



Wisconsin Department of Transportation

December 19, 2024

Division of Transportation Systems
Development
Bureau of Project Development
4822 Madison Yards Way, 4th Floor South
Madison, WI 53705

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NOTICE TO ALL CONTRACTORS:

Proposal #41: 9925-00-70, WISC 2025178
Mellen - USH 2
STH 13 to Ashland/Iron County line
STH 169
Ashland County

9925-00-71, WISC 2025179
Mellen - USH 2
Bad River Bridge B-02-0026
STH 169
Ashland County

Letting of January 14, 2025

This is Addendum No. 02, which provides for the following:

Special Provisions:

Deleted Special Provisions	
Article No.	Description
21	Pulverize and Relay -This special is covered under Addenda 1, Article 33 "Stabilized Base Reclamation"

Plan Sheets:

Added Plan Sheets	
Plan Sheet	Plan Sheet Title (brief description of why sheet was added)
43A	Added missing plan sheet for STA 0+28 – 32+00
74A	Added Standard Detail Drawing 13B01-11a "Pavement Details for Railroad Approach"
74B	Added Standard Detail Drawing 13B01-11b "Typical Sections for Railway Approach"

The responsibility for notifying potential subcontractors and suppliers of these changes remains with the prime contractor.

Sincerely,

Mike Coleman

Proposal Development Specialist
Proposal Management Section

ADDENDUM NO. 2

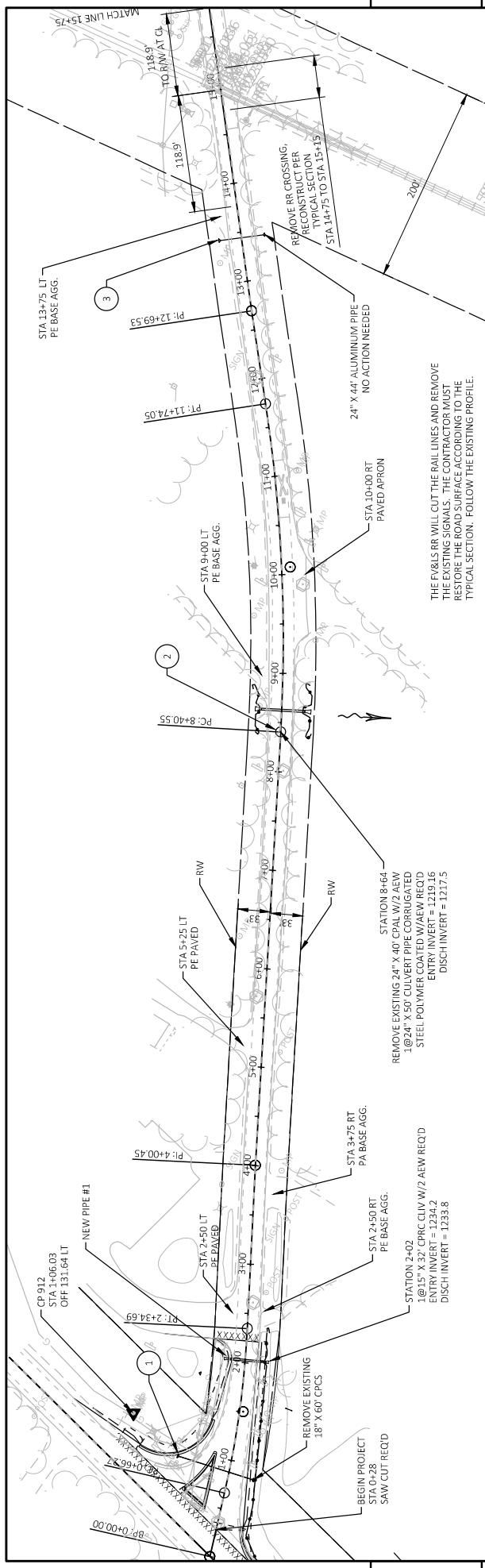
9925-00-70

December 18, 2024

Special Provisions

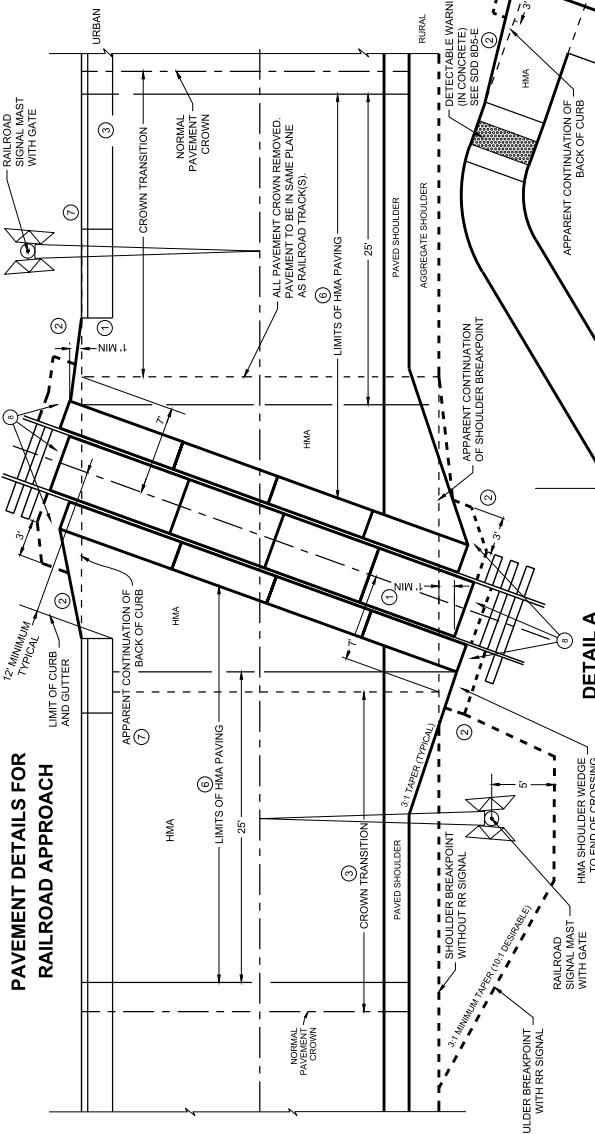
- 21. Deleted**

END OF ADDENDUM



C:\CIVIL 3D\PROJECTS\989250000\SHEETS\PLAN98925000.PLN.DWG
FILE NAME :
PROJECT NO: 9925-00-70 HWY: 5TH 169 COUNTY: ASHLAND PLAN PLOT DATE: 11/13/2024 10:29 AM PLOT BY: PEARSON, MICHAEL R PLOT NAME: 1 IN 100 FT PLOT SCALE:
43A SHEET E Two-Side Print Count: 1

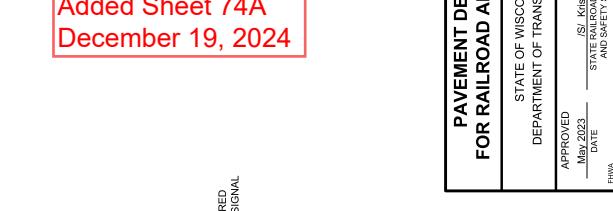
Addendum No. 02
ID 9925-00-70
Added Sheet 74A
December 19, 2024



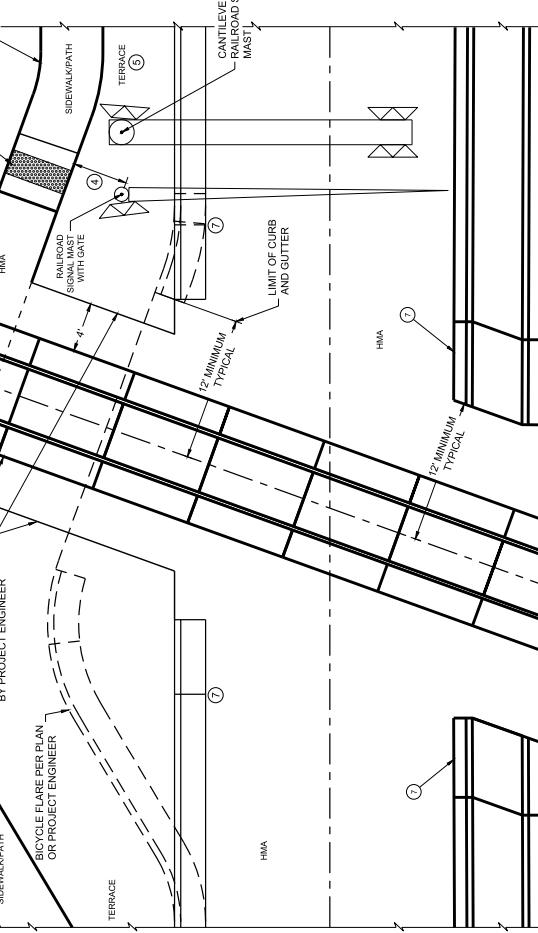
RAILROAD APPROACH

GENERAL NOTES CONTINUED

- ① 1' MINIMUM CROSSING SURFACE COVERAGE PAST THE APPARENT CONTINUATION OF SHOULDER BREAKPOINT BACK OF CURB OR OUTSIDE EDGE OF SIDEWALKPATH. INDIVIDUAL RAILROADS MAY HAVE DIFFERENT MINIMUM STANDARDS.
- ② HMA FLARE FROM OUTSIDE EDGE OF SIDEWALKPATH BACK OF CURB OR AGGREGATE SHOULDER BREAKPOINT TO THE END OF CROSSING SURFACE MATERIAL.
- ③ CROWN TRANSITION LENGTH SHOWN ELSEWHERE IN THE PLAN.
- ④ REFERENCE SDD 8-D-01 END SECTION CURB AND GUTTER. MEDIAN END NEAR THE TRACK SHOULD BE PARALLEL TO THE TRACK. 6'-0" TAVER FOR A MEDIAN SHOULD BE REDUCED TO 6'-0" IF THE CENTER OF SIGNAL OR GATE MAST IS LESS THAN 3'-0" FROM THE CENTER OF SIGNAL OR GATE. NEAR THE EDGE OF SIDEWALKPATH, BACK OF CURB OR AGGREGATE SHOULDER BREAKPOINT TO THE END OF CROSSING SURFACE MATERIAL.
- ⑤ PROJECT ENGINEER THE TERRACE AND SIDEWALKPATH GRADES SHALL BE TRANSITIONED TO MATCH THE GRADE OF THE TRACK FIELD FIT TO AVOID PONDING.
- ⑥ 25' MINIMUM HMA PAVING MEASURED PARALLEL TO THE ROAD OR 10' MINIMUM MEASURED PERPENDICULAR TO THE TRACK FROM THE EDGE OF THE CROSSING SURFACE, WHICHEVER IS GREATER.
- ⑦ REFERENCE SDD 8-D-01 END SECTION CURB AND GUTTER. MEDIAN END NEAR THE TRACK SHOULD BE PARALLEL TO THE TRACK. 6'-0" TAVER FOR A MEDIAN SHOULD BE REDUCED TO 6'-0" IF THE CENTER OF SIGNAL OR GATE MAST IS LESS THAN 3'-0" FROM THE CENTER OF SIGNAL OR GATE. NEAR THE EDGE OF SIDEWALKPATH, BACK OF CURB OR AGGREGATE SHOULDER BREAKPOINT TO THE END OF CROSSING SURFACE MATERIAL.
- ⑧ IF METAL END PLATES ARE NOT INSTALLED BY THE RAILROAD THEN HMA PAVEMENT WEDGE SHALL BE PLACED AT THE END OF THE AS-TAPER TAPERED TO BACK EDGE OF NEXT TIE AND THOROUGHLY COMPACTED SEE DE TALL G.



DETAIL B
MEDIAN AND SIDEWALK/SHARED-USE PATH APPROACH

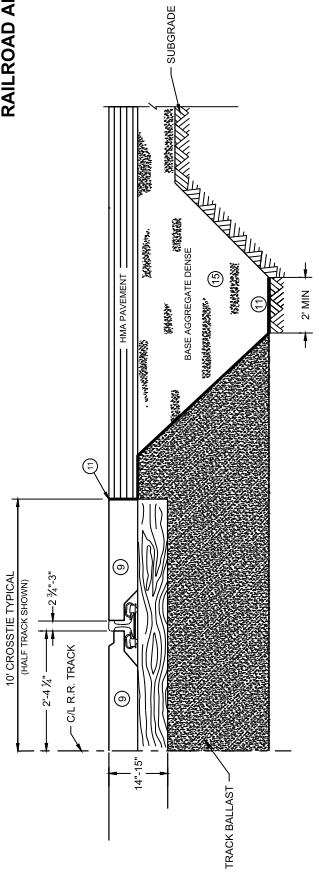


DETAIL B
MEDIAN AND SIDEWALK/SHARED-USE PATH APPROACH

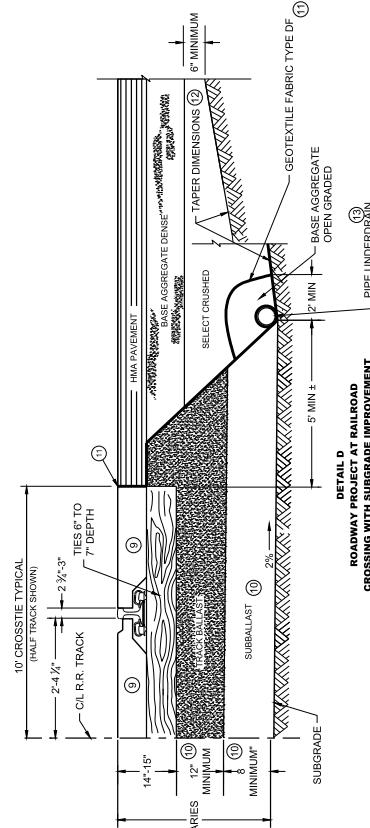
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
APPROVED
May 2023

74A

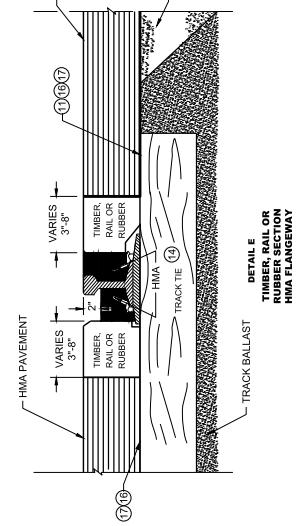
TYPICAL SECTIONS FOR RAILROAD APPROACH



DETAIL C
ROADWAY PROJECT AT RAILROAD
CROSSING WITHOUT SUBGRADE IMPROVEMENT
TYPICAL SECTION



DETAIL D
ROADWAY PROJECT AT RAILROAD
CROSSING WITH SUBGRADE IMPROVEMENT



DETAIL G
END OF CROSSING HMA WEDGE

GENERAL NOTES

- (3) IF METAL END PLATES ARE NOT INSTALLED BY THE RAILROAD THEN HMA PAVEMENT WEDGE SHELL BE PLACED AT THE END OF THE LAST PANEL TAPERED TO BACK EDGE OF NEXT TIE AND THOROUGHLY COMPACTED. SEE A AND B.
 - (4) MATCH THE CROSTING TYPE THAT IS INSTALLED UNLESS OTHERWISE DIRECTED BY PROJECT ENGINEER.
 - (5) TRACK BALAST AND SUBBALLAST REQUIRED 12' AND 8' MINIMUM DEPTHS FURTHERMORE DIMENSION FROM BOTTOM OF TRACK TO HIGH SIDE OF 2% SLOPE. THE 2% SLOPES IS REQUIRED ON RAILROAD SUBBALLAST. SEE A FOR CROWN MATERIAL THICKNESS, AND SLOPE DIRECTION SUBBALLAST IS MA, 1 1/2" BASE AGGREGATE DENSE, SELECT CRUSHED, OR A COMBINATION OF THEM.
 - (6) GEOTEXTILE FABRIC TYPE SAS PLACED IN ORDER TO PROVIDE STABILIZATION AND SEPARATION ON TOP OF THE TRACH BALLAST WHERE IT IS UNDER HMA PAVEMENT. BASE AGGREGATE OR SELECT CRUSHED MATERIAL AND THE FIELD SIDE BALAST CRIBS GEOTEXTILE FABRIC TYPE OF PLACED IN ORDER TO PROVIDE STABILIZATION AND SEPARATION UNDER AND OVER THE PIPE UNDERDRAIN. PLACING GEOTEXTILE FABRIC OR GEOGRID UNDER THE SUBBALLAST IS OPTIONAL.

- (13) IF SHOWN ON THE PLAN, TYPICAL 6-INCH PERFORATED PVC SCHEDULE 80 PIPE UNDERDRAIN TO BE PLACED ALONG THE LINE OF SLOPE, GRADED TO DRAIN AND DAYLIGHT OR INTO A STORM SEWER, BASE AGGREGATE OVERGRADED OVER PIPE UNDERDRAIN AND THEN WRAPPED IN GEOTEXTILE FABRIC, TYPE D OF SPECIFICE A IN ORDER TO STABILIZE AND SEPARATE FROM SELECT CRUSHED

(14) HMA LANGEVANX AND FIELD FILLERS ARE TO BE PLACED AND THOROUGHLY HAND COMPACTED BY THE CONTRACTOR. WHEN NOT PROTECTED BY OTHERS AS PART OF THE CROSSING SURFACE MATERIAL, IF THE CROSSING SURFACE IS NOT BEING REPLACED, THEIR REMOVE AND REPLACE THE HMA LANGEVANX AND FIELD FILLER AS DIRECTED BY THE RAILROAD OR PROJECT ENGINEER

(15) GRADE TO MATCH EXISTING OR PROPOSED TYPICAL SECTION OF ROADWAY. SEE PLAN OR PROJECT ENGINEER FOR MORE DETAIL. IF NOT NOTED OTHERWISE IN THE PLAN, BACKFILL ANY REMOVED BASE AND SUBGRADE WITH BASE AGGREGATE DENSE, DENSE.

- (16) IF THE CROSSING IS NOT BEING REPLACED, REMOVE AND REPLACE HMAAS DIRECTED BY RAILROAD AND PROJECT ENGINEER. CARE MUST BE TAKEN TO DAMAGE CROSSING PANELS, TIES, RAIL, PLATES AND SPIKES.

(17) PLACE HMA FULL DEPTH AGGREGATE IS NOT TO BE PLACED BETWEEN THE

- RAILROAD TIES AND THE HMA PAVEMENT.

Addendum No. 02
ID 9925-00-70
Added Sheet 74B
December 19, 2024

