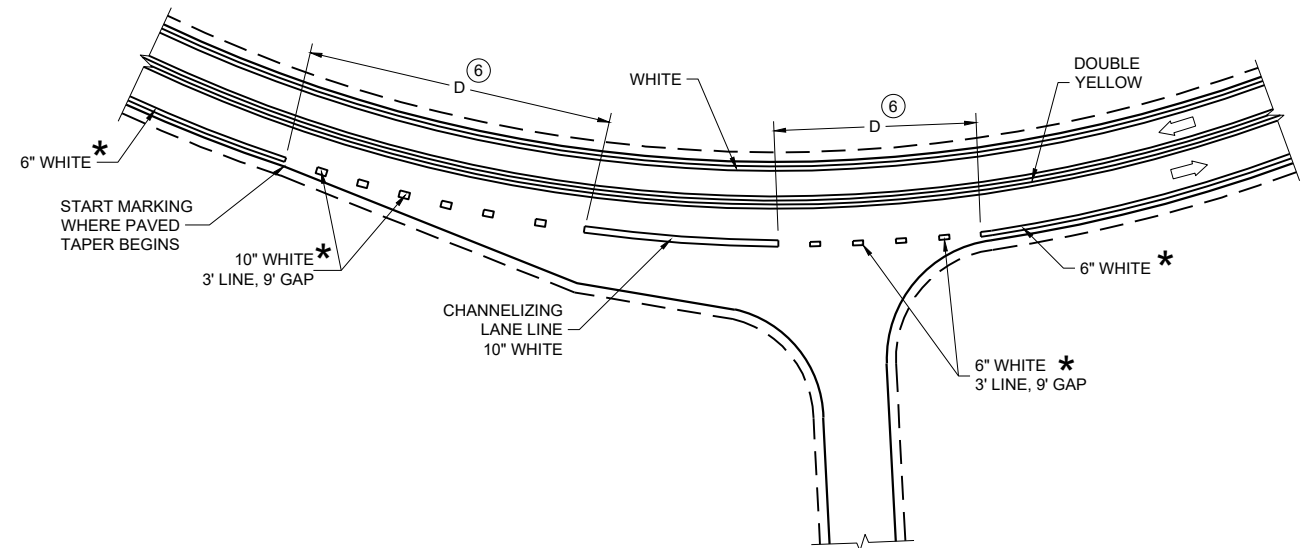
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

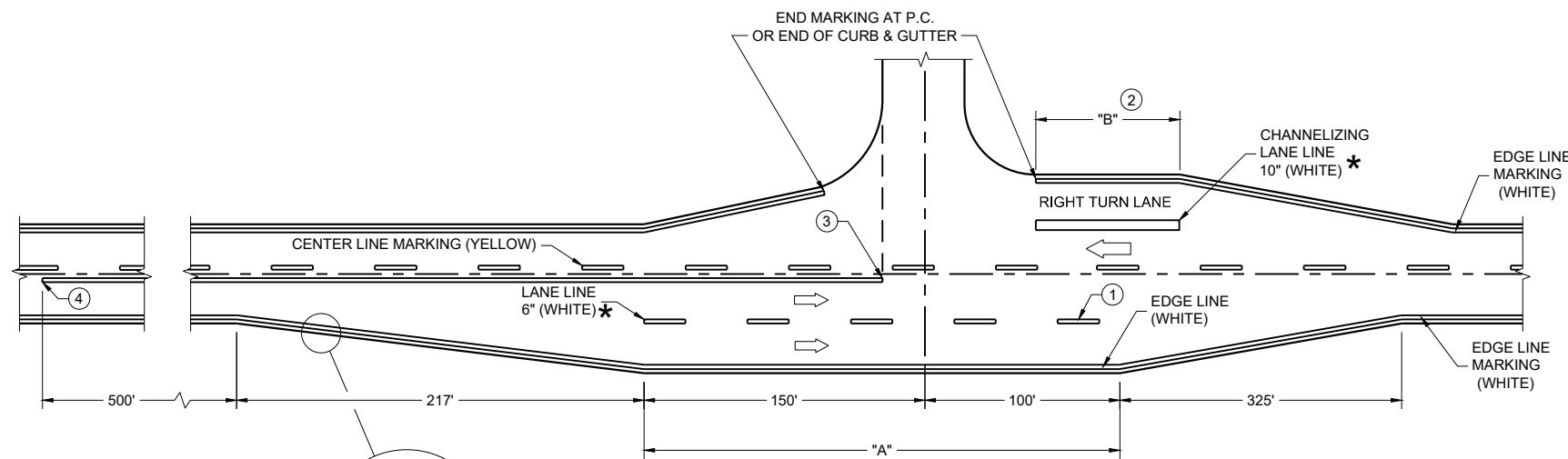
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
- ⑤ WHEN DISTANCE "C" IS LESS THAN 4 FEET, OMIT DOTTED EXTENSION.
- ⑥ WHEN DISTANCE "D" IS LESS THAN 50 FEET, OMIT DOTTED EXTENSION.

LEGEND

➡ DIRECTION OF TRAVEL

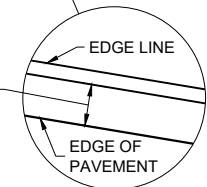


INTERSECTION ON OUTSIDE OF CURVE



**MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**





BYPASS LANE PAVED SHOULDER WIDTH (AS SHOWN ELSEWHERE IN PLANS) - PLUS 2 INCHES



**PAVEMENT MARKING
(INTERSECTIONS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  DIRECTION OF TRAFFIC
-  WORK ZONE

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

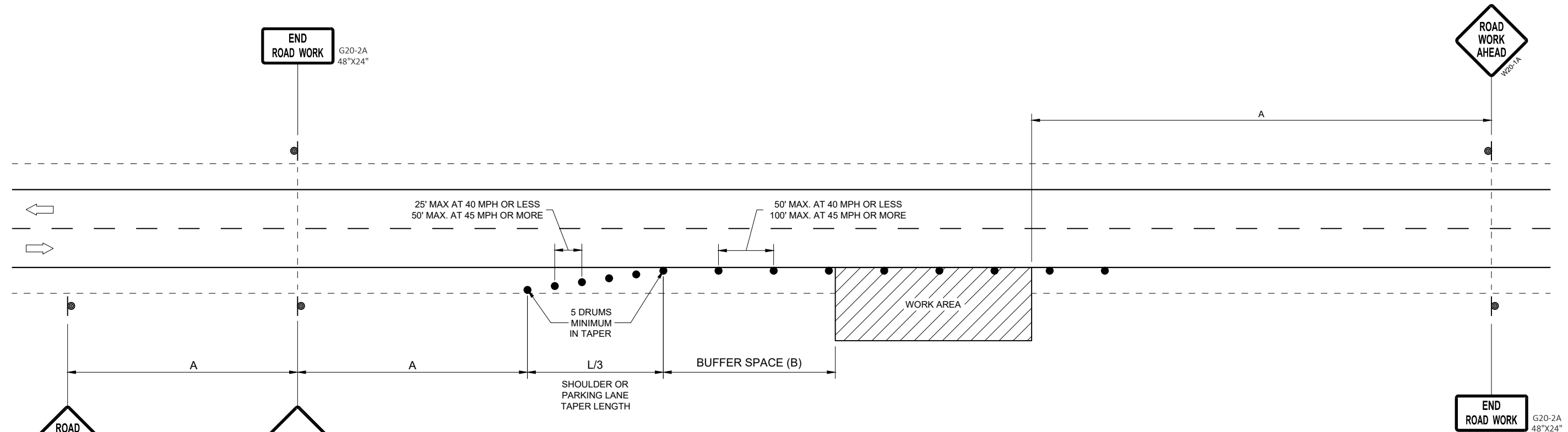
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

6

6



OR
IF TRAFFIC CONTROL DEVICES
ENCROACH ONTO TRAVELED WAY, USE

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHOULDER TAPER L / 3 W, LATERAL OFFSET (FT)						BUFFER SPACE (B) FEET
		3	4	5	6	7	8	
25	200'	10	14	17	21	24	28	55
30	200'	15	20	25	30	35	40	85
35	350'	20	27	34	40	47	54	120
40	350'	26	35	44	53	62	70	170
45	500'	45	59	74	89	104	119	220
50	500'	50	66	83	99	116	132	280
55	500'	54	73	91	109	127	145	335'

**TRAFFIC CONTROL, WORK ON
SHOULDER OR PARKING LANE,
UNDIVIDED ROADWAY**

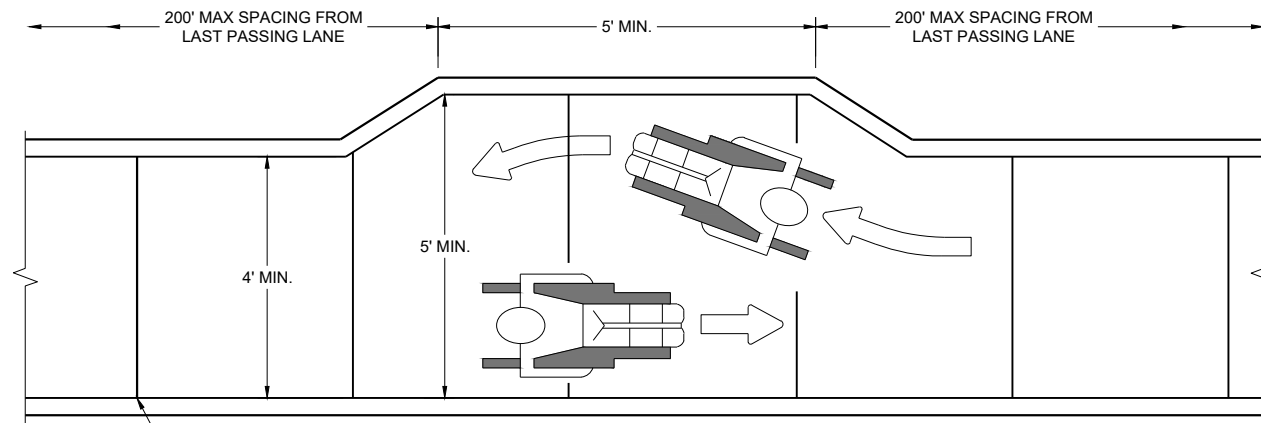
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2020 /S/ Andrew Heidtke
DATE STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

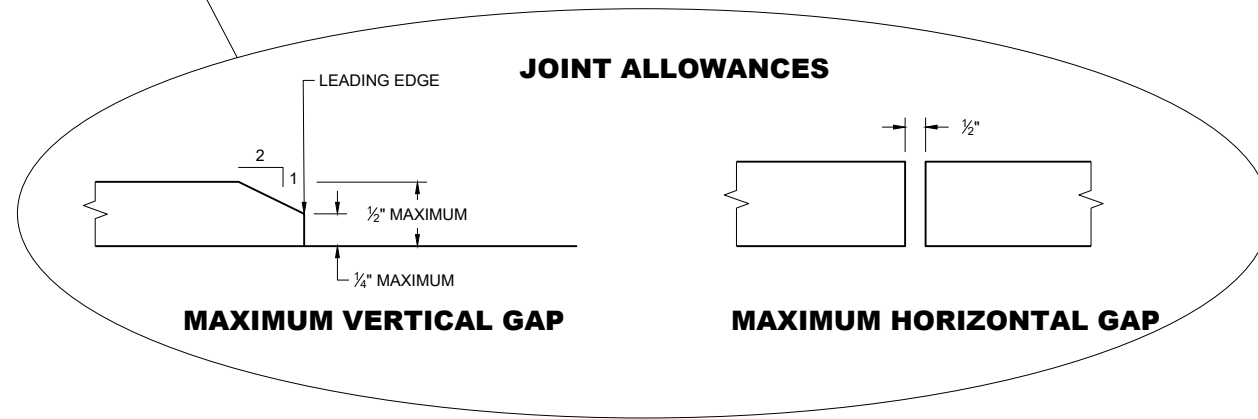
FHWA

SDD 15D28 - 04

SDD 15D28 - 04



NARROW SIDEWALK PASSING DETAIL

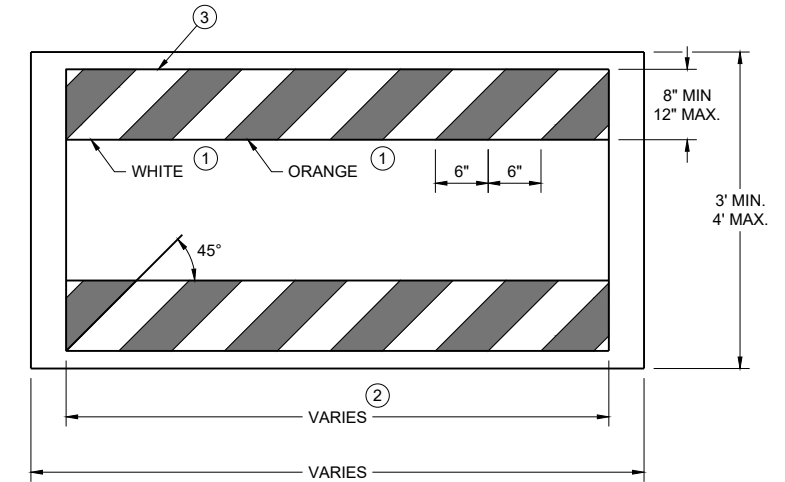


MAXIMUM VERTICAL GAP

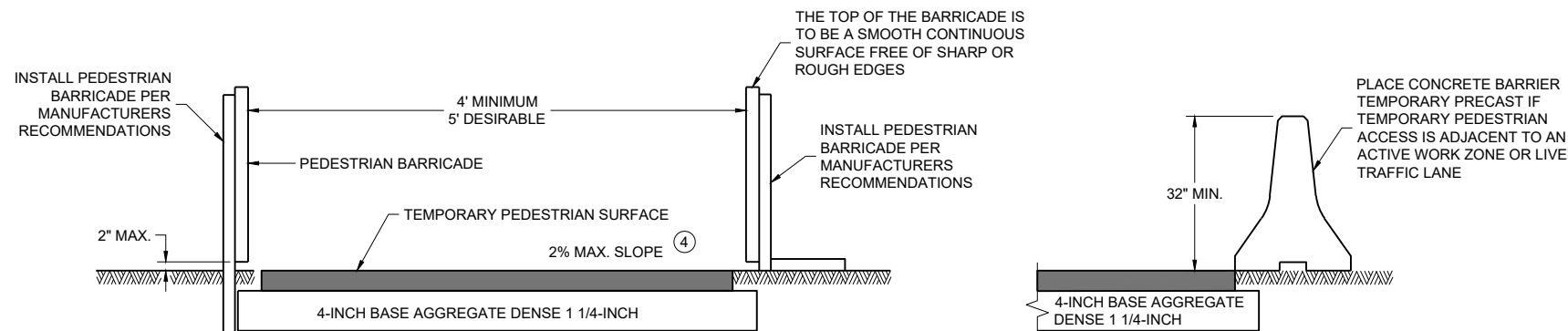
MAXIMUM HORIZONTAL GAP

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- * USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.
- ④ WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.



TEMPORARY PEDESTRIAN BARRICADE *



TEMPORARY PEDESTRIAN ACCESS

**TRAFFIC CONTROL,
PEDESTRIAN
ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

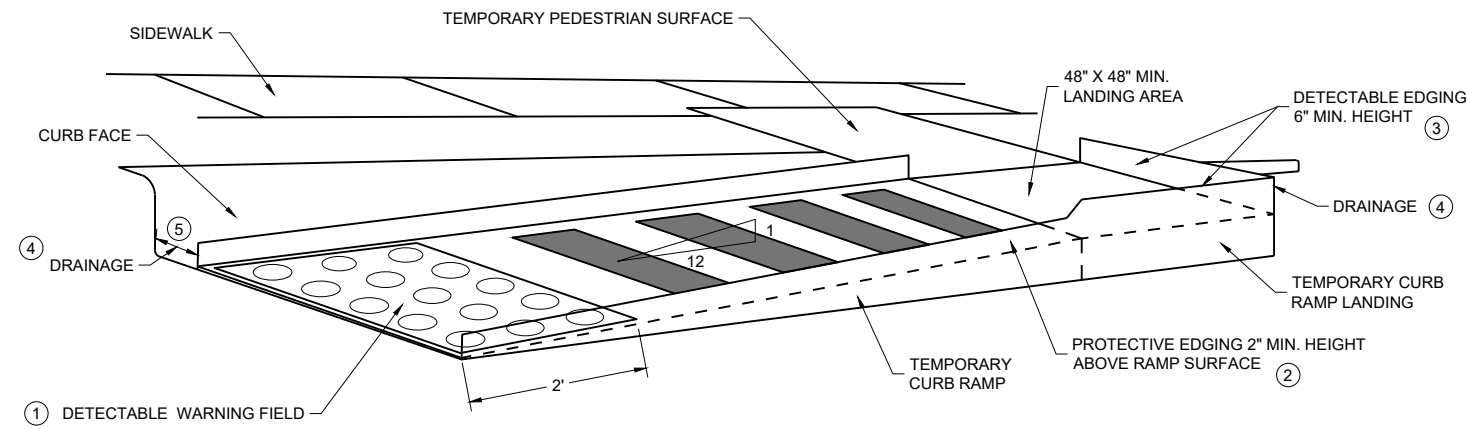
CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

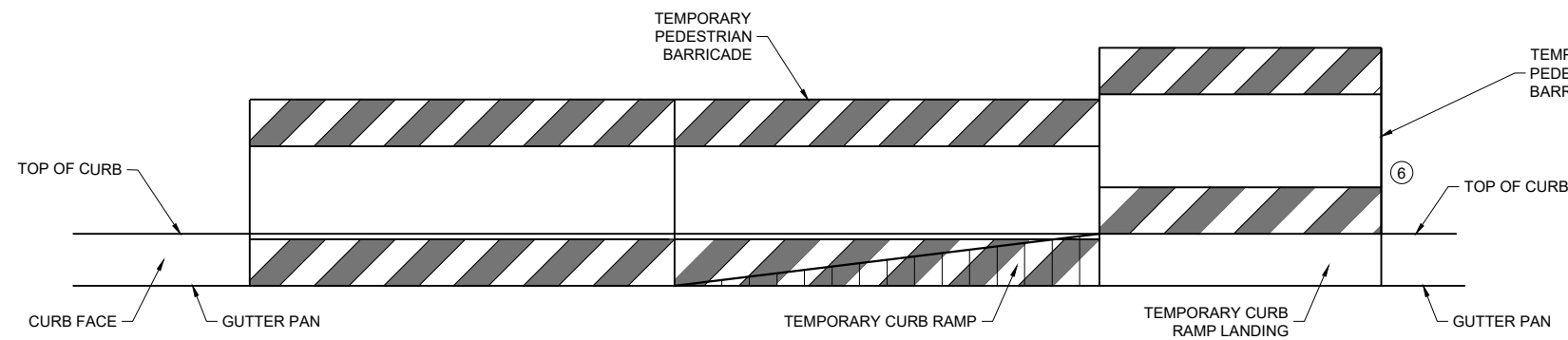
CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP. LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

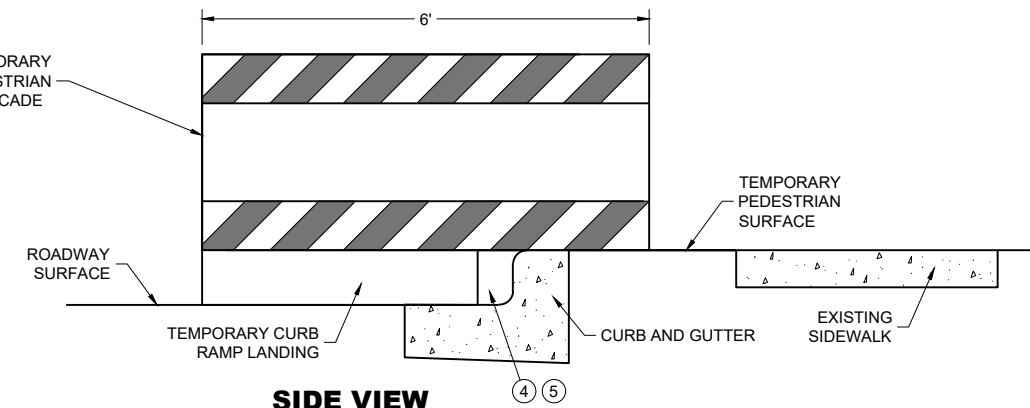
- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ ENSURE CURB RAMP IS OUT OF THE GUTTER PAN.
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



PERSPECTIVE VIEW

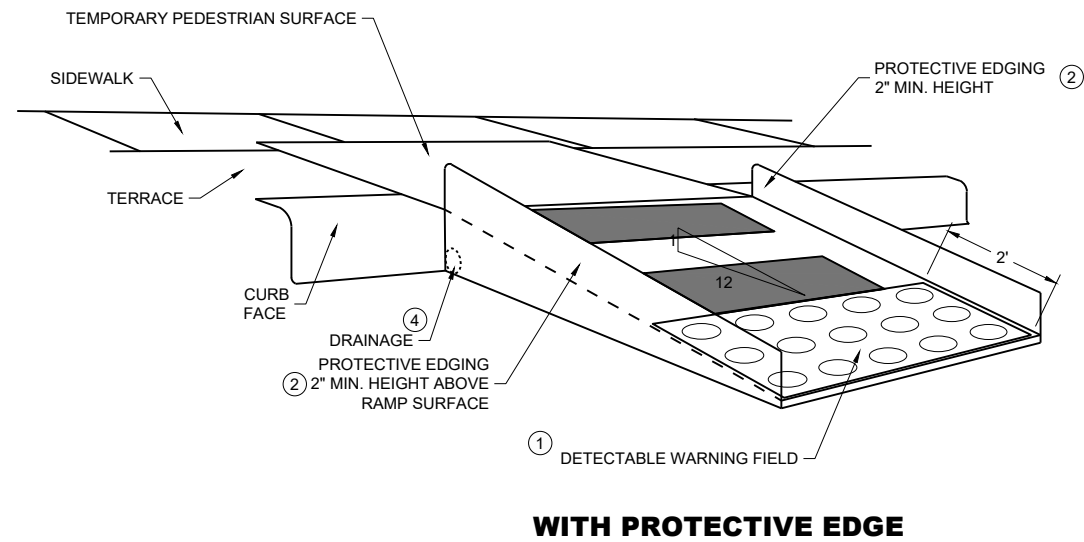
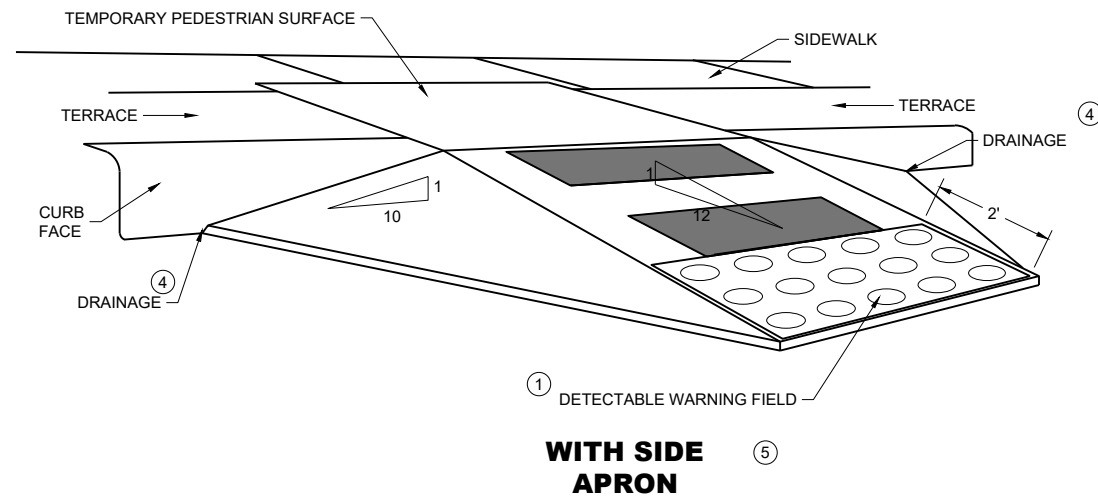


FRONT VIEW



SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB



TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

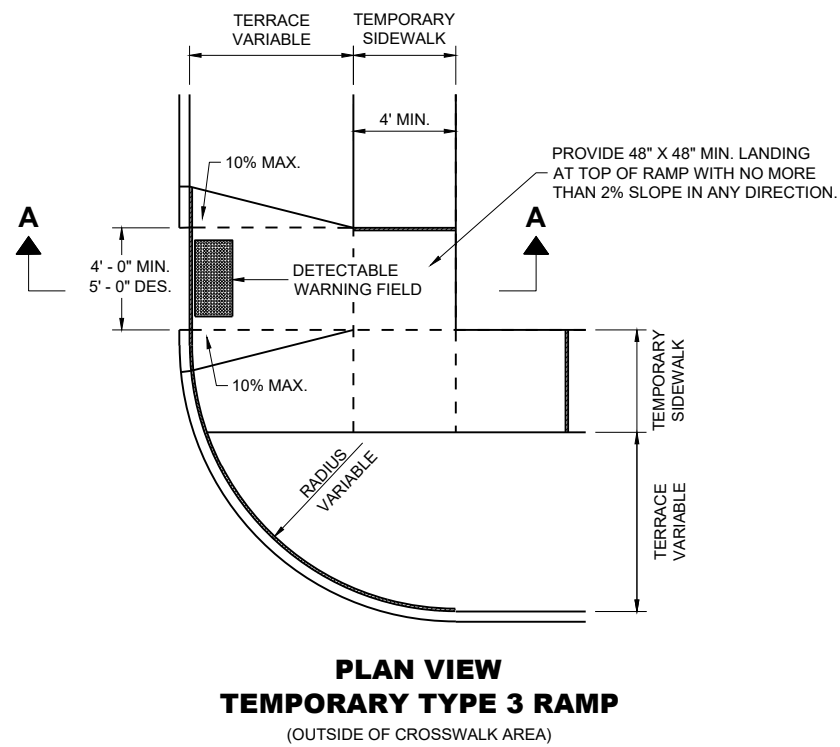
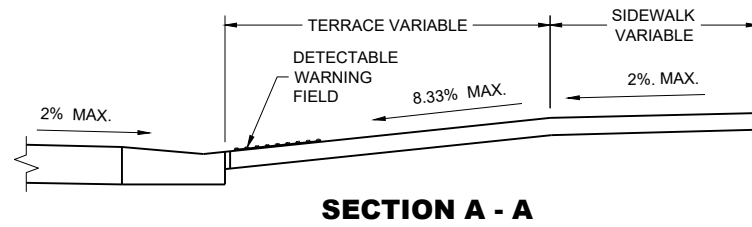
CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

GENERAL NOTES



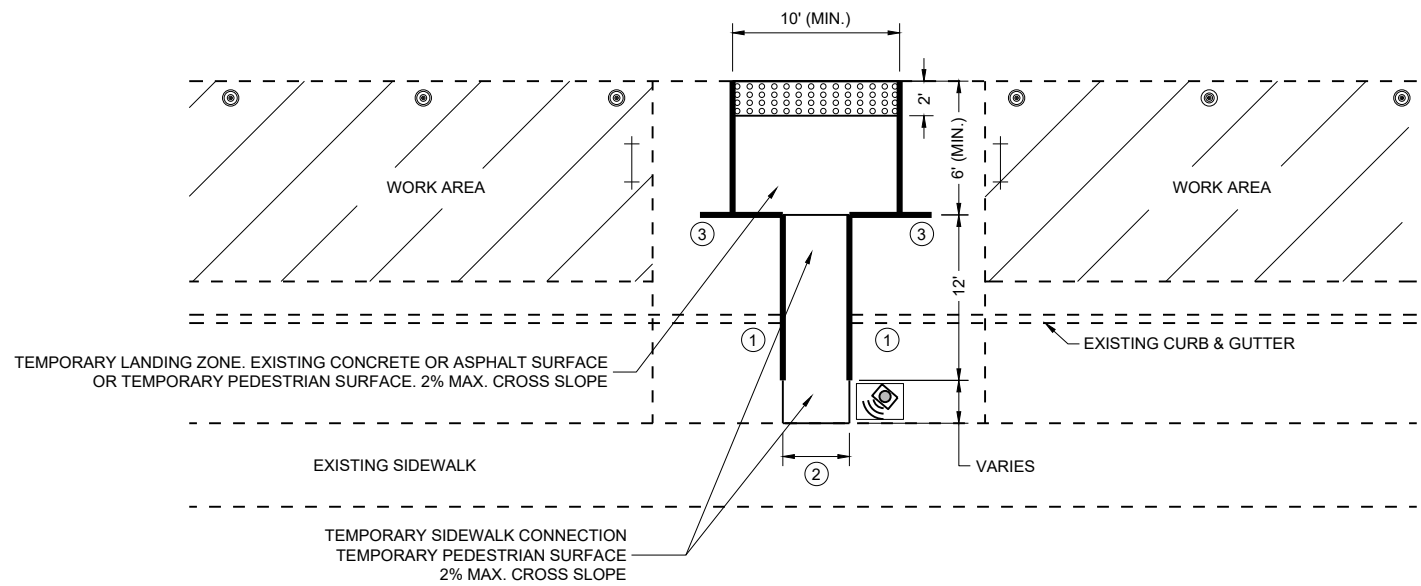
6

6

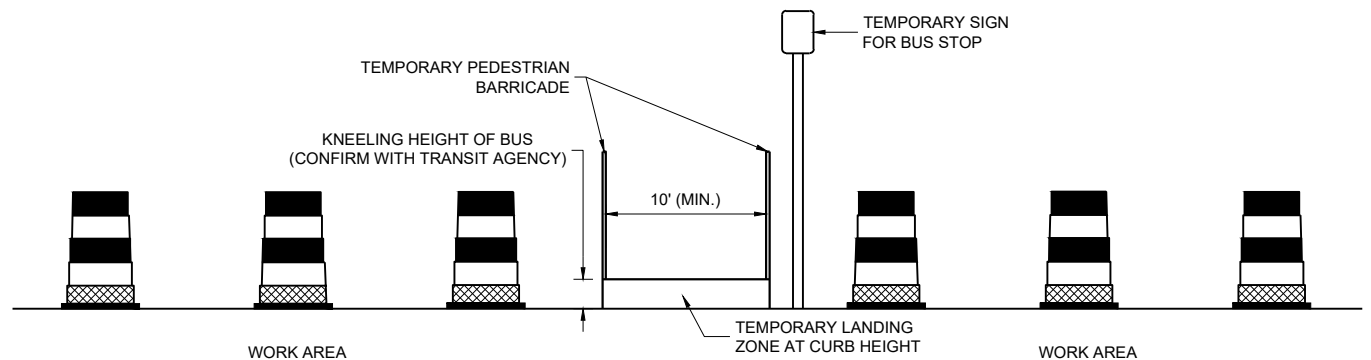
SDD 15D30-10d

SDD 15D30-10d

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Andrew Heidtke <position>
FHWA	



PLAN VIEW



PROFILE VIEW
TEMPORARY BUS STOP PAD

GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.
- PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMP OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
- CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".
- CURB RAMP AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

- ① DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ② 5' WIDE MIN. WITH TEMPORARY PEDESTRIAN BARRICADE, 10' WIDE MIN. WITHOUT TEMPORARY PEDESTRIAN BARRICADE.
- ③ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE INTO THIS SPACE.








LEGEND

- ⊙ TRAFFIC CONTROL DRUM
- ⊥ TYPE III BARRICADE
- ▬ TEMPORARY PEDESTRIAN BARRICADE
- ◻ TEMPORARY DETECTABLE WARNING FIELD
- ▨ WORK AREA
- Ⓜ TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC
-  TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)

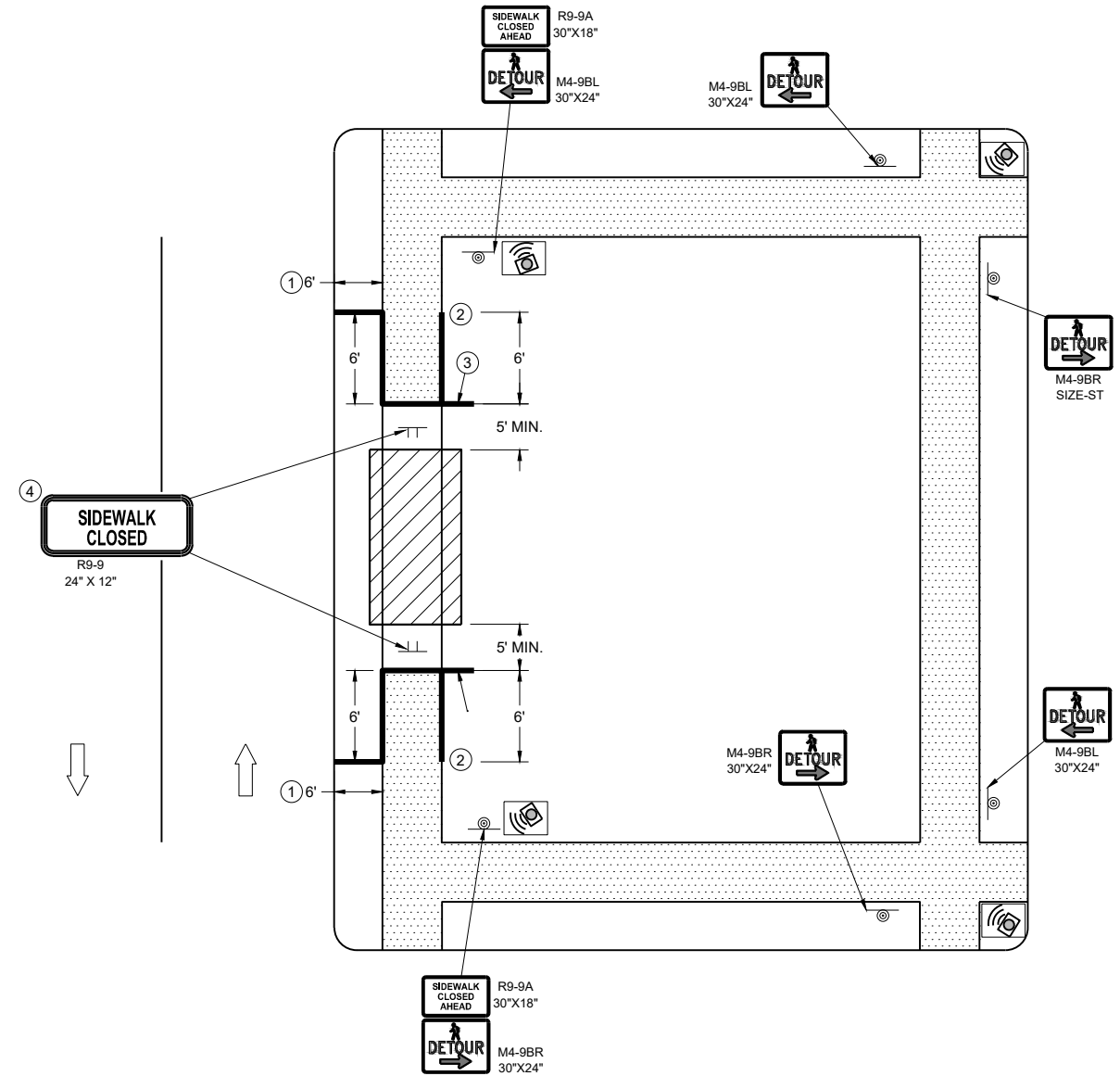
GENERAL NOTES

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.








PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICTS WITH DRIVEWAYS AND OTHER EXISTING FEATURES.

- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY PEDESTRIAN SURFACE
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

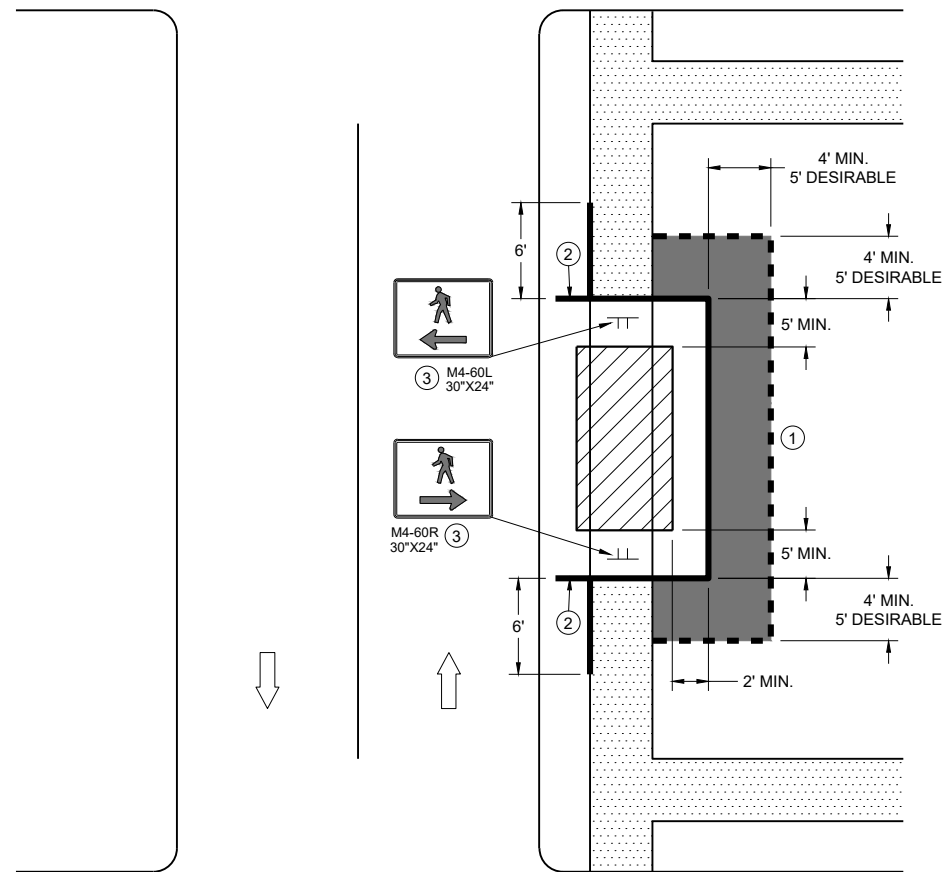
GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.










SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

- ① USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ② IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



**SIDEWALK BYPASS
SINGLE SIDE**

LEGEND

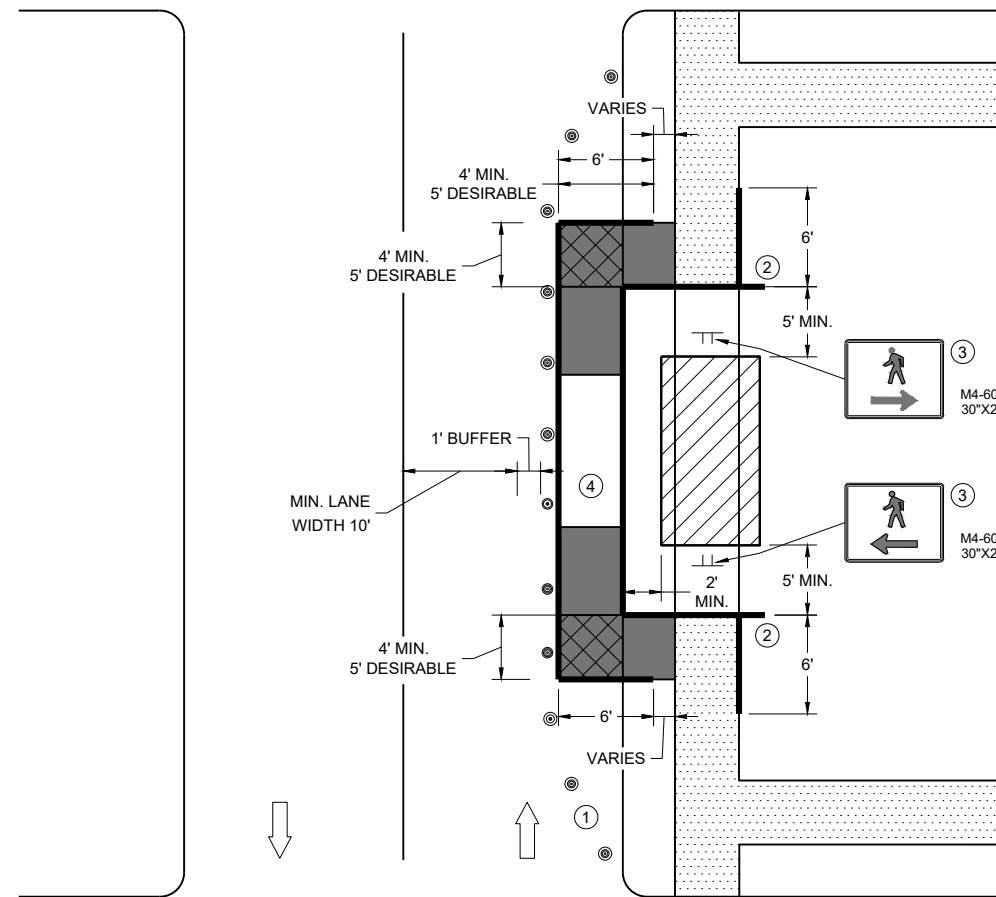
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
- ② PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
- ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
- ④ USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURFACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE. WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.



SIDEWALK BYPASS, SINGLE SIDE

6

6

SDD 15D30-10h

SDD 15D30-10h

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.












TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

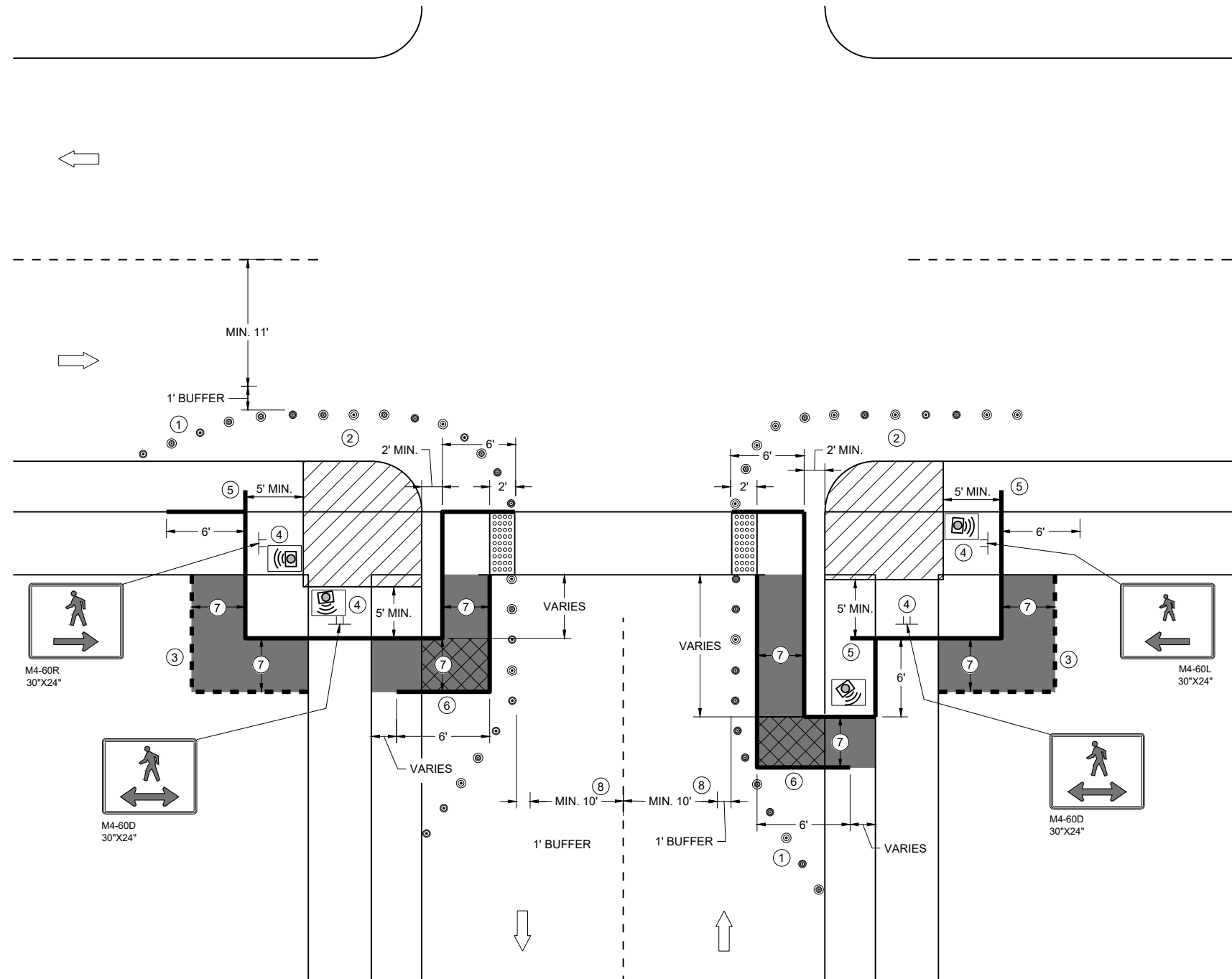
WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑦ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑧ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC
-  TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)



**CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE**

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.












TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

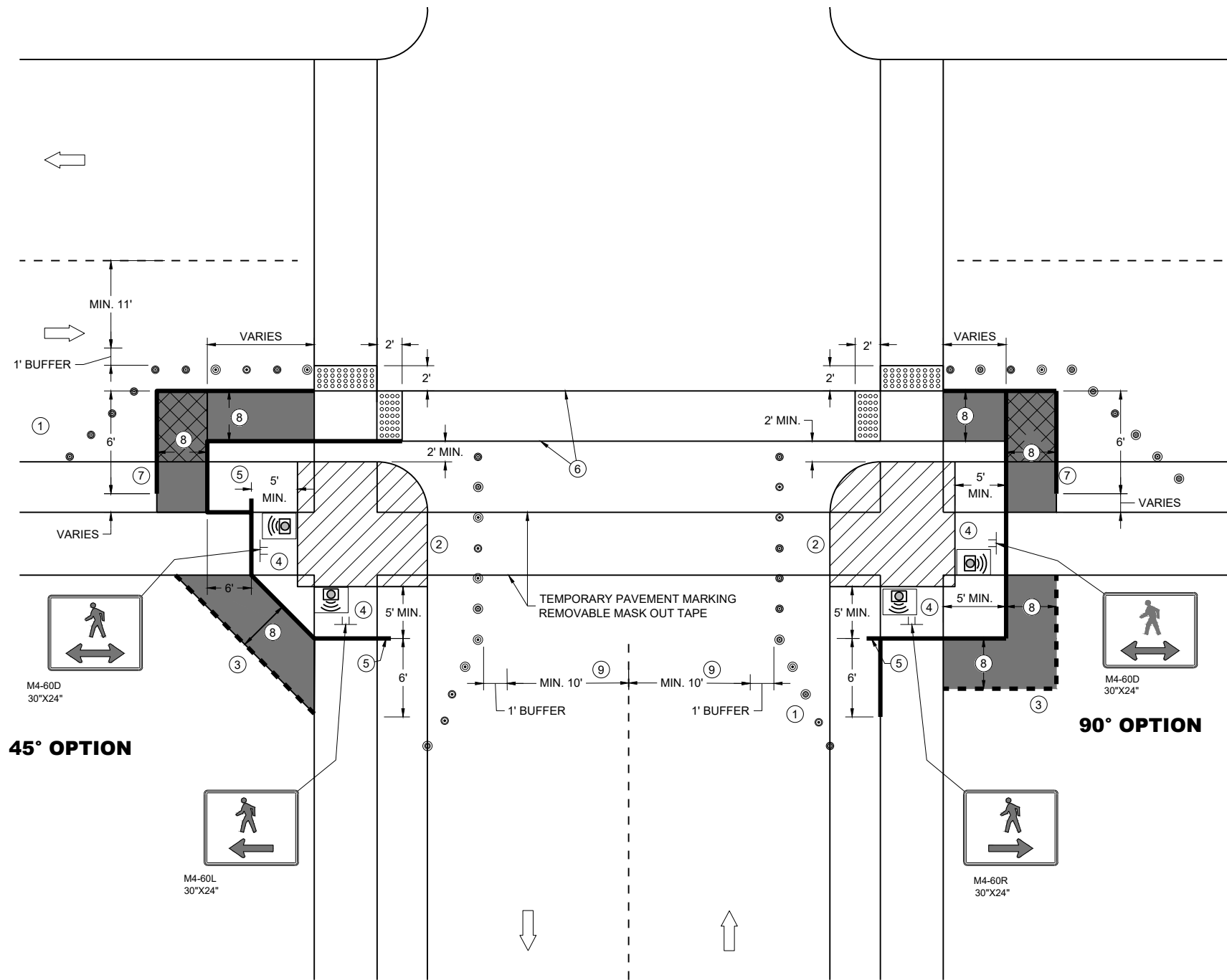
WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ WHITE 6" TEMPORARY PAVEMENT MARKING
- ⑦ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑧ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑨ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC
-  TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)



CURB RAMP PEDESTRIAN TRAFFIC CONTROL

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION








6

6

SDD 15D30-10j

SDD 15D30-10j

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC
-  TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)

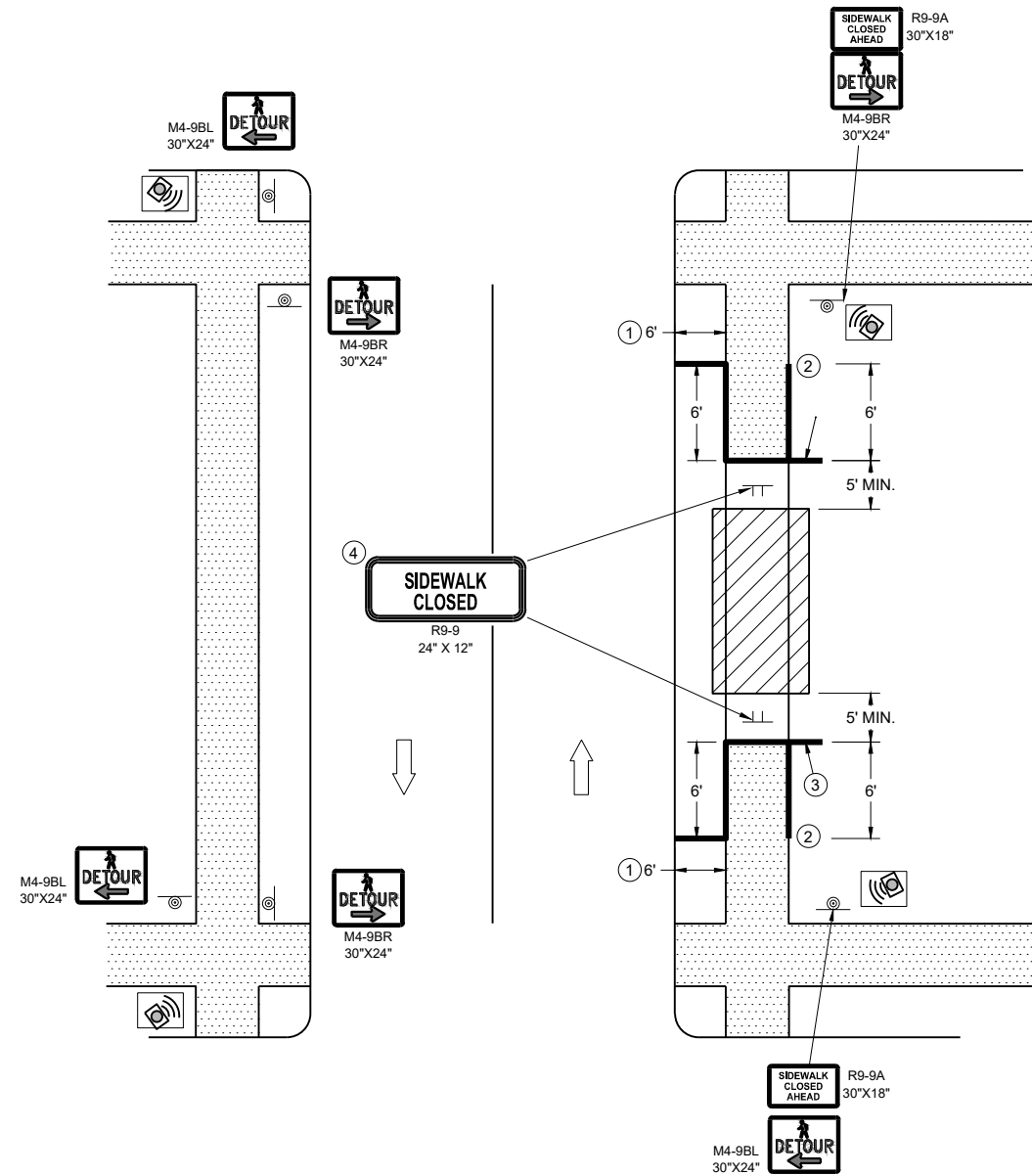
GENERAL NOTES

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICT WITH DRIVEWAYS AND OTHER EXISTING FEATURES.

- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ON BOTH SIDES

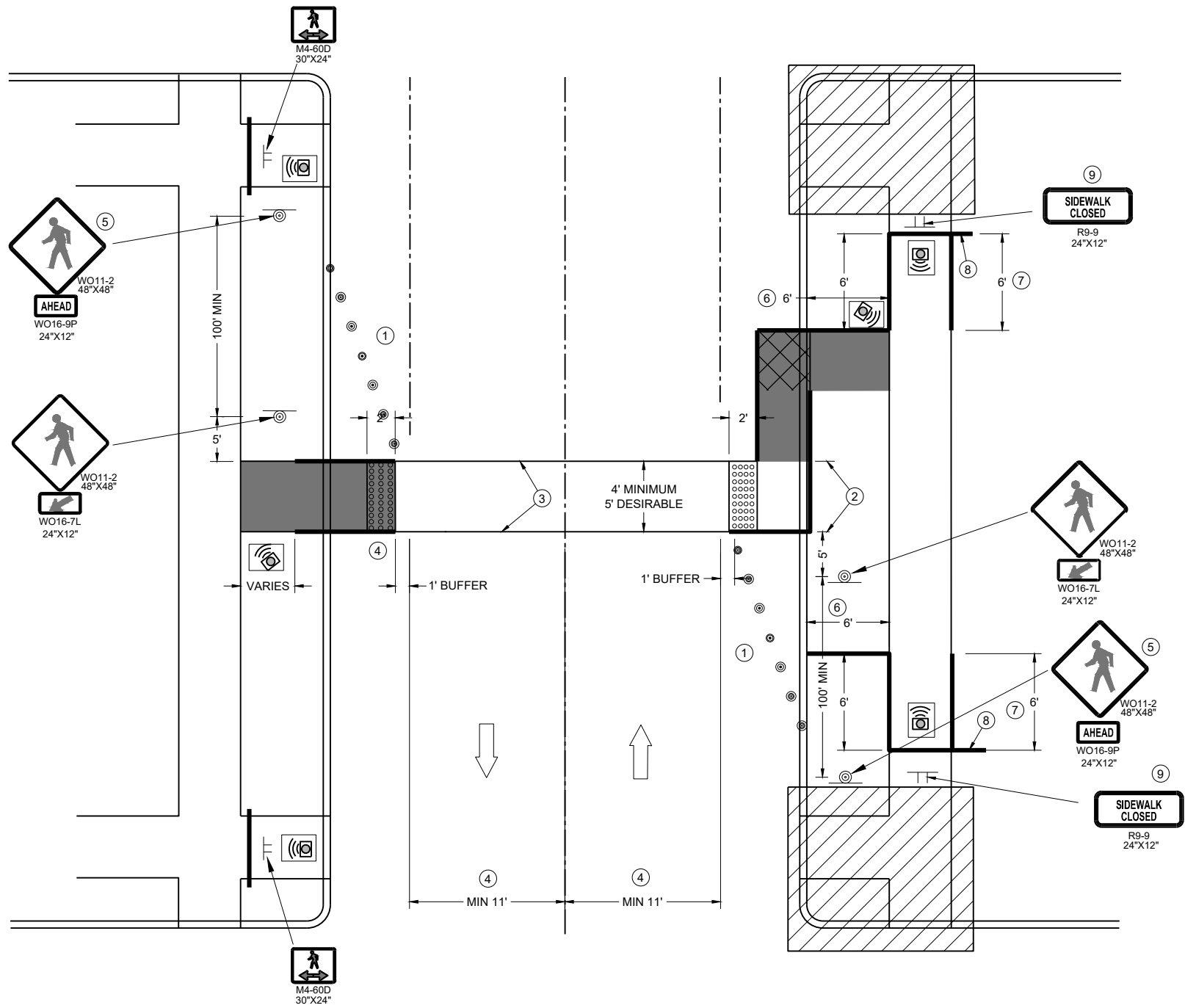
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

6

6

SDD 15D30-10K

SDD 15D30-10K



TEMPORARY PEDESTRIAN CROSSING

GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
SEE OTHER PEDESTRIAN ACCOMMODATION DETAILS FOR SIGNING AND DEVICES FOR DIFFERENT PEDESTRIAN FACILITIES CLOSURES.

WHEN THE TEMPORARY PEDESTRIAN ACCESS ROUTE RUNS PARALLEL ON THE ROADWAY SURFACE, THE MAXIMUM CROSS SLOPE WILL MATCH THE EXISTING ROADWAY CROSS SLOPE.

- ① SHOULDER OR LANE CLOSURE ADVANCED WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② 4 FEET MINIMUM, 5 FEET DESIRABLE.
- ③ WHITE 6" TEMPORARY PAVEMENT MARKING.
- ④ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, PERPENDICULAR CURB RAMPS MAY NEED TO BE UTILIZED.
- ⑤ IF MINIMUM 100' SPACING FROM THE MID-BLOCK CROSSING CANNOT BE ATTAINED BEFORE THE INTERSECTION, REMOVE THIS SIGN ASSEMBLY.
- ⑥ IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ⑦ PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ⑧ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF THE EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ⑨ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF THE SIGN.

LEGEND

- TRAFFIC CONTROL DRUM
- SIGN ON TEMPORARY SUPPORT
- TEMPORARY CURB RAMP
- TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- WORK AREA
- TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC
- TEMPORARY AUDIBLE MESSAGE DEVICE (EXACT PLACEMENT BASED UPON FIELD CONDITIONS)

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

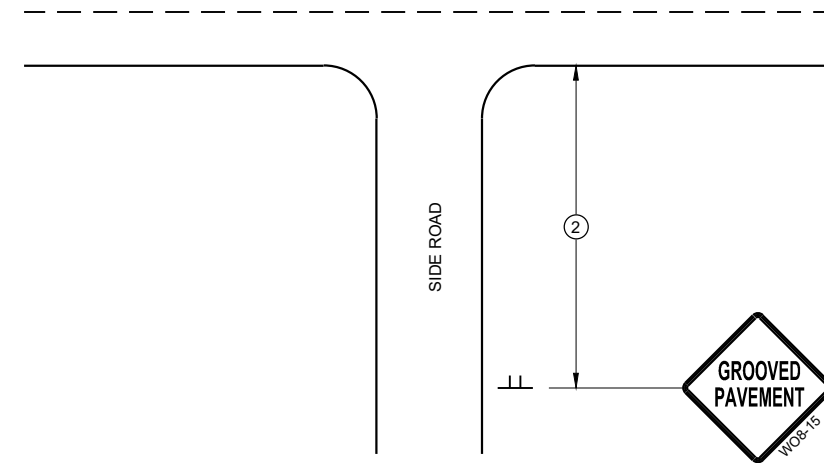
SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

- ① PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- ② PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

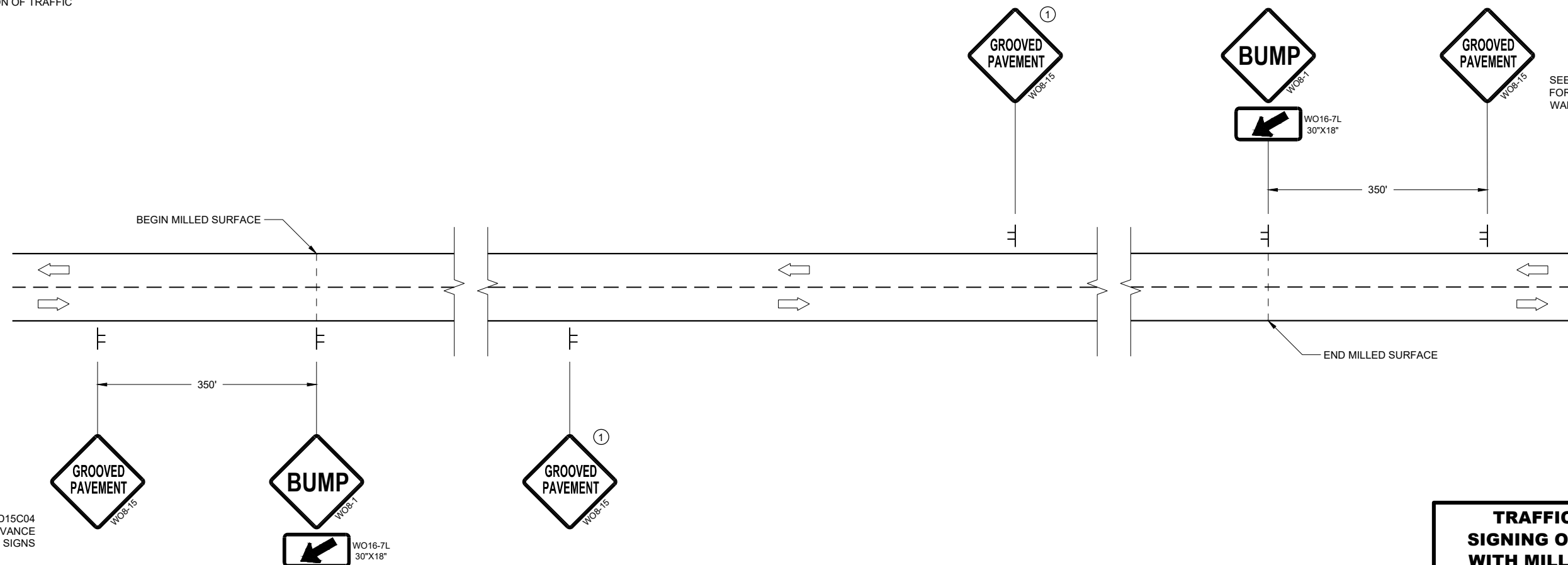
LEGEND

⌋ SIGN ON TEMPORARY SUPPORT

➡ DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH SIGN DETAIL



DETAIL FOR SIGNING ON MILLED SURFACES

SEE SDD15C04 FOR ADVANCE WARNING SIGNS





TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

- V1 WORK VEHICLE
- V2 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  FLASHING ARROW PANEL (CAUTION)
-  WORK AREA
-  DIRECTION OF TRAFFIC

POSTED SPEED PRIOR TO WORK STARTING (MPH)	DECISION SIGHT DISTANCE (D)
0 - 25	550'
30	550'
35	700'
40	700'
45	900'
50	900'
55	1200'

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

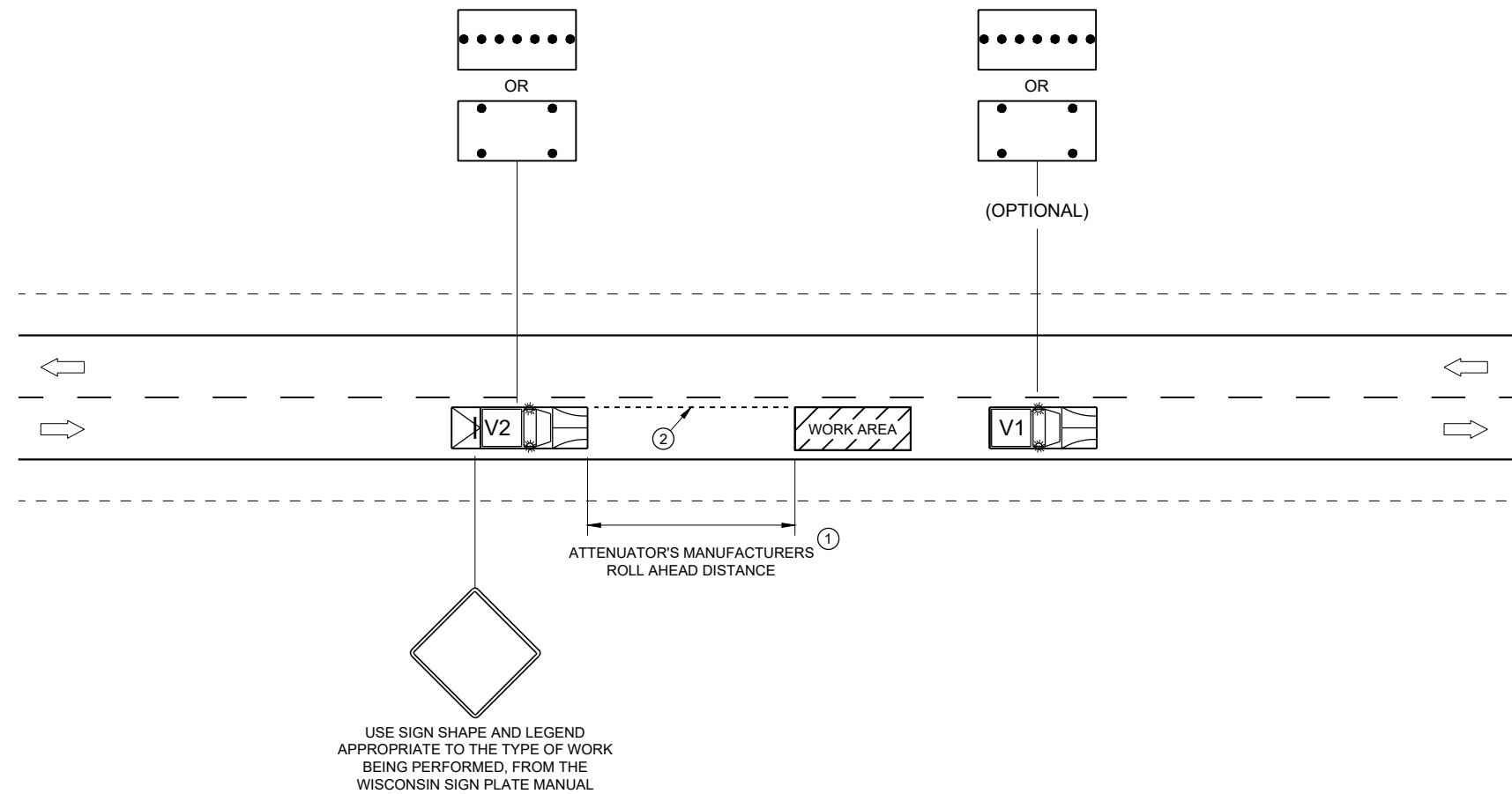
MOBILE IS WORK THAT MOVES CONTINUOUSLY OR MOVES AT LEAST THE DECISION SIGHT DISTANCE EVERY 15 MINUTES.

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL ARROW PANELS SHALL BE REAR FACING, TYPE "B" OR "C", AND DISPLAYING THE FLASHING CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

- ① DISTANCE BETWEEN VEHICLES MAY INCREASE FROM THE ATTENUATOR'S ROLL AHEAD BASED ON TERRAIN, SIGHT DISTANCE, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ② ALIGN LEFT SIDE OF SHADOW VEHICLE WITH EDGE OF WORK AREA.



6

6

SDD 15D51 - 01

SDD 15D51 - 01

**TRAFFIC CONTROL,
MOBILE OPERATIONS ON
AN UNDIVIDED ROADWAY**

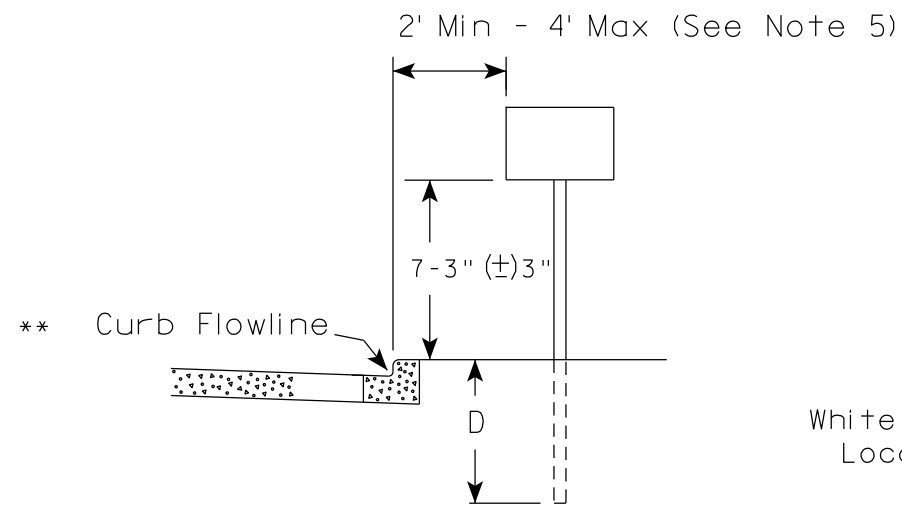
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 /S/ Andrew Heidtke
DATE STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

FHWA

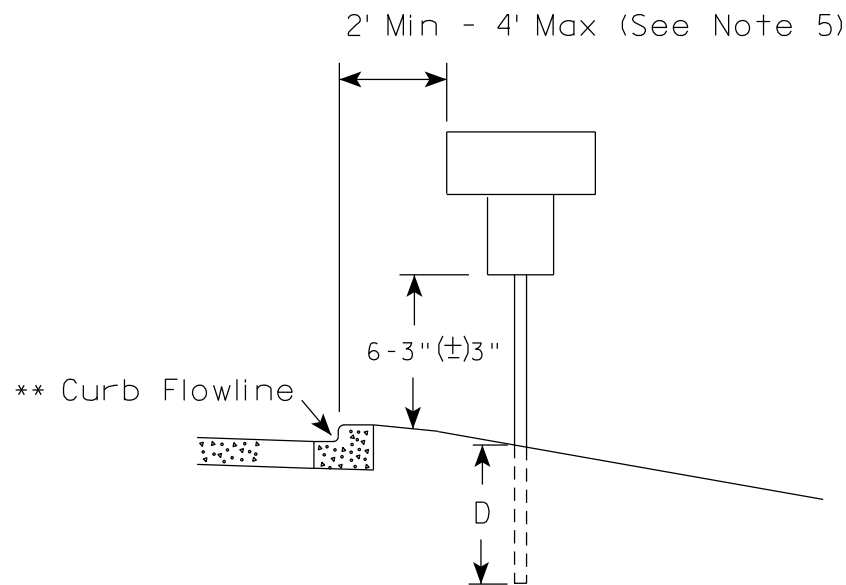
URBAN AREA

RURAL AREA (See Note 2)



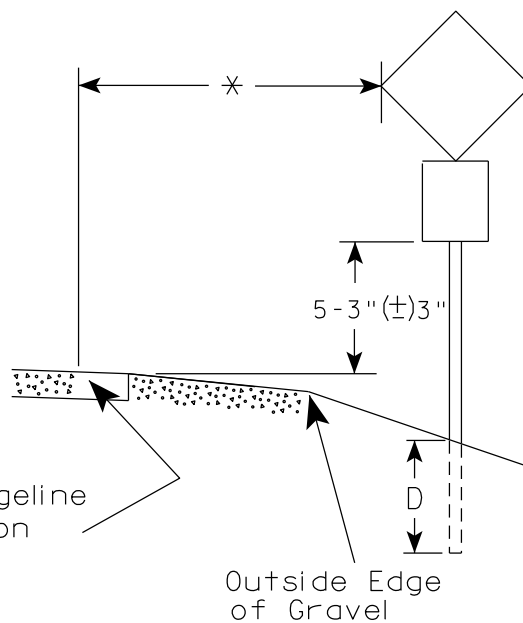
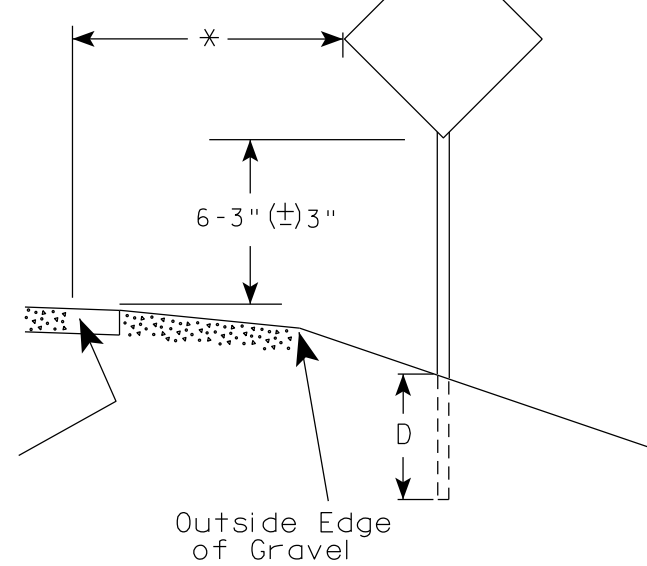
White Edgeline Location

Outside Edge of Gravel



White Edgeline Location

Outside Edge of Gravel



POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".
3. For expressways and freeways, mounting height is 7'- 3" (±) 3" or 6'-3" (±) 3" depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±) 3".
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the Engineer.

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

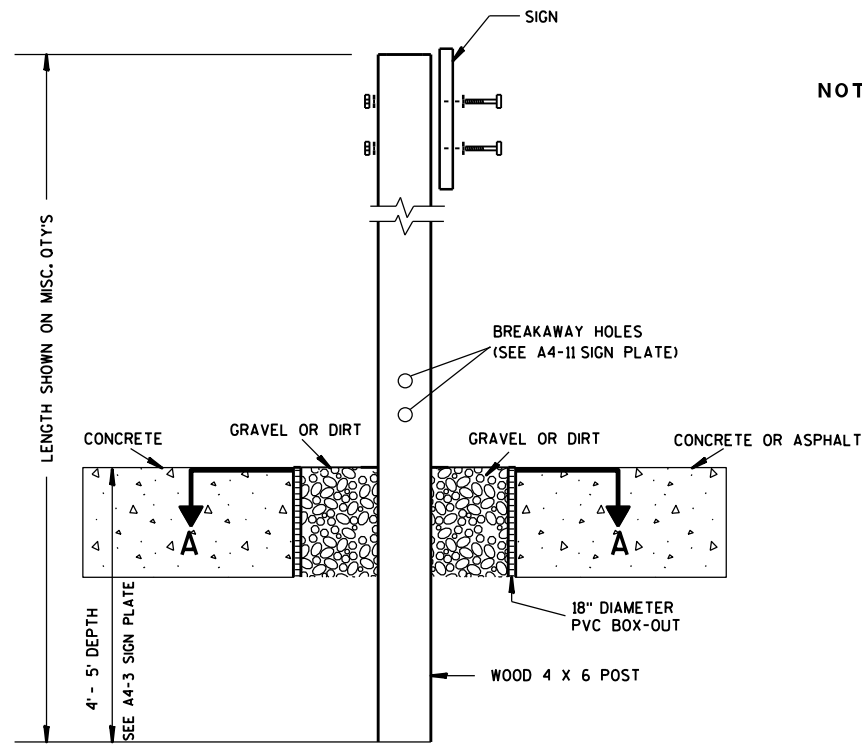
WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R. Raub
for State Traffic Engineer

DATE 12/6/23

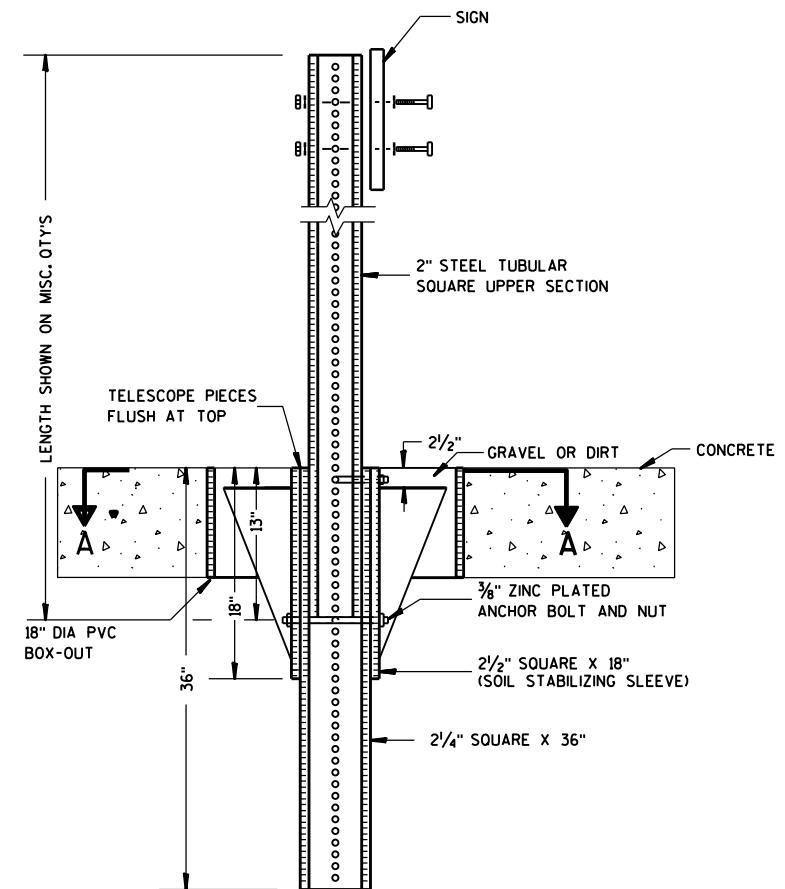
PLATE NO. A4-3.23



ELEVATION VIEW

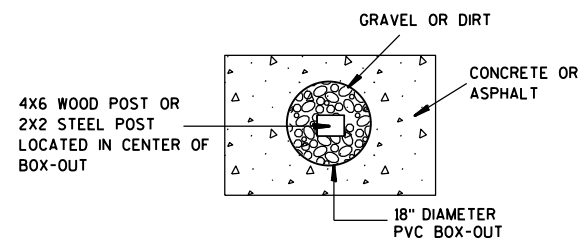
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B	
<small>WISCONSIN DEPT OF TRANSPORTATION</small>	
APPROVED <i>Matthew R. Rauch</i> <small>for State Traffic Engineer</small>	
DATE <u>1/27/14</u>	PLATE NO. <u>A4-3B.1</u>

GENERAL NOTES

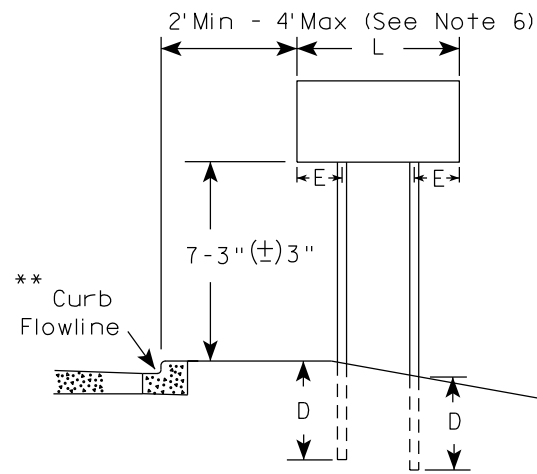
- For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
- See tables below for required number of posts.
- For expressways and freeways, mounting height is 7'-3" (±) 3" or 6'-3" (±) 3" depending upon existence of sub-sign.
- The (±) tolerance for mounting height is 3 inches.
- J-Assemblies are considered to be one sign for mounting height.
- Offset distance shall be consistent with existing signs or consistent throughout length of project.
- Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the engineer.
- The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

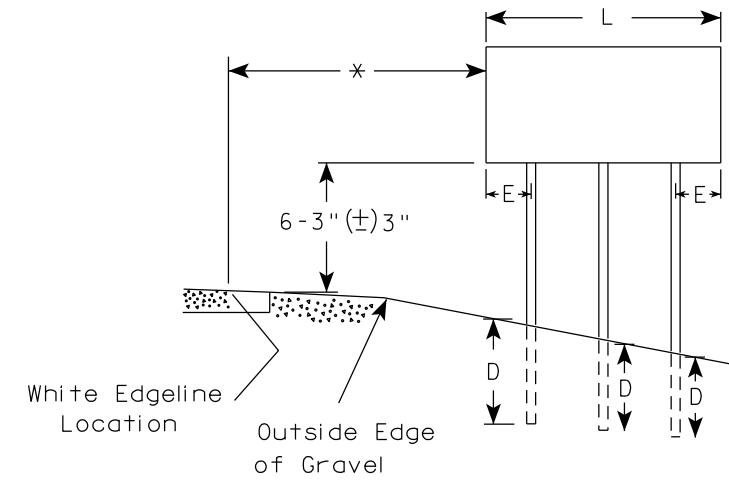
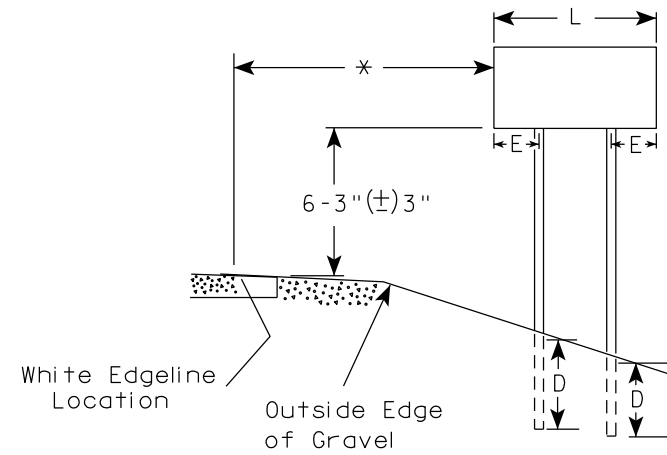
** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

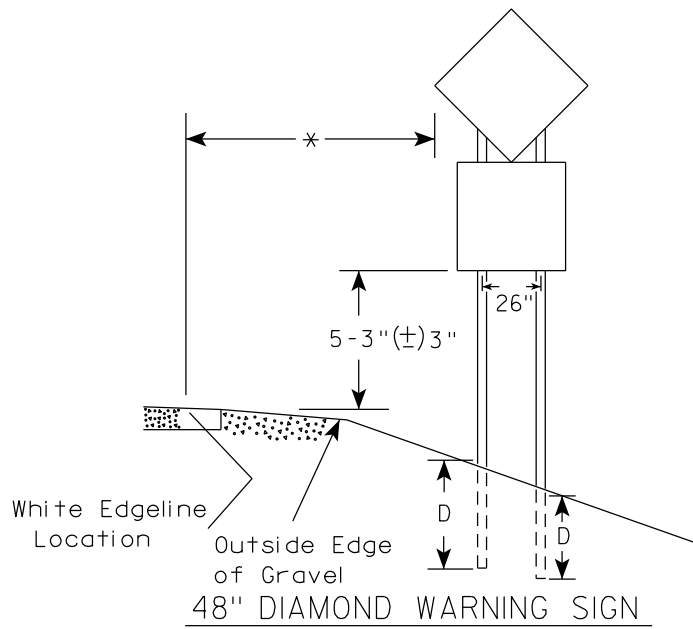
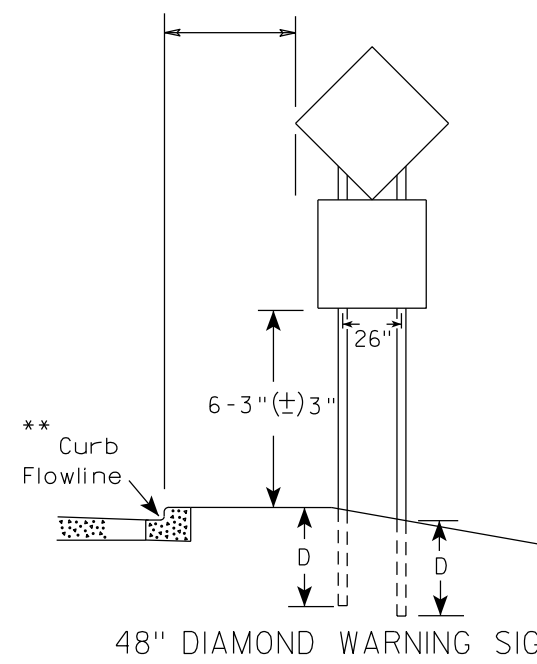
URBAN AREA



RURAL AREA (See Note 3)



URBAN AREA



SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

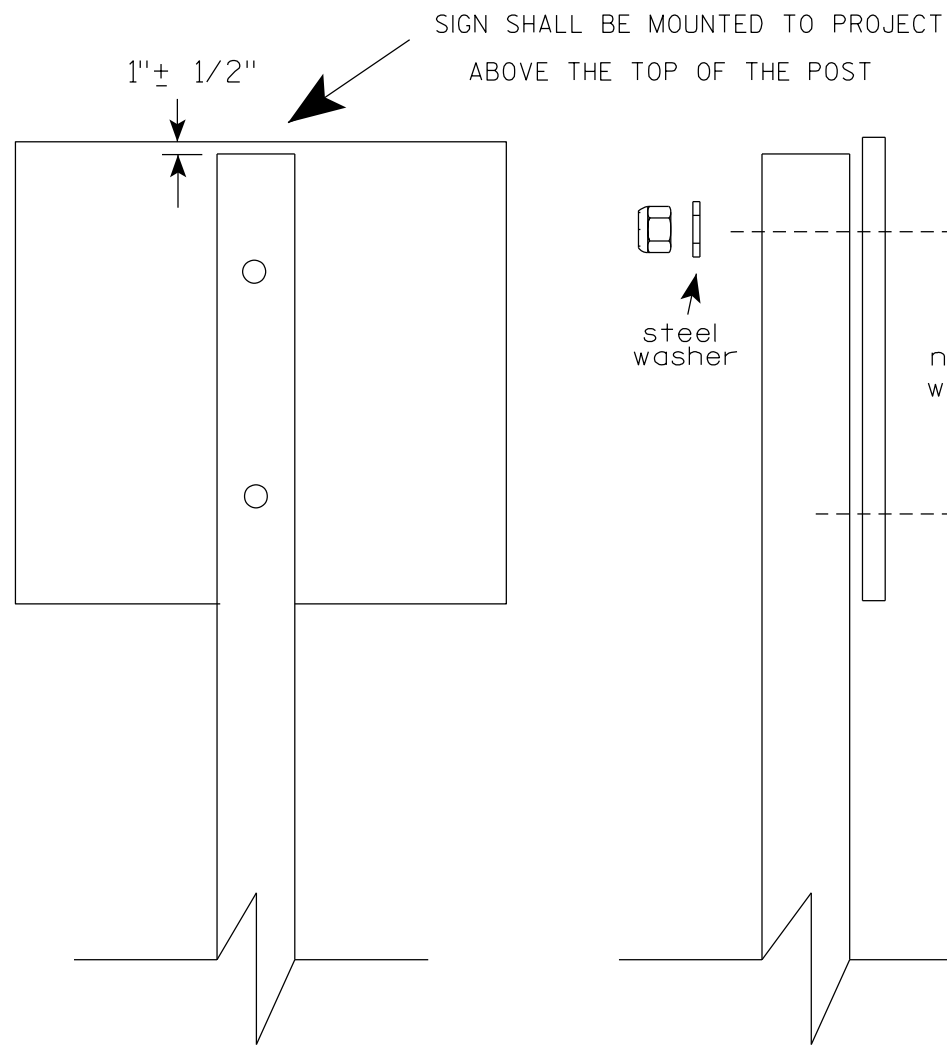
SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION
 APPROVED *Matthew R. Rauch*
 For State Traffic Engineer
 DATE 12/6/23 PLATE NO. A4-4.16



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS
TO POSTS

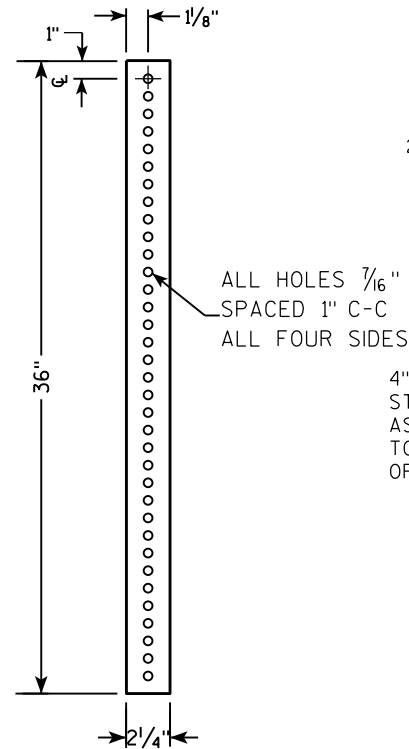
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

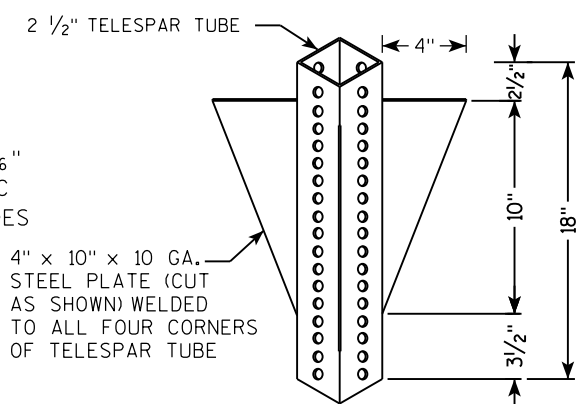
DATE 4/1/2020 PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

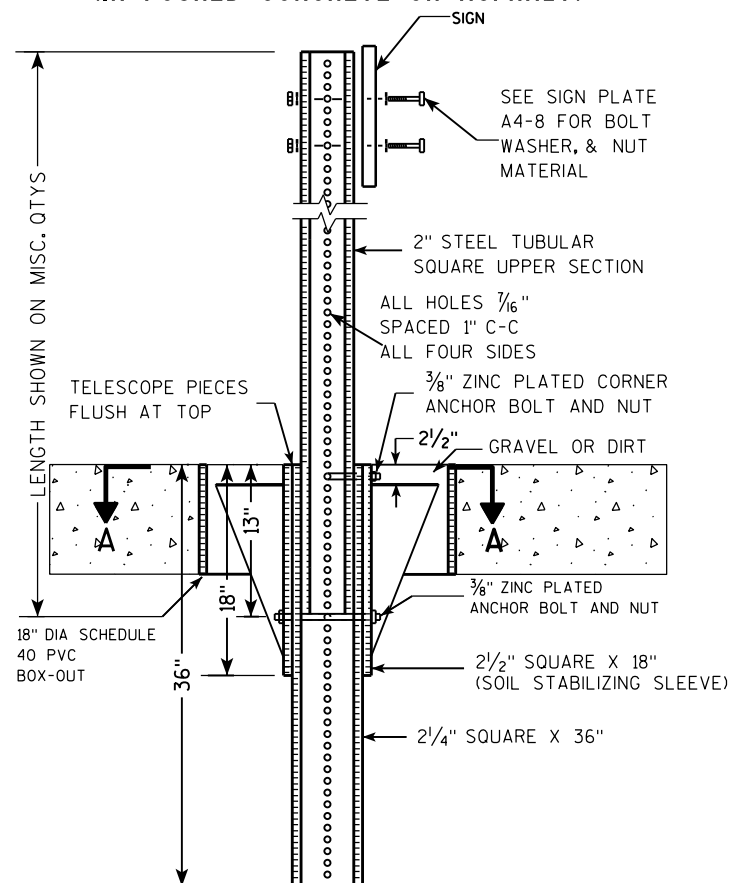
2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



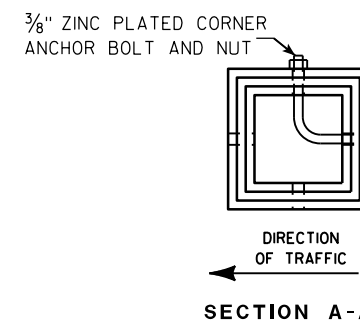
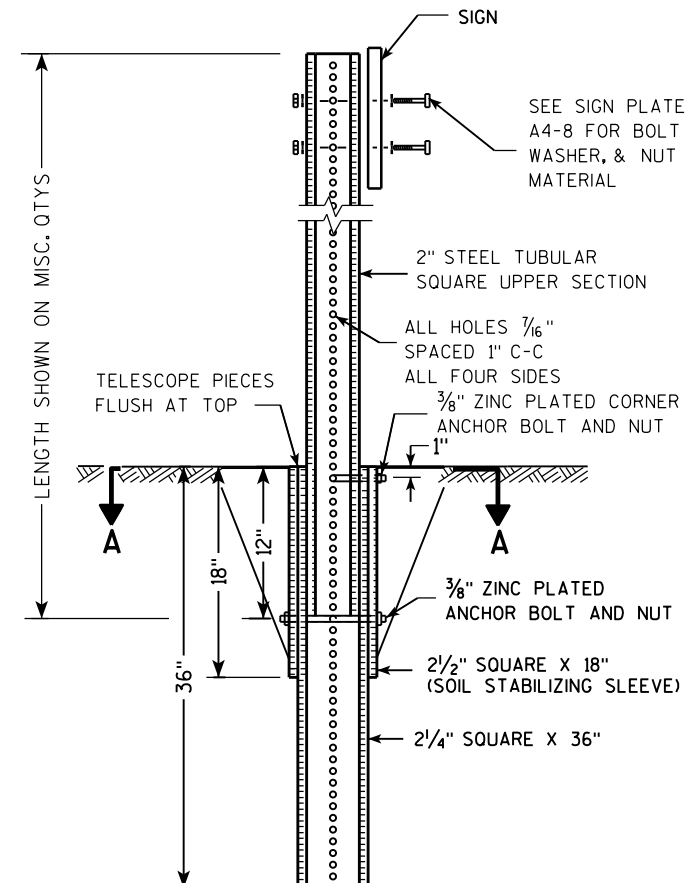
2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**

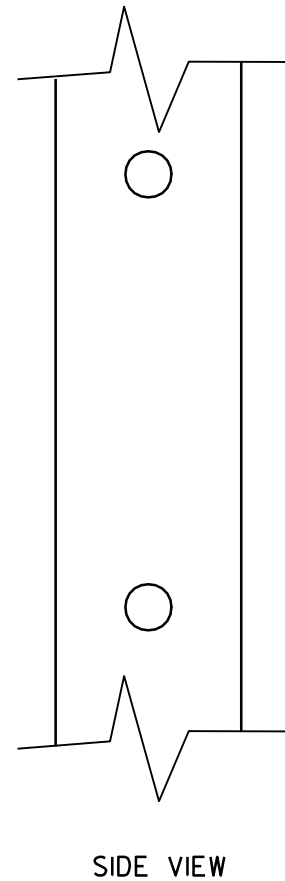
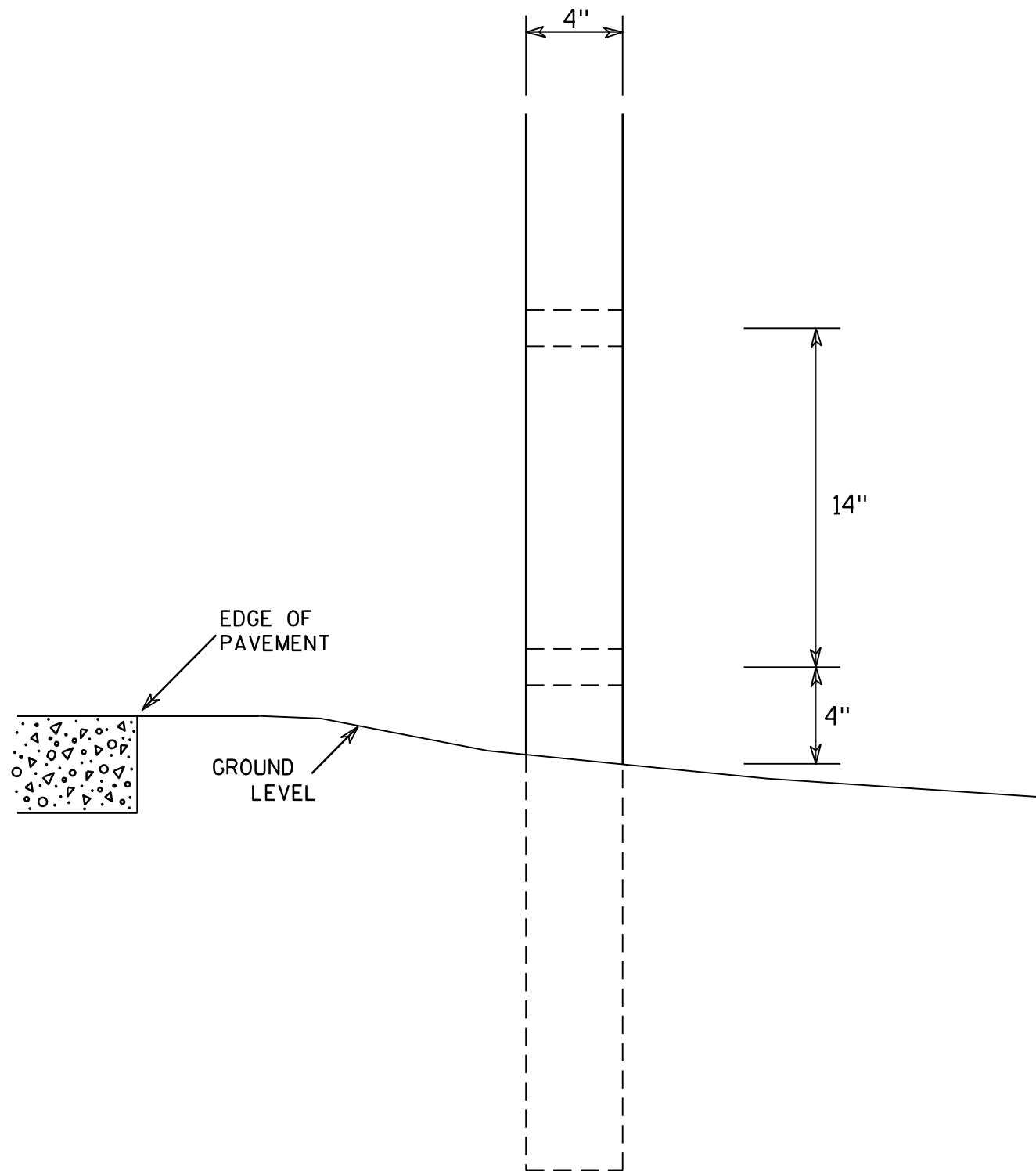


Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R Rauch*
for State Traffic Engineer
DATE 2/05/15 PLATE NO. A4-9.9



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

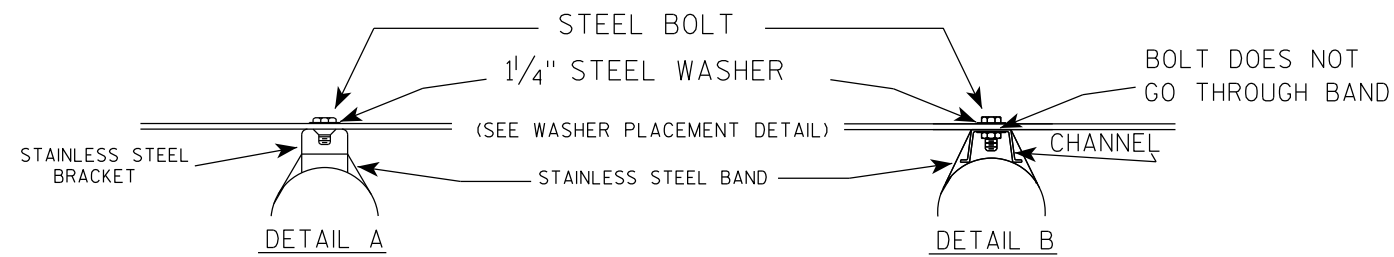
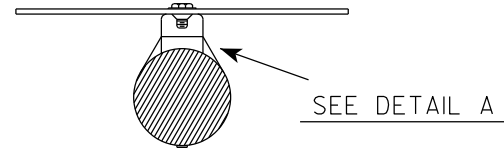
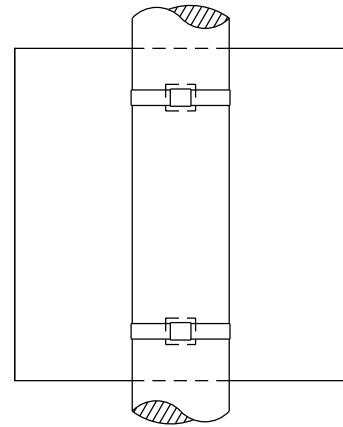
7

7

4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	<i>Chester J Spang</i> for State Traffic Engineer
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

BANDING

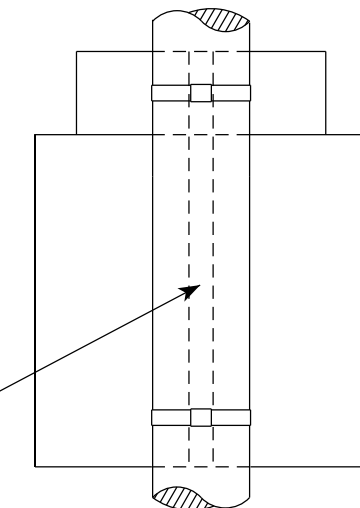
SINGLE SIGN



GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

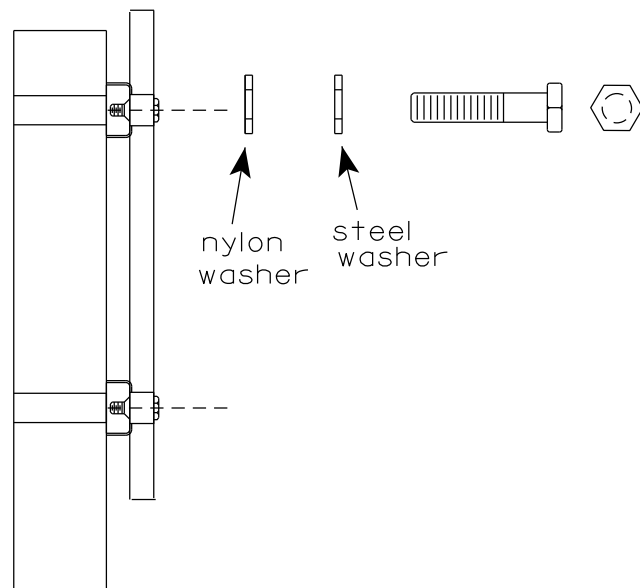
"J" ASSEMBLY



CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET



WASHER PLACEMENT



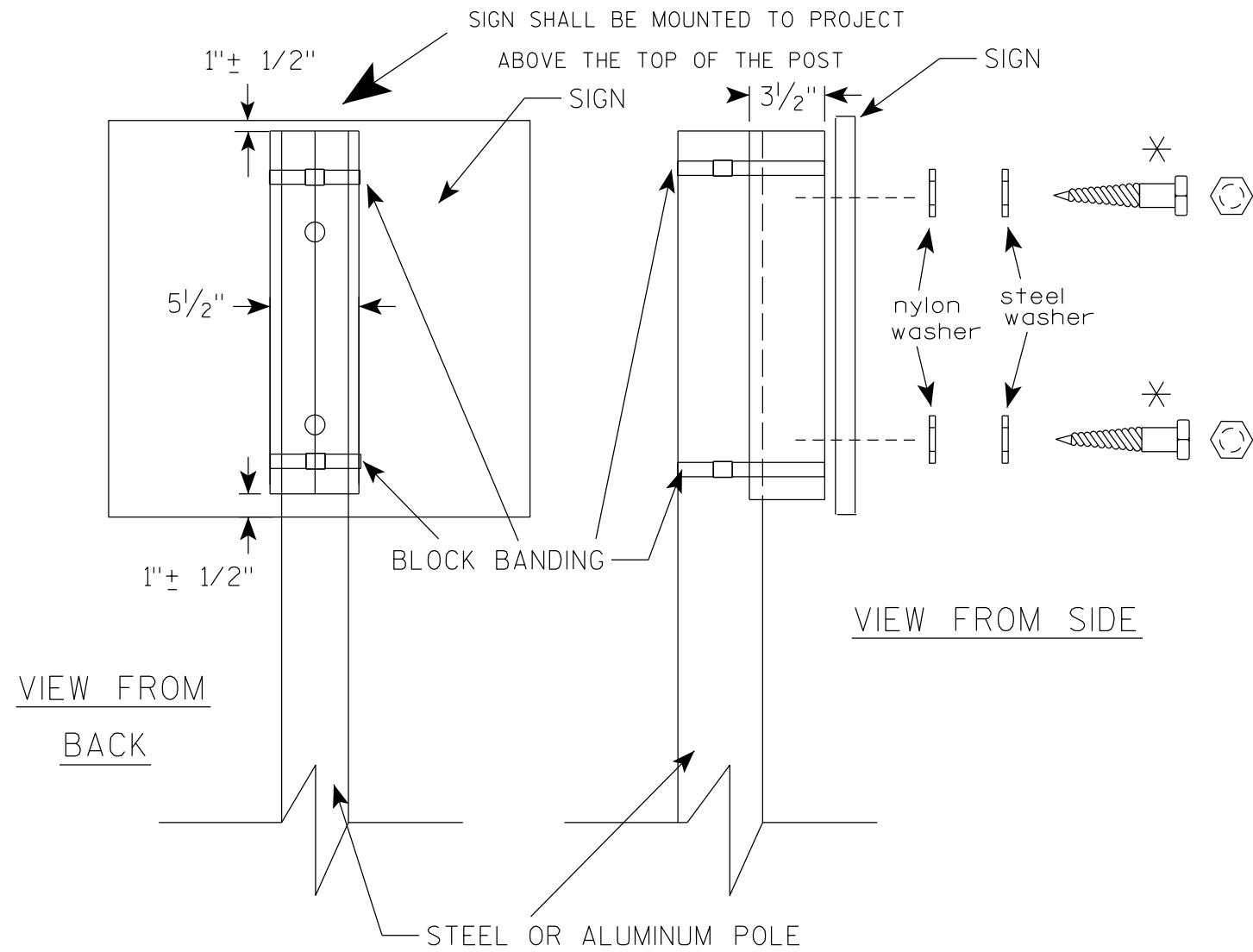
WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

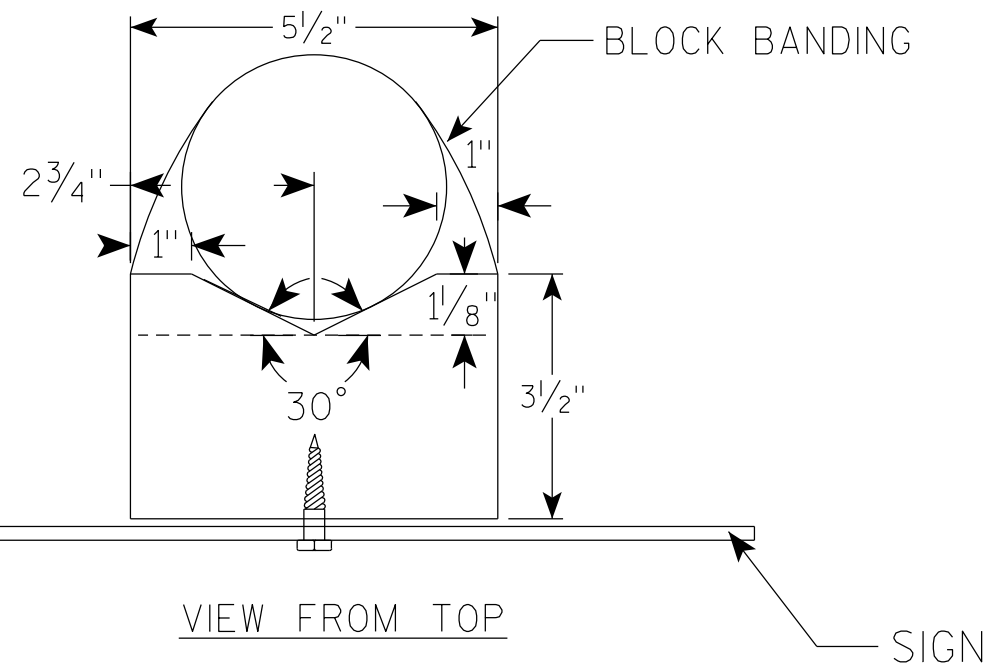
DATE 6/10/19 PLATE NO. A5-9.4



GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

* LAG BOLTS SHALL BE 3/8" X 2 1/2"



BLOCK BANDING DETAIL
(V-BLOCK OPTION)

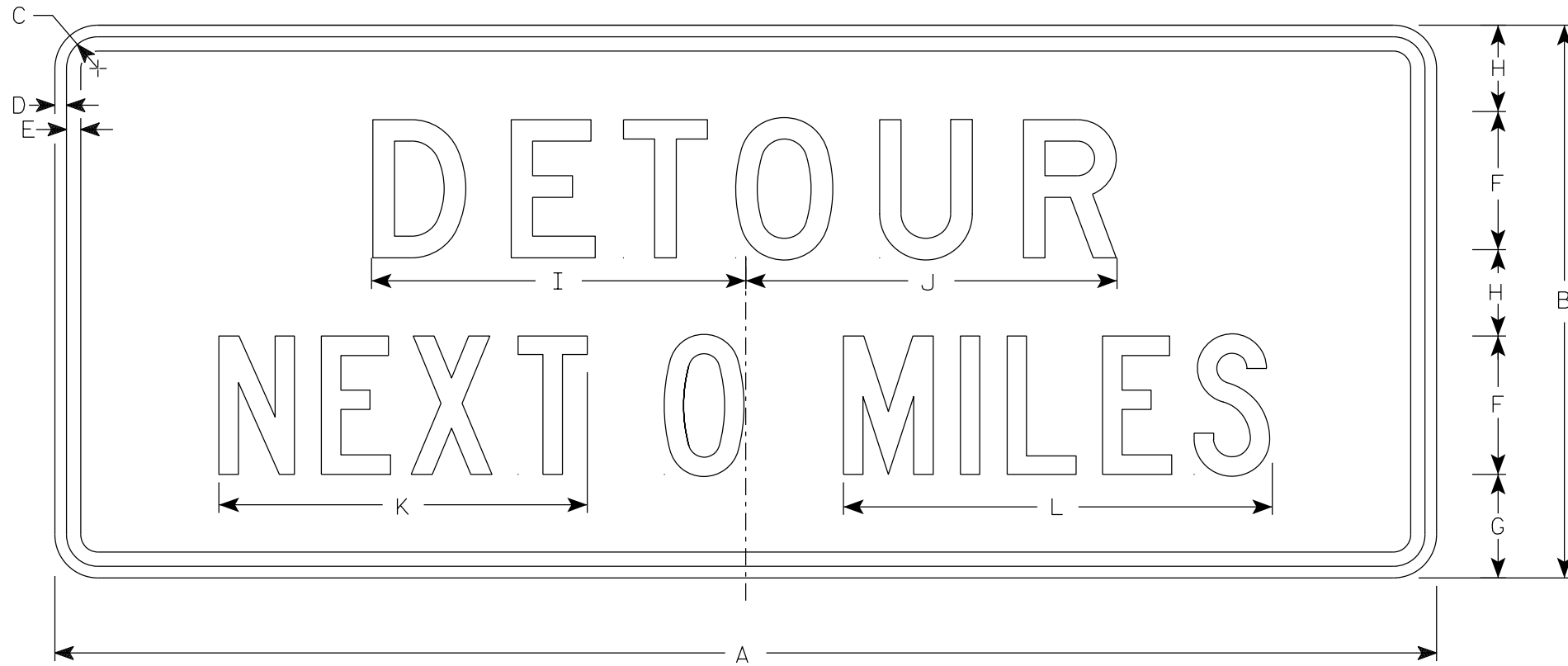
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 4/19/2022 PLATE NO. A5-10.3

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - Line 1 is D and Line 2 is C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Round distance to nearest whole Mile and substitute appropriate numerals and optically adjust spacing to achieve proper balance



G20-51

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	60	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	16 1/4	16 1/8	16	18 5/8															10.0
2M	60	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	16 1/4	16 1/8	16	18 5/8															10.0
3	60	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	16 1/4	16 1/8	16	18 5/8															10.0
4	60	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	16 1/4	16 1/8	16	18 5/8															10.0
5	60	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	16 1/4	16 1/8	16	18 5/8															10.0

STANDARD SIGN
G20-51

WISCONSIN DEPT OF TRANSPORTATION

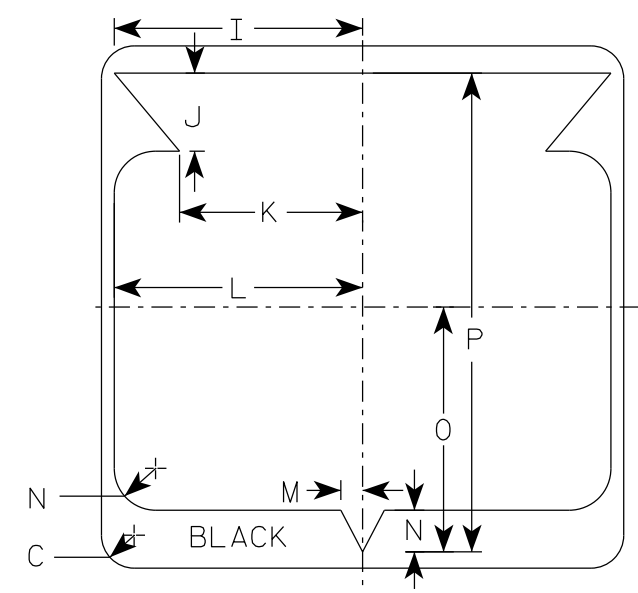
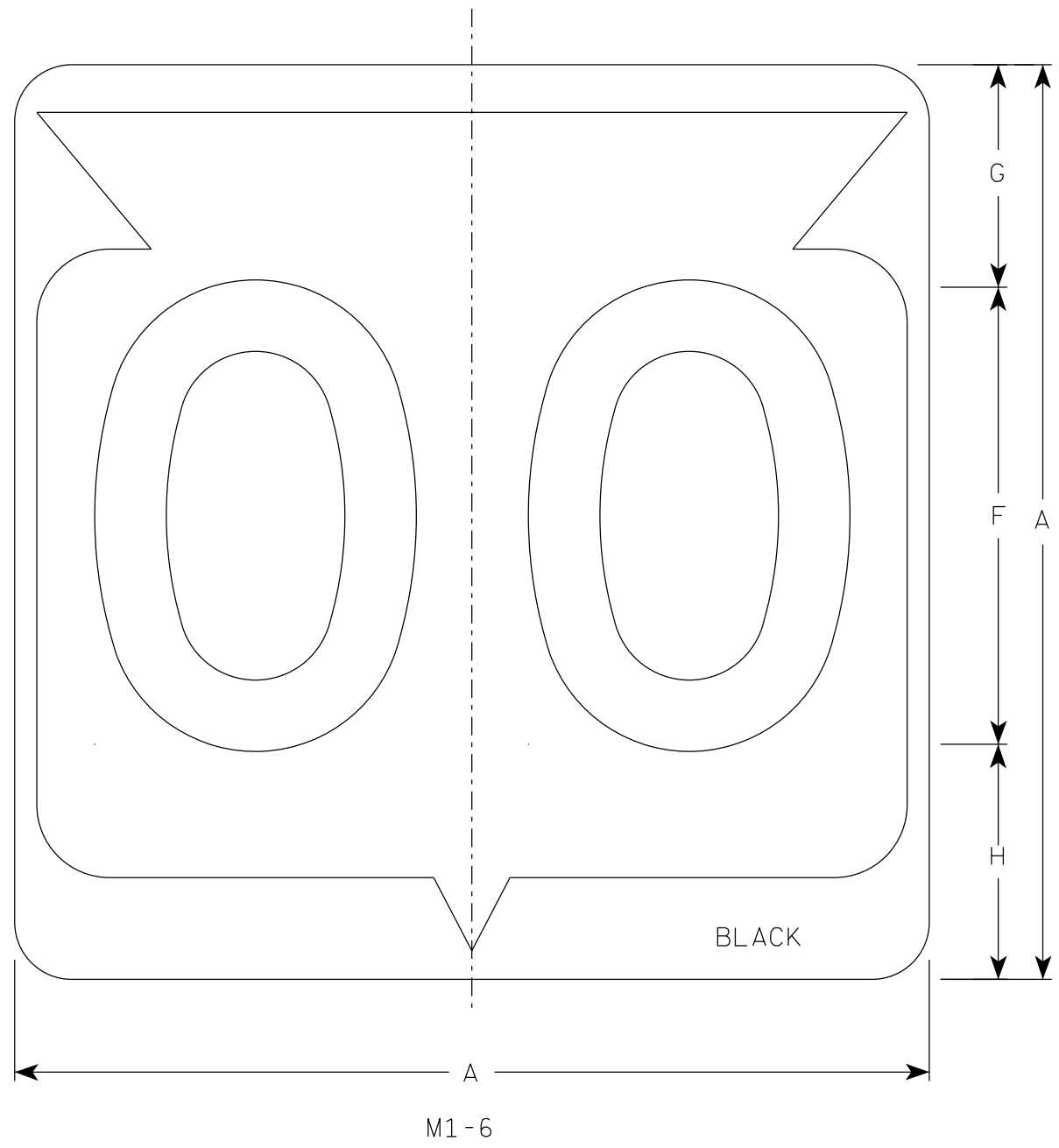
APPROVED *Matthew R. Rauch*
State Traffic Engineer

DATE 1/26/2023 PLATE NO. G20-51.3

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D except 3 number signs Series C



7

7

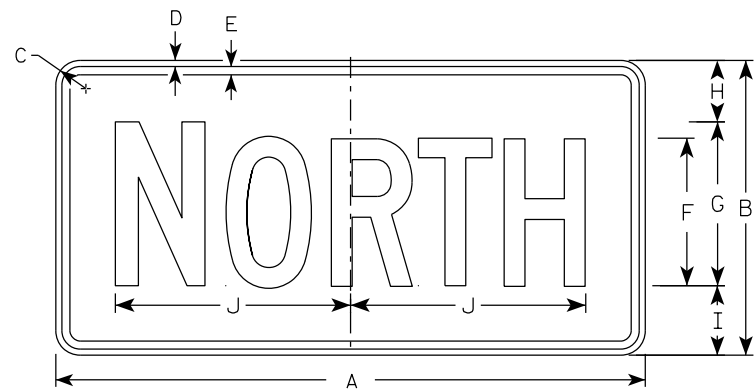
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
2M	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0

STATE ROUTE MARKER
M1-6 FOR ASSEMBLIES

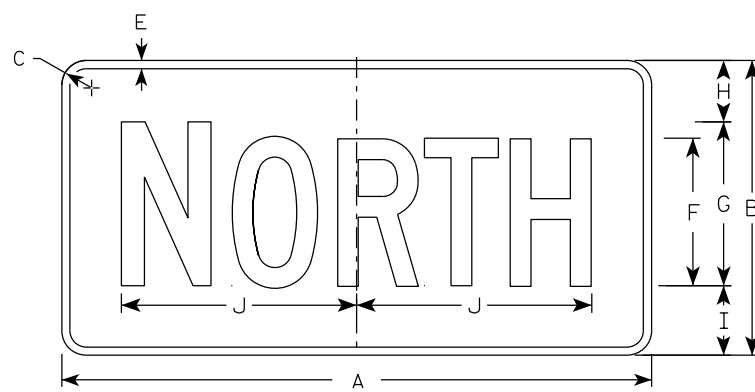
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Raub*
for State Traffic Engineer

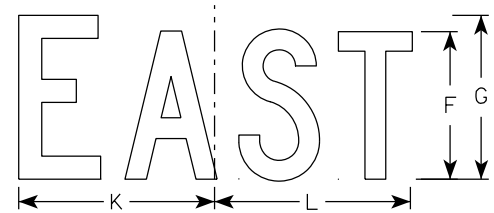
DATE 11/8/2022 PLATE NO. M1-6.11



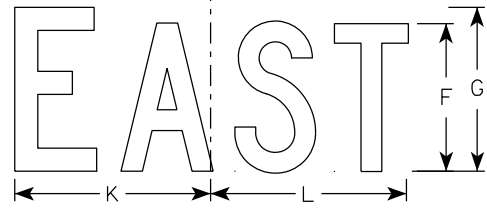
M3-1
MM3-1
MP3-1



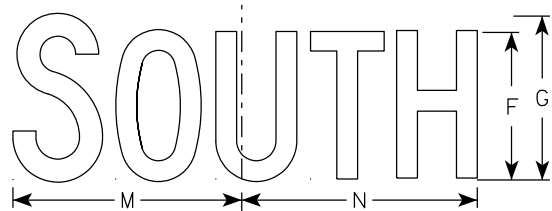
MB3-1
MK3-1
MN3-1



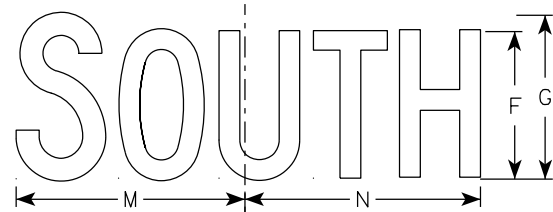
M3-2
MM3-2
MP3-2



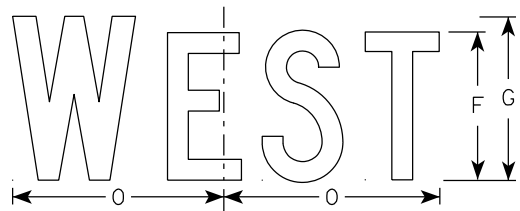
MB3-2
MK3-2
MN3-2



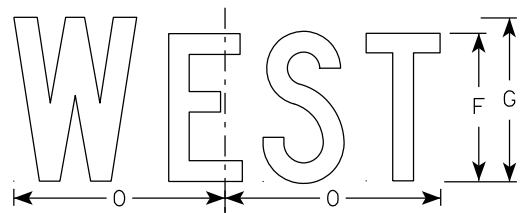
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

NOTES

- All Signs Type II - Type H Reflective
- Color:
 - Background - See note 5
 - Message - See note 5
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M3-1 thru M3-4 Background - White
Message - Black
MB3-1 thru MB3-4 Background - Blue
Message - White
MK3-1 thru MK3-4 Background - Green
Message - White
MM3-1 thru MM3-4 Background - White
Message - Green
MN3-1 thru MN3-4 Background - Brown
Message - White
MP3-1 thru MP3-4 Background - White
Message - Blue
- Note the first letter of each direction is larger than the remainder of the message.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4												2.00
2M	24	12	1 1/2	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4												2.00
3	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5
4	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5
5	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5

STANDARD SIGNS
M3-1 THRU M3-4
SERIES

WISCONSIN DEPT OF TRANSPORTATION

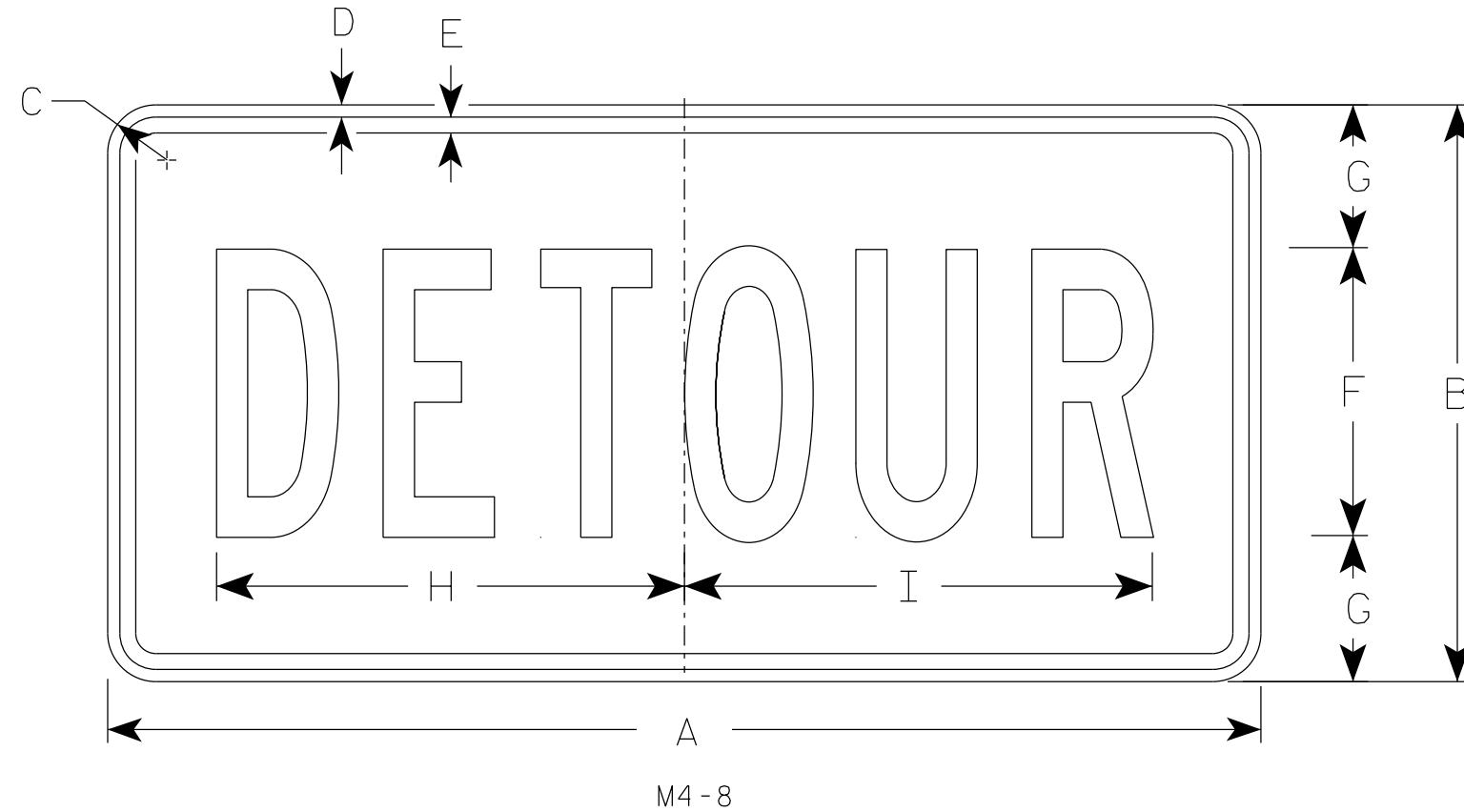
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/8/2023 PLATE NO. M3-1.15

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/2	3/8	3/8	6	3	10	10 1/4																		2.0
2M	24	12	1 1/2	3/8	3/8	6	3	10	10 1/4																		2.0
3	36	18	1 1/2	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
4	36	18	1 1/2	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
5	36	18	1 1/2	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5

STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

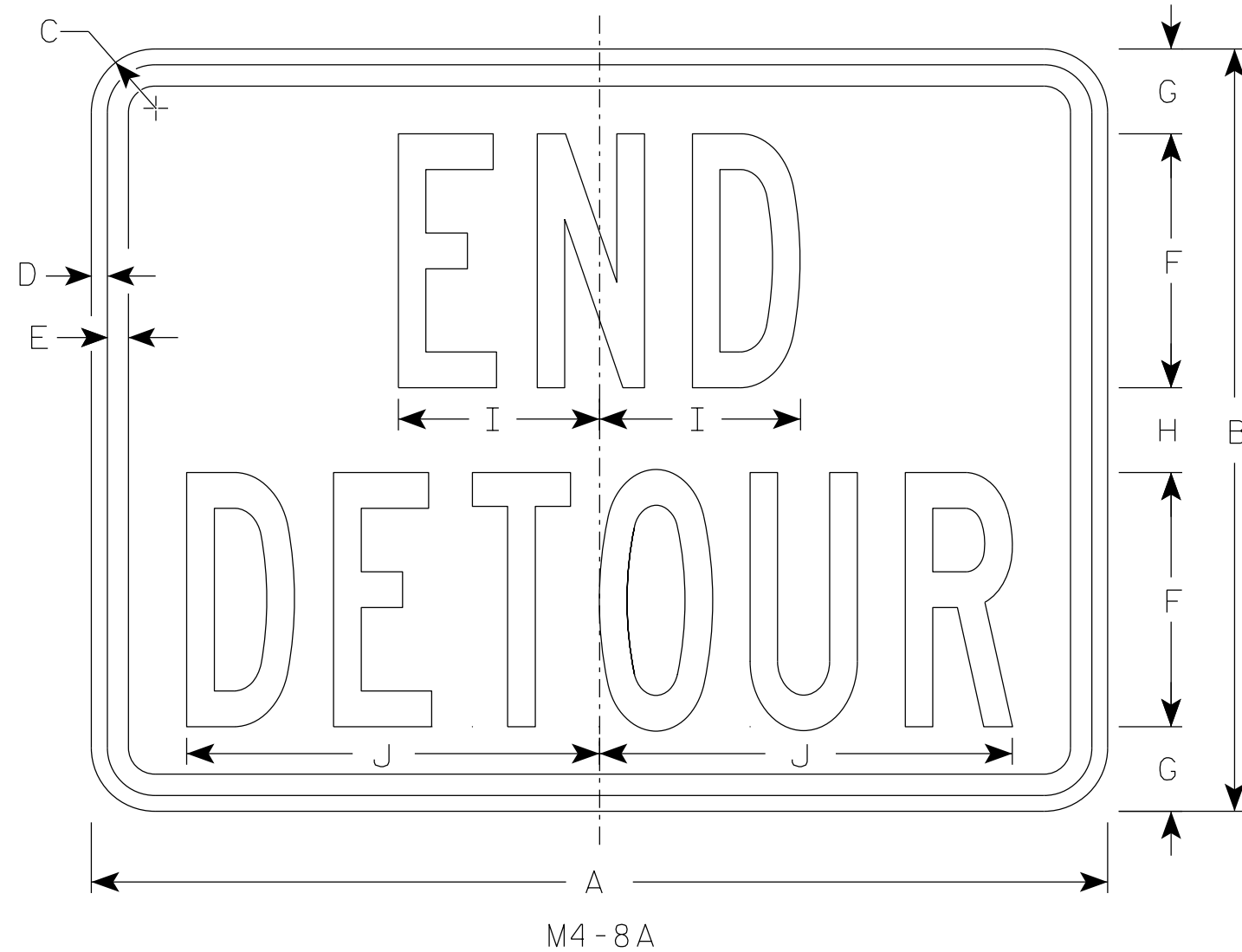
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/9/2023 PLATE NO. M4-8.4

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



M4-8A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 1/2	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
2M	24	18	1 1/2	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
3	30	24	1 1/2	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
4	30	24	1 1/2	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
5	30	24	1 1/2	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0

STANDARD SIGN
M4-8A

WISCONSIN DEPT OF TRANSPORTATION

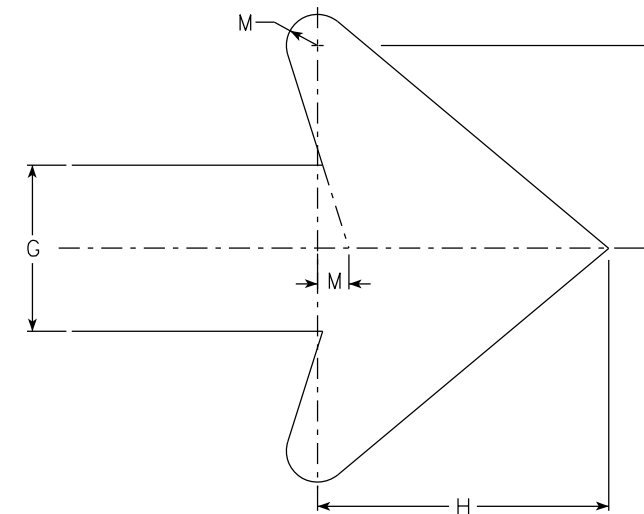
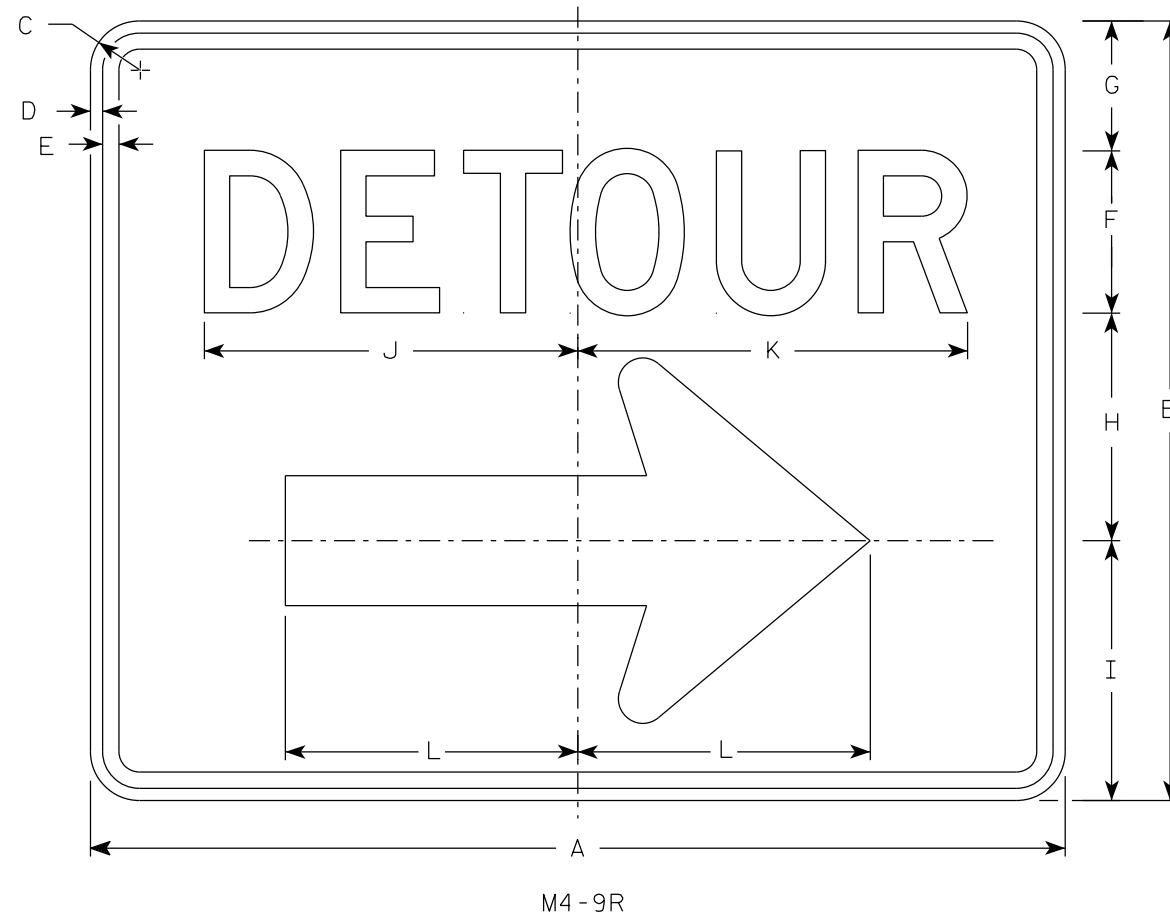
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 2/9/2023 PLATE NO. M4-8A.4

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M4-9L is the same as M4-9R except the arrow is reversed.



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/2	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
2M	30	24	1 1/2	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
3	30	24	1 1/2	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
4	48	36	1 7/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0
5	48	36	1 7/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0

STANDARD SIGN
M4-9 R & L

WISCONSIN DEPT OF TRANSPORTATION

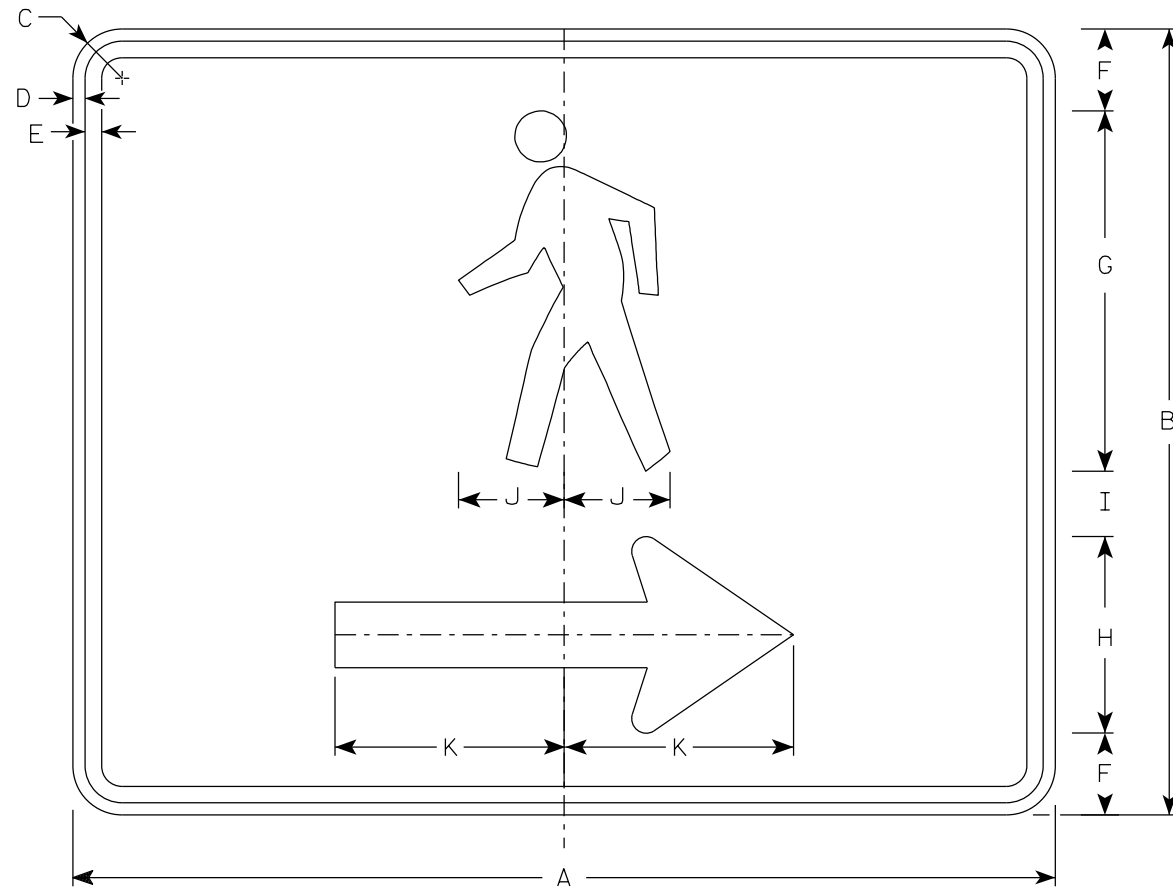
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/9/2023 PLATE NO. M4-9R.6

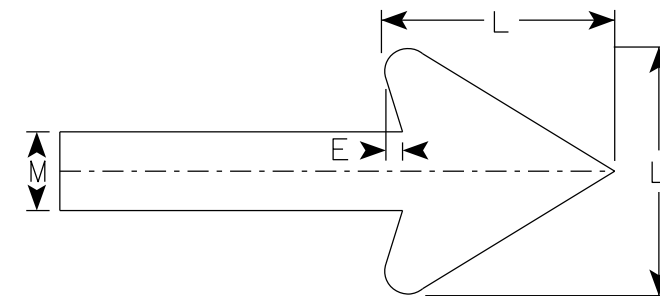
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II- Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M4-60L is the same as M4-60R except the arrow is reversed.



M4-60R



Arrow Detail

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	24	1 1/2	3/8	1/2	2 1/2	11	6	2	3 1/4	7	6	2														5.00
2M	30	24	1 1/2	3/8	1/2	2 1/2	11	6	2	3 1/4	7	6	2														5.00
3																											
4																											
5																											

STANDARD SIGN
M4-60 L&R

WISCONSIN DEPT OF TRANSPORTATION

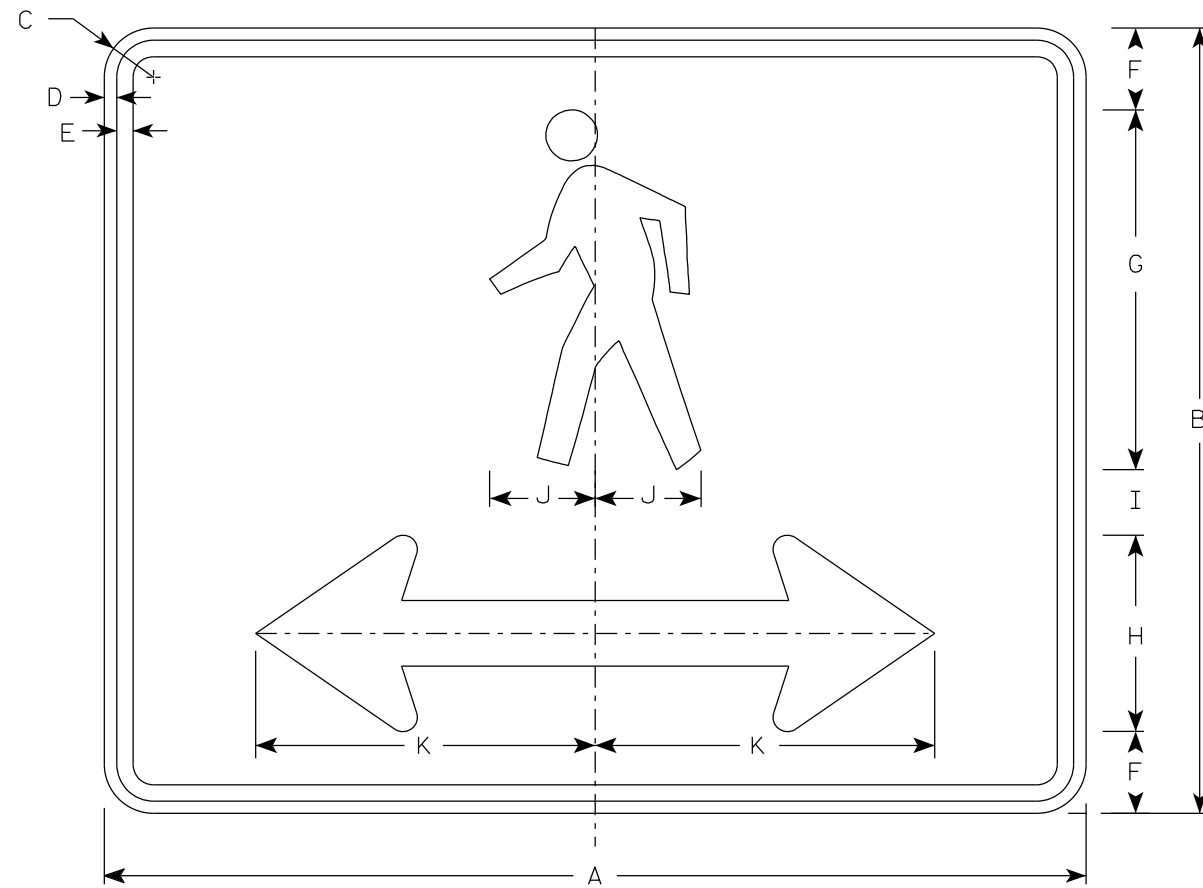
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 2/14/2023 PLATE NO. M4-60.2

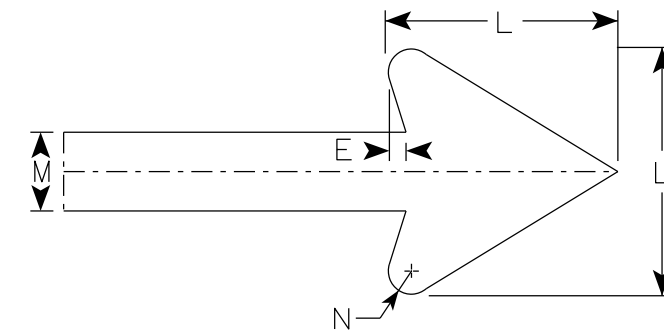
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II- Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



M4-60D



Arrow Detail

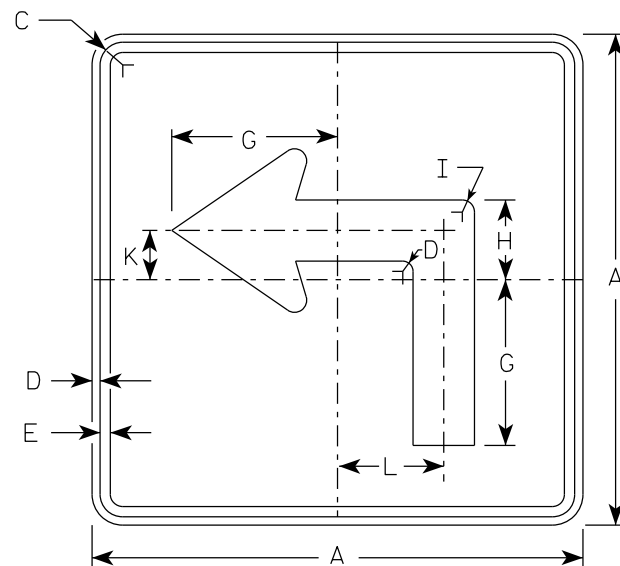
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	24	1 1/2	3/8	1/2	2 1/2	11	6	2	3 1/4	10 3/8	6	2	3/8													5.00
2M	30	24	1 1/2	3/8	1/2	2 1/2	11	6	2	3 1/4	10 3/8	6	2	3/8													5.00
3																											
4																											
5																											

STANDARD SIGN
M4-60D

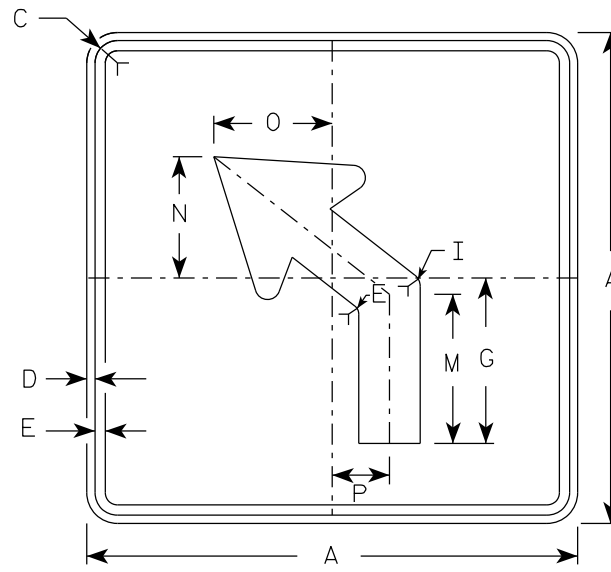
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
for State Traffic Engineer

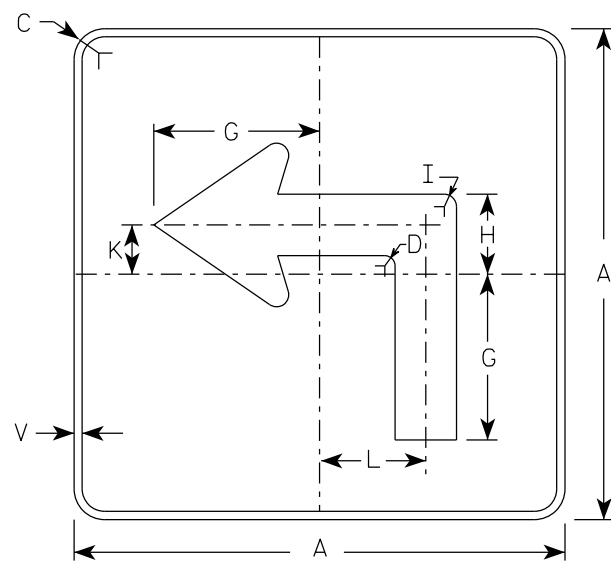
DATE 2/14/2023 PLATE NO. M4-60D.2



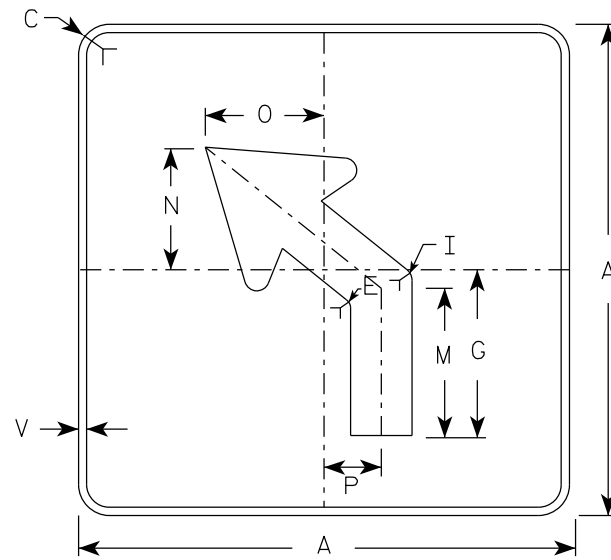
M5-1L
MM5-1L
M05-1L
MP5-1L



M5-2L
MM5-2L
M05-2L
MP5-2L

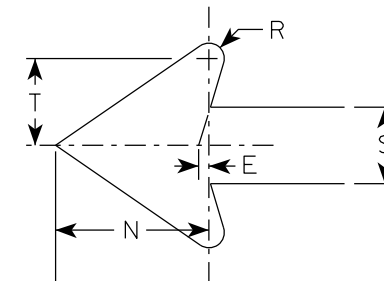


MB5-1L
MK5-1L
MN5-1L
MR5-1L



MB5-2L
MK5-2L
MN5-2L
MR5-2L

ARROW DETAIL



NOTES

- Signs are Type II - Type H reflective except as shown
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- | | |
|-----------------|---|
| M5-1 and M5-2 | Background - White |
| | Message - Black |
| MB5-1 and MB5-2 | Background - Blue |
| | Message - White |
| MK5-1 and MK5-2 | Background - Green |
| | Message - White |
| MM5-1 and MM5-2 | Background - White |
| | Message - Green |
| MN5-1 and MN5-2 | Background - Brown |
| | Message - White |
| M05-1 and M05-2 | Background - Orange - Type F Reflective |
| | Message - Black |
| MP5-1 and MP5-2 | Background - White |
| | Message - Blue |
| MR5-1 and MR5-2 | Background - Brown |
| | Message - Yellow |
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

7

7

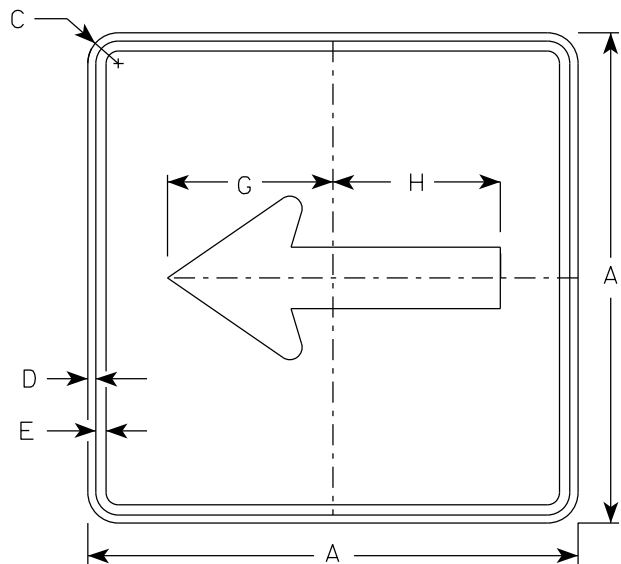
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	21		1 1/2	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3		1/2					3.06
2M	21		1 1/2	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3		1/2					3.06
3	30		1 7/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4		1/2					6.25
4	30		1 7/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4		1/2					6.25
5	30		1 7/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4		1/2					6.25

STANDARD SIGN
M5-1 & M5-2

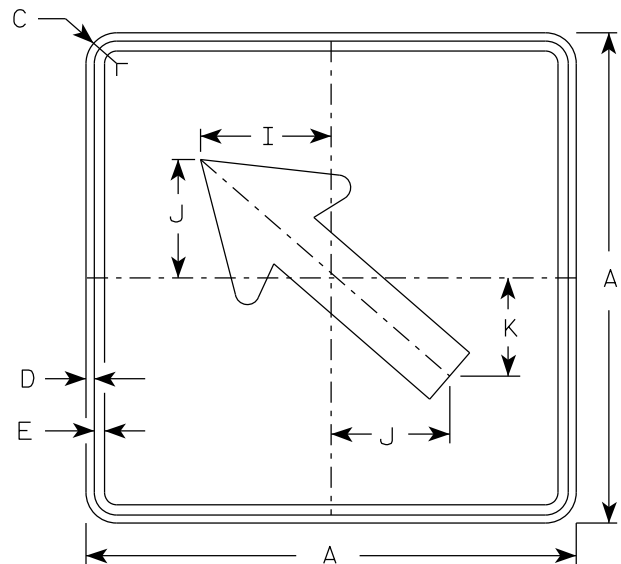
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

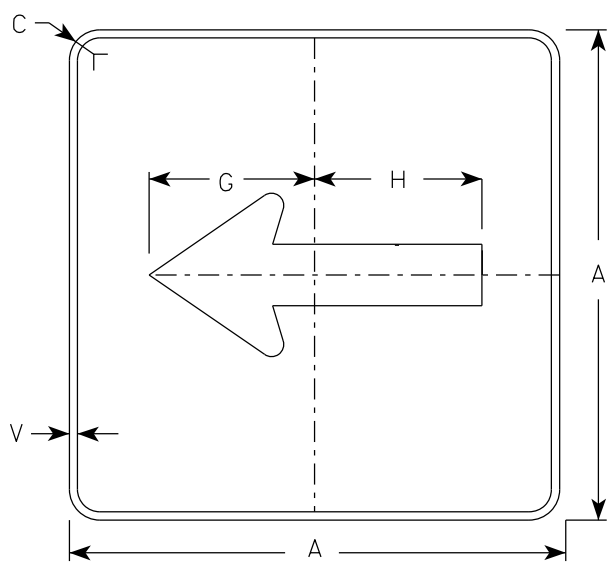
DATE 2/13/2023 PLATE NO. M5-1.15



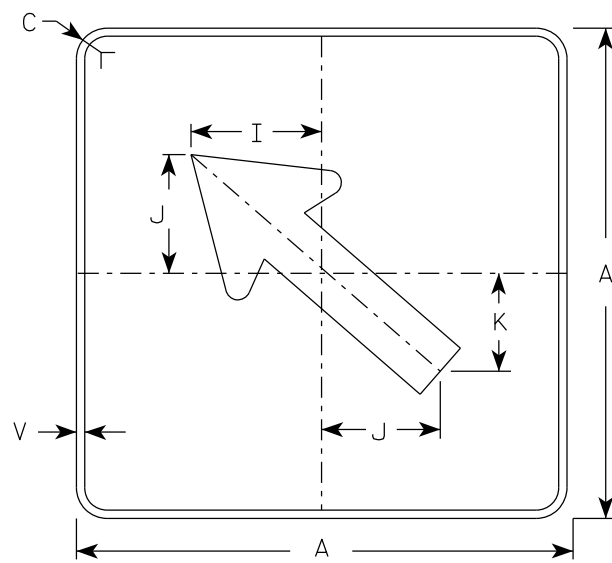
M6-1
MM6-1
M06-1
MP6-1



M6-2
MM6-2
M06-2
MP6-2



MB6-1
MK6-1
MN6-1
MR6-1

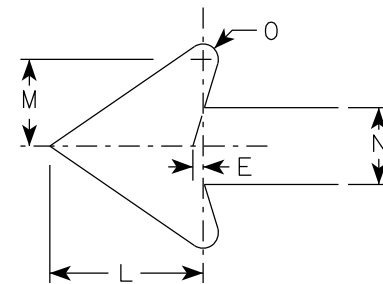


MB6-2
MK6-2
MN6-2
MR6-2

NOTES

- Signs are Type II - Type H Reflective except as Shown
- Color:
 - Background - See note 4
 - Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White
Message - Black
 MB6-1 and MB6-2 Background - Blue
Message - White
 MK6-1 and MK6-2 Background - Green
Message - White
 MM6-1 and MM6-2 Background - White
Message - Green
 MN6-1 and MN6-2 Background - Brown
Message - White
 M06-1 and M06-2 Background - Orange - Type F Reflective
Message - Black
 MP6-1 and MP6-2 Background - White
Message - Blue
 MR6-1 and MR6-2 Background - Brown
Message - Yellow

ARROW DETAIL



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	21		1 1/2	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2							1/2					3.06
2M	21		1 1/2	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2							1/2					3.06
3	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2					6.25
4	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2					6.25
5	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2					6.25

STANDARD SIGN
M6-1 & M6-2
SERIES

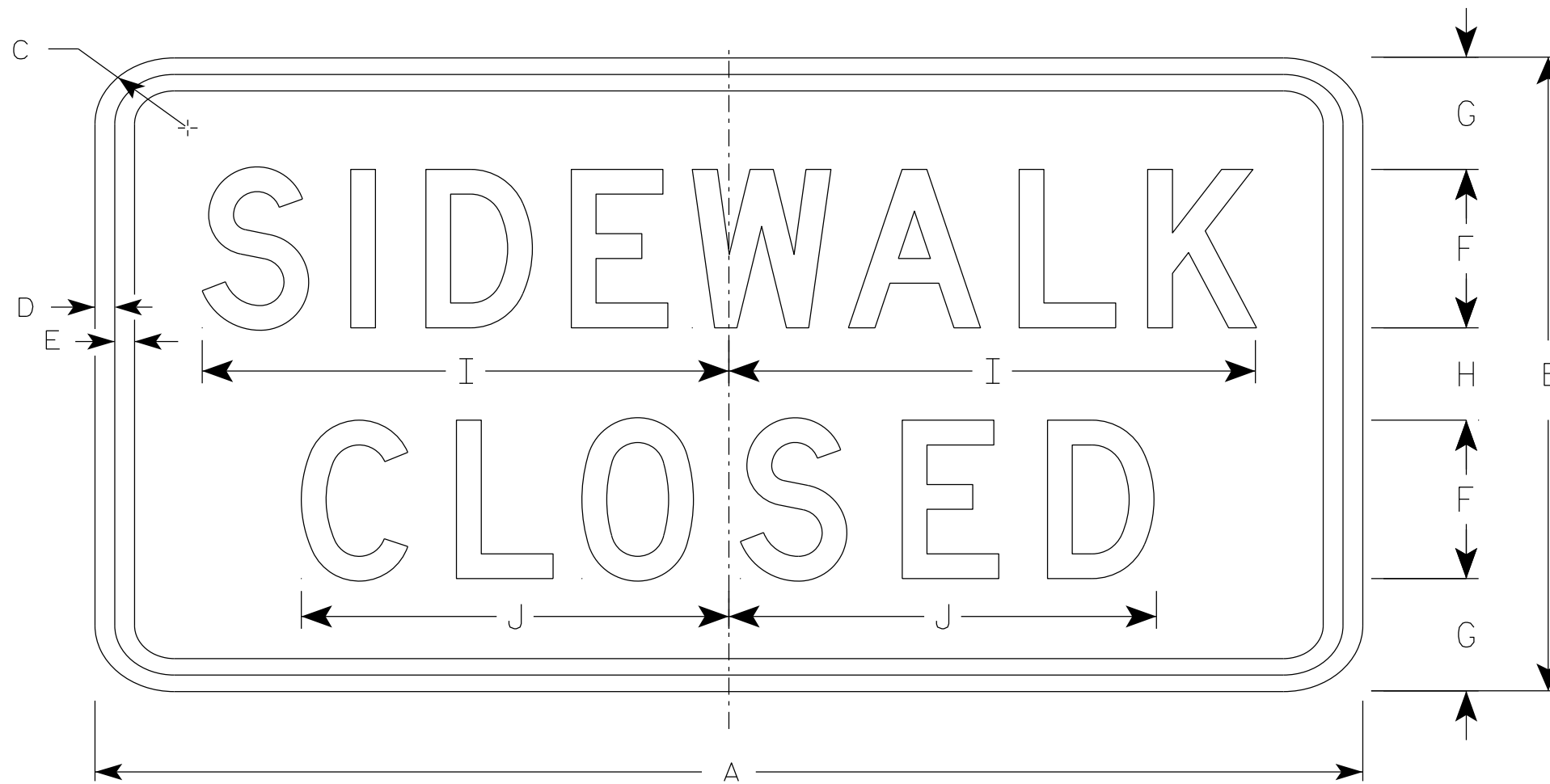
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 2/13/2023 PLATE NO. M6-1.16

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
 - Background - White
 - Message - Black
3. Message Series - C
4. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.



R9-9

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
2M	24	12	1 1/2	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
3	30	18	1 1/2	1/2	1/2	4	3 1/2	3	12 1/2	10 1/4																	3.75
4																											
5																											

STANDARD SIGN
R9-9

WISCONSIN DEPT OF TRANSPORTATION

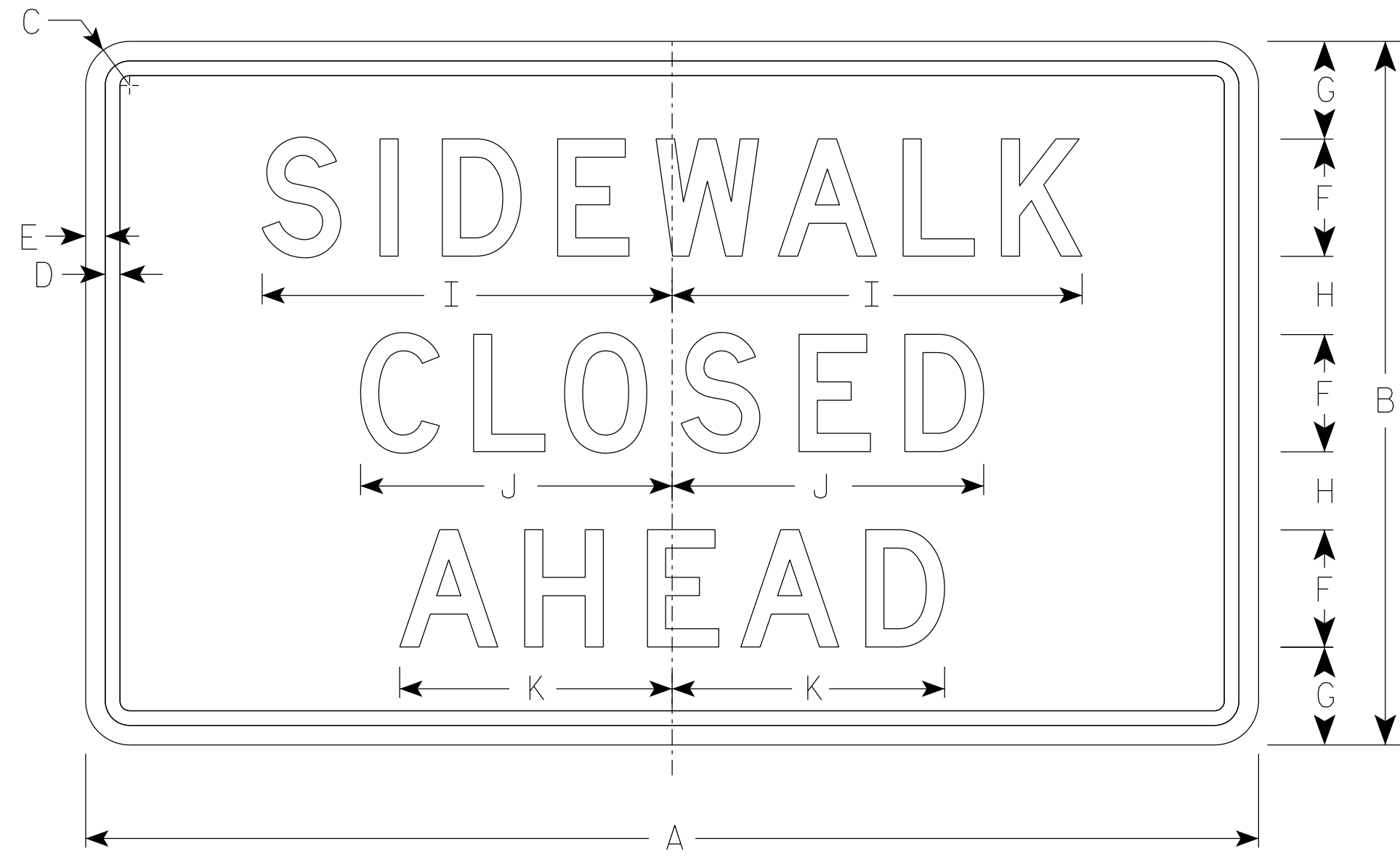
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 1/24/24 PLATE NO. R9-9.7

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D



R9-9A

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 1/2	3/8	1/2	3	2 1/2	2	10 1/2	8	7																3.75
2M	30	18	1 1/2	3/8	1/2	3	2 1/2	2	10 1/2	8	7																3.75
3																											
4																											
5																											

STANDARD SIGN
R9-9A

WISCONSIN DEPT OF TRANSPORTATION

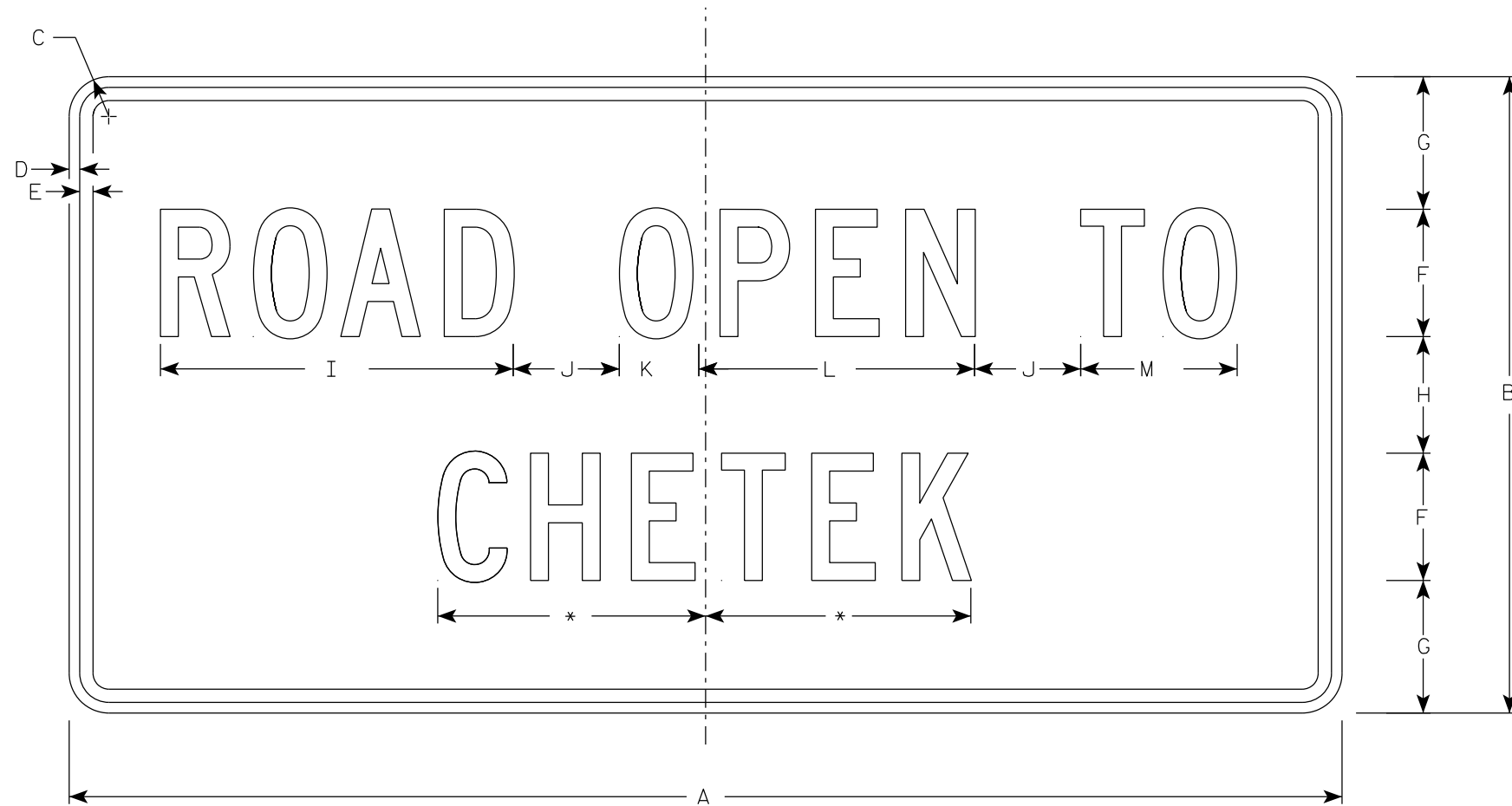
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/24/24 PLATE NO. R9-9A.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate message and optically balance.



R10-61

*See note 5

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	24	1 1/2	1/2	5/8	4	5 1/2	5	10 3/4	2	2 1/8	8 3/8	4 5/8														6.0
2S	60	30	1 7/8	1/2	5/8	6	6 1/4	5 1/2	16 5/8	5	3 3/4	13	7 3/8														12.5
2M	60	30	1 7/8	1/2	5/8	6	6 1/4	5 1/2	16 5/8	5	3 3/4	13	7 3/8														12.5
3																											
4																											
5																											

STANDARD SIGN
R10-61

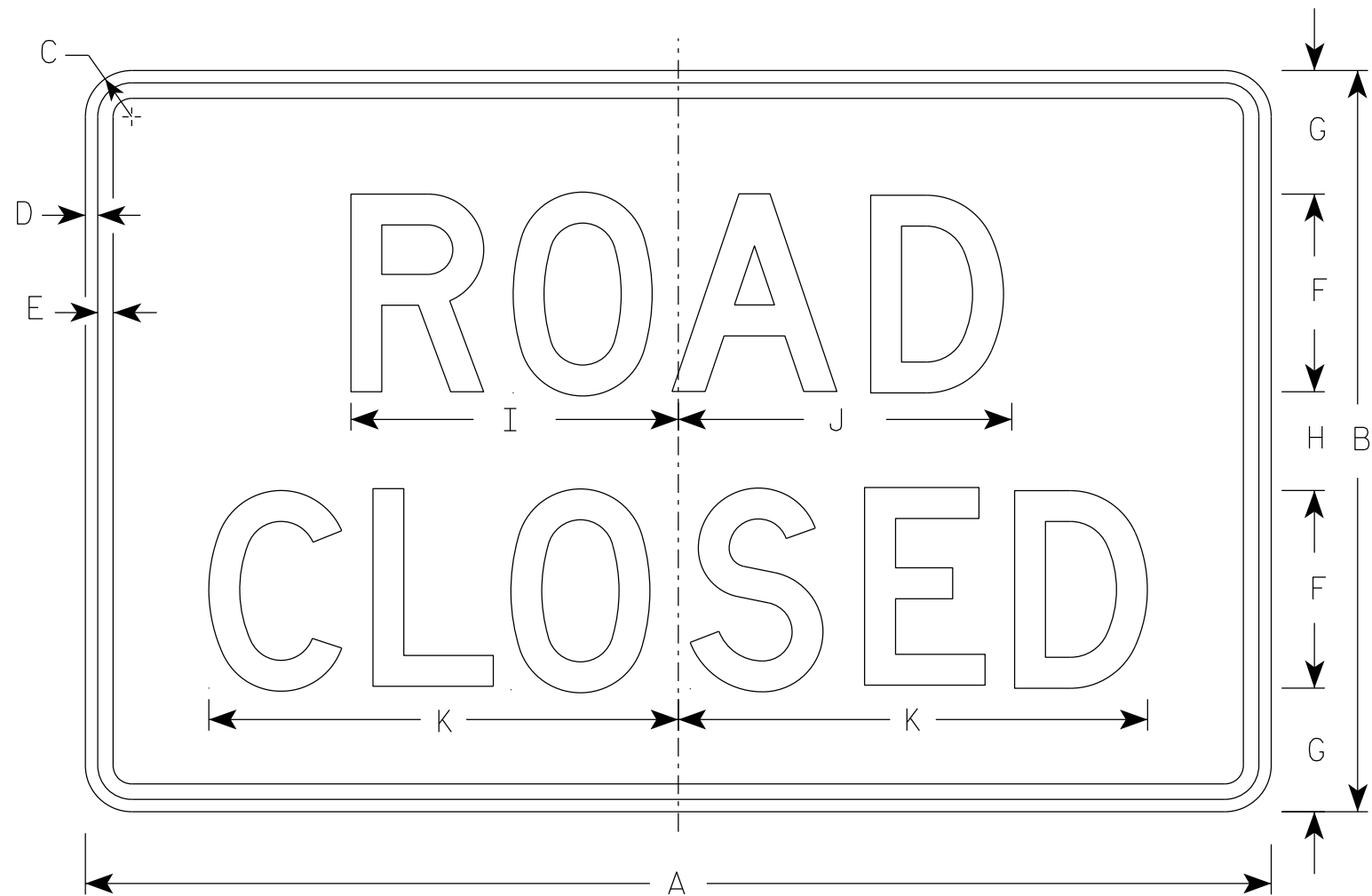
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

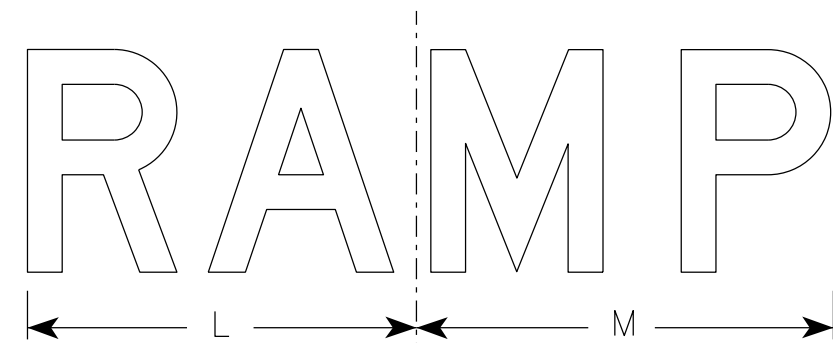
DATE 2/5/24 PLATE NO. R10-61.6

7

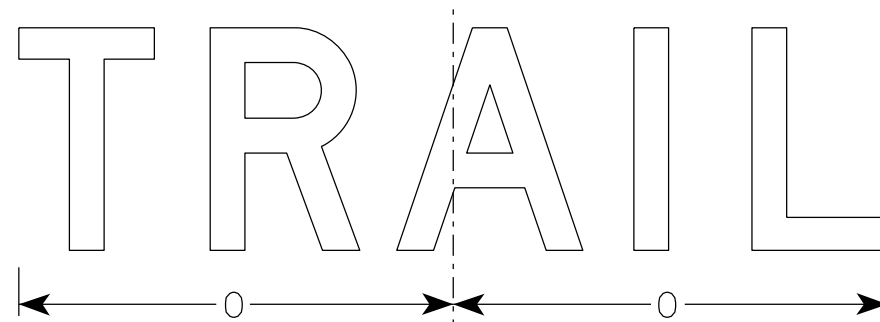
7



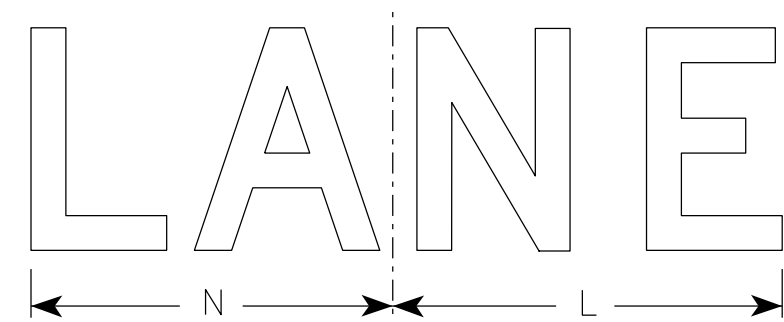
R11-2



R11-2R



R11-2T



R11-2L

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Modify the message as required.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
2M	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
3	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
4	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
5	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0

STANDARD SIGN
R11-2

WISCONSIN DEPT OF TRANSPORTATION

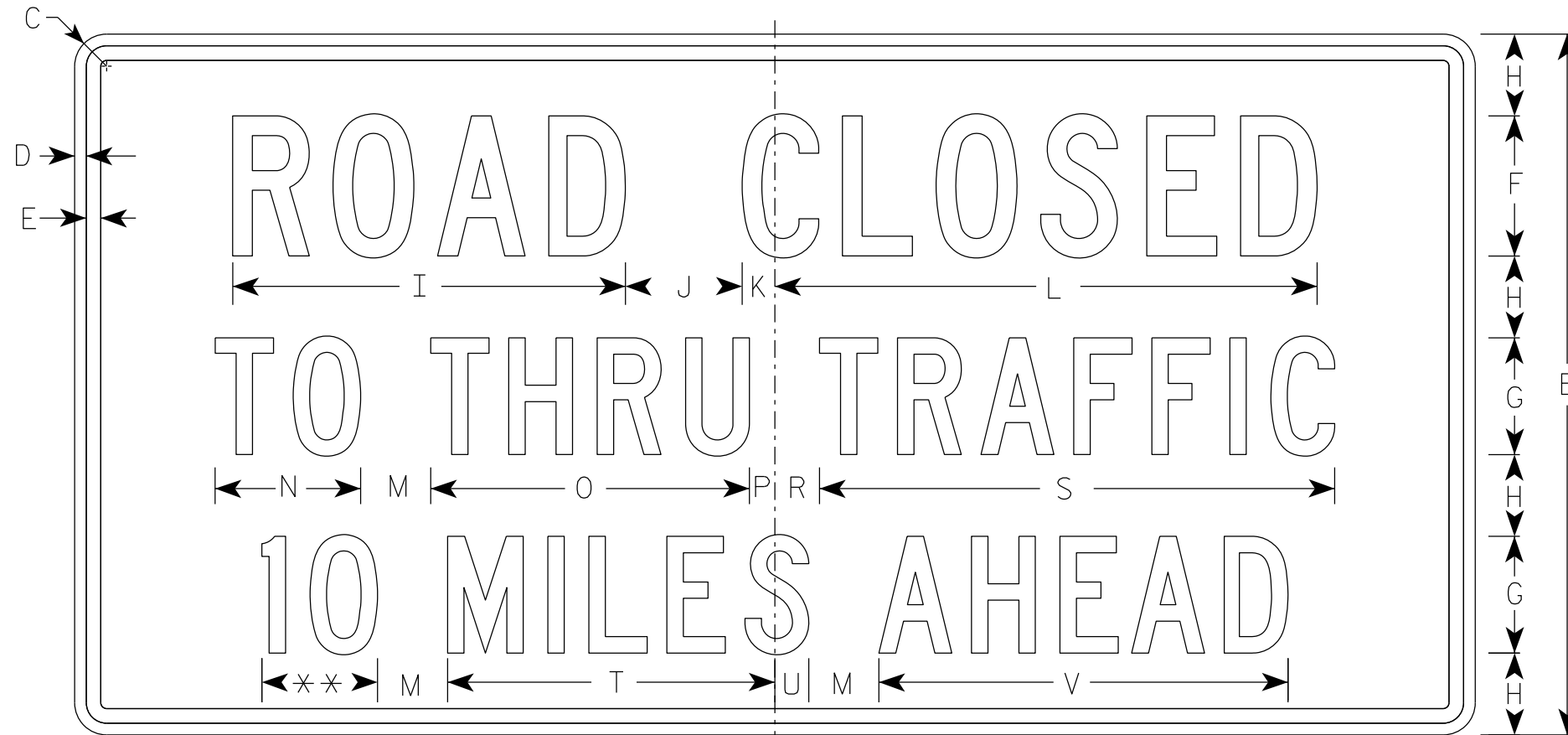
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 2/5/24 PLATE NO. R11-2.12

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

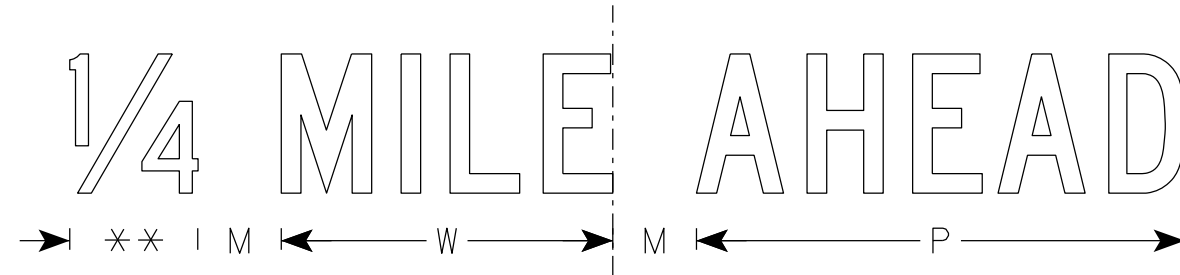
NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals to nearest quarter mile and optically adjust spacing to achieve proper balance.



R11-3

** See Note 5



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	
1	36	18	1 1/2	3/8	3/8	4	3	2	11 1/4	3	1 1/8	15 3/8	2	3 3/4	8 1/4	5/8		1 3/8	13 1/4	8 3/8	7/8	10 1/2	7 1/8				4.5	
2S	60	30	1 7/8	1/2	5/8	6	5	3 1/2	16 7/8	5	1 3/8	23 1/4	3	6 1/4	13 5/8	1 1/8		1 7/8	22 1/8	14	1 1/2	17 1/2	11 7/8				12.5	
2M	60	30	1 7/8	1/2	5/8	6	5	3 1/2	16 7/8	5	1 3/8	23 1/4	3	6 1/4	13 5/8	1 1/8		1 7/8	22 1/8	14	1 1/2	17 1/2	11 7/8				12.5	
3																												
4																												
5																												

STANDARD SIGN
R11-3

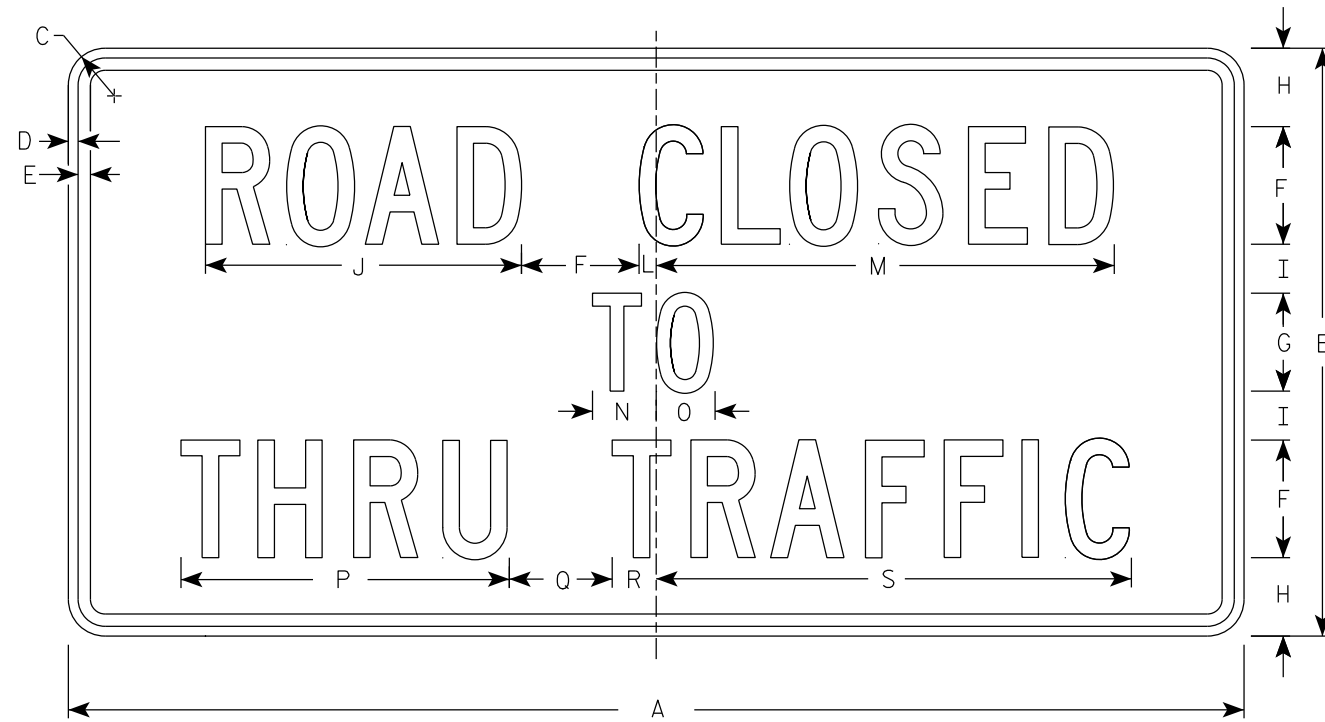
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/5/24 PLATE NO. R11-3.10

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R11-4

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	60	30	1 7/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
2M	60	30	1 7/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
3																											
4																											
5																											

STANDARD SIGN
R11 - 4

WISCONSIN DEPT OF TRANSPORTATION

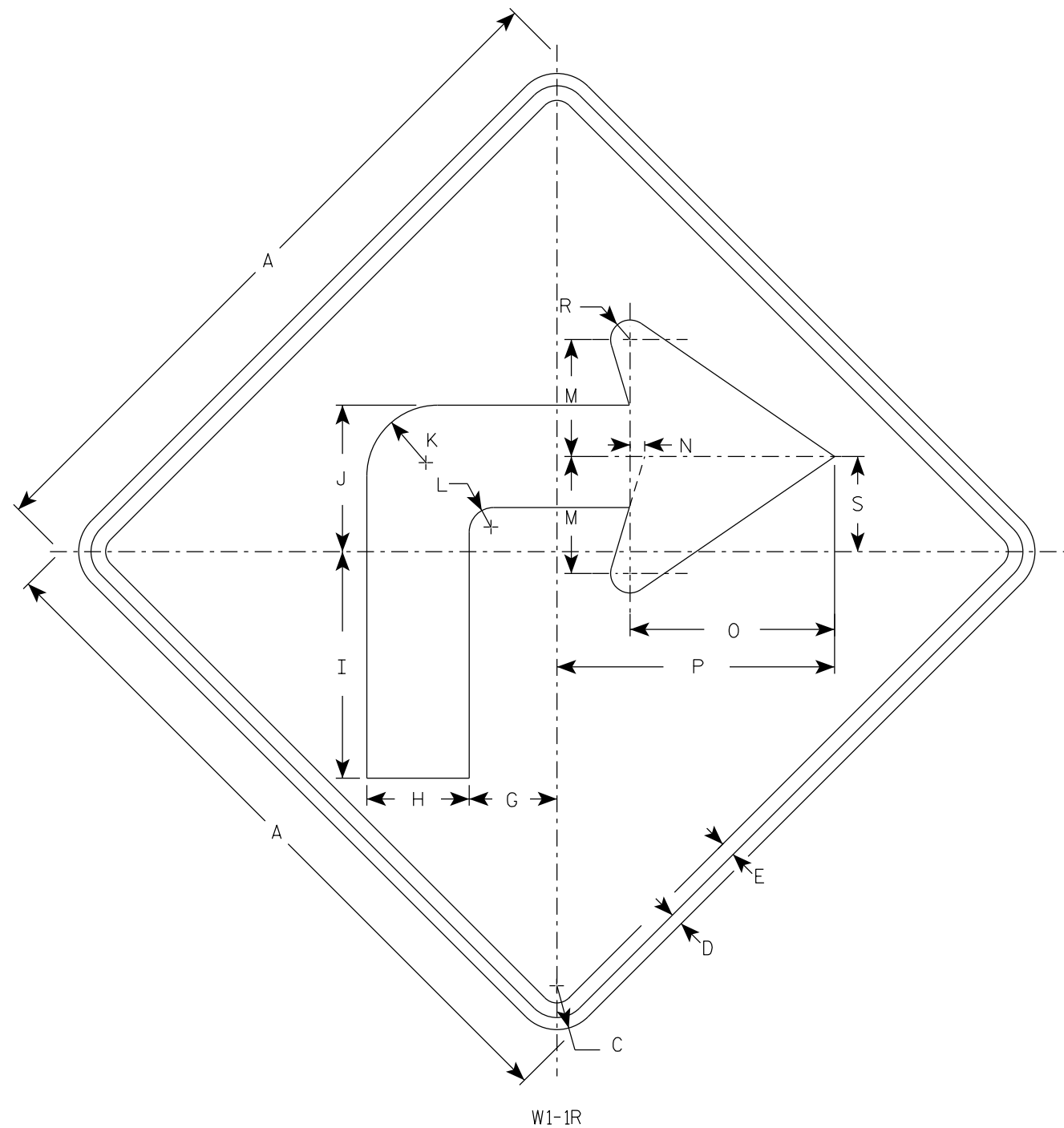
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/5/24 PLATE NO. R11-4.4

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Yellow
Message - Black
3. W1-1L is the same as W1-1R except the arrow is reversed along the vertical centerline.



W1-1R

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/2	3/8	1/2		3	3 1/2	7 3/4	5	2 1/2	7/8	4	1/2	7	9 1/2		5/8	3 1/4								4.0
2S	36		2 1/4	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
2M	36		2 1/4	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
3	36		2 1/4	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
4	48		3	3/4	1		6	7	15 1/2	10	4 7/8	1 5/8	8	1	14	19		1 1/4	6 1/2								16.0
5	48		3	3/4	1		6	7	15 1/2	10	4 7/8	1 5/8	8	1	14	19		1 1/4	6 1/2								16.0

STANDARD SIGN
W1-1

WISCONSIN DEPT OF TRANSPORTATION

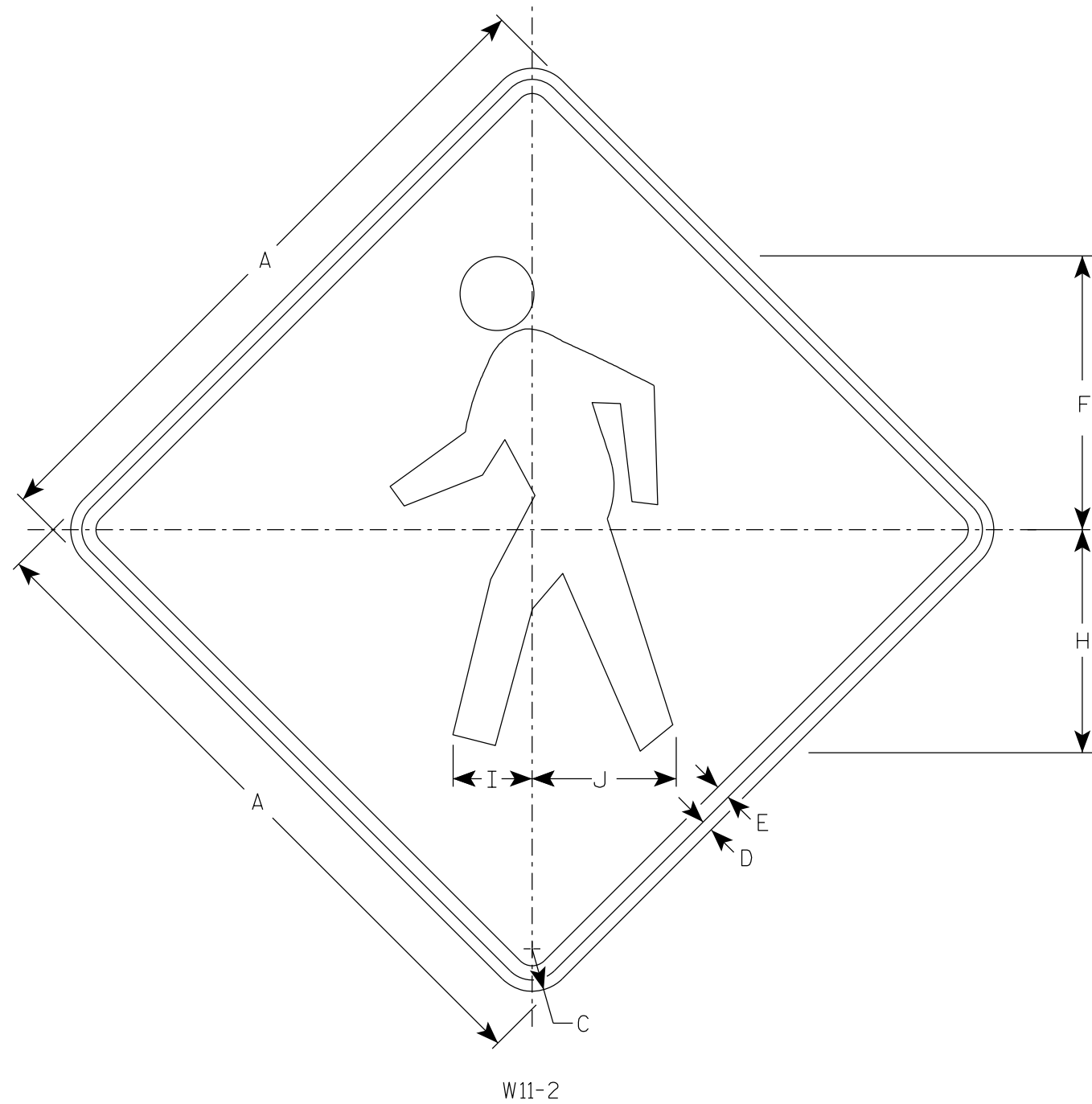
APPROVED *Matthew P. Rauch*
for State Traffic Engineer

DATE 3/22/2023 PLATE NO. W1-1.12

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
 Background - Yellow
 Message - Black



W11-2

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/2	3/8	1/2	9 3/4		7 7/8	2 7/8	5 1/8																	4.0
2S	30		1 7/8	1/2	5/8	12 1/8		9 7/8	3 1/2	6 3/8																	6.25
2M	36		2 1/4	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
3	36		2 1/4	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
4	48		3	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
5																											

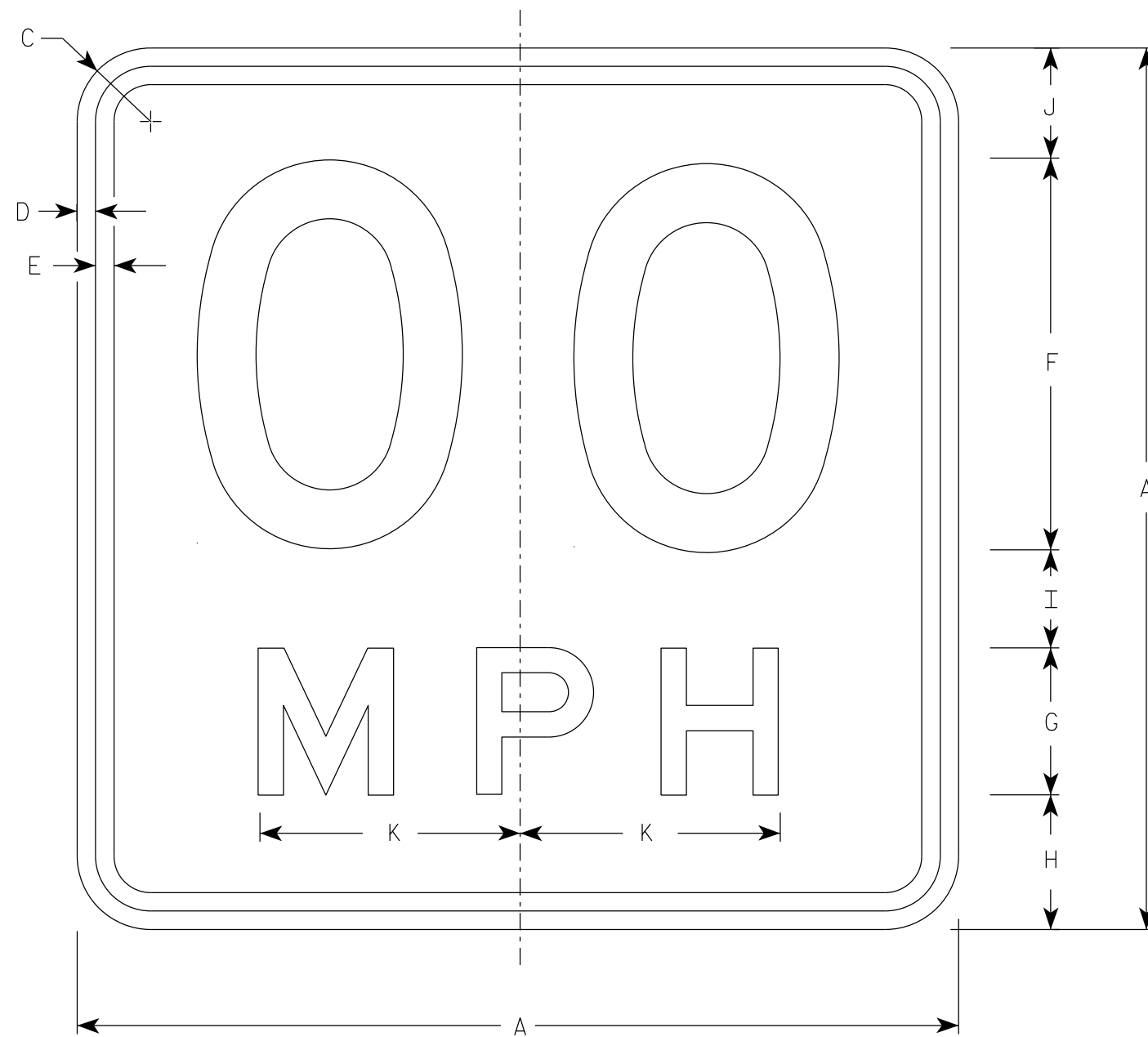
STANDARD SIGN
W11-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
 For State Traffic Engineer

DATE 6/15/2023 PLATE NO. W11-2.9

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Yellow
Message - Black
3. Message Series - See Note 5
4. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
5. Line 1 is Series D
Line 2 is Series E

W13-1

* For 30" x 30" Warning Signs, use 18" x 18" W13-1 signs.
For 36" x 36" Warning Signs, use 24" x 24" W13-1 signs.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18		1 1/2	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2S	18		1 1/2	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2M	18		1 1/2	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
3	24		1 1/2	3/8	1/2	10	4	4	2 3/4	3 1/4	6 5/8																4.00
4	36		2 1/4	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
5	36		2 1/4	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00

STANDARD SIGN
W13-1

WISCONSIN DEPT OF TRANSPORTATION

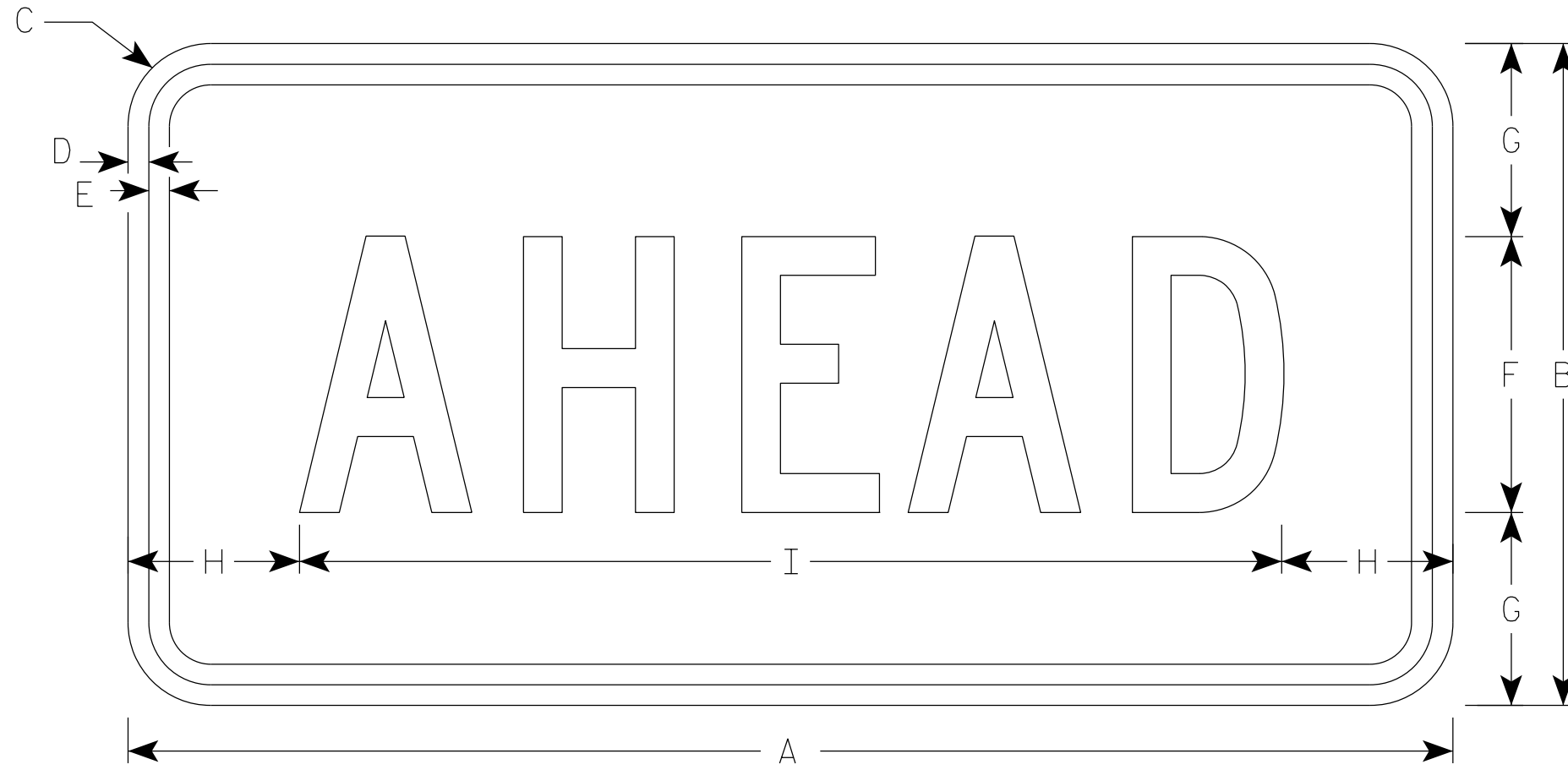
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 1/8/2024 PLATE NO. W13-1.17

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Yellow
Message - Black
3. Message Series - C



W16-9P

- * For 36" x 36" Warning Signs, use 30" x 18" W16-9P signs.
- * For 48" x 48" Warning Signs, use 48" x 24" W16-9P signs.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	3/8	3/8	5	3 1/2	3 1/8	17 3/4																		2.0
* 2M	30	18	1 1/2	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
* 3	30	18	1 1/2	3/8	1/2	7	3 1/2	2 3/4	24 1/2																		3.75
* 4	48	24	1 7/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
5																											

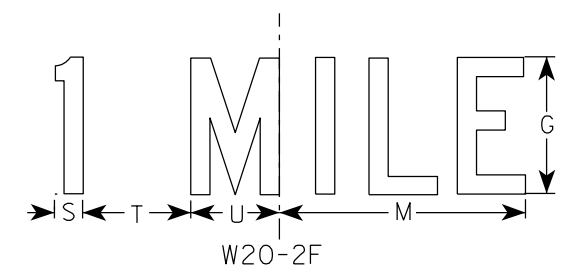
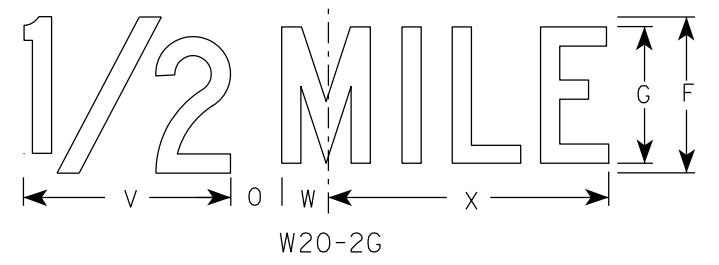
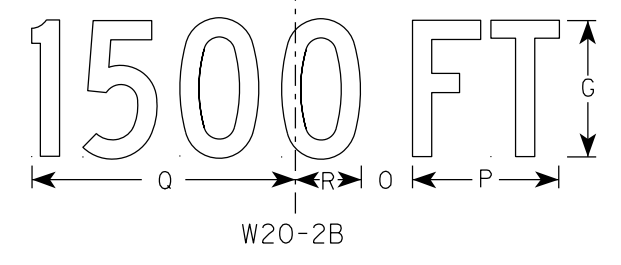
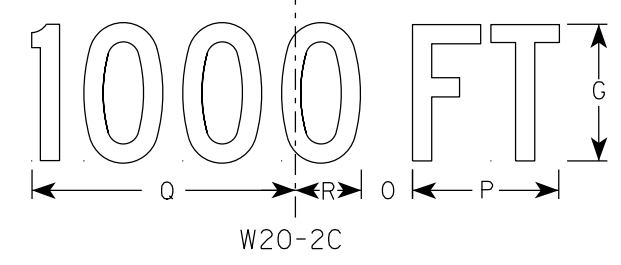
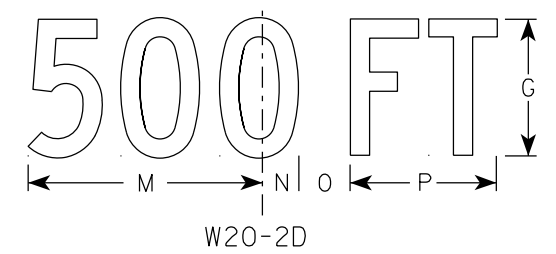
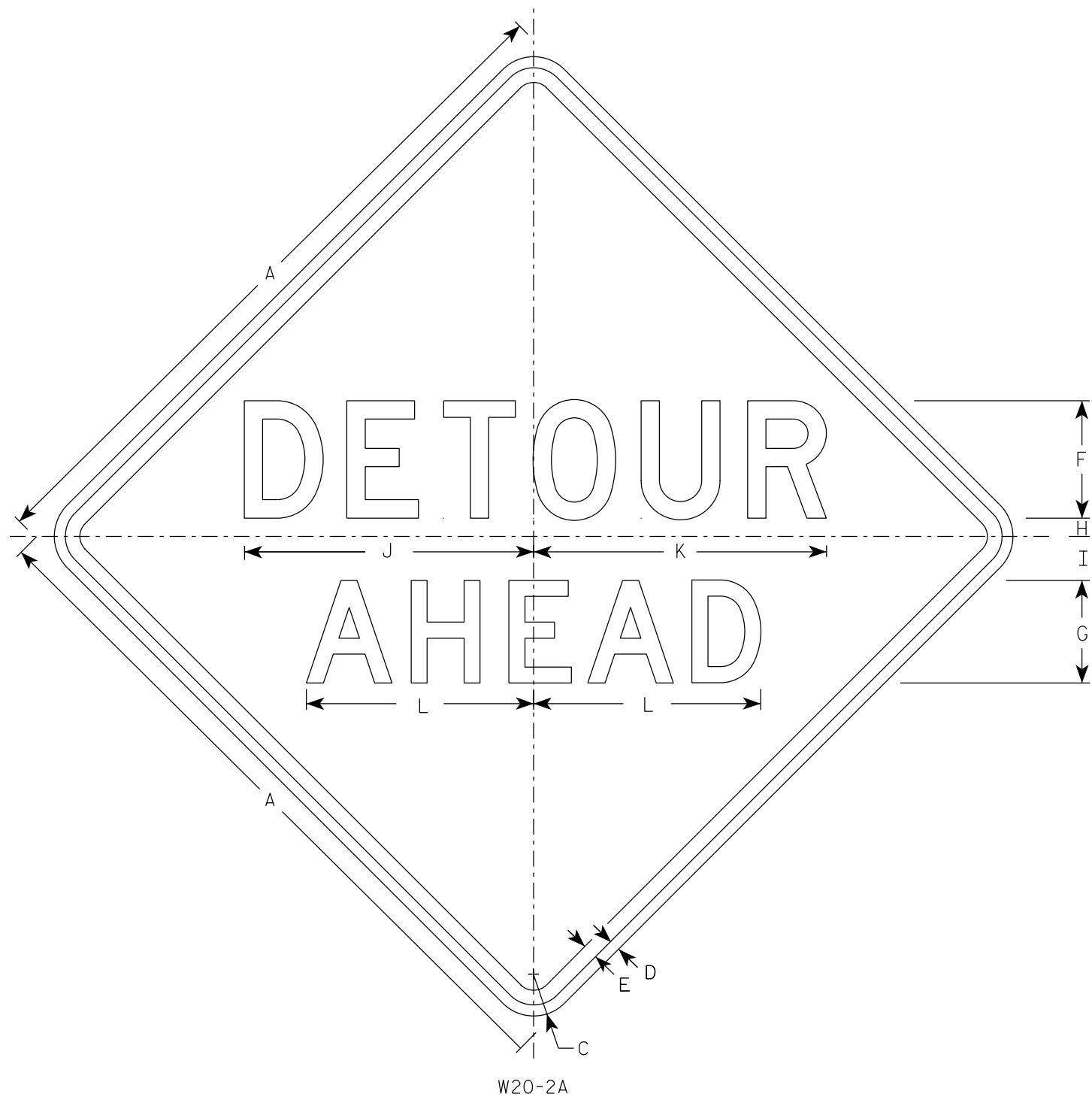
STANDARD SIGN
W16-9P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/9/2024 PLATE NO. W16-9P.9

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**



NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
Line 2 is Series D for AHEAD and Series C for all other distances.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
2M	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
3	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
4	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
5	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0

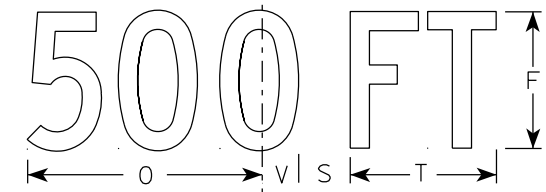
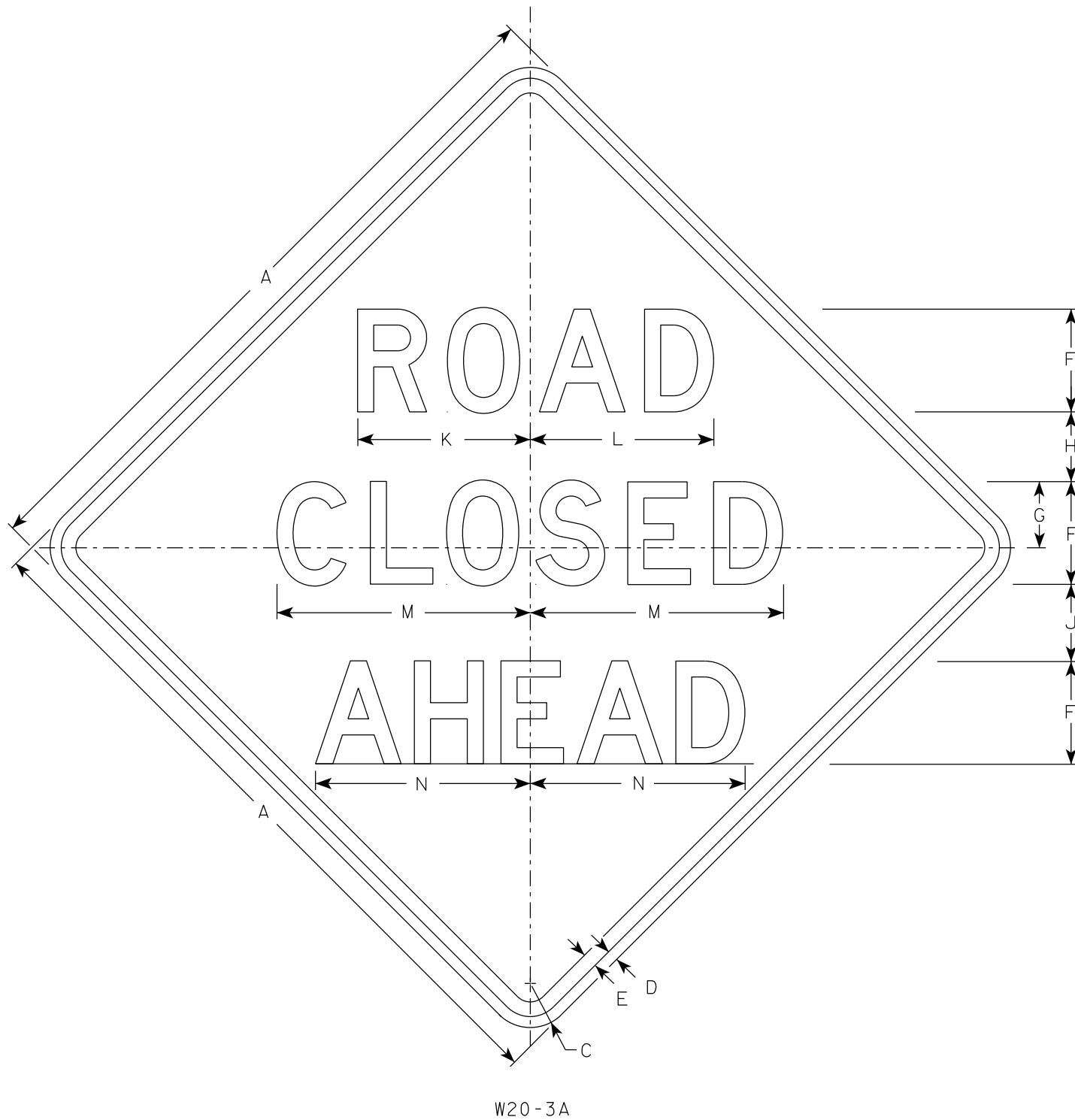
STANDARD SIGN
W20-2A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

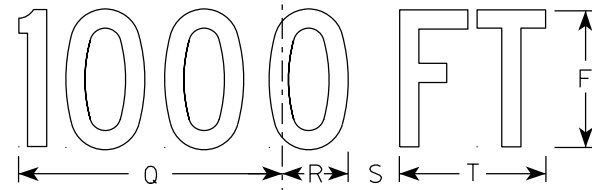
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 1/10/2024 PLATE NO. W20-2.7

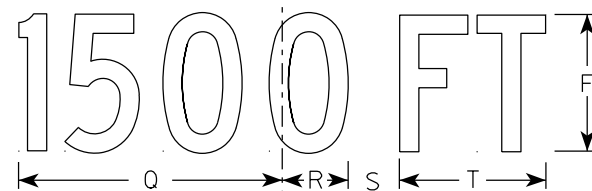
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



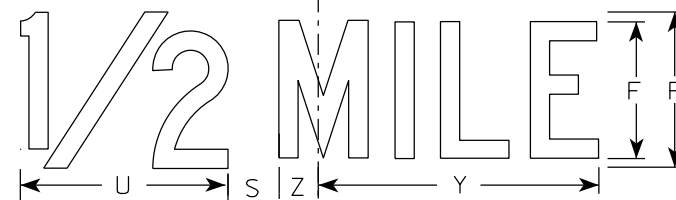
W20-3D



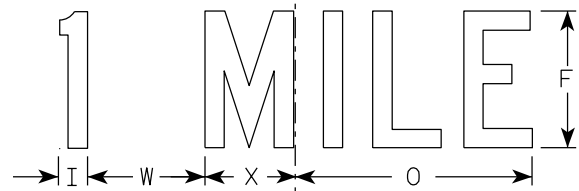
W20-3C



W20-3B



W20-3G



W20-3F

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.
Line 3 is Series D for AHEAD and Series C for all other distances.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	5	3 3/8	3 1/2	1 1/8	4	8 3/8	8 7/8	12 1/2	11	9	6	10 1/8	2 1/2	1 7/8	5 5/8	8	1 3/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
3	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
4	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
5	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0

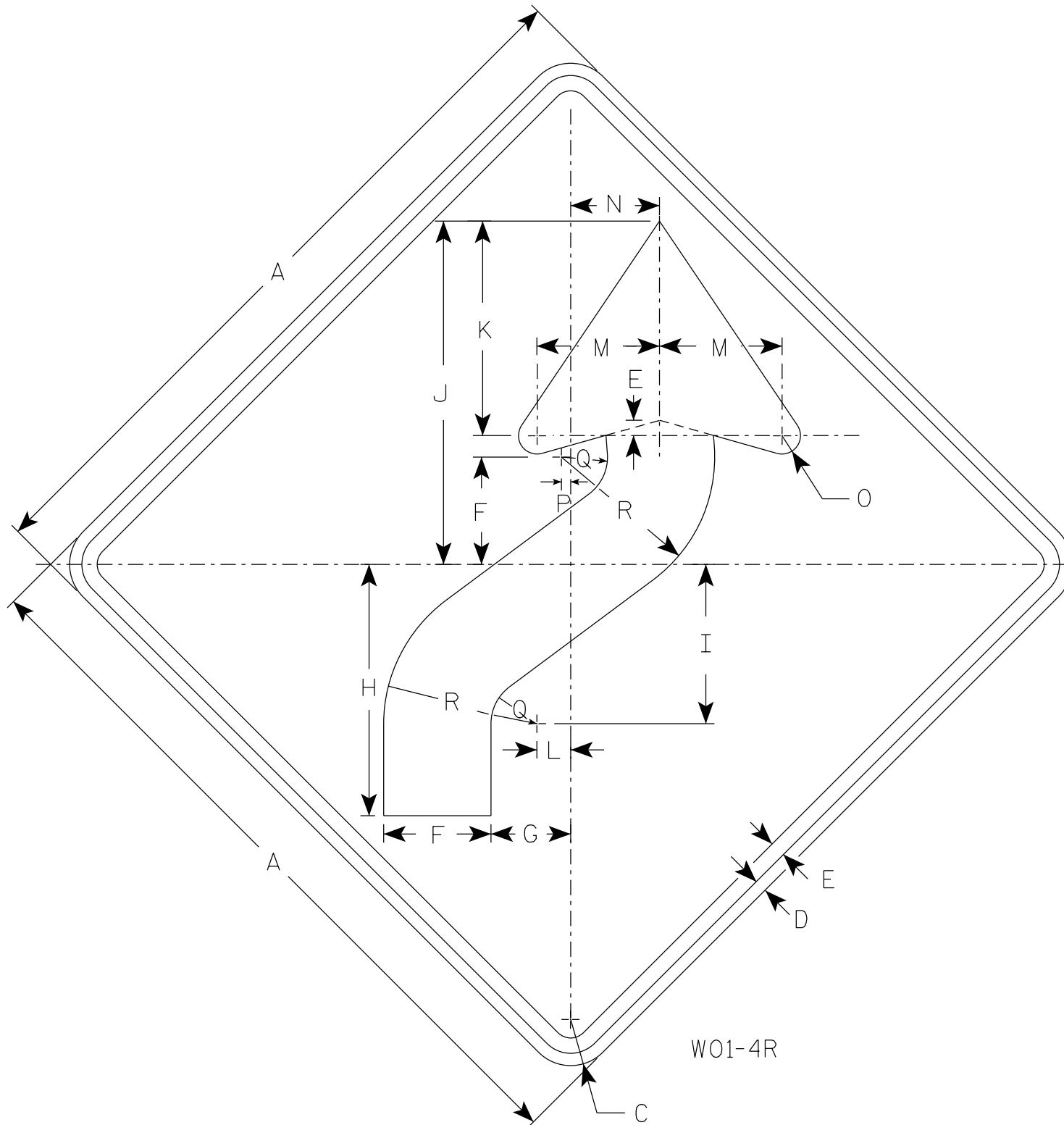
STANDARD SIGN
W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 1/10/2024 PLATE NO. W20-3.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W01-4L is the same as W01-4R except the arrow is reversed along the vertical centerline.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
2S	48		3	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
2M	48		3	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
3	48		3	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
4	48		3	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
5	48		3	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0

STANDARD SIGN
W01-4

WISCONSIN DEPT OF TRANSPORTATION

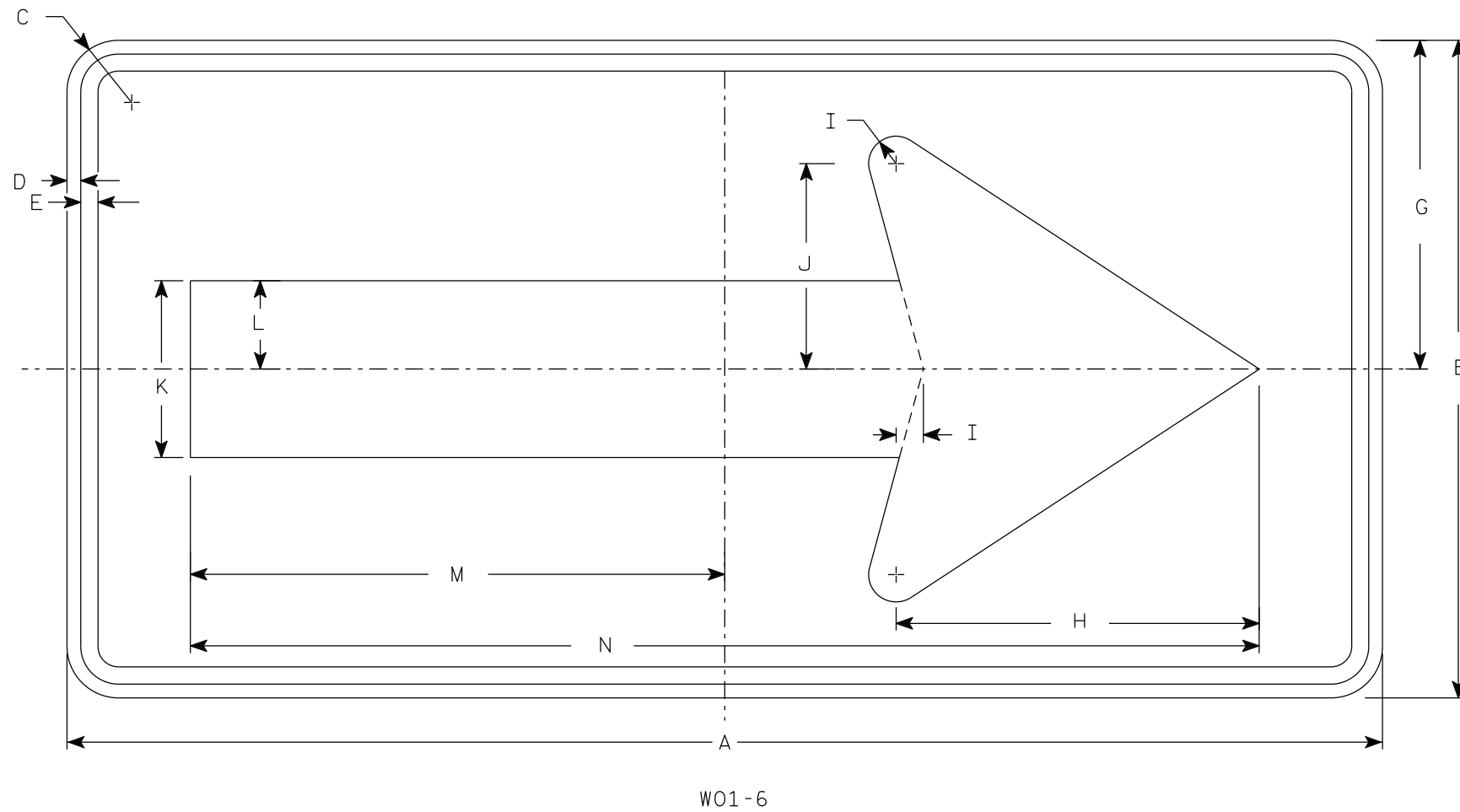
APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 1/24/2024 PLATE NO. W01-4.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	24	1 7/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 7/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 7/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 7/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
5	60	30	1 7/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5

STANDARD SIGN
W01-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED
Matthew R. Rauch
for State Traffic Engineer

DATE 1/24/2024 PLATE NO. W01-6.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

** SEE ROADWAY PLANS FOR TRAFFIC CONTROL STAGING.



CONTRACTOR TO DETERMINE EXACT EXISTING UTILITY LOCATIONS. ALL UTILITIES TO REMAIN IN SERVICE. COORDINATE WITH THE UTILITY COMPANIES.

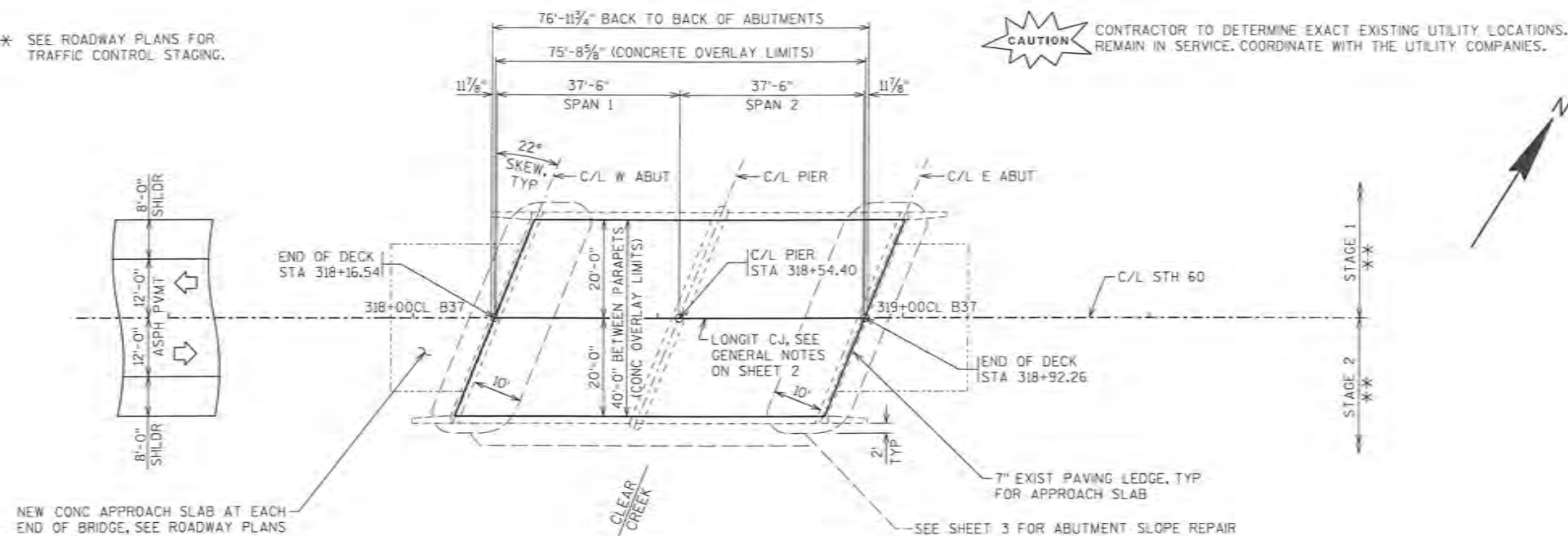
DESIGN DATA

LIVE LOAD:
 DESIGN LOADING: HS-20
 INVENTORY RATING HS25
 OPERATING RATING HS42
 WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV) = 200 KIPS

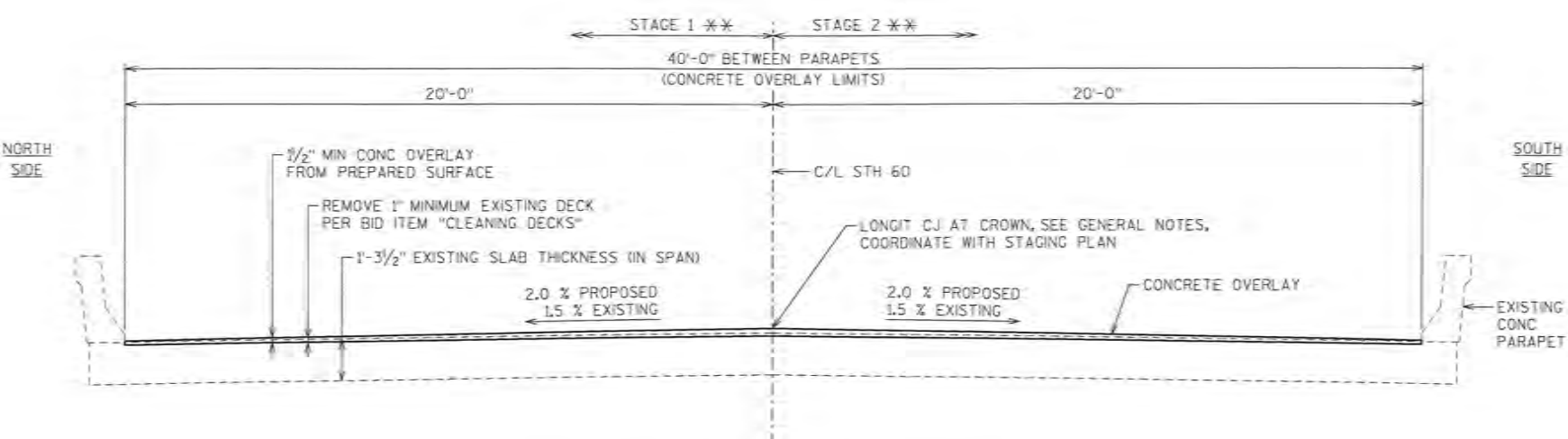
MATERIAL PROPERTIES:
 CONCRETE MASONRY - SUPERSTRUCTURE $f_c = 4,000$ psi
 - OVERLAY $f_c = 4,000$ psi
 - ALL OTHER $f_c = 3,500$ psi
 HIGH STRENGTH BAR STEEL REINFORCEMENT
 AASHTO GRADE 60 $f_y = 60,000$ psi

TRAFFIC DATA

ADT (2013) = 2000-2500
 ADT (2033) = 2200-2800
 DHV = 16.5
 DD = 60/40
 T = 19.6 %
 DESIGN SPEED = 60 MPH



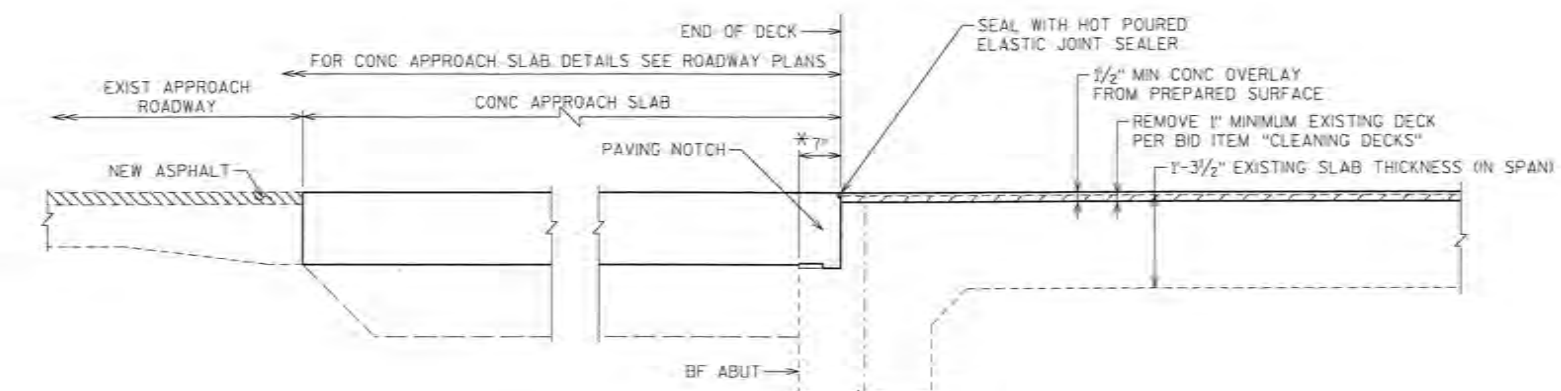
PLAN
 TWO SPAN - HAUNCHED SLAB BRIDGE
 TOP OF DECK SHOWN



CROSS SECTION THRU ROADWAY
 (LOOKING EAST)

LIST OF DRAWINGS

- 1 CONCRETE OVERLAY
- 2 NOTES AND QUANTITIES
- 3 SCOUR, UNDERMINING AND EROSION CONTROL DETAILS



TYPICAL SECTION AT END OF DECK

INDICATES REMOVAL
 *DIMENSIONS GIVEN ARE NORMAL TO C/L OF SUBSTRUCTURE UNIT.



SEH CONTACT: CHR BLUM, PE, 608.620.6192
 WISDOT BRIDGE OFFICE CONTACT: BILL DREHER, PE, 608.266.8489

NO.	DATE	REVISION	BY

SEH SHORT ELLIOTT HENDRICKSON INC.			
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
ACCEPTED	DATE		
<i>William C. Dreher</i> CHIEF STRUCTURES DESIGN ENGINEER	08/01/17		
STRUCTURE B-12-37			
STH 60 OVER CLEAR CREEK			
COUNTY	TOWN/CITY/VILLAGE		
CRAWFORD	MARIETTA		
DESIGN SPEC. REHABILITATION N/A			
DESIGNED BY	DESIGN CK'D.	DRAWN BY	PLANS CK'D.
NCK	CJB	DLF	CJB
CONCRETE OVERLAY		SHEET 1 OF 3	

PLOT TIME: 12:38:19 PM

PLOT DATE: 6/21/2017

FILE NAME: S:\UZ\W\1\aw\104214-prelim-dgn\cadd\12-37\dgn\bl2037pl.dgn

8

8

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

ALL STATIONS AND ELEVATIONS ARE IN FEET.

ALL DIMENSIONS ARE IN FEET AND INCHES UNLESS OTHERWISE NOTED.

STATIONING MAY VARY BASED ON EXACT LOCATION OF BRIDGE TO PROPOSED ALIGNMENT.

DIMENSIONS AND STATIONING SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS AND INSPECTION REPORTS. EXISTING BRIDGE PLANS AVAILABLE AT WISDOT.

CONTRACTOR TO DETERMINE EXACT EXISTING UTILITY LOCATIONS. ALL UTILITIES TO REMAIN IN SERVICE. COORDINATE WITH THE UTILITY COMPANIES.

APPROACH SLABS DESIGN, PLANS AND QUANTITY ARE ROADWAY PLANS ITEM.

ANY EXCAVATION REQUIRED TO COMPLETE THE OVERLAY OR APPROACH SLABS AT THE ABUTMENTS, TO BE CONSIDERED INCIDENTAL TO THE BID ITEM "CONCRETE MASONRY OVERLAY DECKS".

A MINIMUM OF 1-INCH OF CONCRETE SHALL BE REMOVED FROM THE ENTIRE BRIDGE DECK UNDER THE BID ITEM "CLEANING DECKS". CONTRACTOR TO VERIFY COMPLETENESS OF REMOVALS WITH THE FIELD ENGINEER.

LONGITUDINAL CONSTRUCTION JOINTS IN OVERLAY MAY BE USED. LOCATION TO BE DETERMINED BY THE FIELD ENGINEER. COORDINATE WITH STAGING PLANS.

PREPARATION DECKS TYPE 1, PREPARATION DECKS TYPE 2, AND FULL-DEPTH DECK REPAIR AS DETERMINED, LOCATED, MARKED AND MEASURED BY THE FIELD ENGINEER. DECK PREPARATION AND FULL-DEPTH DECK REPAIRS SHALL BE FILLED WITH "CONCRETE MASONRY OVERLAY DECKS".

BOTTOM OF THE EXISTING DECK WILL BE INSPECTED FOR AREAS OF FULL-DEPTH DECK REPAIR PRIOR TO DECK PREPARATION OPERATIONS.

BOTTOM OF THE EXISTING DECK IS TO BE INSPECTED FOR AREAS OF DISTRESS AFTER COMPLETION OF THE DECK PREPARATION AND PRIOR TO OVERLAYING THE BRIDGE.

ALL CONCRETE REMOVAL NOT COVERED WITH A CONCRETE OVERLAY SHALL BE DEFINED BY A 1-INCH DEEP SAW CUT.

PROFILE GRADE LINE SHALL BE DETERMINED IN THE FIELD BASED ON A MINIMUM OVERLAY THICKNESS OF 1/2" PLACED ABOVE THE DECK SURFACE AFTER SURFACE PREPARATION. EXPECTED AVERAGE OVERLAY THICKNESS IS 2 1/8". IF EXPECTED AVERAGE OVERLAY THICKNESS IS EXCEEDED BY MORE THAN 1/2", CONTACT THE STRUCTURES DESIGN SECTION.

AFTER DECK CLEANING OPERATIONS, CLEAN AND FILL EXISTING DECK CRACKS WITH LOW VISCOSITY CRACK SEALER PER SECTION 502.2.11 AS DIRECTED BY THE FIELD ENGINEER. COST IS INCIDENTAL TO BID ITEM "CONCRETE MASONRY OVERLAY DECKS".

APPLY A PROTECTIVE SURFACE TREATMENT TO TOP OF BRIDGE DECK, TOP OF APPROACH SLABS AND TOP OF APPROACH SLAB NOTCH. APPLY PIGMENTED SURFACE SEALER TO TOP AND INSIDE FACES OF PARAPET PER THE STANDARD SPECIFICATIONS.

AT THE ABUTMENTS, THE DECK JOINT IS COMPRISED OF A FILLER AND SEALANT, THERE IS NO STRIP SEAL OR COVER PLATED EXPANSION JOINT.

TOTAL ESTIMATED QUANTITIES - B-12-37

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
492.2010.S	SEALING CRACKS AND JOINTS WITH HOT-APPLIED SEALANT	GAL	0.6
③ 502.3200	PROTECTIVE SURFACE TREATMENT	SY	350
③ 502.3210	PIGMENTED SURFACE SEALER	SY	85
509.0301	PREPARATION DECKS TYPE 1	SY	14
509.0302	PREPARATION DECKS TYPE 2	SY	8
② 509.0500	CLEANING DECKS	SY	337
⑤ 509.1500	CONCRETE SURFACE REPAIR	SF	1
⑤ 509.2000	FULL-DEPTH DECK REPAIR	SY	1
① 509.2500	CONCRETE MASONRY OVERLAY DECKS	CY	22
④ 509.9050.S	CLEANING PARAPETS	LF	190
SPV.0035.01	SCOUR REPAIR, GROUT	CY	35
SPV.0035.02	SCOUR REPAIR, GROUT BAGS	CY	50
SPV.0165.01	ARTICULATING BLOCK FABRICATION MAT SLOPE REPAIR	SF	1080
	NON-BID ITEMS		
	FILLER	SIZE	3/4"

QUANTITIES NOTES

- ① CONCRETE FOR:
* PREPARATION DECKS TYPE 1 & 2, *FULL-DEPTH DECK REPAIR, AND, OVERLAY.
- ② BASED ON 1" DEEP BY LIMITS OF OVERLAY.
- ③ APPLY A PROTECTIVE SURFACE TREATMENT TO TOP OF BRIDGE DECK, TOP OF APPROACH SLAB NOTCH AND, TOP OF APPROACH SLABS. APPLY PIGMENTED SURFACE SEALER TO TOP, END AND INSIDE FACES OF PARAPET. APPLICATION AND QUANTITY FOR PROTECTIVE SURFACE TREATMENT ON APPROACH SLABS ARE CONSIDERED INCIDENTAL TO BRIDGE BID ITEM "PROTECTIVE SURFACE TREATMENT".
- ④ INCLUDES PARAPETS ON WINGWALLS.
- ⑤ UNDISTRIBUTED AMOUNT.

* THESE QUANTITIES TOTALS ARE AN ESTIMATE AND ARE INCLUDED IN BID ITEM "CONCRETE MASONRY OVERLAY, DECKS".

-PREPARATION DECKS TYPE 1 = 0.8 CY
-PREPARATION DECKS TYPE 2 = 0.4 CY
-FULL-DEPTH DECK REPAIR = 0.3 CY

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-12-37			
DRAWN BY		DLF	PLANS CK'D. CJB
NOTES AND QUANTITIES			SHEET 2 OF 3

NOTES

BID ITEM FOR EROSION CONTROL AT BOTH ABUTMENTS SHALL BE "ARTICULATING BLOCK FABRICATION MAT SLOPE REPAIR", WHICH INCLUDES ARTICULATING BLOCK FABRICATION MAT, RIPRAP, GEOTEXTILE FABRIC, GROUT TUBE AND ADHESIVE ANCHORS.

GROUT BAGS SHALL BE PAID FOR UNDER BID ITEM "SCOUR REPAIR, GROUT BAGS".

GROUT TO FILL VOID SHALL BE PAID FOR UNDER BID ITEM "SCOUR REPAIR, GROUT".

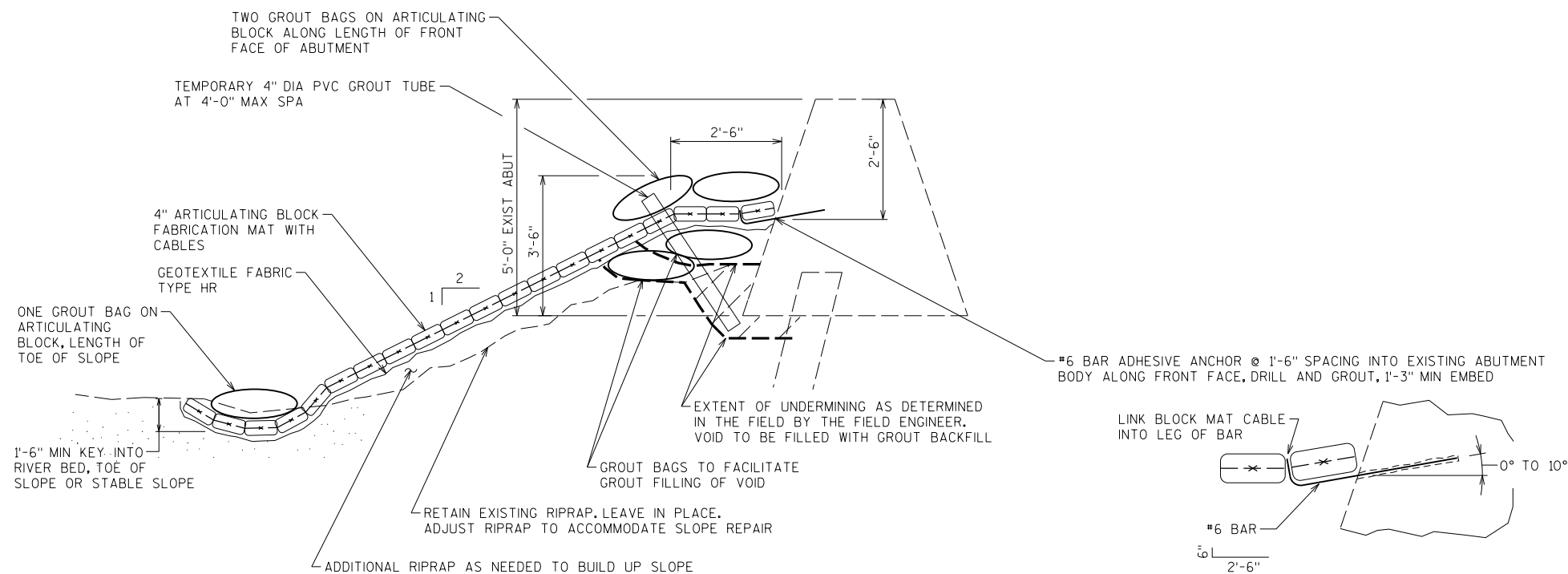
IT IS EXPECTED THAT DIMENSIONS AND QUANTITIES MAY/WILL VARY BASED ON ACTUAL COMPLETED INPLACE LIMITS. COORDINATE LIMITS AND COMPLETENESS OF PROPOSED WORK.

EXTENT OF SCOUR AND UNDERMINING OF ABUTMENTS ARE FROM THE BRIDGE INSPECTION REPORT AND FIELD VISIT.

GROUT BAGS SHALL BE 3' WIDE X 4' LONG WITH 1'-0" MAX THICKNESS.

BAGS AND MAT ARE TO BE PLACED SO THAT THERE IS NO GAP BETWEEN THE BAGS, MAT AND ABUTMENT.

ADJACENT MATS SHALL BE JOINED BEFORE FILLING THE MATS WITH GROUT.



ABUTMENT SLOPE REPAIR

WEST AND EAST ABUTMENT
(SEE SHEET 1 FOR EXTENTS)

PLOT TIME: 12:38:19 PM

PLOT DATE: 6/21/2017

FILE NAME : S:\UZ\W\W\17sw\140521\4-prelim-dsgn-rpts\43-prelim-dsgn\CAD\B-12-37\dgn\B12037scour.dgn

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-12-37			
DRAWN BY DLF		PLANS CK'D. CJB	
SCOUR, UNDERMINING AND EROSION CONTROL DETAILS			SHEET 3 OF 3

PROJECT ID 5790-02-72

CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
0010	84+60	8460.0	0	0.00	0.00	0	0	0	0	0
	85+00	8500.0	40	0.03	0.00	0	0	0	0	0
	86+00	8600.0	100	0.12	0.00	0	0	0	0	0
	87+00	8700.0	100	1.41	0.02	3	0	3	0	3
	88+00	8800.0	100	0.05	0.87	3	2	6	2	4
	89+00	8900.0	100	0.03	0.00	0	2	6	4	2
	90+00	9000.0	100	0.01	0.00	0	0	6	4	2
	90+91	9091.0	91	0.00	0.76	0	1	6	6	0
	91+00	9100.0	9	0.00	0.63	0	0	6	6	0
	92+00	9200.0	100	0.00	0.16	0	1	6	8	-2
	93+00	9300.0	100	0.02	0.00	0	0	6	8	-2
	94+00	9400.0	100	0.03	0.00	0	0	6	8	2
	95+00	9500.0	100	0.00	1.17	0	2	6	11	-5
	95+14	9514.4	14	0.00	0.72	0	1	6	12	-5
	96+00	9600.0	86	0.00	0.00	0	1	6	13	-7
	97+00	9700.0	100	0.00	0.00	0	0	6	13	-7
	98+00	9800.0	100	0.00	0.00	0	0	6	13	-7
	98+60	9860.0	60	0.00	4.45	0	5	6	19	-13
	99+00	9900.0	40	0.00	7.51	0	9	6	30	-24
	100+00	10000.0	100	0.00	5.81	0	25	6	61	-55
	101+00	10100.0	100	0.00	3.01	0	16	6	81	75
	102+00	10200.0	100	0.00	2.97	0	11	6	95	-89
	103+00	10300.0	100	0.00	0.00	0	6	6	102	-96
	104+00	10400.0	100	0.00	0.00	0	0	6	102	-96
	104+85	10485.5	85	0.00	0.00	0	0	6	102	-96
	105+00	10500.0	15	0.00	0.00	0	0	6	102	-96
	106+00	10600.0	100	0.00	0.17	0	0	6	103	-96
	107+00	10700.0	100	0.00	0.58	0	1	6	104	98
	107+52	10752.4	52	0.00	0.00	0	1	6	105	-99
	108+00	10800.0	48	0.00	0.00	0	0	6	105	-99
	109+00	10900.0	100	0.00	0.00	0	0	6	105	-99
	110+00	11000.0	100	0.02	0.00	0	0	6	105	-99
	111+00	11100.0	100	0.00	0.02	0	0	6	105	99
	112+00	11200.0	100	0.00	0.58	0	1	6	106	-100
	113+00	11300.0	100	0.00	0.24	0	2	6	108	-102
	114+00	11400.0	100	0.00	4.44	0	9	6	119	-113
	115+00	11500.0	100	0.00	1.13	0	29	6	155	-149
	116+00	11600.0	100	0.00	13.36	0	45	6	212	-206
	117+00	11700.0	100	0.00	8.53	0	41	6	263	-256
	117+04	11704.0	4	0.00	7.33	0	1	6	264	-258
	117+29	11729.0	25	0.00	4.98	0	6	6	271	-265
	117+54	11754.0	25	0.00	3.22	0	4	6	276	270
	118+00	11800.0	46	0.71	0.00	1	3	7	279	-272
	119+00	11900.0	100	0.00	0.52	1	1	8	281	-272
	120+00	12000.0	100	0.00	4.04	0	8	8	291	-283
	120+69	12069.3	69	0.00	6.11	0	13	8	307	-299
	120+94	12094.3	25	0.00	2.16	0	4	8	312	-304
						8	250			

9

PROJECT ID 5790-02-72

CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
0010	120+94	12094.3	0	0.00	2.16	0	0	0	0	0
	121+00	12100.0	6	0.00	0.59	0	0	0	0	0
	121+19	12119.3	19	0.03	0.00	0	0	0	0	0
	121+50	12149.6	30	0.00	0.05	0	0	0	0	0
	122+00	12200.0	50	0.00	0.99	0	1	0	-	-1
	123+00	12300.0	100	0.00	0.00	0	2	0	3	3
	124+00	12400.0	100	0.00	5.77	0	11	0	15	-15
	125+00	12500.0	100	0.00	4.26	0	19	0	26	-26
	125+78	12577.7	78	0.33	0.00	0	6	0	22	-22
	126+00	12600.0	22	0.00	0.00	0	0	0	26	-26
	126+03	12602.7	3	0.00	0.03	0	0	0	23	-22
	126+28	12627.7	25	0.00	3.56	0	2	0	28	-28
	127+00	12700.0	72	0.00	5.09	0	12	0	37	-36
	128+00	12800.0	100	0.00	3.93	0	17	0	49	-49
	129+00	12900.0	100	0.00	2.78	0	12	0	53	-52
	129+06	12906.5	6	0.00	3.38	0	1	0	53	-53
	129+32	12931.7	25	0.00	4.92	0	4	0	58	-58
	129+57	12957.0	25	0.00	5.59	0	5	0	64	-64
	129+65	12965.4	8	0.00	4.46	0	2	0	66	66
	130+00	13000.0	35	0.00	4.62	0	6	0	74	-73
						1	98			

PROJECT ID 5790-02-72

CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
0010	130+00	13000.0	0	0.00	4.62	0	0	0	0	0
	131+00	13100.0	100	0.00	11.01	0	29	0	36	-36
	132+00	13200.0	100	0.00	12.80	0	44	0	55	-55
	133+00	13300.0	100	0.00	11.19	0	44	0	92	-92
	134+00	13400.0	100	0.00	0.64	0	22	0	83	-83
	135+00	13500.0	100	0.03	0.00	0	1	0	93	-93
	136+00	13600.0	100	0.02	0.00	0	0	0	83	-82
	137+00	13700.0	100	0.00	6.01	0	11	0	107	-107
	137+96	13796.1	96	0.00	10.35	0	29	0	119	-119
						0	181			

9

PROJECT NO: 5790-02-72

HWY: STH 171

COUNTY: CRAWFORD

EARTHWORK DATA

SHEET

E

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	139+96	13996.5	0	0.00	1.86	0	0	0	0	0
	140+00	14000.0	4	0.00	2.03	0	0	0	0	0
	140+21	14021.5	21	0.00	2.14	0	2	0	2	-2
	140+46	14046.5	25	0.00	1.26	0	2	0	2	-2
	142+06	14206.5	160	0.00	2.41	0	11	0	16	-16
	142+31	14231.5	25	0.00	0.00	0	1	0	4	-4
	142+56	14256.5	25	0.25	5.19	0	2	0	19	-19
						0	18			

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	145+00	14500.0	0	0.00	0.00	0	0	0	0	0
	146+00	14600.0	100	0.00	0.00	0	0	0	0	0
	147+00	14700.0	100	0.02	0.00	0	0	0	0	0
	148+00	14800.0	100	0.11	0.00	0	0	0	0	0
	149+00	14900.0	100	0.00	0.00	0	0	0	0	0
	150+00	15000.0	100	0.00	0.00	0	0	0	0	0
	151+00	15100.0	100	0.00	1.73	0	3	0	4	-4
	152+00	15200.0	100	0.00	0.60	0	4	0	9	-9
	153+00	15300.0	100	0.00	1.22	0	3	0	14	-13
	154+00	15400.0	100	0.28	0.04	1	2	1	17	16
	155+00	15500.0	100	0.00	2.10	1	4	2	21	-20
	156+00	15600.0	100	0.00	0.00	0	4	2	26	-25
	157+00	15700.0	100	0.03	0.00	0	0	2	26	-25
	157+67	15766.8	67	4.64	5.33	6	7	7	35	-27
	158+00	15800.0	33	0.00	2.36	3	5	10	40	30
	159+00	15900.0	100	0.00	2.29	0	9	10	51	-41
	160+00	16000.0	100	0.00	0.00	0	4	10	57	-46
	161+00	16100.0	100	0.00	9.31	0	17	10	78	-68
	162+00	16200.0	100	0.16	0.44	0	18	11	101	-90
	163+00	16300.0	100	0.00	0.00	0	1	11	102	-91
	164+00	16400.0	100	0.26	0.14	0	0	11	102	-91
	165+00	16500.0	100	0.00	2.27	0	4	17	108	-96
	166+00	16600.0	100	0.03	0.29	0	5	12	114	-102
	167+00	16700.0	100	0.00	0.38	0	1	12	115	-103
	167+45	16745.1	45	0.00	0.32	0	1	12	116	-104
	167+70	16770.0	25	0.00	0.25	0	0	12	116	104
	167+95	16795.0	25	0.00	0.78	0	0	17	117	-105
	168+00	16800.0	5	0.00	0.46	0	0	12	117	-105
	169+00	16900.0	100	0.02	0.00	0	1	12	118	-106
	169+57	16957.4	57	3.35	0.00	4	0	15	118	-102
	170+00	17000.0	43	0.00	0.17	3	0	18	118	100
	170+04	17003.6	4	1.12	0.04	0	0	18	118	-100
	170+29	17028.6	25	0.00	0.22	1	0	19	118	-100
	170+40	17039.7	11	0.00	0.28	0	0	19	118	-100
	170+54	17053.6	14	0.00	0.53	0	0	19	119	-100
	171+00	17100.0	46	0.10	0.11	0	1	19	119	-101
	172+00	17200.0	100	0.00	1.79	0	4	19	124	-105
	173+00	17300.0	100	0.00	5.67	0	14	19	141	-122
	174+00	17400.0	100	0.00	0.52	0	11	19	155	-136
	175+00	17500.0	100	0.02	0.00	0	1	19	157	-138
	176+00	17600.0	100	0.00	0.46	0	1	19	158	-139
	177+00	17700.0	100	0.00	5.62	0	11	19	172	-153
	178+00	17800.0	100	0.00	0.00	0	10	19	185	-166
	179+00	17900.0	100	0.01	0.10	0	0	19	185	-166
	179+78	17978.2	78	13.50	0.00	20	0	39	185	-146
	180+00	18000.0	22	0.00	0.28	5	0	44	185	-141
	181+00	18100.0	100	0.00	3.35	0	7	44	194	150
						44	155			

9

9

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	181+00	18100.0	0	0.00	3.35	0	0	0	0	0
	182+00	18200.0	100	0.00	1.80	0	10	0	12	-12
	183+00	18300.0	100	0.00	5.63	0	14	0	29	-29
	184+00	18400.0	100	0.00	4.65	0	19	0	53	-53
	185+00	18500.0	100	0.00	5.50	0	19	0	76	-76
	185+80	18579.8	80	0.00	11.19	0	25	0	107	-107
	186+00	18600.0	20	0.00	12.88	0	9	0	119	-119
	187+00	18700.0	100	0.00	4.08	0	31	0	158	-158
	188+00	18800.0	100	0.00	0.20	0	8	0	168	-168
	189+00	18900.0	100	0.00	0.33	0	1	0	169	-169
	190+00	19000.0	100	0.00	1.23	0	3	0	173	-173
	191+00	19100.0	100	0.00	9.03	0	19	0	196	-196
	191+95	19195.5	95	0.00	0.29	0	16	0	217	217
	192+00	19200.0	5	0.00	0.71	0	0	0	217	-217
	193+00	19300.0	100	0.00	2.50	0	5	0	223	-223
	194+00	19400.0	100	0.00	1.16	0	7	0	232	-232
	195+00	19500.0	100	0.00	11.55	0	24	0	261	-261
	196+00	19600.0	100	0.00	0.56	0	22	0	289	289
	196+38	19637.8	38	0.01	0.03	0	0	0	290	-290
	197+50	19750.0	112	0.00	0.24	0	1	0	290	-290
	198+00	19800.0	50	0.00	0.30	0	1	0	291	-291
	199+00	19900.0	100	0.00	1.30	0	3	0	295	-295
	200+00	20000.0	100	1.99	0.00	4	2	4	298	-294
	201+00	20100.0	100	0.00	0.00	4	0	7	298	-290
	201+17	20117.1	17	27.65	0.00	9	0	16	298	-282
	202+00	20200.0	83	0.00	1.84	42	3	59	301	-243
	203+00	20300.0	100	0.54	0.03	1	3	60	306	-246
	204+00	20400.0	100	7.50	0.00	15	0	74	306	-231
	204+17	20417.2	17	4.43	0.00	10	0	85	306	-221
	204+72	20472.0	25	0.02	0.00	2	0	87	306	-219
	204+97	20496.7	25	0.00	0.03	0	0	87	306	-219
	205+00	20500.0	3	3.60	0.01	0	0	87	306	218
	206+00	20600.0	100	0.03	0.00	7	0	94	306	-212
	207+00	20700.0	100	0.00	0.00	0	0	94	306	-212
	207+15	20714.8	15	0.60	0.01	0	0	94	306	-212
						94	245			

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	234+43	23443.3	0	0.00	4.94	0	0	0	0	0
	234+69	23468.8	25	0.00	0.82	0	3	0	3	-3
	234+94	23494.2	25	0.17	0.21	0	0	0	4	-4
	235+77	23576.9	83	0.21	0.15	1	1	1	5	-4
	236+00	23600.0	23	0.00	0.77	0	0	1	5	-4
	236+02	23602.3	2	0.00	0.89	0	0	1	5	-5
	236+28	23627.7	25	0.00	2.07	0	1	1	7	-6
						1	6			

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	250-70	25069.9	0	0.00	17.41	0	0	0	0	0
	251-00	25100.0	30	0.00	3.62	0	12	0	15	-15
	252-00	25200.0	100	0.06	0.00	0	7	0	23	-23
	252-72	25272.1	72	0.50	15.88	1	21	1	50	-49
	257-17	25716.6	445	0.00	0.02	4	131	5	213	-208
	257-42	25741.6	25	0.02	0.00	0	0	5	213	-208
	257-67	25766.6	25	0.38	4.26	0	7	5	216	-210
	258-64	25864.4	98	1.08	0.04	3	8	8	225	-218
	259-00	25900.0	36	0.01	0.00	1	0	9	225	-217
	260-00	26000.0	100	0.00	0.00	0	0	9	225	-217
	260-26	26025.9	26	0.00	2.93	0	1	9	227	-219
	260-51	26051.5	26	0.00	4.73	0	4	9	232	-223
	260-77	26077.1	26	0.00	6.21	0	5	9	238	-230
	261-00	26100.0	23	0.00	5.55	0	5	9	244	-236
	261-05	26105.0	5	0.00	13.51	0	2	9	247	238
	262-00	26200.0	95	0.53	0.00	1	24	9	276	-267
	263-00	26300.0	100	0.00	0.00	1	0	10	276	-266
	263-26	26325.9	26	0.00	0.00	0	0	10	276	-266
						10	221			

9

9

PROJECT NO: 5790-02-72

HWY: STH 171

COUNTY: CRAWFORD

EARTHWORK DATA

SHEET

E

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
0010	320-53	32053.4	0	0.00	2.36	0	0	0	0	0
	320-79	32078.8	25	0.00	1.07	0	2	0	2	-2
	321-04	32104.3	25	0.00	0.97	0	1	0	3	-3
	321-17	32117.4	13	1.30	0.05	0	0	0	4	-3
	321-42	32141.9	25	1.40	0.00	1	0	2	4	-2
	321-66	32166.5	25	0.00	0.00	1	0	2	4	-1
	322-00	32200.0	34	0.00	0.00	0	0	2	4	1
	323-00	32300.0	100	0.00	0.00	0	0	2	4	-1
	324-00	32400.0	100	0.00	0.24	0	0	2	4	-2
	325-00	32500.0	100	0.02	0.00	0	0	2	5	-2
	326-00	32600.0	100	0.00	0.03	0	0	2	5	-2
	326-06	32606.4	6	0.00	0.00	0	0	2	5	-2
	326-37	32631.9	26	0.00	2.57	0	1	2	6	-4
	326-55	32655.4	23	0.00	1.77	0	2	2	9	-6
	327-00	32700.0	45	1.47	0.00	1	1	3	10	-7
	328-00	32800.0	100	0.00	0.00	3	0	6	10	-4
	329-00	32900.0	100	2.57	0.00	5	0	11	10	1
	330-00	33000.0	100	0.28	0.00	5	0	16	10	6
	331-00	33100.0	100	0.08	0.00	1	0	17	10	6
	332-00	33200.0	100	0.37	0.00	1	0	18	10	7
	333-00	33300.0	100	0.04	0.00	1	0	18	10	8
	334-00	33400.0	100	5.14	0.00	10	0	28	10	17
	335-00	33500.0	100	3.34	0.00	16	0	44	10	33
	335-68	33568.1	68	1.58	0.00	6	0	50	10	39
	336-00	33600.0	32	0.00	0.00	1	0	51	10	40
	337-00	33700.0	100	4.61	0.00	9	0	59	10	49
	338-00	33800.0	100	0.40	0.00	9	0	69	10	58
	339-00	33900.0	100	0.00	0.01	1	0	69	10	59
	340-00	34000.0	100	0.00	0.00	0	0	69	10	59
	341-00	34100.0	100	0.00	0.00	0	0	69	10	59
	342-00	34200.0	100	0.00	0.00	0	0	69	10	59
	343-00	34300.0	100	5.69	2.06	11	4	80	15	65
	344-00	34400.0	100	42.88	8.08	90	19	170	39	131
	345-00	34500.0	100	10.04	11.27	98	36	268	84	184
	345-04	34503.7	4	10.33	10.28	1	1	269	85	184
	346-00	34600.0	96	15.57	0.00	46	18	315	108	207
	347-00	34700.0	100	26.81	1.64	78	3	394	112	282
	348-00	34800.0	100	21.94	1.08	90	5	484	118	366
	349-00	34900.0	100	15.65	0.59	70	3	554	122	431
	350-00	35000.0	100	7.75	0.00	43	1	597	124	473
	351-00	35100.0	100	3.74	0.00	21	0	618	124	495
	352-00	35200.0	100	2.00	0.00	11	0	629	124	505
	352-89	35289.5	89	6.72	0.00	14	0	643	124	520
						643	99			

9

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
0010	365+36	36536.4	0	0.45	0.00	0	0	0	0	0
	366+00	36600.0	64	0.42	0.00	1	0	1	0	1
	367+00	36700.0	100	0.00	0.00	1	0	2	0	2
	368+00	36800.0	100	0.00	6.07	0	11	2	14	-12
	368+49	36849.4	49	0.00	3.91	0	9	2	25	-24
	369+00	36900.0	51	0.00	2.69	0	6	2	33	-31
	370+00	37000.0	100	0.01	0.00	0	5	2	39	-38
	371+00	37100.0	100	0.00	1.57	0	3	2	43	-41
	372+00	37200.0	100	0.03	0.00	0	3	2	47	-45
	373+00	37300.0	100	0.00	0.00	0	0	2	47	-45
	373+84	37383.7	84	0.00	0.10	0	0	2	47	-45
						2	38			

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
0010	379+00	37900.0	0	0.00	1.39	0	0	0	0	0
	380+00	38000.0	100	0.01	0.00	0	3	0	3	-3
	381+00	38100.0	100	0.00	0.00	0	0	0	3	-3
	382+00	38200.0	100	0.03	0.02	0	0	0	3	-3
	383+00	38300.0	100	0.00	0.00	0	0	0	3	-3
	384+00	38400.0	100	0.13	0.00	0	0	0	3	-3
	384+19	38418.7	19	0.26	0.00	0	0	1	3	-3
						1	3			

9

PROJECT NO: 5790-02-72

HWY: STH 171

COUNTY: CRAWFORD

EARTHWORK DATA

SHEET

E

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	395+86	39586.1	0	0.00	0.00	0	0	0	0	0
	396+00	39600.0	14	0.00	0.00	0	0	0	0	0
	397+00	39700.0	100	0.00	0.00	0	0	0	0	0
	398+00	39800.0	100	0.00	0.00	0	0	0	0	0
	399+00	39900.0	100	0.00	0.00	0	0	0	0	0
	399+82	39982.1	82	0.00	0.04	0	0	0	0	0
	405+02	40502.2	520	2.65	0.00	26	0	26	1	25
	406+00	40600.0	98	1.55	0.00	8	0	33	1	33
	407+00	40700.0	100	0.00	0.00	3	0	36	1	35
	408+00	40800.0	100	0.00	0.00	0	0	36	1	35
	408+03	40802.9	3	0.00	0.00	0	0	36	1	35
						36	0			

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	455+48	45548.0	0	0.00	0.00	0	0	0	0	0
	456+00	45600.0	52	0.00	0.00	0	0	0	0	0
	457+00	45700.0	100	0.00	0.00	0	0	0	0	0
	458+00	45800.0	100	0.35	0.00	1	0	1	0	1
	459+00	45900.0	100	0.00	0.00	1	0	1	0	1
	459+67	45967.0	67	0.00	2.62	0	3	1	4	-3
						1	3			

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	591+84	59184.4	0	6.86	0.00	0	0	0	0	0
	592+00	59200.0	16	7.87	0.00	4	0	4	0	4
	593+00	59300.0	100	0.00	0.00	15	0	19	0	19
	594+00	59400.0	100	0.00	0.00	0	0	19	0	19
	595+00	59500.0	100	0.00	3.10	0	6	19	7	12
	596+00	59600.0	100	0.29	0.00	1	6	19	14	5
	597+00	59700.0	100	3.40	0.00	7	0	26	14	12
	598+00	59800.0	100	0.00	0.12	6	0	32	15	18
	599+00	59900.0	100	0.47	0.00	1	0	33	15	18
	600+00	60000.0	100	0.41	0.00	2	0	35	15	20
	600+32	60031.8	32	0.31	0.00	0	0	35	15	21
						35	12			

PROJECT ID 5790-02-72

STH 171 'A'										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	9+32	931.9	0	140.72	0.53	0	0	0	0	0
	9+50	950.0	18	143.85	2.51	95	1	95	1	94
	9+75	975.0	25	140.93	5.62	132	4	227	6	221
	10+00	1000.0	25	126.08	0.00	124	3	351	9	342
	10+25	1025.0	25	121.93	0.00	115	0	466	9	456
	10+50	1050.0	25	106.96	0.87	106	0	572	10	562
	10+75	1075.0	25	104.40	1.22	98	1	669	11	659
	11+00	1100.0	25	102.29	1.63	96	1	765	13	753
	11+25	1125.0	25	106.08	2.17	96	2	862	15	847
	11+50	1150.0	25	100.44	1.45	96	2	957	17	940
	11+73	1172.5	23	97.69	1.85	83	1	1,040	19	1,021
	11+98	1198.0	26	18.75	0.41	55	1	1,095	20	1,075
	12+25	1225.0	27	13.03	0.47	16	0	1,111	20	1,090
	12+50	1250.0	25	11.76	0.87	11	1	1,122	21	1,101
	12+75	1275.0	25	14.35	0.13	12	0	1,134	22	1,112
	13+02	1302.4	27	8.92	16.81	12	9	1,146	33	1,113
	13+13	1313.3	11	10.10	10.92	4	6	1,150	40	1,110
						1150	32			

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	288+99	28898.8	0	9.30	9.83	0	0	0	0	0
	289+00	28900.0	1	27.73	1.80	1	0	1	0	1
	289+06	28906.0	6	55.88	18.59	9	2	10	3	7
	289+25	28925.0	19	90.03	21.39	51	14	61	21	41
	289+50	28950.0	25	111.46	4.29	93	12	155	36	119
	289+75	28975.0	25	122.40	1.39	108	3	263	39	224
	289+97	28997.5	22	126.84	1.82	104	1	367	41	326
						367	32			

PROJECT ID 5790-02-72

STH 131 'C'										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	391+23	39123.2	0	88.21	1.67	0	0	0	0	0
	391+25	39125.0	2	87.64	1.73	6	0	6	0	6
	391+50	39150.0	25	84.68	0.26	80	1	86	1	84
	391+75	39175.0	25	87.25	0.47	80	0	165	2	163
	392+00	39200.0	25	60.09	6.80	68	3	233	6	227
	392+25	39225.0	25	57.75	26.91	55	16	288	25	263
	392+36	39235.8	11	42.84	9.62	20	7	308	35	274
	392+50	39250.0	14	29.33	11.23	19	5	327	41	286
	392+75	39275.0	25	30.73	10.40	28	10	355	54	301
	392+85	39285.0	10	22.81	11.90	10	4	365	59	306
						365	47			

9

9

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	2+26	225.5	0	7.62	0.01	0	0	0	0	0
	3+00	300	75	7.57	0.00	21	0	21	0	21
	4+00	400	100	13.54	1.26	39	2	60	3	57
	4+50	450	50	10.39	1.41	22	2	82	6	76
						82	5			

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	84+60	8460.0	0	3.73	3.33	0	0	0	0	0
	85+00	8500.0	40	5.08	0.20	7	3	7	3	3
	86+00	8600.0	100	4.14	0.19	17	1	24	4	19
	87+00	8700.0	100	7.80	0.00	22	0	46	5	41
	88+00	8800.0	100	6.27	0.00	26	0	72	5	67
	89+00	8900.0	100	6.15	0.00	23	0	95	5	90
	90+00	9000.0	100	9.62	0.00	29	0	174	5	119
	90+91	9091.0	91	0.00	14.17	16	24	140	34	106
	91+00	9100.0	9	5.05	6.36	1	3	141	39	102
	92+00	9200.0	100	6.12	0.08	21	12	162	54	108
	93+00	9300.0	100	5.78	0.05	22	0	184	54	130
	94+00	9400.0	100	5.09	0.91	20	2	204	56	148
	95+00	9500.0	100	3.49	0.34	16	2	220	59	161
	95+14	9514.4	14	5.67	0.00	2	0	222	59	163
	96+00	9600.0	85	7.01	0.00	20	0	242	59	183
	97+00	9700.0	100	4.82	0.24	22	0	264	60	204
	98+00	9800.0	100	8.52	0.00	25	0	289	60	229
	98+60	9860.0	60	4.69	0.00	15	0	304	60	243
	99+00	9900.0	40	6.90	0.03	9	0	312	60	252
	100+00	10000.0	100	7.34	0.00	26	0	339	60	278
	101+00	10100.0	100	5.71	0.87	24	2	363	62	300
	102+00	10200.0	100	6.62	2.69	23	7	386	71	315
	103+00	10300.0	100	5.52	1.56	22	8	408	80	328
	104+00	10400.0	100	5.63	1.29	21	5	429	87	342
	104+85	10485.5	85	4.01	5.57	15	11	444	101	343
	105+00	10500.0	15	5.01	0.55	2	2	446	103	344
	106+00	10600.0	100	3.94	1.24	17	3	463	107	356
	107+00	10700.0	100	3.17	0.70	13	4	476	111	365
	107+52	10752.4	52	4.08	0.34	7	1	483	113	371
	108+00	10800.0	48	4.07	1.29	7	1	490	114	376
	109+00	10900.0	100	4.02	0.04	15	2	505	117	388
	110+00	11000.0	100	4.61	0.21	16	0	521	118	403
	111+00	11100.0	100	4.31	0.34	17	1	538	119	418
	112+00	11200.0	100	2.95	3.58	13	7	551	128	423
	113+00	11300.0	100	2.68	0.64	10	8	562	138	424
	114+00	11400.0	100	2.60	0.36	10	2	571	140	431
	115+00	11500.0	100	4.89	0.00	14	1	585	141	444
	116+00	11600.0	100	2.97	0.09	15	0	600	142	458
	117+00	11700.0	100	4.50	0.13	14	0	614	142	472
	117+04	11704.0	4	4.90	0.08	1	0	614	142	472
	117+29	11729.0	25	6.36	0.19	5	0	620	142	477
	117+54	11754.0	25	5.91	1.14	6	1	625	143	482
	118+00	11800.0	46	2.62	6.91	7	7	633	152	481
	119+00	11900.0	100	3.89	16.28	12	43	645	205	439
	120+00	12000.0	100	2.87	1.40	13	33	657	246	411
	120+69	12069.3	69	4.82	0.01	10	2	667	248	419
	120+94	12094.3	25	5.93	0.00	5	0	672	248	474
						672	199			

*EARTHWORK DATA ON THIS SHEET PROVIDED FOR INFORMATIONAL PURPOSES ONLY
 EARTHWORK PAID UNDER BID ITEM "GRADING, SHAPING, AND FINISHING FOR CONCRETE CURB AND GUTTER"

PROJECT NO: 5790-02-72

HWY: STH 171

COUNTY: CRAWFORD

EARTHWORK DATA

SHEET

E

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	120+94	12094.3	0	5.93	0.00	0	0	0	0	0
	121+00	12100.0	6	6.17	0.00	1	0	1	0	1
	121+19	12119.3	19	8.17	0.00	5	0	5	0	5
	121+50	12149.6	30	3.70	0.00	7	0	8	0	8
	122+00	12200.0	50	6.44	0.08	9	0	15	0	14
	123+00	12300.0	100	9.44	0.00	29	0	37	0	37
	124+00	12400.0	100	3.38	0.01	24	0	38	0	38
	125+00	12500.0	100	2.34	7.82	11	15	48	18	30
	125+78	12577.7	78	2.77	2.07	7	14	46	18	28
	126+00	12600.0	22	0.85	2.16	1	2	49	20	29
	126+03	12602.7	3	0.81	2.47	0	0	46	18	28
	126+28	12627.7	25	0.87	6.26	1	4	50	26	25
	127+00	12700.0	72	0.40	6.23	2	17	47	39	8
	128+00	12800.0	100	2.05	4.48	5	20	55	50	4
	129+00	12900.0	100	0.42	1.62	5	11	52	53	-1
	129+06	12906.5	6	0.34	2.74	0	1	52	54	-2
	129+32	12931.7	25	1.01	1.85	1	2	53	57	-4
	129+57	12957.0	25	3.38	2.24	2	2	55	59	-4
	129+65	12965.4	8	2.82	6.07	1	1	56	61	5
	130+00	13000.0	35	2.25	1.82	3	5	59	67	-8
						114	94			

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	130+00	13000.0	0	2.25	1.82	0	0	0	0	0
	131+00	13100.0	100	5.67	0.90	15	5	15	6	8
	132+00	13200.0	100	8.47	0.00	26	2	26	2	24
	133+00	13300.0	100	8.89	0.00	32	0	47	6	41
	134+00	13400.0	100	5.84	0.14	27	0	53	2	51
	135+00	13500.0	100	5.40	0.00	21	0	68	7	61
	136+00	13600.0	100	5.91	0.10	21	0	74	3	72
	137+00	13700.0	100	2.19	0.39	15	1	83	8	75
	137+96	13796.1	96	1.36	2.12	6	4	81	8	73
						163	13			

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	145+00	14500.0	0	9.68	0.00	0	0	0	0	0
	146+00	14600.0	100	11.21	0.00	39	0	39	0	39
	147+00	14700.0	100	10.61	0.00	40	0	79	0	79
	148+00	14800.0	100	12.47	0.00	43	0	122	0	122
	149+00	14900.0	100	10.44	0.00	42	0	164	0	164
	150+00	15000.0	100	6.19	0.01	31	0	195	0	195
	151+00	15100.0	100	7.59	0.00	26	0	221	0	221
	152+00	15200.0	100	9.08	0.00	31	0	251	0	251
	153+00	15300.0	100	5.22	11.88	26	22	278	28	250
	154+00	15400.0	100	5.75	4.39	20	30	298	65	233
	155+00	15500.0	100	6.04	0.90	22	10	320	77	243
	156+00	15600.0	100	6.23	0.00	23	2	343	80	263
	157+00	15700.0	100	12.48	0.00	35	0	377	80	298
	157+67	15766.8	67	25.41	0.00	47	0	424	80	345
	158+00	15800.0	33	7.95	0.00	21	0	445	80	365
	159+00	15900.0	100	9.80	0.00	33	0	478	80	398
	160+00	16000.0	100	22.54	0.00	60	0	538	80	458
	161+00	16100.0	100	6.86	0.00	54	0	592	80	512
	162+00	16200.0	100	5.57	0.29	23	1	615	80	535
	163+00	16300.0	100	5.60	0.20	21	1	636	81	554
	164+00	16400.0	100	4.95	1.29	20	3	655	85	570
	165+00	16500.0	100	5.39	1.11	19	4	674	90	584
	166+00	16600.0	100	4.78	0.32	19	3	693	94	600
	167+00	16700.0	100	4.77	1.13	18	3	711	97	614
	167+15	16745.1	45	6.53	0.00	9	1	720	98	622
	167+70	16770.0	25	5.13	0.09	5	0	726	98	628
	167+95	16795.0	25	4.67	1.47	5	1	730	99	631
	168+00	16800.0	5	4.66	1.95	1	0	731	100	632
	169+00	16900.0	100	2.37	3.26	13	10	744	112	633
	169+57	16957.4	57	3.62	13.18	6	17	751	133	617
	170+00	17000.0	43	4.64	1.07	7	11	757	148	610
	170+04	17003.6	4	4.19	1.04	1	0	758	148	610
	170+29	17028.6	25	4.07	0.73	4	1	761	149	613
	170+40	17039.7	11	4.33	0.61	2	0	763	149	614
	170+54	17053.6	14	4.30	0.55	2	0	765	149	616
	171+00	17100.0	46	5.10	0.32	8	1	773	150	623
	172+00	17200.0	100	5.02	1.32	19	3	792	154	638
	173+00	17300.0	100	3.74	2.63	16	7	808	163	645
	174+00	17400.0	100	3.49	6.42	13	17	822	184	638
	175+00	17500.0	100	5.36	1.72	16	15	838	203	635
	176+00	17600.0	100	6.14	0.03	21	3	860	207	652
	177+00	17700.0	100	6.35	0.27	23	1	883	208	675
	178+00	17800.0	100	6.37	0.83	24	2	906	210	696
	179+00	17900.0	100	7.33	0.00	25	2	932	212	719
	179+78	17978.2	78	5.16	0.00	18	0	950	212	737
	180+00	18000.0	22	6.52	0.31	5	0	954	212	742
	181+00	18100.0	100	6.67	0.00	24	1	979	213	766
						979	171			

*EARTHWORK DATA ON THIS SHEET PROVIDED FOR INFORMATIONAL PURPOSES ONLY
EARTHWORK PAID UNDER BID ITEM "GRADING, SHAPING, AND FINISHING FOR CONCRETE CURB AND GUTTER"

PROJECT NO: 5790-02-72

HWY: STH 171

COUNTY: CRAWFORD

EARTHWORK DATA

SHEET

E

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	181+00	18100.0	0	6.67	0.00	0	0	0	0	0
	182+00	18200.0	100	6.57	0.00	25	0	25	0	25
	183+00	18300.0	100	6.78	0.00	25	0	49	0	49
	184+00	18400.0	100	7.55	0.00	27	0	76	0	76
	185+00	18500.0	100	5.50	0.00	24	0	100	0	100
	185+80	18579.8	80	24.01	0.03	44	0	144	0	143
	186+00	18600.0	20	4.20	0.41	11	0	154	0	154
	187+00	18700.0	100	6.35	0.00	20	1	174	1	172
	188+00	18800.0	100	12.48	0.00	35	0	209	1	207
	189+00	18900.0	100	8.64	0.00	39	0	248	1	246
	190+00	19000.0	100	10.43	0.00	35	0	283	1	282
	191+00	19100.0	100	10.19	0.00	38	0	321	1	320
	191+95	19195.5	95	4.37	0.06	26	0	347	1	346
	192+00	19200.0	5	4.56	0.00	1	0	348	1	346
	193+00	19300.0	100	5.22	0.00	18	0	366	1	364
	194+00	19400.0	100	5.72	0.37	20	1	386	2	384
	195+00	19500.0	100	5.54	1.47	21	3	407	6	400
	196+00	19600.0	100	6.25	0.93	22	4	429	12	417
	196+38	19637.8	38	6.35	1.74	9	2	437	14	423
	197+50	19750.0	112	7.55	0.00	29	4	466	19	447
	198+00	19800.0	50	7.44	1.58	14	1	480	21	460
	199+00	19900.0	100	7.75	0.00	28	3	508	24	484
	200+00	20000.0	100	11.34	0.00	35	0	544	24	519
	201+00	20100.0	100	36.57	0.00	89	0	632	24	608
	201+17	20117.1	17	71.07	0.00	34	0	667	24	642
	202+00	20200.0	83	7.47	0.00	121	0	787	24	763
	203+00	20300.0	100	6.82	0.00	26	0	814	24	789
	204+00	20400.0	100	13.98	0.00	39	0	852	24	828
	204+47	20447.2	47	19.15	0.00	29	0	881	24	857
	204+72	20472.0	25	22.13	0.00	19	0	900	24	876
	204+97	20496.7	25	19.91	0.00	19	0	919	24	895
	205+00	20500.0	3	18.48	0.00	2	0	922	24	897
	206+00	20600.0	100	5.05	0.15	44	0	965	25	940
	207+00	20700.0	100	4.42	2.19	18	4	983	30	953
	207+15	20714.8	15	4.31	0.93	2	1	985	31	954
						985	25			

PROJECT ID 5790-02-72

STH 171										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
0010	250+70	25069.9	0	3.49	1.80	0	0	0	0	0
	251+00	25100.0	30	4.31	0.57	4	1	4	2	3
	252+00	25200.0	100	4.22	1.59	16	4	20	7	13
	252+72	25272.1	72	6.05	0.09	14	2	34	9	24
	257+17	25716.6	445	0.00	0.00	50	1	84	10	73
	257+42	25741.6	25	0.00	0.00	0	0	84	10	73
	257+67	25766.6	25	5.50	0.00	3	0	86	10	76
	258+64	25864.4	98	3.17	4.15	16	8	102	20	82
	259+00	25900.0	36	4.34	2.04	5	4	107	25	82
	260+00	26000.0	100	4.79	1.23	17	6	124	32	91
	260+26	26025.9	26	4.70	4.17	5	3	128	36	93
	260+51	26051.5	26	5.33	1.95	5	3	133	39	94
	260+77	26077.1	26	2.05	4.48	4	3	137	43	93
	261+00	26100.0	23	1.82	3.86	2	4	138	48	91
	261+05	26105.0	5	2.56	2.87	0	1	139	48	90
	262+00	26200.0	95	5.30	1.46	14	8	152	58	95
	263+00	26300.0	100	13.60	0.12	35	3	187	61	126
	263+26	26325.9	26	14.74	0.00	14	0	201	62	139
						201	49			

*EARTHWORK DATA ON THIS SHEET PROVIDED FOR INFORMATIONAL PURPOSES ONLY
EARTHWORK PAID UNDER BID ITEM "GRADING, SHAPING, AND FINISHING FOR CONCRETE CURB AND GUTTER"

PROJECT NO: 5790-02-72

HWY: STH 171

COUNTY: CRAWFORD

EARTHWORK DATA

SHEET

E

PROJECT ID 5790-02-72

SIH 1/1

CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)			
				CUT	FILL	CUT	FILL	CUT	EXPANDED		MASS ORDINATE
									1.00	1.25	
0010	321+17	32117.4	0	1.27	0.05	0	0	0	0	0	
	321+42	32141.9	25	7.18	1.07	4	1	4	1	3	
	321+66	32166.5	25	8.81	1.71	7	1	11	2	9	
	322+00	32200.0	34	5.91	0.01	9	1	20	4	17	
	323+00	32300.0	100	4.90	0.61	20	1	40	5	35	
	324+00	32400.0	100	4.19	1.01	17	3	57	9	48	
	325+00	32500.0	100	4.95	2.09	17	6	74	16	58	
	326+00	32600.0	100	5.03	10.25	18	23	93	44	48	
	326+06	32606.4	6	3.36	5.77	1	2	93	47	47	
	326+32	32631.9	26	4.91	7.55	4	6	97	55	43	
	326+55	32655.4	23	5.92	1.81	5	4	102	60	42	
	327+00	32700.0	45	6.08	0.18	10	2	112	62	50	
	328+00	32800.0	100	5.11	5.41	21	10	133	75	58	
	329+00	32900.0	100	6.46	0.49	21	11	154	88	66	
	330+00	33000.0	100	6.41	1.84	24	4	178	94	84	
	331+00	33100.0	100	5.36	0.37	22	4	200	99	101	
	332+00	33200.0	100	5.69	1.71	20	4	220	104	116	
	333+00	33300.0	100	6.24	0.25	22	4	242	108	134	
	334+00	33400.0	100	6.35	1.23	23	3	266	112	154	
	335+00	33500.0	100	7.50	1.19	26	4	291	117	174	
	335+68	33568.1	68	6.88	0.13	18	2	309	119	190	
	336+00	33600.0	37	5.38	0.77	7	0	317	120	197	
	337+00	33700.0	100	6.69	1.61	22	3	339	124	215	
	338+00	33800.0	100	6.19	0.15	24	3	363	128	235	
	339+00	33900.0	100	6.77	0.27	24	1	387	129	258	
	340+00	34000.0	100	6.16	0.15	24	1	411	130	281	
	341+00	34100.0	100	6.10	0.00	23	0	434	130	303	
	342+00	34200.0	100	7.12	0.26	24	0	458	131	327	
	343+00	34300.0	100	6.73	0.09	26	1	484	132	352	
	344+00	34400.0	100	6.77	0.60	25	1	509	133	375	
	345+00	34500.0	100	6.79	0.36	25	2	534	136	398	
	345+04	34503.7	4	10.80	0.32	1	0	535	136	399	
	346+00	34600.0	96	4.80	0.07	28	1	563	136	426	
	347+00	34700.0	100	3.32	1.52	15	3	578	140	438	
	348+00	34800.0	100	5.85	4.05	17	10	595	153	442	
	349+00	34900.0	100	5.38	1.92	21	11	616	167	449	
	350+00	35000.0	100	6.42	3.14	22	9	637	179	459	
	351+00	35100.0	100	6.30	0.12	24	6	661	186	475	
	352+00	35200.0	100	5.68	0.21	22	1	683	187	496	
	352+89	35289.5	89	5.47	1.15	18	2	702	190	512	
						702	152				

CATEGORY	STATION	REAL STATION	DISTANCE	CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
0010	365+36	36536.4	0	9.52	0.00	0	0	0	0	0
	366+00	36600.0	64	19.20	0.00	34	0	34	0	34
	367+00	36700.0	100	7.43	0.01	49	0	83	0	83
	368+00	36800.0	100	3.63	3.51	20	7	104	8	95
	368+49	36849.4	49	2.57	7.86	6	10	109	21	88
	369+00	36900.0	51	3.82	3.28	6	10	115	34	81
	370+00	37000.0	100	4.44	1.64	15	9	131	46	85
	371+00	37100.0	100	5.18	1.23	18	5	148	52	96
	372+00	37200.0	100	6.90	0.09	22	2	171	55	115
	373+00	37300.0	100	5.73	0.45	23	1	194	57	138
	373+84	37383.7	84	9.93	0.88	24	2	218	59	159
						218	47			

CATEGORY	STATION	REAL STATION	DISTANCE	CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
0010	379+00	37900.0	0	4.86	1.63	0	0	0	0	0
	380+00	38000.0	100	5.76	0.58	20	4	20	5	15
	381+00	38100.0	100	6.56	0.00	23	1	42	6	36
	382+00	38200.0	100	7.83	0.00	27	0	69	6	63
	383+00	38300.0	100	5.16	0.65	24	1	93	8	85
	384+00	38400.0	100	4.32	0.91	18	3	111	12	99
	384+19	38418.7	19	4.91	0.65	3	1	114	12	102
						114	10			

PROJECT ID 5790-02-72

CATEGORY	STATION	REAL STATION	DISTANCE	CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
0010	395+86	39586.1	0	5.34	0.13	0	0	0	0	0
	396+00	39600.0	14	5.31	0.14	3	0	3	0	3
	397+00	39700.0	100	5.30	0.12	20	0	22	1	22
	398+00	39800.0	100	7.48	0.39	14	1	37	2	35
	399+00	39900.0	100	2.39	0.28	9	1	46	3	42
	399+82	39982.1	82	5.77	1.54	12	3	58	7	51
	405+02	40502.2	520	4.16	2.54	96	39	154	56	98
	406+00	40600.0	98	7.33	0.00	21	5	175	62	113
	407+00	40700.0	100	2.68	1.89	19	4	193	66	127
	408+00	40800.0	100	1.40	7.30	8	17	201	87	113
	408+03	40802.9	3	1.52	10.06	0	1	201	89	112
						201	71			

PROJECT ID 5790 02 72

*EARTHWORK DATA ON THIS SHEET PROVIDED FOR INFORMATIONAL PURPOSES ONLY
EARTHWORK PAID UNDER BID ITEM "GRADING, SHAPING, AND FINISHING FOR CONCRETE CURB AND GUTTER

PROJECT NO: 5790-02-72

HWY: STH 171

COUNTY: CRAWFORD

EARTHWORK DATA

SHEET

E

CATEGORY	STATION	REAL STATION	DISTANCE	CUT		FILL		CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
				CUT	FILL	CUT	FILL			
0010	455+48	45548.0	0	5.27	1.43	0	0	0	0	0
	456+00	45600.0	52	6.02	2.81	11	4	11	5	6
	457+00	45700.0	100	5.57	1.27	21	8	32	15	18
	458+00	45800.0	100	5.34	1.60	20	5	53	21	31
	459+00	45900.0	100	5.65	1.18	20	5	73	28	45
	459+67	45967.0	67	5.42	0.14	14	2	87	30	57
					87	24				

PROJECT ID 5790-02-72

CATEGORY	STATION	REAL STATION	DISTANCE	CUT		FILL		CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
				CUT	FILL	CUT	FILL			
0010	464+85	46485.4	0	6.05	0.02	0	0	0	0	0
	465+00	46500.0	15	5.94	0.73	3	0	3	0	3
	466+00	46600.0	100	5.06	0.86	20	3	24	4	20
	467+00	46700.0	100	5.31	1.33	19	4	43	9	34
	467+40	46740.4	40	2.55	5.56	6	5	49	15	33
	468+00	46800.0	60	2.31	2.25	5	9	54	26	28
	469+00	46900.0	100	5.26	0.19	14	5	68	32	36
	470+00	47000.0	100	3.89	4.03	17	8	85	42	43
	470+24	47024.0	24	3.20	1.78	3	3	88	45	43
					88	36				

PROJECT ID 5790-02-72

CATEGORY	STATION	REAL STATION	DISTANCE	CUT		FILL		CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
				CUT	FILL	CUT	FILL			
0010	503+19	50319.0	0	5.01	16.22	0	0	0	0	0
	504+00	50400.0	81	6.45	12.68	17	43	17	54	-37
	505+00	50500.0	100	8.16	5.56	27	34	44	96	-52
	506+00	50600.0	100	10.79	2.75	35	15	79	116	-36
	507+00	50700.0	100	11.86	1.48	42	8	121	125	-4
	508+00	50800.0	100	9.10	3.20	39	9	160	136	24
	508+30	50829.5	30	10.74	1.11	11	2	171	139	32
					171	111				

CATEGORY	STATION	REAL STATION	DISTANCE	CUT		FILL		CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE	
				CUT	FILL	CUT	FILL				
0010	591+84	59184.4	0	2.78	3.80	0	0	0	0	0	
	592+00	59200.0	16	2.65	5.29	2	3	2	3	-2	
	593+00	59300.0	100	5.23	1.23	15	12	16	18	-2	
	594+00	59400.0	100	4.60	1.41	18	5	34	24	10	
	595+00	59500.0	100	4.64	1.25	17	5	51	31	21	
	596+00	59600.0	100	6.21	0.02	20	2	72	34	38	
	597+00	59700.0	100	5.45	0.16	22	0	93	34	59	
	598+00	59800.0	100	6.55	0.36	22	1	115	35	80	
	599+00	59900.0	100	4.70	2.46	21	5	136	42	94	
	600+00	60000.0	100	4.08	0.86	16	6	152	49	103	
	600+32	60031.8	32	6.79	0.00	6	1	159	50	109	
					159	40					

*EARTHWORK DATA ON THIS SHEET PROVIDED FOR INFORMATIONAL PURPOSES ONLY
EARTHWORK PAID UNDER BID ITEM "GRADING, SHAPING, AND FINISHING FOR CONCRETE CURB AND GUTTER"

PROJECT NO: 5790-02-72

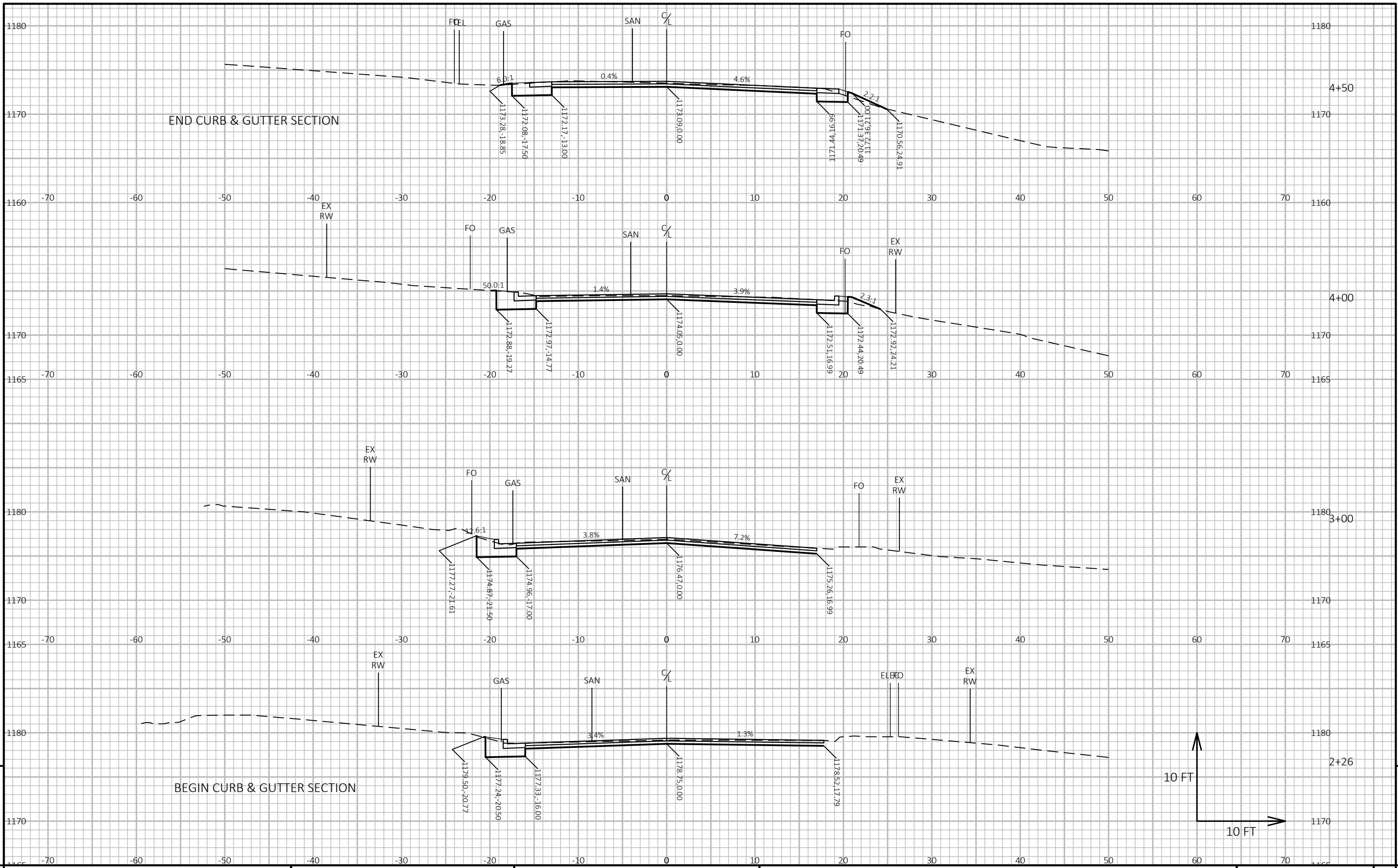
HWY: STH 171

COUNTY: CRAWFORD

EARTHWORK DATA

SHEET

E



9

9

PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

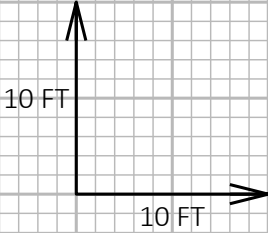
FILE NAME: \\SP-PZ1.SEHINC.COM\PROJECTS\UJ\W\WITSW\162034\5-FINAL-DSGN\51-DRAWINGS\10-CIVIL\C3D\57900202\SHEETS\PLAN\090101_XS.DWG PLOT DATE: 7/19/2024 5:11 AM PLOT BY: SAVANNAH STEHN PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

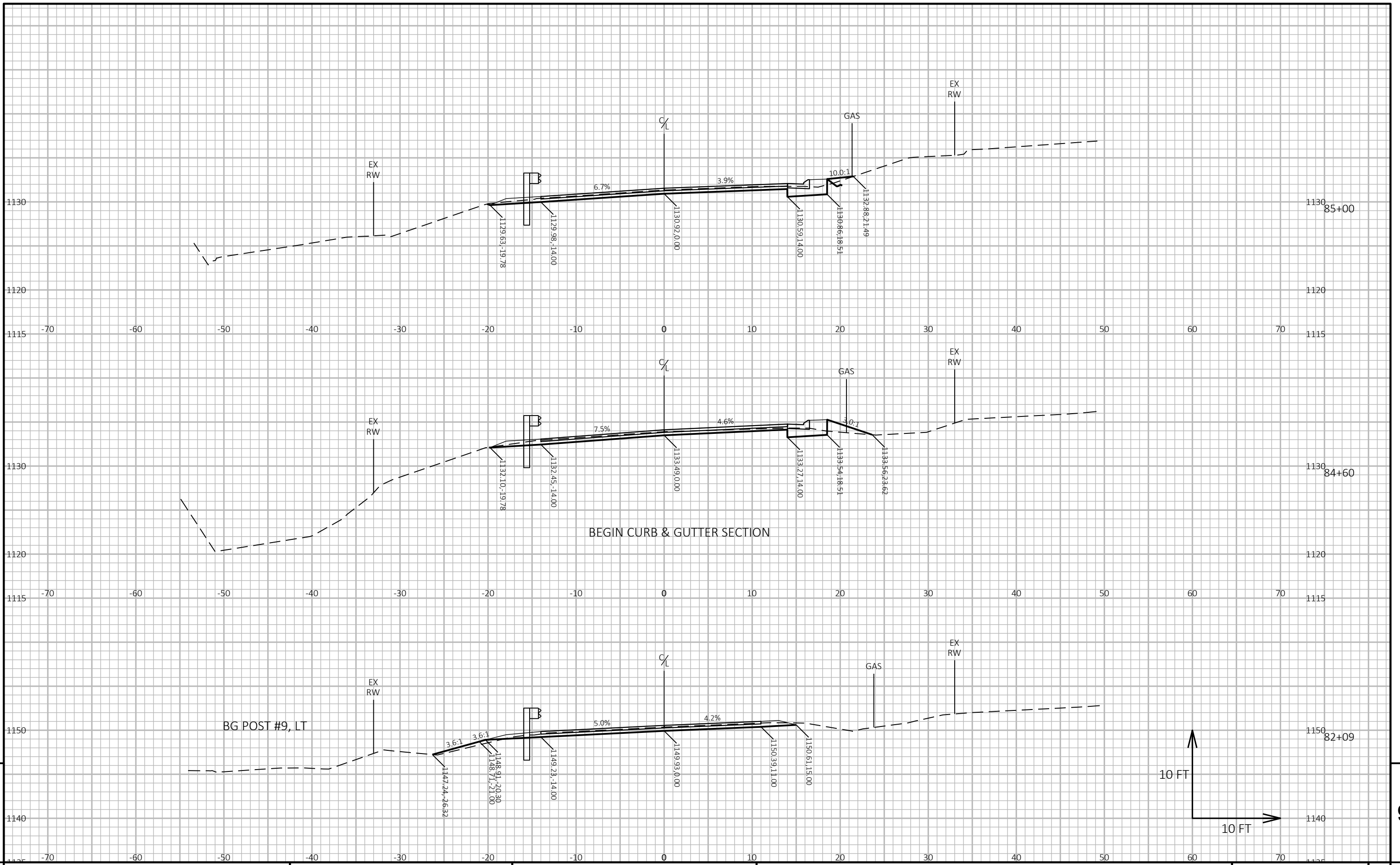
LAYOUT NAME - SG1-1



9

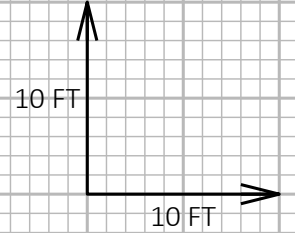
9

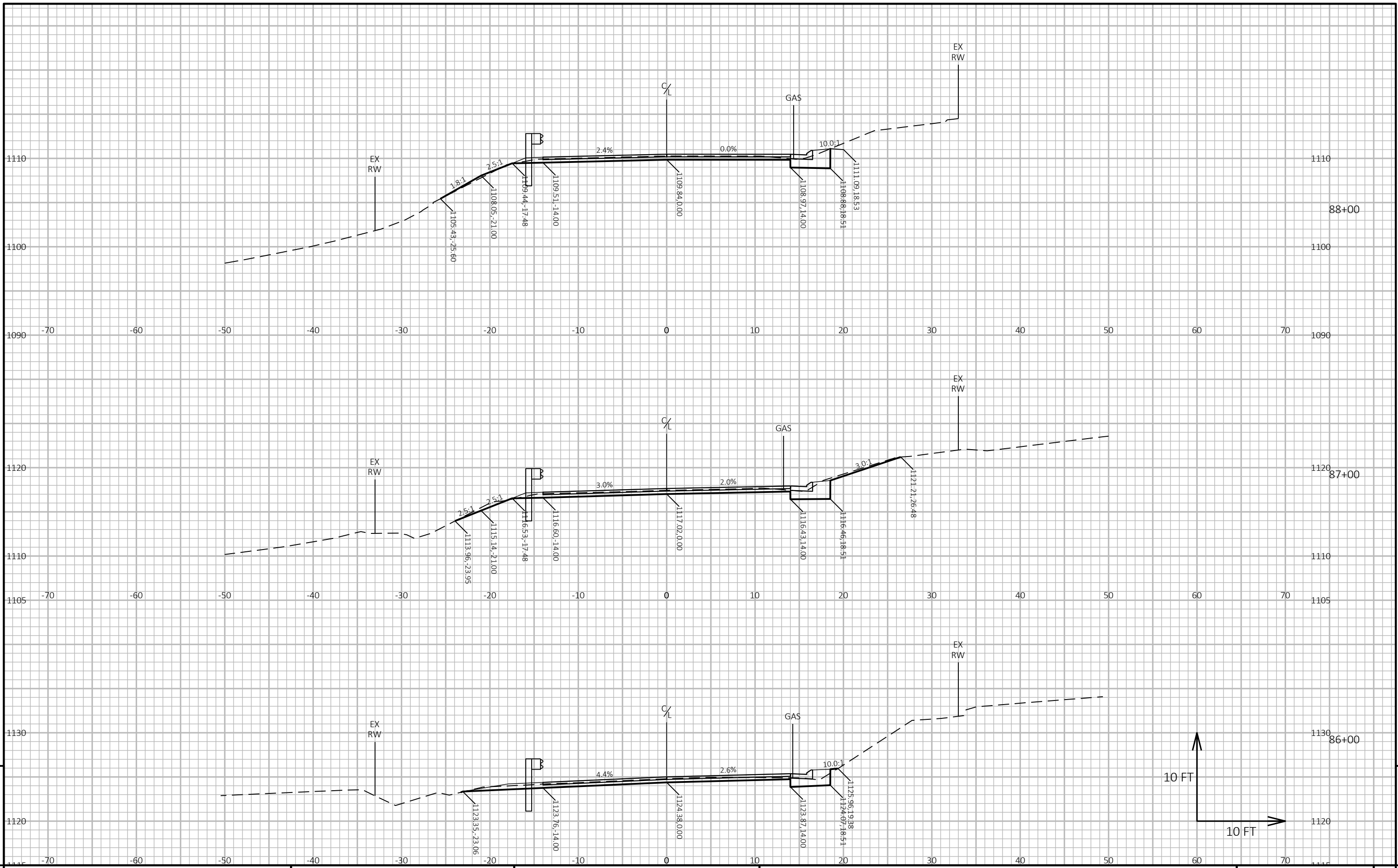




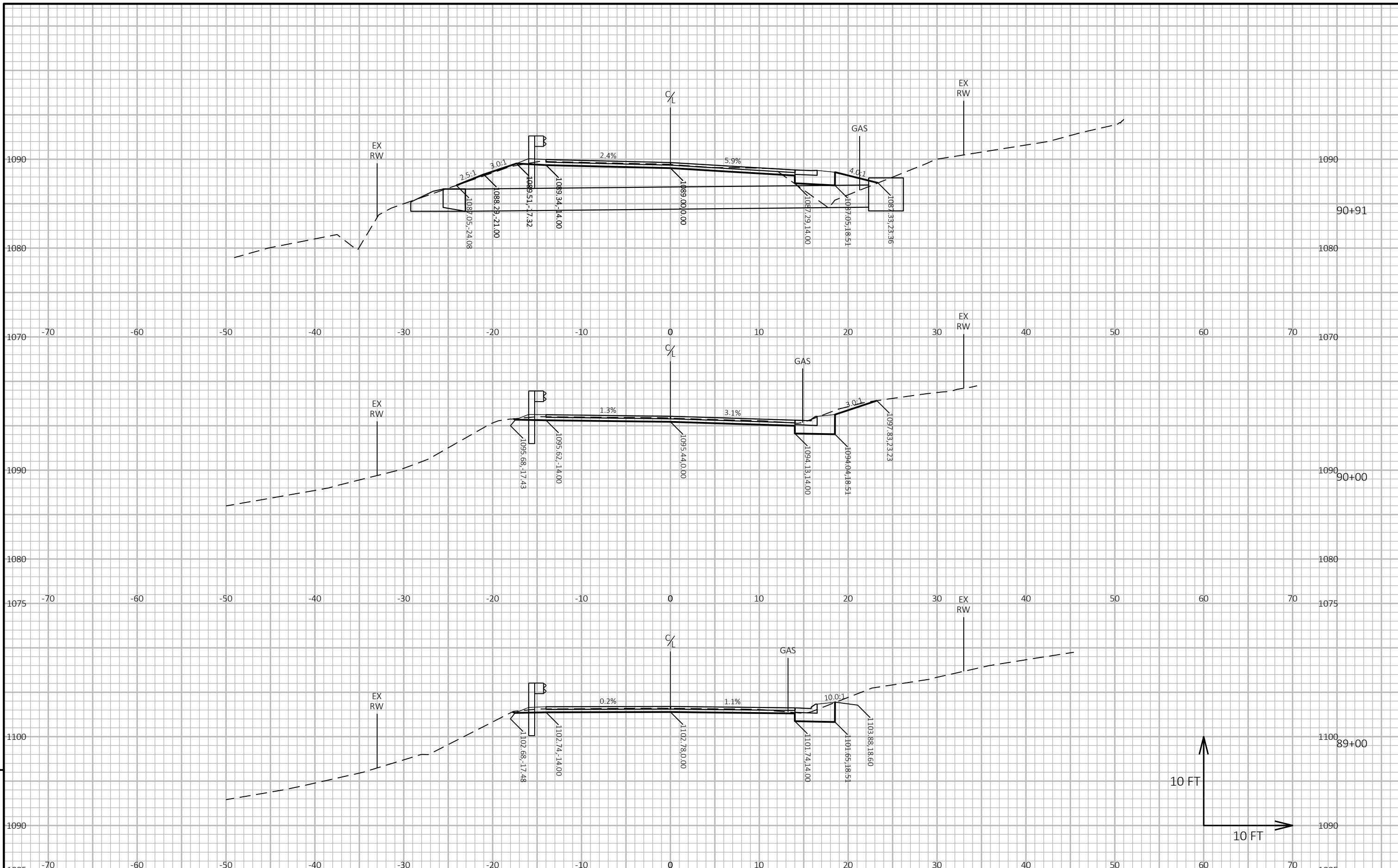
BEGIN CURB & GUTTER SECTION

BG POST #9, LT





PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



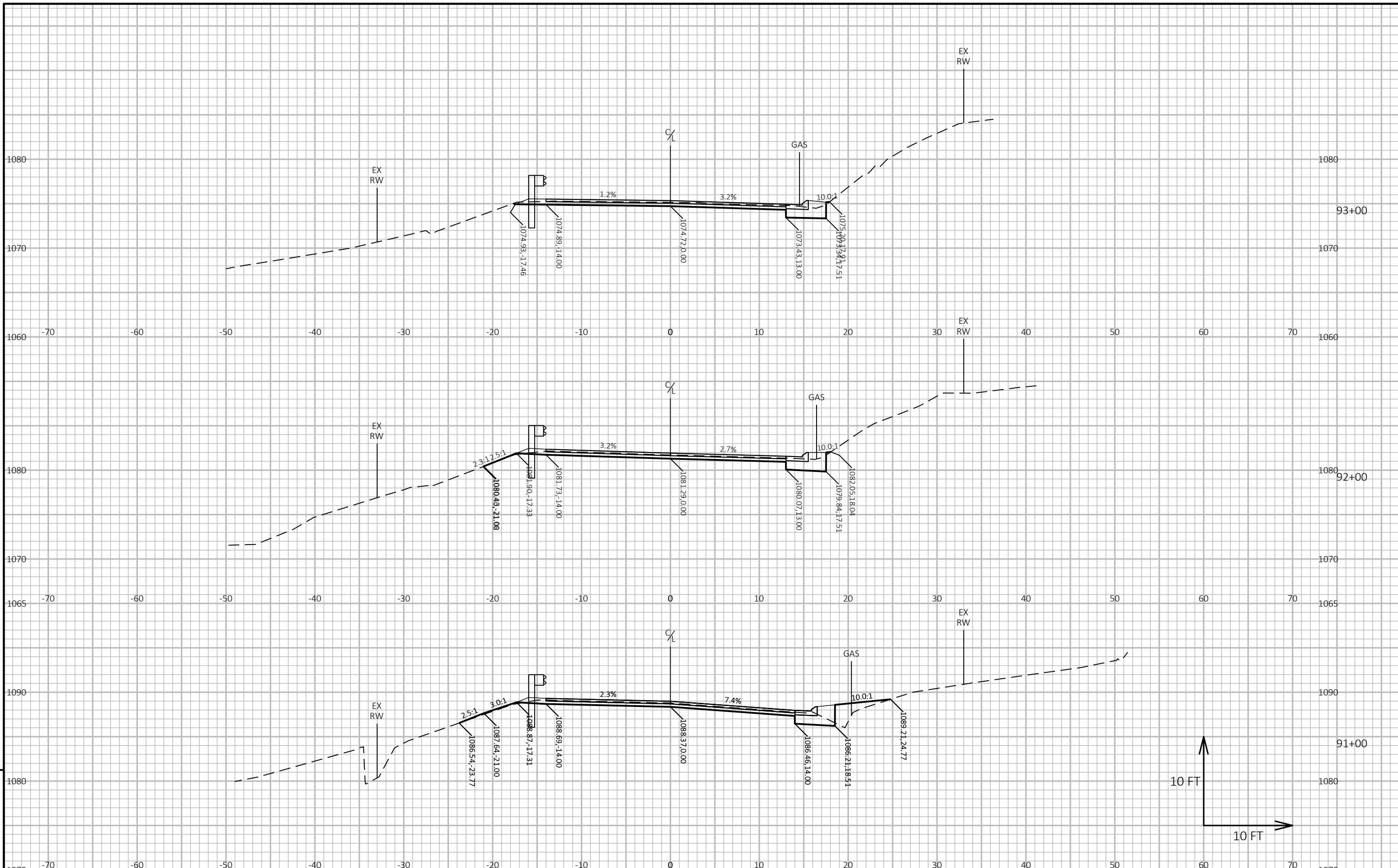
9

9

PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

FILE NAME: \\SP-PZ1.SEHINC.COM\PROJECTS\UZ\W\WITSW\162034\5-FINAL-DSGN\51-DRAWINGS\10-CIVIL\C3D\57900202\SHEETS\PLAN\090101_XS.DWG PLOT DATE: 7/19/2024 5:12 AM PLOT BY: SAVANNAH STEHN PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - SG1-7



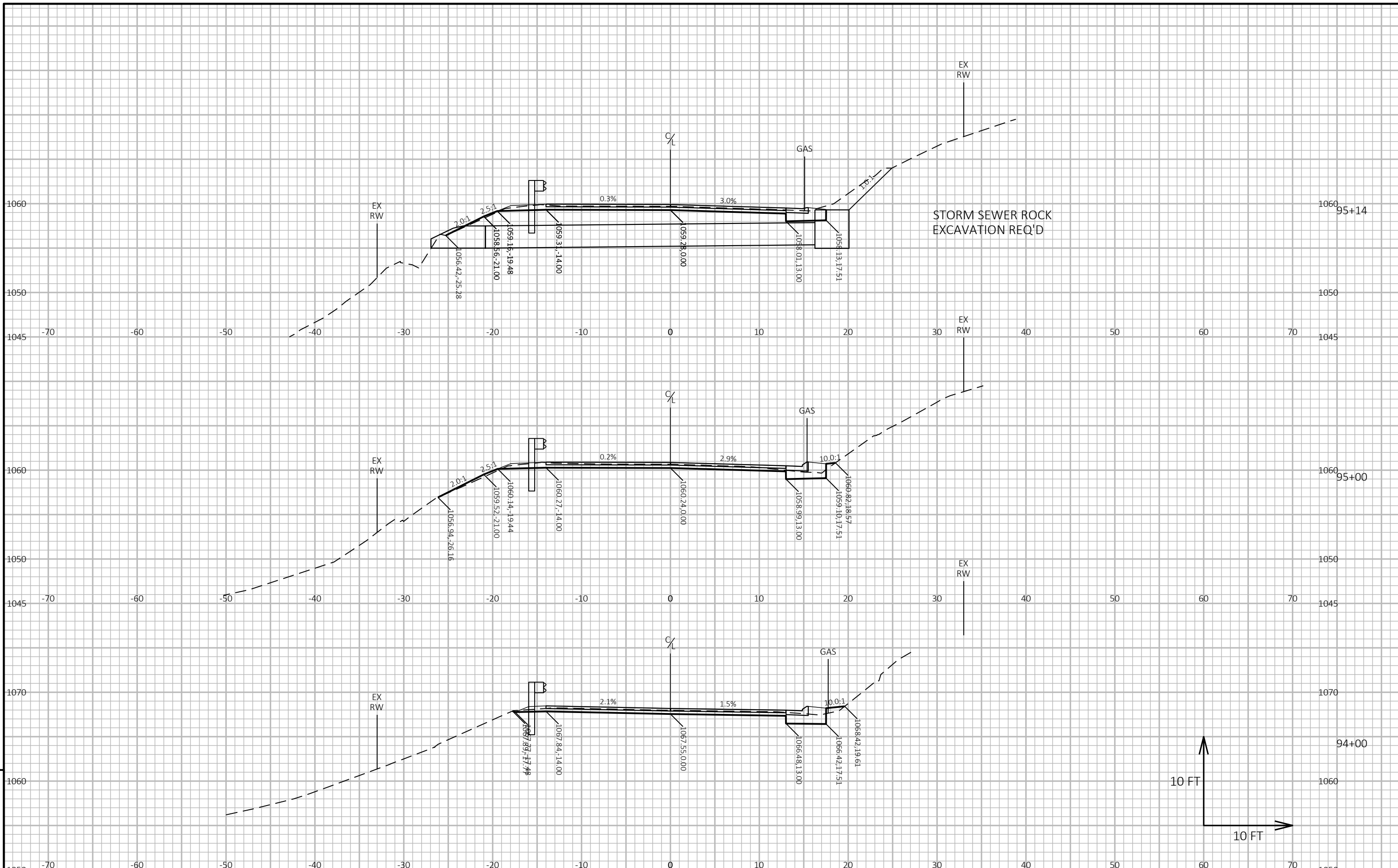
9

9

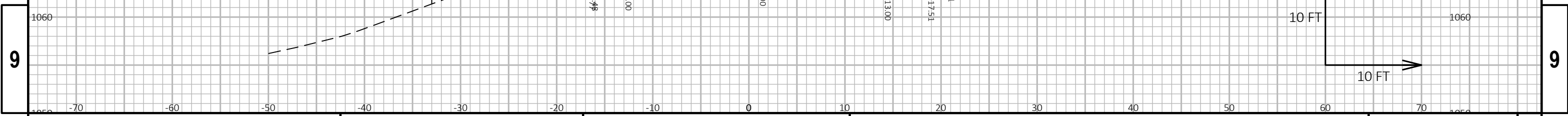
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

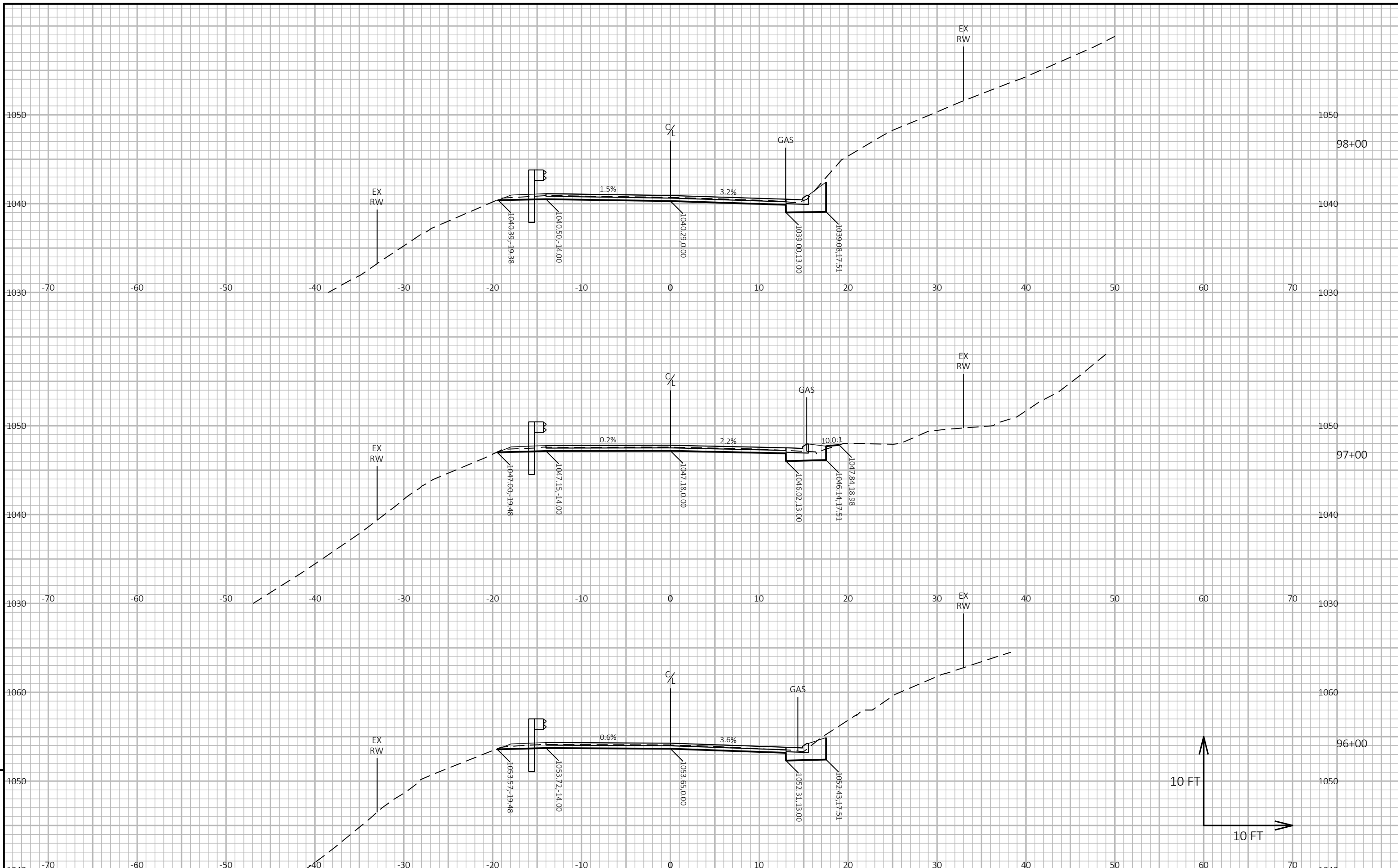
FILE NAME: \\SP-PZ1.SEHINC.COM\PROJECTS\UZ\W\WITSW\162034\5-FINAL-DSGN\51-DRAWINGS\10-CIVIL\C3D\57900202\SHEETS\PLAN\090101_XS.DWG PLOT DATE: 7/19/2024 5:13 AM PLOT BY: SAVANNAH STEHN PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - SG1-8



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E





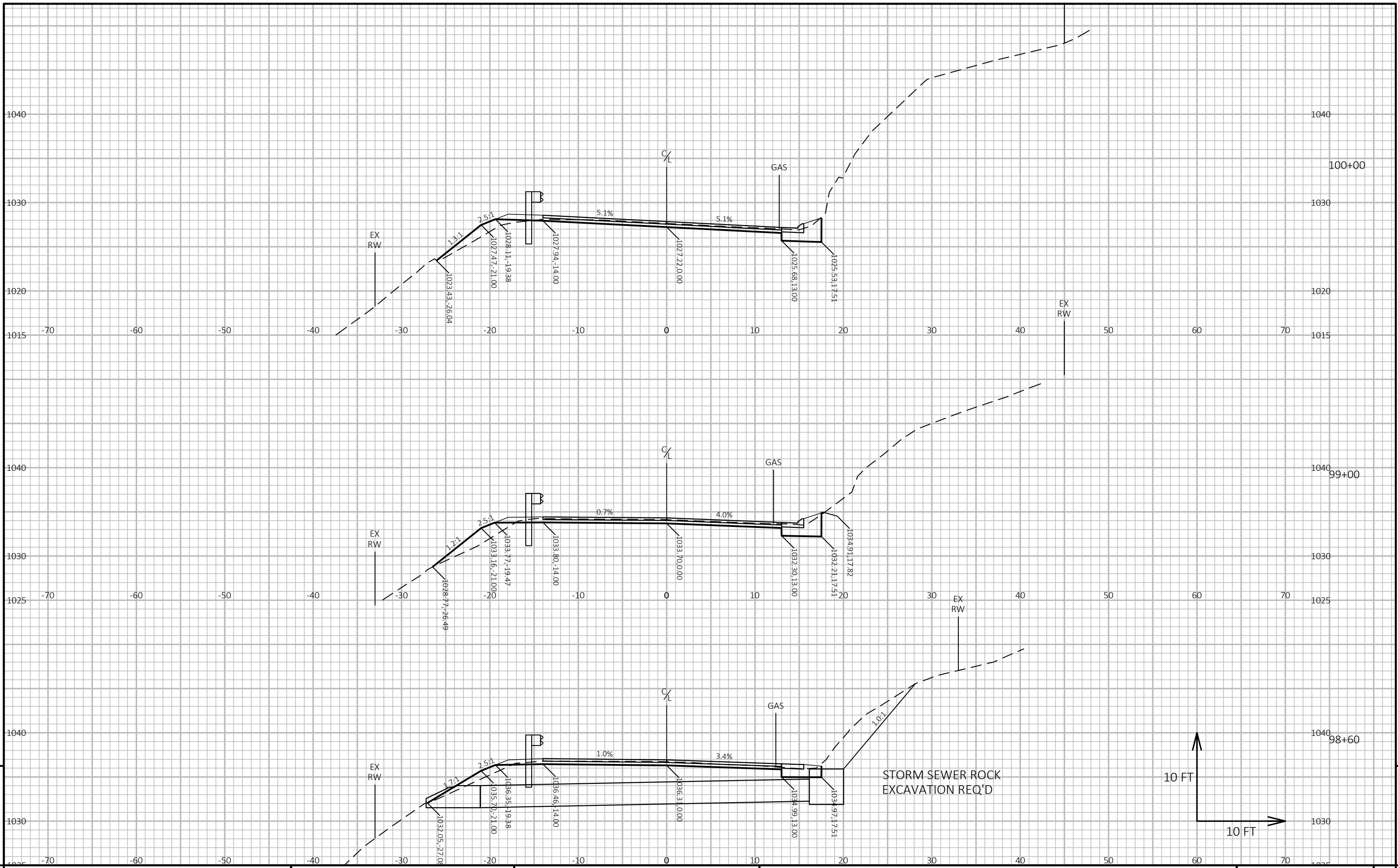
9

9

PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

FILE NAME: \\SP-PZ1.SEHINC.COM\PROJECTS\UZ\W\WITSW\162034\5-FINAL-DSGN\51-DRAWINGS\10-CIVIL\C3D\57900202\SHEETS\PLAN\090101_XS.DWG PLOT DATE: 7/19/2024 5:13 AM PLOT BY: SAVANNAH STEHN PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - SG1-10



PROJECT NO: 5790-02-72

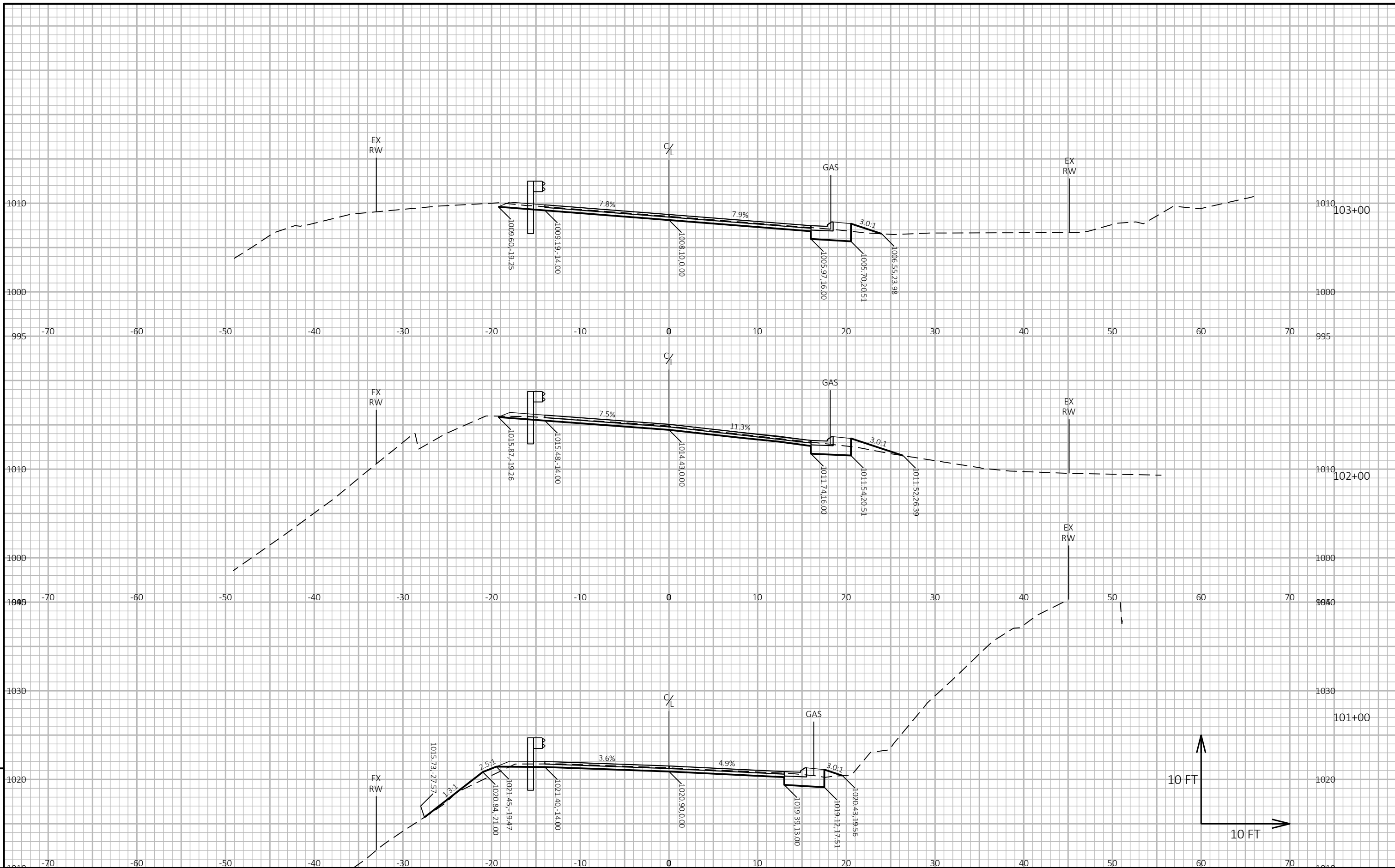
HWY: STH 171

COUNTY: CRAWFORD

CROSS SECTIONS: STH 171

SHEET

E



PROJECT NO: 5790-02-72

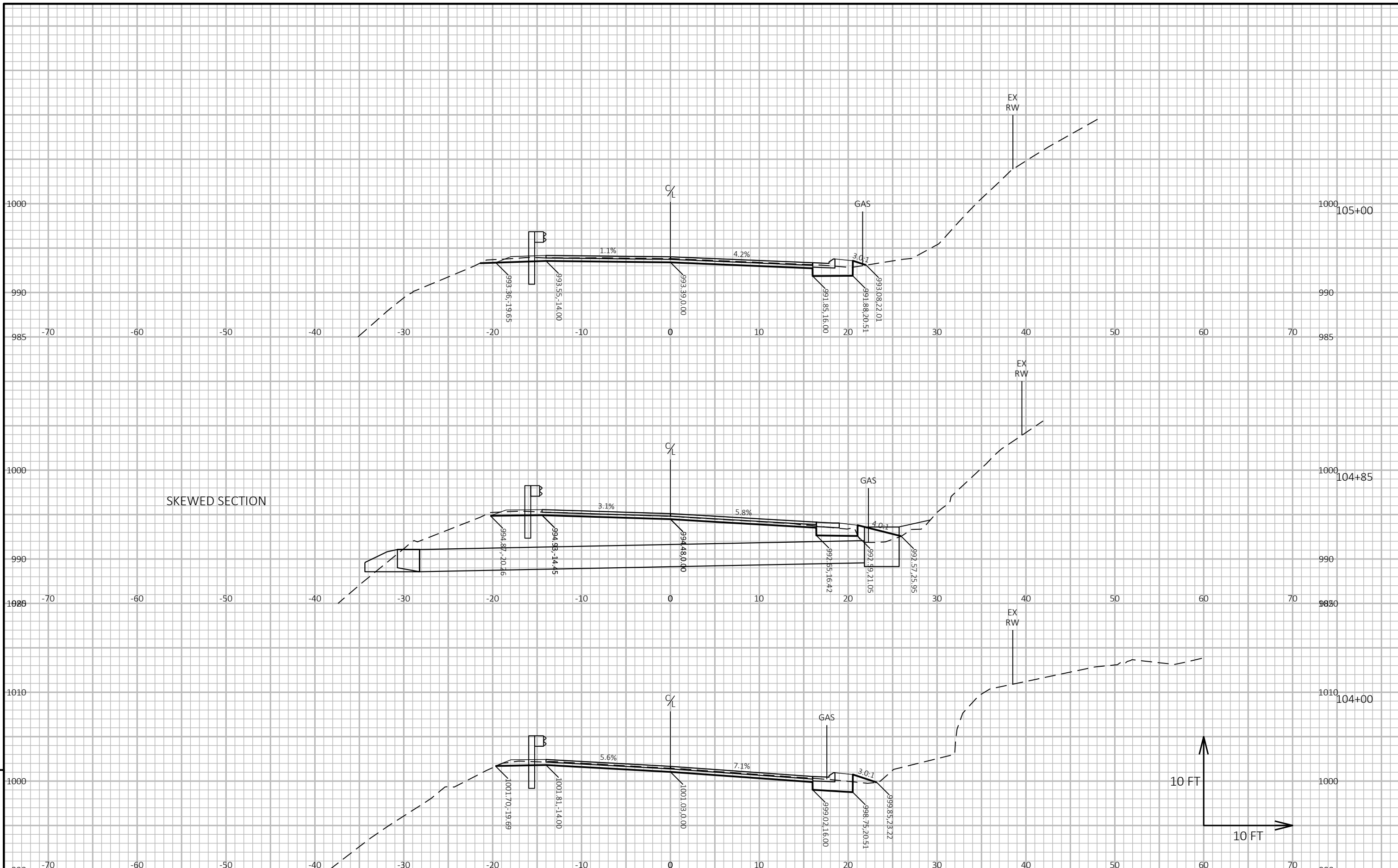
HWY: STH 171

COUNTY: CRAWFORD

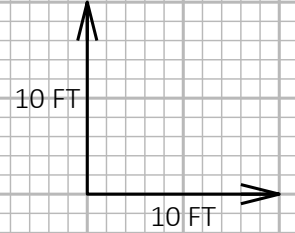
CROSS SECTIONS: STH 171

SHEET

E

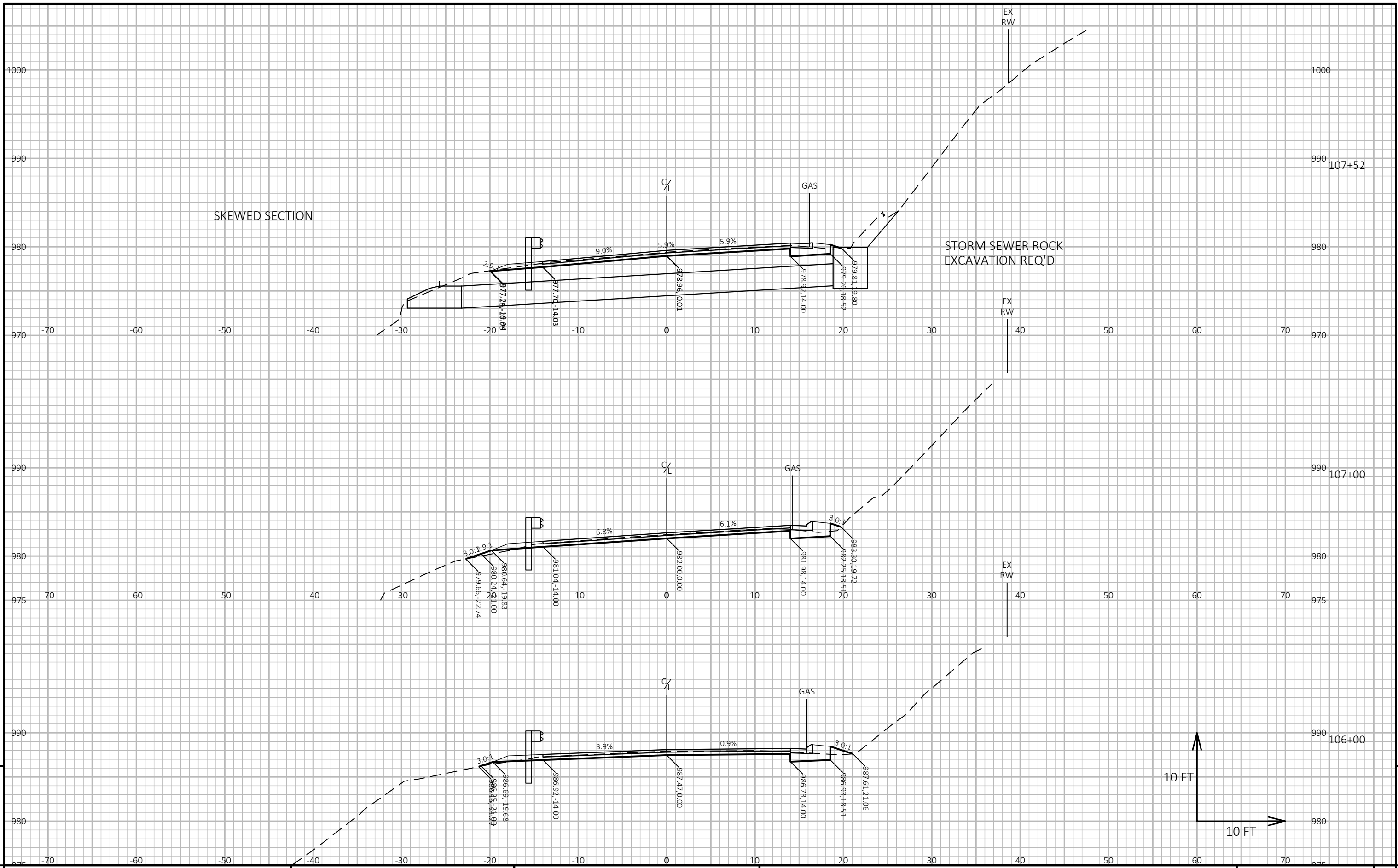


SKEWED SECTION

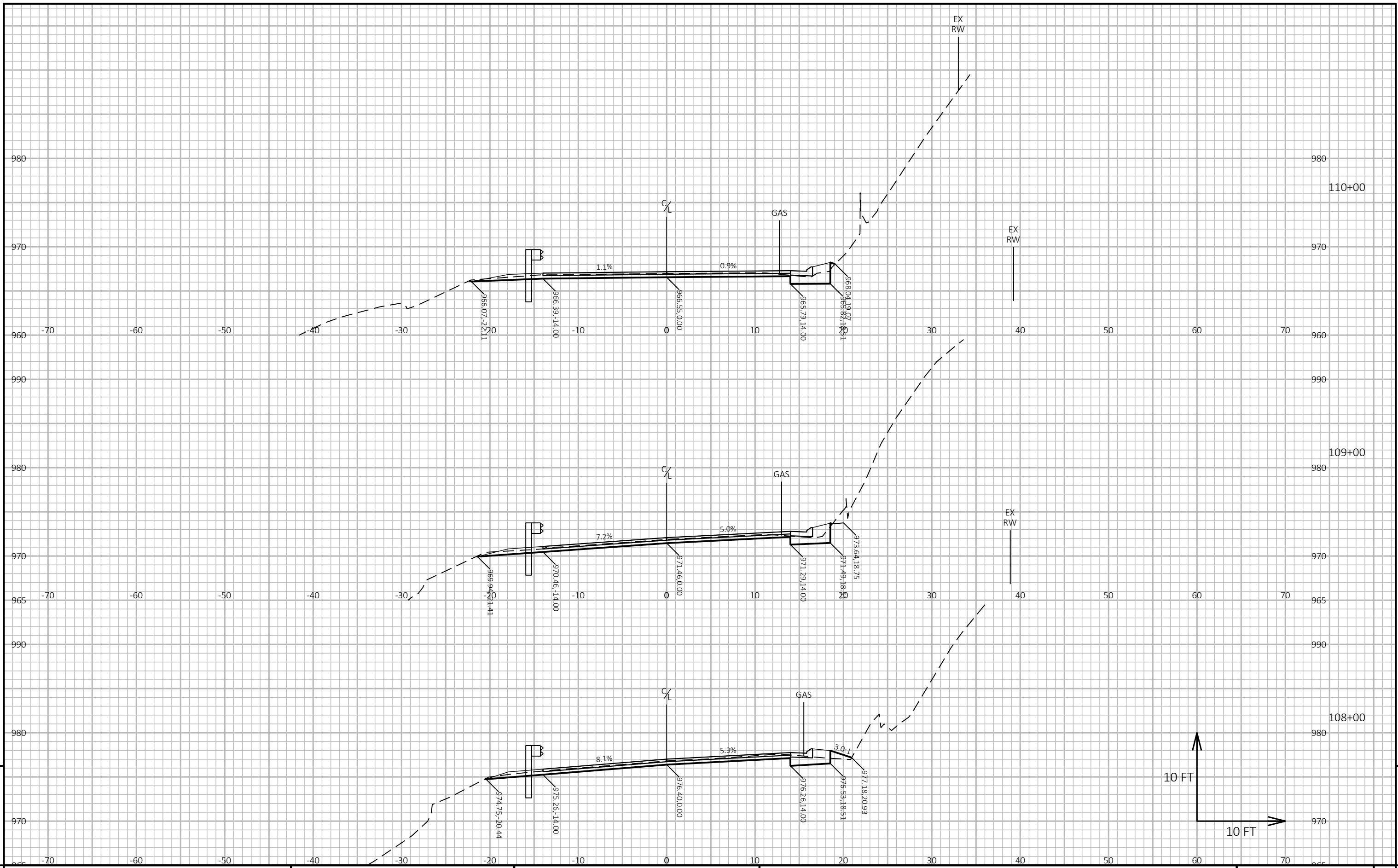


9

9



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



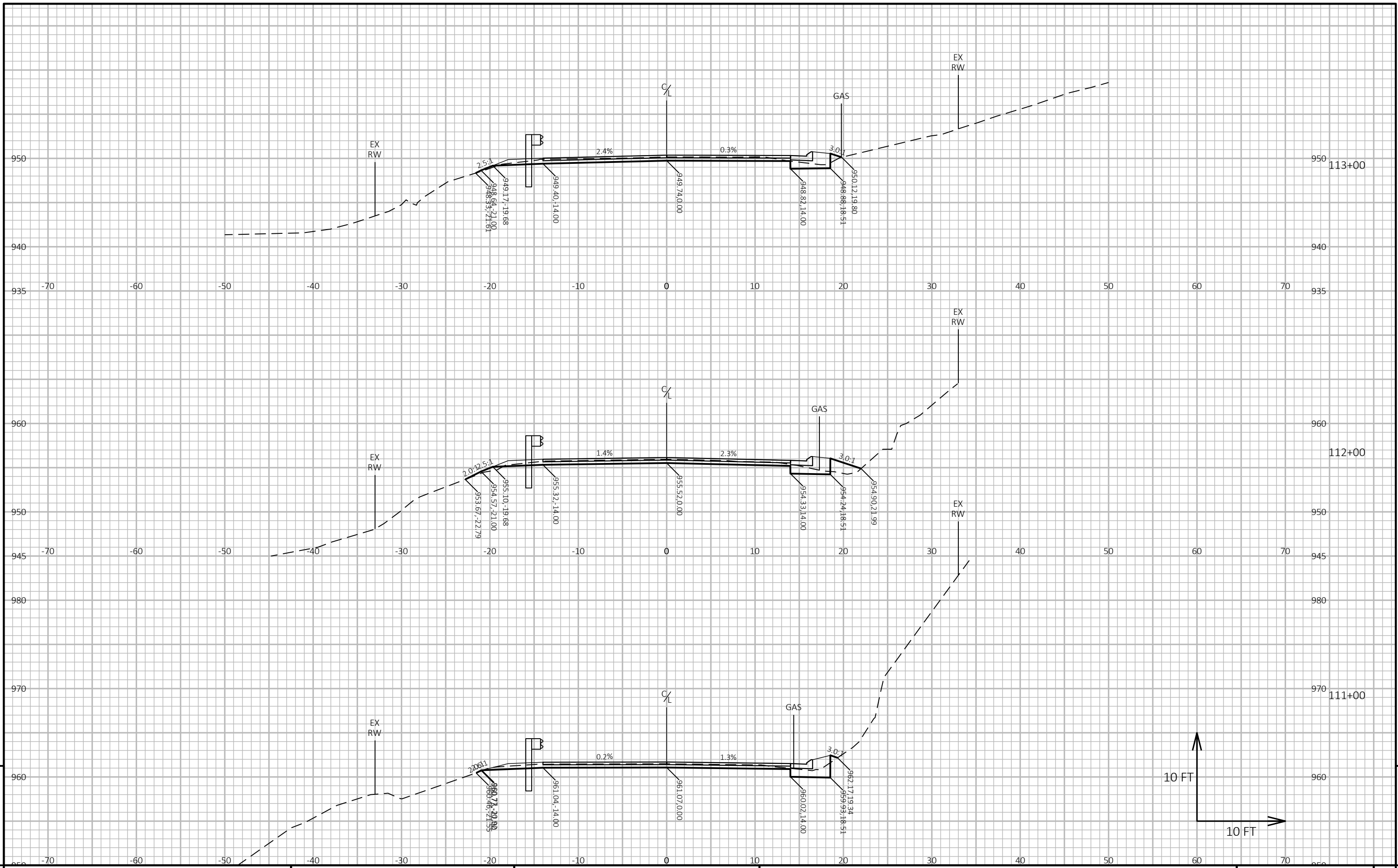
9

9

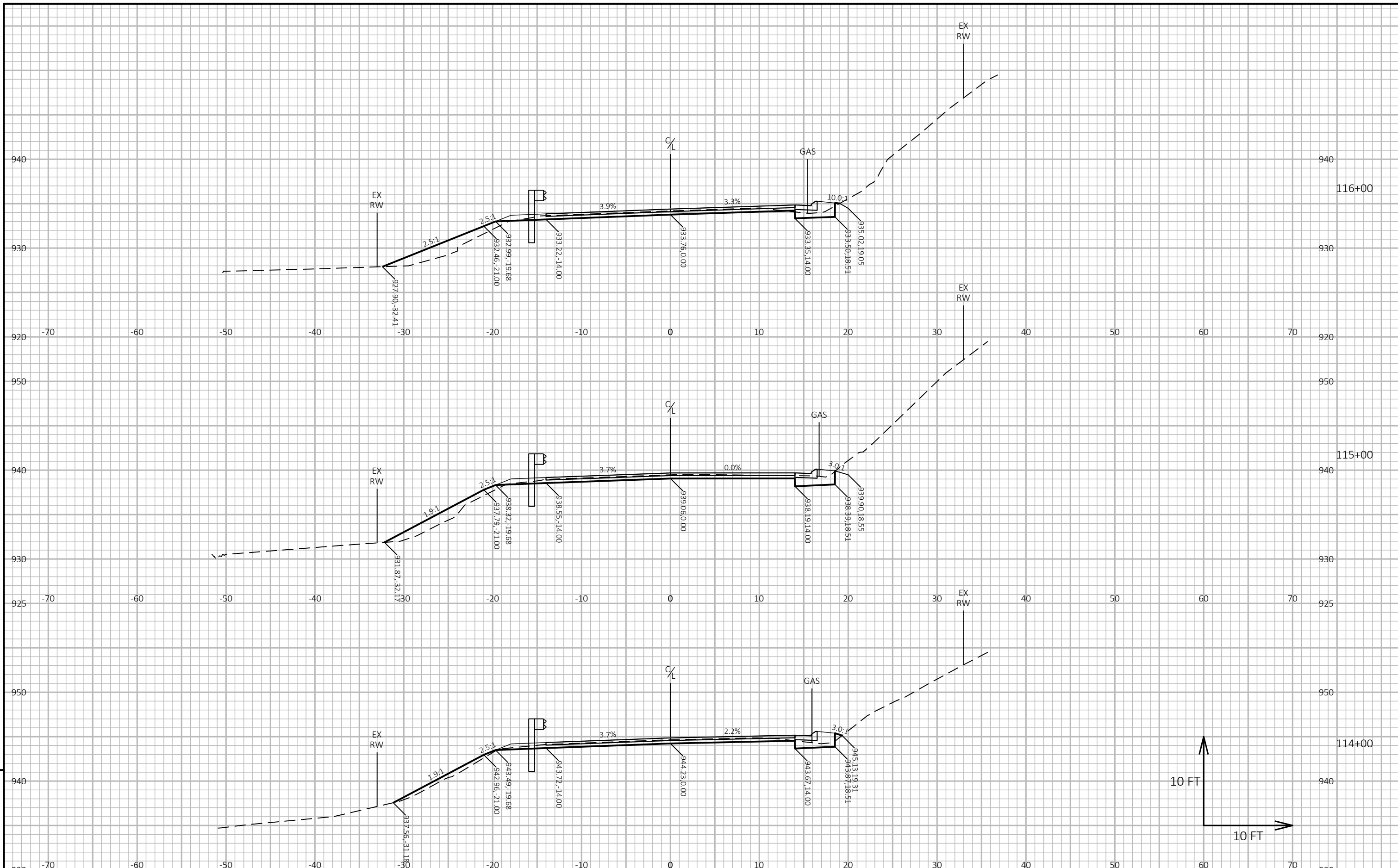
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

FILE NAME: \\SP-PZ1.SEHINC.COM\PROJECTS\UZ\W\WITSW\162034\5-FINAL-DSGN\51-DRAWINGS\10-CIVIL\C3D\57900202\SHEETS\PLAN\090101_XS.DWG PLOT DATE: 7/19/2024 5:15 AM PLOT BY: SAVANNAH STEHN PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - SG1-15



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



PROJECT NO: 5790-02-72

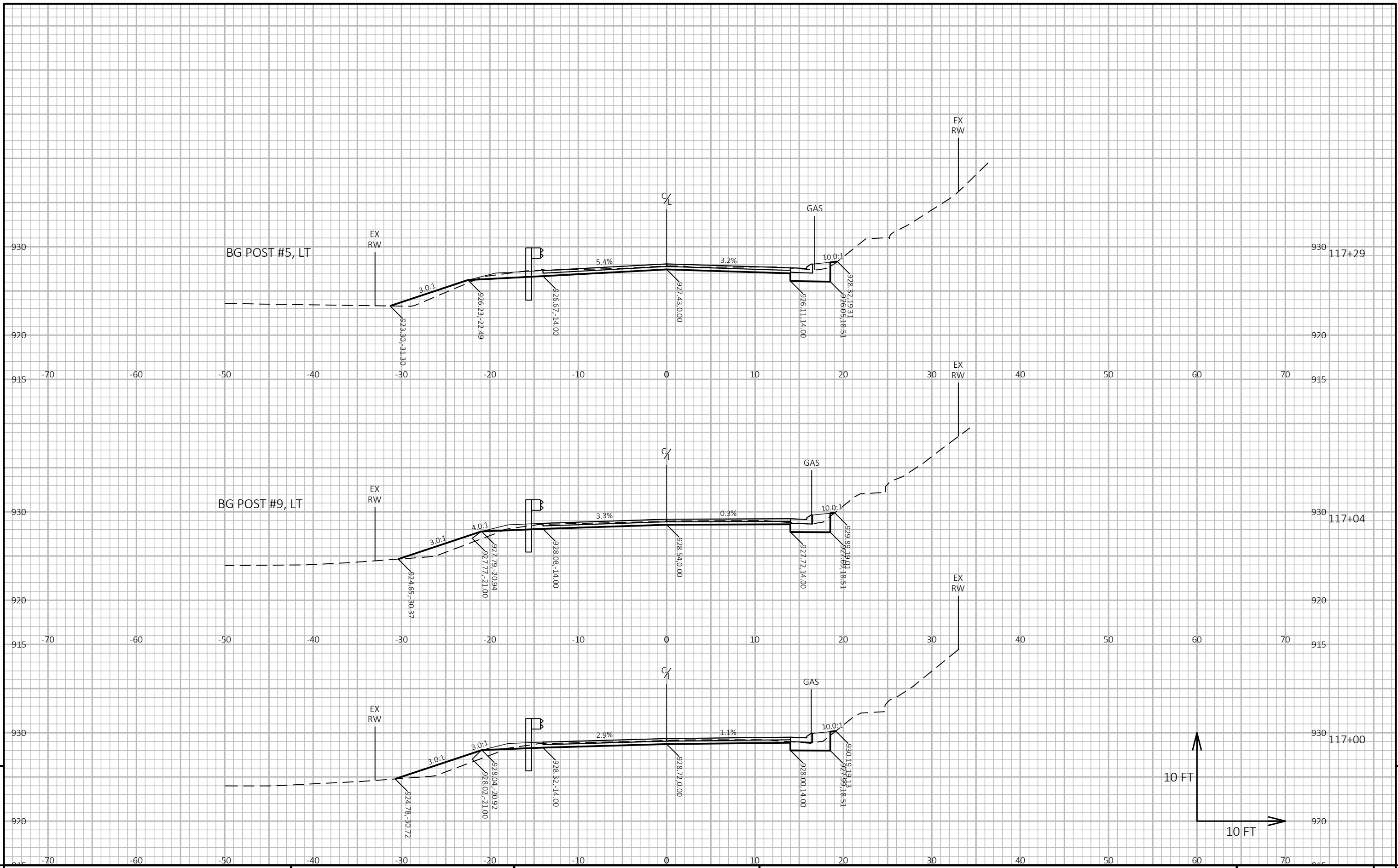
HWY: STH 171

COUNTY: CRAWFORD

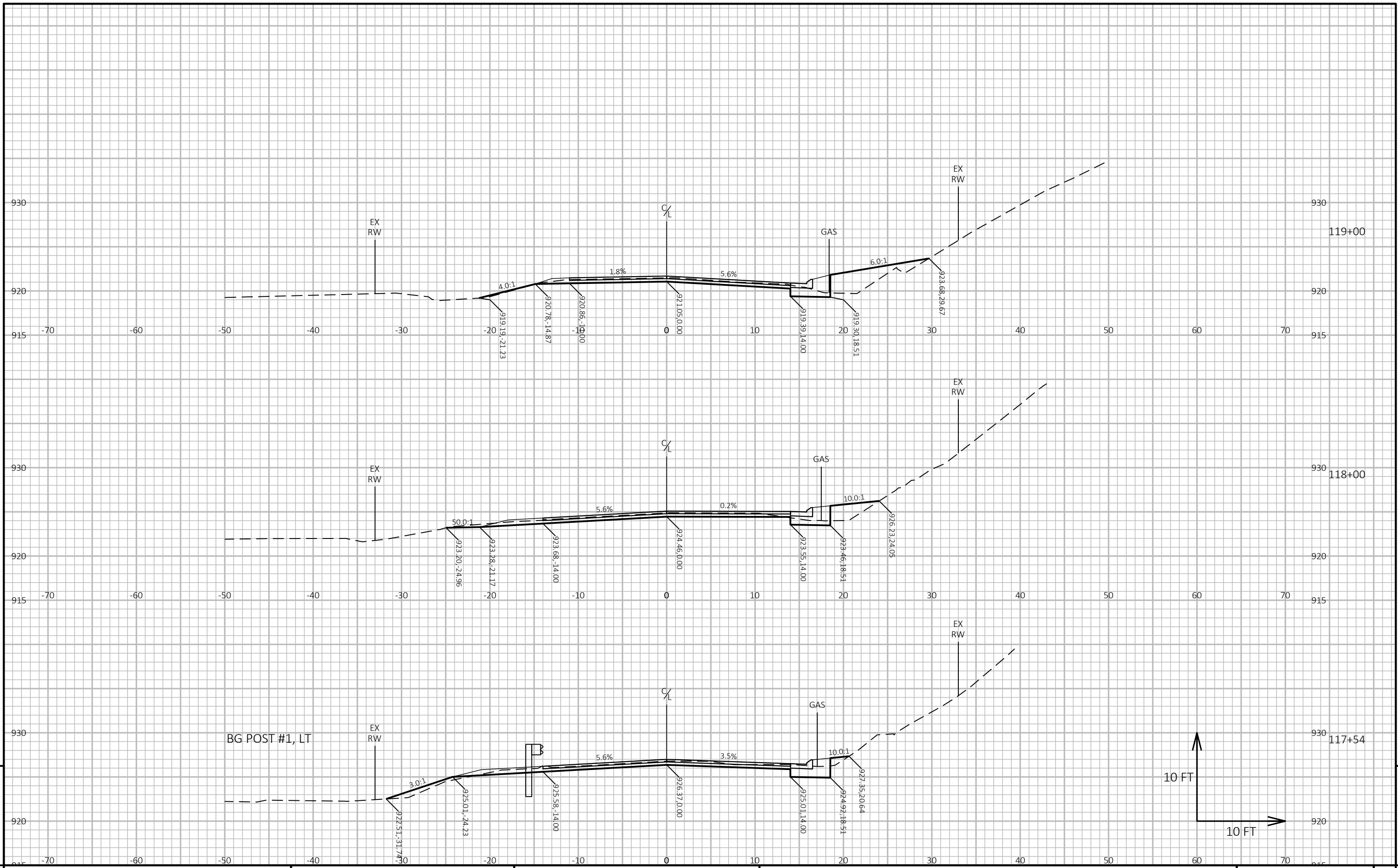
CROSS SECTIONS: STH 171

SHEET

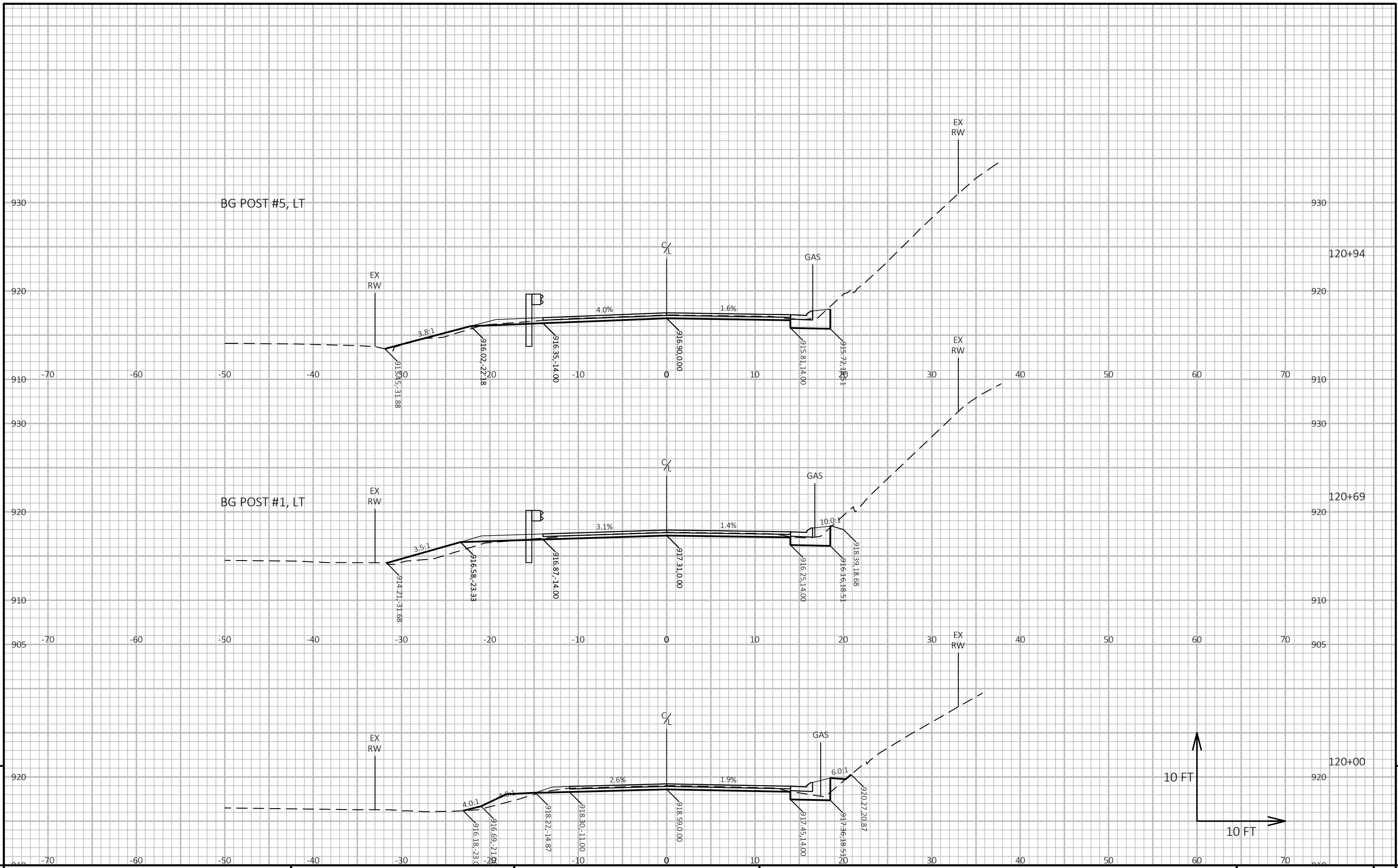
E



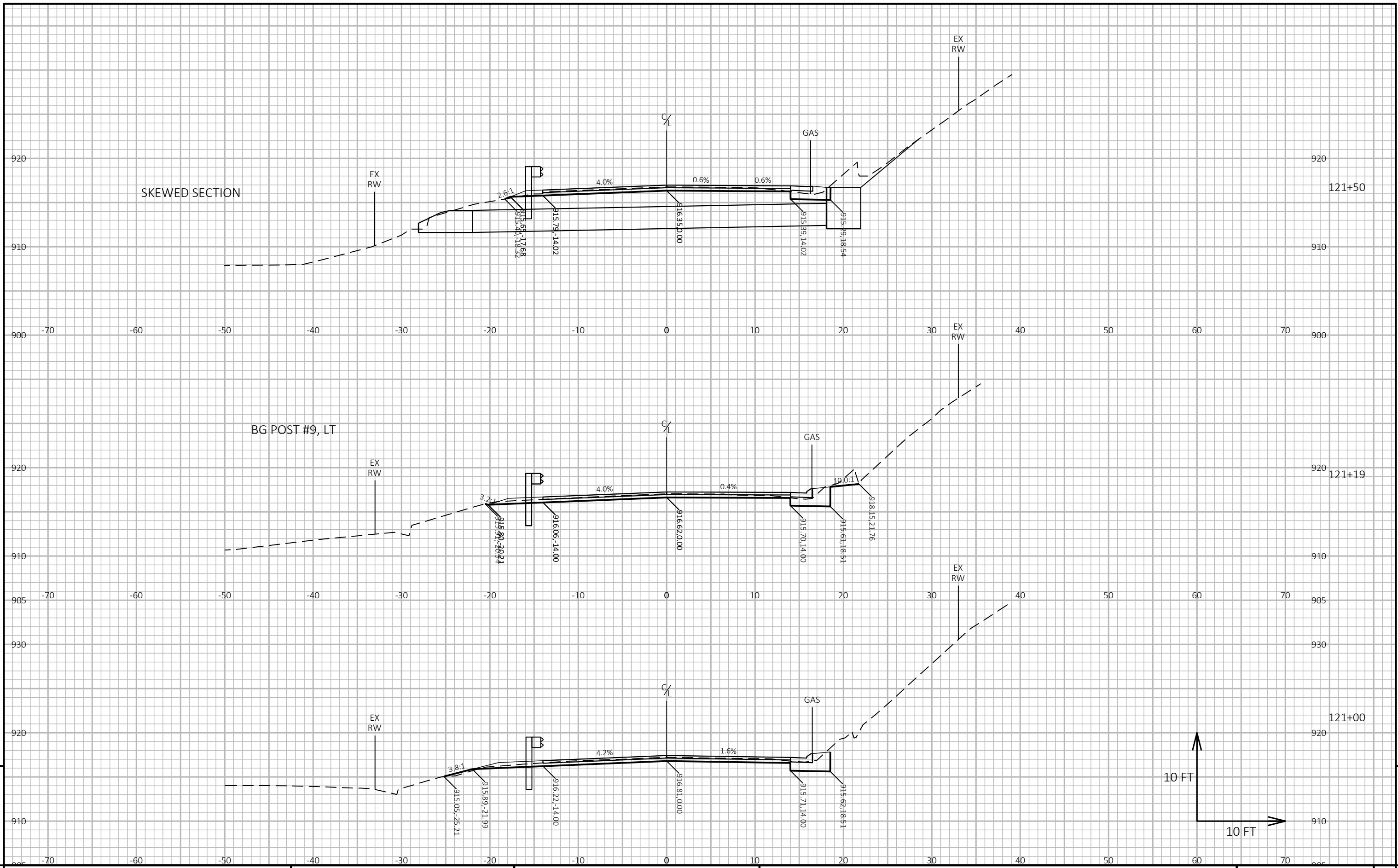
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



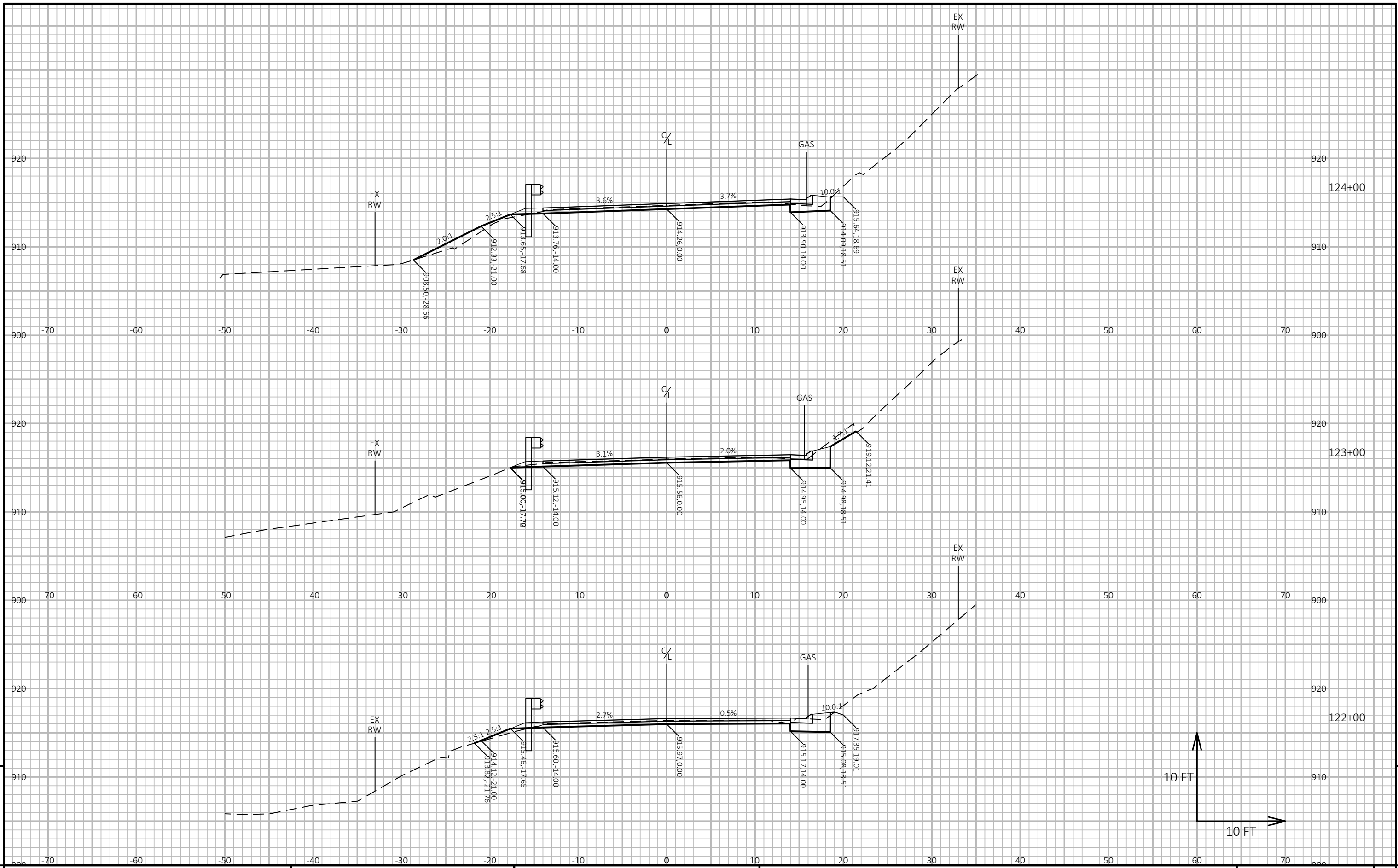
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

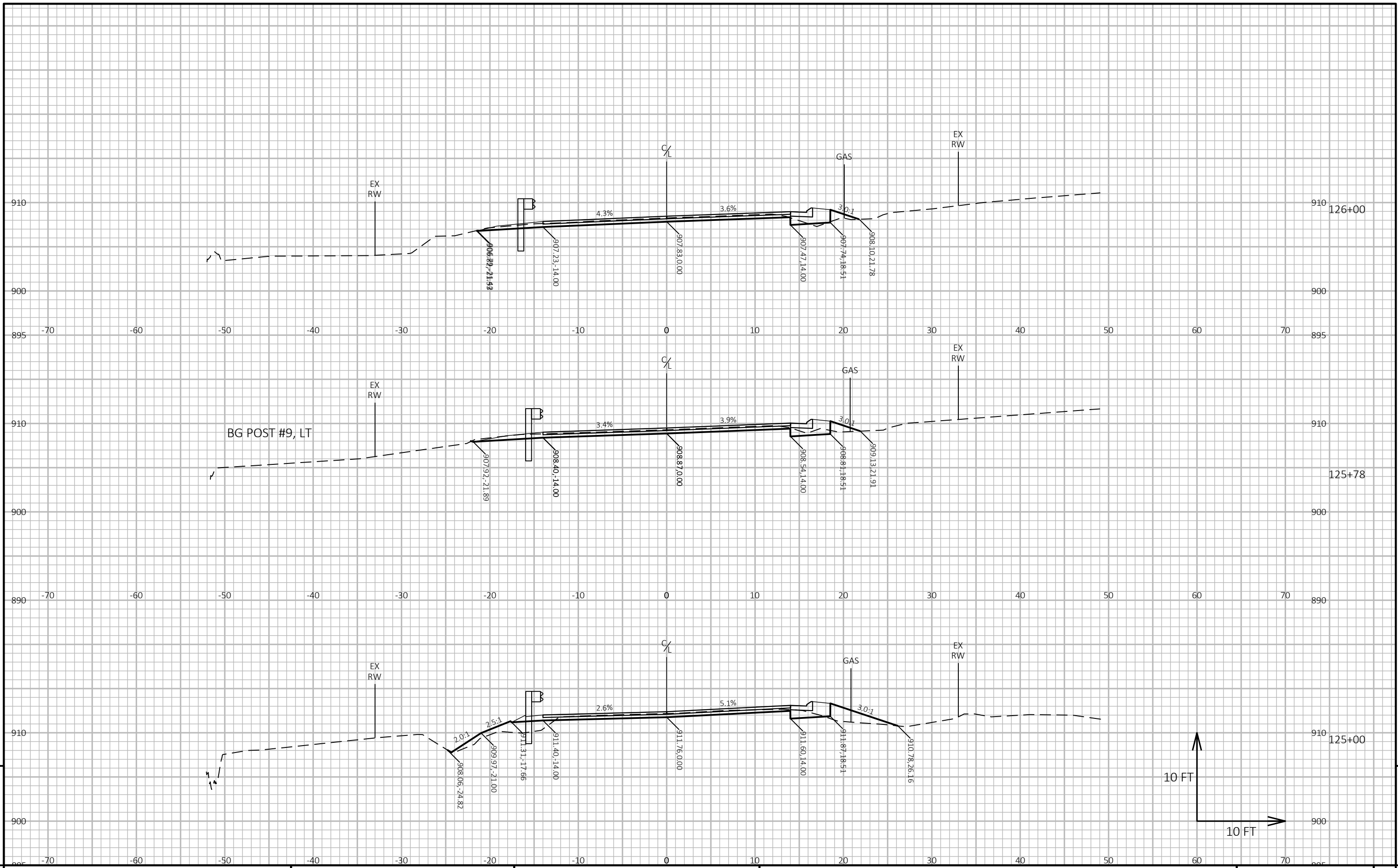


PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171	SHEET	E
------------------------	--------------	------------------	-------------------------	-------	----------

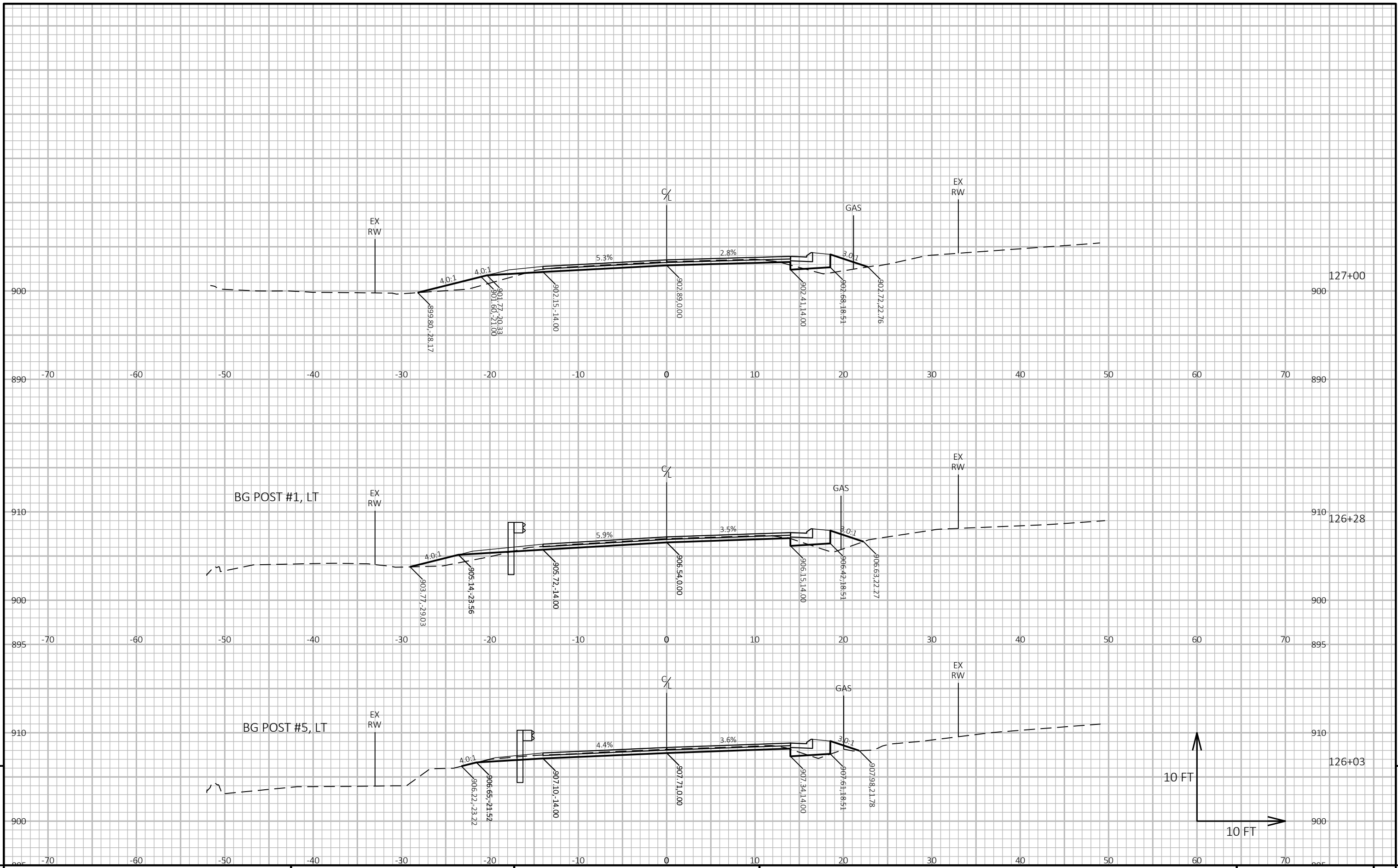
9

9

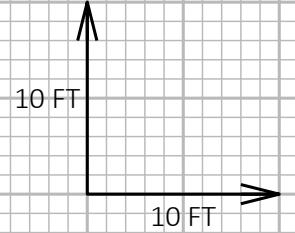
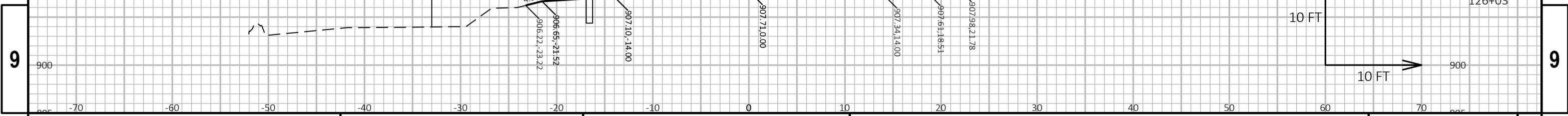


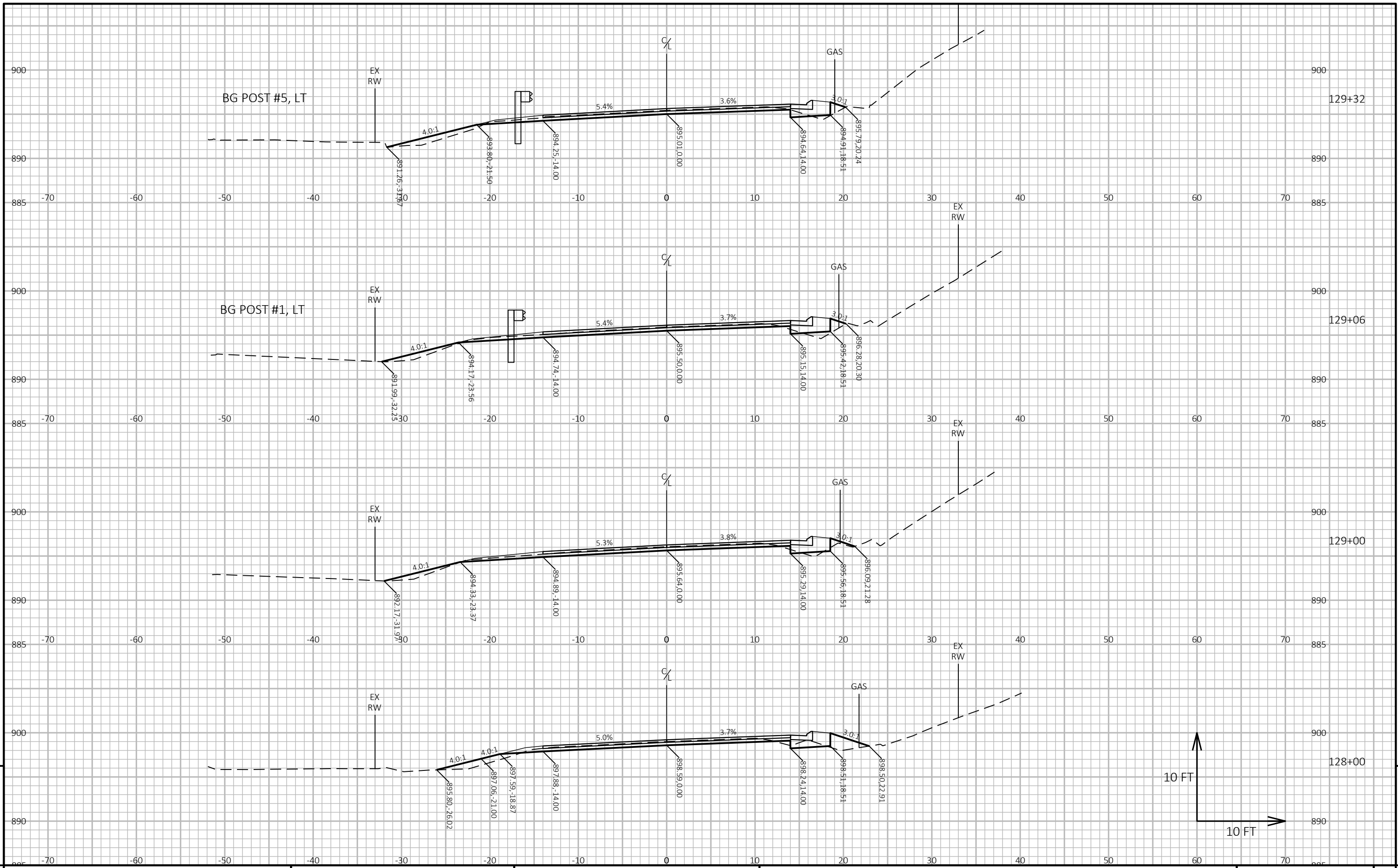


PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

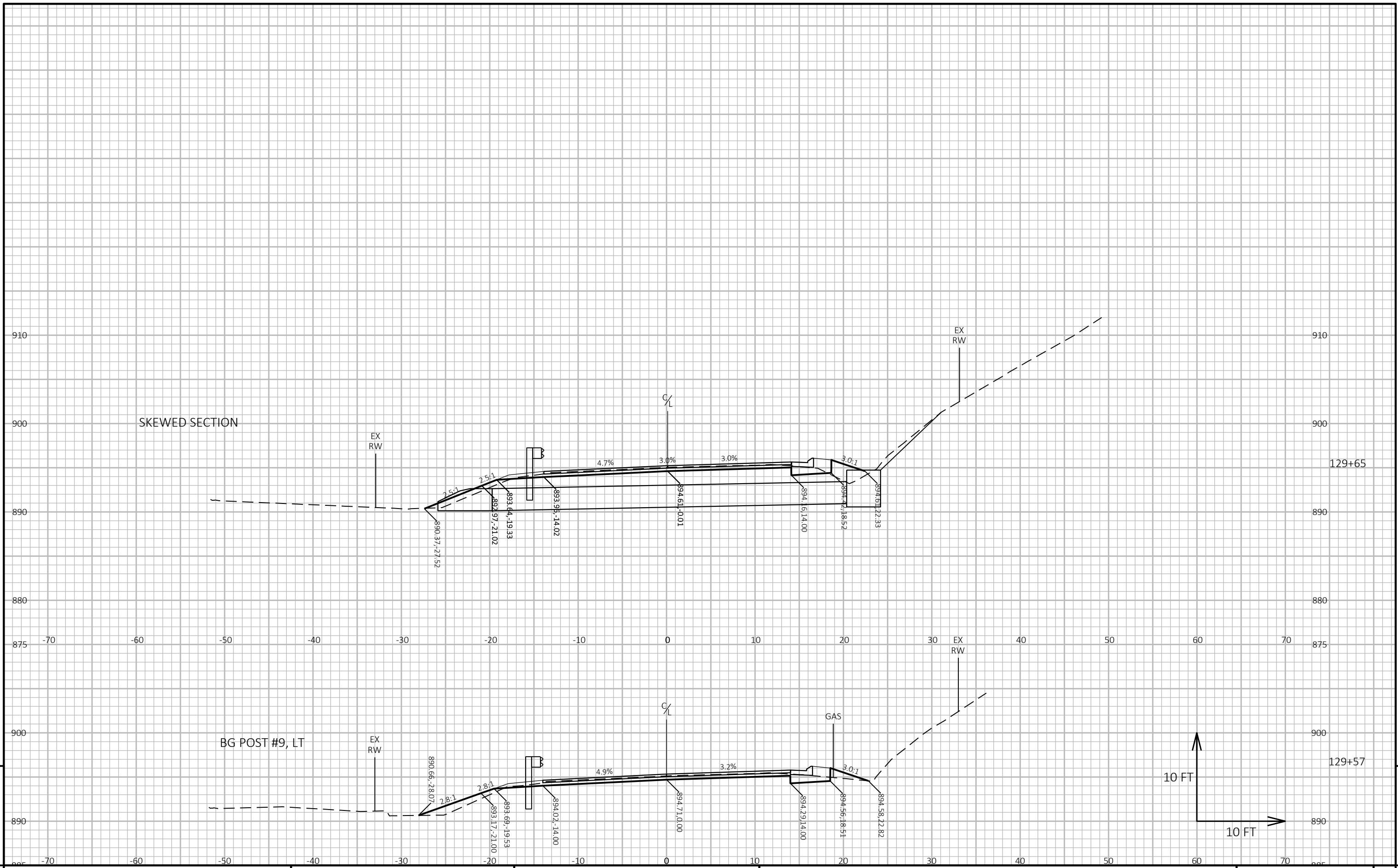


PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

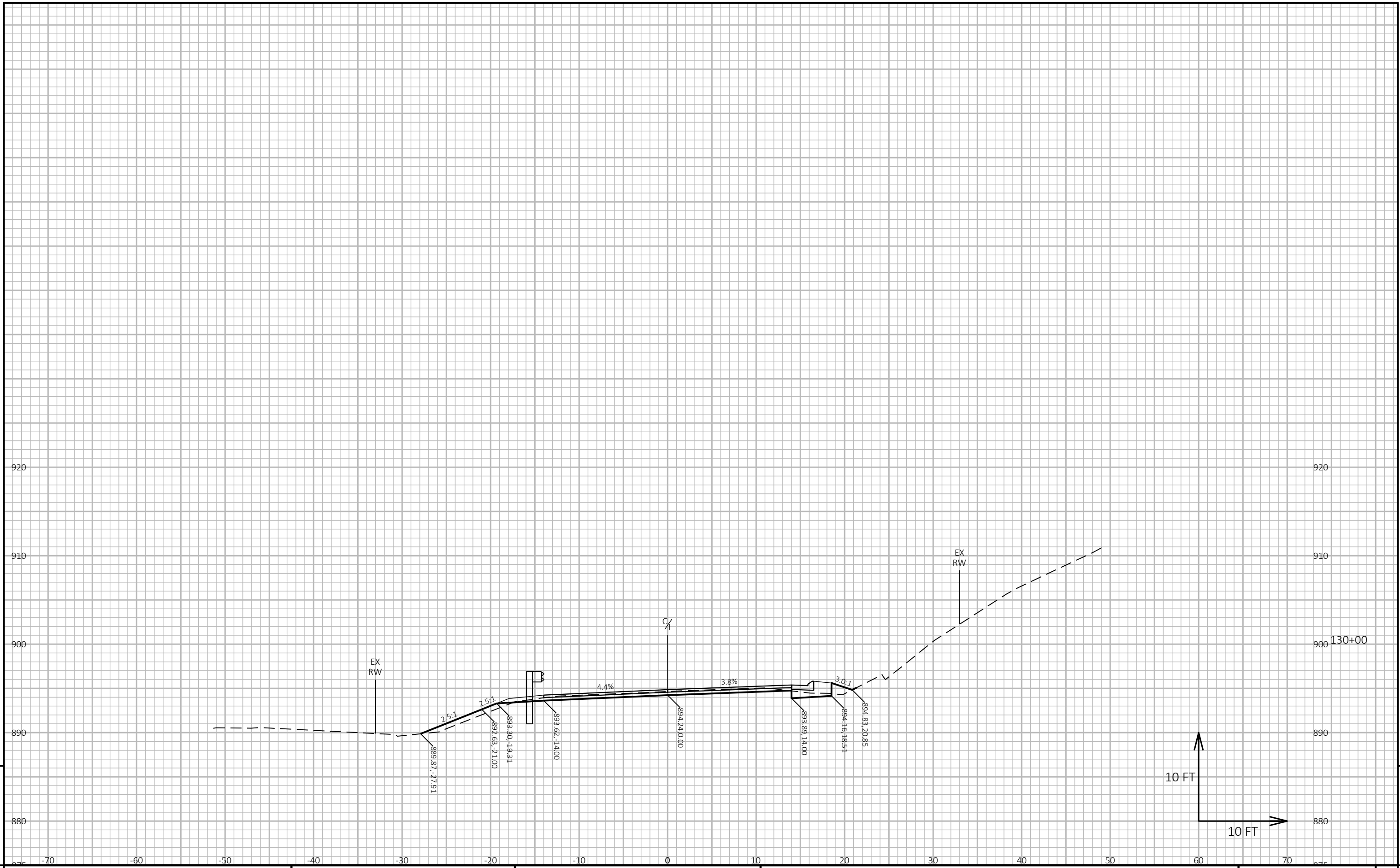




PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171	SHEET E
------------------------	--------------	------------------	-------------------------	---------



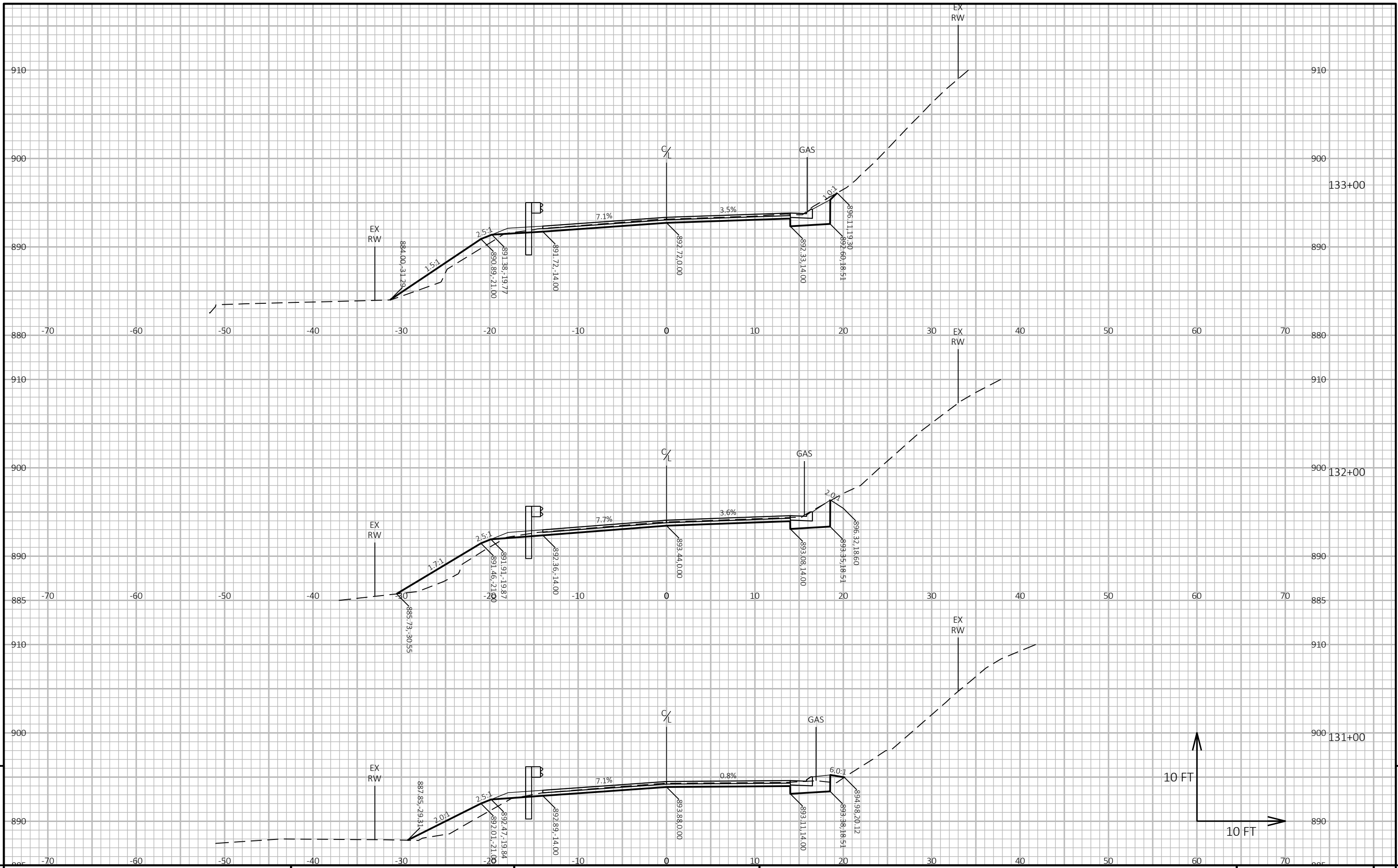
9

9

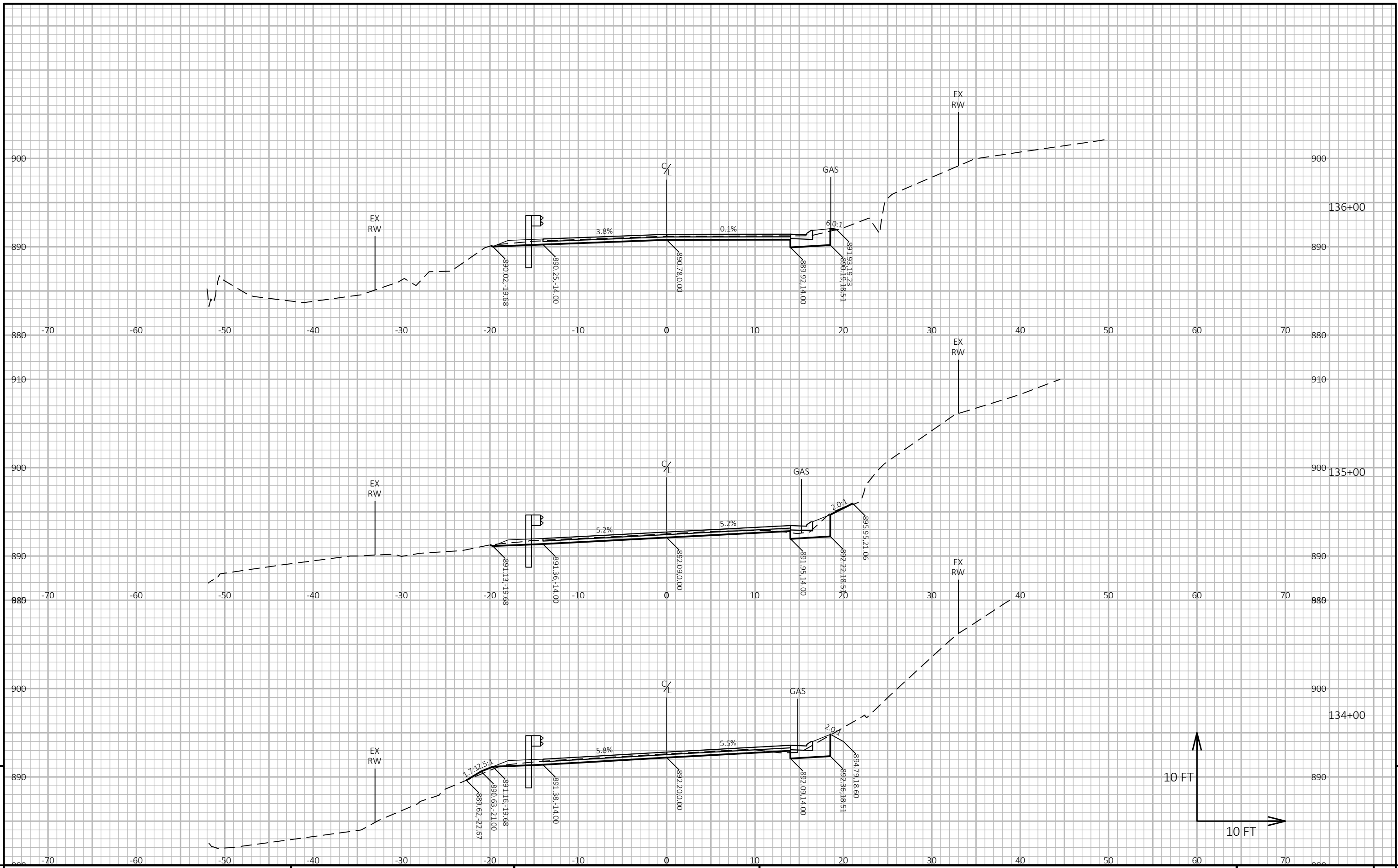
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171	SHEET	E
------------------------	--------------	------------------	-------------------------	-------	---

FILE NAME : \\SP-PZ1.SEHINC.COM\PROJECTS\1\UZ\W\WITSW\162034\5-FINAL-DSGN\51-DRAWINGS\10-CIVIL\C3D\57900202\SHEETS\PLAN\090101_XS.DWG PLOT DATE : 7/19/2024 5:18 AM PLOT BY : SAVANNAH STEHN PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

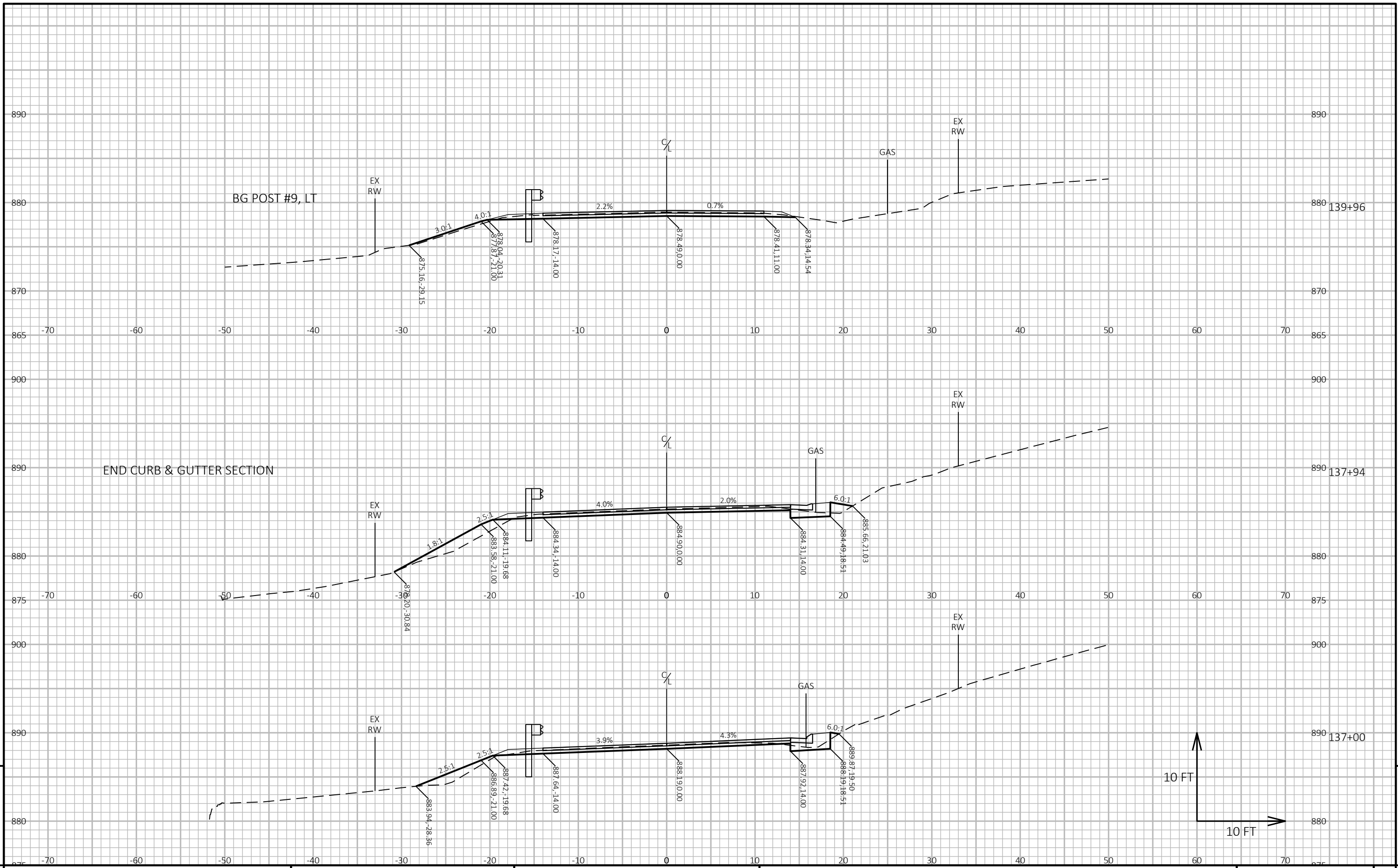
LAYOUT NAME - SG1-27



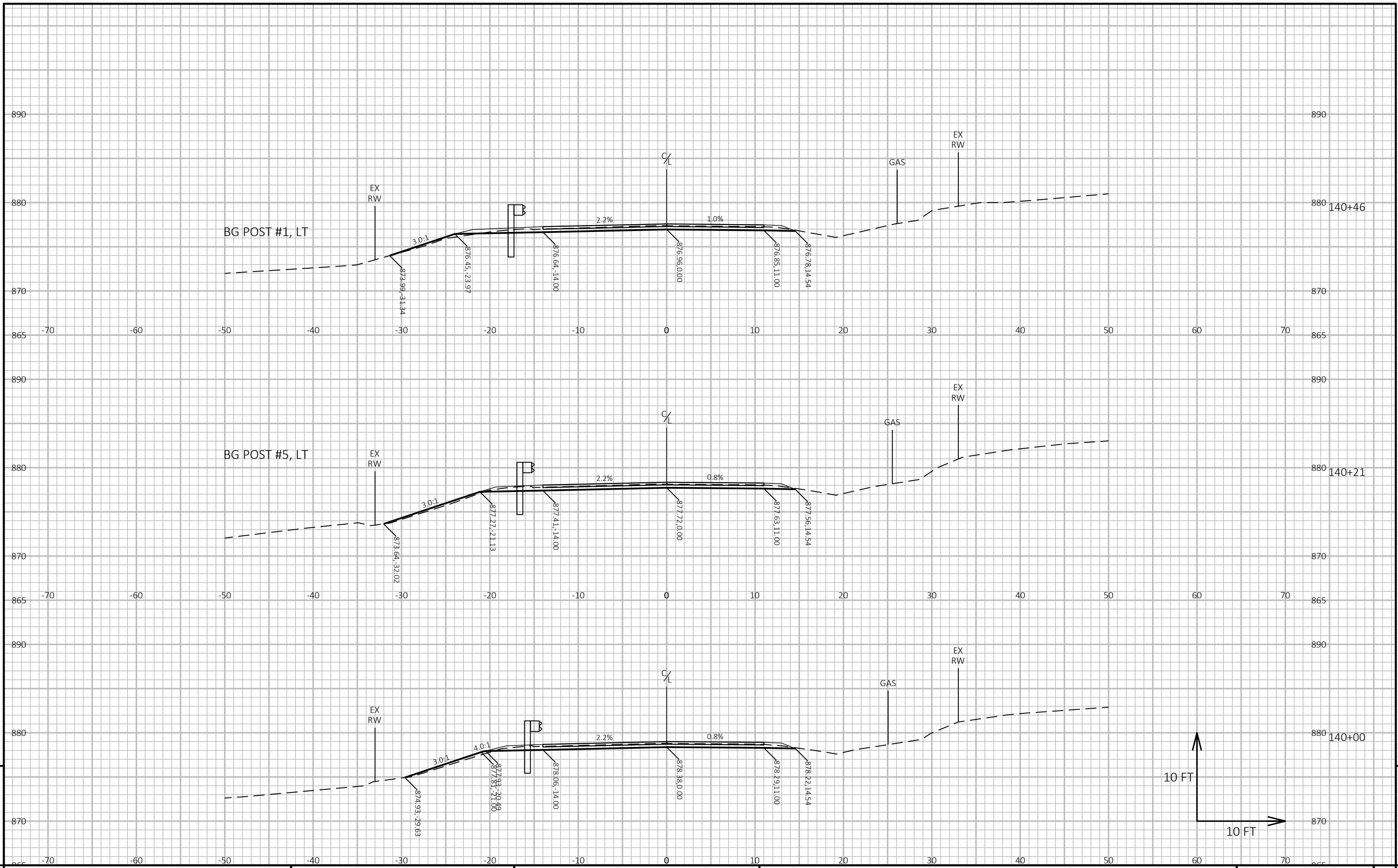
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



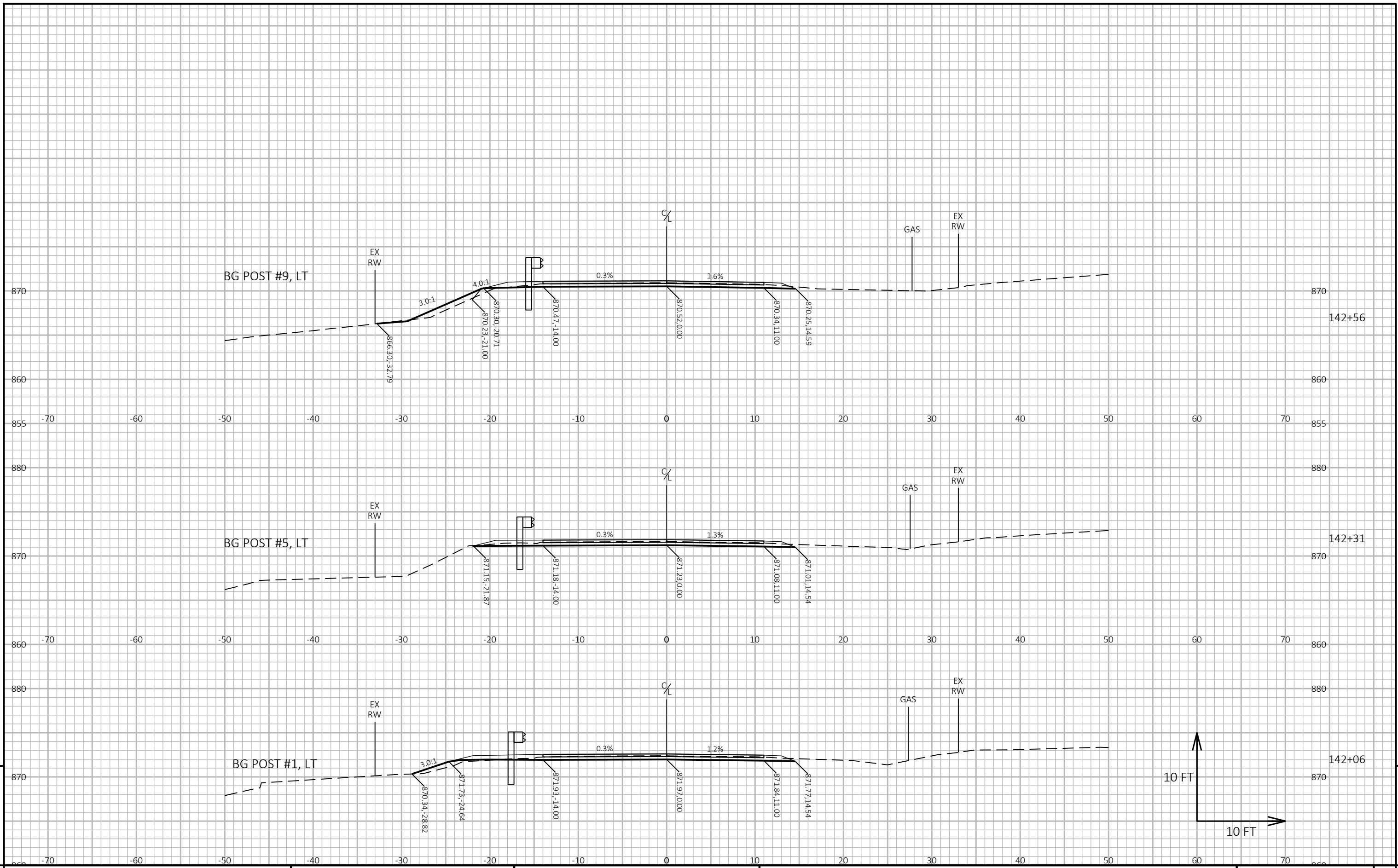
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



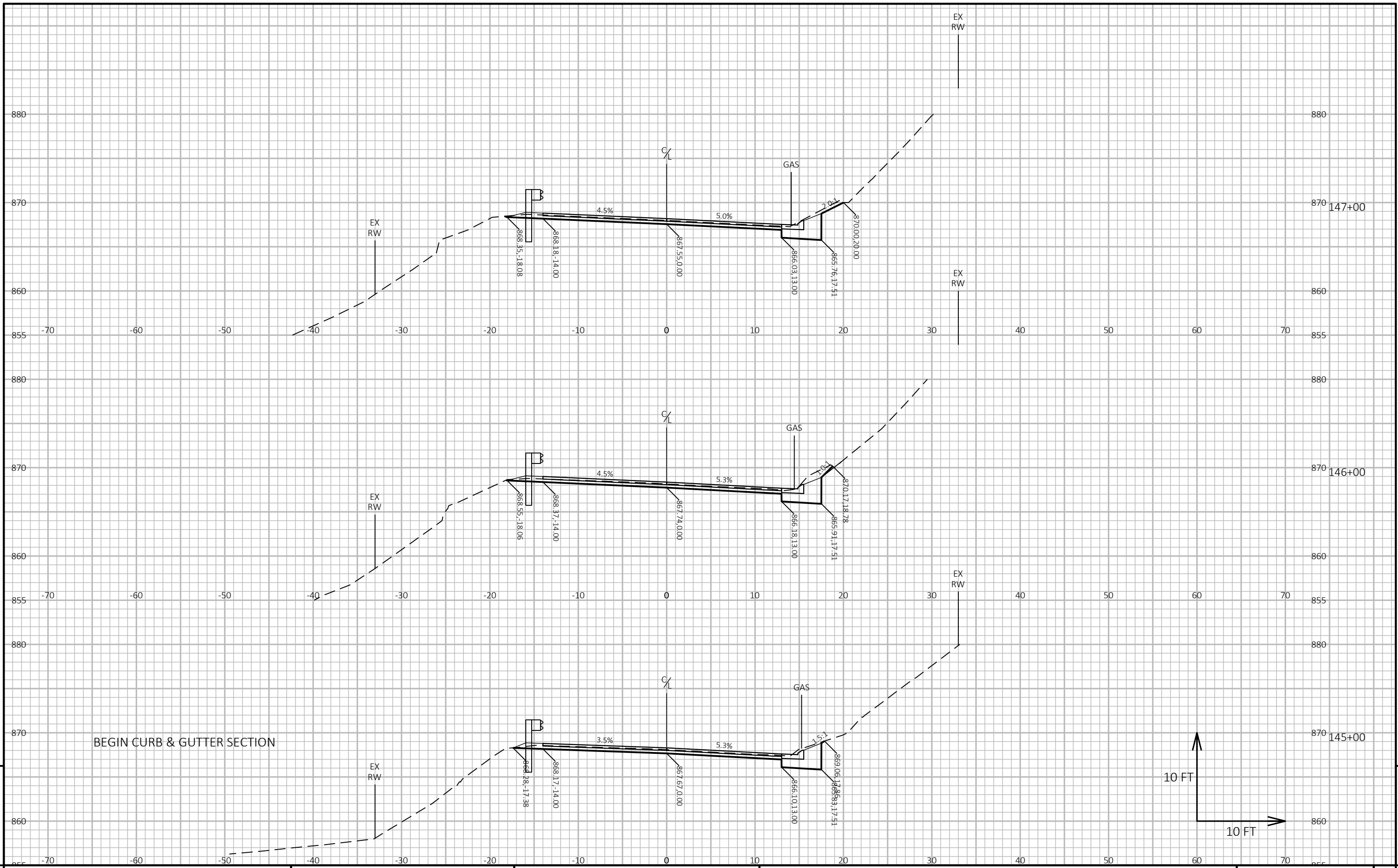
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



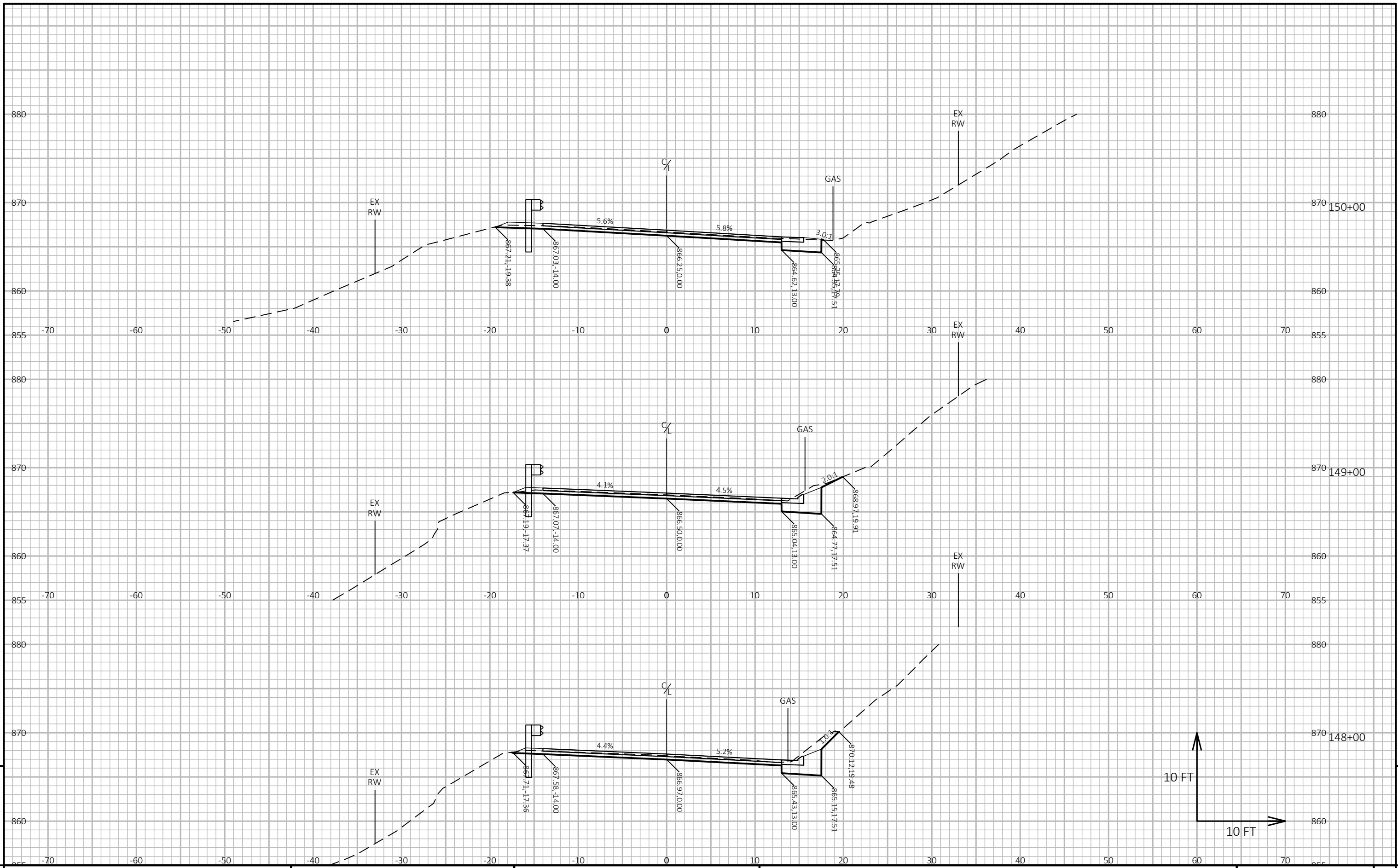
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171	SHEET 9
------------------------	--------------	------------------	-------------------------	---------



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET 9



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



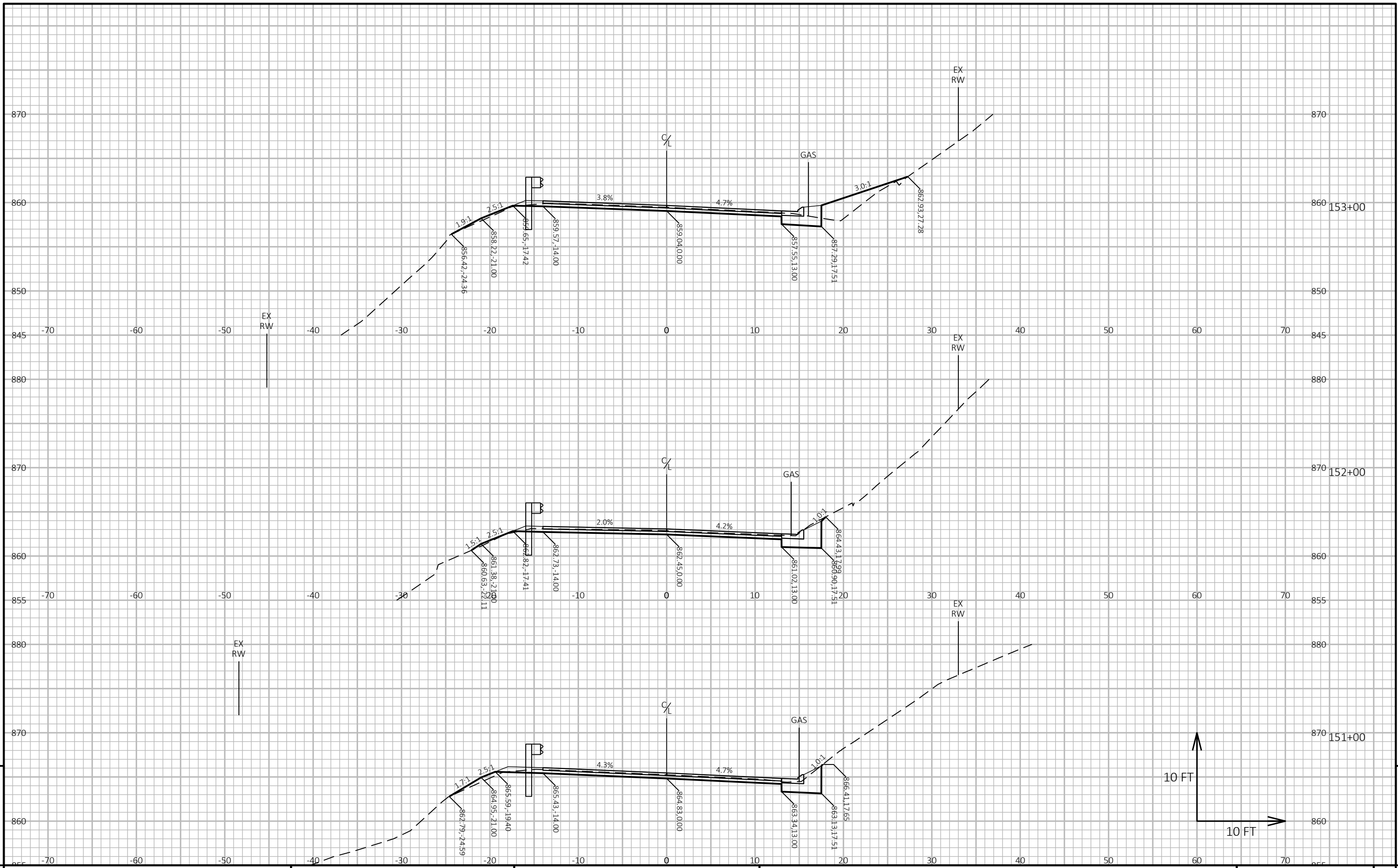
9

9

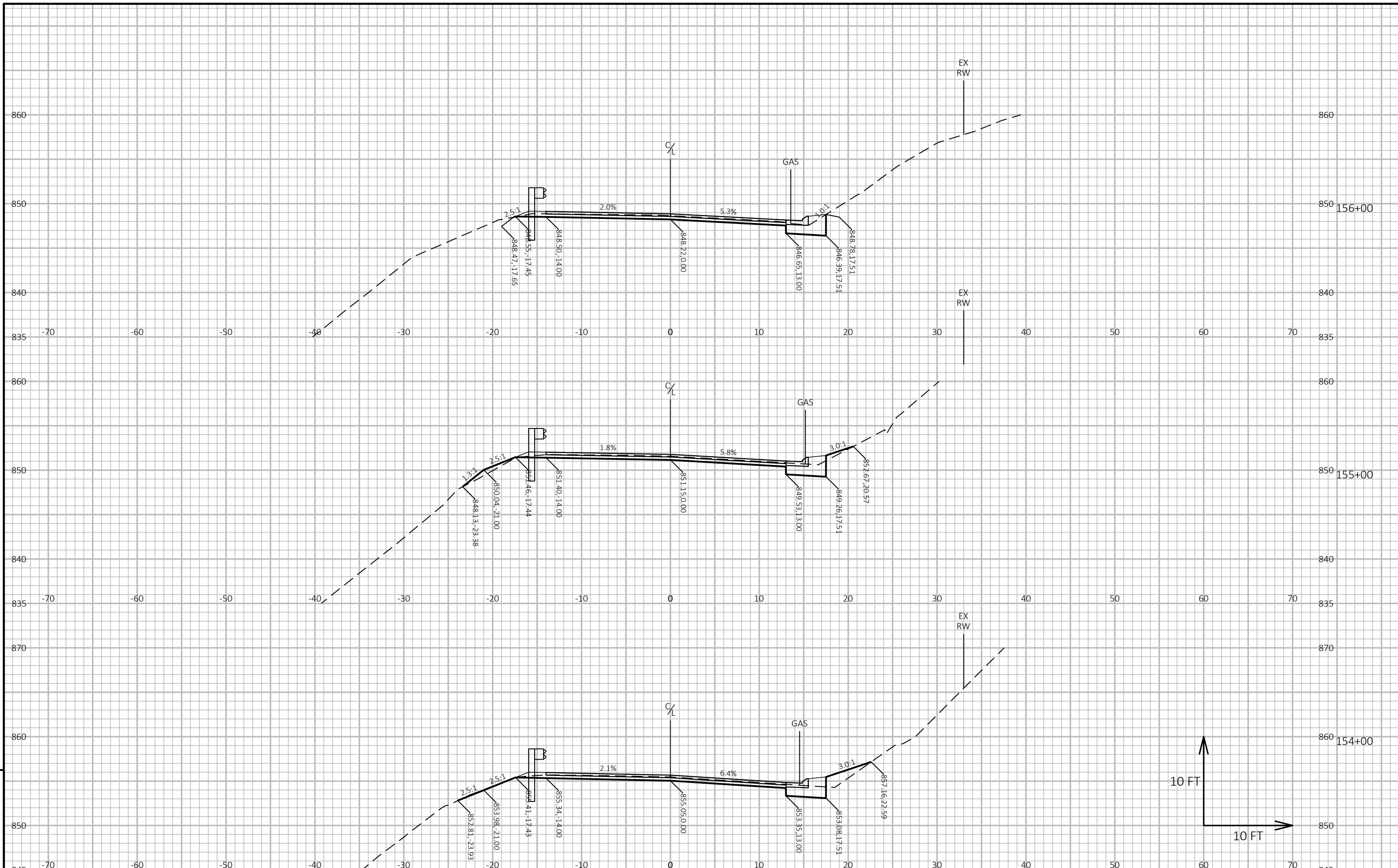
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

FILE NAME: \\SP-PZ1.SEHINC.COM\PROJECTS\UZ\W\WITSW\162034\5-FINAL-DSGN\51-DRAWINGS\10-CIVIL\C3D\57900202\SHEETS\PLAN\090102_XS.DWG PLOT DATE: 7/19/2024 11:30 AM PLOT BY: SAVANNAH STEHN PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

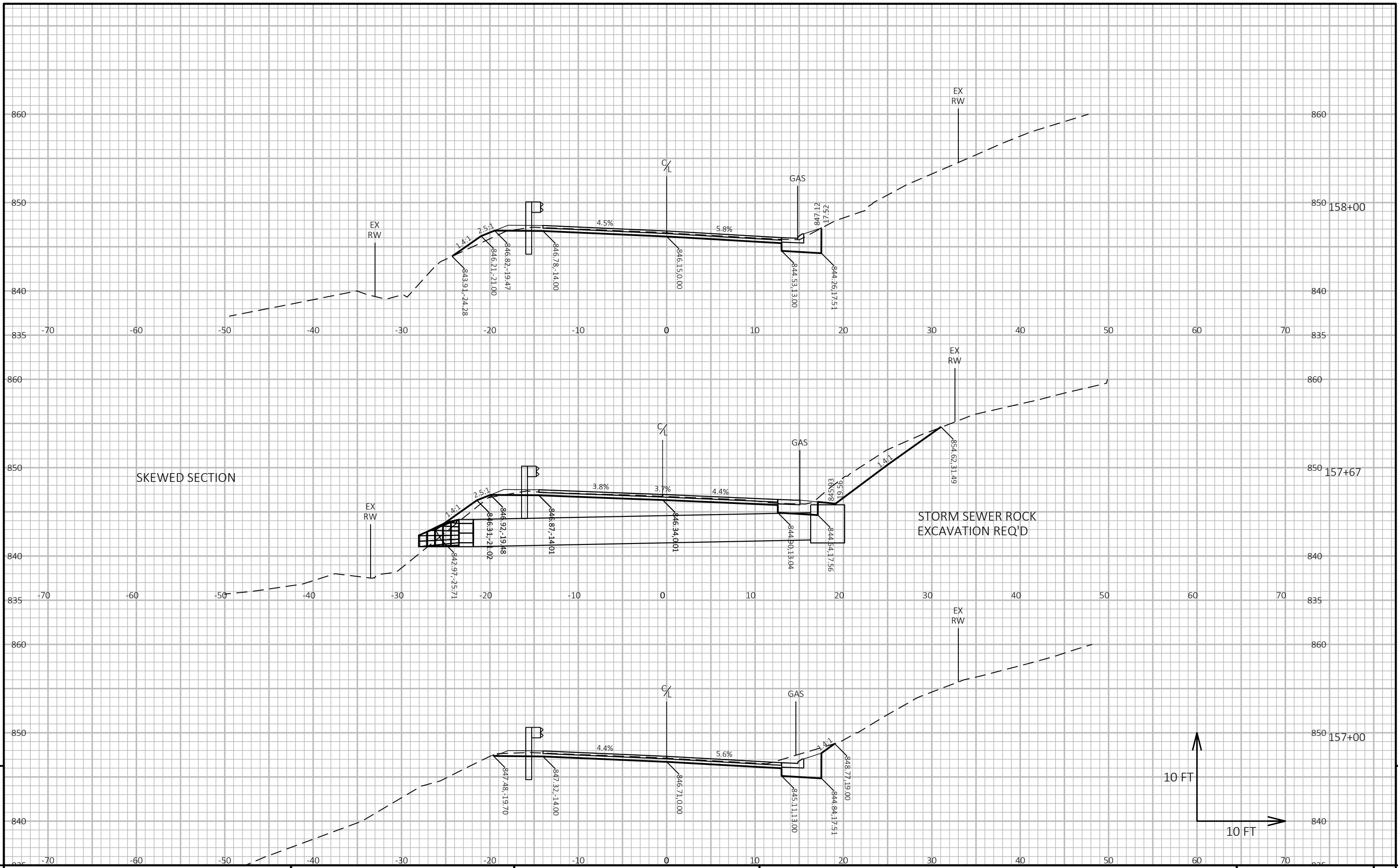
LAYOUT NAME - 06



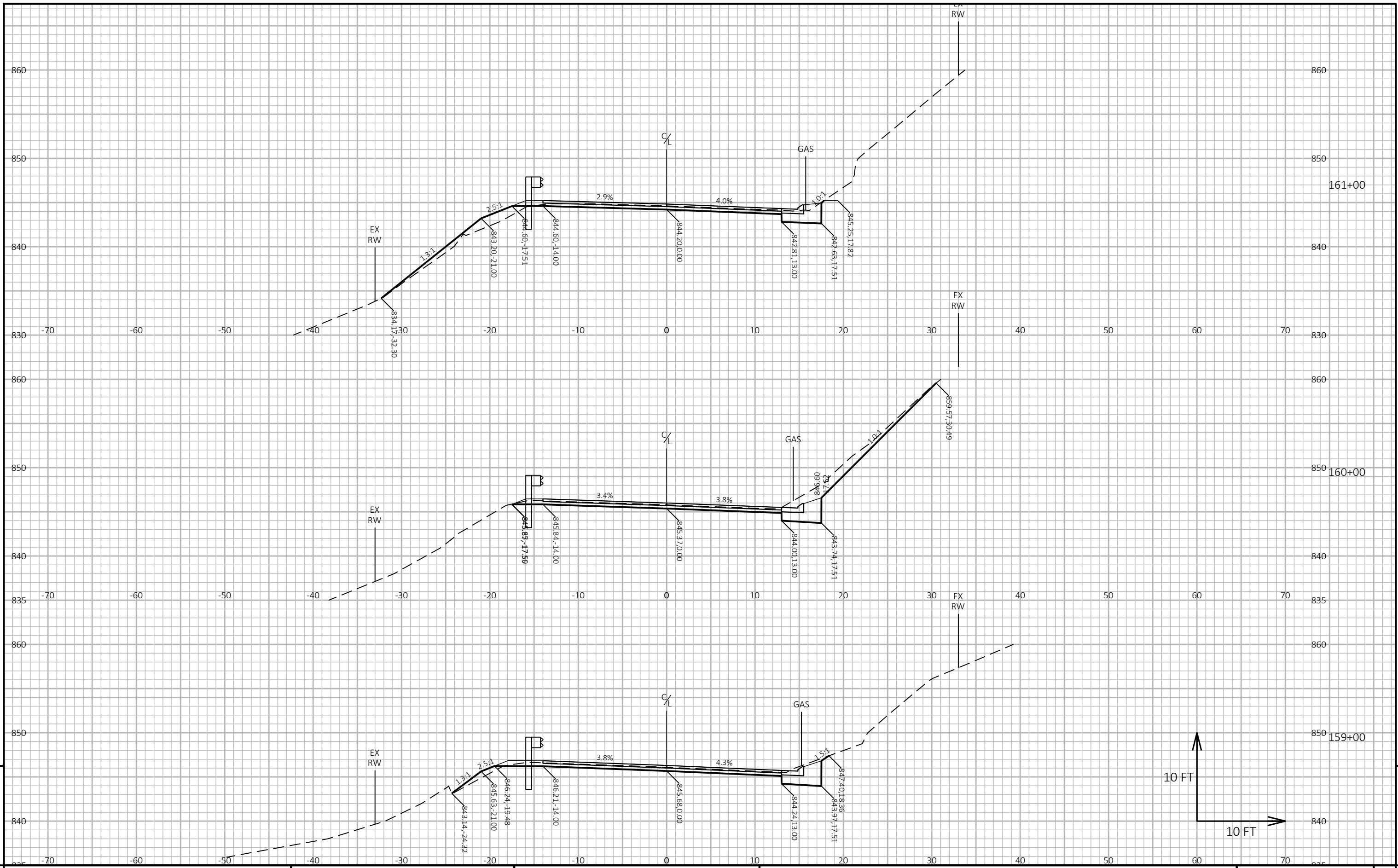
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



PROJECT NO: 5790-02-72

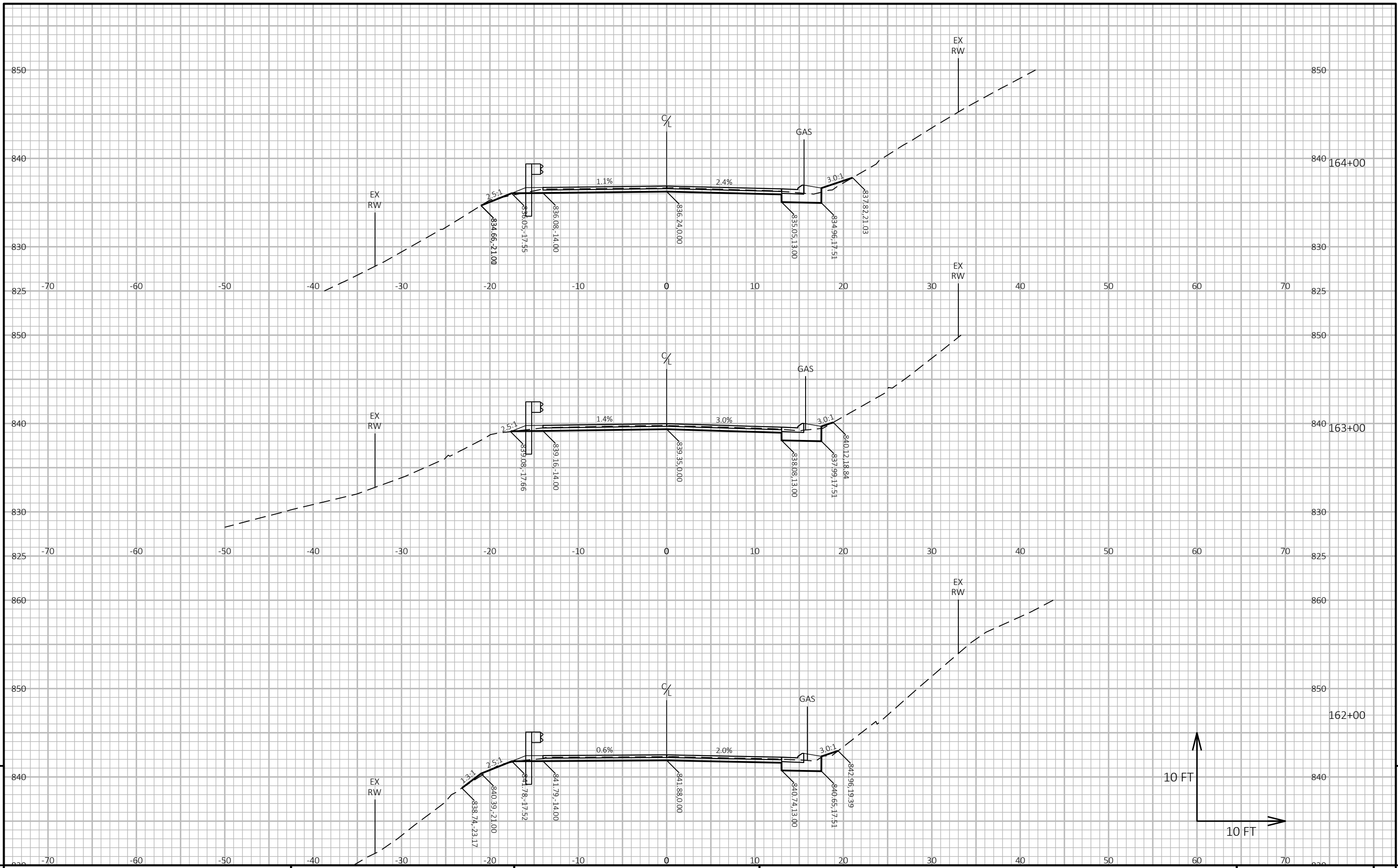
HWY: STH 171

COUNTY: CRAWFORD

CROSS SECTIONS: STH 171

SHEET

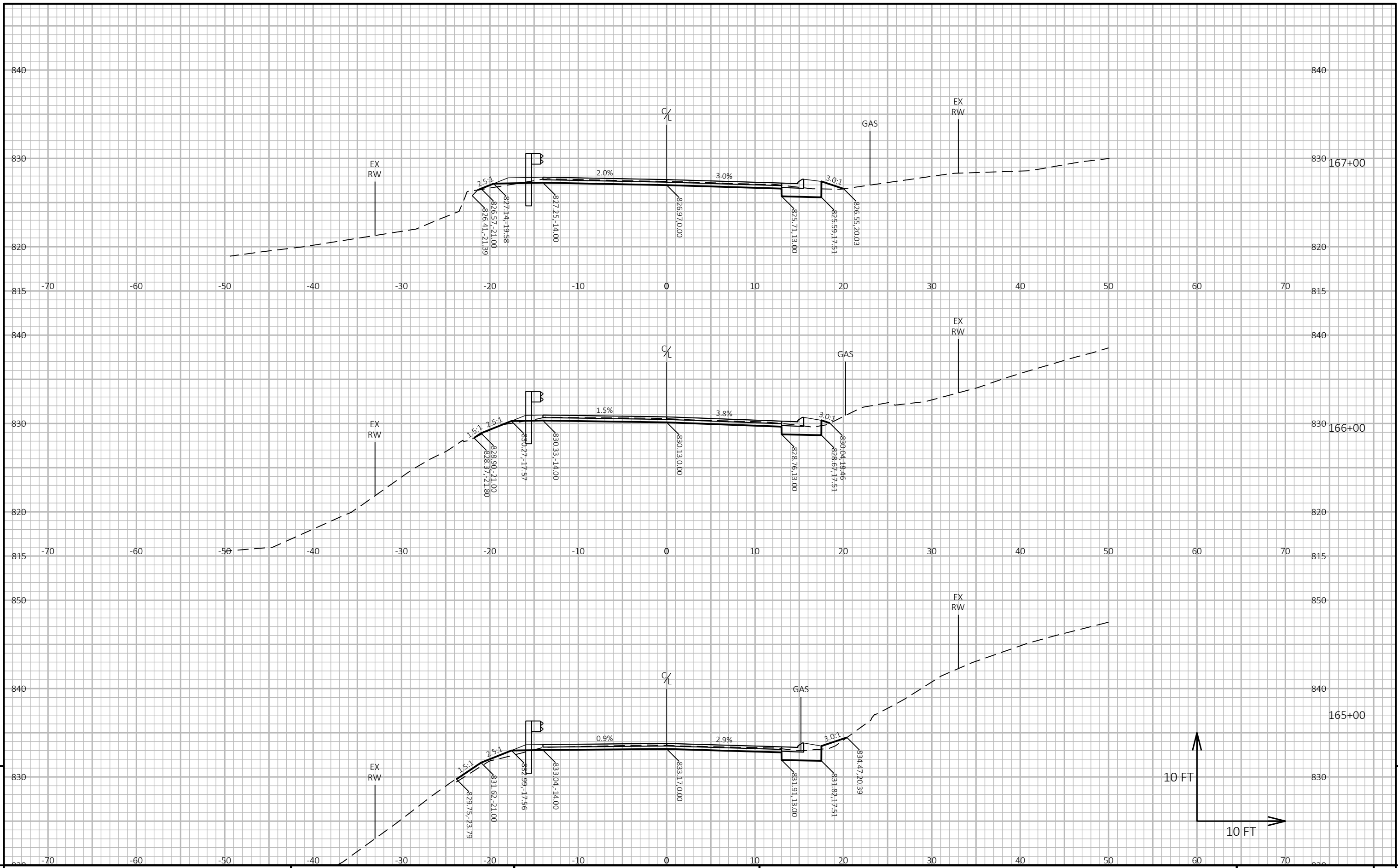
E



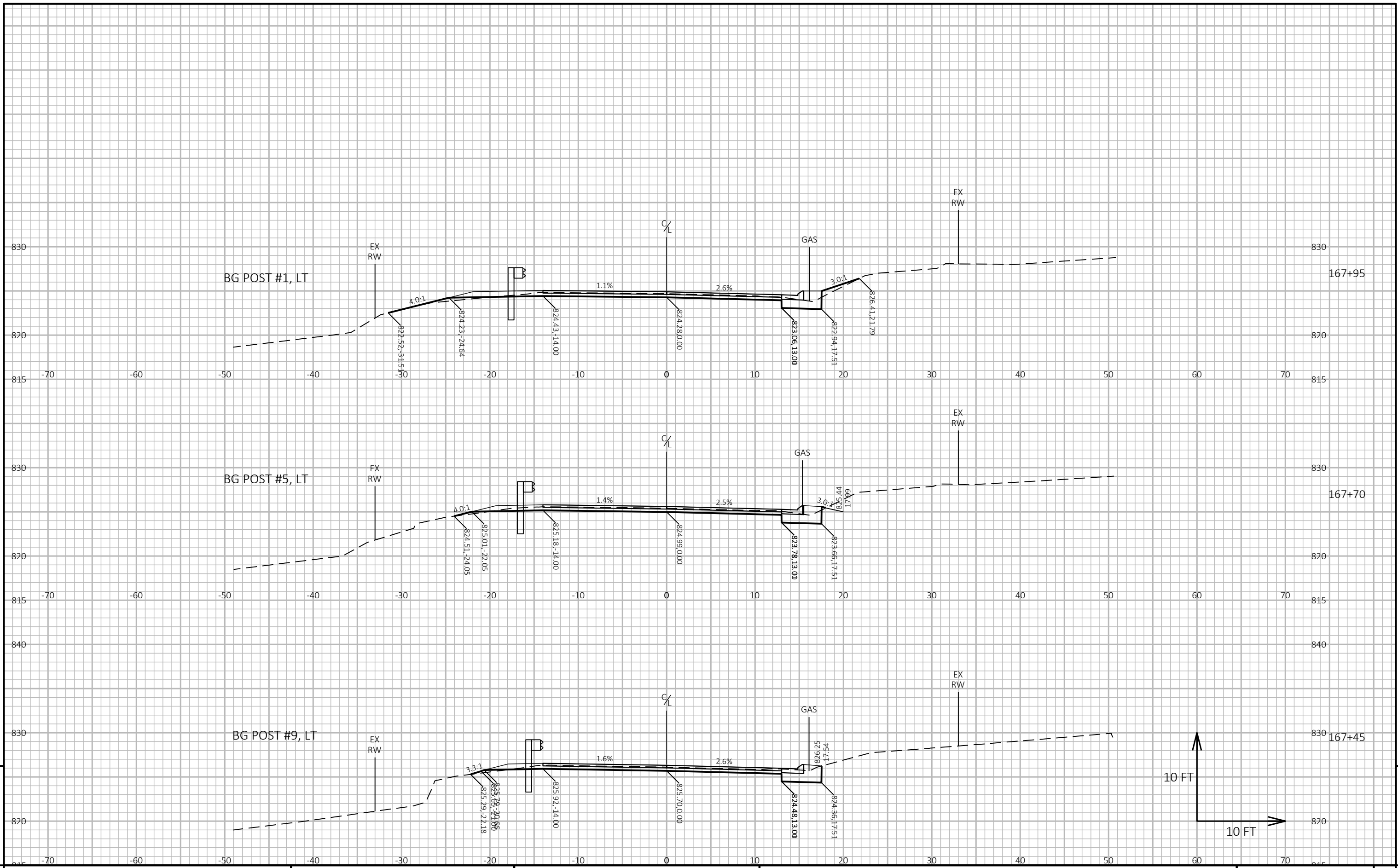
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

9

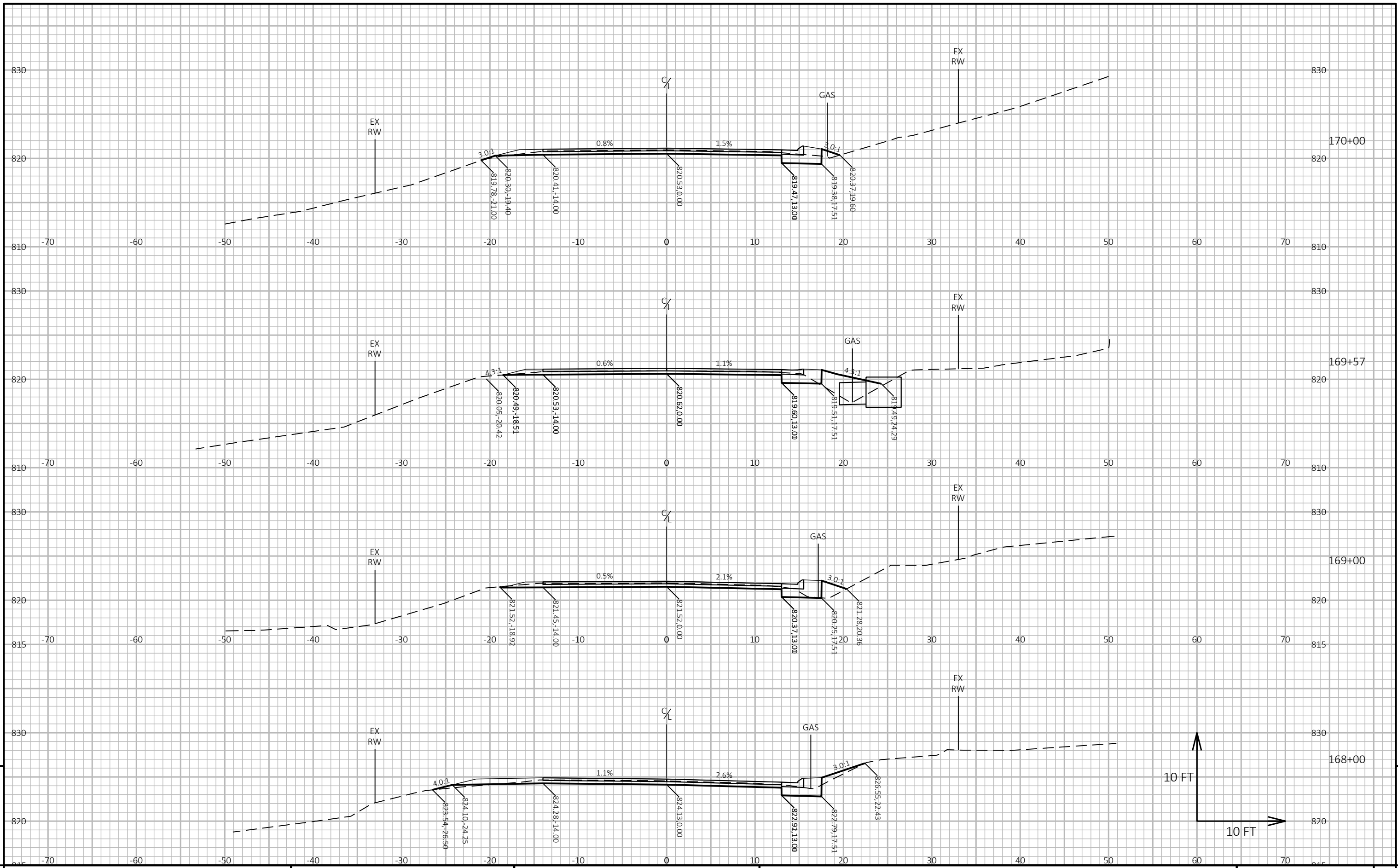
9



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET 9



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET 9



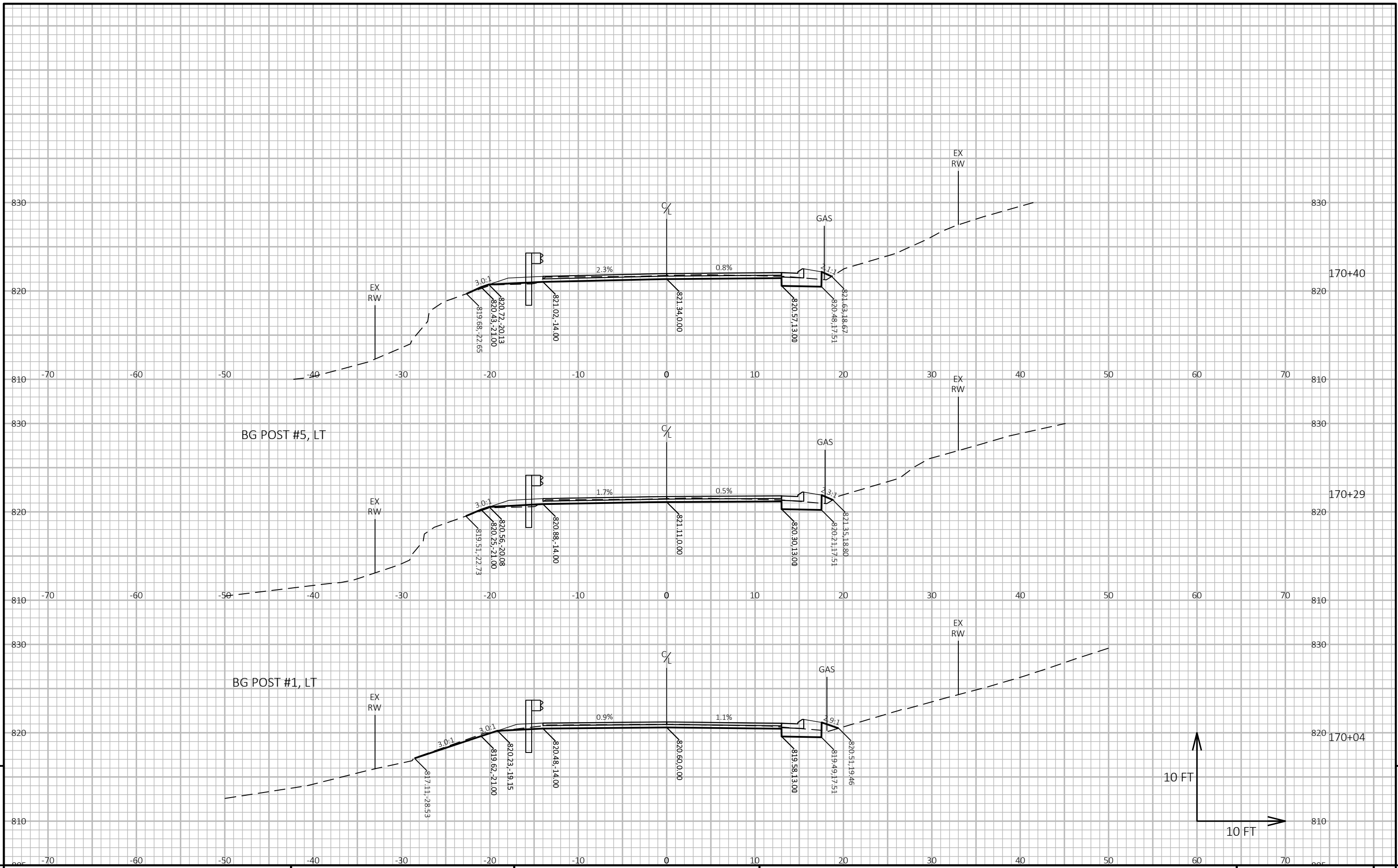
9

9

PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

FILE NAME: \\SP-PZ1.SEHINC.COM\PROJECTS\UJZ\W\WITSW\162034\5-FINAL-DSGN\51-DRAWINGS\10-CIVIL\C3D\57900202\SHEETS\PLAN\090102_XS.DWG PLOT DATE: 7/19/2024 11:32 AM PLOT BY: SAVANNAH STEHN PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

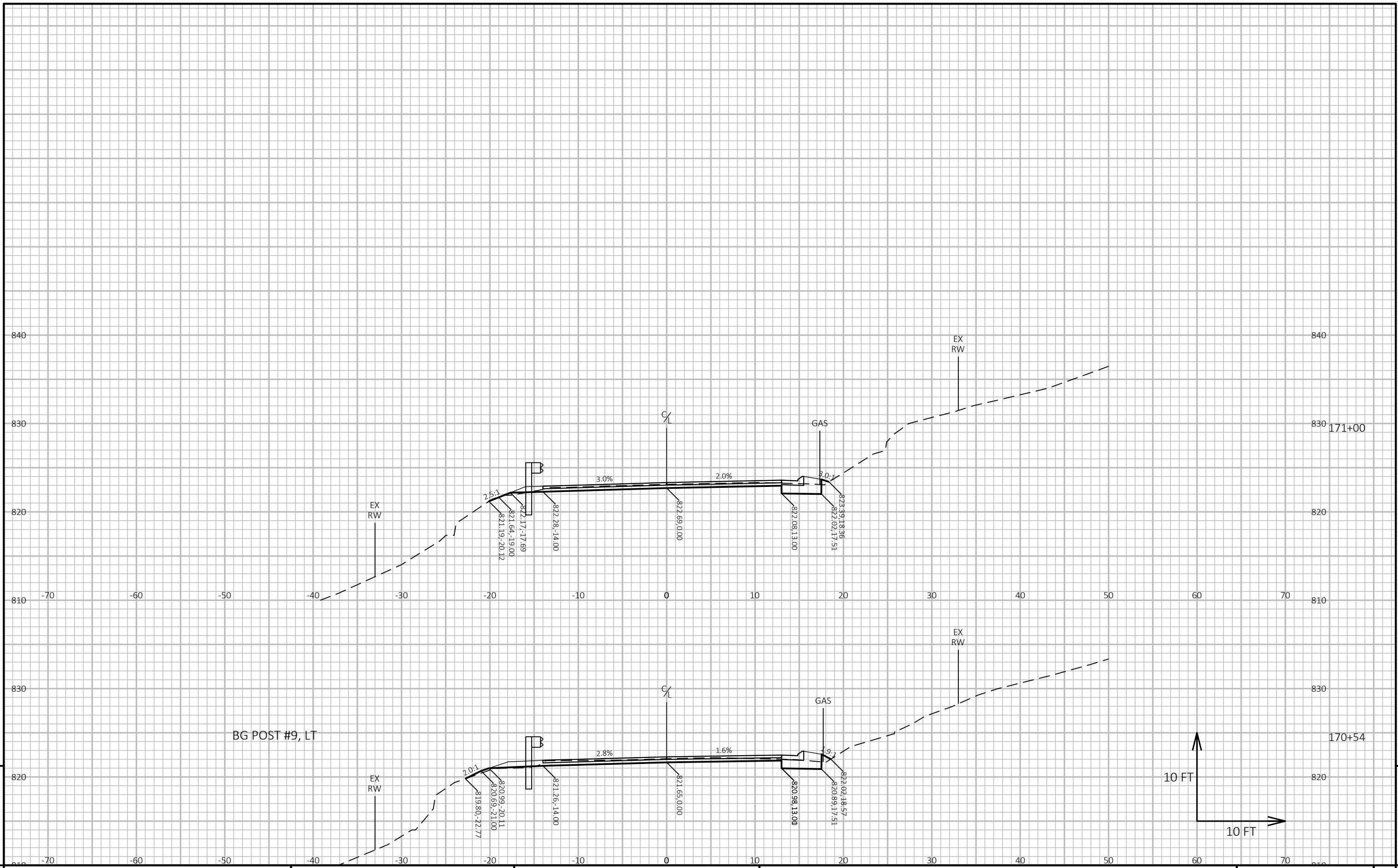
LAYOUT NAME - 14



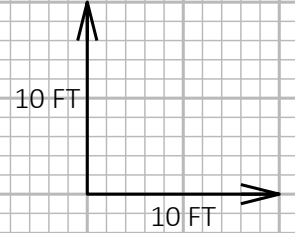
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

9

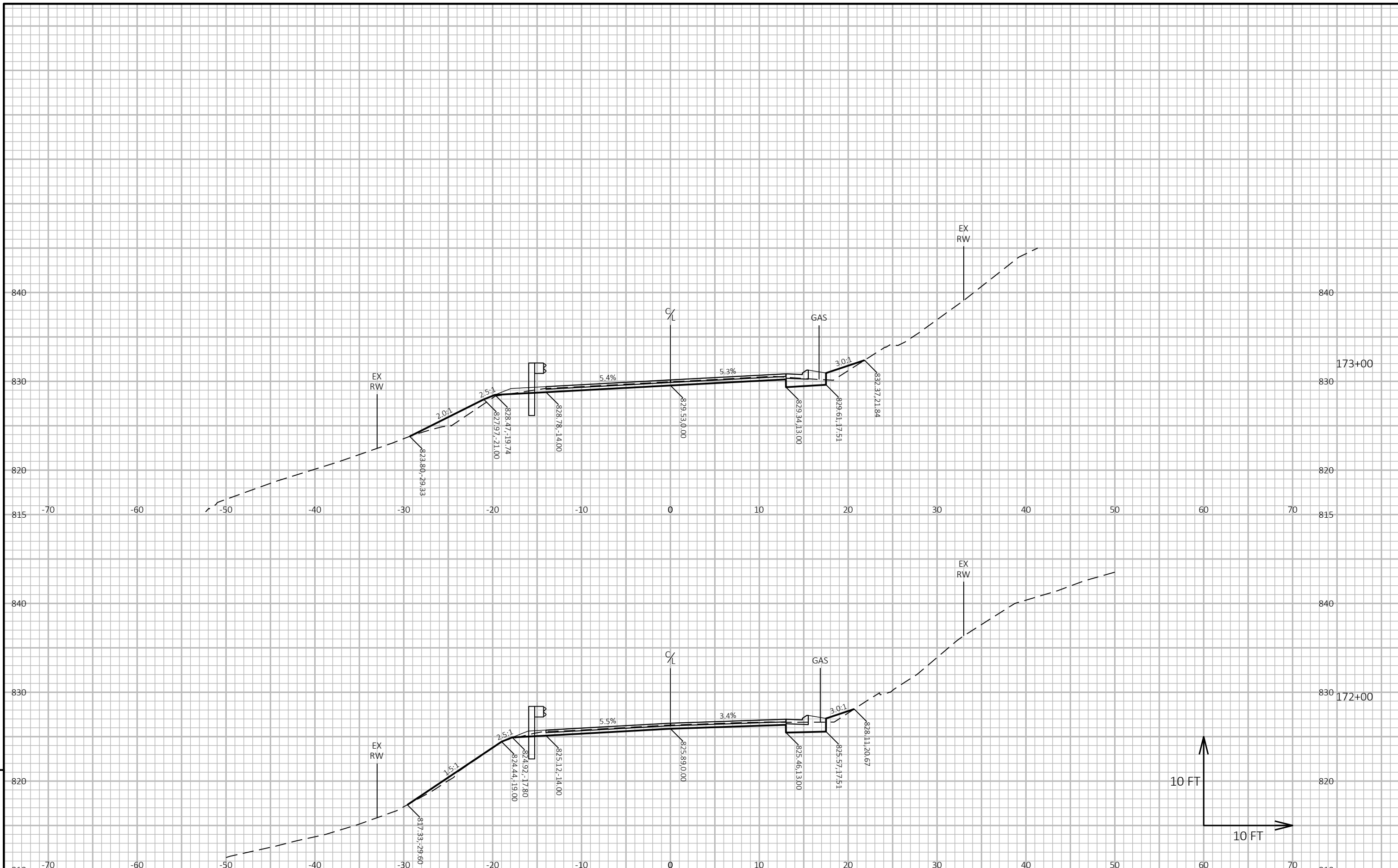
9



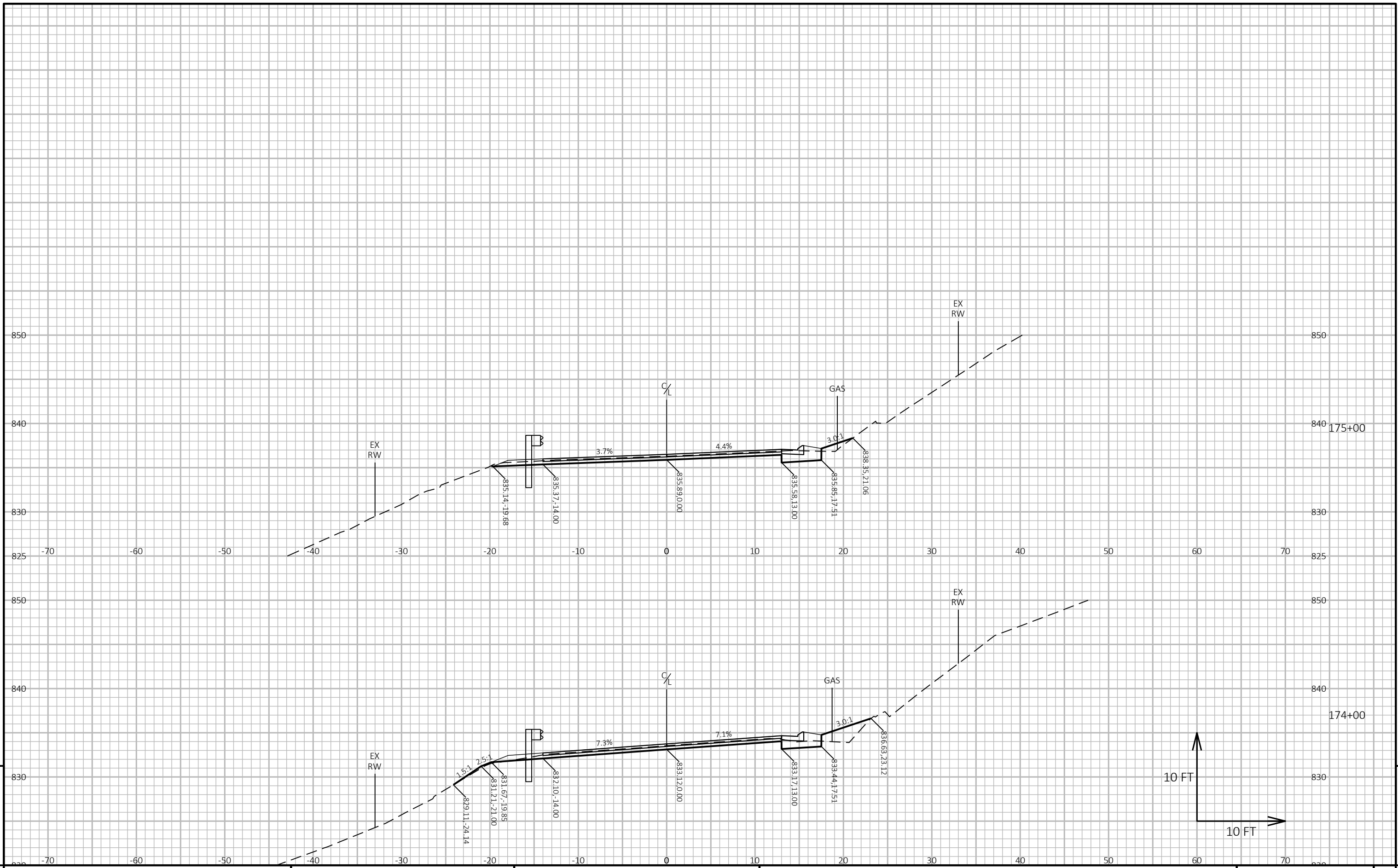
BG POST #9, LT



PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171	SHEET	E
------------------------	--------------	------------------	-------------------------	-------	---



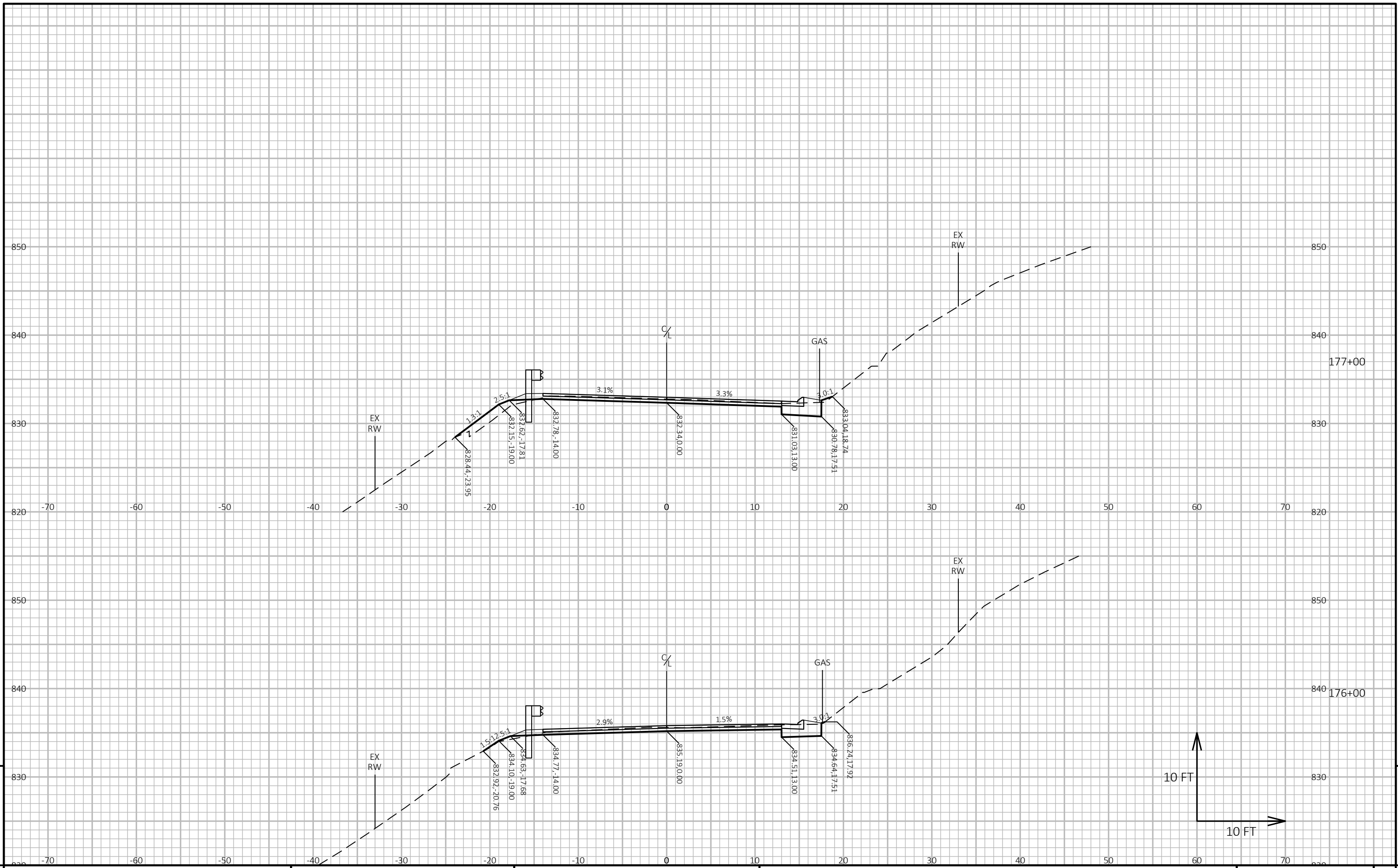
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171	SHEET	E
------------------------	--------------	------------------	-------------------------	-------	---



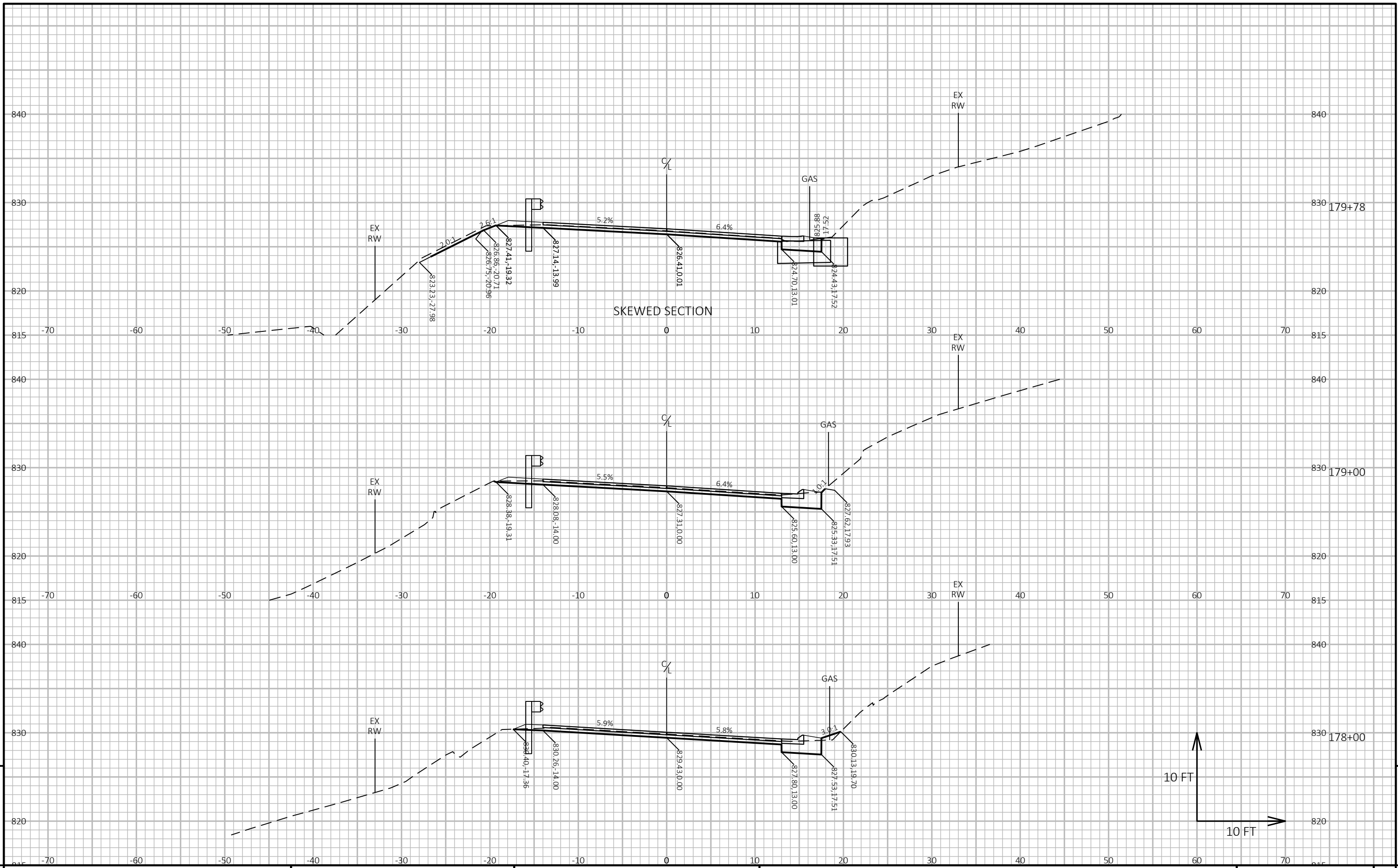
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

9

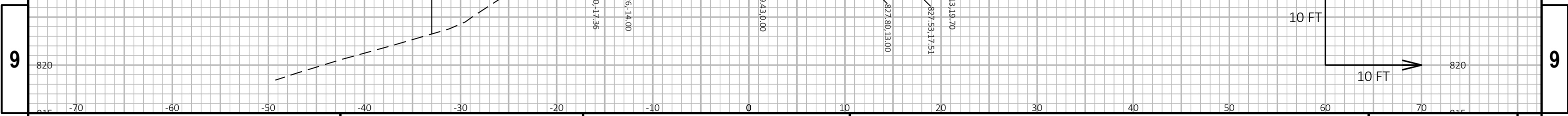
9

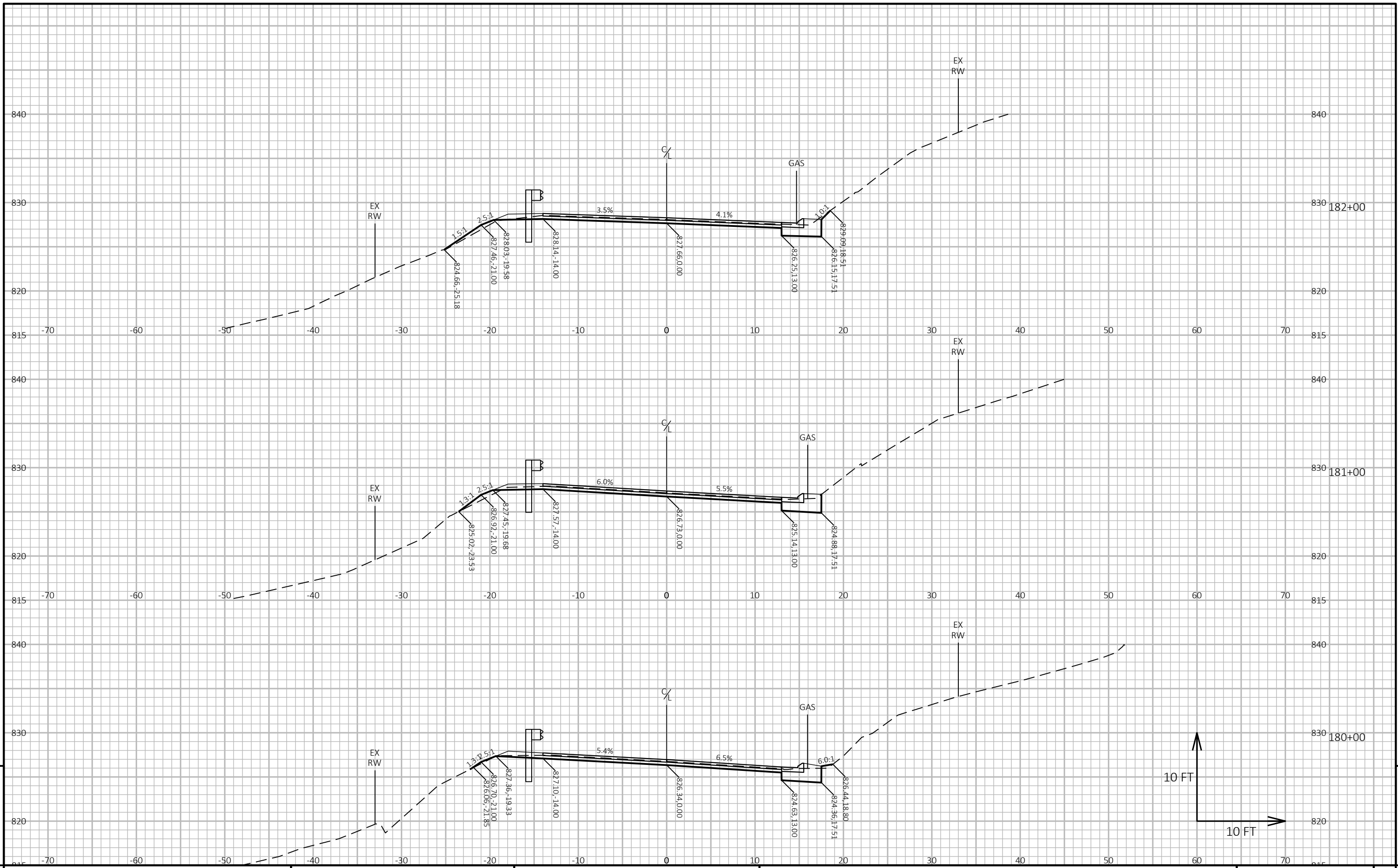


PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171	SHEET	E
------------------------	--------------	------------------	-------------------------	-------	---

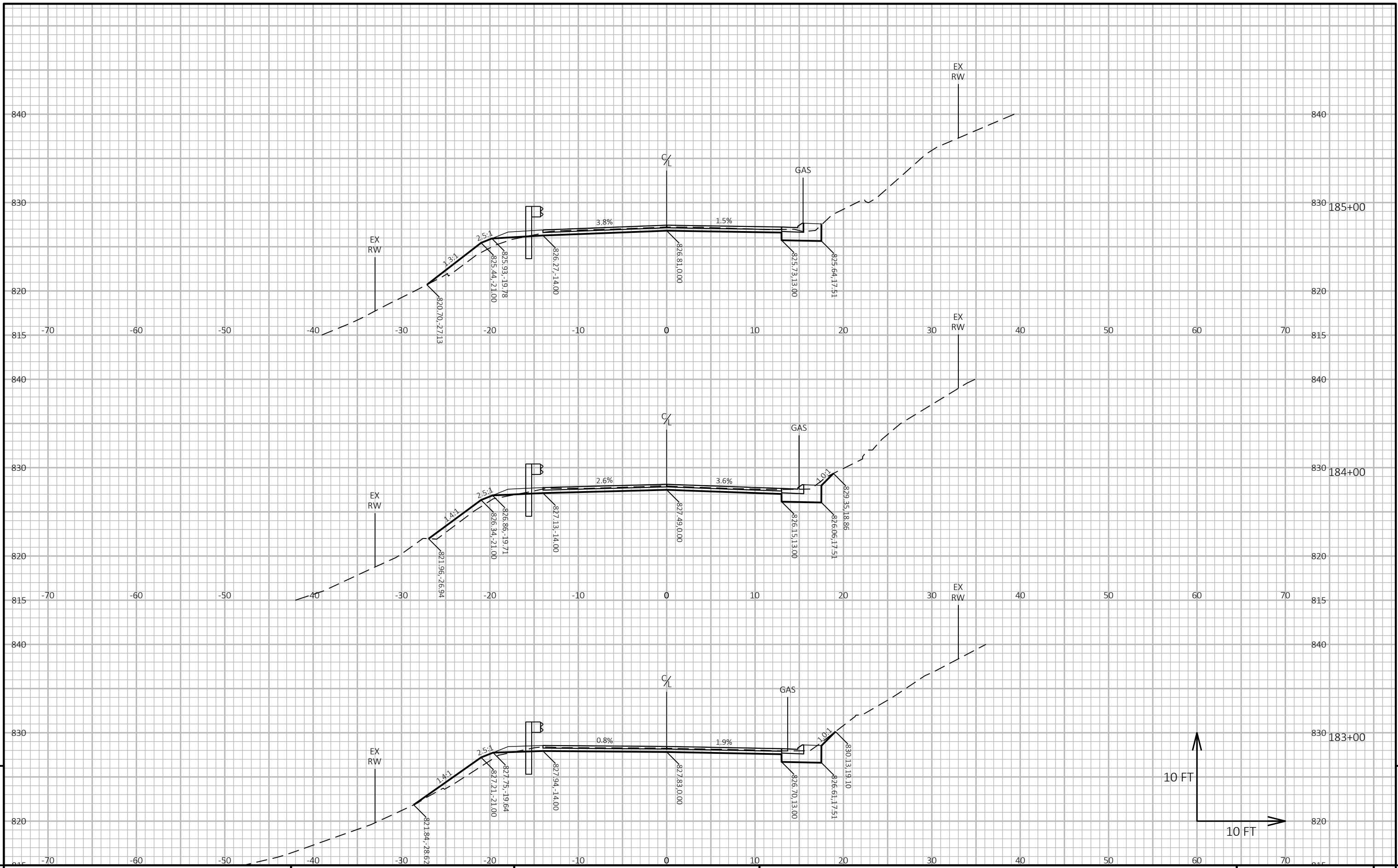


PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E





PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



PROJECT NO: 5790-02-72

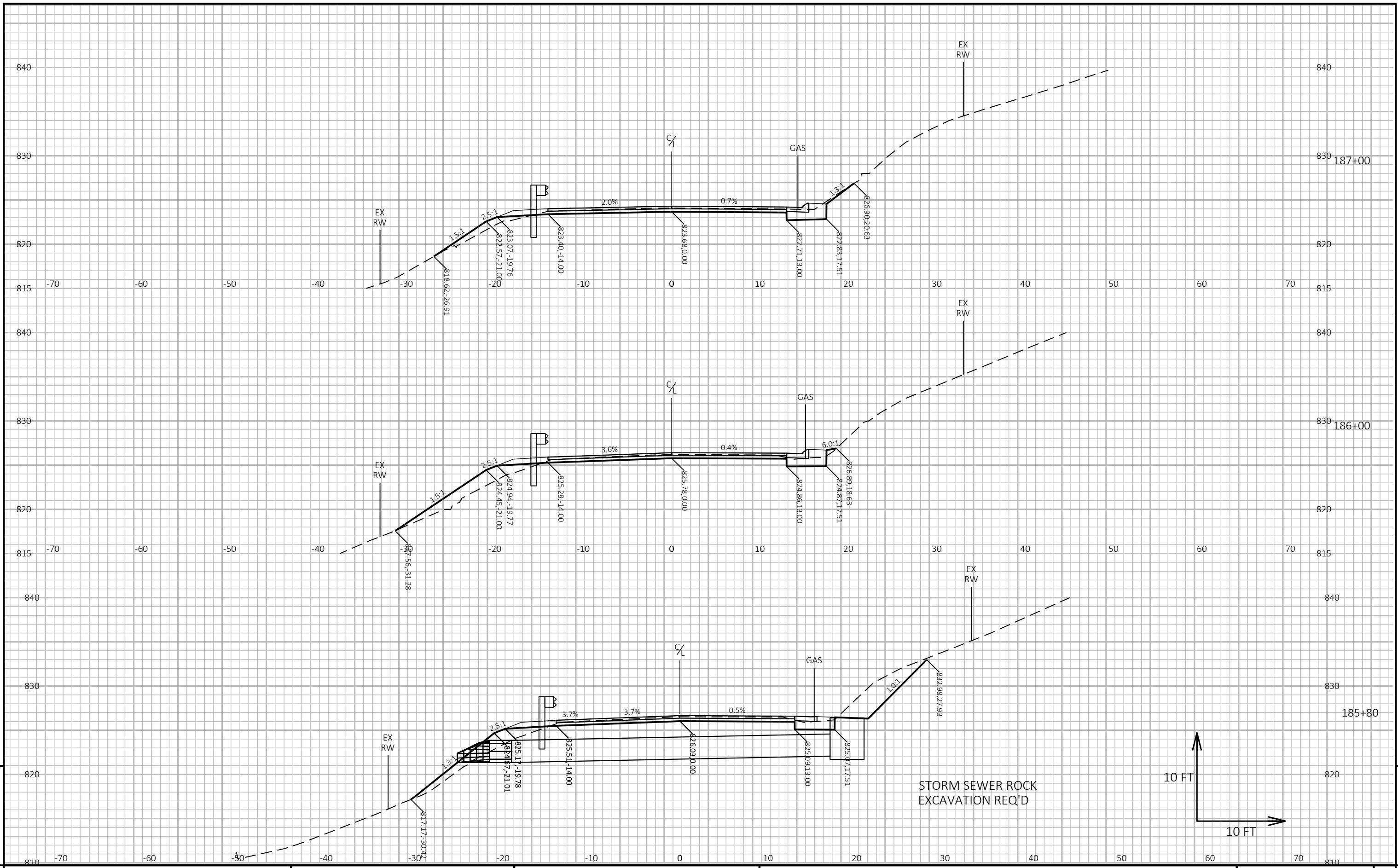
HWY: STH 171

COUNTY: CRAWFORD

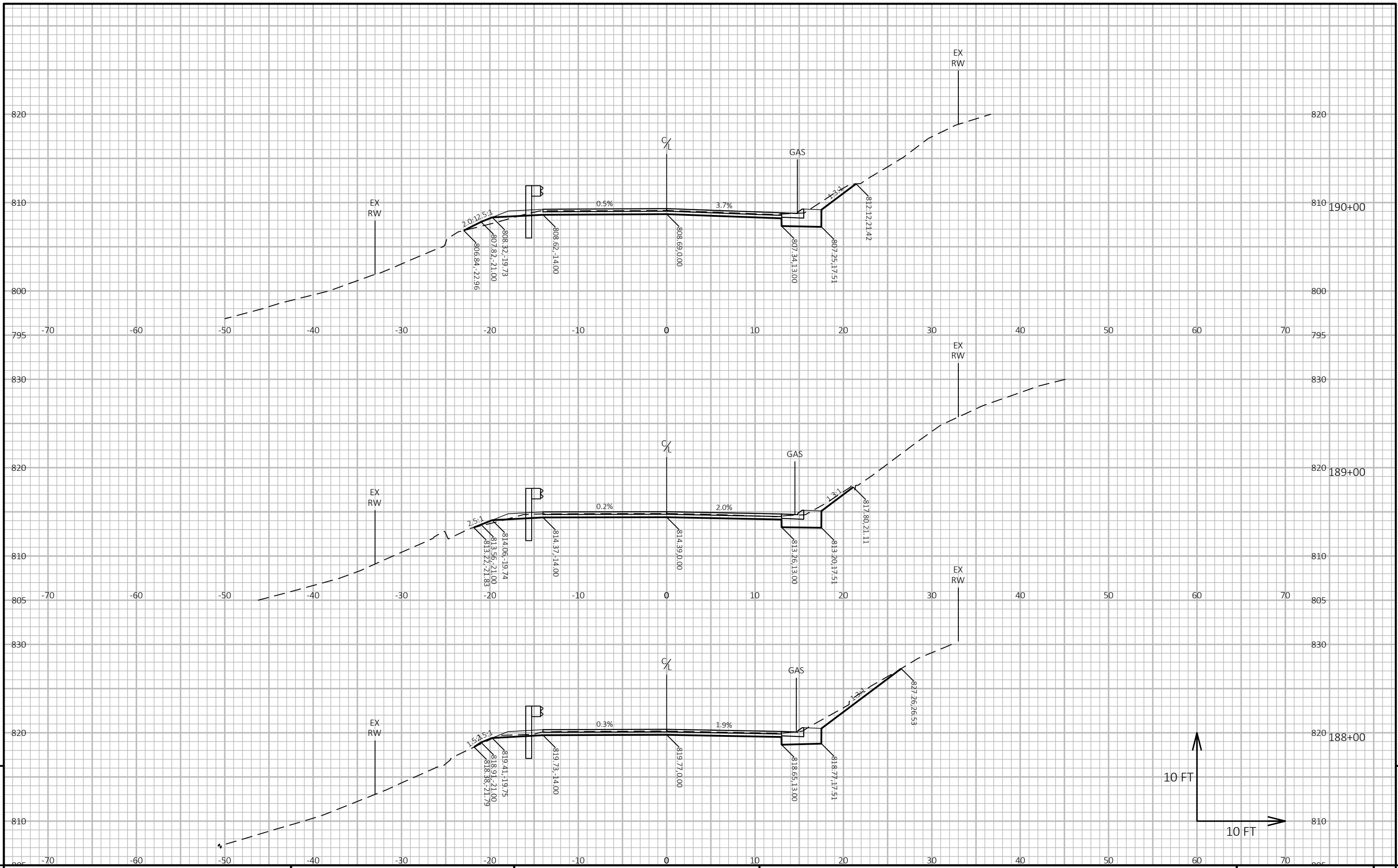
CROSS SECTIONS: STH 171

SHEET

E



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



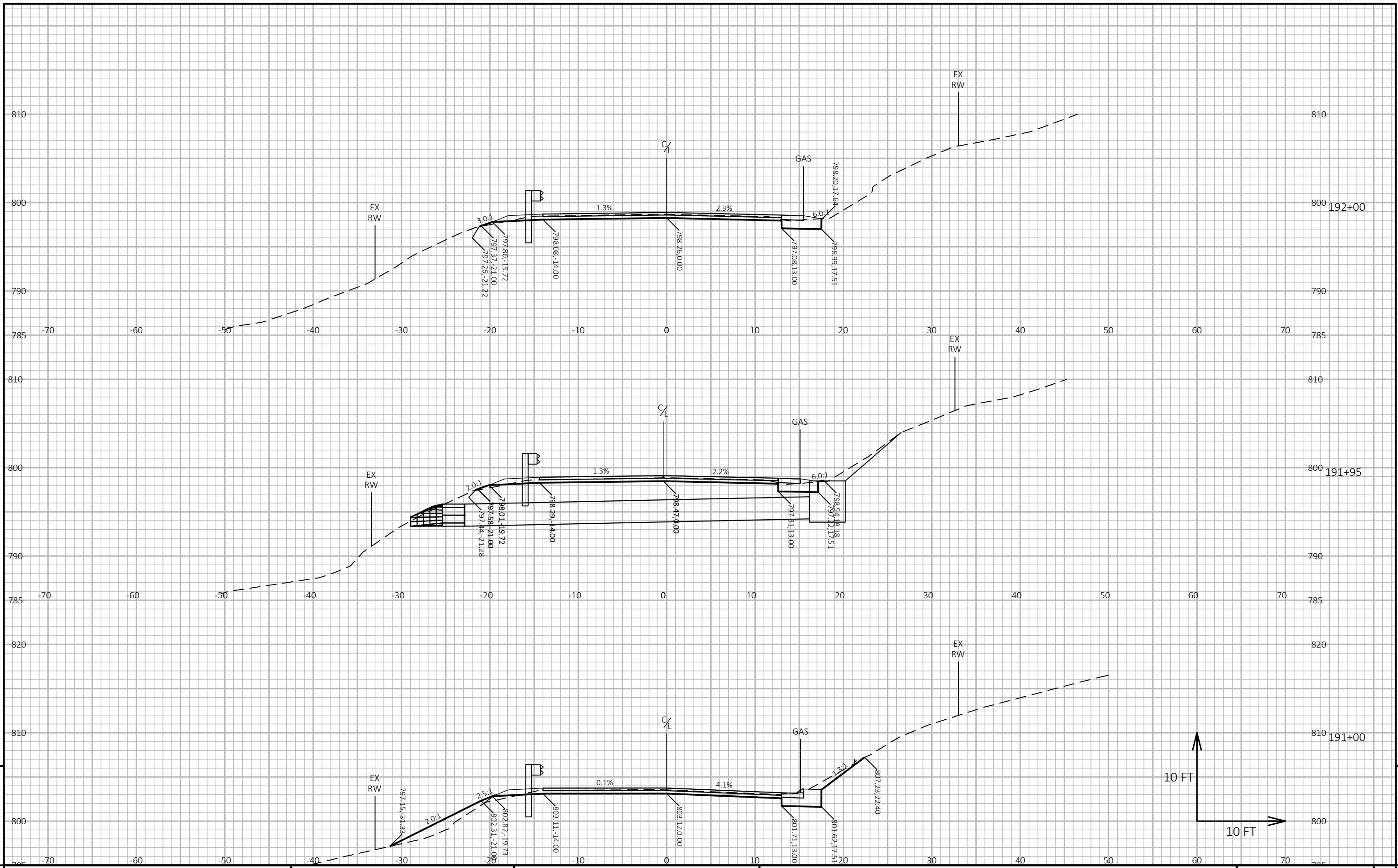
9

9

PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

FILE NAME: \\SP-PZ1.SEHINC.COM\PROJECTS\UZZ\W\WITSW\162034\5-FINAL-DSGN\51-DRAWINGS\10-CIVIL\C3D\57900202\SHEETS\PLAN\090102_XS.DWG PLOT DATE: 7/19/2024 11:36 AM PLOT BY: SAVANNAH STEHN PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 24



PROJECT NO: 5790-02-72

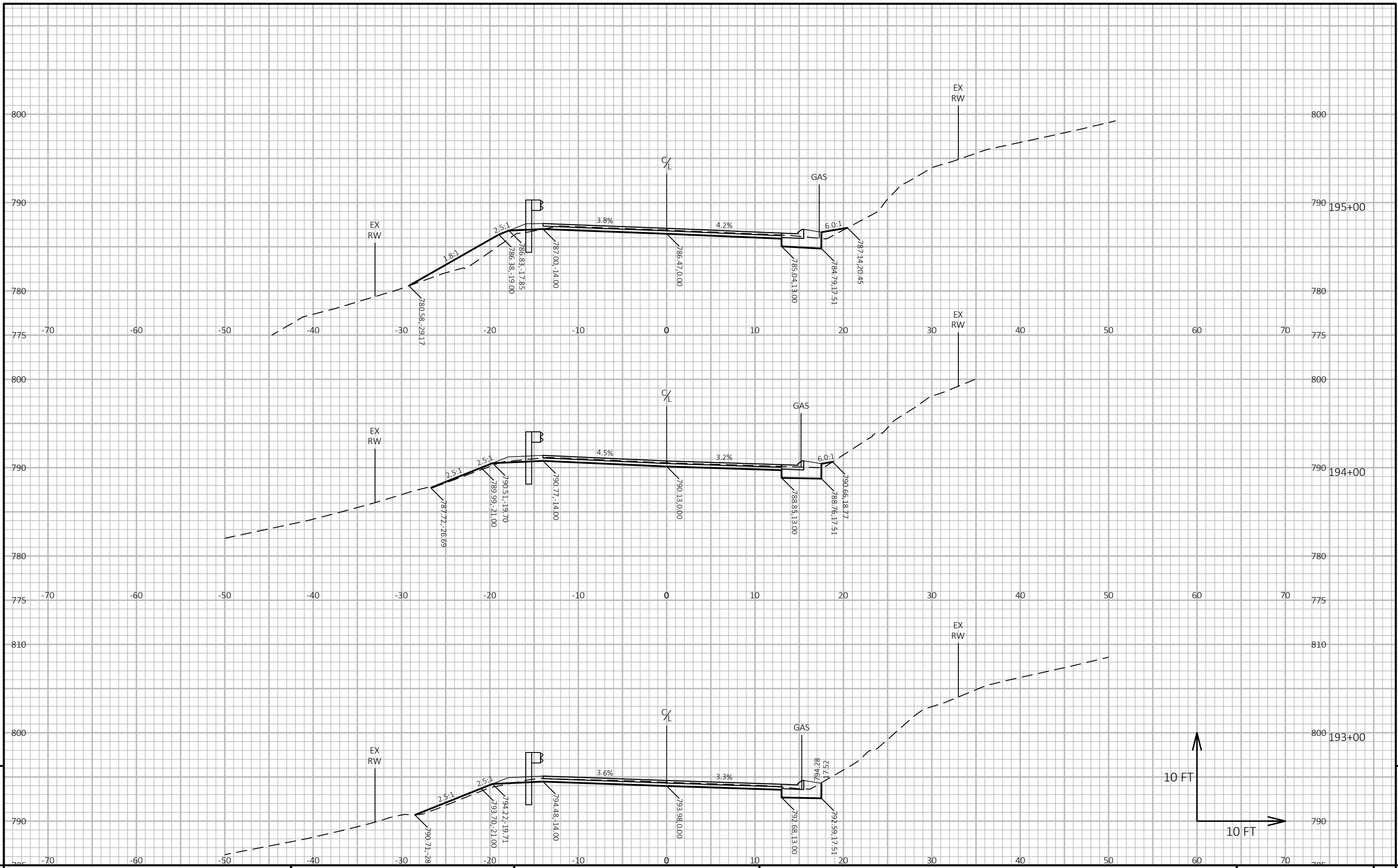
HWY: STH 171

COUNTY: CRAWFORD

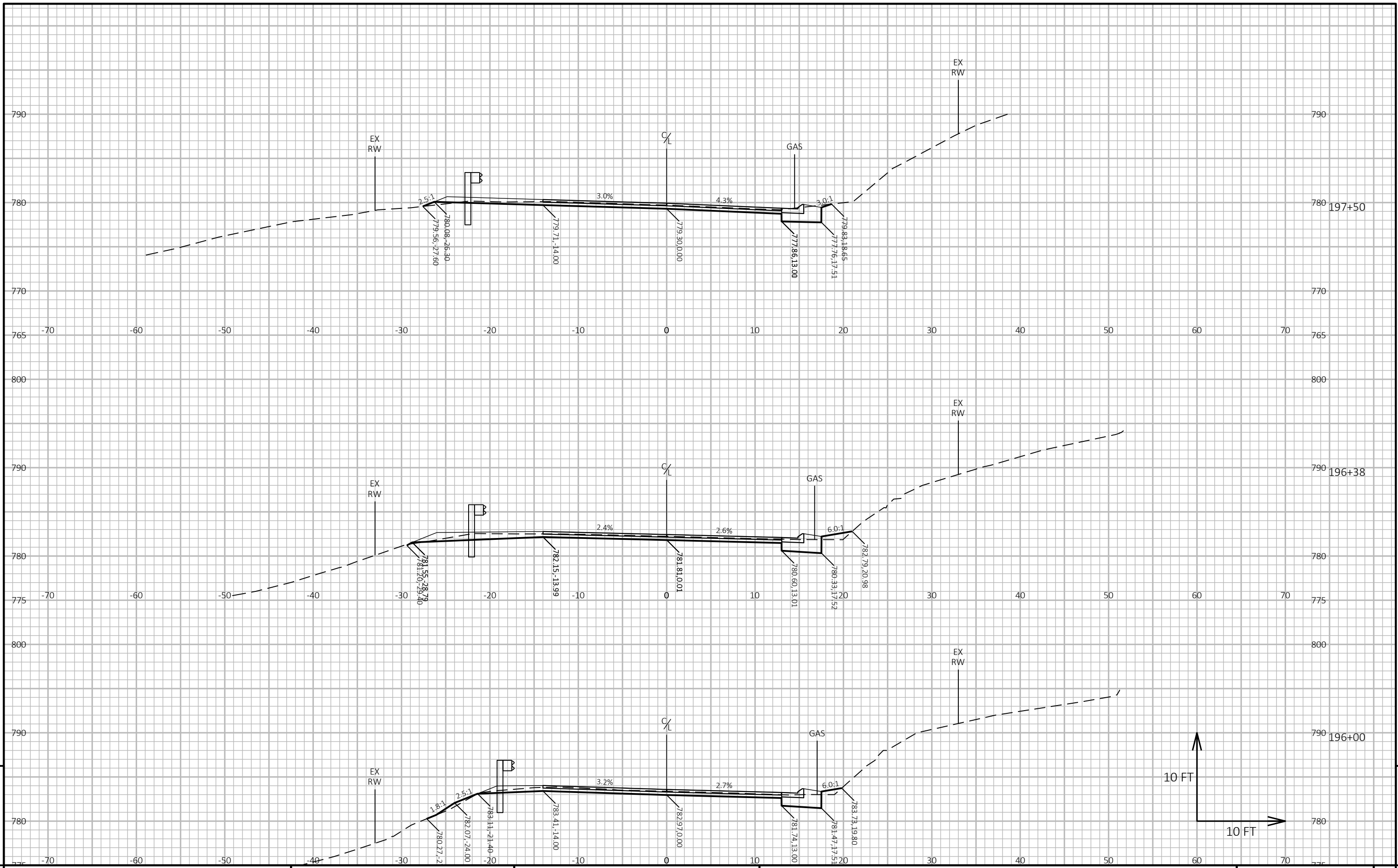
CROSS SECTIONS: STH 171

SHEET

E



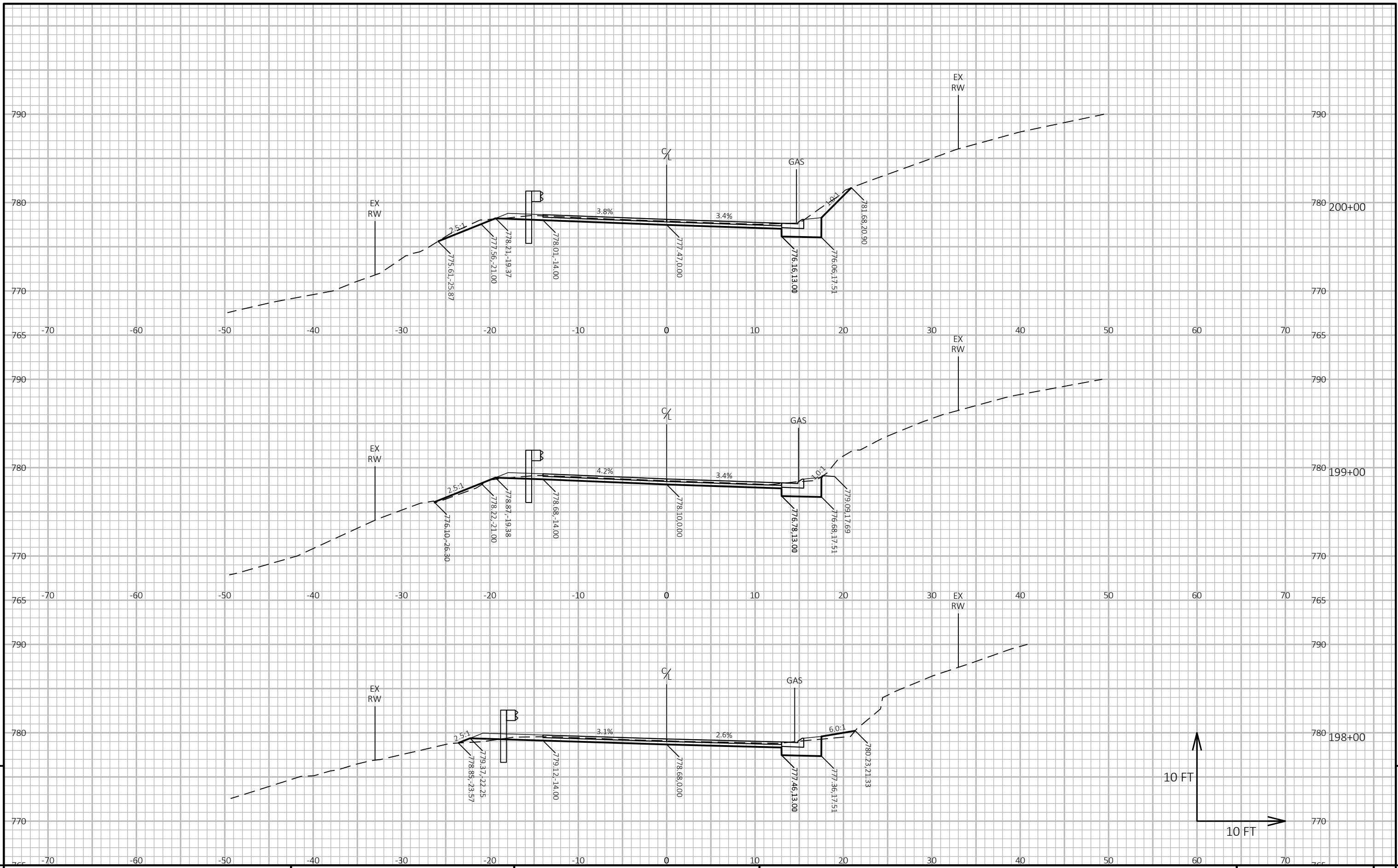
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

9

9



PROJECT NO: 5790-02-72

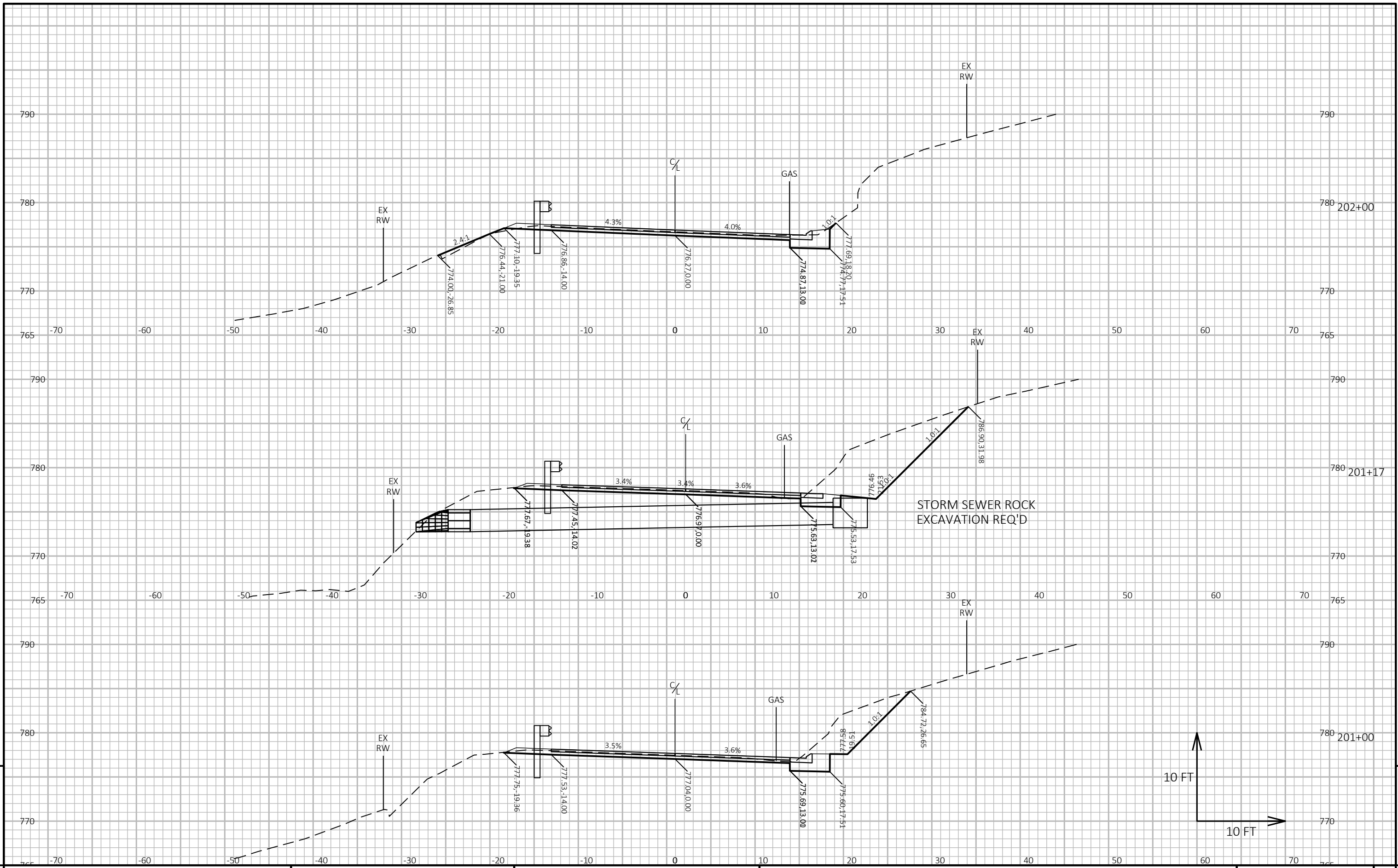
HWY: STH 171

COUNTY: CRAWFORD

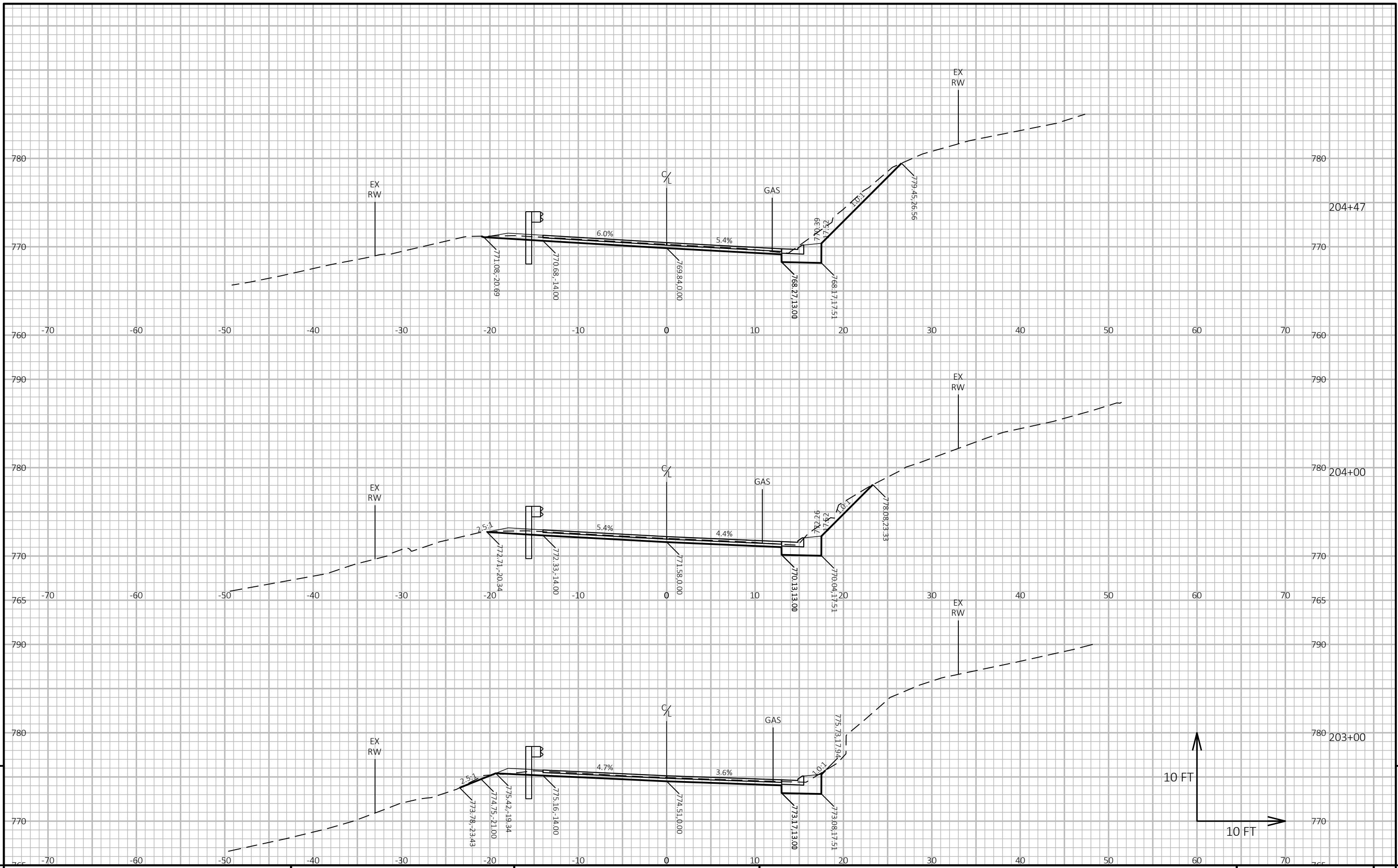
CROSS SECTIONS: STH 171

SHEET

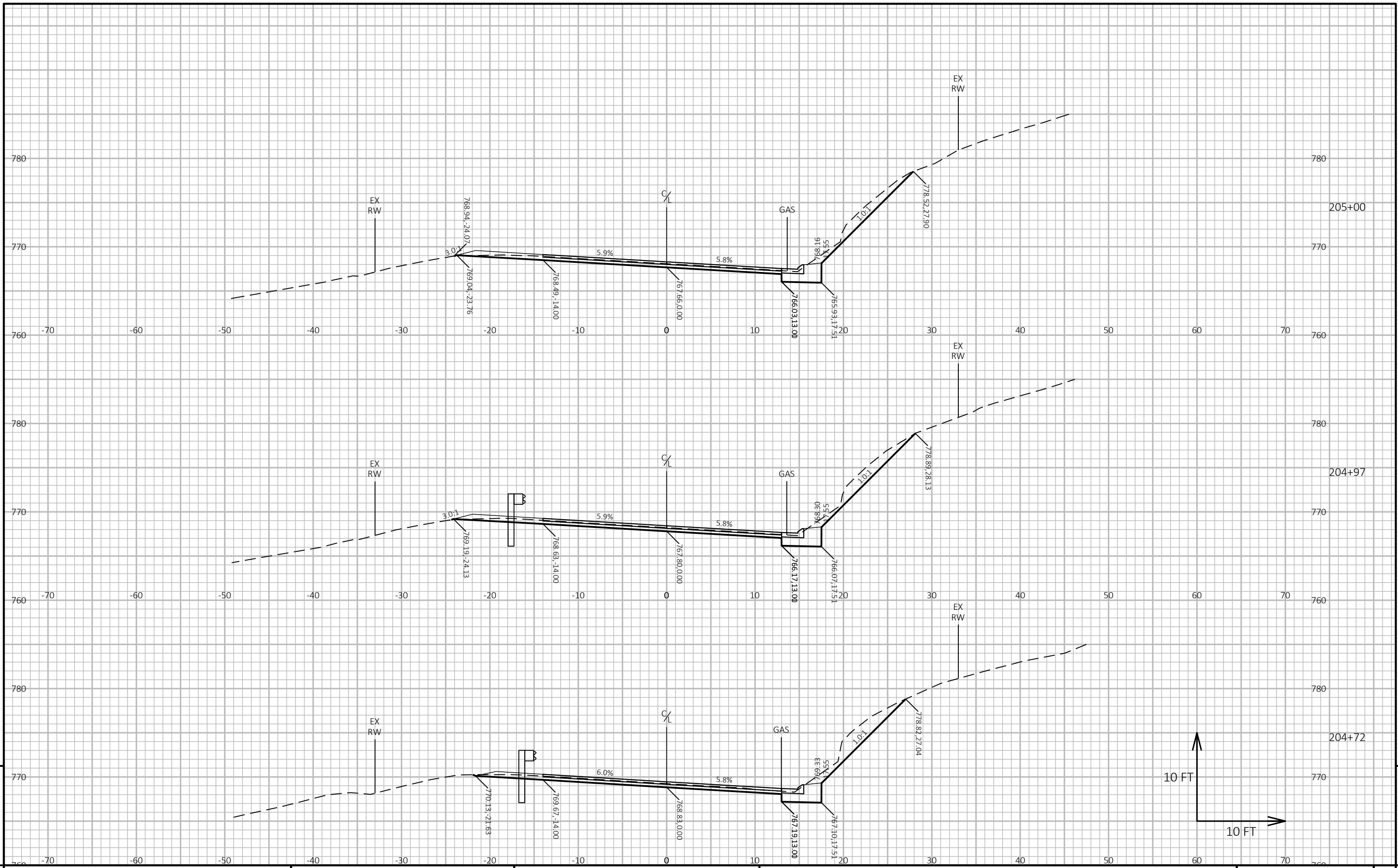
E



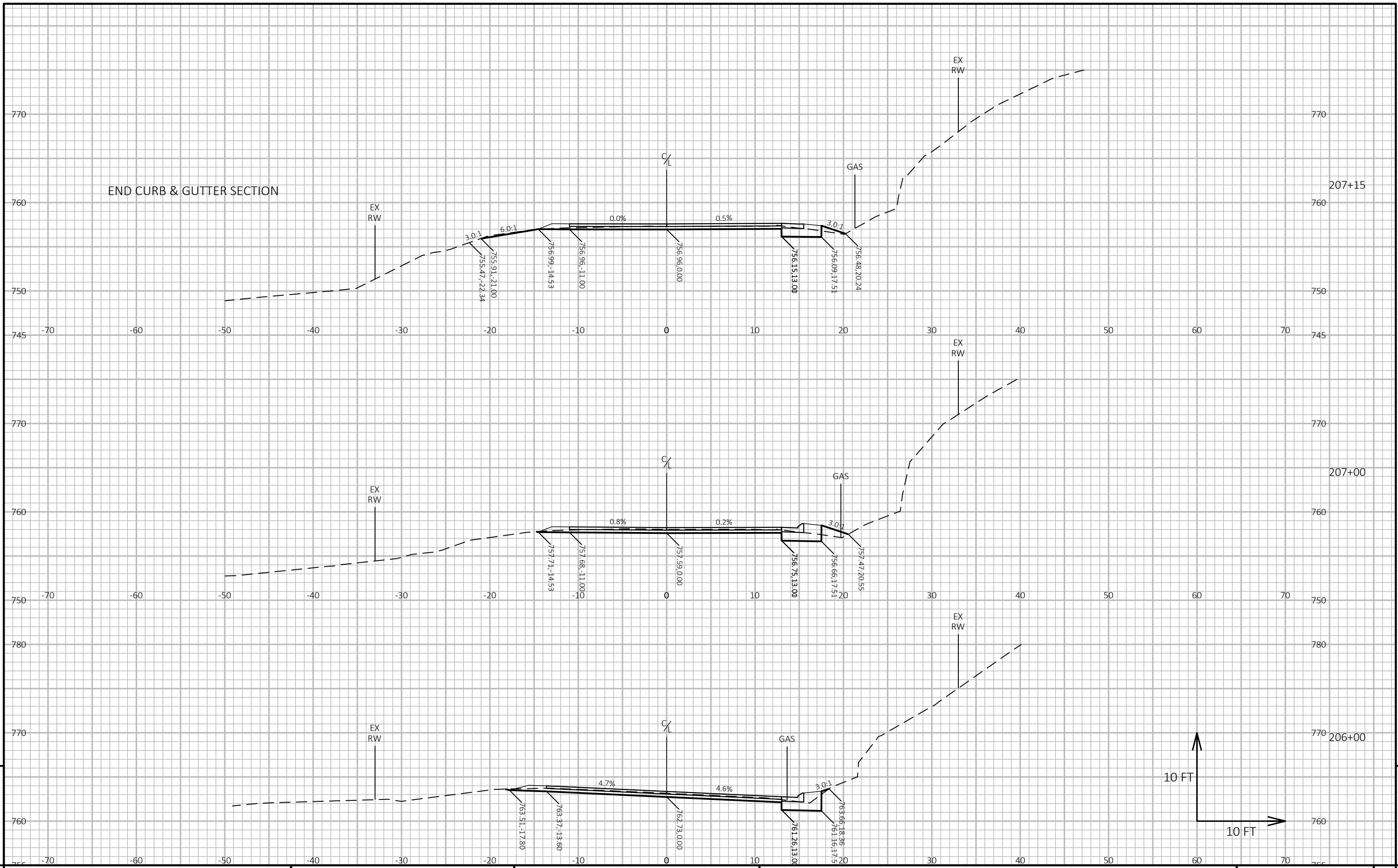
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171	SHEET	E
------------------------	--------------	------------------	-------------------------	-------	---



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



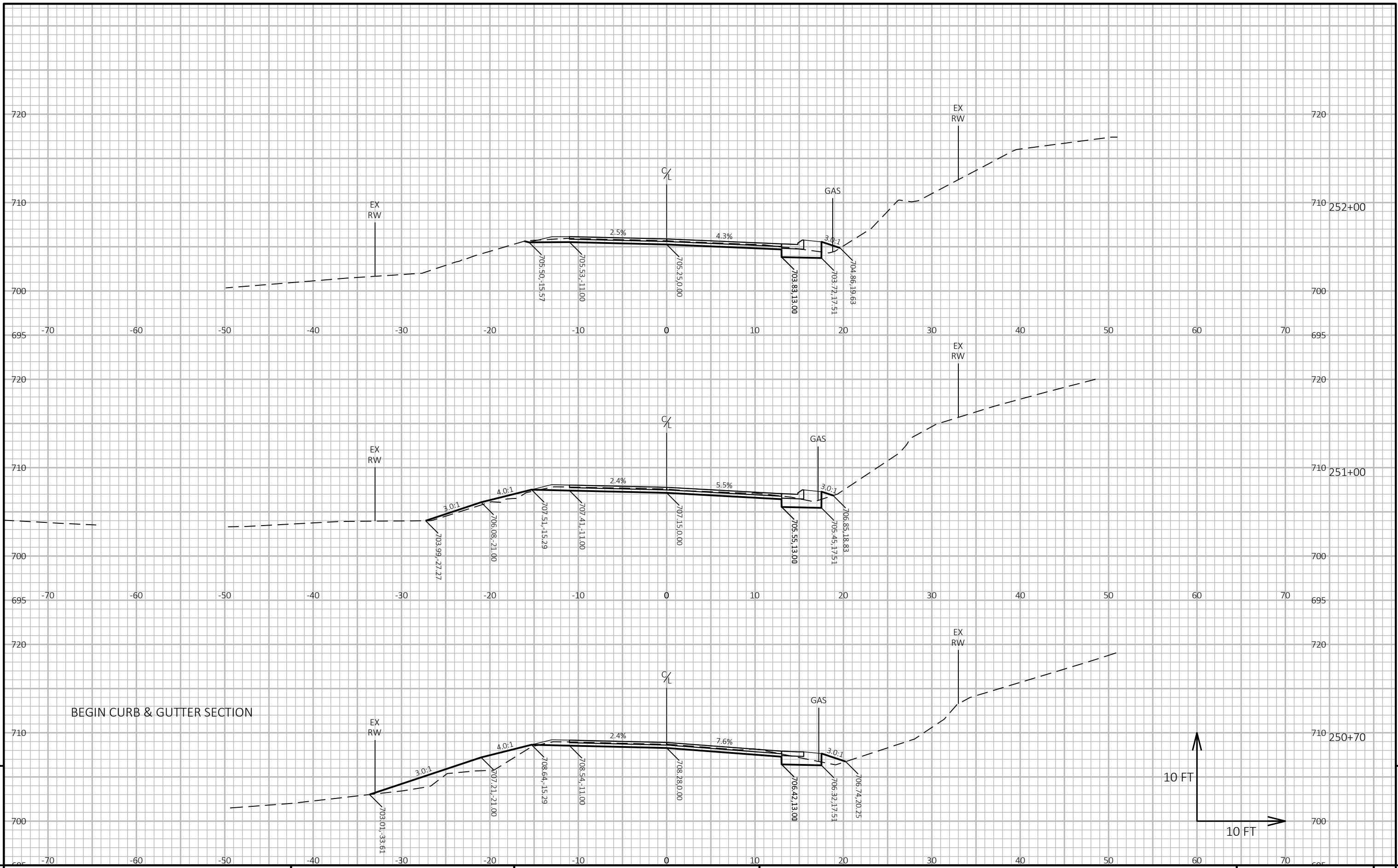
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



9

9

PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171	SHEET E
------------------------	--------------	------------------	-------------------------	---------



PROJECT NO: 5790-02-72

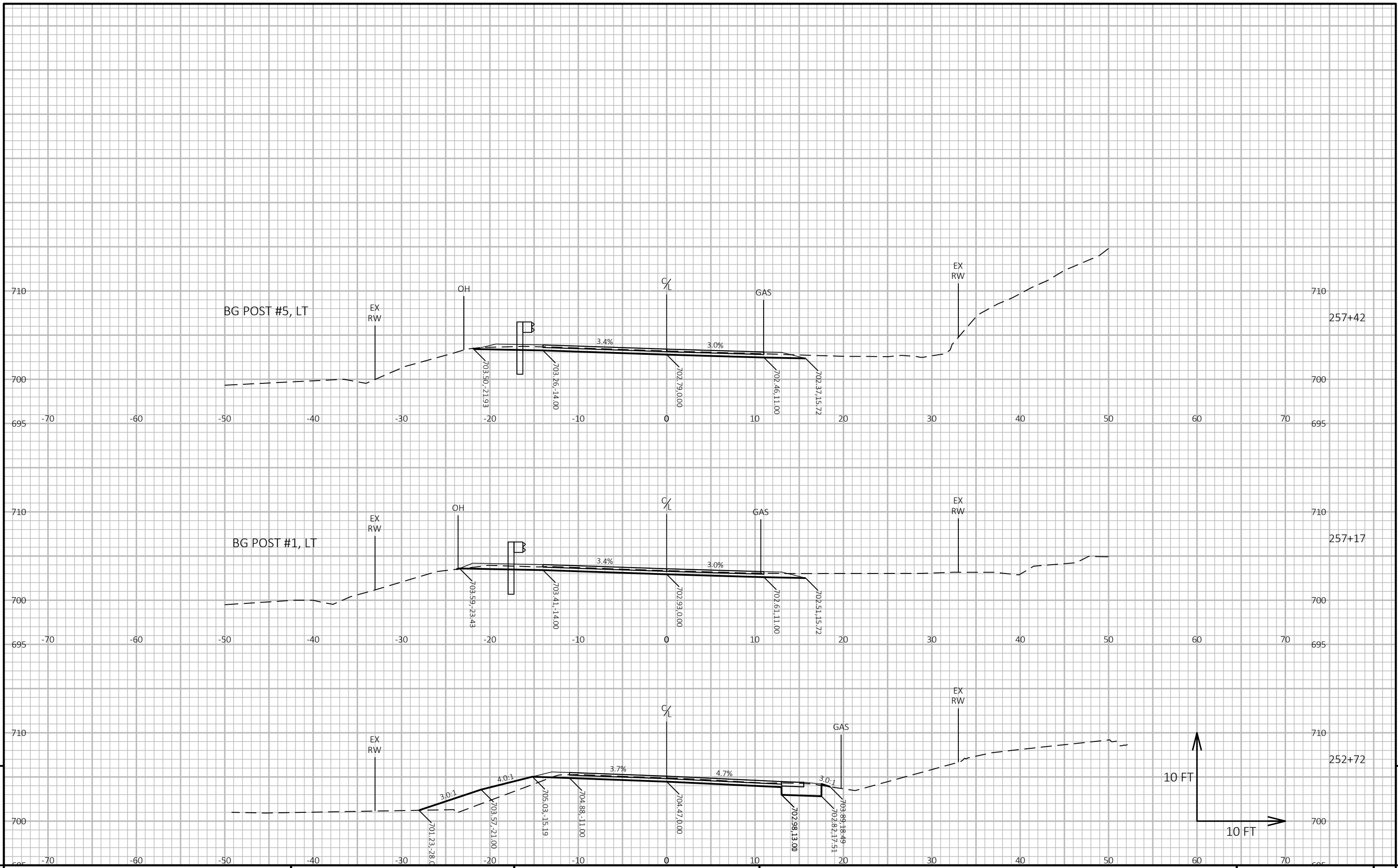
HWY: STH 171

COUNTY: CRAWFORD

CROSS SECTIONS: STH 171

SHEET

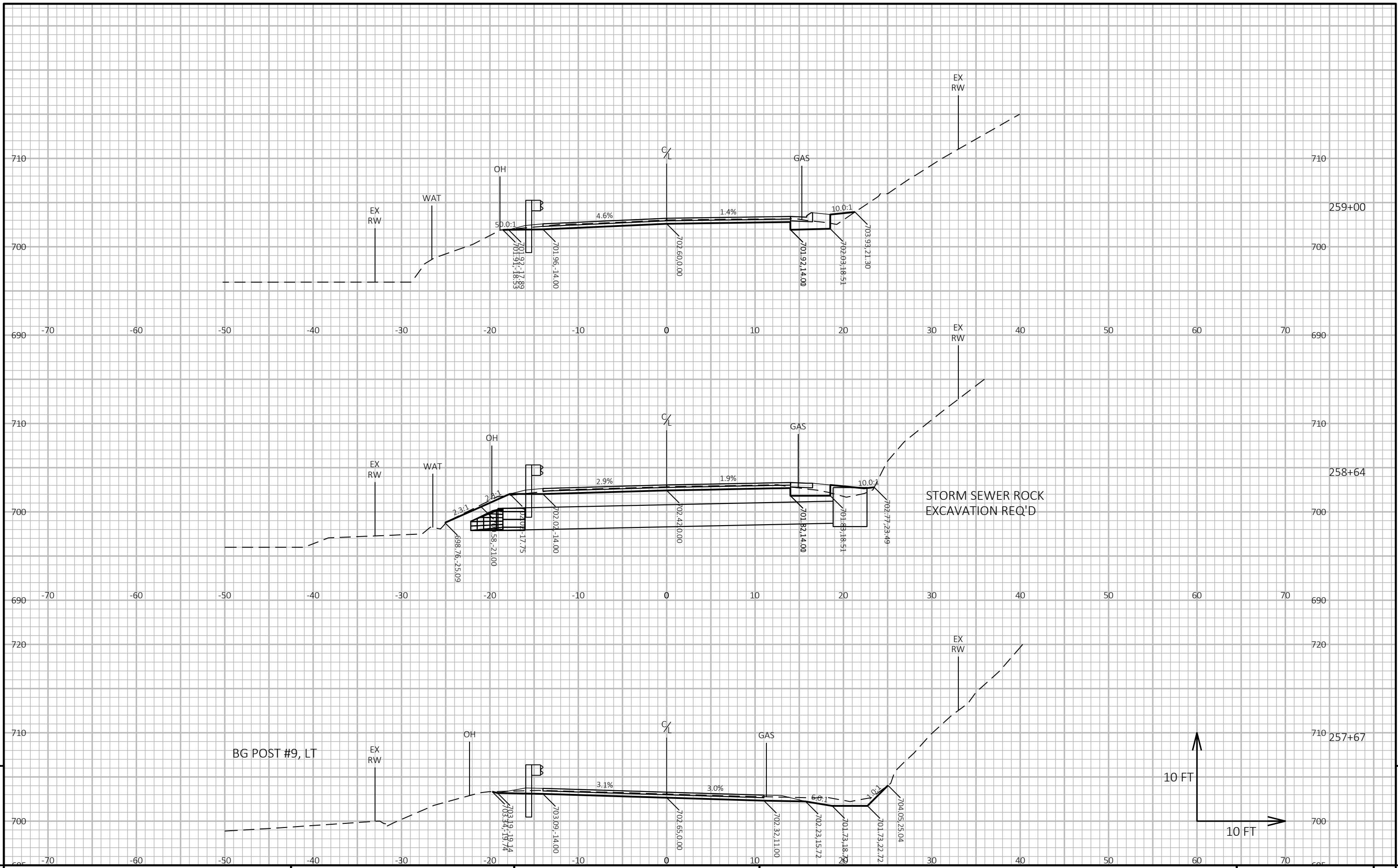
E



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

9

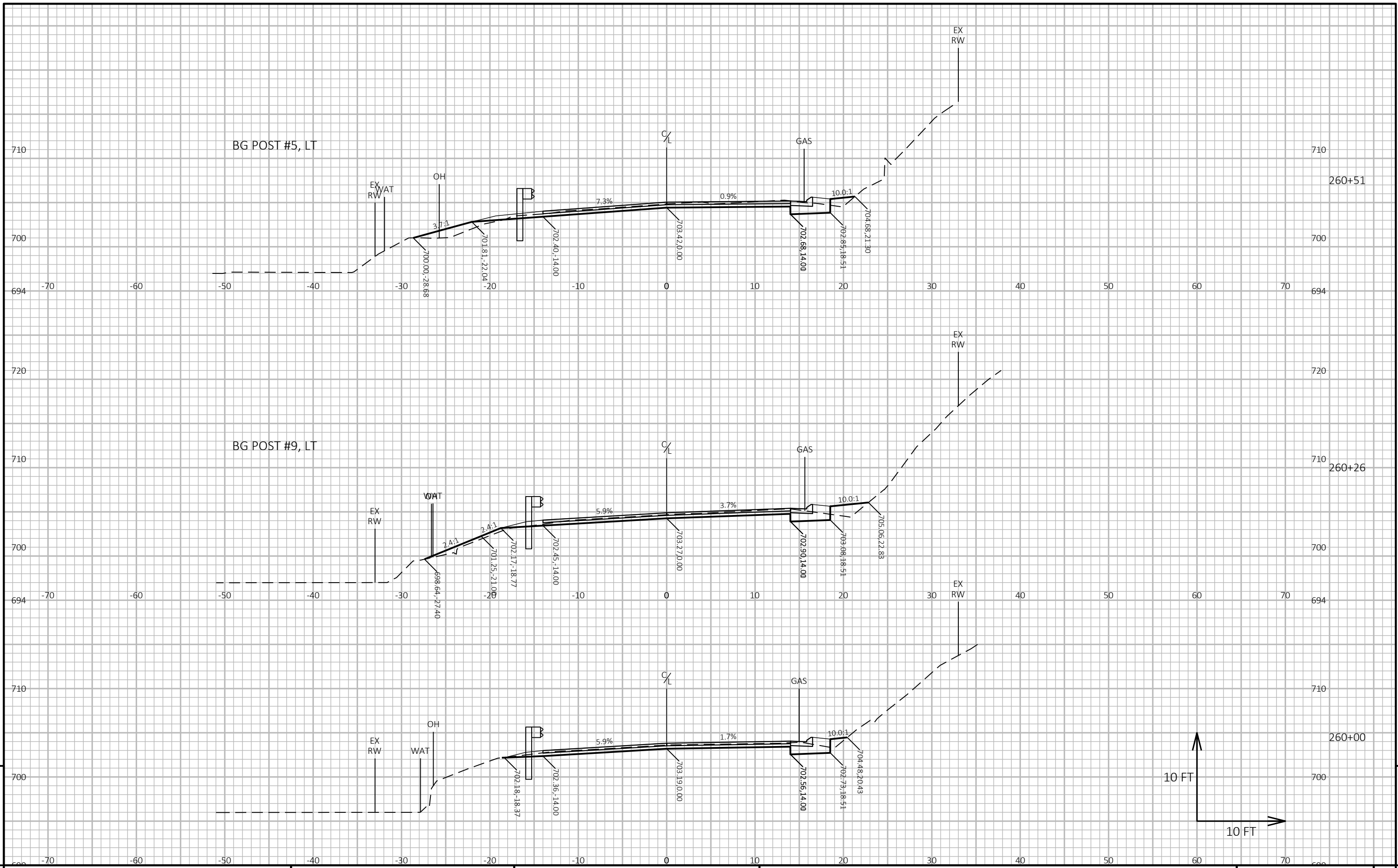
9



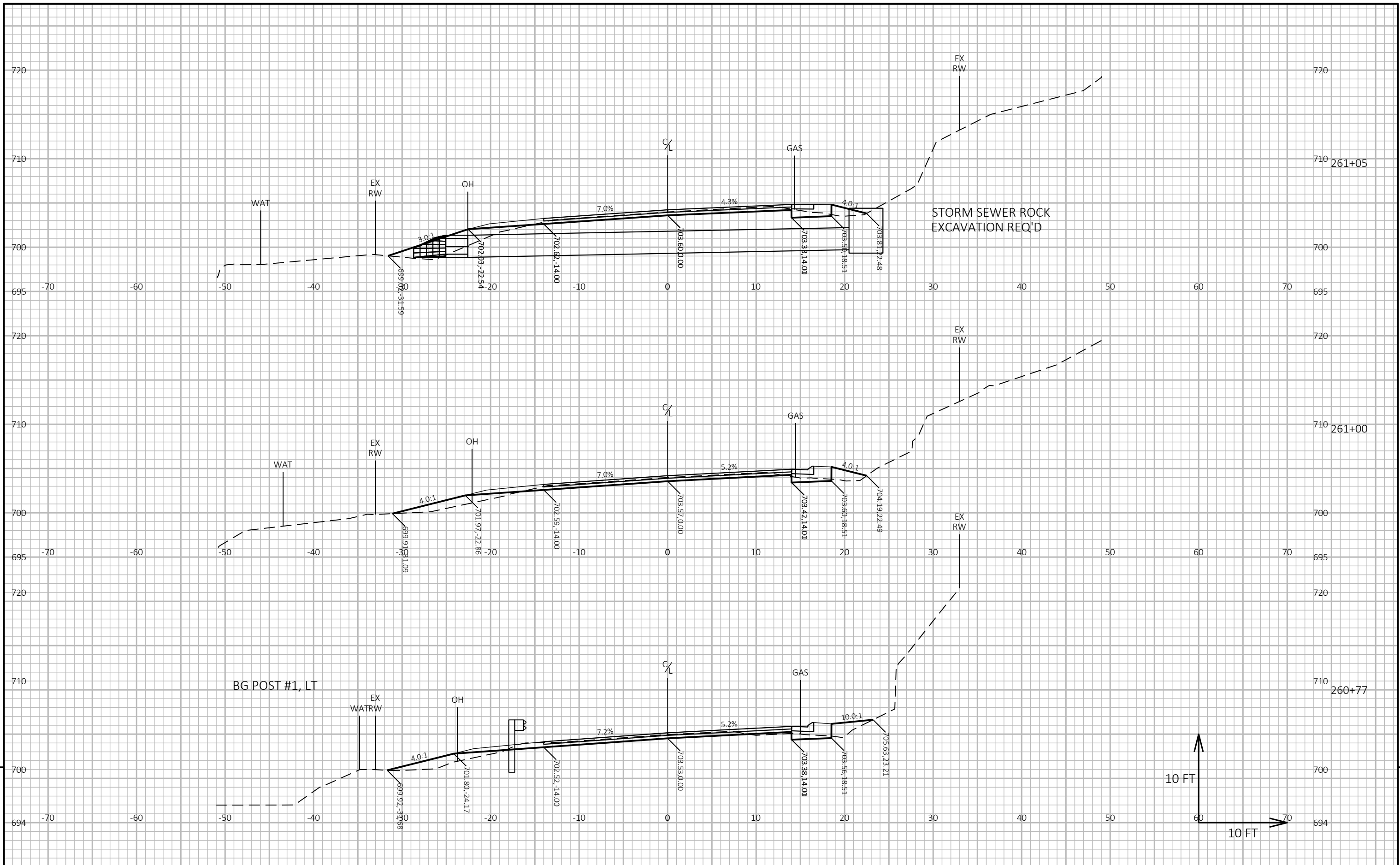
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

9

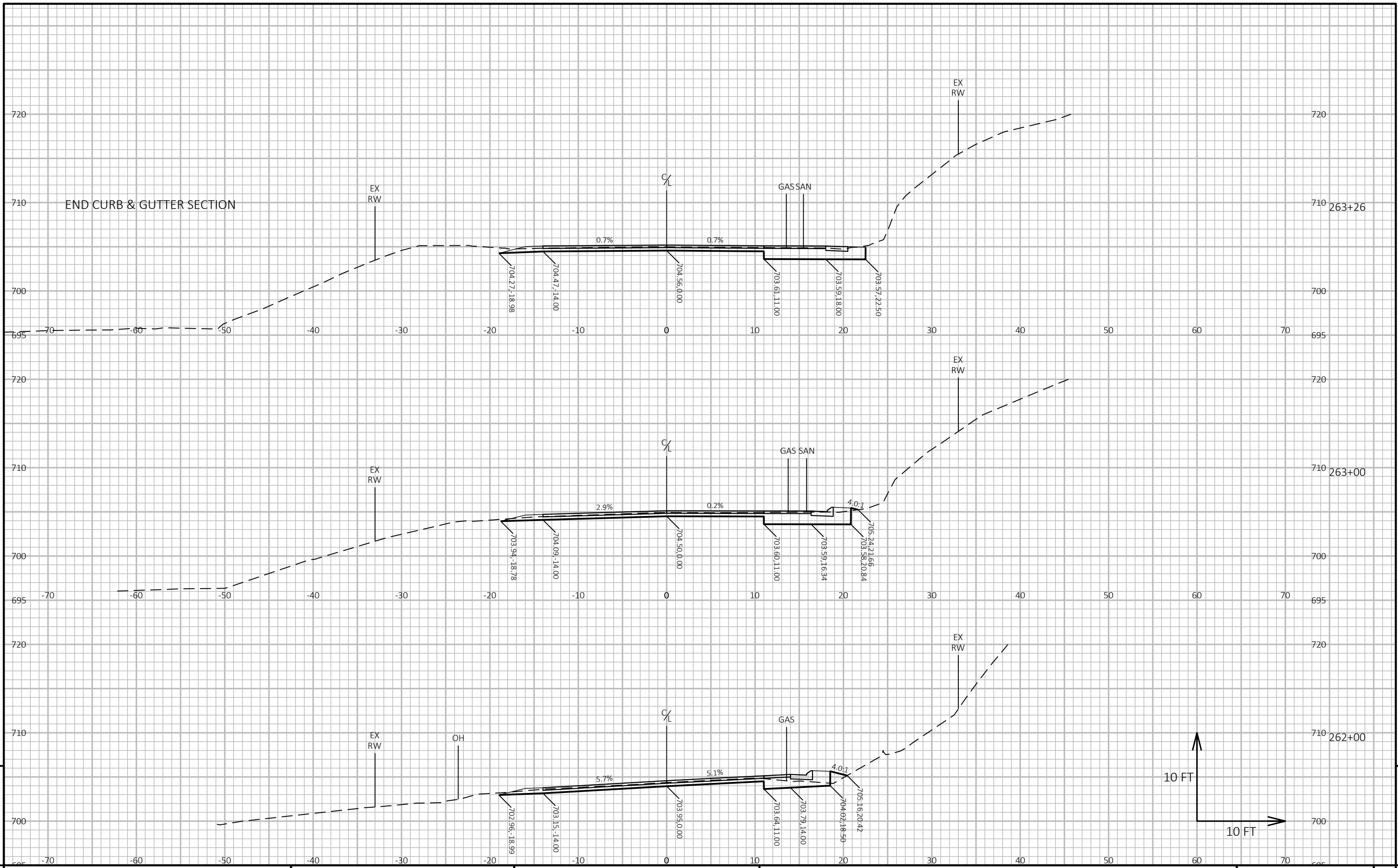
9



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171	SHEET	9
------------------------	--------------	------------------	-------------------------	-------	----------



PROJECT NO: 5790-02-72

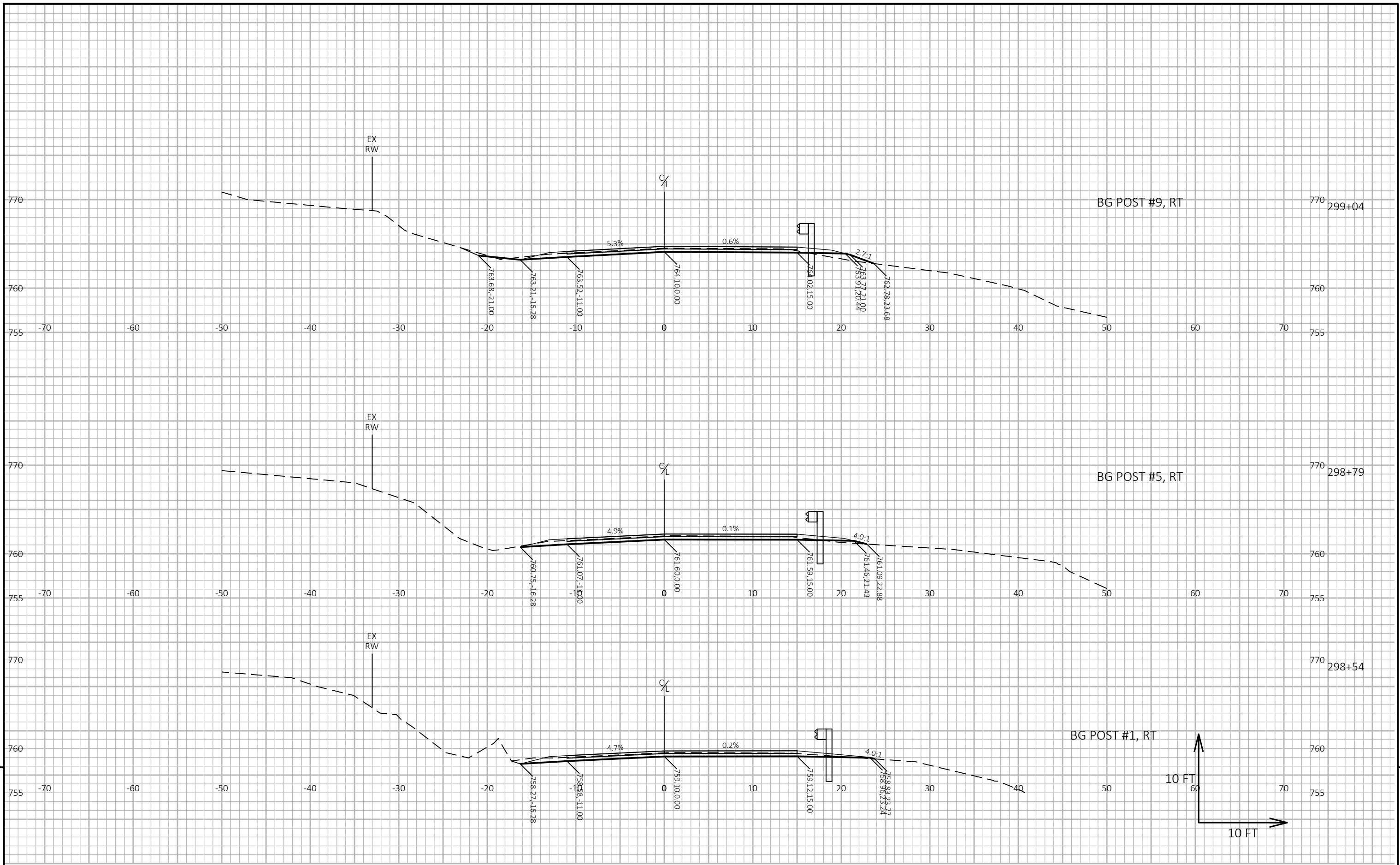
HWY: STH 171

COUNTY: CRAWFORD

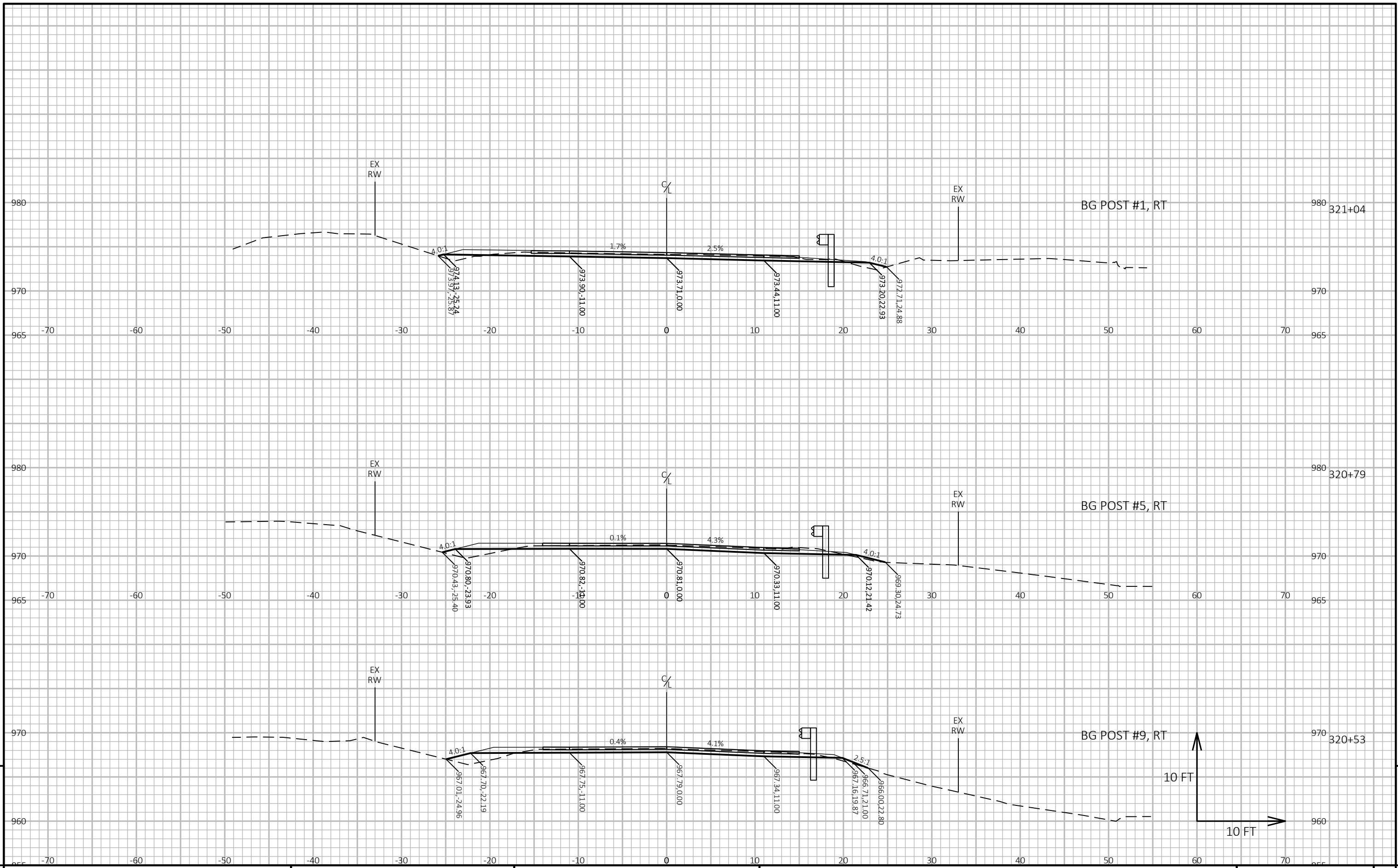
CROSS SECTIONS: STH 171

SHEET

E



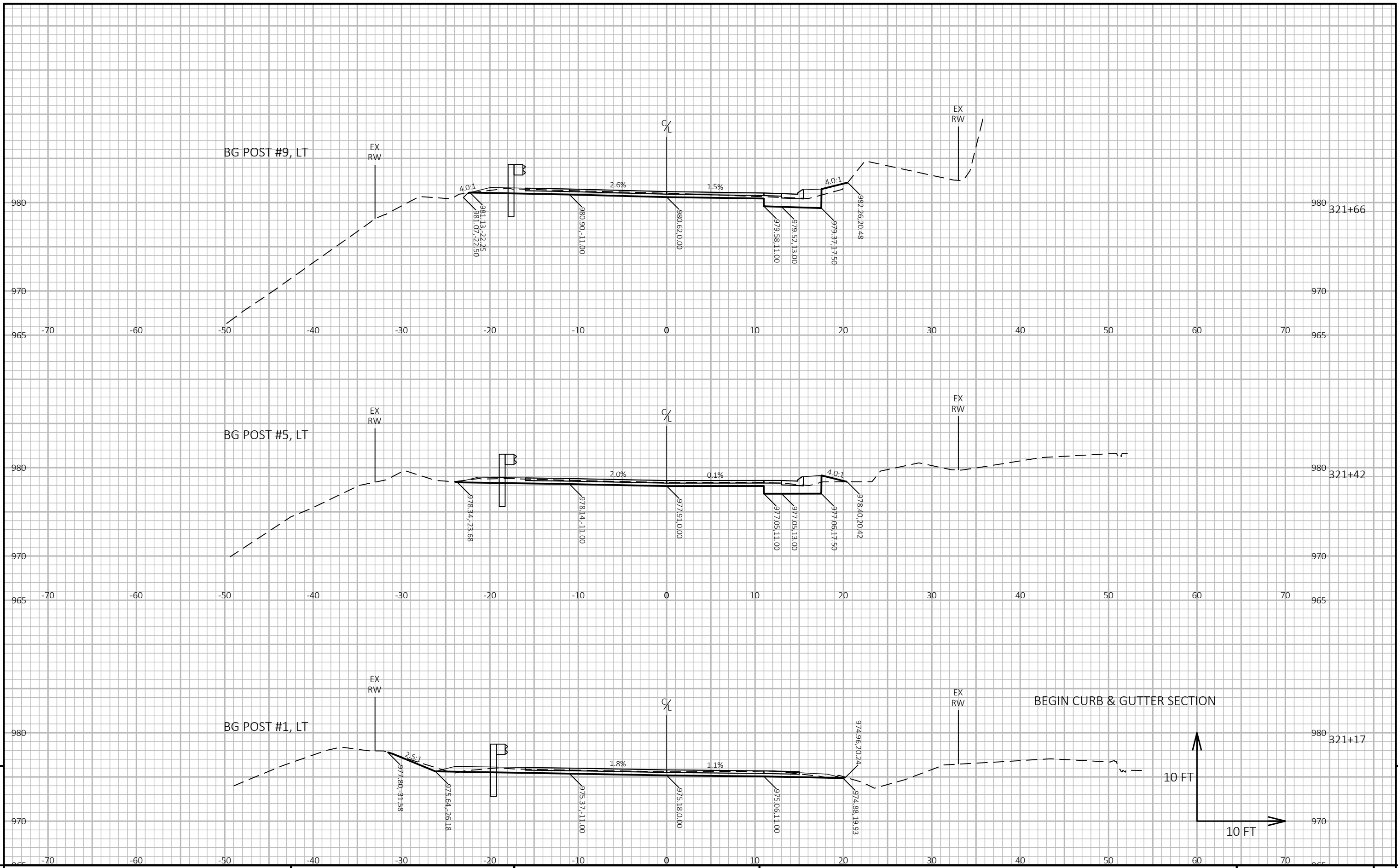
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET: 9



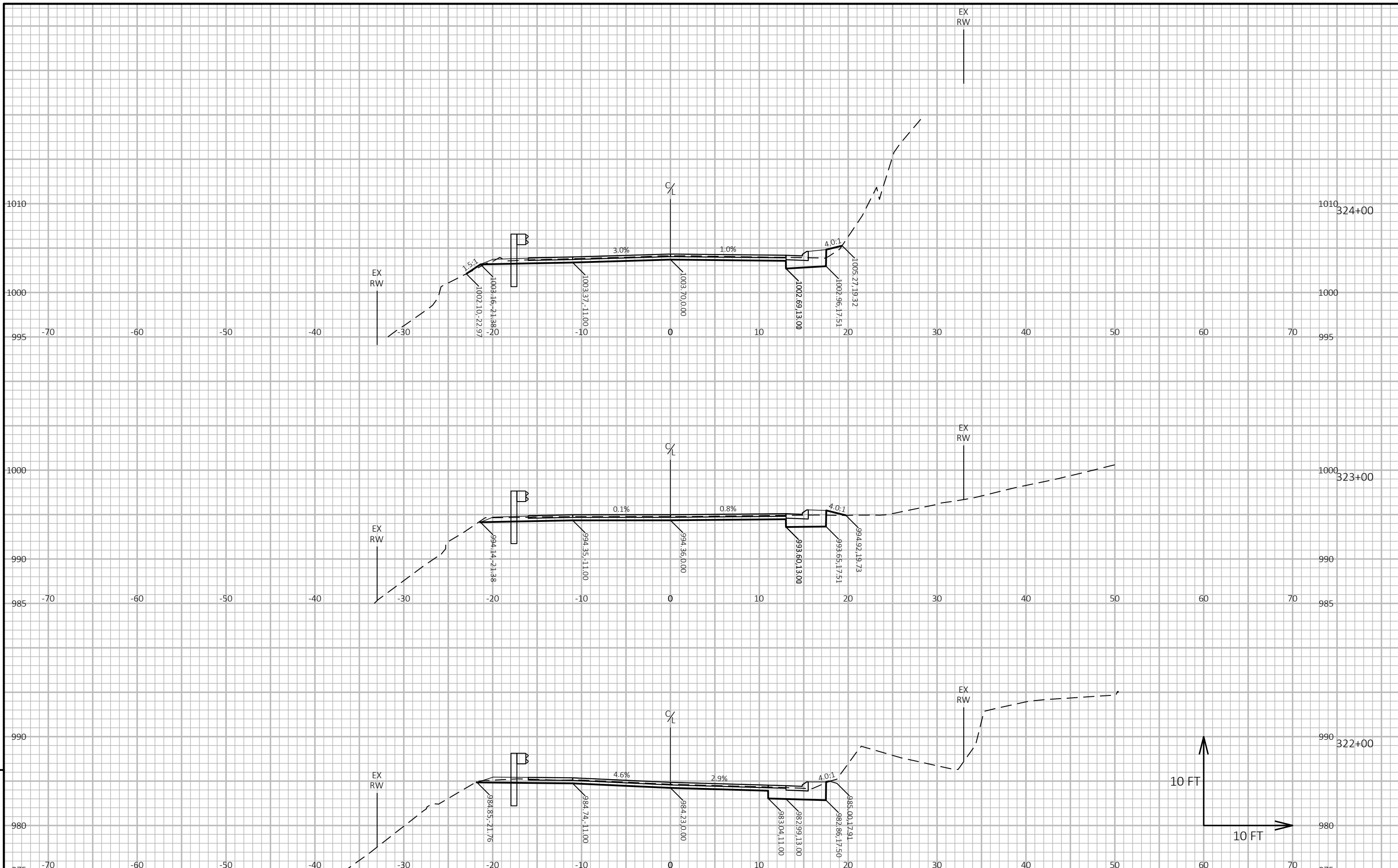
9

9

PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171	SHEET	E
------------------------	--------------	------------------	-------------------------	-------	---



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET 9



PROJECT NO: 5790-02-72

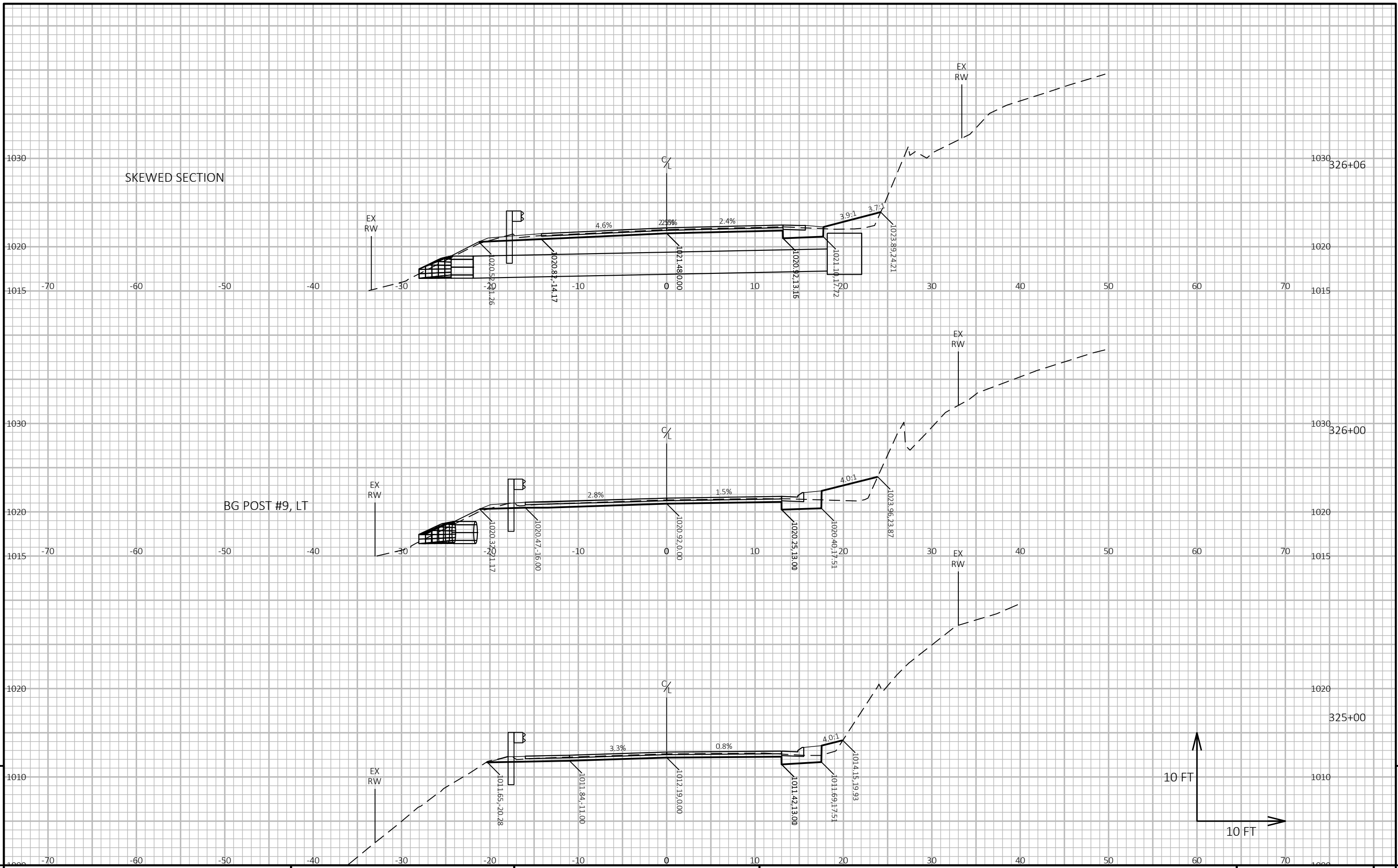
HWY: STH 171

COUNTY: CRAWFORD

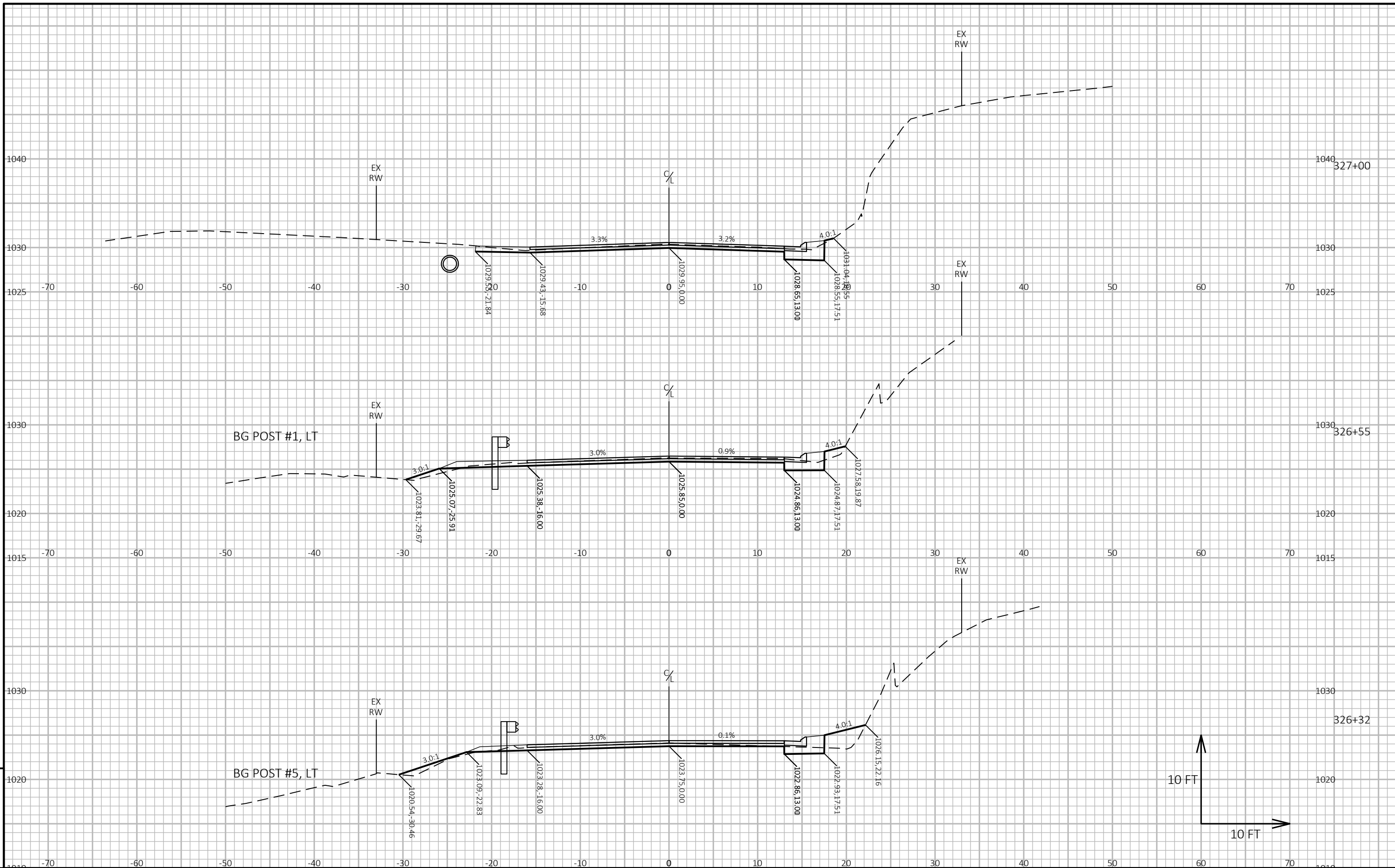
CROSS SECTIONS: STH 171

SHEET

E



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E



PROJECT NO: 5790-02-72

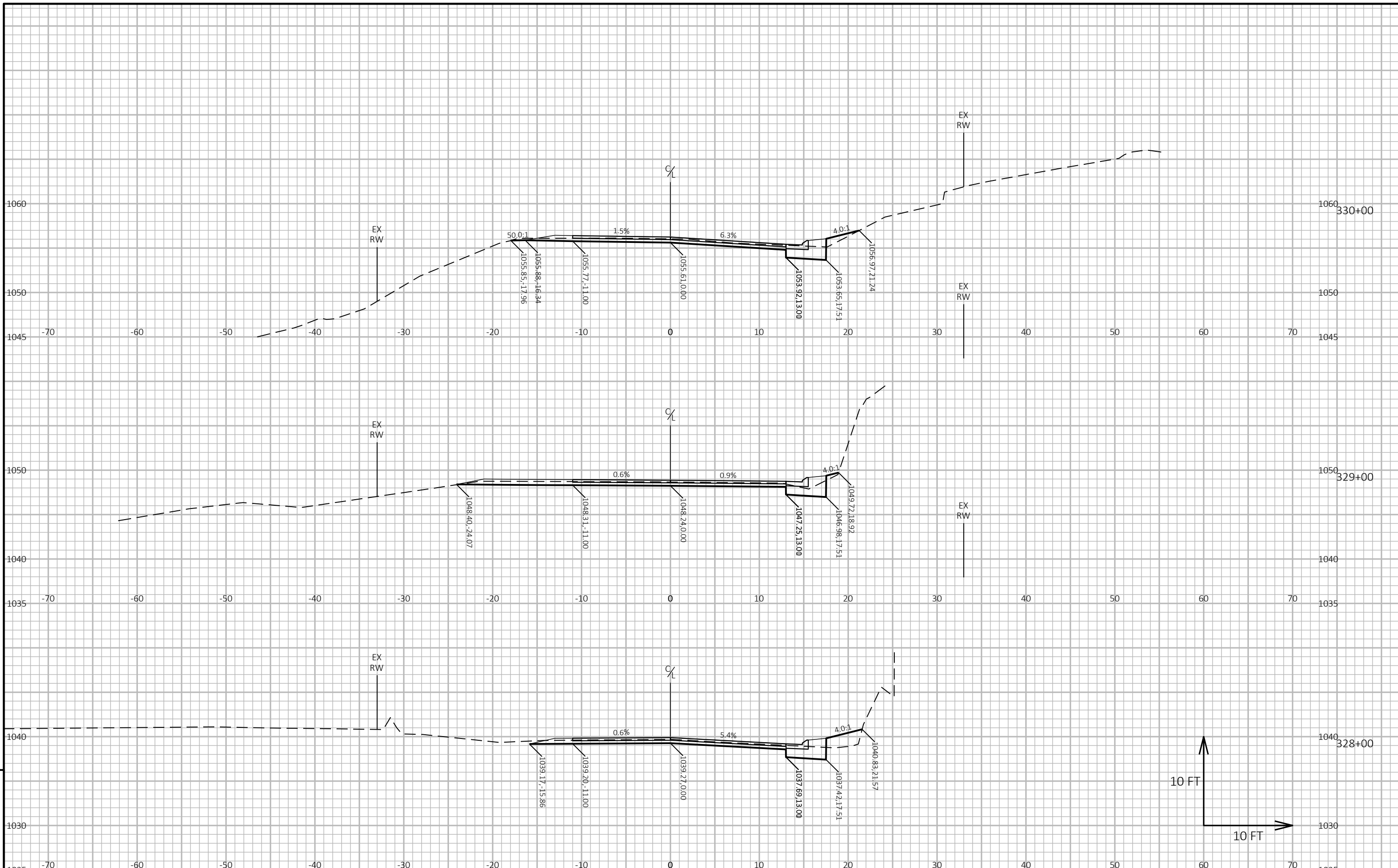
HWY: STH 171

COUNTY: CRAWFORD

CROSS SECTIONS: STH 171

SHEET

E



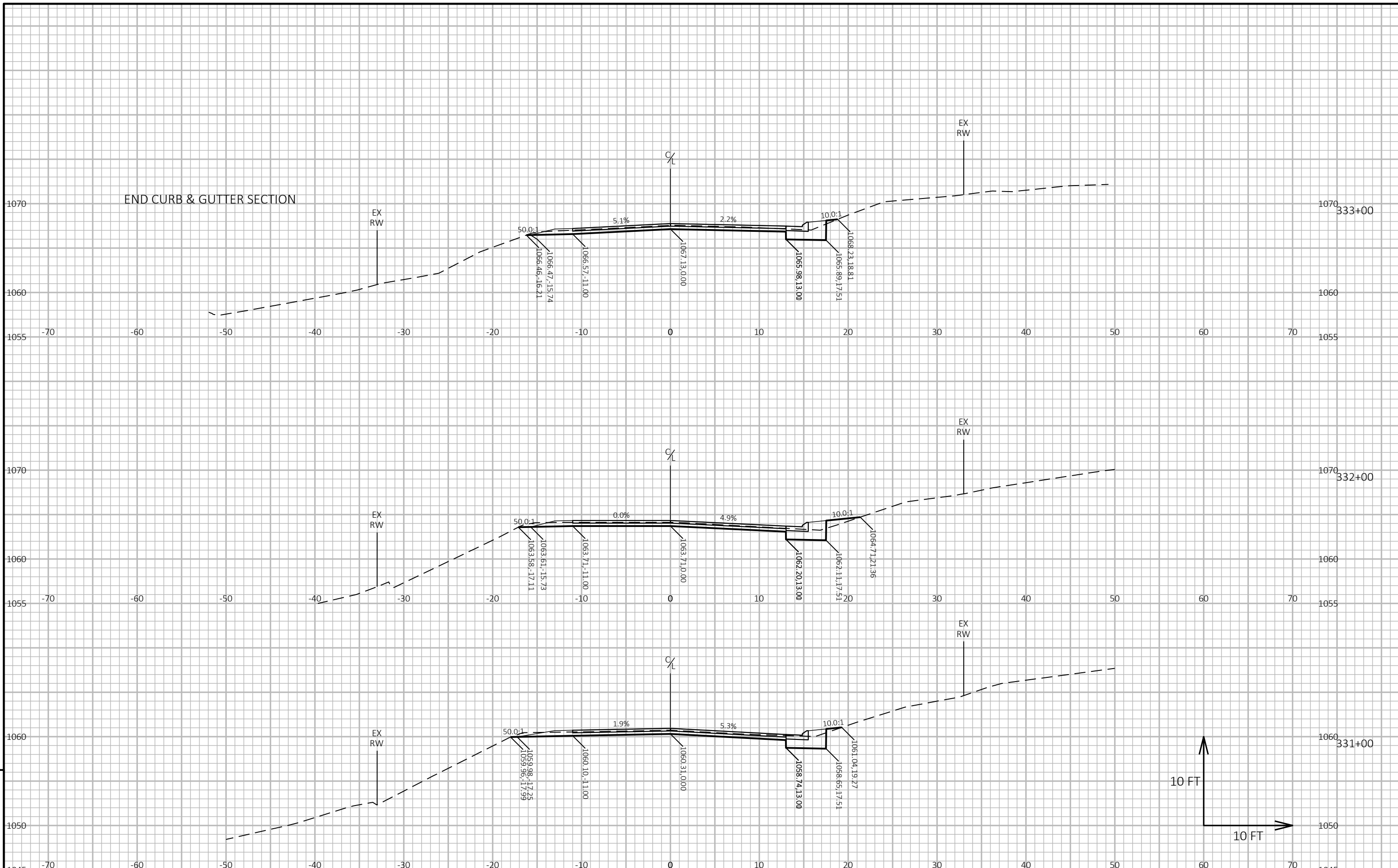
9

9

PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

FILE NAME: \\SP-PZ1.SEHINC.COM\PROJECTS\UZ\W\WITSW\162034\5-FINAL-DSGN\51-DRAWINGS\10-CIVIL\C3D\57900202\SHEETS\PLAN\090103_XS.DWG PLOT DATE: 7/19/2024 10:00 AM PLOT BY: SAVANNAH STEHN PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

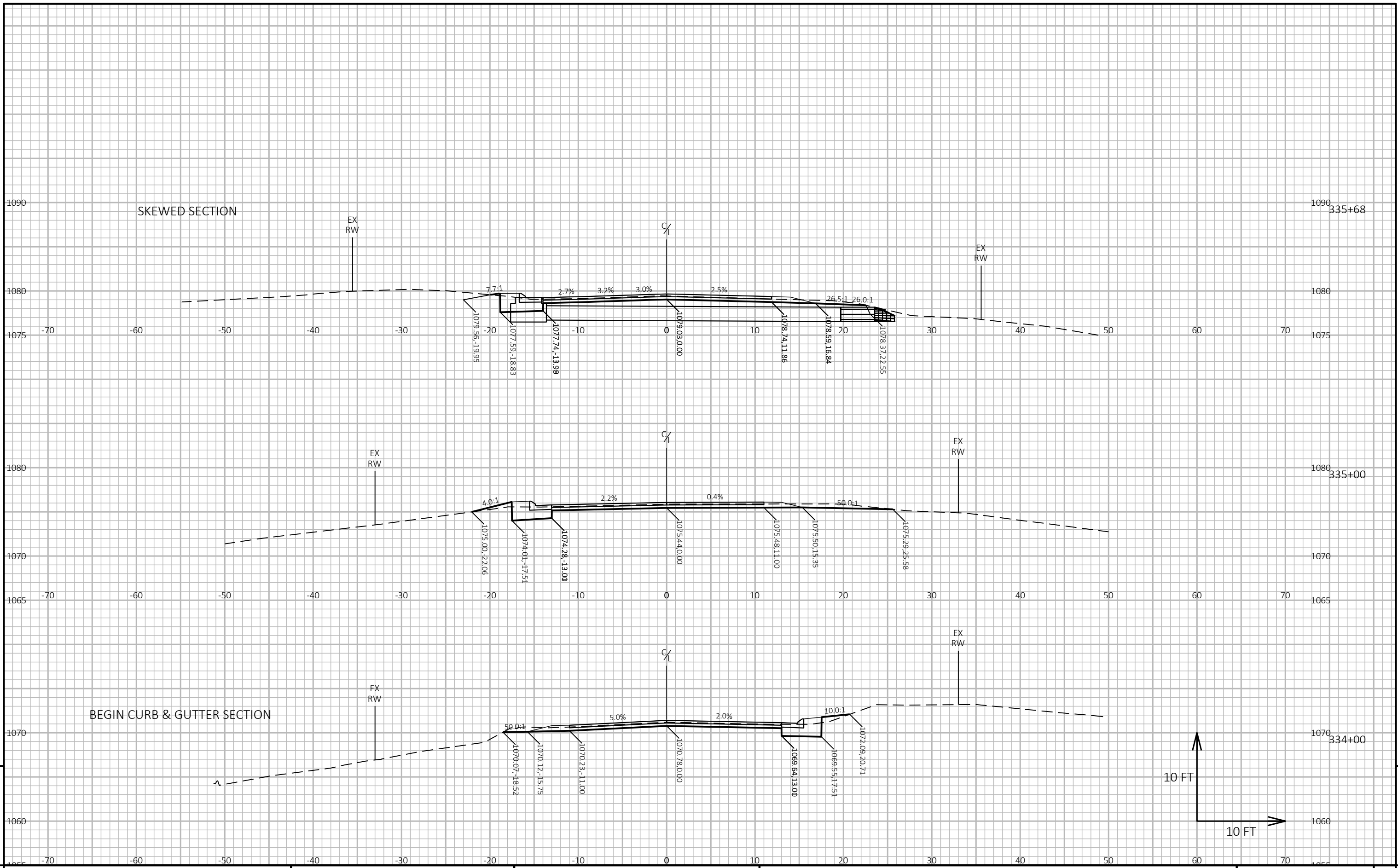
LAYOUT NAME - SG3-7



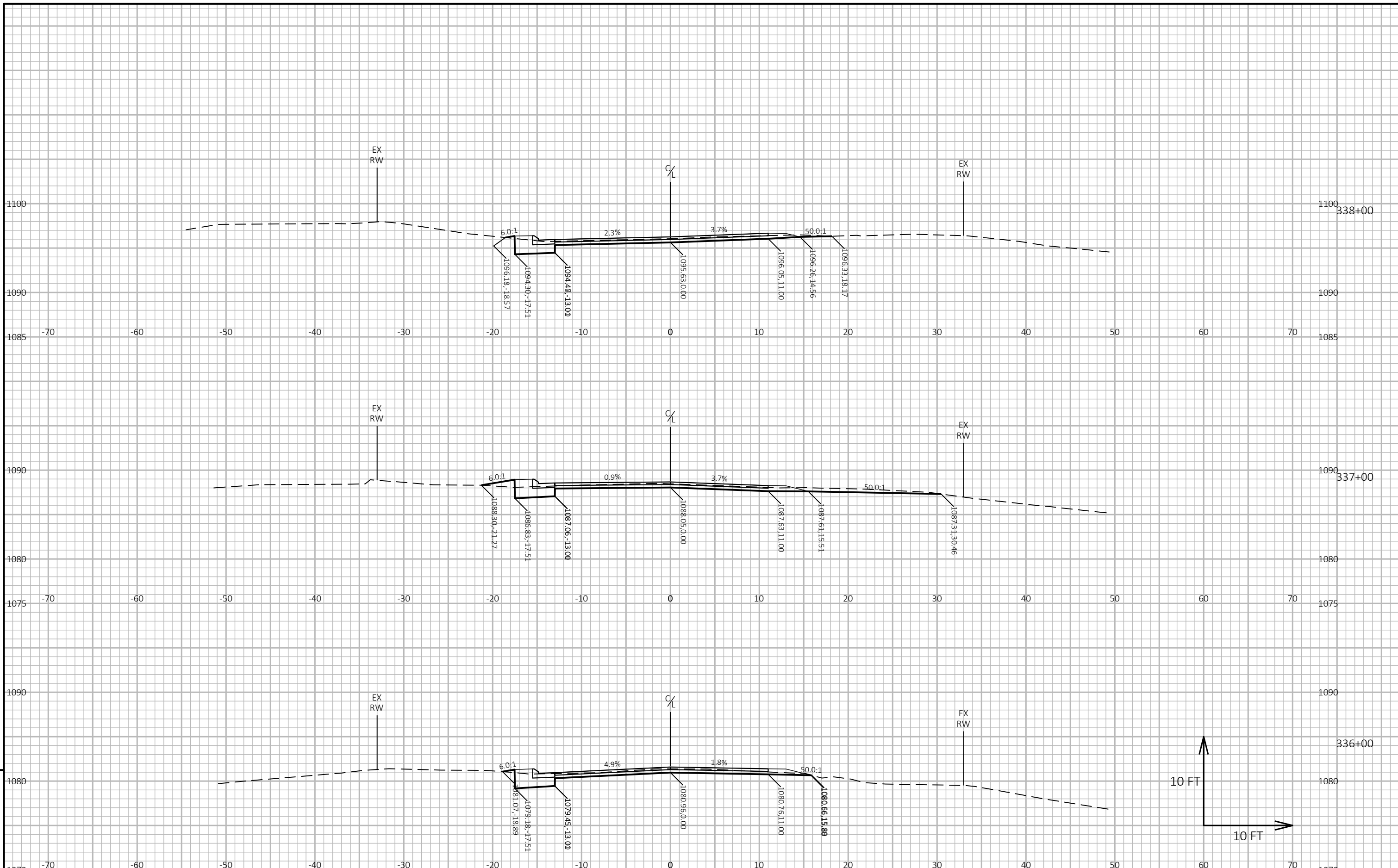
9

9

PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171	SHEET	E
------------------------	--------------	------------------	-------------------------	-------	---



PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171	SHEET	9
------------------------	--------------	------------------	-------------------------	-------	----------



PROJECT NO: 5790-02-72

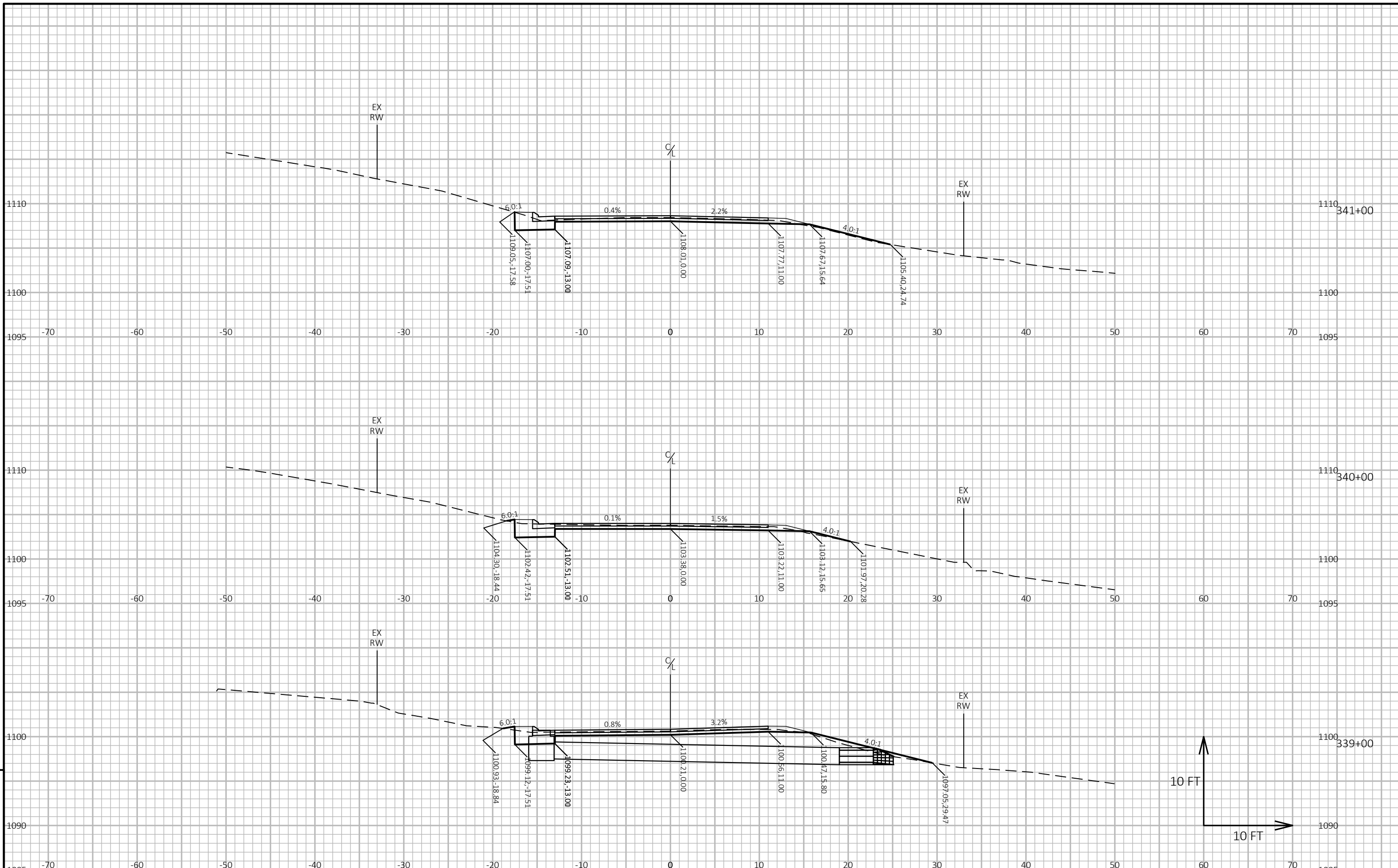
HWY: STH 171

COUNTY: CRAWFORD

CROSS SECTIONS: STH 171

SHEET

E



PROJECT NO: 5790-02-72

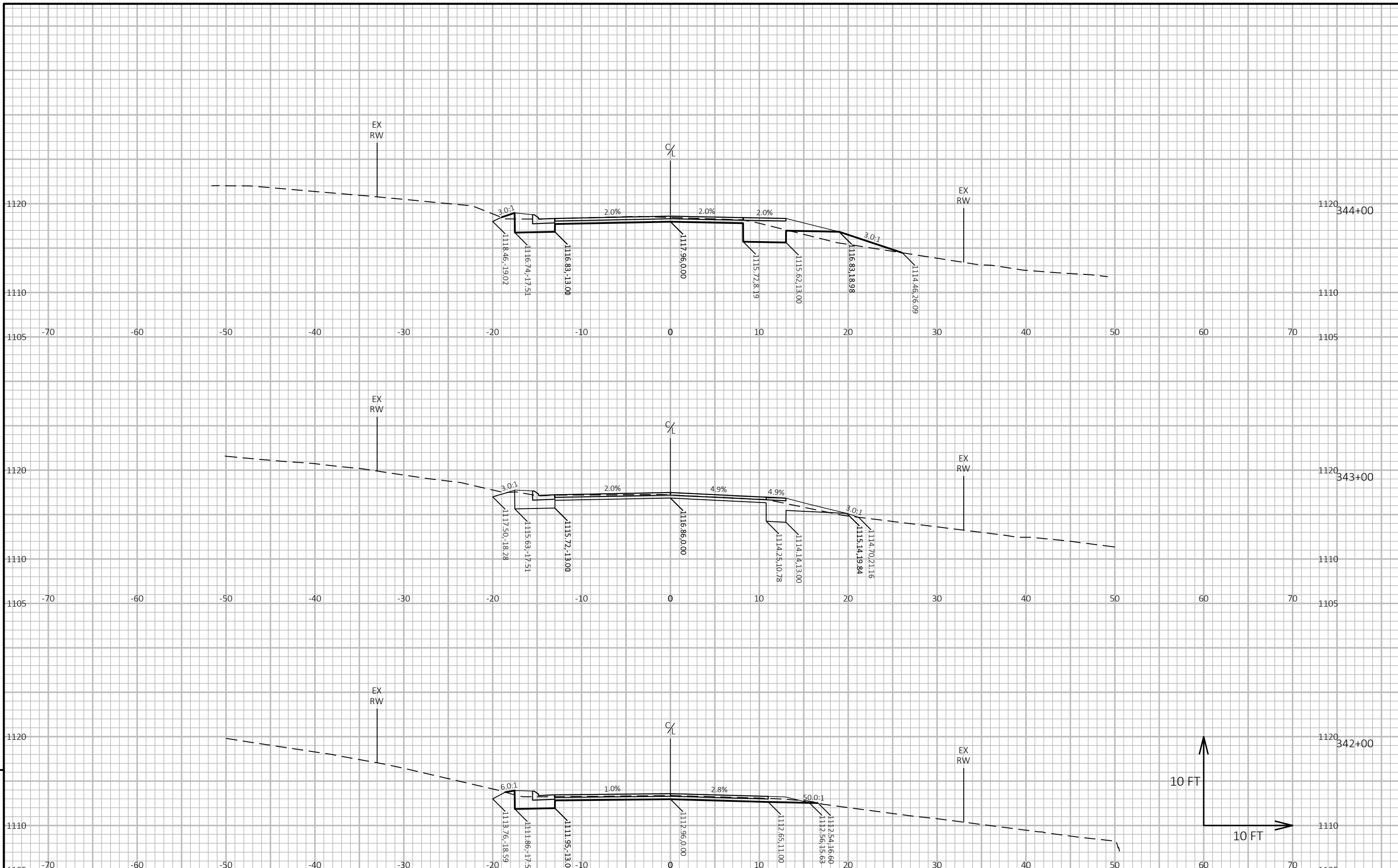
HWY: STH 171

COUNTY: CRAWFORD

CROSS SECTIONS: STH 171

SHEET

E



PROJECT NO: 5790-02-72

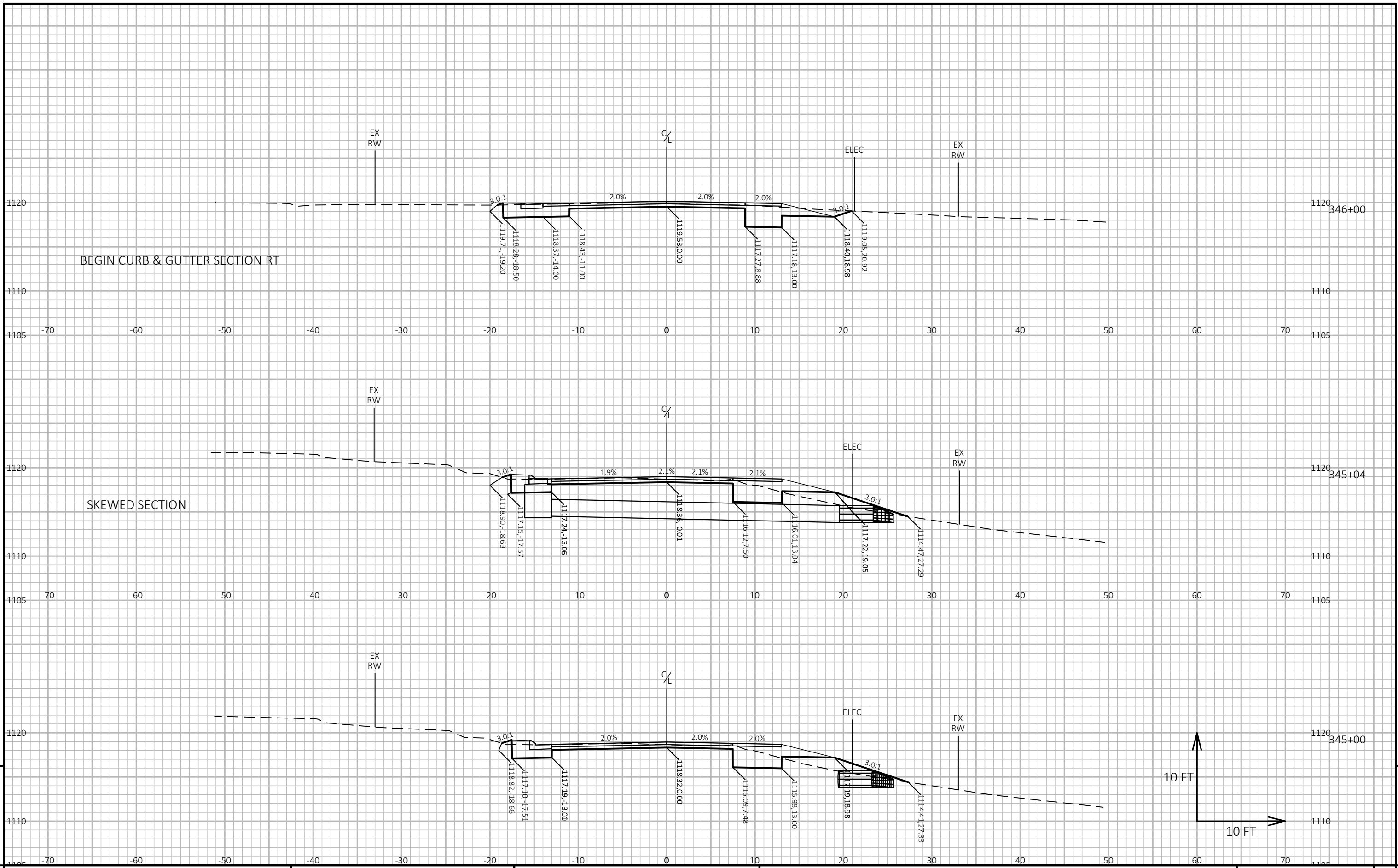
HWY: STH 171

COUNTY: CRAWFORD

CROSS SECTIONS: STH 171

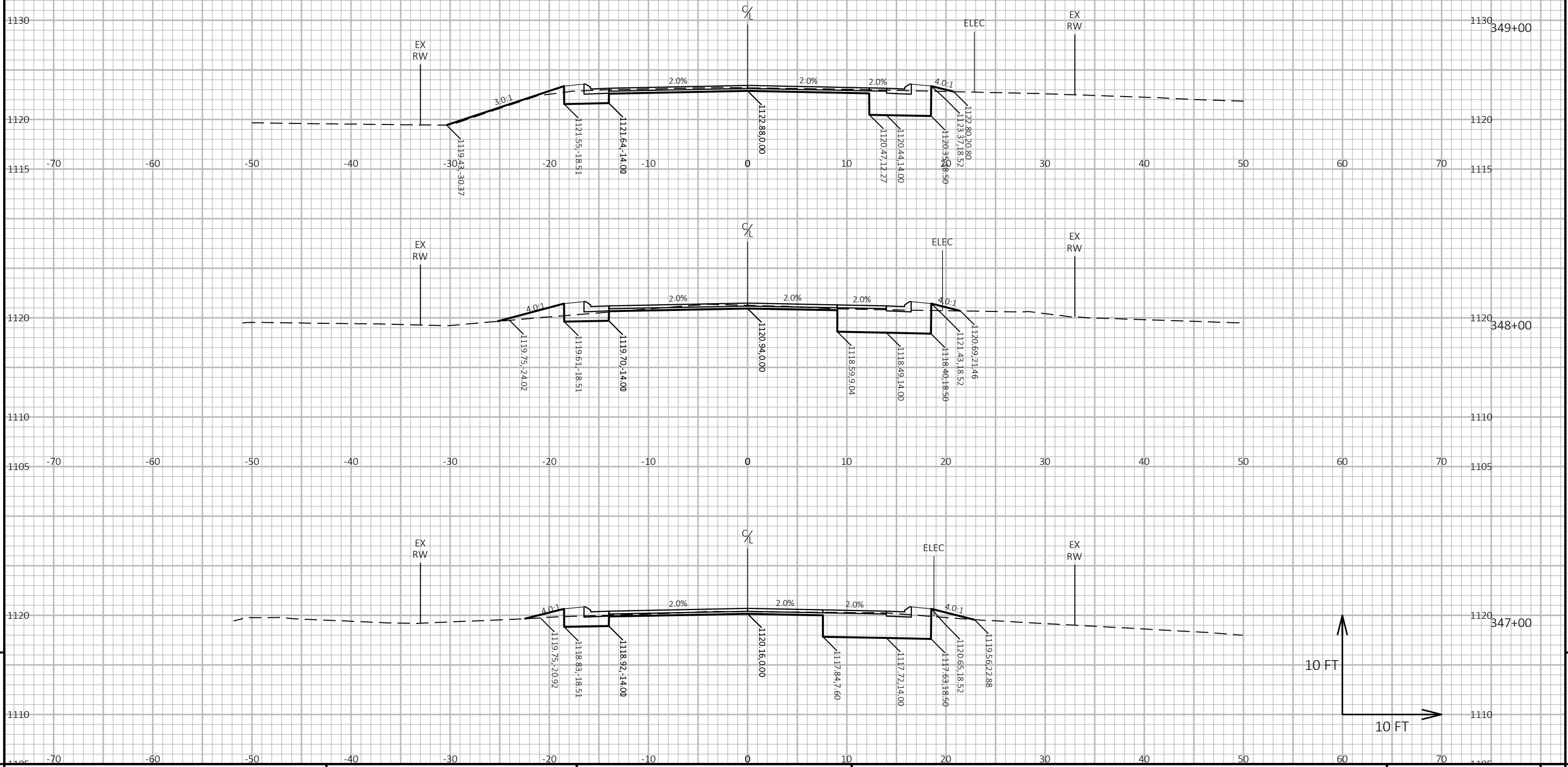
SHEET

E



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

END CURB & GUTTER SECTION LT



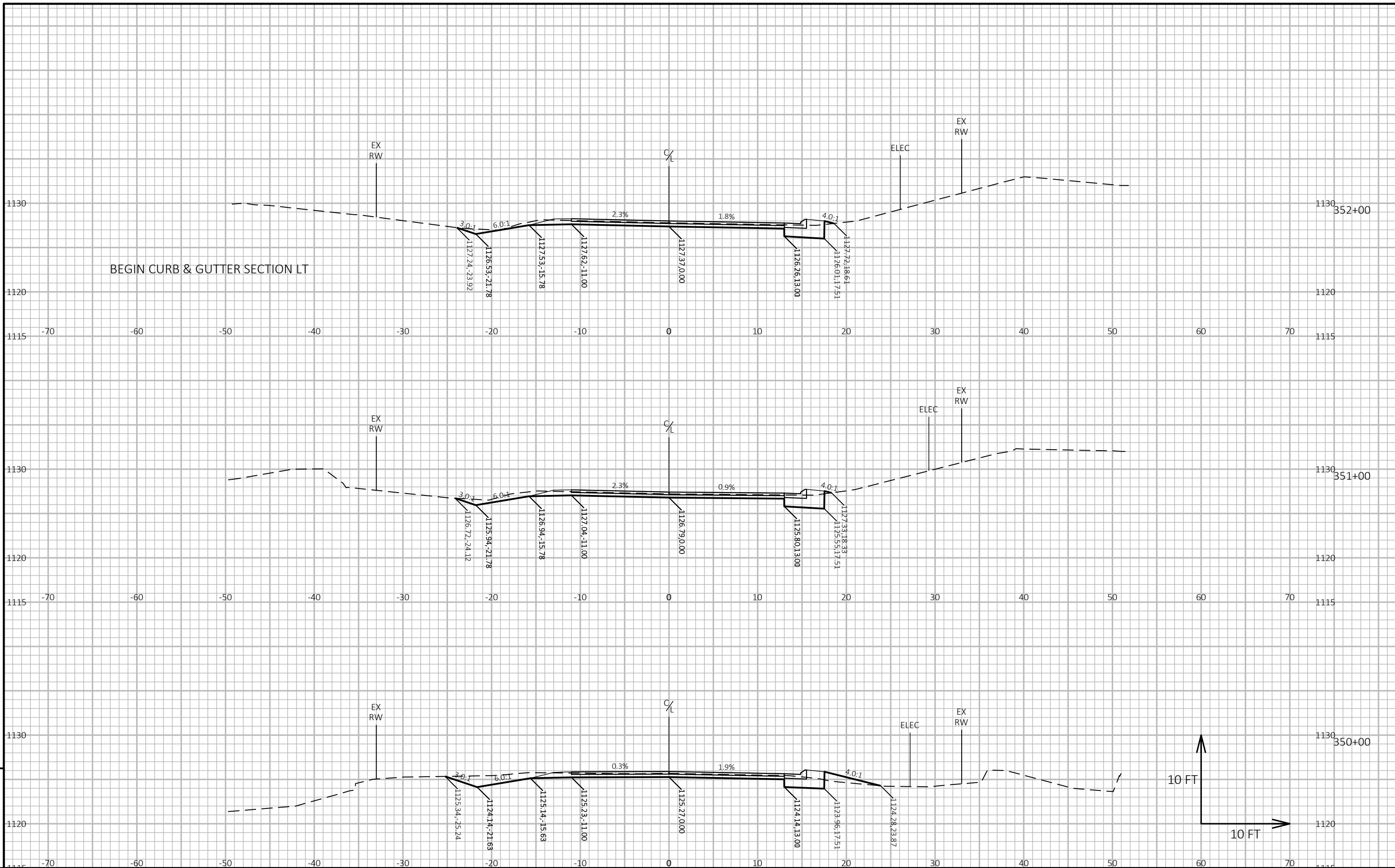
9

9

PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

FILE NAME: \\SP-PZ1.SEHINC.COM\PZPROJECTS1\UZ\W\WITSW\162034\5-FINAL-DSGN\51-DRAWINGS\10-CIVIL\C3D\57900202\SHEETS\PLAN\090103_XS.DWG PLOT DATE: 7/22/2024 3:27 PM PLOT BY: SAVANNAH STEHN PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - SG3-14

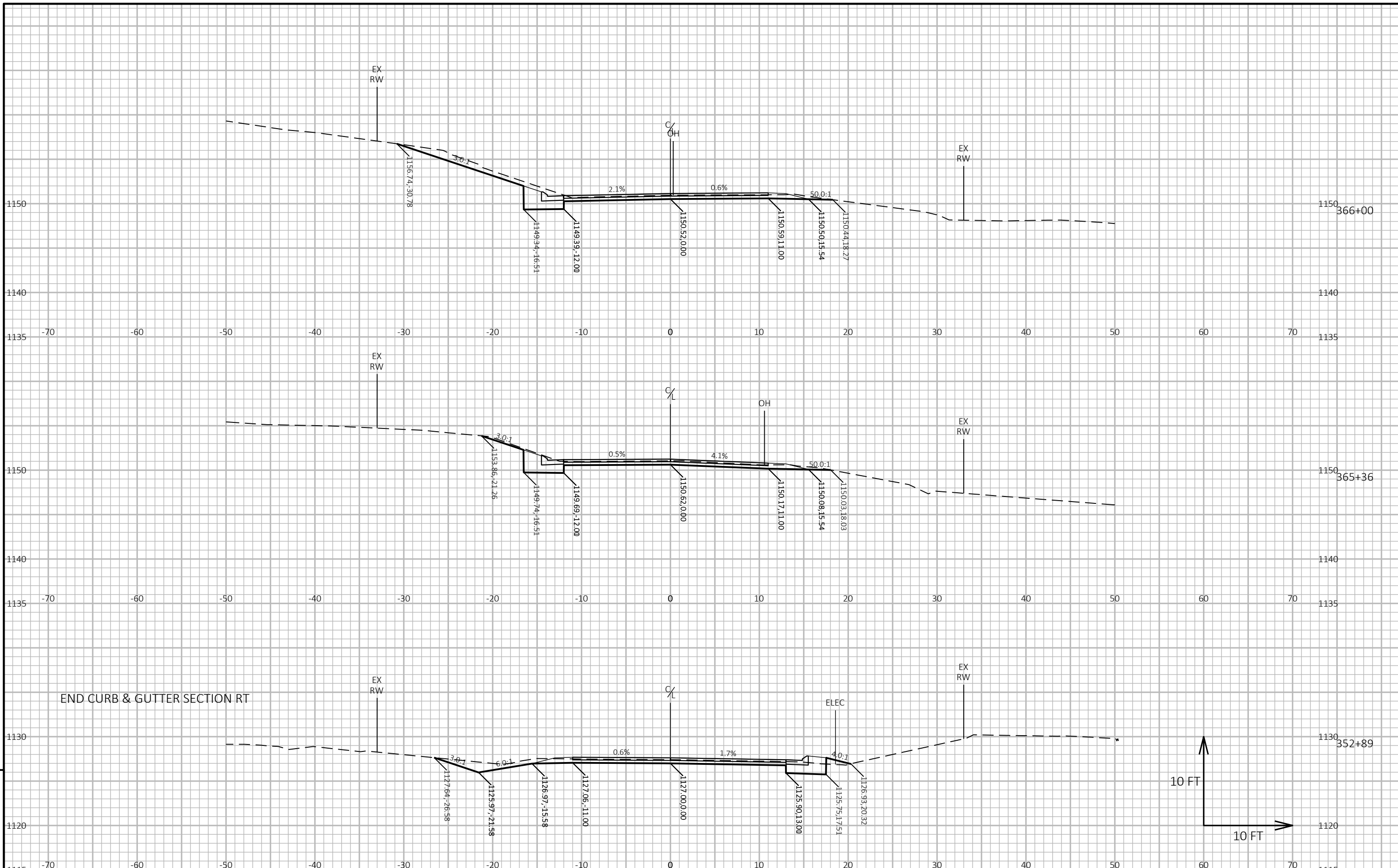


BEGIN CURB & GUTTER SECTION LT

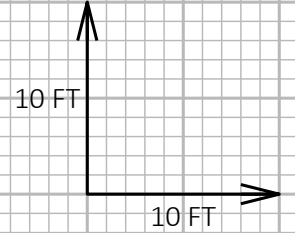
9

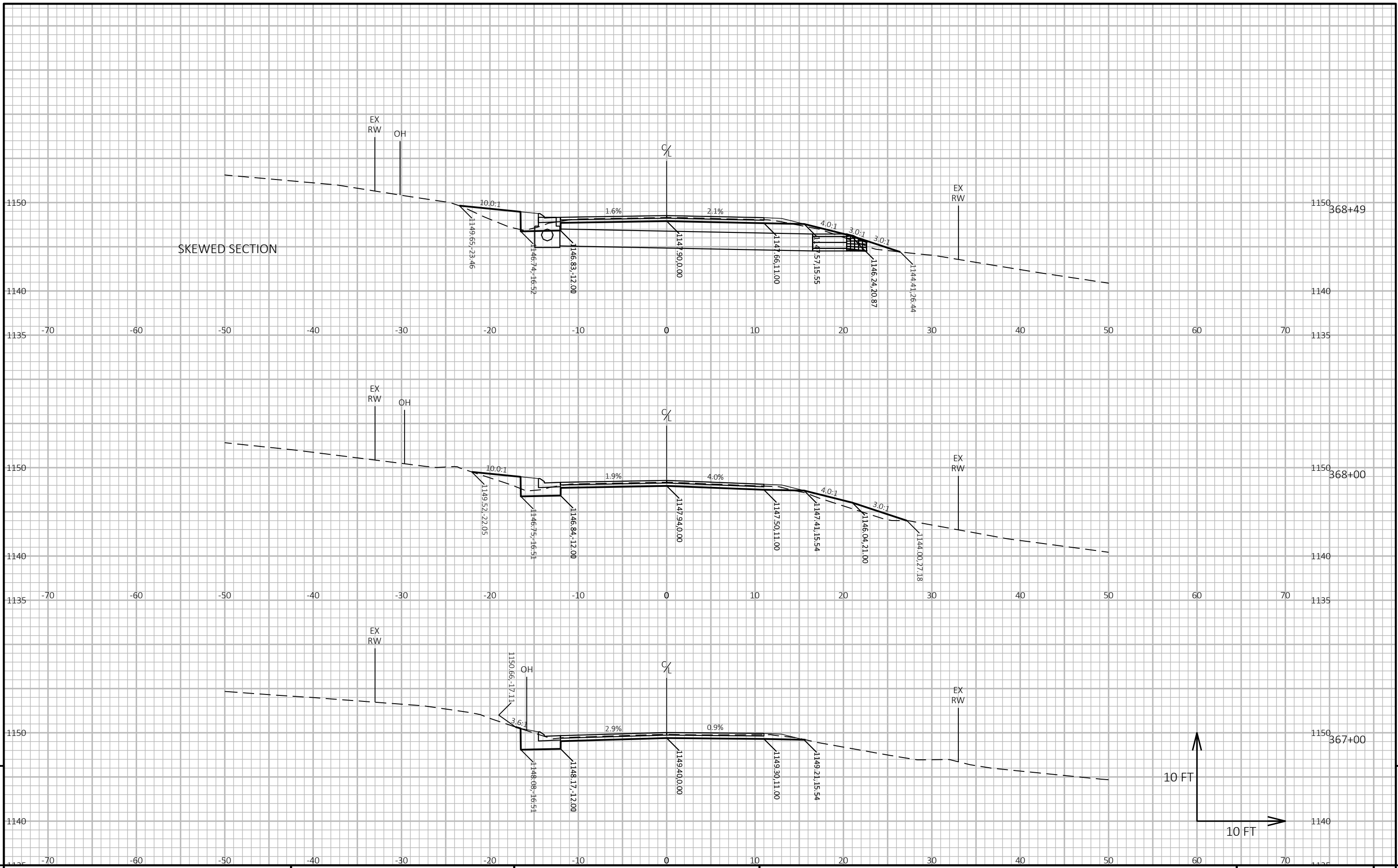
9

E



END CURB & GUTTER SECTION RT





PROJECT NO: 5790-02-72

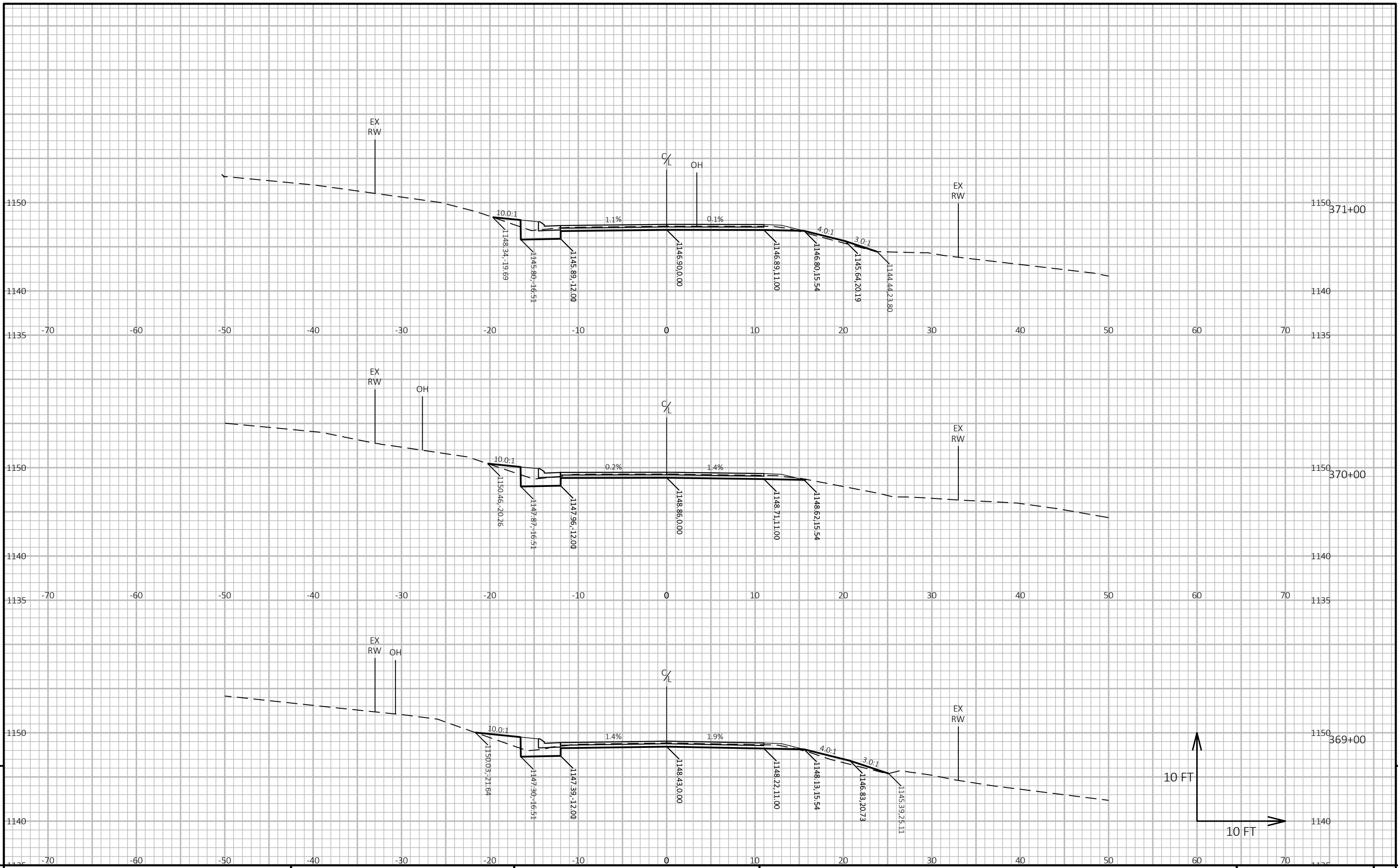
HWY: STH 171

COUNTY: CRAWFORD

CROSS SECTIONS: STH 171

SHEET

E



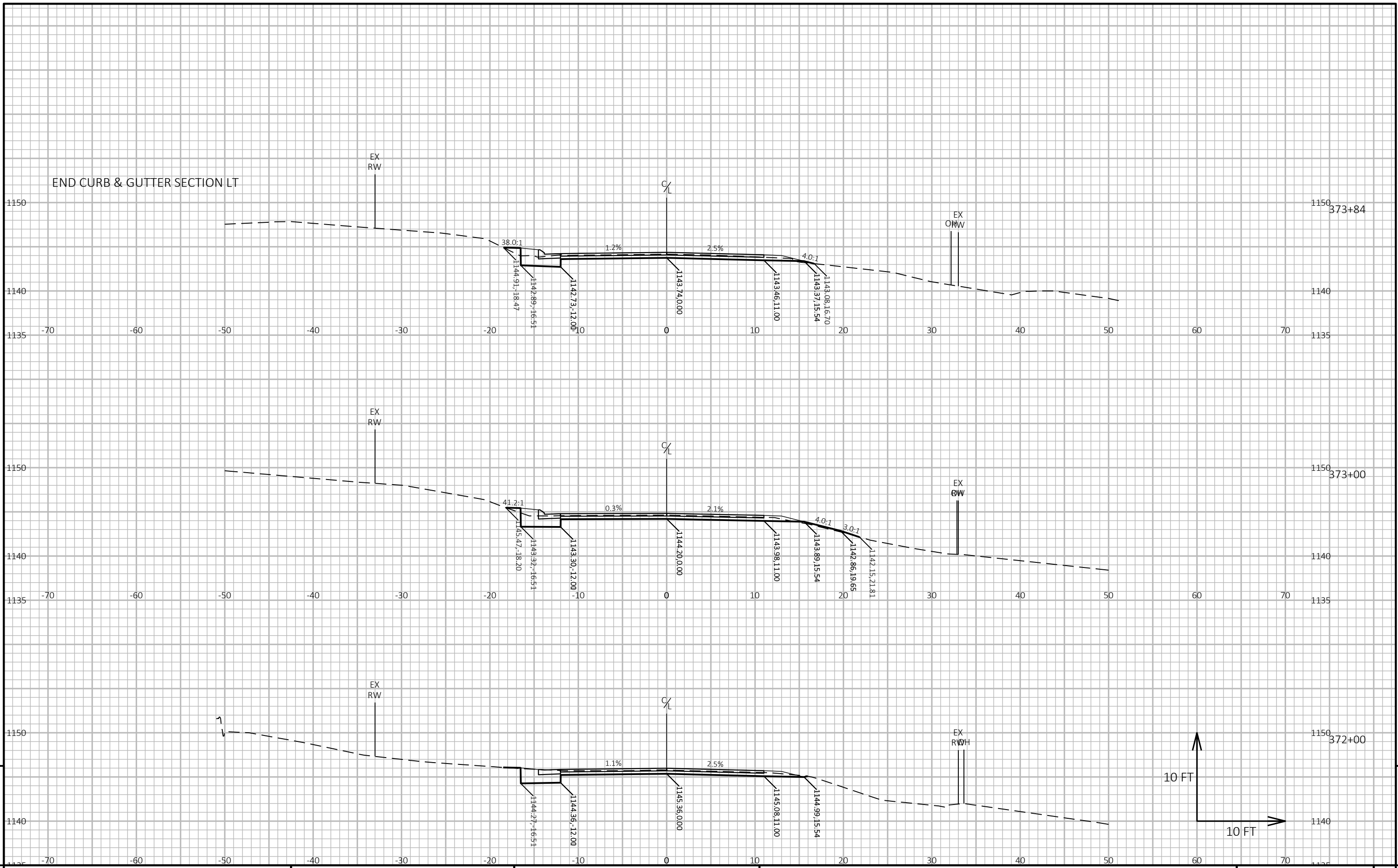
9

9

PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

FILE NAME: \\SP-PZ1.SEHINC.COM\PROJECTS\UZ\W\WITSW\162034\5-FINAL-DSGN\51-DRAWINGS\10-CIVIL\C3D\57900202\SHEETS\PLAN\090103_XS.DWG PLOT DATE: 7/19/2024 10:05 AM PLOT BY: SAVANNAH STEHN PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - SG3-18



PROJECT NO: 5790-02-72

HWY: STH 171

COUNTY: CRAWFORD

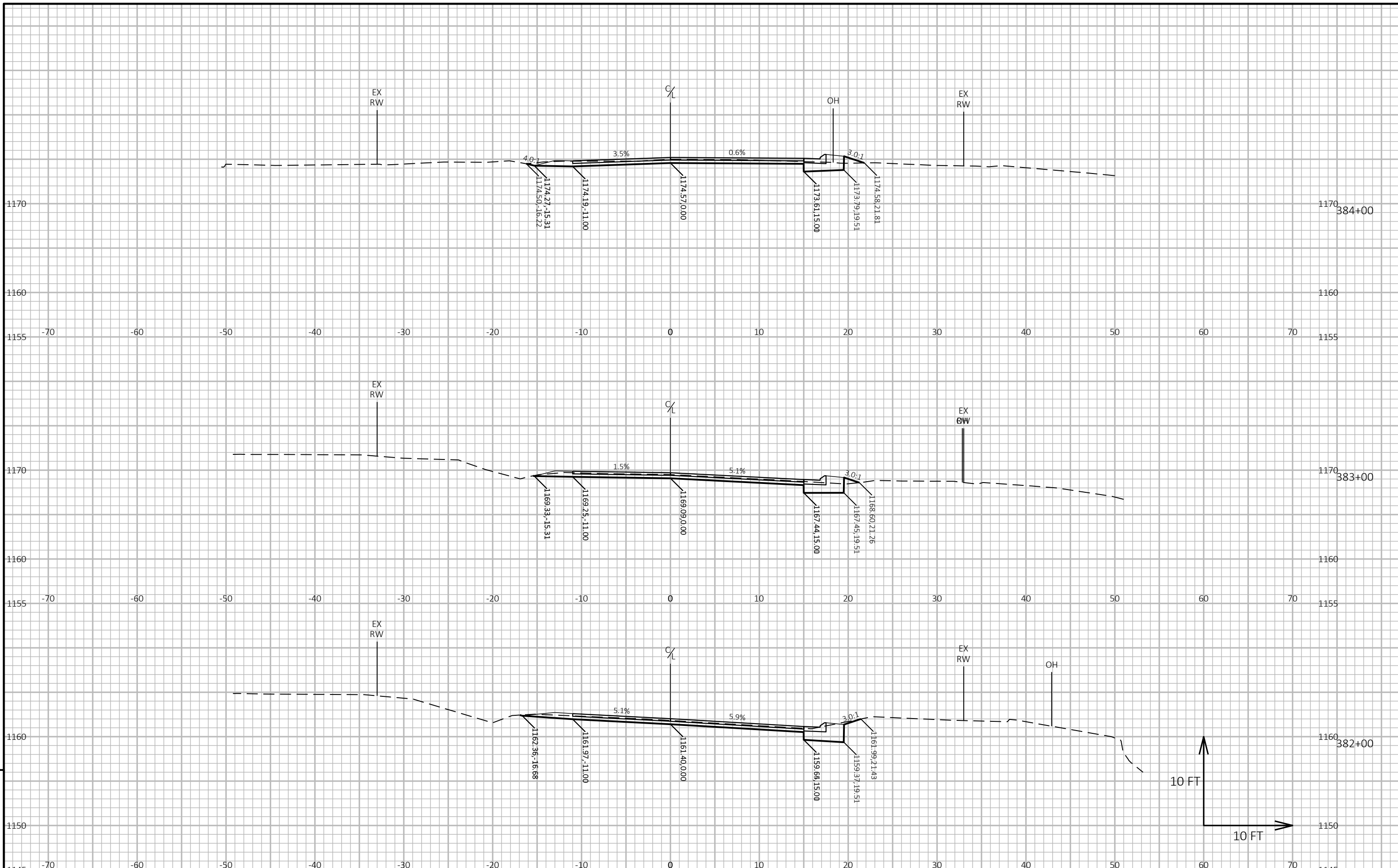
CROSS SECTIONS: STH 171

SHEET

E



PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171	SHEET	9
------------------------	--------------	------------------	-------------------------	-------	----------



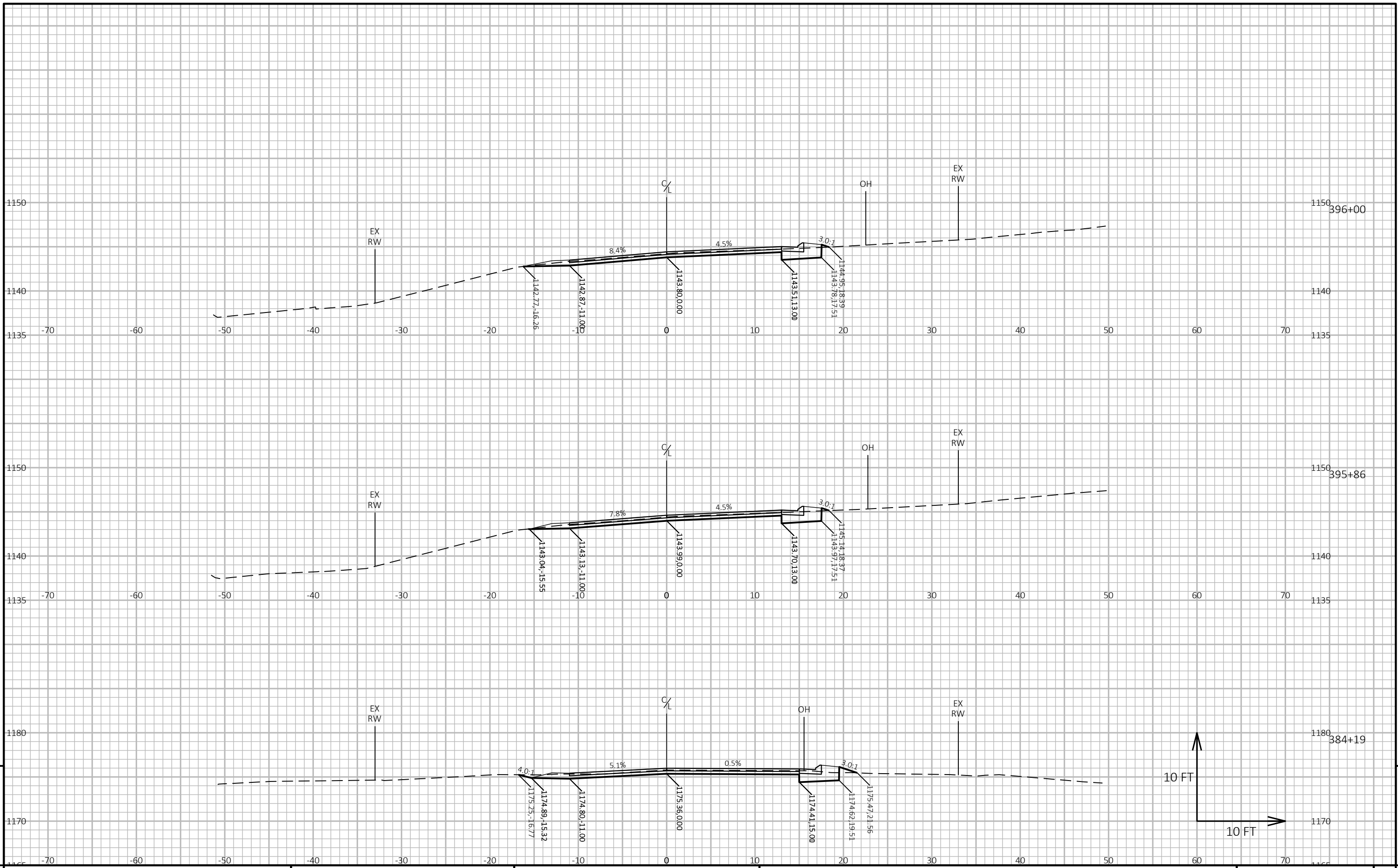
9

9

PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

FILE NAME: \\SP-PZ1.SEHINC.COM\PROJECTS\UZ\W\WITSW\162034\5-FINAL-DSGN\51-DRAWINGS\10-CIVIL\C3D\57900202\SHEETS\PLAN\090103_XS.DWG PLOT DATE: 7/19/2024 10:06 AM PLOT BY: SAVANNAH STEHN PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - SG3-21



PROJECT NO: 5790-02-72

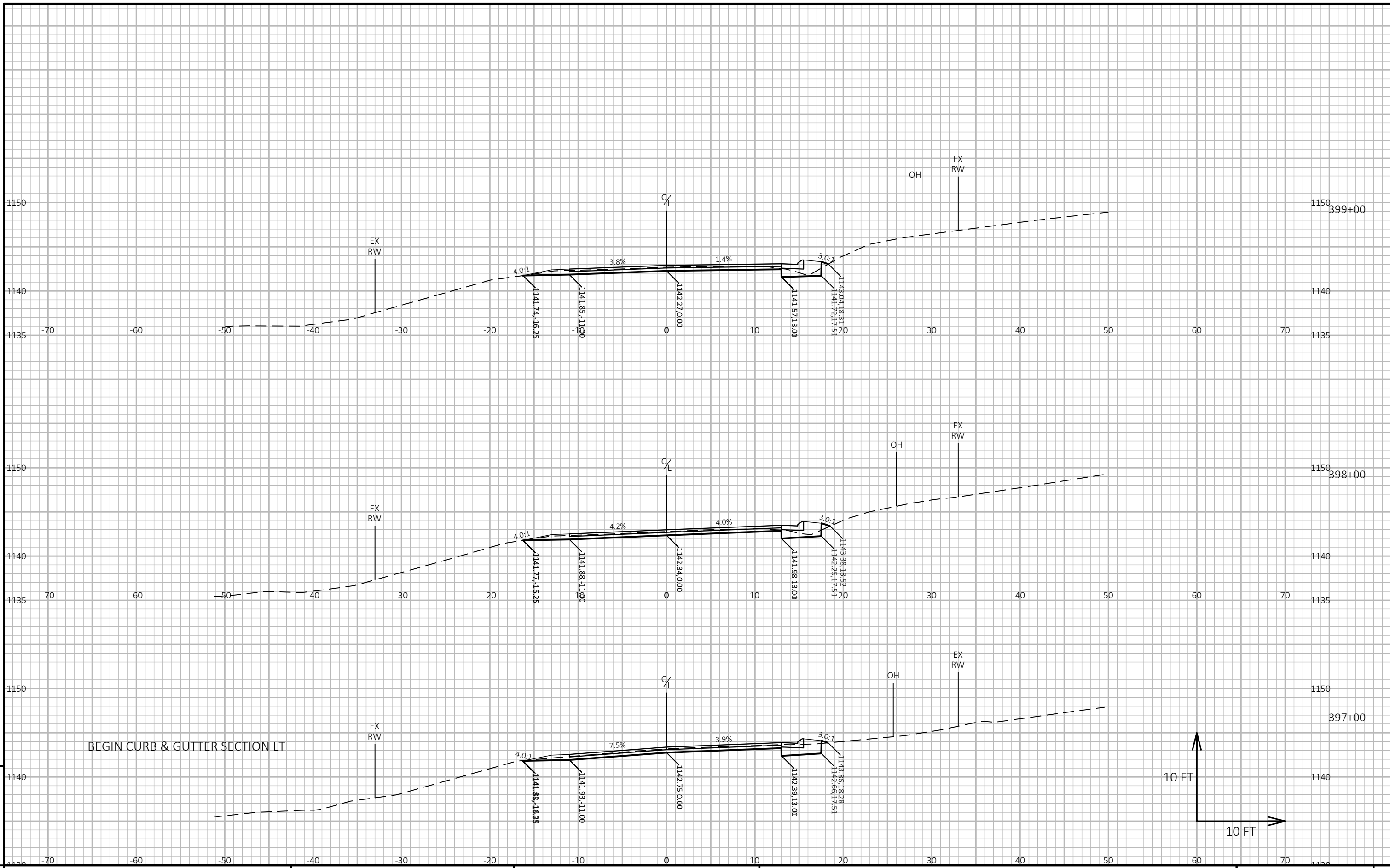
HWY: STH 171

COUNTY: CRAWFORD

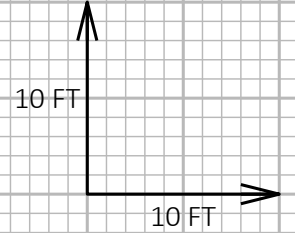
CROSS SECTIONS: STH 171

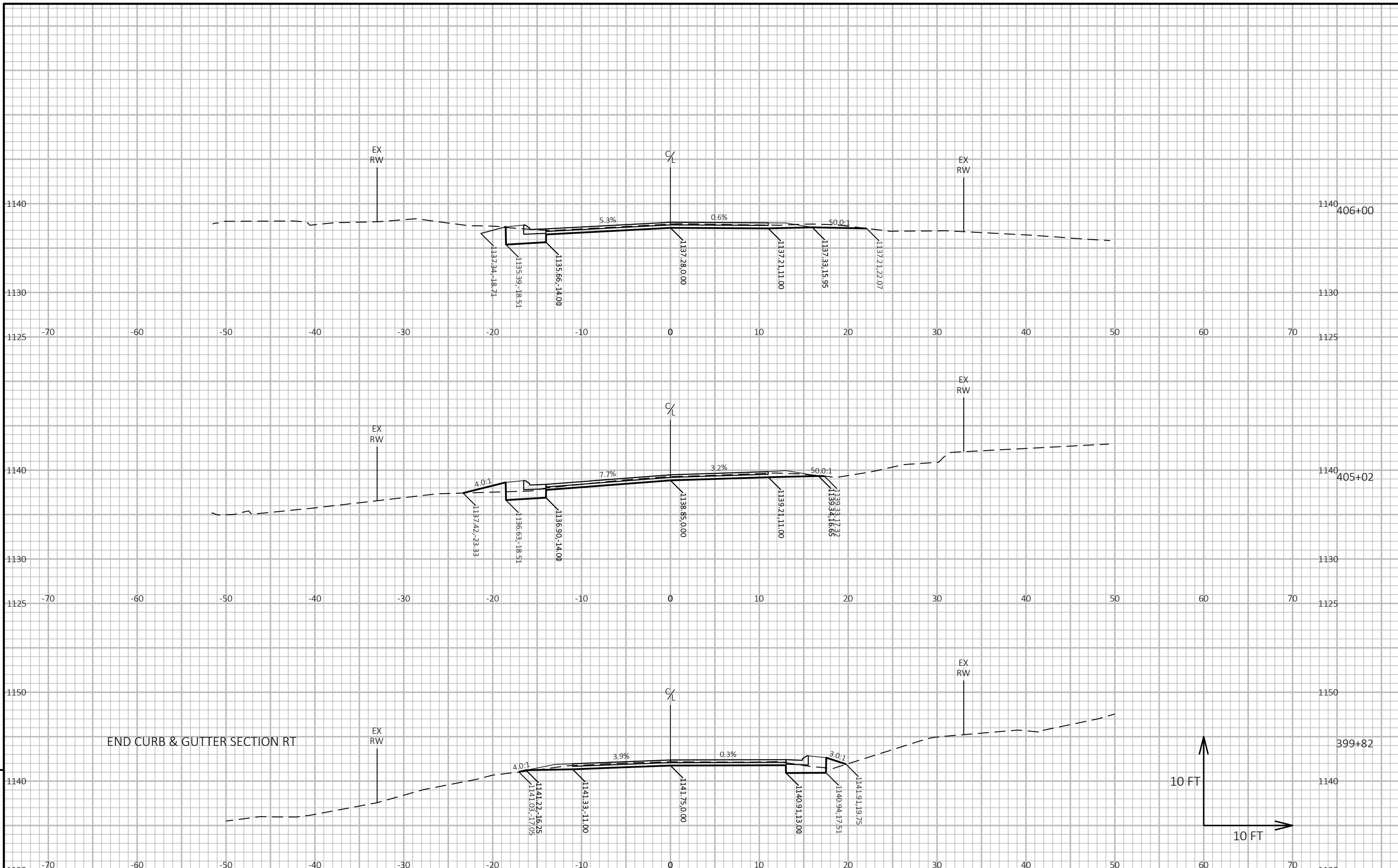
SHEET

E



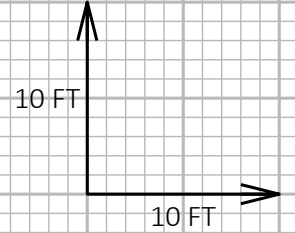
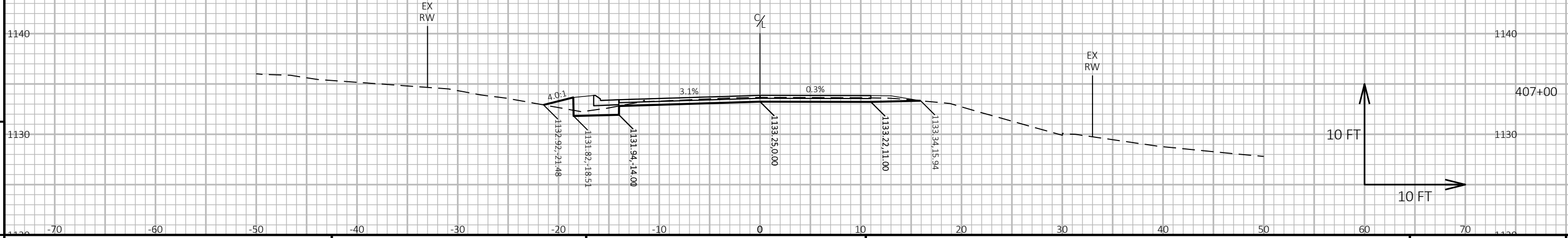
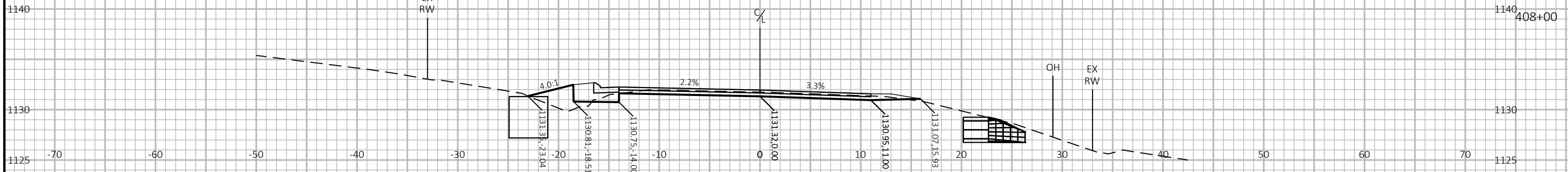
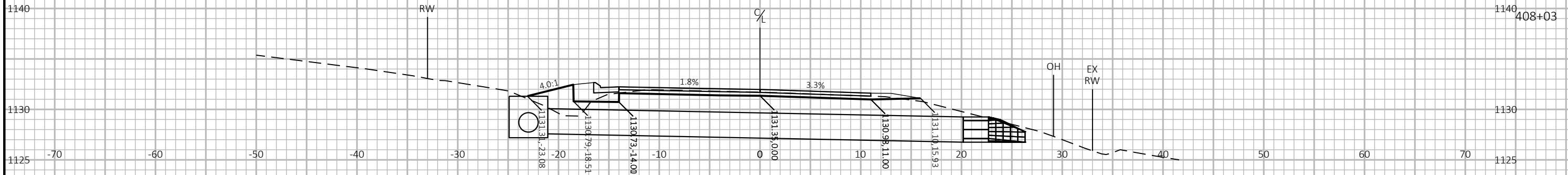
BEGIN CURB & GUTTER SECTION LT





PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

END CURB & GUTTER SECTION LT



PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171	SHEET	E
------------------------	--------------	------------------	-------------------------	-------	---



PROJECT NO: 5790-02-72

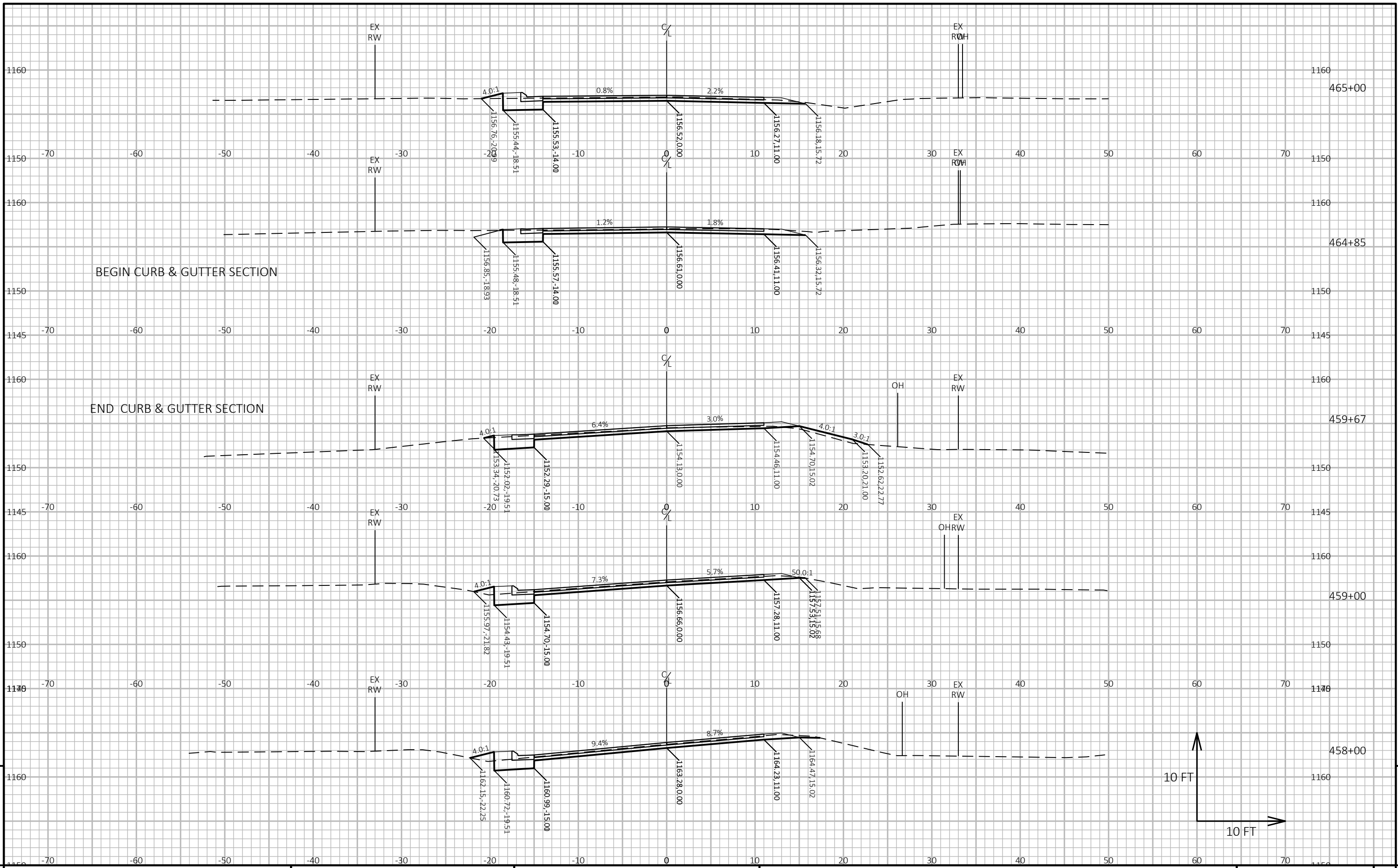
HWY: STH 171

COUNTY: CRAWFORD

CROSS SECTIONS: STH 171

SHEET

E



PROJECT NO: 5790-02-72

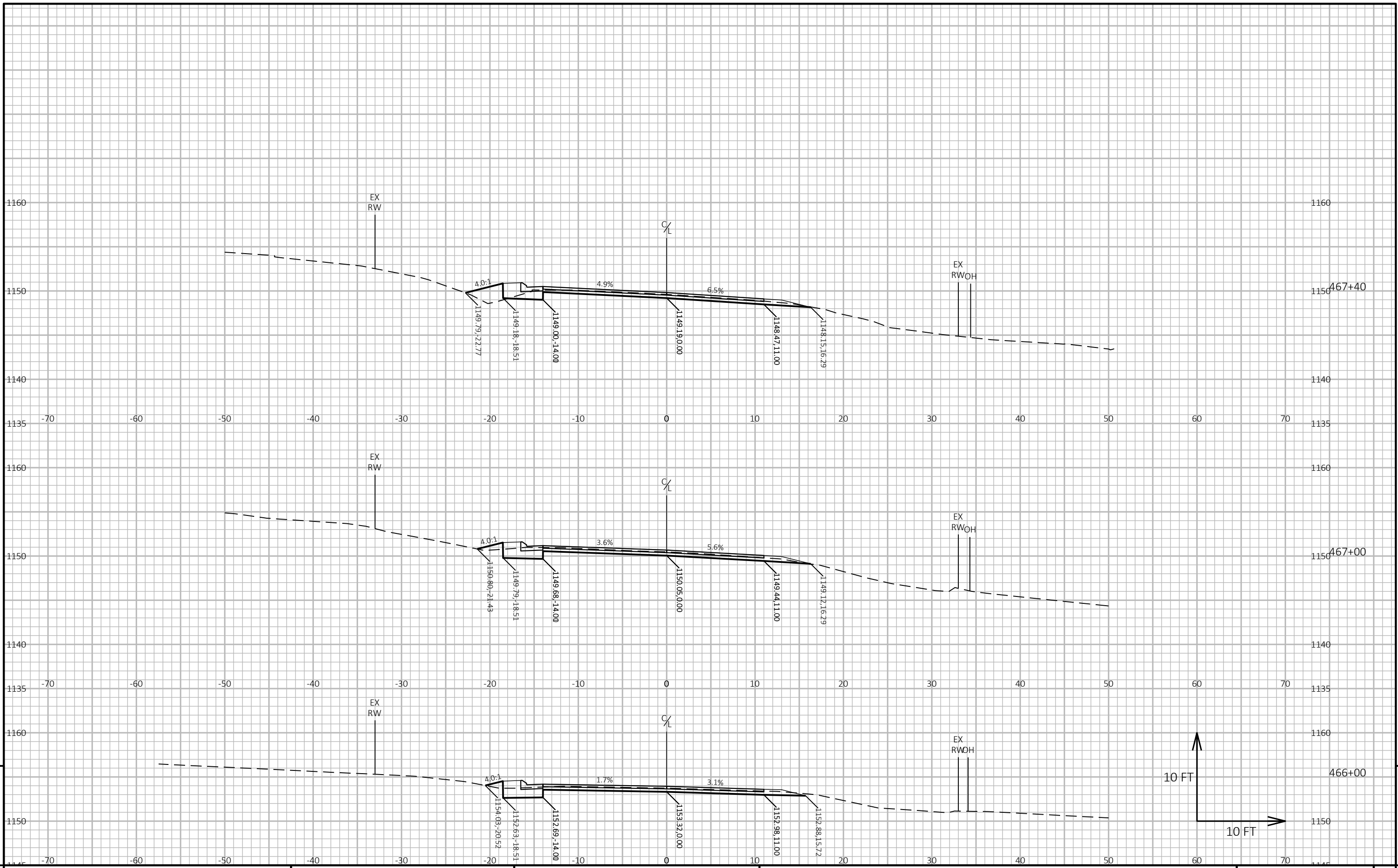
HWY: STH 171

COUNTY: CRAWFORD

CROSS SECTIONS: STH 171

SHEET

E



PROJECT NO: 5790-02-72

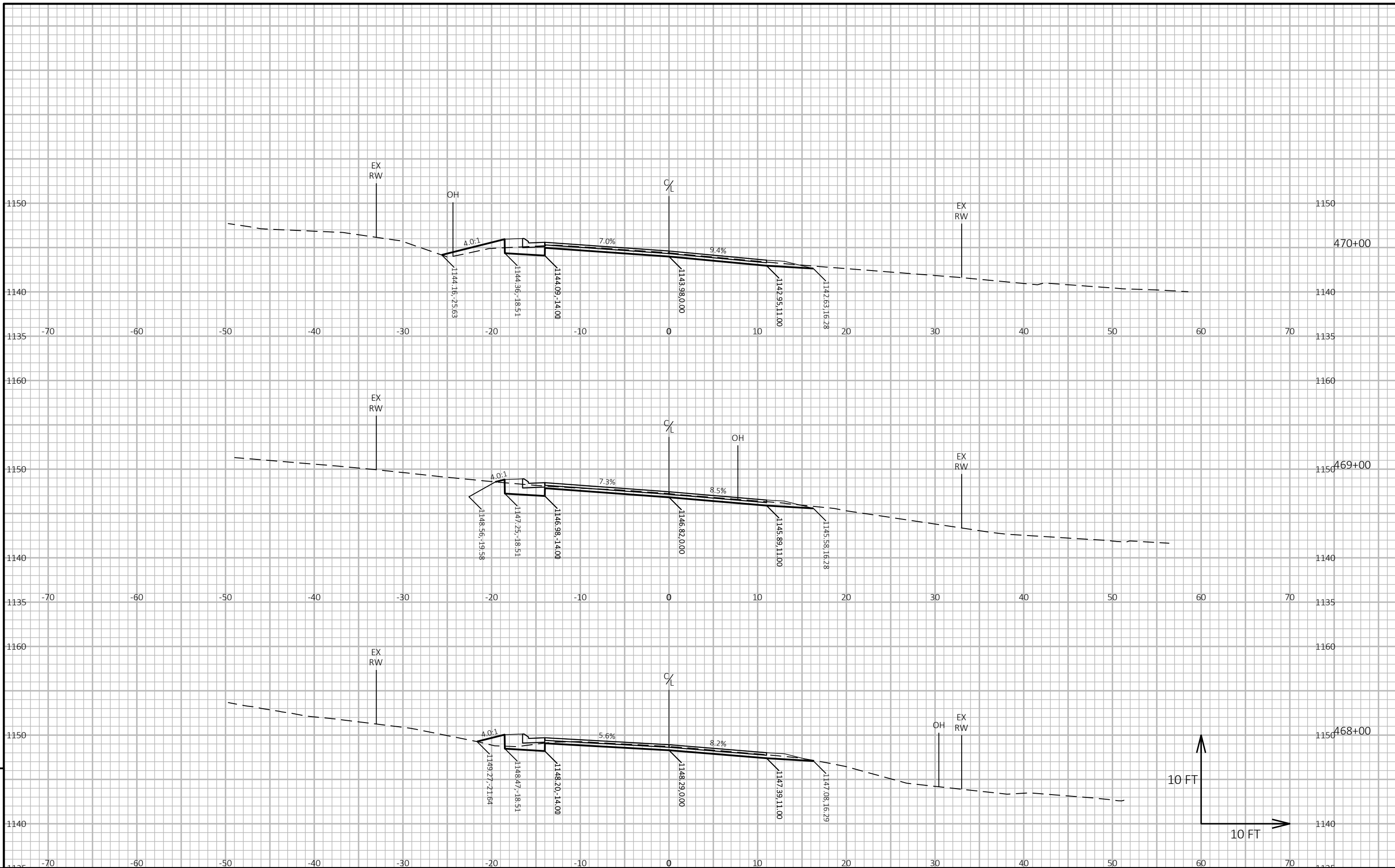
HWY: STH 171

COUNTY: CRAWFORD

CROSS SECTIONS: STH 171

SHEET

E



PROJECT NO: 5790-02-72

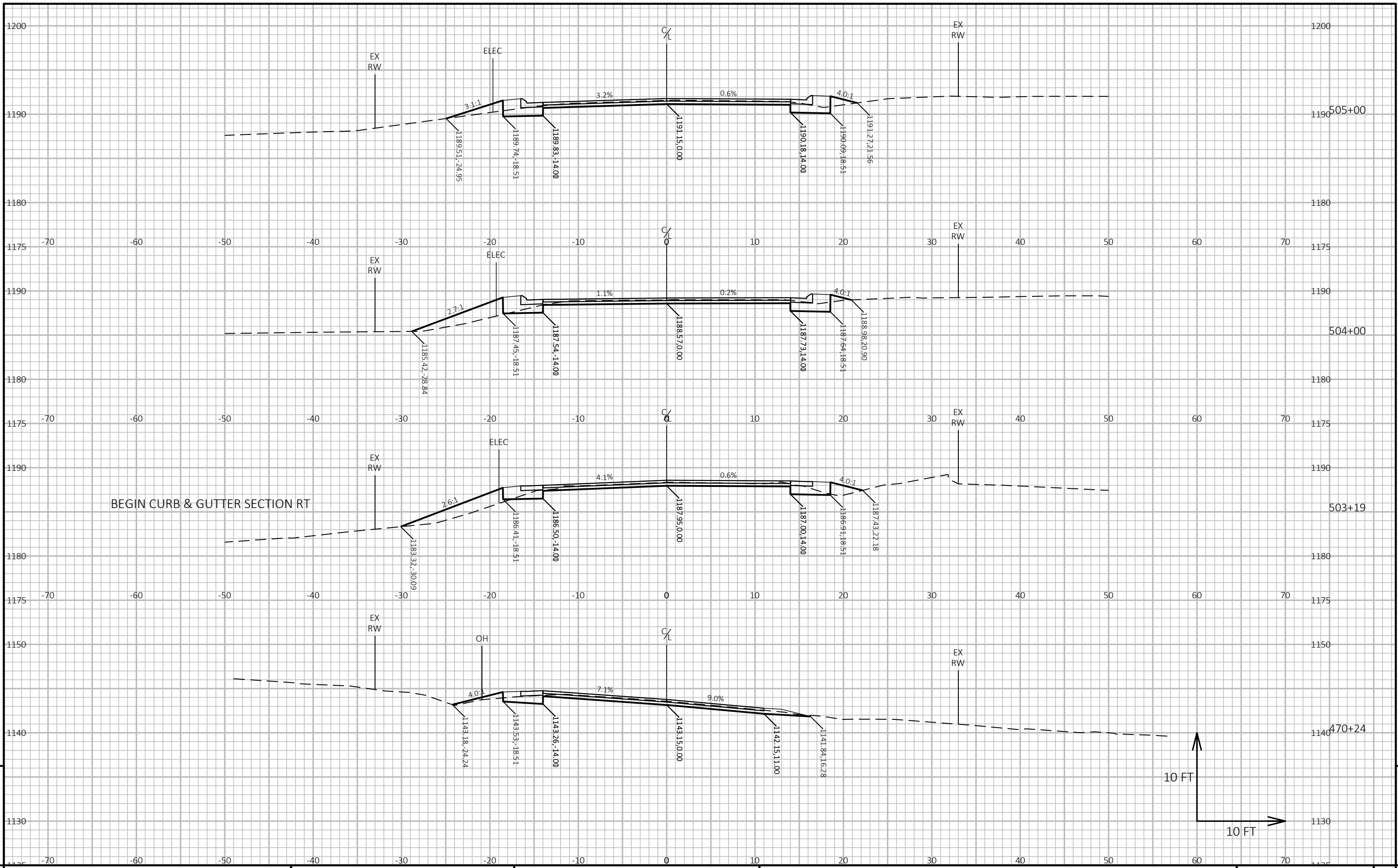
HWY: STH 171

COUNTY: CRAWFORD

CROSS SECTIONS: STH 171

SHEET

E



PROJECT NO: 5790-02-72

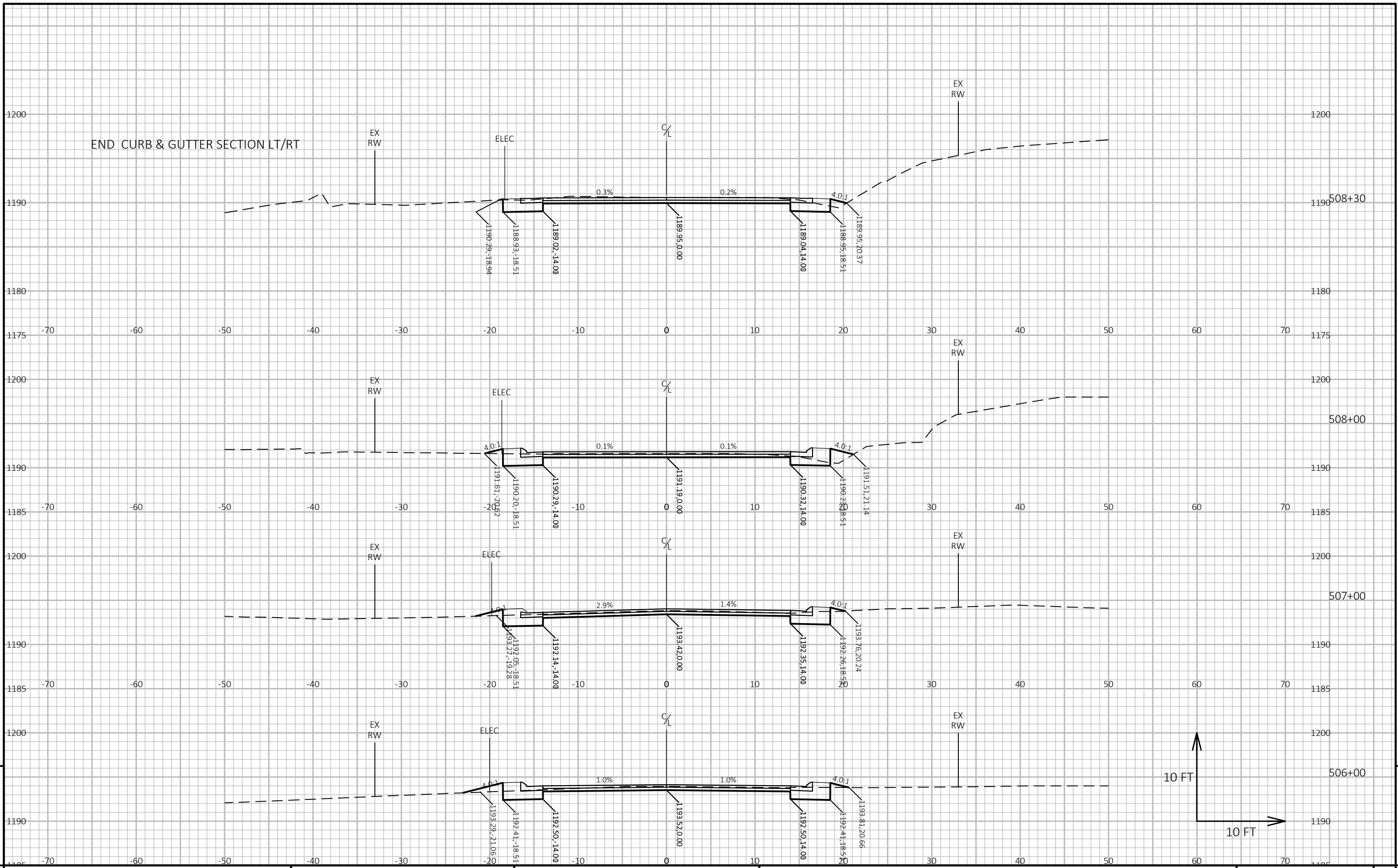
HWY: STH 171

COUNTY: CRAWFORD

CROSS SECTIONS: STH 171

SHEET

E



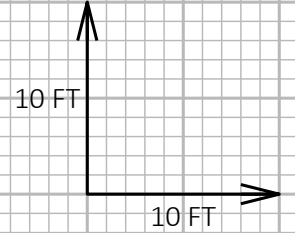
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

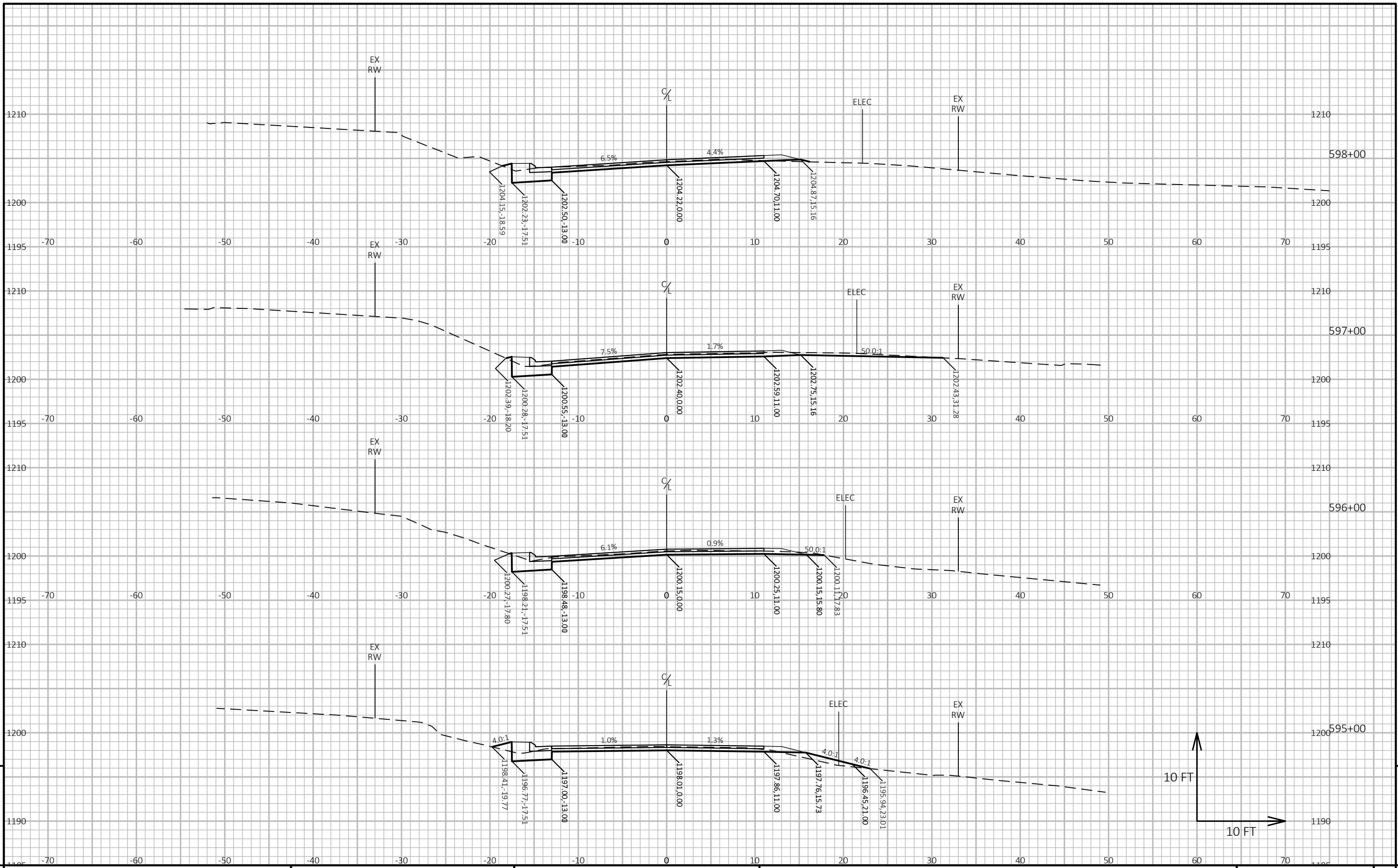
9

9



BEGIN CURB & GUTTER SECTION LT





PROJECT NO: 5790-02-72

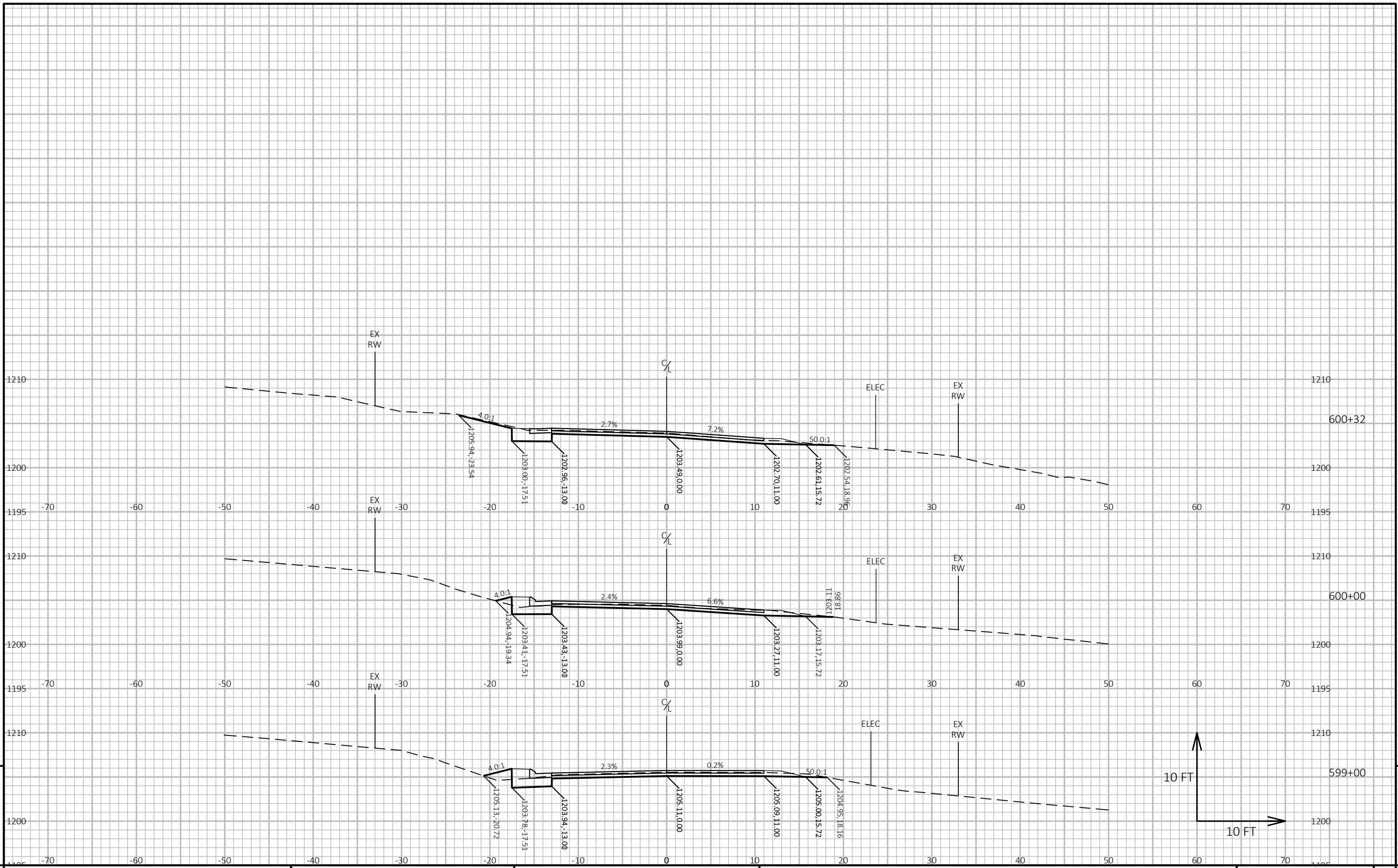
HWY: STH 171

COUNTY: CRAWFORD

CROSS SECTIONS: STH 171

SHEET

E



PROJECT NO: 5790-02-72

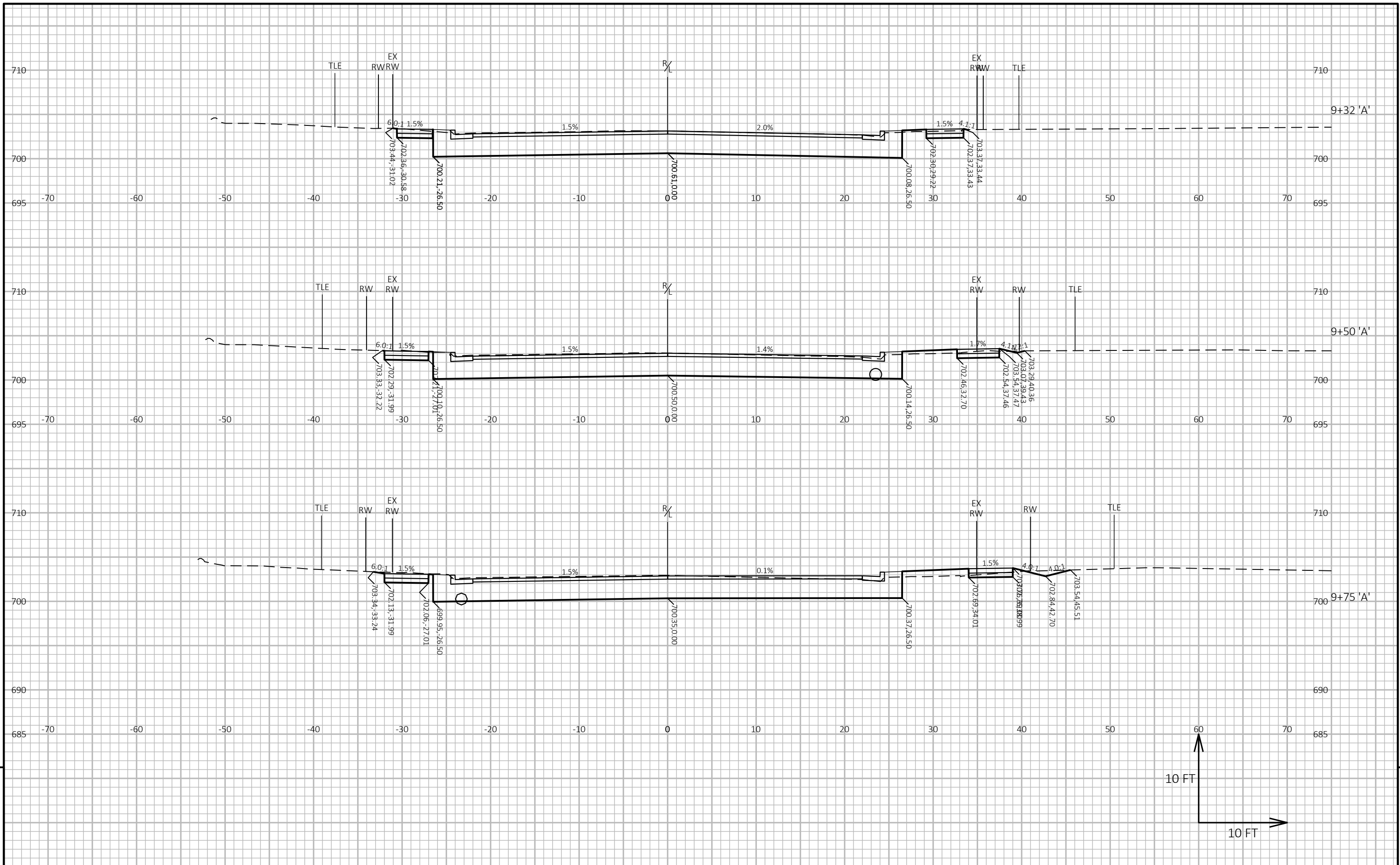
HWY: STH 171

COUNTY: CRAWFORD

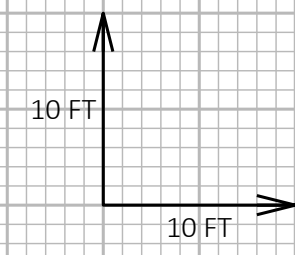
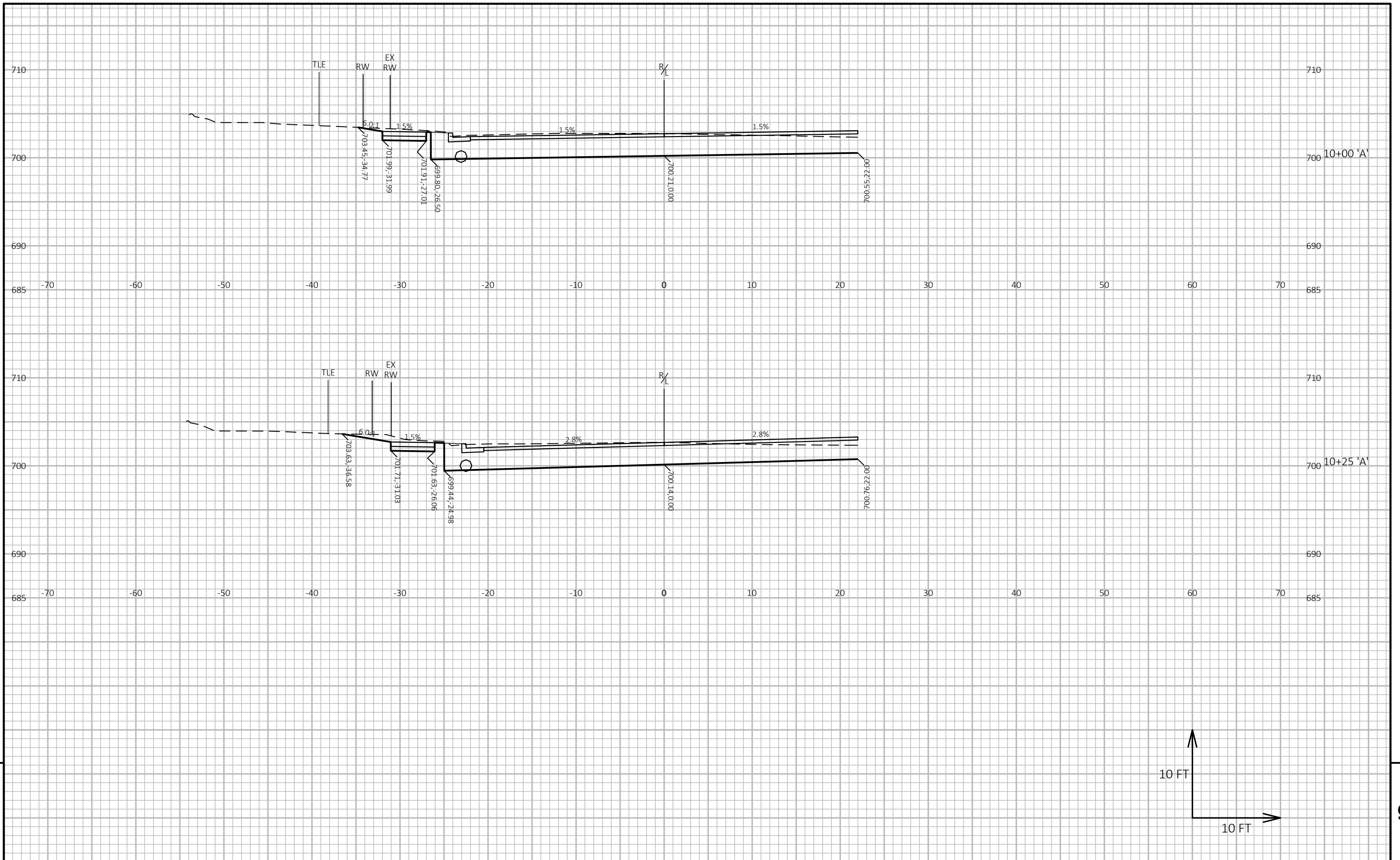
CROSS SECTIONS: STH 171

SHEET

E



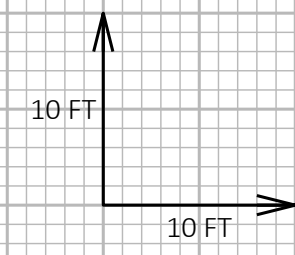
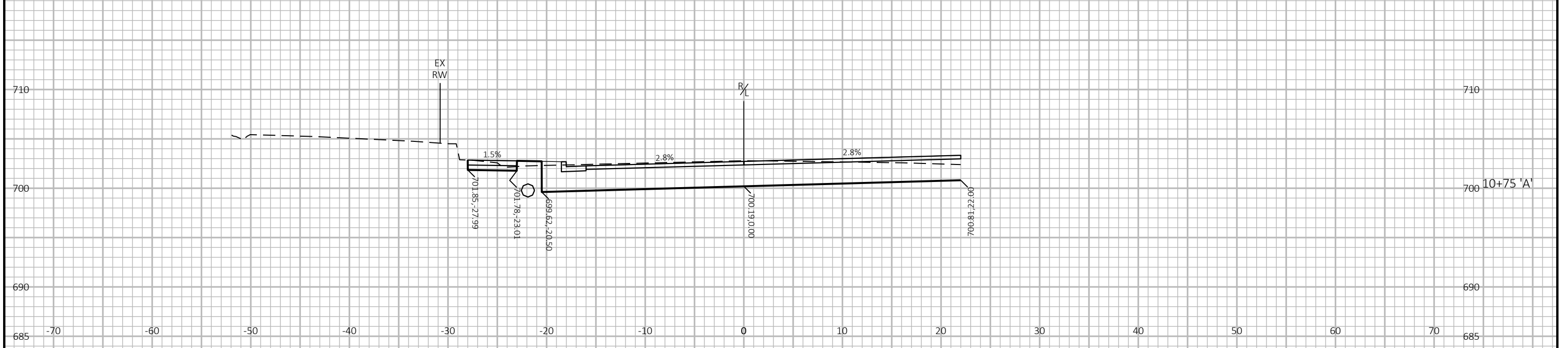
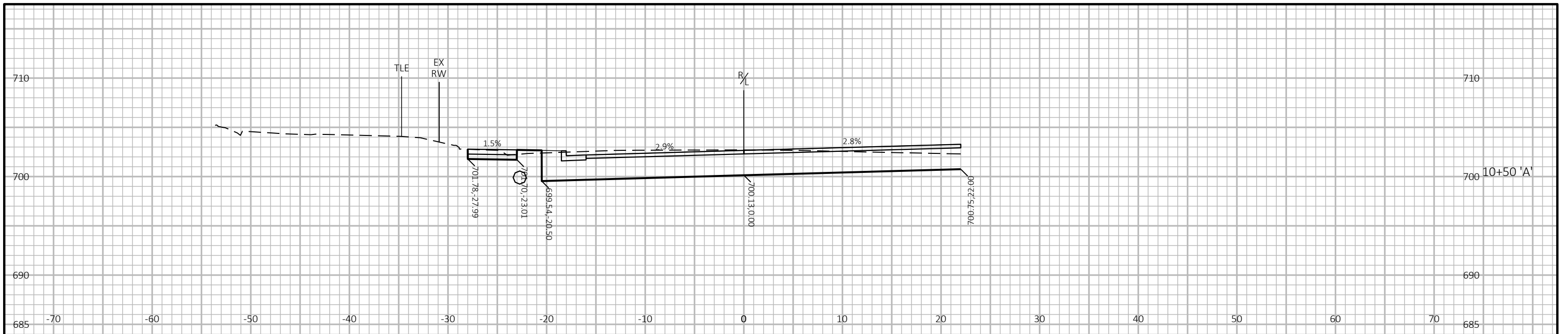
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 & 131 'A' SHEET E



9

9

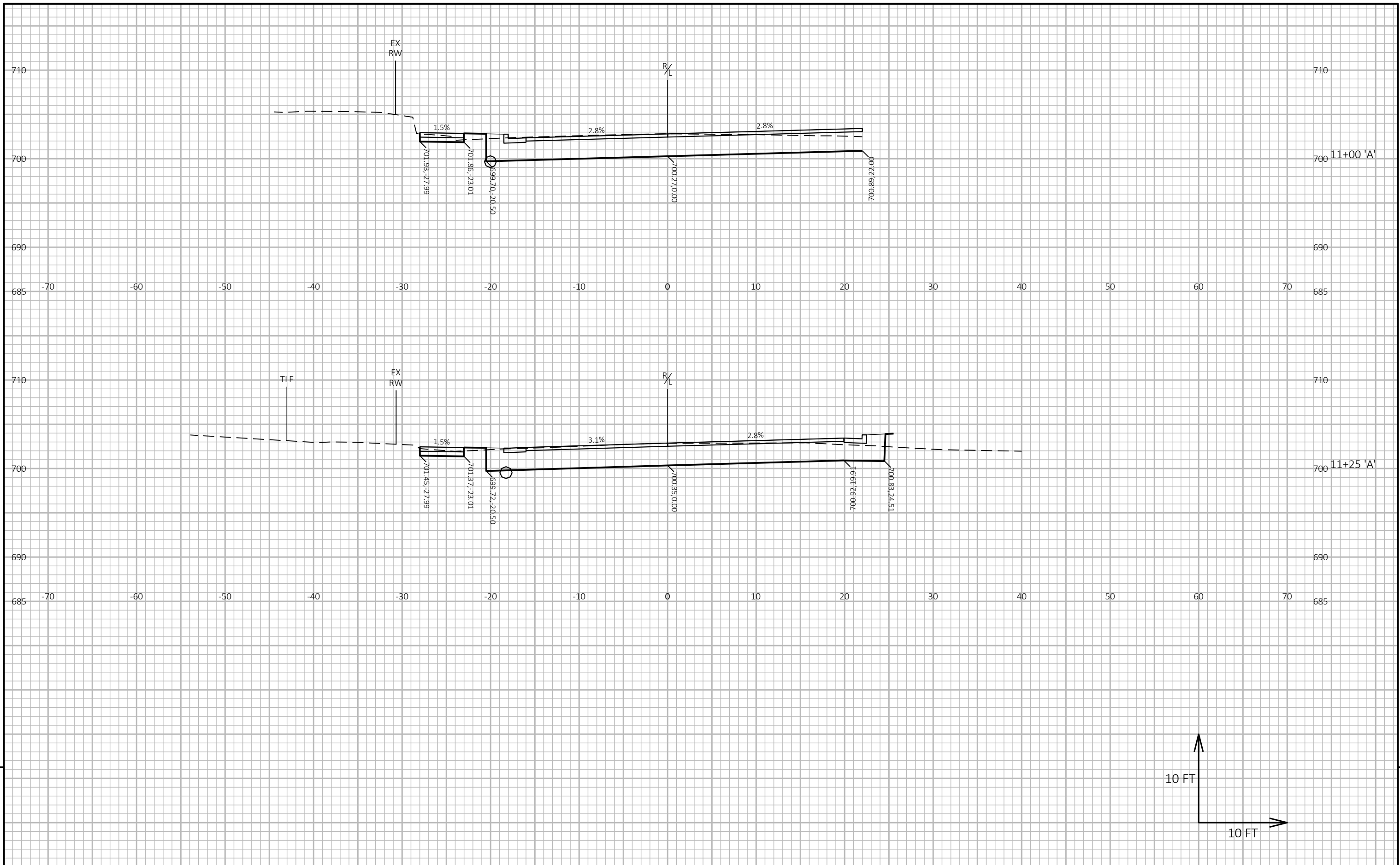
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171 & 131 'A'	SHEET	E
------------------------	--------------	------------------	-----------------------------------	-------	---



9

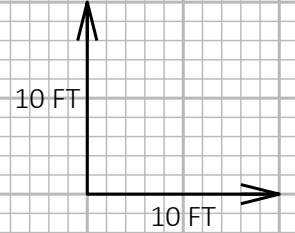
9

PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171 & 131 'A'	SHEET	E
------------------------	--------------	------------------	-----------------------------------	-------	---

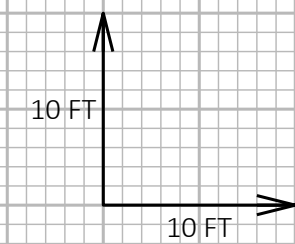
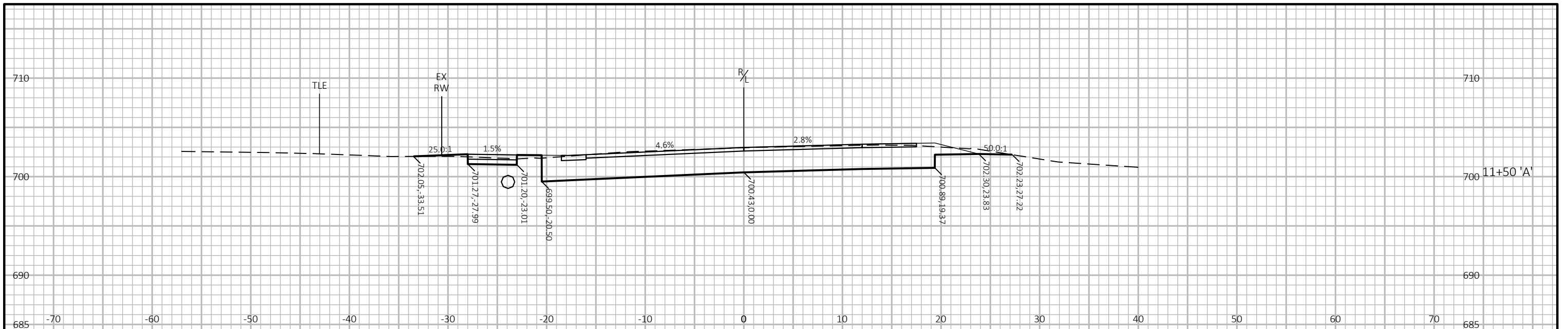


9

9



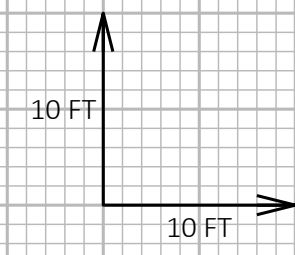
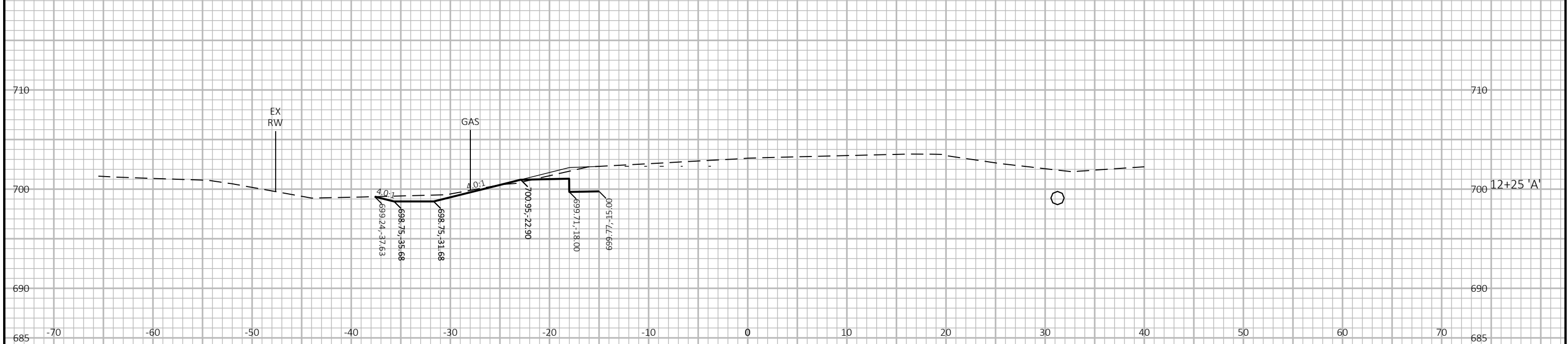
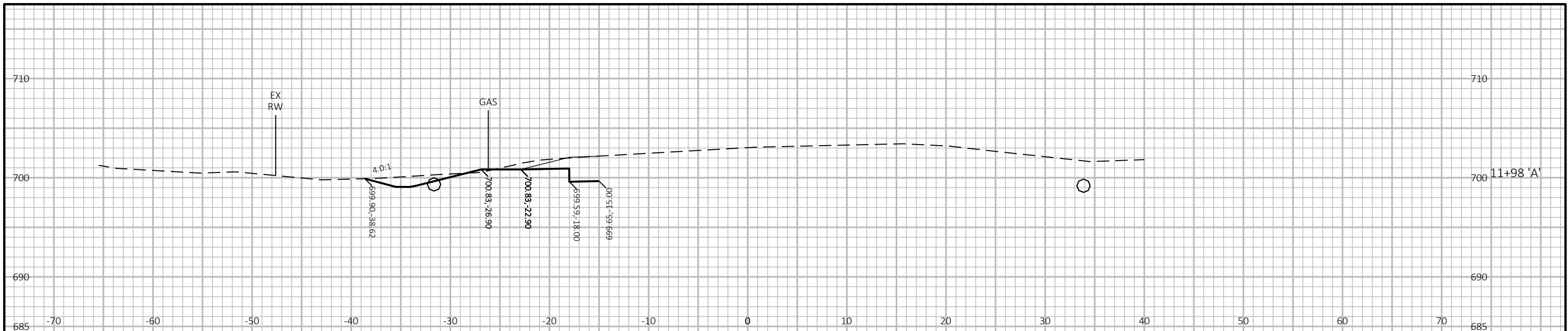
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171 & 131 'A'	SHEET
------------------------	--------------	------------------	-----------------------------------	-------



9

9

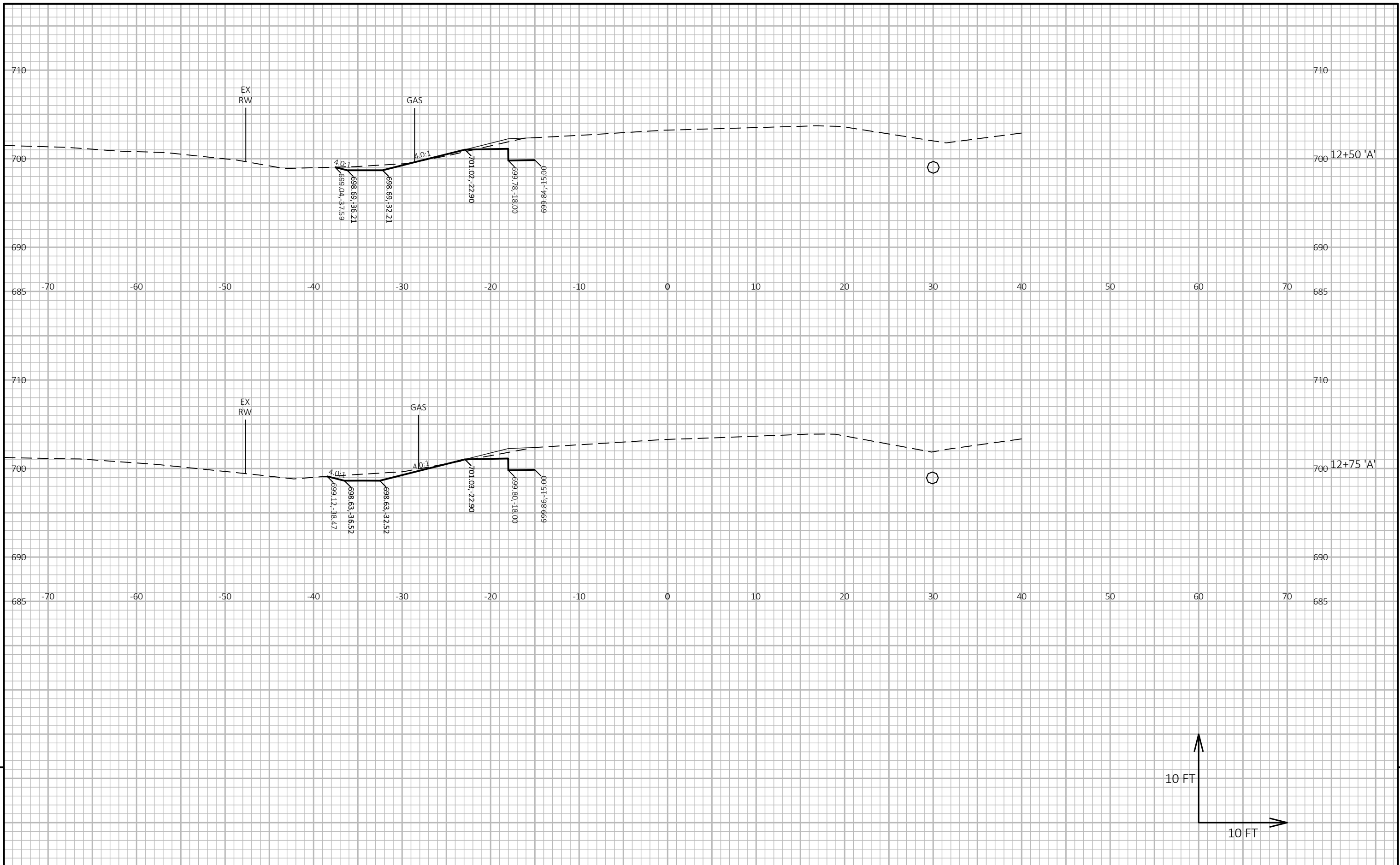
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171 & 131 'A'	SHEET
------------------------	--------------	------------------	-----------------------------------	-------



9

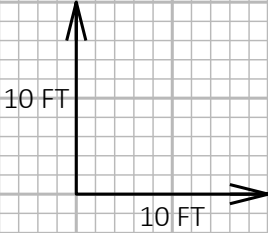
9

PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171 & 131 'A'	SHEET	E
------------------------	--------------	------------------	-----------------------------------	-------	---

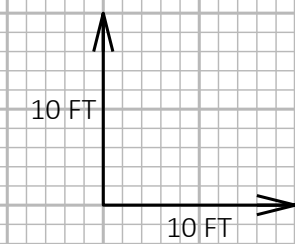
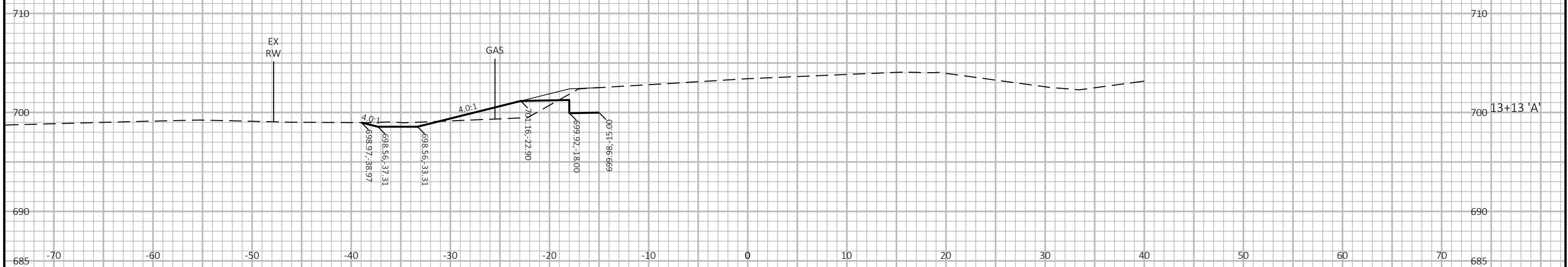
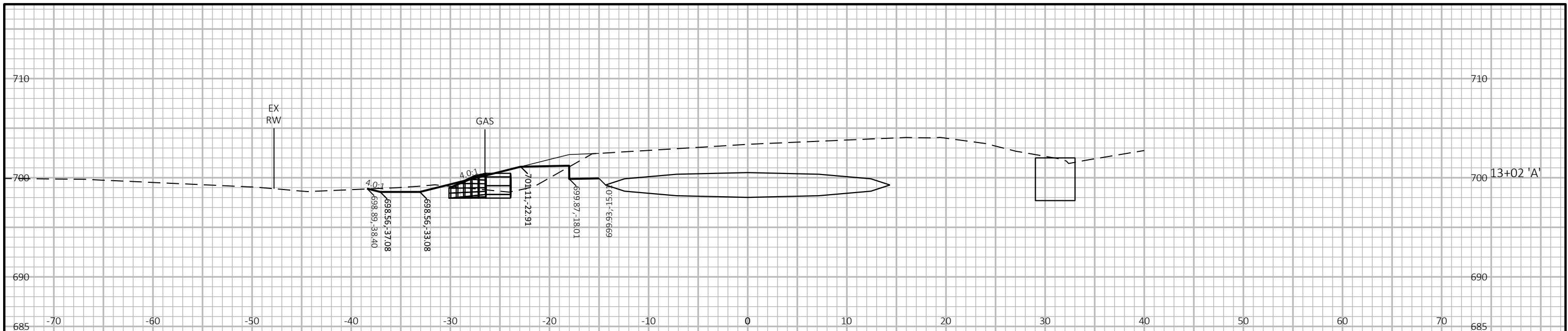


9

9



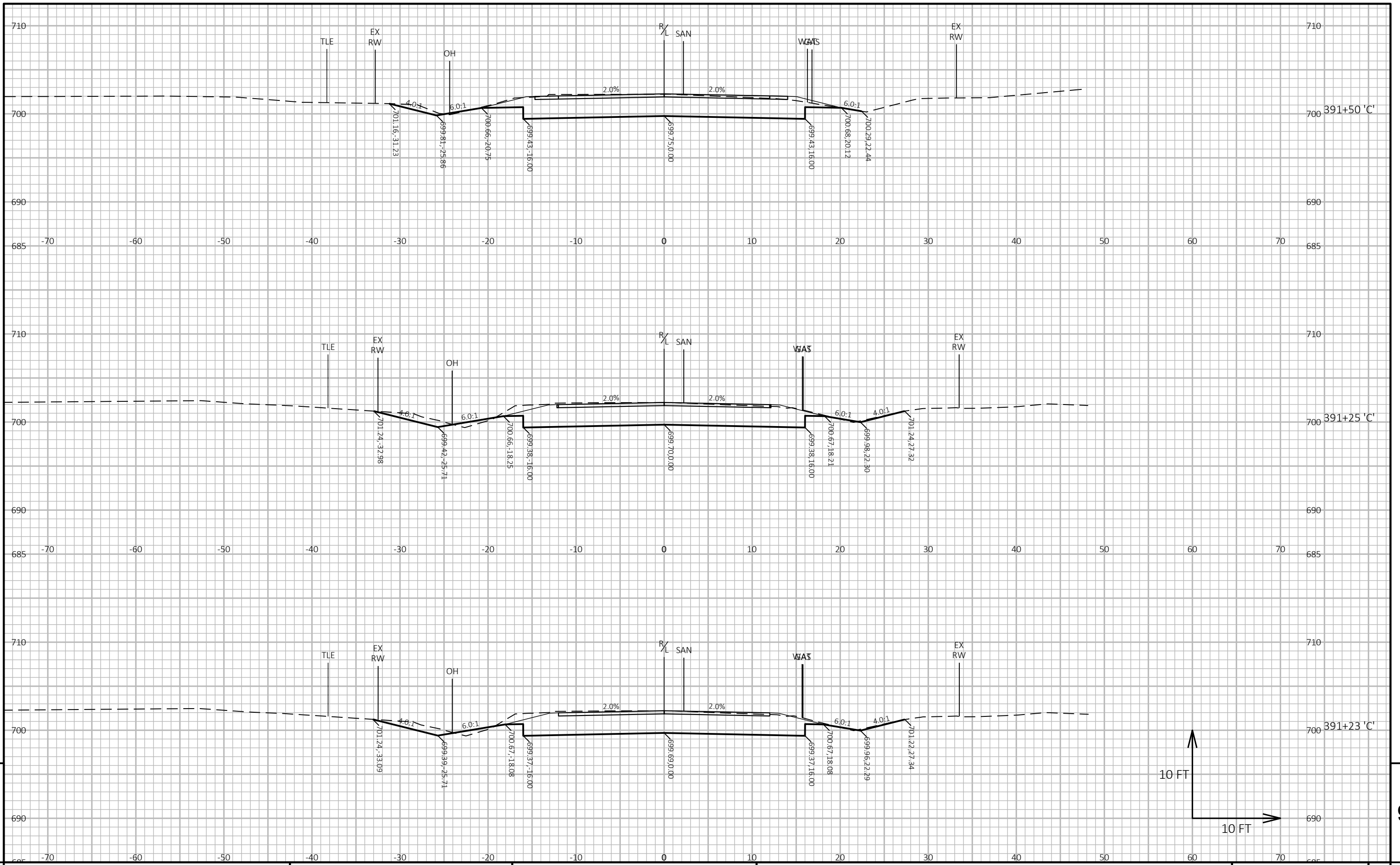
PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171 & 131 'A'	SHEET	E
------------------------	--------------	------------------	-----------------------------------	-------	---



9

9

PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171 & 131 'A'	SHEET	E
------------------------	--------------	------------------	-----------------------------------	-------	---



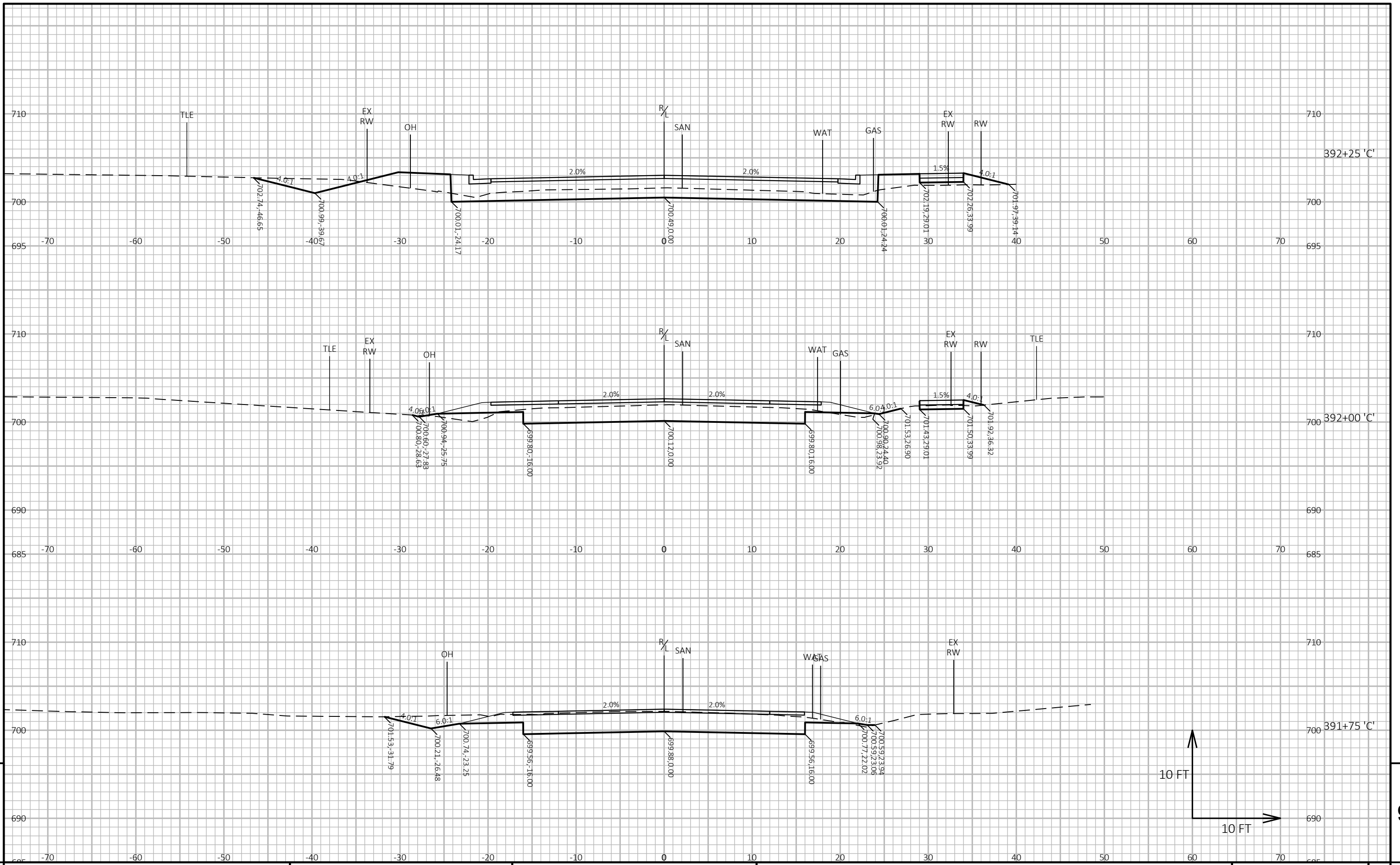
9

9

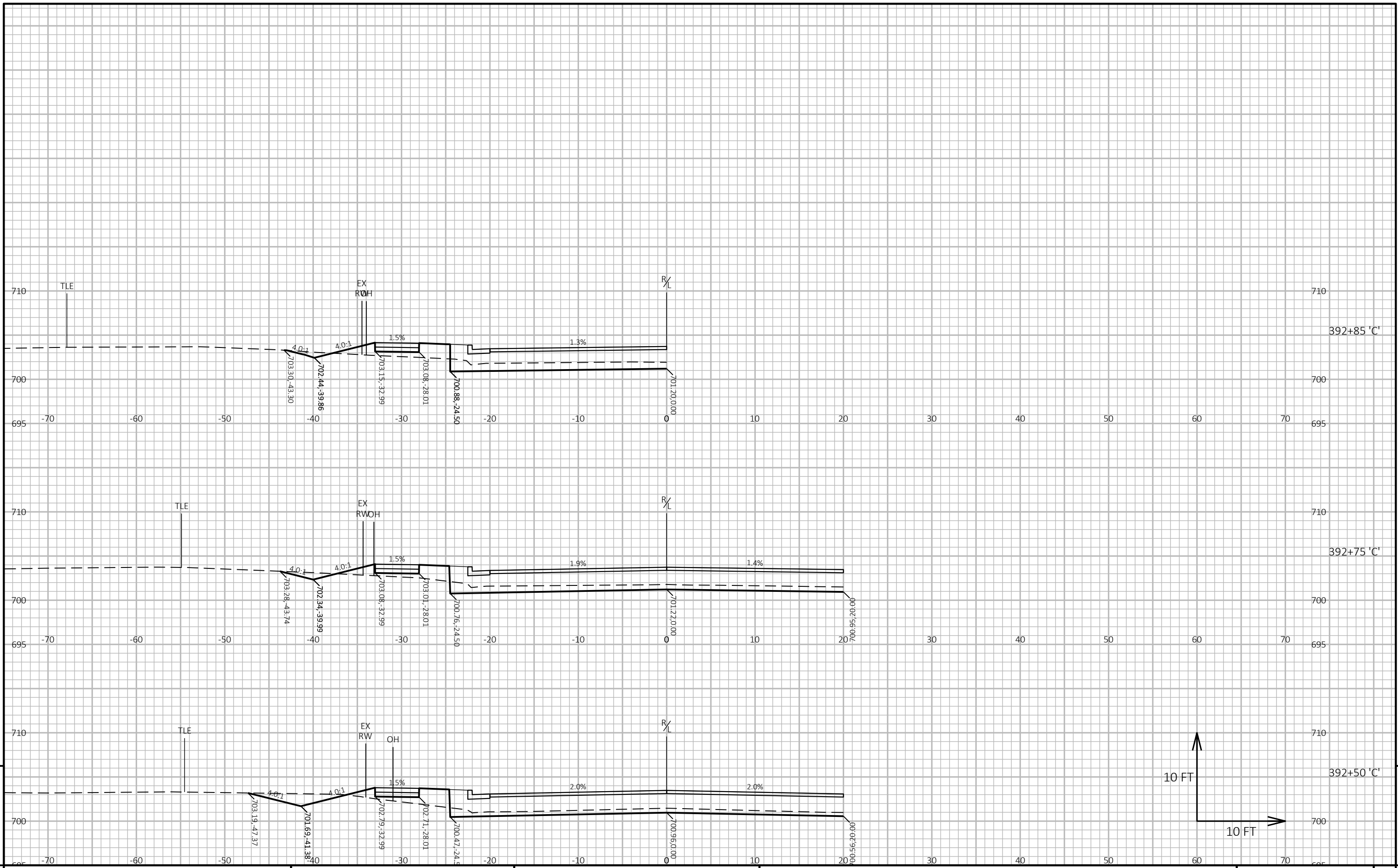
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 131 'C' SHEET E

FILE NAME: \\SP-PZ1.SEHINC.COM\PROJECTS\UZ\W\WITSW\162034\5-FINAL-DSGN\51-DRAWINGS\10-CIVIL\C3D\57900202\SHEETS\PLAN\090105_XS.DWG PLOT DATE: 7/19/2024 1:20 PM PLOT BY: SAVANNAH STEHN PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

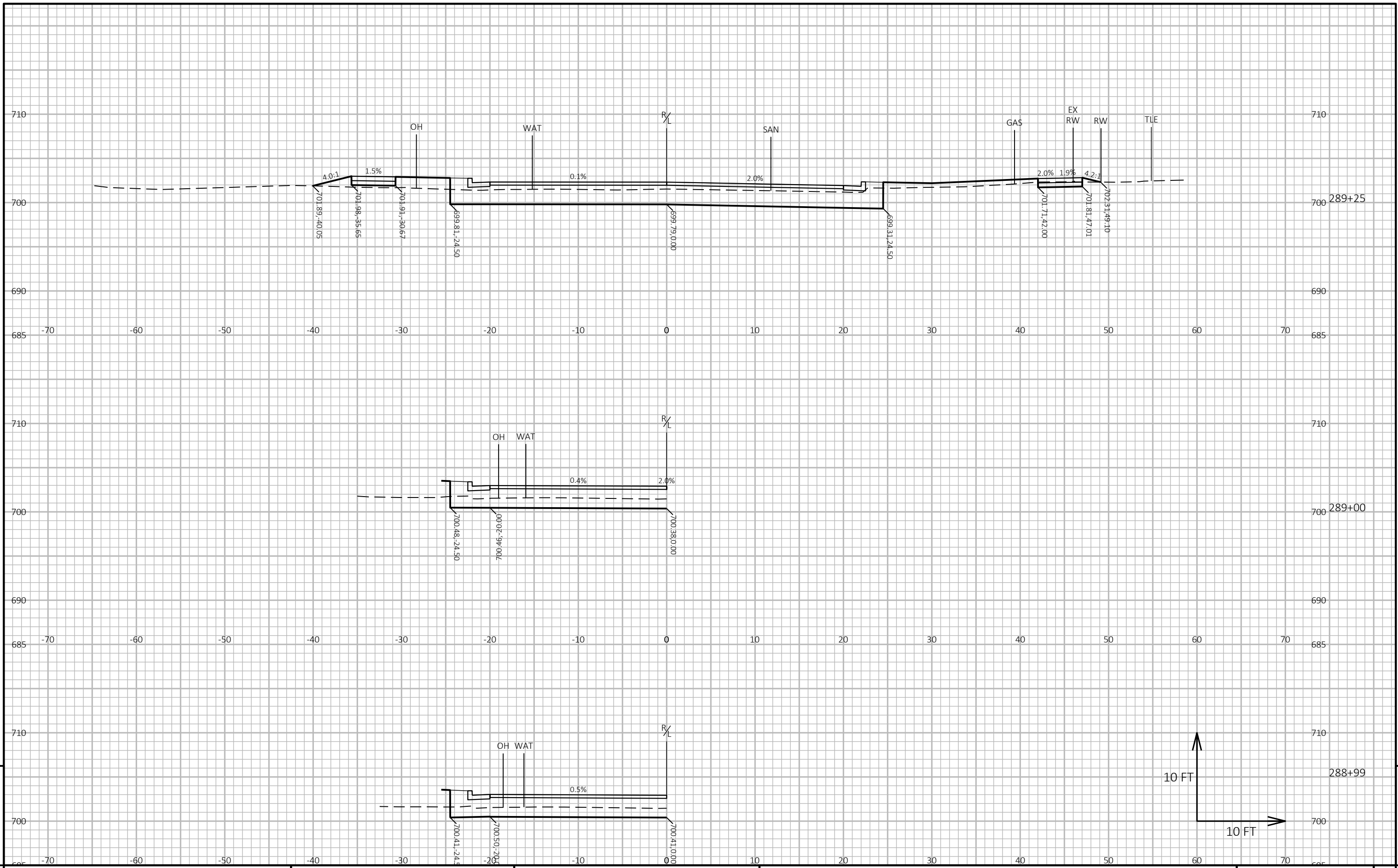
LAYOUT NAME - C1



PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 131 'C' SHEET 9



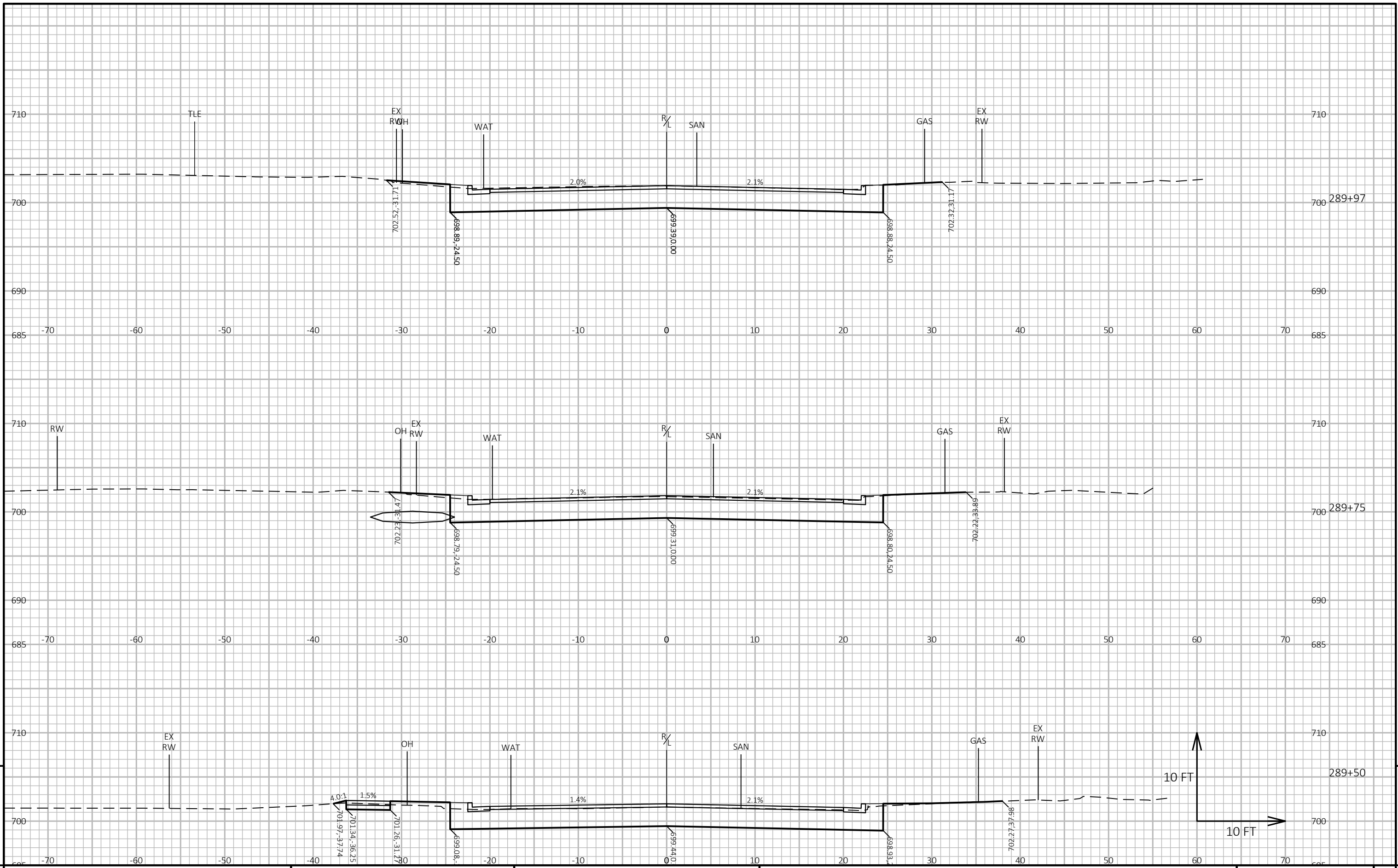
PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 131 'C' SHEET E



PROJECT NO: 5790-02-72	HWY: STH 171	COUNTY: CRAWFORD	CROSS SECTIONS: STH 171	SHEET	E
------------------------	--------------	------------------	-------------------------	-------	---

9

9

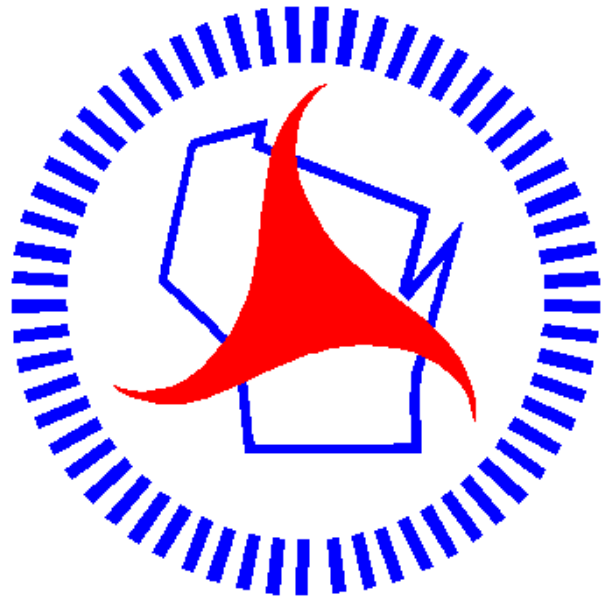


PROJECT NO: 5790-02-72 HWY: STH 171 COUNTY: CRAWFORD CROSS SECTIONS: STH 171 SHEET E

9

9

Notes



Wisconsin Department of Transportation

Dedicated people creating transportation solutions through innovation and exceptional service.

<http://www.dot.wisconsin.gov>