GRE

PROJECT ID: 6090-14-71

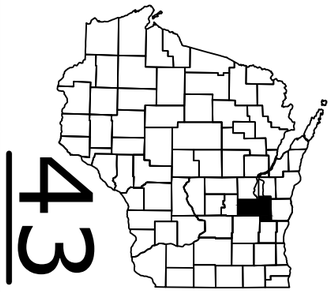
WITH: N/A

COUNTY: FOND DU LAC

MAY 2025

ORDER OF SHEETS

TOTAL SHEETS = 86



N

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

BRANDON-RIPON

WVL BRANDON-STH 23

STH 49

FOND DU LAC COUNTY

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
6090-14-71	WISC 2025508	1

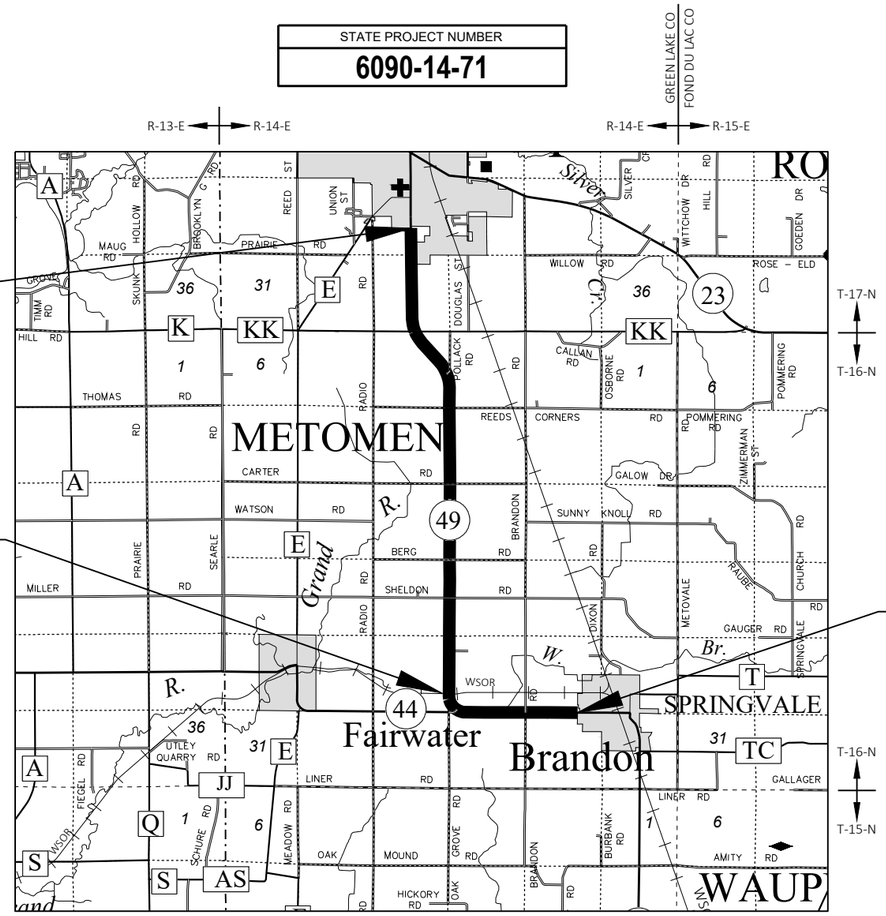
STATE PROJECT NUMBER

6090-14-71

END PROJECT
STA 912+13.00
Y = 404,900.454
X = 714,308.564

NET EXCEPTION TO CL LENGTH
STA 570+96 - STA 571+04

BEGIN PROJECT
STA 473+58.01
Y = 370,308.621
X = 725,943.940



LAYOUT

SCALE 0 2 MI

TOTAL NET LENGTH OF CENTERLINE = 8.299 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), FOND DU LAC COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

DESIGN DESIGNATION

A.A.D.T.	=	2,090 (2027)
A.A.D.T.	=	2,120 (2047)
D.H.V.	=	271
D.D.	=	60-40
T.	=	15.2%
DESIGN SPEED	=	55 MPH
ESALS	=	620,000

CONVENTIONAL SYMBOLS

PLAN

- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE (300'EB')
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE

- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES

- ELECTRIC (E)
- FIBER OPTIC (FO)
- GAS (G)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	NE REGION
Designer	E. HOEFFERLE
Project Manager	R ERDMANN
Regional Examiner	
Regional Supervisor	R. WAGNER

APPROVED FOR THE DEPARTMENT

DATE: 10/30/24 (Signature)

E

FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\010101-TI.DWG PLOT DATE : 10/16/2024 11:13 AM PLOT BY : MILLER, ELIZABETH A PLOT NAME :

2

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

RIGHT OF WAY INFORMATION SHOWN ON THE PLANS IS APPROXIMATE.

BEARINGS SHOWN ARE BASED ON THE COUNTY COORDINATE SYSTEM.

CURVE DATA IS BASED ON THE ARC DEFINITION.

RADIUS DIMENSIONS FOR THE CURB AND GUTTER ARE TO THE FLANGE LINE UNLESS OTHERWISE NOTED.

CURB AND GUTTER PLAN GRADES ARE AT THE FLANGE LINE UNLESS OTHERWISE NOTED.

TOPSOIL SHALL BE PLACED 1-INCH BELOW THE TOP OF ADJACENT CONCRETE CURBS OR SIDEWALKS.

WHEN THE QUANTITY OF THE ITEMS OF BASE AGGREGATE, SUBBASE OR HMA PAVEMENT IS MEASURED FOR PAYMENT BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE LAYERS SHOWN ON THE PLAN IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

THE EXACT LOCATION AND WIDTH OF DRIVEWAYS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. DRIVEWAYS SHALL BE REPLACED IN KIND UNLESS DIRECTED BY THE ENGINEER OR AS SHOWN IN THE PLANS.

THE PROPOSED SHOULDER WIDTH SHOWN IN THE TYPICAL SECTIONS ARE MINIMUM WIDTH. PERPETUATE EXISTING SHOULDERS THAT ARE WIDER THAN WHAT IS SHOWN IN THE TYPICAL SECTIONS.

PRIOR TO PLACEMENT OF BEAM GUARD THE SHOULDERS SHALL BE IN PLACE, SHAPED AND COMPACTED.

CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS AT ALL TIMES EXCEPT WHEN PAVING OR PIPE LAYING OPERATIONS REQUIRE THE DRIVEWAY TO BE CLOSED. ACCESS TO DRIVEWAYS SHALL BE RE-ESTABLISHED IMMEDIATELY AFTER OPERATIONS ARE COMPLETED. ACCESS SHALL BE PROVIDED DURING ALL NON-WORKING HOURS.

DO NOT DRIVE OR STORE EQUIPMENT, OR STORE CONSTRUCTION MATERIALS IN ENVIRONMENTALLY SENSITIVE AREAS, WETLANDS, FLOODPLAINS, OR WATERWAYS.

ORDER OF SECTION 2 DETAIL SHEETS

CONTACTS

WISCONSIN DNR LIAISON

MARTY DILLENBURG
625 E COUNTY ROAD Y, SUITE 70
OSHKOSH, WI 54901-9731
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marty.dillenburg@wisconsin.gov

FOND DU LAC COUNTY HIGHWAY COMMISSIONER

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1820 SOUTH HICKORY STREET
FOND DU LAC, WI 54937
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NE REGION DESIGN PROJECT MANAGER

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ryand.erdmann@dot.wi.gov

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adannhoff@cityofripon.com

ALLYN DANNHOFF
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RIPON, WI 54971-1396
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adannhoff@cityofripon.com

CODY NELSEN
FLINT HILLS RESOURCES, LC - GAS/PETROLEUM
E 3660 SPRUCE ROAD
ELEVA, WI 54738
CELL: (660) 251-1981
Cody.Nelsen@fhr.com

TODD HILDEBRANDT
SPECTRUM - COMMUNICATION LINE
3545 PLANK RD.
APPLETON, WI 54915
OFFICE: (920) 831-9255
Todd.Hildebrandt@charter.com

RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS:	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIPTURF:	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPETURF:			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT:	.70 - .95											
CONCRETE:	.80 - .95											
BRICK:	.70 - .80											
DRIVES, WALKS:	.75 - .85											
ROOFS:	.75 - .95											
GRAVEL ROADS, SHOULDERS:	.40 - .60											

TOTAL PROJECT AREA = 33 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.013 ACRES

DIGGERS HOTLINE
Dial 811 or (800)242-8511
www.DiggersHotline.com

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	GENERAL NOTES	SHEET 2	E

FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\020101-GN.DWG
LAYOUT NAME - 020101-gn
PLOT DATE : 10/29/2024 2:19 PM PLOT BY : CAMPSHURE, MICHAEL R PLOT NAME : PLOT SCALE : 1:1

2

N

CTH E

RADIO ROAD

STH 44

WISCONSIN & SOUTHERN RAILROAD

STA 612+00
ENVIRONMENTALLY SENSITIVE SITE

SHELDON RD

BERG RD

CARTER RD

REEDS CORNERS RD

CTH KK

RINGSTAD DR

850

STH 44/STH 49

900

CITY OF RIPON

OAK GROVE ROAD

600

650

700

750

800

POLLACK RD

END PROJECT
STA 912+13.00
Y=404,900.454
X=714,308.564

STH 49/STH 44

TOLSMA DR

550

NET EXCEPTION TO CL LENGTH
STA 570+96 - STA 571+04

STH 49

BRANDON RD

BRANDON RD

500

BEGIN PROJECT
STA 473+58.01
Y=370,308.621
X=725,943.940

DIXON RD

VILLAGE OF BRANDON

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PROJECT OVERVIEW	SHEET 3	E

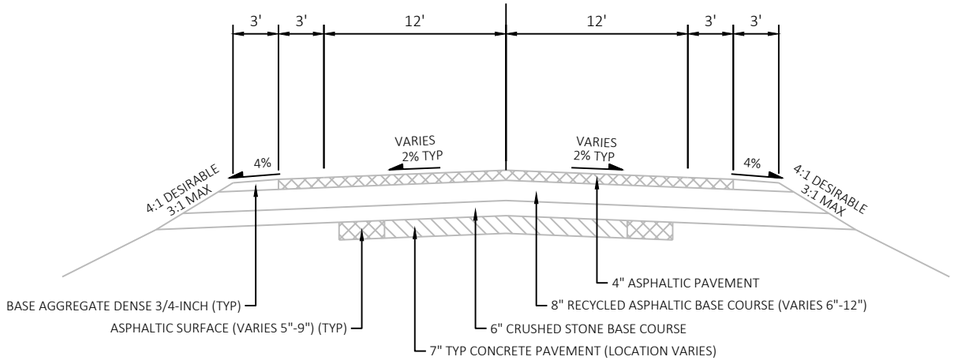
FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\020201-PO.DWG
LAYOUT NAME - 020201-po

PLOT DATE : 8/8/2024 8:57 AM PLOT BY : CAMPSHURE, MICHAEL R PLOT NAME : PLOT SCALE : 1 IN:0.5 MI

WISDOT/CADDS SHEET 42

2

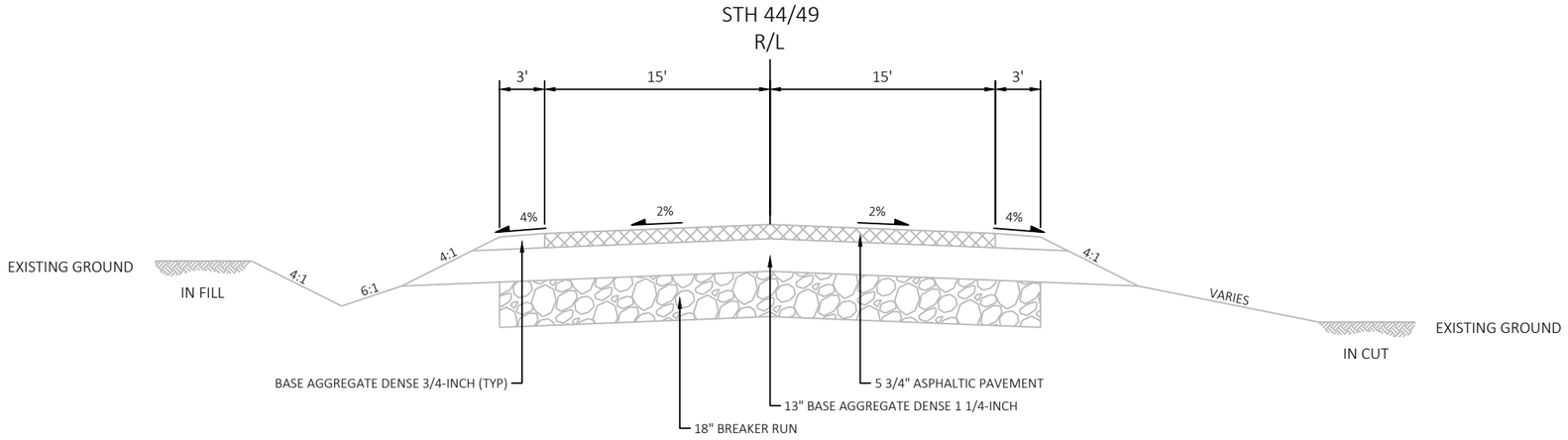
STH 44/49
R/L



EXISTING TYPICAL SECTION STH 44/49

STA 473+58 TO STA 905+00

STH 44/49
R/L



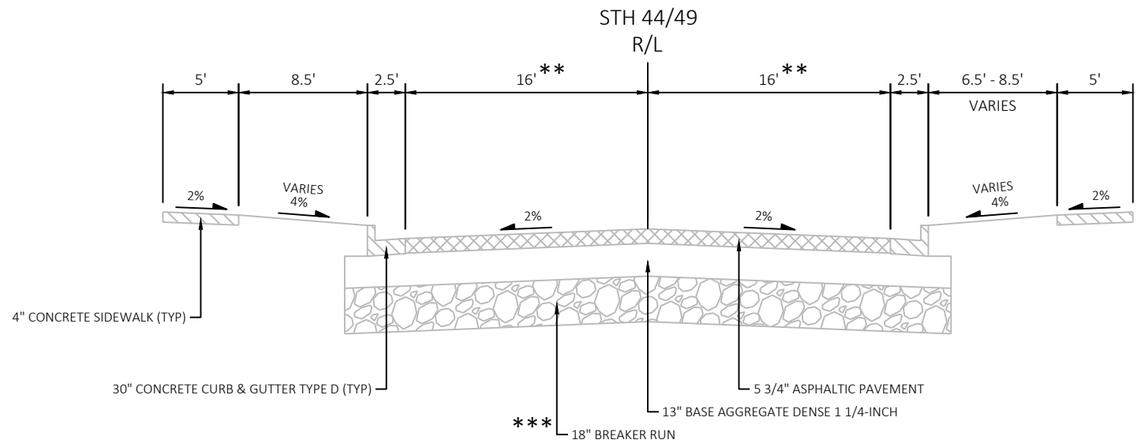
EXISTING TYPICAL SECTION STH 44/49

STA 905+00 TO STA 906+59.57

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	TYPICAL SECTIONS	SHEET 4	E

FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\020301-TS.DWG
LAYOUT NAME - 020301-ts
PLOT DATE : 10/29/2024 1:30 PM
PLOT BY : CAMPSHURE, MICHAEL R
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42

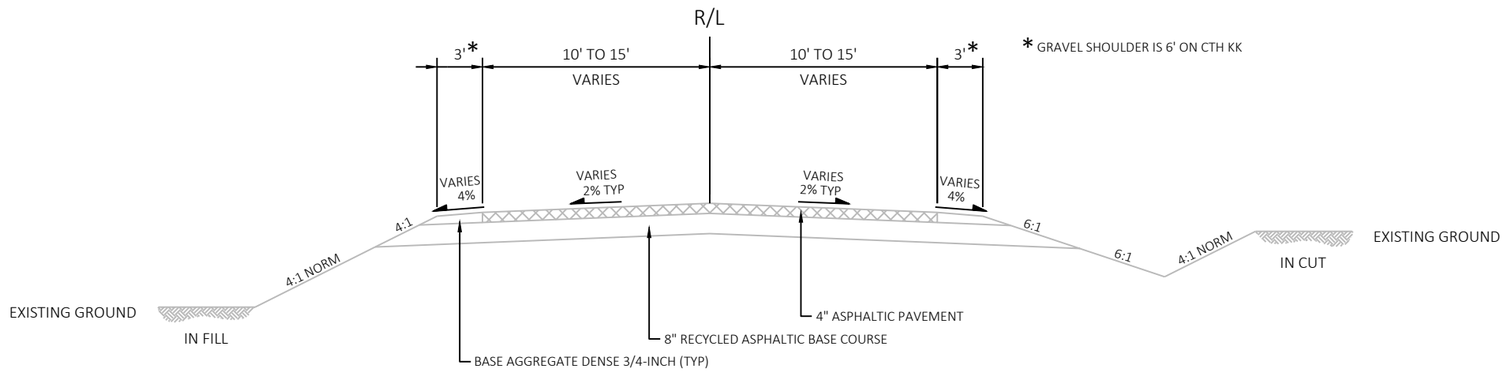
2



** 24' FROM STA 906+59.57 TO STA 910+00 VARIES FROM STA 910+00 TO STA 911+59.85
*** 30" FROM STA 951+00 TO STA 954+38.62

EXISTING TYPICAL SECTION STH 44/49

STA 906+59.57 TO STA 912+13.00

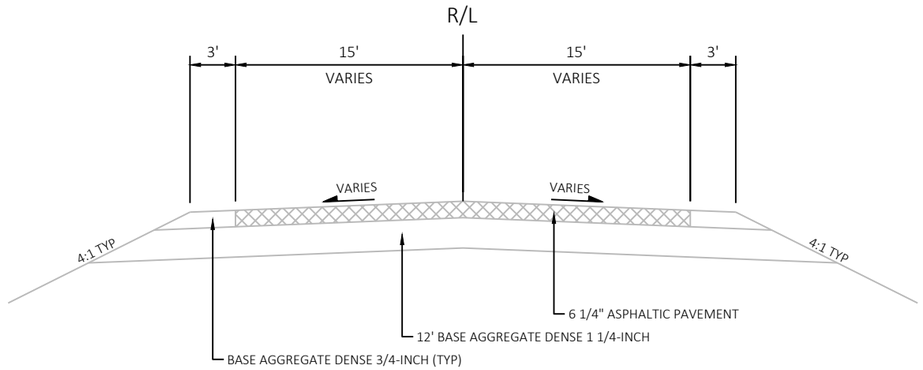


EXISTING TYPICAL SECTION

BRANDON ROAD
STH 44
SHELDON ROAD
BERG ROAD
CARTER ROAD
REEDS CORNERS ROAD
POLLACK ROAD
CTH KK

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	TYPICAL SECTIONS	SHEET 5	E

2



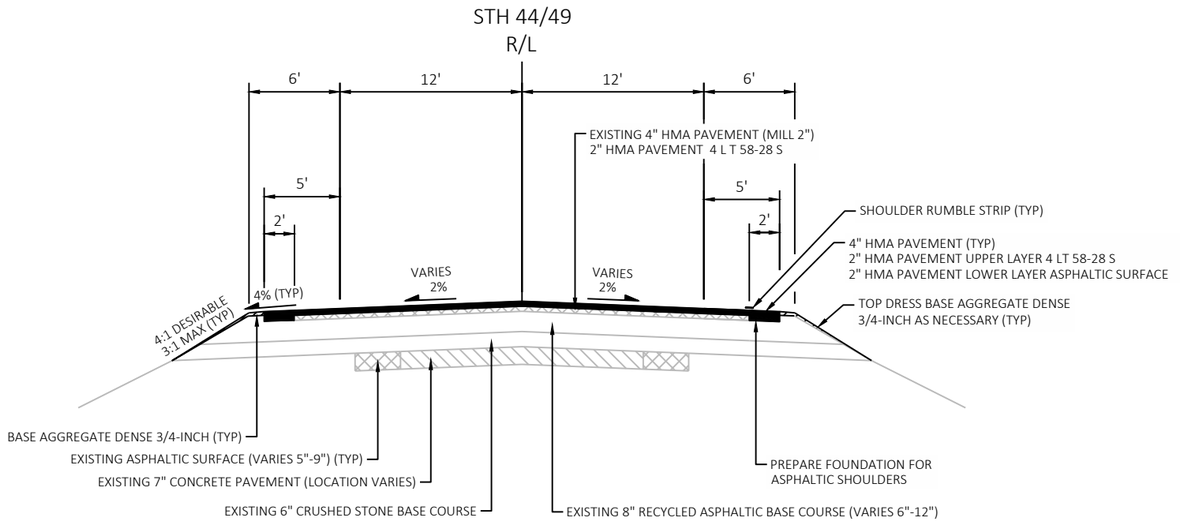
EXISTING TYPICAL SECTION

OAK GROVE ROAD

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	TYPICAL SECTIONS	SHEET 6	E

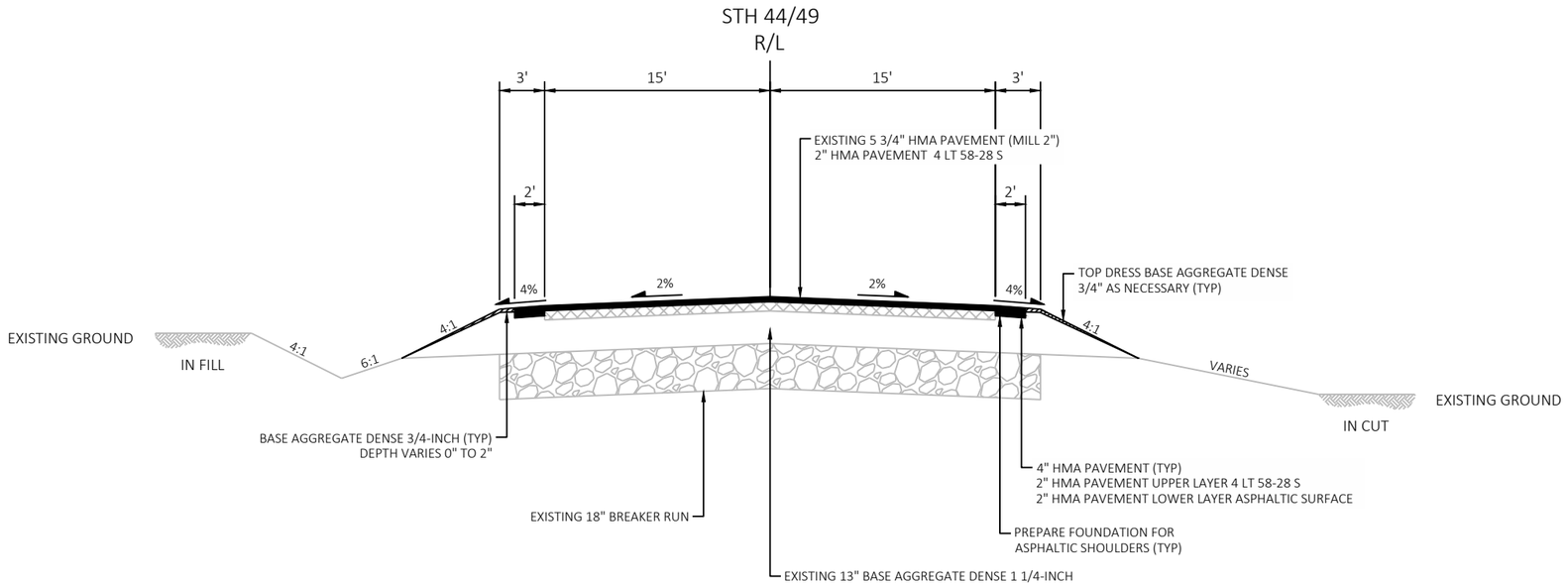
FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\020301-TS.DWG
LAYOUT NAME - 020303-ts

PLOT DATE : 10/29/2024 1:30 PM PLOT BY : CAMPSHURE, MICHAEL R PLOT NAME : PLOT SCALE : 1 IN:10 FT



FINISHED TYPICAL SECTION STH 44/49

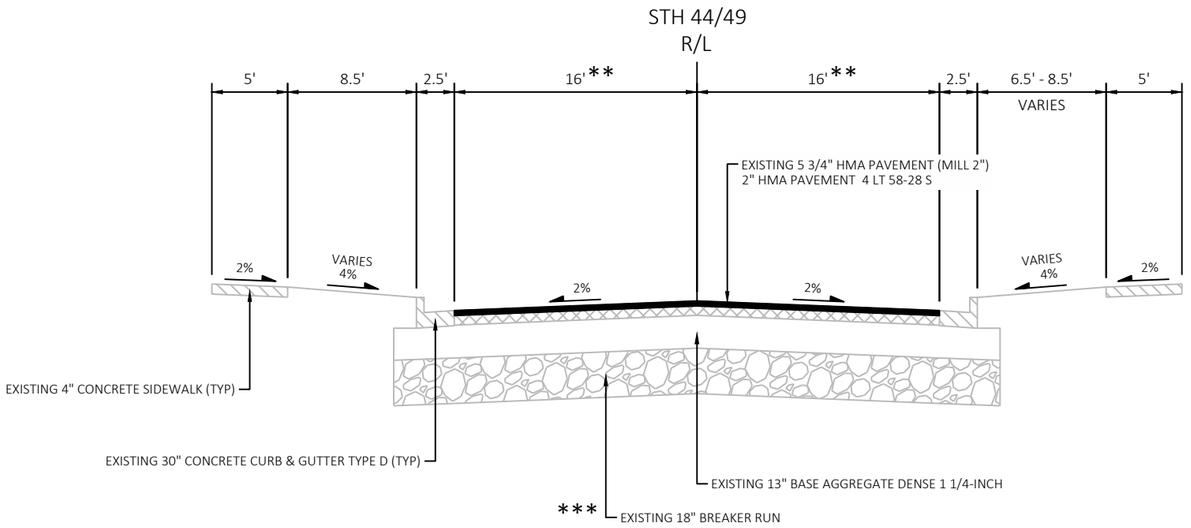
STA 473+58 TO STA 570+96
STA 571+04 TO STA 905+00



FINISHED TYPICAL SECTION STH 44/49

STA 905+00 TO STA 906+59.57

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	TYPICAL SECTIONS	SHEET 7	E

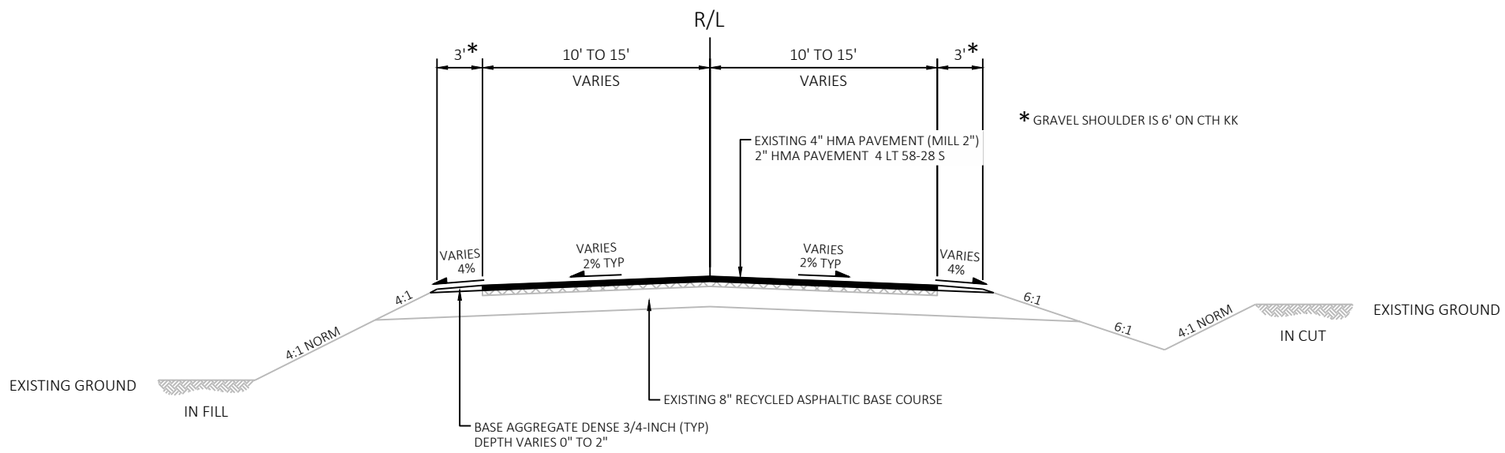


** 24' FROM STA 906+59.57 TO STA 910+00 VARIES FROM STA 910+00 TO STA 911+59.85

*** 30" FROM STA 951+00 TO STA 954+38.62

FINISHED TYPICAL SECTION STH44/49

STA 906+59.57 TO STA 912+13.00

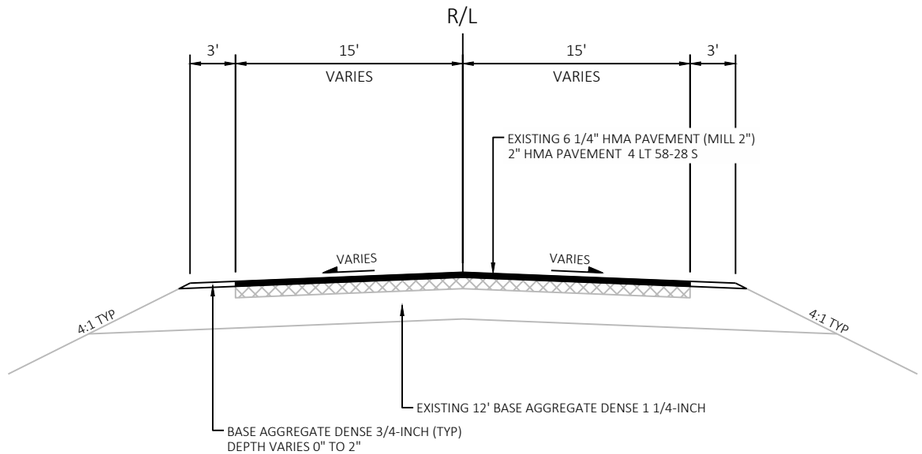


FINISHED TYPICAL SECTION

BRANDON ROAD
STH 44
SHELDON ROAD
BERG ROAD
CARTER ROAD
REEDS CORNERS ROAD
POLLACK ROAD
CTH KK

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	TYPICAL SECTIONS	SHEET 8	E

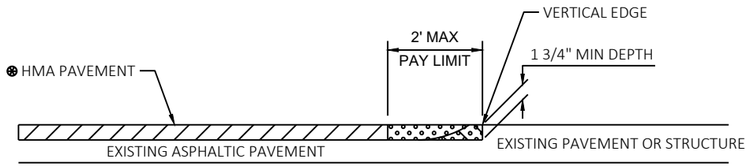
2



FINISHED TYPICAL SECTION

OAK GROVE ROAD

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	TYPICAL SECTIONS	SHEET 9	E



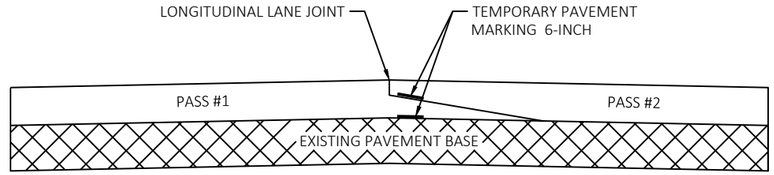
REMOVING ASPHALTIC SURFACE, MILLING

REMOVING ASPHALTIC SURFACE, BUTT JOINTS

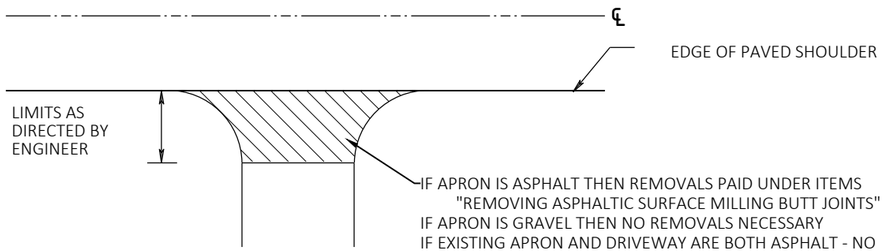
REMOVE ASPHALTIC SURFACE WEDGE AT BUTT JOINT TO CREATE VERTICAL EDGE

SEE TYPICAL CROSS SECTION FOR PAVEMENT TYPE AND THICKNESS OF INDIVIDUAL LAYERS

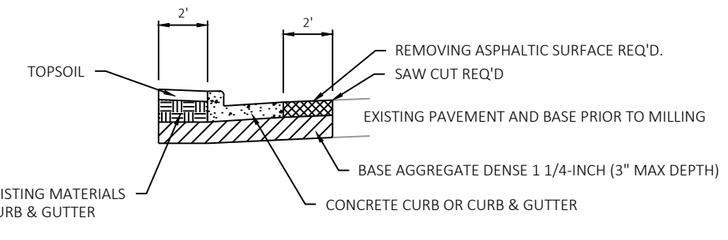
BUTT JOINT DETAIL FOR ASPHALTIC PAVEMENTS (NO PROFILE CHANGE)



PAVEMENT MARKING DETAIL FOR TAPERED OVERLAPPING JOINTS IN HMA PAVEMENTS



RURAL DRIVEWAY DETAIL - NO GRADE CHANGE



NOTES:

BASE COURSE BELOW PROPOSED CURB & GUTTER SHALL BE CONSTRUCTED TO PROVIDE A SUITABLE BASE AS DETERMINED BY THE ENGINEER IN THE FIELD

IN LOCATIONS WHERE SIDEWALK IS PROPOSED BEHIND THE BACK OF CURB & GUTTER, A MAXIMUM OF 3" BASE AGGREGATE DENSE 1 1/4-INCH SHALL BE USED FOR A SUITABLE BASE

CONCRETE CURB & GUTTER REPLACEMENT DETAIL

2

N

*AERIAL MAPPING IS NOT TO SCALE
(BEST FIT)

WISCONSIN & SOUTHERN RAILROAD

EXISTING RR BUNGALOW

EXISTING R/W (TYP)

PT: 569+89.59

15'

4' 3"

PROPOSED SIGNAL/GATE
(WORK BY RAILROAD)

EXISTING RR SIGNAL

EDGE OF GRAVEL SHOULDER

EDGE OF PAVEMENT

LANE EDGELINE

3'

3'

12'

STH 44/STH 49

569

570

571

572

573

574

STH 44/STH 49

4' 3"

40.63'

12'

15'

PROPOSED SIGNAL/GATE
(WORK BY RAILROAD)

EXISTING RR SIGNAL

3:1 TAPER
(TYPICAL)

ROADSIDE CLEARING

EXISTING R/W

WISCONSIN & SOUTHERN RAILROAD

EXISTING R/W

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PLAN DETAIL - WISCONSIN & SOUTHERN RAILROAD CROSSING	SHEET 11	E

2

EXISTING R/W

Ringstad_NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1	907+28.74	65.97' LT	976.03	404415.53	714247.60
2	907+29.85	56.20' LT	975.60	404416.75	714257.36
3	907+31.63	50.47' LT	975.33	404418.59	714263.06
4	907+33.93	45.53' LT	975.11	404420.94	714267.98
5	907+37.39	40.21' LT	975.05	404424.46	714273.27
6	907+40.09	37.01' LT	974.81	404427.19	714276.44
7	907+46.07	31.71' LT	974.50	404433.22	714281.68
8	907+51.15	28.53' LT	974.27	404438.33	714284.81
9	907+56.63	26.10' LT	974.04	404443.84	714287.18
10	907+30.74	65.97' LT	975.95	404417.53	714247.58
11	907+31.80	56.66' LT	975.52	404418.69	714256.88
12	907+33.49	51.20' LT	975.25	404420.44	714262.32
13	907+35.69	46.49' LT	975.03	404422.69	714267.00
14	907+38.98	41.42' LT	974.97	404426.03	714272.04
15	907+41.56	38.38' LT	974.73	404428.64	714275.06
16	907+47.25	33.33' LT	974.42	404434.38	714280.05
17	907+52.09	30.29' LT	974.19	404439.26	714283.03
18	907+57.31	27.98' LT	973.96	404444.50	714285.29
19	907+31.24	65.96' LT	976.45	404418.03	714247.58
20	907+32.28	56.77' LT	976.02	404419.18	714256.76
21	907+33.96	51.38' LT	975.75	404420.90	714262.13

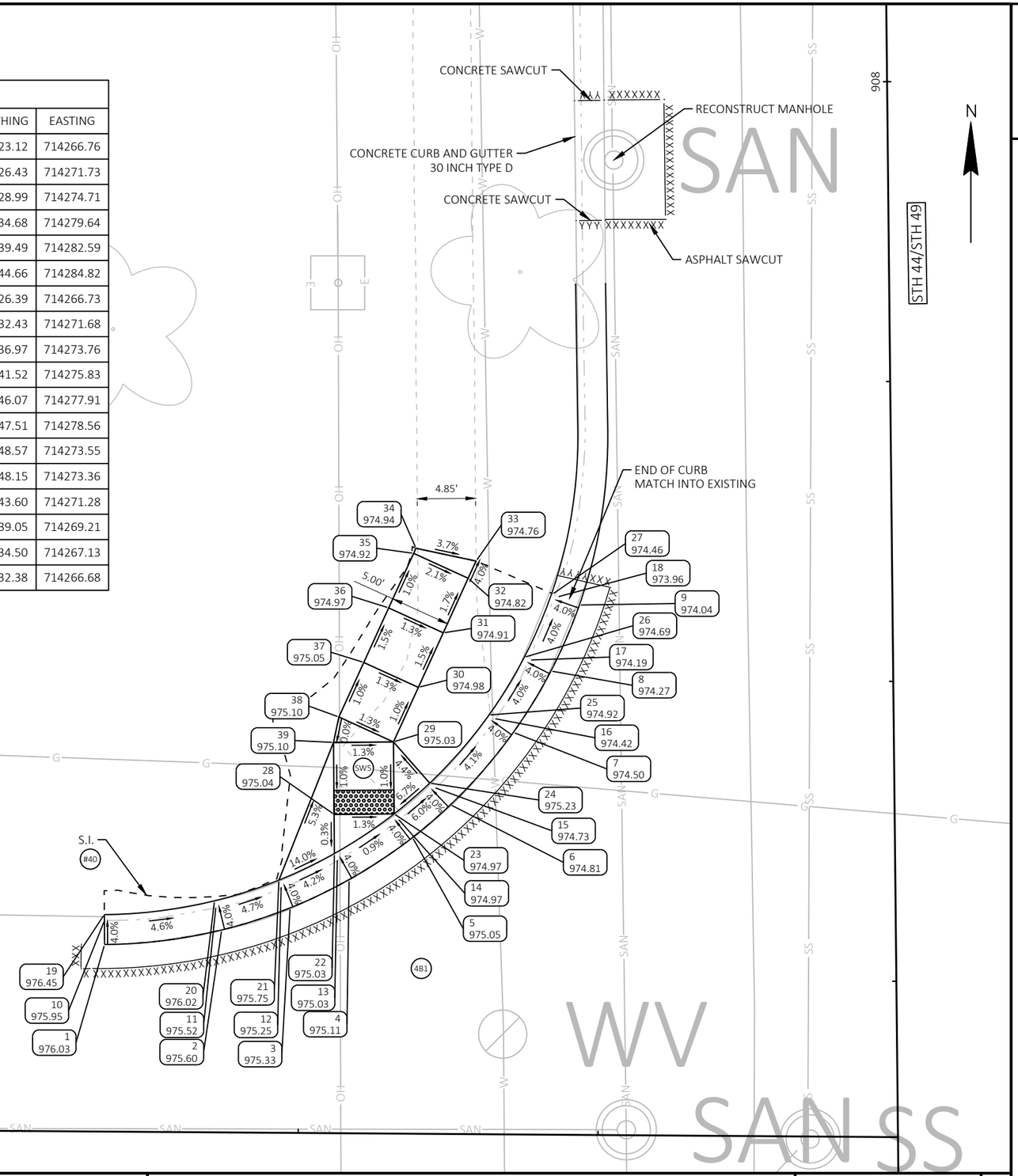
Ringstad_NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
22	907+36.13	46.73' LT	975.03	404423.12	714266.76
23	907+39.38	41.73' LT	974.97	404426.43	714271.73
24	907+41.92	38.72' LT	975.23	404428.99	714274.71
25	907+47.55	33.73' LT	974.92	404434.68	714279.64
26	907+52.32	30.74' LT	974.69	404439.49	714282.59
27	907+57.48	28.45' LT	974.46	404444.66	714284.82
28	907+39.39	46.73' LT	975.04	404426.39	714266.73
29	907+45.38	41.72' LT	975.03	404432.43	714271.68
30	907+49.90	39.59' LT	974.98	404436.97	714273.76
31	907+54.43	37.47' LT	974.91	404441.52	714275.83
32	907+58.96	35.35' LT	974.82	404446.07	714277.91
33	907+60.39	34.68' LT	974.76	404447.51	714278.56
34	907+61.50	39.67' LT	974.94	404448.57	714273.55
35	907+61.08	39.87' LT	974.92	404448.15	714273.36
36	907+56.56	42.00' LT	974.97	404443.60	714271.28
37	907+52.03	44.12' LT	975.05	404439.05	714269.21
38	907+47.50	46.24' LT	975.10	404434.50	714267.13
39	907+45.39	46.72' LT	975.10	404432.38	714266.68

NOTES:

1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION
2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION
3. DASHED LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS
4. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
5. ALL STATION AND OFFET INFORMATION REFERENCE STH 44/STH 49 R/L

LEGEND

- X — CURB RAMP TYPE
- #40 — WISDOT #40 Seed
- SW5 — CONCRETE SIDEWALK 5-INCH
- XXX / XXX.XX — POINT NUMBER & ELEVATION

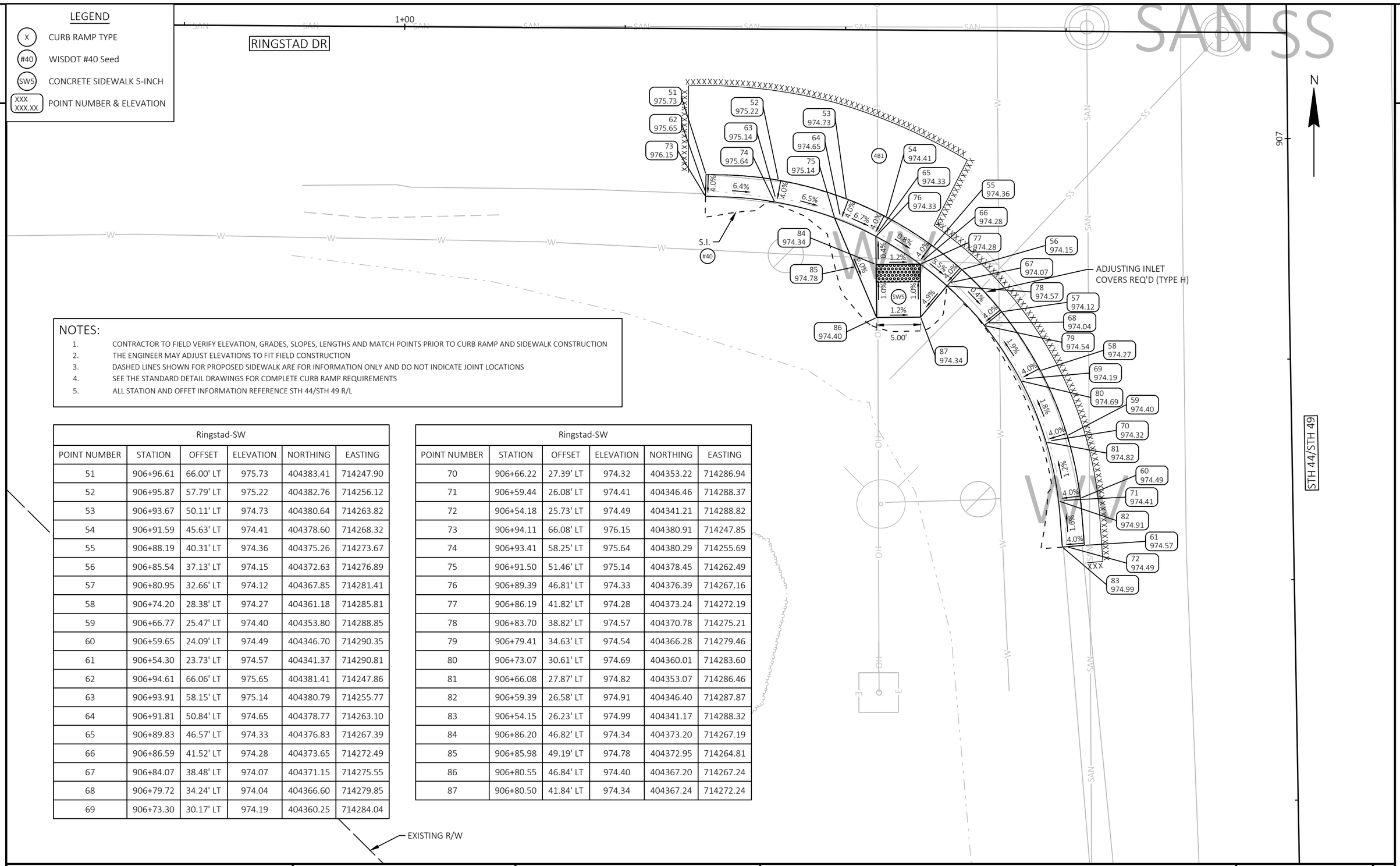


PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	CURB RAMP DETAILS	SHEET 12	E

FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\021301-CR.DWG
LAYOUT NAME - 021301-cr
PLOT DATE : 10/22/2024 9:18 AM
PLOT BY : HOLLAND, DREW M
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42

LEGEND

- (X) CURB RAMP TYPE
- (#40) WISDOT #40 Seed
- (SW5) CONCRETE SIDEWALK 5-INCH
- (XXX / XXX.XX) POINT NUMBER & ELEVATION



NOTES:

1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION
2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION
3. DASHED LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS
4. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
5. ALL STATION AND OFFET INFORMATION REFERENCE STH 44/STH 49 R/L

Ringstad-SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
51	906+96.61	66.00' LT	975.73	404383.41	714247.90
52	906+95.87	57.79' LT	975.22	404382.76	714256.12
53	906+93.67	50.11' LT	974.73	404380.64	714263.82
54	906+91.59	45.63' LT	974.41	404378.60	714268.32
55	906+88.19	40.31' LT	974.36	404375.26	714273.67
56	906+85.54	37.13' LT	974.15	404372.63	714276.89
57	906+80.95	32.66' LT	974.12	404367.85	714281.41
58	906+74.20	28.38' LT	974.27	404361.18	714285.81
59	906+66.77	25.47' LT	974.40	404353.80	714288.85
60	906+59.65	24.09' LT	974.49	404346.70	714290.35
61	906+54.30	23.73' LT	974.57	404341.37	714290.81
62	906+94.61	66.06' LT	975.65	404381.41	714247.86
63	906+93.91	58.15' LT	975.14	404380.79	714255.77
64	906+91.81	50.84' LT	974.65	404378.77	714263.10
65	906+89.83	46.57' LT	974.33	404376.83	714267.39
66	906+86.59	41.52' LT	974.28	404373.65	714272.49
67	906+84.07	38.48' LT	974.07	404371.15	714275.55
68	906+79.72	34.24' LT	974.04	404366.60	714279.85
69	906+73.30	30.17' LT	974.19	404360.25	714284.04

Ringstad-SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
70	906+66.22	27.39' LT	974.32	404353.22	714286.94
71	906+59.44	26.08' LT	974.41	404346.46	714288.37
72	906+54.18	25.73' LT	974.49	404341.21	714288.82
73	906+94.11	66.08' LT	976.15	404380.91	714247.85
74	906+93.41	58.25' LT	975.64	404380.29	714255.69
75	906+91.50	51.46' LT	975.14	404378.45	714262.49
76	906+89.39	46.81' LT	974.33	404376.39	714267.16
77	906+86.19	41.82' LT	974.28	404373.24	714272.19
78	906+83.70	38.82' LT	974.57	404370.78	714275.21
79	906+79.41	34.63' LT	974.54	404366.28	714279.46
80	906+73.07	30.61' LT	974.69	404360.01	714283.60
81	906+66.08	27.87' LT	974.82	404353.07	714286.46
82	906+59.39	26.58' LT	974.91	404346.40	714287.87
83	906+54.15	26.23' LT	974.99	404341.17	714288.32
84	906+86.20	46.82' LT	974.34	404373.20	714267.19
85	906+85.98	49.19' LT	974.78	404372.95	714264.81
86	906+80.55	46.84' LT	974.40	404367.20	714267.24
87	906+80.50	41.84' LT	974.34	404367.24	714272.24

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	CURB RAMP DETAILS	SHEET 13	E

FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\021301-CR.DWG
LAYOUT NAME - 021302-cr
PLOT DATE : 10/22/2024 9:18 AM
PLOT BY : HOLLAND, DREW M
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42

2

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	EROSION CONTROL	SHEET 14	E

FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\022001-EC.DWG
LAYOUT NAME - 022001-ec
PLOT DATE : 10/22/2024 9:15 AM
PLOT BY : HOLLAND, DREW M
PLOT NAME :
PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 42

LEGEND

PCMS — TRAFFIC CONTROL SIGNS PCMS

(1) PLACE TRAFFIC CONTROL SIGNS PER SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER UNDIVIDED ROAD OPEN TO TRAFFIC

(2) PLACE W20-1A SIGNS PER "TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL" IN SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER UNDIVIDED ROAD OPEN TO TRAFFIC

N

OAK GROVE ROAD

BRANDON RD

BRANDON RD

DIXON RD

BRANDON

STH 44

WISCONSIN & SOUTHERN RAILROAD

STH 49

TOLSMA DR

SHELDON RD

BERG RD

R/L STH 49

CARTER RD

REEDS CORNERS RD

CTH E

CTH KK

POLLACK RD

STH 49/STH 44

STH 44/STH 49

RINGSTAD DR

PCMS

500

550

600

650

700

750

800

850

900

BEGIN PROJECT
STA 473+58.01
Y=370,308.621
X=725,943.940

ROAD WORK
NEXT 9 MILES

END PROJECT
STA 912+13.00
Y=404,900.454
X=714,308.564

ROAD WORK
NEXT 9 MILES

NOTES:
1. ALL SIGNS OR PAVEMENT MARKING, TEMPORARY OR EXISTING, WHICH MAY CONFLICT WITH THE CONSTRUCTION TRAFFIC PATTERN SHALL BE REMOVED OR COVERED.
2. PLACE MESSAGE BOARDS AS FAR AWAY FROM LIVE TRAFFIC LANES AS POSSIBLE WITHOUT HAMPERING VISIBILITY. THE LOCATION SELECTED SHOULD BE AT OR SLIGHTLY ABOVE THE ELEVATION OF THE ROADWAY. FOR INTERMITTENT WORK, OR WHERE SITE CONDITIONS DO NOT ALLOW OTHERWISE, THE SIGNS MAY BE PLACED ON THE SHOULDER. THE SITE SHOULD BE VISITED TO ASSURE VISIBILITY, SAFETY, AND MAINTENANCE CONSIDERATIONS. A TAPER OF REFLECTORIZED DRUMS OR BARRICADES SHOULD BE PLACED AHEAD OF PCMS PLACED ON THE SHOULDER IF IT IS NOT SHIELDED BY A BARRIER.
3. PLACE MESSAGE BOARDS AND DISPLAY THE "PRIOR TO CONSTRUCTION" MESSAGE 7 CALENDAR DAYS PRIOR TO THE EXPECTED START OF THE PROPOSED WORK THAT WILL REQUIRE LAND CLOSURES. ADJUST THE MESSAGE DATE ACCORDINGLY. CONTACT WISDOT NE REGION FOR MESSAGES.

ADDITIONAL STANDARD DETAIL DRAWINGS:

1. SEE SDD "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION" DURING DAYTIME WORKING HOURS
2. SEE SDD "MOVING PAVEMENT MARKING OPERATION, TWO-LANE TWO-WAY ROADWAY" DURING MAKING LINE PLACEMENT
3. SEE SDD "TRAFFIC CONTROL, DROP-OFF SIGNING" WHEN UNEVEN LANES ARE PRESENT
4. SEE SDD "TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES" WHEN TRAFFIC WILL RUN ON MILLED SURFACE

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	TRAFFIC CONTROL	SHEET 15	E

FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\025100-TC.DWG
LAYOUT NAME - 025100-tc
PLOT DATE : 3/13/2025 4:00 PM
PLOT BY : CAMPSHURE, MICHAEL R
PLOT NAME :
PLOT SCALE : 1 IN:0.5 MI
WISDOT/CADDS SHEET 42

Estimate Of Quantities

6090-14-71

Line	Item	Item Description	Unit	Total	Qty
0002	202.0110	Roadside Clearing	SY	115.000	115.000
0004	204.0110	Removing Asphaltic Surface	SY	65.000	65.000
0006	204.0115	Removing Asphaltic Surface Butt Joints	SY	96.000	96.000
0008	204.0120	Removing Asphaltic Surface Milling	SY	156,284.000	156,284.000
0010	204.0150	Removing Curb & Gutter	LF	136.000	136.000
0012	204.0155	Removing Concrete Sidewalk	SY	57.000	57.000
0014	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 6090-14-71	EACH	1.000	1.000
0016	211.0400	Prepare Foundation for Asphaltic Shoulders	STA	738.000	738.000
0018	213.0100	Finishing Roadway (project) 01. 6090-14-71	EACH	1.000	1.000
0020	305.0110	Base Aggregate Dense 3/4-Inch	TON	782.000	782.000
0022	450.4000	HMA Cold Weather Paving	TON	5,260.000	5,260.000
0024	455.0605	Tack Coat	GAL	10,924.000	10,924.000
0026	460.0105.S	HMA Percent Within Limits (PWL) Test Strip Volumetrics	EACH	1.000	1.000
0028	460.0110.S	HMA Percent Within Limits (PWL) Test Strip Density	EACH	1.000	1.000
0030	460.2005	Incentive Density PWL HMA Pavement	DOL	14,270.000	14,270.000
0032	460.2007	Incentive Density HMA Pavement Longitudinal Joints	DOL	8,780.000	8,780.000
0034	460.2010	Incentive Air Voids HMA Pavement	DOL	21,040.000	21,040.000
0036	460.5224	HMA Pavement 4 LT 58-28 S	TON	21,053.000	21,053.000
0038	465.0105	Asphaltic Surface	TON	2,199.000	2,199.000
0040	465.0520	Asphaltic Rumble Strips, Shoulder	LF	73,808.000	73,808.000
0042	465.0560	Asphaltic Rumble Strips, Centerline	LF	36,904.000	36,904.000
0044	465.0580	Asphaltic Rumble Strips, Transverse	SY	158.000	158.000
0046	520.8700	Cleaning Culvert Pipes	EACH	1.000	1.000
0048	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	131.000	131.000
0050	602.0410	Concrete Sidewalk 5-Inch	SF	225.000	225.000
0052	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	20.000	20.000
0054	611.0420	Reconstructing Manholes	EACH	2.000	2.000
0056	611.0535	Manhole Covers Type J-Special	EACH	1.000	1.000
0058	611.8115	Adjusting Inlet Covers	EACH	1.000	1.000
0060	618.0100	Maintenance and Repair of Haul Roads (project) 01. 6090-14-71	EACH	1.000	1.000
0062	619.1000	Mobilization	EACH	1.000	1.000
0064	624.0100	Water	MGAL	16.000	16.000
0066	625.0100	Topsoil	SY	83.000	83.000
0068	628.1504	Silt Fence	LF	30.000	30.000
0070	628.1520	Silt Fence Maintenance	LF	15.000	15.000
0072	628.1905	Mobilizations Erosion Control	EACH	1.000	1.000
0074	628.1910	Mobilizations Emergency Erosion Control	EACH	1.000	1.000
0076	628.2008	Erosion Mat Urban Class I Type B	SY	83.000	83.000
0078	628.7015	Inlet Protection Type C	EACH	3.000	3.000
0080	628.7570	Rock Bags	EACH	25.000	25.000
0082	629.0210	Fertilizer Type B	CWT	0.040	0.040
0084	630.0140	Seeding Mixture No. 40	LB	2.000	2.000
0086	630.0500	Seed Water	MGAL	0.500	0.500
0088	633.5200	Markers Culvert End	EACH	3.000	3.000
0090	642.5001	Field Office Type B	EACH	1.000	1.000
0092	643.0300	Traffic Control Drums	DAY	542.000	542.000
0094	643.0420	Traffic Control Barricades Type III	DAY	20.000	20.000
0096	643.0705	Traffic Control Warning Lights Type A	DAY	40.000	40.000
0098	643.0900	Traffic Control Signs	DAY	1,834.000	1,834.000

6090-14-71

Line	Item	Item Description	Unit	Total	Qty
0100	643.1050	Traffic Control Signs PCMS	DAY	28.000	28.000
0102	643.1055.S	Truck or Trailer Mounted Attenuator	DAY	8.000	8.000
0104	643.3165	Temporary Marking Line Paint 6-Inch	LF	41,500.000	41,500.000
0106	643.5000	Traffic Control	EACH	1.000	1.000
0108	644.1810	Temporary Pedestrian Barricade	LF	6.000	6.000
0110	646.2040	Marking Line Grooved Wet Ref Epoxy 6-Inch	LF	129,059.000	129,059.000
0112	646.4040	Marking Line Grooved Wet Ref Epoxy 10-Inch	LF	695.000	695.000
0114	646.5020	Marking Arrow Epoxy	EACH	8.000	8.000
0116	646.5120	Marking Word Epoxy	EACH	1.000	1.000
0118	646.5320	Marking Railroad Crossing Epoxy	EACH	2.000	2.000
0120	646.6120	Marking Stop Line Epoxy 18-Inch	LF	270.000	270.000
0122	646.6466	Cold Weather Marking Epoxy 6-Inch	LF	32,265.000	32,265.000
0124	646.6470	Cold Weather Marking Epoxy 10-Inch	LF	59.000	59.000
0126	646.7120	Marking Diagonal Epoxy 12-Inch	LF	95.000	95.000
0128	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	126.000	126.000
0130	650.8000	Construction Staking Resurfacing Reference	LF	43,867.000	43,867.000
0132	650.9000	Construction Staking Curb Ramps	EACH	2.000	2.000
0134	650.9911	Construction Staking Supplemental Control (project) 01. 6090-14-71	EACH	1.000	1.000
0136	690.0150	Sawing Asphalt	LF	219.000	219.000
0138	690.0250	Sawing Concrete	LF	8.000	8.000
0140	740.0440	Incentive IRI Ride	DOL	66,480.000	66,480.000
0142	ASP.1T0A	On-the-Job Training Apprentice at $5.00/HR	HRS	2,000.000	2,000.000
0144	ASP.1T0G	On-the-Job Training Graduate at $5.00/HR	HRS	1,200.000	1,200.000
0146	SPV.0060	Special 01. Adjust Water Valve	EACH	1.000	1.000

REMOVAL SUMMARY

STATION	TO	STATION	LOCATION	202.0110 ROADSIDE CLEARING SY	204.0115 REMOVING ASPHALTIC SURFACE BUTT JOINTS SY	204.0120 * REMOVING ASPHALTIC SURFACE MILLING SY	204.0150 * REMOVING CURB & GUTTER LF	204.0155 REMOVING CONCRETE SIDEWALK SY	REMARKS
473+58	-	473+58			8				PROJECT START BUTT JOINT
473+58	-	507+05				11,192			PROJECT START TO BRANDON RD
507+05	-	512+80				3,252			BRANDON RD INT.
509+75	-	510+07	RT		7				BRANDON RD INT.
509+78	-	510+11	LT		7				BRANDON RD INT.
512+80	-	554+33				13,830			BRANDON RD TO 44 INT.
554+26	-	569+90				6,868			44 INT.
557+95	-	558+33	LT		9				44 INT.
565+60	-	565+92	LT		8				44 INT.
567+09	-	567+18	LT		6				44 INT.
569+89	-	600+74				10,282			44 INT. TO TOLSMA DR.
569+30		570+00	RT	108					AT RR CROSSING
570+94	-	570+96			7				SOUTH OF RAILROAD TRACK
571+06	-	571+04			7				NORTH OF RAILROAD TRACKS
600+74	-	601+51				378			TOLSMA DR. INT.
601+11	-	601+28	RT		4				TOLSMA DR. INT.
601+51	-	635+36				11,286			TOLSMA DR. TO SHELDON RD
613+10	-	-	LT	7					AT CULVERT ENDWALL
635+36	-	640+44				2,845			SHELDON RD INT.
637+70	-	638+02	RT		7				SHELDON RD INT.
637+74	-	638+06	LT		7				SHELDON RD INT.
640+43	-	663+05				7,562			SHELDON RD TO BERG RD
663+05	-	666+50				2,287			BERG RD INT.
664+20	-	664+52	RT		7				BERG RD INT.
664+27	-	664+58	LT		7				BERG RD INT.
666+50	-	715+29				16,265			BERG RD TO CARTER RD
715+29	-	719+50				2,559			CARTER RD INT.
717+21	-	717+53	RT		7				CARTER RD INT.
717+25	-	717+57	LT		7				CARTER RD INT.
719+50	-	767+41				15,968			CARTER RD TO REEDS CORNERS RD
767+40	-	773+17				3,259			REEDS CORNERS RD INT.
770+10	-	770+42	RT		7				REEDS CORNERS RD INT.
770+16	-	770+47	LT		7				REEDS CORNERS RD INT.
773+17	-	792+65				6,476			REEDS CORNERS RD TO POLLACK RD
792+56	-	796+00				1,623			POLLACK RD INT.
794+25	-	794+59	RT		8				POLLACK RD INT.
795+87	-	829+83				11,277			POLLACK RD TO KK
829+70	-	832+78				954			KK INT. MAINLINE ONLY
832+67	-	905+03				24,156			KK INT TO RINGSTAD DR
905+02	-	912+17				3,835			RINGSTAD DR INT. TO PROJECT END
906+55	-	906+81	LT				54		RINGSTAD CURB SOUTH
906+94	-	907+31	LT		8				RINGSTAD INT.
907+29	-	907+63	LT				67		RINGSTAD CURB NORTH
907+85	-	907+88	LT				5		SAN. MH. 907+87. 25' lt
907+40	-	908+38	LT					57	RINGSTAD CURB RAMP
912+17	-	912+17			8				PROJECT END
			TOTAL 0010	**115**	**96**	**156,156**	**126**	**57**	

(*) = ADDITIONAL QUANTITIES SHOWN ELSWHERE

PROJECT TOTAL 0010 = PROJECT CATEGAORY

3

BASE AGGREGATE SUMMARY

STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	624.0100 WATER MGAL	REMARKS
473+58	-	509+05	STH 49	33	1	RT SHOULDER W/O INTERSECTION
473+58	-	509+28	STH 49	33	1	LT SHOULDER W/O INTERSECTION
510+46	-	558+03	STH 49	44	1	RT SHOULDER W/O INTERSECTION
510+77	-	557+43	STH 49	43	1	LT SHOULDER W/O INTERSECTION
558+03	-	565+50	STH 44/49	7	0	RT SHOULDER W/O INTERSECTION
559+00	-	565+50	STH 44/49	6	0	LT SHOULDER W/O INTERSECTION
565+50	-	600+70	STH 44/49	33	1	RT SHOULDER W/O INTERSECTION
567+00	-	601+18	STH 44/49	32	1	LT SHOULDER W/O INTERSECTION
601+54	-	637+00	STH 44/49	33	1	RT SHOULDER W/O INTERSECTION
601+18	-	637+27	STH 44/49	33	1	LT SHOULDER W/O INTERSECTION
638+50	-	663+50	STH 44/49	23	0	RT SHOULDER W/O INTERSECTION
638+75	-	663+77	STH 44/49	23	0	LT SHOULDER W/O INTERSECTION
665+05	-	716+52	STH 44/49	48	1	RT SHOULDER W/O INTERSECTION
665+10	-	716+74	STH 44/49	48	1	LT SHOULDER W/O INTERSECTION
718+00	-	769+50	STH 44/49	48	1	RT SHOULDER W/O INTERSECTION
715+25	-	769+65	STH 44/49	50	1	LT SHOULDER W/O INTERSECTION
770+89	-	793+57	STH 44/49	21	0	RT SHOULDER W/O INTERSECTION
771+17	-	794+38	STH 44/49	21	0	LT SHOULDER W/O INTERSECTION
795+00	-	830+48	STH 44/49	33	1	RT SHOULDER W/O INTERSECTION
794+38	-	830+81	STH 44/49	34	1	LT SHOULDER W/O INTERSECTION
832+12	-	906+43	STH 44/49	69	1	RT SHOULDER W/O INTERSECTION
832+42	-	905+72	STH 44/49	68	1	LT SHOULDER W/O INTERSECTION
			TOTAL 0010	**782**	**16**	

PROJECT TOTAL 0010 = PROJECT CATEGAORY

HMA SUMMARY

				450.4000	455.0605	460.0105.S	460.0110.S	460.5224	465.0105	
STATION	TO	STATION	LOCATION	HMA COLD WEATHER PAVING TON	TACK COAT GAL	HMA PERCENT WITHIN LIMITS (PWL) TEST STRIP VOLUMETRICS EACH	HMA PERCENT WITHIN LIMITS (PWL) TEST STRIP DENSITY EACH	* HMA PAVEMENT 4 LT 58-28 S TON	* ASPHALTIC SURFACE TON	REMARKS
473+58	-	912+17	Project Limits	3567	8187	1	1	14269		PROJECT LIMIT
473+58	-	508+57	LT	58	82			231	95	PAVED SHOULDER
473+58	-	507+05	RT	54	76			216	91	PAVED SHOULDER
473+58	-	474+61	RT	1	3			6		PROJECT START
506+68	-	511+47	RT	25	57			99		BRANDON ROAD INT.
508+37	-	513+18	LT	25	57			100		BRANDON ROAD INT.
511+27	-	554+82	RT	71	101			284	118	PAVED SHOULDER
512+80	-	556+69	LT	72	102			287	119	PAVED SHOULDER
554+32	-	562+32	RT	25	58			101		44 49 PASSING LANE
556+49	-	561+00	LT	26	60			104		44 49 INT.
560+61	-	564+82	LT	6	9			25	11	PAVED SHOULDER
561+57	-	600+84	RT	64	91			256	106	PAVED SHOULDER
564+61	-	569+06	LT	16	37			64		44 49 INT.
568+79	-	636+53	LT	111	157			443	184	PAVED SHOULDER
600+74	-	601+63	RT	5	10			18		TOLSMA DR INT.
601+51	-	635+78	RT	54	75			215	93	PAVED SHOULDER
635+36	-	639+42	RT	21	49			85		SHELDON RD INT.
636+33	-	640+43	LT	21	49			86		SHELDON RD INT.
639+23	-	662+28	RT	37	53			150	62	PAVED SHOULDER
639+98	-	663+05	LT	37	53			150	63	PAVED SHOULDER
661+90	-	665+90	RT	20	46			80		BERG RD INT.
662+85	-	666+95	LT	22	51			89		BERG RD INT.
665+74	-	715+29	RT	81	115			324	134	PAVED SHOULDER
666+50	-	716+04	LT	84	123			338	134	PAVED SHOULDER
713+96	-	720+45	RT	23	54			94		CARTER RD INT.
715+90	-	719+91	LT	20	46			79		CARTER RRD INT.
718+73	-	767+40	RT	79	113			318	132	PAVED SHOULDER
719+50	-	768+94	LT	81	115			323	134	PAVED SHOULDER
767+03	-	771+83	RT	25	57			100		REEDS CORNERS INT.
768+74	-	773+54	LT	25	58			100		REEDS CORNERS INT.
771+63	-	792+56	RT	34	49			138	57	PAVED SHOULDER
773+17	-	830+90	LT	94	134			377	157	PAVED SHOULDER
792+23	-	795+87	RT	18	41			72		POLLACK RD
795+73	-	830+51	RT	56	80			226	94	PAVED SHOULDER
832+39	-	903+84	RT	117	166			467	194	PAVED SHOULDER
832+41	-	905+03	LT	119	169			475	197	PAVED SHOULDER
903+34	-	905+02	RT	3	7			12		ROAD WIDENING
903+84	-	905+02	RT	1	1			5	3	PAVED SHOULDER
905+02	-	911+78	RT	30	70			121		RINGSTAD PASSING LANE
905+03	-	911+78	LT	28	63			111		RINGSTAND INT.
905+03	-	906+54	LT	1	2			4		PAVED SHOULDER
			TOTAL 0010	**5,260**	**10,924**	**1**	**1**	**21,038**	**2,178**	

PROJECT TOTAL 0010 = PROJECT CATEGAORY

= ADDITIONAL QUANTITIES SHOWN ELSEWH

3

PWL MIX TABLE

Location	Station	to	Station	Mixture Use:	Underlying Surface	Bid Item	Tons	Thickness	Quality Management Program to be used for:	
									Mixture Acceptance	Density Acceptance
Mainline NB, 12' Driving Lanes	473+58	-	912+17	Upper Layer	Milled Existing HMA Surface	4 LT 58-28S	7134	2"	PWL Incentive Air Voids HMA Pavement 460.2010	PWL Incentive Density HMA Pavement 460.2005
Mainline SB, 12' Driving Lanes	473+58	-	912+17	Upper Layer	Milled Existing HMA Surface	4 LT 58-28S	7134	2"	PWL Incentive Air Voids HMA Pavement 460.2010	PWL Incentive Density HMA Pavement 460.2005
5' Paved Shoulder	473+58	-	906+54	Upper Layer	Milled Existing HMA Surface	4 LT 58-28S	5245	2"	PWL Incentive Air Voids HMA Pavement 460.2010	Accpetance testing by department: Not eligble for incentive
2' Paved Shoulder Widening	473+58	-	905+02	Lower Layer	Recycled Asphalitc Base Course	Asphaltic Surface	2178	2"	QMP as per SS 465	Acceptance by ordinary compaction

3

PWL MIX TABLE - CONTINUED

Location	Station	to	Station	Mixture Use:	Underlying Surface	Bid Item	Tons	Thickness	Quality Management Program to be used for:	
									Mixture Acceptance	Density Acceptance
Intersections Various	506+68	-	795+87	Upper Layer	Milled Existing HMA Surface	4 LT 58-28S	1281	2"	PWL Incentive Air Voids HMA Pavement 460.2010	Acceptance testing by department: Not eligble for incentive
BEGIN PROJECT LIMIT	473+58	-	474+61	Upper Layer	Milled Existing HMA Surface	4 LT 58-28S	6	2"	PWL Incentive Air Voids HMA Pavement 460.2010	Acceptance testing by department: Not eligble for incentive
44 49 Passing Lane	554+32	-	562+32				101			
Width Transiton Section	903+34	-	905+02				12			
Ringstad Passing Lane	905+02	-	911+78				121			

PROJECT TOTAL 0010 = PROJECT CATEGAORY

3

MUNICIPAL WORK SUMMARY

STATION	TO	STATION	LOCATION	204.0110 * REMOVING ASPHALTIC SURFACE SY	204.0120 * REMOVING ASPHALTIC SURFACE MILLING SY	204.0150 * REMOVING CURB & GUTTER LF	460.5224 * HMA PAVEMENT 4 LT 58-28 S TON	465.0105 * ASPHALTIC SURFACE TON	601.0411 * CONCRETE CURB & GUTTER 30-INCH TYPE D LF	611.0420 RECONSTRUCTING MANHOLES EACH	690.0150 * SAWING ASPHALT LF	690.0250 * SAWING CONCRETE LF	SPV 0060.01 ADJUSTING WATER VALVE EACH	REMARKS
907+88	-	907+95	STH 44/49	6		10		1	10	1	50	5		SAN. MH - AT CURB LINE
911+78	-	912+13	STH 44/49		128		15			1			1	SAN. MH. AT 912+00 LT
			PROJECT TOTAL 002(	**6**	**128**	**10**	**15**	**1**	**10**	**2**	**50**	**5**	**1**	

(*) = ADDITIONAL QUANTITIES SHOWN ELSWHERE

CONCRETE & DRAINAGE SUMMARY

STATION	TO	STATION	LOCATION	465.0105 * ASPHALTIC SURFACE TON	204.0110 * REMOVING ASPHALTIC SURFACE SY	601.0411 * CONCRETE CURB & GUTTER 30-INCH TYPE D LF	602.0410 CONCRETE SIDEWALK 5-INCH SF	602.0505 CURB RAMP DETECTABLE WARNING FIELD YELLOW SF	611.0535 MANHOLE COVERS TYPE J-SPECIAL EACH	611.8115 ADJUSTING INLET COVER EACH	690.0150 * SAWING ASPHALT LF	690.0250 * SAWING CONCRETE LF	REMARKS
907+29	-	907+63	LT	5	14	54	156	10			63	3	NW QUADRANT AT RINGSTAD DR
906+55	-	906+81	LT	15	45	67	69	10		1	98		SW QUADRANT AT RINGSTAD DR
911+60	-	911+63	LT						1		8		SS.MH 911+61, 6'LT
			TOTAL 0010	**20**	**59**	**121**	**225**	**20**	**1**	**1**	**169**	**3**	

(*) = ADDITIONAL QUANTITIES SHOWN ELSEWHERE

PROJECT TOTAL 0020 = PROJECT CATEGAORY

LANDSCAPING SUMMARY

STATION	TO	STATION	LOCATION	625.0100 TOPSOIL SY	628.2008 EROSION MAT URBAN CLASS I TYPE B SY	629.0210 FERTILIZER TYPE B CWT	630.0140 SEEDING MIXTURE NO. 40 LB	630.0500 SEED WATER MGAL	REMARKS
1+48	-	1+78	RINGSTAD DR LT	39	39	0.02	0.7	0.2	NEW SIDEWAK/CURB RAMP
1+48	-	1+78	RINGSTAD DR RT	44	44	0.02	0.8	0.3	NEW SIDEWAK/CURB RAMP
			TOTAL 0010	**83**	**83**	**0.04**	**2.0**	**0.5**	

RUMBLE STRIPS

STATION	TO	STATION	LOCATION	465.0520 ASPHALTIC RUMBLE STRIPS, SHOULDER LF	465.0580 ASPHALTIC RUMBLE STRIPS TRANSVERSE SY	465.0560 ASPHALTIC RUMBLE STRIPS, CENTERLINE LF
473+58	-	508+00	STH 44/49	6,884		3,442
512+00	-	550+00	STH 44/49	7,600		3,800
552+00	-	556+00	STH 44/49	800		400
560+00	-	564+00	STH 44/49	800		400
572+00	-	577+00	STH 44/49	1,000		500
579+00	-	599+00	STH 44/49	4,000		2,000
603+00	-	617+00	STH 44/49	2,800		1,400
619+00	-	636+00	STH 44/49	3,400		1,700
640+00	-	662+50	STH 44/49	4,500		2,250
666+50	-	714+00	STH 44/49	9,500		4,750
719+50	-	768+20	STH 44/49	9,740		4,870
772+20	-	780+00	STH 44/49	1,560		780
782+00	-	792+40	STH 44/49	2,080		1,040
796+40	-	804+00	STH 44/49	1,520		760
806+00	-	829+40	STH 44/49	4,680		2,340
821+03			STH 44/49		26	
824+79			STH 44/49		26	
827+04			STH 44/49		26	
835+79			STH 44/49		26	
838+04			STH 44/49		26	
841+80			STH 44/49		26	
833+40	-	859+00	STH 44/49	5,120		2,560
861+00	-	900+12	STH 44/49	7,824		3,912
			TOTAL 0010	**73,808**	**158**	**36,904**

3

CULVERT WORK SUMMARY

STATION	LOCATION	520.8700 CLEANING CULVERT PIPES EACH	633.5200 MARKERS CULVERT END EACH	628.7570 ROCK BAGS EACH	633.5200 MARKERS CULVERT END EACH
603+44	STH 44/49 RT		1		1
613+10	STH 44/49 LT	1		25	
663+69	STH 44/49 LT		1		1
695+54	STH 44/49 LT		1		1
	TOTAL 0010	**1**	**3**	**25**	**3**

EROSION CONTROL SUMMARY

STATION	TO	STATION	LOCATION	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH	628.7015 INLET PROTECTION TYPE C EACH
473+58	-	912+17	PROJECT WIDE			1	1	
1+48	-	1+78	RINGSTADT DR LT					
1+48	-	1+78	RINGSTADT DR RT	30	15			
	-	909+95	STH 44/49 RT & LT					3
			TOTAL 0010	30	15	1	1	3

PAVEMENT MARKINGS

STATION	TO	STATION	LOCATION	646.2040 MARKING LINE GROOVED WET REF EPOXY 6-INCH YELLOW LF	646.2040 MARKING LINE GROOVED WET REF EPOXY 6-INCH WHITE LF	646.4040 MARKING LINE GROOVED WET REF EPOXY 10-INCH LF	646.5020 MARKING ARROW EPOXY EACH	646.5120 MARKING WORD EPOXY EACH	646.5320 MARKING RAILROAD CROSSINGS EPOXY EACH	646.6120 MARKING STOP LINE EPOXY 18-INCH LF	646.6466 COLD WEATHER MARKING EPOXY 6-INCH LF	646.6468 COLD WEATHER MARKING EPOXY 10-INCH LF	643.3165 TEMPORARY MARKING LINE PAINT 6-INCH LF	646.7120 MARKING DIAGONAL EPOXY 12-INCH LF	REMARKS
473+58	-	912+17	STH 44/49		81,059	695					32,265	59			EDGELINES & TURNLANES
473+58	-	912+17	STH 44/49	48,000									41,500		CENTERLINE
473+58	-	912+17	STH 44/49				8	1							SIDE ROADS AND TURN LANES
473+58	-	912+17	STH 44/49							270				95	INTERSECTION AT STH 44/49
473+58	-	912+17	STH 44/49						2						STH 44/49 AT THE RR CROSSING
			TOTAL 0010	48,000	81,059	**695**	**8**	**1**	**2**	**270**	**32,265**	**59**	**41,500**	**95**	
				129,059											

PROJECT TOTAL 0010 = PROJECT CATEGAORY

3

TRAFFIC CONTROL

ROADWAY	LOCATION	DURATION DAYS	643.0300 TRAFFIC CONTROL DRUMS EACH	643.0300 TRAFFIC CONTROL DRUMS DAY	643.0420 TRAFFIC CONTROL BARRICADES TYPE III EACH	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAY	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A EACH	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	643.0900 TRAFFIC CONTROL SIGNS EACH	643.0900 TRAFFIC CONTROL SIGNS DAY	643.1050 TRAFFIC CONTROL SIGNS PCMS EACH	643.1050 TRAFFIC CONTROL SIGNS PCMS DAY	643.1055.S TRAILER MOUNTED ATTENUATOR EACH	643.1055.S TRAILER MOUNTED ATTENUATOR DAY	644.1810 TEMPORARY PEDESTRIAN BARRICADE LF	REMARKS
STH 49/44	NB & SB LIMITS	14	4	56							2	14				PCMS ADVANCED NOTICE
STH 49/44	MAJOR SIDE ROADS	14	4	56							2	14				PCMS ADVANCED NOTICE
STH 49/44	MAINLINE /PROJECT LIMITS	40							22	880						ADVANCED WARNING/FLAGGING
STH 49/44	SIDE ROADS TO PROJECT	40							22	880						ADVANCED WARNING/FLAGGING
STH 49/44	WITHIN PROJECT LIMITS	8							2	16			1	8		MOVING OPERATIONS- MARKINGS/RUMBLES
STH 49/44	STA 569+00 - STA 573+00	14	20	280					2	28						RR CROSSING UPGRADE
STH 49/44	STA 907+20 - STA 908+00	10	15	150	2	20	4	40	3	30					6	CURB RAMP REPLACEMENT
	TOTAL 0010		**542**		**20**		**40**		**1,834**		**28**		**8**		**6**	

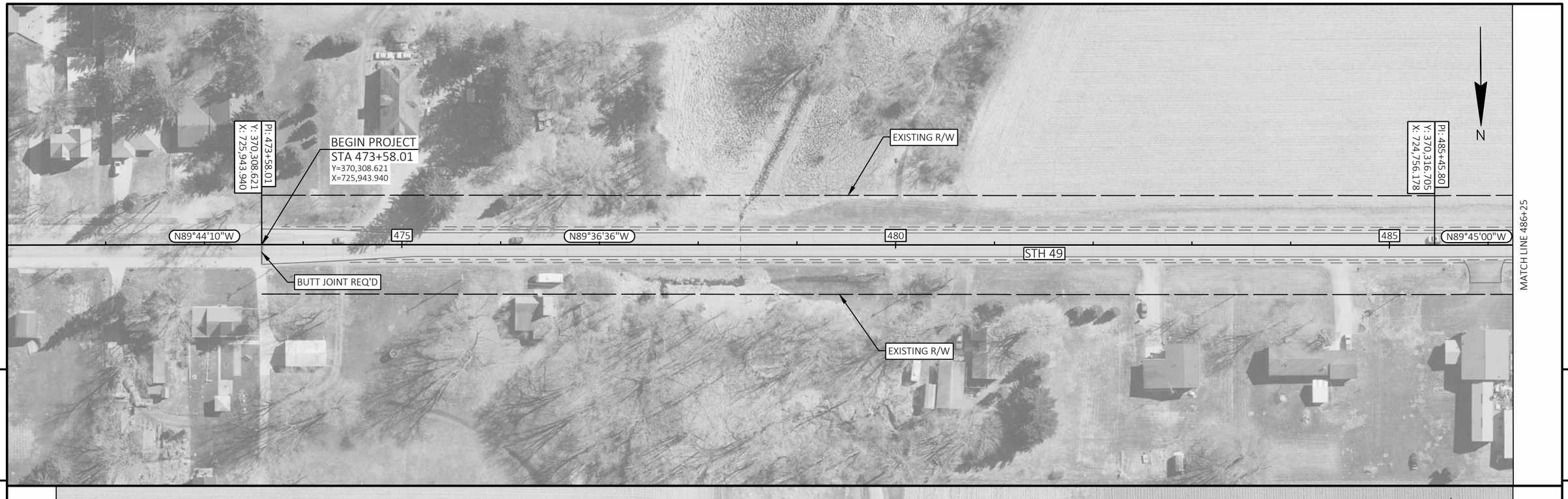
STAKING ITEMS

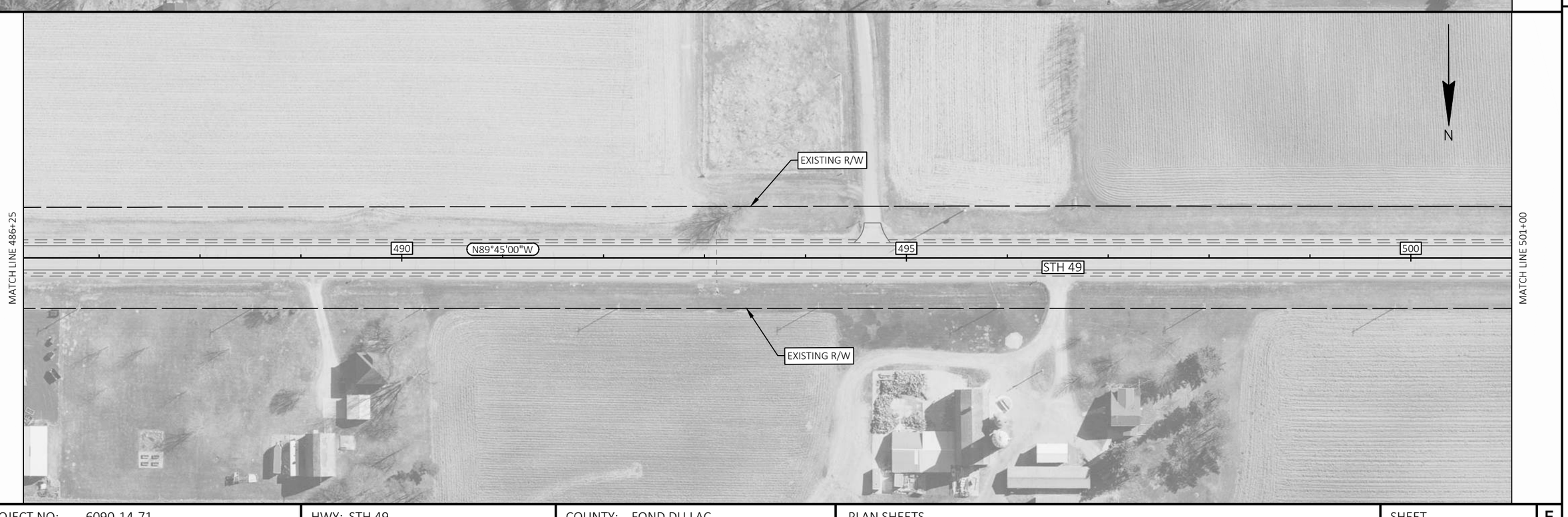
STATION	TO	STATION	LOCATION	650.5500 CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER LF	650.8000 CONSTRUCTION STAKING RESURFACING REFERENCE LF	650.9000 CONSTRUCTION STAKING CURB RAMPS EACH	650.9910.01 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (6090-14-71) LS	REMARKS
473+58	-	912+17	STH 44/49		43,867		1	
907+20	-	908+00	STH 44/49	126		2		AT RINGSTAD DR.
			TOTAL 0010	**126**	**43,867**	**2**	**1**	

MISCELLANEOUS ITEMS

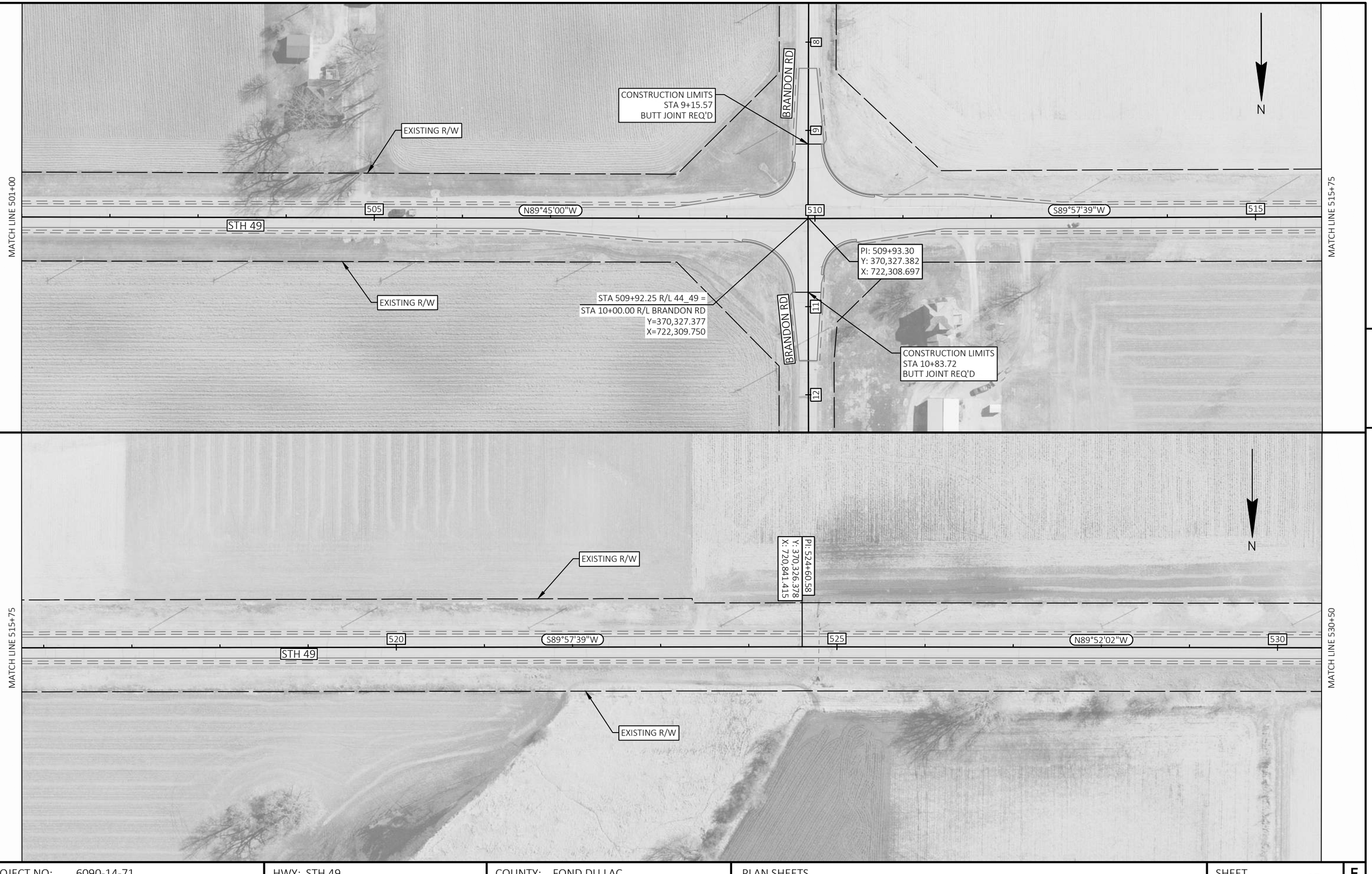
STATION	TO	STATION	LOCATION	211.0400 PREPARE FOUNDATION FOR ASPHALTIC SHOULDERS STA	213.0100.01 FINISHING ROADWAY (PROJECT) (01. TBD) EACH	618.0100.01 MAINTENANCE AND REPAIR OF HAUL ROADS (PROJECT) (01. TBD) EACH	619.1000 MOBILIZATION EACH	642.5001 FIELD OFFICE TYPE B EACH
473+58	-	912+17	STH 44/49	738	1	1	1	1
			TOTAL 0010	**738**	**1**	**1**	**1**	**1**

PROJECT TOTAL 0010 = PROJECT CATEGAORY





PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PLAN SHEETS	SHEET 26 E



5

5

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PLAN SHEETS	SHEET 27	E

MATCH LINE 530+50

MATCH LINE 545+25

5

5

EXISTING R/W

535

N89°52'02"W

540

545

STH 49

EXISTING R/W

N

PC: 551+71.79
Y: 370,332.666
X: 718,130.215

EXISTING R/W

CONSTRUCTION LIMITS
STA 9+11.44
BUTT JOINT REQ'D

7

8

9

STH 44

555

550

N89°52'02"W

STH 49

EXISTING R/W

CURVE 1

N

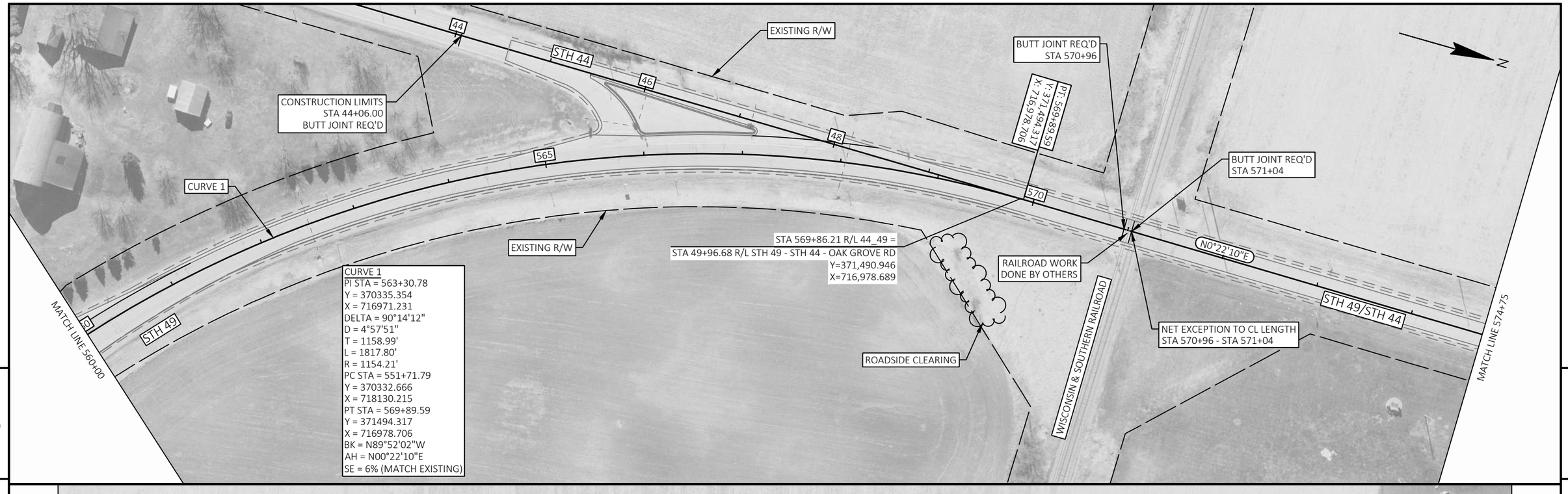
MATCH LINE 545+25

CURVE 1
PI STA = 563+30.78
Y = 370335.354
X = 716971.231
DELTA = 90°14'12"
D = 4°57'51"
T = 1158.99'
L = 1817.80'
R = 1154.21'
PC STA = 551+71.79
Y = 370332.666
X = 718130.215
PT STA = 569+89.59
Y = 371494.317
X = 716978.706
BK = N89°52'02"W
AH = N00°22'10"E
SE = 6.0% (MATCH EXISTING)

STA 558+02.73 R/L 44_49 =
STA 10+00.00 R/L STH 44
Y=370,502.254
X=717,530.618

MATCH LINE 560+00

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PLAN SHEETS	SHEET 28	E



PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PLAN SHEETS	SHEET 29	E

5

5

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PLAN SHEETS	SHEET 30	E

FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\050201-PN.DWG
LAYOUT NAME - 050205-pn

PLOT DATE : 3/13/2025 4:21 PM PLOT BY : CAMPSHURE, MICHAEL R PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

5

5

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PLAN SHEETS	SHEET 31	E

MATCH LINE 648+50

MATCH LINE 663+25

5

N

650

STH 49/STH 44

N0°22'45"W

655

EXISTING R/W

PC: 659+67.30
Y: 380,471.841
X: 717,019.735

660

CURVE 2

EXISTING R/W

CURVE 2
PI STA = 665+26.79
Y = 381031.323
X = 717016.032
DELTA = 0°54'53"
D = 0°04'54"
T = 559.49'
L = 1118.97'
R = 70095.91'
PC STA = 659+67.30
Y = 380471.841
X = 717019.735
PT STA = 670+86.26
Y = 381590.793
X = 717021.260
BK = N00°22'45"W
AH = N00°32'07"E
SE = N.C.

MATCH LINE 663+25

MATCH LINE 678+00

5

N

8

BERG RD

9

CULVERT MARKER (1 EACH)

CONSTRUCTION LIMITS
STA 9+14.24
BUTT JOINT REQ'D

665

STH 49/STH 44

670

PT: 670+86.26
Y: 381,590.793
X: 717,021.260

EXISTING R/W

N0°32'07"E

675

STA 664+39.46 R/L 44_49 =
STA 10+00.00 R/L BERG RD
Y=380,943.998
X=717,018.200

EXISTING R/W

11

BERG RD

CONSTRUCTION LIMITS
STA 10+83.70
BUTT JOINT REQ'D

12

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PLAN SHEETS	SHEET 32	E

FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\050201-PN.DWG
LAYOUT NAME - 050207-pn

PLOT DATE : 3/13/2025 4:21 PM PLOT BY : CAMPSHURE, MICHAEL R PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

5

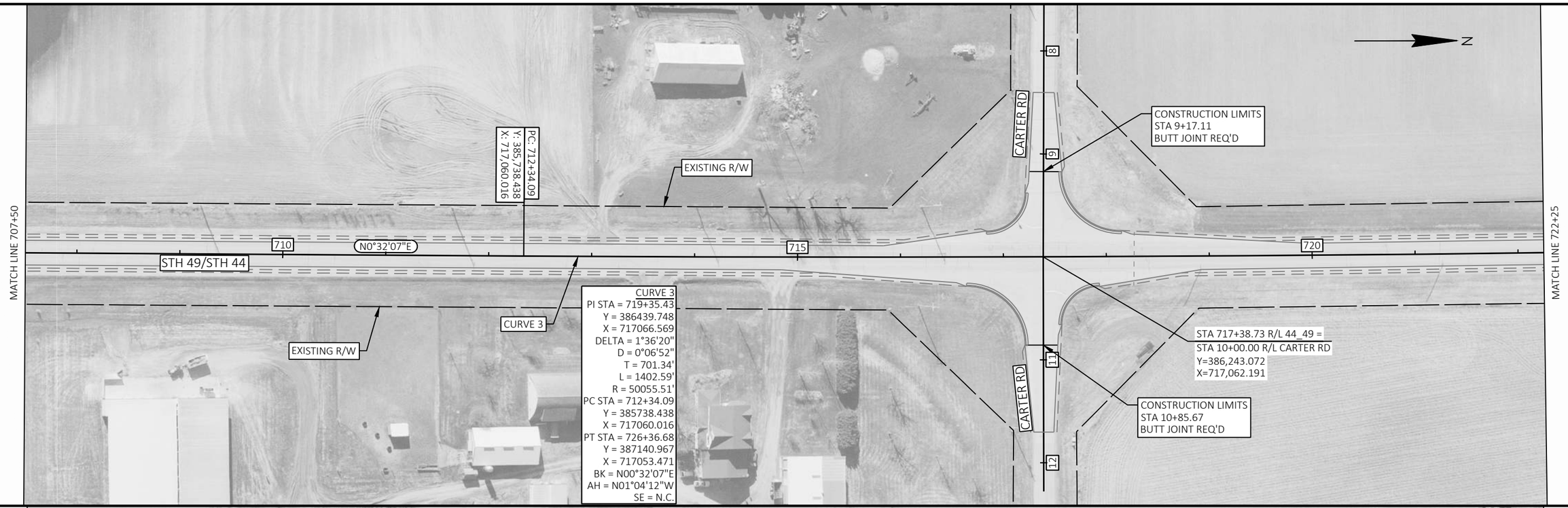
5

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PLAN SHEETS	SHEET 33	E

FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\050201-PN.DWG
LAYOUT NAME - 050208-pn

PLOT DATE : 3/13/2025 4:21 PM PLOT BY : CAMPSHURE, MICHAEL R PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44



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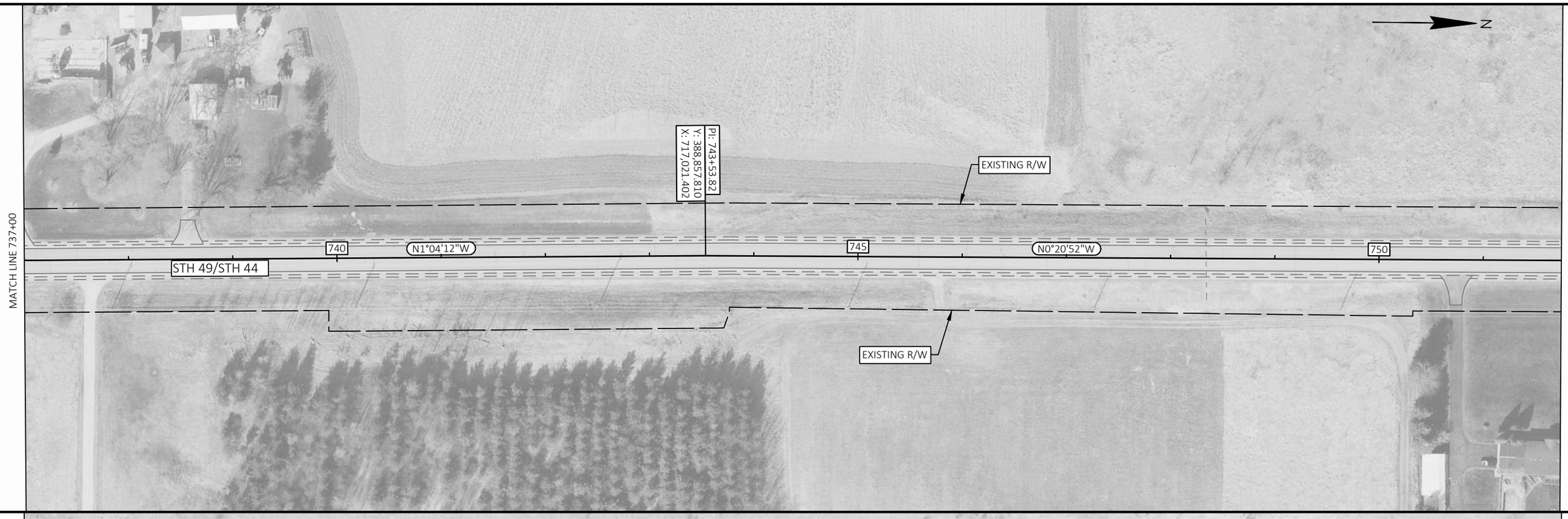
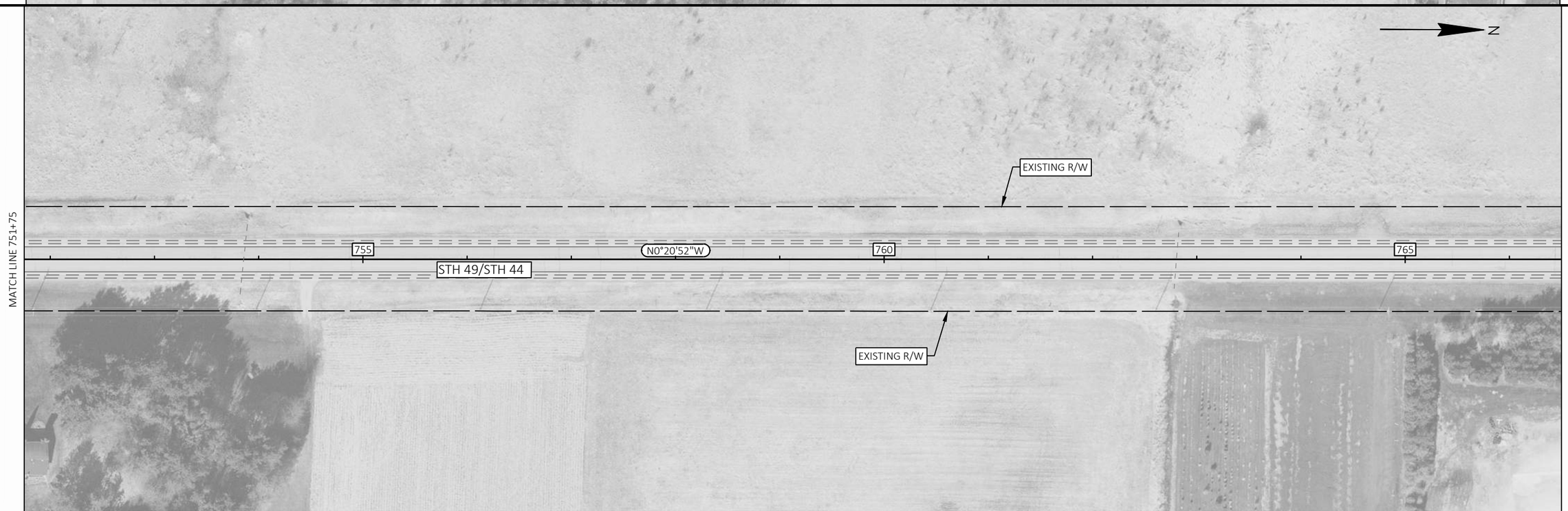
PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PLAN SHEETS	SHEET 34	E

FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\050201-PN.DWG
LAYOUT NAME - 050209-pn

PLOT DATE : 3/13/2025 4:21 PM PLOT BY : CAMPSHURE, MICHAEL R PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44





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PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PLAN SHEETS	SHEET 35	E

FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\050201-PN.DWG
LAYOUT NAME - 050210-pn

PLOT DATE : 3/13/2025 4:21 PM PLOT BY : CAMPSHURE, MICHAEL R PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

5

5

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PLAN SHEETS	SHEET 36	E

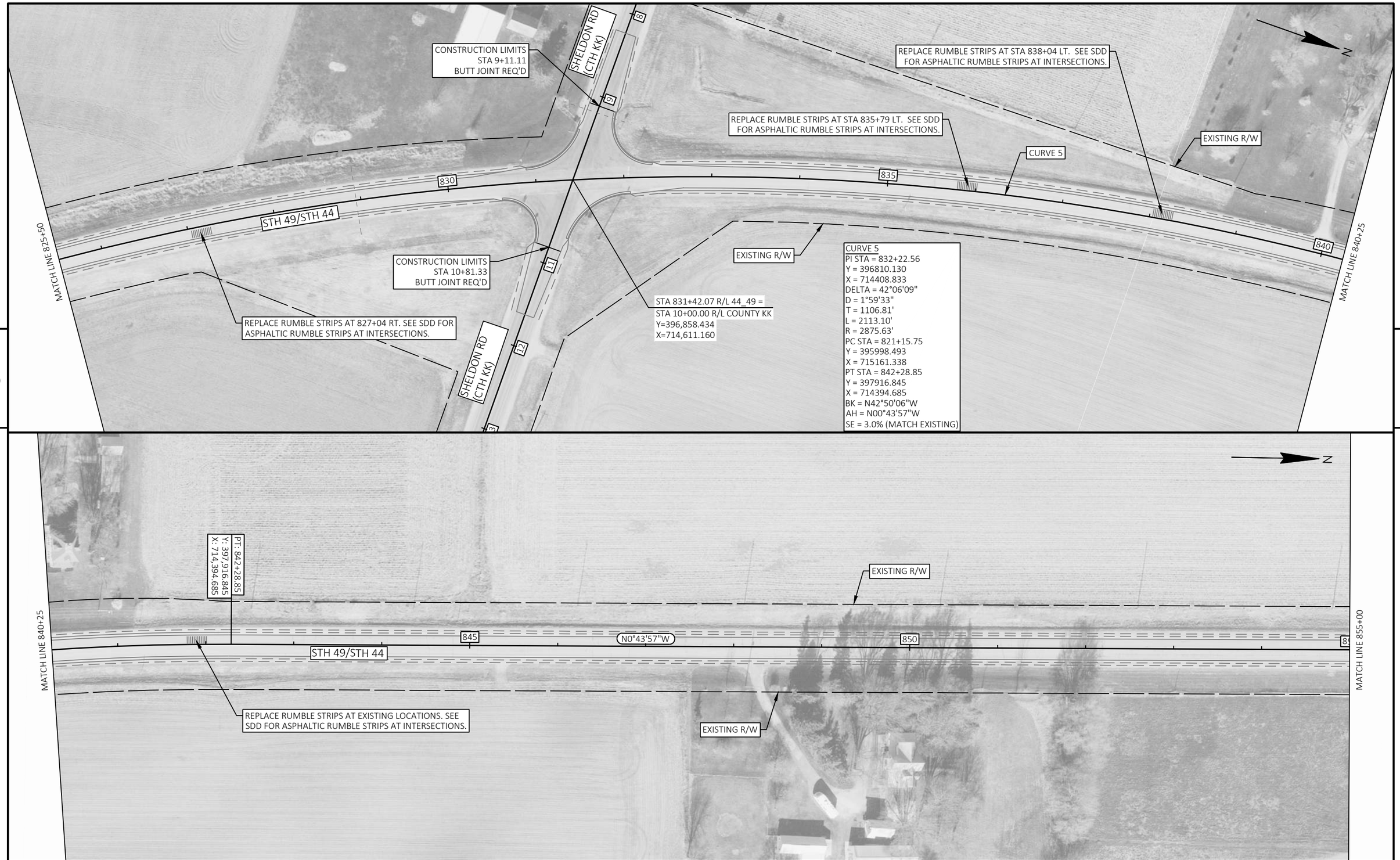
FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\050201-PN.DWG
LAYOUT NAME - 050211-pn

PLOT DATE : 3/13/2025 4:21 PM PLOT BY : CAMPSHURE, MICHAEL R PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PLAN SHEETS	SHEET 37 E

FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\050201-PN.DWG
LAYOUT NAME - 050212-pn
PLOT DATE : 3/13/2025 4:21 PM
PLOT BY : CAMPSHURE, MICHAEL R
PLOT NAME :
PLOT SCALE : 1 IN:100 FT
WISDOT/CADDS SHEET 44



PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PLAN SHEETS	SHEET 38 E

FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\050201-PN.DWG
LAYOUT NAME - 050213-pn

PLOT DATE : 3/13/2025 4:22 PM PLOT BY : CAMPSHURE, MICHAEL R PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

MATCH LINE 855+00

N

EXISTING R/W

855

860

N0°43'57"W

865

870

STH 49/STH 44

EXISTING R/W

MATCH LINE 870+25

N

EXISTING R/W

875

N0°43'57"W

880

885

STH 49/STH 44

EXISTING R/W

MATCH LINE 870+25

MATCH LINE 885+50

5

5

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PLAN SHEETS	SHEET 39	E

FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\050201-PN.DWG
LAYOUT NAME - 050214-pn

PLOT DATE : 3/13/2025 4:22 PM PLOT BY : CAMPSHURE, MICHAEL R PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

MATCH LINE 885+50

MATCH LINE 900+50

N

EXISTING R/W

PI: 893+79.57
Y: 403,067.139
X: 714,328.844

N0°43'57"W

890

STH 49/STH 44

895

N0°32'08"W

900

EXISTING R/W

5

5

BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
600	906+58	977.220'	BURY BOLT ON HYD SW QUAD
601	906+93	984.490'	E BOLT WITH CHIS X 222 FT WEST ON RINGSTAD

BM 601

REMOVE CURB & GUTTER

SEE CURB RAMP DETAILS

RINGSTAD DR

CONSTRUCTION LIMITS
STA 1+33.80
BUTT JOINT REQ'D

CURB RAMP
RECONSTRUCTION
(SEE CURB RAMP DETAILS)

REMOVE SIDEWALK AND CURB & GUTTER

REMOVE CURB & GUTTER
RECONSTRUCTING MANHOLE (SANITARY)
(SEE CURB RAMP DETAILS)

INSTALL MANHOLE COVERS
TYPE J - SPECIAL (STORM SEWER)
STA 911+68, 6' LT

RECONSTRUCTING MANHOLES
(CASTING/COVER PROVIDED BY MUNICIPALITY)
STA 912+07, 8' LT

CONSTRUCTION LIMITS
STA 912+13.00
BUTT JOINT REQ'D

EP: 914+78.06
Y: 405,165.504
X: 714,305.827

PI: 903+68.92
Y: 404,056.450
X: 714,319.596

EXISTING R/W

PI: 906+82.12
Y: 404,369.606
X: 714,314.045

BM 600

N

MATCH LINE 900+50

N0°32'08"W

905

N1°00'56"W

910

N0°35'30"W

N0°35'30"W

STH 49/STH 44

STH 49/STH 44

EXISTING R/W

STA 907+12.00 R/L 44_49 =
STA 2+00.00 R/L RINGSTAD DR
Y=404,399.476
X=714,313.737

END PROJECT
STA 912+13.00
Y=404,900.454
X=714,308.564

SANDMAR DR

PROJECT NO: 6090-14-71	HWY: STH 49	COUNTY: FOND DU LAC	PLAN SHEETS	SHEET 40	E

FILE NAME : N:\PDS\C3D\60901400\SHEETSPLAN\050201-PN.DWG
LAYOUT NAME - 050215-pn

PLOT DATE : 3/13/2025 4:22 PM PLOT BY : CAMPSHURE, MICHAEL R PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

Standard Detail Drawing List

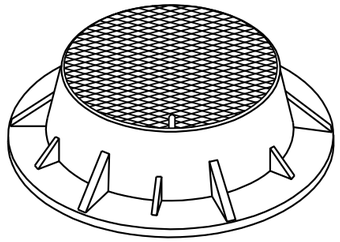
08A05-21E	INLET COVER TYPE BW, MANHOLE COVERS, TYPE K, J, J-S, L & M
08D01-23A	CONCRETE CURB & GUTTER
08D01-23B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-21D	CURB RAMPS TYPE 4B AND 4B1
08D05-21G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D16-11	CONCRETE GUTTER, CURB AND GUTTER AND PAVEMENT TIES
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
13A08-02	TRANSVERSE RUMBLE STRIPS, ASPHALTIC
13A10-03A	SHOULDER RUMBLE STRIPS - ASPHALT
13A10-03G	SHOULDER AND EDGE LINE RUMBLE STRIPS - CROSSINGS, INTERSECTIONS, BRIDGES, DRIVEWAYS
13A10-03H	SHOULDER AND EDGE LINE RUMBLE STRIPS - RAILROAD, PASSING, CLIMBING AND BYPASS LANES
13A11-04A	CENTERLINE RUMBLE STRIPS - ASPHALT
13A11-04D	CENTERLINE RUMBLE STRIPS - INTERSECTIONS, DRIVEWAYS, BRIDGES, RAILROADS
13C19-03	HMA LONGITUDINAL JOINTS
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS
15C07-16B	PAVEMENT MARKING WORDS
15C07-16C	PAVEMENT MARKING ARROWS
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C08-23B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C08-23D	PAVEMENT MARKING (TURN LANES)
15C09-13A	SIGNING AND PAVEMENT MARKING DETAILS FOR RAILROAD-HIGHWAY GRADE CROSSINGS
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C19-09A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C33-05	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-06A	PAVEMENT MARKING (INTERSECTIONS)
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D39-02	TRAFFIC CONTROL, DROP-OFF SIGNING
15D43-02	TRAFFIC CONTROL, SHORT DURATION MOBILE OPERATIONS
15D44-02	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES
15D51-01	TRAFFIC CONTROL, MOBILE OPERATIONS ON AN UNDIVIDED ROADWAY

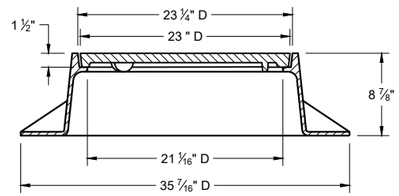
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR MANHOLE COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

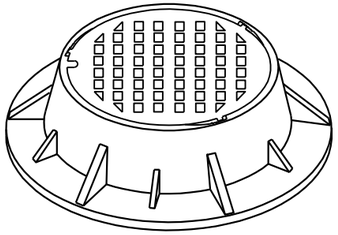
ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

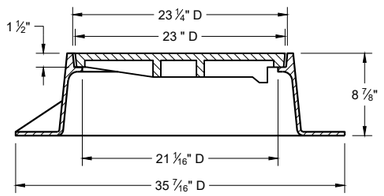




TYPE "J"

NOTE: EITHER CASTING IS ACCEPTABLE

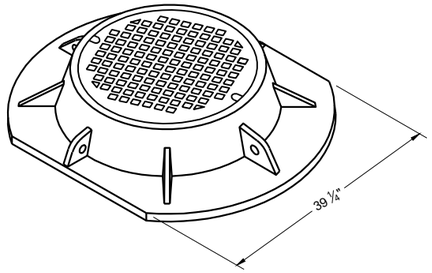


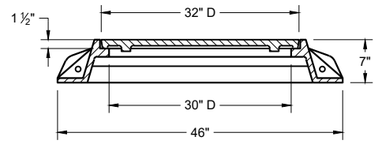


TYPE "J" SPECIAL

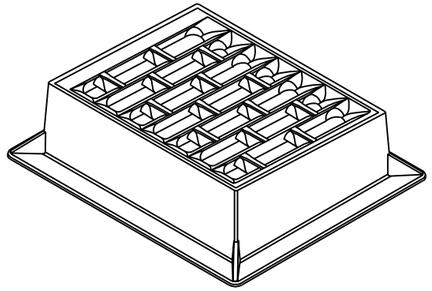
TYPE "B" NON-ROCKING SELF-SEAL LID
(NOTED AS TYPE J-S ON THE DRAINAGE TABLE)

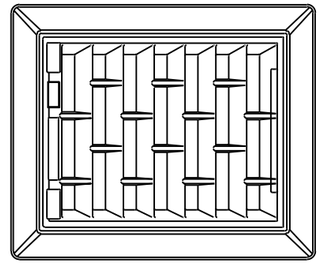
NOTE: EITHER CASTING IS ACCEPTABLE

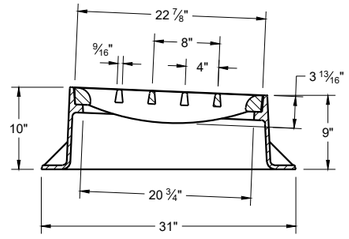


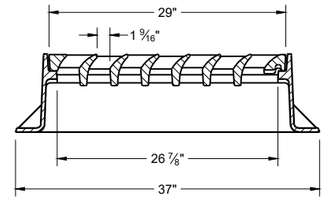


TYPE "K"

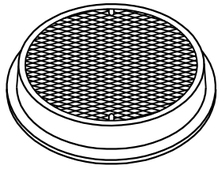


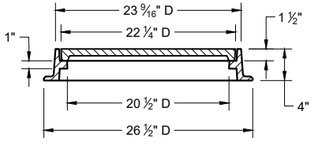




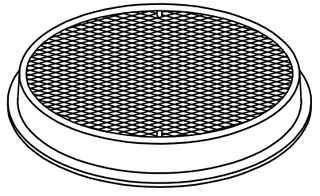


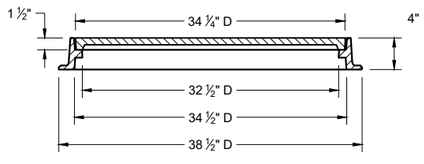
INLET COVER TYPE "BW"





TYPE "L"





TYPE "M"

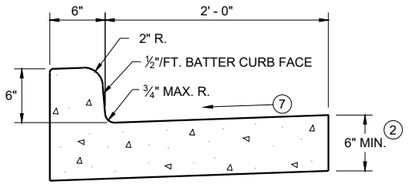
INLET COVERS TYPES BW
MANHOLE COVERS TYPES K, J, J-S, L, AND M

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

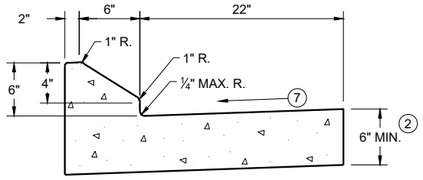
APPROVED
December 2023 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

FHWA

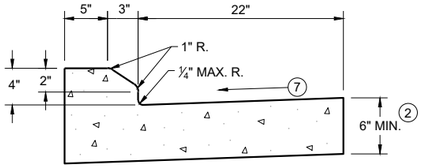
6
SDD 08A05-21e



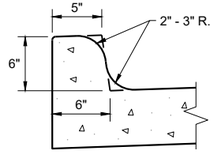
TYPES A (1) & D



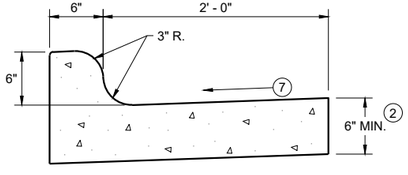
6" SLOPED CURB TYPES G (1) & J



4" SLOPED CURB TYPES G (1) & J

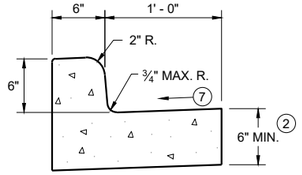


TYPES K (1) & L
(OPTIONAL CURB SHAPE)



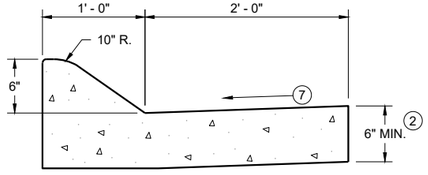
TYPES K (1) & L

CONCRETE CURB AND GUTTER 30"

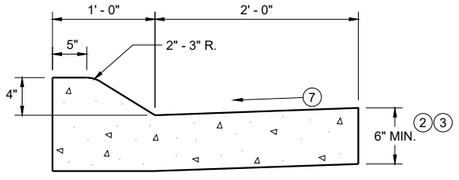


TYPES A (1) & D

CONCRETE CURB AND GUTTER 18"

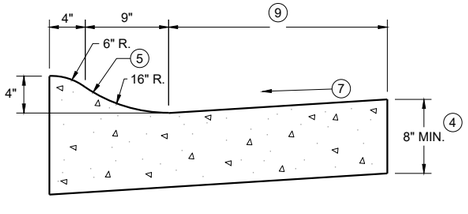


6" SLOPED CURB TYPES A (1) & D



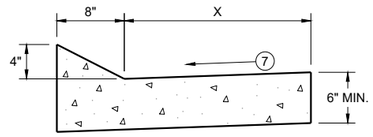
4" SLOPED CURB TYPES A (1) & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R (1) & T

TBT & TBTT	X
30"	22"
36"	28"

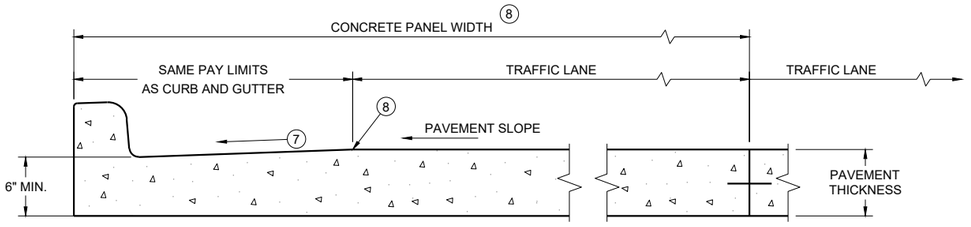


TYPES TBT & TBTT (1)

CONCRETE CURB AND GUTTER

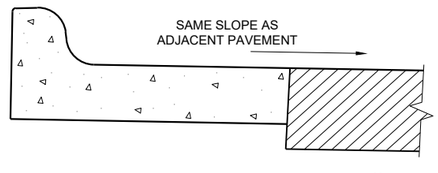
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT * WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER (6)
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.

(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(3) USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.

(4) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(5) UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.

(6) WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.

(7) USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.

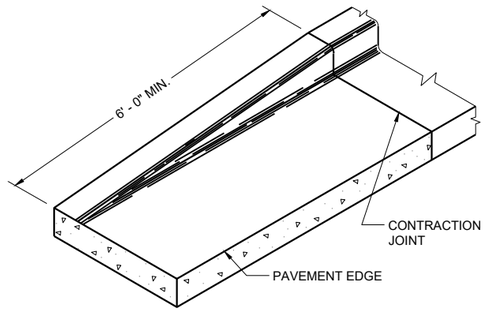
(8) INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.

(9) CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES

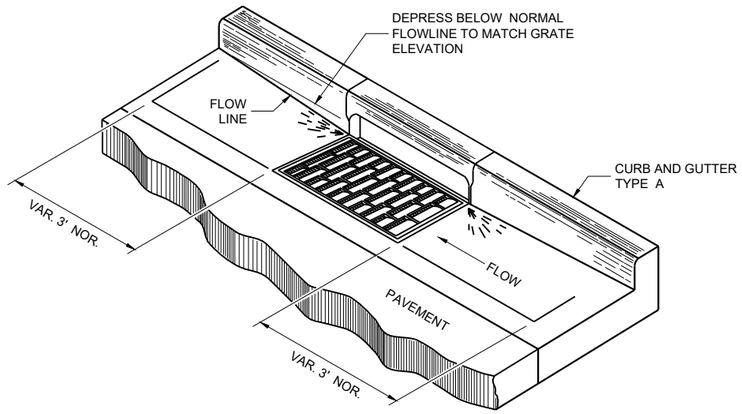
CONCRETE CURB AND GUTTER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

SDD 08D01-23a



END SECTION CURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS

(TYPICAL H INLET COVER SHOWN)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

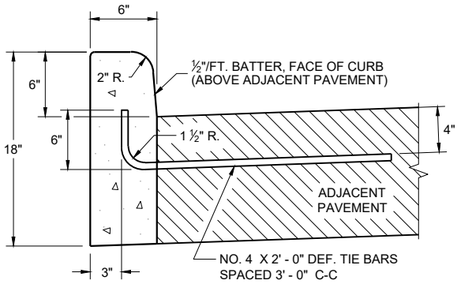
UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.

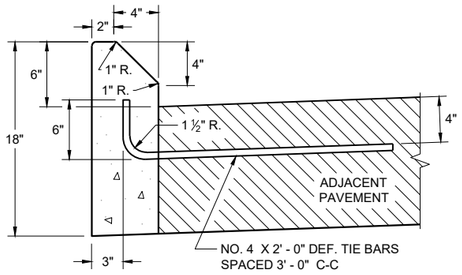
(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(10) REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.

(11) PLACE 1" THICK EXPANSION JOINT MATERIAL BETWEEN VERTICAL FACE CURB TYPES EXTENDING FROM THE TOP OF CURB TO 1 INCH BELOW THE ADJOINING CONCRETE SURFACE. RIGID CONCRETE STRUCTURES INCLUDE RAISED CONCRETE MEDIANS, CONCRETE SAFETY ISLANDS, SPLITTER ISLANDS, OR LOCATIONS IDENTIFIED ON THE PLANS.

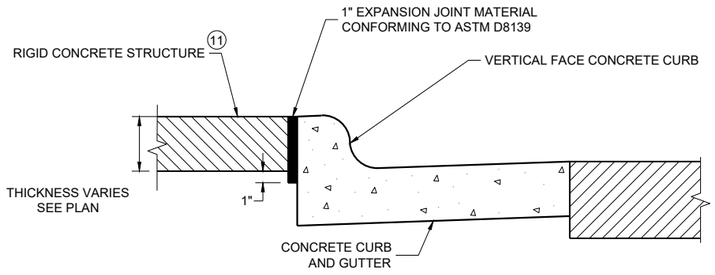


TYPES A (1) & D

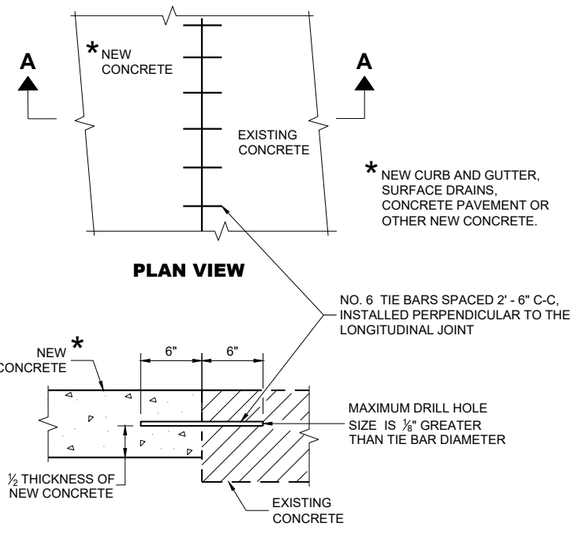


TYPES G (1) & J

CONCRETE CURB



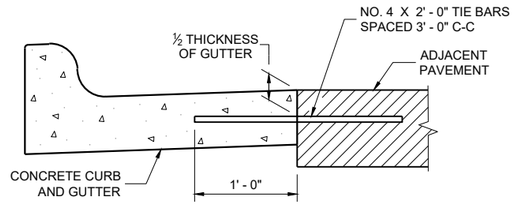
EXPANSION JOINT DETAIL FOR VERTICAL CURB ABUTTING A RIGID STRUCTURE (11)



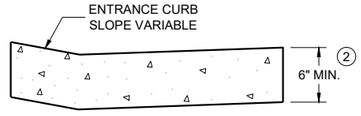
PLAN VIEW

SECTION A - A

TIE BARS DRILLED INTO EXISTING PAVEMENT



TYPICAL TIE BAR LOCATION (1)



DRIVEWAY ENTRANCE CURB (10)

(WHEN DIRECTED BY THE ENGINEER)

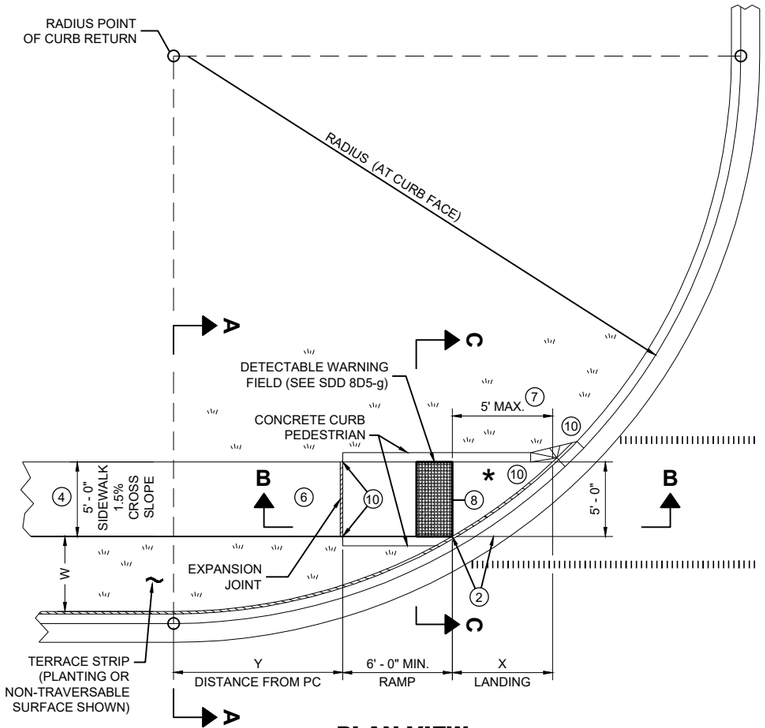
CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Rodnery Taylor — ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

FHWA

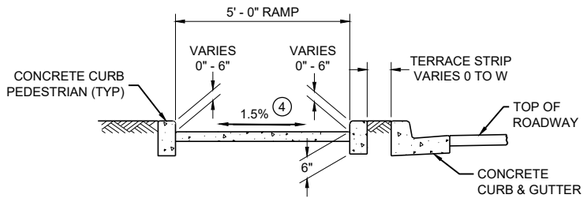
6
SDD 08D01-23b



PLAN VIEW
CURB RAMP TYPE 4B

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET			4' - 11 1/2"	5' - 1 3/4"	4' - 3 1/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET									4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET															4' - 10 3/4"	19' - 8 1/4"

INTERMEDIATE RADII CAN BE INTERPOLATED
DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH



SECTION C - C FOR TYPE 4B

LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

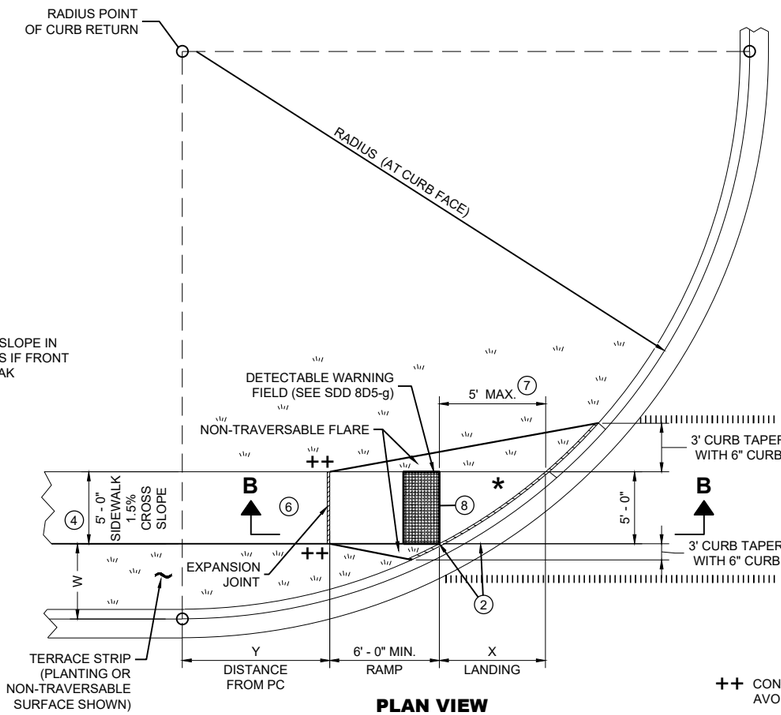
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

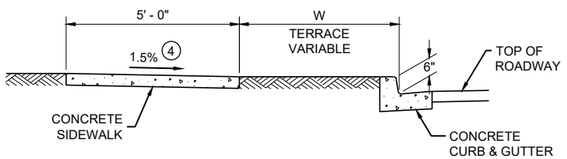
(7) WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

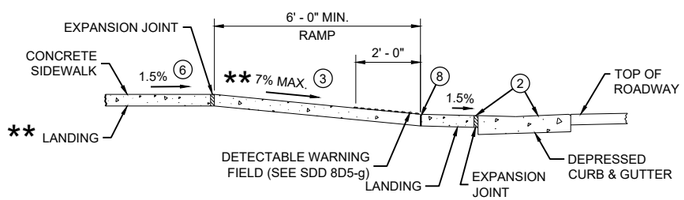
(10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.



PLAN VIEW
CURB RAMP TYPE 4B1



SECTION A - A FOR TYPE 4B

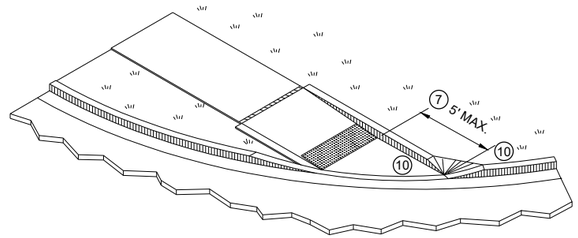


SECTION B - B FOR TYPE 4B AND TYPE 4B1

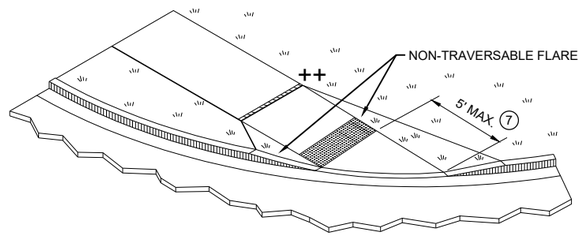
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



ISOMETRIC VIEW FOR TYPE 4B



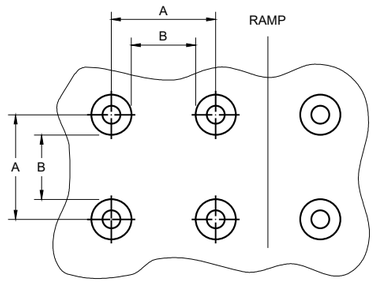
ISOMETRIC VIEW FOR TYPE 4B1

CURB RAMPS
TYPE 4B AND 4B1

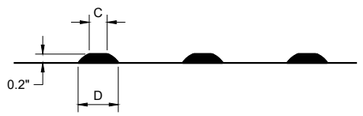
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

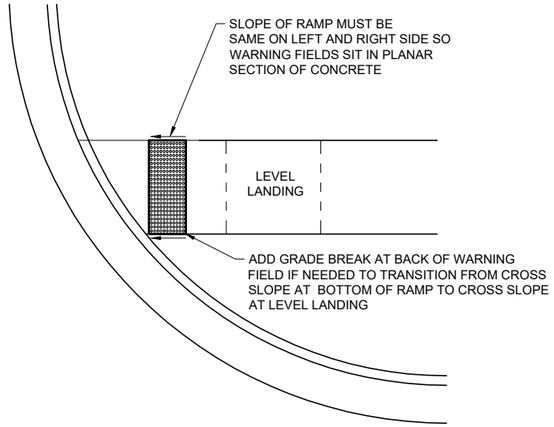
* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.



PLAN VIEW



ELEVATION VIEW



DETECTABLE WARNING FIELD
PLANAR INSTALLATION

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

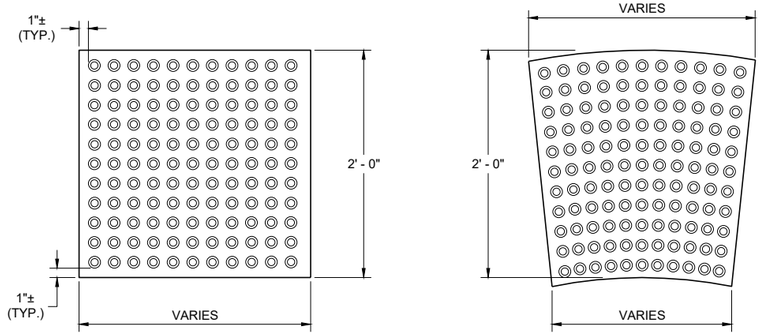
FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

(15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN ⅛" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

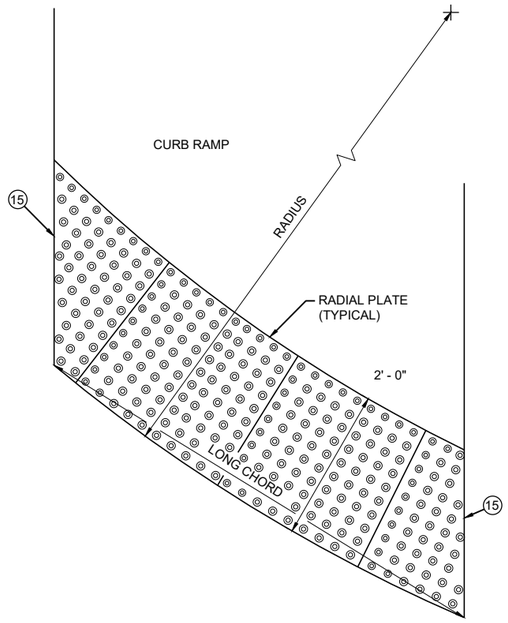
TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL



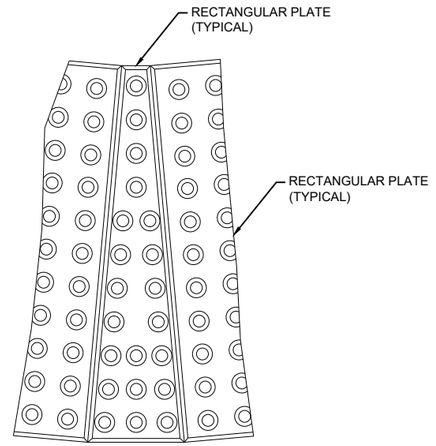
RECTANGULAR PLATES

RADIAL PLATES

PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)



PLAN VIEW
RADIAL DETECTABLE WARNING FIELD ATTRIBUTES



PLAN VIEW
RADIAL WEDGE PLATE CONNECTION DETAIL

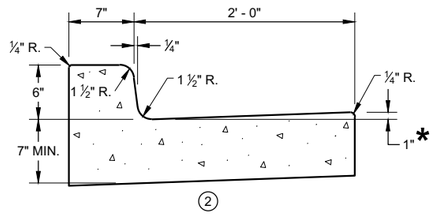
CURB RAMPS
RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2023 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

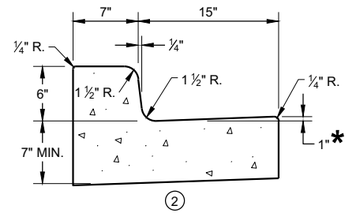
FHWA

6
SDD 08D05-21g

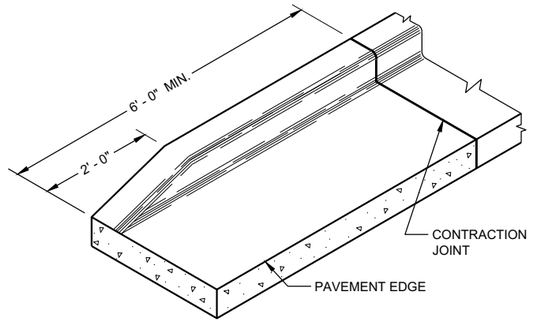


CONCRETE CURB AND GUTTER 31" ①

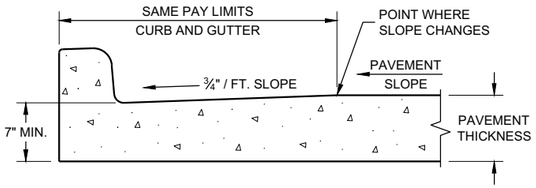
*TO BE MEASURED TO A MAXIMUM OF 3" WHERE DRAINAGE PROBLEMS EXIST.



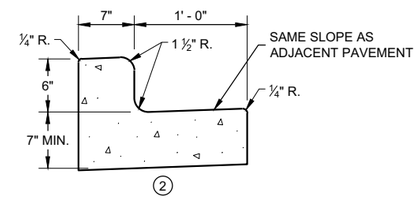
CONCRETE CURB AND GUTTER 22" ①



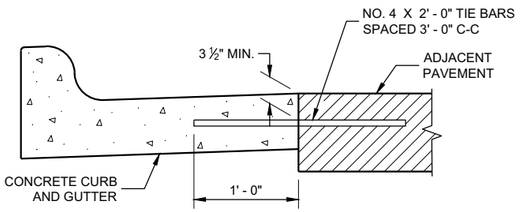
END SECTIONCURB AND GUTTER



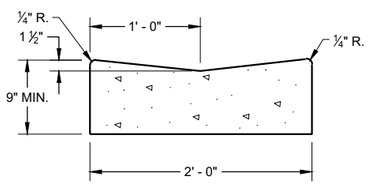
PARTIAL SECTION OF PAVEMENT WITH INTEGRAL CURB AND GUTTER



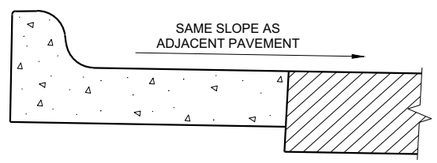
CONCRETE CURB AND GUTTER 19" ①



TYPICAL TIE BAR LOCATION ①



CONCRETE GUTTER 24" ①



HIGH SIDE SECTION ③

(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

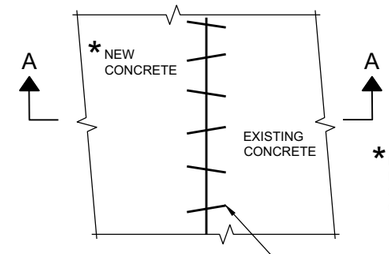
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE. A LONGITUDINAL CONSTRUCTION JOINT IS NOT REQUIRED WITH INTEGRAL CURB AND GUTTER.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE COURSE AND UNCLASSIFIED EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

① WHEN PLACED ADJACENT TO NEW CONCRETE, TIE BARS ARE REQUITED FOR CURB AND GUTTER 31", 22", 19" AND CONCRETE GUTTER 24".

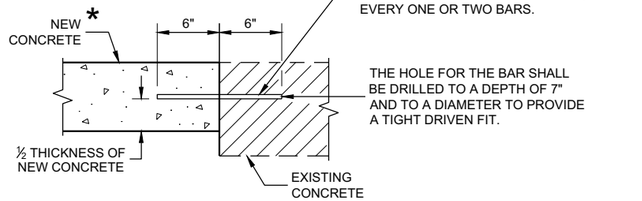
② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE COURSE PROVIDED A 7" MINIMUM GUTTER THICKNESS IS MAINTAINED.

③ WHEN HIGH SIDE CURB SECTION IS REQUIRED, THE LOCATION(S) WILL BE NOTED ON THE PLANS



PLAN VIEW

* EXISTING AND NEW CONCRETE MAY BE CURB AND GUTTER, SURFACE DRAINS, PAVEMENT OR OTHER CONCRETE STRUCTURE.



SECTION A - A

PAVEMENT TIES

CONCRETE GUTTER, CURB AND GUTTER AND PAVEMENT TIES

(For Optional use in Milwaukee Co. Only)

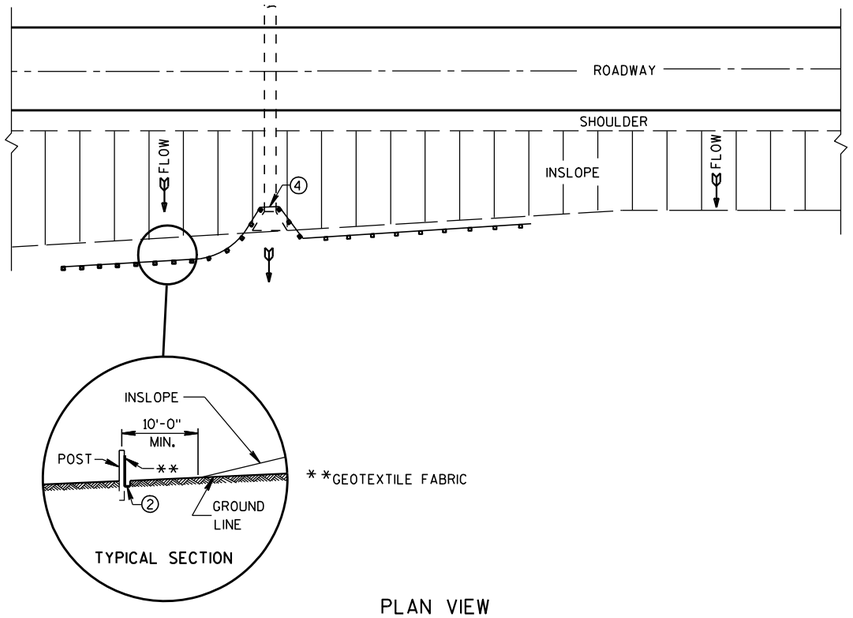
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVE ENGINEER
FHWA

6

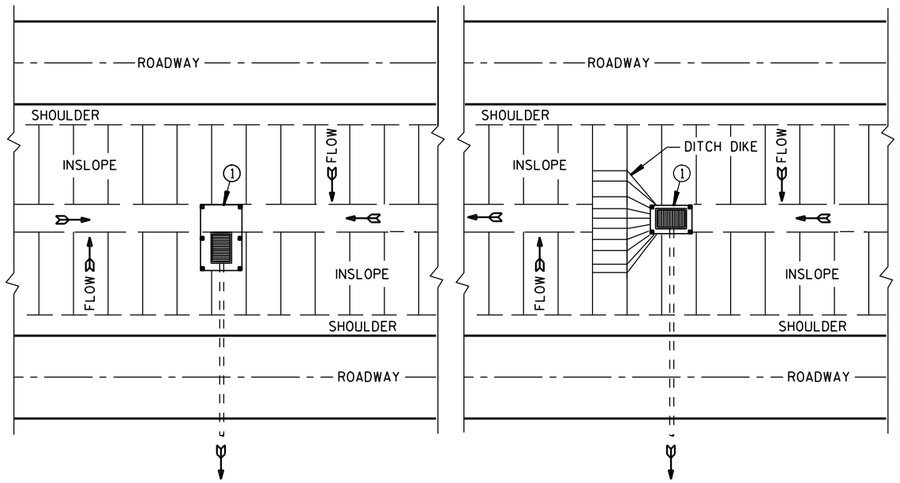
SDD 08D16 - 11

SDD 08D16 - 11



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



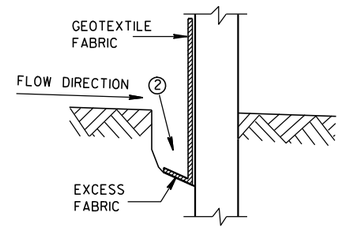
PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

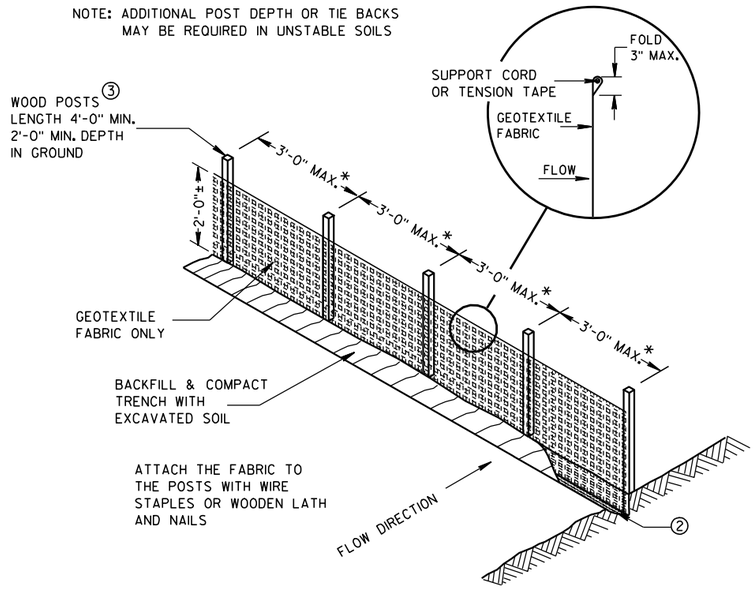
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

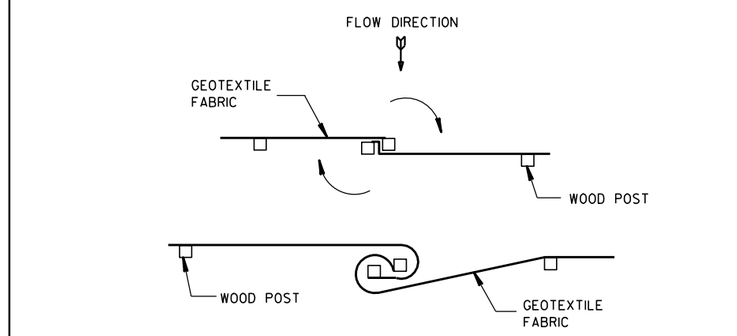
1. HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
2. FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
3. WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.
4. SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
5. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



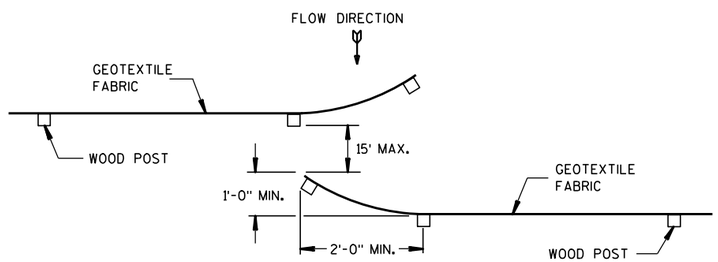
TRENCH DETAIL



SILT FENCE

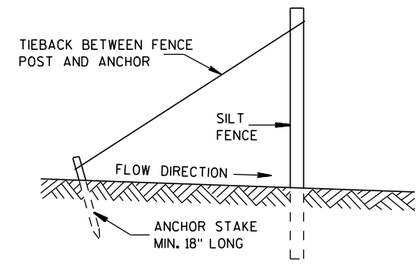


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE 5



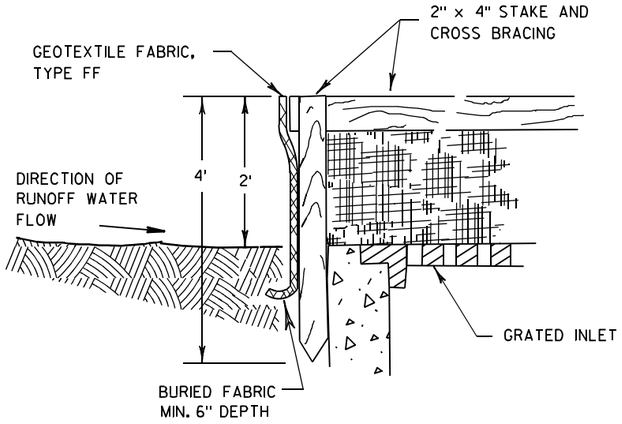
SILT FENCE TIE BACK

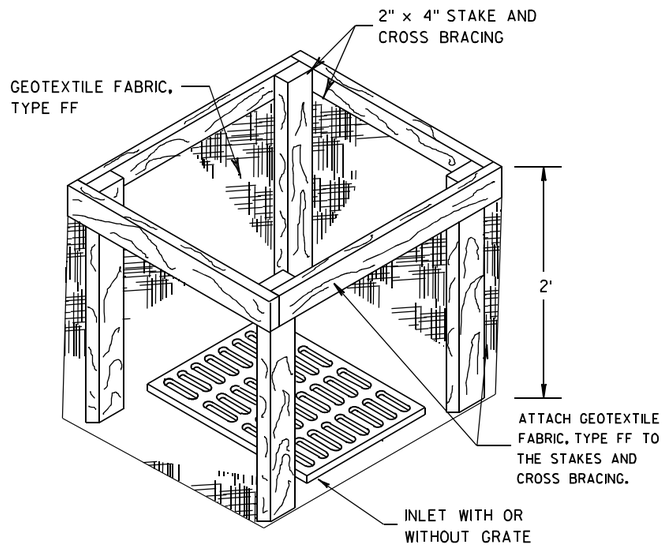
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED	
4-29-05 DATE	/S/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	

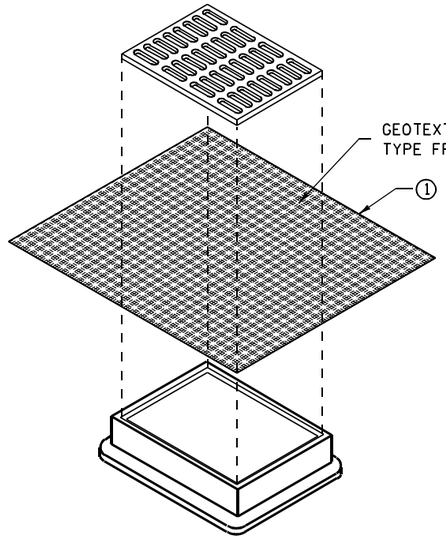
6

S.D.D. 8 E 9-6



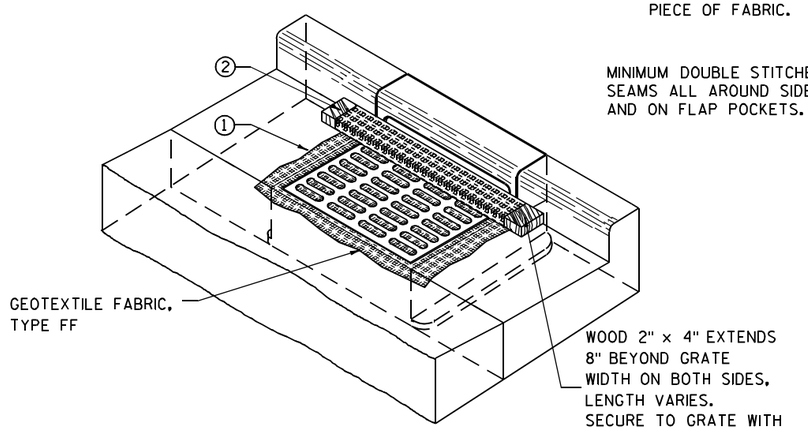


INLET PROTECTION, TYPE A

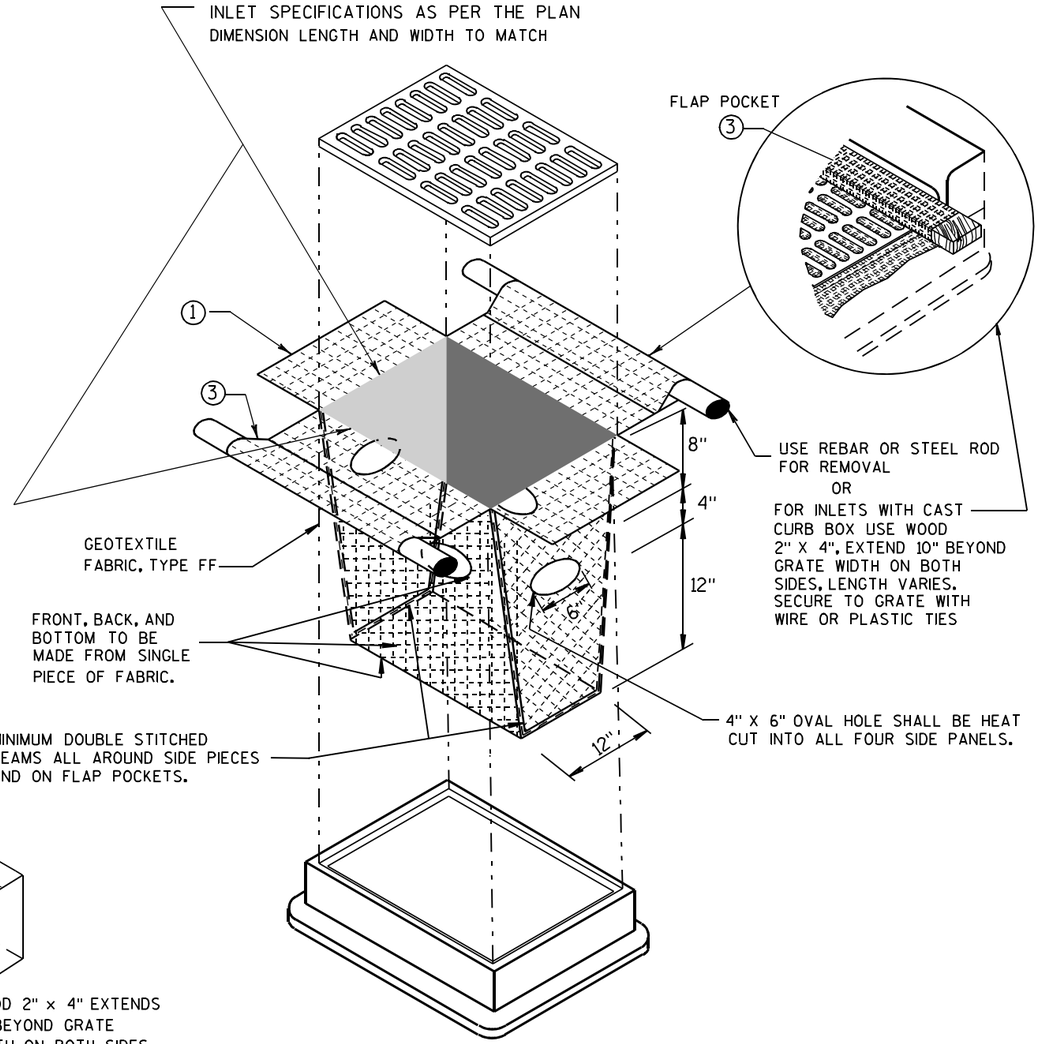


INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH
OR WITHOUT A CURB BOX AS PER NOTE ②)

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.

③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION
TYPE A, B, C, AND D

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02
DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

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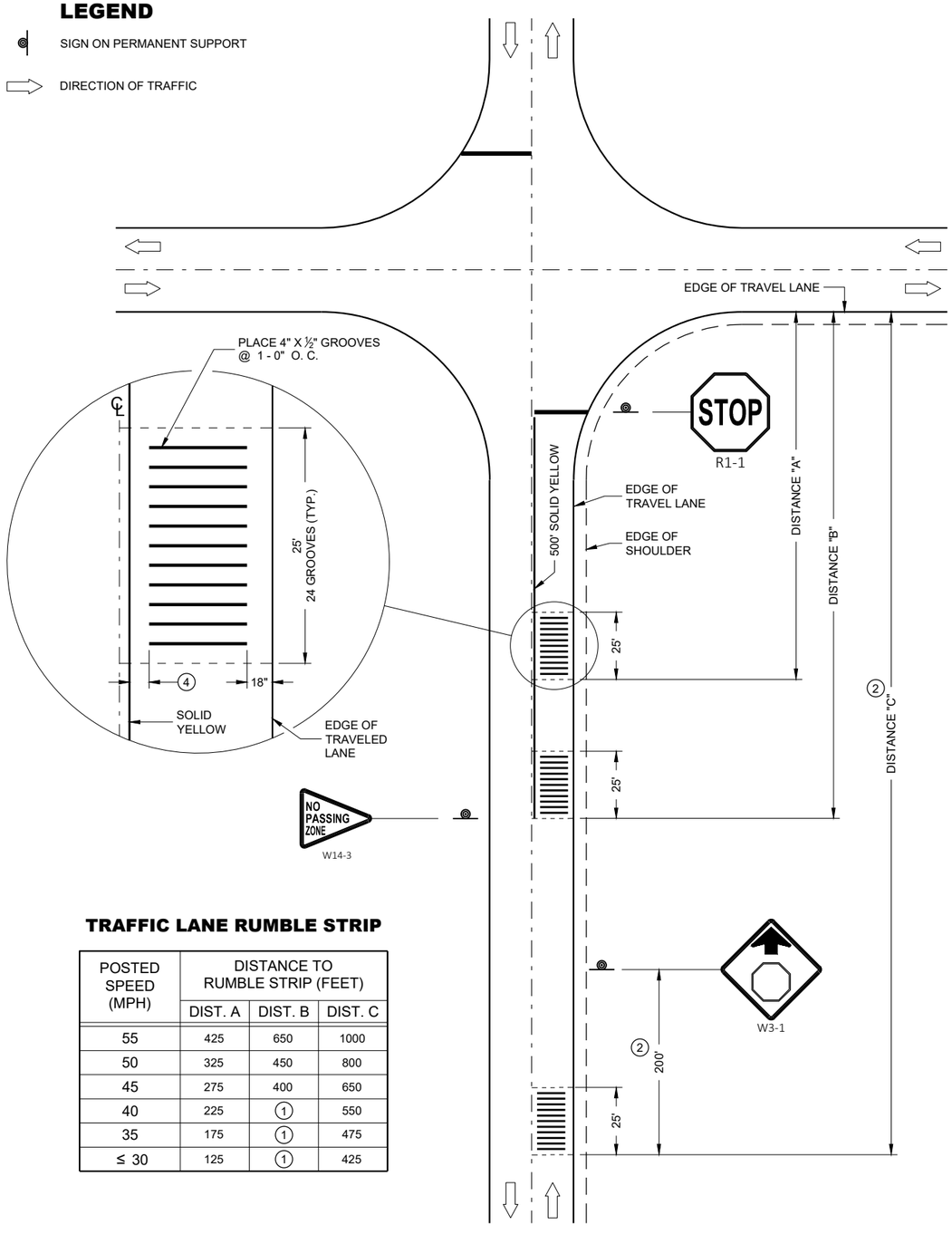
S.D.D. 8 E 10-2

LEGEND

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC



TRAFFIC LANE RUMBLE STRIP

POSTED SPEED (MPH)	DISTANCE TO RUMBLE STRIP (FEET)		
	DIST. A	DIST. B	DIST. C
55	425	650	1000
50	325	450	800
45	275	400	650
40	225	(1)	550
35	175	(1)	475
≤ 30	125	(1)	425

RUMBLE STRIP LOCATION

GENERAL NOTES

CONTRACTOR SHALL CONFIRM RUMBLE STRIP LOCATION WITH THE ENGINEER PRIOR TO INSTALLATION. THE ENGINEER MAY MODIFY THE RUMBLE STRIP LOCATION AS FIELD CONDITIONS DICTATE.

WHEN ASPHALTIC PAVEMENT IS NEW IN THE RUMBLE AREA, THE CONTRACTOR SHALL ALLOW THE PAVEMENT TO CURE A MINIMUM OF 7 DAYS PRIOR TO RUMBLE INSTALLATION.

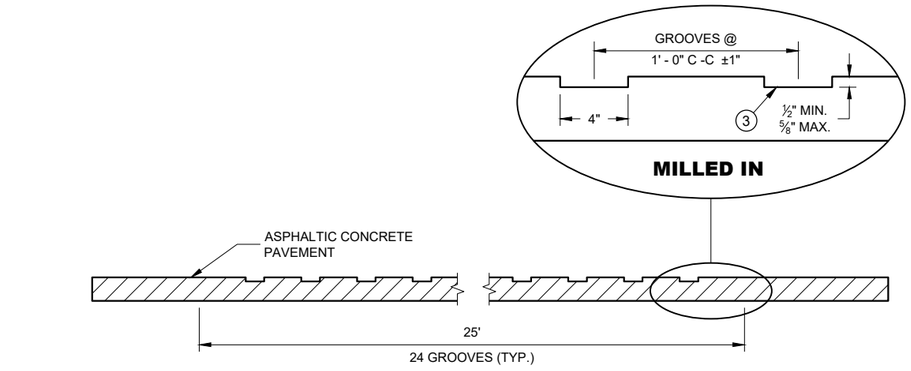
PAVEMENT MARKING AND SIGNING DETAILS AND SPECIFICATIONS ARE PROVIDE ELSEWHERE IN THE CONTRACT.

(1) ELIMINATE THE MIDDLE SET OF RUMBLE STRIPS.

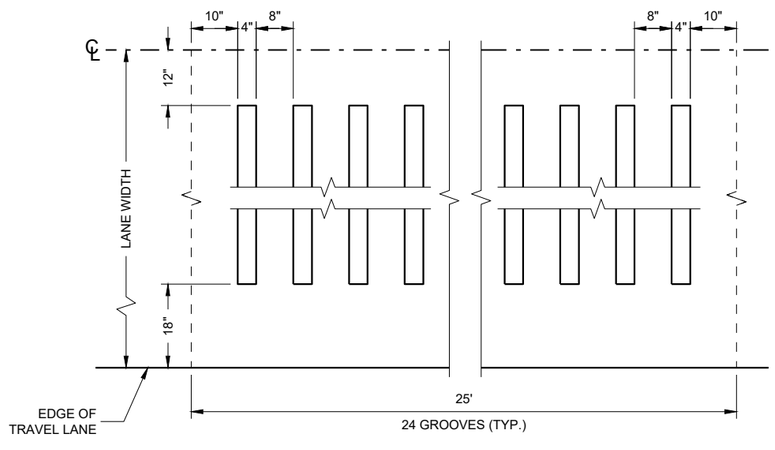
(2) LOCATE RUMBLE STRIP 200 FEET IN ADVANCE OF W3-1 SIGN AS SHOWN. IF W3-1 IS NOT IN PLACE, USE DISTANCE "C".

(3) TYPICAL VERTICAL VARIATION BETWEEN PEAKS AND VALLEYS WITHIN THE CUT APPROXIMATELY $\frac{1}{16}$".

(4) 12 INCH CLEAR BETWEEN THE SOLID YELLOW LINE AND THE EDGE OF THE RUMBLE.



ELEVATION VIEW



PLAN VIEW

ASPHALTIC PAVEMENT MILLED IN

TRANSVERSE RUMBLE STRIPS, ASPHALTIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE

/S/ Rodney Taylor
ROADWAY DESIGN STANDARDS UNIT SUPERVISOR

FHWA

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SDD 13A08 - 02

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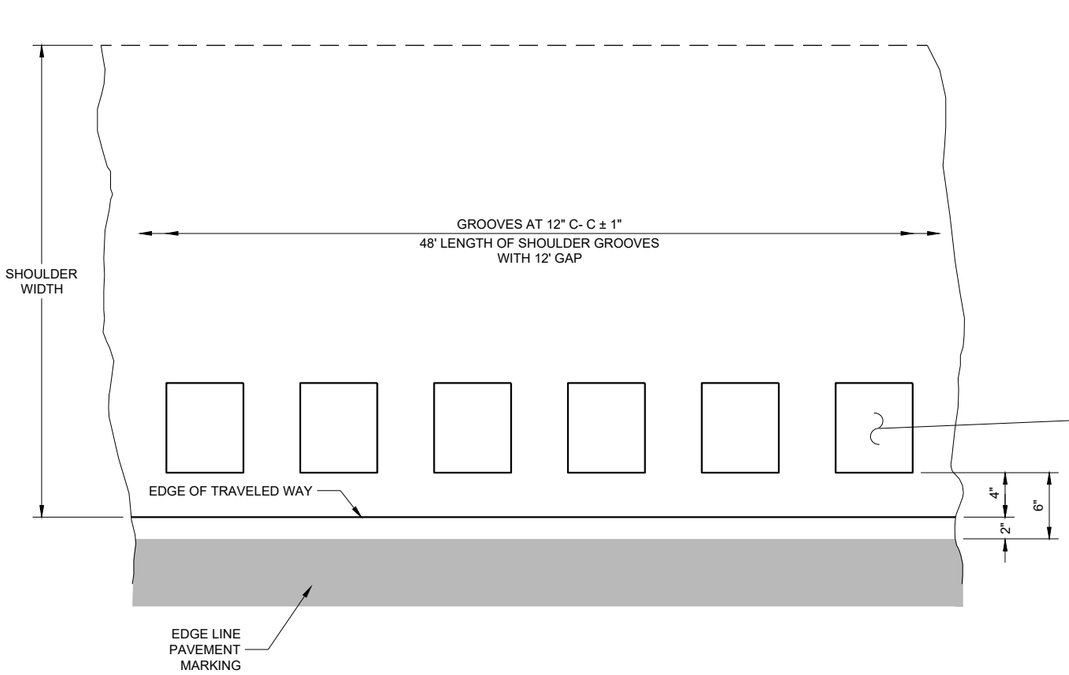
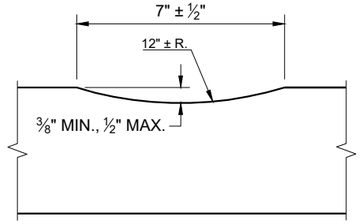
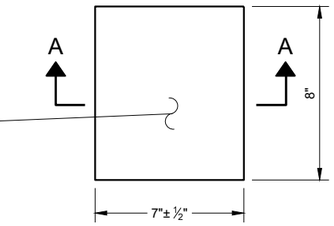
SDD 13A08 - 02



PLAN DETAIL VIEW
SHOULDER WITH GROOVES



SECTION A - A

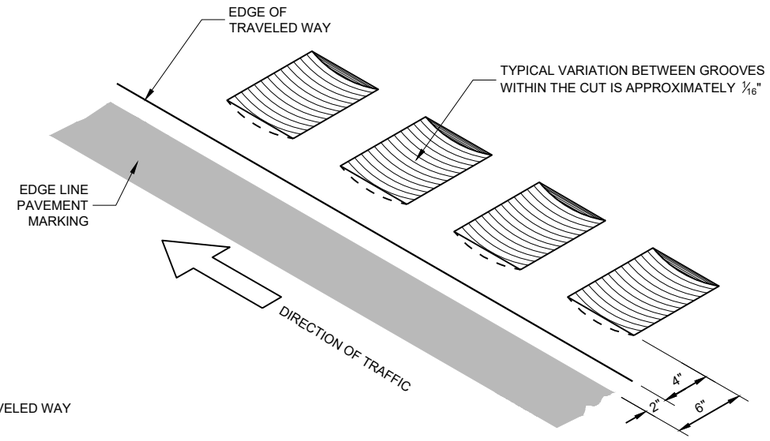


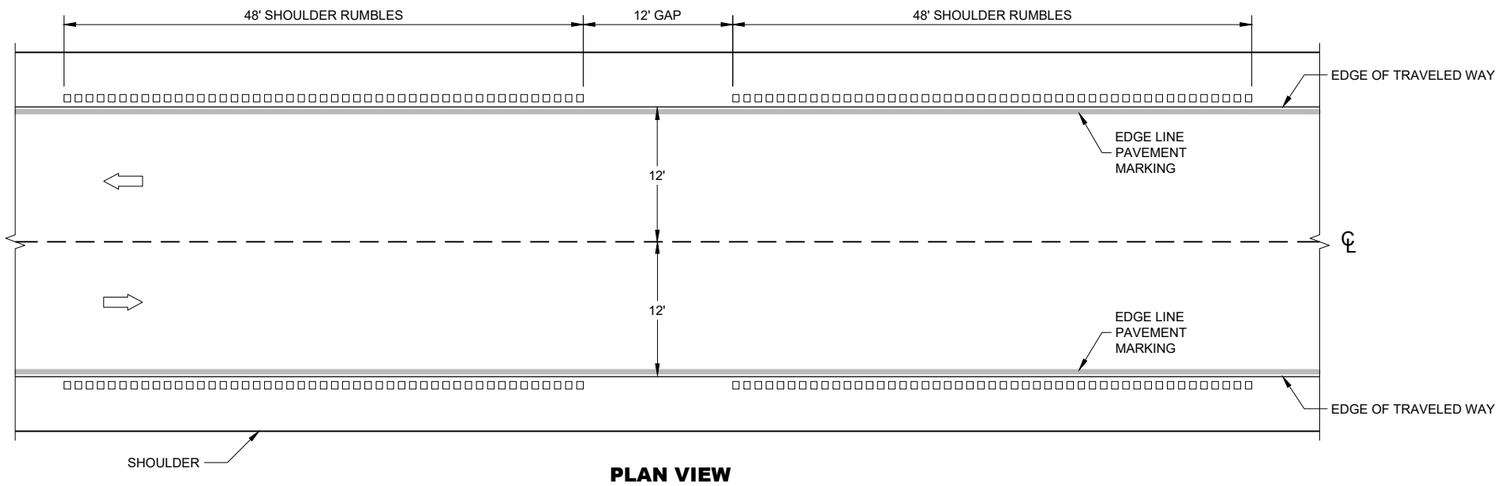
PLAN VIEW
(SINGLE GROOVE)

GENERAL NOTES

DO NOT MILL SHOULDER GROOVES THROUGH INTERSECTIONS, MARKED CROSSWALKS, NON-MOTORIZED PATH CROSSINGS, ETC. REFER TO SDD 13A10 SHEETS "g" AND "h".

SHOULDER GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS WHEN DIRECTED BY THE ENGINEER.





PLAN VIEW

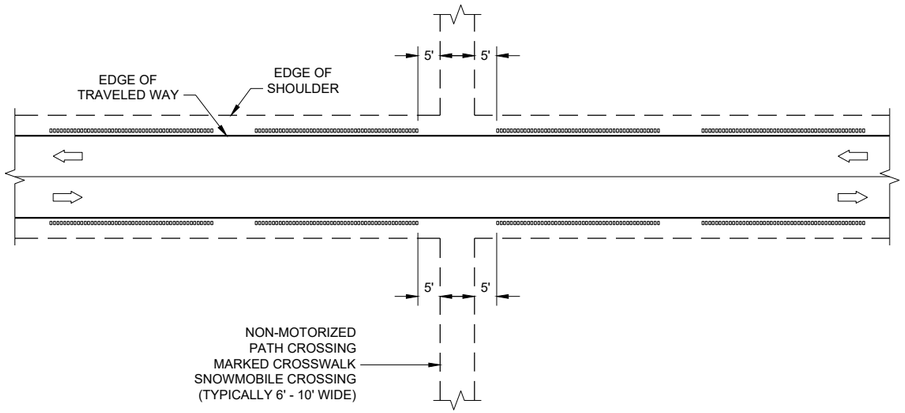
SHOULDER RUMBLE STRIPS - ASPHALT

SHOULDER RUMBLE STRIPS ASPHALT

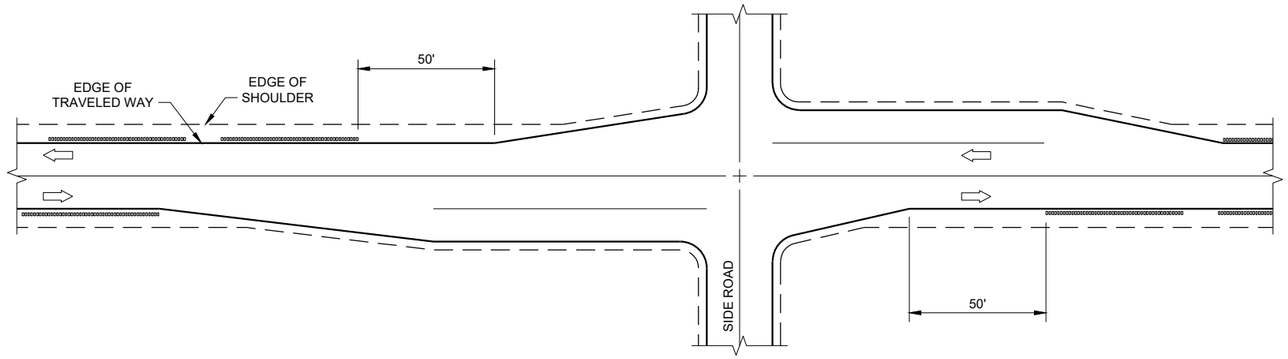
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTA

6

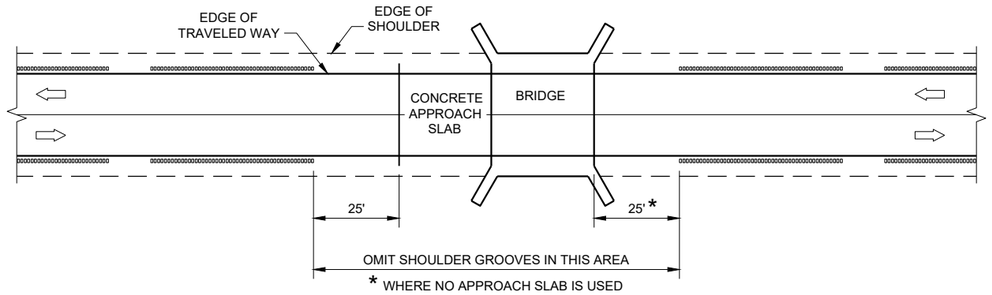
SDD 13A10 - 03a



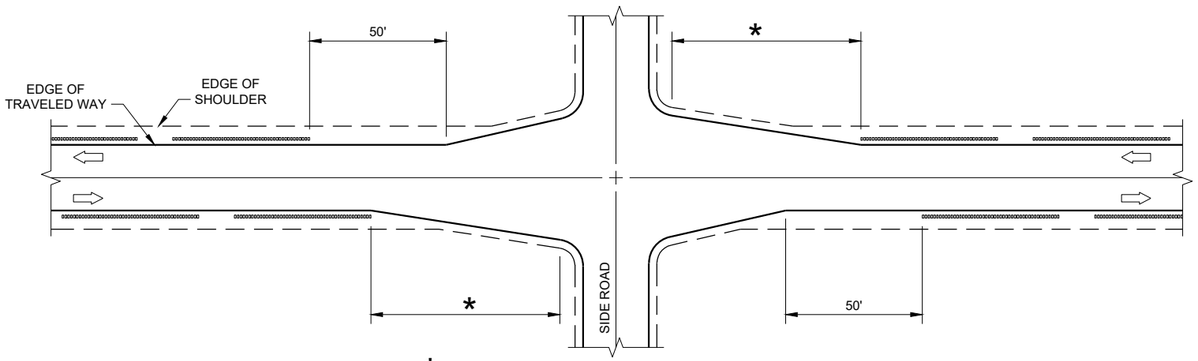
GROOVES AT MISCELLANEOUS CROSSINGS



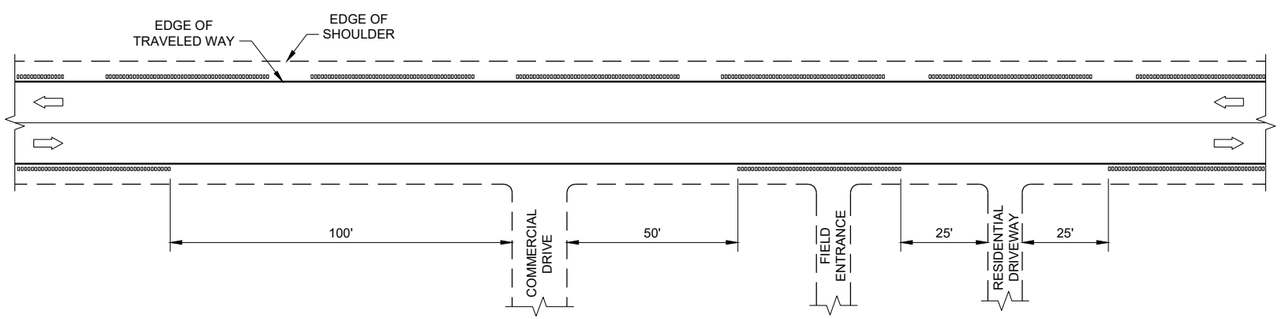
GROOVES AT RIGHT TURN LANE



GROOVES AT BRIDGES



GROOVES AT INTERSECTIONS WITH APPROACH TAPER



GROOVES AT DRIVEWAYS ①

GENERAL NOTES

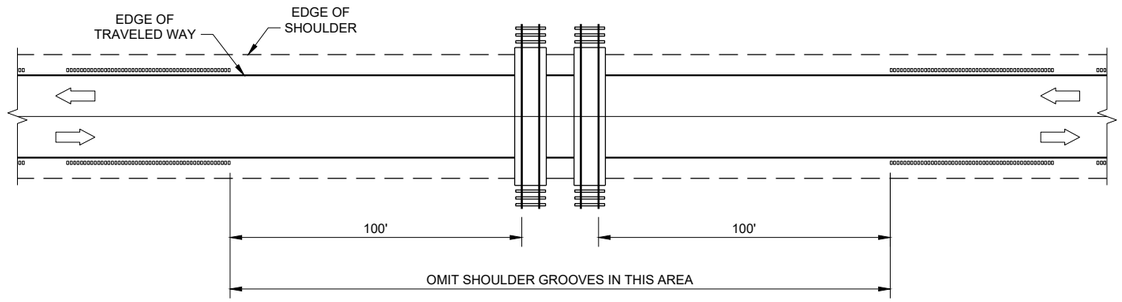
① SHOULDER GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.

SHOULDER AND EDGE LINE RUMBLE STRIPS CROSSINGS, INTERSECTIONS, BRIDGES, DRIVEWAYS

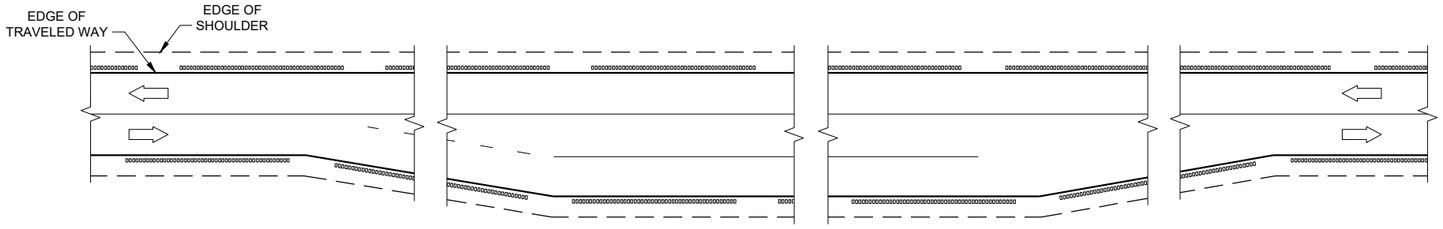
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

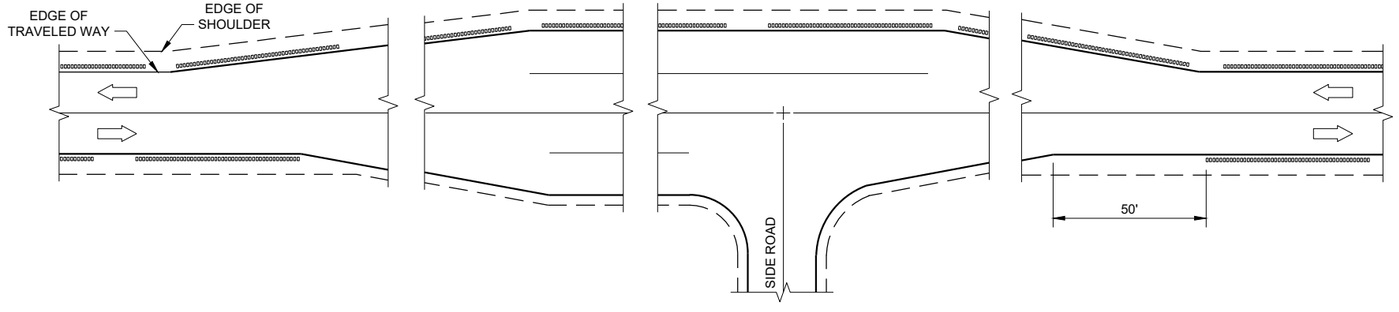
SDD 13A10 - 03g



GROOVES AT RAILROADS



GROOVES AT PASSING AND CLIMBING LANES



GROOVES AT BYPASS LANES

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SDD 13A10 - 03h

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SDD13A10 - 03h

SHOULDER AND EDGE LINE RUMBLE STRIPS - RAILROAD, PASSING, CLIMBING AND BYPASS LANES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

May 2023
DATE

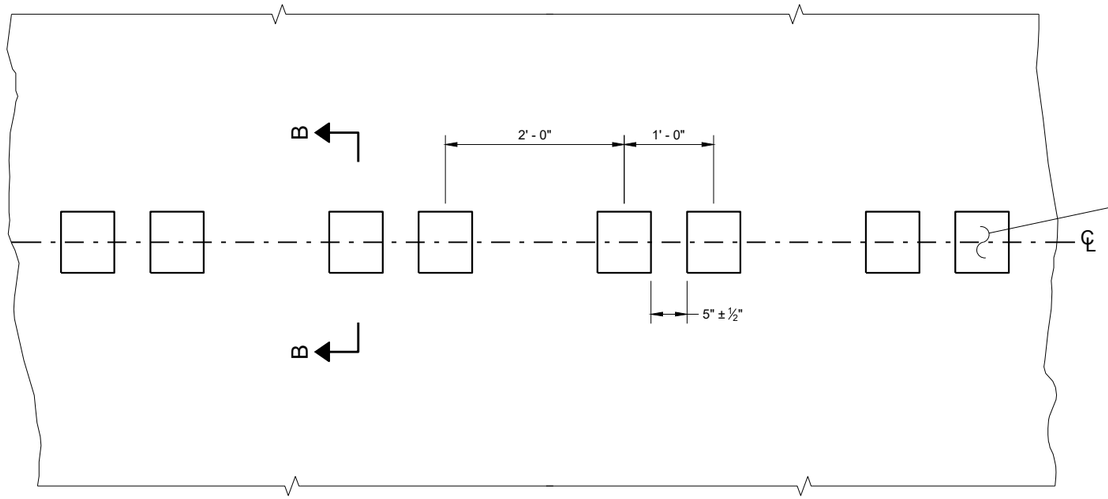
/S/ John Jenkins
ROADWAY STANDARDS DEVE ENGINEER

FHWA

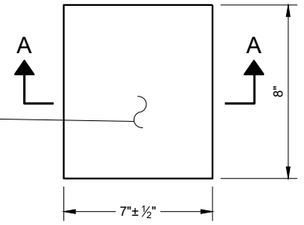
GENERAL NOTES

DO NOT MILL SHOULDER GROOVES THROUGH INTERSECTIONS, MARKED CROSSWALKS, NON-MOTORIZED PATH CROSSINGS, ETC. REFER TO SDD 13A11 SHEETS "d" AND "e".

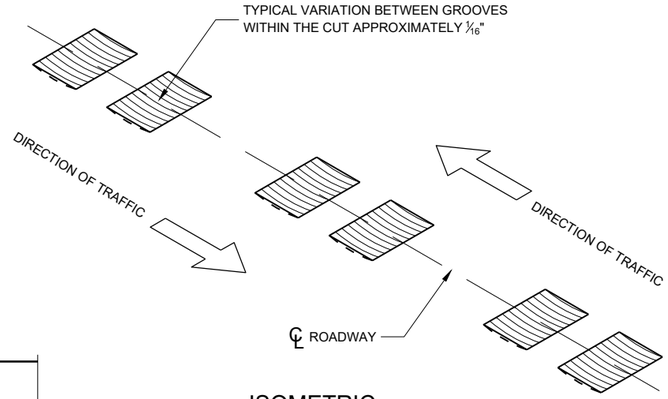
CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS WHEN DIRECTED BY THE ENGINEER.



PLAN DETAIL VIEW



PLAN VIEW
(SINGLE GROOVE)



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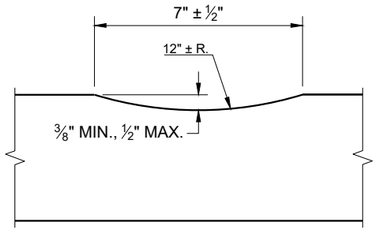
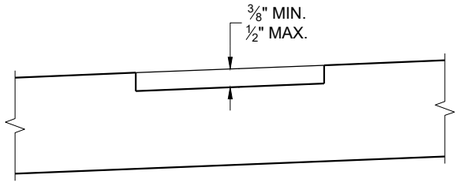
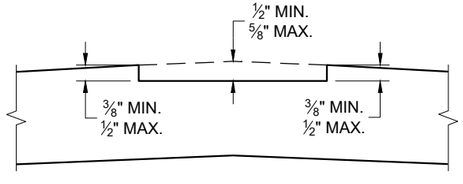


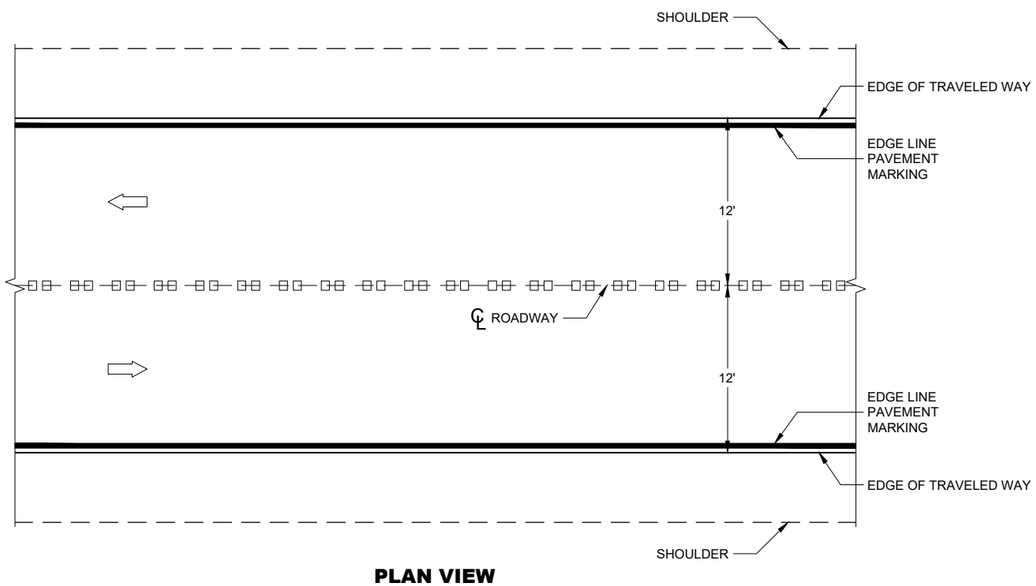
SECTION A - A



SECTION B - B
SUPERELEVATED ROADWAY



SECTION B - B
CROWNED ROADWAY



PLAN VIEW

CENTERLINE RUMBLE STRIPS - ASPHALT

CENTERLINE RUMBLE STRIPS - ASPHALT	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	54

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SDD 13A11 - 04a

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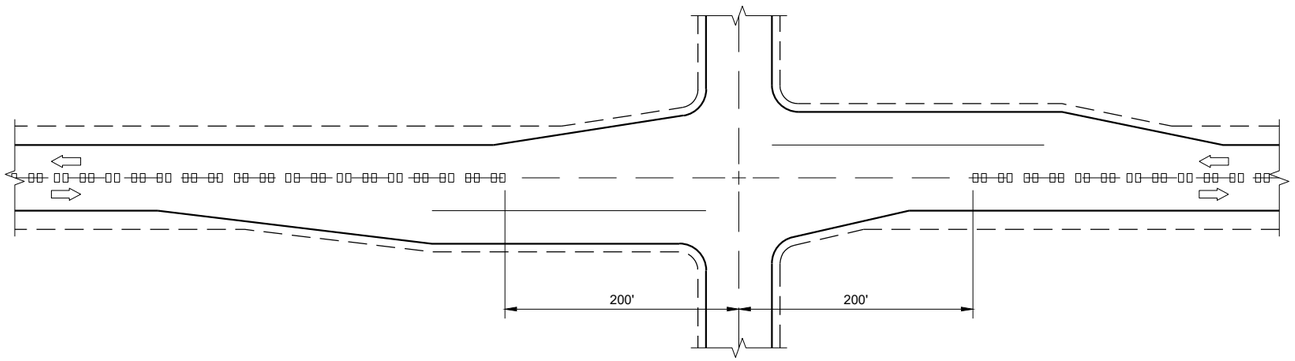
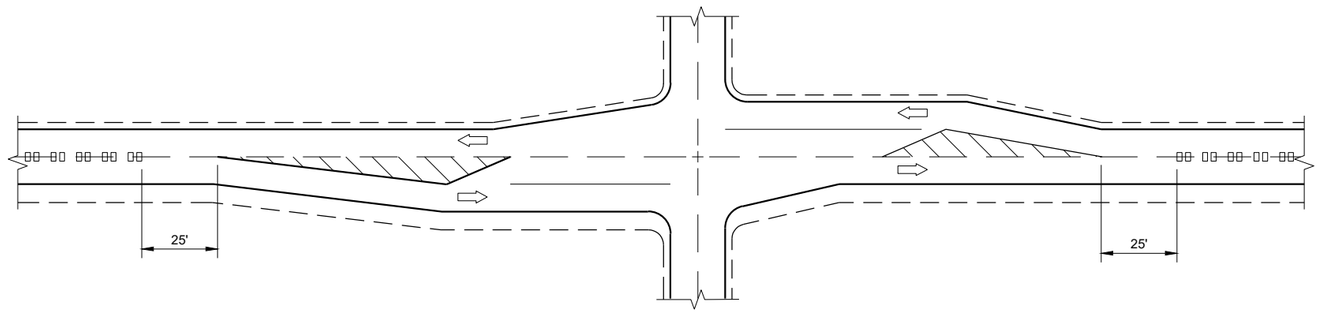
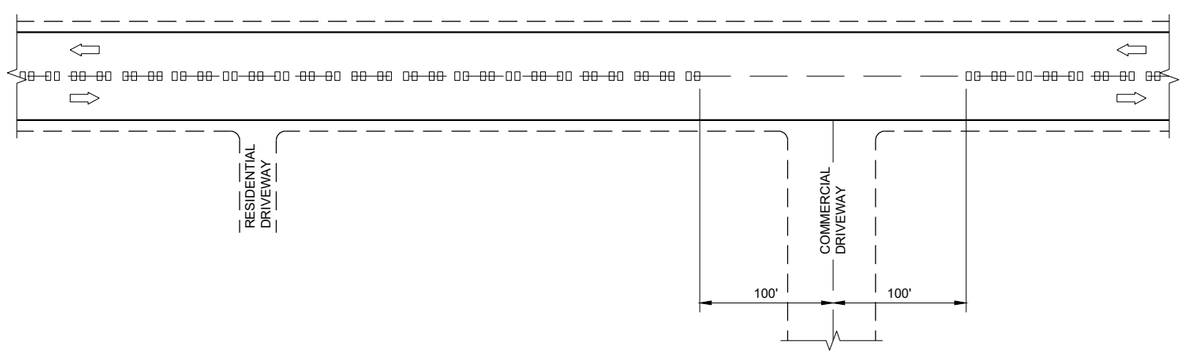
SDD 13A11 - 04a



CENTERLINE GROOVES AT INTERSECTIONS



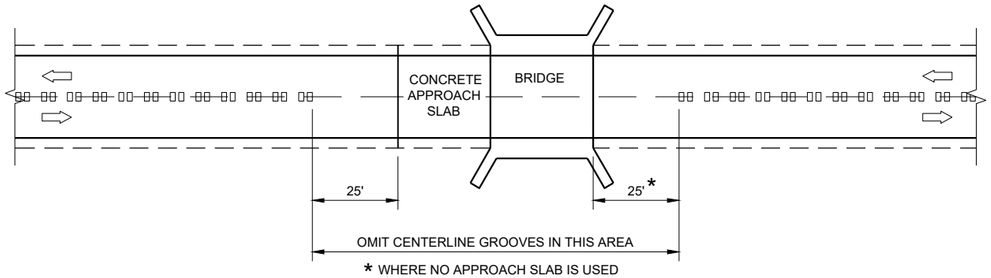
CENTERLINE GROOVES AT INTERSECTIONS (WITH LEFT TURN LANES)



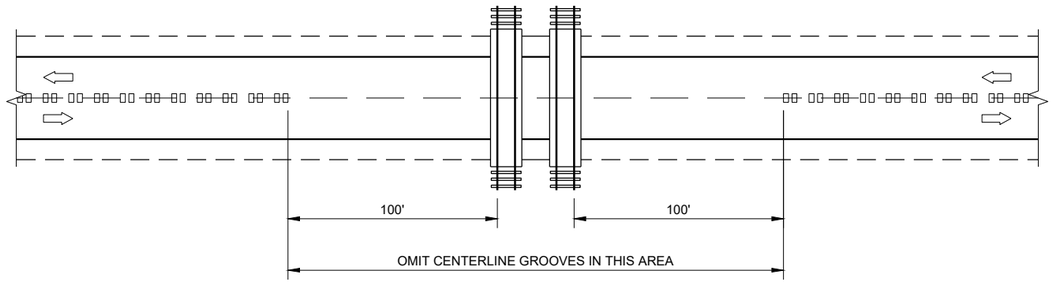
CENTERLINE GROOVES AT DRIVEWAYS (1)

GENERAL NOTES

(1) CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS WHEN DIRECTED BY THE ENGINEER.



CENTERLINE GROOVES AT BRIDGES



CENTERLINE GROOVES AT RAILROADS

CENTER LINE RUMBLE STRIPS - INTERSECTIONS, DRIVEWAYS, BRIDGES, RAIL ROADS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ John Jenkins — ROADWAY STANDARDS DEVE ENGINEER

FHWA

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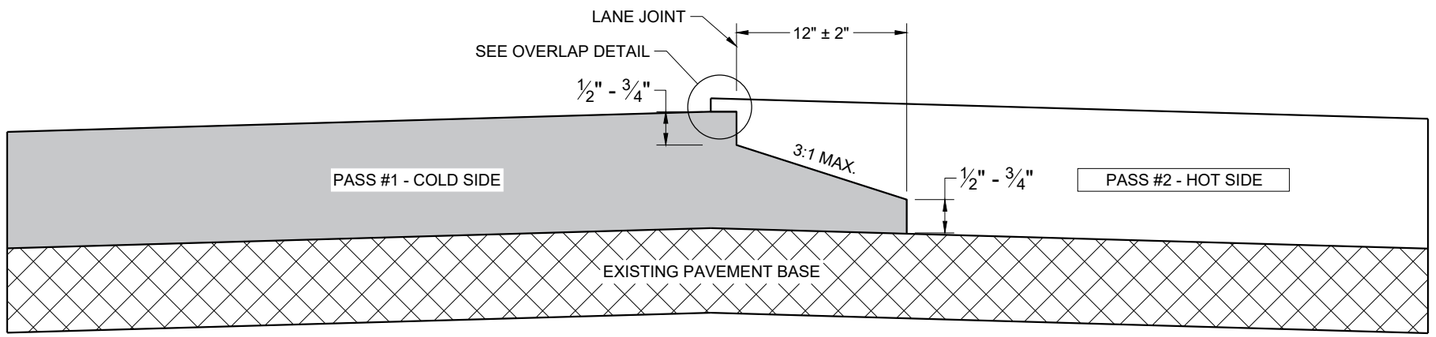
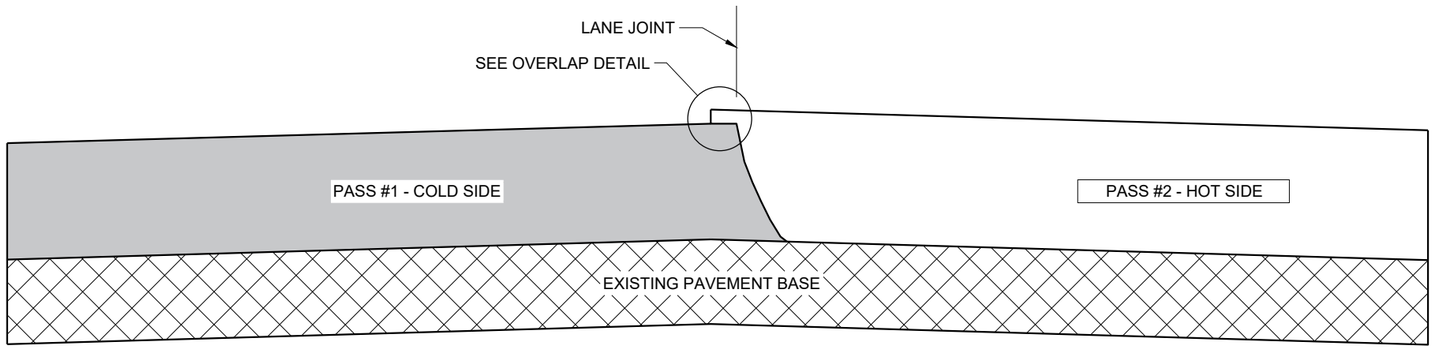
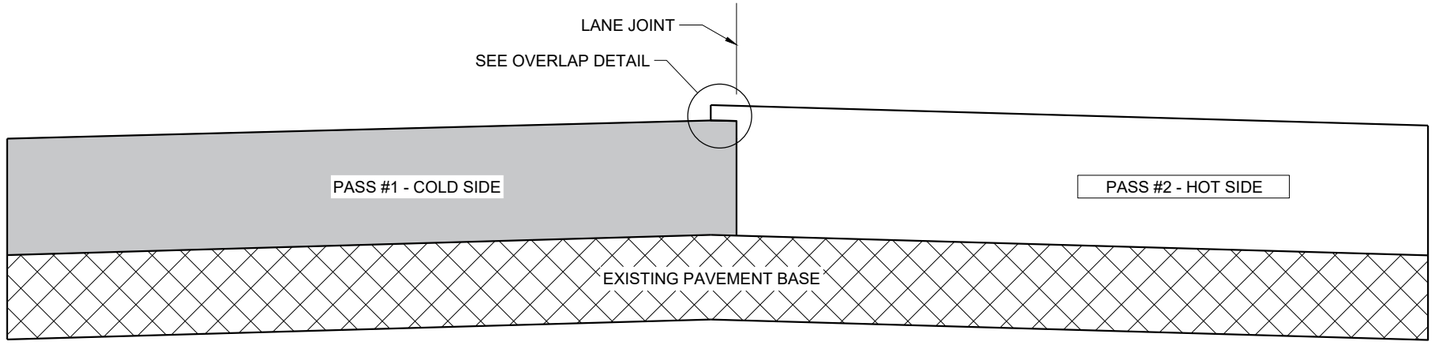
SDD 13A11 - 04d



TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)

GENERAL NOTES

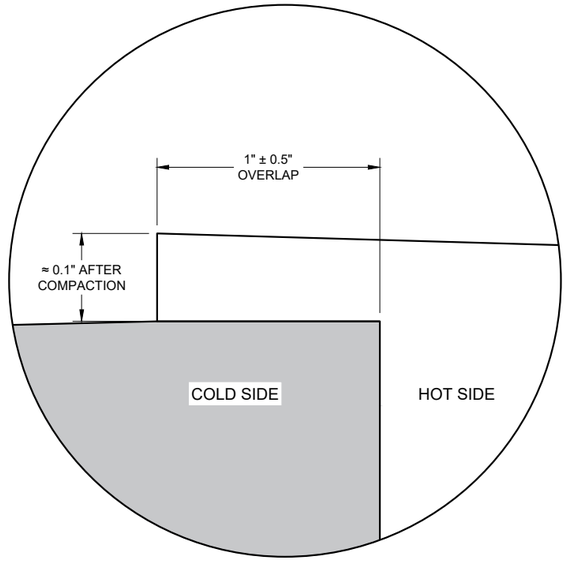
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY 1" ± 0.5" AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

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SDD 13C19 - 03

SDD 13C19 - 03

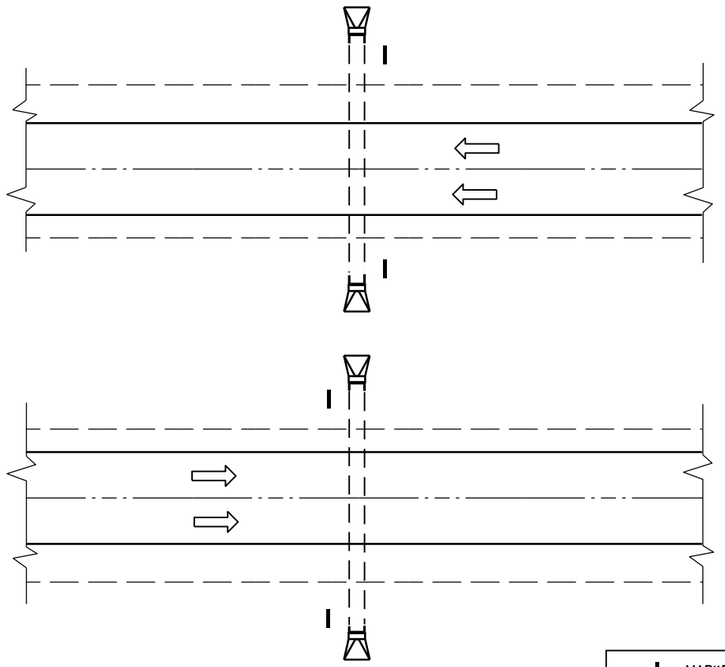
HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2020
DATE

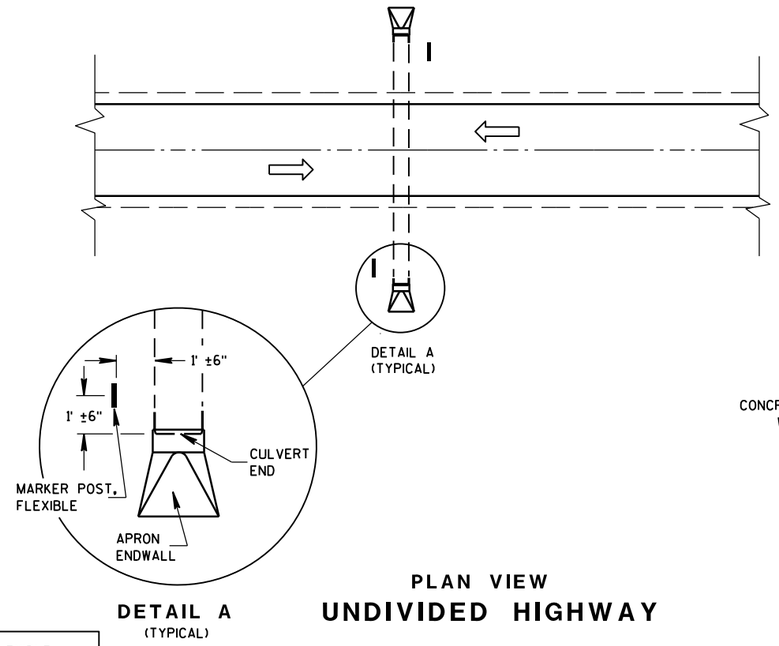
/S/ Steven Hefel
HMA PAVEMENT ENGIN

FHWA



PLAN VIEW

DIVIDED HIGHWAY



DETAIL A
(TYPICAL)

PLAN VIEW

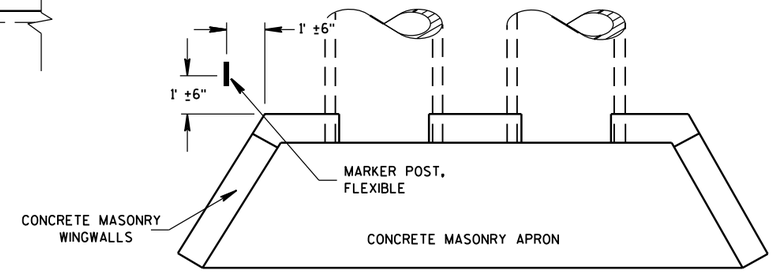
UNDIVIDED HIGHWAY

MARKER POST, FLEXIBLE

DIRECTION OF TRAFFIC FLOW

GENERAL NOTES

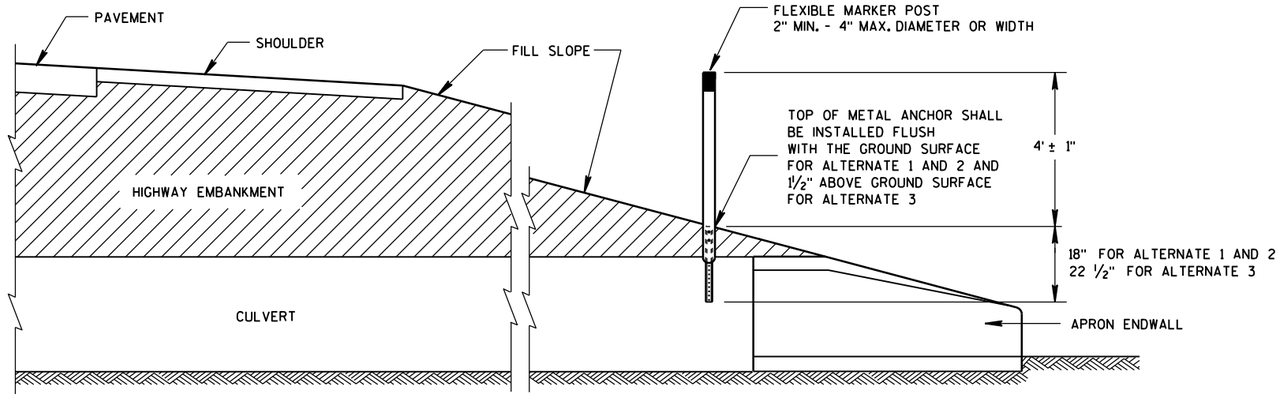
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



PLAN VIEW

CONCRETE MASONRY ENDWALLS FOR CULVERT PIPE AND PIPE ARCH

FLEXIBLE MARKER POST LOCATION



CROSS SECTION

FLEXIBLE MARKER POST

FLEXIBLE MARKER POST
FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

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S.D.D. 15 A 3-2a

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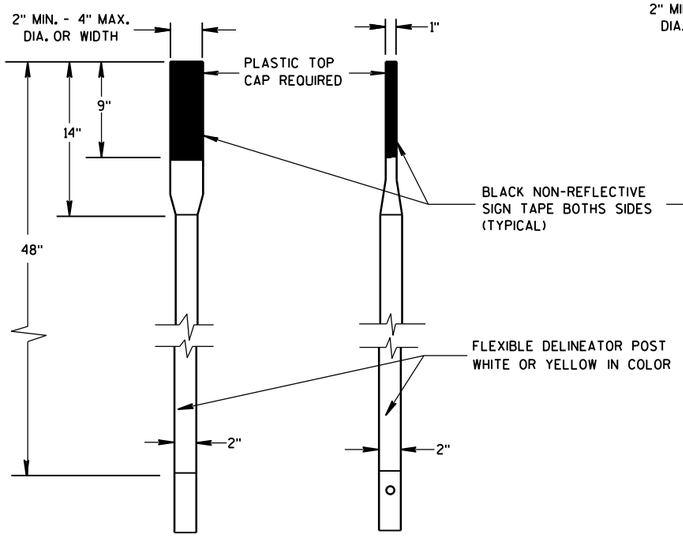
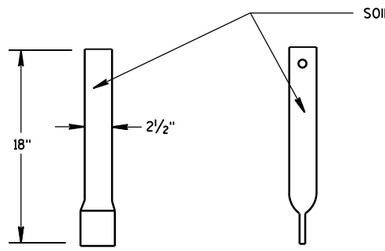
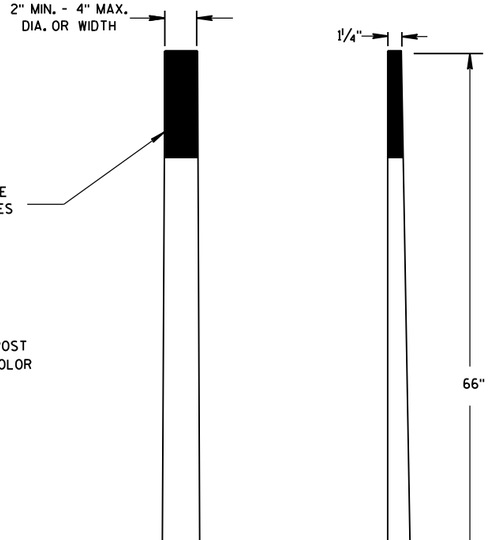
S.D.D. 15 A 3-2a





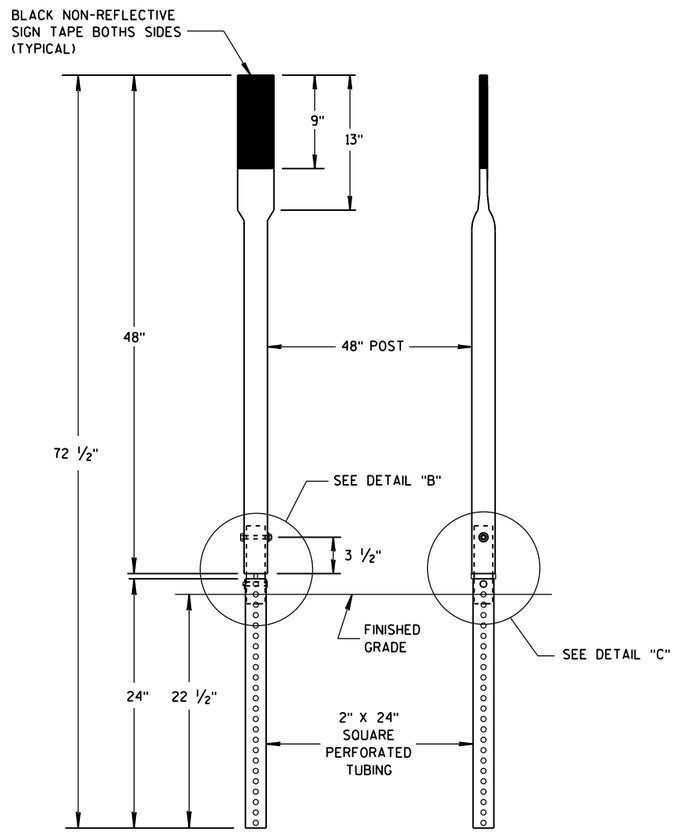
FRONT VIEW SIDE VIEW

ALTERNATE 1



FRONT VIEW SIDE VIEW

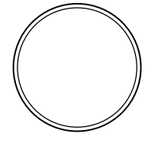
ALTERNATE 2



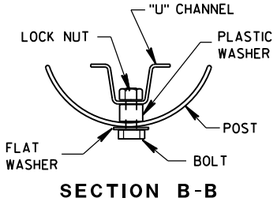
FRONT VIEW SIDE VIEW

ALTERNATE 3

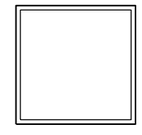
FLEXIBLE MARKER POSTS



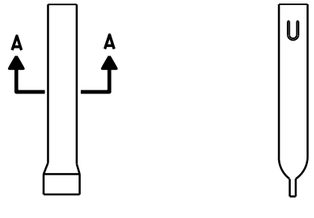
SECTION A-A



SECTION B-B

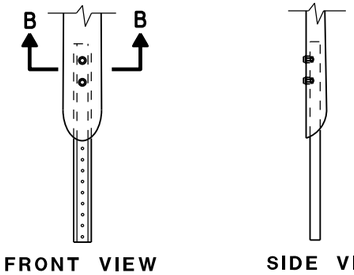


SECTION C-C



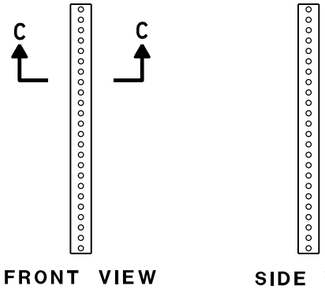
FRONT VIEW SIDE VIEW

ALTERNATE 1



FRONT VIEW SIDE VIEW

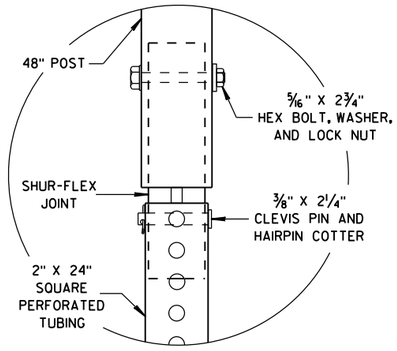
ALTERNATE 2



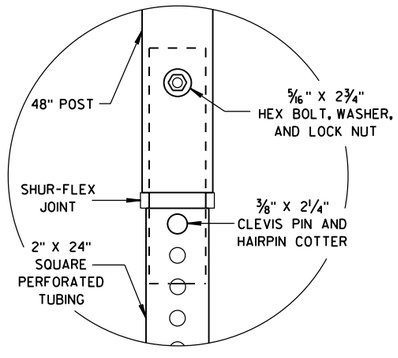
FRONT VIEW SIDE VIEW

ALTERNATE 3

FLEXIBLE MARKER POST ANCHORS



DETAIL B



DETAIL C

FLEXIBLE MARKER POST
FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/1/2012 — DATE
/S/ Travis Feltes — STATE TRAFFIC ENGINEER
FHWA

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S.D.D. 15 A 3-2b

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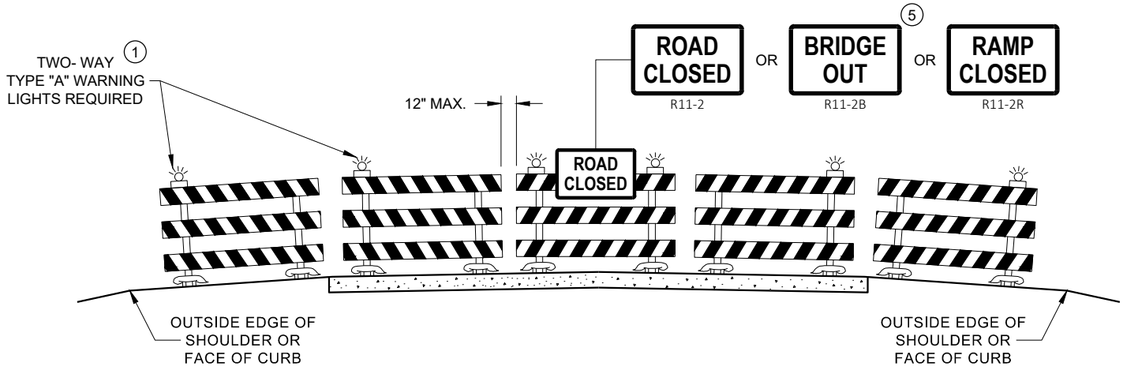
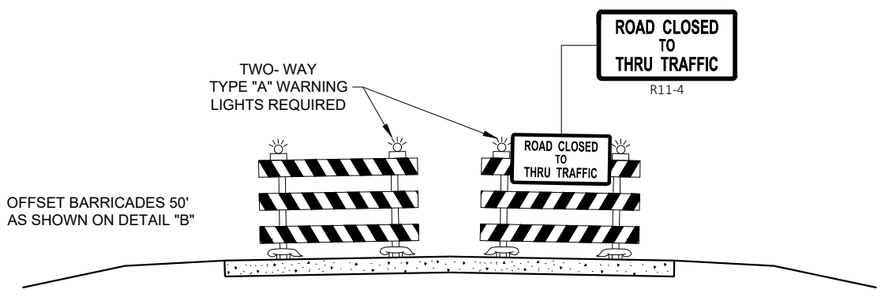
S.D.D. 15 A 3-2b



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60 " X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING.

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR VARIOUS CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Andrew Heidtke — WORK ZONE ENGINEER
FHWA

6

SDD 15C02 - 09b

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SDD 15C02 - 09b

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

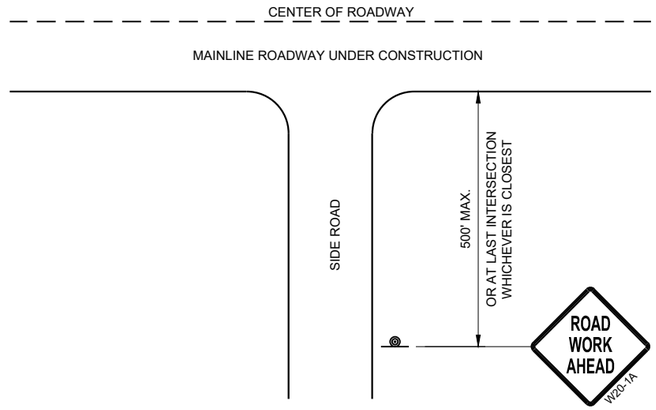
ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS

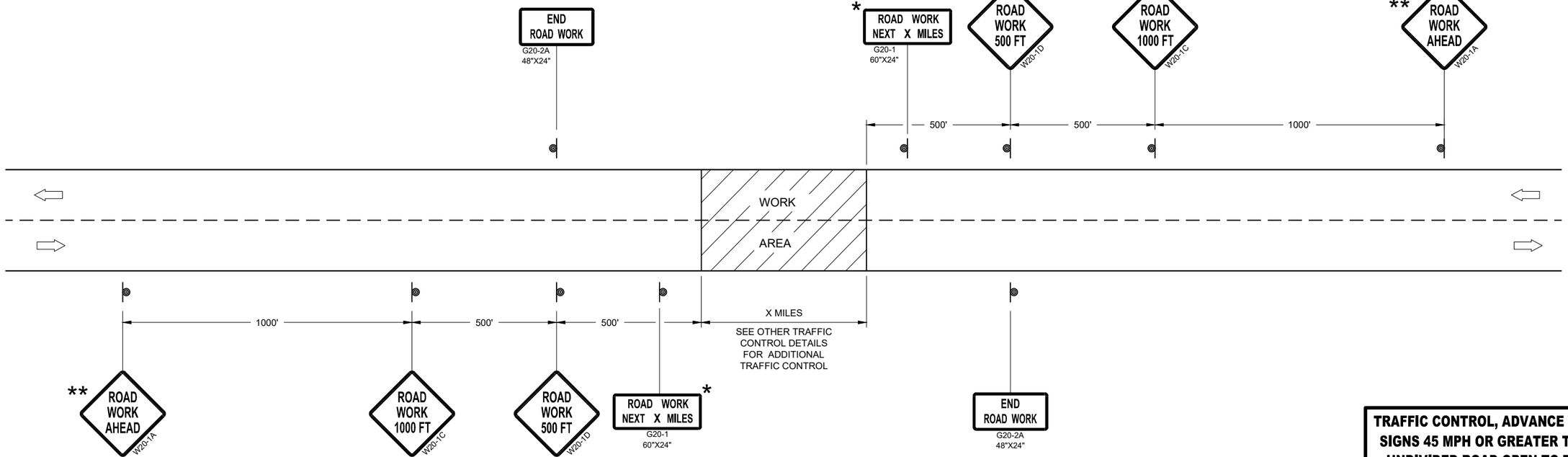
** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.



TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL

LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 MPH OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6

SDD 15C04 - 05

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

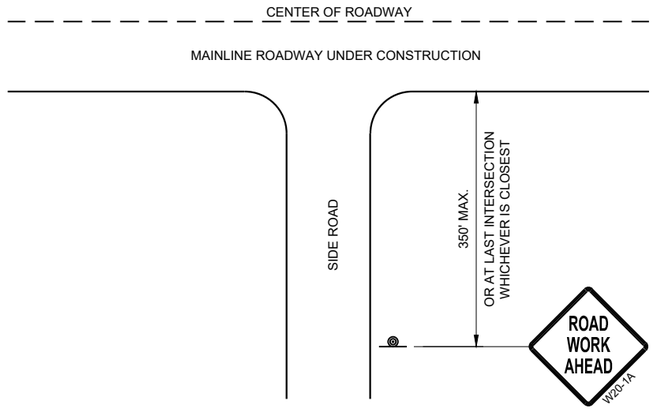
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

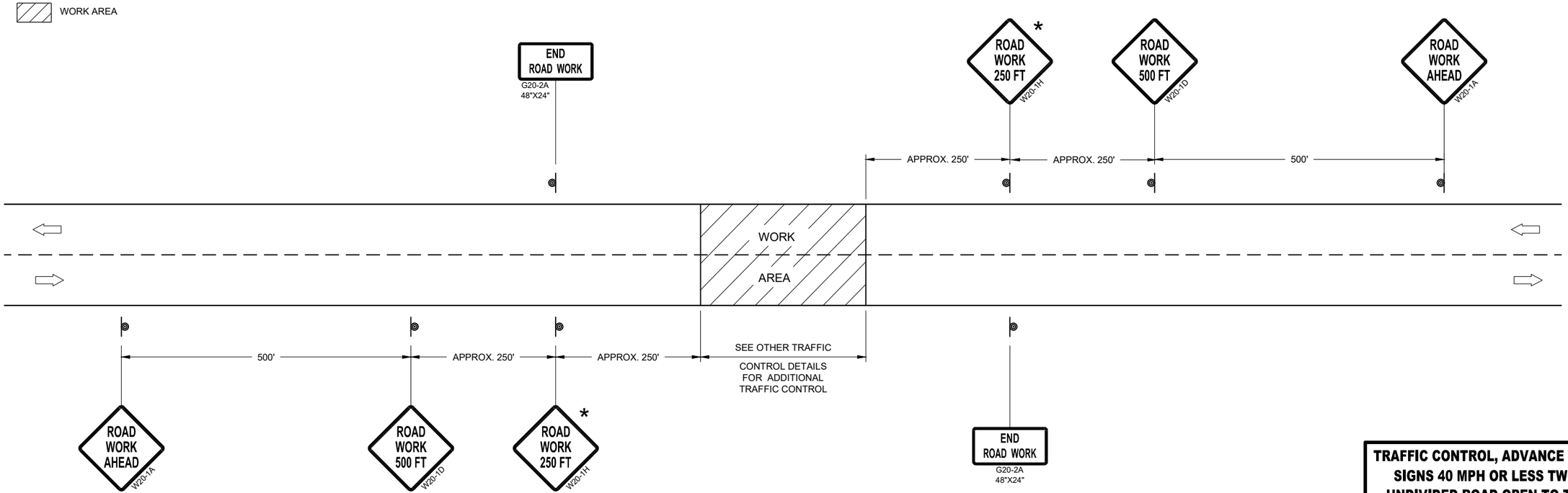
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL

LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 MPH OR LESS TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

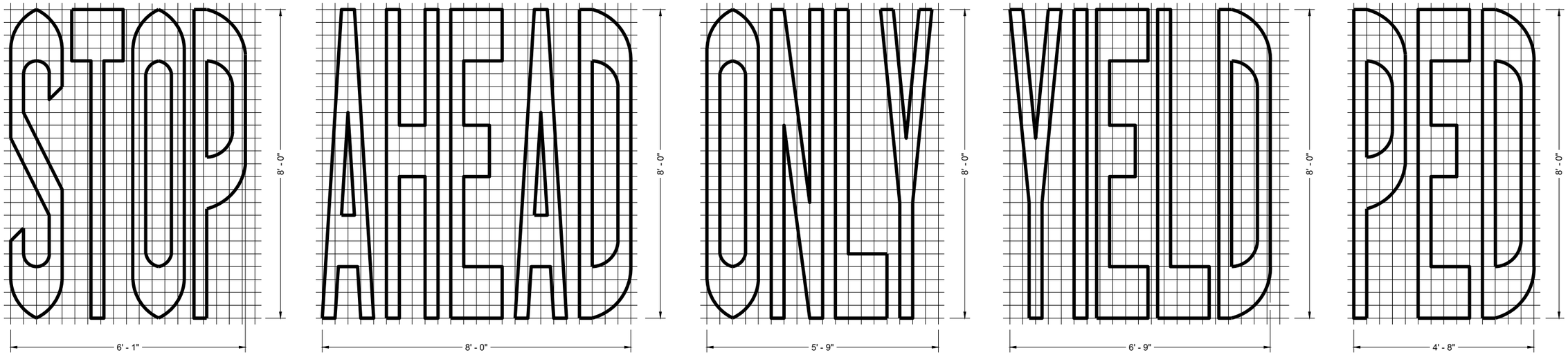
APPROVED
July 2018
DATE

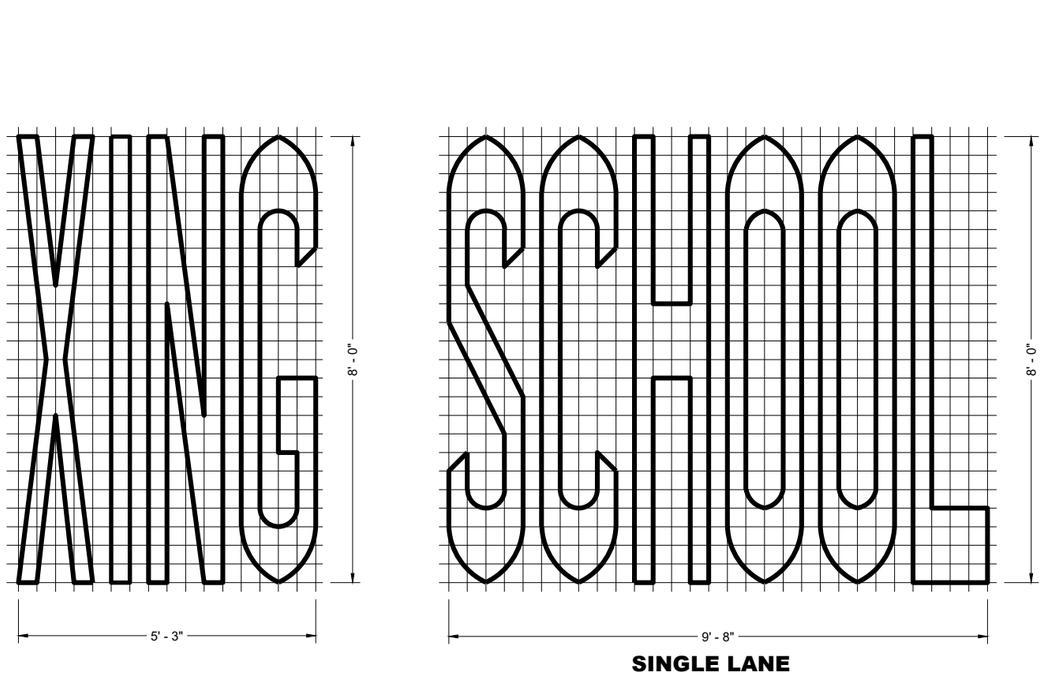
/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

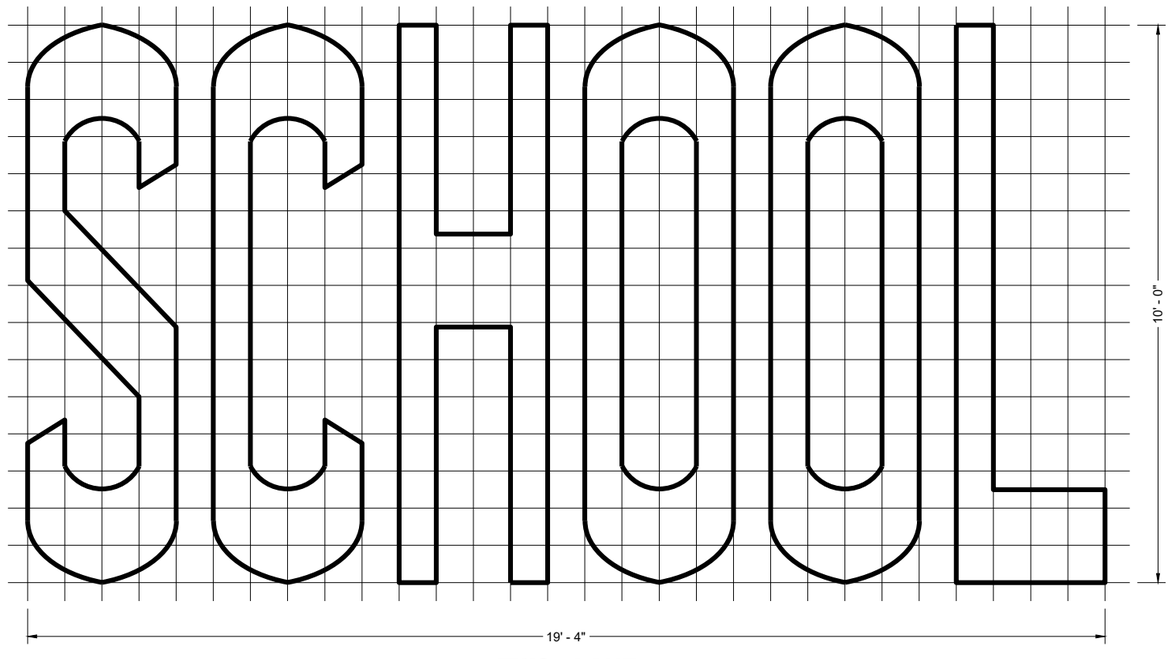
6

SDD 15C05 - 05





SINGLE LANE



TWO - LANE

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING WORDS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2024 — DATE
/S/ Matthew Rauch — STATE SIGNING AND MARKING ENGINEER

FHWA

6

SDD 15C07-16b

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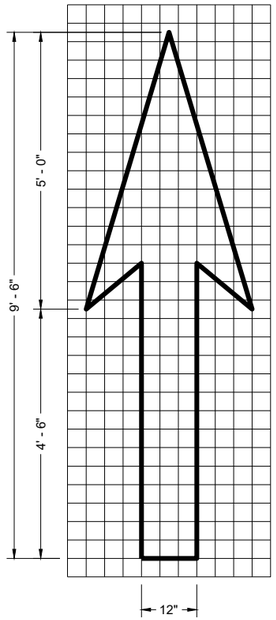
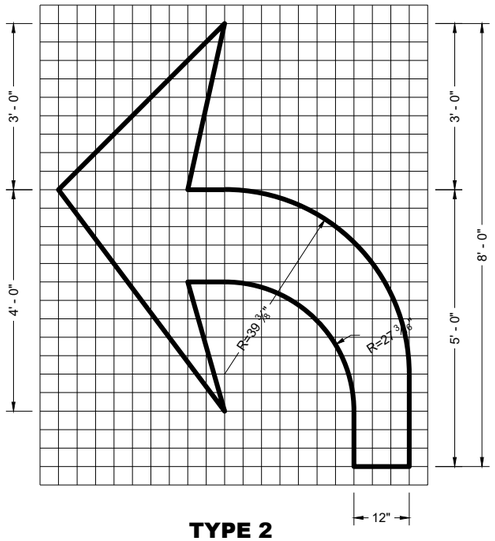
SDD 15C07-16b

GENERAL NOTES

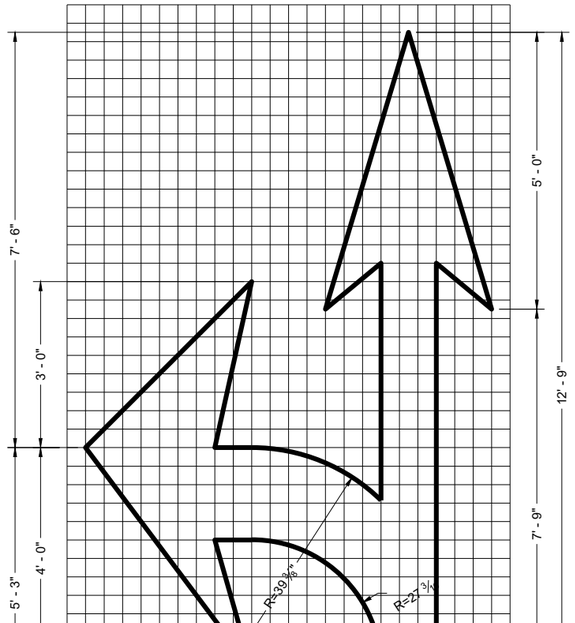
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



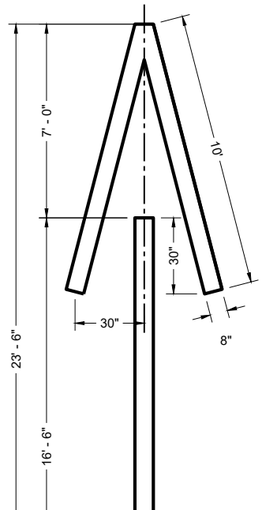
TYPE 1



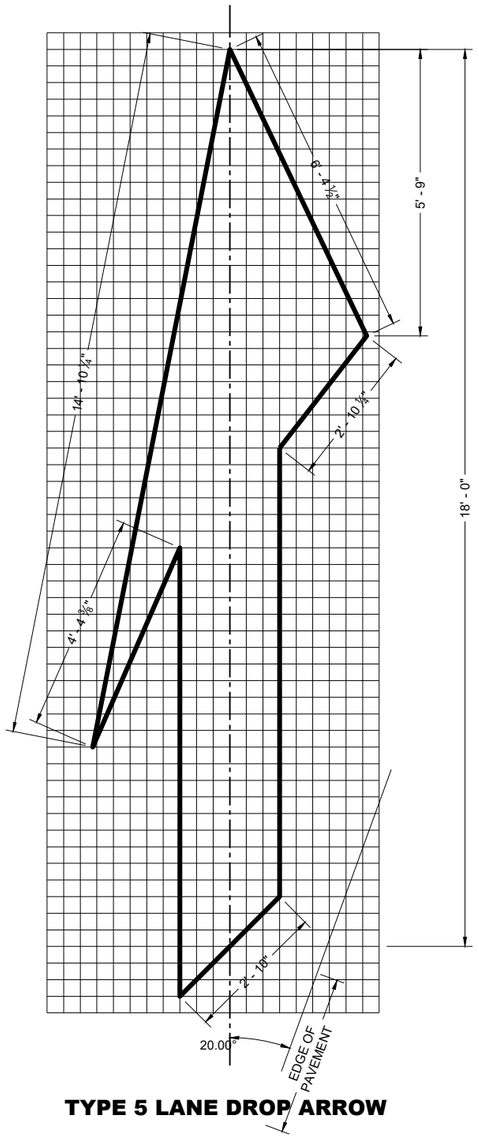
TYPE 2



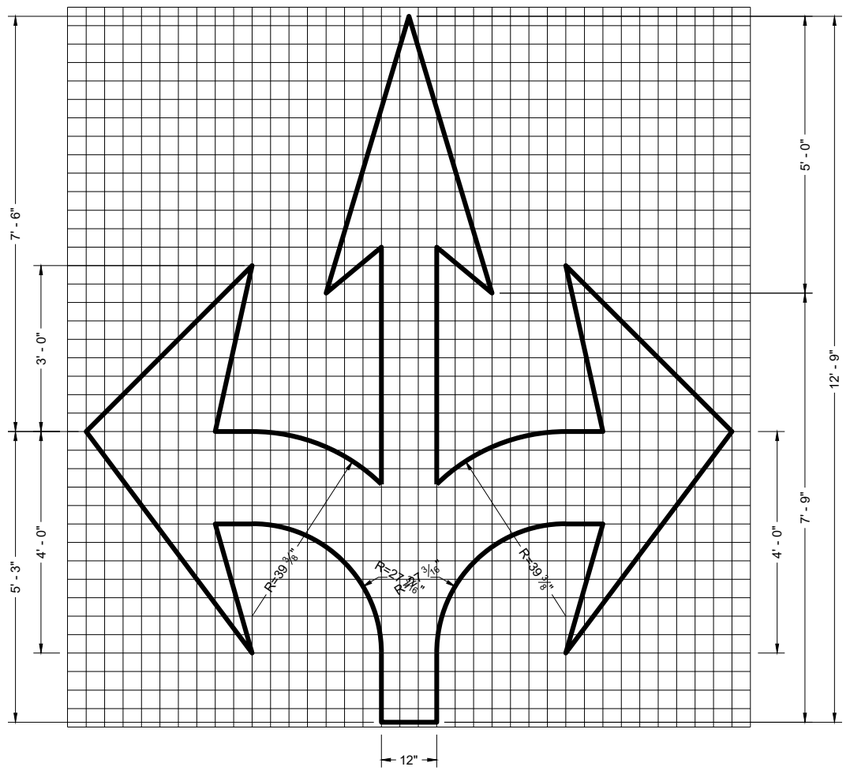
TYPE 3



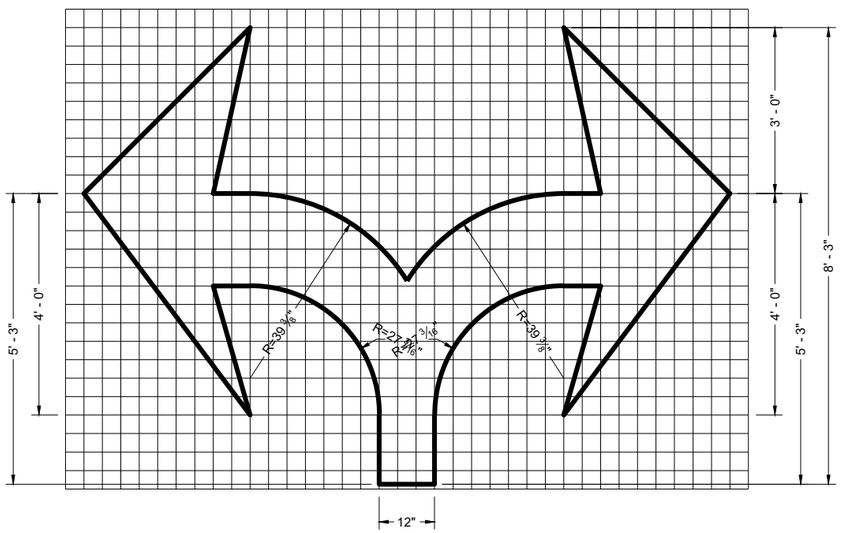
TYPE 4



TYPE 5 LANE DROP ARROW



TYPE 6



TYPE 7

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2024 — DATE
/S/ Matthew Rauch — STATE SIGNING AND MARKING ENGINEER

FHWA

6

SDD 15C07-16c

6

SDD 15C07-16c

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

(1) LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING

(2) MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

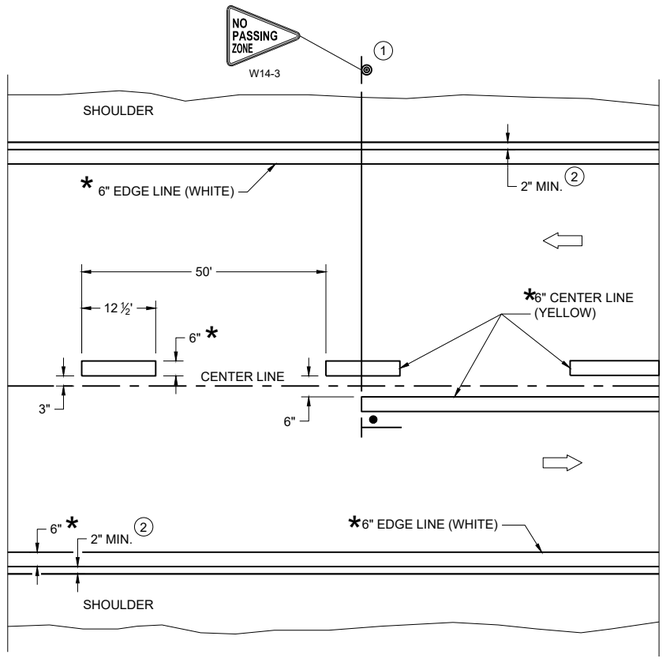
*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

LEGEND

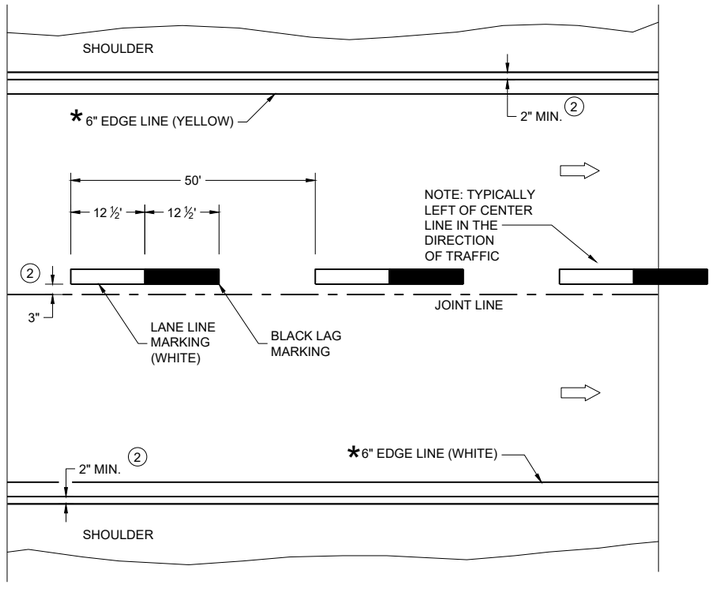
"T" MARKING

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

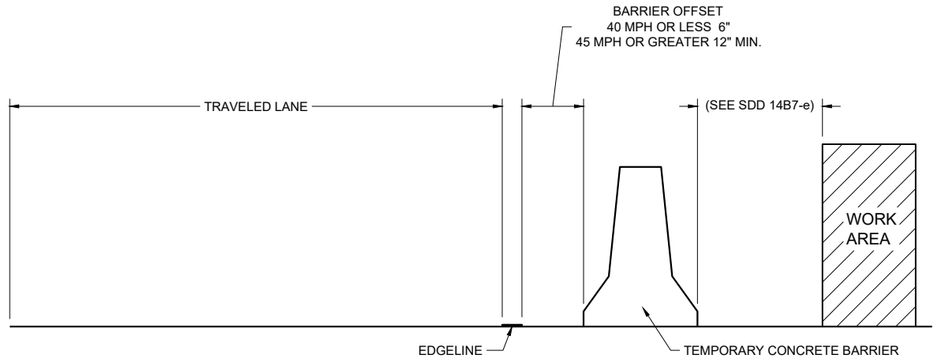
PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Jeannie Silver — Statewide Pavement Marking Engineer
FHWA

6

SDD 15C08-23a



TEMPORARY BARRIER OFFSET FROM EDGELINE

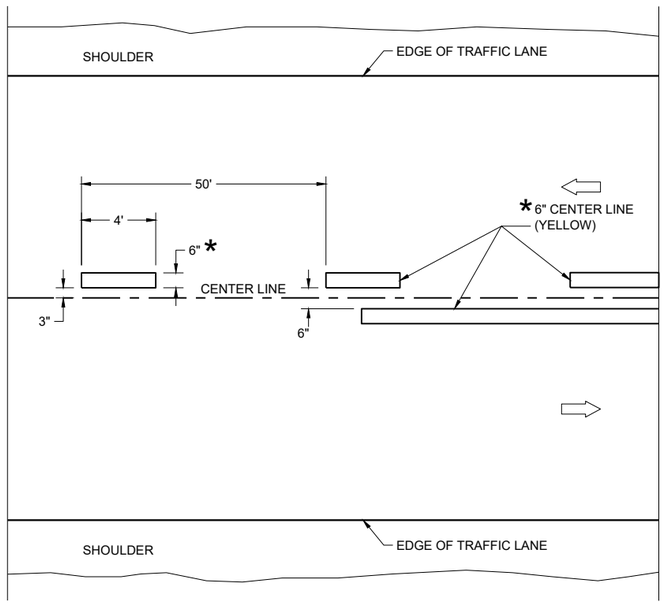
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

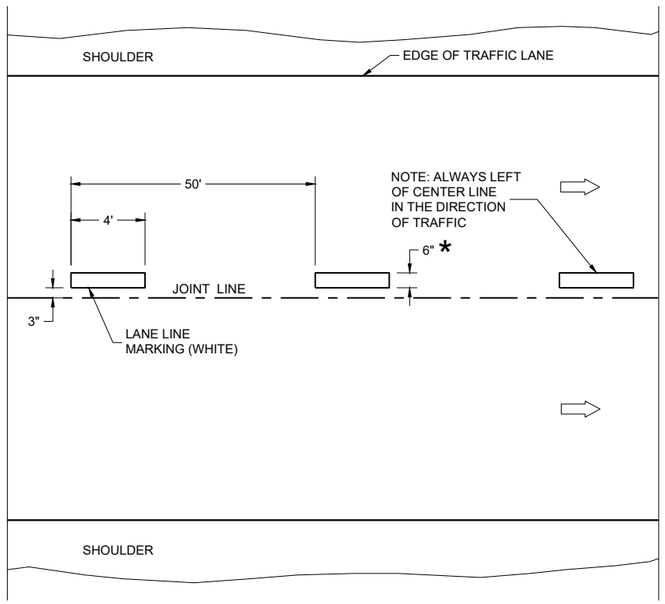
LEGEND

DIRECTION OF TRAFFIC

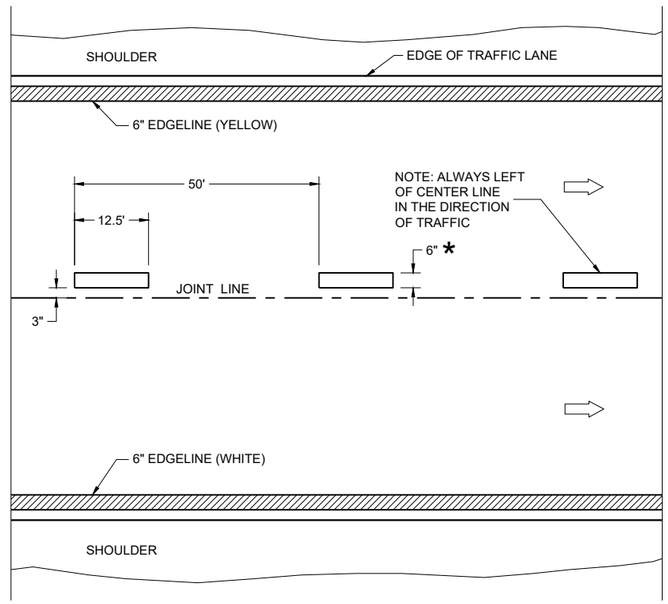
*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Jeannie Silver — Statewide Pavement Marking Engineer

FHWA

6

SDD 15C08-23b

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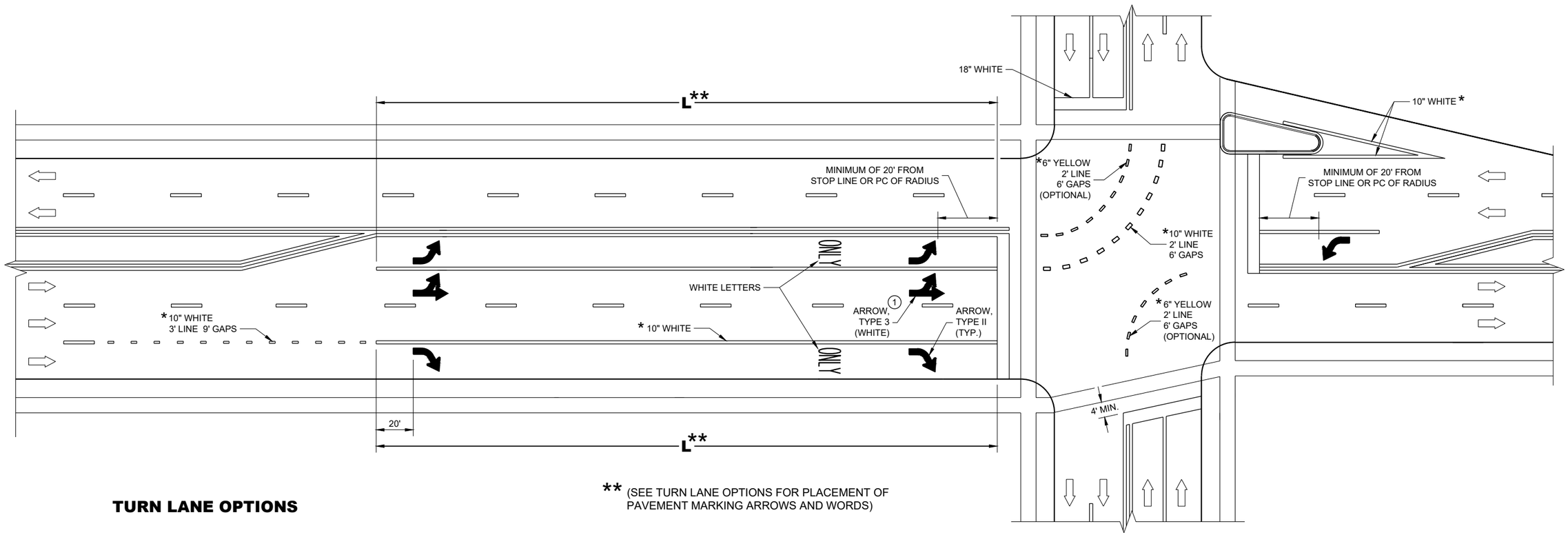
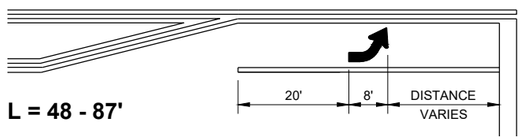
SDD 15C08-23b



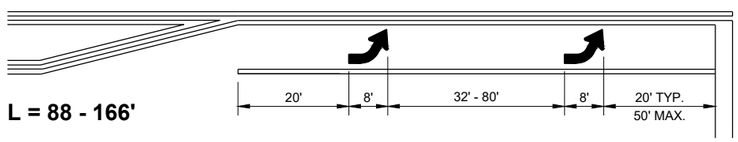
** (SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

TURN LANE OPTIONS

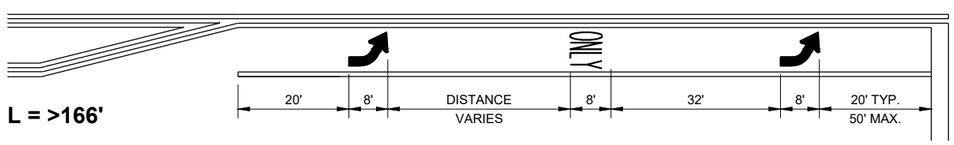
LENGTH OF TURN BAY (**L**) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



L = 48 - 87'



L = 88 - 166'



L = >166'

GENERAL NOTES

(1) QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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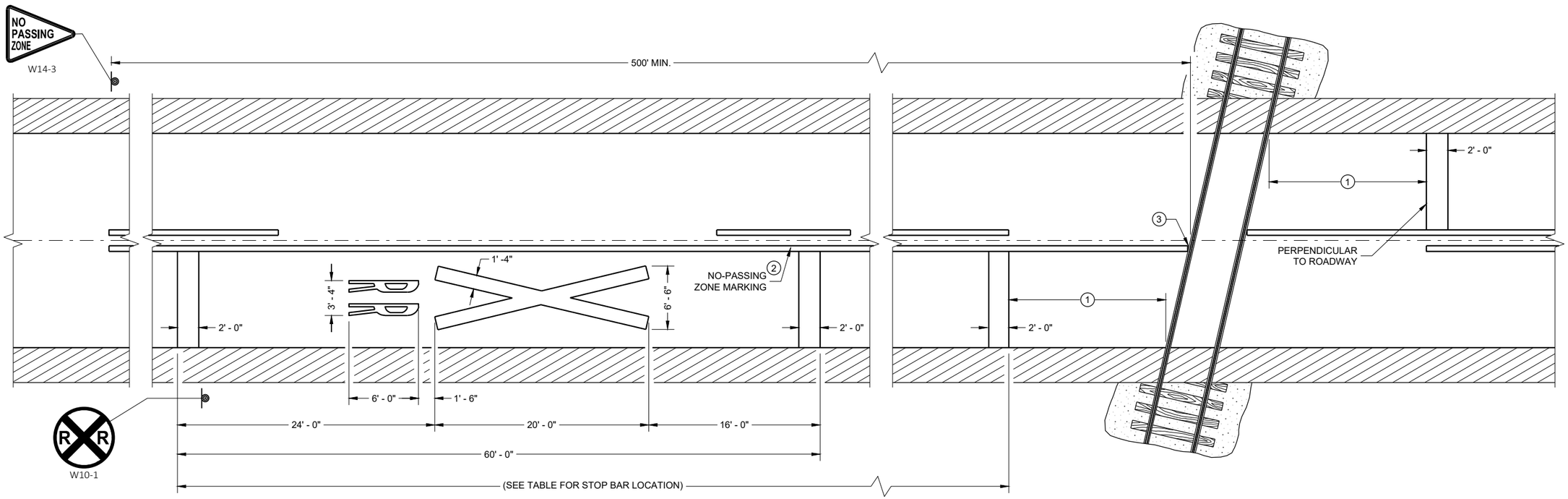
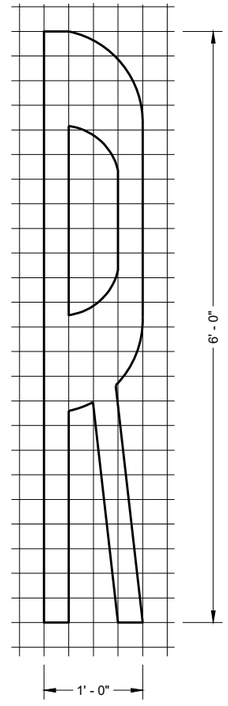
SDD 15C08-23d



PAVEMENT MARKING



LEGEND

SIGN ON PERMANENT SUPPORT

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

ON MULTI-LANE ROADS THE TRANSVERSE BANDS SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.

CENTER OR LANE LINES AND NO-PASSING ZONE MARKINGS SHOWN ON THIS DRAWING ARE REQUIRED AND PAID FOR UNDER OTHER ITEMS IN THE CONTRACT.

TRACE EXISTING SYMBOL WHERE EXISTING SYMBOLS ARE PLACED.

(1) PLACE STOP BAR APPROXIMATELY 8 FEET IN ADVANCE OF THE GATE (IF PRESENT), BUT NO CLOSER THAN 15 FEET IN ADVANCE OF THE NEAREST RAIL. FIELD-FIT STOP BAR TO MAXIMIZE VIEW OF APPROACHING TRAIN.

(2) 500' MINIMUM. MARKING LIMITS MAY BE EXTENDED AS DIRECTED BY THE ENGINEER TO MEET ADJACENT NO-PASSING ZONE MARKINGS.

(3) FOR MULTIPLE TRACK CROSSINGS, THE BARRIER LINE SHALL EXTEND TO THE NEAR RAIL OF THE FURTHEST TRACK IN THE DIRECTION OF HIGHWAY TRAVEL.

DISTANCE TABLE

TABLE BASED UPON 2C-4 WISCONSIN SUPPLEMENT OF MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

POSTED SPEED (M.P.H.)	DIMENSION RANGE (FEET)
25	150* - 250'
30	200* - 300'
35	250* - 450'
40	300* - 500'
45	400* - 650'
50	550* - 800'
55	750* - 1000'
60	1000* - 1250'
65	1000* - 1250'

* THE MINIMUM DISTANCES IN THE TABLE ARE DESIRABLE AND SHOULD BE USED. THE DISTANCES MAY BE INCREASED UP TO THE MAXIMUM TO ALLOW FOR FIELD CONDITIONS SUCH AS THE CLOSED PROXIMITY OF DRIVEWAYS, BRIDGES, SIDE ROADS OR OTHER FEATURES THAT WOULD PROHIBIT THE MINIMUM DISTANCES FROM BEING USED.

SIGNING AND PAVEMENT MARKING DETAILS FOR RAILROAD - HIGHWAY GRADE CROSSINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

May 2023
DATE

/S/ Matthew R. Rauch
STATE SIGNING AND MAR ENGINEER

FHWA

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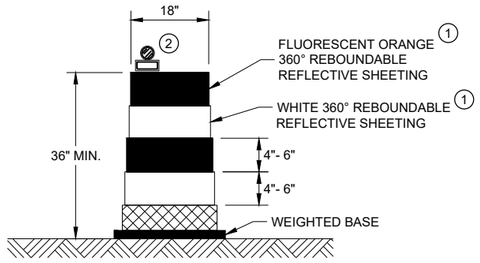
SDD 15C09-13a

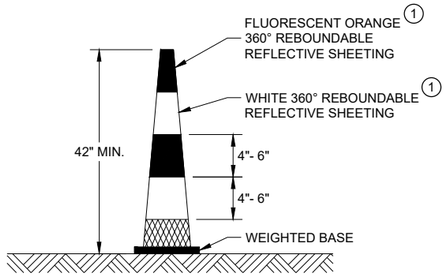
GENERAL NOTES

1. REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
2. LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



DRUM

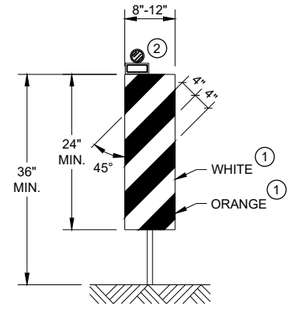
BALLAST WIDTHS
RANGE FROM 24"-36"



42" CONE

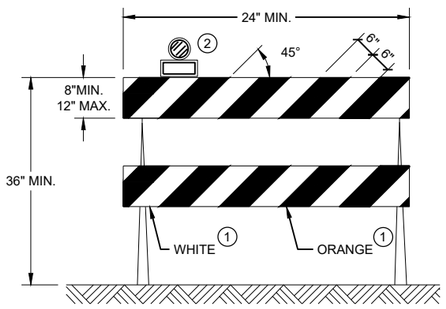
DO NOT USE IN TAPERS
½ SPACING OF DRUMS

BALLAST WIDTHS
RANGE FROM 14"-20"



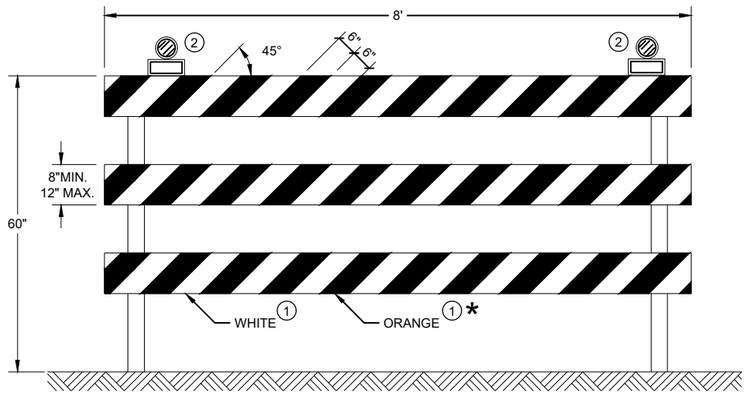
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6

SDD 15C11 - 10b

6

SDD 15C11 - 10b

LEGEND

SIGN ON PORTABLE OR PERMANENT SUPPORT

TEMPORARY PORTABLE RUMBLE STRIP ARRAY

DIRECTION OF TRAFFIC

WORK AREA

FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

(1) FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.

(2) SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

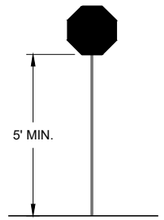
(3) EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



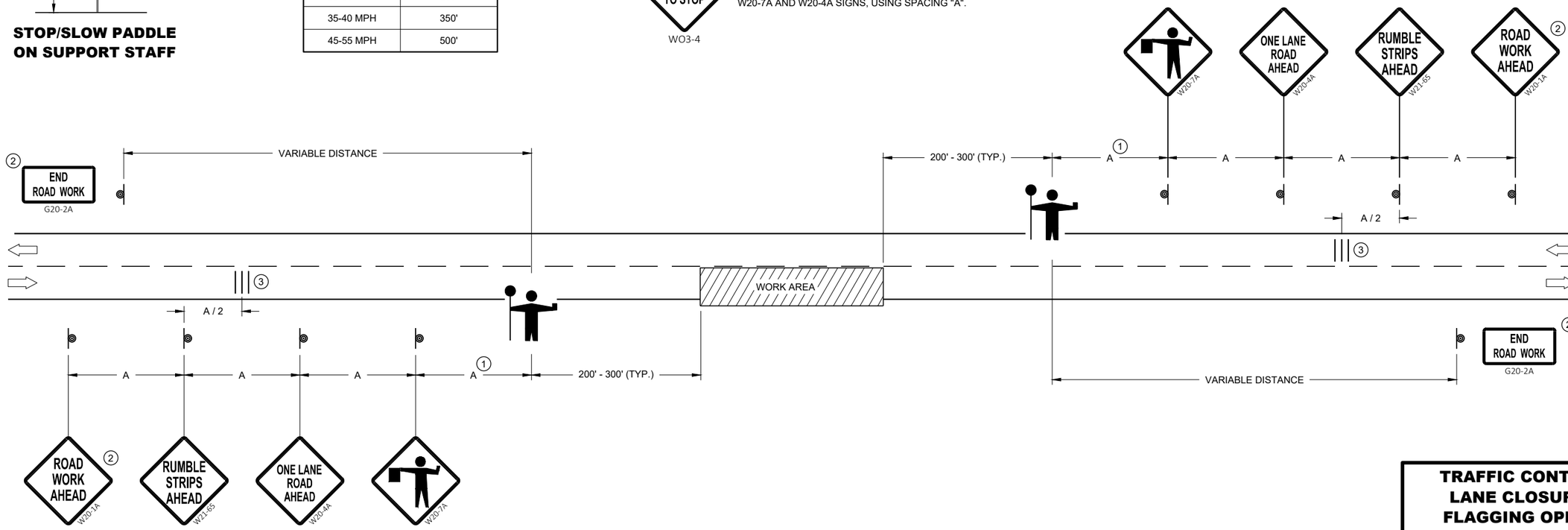
STOP/SLOW PADDLE ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'

WO3-4

USE OF WO3-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

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SDD 15C12 - 09a

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SDD 15C12 - 09a

LEGEND

V1 LEAD VEHICLE

V2 MARKING VEHICLE

V3 SHADOW VEHICLE

TRUCK MOUNTED ATTENUATOR (TMA)

SIGN ON TEMPORARY SUPPORT

DIRECTION OF TRAFFIC

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

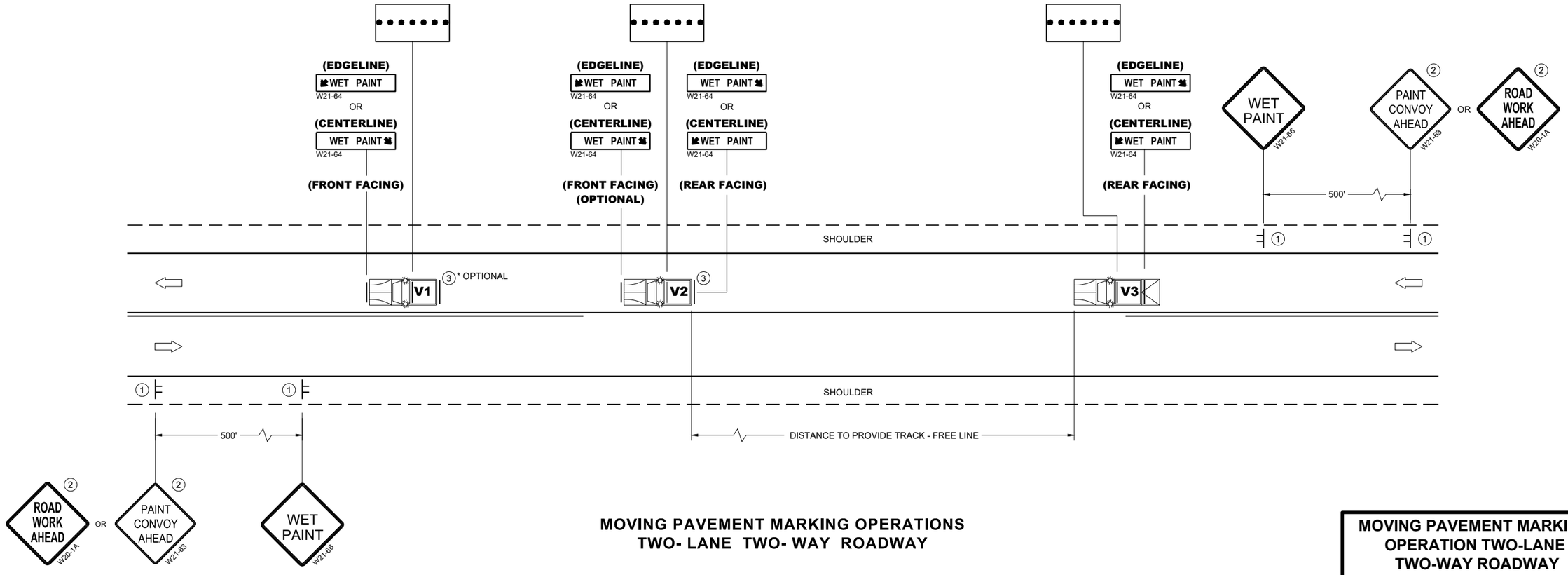
CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING .

(1) SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES AND AFTER EVERY MAJOR INTERSECTION.

(2) IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.

(3) V1 AND V2 CAN BE SWITCHED SO THAT THE MARKER IS THE LEAD VEHICLE.



MOVING PAVEMENT MARKING OPERATIONS
TWO- LANE TWO- WAY ROADWAY

MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2024
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEER
FHWA

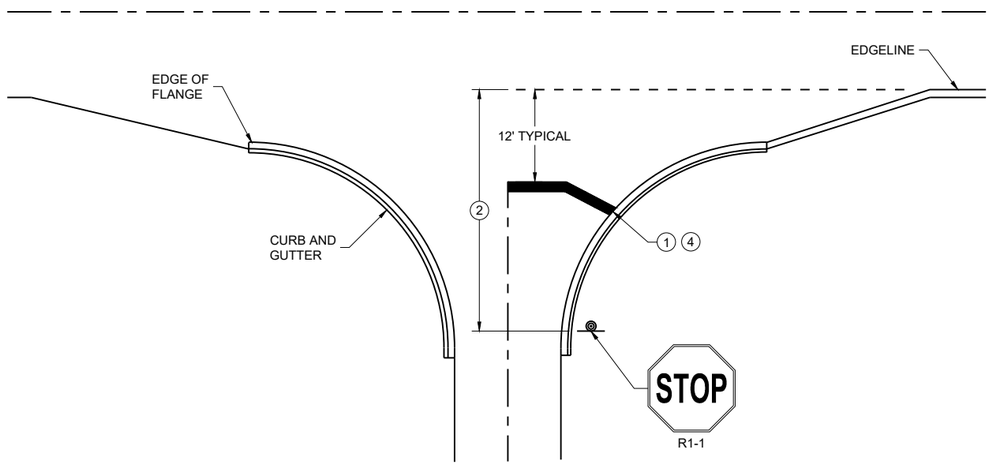
6

SDD 15C19-9a

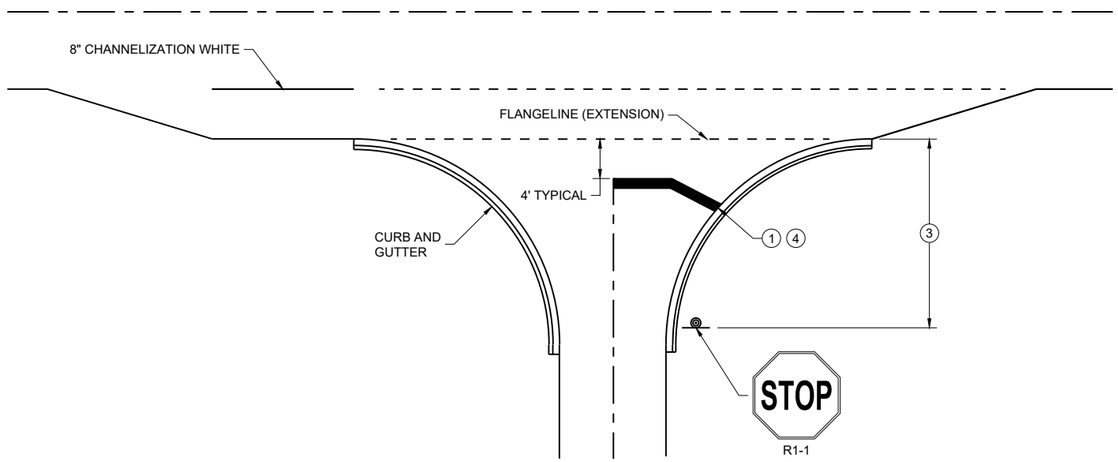
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

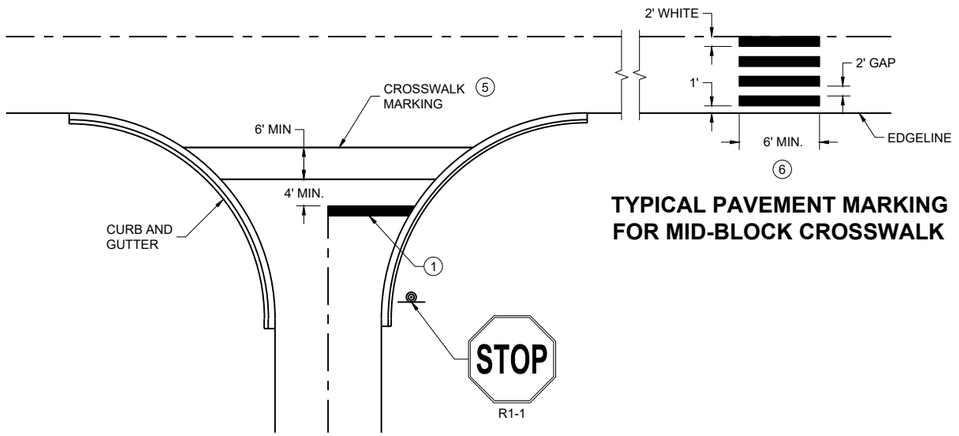
1. 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
2. NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
3. NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGE LINE EXTENSION.
4. MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
5. LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES.
6. POSTED SPEED LIMITS OF 40 MPH OR GREATER USE A MINIMUM WIDTH OF 8' FOR MIDBLOCK CROSSWALKS



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

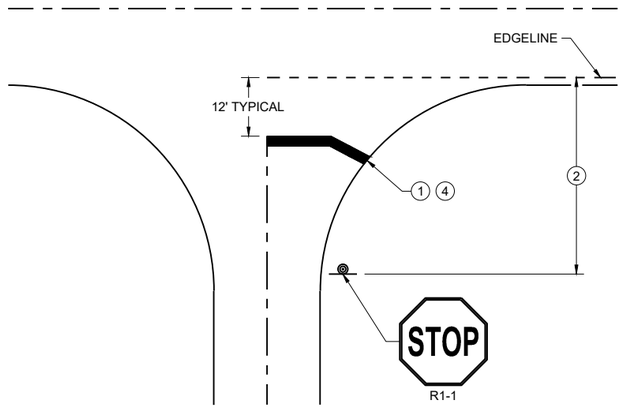


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDE ROADS WITH RIGHT TURN LANE



TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK (5)

TYPICAL STOP LINE PAVEMENT MARKING FOR SIDE ROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

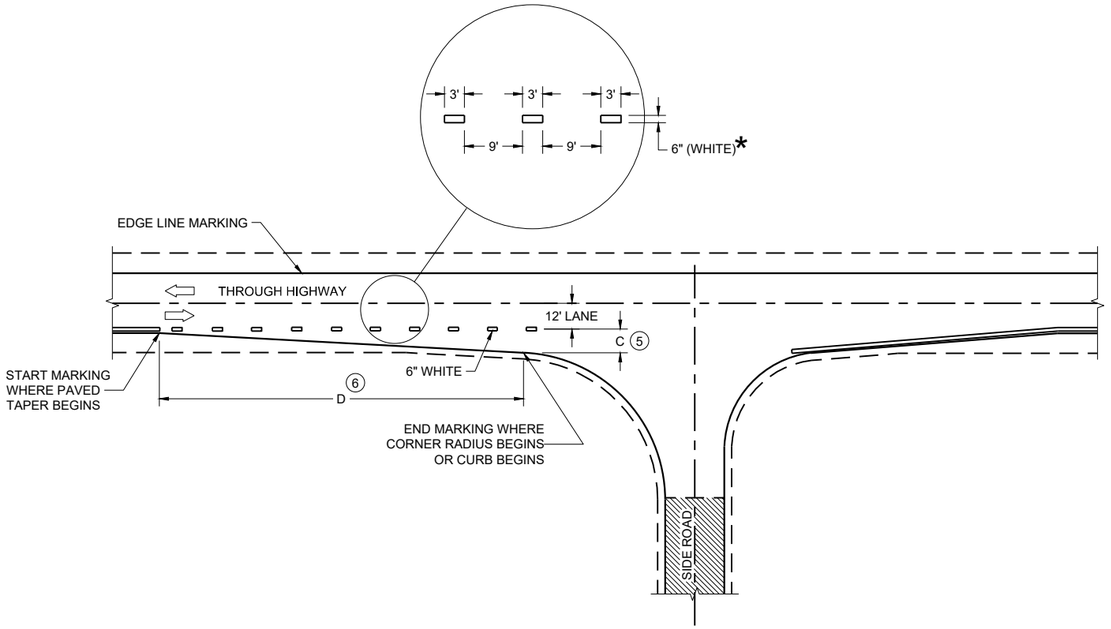
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

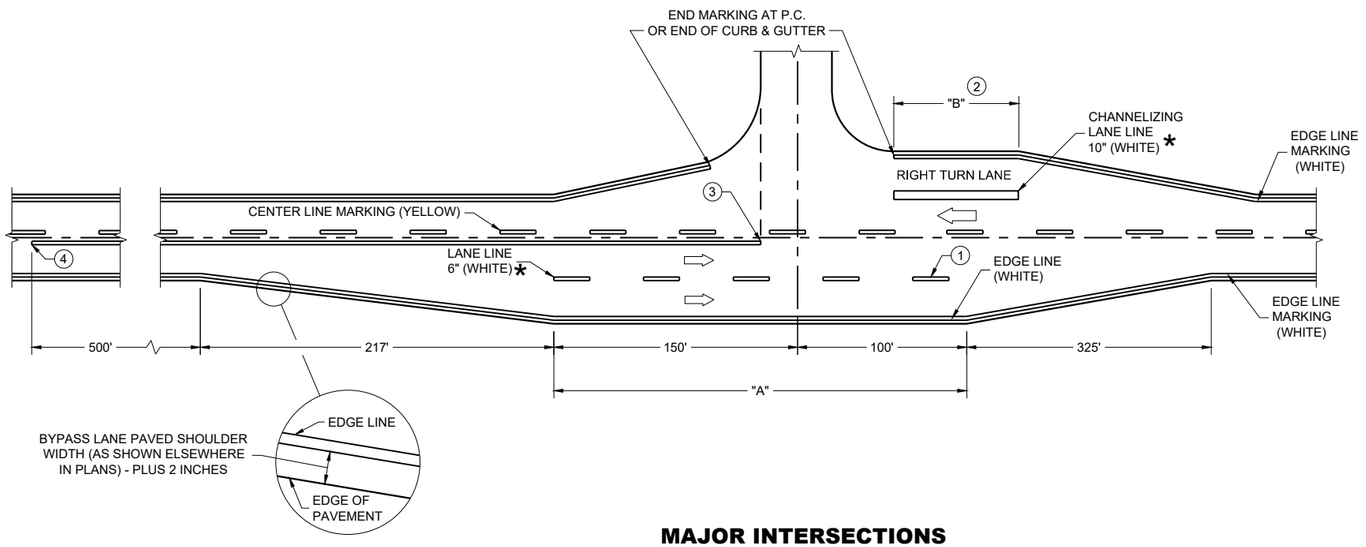
March 2024 — DATE

/S/ Matthew Rauch — STATE SIGNING AND MARKING ENGINEER

FHWA



MINOR INTERSECTION



MAJOR INTERSECTIONS

(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)

*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

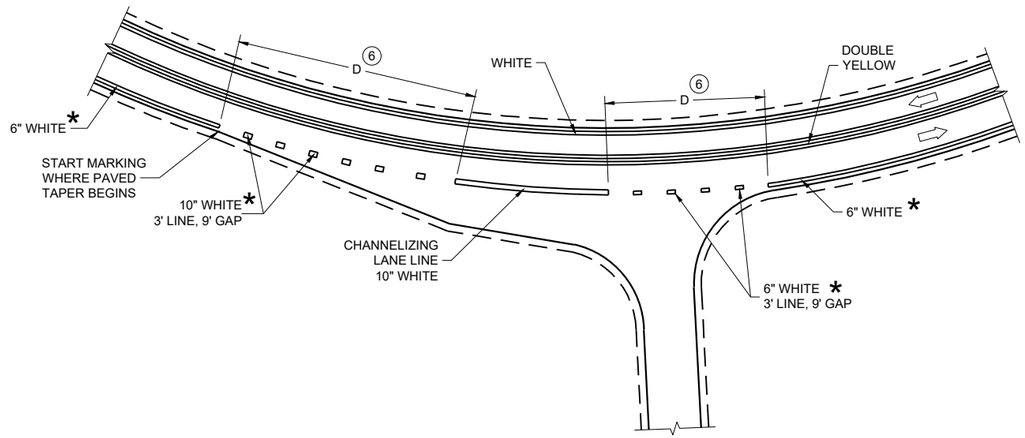
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

1. WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
2. WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
3. BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
4. BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
5. WHEN DISTANCE "C" IS LESS THAN 4 FEET, OMIT DOTTED EXTENSION.
6. WHEN DISTANCE "D" IS LESS THAN 50 FEET, OMIT DOTTED EXTENSION.

LEGEND

DIRECTION OF TRAVEL



INTERSECTION ON OUTSIDE OF CURVE

PAVEMENT MARKING (INTERSECTIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6
SDD 15C35-06a

LEGEND

SIGN ON PERMANENT SUPPORT

TRAFFIC CONTROL DRUM

DIRECTION OF TRAFFIC

WORK ZONE

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

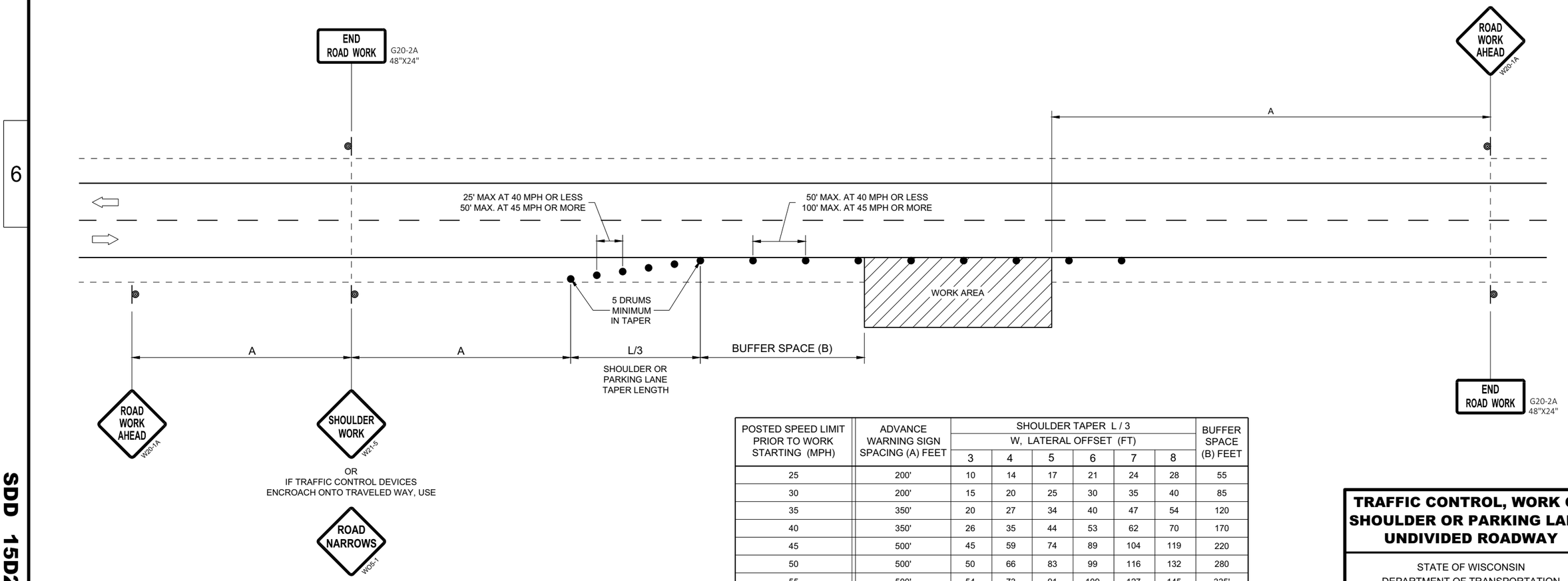
ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHOULDER TAPER L / 3 W, LATERAL OFFSET (FT)						BUFFER SPACE (B) FEET
		3	4	5	6	7	8	
25	200'	10	14	17	21	24	28	55
30	200'	15	20	25	30	35	40	85
35	350'	20	27	34	40	47	54	120
40	350'	26	35	44	53	62	70	170
45	500'	45	59	74	89	104	119	220
50	500'	50	66	83	99	116	132	280
55	500'	54	73	91	109	127	145	335'

TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

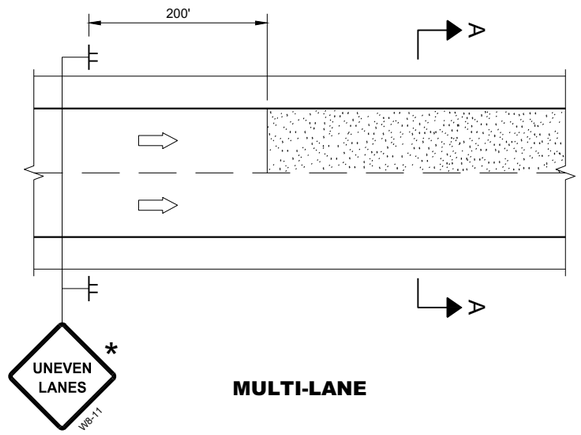
APPROVED
May 2020
DATE

/S/ Andrew Heidtke
STATEWIDE WORK ZONE T SAFETY ENGINEER

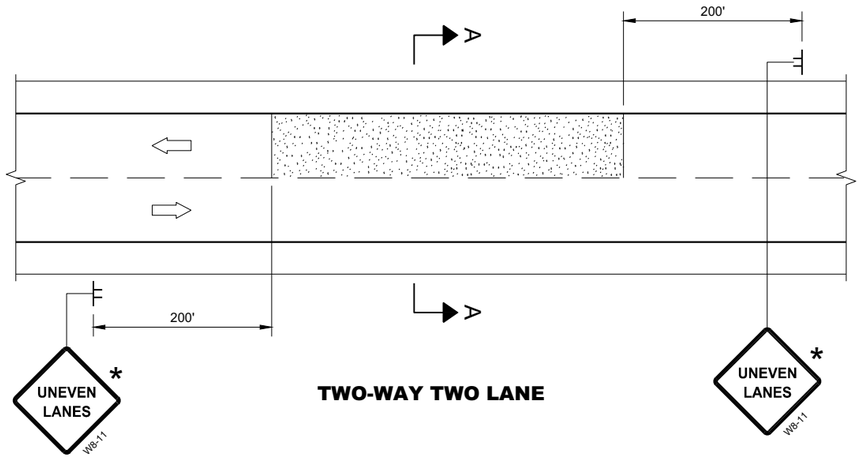
FHWA

SDD 15D28 - 04

SDD 15D28 - 04



MULTI-LANE



TWO-WAY TWO LANE

GENERAL NOTES

FOR SPOT LOCATIONS USE ENGINEERING JUDGEMENT WHEN PLACING ADDITIONAL SIGNS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

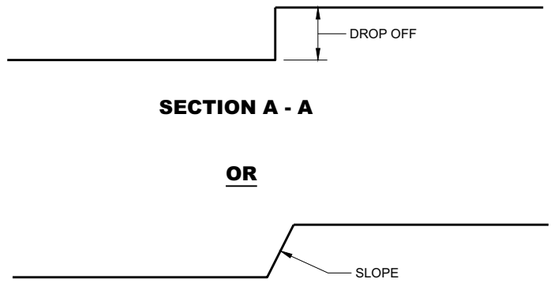
WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

* IF THE DROP-OFF IS CONTINUOUS ALONG THE PROJECT, PLACE ADDITIONAL SIGNS EVERY 1 MILE AND AFTER EVERY ENTRANCE RAMP.

(1) USE CLOSER SPACING WHEN DELINEATING DROP-OFF.

LEGEND

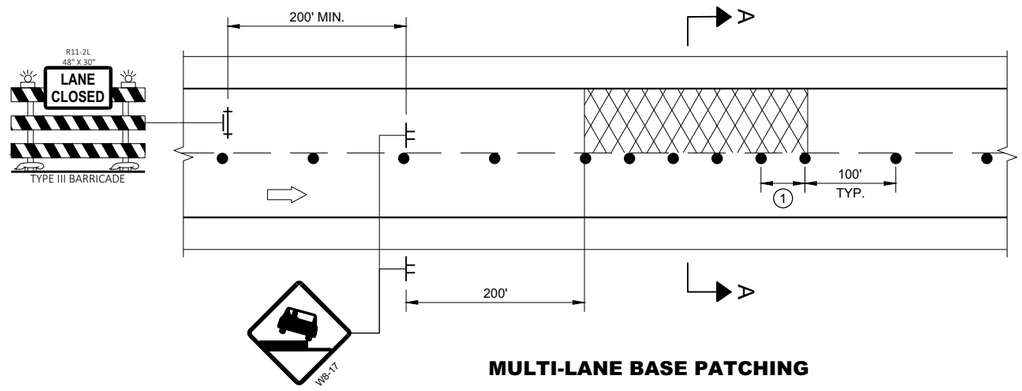
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- WORK AREA WITH DROP-OFF
- MILLED SURFACE



SECTION A - A

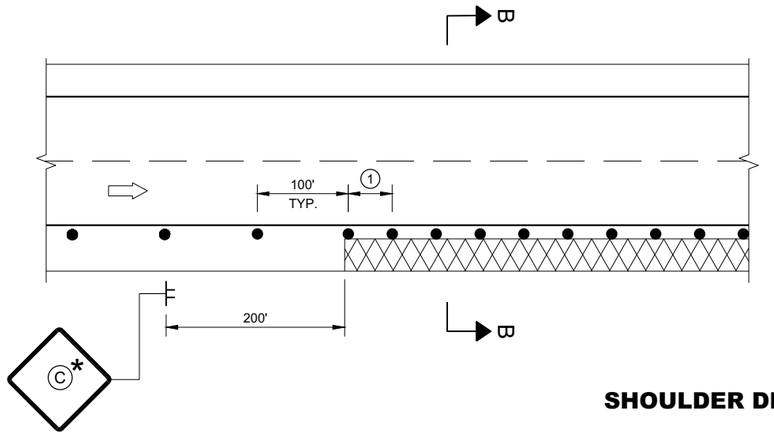
OR

SECTION A - A

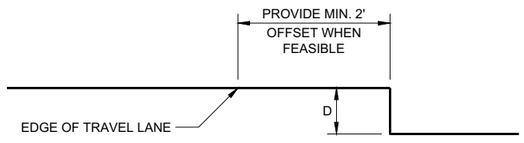


MULTI-LANE BASE PATCHING

ADJACENT LANE DROP-OFFS



SHOULDER DROP-OFFS



SECTION B - B

D	SIGN (C)
< 2" WITH A SLOPE STEEPER THAN 3:1	LOW SHOULDER WO8-9
2" < 6" WITH A SLOPE STEEPER THAN 3:1	SHOULDER DROP - OFF W8-9A PROVIDE A 3:1 OR FLATTER SLOPE OF MATERIAL ADJACENT TO THE PAVEMENT

TRAFFIC CONTROL, DROP-OFF SIGNING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

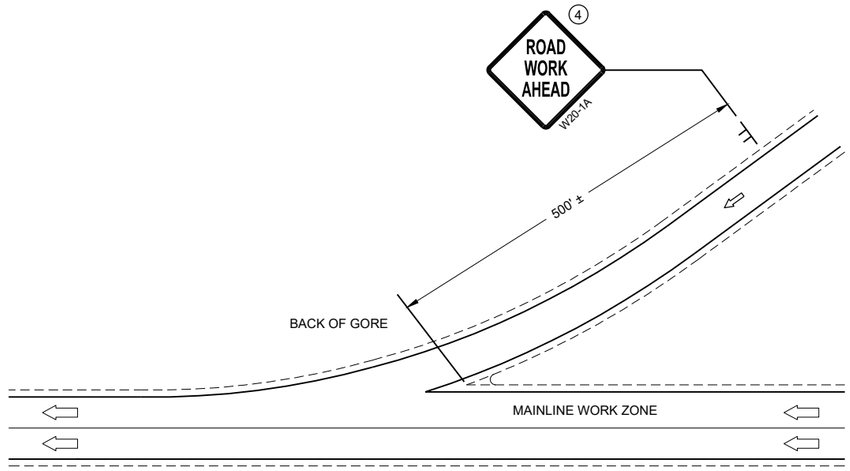
FHWA

6

SDD 15D39 - 02

LEGEND

- V1 SHADOW VEHICLE 1
- V2 SHADOW VEHICLE 2
- V3 ADVANCE WARNING TRUCK
- ● TRAFFIC CONTROL DRUM
- TRUCK MOUNTED ATTENUATOR (TMA)
- SIGN ON TEMPORARY SUPPORT
- DIRECTION OF TRAFFIC
- FLASHING ARROW PANEL (MERGE)
- FLASHING ARROW PANEL (CAUTION)
- PCMS PORTABLE CHANGEABLE MESSAGE SIGN
- WORK AREA



GENERAL NOTES

SHORT DURATION IS WORK THAT OCCUPIES A LOCATION UP TO 1 HOUR.

MOBILE IS WORK THAT MOVES INTERMITTENTLY OR CONTINUOUSLY.

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

WHEN WORK ACTIVITY BLOCKS THE RIGHT LANE, REVERSE TRAFFIC CONTROL.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.

USE DOUBLE ARROWS WHEN CONVOY IS IN CENTER LANE ONLY.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC

(1) DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

(2) DRUMS ARE TO BE USED FOR BRIDGE DECK SEALING AND OTHER PROJECTS THAT REQUIRE DELINEATION.

(3) WITHIN 5 MILES, RELOCATE SIGNS AS WORK PROGRESSES AND NECESSARY OR AS DIRECTED BY THE ENGINEER.

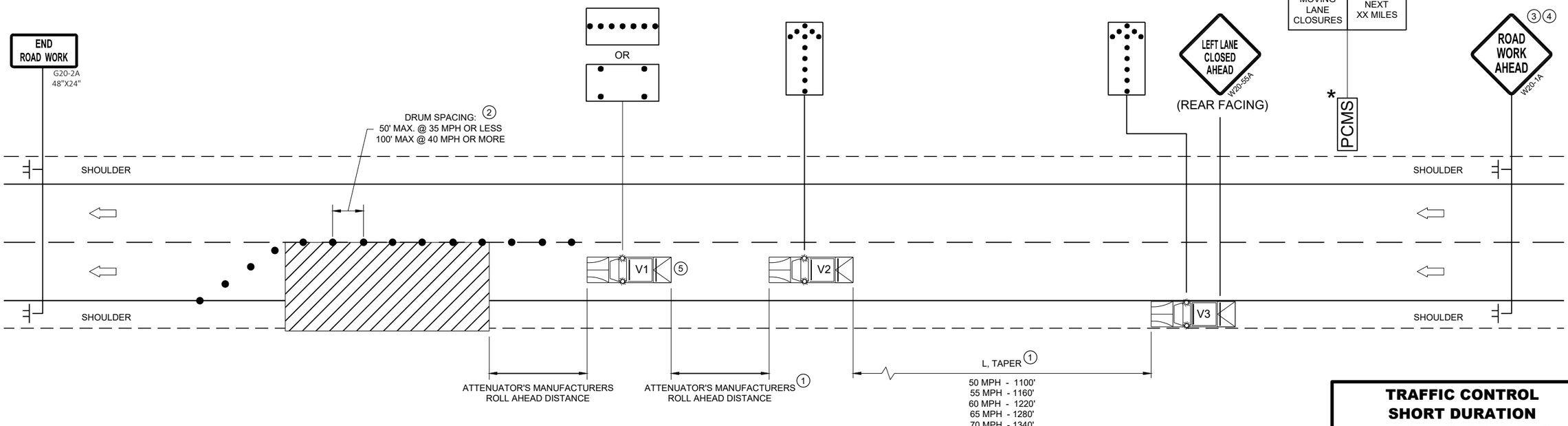
(4) SIGN NOT REQUIRED IF MOVING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

(5) SHADOW VEHICLE 1 (V1) IS OPTIONAL

* PCMS OPTIONAL

PCMS MESSAGING

FRAME 1	FRAME 2
MOVING LANE CLOSURES	NEXT XX MILES



TRAFFIC CONTROL SHORT DURATION MOBILE OPERATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
August 2021
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6

SDD 15D43 - 02

6

SDD 15D43 - 02

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

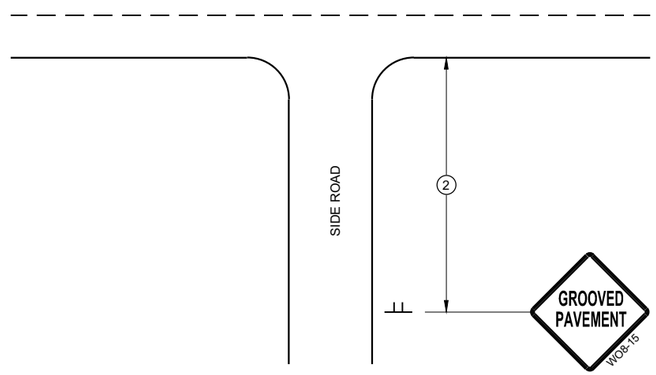
(1) PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.

(2) PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

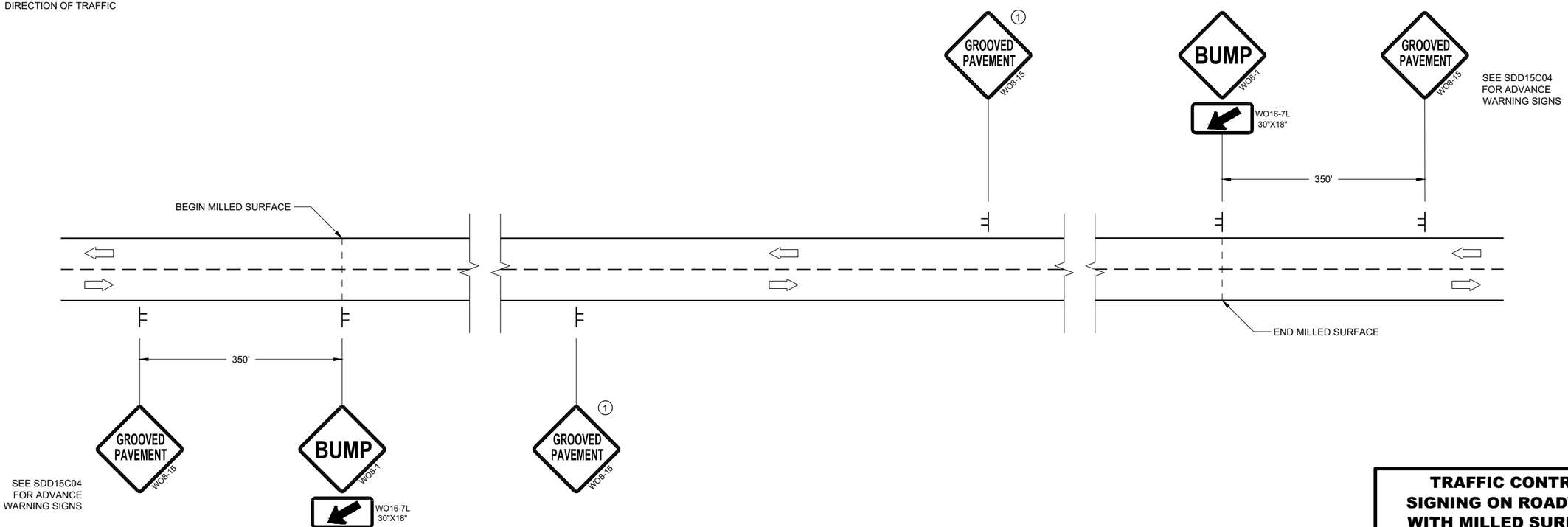
LEGEND

SIGN ON TEMPORARY SUPPORT

DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH SIGN DETAIL



DETAIL FOR SIGNING ON MILLED SURFACES

TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

6
SDD 15D44 - 02

LEGEND

V1 WORK VEHICLE

V2 SHADOW VEHICLE

TRUCK MOUNTED ATTENUATOR (TMA)

FLASHING ARROW PANEL (CAUTION)

WORK AREA

DIRECTION OF TRAFFIC

POSTED SPEED PRIOR TO WORK STARTING (MPH)	DECISION SIGHT DISTANCE (D)
0 - 25	550'
30	550'
35	700'
40	700'
45	900'
50	900'
55	1200'

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

MOBILE IS WORK THAT MOVES CONTINUOUSLY OR MOVES AT LEAST THE DECISION SIGHT DISTANCE EVERY 15 MINUTES.

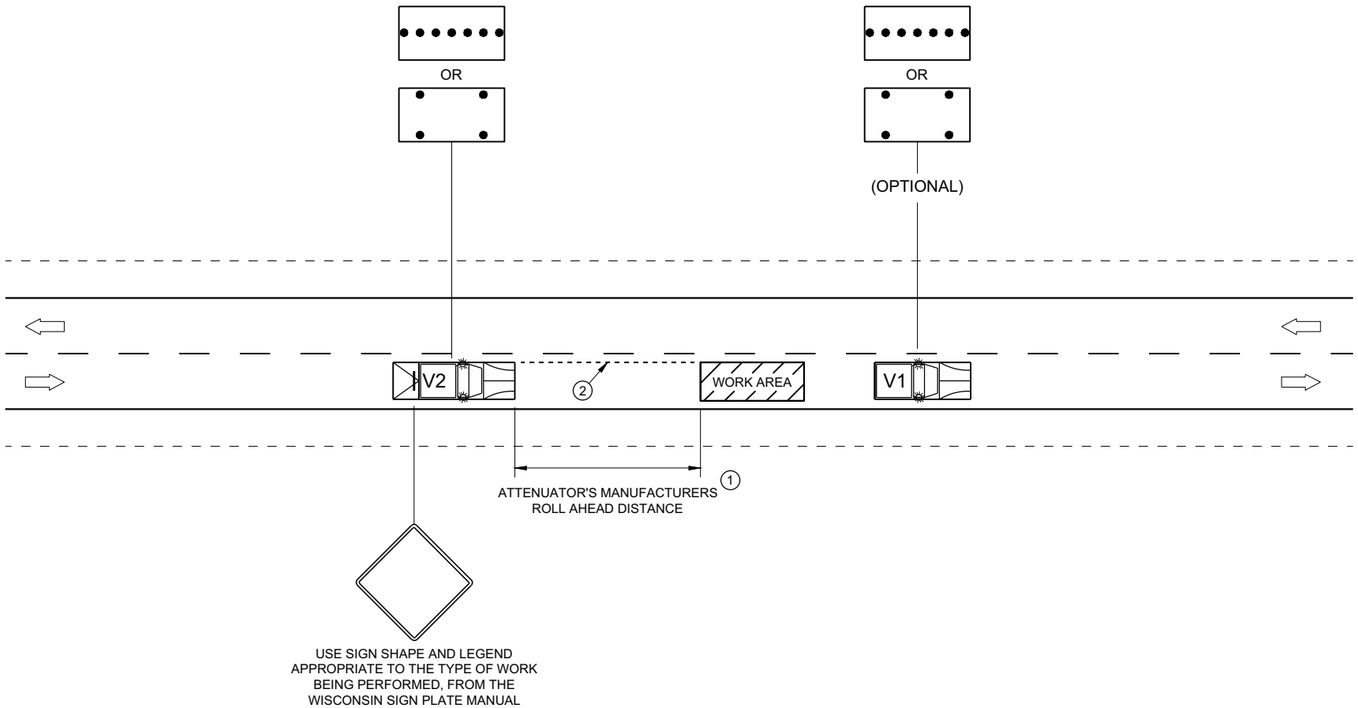
ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL ARROW PANELS SHALL BE REAR FACING, TYPE "B" OR "C", AND DISPLAYING THE FLASHING CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

(1) DISTANCE BETWEEN VEHICLES MAY INCREASE FROM THE ATTENUATOR'S ROLL AHEAD BASED ON TERRAIN, SIGHT DISTANCE, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

(2) ALIGN LEFT SIDE OF SHADOW VEHICLE WITH EDGE OF WORK AREA.



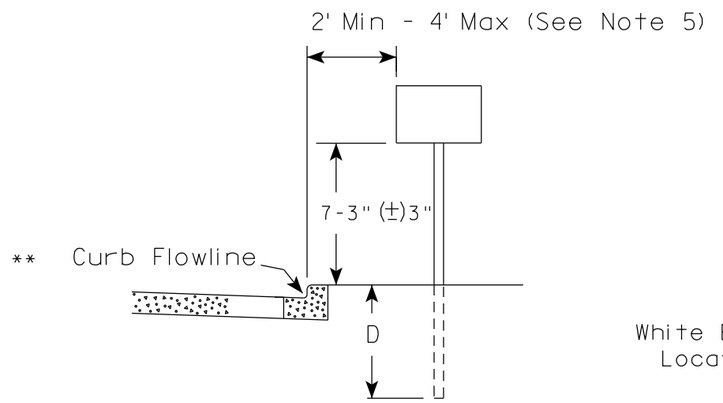
TRAFFIC CONTROL, MOBILE OPERATIONS ON AN UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

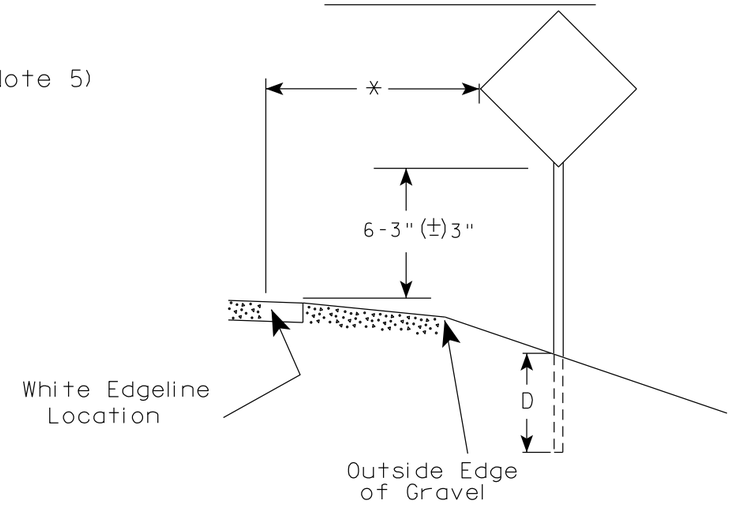
APPROVED
February 2021 — DATE
/S/ Andrew Heidtke — STATEWIDE WORK ZONE T SAFETY ENGINEER
FHWA

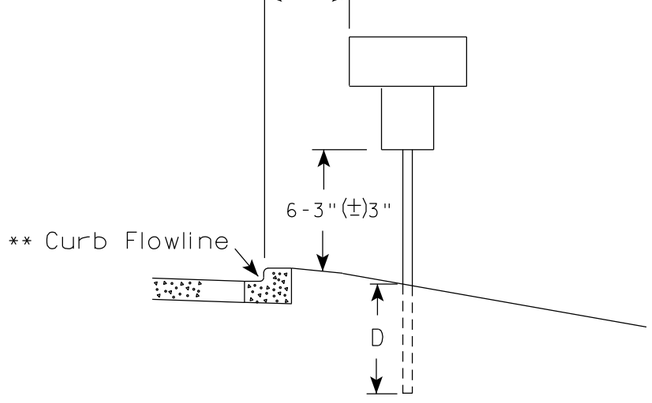
6
SDD 15D51 - 01

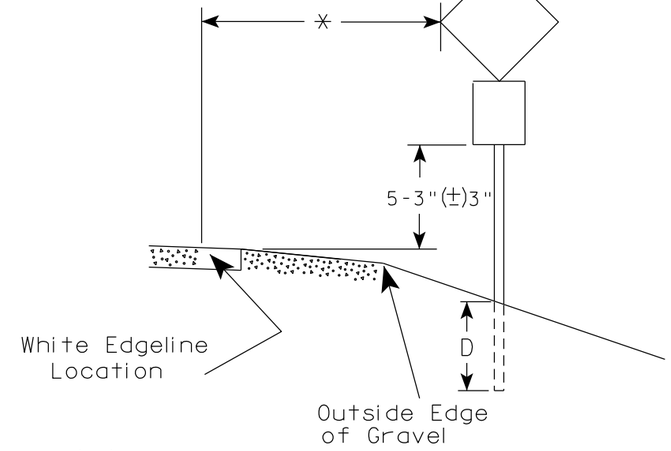
URBAN AREA



RURAL AREA (See Note 2)







** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
   The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".
3. For expressways and freeways, mounting height is 7'- 3" (±) 3" or 6'-3" (±) 3" depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±) 3".
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directd by the Engineer.

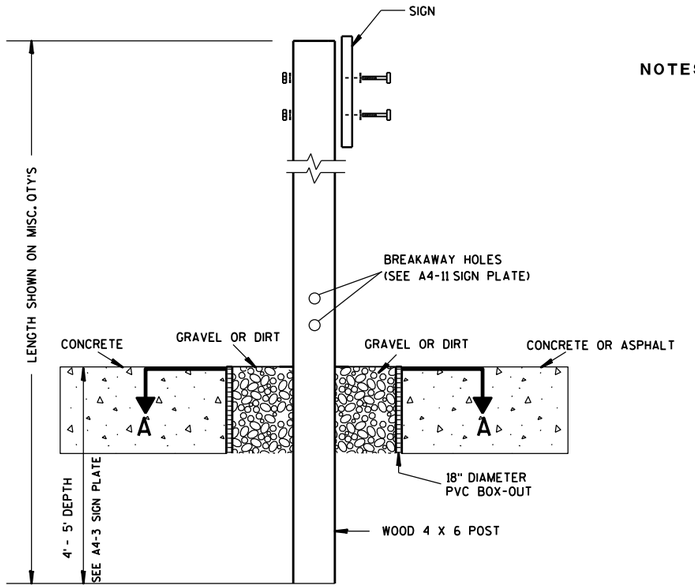
TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-3.23

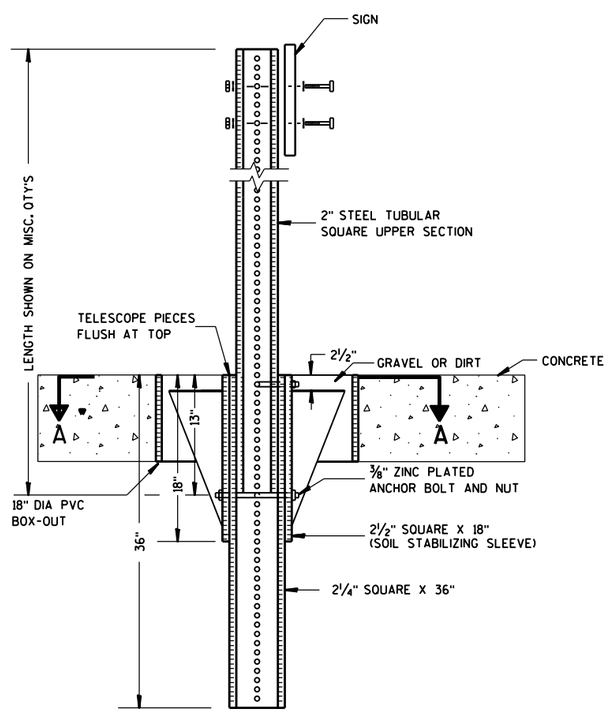
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 78	E



ELEVATION VIEW

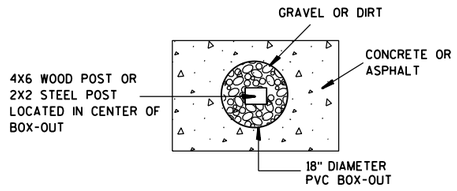
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

NOTES:
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

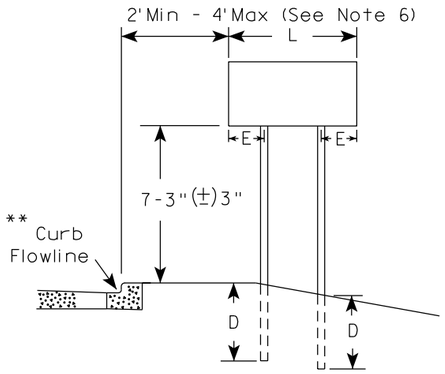
APPROVED Matthew R Rauch
for State Traffic Engineer

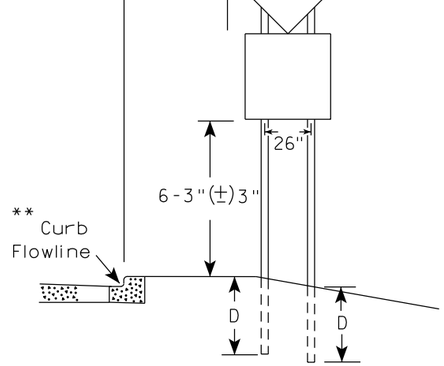
DATE 1/27/14 PLATE NO. A4-3B.1

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN PLOT DATE : 27-JAN-2014 09:48 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42

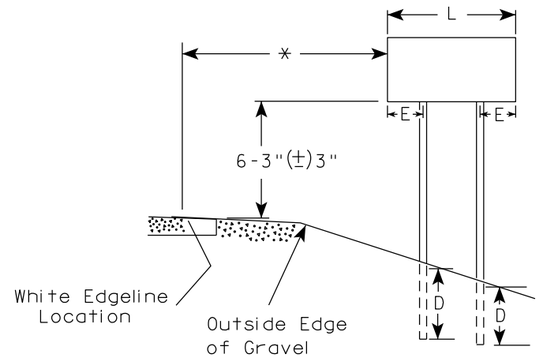
URBAN AREA

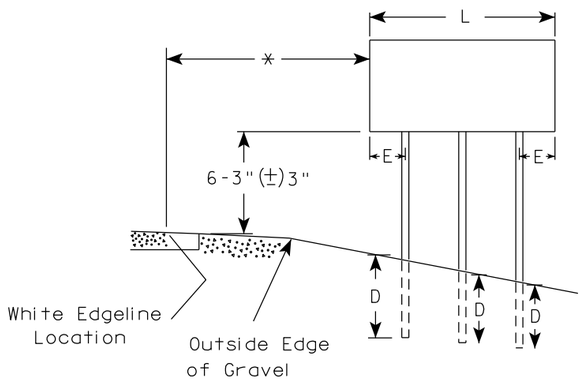


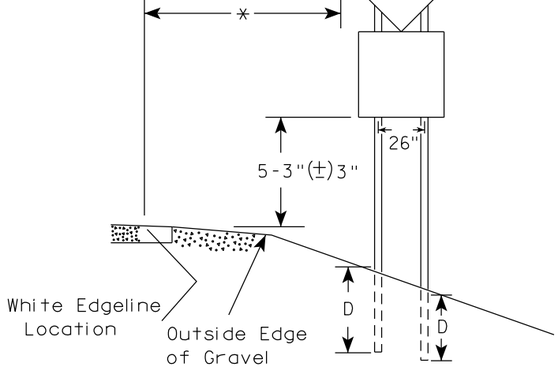


48" DIAMOND WARNING SIGN

RURAL AREA (See Note 3)







48" DIAMOND WARNING SIGN

GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) 3" or 6'-3" (±) 3" depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±) 3".

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)		
	L	E
***	Greater than 48" Less than 60"	12"
	60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

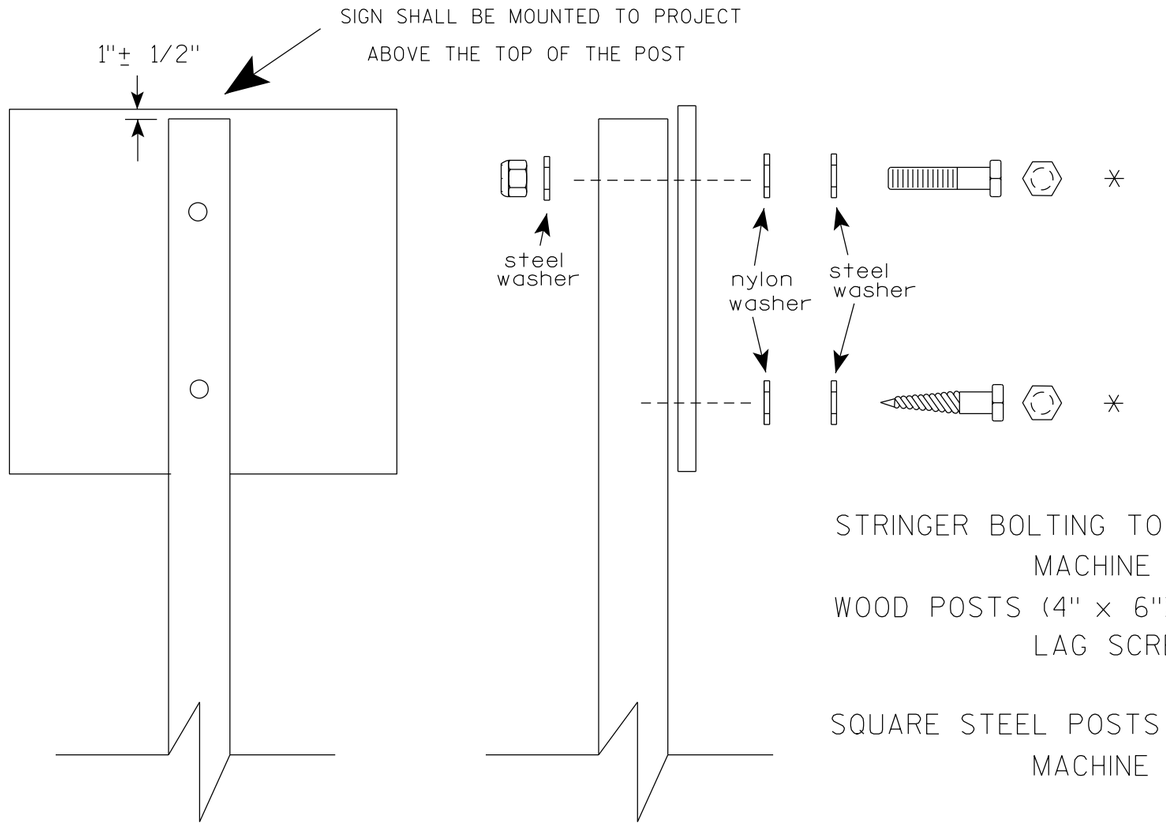
TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-4.16

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 80	E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3

b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

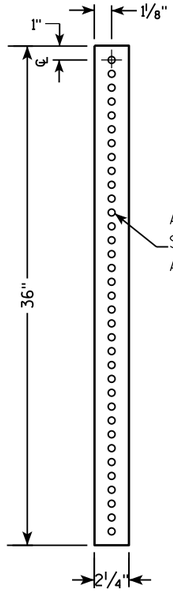
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

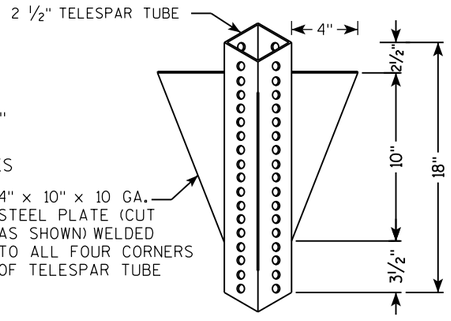
DATE 4/1/2020 PLATE NO. A4-8.9

TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM

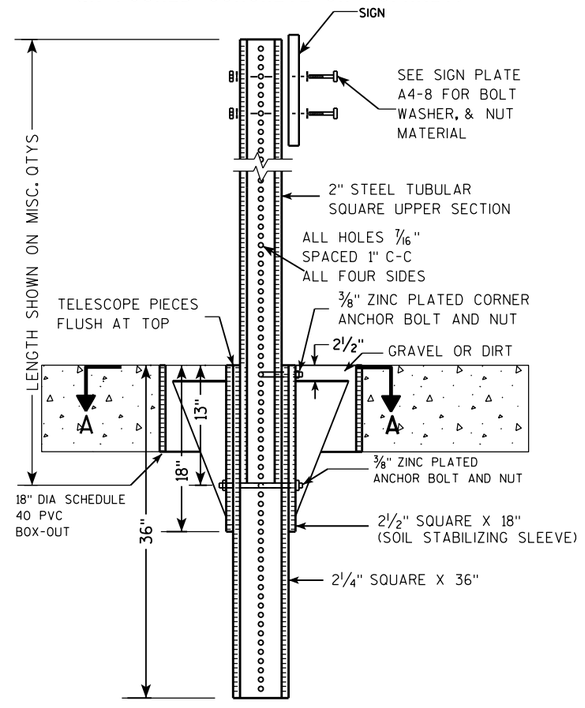
2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



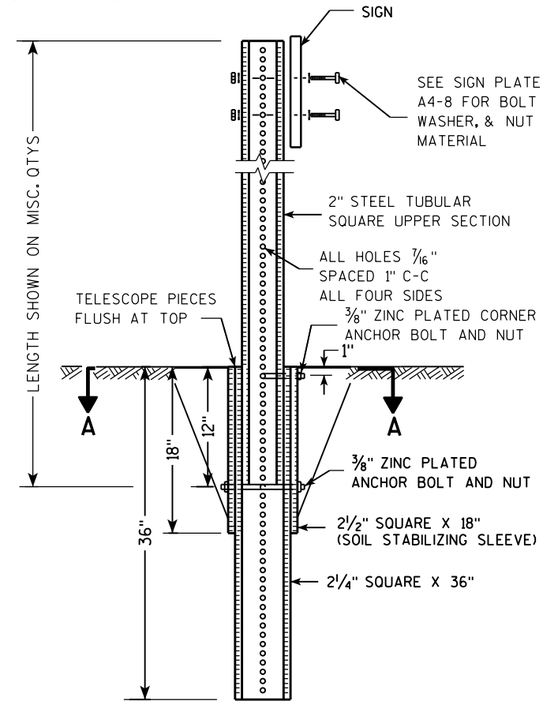
2 1/2 " SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH

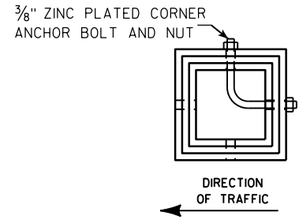


DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)





SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

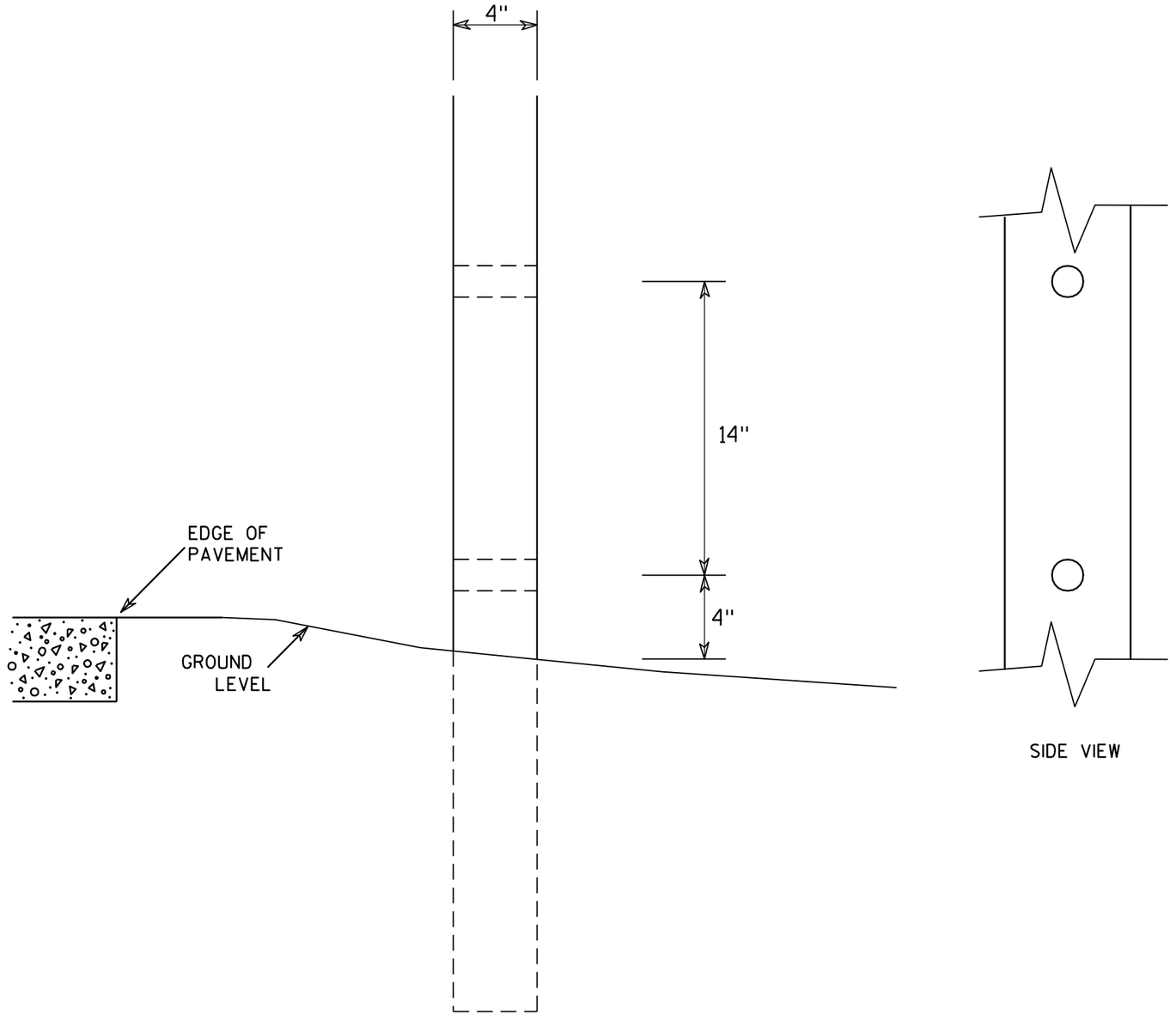
TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/05/15 PLATE 82 A4-9.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

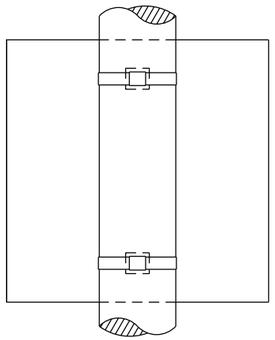
APPROVED Chester J Spang for State Traffic Engineer

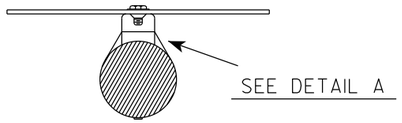
DATE 3/27/97 PLATE NO. A4-11.2

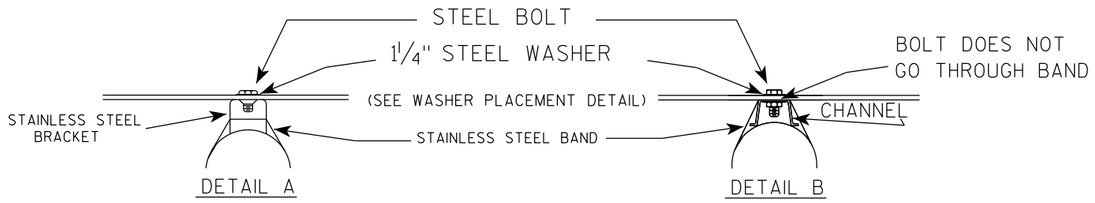
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 83	E

BANDING

SINGLE SIGN



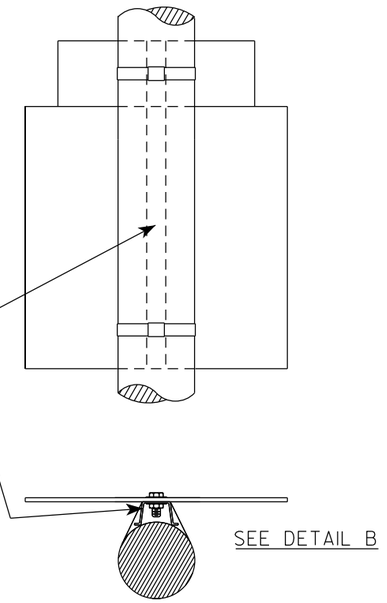




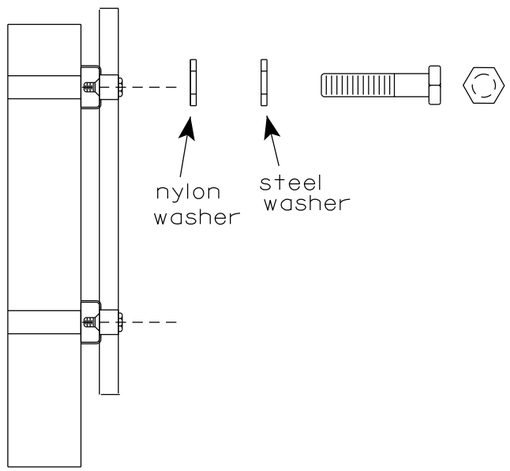
GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be ¾" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



WASHER PLACEMENT



WASHERS (ALL POSTS) -
1-1/4" O.D. X ⅜" I.D. X 1/16" STEEL
1-1/4" O.D. X ⅜" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

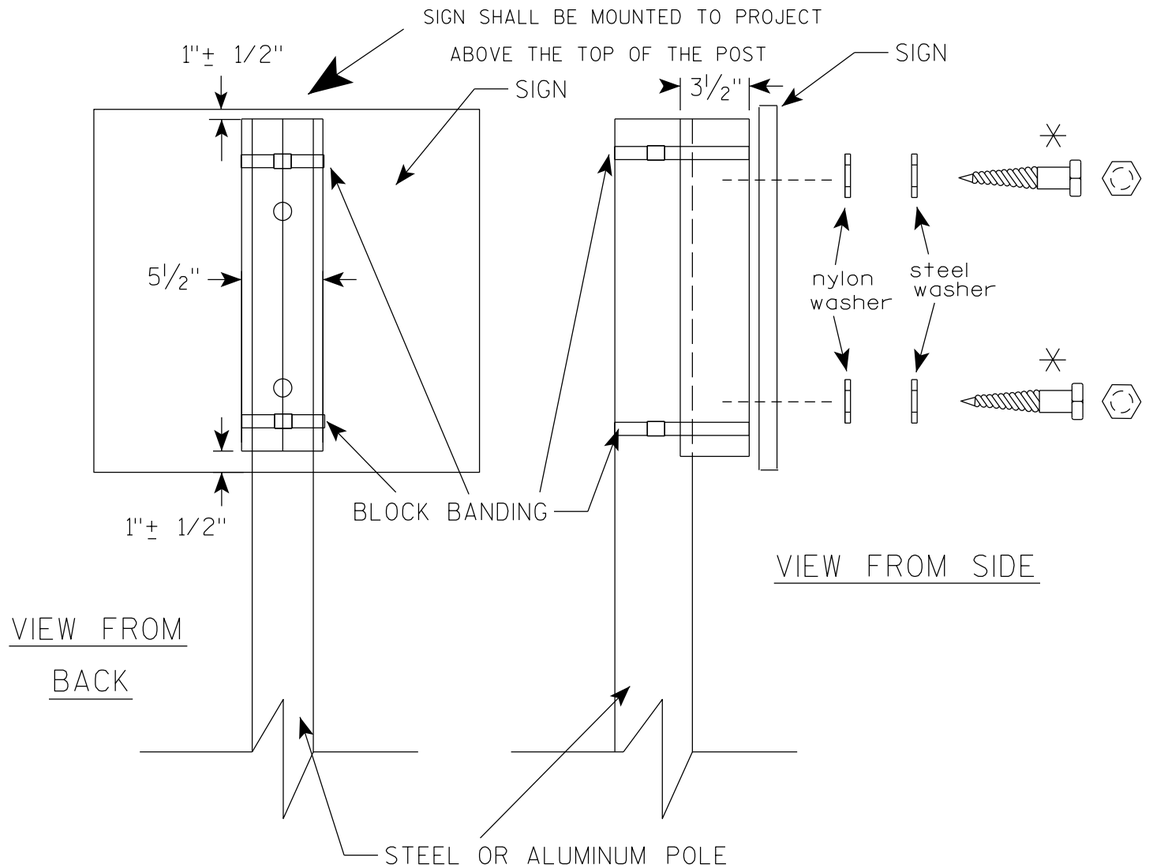
WISCONSIN DEPT OF TRANSPORTATION

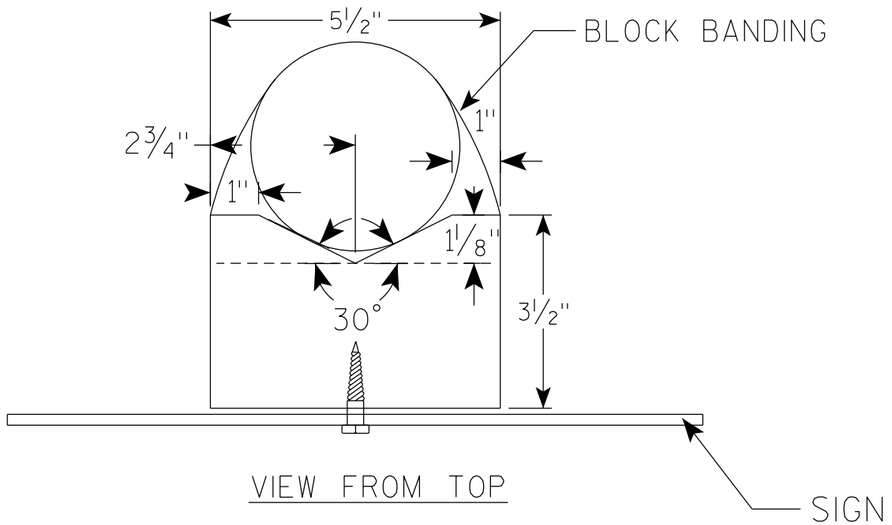
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 6/10/19 PLATE NO. A5-9.4

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 84	E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A59.dgn PLOT DATE : 10-JUN 2019 4:10 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : $$.....plotscale.....$$





GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WisDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, ¾" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORNALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✳ LAG BOLTS SHALL BE ⅜" X 2½"

BLOCK BANDING DETAIL (V-BLOCK OPTION)

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 4/19/2022 PLATE NO. A5-10.3

PROJECT NO:

SHEET NO: 85 E

Wisconsin Department of Transportation

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through innovation and exceptional service.

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