

MAD

PROJECT ID:  
WITH: N/A

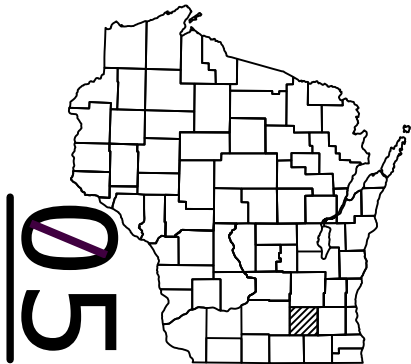
3576-01-75

COUNTY:  
JEFFERSON

JANUARY 2025  
ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details (Includes Erosion Control Plan)
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
<del>Section No.</del>	<del>8</del>	<del>Structure Plans</del>
<del>Section No.</del>	<del>9</del>	<del>Computer Earthwork Data</del>
<del>Section No.</del>	<del>0</del>	<del>Cross Sections</del>

TOTAL SHEETS = 70



DESIGN DESIGNATION

A.A.D.T.	2025	=	1210
A.A.D.T.	2045	=	1390
D.H.V.		=	125
D.D.		=	60/40
T.		=	21.4%
DESIGN SPEED		=	30 m.p.h.
ESALS		=	390,000

CONVENTIONAL SYMBOLS

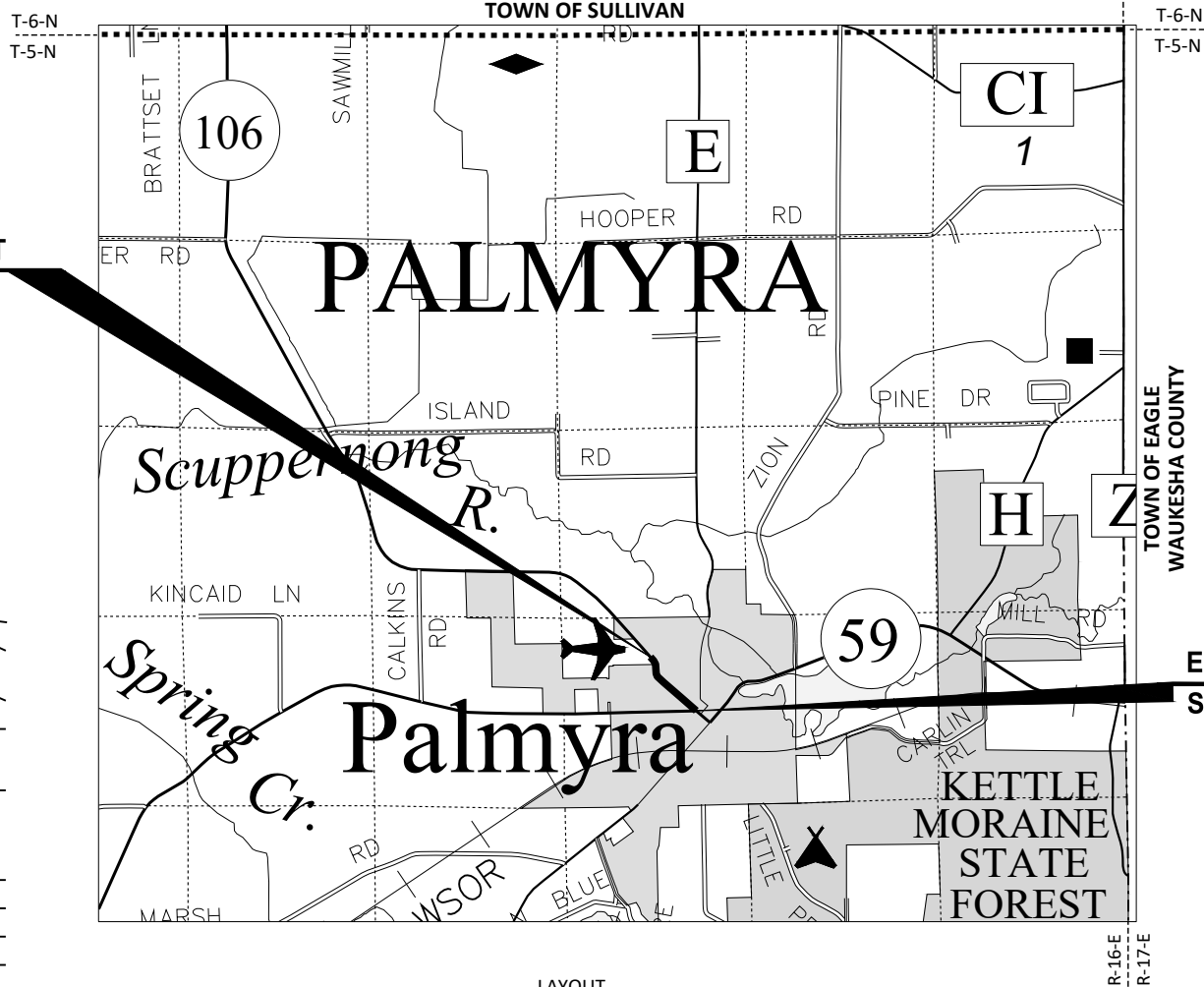
PLAN

CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE

GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

BEGIN PROJECT  
STA. 8+12.19  
Y = 514,164.99  
X = 913,146.70



LAYOUT  
SCALE 0 1 MI  
TOTAL NET LENGTH OF CENTERLINE = 0.364 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, JEFFERSON COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCE MAY BE USED AS GROUND DISTANCES.

ELEVATION SHOWN ON THIS PLAN ARE REFERENCE TO THE NORTH AMERICAN VERTICAL DATUM OF 1988, NAVD88 (2012).

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
3576-01-75	WISC 2025149	1

ORIGINAL PLANS PREPARED BY

**JEWELL**  
associates engineers, inc  
Engineers - Architects - Surveyors



STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

PREPARED BY	JEWELL ASSOCIATES ENGINEERS, INC.
Surveyor	JEWELL ASSOCIATES ENGINEERS, INC.
Designer	MATTHEW LAMB, P.E.
Project Manager	SW REGION
Regional Examiner	JUSTIN KUTSCHENREUTER, P.E.
Regional Supervisor	

APPROVED FOR THE DEPARTMENT  
DATE: 7/24/2024  
(Signature)

E

GENERAL NOTES

NO TREES OR SHRUBS ARE TO BE REMOVED UNLESS SUCH TREES OR SHRUBS HAVE FIRST BEEN INDICATED FOR REMOVAL BY THE ENGINEER IN THE FIELD.

EROSION CONTROL ITEMS IN THE MISC. QUAN. ARE SUGGESTED. EXACT LOCATIONS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD. MAINTAIN EROSION CONTROL ITEMS UNTIL SUCH TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NECESSARY.

DISTURBED AREAS SHOWN WITHIN THE RIGHT-OF-WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS ARE TO BE FERTILIZED (TYPE B), SEEDED (USE SEED MIX NO. 40), AND EROSION MATTED AS DIRECTED BY THE ENGINEER.

EXISTING SHOULDER AGGREGATE SHALL BE INCORPORATED INTO THE NEW SHOULDERS UNLESS OTHERWISE DIRECTED BY THE ENGINEER IN THE FIELD.

WHEN THE QUANTITY OF THE ITEM OF BASE AGGREGATE DENSE OR HMA PAVEMENT IS MEASURED FOR PAYMENT BY THE TON, THE DEPTH OR THICKNESS OF THE COURSE SHOWN ON THE PLANS IS APPROXIMATE, AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER IN THE FIELD.

REMOVAL OF ASPHALTIC SURFACES WHERE AN ABUTTING ASPHALTIC SURFACE IS TO REMAIN IN PLACE SHALL REQUIRE A VERTICAL EDGE MEETING THE APPROVAL OF THE ENGINEER IN THE FIELD.

HMA PAVEMENT QUANTITIES WERE CALCULATED USING 112 LB/SY/IN.

2-INCHES OF HMA PAVEMENT SHALL BE CONSTRUCTED WITH A SINGLE LAYER OF HMA PAVEMENT 4 LT 58-28 S.

PAVING LIMITS AT INTERSECTIONS ARE TO BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE EXACT LOCATIONS AND LIMITS OF PRIVATE ENTRANCES, COMMERCIAL ENTRANCES, AND FIELD ENTRANCES SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

APPLY TACK COAT TO THE MILLED SURFACE PRIOR TO PLACEMENT OF HMA PAVEMENT AT A RATE OF 0.07 GAL/SY.

THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, OR PASSING LANE.

EXACT LOCATIONS FOR THE REMOVING DISTRESSED PAVEMENT MILLING SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

THE LOW SIDE SHOULDER SLOPE ON SUPERELEVATED SECTIONS EQUALS THE SUPERELEVATION WHEN THE SUPERELEVATION IS GREATER THAN 0.04 FT./FT. IF THE SUPERELEVATION IS LESS THAN OR EQUALS 0.04 FT./FT., THEN THE LOW SIDE SHOULDER SLOPE IS 0.04 FT./FT. THE HIGH SIDE SHOULDER SLOPE ON THE SUPERELEVATED SECTION EQUALS THE SUPERELEVATION.

CURVE DATA IS BASED ON THE ARC DEFINITION.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, ALL SIGNS RELATING TO THIS OPERATION SHALL BE COVERED OR REMOVED AND FACILITY RESTORED TO NORMAL OPERATIONS.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

EXISTING SIDEWALK SHALL BE REMOVED TO AN EXISTING JOINT OR SAWED AS DETERMINED BY THE ENGINEER IN THE FIELD OR AS SHOWN ON THE PLANS.

ALL RADII DIMENSIONS ON THE PLAN FOR CURB AND GUTTER ARE TO THE EDGE OF ASPHALT.

THE LOCATION OF ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD PRIOR TO PLACEMENT.

△ CONTROL POINTS

NO.	STA.	DESCRIPTION	Y	X	Z
1	13+50	¾" I.R.S., 31.0 RT.	513671.48	913286.25	844.83
2	23+00	¾" I.R.S., 23.3 RT.	513014.34	913987.31	854.27
3	27+24	¾" I.R.S., 36.5 LT.	512780.66	914345.24	854.19

CONTACTS

WISCONSIN DEPARTMENT OF TRANSPORTATION:

WisDOT PROJECT MANAGER  
2101 WRIGHT STREET  
MADISON, WI 53704  
ATTN: MATTHEW LAMB, P.E.  
PH: (608) 246-5638  
EMAIL: Matthew.Lamb@dot.wi.gov

JEFFERSON COUNTY HIGHWAY DEPARTMENT:

HIGHWAY OPERATIONS MANAGER  
1425 SOUTH WISCONSIN DRIVE  
JEFFERSON, WI 53549  
ATTN: BRIAN UDOVICH  
PH: (920) 723-7273  
EMAIL: brianu.jeffersoncountywi.gov

DESIGN CONSULTANT:

JEWELL ASSOCIATES ENGINEERS, INC.  
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SPRING GREEN, WI 53588  
ATTN: DAN TRACY, P.E.  
PH: (608) 459-6052  
CELL: (608) 604-6905  
EMAIL: dan.tracy@jewellassoc.com

WDNR LIAISON:

STATE OF WISCONSIN  
DNR SOUTH CENTRAL REGION HQ  
3911 FISH HATCHERY ROAD  
FITCHBURG, WI 53711  
ATTN: SHELLEY NELSON  
PH: (608) 444-2835  
EMAIL: Shelley.Nelson@wisconsin.gov

UTILITIES

COMMUNICATION LINE

BRIGHTSPEED OF WESTERN WISCONSIN, LLC  
ATTN: JAMES WINTER  
224 INDUSTRIAL DRIVE  
NORTH PRAIRIE, WI 53153  
PH: (262) 392-5210  
EMAIL: james.winter@brightspeed.com

SPECTRUM

ATTN: BRAD WAGNER  
1348 PLAINFIELD AVE.  
JANESVILLE, WI 53545  
PH: (608) 826-1318  
EMAIL: brad.wagner@charter.com

SANITARY SEWER

VILLAGE OF PALMYRA WATER & SEWER UTILITIES  
ATTN: SCOTT HALBRUCKER  
700 BRENNAN ROAD  
PALMYRA, WI 53156  
PH: (262) 495-4106  
EMAIL: publicworks@villageofpalmyra.com

WATER

VILLAGE OF PALMYRA WATER & SEWER UTILITIES  
ATTN: SCOTT HALBRUCKER  
700 BRENNAN ROAD  
PALMYRA, WI 53156  
PH: (262) 495-4106  
EMAIL: publicworks@villageofpalmyra.com

ELECTRIC

WE ENERGIES  
ATTN: ERIC KICKHAVER  
500 S. 116TH STREET  
WEST ALLIS, WI 53214  
PH: (414) 944-5917  
EMAIL: eric.kickhaver@we-energies.com

GAS

WE ENERGIES  
ATTN: JASON DODGE  
500 S. 116TH STREET  
WEST ALLIS, WI 53214  
PH: (414) 944-5507  
EMAIL: jason.dodge@we-energies.com



ORDER OF SECTION 2 SHEETS:

- WRITTEN MATERIAL
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS (INCLUDES EROSION CONTROL PLAN)
- TRAFFIC CONTROL
- PAVEMENT MARKING & PERMANENT SIGNING

LIST OF STANDARD ABBREVIATIONS

ABUT	Abutment	INV	Invert	RDWY	Roadway
AC	Acre	IP	Iron Pipe or Pin	SALV	Salvaged
AGG	Aggregate	IRS	Iron Rod Set	SAN S	Sanitary Sewer
AH	Ahead	JT	Joint	SEC	Section
<	Angle	JCT	Junction	SHLDR	Shoulder
ASPH	Asphaltic	LHF	Left-Hand Forward	SHR	Shrinkage
AVG	Average	L	Length of Curve	SW	Sidewalk
ADT	Average Daily Traffic	LIN FT	Linear Foot	S	South
BAD	Base Aggregate Dense	or LF		SQ	Square
BK	Back	LC	Long Chord of Curve	SF or SQ FT	Square Feet
BF	Back Face	MH	Manhole	SV or SQ YD	Square Yard
BM	Bench Mark	MB	Mailbox	STD	Standard
BR	Bridge	ML or M/L	Match Line	SDD	Standard Detail Drawings
C or C/L	Center Line	N	North	STH	State Trunk Highways
CC	Center to Center	Y	North Grid Coordinate	STA	Station
C.E.	Commercial Entrance	OD	Outside Diameter	SS	Storm Sewer
CTH	County Trunk Highway	PLE	Permanent Limited Easement	SG	Subgrade
CR	Creek	PT	Point	SE	Superelevation
CR	Crushed	PC	Point of Curvature	SL or S/L	Survey Line
CY or CU YD	Cubic Yard	PI	Point of Intersection	SV	Septic Vent
CP	Culvert Pipe	PRC	Point of Reverse Curvature	T	Tangent
C & G	Curb and Gutter			TEL	Telephone
D	Degree of Curve	PT	Point of Tangency	TEMP	Temporary
DHV	Design Hour Volume	POC	Point On Curve	TI	Temporary Interest
DIA	Diameter	POT	Point on Tangent	TLE	Temporary Limited Easement
E	East	PVC	Polyvinyl Chloride		Ton
X	East Grid Coordinate	PCC	Portland Cement Concrete	t	Town
ELEC	Electric (al)	LB	Pound	T or TN	TRANS
EL or ELEV	Elevation	PSI	Pounds Per Square Inch	TL or T/L	Trucks (percent of)
ESALS	Equivalent Single Axle Loads	P.E.	Private Entrance	T	Typical
EBS	Excavation Below Subgrade	R	Radius	TYP	Unclassified
		RR	Railroad	UNCL	Underground Cable
FF	Face to Face	R	Range	UG	United States Highway
F.E.	Field Entrance	RL or R/L	Reference Line	VAR	Variable
F	Fill	RP	Reference Point	V	Velocity or Design Speed
FG	Finished Grade	RCCP	Reinforced Concrete Culvert Pipe	VERT	Vertical
FL or F/L	Flow Line	REQD	Required	VC	Vertical Curve
FT	Foot	RES	Residence or Residential	VOL	Volume
FTG	Footing	RW	Retaining Wall	WM	Water Main
GN	Grid North	RT	Right	WV	Water Valve
HT	Height	RHF	Right-Hand Forward	W	West
CWT	Hundredweight	R/W	Right-of-Way	WB	Westbound
HYD	Hydrant	RD	Road	YD	Yard
INL	Inlet	R	River		
ID	Inside Diameter				

FOR INFORMATION ONLY

BORING LOG

NO.	STATION/OFFSET	EXISTING ASPHALTIC SURFACE THICKNESS (IN.)	EXISTING B.A.D. THICKNESS (IN.)	FILL, OTHER MATERIAL
1	13+60, 2' RT.	4.75	9.5	2.5-FT FINE SAND FILL, FIRM, DRY, OVER 2-FT FINE SAND FILL, SOME SILT, VERY LOOSE, DRY
2	18+80, 7' LT.	5	8.5	2.5-FT FINE SILTY SAND FILL, VERY LOOSE, DRY, OVER 1.2-FT FINE SAND, LITTLE SILT, LOOSE, DRY
3	23+75, 6' RT.	4.5	2.5	1.3-FT FINE SAND FILL, LITTLE SILT, DRY, OVER 2-FT CLAYEY FINE SAND FILL, VERY LOOSE, DRY OVER 1.2-FT FINE SAND FILL, LITTLE SILT AND GRAVEL, LOOSE, DRY

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT												
ASPHALT						.70 - .95						
CONCRETE						.80 - .95						
BRICK						.70 - .80						
DRIVES, WALKS						.75 - .85						
ROOFS						.75 - .95						
GRAVEL ROADS, SHOULDERS						.40 - .60						

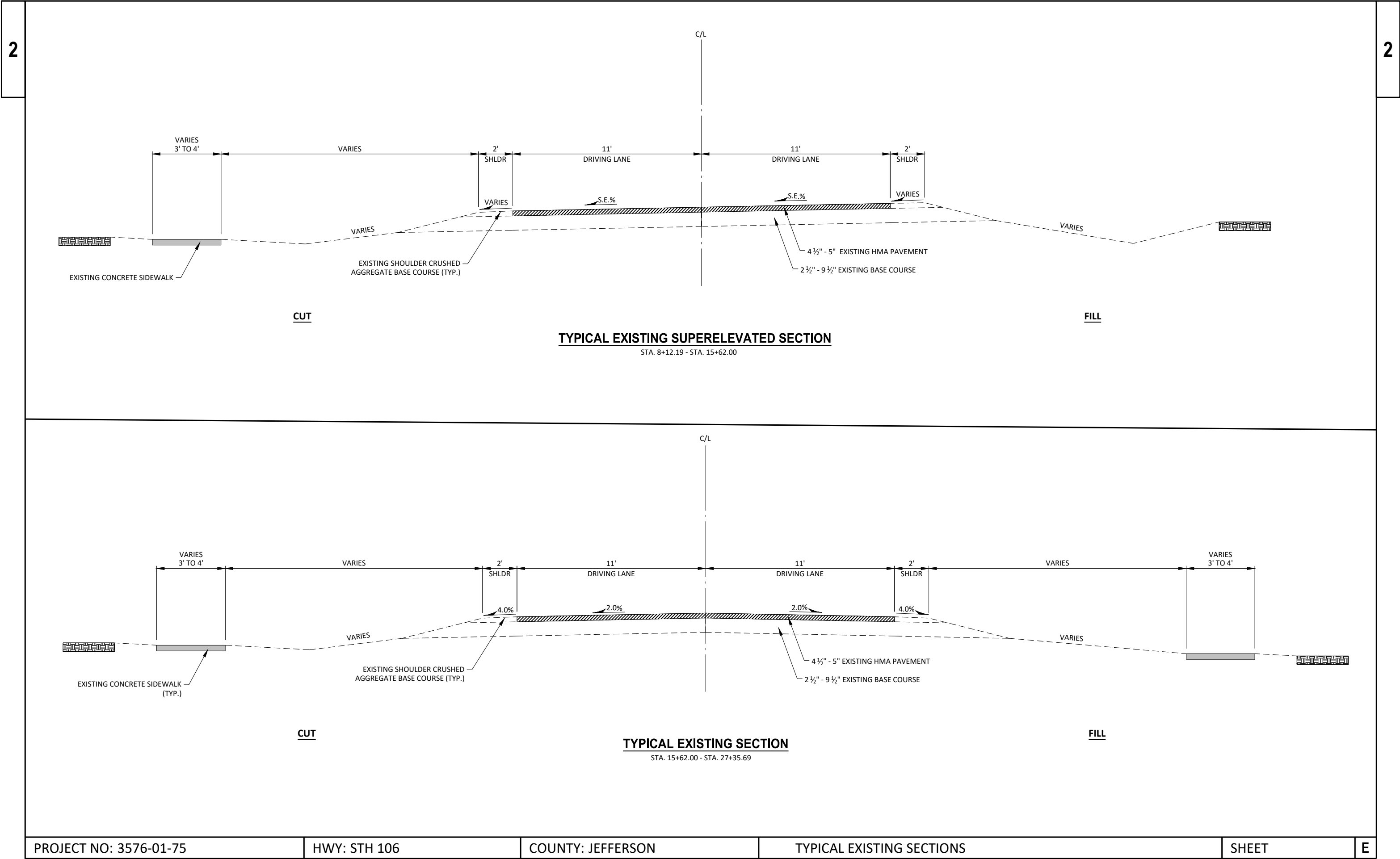
TOTAL PROJECT AREA= 2.75 ACRES

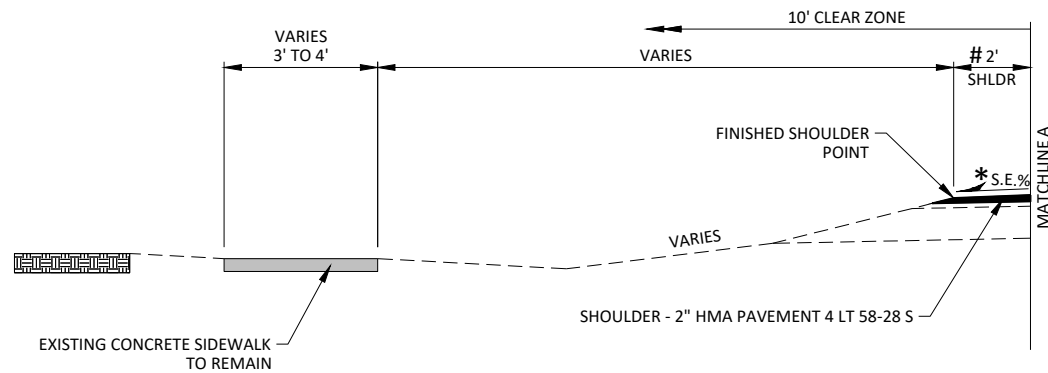
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.03 ACRES



PALMYRA  
MUNICIPAL  
AIRPORT





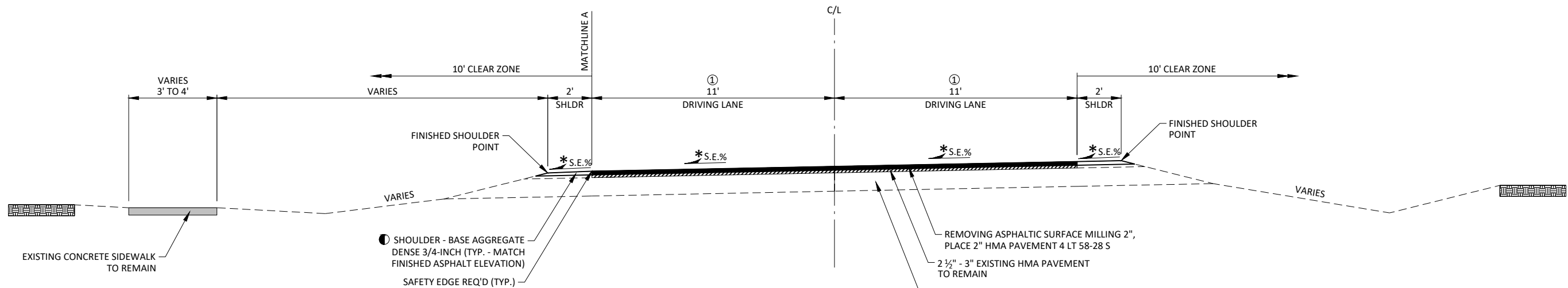
**TYPICAL FINISHED SUPERELEVATED PARTIAL SECTION**

STA. 12+65 - STA. 15+06, LT

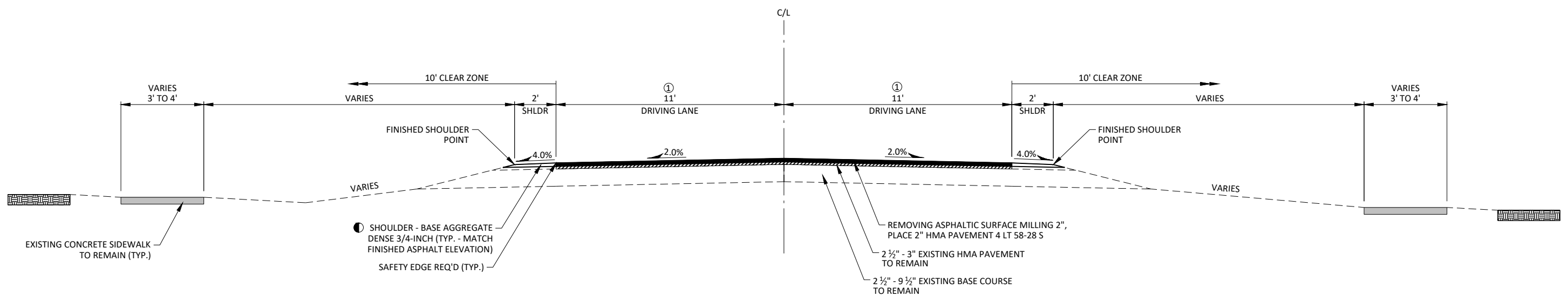
\* MATCH EXISTING SUPERELEVATION

# PREPARE FOUNDATION FOR ASPHALTIC SHOULDERS  
STA. 12+65 - STA. 15+06, LT. (SEE CONSTRUCTION DETAILS FOR ADDITIONAL INFORMATION)① INCORPORATE EXISTING SHOULDER AGGREGATE TO GREATEST EXTENT PRACTICABLE PRIOR  
TO PLACEMENT OF BASE AGGREGATE DENSE ¾-INCH

① PREPARE FOUNDATION FOR ASPHALTIC PAVING

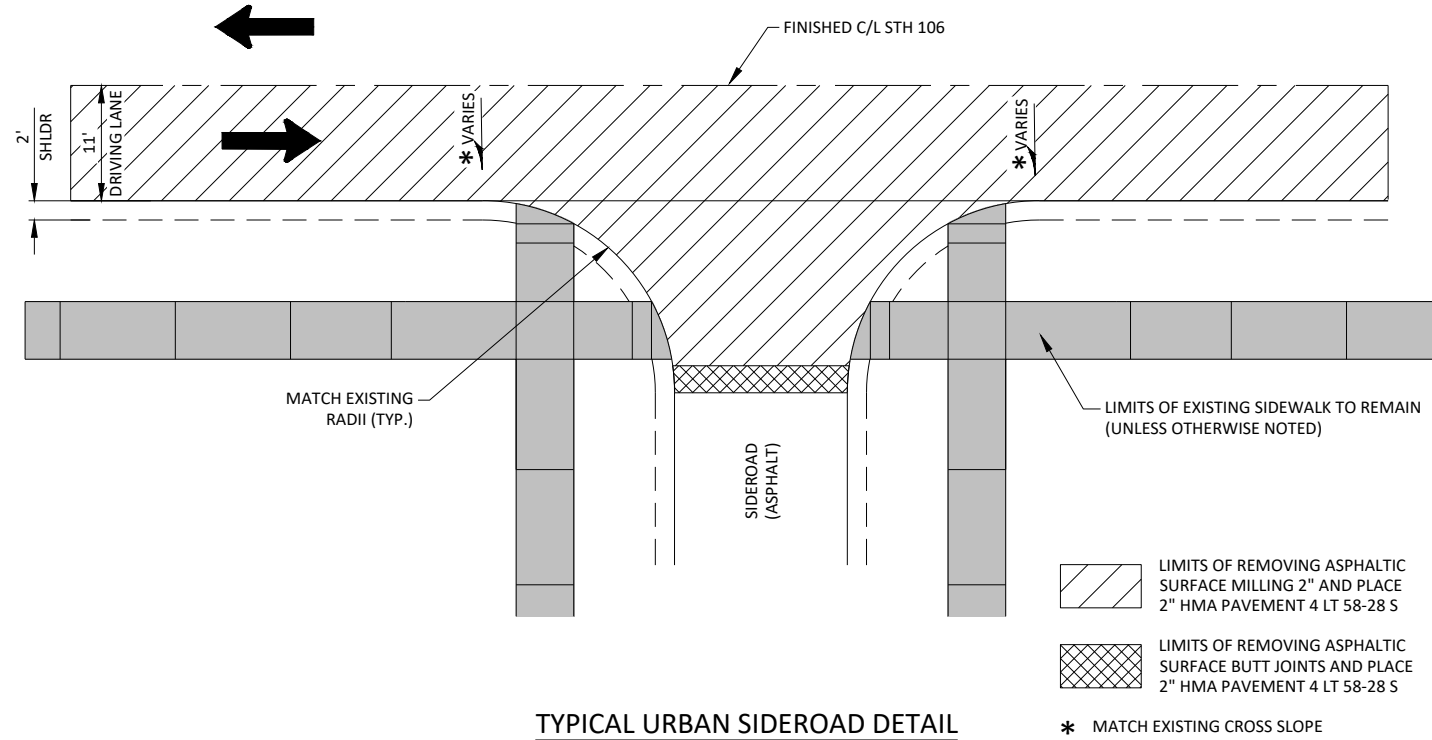
**CUT****TYPICAL FINISHED SUPERELEVATED SECTION**

STA. 8+12.19 - STA. 15+62.00

**FILL****CUT****TYPICAL FINISHED SECTION**

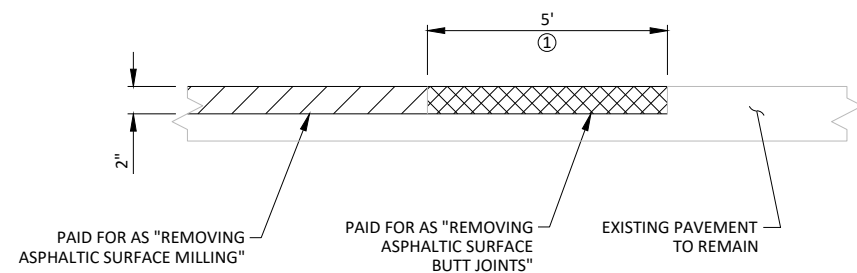
STA. 15+62.00 - STA. 27+35.69

**FILL**



TYPICAL URBAN SIDEROAD DETAIL

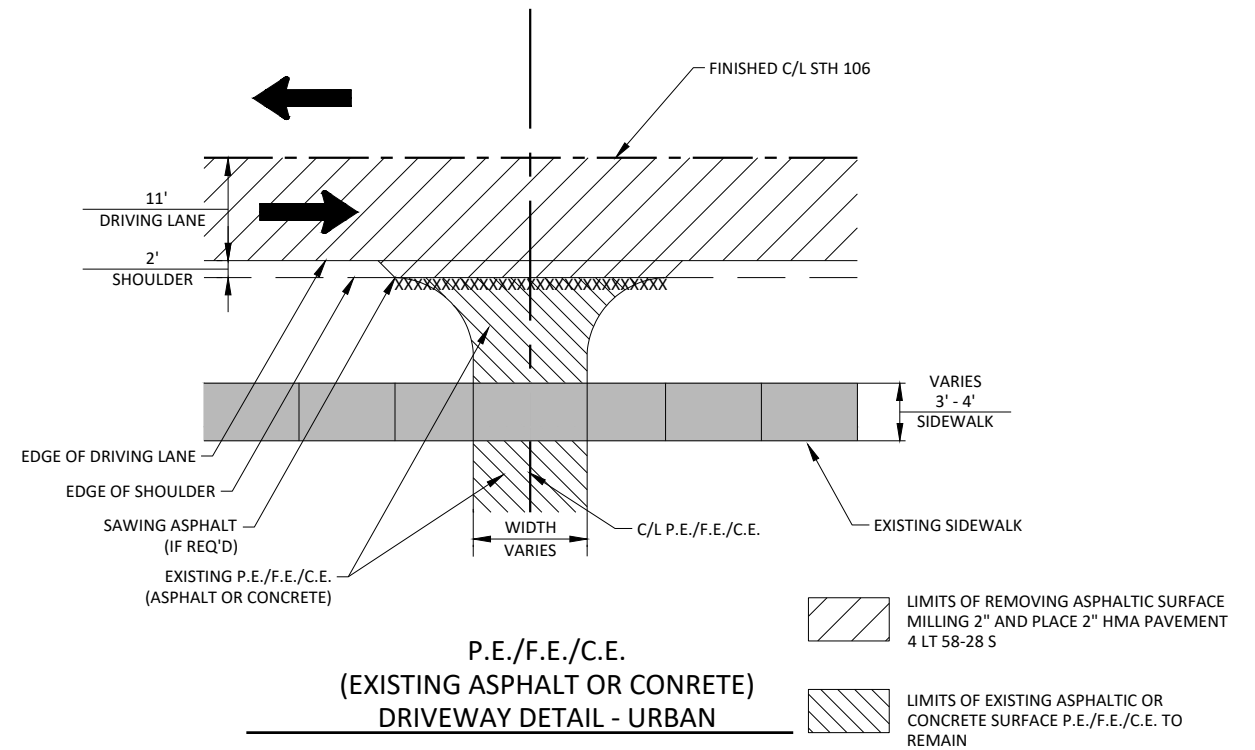
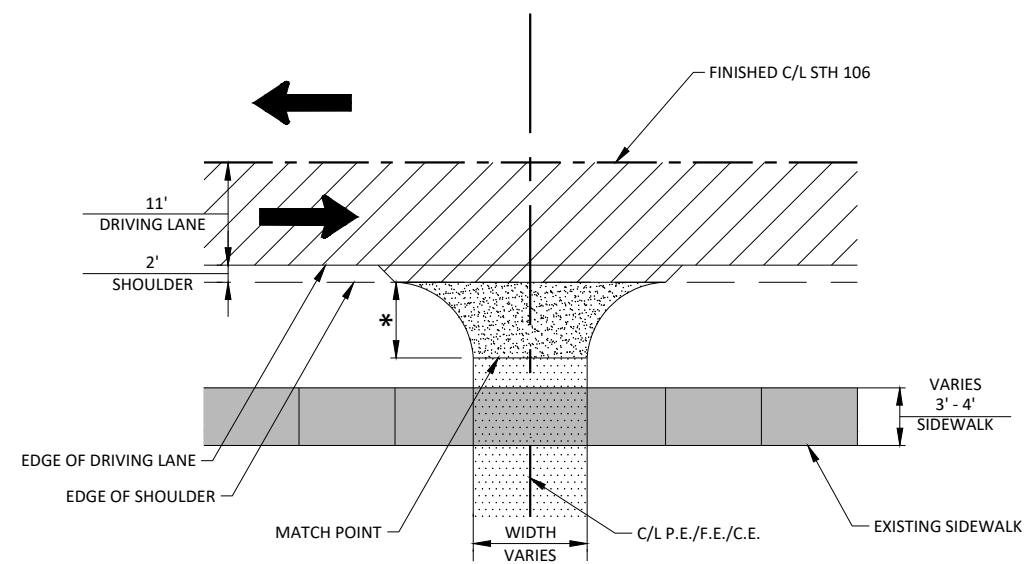
N FIRST STREET  
AIRPORT ROAD  
NORTHWEST STREET (SOUTH)  
NORTHWEST STREET (NORTH)  
W WISCONSIN STREET (SOUTH)  
W WISCONSIN STREET (NORTH)



REMOVING ASPHALTIC SURFACE BUTT JOINTS DETAIL

STA. 8+12.19  
AIRPORT ROAD  
N FIRST STREET  
NORTHWEST STREET (SOUTH)  
NORTHWEST STREET (NORTH)  
W WISCONSIN STREET (SOUTH)  
W WISCONSIN STREET (NORTH)  
STA. 27+35.69

① LIMITS OF REMOVING ASPHALTIC SURFACE BUTT JOINTS REQ'D.

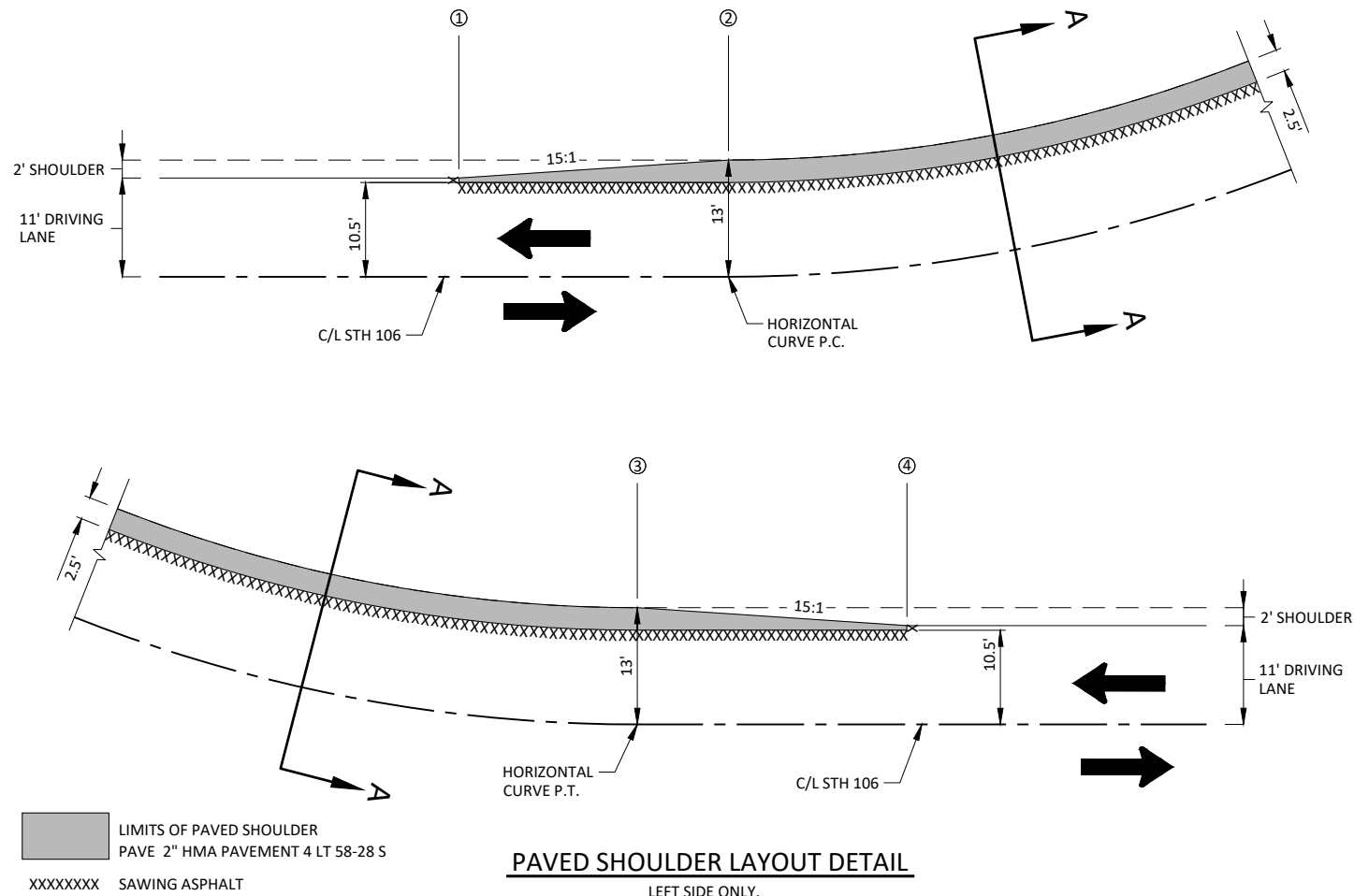
P.E./F.E./C.E.  
(EXISTING ASPHALT OR CONCRETE)  
DRIVEWAY DETAIL - URBANP.E./F.E./C.E. (EXISTING B.A.D.)  
DRIVEWAY DETAIL-URBAN

\* REPLACE IN KIND TO THE RADIUS POINTS OF EACH DRIVEWAY

LIMITS OF REMOVING ASPHALTIC SURFACE MILLING 2" AND PLACE 2" HMA PAVEMENT 4 LT 58-28 S

LIMITS OF BASE AGGREGATE DENSE 3/4-INCH P.E./F.E./C.E.

LIMITS OF EXISTING B.A.D SURFACE P.E./F.E./C.E. TO REMAIN



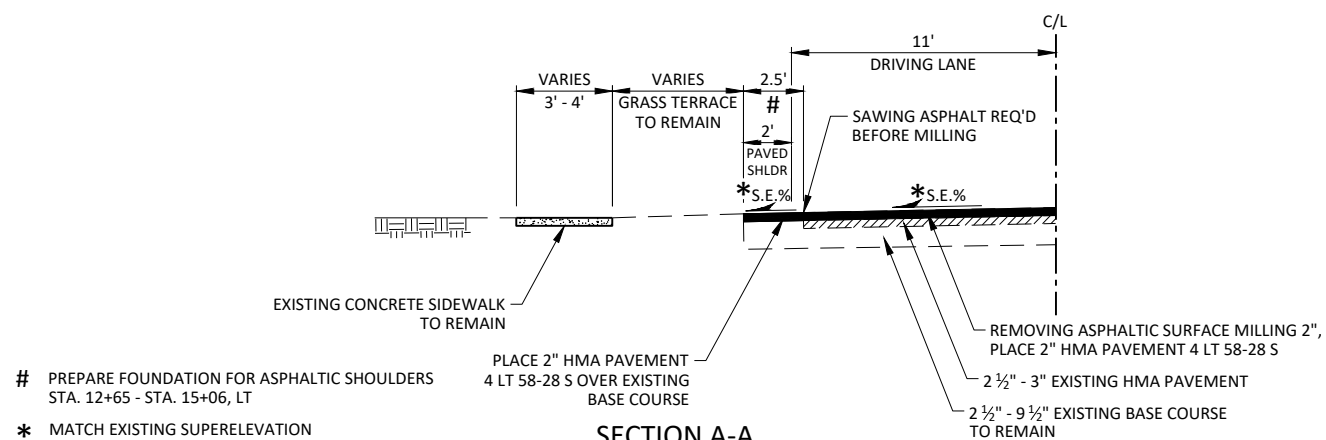
LIMITS OF PAVED SHOULDER  
PAVE 2" HMA PAVEMENT 4 LT 58-28 S

XXXXXXX SAWING ASPHALT

DIRECTION OF TRAFFIC

**PAVED SHOULDER LAYOUT DETAIL**  
LEFT SIDE ONLY.

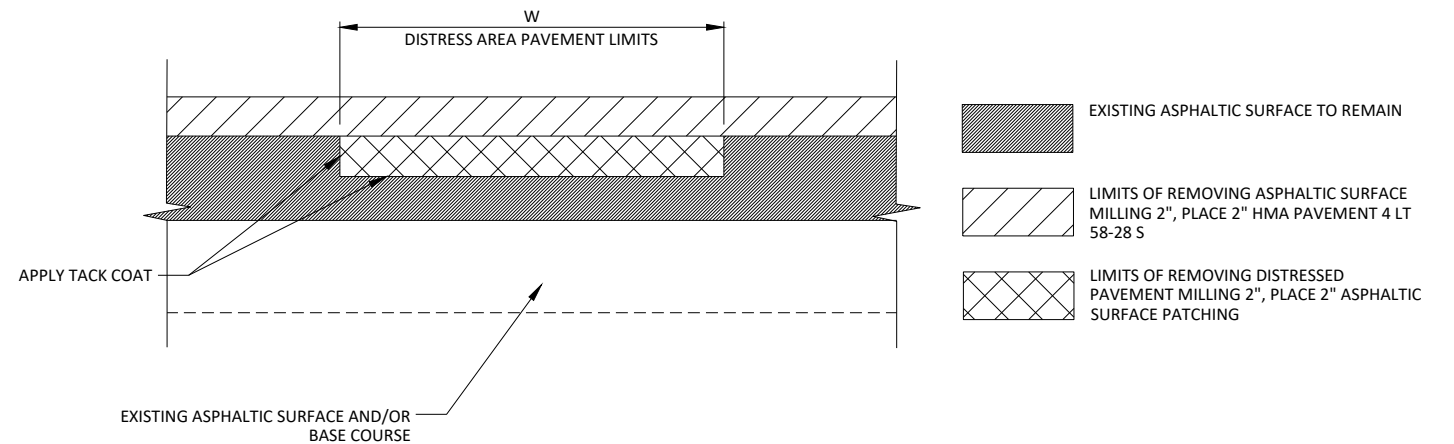
STATION-STATION	CURVE #	LOCATION	①	②	③	④
12+65 - 15+06	2	MAINLINE, LT.	12+65	12+95	14+76	15+06



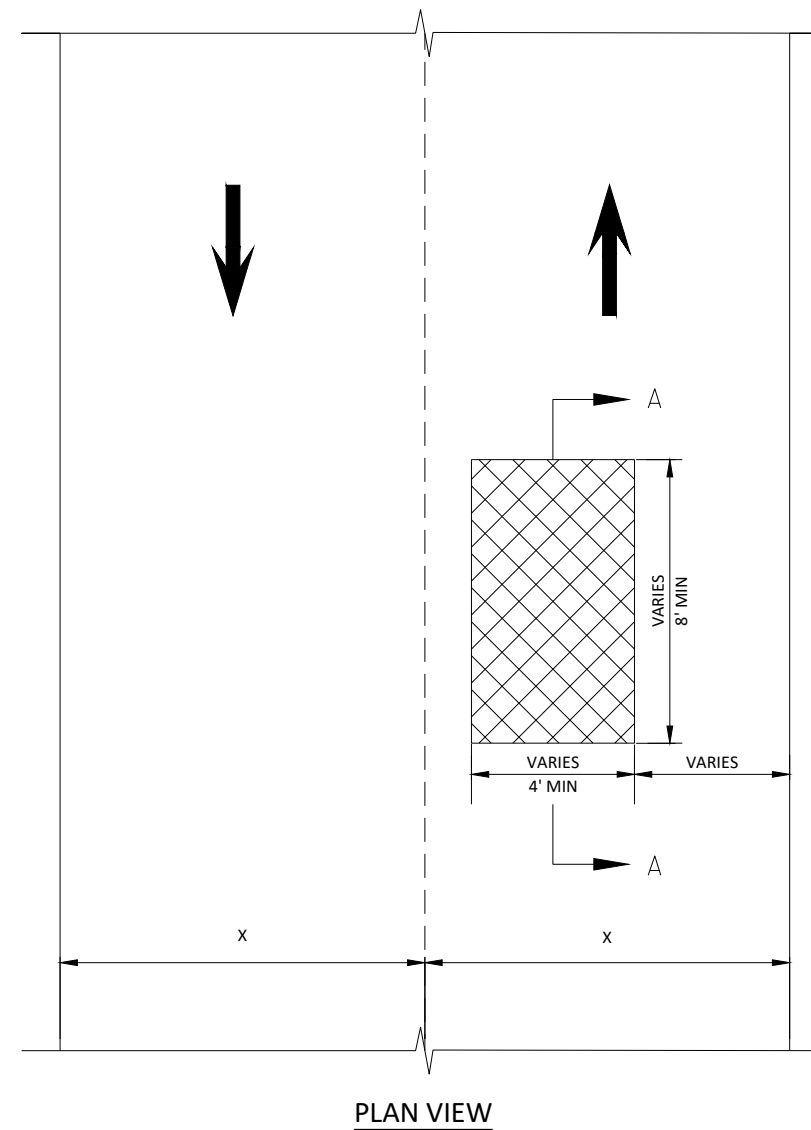
# PREPARE FOUNDATION FOR ASPHALTIC SHOULDERS  
STA. 12+65 - STA. 15+06, LT

\* MATCH EXISTING SUPERELEVATION

**SECTION A-A**



**REMOVING DISTRESS PAVEMENT MILLING**  
SECTION A-A



LIMITS OF REMOVING DISTRESSED  
PAVEMENT MILLING 2", PLACE 2" ASPHALTIC  
SURFACE PATCHING

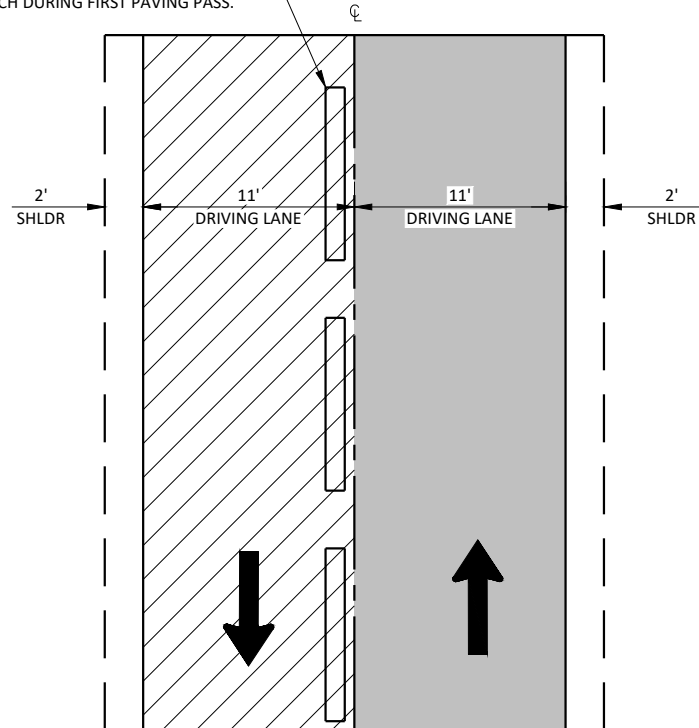
TRAVEL DIRECTION

STATION-STATION	W (FT)
8+12.19 - 8+25, RT.	11
9+10 - 9+20, LT. & RT.	22
10+00 - 10+60, RT.	6
10+60 - 11+25, LT. & RT.	24
12+50 - 13+00, RT.	4
12+65 - 12+75, LT. & RT.	14
12+65 - 12+90, LT.	4
13+25 - 13+50, LT.	6
13+50 - 13+75, LT.	11
14+15 - 14+35, LT.	11
14+25 - 14+50, RT.	11
14+50 - 15+10, LT. & RT.	22
16+60 - 16+70, LT. & RT.	22
17+55 - 17+65, LT. & RT.	11
17+65 - 17+90, LT.	11
17+80 - 18+15, RT.	11
18+15 - 18+30, RT.	6
18+60 - 19+00, RT.	11
19+25 - 19+35, LT. & RT.	22
19+55 - 19+75, LT. & RT.	22
19+95 - 20+10, RT.	11
20+10 - 20+90, RT.	6
21+10 - 21+65, LT.	6
22+30 - 22+80, RT.	6
23+40 - 23+55, RT.	11
26+10 - 26+30, LT. & RT.	22
26+70 - 26+80, LT.	11
27+20 - 27+35.69, LT. & RT.	22

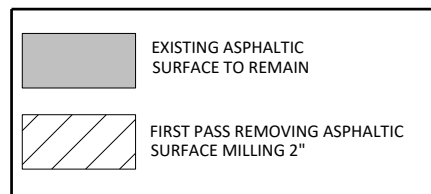
EXACT LOCATION AND LIMITS OF REMOVING DISTRESSED  
PAVEMENT MILLING TO BE DETERMINED BY THE  
ENGINEER IN THE FIELD



TEMPORARY MARKING LINE PAINT 6-INCH (SINGLE LINE 4' SKIPS) TO BE PLACED ON MILLED SURFACE SAME DAY AS FIRST MILLING PASS OPERATION. PLACE PAINT SO AS NOT TO BE COVERED UP BY PAVING NOTCH DURING FIRST PAVING PASS.

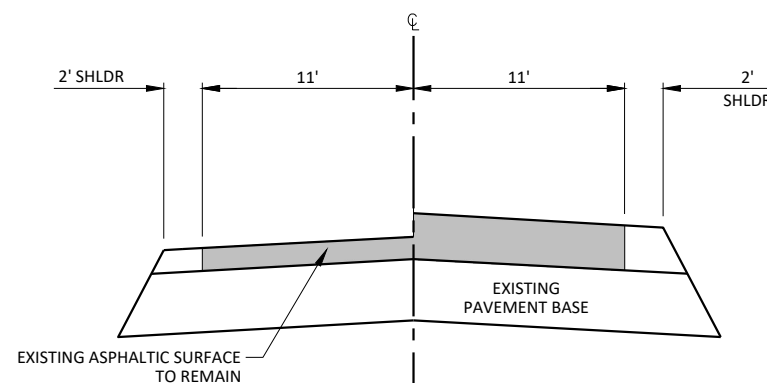


PLAN VIEW



NOTES:  
PLACE "NO PASSING ZONE (R4-1, 24"x30")", "UNEVEN LANES" (W8-11, 36"x36")", "LOW SHOULDER" (W8-9, 36"x36")", OR "SHOULDER DROPOFF" (W8-9A, 36"x36") AT THE BEGINNING AND END OF PROJECT WHEN APPLICABLE.

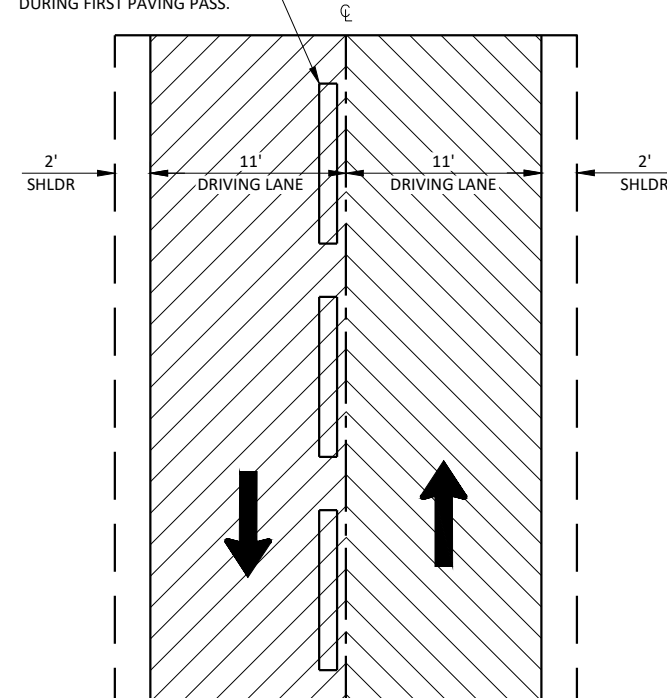
REFER TO STANDARD DETAIL DRAWINGS "TEMPORARY LONGITUDINAL PAVEMENT MARKING", "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION", AND "TRAFFIC CONTROL, DROP-OFF SIGNING" FOR ADDITIONAL INFORMATION



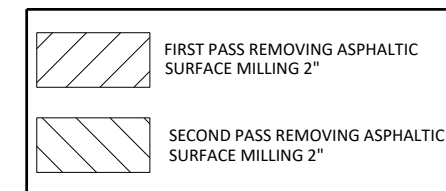
CROSS SECTION VIEW

FIRST MILLING PASS DETAIL

TEMPORARY MARKING LINE PAINT 6-INCH (SINGLE LINE 4' SKIPS) PLACED AFTER FIRST MILLING PASS TO REMAIN FOR SECOND MILLING PASS AND FIRST PAVING PASS. PLACE PAINT SO AS NOT TO BE COVERED UP BY PAVING NOTCH DURING FIRST PAVING PASS.

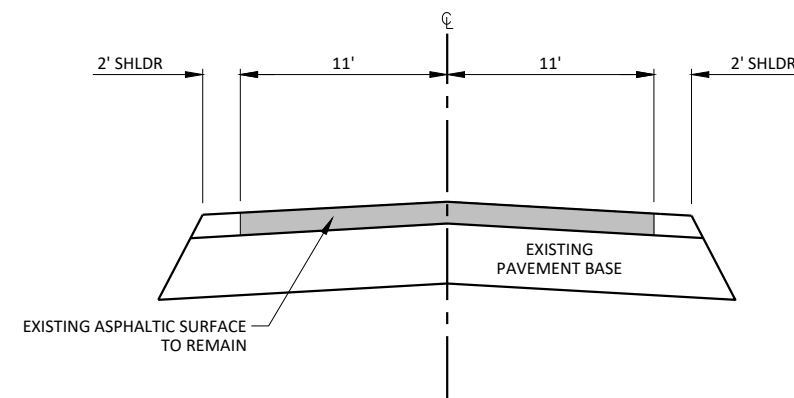


PLAN VIEW



NOTES:  
PLACE "NO PASSING ZONE (R4-1, 24"x30")", "UNEVEN LANES" (W8-11, 36"x36")", "LOW SHOULDER" (W8-9, 36"x36")", OR "SHOULDER DROPOFF" (W8-9A, 36"x36") AT THE BEGINNING AND END OF PROJECT WHEN APPLICABLE.

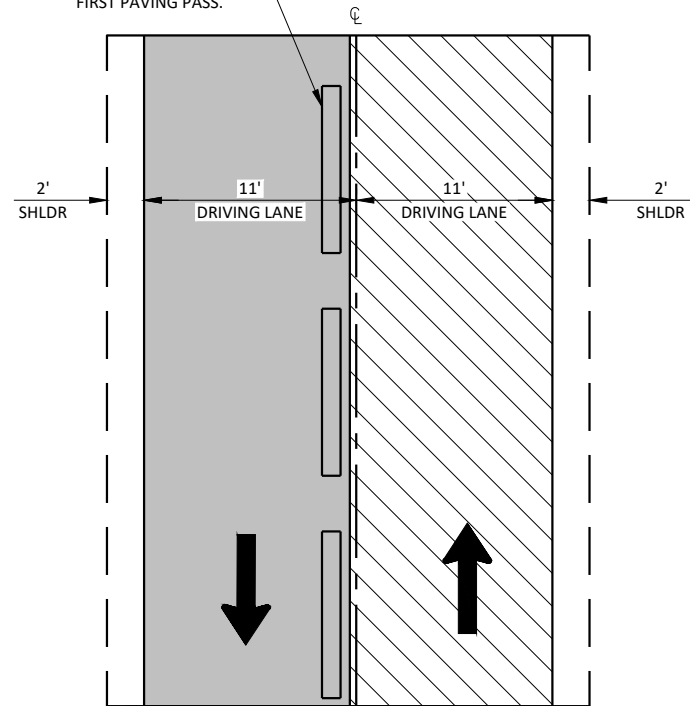
REFER TO STANDARD DETAIL DRAWINGS "TEMPORARY LONGITUDINAL PAVEMENT MARKING", "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION", AND "TRAFFIC CONTROL, DROP-OFF SIGNING" FOR ADDITIONAL INFORMATION



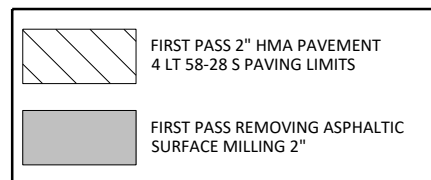
CROSS SECTION VIEW

SECOND MILLING PASS DETAIL

TEMPORARY MARKING LINE PAINT 6-INCH (SINGLE LINE 4' SKIPS) PLACED AFTER FIRST MILLING PASS TO REMAIN FOR SECOND MILLING PASS AND FIRST PAVING PASS. PLACE PAINT SO AS NOT TO BE COVERED UP BY PAVING NOTCH DURING FIRST PAVING PASS.

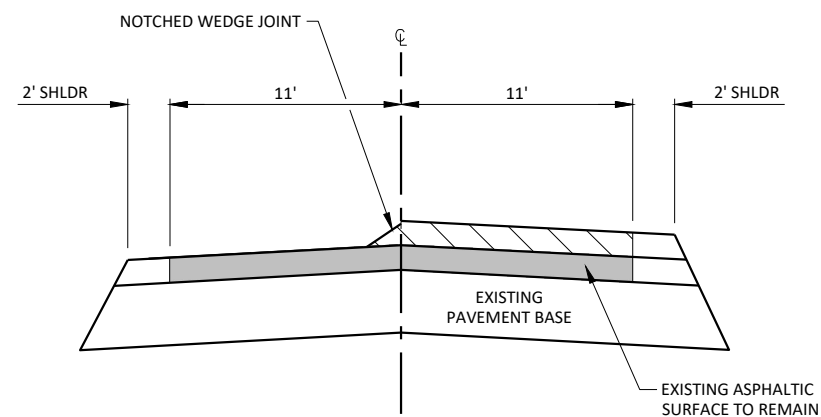


PLAN VIEW



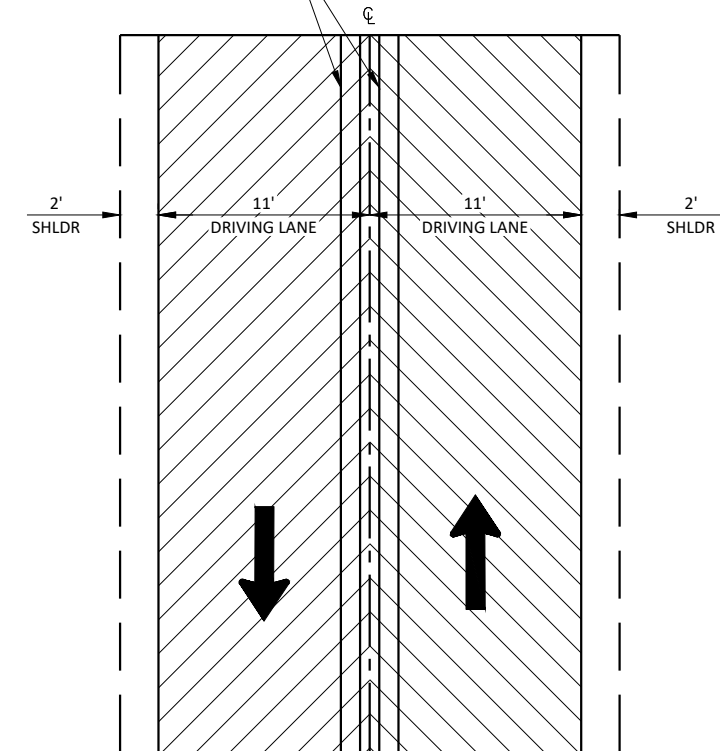
NOTES:  
PLACE "NO PASSING ZONE (R4-1, 24"x30")", "UNEVEN LANES" (W8-11, 36"x36")", "LOW SHOULDER" (W8-9, 36"x36")", OR "SHOULDER DROPOFF" (W8-9A, 36"x36") AT THE BEGINNING AND END OF PROJECT WHEN APPLICABLE.

REFER TO STANDARD DETAIL DRAWINGS "TEMPORARY LONGITUDINAL PAVEMENT MARKING", "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION", AND "TRAFFIC CONTROL, DROP-OFF SIGNING" FOR ADDITIONAL INFORMATION

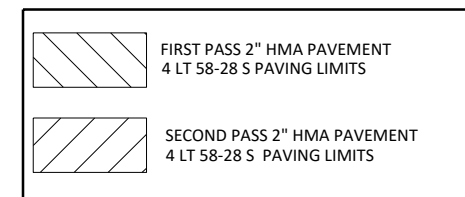


CROSS SECTION VIEW  
FIRST PAVING PASS DETAIL

TEMPORARY MARKING LINE PAINT 6-INCH (DOUBLE YELLOW) TO BE PLACED SAME DAY AS SECOND PAVING PASS OPERATION.

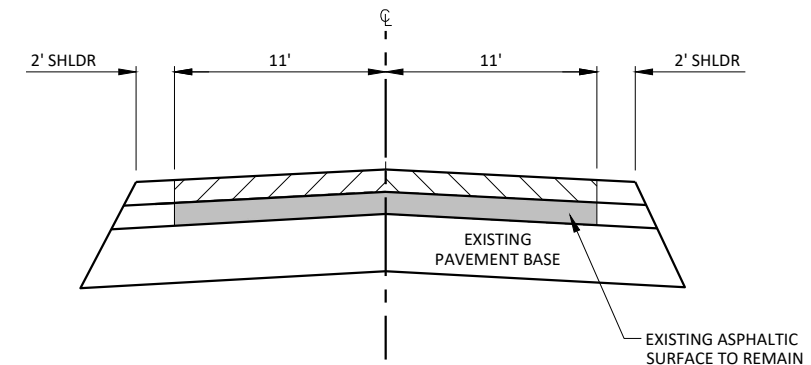


PLAN VIEW



NOTES:  
PLACE MARKING LINE GROOVED WET REF EPOXY 6-INCH AFTER THE MINIMUM 48-HOUR WAITING PERIOD FOLLOWING THE SECOND PAVING PASS.

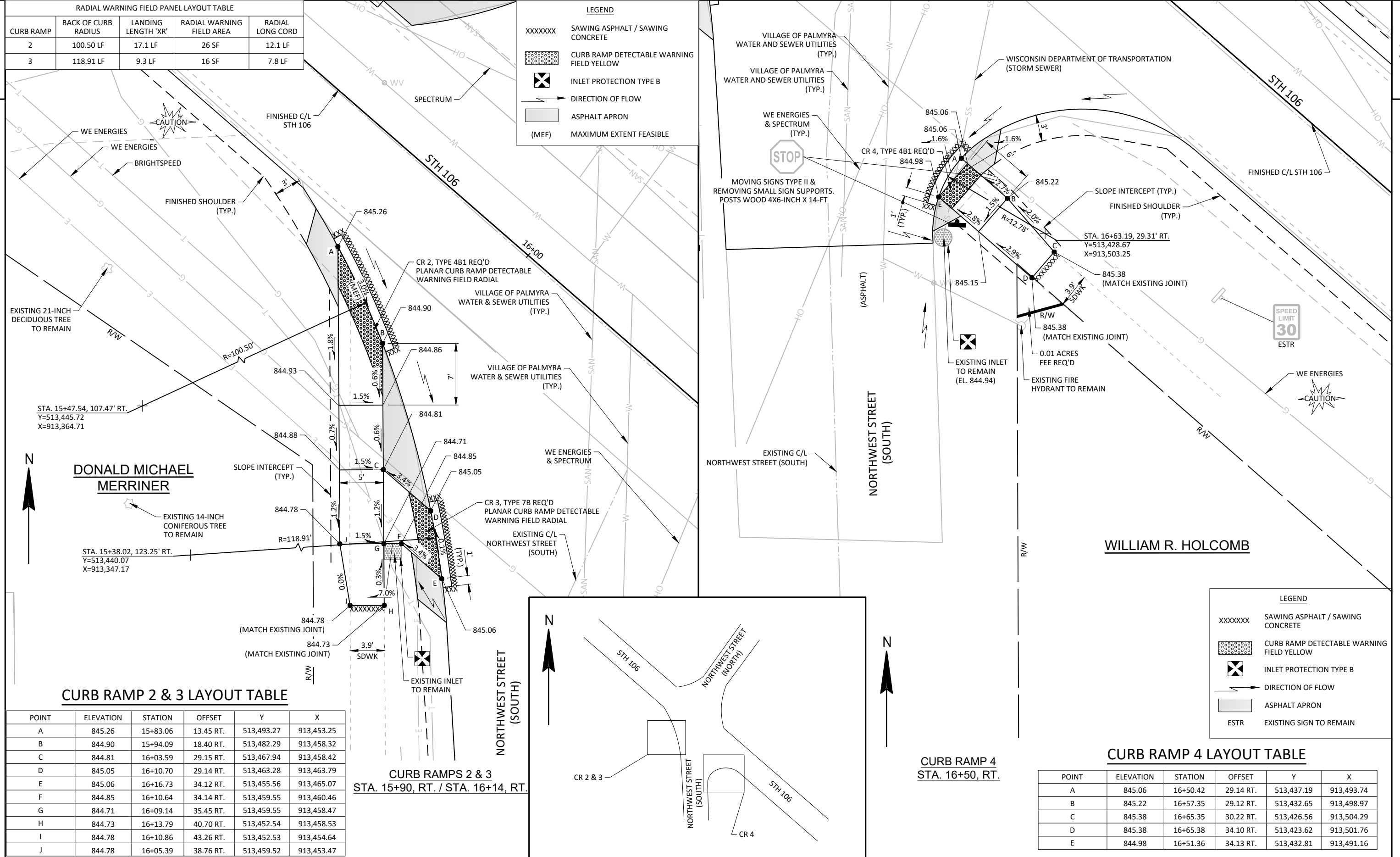
REFER TO STANDARD DETAIL DRAWINGS "TEMPORARY LONGITUDINAL PAVEMENT MARKINGS", AND "PERMANENT LONGITUDINAL PAVEMENT MARKINGS" FOR ADDITIONAL INFORMATION



CROSS SECTION VIEW  
SECOND PAVING PASS DETAIL

POINT	ELEVATION	STATION	OFFSET	Y	X
A	844.76	10+76.75	22.95' LT.	513,957.53	913,320.06
B	843.95	10+80.75	43.95 LT.	513,959.26	913,341.46
C	843.60	10+82.27	49.61 LT.	513,959.13	913,347.39
D	843.60	10+85.45	48.90 LT.	513,955.36	913,347.70
E	844.02	10+84.94	42.48 LT.	513,954.23	913,341.36
F	843.90	10+89.17	41.09 LT.	513,949.21	913,341.26
G	843.85	10+93.36	39.20 LT.	513,944.13	913,340.60
H	843.85	10+92.50	35.57 LT.	513,944.20	913,336.84
I	843.97	10+88.40	36.14 LT.	513,948.80	913,336.25
J	844.09	10+84.11	37.55 LT.	513,953.83	913,336.35
K	844.85	10+80.93	20.00 LT.	513,952.38	913,318.51

RADIAL WARNING FIELD PANEL LAYOUT TABLE				
CURB RAMP	BACK OF CURB RADIUS	LANDING LENGTH 'XR'	RADIAL WARNING FIELD AREA	RADIAL LONG CORD
2	100.50 LF	17.1 LF	26 SF	12.1 LF
3	118.91 LF	9.3 LF	16 SF	7.8 LF



POINT	ELEVATION	STATION	OFFSET	Y	X
A	845.26	15+83.06	13.45 RT.	513,493.27	913,453.25
B	844.90	15+94.09	18.40 RT.	513,482.29	913,458.32
C	844.81	16+03.59	29.15 RT.	513,467.94	913,458.42
D	845.05	16+10.70	29.14 RT.	513,463.28	913,463.79
E	845.06	16+16.73	34.12 RT.	513,455.56	913,465.07
F	844.85	16+10.64	34.14 RT.	513,459.55	913,460.46
G	844.71	16+09.14	35.45 RT.	513,459.55	913,458.47
H	844.73	16+13.79	40.70 RT.	513,452.54	913,458.53
I	844.78	16+10.86	43.26 RT.	513,452.53	913,454.64
J	844.78	16+05.39	38.76 RT.	513,459.52	913,453.47

POINT	ELEVATION	STATION	OFFSET	Y	X
A	845.06	16+50.42	29.14 RT.	513,437.19	913,493.74
B	845.22	16+57.35	29.12 RT.	513,432.65	913,498.97
C	845.38	16+65.35	30.22 RT.	513,426.56	913,504.29
D	845.38	16+65.38	34.10 RT.	513,423.62	913,501.76
E	844.98	16+51.36	34.13 RT.	513,432.81	913,491.16



LEGEND

XXXXXX

SAWING ASPHALT / SAWING CONCRETE

CURB RAMP DETECTABLE WARNING FIELD YELLOW

DIRECTION OF FLOW

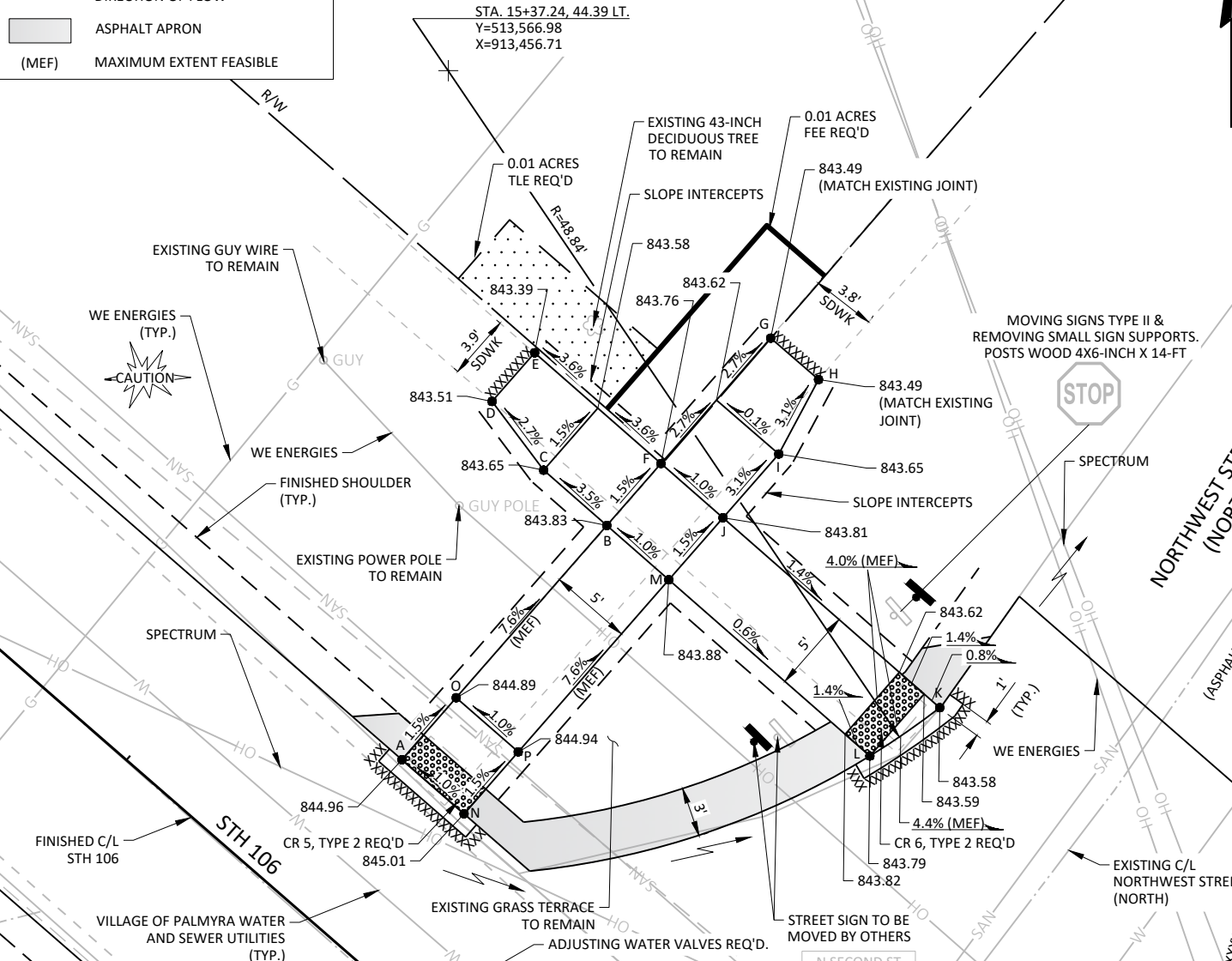
ASPHALT APRON

(MEF)

MAXIMUM EXTENT FEASIBLE

BRIAN A. REMMEL

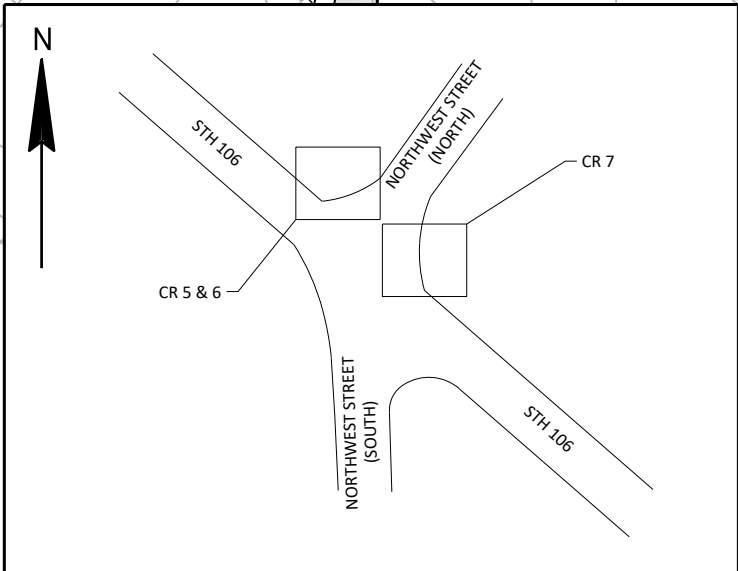
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CURB RAMP 5 & 6 LAYOUT TABLE

POINT	ELEVATION	STATION	OFFSET	Y	X
A	844.96	15+62.60	11.00 LT.	513,525.14	913,453.89
B	843.83	15+62.64	29.89 LT	513,539.36	913,466.33
C	843.65	15+57.54	29.90 LT	513,542.72	913,462.49
D	843.51	15+52.44	31.00 LT	513,546.89	913,459.37
E	843.39	15+52.45	34.92 LT	513,549.83	913,461.93
F	843.76	15+62.65	34.89 LT	513,543.12	913,469.62
G	843.49	15+62.66	45.00 LT	513,550.73	913,476.28
H	843.49	15+66.51	45.01 LT	513,548.22	913,479.18
I	843.65	15+67.66	40.00 LT	513,543.70	913,476.77
J	843.81	15+67.65	34.88 LT	513,539.83	913,473.38
K	843.58	15+85.14	34.83 LT	513,528.30	913,486.54
L	843.79	15+83.87	29.83 LT	513,525.37	913,482.30
M	843.88	15+67.64	29.88 LT	513,536.07	913,470.09
N	845.01	16+67.60	11.00 LT	513,521.86	913,457.66
O	844.89	15+62.61	16.00 LT	513,528.90	913,457.18
P	844.94	15+67.61	16.00 LT	513,525.62	913,460.95

CURB RAMPS 5 & 6  
STA. 15+65, LT. / STA. 15+84, LT.



CURB RAMP 7  
STA. 16+13, LT.

CURB RAMP 7 LAYOUT TABLE

POINT	ELEVATION	STATION	OFFSET	Y	X
A	843.72	16+09.85	35.26 LT.	513,512.39	913,505.45
B	843.77	16+27.93	35.09 LT.	513,500.39	913,518.97
C	843.84	16+27.91	31.17 LT.	513,497.44	913,516.38
D	843.73	16+23.61	30.13 LT.	513,499.49	913,512.46
E	843.88	16+12.61	30.23 LT.	513,506.79	913,504.23

LEGEND

XXXXXX

SAWING ASPHALT / SAWING CONCRETE

CURB RAMP DETECTABLE WARNING FIELD YELLOW

INLET PROTECTION TYPE B

DIRECTION OF FLOW

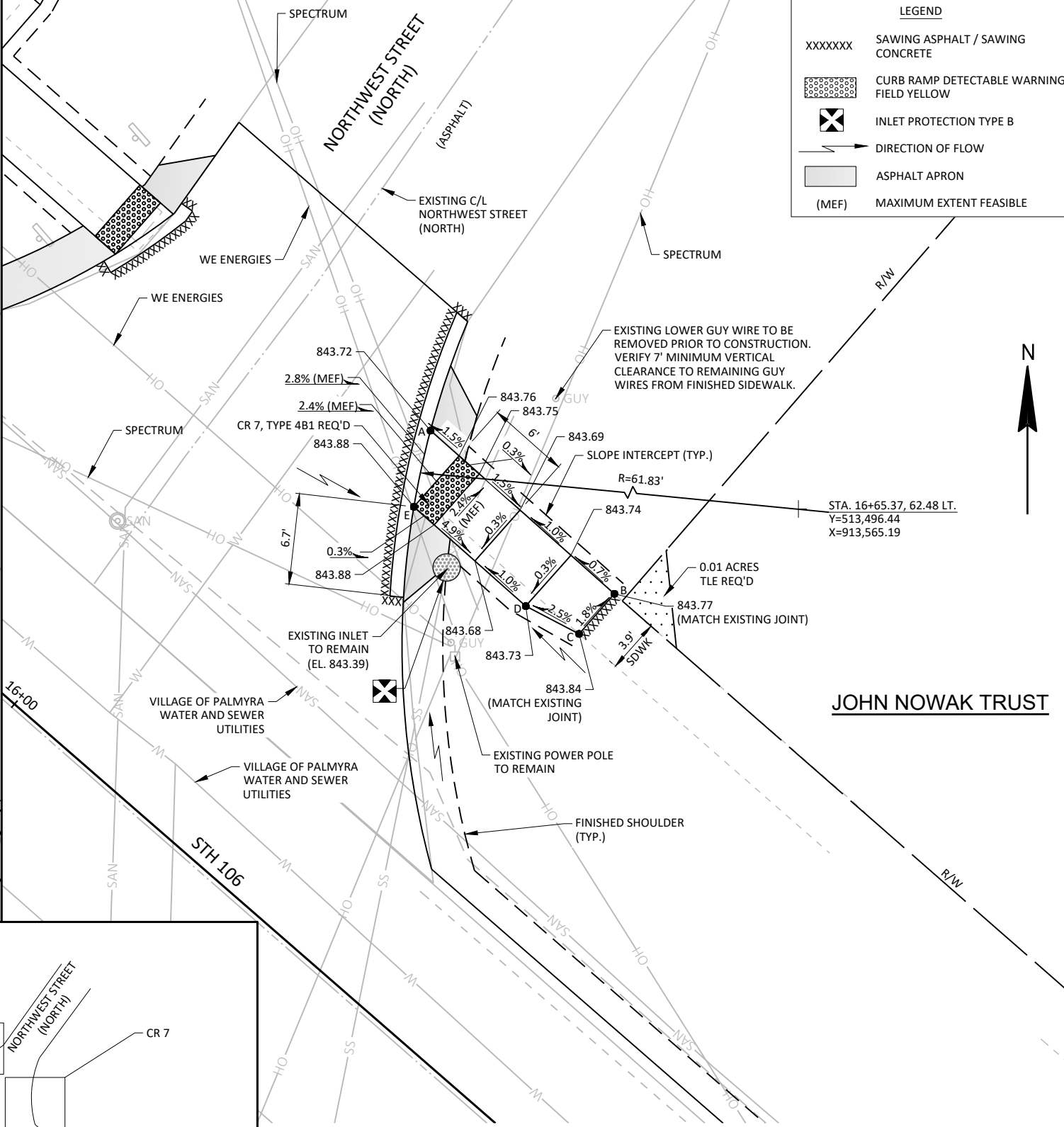
ASPHALT APRON

(MEF)

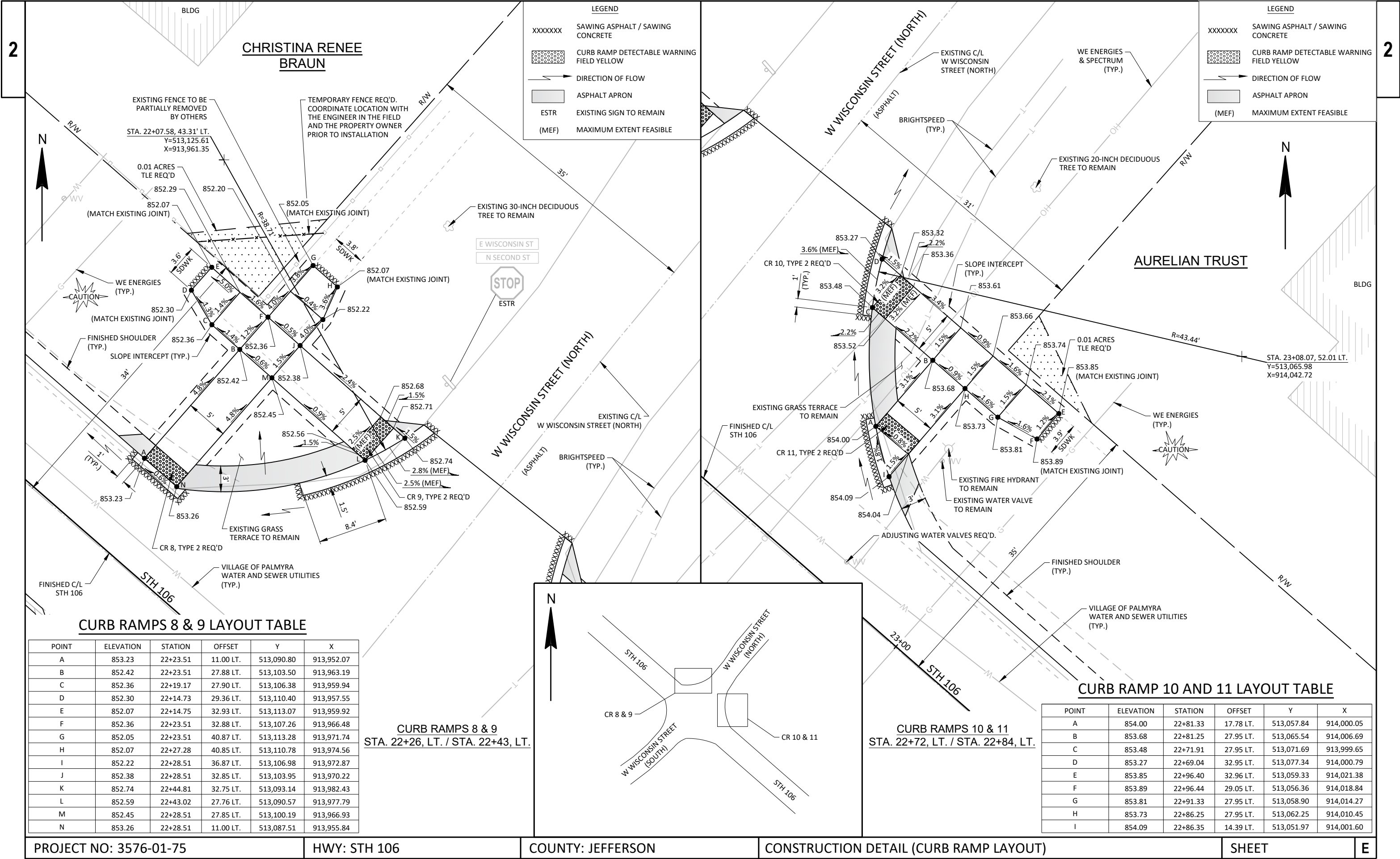
MAXIMUM EXTENT FEASIBLE

JOHN NOWAK TRUST

N







CURB RAMP 8 & 9 LAYOUT TABLE

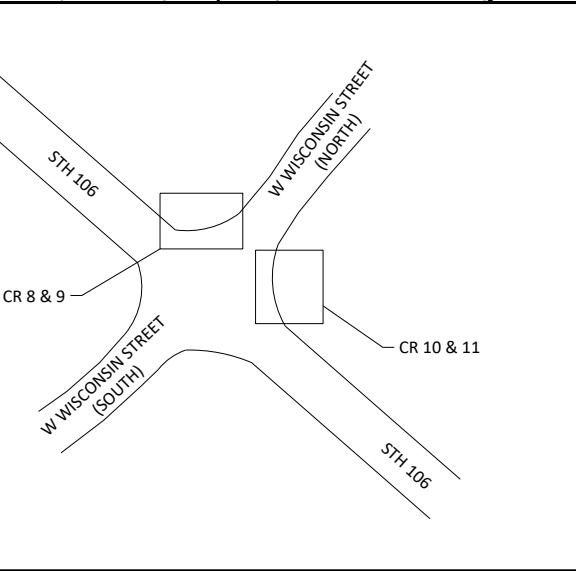
POINT	ELEVATION	STATION	OFFSET	Y	X
A	853.23	22+23.51	11.00 LT.	513,090.80	913,952.07
B	852.42	22+23.51	27.88 LT.	513,103.50	913,963.19
C	852.36	22+19.17	27.90 LT.	513,106.38	913,959.94
D	852.30	22+14.73	29.36 LT.	513,110.40	913,957.55
E	852.07	22+14.75	32.93 LT.	513,113.07	913,959.92
F	852.36	22+23.51	32.88 LT.	513,107.26	913,966.48
G	852.05	22+23.51	40.87 LT.	513,113.28	913,971.74
H	852.07	22+27.28	40.85 LT.	513,110.78	913,974.56
I	852.22	22+28.51	36.87 LT.	513,106.98	913,972.87
J	852.38	22+28.51	32.85 LT.	513,103.95	913,970.22
K	852.74	22+44.81	32.75 LT.	513,093.14	913,982.43
L	852.59	22+43.02	27.76 LT.	513,090.57	913,977.79
M	852.45	22+28.51	27.85 LT.	513,100.19	913,966.93
N	853.26	22+28.51	11.00 LT.	513,087.51	913,955.84

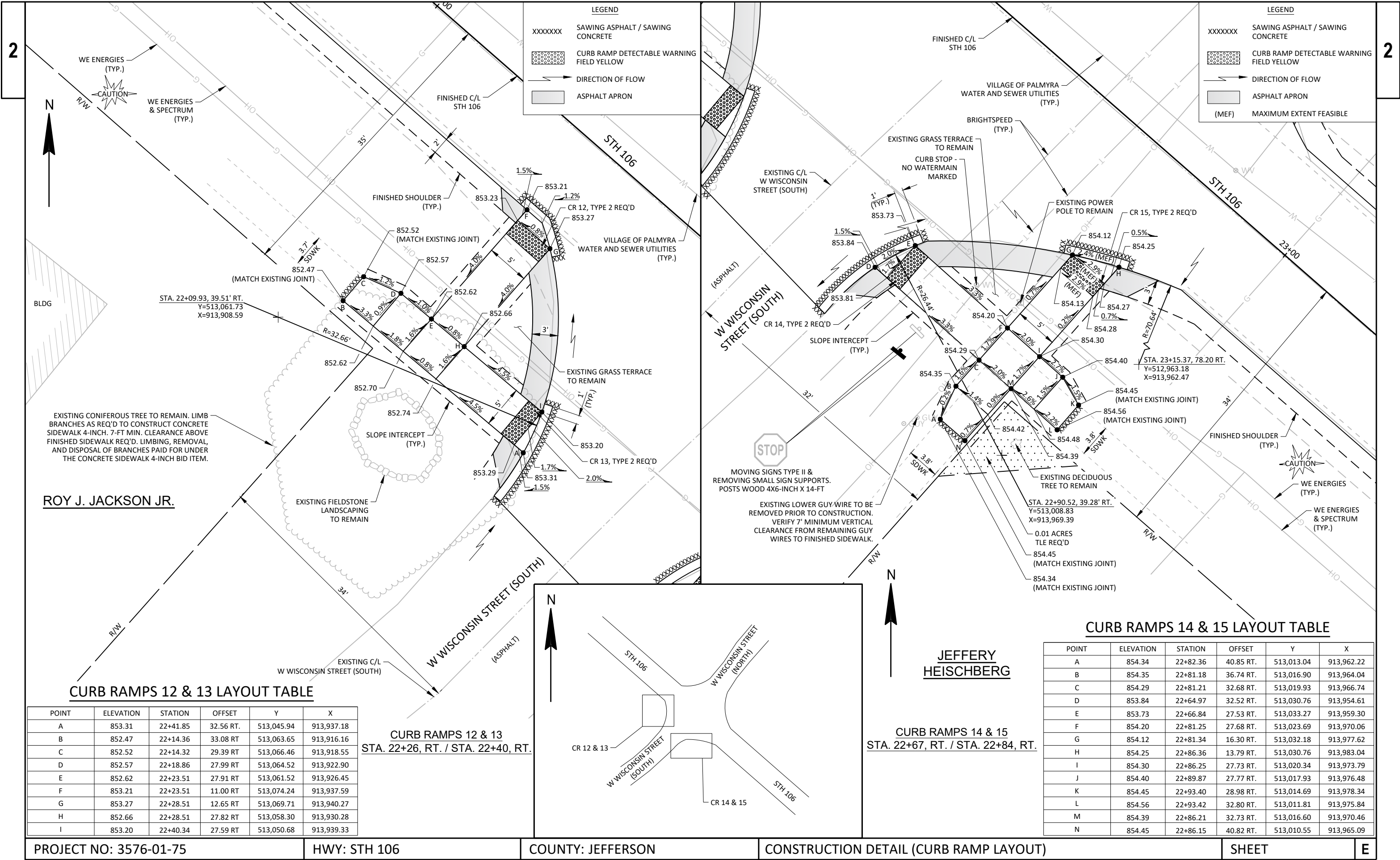
CURB RAMP 8 & 9  
STA. 22+26, LT. / STA. 22+43, LT.

CURB RAMP 10 AND 11 LAYOUT TABLE

POINT	ELEVATION	STATION	OFFSET	Y	X
A	854.00	22+81.33	17.78 LT.	513,057.84	914,000.05
B	853.68	22+81.25	27.95 LT.	513,065.54	914,006.69
C	853.48	22+71.91	27.95 LT.	513,071.69	913,999.65
D	853.27	22+69.04	32.95 LT.	513,077.34	914,000.79
E	853.85	22+96.40	32.96 LT.	513,059.33	914,021.38
F	853.89	22+96.44	29.05 LT.	513,056.36	914,018.84
G	853.81	22+91.33	27.95 LT.	513,058.90	914,014.27
H	853.73	22+86.25	27.95 LT.	513,062.25	914,010.45
I	854.09	22+86.35	14.39 LT.	513,051.97	914,001.60

CURB RAMP 10 & 11  
STA. 22+72, LT. / STA. 22+84, LT.





ROY J. JACKSON JR.

JEFFERY HEISCHBERG

CURB RAMPS 12 & 13 LAYOUT TABLE

POINT	ELEVATION	STATION	OFFSET	Y	X
A	853.31	22+41.85	32.56 RT.	513,045.94	913,937.18
B	852.47	22+14.36	33.08 RT.	513,063.65	913,916.16
C	852.52	22+14.32	29.39 RT.	513,066.46	913,918.55
D	852.57	22+18.86	27.99 RT.	513,064.52	913,922.90
E	852.62	22+23.51	27.91 RT.	513,061.52	913,926.45
F	853.21	22+23.51	11.00 RT.	513,074.24	913,937.59
G	853.27	22+28.51	12.65 RT.	513,069.71	913,940.27
H	852.66	22+28.51	27.82 RT.	513,058.30	913,930.28
I	853.20	22+40.34	27.59 RT.	513,050.68	913,939.33

CURB RAMPS 12 & 13  
STA. 22+26, RT. / STA. 22+40, RT.

CURB RAMPS 14 & 15 LAYOUT TABLE

POINT	ELEVATION	STATION	OFFSET	Y	X
A	854.34	22+82.36	40.85 RT.	513,013.04	913,962.22
B	854.35	22+81.18	36.74 RT.	513,016.90	913,964.04
C	854.29	22+81.21	32.68 RT.	513,019.93	913,966.74
D	853.84	22+64.97	32.52 RT.	513,030.76	913,954.61
E	853.73	22+66.84	27.53 RT.	513,033.27	913,959.30
F	854.20	22+81.25	27.68 RT.	513,023.69	913,970.06
G	854.12	22+81.34	16.30 RT.	513,032.18	913,977.62
H	854.25	22+86.36	13.79 RT.	513,030.76	913,983.04
I	854.30	22+86.25	27.73 RT.	513,020.34	913,973.79
J	854.40	22+89.87	27.77 RT.	513,017.93	913,976.48
K	854.45	22+93.40	28.98 RT.	513,014.69	913,978.34
L	854.56	22+93.42	32.80 RT.	513,011.81	913,975.84
M	854.39	22+86.21	32.73 RT.	513,016.60	913,970.46
N	854.45	22+86.15	40.82 RT.	513,010.55	913,965.09

CURB RAMPS 14 & 15  
STA. 22+67, RT. / STA. 22+84, RT.



NOTES

TRAFFIC CONTROL SHOWN FOR CORNER CURB RAMPS WORK ZONE WHERE SDD TRAFFIC CONTROL PEDESTRIAN ACCOMMODATION IS NOT ADEQUATE. FOR ALL OTHER CURB RAMP WORK ZONES FOLLOW THE SDD AND UTILIZE TEMPORARY CROSSWALK AS SHOWN IN THE DETAIL.

LIMIT SIDEWALK CLOSURES TO ONE INTERSECTION QUADRANT AT A TIME TO MINIMIZE PEDESTRIAN DISRUPTION. ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

PEDESTRIAN PATH AREA TO BE EXISTING CONCRETE SIDEWALK OR APPROVED TEMPORARY PEDESTRIAN SURFACE.

LOCATION OF TEMPORARY CROSSWALK TO BE DETERMINED BY THE ENGINEER. DO NOT LOCATE IN EXISTING DRIVEWAY.

LOCATION OF TRAFFIC CONTROL DEVICES TO BE DETERMINED BY THE ENGINEER.

TEMPORARY CROSSWALKS SHALL BE MOVED AS NEEDED TO PROVIDE CONTINUOUS PEDESTRIAN ACCESS THROUGHOUT THE PROJECT.

WHEN CLOSING OR RELOCATING CROSSWALKS OR SIDEWALKS, PROVIDE DETECTABLE TEMPORARY FACILITIES AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH EXISTING PEDESTRIAN FACILITIES.

TEMPORARY TRAFFIC CONTROL DEVICES FOR PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL VEHICULAR TRAFFIC. STAGE WORK AS NECESSARY, TO PROVIDE A TEMPORARY PEDESTRIAN ACCESS ROUTE AT ALL TIMES. FOR ROADWAYS WITH NO AVAILABLE DETOURS, MAINTAIN ONE OPEN SIDEWALK AT ALL TIMES.

"WO" SIGN IS THE SAME AS "W" SIGN, EXCEPT THE BACKGROUND IS ORANGE.









FOR NIGHTTIME CLOSURE, USE TYPE "A" FLASHING WARNING LIGHTS ON BARRICADES AND SUPPORTING SIGNS. USE TYPE "C" STEADY BURN LIGHTS ON CHANNELIZING DEVICES SEPARATING THE WORK AREA FROM VEHICULAR TRAFFIC.

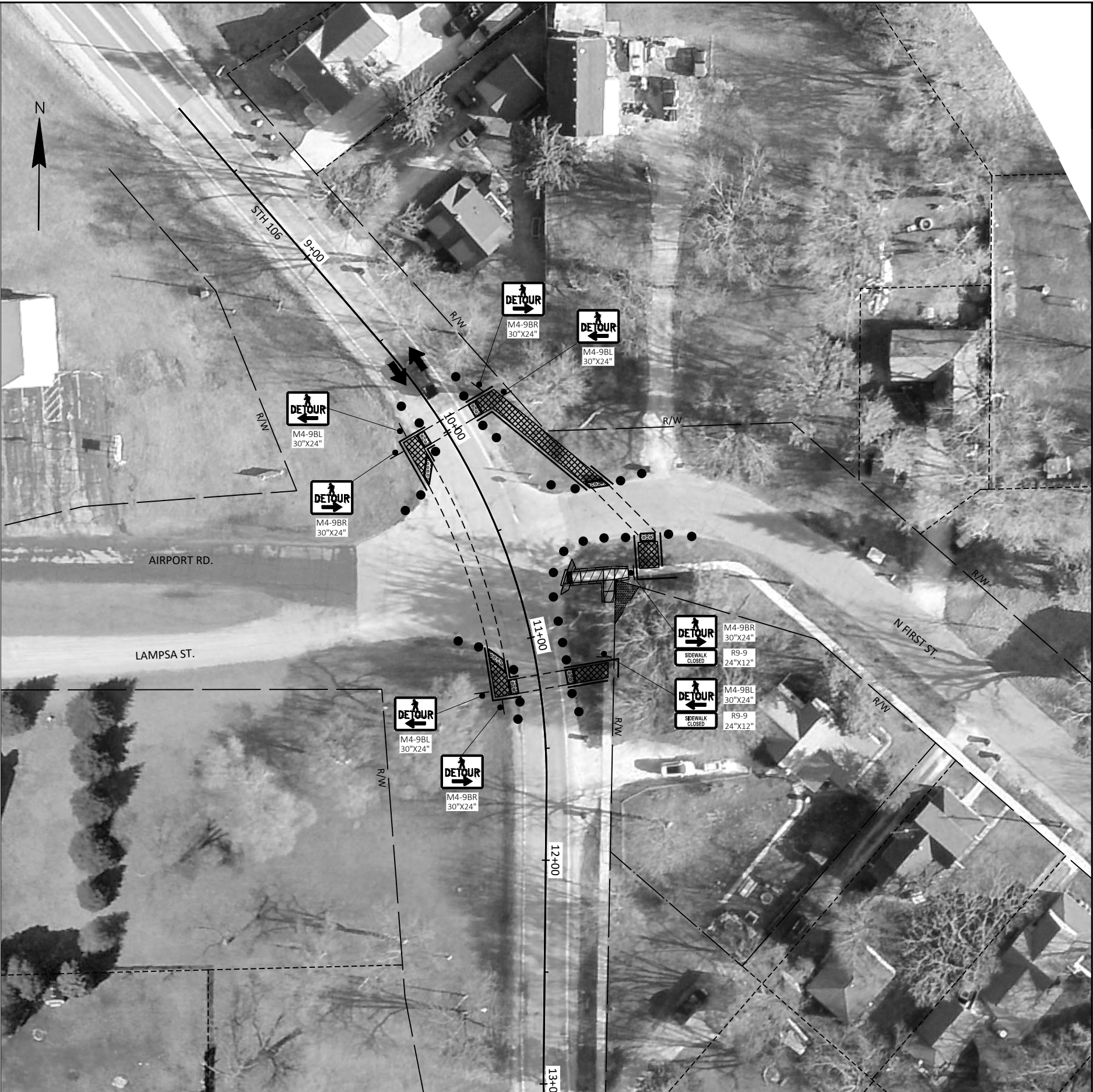
POST MOUNTED SIGNS LOCATED ADJACENT TO A SIDEWALK SHALL HAVE A 7 FOOT MINIMUM CLEARANCE FROM THE BOTTOM OF THE SIGN TO THE SIDEWALK SURFACE.

SDD

SDD TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

LEGEND

-  DIRECTION OF TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN SURFACE MATTING
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  TRAFFIC CONTROL DRUM AT MAXIMUM 10 FOOT SPACING
-  SIGN ON PERMANENT SUPPORT
-  TEMPORARY MARKING FOR CROSSWALK (TEMPORARY MARKING LINE PAINT 6-INCH)



## NOTES

TRAFFIC CONTROL SHOWN FOR CORNER CURB RAMPS WORK ZONE WHERE SDD TRAFFIC CONTROL PEDESTRIAN ACCOMMODATION IS NOT ADEQUATE. FOR ALL OTHER CURB RAMP WORK ZONES FOLLOW THE SDD AND UTILIZE TEMPORARY CROSSWALK AS SHOWN IN THE DETAIL.

LIMIT SIDEWALK CLOSURES TO ONE INTERSECTION QUADRANT AT A TIME TO MINIMIZE PEDESTRIAN DISRUPTION. ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

PEDESTRIAN PATH AREA TO BE EXISTING CONCRETE SIDEWALK OR APPROVED TEMPORARY PEDESTRIAN SURFACE.

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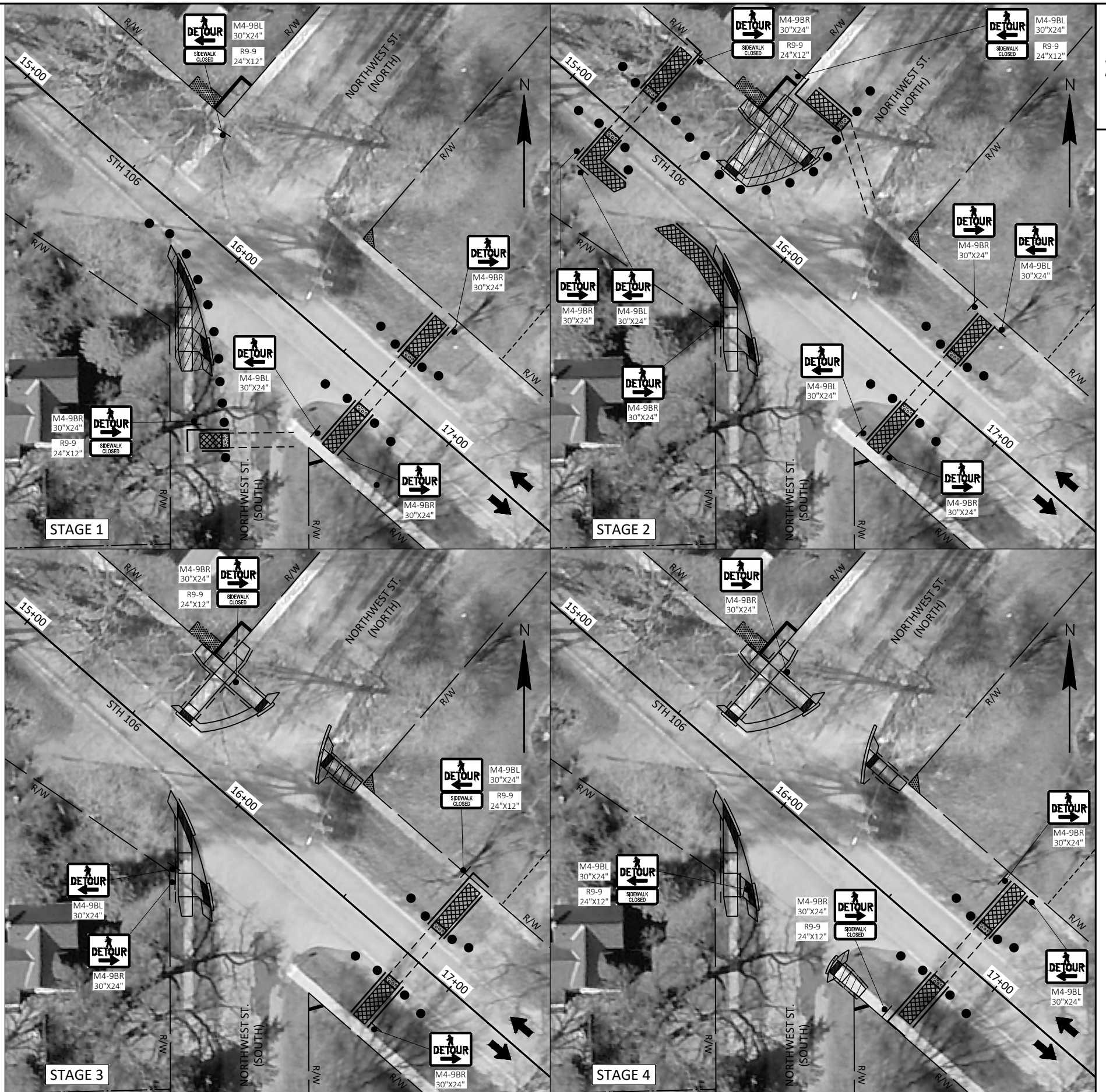
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## SDD

SDD TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

## LEGEND

- ➔ DIRECTION OF TRAFFIC
- ▨ WORK AREA
- ▩ TEMPORARY PEDESTRIAN SURFACE MATTING
- ▤ TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN BARRICADE
- TRAFFIC CONTROL DRUM AT MAXIMUM 10 FOOT SPACING
- 🚧 SIGN ON PERMANENT SUPPORT
- TEMPORARY MARKING FOR CROSSWALK (TEMPORARY MARKING LINE PAINT 6-INCH)





## NOTES

TRAFFIC CONTROL SHOWN FOR CORNER CURB RAMPS WORK ZONE WHERE SDD TRAFFIC CONTROL PEDESTRIAN ACCOMMODATION IS NOT ADEQUATE. FOR ALL OTHER CURB RAMP WORK ZONES FOLLOW THE SDD AND UTILIZE TEMPORARY CROSSWALK AS SHOWN IN THE DETAIL.

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
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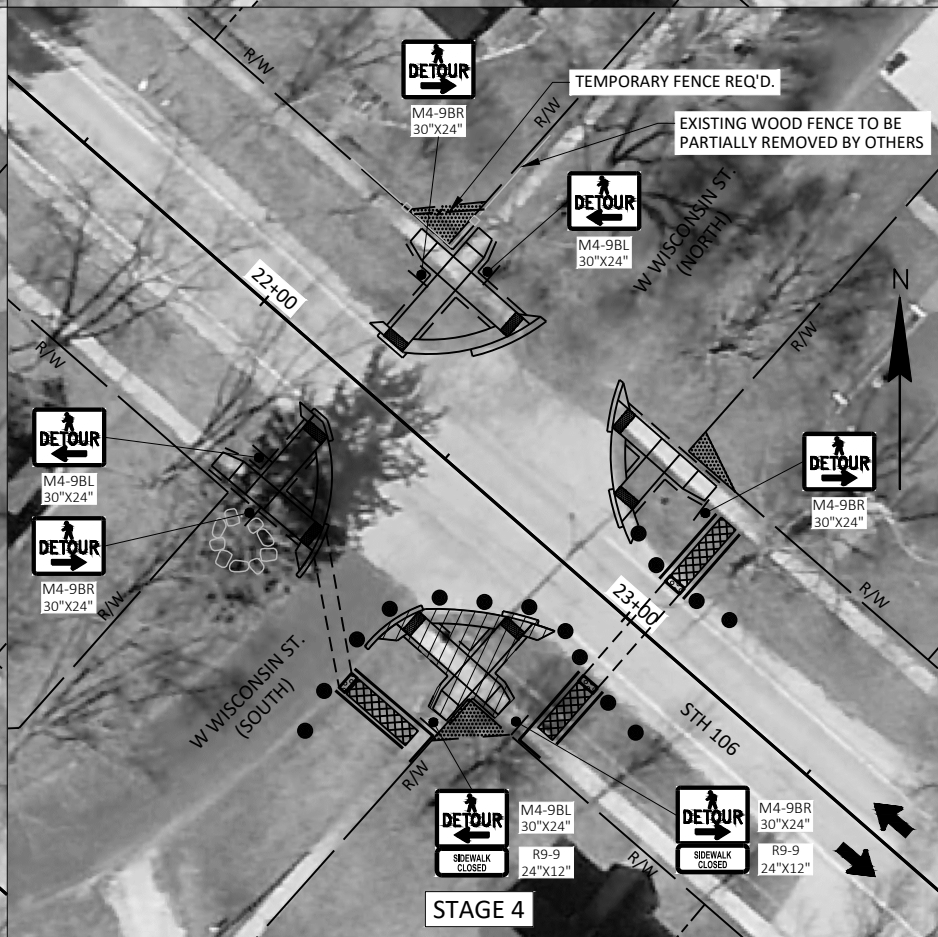
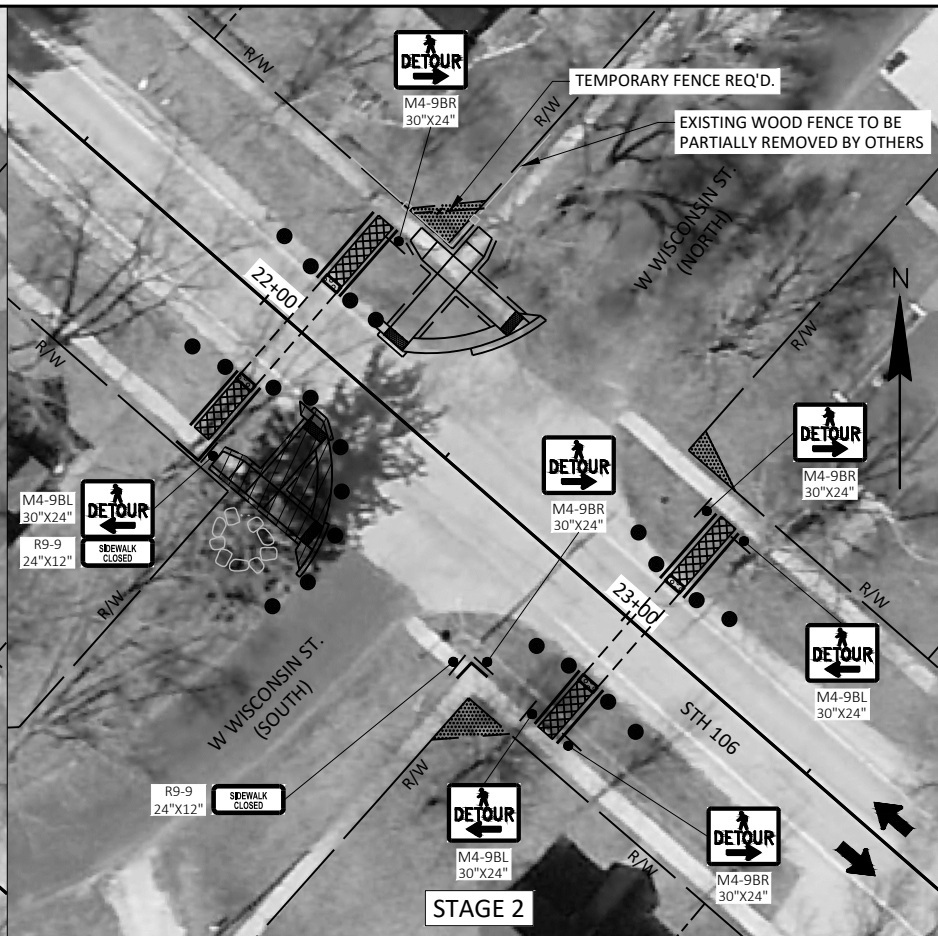
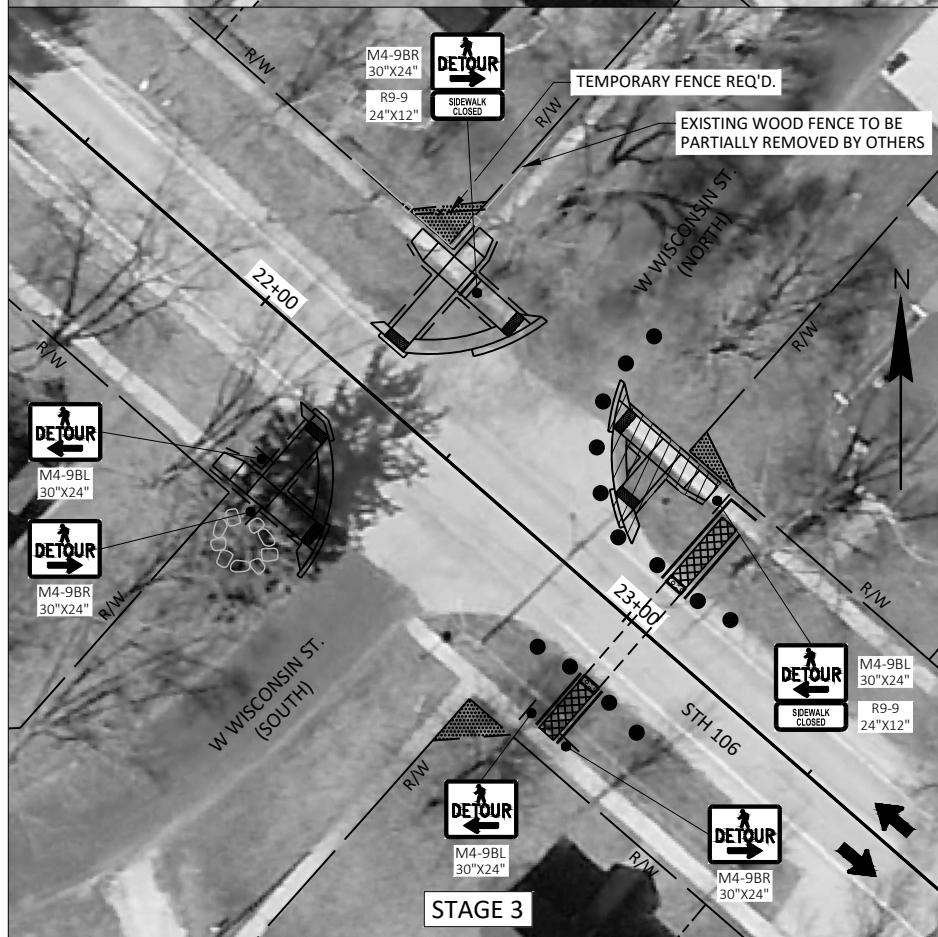
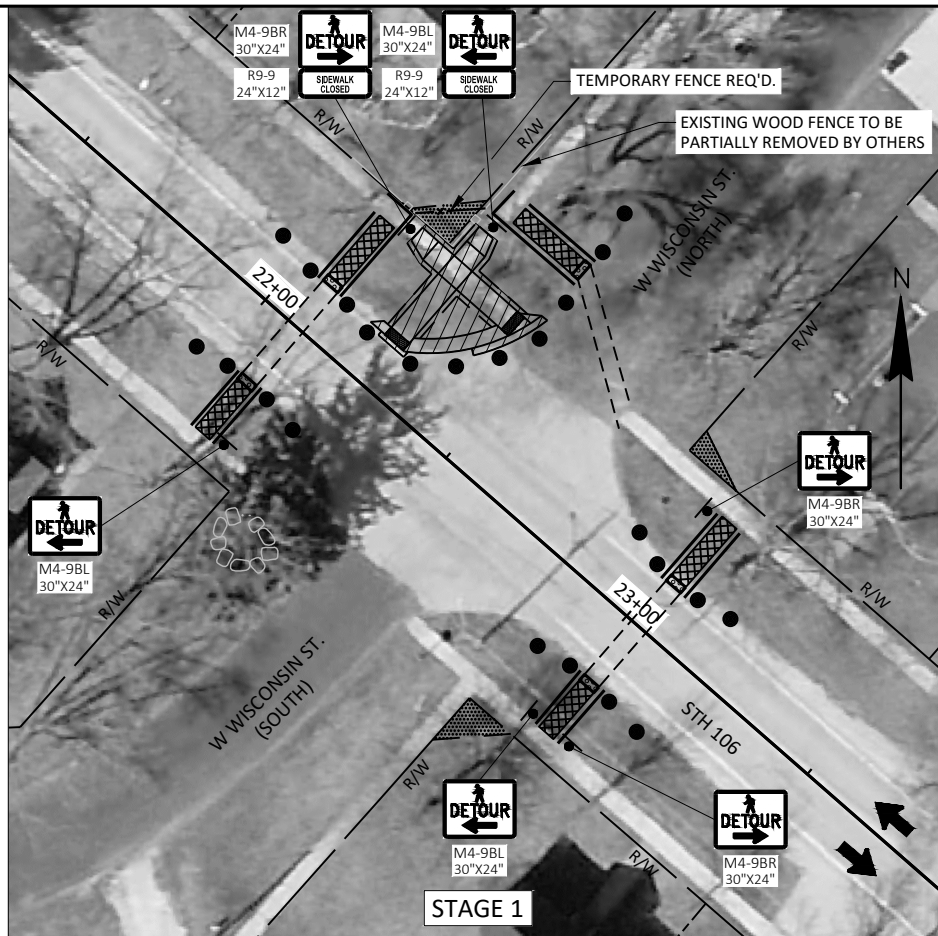
POST MOUNTED SIGNS LOCATED ADJACENT TO A SIDEWALK SHALL HAVE A 7 FOOT MINIMUM CLEARANCE FROM THE BOTTOM OF THE SIGN TO THE SIDEWALK SURFACE.

## SDD

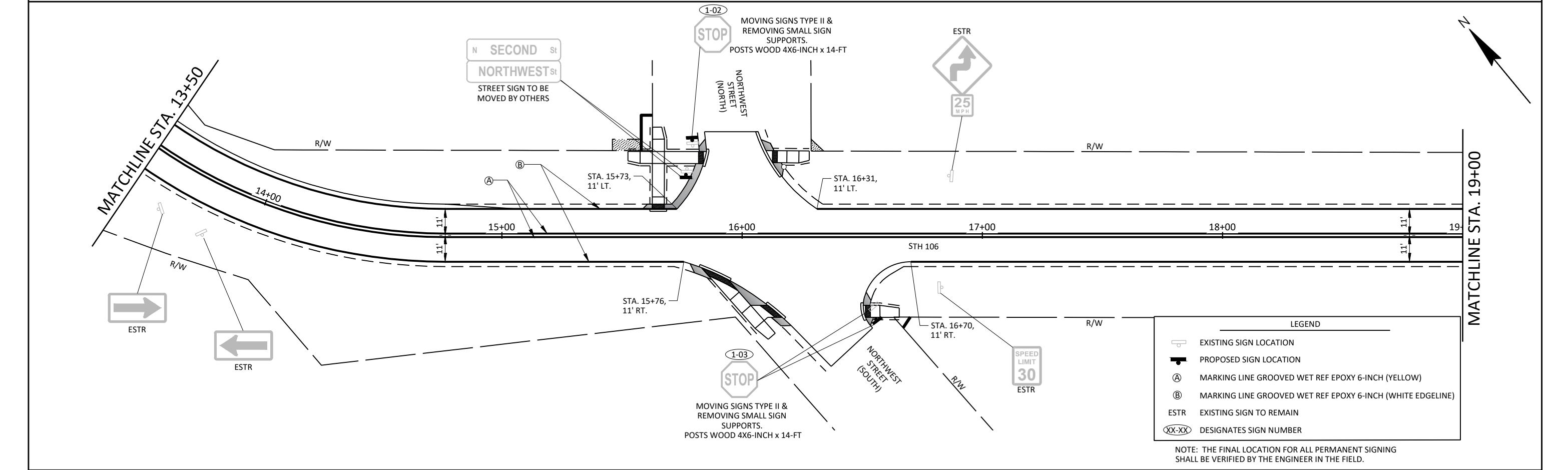
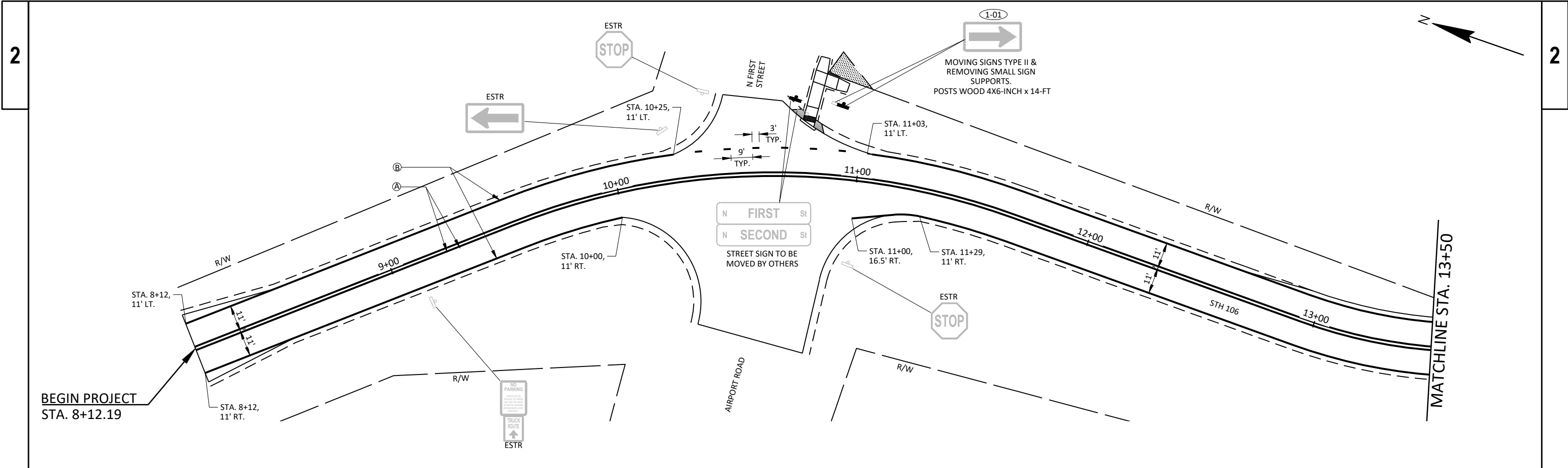
SDD TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

## LEGEND

-  DIRECTION OF TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN SURFACE MATTING
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  TRAFFIC CONTROL DRUM AT MAXIMUM 10 FOOT SPACING
-  SIGN ON PERMANENT SUPPORT
-  TEMPORARY MARKING FOR CROSSWALK (TEMPORARY MARKING LINE PAINT 6-INCH)

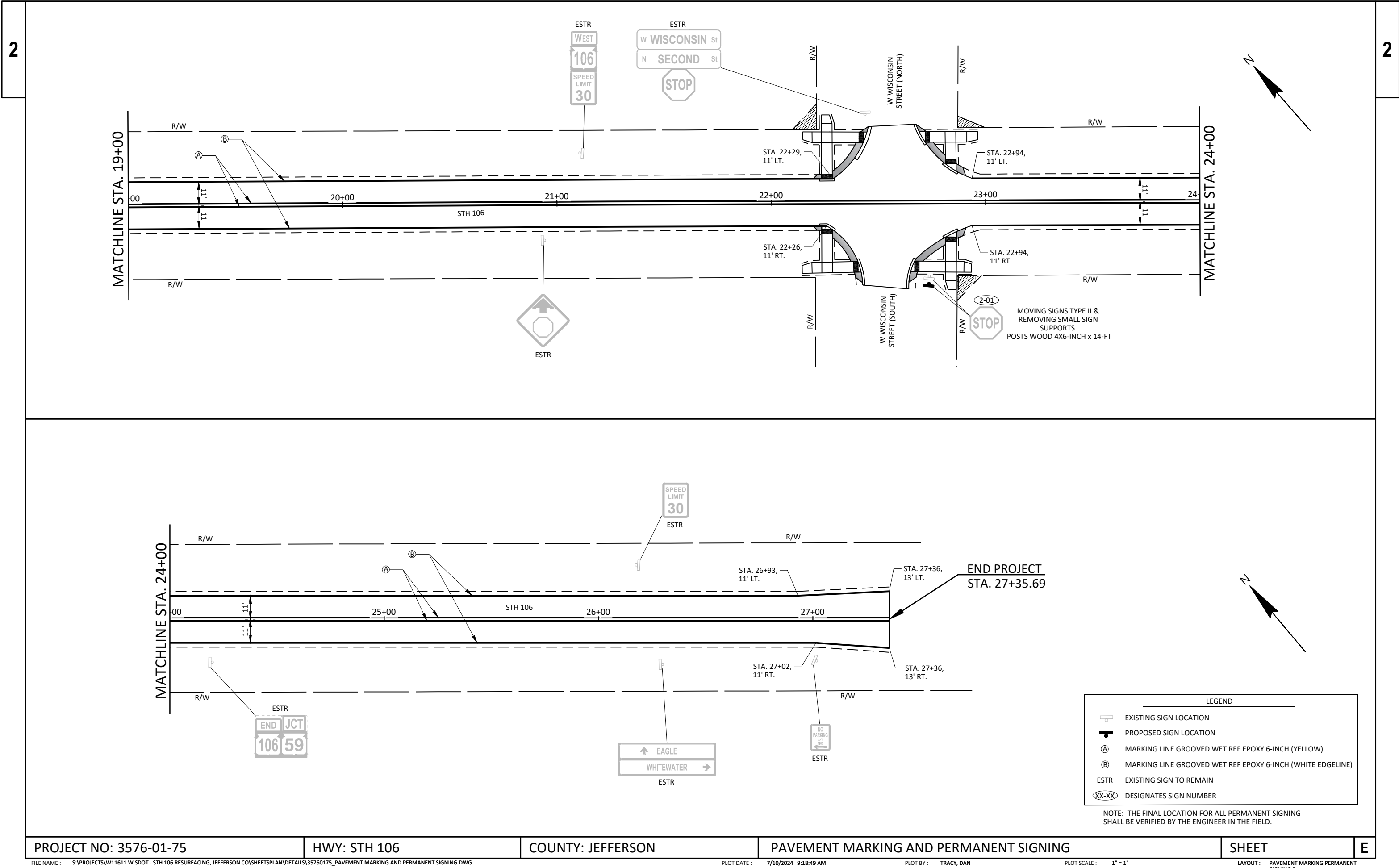






LEGEND	
	EXISTING SIGN LOCATION
	PROPOSED SIGN LOCATION
	MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW)
	MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE EDGELINE)
	EXISTING SIGN TO REMAIN
	DESIGNATES SIGN NUMBER

NOTE: THE FINAL LOCATION FOR ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.



LEGEND

EXISTING SIGN LOCATION

PROPOSED SIGN LOCATION

MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW)

MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE EDGELINE)

EXISTING SIGN TO REMAIN

DESIGNATES SIGN NUMBER

NOTE: THE FINAL LOCATION FOR ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.

Estimate Of Quantities

3576-01-75

Line	Item	Item Description	Unit	Total	Qty
0002	204.0110	Removing Asphaltic Surface	SY	20.000	20.000
0004	204.0115	Removing Asphaltic Surface Butt Joints	SY	120.000	120.000
0006	204.0120	Removing Asphaltic Surface Milling	SY	5,650.000	5,650.000
0008	204.0155	Removing Concrete Sidewalk	SY	120.000	120.000
0010	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 3576-01-75	EACH	1.000	1.000
0012	211.0400	Prepare Foundation for Asphaltic Shoulders	STA	6.000	6.000
0014	213.0100	Finishing Roadway (project) 01. 3576-01-75	EACH	1.000	1.000
0016	305.0110	Base Aggregate Dense 3/4-Inch	TON	240.000	240.000
0018	455.0605	Tack Coat	GAL	400.000	400.000
0020	460.2000	Incentive Density HMA Pavement	DOL	420.000	420.000
0022	460.5224	HMA Pavement 4 LT 58-28 S	TON	650.000	650.000
0024	465.0105	Asphaltic Surface	TON	10.000	10.000
0026	465.0110	Asphaltic Surface Patching	TON	120.000	120.000
0028	602.0405	Concrete Sidewalk 4-Inch	SF	1,760.000	1,760.000
0030	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	130.000	130.000
0032	602.0605	Curb Ramp Detectable Warning Field Radial Yellow	SF	42.000	42.000
0034	618.0100	Maintenance and Repair of Haul Roads (project) 01. 3576-01-75	EACH	1.000	1.000
0036	619.1000	Mobilization	EACH	1.000	1.000
0038	624.0100	Water	MGAL	3.600	3.600
0040	625.0100	Topsoil	SY	95.000	95.000
0042	628.1504	Silt Fence	LF	100.000	100.000
0044	628.1520	Silt Fence Maintenance	LF	200.000	200.000
0046	628.1905	Mobilizations Erosion Control	EACH	3.000	3.000
0048	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0050	628.2006	Erosion Mat Urban Class I Type A	SY	95.000	95.000
0052	628.7010	Inlet Protection Type B	EACH	3.000	3.000
0054	629.0210	Fertilizer Type B	CWT	0.600	0.600
0056	630.0140	Seeding Mixture No. 40	LB	2.000	2.000
0058	630.0500	Seed Water	MGAL	2.000	2.000
0060	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	4.000	4.000
0062	638.2102	Moving Signs Type II	EACH	4.000	4.000
0064	638.3000	Removing Small Sign Supports	EACH	4.000	4.000
0066	642.5001	Field Office Type B	EACH	1.000	1.000
0068	643.0300	Traffic Control Drums	DAY	1,250.000	1,250.000
0070	643.0900	Traffic Control Signs	DAY	1,700.000	1,700.000
0072	643.1050	Traffic Control Signs PCMS	DAY	14.000	14.000
0074	643.3165	Temporary Marking Line Paint 6-Inch	LF	4,010.000	4,010.000
0076	643.3305	Temporary Marking Crosswalk Paint 6-inch	LF	740.000	740.000
0078	643.5000	Traffic Control	EACH	1.000	1.000
0080	644.1440	Temporary Pedestrian Surface Matting	SF	1,710.000	1,710.000
0082	644.1605	Temporary Pedestrian Detectable Warning Field	SF	200.000	200.000
0084	644.1810	Temporary Pedestrian Barricade	LF	850.000	850.000
0086	646.2040	Marking Line Grooved Wet Ref Epoxy 6-Inch	LF	7,300.000	7,300.000
0088	650.8000	Construction Staking Resurfacing Reference	LF	1,930.000	1,930.000
0090	650.9000	Construction Staking Curb Ramps	EACH	15.000	15.000
0092	650.9500	Construction Staking Sidewalk (project) 01. 3576-01-75	EACH	1.000	1.000
0094	650.9911	Construction Staking Supplemental Control (project) 01. 3576-01-75	EACH	1.000	1.000
0096	690.0150	Sawing Asphalt	LF	440.000	440.000
0098	690.0250	Sawing Concrete	LF	55.000	55.000

Estimate Of Quantities

3576-01-75

Line	Item	Item Description	Unit	Total	Qty
0100	740.0440	Incentive IRI Ride	DOL	1,460.000	1,460.000
0102	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	1,200.000	1,200.000
0104	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	600.000	600.000
0106	SPV.0060	Special 01. Adjusting Water Valves	EACH	4.000	4.000
0108	SPV.0060	Special 02. Temporary Fence	EACH	1.000	1.000
0110	SPV.0180	Special 01. Removing Distressed Pavement Milling	SY	1,060.000	1,060.000

3

REMOVING ASPHALTIC SURFACE			
STATION - STATION	LOCATION	DESCRIPTION	204.0110 (SY)
10+75 - 10+82	MAINLINE, LT.	CR 1	1
15+62 - 15+86	MAINLINE, LT.	CR 5 & CR 6	2
15+82 - 16+18	MAINLINE, RT.	CR 2 & CR 3	3
16+06 - 16+16	MAINLINE, LT.	CR 7	3
16+49 - 16+50	MAINLINE, RT.	CR 4	1
22+23 - 22+47	MAINLINE, LT.	CR 8 & CR 9	4
22+23 - 22+44	MAINLINE, RT.	CR 12 & CR 13	2
22+63 - 22+87	MAINLINE, RT.	CR 14 & CR 15	2
22+66 - 22+87	MAINLINE, LT.	CR 10 & CR 11	2
TOTAL =			20

REMOVING ASPHALTIC SURFACE BUTT JOINTS	
	204.0115
LOCATION	(SY)
STA. 8+12.19	15
AIRPORT ROAD	25
N FIRST STREET	14
NORTHWEST STREET (SOUTH)	13
NORTHWEST STREET (NORTH)	13
W WISCONSIN STREET (SOUTH)	13
W WISCONSIN STREET (NORTH)	12
STA. 27+35.69	15
TOTAL =	120

REMOVING ASPHALTIC SURFACE MILLING		
STATION	LOCATION	204.0120 (SY)
STA. 8+12.19 - STA. 27+35.69	MAINLINE	5,650
TOTAL =		5,650

REMOVING CONCRETE SIDEWALK			
STATION - STATION	LOCATION	DESCRIPTION	204.0155 (SY)
10+76 - 10+93	MAINLINE, LT.	CR 1	16
15+52 - 15+82	MAINLINE, LT.	CR 5 & CR 6	27
15+86 - 16+14	MAINLINE, RT.	CR 2 & CR 3	13
16+15 - 16+28	MAINLINE, LT.	CR 7	6
16+53 - 16+65	MAINLINE, RT.	CR 4	6
22+15 - 22+41	MAINLINE, LT.	CR 8 & CR 9	16
22+14 - 22+39	MAINLINE, RT.	CR 12 & CR 13	10
22+67 - 22+93	MAINLINE, RT.	CR 14 & CR 15	15
22+71 - 22+96	MAINLINE, LT.	CR 10 & CR 11	11
TOTAL =			120

3

BASE AGGREGATE DENSE		
STATION - STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH (TON)
8+12.19 - 27+35.69	MAINLINE, LT. & RT.	105
-	CURB RAMP LOCATIONS	110
-	DRIVEWAYS	25
TOTALS =		240

HMA PAVEMENT								
		211.0101 PREPARE FOUNDATION FOR ASPHALTIC PAVING (01. 3576-01-75) (EACH)	211.0400 PREPARE FOUNDATION FOR ASPHALTIC SHOULDERS (STA)	455.0605 TACK COAT (GAL)	460.6224 HMA PAVEMENT 4 LT 58-28 S (TON)	465.0105 ASPHALTIC SURFACE (TON)	* 465.0110 ASPHALTIC SURFACE PATCHING (TON)	* SPV.0180.01 REMOVING DISTRESSED PAVEMENT MILLING (SY)
STATION - STATION	LOCATION							
8+12.19 - 27+35.69	MAINLINE	-	-	400	650	-	-	-
12+65 - 15+06	MAINLINE, LT.	-	3	-	-	-	-	-
-	N FIRST STREET	-	1	-	-	1	-	-
-	NORTHWEST STREET (SOUTH)	-	1	-	-	2	-	-
-	NORTHWEST STREET (NORTH)	-	-	-	-	2	-	-
-	W WISCONSIN STREET (SOUTH)	-	1	-	-	3	-	-
-	W WISCONSIN STREET (NORTH)	-	-	-	-	2	-	-
-	PROJECT	1	-	-	-	-	120	1060
TOTALS =		1	6	400	650	10	120	1,060
* EXACT LOCATION AND LIMITS OF REMOVING DISTRESSED PAVEMENT MILLING AND ASPHALTIC SURFACE PATCHING TO BE DETERMINED BY ENGINEER IN THE FIELD.								

CONCRETE SIDEWALK 4-INCH			
STATION - STATION	LOCATION	DESCRIPTION	602.0405 (SF)
10+76 - 10+93	MAINLINE, LT.	CR 1	185
15+52 - 15+82	MAINLINE, LT.	CR 5 & CR 6	300
15+86 - 16+14	MAINLINE, RT.	CR 2 & CR 3	215
16+15 - 16+28	MAINLINE, LT.	CR 7	80
16+53 - 16+65	MAINLINE, RT.	CR 4	70
22+15 - 22+41	MAINLINE, LT.	CR 8 & CR 9	265
22+14 - 22+39	MAINLINE, RT.	CR 12 & CR 13	215
22+67 - 22+93	MAINLINE, RT.	CR 14 & CR 15	240
22+71 - 22+96	MAINLINE, LT.	CR 10 & CR 11	190
TOTAL =			1760

CURB RAMP DETECTABLE WARNING FIELD YELLOW				
STATION	LOCATION	DESCRIPTION	602.0505 CURB RAMP DETECTABLE WARNING FIELD YELLOW (SF)	602.0605 CURB RAMP DETECTABLE WARNING FIELD RADIAL YELLOW (EACH)
10+79	MAINLINE, LT.	CR 1	10	-
15+65	MAINLINE, LT.	CR 5	10	-
15+83	MAINLINE, LT.	CR 6	10	-
15+90	MAINLINE, RT.	CR 2	-	26
16+11	MAINLINE, RT.	CR 3	-	16
16+14	MAINLINE, LT.	CR 7	10	-
16+52	MAINLINE, RT.	CR 4	10	-
22+26	MAINLINE, LT.	CR 8	10	-
22+26	MAINLINE, RT.	CR 12	10	-
22+39	MAINLINE, RT.	CR 13	10	-
22+42	MAINLINE, LT.	CR 9	10	-
22+68	MAINLINE, RT.	CR 14	10	-
22+73	MAINLINE, LT.	CR 10	10	-
22+84	MAINLINE, LT.	CR 11	10	-
22+84	MAINLINE, RT.	CR 15	10	-
TOTAL =			130	42

FINISHING ITEMS						
		625.0100 TOPSOIL (SY)	628.2006 EROSION MAT URBAN CLASS I TYPE A (SY)	629.0210 FERTILIZER TYPE B (CWT)	630.0140 SEEDING MIXTURE NO. 40 (LB)	630.0500 SEED WATER (MGAL)
STATION - STATION	LOCATION					
10+70 - 10+93	N FIRST STREET	10	10	0.1	0.2	0.2
15+85 - 16+11	NORTHWEST STREET (SOUTH)	10	10	0.1	0.2	0.2
15+52 - 16+28	NORTHWEST STREET ( NORTH)	15	15	0.1	0.4	0.4
22+15 - 22+93	W WISCONSIN STREET (SOUTH)	20	20	0.1	0.4	0.4
22+15 - 22+96	W WISCONSIN STREET (NORTH)	20	20	0.1	0.4	0.4
UNDISTRIBUTED	PROJECT	20	20	0.1	0.4	0.4
TOTALS =		95	95	0.6	2	2

WATER	
	624.0100 (MGAL)
LOCATION	
PROJECT	3.6
TOTAL =	
	3.6

SILT FENCE			
STATION - STATION	LOCATION	628.1504 SILT FENCE (LF)	628.1520 SILT FENCE MAINTENANCE (LF)
* UNDISTIBUTED	PROJECT	100	200
TOTALS =		100	200
* THE USE OF SILT FENCE SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD			



3

MOBILIZATIONS EROSION CONTROL			INLET PROTECTION			PERMANENT SIGNING										ALL ITEMS CATEGORY 010 UNLESS OTHERWISE NOTED		
PROJECT	628.1905	628.1910	STATION	LOCATION	628.7010	SIGN	APPROX.	LOCATION	SIGN	SIGN	SIZE	638.2102	638.3000	634.0614				
	MOBILIZATIONS	MOBILIZATIONS			INLET PROTECTION							MOVING SIGNS	REMOVING SMALL	POSTS WOOD				
	EROSION CONTROL	EROSION CONTROL			TYBE B						(INCH X INCH)	TYPE II	SIGN SUPPORTS	4X6-INCH X 14-FT				
	(EACH)	(EACH)			(EACH)				CODE	DESCRIPTION		(EACH)	(EACH)	(EACH)				
3576-01-75	3	2	16+11	MAINLINE, RT.	1	1-01	10+90	MAINLINE, LT.	W1-6	DIRECTIONAL ARROW	48X24	1	1	1				
			16+17	MAINLINE, LT.	1	1-02	15+79	MAINLINE, LT.	R1-1	STOP	30X30	1	1	1				
			16+55	MAINLINE, RT.	1	1-03	16+54	MAINLINE, RT.	R1-1	STOP	30X30	1	1	1				
						2-01	22+74	MAINLINE, RT.	R1-1	STOP	30X30	1	1	1				
TOTALS =	3	2		TOTALS =	3						TOTALS =	4	4	4				

3

TRAFFIC CONTROL							TEMPORARY PEDESTRIAN ACCESS							
		* DURATION	643.0300	643.0900	643.1050	643.5000			643.3305	644.1440	644.1605	644.1810		
STAGE	LOCATION	CALENDAR	DRUMS	SIGNS	PCMS	TRAFFIC	STAGE	LOCATION	TEMPORARY MARKING	TEMPORARY	TEMPORARY PEDESTRIAN	TEMPORARY	COMMENTS	
		DAYS	(DAYS)	(DAYS)	(DAYS)	CONTROL			CROSSWALK PAINT	PEDESTRIAN	DETECTABLE	PEDESTRIAN		
						(EACH)			6-INCH	SURFACE MATTING	WARNING FIELD	BARRICADE		
									(LF)	(SF)	(SF)	(LF)		
PROJECT	-	40	70	1,168	14	1	-	N FIRST STREET	320	590	104	290	3 TEMP CROSSWALK REQ'D FOR CONSTRUCTING CURB RAMP 1 ( STA 10+78, LT)	
-	N FIRST STREET	7	220	70	-	-	1	NORTHWEST STREET	92	210	24	110	2 TEMP CROSSWALK REQ'D FOR CONSTRUCTING CURB RAMPS 2 & 3 (STA 15+90, RT. & STA 16+14, RT.)	
1	NORTHWEST STREET	7	135	49	-	-	2	NORTHWEST STREET	98	450	24	165	3 TEMP CROSSWALK REQ'D FOR CONSTRUCTING CURB RAMPS 5 & 6 (STA 15+65, LT. & STA 15+84, LT.)	
2	NORTHWEST STREET	7	190	77	-	-	3	NORTHWEST STREET	-	-	-	-	1 TEMP CROSSWALK REQ'D FOR CONSTRUCTING CURB RAMP 7 (STA 16+13, LT.)	
3	NORTHWEST STREET	7	60	49	-	-	4	NORTHWEST STREET	-	-	-	5	1 TEMP CROSSWALK REQ'D FOR CONSTRUCTING CURB RAMP 4. (STA 16+50, RT)	
4	NORTHWEST STREET	7	60	49	-	-	1	W WISCONSIN STREET	170	380	40	185	3 TEMP CROSSWALK REQ'D FOR CONSTRUCTING CURB RAMPS 8 & 9 ( STA 22+26, LT. & STA 22+43, LT)	
1	W WISCONSIN STREET	7	170	56	-	-	2	W WISCONSIN STREET	-	-	-	20	2 TEMP CROSSWALK REQ'D FOR CONSTRUCTING CURB RAMPS 12 & 13 (STA 22+26, RT. & 22+40, RT)	
2	W WISCONSIN STREET	7	149	63	-	-	3	W WISCONSIN STREET	-	-	-	15	1 TEMP CROSSWALK REQ'D FOR CONSTRUCTING CURB RAMPS 10 & 11 (STA 22+72, LT & STA 22+84, LT)	
3	W WISCONSIN STREET	7	91	56	-	-	4	W WISCONSIN STREET	60	80	8	60	2 TEMP CROSSWALK REQ'D FOR CONSTRUCTING CURB RAMPS 14 & 15 (STA 22+67, RT. & STA 22+84, RT)	
4	W WISCONSIN STREET	7	105	63	-	-	TOTALS =							
TOTALS =			1,250	1,700	14	1	TOTALS =							
* FOR INFORMATION ONLY														

PAVEMENT MARKING						
STATION - STATION	LOCATION	DESCRIPTION	* 643.3165		646.2040	
			TEMPORARY MARKING		MARKING LINE GROOVED	
			LINE PAINT 6-INCH		WET REF EPOXY 6-INCH	
			YELLOW 4' SKIPS (LF)	DOUBLE YELLOW (LF)	WHITE SOLID (LF)	YELLOW SOLID (LF)
8+12.19 - 27+35.69	MAINLINE	TEMPORARY C/L	160	3850	-	-
8+12.19 - 27+35.69	MAINLINE	WHITE EDGELINES	-	-	3450	-
8+12.19 - 27+35.69	MAINLINE	DOUBLE YELLOW C/L	-	-	-	3850
SUBTOTALS =			160	3850	3450	3850
TOTALS =			4010		7300	
* YELLOW 4' SKIPS SHALL BE PLACED SAME DAY AS FIRST MILLING PASS AND DOUBLE YELLOW SHALL BE PLACED SAME DAY AS FINAL PAVING PASS. SEE CONSTRUCTION DETAILS FOR ADDITIONAL INFORMATION.						

CONSTRUCTION STAKING						
STATION - STATION	LOCATION	DESCRIPTION	650.8000	650.9000	650.9500	650.9911
			RESURFACING REFERENCE (LF)	CURB RAMPS (EACH)	SIDEWALK (01. 3576-01-75) (EACH)	SUPPLEMENTAL CONTROL (01. 3576-01-75) (EACH)
8+12.19 - 27+35.69	MAINLINE	-	1930	-	-	-
10+79	MAINLINE, LT.	CR 1	-	1	-	-
15+65	MAINLINE, LT.	CR 5	-	1	-	-
15+83	MAINLINE, LT.	CR 6	-	1	-	-
15+90	MAINLINE, RT.	CR 2	-	1	-	-
16+11	MAINLINE, RT.	CR 3	-	1	-	-
16+14	MAINLINE, LT.	CR 7	-	1	-	-
16+52	MAINLINE, RT.	CR 4	-	1	-	-
22+26	MAINLINE, LT.	CR 8	-	1	-	-
22+26	MAINLINE, RT.	CR 12	-	1	-	-
22+39	MAINLINE, RT.	CR 13	-	1	-	-
22+42	MAINLINE, LT.	CR 9	-	1	-	-
22+68	MAINLINE, RT.	CR 14	-	1	-	-
22+73	MAINLINE, LT.	CR 10	-	1	-	-
22+84	MAINLINE, LT.	CR 11	-	1	-	-
22+84	MAINLINE, RT.	CR 15	-	1	-	-
-	PROJECT	-	-	-	1	1
TOTALS =			1930	15	1	1

SAWING			ADJUSTING WATER VALVES	
690.0150	690.0250		SPV.0060.01	
			ADJUSTING	
			WATER VALVES	

SAWING			ADJUSTING WATER VALVES			TEMPORARY FENCE		
STATION	LOCATION	690.0150 ASPHALT (LF)	STATION	LOCATION	SPV.0060.01 ADJUSTING WATER VALVES (CATEGORY 020) (EACH)	STATION - STATION	LOCATION	SPV.0060.02 TEMPORARY FENCE (EACH)
12+65 - 15+06	MAINLINE, LT.	241	10+58	MAINLINE, LT.	1	22+10 - 22+22	MAINLINE, LT.	1
-	CURB RAMPS	199	11+17	MAINLINE, RT.	1	TOTAL =		
TOTAL =		440	15+75	MAINLINE, LT.	1			
		55	22+89	MAINLINE, LT.	1			
			TOTALS =		4			





STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION  
TRANSPORTATION PROJECT PLAT TITLE SHEET  
PROJECT NO. 3576-01-25  
FT ATKINSON - PALMYRA

FIRST STREET TO STH 59

STH 106  
JEFFERSON COUNTY



CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	●
QUARTER LINE	---	NON-MONUMENTED R/W POINT	○		
SIXTEENTH LINE	---	FOUND IRON PIN (1-INCH UNLESS NOTED)	IP		
NEW REFERENCE LINE		SECTION CORNER MONUMENT			
NEW R/W LINE	---	GEODETIC SURVEY MONUMENT			
EXISTING R/W OR HE LINE	---	SIXTEENTH CORNER MONUMENT			
PROPERTY LINE	---	SIGN		OFF-PREMISE SIGN	
LOT, TIE & OTHER MINOR LINES	---				
SLOPE INTERCEPT	---				
CORPORATE LIMITS	---				
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)	---				
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---				
TEMPORARY LIMITED EASEMENT AREA	---				
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---				
TRANSMISSION STRUCTURES	---				
BUILDING	---				
BRIDGE	---				
CULVERT	---				
TO BE REMOVED	---				
PARALLEL OFFSETS	---				

CONVENTIONAL ABBREVIATIONS

ACCESS RIGHTS	AR	POINT OF INTERSECTION	PI
ACRES	AC	PROPERTY LINE	PL
AHEAD	AH	RECORDED AS	(100' )
ALUMINUM	ALUM	REEL / IMAGE	R/I
AND OTHERS	ET AL	REFERENCE LINE	R/L
BACK	BK	REMAINING	REM
BLOCK	BLK	RESTRICTIVE DEVELOPMENT	RDE
CENTERLINE	C/L	EASEMENT	
CERTIFIED SURVEY MAP	CSM	RIGHT	RT
CONCRETE	CONC	RIGHT OF WAY	R/W
COUNTY	CO	SECTION	SEC
COUNTY TRUNK HIGHWAY	CTH	SEPTIC VENT	SEPV
DISTANCE	DIST	SQUARE FEET	SF
CORNER	COR	STATE TRUNK HIGHWAY	STH
DOCUMENT NUMBER	DOC	STATION	STA
EASEMENT	EASE	TELEPHONE PEDESTAL	TP
EXISTING	EX	TEMPORARY LIMITED	TLE
GAS VALVE	GV	EASEMENT	
GRID NORTH	GN	TRANSPORTATION PROJECT	TPP
HIGHWAY EASEMENT	HE	PLAT	
IDENTIFICATION	ID	UNITED STATES HIGHWAY	USH
LAND CONTRACT	LC	VOLUME	V
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY	NGS		
NUMBER	NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		
POINT OF COMPOUND CURVE	PCC		

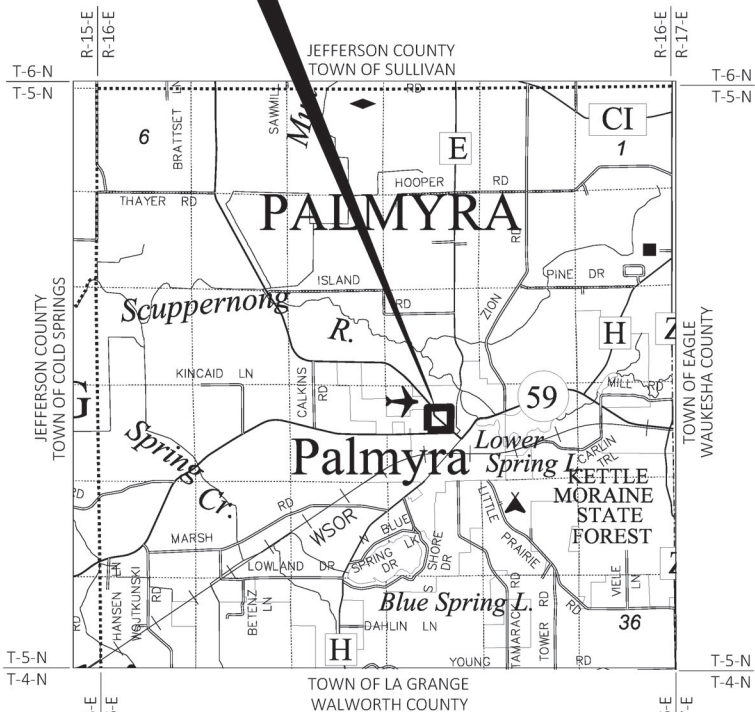
CURVE DATA

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL UTILITY SYMBOLS

W	WATER
G	GAS
T	TELEPHONE
OH	OVERHEAD
E	ELECTRIC
TV	CABLE TELEVISION
FO	FIBER OPTIC
SAN	SANITARY SEWER
SS	STORM SEWER

PROJECT LOCATION



GN

LAYOUT  
SCALE 0 1.0 MI.

THE NOTES, CONVENTIONAL SYMBOLS, AND ABBREVIATIONS ARE ASSOCIATED WITH EACH TRANSPORTATION PROJECT PLAT FOR PROJECT 3576-01-25

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), JEFFERSON COUNTY, NAD 83 (2011), IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

A PERMANENT LIMITED EASEMENT (PLE) IS A RIGHT FOR CONSTRUCTION AND MAINTENANCE PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE, BUT WITHOUT PREJUDICE TO THE OWNER'S RIGHTS TO MAKE OR CONSTRUCT IMPROVEMENTS ON SAID LANDS OR TO FLATTEN THE SLOPES, PROVIDING SAID ACTIVITIES WILL NOT IMPAIR OR OTHERWISE ADVERSELY AFFECT THE HIGHWAY FACILITIES.

FOR CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN MADISON.

PARCEL AND UTILITY IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE TPP DETAIL SHEETS.

INFORMATION FOR THE BASIS OF EXISTING HIGHWAY RIGHT-OF-WAY POINTS OF REFERENCE AND ACCESS CONTROL ARE LISTED ON THE TPP DETAIL PAGES.

PROJECT NUMBER 3576-01-25 -4.01  
SHEET 2 OF 2  
AMENDMENT NO:

TRANSPORTATION PROJECT PLAT NO: 3576-01-25 - 4.02

PART OF LOT 8 BLOCK 6 HIGGINS ADDITION TO THE VILLAGE OF PALMYRA, PART OF LOT 9 BLOCK 5 HIGGINS ADDITION TO THE VILLAGE OF PALMYRA, AND PART OF LOT 6 BLOCK 3 HIGGINS ADDITION TO THE VILLAGE OF PALMYRA, ALL BEING LOCATED IN THE SW¼-NE¼ OF SECTION 22, TOWNSHIP 5 NORTH, RANGE 16 EAST, VILLAGE OF PALMYRA, JEFFERSON COUNTY, WISCONSIN.

RELOCATION ORDER STH 106, FT ATKINSON - PALMYRA, FIRST STREET TO STH 59, JEFFERSON COUNTY.

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELIQUATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

- THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
- THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

FOR CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN MADISON.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), JEFFERSON COUNTY, NAD 83 (2011), IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

EXISTING STH 106 RIGHT-OF-WAY SHOWN HEREIN IS BASED ON CERTIFIED SURVEY MAP 3923 AND FOUND MONUMENTATION ASSOCIATED WITH SAID SURVEY. ALSO PLATS OF SURVEY 503-111 AND FOUND MONUMENTATION ASSOCIATED WITH SAID SURVEY. ALSO HIGGINS ADDITION TO THE VILLAGE OF PALMYRA AND FOUND MONUMENTATION ASSOCIATED WITH SAID PLAT.

EXISTING WEST WISCONSIN STREET RIGHT-OF-WAY SHOWN HEREIN IS BASED ON CERTIFIED SURVEY MAP 3923 AND FOUND MONUMENTATION ASSOCIATED WITH SAID SURVEY. ALSO PLAT OF SURVEY 503-111 AND FOUND MONUMENTATION ASSOCIATED WITH SAID SURVEY. ALSO HIGGINS ADDITION TO THE VILLAGE OF PALMYRA AND FOUND MONUMENTATION ASSOCIATED WITH SAID PLAT.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 8" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

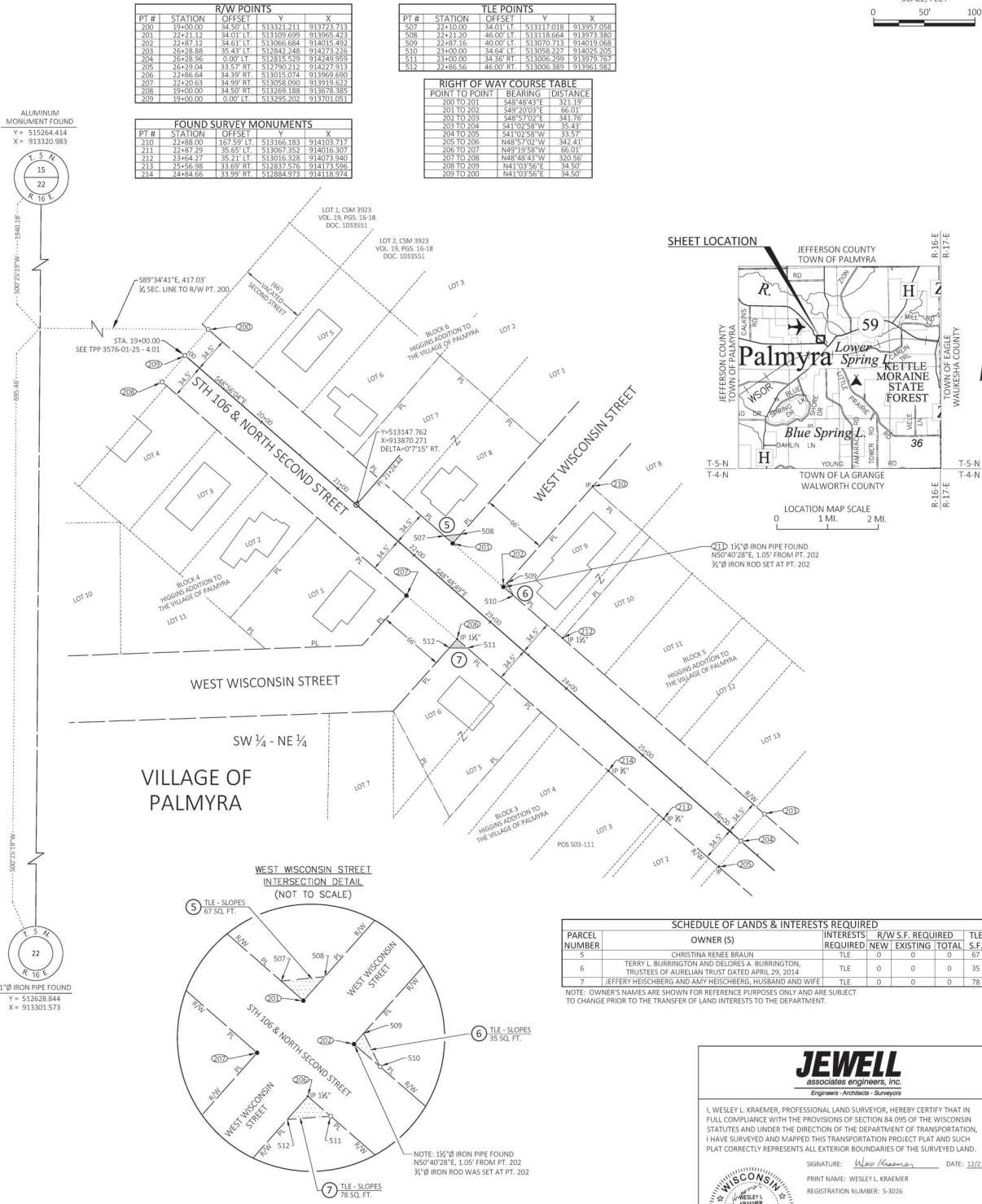
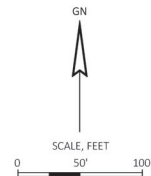
FOUND IRON PINS (IP) ARE 1" DIAMETER UNLESS OTHERWISE NOTED.

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED IN THE OFFICE OF REGISTER OF DEEDS, JEFFERSON COUNTY AS SHEET 2 OF 2 OF DOC. 1457942.

1458318

Office of Register of Deeds  
Jefferson County, WI  
RECEIVED FOR RECORD  
01/31/2022 09:54:24 AM  
Staci M. Hoffman  
Total Pages: 1  
REC FEE: 25.00  
\*\*The above recording information  
verifies that this document has  
been electronically recorded  
and returned to the submitter\*\*

RESERVED FOR REGISTER OF DEEDS  
PROJECT NUMBER 3576-01-25-4.02



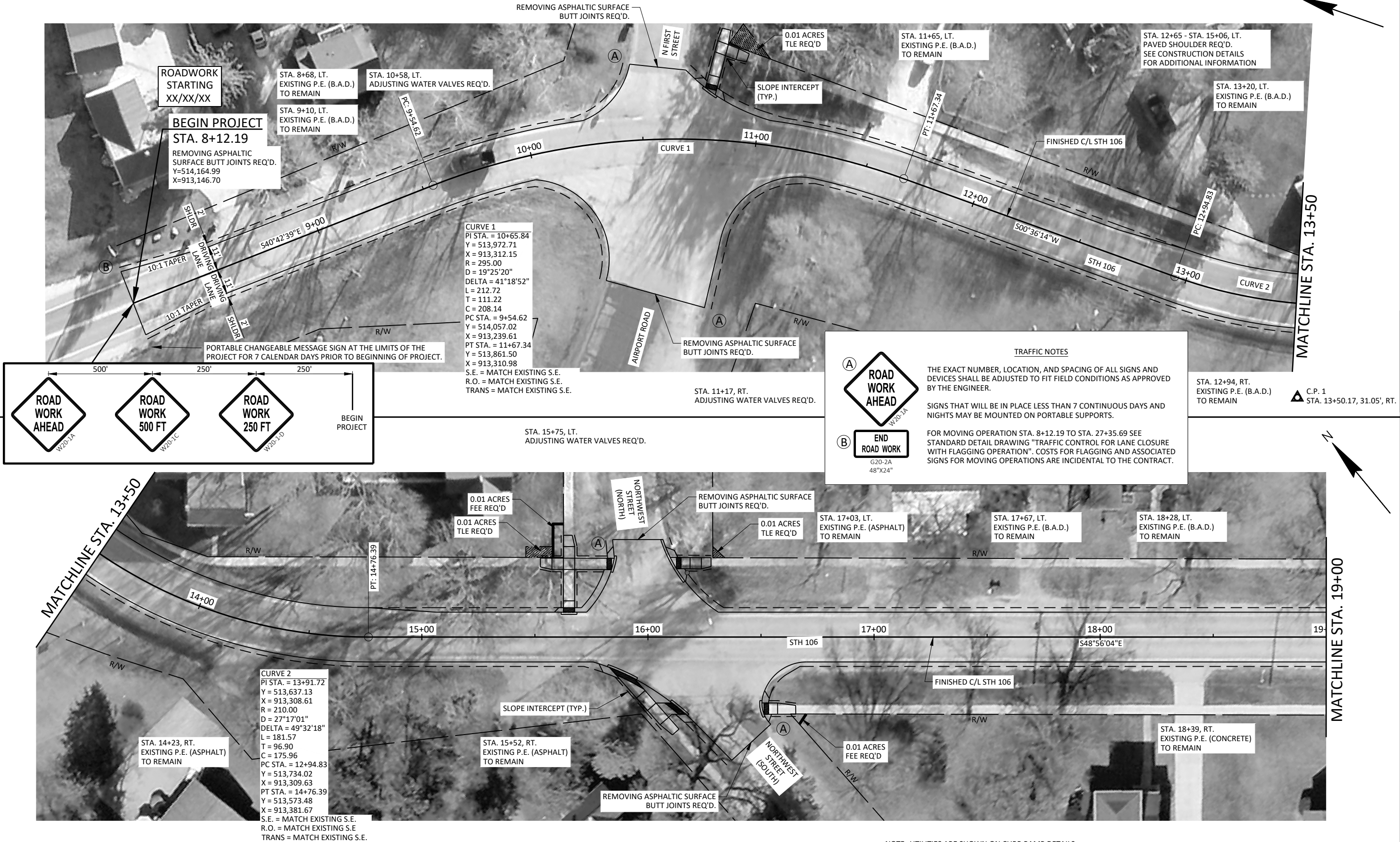
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PLAT DATE: 1/26/2022 2:51 PM

FILENAME: S:\Projects\WI1111\DOT - STH 106 Resurfacing, Jefferson\3576-01-25\STH 106 TYP.dwg  
APPRAISAL PLAT DATE: 12/21/2021



NOTE: UTILITIES ARE SHOWN ON CURB RAMP DETAILS



5

5

PROJECT NO: 3576-01-75

HWY: STH 106

COUNTY: JEFFERSON

PLAN DETAILS

SHEET

E

FILE NAME : S:\PROJECTS\W11611 WISDOT - STH 106 RESURFACING, JEFFERSON CO\SHEETS\PLAN\PLAN & PROFILE SHEETS\35760175\_PLAN DETAILS.DWG

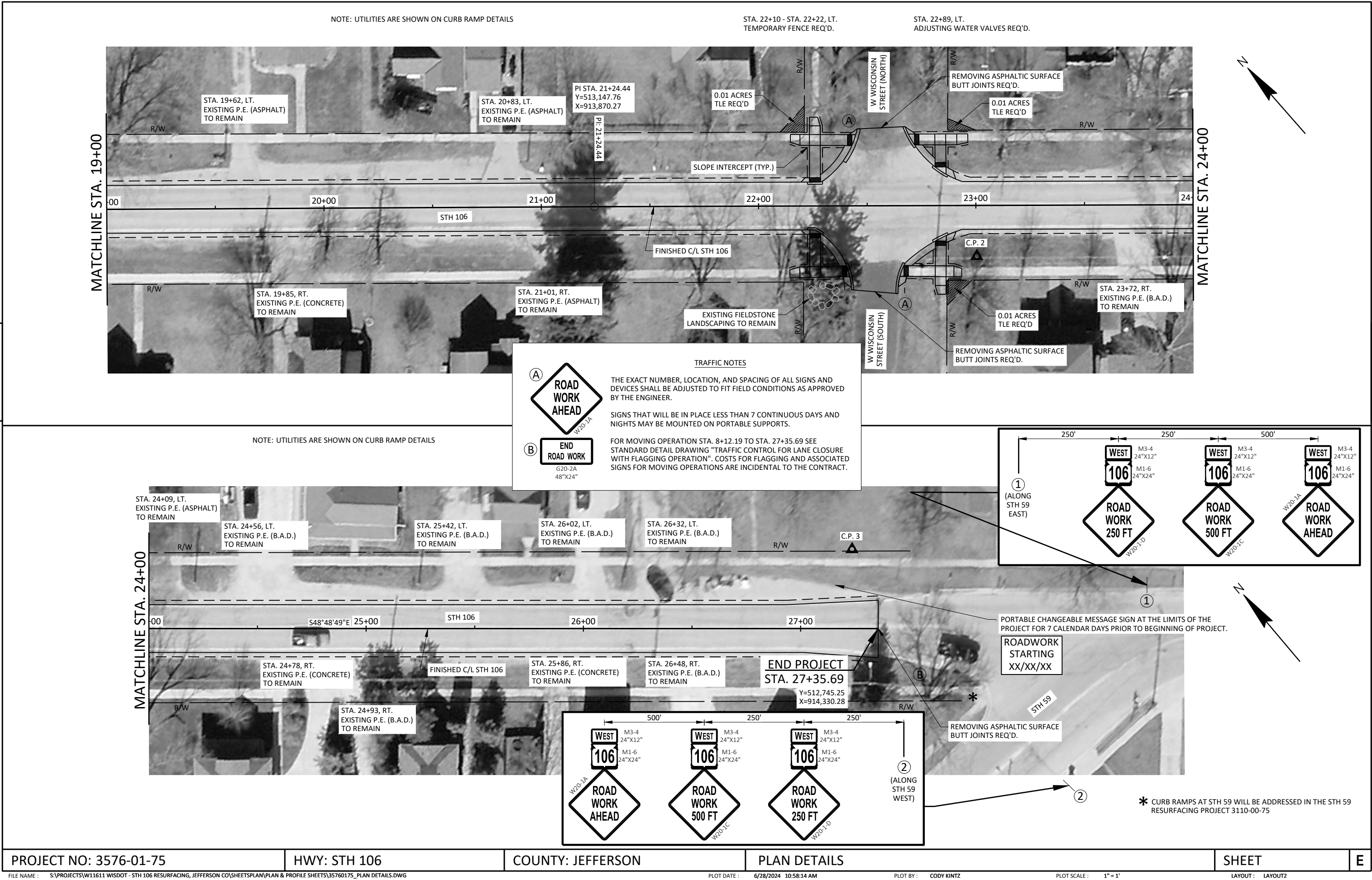
PLOT DATE : 6/28/2024 10:58:07 AM

PLOT BY : CODY KINTZ

PLOT SCALE : 1" = 1'

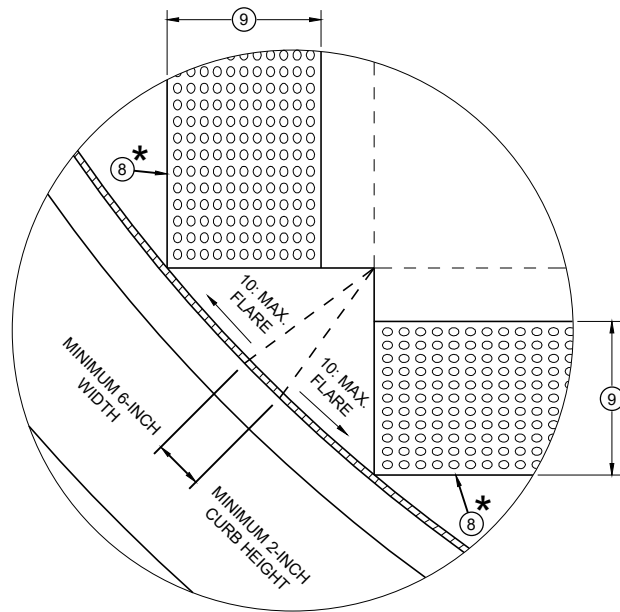
LAYOUT : LAYOUT1



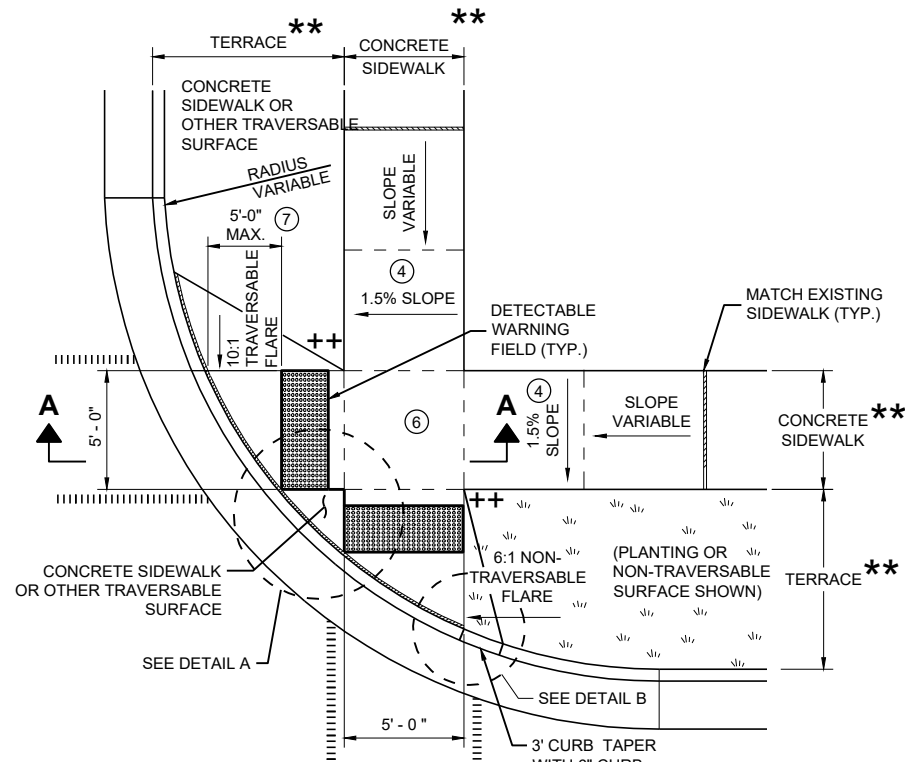


Standard Detail Drawing List

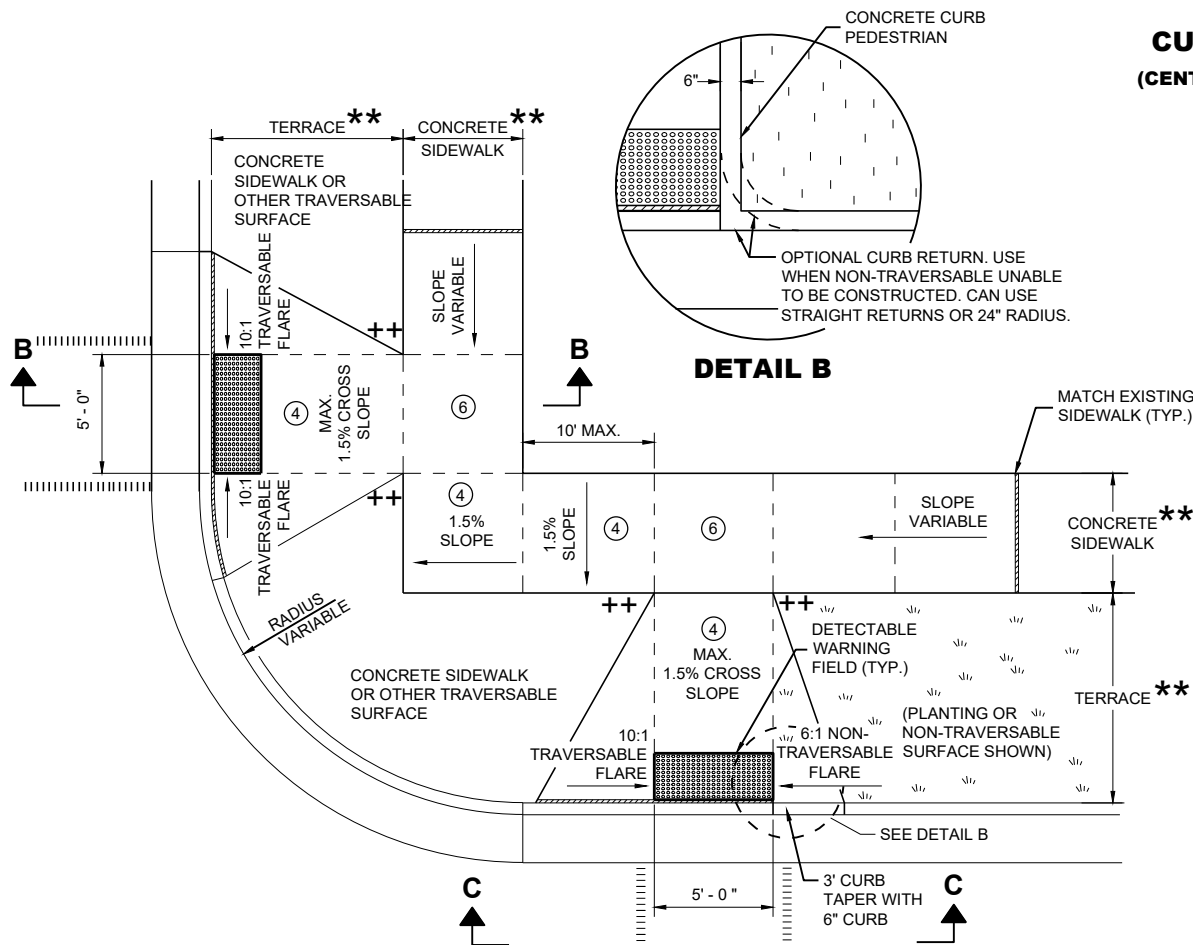
08D05-21B	CURB RAMPS TYPES 2 AND 3
08D05-21D	CURB RAMPS TYPE 4B AND 4B1
08D05-21E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-21F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-21G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D21-01	DRIVEWAYS WITHOUT CURB & GUTTER
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
13C19-03	HMA LONGITUDINAL JOINTS
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C08-23B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C19-09A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C35-06A	PAVEMENT MARKING (INTERSECTIONS)
15D30-09A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09B	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09E	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09G	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09J	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09K	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09L	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D39-02	TRAFFIC CONTROL, DROP-OFF SIGNING
15D44-02	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES



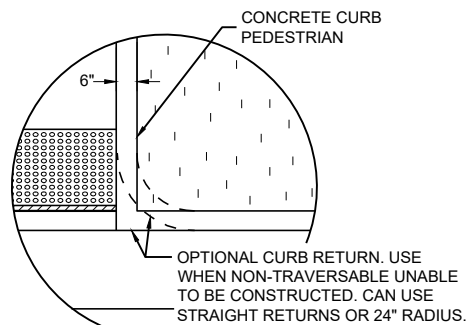
DETAIL A



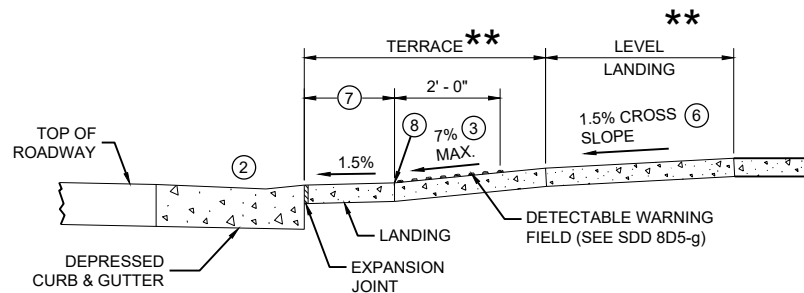
PLAN VIEW  
CURB RAMP TYPE 2  
(CENTER OF CORNER RADIUS)



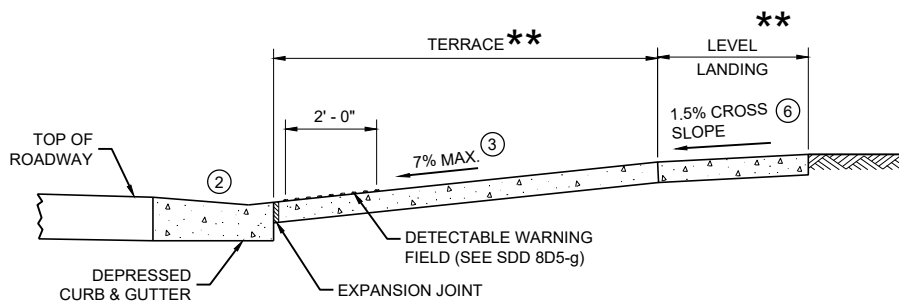
PLAN VIEW  
CURB RAMP TYPE 3  
(OUTSIDE OF CROSSWALK AREA)



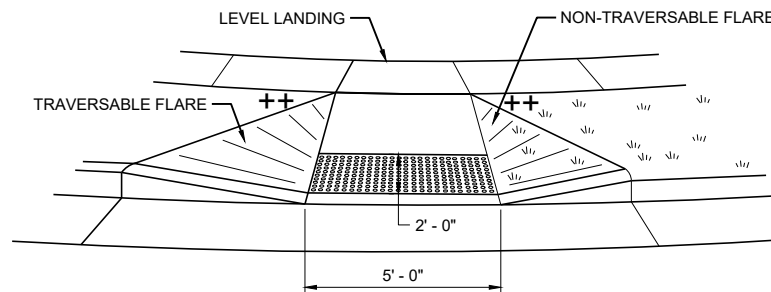
DETAIL B



SECTION A - A FOR TYPE 2



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

### GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
  - AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
  - ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
  - PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
  - WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
  - PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
  - WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

\* MAXIMUM 2.0% SLOPE  
IN ALL DIRECTIONS IN  
FRONT OF GRADE BREAK

\*\* WIDTH SHOWN ELSEWHERE  
IN THE PLANS

++ CONSTRUCT 6" WEDGE TO  
AVOID CONCRETE BREAKAGE

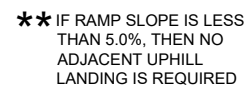
### LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

### CURB RAMPS TYPE 2 AND 3

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION





### SECTION B - B FOR TYPE 4B AND TYPE 4B1

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 ¼"	0' - 5"	2' - 1"	1' - 4 ½"	1' - 5"	2' - 1"	0' - 10"	2' - 7 ½"	0' - 3 ¼"	3' - 0 ¼"						
15 FEET	4' - 6 ¾"	2' - 1 ¾"	3' - 9"	3' - 5 ¾"	3' - 1 ¼"	4' - 6"	2' - 6 ¾"	5' - 4 ½"	2' - 1"	6' - 1"	1' - 8"	6' - 8 ½"	1' - 3 ¾"	7' - 2 ½"	0' - 10 ¾"	7' - 7 ½"
20 FEET			4' - 11 ½"	5' - 1 ¾"	4' - 3 ¼"	6' - 5 ½"	3' - 8 ¾"	7' - 7"	3' - 3"	8' - 6 ½"	2' - 10"	9' - 4 ½"	2' - 5 ½"	10' - 1 ¼"	2' - 1 ½"	10' - 9"
30 FEET									4' - 10 ¾"	12' - 5 ¾"	4' - 5 ½"	13' - 7 ¾"	4' - 0 ¾"	14' - 8 ½"	3' - 8 ½"	15' - 8 ½"
40 FEET															4' - 10 ¾"	19' - 8 ½"

INTERMEDIATE RADII CAN BE INTERPOLATED  
 DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH  
 DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH



++ CONSTRUCT 6" WEDGE TO  
AVOID CONCRETE BREAKAGE

## GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.


- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/2 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

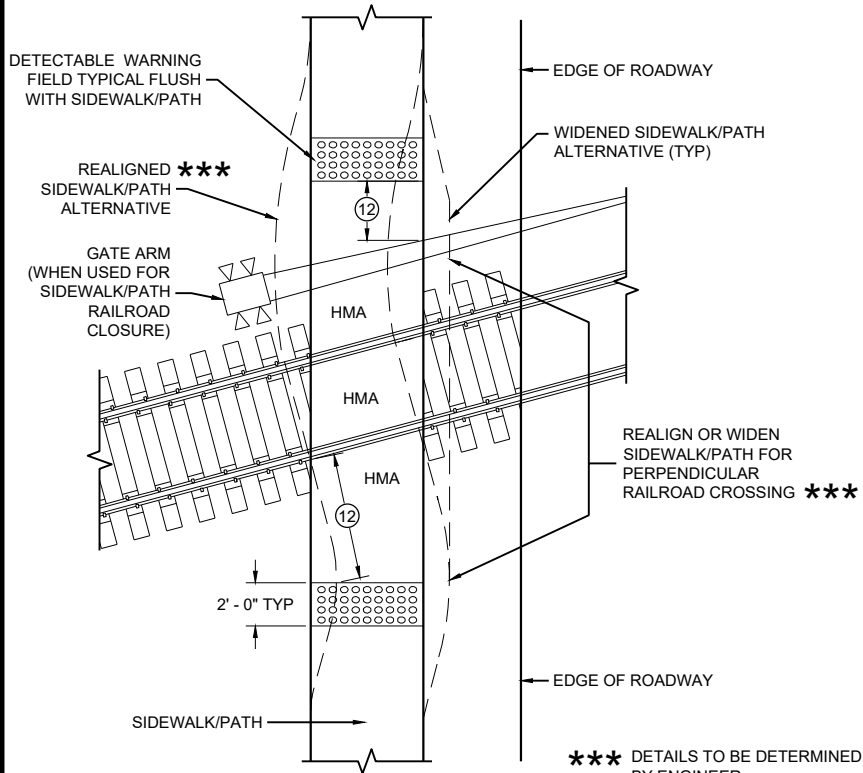


## CURB RAMPS TYPE 4B AND 4B1

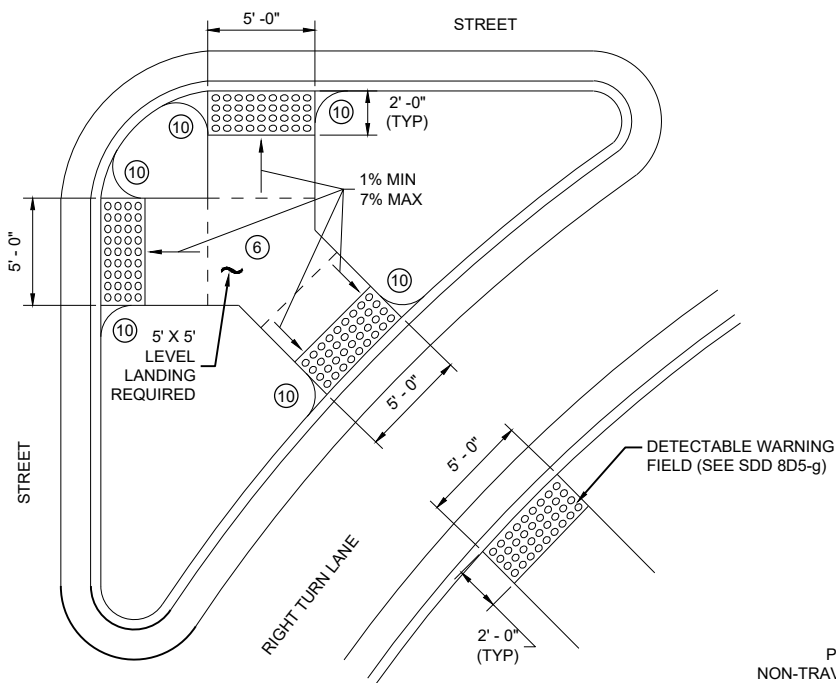
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

## LEGEND

- |   |                                       |
|---|---------------------------------------|
|  | 1/2" EXPANSION JOINT SIDEWALK         |
| - - - -   | CONTRACTION JOINT SIDEWALK            |
|   | PAVEMENT MARKING<br>CROSSWALK (WHITE) |

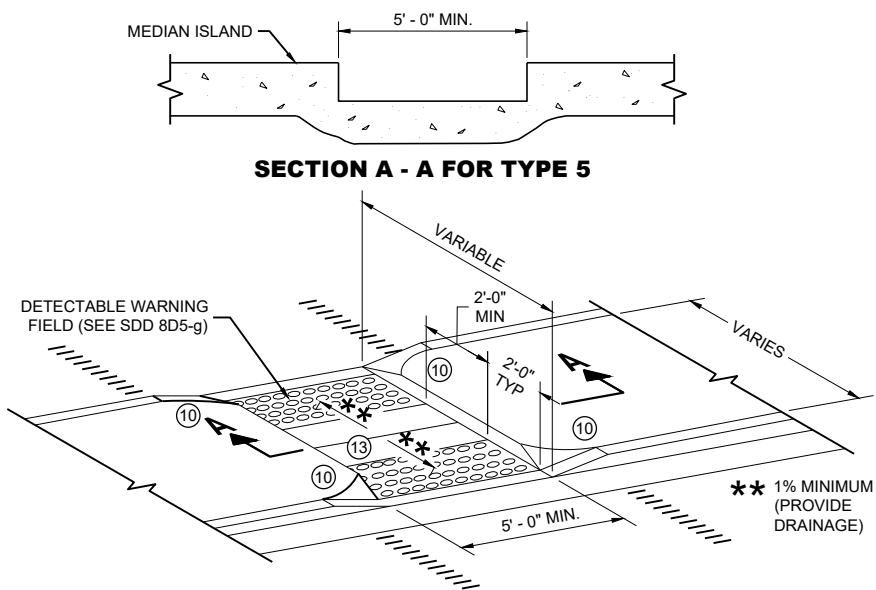


**CURB RAMP TYPE 8**  
**DETECTABLE WARNINGS**  
**FOR SIDEWALKS OR SHARED USE PATHS**  
**AT RAILROAD CROSSINGS**

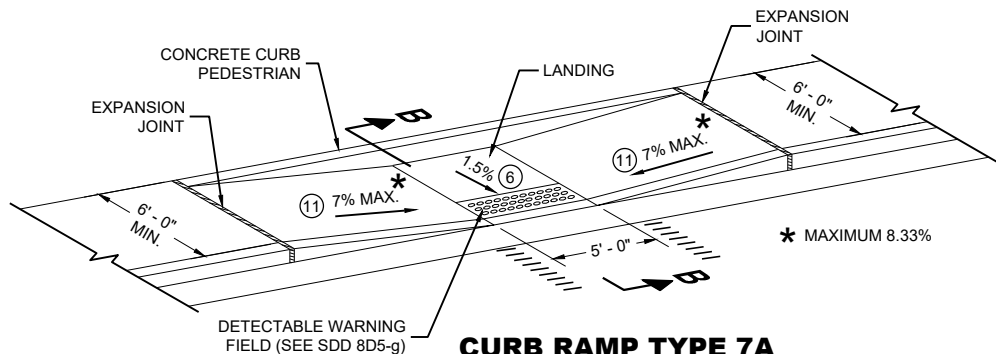


**CURB RAMP TYPE 6**  
**DETECTABLE WARNING AT ISLANDS**

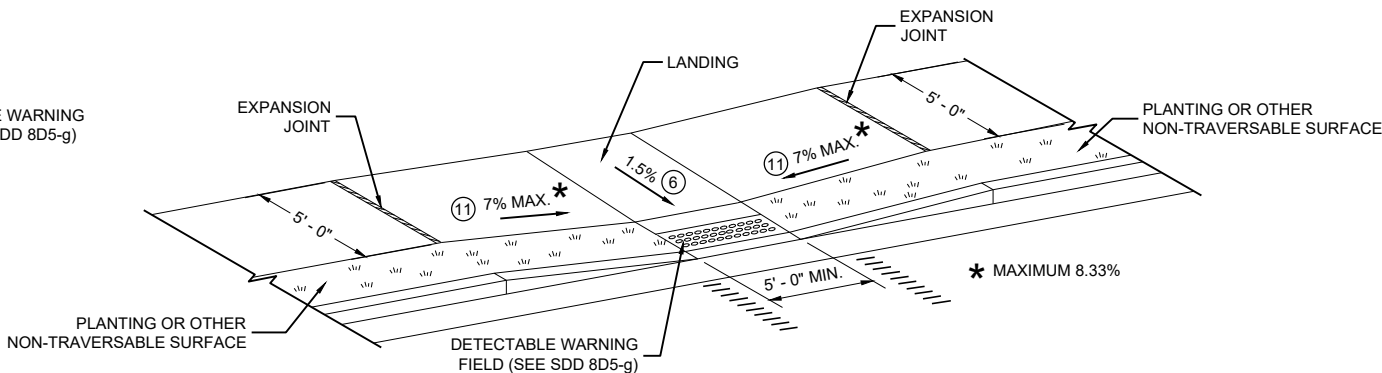
REFER TO GENERAL NOTES ② AND ③  
FOR ALL ISLAND CURB RAMPS



**CURB RAMP TYPE 5**  
**MEDIAN ISLAND**  
**NON-ELEVATED PEDESTRIAN CROSSING**



**CURB RAMP TYPE 7A**  
**FOR INTERSECTIONS AND**  
**MID BLOCK CROSSINGS**



**CURB RAMP TYPE 7B**  
**FOR INTERSECTIONS AND**  
**MID BLOCK CROSSINGS**

**GENERAL NOTES**

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

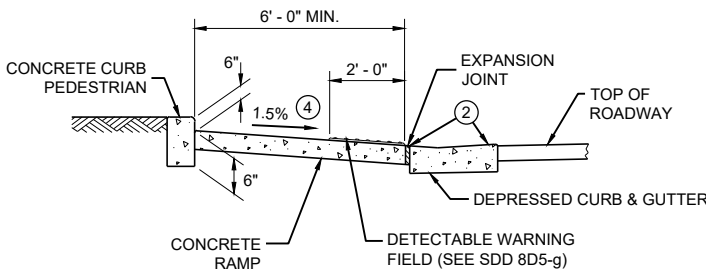
SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/8 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- ⑪ SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- ⑫ THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK/PATH. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD TRACK IS 15 FEET MAXIMUM AND 12 FEET MINIMUM, 15 FEET TYPICAL FROM THE NEAREST RAIL.
- ⑬ DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STEET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

**LEGEND**

- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

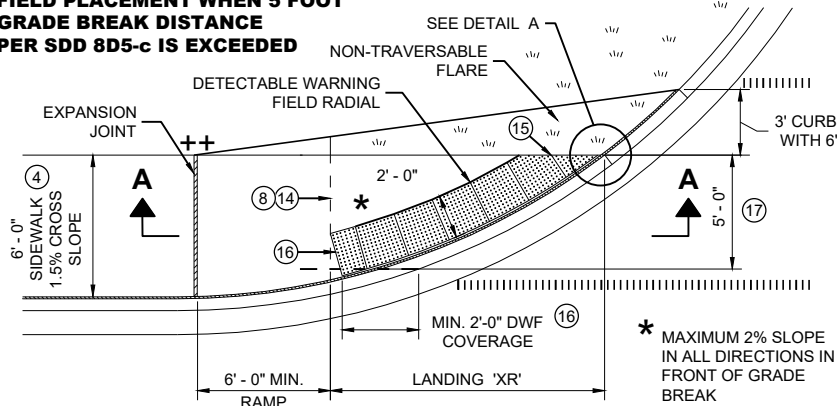


**SECTION B - B FOR TYPE 7A**

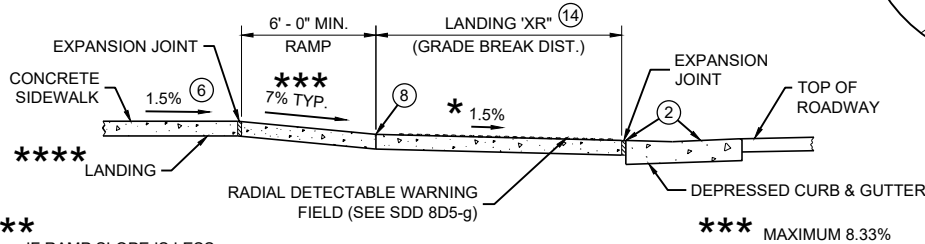
**CURB RAMPS**  
**TYPE 5, 6, 7A, 7B & 8**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

**RADIAL DETECTABLE WARNING  
FIELD PLACEMENT WHEN 5 FOOT  
GRADE BREAK DISTANCE  
PER SDD 8D5-c IS EXCEEDED**

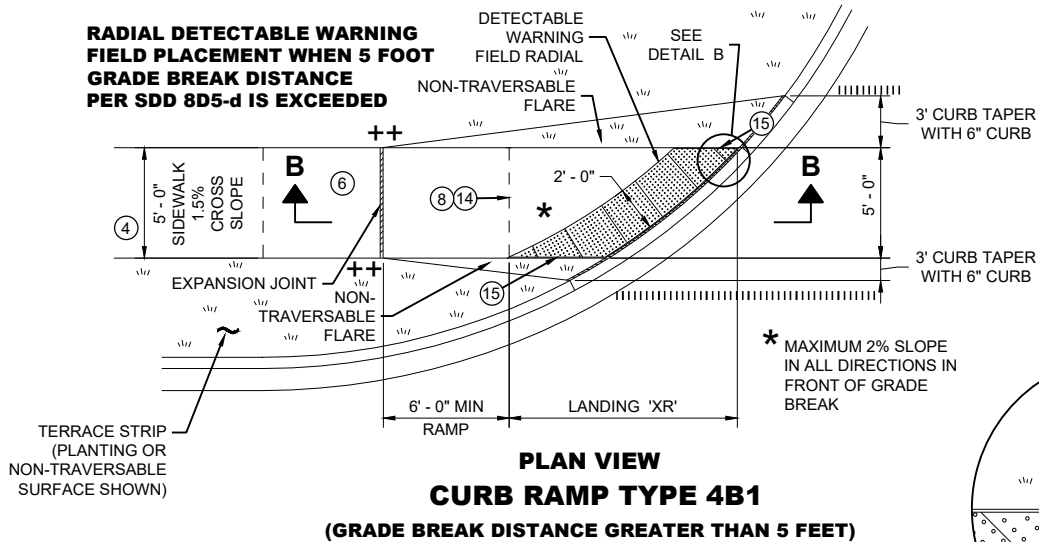


**PLAN VIEW  
CURB RAMP TYPE 4A1  
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**

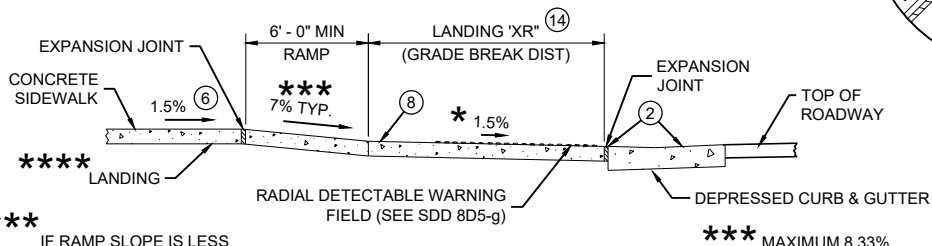


**SECTION A - A FOR TYPE 4A1**

**RADIAL DETECTABLE WARNING  
FIELD PLACEMENT WHEN 5 FOOT  
GRADE BREAK DISTANCE  
PER SDD 8D5-d IS EXCEEDED**



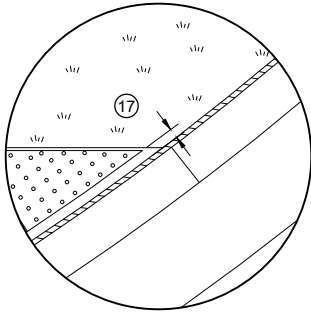
**PLAN VIEW  
CURB RAMP TYPE 4B1  
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**



**SECTION B - B FOR TYPE 4B1**

**LEGEND**

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)



**DETAIL A**

**GENERAL NOTES**

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B CURB RAMPS ARE NOT SHOWN.

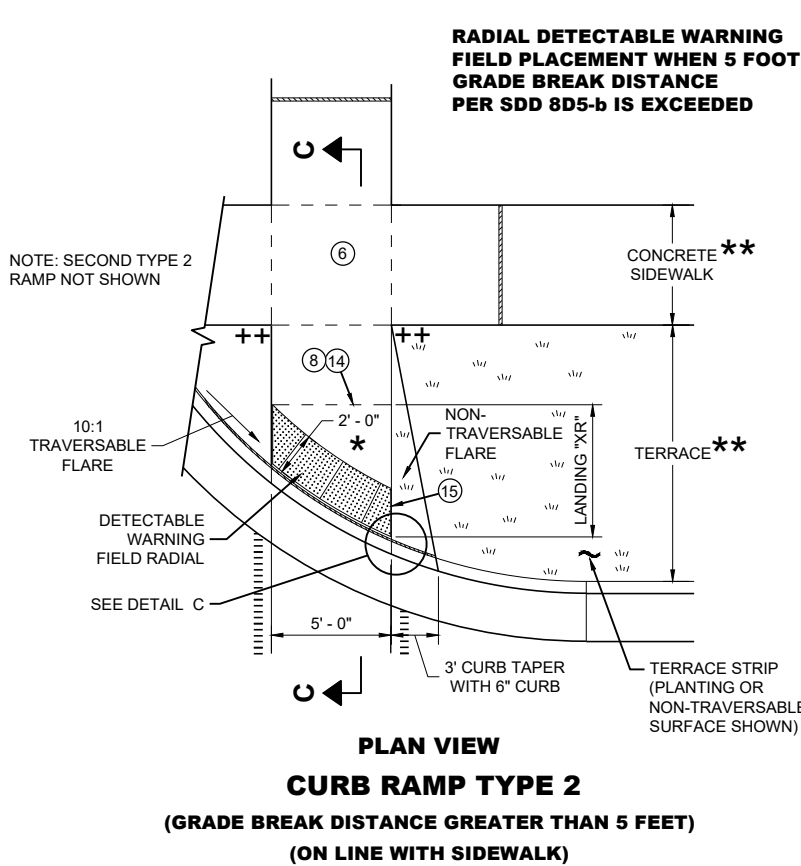
REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

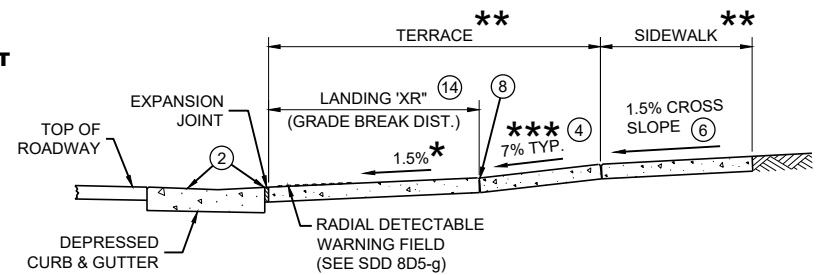
DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

- 2 GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- 8 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 14 CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- 15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/2" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- 16 USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2'-0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- 17 A MAXIMUM 3 INCH CONCRETE BORDER WIDTH IS ALLOWABLE IN FRONT OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

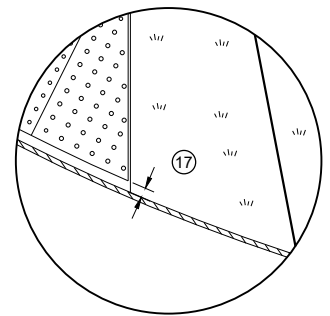
**RADIAL DETECTABLE WARNING  
FIELD PLACEMENT WHEN 5 FOOT  
GRADE BREAK DISTANCE  
PER SDD 8D5-b IS EXCEEDED**



**PLAN VIEW  
CURB RAMP TYPE 2  
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)  
(ON LINE WITH SIDEWALK)**



**SECTION C - C FOR TYPE 2**



**DETAIL C**

\*\*\* MAXIMUM 2% SLOPE  
IN ALL DIRECTIONS IN  
FRONT OF GRADE  
BREAK

\*\* WIDTH SHOWN ELSEWHERE  
IN THE PLANS

\*\*\* MAXIMUM 8.33%

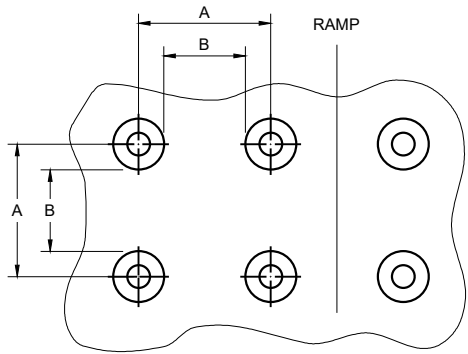
++ CONSTRUCT 6" WEDGE TO  
AVOID CONCRETE BREAKAGE

**CURB RAMPS  
RADIAL DETECTABLE WARNING**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

\* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

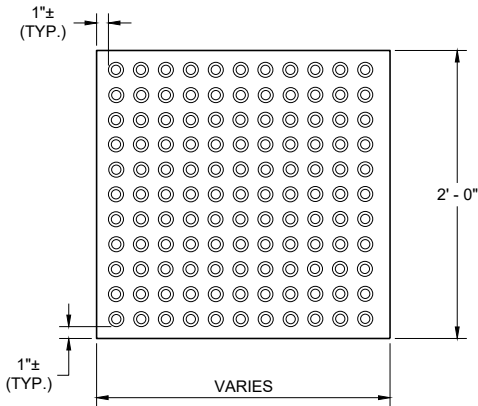


PLAN VIEW

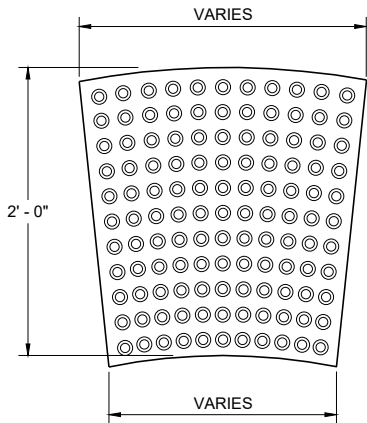


ELEVATION VIEW

TRUNCATED DOMES  
DETECTABLE WARNING PATTERN DETAIL

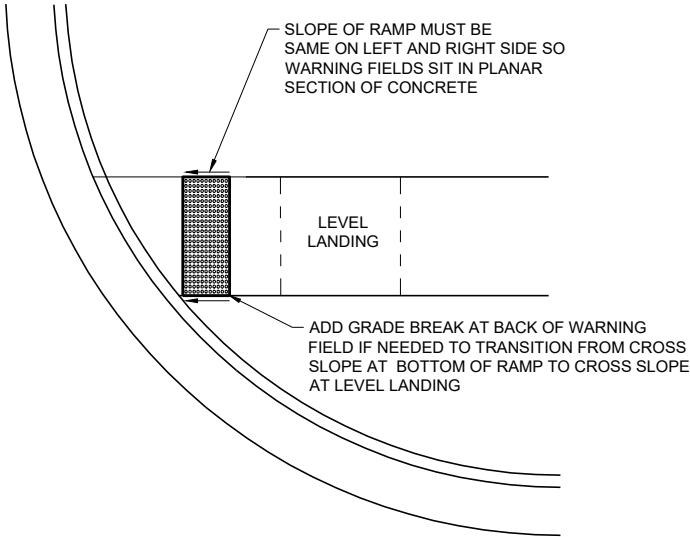


RECTANGULAR  
PLATES

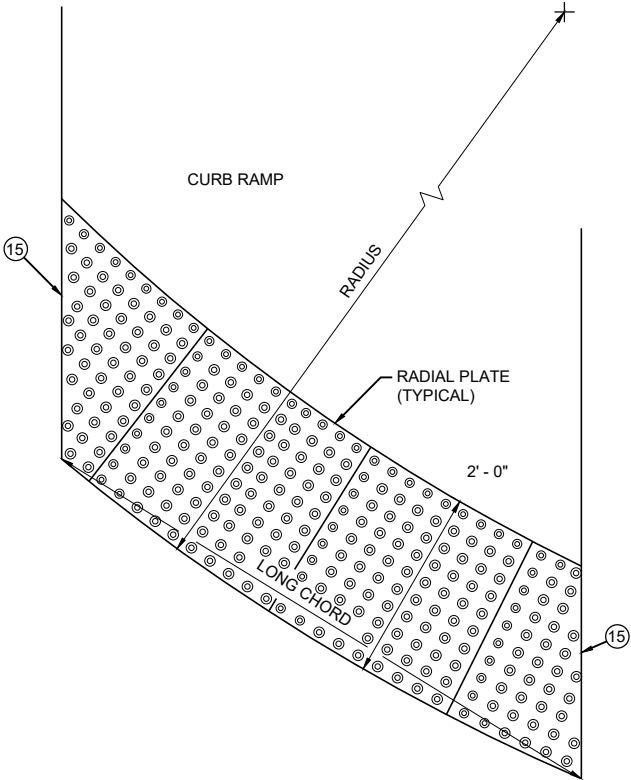


RADIAL  
PLATES

PLAN VIEW  
DETECTABLE WARNING FIELDS (TYPICAL)



DETECTABLE WARNING FIELD  
PLANAR INSTALLATION



PLAN VIEW  
RADIAL DETECTABLE  
WARNING FIELD ATTRIBUTES

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.

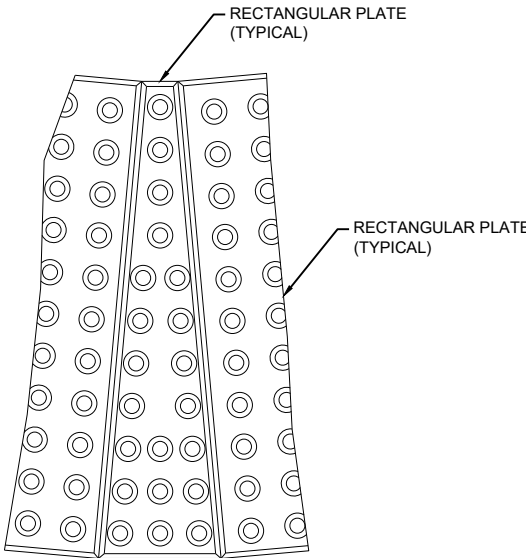
DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8\"/>

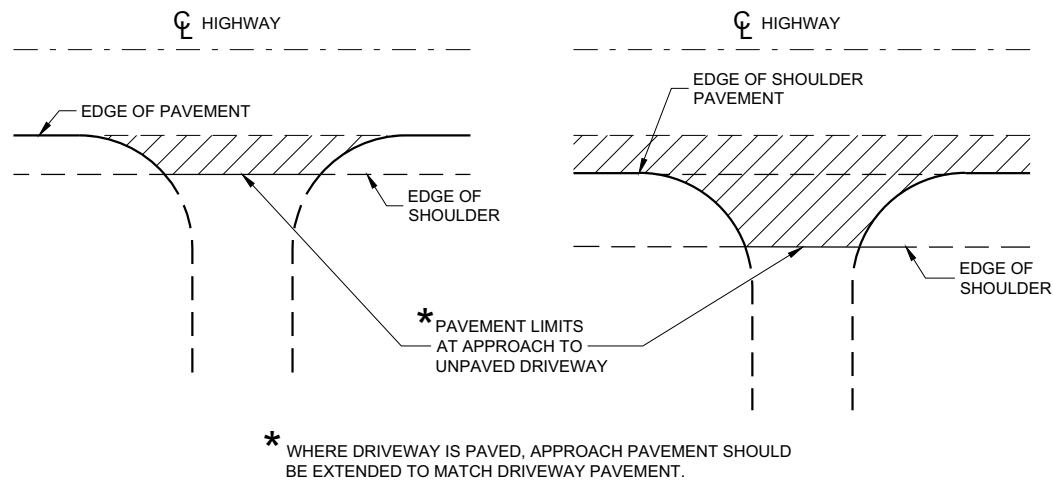


PLAN VIEW  
RADIAL WEDGE PLATE  
CONNECTION DETAIL

CURB RAMPS  
RECTANGULAR AND RADIAL  
DETECTABLE WARNING PLATES

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

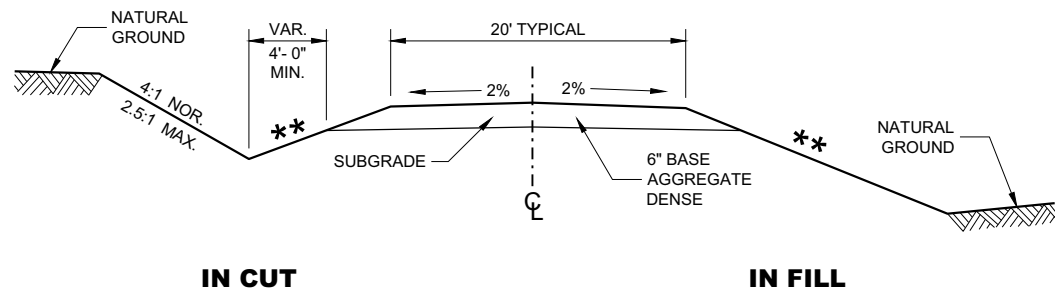
APPROVED  
July 2023 /S/ Rodney Taylor  
DATE ROADWAY STANDARDS DEVELOPMENT  
FHWA UNIT SUPERVISOR



**PLAN VIEW**  
(UNPAVED SHOULDER ON HIGHWAY)

**PLAN VIEW**  
(PAVED SHOULDER ON HIGHWAY)

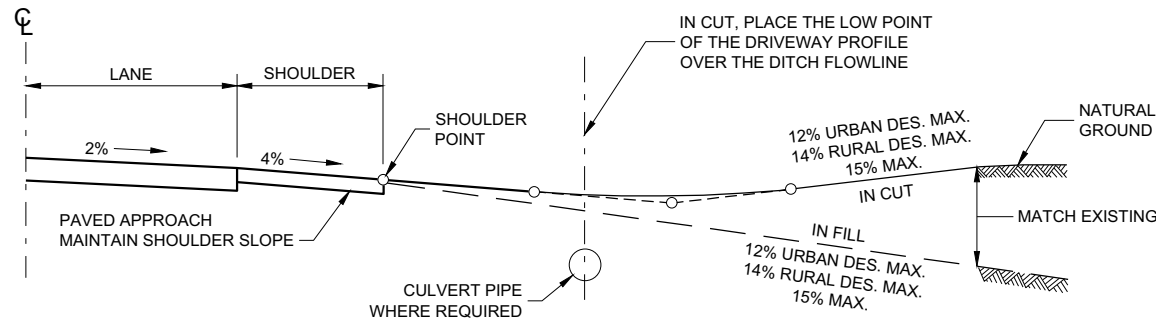
**RURAL DRIVEWAY INTERSECTION DETAIL  
(NO CURB AND GUTTER OR SIDEWALK)**



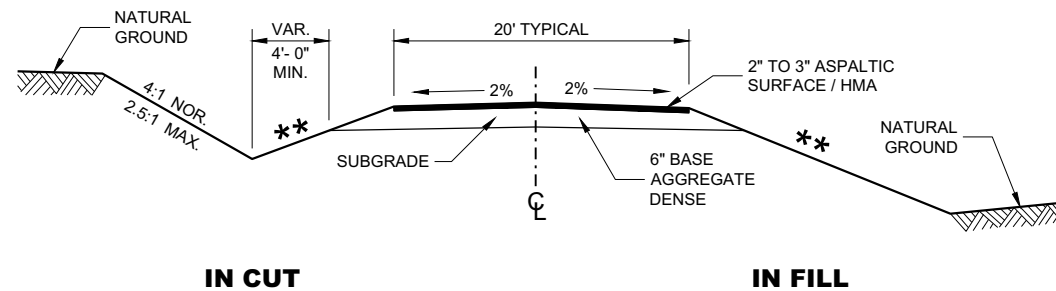
**TYPICAL CROSS SECTION FOR  
PRIVATE DRIVE OR FIELD ENTRANCE  
AGGREGATE SURFACE**

**\*\*** SLOPE CAN VARY WITH SPEED. SEE 11-45-30.6.2

POSTED SPEED MPH	MAX. SLOPE
<35	4:1
≥ 35 TO < 60	6:1
≥60	10:1



**TYPICAL DRIVEWAY PROFILES**



**TYPICAL CROSS SECTION FOR  
PRIVATE DRIVE OR FIELD ENTRANCE  
ASPHALTIC SURFACE**

**DRIVEWAYS WITHOUT  
CURB AND GUTTER**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
December 2017  
DATE /S/ Rodney Taylor  
ROADWAY STANDARDS DEVELOPMENT  
UNIT SUPERVISOR  
FHWA

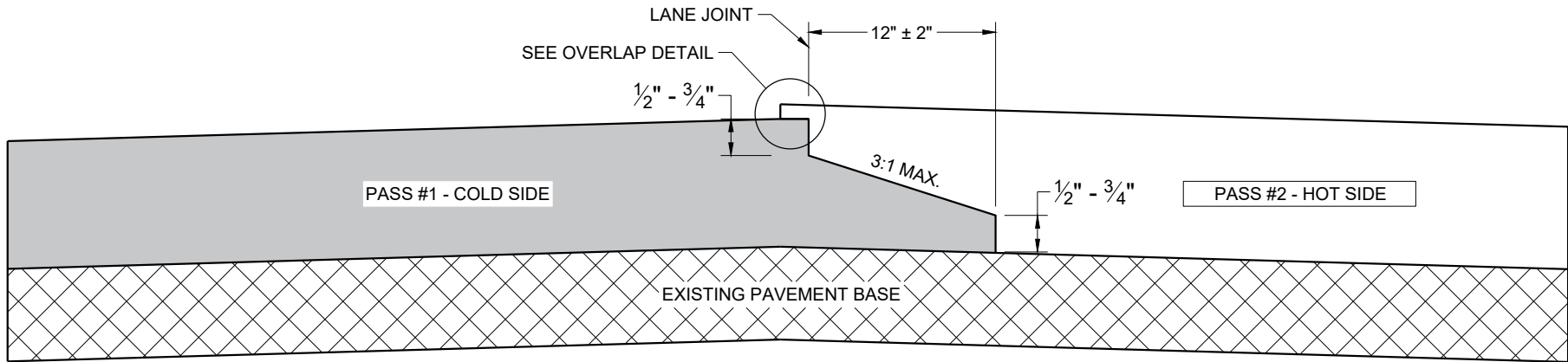


- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1½" X 1½" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

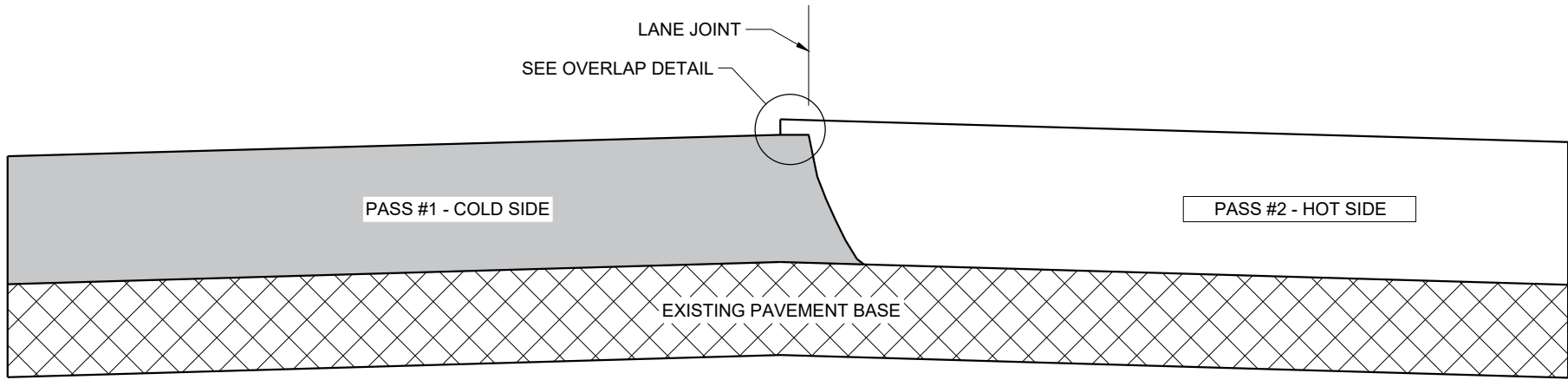


<b>SILT FENCE</b>	
<b>STATE OF WISCONSIN</b>	
<b>DEPARTMENT OF TRANSPORTATION</b>	
<b>APPROVED</b>	
<u>4-29-05</u>	<u>/S/ Beth Cannestra</u>
<b>DATE</b>	<b>CHIEF ROADWAY DEVELOPMENT ENGINEER</b>
<b>FHWA</b>	

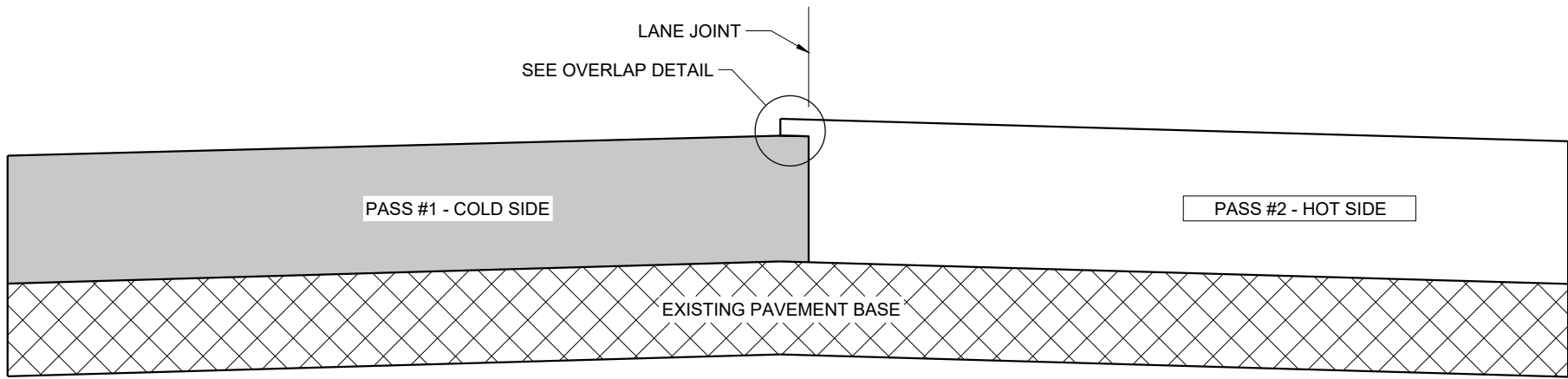




**TYPICAL PAVEMENT CROSS SECTION  
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION  
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION  
VERTICAL JOINT (MILLED)**

**GENERAL NOTES**

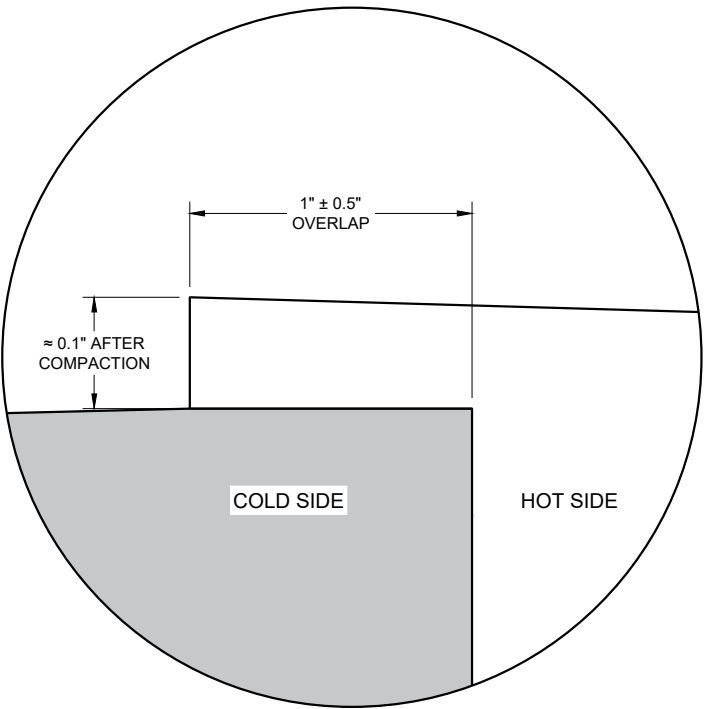
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY  $1" \pm 0.5"$  AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY  $0.1"$  AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO  $2"$  FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



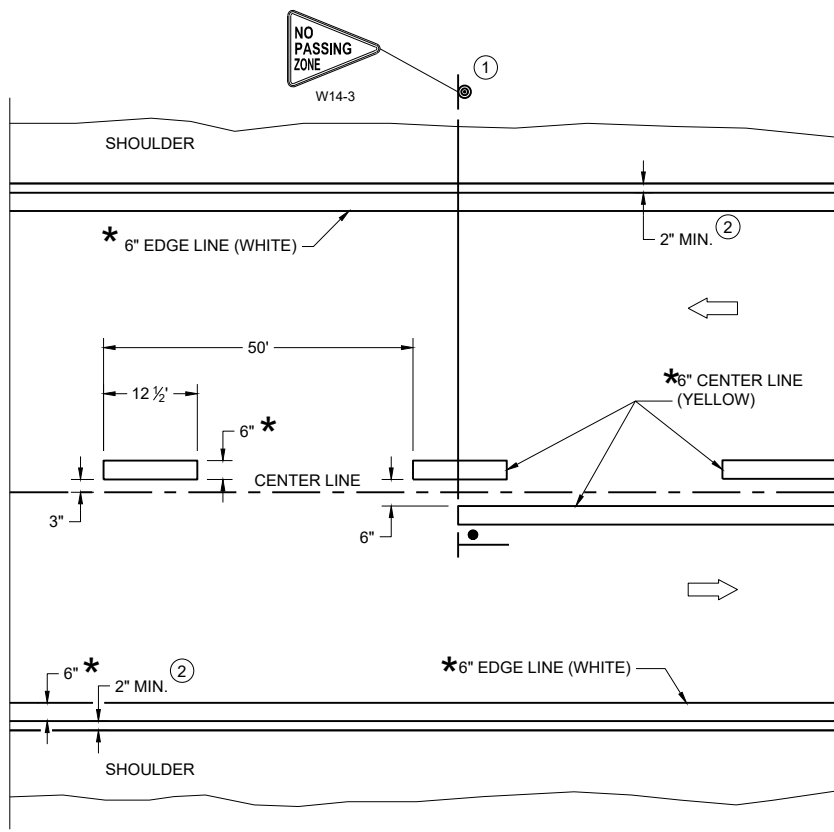
**OVERLAP DETAIL (TYPICAL)**

**HMA LONGITUDINAL JOINTS**

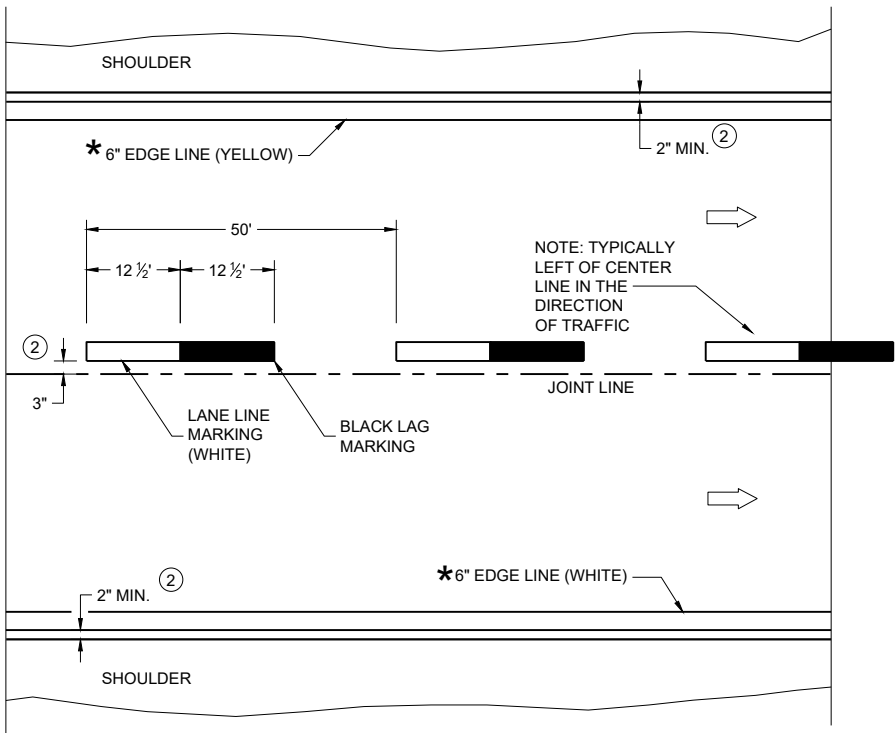
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
November 2020 /S/ Steven Hefel  
DATE HMA PAVEMENT ENGINEER  
FHWA





TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

\* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

GENERAL NOTES

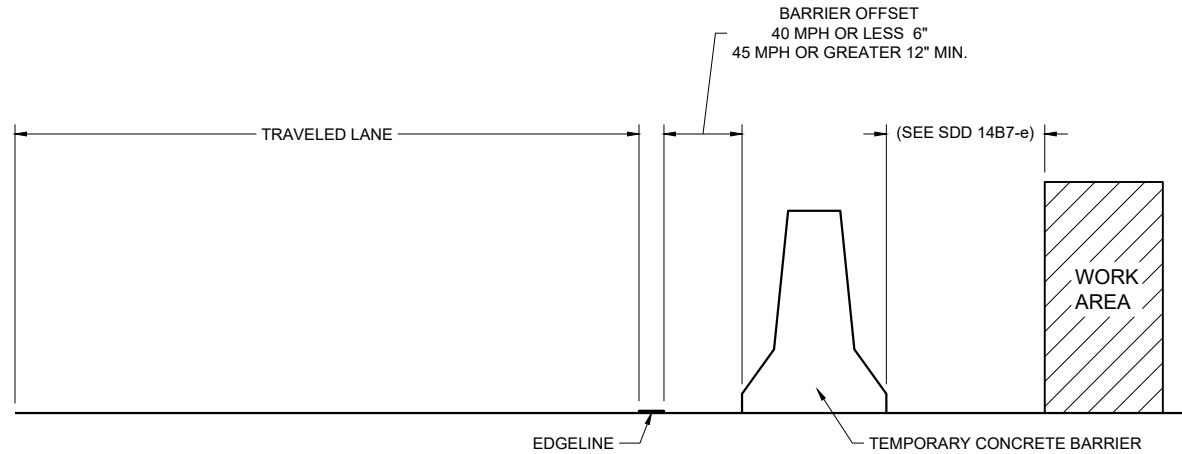
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

- "T" MARKING
- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC

PERMANENT LONGITUDINAL PAVEMENT MARKINGS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Jeannie Silver Statewide Pavement Marking Engineer
FHWA	



TEMPORARY BARRIER OFFSET FROM EDGE LINE

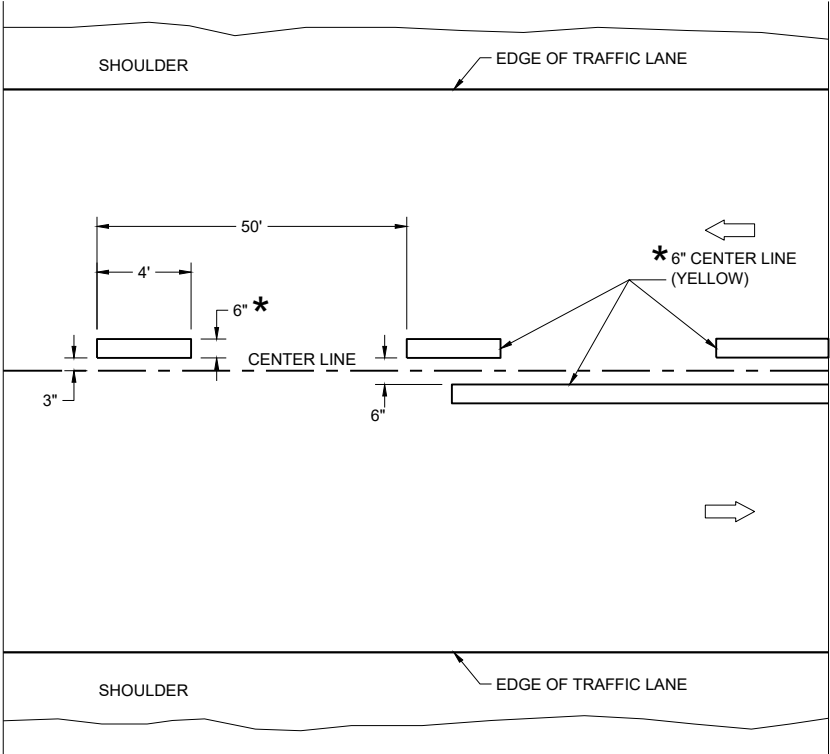
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

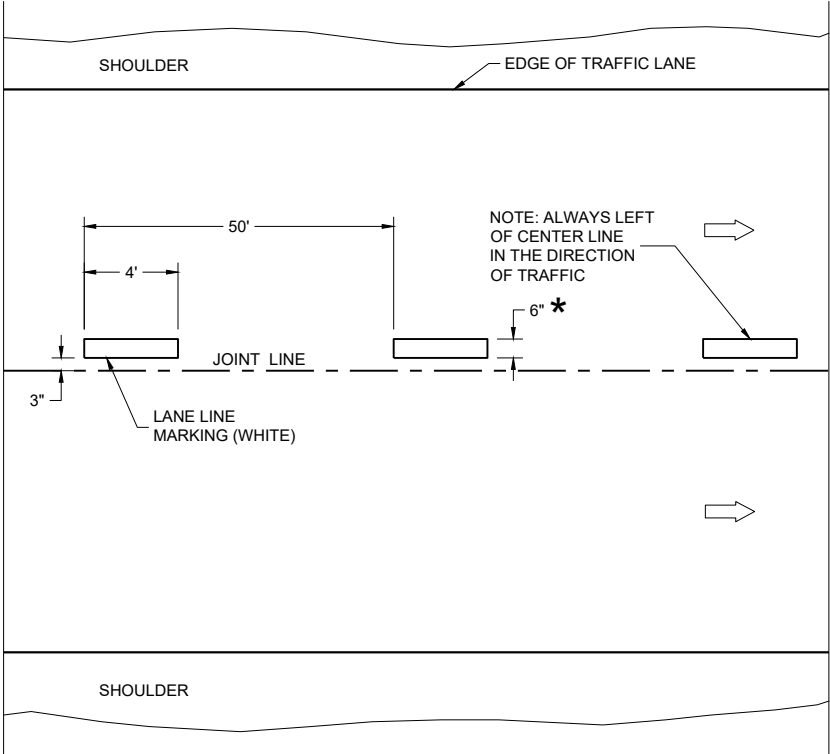
LEGEND

DIRECTION OF TRAFFIC

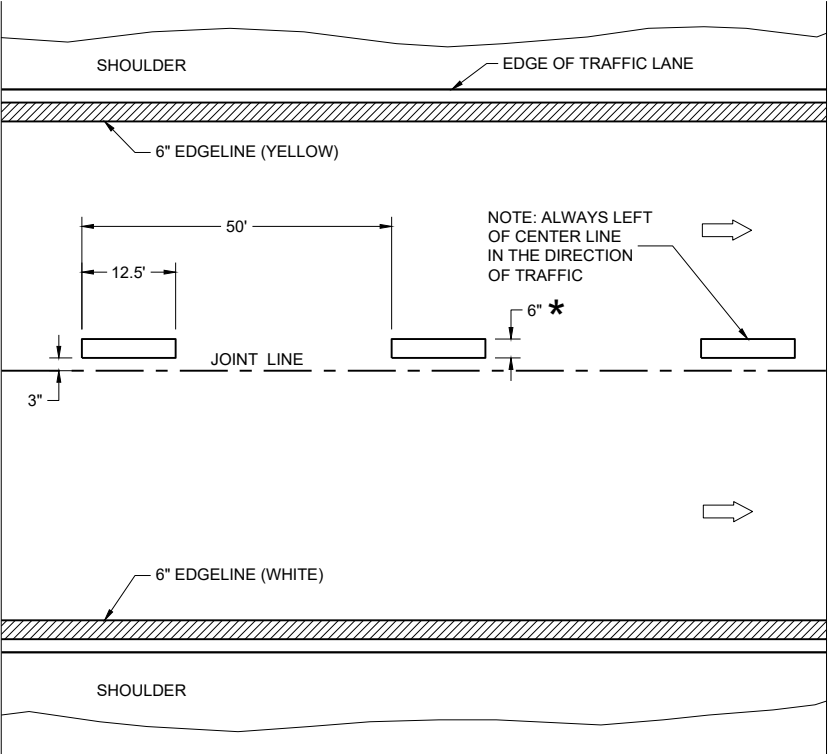
\* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



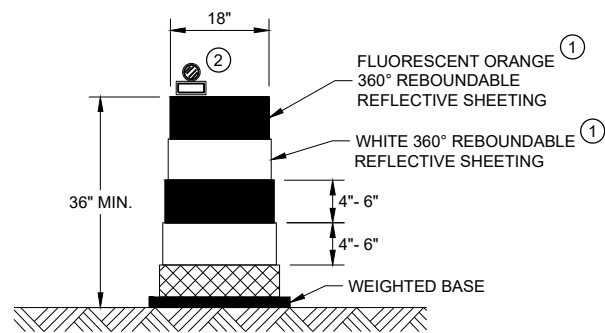
ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

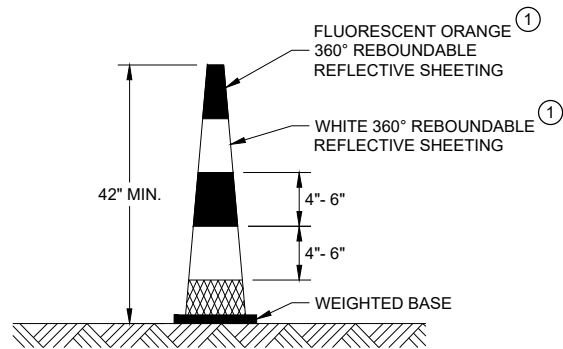
TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Jeannie Silver Statewide Pavement Marking Engineer
FHWA	



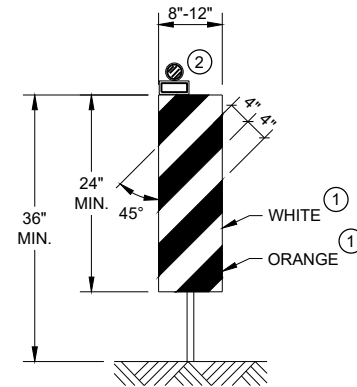
**DRUM**

BALLAST WIDTHS  
RANGE FROM 24"-36"



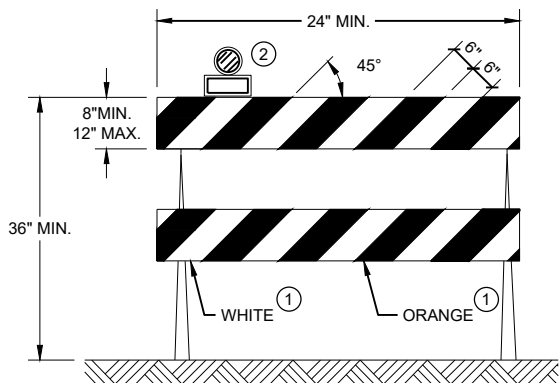
**42" CONE**

DO NOT USE IN TAPERS  
½ SPACING OF DRUMS  
BALLAST WIDTHS  
RANGE FROM 14"-20"



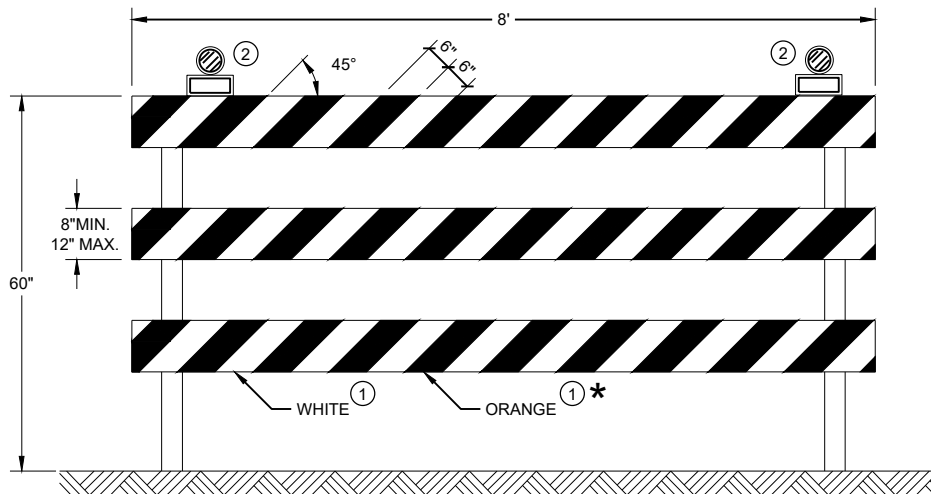
**VERTICAL PANEL**

THE STRIPES SHALL SLOPE DOWNWARD TO  
THE TRAFFIC SIDE FOR CHANNELIZATION.



**TYPE II BARRICADE**

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES  
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD  
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



**TYPE III BARRICADE**

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP  
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

\* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

**GENERAL NOTES**

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.






**CHANNELIZING DEVICES  
DRUMS, CONES, BARRICADES  
AND VERTICAL PANELS**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
November 2022 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

FHWA

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

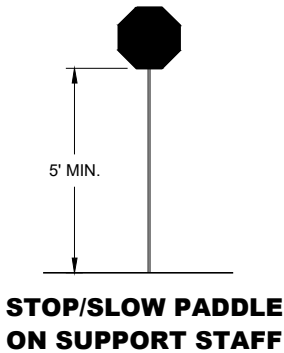
FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

- UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER
- ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.
- INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.
- PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.
- DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

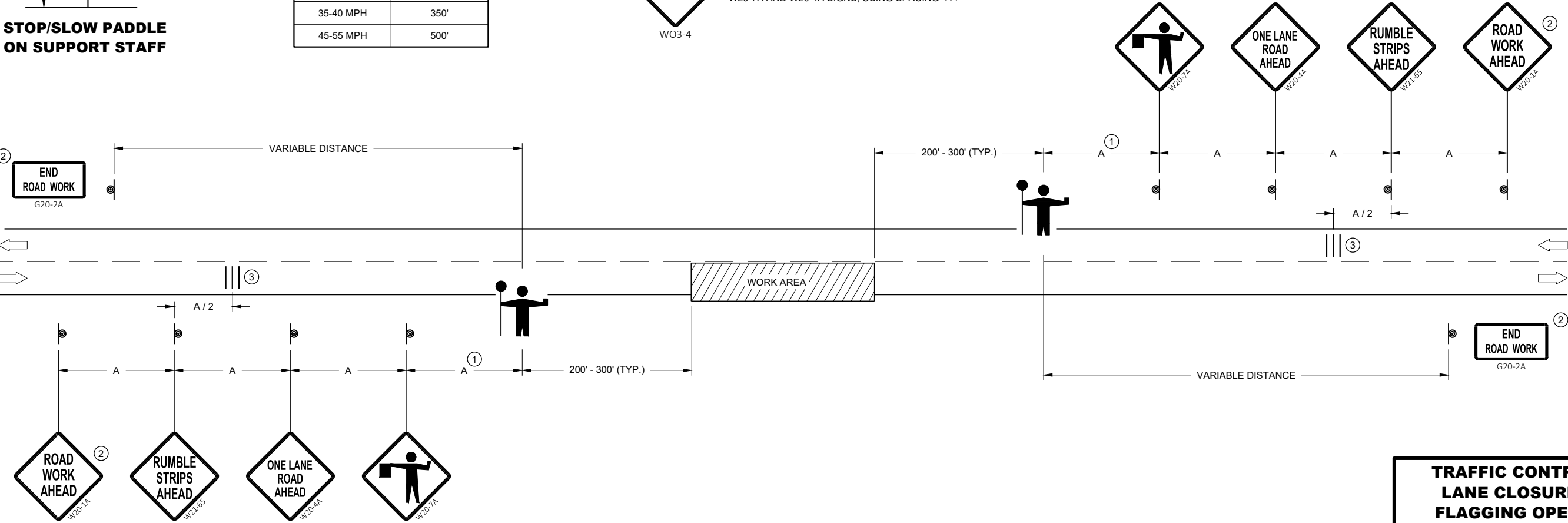


SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



<b>TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

- V1

LEAD VEHICLE
- V2

MARKING VEHICLE
- V3

SHADOW VEHICLE
- TRUCK MOUNTED ATTENUATOR (TMA)
- SIGN ON TEMPORARY SUPPORT
- DIRECTION OF TRAFFIC

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

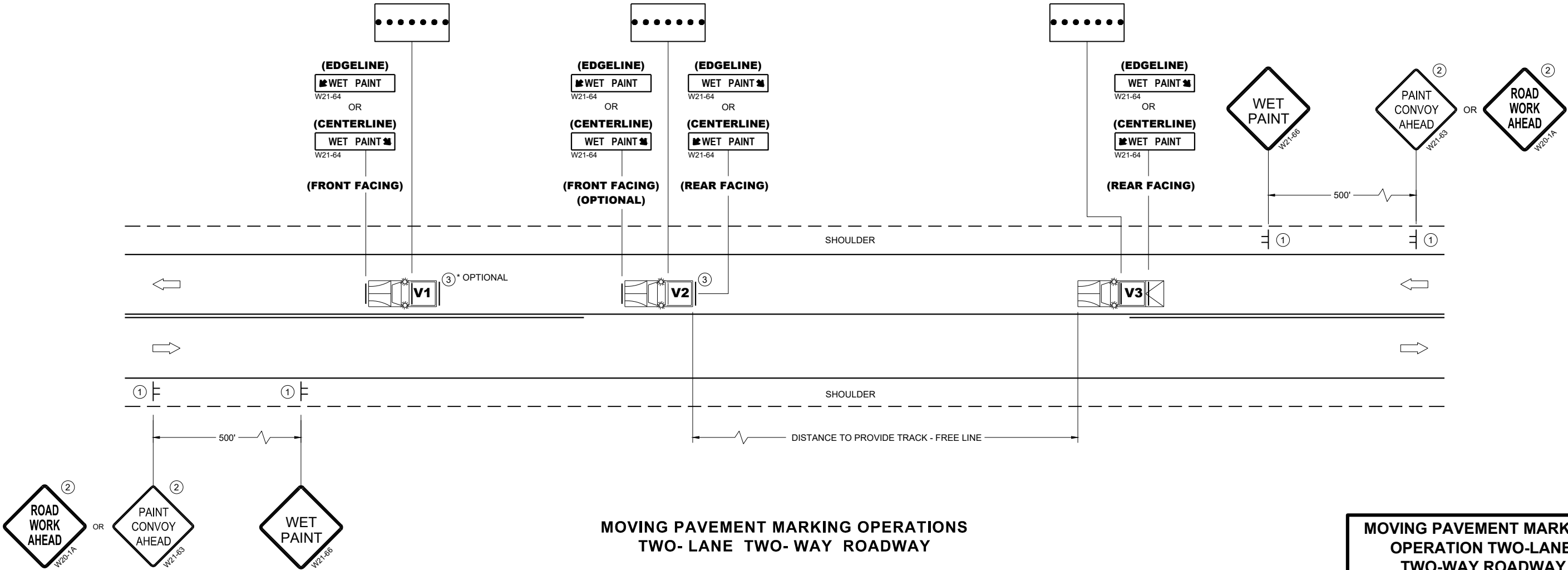
WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH

UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING .

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES AND AFTER EVERY MAJOR INTERSECTION.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.
- ③ V1 AND V2 CAN BE SWITCHED SO THAT THE MARKER IS THE LEAD VEHICLE.

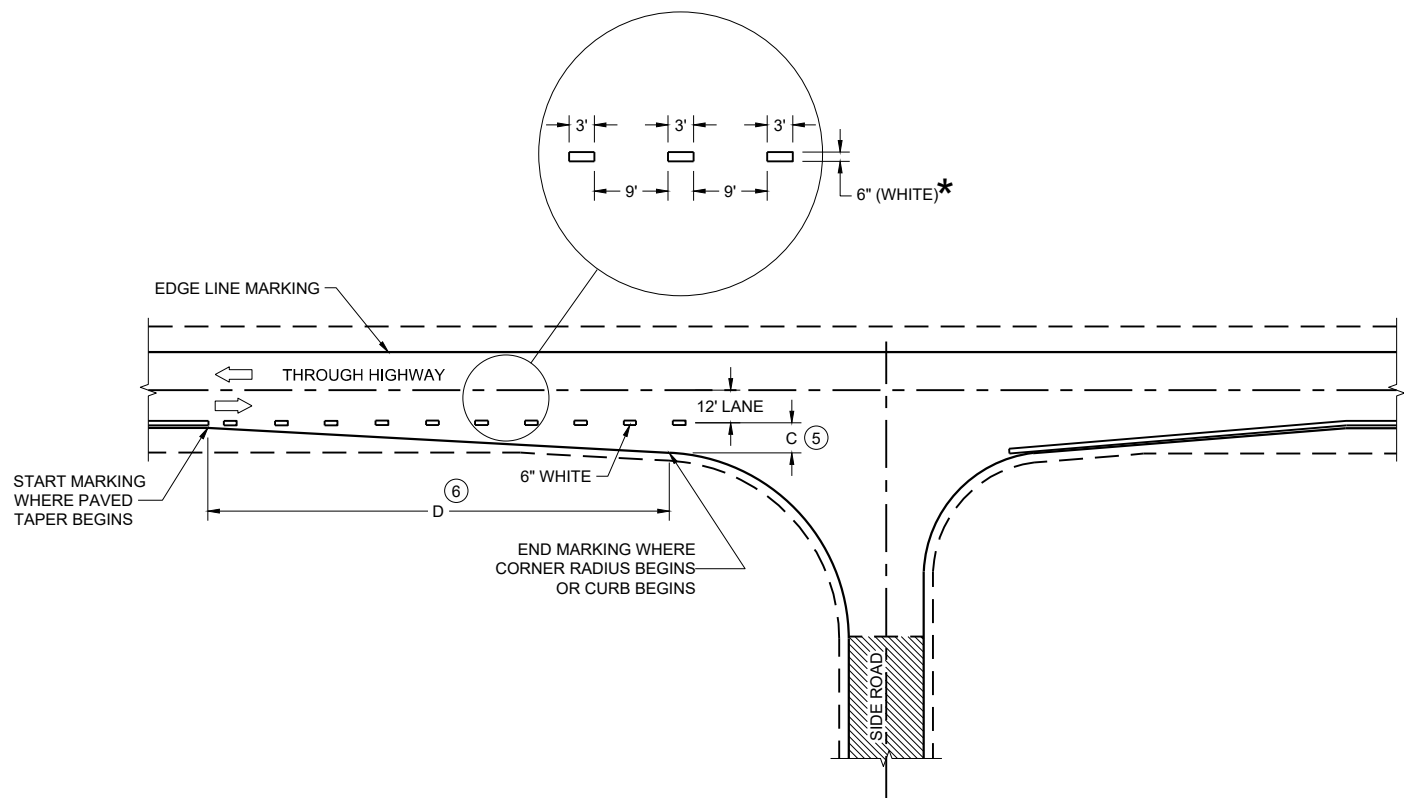


MOVING PAVEMENT MARKING  
OPERATION TWO-LANE  
TWO-WAY ROADWAY

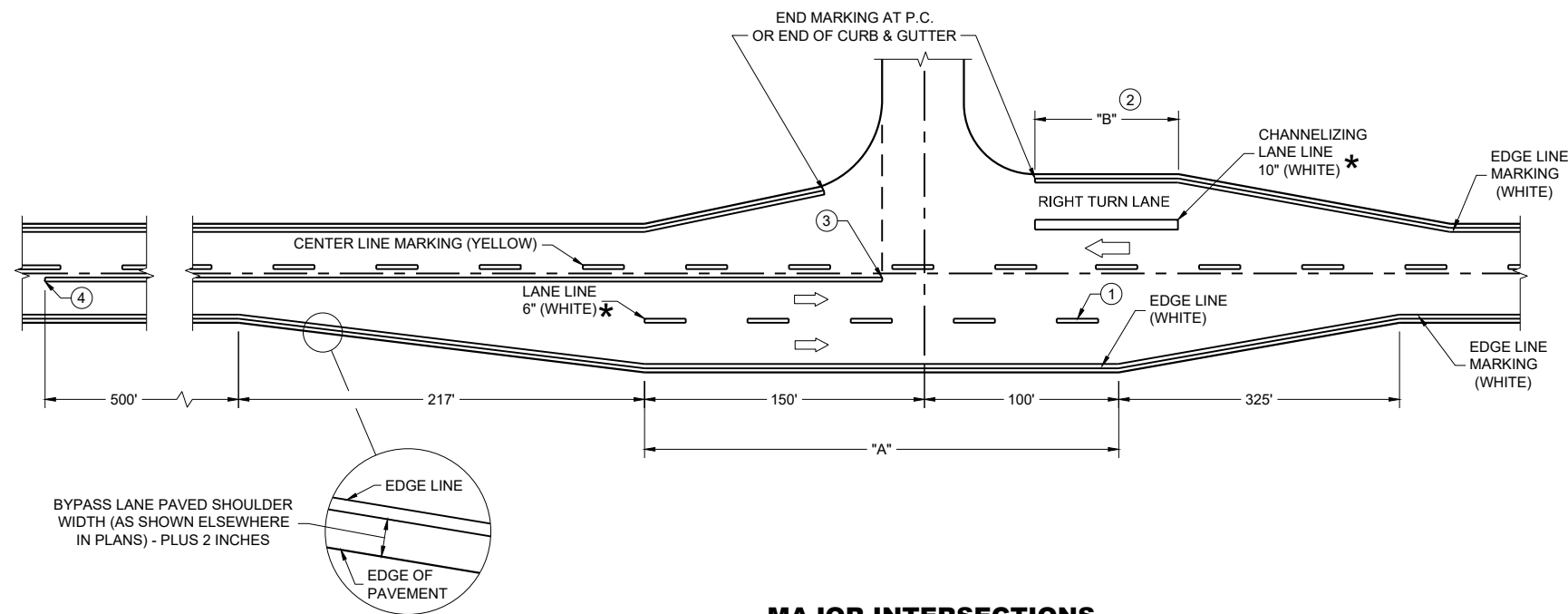
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
March 2024 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

FHWA



MINOR INTERSECTION



MAJOR INTERSECTIONS  
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)

\*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

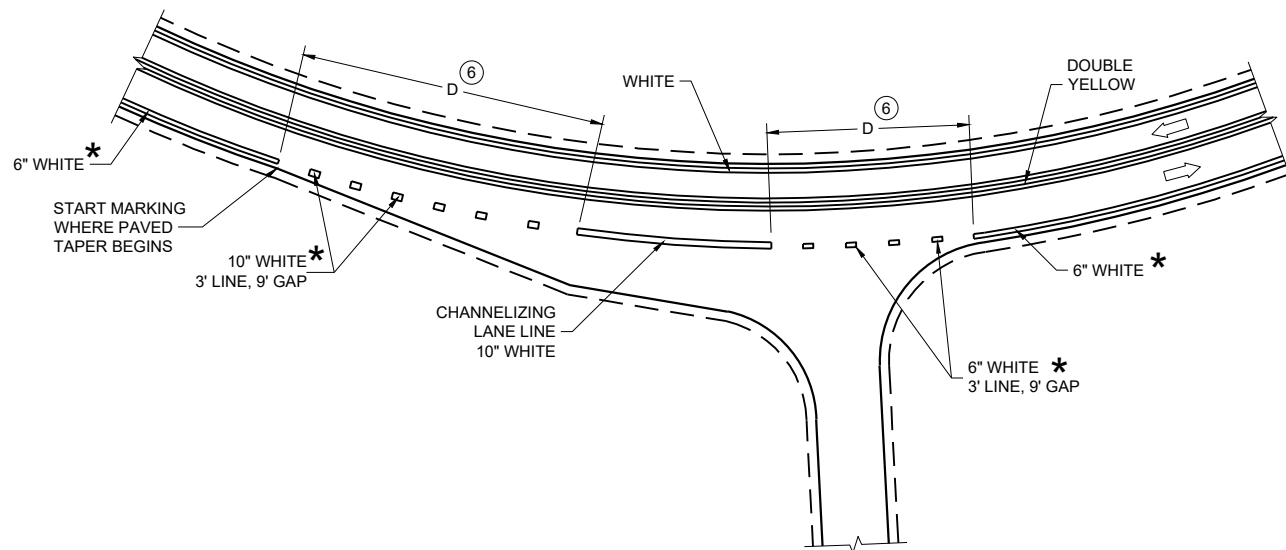
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
- ⑤ WHEN DISTANCE "C" IS LESS THAN 4 FEET, OMIT DOTTED EXTENSION.
- ⑥ WHEN DISTANCE "D" IS LESS THAN 50 FEET, OMIT DOTTED EXTENSION.

LEGEND

➡ DIRECTION OF TRAVEL



INTERSECTION ON OUTSIDE OF CURVE

PAVEMENT MARKING  
(INTERSECTIONS)

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

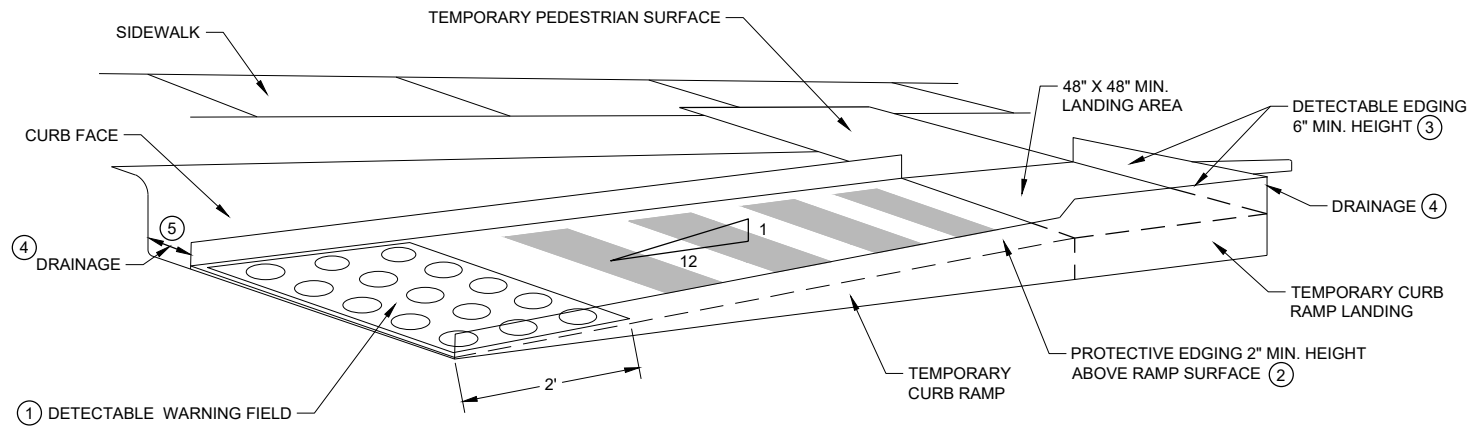




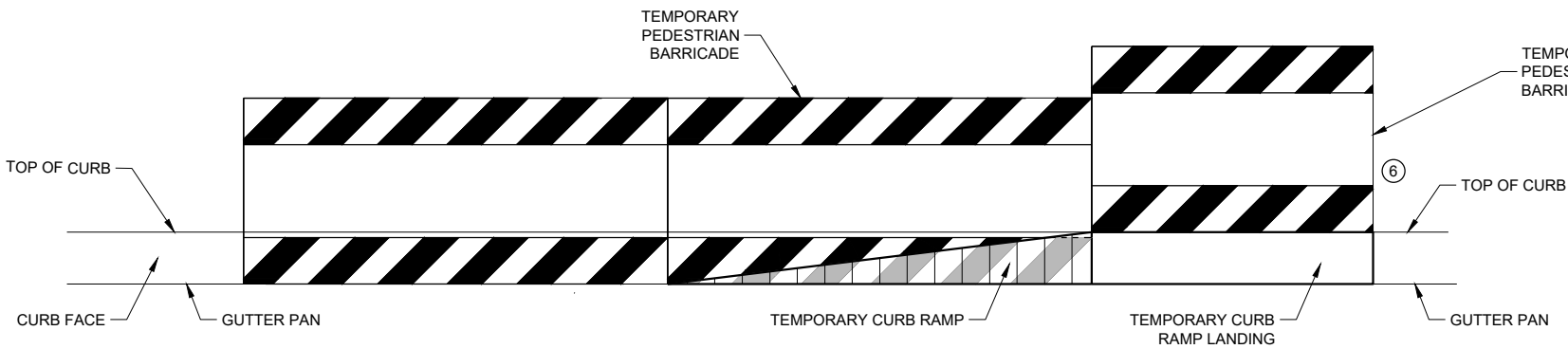
## BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST

- \* USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.

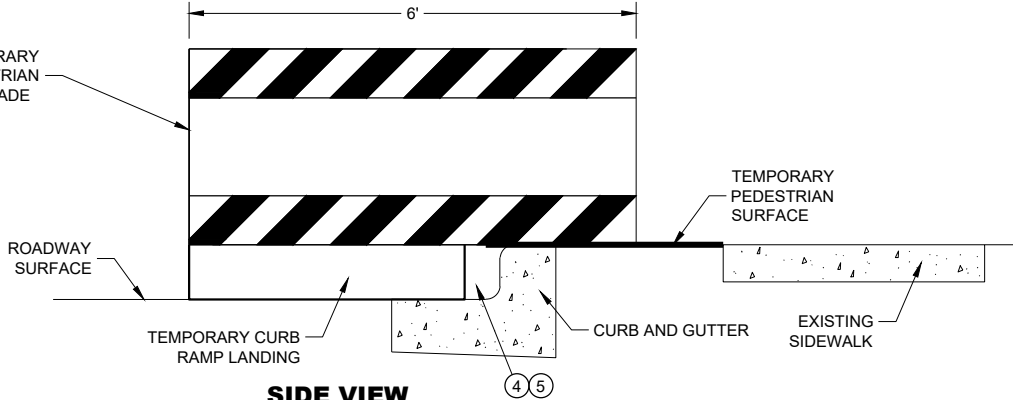




PERSPECTIVE VIEW



FRONT VIEW



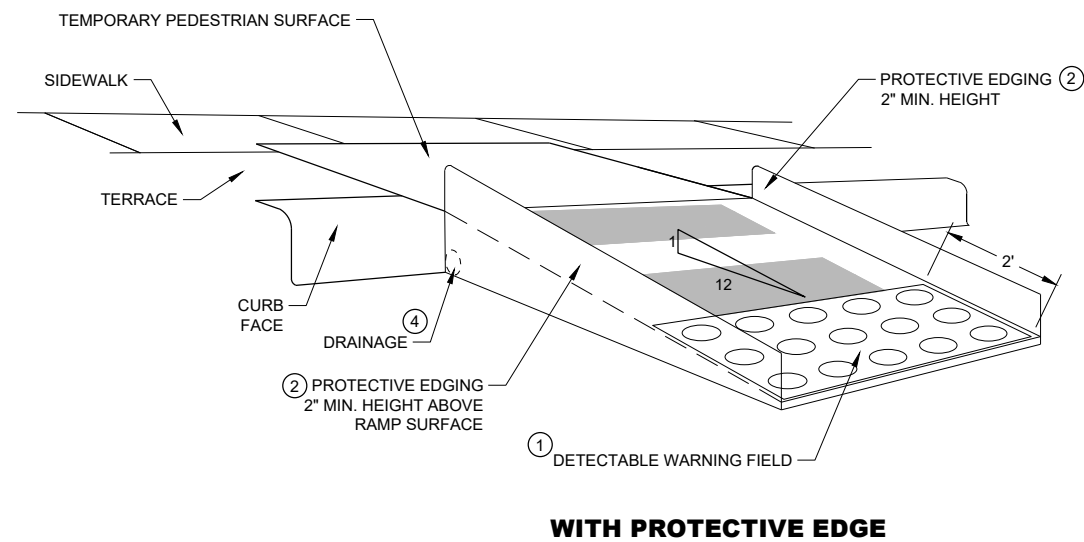
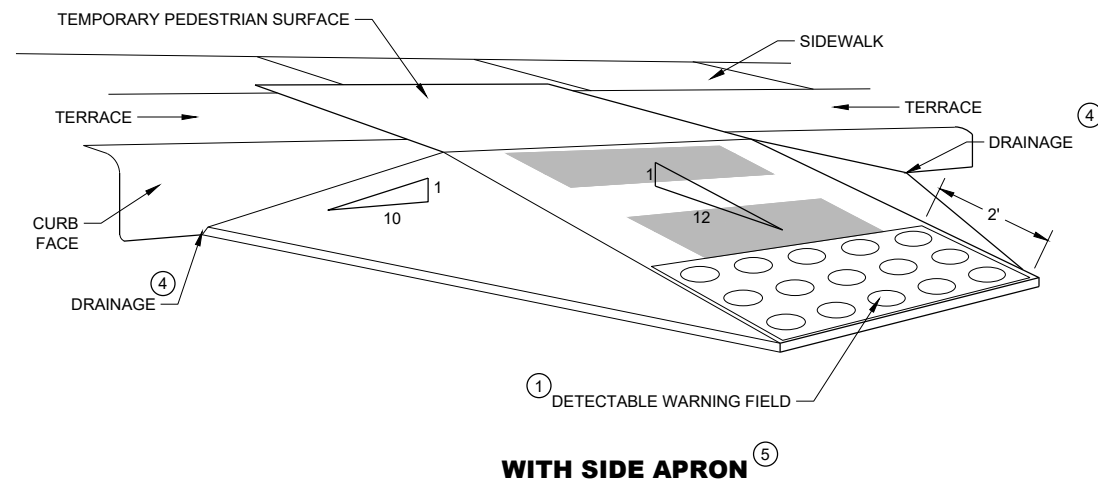
SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

GENERAL NOTES

- CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.
- CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
- CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP. LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
- CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ ENSURE CURB RAMP IS OUT OF THE GUTTER PAN.
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



**TEMPORARY CURB RAMP PERPENDICULAR TO CURB**

## GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

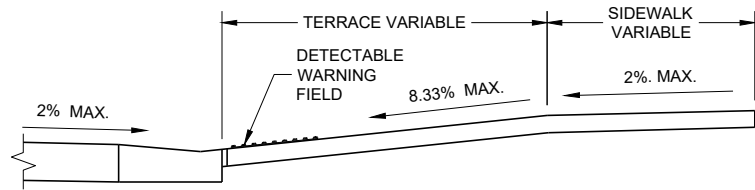
LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN ½" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED ½". LATERAL EDGES MAY BE VERTICAL UP TO ¼" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN ¼" AND ½".

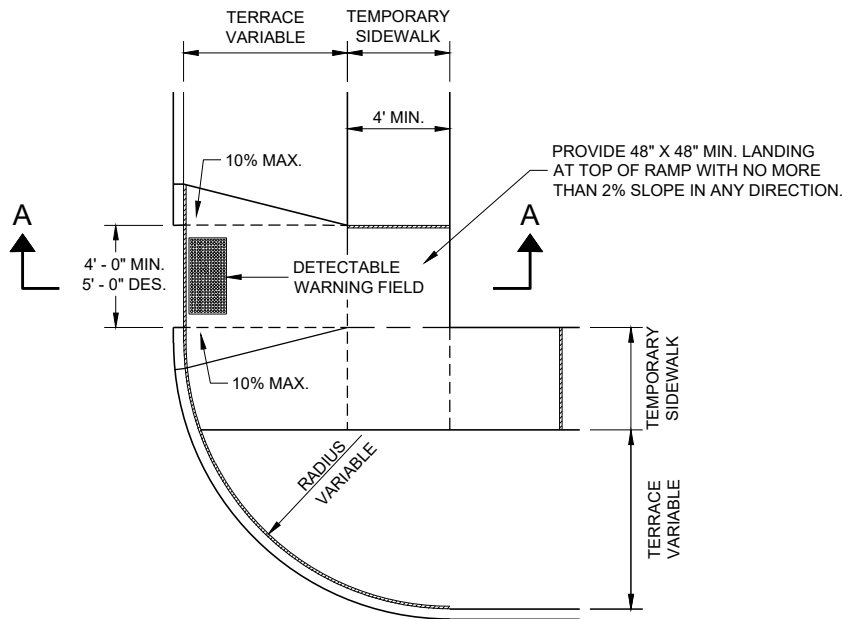
- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

**TRAFFIC CONTROL,  
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



SECTION A - A



PLAN VIEW  
TEMPORARY TYPE 3 RAMP  
(OUTSIDE OF CROSSWALK AREA)

GENERAL NOTES

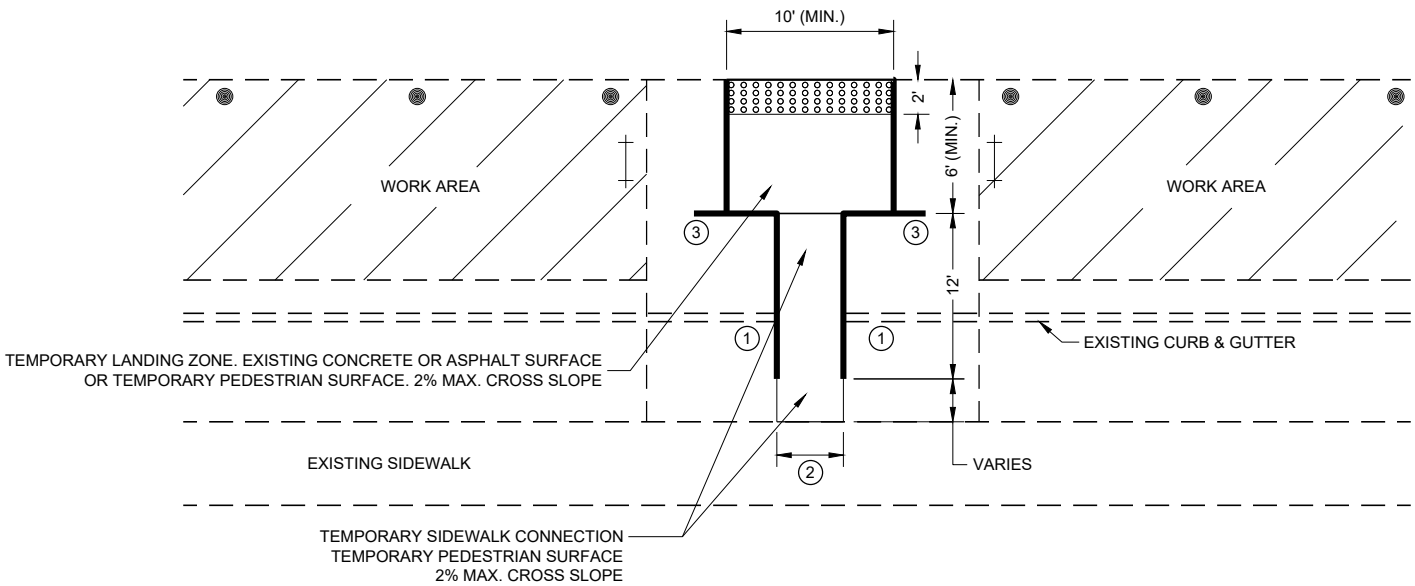
- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
  - ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
  - ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- ★ USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.

TRAFFIC CONTROL,  
PEDESTRIAN ACCOMMODATION

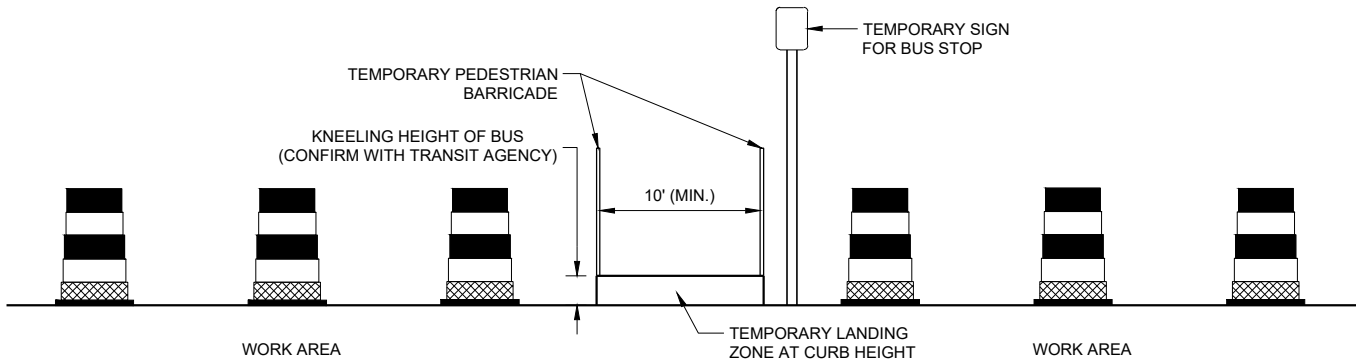
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
May 2023 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

FHWA



PLAN VIEW



PROFILE VIEW  
TEMPORARY BUS STOP PAD

GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.
- PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMP OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN ½" WIDTH.
- CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED ½". LATERAL EDGES MAY BE VERTICAL UP TO ¼" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN ¼" AND ½".
- CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.







- ① DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ② 5' WIDE MIN. WITH TEMPORARY PEDESTRIAN BARRICADE, 10' WIDE MIN. WITHOUT TEMPORARY PEDESTRIAN BARRICADE.
- ③ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE INTO THIS SPACE.

LEGEND

- TRAFFIC CONTROL DRUM
- ⊥ TYPE III BARRICADE
- TEMPORARY PEDESTRIAN BARRICADE
- ⊞ TEMPORARY DETECTABLE WARNING FIELD
- ▨ WORK AREA



LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

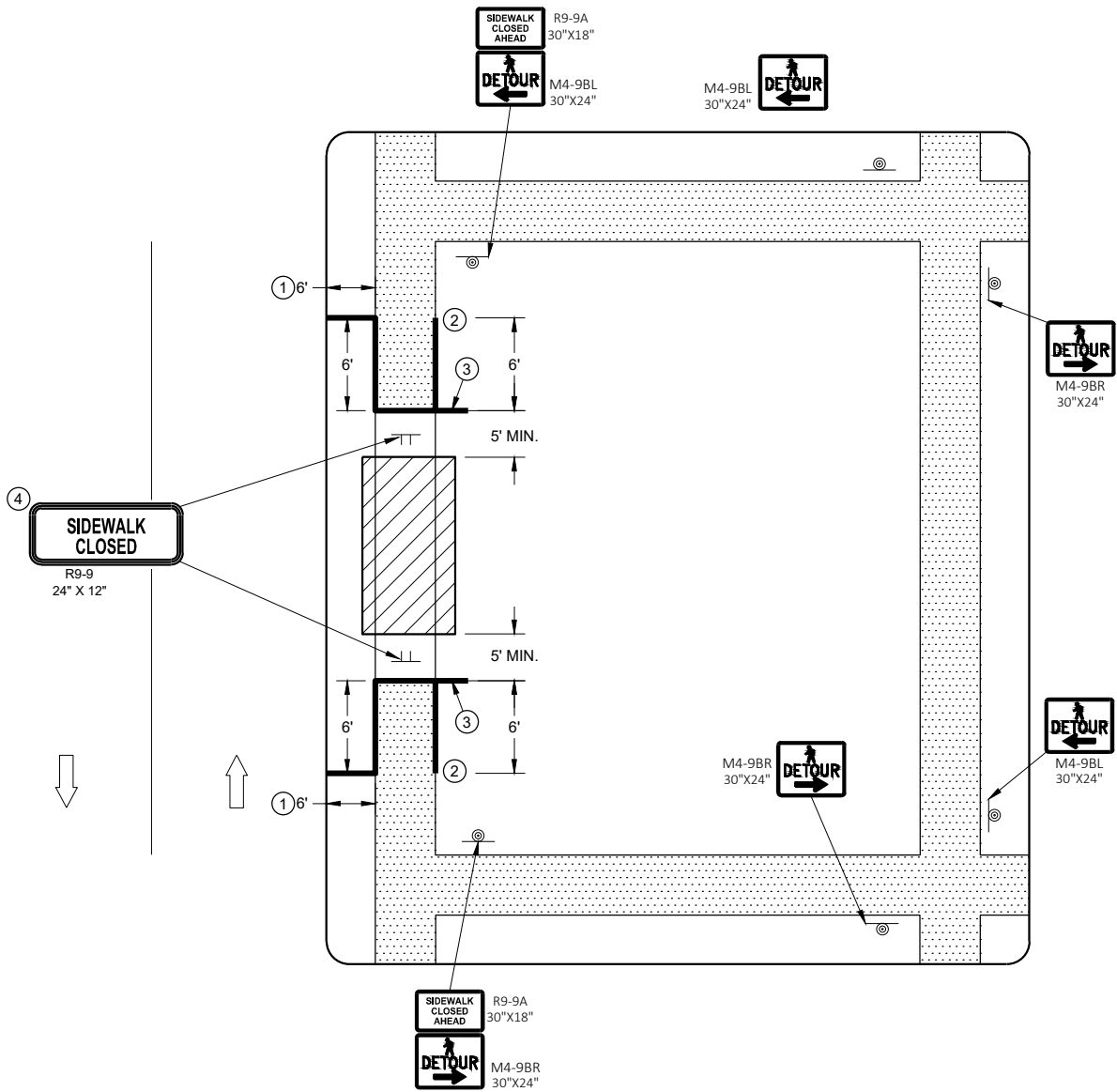
SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.








PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICTS WITH DRIVEWAYS AND OTHER EXISTING FEATURES.

- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

LEGEND

- SIGN ON TEMPORARY SUPPORT
- WORK AREA
- UNDER PEDESTRIAN TRAFFIC
- TEMPORARY PEDESTRIAN SURFACE
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

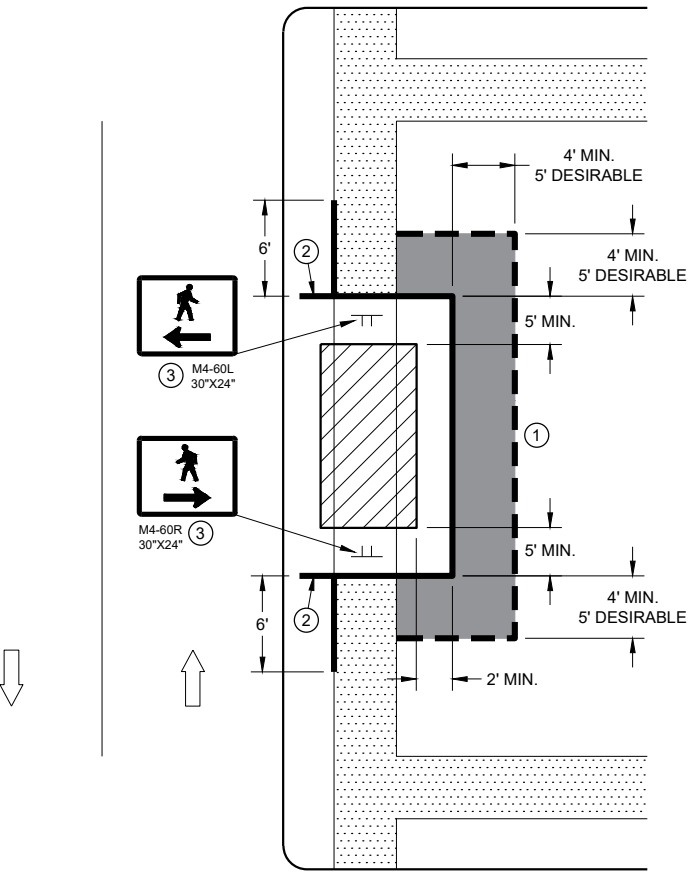
GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ①

USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ②










IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ③

MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DIVERSION  
SINGLE SIDE

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- UNDER PEDESTRIAN TRAFFIC
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- ①

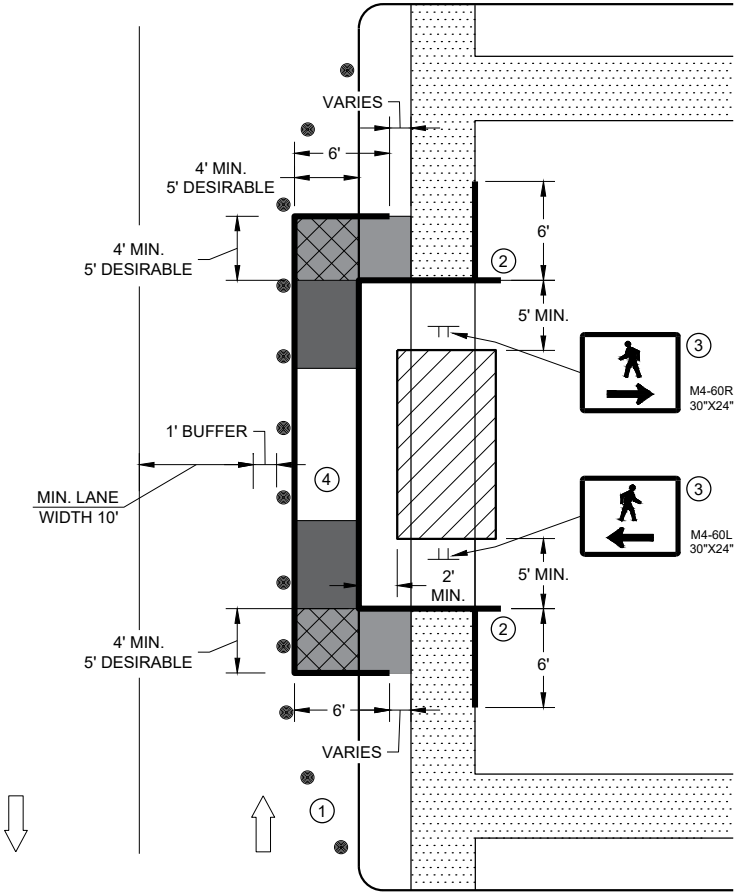
SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
- ②

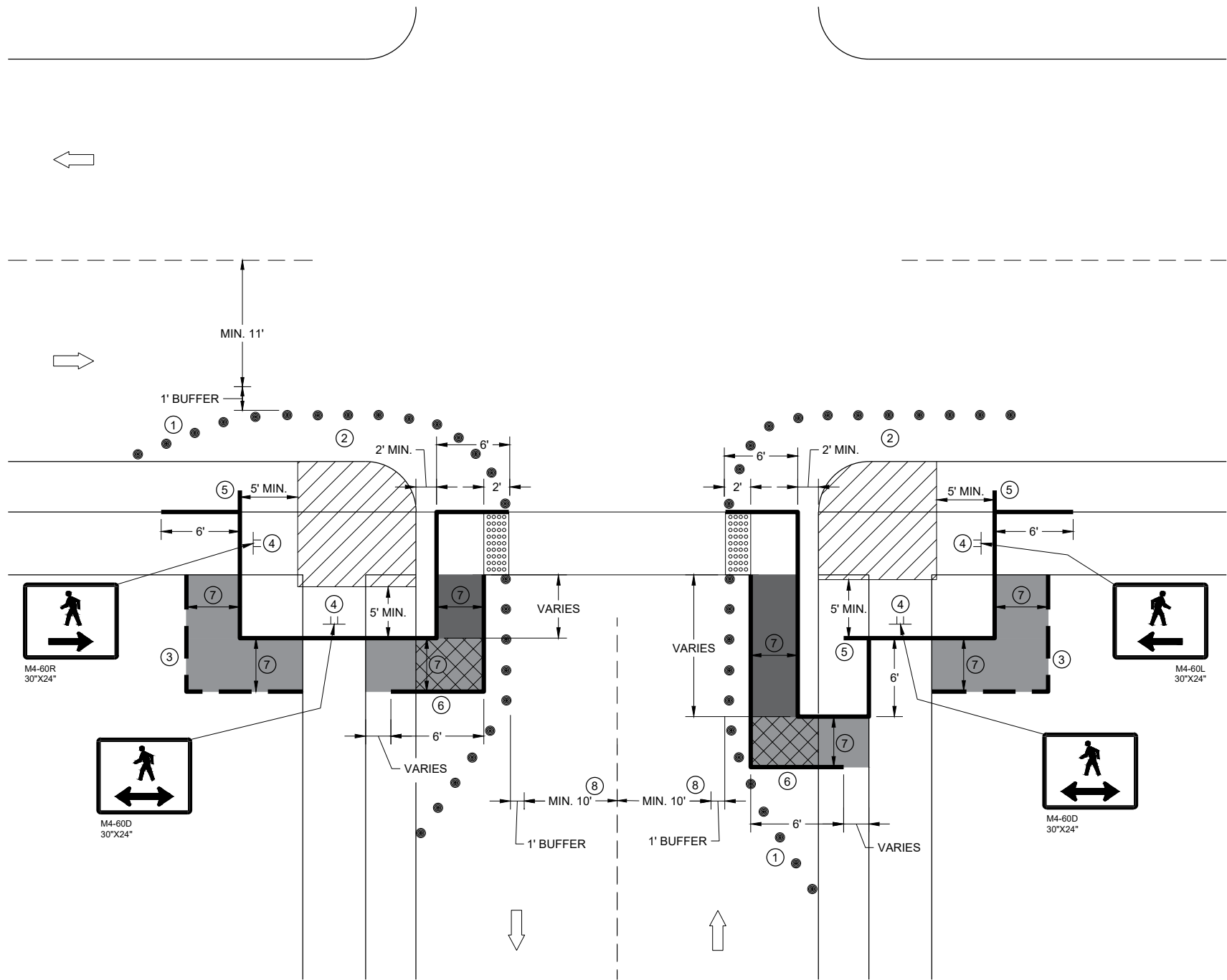
PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
- ③

MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
- ④

USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE.

SIDEWALK DIVERSION, SINGLE SIDE





CURB RAMP PEDESTRIAN TRAFFIC CONTROL  
SIDEWALK ON SINGLE SIDE

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

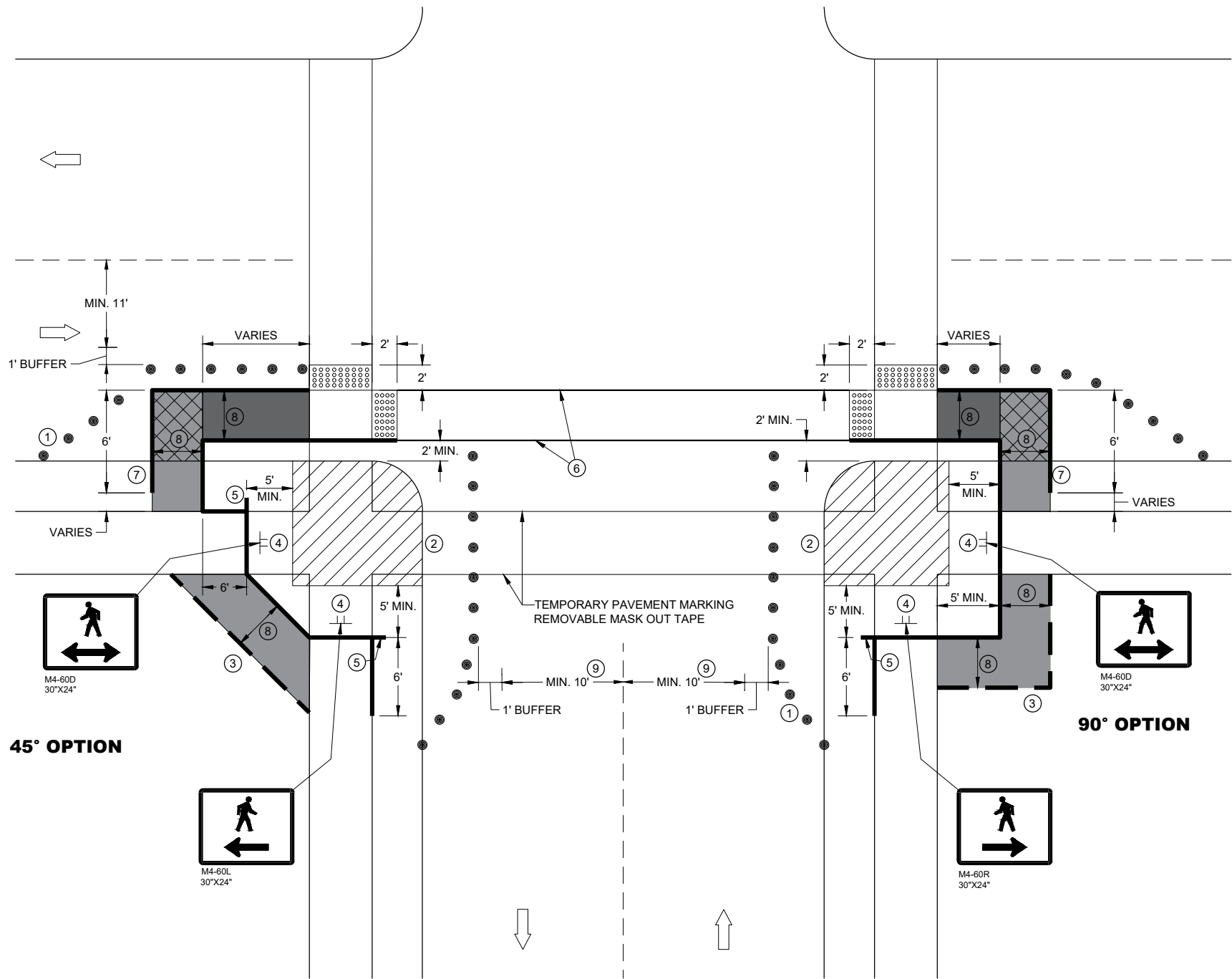
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑦ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑧ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

TRAFFIC CONTROL,  
PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



**GENERAL NOTES**

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.





- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ WHITE 6" TEMPORARY PAVEMENT MARKING
- ⑦ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑧ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑨ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

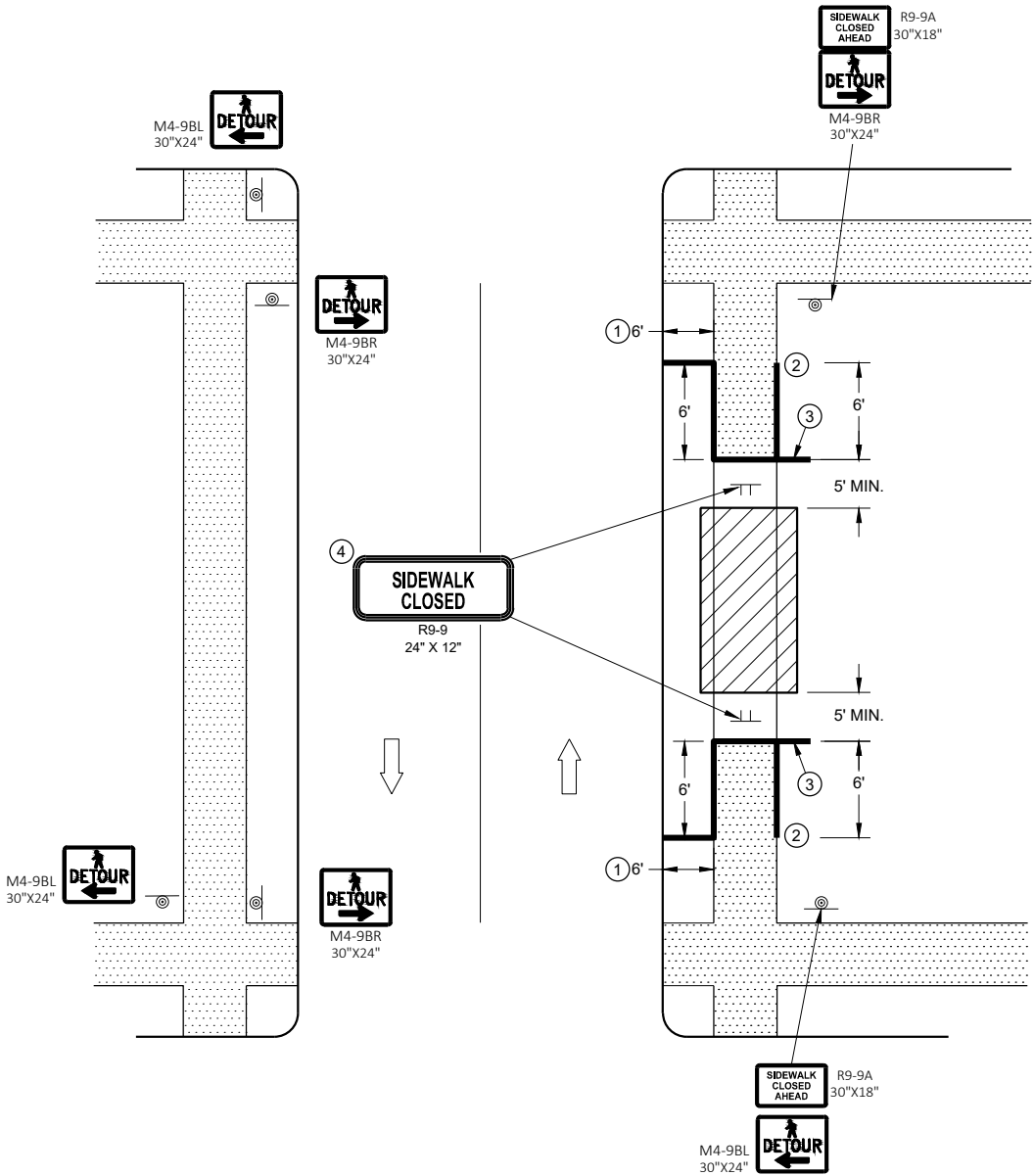
**LEGEND**

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC



LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



SIDEWALK DETOUR, SIDEWALK ON BOTH SIDES

GENERAL NOTES

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICT WITH DRIVEWAYS AND OTHER EXISTING FEATURES.

- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.

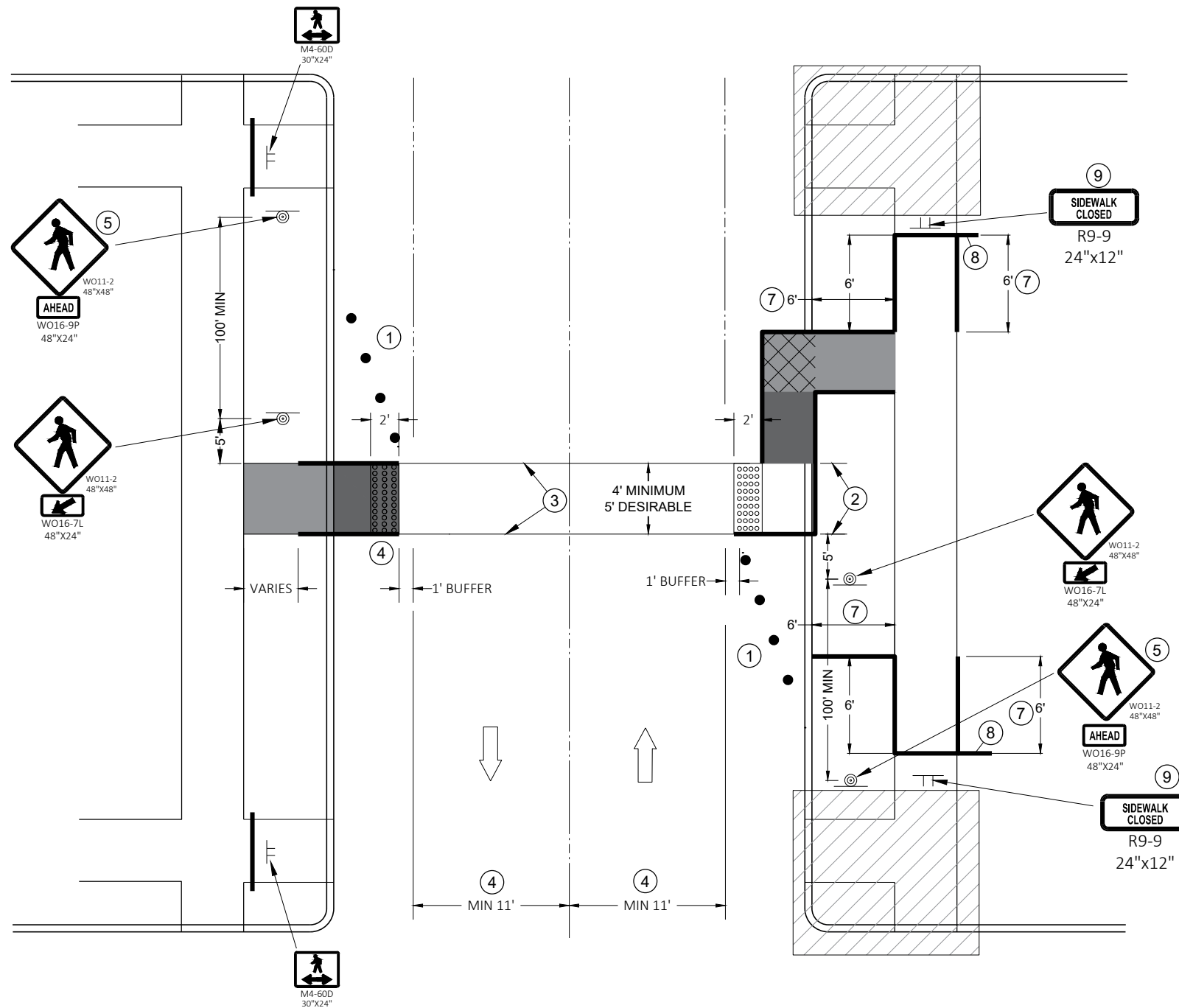
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

SEE OTHER PEDESTRIAN ACCOMMODATION DETAILS FOR SIGNING AND DEVICES FOR DIFFERENT PEDESTRIAN FACILITIES CLOSURES.

- ## LEGEND

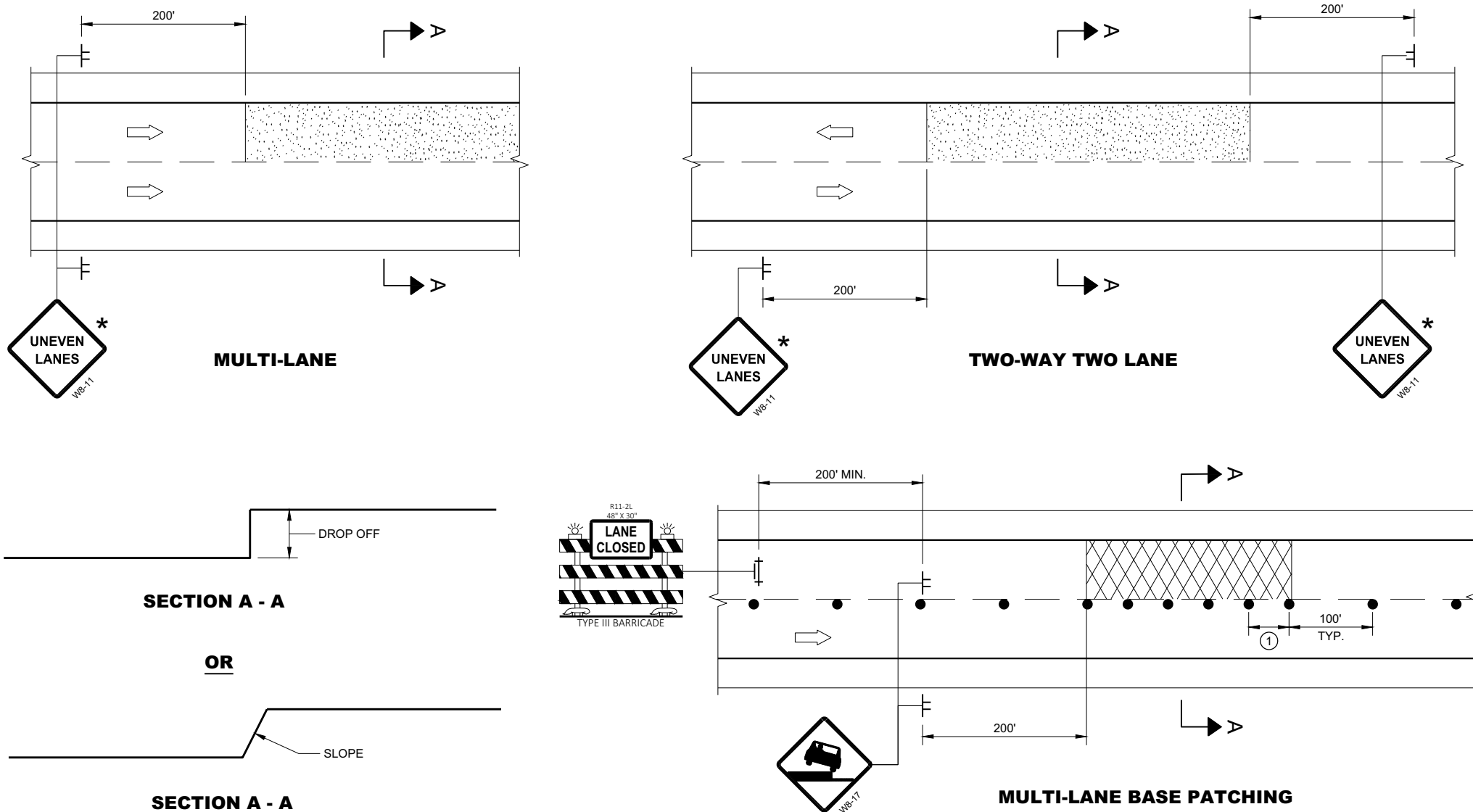
- |   |                                    |
|---|------------------------------------|
|  | TRAFFIC CONTROL DRUM               |
|  | SIGN ON TEMPORARY SUPPORT          |
|  | TEMPORARY CURB RAMP                |
|  | TEMPORARY DETECTABLE WARNING FIELD |
|  | TEMPORARY PEDESTRIAN SURFACE "A"   |
|  | TEMPORARY PEDESTRIAN SURFACE "B"   |
|  | WORK AREA                          |
|  | TEMPORARY PEDESTRIAN BARRICADE     |
|  | DIRECTION OF TRAFFIC               |



## TEMPORARY PEDESTRIAN CROSSING

**TRAFFIC CONTROL,  
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



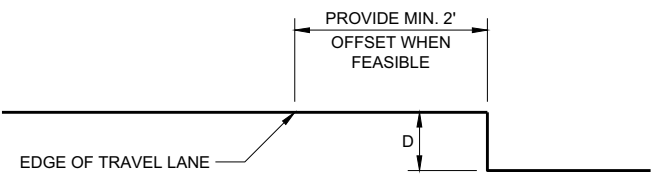
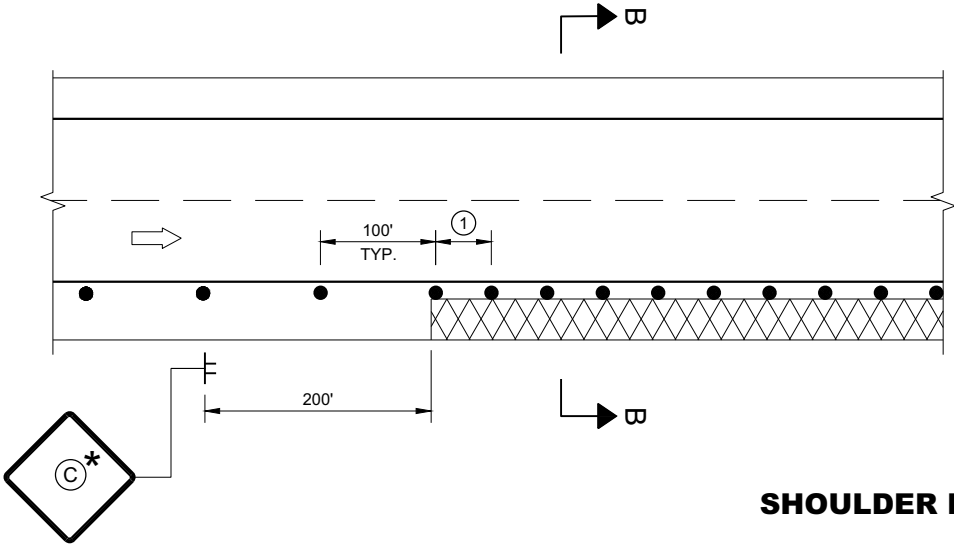
ADJACENT LANE DROP-OFFS

GENERAL NOTES

- FOR SPOT LOCATIONS USE ENGINEERING JUDGEMENT WHEN PLACING ADDITIONAL SIGNS.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.
- "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.
- \* IF THE DROP-OFF IS CONTINUOUS ALONG THE PROJECT, PLACE ADDITIONAL SIGNS EVERY 1 MILE AND AFTER EVERY ENTRANCE RAMP.
- ① USE CLOSER SPACING WHEN DELINEATING DROP-OFF.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- WORK AREA WITH DROP-OFF
- MILLED SURFACE



SECTION B - B

SHOULDER DROP-OFFS

D	SIGN (C)
< 2" WITH A SLOPE STEEPER THAN 3:1	LOW SHOULDER WO8-9
2" < 6" WITH A SLOPE STEEPER THAN 3:1	SHOULDER DROP - OFF WB-9A PROVIDE A 3:1 OR FLATTER SLOPE OF MATERIAL ADJACENT TO THE PAVEMENT

TRAFFIC CONTROL, DROP-OFF SIGNING

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
March 2018 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER  
FHWA

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.



THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

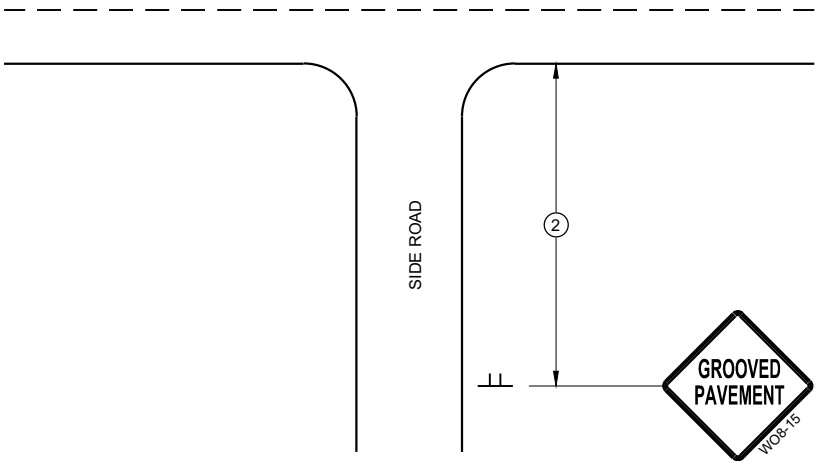
ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

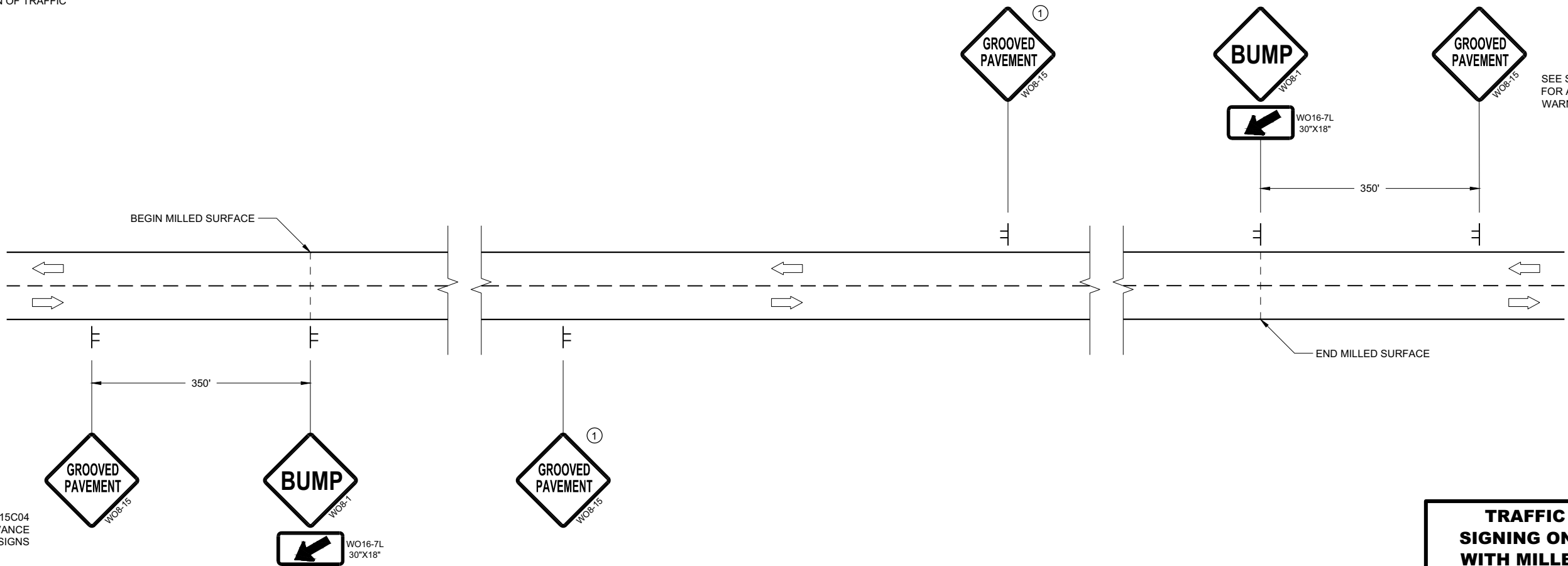
- 1 PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- 2 PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH  
SIGN DETAIL



SEE SDD15C04  
FOR ADVANCE  
WARNING SIGNS

SEE SDD15C04  
FOR ADVANCE  
WARNING SIGNS

DETAIL FOR SIGNING ON MILLED SURFACES

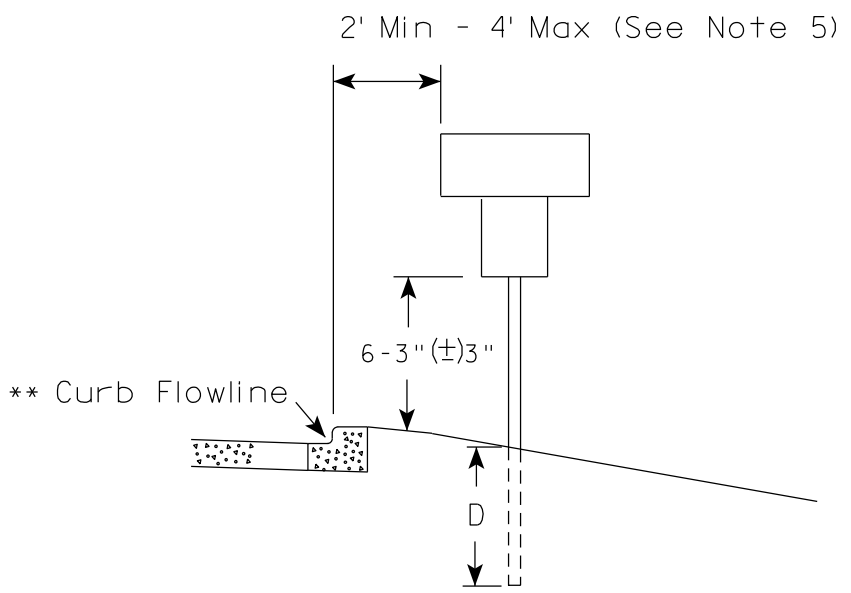
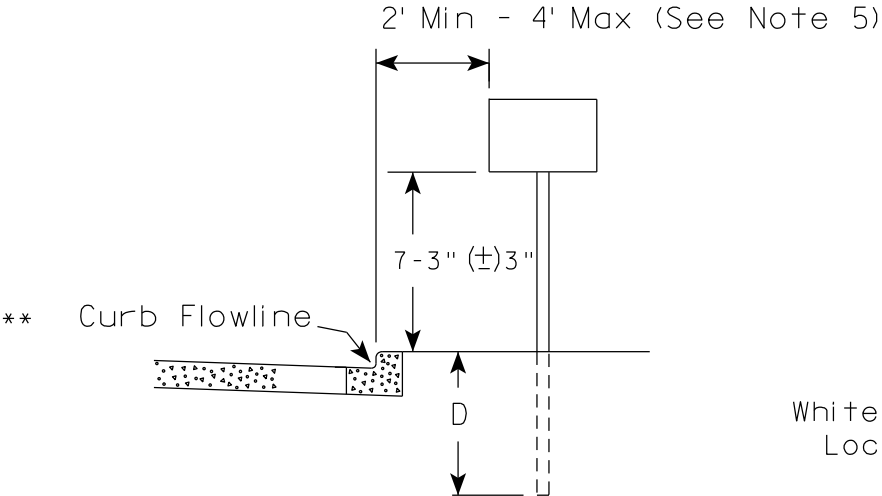
TRAFFIC CONTROL,  
SIGNING ON ROADWAYS  
WITH MILLED SURFACES

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

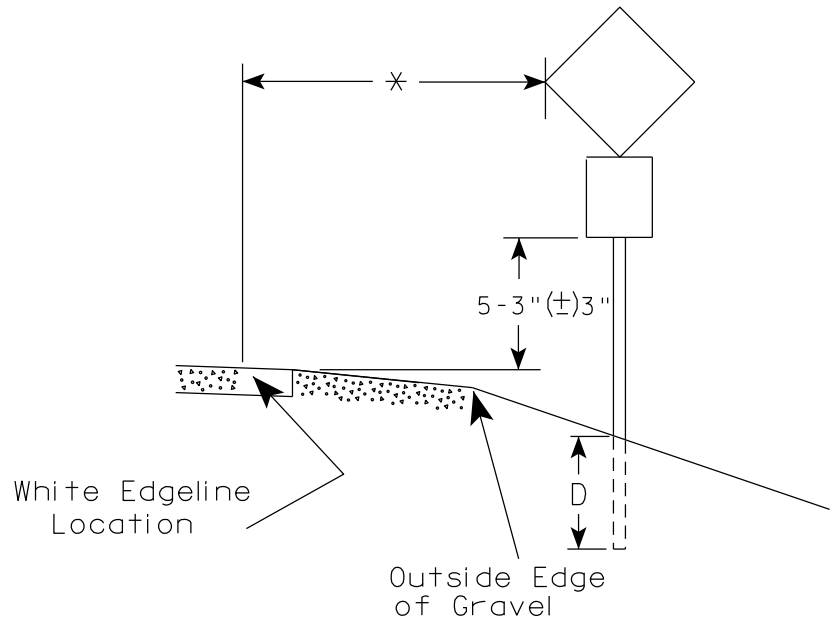
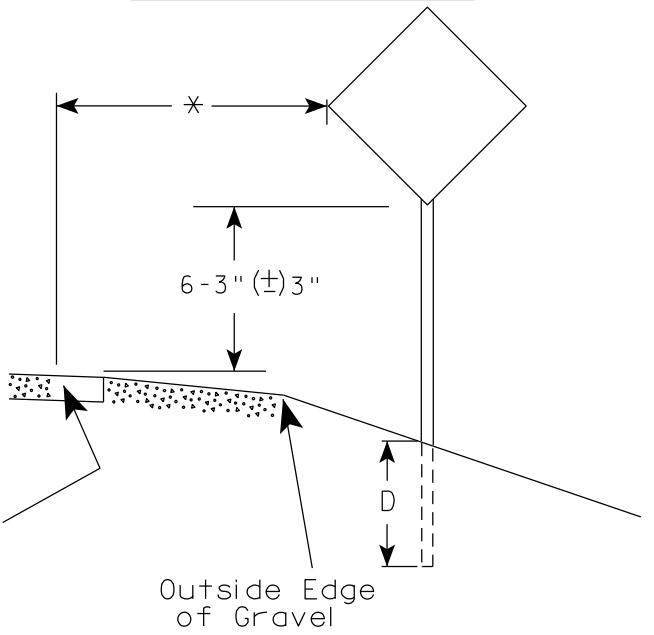
APPROVED  
February 2020 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

FHWA

URBAN AREA



RURAL AREA (See Note 2)



POST EMBEDMENT DEPTH

Area of Sign Installation ( Sq. Ft. )	D ( Min )
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.  
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".
3. For expressways and freeways, mounting height is 7'- 3" (±) 3" or 6'-3" (±) 3" depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±) 3".
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the Engineer.

\* \* The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

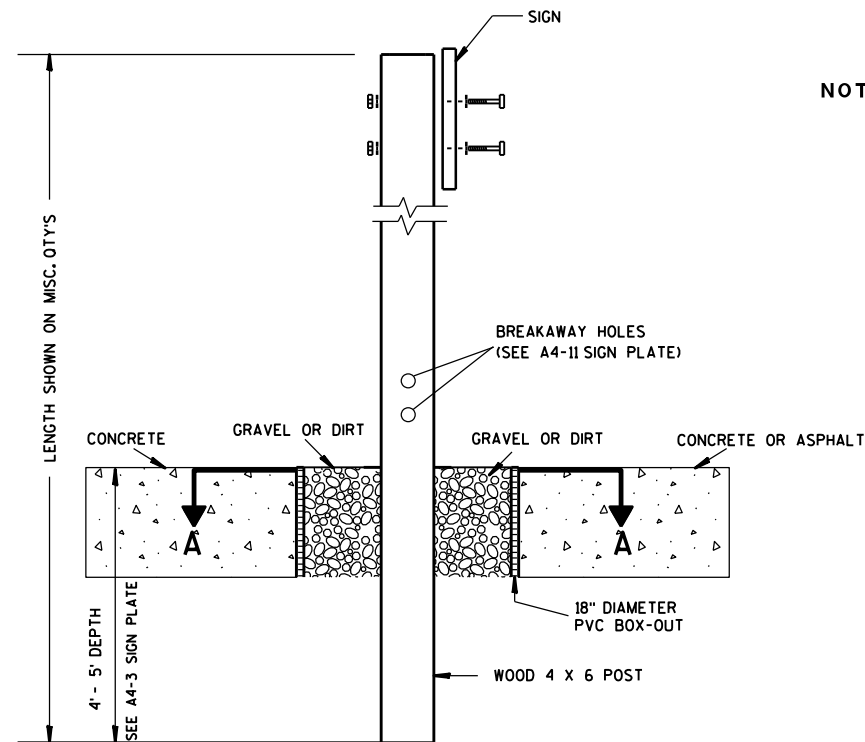
\* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION  
OF PERMANENT TYPE II  
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

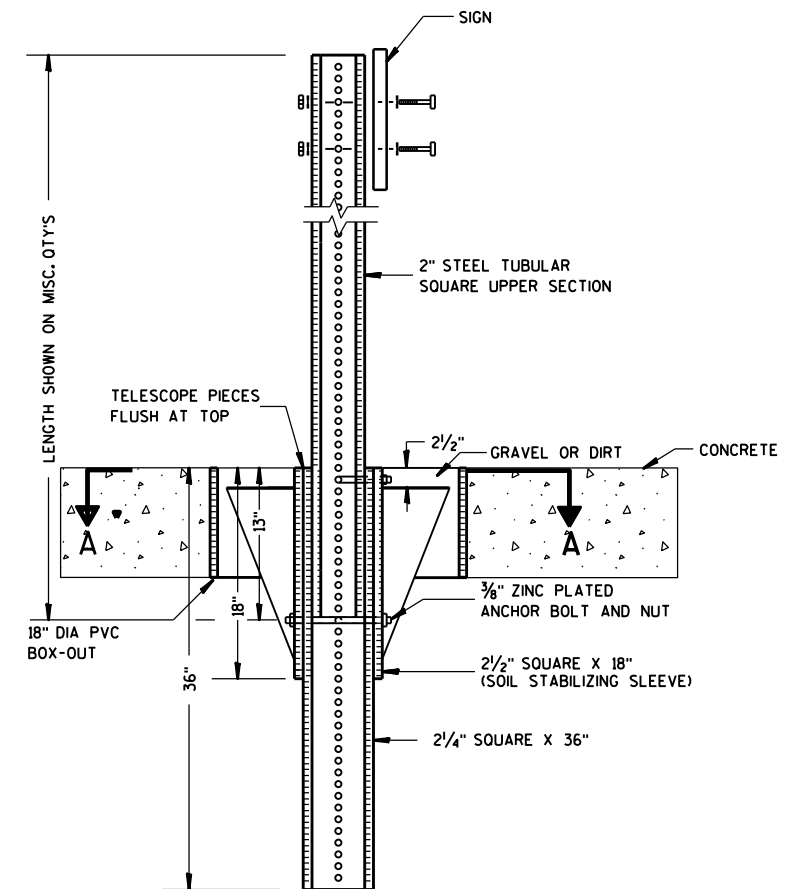
DATE 12/6/23 PLATE NO. A4-3.23



### ELEVATION VIEW

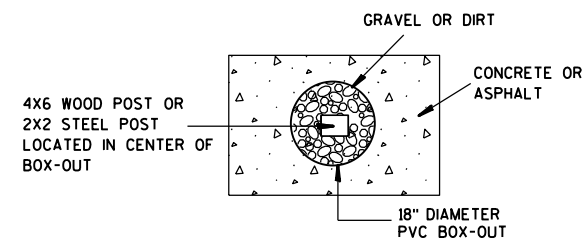
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES: 1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



### ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



### PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST  
BOX-OUTS  
A4-3B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

PROJECT NO:

HWY:

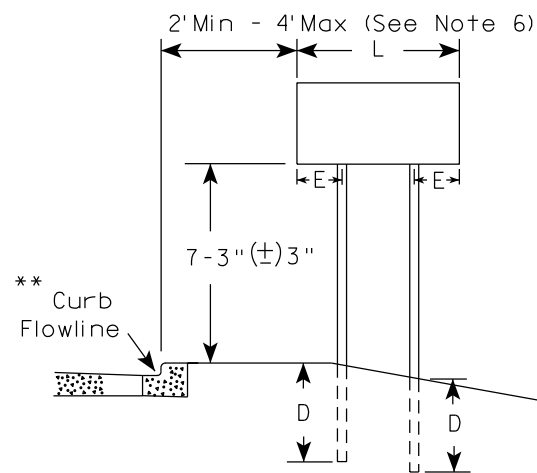
COUNTY:

SHEET NO:

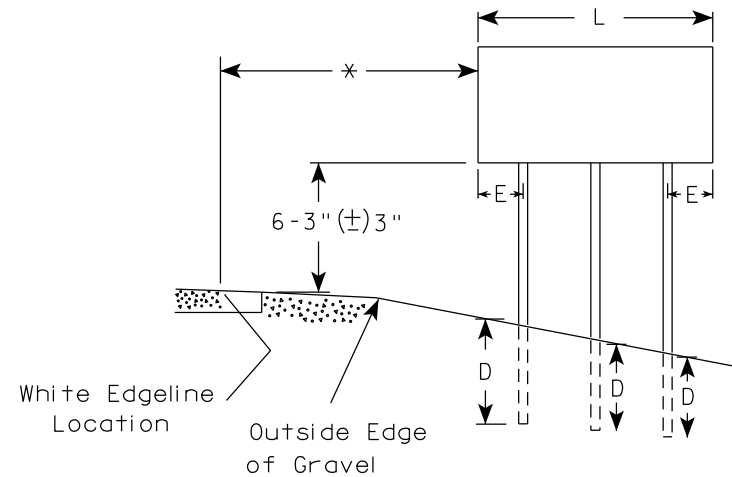
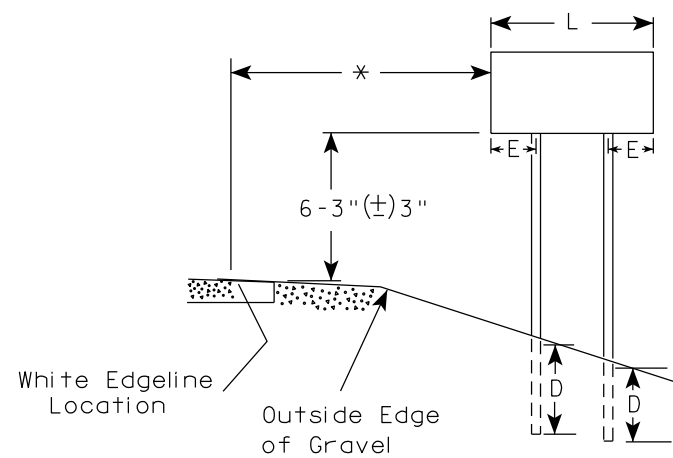
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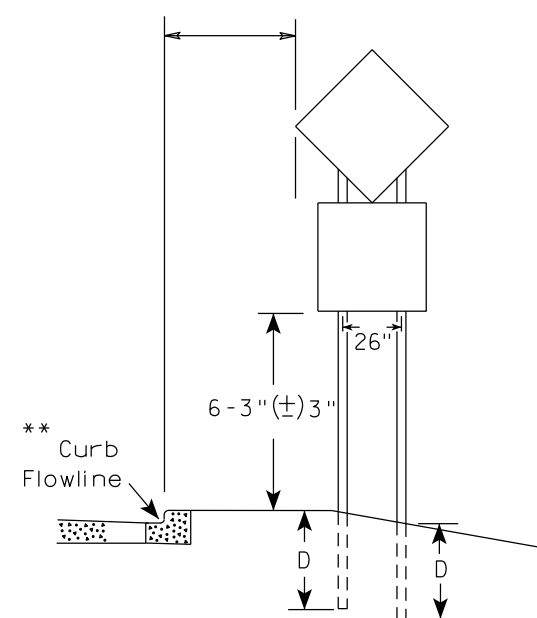
URBAN AREA



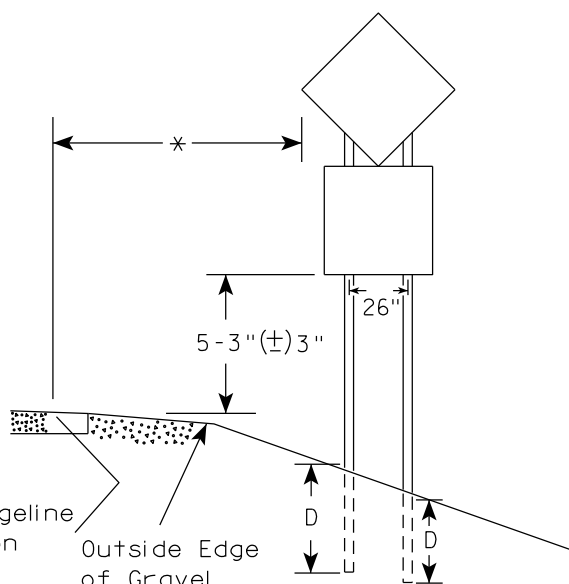
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

\*\*\*

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation ( Sq.Ft. )	D ( Min )
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION  
OF TYPE II SIGNS  
ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION  
APPROVED *Matthew R. Rauch*  
for State Traffic Engineer  
DATE 12/6/23 PLATE NO. A4-4.16

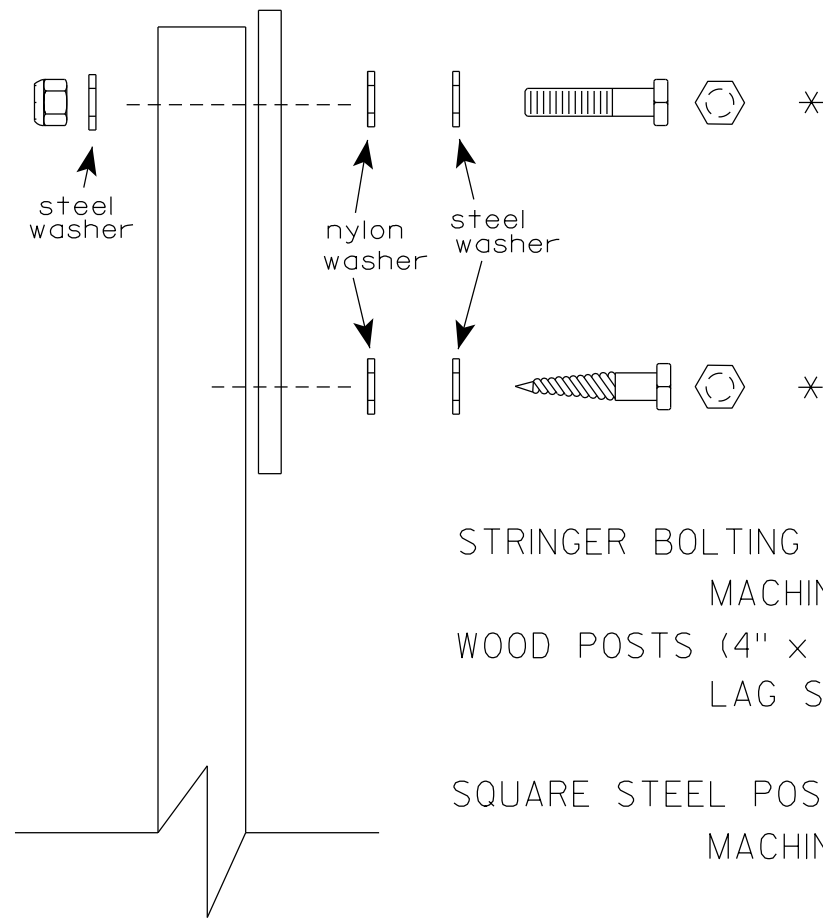
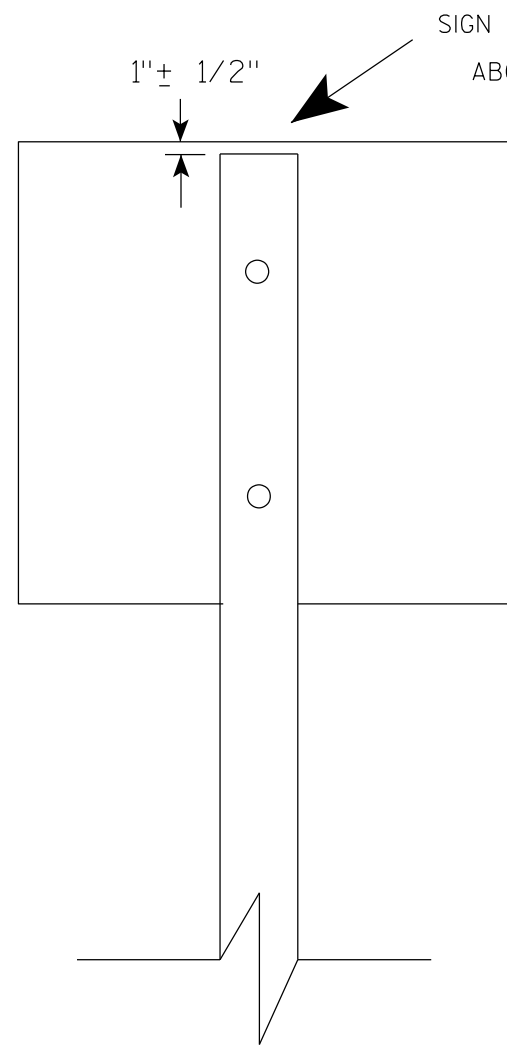
GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) 3" or 6'-3" (±) 3" depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".

\* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

\*\* The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

\*\*\* See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS -  $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS -  $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)  
 $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS -  $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)  
 $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS -  $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL  
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X  $\frac{3}{8}$ " I.D. X  $\frac{1}{16}$ " STEEL
  - 1-1/4" O.D. X  $\frac{3}{8}$ " I.D. X .080 NYLON

\* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 4/1/2020	PLATE NO. A4-8.9

**2 1/4 " SQUARE  
12 GAUGE  
PERFORATED  
GALVANIZED FINISH**

4" x 10" x 10 GA. ———→  
STEEL PLATE (CUT  
AS SHOWN) WELDED  
TO ALL FOUR CORNERS  
OF TELESPAR TUBE

**2 1/2" SQUARE  
12 GAUGE  
OMNI-DIRECTIONAL  
PERFORATED  
SOIL STABILIZING SLEEVE  
GALVANIZED FINISH**

2 1/2" TELES PAR TUBE

4"

2 1/2"

10"

3 1/2"

16"

4" x 10" x 10 GA. STEEL PLATE (CUT AS SHOWN) WELDED TO ALL FOUR CORNERS OF TELES PAR TUBE

TELESCOPE PIECES  
FLUSH AT TOP

18" DIA SCHEDULE  
40 PVC  
BOX-OUT

36"

13"

18"

2 1/2" GRAVEL OR DIRT

3/8" ZINC PLATED CORNER  
ANCHOR BOLT AND NUT

2 1/2" SQUARE X 18"  
(SOIL STABILIZING SLEEVE)

2 1/4" SQUARE X 36"

2" STEEL TUBULAR  
SQUARE UPPER SECTION

ALL HOLES 7/16"  
SPACED 1" C-C  
ALL FOUR SIDES

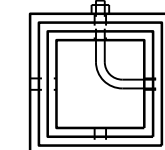
SEE SIGN PLATE  
A4-8 FOR BOLT  
WASHER, & NUT  
MATERIAL

SIGN

TECHNICAL DRAWING OF A SIGNPOST ASSEMBLY:

- Dimensions:**
  - Overall height: LENGTH SHOWN ON MISC. Q'TYS
  - Section A-A: 36" (total), 18" (upper), 12" (lower)
  - Section B-B: 1"
- Components:**
  - SIGN
  - SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL
  - 2" STEEL TUBULAR SQUARE UPPER SECTION
  - ALL HOLES  $\frac{7}{16}$ " SPACED 1" C-C
  - ALL FOUR SIDES
  - $\frac{3}{8}$ " ZINC PLATED CORNER ANCHOR BOLT AND NUT
  - $\frac{3}{8}$ " ZINC PLATED ANCHOR BOLT AND NUT
  - 2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)
  - 2 1/4" SQUARE X 36"
  - TELESCOPE PIECES FLUSH AT TOP

3/8" ZINC PLATED CORNER  
ANCHOR BOLT AND NUT



DIRECTION  
OF TRAFFIC

SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

**Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).**

TUBULAR STEEL  
SIGN POST  
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R. Rauch

for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

PROJECT NO:

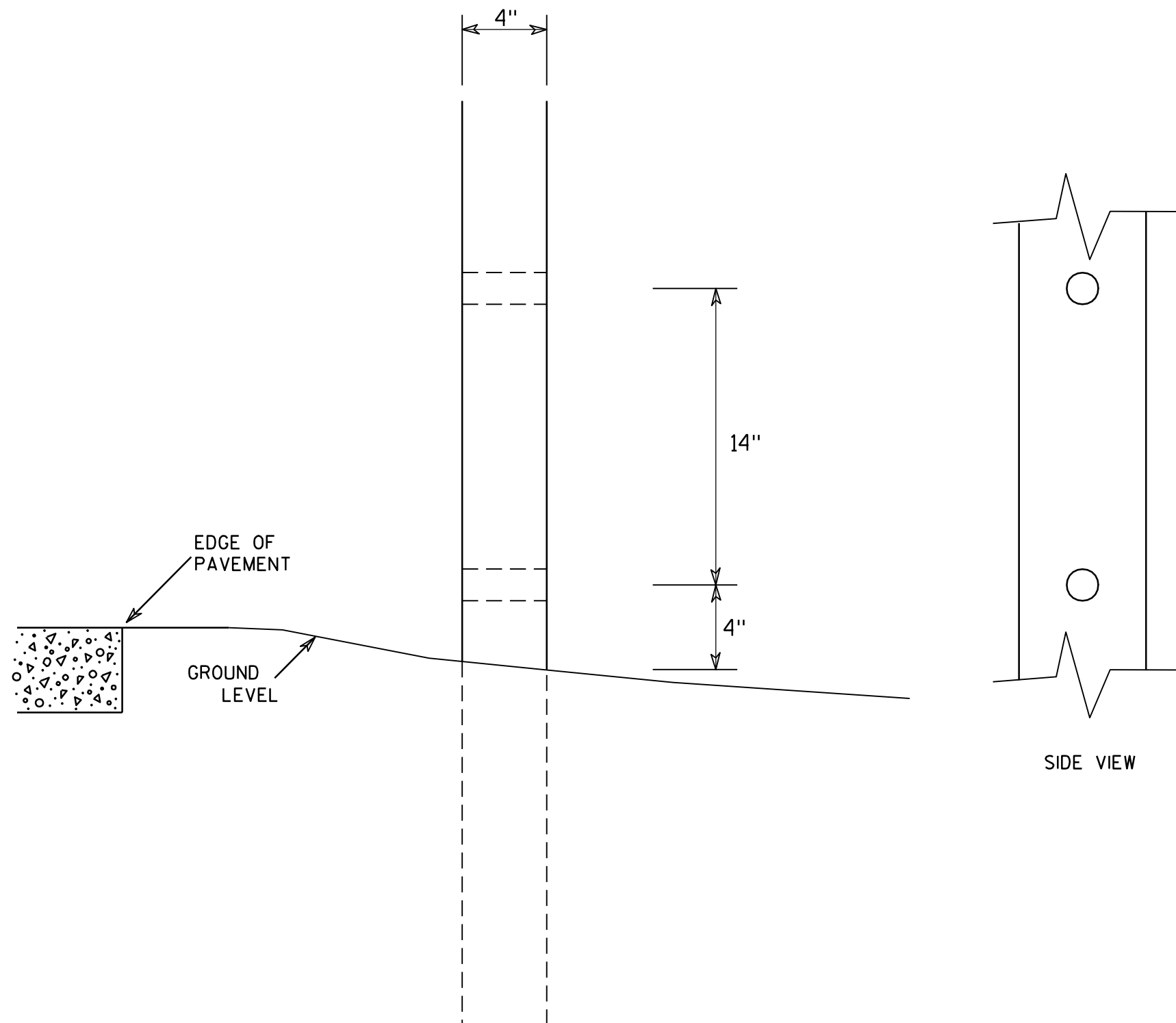
HWY:

COUNTY:

SHEET NO:

**T**

7



### GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

### 4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

*Chester J. Spang*  
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

PROJECT NO:

HWY:

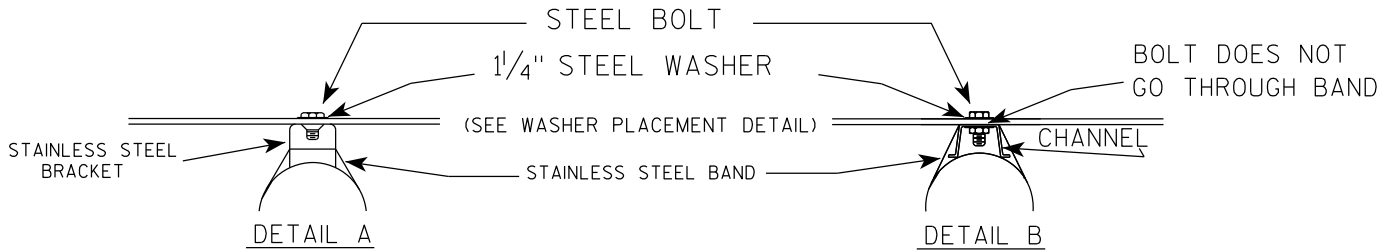
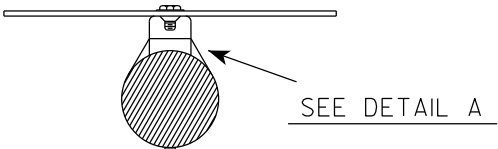
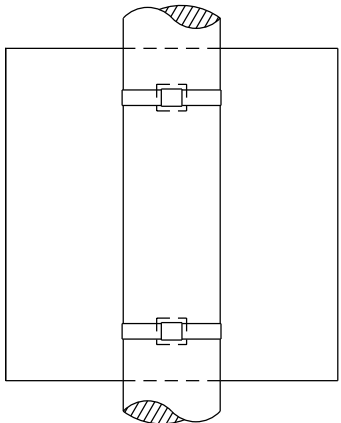
COUNTY:

SHEET NO:

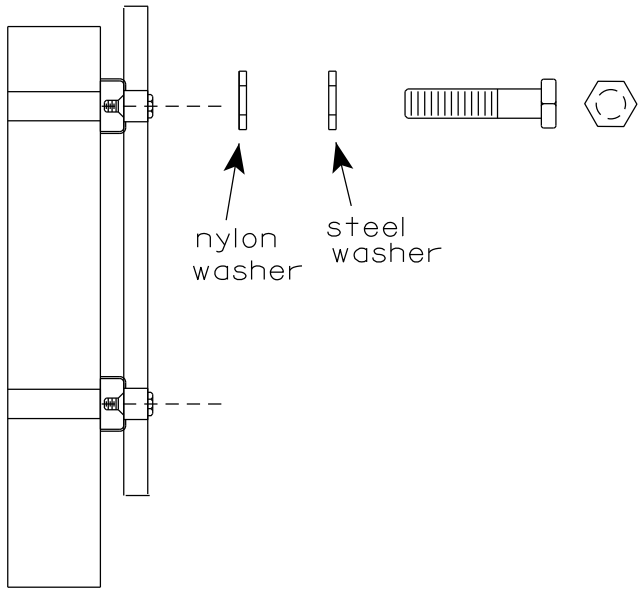
E

BANDING

SINGLE SIGN



WASHER PLACEMENT

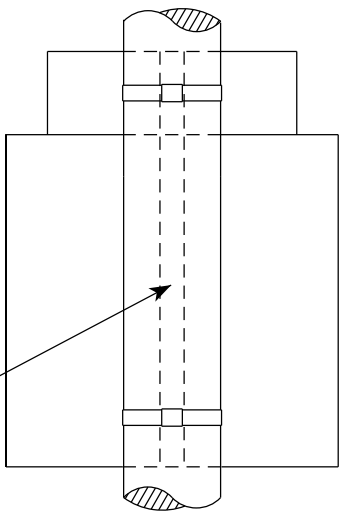


WASHERS (ALL POSTS) -  
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL  
1-1/4" O.D. X 3/8" I.D. X .080 NYLON  
FOR ALL TYPE H SIGNS

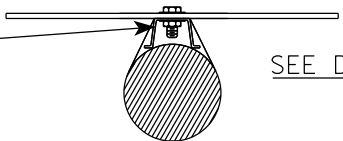
GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
  - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
  - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



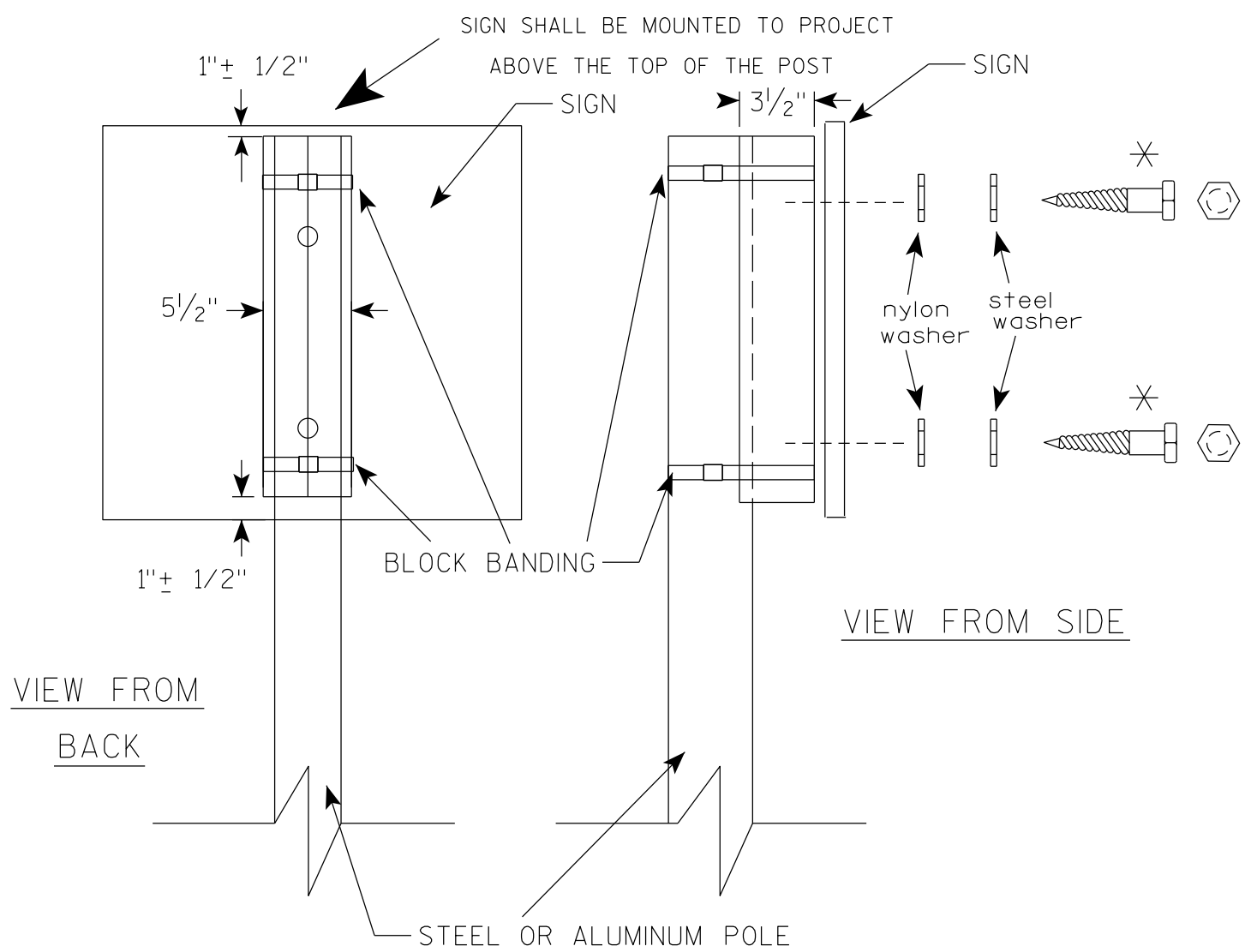
CHANNEL  
SEE TYPICAL PANEL  
INSTALLATION SHEET



STANDARD SIGN  
SIGN BANDING DETAILS

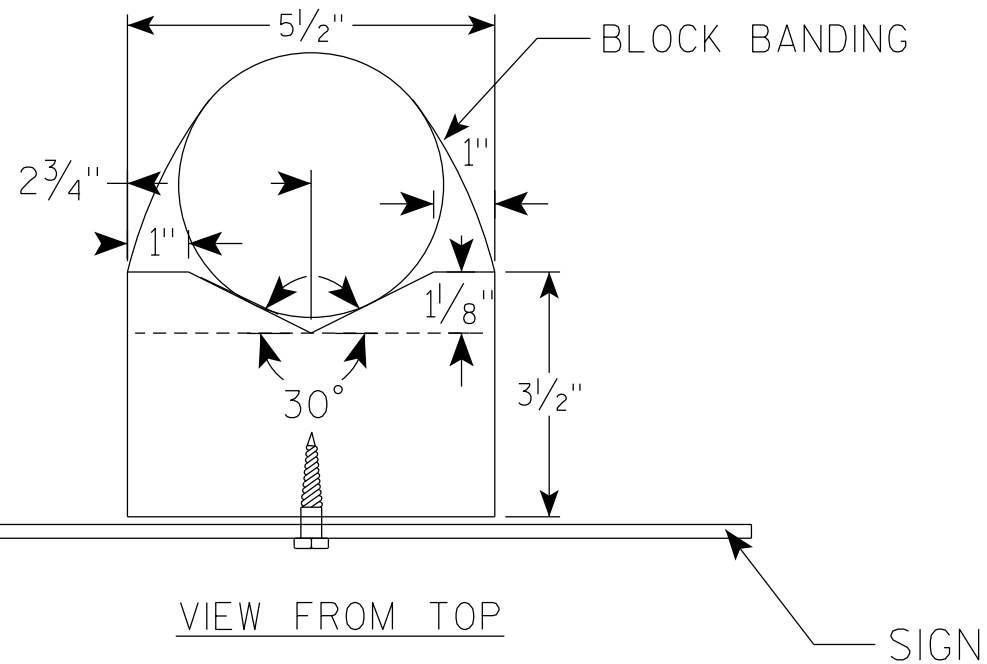
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer  
DATE 6/10/19 PLATE NO. A5-9.4



VIEW FROM  
BACK

VIEW FROM SIDE



VIEW FROM TOP

## GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL,  $\frac{3}{4}$ " WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
  - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
  - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE  $\frac{1}{4}$ " O.D. X  $\frac{3}{8}$ " I.D. X  $\frac{1}{16}$ "
8. NYLON WASHERS SHALL BE  $\frac{1}{4}$ " O.D. X  $\frac{3}{8}$ " I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✱ LAG BOLTS SHALL BE  $\frac{3}{8}$ " X  $2\frac{1}{2}$ "

BLOCK BANDING DETAIL  
( V-BLOCK OPTION )

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 4/19/2022 PLATE NO. A5-10.3

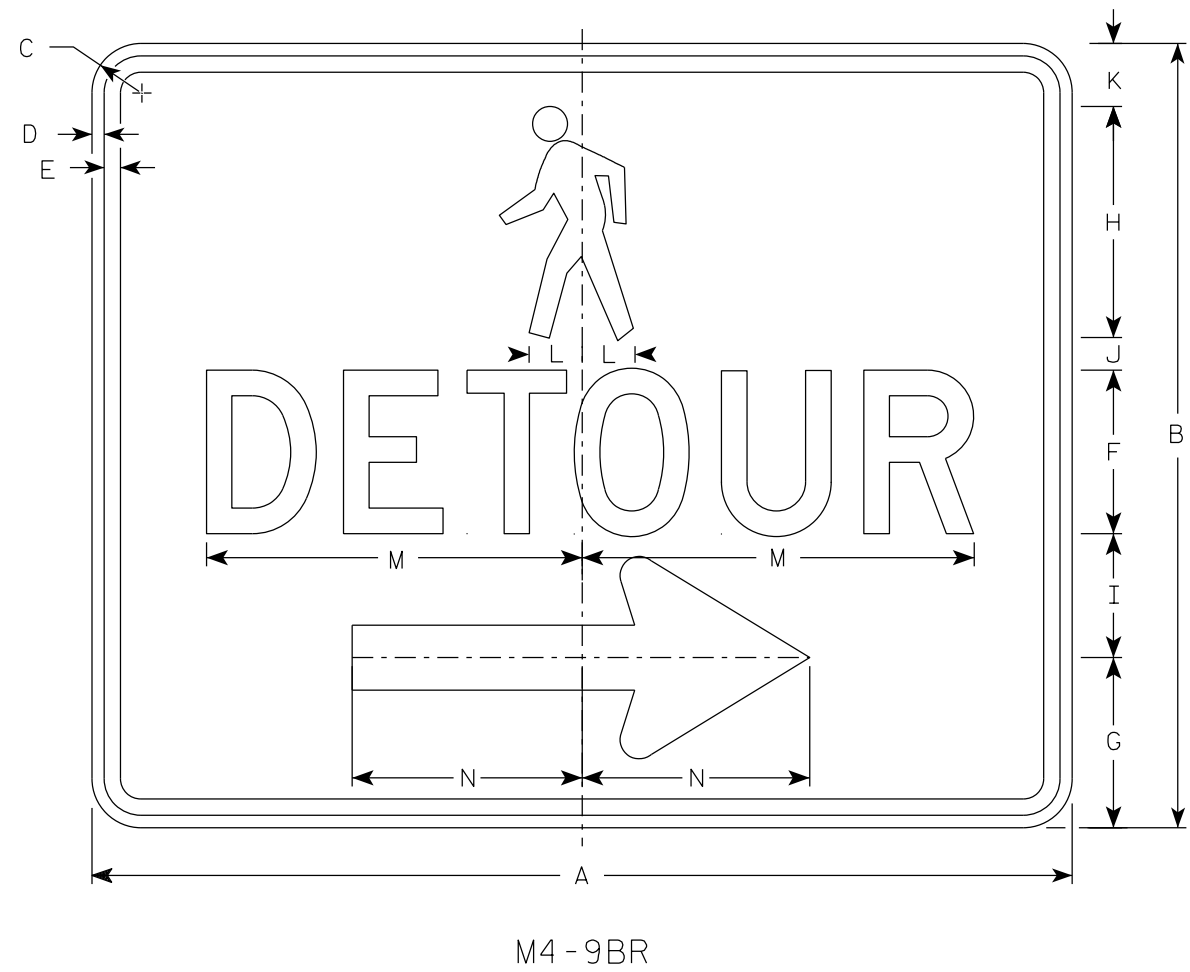
PROJECT NO:

SHEET NO:

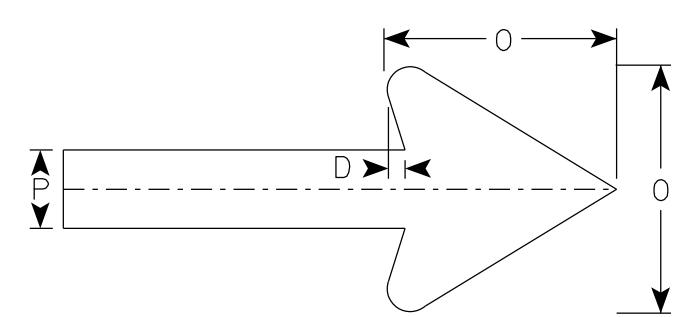
E



7



- NOTES
- 1. Sign is Type II-Type F Reflective
  - 2. Color:  
Background - Orange  
Message - Black
  - 3. Message Series - D
  - 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
  - 5. M4-9BL is the same as M4-9BR except the arrow is reversed.



Arrow Detail

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	24	1 1/2	3/8	1/2	5	5 1/4	7 1/8	3 3/4	1	1 1/8	1 5/8	11 3/4	7	6	2											5.0
2M	30	24	1 1/2	3/8	1/2	5	5 1/4	7 1/8	3 3/4	1	1 1/8	1 5/8	11 3/4	7	6	2											5.0
3																											
4																											
5																											

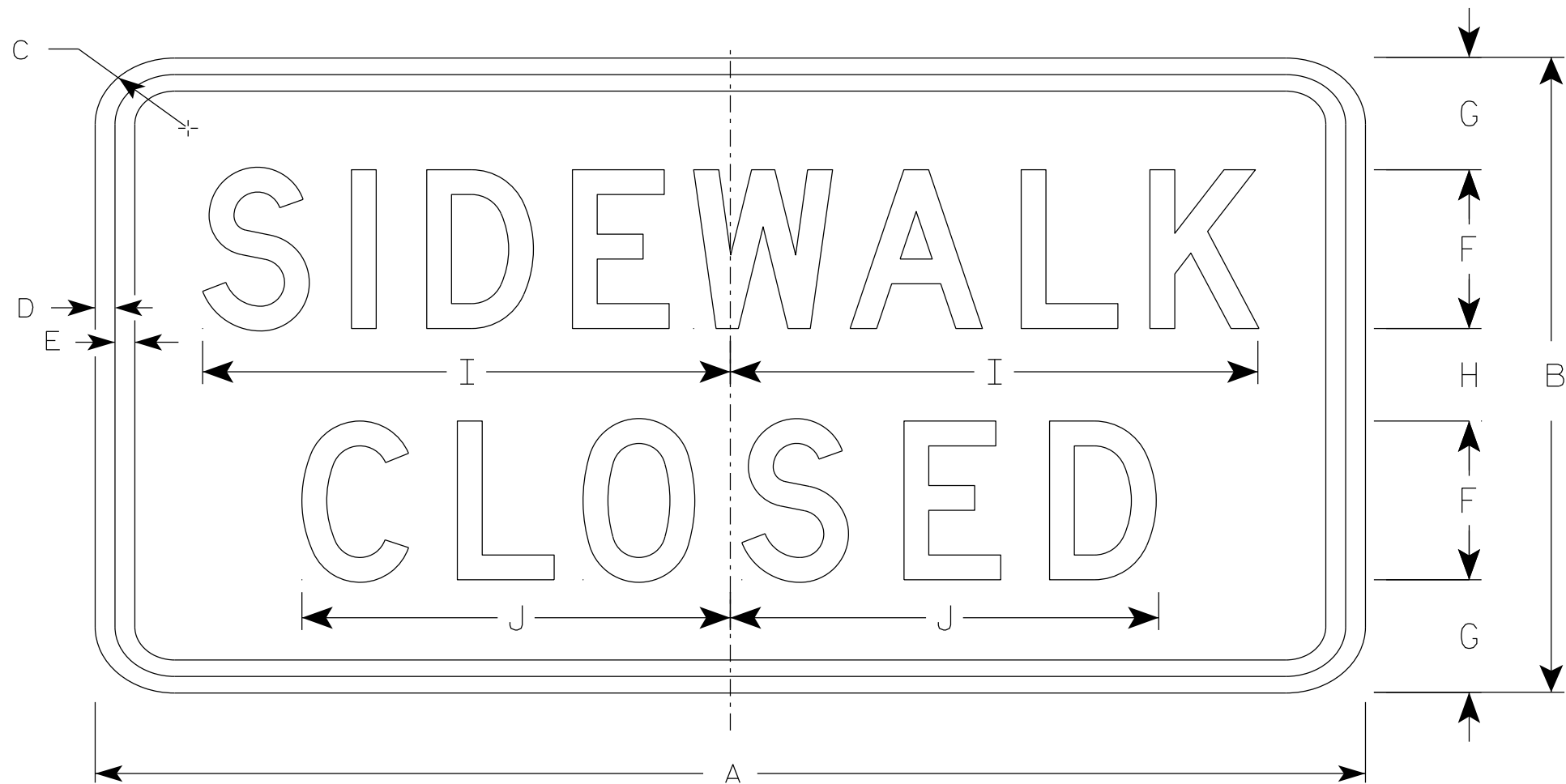
STANDARD SIGN  
M4-9B L&R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 2/9/2023 PLATE NO. M4-9B.4

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R9-9

NOTES

- 1. Sign is Type II - Type H Reflective
- 2. Color:
  - Background - White
  - Message - Black
- 3. Message Series - C
- 4. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
2M	24	12	1 1/2	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
3	30	18	1 1/2	1/2	1/2	4	3 1/2	3	12 1/2	10 1/4																	3.75
4																											
5																											

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E

STANDARD SIGN

R9-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*

For State Traffic Engineer

DATE 1/24/24

PLATE NO. R9-9.7

## Notes



## ***Wisconsin Department of Transportation***

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