



Wisconsin Department of Transportation

December 19, 2024

Division of Transportation Systems Development

Bureau of Project Development
4822 Madison Yards Way, 4th Floor South
Madison, WI 53705

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NOTICE TO ALL CONTRACTORS:

Proposal #41: 9925-00-70, WISC 2025178
Mellen - USH 2
STH 13 to Ashland/Iron County line
STH 169
Ashland County

9925-00-71, WISC 2025179
Mellen - USH 2
Bad River Bridge B-02-0026
STH 169
Ashland County

Letting of January 14, 2025

This is Addendum No. 02, which provides for the following:

Special Provisions:

Deleted Special Provisions	
Article No.	Description
21	Pulverize and Relay -This special is covered under Addenda 1, Article 33 "Stabilized Base Reclamation"

Plan Sheets:

Added Plan Sheets	
Plan Sheet	Plan Sheet Title (brief description of why sheet was added)
43A	Added missing plan sheet for STA 0+28 – 32+00
74A	Added Standard Detail Drawing 13B01-11a "Pavement Details for Railroad Approach"
74B	Added Standard Detail Drawing 13B01-11b "Typical Sections for Railway Approach"

The responsibility for notifying potential subcontractors and suppliers of these changes remains with the prime contractor.

Sincerely,

Mike Coleman

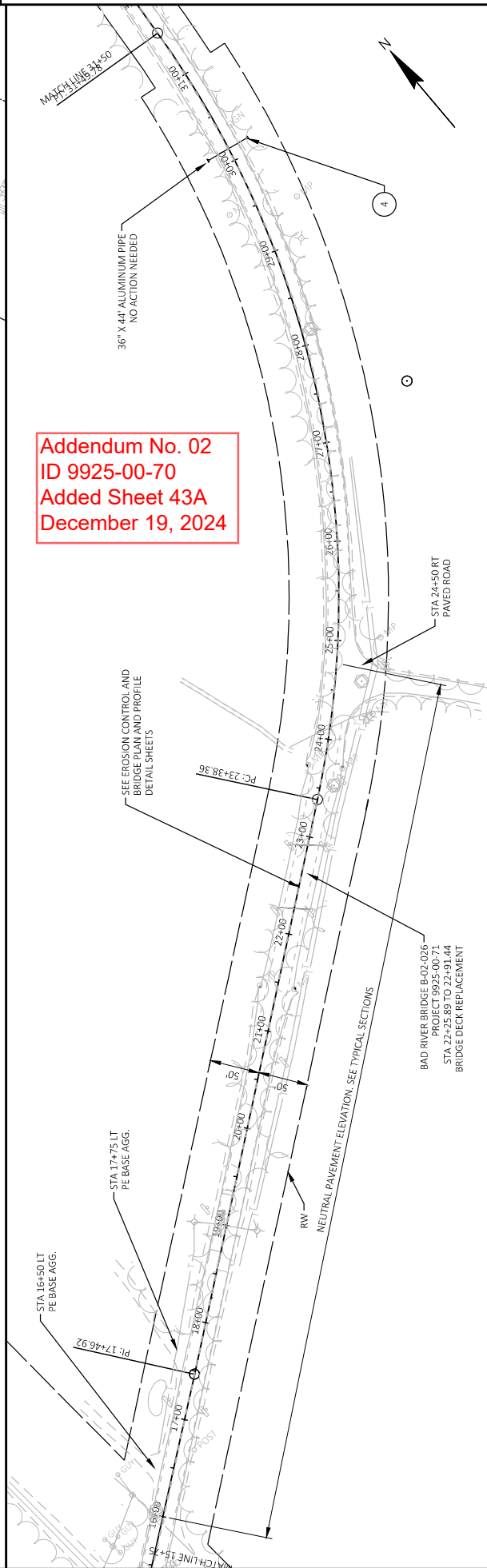
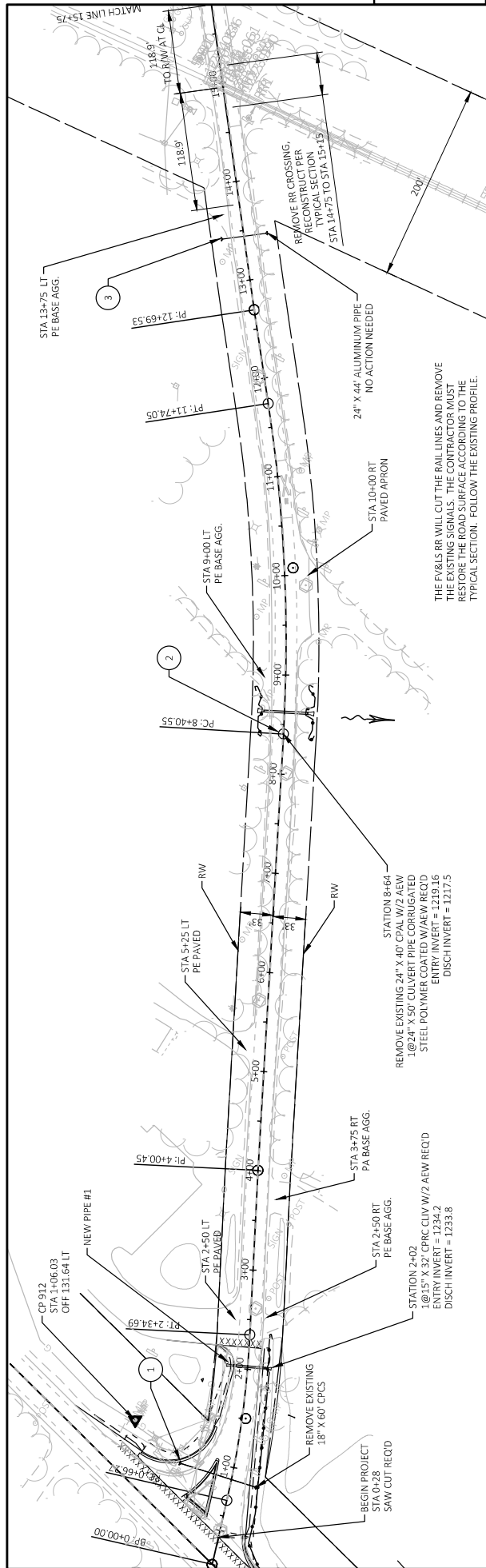
Proposal Development Specialist
Proposal Management Section

ADDENDUM NO. 2
9925-00-70
December 18, 2024

Special Provisions

21. Deleted

END OF ADDENDUM

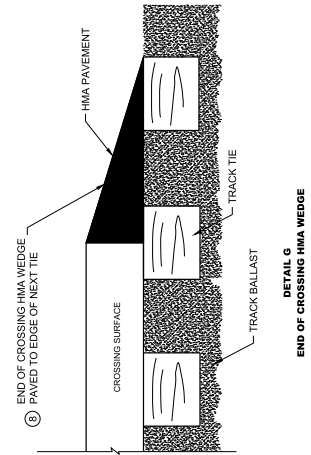
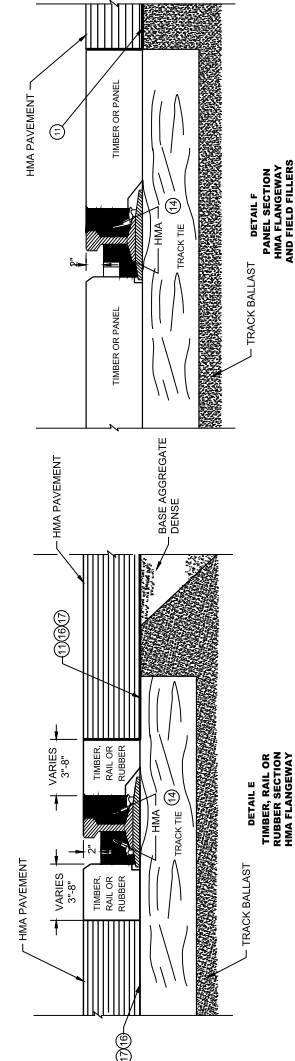
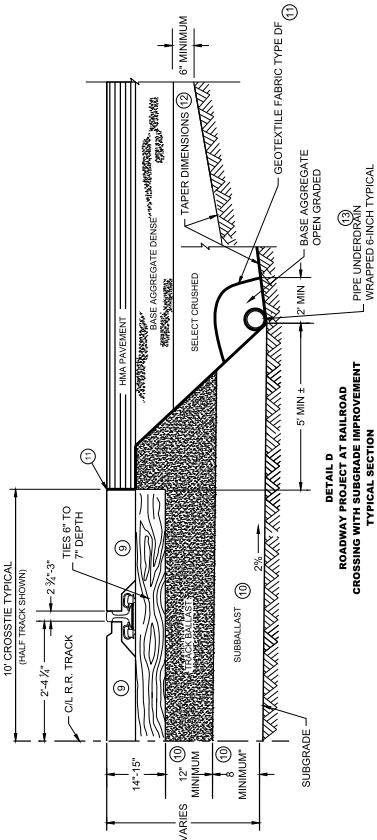
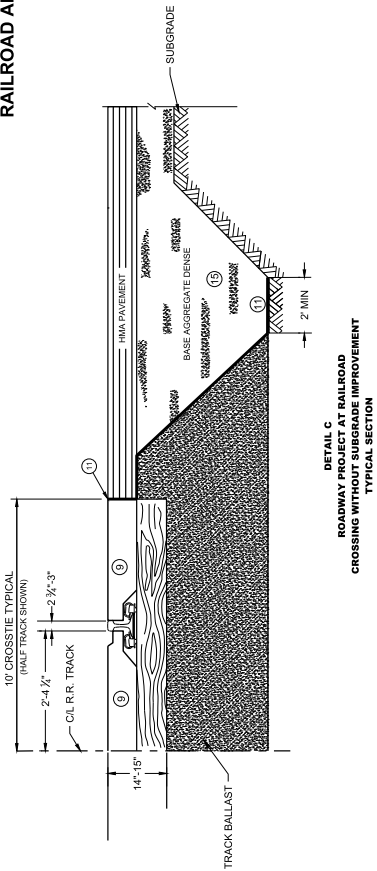


Addendum No. 02
ID 9925-00-70
Added Sheet 43A
December 19, 2024

PROJECT NO:	9925-00-70	HWY:	STH 169	COUNTY:	ASHLAND	PLAN	PEARSON, MICHAEL R	1 IN=100 FT	SHEET	43A	E
FILE NAME:	C:\CIVIL 3D PROJECTS\99250000\SHEETS\PLAN\99250000_PLN.DWG										
LAYOUT NAME:	P1										

WISDOT C-005 SHEET 44

TYPICAL SECTIONS FOR RAILROAD APPROACH



GENERAL NOTES

- ⑧ IF METAL END PLATES ARE NOT INSTALLED BY THE RAILROAD THEN HMA PAVEMENT WEDGE SHALL BE PLACED AT THE END OF THE LAST PANEL TAPERED TO BACK EDGE OF NEXT TIE AND THOROUGHLY COMPACTED. SEE DETAIL A AND B.
- ⑨ MATCH THE CROSSING TYPE THAT IS INSTALLED UNLESS OTHERWISE DIRECTED BY PROJECT ENGINEER.
- ⑩ TRACK BALLAST AND SUBBALLAST REQUIRED 12" AND 8" MINIMUM DEPTHS RESPECTIVELY. DIMENSION FROM BOTTOM OF TRACK TIE TO HIGH SIDE OF 2% SLOPE. THE 2% SLOPE IS REQUIRED ON RAILROAD SUBBALLAST. SEE PLAN FOR CROWN, MATERIAL, THICKNESS, AND SLOPE DIRECTION. SUBBALLAST CAN BE HMA, 1 1/2" BASE AGGREGATE DENSE, SELECT CRUSHED, OR A COMBINATION OF THEM.
- ⑪ GEOTEXTILE FABRIC TYPE SAS PLACED IN ORDER TO PROVIDE STABILIZATION AND SEPARATION ON TOP OF THE TRACK BALLAST WHERE IT IS UNDER HMA PAVEMENT. BASE AGGREGATE DENSE OR SELECT CRUSHED MATERIAL AND THE FIELD SIDE BALLAST CRIBS. GEOTEXTILE FABRIC TYPE DP PLACED IN ORDER TO PROVIDE STABILIZATION AND SEPARATION ON TOP OF THE TRACK BALLAST WHERE IT IS UNDER HMA PAVEMENT. PLACING GEOTEXTILE FABRIC OR GEOGRID UNDER THE SUBBALLAST IS OPTIONAL.
- ⑫ TAPER DIMENSIONS PROVIDED BY PLAN OR BY PROJECT ENGINEER.
- ⑬ IF SHOWN ON THE PLAN, TYPICAL 6-INCH PERFORATED P.V.C. SCHEDULE 80 PIPE UNDERDRAIN TO BE PLACED ALONG THE TOE OF SLOPE, GRADED TO DRAIN AND DAYLIGHT OR INTO STORM SEWER. BASE AGGREGATE OPEN GRADED OVER PIPE UNDERDRAIN AND THEN WRAPPED IN GEOTEXTILE FABRIC TYPE DF SCHEDULE A IN ORDER TO STABILIZE AND SEPARATE FROM SELECT CRUSHED.
- ⑭ HMA FLANGEWAY AND FIELD FILLERS ARE TO BE PLACED AND THOROUGHLY HAND COMPACTED BY THE CONTRACTOR. WHEN NOT PROVIDED BY OTHERS AS PART OF THE CROSSING SURFACE MATERIAL. IF THE CROSSING SURFACE IS NOT BEING REPLACED, THEN REMOVE AND REPLACE THE HMA FLANGEWAY AND FIELD FILLERS AS DIRECTED BY THE RAILROAD OR PROJECT ENGINEER.
- ⑮ GRADE TO MATCH EXISTING OR PROPOSED TYPICAL SECTION OF ROADWAY. SEE PLAN OR PROJECT ENGINEER FOR MORE DETAIL. IF NOT NOTED OTHERWISE IN THE PLAN, BACKFILL ANY REMOVED BASE AND SUBGRADE WITH BASE AGGREGATE DENSE.
- ⑯ IF THE CROSSING IS NOT BEING REPLACED, REMOVE AND REPLACE HMA AS DIRECTED BY RAILROAD AND PROJECT ENGINEER. CARE MUST BE TAKEN TO NOT DAMAGE CROSSING PANELS, TIES, RAIL PLATES AND SPIKES.
- ⑰ PLACE HMA FULL DEPTH. AGGREGATE IS NOT TO BE PLACED BETWEEN THE RAILROAD TIES AND THE HMA PAVEMENT.

Addendum No. 02
ID 9925-00-70
Added Sheet 74B
December 19, 2024

TYPICAL SECTIONS FOR RAILWAY APPROACH

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE
STATE SAFETY SUPERVISOR
AND SAFETY SUPERVISOR

74B

