EAU

PROJECT ID: WITH: N/A

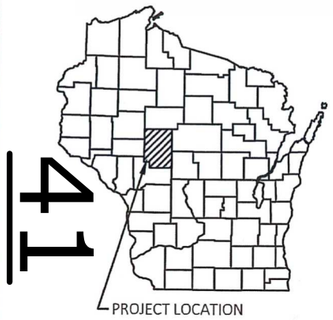
7852-00-71

COUNTY: CLARK

MARCH 2025

ORDER OF SHEETS

TOTAL SHEETS = 46



DESIGN DESIGNATION 7852-00-01

A.A.D.T.	2025	=	<100
A.A.D.T.	2045	=	<100
D.H.V.		=	
D.D.		=	60/40
T.		=	
DESIGN SPEED		=	30 MPH
ESALS		=	

CONVENTIONAL SYMBOLS

PLAN		PROFILE	
CORPORATE LIMITS		GRADE LINE	
PROPERTY LINE		ORIGINAL GROUND	
LOT LINE		MARSH OR ROCK PROFILE (To be noted as such)	ROCK
LIMITED HIGHWAY EASEMENT		SPECIAL DITCH	LABEL
EXISTING RIGHT OF WAY		GRADE ELEVATION	95.36
PROPOSED OR NEW R/W LINE		CULVERT (Profile View)	
SLOPE INTERCEPT		UTILITIES	
REFERENCE LINE	300'EB'	ELECTRIC	E
EXISTING CULVERT		FIBER OPTIC	FO
PROPOSED CULVERT (Box or Pipe)		GAS	G
COMBUSTIBLE FLUIDS	CAUTION	SANITARY SEWER	SAN
		STORM SEWER	SS
		TELEPHONE	T
MARSH AREA		WATER	W
		UTILITY PEDESTAL	
		POWER POLE	
WOODED OR SHRUB AREA		TELEPHONE POLE	

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

T MENTOR, MAIN STREET

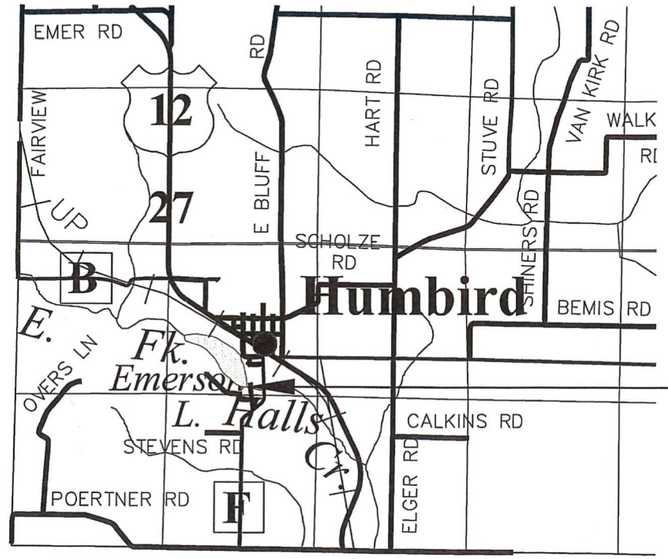
E FORK HALLS CREEK BRIDGE B-10-0257

LOC STR

CLARK

STATE PROJECT NUMBER
7852-00-71

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
7852-00-71	WISC 2025392	1



Town of Cleveland

Town of Alma

BEGIN PROJECT
STA 9+25.00
Y = 337277.8276
X = 608249.5247

STRUCTURE B-10-0257
STA 9+98.00

END PROJECT
STA 10+71.00
Y = 337422.5829
X = 608230.5426

LAYOUT

SCALE 0 1.0 MI

TOTAL NET LENGTH OF CENTERLINE = 0.028 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), CLARK COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2001). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

ACCEPTED FOR
TOWN OF
MENTOR

DATE 10-24-24 Timothy Gibb
(Signature)

Town Chairman
(Title)

ORIGINAL PLANS PREPARED BY

Cedar CORPORATION

MENOMONIE - MADISON - GREEN BAY - CEDARBURG
www.cedarcorp.com
800-472-7372

WISCONSIN
WILLIAM A. BETZIG
E-27689
HARTLAND, WI
PROFESSIONAL ENGINEER

DATE: 10/14/24 William A Betzig
(Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	CEDAR CORPORATION
Designer	CEDAR CORPORATION
Project Manager	TOU YANG, P.E.
Regional Examiner	NW REGION
Regional Supervisor	TOU YANG, P.E.

APPROVED FOR THE DEPARTMENT

DATE: 10/28/2024 Jon R Yang
(Signature)

E

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

NO TREES OR SHRUBS ARE TO BE REMOVED UNLESS SUCH TREES OR SHRUBS HAVE FIRST BEEN INDICATED FOR REMOVAL BY THE ENGINEER IN THE FIELD.

BEARINGS REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), CLARK COUNTY.

DISTURBED AREAS WITHIN THE RIGHT OF WAY ARE TO BE TOPSOILED, FERTILIZED, SEEDED, AND COVERED WITH EROSION MAT.

WETLANDS ARE PRESENT WITHIN THE PROJECT LIMITS. DO NOT OPERATE EQUIPMENT OUTSIDE OF THE SLOPE INTERCEPTS. DO NOT STORE OR STOCKPILE MATERIALS IN WETLANDS

WHEN THE QUANTITY OF ITEM BASE LAYER OR SURFACE LAYER IS MEASURED FOR PAYMENT BY THE TON, THE THICKNESS OF THE MATERIAL THAT IS SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF MATERIAL AS DIRECTED BY THE ENGINEER.

THE WISCONSIN DEPARTMENT OF TRANSPORTATION WILL FURNISH THE CONTRACTOR A MONUMENT WHICH SHALL BE SET IN THE STRUCTURE AS DESIGNATED BY ENGINEER.

DNR CONTACT

BLACK RIVER FALLS DNR SERVICE CENTER
910 HWY 54 E
BLACK RIVER FALLS, WI 54615
ATTN: BRAD BETTHAUSER
PH: (715) 213-9064
EMAIL: bradley.betthauser@wisconsin.gov

DESIGN CONSULTANT CONTACT

CEDAR CORPORATION
604 WILSON AVENUE
MENOMONIE, WI 54751
ATTN: DENNIS A. MACK, P.E.
PH: (715) 235-9081
EMAIL: dennis.mack@cedarcorp.com

MUNICIPALITY

TOWN OF MENTOR
N3049 KING STREET
HUMBIRD, WI 54746
ATTN: TIM GILE, TOWN CHAIRMAN
PH: (715) 964-1019
EMAIL: chairman@townofmentor.com

UTILITY CONTACTS

ELECTRIC
XCEL ENERGY, INC.
1400 WESTERN AVENUE
EAU CLAIRE, WI 54701
ATTN: JOHN KELSER, PRINCIPAL DESIGNER
PH: (715)737-6020
EMAIL: john.kelser@xcelenergy.com

GAS
WE ENERGIES
1921 8TH STREET SOUTH
WISCONSIN RAPIDS, WI 54494
ATTN: LARRY KOCH
PH: (715) 421-9293
EMAIL: larry.koch@we-energies.com

DIGGERS HOTLINE
Dial 811 or (800)242-8511
www.DiggersHotline.com
**DENOTES UTILITIES THAT ARE NOT DIGGERS HOTLINE MEMBERS.

2

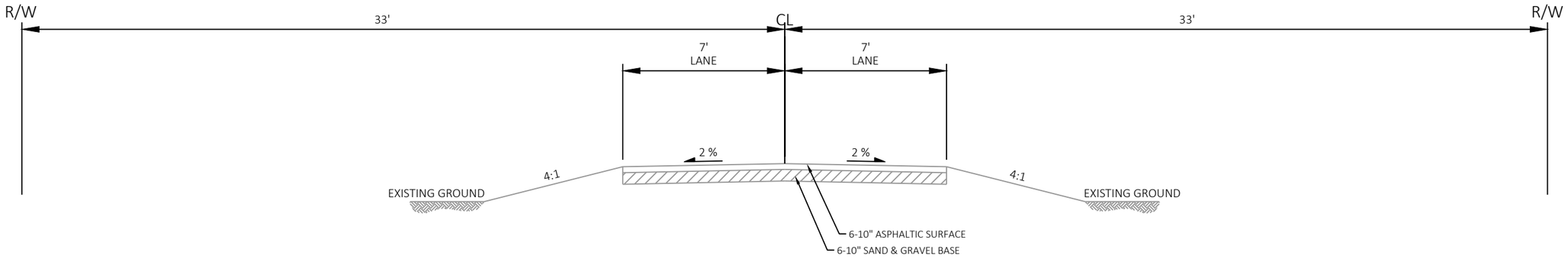
STANDARD ABBREVIATIONS

ABUT	ABUTMENT	OFF	OFFSET
AGG	AGGREGATE	PC	POINT OF CURVATURE
ET AL	AND OTHERS	PI	POINT OF INTERSECTION
AADT	ANNUAL AVERAGE DAILY TRAFFIC	PT	POINT OF TANGENCY
BF	BACK FACE	POL	POINT ON LINE
BM	BENCHMARK	PE	PRIVATE ENTRANCE
C/L OR ℄	CENTERLINE	PL	PROPERTY LINE
Δ	CENTRAL ANGLE OR DELTA	PSI	POUNDS/SQUARE INCH
CLR	CLEAR	PROP	PROPOSED
CONC	CONCRETE	R	RADIUS
CONST	CONSTRUCTION	RR	RAILROAD
COR	CORNER	REBAR	REINFORCEMENT BAR
CMP	CORRUGATED METAL PIPE	REQ'D	REQUIRED
CTH	COUNTY TRUNK HIGHWAY	RT	RIGHT
CR	CREEK	RHF	RIGHT-HAND FORWARD
CFS	CUBIC FEET/SECOND	R/W	RIGHT-OF-WAY
CULV	CULVERT	RD	ROAD
D	DEGREE OF CURVE	SEC	SECTION
DHV	DESIGN HOUR VOLUME	S	SOUTH
DIA	DIAMETER	SE	SOUTHEAST
E	EAST	SW	SOUTHWEST
EL	ELEVATION	STH	STATE TRUNK HIGHWAY
EST	ESTIMATED	STA	STATION
FPS	FEET PER SECOND	SE	SUPER ELEVATION
FE	FIELD ENTRANCE	T	TANGENT
FT	FOOT (FEET)	TEL	TELEPHONE
FTG	FOOTING	TEMP	TEMPORARY
FDN	FOUNDATION	TI	TEMPORARY INTEREST
FF	FRONT FACE	TLE	TEMPORARY LIMITED EASEMENT
IP	IRON PIN	TL OR T/L	TRANSIT LINE
LT	LEFT	T	TRUCKS
LHF	LEFT-HAND FORWARD	TYP	TYPICAL
L	LENGTH OF CURVE	U/G	UNDERGROUND
LF	LINEAR FOOT	USH	UNITED STATES HIGHWAY
MAX	MAXIMUM	VAR	VARIABLE
MI	MILE	V	VELOCITY
MIN	MINIMUM	VPC	VERTICAL POINT OF CURVATURE
NC	NORMAL CROWN	VPI	VERTICAL POINT OF INTERSECTION
N	NORTH	VPT	VERTICAL POINT OF TANGENCY
NE	NORTHEAST	W	WEST
NW	NORTHWEST	YB	YARD
NO	NUMBER		

RUNOFF COEFFICIENT TABLE

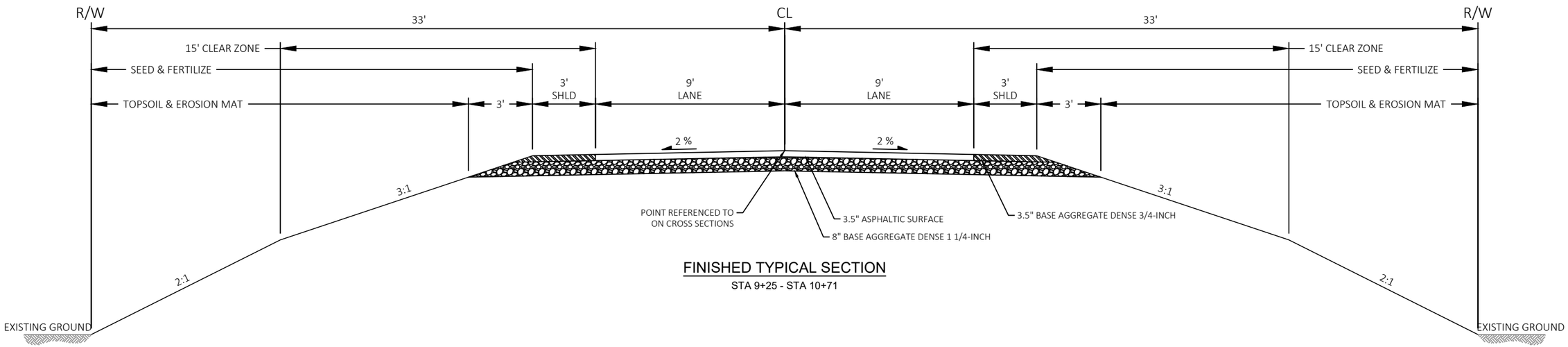
	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0 - 2	2 - 6	6 & OVER	0 - 2	2 - 6	6 & OVER	0 - 2	2 - 6	6 & OVER	0 - 2	2 - 6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAYMENT:												
ASPHALT:	.70 - .95											
CONCRETE:	.80 - .95											
BRICK:	.70 - .80											
DRIVES, WALKS:	.75 - .85											
ROOFS:	.75 - .95											
GRAVEL ROADS, SHOULDERS:	.40 - .60											

TOTAL PROJECT AREA = 0.20 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.14 ACRES



EXISTING TYPICAL SECTION

STA 9+25 - STA 10+71

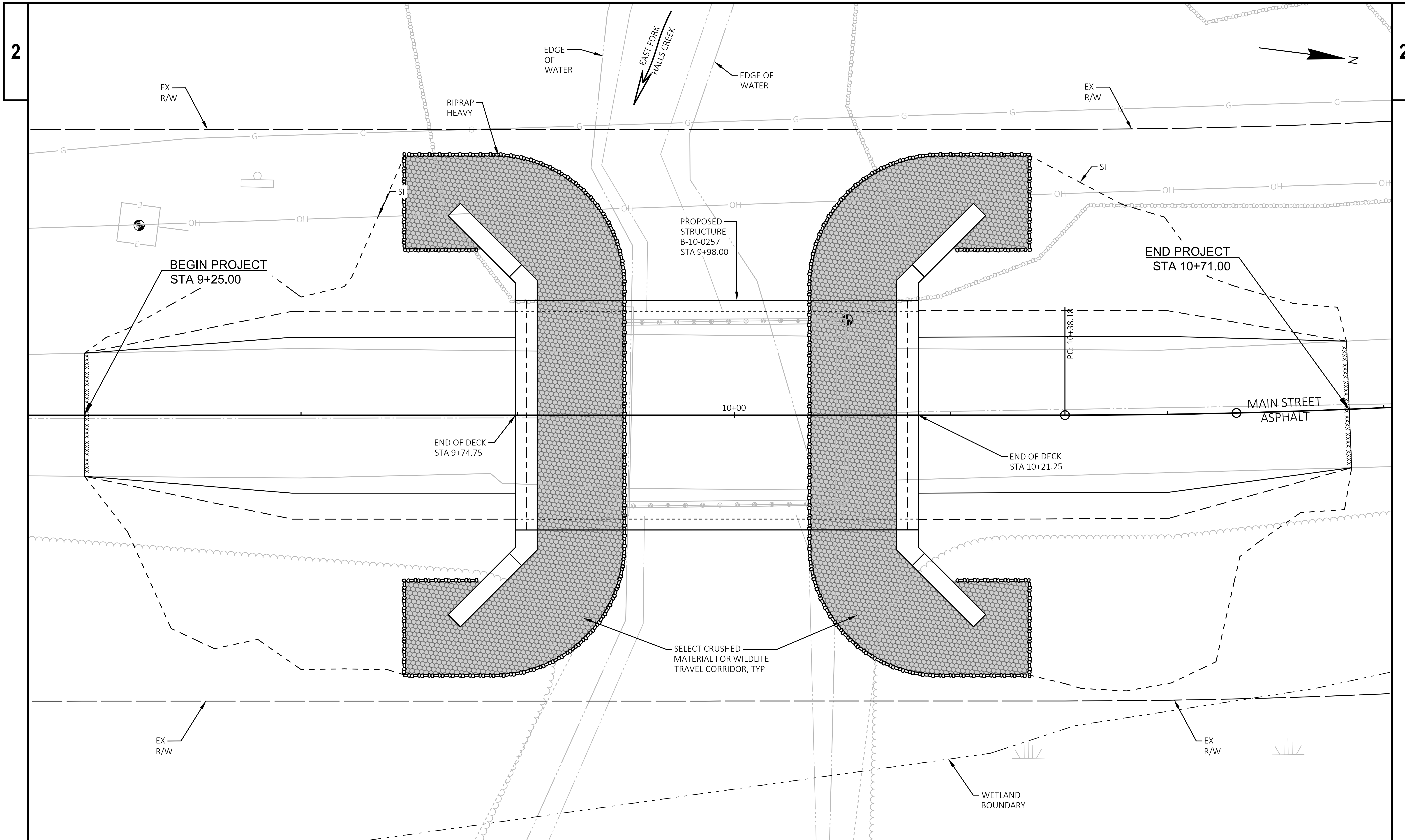


FINISHED TYPICAL SECTION

STA 9+25 - STA 10+71

PROJECT NO: 7852-00-71	HWY: MAIN STREET	COUNTY: CLARK	TYPICAL SECTION	SHEET 3	E

FILE NAME : I:\CLIENTS-MENO\W\W3900 WDOT NW REGION - EAU CLAIRE\031 7852-00-01 T MENTOR E FORK HALLS CREEK P-10-0196 LOCAL STREET CLARK COUNTY\04 CADD\WDOT CADD PROJECT NUMBER\SHEETSPLAN\020301-TS.DWG
LAYOUT NAME - 01-10ft
PLOT DATE : 10/7/2024 10:43 AM
PLOT BY : JORDAN DISTERHAFT
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42



PROJECT NO: 7852-00-71	HWY: MAIN STREET	COUNTY: CLARK	CONSTRUCTION DETAILS	SHEET 4	E

2

2

EDGE OF WATER

EAST FORK HALLS CREEK

EDGE OF WATER

N

EX R/W

RIPRAP HEAVY

EX R/W

SI

SI

PROPOSED STRUCTURE B-10-0257 STA 9+98.00

BEGIN PROJECT STA 9+25.00

END PROJECT STA 10+71.00

PC: 10+38.18

10+00

MAIN STREET ASPHALT

END OF DECK STA 9+74.75

END OF DECK STA 10+21.25

CREOSOTE CONTAINMENT AREA

EX R/W

EX R/W

WETLAND BOUNDARY

PROJECT NO: 7852-00-71	HWY: MAIN STREET	COUNTY: CLARK	CONSTRUCTION DETAILS	SHEET 5	E

FILE NAME : I:\CLIENTS-MENO\W\W3900 WDOT NW REGION - EAU CLAIRE\031 7852-00-01 T MENTOR E FORK HALLS CREEK P-10-0196 LOCAL STREET CLARK COUNTY\04 CADD\WDOT CADD PROJECT NOTE SHEETS\PLAN\021002-CD.DWG

LAYOUT NAME - 021002-cd

PLOT BY : JORDAN DISTERHAFT PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 42

2

LEGEND

- SURFACE WATER FLOW
- SLOPE INTERCEPT
- SILT FENCE
- TURBIDITY BARRIER
- RIPRAP BOUNDARY
- SEED, FERTILIZER, TOPSOIL, AND EROSION URBAN MAT CLASS 1, TYPE B
- CLEARING AND GRUBBING
- EXISTING WETLANDS

N

EDGE OF WATER

EDGE OF WATER

INSTALL TURBIDITY BARRIER

PROPOSED STRUCTURE B-10-0257 STA 9+98.00

INSTALL SILT FENCE

EX R/W

EX R/W

PRC: 8+48.59

PT: 9+13.28

PC: 10+39.18

PT: 10+77.76

9+00

10+00

11+00

MAIN STREET ASPHALT

END OF DECK STA 9+74.75

END OF DECK STA 10+21.25

BEGIN PROJECT STA 9+25.00

END PROJECT STA 10+71.00

EX R/W

EX R/W

EXISTING STRUCTURE P-10-0196 TO BE REMOVED

EDGE OF WATER

EAST FORK HALLS CREEK

EDGE OF WATER

NOTES:

1. USE SEED MIX NO. 20
2. FERTILIZER SHALL NOT BE PLACED WITHIN 20 FEET OF A WATERWAY OR WETLAND.
3. SILT FENCE IN WETLAND TO BE INSTALLED BY HAND.

PROJECT NO: 7852-00-71	HWY: MAIN STREET	COUNTY: CLARK	EROSION CONTROL	SHEET 6	E

FILE NAME : I:\CLIENTS-MENO\W\W3900 WDOT NW REGION - EAU CLAIRE\031 7852-00-01 T MENTOR E FORK HALLS CREEK P-10-0196 LOCAL STREET CLARK COUNTY\04 CADD\WDOT CADD PROJECT NUMBER\SHEETSPLAN\022001-EC.DWG
LAYOUT NAME - 022001-ec
PLOT DATE : 10/7/2024 9:02 AM PLOT BY : JORDAN DISTERHAFT PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 42

Estimate Of Quantities

7852-00-71

Line	Item	Item Description	Unit	Total	Qty
0002	201.0205	Grubbing	STA	2.000	2.000
0004	203.0260	Removing Structure Over Waterway Minimal Debris (structure) 01. P-10-0196	EACH	1.000	1.000
0006	205.0100	Excavation Common	CY	84.000	84.000
0008	205.0505.S	Excavation, Hauling, and Disposal of Creosote Contaminated Soil and Management of Contaminated Groundwater	TON	300.000	300.000
0010	206.1001	Excavation for Structures Bridges (structure) 01. B-10-0257	EACH	1.000	1.000
0012	210.1100	Backfill Structure Type A	CY	640.000	640.000
0014	213.0100	Finishing Roadway (project) 01. 7852-00-71	EACH	1.000	1.000
0016	305.0110	Base Aggregate Dense 3/4-Inch	TON	10.000	10.000
0018	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	107.000	107.000
0020	465.0105	Asphaltic Surface	TON	37.000	37.000
0022	502.0100	Concrete Masonry Bridges	CY	190.000	190.000
0024	502.3200	Protective Surface Treatment	SY	205.000	205.000
0026	505.0400	Bar Steel Reinforcement HS Structures	LB	3,960.000	3,960.000
0028	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	20,560.000	20,560.000
0030	513.4061	Railing Tubular Type M	LF	93.000	93.000
0032	516.0500	Rubberized Membrane Waterproofing	SY	20.000	20.000
0034	550.0500	Pile Points	EACH	14.000	14.000
0036	550.1100	Piling Steel HP 10-Inch X 42 Lb	LF	350.000	350.000
0038	606.0300	Riprap Heavy	CY	170.000	170.000
0040	612.0406	Pipe Underdrain Wrapped 6-Inch	LF	160.000	160.000
0042	616.0700.S	Fence Safety	LF	100.000	100.000
0044	618.0100	Maintenance and Repair of Haul Roads (project) 01. 7852-00-71	EACH	1.000	1.000
0046	619.1000	Mobilization	EACH	1.000	1.000
0048	624.0100	Water	MGAL	2.000	2.000
0050	625.0100	Topsoil	SY	256.000	256.000
0052	628.1504	Silt Fence	LF	319.000	319.000
0054	628.1520	Silt Fence Maintenance	LF	319.000	319.000
0056	628.1905	Mobilizations Erosion Control	EACH	3.000	3.000
0058	628.1910	Mobilizations Emergency Erosion Control	EACH	3.000	3.000
0060	628.2008	Erosion Mat Urban Class I Type B	SY	256.000	256.000
0062	628.6005	Turbidity Barriers	SY	69.000	69.000
0064	629.0210	Fertilizer Type B	CWT	0.200	0.200
0066	630.0120	Seeding Mixture No. 20	LB	5.000	5.000
0068	630.0500	Seed Water	MGAL	3.000	3.000
0070	634.0612	Posts Wood 4x6-Inch X 12-FT	EACH	4.000	4.000
0072	637.2230	Signs Type II Reflective F	SF	12.000	12.000
0074	638.2602	Removing Signs Type II	EACH	4.000	4.000
0076	638.3000	Removing Small Sign Supports	EACH	6.000	6.000
0078	642.5001	Field Office Type B	EACH	1.000	1.000
0080	643.0420	Traffic Control Barricades Type III	DAY	480.000	480.000
0082	643.0705	Traffic Control Warning Lights Type A	DAY	960.000	960.000
0084	643.0900	Traffic Control Signs	DAY	720.000	720.000
0086	643.1000	Traffic Control Signs Fixed Message	SF	36.000	36.000
0088	643.5000	Traffic Control	EACH	1.000	1.000
0090	645.0111	Geotextile Type DF Schedule A	SY	60.000	60.000
0092	645.0120	Geotextile Type HR	SY	310.000	310.000
0094	650.4500	Construction Staking Subgrade	LF	100.000	100.000
0096	650.5000	Construction Staking Base	LF	100.000	100.000

7852-00-71

Line	Item	Item Description	Unit	Total	Qty
0098	650.6501	Construction Staking Structure Layout (structure) 01. B-10-0257	EACH	1.000	1.000
0100	650.9911	Construction Staking Supplemental Control (project) 01. 7852-00-71	EACH	1.000	1.000
0102	650.9920	Construction Staking Slope Stakes	LF	100.000	100.000
0104	690.0150	Sawing Asphalt	LF	30.000	30.000
0106	715.0502	Incentive Strength Concrete Structures	DOL	1,134.000	1,134.000
0108	999.2005.S	Maintaining Bird Deterrent System (station) 01. 9+98	EACH	1.000	1.000
0110	ASP.1T0A	On-the-Job Training Apprentice at $5.00/HR	HRS	300.000	300.000
0112	ASP.1T0G	On-the-Job Training Graduate at $5.00/HR	HRS	300.000	300.000
0114	SPV.0090	Special 01. Flashing Stainless Steel	LF	83.000	83.000
0116	SPV.0195	Special 01. Select Crushed Material for Wildlife Travel Corridor	TON	80.000	80.000

DIVISION	FROM/TO STATION	LOCATION	205.0100 COMMON EXCAVATION CUT (2)	AVAILABLE MATERIAL (5)	UNEXPANDED FILL	EXPANDED FILL (13) FACTOR 1.25	MASS ORDINATE +/- (14)	WASTE	208.0100 BORROW	COMMENT
DIVISION 1										
MAIN STREET	9+25 - 10+71	MAIN STREET	84	84	76	95	-11		11	
DIVISION 1 SUBTOTAL			84	84	76	95	-11			
GRAND TOTAL			84	84	76	95	-11	0	11	
TOTAL COMMON EXC			84							

NOTES:

(1) COMMON EXCAVATION IS THE SUM OF THE CUT AND EBS EXCAVATION COLUMNS. ITEM NUMBER 205.0100

5) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSUABLE PAVEMENT MATERIAL

(13) EXPANDED FILL FACTOR = 1.25

DEPENDING ON SELECTIONS: EXPANDED FILL = (UNEXPANDED FILL - EXPANDED ROCK - REDUCED MARSH - REDUCED EBS) * FILL FACTOR

OR EXPANDED FILL = (UNEXPANDED FILL - EXPANDED ROCK - REDUCED EBS) * FILL FACTOR

OR EXPANDED FILL = (UNEXPANDED FILL - EXPANDED ROCK - REDUCED MARSH) * FILL FACTOR

OR EXPANDED FILL = (UNEXPANDED FILL - EXPANDED ROCK) * FILL FACTOR

(14) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.

(15) FACTORS USED TO COMPUTE ANTICIPATED WASTE AND THE COMPUTED WASTE VOLUME IDENTIFIED ARE FOR GENERAL INFORMATION ONLY.

3

3

GRUBBING

CATEGORY	STATION	TO	STATION	LOCATION	201.0205 GRUBBING STA	REMARKS
0010	9+29	-	9+79	MAIN STREET	1	
0010	10+16	-	10+66	MAIN STREET	1	
				TOTAL 0010	2	

HAZ MAT

CATEGORY	STATION	TO	STATION	LOCATION	205.0505.S EXCAVATION, HAULING, AND DISPOSAL OF CREOSOTE CONTAMINATED SOIL AND MANAGEMENT OF CONTAMINATED GROUNDWATER TON	616.0700.S FENCE SAFETY LF	REMARKS
0010	9+75	-	10+25	EXISTING STRUCTURE	300	-	
0010	10+90	-	11+82	MAIN STREET	-	100	JAPANESE GIANT KNOTWEED
				TOTAL 0010	300	100	

AGGREGATE

CATEGORY	STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	624.0100 WATER MGAL	REMARKS
0010	9+25	-	9+74.75	MAIN STREET	5	53	1	
0010	10+21.25	-	10+71	MAIN STREET	5	54	1	
				TOTAL 0010	10	107	2	

HMA

CATEGORY	STATION	TO	STATION	LOCATION	465.0105 ASPHALTIC SURFACE TON	REMARKS
0010	9+25	-	9+74.75	MAIN STREET	18	
0010	10+21.25	-	10+71	MAIN STREET	19	
				TOTAL 0010	37	

HAUL ROADS

CATEGORY	LOCATION	618.0100.01 MAINTENANCE AND REPAIR OF HAUL ROADS (PROJECT) (01. 7852-00-71) EACH	REMARKS
0030	PROJECT	1	
	TOTAL 0030	1	

MISCELLANEOUS

CATEGORY	STATION	TO	STATION	LOCATION	213.0100.01 FINISHING ROADWAY (PROJECT) (01. 7852-00-71) EACH	619.1000 MOBILIZATION EACH	642.5001 FIELD OFFICE TYPE B EACH	999.2005.S.01 MAINTAINING BIRD DETERRENT SYSTEM (STATION) (01. 9+98) EACH	REMARKS
0010	9+25	-	10+71	MAIN STREET	1	1	1	1	
				TOTAL 0010	1	1	1	1	

RESTORATION

CATEGORY	STATION	TO	STATION	LOCATION	625.0100 TOPSOIL SY	628.2008 EROSION MAT URBAN CLASS I TYPE B SY	629.0210 FERTILIZER TYPE B CWT	630.0120 SEEDING MIXTURE NO. 20 LB	630.0500 SEED WATER MGAL	REMARKS
0010	9+25	-	9+74.75	MAIN STREET	95	95	0.1	2	1	
0010	10+21.25	-	10+71	MAIN STREET	110	110	0.1	2	1	
0010	9+25	-	10+71	MAIN STREET	51	51	0	1	1	UNDISTRIBUTED QUANTITY (25%)
				TOTAL 0010	256	256	0.2	5	3	

EROSION CONROL

CATEGORY	STATION	TO	STATION	LOCATION	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH	628.6005 TURBIDITY BARRIERS SY	REMARKS
0010	9+25	-	9+74.75	MAIN STREET	120	120	3	3	37	
0010	10+21.25	-	10+71	MAIN STREET	135	135	-	-	32	
0010	9+25	-	10+71	MAIN STREET	64	64	-	-	-	UNDISTRIBUTED QUANTITY (25%)
				TOTAL 0010	319	319	3	3	69	

TYPE II SIGNING

CATEGORY	STATION	SIDE	SIGN CODE	WXH	LOCATION	634.0612 POSTS WOOD 4X6-INCH X 12-FT EACH	637.2230 SIGNS TYPE II REFLECTIVE F SF	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	REMARKS
0010	9+74.75	LT	W5-52L	12X36	MAIN STREET	1	3	1	1	BRIDGE HASH MARKS
0010	9+74.75	RT	W5-52L	12X36	MAIN STREET	1	3	1	2	BRIDGE HASH MARKS & WEIGHT LIMIT 5 TONS
0010	10+21.25	LT	W5-52L	12X36	MAIN STREET	1	3	1	2	BRIDGE HASH MARKS & WEIGHT LIMIT 5 TONS
0010	10+21.25	RT	W5-52L	12X36	MAIN STREET	1	3	1	1	BRIDGE HASH MARKS
					TOTAL 0010	4	12	4	6	

TRAFFIC CONTROL

CATEGORY	LOCATION	DAYS	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAY	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	643.0900 TRAFFIC CONTROL SIGNS DAY	643.1000 TRAFFIC CONTROL SIGNS FIXED MESSAGE SF	643.5000 TRAFFIC CONTROL EACH	REMARKS
0010	MAIN STREET	7	-	-	-	36	-	7-DAY ADVANCED WARNING (G20-57C)
0010	MAIN STREET	60	480	960	720	-	1	
		TOTAL 0010	480	960	720	36	1	

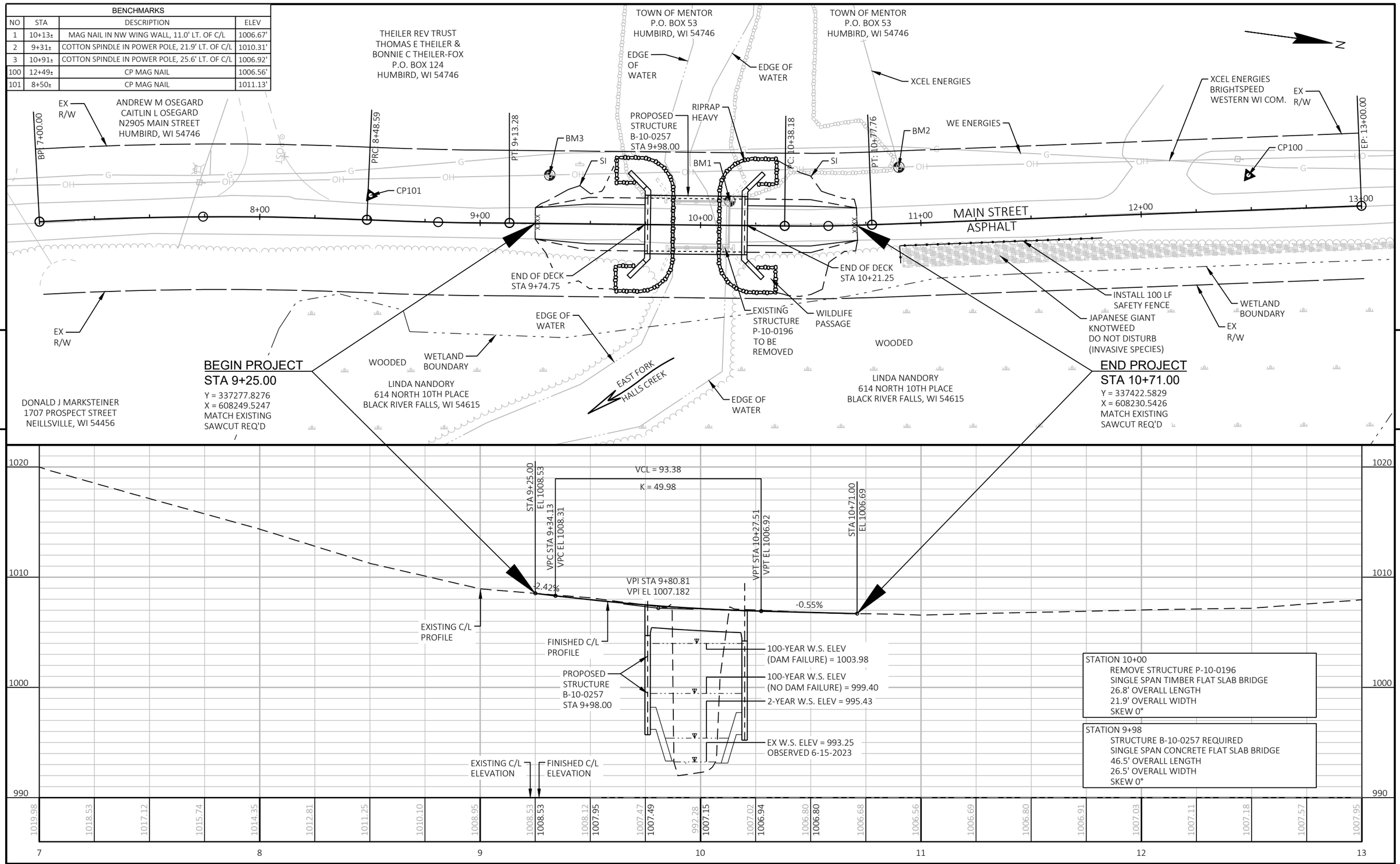
STAKING

CATEGORY	STATION	TO	STATION	LOCATION	650.4500 CONSTRUCTION STAKING SUBGRADE LF	650.5000 CONSTRUCTION STAKING BASE LF	650.6501.01 CONSTRUCTION STAKING STRUCTURE LAYOUT (STRUCTURE) (01. B-10-0257) EACH	650.9911.01 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (01. 7852-00-71) EACH	650.9920 CONSTRUCTION STAKING SLOPE STAKES LF	REMARKS
0010	9+25	-	9+74.75	MAIN STREET	50	50	1	1	50	
0010	10+21.25	-	10+71	MAIN STREET	50	50	-	-	50	
				TOTAL 0010	100	100	1	1	100	

SAWING

CATEGORY	STATION	LOCATION	690.0150 SAWING ASPHALT LF	REMARKS
0010	9+25	MAIN STREET	15	
0010	10+71	MAIN STREET	15	
		TOTAL 0010	30	

BENCHMARKS			
NO	STA	DESCRIPTION	ELEV
1	10+13±	MAG NAIL IN NW WING WALL, 11.0' LT. OF C/L	1006.67'
2	9+31±	COTTON SPINDLE IN POWER POLE, 21.9' LT. OF C/L	1010.31'
3	10+91±	COTTON SPINDLE IN POWER POLE, 25.6' LT. OF C/L	1006.92'
100	12+49±	CP MAG NAIL	1006.56'
101	8+50±	CP MAG NAIL	1011.13'

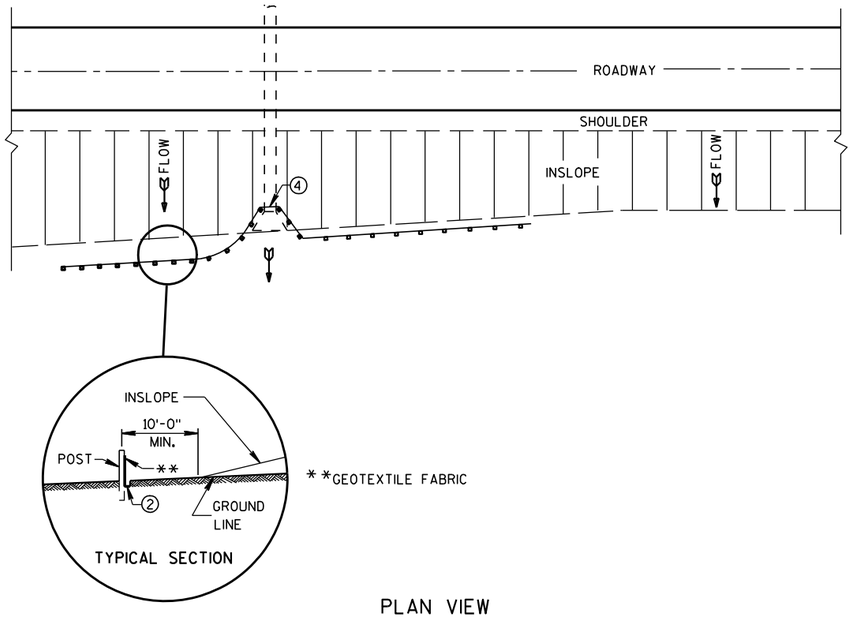


PROJECT NO: 7852-00-71	HWY: MAIN STREET	COUNTY: CLARK	PLAN AND PROFILE: MAIN STREET	SHEET 13	E

FILE NAME : I:\CLIENTS-MENO\W\W3900 WDOT NW REGION - EAU CLAIRE\031 7852-00-01 T MENTOR E FORK HALLS CREEK P-10-0196 LOCAL STREET CLARK COUNTY\04 CADD\WDOT CADD PROJECT NUMBER\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - 050101-pp
PLOT DATE : 1/7/2025 10:47 AM
PLOT BY : JORDAN DISTERHAFT
PLOT NAME :
PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 44

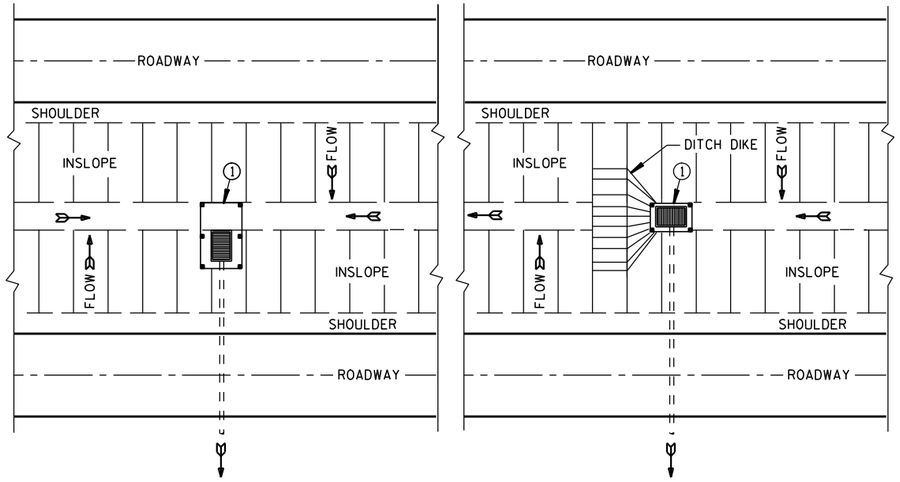
Standard Detail Drawing List

08E09-06	SILT FENCE
08E11-02	TURBIDITY BARRIER
12A03-10	NAME PLATE (STRUCTURES)
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS
15C06-12	SIGNING & MARKING FOR TWO LANE BRIDGES



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



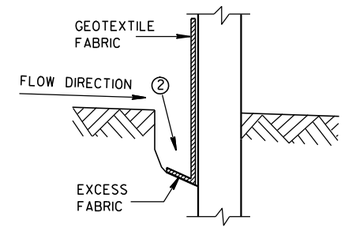
PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

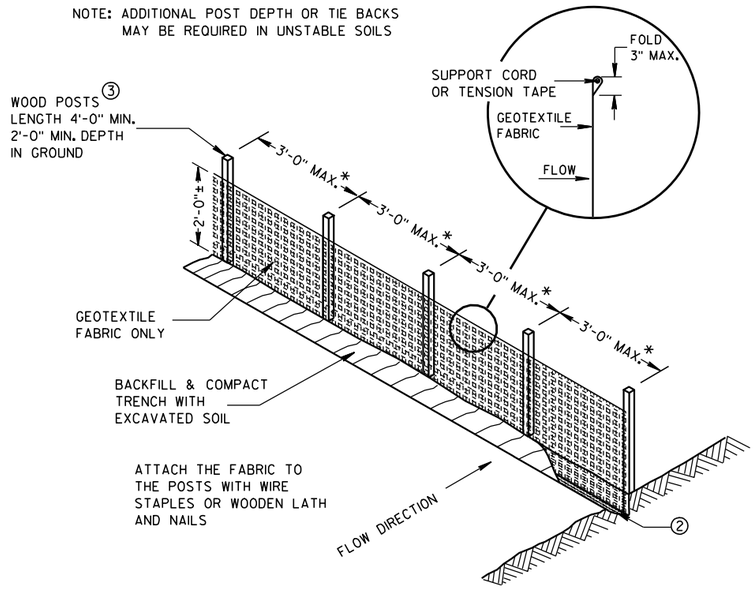
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

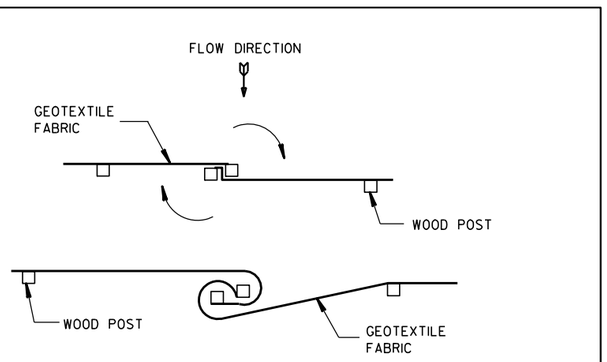
1. HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
2. FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
3. WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.
4. SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
5. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



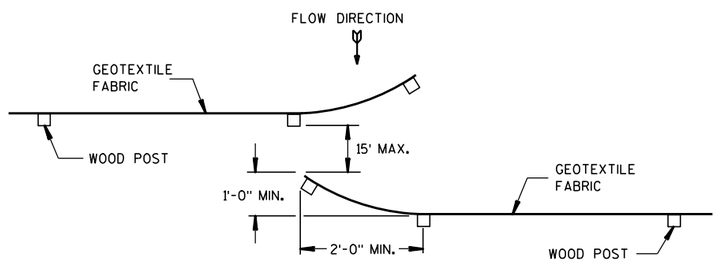
TRENCH DETAIL



SILT FENCE

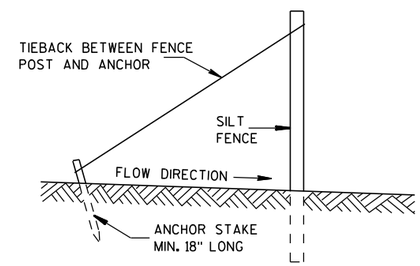


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE 5



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

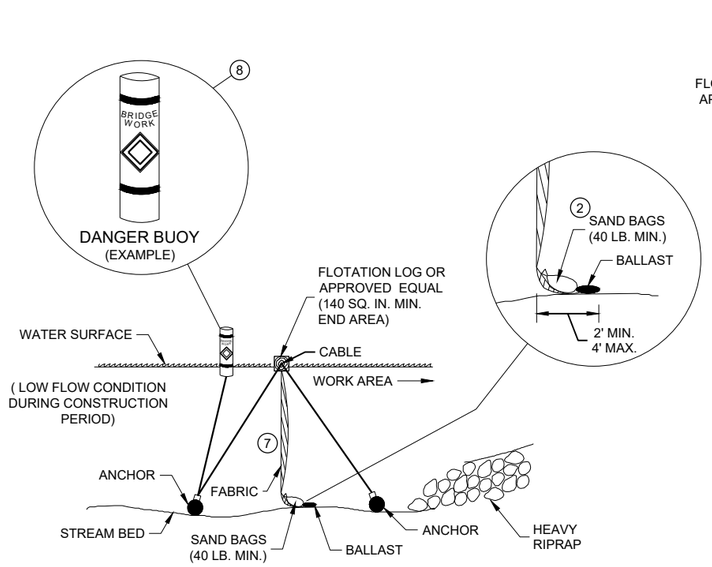
APPROVED
4-29-05 DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6

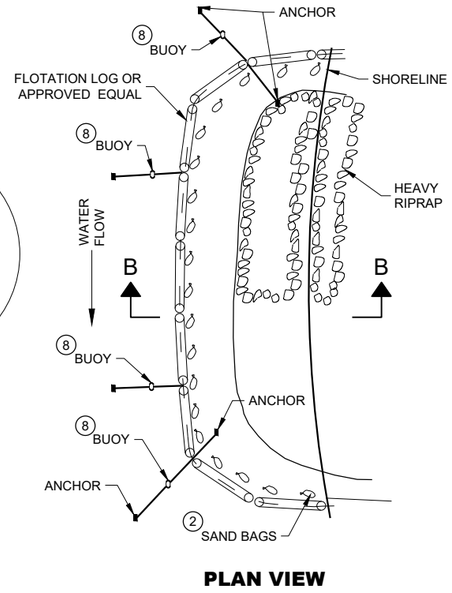
S.D.D. 8 E 9-6

6

S.D.D. 8 E 9-6

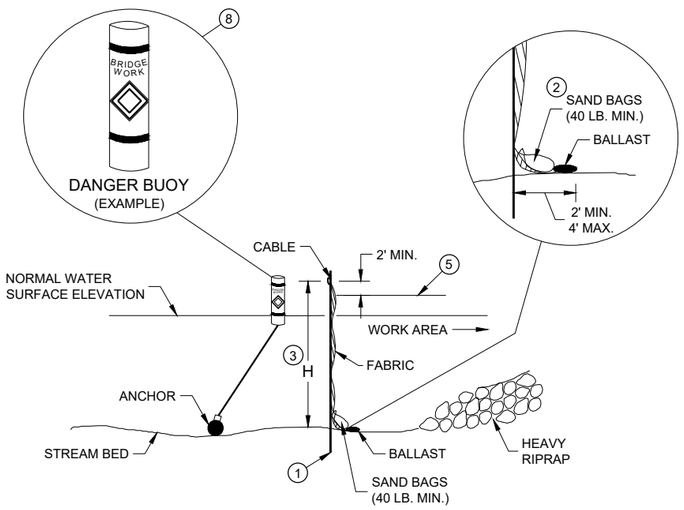


SECTION B - B

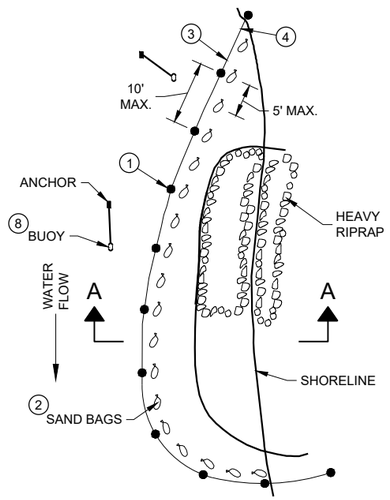


PLAN VIEW

TURBIDITY BARRIER - FLOAT ALTERNATIVE
CAUTION - SEE NOTE 6



SECTION A - A



PLAN VIEW

TURBIDITY BARRIER - STANDARD POST INSTALLATION

TURBIDITY BARRIER PLACEMENT DETAILS

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TURBIDITY BARRIER MAY BE REMOVED AT THE ENGINEERS DISCRETION, WHEN PERMANENT EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

(1) DRIVEN STEEL POSTS, PIPES, OR CHANNELS. LENGTH SHALL BE SUFFICIENT TO SECURELY SUPPORT BARRIER AT HIGH WATER ELEVATIONS.

(2) SAND BAGS TO BE USED AS ADDITIONAL BALLAST WHEN ORDERED BY THE ENGINEER TO MEET ADVERSE FIELD CONDITIONS. SPACE AS APPROPRIATE FOR SITE CONDITIONS.

(3) WHEN BARRIER HEIGHT "H" EXCEEDS 8 FEET, POST SPACING MAY NEED TO BE DECREASED.

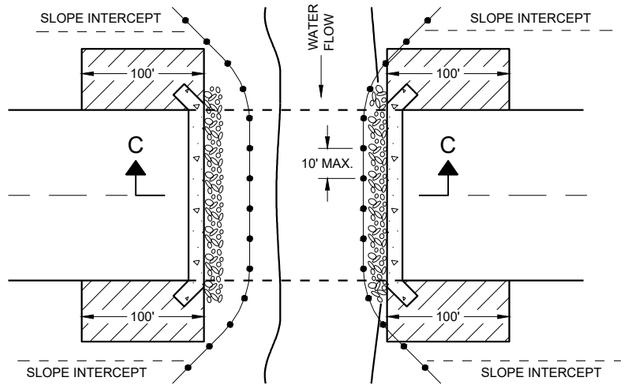
(4) IN WATERWAYS SUBJECT TO FLUCTUATING WATER ELEVATIONS, PROVISIONS SHOULD BE MADE TO ALLOW THE WATER TO EQUALIZE ON EACH SIDE OF THE BARRIER. THIS MAY BE ACCOMPLISHED BY LEAVING A PORTION OF THE BARRIER OPEN ON THE UPSTREAM END.

(5) ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION PERIOD. MINIMUM BARRIER HEIGHT SHALL BE 2' GREATER THAN EITHER THE Q2 ELEVATION OR THE ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION, WHICHEVER IS GREATER.

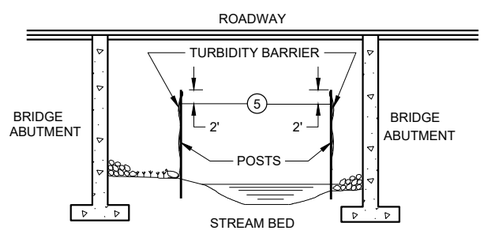
(6) FLOAT ALTERNATIVE WILL ONLY BE ALLOWED WITH WRITTEN APPROVAL OF THE ENGINEER, AND IS MEANT FOR LOCATIONS WHERE BEDROCK PREVENTS THE INSTALLATION OF POSTS.

(7) ALLOW SUFFICIENT SLACK VERTICALLY AND HORIZONTALLY SO THAT SEDIMENT BUILD UP WILL NOT SEPARATE OR LOWER THE TURBIDITY BARRIER.

(8) USE AS DIRECTED BY COAST GUARD OR DNR PERMIT WHEN WORKING IN NAVIGABLE WATERWAYS.



PLAN VIEW



SECTION C - C

TURBIDITY BARRIER DETAIL SHOWING TYPICAL PLACEMENT AT STRUCTURES

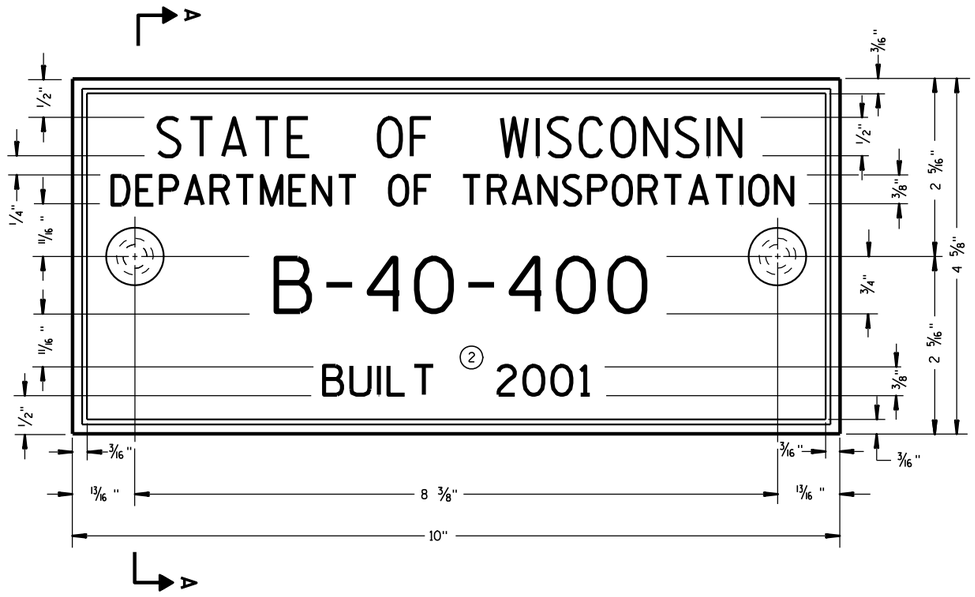
TURBIDITY BARRIER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

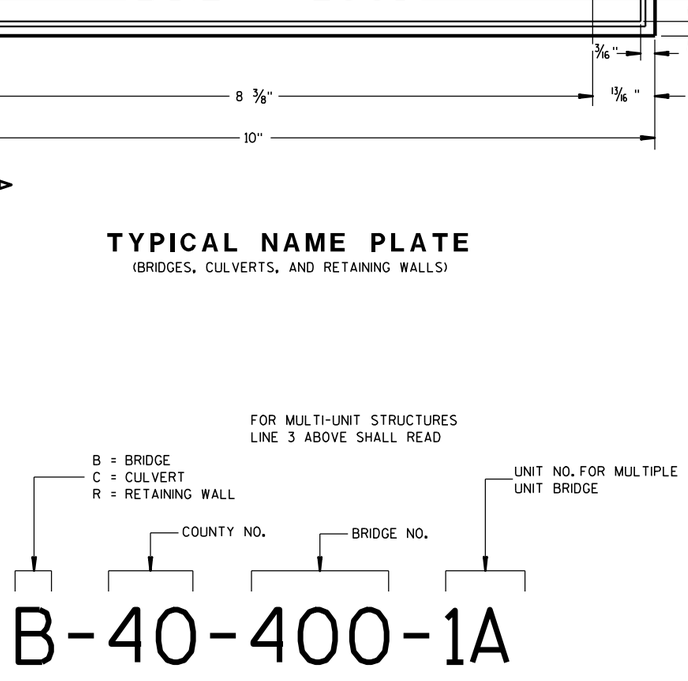
APPROVED
6/4/02
DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA

6
SDD 08E11 - 02



TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)



NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES

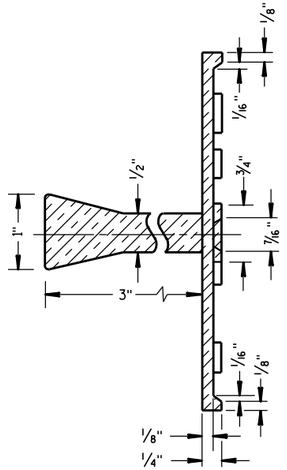
GENERAL NOTES

NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

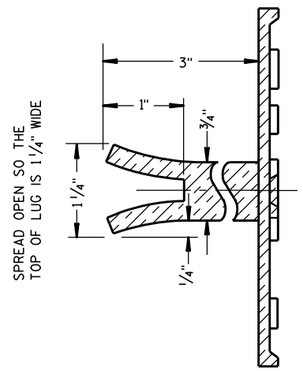
THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

(1) EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.

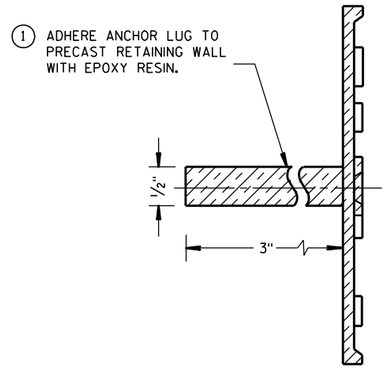
(2) REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.



SECTION A-A



ALTERNATE LUG



ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

NAME PLATE (STRUCTURES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

3/26/10 DATE

/S/ Scot Beck -- CHIEF STRUCTURAL DEVELOPMENT ENGINEER

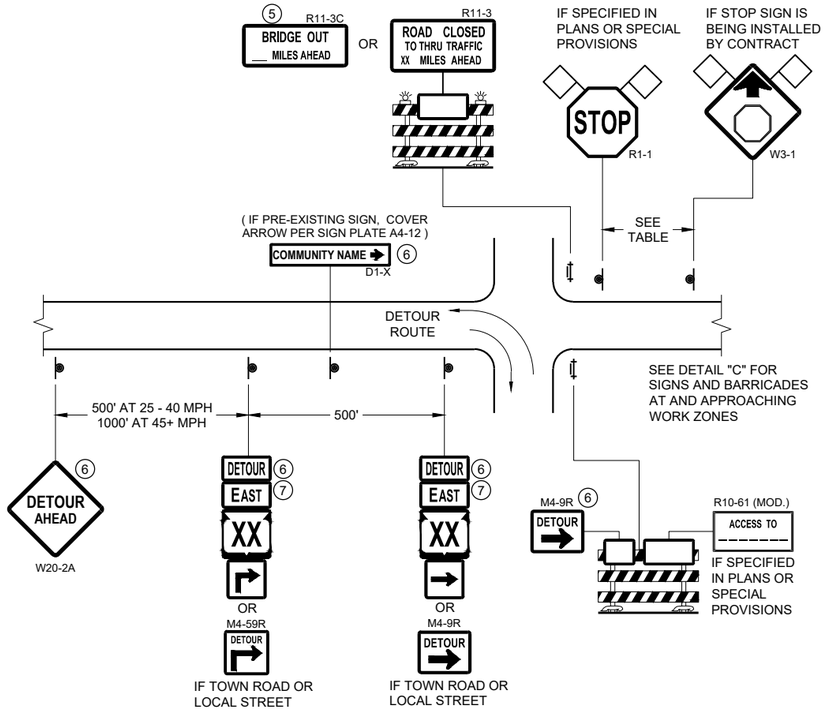
FHWA

6

S.D.D. 12 A 3-10

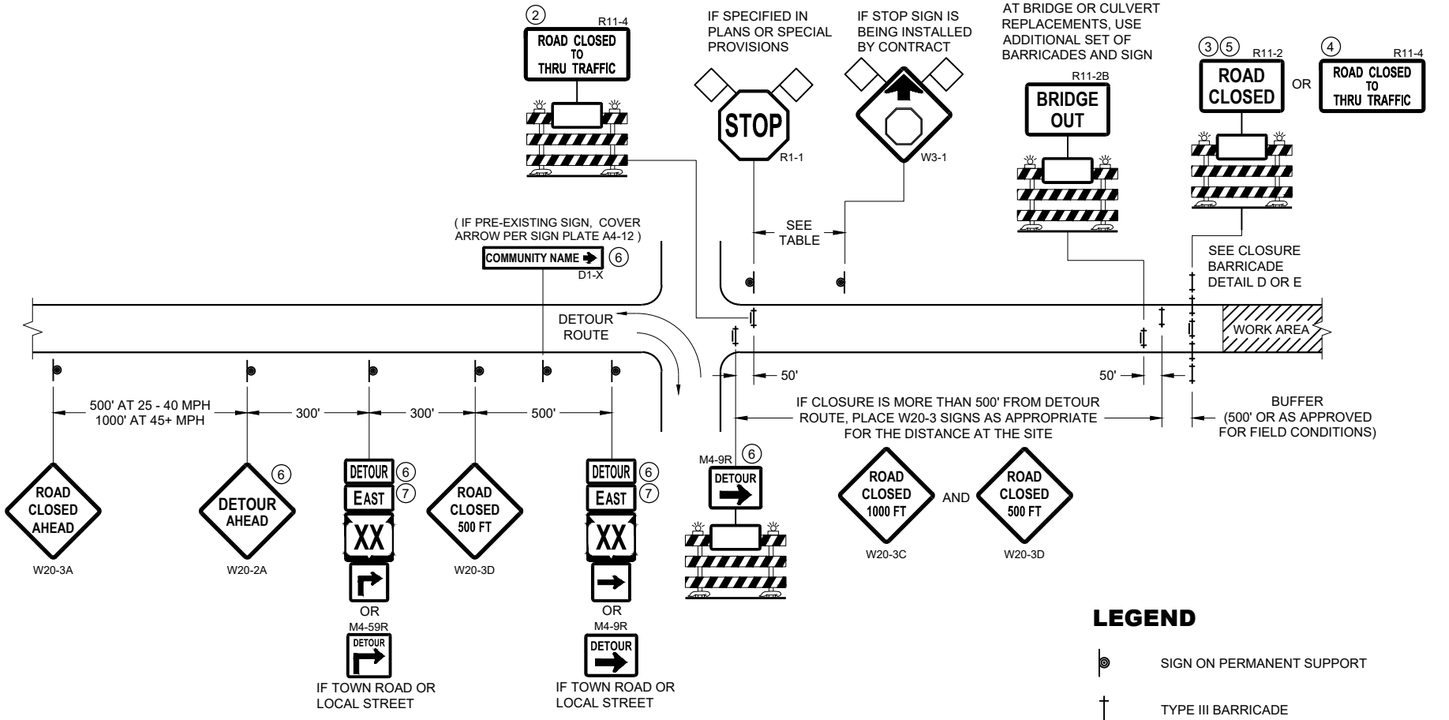
6

S.D.D. 12 A 3-10



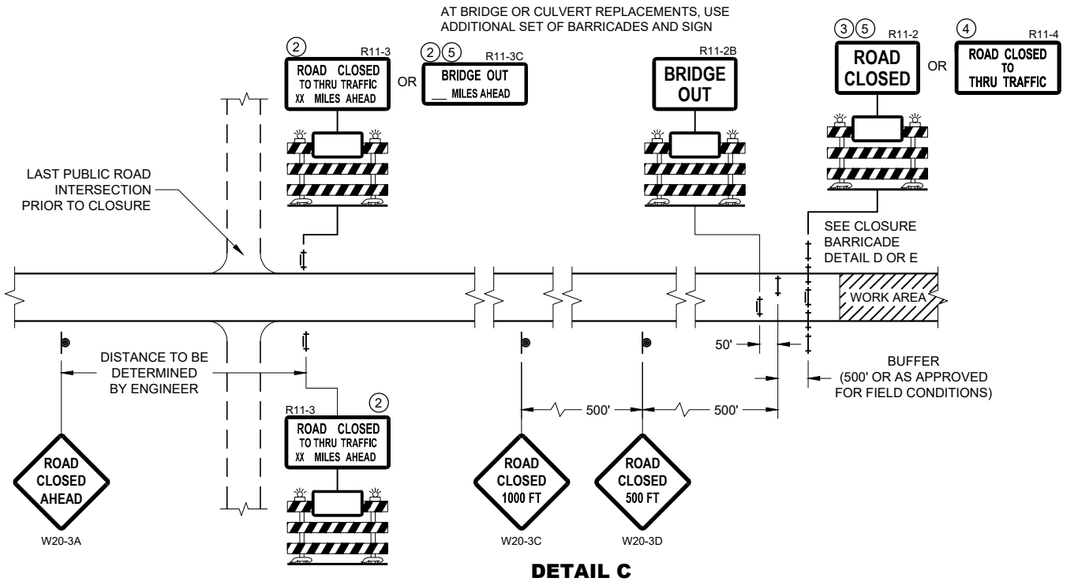
DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN OR EQUAL TO ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)
- DETOUR — M4 - 8
- EAST — M3 - X
- XX — M1 - 4 OR XX — M1 - 6 OR COUNTY X — M1 - 5A
- M05 - 1 OR M06 - 1

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

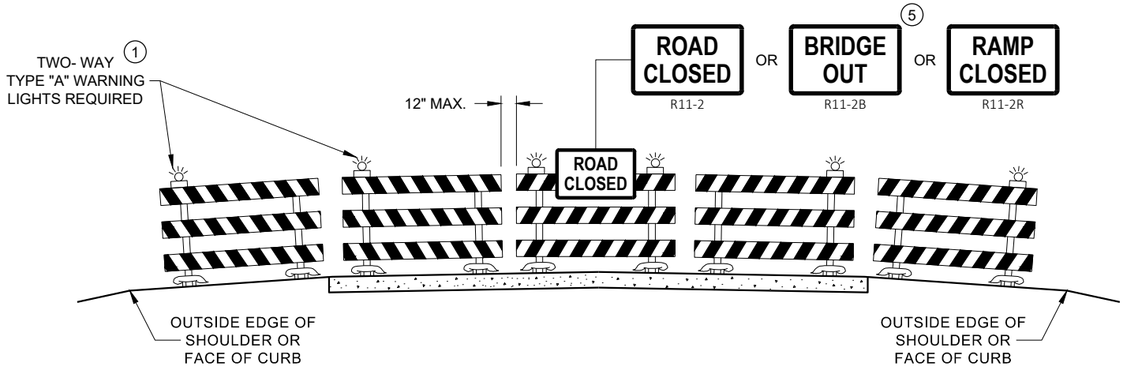
APPROVED
May 2023
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

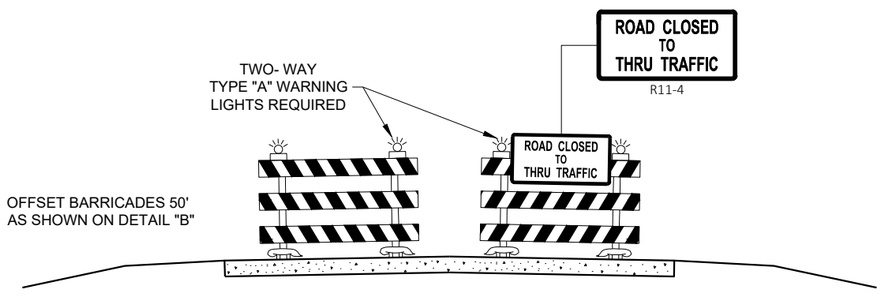
6

SDD 15C02 - 09a

SDD 15C02 - 09a



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60 " X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING.

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR VARIOUS CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEE
FHWA

6

SDD 15C02 - 09b

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

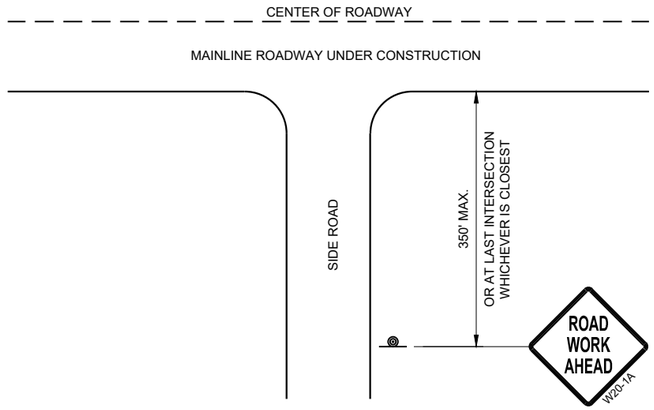
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

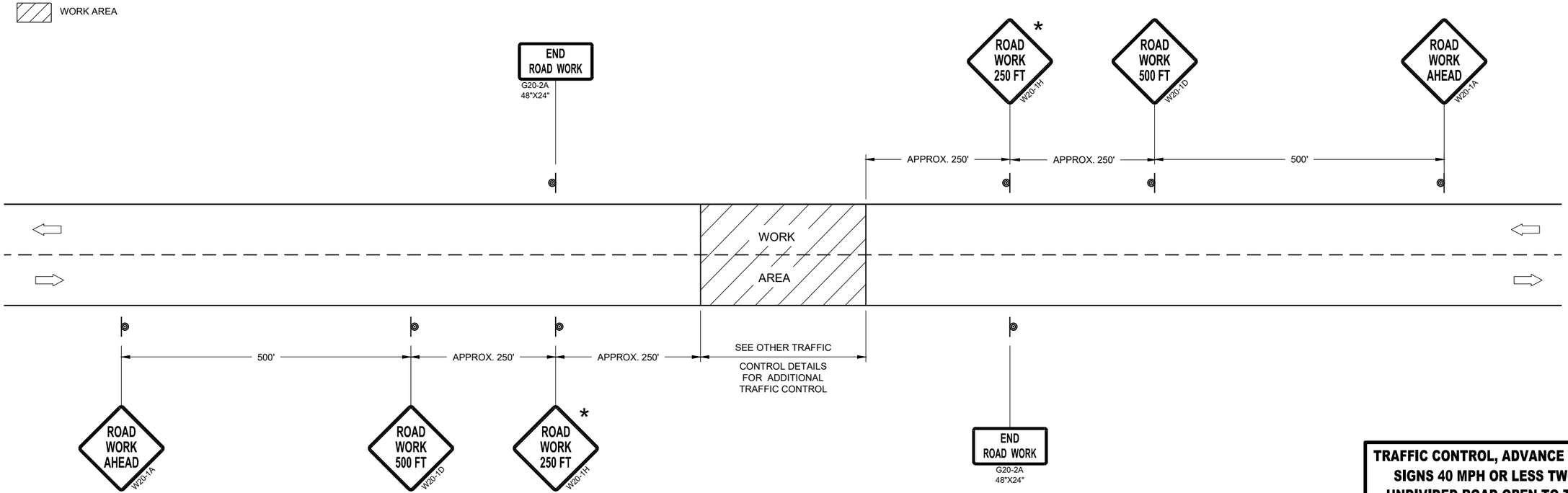
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL

LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 MPH OR LESS TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

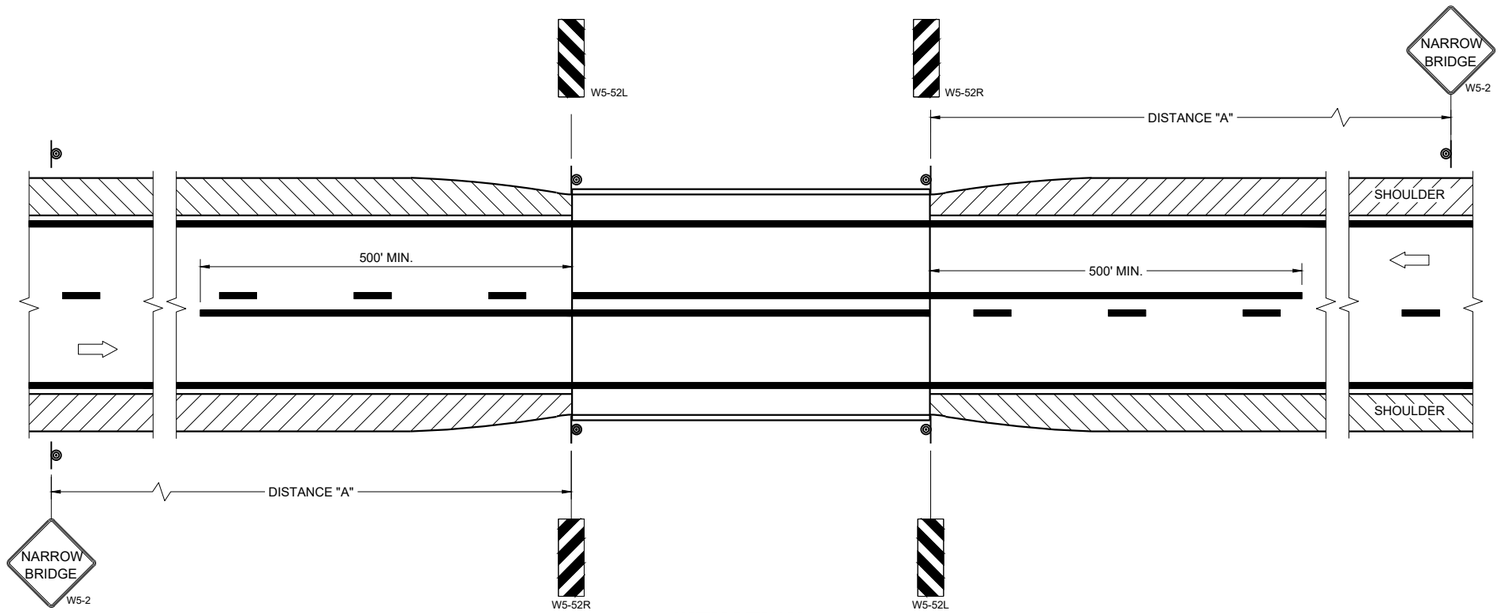
APPROVED
July 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

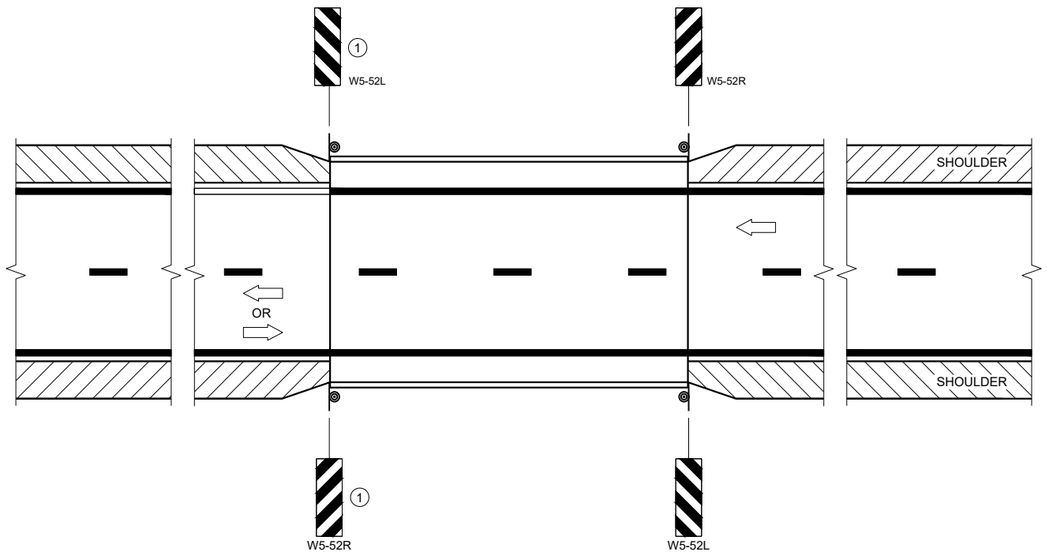
6

SDD 15C05 - 05



SITUATION 1

WARRANTING CRITERIA:
BRIDGE WIDTH IS AT LEAST 16 FEET BUT LESS THAN 24 FEET.



SITUATION 2

WARRANTING CRITERIA:
1. BRIDGE WIDTH IS AT LEAST 24 FEET AND
2. BRIDGE SHOULDER WIDTH IS LESS THAN 6 FEET

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THE DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

LOCATE W5-52 SIGN POST(S) BEHIND GUARDRAIL WHEN PRESENT.

PLACE THE EDGE OF THE W5-52 SIGN IN LINE WITH FACE OF CURB OR PARAPET.

ON BRIDGE ONLY PROJECTS, PLACE 300 FEET OF EDGELINE.

OMIT EDGELINES ON ROADWAYS WITHOUT EXISTING EDGELINES.

(1) OMIT ON ONE-WAY TRAVELED WAYS.

LEGEND

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC

DISTANCE TABLE

POSTED OR 85TH PERCENTILE SPEED	DISTANCE "A"
25	150'
30	200'
35	250'
40	300'
45	400'
50	550'
55	700'

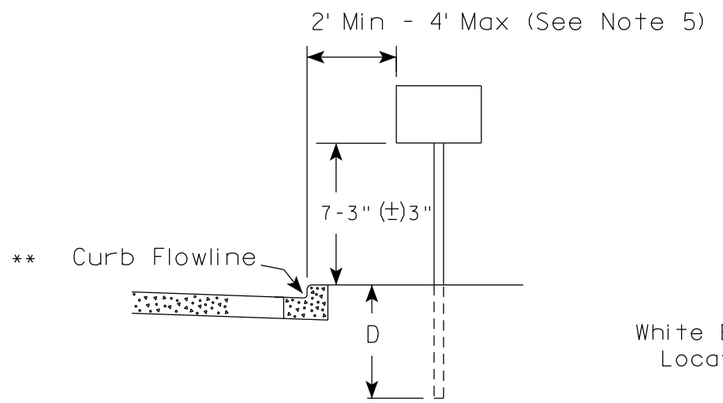
SIGNING AND MARKING FOR TWO LANE BRIDGES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

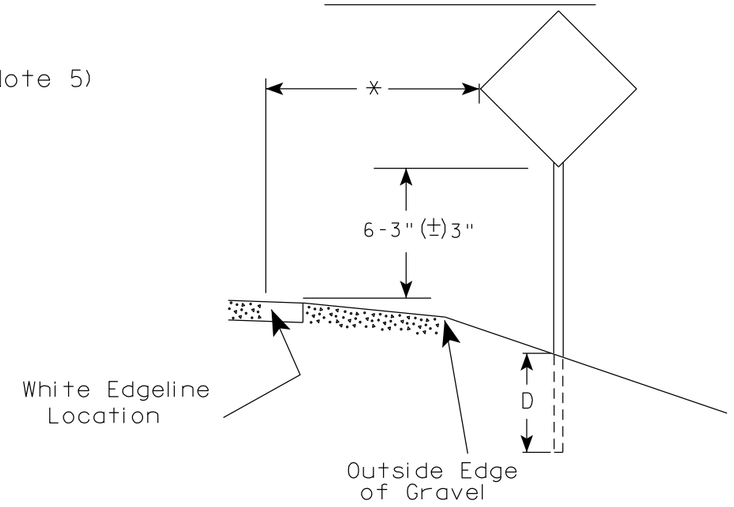
APPROVED
May 2023
DATE
/S/ Jeannie Silver
Statewide Pavement Marking Engineer
FHWA

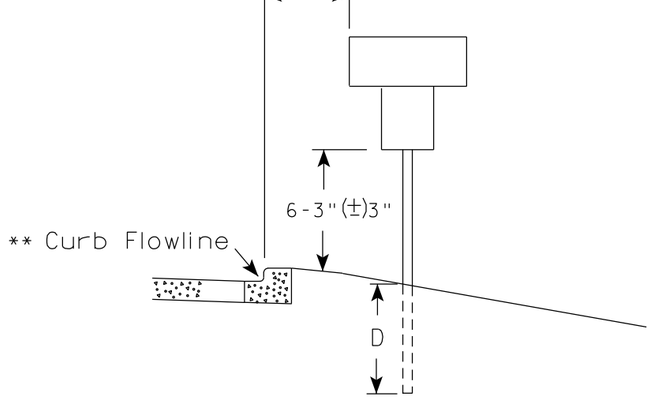
6
SDD 15C06-12

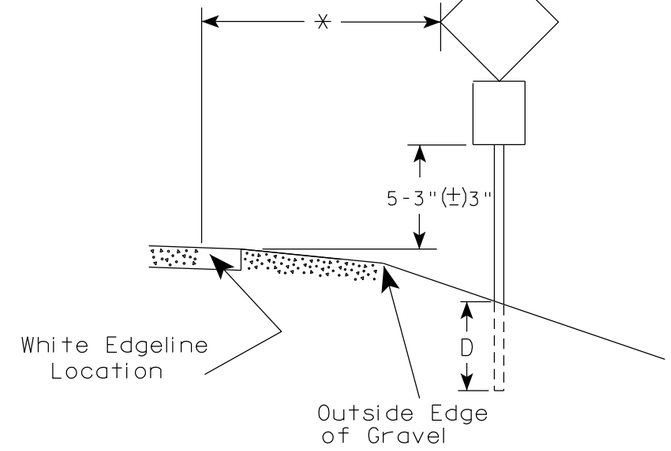
URBAN AREA



RURAL AREA (See Note 2)







** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
   The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".
3. For expressways and freeways, mounting height is 7'- 3" (±) 3" or 6'-3" (±) 3" depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±) 3".
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directd by the Engineer.

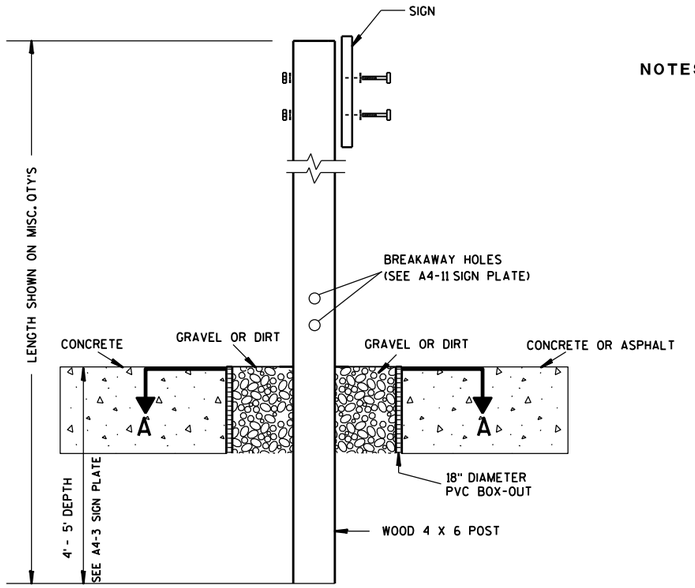
TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-3.23

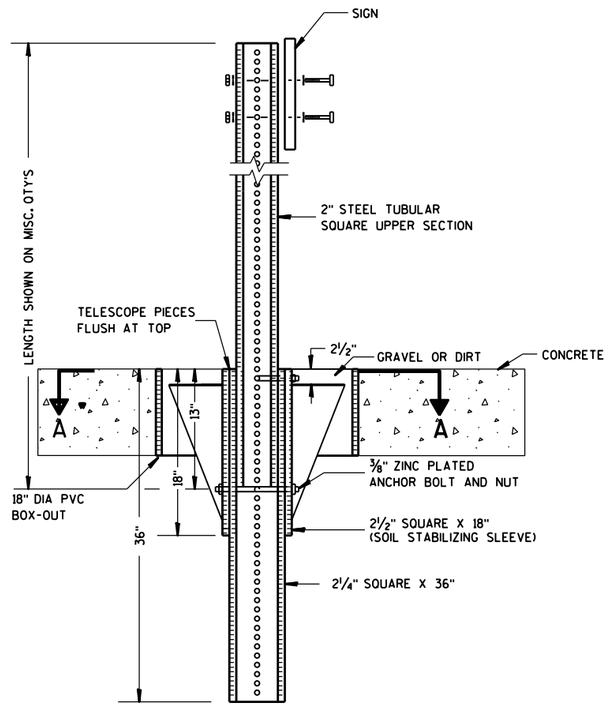
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 22	E



ELEVATION VIEW

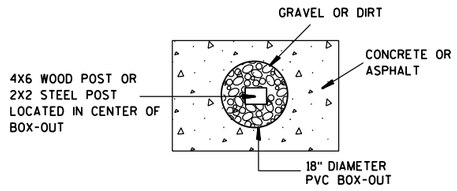
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

NOTES:
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

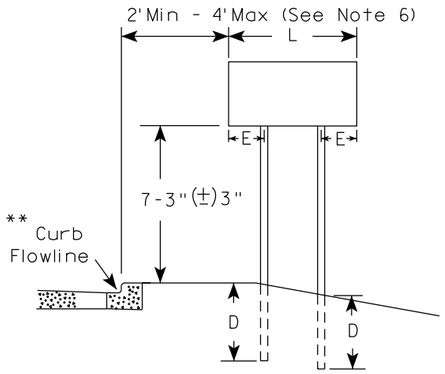
APPROVED Matthew R Rauch
for State Traffic Engineer

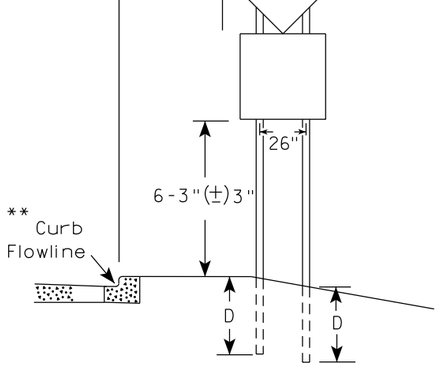
DATE 1/27/14 PLATE NO. A4-3B.1

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN PLOT DATE : 27-JAN-2014 09:48 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42

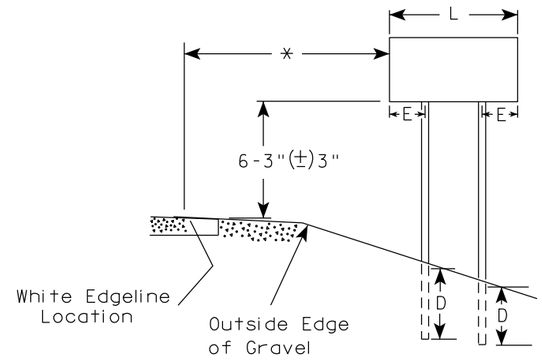
URBAN AREA

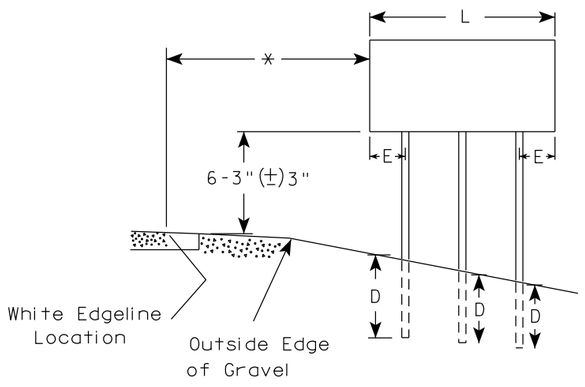


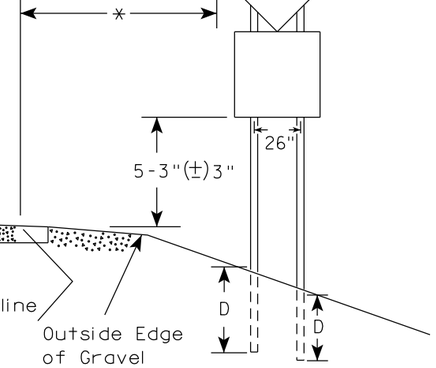


48" DIAMOND WARNING SIGN

RURAL AREA (See Note 3)







48" DIAMOND WARNING SIGN

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)		
	L	E
***	Greater than 48" Less than 60"	12"
	60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) 3" or 6'-3" (±) 3" depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±) 3".

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

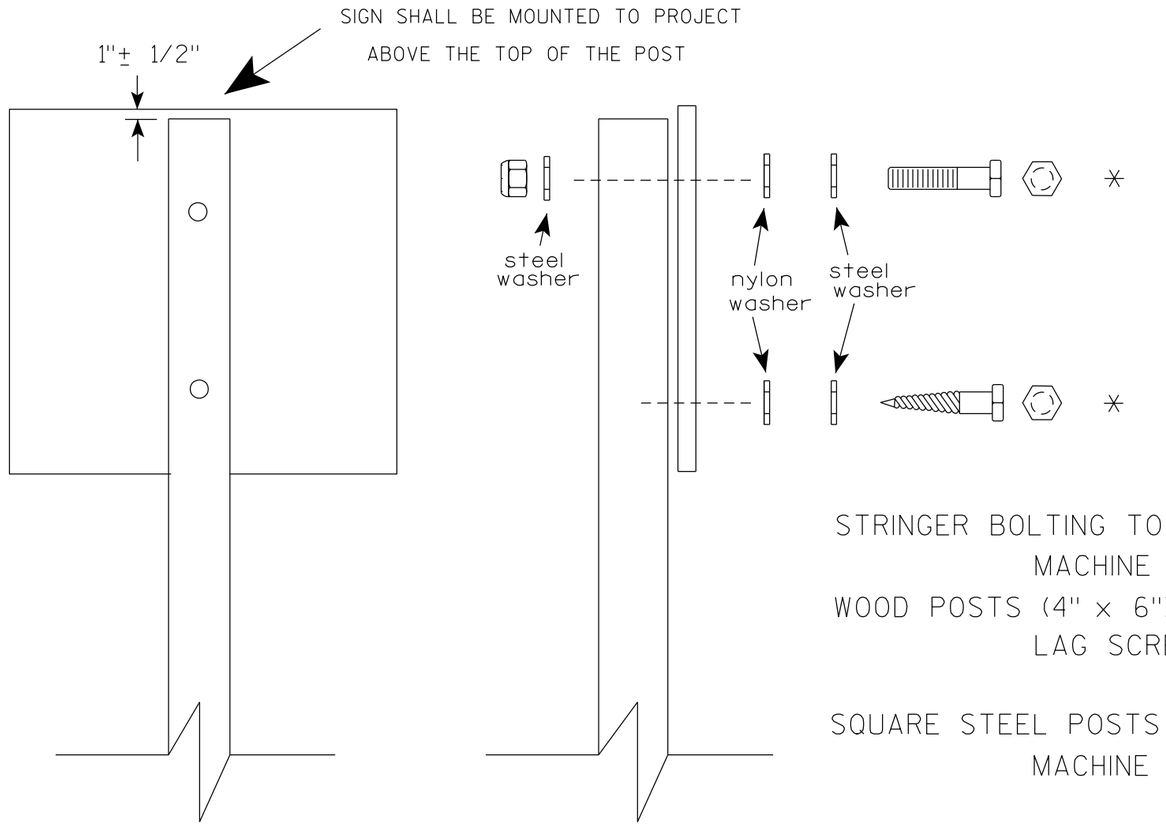
TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-4.16

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 24	E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3

b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

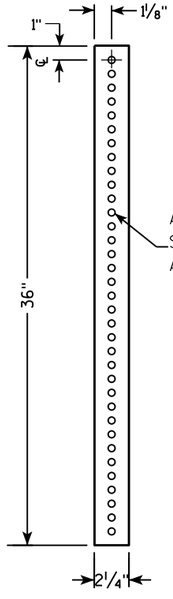
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

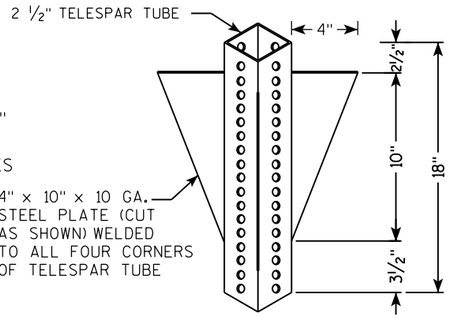
DATE 4/1/2020 PLATE NO. A4-8.9

TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM

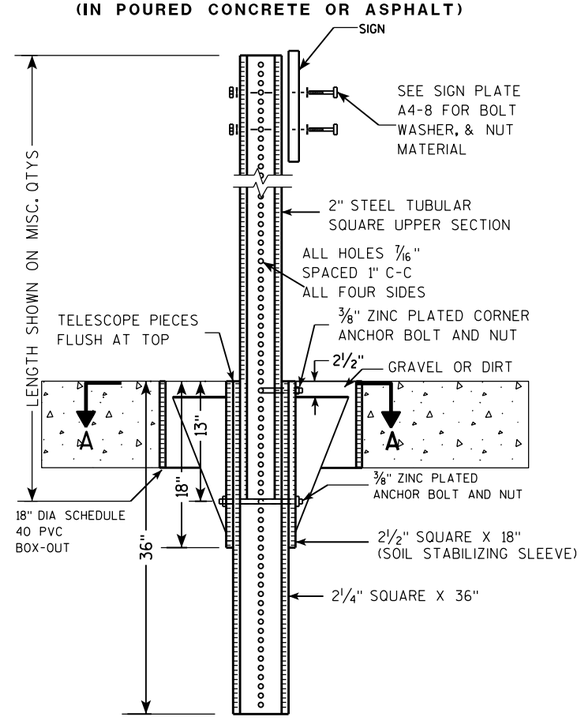
2 ¼ " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



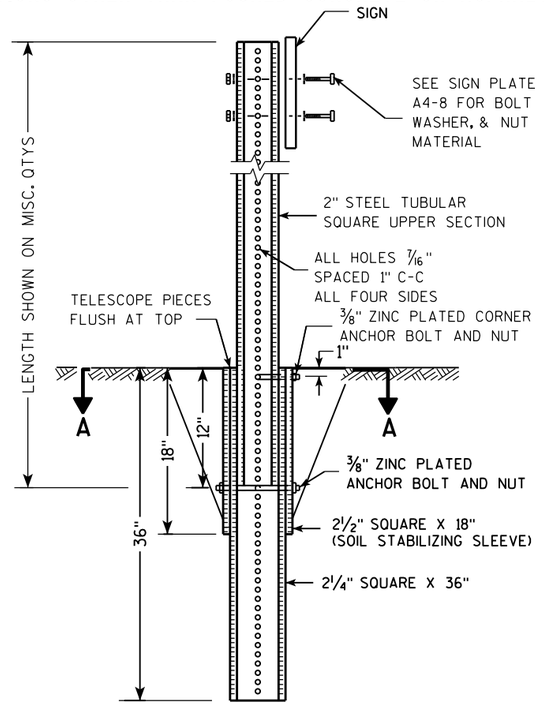
2 ½ " SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH

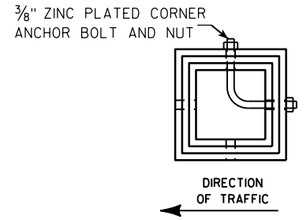


DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)





SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

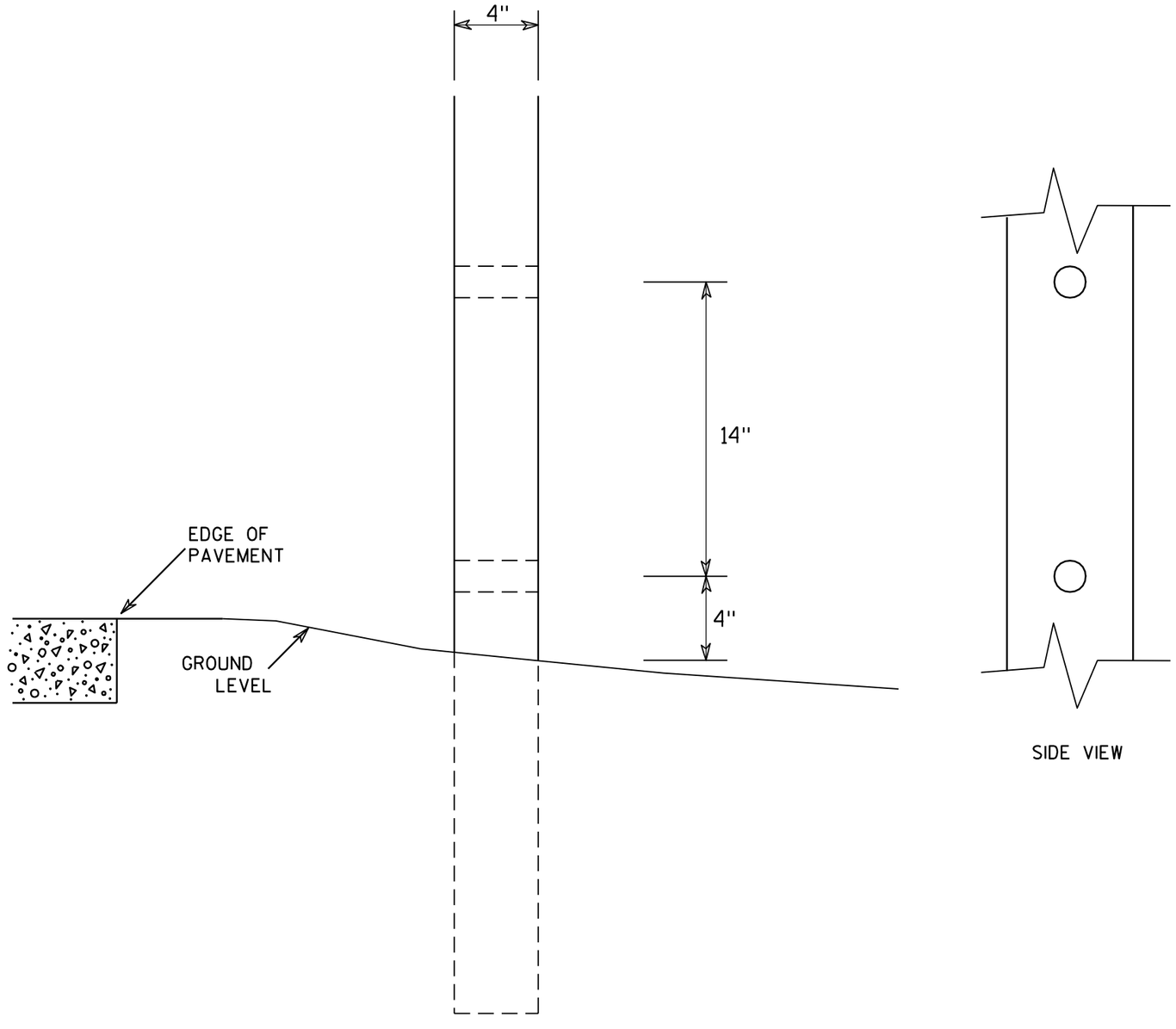
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/05/15 PLATE A4-9.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE : 05-FEB-2015 17:09 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

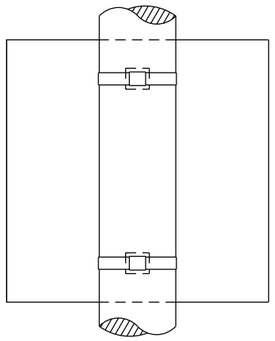
APPROVED Chester J Spang for State Traffic Engineer

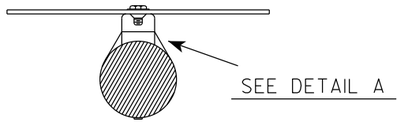
DATE 3/27/97 PLATE NO. A4-11.2

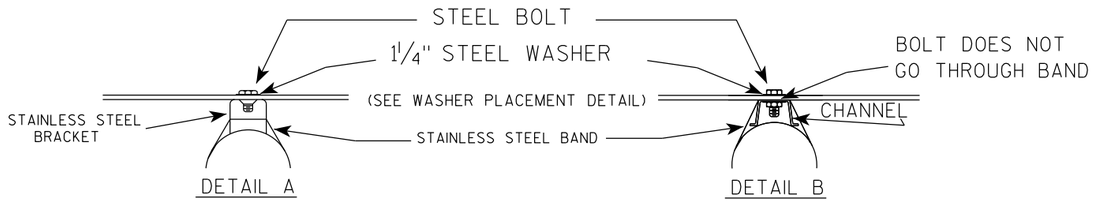
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 27	E

BANDING

SINGLE SIGN



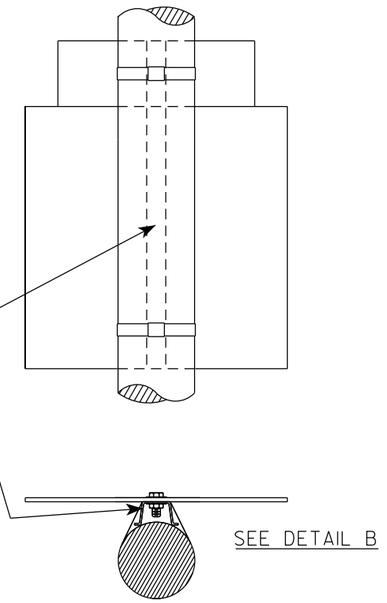




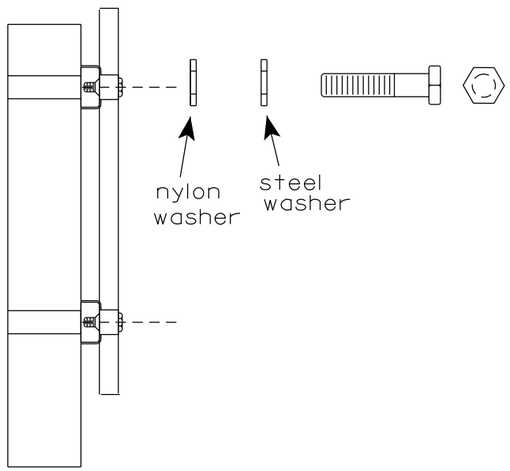
GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be ¾" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



WASHER PLACEMENT



WASHERS (ALL POSTS) -
1-1/4" O.D. X ⅜" I.D. X 1/16" STEEL
1-1/4" O.D. X ⅜" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

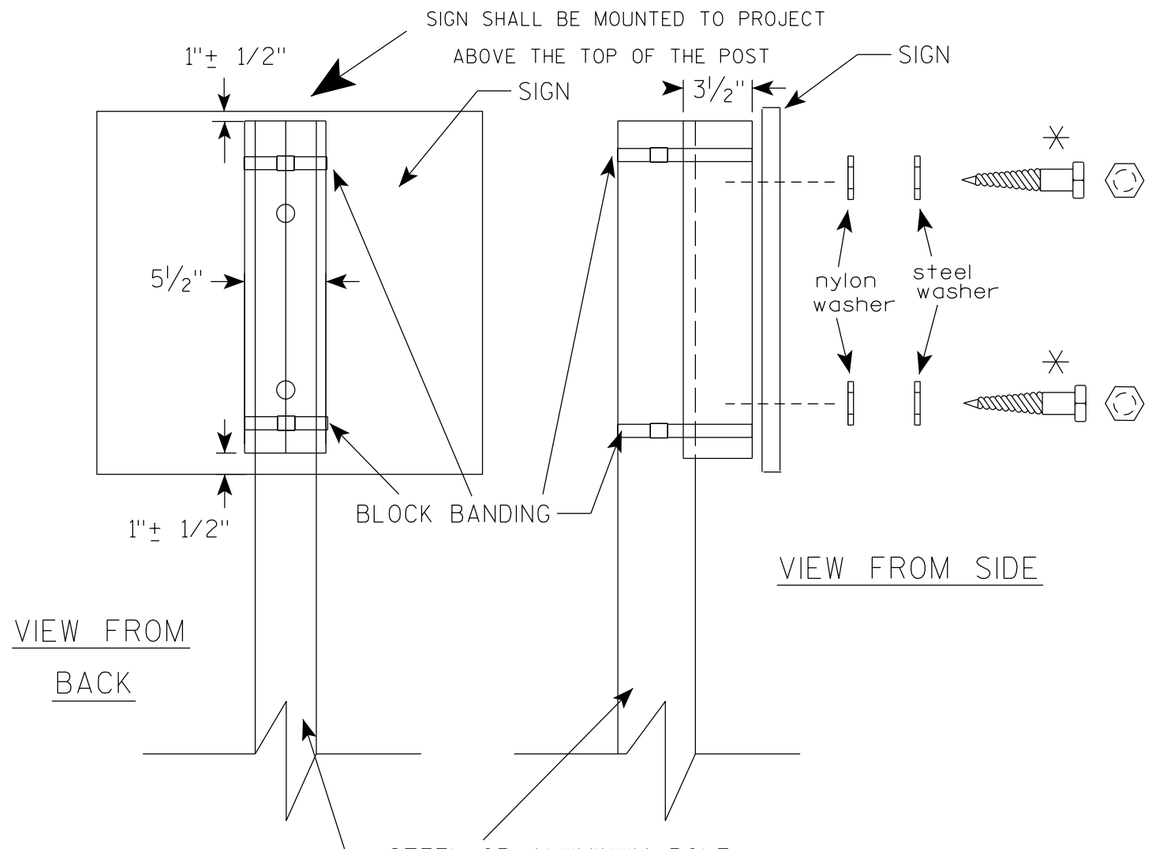
WISCONSIN DEPT OF TRANSPORTATION

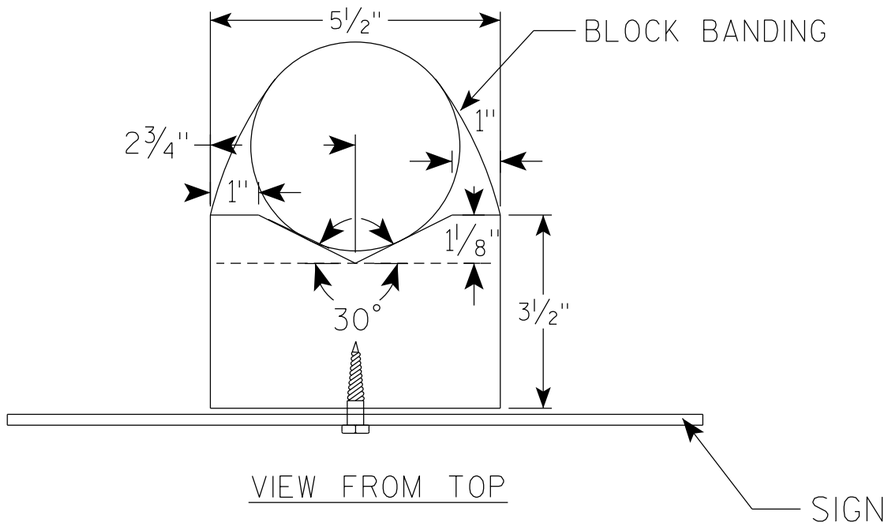
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 6/10/19 PLATE NO. A5-9.4

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 28	E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A59.dgn PLOT DATE : 10-JUN 2019 4:10 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : $$.....plotscale.....$$





GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WisDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, ¾" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORNALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✳ LAG BOLTS SHALL BE ⅜" X 2½"

BLOCK BANDING DETAIL (V-BLOCK OPTION)

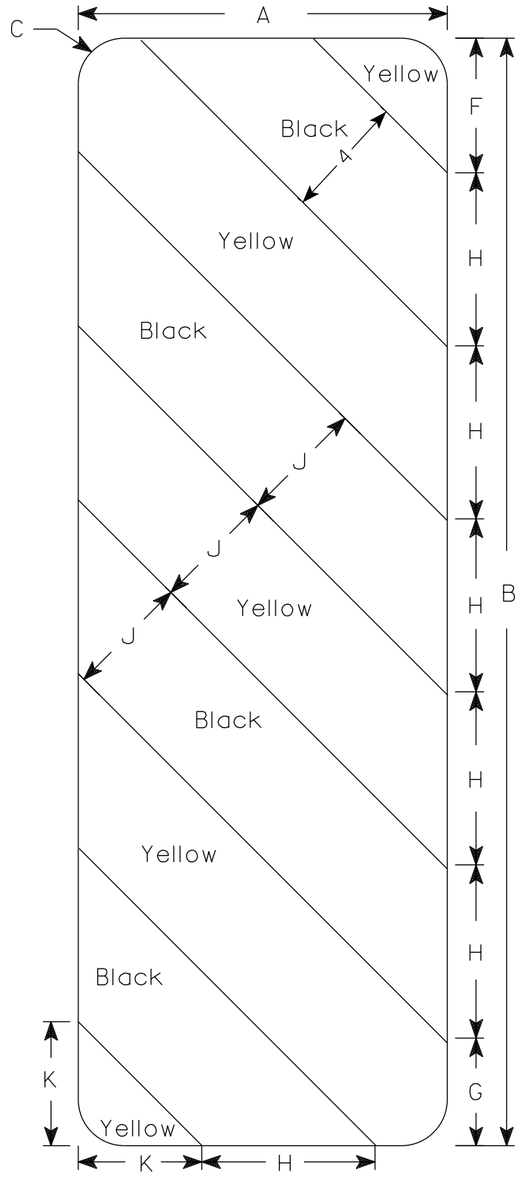
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

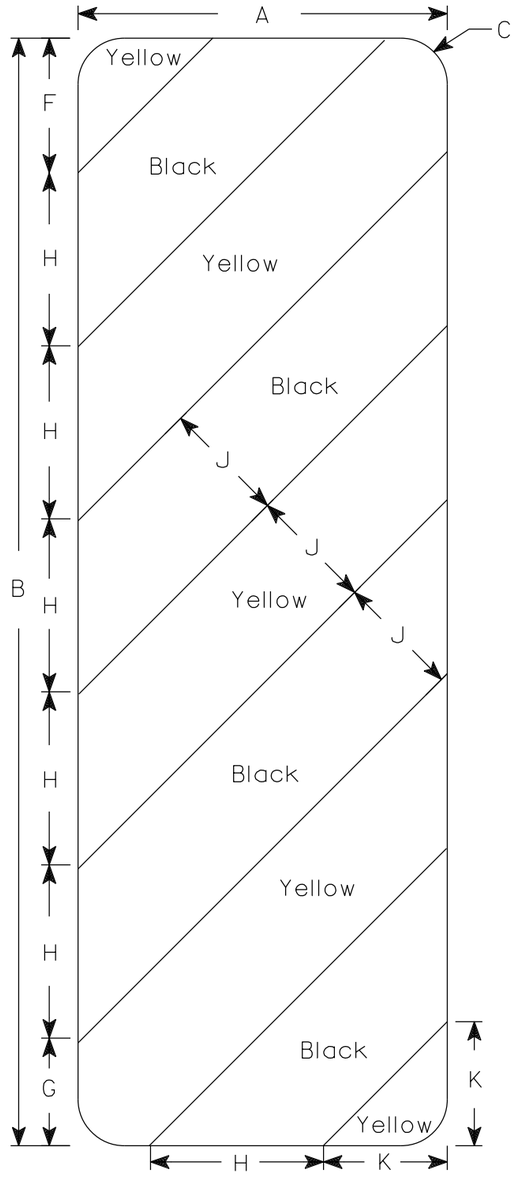
DATE 4/19/2022 PLATE NO. A5-10.3

PROJECT NO:

SHEET NO: 29 E



W5-52L



W5-52R

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Yellow
   Message - Black
3. Alternate colors of stripes as shown.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	36	1 1/2			4 3/8	3 1/2	5 5/8	45°	4	4																3.0
2M	12	36	1 1/2			4 3/8	3 1/2	5 5/8	45°	4	4																3.0
3	18	54	1 1/2			6	5 1/2	8 1/2	45°	6	6 9/16																6.75
4																											
5																											

STANDARD SIGN

W5-52L & W5-52R

WISCONSIN DEPT OF TRANSPORTATION

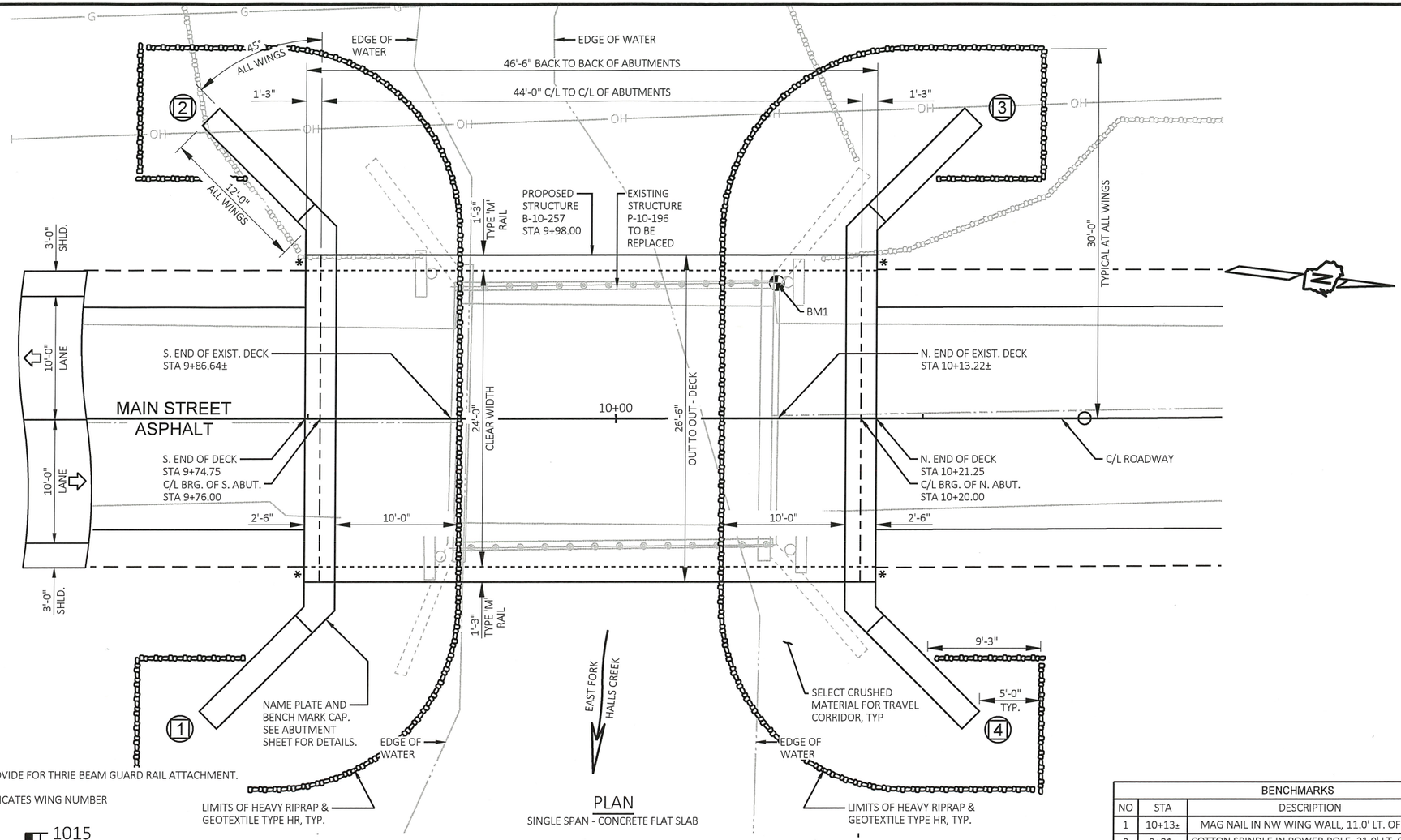
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/4/2024 PLATE NO. W5-52.10

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 30 E

STATE PROJECT NUMBER

7852-00-71

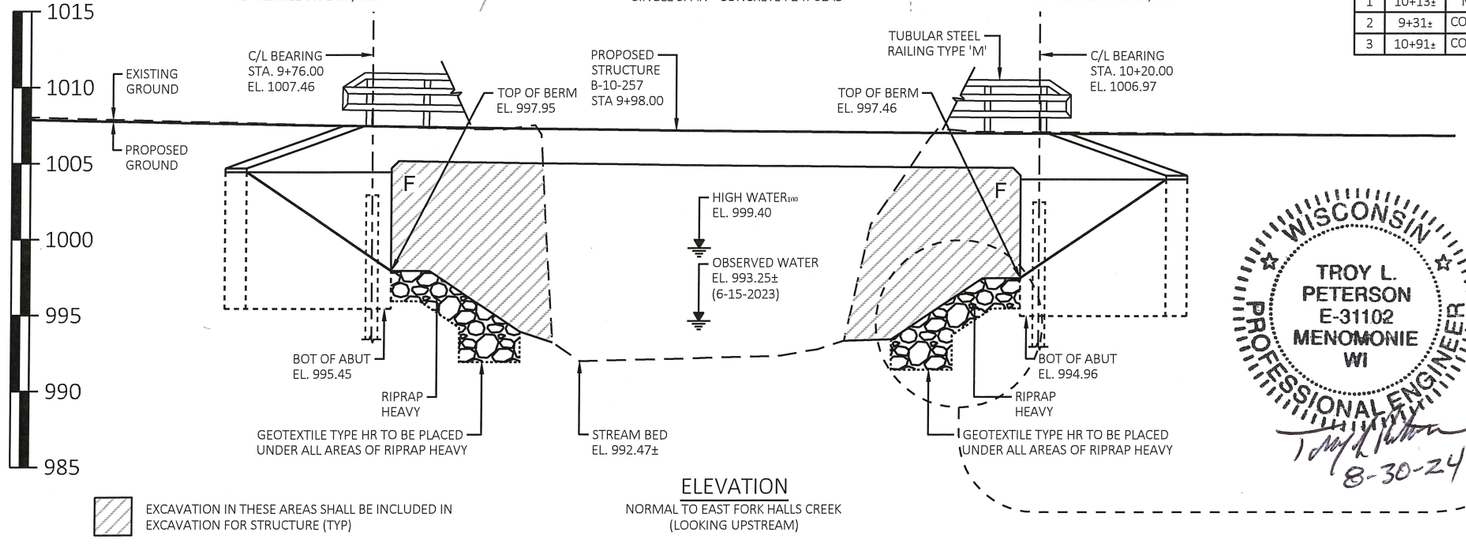


* PROVIDE FOR THRIE BEAM GUARD RAIL ATTACHMENT.

INDICATES WING NUMBER

PLAN

SINGLE SPAN - CONCRETE FLAT SLAB



EXCAVATION IN THESE AREAS SHALL BE INCLUDED IN EXCAVATION FOR STRUCTURE (TYP)

ELEVATION

NORMAL TO EAST FORK HALLS CREEK (LOOKING UPSTREAM)

DESIGN DATA

STRUCTURE IS DESIGNED FOR FUTURE WEARING SURFACE OF 20 POUNDS PER SQUARE FOOT.

LIVE LOAD:

DESIGN LOADING	HL - 93
INVENTORY RATING FACTOR	RF = 1.05
OPERATING RATING FACTOR	RF = 1.36
WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV)	250 KIPS

MATERIAL PROPERTIES:

CONCRETE MASONRY

SLAB	f'c = 4,000 PSI
ALL OTHER	f'c = 3,500 PSI
BAR STEEL REINFORCEMENT, GRADE 60 (INCLUDES STAINLESS STEEL REINFORCEMENT)	fy = 60,000 PSI

HYDRAULIC DATA

100 YEAR FREQUENCY

Q_{100}	900	C.F.S.
Q_{BRDGE}	900	C.F.S.
VEL.	4.49	F.P.S.
HW_{100}	EL. 999.40	FT.
WATERWAY AREA	200	SQ. FT.
DRAINAGE AREA	7.33	SQ. MI.
SCOUR CRITICAL CODE	5	

2 YEAR FREQUENCY

Q_2 TOTAL	160	C.F.S.
VEL.	2.95	F.P.S.
HW_2	EL. 995.43	FT.

TRAFFIC DATA

AADT (2025)	<100
AADT (2045)	<100
DESIGN SPEED	30 MPH

FOUNDATION DATA

ABUTMENTS TO BE SUPPORTED ON PILING STEEL HP 10x42, WITH A REQUIRED DRIVING RESISTANCE OF 150 TONS * PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC EQUATION.

ESTIMATED LENGTH 25'-0" NORTH ABUTMENT
ESTIMATED LENGTH 25'-0" SOUTH ABUTMENT

* THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS THE REQUIRED DRIVING RESISTANCE MULTIPLIED BY A RESISTANCE FACTOR OF 0.5 USING MODIFIED GATES TO DETERMINE DRIVEN PILE CAPACITY.

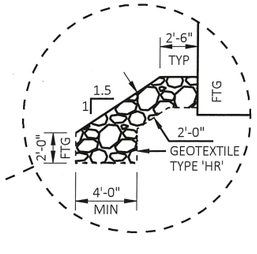
BENCHMARKS			
NO	STA	DESCRIPTION	ELEV
1	10+13±	MAG NAIL IN NW WING WALL, 11.0' LT. OF C/L	1006.67
2	9+31±	COTTON SPINDLE IN POWER POLE, 21.9' LT. OF C/L	1010.31
3	10+91±	COTTON SPINDLE IN POWER POLE, 25.6' LT. OF C/L	1006.92

BRIDGE OFFICE CONTACT
AARON M. BONK
(608) 261-0261

CONSULTANT CONTACT
TROY L. PETERSON
(715) 235-9081

LIST OF DRAWINGS

WISCONSIN
TROY L. PETERSON
E-31102
MENOMONIE
WI
PROFESSIONAL ENGINEER
8-30-24



NO.	DATE	REVISION	BY

ORIGINAL PLANS PREPARED BY

Cedar CORPORATION
www.cedarcorp.com 800-472-7372

ACCEPTED SDR 11/18/24
CHIEF STRUCTURES DESIGN ENGINEER — DATE

STRUCTURE B-10-257

MAIN STREET BRIDGE OVER E FORK HALLS CREEK

COUNTY	TOWN ~~CITY/VILLAGE~~
CLARK	MENTOR

DESIGN SPEC.
AASHTO LRFD BRIDGE DESIGN SPECIFICATION

DESIGNED BY	DESIGNED CK'D	DRAWN BY	PLANS CK'D
TLP	DWM	NJT	TLP

GENERAL PLAN

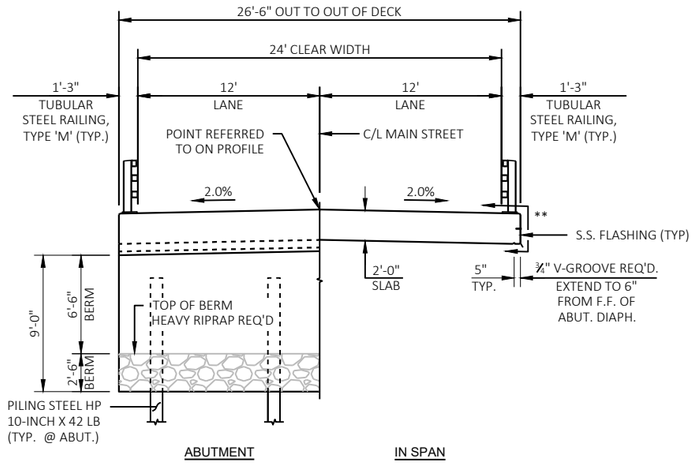
SHEET 1 OF 9
31

SCALE =

I.D. 7852-00-01 DATE:

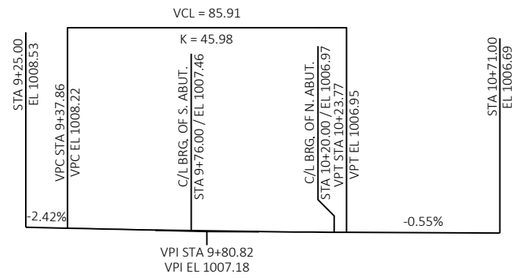
8

8

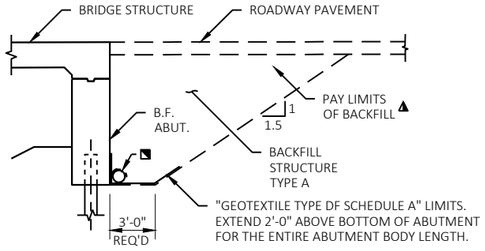


CROSS SECTION THRU STRUCTURE

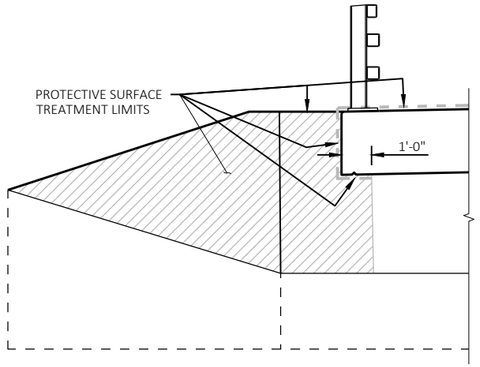
(LOOKING NORTH)



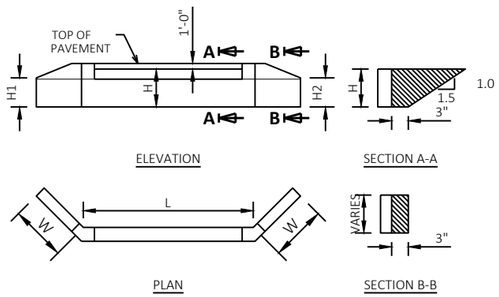
PROPOSED GRADE ON MAIN STREET



STRUCTURE BACKFILL & LIMITS

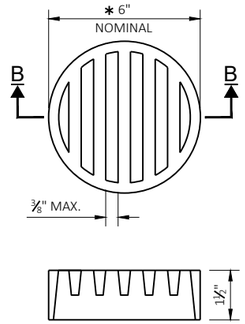


PROTECTIVE SURFACE TREATMENT DETAILS



ABUTMENT BACKFILL DIAGRAM

L = ABUTMENT BODY LENGTH AT BACKFACE (FT)
H = AVERAGE ABUTMENT FILL HEIGHT (FT)
H1 = WING 1 HEIGHT AT TIP (FT)
H2 = WING 2 HEIGHT AT TIP (FT)
W = WING LENGTH (FT)
EF = EXPANSION FACTOR (1.20 FOR CY BID ITEMS AND 1.00 FOR TON BID ITEMS)
V_{CF} = (L)(3.0')(H) + (L)(0.5)(1.5H)(H) + (3')(0.5)(H1+H2+H+H)(W)
V_{CY} = V_{CF}(EF)/27
V_{TON} = V_{CY}(2.0)



RODENT SHIELD DETAIL

* DIMENSION IS APPROXIMATE. THE GRATE IS SIZED TO FIT INTO A PIPE COUPLING. ORIENT SO SLOTS ARE VERTICAL.

THE RODENT SHIELD, PIPE COUPLING AND SCREWS SHALL BE CONSIDERED INCIDENTAL TO THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6-INCH".

THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALLY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE EXPOSED END OF THE PIPE UNDERDRAIN. THE SHIELD SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1-INCH STAINLESS STEEL SHEET METAL SCREWS.

STATE PROJECT NUMBER

7852-00-71

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

ALL STATIONS AND ALL ELEVATIONS ARE IN FEET.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS SHOWN OR NOTED OTHERWISE.

ALL REINFORCING BARS ARE ENGLISH. THE FIRST DIGIT OF A THREE-DIGIT BAR MARK OR THE FIRST TWO DIGITS OF A FOUR-DIGIT BAR MARK SIGNIFIES THE BAR SIZE.

JOINT FILLER SHALL CONFORM TO THE REQUIREMENTS OF A.A.S.H.T.O. DESIGNATION M 153, TYPE I, II OR III OR A.A.S.H.T.O. DESIGNATION M213.

THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH RIPRAP HEAVY AND GEOTEXTILE TYPE 'HR' TO THE EXTENT SHOWN ON SHEETS 1 AND 2 AND IN THE ABUTMENT DETAILS.

THE EXISTING STRUCTURE (P-10-196) IS A 26.8' LONG BY 21.9' WIDE SINGLE SPAN TIMBER FLAT SLAB BRIDGE.

** PROTECTIVE SURFACE TREATMENT SHALL BE APPLIED TO THE TOP AND EDGES OF THE SLAB AND TO THE OUTSIDE 1'-0" OF THE UNDERSIDE OF THE SLAB.

THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES BRIDGES B-10-257" SHALL BE THE EXISTING GRADE LINE.

AT THE BACK FACE OF ABUTMENT ALL VOLUME WHICH CANNOT BE IN PLACE BEFORE ABUTMENT CONSTRUCTION AND NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH BACKFILL STRUCTURE.

AT ABUTMENTS, CONCRETE POURED UNDERWATER WILL BE ALLOWED AND SHALL BE DONE IN ACCORDANCE WITH SECTION 502.3.5.3 OF THE STANDARD SPECIFICATIONS.

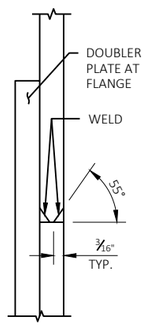
SLAB FALSE WORK SHALL BE SUPPORTED ON PILES OR THE SUBSTRUCTURE UNLESS AN ALTERNATE METHOD IS APPROVED BY THE ENGINEER.

▲ BACKFILL PAY LIMITS, BACKFILL BEYOND BACKFILL PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.

◥ PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN.

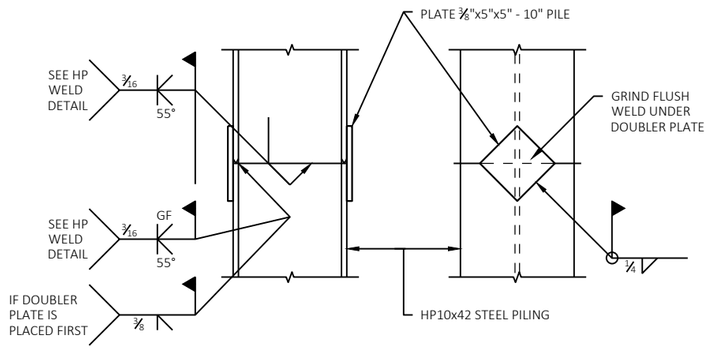
TOTAL ESTIMATED QUANTITIES

ITEM NUMBER	BID ITEMS	UNIT	SOUTH ABUT.	NORTH ABUT.	SUPER.	TOTALS
203.0260	REMOVING STRUCTURE OVER WATERWAY MINIMAL DEBRIS P-10-196	EACH	-	-	-	1
206.1001	EXCAVATION FOR STRUCTURES BRIDGES B-10-257	EACH	-	-	-	1
210.1500	BACKFILL STRUCTURE TYPE A	TON	320	320	-	640
502.0100	CONCRETE MASONRY BRIDGES	CY	47.3	47.3	95.4	190
502.3200	PROTECTIVE SURFACE TREATMENT	SY	20	20	165	205
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	1980	1980	-	3960
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	2030	2030	16500	20560
513.4061	RAILING TUBULAR TYPE M	LF	-	-	93	93
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	10	10	-	20
550.0500	PILE POINTS	EACH	7	7	-	14
550.1100	PILING STEEL HP 10-INCH X 42 LB	LF	175	175	-	350
606.0300	RIPRAP HEAVY	CY	85	85	-	170
612.0406	PIPE UNDERDRAIN WRAPPED 6-INCH	LF	80	80	-	160
645.0111	GEOTEXTILE TYPE DF SCHEDULE A	SY	30	30	-	60
645.0120	GEOTEXTILE TYPE HR	SY	155	155	-	310
SPV.0090.01	FLASHING STAINLESS STEEL	LF	-	-	83	83
SPV.0195.01	SELECT CRUSHED MATERIAL FOR TRAVEL CORRIDOR	TON	40	40	-	80
	NON-BID ITEMS					
	FILLER	SIZE	-	-	-	½" X ¾"



HP WELD DETAIL

FLANGE SHOWN, WEB SIMILAR



PILE SPLICE DETAILS

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-10-257

DRAWN BY NJT | PLANS CK'D TLP

CROSS SECTIONS, QUANTITIES & NOTES

SHEET 2 OF 9

32

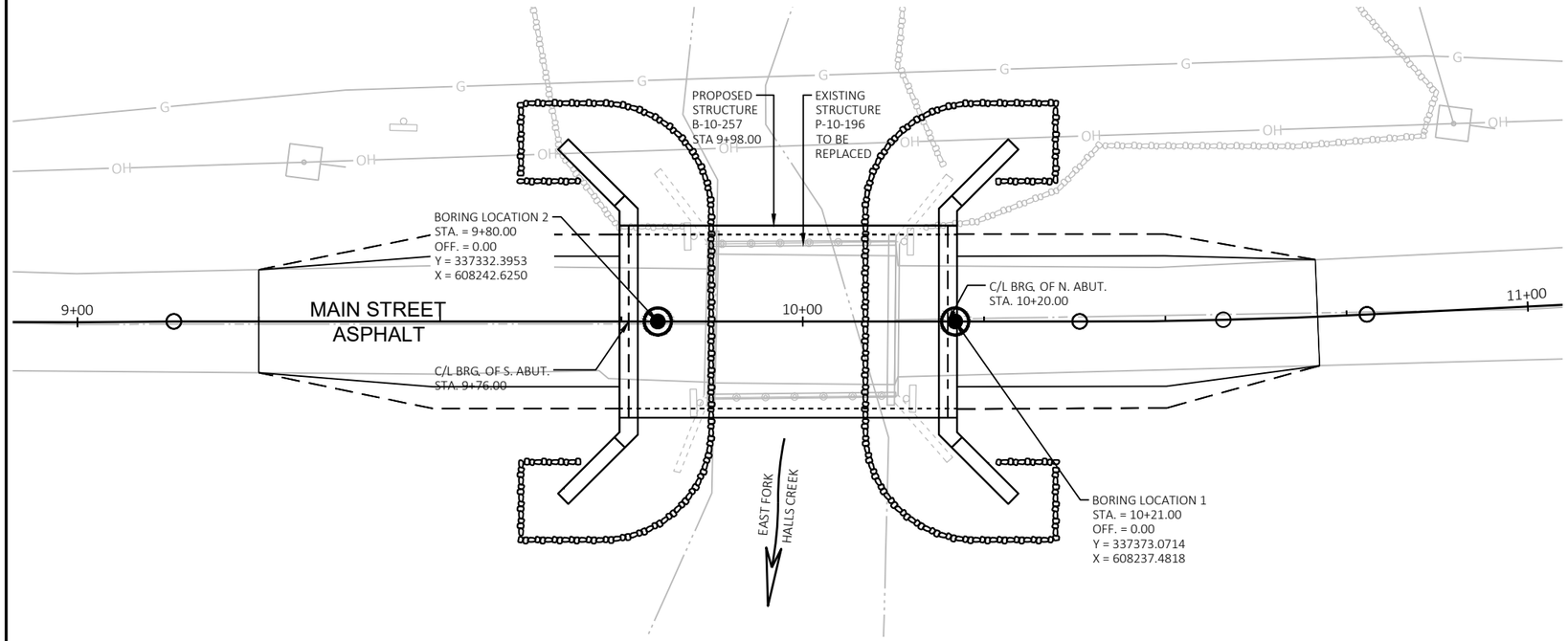
SCALE =

8

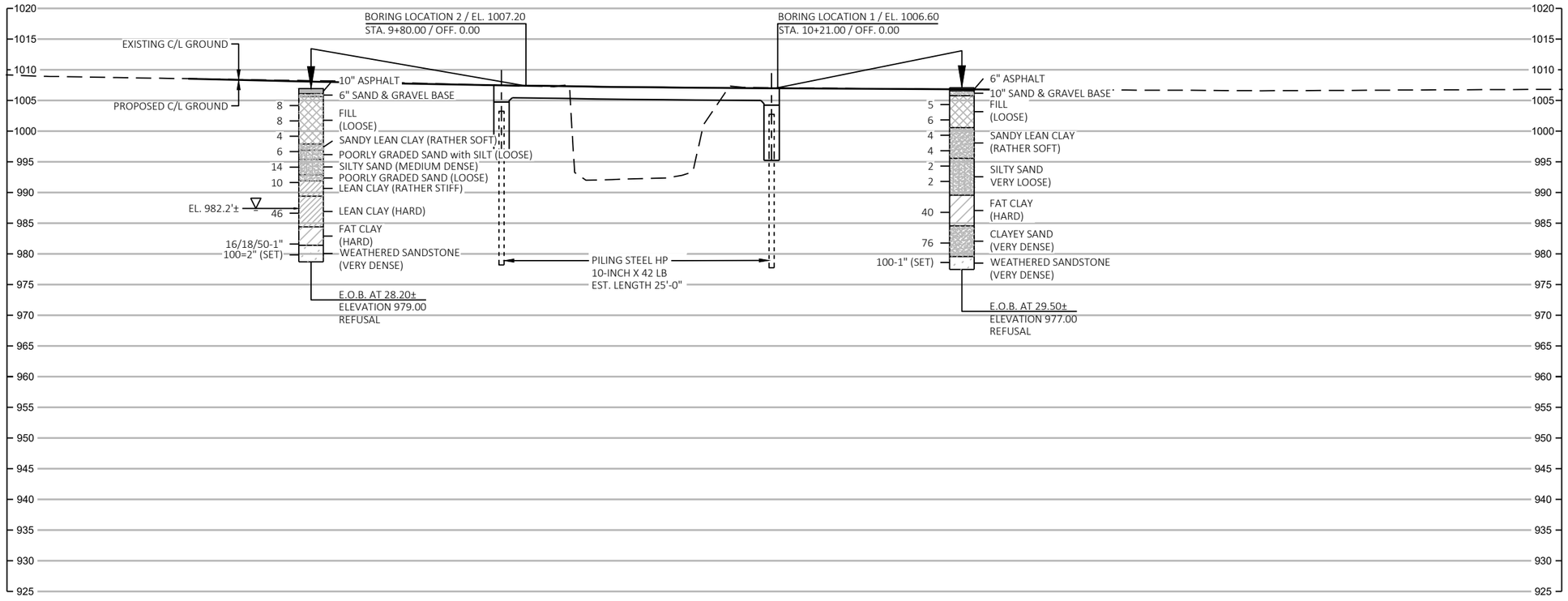
8

STATE PROJECT NUMBER

7852-00-71



PLAN



ELEVATION

SUBSURFACE NOTES

THE SUBSURFACE INFORMATION PRESENTED HEREIN IS AN ABBREVIATED VERSION OF THE INFORMATION PRESENTED IN THE GEOTECHNICAL ENGINEERING REPORT. REVIEW THE APPROPRIATE GEOTECHNICAL REPORT AND SOIL BORING LOGS FOR ADDITIONAL SUBSURFACE INFORMATION.

BORINGS & REPORT COMPLETED BY:

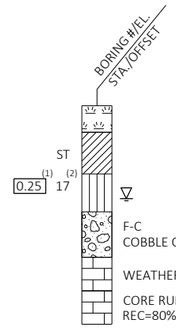
CHOSEN VALLEY TESTING, INC.
1019 SECOND AVENUE SW
ONALASKA, WI 54650
(608) 782-5505

BORINGS PERFORMED ON:
12/5/2023 - B1
12/5/2023 - B2

MATERIAL SYMBOLS

ASPHALT, TOPSOIL, PEAT, CONCRETE, FILL, GRAVEL, SAND, CLAY, SILT, BOULDERS OR COBBLES, LIMESTONE, BEDROCK (UNKNOWN), SHALE, SANDSTONE, IGNEOUS/ META

LEGEND OF BORING



(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)

(2) UNLESS OTHERWISE, SPECIFIED THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION

- AT TIME OF DRILLING
- END OF DRILLING
- AFTER DRILLING

ABBREVIATIONS

F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-10-257

DRAWN BY NJT | PLANS CK'D TLP

SUBSURFACE EXPLORATION

SHEET 3 OF 9
33

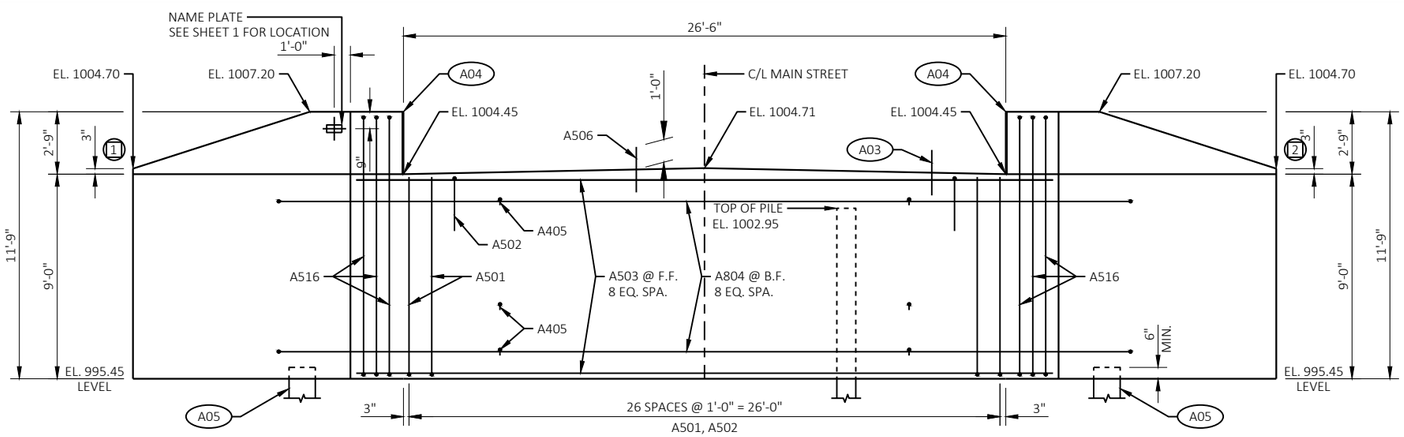
SCALE =

8

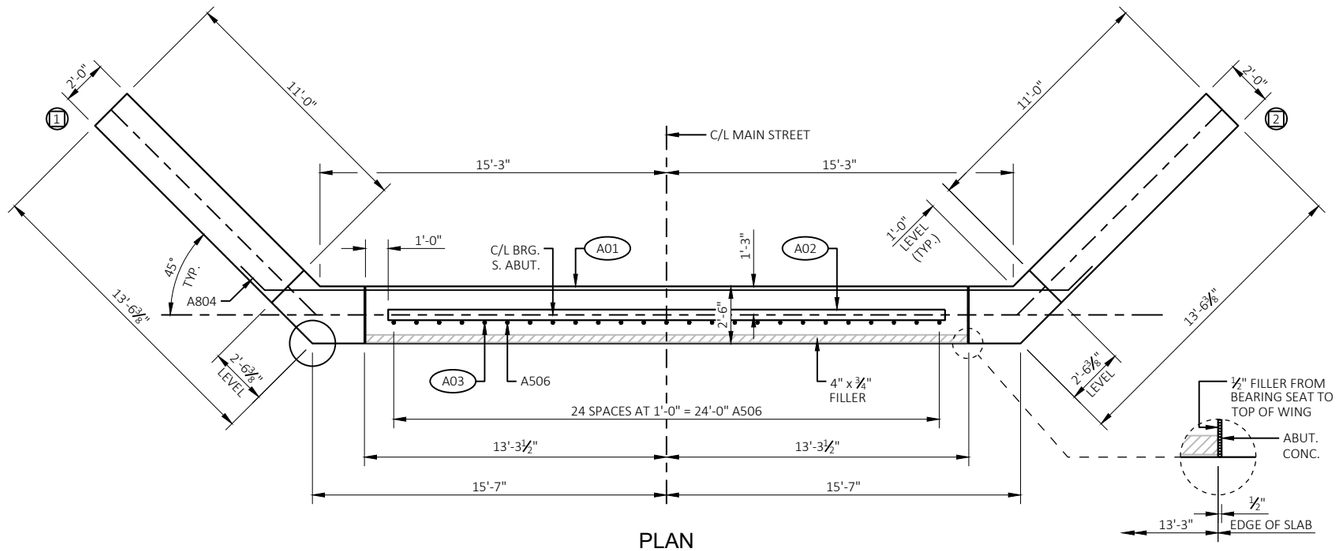
8

STATE PROJECT NUMBER

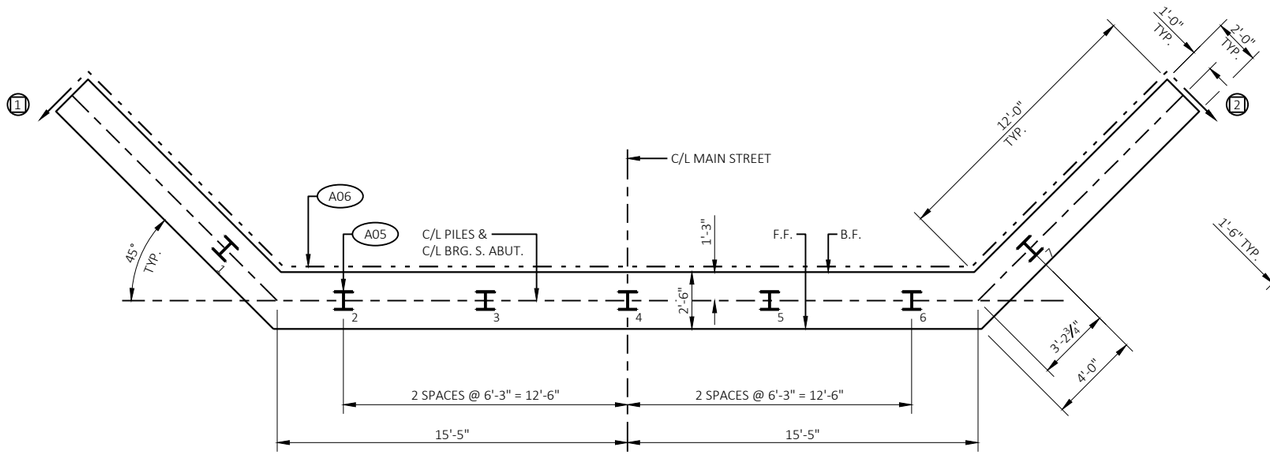
7852-00-71



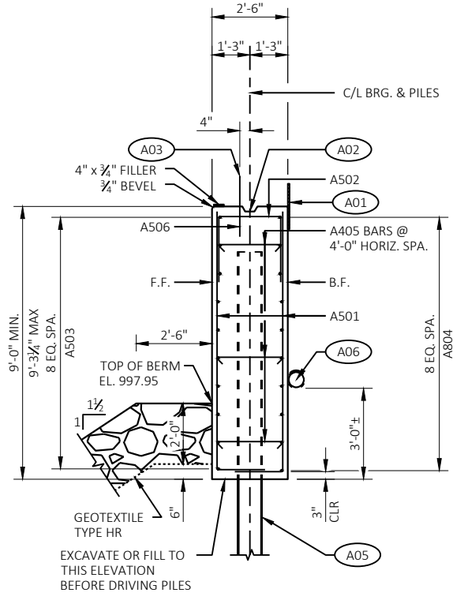
ELEVATION
(LOOKING SOUTH)



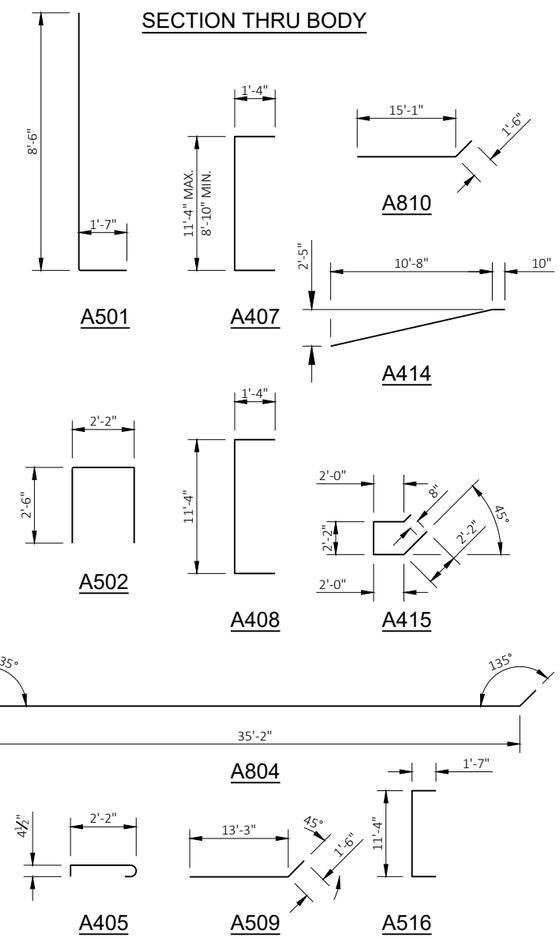
PLAN



PILE PLAN



SECTION THRU BODY



BILL OF BARS

1980# UNCOATED 2030# COATED

BAR MARK	COAT	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION
A501		54	9-11	X		BODY - VERTICAL
A502		27	6-11	X		BODY - VERTICAL
A503		9	30-10			BODY - HORIZONTAL F.F.
A804		9	37-11	X		BODY - HORIZONTAL B.F.
A405		15	2-11	X		BODY - VERTICAL TIE BARS
A506	X	25	2-0			BODY - TOP DOWELS BARS
A407	X	64	12-7	X	☒	WING 1 & 2 - VERTICAL
A408	X	6	13-10	X		WING 1 & 2 - VERTICAL
A509	X	18	14-7	X		WING 1 & 2 - BODY - HORIZ. F.F.
A810	X	18	16-5	X		WING 1 & 2 - HORIZONTAL - B.F.
A411	X	4	12-0			WING 1 & 2 - HORIZ. - F.F. & B.F.
A412	X	4	7-11			WING 1 & 2 - HORIZ. - F.F. & B.F.
A413	X	4	4-8			WING 1 & 2 - HORIZ. - F.F. & B.F.
A414	X	4	11-9	X		WING 1 & 2 - HORIZ. - F.F. & B.F.
A415	X	8	8-10	X		WING 1 & 2 - HORIZ. TOP-F.F. & B.F.
A516	X	12	14-3	X		BODY - VERTICAL

NOTE: BAR DIMENSIONS ARE OUT TO OUT OF BAR. THE FIRST DIGIT OF A THREE-DIGIT BAR MARK OR THE FIRST TWO DIGITS OF A FOUR-DIGIT BAR MARK SIGNIFIES THE BAR SIZE.

☒ LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTH.

BAR SERIES TABLE

BUNDLE AND TAG EACH SERIES SEPARATELY

BAR MARK	NO. REQUIRED	LENGTH
A407	4 SERIES OF 16	11-4 TO 13-10

NOTE: B.F. = BACK FACE
F.F. = FRONT FACE

LEGEND

①	INDICATES WING NUMBER
A01	18" RUBBERIZED MEMBRANE WATERPROOFING (R.M.W). SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACK FACE.
A02	KEYED CONSTRUCTION JOINT FORMED BY A BEVELED 2 X 6.
A03	BARS MAY BE PLACED AFTER CONCRETE IS POURED, BUT BEFORE INITIAL SET HAS TAKEN PLACE.
A04	SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.
A05	STEEL PILING HP 10 X 42 WITH A REQUIRED DRIVING RESISTANCE OF 150 TONS PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED LENGTH = 25 LF.
A06	PIPE UNDERDRAIN WRAPPED 6-INCH. SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN. RODENT SHIELD TO BE INCLUDED IN BID PRICE OF "PIPE UNDERDRAIN WRAPPED 6-INCH".

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-10-257

DRAWN BY NJT | PLANS CK'D TLP

SOUTH ABUTMENT

SHEET 4 OF 9
34

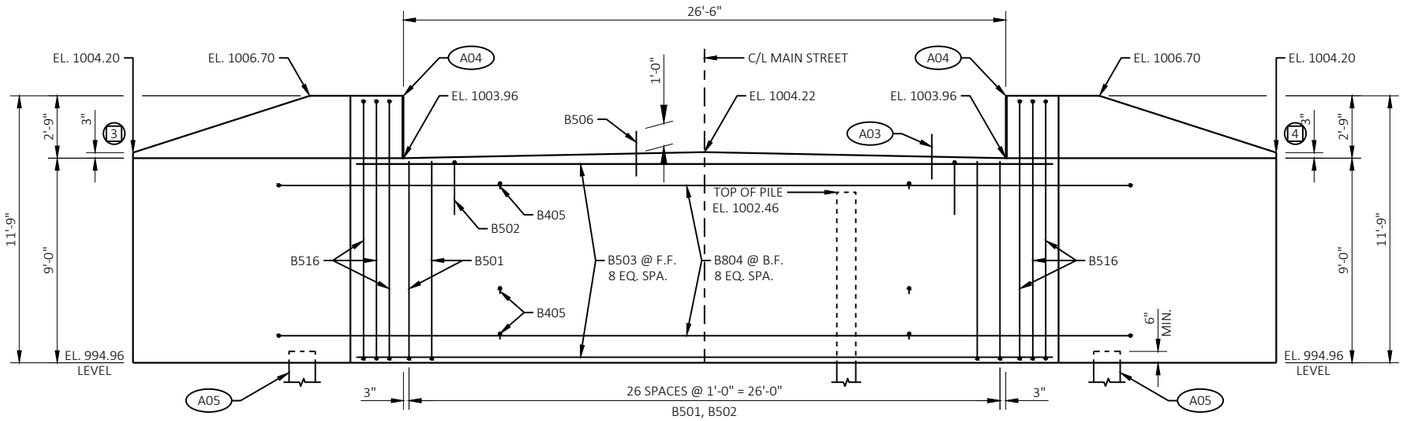
SCALE =

8

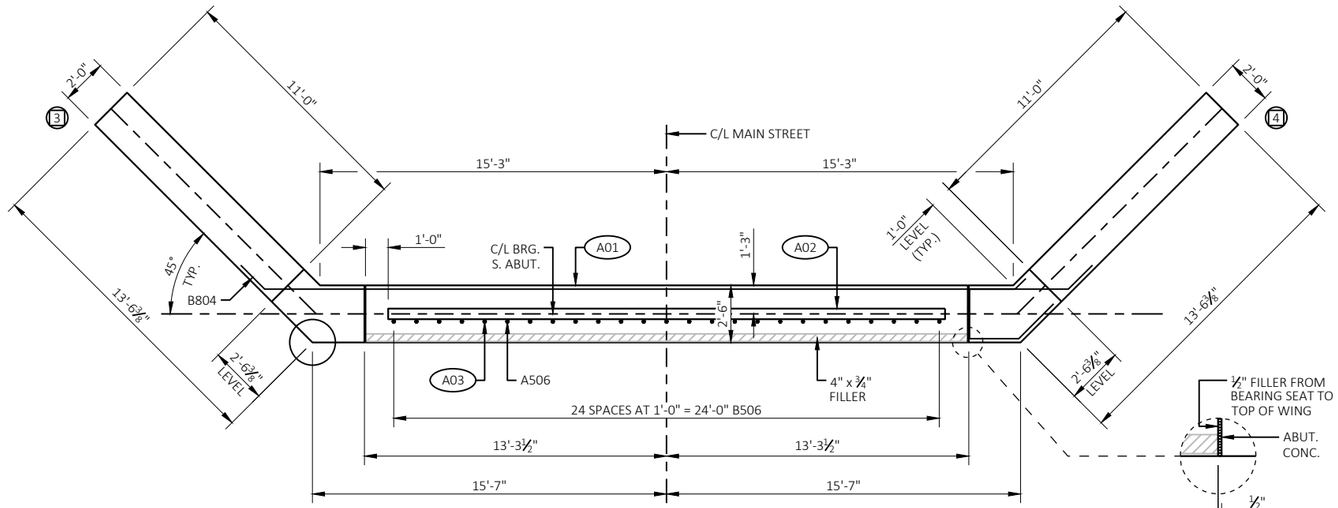
8

STATE PROJECT NUMBER

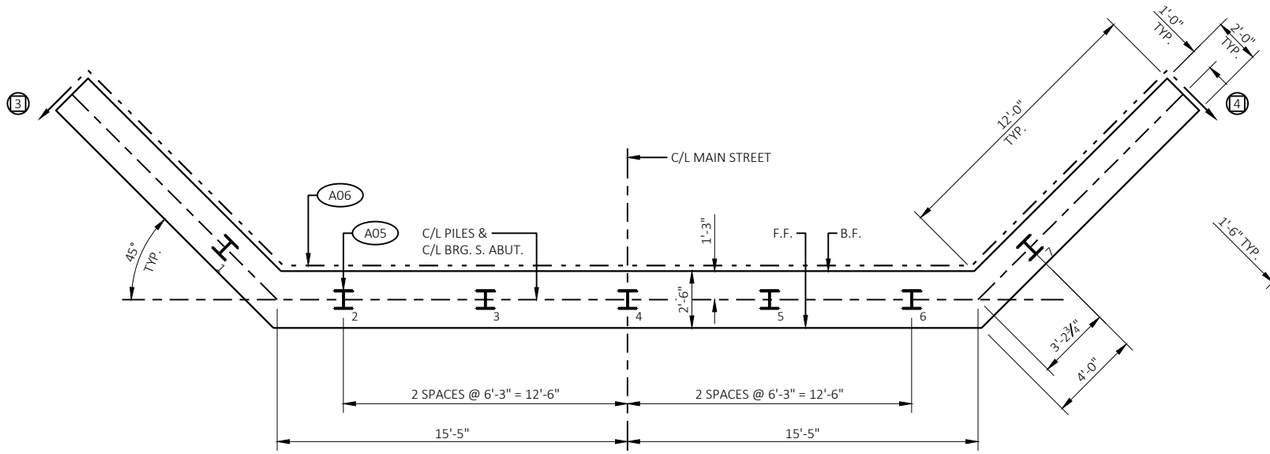
7852-00-71



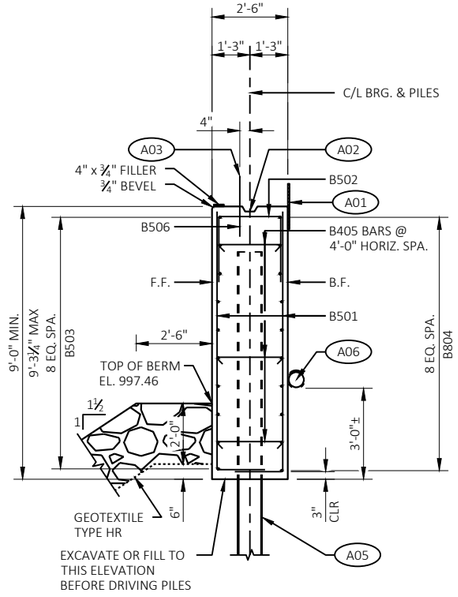
ELEVATION
(LOOKING NORTH)



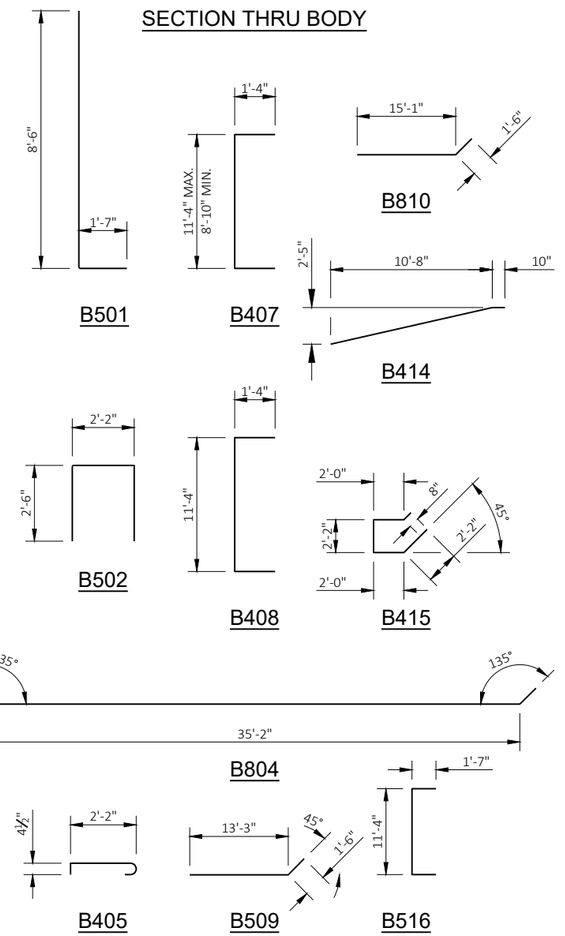
PLAN



PILE PLAN



SECTION THRU BODY



BILL OF BARS

1980# UNCOATED 2030# COATED

BAR MARK	COAT	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION
B501		54	9-11	X		BODY - VERTICAL
B502		27	6-11	X		BODY - VERTICAL
B503		9	30-10			BODY - HORIZONTAL F.F.
B804		9	37-11	X		BODY - HORIZONTAL B.F.
B405		15	2-11	X		BODY - VERTICAL TIE BARS
B506	X	25	2-0			BODY - TOP DOWELS BARS
B407	X	64	12-7	X	☒	WING 3 & 4 - VERTICAL
B408	X	6	13-10	X		WING 3 & 4 - VERTICAL
B509	X	18	14-7	X		WING 3 & 4 - BODY - HORIZ. F.F.
B810	X	18	16-5	X		WING 3 & 4 - HORIZONTAL - B.F.
B411	X	4	12-0			WING 3 & 4 - HORIZ. - F.F. & B.F.
B412	X	4	7-11			WING 3 & 4 - HORIZ. - F.F. & B.F.
B413	X	4	4-8			WING 3 & 4 - HORIZ. - F.F. & B.F.
B414	X	4	11-9	X		WING 3 & 4 - HORIZ. - F.F. & B.F.
B415	X	8	8-10	X		WING 3 & 4 - HORIZ. TOP-F.F. & B.F.
B516	X	12	14-3	X		BODY - VERTICAL

NOTE: BAR DIMENSIONS ARE OUT TO OUT OF BAR. THE FIRST DIGIT OF A THREE-DIGIT BAR MARK OR THE FIRST TWO DIGITS OF A FOUR-DIGIT BAR MARK SIGNIFIES THE BAR SIZE.

☒ LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTH.

BAR SERIES TABLE

BUNDLE AND TAG EACH SERIES SEPARATELY

BAR MARK	NO. REQUIRED	LENGTH
B407	4 SERIES OF 16	11-4 TO 13-10

NOTE: B.F. = BACK FACE
F.F. = FRONT FACE

LEGEND

⬡	INDICATES WING NUMBER
A01	18" RUBBERIZED MEMBRANE WATERPROOFING (R.M.W). SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACK FACE.
A02	KEYED CONSTRUCTION JOINT FORMED BY A BEVELED 2 X 6.
A03	BARS MAY BE PLACED AFTER CONCRETE IS POURED, BUT BEFORE INITIAL SET HAS TAKEN PLACE.
A04	SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.
A05	STEEL PILING HP 10 X 42 WITH A REQUIRED DRIVING RESISTANCE OF 150 TONS PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED LENGTH = 25 LF.
A06	PIPE UNDERDRAIN WRAPPED 6-INCH. SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN. RODENT SHIELD TO BE INCLUDED IN BID PRICE OF "PIPE UNDERDRAIN WRAPPED 6-INCH".

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-10-257

DRAWN BY NJT | PLANS CK'D TLP

NORTH ABUTMENT

SHEET 5 OF 9
35

SCALE =

8

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STATE PROJECT NUMBER

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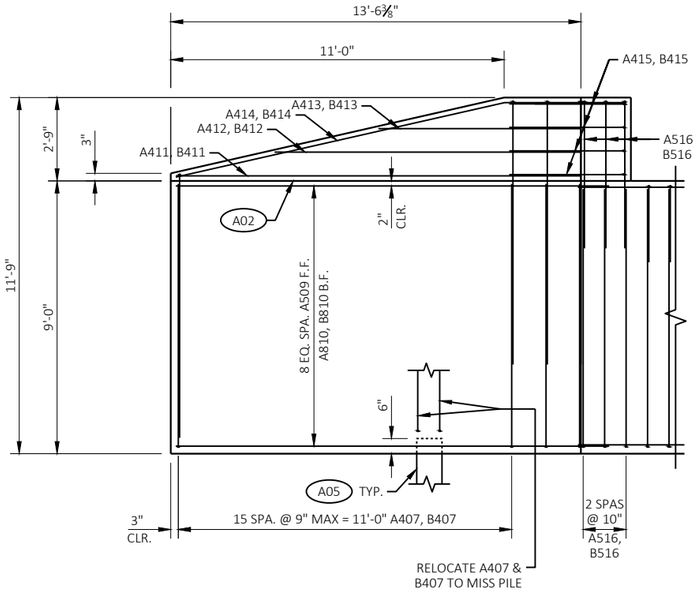
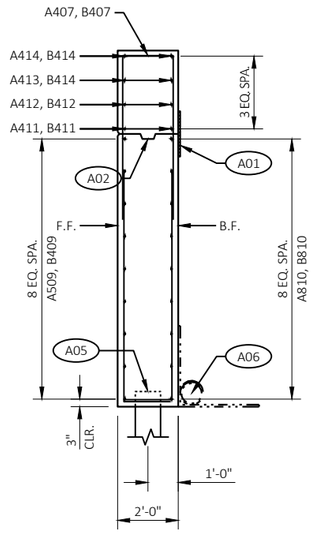
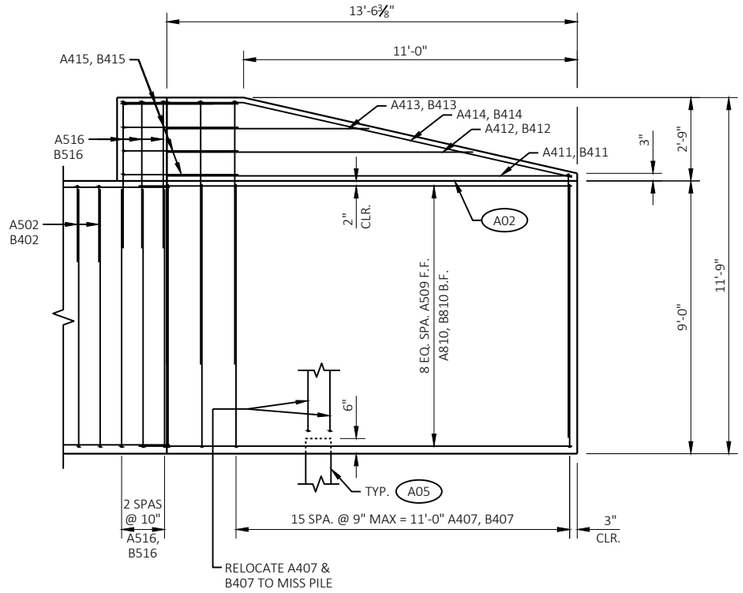


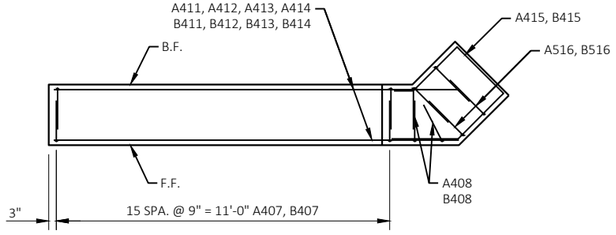
WINGS 1 & 3 ELEVATION
SHOWING F.F. WING



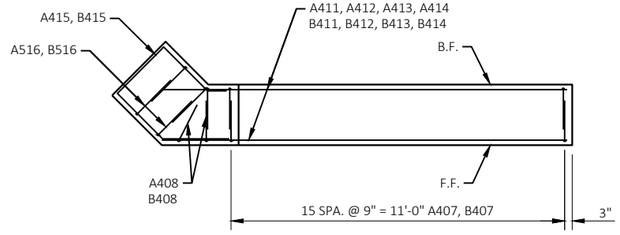
SECTION THRU WING (TYP.)



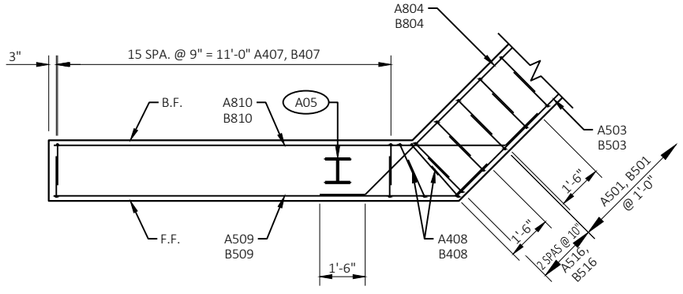
WINGS 2 & 4 ELEVATION
SHOWING F.F. WING



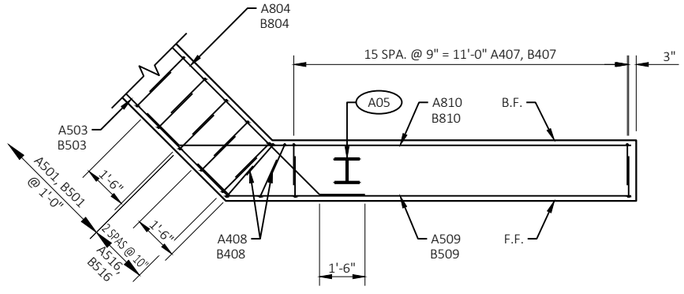
WINGS 1 & 3 PLAN
SHOWING UPPER WING REINFORCEMENT



WINGS 2 & 4 PLAN
SHOWING UPPER WING REINFORCEMENT



WINGS 1 & 3 PLAN
SHOWING LOWER WING REINFORCEMENT



WINGS 2 & 4 PLAN
SHOWING LOWER WING REINFORCEMENT

NOTE: B.F. = BACK FACE
F.F. = FRONT FACE

LEGEND

⬭	INDICATES WING NUMBER
A01	18" RUBBERIZED MEMBRANE WATERPROOFING (R.M.W). SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACK FACE.
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A05	STEEL PILING HP 10 X 42 WITH A REQ'D DRIVING RESISTANCE OF 150 TONS PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED LENGTH = 25 LF. BOTH ABUTMENTS
A06	PIPE UNDERDRAIN WRAPPED 6-INCH. SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN. RODENT SHIELD TO BE INCLUDED IN BID PRICE OF "PIPE UNDERDRAIN WRAPPED 6-INCH".

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-10-257

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ABUTMENT DETAILS

SHEET 6 OF 9
36

SCALE =

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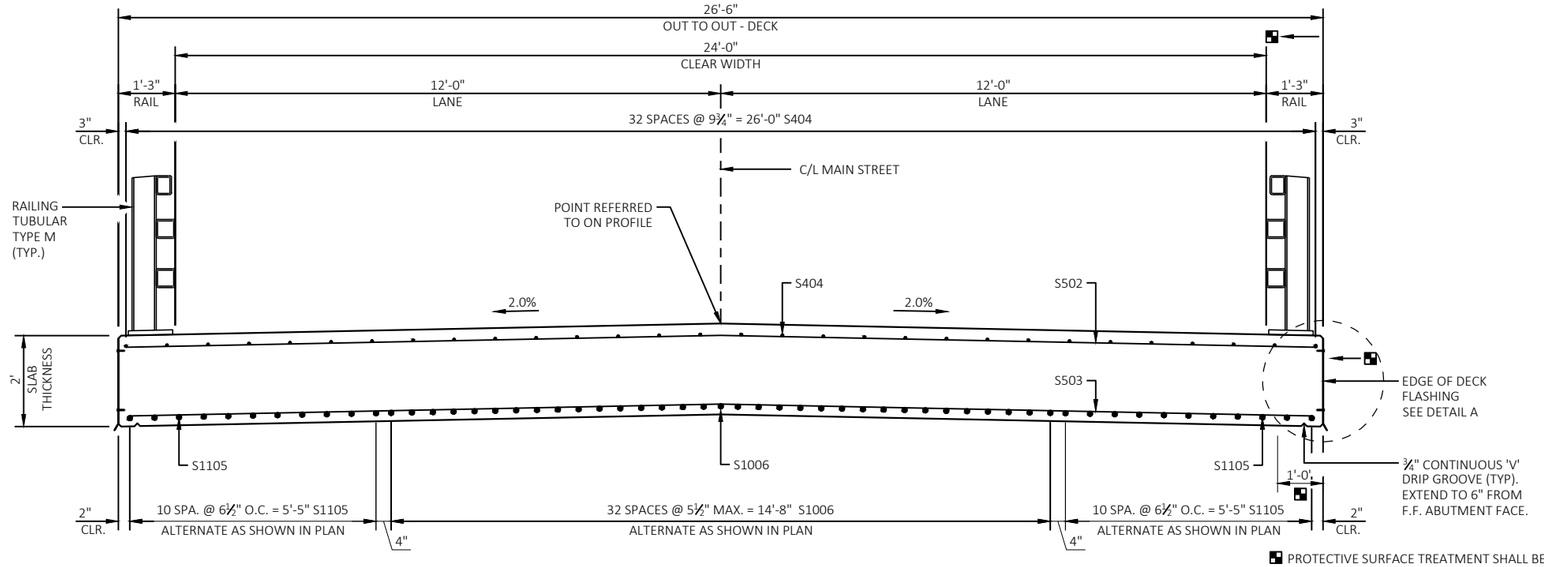


CROSS SECTION THRU ROADWAY

PROTECTIVE SURFACE TREATMENT SHALL BE APPLIED TO THE TOP OF DECK, SIDES OF DECK, AND UNDERDECK EDGE AS SHOWN.

BILL OF BARS

16500# COATED

BAR MARK	COAT	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION
S501	X	54	7-10	X		AT END OF DECK
S502	X	53	26-2			SLAB, TOP, TRANSVERSE
S503	X	64	26-2			SLAB, BOTTOM, TRANSVERSE
S404	X	33	46-2			SLAB, TOP, LONGITUDINAL
S1105	X	22	41-7			SLAB, BOTTOM, LONG. EXTERIOR
S1006	X	33	41-7			SLAB, BOTTOM, LONGITUDINAL
S607	X	32	11-2	X		AT RAIL POSTS
S608	X	16	4-8	X		AT END RAIL POSTS
S609	X	48	6-0			AT INTERIOR RAIL POSTS

NOTE: BAR DIMENSIONS ARE OUT TO OUT OF BAR. THE FIRST DIGIT OF A THREE-DIGIT BAR MARK OR THE FIRST TWO DIGITS OF A FOUR-DIGIT BAR MARK SIGNIFIES THE BAR SIZE.

GENERAL NOTES

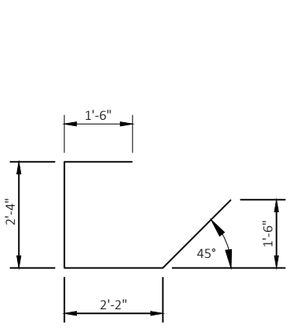
TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 3'-0" CENTERS EACH WAY. BOTTOM LONGITUDINAL BARS TO BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 4'-0" CENTERS.

TRANSVERSE BARS SHALL BE PLACED PARALLEL TO THE C/L OF SUBSTRUCTURE UNITS.

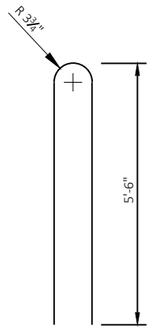
ALL SLAB THICKNESS DIMENSIONS ARE MINIMUM. ANY TOLERANCES NECESSARY TO CORRECT CONSTRUCTION DISCREPANCIES ARE TO BE PLUS (+).

CAMBER SPAN AS SHOWN TO PROVIDE FOR DEAD LOAD DEFLECTION & FUTURE CREEP. CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT. DEAD LOAD DEFLECTIONS ONLY EQUAL APPROXIMATELY 1/3 OF CAMBER VALUES SHOWN.

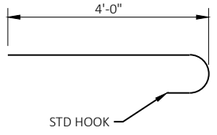
PRIOR TO RELEASING SLAB FALSEWORK, TAKE TOP OF SLAB ELEVATION AT THE C/L OF ABUTMENTS, AND AT 5/10 PTS. TO VERIFY CAMBER, TAKE ELEVATIONS ALONG GUTTER LINES, AND CROWN OR C/L.



S501



S607



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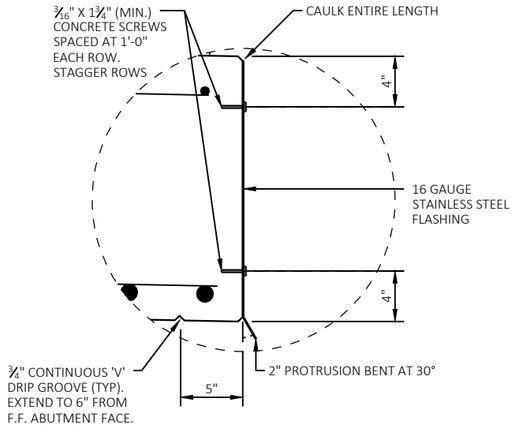


DETAIL A

RAILING NOT SHOWN FOR CLARITY

DECK FLASHING NOTES

THE BID ITEM "FLASHING STAINLESS STEEL" SHALL INCLUDE PROVIDING AND INSTALLING THE STAINLESS STEEL FLASHING, SILICONE CAULK AND 3/16-INCH CONCRETE SCREWS.

FLASHING SHALL BE INSTALLED AFTER PROTECTIVE SURFACE TREATMENT APPLICATION.

CONCRETE SCREWS SHALL BE 410 STAINLESS STEEL.

EXTEND FLASHING TO BACK FACE OF ABUTMENT DIAPHRAGM.

TOP OF FLASHING TO BEGIN APPROXIMATELY 1-INCH BELOW TOP OF DECK/SLAB SURFACE.

PROVIDE 2-INCH MINIMUM FLASHING OVERLAP, FASTEN WITH 3/16 x 2-INCH (MIN.) CONCRETE SCREWS. CAULK SHALL BE NON-STAINING, GRAY NON-BITUMINOUS JOINT SEALER.

THE FLASHING IS TO BE A CONSTANT HEIGHT BASED ON THE THINNEST SLAB DEPTH OVER THE BRIDGE LENGTH.

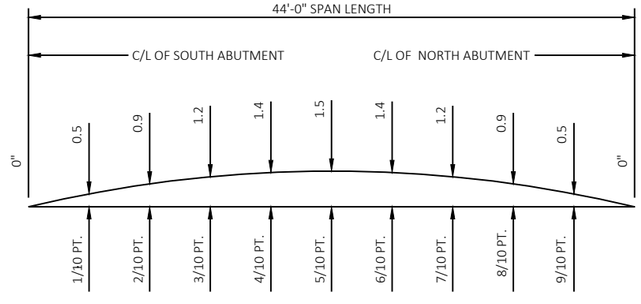
TOP OF DECK ELEVATIONS

LOCATION	SOUTH ABUT.	1/10	2/10	3/10	4/10	5/10	6/10	7/10	8/10	9/10	NORTH ABUT.
WEST EDGE OF DECK	1007.20	1007.13	1007.00	1007.06	1006.95	1006.90	1006.85	1006.81	1006.77	1006.74	1006.71
C/L OF BRIDGE DECK	1007.46	1007.39	1007.32	1007.26	1007.21	1007.16	1007.11	1007.07	1007.03	1007.00	1006.97
EAST EDGE OF DECK	1007.20	1007.13	1007.00	1007.06	1006.95	1006.90	1006.85	1006.81	1006.77	1006.74	1006.71

ELEVATIONS SHOWN ARE FINISHED DECK AND DO NOT INCLUDE ALLOWANCES OF DEAD LOAD DEFLECTION AND FUTURE CREEP.

SURVEY TOP OF SLAB ELEVATIONS

LOCATION	WEST ABUT.	5/10 PT.	EAST ABUT.
NORTH GUTTER			
CENTERLINE			
SOUTH GUTTER			



CAMBER DIAGRAM

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-10-257

DRAWN BY NJT | PLANS CK'D TLP

SUPERSTRUCTURE

SHEET 7 OF 9

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SCALE =

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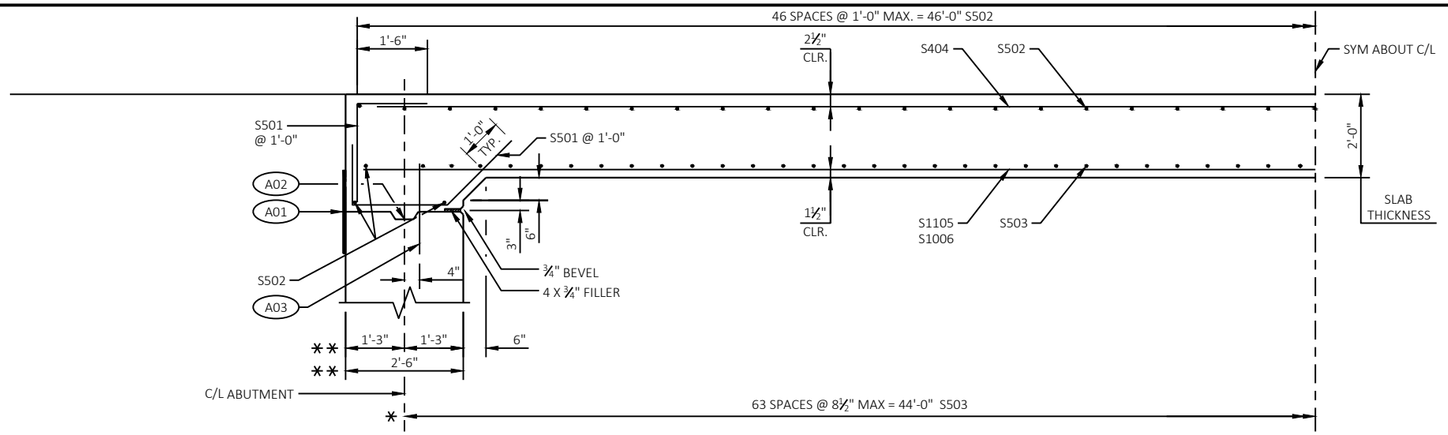
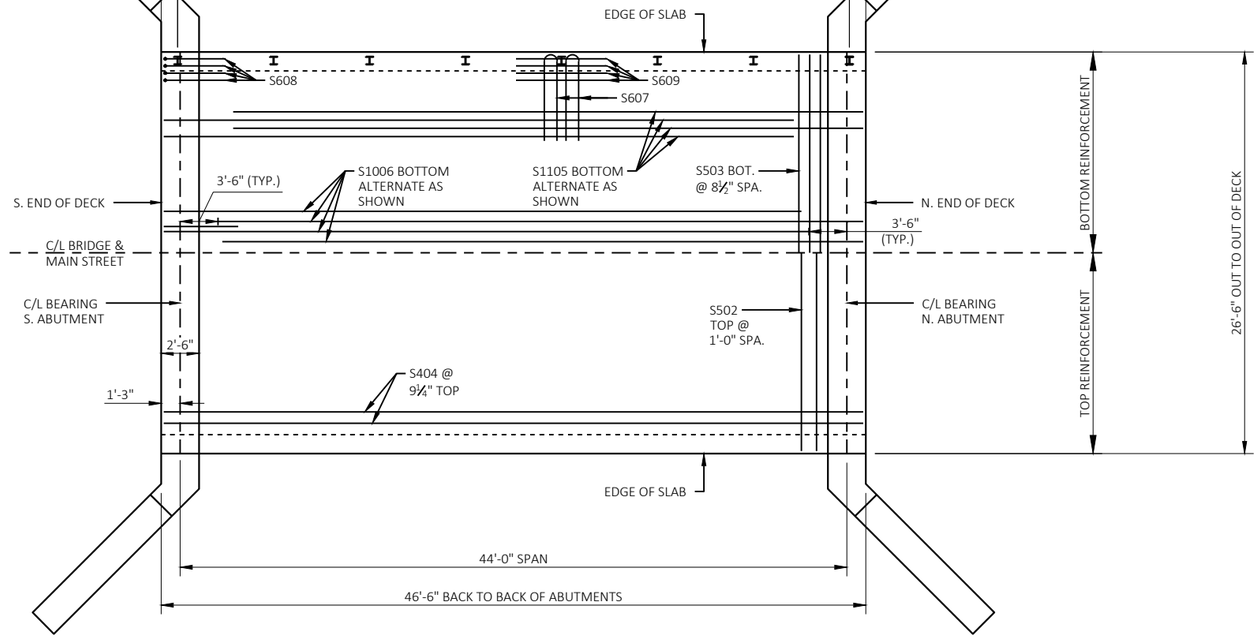


PART LONGITUDINAL SECTION



PLAN

STATE PROJECT NUMBER

7852-00-71

GENERAL NOTES

^ DIMENSIONS MEASURED ALONG C/L OF BRIDGE

** DIMENSIONS MEASURED NORMAL TO C/L OF SUBSTRUCTURE

LEGEND

A01	18" RUBBERIZED MEMBRANE WATERPROOFING (R.M.W). SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACK FACE.
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A03	BARS MAY BE PLACED AFTER CONCRETE IS POURED, BUT BEFORE INITIAL SET HAS TAKEN PLACE.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
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STRUCTURE B-10-257

DRAWN BY NJT | PLANS CK'D TLP

SUPERSTRUCTURE DETAILS

SHEET 8 OF 9

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SCALE =

8

8

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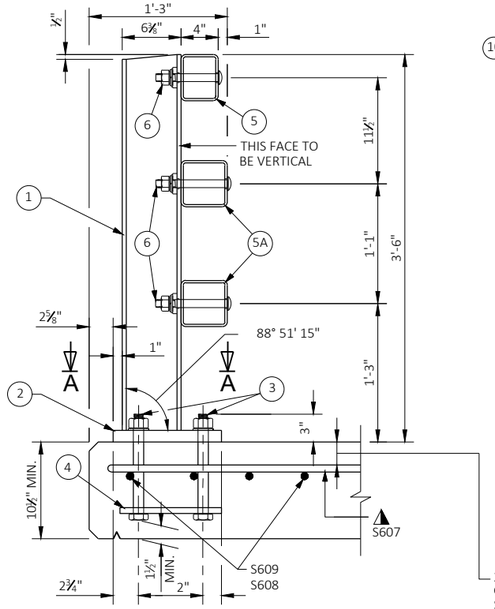
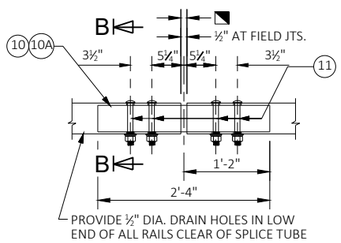
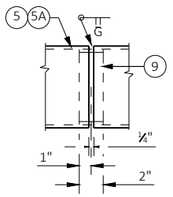


SECTION THRU RAILING ON DECK

2½" FOR SLABS ON GIRDERS; FOR OTHER STRUCTURES, PLACE BELOW TOP MAT SLAB REINFORCEMENT.

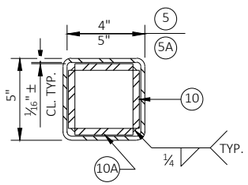


FIELD ERECTION JOINT DETAIL

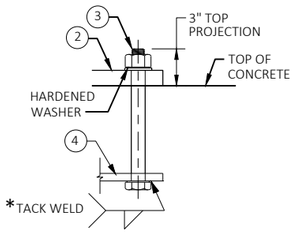


SHOP RAIL SPLICE DETAIL

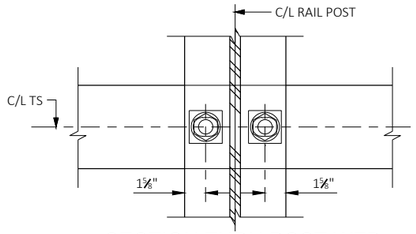
LOCATION MUST BE SHOWN ON SHOP DRAWINGS



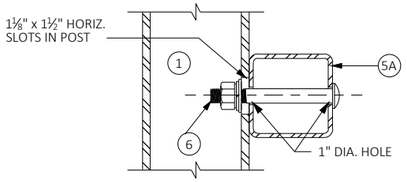
SECTION B-B



ANCHOR BOLTS



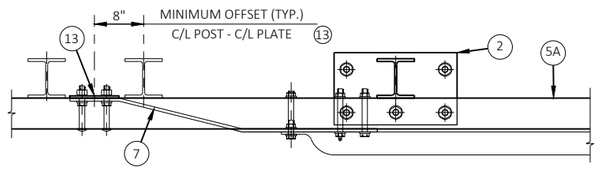
SECTION THRU POST WEB



SECTION THRU RAIL

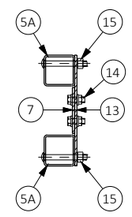
NOTE: CONNECTIONS AT LOWER RAILS SHOWN. CONNECTIONS AT TOP RAIL SIMILAR.

TYPICAL RAIL TO POST CONNECTIONS

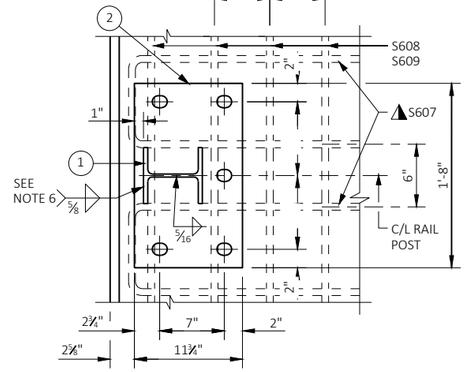


TOP VIEW AT END POST

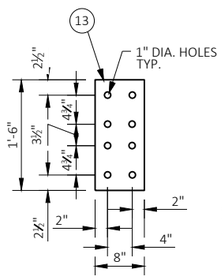
THRIE BEAM RAIL ATTACHMENT



SECTION C-C

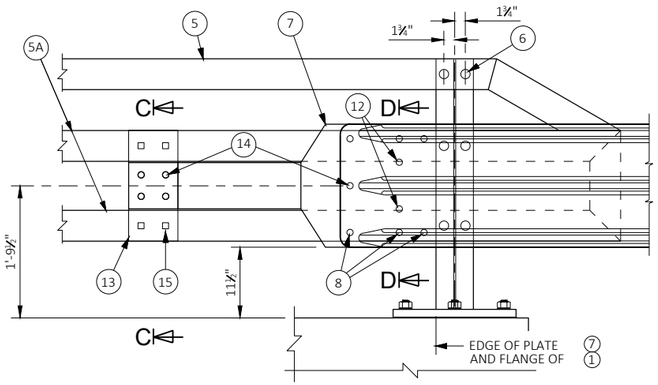


SECTION A-A



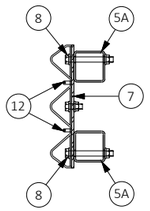
ANCHOR PLATE

AT BEAM GUARD ATTACHMENT

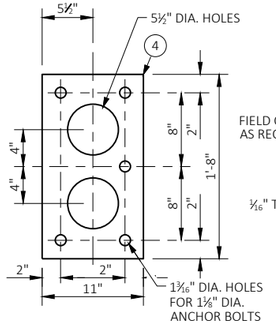


DETAIL AT END POST

THRIE BEAM RAIL ATTACHMENT

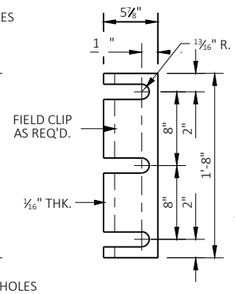


SECTION D-D

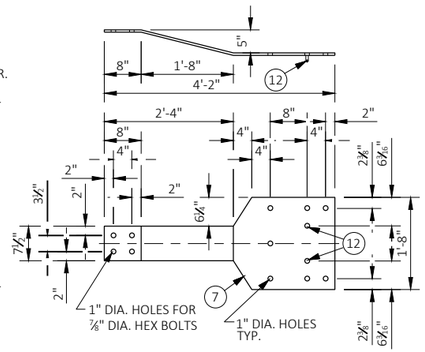


ANCHOR PLATE

AT RAIL TO DECK CONNECTION

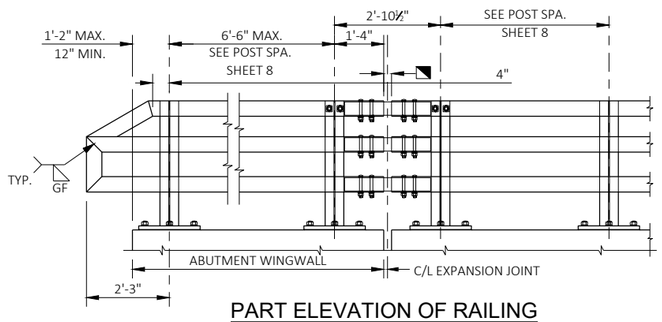


POST SHIM DETAIL



BACK-UP PLATE DETAIL

AT BEAM GUARD ATTACHMENT



PART ELEVATION OF RAILING

LEGEND

1. W6 x 25 WITH 1⅛" x 1½" HORIZ. SLOTS ON EACH SIDE OF POST FOR BOLT NO. 6. CUT BOTTOM OF POST TO MATCH CROSS SLOPE OF ROADWAY. PLACE POST VERTICAL. PLACE POSTS NORMAL TO GRADE LINE.
2. PLATE 1¼" x 11¾" x 1'-8" WITH 1¾16" x 1⅝" SLOTTED HOLES FOR ANCHOR BOLTS NO. 3. WELD TO NO. 1 AS SHOWN. SLOTS PARALLEL TO SHORT SIDE OF PLATE.
3. ASTM A449 - 1⅛" DIA. ANCHOR BOLTS WITH NUT AND HARDENED WASHER (ALL GALVANIZED). 5 REQ'D. PER POST. THREAD 3" & PLACE NORMAL TO PLATE NO. 2. CHAMFER TOP OF BOLTS BEFORE THREADING. USE 1'-9" LONG IN ABUTMENT WINGS. AT POSTS ON CONCRETE SLAB SUPERSTRUCTURES WHERE THE SLAB THICKNESS IS > 16" USE 1'-3" LONG. USE 10¾" LONG AT ALL OTHER LOCATIONS. (AN EQUIVALENT THREADED ROD WITH NUTS AND HARDENED WASHERS MAY BE SUBSTITUTED FOR ANCHOR BOLTS IN WINGS IF REQ'D. FOR CONSTRUCTIBILITY.)
4. ¾" x 11" x 1'-8" ANCHOR PLATE (GALVANIZED) WITH 1¾16" DIA. HOLES FOR ANCHOR BOLTS NO. 3
5. TS 5 x 4 x 0.25 STRUCTURAL TUBING. ATTACH TO NO. 1 WITH NO. 6.
5A. TS 5 x 5 x 0.25 STRUCTURAL TUBING. ATTACH TO NO. 1 WITH NO. 6.
6. ⅞" DIA. A325 SLOTTED ROUND HEAD BOLT WITH NUT, 3⁄16" x 1⅝" x 1⅝" WASHER, AND LOCK WASHER (2 REQ'D. AT EACH RAIL TO POST LOCATION.)
7. ½" THICK BACK-UP PLATE WITH 2 - ⅞" x 1½" THREADED SHOP WELDED STUDS (NO. 12). BOLT TO RAIL AS SHOWN IN DETAIL. REQUIRED AT THRIE BEAM GUARD RAIL ATTACHMENTS ONLY. PLACE SYMMETRICALLY ABOUT TUBES NO. 5A.
8. 1" DIA. HOLES IN PLATE NO. 7 & TUBES NO. 5A FOR ⅞" DIA. A325 BOLTS WITH HEX NUTS AND WASHERS. 6 HOLES IN TUBES AND PLATE NO. 7.
9. SPLICE SLEEVE FABRICATED FROM ¼" PLATE. PROVIDE "SLIDING FIT".
10. ⅜" x 3⅜" x 2' - 4" PLATE. 2 PER RAIL. USED IN NO. 5 & 5A.
10A. ⅜" x 2⅝" x 2' - 4" PLATE USED IN NO. 5, ⅜" x 3⅜" x 2' - 4" PLATE USED IN NO. 5A. 2 PER RAIL.
11. ⅞" DIA. A325 ROUND HEAD BOLT WITH NUT, WASHER, AND LOCK WASHER. USE 15⁄16" x 1¼" LONGIT. SLOTTED HOLES AT FIELD JOINTS AND 15⁄16" x 2¼" MIN. LONGITUDINAL SLOTTED HOLES AT EXPOSED JOINTS IN PLATE NO. 10A.
12. ⅞" DIA. x 1½" LONG THREADED SHOP WELDED STUDS (2 REQ'D).
13. ⅜" x 8" x 1'-6" PLATE. BOLT TO RAIL AS SHOWN IN DETAIL. REQ'D. AT THRIE BEAM GUARD RAIL ATTACHMENTS ONLY. PLACE SYM. ABOUT TUBES NO. 5A.
14. ⅞" DIA. x 2" LONG A325 HEX BOLT WITH NUT AND WASHER (5 REQ'D.).
15. 1" DIA. HOLES IN TUBES NO. 5A FOR ⅞" DIA. A325 ROUND HEAD BOLT WITH NUT, WASHER AND LOCK WASHER (4 REQ'D.). 4 HOLES IN TUBES.

GENERAL NOTES

1. BID ITEM SHALL BE "RAILING TUBULAR TYPE M B-10-0257" WHICH INCLUDES ALL ITEMS SHOWN.
2. RAIL POST AND BASE PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A709 GRADE 50. HOLLOW RAILING STRUCTURAL TUBING SHALL CONFORM TO THE REQUIREMENTS OF ASTM A500 GRADE B OR C WITH A CERTIFIED FY = 50 ksi. ANCHOR PLATES, AND SPLICE TUBE PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A709 GRADE 36.
3. THE NUT SECURING THE POST BASE PLATE TO THE CONCRETE SHALL BE TIGHTENED TO A SNUG FIT AND GIVEN AN ADDITIONAL ⅛ TURN.
4. RAILS SHALL BE CONTINUOUS OVER A MINIMUM OF THREE (3) POSTS WITHOUT SPLICES WHERE POSSIBLE. RAILS SHALL BE SPLICED IN A PANEL OVER EXPANSION JOINTS.
5. ENDS OF TUBE SECTIONS SHALL BE SAWED. GRIND SMOOTH EXPOSED EDGES. ALL CUT ENDS SHALL BE TRUE AND SMOOTH.
6. WELD IS THE SAME ON BOTH FLANGES. FLANGE WELD DOES NOT REQUIRE MAGNETIC PARTICLE TESTING.
7. FILL BOLT SLOT OPENINGS IN POST SHIMS AND PLATE NO. 2 AND CAULK AROUND PERIMETER OF PLATE NO. 2 WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. STEEL POST SHIMS MAY BE USED UNDER POSTS WHERE REQ'D. FOR ALIGNMENT.
8. POST BASE PLATES SHALL BE FLAT WITH ALL SURFACES SMOOTH AND FREE FROM WARP AND ALL EDGES SMOOTH, STRAIGHT AND VERTICAL. ALL PLATE CUTS SHALL BE MACHINE OR MACHINE FLAME CUT.
9. ALL MATERIAL SHALL BE GALVANIZED AFTER FABRICATION. PRIOR TO GALVANIZING, ALL STEEL RAILING POSTS & STEEL TUBING SHALL BE GIVEN A NO. 6 BLAST CLEANING BY SSPC SPECIFICATIONS.
10. THIS RAILING MEETS NCHRP REPORT 350 EVALUATION CRITERIA FOR TEST LEVEL 4 (TL-4).

▲ TIE TO TOP MAT OF STEEL.

* FOR ANCHOR BOLTS IN WINGS, TACK WELD MAY BE USED IN FIELD AFTER ANCHOR PLATE IS IN POSITION IF REQ'D. FOR CONSTRUCTIBILITY.

◥ RDWY. OPENING OR 2½" MIN. FOR STRIP SEAL EXP. JOINT & ½" OPENING FOR A1 ABUTMENT.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-10-257

DRAWN BY NJT | PLANS CK'D TLP

RAILING TUBULAR TYPE M

SHEET 9 OF 9

39

SCALE =

DIVISION -1- MAIN STREET

STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY) (UNADJUSTED)		CUMULATIVE VOL (CY)		
			CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
							1.00	1.25	
					NOTE 1	NOTE 3	NOTE 1		NOTE 8
09+25	925.00	0.00	13.33	0.01	0	0	0	0	0
09+40	940.00	15.00	21.90	11.69	10	3	10	4	6
09+50	950.00	10.00	24.30	19.42	9	6	19	11	8
09+60	960.00	10.00	22.46	20.98	9	7	28	20	8
09+70	970.00	10.00	20.10	43.69	8	12	36	35	1
09+77.24	977.24	7.24	65.07	10.64	11	7	47	44	3
09+77.25	977.25	0.01	0.00	0.00	0	0	47	44	3
10+18.75	1018.75	41.50	0.00	0.00	0	0	47	44	3
10+18.76	1018.76	0.01	45.83	23.82	0	0	47	44	3
10+25	1025.00	6.24	21.15	46.41	8	8	55	54	1
10+40	1040.00	15.00	17.68	24.38	11	20	66	79	-13
10+50	1050.00	10.00	16.51	22.80	6	9	72	90	-18
10+60	1060.00	10.00	16.00	0.58	6	4	78	95	-17
10+71	1071.00	11.00	12.43	0.00	6	0	84	95	-11
				TOTAL	84	76			

Notes:	
1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL
3 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME
8 - MASS ORDINATE	[(CUT) - ((FILL) * FILL FACTOR)]

9

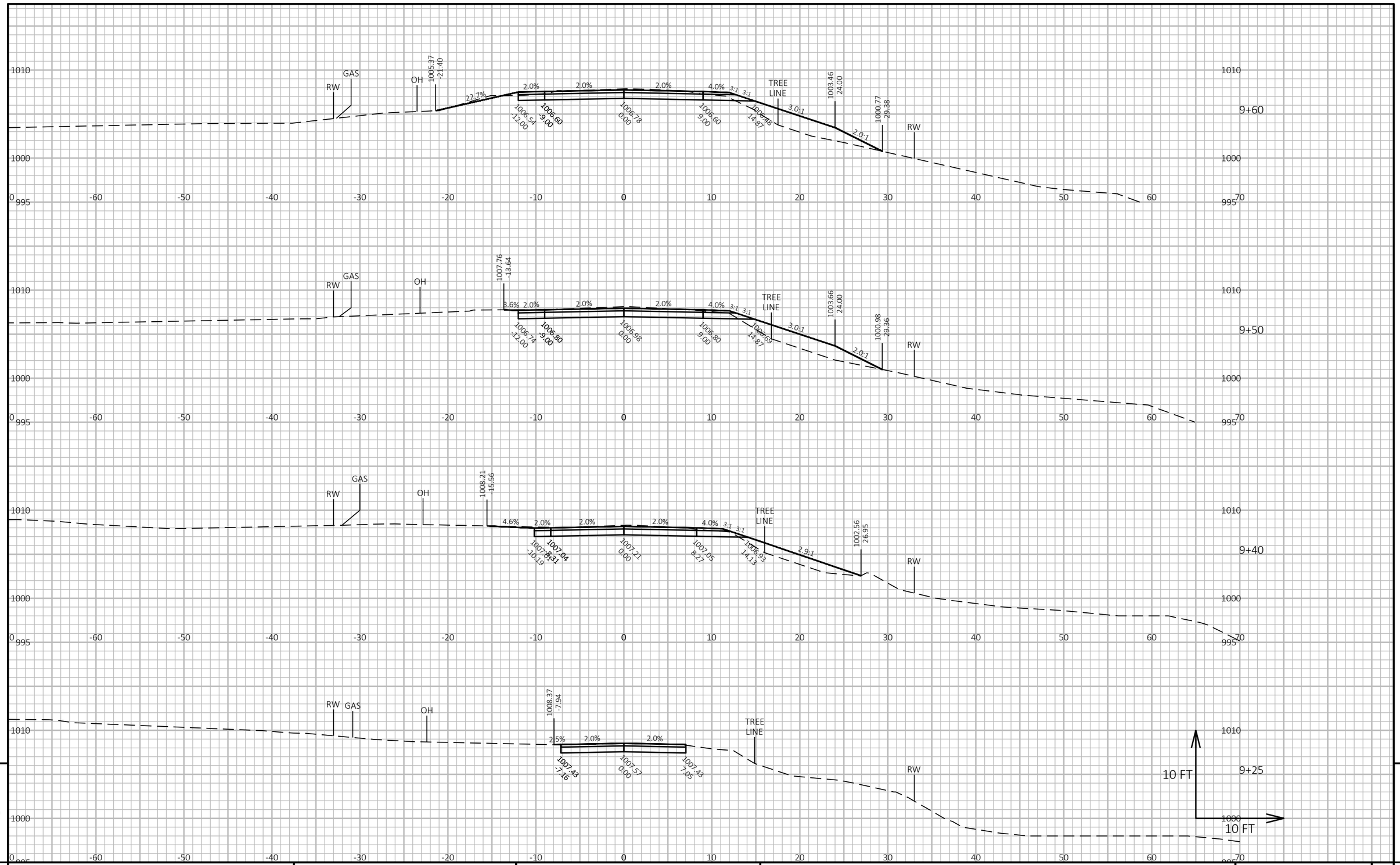
9

PROJECT NO: 7852-00-71	HWY: MAIN STREET	COUNTY: CLARK	EARTHWORK DATA	SHEET 40	E

FILE NAME : I:\CLIENTS-MENO\W\W3900 WDOT NW REGION - EAU CLAIRE\031 7852-00-01 T MENTOR E FORK HALLS CREEK P-10-0196 LOCAL STREET CLARK COUNTY\04 CADD\WDOT CADD PROJECT NOTES\SHEETS\PLAN\07202024-EW.DWG
LAYOUT NAME - 01

PLOT BY : JORDAN DISTERHAFT PLOT NAME : PLOT SCALE : 1" = 1'

WISDOT/CADDS SHEET 49



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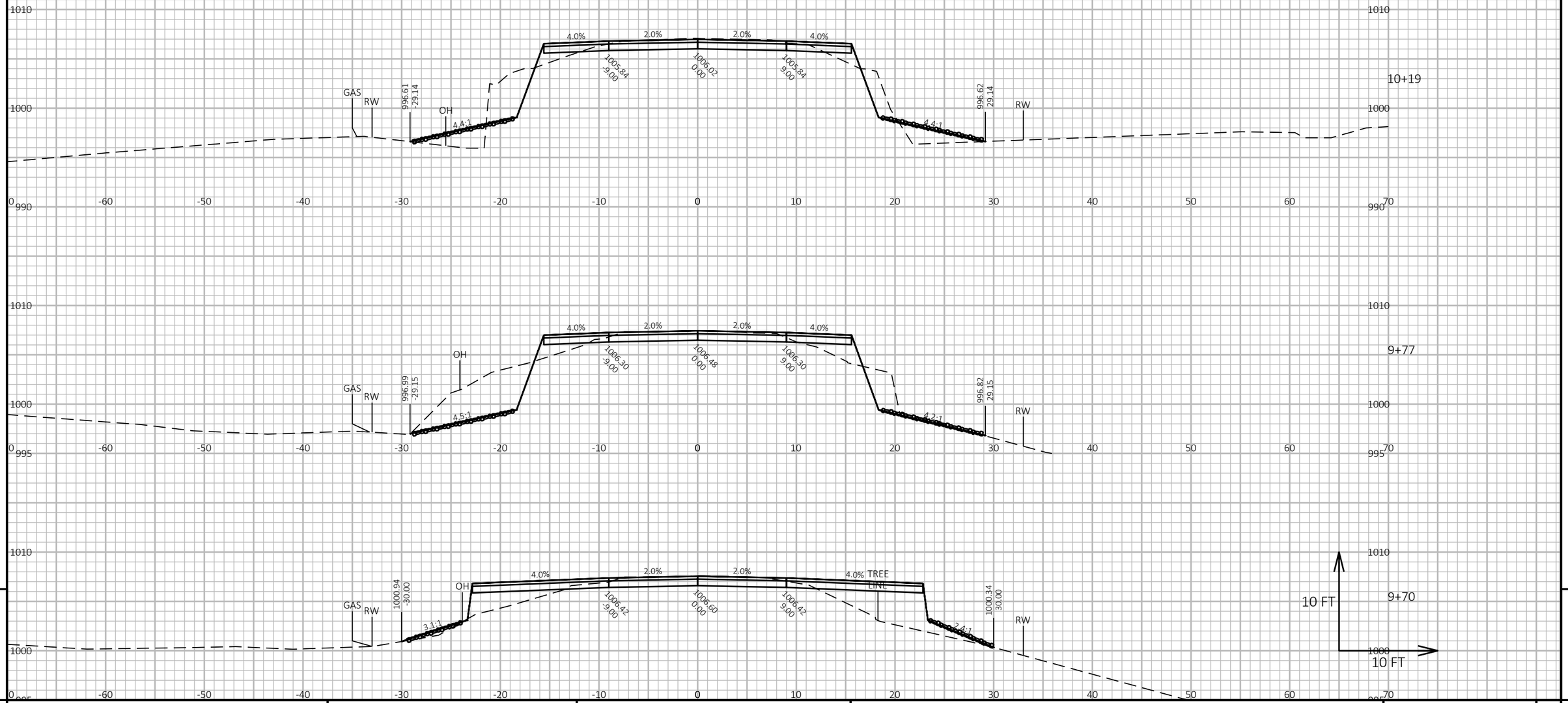
9

PROJECT NO: 7852-00-01	HWY: MAIN STREET	COUNTY: CLARK	CROSS SECTIONS: MAIN STREET	SHEET 41	E

FILE NAME : I:\CLIENTS-MENO\W\W3900 WDOT NW REGION - EAU CLAIRE\031 7852-00-01 T MENTOR E FORK HALLS CREEK P-10-0196 LOCAL STREET CLARK COUNTY\04 CADD\WDOT CADD PROJECT NOTE BASE SHEETS\PLAN\07-2024-8512.DWG
LAYOUT NAME - XS-1

PLOT BY : JORDAN DISTERHAFT PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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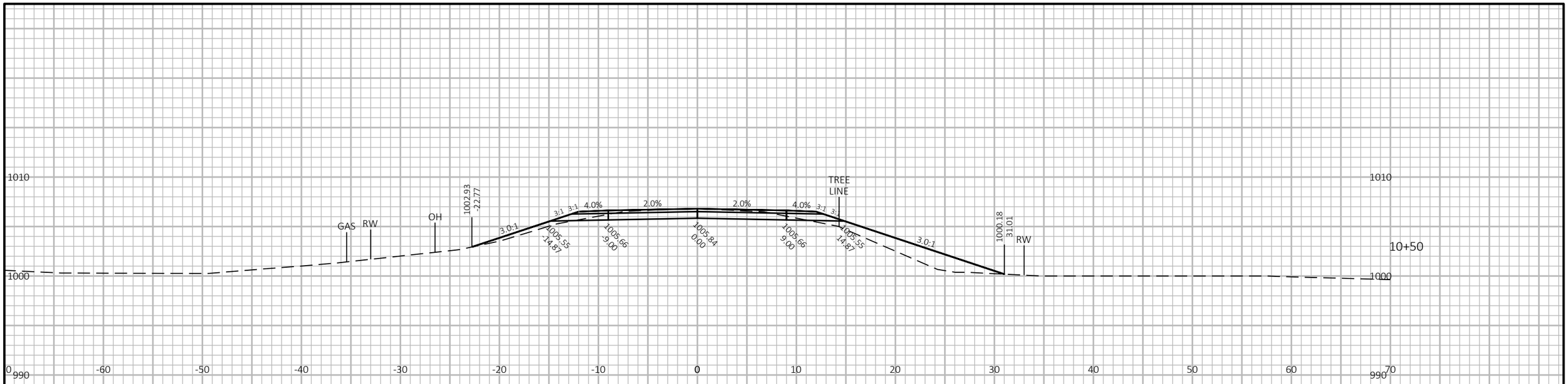
9

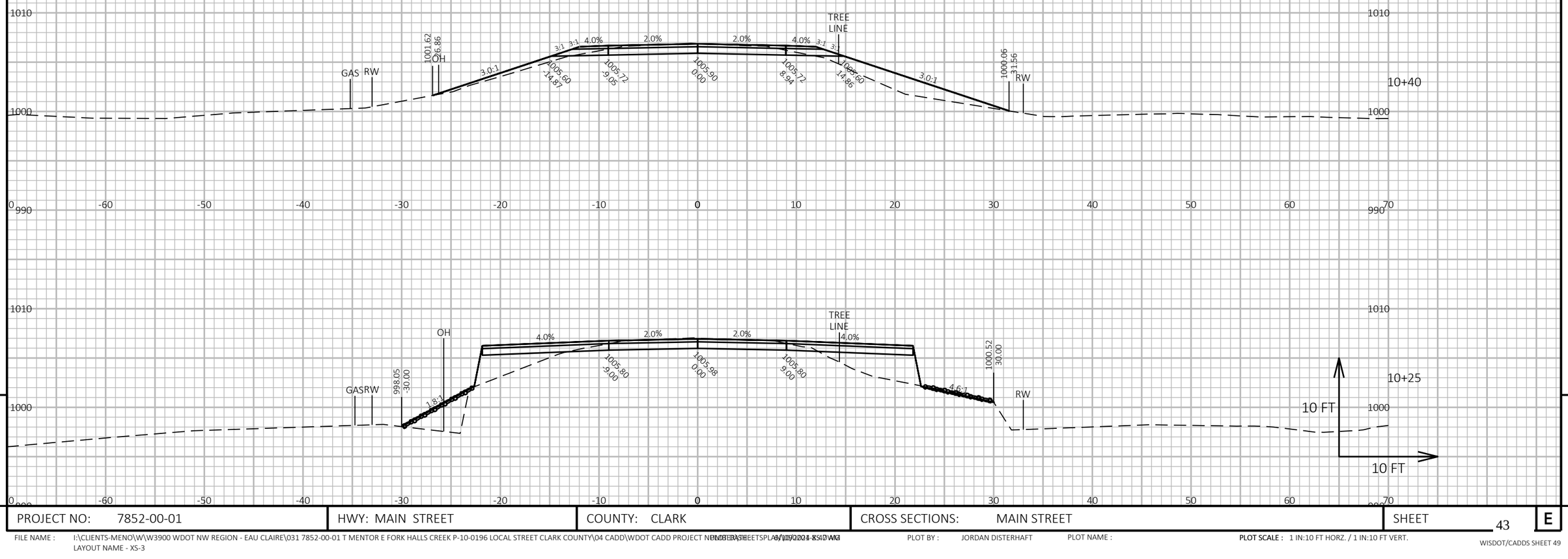
PROJECT NO: 7852-00-01	HWY: MAIN STREET	COUNTY: CLARK	CROSS SECTIONS: MAIN STREET	SHEET 42	E

FILE NAME : I:\CLIENTS-MENO\W\W3900 WDOT NW REGION - EAU CLAIRE\031 7852-00-01 T MENTOR E FORK HALLS CREEK P-10-0196 LOCAL STREET CLARK COUNTY\04 CADD\WDOT CADD PROJECT NOTEBOOK\SHEETS\PLAN\07202024 8:42 AM
LAYOUT NAME - XS-2

PLOT BY : JORDAN DISTERHAFT PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49





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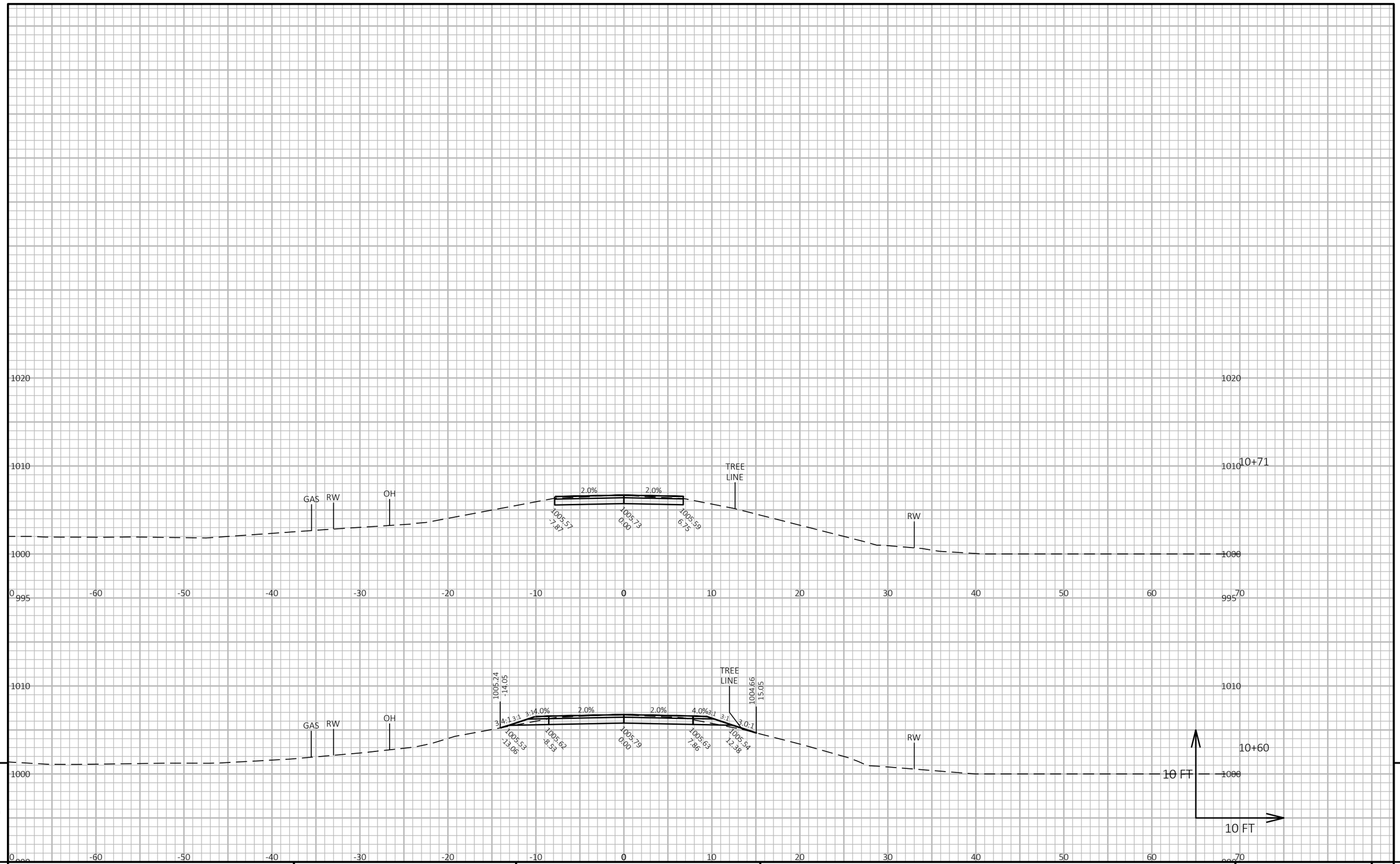
9

PROJECT NO: 7852-00-01	HWY: MAIN STREET	COUNTY: CLARK	CROSS SECTIONS: MAIN STREET	SHEET 43	E

FILE NAME : I:\CLIENTS-MENO\W\W3900 WDOT NW REGION - EAU CLAIRE\031 7852-00-01 T MENTOR E FORK HALLS CREEK P-10-0196 LOCAL STREET CLARK COUNTY\04 CADD\WDOT CADD PROJECT NOTE BASE SHEETS\PLAN SHEETS.DWG
LAYOUT NAME - XS-3

PLOT BY : JORDAN DISTERHAFT PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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9

PROJECT NO: 7852-00-01	HWY: MAIN STREET	COUNTY: CLARK	CROSS SECTIONS: MAIN STREET	SHEET 44	E

FILE NAME : I:\CLIENTS-MENO\W\W3900 WDOT NW REGION - EAU CLAIRE\031 7852-00-01 T MENTOR E FORK HALLS CREEK P-10-0196 LOCAL STREET CLARK COUNTY\04 CADD\WDOT CADD PROJECT NOTE BASE SHEETS\PLAN SHEETS.DWG
LAYOUT NAME - XS-4

PLOT BY : JORDAN DISTERHAFT PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49

Notes

Wisconsin Department of Transportation

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