

EAU
WITH: N/A

PROJECT ID:
7995-02-67

COUNTY:
EAU CLAIRE

MAY 2025
ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plot
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 50



DESIGN DESIGNATION

A.A.D.T.	=	N/A
A.A.D.T.	=	N/A
D.H.V.	=	N/A
D.D.	=	N/A
T.	=	N/A
DESIGN SPEED	=	N/A
ESALS	=	N/A

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

C EAU CLAIRE, VARIOUS LOCATIONS

CITY TRANSIT STOP IMPROVMENTS

VAR HWY

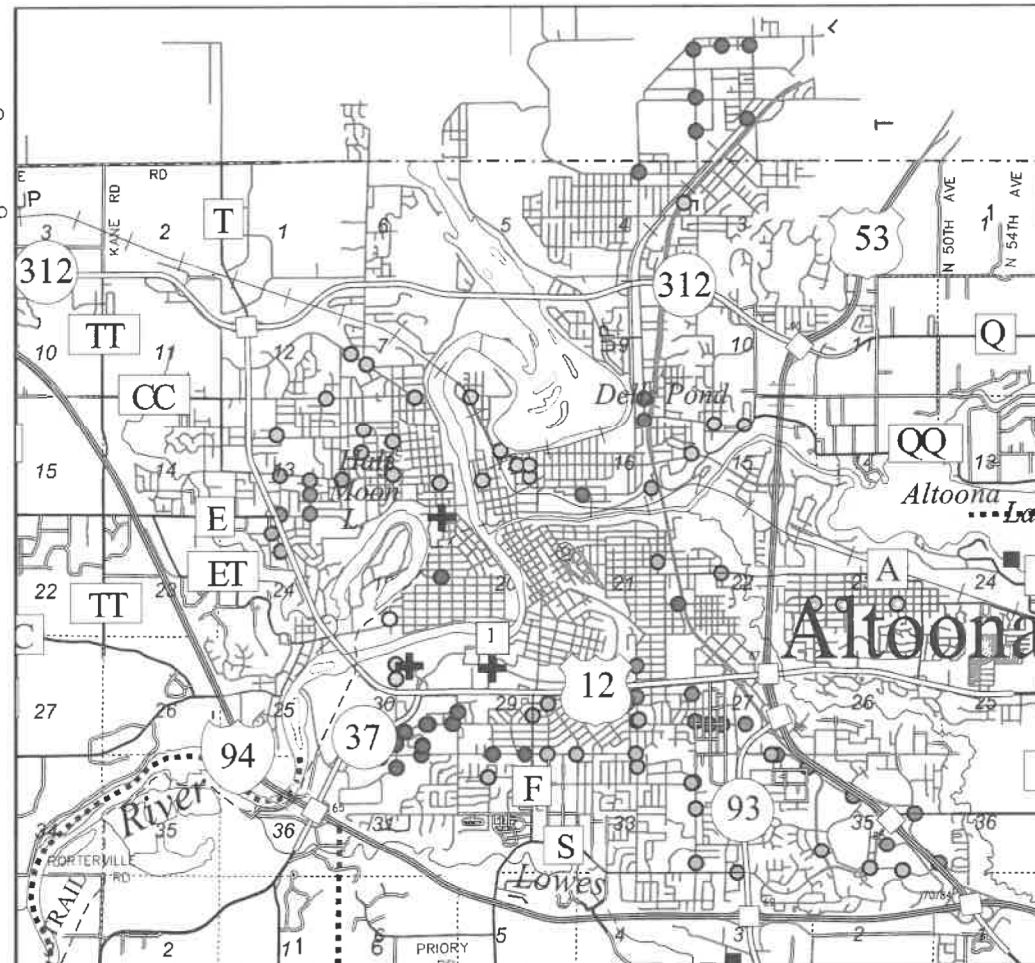
EAU CLAIRE COUNTY

STATE PROJECT NUMBER

7995-02-67

R-9-W R-8-W

CHIPPEWA CO
EAU CLAIRE CO



T-28-N
T-27-N

T-27-N
T-26-N

LAYOUT

SCALE 0 N/A

TOTAL NET LENGTH OF CENTERLINE = 0.000

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), EAU CLAIRE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 09.

STATE PROJECT

7995-02-67

FEDERAL PROJECT

PROJECT

WISC 2025485

CONTRACT

1

ACCEPTED FOR

CITY OF EAU CLAIRE

1-22-25 *Taylor Jackson*

Date (Signature and Title of Official)

Transit Manager

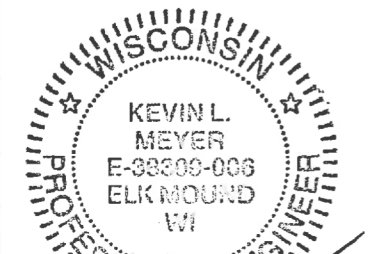
ORIGINAL PLANS PREPARED BY

CORRE



ENGINEERING

MADISON | EAU CLAIRE | WAUKESHA | APPLETON | TOMAH | WAUSAU



[Signature]
1/15/25

DATE: (Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY	
Surveyor	CORRE, INC.
Designer	CORRE, INC.
Project Manager	TOU YANG, PE
Regional Examiner	TOU YANG, PE
Regional Supervisor	TOU YANG, PE

APPROVED FOR THE DEPARTMENT
DATE: *Tou X. Yang*
(Signature)

E

UTILITIES CONTACTS

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WISCONSIN DNR LIAISON

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CITY TRANSIT CONTACT

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COMMUNITY SERVICES OPERATIONS MANAGER
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DESIGN PROJECT MANAGER

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GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

RIGHT OF WAY INFORMATION SHOWN ON THE PLANS IS APPROXIMATE.

THE CONTRACTOR IS TO WORK WITH UTMOST CARE AND PROTECT ALL SURVEY MARKERS. REMOVAL OF ANY SURVEY MARKER IS TO BE WITH THE APPROVAL OF THE ENGINEER.

TOPSOIL SHALL BE PLACED 1-INCH BELOW THE TOP OF ADJACENT CONCRETE CURBS OR SIDEWALKS.

TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ORDER OF SECTION 2 DETAIL SHEETS

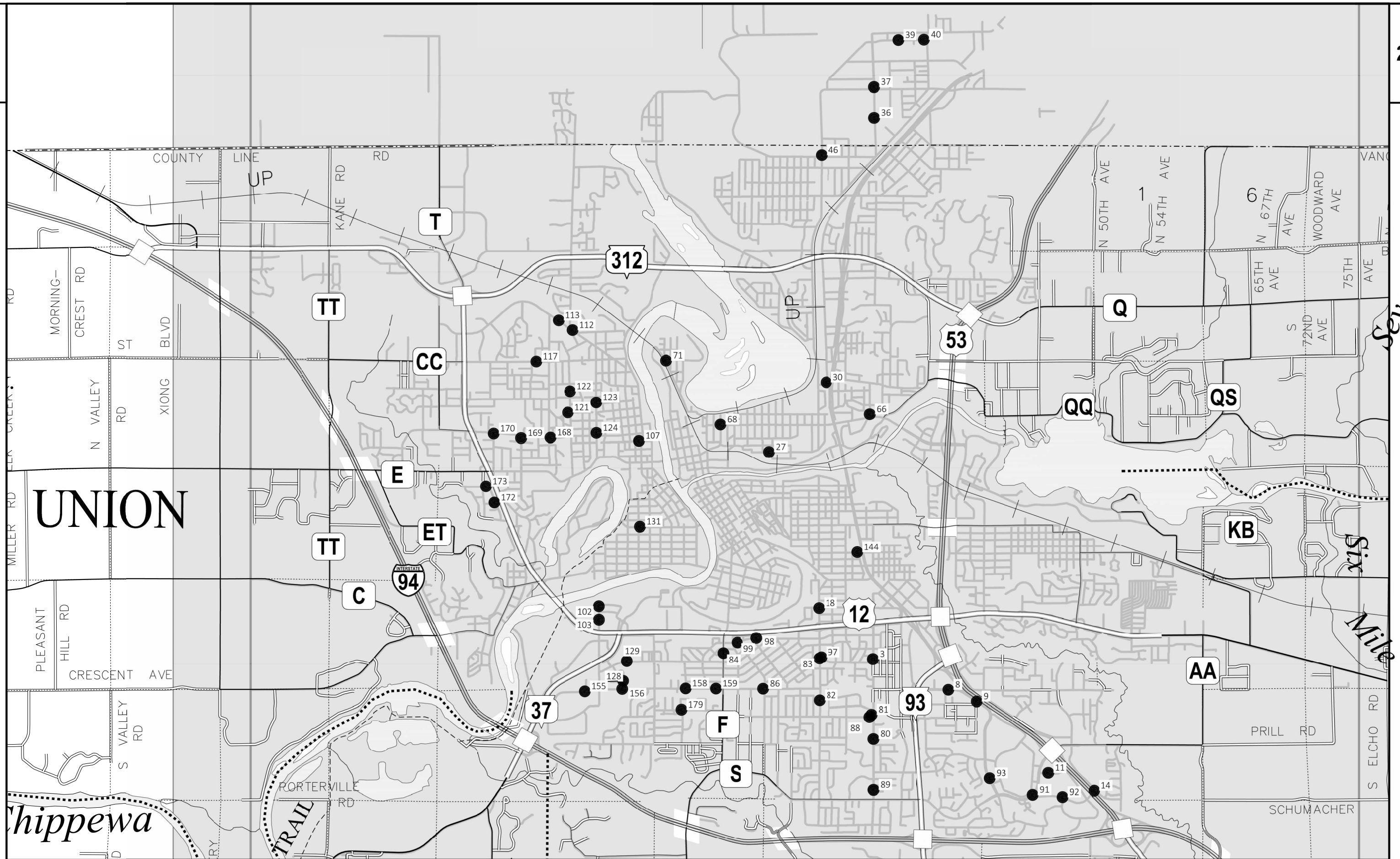
GENERAL NOTES
PROJECT OVERVIEW
CONSTRUCTION DETAILS
PLAN DETAILS



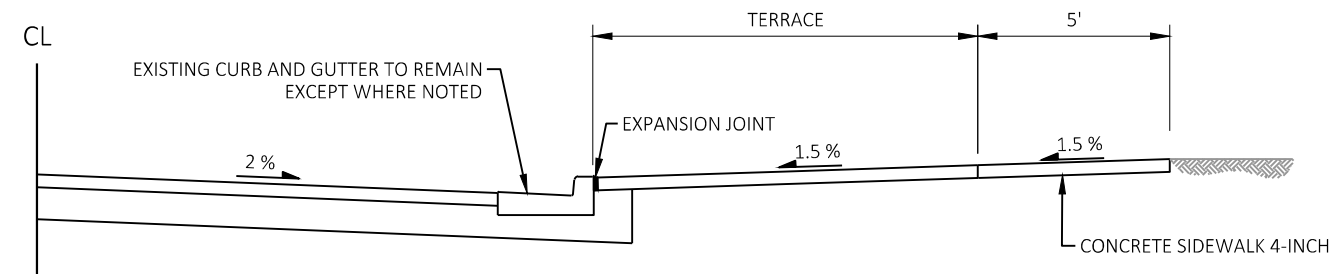
RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS:	.08	.16	.22	.12	.20	.27	.15	.24	.33	.19	.28	.38
	.22	.30	.38	.26	.34	.44	.30	.37	.50	.34	.41	.56
MEDIAN STRIPTURF:	.19	.20	.24	.19	.22	.26	.20	.23	.30	.20	.25	.30
	.24	.26	.30	.25	.28	.33	.26	.30	.37	.27	.32	.40
SIDE SLOPETURF:			.25			.27			.28			.30
			.32			.34			.36			.38
PAVEMENT:												
ASPHALT:	.70 - .95											
CONCRETE:	.80 - .95											
BRICK:	.70 - .80											
DRIVES, WALKS:	.75 - .85											
ROOFS:	.75 - .95											
GRAVEL ROADS, SHOULDERS:	.40 - .60											

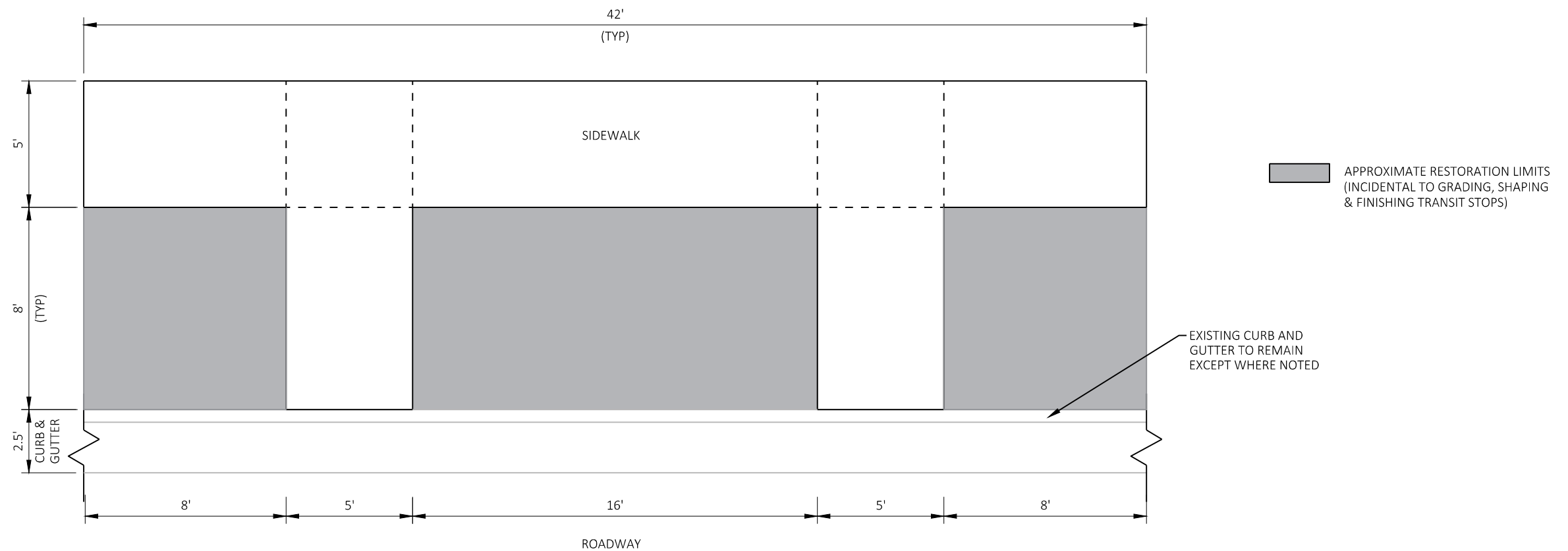
TOTAL PROJECT AREA = 0.7 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.35 ACRES



PROJECT NO: 7995-02-67	HWY: VAR HWY	COUNTY: EAU CLAIRE	PROJECT OVERVIEW	SHEET	E
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TRANSIT LOADING AREA DETAIL
SECTION VIEW



TRANSIT LOADING AREA DETAIL
PLAN VIEW

PROJECT NO: 7995-02-67

HWY: VAR HWY

COUNTY: EAU CLAIRE

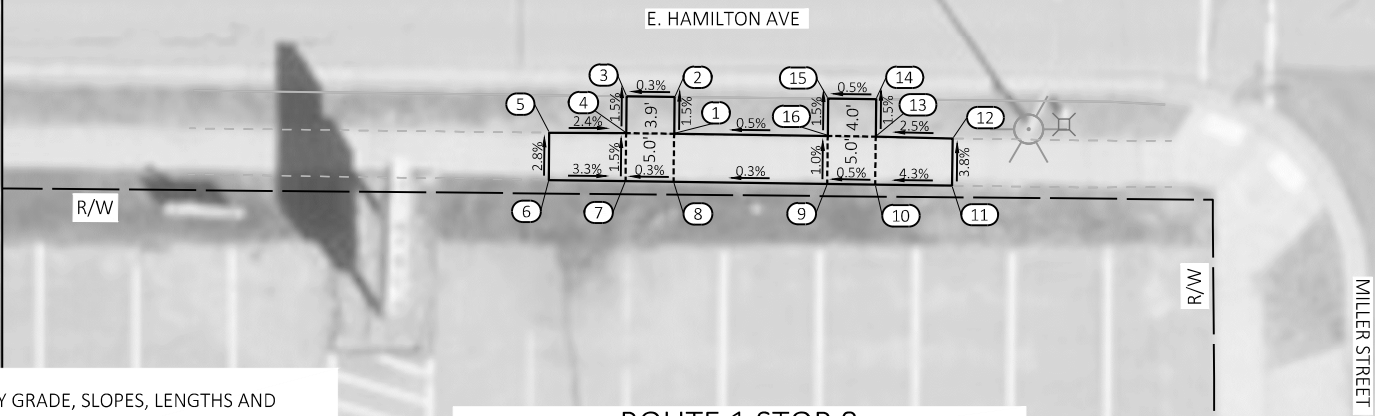
CONSTRUCTION DETAILS

SHEET

E

ROUTE 1 STP8-E HAMILTON - MILLER			
POINT NO.	ELEVATION	Y	X
9	877.33	269220.16	350823.61
10	877.42	269220.09	350828.60
11	877.69	269219.97	350836.61
12	877.50	269224.89	350836.66
13	877.30	269225.09	350828.66
14	877.24	269228.99	350828.70
15	877.22	269229.04	350823.70
16	877.28	269225.16	350823.66

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR SINGLE RIGHT
LANE CLOSURE, UNDIVIDED
NON-FREEWAY/EXPRESSWAY AND LANE CLOSURE
WITH FLAGGING OPERATION"

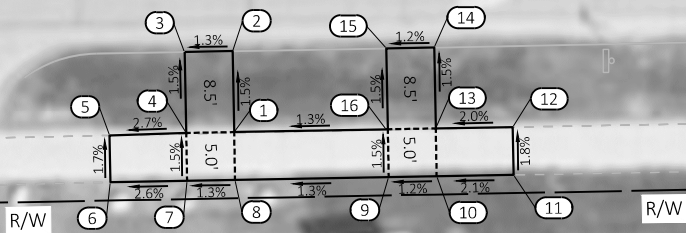


ROUTE 1 STOP 8
E. HAMILTON AVE. & MILLER STREET

- NOTE:
"LANE CLOSURE TO FOLLOW SINGLE LANE CLOSURE,
DIVIDED NON-FREEWAY/EXPRESSWAY AND LANE
CLOSURE WITH FLAGGING OPERATION"

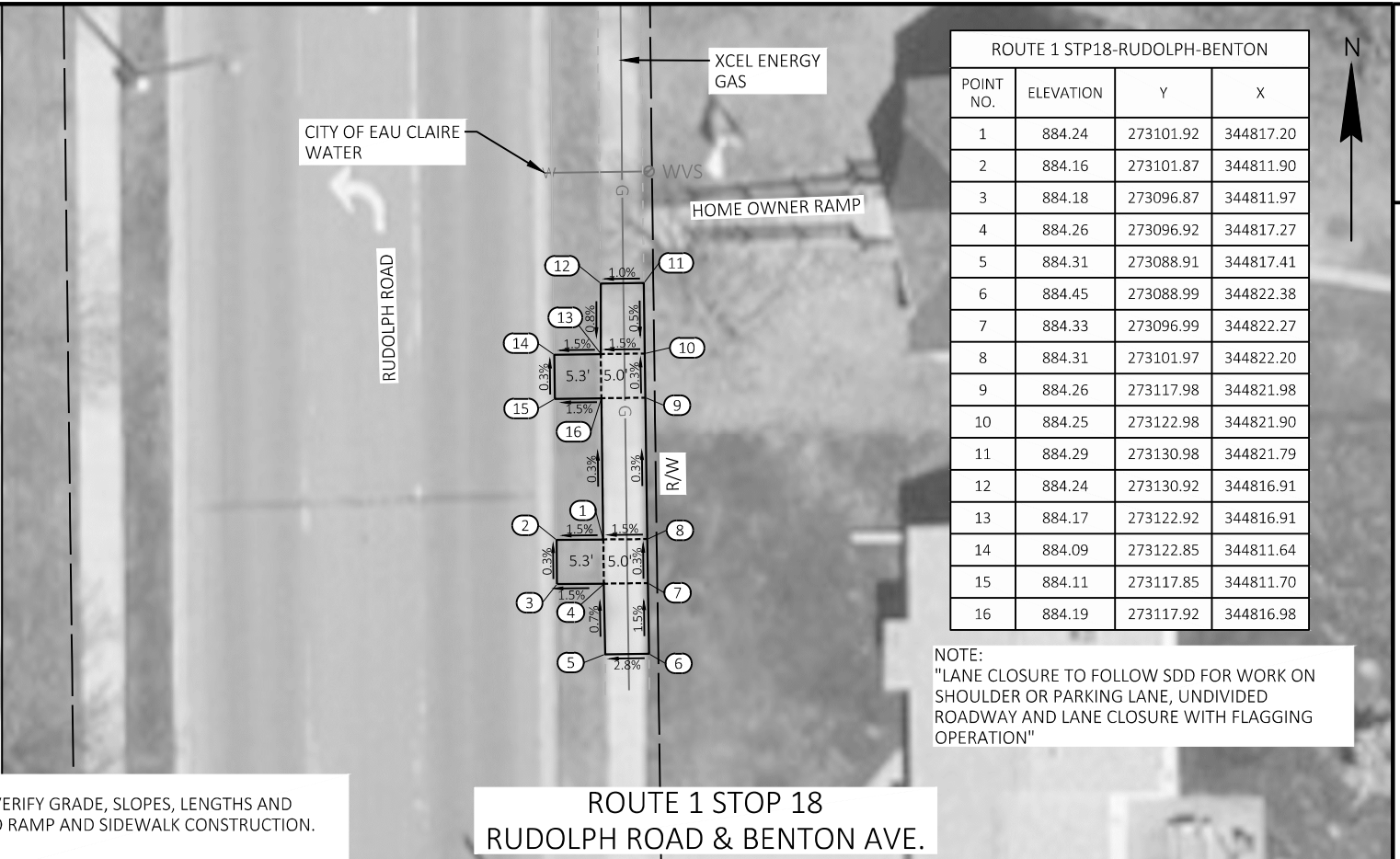
ROUTE 1 STP11-COMMON WEALTH - BAM BOOKSTORE			
POINT NO.	ELEVATION	Y	X
1	918.10	265195.38	355852.21
2	917.97	265203.81	355852.05
3	917.91	265203.72	355847.05
4	918.03	265195.29	355847.22
5	917.82	265195.03	355839.23
6	917.90	265190.15	355839.32
7	918.11	265190.29	355847.31
8	918.17	265190.38	355852.31
9	918.38	265190.67	355868.31
10	918.44	265190.76	355873.31
11	918.61	265190.91	355881.31
12	918.52	265195.88	355881.21
13	918.36	265195.76	355873.21
14	918.24	265204.18	355873.05
15	918.18	265204.09	355868.07
16	918.30	265195.62	355868.06

COMMONWEALTH AVE.



ROUTE 1 STOP 11
COMMONWEALTH AVE. & BAM BOOKSTORE

ROUTE 1 STP18-RUDOLPH-BENTON			
POINT NO.	ELEVATION	Y	X
1	884.24	273101.92	344817.20
2	884.16	273101.87	344811.90
3	884.18	273096.87	344811.97
4	884.26	273096.92	344817.27
5	884.31	273088.91	344817.41
6	884.45	273088.99	344822.38
7	884.33	273096.99	344822.27
8	884.31	273101.97	344822.20
9	884.26	273117.98	344821.98
10	884.25	273122.98	344821.90
11	884.29	273130.98	344821.79
12	884.24	273130.92	344816.91
13	884.17	273122.92	344816.91
14	884.09	273122.85	344811.64
15	884.11	273117.85	344811.70
16	884.19	273117.92	344816.98



ROUTE 3 STP27 - BELLVUE - SPRING			
POINT NO.	ELEVATION	Y	X
12	875.50	280665.20	342495.27
13	875.34	280665.29	342487.27
14	875.26	280670.69	342487.24
15	875.24	280670.67	342482.24
16	875.32	280665.26	342482.27

NO.	ELEVATION	Y	X	NO.	ELEVATION	Y	X
1	875.23	280665.17	342466.27	12	875.50	280665.20	342495.27
2	875.14	280670.60	342466.24	13	875.34	280665.29	342487.27
3	875.12	280670.58	342461.24	14	875.26	280670.69	342487.24
4	875.20	280665.15	342461.27	15	875.24	280670.67	342482.24
5	875.31	280665.03	342453.27	16	875.32	280665.26	342482.27
6	875.44	280660.10	342453.30				
7	875.27	280660.15	342461.30				
8	875.30	280660.17	342466.30				
9	875.39	280660.26	342482.30				
10	875.42	280660.29	342487.30				
11	875.64	280660.34	342495.30				

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON
SHOULDER OR PARKING LANE, UNDIVIDED
ROADWAY AND LANE CLOSURE WITH FLAGGING
OPERATION"

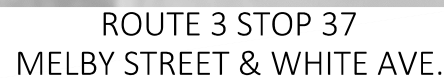
ROUTE 3 STOP 27
BELLVUE AVE. & SPRING STREET

ROUTE 3 STP30 - SEYMOUR - WESTERN			
POINT NO.	ELEVATION	Y	X
1	898.72	284024.14	345178.52
2	898.61	284027.60	345171.79
3	898.55	284023.39	345169.54
4	898.66	284019.72	345176.18
5	898.69	284012.86	345172.20
6	898.87	284010.21	345176.41
7	898.74	284017.31	345180.55
8	898.80	284021.86	345182.97
9	898.97	284036.84	345189.80
10	899.04	284041.65	345191.64
11	899.44	284049.50	345194.30
12	899.32	284050.94	345189.51
13	898.96	284043.37	345186.94
14	898.85	284045.95	345179.87
15	898.78	284041.50	345178.15
16	898.89	284038.70	345185.15

NOTE:
"FOR EASTBOUND TRAFFIC ON SEYMOUR ROAD, LANE CLOSURE TO FOLLOW SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY. FOR WESTBOUND TRAFFIC ON SEYMOUR ROAD, LANE CLOSURE WILL FOLLOW MAINLINE CLOSURE WITH POSTED DETOUR ON A SHORT TERM BASIS WITH LIMITED DAYTIME HOURS"
SEE TEMPORARY DETOUR DETAIL.

ROUTE 3 STP37 - WHITE - MELBY			
POINT NO.	ELEVATION	Y	X
1	885.78	298205.38	347479.53
2	885.70	298205.15	347474.13
3	885.72	298200.14	347474.34
4	885.80	298200.38	347479.75
5	885.91	298192.39	347480.11
6	886.03	298192.60	347485.08
7	885.88	298200.58	347484.74
8	885.86	298205.59	347484.53
9	885.78	298221.57	347483.85
10	885.76	298226.57	347483.63
11	885.91	298234.56	347483.29
12	885.76	298234.35	347478.42
13	885.68	298226.36	347478.64
14	885.60	298226.13	347473.27
15	885.62	298221.13	347473.49
16	885.70	298221.36	347478.85

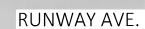
NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON
SHOULDER OR PARKING LANE, UNDIVIDED
ROADWAY AND LANE CLOSURE WITH FLAGGING
OPERATION"



- WHITE AVE.

ROUTE 3 STP39 - RUNWAY - CARDINAL			
POINT NO.	ELEVATION	Y	X
9	892.80	300450.90	348647.22
10	892.82	300450.98	348652.24
11	892.87	300451.08	348657.39
12	892.83	300455.95	348657.31
13	892.74	300455.98	348652.15
14	892.62	300464.12	348652.01
15	892.60	300464.05	348647.01
16	892.72	300455.89	348647.15

RUN

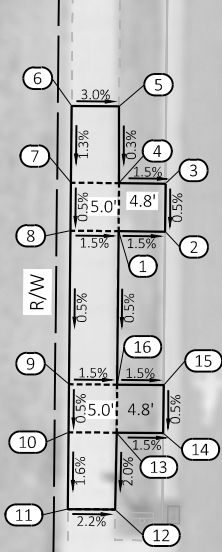


ROUTE 3 STOP 40
RUNWAY AVE. & ROBIN ROAD

The diagram illustrates a three-lane road with a center turn lane. The road is divided into three sections by two 'R/W' (Right of Way) lines. The left section is labeled 'CARRIAGE WAY' and the right section is labeled 'CARRIAGE WAY'. The center section is labeled 'R/W'. The road is divided into three lanes: a left lane, a center turn lane, and a right lane. The center turn lane is labeled 'G' and '0.2%'. The left lane is labeled '5.0%' and the right lane is labeled '5.0%'. The diagram shows two configurations for lane closure: one for 'SDD' (Shoulder/Dropout) and one for 'FLAGGING OPERATION'. The 'SDD' configuration shows a closure of the center turn lane and the right lane, with a closure length of 15.0'. The 'FLAGGING OPERATION' configuration shows a closure of the center turn lane and the left lane, with a closure length of 15.0'. The diagram also shows the 'R/W' lines and the 'CARRIAGE WAY' labels. A note at the bottom states: "NOTE: 'LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION'".

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON
SHOULDER OR PARKING LANE, UNDIVIDED
ROADWAY AND LANE CLOSURE WITH FLAGGING
OPERATION"

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION"



STARR AVE.

ROUTE 3 STP46 - STARR - NORTH			
POINT NO.	ELEVATION	Y	X
1	891.22	294935.74	344896.12
2	891.14	294935.69	344900.97
3	891.17	294940.69	344901.02
4	891.24	294940.74	344896.17
5	891.27	294948.74	344896.18
6	891.42	294948.79	344891.25
7	891.31	294940.79	344891.17
8	891.29	294935.79	344891.12
9	891.22	294919.80	344890.97
10	891.25	294914.80	344890.91
11	891.07	294906.80	344890.85
12	890.96	294906.75	344895.76
13	891.12	294914.75	344895.92
14	891.05	294914.73	344900.73
15	891.07	294919.70	344900.82
16	891.14	294919.75	344895.97



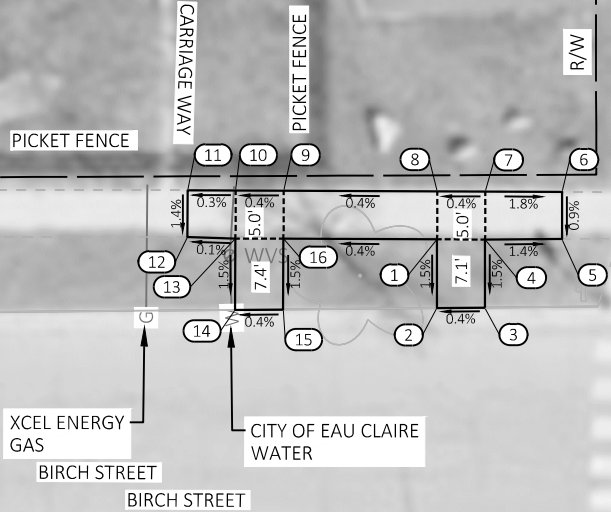
ROUTE 3 STOP 46
STARR AVE. & NORTH LANE

- NOTES:
1. CONTRACTOR TO FIELD VERIFY GRADE, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO RAMP AND SIDEWALK CONSTRUCTION.
 2. ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION.
 3. SIDEWALK CLOSURE TO FOLLOW SDD FOR TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION.

ROUTE 4 STP68-BIRCH - GERMANIA			
POINT NO.	ELEVATION	Y	X
1	877.58	282013.52	340009.25
2	877.48	282006.36	340009.22
3	877.50	282006.41	340014.22
4	877.60	282013.50	340014.25
5	877.49	282013.55	340022.25
6	877.53	282018.46	340022.27
7	877.68	282018.50	340014.27
8	877.66	282018.51	340009.27
9	877.60	282018.59	339993.27
10	877.58	282018.61	339988.27
11	877.57	282018.63	339983.27
12	877.50	282013.77	339983.25
13	877.50	282013.61	339988.25
14	877.39	282006.16	339988.22
15	877.41	282006.20	339993.22
16	877.52	282013.59	339993.25

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION"

ROUTE 4 STOP 68
BIRCH STREET & GERMANIA STREET



GERMANIA STREET

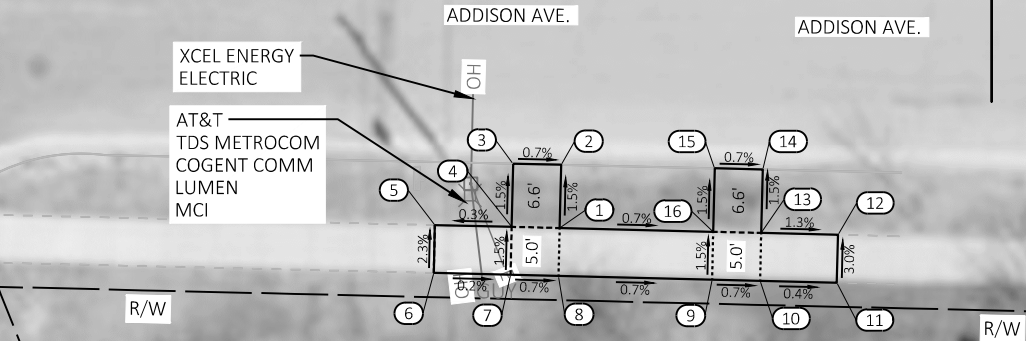


ROUTE 4 STP71-SHERIDAN - ADDISON			
POINT NO.	ELEVATION	Y	X
1	879.59	285121.17	337531.99
2	879.49	285127.76	337532.14
3	879.53	285127.87	337527.14
4	879.62	285121.29	337526.99
5	879.60	285121.47	337518.99
6	879.71	285116.49	337518.88
7	879.70	285116.30	337526.88
8	879.66	285116.17	337531.87
9	879.55	285115.78	337547.87
10	879.52	285115.66	337552.87
11	879.49	285115.47	337560.87
12	879.34	285120.46	337560.98
13	879.44	285120.66	337552.98
14	879.34	285127.27	337553.14
15	879.38	285127.39	337548.14
16	879.48	285120.78	337547.99

SHERIDAN AVE.



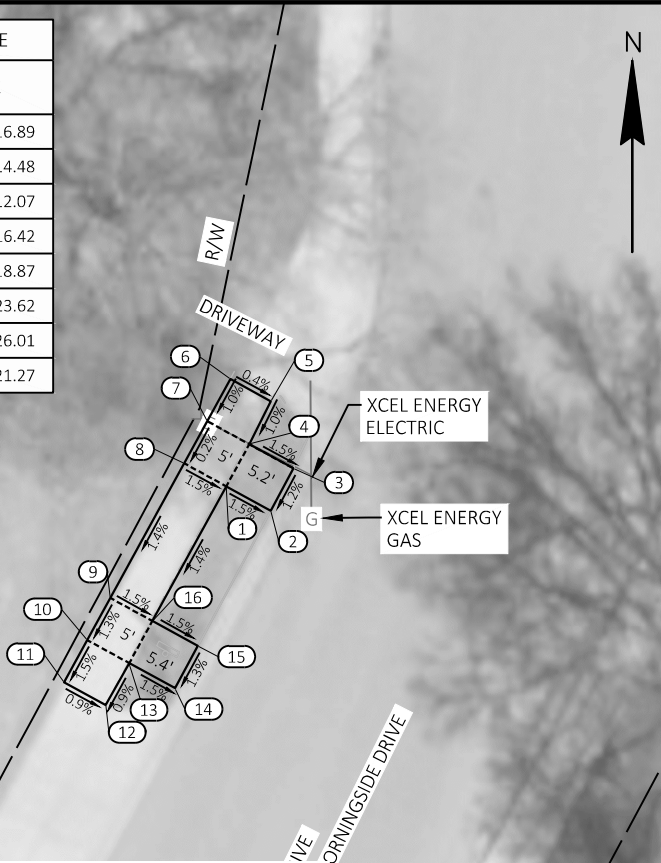
ROUTE 4 STOP 71
SHERIDAN ROAD & ADDISON AVE.



ROUTE 4 STP66-BIRCH - MORNINGSIDE			
POINT NO.	ELEVATION	Y	X
1	893.98	282486.27	347228.97
2	893.90	282483.73	347233.61
3	893.97	282488.14	347235.95
4	894.04	282490.66	347231.37
5	894.09	282495.13	347233.61
6	894.11	282497.45	347229.39
7	894.07	282493.06	347226.99
8	894.06	282488.68	347224.58

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION"

ROUTE 4 STP66-BIRCH - MORNINGSIDE			
POINT NO.	ELEVATION	Y	X
9	893.83	282474.65	347216.89
10	893.77	282470.27	347214.48
11	893.69	282465.86	347212.07
12	893.65	282463.51	347216.42
13	893.69	282467.86	347218.87
14	893.61	282465.25	347223.62
15	893.68	282469.65	347226.01
16	893.76	282472.24	347221.27



XCEL ENERGY ELECTRIC

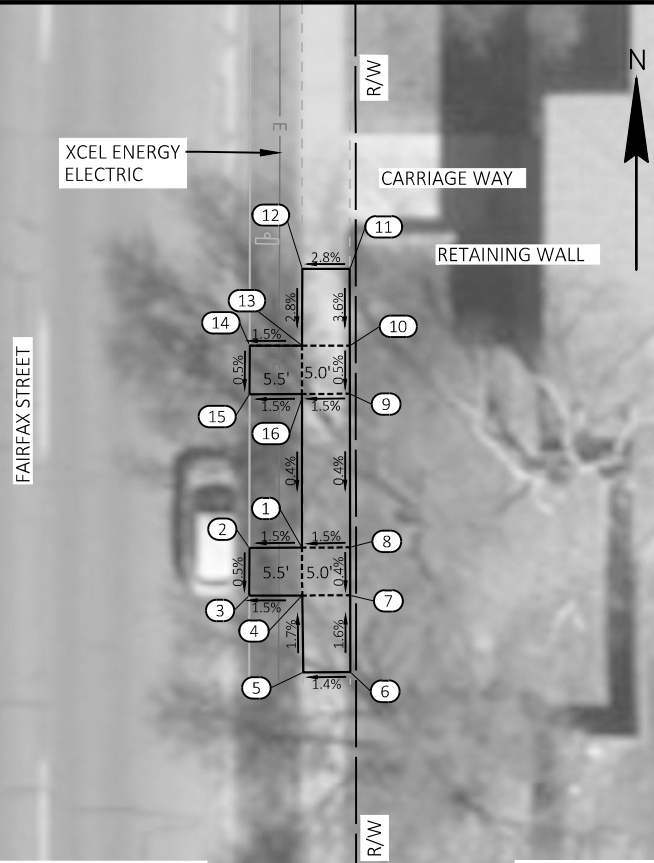
XCEL ENERGY GAS

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR LANE CLOSURE AND LANE CLOSURE WITH FLAGGING OPERATION"

ROUTE 4 STP80-FAIRFAX - SESSIONS			
POINT NO.	ELEVATION	Y	X
1	904.61	266830.37	347453.24
2	904.52	266830.34	347447.75
3	904.50	266825.39	347447.74
4	904.58	266825.39	347453.25
5	904.72	266817.39	347453.37
6	904.78	266817.40	347458.26
7	904.66	266825.40	347458.25
8	904.68	266830.40	347458.24
9	904.75	266846.40	347458.22
10	904.78	266851.40	347458.22
11	905.06	266859.40	347458.21
12	904.92	266859.39	347453.26
13	904.70	266851.41	347453.22
14	904.62	266851.42	347447.79
15	904.60	266846.34	347447.78
16	904.68	266846.15	347453.29

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION"

ROUTE 5 STOP 80
FAIRFAX STREET & SESSIONS STREET

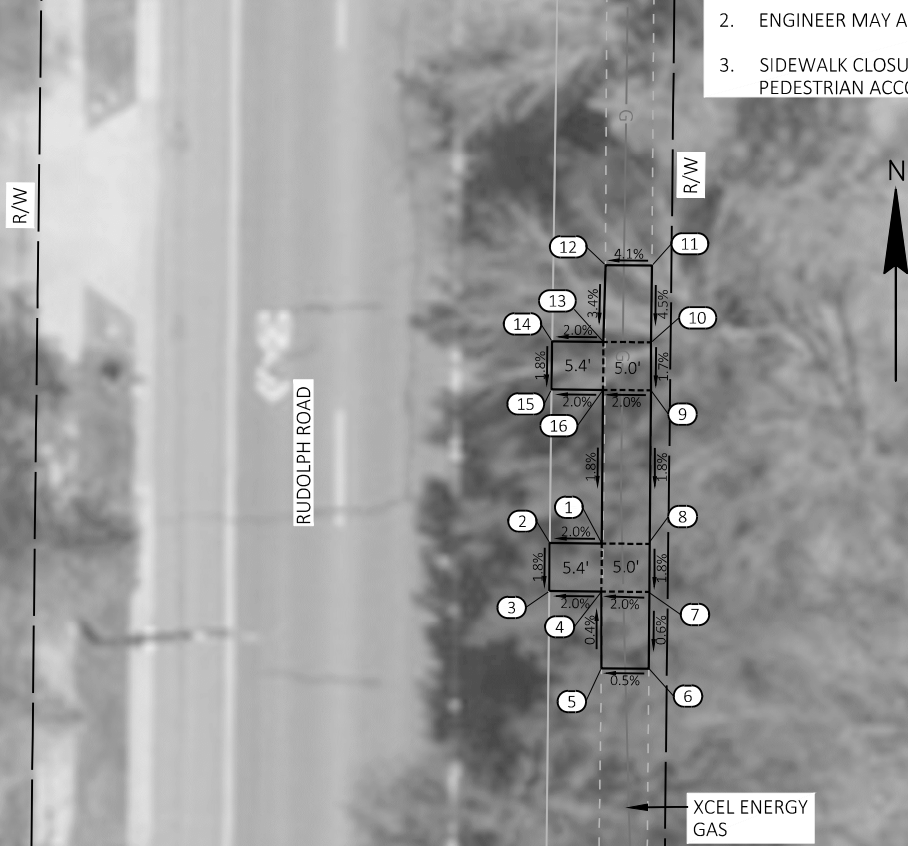


- NOTES:
1. CONTRACTOR TO FIELD VERIFY GRADE, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO RAMP AND SIDEWALK CONSTRUCTION.
 2. ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION.
 3. SIDEWALK CLOSURE TO FOLLOW SDD FOR TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION.

ROUTE 5 STP82-RUDOLPH - LEHMAN			
POINT NO.	ELEVATION	Y	X
1	896.66	268713.10	344862.16
2	896.55	268713.14	344856.73
3	896.46	268708.14	344856.67
4	896.57	268708.10	344862.12
5	896.60	268700.10	344862.11
6	896.63	268700.17	344867.00
7	896.67	268708.11	344866.95
8	896.76	268713.11	344867.05
9	897.05	268729.05	344867.28
10	897.13	268734.05	344867.29
11	897.50	268742.05	344867.38
12	897.31	268742.09	344862.53
13	897.03	268734.10	344862.32
14	896.93	268734.14	344856.98
15	896.84	268729.14	344856.91
16	896.95	268729.10	344862.28

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION"

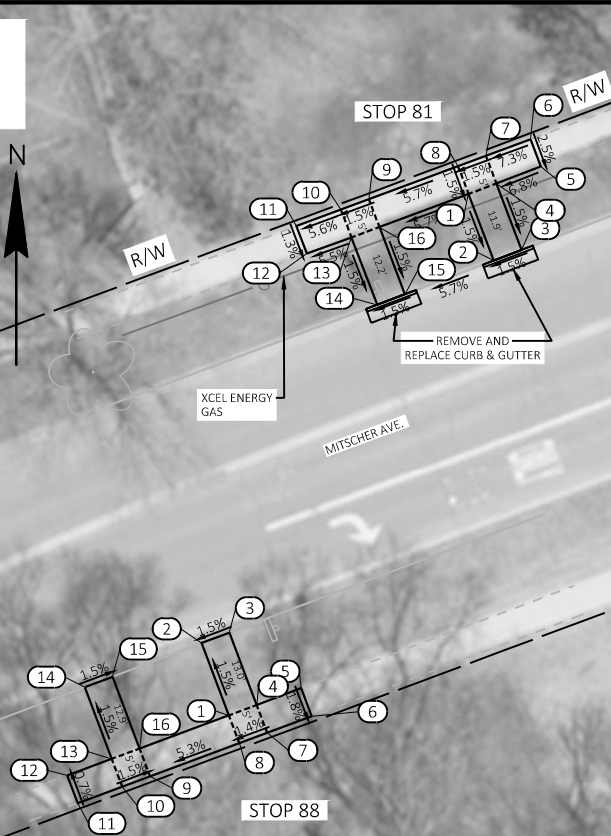
ROUTE 5 STOP 82
RUDOLPH ROAD & LEHMAN STREET



NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION"

ROUTE 6 STOP 88-MITSCHER - FAIRFAX			
POINT NO.	ELEVATION	Y	X
1	920.16	267981.53	347288.63
2	919.96	267993.67	347284.05
3	920.03	267995.44	347288.72
4	920.23	267983.27	347293.33
5	920.83	267986.10	347300.80
6	920.74	267981.37	347302.59
7	920.30	267978.59	347295.09
8	920.23	267976.85	347290.40
9	919.38	267971.28	347275.40
10	919.46	267969.54	347270.71
11	918.68	267966.75	347263.21
12	918.65	267971.44	347261.44
13	919.38	267974.21	347268.95
14	919.19	267986.25	347264.40
15	919.11	267988.02	347269.08
16	919.31	267975.95	347273.63

ROUTE 5 STOP 81 / ROUTE 6 STOP 88
FAIRFAX STREET & MITSCHER AVE.



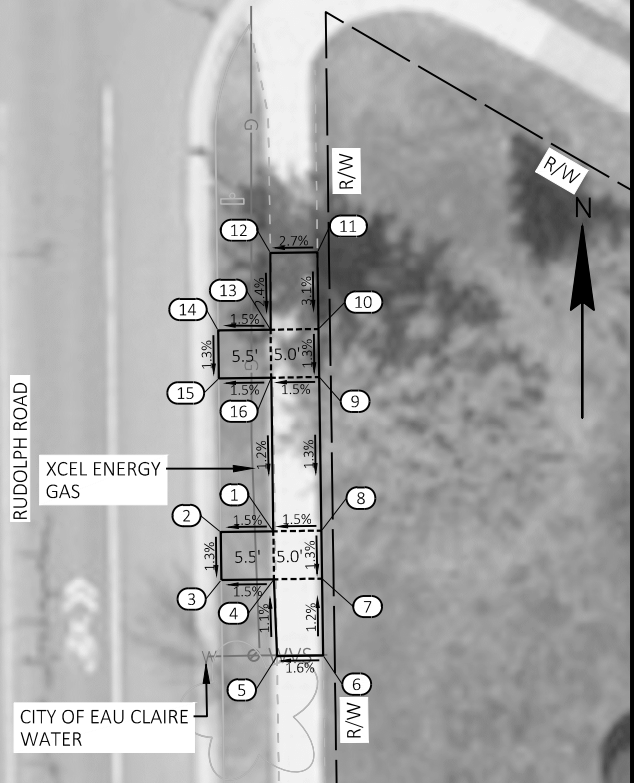
ROUTE 5 STP81-FAIRFAX - MITSCHER			
POINT NO.	ELEVATION	Y	X
1	923.20	268069.18	347328.91
2	923.02	268058.01	347333.20
3	923.09	268059.79	347337.87
4	923.27	268070.93	347333.60
5	923.82	268073.81	347341.06
6	923.94	268078.40	347339.36
7	923.35	268075.61	347331.86
8	923.27	268073.87	347327.17
9	922.36	268068.30	347312.17
10	922.29	268066.56	347307.48
11	921.84	268063.78	347299.98
12	921.77	268059.14	347301.71
13	922.21	268061.87	347309.22
14	922.03	268050.55	347313.57
15	922.11	268052.33	347318.24
16	922.29	268063.61	347313.91

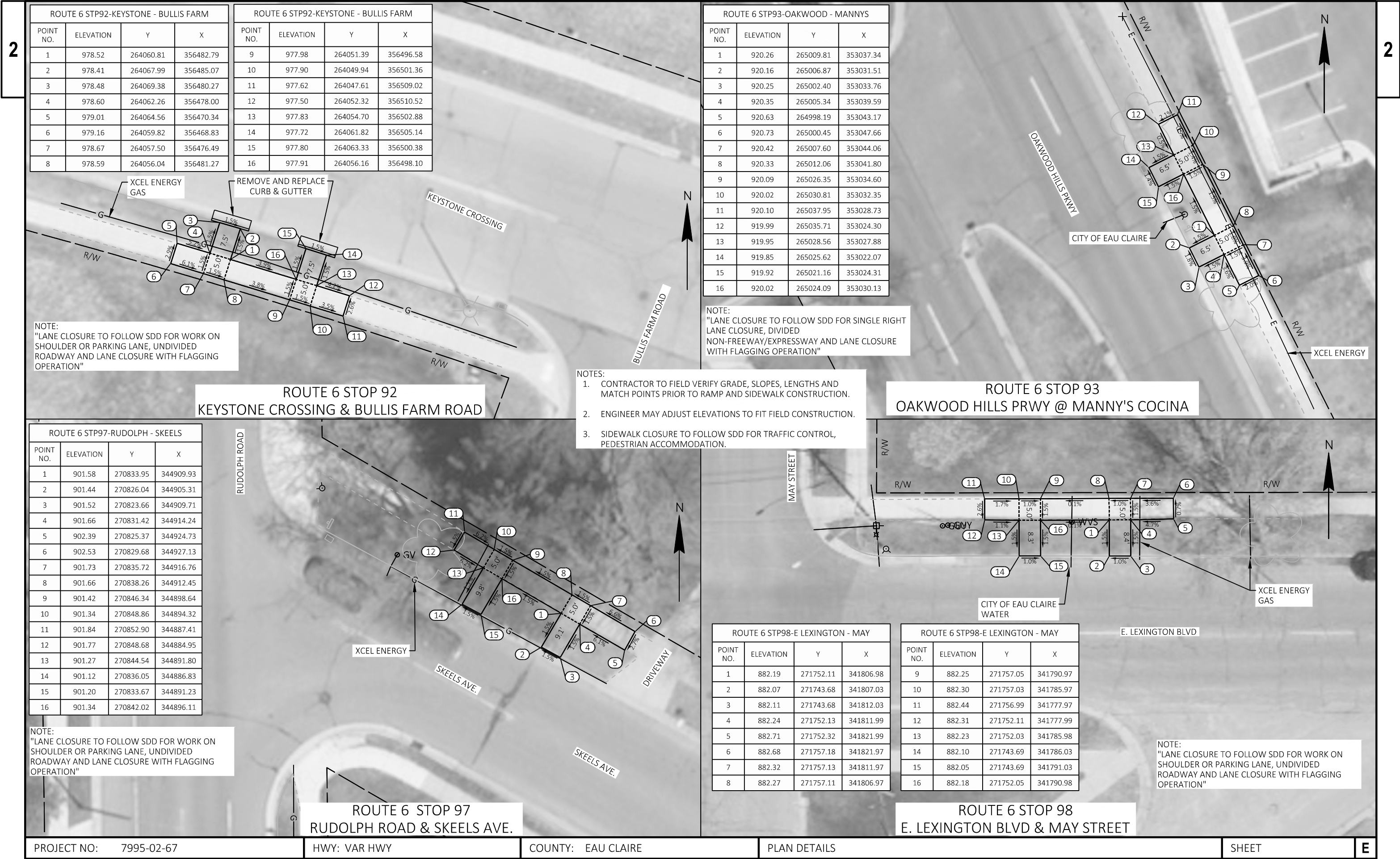
NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION"

ROUTE 5 STP83-RUDOLPH - SKEELS			
POINT NO.	ELEVATION	Y	X
1	899.50	270743.89	344849.84
2	899.41	270743.82	344844.36
3	899.35	270738.82	344844.43
4	899.43	270738.89	344849.91
5	899.52	270730.90	344850.23
6	899.60	270730.96	344855.02
7	899.51	270738.96	344854.91
8	899.57	270743.96	344854.84
9	899.77	270759.96	344854.62
10	899.84	270764.96	344854.55
11	900.09	270772.96	344854.44
12	899.96	270772.89	344849.54
13	899.76	270764.88	344849.55
14	899.68	270764.82	344844.09
15	899.62	270759.82	344844.15
16	899.70	270759.88	344849.62

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION"

ROUTE 5 STOP 83
RUDOLPH ROAD & SKEELS AVE.







ROUTE 6 STP99-E LEXINGTON - NIMITZ			
POINT NO.	ELEVATION	Y	X
1	884.41	271571.20	340896.99
2	884.29	271563.36	340899.07
3	884.30	271564.73	340903.89
4	884.42	271572.47	340901.82
5	884.52	271574.90	340909.49
6	884.49	271579.41	340908.24
7	884.49	271577.30	340900.53
8	884.49	271576.04	340895.73
9	884.43	271572.21	340880.31
10	884.40	271571.09	340875.52
11	884.44	271569.33	340867.62
12	884.45	271564.89	340868.58
13	884.33	271566.22	340876.65
14	884.21	271558.64	340878.42
15	884.23	271559.68	340883.27
16	884.35	271567.34	340881.46

ROUTE 7 STP102-CRAIG - OAK RIDGE			
POINT NO.	ELEVATION	Y	X
1	871.37	273267.36	334217.78
2	871.29	273267.39	334222.88
3	871.22	273272.39	334222.89
4	871.29	273272.36	334217.76
5	871.12	273280.36	334217.71
6	871.33	273280.33	334212.74
7	871.37	273272.33	334212.76
8	871.44	273267.33	334212.78
9	871.65	273251.33	334212.83
10	871.71	273246.33	334212.85
11	871.70	273242.12	334212.86
12	871.62	273242.14	334217.85
13	871.64	273246.36	334217.85
14	871.56	273246.39	334222.83
15	871.50	273251.39	334222.84
16	871.58	273251.36	334217.83

ROUTE 7 STP103-CRAIG - MARSHFIELD HOSP			
POINT NO.	ELEVATION	Y	X
1	875.95	272605.28	334217.84
2	875.86	272605.29	334223.29
3	875.89	272610.29	334223.29
4	875.97	272610.28	334217.83
5	876.14	272618.28	334217.72
6	876.28	272618.27	334212.82
7	876.05	272610.27	334212.83
8	876.02	272605.27	334212.84
9	875.93	272589.27	334212.87
10	875.90	272584.27	334212.88
11	875.89	272576.27	334212.89
12	875.76	272576.28	334217.80
13	875.82	272584.28	334217.88
14	875.85	272584.30	334223.40
15	875.77	272589.29	334223.29
16	875.85	272589.28	334217.87

ROUTE 7 STP107-PLATT - SECOND			
POINT NO.	ELEVATION	Y	X
1	801.48	281230.54	336132.54
2	801.41	281225.71	336132.70
3	801.39	281225.88	336137.87
4	801.47	281230.72	336137.62
5	801.66	281231.19	336145.52
6	801.67	281235.98	336145.36
7	801.54	281235.71	336137.36
8	801.56	281235.54	336132.37

ROUTE 7 STP107-PLATT - SECOND			
POINT NO.	ELEVATION	Y	X
9	801.60	281235.00	336116.38
10	801.62	281234.85	336111.38
11	801.86	281234.58	336103.38
12	801.94	281229.70	336103.55
13	801.54	281229.85	336111.55
14	801.47	281225.03	336111.71
15	801.46	281225.19	336116.71
16	801.53	281230.02	336116.55

ROUTE 8 STP112-TRUAX - GARDEN			
POINT NO.	ELEVATION	Y	X
1	878.24	286583.57	332928.24
2	878.13	286577.26	332923.89
3	878.10	286574.43	332928.01
4	878.21	286580.73	332932.36
5	878.71	286573.93	332942.25
6	878.88	286578.02	332945.06
7	878.35	286584.85	332935.20
8	878.32	286587.67	332931.08
9	878.42	286596.71	332917.95
10	878.45	286599.61	332913.79
11	878.82	286604.01	332907.42
12	878.76	286599.91	332904.62
13	878.37	286595.50	332910.96
14	878.26	286589.17	332906.59
15	878.23	286586.33	332910.71
16	878.34	286592.66	332915.07

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON
SHOULDER OR PARKING LANE, UNDIVIDED
ROADWAY AND LANE CLOSURE WITH FLAGGING
OPERATION"

ROUTE 8 STOP 112
TRUAX BLVD & GARDEN STREET / JEFFERS ROAD

- NOTES:
1. CONTRACTOR TO FIELD VERIFY GRADE, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO RAMP AND SIDEWALK CONSTRUCTION.
 2. ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION.
 3. SIDEWALK CLOSURE TO FOLLOW SDD FOR TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION.

ROUTE 8 STP117-FOLSOM - ARNSTAD			
POINT NO.	ELEVATION	Y	X
1	904.27	285065.52	331239.55
2	904.16	285057.70	331239.54
3	904.17	285057.75	331244.54
4	904.29	285065.52	331244.55
5	904.63	285065.89	331252.53
6	904.72	285070.52	331252.55
7	904.36	285070.52	331244.55
8	904.35	285070.52	331239.53
9	904.43	285070.52	331223.55
10	904.47	285070.52	331218.55
11	904.89	285070.53	331209.53
12	904.86	285065.63	331209.54
13	904.39	285065.52	331218.55
14	904.27	285057.62	331218.54
15	904.24	285057.63	331223.54
16	904.36	285065.52	331223.55

ROUTE 8 STOP 117
FOLSOM STREET & ARNSTAD PLACE

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR LANE
CLOSURE AND LANE CLOSURE WITH FLAGGING
OPERATION"

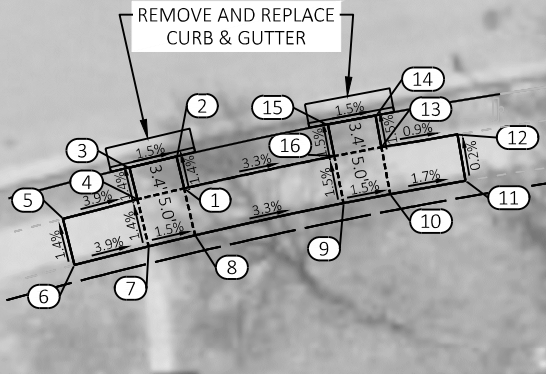
ROUTE 8 STP113-TRUAX - JEFFERS			
POINT NO.	ELEVATION	Y	X
1	881.57	287005.55	332313.09
2	881.46	286999.55	332309.00
3	881.42	286996.73	332313.12
4	881.53	287002.72	332317.22
5	881.61	286998.31	332323.90
6	881.69	287002.33	332326.65
7	881.60	287006.85	332320.05
8	881.64	287009.67	332315.92
9	881.76	287018.71	332302.72
10	881.82	287021.53	332298.59
11	882.13	287026.05	332291.99
12	882.04	287022.04	332289.25
13	881.74	287017.40	332295.77
14	881.63	287011.42	332291.68
15	881.58	287008.60	332295.80
16	881.69	287014.58	332299.89

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON
SHOULDER OR PARKING LANE, UNDIVIDED
ROADWAY AND LANE CLOSURE WITH FLAGGING
OPERATION"

ROUTE 8 STOP 113
TRUAX BLVD & JEFFERS ROAD / 14 TH STREET

ROUTE 8 STP121-CEDAR - ELEVENTH			
POINT NO.	ELEVATION	Y	X
1	871.56	282555.16	332734.76
2	871.51	282558.36	332734.01
3	871.58	282557.21	332729.11
4	871.63	282554.01	332729.89
5	871.94	282551.99	332722.15
6	872.01	282547.17	332723.33
7	871.70	282549.12	332731.07
8	871.63	282550.27	332735.90

ROUTE 8 STP121-CEDAR - ELEVENTH			
POINT NO.	ELEVATION	Y	X
9	871.10	282553.60	332751.37
10	871.03	282554.54	332756.23
11	870.89	282555.94	332764.05
12	870.88	282560.77	332763.23
13	870.95	282559.45	332755.31
14	870.90	282562.64	332754.71
15	870.98	282561.70	332749.77
16	871.03	282558.50	332750.40



ROUTE 8 STOP 121
CEDAR STREET & ELEVENTH STREET

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON
SHOULDER OR PARKING LANE, UNDIVIDED
ROADWAY AND LANE CLOSURE WITH FLAGGING
OPERATION"

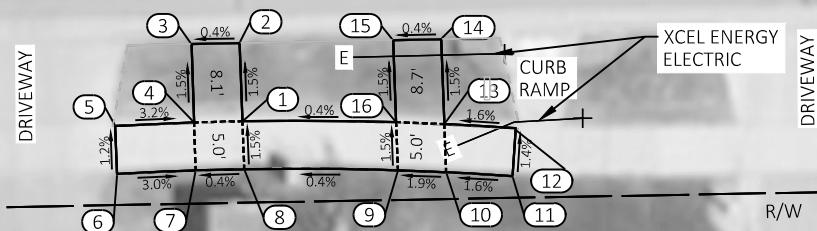
NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON
SHOULDER OR PARKING LANE, UNDIVIDED
ROADWAY AND LANE CLOSURE WITH FLAGGING
OPERATION"

ROUTE 8 STP124-SEVENTH - VINE			
POINT NO.	ELEVATION	Y	X
1	871.19	281608.74	334094.46
2	871.07	281608.80	334102.87
3	871.15	281613.80	334102.83
4	871.27	281613.74	334094.38
5	871.31	281621.35	334094.25
6	871.38	281621.30	334089.26
7	871.34	281613.69	334089.38
8	871.26	281608.69	334089.46
9	870.77	281592.69	334089.72
10	870.70	281587.69	334089.80
11	870.55	281579.69	334089.93
12	870.47	281579.74	334094.87
13	870.62	281587.74	334094.80
14	870.50	281587.82	334103.12
15	870.57	281592.82	334103.05
16	870.70	281592.74	334094.72



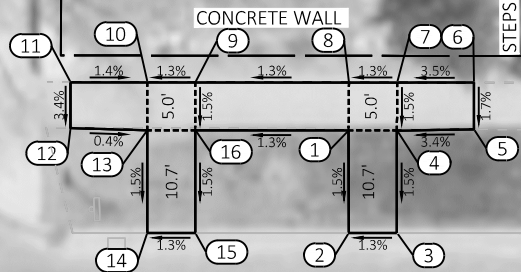
- NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR SINGLE RIGHT
LANE CLOSURE, UNDIVIDED
NON-FREEWAY/EXPRESSWAY AND LANE CLOSURE
WITH FLAGGING OPERATION"

ROUTE 9 STP129-W MACAUTHUR - RUTH			
POINT NO.	ELEVATION	Y	X
9	872.65	270574.34	335585.40
10	872.68	270574.13	335590.41
11	872.72	270573.73	335597.33
12	872.65	270578.73	335597.55
13	872.53	270579.16	335590.22
14	872.40	270588.01	335589.88
15	872.38	270587.91	335584.88
16	872.51	270579.35	335585.21



ROUTE 9 STOP 129
W. MAC ARTHUR AVE. & RUTH STREET

ROUTE 19 STP131-SIXTH - LAKE			
POINT NO.	ELEVATION	Y	X
1	788.66	277109.45	336194.54
2	788.50	277098.76	336194.57
3	788.56	277098.76	336199.57
4	788.72	277109.45	336199.58
5	788.99	277109.56	336207.58
6	789.08	277114.45	336207.58
7	788.80	277114.45	336199.58
8	788.73	277114.45	336194.58
9	788.53	277114.46	336178.58
10	788.47	277114.46	336173.58
11	788.58	277114.47	336165.58
12	788.42	277109.72	336165.58
13	788.39	277109.46	336173.58
14	788.23	277098.77	336173.57
15	788.30	277098.77	336178.57
16	788.46	277109.46	336178.58



NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION"

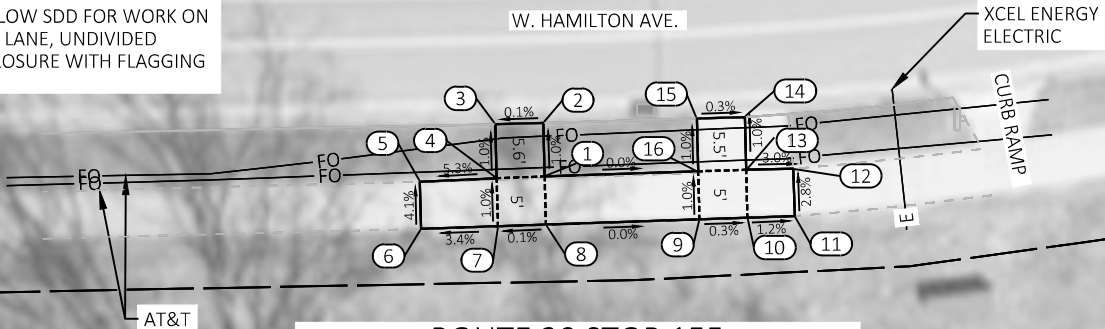
ROUTE 19 STOP 131
SIXTH AVE. & LAKE STREET

- NOTES:
1. CONTRACTOR TO FIELD VERIFY GRADE, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO RAMP AND SIDEWALK CONSTRUCTION.
 2. ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION.
 3. SIDEWALK CLOSURE TO FOLLOW SDD FOR TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION.

ROUTE 20 STP155-W HAMILTON - HIDDEN PLC			
POINT NO.	ELEVATION	Y	X
1	848.59	269118.96	333549.29
2	848.54	269124.54	333549.15
3	848.53	269124.41	333544.15
4	848.59	269118.79	333544.29
5	848.17	269118.42	333536.30
6	848.37	269113.52	333536.43
7	848.64	269113.77	333544.42
8	848.64	269113.95	333549.42

ROUTE 20 STP155-W HAMILTON - HIDDEN PLC			
POINT NO.	ELEVATION	Y	X
9	848.64	269114.49	333565.41
10	848.63	269114.67	333570.41
11	848.57	269114.86	333575.34
12	848.43	269119.78	333575.17
13	848.58	269119.67	333570.28
14	848.52	269125.15	333570.09
15	848.54	269125.00	333565.14
16	848.59	269119.36	333565.37

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION"

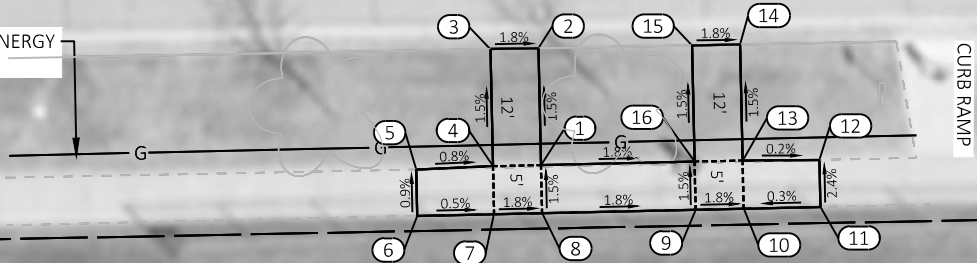


ROUTE 20 STOP 155
W. HAMILTON AVE. & HIDDEN PLACE

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION"

W. HAMILTON AVE.

XCEL ENERGY GAS



ROUTE 20 STOP 156
ELDORADO BLVD & W. HAMILTON AVE.

ROUTE 18 STP144-FENWICK - DONNELLAN			
POINT NO.	ELEVATION	Y	X
1	885.80	275835.57	346650.83
2	885.68	275843.59	346650.75
3	885.63	275843.54	346645.75
4	885.75	275835.53	346645.84
5	885.86	275835.29	346638.89
6	886.05	275830.48	346638.94
7	885.83	275830.55	346645.89
8	885.88	275830.57	346650.89
9	886.04	275830.70	346666.89
10	886.09	275830.74	346671.88
11	886.26	275830.80	346679.88
12	886.19	275835.62	346679.84
13	886.02	275835.74	346671.83
14	885.90	275843.76	346671.75
15	885.84	275843.72	346666.75
16	885.96	275835.70	346666.83

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION"

XCEL ENERGY GAS

DRIVEWAY

R/W

ROUTE 18 STOP 144
FENWICK AVE. & DONNELLAN LANE

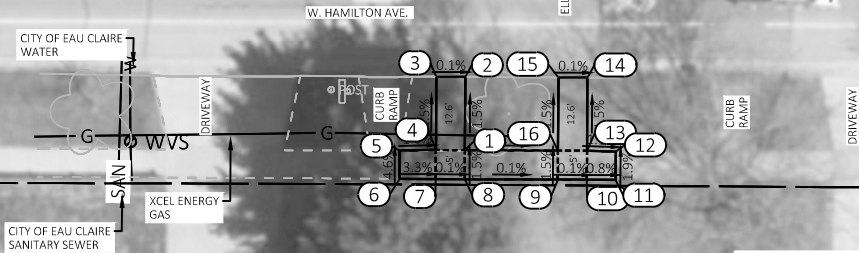
ROUTE 20 STP156-ELDORADO - W HAMILTON			
POINT NO.	ELEVATION	Y	X
1	876.79	269248.93	335325.03
2	876.61	269261.11	335324.75
3	876.70	269261.02	335319.76
4	876.88	269248.82	335320.03
5	876.95	269248.48	335312.03
6	876.99	269243.64	335312.15
7	876.96	269243.82	335320.14
8	876.87	269243.93	335325.14

ROUTE 20 STP156-ELDORADO - W HAMILTON			
POINT NO.	ELEVATION	Y	X
9	876.59	269244.29	335341.14
10	876.50	269244.41	335346.13
11	876.52	269244.59	335354.14
12	876.41	269249.47	335354.02
13	876.42	269249.40	335346.03
14	876.24	269261.50	335345.75
15	876.33	269261.41	335340.75
16	876.51	269249.29	335341.03

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION"

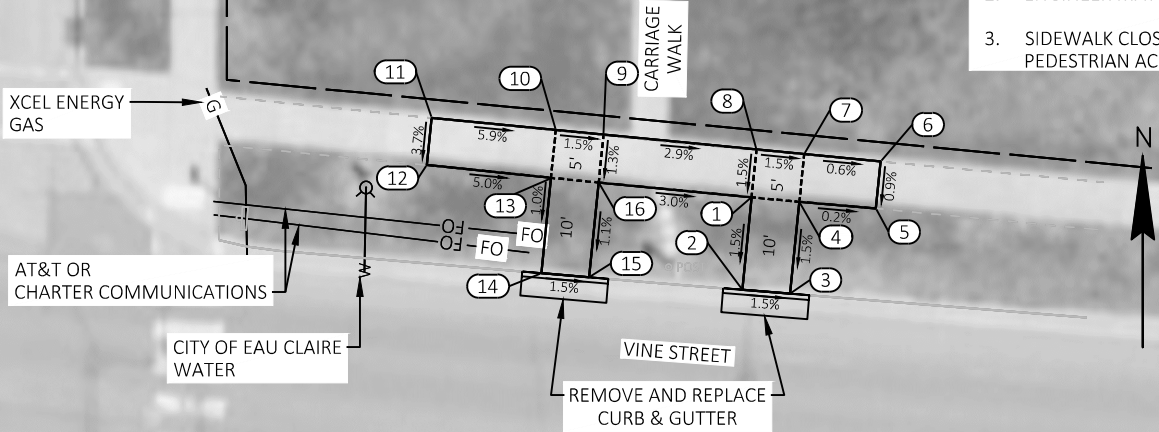
ROUTE 20 STP158-ELLIS - W HAMILTON			
POINT NO.	ELEVATION	Y	X
1	877.84	269271.43	338429.55
2	877.65	269284.02	338429.64
3	877.66	269284.05	338424.64
4	877.85	269271.46	338424.55
5	877.91	269271.32	338418.32
6	878.13	269266.49	338418.29
7	877.93	269266.46	338424.51
8	877.92	269266.43	338429.51

ROUTE 20 STP158-ELLIS - W HAMILTON			
POINT NO.	ELEVATION	Y	X
9	877.90	269266.34	338445.51
10	877.89	269266.31	338450.51
11	877.85	269266.31	338455.47
12	877.76	269271.13	338455.49
13	877.81	269271.31	338450.55
14	877.62	269283.90	338450.64
15	877.63	269283.93	338445.64
16	877.82	269271.34	338445.55



ROUTE 20 STOP 158
ELLIS STREET & W. HAMILTON AVE.

- NOTES:
1. CONTRACTOR TO FIELD VERIFY GRADE, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO RAMP AND SIDEWALK CONSTRUCTION.
 2. ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION.
 3. SIDEWALK CLOSURE TO FOLLOW SDD FOR TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION.



ROUTE 12 STP168- VINE - FOURTEENTH			
POINT NO.	ELEVATION	Y	X
1	903.50	281388.00	331909.76
2	903.36	281378.36	331908.78
3	903.28	281377.96	331913.77
4	903.43	281387.52	331914.73
5	903.42	281386.83	331922.71
6	903.46	281391.72	331923.20
7	903.51	281392.49	331915.24
8	903.58	281392.97	331910.26

ROUTE 12 STP168- VINE - FOURTEENTH			
POINT NO.	ELEVATION	Y	X
9	904.05	281394.51	331894.33
10	904.12	281394.99	331889.36
11	904.89	281396.27	331876.42
12	904.70	281391.36	331875.92
13	904.06	281390.02	331888.85
14	903.96	281380.09	331887.85
15	903.88	281379.67	331892.83
16	903.99	281389.54	331893.83

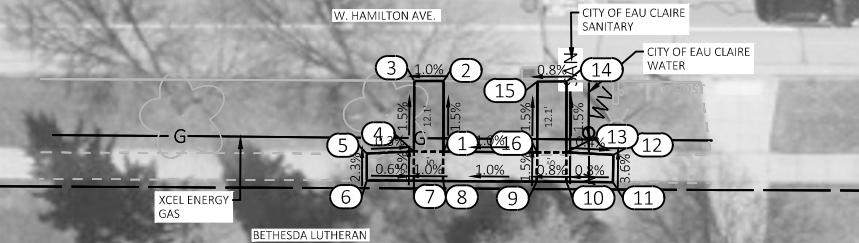
NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION"

ROUTE 12 STOP 168
VINE STREET & FOURTEENTH STREET

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION"

ROUTE 20 STP159- W HAMILTON - BETHESDA LUTH			
POINT NO.	ELEVATION	Y	X
1	882.93	269264.69	339830.62
2	882.75	269276.75	339830.68
3	882.69	269276.78	339825.68
4	882.88	269264.72	339825.62
5	882.90	269264.34	339817.61
6	883.00	269259.77	339817.59
7	882.95	269259.72	339825.59
8	883.00	269259.69	339830.59

ROUTE 20 STP159- W HAMILTON - BETHESDA LUTH			
POINT NO.	ELEVATION	Y	X
9	883.16	269259.60	339846.59
10	883.20	269259.57	339851.59
11	883.26	269259.52	339859.59
12	883.09	269264.13	339859.61
13	883.12	269264.57	339851.61
14	882.94	269276.71	339851.68
15	882.90	269276.66	339846.68
16	883.08	269264.60	339846.63



ROUTE 20 STOP 159
W. HAMILTON AVE. & BETHESDA LUTHERAN

ROUTE 12 STP169- VINE - FLORENCE			
POINT NO.	ELEVATION	Y	X
1	885.38	281372.77	330522.87
2	885.25	281372.69	330514.46
3	885.18	281367.69	330514.49
4	885.30	281367.77	330522.91
5	884.98	281359.77	330522.99
6	885.17	281359.82	330527.99
7	885.38	281367.82	330527.91
8	885.45	281372.81	330527.87
9	886.40	281388.81	330527.72
10	886.48	281393.81	330527.67
11	887.13	281401.81	330527.60
12	887.08	281401.76	330522.66
13	886.40	281393.77	330522.67
14	886.28	281393.69	330514.29
15	886.20	281388.69	330514.33
16	886.32	281388.77	330522.72

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY AND LANE CLOSURE WITH FLAGGING OPERATION"



ROUTE 12 STOP 169
VINE STREET & FLORENCE AVE.



NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON
SHOULDER OR PARKING LANE, UNDIVIDED
ROADWAY AND LANE CLOSURE WITH FLAGGING
OPERATION"

ROUTE 12 STP170- VINE - WARDEN			
POINT NO.	ELEVATION	Y	X
1	881.96	281517.25	329155.56
2	881.83	281517.22	329163.98
3	881.75	281522.22	329164.00
4	881.87	281522.25	329155.56
5	881.89	281530.25	329155.35
6	881.92	281530.26	329150.57
7	881.95	281522.26	329150.56
8	882.03	281517.26	329150.56
9	882.31	281501.26	329150.53
10	882.40	281496.26	329150.53
11	882.71	281488.26	329150.52
12	882.64	281488.25	329155.38
13	882.32	281496.25	329155.53
14	882.19	281496.22	329163.93
15	882.11	281501.22	329163.94
16	882.23	281501.25	329155.53

ROUTE 12 STP172- MERIDIAN - CRESTVIEW			
POINT NO.	ELEVATION	Y	X
1	860.20	278211.92	329224.17
2	860.07	278219.85	329220.99
3	860.13	278217.99	329216.35
4	860.25	278210.05	329219.54
5	860.37	278206.97	329212.15
6	860.48	278202.41	329213.98
7	860.33	278205.41	329221.40
8	860.28	278207.28	329226.03

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON
SHOULDER OR PARKING LANE, UNDIVIDED
ROADWAY AND LANE CLOSURE WITH FLAGGING
OPERATION"

ROUTE 12 STP172- MERIDIAN - CRESTVIEW			
POINT NO.	ELEVATION	Y	X
9	860.11	278213.28	329240.87
10	860.06	278215.15	329245.50
11	859.94	278218.15	329252.92
12	859.88	278222.74	329251.08
13	859.99	278219.79	329243.63
14	859.86	278227.67	329240.48
15	859.91	278225.81	329235.84
16	860.04	278217.92	329239.00

- NOTES:
1. CONTRACTOR TO FIELD VERIFY GRADE, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO RAMP AND SIDEWALK CONSTRUCTION.
 2. ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION.
 3. SIDEWALK CLOSURE TO FOLLOW SDD FOR TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION.

NOTE:
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON
SHOULDER OR PARKING LANE, UNDIVIDED
ROADWAY AND LANE CLOSURE WITH FLAGGING
OPERATION"

ROUTE 12 STP179- WESTOVER - ELLIS			
POINT NO.	ELEVATION	Y	X
1	874.03	268255.55	338155.21
2	873.91	268263.73	338155.23
3	873.93	268263.77	338150.23
4	874.05	268255.57	338150.21
5	874.17	268255.55	338142.20
6	874.22	268250.61	338142.17
7	874.13	268250.57	338150.17
8	874.10	268250.55	338155.17

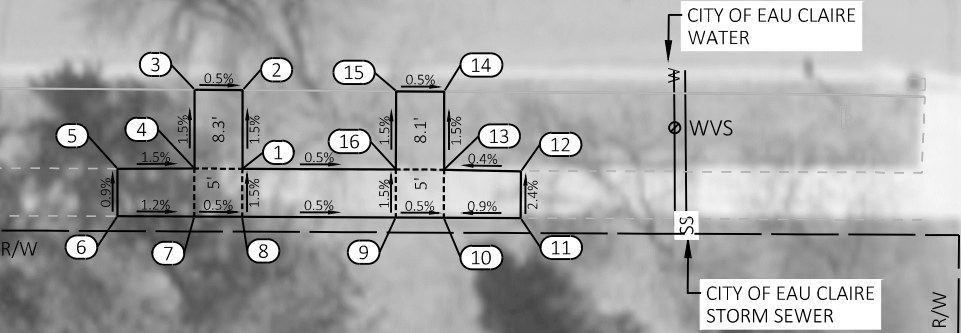
ROUTE 12 STP179- WESTOVER - ELLIS			
POINT NO.	ELEVATION	Y	X
9	874.02	268250.48	338171.17
10	874.00	268250.46	338176.17
11	874.07	268250.42	338184.17
12	873.95	268255.29	338184.21
13	873.92	268255.45	338176.21
14	873.80	268263.59	338176.23
15	873.83	268263.62	338171.23
16	873.95	268255.48	338171.20

NOTE:
"SDD FOR TRAFFIC CONTROL FOR LANE CLOSURE WITH
FLAGGING OPERATION TO BE FOLLOWED WHILE
WORK IS OCCURRING"
"LANE CLOSURE TO FOLLOW SDD FOR WORK ON
SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
AND LANE CLOSURE WITH FLAGGING OPERATION"

ROUTE 12 STP173- SUNSET - FAIRMONT			
POINT NO.	ELEVATION	Y	X
1	865.41	279038.19	328757.71
2	865.30	279030.82	328757.69
3	865.38	279030.81	328762.69
4	865.48	279038.17	328762.71
5	865.94	279038.40	328770.71
6	865.99	279043.15	328770.72
7	865.56	279043.17	328762.72
8	865.49	279043.19	328757.72

ROUTE 12 STP173- SUNSET - FAIRMONT			
POINT NO.	ELEVATION	Y	X
9	865.01	279043.23	328741.72
10	864.94	279043.24	328736.72
11	864.90	279043.26	328728.72
12	864.85	279038.47	328728.71
13	864.86	279038.24	328736.71
14	864.75	279030.87	328736.69
15	864.83	279030.86	328741.71
16	864.94	279038.23	328741.71

WESTOVER ROAD



ROUTE 12 STOP 179
WESTOVER ROAD & ELLIS STREET



TEMPORARY DETOUR DETAIL FOR ROUTE 3 STOP 30
SEYMOUR ROAD & WESTERN AVE.

Estimate Of Quantities

7995-02-67					
Line	Item	Item Description	Unit	Total	Qty
0002	204.0150	Removing Curb & Gutter	LF	126.000	126.000
0004	204.0155	Removing Concrete Sidewalk	SY	1,230.000	1,230.000
0006	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	126.000	126.000
0008	602.0405	Concrete Sidewalk 4-Inch	SF	14,970.000	14,970.000
0010	618.0100	Maintenance and Repair of Haul Roads (project) 01. 7995-02-67	EACH	1.000	1.000
0012	619.1000	Mobilization	EACH	1.000	1.000
0014	628.1905	Mobilizations Erosion Control	EACH	54.000	54.000
0016	628.1910	Mobilizations Emergency Erosion Control	EACH	3.000	3.000
0018	628.7015	Inlet Protection Type C	EACH	53.000	53.000
0020	643.0300	Traffic Control Drums	DAY	4,020.000	4,020.000
0022	643.0420	Traffic Control Barricades Type III	DAY	120.000	120.000
0024	643.0705	Traffic Control Warning Lights Type A	DAY	230.000	230.000
0026	643.0715	Traffic Control Warning Lights Type C	DAY	860.000	860.000
0028	643.0800	Traffic Control Arrow Boards	DAY	120.000	120.000
0030	643.0900	Traffic Control Signs	DAY	7,730.000	7,730.000
0032	643.3180	Temporary Marking Line Removable Tape 6-Inch	LF	2,430.000	2,430.000
0034	643.5000	Traffic Control	EACH	1.000	1.000
0036	644.1440	Temporary Pedestrian Surface Matting	SF	1,440.000	1,440.000
0038	644.1601	Temporary Pedestrian Curb Ramp	DAY	170.000	170.000
0040	644.1810	Temporary Pedestrian Barricade	LF	3,900.000	3,900.000
0042	646.9002	Marking Removal Line 6-Inch	LF	2,430.000	2,430.000
0044	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	126.000	126.000
0046	650.9500	Construction Staking Sidewalk (project) 01. 7995-02-67	EACH	1.000	1.000
0048	650.9911	Construction Staking Supplemental Control (project) 01. 7995-02-67	EACH	1.000	1.000
0050	690.0250	Sawing Concrete	LF	690.000	690.000
0052	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	300.000	300.000
0054	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	300.000	300.000
0056	SPV.0060	Special 01. Grading, Shaping and Finishing Transit Stops	EACH	53.000	53.000

			204.0150	601.0411	650.5500
			REMOVING CURB & GUTTER	CONCRETE CURB & GUTTER 30- INCH TYPE D	CONSTRUCTION STAKING CURB GUTTER AND CURB
CATEGORY	ROUTE	STOP	LF	LF	LF
0010	5	81	18	18	18
0010	6	92	18	18	18
0010	8	121	18	18	18
0010	8	124	18	18	18
0010	12	168	18	18	18
0010	12	169	18	18	18
0010	12	173	18	18	18
TOTAL 0010			126	126	126

			204.0155	602.0405	690.0250
			REMOVING CONCRETE SIDEWALK	CONCRETE SIDEWALK 4- INCH	SAWING CONCRETE
CATEGORY	ROUTE	STOP	SY	SF	LF
0010	1	3	28	278	10
0010	1	8	23	248	10
0010	1	9	22	237	10
0010	1	11	23	294	10
0010	1	14	20	222	10
0010	1	18	23	262	10
0010	3	27	23	263	10
0010	3	30	24	288	10
0010	3	36	22	266	10
0010	3	37	23	263	10
0010	3	39	20	262	10
0010	3	40	23	292	10
0010	3	46	23	258	10
0010	4	66	20	233	10
0010	4	68	22	267	10
0010	4	71	23	276	10
0010	5	80	23	264	10
0010	5	81	23	330	32
0010	5	82	23	263	10
0010	5	83	23	264	10
0010	6	84	23	309	10
0010	6	86	23	337	10
0010	6	88	23	340	10
0010	6	89	23	263	10
0010	6	91	23	271	10
0010	6	92	23	285	32
0010	6	93	23	275	10
0010	6	97	26	324	10
0010	6	98	24	303	10
0010	6	99	23	287	10

NOTE: TABLE CONTINUED TO RIGHT

			204.0155	602.0405	690.0250
			REMOVING CONCRETE SIDEWALK	CONCRETE SIDEWALK 4- INCH	SAWING CONCRETE
CATEGORY	ROUTE	STOP	SY	SF	LF
0010	7	102	21	241	10
0010	7	103	23	264	10
0010	8	107	23	257	10
0010	8	112	25	305	10
0010	8	113	23	281	10
0010	8	117	24	292	10
0010	8	121	23	243	32
0010	8	122	23	300	10
0010	8	124	23	292	32
0010	9	128	21	275	10
0010	9	129	23	293	10
0010	19	131	23	315	10
0010	18	144	23	284	10
0010	20	155	22	249	10
0010	20	156	23	330	10
0010	20	158	21	311	10
0010	20	159	23	328	10
0010	12	168	26	332	32
0010	12	169	23	294	32
0010	12	170	23	293	10
0010	12	172	23	295	10
0010	12	173	23	282	32
0010	12	179	23	291	10
TOTAL 0010			1,230	14,970	690

NOTE: TABLE BEGINS TO LEFT

			*	*	*		*	*	*	
			205.0100	625.0100	628.2008	628.7015	629.0210	630.0140	630.0500	SPV.0060.01
										SPECIAL (01.
										GRADING,
										SHAPING &
										FINISHING
			EXCAVATION		EROSION MAT	INLET		SEEDING		TRANSIT
			COMMON	TOPSOIL	URBAN CLASS I	PROTECTION	FERTILIZER	MIXTURE NO.	SEED WATER	STOPS)
			CY	SY	SY	TYPE C	TYPE B	40	MGAL	EACH
						EACH	CWT	LB		
0010	1	3	5.2	32.0	32.0	1	0.02	1.44	0.54	1
0010	1	8	4.6	18.4	18.4	1	0.01	0.83	0.31	1
0010	1	9	4.4	18.4	18.4	1	0.01	0.83	0.31	1
0010	1	11	5.4	34.7	34.7	1	0.02	1.56	0.58	1
0010	1	14	4.1	16.6	16.6	1	0.01	0.75	0.28	1
0010	1	18	4.9	23.5	23.5	1	0.01	1.06	0.40	1
0010	3	27	4.9	24.1	24.1	1	0.02	1.08	0.41	1
0010	3	30	5.3	33.6	33.6	1	0.02	1.51	0.57	1
0010	3	36	4.9	28.0	28.0	1	0.02	1.26	0.47	1
0010	3	37	4.9	23.9	23.9	1	0.02	1.08	0.40	1
0010	3	39	4.9	28.0	28.0	1	0.02	1.26	0.47	1
0010	3	40	5.4	34.7	34.7	1	0.02	1.56	0.58	1
0010	3	46	4.8	22.0	22.0	1	0.01	0.99	0.37	1
0010	4	66	4.3	19.7	19.7	1	0.01	0.89	0.33	1
0010	4	68	4.9	28.6	28.6	1	0.02	1.29	0.48	1
0010	4	71	5.1	28.1	28.1	1	0.02	1.26	0.47	1
0010	5	80	4.9	24.0	24.0	1	0.02	1.08	0.40	1
0010	5	81	6.1	47.8	47.8	1	0.03	2.15	0.80	1
0010	5	82	4.9	24.1	24.1	1	0.02	1.09	0.41	1
0010	5	83	4.9	24.3	24.3	1	0.02	1.09	0.41	1
0010	6	84	5.7	40.1	40.1	1	0.03	1.81	0.68	1
0010	6	86	6.2	49.7	49.7	1	0.03	2.24	0.84	1
0010	6	88	6.3	45.8	45.8	1	0.03	2.06	0.77	1
0010	6	89	4.9	23.8	23.8	1	0.02	1.07	0.40	1
0010	6	91	5.0	26.4	26.4	1	0.02	1.19	0.44	1
0010	6	92	5.3	31.2	31.2	1	0.02	1.41	0.53	1
0010	6	93	5.1	26.6	26.6	1	0.02	1.20	0.45	1
0010	6	97	6.0	44.1	44.1	1	0.03	1.99	0.74	1
0010	6	98	5.6	36.6	36.6	1	0.02	1.65	0.62	1
0010	6	99	5.3	32.8	32.8	1	0.02	1.48	0.55	1

* STARRED ITEMS ARE INCIDENTAL TO ITEM SPV.0060.01 AND ARE FOR INFORMATION ONLY.
NOTE: TABLE CONTINUED ON THE NEXT PAGE

			*	*	*		*	*	*	
			205.0100	625.0100	628.2008	628.7015	629.0210	630.0140	630.0500	SPV.0060.01
										SPECIAL (01.
										GRADING,
										SHAPING &
										FINISHING
			EXCAVATION		EROSION MAT	INLET		SEEDING		TRANSIT
			COMMON	TOPSOIL	URBAN CLASS I	PROTECTION	FERTILIZER	MIXTURE	SEED	STOPS)
			CY	SY	TYPE B	TYPE C	TYPE B	NO. 40	WATER	EACH
CATEGORY	ROUTE	STOP				EACH	CWT	LB	MGAL	
0010	7	102	4.5	19.9	19.9	1	0.01	0.89	0.33	1
0010	7	103	4.9	24.1	24.1	1	0.02	1.09	0.41	1
0010	8	107	4.8	22.1	22.1	1	0.01	1.00	0.37	1
0010	8	112	5.6	35.5	35.5	1	0.02	1.60	0.60	1
0010	8	113	5.2	30.6	30.6	1	0.02	1.38	0.51	1
0010	8	117	5.4	34.0	34.0	1	0.02	1.53	0.57	1
0010	8	121	4.5	16.8	16.8	1	0.01	0.75	0.28	1
0010	8	122	5.5	37.2	37.2	1	0.02	1.67	0.63	1
0010	8	124	5.4	34.2	34.2	1	0.02	1.54	0.58	1
0010	9	128	5.1	31.4	31.4	1	0.02	1.41	0.53	1
0010	9	129	5.4	32.2	32.2	1	0.02	1.45	0.54	1
0010	19	131	5.8	42.8	42.8	1	0.03	1.93	0.72	1
0010	18	144	5.3	32.3	32.3	1	0.02	1.45	0.54	1
0010	20	155	4.6	22.6	22.6	1	0.01	1.01	0.38	1
0010	20	156	6.1	48.1	48.1	1	0.03	2.17	0.81	1
0010	20	158	5.8	42.3	42.3	1	0.03	1.90	0.71	1
0010	20	159	6.1	47.9	47.9	1	0.03	2.16	0.81	1
0010	12	168	6.2	46.2	46.2	1	0.03	2.08	0.78	1
0010	12	169	5.4	34.7	34.7	1	0.02	1.56	0.58	1
0010	12	170	5.4	34.7	34.7	1	0.02	1.56	0.58	1
0010	12	172	5.5	35.1	35.1	1	0.02	1.58	0.59	1
0010	12	173	5.2	31.0	31.0	1	0.02	1.40	0.52	1
0010	12	179	5.4	33.9	33.9	1	0.02	1.53	0.57	1
TOTAL 0010			280	1,670	1,670	53	1.1	80	30	53

				643.0300	643.0420	643.0705	643.0715	643.0800	643.0900	643.3180	644.1440	644.1601	644.1810	646.9002		
				TRAFFIC CONTROL	TRAFFIC CONTROL	TRAFFIC CONTROL	TRAFFIC CONTROL	TRAFFIC CONTROL	TRAFFIC CONTROL	TEMPORARY	TEMPORARY	TEMPORARY	TEMPORARY	MARKING		
				DRUMS	BARRICADES TYPE	WARNING LIGHTS	WARNING LIGHTS	ARROW BOARDS	SIGNS	MARKING LINE	PEDESTRIAN	PEDESTRIAN CURB	PEDESTRIAN	REMOVAL LINE 6-		
				DAY	III	TYPE A	TYPE C	DAY	DAY	6-INCH	SURFACE MATTING	RAMP	BARRICADE	INCH		
CATEGORY	ROUTE	STOP	DURATION	DAY	DAY	DAY	DAY	DAY	DAY	LF	SF	DAY	LF	LF	REMARKS	
0010	1	3	14	308	14	28	168	14	266	600	0	0	48	600	Sidewalk detour	
0010	1	8	14	280	14	28	70	14	238	125	0	0	48	125	Sidewalk detour	
0010	1	9	14	448	14	28	70	14	126	125	144	28	258	125	Sidewalk diversion	
0010	1	11	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	1	14	14	179	0	0	0	0	33	0	575	0	138	0	Sidewalk diversion	
0010	1	18	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	3	27	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	3	30	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	3	36	14	179	0	0	0	0	33	0	144	28	258	0	Sidewalk diversion	
0010	3	37	14	179	0	0	0	0	33	0	144	28	258	0	Sidewalk diversion	
0010	3	39	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	3	40	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	3	46	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	4	66	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	4	68	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	4	71	14	308	14	28	168	14	266	600	0	0	48	600	Sidewalk detour	
0010	5	80	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	5	81	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	5	82	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	5	83	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	6	84	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	6	86	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	6	88	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	6	89	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	6	91	14	448	14	28	70	14	126	125	144	28	258	125	Sidewalk diversion	
0010	6	92	14	179	0	0	0	0	33	0	144	28	258	0	Sidewalk diversion	
0010	6	93	14	448	14	28	70	14	126	125	144	28	258	125	Sidewalk diversion	
0010	6	97	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	6	98	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	
0010	6	99	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour	

NOTE: TABLE CONTINUED ON THE NEXT PAGE

				643.0300	643.0420	643.0705	643.0715	643.0800	643.0900	643.3180	644.1440	644.1601	644.1810	646.9002	REMARKS
				TRAFFIC CONTROL DRUMS	TRAFFIC CONTROL BARRICADES TYPE III	TRAFFIC CONTROL WARNING LIGHTS TYPE A	TRAFFIC CONTROL WARNING LIGHTS TYPE C	TRAFFIC CONTROL ARROW BOARDS	TRAFFIC CONTROL SIGNS	TEMPORARY MARKING LINE REMOVABLE TAPE 6-INCH	TEMPORARY PEDESTRIAN SURFACE MATTING	TEMPORARY PEDESTRIAN CURB RAMP	TEMPORARY PEDESTRIAN BARRICADE	MARKING REMOVAL LINE 6- INCH	
CATEGORY	ROUTE	STOP	DURATION DAY	DAY	DAY	DAY	DAY	DAY	DAY	LF	SF	DAY	LF	LF	
0010	7	102	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	7	103	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	8	107	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	8	112	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	8	113	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	8	117	14	308	14	28	168	14	266	600	0	0	48	600	Sidewalk detour
0010	8	121	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	8	122	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	8	124	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	9	128	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	9	129	14	300	14	28	70	14	238	125	0	0	48	125	Sidewalk detour
0010	19	131	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	18	144	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	20	155	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	20	156	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	20	158	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	20	159	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	12	168	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	12	169	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	12	170	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	12	172	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	12	173	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
0010	12	179	14	11	0	0	0	0	145	0	0	0	48	0	Sidewalk detour
TOTAL 0010				4,020	120	230	860	120	7,730	2,430	1,440	170	3,900	2,430	

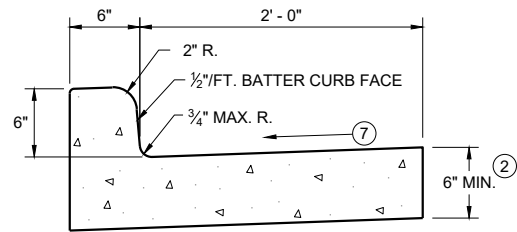
NOTE: TABLE BEGINS ON PREVIOUS PAGE

		628.1905	628.1910
		MOBILIZATIONS EROSION CONTROL EACH	MOBILIZATIONS EMERGENCY EROSION CONTROL EACH
CATEGORY	LOCATION		
0010	Project	53	3
0010			
TOTAL 0010		53	3

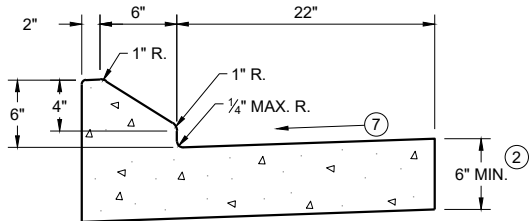
		650.9500.01	650.9911.01
		CONSTRUCTION STAKING SIDEWALK (PROJECT) (01. 7995-02-67) EACH	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (01. 7995-02-67) EACH
CATEGORY	LOCATION		
0010	Project	1	1
0010			
TOTAL 0010		1	1

Standard Detail Drawing List

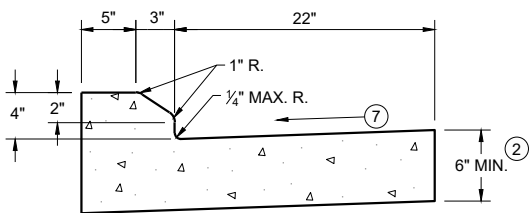
08D01-23A	CONCRETE CURB & GUTTER
08D01-23B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08E10-02	INLET PROTECTION TYPE A, B, C AND D
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15D12-14A	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE
15D20-08A	TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY
15D20-08B	TRAFFIC CONTROL, SINGLE RIGHT LANE CLOSURE, UNDIVIDED NON-FREEWAY/EXPRESSWAY
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D30-09F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09G	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09K	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION



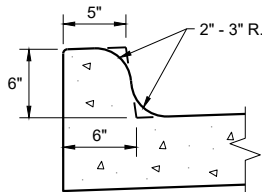
TYPES A^① & D



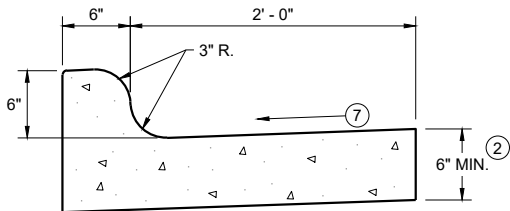
6" SLOPED CURB TYPES G^① & J



4" SLOPED CURB TYPES G^① & J

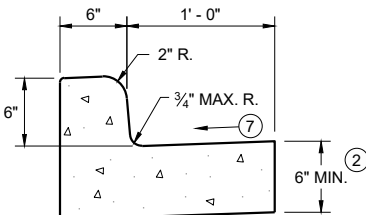


TYPES K^① & L
(OPTIONAL CURB SHAPE)



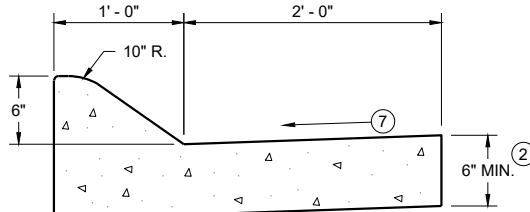
TYPES K^① & L

CONCRETE CURB AND GUTTER 30"

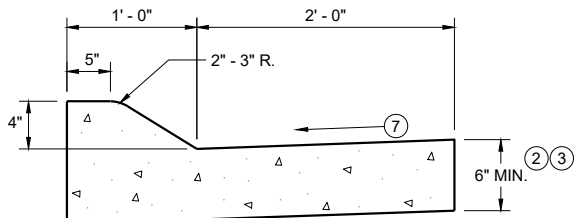


TYPES A^① & D

CONCRETE CURB AND GUTTER 18"

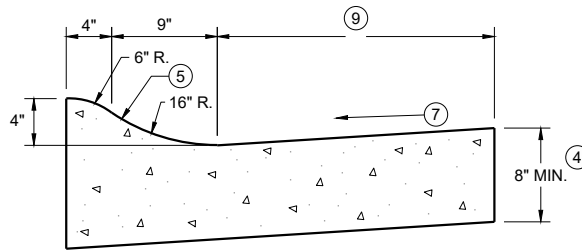


6" SLOPED CURB TYPES A^① & D



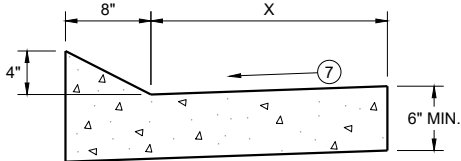
4" SLOPED CURB TYPES A^① & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R^① & T

TBT & TBTT	X
30"	22"
36"	28"

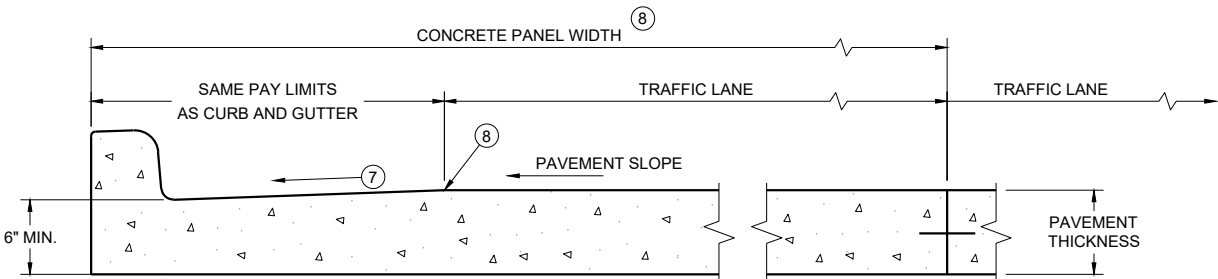


TYPES TBT & TBTT^①

CONCRETE CURB AND GUTTER

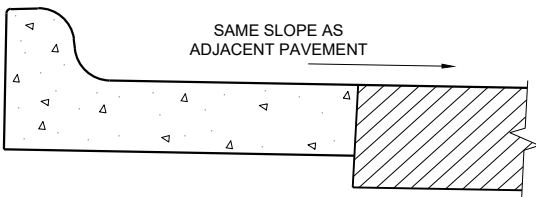
PAVEMENT THICKNESS
AND MAXIMUM CONCRETE
PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT
WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER^⑥
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

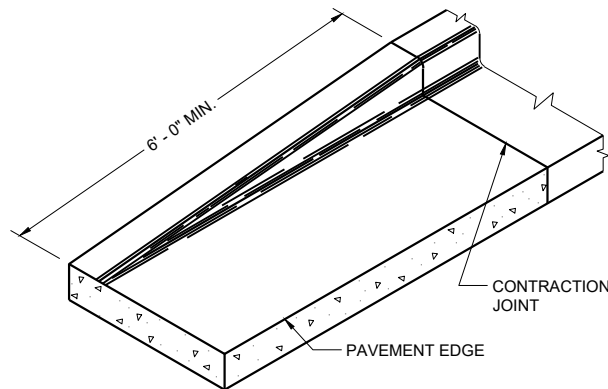
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

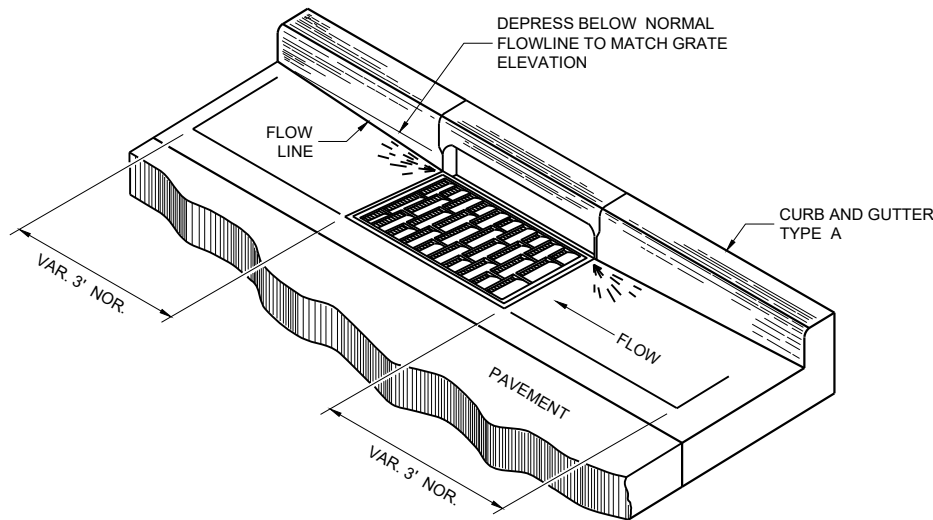
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES

CONCRETE CURB AND GUTTER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

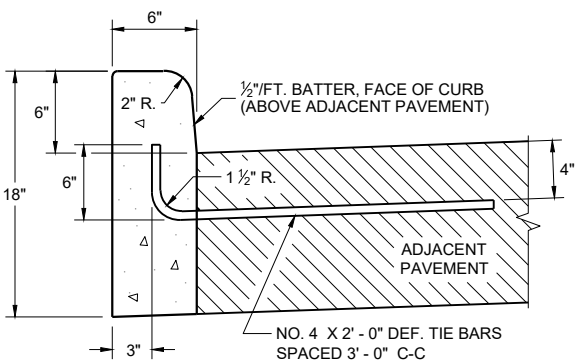


END SECTION CURB AND GUTTER

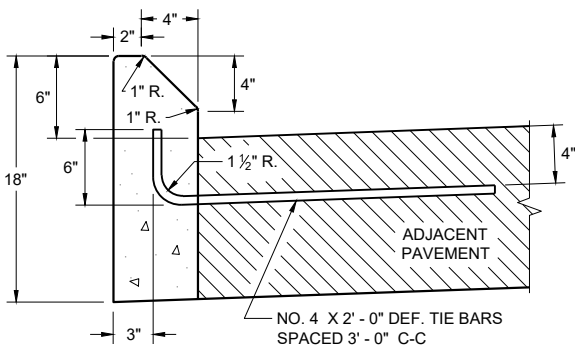


DETAIL OF CURB AND GUTTER AT INLETS

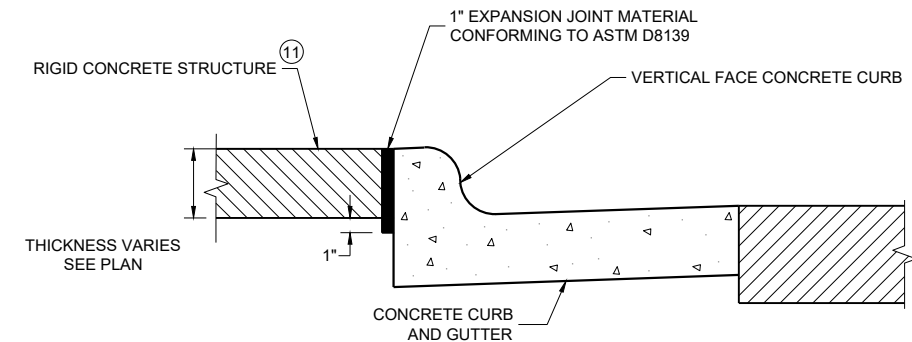
(TYPICAL H INLET COVER SHOWN)



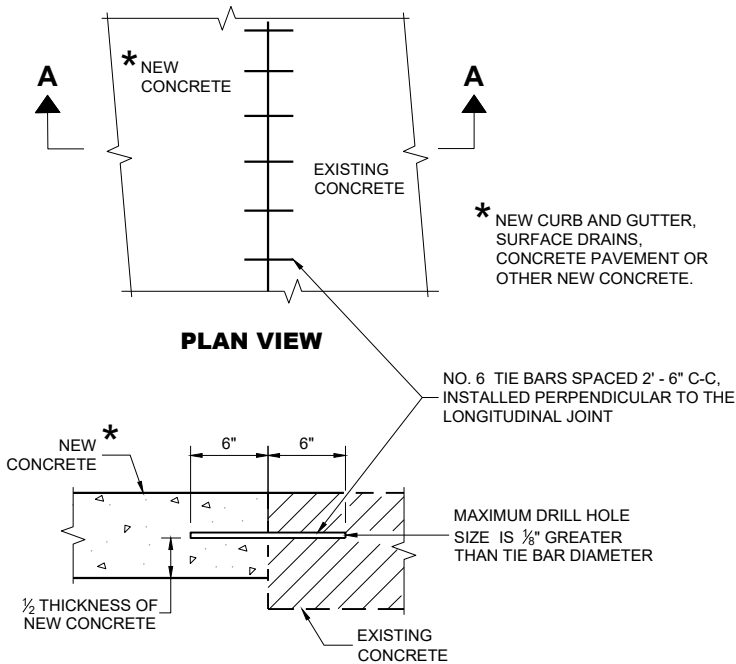
TYPES A^① & D



TYPES G^① & J
CONCRETE CURB



EXPANSION JOINT DETAIL FOR VERTICAL CURB ABUTTING A RIGID STRUCTURE^⑪



SECTION A - A
TIE BARS DRILLED INTO EXISTING PAVEMENT

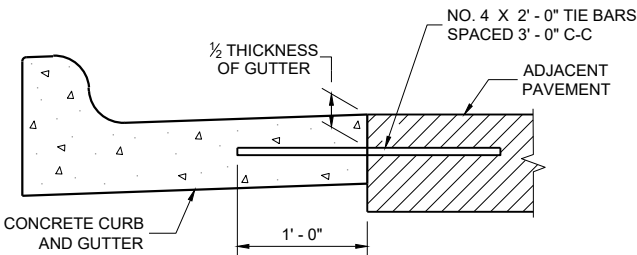
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

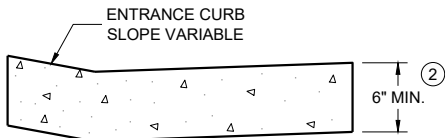
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑩ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.
- ⑪ PLACE 1" THICK EXPANSION JOINT MATERIAL BETWEEN VERTICAL FACE CURB TYPES EXTENDING FROM THE TOP OF CURB TO 1 INCH BELOW THE ADJOINING CONCRETE SURFACE. RIGID CONCRETE STRUCTURES INCLUDE RAISED CONCRETE MEDIANS, CONCRETE SAFETY ISLANDS, SPLITTER ISLANDS, OR LOCATIONS IDENTIFIED ON THE PLANS.



TYPICAL TIE BAR LOCATION^①

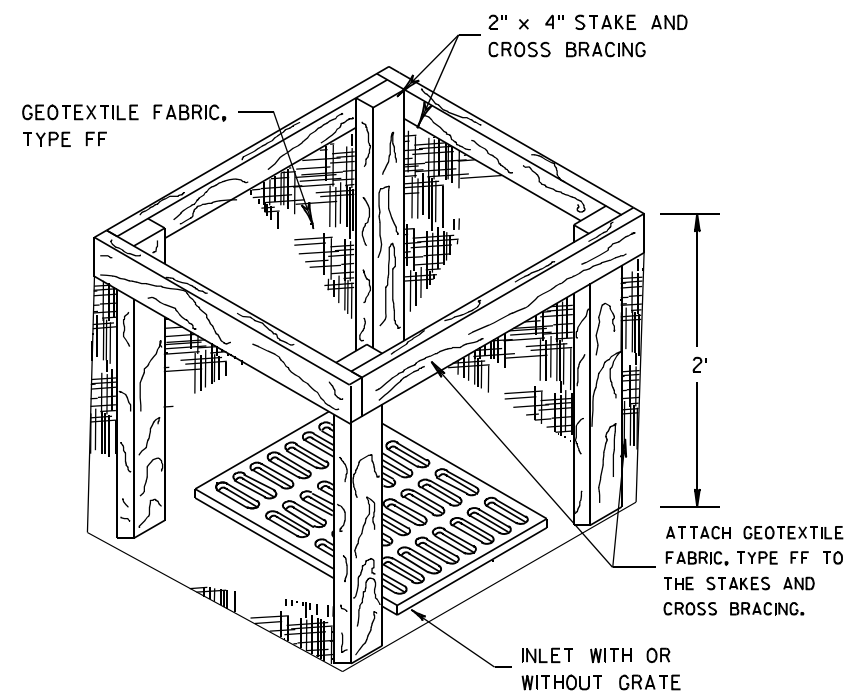
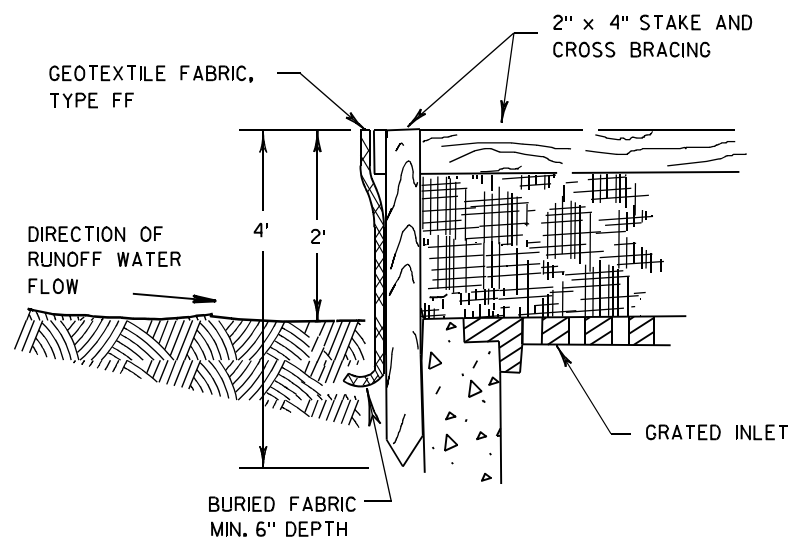


DRIVEWAY ENTRANCE CURB^⑩
(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES
AND CURB AND GUTTER
APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR



INLET PROTECTION, TYPE A

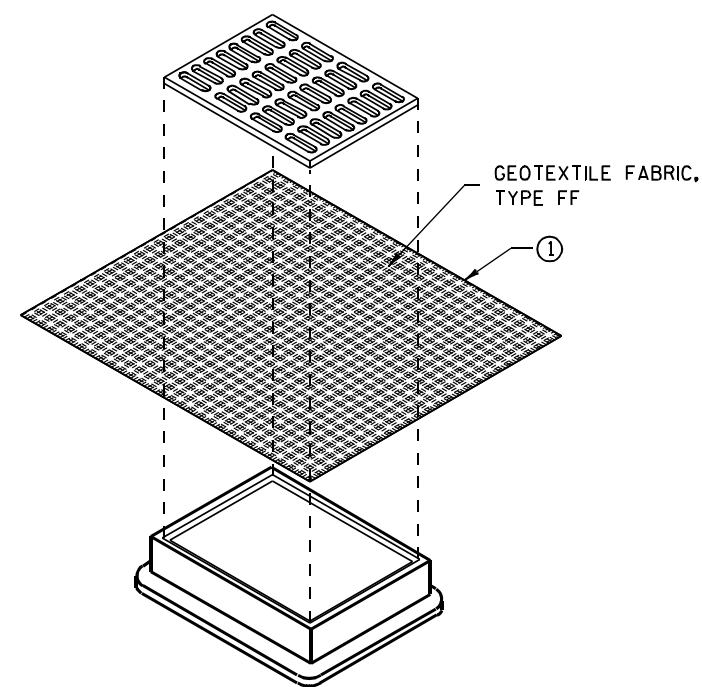
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

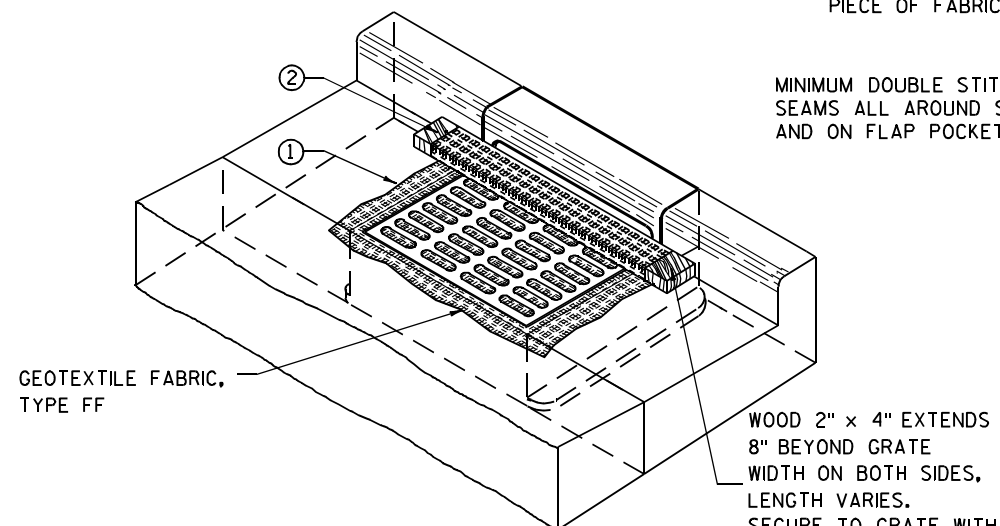
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

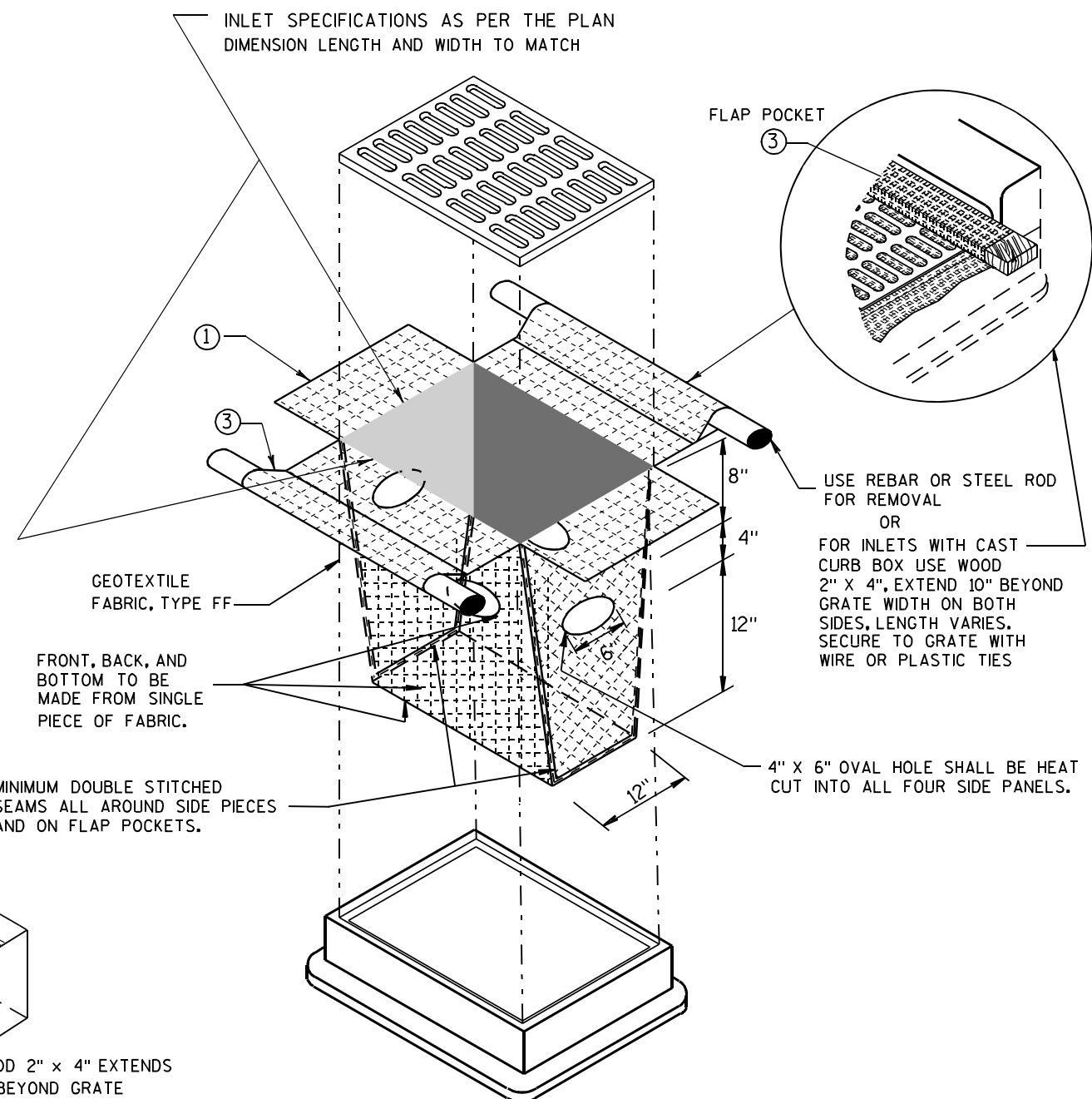
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLower THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

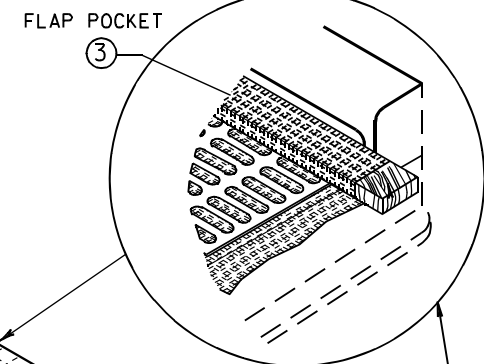
TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)



USE REBAR OR STEEL ROD FOR REMOVAL OR
FOR INLETS WITH CAST CURB BOX USE WOOD 2" X 4", EXTEND 10" BEYOND GRATE WIDTH ON BOTH SIDES, LENGTH VARIES. SECURE TO GRATE WITH WIRE OR PLASTIC TIES

4" X 6" OVAL HOLE SHALL BE HEAT CUT INTO ALL FOUR SIDE PANELS.

MINIMUM DOUBLE STITCHED SEAMS ALL AROUND SIDE PIECES AND ON FLAP POCKETS.

FRONT, BACK, AND BOTTOM TO BE MADE FROM SINGLE PIECE OF FABRIC.






WOOD 2" X 4" EXTENDS 8" BEYOND GRATE WIDTH ON BOTH SIDES, LENGTH VARIES. SECURE TO GRATE WITH WIRE OR PLASTIC TIES

**INLET PROTECTION
TYPE A, B, C, AND D**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02 /S/ Beth Cannestra
DATE
FHWA CHIEF ROADWAY DEVELOPMENT ENGINEER

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

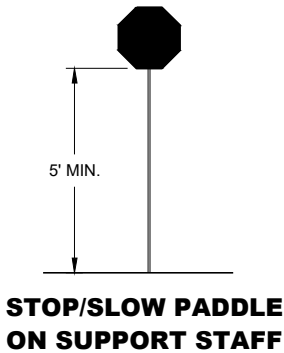
FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

- UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER
- ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.
- INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.
- PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.
- DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

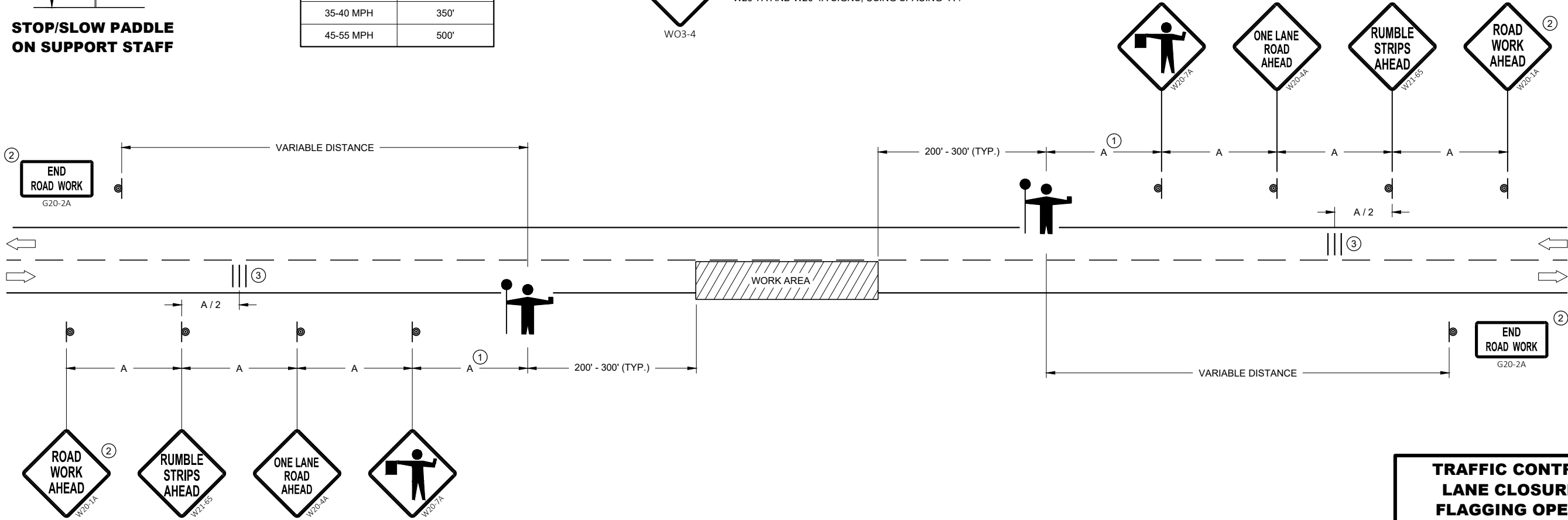


SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

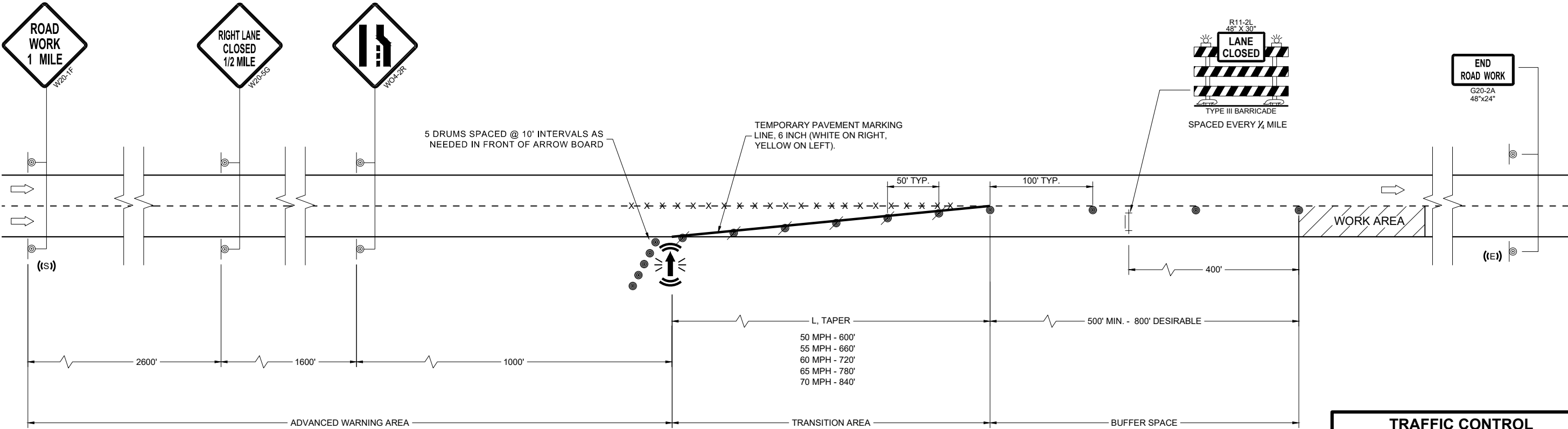
ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKINGS
- DIRECTION OF TRAFFIC
- WORK AREA
- CONNECTED ARROW BOARD
- WZ START LOCATION MARKER
- WZ END LOCATION MARKER

6



SDD 15D12-14a

TRAFFIC CONTROL LANE CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2024 DATE	/S/ Andrew Heidtke ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	

6

SDD 15D12-14a

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKINGS
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- WORK AREA

GENERAL NOTES

FOR WORK ON ROADWAYS WITH SPEEDS GREATER THAN 45MPH, USE SDD 15D12.

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN

PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

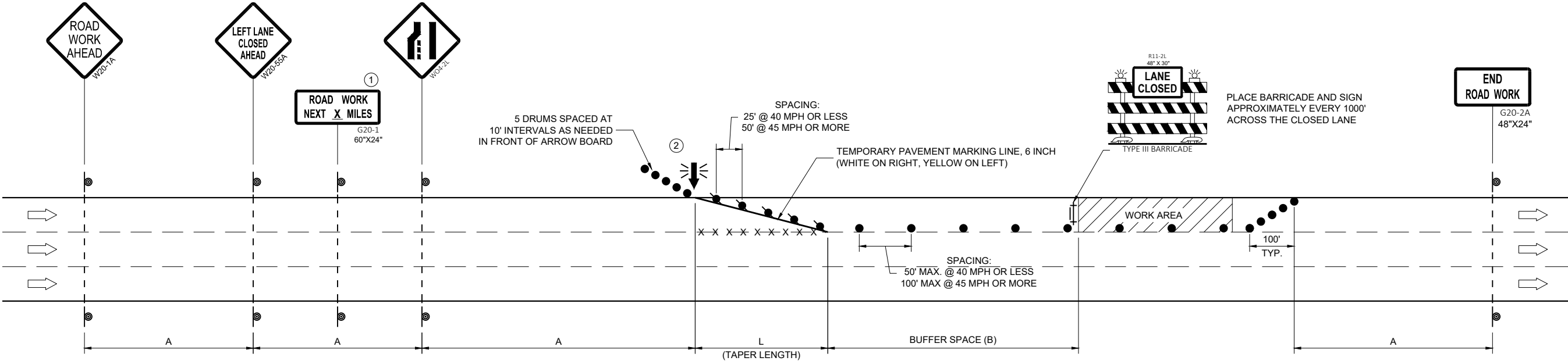
CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

- 1 OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.
- 2 WHERE THE SHOULDER OR TERRACE HAS INSUFFICIENT SPACE TO PLACE THE ARROW BOARD AS SHOWN, PLACE THE ARROW BOARD AT THE END OF THE TAPER.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'

TRAFFIC CONTROL, SINGLE
LANE CLOSURE, DIVIDED
NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2024 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKINGS
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- WORK AREA

GENERAL NOTES

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

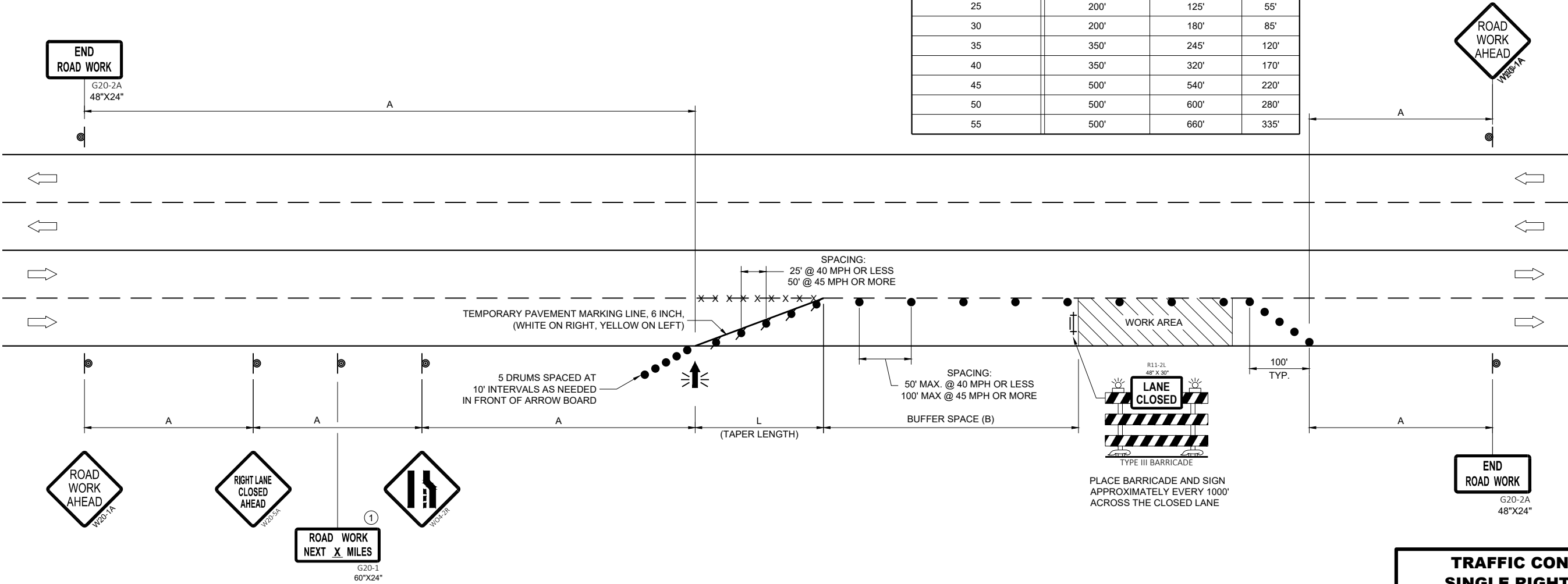
BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'
50	500'	600'	280'
55	500'	660'	335'

① OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.







**TRAFFIC CONTROL,
SINGLE RIGHT LANE
CLOSURE, UNDIVIDED
NON-FREEWAY/EXPRESSWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2024 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  DIRECTION OF TRAFFIC
-  WORK ZONE

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

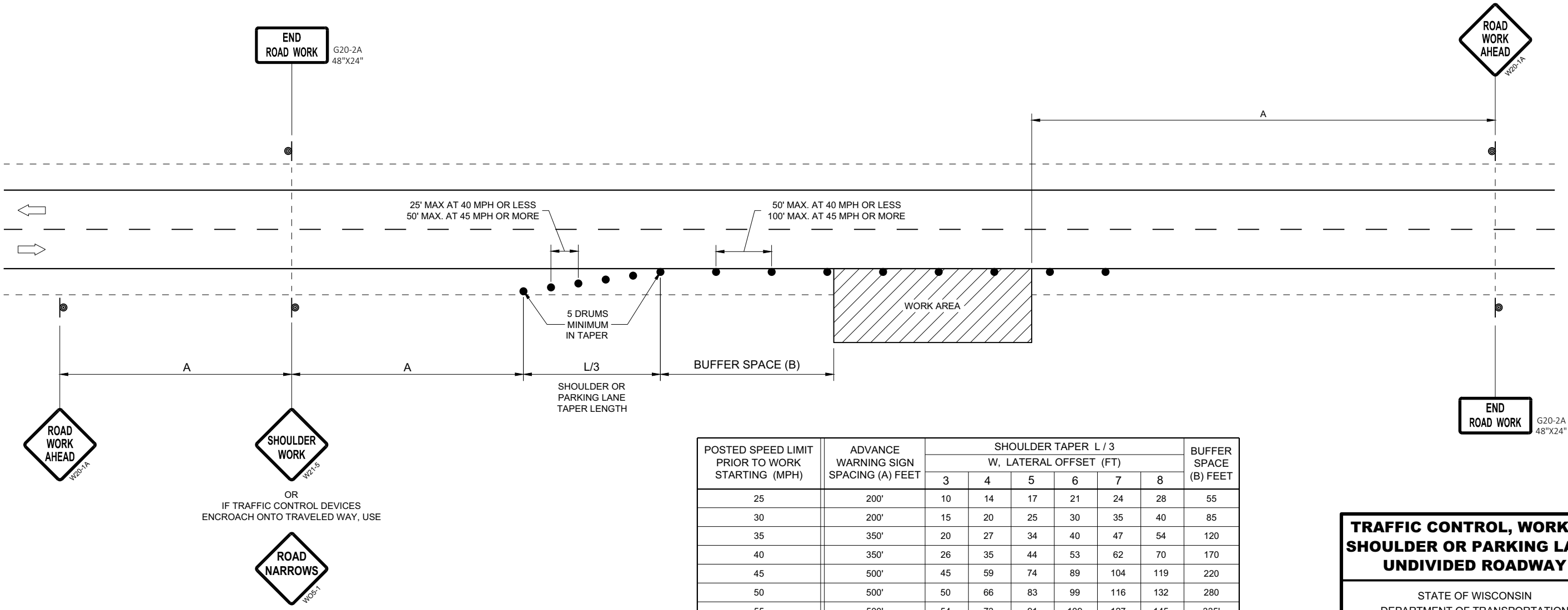
ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHOULDER TAPER L / 3 W, LATERAL OFFSET (FT)						BUFFER SPACE (B) FEET
		3	4	5	6	7	8	
25	200'	10	14	17	21	24	28	55
30	200'	15	20	25	30	35	40	85
35	350'	20	27	34	40	47	54	120
40	350'	26	35	44	53	62	70	170
45	500'	45	59	74	89	104	119	220
50	500'	50	66	83	99	116	132	280
55	500'	54	73	91	109	127	145	335'

TRAFFIC CONTROL, WORK ON
SHOULDER OR PARKING LANE,
UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2020 /S/ Andrew Heidtke
DATE STATEWIDE WORK ZONE TRAFFIC
SAFETY ENGINEER
FHWA

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

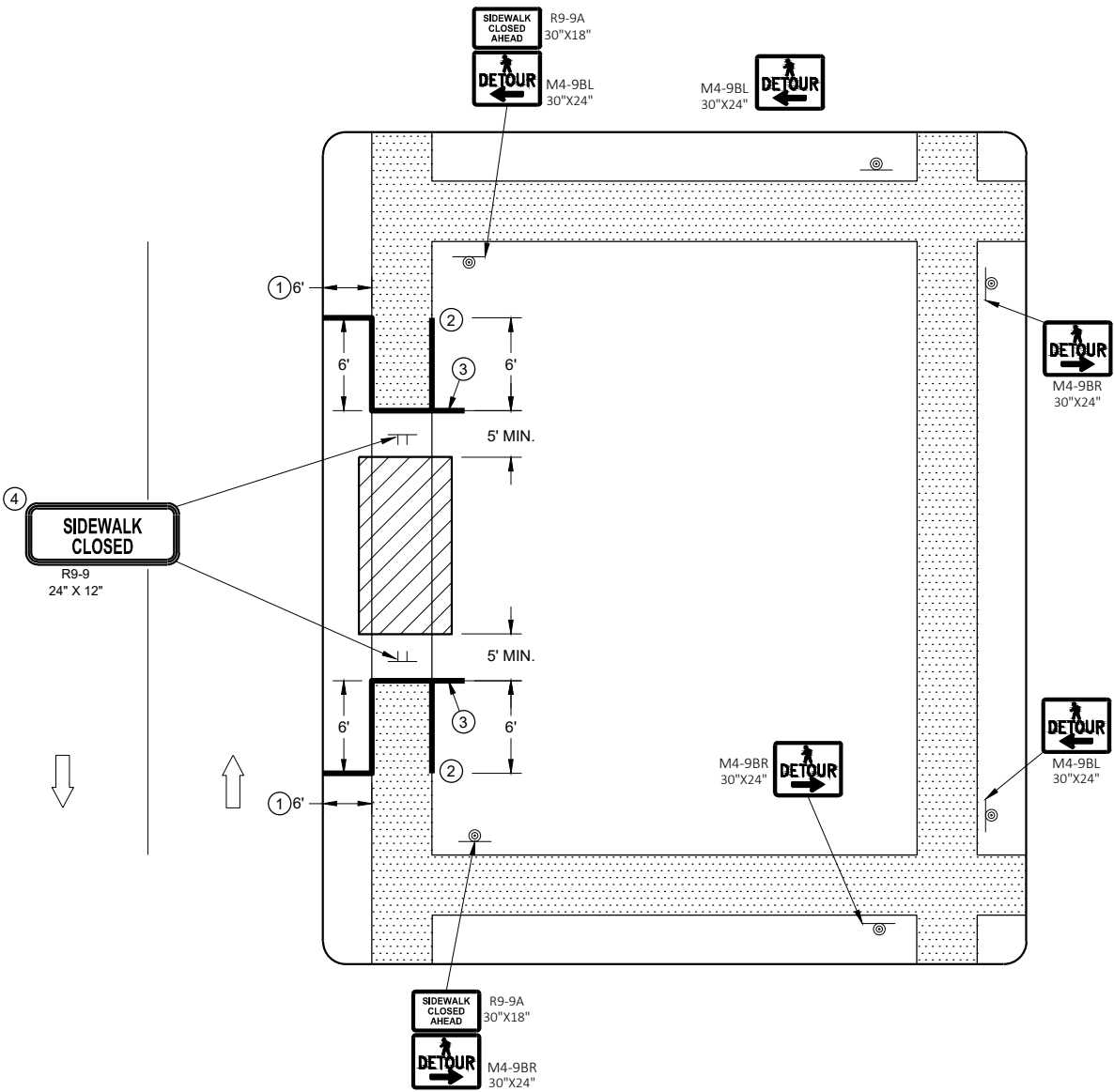
SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.








PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICTS WITH DRIVEWAYS AND OTHER EXISTING FEATURES.

- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

LEGEND

- SIGN ON TEMPORARY SUPPORT
- WORK AREA
- UNDER PEDESTRIAN TRAFFIC
- TEMPORARY PEDESTRIAN SURFACE
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

GENERAL NOTES

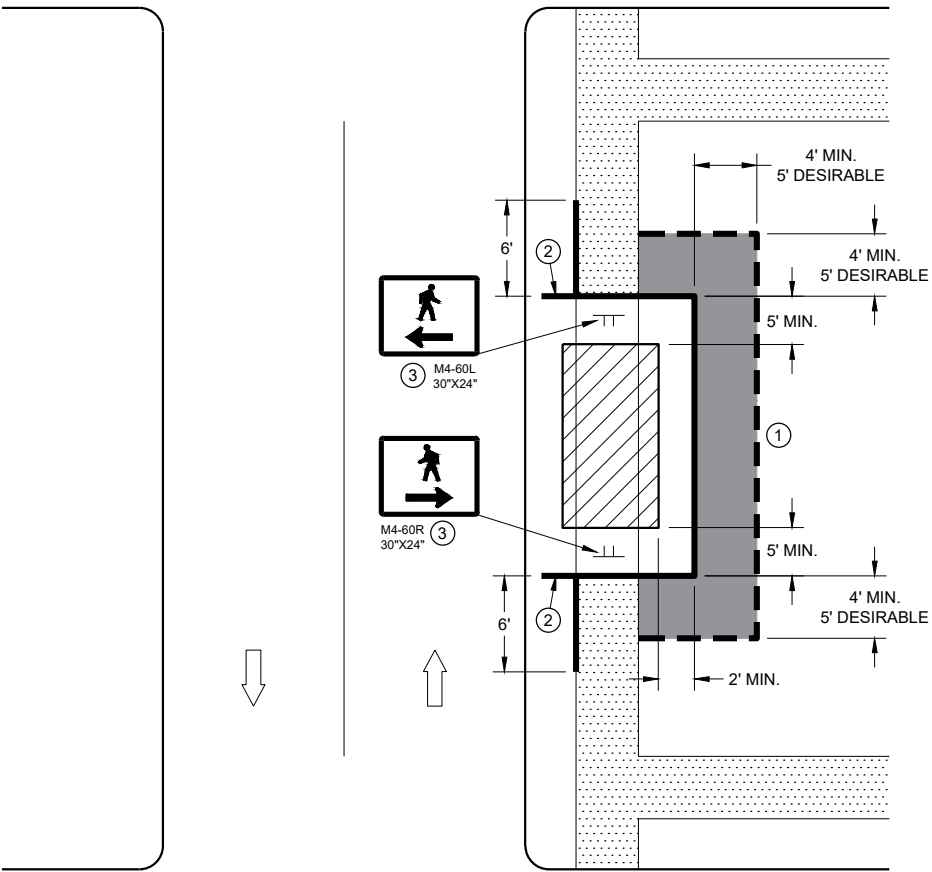
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

- ① USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ② IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



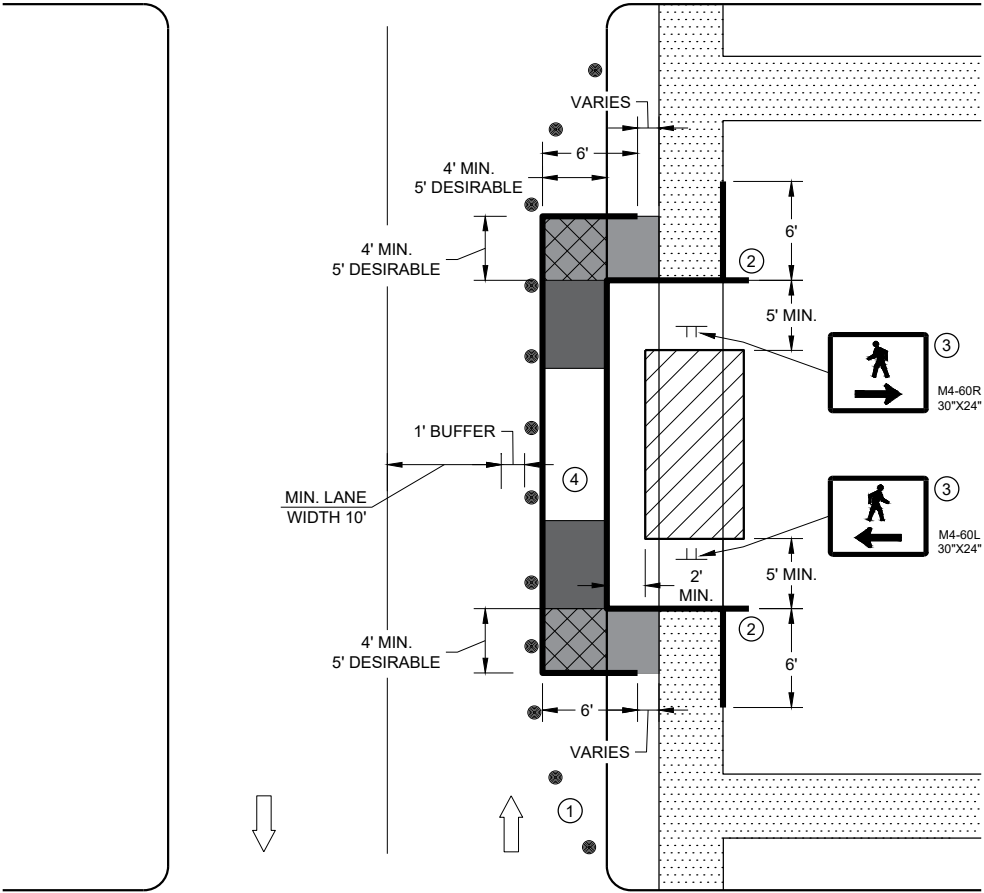
SIDEWALK DIVERSION
SINGLE SIDE

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- UNDER PEDESTRIAN TRAFFIC
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC






GENERAL NOTES

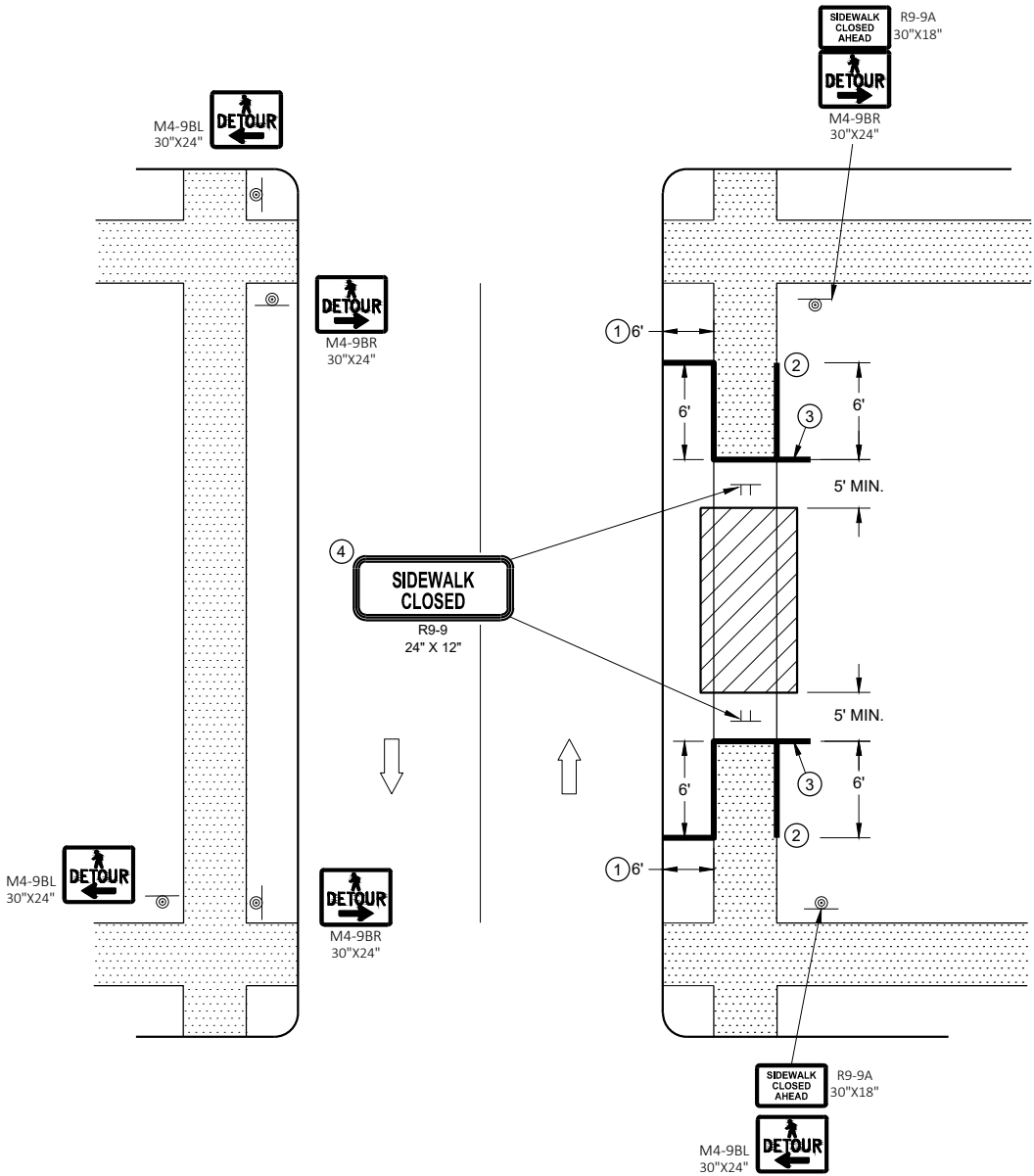
- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
 - ② PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
 - ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
 - ④ USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE.



SIDEWALK DIVERSION, SINGLE SIDE

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



SIDEWALK DETOUR, SIDEWALK ON BOTH SIDES

GENERAL NOTES

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

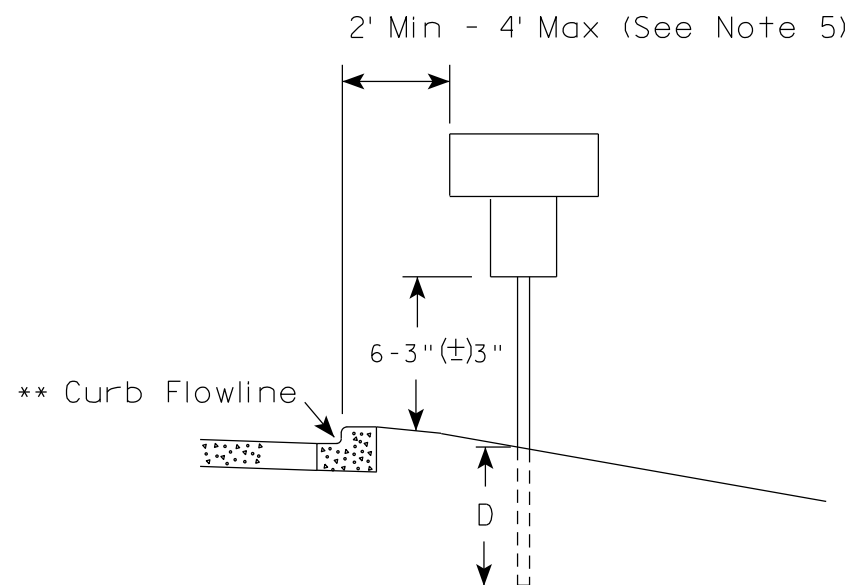
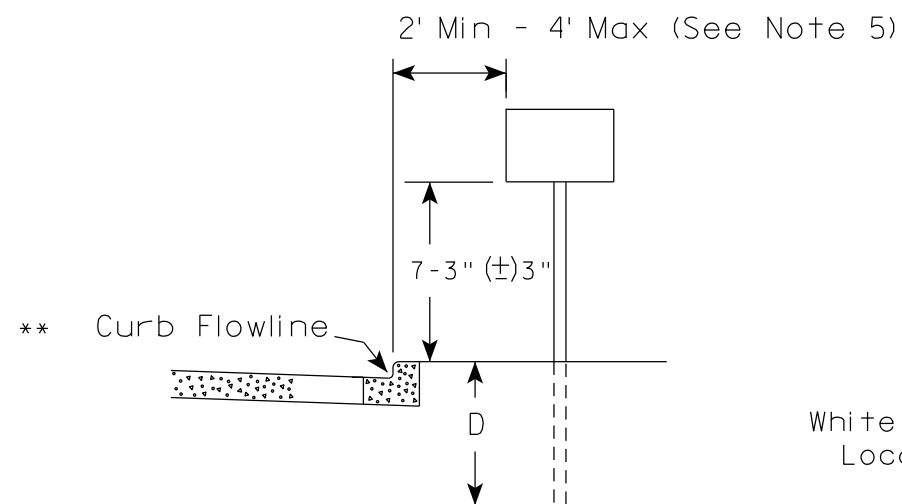
WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICT WITH DRIVEWAYS AND OTHER EXISTING FEATURES.

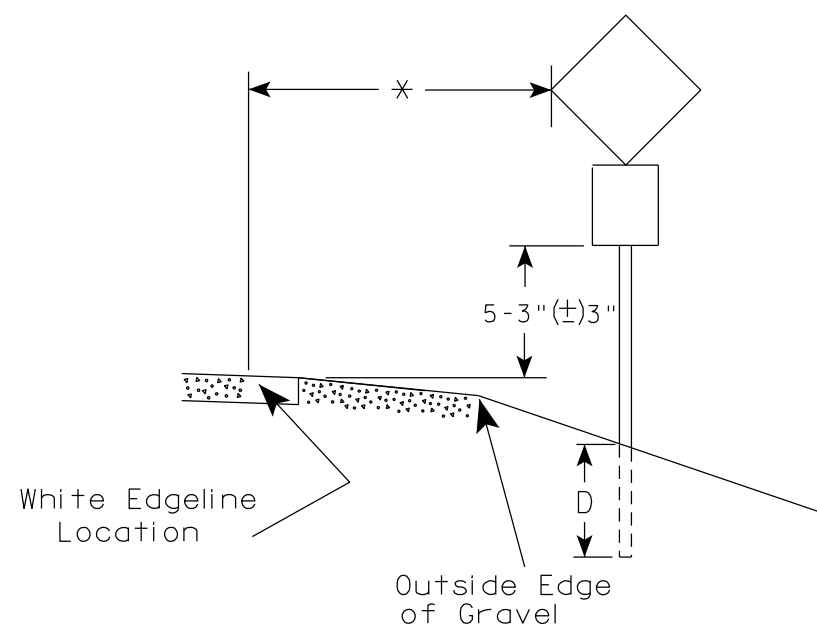
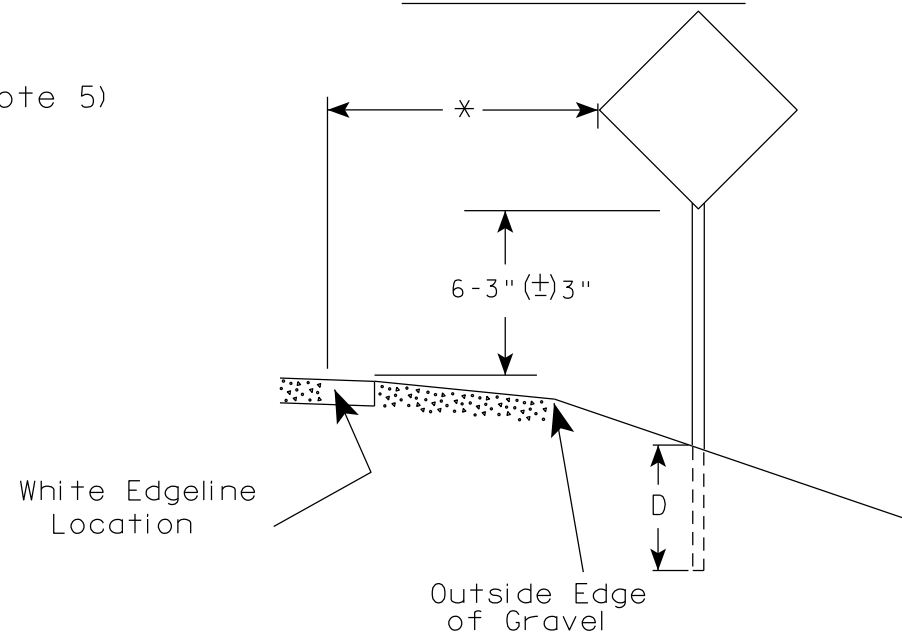
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.

URBAN AREA



* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

RURAL AREA (See Note 2)



* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

- Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
- If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".
- For expressways and freeways, mounting height is 7'- 3" (±) 3" or 6'-3" (±) 3" depending upon existence of a sub-sign.
- Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±) 3".
- Offset distance shall be consistent with existing signs or consistent throughout length of project.
- Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the Engineer.

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R. Rauch
for State Traffic Engineer

DATE 12/6/23

PLATE NO. A4-3.23

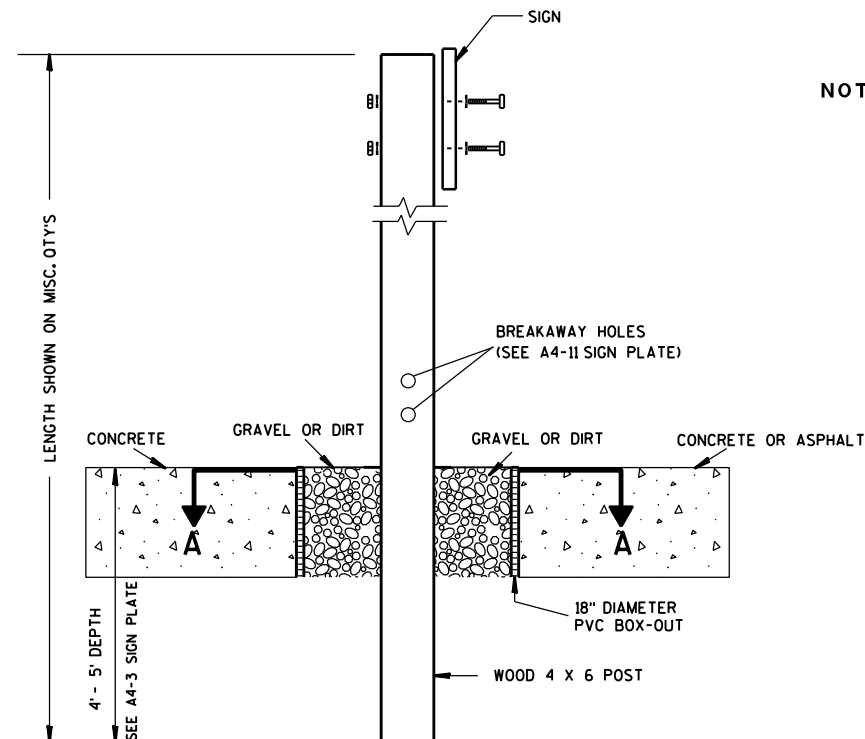
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

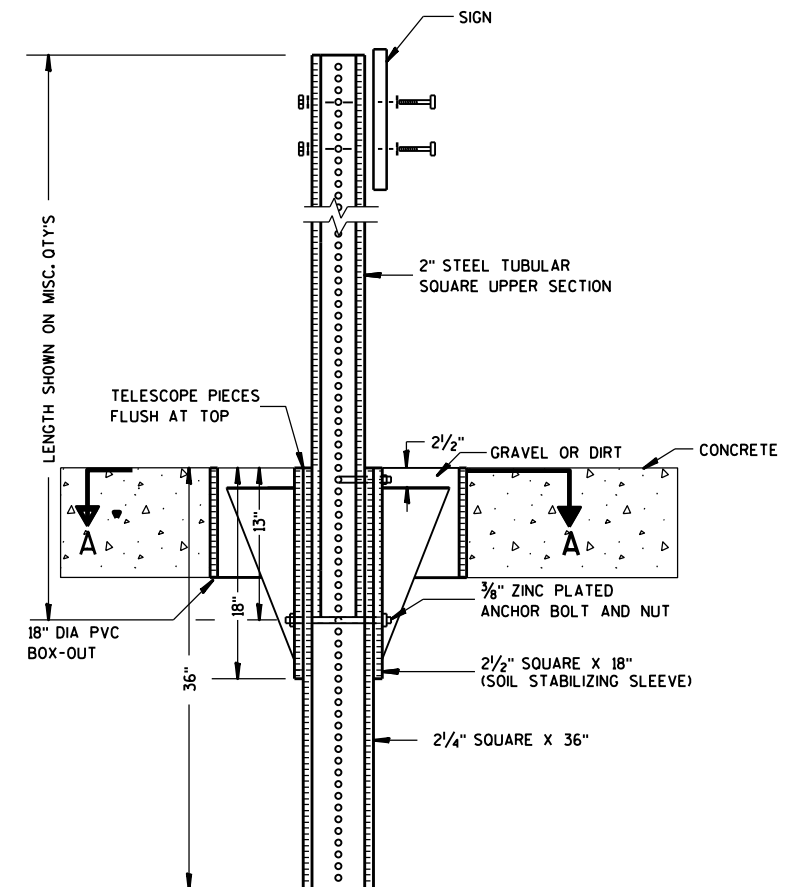
E



ELEVATION VIEW

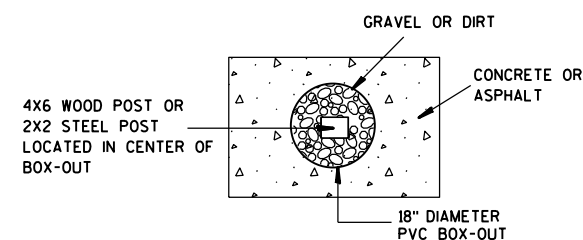
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES: 1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST
BOX-OUTS
A4-3B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

PROJECT NO:

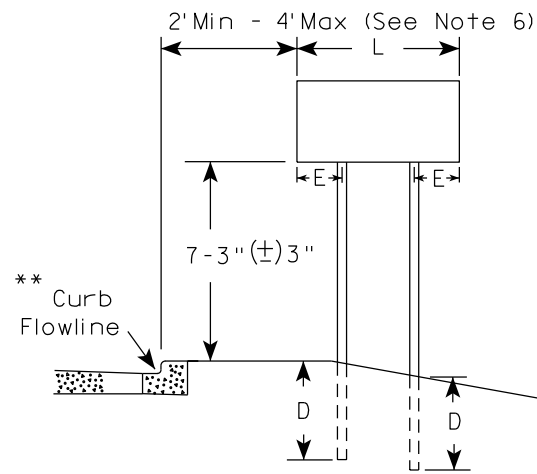
HWY:

COUNTY:

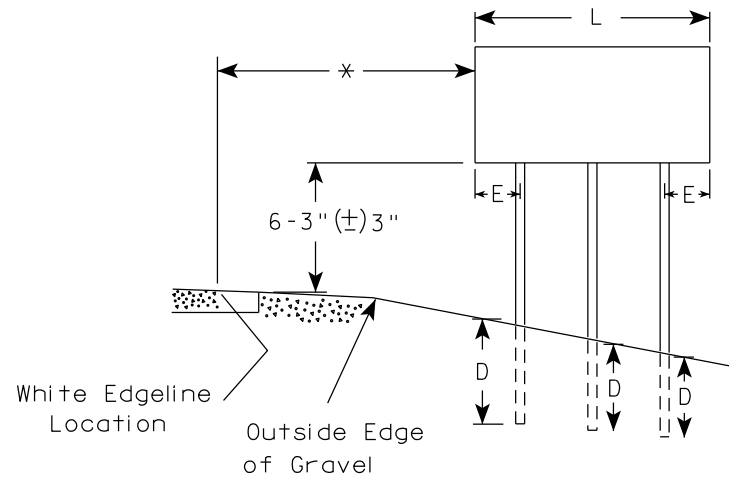
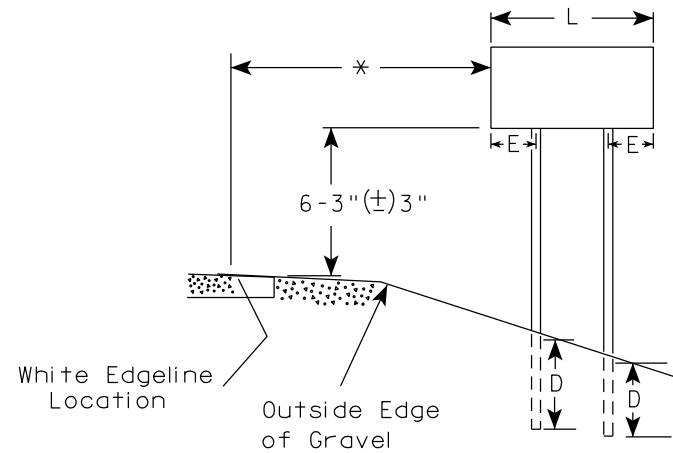
SHEET NO:

E

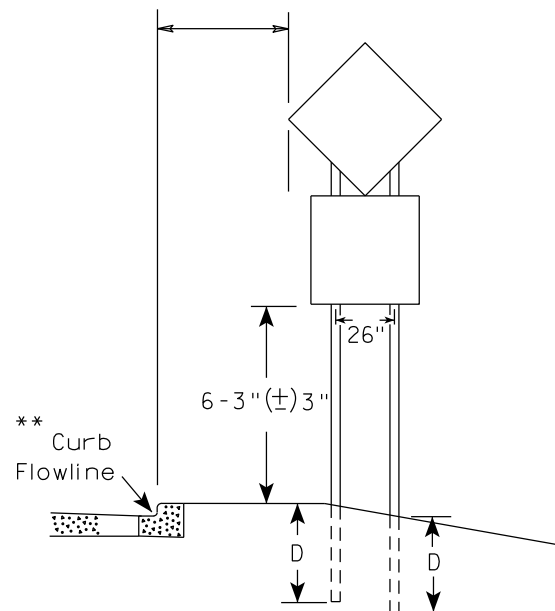
URBAN AREA



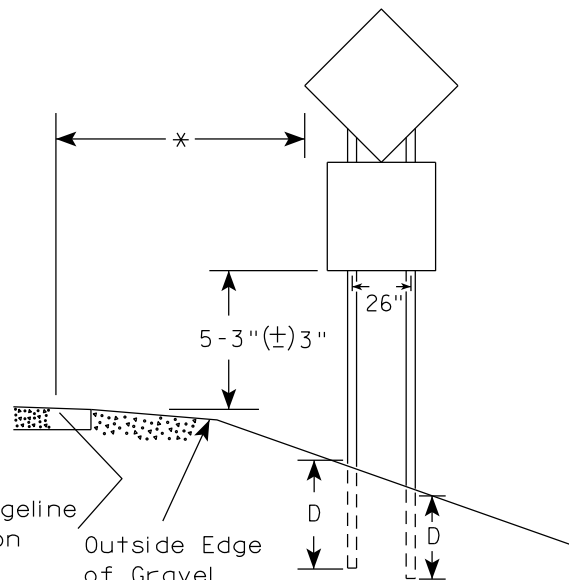
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION
OF TYPE II SIGNS
ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-4.16

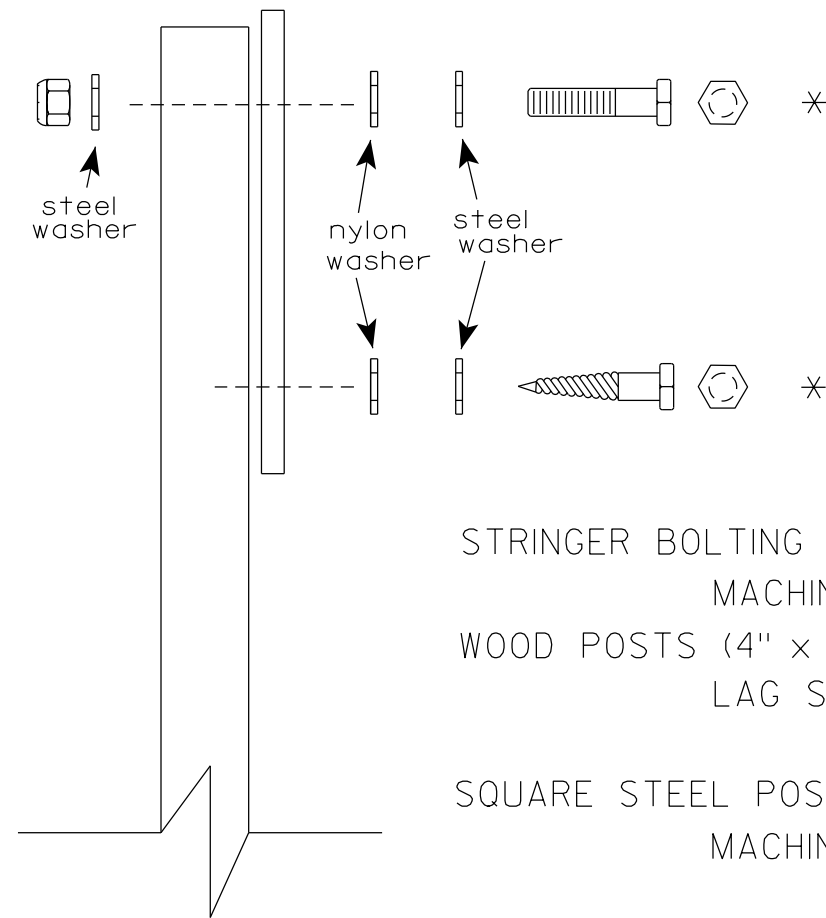
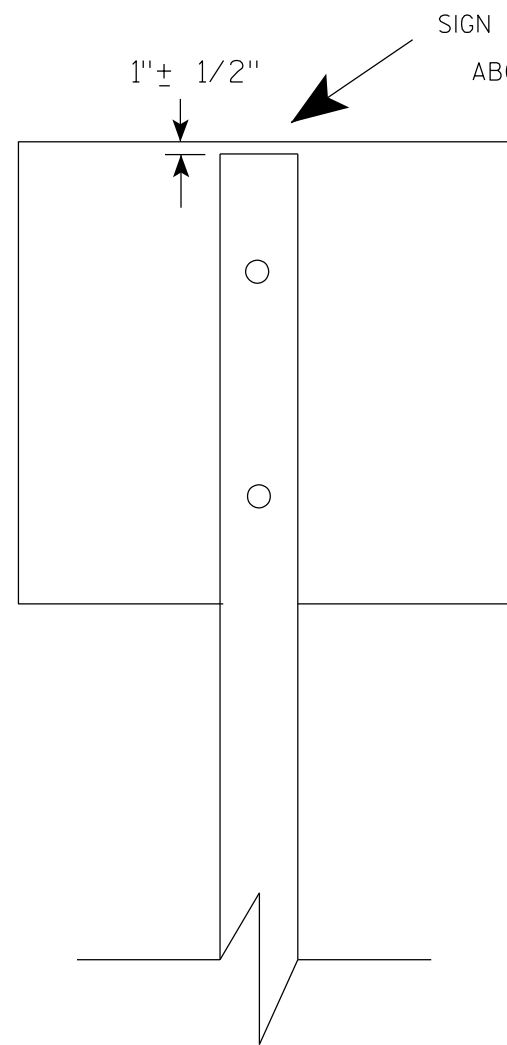
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

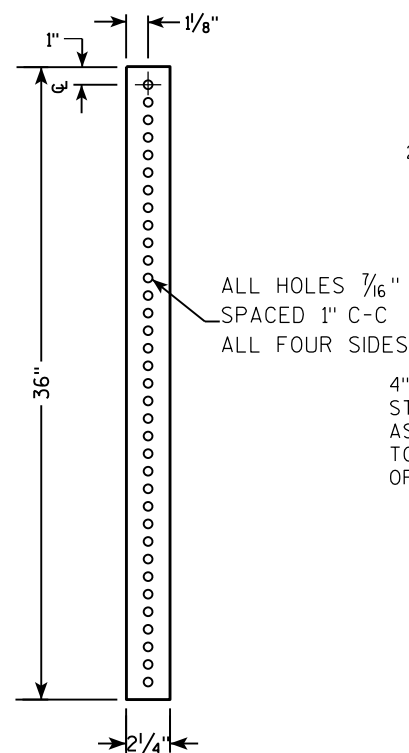
Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS - $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)
 - $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
 - $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
- O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
 - 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON

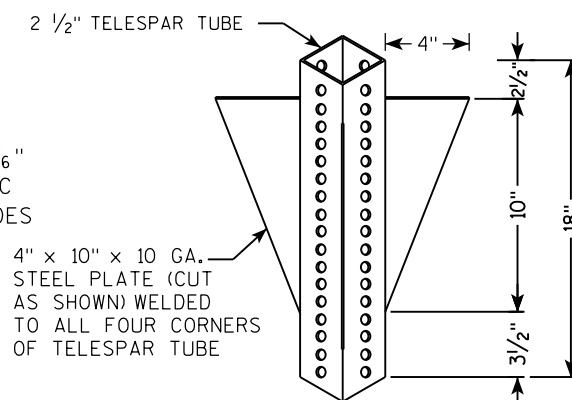
* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 4/1/2020	PLATE NO. A4-8.9

**2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH**



**2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH**



TELESCOPE PIECES
FLUSH AT TOP

18" DIA SCHEDULE
40 PVC
BOX-OUT

36"

13"

18"

2 1/2" GRAVEL OR DIRT

3/8" ZINC PLATED CORNER
ANCHOR BOLT AND NUT

2 1/2" SQUARE X 18"
(SOIL STABILIZING SLEEVE)

2 1/4" SQUARE X 36"

2" STEEL TUBULAR
SQUARE UPPER SECTION

ALL HOLES 7/16"
SPACED 1" C-C
ALL FOUR SIDES

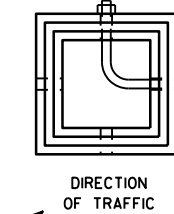
SEE SIGN PLATE
A4-8 FOR BOLT
WASHER, & NUT
MATERIAL

SIGN

TECHNICAL DRAWING OF A SIGNPOST ASSEMBLY:

- TELESCOPE PIECES FLUSH AT TOP**: Indicated by a dimension line on the left.
- 2" STEEL TUBULAR SQUARE UPPER SECTION**: The main vertical support.
- ALL HOLES $\frac{7}{16}$ " SPACED 1" C-C ALL FOUR SIDES**: Specification for the perforations in the upper section.
- 3/8" ZINC PLATED CORNER ANCHOR BOLT AND NUT**: Located at the top of the upper section.
- 1"**: Dimension for the offset of the anchor bolt.
- 3/8" ZINC PLATED ANCHOR BOLT AND NUT**: Located at the base of the upper section.
- 2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)**: The lower section of the post.
- 2 1/4" SQUARE X 36"**: The base section of the post.
- SIGN**: The sign plate at the top.
- SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL**: Reference to the sign plate for hardware details.
- LENGTH SHOWN ON MISC. QTY'S**: Dimension line on the left indicating the total length of the assembly.
- Dimensions**:
 - 36" (Total length of the base section)
 - 18" (Length of the soil stabilizing sleeve)
 - 12" (Offset of the anchor bolt from the centerline)

3/8" ZINC PLATED CORNER
ANCHOR BOLT AND NUT



SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch

for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

PROJECT NO:

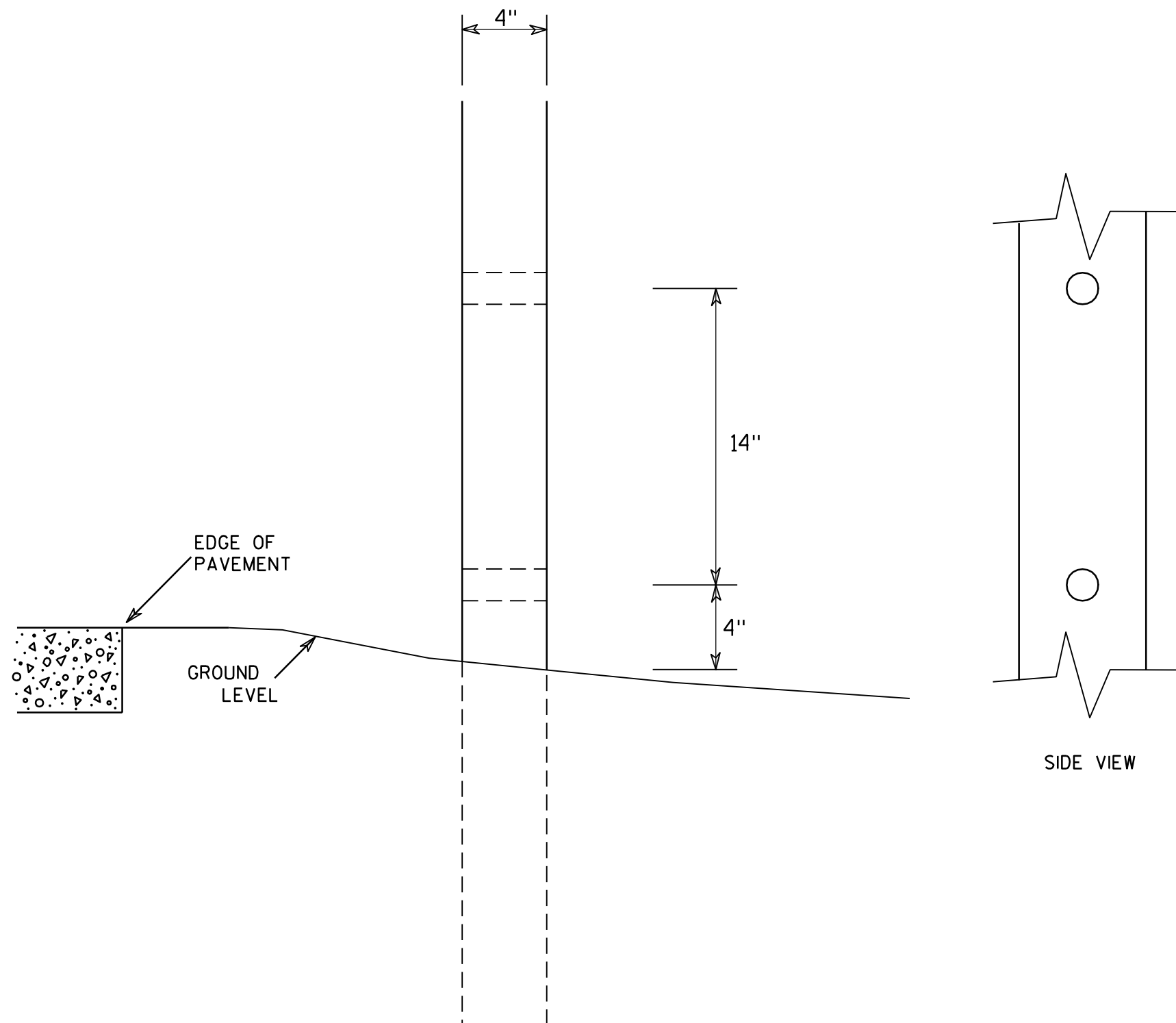
HWY:

COUNTY:

SHEET NO:

E

7



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Chester J. Spang
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

PROJECT NO:

HWY:

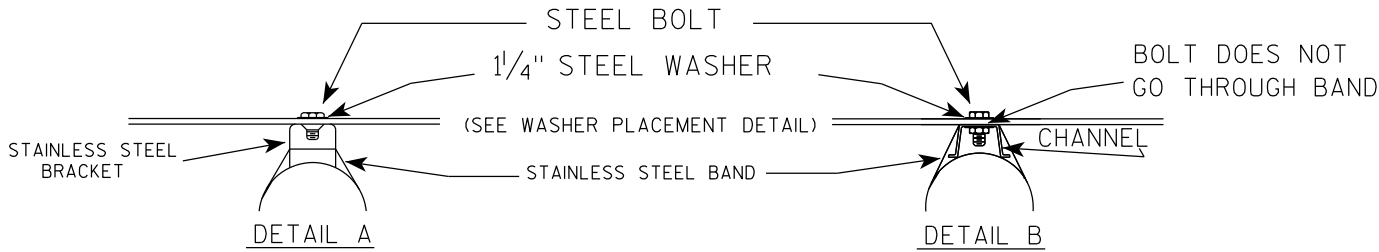
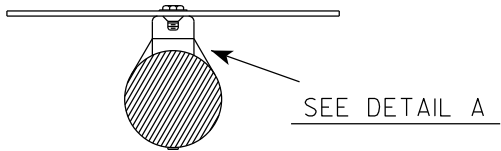
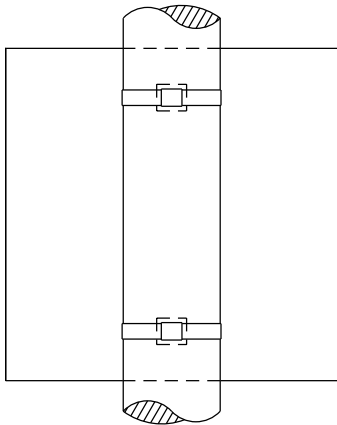
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SHEET NO:

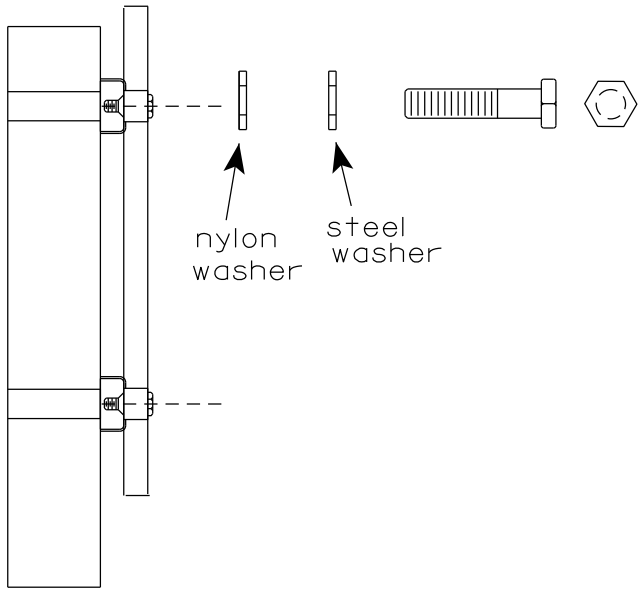
E

BANDING

SINGLE SIGN



WASHER PLACEMENT

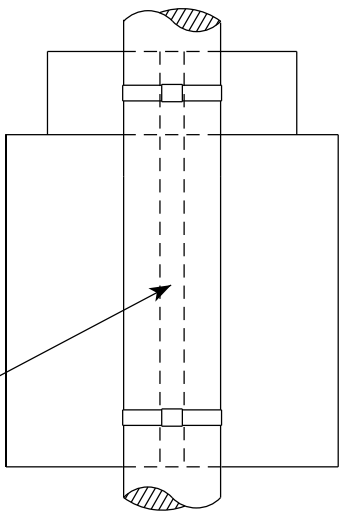


WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

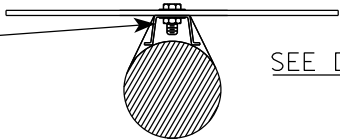
GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



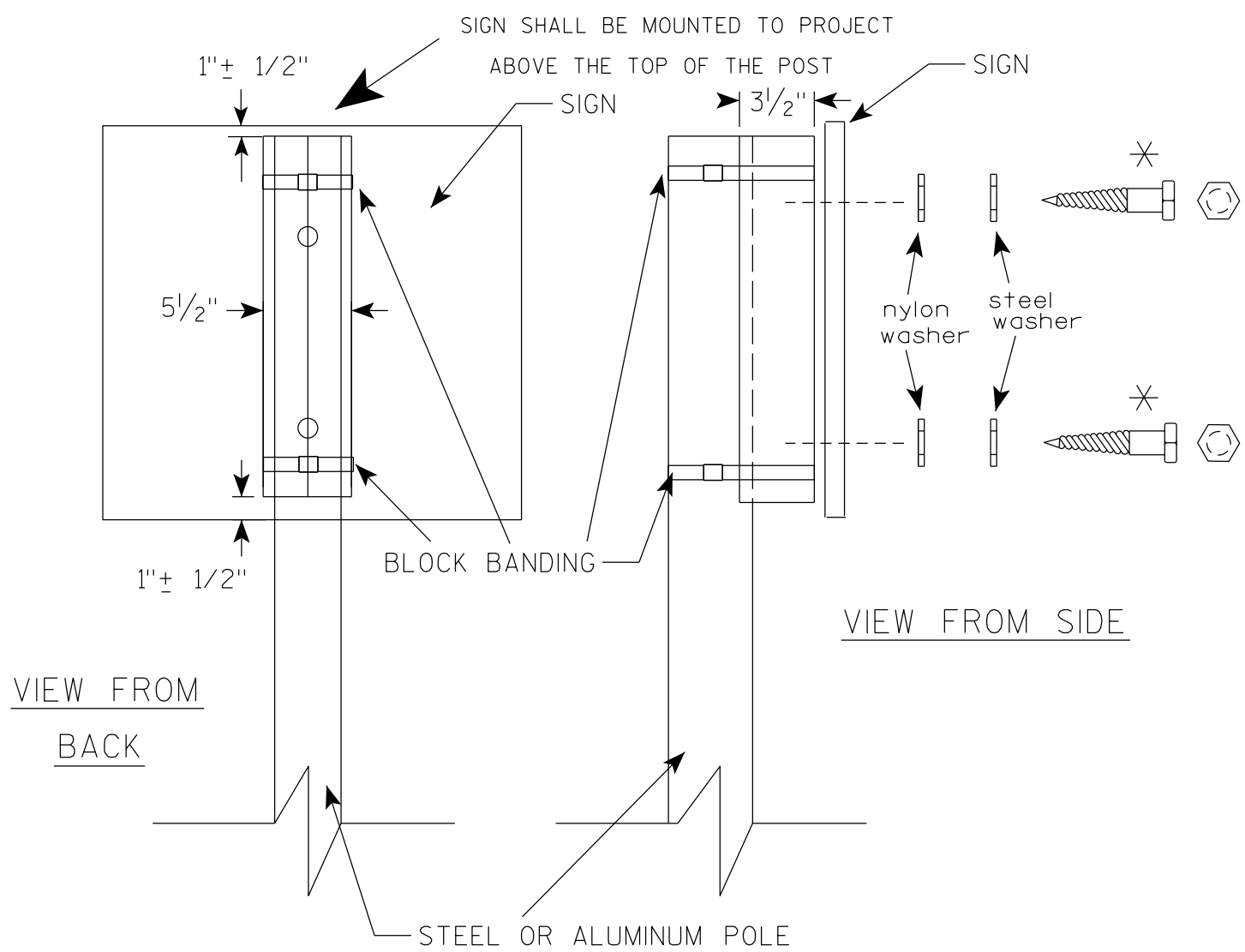
CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET



STANDARD SIGN
SIGN BANDING DETAILS

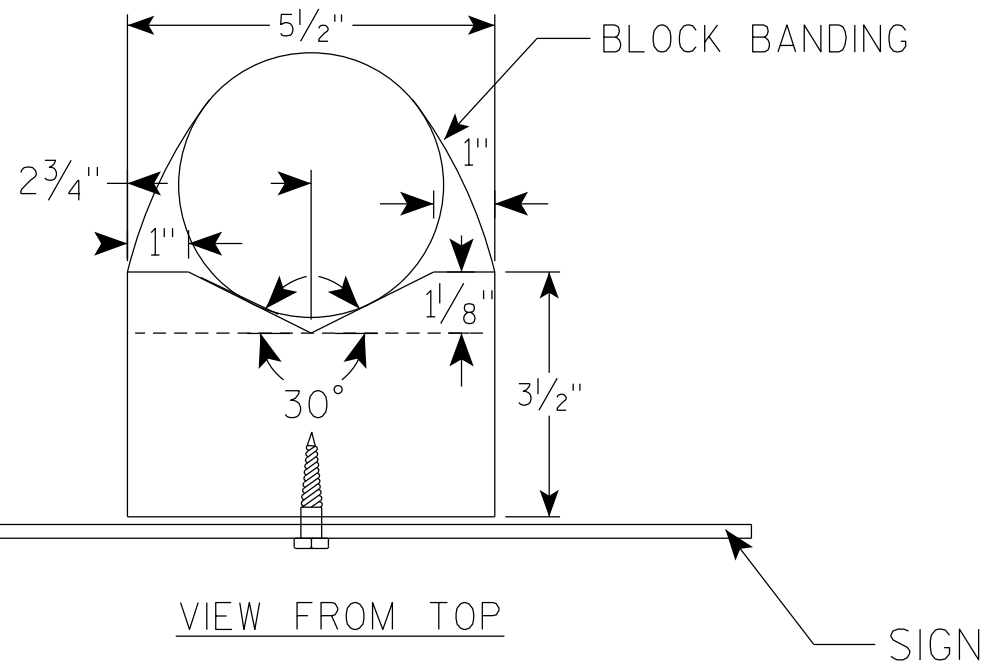
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 6/10/19 PLATE NO. A5-9.4



VIEW FROM
BACK

VIEW FROM SIDE



VIEW FROM TOP

GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, $\frac{3}{4}$ " WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE $\frac{1}{4}$ " O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ "
8. NYLON WASHERS SHALL BE $\frac{1}{4}$ " O.D. X $\frac{3}{8}$ " I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✱ LAG BOLTS SHALL BE $\frac{3}{8}$ " X $2\frac{1}{2}$ "

BLOCK BANDING DETAIL
(V-BLOCK OPTION)

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

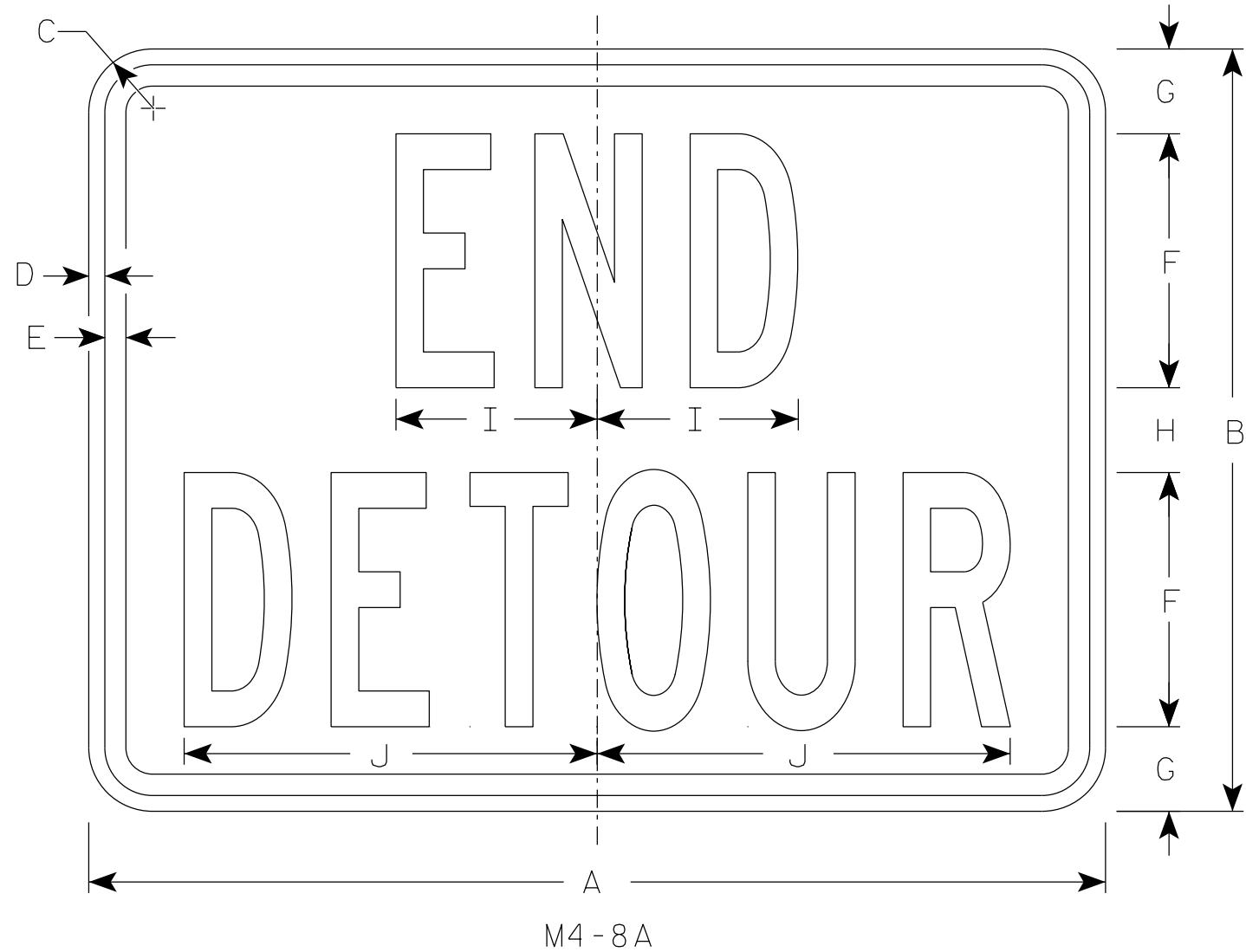
DATE 4/19/2022 PLATE NO. A5-10.3

PROJECT NO:

SHEET NO:

E

7



NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:
Background - Orange
Message - Black
- 3. Message Series - B
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 1/2	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
2M	24	18	1 1/2	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
3	30	24	1 1/2	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
4	30	24	1 1/2	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
5	30	24	1 1/2	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0

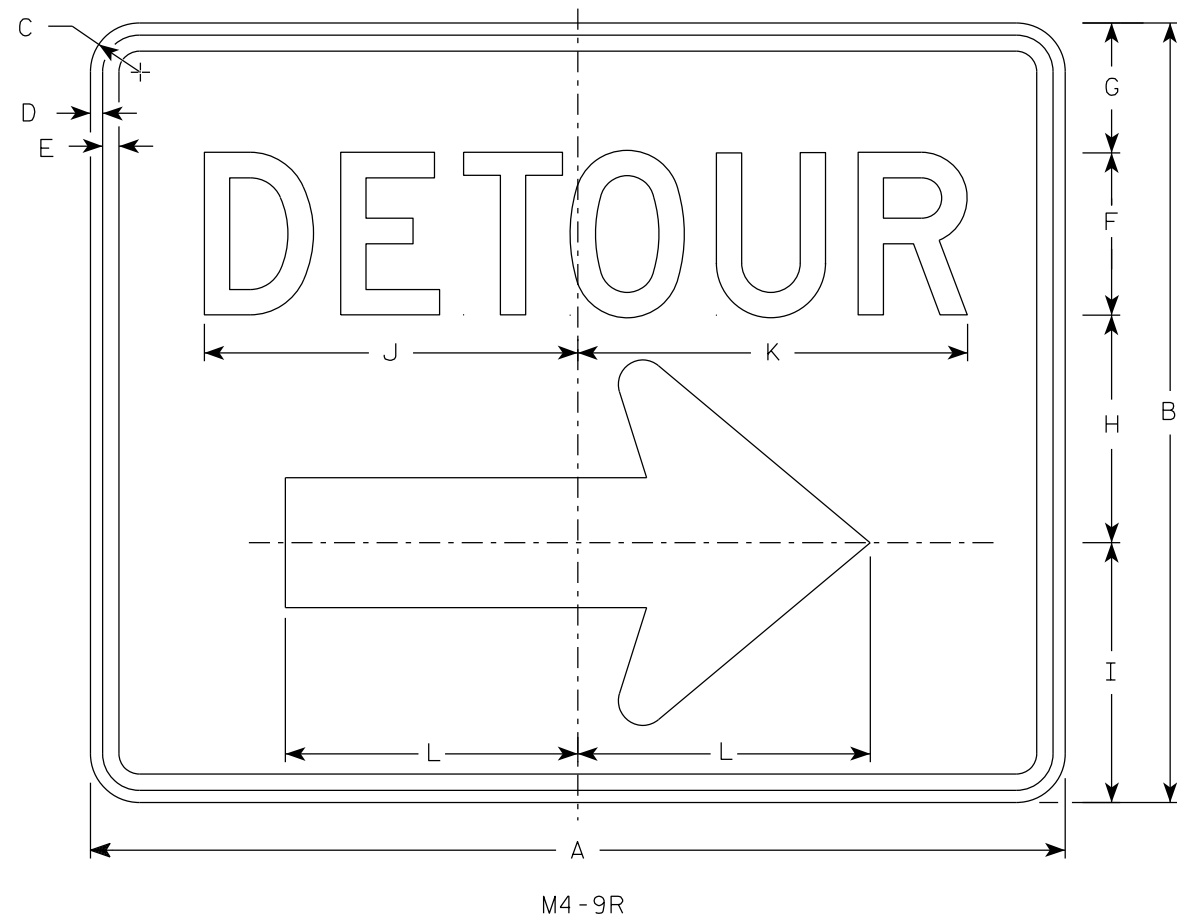
STANDARD SIGN

M4-8A

WISCONSIN DEPT OF TRANSPORTATION

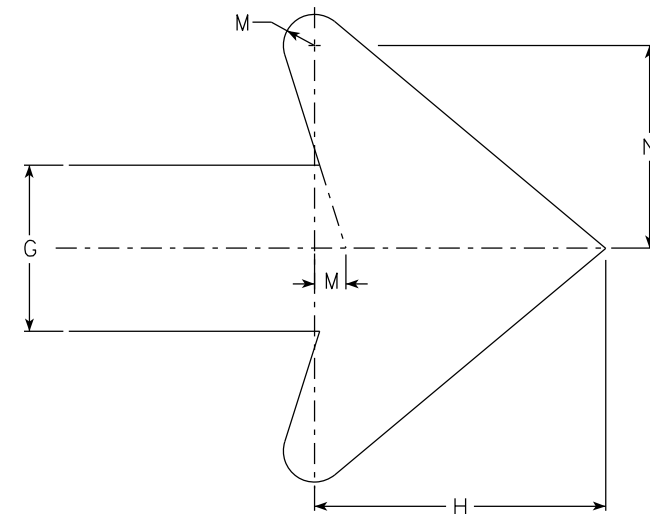
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/9/2023 PLATE NO. M4-8A.4



NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M4-9L is the same as M4-9R except the arrow is reversed.



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/2	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
2M	30	24	1 1/2	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
3	30	24	1 1/2	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
4	48	36	1 7/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0
5	48	36	1 7/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0

STANDARD SIGN
M4-9 R & L

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/9/2023 PLATE NO. M4-9R.6

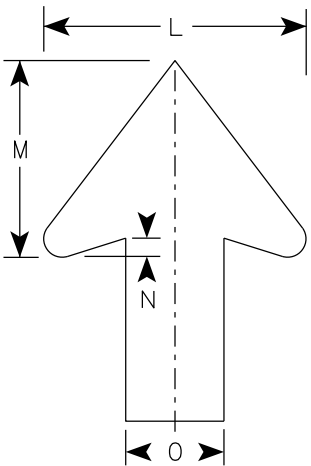
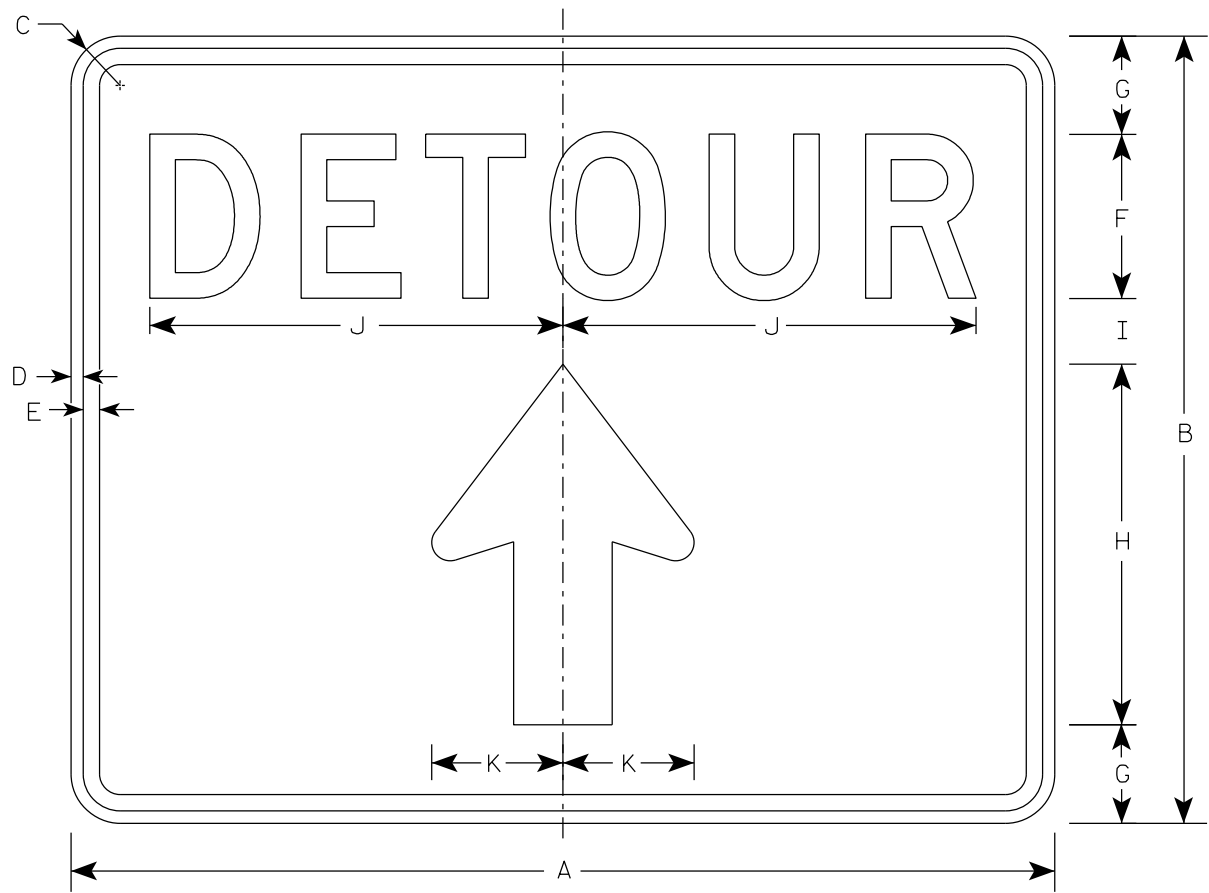
PROJECT NO: HWY: COUNTY: SHEET NO: **E**

7

7

NOTES

- 1. Sign is Type II-Type F Reflective
- 2. Color:
Background - Orange
Message - Black
- 3. Message Series - D



Arrow Detail

M4 - 9RA

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	24	1 1/2	3/8	1/2	5	3	11	2	12 5/8	4	8	6	1/2	3												5.00
2M	30	24	1 1/2	3/8	1/2	5	3	11	2	12 5/8	4	8	6	1/2	3												5.00
3																											
4																											
5																											

STANDARD SIGN

M4-9RA

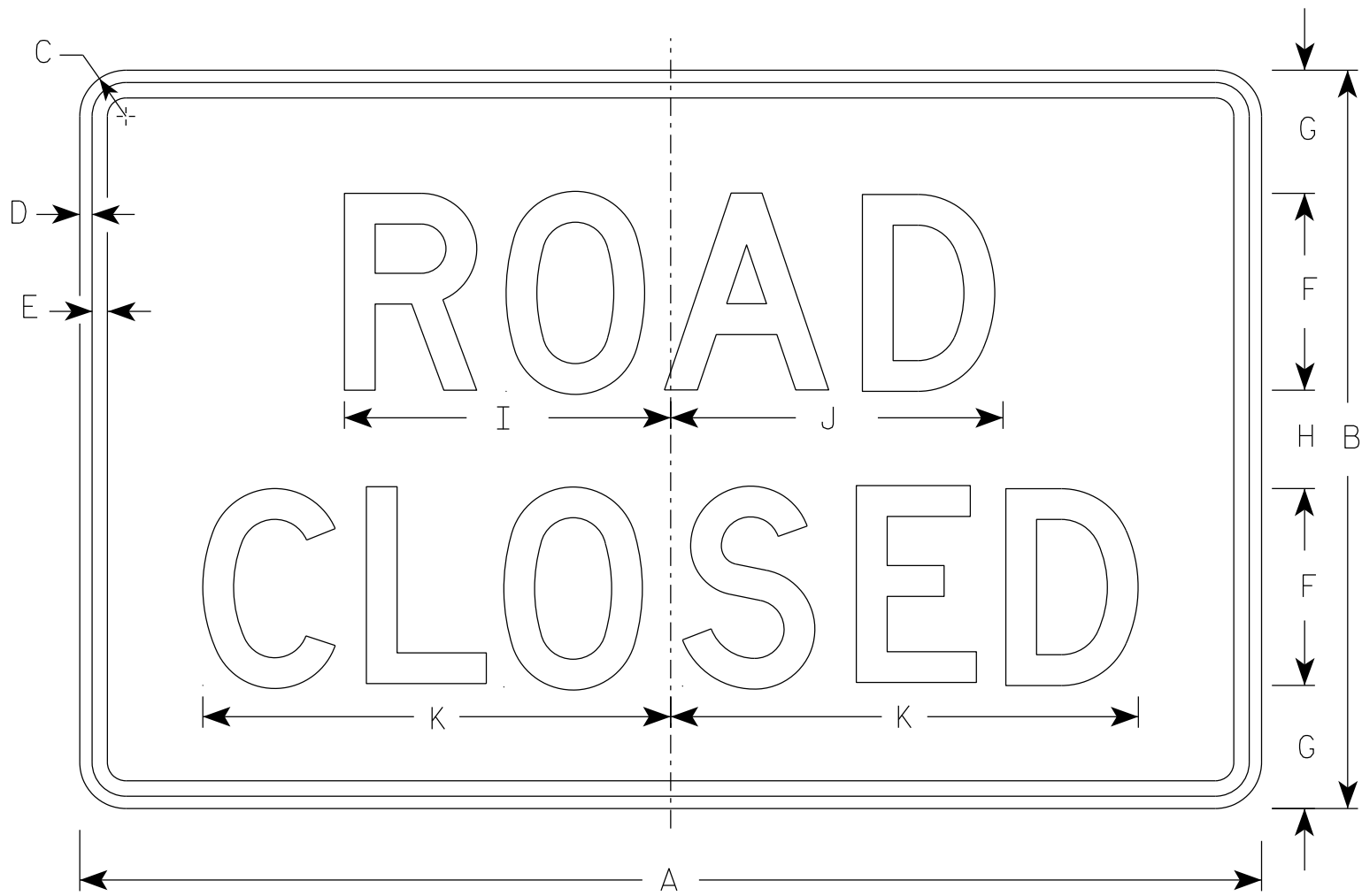
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*

for State Traffic Engineer

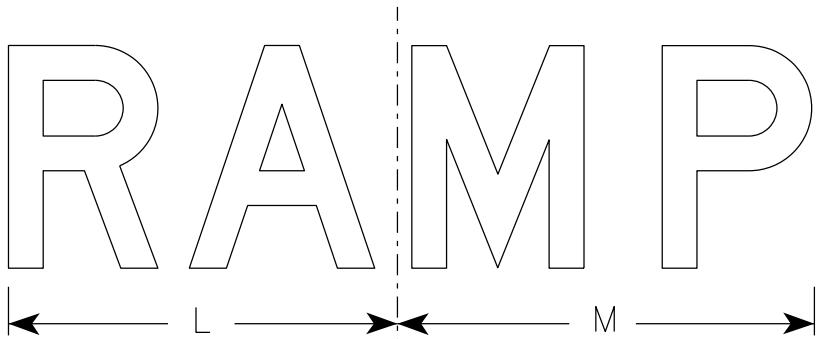
DATE 2/9/2023

PLATE NO. M4-9RA.3

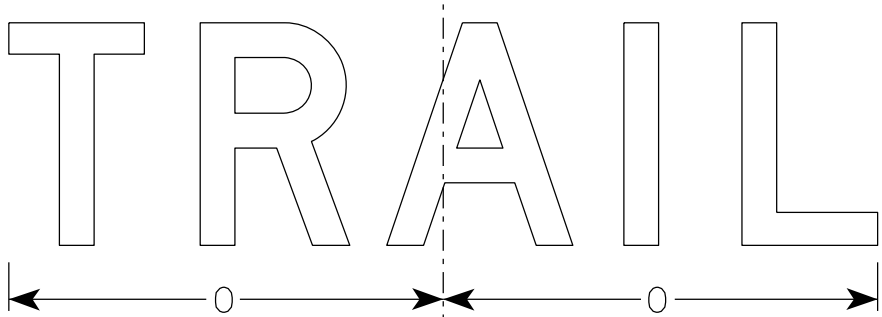


R11-2

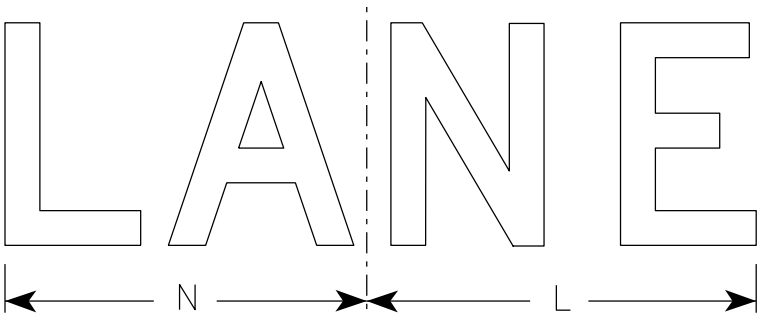
- NOTES
1. Sign is Type II - Type H Reflective
 2. Color:
Background - White
Message - Black
 3. Message Series - D
 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
 5. Modify the message as required.



R11-2R



R11-2T



R11-2L

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
2M	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
3	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
4	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
5	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0

STANDARD SIGN R11-2	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 2/5/24	PLATE NO. R11-2.12

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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Wisconsin Department of Transportation

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