LAX

PROJECT ID: 5991-02-71

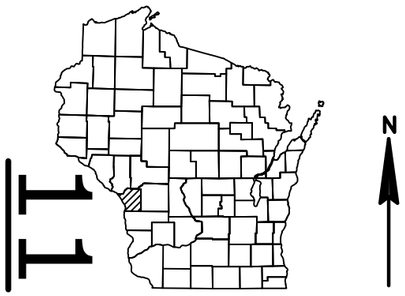
WITH: N/A

COUNTY: LA CROSSE

FEBRUARY 2025

ORDER OF SHEETS

TOTAL SHEETS = 116



N

DESIGN DESIGNATION 5991-02-70

A.A.D.T.	2025	=	10890
A.A.D.T.	2045	=	19965
D.H.V.		=	___
D.D.		=	___
T.		=	6.6%
DESIGN SPEED		=	30 MPH
ESALS		=	N/A

CONVENTIONAL SYMBOLS

PLAN

- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE (300'EB')
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE

- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES

- ELECTRIC (E)
- FIBER OPTIC (FO)
- GAS (G)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

CITY OF ONALASKA, THEATER ROAD

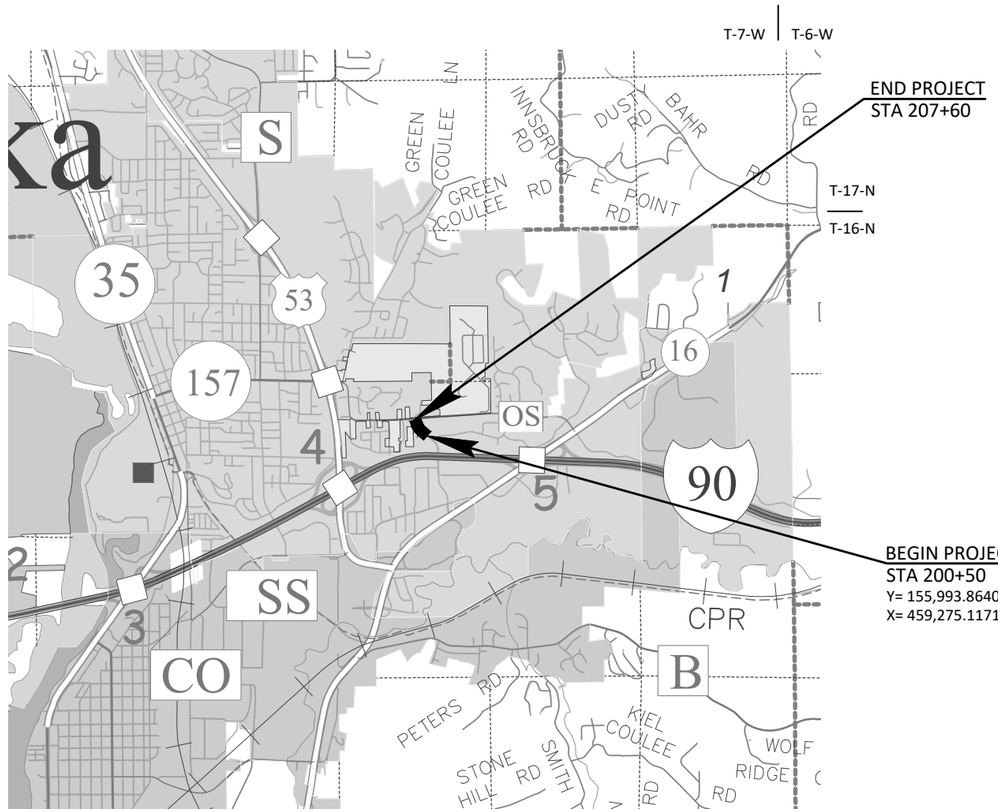
CTH OS TO MIDWEST DRIVE

LOC STR

LA CROSSE COUNTY

STATE PROJECT NUMBER
5991-02-71

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5991-02-71	WISC 2025310	1



LAYOUT

SCALE 0 1 MI

TOTAL NET LENGTH OF CENTERLINE = 0.134 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), LA CROSSE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 18.

ACCEPTED FOR

CITY OF ONALASKA

9-16-24 (Date) (City Engineer Signature)

ORIGINAL PLANS PREPARED BY

Short Elliott Hendrickson Inc.
329 Jay Street, Suite 301
La Crosse, WI 54601-4007
608.782.3161 main |
888.908.8166 fax
www.sehinc.com
Building a Better World for All of Us

WISCONSIN PROFESSIONAL ENGINEER
TOREY R. LEONARD
E-42982-6
HOLMEN, WI

9/16/2024 (Date) (Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	SEH
Designer	SEH
Project Manager	JOSH SCHOENMANN
Regional Examiner	SW REGION
Regional Supervisor	KYLE HEMP

APPROVED FOR THE DEPARTMENT

DATE: 10/25/24 (Signature)

E

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 01 TITLE\010101-TI.DWG PLOT DATE : 9/16/2024 9:15 AM PLOT BY : TOREY LEONARD PLOT NAME :

STANDARD ABBREVIATIONS

ABUT	ABUTMENT
AC	ACRE
AGG	AGGREGATE
AECPRC	APRON ENDWALL FOR CULVERT PIPE REINFORCED CONCRETE
AECPCS	APRON ENDWALL FOR CULVERT PIPE CORRUGATED STEEL
ASPH	ASPHALTIC
AVG	AVERAGE
ADT	AVERAGE DAILY TRAFFIC
BAD	BASE AGGREGATE DENSE
BF	BACK FACE
BM	BENCH MARK
BR	BRIDGE
CE	COMMERCIAL ENTRANCE
C/L	CENTER LINE
Δ	CENTRAL ANGLE OR DELTA
COB	CENTER OF BARRIER
CONC	CONCRETE
CPRC	CULVERT PIPE REINFORCED CONCRETE
CPRCHE	CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL
CR	CREEK
CY	CUBIC YARD
C&G	CURB AND GUTTER
D	DEGREE OF CURVE
DHV	DESIGN HOUR VOLUME
DISCH	DISCHARGE
DG	DITCH GRADE
DWY	DRIVEWAY
X	EAST GRID COORDINATE
EAT	STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL
EOR	END POINT OF RADIUS
EL	ELEVATION
ENT	ENTRANCE
ESALS	EQUIVALENT SINGLE AXLE LOADS
EXC	EXCAVATION
EBS	EXCAVATION BELOW SUBGRADE
EXIST	EXISTING
FC	FACE OF CURB
FF	FACE TO FACE
FERT	FERTILIZE
FE	FIELD ENTRANCE
FL	FLOW LINE
FO	FIBER OPTIC
CWT	HUNDREDWEIGHT
HYD	HYDRANT
ID	INSIDE DIAMETER
INV	INVERT
IP	IRON PIPE ON PIN
LHF	LEFT-HAND FORWARD
L	LENGTH OF CURVE
LF	LINEAR FOOT
LC	LONG CHORD OF CURVE
LS	LUMP SUM
MH	MANHOLE
MOR	MID POINT OF RADIUS
NC	NORMAL CROWN
NO	NUMBER
OBLIT	OBLITERATE
PAVT	PAVEMENT
PE	PRIVATE ENTRANCE
PVRC	POINT OF VERTICAL REVERSE CURVE
QOR	QUARTER POINT OF RADIUS
R	RADIUS
REQ'D	REQUIRED
RES	RESIDENCE OR RESIDENTIAL
RHF	RIGHT-HAND FORWARD
R/W	RIGHT-OF-WAY
R	RIVER
RDWY	ROADWAY
R/L	REFERENCE LINE
SALV	SALVAGED
SAN	SANITARY SEWER
SF	SQUARE FEET
SY	SQUARE YARD
SDD	STANDARD DETAIL DRAWINGS
STA	STATION
SS	STORM SEWER
SSPRC	STORM SEWER PIPE REINFORCED CONCRETE
SE	SUPERELEVATION RATE
TC	TOP OF CURB
T OR TN	TOWN
T	TRUCKS (PERCENT OF)
TYP	TYPICAL
VAR	VARIABLE
VC	VERTICAL CURVE
Y	NORTH GRID COORDINATE
YD	YARD

RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP- TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE- TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = 1.9 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES =0.05 ACRES

WISDOT CONTACT:

WISCONSIN DEPT OF TRANSPORTATION
SOUTHWEST REGION
2101 WRIGHT STREET
MADISON, WI 53704
TELEPHONE: (608) 246-5448
ATTENTION: JOSH SCHOENMANN
LOCAL PROGRAM PROJECT MANAGER
EMAIL: JOSH.SCHOENMANN@DOT.WI.GOV

DNR AREA LIAISON:

WI DEPT OF NATURAL RESOURCES
DNR SERVICE CENTER
3550 MORMON COULEE RD
LA CROSSE, WI 54601
TELEPHONE: 608.785.9115
ATTENTION: KAREN KALVELAGE
EMAIL: KAREN.KALVELAGE@WISCONSIN.GOV

OTHER FACILITIES CONTACT LIST:

KEVIN SCHUBERT
CITY OF ONALASKA - ROAD FACILITY
415 MAIN ST
ONALASKA, WI 54650
(608) 781-9537
KSCHUBERT@CITYOFONALASKA.COM

KEVIN SCHUBERT
CITY OF ONALASKA - STREET LIGHTING
415 MAIN ST
ONALASKA, WI 54650
(608) 781-9537
KSCHUBERT@CITYOFONALASKA.COM

CITY CONTACT:

CITY OF ONALASKA
415 MAIN STREET
ONALASKA, WI 54650
TELEPHONE: (608) 781-9537
ATTN: KEVIN SCHUBERT
CITY ENGINEER
KSCHUBERT@CITYOFONALASKA.COM

DESIGN CONTACT:

SEH INC.
329 JAY STREET, SUITE 301
LA CROSSE, WI 54601
TELEPHONE: 608.498.4019
ATTENTION: TOREY LEONARD, P.E.
PROJECT MANAGER
EMAIL: TLEONARD@SEHINC.COM

GENERAL NOTES:

1. NO TREES OR SHRUBS SHALL BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.
2. THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.
3. THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY LOCAL MUNICIPAL UTILITY WHICH IS NOT A MEMBER OF THE DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.
4. TOPSOIL SHALL BE PLACED WITH 4-INCH TYPICAL DEPTH.
5. TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
6. REMOVAL OF EROSION CONTROL DEVICES IS INCLUDED IN THE COST OF THEIR RESPECTIVE BID ITEMS.
7. THE EROSION CONTROL FEATURES AS SHOWN IN THE PLANS ARE AT SUGGESTED LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.
8. ASPHALTIC AND CONCRETE SURFACES SHALL BE SAWCUT AT THE MATCH LINE AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER.
9. DISTURBED AREAS WITHIN THE RIGHT OF WAY SHALL BE TOP SOILED, FERTILIZED AND SEEDED.
10. A CONVERSION FACTOR OF 2.0 TONS/CY IS USED TO ESTIMATE QUANTITIES FOR BASE AGGREGATE DENSE 1 1/4-INCH.
11. APPLY TACK COAT AT A RATE OF 0.07 GAL/SY TO MILLED SURFACE AND 0.05 GAL/SY BETWEEN LAYERS OF HMA PAVEMENT.
12. HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN.
13. THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN AND TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, PASSING OR PARKING LANE.
14. AERIAL PHOTOS MAY NOT REPRESENT ACTUAL FIELD CONDITIONS, CONTRACTORS SHALL VERIFY EXISTING CONDITIONS.

UTILITY CONTACT LIST:

BRIAN STELPLUGH
BRIGHTSPEED OF WISCONSIN, LLC - COMMUNICATION LINE
1905 WARD AVENUE S
LA CROSSE, WI 54601
(980)376-1557
BRIAN.STELPLUGH@BRIGHTSPEED.COM

ANDREW DELEEUW
SPECTRUM - COMMUNICATION LINE
1228 12TH AVE S
ONALASKA, WI 54650
(715) 519-0033
ANDREW.DELEEUW@CHARTER.COM

BEN GRILLEY
LEMONWEIR VALLEY TELEPHONE
127 US HWY 12/16
CAMP DOUGLAS, WI 54618
(608) 427-3438
BEN.GRILLEY@GETLYNXX.COM

JARROD HOLTER
CITY OF ONALASKA - SEWER
415 MAIN ST
ONALASKA, WI 54650
(608) 781-9537
JHOLTER@CITYOFONALASKA.COM

JARROD HOLTER
CITY OF ONALASKA - WATER
415 MAIN ST
ONALASKA, WI 54650
(608) 781-9537
JHOLTER@CITYOFONALASKA.COM

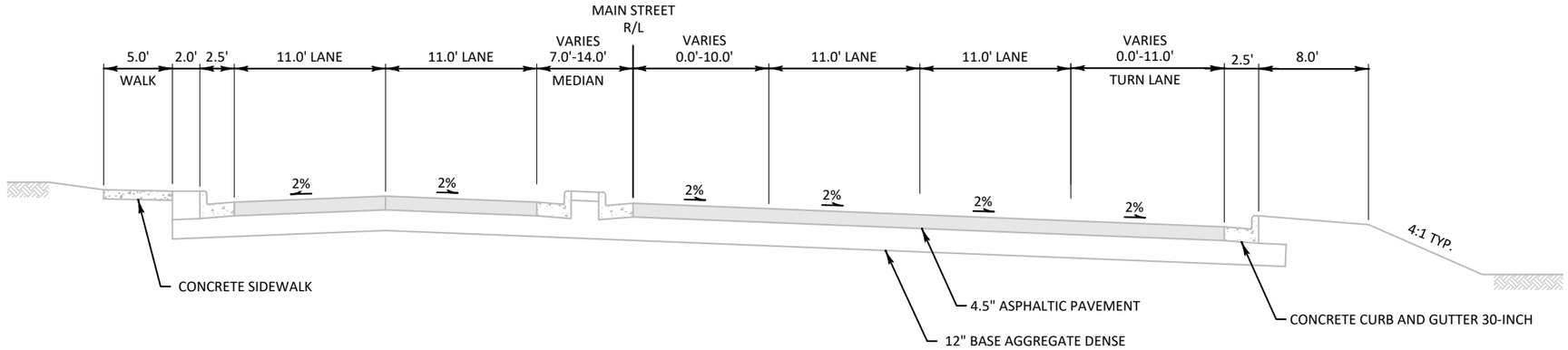
RUSS KENNY
TDS TELECOM - COMMUNICATION LINE
525 JUNCTION RD
MADISON, WI 53717
(715) 391-9644
RUSS.KENNY@TDSTELECOM.COM

TARAN WELCHLIN
METRONET - COMMUNICATION LINE
149 CAUSEWAY BLVD
LA CROSSE, WI 54603
(608)606-2043
TARAN.WELCHLIN@METRONET.COM

DAWN SCHULTZ
XCEL ENERGY - ELECTRICITY
3215 COMMERCE STREET
LA CROSSE, WI 54603
(608)789-3628
DAWN.SCHULTZ@XCELENERGY.COM

DAWN SCHULTZ
XCEL ENERGY - GAS/PETROLEUM
3215 COMMERCE STREET
LA CROSSE, WI 54603
(608)789-3681
DAWN.SCHULTZ@XCELENERGY.COM

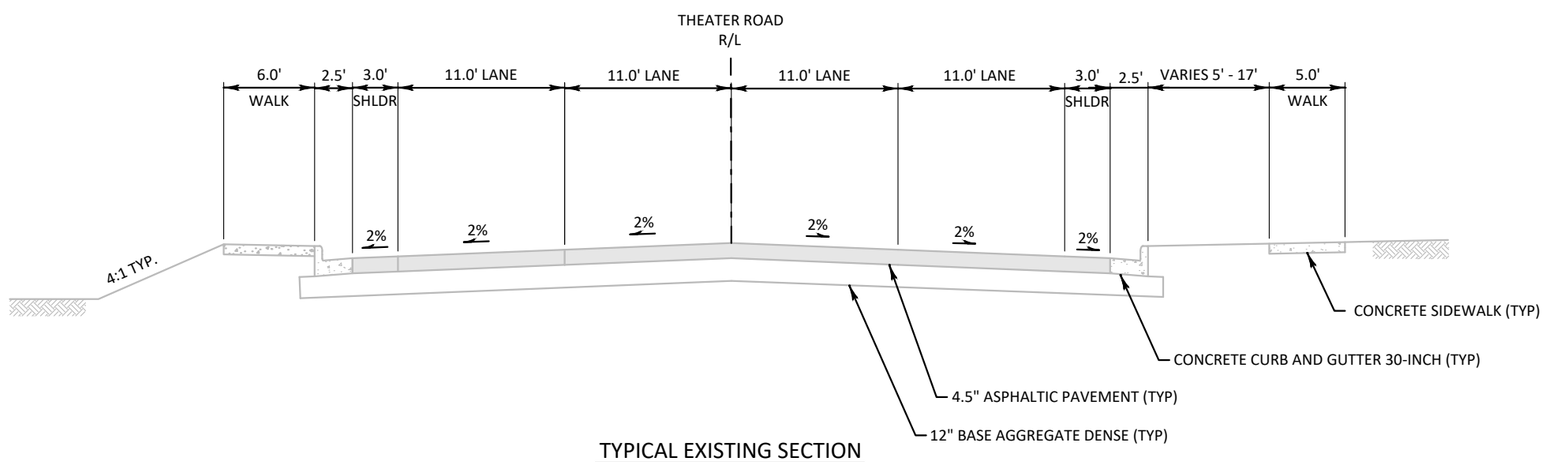
DIGGERS HOTLINE
Dial 811 or (800)242-8511
www.DiggersHotline.com



TYPICAL EXISTING SECTION

MAIN ST

STA 100+00'OS' TO STA 103+75'OS'

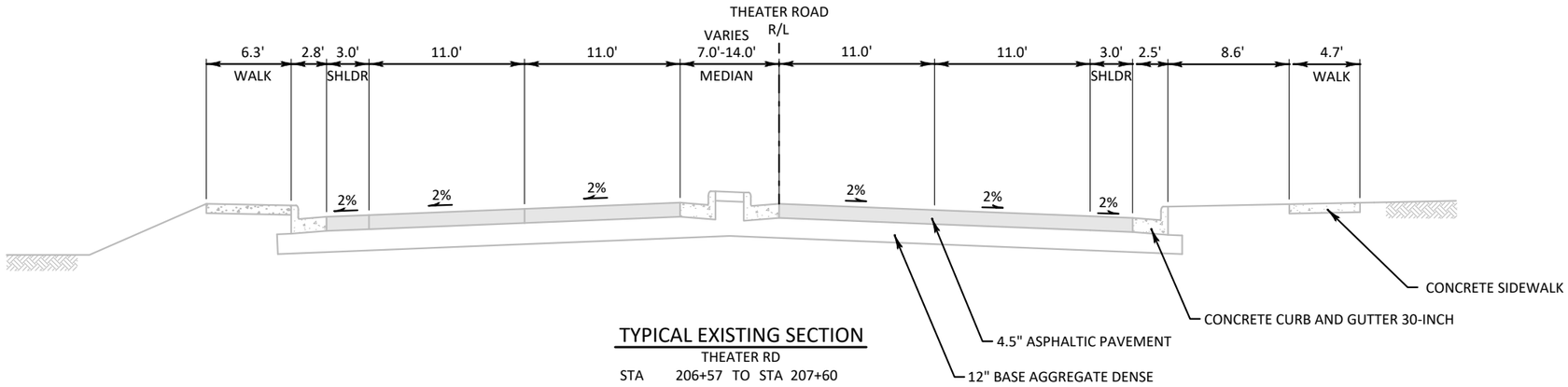


TYPICAL EXISTING SECTION

THEATER RD

STA 200+50'TR' TO STA 206+57'TR'

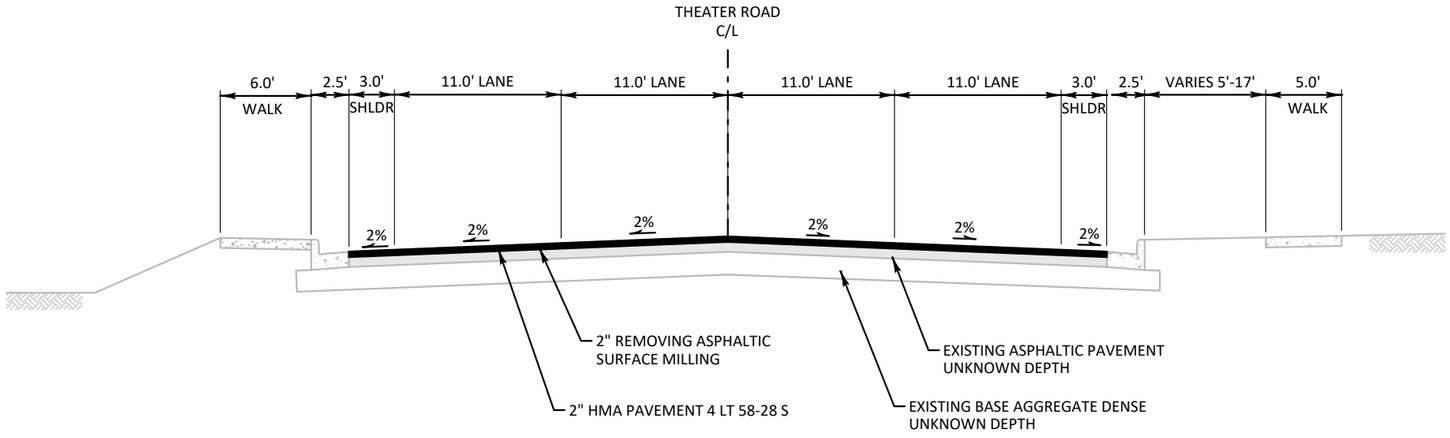
PROJECT NO: 5991-02-71	HWY: THEATER ROAD	COUNTY: LA CROSSE	TYPICAL SECTIONS	SHEET 3 E



TYPICAL EXISTING SECTION

THEATER RD

STA 206+57 TO STA 207+60



TYPICAL PROPOSED SECTION

THEATER RD

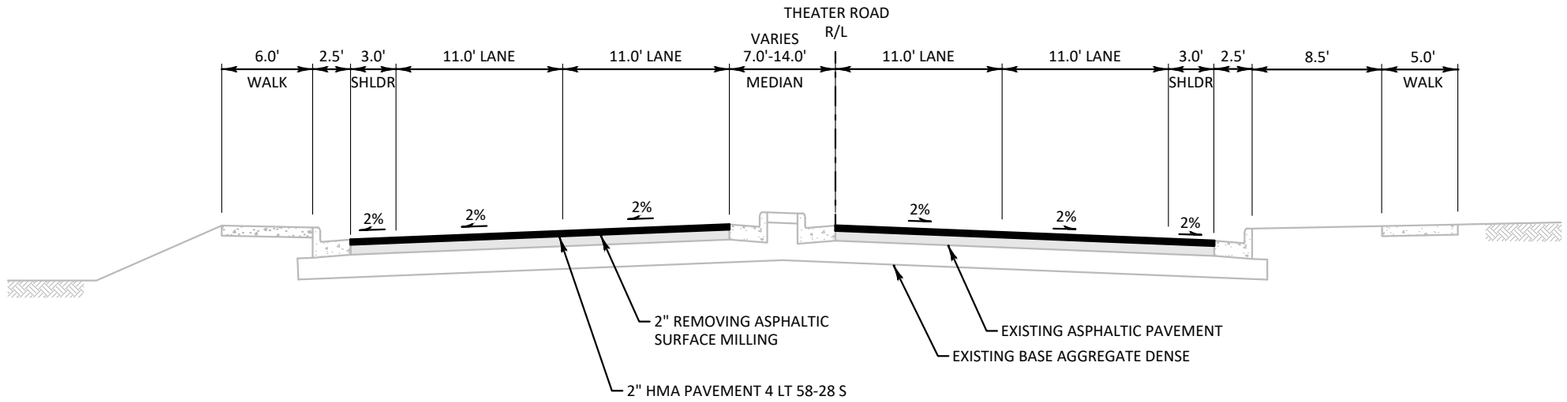
STA 200+50 TO STA 206+57

PROJECT NO: 5991-02-71	HWY: THEATER ROAD	COUNTY: LA CROSSE	TYPICAL SECTIONS	SHEET 4 E

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 02 TYP SEC & DETAILS\020300-TS (TYPICAL SECTIONS).DWG
LAYOUT NAME - 02

PLOT DATE : 10/4/2024 11:21 AM PLOT BY : BEN PIERCE PLOT NAME : PLOT SCALE : 1 IN:10 FT

2



TYPICAL PROPOSED SECTION

THEATER RD
STA 206+57 TO STA 207+60

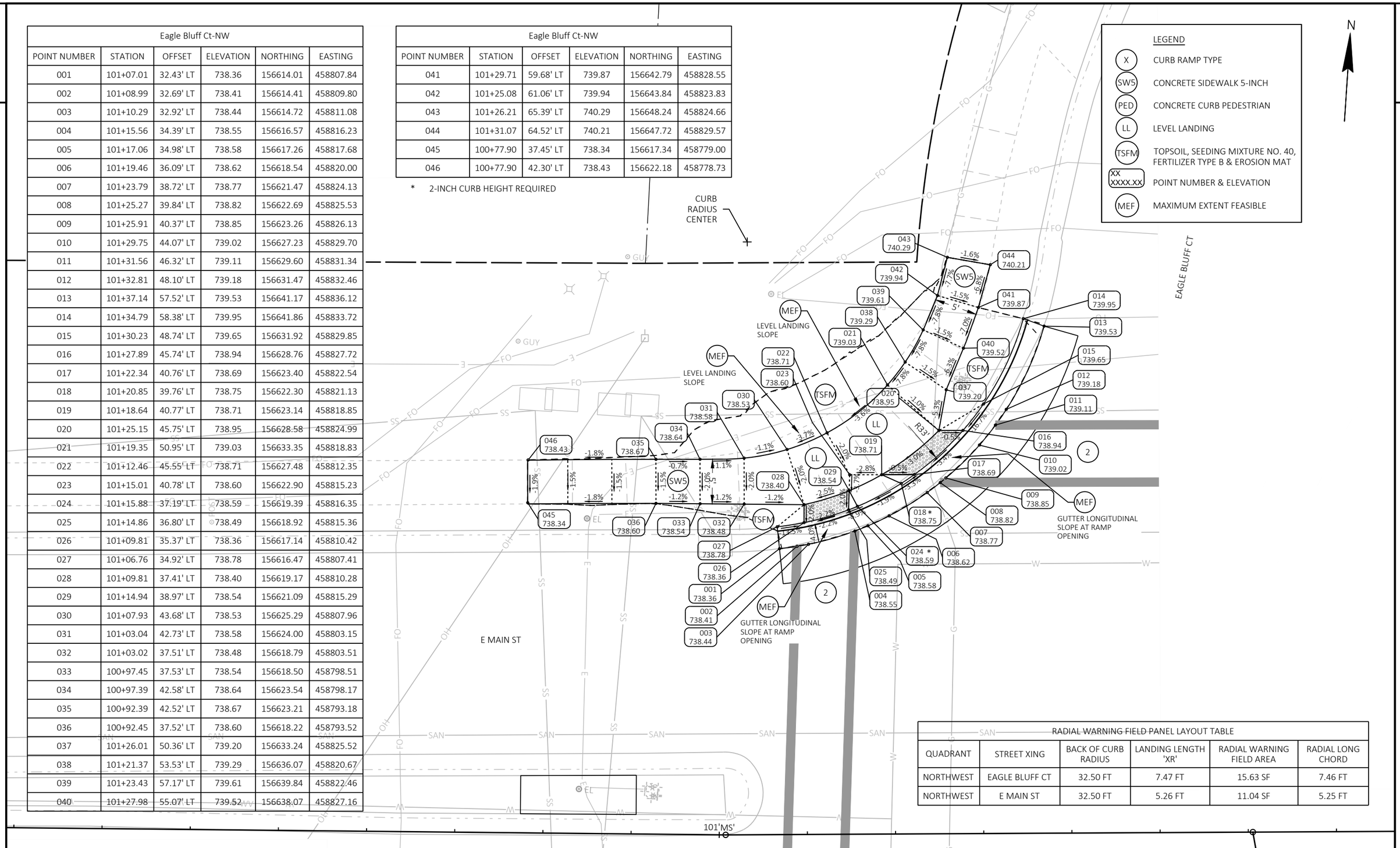
PROJECT NO: 5991-02-71	HWY: THEATER ROAD	COUNTY: LA CROSSE	TYPICAL SECTIONS	SHEET 5	E

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 02 TYP SEC & DETAILS\020300-TS (TYPICAL SECTIONS).DWG
LAYOUT NAME - 03
PLOT DATE : 10/4/2024 11:21 AM
PLOT BY : BEN PIERCE
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42

Eagle Bluff Ct-NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
001	101+07.01	32.43' LT	738.36	156614.01	458807.84
002	101+08.99	32.69' LT	738.41	156614.41	458809.80
003	101+10.29	32.92' LT	738.44	156614.72	458811.08
004	101+15.56	34.39' LT	738.55	156616.57	458816.23
005	101+17.06	34.98' LT	738.58	156617.26	458817.68
006	101+19.46	36.09' LT	738.62	156618.54	458820.00
007	101+23.79	38.72' LT	738.77	156621.47	458824.13
008	101+25.27	39.84' LT	738.82	156622.69	458825.53
009	101+25.91	40.37' LT	738.85	156623.26	458826.13
010	101+29.75	44.07' LT	739.02	156627.23	458829.70
011	101+31.56	46.32' LT	739.11	156629.60	458831.34
012	101+32.81	48.10' LT	739.18	156631.47	458832.46
013	101+37.14	57.52' LT	739.53	156641.17	458836.12
014	101+34.79	58.38' LT	739.95	156641.86	458833.72
015	101+30.23	48.74' LT	739.65	156631.92	458829.85
016	101+27.89	45.74' LT	738.94	156628.76	458827.72
017	101+22.34	40.76' LT	738.69	156623.40	458822.54
018	101+20.85	39.76' LT	738.75	156622.30	458821.13
019	101+18.64	40.77' LT	738.71	156623.14	458818.85
020	101+25.15	45.75' LT	738.95	156628.58	458824.99
021	101+19.35	50.95' LT	739.03	156633.35	458818.83
022	101+12.46	45.55' LT	738.71	156627.48	458812.35
023	101+15.01	40.78' LT	738.60	156622.90	458815.23
024	101+15.88	37.19' LT	738.59	156619.39	458816.35
025	101+14.86	36.80' LT	738.49	156618.92	458815.36
026	101+09.81	35.37' LT	738.36	156617.14	458810.42
027	101+06.76	34.92' LT	738.78	156616.47	458807.41
028	101+09.81	37.41' LT	738.40	156619.17	458810.28
029	101+14.94	38.97' LT	738.54	156621.09	458815.29
030	101+07.93	43.68' LT	738.53	156625.29	458807.96
031	101+03.04	42.73' LT	738.58	156624.00	458803.15
032	101+03.02	37.51' LT	738.48	156618.79	458803.51
033	100+97.45	37.53' LT	738.54	156618.50	458798.51
034	100+97.39	42.58' LT	738.64	156623.54	458798.17
035	100+92.39	42.52' LT	738.67	156623.21	458793.18
036	100+92.45	37.52' LT	738.60	156618.22	458793.52
037	101+26.01	50.36' LT	739.20	156633.24	458825.52
038	101+21.37	53.53' LT	739.29	156636.07	458820.67
039	101+23.43	57.17' LT	739.61	156639.84	458822.46
040	101+27.98	55.07' LT	739.52	156638.07	458827.16

Eagle Bluff Ct-NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
041	101+29.71	59.68' LT	739.87	156642.79	458828.55
042	101+25.08	61.06' LT	739.94	156643.84	458823.83
043	101+26.21	65.39' LT	740.29	156648.24	458824.66
044	101+31.07	64.52' LT	740.21	156647.72	458829.57
045	100+77.90	37.45' LT	738.34	156617.34	458779.00
046	100+77.90	42.30' LT	738.43	156622.18	458778.73

* 2-INCH CURB HEIGHT REQUIRED



RADIAL WARNING FIELD PANEL LAYOUT TABLE					
QUADRANT	STREET XING	BACK OF CURB RADIUS	LANDING LENGTH 'XR'	RADIAL WARNING FIELD AREA	RADIAL LONG CHORD
NORTHWEST	EAGLE BLUFF CT	32.50 FT	7.47 FT	15.63 SF	7.46 FT
NORTHWEST	E MAIN ST	32.50 FT	5.26 FT	11.04 SF	5.25 FT

PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	CURB RAMP DETAILS	SHEET 6	E

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 02 TYP SEC & DETAILS\021300-CR (CURB RAMP DETAILS).DWG
LAYOUT NAME - 01-Eagle Bluff Ct-NW
PLOT DATE : 10/17/2024 2:50 PM PLOT BY : BEN PIERCE PLOT NAME : PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42

Eagle Bluff Ct-NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
050	102+08.21	49.22' LT	740.91	156637.92	458907.59
051	102+11.17	46.10' LT	741.03	156635.02	458910.77
052	102+13.50	43.98' LT	741.13	156633.07	458913.24
053	102+15.99	41.99' LT	741.18	156631.26	458915.87
054	102+19.52	39.58' LT	741.24	156629.11	458919.56
055	102+22.22	38.00' LT	741.29	156627.72	458922.37
056	102+25.12	36.56' LT	741.32	156626.49	458925.36
057	102+29.30	34.87' LT	741.37	156625.10	458929.65
058	102+35.68	33.07' LT	741.49	156623.76	458936.14
059	102+45.74	32.01' LT	741.93	156623.41	458946.26
060	102+45.75	34.51' LT	742.35	156625.90	458946.10
061	102+39.64	34.93' LT	742.07	156625.89	458939.97
062	102+35.64	35.64' LT	741.40	156626.32	458935.91
063	102+29.74	37.36' LT	741.28	156627.62	458929.91
064	102+28.20	37.95' LT	741.44	156628.10	458928.33
065	102+29.89	39.44' LT	741.30	156629.70	458929.91
066	102+35.78	37.66' LT	741.42	156628.34	458935.91
067	102+33.22	46.34' LT	741.42	156636.82	458932.75
068	102+38.24	43.96' LT	741.52	156634.80	458937.94
069	102+24.78	39.49' LT	741.40	156629.39	458924.82
070	102+22.87	40.49' LT	741.20	156630.25	458922.84
071	102+15.56	45.51' LT	741.05	156634.74	458915.19
072	102+12.27	48.52' LT	741.43	156637.51	458911.70
073	102+18.80	45.50' LT	741.09	156634.96	458918.42
074	102+27.36	40.48' LT	741.25	156630.56	458927.32
075	102+27.37	45.48' LT	741.26	156635.55	458926.98
076	102+29.57	48.62' LT	741.33	156638.84	458928.94
077	102+25.07	52.26' LT	741.25	156642.15	458924.20
078	102+10.10	50.86' LT	741.33	156639.69	458909.36
079	102+09.94	58.75' LT	741.32	156647.55	458908.64
080	102+17.02	62.48' LT	741.42	156651.77	458915.44
081	102+18.77	59.63' LT	741.38	156649.05	458917.39
082	102+12.32	54.83' LT	741.26	156643.81	458911.30
083	102+15.17	50.72' LT	741.20	156639.91	458914.44
084	102+21.80	55.65' LT	741.32	156645.30	458920.70
085	102+43.32	42.33' LT	741.89	156633.53	458943.12
086	102+43.38	37.45' LT	741.82	156628.67	458943.52
087	102+48.38	37.43' LT	742.14	156629.01	458948.51
088	102+48.34	42.36' LT	742.21	156633.92	458948.13
089	102+53.32	42.41' LT	742.53	156634.32	458953.09

Eagle Bluff Ct-NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
090	102+53.38	37.45' LT	742.45	156629.38	458953.50
091	102+58.38	37.47' LT	742.77	156629.75	458958.48
092	102+58.32	42.42' LT	742.84	156634.68	458958.08
093	102+63.43	42.46' LT	743.16	156635.08	458963.17
094	102+63.49	37.49' LT	743.09	156630.13	458963.58

* 2-INCH CURB HEIGHT REQUIRED

LEGEND

- X — CURB RAMP TYPE
- SW5 — CONCRETE SIDEWALK 5-INCH
- PED — CONCRETE CURB PEDESTRIAN
- LL — LEVEL LANDING
- TSFM — TOPSOIL, SEEDING MIXTURE NO. 40, FERTILIZER TYPE B & EROSION MAT
- XX XXXX.XX — POINT NUMBER & ELEVATION
- MEF — MAXIMUM EXTENT FEASIBLE

CURB RADIUS CENTER

EAGLE BLUFF CT

E MAIN ST

102'MS'

103'MS'

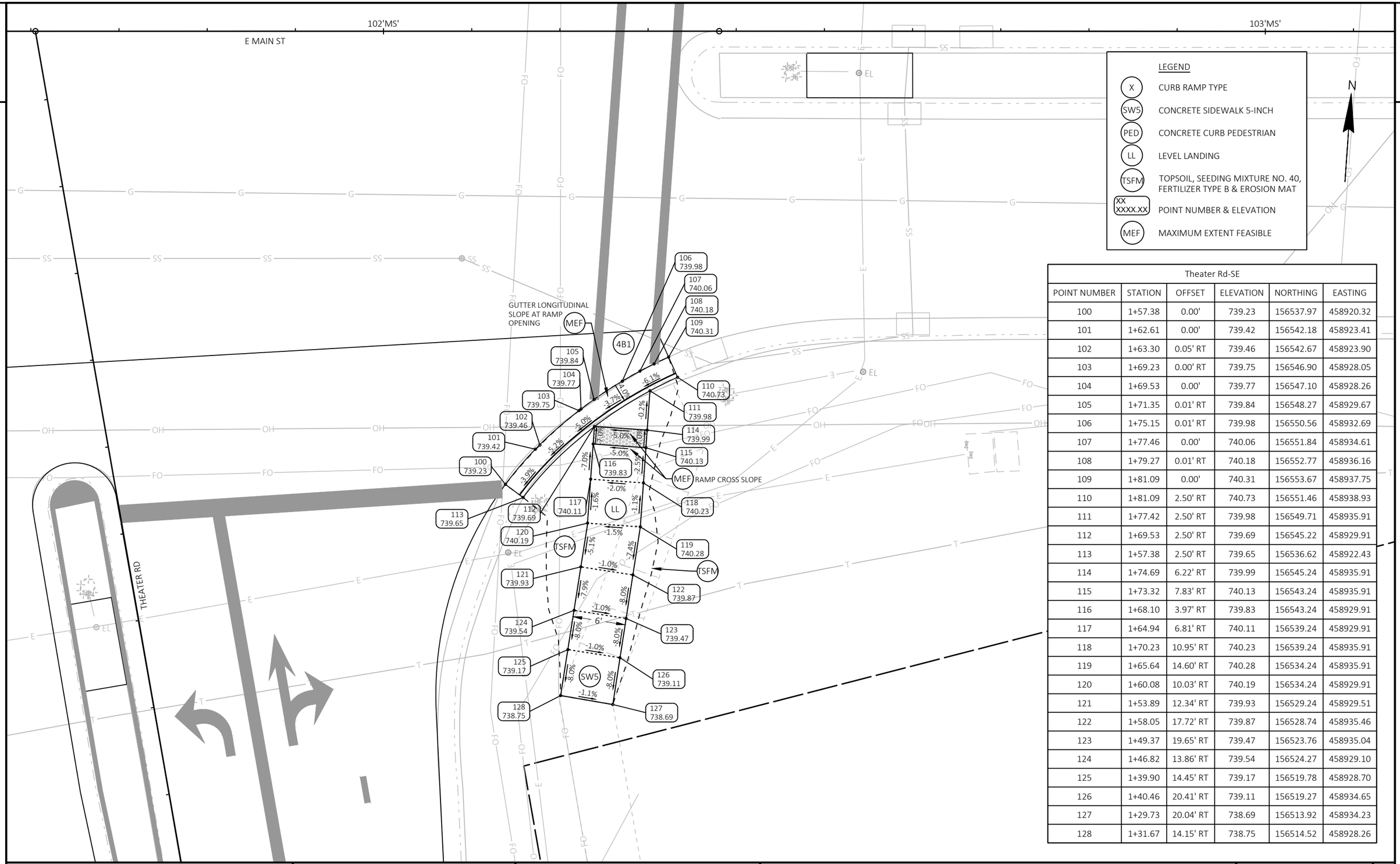
RADIAL WARNING FIELD PANEL LAYOUT TABLE					
QUADRANT	STREET XING	BACK OF CURB RADIUS	LANDING LENGTH 'XR'	RADIAL WARNING FIELD AREA	RADIAL LONG CHORD
NORTHEAST	EAGLE BLUFF CT	47.50 FT	8.89 FT	18.75 SF	8.87 FT
NORTHEAST	E MAIN ST	47.50 FT	6.14FT	16.41 SF	6.14 FT

PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	CURB RAMP DETAILS	SHEET 7	E

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 02 TYP SEC & DETAILS\021300-CR (CURB RAMP DETAILS).DWG
LAYOUT NAME - 02-Eagle Bluff Ct-NE

PLOT DATE : 10/17/2024 2:50 PM PLOT BY : BEN PIERCE PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 42

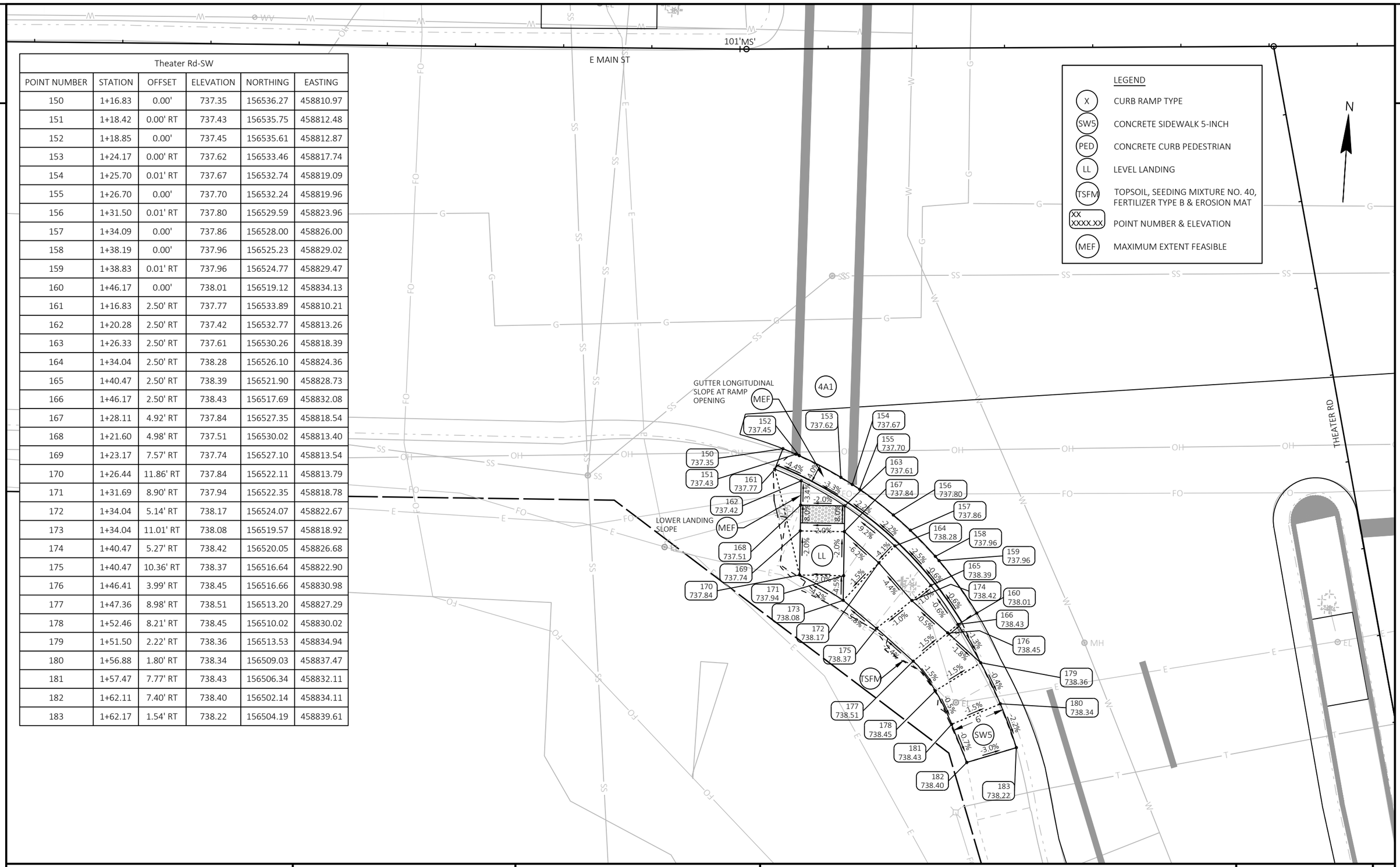


Theater Rd-SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
100	1+57.38	0.00'	739.23	156537.97	458920.32
101	1+62.61	0.00'	739.42	156542.18	458923.41
102	1+63.30	0.05' RT	739.46	156542.67	458923.90
103	1+69.23	0.00' RT	739.75	156546.90	458928.05
104	1+69.53	0.00'	739.77	156547.10	458928.26
105	1+71.35	0.01' RT	739.84	156548.27	458929.67
106	1+75.15	0.01' RT	739.98	156550.56	458932.69
107	1+77.46	0.00'	740.06	156551.84	458934.61
108	1+79.27	0.01' RT	740.18	156552.77	458936.16
109	1+81.09	0.00'	740.31	156553.67	458937.75
110	1+81.09	2.50' RT	740.73	156551.46	458938.93
111	1+77.42	2.50' RT	739.98	156549.71	458935.91
112	1+69.53	2.50' RT	739.69	156545.22	458929.91
113	1+57.38	2.50' RT	739.65	156536.62	458922.43
114	1+74.69	6.22' RT	739.99	156545.24	458935.91
115	1+73.32	7.83' RT	740.13	156543.24	458935.91
116	1+68.10	3.97' RT	739.83	156543.24	458929.91
117	1+64.94	6.81' RT	740.11	156539.24	458929.91
118	1+70.23	10.95' RT	740.23	156539.24	458935.91
119	1+65.64	14.60' RT	740.28	156534.24	458935.91
120	1+60.08	10.03' RT	740.19	156534.24	458929.91
121	1+53.89	12.34' RT	739.93	156529.24	458929.51
122	1+58.05	17.72' RT	739.87	156528.74	458935.46
123	1+49.37	19.65' RT	739.47	156523.76	458935.04
124	1+46.82	13.86' RT	739.54	156524.27	458929.10
125	1+39.90	14.45' RT	739.17	156519.78	458928.70
126	1+40.46	20.41' RT	739.11	156519.27	458934.65
127	1+29.73	20.04' RT	738.69	156513.92	458934.23
128	1+31.67	14.15' RT	738.75	156514.52	458928.26

PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	CURB RAMP DETAILS	SHEET 8	E

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 02 TYP SEC & DETAILS\021300-CR (CURB RAMP DETAILS).DWG
LAYOUT NAME - 03-Theater Rd-SE
PLOT DATE : 10/17/2024 2:50 PM PLOT BY : BEN PIERCE PLOT NAME : PLOT SCALE : 1 IN:10 FT

Theater Rd-SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
150	1+16.83	0.00'	737.35	156536.27	458810.97
151	1+18.42	0.00' RT	737.43	156535.75	458812.48
152	1+18.85	0.00'	737.45	156535.61	458812.87
153	1+24.17	0.00' RT	737.62	156533.46	458817.74
154	1+25.70	0.01' RT	737.67	156532.74	458819.09
155	1+26.70	0.00'	737.70	156532.24	458819.96
156	1+31.50	0.01' RT	737.80	156529.59	458823.96
157	1+34.09	0.00'	737.86	156528.00	458826.00
158	1+38.19	0.00'	737.96	156525.23	458829.02
159	1+38.83	0.01' RT	737.96	156524.77	458829.47
160	1+46.17	0.00'	738.01	156519.12	458834.13
161	1+16.83	2.50' RT	737.77	156533.89	458810.21
162	1+20.28	2.50' RT	737.42	156532.77	458813.26
163	1+26.33	2.50' RT	737.61	156530.26	458818.39
164	1+34.04	2.50' RT	738.28	156526.10	458824.36
165	1+40.47	2.50' RT	738.39	156521.90	458828.73
166	1+46.17	2.50' RT	738.43	156517.69	458832.08
167	1+28.11	4.92' RT	737.84	156527.35	458818.54
168	1+21.60	4.98' RT	737.51	156530.02	458813.40
169	1+23.17	7.57' RT	737.74	156527.10	458813.54
170	1+26.44	11.86' RT	737.84	156522.11	458813.79
171	1+31.69	8.90' RT	737.94	156522.35	458818.78
172	1+34.04	5.14' RT	738.17	156524.07	458822.67
173	1+34.04	11.01' RT	738.08	156519.57	458818.92
174	1+40.47	5.27' RT	738.42	156520.05	458826.68
175	1+40.47	10.36' RT	738.37	156516.64	458822.90
176	1+46.41	3.99' RT	738.45	156516.66	458830.98
177	1+47.36	8.98' RT	738.51	156513.20	458827.29
178	1+52.46	8.21' RT	738.45	156510.02	458830.02
179	1+51.50	2.22' RT	738.36	156513.53	458834.94
180	1+56.88	1.80' RT	738.34	156509.03	458837.47
181	1+57.47	7.77' RT	738.43	156506.34	458832.11
182	1+62.11	7.40' RT	738.40	156502.14	458834.11
183	1+62.17	1.54' RT	738.22	156504.19	458839.61



PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	CURB RAMP DETAILS	SHEET 9	E

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 02 TYP SEC & DETAILS\021300-CR (CURB RAMP DETAILS).DWG
LAYOUT NAME - 04-Theater Rd-SW
PLOT DATE : 10/17/2024 2:50 PM PLOT BY : BEN PIERCE PLOT NAME : PLOT SCALE : 1 IN:10 FT

REMOVAL LEGEND

- CONTROL CABINET
- NONMETALLIC CONDUIT 3", UNLESS OTHERWISE NOTED
- LOOP DETECTOR CONDUIT 1" NONMETALLIC
- SIGNAL HEAD, TRAFFIC SIGNAL STANDARD, PEDESTAL BASE
- (XX) SIGNAL HEAD, TRAFFIC SIGNAL POLE, TRANSFORMER BASE
- PEDESTRIAN HEAD WITH PUSH BUTTON
- LOOP DETECTOR IN 1" NONMETALLIC CONDUIT
- PULL BOX, 24" X 36"
- XX SIGNAL HEAD NUMBER
- R RED CIRCULAR INDICATOR
- Y YELLOW CIRCULAR INDICATOR
- G GREEN CIRCULAR INDICATOR
- DON'T WALK INDICATOR 16"
  WALK INDICATOR 16"
- LANE DESIGNATION FOR INFO ONLY

NOTES:

- ALL LENSES ARE 12-INCH
- GRAYSHADE REPRESENTS EXISTING

REMOVE EX 13' STANDARD, EX CONC BASE TO REMAIN

EAGLE BLUFF CT

CTH OS (E MAIN ST)

THEATER RD

PB1 PB2 PB3 PB4 PB5 PB6 PB7 PB8 PB9 PB10 PB11

CB1

100'MS' 101'MS' 102'MS' 103'MS'

270'TR' 270'TR'

N

PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	TRAFFIC SIGNAL REMOVAL PLAN	SHEET 10 E

MAIN STREET & THEATER ROAD
CITY OF ONALASKA
LA CROSSE COUNTY

SIGNAL NO: LOCAL **CABINET TYPE: TS2**
CONTROLLER TYPE: EPAC
DATE: 08/2024 **PAGE NO.** 1 OF 1

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 02 TYP SEC & DETAILS\024001_SP (TRF SIG SHT REMOVALS).DWG PLOT DATE : 10/4/2024 11:22 AM PLOT BY : SEH LAYOUT NAME : 01 PLOT SCALE : 1.0 IN = 40.0 FT
WISDOT/CADDS SHEET 42

2

2

LEGEND

- CONTROL CABINET
- NONMETALLIC CONDUIT 3", UNLESS OTHERWISE NOTED
- SIGNAL HEAD, TRAFFIC SIGNAL STANDARD, PEDESTAL BASE
- (XX) SIGNAL HEAD, TRAFFIC SIGNAL POLE, TRANSFORMER BASE
- B PEDESTRIAN HEAD WITH PUSH BUTTON
- PULL BOX, 24" X 42"
- XX SIGNAL HEAD NUMBER
- R RED CIRCULAR INDICATOR
- Y YELLOW CIRCULAR INDICATOR
- G GREEN CIRCULAR INDICATOR
- DON'T WALK INDICATOR 16"
  WALK INDICATOR 16"
- EVP DETECTOR HEAD

NOTES:

- ALL LENSES ARE 12-INCH
- GRAYSHADE REPRESENTS EXISTING

N

EAGLE BLUFF CT

CTH OS (E MAIN ST)

CAUTION

CAUTION

CAUTION

THEATER RD

100'MS'

101'MS'

102'MS'

103'MS'

207'TR'

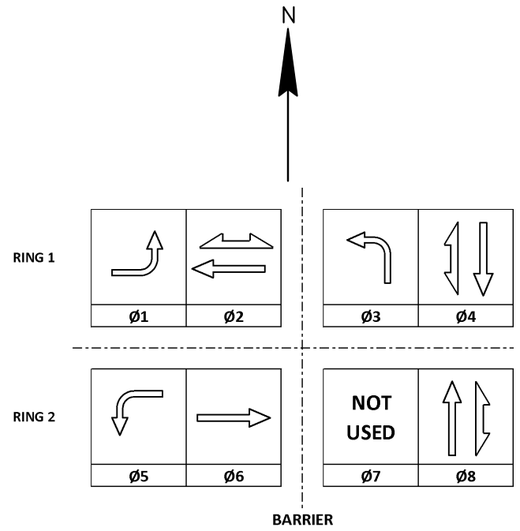
96'TR'

2-3"

MAIN STREET & THEATER ROAD	
CITY OF ONALASKA	
LA CROSSE COUNTY	
SIGNAL NO: LOCAL	CABINET TYPE: TS2
	CONTROLLER TYPE: EPAC
DATE: 08/2024	PAGE NO. 1 OF 3

PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	TRAFFIC SIGNAL PLAN	SHEET 11 E

	HEAD NUMBERS	FLASH
Ø1	2,3	-
Ø2	5,6,7,8	R
Ø3	10,11	-
Ø4	13,14,15	R
Ø5	6,7	-
Ø6	1,2,3,4	R
Ø7		
Ø8	9,10,11,12	R
Ø2P	18,19	
Ø4P	16,17	
Ø6P		
Ø8P	20,21	
OLA		
OLB		
OLC		
OLD		



CONTROLLER LOGIC

PHASE NUMBER	PHASE LOCKING	DUAL ENTRY W / Ø	PHASE RECALL	PHASE ACTIVE
1		6		X
2	X	6	MIN	X
3		8		X
4		8		X
5		2	---	X
6	X	2	MIN	X
7				
8		4	---	X

TYPE OF INTERCONNECT/COMMUNICATION	
NONE	X
CLOSED LOOP	
TWISTED PAIR	
FIBER OPTIC*	
FIBER OPTIC (ETHERNET)	
RADIO	
CELL MODEM	

TYPE OF COORDINATION	
NONE	X
TBC	
TRAFFIC RESPONSIVE	
ADAPTIVE	
*LOCATION OF MASTER CONTROLLER NO: S-	
SIGNAL SYSTEM NO: SS-	

TYPE OF LIGHTING	
BY OTHER AGENCY	
IN TRAFFIC CABINET	X
IN SEPARATE DOT LIGHTING CABINET	

TYPE OF PRE-EMPT	
NONE	
RAILROAD	
EMERGENCY VEHICLE	X
GTT	
TOMAR	
HARDWIRE	
OTHER	
CONFIRMATION LIGHTS	
LIFT BRIDGE	
QUEUE DETECTION	

DETECTOR LOGIC

DETECTOR INPUT	3	1	7	5	11	9	15	13	19	17	23	21	27	25	31	29
PLAN LOOP DETECTOR*(S)	11	21	32	42	52	81										
ASSIGNED PHASE	1	2	3	4	5	8										
OPERATION MODE	VEH	VEH	VEH	VEH	VEH	VEH										
SWITCH																
EXTEND																
DELAY																

DETECTOR INPUT	4	2	8	6	12	10	16	14	20	18	24	22	28	26	32	30
PLAN LOOP DETECTOR*(S)	12	31	41	51	61	82										
ASSIGNED PHASE	1	3	4	5	6	8										
OPERATION MODE	VEH	VEH	VEH	VEH	VEH	VEH										
SWITCH																
EXTEND																
DELAY																

EMERGENCY VEHICLE PREEMPTION SEQUENCE

EMERGENCY VEHICLE PREEMPTOR	A	B	C	D
MOVEMENT		→		↰ ↑
PHASE		6		8+3

AFTER PREEMPTION SEQUENCE 2+5 OR 6+1, CONTROLLER SHALL RETURN TO PHASES 2+6.
AFTER PREEMPTION SEQUENCE 4 OR 8, CONTROLLER SHALL RETURN TO PHASES 4+8.

MAIN STREET & THEATER ROAD
CITY OF ONALASKA
LA CROSSE COUNTY

SIGNAL NO: LOCAL CABINET TYPE: TS2
CONTROLLER TYPE: EPAC
DATE: 08/2024 PAGE NO. 2 OF 3

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 02 TYP SEC & DETAILS\024303_PH_CR (TRF SIG SHT SEQ OF OP & CABLE RTE).DWG PLOT DATE : 10/4/2024 11:24 AM PLOT BY : SEH LAYOUT NAME : SEQ OF OP PLOT SCALE : 1.0 IN = 40.0 FT
WISDOT/CADDS SHEET 42

PROJECT ID:	LOCAL
INTERSECTION:	MAIN STREET & THEATER ROAD

SIGNAL WIRE COLOR CODING	BLK - BLACK	RED - RED	GRN - GREEN
	WHT - WHITE	BLU - BLUE	ORG - ORANGE

CB_ TO	AWG 14 # OF	HEAD NO.	SIGNAL INDICATION WIRE COLOR									PED BUTTON
			RED	YELLOW	GREEN	<RED>	<YELLOW>	<GREEN>	<FLASHING>	D/WALK	WALK	
SB1	7	4	RED	ORG	GRN							
SB2	7	21								BLK	BLU	
		BUTTON (WIRE 1)										WHT/BLK
		BUTTON (WIRE 2)										WHT
SB3	7	9	RED	ORG	GRN							
SB4	12	10	RED	ORG	GRN		ORG/BLK	GRN/BLK				
		14	RED/BLK	BLU/BLK	BLK/WHT		BLU	BLK				
SB5	7	15	RED	ORG	GRN							
SB6	7	16								BLK	BLU	
		BUTTON (WIRE 1)										WHT/BLK
		BUTTON (WIRE 2)										WHT
SB7	7	1	RED	ORG	GRN							
SB8	12	2	RED	ORG	GRN		ORG/BLK	GRN/BLK				
		7	RED/BLK	BLU/BLK	BLK/WHT		BLU	BLK				
SB9	7	8	RED	ORG	GRN							
SB10	7	17								BLK	BLU	
		BUTTON (WIRE 1)										WHT/BLK
		BUTTON (WIRE 2)										WHT
SB11	7	18								BLK	BLU	
		BUTTON (WIRE 1)										WHT/BLK
		BUTTON (WIRE 2)										WHT
SB12	12	11	RED	ORG	GRN		ORG/BLK	GRN/BLK				
		13	RED/BLK	BLU/BLK	BLK/WHT							
SB13	7	12	RED	ORG	GRN							
SB14	7	19								BLK	BLU	
		BUTTON (WIRE 1)										WHT/BLK
		BUTTON (WIRE 2)										WHT
SB15	7	20								BLK	BLU	
		BUTTON (WIRE 1)										WHT/BLK
		BUTTON (WIRE 2)										WHT
SB16	7	5	RED	ORG	GRN							
SB17	12	3	RED	ORG	GRN		ORG/BLK	GRN/BLK				
		6	RED/BLK	BLU/BLK	BLK/WHT		BLU	BLK				

EQUIPMENT GROUNDING CONDUCTOR 10 AWG GREEN XLP

FROM	TO
CB1	SB1
SB1	SB2
SB2	SB3
SB3	SB4
SB4	SB5
SB5	SB6
SB6	SB7
SB7	SB8
SB8	SB9
SB9	SB10
SB10	SB11
SB11	SB12
SB12	SB13
SB13	SB14
SB14	SB15
SB15	SB16
SB16	SB17
SB17	CB1

LIGHTING UF 2-10 AWG W/ GROUND

FROM	TO
CB1	SB4
CB1	SB8
CB1	SB13
CB1	SB17

EMERGENCY VEHICLE PREEMPTION

FROM	TO
CB1	SB1 (HEAD A)
CB1	SB3 (HEAD B)

*USE THE WHITE CONDUCTOR IN THE CABLE ASSEMBLY AS THE GROUNDED CONDUCTOR FOR ALL TRAFFIC SIGNAL INDICATIONS
*ENSURE THE GROUNDED CONDUCTOR IN THE FEEDER CABLE AND THE POLE CABLES ARE BOTH 12" LONGER THAN THE UNGROUNDED CONDUCTORS.
*AT THE SIGNAL BASES, CONNECT ONE TERMINAL FROM THE PEDESTRAIN PUSH BUTTONS TO THE COLOR INDICATED IN THE CHART. CONNECT THE OTHER TERMINAL TO THE GROUNDED CONDUC
"OTHER" COLUMN MAY INCLUDE SHADOW BOX SIGN

MAIN STREET & THEATER ROAD
CITY OF ONALASKA
LA CROSSE COUNTY

SIGNAL NO: LOCAL **CABINET TYPE: TS2**
CONTROLLER TYPE: EPAC
DATE: 08/2024 **PAGE NO. 3 OF 3**

GENERAL NOTES: PAVEMENT MARKING

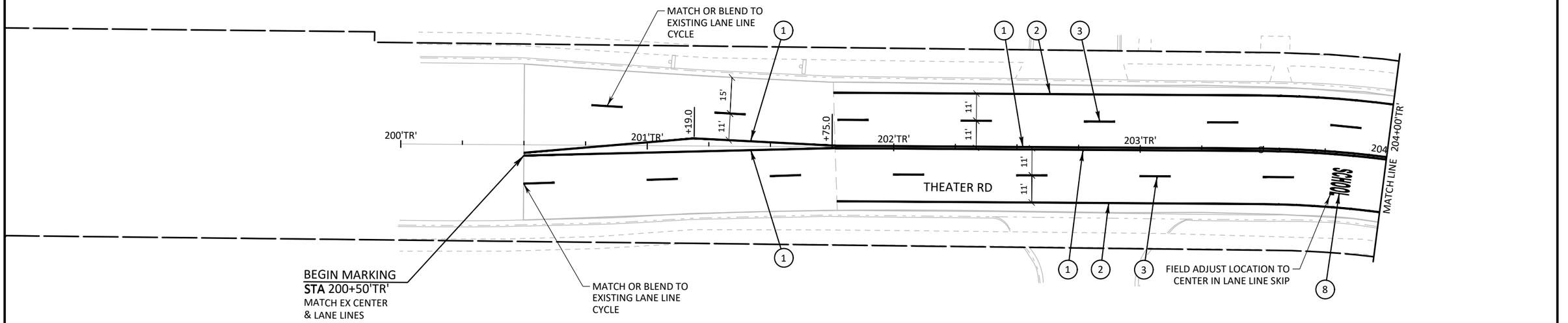
UNLESS SHOWN OTHERWISE, CROSSWALK MARKINGS SHALL BE 6' WIDE (INSIDE EDGE TO INSIDE EDGE), PARALLEL TO AND CENTERED ON A LINE CONNECTING THE CENTER POINTS OF THE CURB RAMP.

UNLESS SHOWN OTHERWISE, STOP LINES SHALL BE PARALLEL TO THE CROSSWALK LINE AND 4' (INSIDE EDGE TO INSIDE EDGE) IN ADVANCE OF THE CROSSWALK LINE.

WIDTH DIMENSIONS SHOWN ARE NOMINAL.

SEE THE PERTINENT SPECIFICATIONS AND STANDARD DETAIL DRAWINGS FOR DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN.

KEYNOTE LEGEND: PAVEMENT MARKING	
1	MARKING LINE EPOXY 4-INCH (YELLOW)
2	MARKING LINE EPOXY 4-INCH (WHITE)
3	MARKING LINE EPOXY 4-INCH (WHITE) (12.5 FT LINE 37.5 FT SKIP)
4	MARKING LINE EPOXY 4-INCH (WHITE) (3.0 FT LINE 9.0 FT SKIP)
5	MARKING LINE EPOXY 8-INCH (WHITE)
6	MARKING CURB EPOXY
7	MARKING ARROW EPOXY (WHITE)
8	MARKING WORD EPOXY (WHITE)
9	MARKING CROSSWALK EPOXY TRANSVERSE 12-INCH (WHITE)
10	MARKING STOP LINE EPOXY 18-INCH (WHITE)
11	MARKING ISLAND NOSE EPOXY (YELLOW)
12	MARKING REMOVAL LINE WATER BLASTING 10-INCH



KEYNOTE LEGEND: PAVEMENT MARKING	
1	MARKING LINE EPOXY 4-INCH (YELLOW)
2	MARKING LINE EPOXY 4-INCH (WHITE)
3	MARKING LINE EPOXY 4-INCH (WHITE) (12.5 FT LINE 37.5 FT SKIP)
4	MARKING LINE EPOXY 4-INCH (WHITE) (3.0 FT LINE 9.0 FT SKIP)
5	MARKING LINE EPOXY 8-INCH (WHITE)
6	MARKING CURB EPOXY
7	MARKING ARROW EPOXY (WHITE)
8	MARKING WORD EPOXY (WHITE)
9	MARKING CROSSWALK EPOXY TRANSVERSE 12-INCH (WHITE)
10	MARKING STOP LINE EPOXY 18-INCH (WHITE)
11	MARKING ISLAND NOSE EPOXY (YELLOW)
12	MARKING REMOVAL LINE WATER BLASTING 10-INCH

RETRACE E MAIN STREET WEST BOUND CHANNELIZING LANE LINE AND SKIPS STARTING FROM THE BEGINNING OF EXISTING CHANNELIZING LANE LINE

N

CTH OS (E MAIN ST)

ONLY

100'MS'

101'MS'

6' CROSSWALK WIDTH

STA 207+97.85'TR'
STA 101+60.52'MS'

END MARKING
STA 207+50'TR'

102'MS'

EAGLE BLUFF CT

THEATER RD

207'TR'

206'TR'

205'TR'

+82.0

+87.0

+13.0

11'

MATCH LINE 204+00'TR'

TR'

CTH OS (E MAIN ST)

103'MS'

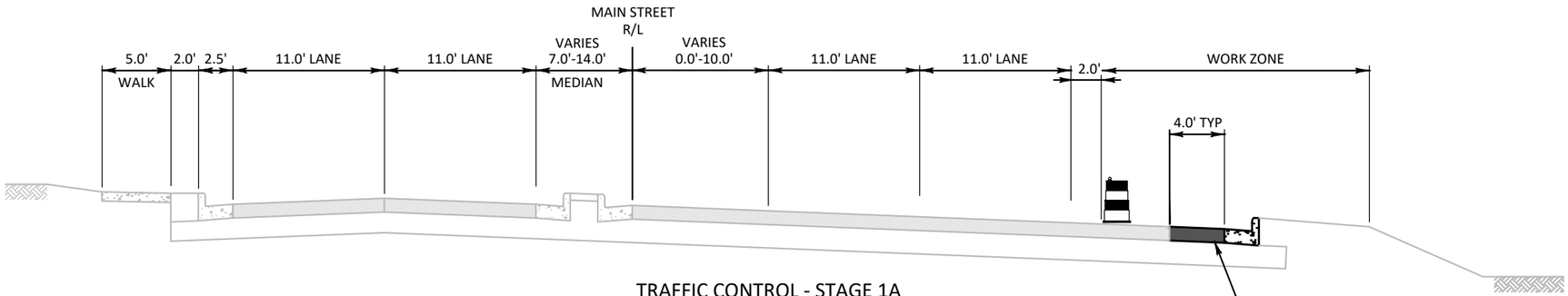
ONLY

RETRACE E MAIN STREET EASTBOUND CHANNELIZING LANE LINE AND SKIPS STARTING FROM THE BEGINNING OF EXISTING CHANNELIZING LANE LINE

2

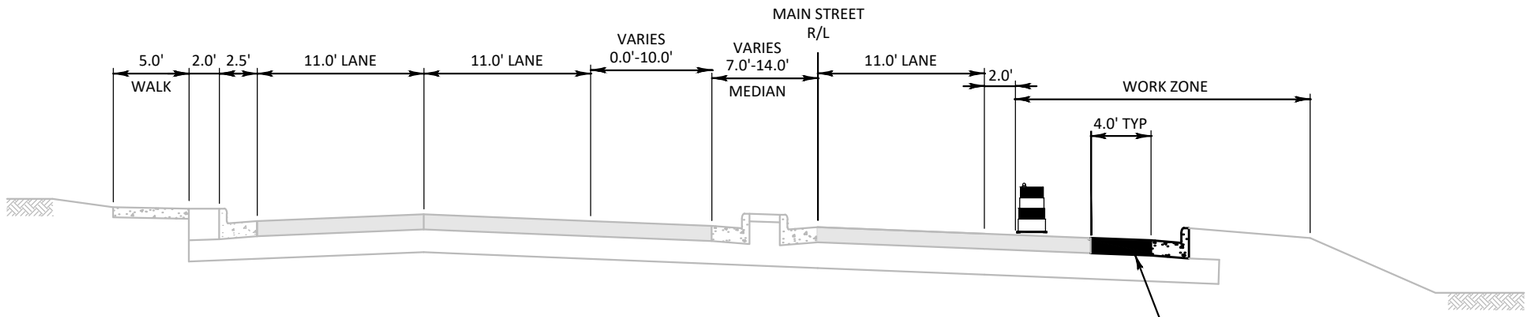
TRAFFIC CONTROL NOTES:

1. MAINTAIN MINIMUM ONE 11' LANE IN EACH DIRECTION.
2. MAINTAIN ACCESS TO ALL DRIVEWAYS EXCEPT WHEN WORKING IMMEDIATELY IN FRONT OF DRIVEWAY.
3. USE SSD TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY.



TRAFFIC CONTROL - STAGE 1A

E MAIN ST - WEST OF THEATER ROAD
EASTBOUND RIGHT TURN LANE CLOSED
AT THEATER ROAD



TRAFFIC CONTROL - STAGE 1B

E MAIN ST - EAST OF THEATER ROAD
EASTBOUND RIGHT LANE CLOSED AT
THEATER ROAD

PROJECT NO: 5991-02-71	HWY: THEATER ROAD	COUNTY: LA CROSSE	TRAFFIC CONTROL - STAGE 1 TYPICAL SECTION	SHEET 16 E

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 02 TYP SEC & DETAILS\020300-TS (TRAFFIC CONTROL TYPICAL SECTIONS).DWG
LAYOUT NAME - 01
PLOT DATE : 10/4/2024 1:03 PM
PLOT BY : BEN PIERCE
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42

2

TRAFFIC CONTROL LEGEND

- TRAFFIC FLOW
- TRAFFIC CONTROL DRUMS
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH SIGN
- TRAFFIC CONTROL DRUMS WITH WARNING LIGHTS TYPE C
- FLASHING ARROW BOARDS
- XXXX REMOVABLE MASK OUT TAPE 6-INCH (BLACK)
- TA TEMPORARY MARKING ARROW REMOVABLE TAPE (WHITE)
- T4W TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (WHITE)
- T8W TEMPORARY MARKING LINE REMOVABLE TAPE 8-INCH (WHITE)
- COVERING SIGNS TYPE II

WORK SPACE, CLOSED TO TRAFFIC

MB
PCMS MESSAGING
ROAD WORK BEGINS XX/XX

ROAD WORK AHEAD W20-1A
RIGHT LANE CLOSED AHEAD W20-5A
W04-2R

200' 200' 200' 200'

END ROAD WORK G20-2A 48"X24"

CTH OS (E MAIN ST)

T4W

SEE NOTE 8 TA T8W

SEE NOTE 8 TA

ONLY

ONLY

W3-1 36"X36"

BEGIN RIGHT TURN LANE R3-20R 24"X36"

RIGHT LANE MUST TURN RIGHT R3-7R 30"X30"

LANE CLOSED R11-2L 48"X30"

STOP R1-1 30"X30"

THEATER ROAD

EAGLE BLUFF COURT

ROAD WORK AHEAD W20-1A

END ROAD WORK G20-2A 48"X24"

ROAD WORK AHEAD W20-1A

W3-1 36"X36"

LANE CLOSED R11-2L 48"X30"

400'

END ROAD WORK G20-2A 48"X24"

W3-1 36"X36"

N

STAGE 1A CONSTRUCTION
SEE TEMPORARY PEDESTRIAN ACCOMMODATIONS

STAGE 1B CONSTRUCTION
SEE TEMPORARY PEDESTRIAN ACCOMMODATIONS

FOR ADVANCED WARNING SIGNING ON THEATER ROAD USE SDD 15C5.

GENERAL NOTES: TRAFFIC CONTROL

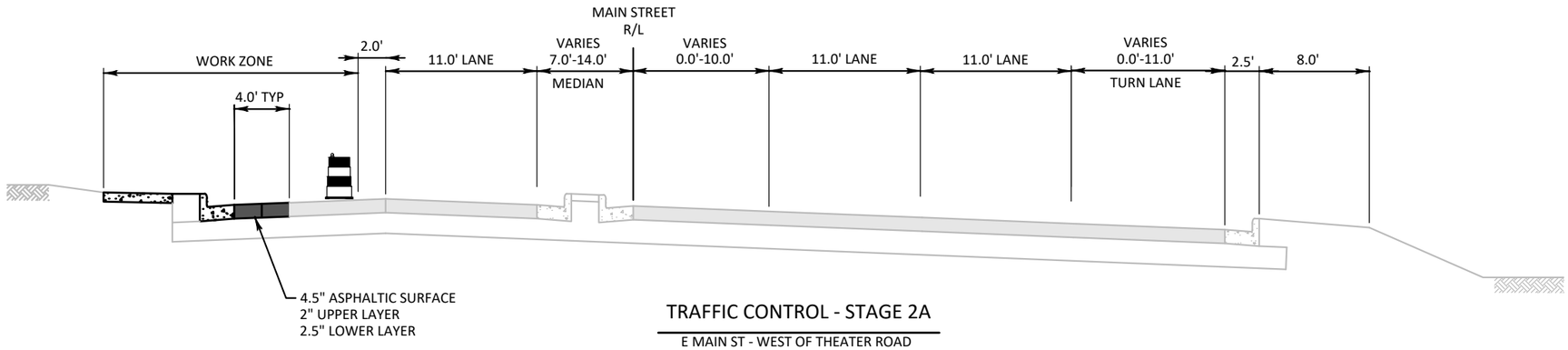
1. THE FOLLOWING NOTES ARE APPLICABLE TO ALL STAGES. SEE THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR ADDITIONAL TRAFFIC CONTROL AND CONSTRUCTION STAGING REQUIREMENTS.
2. THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER IN THE FIELD.
3. ALL "WO" OR "W" DIAMOND SHAPED WARNING SIGNS, SIGNS ARE 36"x36" UNLESS OTHERWISE NOTED.
4. MOUNT ALL PORTABLE TRAFFIC CONTROL SIGNS AT A MINIMUM HEIGHT OF 5 FEET, MEASURED FROM THE BOTTOM OF THE SIGN, ABOVE THE EDGE OF PAVEMENT.
5. THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 50 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.
6. SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
7. DURING TRAFFIC SIGNAL WORK THE TRAFFIC SIGNALS WILL NOT BE FUNCTIONING AND THE INTERSECTION WILL BE ALL WAY STOP CONTROLLED. USE THE FOLDING STOP SIGNS ON THE EXISTING TRAFFIC SIGNALS.
8. COVER EXISTING OR CONFLICTING ARROW WITH MASK OUT TAPE, PAID FOR AS A TEMPORARY MARKING ARROW REMOVABLE TAPE. NEW TEMPORARY MARKING ARROW REMOVABLE TAPE (WHITE) PAID SEPARATELY.

PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	TRAFFIC CONTROL - STAGE 1	SHEET 17	E

2

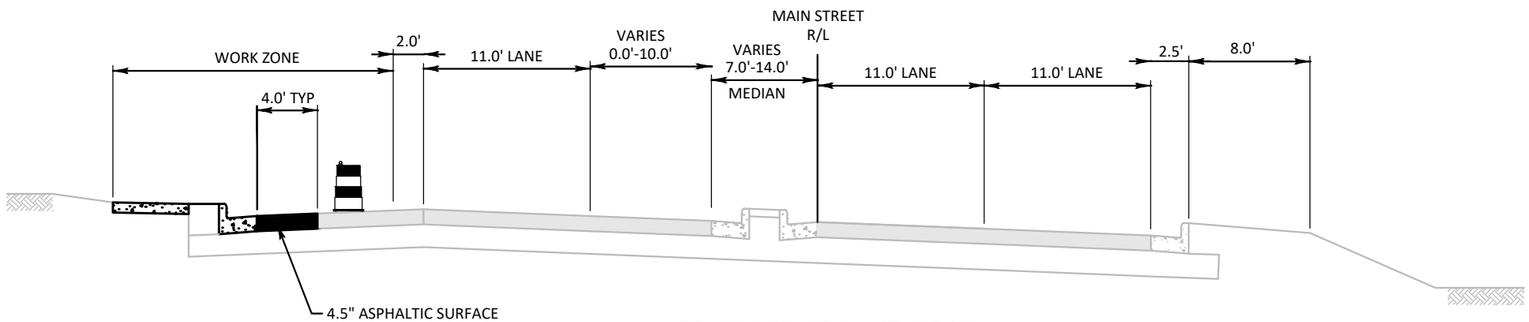
TRAFFIC CONTROL NOTES:

1. MAINTAIN MINIMUM ONE 11' LANE IN EACH DIRECTION.
2. MAINTAIN ACCESS TO ALL DRIVEWAYS EXCEPT WHEN WORKING IMMEDIATELY IN FRONT OF DRIVEWAY.
3. USE SSD TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY.



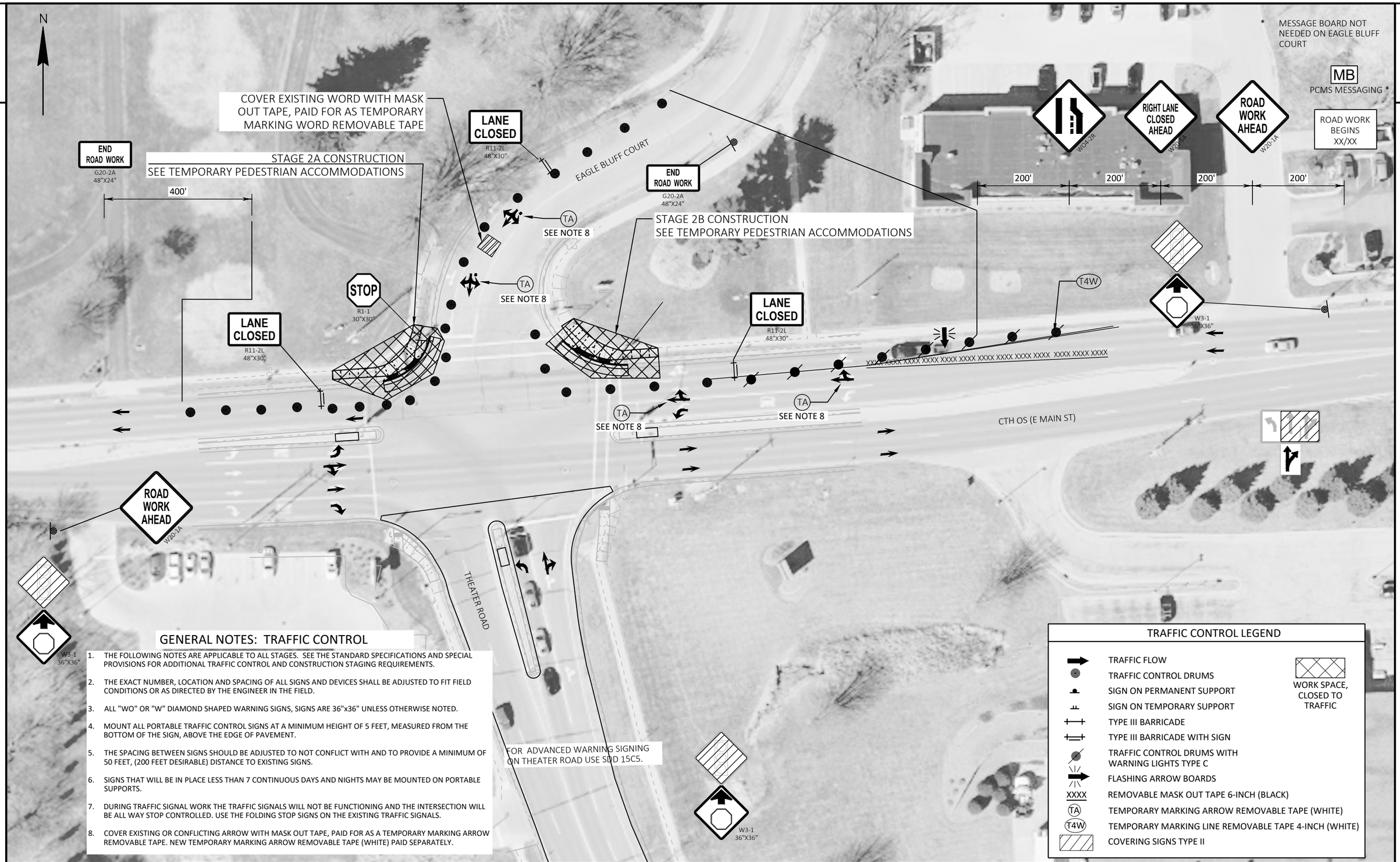
TRAFFIC CONTROL - STAGE 2A

E MAIN ST - WEST OF THEATER ROAD
EASTBOUND RIGHT LANE CLOSED AT
THEATER ROAD



TRAFFIC CONTROL - STAGE 2B

E MAIN ST - EAST OF THEATER ROAD
EASTBOUND RIGHT TURN LANE CLOSED
AT THEATER ROAD



GENERAL NOTES: TRAFFIC CONTROL

1. THE FOLLOWING NOTES ARE APPLICABLE TO ALL STAGES. SEE THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR ADDITIONAL TRAFFIC CONTROL AND CONSTRUCTION STAGING REQUIREMENTS.
2. THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER IN THE FIELD.
3. ALL "WO" OR "W" DIAMOND SHAPED WARNING SIGNS, SIGNS ARE 36"x36" UNLESS OTHERWISE NOTED.
4. MOUNT ALL PORTABLE TRAFFIC CONTROL SIGNS AT A MINIMUM HEIGHT OF 5 FEET, MEASURED FROM THE BOTTOM OF THE SIGN, ABOVE THE EDGE OF PAVEMENT.
5. THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 50 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.
6. SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
7. DURING TRAFFIC SIGNAL WORK THE TRAFFIC SIGNALS WILL NOT BE FUNCTIONING AND THE INTERSECTION WILL BE ALL WAY STOP CONTROLLED. USE THE FOLDING STOP SIGNS ON THE EXISTING TRAFFIC SIGNALS.
8. COVER EXISTING OR CONFLICTING ARROW WITH MASK OUT TAPE, PAID FOR AS A TEMPORARY MARKING ARROW REMOVABLE TAPE. NEW TEMPORARY MARKING ARROW REMOVABLE TAPE (WHITE) PAID SEPARATELY.

TRAFFIC CONTROL LEGEND

- TRAFFIC FLOW
- TRAFFIC CONTROL DRUMS
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH SIGN
- TRAFFIC CONTROL DRUMS WITH WARNING LIGHTS TYPE C
- FLASHING ARROW BOARDS
- XXXX REMOVABLE MASK OUT TAPE 6-INCH (BLACK)
- TA TEMPORARY MARKING ARROW REMOVABLE TAPE (WHITE)
- T4W TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (WHITE)
- COVERING SIGNS TYPE II
- WORK SPACE, CLOSED TO TRAFFIC

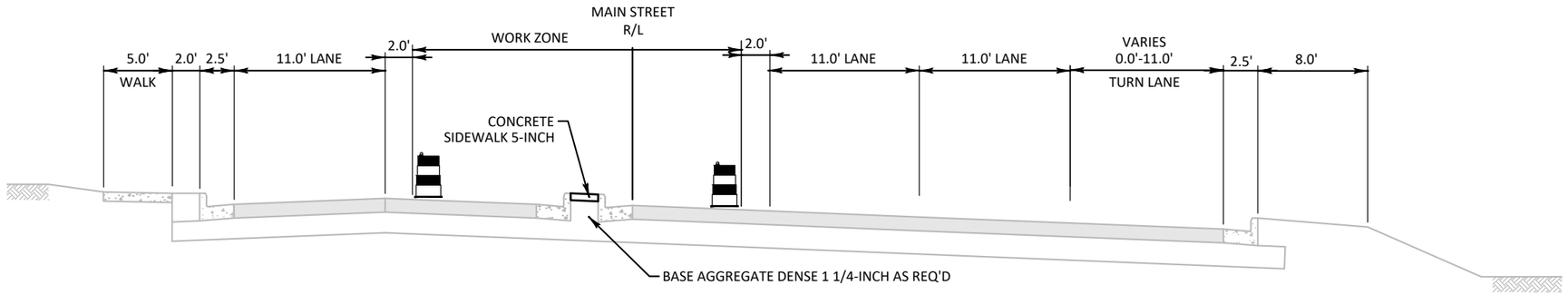
PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	TRAFFIC CONTROL - STAGE 2	SHEET 19	E

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 02 TYP SEC & DETAILS\026001_S2.DWG
LAYOUT NAME - 01
PLOT DATE : 10/4/2024 11:26 AM PLOT BY : BEN PIERCE PLOT NAME : PLOT SCALE : 1 IN:50 FT

2

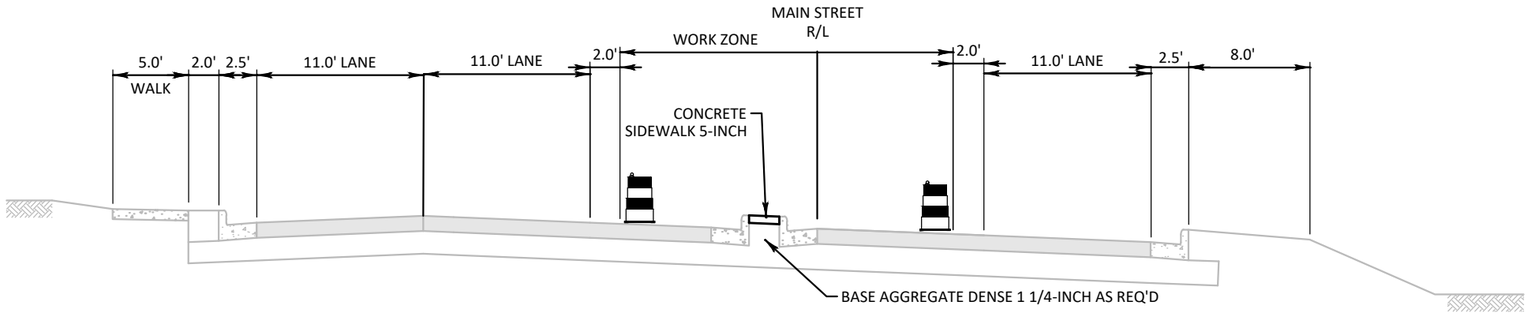
TRAFFIC CONTROL NOTES:

1. MAINTAIN MINIMUM ONE 11' LANE IN EACH DIRECTION.
2. MAINTAIN ACCESS TO ALL DRIVEWAYS EXCEPT WHEN WORKING IMMEDIATELY IN FRONT OF DRIVEWAY.
3. USE SSD TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY.



TRAFFIC CONTROL - STAGE 3

E MAIN ST - WEST OF THEATER ROAD



TRAFFIC CONTROL - STAGE 3

E MAIN ST - EAST OF THEATER ROAD

2

N

MB
PCMS MESSAGING
ROAD WORK BEGINS XX/XX

ROAD WORK AHEAD W20-1A
LEFT LANE CLOSED AHEAD W20-5SA
W04-2L

200' 200' 200' 200'

END ROAD WORK
G20-2A
48"X24"

W3-1
36"X36"

ONLY

CTH OS (E MAIN ST)

T4W T8W

LANE CLOSED
R11-2L
48"X30"

BEGIN LEFT TURN LANE
R3-20L
24"X36"

LEFT LANE MUST TURN LEFT
R3-7L
30"X30"

STOP
R1-1
30"X30"

TA
SEE NOTE 8

EAGLE BLUFF COURT

THEATER ROAD

FOR ADVANCED WARNING SIGNING ON THEATER ROAD USE SDD 15C5.

GENERAL NOTES: TRAFFIC CONTROL

1. THE FOLLOWING NOTES ARE APPLICABLE TO ALL STAGES. SEE THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR ADDITIONAL TRAFFIC CONTROL AND CONSTRUCTION STAGING REQUIREMENTS.
2. THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER IN THE FIELD.
3. ALL "WO" OR "W" DIAMOND SHAPED WARNING SIGNS, SIGNS ARE 36"x36" UNLESS OTHERWISE NOTED.
4. MOUNT ALL PORTABLE TRAFFIC CONTROL SIGNS AT A MINIMUM HEIGHT OF 5 FEET, MEASURED FROM THE BOTTOM OF THE SIGN, ABOVE THE EDGE OF PAVEMENT.
5. THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 50 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.
6. SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
7. DURING TRAFFIC SIGNAL WORK THE TRAFFIC SIGNALS WILL NOT BE FUNCTIONING AND THE INTERSECTION WILL BE ALL WAY STOP CONTROLLED. USE THE FOLDING STOP SIGNS ON THE EXISTING TRAFFIC SIGNALS.
8. COVER EXISTING OR CONFLICTING ARROW WITH MASK OUT TAPE, PAID FOR AS A TEMPORARY MARKING ARROW REMOVABLE TAPE. NEW TEMPORARY MARKING ARROW REMOVABLE TAPE (WHITE) PAID SEPARATELY.

TRAFFIC CONTROL LEGEND

- TRAFFIC FLOW
- TRAFFIC CONTROL DRUMS
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH SIGN
- TRAFFIC CONTROL DRUMS WITH WARNING LIGHTS TYPE C
- FLASHING ARROW BOARDS
- XXXX REMOVABLE MASK OUT TAPE 6-INCH (BLACK)
- TA TEMPORARY MARKING ARROW REMOVABLE TAPE (WHITE)
- T4W TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (WHITE)
- T8W TEMPORARY MARKING LINE REMOVABLE TAPE 8-INCH (WHITE)
- COVERING SIGNS TYPE II
- WORK SPACE, CLOSED TO TRAFFIC

PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	TRAFFIC CONTROL - STAGE 3	SHEET 21	E

2

N

MODIFY EXISTING ARROW TO SHOW RIGHT ARROW ONLY. COVER THRU ARROW PORTION WITH MASK OUT TAPE, PAID FOR AS TEMPORARY MARKING ARROW REMOVABLE TAPE

RIGHT LANE MUST TURN RIGHT
R3-7R
30"X30"

EAGLE BLUFF CT

LANE CLOSED
R11-2L
48"X30"

W3-1
36"X36"

W3-1
36"X36"

ROAD CLOSED
R11-2
48"X30"

E MAIN ST

R1-1
30"X30"
STOP
R3-2
24"X24"

ONLY

LANE CLOSED
R11-2L
48"X30"

R1-1
30"X30"
STOP
R3-1
24"X24"

MB
PCMS MESSAGING
ROAD WORK BEGINS XX/XX

THEATER ROAD

PLACE PCMS 7 DAYS IN ADVANCE OF CLOSURE

PHASE 1:
THEATER ROAD CLOSED

PHASE 2:
X/XX AND X/XX

GENERAL NOTES: TRAFFIC CONTROL

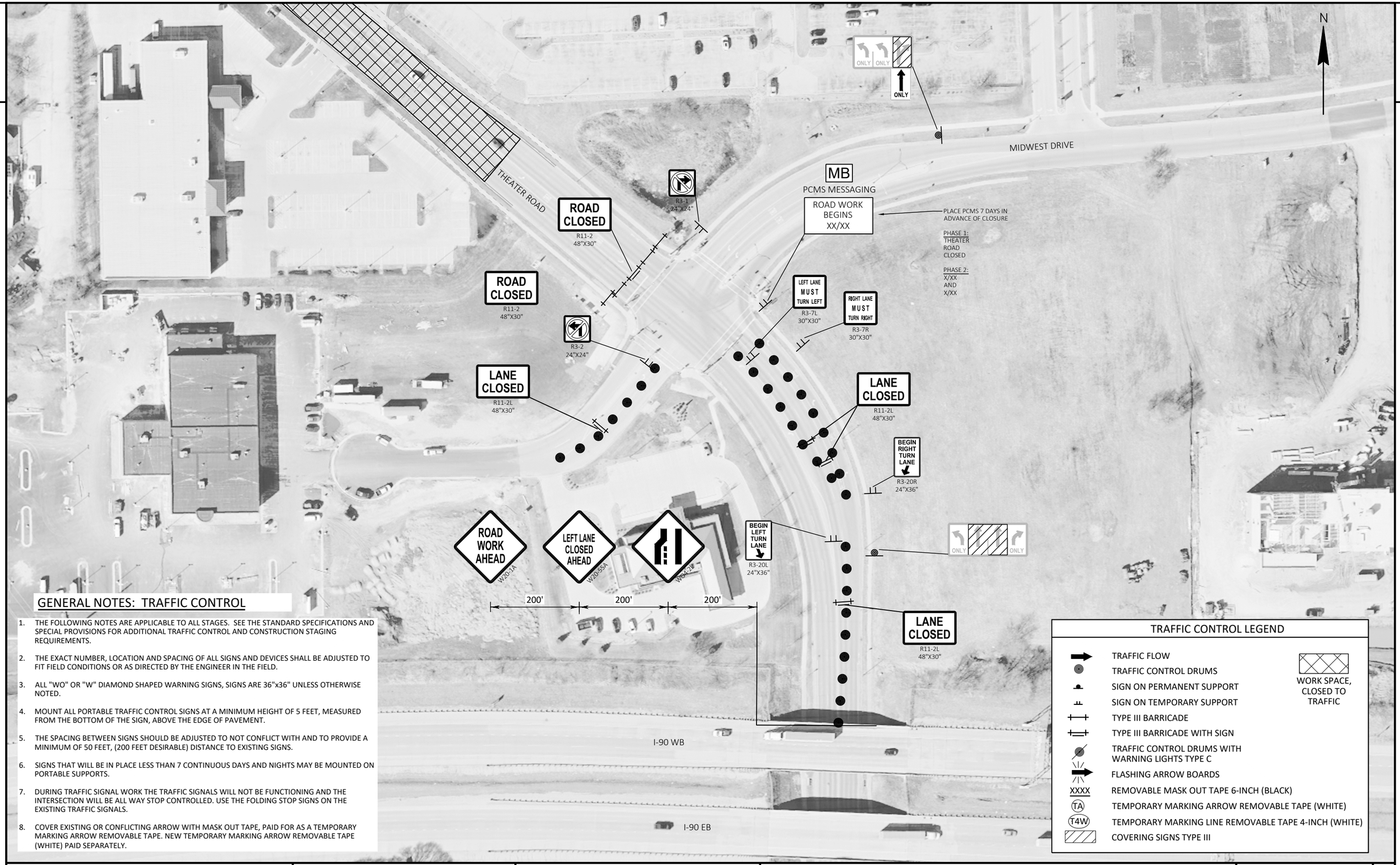
1. THE FOLLOWING NOTES ARE APPLICABLE TO ALL STAGES. SEE THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR ADDITIONAL TRAFFIC CONTROL AND CONSTRUCTION STAGING REQUIREMENTS.
2. THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER IN THE FIELD.
3. ALL "WO" OR "W" DIAMOND SHAPED WARNING SIGNS, SIGNS ARE 36"x36" UNLESS OTHERWISE NOTED.
4. MOUNT ALL PORTABLE TRAFFIC CONTROL SIGNS AT A MINIMUM HEIGHT OF 5 FEET, MEASURED FROM THE BOTTOM OF THE SIGN, ABOVE THE EDGE OF PAVEMENT.
5. THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 50 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.
6. SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
7. DURING TRAFFIC SIGNAL WORK THE TRAFFIC SIGNALS WILL NOT BE FUNCTIONING AND THE INTERSECTION WILL BE ALL WAY STOP CONTROLLED. USE THE FOLDING STOP SIGNS ON THE EXISTING TRAFFIC SIGNALS.
8. COVER EXISTING OR CONFLICTING ARROW WITH MASK OUT TAPE, PAID FOR AS A TEMPORARY MARKING ARROW REMOVABLE TAPE. NEW TEMPORARY MARKING ARROW REMOVABLE TAPE (WHITE) PAID SEPARATELY.

TRAFFIC CONTROL LEGEND

- TRAFFIC FLOW
- TRAFFIC CONTROL DRUMS
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH SIGN
- TRAFFIC CONTROL DRUMS WITH WARNING LIGHTS TYPE C
- FLASHING ARROW BOARDS
- XXXX — REMOVABLE MASK OUT TAPE 6-INCH (BLACK)
- TA — TEMPORARY MARKING ARROW REMOVABLE TAPE (WHITE)
- T4W — TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (WHITE)
- COVERING SIGNS TYPE III
- WORK SPACE, CLOSED TO TRAFFIC

PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	TRAFFIC CONTROL - STAGE 4	SHEET 22	E

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 02 TYP SEC & DETAILS\026001_S4.DWG
LAYOUT NAME - 01
PLOT DATE : 10/4/2024 11:28 AM
PLOT BY : BEN PIERCE
PLOT NAME :
PLOT SCALE : 1 IN:100 FT
WISDOT/CADDS SHEET 42



GENERAL NOTES: TRAFFIC CONTROL

1. THE FOLLOWING NOTES ARE APPLICABLE TO ALL STAGES. SEE THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR ADDITIONAL TRAFFIC CONTROL AND CONSTRUCTION STAGING REQUIREMENTS.
2. THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER IN THE FIELD.
3. ALL "WO" OR "W" DIAMOND SHAPED WARNING SIGNS, SIGNS ARE 36"x36" UNLESS OTHERWISE NOTED.
4. MOUNT ALL PORTABLE TRAFFIC CONTROL SIGNS AT A MINIMUM HEIGHT OF 5 FEET, MEASURED FROM THE BOTTOM OF THE SIGN, ABOVE THE EDGE OF PAVEMENT.
5. THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 50 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.
6. SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
7. DURING TRAFFIC SIGNAL WORK THE TRAFFIC SIGNALS WILL NOT BE FUNCTIONING AND THE INTERSECTION WILL BE ALL WAY STOP CONTROLLED. USE THE FOLDING STOP SIGNS ON THE EXISTING TRAFFIC SIGNALS.
8. COVER EXISTING OR CONFLICTING ARROW WITH MASK OUT TAPE, PAID FOR AS A TEMPORARY MARKING ARROW REMOVABLE TAPE. NEW TEMPORARY MARKING ARROW REMOVABLE TAPE (WHITE) PAID SEPARATELY.

TRAFFIC CONTROL LEGEND

- TRAFFIC FLOW
- TRAFFIC CONTROL DRUMS
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH SIGN
- TRAFFIC CONTROL DRUMS WITH WARNING LIGHTS TYPE C
- FLASHING ARROW BOARDS
- XXXX REMOVABLE MASK OUT TAPE 6-INCH (BLACK)
- TA TEMPORARY MARKING ARROW REMOVABLE TAPE (WHITE)
- T4W TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (WHITE)
- COVERING SIGNS TYPE III
- WORK SPACE, CLOSED TO TRAFFIC

PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	TRAFFIC CONTROL - STAGE 4	SHEET 23	E

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 02 TYP SEC & DETAILS\026001_S4.DWG
LAYOUT NAME - 02
PLOT DATE : 10/4/2024 11:28 AM
PLOT BY : BEN PIERCE
PLOT NAME :
PLOT SCALE : 1 IN:100 FT
WISDOT/CADDS SHEET 42

STAGE 1A PEDESTRIAN TRAFFIC CONTROL

SW CORNER RAMP CONSTRUCTION AT THE INTERSECTION OF THEATER ROAD AND CTH OS/E MAIN ST. CURB RAMP TO BE CONSTRUCTED UNDER OUTSIDE LANE CLOSURE ON CTH OS/E MAIN ST.

TRAFFIC CONTROL LEGEND

SIGN ON TEMPORARY SUPPORT

WORK SPACE

N

CTH OS (E MAIN ST)

THEATER ROAD

MIDWEST DRIVE

R9-9A 30"X18" SIDEWALK CLOSED AHEAD / M4-9BL 30"X24" DETOUR — 2030

M4-9BR 30"X24" DETOUR / M4-9BL 30"X24" DETOUR — 2031

2032

R9-9 24"X12" SIDEWALK CLOSED

M4-9BR 30"X24" DETOUR / M4-9BL 30"X24" DETOUR

R9-9A 30"X18" SIDEWALK CLOSED AHEAD / M4-9BR 30"X24" DETOUR

PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	TEMPORARY PEDESTRIAN ACCOMMODATIONS - STAGE 1A	SHEET 24	E

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 02 TYP SEC & DETAILS\026101_S1A (PEDESTRIAN).DWG
LAYOUT NAME - 01

PLOT DATE : 10/4/2024 11:29 AM PLOT BY : BEN PIERCE PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 42

2

STAGE 1B PEDESTRIAN TRAFFIC CONTROL

SE CORNER CURB RAMP CONSTRUCTION AT THE INTERSECTION OF THEATER ROAD AND CTH OS/E MAIN ST. CURB RAMP TO BE CONSTRUCTED UNDER OUTSIDE LANE CLOSURE ON CTH OS/E MAIN ST.

N

TRAFFIC CONTROL LEGEND

- SIGN ON TEMPORARY SUPPORT
- WORK SPACE
- PM6 — MARKING LINE REMOVABLE TAPE 6-INCH

DETOUR
M4-9BL
30"X24"

SIDEWALK CLOSED AHEAD
R9-9A
30"X18"

DETOUR
M4-9BR
30"X24"

CTH OS (E MAIN ST)

PM6

SIDEWALK CLOSED
R9-9
24"X12"

THEATER ROAD

MIDWEST DRIVE

DETOUR
M4-9BR
30"X24"

DETOUR
M4-9BL
30"X24"

SIDEWALK CLOSED AHEAD
R9-9A
30"X18"

DETOUR
M4-9BL
30"X24"

2

PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	TEMPORARY PEDESTRIAN ACCOMMODATIONS - STAGE 1B	SHEET 25	E

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 02 TYP SEC & DETAILS\026101_S1B (PEDESTRIAN).DWG
LAYOUT NAME - Plan 1 IN 100 FT

PLOT DATE : 10/4/2024 11:30 AM PLOT BY : BEN PIERCE PLOT NAME : PLOT SCALE : 1 IN:100 FT

2

2

STAGE 2A PEDESTRIAN TRAFFIC CONTROL

NW CORNER CURB RAMP CONSTRUCTION AT THE INTERSECTION OF THEATER ROAD AND CTH OS/E MAIN ST. CURB RAMP TO BE CONSTRUCTED UNDER OUTSIDE LANE CLOSURE ON CTH OS/E MAIN ST.

N

TRAFFIC CONTROL LEGEND

- SIGN ON TEMPORARY SUPPORT
- WORK SPACE
- TRAFFIC FLOW/DETOUR ROUTE
- TEMPORARY PEDESTRIAN CURB RAMP
- PM6 MARKING LINE REMOVABLE TAPE 6-INCH
- TEMPORARY PEDESTRIAN BARRICADE
- XXXX XXXX REMOVABLE MASK OUT TAPE 10-INCH (BLACK)

R9-9A 30"X18" — SIDEWALK CLOSED AHEAD / DETOUR — M4-9BL 30"X24"

M4-9BR 30"X24" — DETOUR

R9-9 24"X12" — SIDEWALK CLOSED

EAGLE BLUFF COURT

WO11-2 48"X48"

WO16-7L 24"X12"

M4-60D 30"X24"

M4-9BR 30"X24" — DETOUR

M4-60R 30"X24"

TEMPORARY DETECTABLE WARNING FIELD

TEMPORARY DETECTABLE WARNING FIELD

CTH OS (E MAIN ST)

PM6

PM6

THEATER ROAD

WO11-2 48"X48"

WO16-7L 24"X12"

PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	TEMPORARY PEDESTRIAN ACCOMMODATIONS - STAGE 2A	SHEET 26	E

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 02 TYP SEC & DETAILS\026101_S2A (PEDESTRIAN).DWG
LAYOUT NAME - 01
PLOT DATE : 10/22/2024 12:14 PM PLOT BY : BEN PIERCE PLOT NAME : PLOT SCALE : 1 IN:100 FT
WISDOT/CADDS SHEET 42

2

2

STAGE 2B PEDESTRIAN TRAFFIC CONTROL

NE CORNER CURB RAMP CONSTRUCTION AT THE INTERSECTION OF THEATER ROAD AND CTH OS/E MAIN ST. CURB RAMP TO BE CONSTRUCTED UNDER OUTSIDE LANE CLOSURE ON CTH OS/E MAIN ST.

N

R9-9A
30"X18"
SIDEWALK CLOSED AHEAD
DETOUR
M4-9BR
30"X24"

EAGLE BLUFF COURT

WO11-2
48"X48"
WO16-7L
24"X12"

SIDEWALK CLOSED
R9-9
24"X12"

M4-60D
30"X24"

DETOUR
M4-9BL
30"X24"

M4-60L
30"X24"

CTH OS (E MAIN ST)

TEMPORARY
DETECTABLE WARNING FIELD

PM6

TEMPORARY
DETECTABLE WARNING FIELD

THEATER ROAD

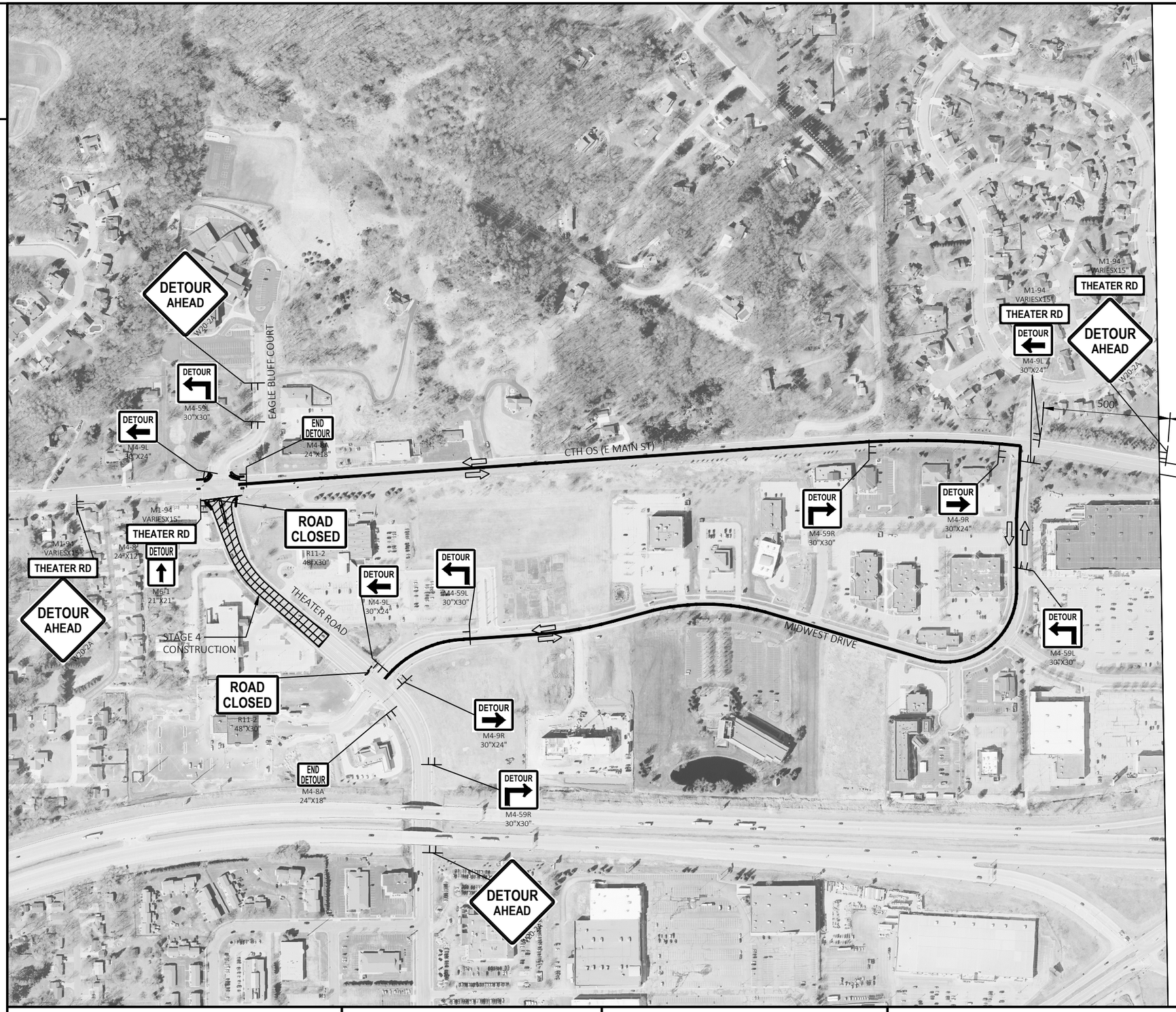
WO11-2
48"X48"
WO16-7L
24"X12"

TRAFFIC CONTROL LEGEND

- SIGN ON TEMPORARY SUPPORT
- WORK SPACE
- TRAFFIC FLOW/DETOUR ROUTE
- TEMPORARY PEDESTRIAN CURB RAMP
- PM6 — MARKING LINE REMOVABLE TAPE 6-INCH
- TEMPORARY PEDESTRIAN BARRICADE
- XXXX XXXX — REMOVABLE MASK OUT TAPE 10-INCH (BLACK)

PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	TEMPORARY PEDESTRIAN ACCOMMODATIONS - STAGE 2B	SHEET 27	E

FILE NAME : X:\KO\O\ONALA\174493\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\59910270\SHEETS\SEC 02 TYP SEC & DETAILS\026101_S2B (PEDESTRIAN).DWG
LAYOUT NAME - 01
PLOT DATE : 10/22/2024 12:19 PM
PLOT BY : BEN PIERCE
PLOT NAME :
PLOT SCALE : 1 IN:100 FT
WISDOT/CADDS SHEET 42



N

PHASE 1:
"THEATER ROAD CLOSED"

PHASE 2:
"FOLLOW DETOUR"

GENERAL NOTES: DETOURS

SEE STANDARD DETAIL DRAWINGS FOR "BARRICADES AND SIGNS FOR MAINLINE CLOSURES," "DETAIL F, DETOUR SIGNING."

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER IN THE FIELD.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE TRAFFIC CONTROL PLAN, INCLUDING PRE-EXISTING SIGNING IN THE VICINITY, SHALL BE COVERED OR REMOVED AS SPECIFIED IN THE PLANS OR AS DIRECTED BY THE ENGINEER IN THE FIELD.

ALL "W" AND "WO" SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

ALL M05 AND M06 ARROW SIGNS SHALL BE THE SAME AS "M" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

DETOUR PLAN LEGEND

- SIGN ON TEMPORARY SUPPORT
- SIGN ON PERMANENT SUPPORT (EXISTING/PROPOSED)
- TYPE III BARRICADE (WITH/WITHOUT SIGN)
- PCMS — TRAFFIC CONTROL SIGN PCMS (PORTABLE CHANGEABLE MESSAGE SIGNS)
- WORK SPACE
- TRAFFIC CONTROL COVERING SIGNS TYPE I, OR TYPE II (BLANK ORANGE PANEL TYPICAL FOR ALL COVERING)
- TRAFFIC FLOW/DETOUR ROUTE

PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	DETOUR - STAGE 4	SHEET 28	E

Estimate Of Quantities

5991-02-71

Line	Item	Item Description	Unit	Total	Qty
0002	204.0110	Removing Asphaltic Surface	SY	29.000	29.000
0004	204.0115	Removing Asphaltic Surface Butt Joints	SY	47.000	47.000
0006	204.0120	Removing Asphaltic Surface Milling	SY	4,325.000	4,325.000
0008	204.0150	Removing Curb & Gutter	LF	132.000	132.000
0010	204.0155	Removing Concrete Sidewalk	SY	180.000	180.000
0012	204.0195	Removing Concrete Bases	EACH	1.000	1.000
0014	204.9060.S	Removing (item description) 01. Removing Traffic Signals (Main St & Theater Road)	EACH	1.000	1.000
0016	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 5991-02-71	EACH	1.000	1.000
0018	213.0100	Finishing Roadway (project) 01. 5991-02-71	EACH	1.000	1.000
0020	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	10.000	10.000
0022	416.0610	Drilled Tie Bars	EACH	16.000	16.000
0024	455.0605	Tack Coat	GAL	306.000	306.000
0026	460.2000	Incentive Density HMA Pavement	DOL	310.000	310.000
0028	460.5224	HMA Pavement 4 LT 58-28 S	TON	490.000	490.000
0030	465.0105	Asphaltic Surface	TON	8.000	8.000
0032	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	132.000	132.000
0034	602.0410	Concrete Sidewalk 5-Inch	SF	1,453.000	1,453.000
0036	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	20.000	20.000
0038	602.0605	Curb Ramp Detectable Warning Field Radial Yellow	SF	57.000	57.000
0040	618.0100	Maintenance and Repair of Haul Roads (project) 01. 5991-02-71	EACH	1.000	1.000
0042	619.1000	Mobilization	EACH	1.000	1.000
0044	625.0500	Salvaged Topsoil	SY	180.000	180.000
0046	628.1504	Silt Fence	LF	95.000	95.000
0048	628.1520	Silt Fence Maintenance	LF	95.000	95.000
0050	628.1905	Mobilizations Erosion Control	EACH	2.000	2.000
0052	628.1910	Mobilizations Emergency Erosion Control	EACH	1.000	1.000
0054	628.2006	Erosion Mat Urban Class I Type A	SY	180.000	180.000
0056	628.7015	Inlet Protection Type C	EACH	8.000	8.000
0058	629.0205	Fertilizer Type A	CWT	0.200	0.200
0060	630.0140	Seeding Mixture No. 40	LB	3.100	3.100
0062	630.0200	Seeding Temporary	LB	4.600	4.600
0064	630.0500	Seed Water	MGAL	3.900	3.900
0066	642.5001	Field Office Type B	EACH	1.000	1.000
0068	643.0300	Traffic Control Drums	DAY	1,468.000	1,468.000
0070	643.0420	Traffic Control Barricades Type III	DAY	144.000	144.000
0072	643.0705	Traffic Control Warning Lights Type A	DAY	288.000	288.000
0074	643.0715	Traffic Control Warning Lights Type C	DAY	368.000	368.000
0076	643.0800	Traffic Control Arrow Boards	DAY	54.000	54.000
0078	643.0900	Traffic Control Signs	DAY	886.000	886.000
0080	643.0920	Traffic Control Covering Signs Type II	EACH	19.000	19.000
0082	643.1050	Traffic Control Signs PCMS	DAY	18.000	18.000
0084	643.3150	Temporary Marking Line Removable Tape 4-Inch	LF	1,274.000	1,274.000
0086	643.3180	Temporary Marking Line Removable Tape 6-Inch	LF	1,032.000	1,032.000
0088	643.3250	Temporary Marking Line Removable Tape 8-Inch	LF	225.000	225.000
0090	643.3550	Temporary Marking Arrow Removable Tape	EACH	24.000	24.000
0092	643.3650	Temporary Marking Word Removable Tape	EACH	1.000	1.000
0094	643.3960	Temporary Marking Removable Mask Out Tape 6-Inch	LF	536.000	536.000
0096	643.3970	Temporary Marking Removable Mask Out Tape 10-Inch	LF	156.000	156.000
0098	643.5000	Traffic Control	EACH	1.000	1.000

5991-02-71

Line	Item	Item Description	Unit	Total	Qty
0100	644.1601	Temporary Pedestrian Curb Ramp	DAY	16.000	16.000
0102	644.1605	Temporary Pedestrian Detectable Warning Field	SF	80.000	80.000
0104	644.1810	Temporary Pedestrian Barricade	LF	368.000	368.000
0106	646.1020	Marking Line Epoxy 4-Inch	LF	3,773.000	3,773.000
0108	646.3020	Marking Line Epoxy 8-Inch	LF	583.000	583.000
0110	646.5020	Marking Arrow Epoxy	EACH	22.000	22.000
0112	646.5120	Marking Word Epoxy	EACH	5.000	5.000
0114	646.6120	Marking Stop Line Epoxy 18-Inch	LF	145.000	145.000
0116	646.8220	Marking Island Nose Epoxy	EACH	4.000	4.000
0118	646.9112	Marking Removal Line Water Blasting 10-Inch	LF	277.000	277.000
0120	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	132.000	132.000
0122	650.8000	Construction Staking Resurfacing Reference	LF	710.000	710.000
0124	650.8501	Construction Staking Electrical Installations (project) 01. 5991-02-71	EACH	1.000	1.000
0126	650.9000	Construction Staking Curb Ramps	EACH	6.000	6.000
0128	650.9500	Construction Staking Sidewalk (project) 01. 5991-02-71	EACH	1.000	1.000
0130	650.9911	Construction Staking Supplemental Control (project) 01. 5991-02-71	EACH	1.000	1.000
0132	652.0225	Conduit Rigid Nonmetallic Schedule 40 2-Inch	LF	153.000	153.000
0134	652.0235	Conduit Rigid Nonmetallic Schedule 40 3-Inch	LF	410.000	410.000
0136	652.0615	Conduit Special 3-Inch	LF	684.000	684.000
0138	653.0164	Pull Boxes Non-Conductive 24x42-Inch	EACH	11.000	11.000
0140	653.0905	Removing Pull Boxes	EACH	11.000	11.000
0142	654.0101	Concrete Bases Type 1	EACH	6.000	6.000
0144	654.0217	Concrete Control Cabinet Bases Type 9 Special	EACH	1.000	1.000
0146	655.0230	Cable Traffic Signal 5-14 AWG	LF	90.000	90.000
0148	655.0240	Cable Traffic Signal 7-14 AWG	LF	3,167.000	3,167.000
0150	655.0260	Cable Traffic Signal 12-14 AWG	LF	960.000	960.000
0152	655.0305	Cable Type UF 2-12 AWG Grounded	LF	864.000	864.000
0154	655.0515	Electrical Wire Traffic Signals 10 AWG	LF	1,648.000	1,648.000
0156	655.0900	Traffic Signal EVP Detector Cable	LF	229.000	229.000
0158	656.0201	Electrical Service Meter Breaker Pedestal (location) 01. East Main Street & Theater Road	EACH	1.000	1.000
0160	657.0100	Pedestal Bases	EACH	7.000	7.000
0162	657.0425	Traffic Signal Standards Aluminum 15-FT	EACH	1.000	1.000
0164	657.0430	Traffic Signal Standards Aluminum 10-FT	EACH	6.000	6.000
0166	658.0175	Traffic Signal Face 5S 12-Inch	EACH	2.000	2.000
0168	658.0416	Pedestrian Signal Face 16-Inch	EACH	6.000	6.000
0170	658.5070	Signal Mounting Hardware (location) 01. East Main Street & Theater Road	EACH	1.000	1.000
0172	690.0150	Sawing Asphalt	LF	381.000	381.000
0174	690.0250	Sawing Concrete	LF	53.000	53.000
0176	SPV.0060	Special 01. APS Push Button	EACH	6.000	6.000
0178	SPV.0060	Special 02. Furnish & Install Traffic Signal Cabinet & Controller (Main St & Theater Rd)	EACH	1.000	1.000
0180	SPV.0090	Special 01. Marking Crosswalk Epoxy Transverse Line 12-Inch	LF	485.000	485.000

ALL ITEMS ARE CATEGORY 0010.

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REMOVING CURB AND GUTTER

STATION	LOCATION	204.0150 REMOVING CURB & GUTTER LF	REMARKS
SW RAMP	RT	28	-
SE RAMP	RT	23	-
NE RAMP	LT	41	-
NW RAMP	LT	40	-
PROJECT TOTALS		132	

REMOVING ASPHALTIC PAVEMENT

STATION	LOCATION	204.0110 REMOVING ASPHALTIC SURFACE SY	204.0115 REMOVING ASPHALTIC SURFACE BUTT JOINTS SY	204.0120 REMOVING ASPHALTIC SURFACE MILLING SY	REMARKS
200+50 - 207+60	THEATER RD	-	47	4325	-
CURB RAMPS	LT&RT	29	-	-	-
PROJECT TOTALS		29	47	4325	

REMOVING SIDEWALK

STATION	LOCATION	204.0155 REMOVING CONCRETE SIDEWALK SY	REMARKS
SW RAMP	RT	30	-
SE RAMP	RT	26	-
NE RAMP	LT	53	-
NW RAMP	LT	53	-
EAST MEDIAN	LT&RT	7	-
SOUTH MEDIAN	LT&RT	5	-
WEST MEDIAN	LT&RT	6	-
PROJECT TOTALS		180	

ASPHALTIC ITEMS

STATION	LOCATION	455.0605 TACK COAT GAL	460.5224 HMA PAVEMENT 4 LT 58-28 S TON	465.0105 ASPHALTIC SURFACE TON	REMARKS
200+50 - 207+60	THEATER RD	306	490	-	-
CURB RAMPS	LT&RT	-	-	8	-
PROJECT TOTALS		306	490	8	

CONCRETE CURB AND GUTTER

STATION	LOCATION	305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	416.0610 DRILLED TIE BARS EACH	601.0411 CONCRETE CURB AND GUTTER 30-INCH TYPE D LF	REMARKS
SW RAMP	RT	-	4	28	-
SE RAMP	RT	-	4	23	-
NE RAMP	LT	-	4	41	-
NW RAMP	LT	-	4	40	-
UNDISTRIBUTED	-	10	-	-	-
PROJECT TOTALS		10	16	132	

CONCRETE SIDEWALK

STATION	LOCATION	602.0410 CONCRETE SIDEWALK 5-INCH SF	REMARKS
SW RAMP	RT	245	-
SE RAMP	RT	200	-
NE RAMP	LT	440	-
NW RAMP	LT	402	-
EAST MEDIAN	LT&RT	61	-
SOUTH MEDIAN	LT&RT	48	-
WEST MEDIAN	LT&RT	57	-
PROJECT TOTALS		1453	

ALL ITEMS ARE CATEGORY 0010.

3

3

MARKING LINE ITEMS

STATION - STATION	LOCATION	SOLID WHITE LF	SOLID YELLOW LF	646.1020 MARKING LINE EPOXY 4-INCH 12.5' LINE 37.5' SKIP WHITE LF	1.0' LINE 3.0' SKIP WHITE LF	646.3020 MARKING LINE EPOXY 8-INCH LF	646.5020 MARKING ARROW EPOXY EACH	646.5120 MARKING WORD EPOXY EACH	646.6120 MARKING STOP LINE EPOXY 18-INCH LF	SPV.0090.01 MARKING CROSSWALK EPOXY TRANSVERSE LINE 12-INCH LF	646.8220 MARKING ISLAND NOSE EPOXY EACH	646.9112 MARKING REMOVAL LINE WATER BLASTING 10-INCH LF	REMARKS
ENTIRE PROJECT	LT&RT	1,069	2,232	462	10	583	22	5	145	485	4	277	-
PROJECT TOTAL				3,773		583	22	5	145	485	4	277	

TEMPORARY PAVEMENT MARKING ITEMS

STATION - STATION	LOCATION	643.3150 TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH LF	643.3250 TEMPORARY MARKING LINE REMOVABLE TAPE 8-INCH LF	643.3550 TEMPORARY MARKING ARROW REMOVABLE TAPE EACH	643.3650 TEMPORARY MARKING WORD REMOVABLE TAPE EACH	643.3960 TEMPORARY MARKING REMOVABLE MASK OUT TAPE 6-INCH LF	643.3970 TEMPORARY MARKING REMOVABLE MASK OUT TAPE 10-INCH LF	REMARKS
ENTIRE PROJECT	LT&RT	1,274	225	24	1	536	156	-
	PROJECT TOTAL	1,274	225	24	1	536	156	

STAKING ITEMS

STATION	LOCATION	650.8000 CONSTRUCTION STAKING RESURFACING REFERENCE LF	650.8501 CONSTRUCTION STAKING ELECTRICAL INSTALLATIONS (5991-02-71) EACH	650.9000 CONSTRUCTION STAKING CURB RAMPS EACH	650.9911 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (5991-02-71) EACH	650.5500 CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER LF	650.9500 CONSTRUCTION STAKING SIDEWALK (5991-02-71) EACH	REMARKS
ENTIRE PROJECT	LT&RT	710	1	6	1	-	1	-
SW RAMP	RT	-	-	-	-	28	-	-
SE RAMP	RT	-	-	-	-	23	-	-
NE RAMP	LT	-	-	-	-	41	-	-
NW RAMP	LT	-	-	-	-	40	-	-
PROJECT TOTALS		710	1	6	1	132	1	

DETECTABLE WARNING FIELDS

STATION	LOCATION	602.0505 CURB RAMP DETECTABLE WARNING FIELD YELLOW SF	602.0605 CURB RAMP DETECTABLE WARNING FIELD RADIAL YELLOW SF	REMARKS
SW RAMP	RT	10	-	-
SE RAMP	RT	10	-	-
NE RAMP	LT	-	31	-
NW RAMP	LT	-	26	-
PROJECT TOTALS		20	57	

SAWING

STATION	LOCATION	690.0150 SAWING ASPHALT LF	690.0250 SAWING CONCRETE LF	REMARKS
BEGIN PROJECT	THEATER ROAD	63	-	-
SW RAMP	LT	32	10	-
SE RAMP	RT	26	10	-
NE RAMP	LT	50	18	-
NW RAMP	RT	50	15	-
END PROJECT	THEATER ROAD	160	-	-
PROJECT TOTALS		381	53	

ALL ITEMS ARE CATEGORY 0010.

FINISHING ITEMS

STATION - STATION	625.0500 SALVAGED TOPSOIL SY	629.0205 FERTILIZER TYPE A CWT	630.0140 SEEDING MIXTURE NO. 40 LB	630.0200 SEEDING TEMPORARY LB	630.0500 SEED WATER MGAL
ENTIRE PROJECT	140	0.1	2.5	3.7	3.1
UNDISTRIBUTED QTY	40	0.1	0.6	0.9	0.8
PROJECT TOTALS	180	0.2	3.1	4.6	3.9

EROSION CONTROL

STATION	LT/RT	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF	628.2006 EROSION MAT URBAN CLASS I TYPE A SY	628.7015 INLET PROTECTION TYPE C EACH	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATONS EMERGENCY EROSION CONTROL EACH	REMARKS
PROJET LENGTH		95	95	140	7	2	1	-
UNDISTRIBUTED		-	-	40	1	-	-	-
PROJECT TOTAL		95	95	180	8	2	1	

TRAFFIC CONTROL

LOCATION	APPROX. SERVICE PERIOD DAYS	643.0300 TRAFFIC CONTROL DRUMS QTY.	643.0300 DAYS	643.0420 TRAFFIC CONTROL BARRICADES TYPE III QTY.	643.0420 DAYS	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A QTY.	643.0705 DAYS	643.0715 TRAFFIC CONTROL WARNING LIGHTS TYPE C QTY.	643.0715 DAYS	643.0800 TRAFFIC CONTROL ARROW BOARDS QTY.	643.0800 DAYS	643.0900 TRAFFIC CONTROL SIGNS* QTY.	643.0900 DAYS	643.0920 TRAFFIC CONTROL COVERING SIGNS TYPE II SIGNS	CYCLES	EACH	643.1050 TRAFFIC CONTROL SIGNS PCMS QTY.	643.1050 DAYS	NOTES
PRE-WARNING	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	14	PRE-WARN PRIOR TO CONSTRUCTION START
STAGE 1	12	35	420	2	24	4	48	6	72	1	12	14	168	4	1	4	0	0	-
STAGE 2	16	32	512	2	32	4	64	11	176	2	32	12	192	4	1	4	0	0	-
STAGE 3	10	44	440	6	60	12	120	12	120	1	10	21	210	5	1	5	0	0	-
STAGE 4	2	48	96	14	28	28	56	0	0	0	0	20	40	6	1	6	2	4	DETOUR
			1468		144		288		368		54		610			19		18	
PROJECT TOTALS			1468		144		288		368		54		610			19		18	

* ADDITIONAL QUANTITY SHOWN ELSEWHERE

PEDESTRIAN TRAFFIC CONTROL

LOCATION	APPROX. SERVICE PERIOD DAYS	643.0900 TRAFFIC CONTROL SIGNS* QTY.	643.0900 DAYS	643.3180 TEMPORARY MARKING LINE REMOVABLE TAPE 6-INCH LF	644.1601 TEMPORARY PEDESTRIAN CURB RAMP QTY.	644.1601 DAYS	644.1605 TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD SF	644.1810 TEMPORARY PEDESTRIAN BARRICADE LF	NOTES
STAGE 1A	6	9	54	0	0	0	0	0	-
STAGE 1B	6	9	54	162	0	0	0	0	-
STAGE 2A	8	11	88	527	1	8	40	198	-
STAGE 2B	8	10	80	343	1	8	40	170	-
			276	1032		16	80	368	
PROJECT TOTALS			276	1032		16	80	368	

* ADDITIONAL QUANTITY SHOWN ELSEWHERE

ALL ITEMS ARE CATEGORY 0010.

CONDUIT

FROM	TO	652.0225 CONDUIT RIGID NONMETALLIC SCHEDULE 40 2-INCH LF	652.0235 CONDUIT RIGID NONMETALLIC SCHEDULE 40 3-INCH LF	652.0615 CONDUIT SPECIAL 3-INCH LF
EAST MAIN STREET & THEATER ROAD				
CB1	PB1	--	48	--
PB1	PB2	--	90	--
PB2	PB3	--	--	96
PB3	PB4	--	--	90
PB4	PB5	--	74	--
PB5	PB6	--	--	124
PB6	PB7	--	--	64
PB7	PB8	--	96	--
PB8	PB9	--	--	160
PB9	PB10	--	102	--
PB10	PB11	--	--	82
PB11	PB1	--	--	68
PB1	SB1	5	--	--
PB2	SB2	11	--	--
PB2	SB3	5	--	--
PB3	SB4	5	--	--
PB4	SB5	5	--	--
PB5	SB6	15	--	--
PB5	SB7	5	--	--
PB6	SB8	5	--	--
PB7	SB9	5	--	--
PB7	SB10	26	--	--
PB8	SB11	11	--	--
PB8	SB12	5	--	--
PB9	SB13	5	--	--
PB9	SB14	13	--	--
PB10	SB15	22	--	--
PB10	SB16	5	--	--
PB11	SB17	5	--	--
ITEM TOTALS		153	410	684

PULL BOXES NON-CONDUCTIVE

PULL BOX NUMBER	653.0164 PULL BOXES NON-CONDUCTIVE 24 X 42 - INCH EACH
MAIN STREET & THEATER ROAD	
PB1	1
PB2	1
PB3	1
PB4	1
PB5	1
PB6	1
PB7	1
PB8	1
PB9	1
PB10	1
PB11	1
ITEM TOTALS	**11**

PULL BOX REMOVALS

EXISTING PULL BOX NUMBER	653.0905 REMOVING PULL BOXES EACH
MAIN STREET & THEATER ROAD	
PB1	1
PB2	1
PB3	1
PB4	1
PB5	1
PB6	1
PB7	1
PB8	1
PB9	1
PB10	1
PB11	1
ITEM TOTALS	**11**

TRAFFIC SIGNAL CABLE NO. 14 (BELOW GROUND)

FROM	TO	*655.0240 CABLE TRAFFIC SIGNAL 7 - 14 AWG LF	655.0260 CABLE TRAFFIC SIGNAL 12 - 14 AWG LF
MAIN STREET & THEATER ROAD			
CB1	SB1	59	--
CB1	SB2	115	--
CB1	SB3	110	--
CB1	SB4	--	173
CB1	SB5	243	--
CB1	SB6	297	--
CB1	SB7	297	--
CB1	SB8	--	368
CB1	SB9	425	--
CB1	SB10	434	--
CB1	SB11	324	--
CB1	SB12	--	318
CB1	SB13	222	--
CB1	SB14	230	--
CB1	SB15	172	--
CB1	SB16	165	--
CB1	SB17	--	101
	ITEM TOTALS	**3093**	**960**

*ADDITIONAL QUANTITY SHOWN ELSEWHERE

CONCRETE BASE REMOVALS

EXISTING BASE NUMBER	204.0195 REMOVING CONCRETE BASES EACH
MAIN STREET & THEATER ROAD	
CB1	1
ITEM TOTALS	**1**

CONCRETE BASES

BASE NUMBER	654.0101 CONCRETE BASES TYPE 1 EACH	654.0217 CONCRETE CONTROL CABINET BASES TYPE 9 SPECIAL EACH
MAIN STREET & THEATER ROAD		
SB2	1	--
SB6	1	--
SB10	1	--
SB11	1	--
SB14	1	--
SB15	1	--
CB1	--	1
ITEM TOTALS	**6**	**1**

TRAFFIC SIGNAL CABLE NO. 14 (ABOVE GROUND)

FROM	TO	655.0230 CABLE TRAFFIC SIGNAL 5 - 14 AWG LF	*655.0240 CABLE TRAFFIC SIGNAL 7 - 14 AWG LF
MAIN STREET & THEATER ROAD			
SB2	HEAD 21	15	--
SB6	HEAD 16	15	;--
SB10	HEAD 17	15	--
SB11	HEAD 18	15	--
SB12	HEAD 11	--	23
SB13	HEAD 12	--	51
SB14	HEAD 19	15	--
SB15	HEAD 20	15	--
ITEM TOTALS		**90**	**74**

*ADDITIONAL QUANTITY SHOWN ELSEWHERE

ALL ITEMS ARE CATEGORY 0010.

3

TRAFFIC SIGNAL EVP DETECTOR CABLE

FROM	TO	655.0900 TRAFFIC SIGNAL EVP DETECTOR CABLE LF
MAIN STREET & THEATER ROAD		
CB1	SB1	99
CB1	SB3	130
ITEM TOTALS		**229**

ELECTRICAL SERVICE METER BREAKER PEDESTAL

LOCATION	656.0201.01 ELECTRICAL SERVICE METER BREAKER PEDESTAL EACH
MAIN STREET & THEATER ROAD	1
ITEM TOTALS	**1**

CAST BASES, POLES, MONOTUBE ARMS, LUMINAIRES, AND PUSH BUTTONS

SIGNAL BASE NUMBER	657.0100 PEDESTAL BASES EACH	657.0425 TRAFFIC SIGNAL STANDARDS ALUMINUM 15 - FT EACH	657.0430 TRAFFIC SIGNAL STANDARDS ALUMINUM 10 - FT EACH	SPV.0060.01 APS PUSH BUTTON EACH
MAIN STREET & THEATER ROAD				
SB2	1	--	1	1
SB6	1	--	1	1
SB10	1	--	1	1
SB11	1	--	1	1
SB12	1	1	--	--
SB14	1	--	1	1
SB15	1	--	1	1
ITEM TOTALS	**7**	**1**	**6**	**6**

ELECTRIC WIRE TRAFFIC SIGNALS

FROM	TO	655.0515 ELECTRICAL WIRE TRAFFIC SIGNALS 10 AWG LF
MAIN STREET & THEATER ROAD		
CB1	SB1	59
SB1	SB2	101
SB2	SB3	41
SB3	SB4	100
SB4	SB5	105
SB5	SB6	170
SB6	SB7	54
SB7	SB8	125
SB8	SB9	97
SB9	SB10	67
SB10	SB11	125
SB11	SB12	40
SB12	SB13	130
SB13	SB14	42
SB14	SB15	126
SB15	SB16	61
SB16	SB17	104
SB17	CB1	101
ITEM TOTALS		**1648**

SIGNAL MOUNTING HARDWARE

LOCATION	658.5070.01 SIGNAL MOUNTING HARDWARE EACH
MAIN STREET & THEATER ROAD	1
ITEM TOTALS	**1**

LIGHTING WIRE

FROM	TO	655.0305 CABLE TYPE UF 2 - 12 AWG GROUNDED LF
MAIN STREET & THEATER ROAD		
CB1	SB4	173
CB1	SB8	368
CB1	SB13	222
CB1	SB17	101
ITEM TOTALS		**864**

TRAFFIC SIGNAL AND PEDESTRIAN FACES, AND BACKPLATES

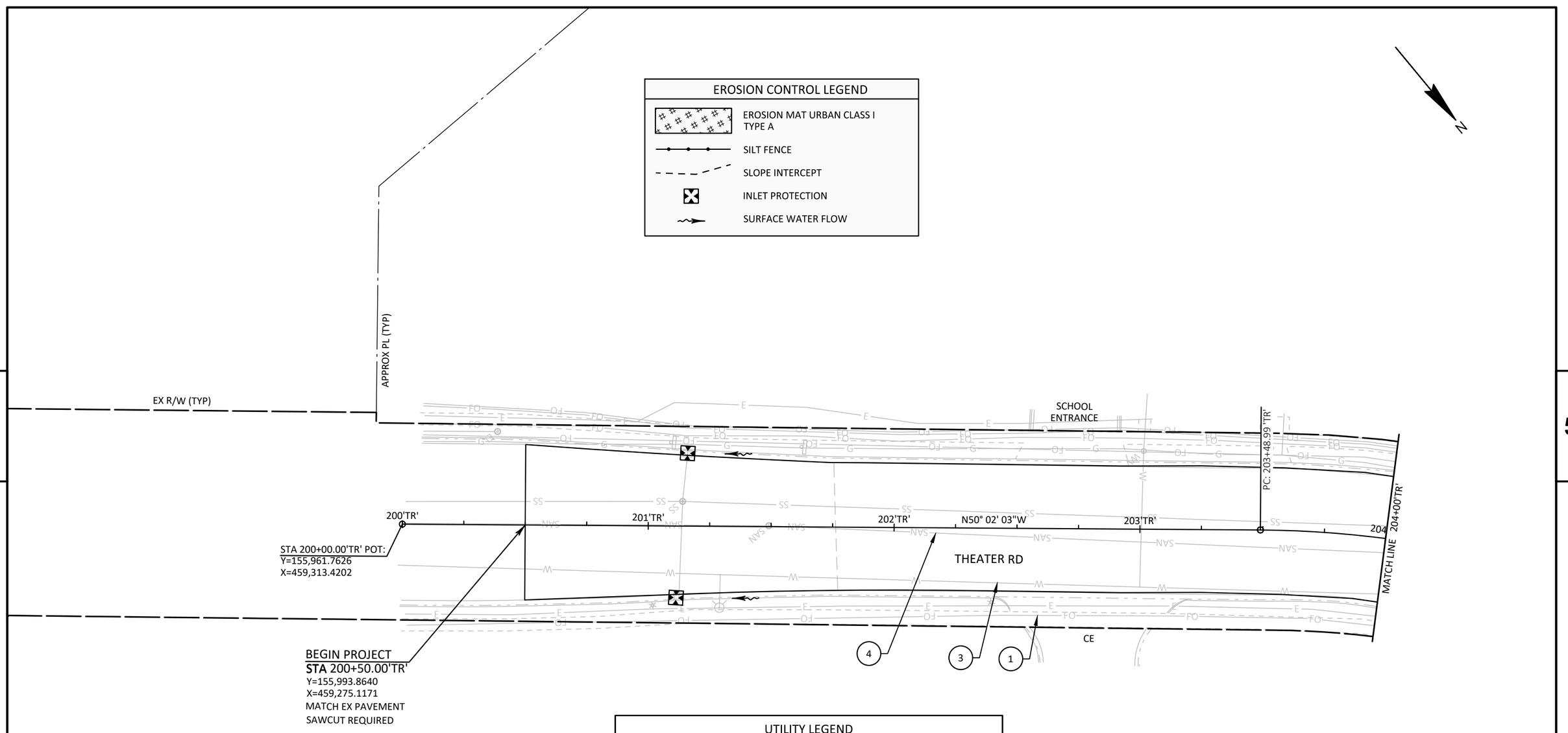
SIGNAL HEAD NUMBER	SIGNAL BASE NUMBER	658.0175 TRAFFIC SIGNAL FACE 5S-12 INCH EACH	658.0416 PEDESTRIAN SIGNAL FACE 16-INCH EACH
MAIN STREET & THEATER ROAD			
11	12	1	--
12	13	1	--
16	6	--	1
17	10	--	1
18	11	--	1
19	14	--	1
20	15	--	1
21	2	--	1
ITEM TOTALS		**2**	**6**

REMOVING TRAFFIC SIGNALS

LOCATION	204.9060.S.01 REMOVING TRAFFIC SIGNALS (MAIN ST & THEATER RD) EACH
MAIN ST & THEATER RD	1
ITEM TOTALS	**1**

FURNISH AND INSTALL TRAFFIC SIGNAL CABINET AND CONTROLLER

	SPV.0060.02 FURNISH & INSTALL TRAFFIC SIGNAL CABINET & CONTROLLER EACH
MAIN ST & THEATER RD	1
ITEM TOTALS	**1**



UTILITY LEGEND	
1 BRIGHTSPEED (FO)	5 LEMONWEIR VALLEY TELEPHONE (FO)
2 SPECTRUM (FO)	6 TDS TELECOM (FO)
3 CITY OF ONALASKA (WATER)	7 XCEL ENERGY (E)
4 CITY OF ONALASKA (SANITARY SEWER)	8 XCEL ENERGY (GAS)
	9 METRONET (FO)

PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	PLAN	SHEET 36 E

FILE NAME : X:\KO\O\ONALA\174493\5-final-dsgn\51-drawings\40-TransHwy\59910270\sheets\SEC 05 Plan & Profile\050201-pn.dwg PLOT DATE : 10/4/2024 11:33 AM PLOT BY : SEH LAYOUT NAME : 01 PLOT SCALE : 1.0 IN = 40.0 FT

WISDOT/CADDS SHEET 44

EROSION CONTROL LEGEND

- EROSION MAT URBAN CLASS I TYPE A
- SILT FENCE
- SLOPE INTERCEPT
- INLET PROTECTION
- SURFACE WATER FLOW

UTILITY LEGEND

1. BRIGHTSPEED (FO)
2. SPECTRUM (FO)
3. CITY OF ONALASKA (WATER)
4. CITY OF ONALASKA (SANITARY SEWER)
5. LEMONWEIR VALLEY TELEPHONE (FO)
6. TDS TELECOM (FO)
7. XCEL ENERGY (E)
8. XCEL ENERGY (GAS)
9. METRONET (FO)

N

CTH OS (E MAIN ST)

100'MS' N85° 48' 48"E

STA 100+00.00'MS' POT:
Y=156,575.6181
X=458,703.2990

BEGIN CONSTRUCTION
STA 100+75.00'MS'

CONC SIDE-WALK 5-INCH

PI STA 101+00.73'MS'
Δ = 0°52'39" LT
Y = 156581.2123
X = 458803.8700

100+88.86'MS'
42.25'RT

+77.4

+90.6

101'MS'

CAUTION

+77.9

MATCH EX SIDEWALK (TYP)

100+84.78'MS'
32.05'LT

APPROX PL (TYP)

BM1

SLOPE INTERCEPT (TYP)

SILT FENCE (TYP)

INT-EAGLE BLUFF CT-NW-FLAG

BM3

CP2

CAUTION

N85° 56' 19"E

STA 207+97.85'TR'
STA 101+60.52'MS'
Y=156,585.4468
X=458,863.5077

INT-THEATER RD-SW-FLAG

CONC SIDE-WALK 5-INCH

+23.0

+33.0

207'TR'

N85° 56' 19"E

102'MS'

101+99.07'MS'
66.72'LT

EAGLE BLUFF CT

INT-EAGLE BLUFF CT-NE-FLAG

PI: 204+85.58'TR'

206'TR'

N14° 00' 09"W

THEATER RD

206+84.33'TR'
27.72'RT

CP3

205'TR'

C1

CP1

+48.0

+60.0

CONC SIDE-WALK 4-INCH

CTH OS (E MAIN ST)

EX R/W (TYP)

END CONSTRUCTION
STA 102+65.00'MS'

PI STA 102+38.06'MS'
Δ = 0°01'25" RT
Y = 156590.9390
X = 458940.8590

103'MS'

N85° 57' 44"E

BM2

STA 103+75.00'MS' POT:
Y=156,600.5814
X=459,077.4587

INT-THEATER RD-SE-FLAG

END PROJECT
STA 207+59.74'TR'
MATCH EX PAVEMENT
SAWCUT REQUIRED

TR

MATCH LINE 204+00'TR'

BENCH MARKS

NO.	STATION	ELEV.	DESCRIPTION
BM1	100+89.88'MS', 56.00L	740.467	SPIKE IN PPOL (PNT 2030)
BM2	103+15.51'MS', 35.91L	748.321	TOP NUT OF HYD (PNT 2031)
BM3	207+08.55'TR', 49.32L	741.618	TOP NUT OF HYD (PNT 2032)

CONTROL POINTS

NO.	STATION	ELEV.	DESCRIPTION
CP1	102+64.27'MS', 80.54L	743.592	SPIKE
CP2	207+15.87'TR', 50.45L	738.154	SPIKE
CP3	204+37.10'TR', 36.02L	732.543	SPIKE

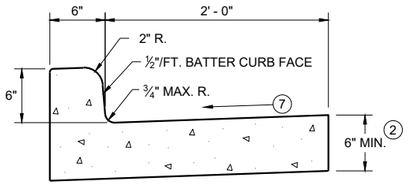
PROJECT NO: 5991-02-71	HWY: LOCAL (THEATER RD)	COUNTY: LA CROSSE	PLAN	SHEET 37 E

FILE NAME : X:\KO\O\ONALA\174493\5-final-dsgn\51-drawings\40-TransHwy\59910270\sheets\SEC 05 Plan & Profile\050201-pn.dwg PLOT DATE : 10/4/2024 11:33 AM PLOT BY : SEH LAYOUT NAME : 02 PLOT SCALE : 1.0 IN = 40.0 FT

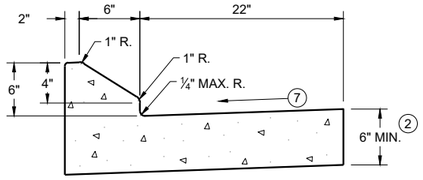
WISDOT/CADDS SHEET 44

Standard Detail Drawing List

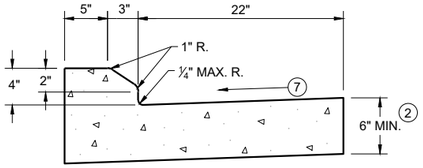
08D01-23A	CONCRETE CURB & GUTTER
08D01-23B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-21A	CURB RAMPS TYPES 1 AND 1-A
08D05-21B	CURB RAMPS TYPES 2 AND 3
08D05-21C	CURB RAMPS TYPES 4A AND 4A1
08D05-21D	CURB RAMPS TYPE 4B AND 4B1
08D05-21E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-21F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-21G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
09B02-10	CONDUIT
09B16-02	PULL BOX NON-CONDUCTIVE
09C02-09	CONCRETE BASES, TYPES 1, 2, 5, & 6
09C03-04	TRANSFORMER/PEDESTAL BASES
09C06-07	CONCRETE CONTROL CABINET BASE, TYPE 9, SPECIAL
09E01-15A	POLE MOUNTINGS FOR TRAFFIC SIGNALS TYPE 2
09E01-15C	POLE MOUNTINGS FOR TRAFFIC SIGNALS AND LIGHTING UNITS, TYPE 4
09E01-15G	HARDWARE DETAILS FOR POLE MOUNTINGS
09E06-05	TRAFFIC SIGNAL STANDARD POLY BRACKET MOUNTINGS (TYPICAL) 13 FT. OR 15 FT.
09E07-06	TRAFFIC SIGNAL STANDARD PEDESTRIAN AND FLASHER TYPICAL MOUNTING DETAILS
13C19-03	HMA LONGITUDINAL JOINTS
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-09C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C07-15B	PAVEMENT MARKING WORDS
15C07-15C	PAVEMENT MARKING ARROWS
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C08-23B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C08-23C	PAVEMENT MARKING (TURN LANES)
15C08-23D	PAVEMENT MARKING (TURN LANES)
15C18-09A	MEDIAN ISLAND MARKINGS
15C18-09B	PAVEMENT MARKINGS, MEDIAN ISLAND NOSE
15C33-05	STOP LINE AND CROSSWALK PAVEMENT MARKING
15D20-08A	TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY
15D20-08B	TRAFFIC CONTROL, SINGLE RIGHT LANE CLOSURE, UNDIVIDED NON-FREEWAY/EXPRESSWAY
15D20-08C	TRAFFIC CONTROL, SINGLE LEFT LANE CLOSURE, UNDIVIDED NON-FREEWAY/EXPRESSWAY
15D21-07A	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE
15D21-07B	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE
15D30-09B	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09K	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION



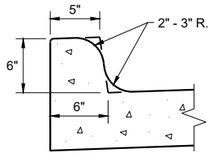
TYPES A (1) & D



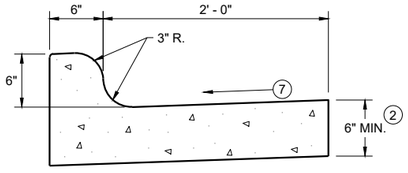
6" SLOPED CURB TYPES G (1) & J



4" SLOPED CURB TYPES G (1) & J

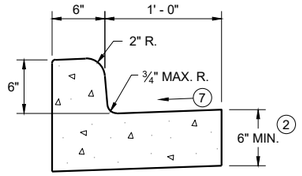


TYPES K (1) & L
(OPTIONAL CURB SHAPE)



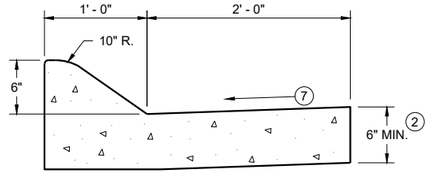
TYPES K (1) & L

CONCRETE CURB AND GUTTER 30"

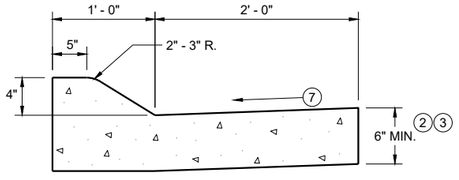


TYPES A (1) & D

CONCRETE CURB AND GUTTER 18"

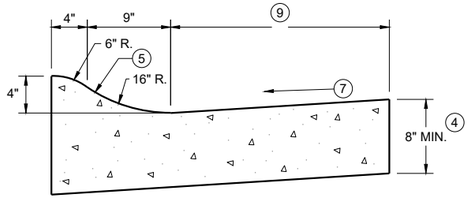


6" SLOPED CURB TYPES A (1) & D



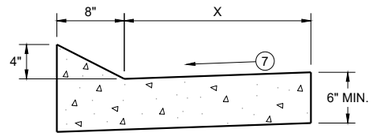
4" SLOPED CURB TYPES A (1) & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R (1) & T

TBT & TBTT	X
30"	22"
36"	28"

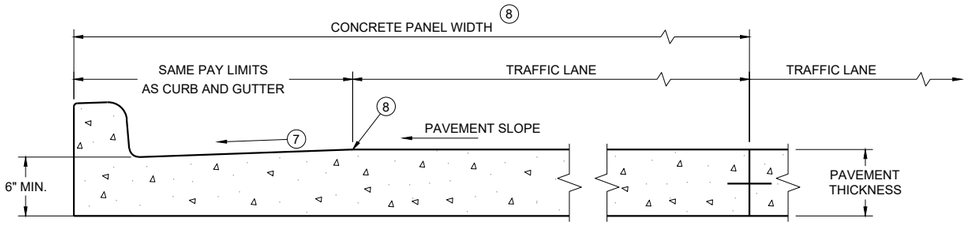


TYPES TBT & TBTT (1)

CONCRETE CURB AND GUTTER

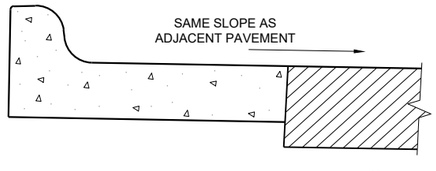
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT WITH INTEGRAL CURB AND GUTTER *

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER (6)
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.

(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(3) USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.

(4) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(5) UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.

(6) WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.

(7) USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.

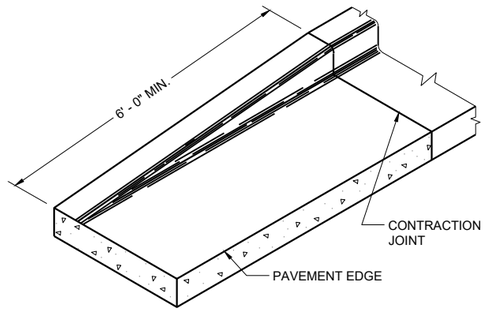
(8) INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.

(9) CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES

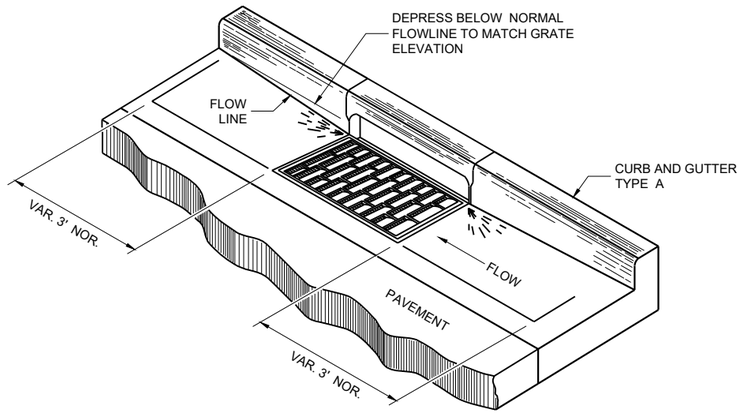
CONCRETE CURB AND GUTTER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

SDD 08D01-23a



END SECTION CURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS

(TYPICAL H INLET COVER SHOWN)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

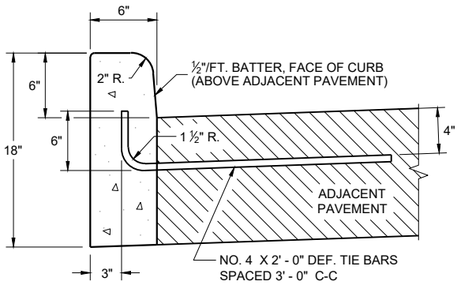
UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.

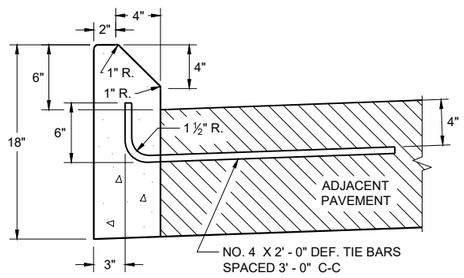
(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(10) REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.

(11) PLACE 1" THICK EXPANSION JOINT MATERIAL BETWEEN VERTICAL FACE CURB TYPES EXTENDING FROM THE TOP OF CURB TO 1 INCH BELOW THE ADJOINING CONCRETE SURFACE. RIGID CONCRETE STRUCTURES INCLUDE RAISED CONCRETE MEDIANS, CONCRETE SAFETY ISLANDS, SPLITTER ISLANDS, OR LOCATIONS IDENTIFIED ON THE PLANS.

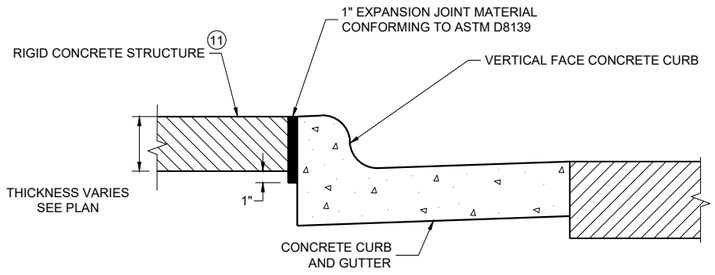


TYPES A (1) & D

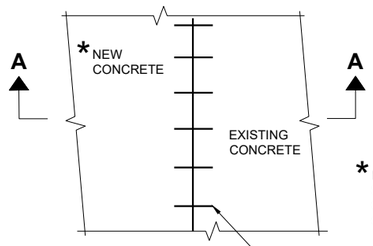


TYPES G (1) & J

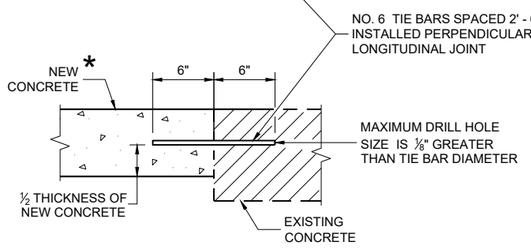
CONCRETE CURB



EXPANSION JOINT DETAIL FOR VERTICAL CURB ABUTTING A RIGID STRUCTURE (11)

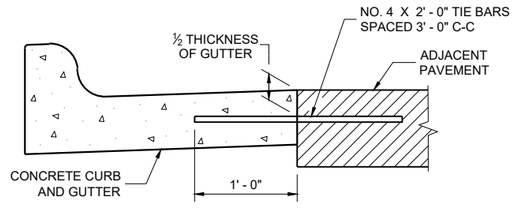


PLAN VIEW

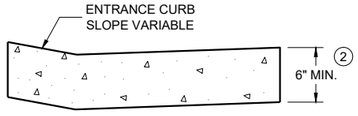


SECTION A - A

TIE BARS DRILLED INTO EXISTING PAVEMENT



TYPICAL TIE BAR LOCATION (1)



DRIVEWAY ENTRANCE CURB (10)

(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Rodnery Taylor — ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

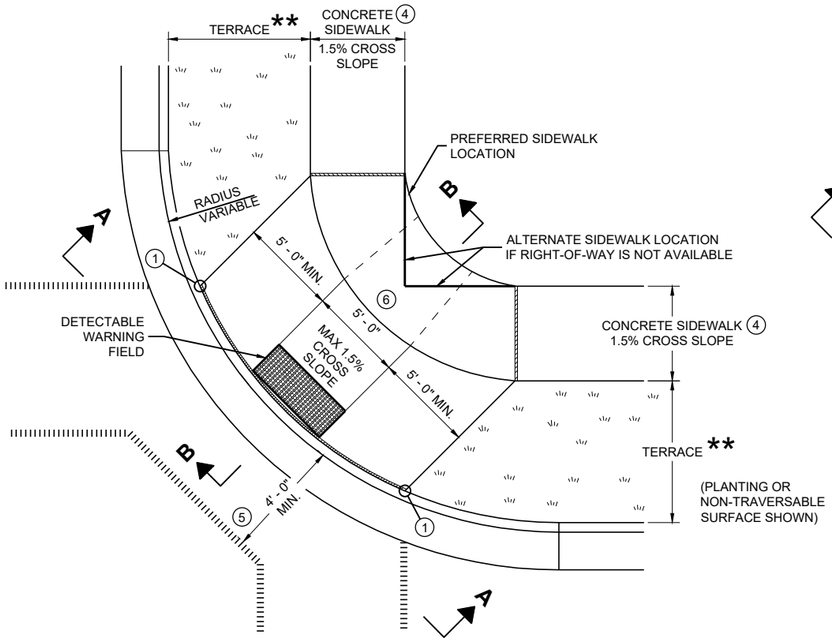
FHWA

6

SDD 08D01-23b

6

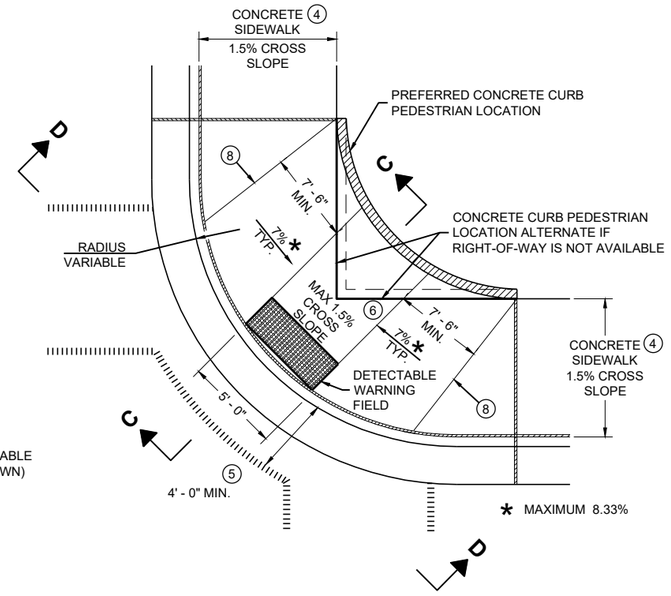
SDD 08D01-23b



PLAN VIEW

CURB RAMP TYPE 1

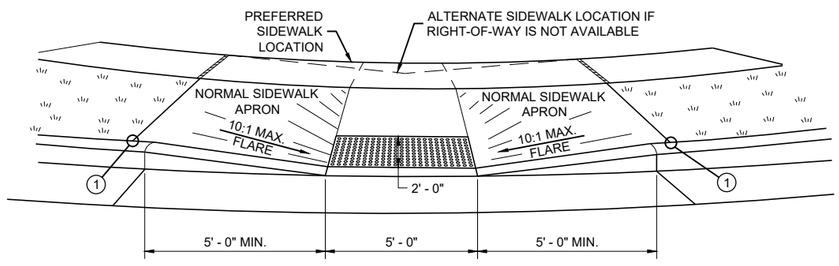
(CENTER OF CORNER RADIUS)



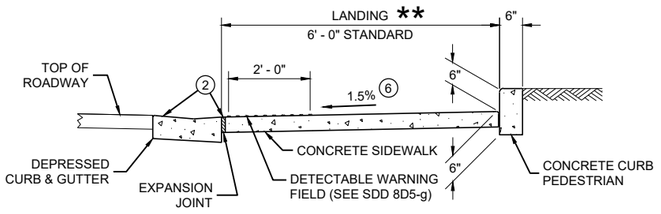
PLAN VIEW

CURB RAMP TYPE 1 - A

(NO TERRACE)

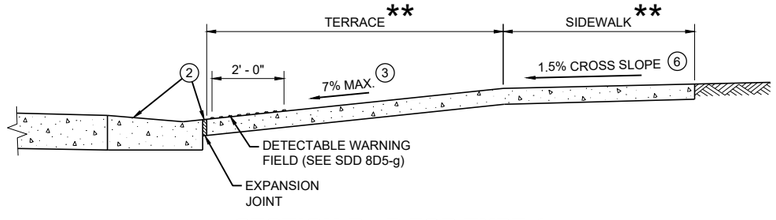


VIEW A - A FOR TYPE 1

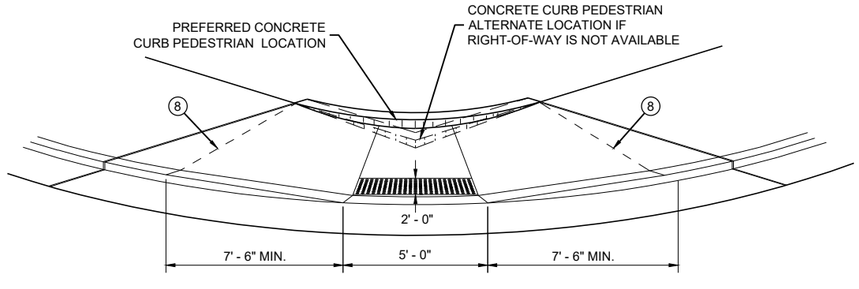


SECTION C - C FOR TYPE 1 - A

** WIDTH SHOWN ELSEWHERE IN THE PLANS



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.

TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FILED SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

(1) THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ½ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(5) PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA. 4 FOOT WIDTH IS MEASURED FROM THE FLANGE LINE

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

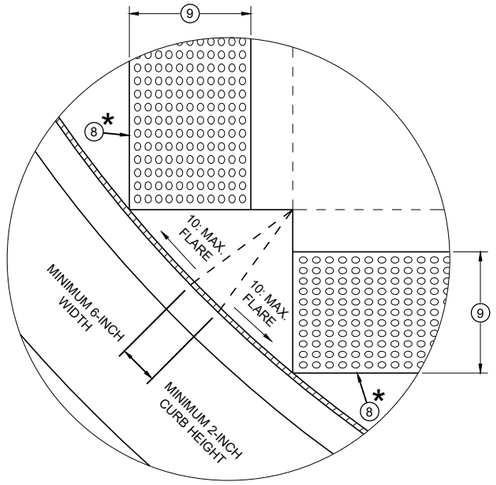
LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

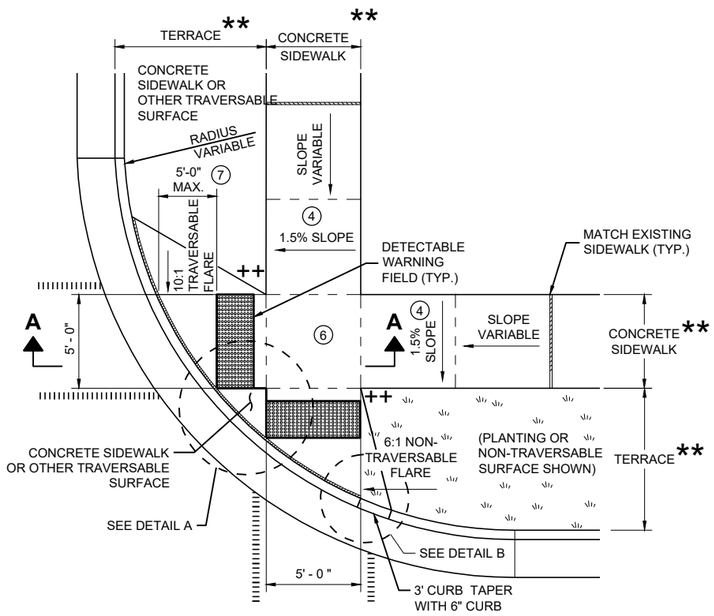
CURB RAMPS TYPE 1 AND 1-A

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

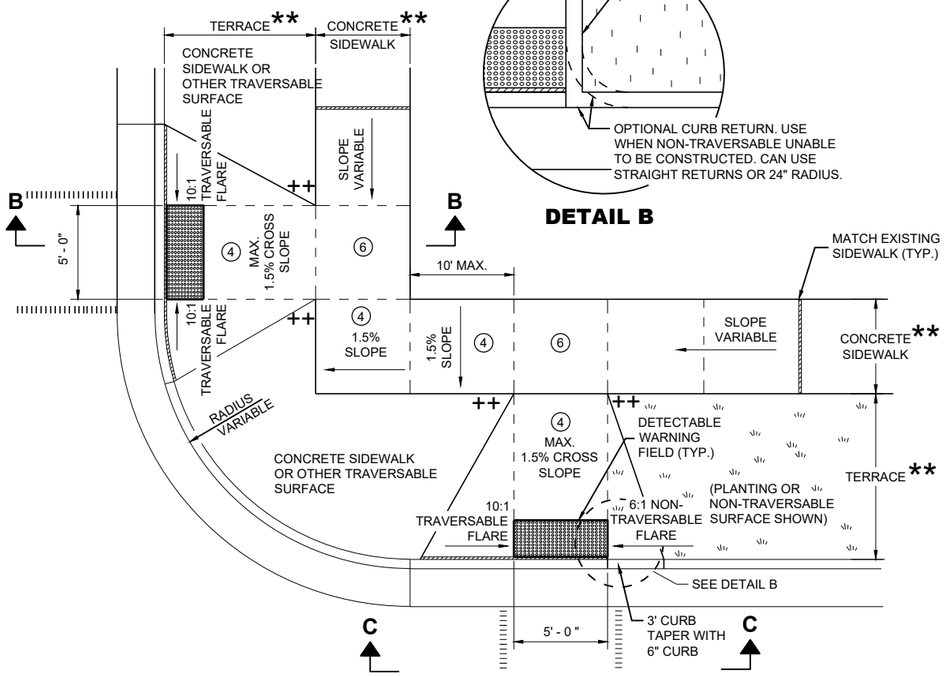
6
SDD 08D05-21a



DETAIL A



PLAN VIEW
CURB RAMP TYPE 2
(CENTER OF CORNER RADIUS)



DETAIL B

PLAN VIEW
CURB RAMP TYPE 3
(OUTSIDE OF CROSSWALK AREA)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.

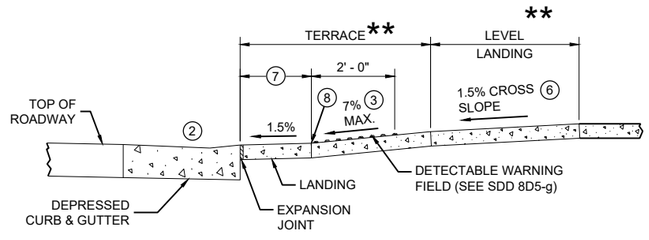
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.

(7) WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

(9) WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

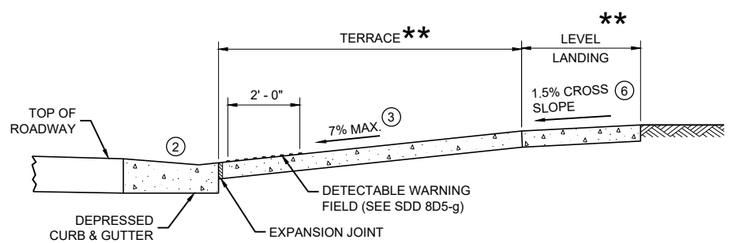


SECTION A - A FOR TYPE 2

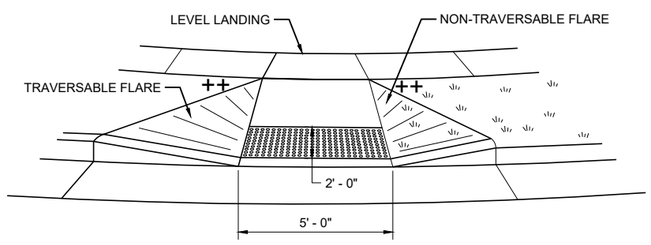
* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

** WIDTH SHOWN ELSEWHERE IN THE PLANS

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

LEGEND

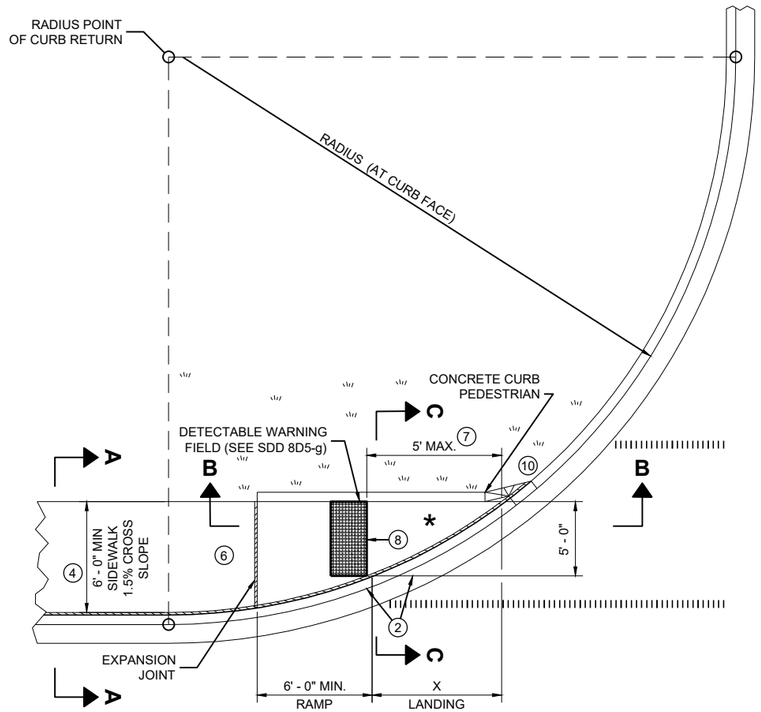
½" EXPANSION JOINT SIDEWALK

CONTRACTION JOINT SIDEWALK

PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS TYPE 2 AND 3

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

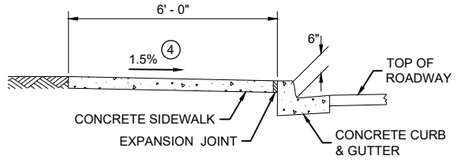


PLAN VIEW

CURB RAMP TYPE 4A

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"

INTERMEDIATE RADII CAN BE INTERPOLATED



SECTION A - A FOR TYPE 4A

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

(7) WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

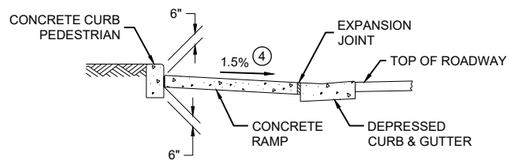
(10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

LEGEND

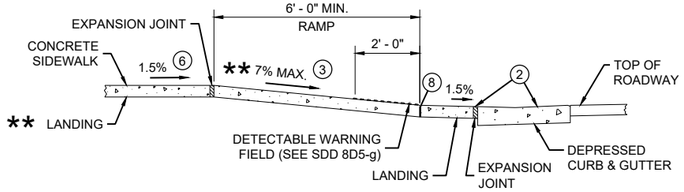
½" EXPANSION JOINT SIDEWALK

CONTRACTION JOINT SIDEWALK

PAVEMENT MARKING CROSSWALK (WHITE)

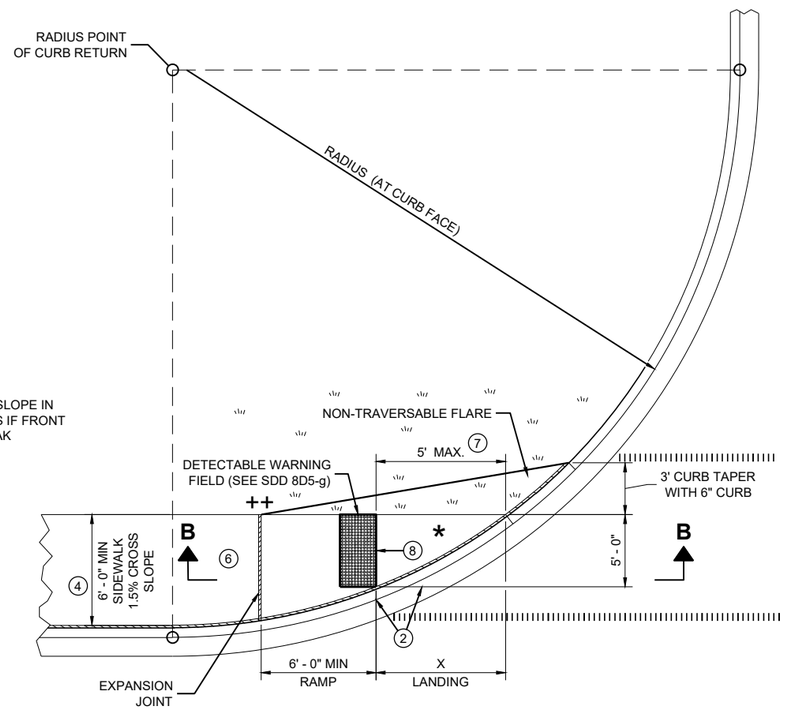


SECTION C - C FOR TYPE 4A



SECTION B - B FOR TYPE 4A AND TYPE 4A1

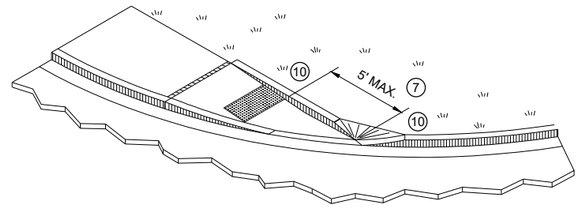
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



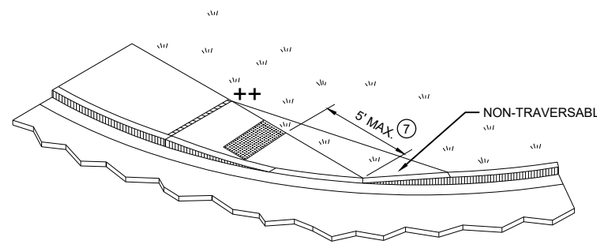
* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

PLAN VIEW

CURB RAMP TYPE 4A1



ISOMETRIC VIEW FOR TYPE 4A

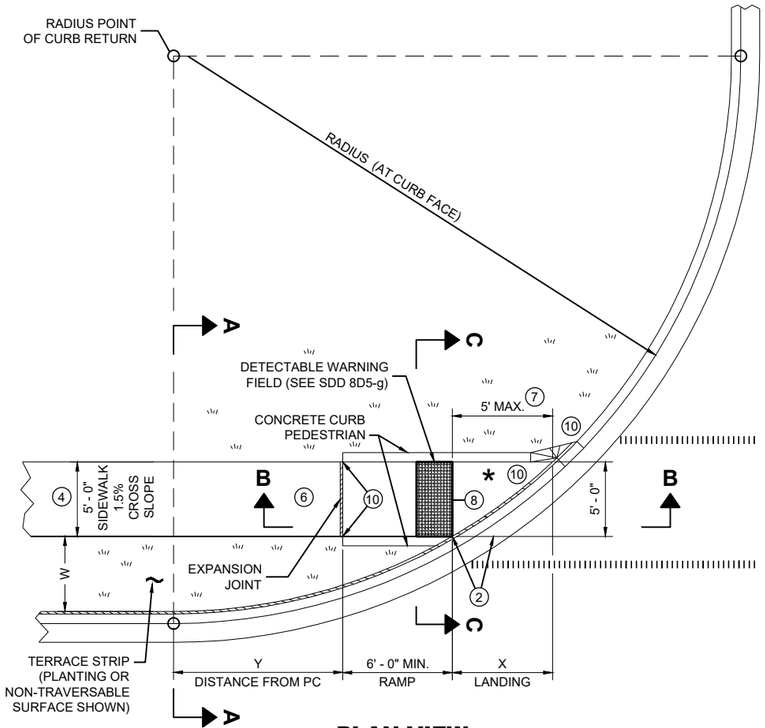


ISOMETRIC VIEW FOR TYPE 4A1

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE

CURB RAMPS
TYPE 4A AND 4A1

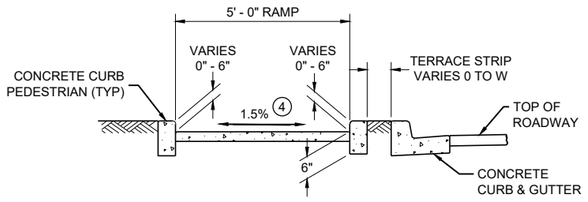
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PLAN VIEW
CURB RAMP TYPE 4B

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET			4' - 11 1/2"	5' - 1 3/4"	4' - 3 1/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET									4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET															4' - 10 3/4"	19' - 8 1/4"

INTERMEDIATE RADII CAN BE INTERPOLATED
DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH



SECTION C - C FOR TYPE 4B

LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

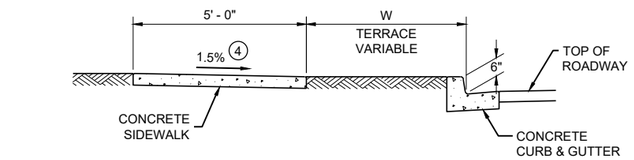
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

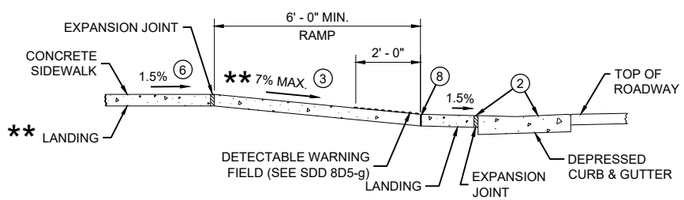
(7) WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

(10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

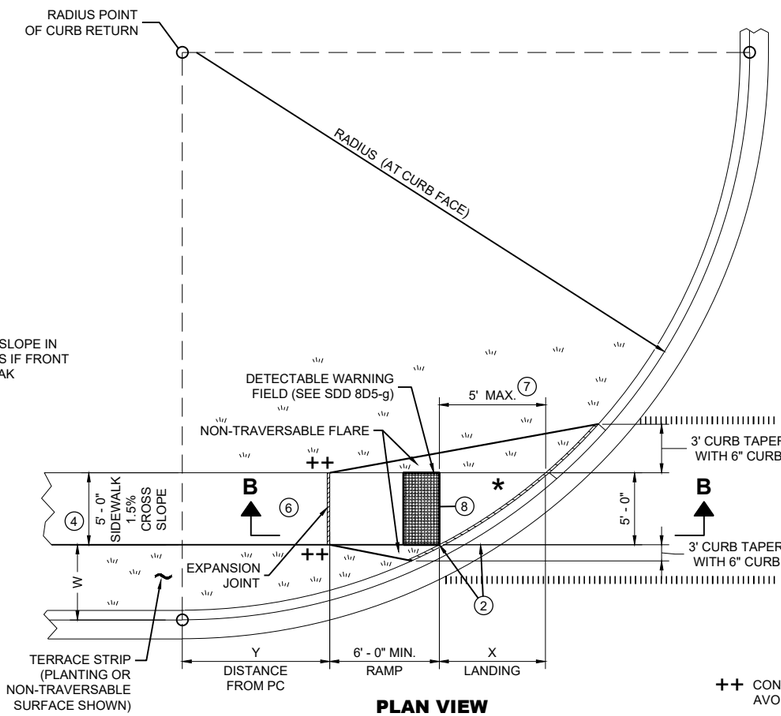


SECTION A - A FOR TYPE 4B



SECTION B - B FOR TYPE 4B AND TYPE 4B1

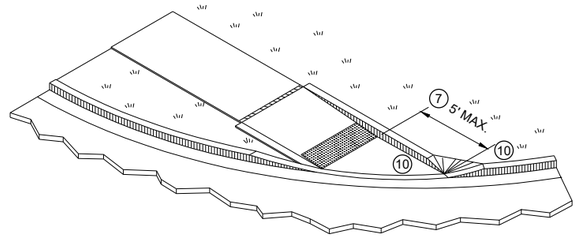
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



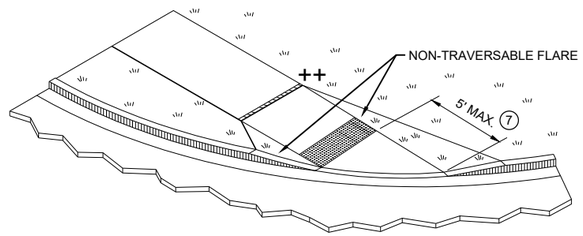
PLAN VIEW
CURB RAMP TYPE 4B1

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



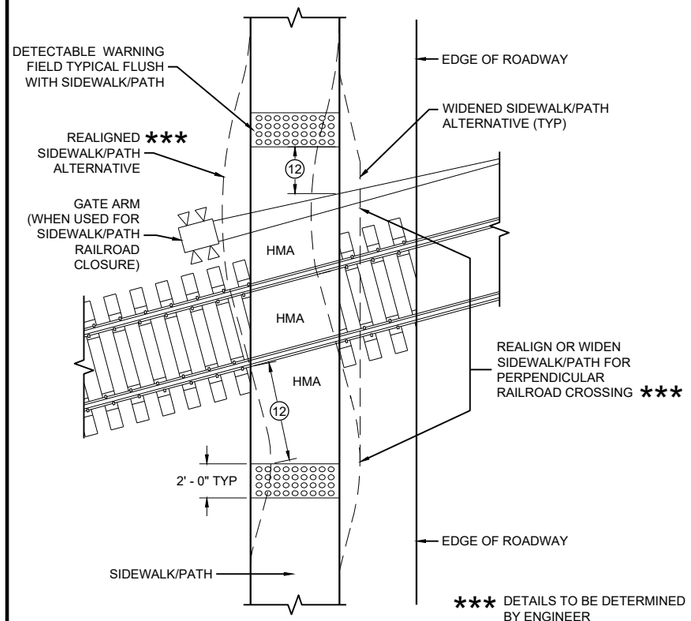
ISOMETRIC VIEW FOR TYPE 4B



ISOMETRIC VIEW FOR TYPE 4B1

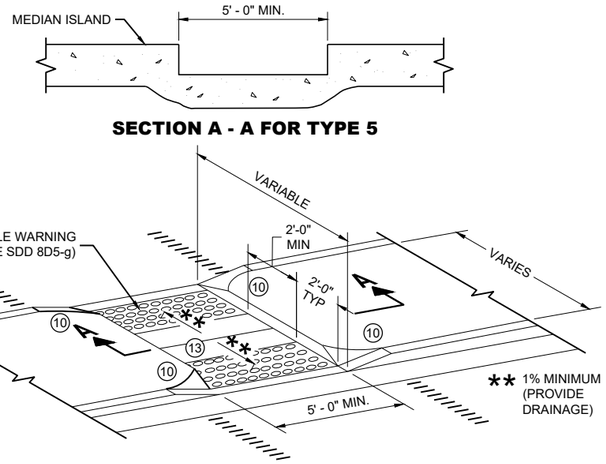
CURB RAMPS
TYPE 4B AND 4B1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



CURB RAMP TYPE 8

DETECTABLE WARNINGS FOR SIDEWALKS OR SHARED USE PATHS AT RAILROAD CROSSINGS



SECTION A - A FOR TYPE 5

CURB RAMP TYPE 5

MEDIAN ISLAND NON-ELEVATED PEDESTRIAN CROSSING

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

(10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

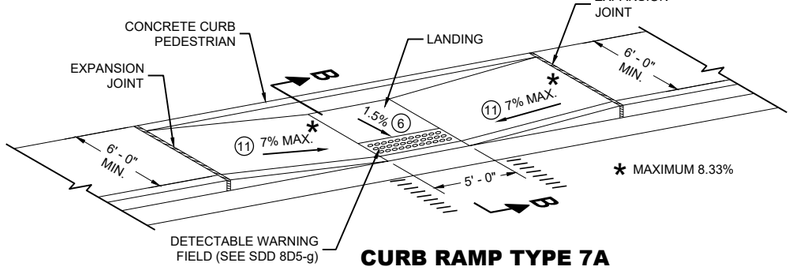
(11) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.

(12) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK/PATH. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD TRACK IS 15 FEET MAXIMUM AND 12 FEET MINIMUM, 15 FEET TYPICAL FROM THE NEAREST RAIL.

(13) DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STEET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

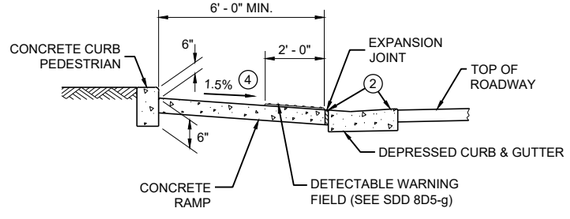
LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

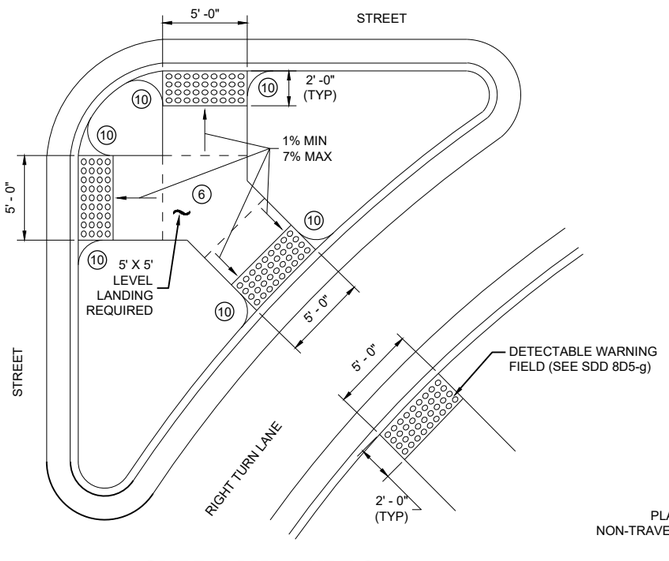


CURB RAMP TYPE 7A

FOR INTERSECTIONS AND MID BLOCK CROSSINGS



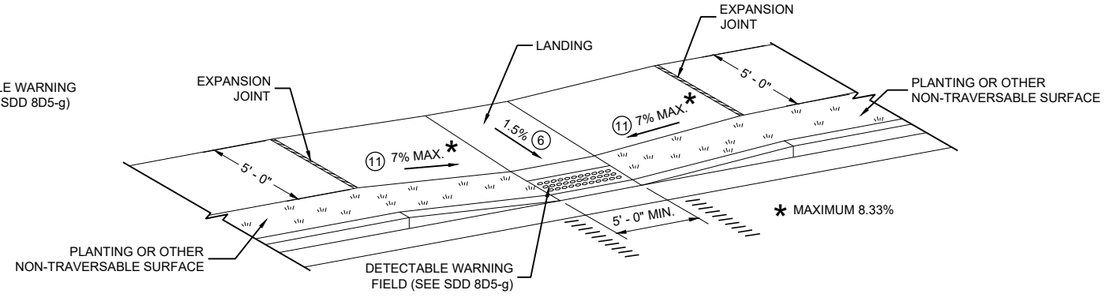
SECTION B - B FOR TYPE 7A



CURB RAMP TYPE 6

DETECTABLE WARNING AT ISLANDS

REFER TO GENERAL NOTES (2) AND (3) FOR ALL ISLAND CURB RAMPS



CURB RAMP TYPE 7B

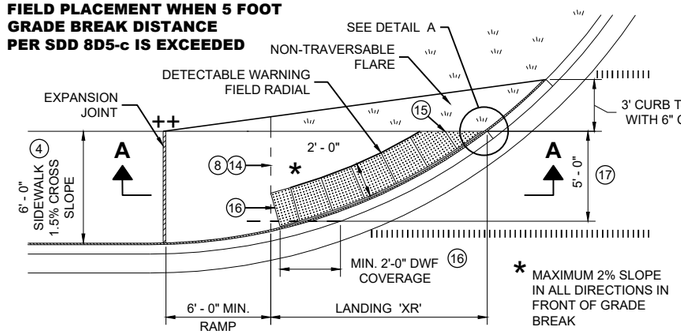
FOR INTERSECTIONS AND MID BLOCK CROSSINGS

CURB RAMPS TYPE 5, 6, 7A, 7B & 8

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

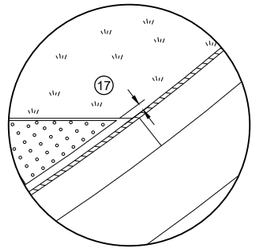
SDD 08D05-21e

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED

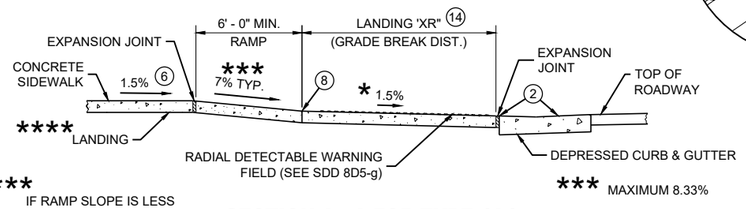


PLAN VIEW

CURB RAMP TYPE 4A1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)



DETAIL A



**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

SECTION A - A FOR TYPE 4A1

LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B CURB RAMPS ARE NOT SHOWN.

REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AD ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

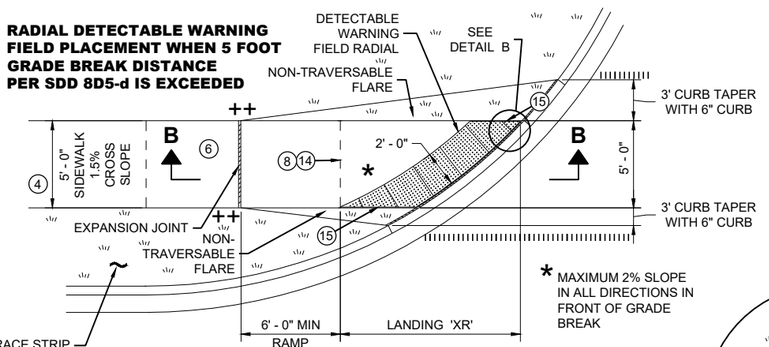
(14) CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.

(15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN ⅛" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

(16) USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.

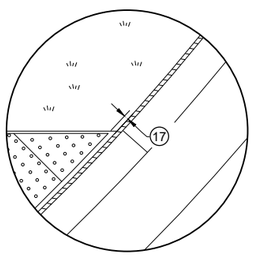
(17) A MAXIMUM 3 INCH CONCRETE BORDER WIDTH IS ALLOWABLE IN FRONT OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED

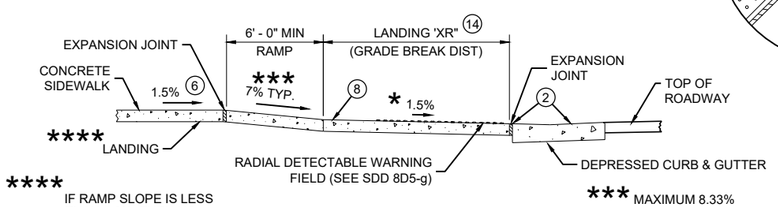


PLAN VIEW

CURB RAMP TYPE 4B1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)



DETAIL B

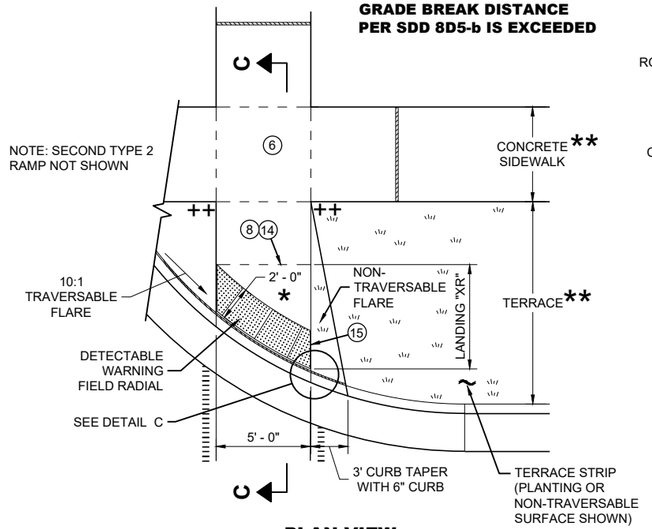


**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

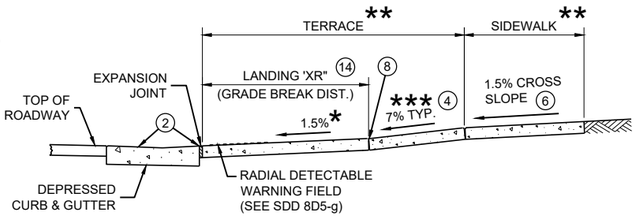
SECTION B - B FOR TYPE 4B1

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED

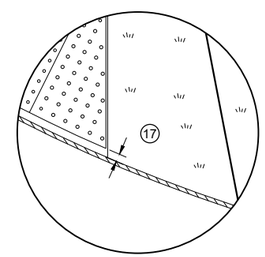


PLAN VIEW

CURB RAMP TYPE 2
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)
(ON LINE WITH SIDEWALK)



SECTION C - C FOR TYPE 2



DETAIL C

* MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

** WIDTH SHOWN ELSEWHERE IN THE PLANS

*** MAXIMUM 8.33%

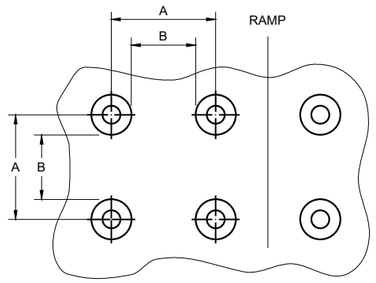
++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE

CURB RAMPS
RADIAL DETECTABLE WARNING

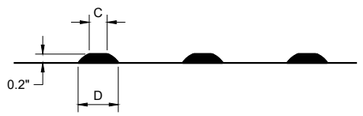
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

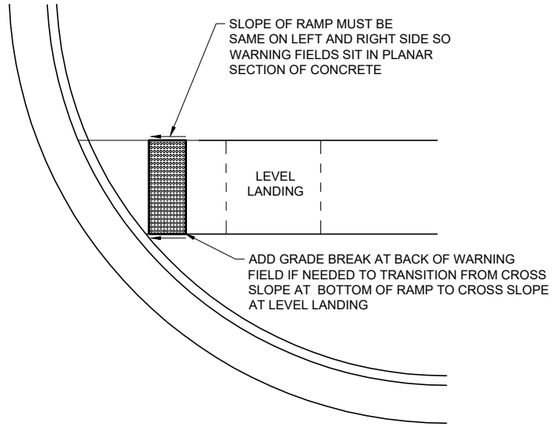
* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.



PLAN VIEW



ELEVATION VIEW



DETECTABLE WARNING FIELD
PLANAR INSTALLATION

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

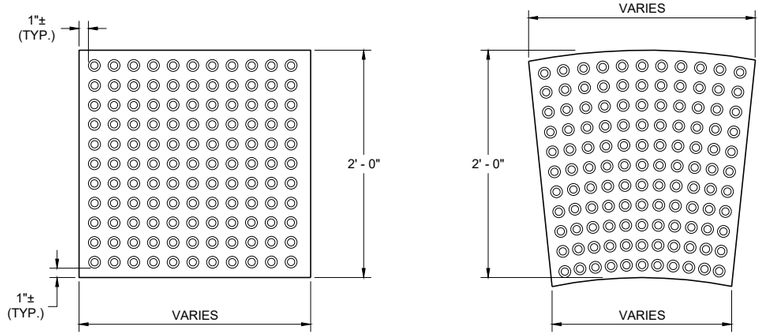
FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

(15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN ⅛" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

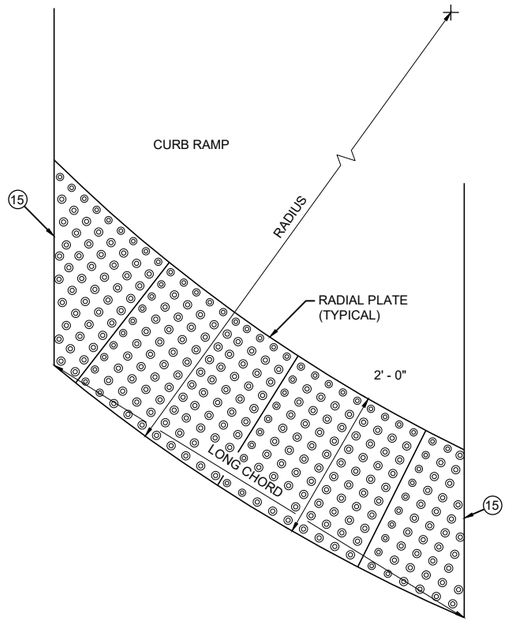
TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL



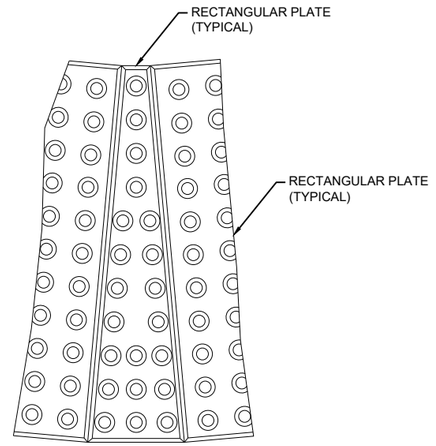
RECTANGULAR PLATES

RADIAL PLATES

PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)



PLAN VIEW
RADIAL DETECTABLE WARNING FIELD ATTRIBUTES



PLAN VIEW
RADIAL WEDGE PLATE CONNECTION DETAIL

CURB RAMPS
RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

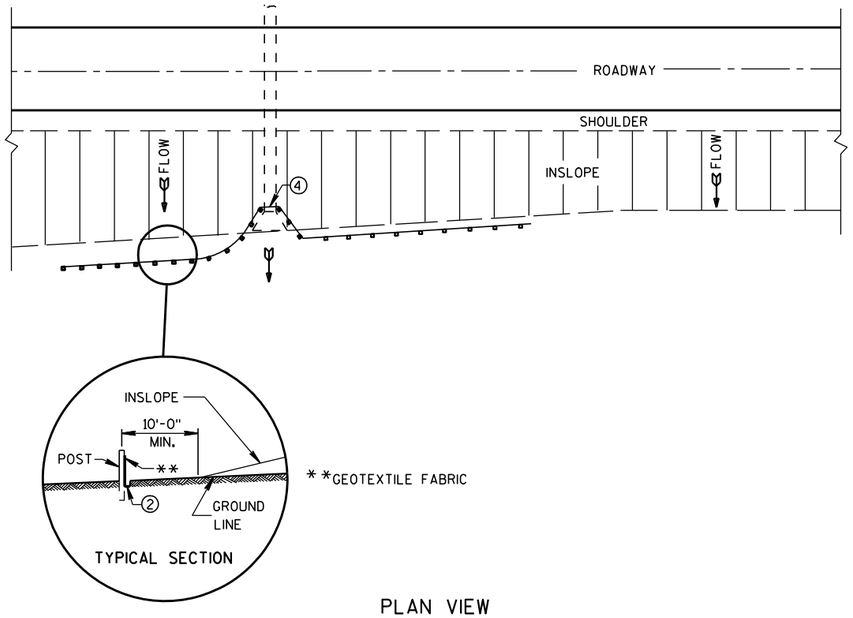
APPROVED

July 2023 — DATE

/S/ Rodney Taylor — ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

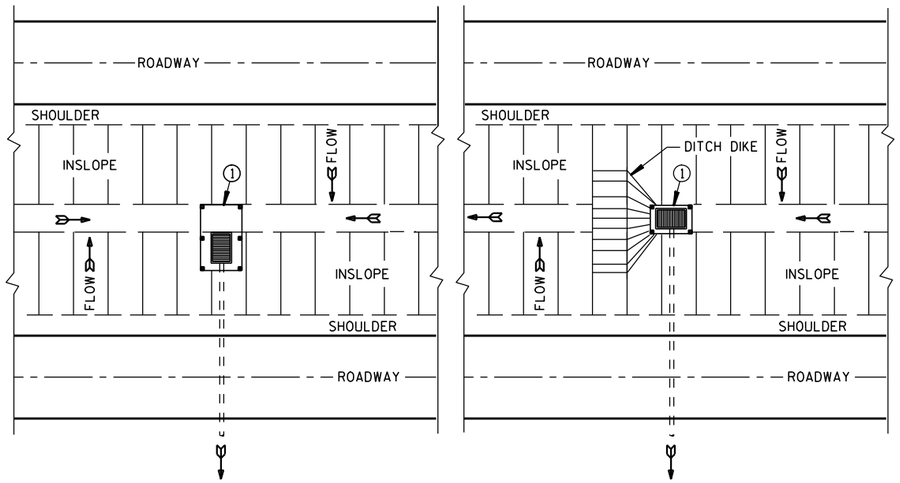
FHWA

6
SDD 08D05-21g



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



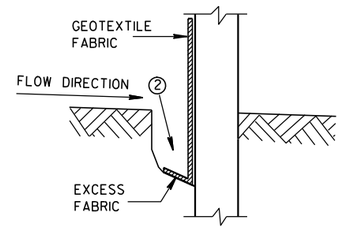
PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

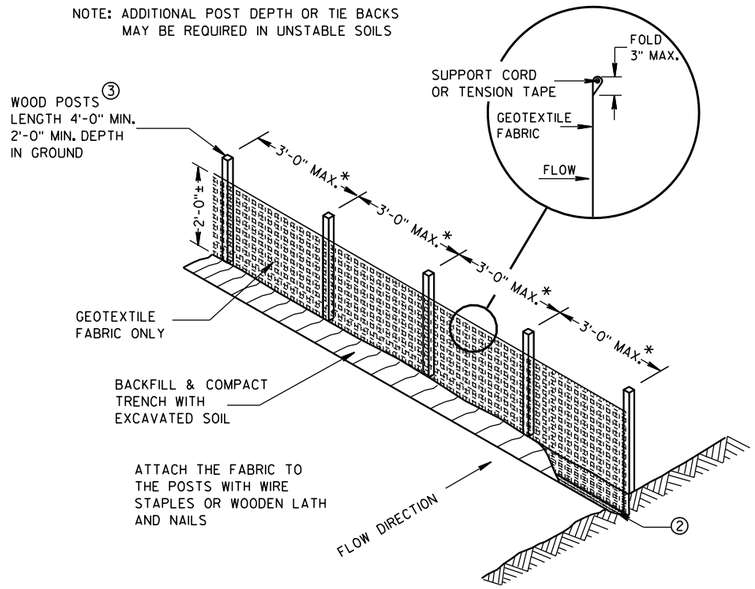
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

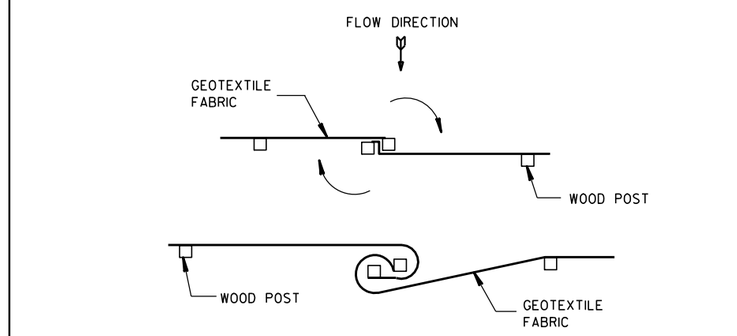
1. HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
2. FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
3. WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.
4. SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
5. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



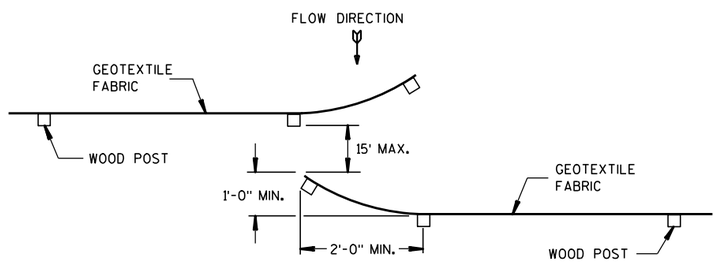
TRENCH DETAIL



SILT FENCE

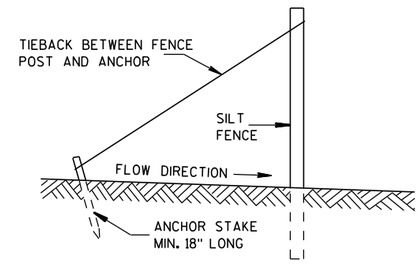


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE 5



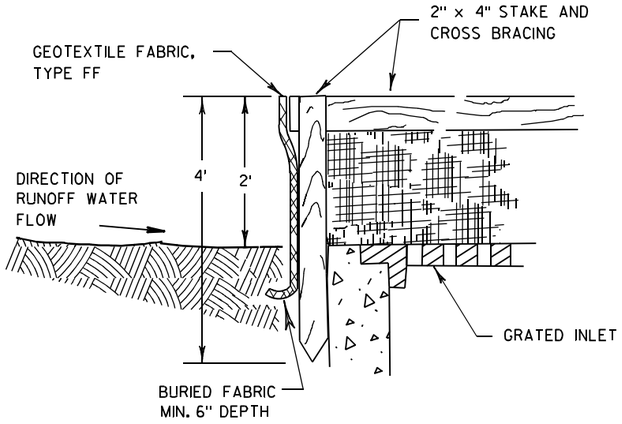
SILT FENCE TIE BACK

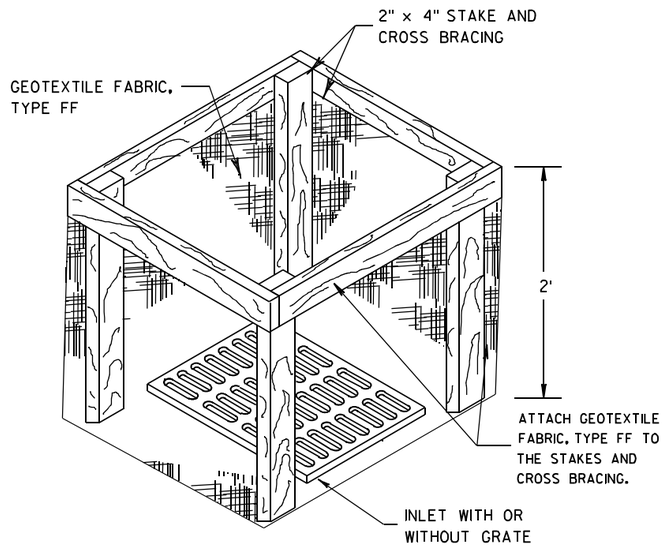
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED	
4-29-05 DATE	/S/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	

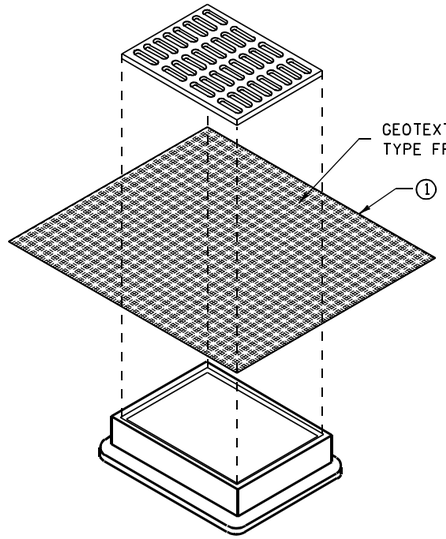
6

S.D.D. 8 E 9-6



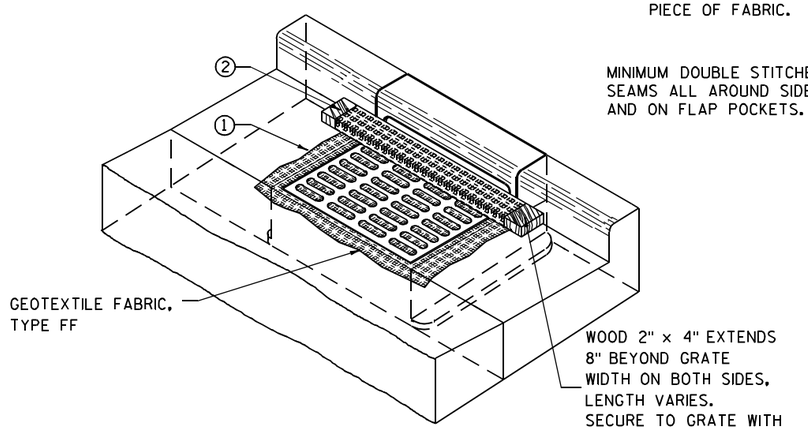


INLET PROTECTION, TYPE A

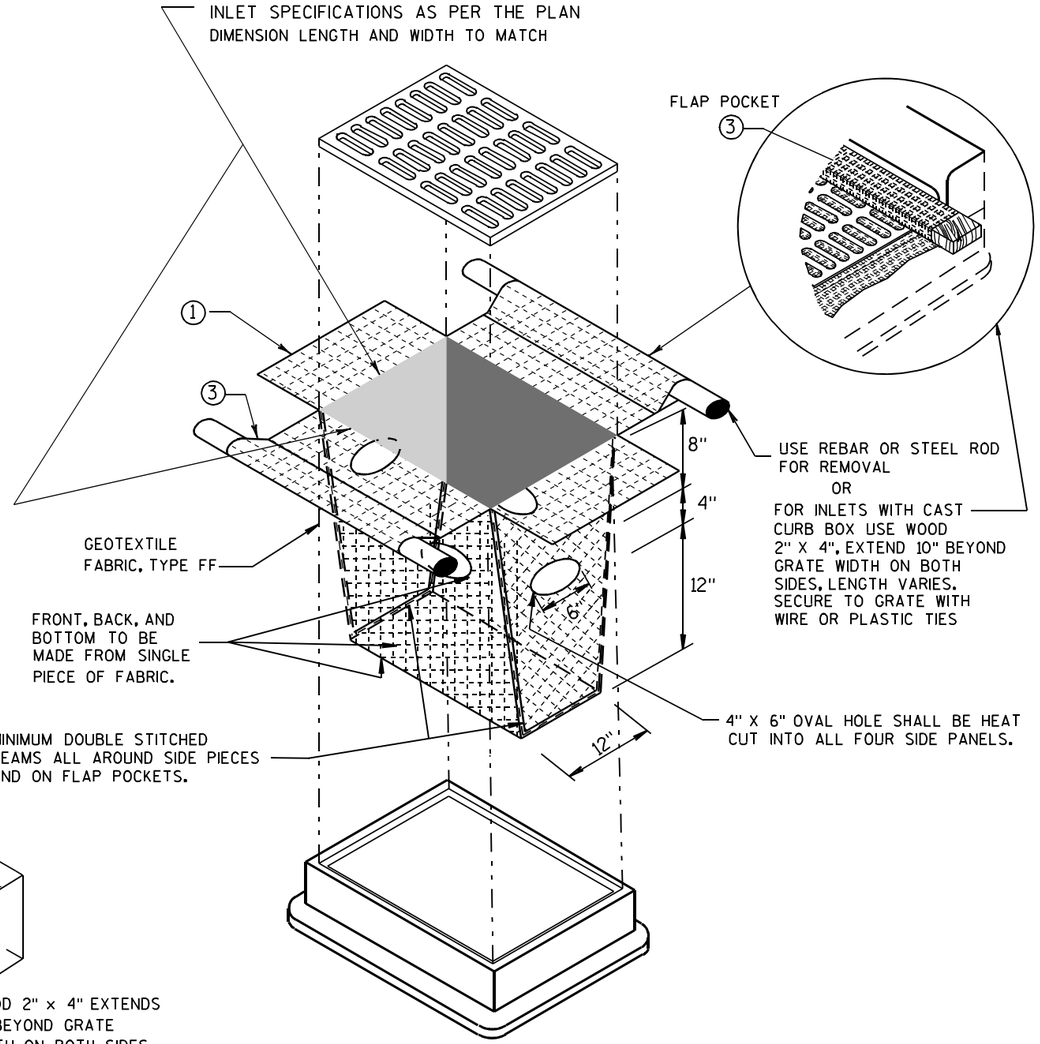


INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH
OR WITHOUT A CURB BOX AS PER NOTE ②)

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.

③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION
TYPE A, B, C, AND D

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02
DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

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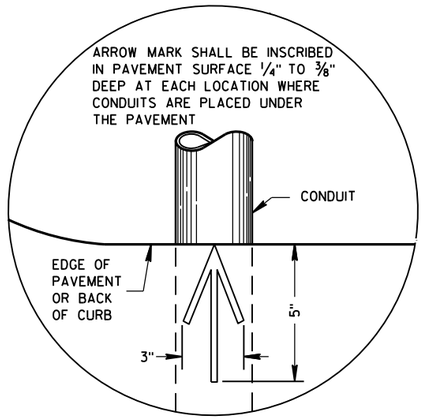
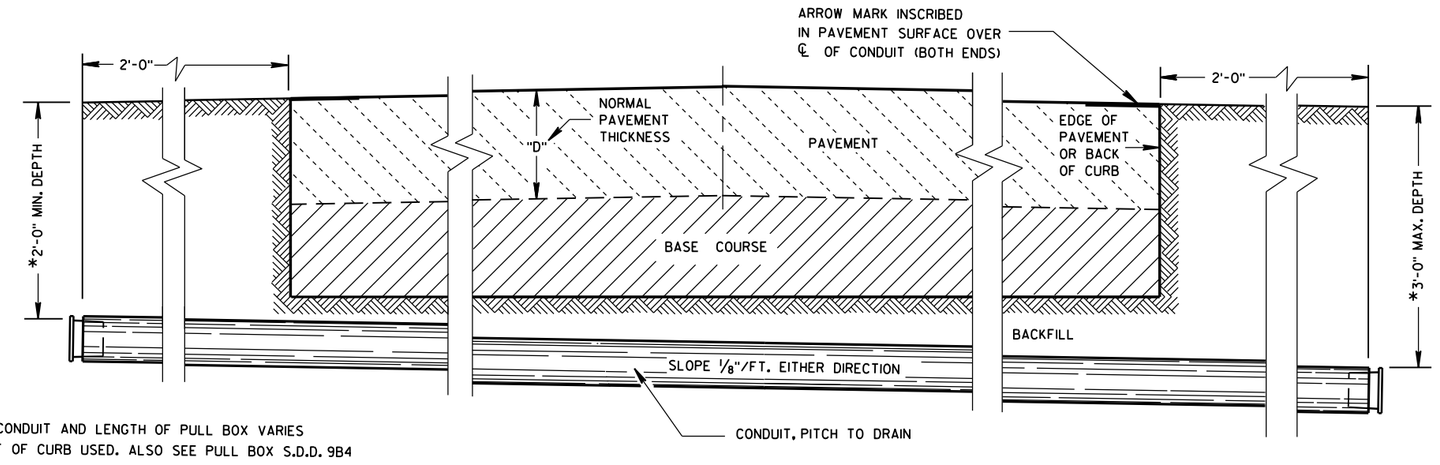
S.D.D. 8 E 10-2



PLAN VIEW

ARROW MARK



*DEPTH OF CONDUIT AND LENGTH OF PULL BOX VARIES WITH HEIGHT OF CURB USED. ALSO SEE PULL BOX S.D.D. 9B4

SIDE ELEVATION

DETAIL FOR CONDUIT UNDER PAVED HIGHWAYS

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

METALLIC (STANDARD SPECIFICATION 652.2.2) OR NONMETALLIC (STANDARD SPECIFICATION 652.2.3) CONDUIT SHALL BE FURNISHED AND PLACED AS SHOWN.

DEPTH OF CONDUIT INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES MINIMUM AND 36 INCHES MAXIMUM.

DEPTH OF CONDUIT INSTALLED THAT IS NOT BELOW THE TRAVELED WAY SHALL BE 18 INCHES MINIMUM AND 36 INCHES MAXIMUM.

ANY EXCEPTION TO THE MAXIMUM DEPTH SHALL BE ONLY WITH THE WRITTEN APPROVAL OF THE ENGINEER.

THE TRENCH SHALL NOT BE BACKFILLED PRIOR TO INSPECTION OF THE CONDUIT.

ALL METALLIC CONDUIT RACEWAY ENDS SHALL BE REAMED AND THREADED.

ALL METALLIC CONDUIT IN WHICH WIRE OR CABLE IS TO BE INSTALLED SHALL BE BUSHED WITH APPROVED THREADED BUSHINGS BEFORE INSTALLATION OF THE WIRE OR CABLE.

ALL METALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT TO BE INSTALLED SHALL BE CAPPED WITH THREADED PROTECTIVE CAPS, AS APPROVED BY THE ENGINEER.

ALL NONMETALLIC CONDUIT SHALL BE CAPPED OR PLUGGED IMMEDIATELY AFTER INSTALLATION AND SHALL REMAIN CAPPED OR PLUGGED UNTIL WIRE/CABLES ARE INSTALLED.

NONMETALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT BEING INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BENDING OF PVC ELECTRICAL CONDUIT SHALL BE ACCOMPLISHED BY USING A BLANKET OR EMERSION TYPE TANK DESIGNED FOR THE PURPOSE OF BENDING PVC ELECTRICAL CONDUIT.

ALL CUT ENDS SHALL BE TRIMMED INSIDE AND OUTSIDE TO REMOVE ALL ROUGH EDGES ON NONMETALLIC CONDUIT. (SEE NEC 347.5)

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY U.L. LISTED ADAPTER FITTINGS SHALL BE USED.

PRIOR TO CONDUIT ACCEPTANCE, CONDUIT CAPS OR PLUGS SHALL BE REMOVED, AND THE CAPS, PLUGS AND CONDUIT ENDS SHALL BE THOROUGHLY CLEANED AND THEN THE CAPS OR PLUGS REINSTALLED TO ENSURE THAT THE CAPS OR PLUGS CAN BE EASILY REMOVED IN THE FUTURE.

ALL CONDUIT BEING FURNISHED AND INSTALLED SHALL HAVE THE U.L. LABEL FIRMLY ATTACHED.

CONDUIT RUNS SHALL BE THE SAME SIZE OF CONDUIT FROM ONE END TO THE OTHER (FROM PULL BOX TO PULL BOX-OR-JUNCTION BOX TO JUNCTION BOX-OR-BASE TO BASE, ETC.).

TRACER WIRE SHALL BE INSTALLED AS STATED IN THE STANDARD SPECIFICATION, ITEM 652.3.1.1.

ALL CONDUIT RUNS SHALL BE STRAIGHT (WITHOUT BENDS) FROM PULL BOX TO PULL BOX, PULL BOX TO BASE AND BASE TO BASE AS SHOWN ON THE PLANS.

CONDUIT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March, 2017 — DATE
/S/ Ahmet Demirbilek — STATE ELECTRICAL
FHWA

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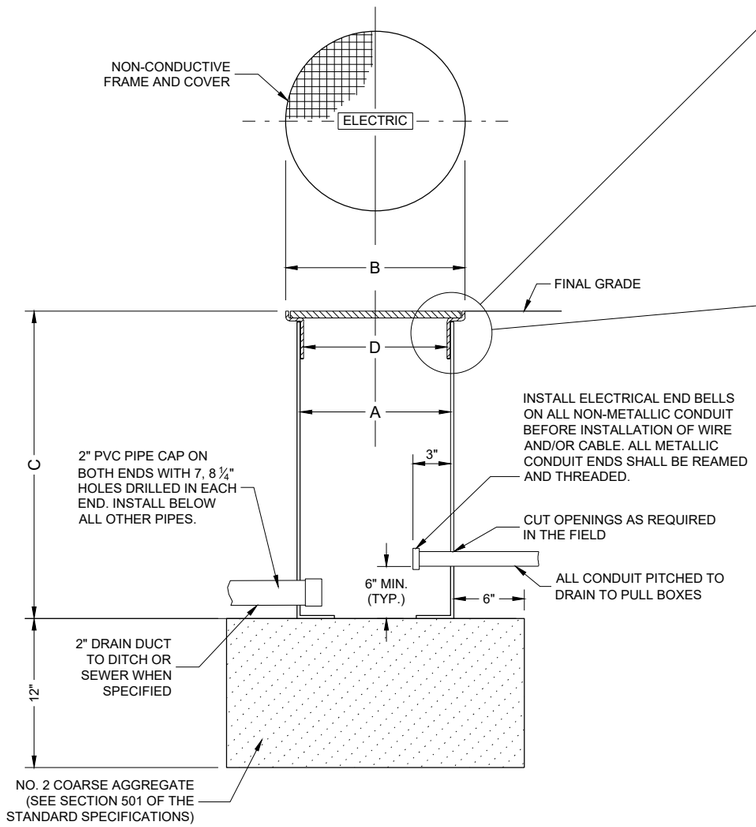
S.D.D. 9 B 2-10

TABLE OF NOMINAL DIMENSIONS AND WEIGHTS

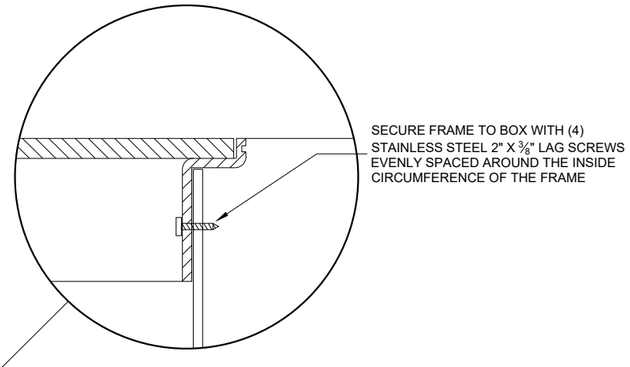
DIMENSION IN INCHES		NON- CONDUCTIVE PULL BOX	
BOX DIAMETER ** (INSIDE)	A	24	24
BOX OVERALL OUTSIDE DIAMETER	B	27	27
BOX LENGTH	C	36	42
FRAME OPENING	D	22 ½	22 ½
WEIGHT IN POUNDS *			
COVER		50	50
BOX ONLY		75	85

* THE ACTUAL WEIGHT OF THE COVER OR BOX ONLY MAY VARY NOT TO EXCEED 100 LBS INDIVIDUALLY.

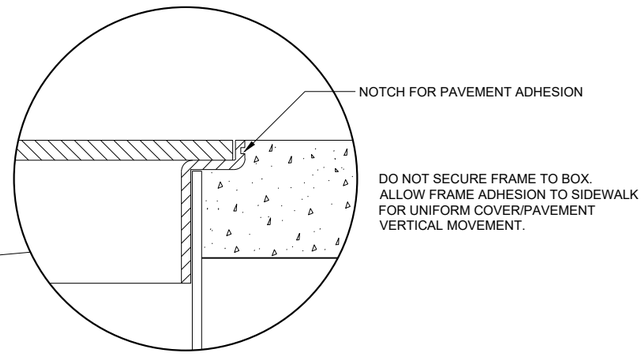
** DIAMETER VARIES FROM TOP TO BOTTOM WITH THE DIAMETER LARGER AT THE BOTTOM TO PREVENT FROST HEAVE.



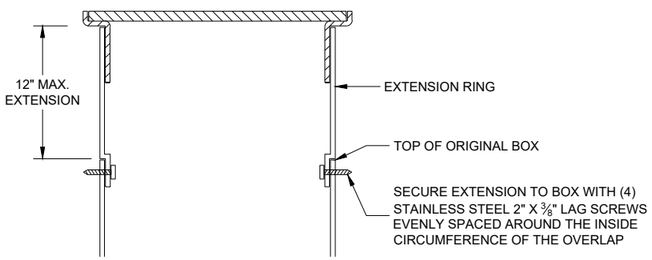
NON-CONDUCTIVE PULL BOX



INSTALLED IN SOD OR CRUSHED AGGREGATE



INSTALLED IN SIDEWALK



BOX EXTENSION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

ALL BOXES, FRAMES AND COVERS SHALL BE SUITABLE FOR TIER 15 LOADING AS SPECIFIED IN ANSI/SCTE 77.

PROVIDE AN OPENING FOR TOOL ASSISTED COVER REMOVAL NOT LARGE ENOUGH TO PERMIT PASSAGE OF A SPHERE MORE THAN ½" DIAMETER

ENSURE COVER SURFACE IS SKID RESISTANT WITH A COEFFICIENT OF FRICTION OF AT LEAST 0.5 AND VERTICAL SURFACE DICONTINUITIES LESS THAN ¼".

COVER SHALL BE MAGNETICALLY LOCATABLE.

BOXES AND EXTENSIONS ARE TRIMMABLE FOR CUSTOM LENGTHS. TRIMMED PIECES SHALL MAINTAIN A UNIFORM LENGTH.

ENTRANCE HOLES INTO PULL BOXES SHALL BE CUT WITH A CIRCULAR HOLE SAW OR HYDRAULIC CONDUIT PUNCH. HOLE SIZE SHALL BE THE OUTSIDE DIAMETER OF THE CONDUIT THAT IS TO FIT IN THE OPENING PLUS NO MORE THAN ¼".

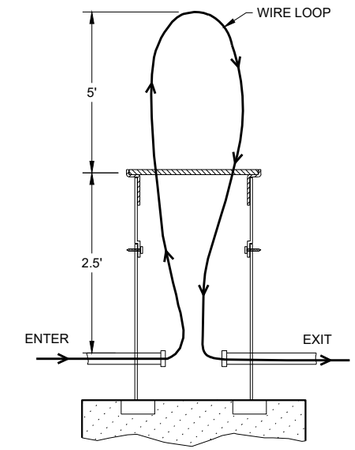
THE CONTRACTOR SHALL NOT INSTALL WIRE IN ANY PULL BOX UNTIL ITS INSTALLATION HAS BEEN INSPECTED AND ACCEPTED BY THE ENGINEER.

ALL METALLIC CONDUIT IN WHICH WIRE AND/OR CABLE IS TO BE INSTALLED, SHALL BE BUSHED BEFORE INSTALLATION OF THE WIRE AND/OR CABLE.

ENTIRE BOX MUST BE CONSTRUCTED OF NON-CONDUCTIVE MATERIALS WITH THE EXCEPTION OF STAINLESS STEEL FASTENERS AND MAGNETIC LOCATABLE DEVICE.

WHEN A PULL BOX IS INSTALLED IN CRUSHED AGGREGATE SHOULDERS, PLACE IT 2-3 INCHES BELOW GRADE AND COVER IT WITH 2-3 INCHES OF CRUSHED AGGREGATE.

LABEL ON COVER SHALL READ "ELECTRIC" FOR SIGNAL AND LIGHTING SYSTEMS, "WISDOT ITS" FOR COMMUNICATIONS AND ITS EQUIPMENT SYSTEMS.



MEASUREMENT DETAIL FOR WIRE/CABLE IN THE PULL BOX

PULL BOXES NON-CONDUCTIVE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022
DATE
/S/ Ahmet Demirbilek
STATE ELECTRICAL ENGI

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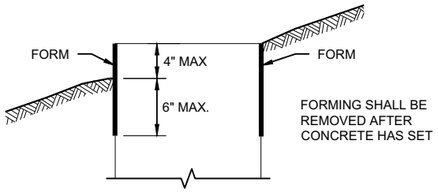
FORM DEPTH SHALL BE NO MORE THAN 6" BELOW GRADE ON THE LOWER SIDE OF BASE



FORMING DETAIL

QUANTITY REQUIREMENTS	CONCRETE BASE TYPE		
	1	2	5 & 6
APPROX. CUBIC YARDS OF CONCRETE	0.40	0.57	0.40
LBS. OF HOOP BAR STEEL	NONE	23	16
LBS. OF VERTICAL BAR STEEL	NONE	60	18

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWINGSHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

BASES SHALL BE EXCAVATED BY USE OF A CIRCULAR AUGER.

TOP SURFACES OF CONCRETE BASES SHALL BE TROWEL FINISHED SMOOTH AND LEVEL.

CONDUIT SIZES AND LOCATIONS SHALL BE SHOWN ON THE PLANS

THE FINAL OR TERMINATING CONCRETE BASE IN A CONDUIT RUN SHALL HAVE A 6" EXIT STUB INSTALLED FOR FUTURE CABLING USE. THE EXIT STUB SHALL BE SIZED AS USED THROUGHOUT THE CONDUIT RUN AS SHOWN A THE ENTRANCE OF THE BASE.

ENDS OF CONDUIT INSTALLED BELOW GRADE FRO FUTURE USE SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON-METALLIC.

MINIMUM BENDING RADIUS OF CONDUIT IS EQUAL TO 6X THE DIAMETER.

CONDUIT HEIGHT ABOVE CONCRETE BASES SHALL BE 1 INCH. ALL METALLIC CONDUIT ENDS SHALL BE REAMED AND THREADED.

ALL CONDUIT ENDS AT THE TOP OF CONCRETE BASES SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON-METALLIC IMMEDIATELY AFTER PLACEMENT AND BEFORE CONCRETE IS POURED. CONDUITS IN WHICH WIRE OR CABLE IS NOT INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BELL ENDS SHALL BE INSTALLED ON ALL PVC CONDUIT EXPOSED AT THE TOP OF CONCRETE BASES BEFORE INSTALLATION.

WHEN REQUIRED TO CONNECT NON-METALLIC CONDUIT TO METALLIC CONDUIT, ONLY ADAPTER FITTINGS, U.L. LISTED FOR ELECTRICAL USE, SHALL BE USED.

IF A BASE REQUIRES A DEEP FORM BECAUSE OF LOOSE DIRT OR FILL, THE FORM SHALL BE REMOVED BEFORE BACKFILLING AROUND THE BASE. BACKFILL SHALL BE TAMPED TIGHT AGAINST THE BARE CONCRETE BASE IN LAYERS OF 1 FOOT OR LESS.

A NO. 4 AWG STRANDED COPPER EQUIPMENT GROUNDING CONDUCTOR SHALL BE EXOTHERMICALLY WELDED TO THE EQUIPMENT GROUNDING ELECTRODE (GROUND ROD) FOR TYPE 2, TYPE 5 AND TYPE 6 BASES.

THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE FURNISHED AND INSTALLED TO ENTER ALL BASE TYPES THROUGH A 1 INCH CONDUIT INSTALLED FOR GROUNDING PURPOSES, LEAVING A 4 FOOT COIL OF WIRE ABOVE THE CONCRETE BASE. THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE NEATLY COILED AND THE COILS TIED TOGETHER.

ANCHOR RODS SHALL BE THREADED 12" IN LENGTH ON EACH END OF THE ROD. ANCHOR RODS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 654.2.1 OF THE STANDARD SPECIFICATIONS.

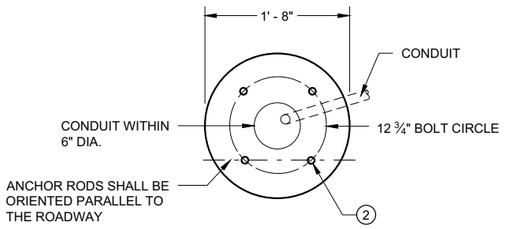
WASHERS AND LOCK WASHERS ARE REQUIRED ON ALL ANCHOR RODS.

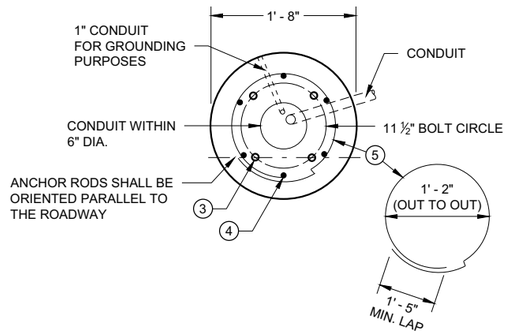
WHEN ANCHOR RODS USING THE ALTERNATE "L" BEND ARE FURNISHED, THE 4 INCH"L" BEND SHALL BE IN ADDITION TO THE SPECIFIED ANCHOR ROD BAR LENGTH. THE "L" BEND SHALL NOT BE THREADED.

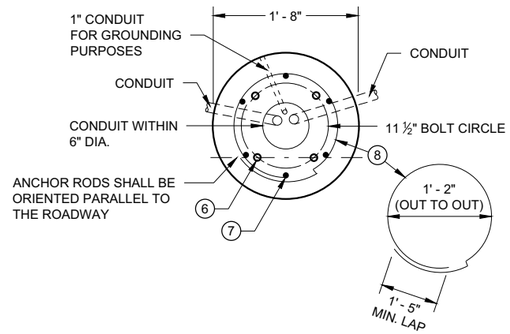
ANCHOR RODS SHALL BE INSTALLED WITH MISALIGNMENTS OF LESS THAN 1:40 FROM VERTICAL.

WELDING OF THE ANCHOR RODS TO THE CAGE IS UNACCEPTABLE. TIE WIRES SHALL BE USED.

BAR STEEL REINFORCEMENT SHALL BE COATED WITH POWDERED EPOXY RESIN IN ACCORDANCE WITH SECTION 505 OF THE STANDARD SPECIFICATIONS (LATEST EDITION).







1 THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE AND INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES. THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE THAT IS NOT INSTALLED BELOW THE TRAVELED WAY SHALL BE 18 INCHES. THE MAXIMUM DEPTH OF ALL CONDUIT SHALL BE 36 INCHES EXCEPT WITH WRITTEN APPROVAL OF THE ENGINEER.

2 (4) 1" DIA. X 3' - 6" ANCHOR RODS.

3 (4) 1" DIA. X 5' - 0" ANCHOR RODS.

4 (6) NO. 6 X 6' - 8" BAR STEEL REINFORCEMENT.

5 (7) NO. 4 X 5' - 1" BAR STEEL REINFORCEMENT @ 1' - 0" C - C.

6 (4) 1" DIA. X 3' - 6" ANCHOR RODS.

7 (6) NO. 4 X 4' - 8" BAR STEEL REINFORCEMENT.

8 (5) NO. 4 X 5' - 1" BAR STELL REINFORCEMENT @ 1' - 0" C -C.

9 EXOTHERMIC CONNECTION TO EUIPMENT GROUNDING CONDUCTOR

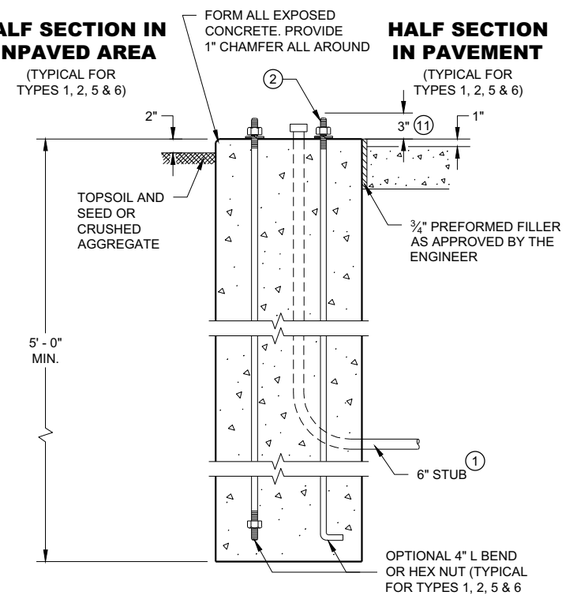
10 ⅝" DIA. X 8' -0" COPPERCLAD EQUIPMENT GROUNDING ELECTRODE REQUIRED

11 ANY ANCHOR ROD PROJECTION SHORTER THAN 2 ¾" OR LONGER THAN 3 ¼" SHALL REQUIRE THE BASE TO BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.

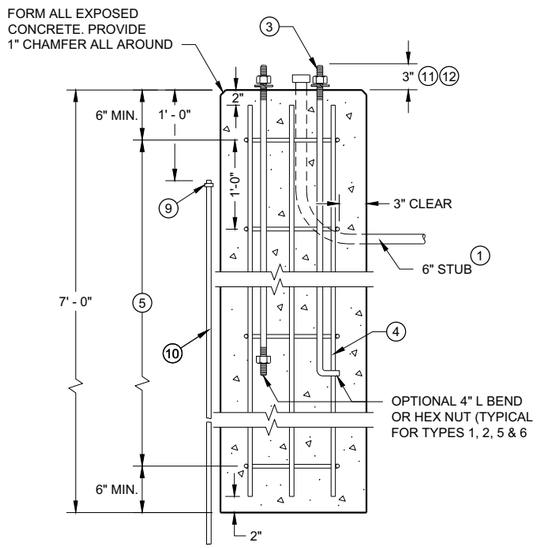
12 FOR NON - BREAKAWAY INSTALLATIONS, 4 ½" ± ANCHOR ROD PROJECTION WITH THE USE OF LEVELING NUTS. RODENT SCREEN REQUIRED.

HALF SECTION IN UNPAVED AREA
(TYPICAL FOR TYPES 1, 2, 5 & 6)

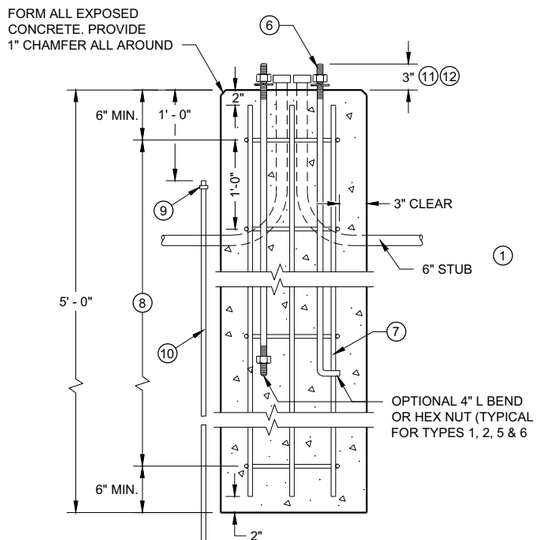
HALF SECTION IN PAVEMENT
(TYPICAL FOR TYPES 1, 2, 5 & 6)



TYPE 1



TYPE 2



TYPE 5 & 6

CONCRETE BASES

CONCRETE BASES
TYPES 1, 2, 5, & 6

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019
DATE
/S/ Ahmet Demirbilek
STATE ELECTRICAL ENGI 52

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SDD 09C02 - 09

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

FOUR (4) BOLTS SHALL BE FURNISHED WITH EACH TRANSFORMER BASE. BOLTS SHALL BE 1" DIAMETER, 4" IN LENGTH, WITH WASHERS, LOCK WASHERS AND NUTS. BOLTS, NUTS AND WASHERS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 531.2.2 OF THE STANDARD SPECIFICATIONS.

LEVELING SHIMS, IF NEEDED, SHALL BE DESIGNED FOR THE PURPOSE AND USED UNDER CAST BASES WHEN PLUMBING POLES OR STANDARDS DURING INSTALLATION. THE USE OF WASHERS IN LIEU OF PROPER LEVELING SHIMS IS NOT ACCEPTABLE.

SHIM LENGTH SHALL BE LONG ENOUGH TO COMPLETELY COVER THE AREA UNDER THE LENGTH AND WIDTH OF THE BASE MOUNTING FLANGE.

DOUBLE NUTTING IS NOT ACCEPTABLE FOR LEVELING OR MOUNTING PURPOSES.

A NEMA APPROVED, U.L. LISTED, COPPER WITH BRASS OR STAINLESS STEEL SET SCREW, DIRECT BURY RATED, MECHANICAL CONNECTOR (LUG), SIZED TO ACCEPT AWG. #10 TO #4 COPPER STRANDED WIRE SHALL BE FURNISHED AND INSTALLED IN THE PEDESTAL AND TRANSFORMER BASES.

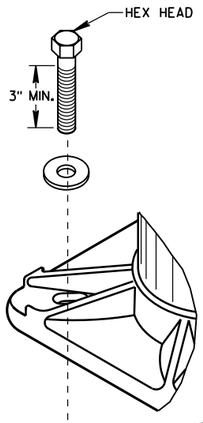
THE MECHANICAL CONNECTOR SHALL BE INSTALLED USING A 1/4" - 20 (TPI) STAINLESS STEEL HEX HEAD BOLT OF SUFFICIENT LENGTH TO FIRMLY ATTACH THE LUG TO THE BASE.

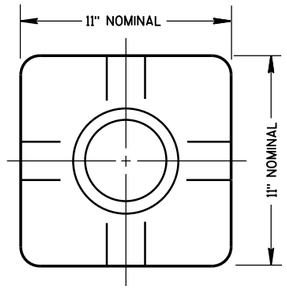
SHOULD THE MANNER OF ATTACHMENT OF THE LUG REQUIRE WASHERS, HEX NUTS, LOCK WASHER - THEY SHALL BE STAINLESS STEEL AS IS THE BOLT. THE MANNER OF ATTACHMENT SHALL NOT BLOCK ACCESSIBILITY TO WIRE PLACEMENT IN THE CONNECTOR.

PEDESTAL BASE COLLAR THREADING SHALL BE TAPERED AND IN ACCORDANCE WITH NATIONAL PIPE THREADING DIMENSIONS.

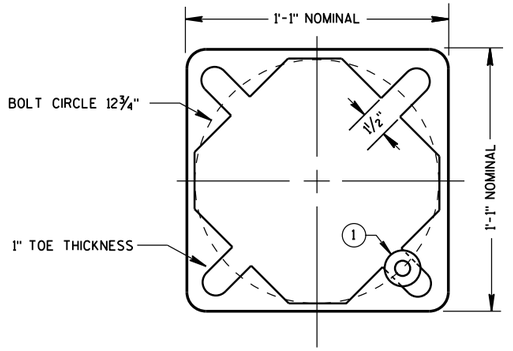
BASE COLLAR THREADING SHALL EXTEND INTO THE BASE COLLAR WITH SUFFICIENT DEPTH TO ACCEPT THE INSTALLATION OF TRAFFIC SIGNAL STANDARDS TO A DEPTH OF 1 1/2", THEN TIGHTENING TO A POINT OF BEING IMMOVABLE.

THE ACCESS DOOR SHALL BE OF THE SAME MATERIAL AS THE BASE.

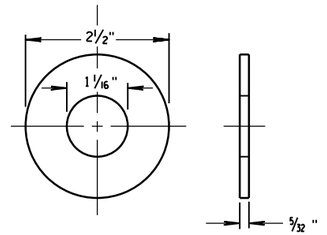




TOP VIEW
(PEDESTAL BASE)

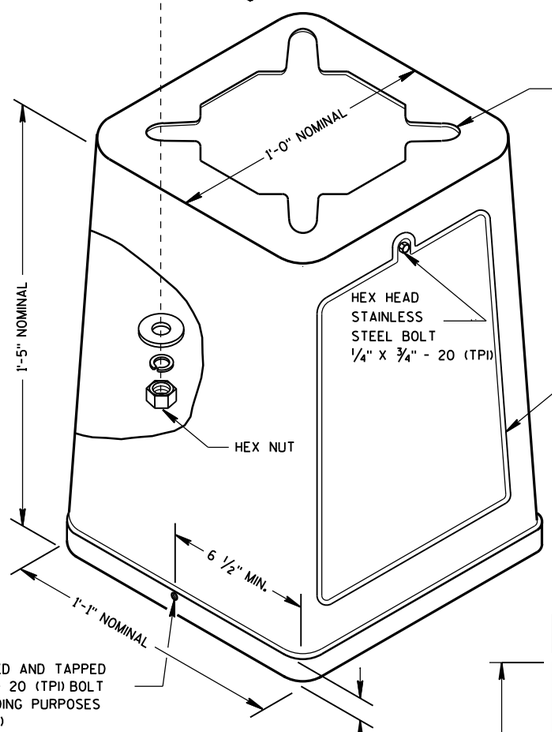


BOTTOM VIEW
(PEDESTAL BASE)

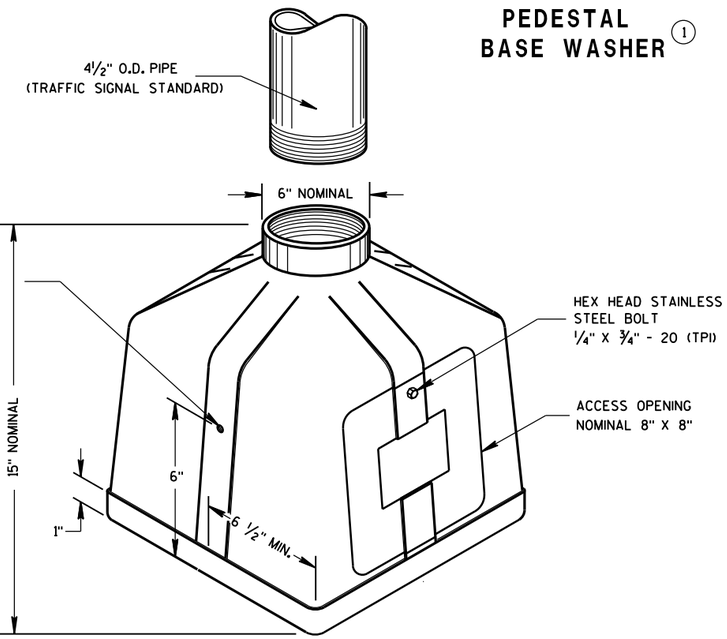


ZINC COATED STEEL WASHER
TO BE PROVIDED BY THE CONTRACTOR

PEDESTAL BASE WASHER (1)

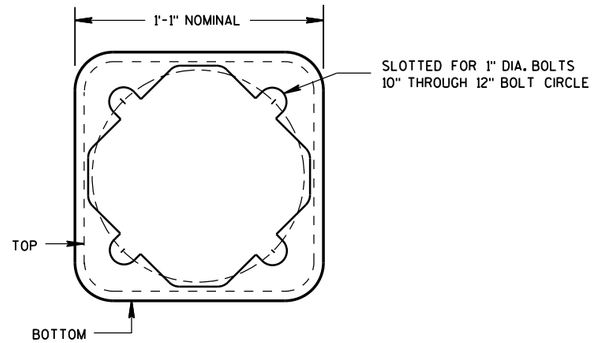


ISOMETRIC VIEW

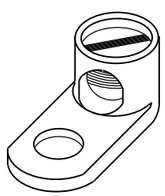


ISOMETRIC VIEW

PEDESTAL BASE



BOTTOM VIEW
(TRANSFORMER BASE)

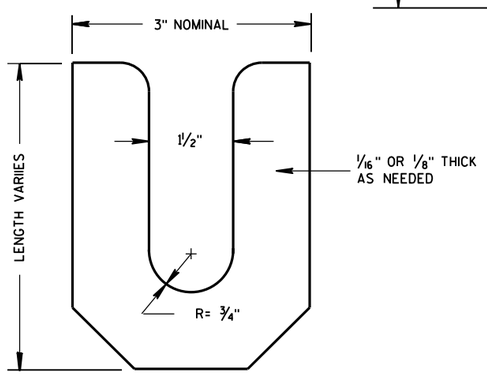


TYPICAL MECHANICAL CONNECTOR LUG

TO BE FURNISHED WITH EACH BASE

TRANSFORMER BASE

INTENDED FOR USE WITH TYPE 2, 3, 4, 5 & 6 POLES



LEVELING SHIM

TRANSFORMER/PEDESTAL BASES

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DEPARTMENT OF TRANSPORTATION

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Sept. 2014 — DATE
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S.D.D. 9 C 3-4

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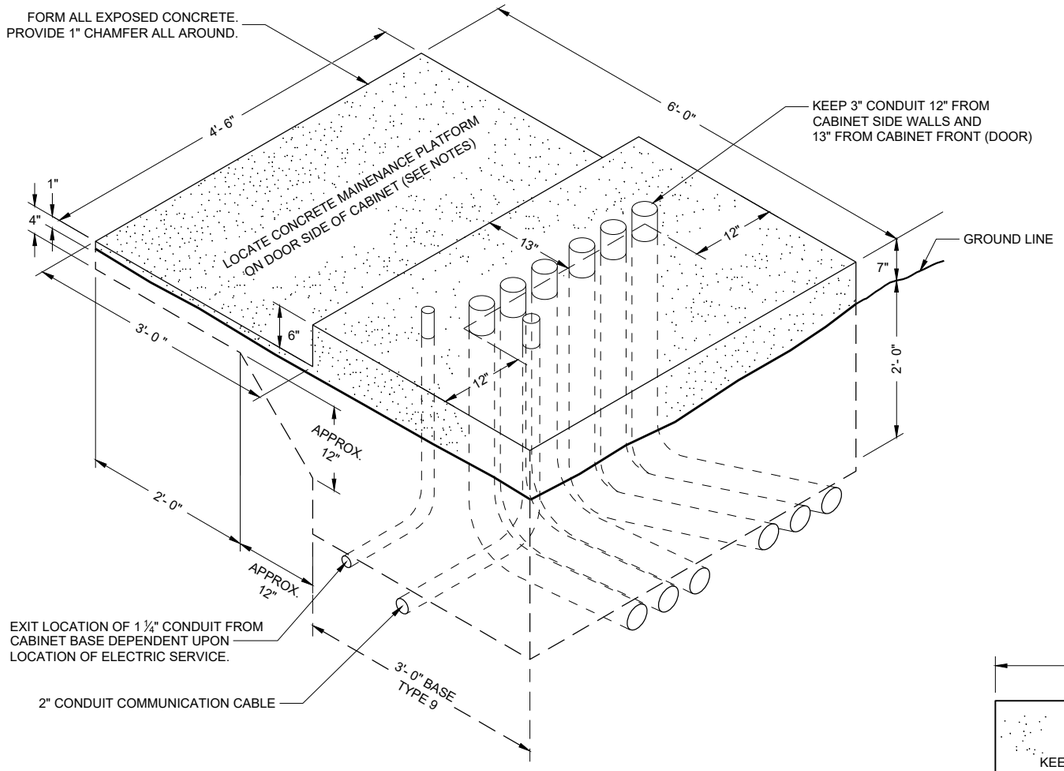
S.D.D. 9 C 3-4

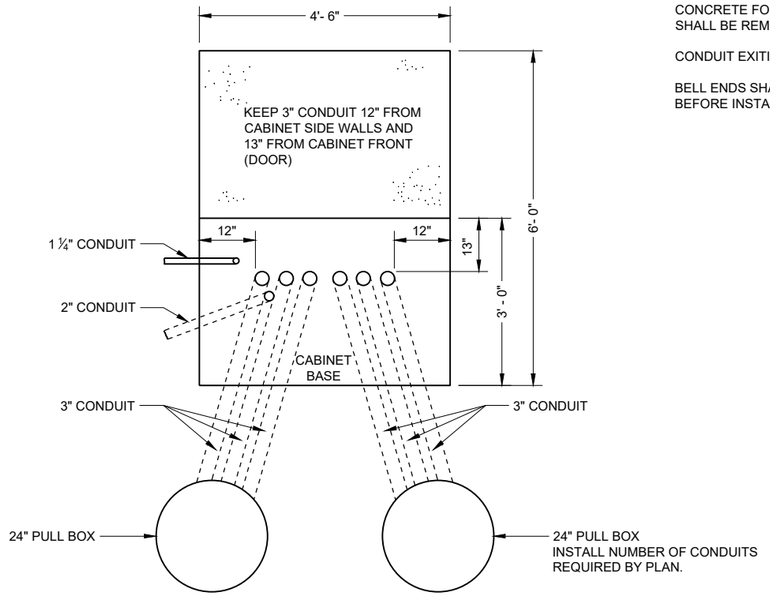
SDD 09C06 Concrete Control Cabinet Base, Type 9, Special



ISOMETRIC VIEW

TYPE 9 SPECIAL

(C.Y. CONCRETE = APPROX. 1.56)



PLAN VIEW

CONCRETE CONTROL CABINET BASE, TYPE 9 SPECIAL

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

INSTALL FOUR INCH MINIMUM DIAMETER X 4 INCH MINIMUM LENGTH STAINLESS STEEL APPROVED CONCRETE MASONRY ANCHORS WITH A PULLOUT STRENGTH OF 9,000 LBS. TO ANCHOR THE CABINET TO TYPE 6, 7, 8, AND 9 BASES. THE ANCHOR STUDS SHALL BE LOCATED AS DIRECTED BY THE ENGINEER TO PROPERLY ANCHOR THE CONTROL CABINET TO THE BASE.

WHEN REQUIRED TO CONNECT NON - METALLIC CONDUIT TO METALLIC CONDUIT, ONLY ADAPTER FITTINGS, U. L. LISTED FOR ELECTRICAL USE, SHALL BE USED.

CONDUIT HEIGHT ABOVE THE CONCRETE BASE SHALL BE 1 INCH.

DEPTH OF CONDUIT INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES MINIMUM AND 36 INCHES MAXIMUM.

DEPTH OF CONDUIT INSTALLED THAT IS NOT BELOW THE TRAVELED WAY SHALL BE 18 INCHES MINIMUM AND 36 INCHES MAXIMUM.

ANY EXCEPTION TO THE MAXIMUM DEPTH SHALL BE ONLY WITH THE WRITTEN APPROVAL OF THE ENGINEER.

CONTROL CABINET BASE TOP SURFACE SHALL BE TROWEL FINISHED SMOOTH AND LEVEL.

MAINTENANCE PLATFORM SHALL BE FLOAT OR BROOM FINISHED AND LEVEL.

MAINTENANCE PLATFORMS ARE NOT REQUIRED WHEN THE SURROUNDING AREA IS PAVED.

MINIMUM BENDING RADIUS OF CONDUIT EQUALS 6 TIMES THE DIAMETER.

ALL METALLIC CONDUIT ENDS SHALL BE REAMED AND THREADED.

CAP ALL BELOW GRADE METALLIC CONDUIT ENDS IN WHICH WIRE OR CABLE IS NOT BEING INSTALLED.

PLUG ALL BELOW GRADE NON - METALLIC CONDUIT ENDS IN WHICH WIRE OR CABLE IS NOT BEING INSTALLED.

ALL CONDUIT ENDS AT THE TOP OF CONCRETE BASES SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON - METALLIC IMMEDIATELY AFTER PLACEMENT AND BEFORE CONCRETE IS POURED. CONDUITS IN WHICH WIRE OR CABLE IS NOT BEING INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

CONCRETE FORM DEPTH BELOW FINISHED GRADE SHALL BE 6 INCHES MAXIMUM. CONCRETE FORMS SHALL BE REMOVED AFTER CONCRETE HAS SET.

CONDUIT EXITING THE CONCRETE BASE (SIX 3") SHALL TERMINATE IN PULL BOXES AS SHOWN ON THE PLANS.

BELL ENDS SHALL BE INSTALLED ON ALL PVC CONDUIT EXPOSED AT THE TOP OF THE CONCRETE BASE BEFORE INSTALLATION OF CABLE OR WIRE.

CONCRETE CONTROL CABINET BASE TYPE 9, SPECIAL

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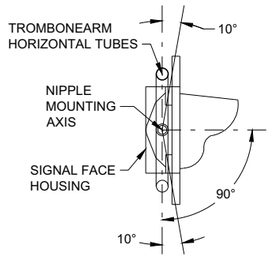
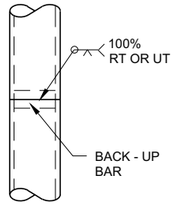


SECTION A-A

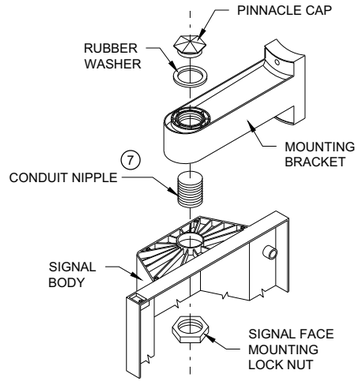
(10 DEGREES TILT REQUIREMENT OF FACE(S) IN THE TROMBONE MOUNTING)

FOR MANUFACTURERS USE ONLY

WELD TO BE 100% R.T. OR U.T. TESTED AS PER THE REQUIREMENTS OF AWS D 1.5-88. RECORDS OF COMPLIANCE OF SUCH TESTING SHALL BE FURNISHED TO THE OFFICE OF DESIGN / BRIDGE FOR VERIFICATION AND APPROVAL.



POLE SPLICE DETAIL



SIGNAL FACE MOUNTING DETAIL

(BANDED)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THEPERTINENT REQUIREMENTS OF THE CONTRACT.

POLES SHALL BE EITHER ALUMINUM OR GALVANIZED STEEL AS CALLED FOR IN THE CONTRACT.

SECTION 657, POLES, OF THE STANDARD SPECIFICATIONS SHALL APPLY TO THIS DRAWING.

A PULL WIRE / ROPE SHALL BE INSTALLED IN EACH TROMBONE ARM RACEWAY DURING THE MANUFACTURING PROCESS.

TYPE 2 ALUMINUM POLES SHALL BE CONSTRUCTED OF 6063 - T6 ALUMINUM ALLOY. SLEEVING INSIDE THE POLE IS NOT ACCEPTABLE.

WHEN TRANSFORMER BASES ARE USED, WIRE CONNECTIONS SHALL BE MADE IN THE TRANSFORMER BASE.

(1) 4" X 6" REINFORCED HANDHOLE AND COVER ASSEMBLY WITH TWO (2) ¼" X ¾" - 20 TPI , STAINLESS STEEL, HEX HEAD BOLTS.

(2) SIGNAL FACE MOUNTING BRACKETS. MOUNT WITH CAP SCREWS AND BANDING.

(3) GROMMETS. 1" CHASE NIPPLES OR 1" CLOSE CONDUIT NIPPLES WITH BUSHINGS SHALL BE PROVIDED FOR 1 ⅜" HOLE IN POLE SHAFT FOR WIRING.

(4) SECURELY MOUNT DULL BLACK POLYCARBONATE BACKPLATES, PROJECTING 5" BEYOND ALL SIDES OF THE SIGNAL FACE HOUSING,PER MANUFACTURER'S RECOMMENDATIONS.

(5) POLE MOUNTED SIGNAL FACES SHALL REQUIRE ONE OR MORE MOUNTING SPACERS UNDER THE TOP MOUNTING BRACKET(S) ASREQUIRED, TO PLUMB THE SIGNAL FACES.

(6) CAST ALUMINUM TRANSFORMER BASE, WHEN REQUIRED.

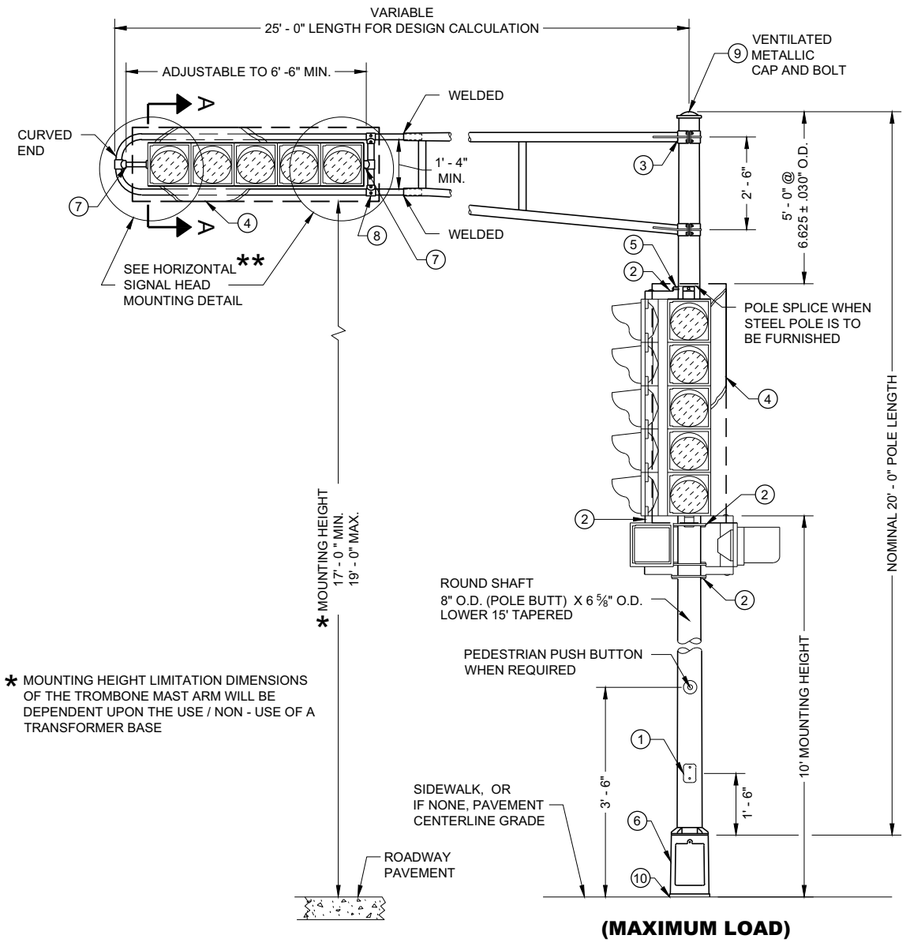
(7) USE 1 ½" ID NIPPLES ZINC-COATED RIGID METAL CONDUIT, LONG ENOUGH TO ACCOMMODATE FULL DEPTH THREADING INTO THE HEAD MOUNTING LOCK NUT IN ORDER TO TIGHTEN THE FACE, BUT THAT DO NOTINTERFERE WITH REFLECTOR CLOSURE. THREAD THE NIPPLE INTO THE MOUNTING BRACKET/ELBOW UNTIL TIGHT. USE APPROVED PINNACLE TYPE HARDWARE FROM A DEPARTMENT APPROVED MANUFACTURER TO CLOSE THE UNUSED 1 ½" OPENING IN SIGNAL FACES AND BRACKET ENDS.

(8) VERTICAL STRUT (ADJUSTABLE). ONE (1) SET SCREW (¼" X ¾" - 20 TPI STAINLESS STEEL, HEX HEAD) INTO EACH ARM MEMBER IF STRUTIS THE SLIDING TYPE.

(9) FURNISH AND INSTALL VENTILATED, CAST METALLIC (ALUMINUM ALLOY) CAPS. FASTEN CAPS WITH ONE (1) ¼" X ¾" - 20 TPI STAINLESS STEEL, HEX HEAD BOLT.

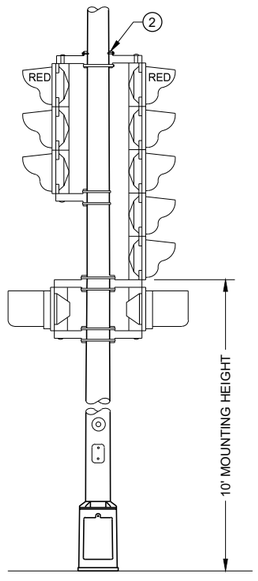
(10) SHIMMING, IF NEEDED, SHALL BE LOCATED BETWEEN THE CONCRETE FOUNDATION AND THE TRANSFORMER BASE.

(11) USE SERRATED LOCK WASHERS WITH NOTCHES BETWEEN END TEE AND SIGNAL HEAD.

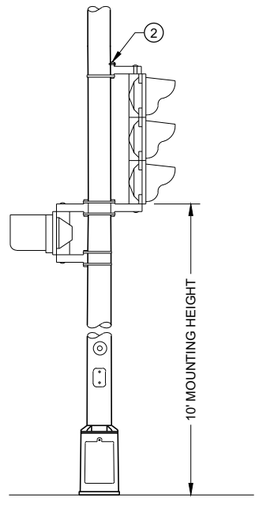


* MOUNTING HEIGHT LIMITATION DIMENSIONS OF THE TROMBONE MAST ARM WILL BE DEPENDENT UPON THE USE / NON - USE OF A TRANSFORMER BASE

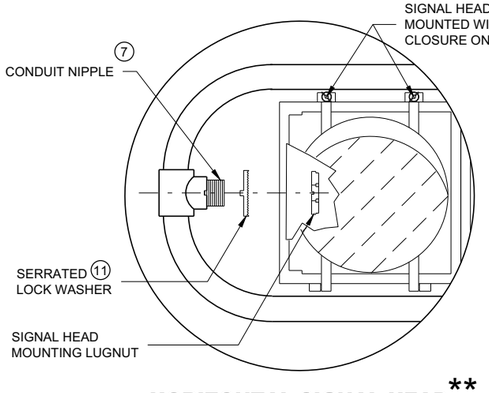
(MAXIMUM LOAD)



TYPICAL MOUNTING OF BACK TO BACK 3 AND 5 SECTION SIGNAL FACES



TYPICAL MOUNTING OF 3 SECTION SIGNAL FACE



HORIZONTAL SIGNAL HEAD MOUNTING DETAIL **

** SIGNAL HEAD ATTACHMENT ALSO APPLIES TO MOUNTING AT CROSS BAR

TYPE 2 POLE MOUNTING CONFIGURATION

POLE MOUNTINGS FOR TRAFFIC SIGNALS TYPE 2

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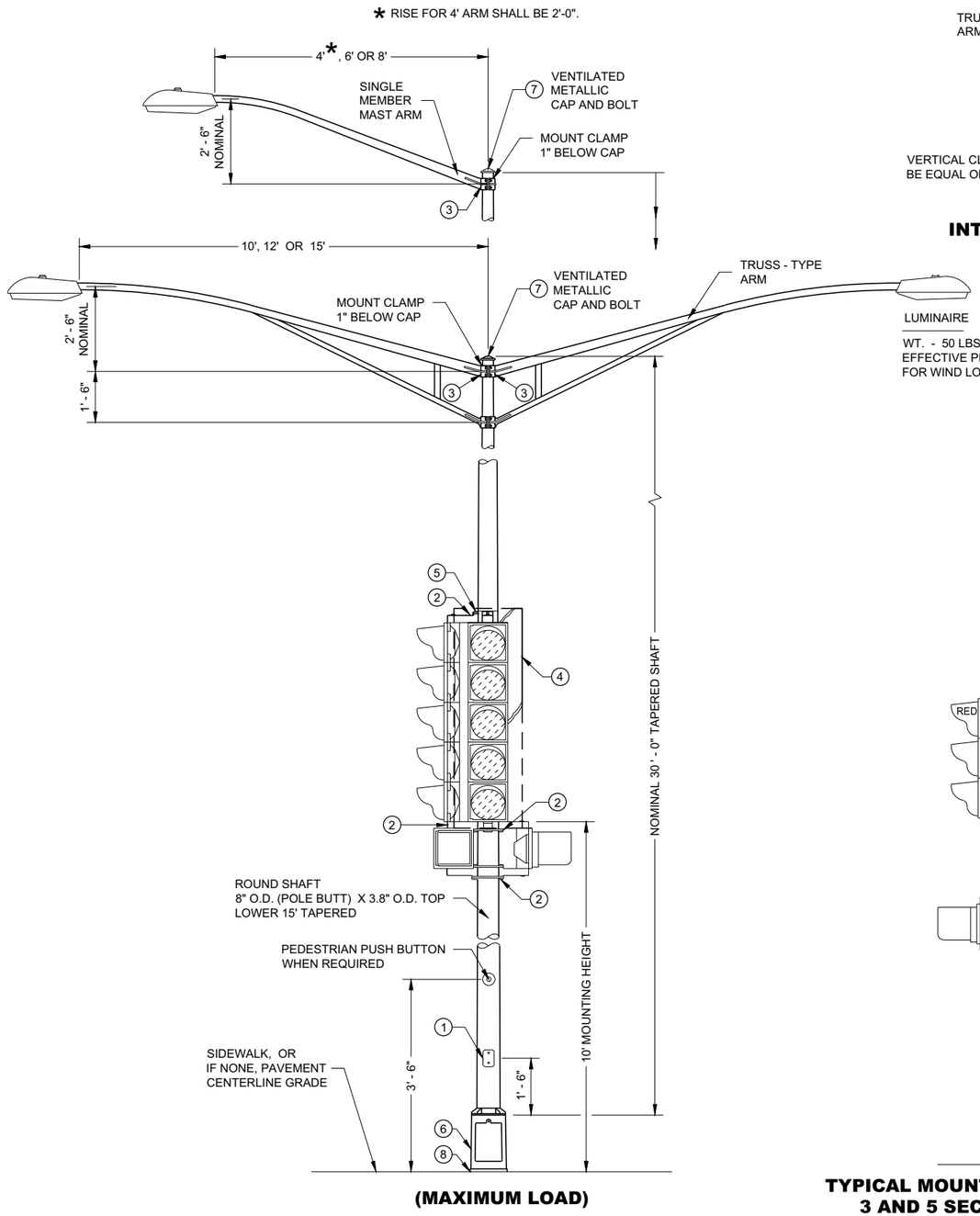
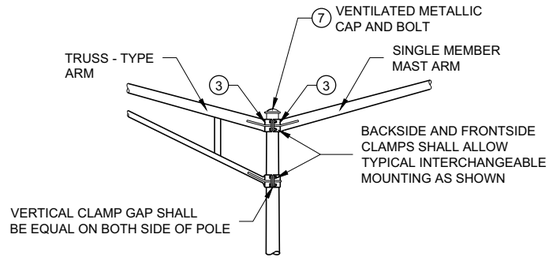


(MAXIMUM LOAD)



INTERCHANGEABLE MOUNTING DETAIL

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

ALL TYPE 4 POLE MOUNTINGS SHALL BE DESIGNED TO INCLUDE TWIN 15' ARMS WITH LUMINAIRES.

POLES SHALL BE GALVANIZED STEEL WITH A MINIMUM WALL THICKNESS OF U.S. STANDARD 11 GAGE (.1196").

SECTION 657, POLES, OF THE STANDARD SPECIFICATIONS SHALL APPLY TO THIS DRAWING.

A PULL WIRE / ROPE SHALL BE INSTALLED IN EACH TROMBONE ARM RACEWAY DURING THE MANUFACTURING PROCESS.

THE SLIPFITTER END OF THE LUMINAIRE MAST ARM SHALL BE A NOMINAL 2 ⅜ INCHES IN OUTSIDE DIAMETER. THE STRAIGHT PORTION OF THE SLIPFITTER END OF THE LUMINAIRE MAST ARM SHALL BE A NOMINAL 12 INCHES IN LENGTH.

WHEN TRANSFORMER BASES ARE USED, WIRE CONNECTIONS SHALL BE MADE IN THE TRANSFORMER BASE.

(1) 4" X 6" REINFORCED HANDHOLE AND COVER ASSEMBLY WITH TWO (2) ¼" X ¾" - 20 TPI , STAINLESS STEEL, HEX HEAD BOLTS.

(2) SIGNAL FACE MOUNTING BRACKETS. MOUNT WITH CAP SCREWS AND BANDING.

(3) GROMMETS. 1" CHASE NIPPLES OR 1" CLOSE CONDUIT NIPPLES WITH BUSHINGS SHALL BE PROVIDED FOR 1 ⅜" HOLE IN POLE SHAFT FOR WIRING.

(4) SECURELY MOUNT DULL BLACK POLYCARBONATE BACKPLATES, PROJECTING 5" BEYOND ALL SIDES OF THE SIGNAL FACE HOUSING, PER MANUFACTURER'S RECOMMENDATIONS.

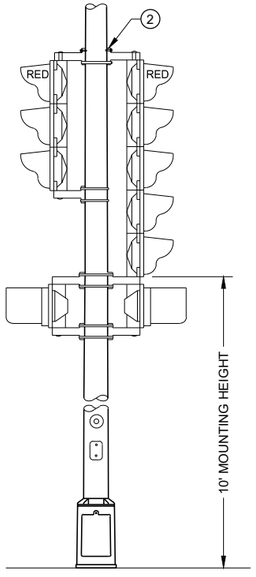
(5) POLE MOUNTED SIGNAL FACES SHALL REQUIRE ONE OR MORE MOUNTING SPACERS UNDER THE TOP MOUNTING BRACKET(S) AS REQUIRED, TO PLUMB THE SIGNAL FACE.

(6) CAST ALUMINUM TRANSFORMER BASE, WHEN REQUIRED.

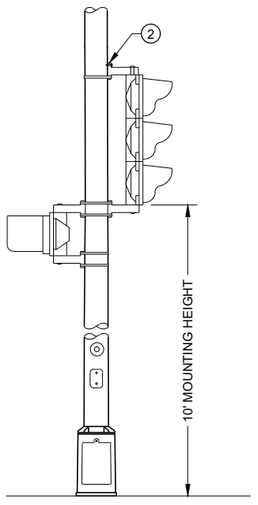
(7) FURNISH AND INSTALL VENTILATED, CAST METALLIC (ALUMINUM ALLOY) CAPS. FASTEN CAPS WITH ONE (1) ¼" X ¾" - 20 TPI STAINLESS STEEL, HEX HEAD BOLT.

(8) SHIMMING, IF NEEDED, SHALL BE LOCATED BETWEEN THE CONCRETE FOUNDATION AND POLE.

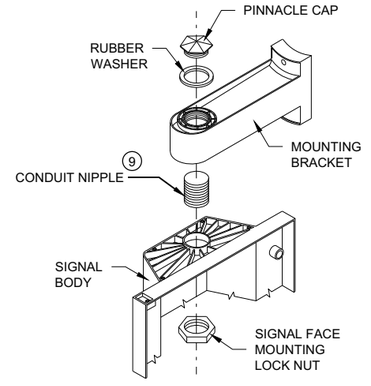
(9) USE 1 ½" ID NIPPLES ZINC-COATED RIGID METAL CONDUIT, LONG ENOUGH TO ACCOMMODATE FULL DEPTH THREADING INTO THE HEAD MOUNTING LOCK NUT IN ORDER TO TIGHTEN THE FACE, BUT THAT DO NOT INTERFERE WITH REFLECTOR CLOSURE. THREAD THE NIPPLE INTO THE MOUNTING BRACKET/ELBOW UNTIL TIGHT. USE APPROVED PINNACLE TYPE HARDWARE FROM A DEPARTMENT APPROVED MANUFACTURER TO CLOSE THE UNUSED 1 ½" OPENING IN SIGNAL FACES AND BRACKET ENDS.



TYPICAL MOUNTING OF BACK TO BACK 3 AND 5 SECTION SIGNAL FACES



TYPICAL MOUNTING OF 3 SECTION SIGNAL FACE



SIGNAL FACE MOUNTING DETAIL
(BANDED)

TYPE 4 POLE MOUNTING CONFIGURATION

POLE MOUNTINGS FOR TRAFFIC SIGNALS AND LIGHTING UNITS, TYPE 4

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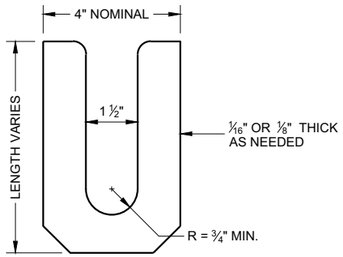
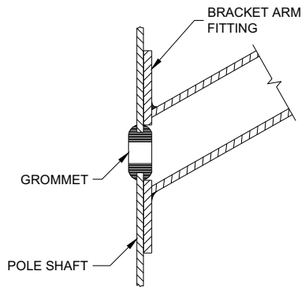
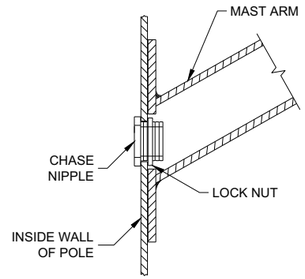


LEVELING SHIM (1)
SHALL BE ALUMINUM



TYPICAL APPLICATION OF GROMMET IN POLE SHAFT



TYPICAL APPLICATION OF CHASE NIPPLE IN POLE SHAFT

GENERAL NOTES

CLAMP BOLT-NUT TIGHTENING TORQUE SHALL BE INDICATED BY INDENT STAMPING (1/2 INCH NUMERALS AND LETTERS) OR WEATHERPROOF PRINTING ON THE INSIDE OF THE CLAMP THAT IS WELDED TO THE ARM MEMBER.

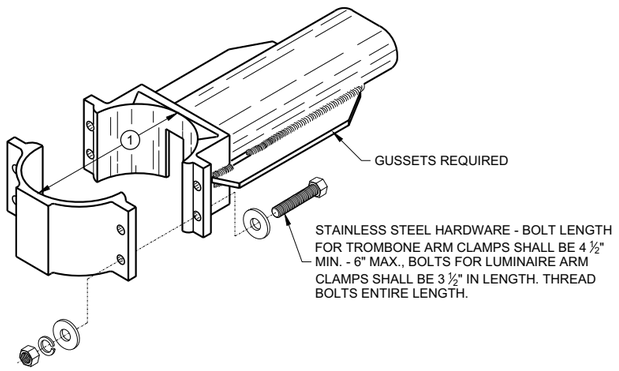
(1) 4.5" I.D. FOR LUMINAIRE MAST ARM CLAMP. 6.625" I.D. FOR TROMBONE MAST ARM CLAMP.

(2) INDIVIDUAL BASE PLATE ANCHOR ROD COVERS. (4 REQUIRED)

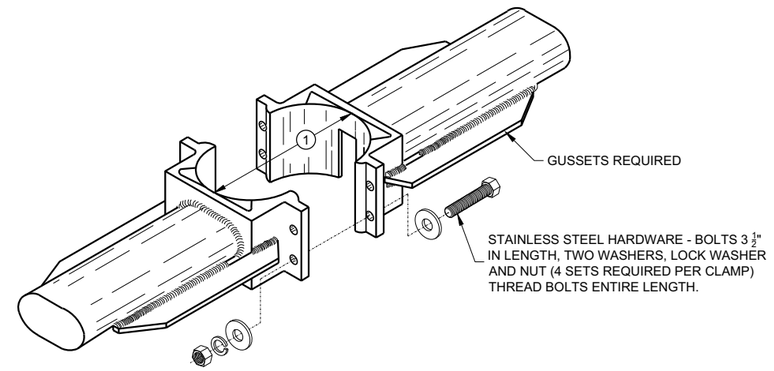
(3) BASE PLATE SLOTTED TO ACCEPT 11" THROUGH 12" BOLT CIRCLE USING 1" DIAMETER ANCHOR RODS.

(4) LEVELING SHIMS, DESIGNED FOR THE PURPOSE, SHALL BE USED WHEN PLUMBING POLES. THE USE OF WASHERS IN LIEU OF PROPER LEVELING SHIMS IS NOT ACCEPTABLE. LEVELING SHIMS SHALL BE USED ONLY BETWEEN THE TOP OF THE CONCRETE BASE AND A METALLIC BASE PLATE.

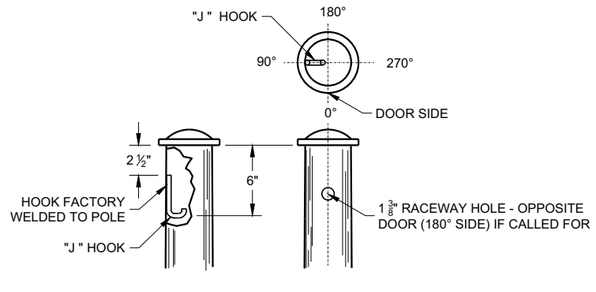
SHIMS SHALL BE LONG ENOUGH AND WIDE ENOUGH TO COMPLETELY COVER THE AREA UNDER THE LENGTH AND WIDTH OF THE BASE MOUNTING FLANGE.



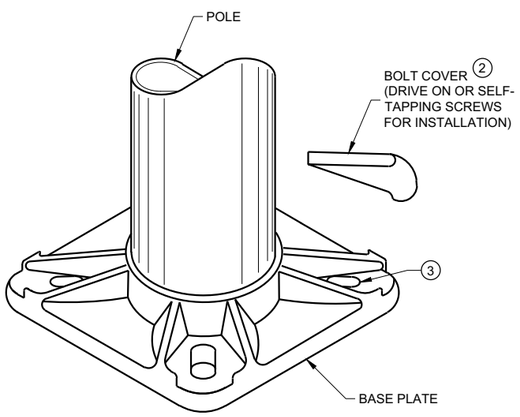
TYPICAL TROMBONE MAST ARM AND SINGLE LUMINAIRE MAST ARM MOUNTING CLAMP



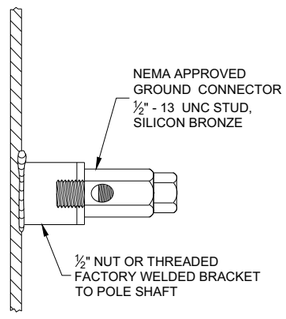
TYPICAL LUMINAIRE MAST ARM (DOUBLE) MOUNTING BRACKETS

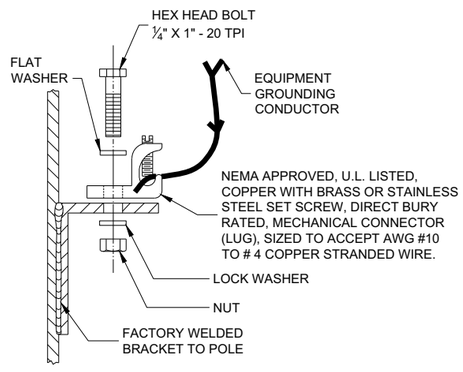


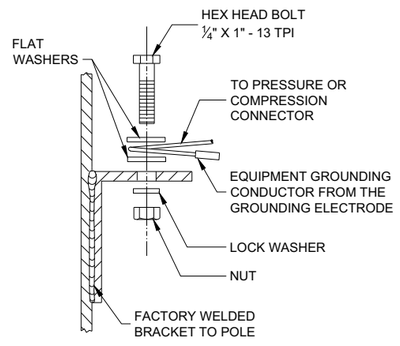
TYPICAL "J" HOOK LOCATION



BASE PLATE







TYPICAL GROUNDING CONNECTIONS
NUT, BOLT AND WASHERS SHALL BE STAINLESS STEEL

HARDWARE DETAILS FOR POLE MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018
DATE
/S/ Ahmet Demirbilek
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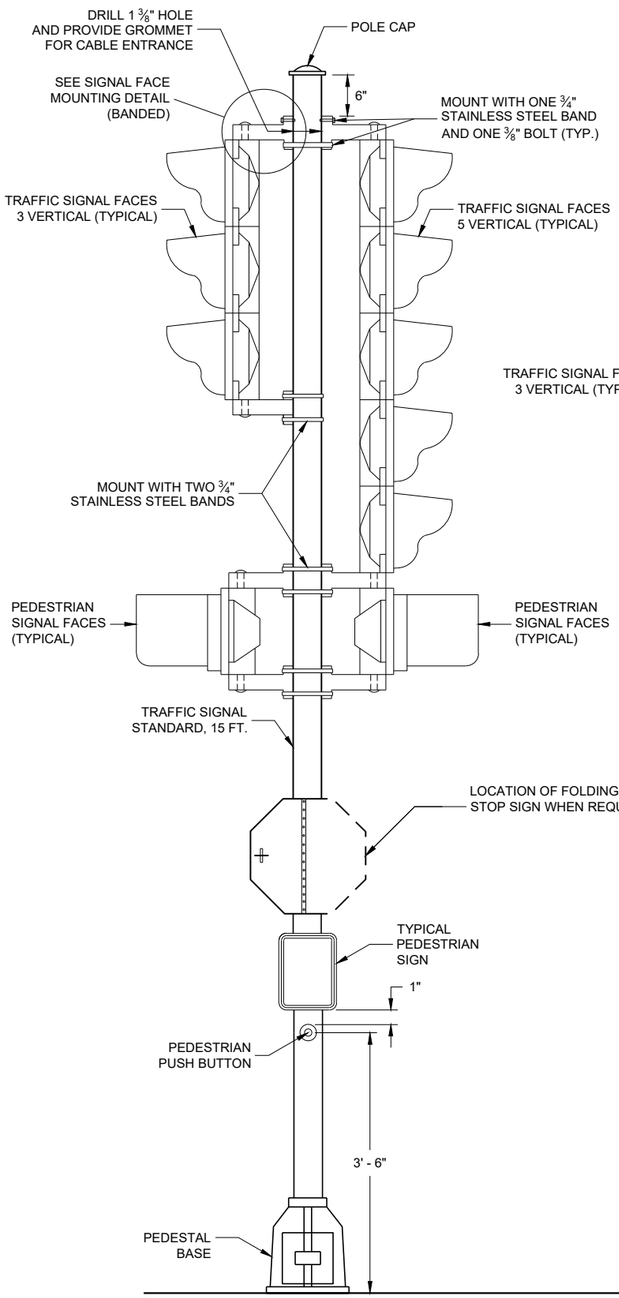
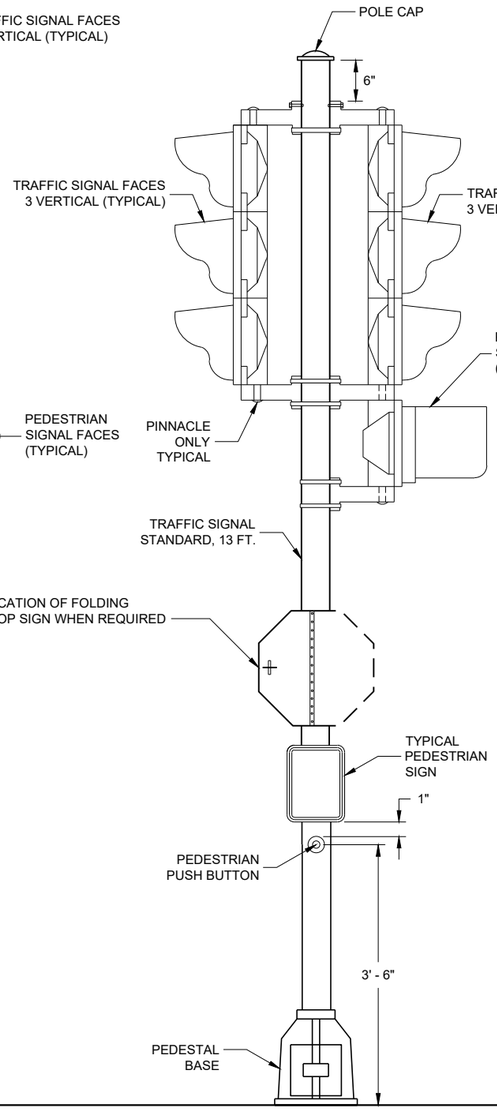
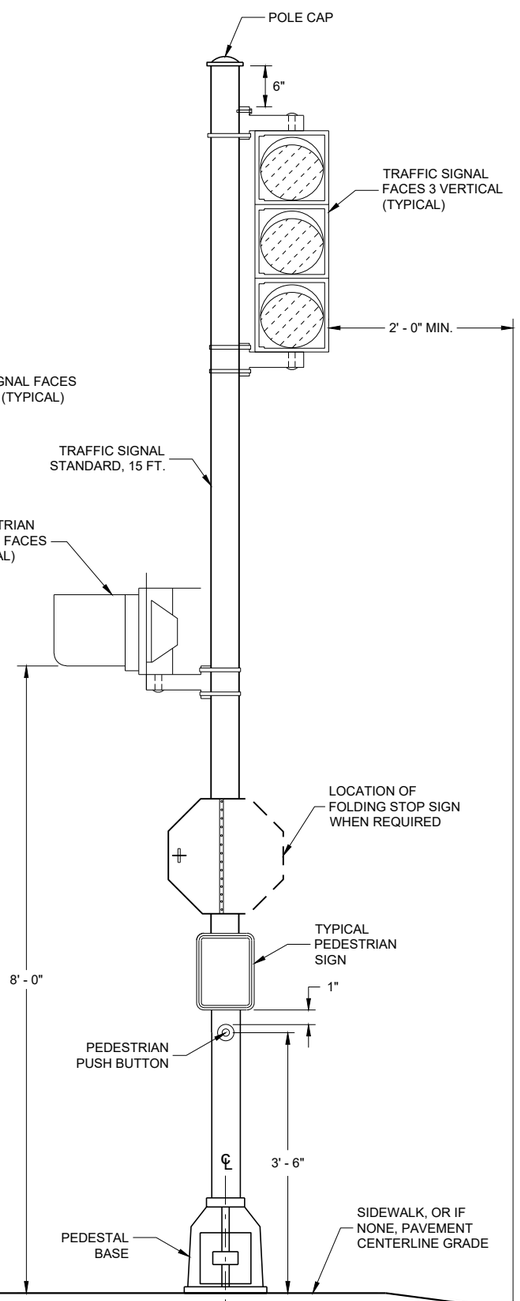


TRAFFIC SIGNAL STANDARD - 15 FT.



TRAFFIC SIGNAL STANDARD - 13 FT.



TRAFFIC SIGNAL TANDARD - 15 FT.
3M MOUNTING (TYPICAL)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

SEE THE SIGNAL PLAN FOR REQUIRED SIGNAL FACE SIZES.

ALL PEDESTAL BASES SHALL BE MOUNTED ON CONCRETE BASE - TYPE 1.

FOR APPROVED MOUNTING HARDWARE, SEE THE CONTRACT SPECIAL PROVISIONS.

POLYCARBONATE MOUNTING BRACKETS SHALL BE USED.

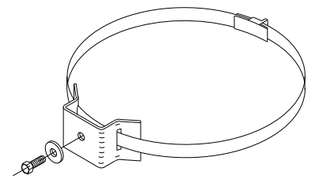
LENGTH AND LOCATION OF TRAFFIC SIGNAL STANDARDS SHALL BE AS SHOWN ON THE PLANS.

OPTICALLY PROGRAMMED SIGNAL FACES SHALL BE MASKED IN ACCORDANCE WITH MANUFACTURERS INSTRUCTIONS, AND UNDER THE DIRECTIONS OF THE REGION TRAFFIC ENGINEER.

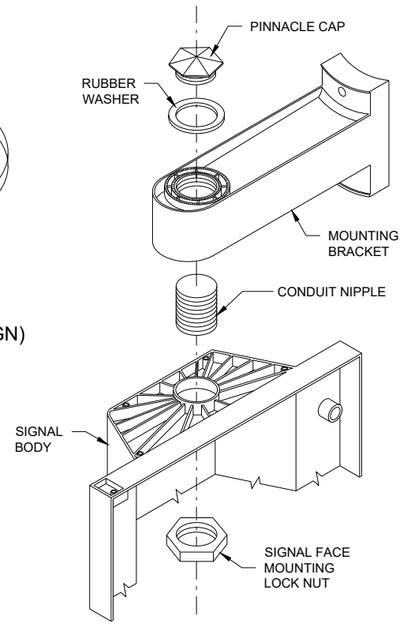
FOLDING STOP SIGNS SHALL BE IN ACCORDANCE WITH THE MUTCD AND/OR THE LATEST WISCONSIN SUPPLEMENT. THE SIGNS SHALL BE SIZED AND LOCATED AS CALLED FOR IN IN THE PLANS.

PEDESTRIAN SIGNS SHALL BE AS DESIGNATED IN THE PLANS.

FURNISH AND INSTALL VENTILATED, CAST, METALLIC (ALUMINUM ALLOY) CAPS. FASTEN CAPS WITH ONE (1) 1/4" X 3/4" - 20 TPI STAINLESS STEEL, HEX HEAD BOLT.



TYPICAL SIGN MOUNTING BAND
(TOP AND BOTTOM OF SIGN)



SIGNAL FACE MOUNTING DETAIL (BANDED)

TRAFFIC SIGNAL STANDARD POLY BRACKET MOUNTINGS (TYPICAL) 13 FT. OR 15 FT.

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
2/28/2013
DATE
/S/ Ahmet Demirbilek
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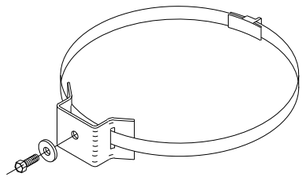
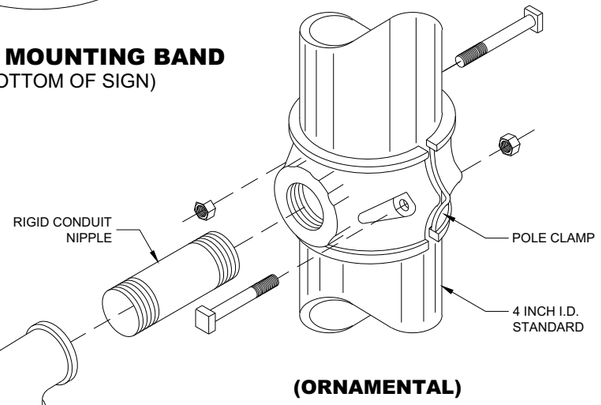
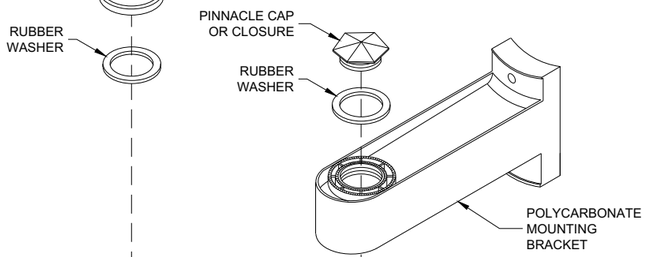


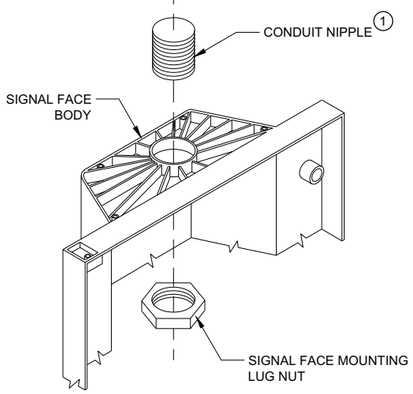
TYPICAL SIGN MOUNTING BAND
(TOP AND BOTTOM OF SIGN)



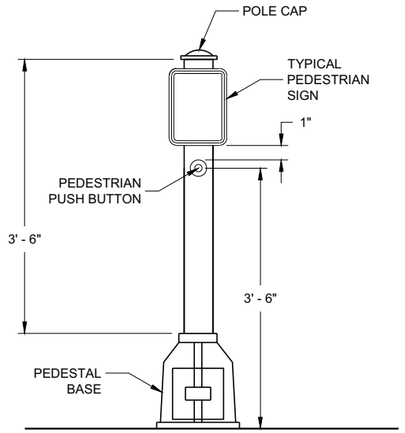
(ORNAMENTAL)



(BANDED)



SIGNAL FACE MOUNTING DETAILS



PEDESTRIAN PUSH BUTTON TYPICAL MOUNTING

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

SEE THE SIGNAL PLAN FOR REQUIRED SIGNAL FACE SIZES.

LOCATIONS SHALL BE AS SHOWN ON THE PLANS, UNLESS APPROVED BY THE ENGINEER IN THE FIELD.

ALL PEDESTAL BASES SHALL BE MOUNTED ON CONCRETE BASE - TYPE 1.

FOR APPROVED MOUNTING HARDWARE, SEE THE CONTRACT SPECIFICATIONS.

POLYCARBONATE SIGNAL FACE MOUNTING BRACKETS SHALL BE USED UNLESS ORNAMENTAL POLE CLAMPS ARE SPECIFIED.

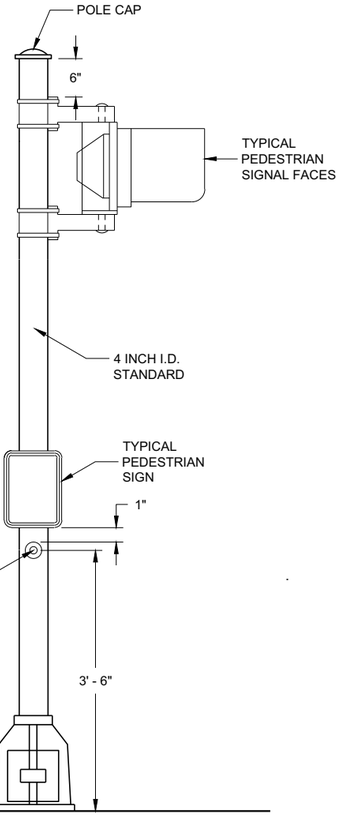
LENGTH OF TRAFFIC STANDARDS SHALL BE AS SHOWN ON THE PLANS.

MOUNTINGS AND BRACKETS SHALL BE AS SHOWN ON THE PLANS OR DESCRIBED IN THE SPECIAL PROVISIONS (BY THE REGION TRAFFIC ENGINEER).

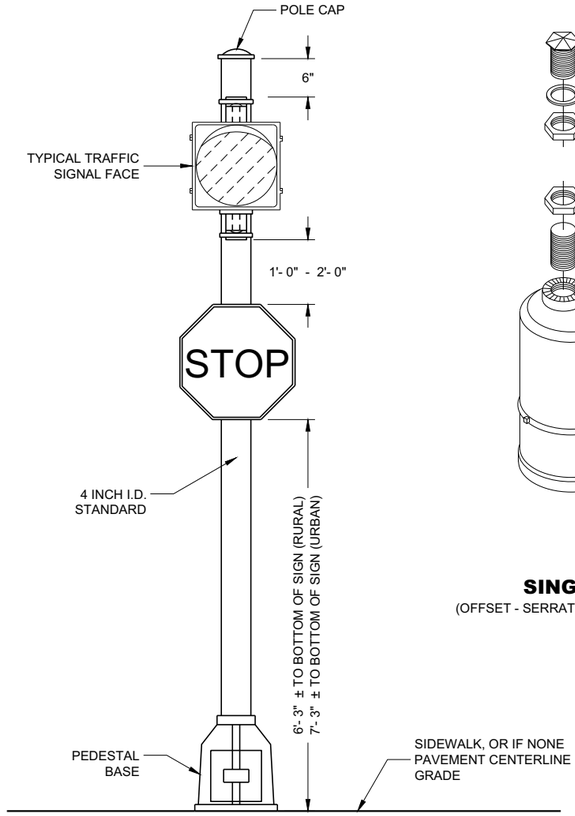
PEDESTRIAN SIGNS SHALL BE AS DESIGNATED IN THE PLANS.

FURNISH AND INSTALL VENTILATED, CAST, METALLIC (ALUMINUM ALLOY) CAPS. FASTEN CAPS WITH ONE (1) ¼" X ¾" - 20 TPI STAINLESS STEEL, HEX HEAD BOLT.

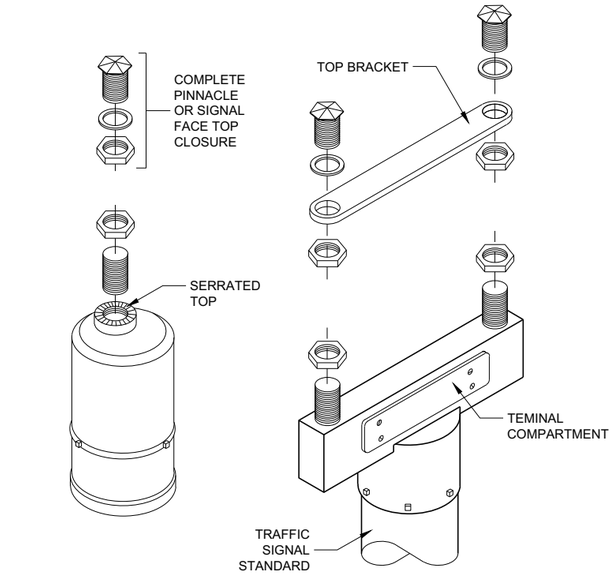
(1) USE 1 ½" ID NIPPLES ZINC-COATED RIGID METAL CONDUIT, LONG ENOUGH TO ACCOMMODATE FULL DEPTH THREADING INTO THE HEAD MOUNTING LOCK NUT IN ORDER TO TIGHTEN THE FACE, BUT THAT DO NOT INTERFERE WITH REFLECTOR CLOSURE. THREAD THE NIPPLE INTO THE MOUNTING BRACKET/ELBOW UNTIL TIGHT. USE APPROVED PINNACLE TYPE HARDWARE FROM A DEPARTMENT APPROVED MANUFACTURER TO CLOSE THE UNUSED 1 ½" OPENING IN SIGNAL FACESAND BRACKET ENDS.



PEDESTRIAN FACE STANDARD - 10 FT.
(WALK - DON'T WALK)



STANDARD FLASHER
10 FOOT, 13 FOOT OR 15 FOOT AS REQUIRED



SINGLE
(OFFSET - SERRATED MOUNTING)

DOUBLE
(SERRATED MOUNTING)

SLIPFITTERS

TRAFFIC SIGNAL STANDARD PEDESTRIAN AND FLASHER TYPICAL MOUNTING DETAILS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018
DATE

/S/ Ahmet Demirbilek
STATE ELECTRICAL ENGI

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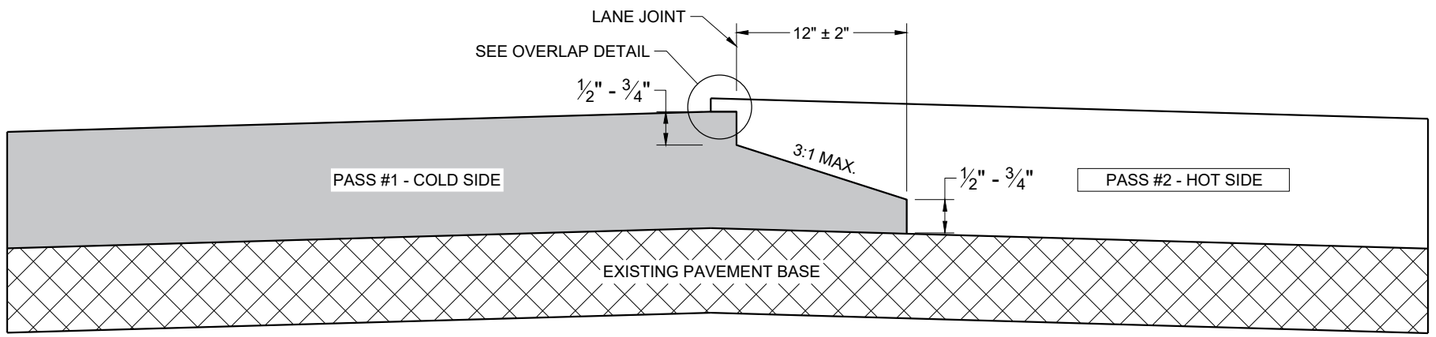
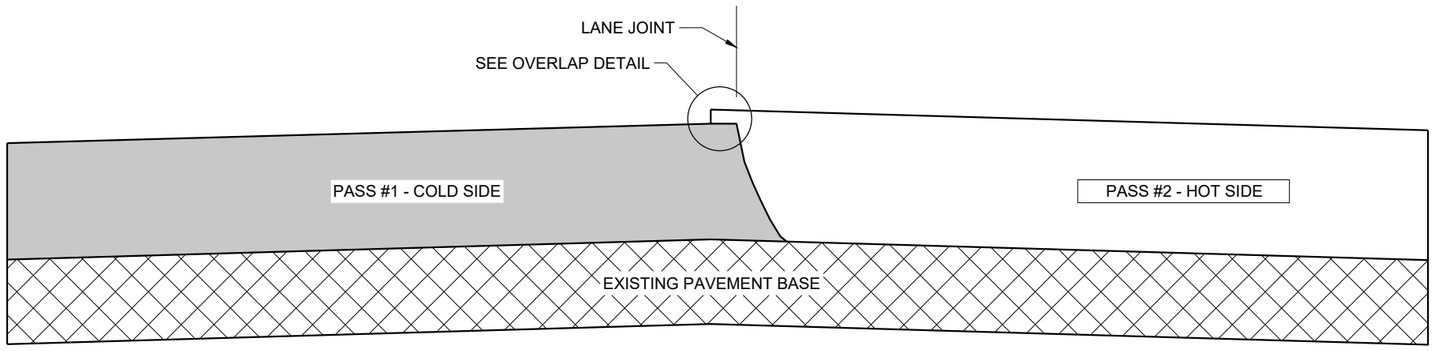
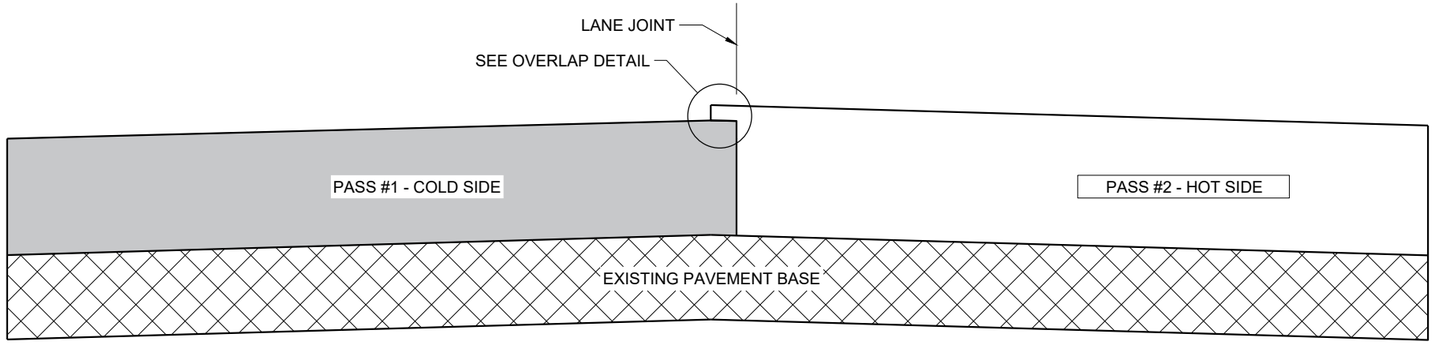


TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)

GENERAL NOTES

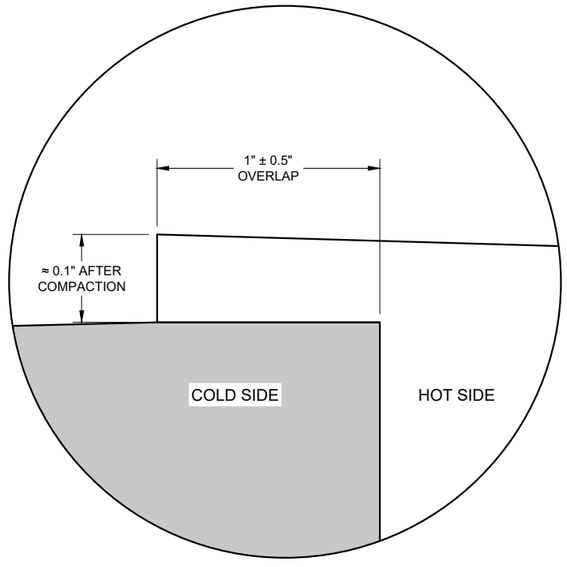
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY 1" ± 0.5" AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2020
DATE

/S/ Steven Hefel
HMA PAVEMENT ENGIN

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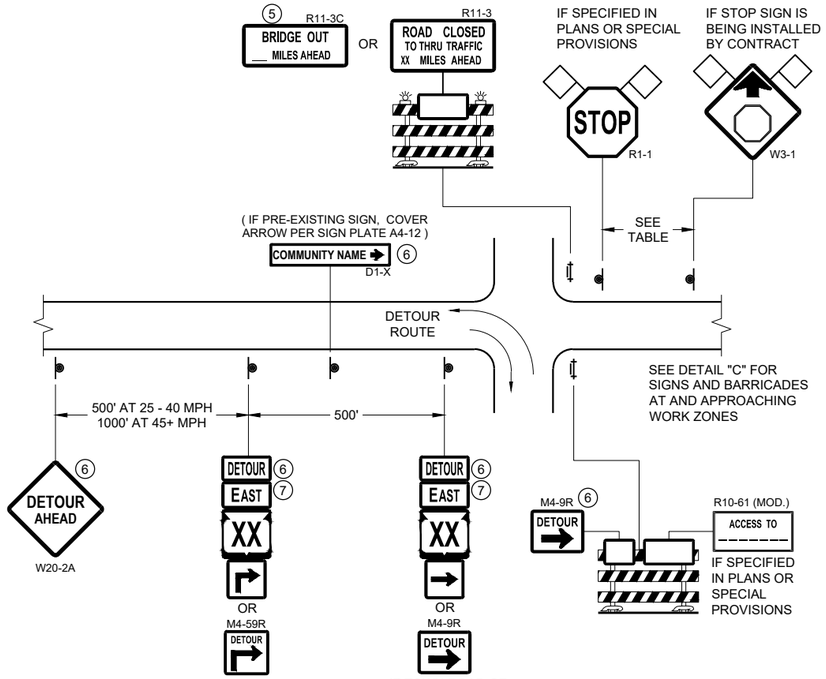
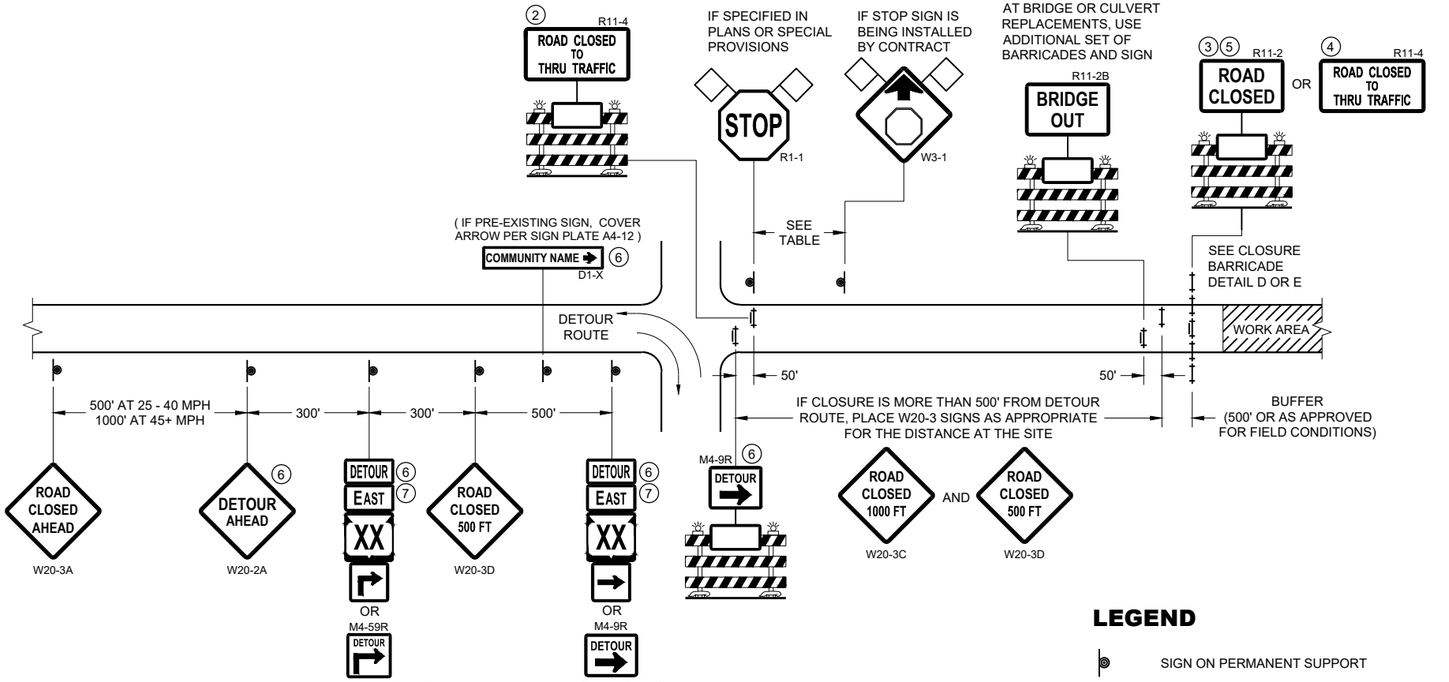


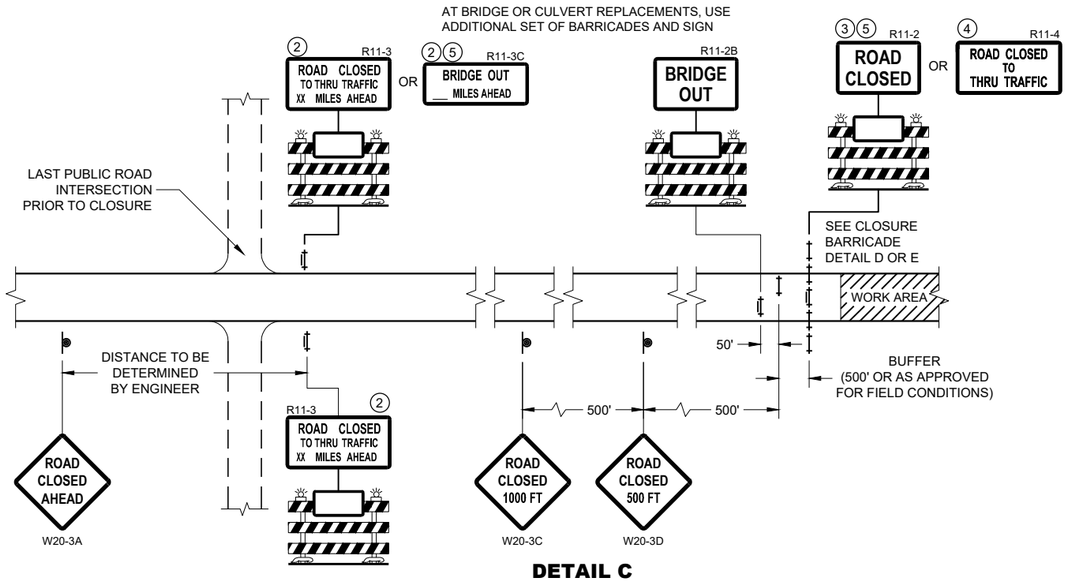


DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN OR EQUAL TO ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)

DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)
- DETOUR — M4 - 8
- EAST — M3 - X
- XX — M1 - 4 OR XX — M1 - 6 OR COUNTY X — M1 - 5A
- M05 - 1 OR M06 - 1

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

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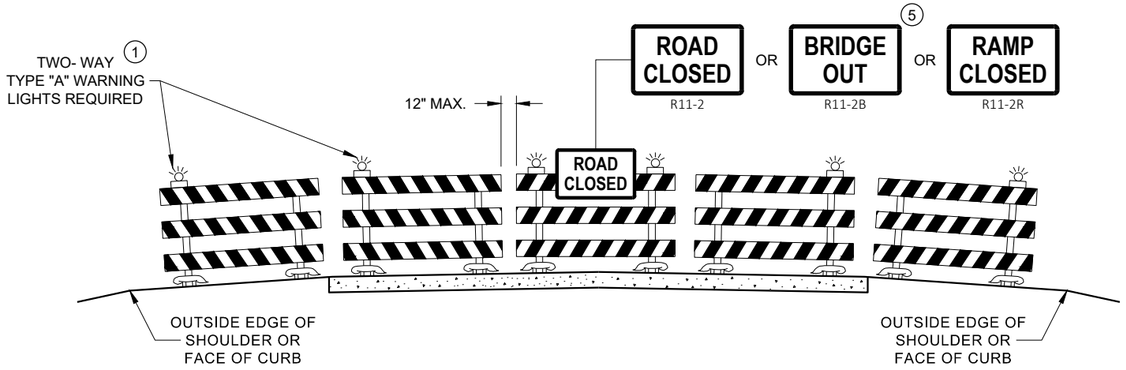
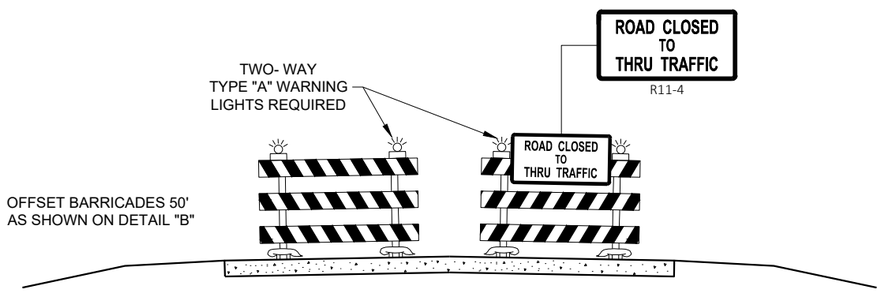
SDD 15C02 - 09a



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60 " X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING.

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR VARIOUS CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEE
FHWA

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SDD 15C02 - 09b

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SDD 15C02 - 09b

THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- DETOUR — M4 - 8
- EAST — M3 - X
- XX — M1 - 4 OR XX — M1 - 6 OR COUNTY X — M1 - 5A
- M05 - 1 OR M06 - 1 OR M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

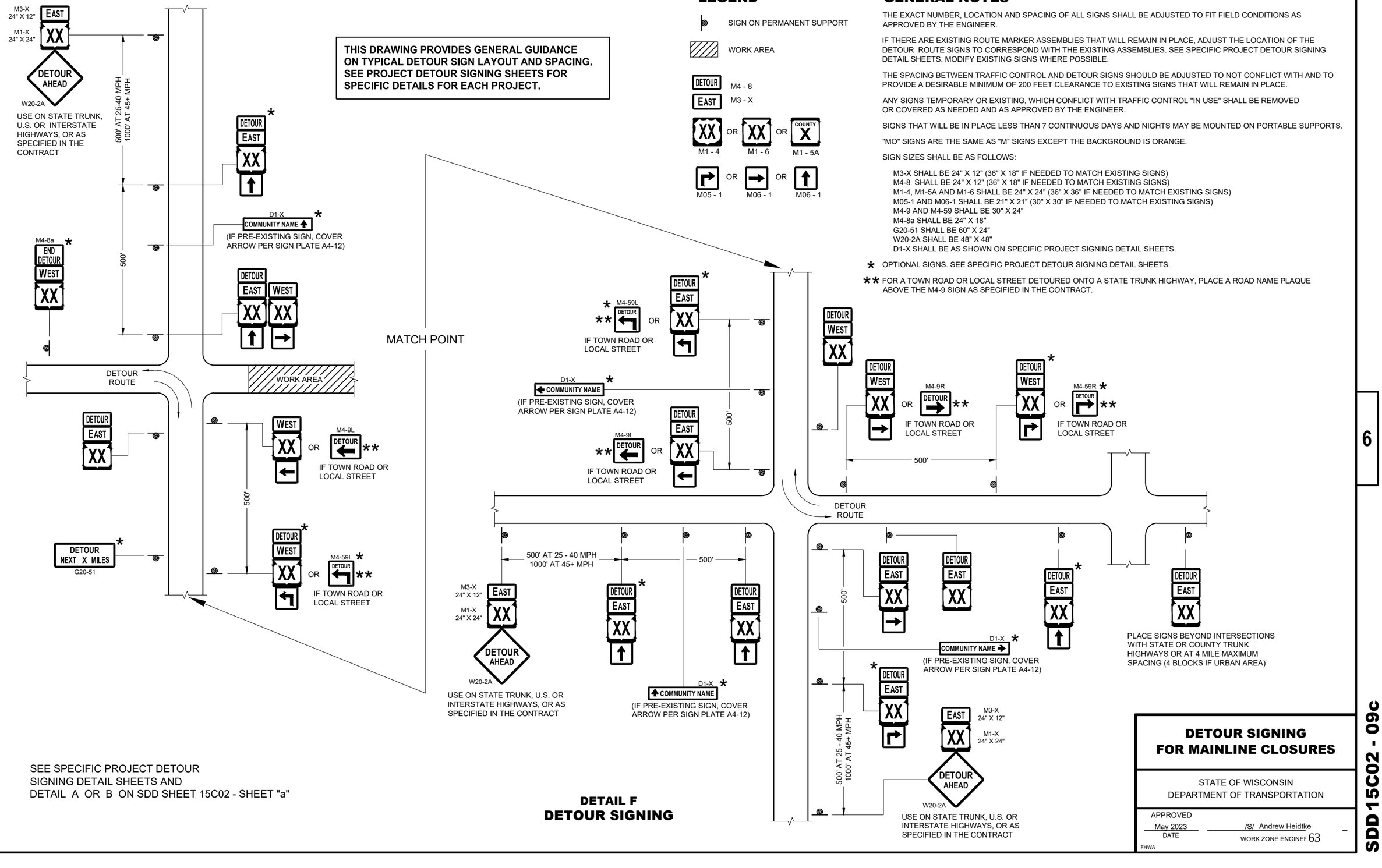
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-59 SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

* OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.

** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT



SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

DETAIL F
DETOUR SIGNING

DETOUR SIGNING FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Andrew Heidtke — WORK ZONE ENGINEER

FHWA

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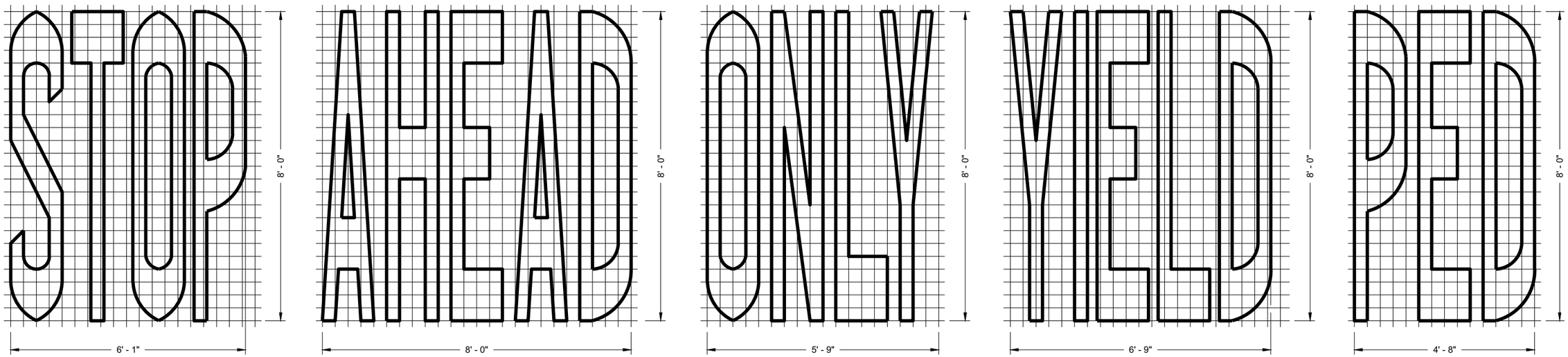
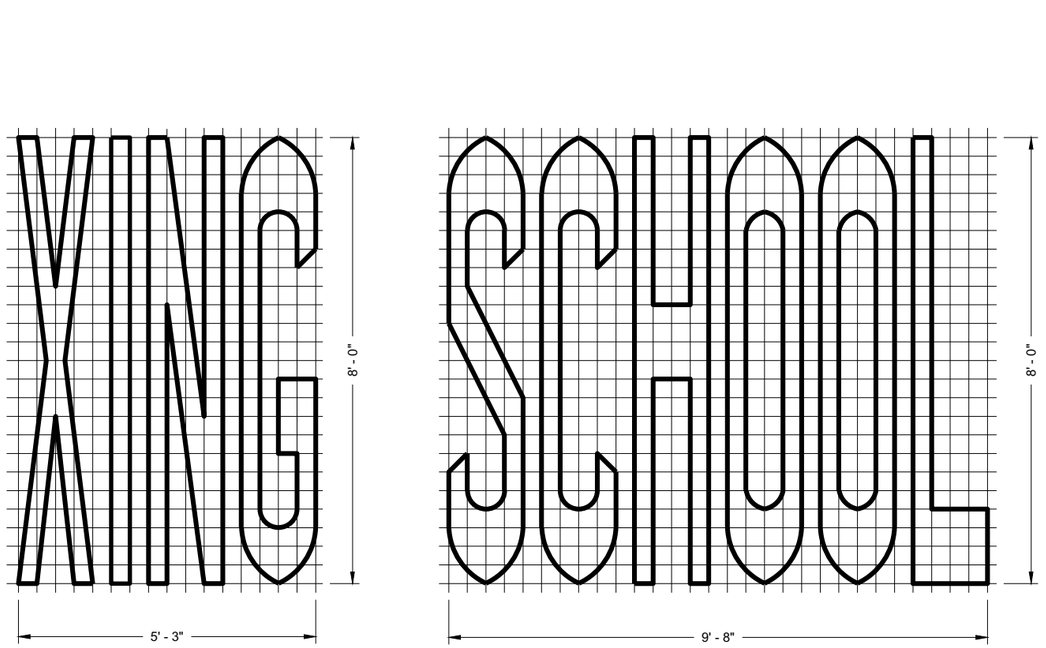
SDD 15C02 - 09c





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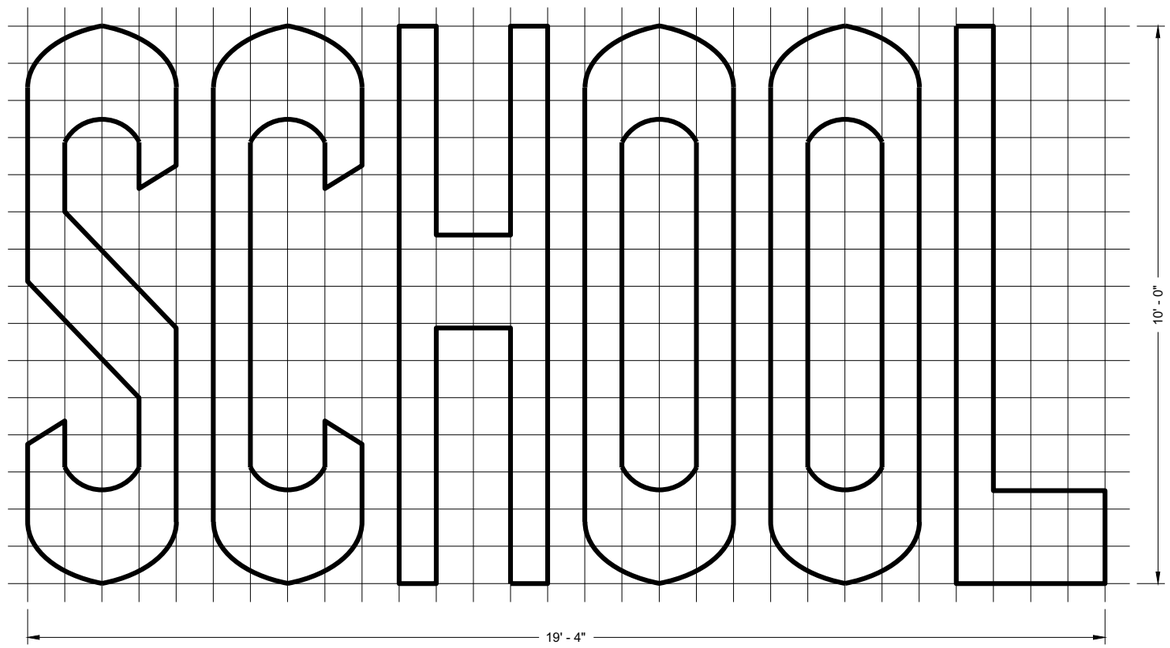

TWO - LANE

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING WORDS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019
DATE

/S/ Matthew Rauch
STATE SIGNING AND MAF ENGINEER

FHWA

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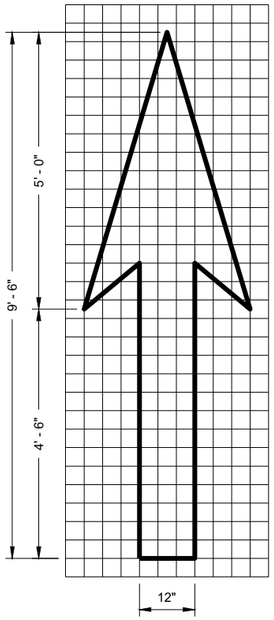
SDD 15C07 - 15b

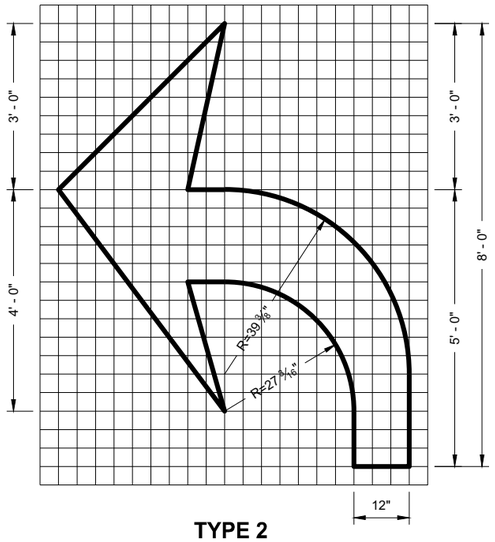
SDD 15C07 - 15b

GENERAL NOTES

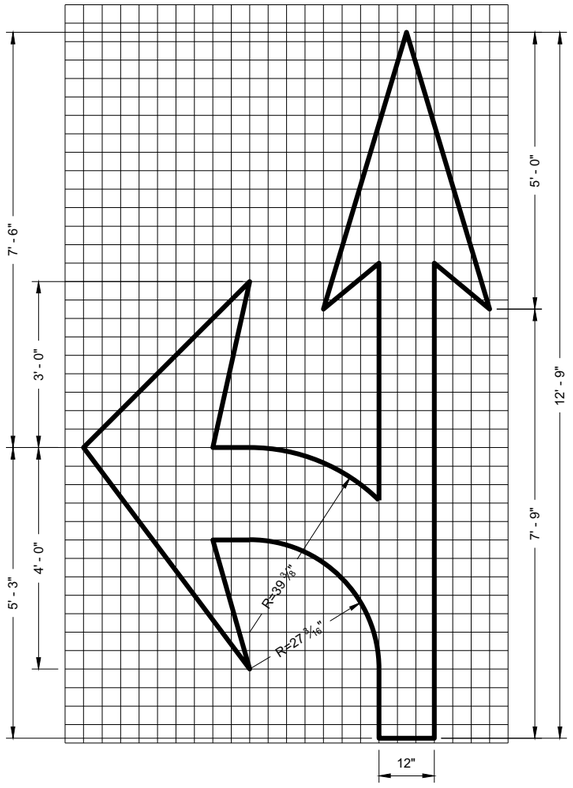
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



TYPE 1



TYPE 2



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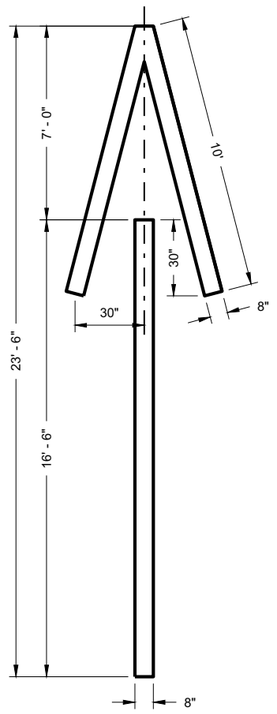

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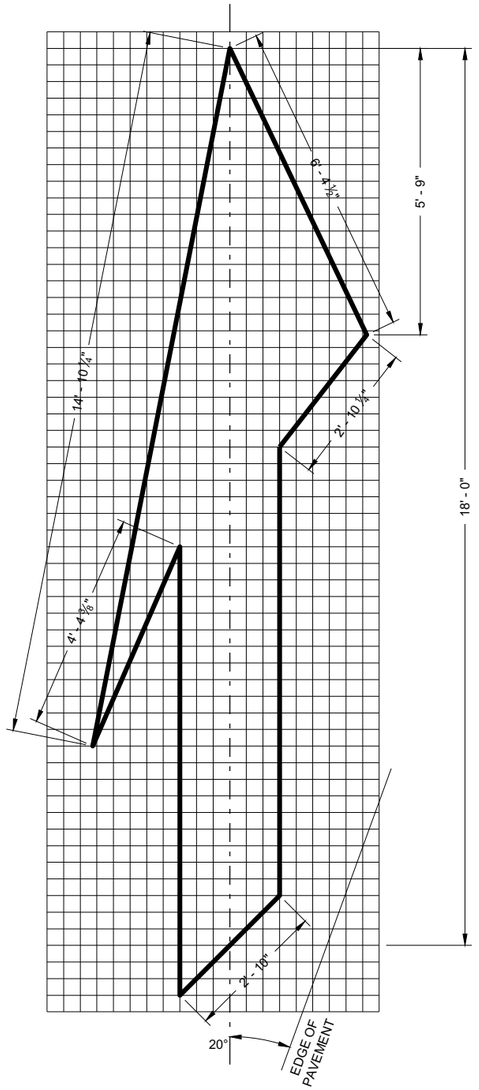
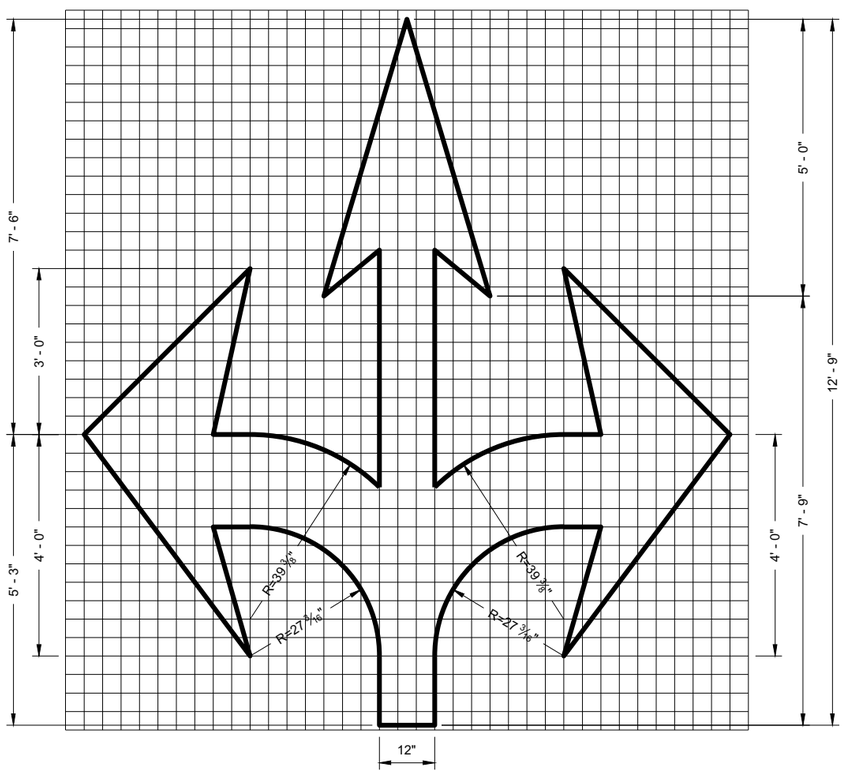
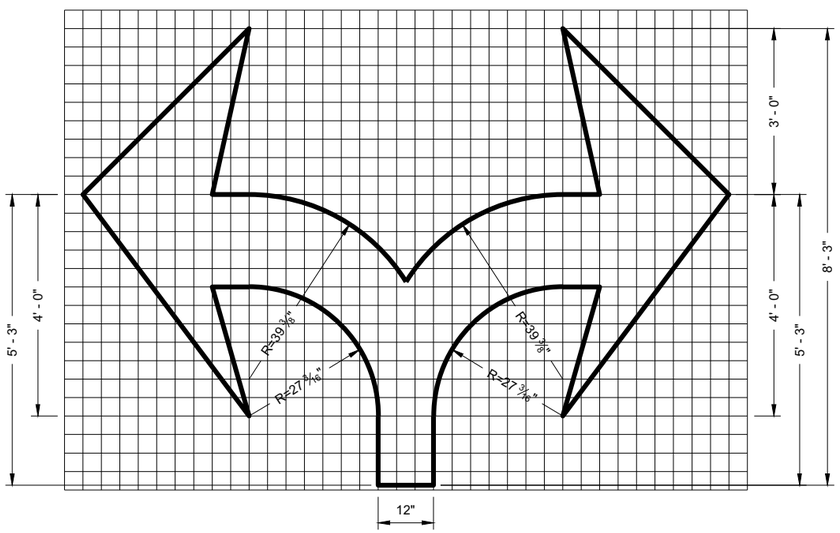


TYPE 5 LANE DROP ARROW



TYPE 6



TYPE 7

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 — DATE
/S/ Matthew Rauch — STATE SIGNING AND MARKING ENGINEER

FHWA

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SDD 15C07 - 15c

6

SDD 15C07 - 15c

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

(1) LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING

(2) MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

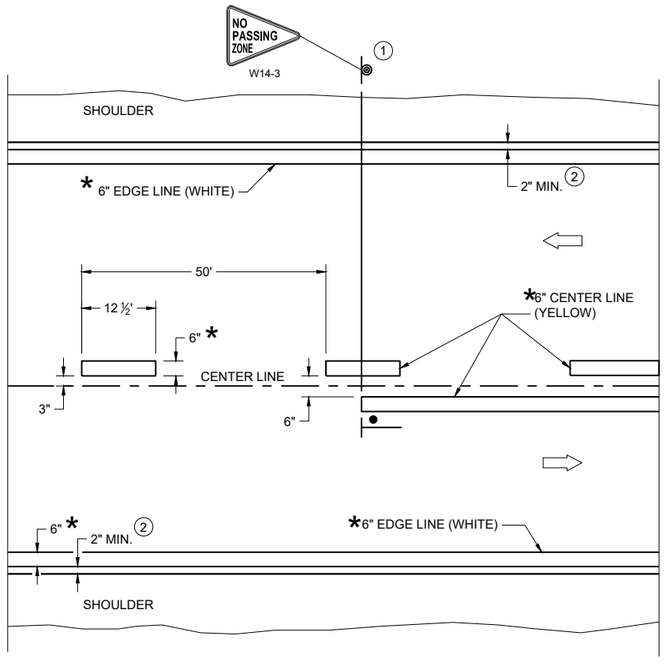
*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

LEGEND

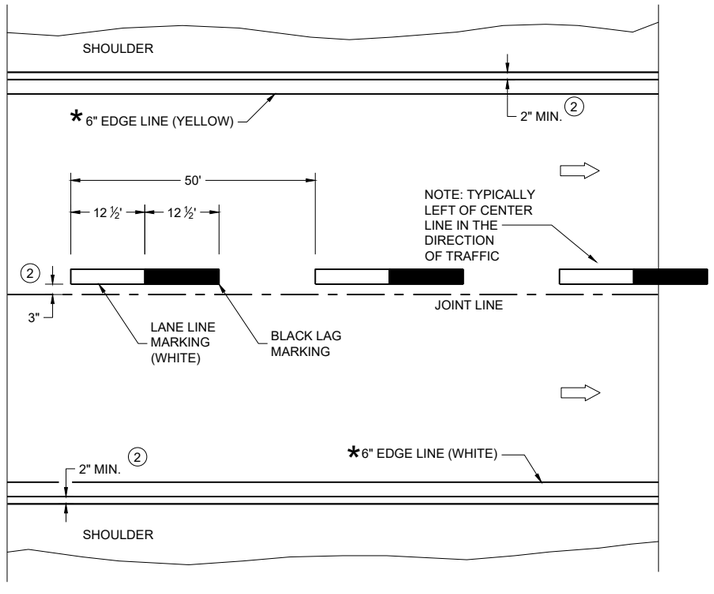
"T" MARKING

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

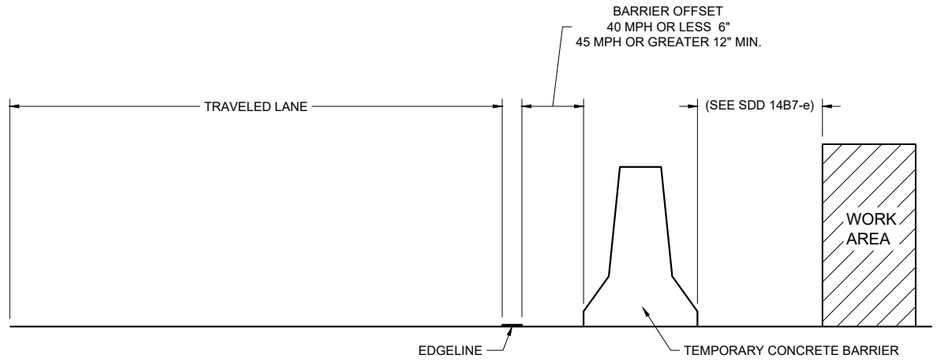
PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Jeannie Silver — Statewide Pavement Marking Engineer
FHWA

6

SDD 15C08-23a



TEMPORARY BARRIER OFFSET FROM EDGELINE

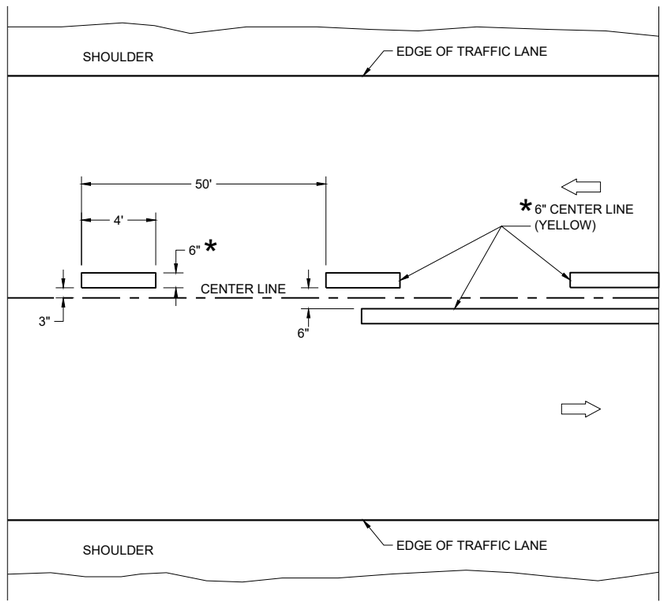
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

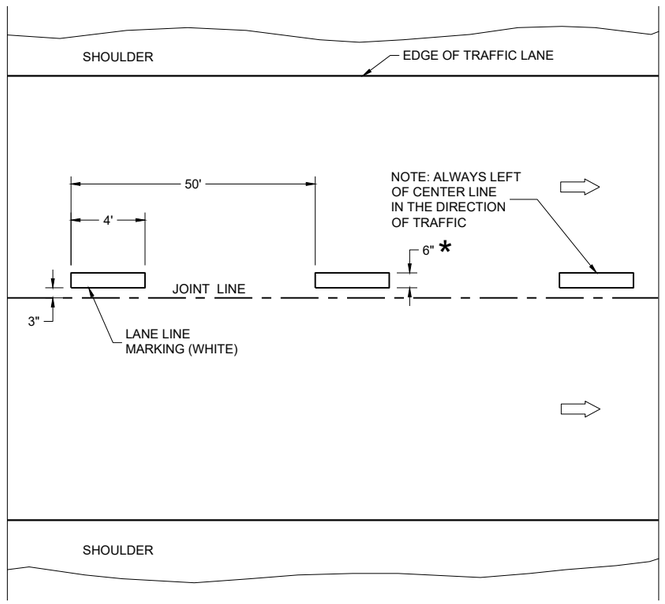
LEGEND

DIRECTION OF TRAFFIC

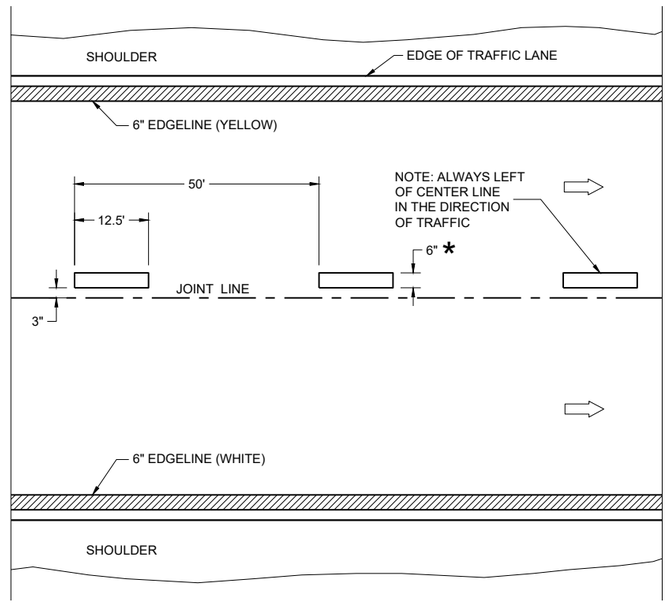
*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Jeannie Silver — Statewide Pavement Marking Engineer

FHWA

6

SDD 15C08-23b

6

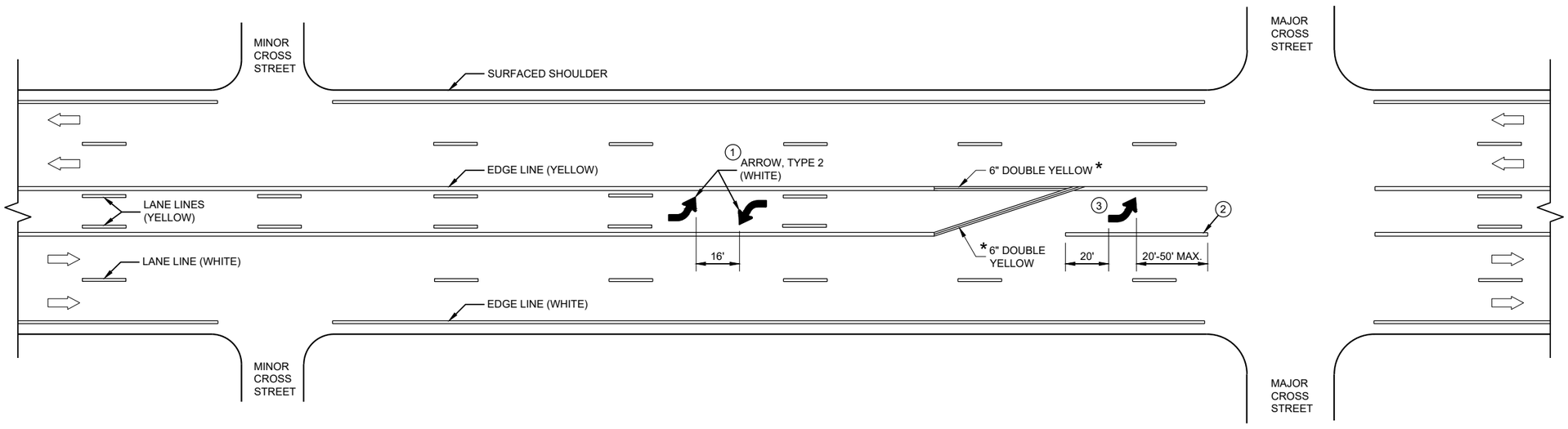
SDD 15C08-23b

GENERAL NOTES

1. A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
2. 10" WHITE
3. TURN BAY LENGTH OF LESS THAN 48' DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT.

⇨ DIRECTION OF TRAFFIC

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY LEFT TURN LANE

6

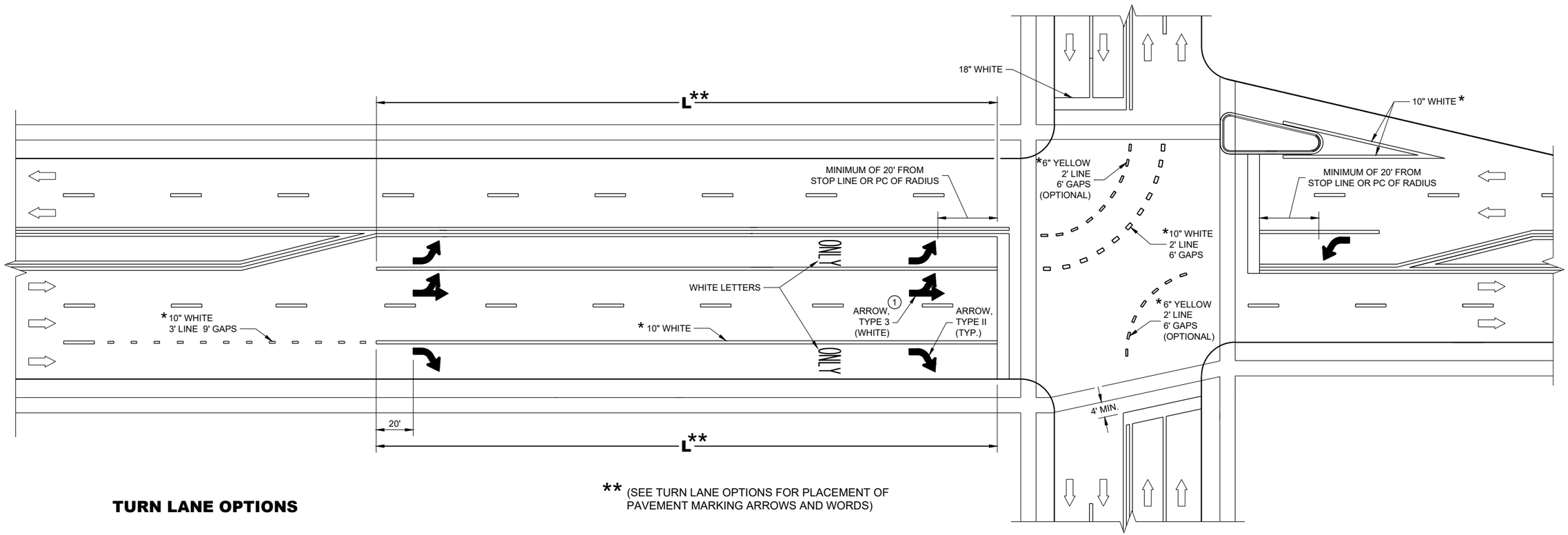
SDD 15C08-23c

PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTA· 68

6

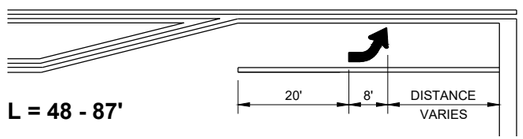
SDD 15C08-23c



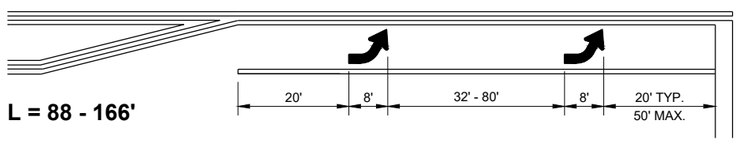
** (SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

TURN LANE OPTIONS

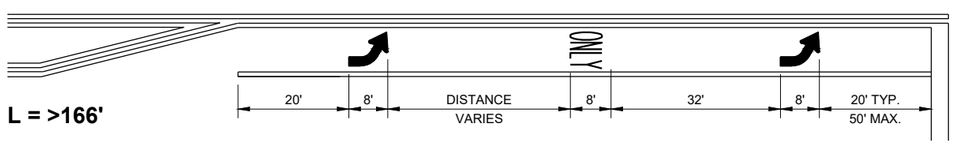
LENGTH OF TURN BAY (**L**) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



L = 48 - 87'



L = 88 - 166'



L = >166'

GENERAL NOTES

1. QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

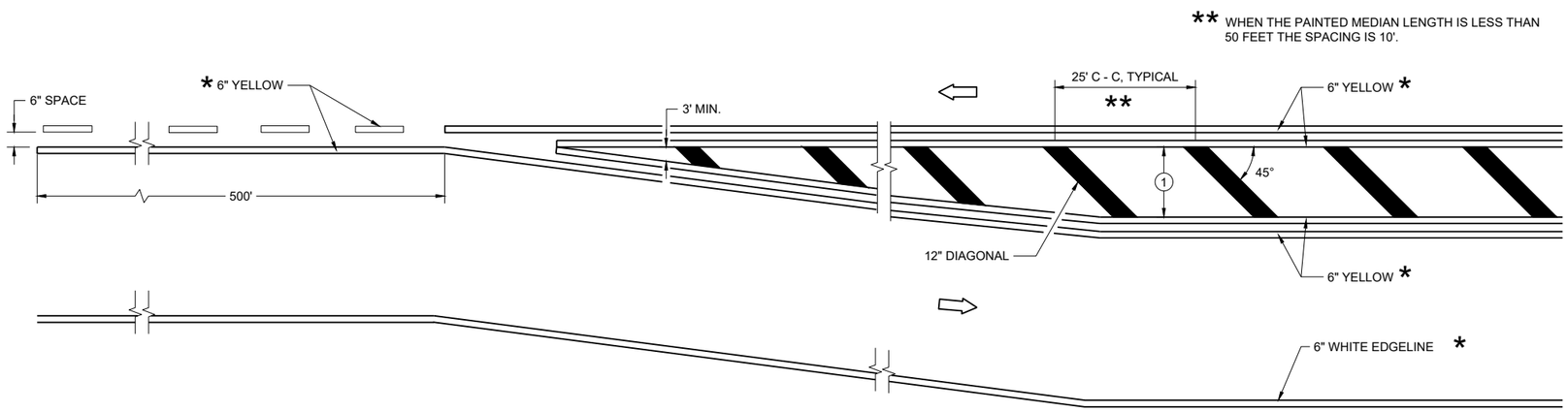
* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 15C08-23d



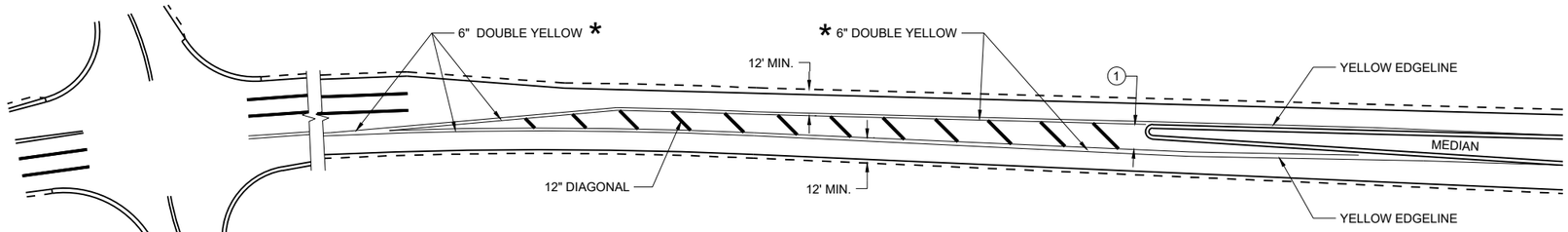
MEDIAN ISLAND DETAIL

GENERAL NOTES

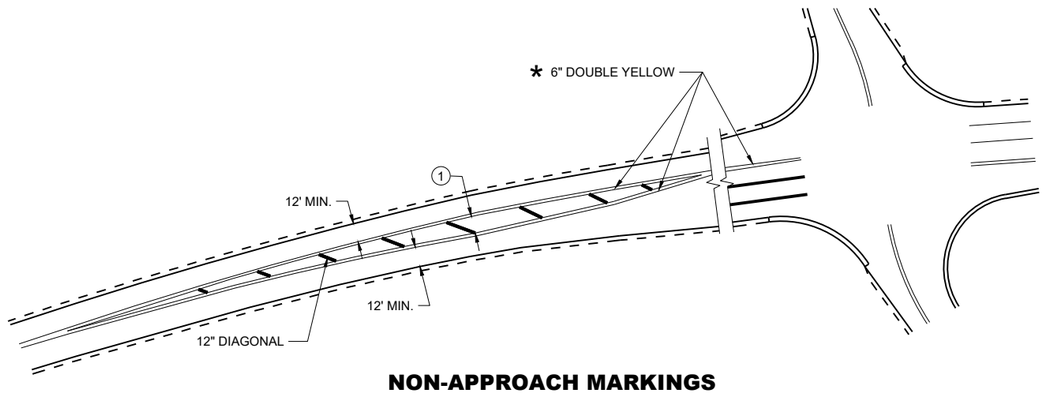
(1) DIAGONALS ARE OPTIONAL WHEN PAINTED ISLAND IS LESS THAN 6 FEET AT THE WIDEST POINT. OMIT DIAGONALS IF WIDTH IS LESS THAN 4 FEET.

DIRECTION OF TRAVEL

*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



APPROACH MARKINGS FOR OTHER MEDIAN TYPES



NON-APPROACH MARKINGS

MEDIAN ISLAND PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
August 2024
DATE

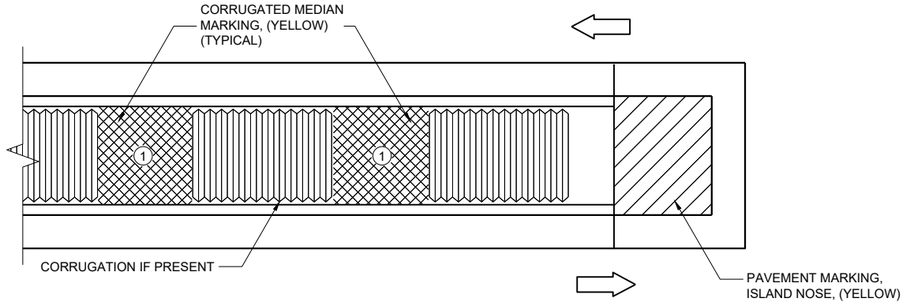
/S/ Jeannie Silver
Statewide Pavement Marking Engineer

FHWA

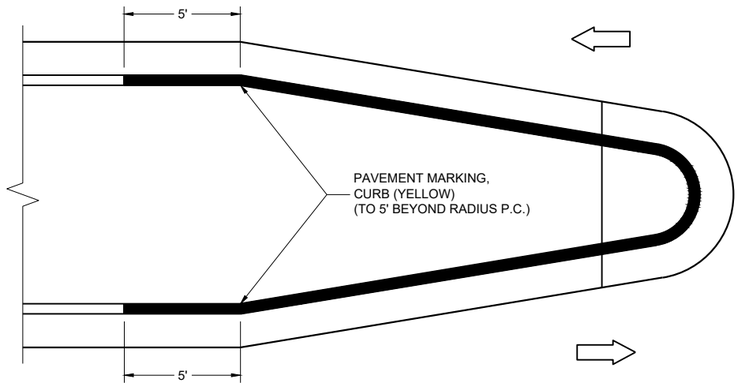
6

SDD 15C18-09a

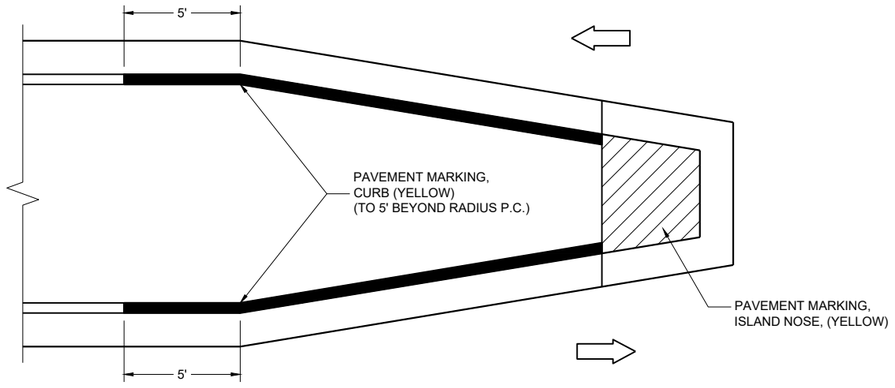
SDD 15C18-09a



MEDIAN ISLAND WITH SQUARE BLUNT NOSE



MEDIAN ISLAND WITH ROUND BLUNT NOSE



MEDIAN ISLAND WITH SLOPED NOSE

**TYPICAL PLACEMENT OF
PAVEMENT MARKING ON MEDIAN ISLANDS**

GENERAL NOTES

WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION, YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN. THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.

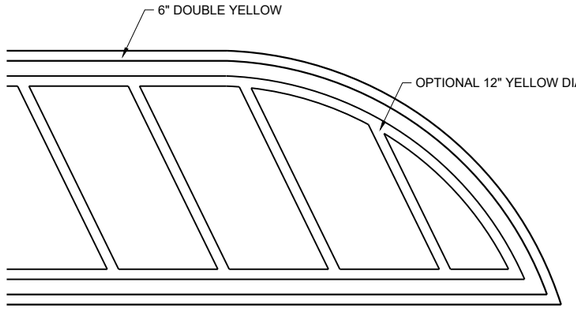
(1) APPLY PAVEMENT MARKING TO THE FLAT PORTION OF CORRUGATED MEDIAN.

ISLAND NOSE MARKING

CURB MARKING

CORRUGATED MEDIAN MARKING

DIRECTION OF TRAVEL



FLUSH MEDIAN ISLAND NOSE

**PAVEMENT MARKINGS,
MEDIAN ISLAND NOSE**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
August 2024
DATE

/S/ Jeannie Silver
Statewide Pavement Marking Engineer

FHWA

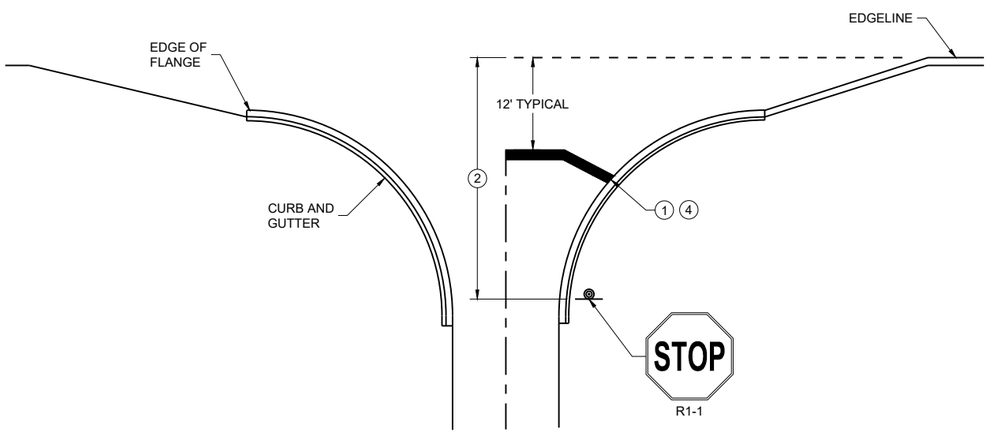
6

SDD 15C18-09b

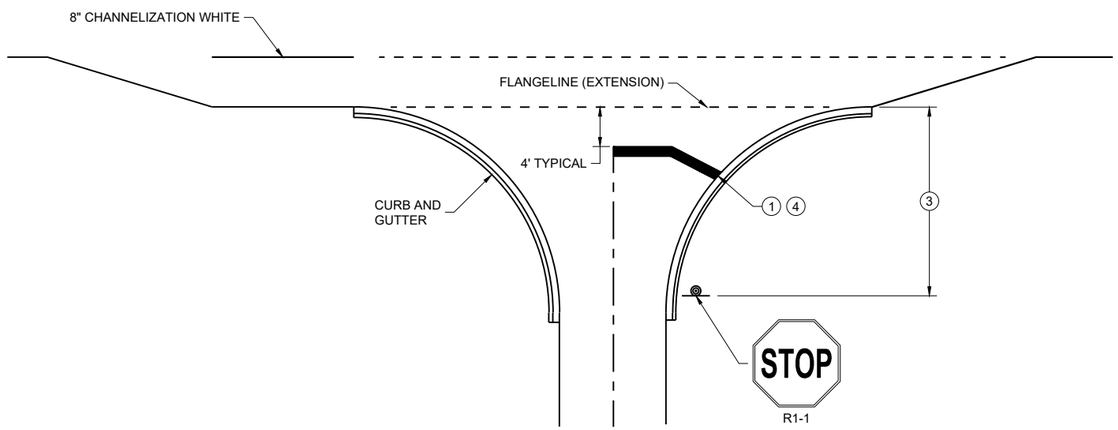
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

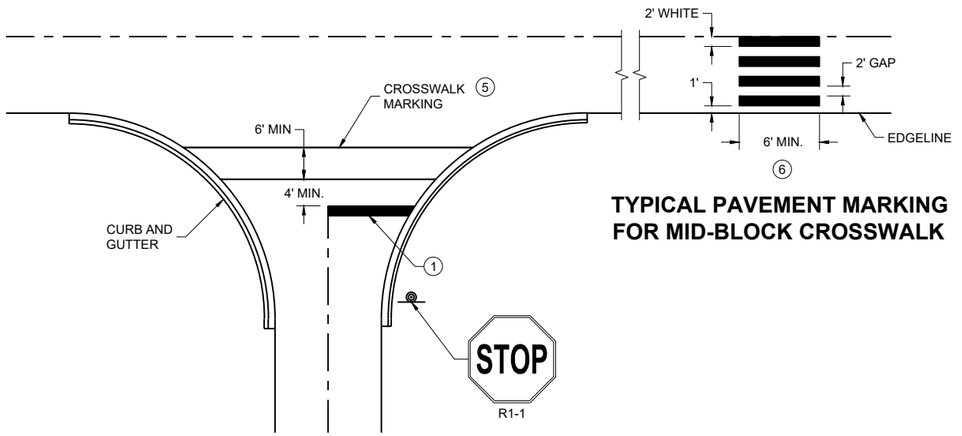
1. 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
2. NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
3. NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGE LINE EXTENSION.
4. MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
5. LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES.
6. POSTED SPEED LIMITS OF 40 MPH OR GREATER USE A MINIMUM WIDTH OF 8' FOR MIDBLOCK CROSSWALKS



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

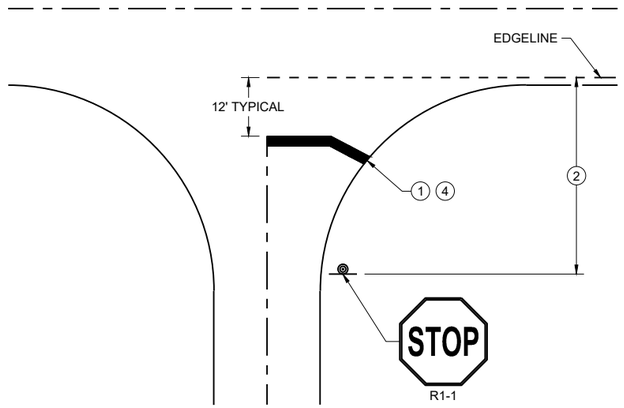


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDE ROADS WITH RIGHT TURN LANE



TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK (5)

TYPICAL STOP LINE PAVEMENT MARKING FOR SIDE ROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

March 2024 — DATE

/S/ Matthew Rauch — STATE SIGNING AND MARKING ENGINEER

FHWA

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKINGS
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- WORK AREA

GENERAL NOTES

FOR WORK ON ROADWAYS WITH SPEEDS GREATER THAN 45MPH, USE SDD 15D12.

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

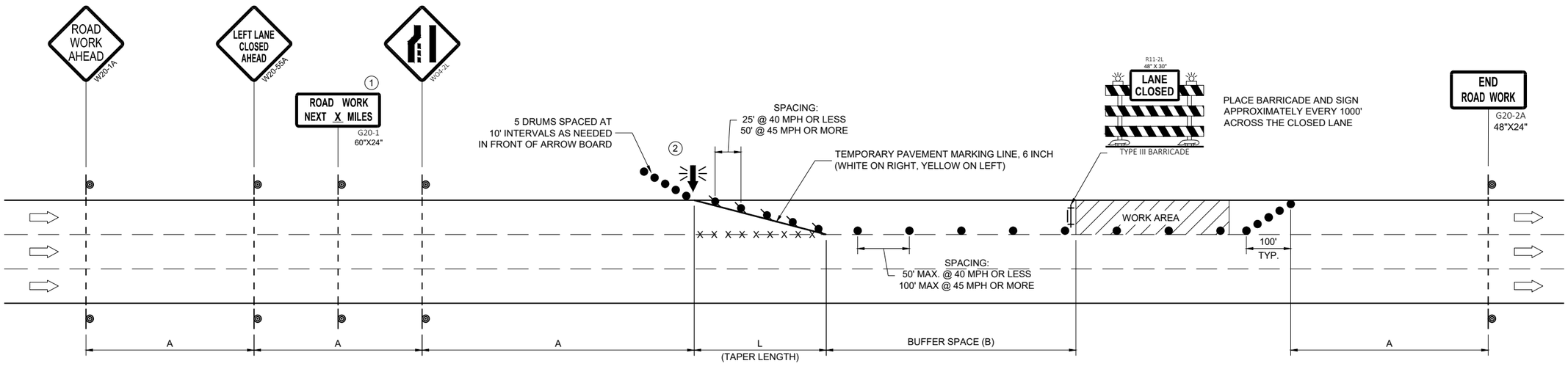
BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

(1) OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

(2) WHERE THE SHOULDER OR TERRACE HAS INSUFFICIENT SPACE TO PLACE THE ARROW BOARD AS SHOWN, PLACE THE ARROW BOARD AT THE END OF THE TAPER.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'

TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2024
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

6
SDD 15D20-08a

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKINGS
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- WORK AREA

GENERAL NOTES

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

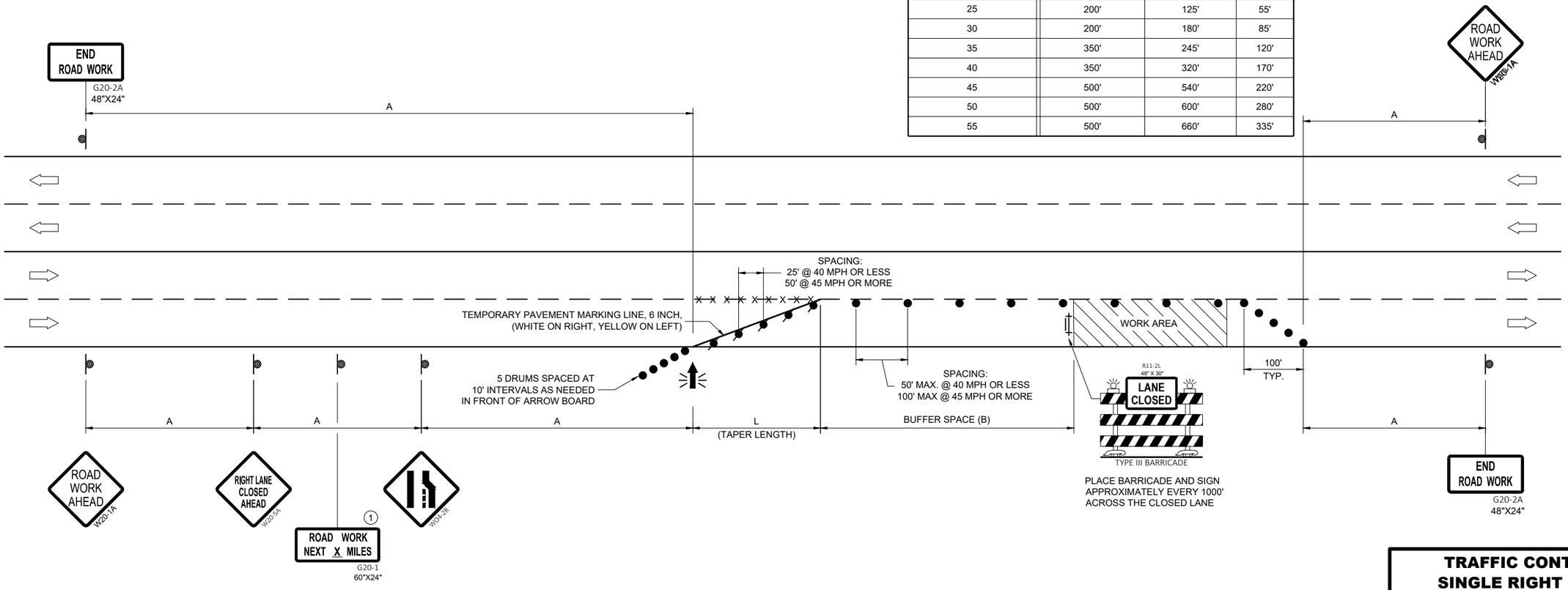
BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

(1) OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'
50	500'	600'	280'
55	500'	660'	335'



TRAFFIC CONTROL, SINGLE RIGHT LANE CLOSURE, UNDIVIDED NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

March 2024
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

6

SDD 15D20-08b

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKINGS
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- WORK AREA

GENERAL NOTES

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

DUE TO LACK OF SHOULDER/MEDIAN, ARROW BOARD IS PLACED AT THE THE END OF THE TAPER.

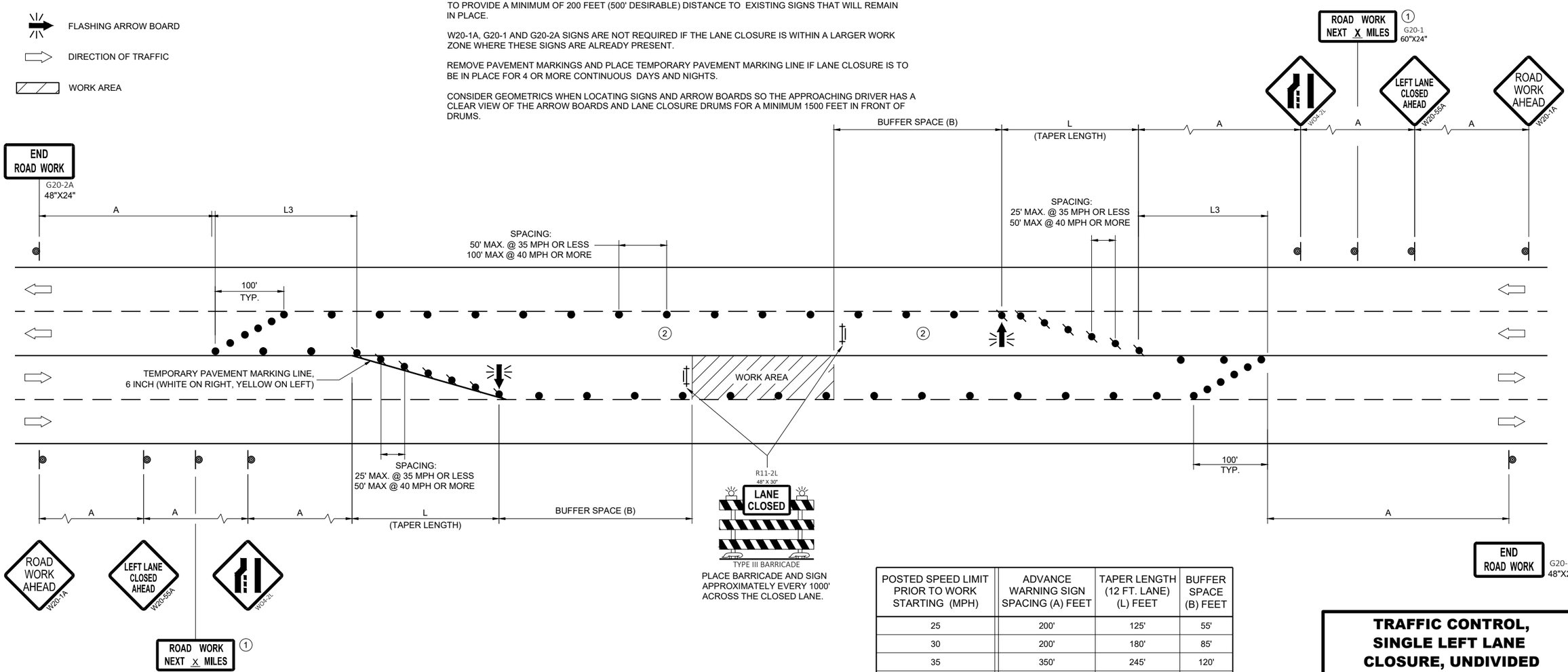
BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

(1) OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

(2) LANE MAY BE OPENED WHEN WORKERS ARE NOT PRESENT IN THE WORK AREA.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'
50	500'	600'	280'
55	500'	660'	335'

TRAFFIC CONTROL, SINGLE LEFT LANE CLOSURE, UNDIVIDED NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

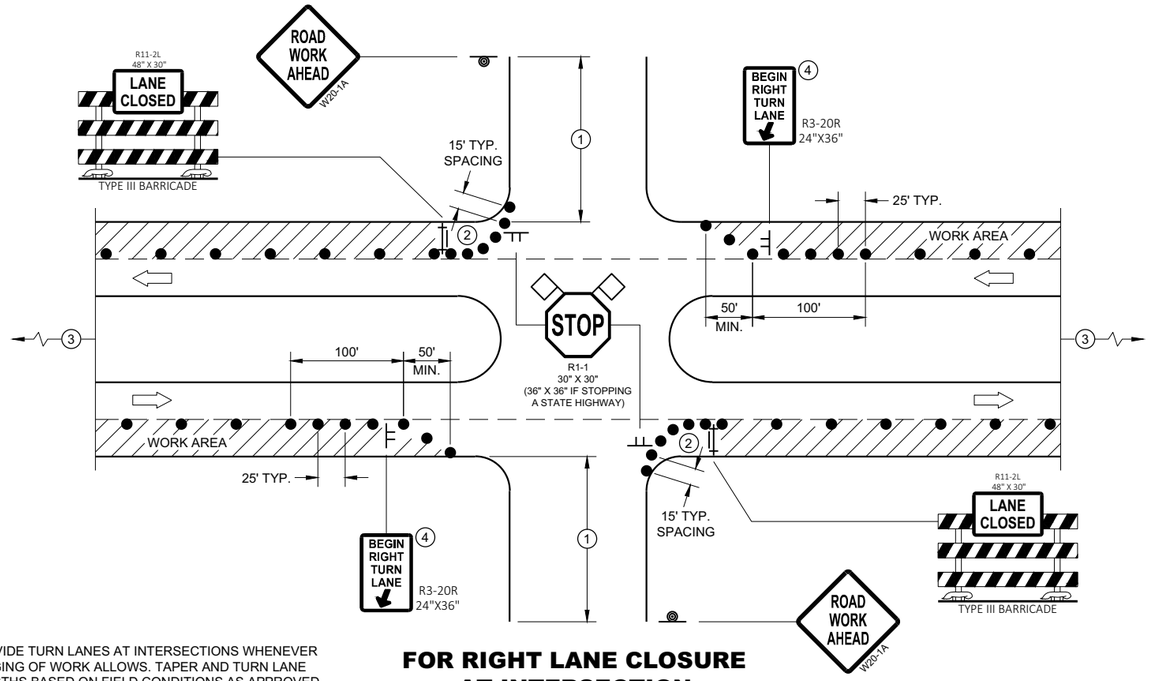
March 2024
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

6

SDD 15D20-08c
SDD 15D20-08c



PROVIDE TURN LANES AT INTERSECTIONS WHENEVER STAGING OF WORK ALLOWS. TAPER AND TURN LANE LENGTHS BASED ON FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

FOR RIGHT LANE CLOSURE AT INTERSECTION

FOR RIGHT LANE CLOSURE AT INTERSECTION (WITH RIGHT TURN BAY OPEN)

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" MAY BE USED IF APPROVED BY THE DISTRICT TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL REMAIN IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON PORTABLE SUPPORTS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

(1) 500' TYPICAL OR AT LAST INTERSECTION, WHICHEVER IS CLOSER.
350' IF 35 - 40 MPH.
200' IF 25 - 30 MPH.

(2) ALSO USE BARRICADE AND 15 FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS

(3) SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.

(4) MINIMUM MOUNTING HEIGHT OF 5 FEET FROM EDGE OF PAVEMENT (AT EDGE LINE LOCATION) TO BOTTOM OF SIGN.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- DIRECTION OF TRAFFIC
- FLAGS, 16" X 16" MIN., ORANGE
- WORK AREA

TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE RIGHT LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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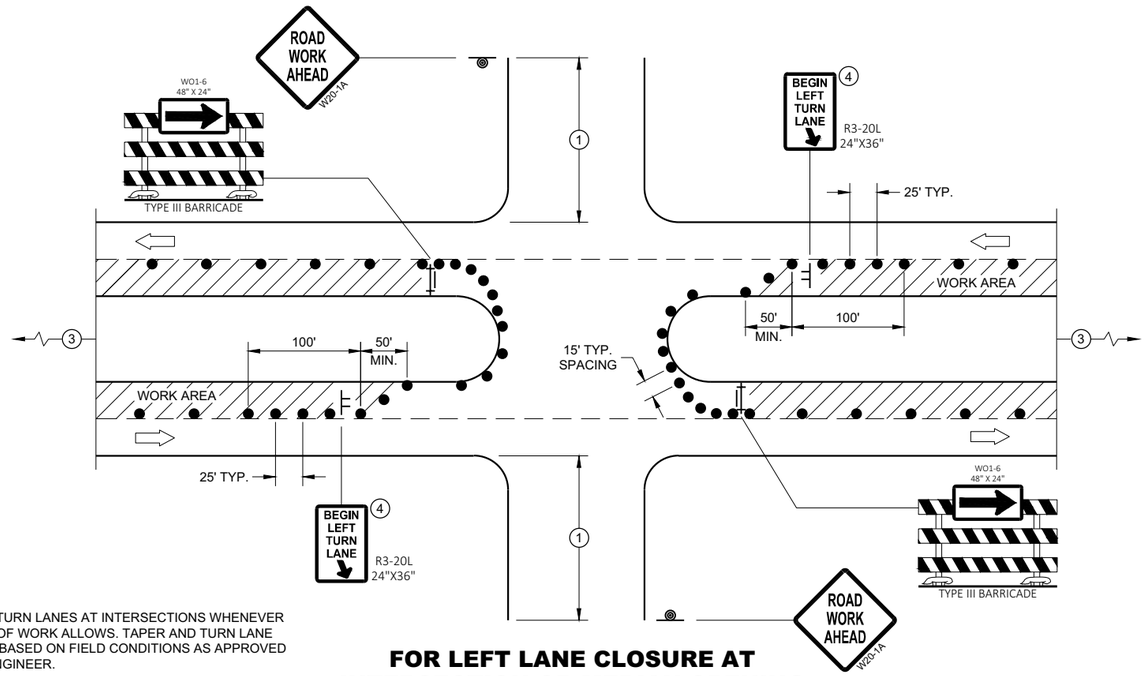
SDD15D21 - 07a



PROVIDE TURN LANES AT INTERSECTIONS WHENEVER STAGING OF WORK ALLOWS. TAPER AND TURN LANE LENGTHS BASED ON FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

FOR LEFT LANE CLOSURE AT INTERSECTION OR MEDIAN OPENING

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" MAY BE USED IF APPROVED BY THE DISTRICT TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL REMAIN IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON PORTABLE SUPPORTS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

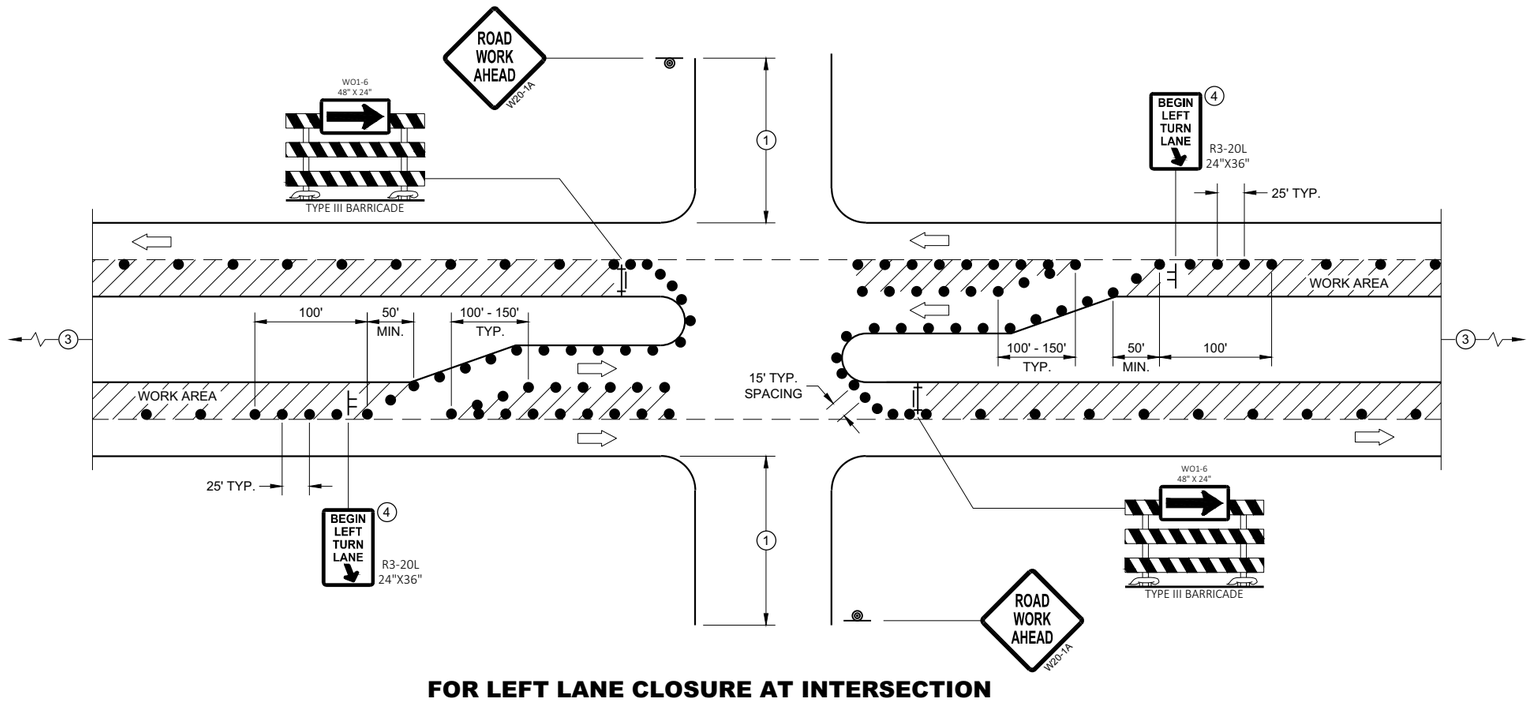
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

(1) 500' TYPICAL OR AT LAST INTERSECTION, WHICHEVER IS CLOSER.
350' IF 35 - 40 MPH.
200' IF 25 - 30 MPH.

(2) ALSO USE BARRICADE AND 15 FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS

(3) SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.

(4) MINIMUM MOUNTING HEIGHT OF 5 FEET FROM EDGE OF PAVEMENT (AT EDGE LINE LOCATION) TO BOTTOM OF SIGN.



FOR LEFT LANE CLOSURE AT INTERSECTION OR MEDIAN OPENING (WITH LEFT TURN BAY OPEN)

LEGEND

- SIGN ON TEMPORARY SUPPORT
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- DIRECTION OF TRAFFIC
- FLAGS, 16" X 16" MIN., ORANGE
- WORK AREA

TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LEFT LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
August 2020
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

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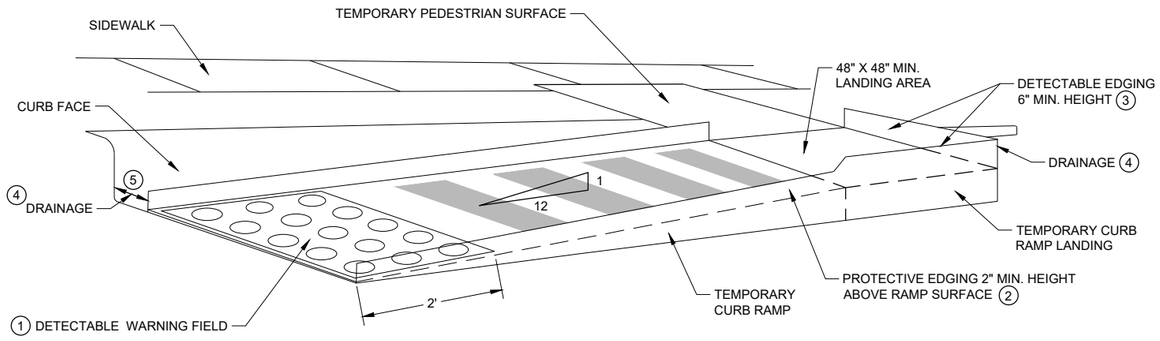
SDD 15D21 - 07b
SDD 15D21 - 07b



PERSPECTIVE VIEW

GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP. LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN ½" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED ½". LATERAL EDGES MAY BE VERTICAL UP TO ¼" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN ¼" AND ½".

(1) INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.

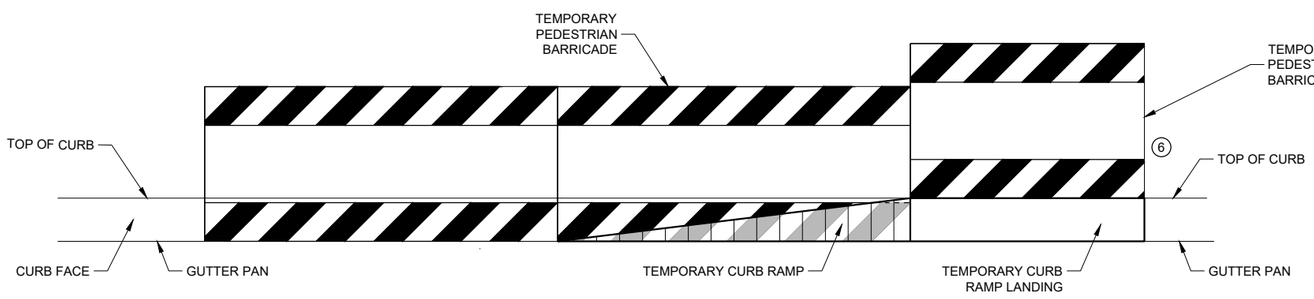
(2) PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.

(3) DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).

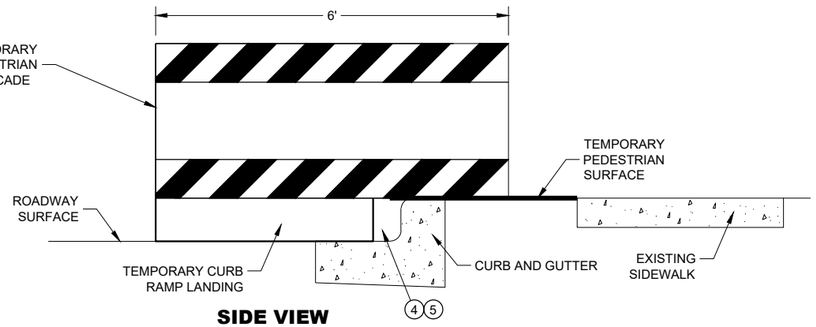
(4) DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.

(5) ENSURE CURB RAMP IS OUT OF THE GUTTER PAN.

(6) IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



FRONT VIEW



SIDE VIEW

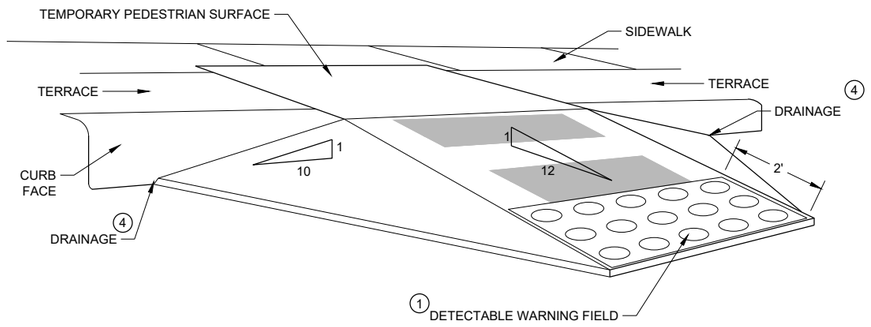
TEMPORARY CURB RAMP PARALLEL TO CURB

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

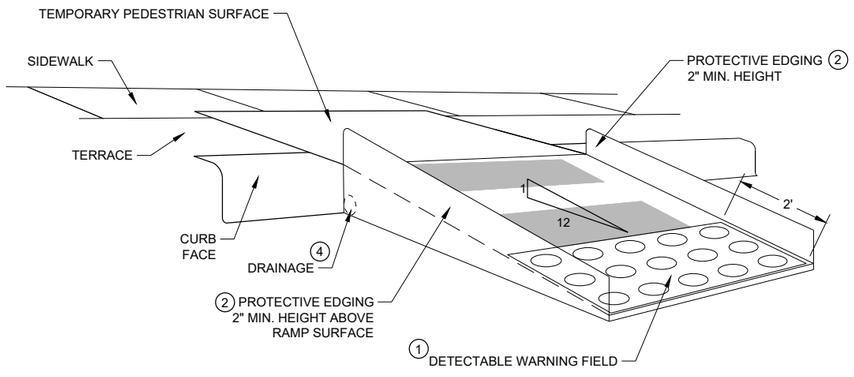
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 15D30 - 09b



WITH SIDE APRON (5)



WITH PROTECTIVE EDGE

TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN ½" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED ½". LATERAL EDGES MAY BE VERTICAL UP TO ¼" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN ¼" AND ½".

(1) INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS

(2) PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.

(3) DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).

(4) DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.

(5) CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

6

SDD 15D30 - 09c

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

GENERAL NOTES

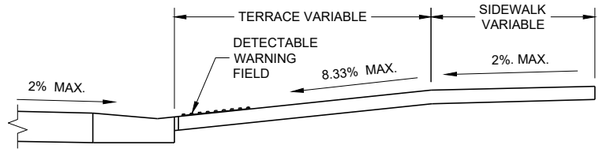
BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST

(1) REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.

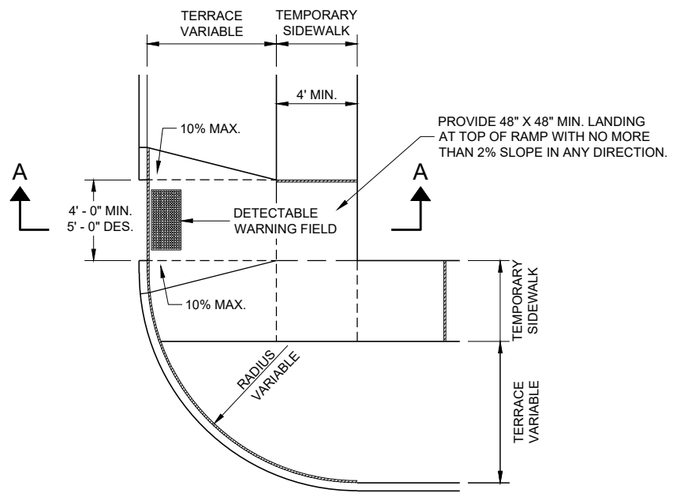
(2) SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.

(3) PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.

* USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



SECTION A - A



PLAN VIEW

TEMPORARY TYPE 3 RAMP

(OUTSIDE OF CROSSWALK AREA)

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

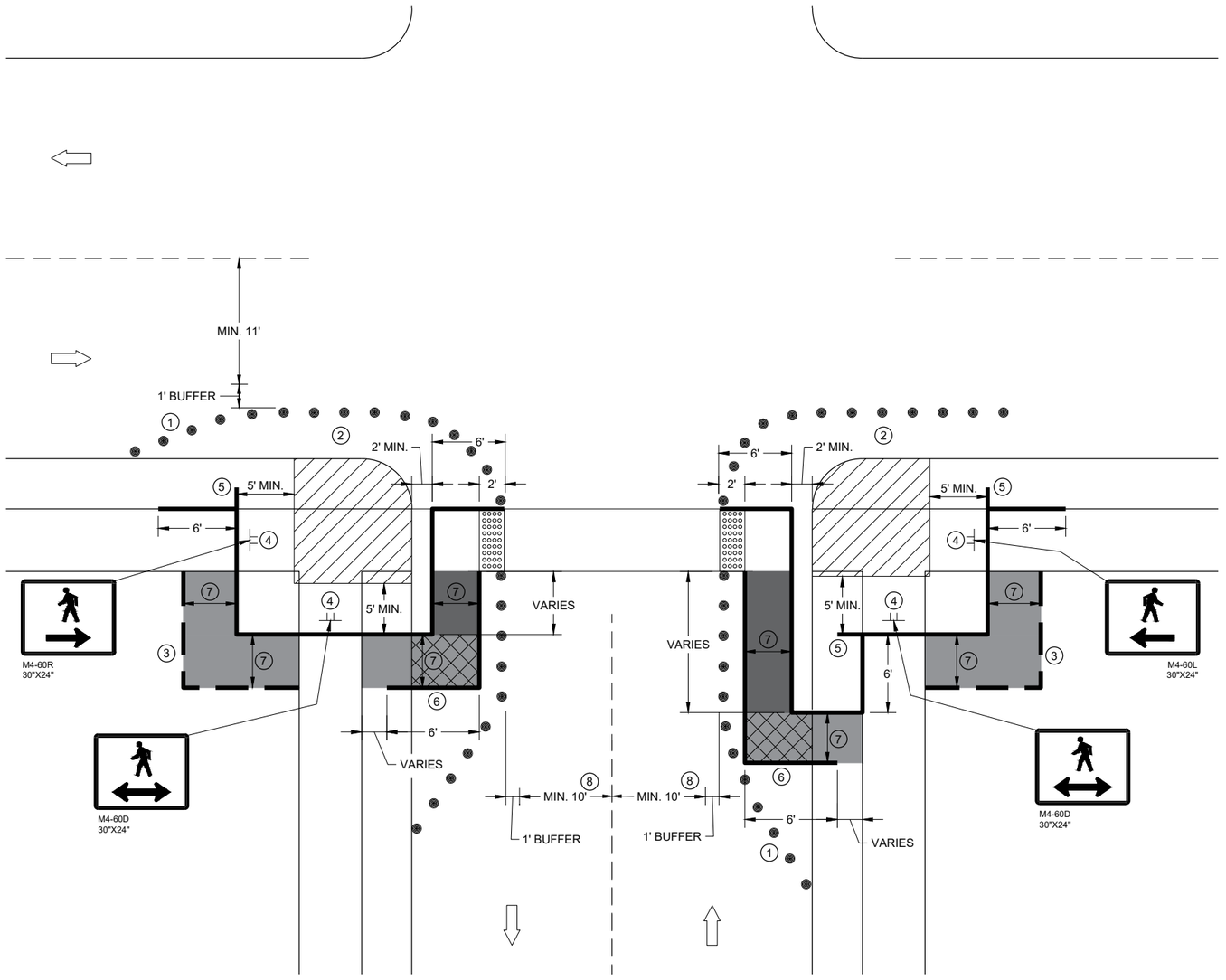
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6
SDD 15D30-09d



CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

(1) SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.

(2) PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS

(3) USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.

(4) MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.

(5) PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.

(6) IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.

(7) 4 FEET MINIMUM, 5 FEET DESIRABLE

(8) IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION

SDD 15D30 - 09i

6

LEGEND

SIGN ON PERMANENT SUPPORT

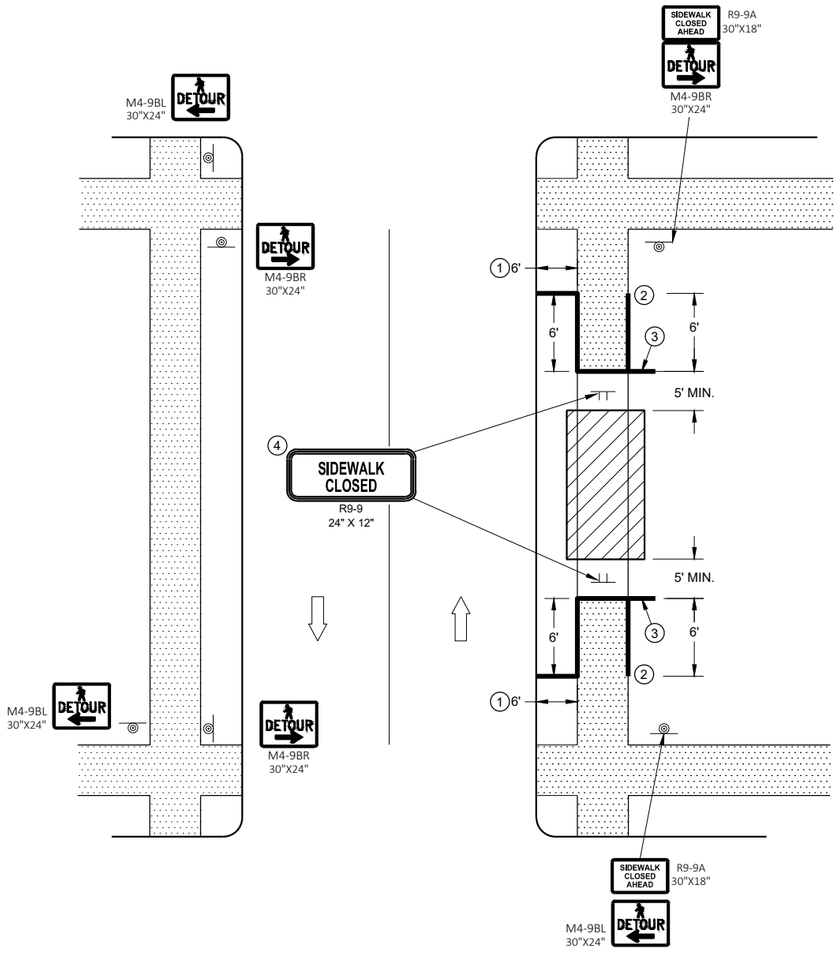
SIGN ON TEMPORARY SUPPORT

UNDER PEDESTRIAN TRAFFIC

WORK AREA

TEMPORARY PEDESTRIAN BARRICADE

DIRECTION OF TRAFFIC



GENERAL NOTES

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICT WITH DRIVEWAYS AND OTHER EXISTING FEATURES.

1. IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
2. PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
3. IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
4. MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.

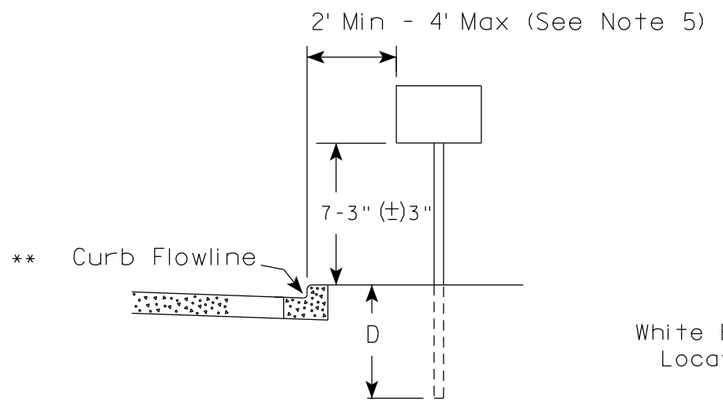
SIDEWALK DETOUR, SIDEWALK ON BOTH SIDES

6

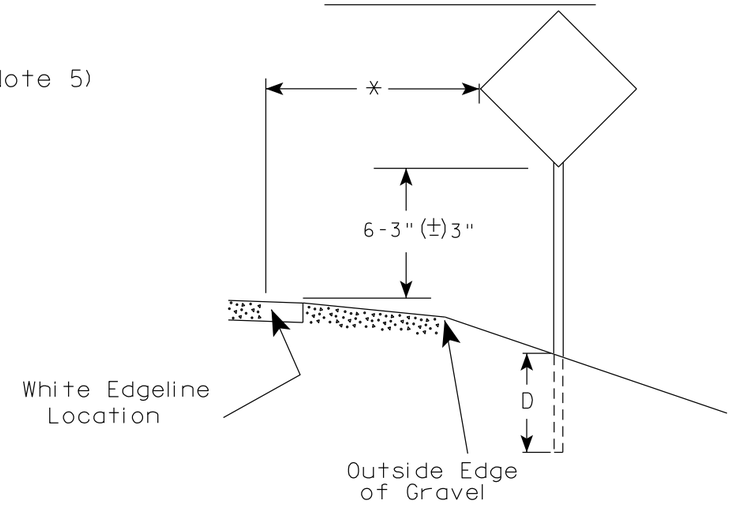
SDD 15D30 - 09k

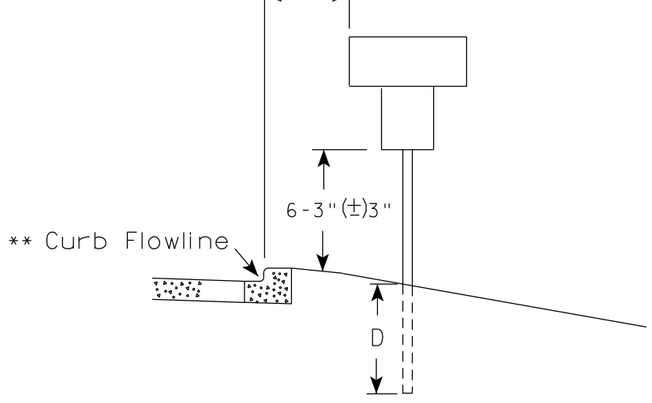
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

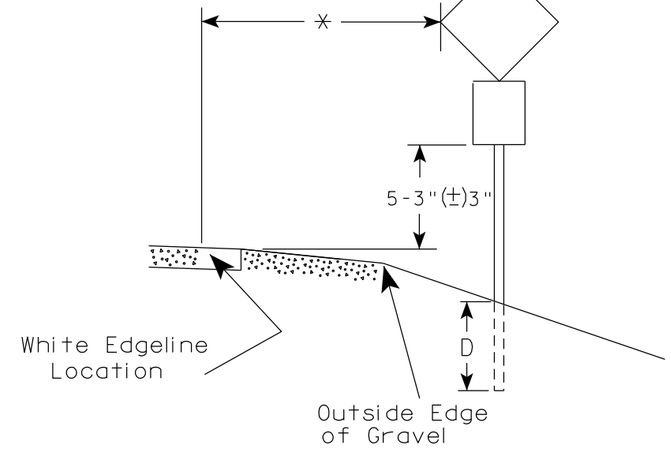
URBAN AREA



RURAL AREA (See Note 2)







** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
   The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".
3. For expressways and freeways, mounting height is 7'- 3" (±) 3" or 6'-3" (±) 3" depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±) 3".
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directd by the Engineer.

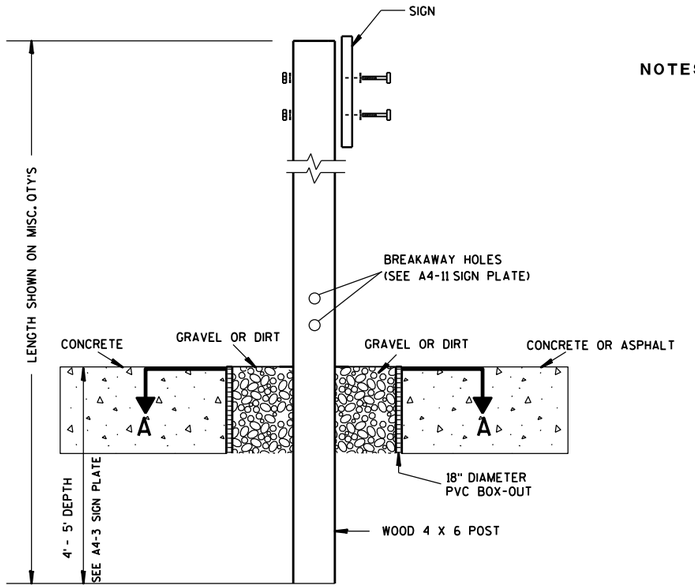
TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-3.23

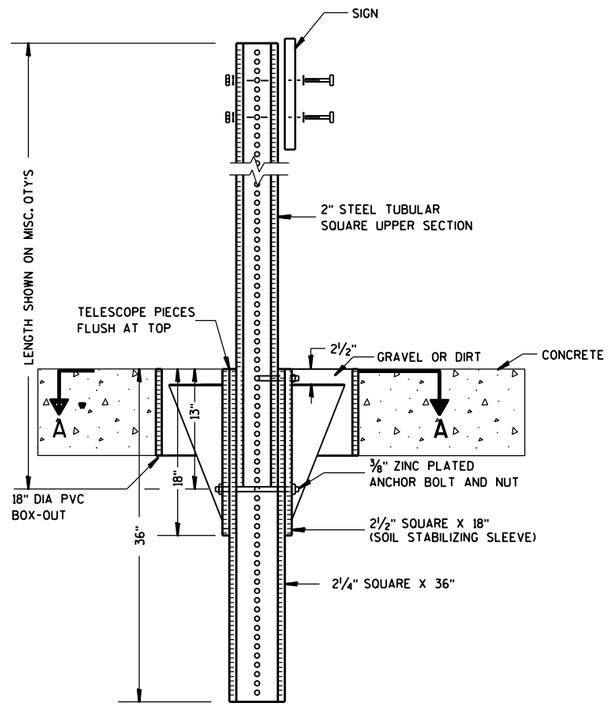
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 83	E



ELEVATION VIEW

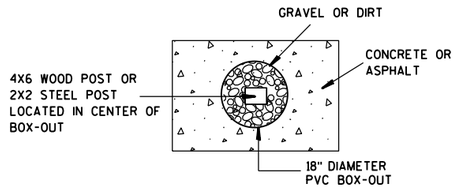
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

NOTES:
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

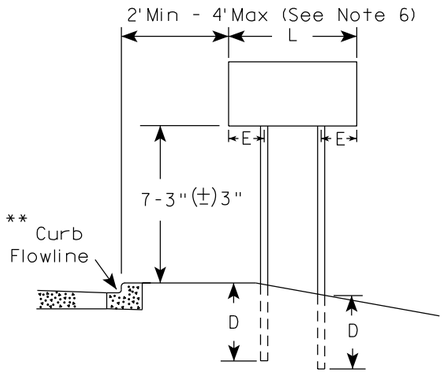
APPROVED Matthew R Rauch
for State Traffic Engineer

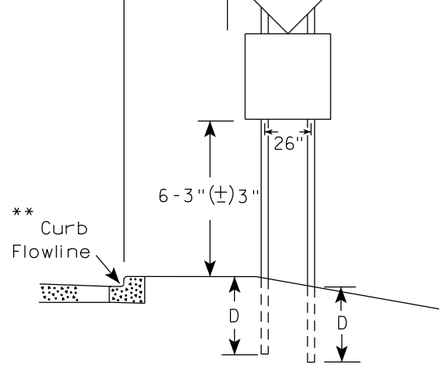
DATE 1/27/14 PLATE NO. A4-3B.1

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN PLOT DATE : 27-JAN-2014 09:48 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42

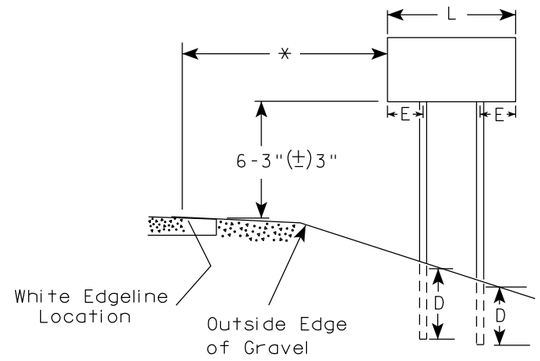
URBAN AREA

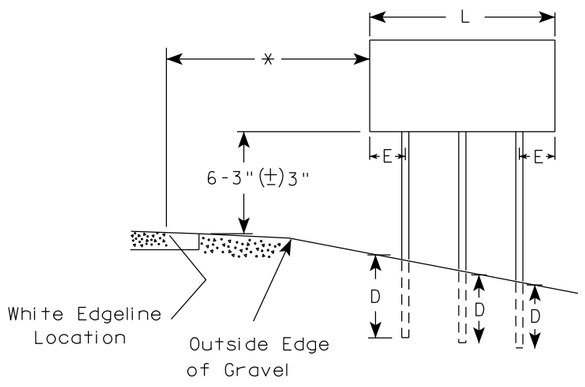


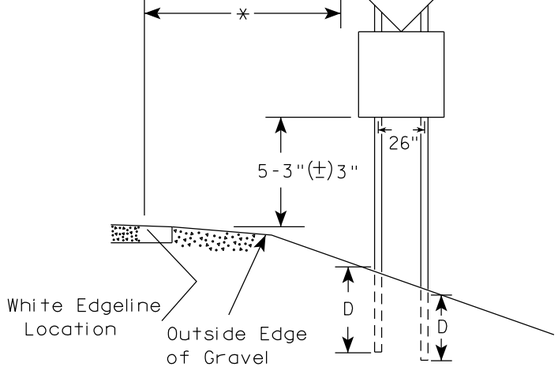


48" DIAMOND WARNING SIGN

RURAL AREA (See Note 3)







48" DIAMOND WARNING SIGN

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)		
	L	E
***	Greater than 48" Less than 60"	12"
	60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) 3" or 6'-3" (±) 3" depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±) 3".

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

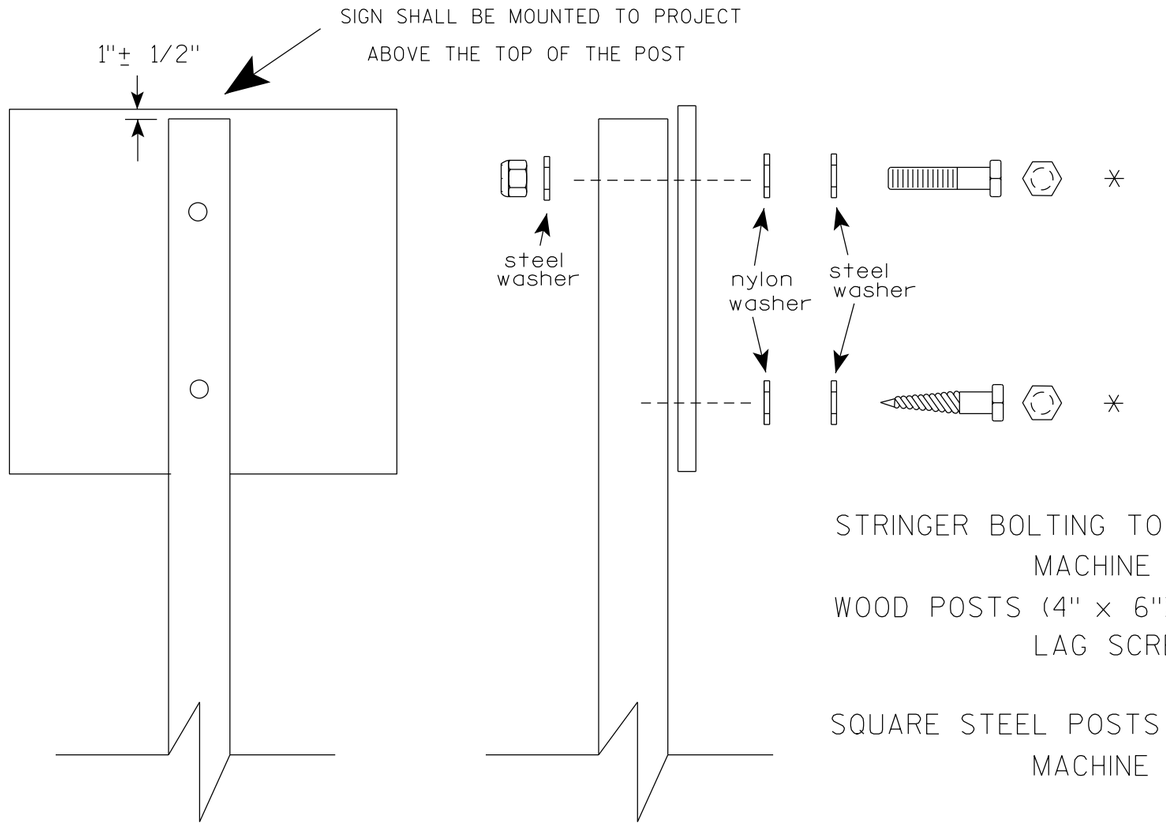
TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-4.16

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 85	E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3

b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

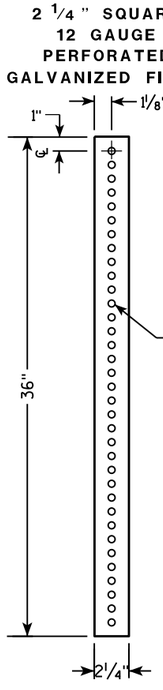
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

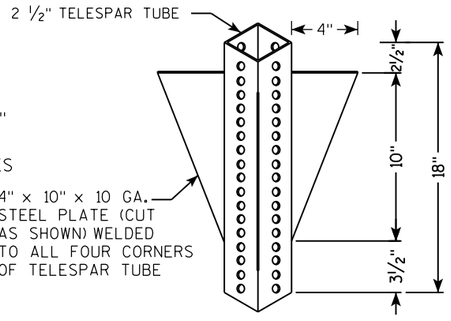
DATE 4/1/2020 PLATE NO. A4-8.9

TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM

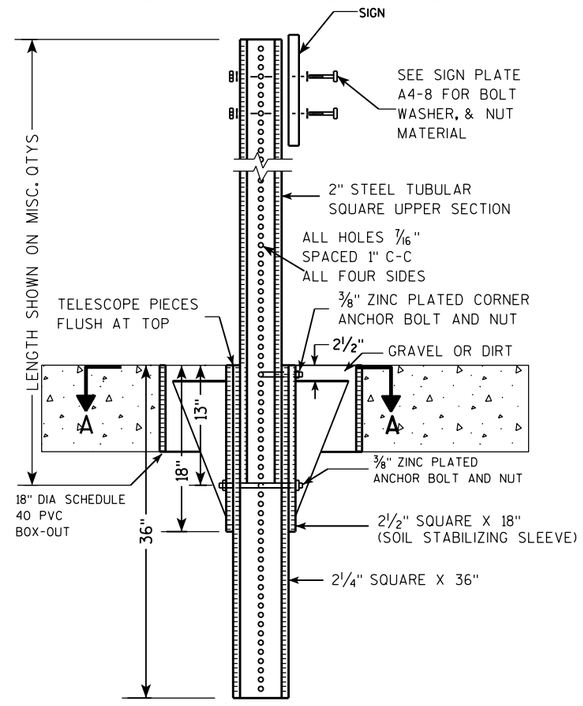
2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



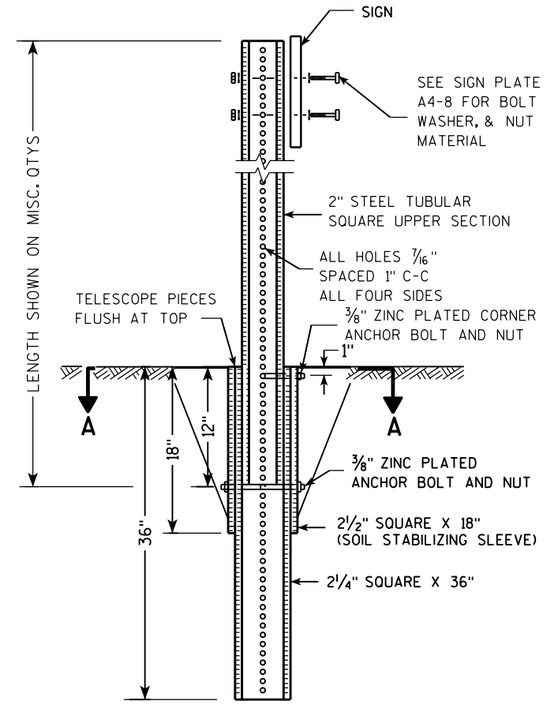
2 1/2 " SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH

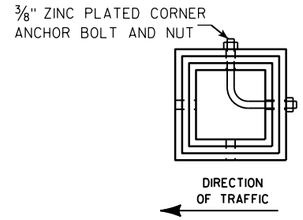


DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)





SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

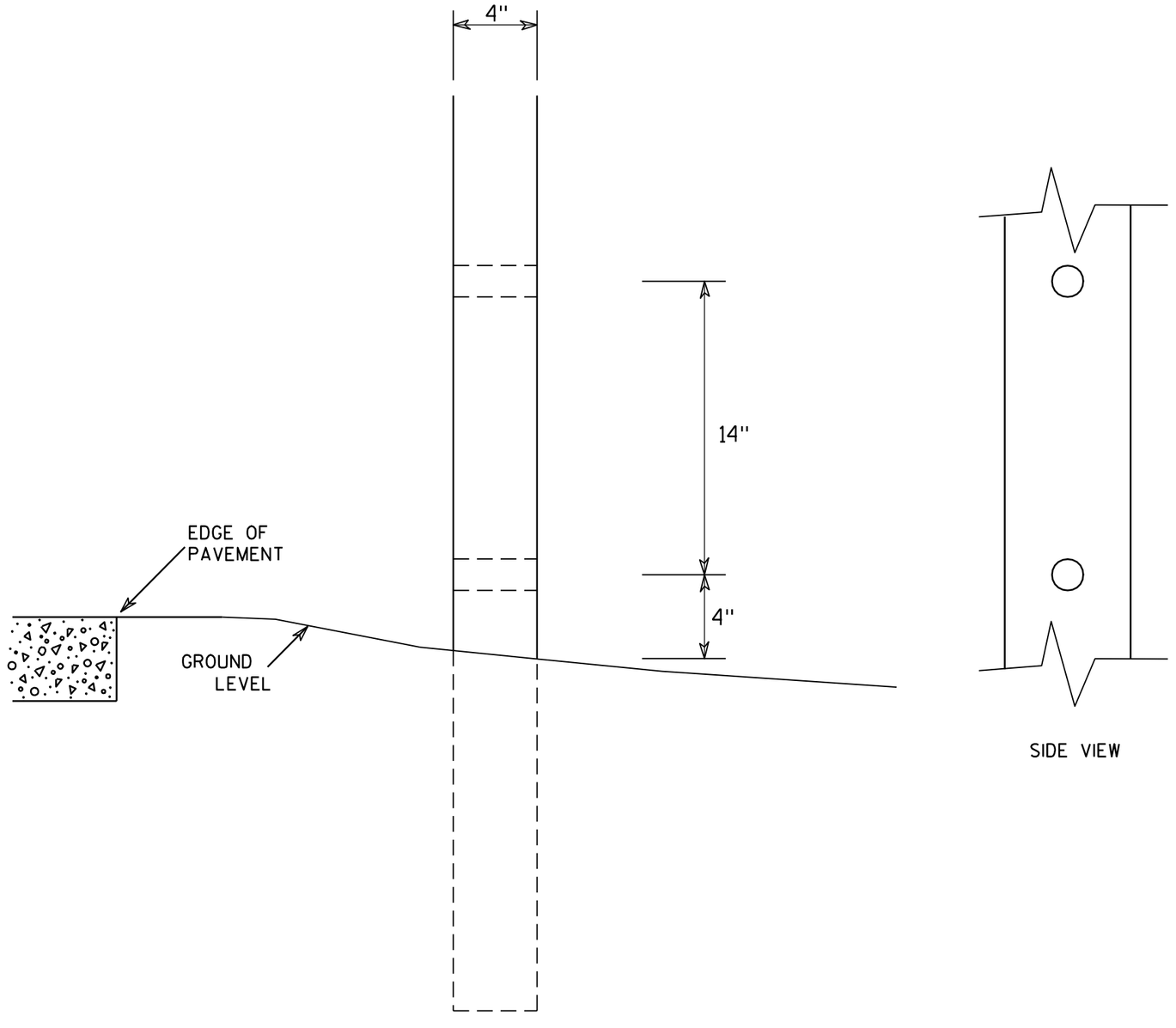
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/05/15 PLATE 87 A4-9.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE : 05-FEB-2015 17:09 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000

WISDOT/CADDS SHEET 42



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

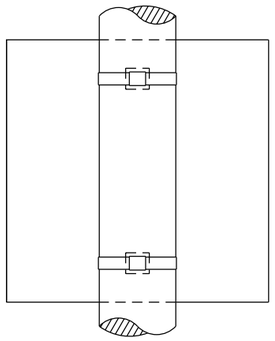
APPROVED Chester J Spang for State Traffic Engineer

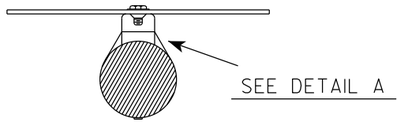
DATE 3/27/97 PLATE NO. A4-11.2

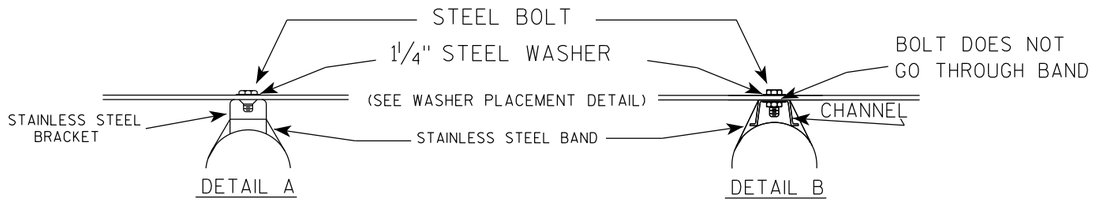
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 88	E

BANDING

SINGLE SIGN



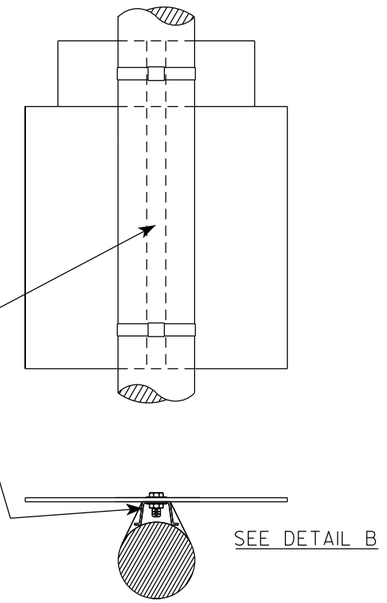




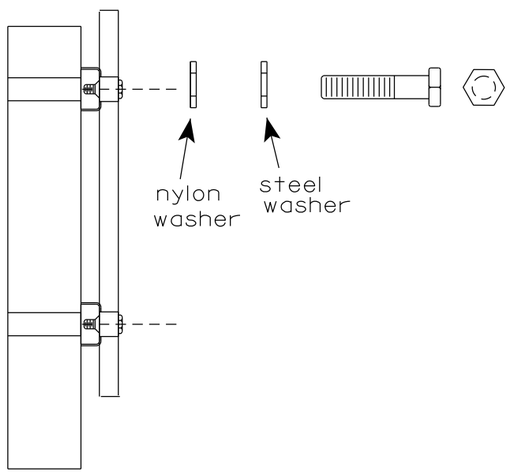
GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be ¾" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



WASHER PLACEMENT



WASHERS (ALL POSTS) -
1-1/4" O.D. X ⅜" I.D. X 1/16" STEEL
1-1/4" O.D. X ⅜" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

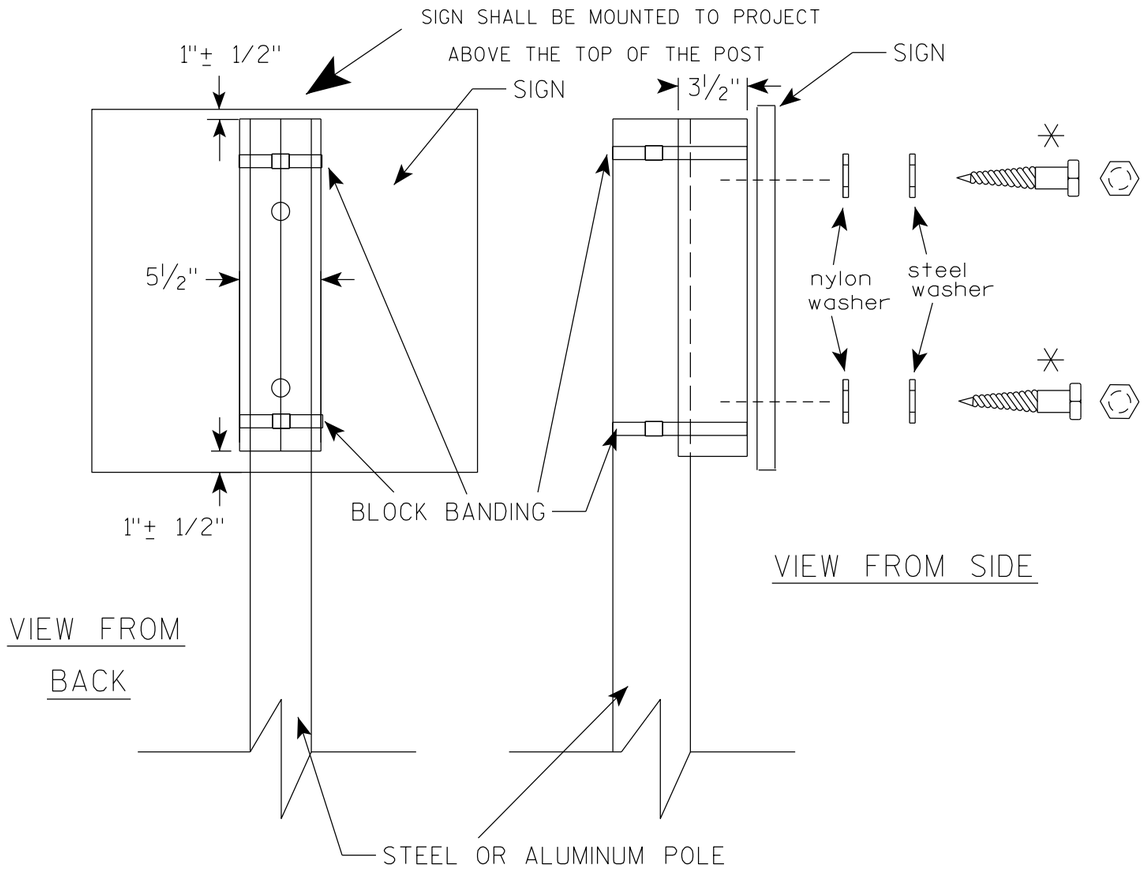
DATE 6/10/19 PLATE NO. A5-9.4

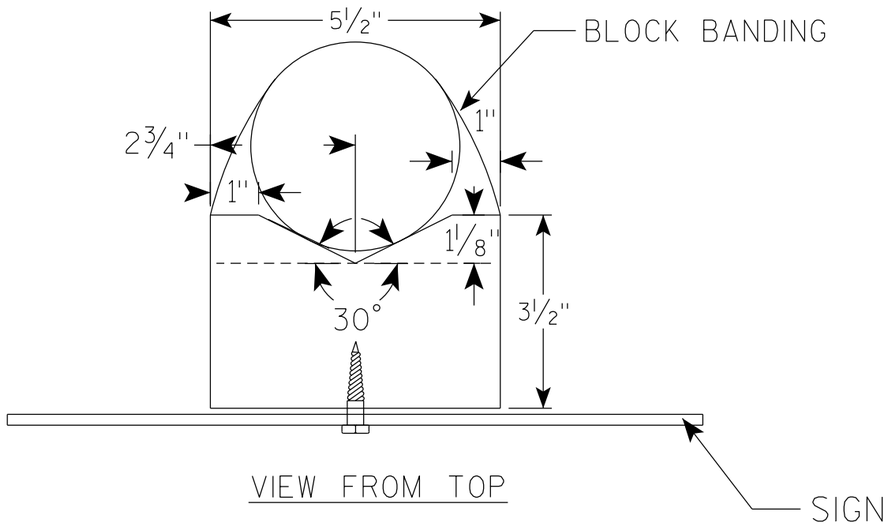
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 89	E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A59.dgn PLOT DATE : 10-JUN 2019 4:10 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : $$.....plotscale.....$$ WISDOT/CADDS SHEET 42

7

7





GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WisDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, ¾" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORNALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✳ LAG BOLTS SHALL BE ⅜" X 2½"

BLOCK BANDING DETAIL (V-BLOCK OPTION)

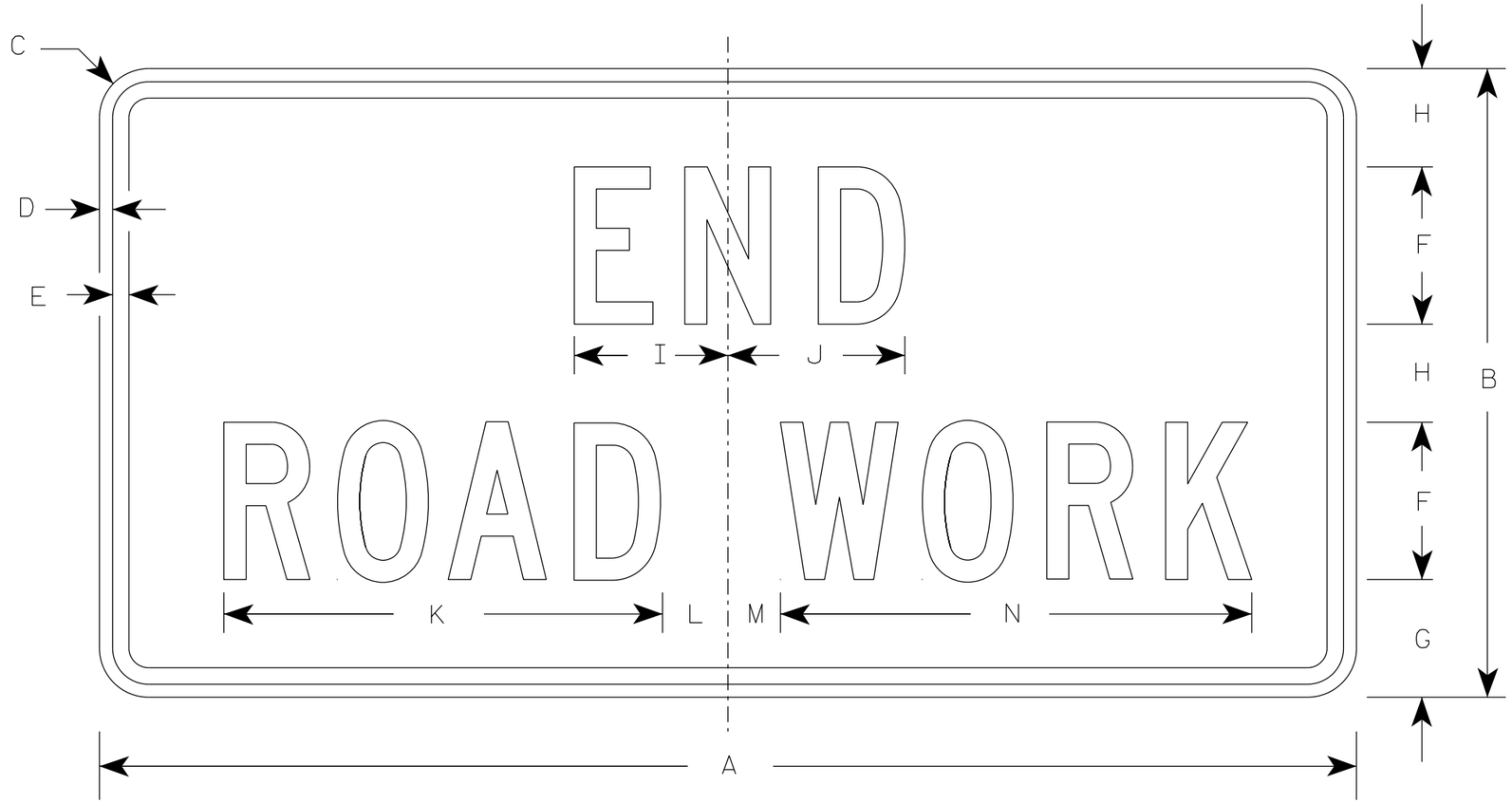
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 4/19/2022 PLATE NO. A5-10.3

PROJECT NO:

SHEET NO: 90 E



G20-2A

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/2	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5
2	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0
2M	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0
3	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0
4	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0
5	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0

STANDARD SIGN

G20-2A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 1/26/2023 PLATE NO. G20-2A.10

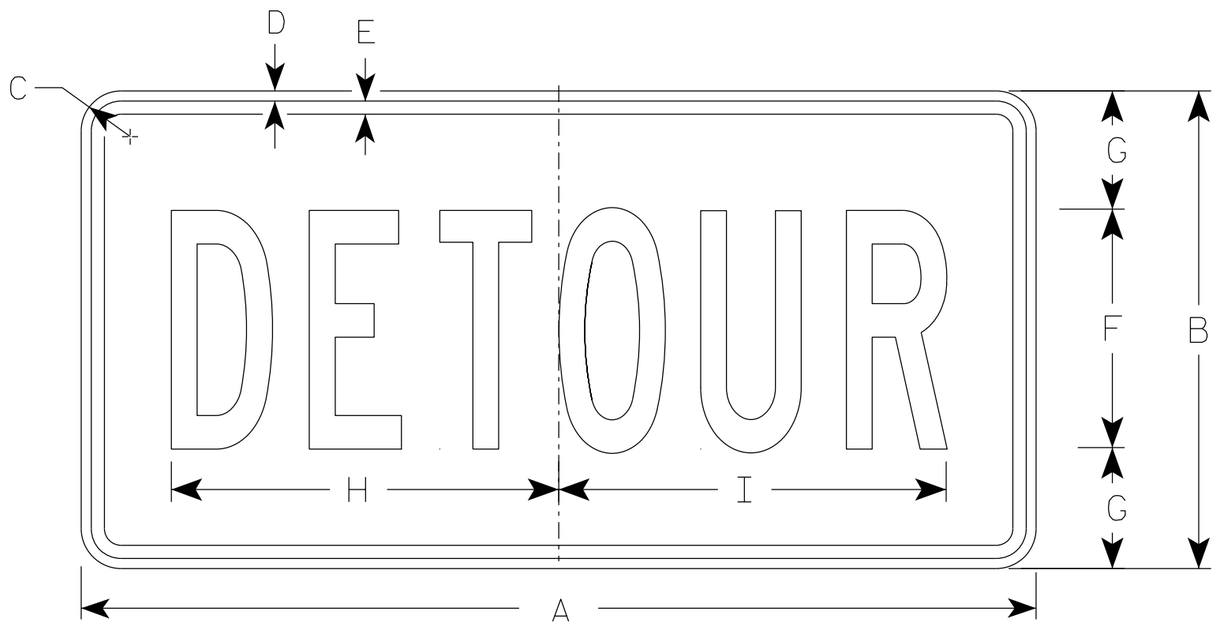
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 91 E

7

7

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



M4-8

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/2	3/8	3/8	6	3	10	10 1/4																		2.0
2M	24	12	1 1/2	3/8	3/8	6	3	10	10 1/4																		2.0
3	36	18	1 1/2	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
4	36	18	1 1/2	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
5	36	18	1 1/2	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5

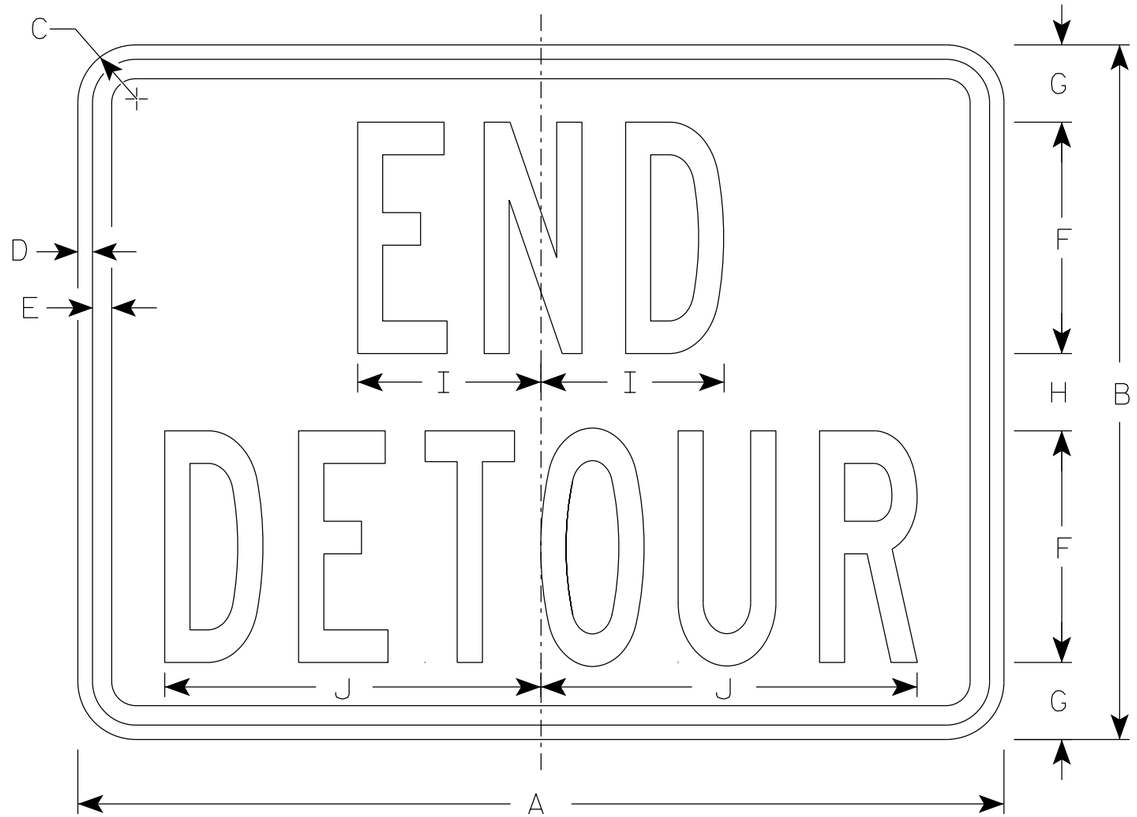
STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/9/2023 PLATE NO. M4-8.4

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 92	E



M4-8A

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 ½	⅜	½	6	2	2	4 ¾	9 ¾																	3.0
2M	24	18	1 ½	⅜	½	6	2	2	4 ¾	9 ¾																	3.0
3	30	24	1 ½	⅜	½	8	2 ½	3	6 ¾	13																	5.0
4	30	24	1 ½	⅜	½	8	2 ½	3	6 ¾	13																	5.0
5	30	24	1 ½	⅜	½	8	2 ½	3	6 ¾	13																	5.0

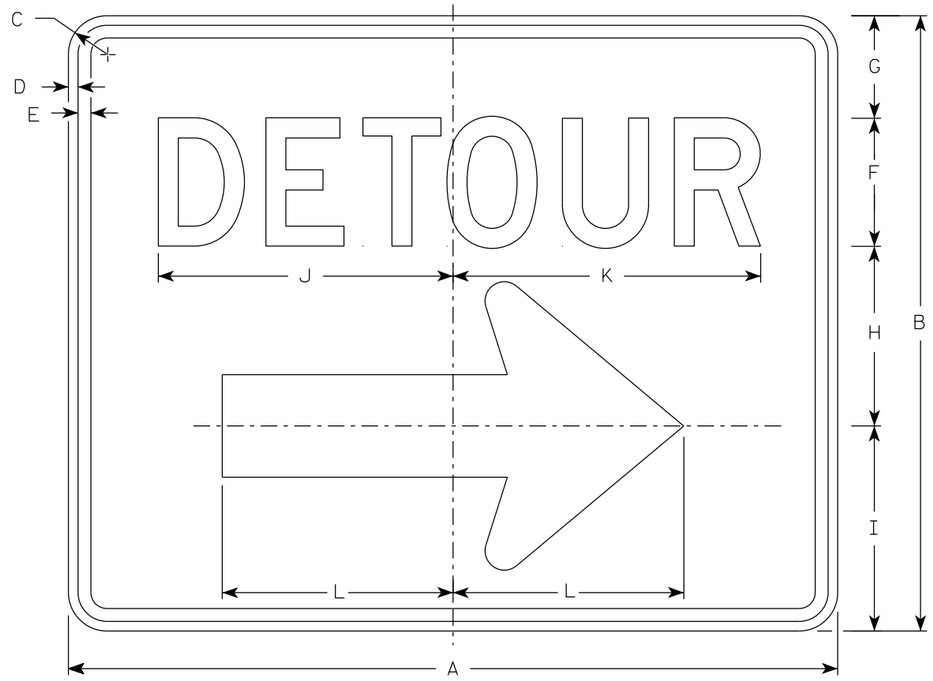
STANDARD SIGN
M4-8A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/9/2023 PLATE NO. M4-8A.4

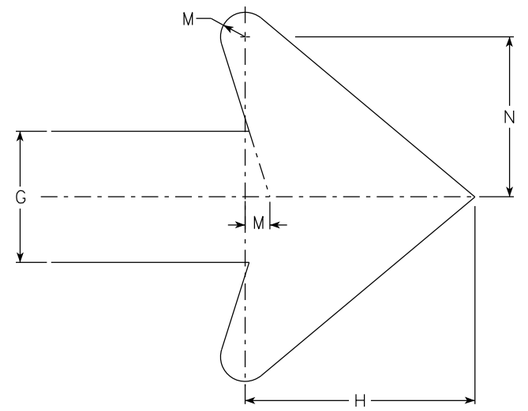
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 93 | E



M4-9R

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M4-9L is the same as M4-9R except the arrow is reversed.



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/2	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
2M	30	24	1 1/2	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
3	30	24	1 1/2	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
4	48	36	1 7/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0
5	48	36	1 7/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0

STANDARD SIGN
M4-9 R & L

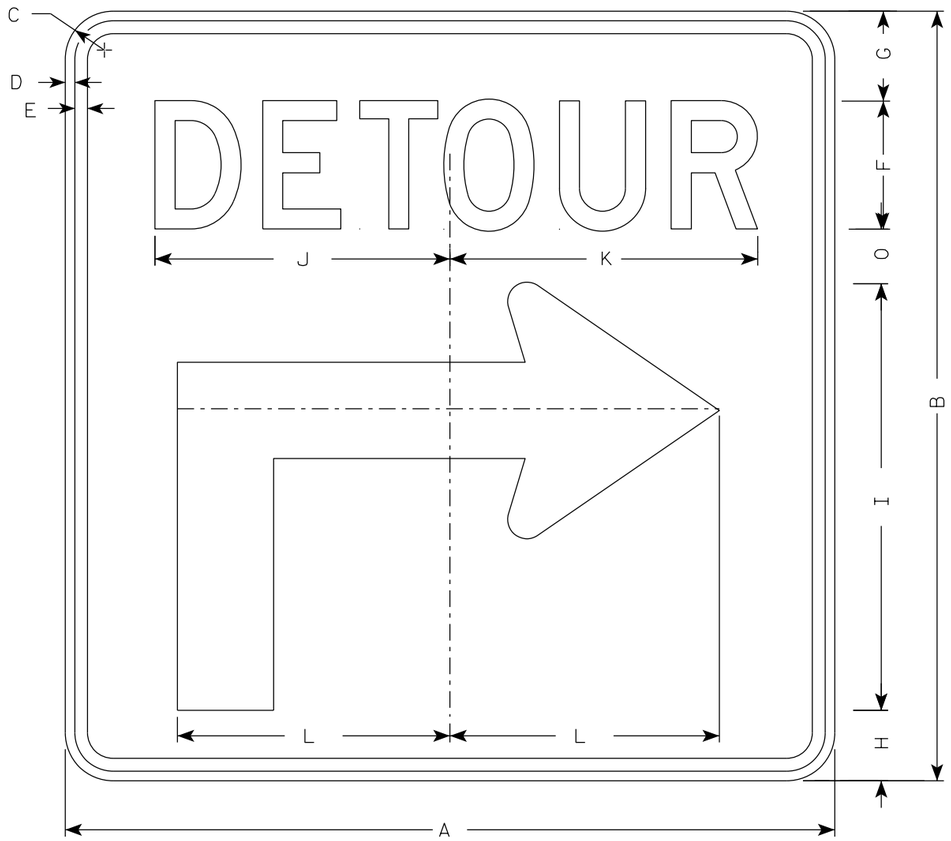
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 2/9/2023 PLATE NO. M4-9R.6

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

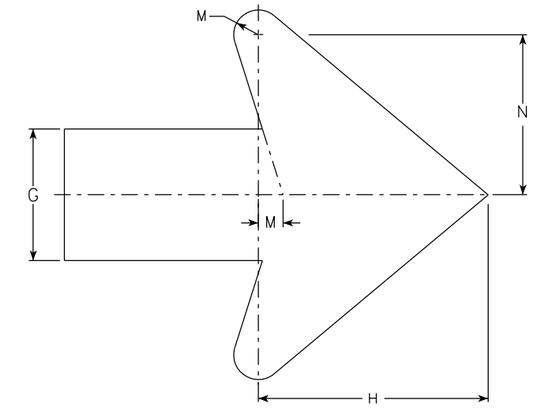
FILE NAME : C:\CAEfiles\Projects\tr_stdplate\M49R.dgn PLOT DATE : 21-MARCH 2023 1:40 PLOT BY : dotc4c PLOT NAME : PLOT SCALE : $$.....plotscale.....$$ WISDOT/CADDS SHEET 42



M4-59R

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown when base material is metal.
5. M4-59L is the same as M4-59R except the arrow is reversed.



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	30	1 7/8	3/8	1/2	5	3 1/2	2 3/4	16 5/8	11 1/2	12	10 1/2	3/4	4 7/8	2 1/8												6.25
2M	30	30	1 7/8	3/8	1/2	5	3 1/2	2 3/4	16 5/8	11 1/2	12	10 1/2	3/4	4 7/8	2 1/8												6.25
3	30	30	1 7/8	3/8	1/2	5	3 1/2	2 3/4	16 5/8	11 1/2	12	10 1/2	3/4	4 7/8	2 1/8												6.25
4	48	48	2 1/4	1/2	5/8	8	5 5/8	4 3/8	26 5/8	20 5/8	20 1/2	17	1 1/8	6 7/8	3 3/8												16.0
5	48	48	2 1/4	1/2	5/8	8	5 5/8	4 3/8	26 5/8	20 5/8	20 1/2	17	1 1/8	6 7/8	3 3/8												16.0

STANDARD SIGN
M4-59 L&R

WISCONSIN DEPT OF TRANSPORTATION

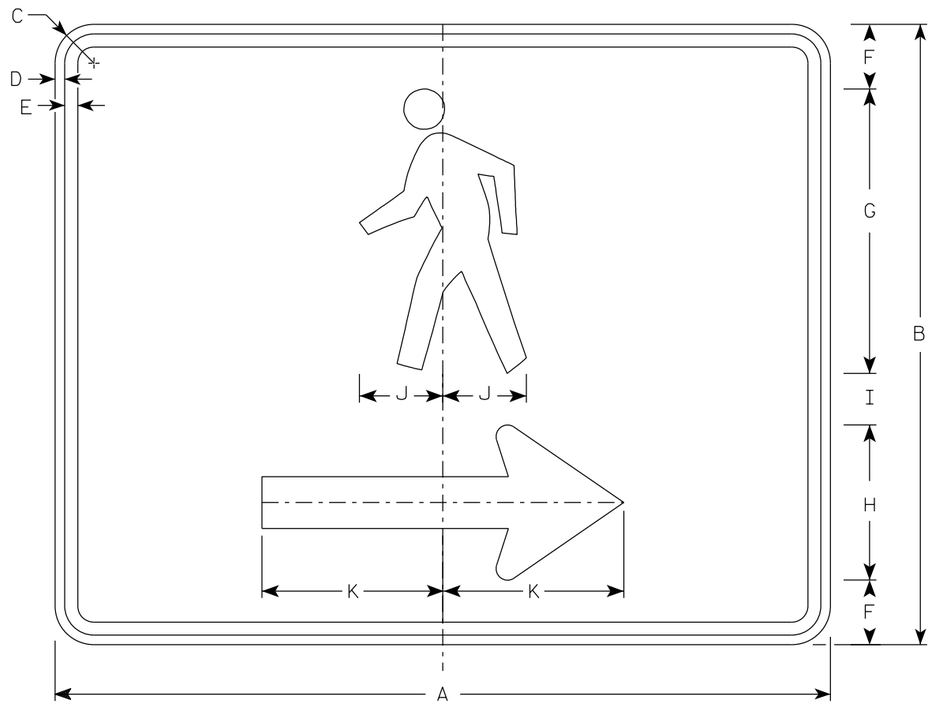
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/13/2023 PLATE NO. M4-59.2

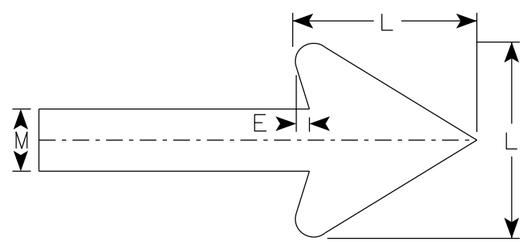
PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

NOTES

1. Sign is Type II- Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M4-60L is the same as M4-60R except the arrow is reversed.



M4-60R



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	24	1 ½	⅜	½	2 ½	11	6	2	3 ¼	7	6	2														5.00
2M	30	24	1 ½	⅜	½	2 ½	11	6	2	3 ¼	7	6	2														5.00
3																											
4																											
5																											

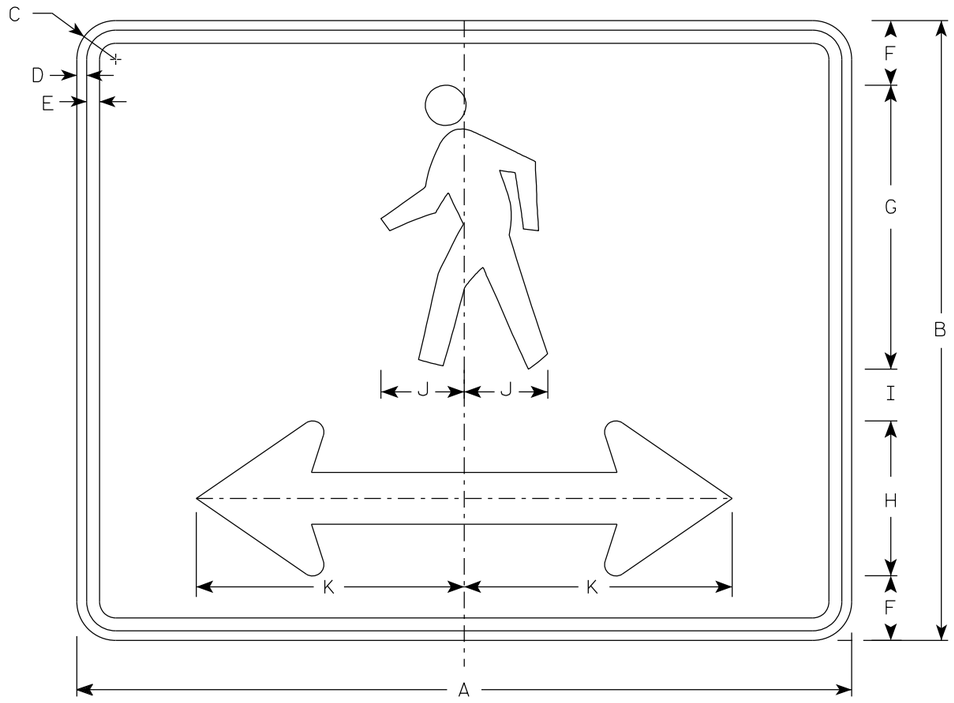
STANDARD SIGN
M4-60 L&R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 2/14/2023 PLATE NO. M4-60.2

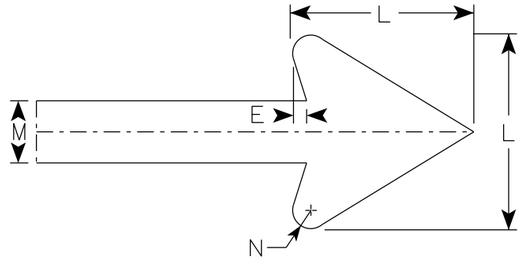
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 96 | E



M4-60D

NOTES

1. Sign is Type II- Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	24	1 1/2	3/8	1/2	2 1/2	11	6	2	3 1/4	10 3/8	6	2	3/8													5.00
2M	30	24	1 1/2	3/8	1/2	2 1/2	11	6	2	3 1/4	10 3/8	6	2	3/8													5.00
3																											
4																											
5																											

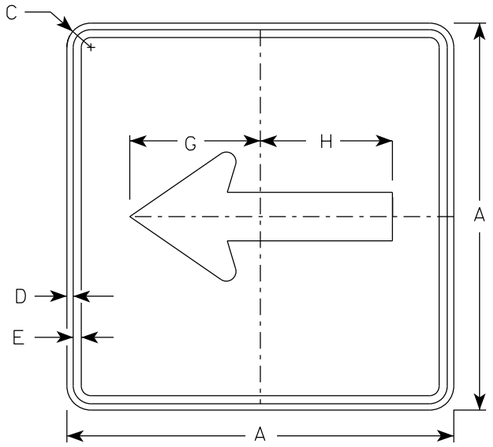
STANDARD SIGN
M4-60D

WISCONSIN DEPT OF TRANSPORTATION

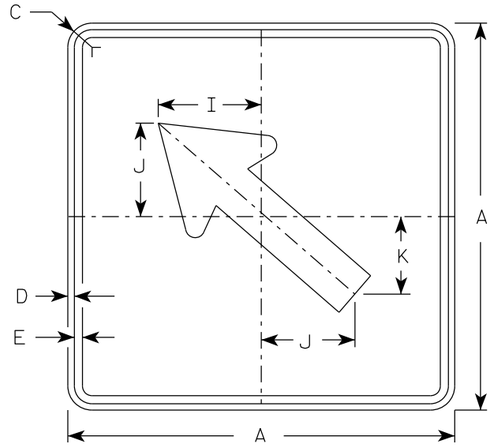
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/14/2023 PLATE NO. M4-60D.2

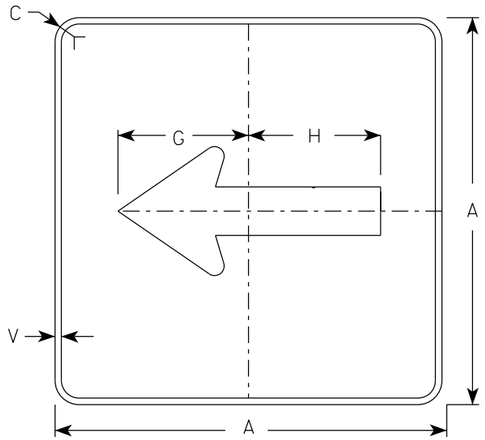
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 97 | E



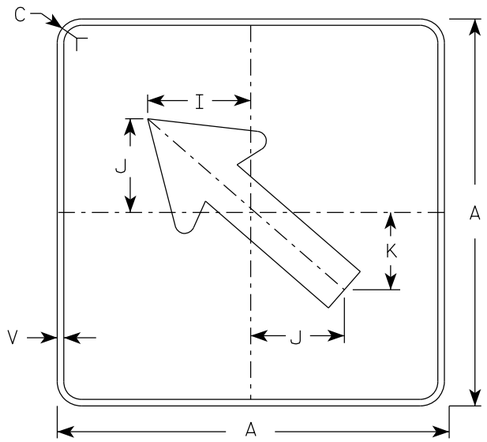
M6-1
MM6-1
MO6-1
MP6-1



M6-2
MM6-2
MO6-2
MP6-2



MB6-1
MK6-1
MN6-1
MR6-1

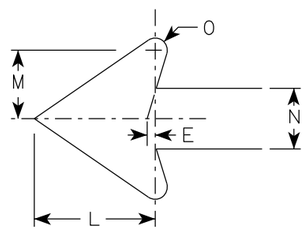


MB6-2
MK6-2
MN6-2
MR6-2

NOTES

1. Signs are Type II - Type H Reflective except as Shown
2. Color:
   Background - See note 4
   Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M6-1 and M6-2 Background - White
   Message - Black
   MB6-1 and MB6-2 Background - Blue
   Message - White
   MK6-1 and MK6-2 Background - Green
   Message - White
   MM6-1 and MM6-2 Background - White
   Message - Green
   MN6-1 and MN6-2 Background - Brown
   Message - White
   MO6-1 and MO6-2 Background - Orange - Type F Reflective
   Message - Black
   MP6-1 and MP6-2 Background - White
   Message - Blue
   MR6-1 and MR6-2 Background - Brown
   Message - Yellow

ARROW DETAIL



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	21		1 1/2	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2							1/2					3.06
2M	21		1 1/2	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2							1/2					3.06
3	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2					6.25
4	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2					6.25
5	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2					6.25

STANDARD SIGN
M6-1 & M6-2
SERIES

WISCONSIN DEPT OF TRANSPORTATION

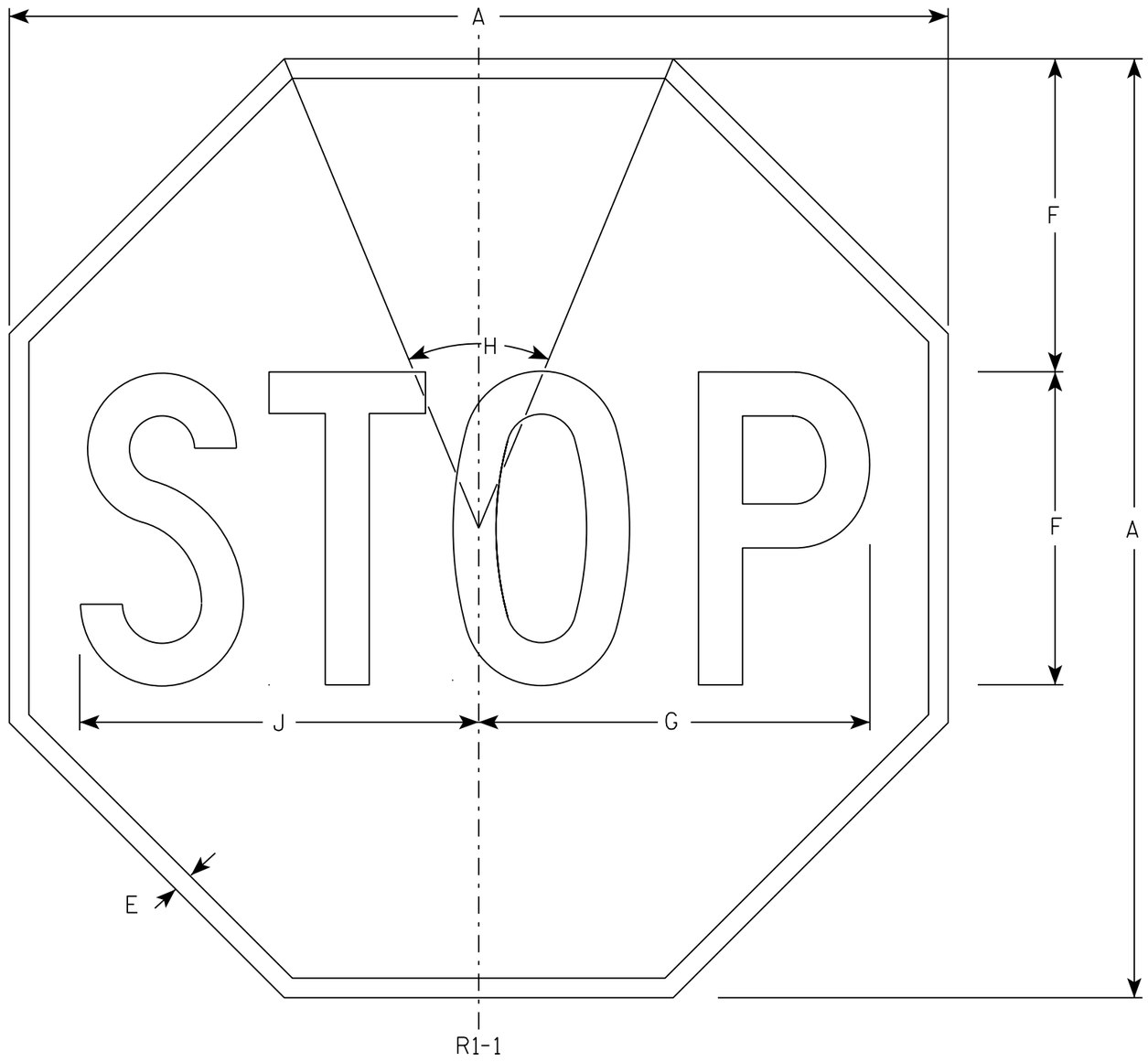
APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 2/13/2023 PLATE NO. M6-1.16

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 98 | E

7

7



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Red
   Message - White
3. Message Series - C

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

STANDARD SIGN

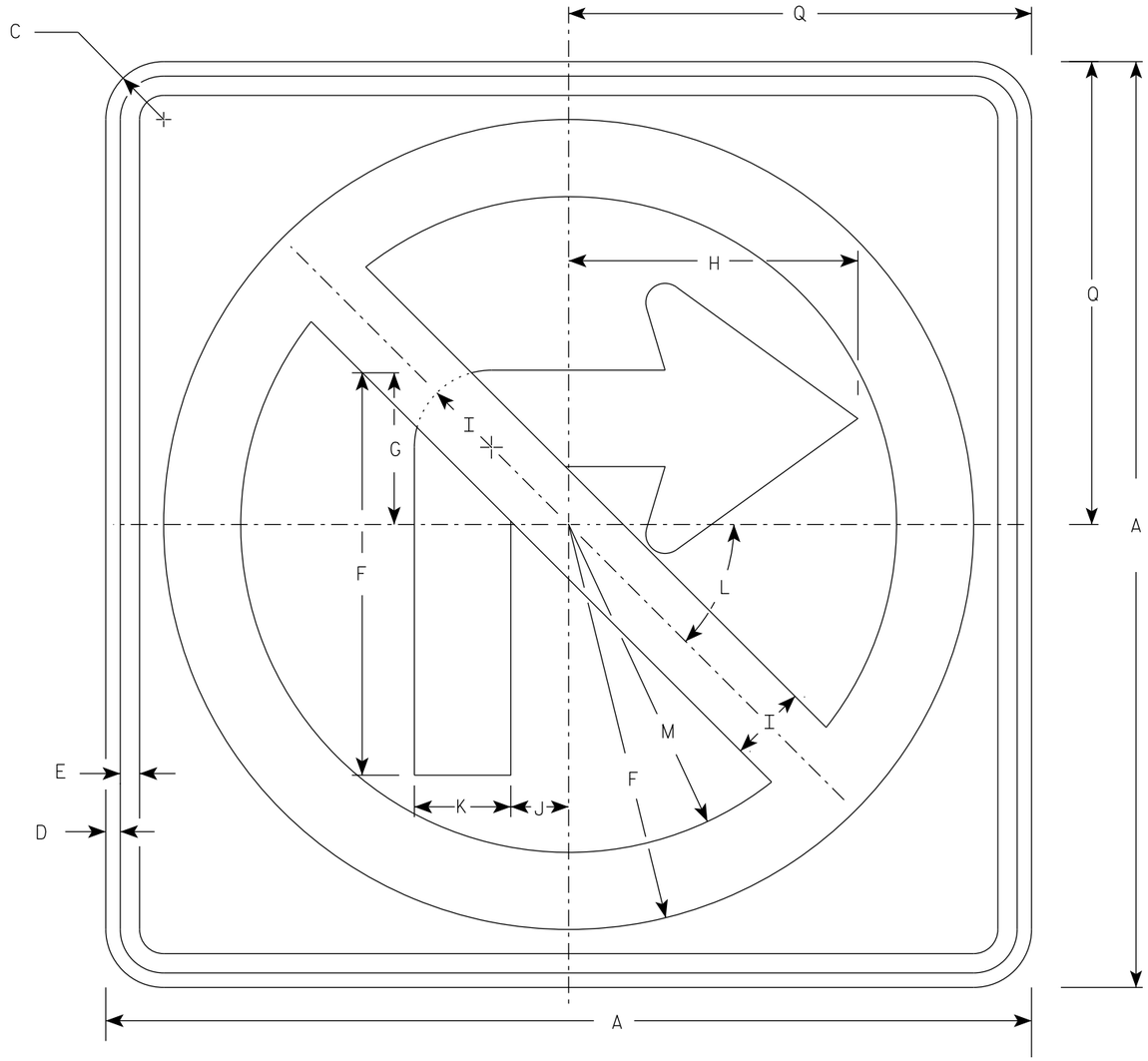
R1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/12/15 PLATE NO. R1-1.13

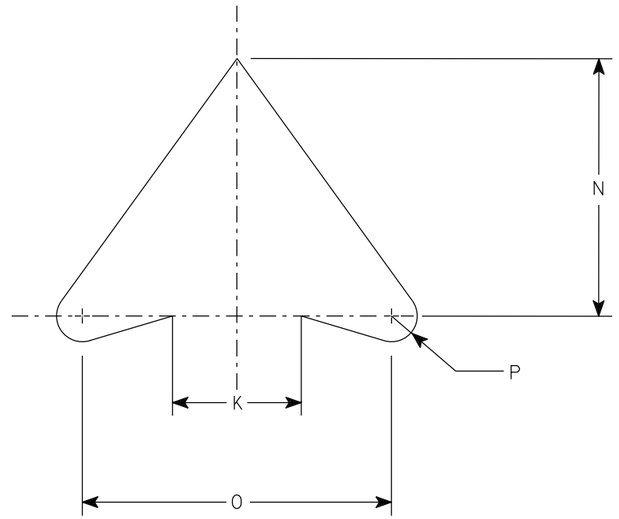
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 99	E



R3-1

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - See note 3
3. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 ½	⅜	½	10 ½	4	7 ½	2	1 ½	2 ½	45	8 ½	5	6	½	12										4.0
2S	24		1 ½	⅜	½	10 ½	4	7 ½	2	1 ½	2 ½	45°	8 ½	5	6	½	12										4.0
2M	36		2 ¼	⅝	¾	15 ¾	6	11 ¼	3	2 ¼	3 ¾	45	12 ¾	7 ½	9	¾	18										9.0
3	36		2 ¼	⅝	¾	15 ¾	6	11 ¼	3	2 ¼	3 ¾	45	12 ¾	7 ½	9	¾	18										9.0
4	36		2 ¼	⅝	¾	15 ¾	6	11 ¼	3	2 ¼	3 ¾	45°	12 ¾	7 ½	9	¾	18										9.0
5	48		2 ¼	¾	1	21	8	15	4	3	5	45°	17	10	12	1	24										16.0

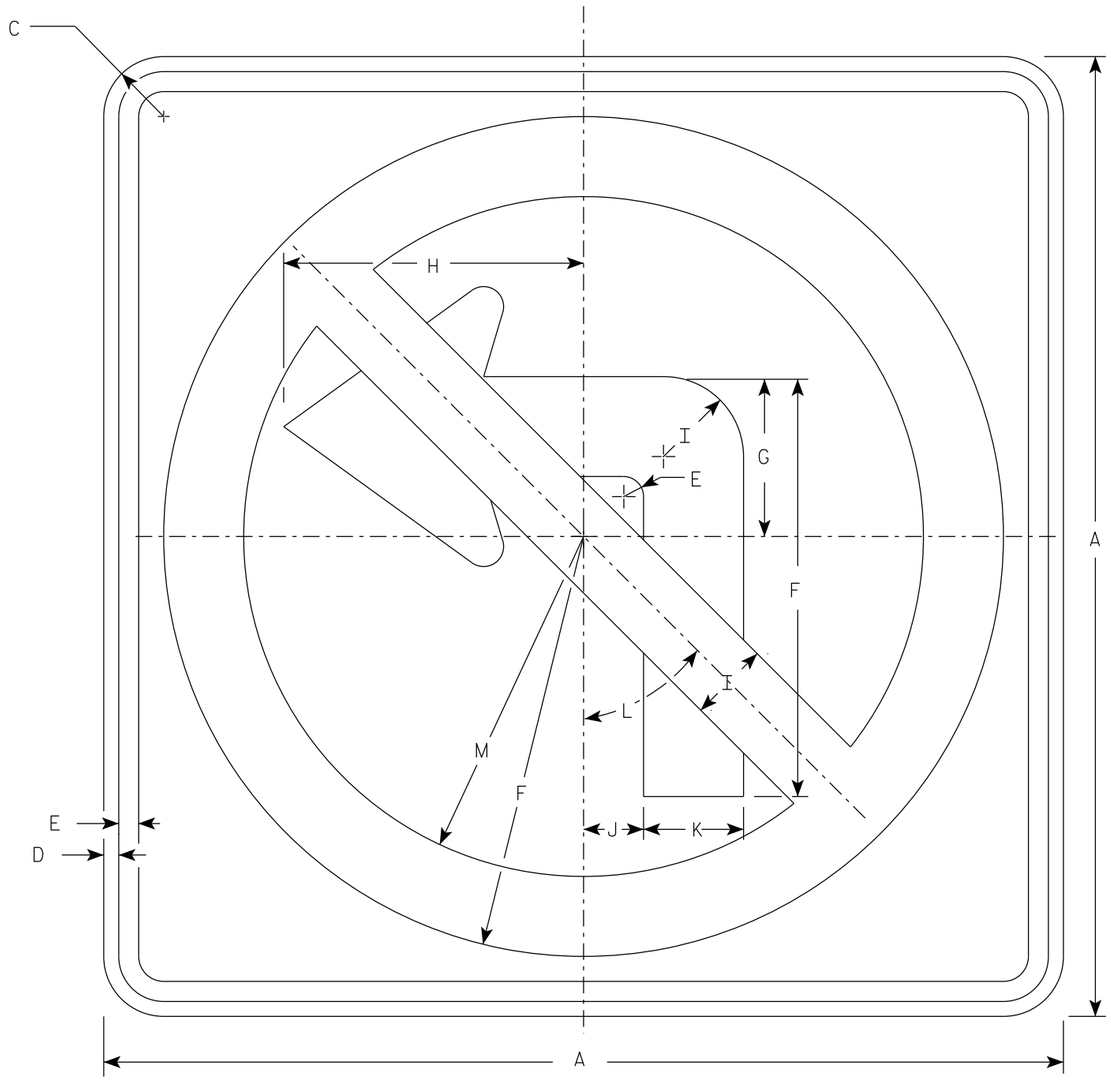
STANDARD SIGN
R3-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/2/23 PLATE NO. R3-1.6

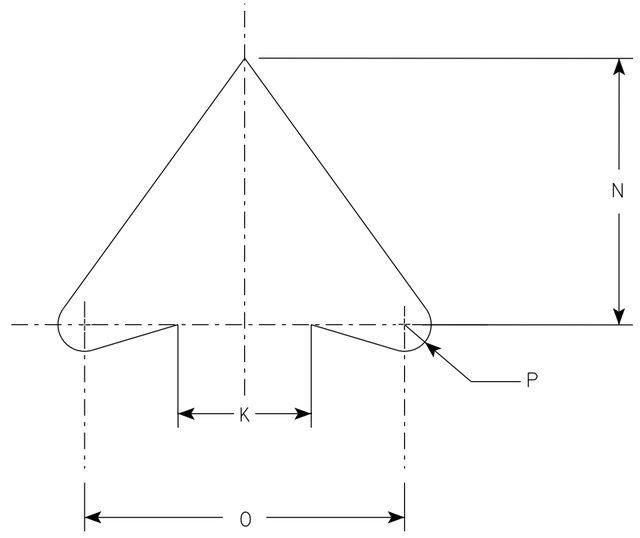
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 100 | E



R3-2

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - See note 3
3. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 ½	⅜	½	10 ½	4	7 ½	2	1 ½	2 ½	45°	8 ½	5	6	½											4.0
2S	24		1 ½	⅜	½	10 ½	4	7 ½	2	1 ½	2 ½	45°	8 ½	5	6	½											4.0
2M	36		2 ¼	⅝	¾	15 ¾	6	11 ¼	3	2 ¼	3 ¾	45°	12 ¾	7 ½	9	¾											9.0
3	36		2 ¼	⅝	¾	15 ¾	6	11 ¼	3	2 ¼	3 ¾	45°	12 ¾	7 ½	9	¾											9.0
4	36		2 ¼	⅝	¾	15 ¾	6	11 ¼	3	2 ¼	3 ¾	45°	12 ¾	7 ½	9	¾											9.0
5	48		2 ¼	¾	1	21	8	15	4	3	5	45°	17	10	12	1											16.0

STANDARD SIGN
R3-2

WISCONSIN DEPT OF TRANSPORTATION

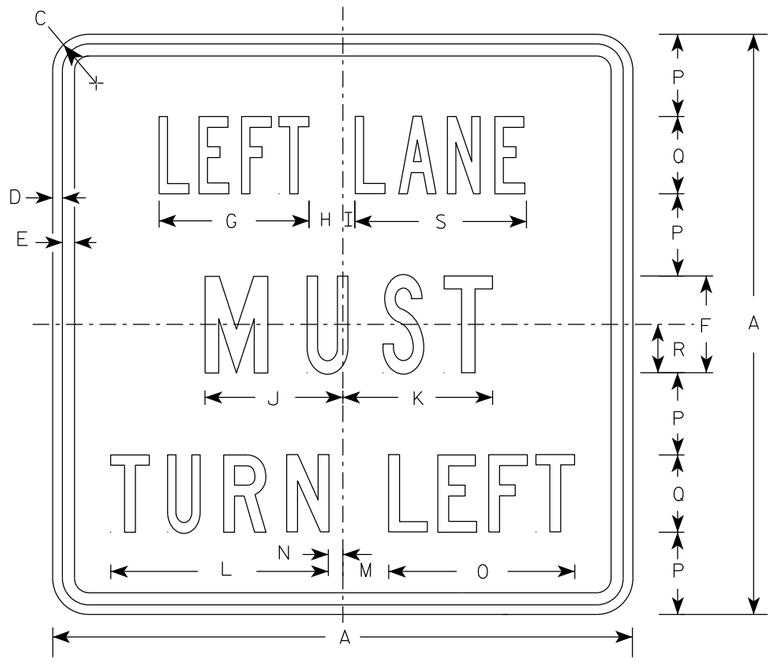
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/2/23 PLATE NO. R3-2.11

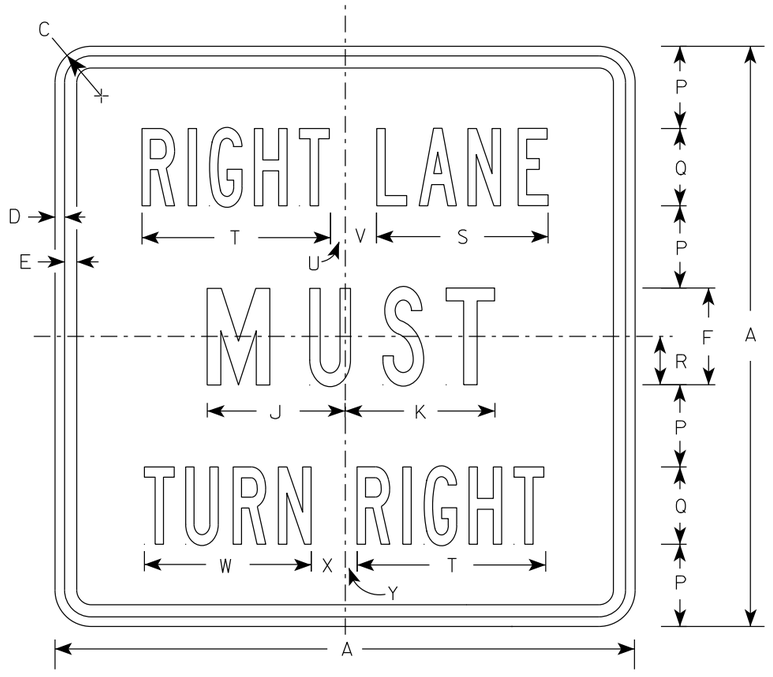
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 101 | E

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - Line 1 is Series B.
   Line 2 is Series C.
   Line 3 on plate R3-7R is Series B and Series C on plate R3-7L.



R3-7L



R3-7R

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 7/8	1/2	5/8	5	7 3/4	1 3/4	5/8	7 1/8	7 3/4	11 1/4	2 3/8	3/4	9 5/8	4 1/4	4	2 1/2	8 7/8	9 3/4	3/4	1 5/8	8 5/8	1 5/8	5/8		6.25
2S	30		1 7/8	1/2	5/8	5	7 3/4	1 3/4	5/8	7 1/8	7 3/4	11 1/4	2 3/8	3/4	9 5/8	4 1/4	4	2 1/2	8 7/8	9 3/4	3/4	1 5/8	8 5/8	1 5/8	5/8		6.25
2M	30		1 7/8	1/2	5/8	5	7 3/4	1 3/4	5/8	7 1/8	7 3/4	11 1/4	2 3/8	3/4	9 5/8	4 1/4	4	2 1/2	8 7/8	9 3/4	3/4	1 5/8	8 5/8	1 5/8	5/8		6.25
3	36		2 1/4	5/8	3/4	6	9 5/8	2	1 1/8	8 3/4	9	13 1/2	3 7/8	1 1/2	12 1/2	5	5	3	10 5/8	12	7/8	2 1/4	10 5/8	2 1/8	1		9.00
4	48		2 1/4	3/4	1	8	13 1/2	2 3/8	1 1/2	11 1/2	11 7/8	17 3/4	3 5/8	2 1/2	16 3/8	6 1/2	7	4	14 3/8	16 7/8	5/8	3 1/4	15 1/8	2 3/4	1 1/8		16.00
5																											

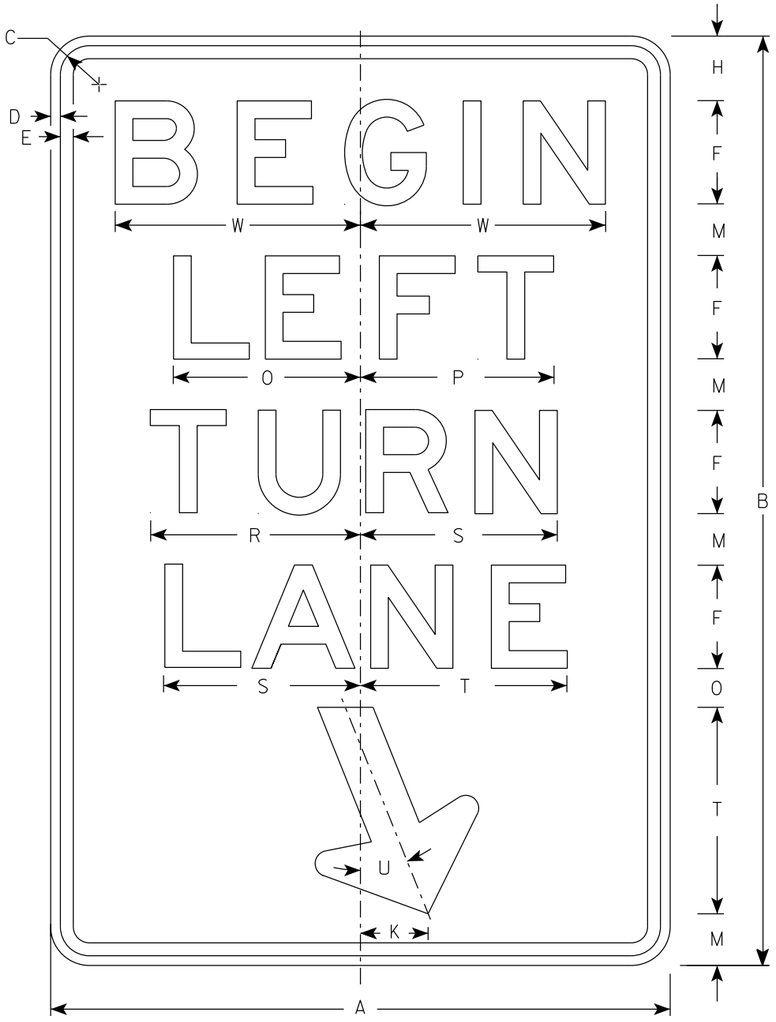
STANDARD SIGN
R3-7L & R3-7R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/13/23 PLATE NO. R3-7.4

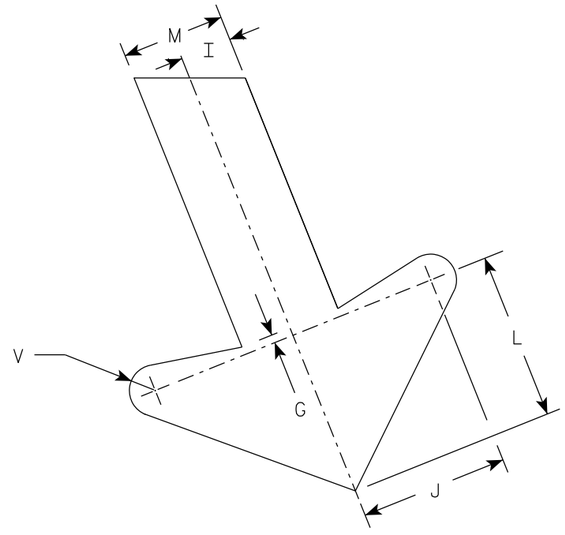
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 102 | E



R3-20L

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - E



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	36	1 1/2	3/8	1/2	4	1/4	2 1/2	1	2 7/8	2 5/8	3 1/4	2	1 1/2	7 1/4	7 1/2		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0
2M	24	36	1 1/2	3/8	1/2	4	1/4	2 1/2	1	2 7/8	2 5/8	3 1/4	2	1 1/2	7 1/4	7 1/2		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0
3	36	54	1 7/8	1/2	5/8	6	3/8	3 3/4	1 1/2	4 1/4	4	4 7/8	3	2 1/4	10 7/8	11 1/4		12 1/4	11 1/2	12	22°	3/4	13 1/4				13.5
4																											
5																											

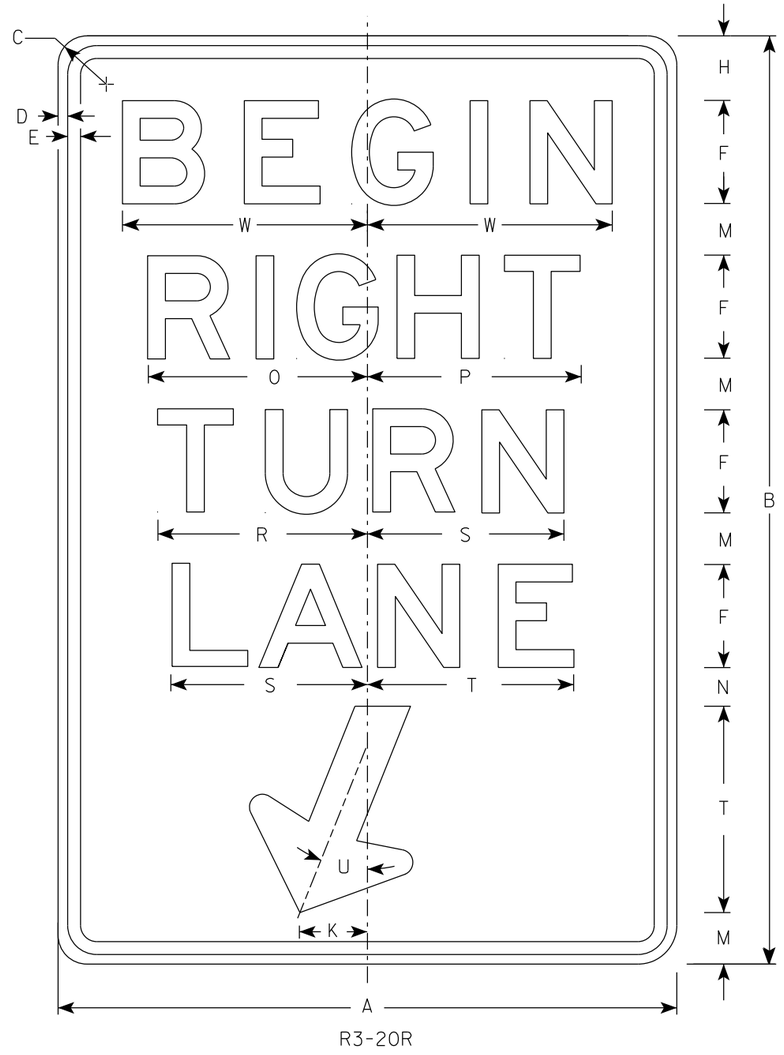
STANDARD SIGN
R3-20L

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/23/23 PLATE NO. R3-20L.8

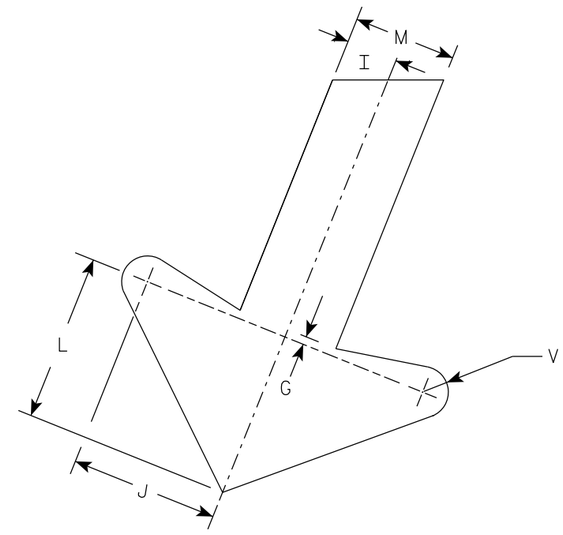
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 103 | E



R3-20R

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - E



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	36	1 1/2	3/8	1/2	4	1/4	2 1/2	1	2 7/8	2 5/8	3 1/4	2	1 1/2	8 1/2	8 1/4		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0
2M	24	36	1 1/2	3/8	1/2	4	1/4	2 1/2	1	2 7/8	2 5/8	3 1/4	2	1 1/2	8 1/2	8 1/4		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0
3	36	54	1 7/8	1/2	5/8	6	3/8	3 3/4	1 1/2	4 1/4	4	4 7/8	3	2 1/4	12 3/4	12 1/2		12 1/4	11 1/2	12	22°	3/4	13 1/4				13.5
4																											
5																											

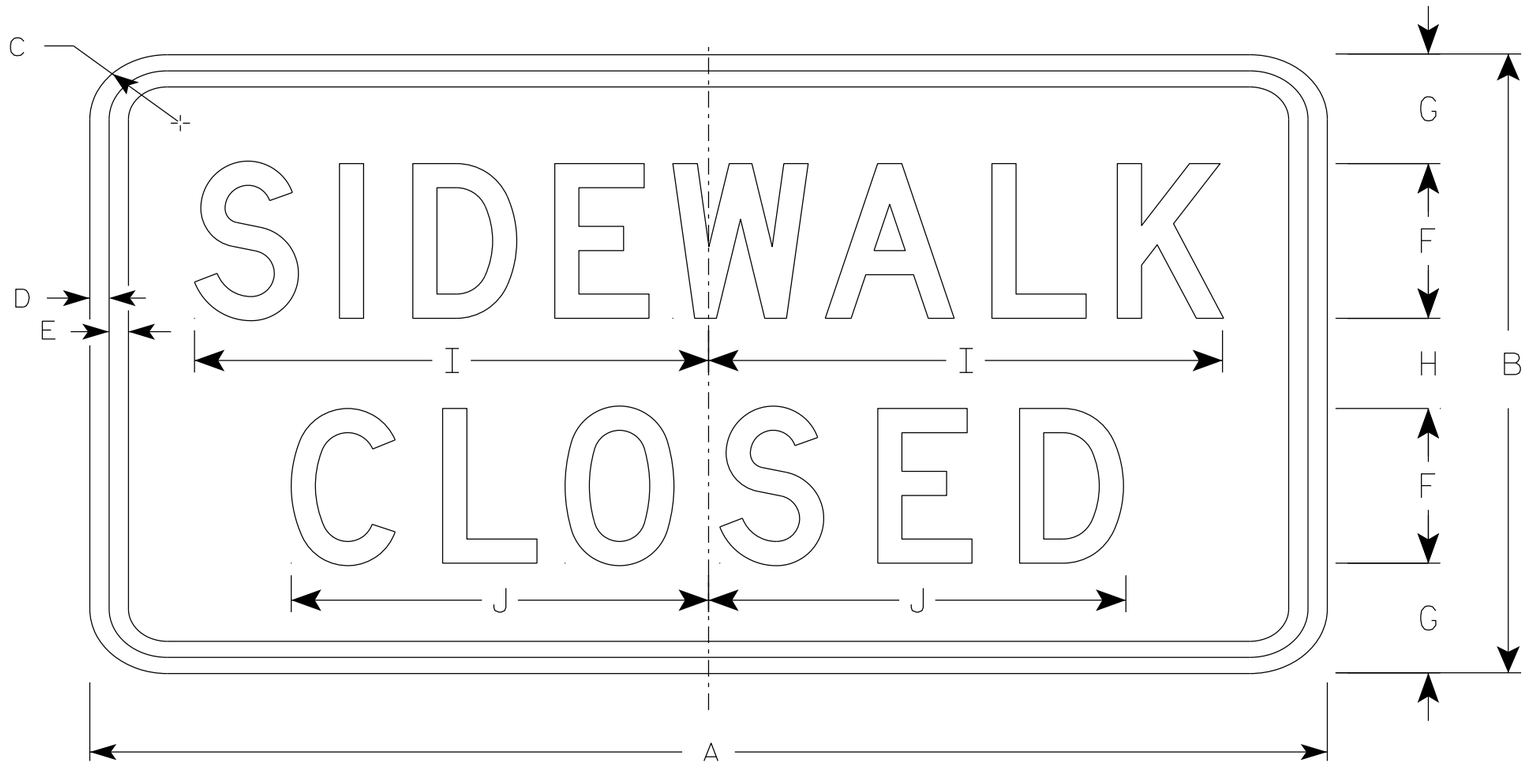
STANDARD SIGN
R3-20R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 2/23/23 PLATE NO. R3-20R.7

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 104 | E



R9-9

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - C
4. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 ½	½	½	3	2 ⅛	1 ¾	10	8 ⅛																	2.0
2M	24	12	1 ½	½	½	3	2 ⅛	1 ¾	10	8 ⅛																	2.0
3	30	18	1 ½	½	½	4	3 ½	3	12 ½	10 ¼																	3.75
4																											
5																											

STANDARD SIGN
R9-9

WISCONSIN DEPT OF TRANSPORTATION

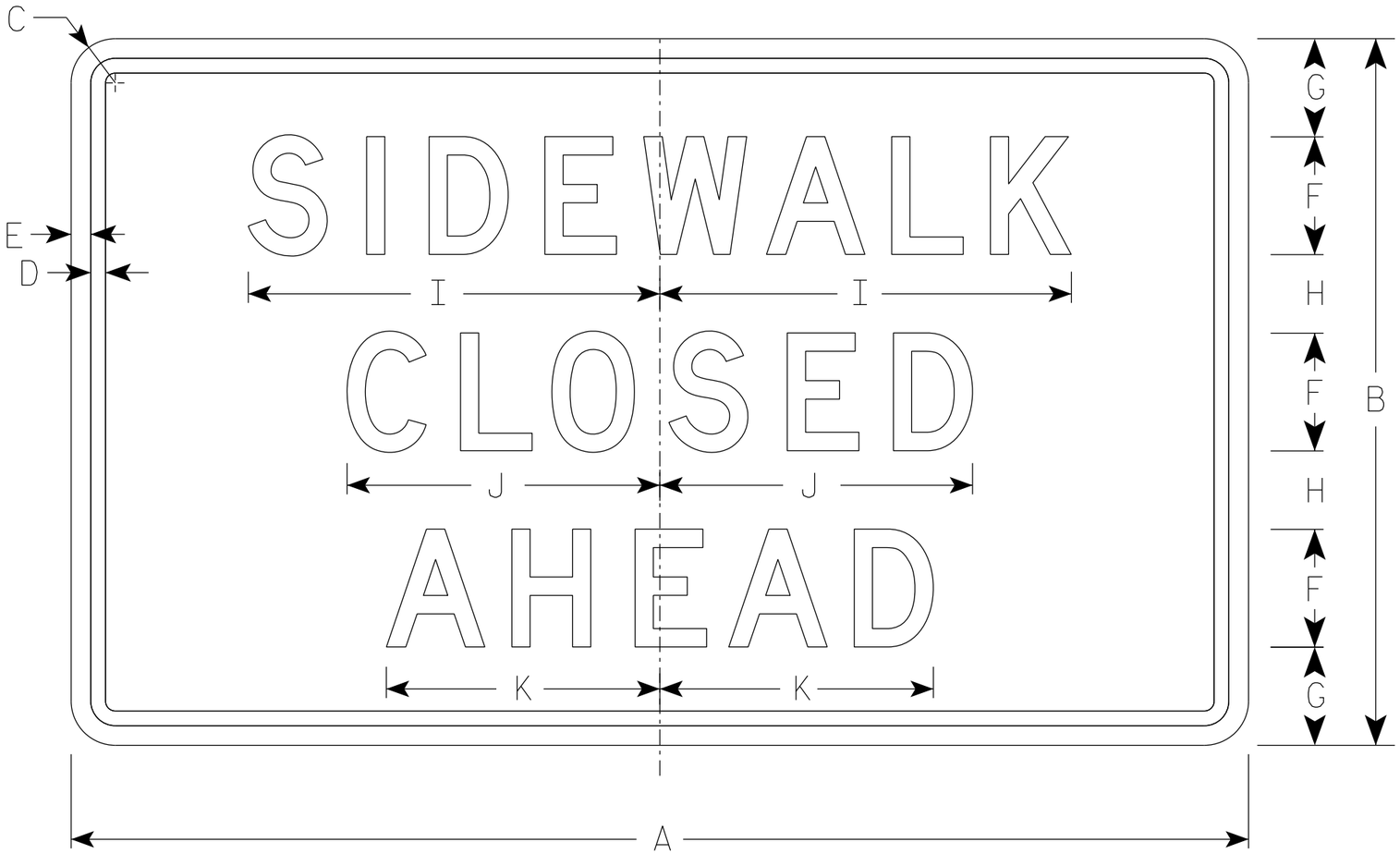
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 1/24/24 PLATE NO. R9-9.7

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 105 | E

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - D



R9-9A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 ½	⅜	½	3	2 ½	2	10 ½	8	7																3.75
2M	30	18	1 ½	⅜	½	3	2 ½	2	10 ½	8	7																3.75
3																											
4																											
5																											

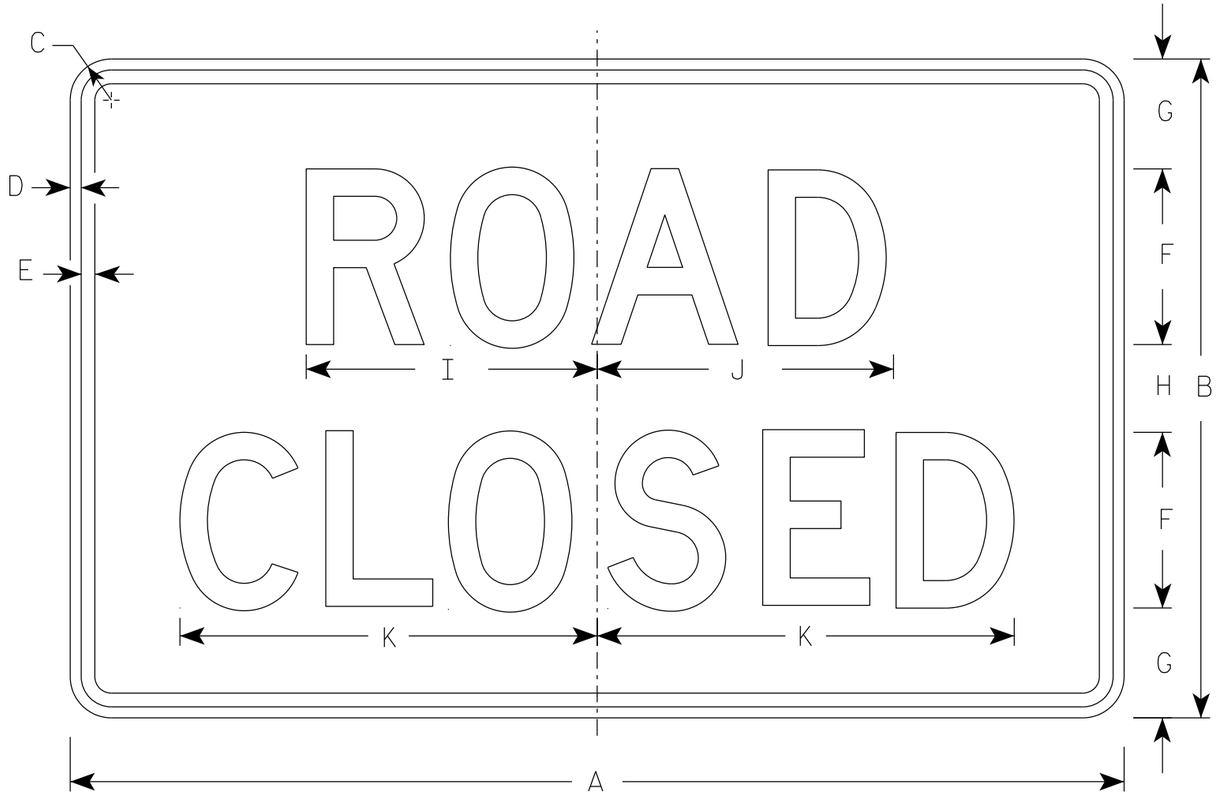
STANDARD SIGN
R9-9A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 1/24/24 PLATE NO. R9-9A.2

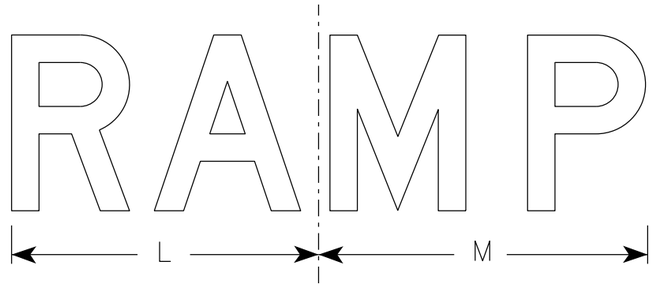
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 106 | E



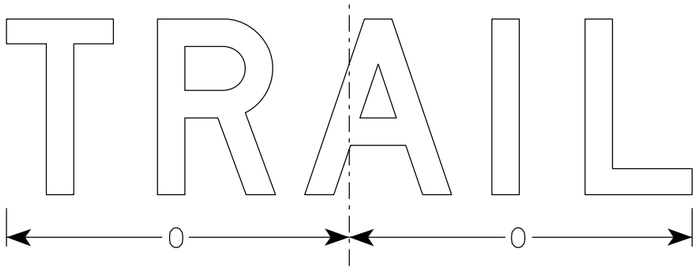
R11-2

NOTES

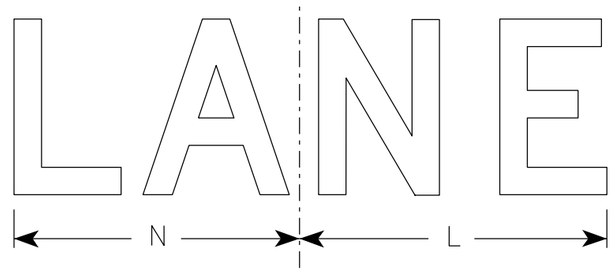
1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Modify the message as required.



R11-2R



R11-2T



R11-2L

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
2M	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
3	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
4	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
5	48	30	1 7/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0

STANDARD SIGN
R11-2

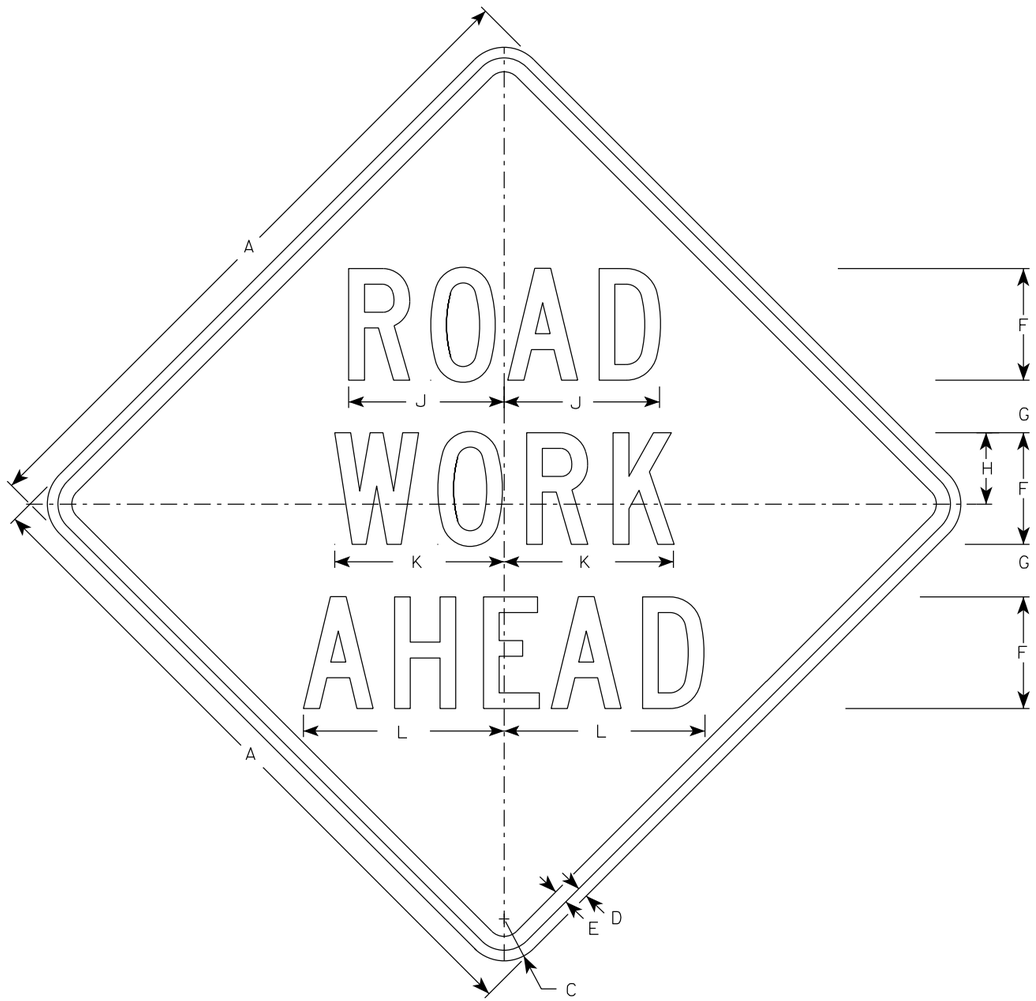
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

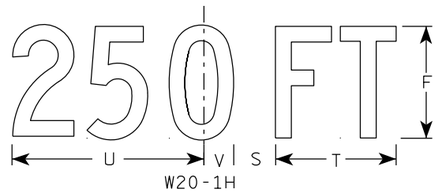
DATE 2/5/24 PLATE NO. R11-2.12

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

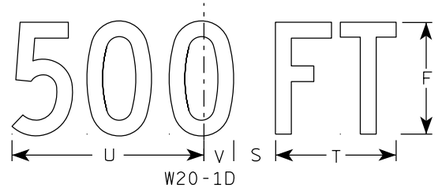
FILE NAME : C:\Users\PROJECTS\tr_stdplate\R112.dgn PLOT DATE : 5-FEB 2024 2:10 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : $$.....plotscale.....$$ WISDOT/CADDS SHEET 42



W20-1A



W20-1H



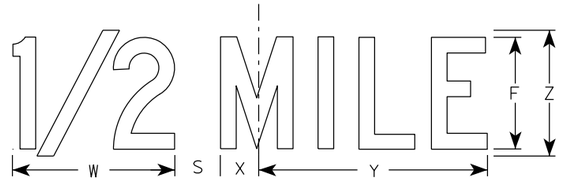
W20-1D

W20-1C

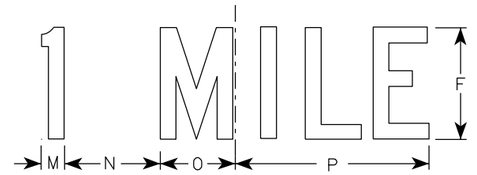
W20-1B

NOTES

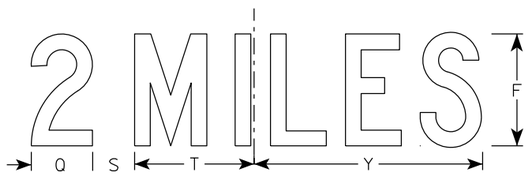
1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W20-1G



W20-1F



W20-1E

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9	3 1/4	2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN
W20-1A, B, C, D, E, F, G & H

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 1/10/2024 PLATE NO. W20-1.12

PROJECT NO:

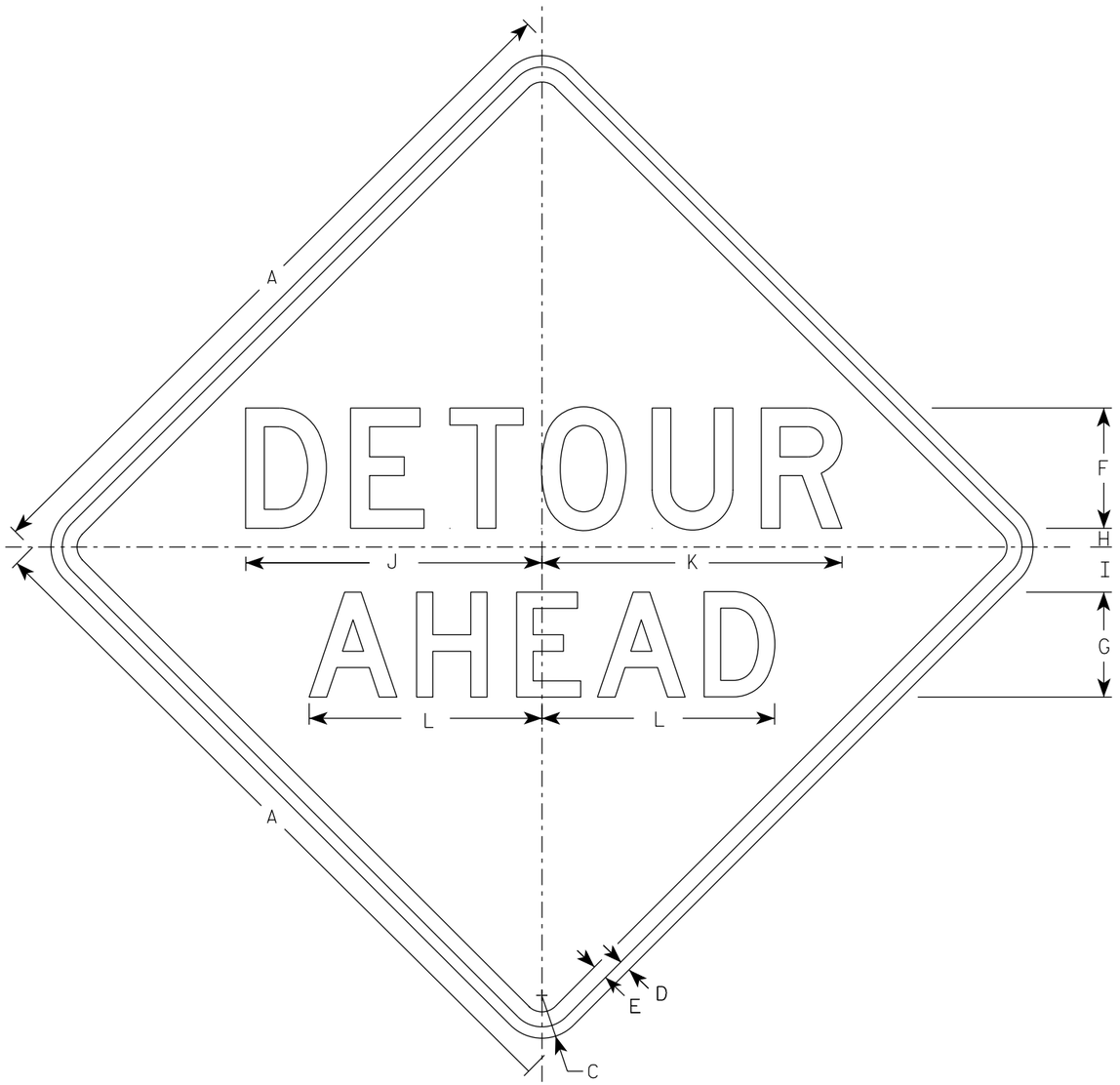
SHEET NO: E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\W201.DGN PLOT DATE : 10-JAN 2024 11:16 PLOT BY : dotc4c

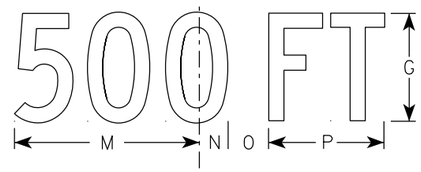
WISDOT/CADDS SHEET 42

7

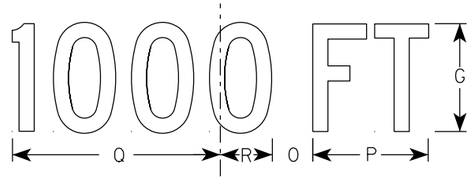
7



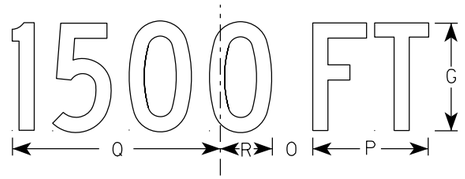
W20-2A



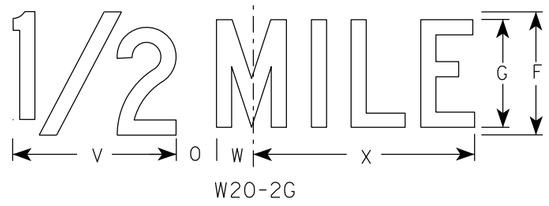
W20-2D



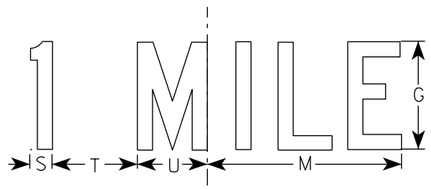
W20-2C



W20-2B



W20-2G



W20-2F

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
   Line 2 is Series D for AHEAD and Series C for all other distances.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
2M	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
3	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
4	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
5	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0

STANDARD SIGN
W20-2A,B,C,D,F & G

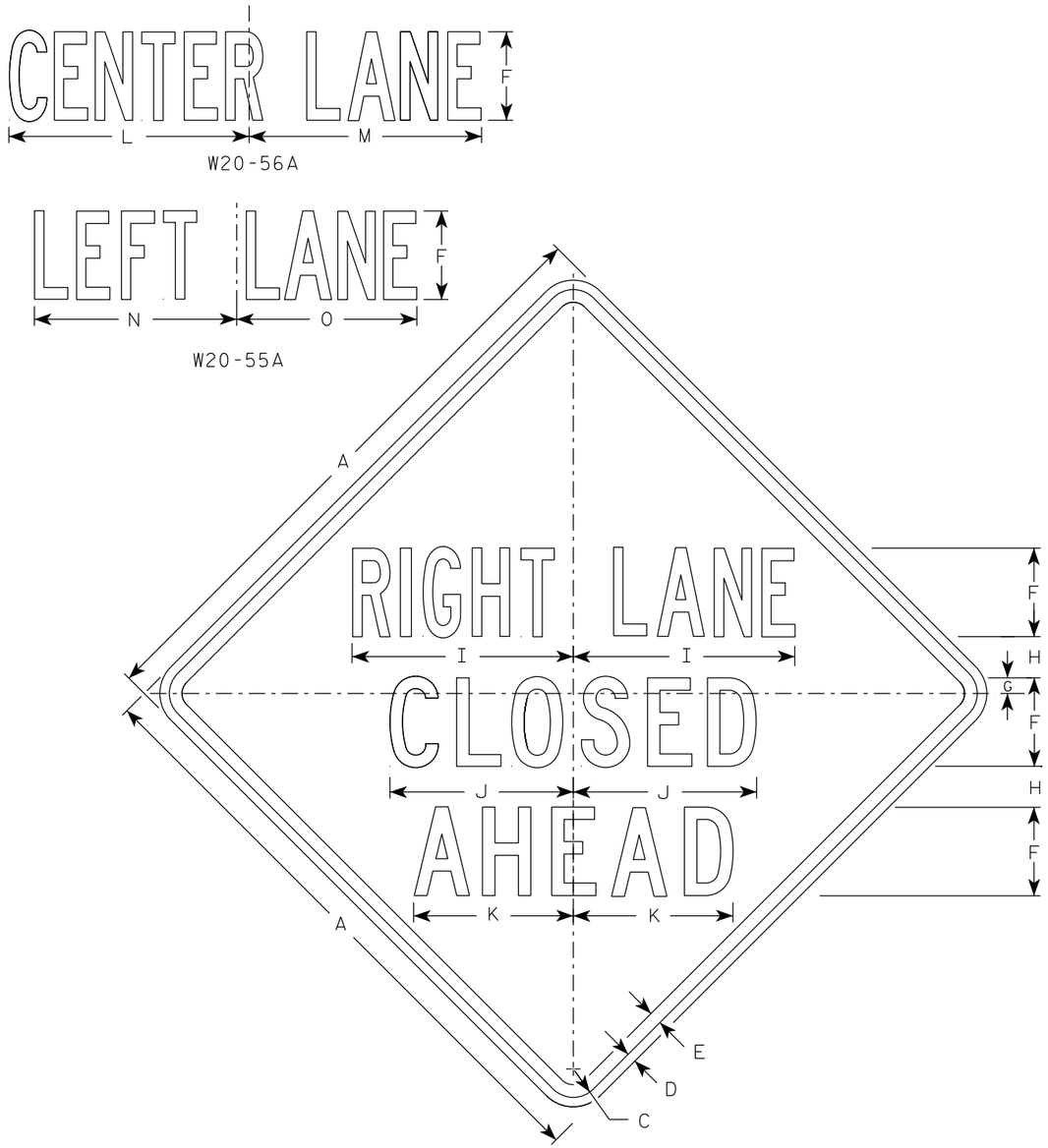
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

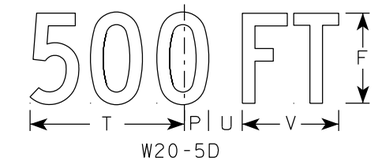
DATE 1/10/2024 PLATE NO. W20-2.7

PROJECT NO:	HWY:	COUNTY:		SHEET NO: E

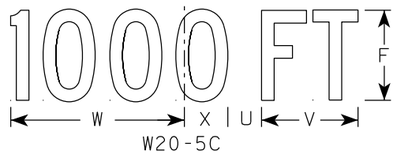
FILE NAME : C:\CAEfiles\Projects\tr_stdplate\W202.DGN PLOT DATE : 10-JAN 2024 11:36 PLOT BY : dotc4c PLOT NAME : PLOT SCALE : $$.....plotscale.....$$ WISDOT/CADDS SHEET 42



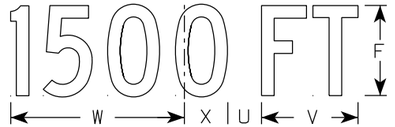
W20-5A



W20-5D



W20-5C



W20-5B

W20-5G

W20-5F

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. " _____ LANE" is Series B.
   All other copy is Series C.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	6	2 1/4	5/8	3/4	5	7/8	2 1/2	13 1/8	10 3/4	9 1/2	14 1/4	13 5/8	12	12	1 3/8	1 1/8	4 1/2	3 1/2	9	1 7/8	5 5/8	10 1/8	2 1/2	1 3/4	8	9.0
2S	48	8	3	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
2M	48	8	3	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
3	48	8	3	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
4	48	8	3	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
5	48	8	3	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0

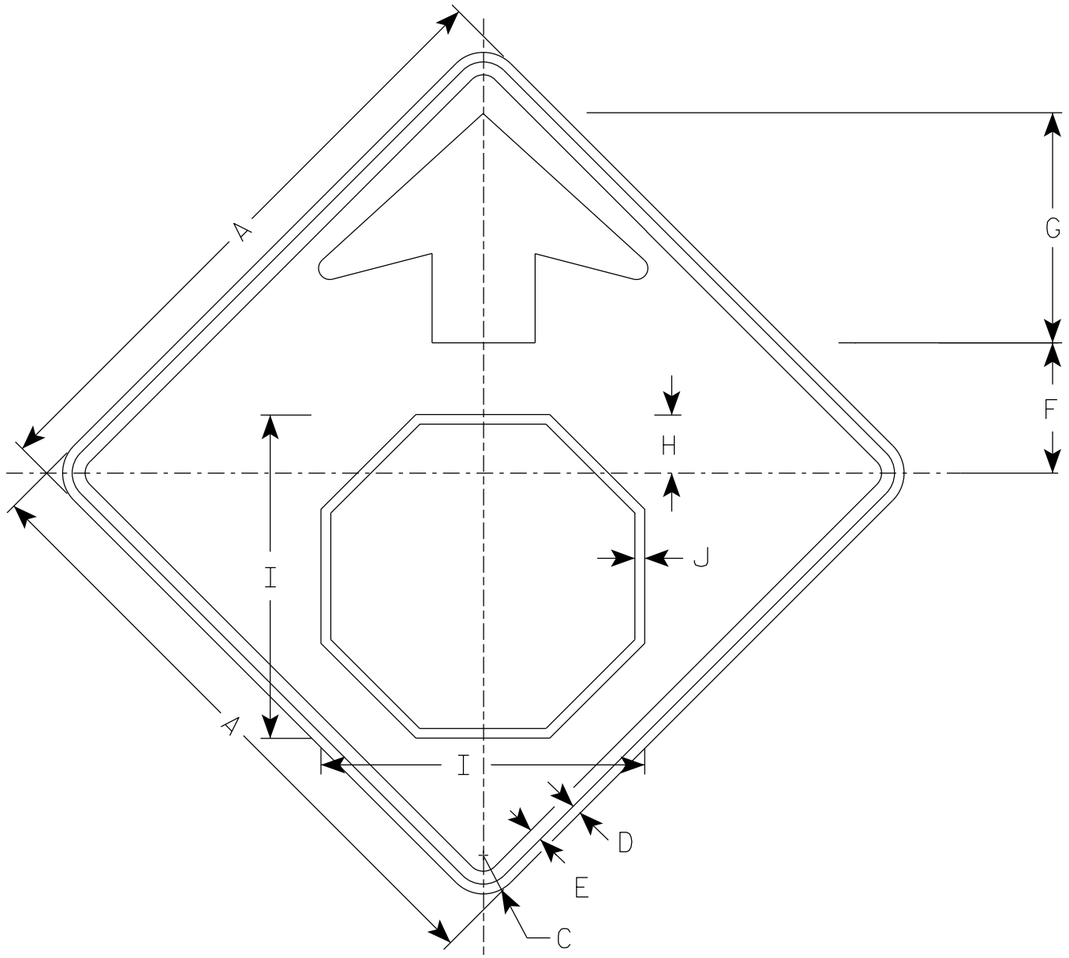
STANDARD SIGN
W20-5A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/27/24 PLATE NO. W20-5.12

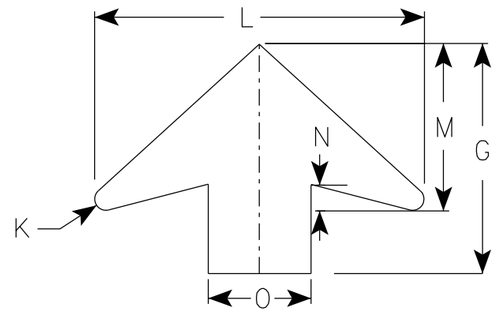
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 110 | E



W03-1

NOTES

1. All Signs Type II - Type F Reflective
2. Color:
   Background - ORANGE
   Arrow & Border - BLACK
   Stop Symbol - WHITE BORDER ON RED BACKGROUND



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
2S	48		3	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0
2M	48		3	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0
3	48		3	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0
4	48		3	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0
5	48		3	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0

STANDARD SIGN
W03-1

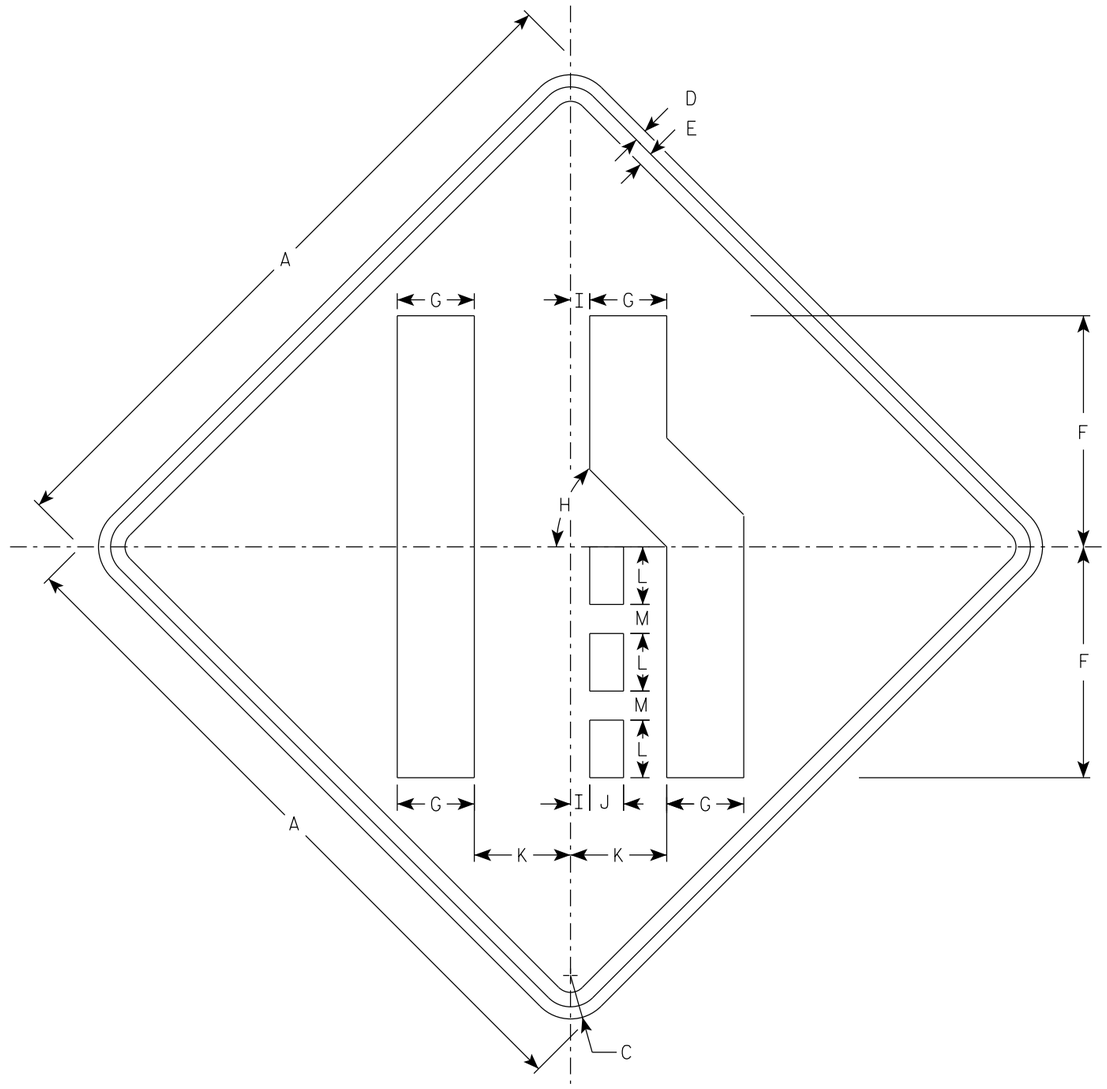
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 1/24/2024 PLATE NO. W03-1.2

PROJECT NO:

SHEET NO: 111 E



W04-2R

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W04-2L is the same as W04-2R except the symbol is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	12	4	45°	1	1 3/4	5	3	1 1/2														9.0
2S	48		3	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
2M	48		3	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
3	48		3	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
4	48		3	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
5	48		3	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0

STANDARD SIGN
W04-2

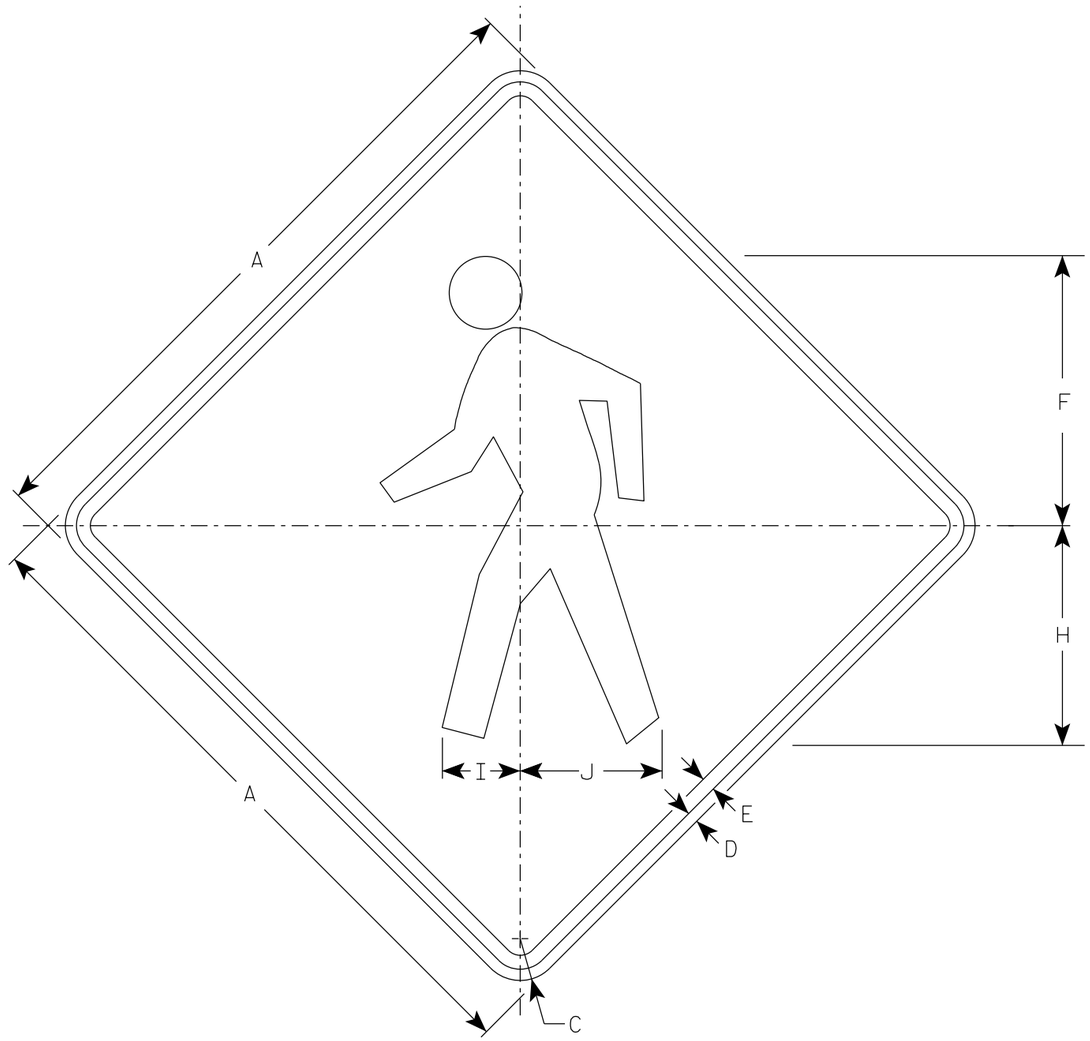
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 1/25/2024 PLATE NO. W04-2.2

PROJECT NO:

SHEET NO: E



WO11-2

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
2S	48		3	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
2M	48		3	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
3	48		3	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
4	48		3	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
5																											

STANDARD SIGN

WO11-2

WISCONSIN DEPT OF TRANSPORTATION

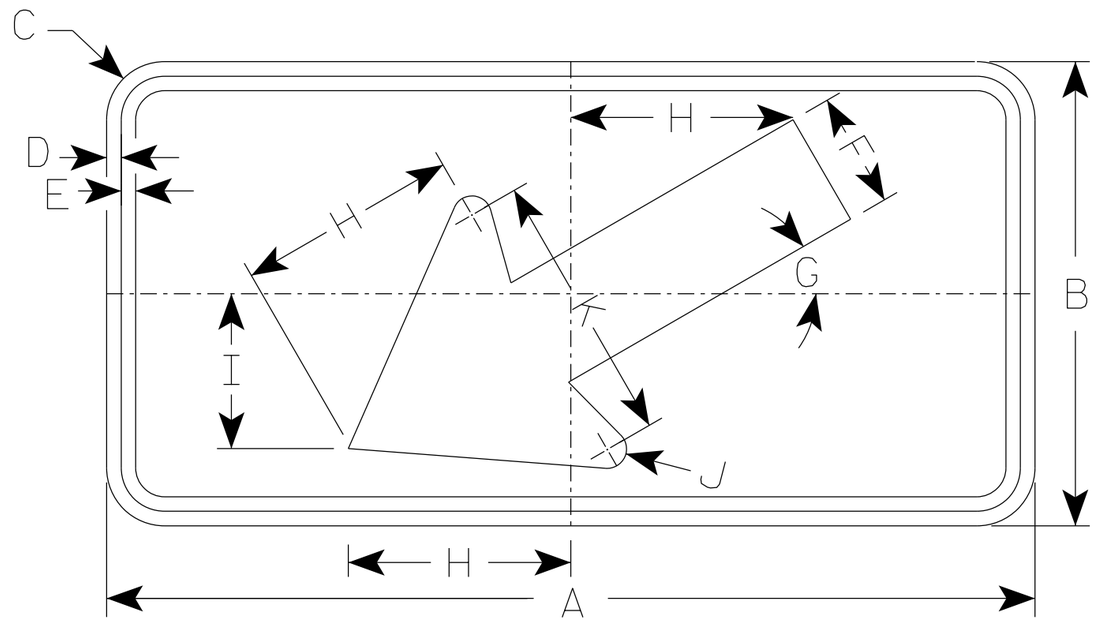
APPROVED Matthew R Rauch

For State Traffic Engineer

DATE 2/1/2024 PLATE NO. WO11-2.2

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 113 | E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\WO112.DGN PLOT DATE : 1-FEB 2024 7:02 PLOT BY : dotc4c PLOT NAME : PLOT SCALE :
WISDOT/CADDS SHEET 42



W016-7L

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Corners may be square or rounded but corners shall be rounded when base material is metal.
4. W016-7R is the same as W016-L except the arrow is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30	18	1 ½	⅜	½	4 ½	30°	8 ½	6	⅝	10 ¼																3.75
2S	48	24	1 ⅞	½	⅝	6	30°	11 ½	8	1	14																8.0
2M	48	24	1 ⅞	½	⅝	6	30°	11 ½	8	1	14																8.0
3	48	24	1 ⅞	½	⅝	6	30°	11 ½	8	1	14																8.0
4	48	24	1 ⅞	½	⅝	6	30°	11 ½	8	1	14																8.0
5	48	24	1 ⅞	½	⅝	6	30°	11 ½	8	1	14																8.0

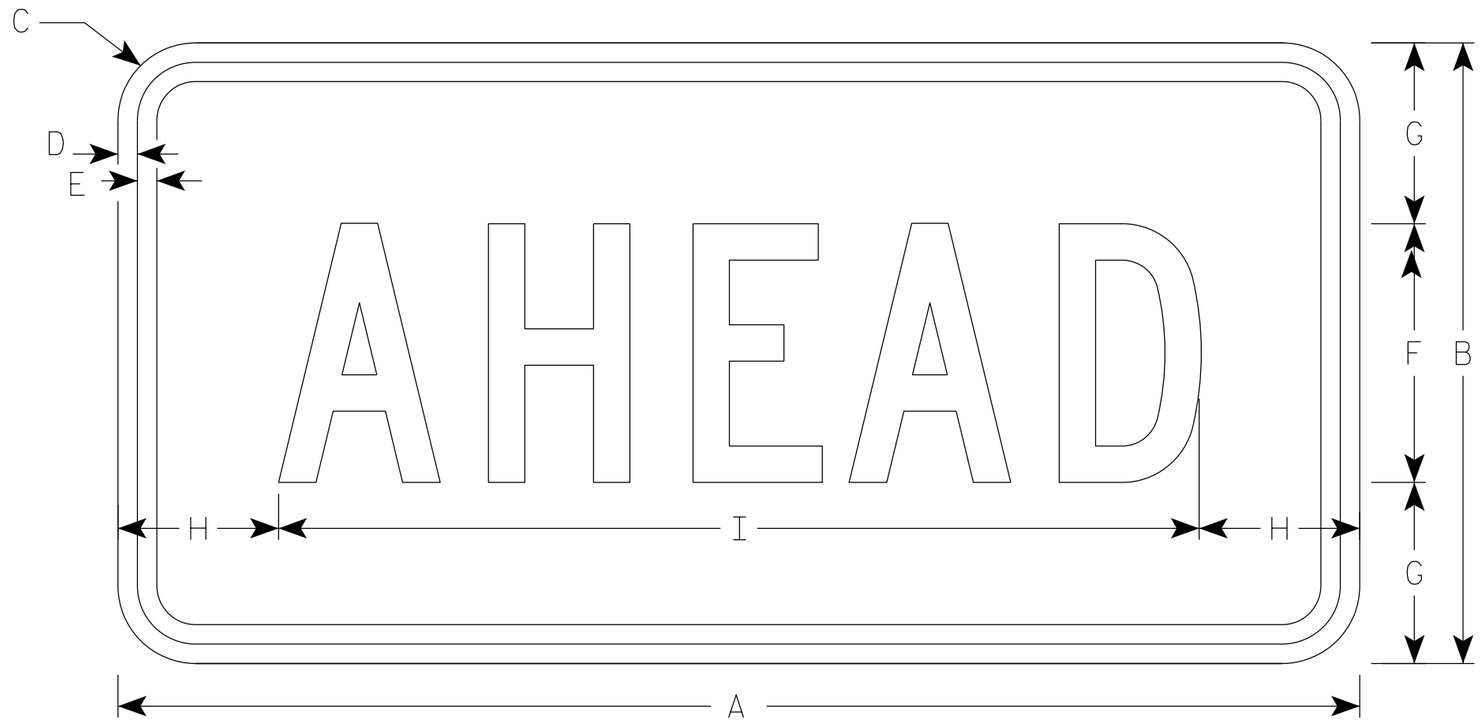
STANDARD SIGN
W016-7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/1/2024 PLATE NO. W016-7.3

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 114	E



W016-9P

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30	18	1 1/2	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
2S	48	24	1 7/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
2M	48	24	1 7/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
3	48	24	1 7/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
4	48	24	1 7/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
5	48	24	1 7/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0

STANDARD SIGN
W016-9P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/1/2024 PLATE NO. W016-9P.8

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 115 E

Wisconsin Department of Transportation

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through innovation and exceptional service.

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