

EAU

PROJECT ID:
WITH: N/A

7893-00-70

COUNTY:

PIERCE

MAY 2025
ORDER OF SHEETS

| | | |
|-------------|---|------------------------------|
| Section No. | 1 | Title |
| Section No. | 2 | Typical Sections and Details |
| Section No. | 3 | Estimate of Quantities |
| Section No. | 3 | Miscellaneous Quantities |
| Section No. | 4 | Right of Way Plat |
| Section No. | 5 | Plan and Profile |
| Section No. | 6 | Standard Detail Drawings |
| Section No. | 7 | Sign Plates |
| Section No. | 8 | Structure Plans |
| Section No. | 9 | Computer Earthwork Data |
| Section No. | 9 | Cross Sections |

TOTAL SHEETS = 44



DESIGN DESIGNATION 7893-00-00

| | | | |
|--------------|------|---|--------|
| A.A.D.T. | 2025 | = | 90 |
| A.A.D.T. | 2045 | = | 98 |
| D.H.V. | | = | |
| D.D. | | = | 50/50 |
| T. | | = | 10% |
| DESIGN SPEED | | = | 60 MPH |
| ESALS | | = | 22,000 |

CONVENTIONAL SYMBOLS

| | |
|--------------------------------|--|
| PLAN | |
| CORPORATE LIMITS | |
| PROPERTY LINE | |
| LOT LINE | |
| LIMITED HIGHWAY EASEMENT | |
| EXISTING RIGHT OF WAY | |
| PROPOSED OR NEW R/W LINE | |
| SLOPE INTERCEPT | |
| REFERENCE LINE | |
| EXISTING CULVERT | |
| PROPOSED CULVERT (Box or Pipe) | |
| COMBUSTIBLE FLUIDS | |
| MARSH AREA | |
| WOODED OR SHRUB AREA | |

| | |
|---|--|
| PROFILE | |
| GRADE LINE | |
| ORIGINAL GROUND | |
| MARSH OR ROCK PROFILE (To be noted as such) | |
| SPECIAL DITCH | |
| GRADE ELEVATION | |
| CULVERT (Profile View) | |
| UTILITIES | |
| ELECTRIC | |
| FIBER OPTIC | |
| GAS | |
| SANITARY SEWER | |
| STORM SEWER | |
| TELEPHONE | |
| WATER | |
| UTILITY PEDESTAL | |
| POWER POLE | |
| TELEPHONE POLE | |

| | |
|------------------------|--|
| ROCK | |
| LABEL | |
| GRADE ELEVATION | |
| CULVERT (Profile View) | |
| UTILITIES | |
| ELECTRIC | |
| FIBER OPTIC | |
| GAS | |
| SANITARY SEWER | |
| STORM SEWER | |
| TELEPHONE | |
| WATER | |
| UTILITY PEDESTAL | |
| POWER POLE | |
| TELEPHONE POLE | |

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
PLAN OF PROPOSED IMPROVEMENT

T DIAMOND BLUFF, 1005TH STREET
WIND RIVER BRIDGE, B-47-0233
LOCAL STR
PIERCE COUNTY

STATE PROJECT NUMBER
7893-00-70



LAYOUT
SCALE 0 1 MI
TOTAL NET LENGTH OF CENTERLINE = 0.038 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), PIERCE COUNTY, NAD83 (1991), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 18.

STATE PROJECT

7893-00-70

FEDERAL PROJECT

PROJECT

WISC 2025484

CONTRACT

1

ACCEPTED FOR

TOWN OF DIAMOND BLUFF

Date: 1-9-25
Chair
(Signature)
(Title of Official)

ORIGINAL PLANS PREPARED BY

SRF

WISCONSIN
PROFESSIONAL ENGINEER
BRIAN M. GENSKOW
E-45795-6
MIDDLETON, WI
1/8/2025

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

| | |
|---------------------|----------------------------|
| Surveyor | SRF CONSULTING GROUP, INC. |
| Designer | SRF CONSULTING GROUP, INC. |
| Project Manager | MATTHEW BERG |
| Regional Examiner | TOU YANG |
| Regional Supervisor | TOU YANG |

APPROVED FOR THE DEPARTMENT

DATE: 1/14/2025
(Signature)

E

UTILITIES CONTACTS

BEVCOMM
COMMUNICATION LINE
CHAD WHITCOMB
PO BOX 125
HAGER CITY, WI 54014
PHONE: 651-380-2379
EMAIL: cwhitcomb@bevcomm.com

PIERCE PEPIN COOPERATIVE SERVICES
ELECTRIC
BRAD RISTOW
W7725 US HIGHWAY 10, PO BOX 420
ELLSWORTH, WI 54011
PHONE: 715-273-2473
EMAIL: bristow@piercepepin.coop

WISCONSIN DNR LIAISON

AMY LESIK
NW REGION
1300 W CLAIREMONT AVE
EAU CLAIRE, WI 54701
PHONE: 715-495-1903
EMAIL: amyl.lesik@wisconsin.gov

DESIGN CONTACT

CASEY BLACK
SRF CONSULTING GROUP, INC.
3701 WAYZATA BLVD, SUITE 100
MINNEAPOLIS, MN 55416
PHONE: 763-452-4751
EMAIL: cblack@srfconsulting.com

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LBS/SY/IN.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

WHEN THE QUANTITY OF THE ITEMS OF BASE AGGREGATE, SUBBASE OR HMA PAVEMENT IS MEASURED FOR PAYMENT BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE LAYERS SHOWN ON THE PLAN IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

EROSION CONTROL FEATURES AS SHOWN IN THE PLANS ARE AT APPROXIMATE LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE CONTRACTOR’S EROSION CONTROL IMPLEMENTATION PLAN (ECIP) AND APPROVED BY THE ENGINEER. MAINTAIN EROSION CONTROL MEASURES UNTIL SUCH A TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NECESSARY.

THE PROPOSED SHOULDER WIDTH SHOWN IN THE TYPICAL SECTIONS ARE MINIMUM WIDTH. PERPETUATE EXISTING SHOULDERS THAT ARE WIDER THAN WHAT IS SHOWN IN THE TYPICAL SECTIONS.

THE CONTRACTOR'S PAVING OPERATION SHALL BE CONSISTENT WITH THE TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, PASSING, OR PARKING LANE.

SAWCUTS, AS SHOWN ON THE PLANS, ARE SUGGESTED LOCATIONS AND MAY BE ADJUSTED AT THE DISCRETION OF THE ENGINEER TO BETTER SUIT FIELD CONDITIONS.

CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS AT ALL TIMES EXCEPT WHEN PAVING OR PIPE LAYING OPERATIONS REQUIRE THE DRIVEWAY TO BE CLOSED. ACCESS TO DRIVEWAYS SHALL BE RE-ESTABLISHED IMMEDIATELY AFTER OPERATIONS ARE COMPLETED. ACCESS SHALL BE PROVIDED DURING ALL NON-WORKING HOURS.

TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

DO NOT DRIVE OR STORE EQUIPMENT, OR STORE CONSTRUCTION MATERIALS IN ENVIRONMENTALLY SENSITIVE AREAS, WETLANDS OR WATERWAYS.



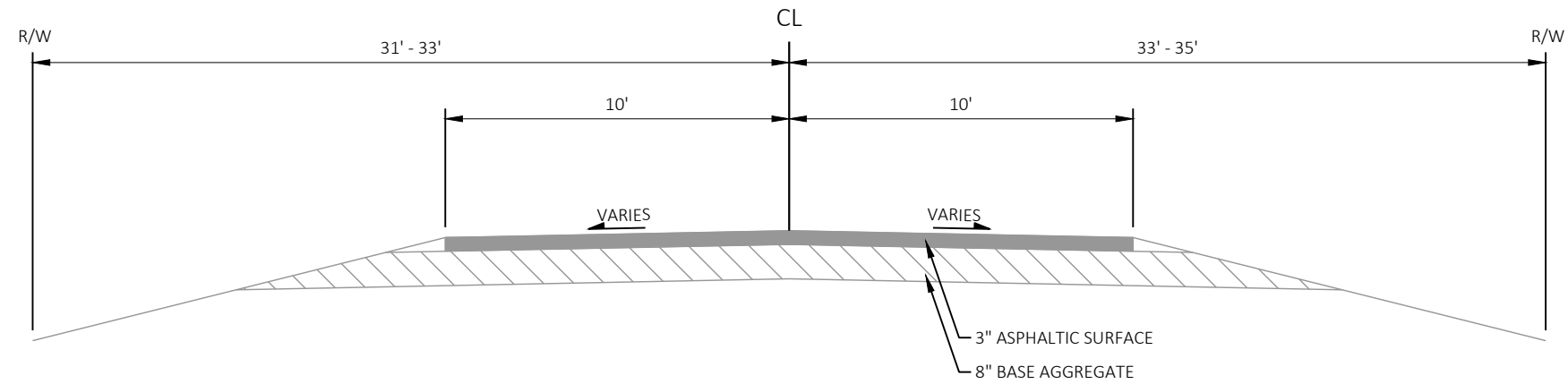
RUNOFF COEFFICIENT TABLE

| | HYDROLOGIC SOIL GROUP | | | | | | | | | | | |
|--------------------------|-----------------------|-----|----------|-----------------------|-----|----------|-----------------------|-----|----------|-----------------------|-----|----------|
| | A | | | B | | | C | | | D | | |
| | SLOPE RANGE (PERCENT) | | | SLOPE RANGE (PERCENT) | | | SLOPE RANGE (PERCENT) | | | SLOPE RANGE (PERCENT) | | |
| LAND USE: | 0-2 | 2-6 | 6 & OVER | 0-2 | 2-6 | 6 & OVER | 0-2 | 2-6 | 6 & OVER | 0-2 | 2-6 | 6 & OVER |
| ROW CROPS: | .08 | .16 | .22 | .12 | .20 | .27 | .15 | .24 | .33 | .19 | .28 | .38 |
| | .22 | .30 | .38 | .26 | .34 | .44 | .30 | .37 | .50 | .34 | .41 | .56 |
| MEDIAN STRIPTURF: | .19 | .20 | .24 | .19 | .22 | .26 | .20 | .23 | .30 | .20 | .25 | .30 |
| | .24 | .26 | .30 | .25 | .28 | .33 | .26 | .30 | .37 | .27 | .32 | .40 |
| SIDE SLOPETURF: | | | .25 | | | .27 | | | .28 | | | .30 |
| | | | .32 | | | .34 | | | .36 | | | .38 |
| PAVEMENT: | | | | | | | | | | | | |
| ASPHALT: | .70 - .95 | | | | | | | | | | | |
| CONCRETE: | .80 - .95 | | | | | | | | | | | |
| BRICK: | .70 - .80 | | | | | | | | | | | |
| DRIVES, WALKS: | .75 - .85 | | | | | | | | | | | |
| ROOFS: | .75 - .95 | | | | | | | | | | | |
| GRAVEL ROADS, SHOULDERS: | .40 - .60 | | | | | | | | | | | |

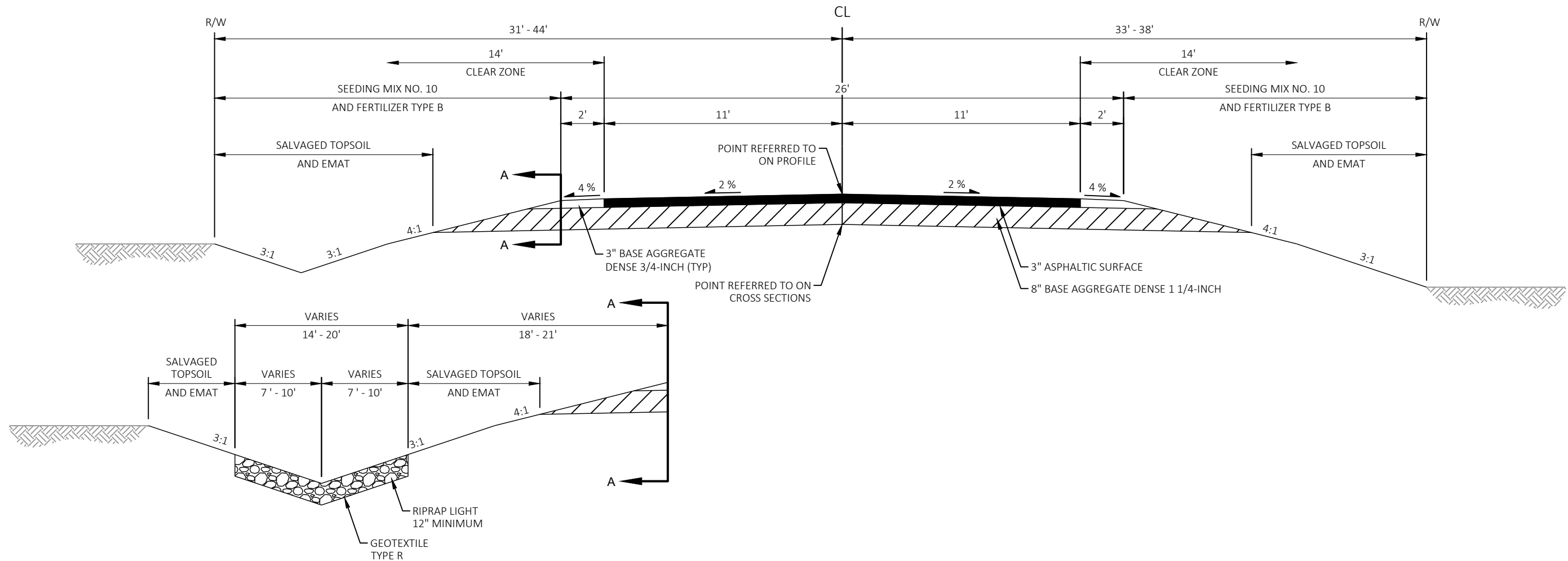
TOTAL PROJECT AREA = 0.35 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.35 ACRES

ORDER OF SECTION 2 DETAIL SHEETS

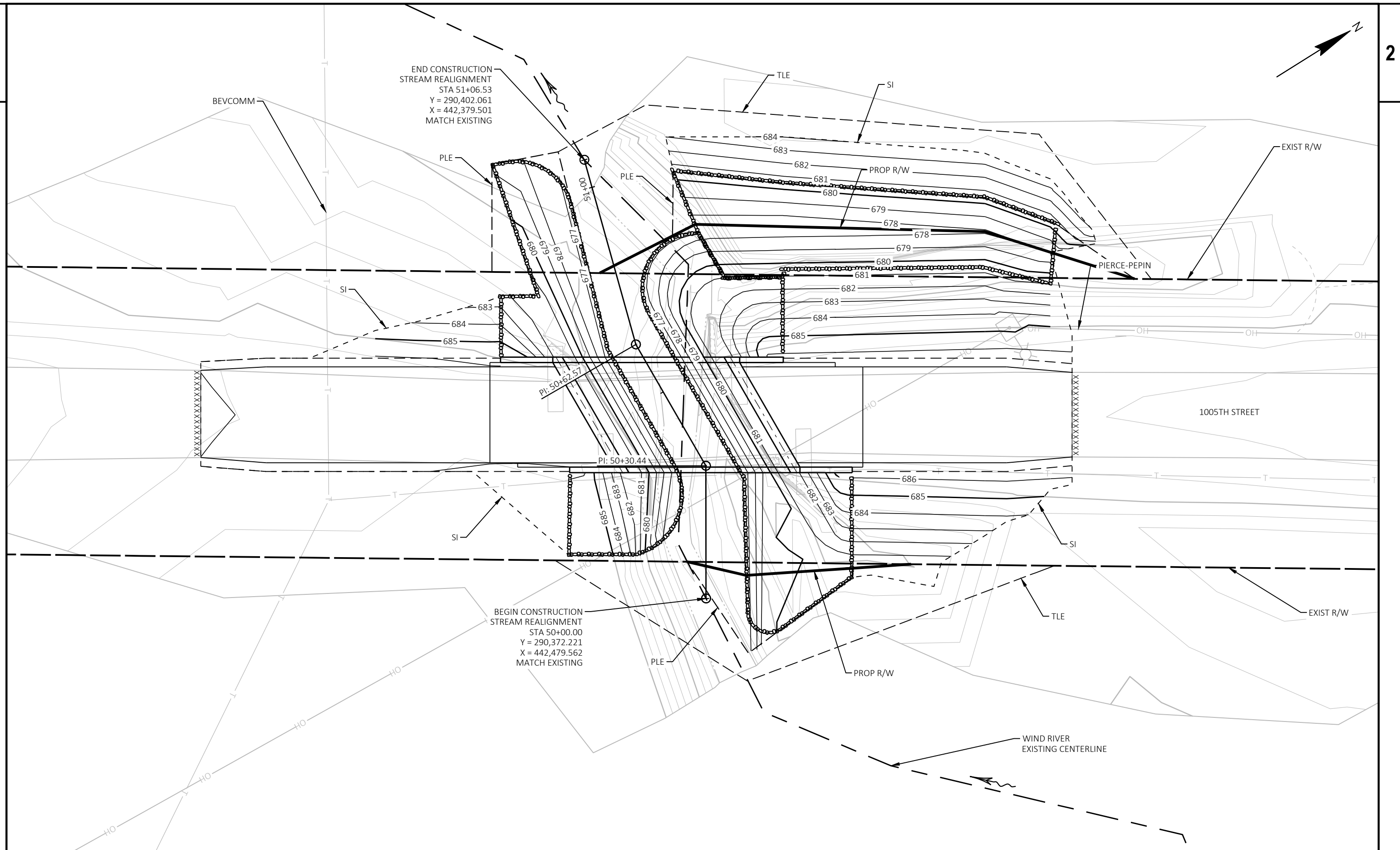
GENERAL NOTES
TYPICAL SECTIONS
PLAN DETAILS

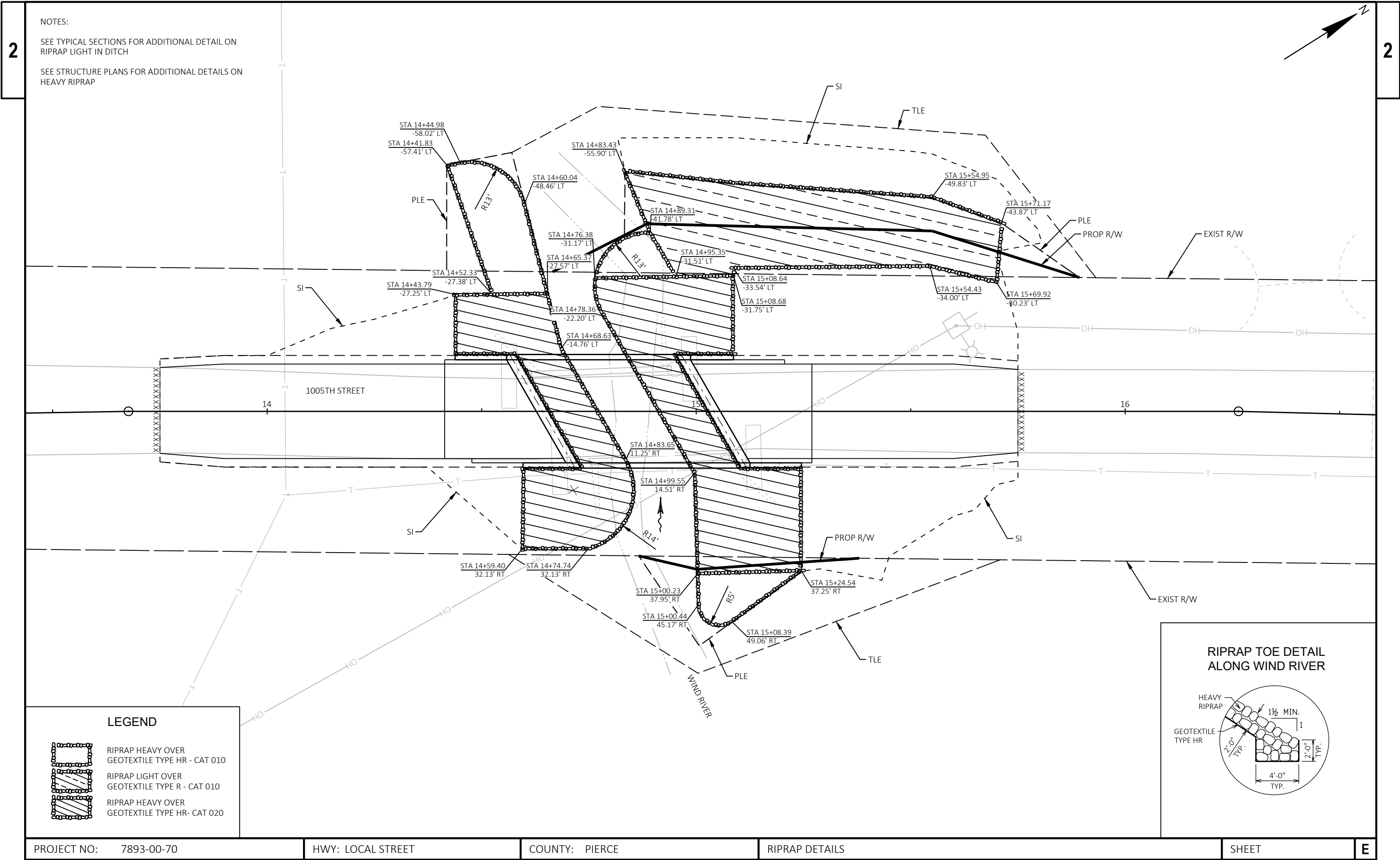
**EXISTING TYPICAL SECTION**

1005TH STREET
STA 13+75.00 - STA 14+67.34
STA 15+00.77 - STA 15+75.00

**FINISHED TYPICAL SECTION**

1005TH STREET
STA 13+75.00 - STA 14+62.72
STA 15+05.61 - STA 15+75.00





Estimate Of Quantities

| 7893-00-70 | | | | | |
|------------|----------|--|------|------------|------------|
| Line | Item | Item Description | Unit | Total | Qty |
| 0002 | 201.0205 | Grubbing | STA | 1.500 | 1.500 |
| 0004 | 203.0250 | Removing Structure Over Waterway Remove Debris (structure) .01 P-47-0054 | EACH | 1.000 | 1.000 |
| 0006 | 205.0100 | Excavation Common | CY | 451.000 | 451.000 |
| 0008 | 206.1001 | Excavation for Structures Bridges (structure) .01 B-47-0233 | EACH | 1.000 | 1.000 |
| 0010 | 210.1500 | Backfill Structure Type A | TON | 240.000 | 240.000 |
| 0012 | 213.0100 | Finishing Roadway (project) .01 7893-00-70 | EACH | 1.000 | 1.000 |
| 0014 | 305.0110 | Base Aggregate Dense 3/4-Inch | TON | 22.000 | 22.000 |
| 0016 | 305.0120 | Base Aggregate Dense 1 1/4-Inch | TON | 220.000 | 220.000 |
| 0018 | 415.0120 | Concrete Pavement 12-Inch | SY | 10.000 | 10.000 |
| 0020 | 415.0410 | Concrete Pavement Approach Slab | SY | 106.000 | 106.000 |
| 0022 | 455.0605 | Tack Coat | GAL | 15.000 | 15.000 |
| 0024 | 465.0105 | Asphaltic Surface | TON | 47.000 | 47.000 |
| 0026 | 502.0100 | Concrete Masonry Bridges | CY | 157.000 | 157.000 |
| 0028 | 502.3200 | Protective Surface Treatment | SY | 121.000 | 121.000 |
| 0030 | 502.3210 | Pigmented Surface Sealer | SY | 64.000 | 64.000 |
| 0032 | 505.0400 | Bar Steel Reinforcement HS Structures | LB | 3,640.000 | 3,640.000 |
| 0034 | 505.0600 | Bar Steel Reinforcement HS Coated Structures | LB | 24,680.000 | 24,680.000 |
| 0036 | 516.0500 | Rubberized Membrane Waterproofing | SY | 18.000 | 18.000 |
| 0038 | 550.1100 | Piling Steel HP 10-Inch X 42 Lb | LF | 525.000 | 525.000 |
| 0040 | 606.0100 | Riprap Light | CY | 56.000 | 56.000 |
| 0042 | 606.0300 | Riprap Heavy | CY | 337.000 | 337.000 |
| 0044 | 612.0406 | Pipe Underdrain Wrapped 6-Inch | LF | 140.000 | 140.000 |
| 0046 | 614.0150 | Anchor Assemblies for Steel Plate Beam Guard | EACH | 4.000 | 4.000 |
| 0048 | 618.0100 | Maintenance and Repair of Haul Roads (project) .01 7893-00-70 | EACH | 1.000 | 1.000 |
| 0050 | 619.1000 | Mobilization | EACH | 1.000 | 1.000 |
| 0052 | 624.0100 | Water | MGAL | 4.000 | 4.000 |
| 0054 | 625.0500 | Salvaged Topsoil | SY | 442.000 | 442.000 |
| 0056 | 628.1504 | Silt Fence | LF | 85.000 | 85.000 |
| 0058 | 628.1520 | Silt Fence Maintenance | LF | 85.000 | 85.000 |
| 0060 | 628.1905 | Mobilizations Erosion Control | EACH | 3.000 | 3.000 |
| 0062 | 628.1910 | Mobilizations Emergency Erosion Control | EACH | 2.000 | 2.000 |
| 0064 | 628.2008 | Erosion Mat Urban Class I Type B | SY | 401.000 | 401.000 |
| 0066 | 628.6005 | Turbidity Barriers | SY | 207.000 | 207.000 |
| 0068 | 629.0210 | Fertilizer Type B | CWT | 0.500 | 0.500 |
| 0070 | 630.0110 | Seeding Mixture No. 10 | LB | 7.000 | 7.000 |
| 0072 | 630.0500 | Seed Water | MGAL | 11.000 | 11.000 |
| 0074 | 634.0614 | Posts Wood 4x6-Inch X 14-FT | EACH | 4.000 | 4.000 |
| 0076 | 637.2230 | Signs Type II Reflective F | SF | 12.000 | 12.000 |
| 0078 | 638.2602 | Removing Signs Type II | EACH | 4.000 | 4.000 |
| 0080 | 638.3000 | Removing Small Sign Supports | EACH | 4.000 | 4.000 |
| 0082 | 642.5001 | Field Office Type B | EACH | 1.000 | 1.000 |
| 0084 | 643.0420 | Traffic Control Barricades Type III | DAY | 1,072.000 | 1,072.000 |
| 0086 | 643.0705 | Traffic Control Warning Lights Type A | DAY | 2,144.000 | 2,144.000 |
| 0088 | 643.0900 | Traffic Control Signs | DAY | 938.000 | 938.000 |
| 0090 | 643.5000 | Traffic Control | EACH | 1.000 | 1.000 |
| 0092 | 645.0111 | Geotextile Type DF Schedule A | SY | 60.000 | 60.000 |
| 0094 | 645.0120 | Geotextile Type HR | SY | 514.000 | 514.000 |
| 0096 | 645.0130 | Geotextile Type R | SY | 190.000 | 190.000 |
| 0098 | 650.5000 | Construction Staking Base | LF | 171.000 | 171.000 |

Estimate Of Quantities

| 7893-00-70 | | | | | |
|------------|----------|--|------|---------|---------|
| Line | Item | Item Description | Unit | Total | Qty |
| 0100 | 650.6501 | Construction Staking Structure Layout (structure) .01 B-47-0233 | EACH | 1.000 | 1.000 |
| 0102 | 650.9911 | Construction Staking Supplemental Control (project) .01 7893-00-70 | EACH | 1.000 | 1.000 |
| 0104 | 650.9920 | Construction Staking Slope Stakes | LF | 171.000 | 171.000 |
| 0106 | 690.0150 | Sawing Asphalt | LF | 40.000 | 40.000 |
| 0108 | 715.0502 | Incentive Strength Concrete Structures | DOL | 950.000 | 950.000 |
| 0110 | 715.0720 | Incentive Compressive Strength Concrete Pavement | DOL | 500.000 | 500.000 |
| 0112 | ASP.1T0A | On-the-Job Training Apprentice at \$5.00/HR | HRS | 300.000 | 300.000 |
| 0114 | ASP.1T0G | On-the-Job Training Graduate at \$5.00/HR | HRS | 300.000 | 300.000 |

3

| GRUBBING | | |
|-------------------|----------|-----------------------------|
| STATION - STATION | LOCATION | 201.0205 GRUBBING STA |
| 14+44 - 14+69 | | 0.5 |
| 14+78 - 15+75 | | 1 |
| ITEM TOTALS | | 1.5 |

| BASE AGGREGATE DENSE | | | | |
|----------------------|----------|-----------------------------|-------------------------------|---------|
| STATION - STATION | LOCATION | 305.0110 3/4-INCH TON | 305.0120 1 1/4-INCH TON | REMARKS |
| 13+75 - 14+70 | | 12 | 124 | |
| 14+99 - 15+75 | | 10 | 96 | |
| ITEM TOTALS | | 22 | 220 | |

| ASPHALTIC SURFACE ITEMS | | | |
|-------------------------|----------|--------------------------------|---|
| STATION - STATION | LOCATION | 455.0605 * TACK COAT GAL | 465.0105 ASPHALTIC SURFACE TON |
| 13+75 - 14+70 | | 9 | 27 |
| 14+99 - 15+75 | | 6 | 20 |
| ITEM TOTAL | | 15 | 47 |

* APPLICATION RATE 0.05 GAL/SY

3

| DIVISION | FROM/TO STATION | LOCATION | 205.0100 COMMON EXCAVATION (1) | SALVAGED/UNUSABLE PAVEMENT MATERIAL (3) | AVAILABLE MATERIAL (4) | UNEXPANDED FILL | EXPANDED FILL (5) | MASS ORDINATE +/- (6) | WASTE |
|---------------------|--------------------|------------|---|---|------------------------------|--------------------|----------------------|--------------------------|-------|
| | | | CUT (2) | | | | FACTOR 1.25 | | |
| DIVISION 1 | | | | | | | | | |
| 1005TH STREET | 13+75.00/15+75.00 | | 363 | 28 | 335 | 132 | 156 | 150 | 150 |
| STREAM REALIGNMENT | | WIND RIVER | 88 | 0 | 88 | 13 | 16 | 76 | 76 |
| DIVISION 1 SUBTOTAL | | | 451 | 28 | 423 | 145 | 172 | 226 | 226 |
| GRAND TOTAL | | | 451 | 28 | 423 | 145 | 172 | 226 | 226 |
| TOTAL COMMON EXC | | | 451 | | | | | | |

NOTES:
(1) COMMON EXCAVATION IS THE SUM OF THE CUT AND EBS EXCAVATION COLUMNS. ITEM NUMBER 205.0100
(2) SALVAGED/UNSUABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.
(3) SALVAGED/UNUSABLE PAVEMENT MATERIAL
(4) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSUABLE PAVEMENT MATERIAL
(5) EXPANDED FILL FACTOR = 1.25
(6) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.

| CONCRETE PAVEMENT | | | | |
|-------------------|----------|---------------------|---------------------------|---------|
| | | 415.0120 12-INCH | 415.0410 APPROACH SLAB | |
| STATION - STATION | LOCATION | SY | SY | REMARKS |
| 13+75 - 14+70 | | 5 | 53 | |
| 14+99 - 15+75 | | 5 | 53 | |
| ITEM TOTALS | | 10 | 106 | |

| WATER | | |
|------------------|----------|------------------|
| STATION -STATION | LOCATION | 624.0100 MGAL |
| 13+75 - 14+70 | | 2 |
| 14+99 - 15+75 | | 2 |
| ITEM TOTALS | | 4 |

NOTE: TABLE QUANTITIES ARE CATEGORY 0010 UNLESS OTHERWISE NOTED

3

| EROSION CONTROL ITEMS | | | | | | | | | |
|--|----------|------------------------------|---|--|-----------------------------------|-------------------------------------|---|--|--|
| STATION | LOCATION | 628.1504 SILT FENCE LF | 628.1520 SILT FENCE MAINTENANCE LF | 628.2008 EROSION MAT URBAN CLASS 1 TYPE B SY | 606.0100 RIPRAP LIGHT CY | 606.0300 * RIPRAP HEAVY CY | 645.0120 * GEOTEXTILE TYPE HR SY | 645.0130 GEOTEXTILE TYPE R SY | 628.6005 TURBIDITY BARRIER SY |
| 13+75 - 14+70 | | 58 | 58 | 61 | -- | 45 | 75 | -- | 97 |
| 14+99 - 15+75 | | 27 | 27 | 340 | 56 | 42 | 64 | 190 | 110 |
| ITEM TOTALS | | 85 | 85 | 401 | 56 | 87 | 139 | 190 | 207 |
| * ADDITIONAL QUANTITIES SHOWN IN THE STRUCTURE PLANS | | | | | | | | | |

| MOBILIZATIONS | | |
|---------------|---|--|
| PROJECT | 628.1905 MOBILIZATION EROSION CONTROL EACH | 628.1910 MOBILIZATION EMERGENCY EROSION CONTROL EACH |
| 7893-00-70 | 3 | 2 |
| ITEM TOTAL | | 3 |
| | | 2 |

3

| TOPSOIL, FERTILIZER, AND SEEDING | | | | | |
|----------------------------------|----------|---------------------------------------|---|--|-----------------------------------|
| STATION - STATION | LOCATION | 625.0500 SALVAGED TOPSOIL SY | 629.0210 FERTILIZER TYPE B CWT | 630.0110 SEEDING MIXTURE NO. 10 LB | 630.0500 SEED WATER MGAL |
| 13+75 - 14+70 | LT | 61 | 0.1 | 1.0 | 2 |
| 14+99 - 15+75 | RT | 340 | 0.3 | 5.0 | 8 |
| UNDISTRIBUTED | | 41 | 0.1 | 1.0 | 1 |
| ITEM TOTALS | | 442 | 0.5 | 7 | 11 |

| PERMANENT SIGNING | | | | | | | | | |
|-------------------|--------|------------------|--------------|------------------------------------|--|---|--|--|---------|
| STATION | OFFSET | LOCATION | SIGN CODE | SIGN SIZE (W x H) IN x IN | 637.2230 SIGNS TYPE II REFLECTIVE F SF | 634.0614 POSTS WOOD 4X6-INCH X 14-FT EACH | 638.2602 REMOVING SIGNS TYPE II EACH | 638.3000 REMOVING SMALL SIGN SUPPORTS EACH | REMARKS |
| 14+41 | 13' LT | SW BRIDGE CORNER | W5-52L | 12X36 | 3 | 1 | 1 | 1 | |
| 14+57 | 13' RT | SE BRIDGE CORNER | W5-52R | 12X36 | 3 | 1 | 1 | 1 | |
| 15+11 | 13' LT | NW BRIDGE CORNER | W5-52R | 12X36 | 3 | 1 | 1 | 1 | |
| 15+28 | 13' RT | NE BRIDGE CORNER | W5-52L | 12X36 | 3 | 1 | 1 | 1 | |
| ITEM TOTAL | | | | | 12 | 4 | 4 | 4 | |

| TRAFFIC CONTROL | | | | |
|--------------------|--|---|---------------------------|--|
| LOCATION | 643.0420 BARRICADES TYPE III DAYS | 643.0705 WARNING LIGHTS TYPE A DAYS | 643.0900 SIGNS DAYS | 643.5000 TRAFFIC CONTROL EACH |
| PROJECT 7893-00-70 | 1072 | 2144 | 938 | 1 |
| ITEM TOTALS | 1072 | 2144 | 938 | 1 |

| CONSTRUCTION STAKING | | | | |
|----------------------|------------------------|---|---|-----------------------------------|
| STATION - STATION | 650.5000 BASE LF | 650.6501 STRUCTURE LAYOUT (B-47-0233) EACH | 650.9911 SUPPLEMENTAL CONTROL (7893-00-70) EACH | 650.9920 SLOPE STAKES LF |
| 13+75 - 14+70 | 95 | -- | -- | 95 |
| 14+99 - 15+75 | 76 | -- | -- | 76 |
| PROJECT 7893-00-70 | -- | 1 | 1 | -- |
| ITEM TOTALS | 171 | 1 | 1 | 171 |

| SAWING | | | |
|---|----------|---------------------------|---------|
| STATION | LOCATION | 690.0150 ASPHALT LF | REMARKS |
| 13+75 | | 20 | |
| 15+75 | | 20 | |
| ITEM TOTAL | | 40 | |
| NOTE: TABLE QUANTITIES ARE CATEGORY 0010 UNLESS OTHERWISE NOTED | | | |

TRANSPORTATION PROJECT PLAT NO: 7893-00-00-4.02

THAT PART OF THE NORTHEAST QUARTER OF THE NORTHEAST QUARTER OF SECTION 14, TOWNSHIP 25 NORTH, RANGE 19 WEST, PIERCE COUNTY, WISCONSIN

RELOCATION ORDER 1005TH ST, STRUCTURE B-47-0233, T DIAMOND BLUFF, PIERCE COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE TOWN OF DIAMOND BLUFF AND THE DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 60.50 AND 82.12, WISCONSIN STATUTES, THE TOWN HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS Laid OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE TOWN FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE TOWN OF DIAMOND BLUFF, PURSUANT TO THE PROVISIONS OF SECTION 60.50 AND 82.12, WISCONSIN STATUTES.

SCHEDULE OF LANDS & INTEREST REQUIRED

| OWNER ID | OWNER (S) | INTEREST REQUIRED | R/W NEW (SQ. FT.) | R/W EXIST (SQ. FT.) | R/W TOTAL (SQ. FT.) | PLE (SQ. FT.) | TLE (SQ. FT.) |
|----------|------------------|-------------------|-------------------|---------------------|---------------------|---------------|---------------|
| 1 | GLEN L. HANSON | FEE, PLE, TLE | 1104 | | 1104 | 1243 | 2966 |
| 2 | JENNIFER A ROUPE | FEE, PLE, TLE | 6 | | 6 | 518 | 438 |

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE TOWN OF DIAMOND BLUFF

CONVENTIONAL SYMBOLS

| | | | |
|---|-----|---|-----|
| SECTION LINE | --- | SECTION CORNER SYMBOL | |
| QUARTER LINE | --- | SECTION CORNER MONUMENT | |
| SIXTEENTH LINE | --- | NEW R/W LINE | --- |
| NEW R/W LINE | --- | EXISTING R/W OR HE LINE | --- |
| EXISTING R/W OR HE LINE | --- | PROPERTY LINE | --- |
| PROPERTY LINE | --- | LOT, TIE & OTHER MINOR LINES | --- |
| LOT, TIE & OTHER MINOR LINES | --- | SLOPE INTERCEPT | --- |
| SLOPE INTERCEPT | --- | NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER) | |
| NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER) | | TEMPORARY LIMITED EASEMENT AREA | |
| TEMPORARY LIMITED EASEMENT AREA | | EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT) | |
| EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT) | | | |

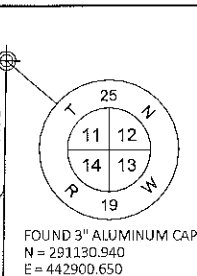
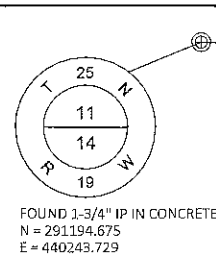
CONVENTIONAL ABBREVIATIONS

| | | | |
|--------------------------|------|------------------------|--------|
| ACCESS RIGHTS | AR | PAGE | P |
| ACRES | AC | PERMANENT LIMITED | PLE |
| ARC LENGTH | A | EASEMENT | |
| BLOCK | BLK | POINT OF BEGINNING | POB |
| CENTRAL ANGLE | DEL | POINT OF INTERSECTION | PI |
| CENTERLINE | CL | PROPERTY LINE | PL |
| CERTIFIED SURVEY MAP | CSM | RADIUS | R |
| CHORD LENGTH | C | RECORDED AS | (100') |
| COUNTY | CO | REEL / IMAGE | R/I |
| DISTANCE | DIST | REFERENCE LINE | RL |
| CORNER | COR | RIGHT OF WAY | RW |
| DOCUMENT NUMBER | DOC | SECTION | SEC |
| EASEMENT | EASE | SQUARE FEET | SF |
| EXISTING | EX | STATION | STA |
| GRID NORTH | GN | TEMPORARY LIMITED | TLE |
| IDENTIFICATION | ID | EASEMENT | |
| LAND CONTRACT | LC | TRANSPORTATION PROJECT | TPP |
| MONUMENT | MON | PLAT | |
| NATIONAL GEODETIC SURVEY | NGS | VOLUME | V |
| NUMBER | NO | | |

CURVE DATA ABBREVIATIONS

| | |
|--------------------|---------|
| LONG CHORD | LCH |
| LONG CHORD BEARING | LCB |
| RADIUS | R |
| DEGREE OF CURVE | D |
| CENTRAL ANGLE | Δ/DELTA |
| LENGTH OF CURVE | L |
| TANGENT | T |
| DIRECTION AHEAD | DA |
| DIRECTION BACK | DB |

| POINT | STATION | OFFSET | Y(NORTHING) | X(EASTING) |
|-------|----------|--------|-------------|-------------|
| 100 | 14+66.55 | -32.52 | 290391.232' | 442403.357' |
| 101 | 14+88.50 | -43.71 | 290415.778' | 442405.549' |
| 102 | 15+55.00 | -42.03 | 290471.203' | 442442.329' |
| 103 | 15+89.33 | -31.19 | 290494.516' | 442469.763' |
| 104 | 15+38.01 | 34.26 | 290416.252' | 442497.908' |
| 105 | 15+00.20 | 36.83 | 290382.862' | 442479.983' |
| 106 | 14+94.07 | 35.42 | 290378.418' | 442475.528' |
| 107 | 14+93.49 | 33.78 | 290378.803' | 442473.830' |
| 108 | 14+86.61 | 33.7 | 290373.017' | 442470.110' |
| 200 | 14+41.83 | -32.79 | 290370.443' | 442389.991' |
| 201 | 14+41.83 | -57.41 | 290383.535' | 442369.135' |
| 202 | 14+51.75 | -59.32 | 290392.946' | 442372.787' |
| 203 | 14+57.01 | -60.34 | 290397.948' | 442374.727' |
| 204 | 14+65.41 | -32.53 | 290390.277' | 442402.743' |
| 205 | 14+58.49 | -47.33 | 290392.276' | 442386.529' |
| 206 | 14+83.27 | -41.04 | 290409.922' | 442405.026' |
| 207 | 14+83.27 | -55.9 | 290417.822' | 442392.442' |
| 208 | 15+08.67 | -53.16 | 290437.882' | 442408.267' |
| 209 | 15+55.00 | -49.83 | 290475.351' | 442435.720' |
| 210 | 15+71.17 | -43.87 | 290485.877' | 442449.365' |
| 211 | 15+24.54 | 35.18 | 290404.355' | 442491.521' |
| 212 | 15+24.54 | 37.25 | 290403.252' | 442493.278' |
| 213 | 15+00.71 | 54.67 | 290372.759' | 442494.836' |



623991
PIERCE COUNTY
REGISTER OF DEEDS
ANGELA HOVEN
RECEIVED FOR RECORDING
10/29/2024 02:30 PM
RECORDING FEE: 25.00
PAGES: 1
RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 7893-00-00

| FROM | TO | DISTANCE(FT) | BEARING |
|------|-----|--------------|-------------|
| 100 | 101 | 24.64 | N5°06'15"E |
| 101 | 102 | 66.52 | N33°31'05"E |
| 102 | 103 | 36.00 | S49°38'35"W |
| 103 | 100 | 122.79 | N32°44'20"E |
| 104 | 105 | 37.9 | S28°13'39"W |
| 105 | 106 | 6.29 | S45°05'06"W |
| 106 | 107 | 1.74 | N77°14'30"W |
| 107 | 104 | 44.52 | S32°44'20"W |
| 108 | 108 | 7.65 | S45°05'06"W |

| FROM | TO | DISTANCE(FT) | BEARING |
|------|---------------|-----------------|-------------|
| 200 | 201 | 24.62 | N57°52'54"W |
| 201 | 202 | 10.09 | N21°12'21"E |
| 202 | 205 LCH 13.76 | LCB S87°12'27"E | |
| | R 102.45 | DELTA 07°42'02" | |
| 205 | 204 LCH 16.34 | LCB S82°58'26"E | |
| | R 1220.96 | DELTA 00°46'00" | |
| 202 | 203 | 5.36 | N21°12'21"E |
| 203 | 204 | 29.05 | S74°41'19"E |
| 100 | 206 | 18.76 | N5°06'15"E |
| 206 | 207 | 14.86 | N57°52'54"W |
| 207 | 208 | 25.39 | N38°16'11"E |
| 208 | 210 | 17.28 | N52°17'48"E |
| 210 | 103 | 22.15 | N67°02'50"E |
| 104 | 211 | 13.5 | S28°13'39"W |
| 211 | 212 | 2.07 | S57°52'54"E |
| 212 | 213 | 30.53 | S02°55'29"E |
| 213 | 106 LCH 20.12 | LCB N73°39'47"W | |
| | R 175.10 | DELTA 06°35'16" | |
| 213 | 108 | 24.73 | S89°24'03"W |

THE NOTES, CONVENTIONAL SIGNS, AND ABBREVIATIONS ARE ASSOCIATED WITH EACH TRANSPORTATION PROJECT PLAT FOR PROJECT 7893-00-00

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), PIERCE COUNTY, NAD83(1991), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, CENTERLINE OF EXISTING PAYMENTS AND/OR EXISTING OCCUPATIONAL LINES.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

A PERMANENT LIMITED EASEMENT (PLE) IS A RIGHT FOR CONSTRUCTION AND MAINTENANCE PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE, BUT WITHOUT PREJUDICE TO THE OWNER'S RIGHTS TO MAKE OR CONSTRUCT IMPROVEMENTS ON SAID LANDS OR TO FLATTEN THE SLOPES, PROVIDING SAID ACTIVITIES WILL NOT IMPAIR OR OTHERWISE ADVERSELY AFFECT THE HIGHWAY FACILITIES.

PROPERTY LINES SHOWN ON THIS PLAT FOR PROPERTIES BEING IMPACTED ARE DRAWN FROM DATA DERIVED FROM FILED/RECORDED MAPS AND DOCUMENTS OF PUBLIC RECORD. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN EAU CLAIRE

PARCEL AND UTILITY IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE TPP DETAIL PAGES.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE: CERTIFIED SURVEY MAP RECORDED IN VOLUME 4 OF C.S.M. PAGE 67 (DOCUMENT NUMBER 342390), AND FROM EXISTING ROADWAY INFORMATION.

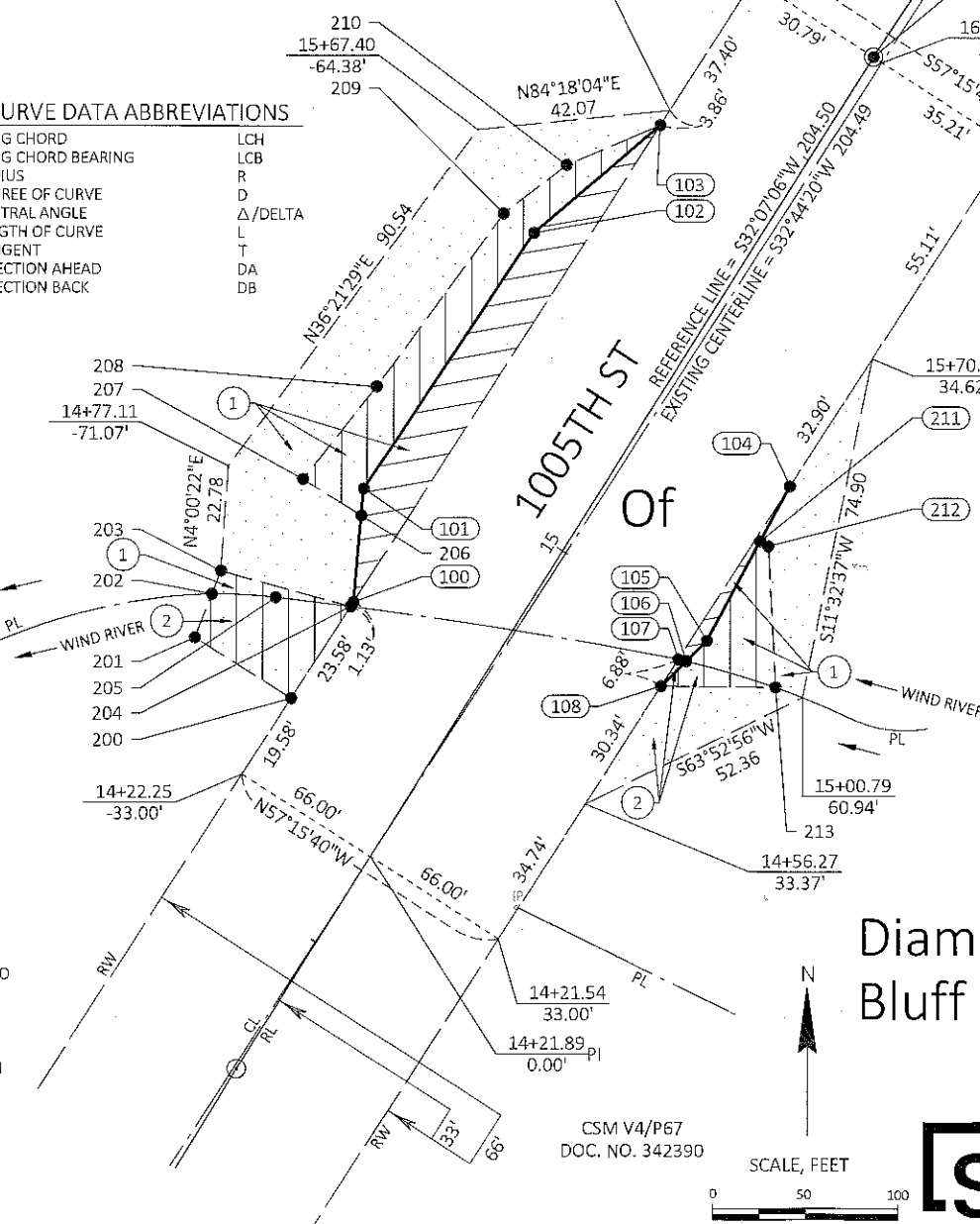
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APPRAISAL PLAT DATE:

PLOT DATE: 10/25/2024 6:45 AM

PLOT BY: VANESSA HEFTI

PLOT NAME:

PLOT SCALE:



END PROJECT
STA 15+75.00

STRUCTURE B-47-233
STA 14+62.73 - STA 15+05.62

BEGIN PROJECT
STA 13+75.00
X = 442,382.225
Y = 290,296.406

Diamond Bluff

I, MICHAEL J. FISCHER, JR., PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF PIERCE COUNTY, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: *Michael J. Fischer, Jr.* DATE: 10/25/24

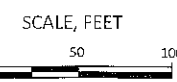
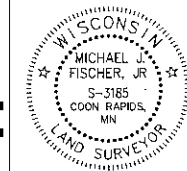
PRINT NAME: MICHAEL J. FISCHER, JR.

REGISTRATION NUMBER: S-3185

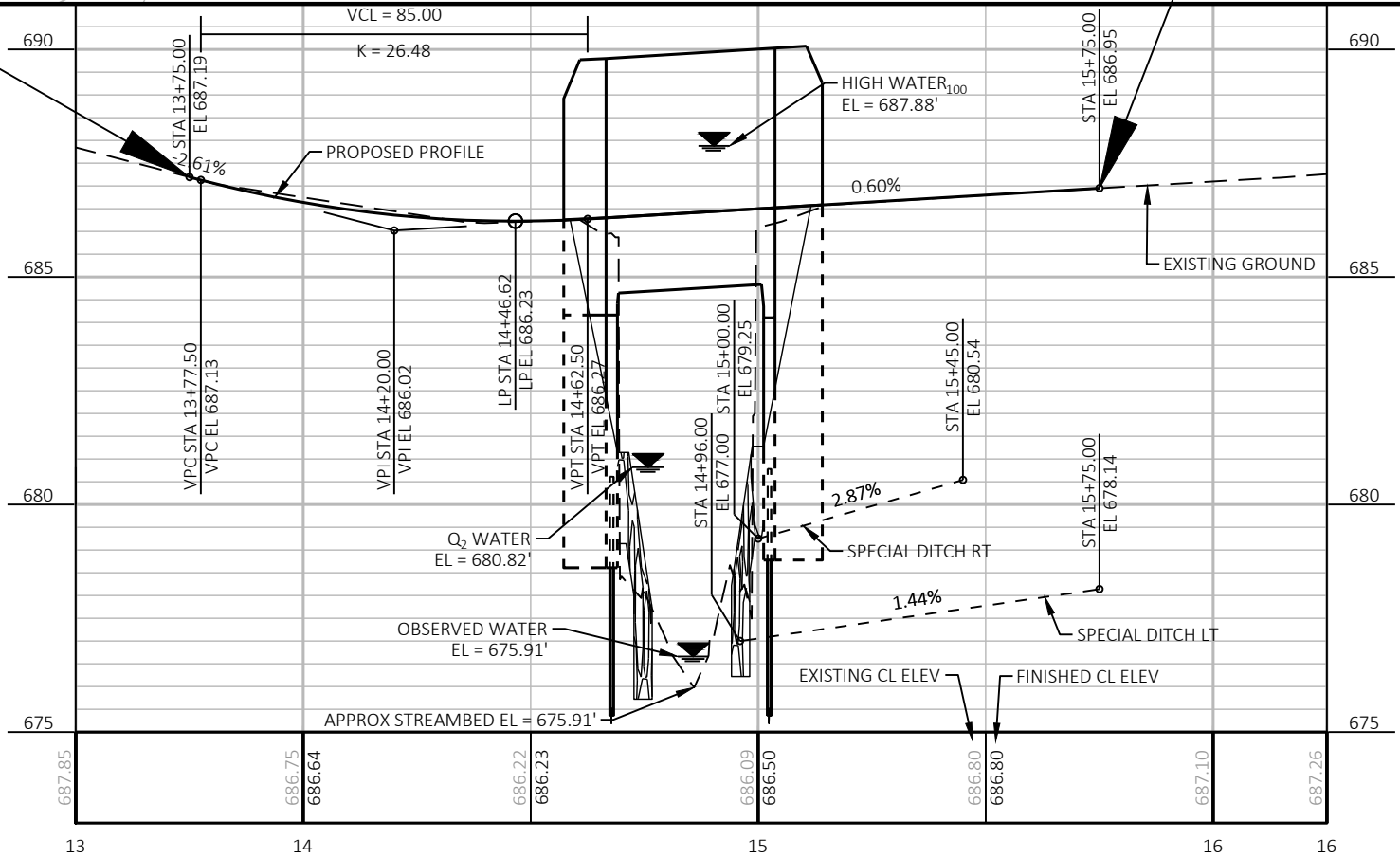
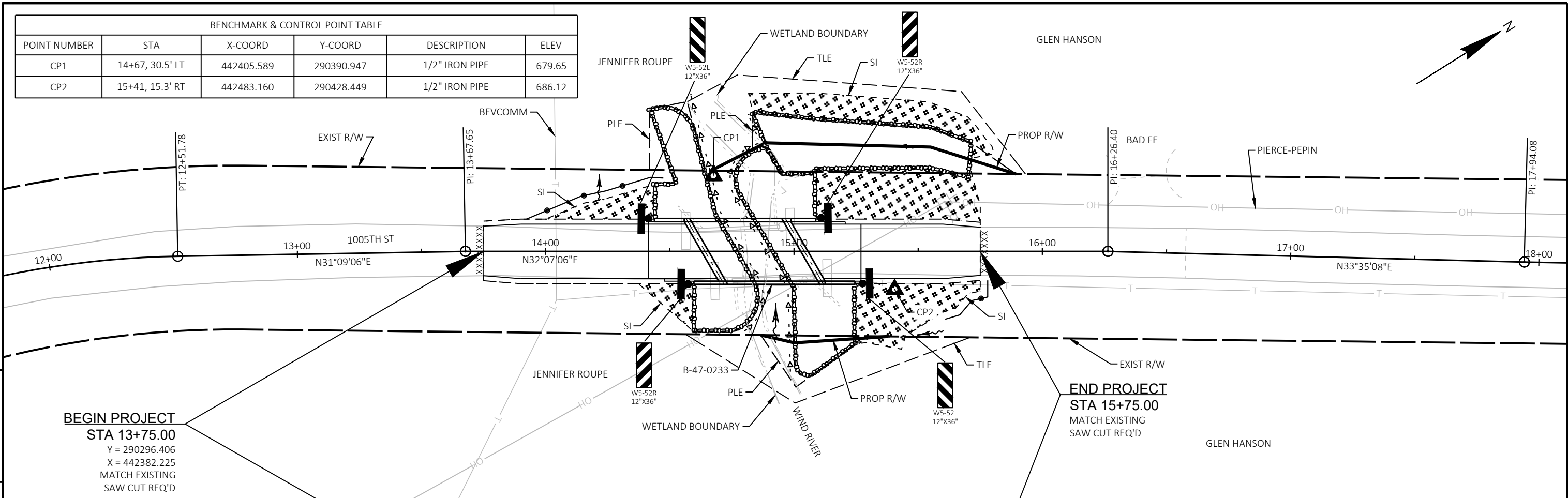
THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE TOWN OF DIAMOND BLUFF

SIGNATURE: *Jeff Holst* DATE: 10/29/24

PRINT NAME: JEFF HOLST, TOWN OF DIAMOND BLUFF, CHAIR



| BENCHMARK & CONTROL POINT TABLE | | | | | |
|---------------------------------|-----------------|------------|------------|----------------|--------|
| POINT NUMBER | STA | X-COORD | Y-COORD | DESCRIPTION | ELEV |
| CP1 | 14+67, 30.5' LT | 442405.589 | 290390.947 | 1/2" IRON PIPE | 679.65 |
| CP2 | 15+41, 15.3' RT | 442483.160 | 290428.449 | 1/2" IRON PIPE | 686.12 |



| LEGEND | |
|--------|-----------------------------------|
| ##### | EROSION MAT URBAN CLASS I, TYPE B |
| ●—● | SILT FENCE |
| ▷—▷ | TURBIDITY BARRIER |
| ~→ | SURFACE WATER FLOW |

STA 14+84
STRUCTURE B-47-0233 REQUIRED
CONCRETE FLAT SLAB STRUCTURE
24' CLEAR ROADWAY
30° SKEW RHF

STA 14+84
REMOVE STRUCTURE P-47-0054
SINGLE SPAN CONCRETE STRUCTURE
37' OVERALL LENGTH
22' OVERALL WIDTH

| | | | | | |
|------------------------|-------------------|----------------|-----------------------------|-------|---|
| PROJECT NO: 7893-00-70 | HWY: LOCAL STREET | COUNTY: PIERCE | PLAN AND PROFILE: 1005TH ST | SHEET | E |
|------------------------|-------------------|----------------|-----------------------------|-------|---|

Standard Detail Drawing List

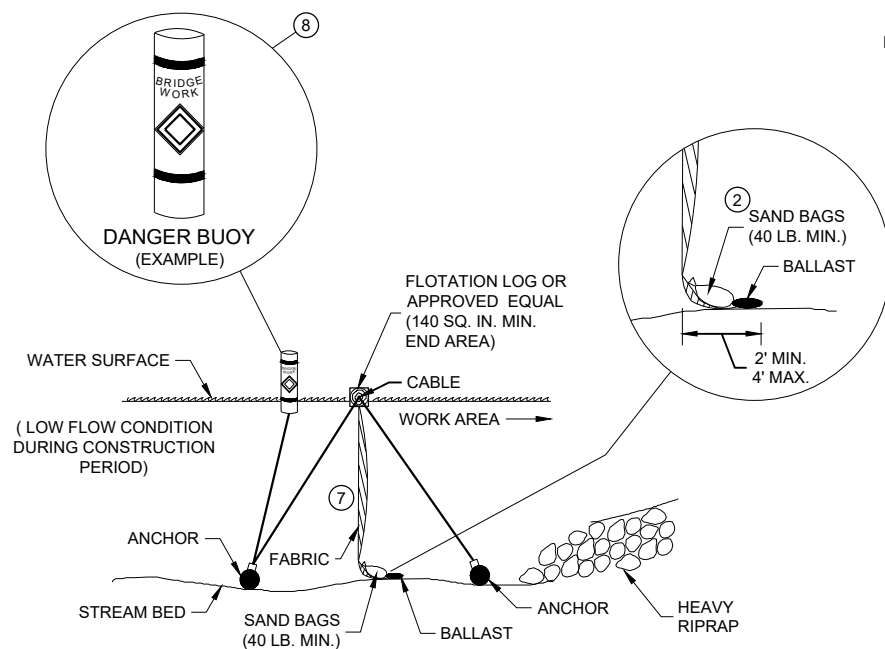
| | |
|-----------|---|
| 08E09-06 | SILT FENCE |
| 08E11-02 | TURBIDITY BARRIER |
| 12A03-10 | NAME PLATE (STRUCTURES) |
| 13B02-09A | CONCRETE PAVEMENT APPROACH SLAB |
| 15C02-09A | BARRICADES AND SIGNS FOR MAINLINE CLOSURES |
| 15C02-09B | BARRICADES AND SIGNS FOR VARIOUS CLOSURES |
| 15C06-12 | SIGNING & MARKING FOR TWO LANE BRIDGES |
| 15C11-10B | CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS |



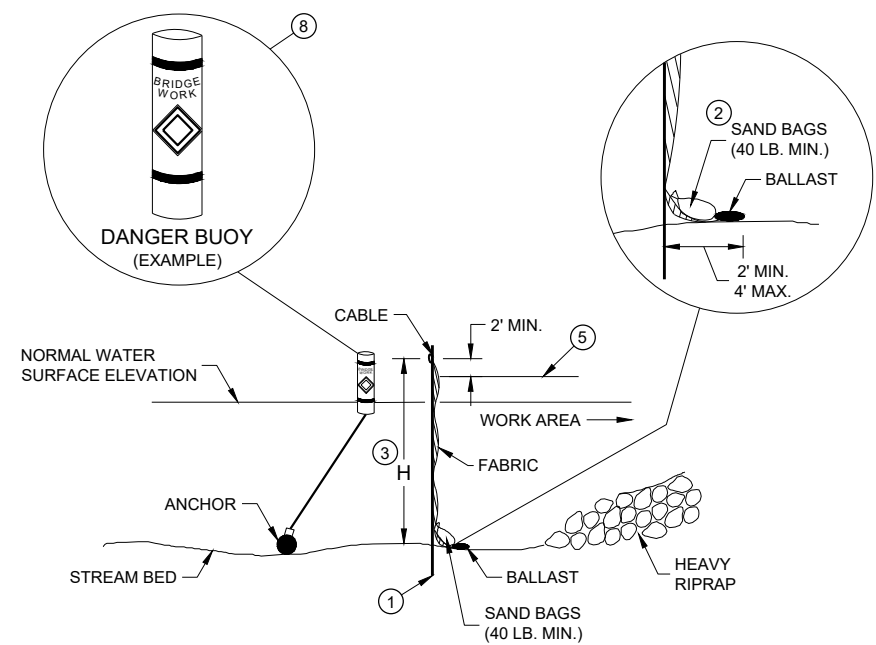
- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1½" X 1½" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



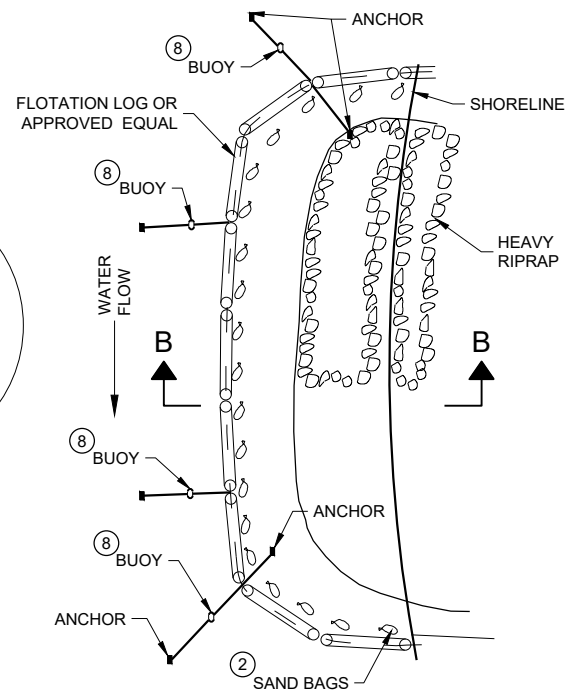
| | |
|--|---|
| <div>SILT FENCE</div> | |
| <div>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</div> | |
| <div>APPROVED</div> | |
| <div>4-29-05</div> | <div>/S/ Beth Canestra</div> |
| <div>DATE</div> | <div>CHIEF ROADWAY DEVELOPMENT ENGINEER</div> |
| <div>FHWA</div> | |



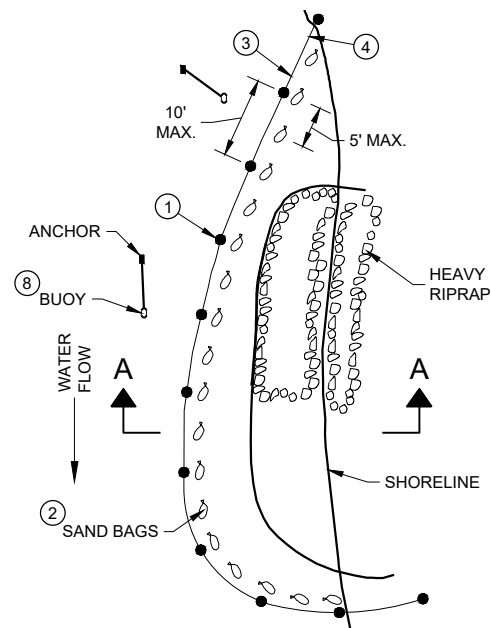
SECTION B - B

**TURBIDITY BARRIER - FLOAT ALTERNATIVE
CAUTION - SEE NOTE 6**

SECTION A - A

TURBIDITY BARRIER - STANDARD POST INSTALLATION

PLAN VIEW



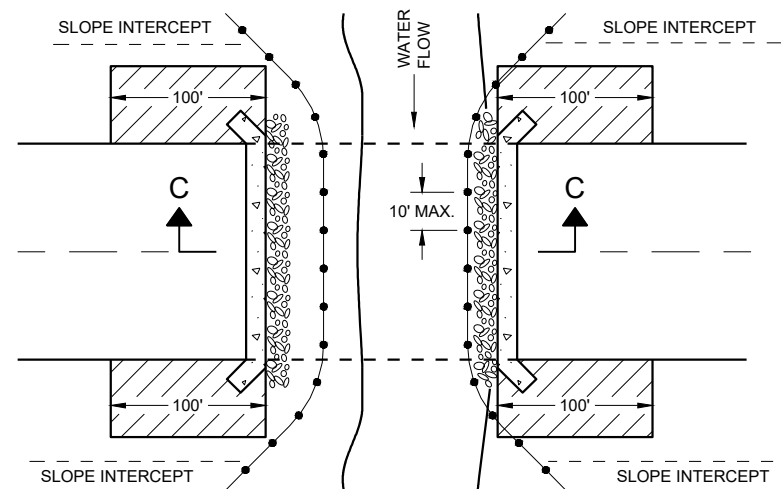
PLAN VIEW

TURBIDITY BARRIER PLACEMENT DETAILS**GENERAL NOTES**

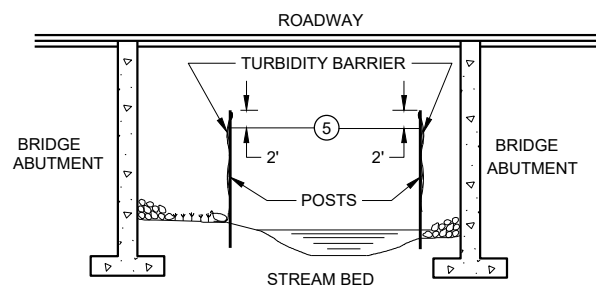
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TURBIDITY BARRIER MAY BE REMOVED AT THE ENGINEERS DISCRETION, WHEN PERMANENT EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

- 1 DRIVEN STEEL POSTS, PIPES, OR CHANNELS. LENGTH SHALL BE SUFFICIENT TO SECURELY SUPPORT BARRIER AT HIGH WATER ELEVATIONS.
- 2 SAND BAGS TO BE USED AS ADDITIONAL BALLAST WHEN ORDERED BY THE ENGINEER TO MEET ADVERSE FIELD CONDITIONS. SPACE AS APPROPRIATE FOR SITE CONDITIONS.
- 3 WHEN BARRIER HEIGHT "H" EXCEEDS 8 FEET, POST SPACING MAY NEED TO BE DECREASED.
- 4 IN WATERWAYS SUBJECT TO FLUCTUATING WATER ELEVATIONS, PROVISIONS SHOULD BE MADE TO ALLOW THE WATER TO EQUALIZE ON EACH SIDE OF THE BARRIER. THIS MAY BE ACCOMPLISHED BY LEAVING A PORTION OF THE BARRIER OPEN ON THE UPSTREAM END.
- 5 ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION PERIOD. MINIMUM BARRIER HEIGHT SHALL BE 2' GREATER THAN EITHER THE Q2 ELEVATION OR THE ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION, WHICHEVER IS GREATER.
- 6 FLOAT ALTERNATIVE WILL ONLY BE ALLOWED WITH WRITTEN APPROVAL OF THE ENGINEER, AND IS MEANT FOR LOCATIONS WHERE BEDROCK PREVENTS THE INSTALLATION OF POSTS.
- 7 ALLOW SUFFICIENT SLACK VERTICALLY AND HORIZONTALLY SO THAT SEDIMENT BUILD UP WILL NOT SEPARATE OR LOWER THE TURBIDITY BARRIER.
- 8 USE AS DIRECTED BY COAST GUARD OR DNR PERMIT WHEN WORKING IN NAVIGABLE WATERWAYS.



PLAN VIEW



SECTION C - C

**TURBIDITY BARRIER DETAIL SHOWING
TYPICAL PLACEMENT AT STRUCTURES****TURBIDITY BARRIER**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

6/4/02

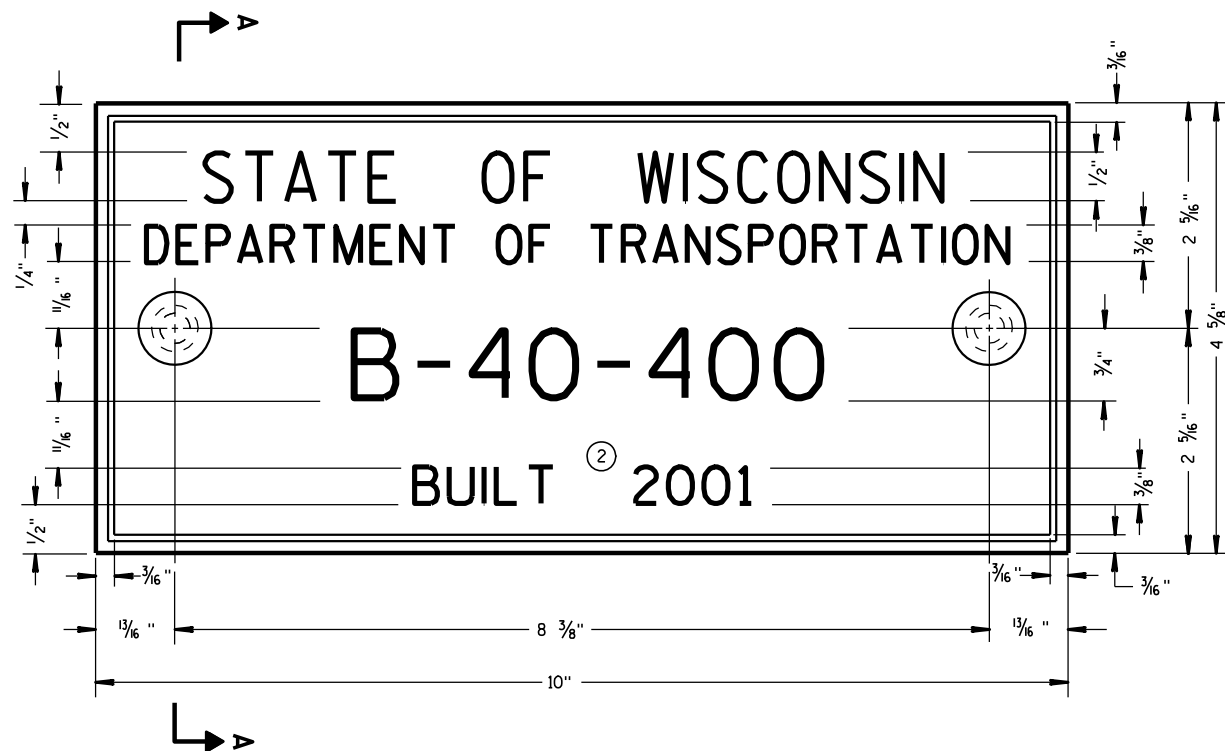
DATE

FHWA

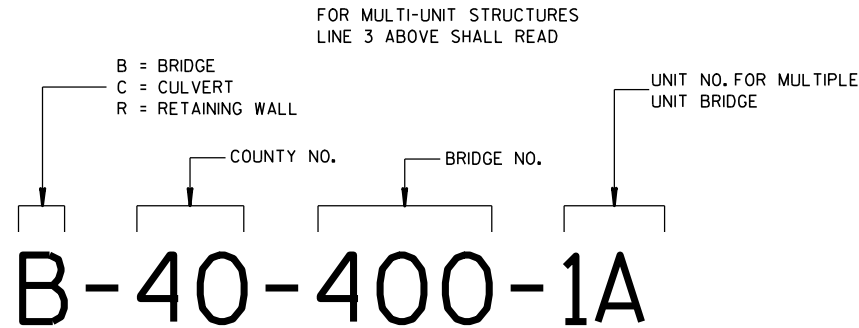
/S/ Beth Canestra

CHIEF ROADWAY DEVELOPMENT

ENGINEER



TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)



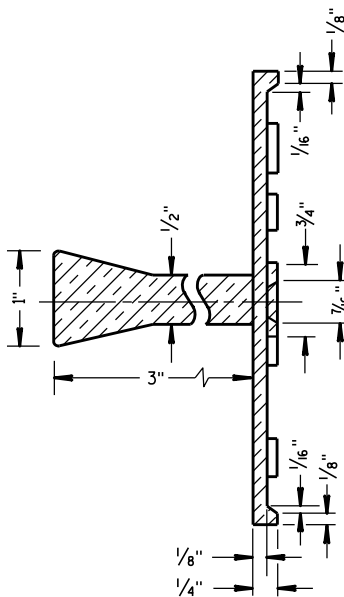
NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES

GENERAL NOTES

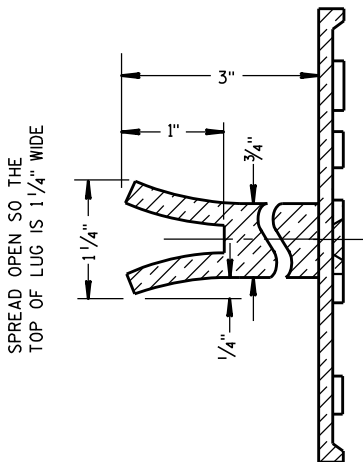
NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

- ① EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- ② REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.

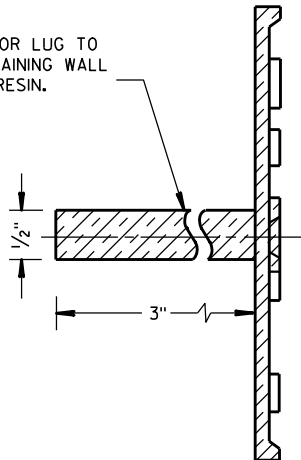


SECTION A-A



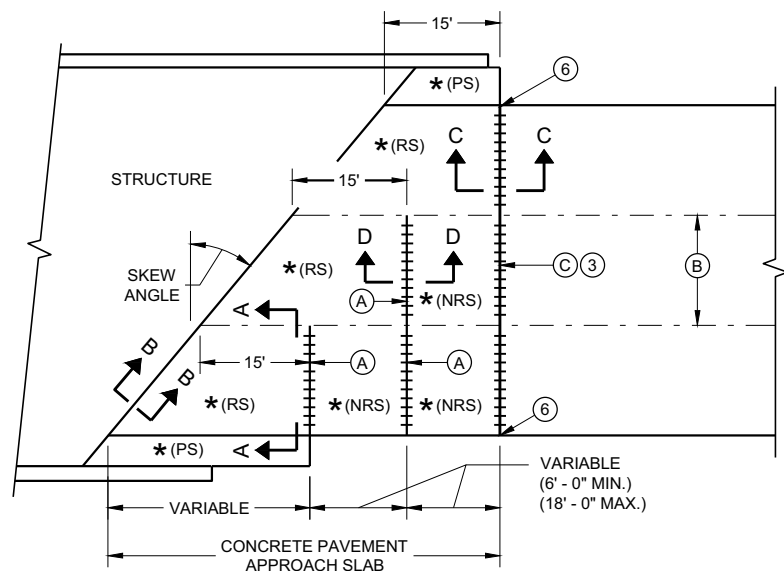
ALTERNATE LUG

- ① ADHERE ANCHOR LUG TO PRECAST RETAINING WALL WITH EPOXY RESIN.

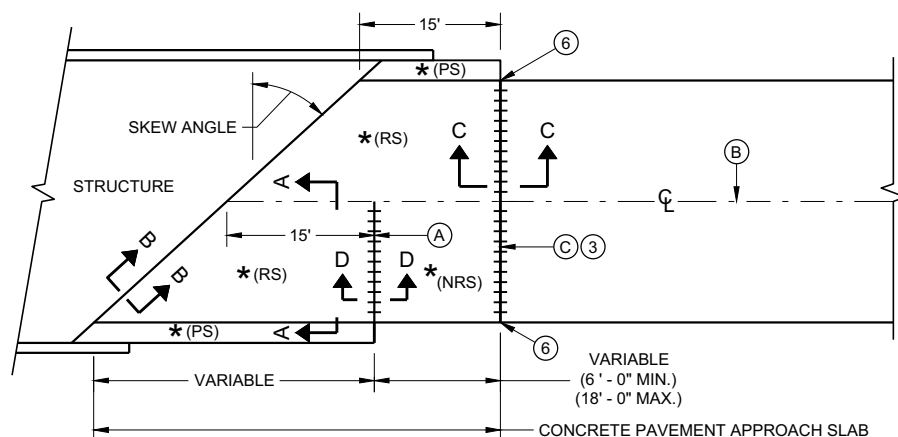


ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

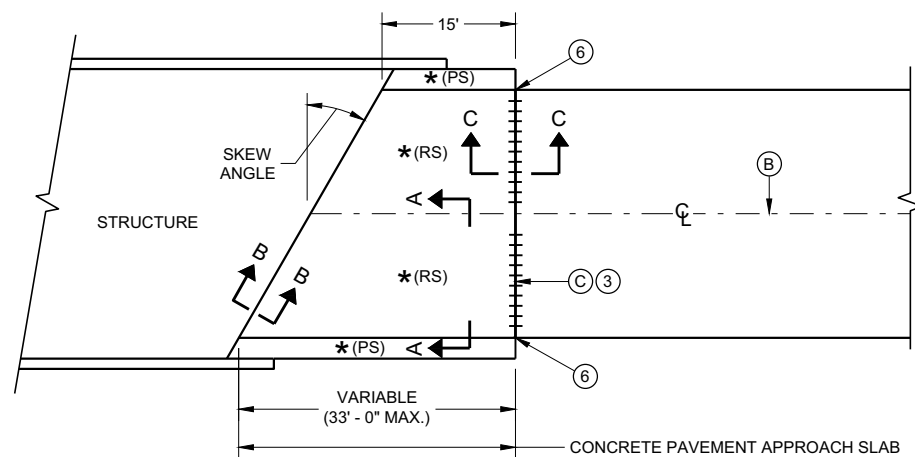
| | |
|--|--|
| NAME PLATE (STRUCTURES) | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED 3/26/10 DATE | /S/ Scot Becker CHIEF STRUCTURAL DEVELOPMENT ENGINEER |
| FHWA | |



**SKewed APPROACH
(PAVEMENT MORE THAN TWO LANES)**



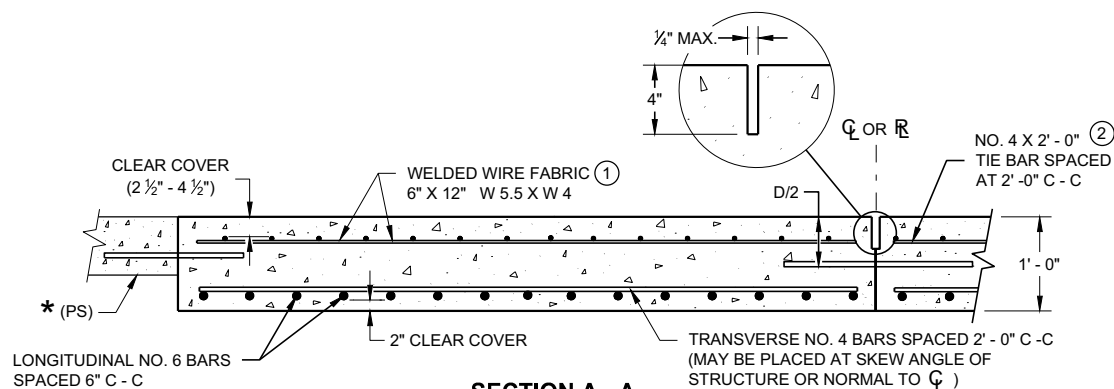
**SKews > 20°
(PAVEMENT WIDTH ≤ 30')**



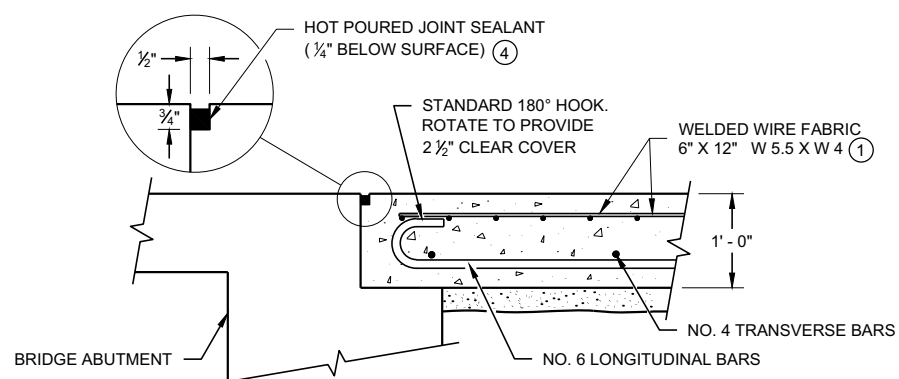
**SKews ≤ 20°
(PAVEMENT WIDTH ≤ 30')**

APPROACH SLAB AND ADJACENT PAVEMENT

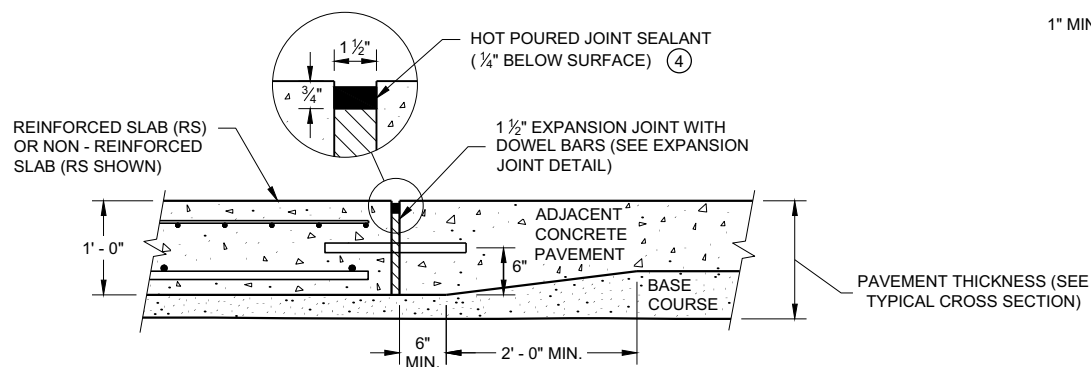
- * (RS) = REINFORCED CONCRETE SLAB
- * (PS) = PAVED CONCRETE SHOULDER OR CONCRETE DRAINAGE SLAB
- * (NRS) = NON - REINFORCED CONCRETE SLAB
- *** STANDARD DOWEL BAR DIAMETER (SEE SDD 13C11 AND SDD 13C13)



**SECTION A - A
REINFORCEMENT POSITIONING DETAIL**



**SECTION B - B
BEND DETAIL
BOTTOM REINFORCEMENT**



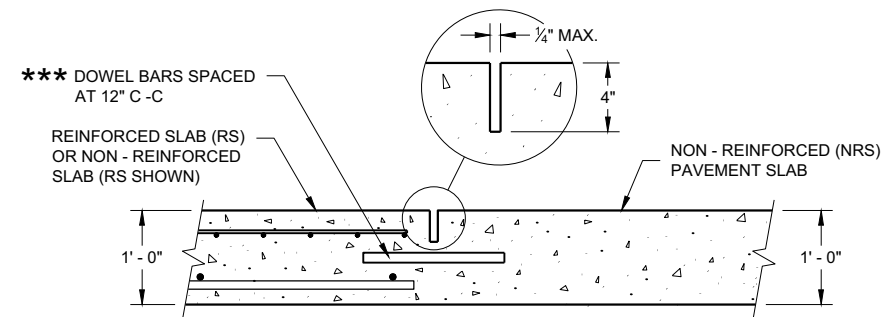
**SECTION C - C
TRANSITION DETAIL
APPROACH SLAB TO ADJACENT PAVEMENT**

GENERAL NOTES

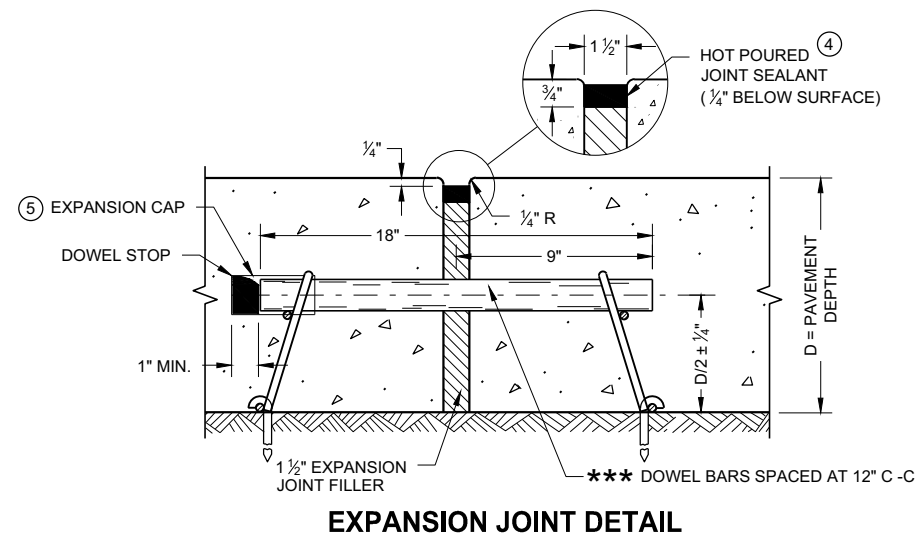
THE CONTRACTOR MAY SPLICE NO. 6 BARS IN THE APPROACH SLAB FOR SKEWED STRUCTURES ONLY. STAGGER SPLICES WITH A MAXIMUM OF ONE SPLICE PER BAR. THE LENGTH OF LAP IS 20 INCHES.

TACK WELD DOWEL BARS TO THE BASKETS ON ALTERNATE ENDS.

- ① THE CONTRACTOR MAY USE NO. 4 BARS SPACED AT 2' - 0" C - C IN BOTH THE LONGITUDINAL AND TRANSVERSE DIRECTIONS FOR TOP REINFORCEMENT AS AN ALTERNATIVE TO THE WELDED WIRE FABRIC.
- ② THE CONTRACTOR MAY OMIT THE BARS BETWEEN REINFORCED SLABS WHERE SLAB REINFORCEMENT BARS EXTEND ACROSS THE CENTERLINE OR REFERENCE LINE.
- ③ DO NOT CONSTRUCT AN EXPANSION JOINT OR INSTALL DOWEL BARS WHEN ABUTTING AN HMA PAVEMENT.
- ④ USE A JOINT SEALANT CONFORMING TO STANDARD SPECIFICATION 415.2.6.
- ⑤ PLACE EXPANSION CAP ON THE END OF THE DOWEL THAT IS NOT TACK WELDED TO THE BASKET. DO NOT FORCE DOWEL BAR PAST THE DOWEL STOP.
- ⑥ EXTEND EXPANSION JOINT THROUGH ANY ADJACENT TIED CONCRETE.
- (A) STANDARD CONTRACTION JOINT NORMAL TO \mathcal{C} OR \mathcal{R} .
- (B) STANDARD LONGITUDINAL JOINT WITH TIE BARS.
- (C) 1 1/2" EXPANSION JOINT WITH DOWEL BARS NORMAL TO \mathcal{C} OR \mathcal{R} .



**SECTION D - D
CONTRACTION JOINT**

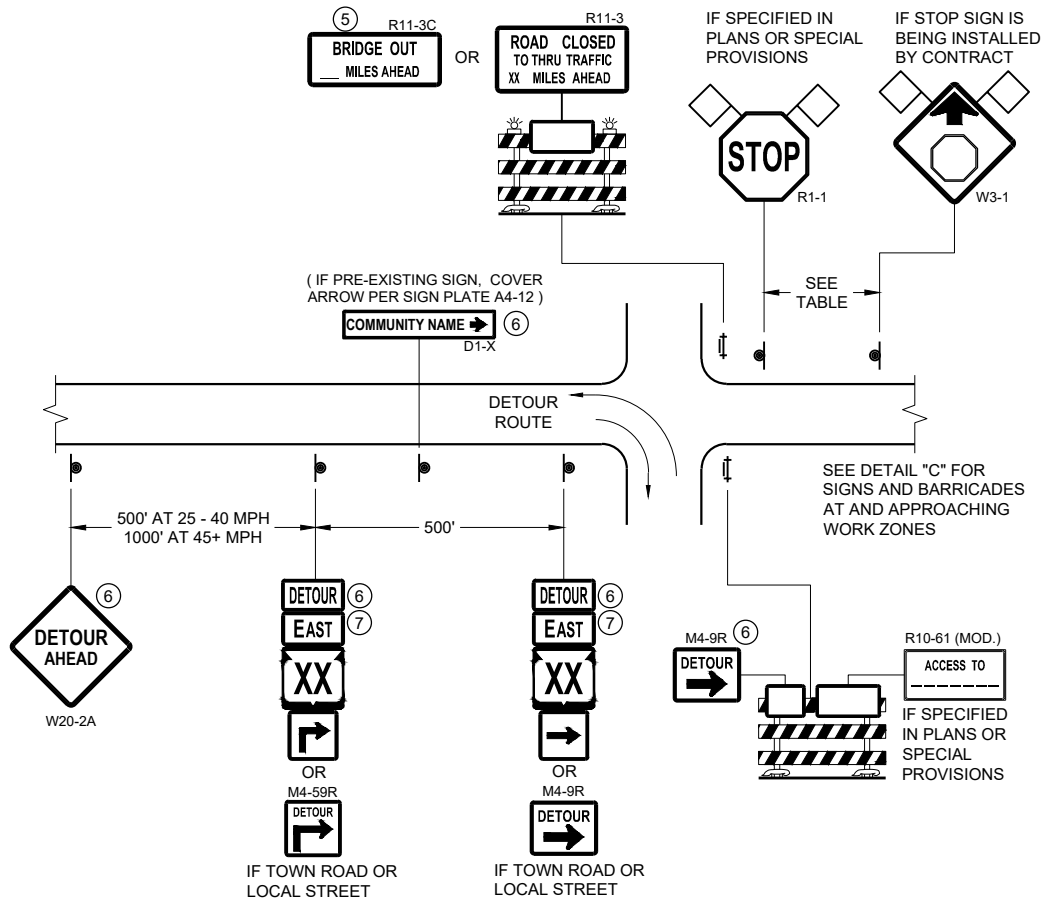


EXPANSION JOINT DETAIL

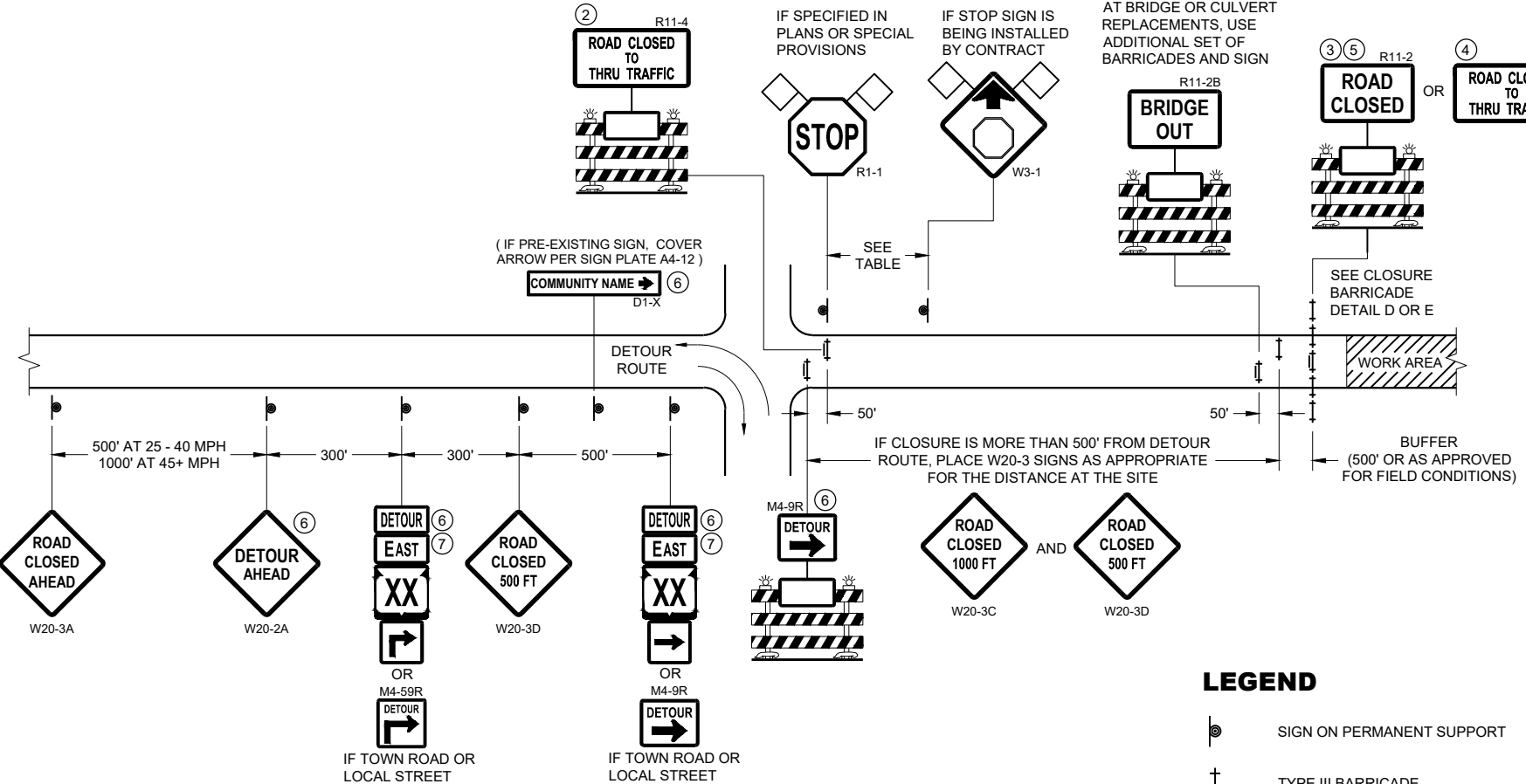
CONCRETE PAVEMENT APPROACH SLAB

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018 /S/ Peter Kemp, P.E.
DATE DATE PAVEMENT SUPERVISOR
FHWA



DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR
WORK ZONE GREATER THAN OR EQUAL TO ½ MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)



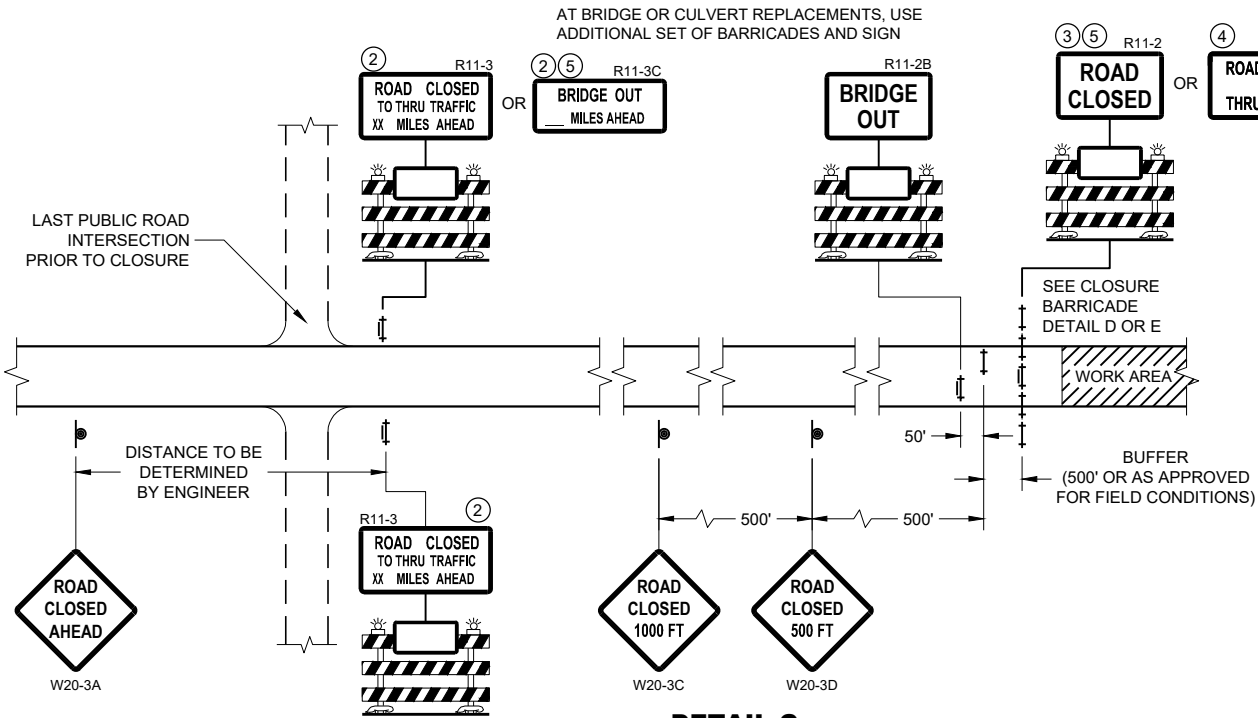
DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR
WORK ZONE LESS THAN ½ MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

- LEGEND**
- SIGN ON PERMANENT SUPPORT
 - TYPE III BARRICADE
 - TYPE III BARRICADE WITH ATTACHED SIGN
 - TYPE "A" WARNING LIGHT (FLASHING)
 - WORK AREA
 - FLAGS, 16" X 16" MIN. (ORANGE)

- DETOUR M4 - 8
- EAST M3 - X
- XX M1 - 4 OR XX M1 - 6 OR COUNTY M1 - 5A
- OR M05 - 1 OR M06 - 1

| SPEED LIMIT (MPH) | "STOP AHEAD" ADVANCE WARNING DISTANCE (FT) |
|-------------------|--|
| 25 | 200 |
| 30 | 200 |
| 35 | 350 |
| 40 | 350 |
| 45 | 500 |
| 50 | 550 |
| 55 | 750 |

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦



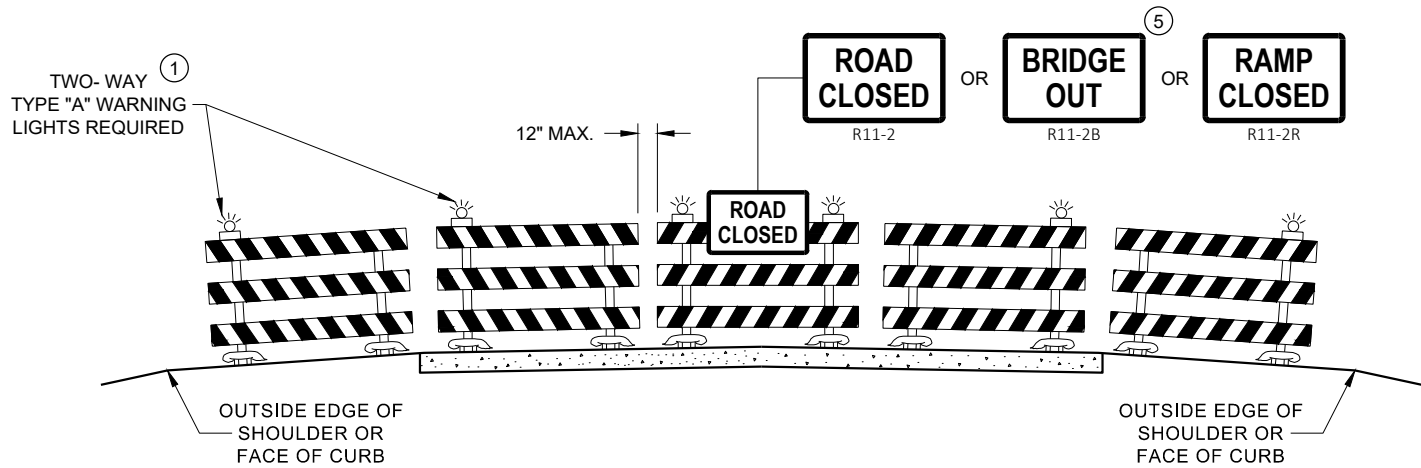
DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

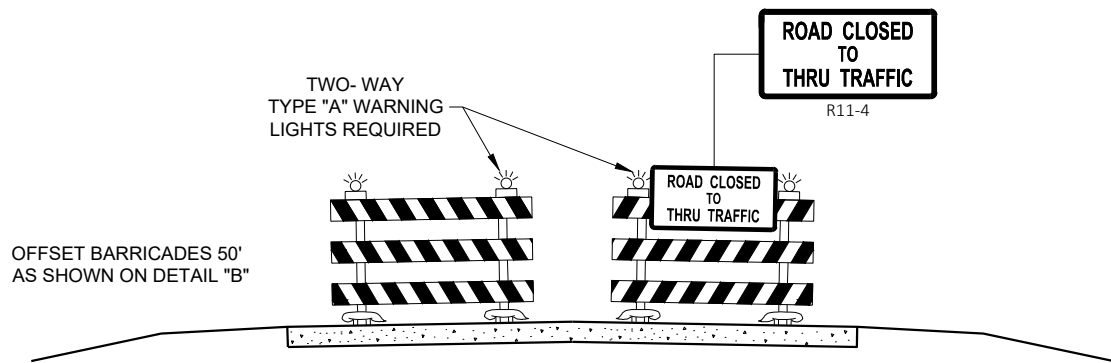
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60 " X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

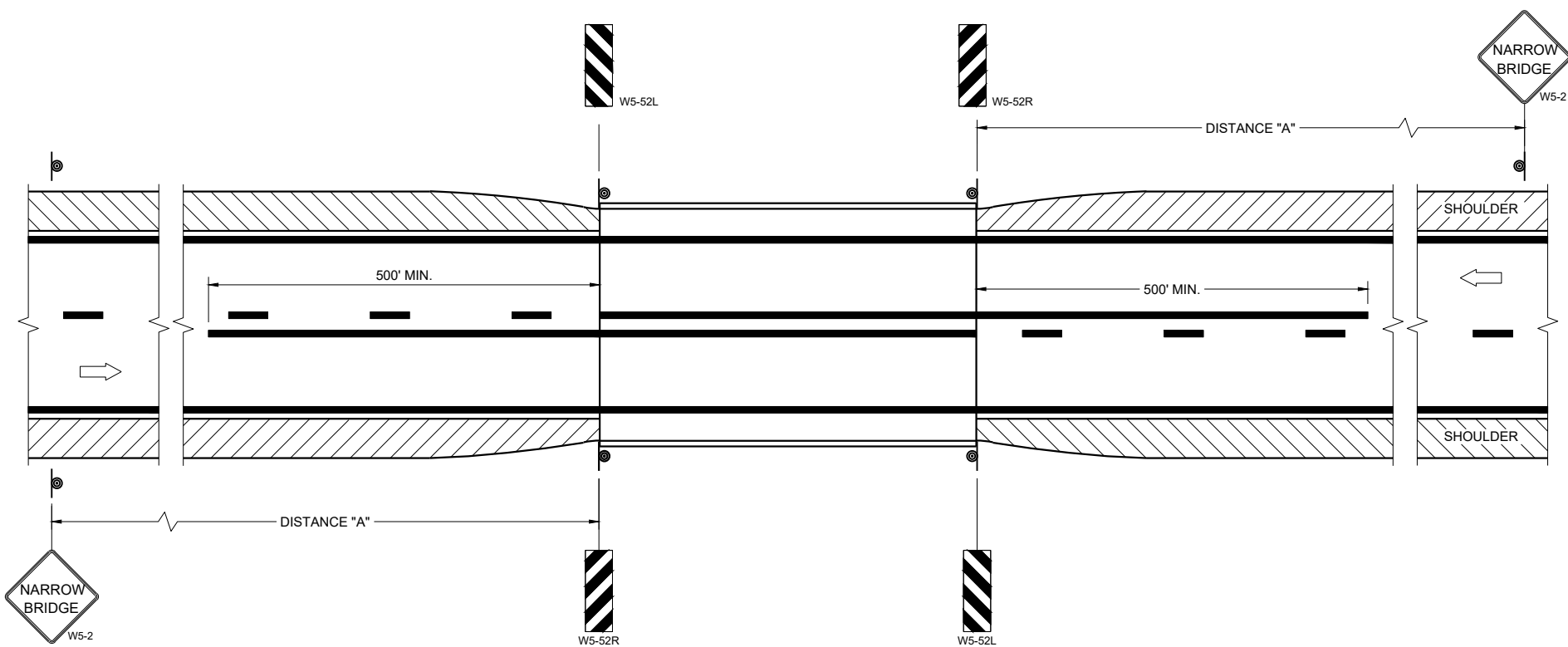
- 1 TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- 2 THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- 3 FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- 4 FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- 5 FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- 6 INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- 7 "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES

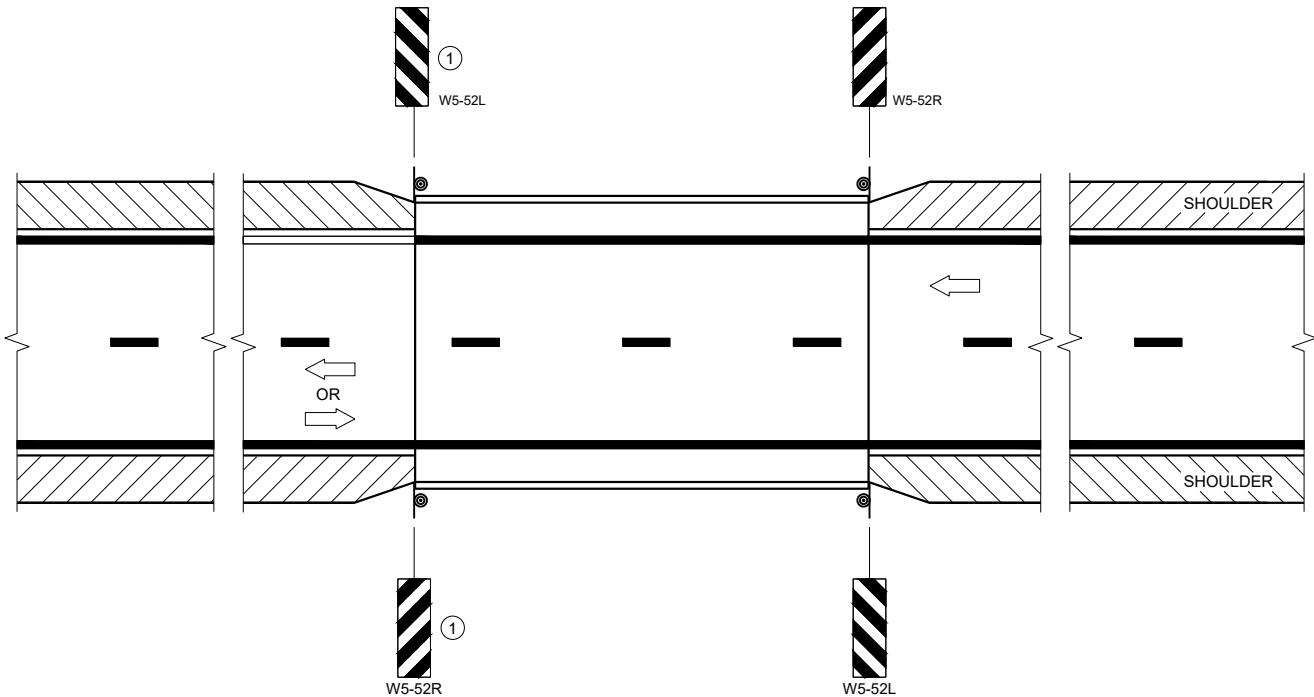
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA



SITUATION 1
WARRANTING CRITERIA:
BRIDGE WIDTH IS AT LEAST 16 FEET BUT LESS THAN 24 FEET.



SITUATION 2
WARRANTING CRITERIA:
1. BRIDGE WIDTH IS AT LEAST 24 FEET AND
2. BRIDGE SHOULDER WIDTH IS LESS THAN 6 FEET

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THE DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

LOCATE W5-52 SIGN POST(S) BEHIND GUARDRAIL WHEN PRESENT.

PLACE THE EDGE OF THE W5-52 SIGN IN LINE WITH FACE OF CURB OR PARAPET.

ON BRIDGE ONLY PROJECTS, PLACE 300 FEET OF EDGELINE.

OMIT EDGELINES ON ROADWAYS WITHOUT EXISTING EDGELINES.

① OMIT ON ONE-WAY TRAVELED WAYS.

LEGEND

⊙ SIGN ON PERMANENT SUPPORT

➡ DIRECTION OF TRAFFIC

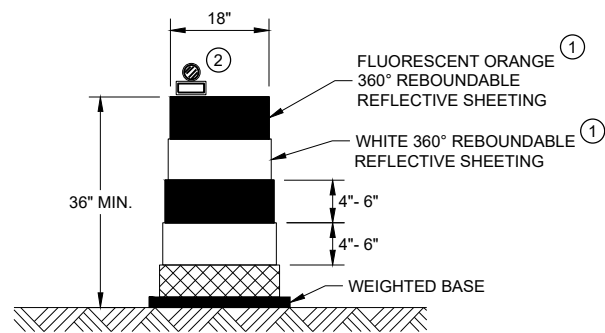
DISTANCE TABLE

| POSTED OR 85TH PERCENTILE SPEED | DISTANCE "A" |
|---------------------------------|--------------|
| 25 | 150' |
| 30 | 200' |
| 35 | 250' |
| 40 | 300' |
| 45 | 400' |
| 50 | 550' |
| 55 | 700' |

**SIGNING AND MARKING
FOR TWO LANE BRIDGES**

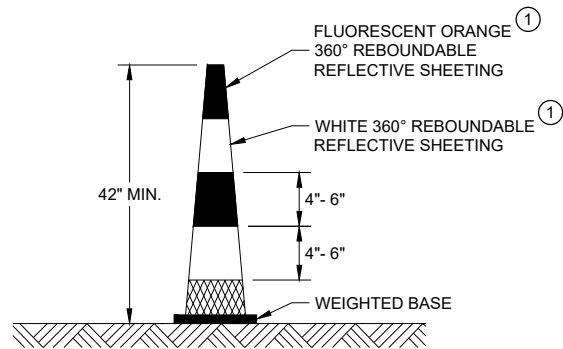
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Jeannie Silver
DATE Statewide Pavement Marking Engineer
FHWA



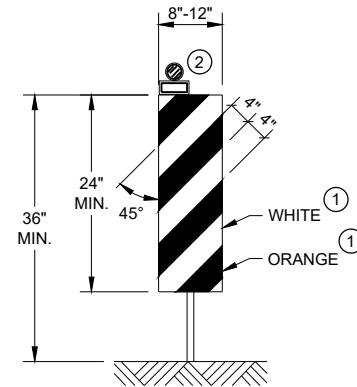
DRUM

BALLAST WIDTHS
RANGE FROM 24"-36"



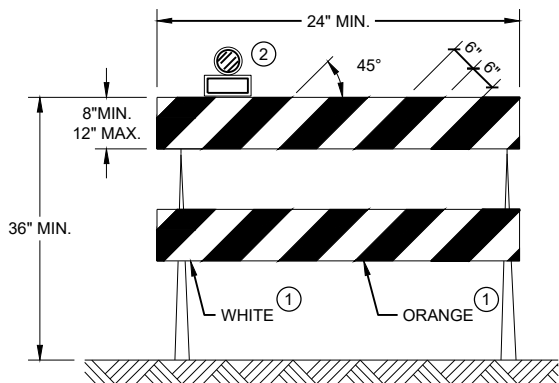
42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS
BALLAST WIDTHS
RANGE FROM 14"-20"



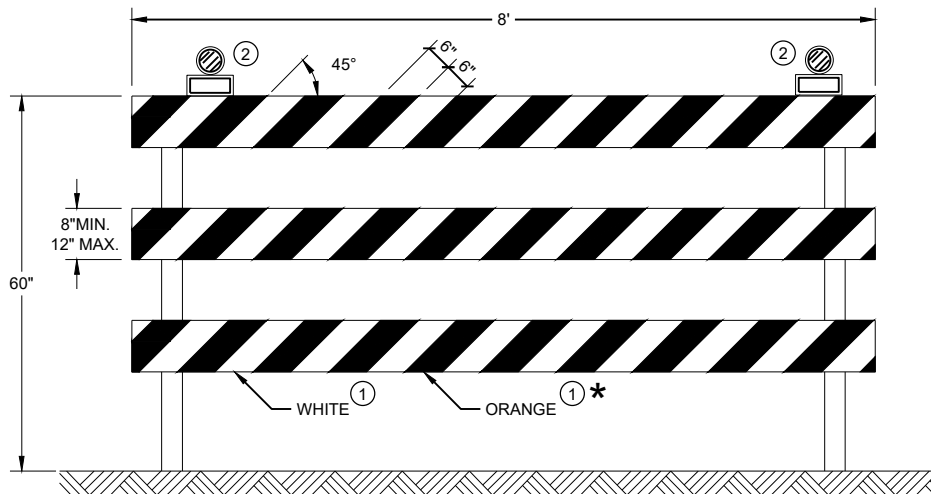
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.

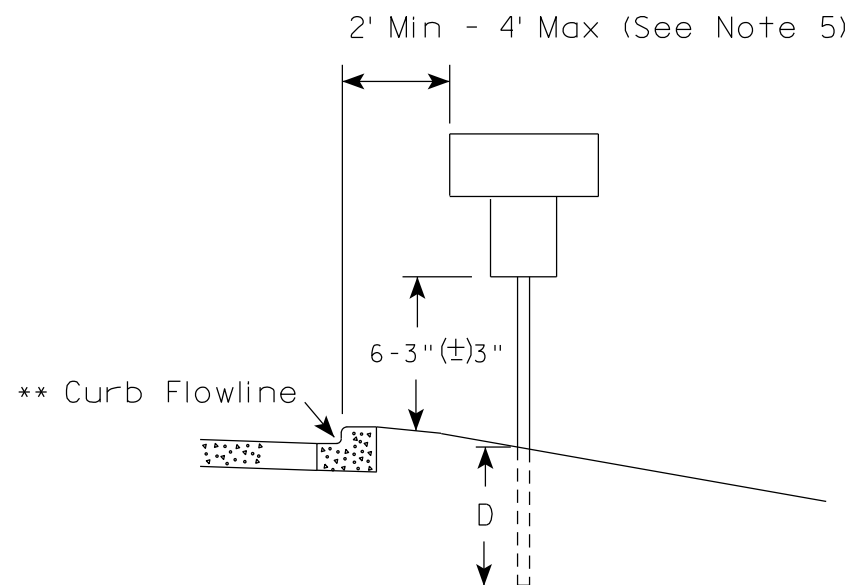
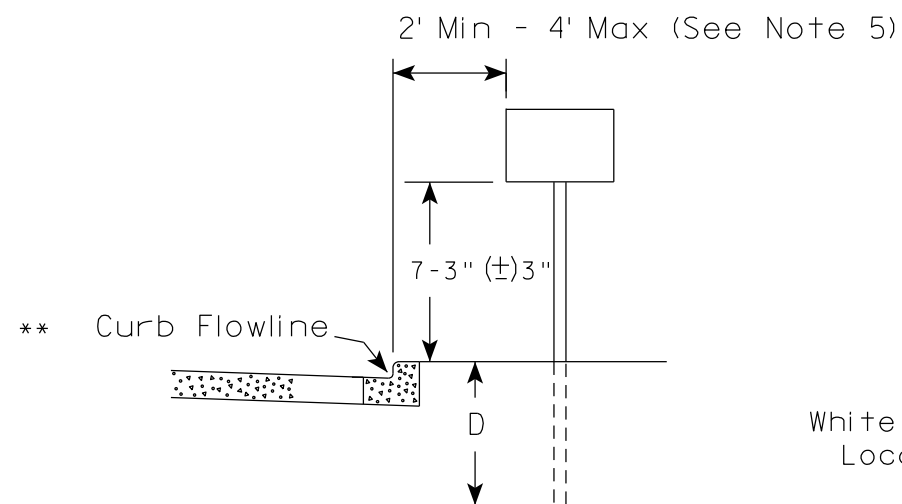
**CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

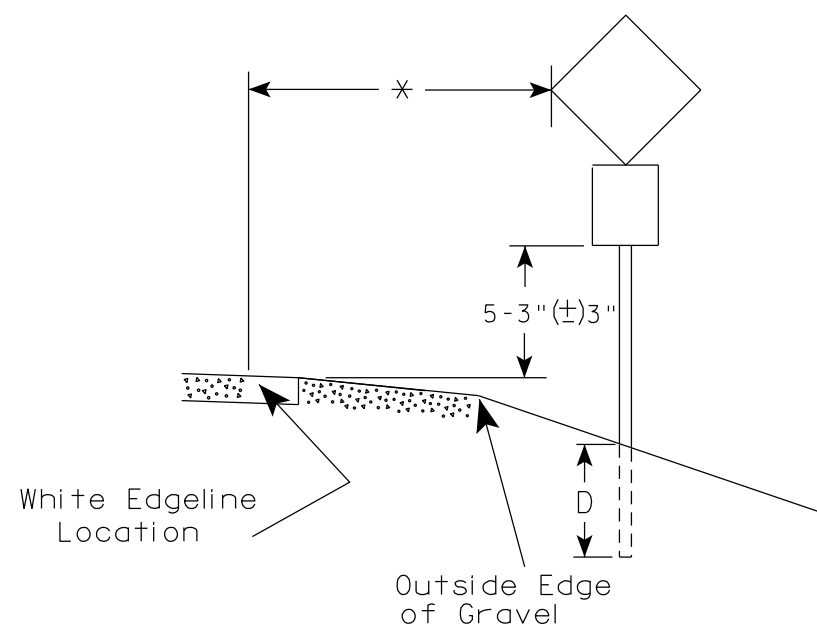
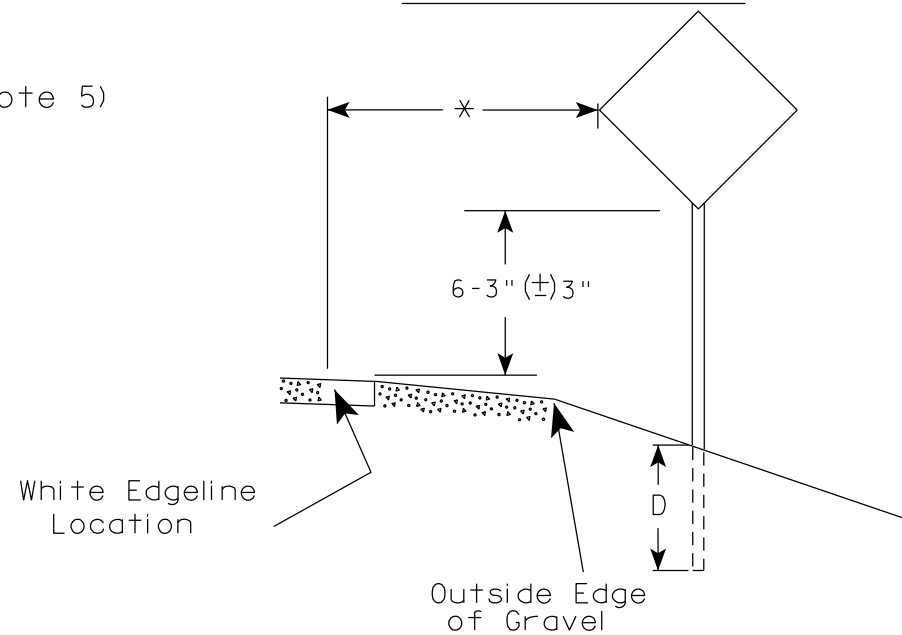
FHWA

URBAN AREA



* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

RURAL AREA (See Note 2)



* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

| Area of Sign Installation (Sq. Ft.) | D (Min) |
|--|--------------|
| 20 or Less | 4' |
| Greater than 20 | 5' |

GENERAL NOTES

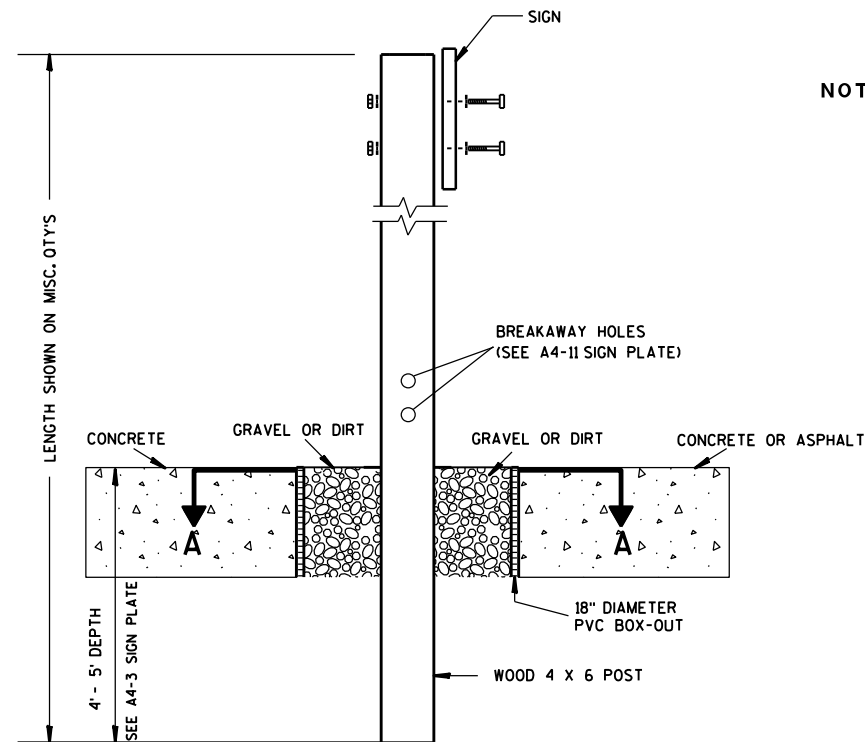
- Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
- If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".
- For expressways and freeways, mounting height is 7'- 3" (±) 3" or 6'-3" (±) 3" depending upon existence of a sub-sign.
- Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±) 3".
- Offset distance shall be consistent with existing signs or consistent throughout length of project.
- Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the Engineer.

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

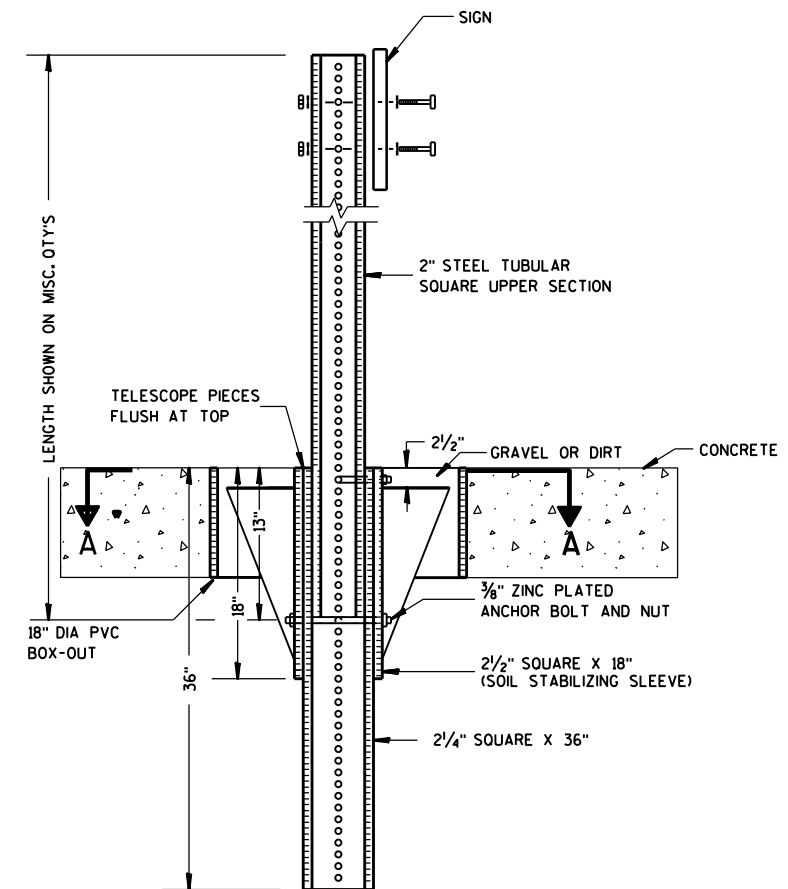
DATE 12/6/23 PLATE NO. A4-3.23



ELEVATION VIEW

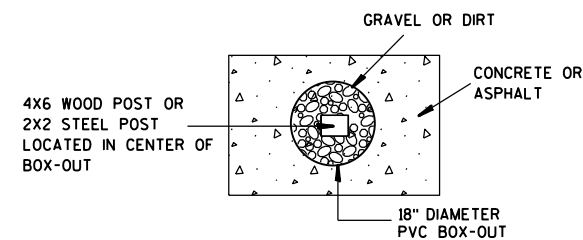
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES: 1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST
BOX-OUTS
A4-3B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

PROJECT NO:

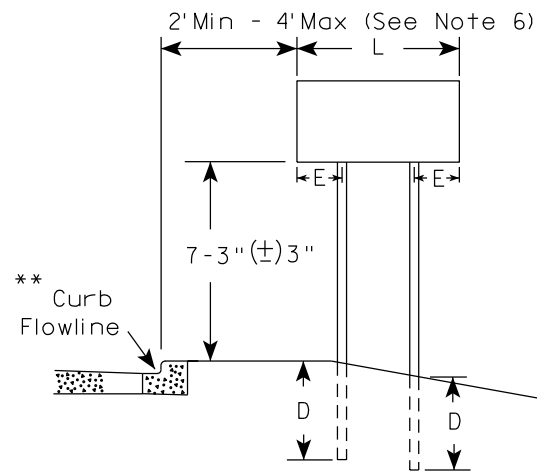
HWY:

COUNTY:

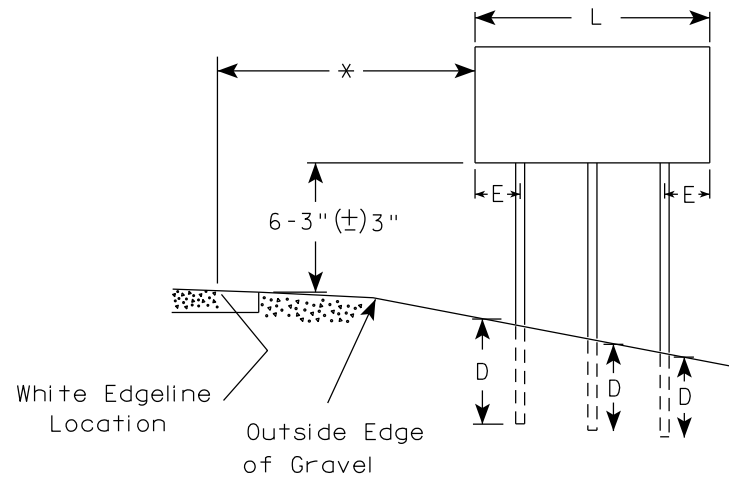
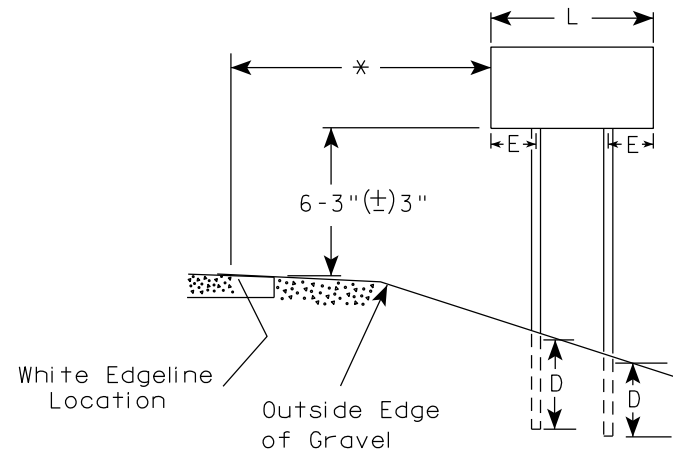
SHEET NO:

E

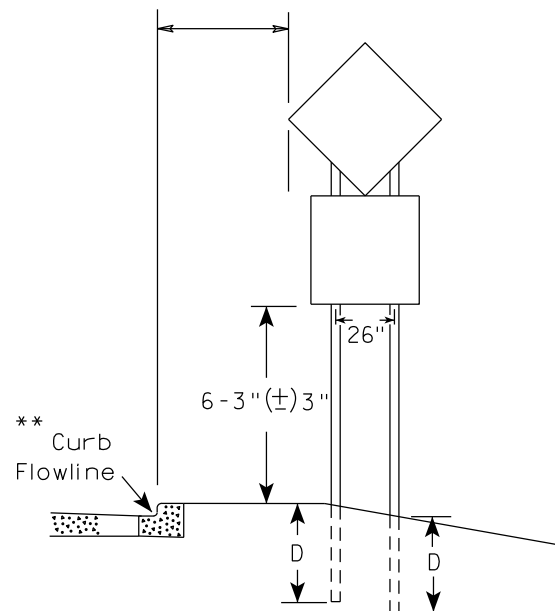
URBAN AREA



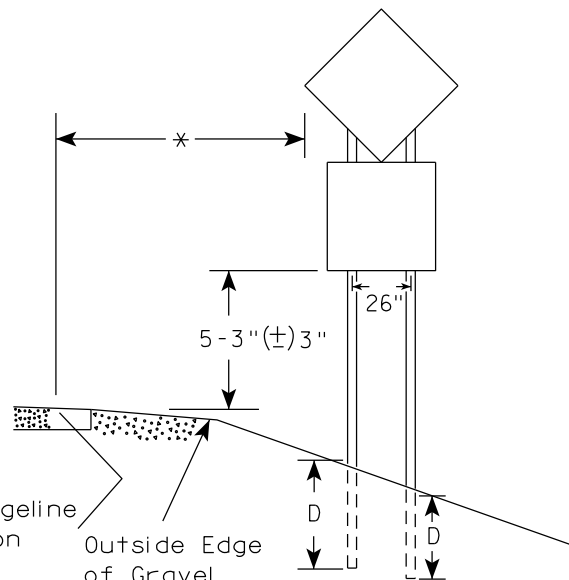
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) 3" or 6'-3" (±) 3" depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

| SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED) | |
|---|-----|
| L | E |
| Greater than 48" Less than 60" | 12" |
| 60" to 108" | L/5 |

| SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED) | |
|---|-----|
| L | E |
| Greater than 108" to 144" | 12" |

POST EMBEDMENT DEPTH

| Area of Sign Installation (Sq.Ft.) | D (Min) |
|--|--------------|
| 20 or Less | 4' |
| Greater than 20 | 5' |

TYPICAL INSTALLATION
OF TYPE II SIGNS
ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-4.16

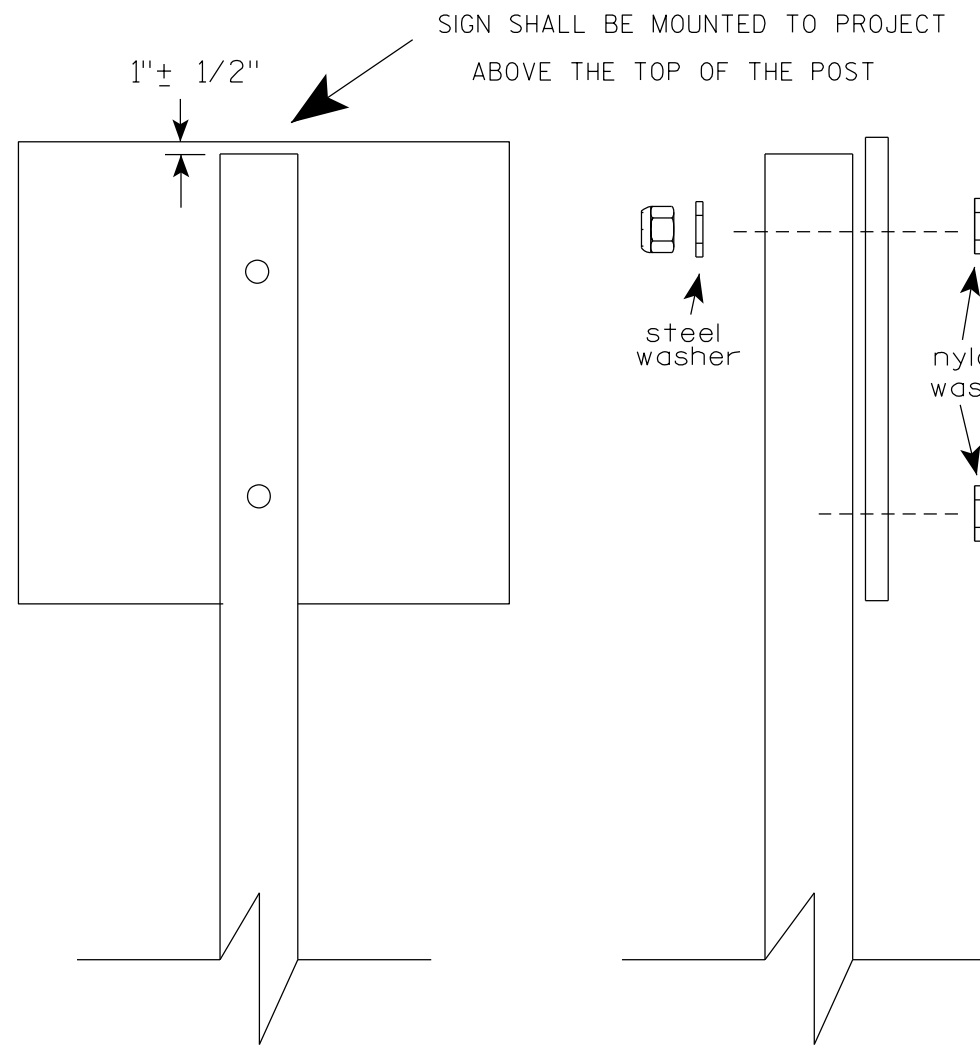
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

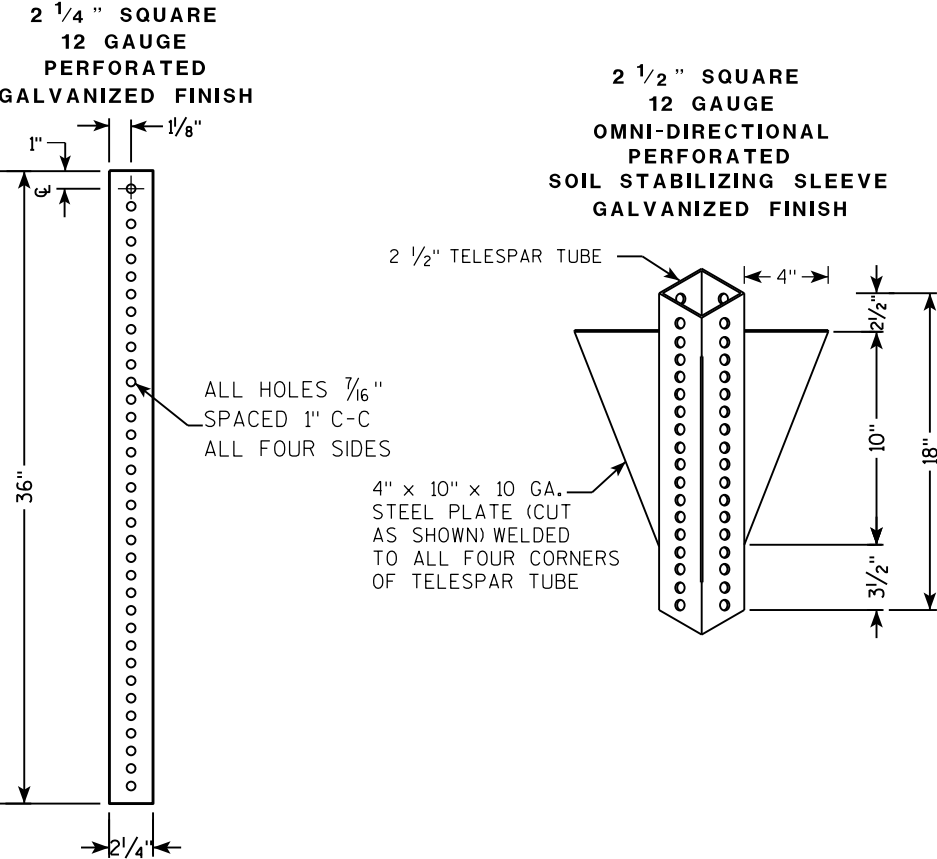
Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS - $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)
- $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
- $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
- O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON

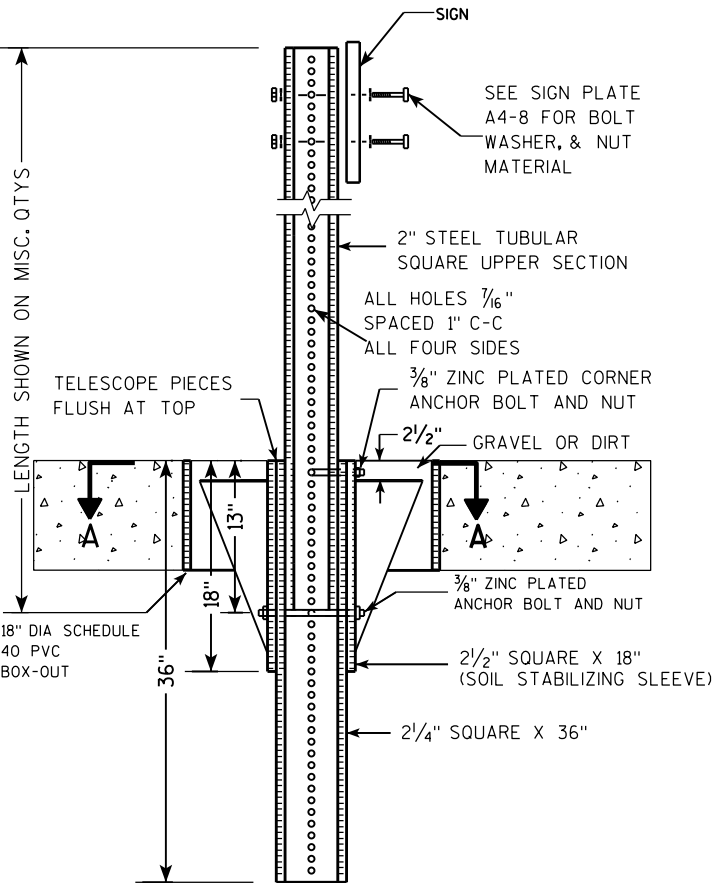
* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

| | |
|----------------------------------|---|
| ATTACHMENT OF SIGNS TO POSTS | |
| WISCONSIN DEPT OF TRANSPORTATION | |
| APPROVED | <i>Matthew R. Rauch</i> For State Traffic Engineer |
| DATE 4/1/2020 | PLATE NO. A4-8.9 |

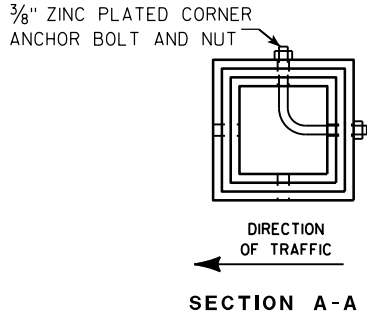
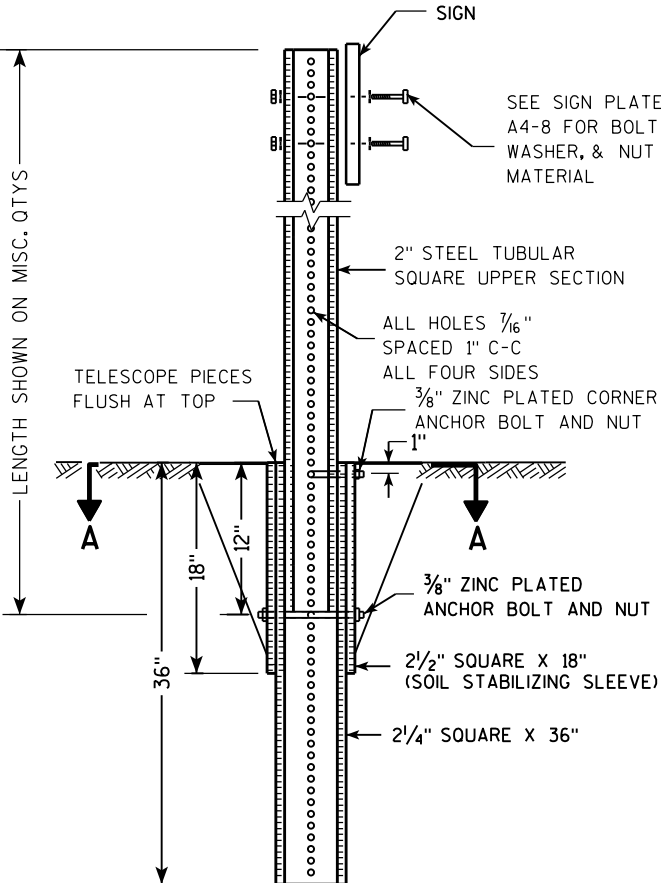
TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM



DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)



| Area of Sign Installation (Sq. Ft.) | Number of Required Posts |
|--|--------------------------|
| 9 or less | 1 |
| Greater than 9 less than or equal to 18 | 2 |
| Greater than 18 less than or equal to 27 | 3 |

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

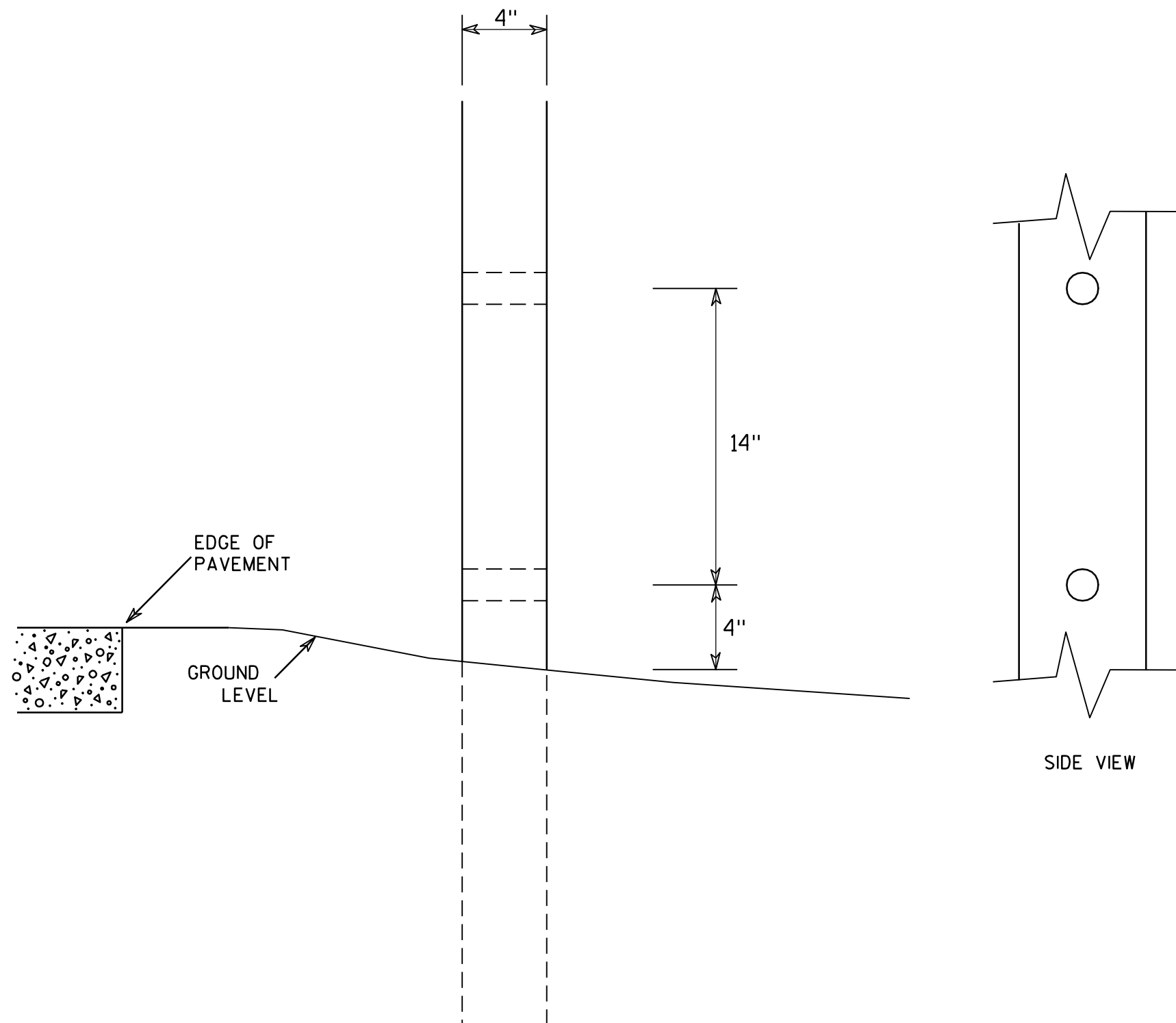
TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

7



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Chester J. Spang
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

PROJECT NO:

HWY:

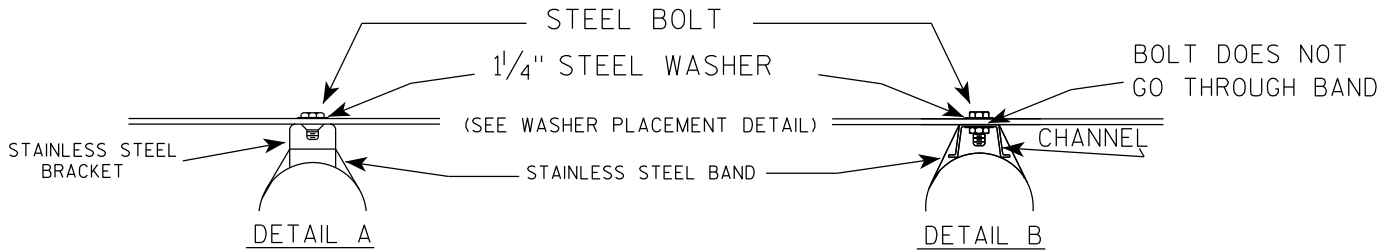
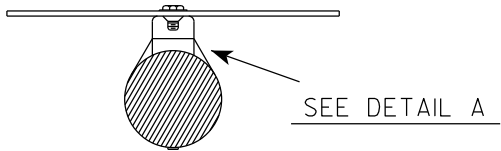
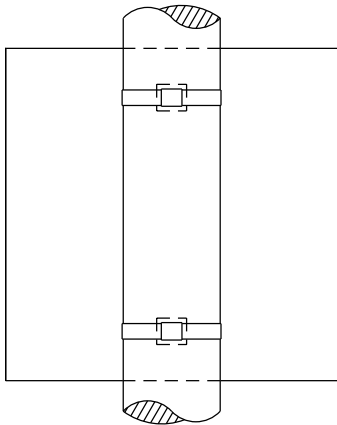
COUNTY:

SHEET NO:

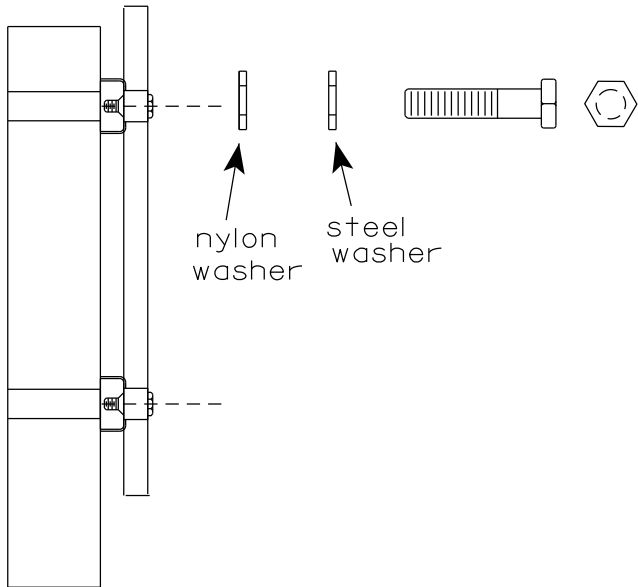
E

BANDING

SINGLE SIGN



WASHER PLACEMENT

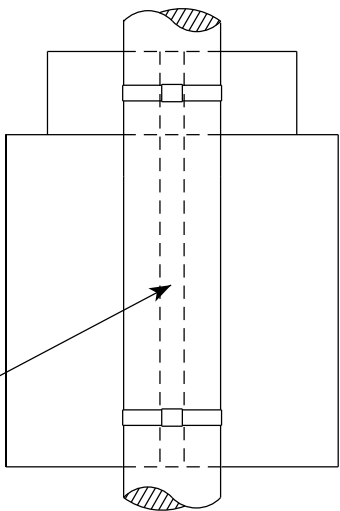


WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET

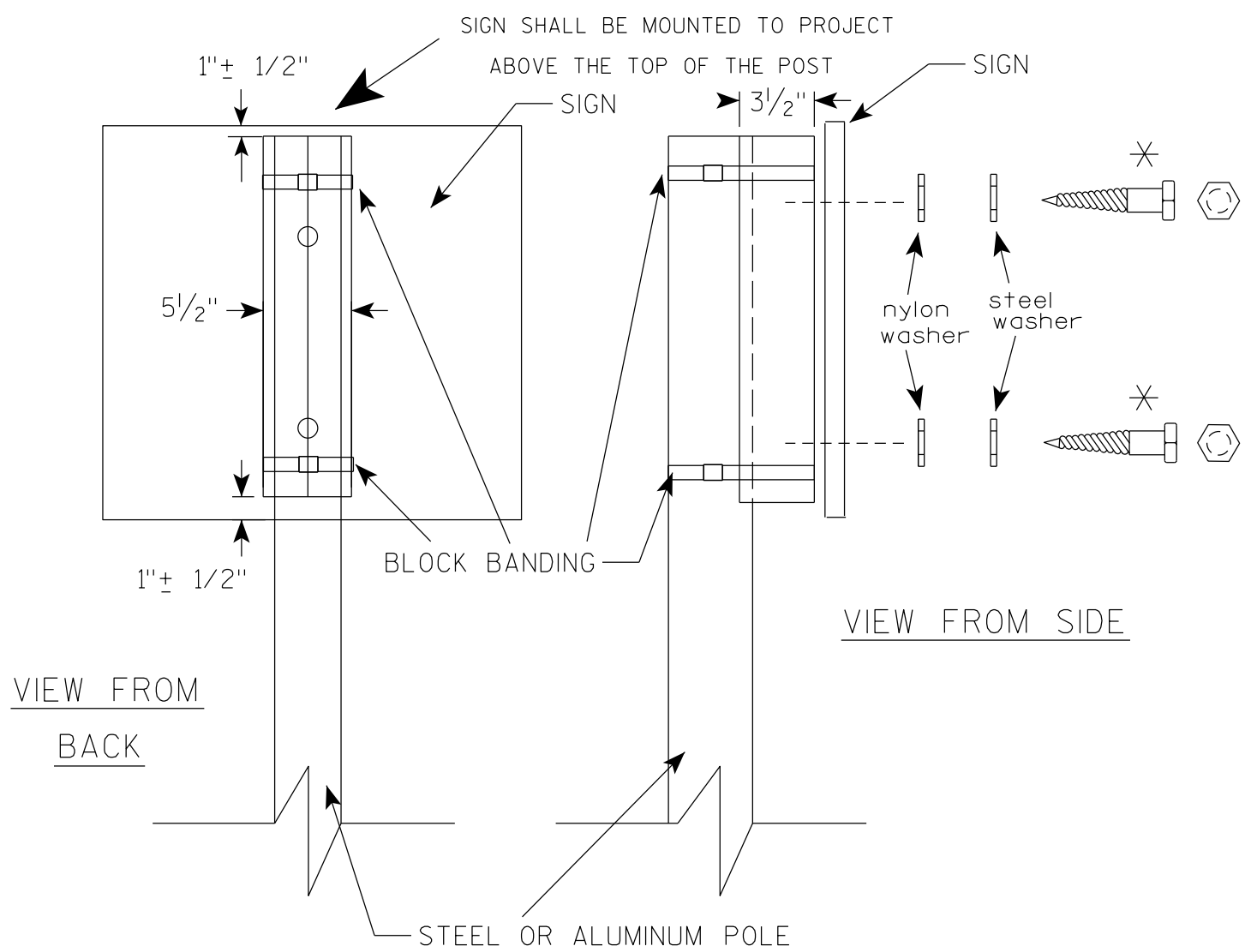
SEE DETAIL B

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

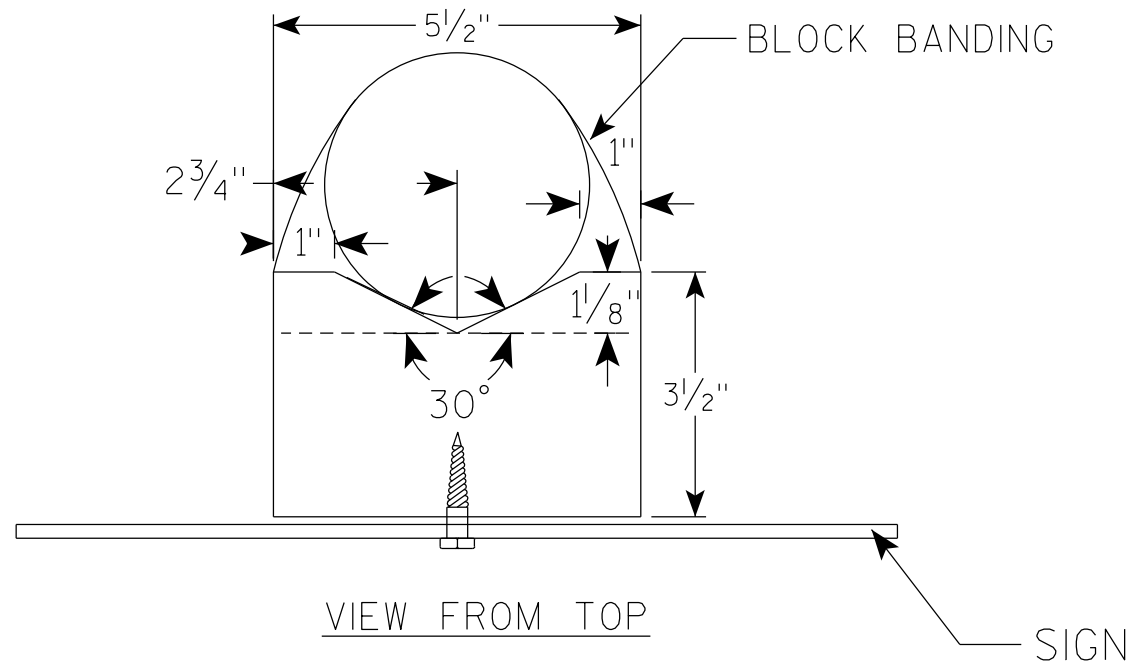
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/10/19 PLATE NO. A5-9.4



VIEW FROM
BACK

VIEW FROM SIDE



VIEW FROM TOP

GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, $\frac{3}{4}$ " WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE $\frac{1}{4}$ " O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ "
8. NYLON WASHERS SHALL BE $\frac{1}{4}$ " O.D. X $\frac{3}{8}$ " I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✱ LAG BOLTS SHALL BE $\frac{3}{8}$ " X $2\frac{1}{2}$ "

BLOCK BANDING DETAIL
(V-BLOCK OPTION)

WISCONSIN DEPT OF TRANSPORTATION

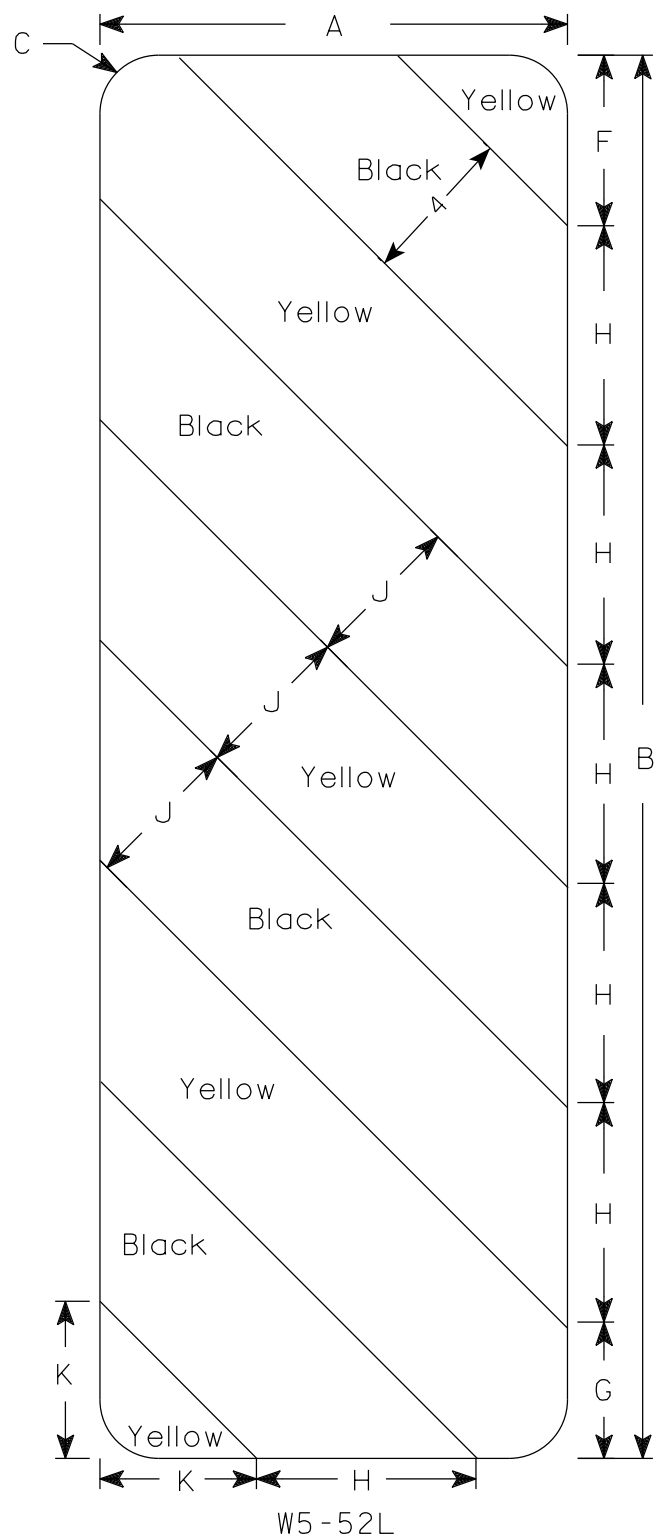
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 4/19/2022 PLATE NO. A5-10.3

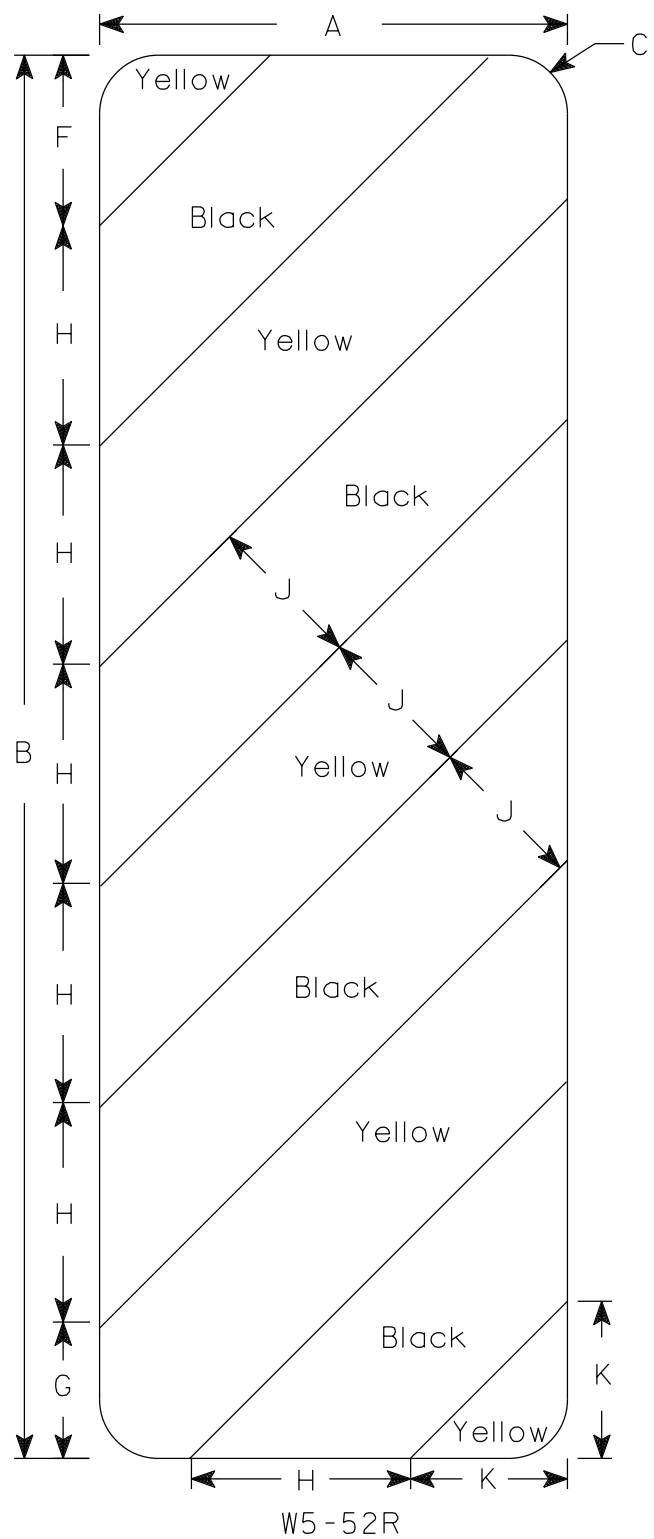
PROJECT NO:

SHEET NO:

E



W5-52L



W5-52R

NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:
 - Background - Yellow
 - Message - Black
- 3. Alternate colors of stripes as shown.

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | v | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|---|---|-------|-------|-------|-----|---|--------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2S | 12 | 36 | 1 1/2 | | | 4 3/8 | 3 1/2 | 5 5/8 | 45° | 4 | 4 | | | | | | | | | | | | | | | | 3.0 |
| 2M | 12 | 36 | 1 1/2 | | | 4 3/8 | 3 1/2 | 5 5/8 | 45° | 4 | 4 | | | | | | | | | | | | | | | | 3.0 |
| 3 | 18 | 54 | 1 1/2 | | | 6 | 5 1/2 | 8 1/2 | 45° | 6 | 6 9/16 | | | | | | | | | | | | | | | | 6.75 |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

STANDARD SIGN
W5-52L & W5-52R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/4/2024 PLATE NO. W5-52.10

① INDICATES WING NUMBER

* PROVIDE FOR THRIE BEAM GUARD RAIL ATTACHMENT. AT UNUSED ANCHOR ASSEMBLIES CAULK HOLES SHUT WITH "100% SILICONE CAULK."

RIPRAP CONTINUES IN NE, NW, & SW QUADRANTS, SEE ROADWAY PLAN, TYP.

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HL-93
INVENTORY RATING: RF = 1.07
OPERATING RATING: RF = 1.39
WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV): 250 (KIPS)

STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 POUNDS PER SQUARE FOOT.

MATERIAL PROPERTIES:

CONCRETE MASONRY: _____ $f'_c = 4,000$ PSI
SUPERSTRUCTURE _____ $f'_c = 3,500$ PSI
ALL OTHER _____

BAR STEEL REINFORCEMENT
GRADE 60 _____ $f_y = 60,000$ PSI

FOUNDATION DATA

ABUTMENTS TO BE SUPPORTED ON HP10x42 PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 180 TONS ** PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA.

ESTIMATED 55'-0" LONG. AT SOUTH ABUTMENT
ESTIMATED 50'-0" LONG. AT NORTH ABUTMENT

**THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS THE REQUIRED DRIVING RESISTANCE MULTIPLIED BY A RESISTANCE FACTOR OF 0.5 USING MODIFIED GATES TO DETERMINE PILE CAPACITY.

HYDRAULIC DATA

100-YEAR FREQUENCY:

$Q_{100} = 2,470$ C.F.S.
 $V_{100} = 8.6$ F.P.S.
 $HW_{100} = \text{EL. } 687.88$
WATERWAY AREA = 197 SQ. FT.
DRAINAGE AREA = 9.01 SQ. MI.
 Q_{100} BRIDGE = 1,705 C.F.S.
 Q_{100} ROADWAY = 765 C.F.S.
SCOUR CRITICAL CODE = 5

2-YEAR FREQUENCY:

$Q_2 = 320$ C.F.S.
 $V_2 = 4.2$ F.P.S.
 $HW_2 = \text{EL. } 680.82$

ROAD OVERTOPPING FREQUENCY:

FREQUENCY = 50 YEARS
 $Q_{50} = 1590$ C.F.S.
 $HW_{50} = \text{EL. } 686.29$

STATE PROJECT NUMBER

7893-00-70

TRAFFIC DATA

FEATURE ON: 1005TH STREET

ADT = 98 (2045)
R.D.S. = 55 MPH



STRUCTURE DESIGN CONTACTS:

TIM BOROWSKI 608-298-5410
AARON BONK 608-261-0261

LIST OF DRAWINGS:

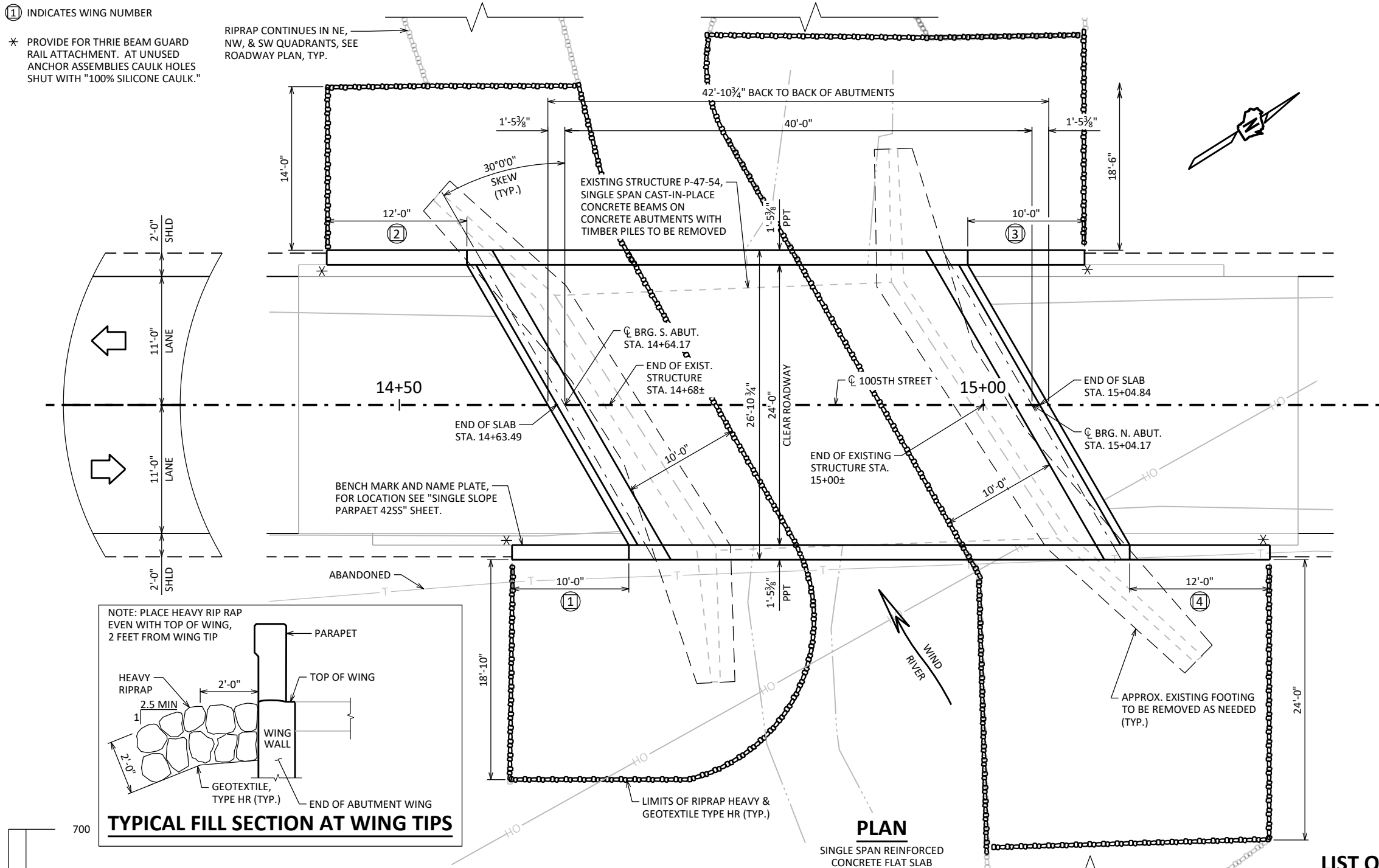
- GENERAL PLANS
- CROSS SECTION & QUANTITIES
- SUBSURFACE EXPLORATION
- SOUTH ABUTMENT
- SOUTH ABUTMENT DETAILS
- NORTH ABUTMENT
- NORTH ABUTMENT DETAILS
- SUPERSTRUCTURE
- SUPERSTRUCTURE DETAILS
- SINGLE SLOPE PARAPET 42SS



| | | | |
|---|---|----------|--------------------------------|
| NO. | DATE | REVISION | BY |
|  | | | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| ACCEPTED |  CHIEF STRUCTURES DESIGN ENGINEER | | 02/11/25 DATE |
| STRUCTURE B-47-233 | | | |
| 1005TH STREET OVER WIND RIVER | | | |
| COUNTY | PIERCE | TOWN | DIAMOND BLUFF |
| DESIGN SPEC. AASHTO LRFD BRIDGE DESIGN SPECIFICATION | | | |
| DESIGNED BY | DESIGNED TAB | CK'D SMN | DRAWN BY VCH PLANS CK'D SMN |
| GENERAL PLAN | | | SHEET 1 OF 10 |

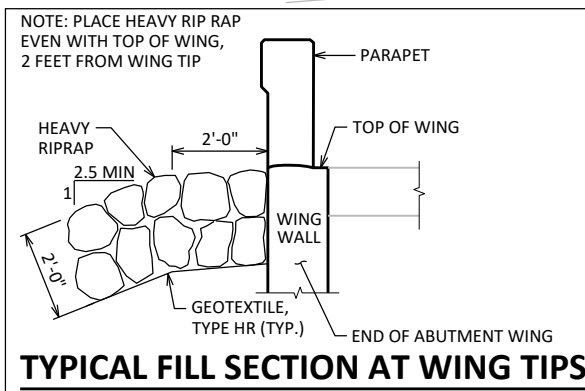
I.D.

DATE: NOV 2024



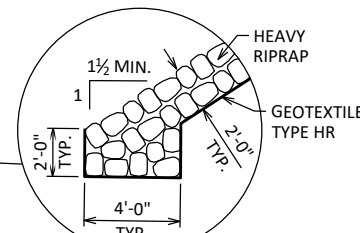
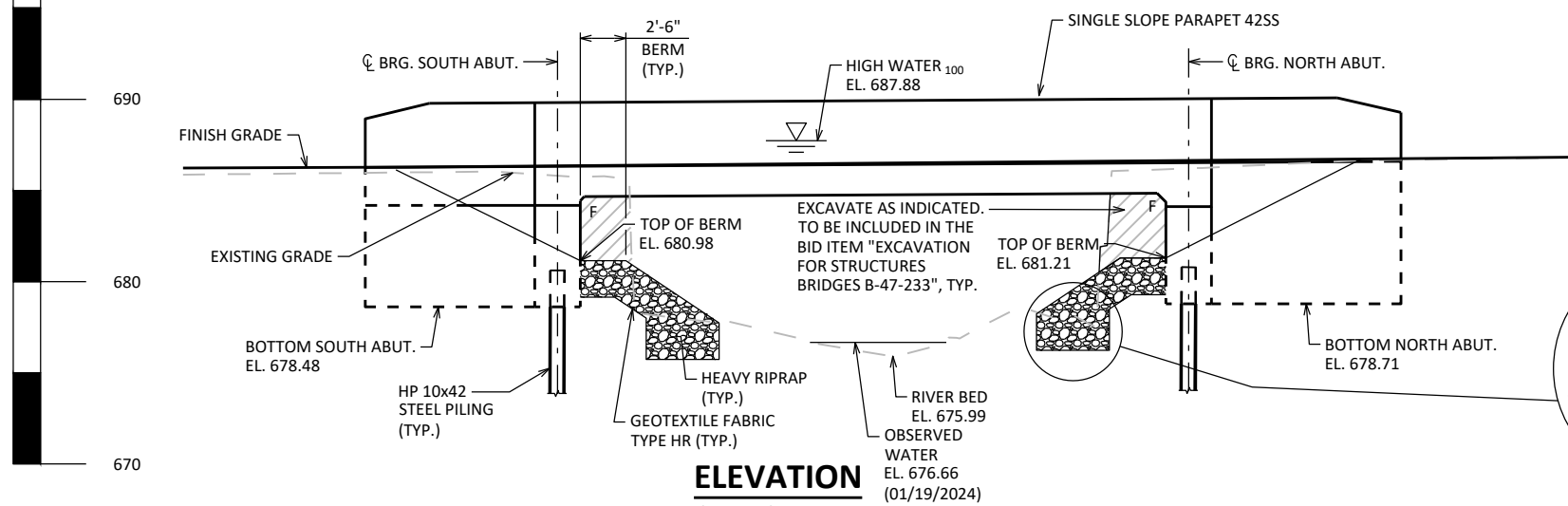
PLAN

SINGLE SPAN REINFORCED
CONCRETE FLAT SLAB



ELEVATION

NORMAL TO WATERWAY



GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BEVEL EXPOSED EDGES OF CONCRETE ¾" UNLESS OTHERWISE NOTED.

THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES BRIDGE B-47-233" SHALL BE THE EXISTING GROUNDLINE.

AT THE BACK FACE OF ABUTMENT ALL VOLUME WHICH CANNOT BE PLACED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL TYPE A.

EXCAVATION BELOW THE ABUTMENT AND ABUTMENT BEDDING MATERIALS REQUIRES ENGINEER APPROVAL. GEOTEXTILE SHALL BE SET AT THE BOTTOM OF EXCAVATION AND EXTEND 2'-0" ABOVE BOTTOM OF ABUTMENT.

THE QUANTITY FOR BACKFILL STRUCTURE IS CALCULATED BASED ON THE DETAIL SHOWN IN THE PLANS.

PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO THE ENTIRE EXPOSED TOP OF DECK AND THE VERTICAL AND HORIZONTAL SURFACES OF THE PAVING NOTCHES.

PIGMENTED SURFACE SEALER TO BE APPLIED TO THE FRONT FACE AND THE TOP OF THE PARAPETS.

THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH HEAVY RIPRAP AND GEOTEXTILE TYPE "HR" TO THE EXTENT SHOWN ON SHEET 1 AND THE ABUTMENT DETAILS. SEE ROADWAY PLANS FOR ADDITIONAL RIPRAP EXTENTS.

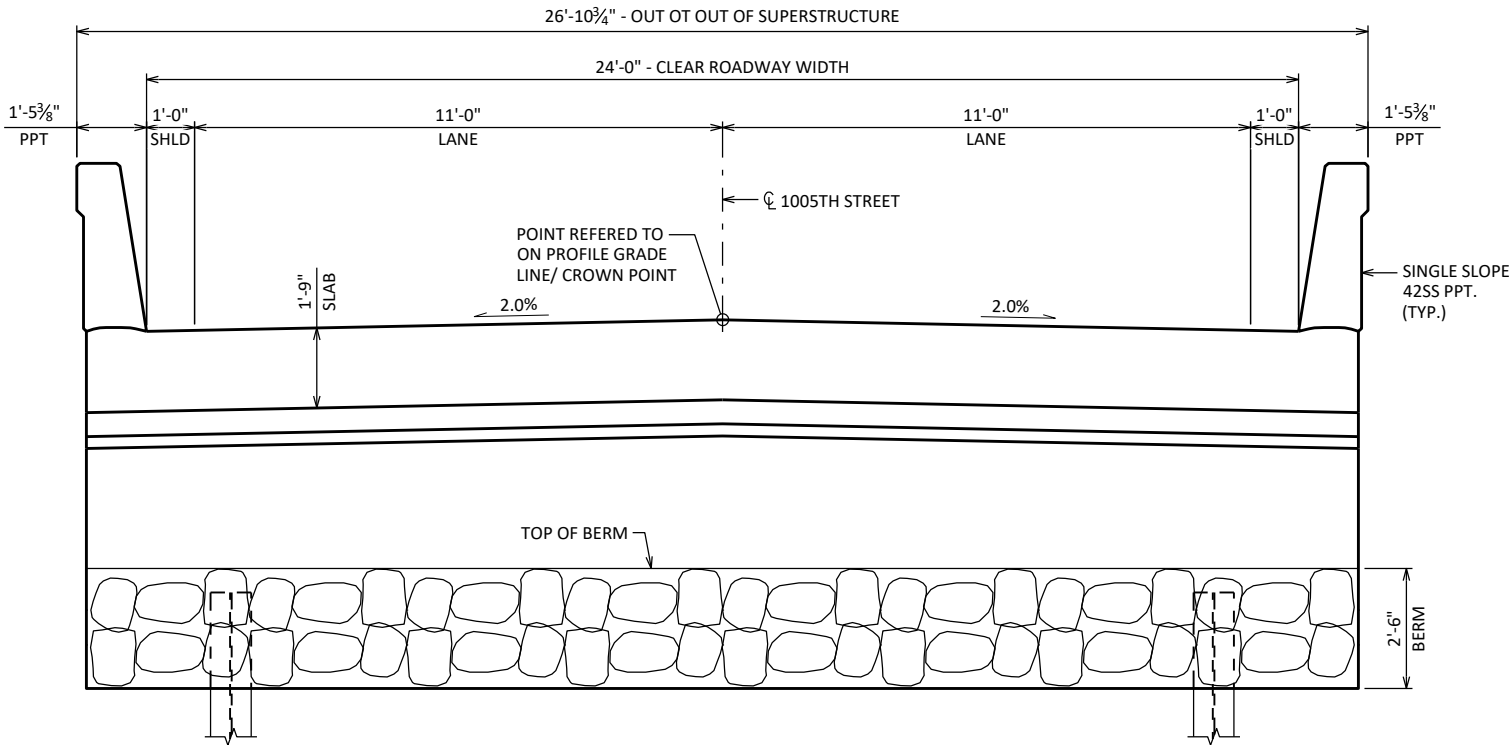
AT ABUTMENTS, HP 12X53 STEEL PILING MAY BE USED IN LIEU OF HP 10X42 STEEL PILING. PAYMENT SHALL BE BASED ON BID PRICE FOR HP 10X42 STEEL PILING.

SLAB FALSEWORK SHALL BE SUPPORTED ON PILES OR THE SUBSTRUCTURE, UNLESS AN ALTERNATE METHOD IS APPROVED BY THE ENGINEER.

AT ABUTMENTS, CONCRETE POURED UNDER WATER WILL BE ALLOWED AND SHALL BE DONE IN ACCORDANCE WITH SECTION 502.3.5.3 OF THE STANDARD SPECIFICATIONS.

P-47-54 FOOTING LOCATIONS ARE SHOWN AT THE BEST APPROXIMATE LOCATIONS. PORTIONS OF FOOTING WILL NEED TO BE REMOVED TO DRIVE NEW PILE.

ANCHOR ASSEMBLIES FOR STEEL PLATE BEAM GUARD ARE TO BE INSTALLED ON ALL WINGS BUT ARE NOT USED AT THIS TIME. CAULK HOLES SHUT WITH "100% SILICONE CAULK".



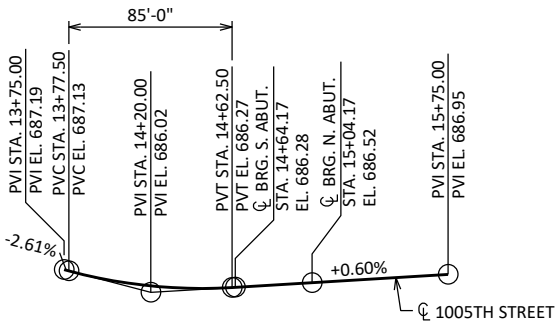
CROSS SECTION THRU ROADWAY

LOOKING NORTH

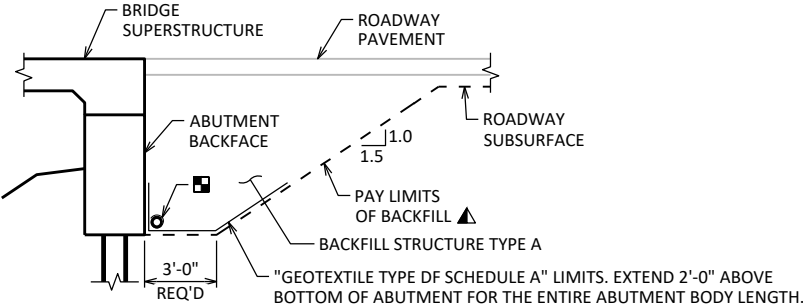
TOTAL ESTIMATED QUANTITIES

| BID ITEM NUMBER | BID ITEMS | UNIT | SUPER | SOUTH ABUTMENT | NORTH ABUTMENT | TOTAL |
|-----------------|--|------|-------|----------------|----------------|------------|
| 203.0250 | REMOVING STRUCTURE OVER WATERWAY REMOVE DEBRIS P-47-54 | EACH | -- | -- | -- | 1 |
| 206.1001 | EXCAVATION FOR STRUCTURES BRIDGES B-47-233 | EACH | -- | -- | -- | 1 |
| 210.1500 | BACKFILL STRUCTRURE TYPE A | TON | -- | 120 | 120 | 240 |
| 502.0100 | CONCRETE MASONRY BRIDGES | CY | 88.7 | 33.9 | 33.9 | 157 |
| 502.3200 | PROTECTIVE SURFACE TREATMENT | SY | 121 | -- | -- | 121 |
| 502.3210 | PIGMENTED SURFACE SEALER | SY | 42 | 11 | 11 | 64 |
| 505.0400 | BAR STEEL REINFORCEMENT HS STRUCTURES | LB | -- | 1820 | 1820 | 3640 |
| 505.0600 | BAR STEEL REINFORCEMENT HS COATED STRUCTURES | LB | 20300 | 2190 | 2190 | 24680 |
| 516.0500 | RUBBERIZED MEMBRANE WATERPROOFING | SY | -- | 9 | 9 | 18 |
| 550.1100 | PILING STEEL HP 10-INCH X 42 LB | LF | -- | 275 | 250 | 525 |
| 606.0300 | RIPRAP HEAVY | CY | -- | 110 | 140 | 250 |
| 612.0406 | PIPE UNDERDRAIN WRAPPED 6-INCH | LF | -- | 70 | 70 | 140 |
| 614.0150 | ANCHOR ASSEMBLIES FOR STEEL PLATE BEAM GUARD | EACH | -- | 2 | 2 | 4 |
| 645.0111 | GEOTEXTILE TYPE DF SCHEDULE A | SY | -- | 30 | 30 | 60 |
| 645.0120 | GEOTEXTILE TYPE HR | SY | -- | 165 | 210 | 375 |
| | | | | | | |
| | NON-BID ITEMS | | | | | |
| | FILLER | SIZE | | | | 1/2", 3/4" |

(1) BRIDGE RIPRAP HEAVY AND GEOTEXTTILE TYPE HR QUANTITIES REPRESENT LIMITS SHOWN ON SHEET 1, GENERAL PLAN. SEE ROADWAY PLANS FOR ADDITIONAL LIMITS AND QUANTITIES AT THE NE, NW, AND SW QUADRANTS.



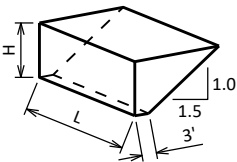
PROFILE GRADE LINE



TYPICAL SECTION THRU ABUTMENT

▲ BACKFILL PAY LIMITS. BACKFILL BEYOND PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.

■ PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN.



ABUTMENT BACKFILL DIAGRAM

L = OUT TO OUT OF ABUTMENT BODY INCLUDING WINGS (FT)
H = AVERAGE ABUTMENT FILL HEIGHT (FT)
EF = EXPANSION FACTOR (1.20 FOR CY BID ITEMS AND 1.00 FOR TON BID ITEMS)
 $V_{CF} = (L)(3.0')(H) + (L)(0.5)(1.5H)(H)$
 $V_{CY} = V_{CF}(EF)/27$
 $V_{TON} = V_{CY}(2.0)$

| NO. | DATE | REVISION | BY |
|--|------|----------|----------------|
| | | | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE B-47-233 | | | |
| DRAWN BY | | ERJ | PLANS CK'D SMN |
| CROSS SECTION & QUANTITIES | | | SHEET 2 |

| BORING # | DATE COMPLETED | NORTHING (Y) | EASTING (X) |
|---|----------------|--------------|-------------|
| B-1 | 12/05/2023 | 290416 | 442446 |
| B-2 | 12/06/2023 | 290367 | 442434 |
| BORINGS COMPLETED BY: PSI INTERTEK - CHIPPEWA FALLS | | | |
| REPORT COMPLETED BY: PSI INTERTEK - CHIPPEWA FALLS | | | |
| ALL COORDINATES REFERENCED TO WCCS NAD 83(91) PIERCE COUNTY | | | |

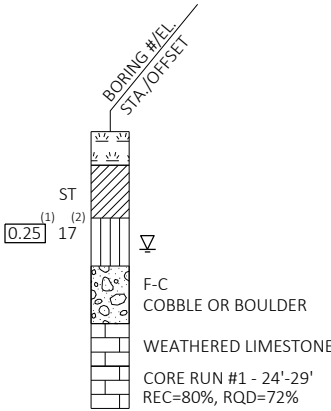
STATE PROJECT NUMBER

7893-00-70

MATERIAL SYMBOLS

| | | | | | |
|--|---------------------|--|-----------|--|-------------------|
| | ASPHALT | | TOPSOIL | | PEAT |
| | CONCRETE | | FILL | | GRAVEL |
| | SAND | | CLAY | | SILT |
| | BOULDERS OR COBBLES | | LIMESTONE | | BEDROCK (UNKNOWN) |
| | SHALE | | SANDSTONE | | IGNEOUS/META |

LEGEND OF BORING



⁽¹⁾ UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)

⁽²⁾ UNLESS OTHERWISE, SPECIFIED THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION

- ▽ AT TIME OF DRILLING
- ▼ END OF DRILLING
- ▼ AFTER DRILLING

ABBREVIATIONS

F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.

| NO. | DATE | REVISION | BY |
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|-----|------|----------|----|

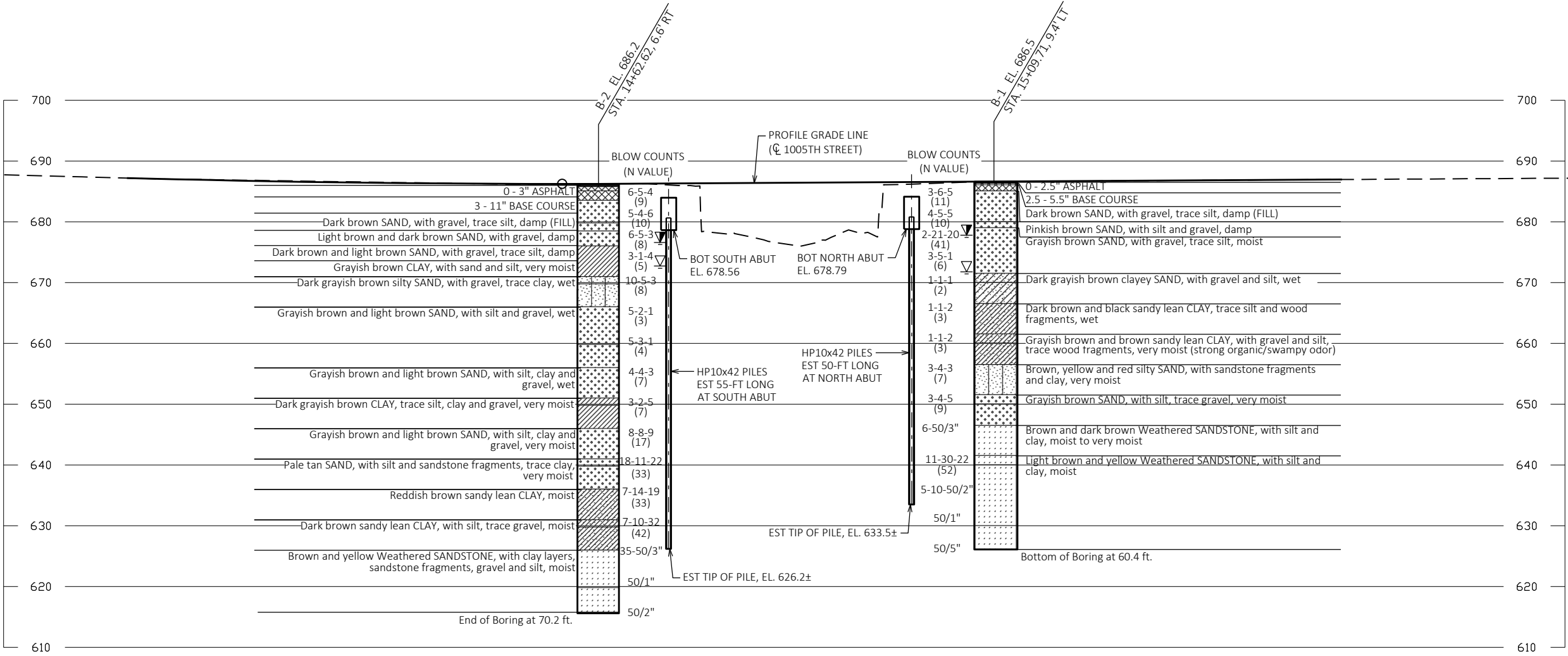
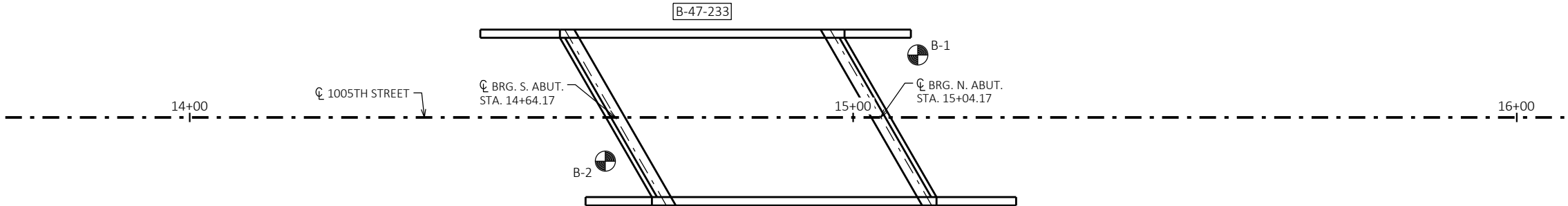
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-47-233

| | | | |
|----------|-----|------------|-----|
| DRAWN BY | ERJ | PLANS CK'D | SMN |
|----------|-----|------------|-----|

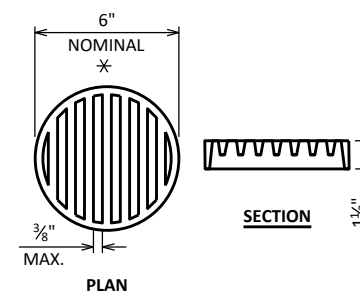
SUBSURFACE
EXPLORATION

SHEET 3





- (A01) CONST. JOINT: KEYWAY FORMED BY A BEVELED 2 x 6.
- (A06) SUPPORT ABUTMENT ON HP 10 x 42 STEEL PILING, ESTIMATED 55'-0" LONG WITH A REQUIRED DRIVING RESISTANCE OF 180 TONS PER PILE.
- (A15) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.
- (A17) 1/2" FILLER (INCLUDED IN WING LENGTH): SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF 3/4" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER (1" DEEP AND HOLD 3/8" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.
- (A19) 18" (RMW) RUBBERIZED MEMBRANE WATERPROOFING SEAL ALL HORIZ. & VERT. JOINTS AT BACKFACE.
- (A22) A508 BARS @ 1'-0" CTRS. MAY BE PLACED AFTER CONCRETE IS POURED BUT BEFORE INITIAL SET HAS TAKEN PLACE. (EMBED 1'-0" INTO CONC.)



RODENT SHIELD DETAIL

* DIMENSIONS ARE APPROXIMATE. THE GRATE IS SIZED TO FIT INTO A PIPE COUPLING. ORIENT SO SLOTS ARE VERTICAL.

THE RODENT SHIELD, PIPE COUPLING AND SCREWS SHALL BE CONSIDERED INCIDENTAL TO THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6-INCH".

THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE EXPOSED END OF THE PIPE UNDERDRAIN. THE SHIELD SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1-INCH STAINLESS STEEL SHEET METAL SCREWS.

| | | | |
|--|-------------|-----------------|-------------------|
| | | | |
| NO. | DATE | REVISION | BY |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE | | B-47-233 | |
| | DRAWN BY | VCH | PLANS CK'D SMN |
| SOUTH ABUTMENT | | SHEET 4 | |
| | | | |

BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

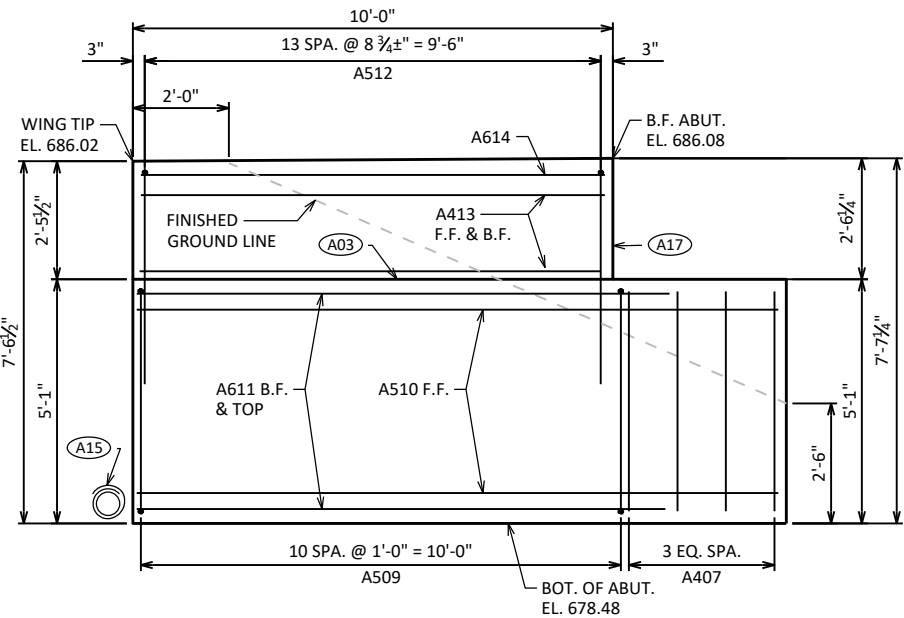
| BAR MARK | COAT | NO. REQ'D. | LENGTH | BENT | BAR SERIES | LOCATION |
|----------|------|------------|---------|------|------------|-------------------------------------|
| A501 | | 32 | 14'-2" | X | | BODY - STIRRUPS |
| A502 | | 8 | 14'-6" | X | ▲ | BODY - STIRRUPS - ENDS |
| A403 | | 10 | 2'-3" | | | BODY - VERT. - 2 PER PILE |
| A404 | | 5 | 28'-0" | X | | BODY - VERT. - AT BODY PILES |
| A805 | | 14 | 16'-7" | X | | BODY - HORIZ. - B.F. |
| A606 | | 11 | 30'-3" | | | BODY - HORIZ. - F.F. TOP AND BOTTOM |
| A407 | X | 7 | 4'-7" | | | BODY - VERT. - ENDS |
| A508 | X | 29 | 2'-0" | | | BODY - VERT. - DOWELS |
| A509 | X | 11 | 15'-10" | X | | WING 1 - STIRRUPS |
| A510 | X | 6 | 13'-0" | | | WING 1 - HORIZ. F.F. |
| A611 | X | 8 | 10'-11" | | | WING 1 - HORIZ. B.F. & TOP |
| A512 | X | 31 | 9'-6" | X | | WINGS 1 & 2 - VERT. |
| A413 | X | 6 | 9'-7" | | | WING 1 - HORIZ. - B.F. & F.F. |
| A614 | X | 2 | 9'-7" | | | WING 1 - HORIZ. - TOP |
| A515 | X | 13 | 15'-8" | X | | WING 2 - STIRRUPS |
| A516 | X | 6 | 13'-10" | | | WING 2 - HORIZ. - F.F. |
| A617 | X | 8 | 14'-11" | | | WING 2 - HORIZ. - B.F. & TOP |
| A418 | X | 6 | 11'-7" | | | WING 2 - HORIZ. - B.F. & F.F. |
| A619 | X | 2 | 11'-7" | | | WING 2 - HORIZ. - TOP |

▲ LENGTH SHOWN FOR BAR IS AN AVERAGE AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.

BAR SERIES TABLE

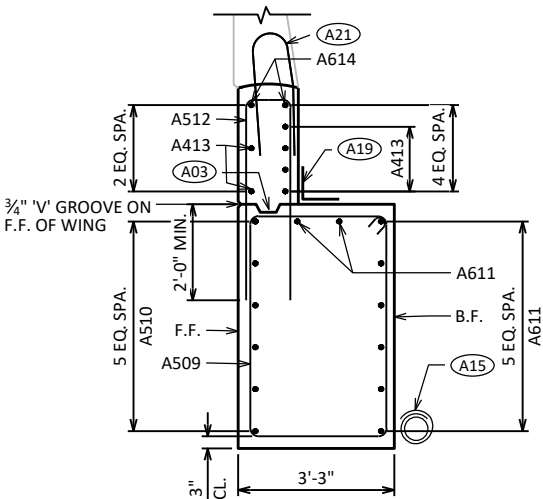
BUNDLE AND TAG EACH SERIES SEPARATELY.

| BAR MARK | NO. REQ'D. | LENGTH |
|----------|---------------|-------------------|
| A502 | 2 SERIES OF 4 | 14'-2" TO 14'-10" |

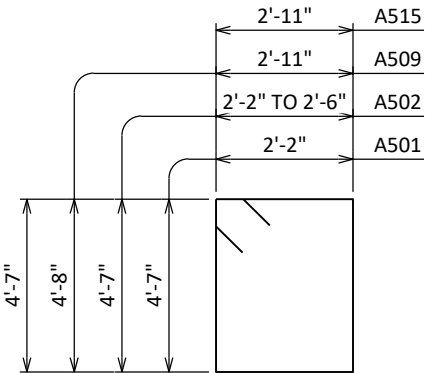


WING 1 ELEVATION

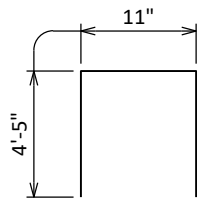
LOOKING NORTHWEST



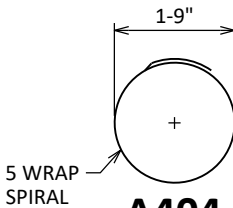
SECTION THRU WING 1



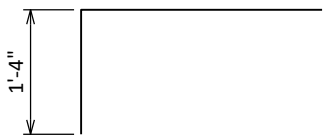
A501, A502, A509, A515



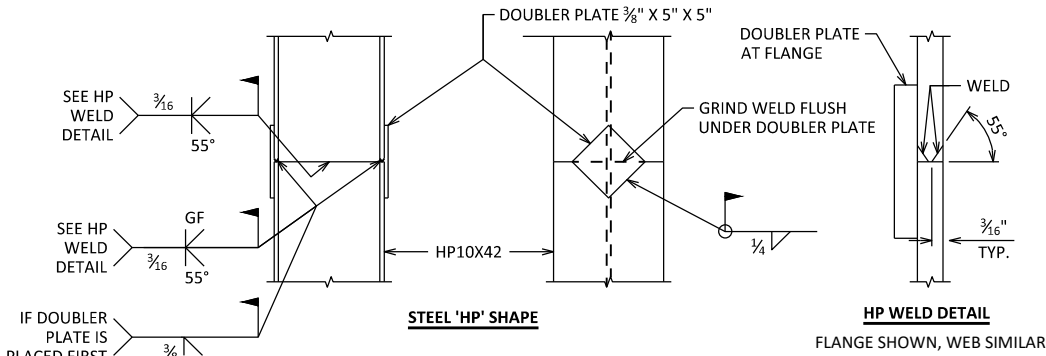
A512



A404

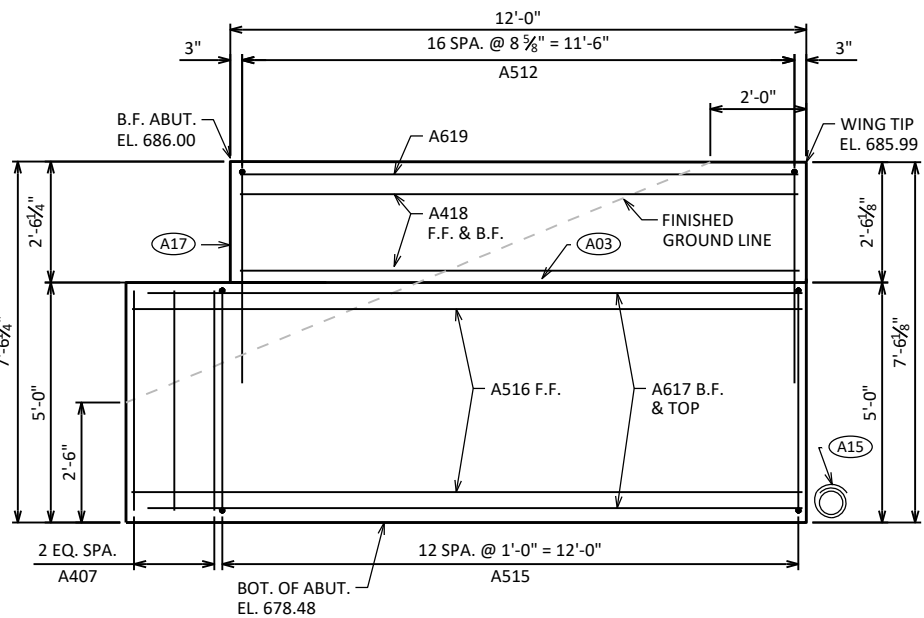


A805



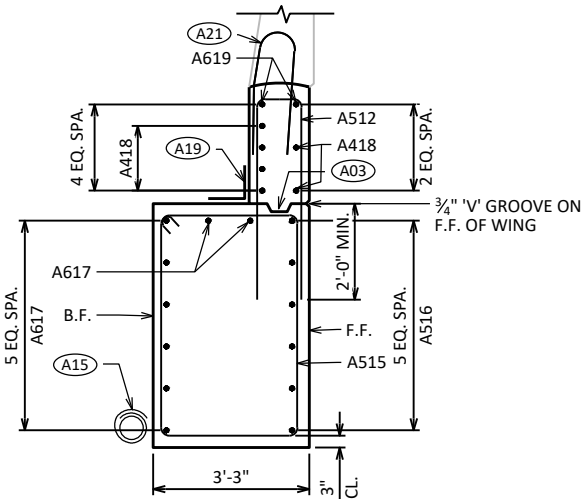
'HP' PILE DETAILS

A06



WING 2 ELEVATION

LOOKING SOUTHEAST



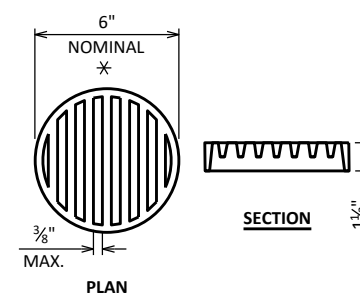
SECTION THRU WING 2

- A03 OPTIONAL CONST. JOINT: KEYWAY FORMED BY BEVELED 2 X 6 (18" RMW @ B.F. & 3/4" "V" GROOVE @ F.F. IF JOINTS IS USED).
- A06 SUPPORT ABUTMENT ON HP 10 x 42 STEEL PILING, ESTIMATED 55'-0" LONG WITH A REQUIRED DRIVING RESISTANCE OF 180 TONS PER PILE.
- A15 PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.
- A17 1/2" FILLER (INCLUDED IN WING LENGTH): SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF 3/4" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.
- A19 18" (RMW) RUBBERIZED MEMBRANE WATERPROOFING SEAL ALL HORIZ. & VERT. JOINTS AT BACKFACE.
- A21 FOR PPT. BARS & DIMENSIONS SEE PARAPET SHEET.

| NO. | DATE | REVISION | BY |
|--|------|----------|----------------|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE B-47-233 | | | |
| DRAWN BY | | VCH | PLANS CK'D SMN |
| SOUTH ABUTMENT DETAILS | | SHEET 5 | |



- (A01) CONST. JOINT: KEYWAY FORMED BY A BEVELED 2 x 6.
- (A06) SUPPORT ABUTMENT ON HP 10 x 42 STEEL PILING, ESTIMATED 50'-0" LONG WITH A REQUIRED DRIVING RESISTANCE OF 180 TONS PER PILE.
- (A15) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.
- (A17) ½" FILLER (INCLUDED IN WING LENGTH); SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF ¾" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD ½" BELOW SURFACE OF CONCRETE) EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.
- (A19) 18" (RMW) RUBBERIZED MEMBRANE WATERPROOFING SEAL ALL HORIZ. AND VERT. JOINTS AT BACKFACE.
- (A22) B508 BARS @ 1'-0" CTRS. SHOULD BE PLACED AFTER CONCRETE IS POURED BUT BEFORE INITIAL SET HAS TAKEN PLACE. (EMBED 1'-0" INTO CONC.)



RODENT SHIELD DETAIL

THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE EXPOSED END OF THE PIPE UNDERDRAIN. THE SHIELD SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1-INCH STAINLESS STEEL SHEET METAL SCREWS.

| | | | |
|--|-------------|-----------------|-------------------|
| | | | |
| NO. | DATE | REVISION | BY |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE | | B-47-233 | |
| | DRAWN BY | VCH | PLANS CK'D SMN |
| NORTH ABUTMENT | | SHEET 6 | |
| | | | |

BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

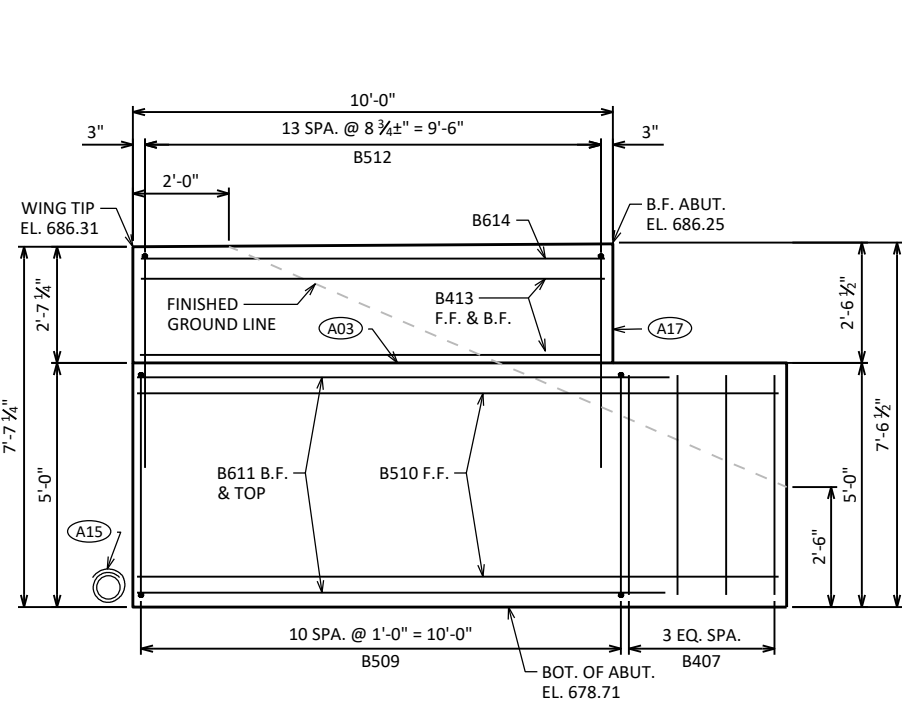
| BAR MARK | COAT | NO. REQ'D. | LENGTH | BENT | BAR SERIES | LOCATION |
|----------|------|------------|---------|------|------------|-------------------------------|
| B501 | | 32 | 14'-2" | X | | BODY - STIRRUPS |
| B502 | | 8 | 14'-6" | X | ▲ | BODY - STIRRUPS - ENDS |
| B403 | | 10 | 2'-3" | | | BODY - VERT. - 2 PER PILE |
| B404 | | 5 | 28'-0" | X | | BODY - VERT. - AT BODY PILES |
| B805 | | 14 | 16'-7" | X | | BODY - HORIZ. - B.F. |
| B606 | | 11 | 30'-3" | | | BODY - HORIZ. |
| B407 | X | 7 | 4'-7" | | | BODY - VERT. - ENDS |
| B508 | X | 29 | 2'-0" | | | BODY - VERT. - DOWELS |
| B509 | X | 11 | 15'-8" | X | | WING 3 - STIRRUPS |
| B510 | X | 6 | 13'-0" | | | WING 3 - HORIZ. F.F. |
| B611 | X | 8 | 10'-11" | | | WING 3 - HORIZ. B.F. & TOP |
| B512 | X | 31 | 9'-8" | X | | WINGS 3 & 4 - VERT. |
| B413 | X | 6 | 9'-7" | | | WING 3 - HORIZ. - B.F. & F.F. |
| B614 | X | 2 | 9'-7" | | | WING 3 - HORIZ. - TOP |
| B515 | X | 13 | 15'-10" | X | | WING 4 - STIRRUPS |
| B516 | X | 6 | 13'-10" | | | WING 4 - HORIZ. - F.F. |
| B617 | X | 8 | 14'-11" | | | WING 4 - HORIZ. - B.F. & TOP |
| B418 | X | 6 | 11'-7" | | | WING 4 - HORIZ. - B.F. & F.F. |
| B619 | X | 2 | 11'-7" | | | WING 4 - HORIZ. - TOP |

▲ LENGTH SHOWN FOR BAR IS AN AVERAGE AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.

BAR SERIES TABLE

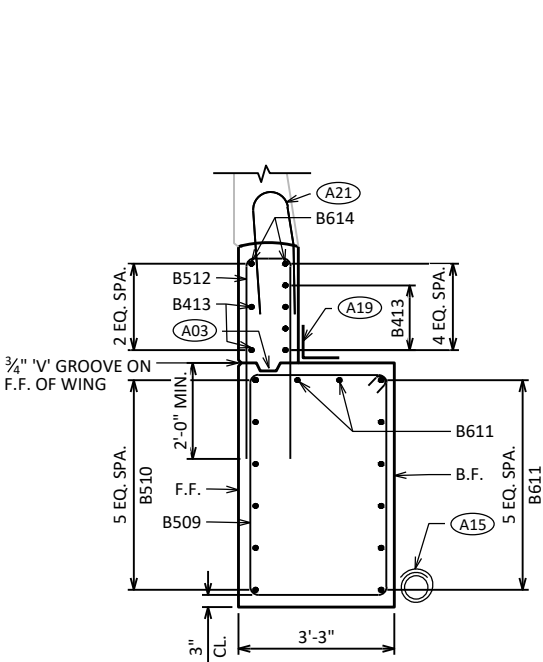
BUNDLE AND TAG EACH SERIES SEPARATELY.

| BAR MARK | NO. REQ'D. | LENGTH |
|----------|---------------|-------------------|
| B502 | 2 SERIES OF 4 | 14'-2" TO 14'-10" |

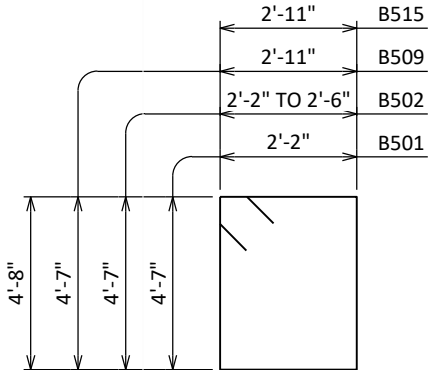


WING 3 ELEVATION

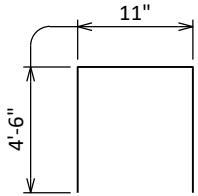
LOOKING SOUTHEAST



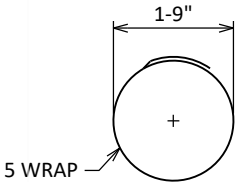
SECTION THRU WING 3



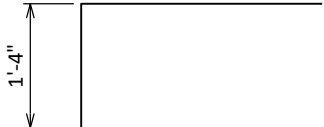
B501, B502, B509, B515



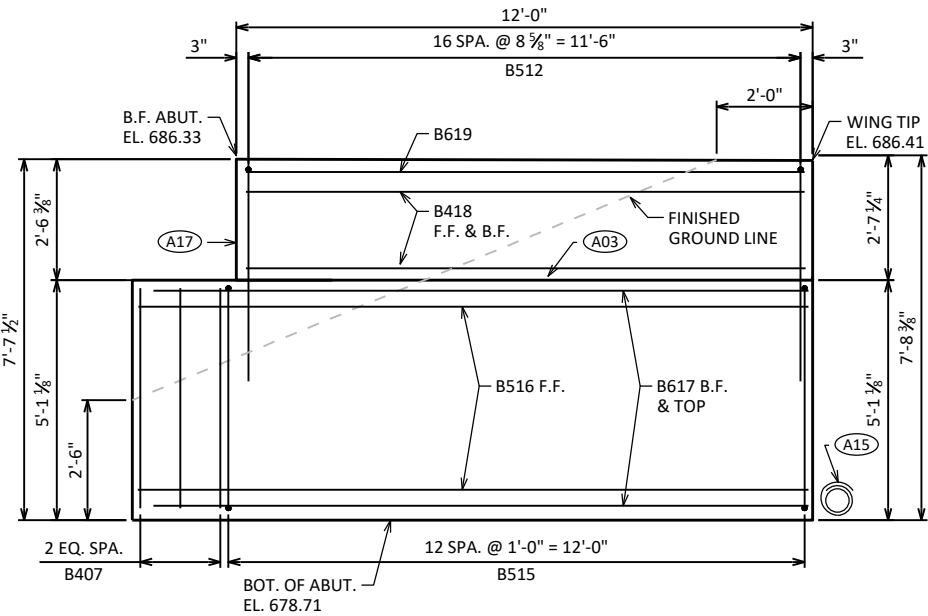
B512



B404

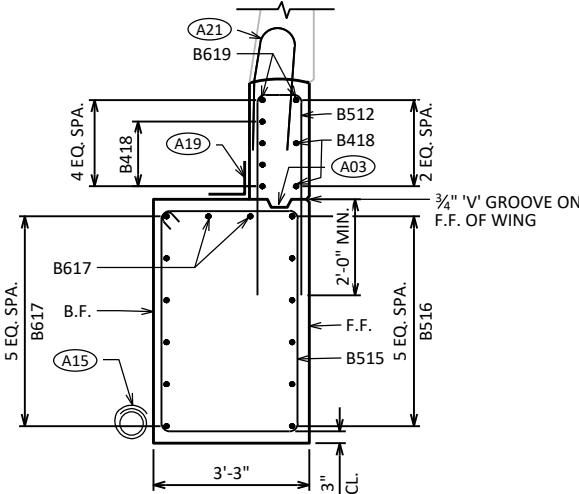


B805

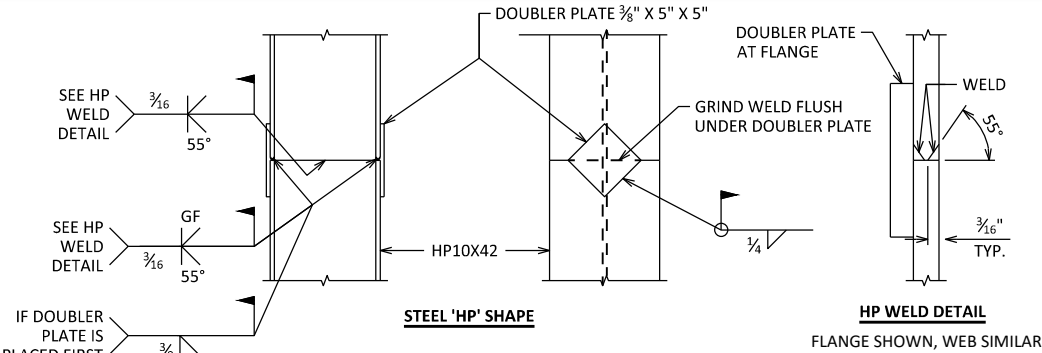


WING 4 ELEVATION

LOOKING NORTHWEST



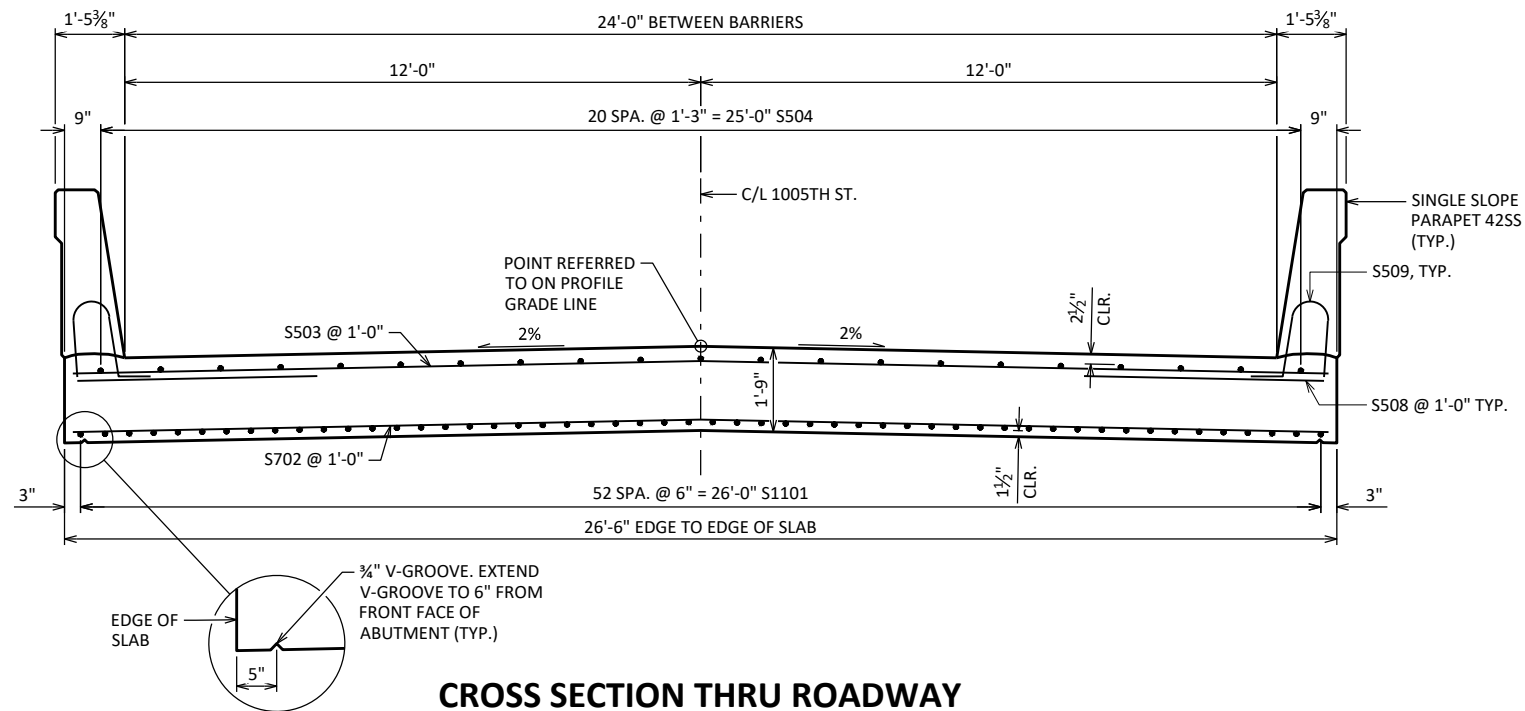
SECTION THRU WING 4



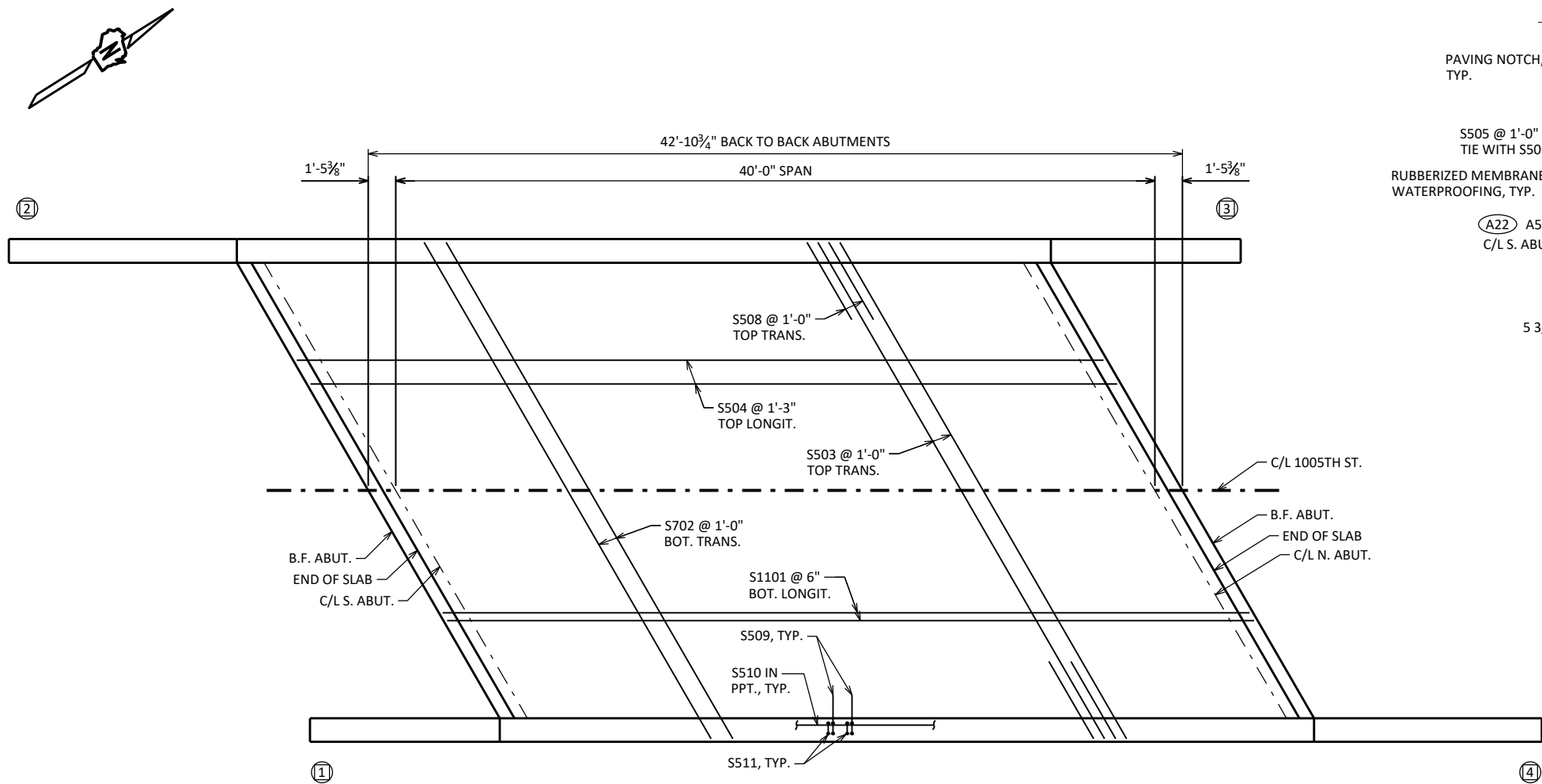
'HP' PILE DETAILS

- A03 OPTIONAL CONST. JOINT: KEYWAY FORMED BY BEVELED 2 X 6 (18" RMW @ B.F. & 3/4" "V" GROOVE @ F.F. IF JOINTS IS USED).
- A06 SUPPORT ABUTMENT ON HP 10 x 42 STEEL PILING, ESTIMATED 50'-0" LONG WITH A REQUIRED DRIVING RESISTANCE OF 180 TONS PER PILE.
- A15 PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.
- A17 1/2" FILLER (INCLUDED IN WING LENGTH); SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 3/8" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.
- A19 18" (RMW) RUBBERIZED MEMBRANE WATERPROOFING SEAL ALL HORIZ. & VERT. JOINTS AT BACKFACE.
- A21 FOR PPT. BARS & DIMENSIONS SEE PARAPET SHEET.

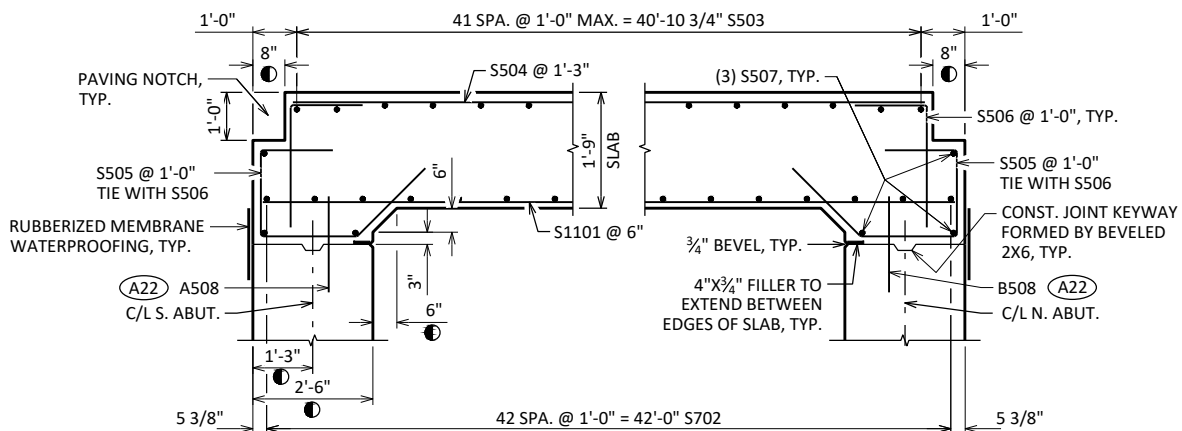
| NO. | DATE | REVISION | BY |
|--|------|----------|----------------|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE B-47-233 | | | |
| DRAWN BY | | VCH | PLANS CK'D SMN |
| NORTH ABUTMENT DETAILS | | | SHEET 7 |



CROSS SECTION THRU ROADWAY



PLAN



LONGITUDINAL SECTION

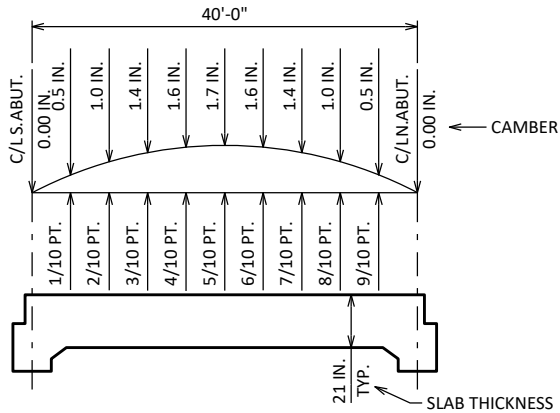
DIMENSIONS ARE GIVEN PARALLEL TO C/L ROADWAY UNLESS OTHERWISE NOTED.

MEASURED NORMAL TO THE C/L OF ABUTMENT. DIMENSIONS ARE TYPICAL FOR BOTH ABUTMENTS.

A508, B508 BARS SPACED @ 1'-0" CNTRS. MAY BE PLACED AFTER CONCRETE IS POURED BUT BEFORE INITIAL SET HAS TAKEN PLACE. (EMBED 1'-0" INTO CONC.)

| NO. | DATE | REVISION | BY |
|--|------|----------|----------------|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE B-47-233 | | | |
| DRAWN BY | | VCH | PLANS CK'D SMN |
| SUPERSTRUCTURE | | SHEET 8 | |

SCALE =



CAMBER AND SLAB THICKNESS DIAGRAM

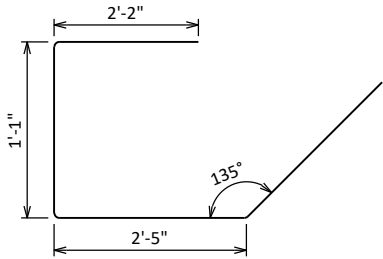
CAMBER SHOWN IS BASED ON 3 TIMES DEAD LOAD DEFLECTIONS. CAMBER SPANS AS SHOWN TO PROVIDE FOR DEAD LOAD DEFLECTION AND FUTURE CREEP. CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT. PARAPETS PLACED ON TOP OF THE SLAB SHALL BE POURED AFTER FALSEWORK HAS BEEN RELEASED, EXCEPT FOR STAGED CONSTRUCTION.

TO DETERMINE FALSEWORK ELEVATION AT EDGE OF SLAB, CROWN OR REFERENCE LINE FOLLOW THIS PROCEDURE:

| | |
|--------|---|
| LESS | TOP OF SLAB ELEVATION AT FINAL GRADE |
| PLUS | SLAB THICKNESS |
| PLUS | CAMBER |
| PLUS | FORM SETTLEMENT/DEFLECTION DUE TO PLACEMENT OF SLAB CONCRETE (TO BE COMPUTED BY THE CONTRACTOR) |
| EQUALS | TOP OF SLAB FALSEWORK ELEVATION |

TOP OF SLAB ELEVATIONS

| LOCATION | C/L BRG. S. ABUT. | 1/10 PT. | 2/10 PT. | 3/10 PT. | 4/10 PT. | 5/10 PT. | 6/10 PT. | 7/10 PT. | 8/10 PT. | 9/10 PT. | C/L BRG. N. ABUT. |
|-----------------|-------------------|----------|----------|----------|----------|----------|----------|----------|----------|----------|-------------------|
| W. EDGE OF DECK | 686.00 | 686.02 | 686.05 | 686.07 | 686.09 | 686.12 | 686.14 | 686.17 | 686.19 | 686.21 | 686.24 |
| CROWN OR R/L | 686.28 | 686.31 | 686.33 | 686.36 | 686.38 | 686.40 | 686.43 | 686.45 | 686.48 | 686.50 | 686.52 |
| E. EDGE OF DECK | 686.09 | 686.11 | 686.14 | 686.16 | 686.18 | 686.21 | 686.23 | 686.26 | 686.28 | 686.30 | 686.33 |



S505

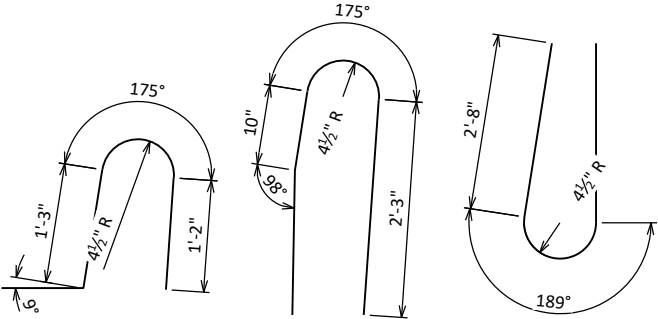


S506

BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

| BAR MARK | COAT | NO. REQ'D. | LENGTH | BENT | BAR SERIES | LOCATION |
|----------|------|------------|--------|------|------------|---------------------------------|
| S1101 | X | 53 | 42'-6" | | | SLAB BOTTOM LONGITUDINAL |
| S702 | X | 43 | 30'-3" | | | SLAB BOTTOM TRANSVERSE |
| S503 | X | 42 | 30'-3" | | | SLAB TOP TRANSVERSE |
| S504 | X | 21 | 41'-0" | | | SLAB TOP LONGITUDINAL |
| S505 | X | 54 | 7'-5" | X | | ABUTMENT DIAPHRAGM STIRRUPS |
| S506 | X | 54 | 3'-6" | X | | L-BARS @ ABUTMENT |
| S507 | X | 6 | 30'-3" | | | ABUTMENT DIAPHRAGM LONGITUDINAL |
| S508 | X | 82 | 5'-0" | | | SLAB TOP EDGE TRANSVERSE |
| S509 | X | 126 | 4'-5" | X | | PARAPET VERT. |
| S510 | X | 16 | 42'-6" | | | PARAPET HORIZ. |
| S511 | X | 130 | 6'-8" | X | | PARAPET VERT. |
| S512 | X | 4 | 5'-10" | X | | PARAPET VERT. @ PAVING NOTCH |



S509

S512

S511

SURVEY TOP OF SLAB ELEVATIONS

| LOCATION | ABUTMENT | 5/10 PT. | ABUTMENT |
|--------------|----------|----------|----------|
| W. GUTTER | | | |
| CROWN OR R/L | | | |
| E. GUTTER | | | |

PRIOR TO RELEASING SLAB FALSEWORK, TAKE TOP OF SLAB ELEVATIONS AT THE C/L OF ABUTMENTS, THE C/L OF PIERS AND AT 5/10 PTS. TO VERIFY CAMBER. TAKE ELEVATIONS ALONG GUTTER LINES AND CROWN OR R/L. RECORD THE ELEVATIONS IN THE ABOVE TABLE FOR THE "AS BUILT" PLANS.

NOTES

FILL IN THE TABLE OF "SURVEY TOP OF SLAB ELEVATIONS" FOR EACH SPAN ON AS BUILT PLANS.

TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 3'-0" CENTERS EACH WAY. BOTTOM LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 4'-0" CENTERS.

ALL SLAB THICKNESS DIMENSIONS ARE MINIMUM. ANY TOLERANCES NECESSARY TO CORRECT CONSTRUCTION DISCREPANCIES ARE TO BE PLUS (+).

PARAPETS PLACED ON TOP OF THE SLAB SHALL BE POURED AFTER FALSEWORK HAS BEEN RELEASED.

| | | | |
|--|------|----------|----------------|
| | | | |
| NO. | DATE | REVISION | BY |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE B-47-233 | | | |
| DRAWN BY | | VCH | PLANS CK'D SMN |
| SUPERSTRUCTURE DETAILS | | SHEET 9 | |
| | | | |

SCALE =

BILL OF BARS

FOR WING PARAPETS

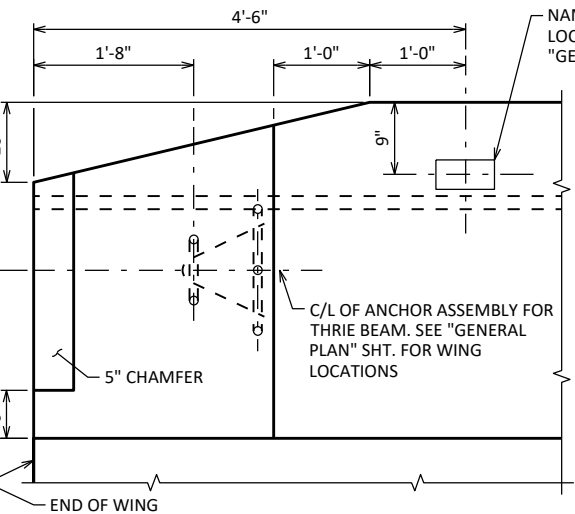
| BAR MARK | COAT | S. ABUT. | N. ABUT. | LENGTH | BENT | BAR SERIES | LOCATION |
|----------|------|----------|----------|--------|------|------------|----------------|
| R501 | X | 9 | 9 | 5'-10" | X | | PARAPET VERT. |
| R502 | X | 9 | 9 | 6'-8" | X | | PARAPET VERT. |
| R503 | X | 24 | 24 | 3'-0" | X | | PARAPET VERT. |
| R504 | X | 34 | 34 | 5'-7" | X | | PARAPET VERT. |
| R505 | X | 10 | 10 | 6'-5" | X | | PARAPET VERT. |
| R506 | X | 12 | 12 | 6'-6" | X | | PARAPET VERT. |
| R507 | X | 1 | 1 | 9'-7" | X | | PARAPET HORIZ. |
| R508 | X | 5 | 5 | 9'-7" | | | PARAPET HORIZ. |
| R509 | X | 12 | 12 | 5'-5" | X | ▲ | PARAPET VERT. |
| R510 | X | 2 | 2 | 9'-7" | X | | PARAPET HORIZ. |
| R511 | X | 1 | 1 | 11'-7" | X | | PARAPET HORIZ. |
| R512 | X | 5 | 5 | 11'-7" | | | PARAPET HORIZ. |
| R513 | X | 2 | 2 | 11'-7" | X | | PARAPET HORIZ. |

▲ LENGTH SHOWN FOR BAR IS AN AVERAGE AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.

BAR SERIES TABLE

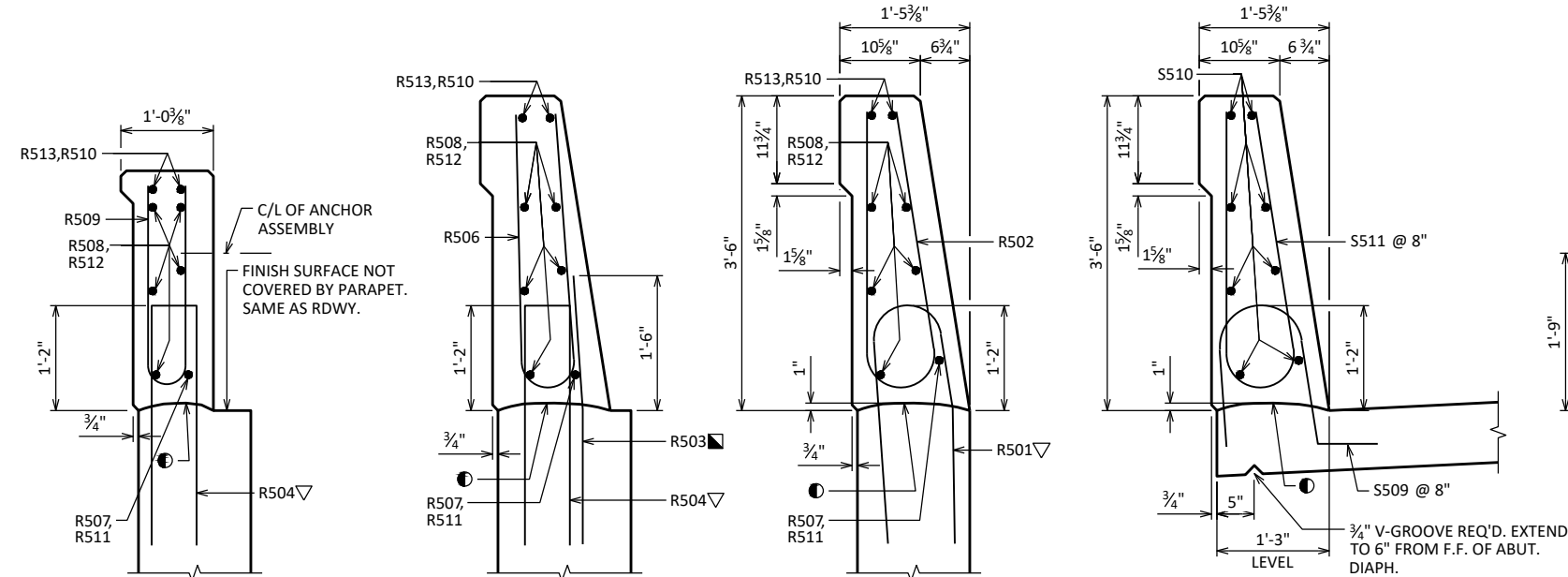
BUNDLE AND TAG EACH SERIES SEPARATELY.

| BAR MARK | NO. REQ'D. | LENGTH |
|----------|---------------|----------------|
| R509 | 4 SERIES OF 6 | 4'-9" TO 6'-1" |



PARAPET END TREATMENT DETAIL

LOOKING AT INSIDE FACE OF PARAPET

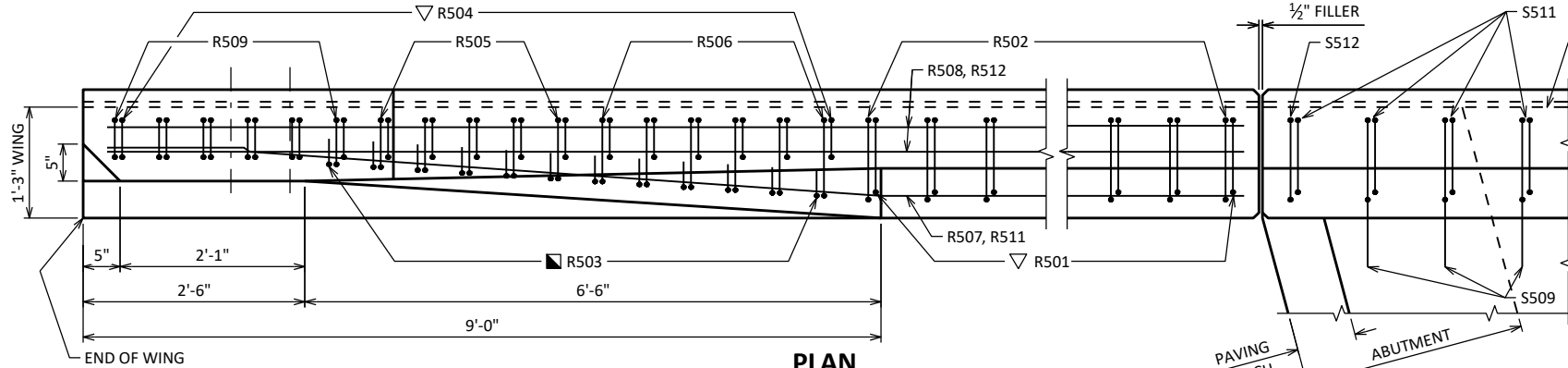


SECTION A-A

SECTION B-B

SECTION C-C

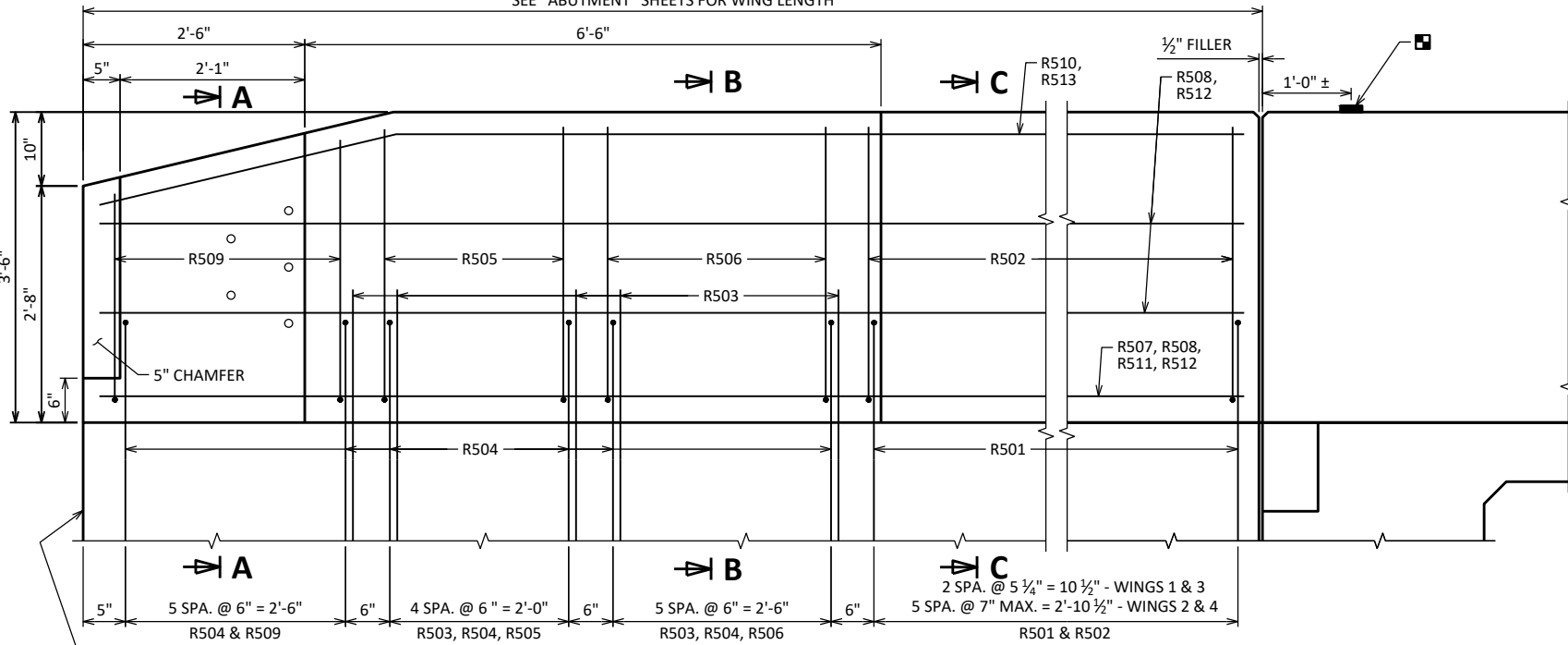
SECTION THRU PARAPET ON DECK



PLAN

SW CORNER SHOWN, OTHERS SIMILAR

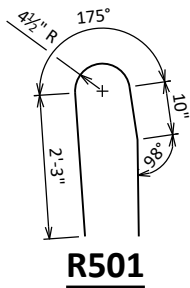
SEE "ABUTMENT" SHEETS FOR WING LENGTH



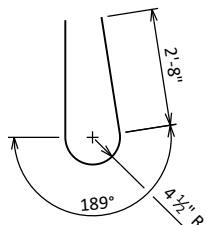
INSIDE ELEVATION

SW CORNER SHOWN, OTHERS SIMILAR

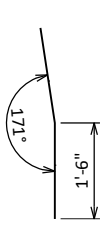
OPTIONAL CONSTRUCTION JOINTS IN THE PARAPETS MAY BE USED. RUN BAR REINF. THRU THE JOINT. LAP LONGIT. BARS A MIN. OF 1'-9". MIN. JOINT SPACING OF 80'-0". DEFINE CONST. JOINT WITH A 3/4" - 'V' GROOVE



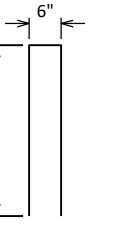
R501



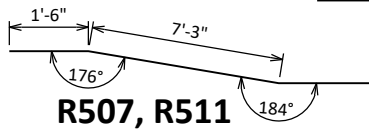
R502



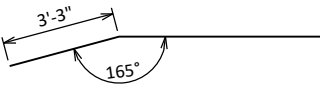
R503



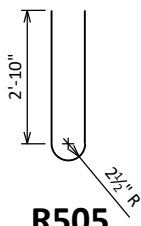
R504



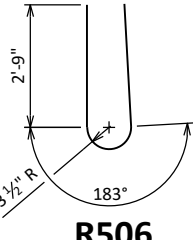
R507, R511



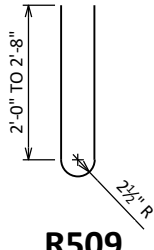
R510, R513



R505

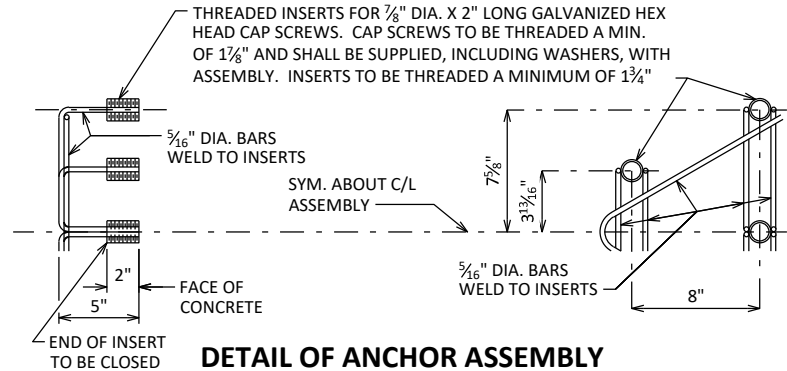


R506



R509

■ BENCH MARK CAP (WHEN SUPPLIED). AVOID PLACING A BENCH MARK CAP BELOW A RAIL OR FENCE SYSTEM THAT IS ATTACHED TO THE TOP OF THE PARAPET.



DETAIL OF ANCHOR ASSEMBLY

NOTE: HEX HEAD CAP SCREWS & WASHERS TO BE GALVANIZED IN ACCORDANCE WITH ASTM F2329.

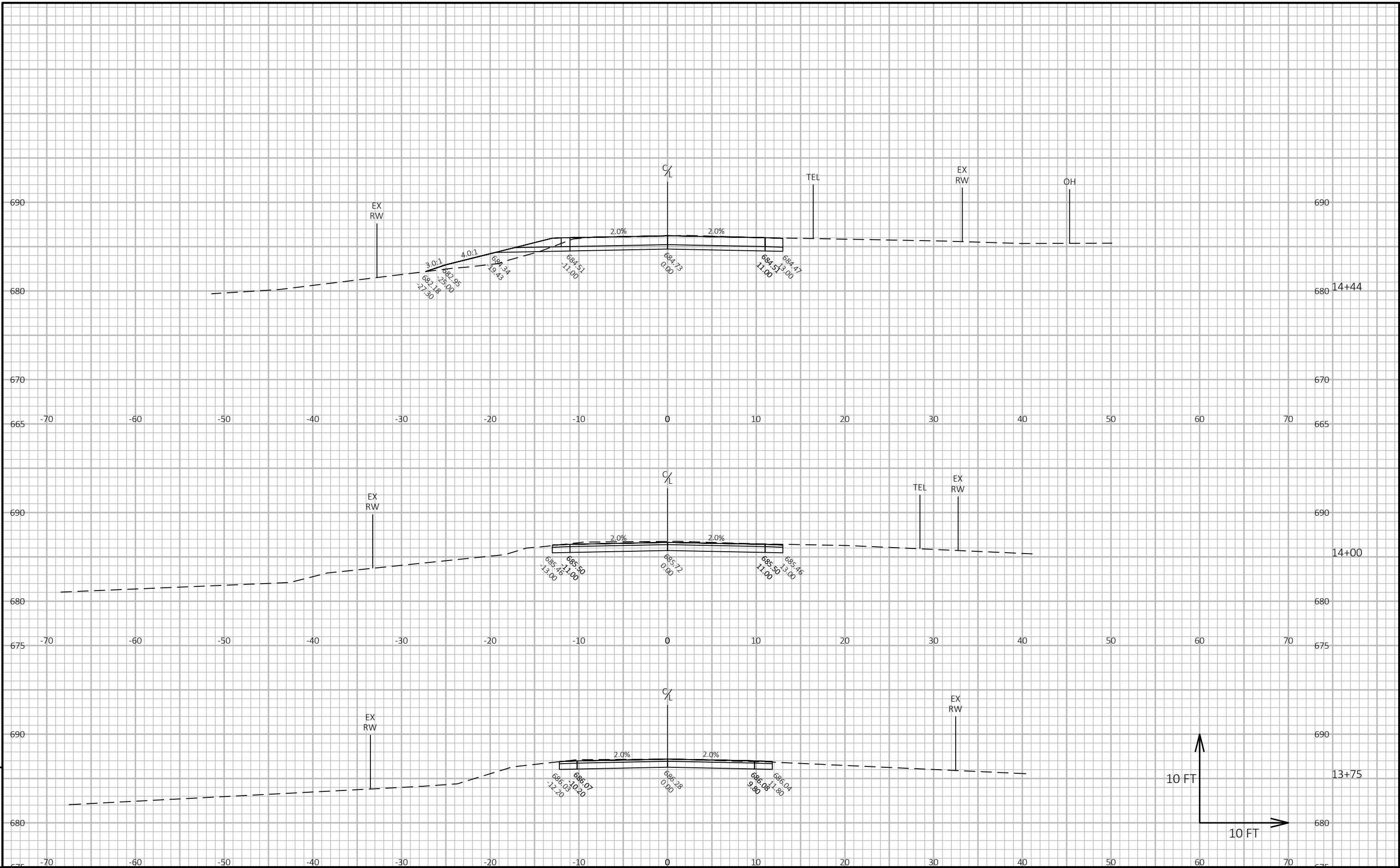
ASSEMBLY SHALL BE BID ITEM "ANCHOR ASSEMBLIES FOR STEEL PLATE BEAM GUARD", EACH.

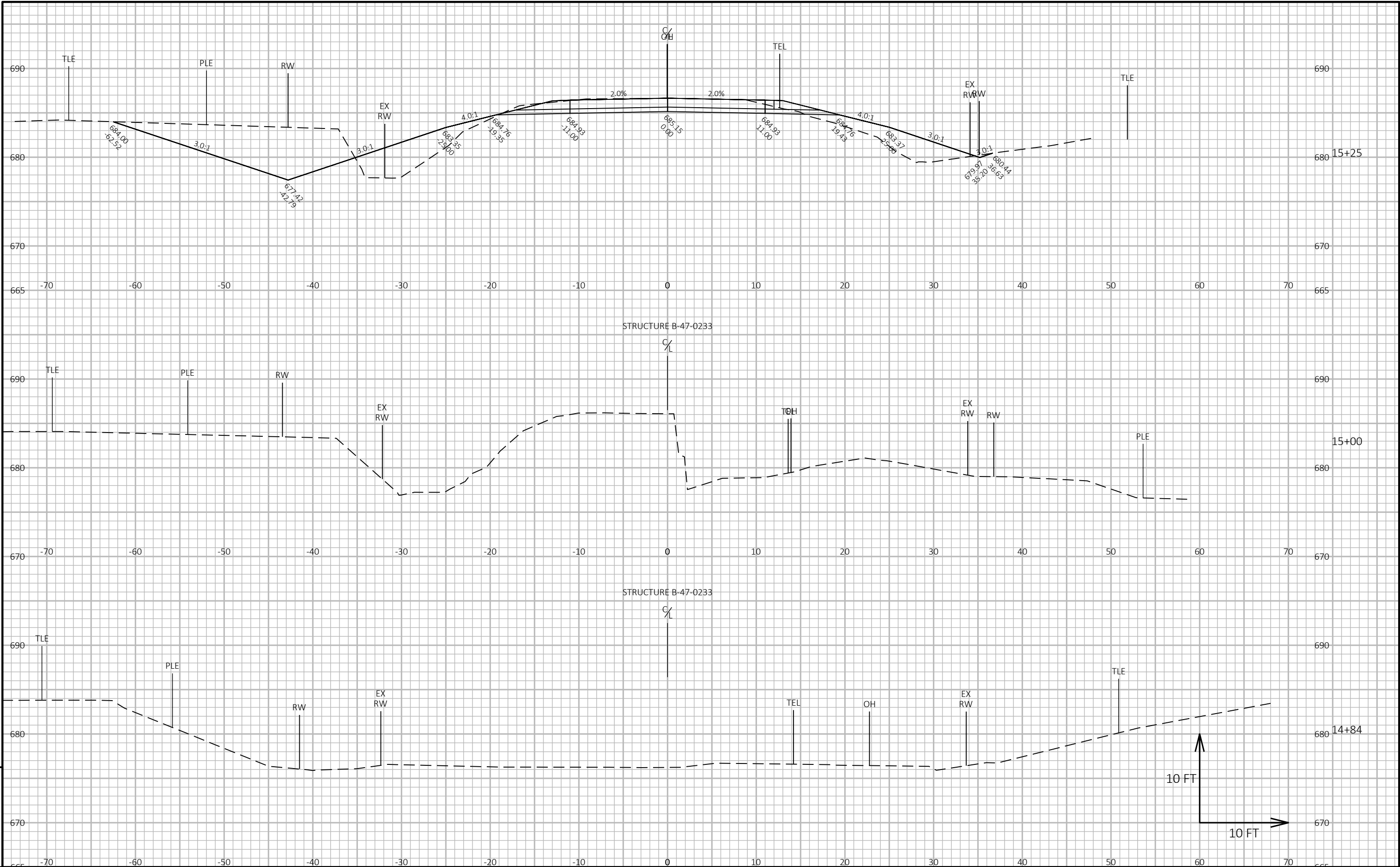
| NO. | DATE | REVISION | BY |
|---|------|----------|----------------|
| | | | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION | | | |
| STRUCTURE B-47-233 | | | |
| DRAWN BY | | VCH | PLANS CK'D SMN |
| SINGLE SLOPE PARAPET 42SS | | SHEET 10 | |

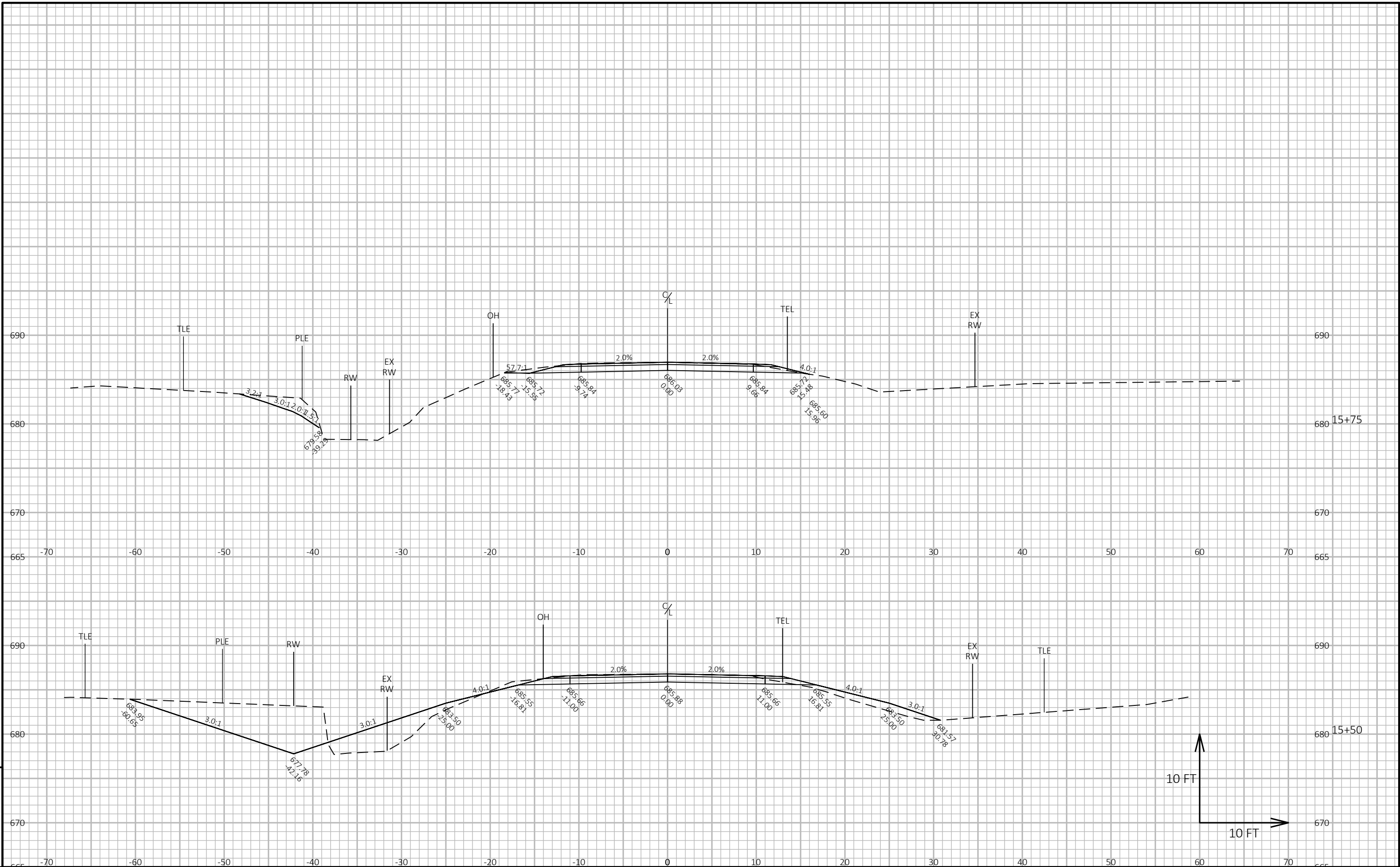
DIVISION 1 -1005 ST

| STATION | REAL STATION | DISTANCE | AREA (SF) | | | INCREMENTAL VOL (CY) (UNADJUSTED) | | | CUMULATIVE VOL (CY) | | |
|----------|--------------|----------|-----------|--|-------|-----------------------------------|--|--------|---------------------|---------------|--------------|
| | | | CUT | SALVAGED/UNUSABLE PAVEMENT MATERIAL | FILL | CUT | SALVAGED/UNUSABLE PAVEMENT MATERIAL | FILL | CUT | EXPANDED FILL | MASS ORDNATE |
| | | | | | | | | | | | |
| | | | | | | NOTE 1 | NOTE 2 | NOTE 3 | NOTE 1 | 1.25 | |
| 13+75.00 | 1375.00 | 0.00 | 18.22 | 4.00 | 0.00 | 0 | 0 | 0 | 0 | 0 | 0 |
| 14+00.00 | 1400.00 | 25.00 | 22.34 | 4.00 | 0.00 | 19 | 4 | 0 | 19 | 0 | 15 |
| 14+43.78 | 1443.78 | 43.78 | 38.02 | 5.00 | 8.79 | 49 | 7 | 7 | 68 | 9 | 48 |
| 14+62.72 | 1462.72 | 18.94 | 38.02 | 5.00 | 8.79 | 27 | 4 | 6 | 95 | 16 | 64 |
| 14+84.17 | 1484.17 | 21.45 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 95 | 16 | 48 |
| 15+00.00 | 1500.00 | 15.83 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 95 | 16 | 48 |
| 15+05.61 | 1505.61 | 5.61 | 133.97 | 5.00 | 63.99 | 14 | 1 | 7 | 109 | 25 | 53 |
| 15+25.00 | 1525.00 | 19.39 | 133.97 | 5.00 | 63.99 | 96 | 4 | 46 | 205 | 74 | 83 |
| 15+50.00 | 1550.00 | 25.00 | 88.46 | 4.00 | 39.80 | 103 | 4 | 48 | 308 | 134 | 122 |
| 15+75.00 | 1575.00 | 25.00 | 29.41 | 4.00 | 0.11 | 55 | 4 | 18 | 363 | 156 | 150 |

| | |
|---|--|
| NOTES: | |
| 1 - CUT | CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL |
| 2 - SALVAGED/UNUSABLE PAVEMENT MATERIAL | THIS DOES NOT SHOW UP IN CROSS SECTIONS |
| 3 - FILL | DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME |







9

9

| | | | | |
|------------------------|-------------------|----------------|---------------------------|---------|
| PROJECT NO: 7893-00-70 | HWY: LOCAL STREET | COUNTY: PIERCE | CROSS SECTIONS: 1005TH ST | SHEET E |
|------------------------|-------------------|----------------|---------------------------|---------|



Wisconsin Department of Transportation

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