EAU

PROJECT ID: 8887-03-76

WITH: N/A

COUNTY: TAYLOR

NOVEMBER 2024
ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 74

N

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

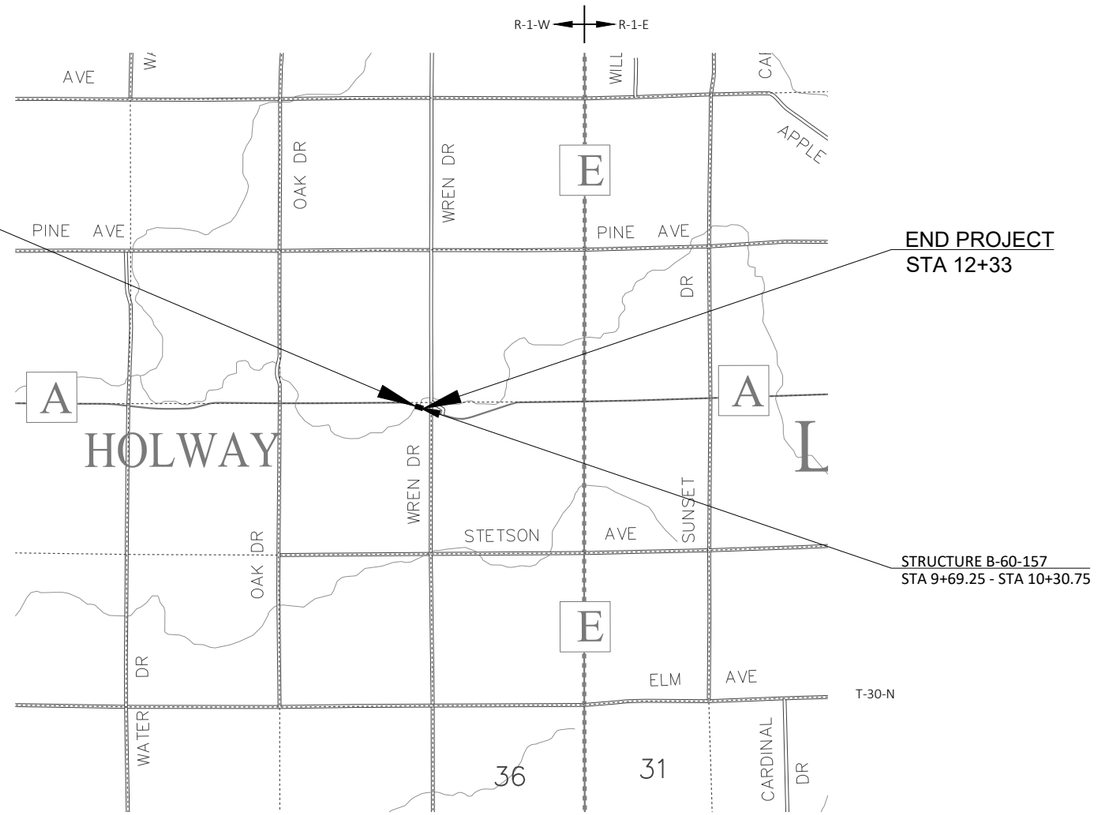
LUBLIN - STETSONVILLE

PINE CREEK BRIDGE B-60-0157

CTH A
TAYLOR COUNTY

STATE PROJECT NUMBER
8887-03-76

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
8887-03-76	WISC 2025049	1



BEGIN PROJECT
STA 7+67
Y= 315679.060
X= 620776.000

END PROJECT
STA 12+33

STRUCTURE B-60-157
STA 9+69.25 - STA 10+30.75

LAYOUT
SCALE 0 1 MI

TOTAL NET LENGTH OF CENTERLINE = 0.093 MI

DESIGN DESIGNATION

A.A.D.T.	(2025)	=	640
A.A.D.T.	(2045)	=	608
D.H.V.		=	N/A
D.D.		=	50/50
T.		=	7.5%
DESIGN SPEED		=	55 MPH
ESALS		=	88,000

CONVENTIONAL SYMBOLS

PLAN
- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE (300'EB')
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE
- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES
- ELECTRIC (E)
- FIBER OPTIC (FO)
- GAS (G)
- OVERHEAD (OH)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), TAYLOR COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 18.

ACCEPTED FOR
TAYLOR COUNTY
Date 7/30/24 *Benjamin Stanfley*
(Signature and Title of Official)

ORIGINAL PLANS PREPARED BY
CORRE ENGINEERING
MADISON | EAU CLAIRE | WAUKESHA | APPLETON | TOMAH | WAUSAU

WISCONSIN ERIC T. PRICE E-39027 MADISON WI PROFESSIONAL ENGINEER
July 30, 2024

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	CORRE, INC
Designer	CORRE, INC
Project Manager	MATTHEW BERG
Regional Examiner	TOU YANG
Regional Supervisor	MATTHEW BERG

APPROVED FOR THE DEPARTMENT
DATE: 7/30/2024
(Signature)

E

FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\501_C3D_2022\88870306\SHEETS\010101-TI.DWG PLOT DATE : 5/14/2024 9:30 AM PLOT BY : NICHOLAS WATHKE, PE PLOT NAME :

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

D.O.T. BRIDGE BENCHMARK MONUMENT TO BE FURNISHED BY THE STATE AND PLACED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER.

CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY OPERATIONS, OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.

ASPHALTIC SURFACE WEIGHT CALCULATIONS ARE BASED ON 112 LBS/SY/IN.

TACK COAT VOLUME CALCULATIONS ARE BASED ON 0.07 GAL/SY.

4-INCH ASPHALTIC SURFACE, SHALL BE CONSTRUCTED WITH 1.75-INCH UPPER LAYER AND 2.25-INCH LOWER LAYER.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

WHEN THE QUANTITY OF THE ITEMS OF BASE AGGREGATE, SUBBASE OR HMA PAVEMENT IS MEASURED FOR PAYMENT BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE LAYERS SHOWN ON THE PLAN IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

EROSION CONTROL FEATURES AS SHOWN IN THE PLANS ARE AT APPROXIMATE LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE CONTRACTOR'S EROSION CONTROL IMPLEMENTATION PLAN (ECIP) AND APPROVED BY THE ENGINEER. MAINTAIN EROSION CONTROL MEASURES UNTIL SUCH A TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NECESSARY.

CURVE DATA IS BASED ON THE ARC DEFINITION.

PRIOR TO PLACEMENT OF BEAM GUARD THE SHOULDERS SHALL BE IN PLACE, SHAPED AND COMPACTED.

BEARINGS SHOWN ON THE PLAN ARE GROUND BEARINGS TO THE NEAREST SECOND.

DO NOT DRIVE OR STORE EQUIPMENT, OR STORE CONSTRUCTION MATERIALS IN ENVIRONMENTALLY SENSITIVE AREAS, WETLANDS OR WATERWAYS.

RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS:	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIPTURF:	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPETURF:			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT:	.70 - .95											
CONCRETE:	.80 - .95											
BRICK:	.70 - .80											
DRIVES, WALKS:	.75 - .85											
ROOFS:	.75 - .95											
GRAVEL ROADS, SHOULDERS:	.40 - .60											

TOTAL PROJECT AREA = 1.07 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.36 ACRES

WISCONSIN DNR LIAISON

WENDY HENNIGES
DNR NORTHERN REGION HQ
107 SUTLIFF AVENUE
RHINELANDER, WI 54501
PHONE: (715) 365-8916
EMAIL: WENDY.HENNIGES@WISCONSIN.GOV

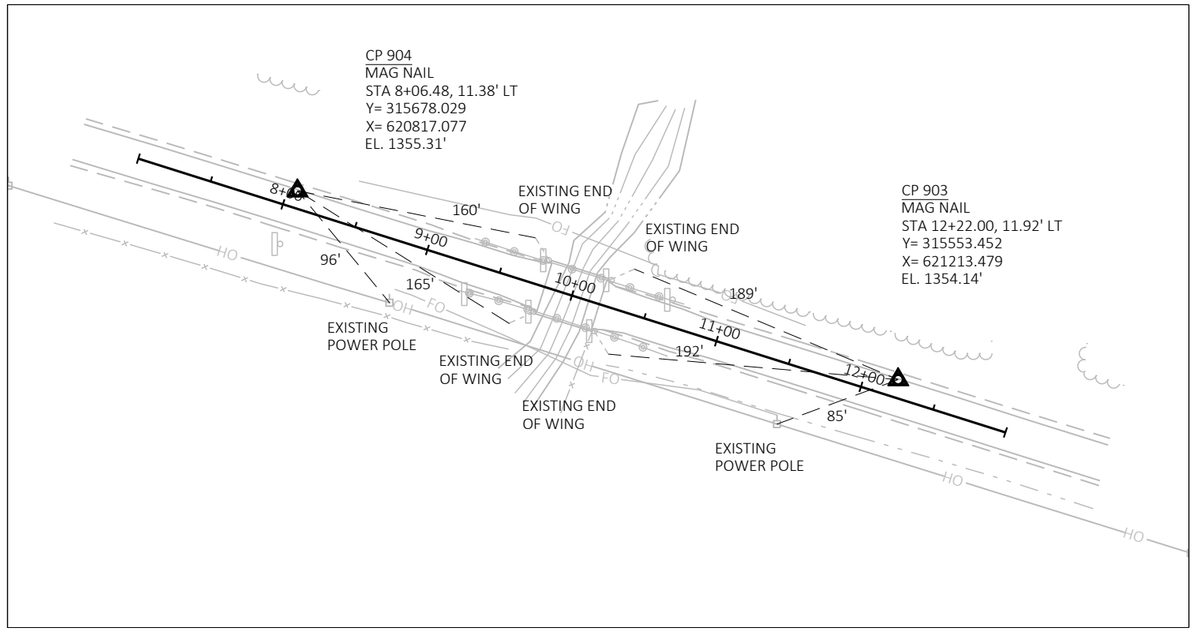
CONSULTANT CONTACT

ERIC PRICE, P.E.
CORRE, INC.
6510 GRAND TETON PLAZA, SUITE 314
MADISON, WI 53719
PHONE: (608) 826-6146
EMAIL: EPRICE@CORREINC.COM

COUNTY HIGHWAY COMMISSIONER

BENJAMIN STANFLEY
TAYLOR COUNTY
PO BOX 89
MEDFORD, WI 54451
PHONE: (715) 748-2456
EMAIL: HWY@CO.TAYLOR.WI.US

CONTROL POINT TIES



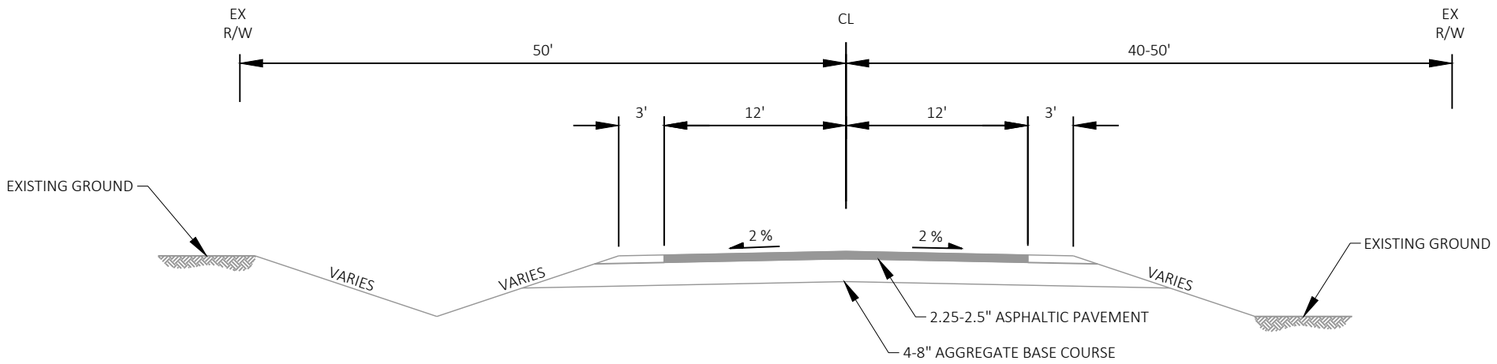
UTILITIES CONTACTS

TAYLOR ELECTRIC COOPERATIVE
ELECTRIC
WADE MATYKA
N1831 STH 13
MEDFORD, WI 54451
PHONE: (715) 678-2411
EMAIL: WADE@TAYLORELECTRIC.ORG

TDS TELECOM
FIBER OPTIC
KEVIN SCHEUNENMANN
202 OGDEN ST
MEDFORD, WI 54451
PHONE: (715) 421-9191
EMAIL: KEVIN.SCHEUNENMANN@TDSTELECOM.COM

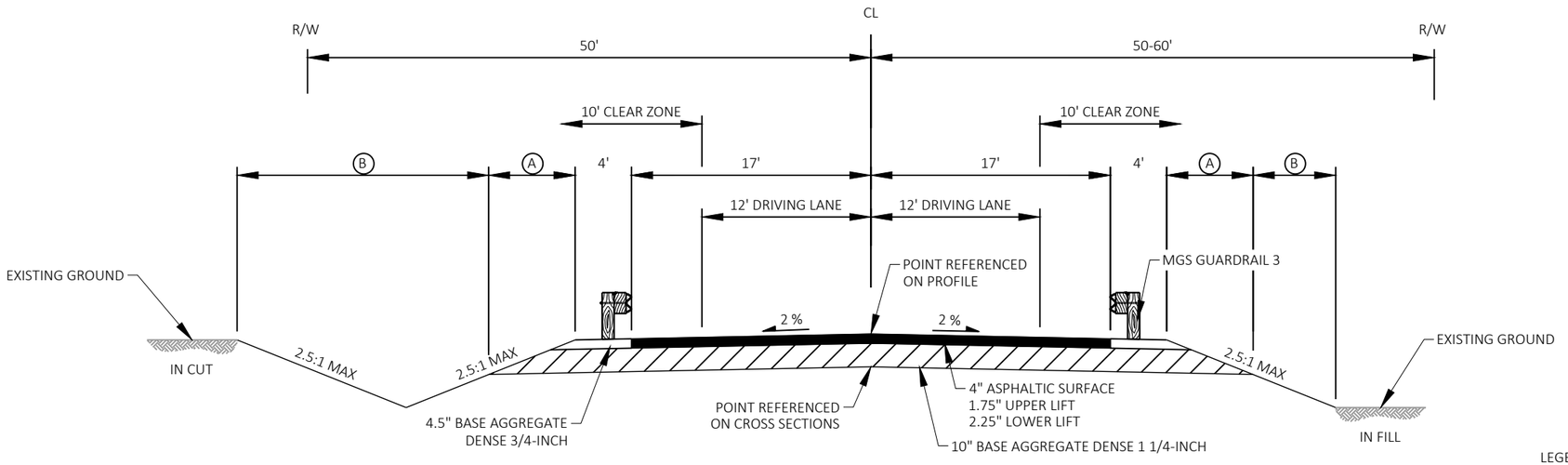
DIGGERS HOTLINE
Dial 811 or (800)242-8511
www.DiggersHotline.com

2



EXISTING TYPICAL SECTION

STA 7+67 - 9+81.0±
STA 10+19.0± - 12+33



FINISHED TYPICAL SECTION

STA 7+67 - 9+69.25
STA 10+30.75 - 12+33

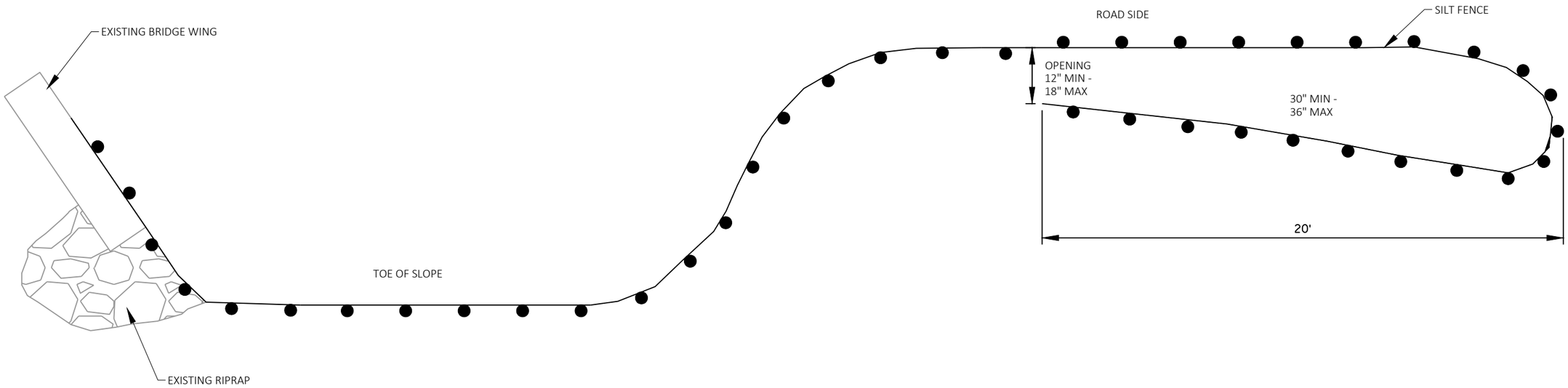
LEGEND

(A) FERTILIZER TYPE B, SEEDING NO. 20

(B) SALVAGED TOPSOIL; EROSION MAT URBAN CLASS I TYPE B; FERTILIZER TYPE B; SEEDING MIXTURE NO. 20

PROJECT NO: 8887-03-76	HWY: CTH A	COUNTY: TAYLOR	TYPICAL SECTIONS	SHEET 3	E

2



GENERAL NOTES:
SILT FENCE POSTS FOR THE TURN-AROUND SHOULD BE ON THE OUTSIDE OF THE TURN-AROUND. AND TRENCHED IN ACCORDING TO SILT FENCE REQUIREMENTS.

PLAN VIEW

TEMPORARY SMALL ANIMAL TURN-AROUND

2

2

LEGEND

APPROXIMATE EXTENT OF CREOSOTE CONTAMINATED SOIL AT DEPTHS OF 3' MAXIMUM TO EXCAVATION DEPTH

N

PINE CREEK

TLE

EX R/W

SLOPE INTERCEPT

9+00

10+00

11+00

CTH A

EX R/W

PROP R/W

PROP R/W

PROPOSED STRUCTURE
B-60-157
SEE STRUCTURE PLANS

NOTE: FINAL LOCATION AND LIMITS OF CREOSOTE CONTAMINATED SOIL TO BE VERIFIED WITH THE DEPARTMENT RETAINED ENVIRONMENTAL CONSULTANT.

PROJECT NO: 8887-03-76	HWY: CTH A	COUNTY: TAYLOR	CONSTRUCTION DETAILS - CONTAMINATED SOIL	SHEET 5	E

FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\501_C3D_2022\88870306\SHEETS\021002-CD.DWG
LAYOUT NAME - 01

PLOT DATE : 7/29/2024 1:16 PM PLOT BY : NICHOLAS WATHKE, PE PLOT NAME : PLOT SCALE : 1 IN:20 FT

WISDOT/CADDS SHEET 42

Estimate Of Quantities

8887-03-76

Line	Item	Item Description	Unit	Total	Qty
0002	201.0105	Clearing	STA	1.000	1.000
0004	201.0205	Grubbing	STA	1.000	1.000
0006	203.0260	Removing Structure Over Waterway Minimal Debris (structure) 01. B-60-14	EACH	1.000	1.000
0008	204.0165	Removing Guardrail	LF	212.000	212.000
0010	205.0100	Excavation Common	CY	690.000	690.000
0012	205.0505.S	Excavation, Hauling, and Disposal of Creosote Contaminated Soil and Management of Contaminated Groundwater	TON	180.000	180.000
0014	206.1001	Excavation for Structures Bridges (structure) 01. B-60-157	EACH	1.000	1.000
0016	208.0100	Borrow	CY	40.000	40.000
0018	210.1500	Backfill Structure Type A	TON	860.000	860.000
0020	213.0100	Finishing Roadway (project) 01. 8887-03-76	EACH	1.000	1.000
0022	305.0110	Base Aggregate Dense 3/4-Inch	TON	105.000	105.000
0024	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	1,020.000	1,020.000
0026	455.0605	Tack Coat	GAL	100.000	100.000
0028	465.0105	Asphaltic Surface	TON	320.000	320.000
0030	502.0100	Concrete Masonry Bridges	CY	189.000	189.000
0032	502.3200	Protective Surface Treatment	SY	329.000	329.000
0034	503.0137	Prestressed Girder Type I 36W-Inch	LF	240.000	240.000
0036	505.0400	Bar Steel Reinforcement HS Structures	LB	6,020.000	6,020.000
0038	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	20,820.000	20,820.000
0040	506.2605	Bearing Pads Elastomeric Non-Laminated	EACH	8.000	8.000
0042	506.4000	Steel Diaphragms (structure) 01. B-60-157	EACH	3.000	3.000
0044	513.4061	Railing Tubular Type M	LF	128.000	128.000
0046	516.0500	Rubberized Membrane Waterproofing	SY	18.000	18.000
0048	550.0500	Pile Points	EACH	16.000	16.000
0050	550.1100	Piling Steel HP 10-Inch X 42 Lb	LF	460.000	460.000
0052	606.0300	Riprap Heavy	CY	185.000	185.000
0054	612.0406	Pipe Underdrain Wrapped 6-Inch	LF	220.000	220.000
0056	614.2300	MGS Guardrail 3	LF	150.000	150.000
0058	614.2500	MGS Thrie Beam Transition	LF	157.600	157.600
0060	614.2610	MGS Guardrail Terminal EAT	EACH	4.000	4.000
0062	618.0100	Maintenance and Repair of Haul Roads (project) 01. 8887-03-76	EACH	1.000	1.000
0064	619.1000	Mobilization	EACH	1.000	1.000
0066	625.0500	Salvaged Topsoil	SY	1,750.000	1,750.000
0068	628.1504	Silt Fence	LF	1,095.000	1,095.000
0070	628.1520	Silt Fence Maintenance	LF	1,095.000	1,095.000
0072	628.1905	Mobilizations Erosion Control	EACH	5.000	5.000
0074	628.1910	Mobilizations Emergency Erosion Control	EACH	3.000	3.000
0076	628.2008	Erosion Mat Urban Class I Type B	SY	1,750.000	1,750.000
0078	628.6005	Turbidity Barriers	SY	100.000	100.000
0080	629.0210	Fertilizer Type B	CWT	1.110	1.110
0082	630.0120	Seeding Mixture No. 20	LB	48.000	48.000
0084	630.0500	Seed Water	MGAL	40.000	40.000
0086	634.0612	Posts Wood 4x6-Inch X 12-FT	EACH	4.000	4.000
0088	637.2230	Signs Type II Reflective F	SF	12.000	12.000
0090	638.2602	Removing Signs Type II	EACH	4.000	4.000
0092	638.3000	Removing Small Sign Supports	EACH	4.000	4.000
0094	642.5001	Field Office Type B	EACH	1.000	1.000
0096	643.0420	Traffic Control Barricades Type III	DAY	1,440.000	1,440.000

8887-03-76

Line	Item	Item Description	Unit	Total	Qty
0098	643.0705	Traffic Control Warning Lights Type A	DAY	2,240.000	2,240.000
0100	643.0900	Traffic Control Signs	DAY	1,120.000	1,120.000
0102	643.5000	Traffic Control	EACH	1.000	1.000
0104	645.0111	Geotextile Type DF Schedule A	SY	120.000	120.000
0106	645.0120	Geotextile Type HR	SY	315.000	315.000
0108	646.1020	Marking Line Epoxy 4-Inch	LF	1,520.000	1,520.000
0110	650.4500	Construction Staking Subgrade	LF	466.000	466.000
0112	650.5000	Construction Staking Base	LF	466.000	466.000
0114	650.6501	Construction Staking Structure Layout (structure) 01. B-60-157	EACH	1.000	1.000
0116	650.9911	Construction Staking Supplemental Control (project) 01. 8887-03-76	EACH	1.000	1.000
0118	650.9920	Construction Staking Slope Stakes	LF	466.000	466.000
0120	690.0150	Sawing Asphalt	LF	48.000	48.000
0122	715.0502	Incentive Strength Concrete Structures	DOL	1,134.000	1,134.000
0124	999.2000.S	Installing and Maintaining Bird Deterrent System (station) 01. 10+00	EACH	1.000	1.000
0126	ASP.1T0A	On-the-Job Training Apprentice at $5.00/HR	HRS	300.000	300.000
0128	ASP.1T0G	On-the-Job Training Graduate at $5.00/HR	HRS	300.000	300.000

205.0505.S

STATION	TO	STATION	LOCATION	201.0105 CLEARING STA	201.0205 GRUBBING STA
11+33	-	12+33	CTH A LT	1	1
			TOTAL 0010	1	1

STATION	TO	STATION	LOCATION	204.0165 REMOVING GUARDRAIL LF
9+27	-	9+80	CTH A LT	53
9+28	-	9+80	CTH A RT	53
10+19	-	10+72	CTH A LT	53
10+19	-	10+72	CTH A RT	53
			TOTAL 0010	212

LOCATION	EXCAVATION, HAULING, AND DISPOSAL OF CREOSOTE CONTAMINATED SOIL AND MANAGEMENT OF CONTAMINATED GROUNDWATER TON
B-60-14	180
TOTAL 0020	180

STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON
7+67	-	9+69	WEST APPROACH	50
10+30	-	12+33	EAST APPROACH	55
			TOTAL 0010	105

DIVISION	FROM/TO STATION	LOCATION	205.0100 COMMON EXCAVATION (1) CUT (2)	205.0100 COMMON EXCAVATION (1) EBS EXCAVATION	SALVAGED/UNUSABLE PAVEMENT MATERIAL (3)	AVAILABLE MATERIAL (4)	UNEXPANDED FILL	EXPANDED FILL (5) FACTOR 1.25	MASS ORDINATE +/- (6)	208.0100 BORROW
DIVISION 1 -- CTH A	7+40 TO 12+33	CTH A	690		77	613	522	653	-40	40
DIVISION 1 SUBTOTAL			690	0	77	613	522	653	-40	
GRAND TOTAL			690	0	77	613	522	653	-40	40
TOTAL COMMON EXC			690							

NOTES:
(1) COMMON EXCAVATION IS THE SUM OF THE CUT AND EBS EXCAVATION COLUMNS. ITEM NUMBER 205.0100
(2) SALVAGED/UNSUABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.
(3) SALVAGED/UNUSABLE PAVEMENT MATERIAL
(4) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSUABLE PAVEMENT MATERIAL
(5) EXPANDED FILL FACTOR = 1.25
(6) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.
(7) FACTORS USED TO COMPUTE ANTICIPATED WASTE AND THE COMPUTED WASTE VOLUME IDENTIFIED ARE FOR GENERAL INFORMATION ONLY.

STATION	TO	STATION	LOCATION	305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON
7+54	-	9+69	WEST APPROACH	505
10+30	-	12+45	EAST APPROACH	515
			TOTAL 0010	1,020

STATION	TO	STATION	LOCATION	455.0605 TACK COAT GAL	465.0105 ASPHALTIC SURFACE TON
7+54	-	9+69	WEST APPROACH	50	160
10+30	-	12+45	EAST APPROACH	50	160
			TOTAL 0010	100	320

STATION	TO	STATION	LOCATION	614.2300 MGS GUARDRAIL 3 LF	614.2500 MGS THRIE BEAM TRANSITION LF	614.2610 MGS GUARDRAIL TERMINAL EAT EACH
8+17	-	9+72	CTH A RT	62.5	39.4	1
8+67	-	9+72	CTH A LT	12.5	39.4	1
10+28	-	11+33	CTH A RT	12.5	39.4	1
10+28	-	11+83	CTH A LT	62.5	39.4	1
			TOTAL 0010	150	157.6	4

STATION	TO	STATION	LOCATION	625.0500 SALVAGED TOPSOIL SY	628.2008 EROSION MAT URBAN CLASS I TYPE B SY	629.0210 FERTILIZER TYPE B CWT	630.0120 SEEDING MIXTURE NO. 20 LB	630.0500 SEED WATER MGAL
7+40	-	9+69	WEST APPROACH	1,060	1,060	0.67	29	24
10+31	-	12+33	EAST APPROACH	690	690	0.44	19	16
			TOTAL 0010	1,750	1,750	1.11	48	40

STATION	TO	STATION	LOCATION	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF
7+40	-	9+69	WEST APPROACH	562	562
10+31	-	12+33	EAST APPROACH	533	533
			TOTAL 0010	1,095	1,095

LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH
PROJECT	5	3
TOTAL 0010	5	3

LOCATION	634.0612 POSTS WOOD 4X6-INCH X 12-FT EACH	637.2230 SIGNS TYPE II REFLECTIVE F SF	REMARKS
PROPOSED STRUCTURE B-60-157	4	12	PROPOSED BRIDGE HASH MARKS; W5-52L & W5-52R
TOTAL 0010	4	12	

NOTE: PLACE BRIDGE HASH MARK SIGNS PER SDD "SIGNING AND MARKING FOR TWO LANE BRIDGES"

STATION	LOCATION	628.6005 TURBIDITY BARRIERS SY
9+82	WEST ABUTMENT	50
10+16	EAST ABUTMENT	50
	TOTAL 0010	100

LOCATION	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	REMARKS
EXISTING BRIDGE B-60-14	4	4	EXISTING BRIDGE HASH MARKS
TOTAL 0010	4	4	

LOCATION	DURATION	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAY	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	643.0900 TRAFFIC CONTROL SIGNS DAY	643.5000 TRAFFIC CONTROL EACH
PROJECT	70 DAYS	1,440	2,240	1,120	1
	TOTAL 0010	1,440	2,240	1,120	1

STATION	TO	STATION	LOCATION	646.1020 MARKING LINE EPOXY 4-INCH LF
7+67	-	12+33	CL (YELLOW)	588
7+67	-	12+33	EDGELINE (WHITE)	932
			TOTAL 0010	1,520

LOCATION	650.6501.01 CONSTRUCTION STAKING STRUCTURE LAYOUT (STRUCTURE) (01. B-60-157) EACH
B-60-157	1
TOTAL 0020	1

STATION	TO	STATION	LOCATION	650.4500 CONSTRUCTION STAKING SUBGRADE LF	650.5000 CONSTRUCTION STAKING BASE LF	650.9911.01 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (01. 8887-03-76) EACH	650.9920 CONSTRUCTION STAKING SLOPE STAKES LF
7+67	-	12+33	CTH A	466	466	1	466
			TOTAL 0010	466	466	1	466

STATION	LOCATION	690.0150 SAWING ASPHALT LF
7+67	BEGIN PROJECT	24
12+33	END PROJECT	24
	TOTAL 0010	48

999.2000.S.01

LOCATION	INSTALLING AND MAINTAINING BIRD DETERRENT SYSTEM (STATION) (01. STA 10+00) EACH
B-60-14	1
TOTAL 0010	1

CONVENTIONAL SYMBOLS

SECTION LINE
QUARTER LINE
SIXTEENTH LINE
NEW REFERENCE LINE
NEW R/W LINE
EXISTING R/W OR HT LINE
PROPERTY LINE
LOT, TIE & OTHER MINOR LINES
SLOPE INTERCEPT
CORPORATE LIMITS
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)
TEMPORARY LIMITED EASEMENT AREA
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)
TRANSMISSION STRUCTURES
BUILDING
TO BE REMOVED
BRIDGE
CULVERT

SECTION CORNER SYMBOL
SECTION CORNER MONUMENT
R/W MONUMENT (TO BE SET)
NON-MONUMENTED R/W POINT
FOUND IRON PIN (5/8 INCH UNLESS NOTED)
GEODETIC SURVEY MONUMENT
SIXTEENTH CORNER MONUMENT
SIGN
OFF-PREMISE SIGN

	COMPENSABLE	NON-COMPENSABLE
ELECTRIC POLE		
TELEPHONE POLE		
PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)		

ACCESS RESTRICTED BY ACQUISITION
NO ACCESS (BY STATUTORY AUTHORITY)
ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)
NO ACCESS (NEW HIGHWAY)
PARCEL NUMBER (25)
UTILITY NUMBER (40)
PARALLEL OFFSETS

CONVENTIONAL ABBREVIATIONS

ACCESS RIGHTS	AR	POINT OF COMPOUND CURVE	PCC
ACRES	AC	POINT OF INTERSECTION	PI
AHEAD	AH	PROPERTY LINE	PL
ALUMINUM	ALUM	RECORDED AS	(100')
AND OTHERS	ET AL	REEL / IMAGE	R/I
BACK	BK	REFERENCE LINE	R/L
BLOCK	BLK	REMAINING	REM
CENTERLINE	C/L	RESTRICTIVE DEVELOPMENT EASEMENT	RDE
CERTIFIED SURVEY MAP	CSM	RIGHT	RT
CONCRETE	CONC	RIGHT OF WAY	R/W
COUNTY	CO	SECTION	SEC
COUNTY TRUNK HIGHWAY	CTH	SEPTIC VENT	SEPV
DISTANCE	DIST	SQUARE FEET	SF
CORNER	COR	STATE TRUNK HIGHWAY	STH
DOCUMENT NUMBER	DOC	STATION	STA
EASEMENT	EASE	TELEPHONE PEDESTAL	TP
EXISTING	EX	TEMPORARY LIMITED EASEMENT	TLE
GAS VALVE	GV	TRANSPORTATION PROJECT PLAT	TPP
GRID NORTH	GN	UNITED STATES HIGHWAY	USH
HIGHWAY EASEMENT	HE	VOLUME	V
IDENTIFICATION	ID		
LAND CONTRACT	LC		
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY	NGS		
NUMBER	NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), TAYLOR COUNTY, NAD83 (2011)IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES. GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY ¾" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD

PROPERTY LINES SHOWN ON THIS PLAT FOR PROPERTIES BEING IMPACTED ARE DRAWN FROM DATA DERIVED FROM FILED/RECORDED MAPS AND DOCUMENTS OF PUBLIC RECORD. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT OF WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, CENTERLINE OF EXISTING PAVEMENTS AND/OR EXISTING OCCUPATIONAL LINES.

INFORMATION FOR THE BASIS OF EXISTING HIGHWAY RIGHT-OF-WAY POINTS OF REFERENCE AND ACCESS CONTROL ARE LISTED ON THE DETAIL PAGES.

FOR CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT TAYLOR COUNTY

CONVENTIONAL UTILITY SYMBOLS

WATER
GAS
TELEPHONE
OVERHEAD TRANSMISSION LINES
ELECTRIC
CABLE TELEVISION
FIBER OPTIC
SANITARY SEWER
STORM SEWER
ELECTRIC TOWER

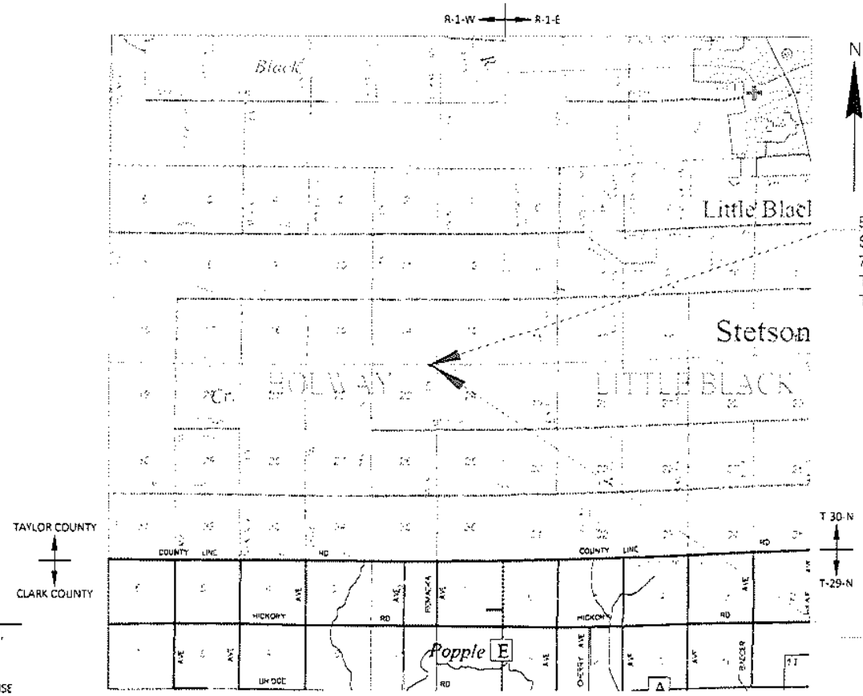
THIS PLAT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSES ONLY. DEEDS MUST BE CHECKED TO DETERMINE PROPERTY BOUNDARIES AND ACCESS RIGHTS.

R/W PROJECT NUMBER	SHEET NUMBER	TOTAL SHEETS
8887-03-06		
FEDERAL PROJECT NUMBER N/A	4.01	2

PLAT OF RIGHT OF WAY REQUIRED FOR

LUBLIN - STETSONVILLE
PINE CREEK BRIDGE B-60-0157

CTH A	TAYLOR COUNTY
CONSTRUCTION PROJECT NUMBER	



BEGIN RELOCATION ORDER
STA 45+21.79
751.47' WEST AND 71.04' SOUTH OF
THE NORTHEAST CORNER OF SEC 25
T-30-N R-1-W

END RELOCATION ORDER
STA 52+00.00
104.73' WEST AND 275.60' SOUTH OF
THE NORTHEAST CORNER OF SEC 23
T-30-N R-1-W

LAYOUT
SCALE 0 1.0 MI.

TOTAL NET LENGTH OF CENTERLINE = 0.128 MI.

CORRE ENGINEERING

WISCONSIN
SCOTT J. KOFFARNUS S-2782 MADISON WI
LAND SURVEYOR

I, SCOTT J. KOFFARNUS, PROFESSIONAL LAND SURVEYOR, S-2782, HEREBY CERTIFY THAT I HAVE SURVEYED THE LAND DESCRIBED HEREON AND THAT THE MAP HEREON IS A CORRECT REPRESENTATION OF THAT SURVEY TO THE BEST OF MY KNOWLEDGE AND BELIEF.

DATE: 02/08/2024 [Signature]

REVISION DATE

COUNTY OF TAYLOR

APPROVED FOR THE COUNTY
DATE 2/20/24 [Signature]

FILL NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\600_CADD\601_C3D_2022\88870306\RW\DWG\040101-RP.DWG
PLOT DATE : 2/8/2024 4:52 PM
PLOT BY : BOBBY JONES
PLOT NAME :

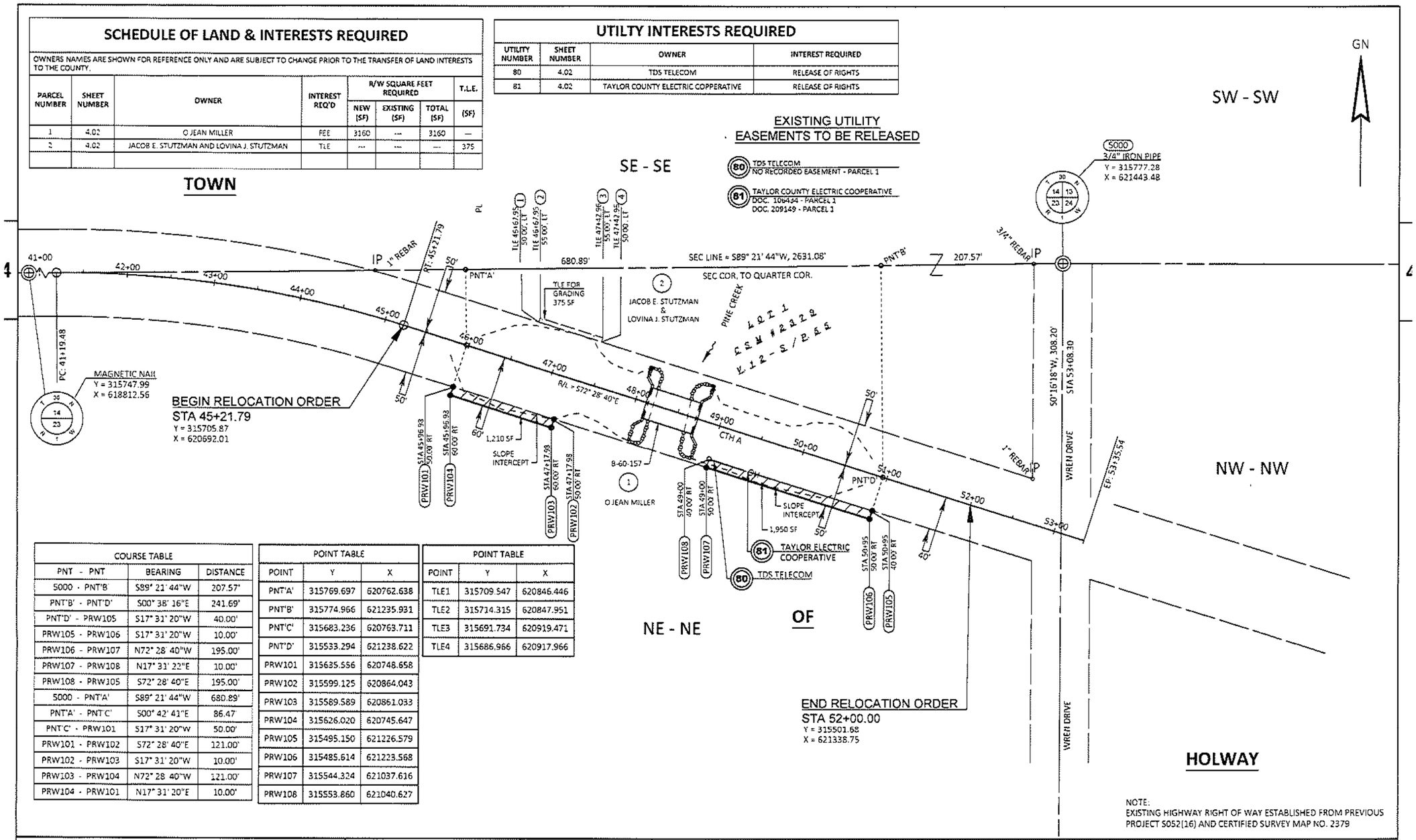
SCHEDULE OF LAND & INTERESTS REQUIRED

OWNERS NAMES ARE SHOWN FOR REFERENCE ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE COUNTY.

PARCEL NUMBER	SHEET NUMBER	OWNER	INTEREST REQ'D	R/W SQUARE FEET REQUIRED			T.L.E.
				NEW (SF)	EXISTING (SF)	TOTAL (SF)	(SF)
1	4.02	O JEAN MILLER	FEE	3160	---	3160	—
2	4.02	JACOB E. STUTZMAN AND LOVINA J. STUTZMAN	TLE	---	---	---	375

UTILTY INTERESTS REQUIRED

UTILITY NUMBER	SHEET NUMBER	OWNER	INTEREST REQUIRED
80	4.02	TDS TELECOM	RELEASE OF RIGHTS
81	4.02	TAYLOR COUNTY ELECTRIC COPPERATIVE	RELEASE OF RIGHTS



COURSE TABLE		
PNT - PNT	BEARING	DISTANCE
5000 - PNT'B'	S89° 21' 44"W	207.57'
PNT'B' - PNT'D'	S00° 38' 16"E	241.69'
PNT'D' - PRW105	S17° 31' 20"W	40.00'
PRW105 - PRW106	S17° 31' 20"W	10.00'
PRW106 - PRW107	N72° 28' 40"W	195.00'
PRW107 - PRW108	N17° 31' 22"E	10.00'
PRW108 - PRW105	S72° 28' 40"E	195.00'
5000 - PNT'A'	S89° 21' 44"W	680.89'
PNT'A' - PNT'C'	S00° 42' 41"E	86.47'
PNT'C' - PRW101	S17° 31' 20"W	50.00'
PRW101 - PRW102	S72° 28' 40"E	121.00'
PRW102 - PRW103	S17° 31' 20"W	10.00'
PRW103 - PRW104	N72° 28' 40"W	121.00'
PRW104 - PRW101	N17° 31' 20"E	10.00'

POINT TABLE		
POINT	Y	X
PNT'A'	315769.697	620762.638
PNT'B'	315774.966	621235.931
PNT'C'	315683.236	620763.711
PNT'D'	315533.294	621238.622
PRW101	315635.556	620748.658
PRW102	315599.125	620864.043
PRW103	315589.589	620861.033
PRW104	315626.020	620745.647
PRW105	315495.150	621226.579
PRW106	315485.614	621223.568
PRW107	315544.324	621037.616
PRW108	315553.860	621040.627

POINT TABLE		
POINT	Y	X
TLE1	315709.547	620846.446
TLE2	315714.315	620847.951
TLE3	315691.734	620919.471
TLE4	315686.966	620917.966

NOTE:
EXISTING HIGHWAY RIGHT OF WAY ESTABLISHED FROM PREVIOUS PROJECT 5052(16) AND CERTIFIED SURVEY MAP NO. 2379

REVISION DATE	DATE	SCALE: FEET 0 50 100	HWY: CTH A
	GRID FACTOR		COUNTY: TAYLOR

STATE R/W PROJECT NUMBER: 8887-03-06	PLAT SHEET 4.02	
CONSTRUCTION PROJECT NUMBER: 8887-03-76	PS&E SHEET ____	E

FILE NAME : D40102-RP.DWG
LAYOUT NAME : D40102-TB
PLOT DATE: 2/8/2024 4:52 PM PLOT BY: BOBBY JONES PLOT NAME: PLOT SCALE:
WISDOT/CADDS SHEET 75

REMOVING GUARDRAIL STA 9+27 TO 9+80 LT
MGS EAT STA 8+66.60 TO 9+19.69 LT
MGS GUARDRAIL 3 STA 9+19+.68 TO 9+32.18 LT
MGS THRIE BEAM TRANSITION STA 9+32.18 TO 9+71.58 LT

JACOB STUTZMAN

TDS TELECOM

PROPOSED STRUCTURE B-60-157

PINE CREEK

REMOVING GUARDRAIL STA 10+19 TO 10+72 LT
MGS THRIE BEAM TRANSITION STA 10+28.42 TO 10+67.82 LT
MGS GUARDRAIL 3 STA 10+67.82 TO 11+30.32 LT
MGS EAT STA 11+30.32 TO 11+83.40 LT

N

WETLAND BOUNDARY

SLOPE INTERCEPT

SILT FENCE

EX R/W

TLE

+16.60, 12.0' LT

Y = 315699.229
X = 620712.108

END CONSTRUCTION
STA 12+33

7+00 | 8+00 | S72°28'49"E 600.00' | 9+00 | 10+00 | 11+00 | CTH A | 12+00 | 13+00

+83.40, 12.0' RT

Y = 315518.608
X = 621284.276

BEGIN CONSTRUCTION
STA 7+40

PROP R/W

SLOPE INTERCEPT

TAYLOR ELECTRIC COOPERATIVE

SILT FENCE

WETLAND BOUNDARY

EX R/W

TURBIDITY BARRIERS

STRUCTURE B-60-014
TO BE REMOVED

TDS TELECOM

PROP R/W

END PROJECT
STA 12+33
MATCH EXISTING
SAWCUT REQ'D
Y = 315538.778
X = 621220.384

BEGIN PROJECT
STA 7+67
MATCH EXISTING
SAWCUT REQ'D
Y = 315679.060
X = 620776.000

REMOVING GUARDRAIL STA 9+28 TO 9+80 RT
MGS EAT STA 8+16.60 TO 8+69.68 RT
MGS GUARDRAIL 3 STA 8+69.68 TO 9+32.18 RT
MGS THRIE BEAM TRANSITION STA 9+32.18 TO 9+71.58 RT

O JEAN MILLER

REMOVING GUARDRAIL STA 10+19 TO 10+72 RT
MGS THRIE BEAM TRANSITION STA 10+28.42 TO 10+67.82 RT
MGS GUARDRAIL 3 STA 10+67.82 TO 10+80.32 RT
MGS EAT STA 10+80.32 TO 11+33.40 RT

VCL = 256.00
K = 273.15

VPI STA 9+81.00
VPI EL 1353.98

LP STA 10+80.64
LP EL 1354.10

-0.83%

EXISTING PROFILE

PROPOSED PROFILE

0.10%

STA 7+67.00
EL 1355.77

VPC STA 8+53.00
VPC EL 1355.05

VPT STA 11+09.00
VPT EL 1354.12

STA 12+33.00
EL 1354.25

HIGH WATER 100
ELEV. 1346.12'

OBSERVED WATER
ELEV. 1340.2'
(7/25/2023)

Q2 WATER
ELEV. 1343.86'

EXISTING ELEVATION

PROPOSED ELEVATION

STA 10+00
REMOVE STRUCTURE B-60-14
SINGLE SPAN CONCRETE FLAT SLAB BRIDGE
39.5' O.A.L. X 34.0' CLEAR
0° SKEW

STA 10+00
STRUCTURE B-60-157 REQ'D
SINGLE SPAN PRESTRESSED CONCRETE GIRDER BRIDGE
61.5' O.A.L. X 34.0' CLEAR
0° SKEW

1355 | 1350 | 1345 | 1340

1356.07 | 1355.88 | 1355.49 | 1355.49 | 1355.11 | 1355.08 | 1354.63 | 1354.70 | 1354.14 | 1354.42 | 1338.32 | 1354.22 | 1353.94 | 1354.12 | 1354.16 | 1354.11 | 1354.30 | 1354.16 | 1354.27 | 1354.21 | 1354.23 | 1354.12

7+00 | 8+00 | 9+00 | 10+00 | 11+00 | 12+00 | 13+00

PROJECT NO: 8887-03-76	HWY: CTH A	COUNTY: TAYLOR	PLAN AND PROFILE: CTH A	SCALE, FEET 0 20 40	SHEET 12	E

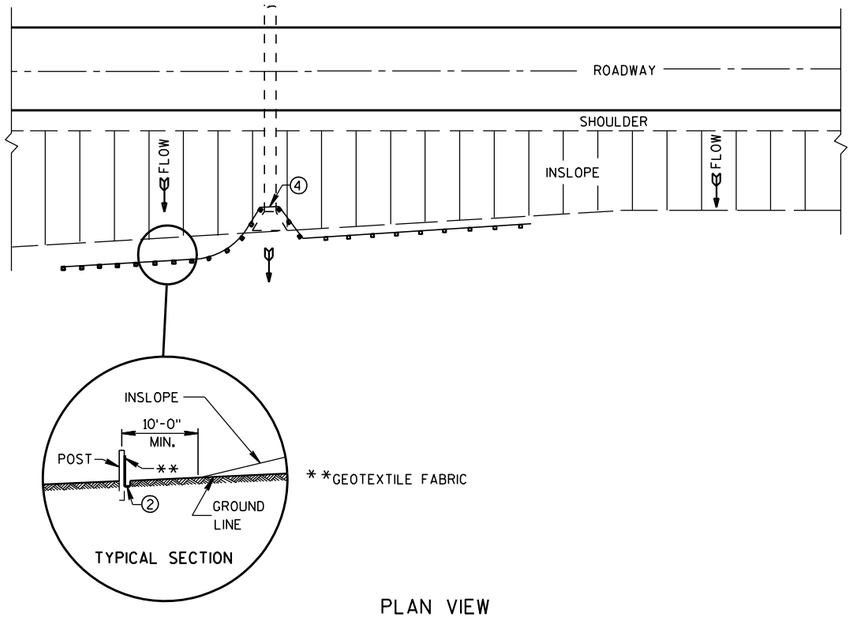
FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\501_C3D_2022\88870306\SHEETS\050101-PP.DWG
LAYOUT NAME - 01

PLOT DATE : 7/10/2024 11:45 AM PLOT BY : NICHOLAS WATHKE, PE PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 44

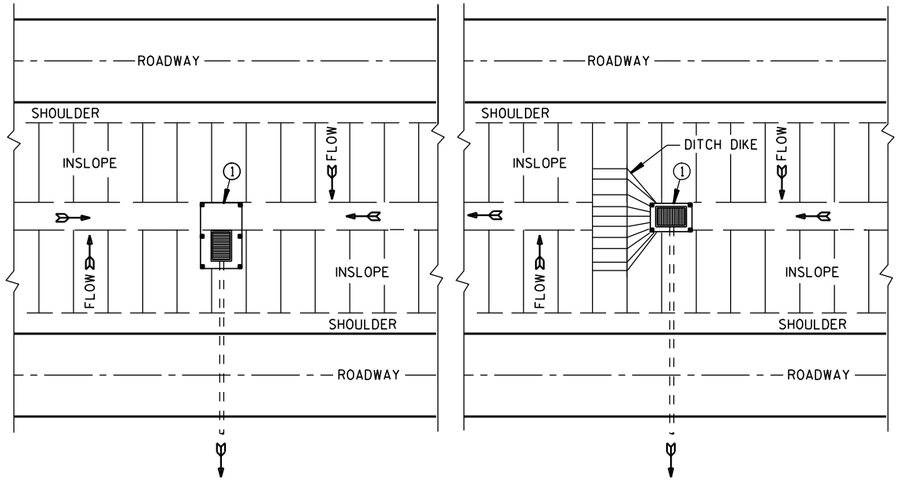
Standard Detail Drawing List

08E09-06	SILT FENCE
08E11-02	TURBIDITY BARRIER
12A03-10	NAME PLATE (STRUCTURES)
14B42-07A	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07B	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07C	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07D	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B44-04A	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-04B	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-04C	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B45-05A	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05B	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05C	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05D	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05E	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05F	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05G	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05H	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05I	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05J	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05K	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05L	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C06-12	SIGNING & MARKING FOR TWO LANE BRIDGES



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



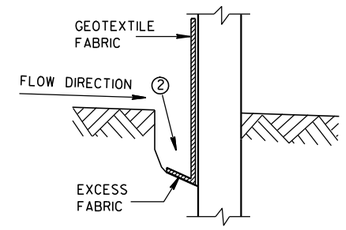
PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

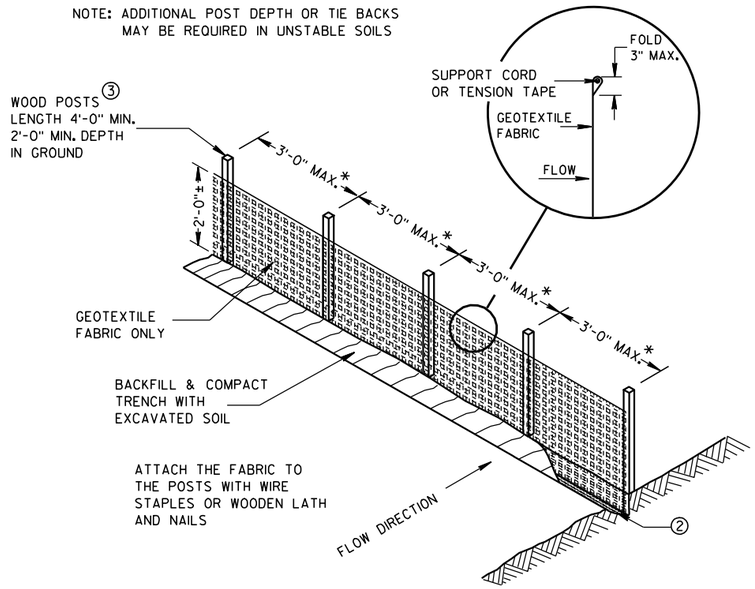
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

1. HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
2. FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
3. WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.
4. SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
5. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

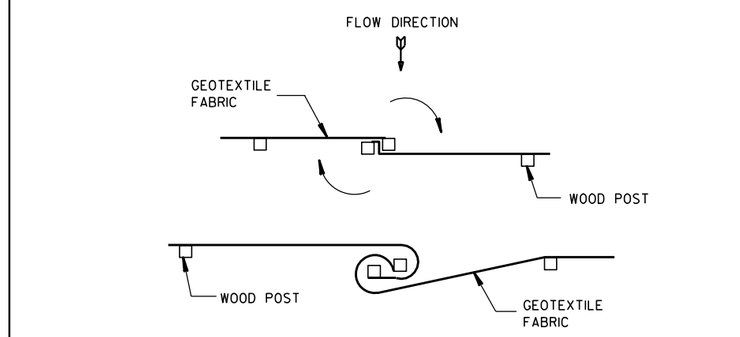


TRENCH DETAIL

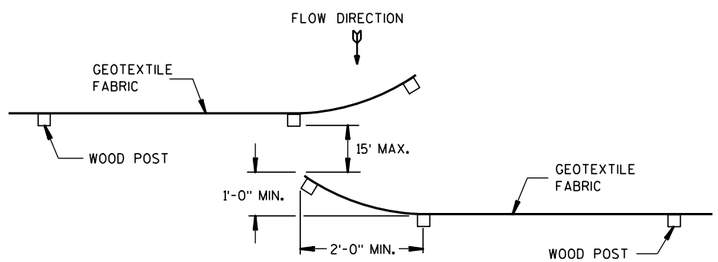


*NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.

SILT FENCE

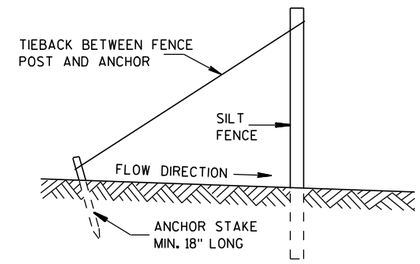


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE 5



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

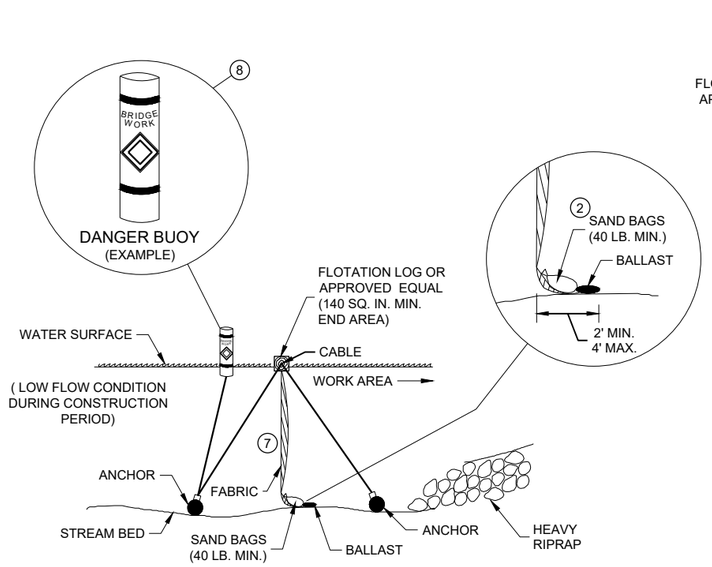
APPROVED
4-29-05 DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6

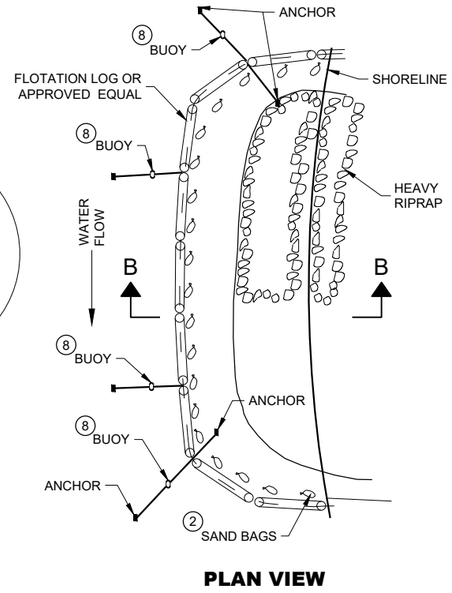
S.D.D. 8 E 9-6

6

S.D.D. 8 E 9-6

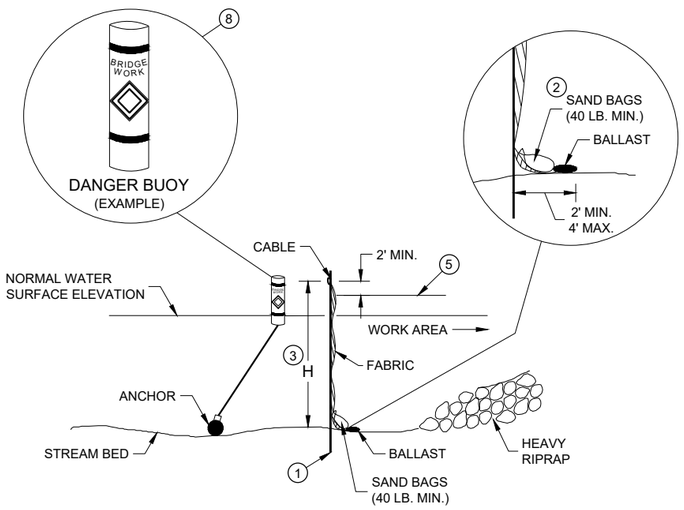


SECTION B - B

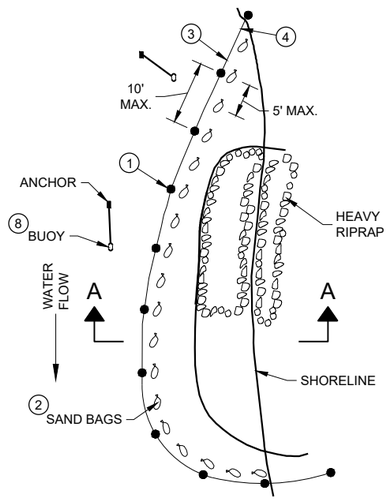


PLAN VIEW

TURBIDITY BARRIER - FLOAT ALTERNATIVE
CAUTION - SEE NOTE 6



SECTION A - A



PLAN VIEW

TURBIDITY BARRIER - STANDARD POST INSTALLATION

TURBIDITY BARRIER PLACEMENT DETAILS

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TURBIDITY BARRIER MAY BE REMOVED AT THE ENGINEERS DISCRETION, WHEN PERMANENT EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

(1) DRIVEN STEEL POSTS, PIPES, OR CHANNELS. LENGTH SHALL BE SUFFICIENT TO SECURELY SUPPORT BARRIER AT HIGH WATER ELEVATIONS.

(2) SAND BAGS TO BE USED AS ADDITIONAL BALLAST WHEN ORDERED BY THE ENGINEER TO MEET ADVERSE FIELD CONDITIONS. SPACE AS APPROPRIATE FOR SITE CONDITIONS.

(3) WHEN BARRIER HEIGHT "H" EXCEEDS 8 FEET, POST SPACING MAY NEED TO BE DECREASED.

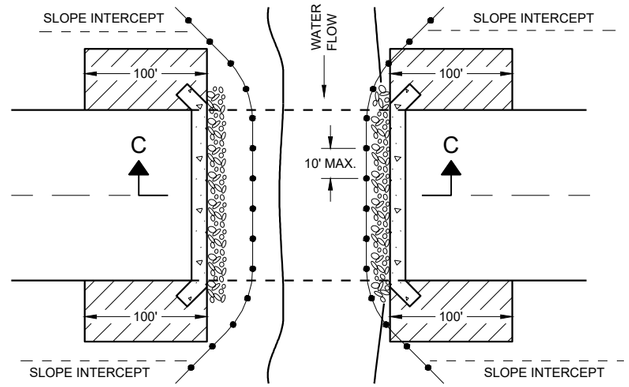
(4) IN WATERWAYS SUBJECT TO FLUCTUATING WATER ELEVATIONS, PROVISIONS SHOULD BE MADE TO ALLOW THE WATER TO EQUALIZE ON EACH SIDE OF THE BARRIER. THIS MAY BE ACCOMPLISHED BY LEAVING A PORTION OF THE BARRIER OPEN ON THE UPSTREAM END.

(5) ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION PERIOD. MINIMUM BARRIER HEIGHT SHALL BE 2' GREATER THAN EITHER THE Q2 ELEVATION OR THE ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION, WHICHEVER IS GREATER.

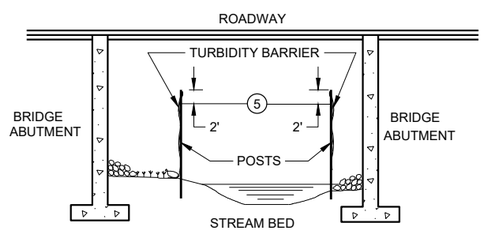
(6) FLOAT ALTERNATIVE WILL ONLY BE ALLOWED WITH WRITTEN APPROVAL OF THE ENGINEER, AND IS MEANT FOR LOCATIONS WHERE BEDROCK PREVENTS THE INSTALLATION OF POSTS.

(7) ALLOW SUFFICIENT SLACK VERTICALLY AND HORIZONTALLY SO THAT SEDIMENT BUILD UP WILL NOT SEPARATE OR LOWER THE TURBIDITY BARRIER.

(8) USE AS DIRECTED BY COAST GUARD OR DNR PERMIT WHEN WORKING IN NAVIGABLE WATERWAYS.



PLAN VIEW



SECTION C - C

TURBIDITY BARRIER DETAIL SHOWING TYPICAL PLACEMENT AT STRUCTURES

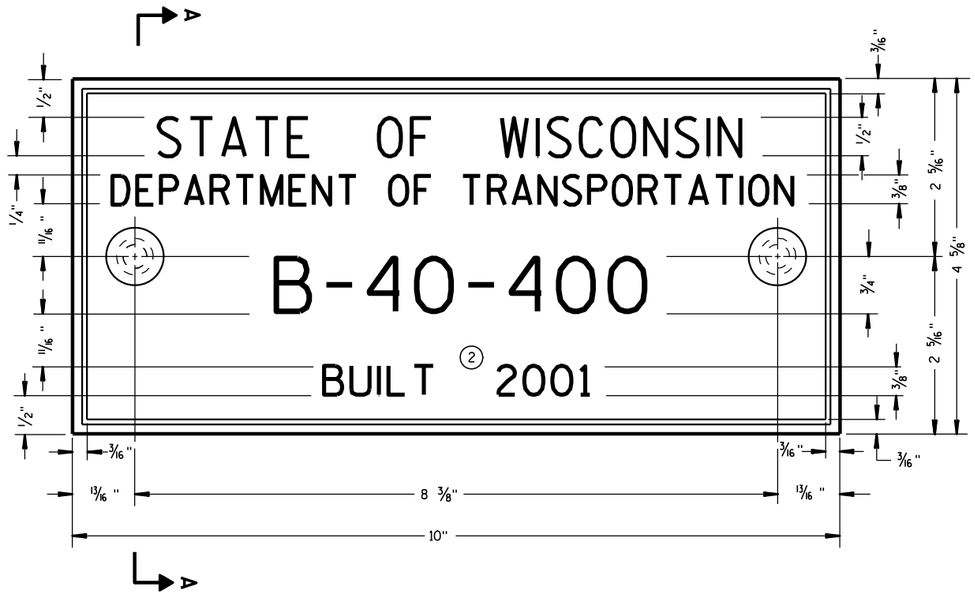
TURBIDITY BARRIER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/4/02
DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA

6
SDD 08E11 - 02



TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)

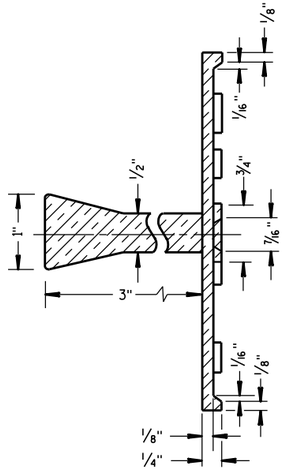
GENERAL NOTES

NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

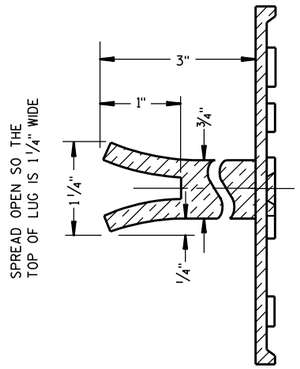
THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

(1) EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.

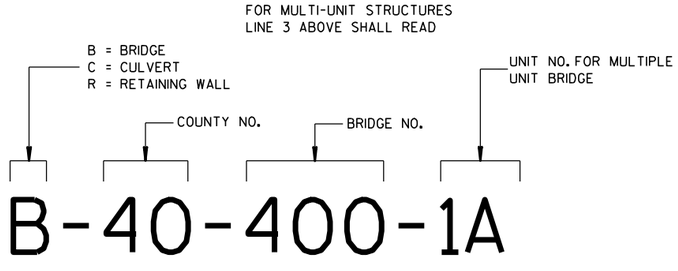
(2) REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.



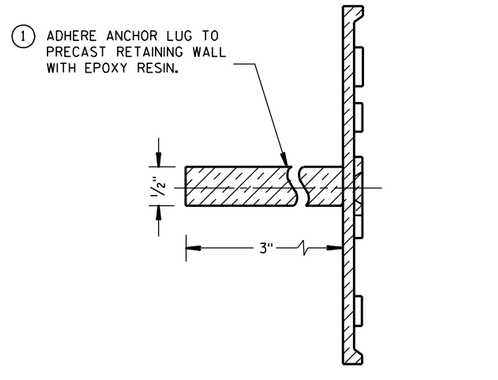
SECTION A-A



ALTERNATE LUG



NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES



ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

NAME PLATE (STRUCTURES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

3/26/10 DATE

/S/ Scot Beck -- CHIEF STRUCTURAL DEVELOPMENT ENGINEER

FHWA

6

S.D.D. 12 A 3-10

(1) WOOD OR STEEL POSTS (w6X9 OR w6X8.5) MAY BE USED. DO NOT INTERMIX WOOD AND STEEL POSTS. INSTALL STEEL POSTS WITH HOLES ON APPROACHING TRAFFIC SIDE.

(2) USE WOOD OR APPROVED PLASTIC BLOCKOUTS. WOOD BLOCKOUTS MAY BE CONSTRUCTED OUT OF TWO OR MORE WOOD BLOCKOUTS. SEE ALTERNATE WOOD BLOCKOUT DETAIL. DIMENSIONS OF APPROVED PLASTIC BLOCKOUTS MAY VARY.

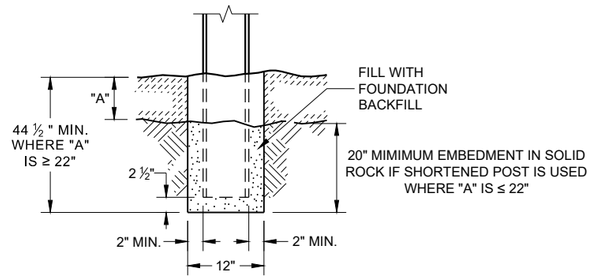
(3) IF ROCK IS ENCOUNTERED DURING EXCAVATION, PROVIDE A HOLE 12 INCHES IN DIAMETER EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE APPROXIMATELY 2 1/2" INCHES OF GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE. CUT THE POSTS THE TO LENGTH AMD INSTALL. BACKFILL WITH EXCAVATED MATERIAL AND COMPACT. BACKFILL IS TO BE FREE OF LARGE ROCKS.

(4) WHEN THE DISTANCE FROM BACK OF POST TO SHOULDER HINGE POINT IS LESS THAN 2 FEET INSTALL LONGER POST AT HALF POST SPACING (K).

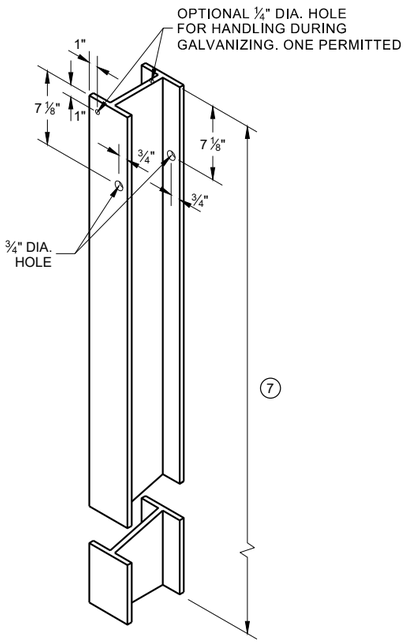
(5) FOR NEW MGS INSTALLATION TOP OF W-BEAM RAIL TOLERANCE IS ±1". FOR EXISTING MGS INSTALLATION TOP OF W-BEAM IS BETWEEN 27 ¾" TO 32".

(6) WHEN USING STEEL POST AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

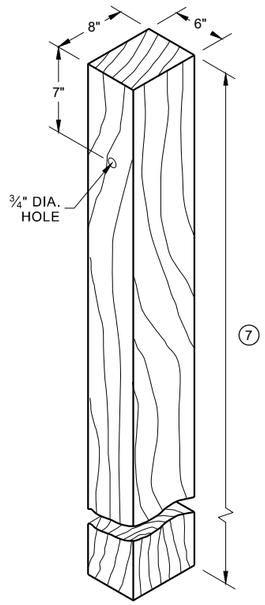
(7) TOTAL POST LENGTH FOR TYPE K IS 7' - 0".
TOTAL POST LENGTH FOR OTHER MGS TYPES IS 6' - 0".



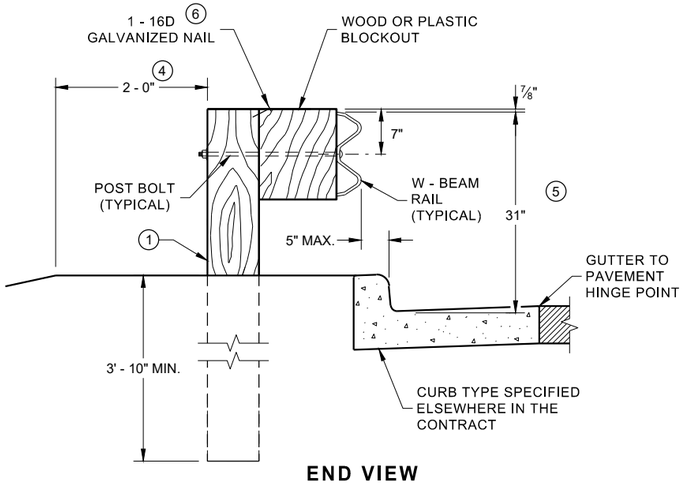
END VIEW
SETTING STEEL OR WOOD POST IN ROCK (3)



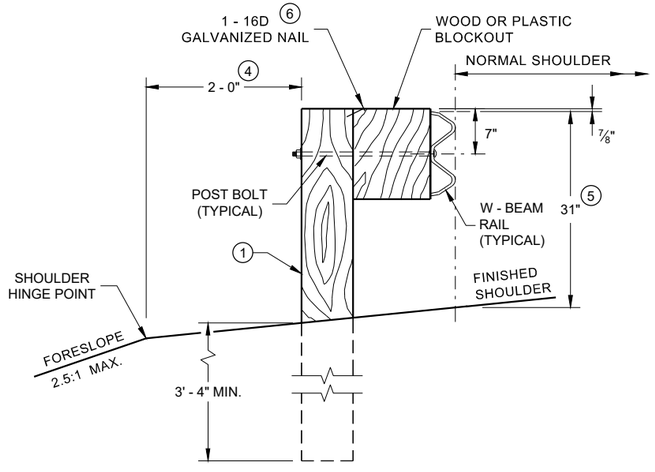
STEEL POST & HOLE PUNCHING DETAIL (W 6 X 9) (1)



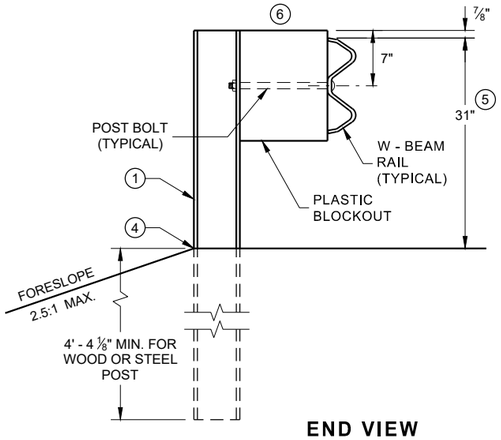
WOOD POST (6" X 8") NOMINAL (1)



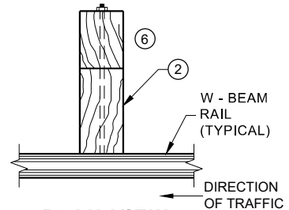
END VIEW
LOCATED ALONG A CURBED ROADWAY



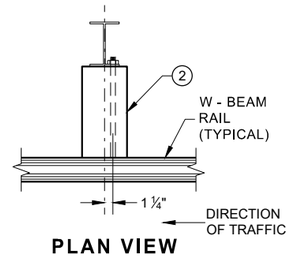
END VIEW
LOCATED ALONG A ROADWAY SHOULDER STANDARD INSTALLATION



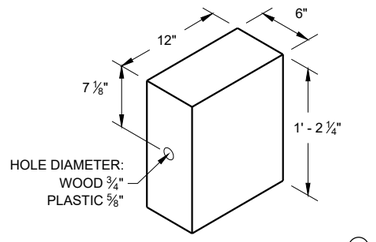
END VIEW
MGS LONGER POST AT HALFPOST SPACING W BEAM (K)



PLAN VIEW WOOD POST, BLOCKOUT & BEAM



PLAN VIEW STEEL POST, PLASTIC BLOCKOUT & BEAM

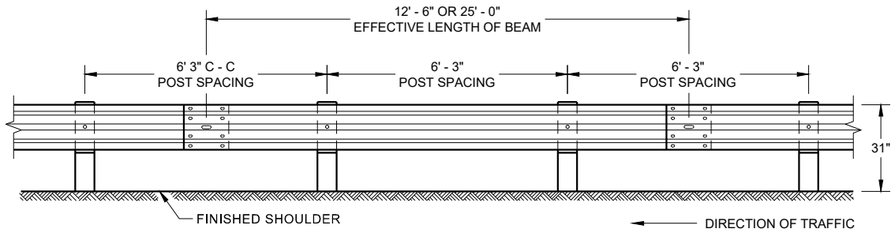


WOOD OR PLASTIC BLOCKOUT (2)

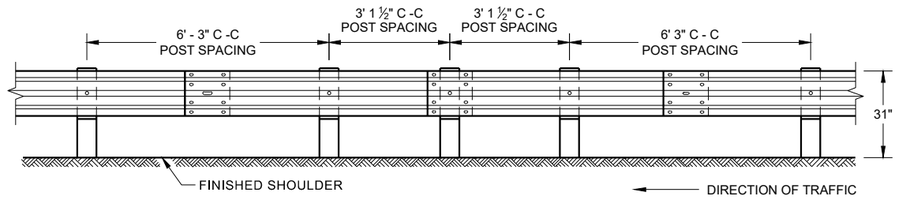
MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

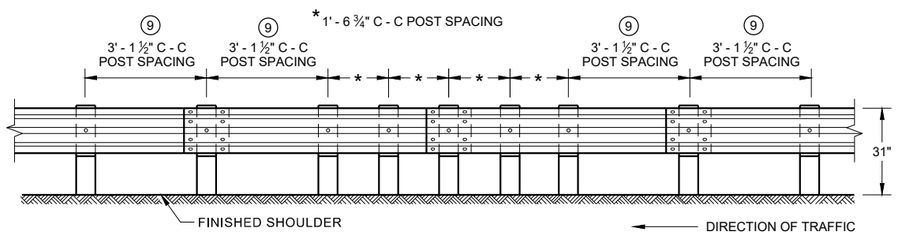
SDD 14B42 - 07a



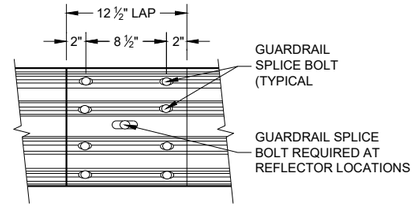
FRONT VIEW
POST SPACING STANDARD INSTALLATION



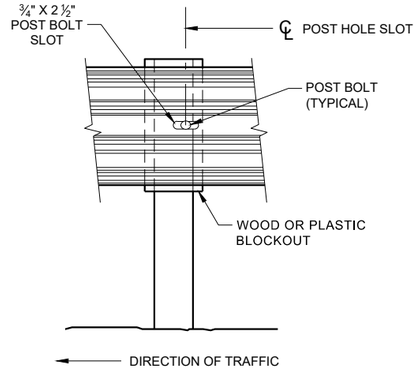
FRONT VIEW
HALF POST SPACING (HS) AND HALF POST SPACING WITH LONGER POSTS (K)



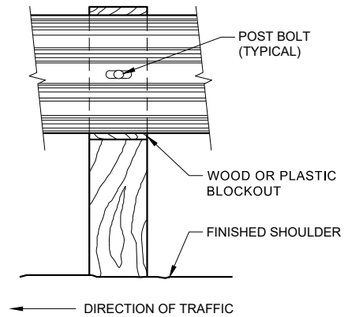
FRONT VIEW
QUARTER POST SPACING (QS)



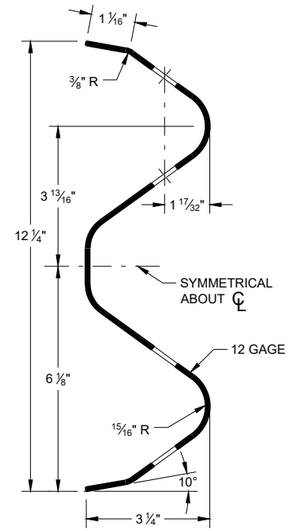
FRONT VIEW
MID-SPAN BEAM SPLICE



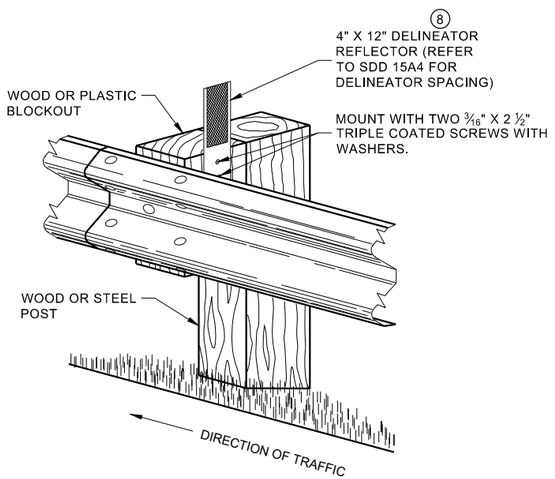
FRONT VIEW AT STEEL POST



FRONT VIEW AT WOOD POST



SECTION THRU W-BEAM RAIL



ONE SIDED REFLECTOR DETAIL AND TYPICAL INSTALLATION

GENERAL NOTES

⑧ DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.

⑨ 25 FEET OF HALF POST SPACING IS REQUIRED ON APPROACH AND DEPARTURE ENDS OF QUARTER POST SPACING.

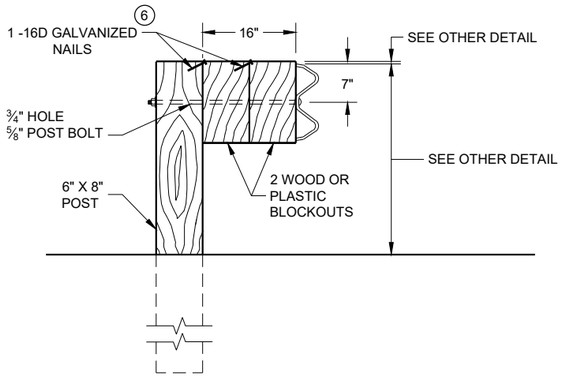
POST BOLTS ARE A ⅝" DIAMETER ASTM A307 GUARDRAIL BOLT. A POST BOLT REQUIRES ⅝" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT AND ⅝" DIAMETER F844 FLAT WASHER. POST BOLTS MAY BE LONGER IF MULTIPLE BLOCKOUTS ARE BEING USED.

GUARD RAIL SPLICE BOLTS ARE A ⅝" DIAMETER ASTM A307 GUARDRAIL HEAD BOLT. A GUARDRAIL SPLICE BOLT REQUIRES ⅝" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT.

MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

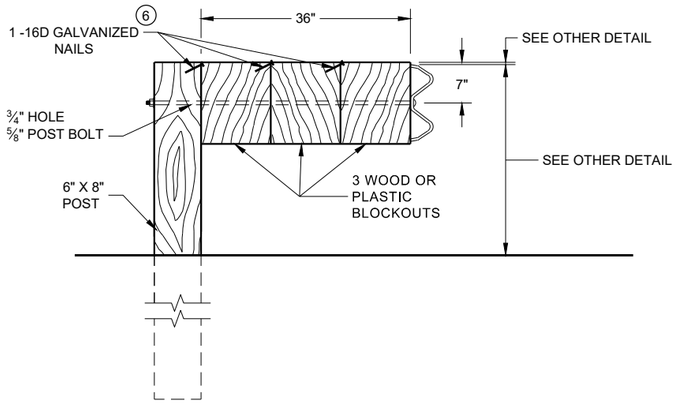
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6
SDD 14B42 - 07b



DETAIL FOR 16" BLOCKOUT DEPTH

IT IS ACCEPTABLE TO USE BLOCKOUTS UP TO 16" DEEP TO INCREASE THE POST OFFSET TO AVOID UNDERGROUND OBSTACLES. THERE IS NO LIMIT TO THE NUMBER OF POSTS THAT CAN HAVE ADDITIONAL BLOCKOUTS UP TO 16" DEEP.



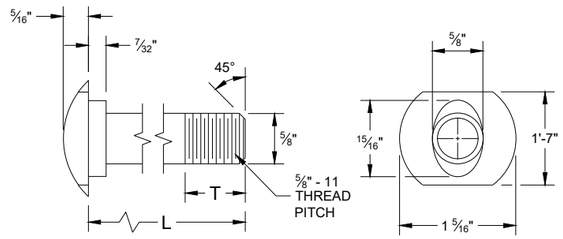
DETAIL FOR 36" BLOCKOUT DEPTH

NOTES: UNDER SPECIAL CIRCUMSTANCES, SUCH AS AVOIDING OBSTACLES THAT ARE NOT RELOCATED, IT IS ACCEPTABLE TO INSTALL ADDITIONAL BLOCKOUTS TO OBTAIN UP TO 36" DEPTH FOR ONE OR TWO POSTS IN A SECTION OF GUARDRAIL.

DO NOT USE 16" OR 36" BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.

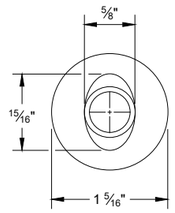
NOTE:

1. ALL FILLETS SHALL HAVE A MINIMUM RADIUS OF 3/16".
2. IF THE BOLT EXTENDS MORE THAN ¼" FROM THE NUT THE BOLT SHOULD BE TRIMMED BACK.

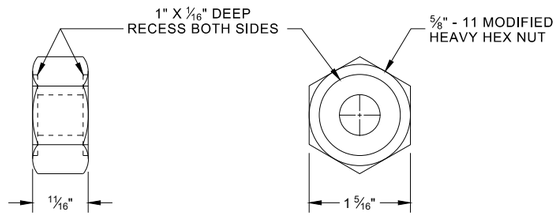


POST BOLT TABLE

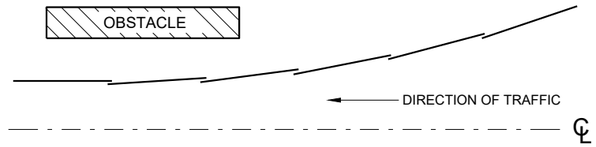
L	T (MIN.)
1 ¼"	1 ⅛"
2"	1 ¾"
10"	4"
14"	4 1/16"
18"	4"
21"	4 1/16"
25"	4"



ALTERNATE BOLT HEAD

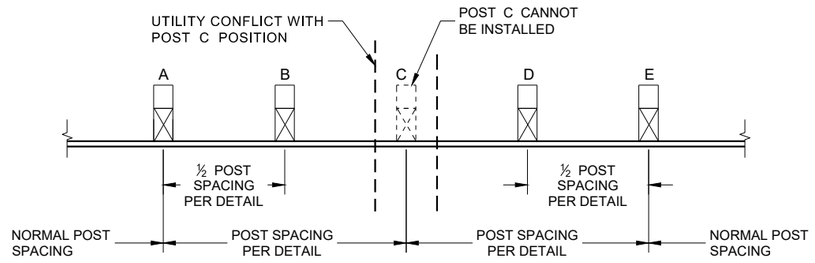


POST BOLT, SPLICE BOLT AND RECESS NUT

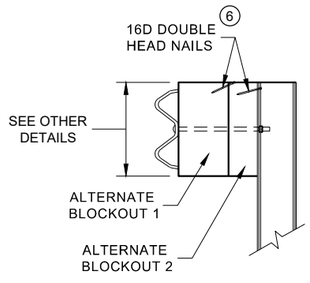


PLAN VIEW

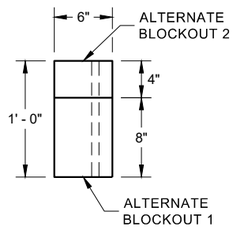
BEAM LAPPING DETAIL



POST DRIVING FOR CONTINUOUS UNDERGROUND OBSTRUCTION



SIDE VIEW

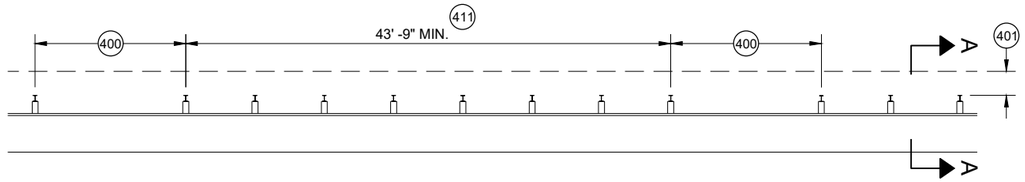


PLAN VIEW

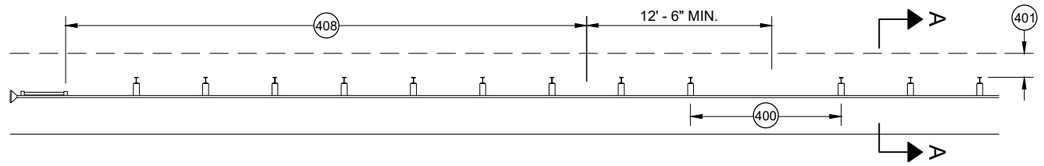
ALTERNATE WOOD BLOCKOUT DETAIL

(6) WHEN USING STEEL POST AD WOOD BLOCKOUTS, INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

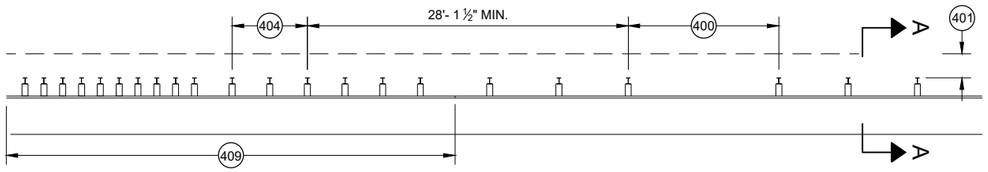
MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL



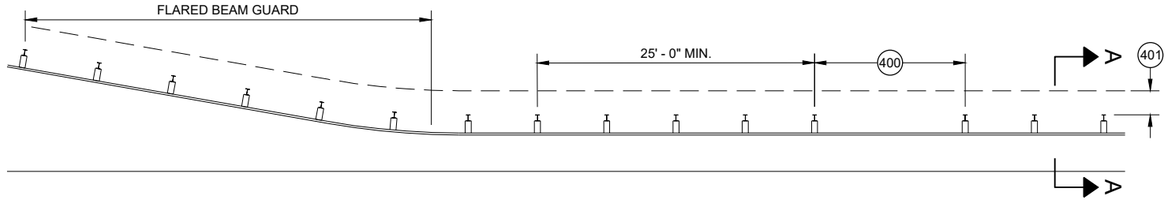
MISSING POST IN MGS GUARDRAIL



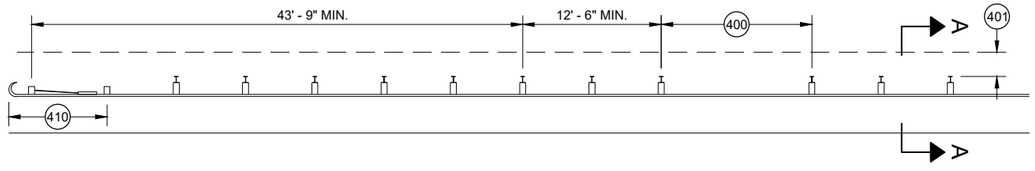
MISSING POST IN MGS GUARDRAIL NEAR EAT



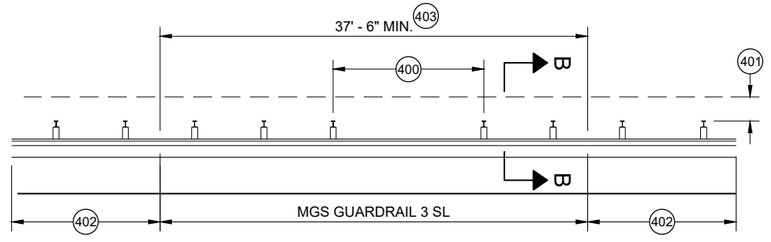
MISSING POST IN MGS GUARDRAIL NEAR AN APPROACH TRANSITION



MISSING POST IN MGS GUARDRAIL NEAR FLARED BEAM GUARD

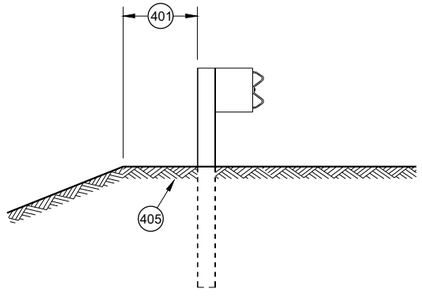


MISSING POST IN MGS GUARDRAIL NEAR A TYPE 2 END TERMINAL

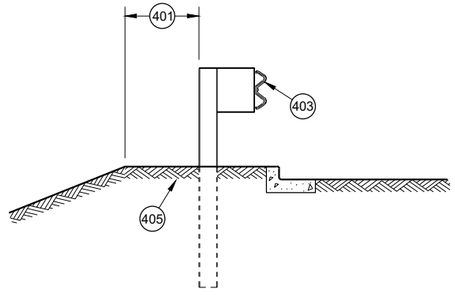


MISSING POST IN SHORT SPAN MGS GUARDRAIL NEAR CURB (SL) 407

(400) MAX SPAN 12' - 6"
(401) 2' MIN.
(402) MGS GUARDRAIL 3
(403) NESTING BEAM GUARD
(404) ASYMMETRIC TRANSITION
(405) SOIL WELL DRAINED AND COMPACTED
(406) SEE OTHER DRAWINGS IN THIS SDD
(407) SEE OTHER DRAWINGS FOR MIN. SPACING BETWEEN SPANS
(408) SEE SDD 14B44
(409) SEE SDD 14B45
(410) SEE SDD 14B47
(411) MINIMUM DISTANCE BETWEEN MISSING POST SPANS.



SECTION A - A 406



SECTION B - B 406

MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2021
DATE

/S/ Rodney Taylor
ROADWAY STANDARDS DEVE
UNIT SUPERVISOR

FHWA

6

SDD 14B42 - 07d

GENERAL NOTES

(A) THE SLOPE IN THE AREA BOUNDED BY THE GRADELINE, THE HINGE POINT LINE (HPL) AND THE CLEAR ZONE LIMITS (CZL) SHALL BE 4:1 OR FLATTER.

(B) AFTER FINAL ASSEMBLY, RECHECK CABLE TO BE SURE IT IS TAUT AND HAS NOT RELAXED

(C) DIFFERENT MANUFACTURERS REQUIRE DIFFERENT PERFORATED W - BEAM RAIL END PANELS. SEE MANUFACTURER'S INFORMATION.

(D) ATTACH ALUMINUM SHEET TO E.A.T. HEAD USING 4 STAINLESS STEEL SELF - TAPPING SCREWS. ONE SCREW PER CORNER.

(E) HARDWARE MAY VARY BETWEEN MANUFACTURER. SEE MANUFACTURER'S DRAWING FOR INFORMATION.

DIMENSIONS MAY VARY, MANUFACTURER'S INFORMATION.

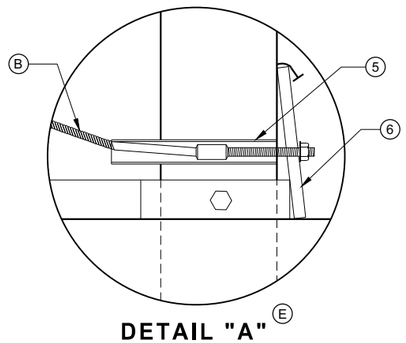
SEE SDD 14B42 FOR MORE INFORMATION.

* DO NOT ATTACH BLOCKOUTS TO POST 1 AND 2.

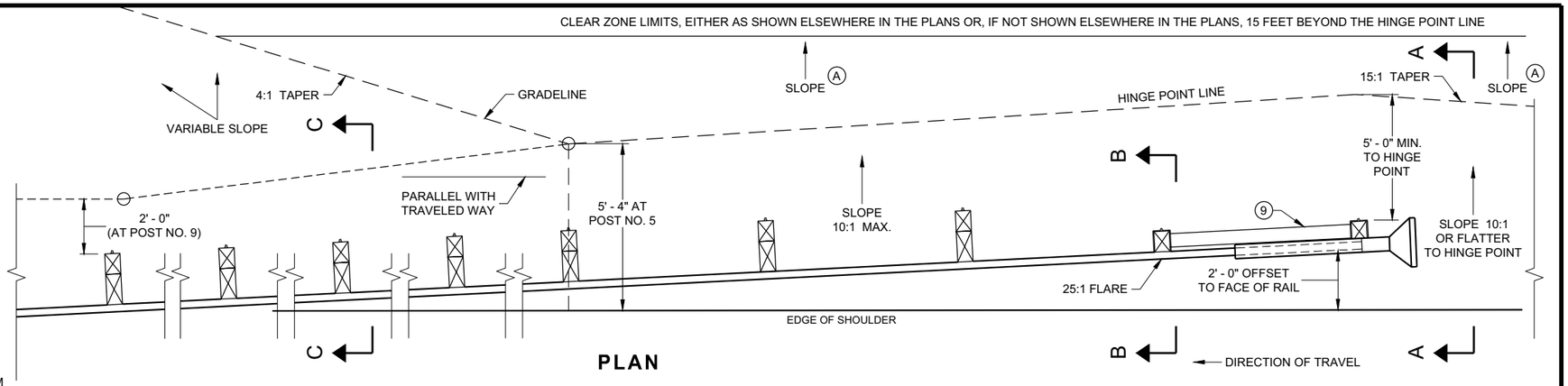
DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.

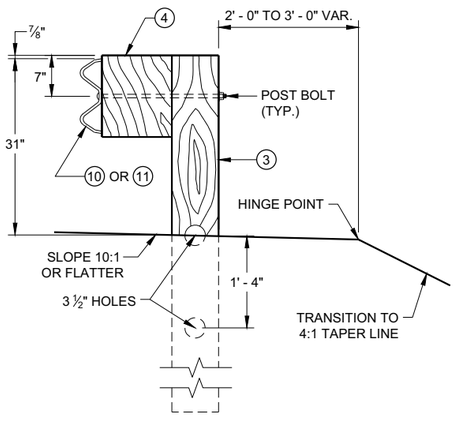
SEE MANUFACTURER'S DRAWING FOR SPLICE LOCATION, HARDWARE DIMENSIONS AND INSTALLATION INSTRUCTIONS.

THE CENTER OF THE UPPER 3 ½" DIAMETER HOLE ON POST NUMBER 3 THROUGH POST 9 IS TO BE FLUSH WITH THE GROUND LINE UP TO A MAXIMUM OF 2" ABOVE GROUND LINE. WOOD BLOCKS ON POSTS NUMBERED 3 THROUGH 9 MAY BE ADJUSTED UP TO 3" ABOVE THE TOP OF POST.

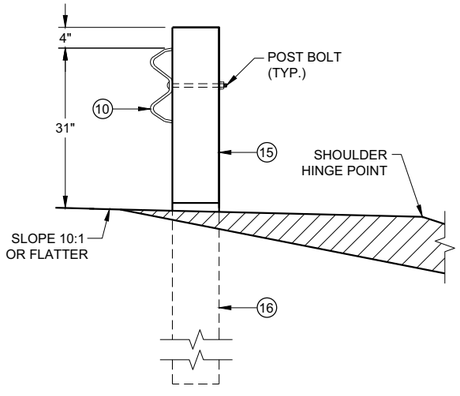


DETAIL "A" (E)

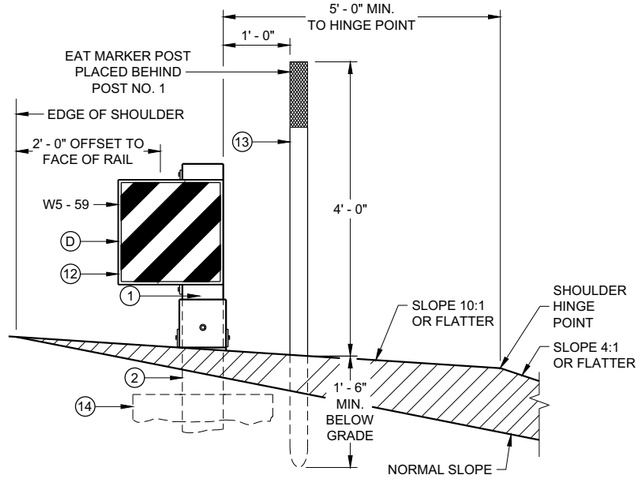




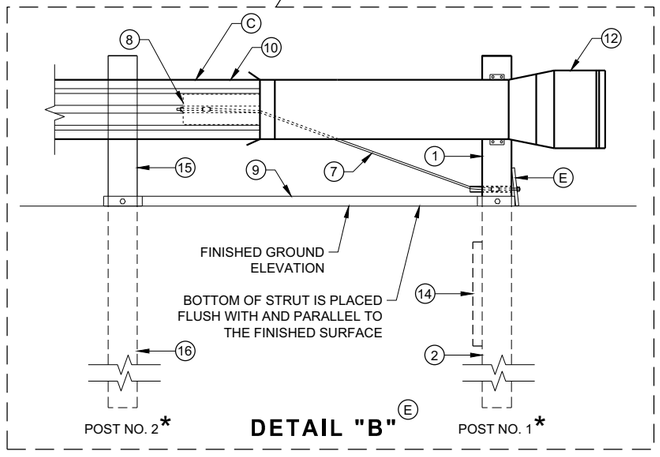
SECTION C - C
TYPICAL AT POST NOS. 3 - 9



SECTION B - B
TYPICAL AT POST NO. 2*



SECTION A - A
TYPICAL AT POST NO. 1*



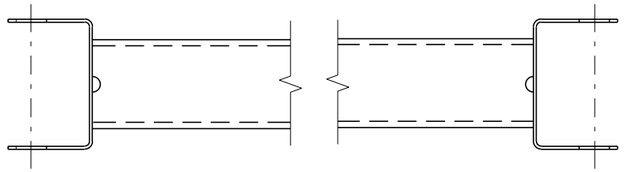
DETAIL "B" (E)

MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

SDD 14B44 - 04a

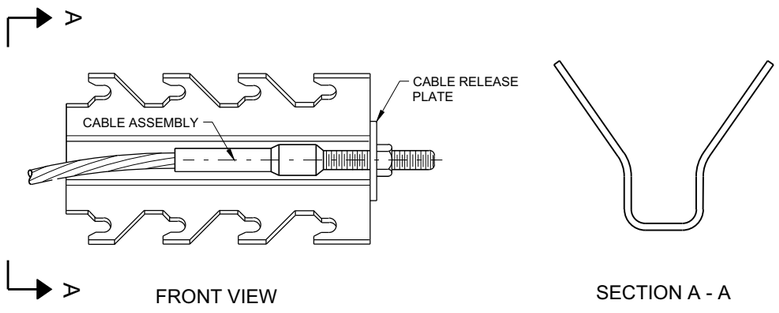
SDD 14B44 - 04a



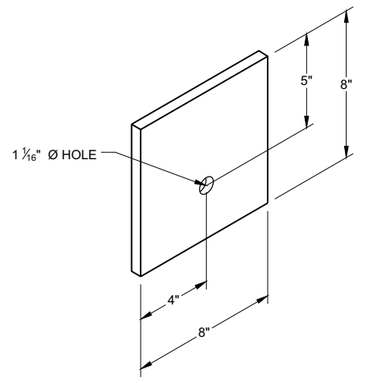
GENERIC GROUND STRUT ⑨ Ⓔ

BILL OF MATERIALS

PART NO.	DESCRIPTION MATERIALS PROVIDED BY MGS EAT MANUFACTURER. SEE MANUGACTURER'S DETAILS FOR MORE INFORMATION.
①	UPPER POST NO. 1 6" X 6" TUBE
②	LOWER POST NO. 1
③	WOOD CRT
④	WOOD BLOCKOUT
⑤	PIPE SLEEVE
⑥	BEARING PLATE
⑦	BCT CABLE ASSEMBLY
⑧	ANCHOR CABLE BOX
⑨	GROUND STRUT
⑩	PERFORATED W-BEAM RAIL END PANEL, 12'-6" LONG.
⑪	STANDARD W-BEAM RAIL. MULTIPLE SECTIONS REQUIRED. SECTIONS VARY IN LENGTH.
⑫	IMPACT HEAD
⑬	EAT MARKER POST - YELLOW (SEE APPROVED PRODUCTS LIST)
⑭	SOIL PLATE
⑮	UPPER POST NO. 2
⑯	LOWER POST NO. 2



GENERIC ANCHOR CABLE BOX ⑧ Ⓔ



BEARING PLATE ⑥ Ⓔ

6

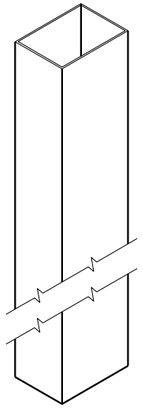
6

SDD 14B44 - 04b

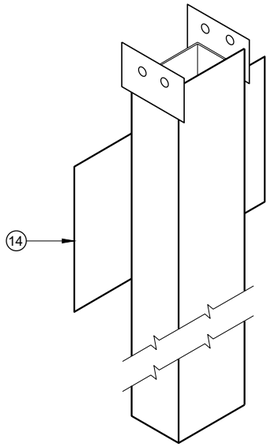
SDD 14B44 - 04b

MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL
(MGS)

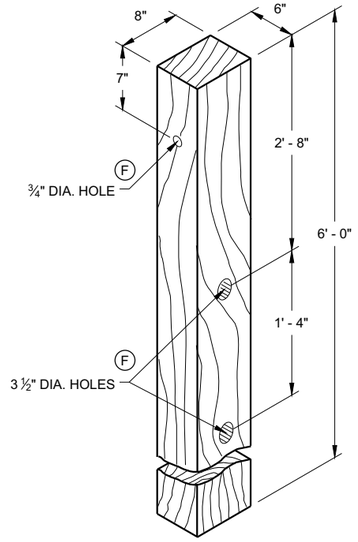
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



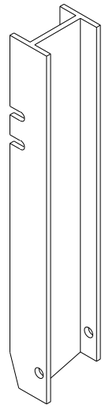
UPPER POST NO. 1 (1) (E)



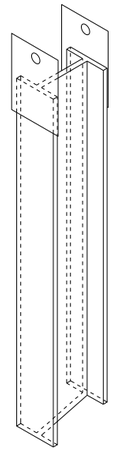
LOWER POST NO. 1 (2) (E)



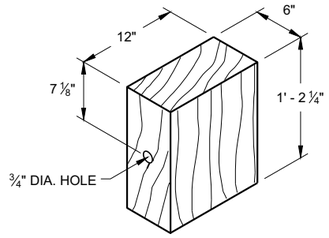
WOOD CRT POST (3) (E)
POSTS NUMBER 3-9



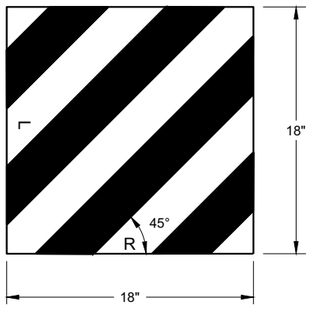
UPPER POST NO. 2 (15) (E)



LOWER POST NO. 2 (16) (E)

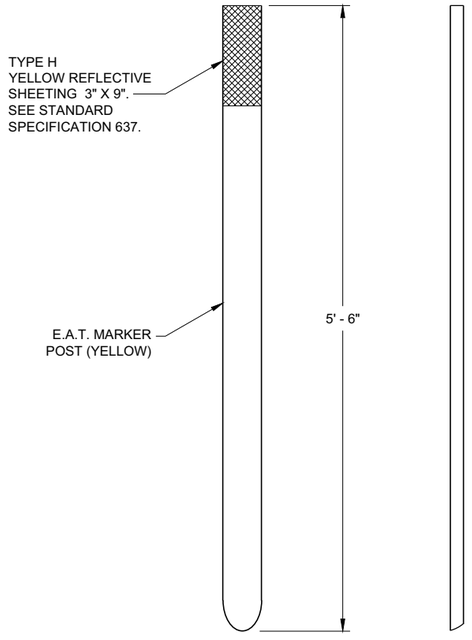


WOOD BLOCKOUT (4)
REQ'D. AT ALL POSTS EXCEPT POST NO'S 1 & 2



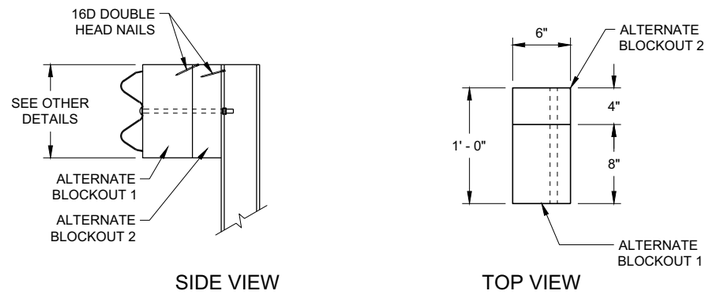
W5 - 59

REFLECTIVE SHEETING DETAIL (E)



FRONT VIEW SIDE VIEW

E.A.T. MARKER POST (13)



SIDE VIEW TOP VIEW

ALTERNATE WOOD BLOCKOUT DETAIL

MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)

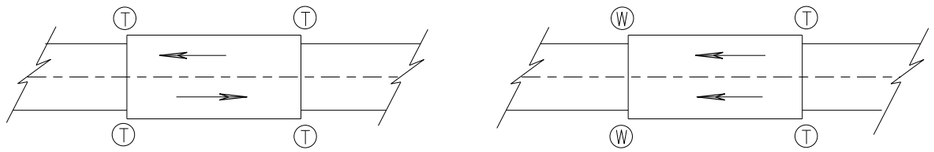
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVE UNIT SUPERVISOR

FHWA

6

SDD 14B44 - 04c



TWO WAY TRAFFIC

ONE WAY TRAFFIC

(T) THRIE BEAM CONNECTION

(W) W-BEAM CONNECTION WHEN REQUIRED

TYPICAL LOCATIONS OF THRIE BEAM AND W-BEAM CONNECTIONS TO BRIDGE

GENERAL NOTES

IF ROCK IS ENCOUNTERED, REMOVE ROCK TO FULL DEPTH OF POST PLUS 2½", AND 12" DIAMETER AROUND POST. SEE 14B42 FOR MORE DETAILS.

TRANSITION USES STEEL POSTS ONLY.

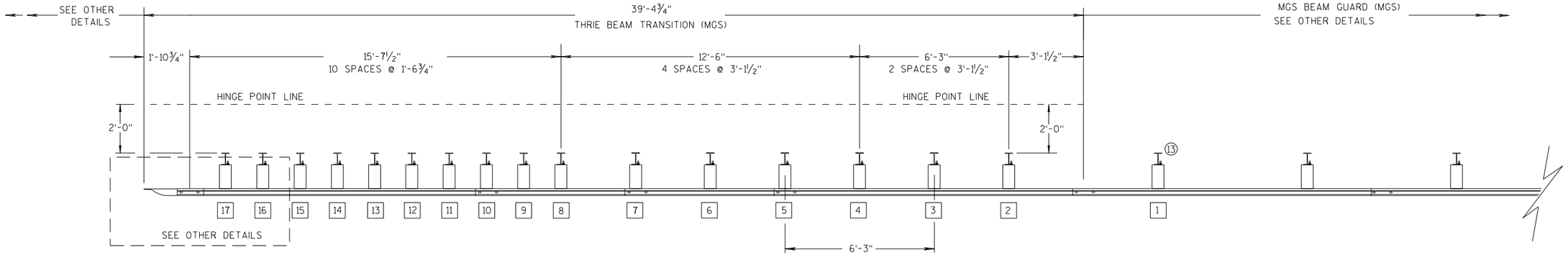
SEE STANDARD DETAIL DRAWING 14 B 42 FOR MORE INFORMATION.

POST 2 THROUGH 17 USES STEEL POST ONLY

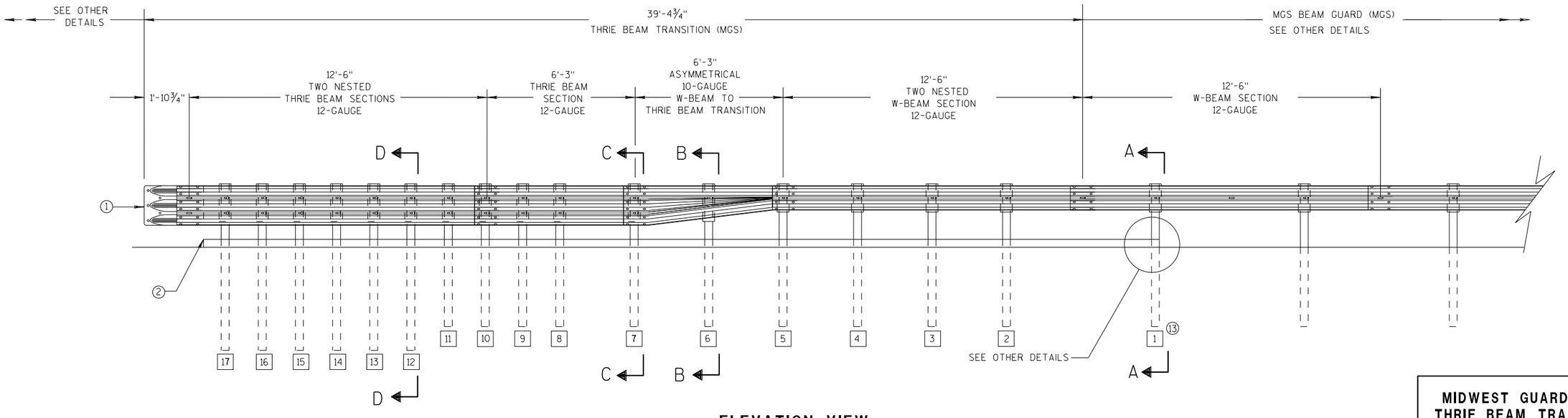
(1) BRIDGE RAILING TYPE "W" DOES NOT REQUIRE A TERMINAL CONNECTOR.

(2) OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

(13) STEEL OR WOOD POST IS ACCEPTABLE AT POST 1. SEE SDD14B42



PLAN VIEW



ELEVATION VIEW

MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 14 B 45-5a

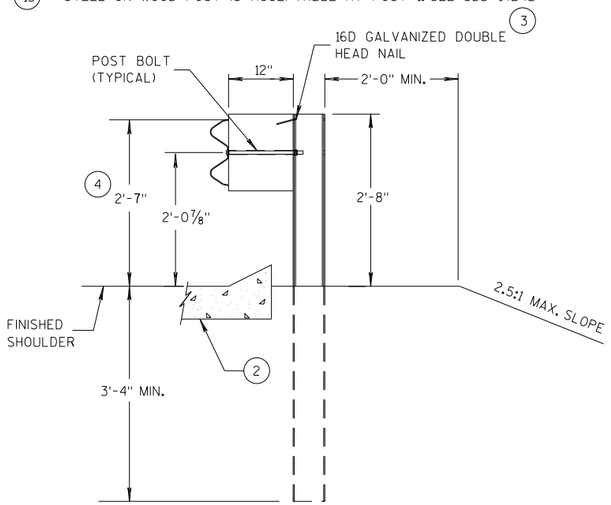
GENERAL NOTES

(2) OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

(3) WHEN USING STEEL POSTS AND WOOD BLOCKOUTS INSTALL FOUR 10D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

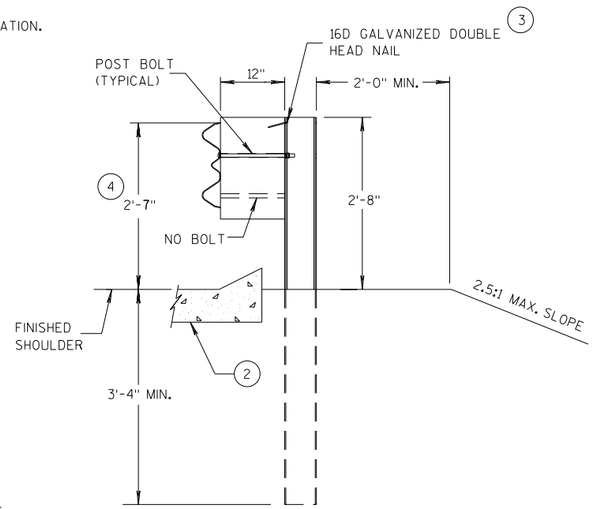
(4) TOLERANCE FOR TOP OF W-BEAM RAIL IS ± 1".

(13) STEEL OR WOOD POST IS ACCEPTABLE AT POST 1. SEE SDD 14B42



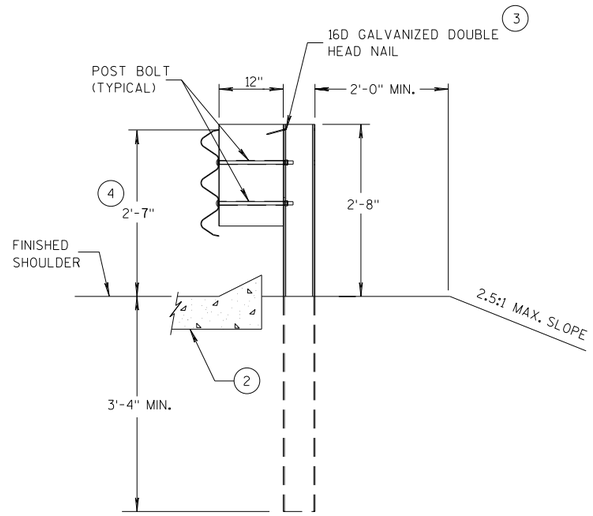
SECTION A-A (13)

POSTS 1-5



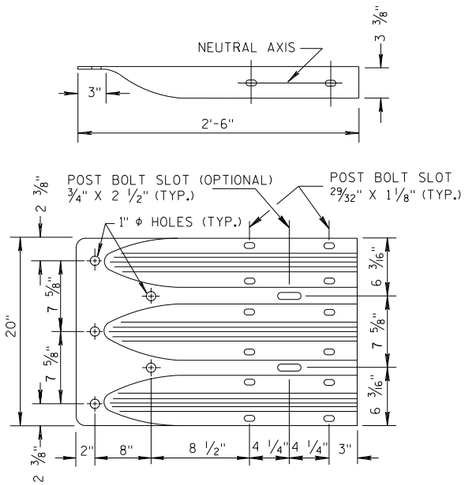
SECTION B-B

POST 6

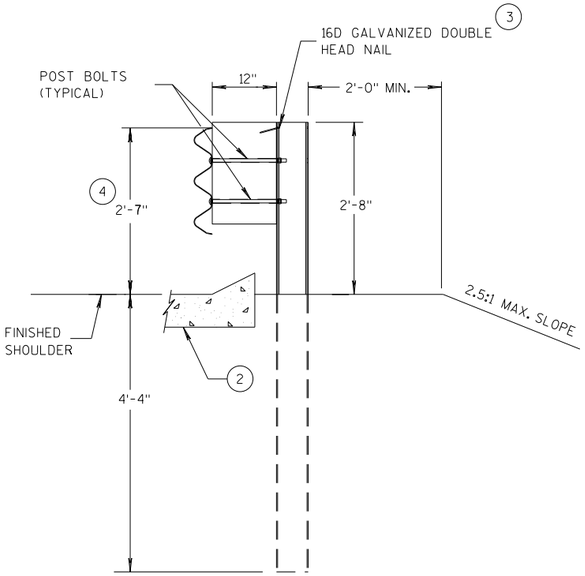


SECTION C-C

POSTS 7-11

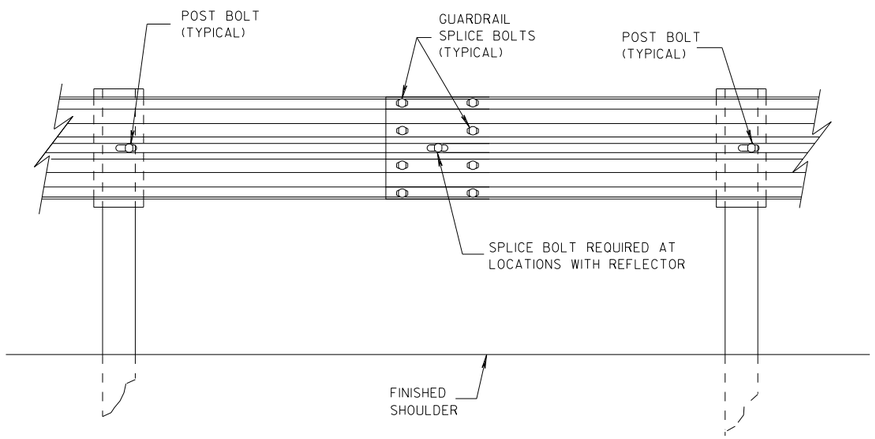


THRIE BEAM TERMINAL CONNECTOR



SECTION D-D

POSTS 12-17



SPLICE DETAIL

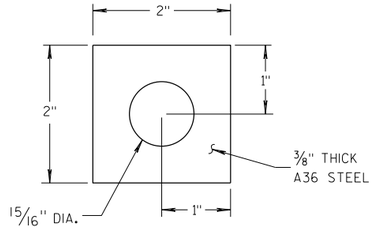
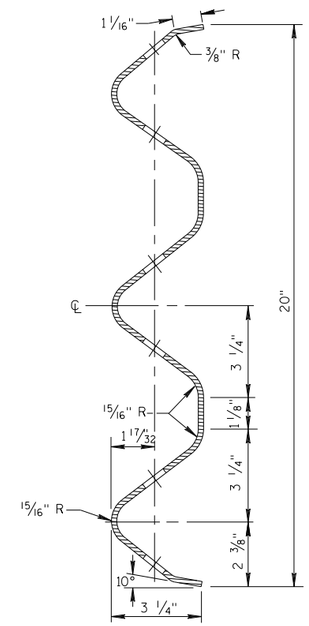


PLATE WASHER DETAIL



SECTION THRU THRIE BEAM RAIL ELEMENT

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

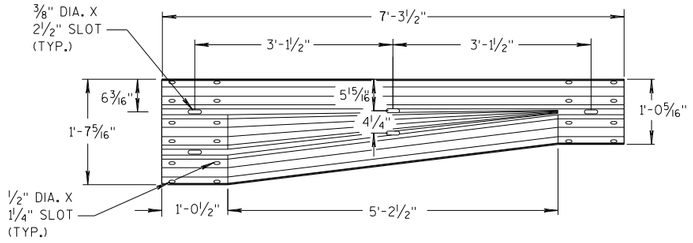
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR 25

6

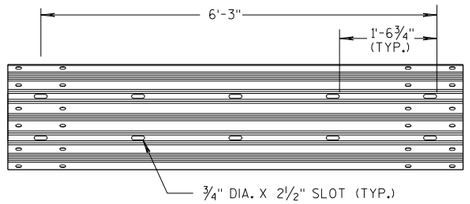
S.D.D. 14 B 45-5b

6

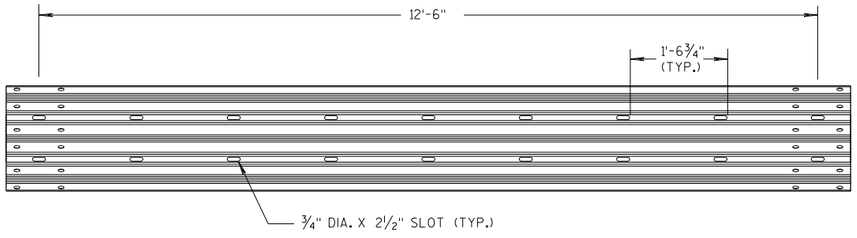
S.D.D. 14 B 45-5b



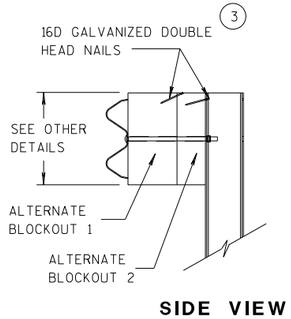
W-BEAM TO THRIE BEAM TRANSITION SECTION



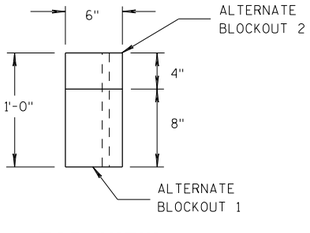
6'-3" THRIE BEAM SECTION



12'-6" THRIE BEAM SECTION

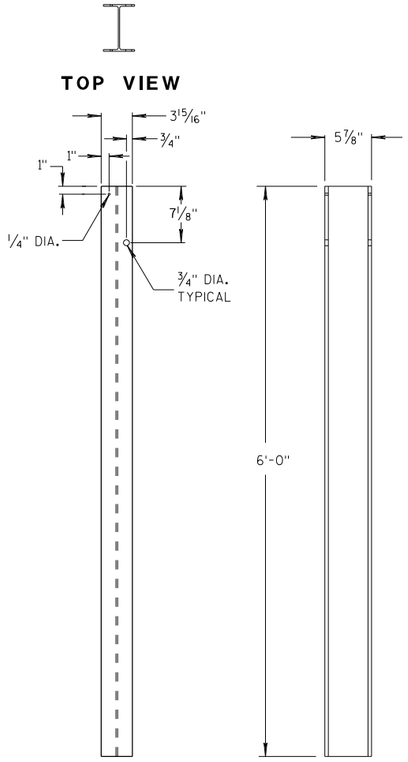


SIDE VIEW



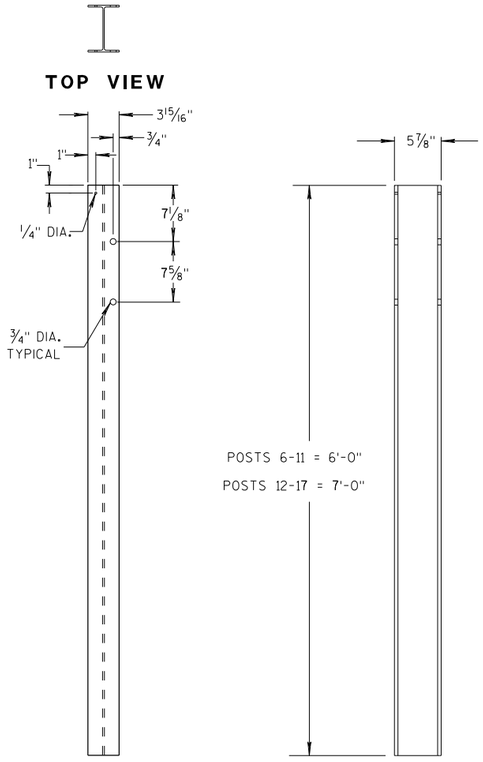
TOP VIEW

ALTERNATE WOOD BLOCKOUT DETAIL



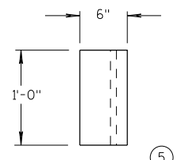
FRONT VIEW SIDE VIEW

STEEL POSTS 1-5 (13)

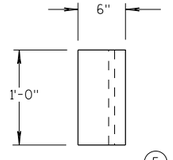


FRONT VIEW SIDE VIEW

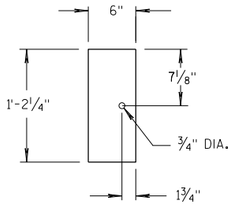
STEEL POSTS 6-17



TOP VIEW (5)

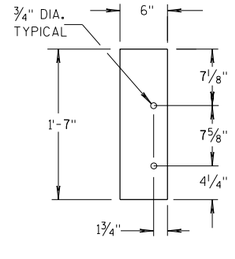


TOP VIEW (5)



FRONT VIEW

BLOCKOUT POSTS 1-5



FRONT VIEW

BLOCKOUT POSTS 6-17

GENERAL NOTES

STEEL POSTS ARE W6X9 OR W6X8.5.

BOLT HOLES FOR POST ARE ON FRONT AND OF SIDE OF POST.

(3) WHEN USING STEEL POSTS AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

(5) WOOD BLOCKS MAY BE CONSTRUCTED OUT OF 2 WOOD BLOCKS. SEE ALTERNATE WOOD BLOCK DETAIL.

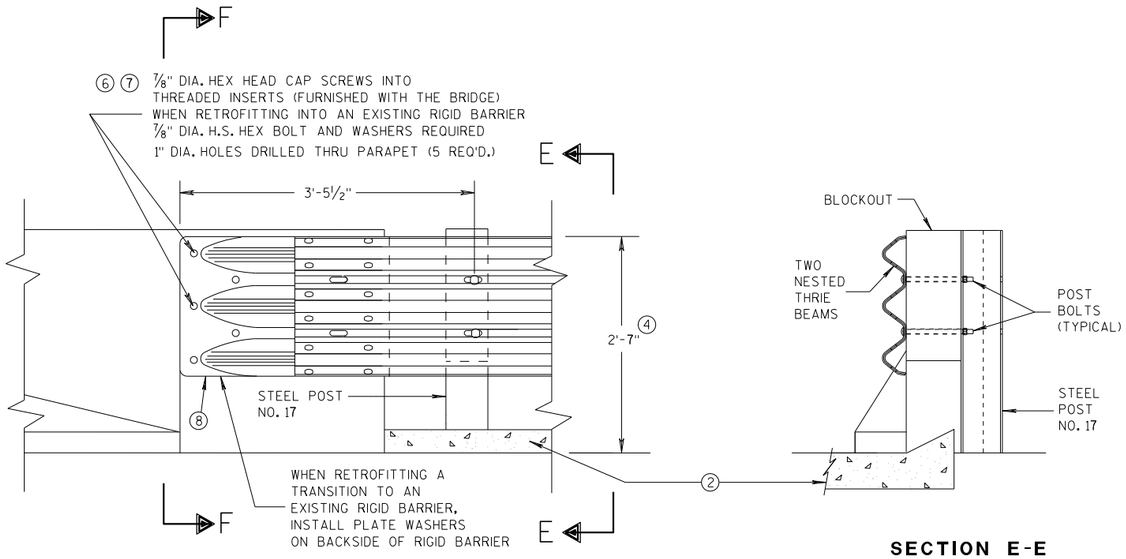
(13) STEEL OR WOOD POST IS ACCEPTABLE AT POST 1. SEE SDD 14B42.

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 14 B 45-5c



FRONT VIEW

SECTION E-E

THRIE BEAM CONNECTION TO BRIDGE PARAPET WITH SQUARE ENDS

GENERAL NOTES

THESE ARE TYPICAL CONNECTION DETAILS. ADJUST THE POSTION OF CONNECTIONS TO EXISTING BRIDGES TO FIT THE ACTUAL BRIDGE AND SITE DIMENSIONS.

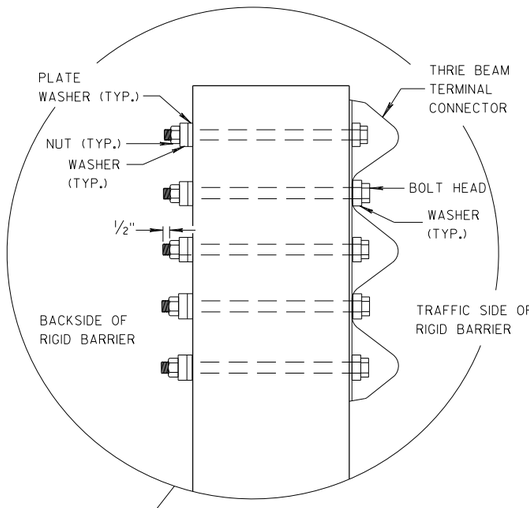
② OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

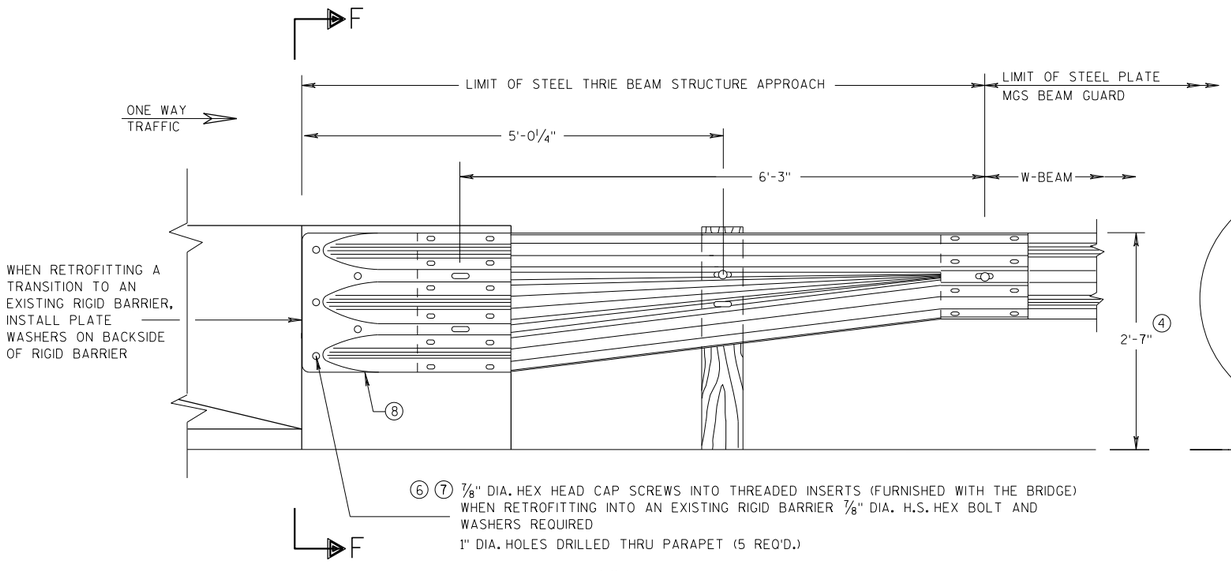
④ TOLERANCE FOR TOP OF BEAM IS ± 1".

⑥ DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

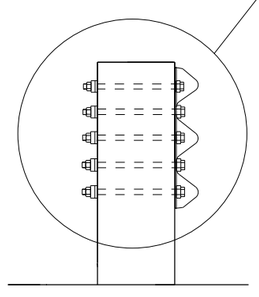
⑦ BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X 5/32" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.

⑧ THE RECESS FOR A W-BEAM CONNECTION, WHICH EXISTS ON SOME PARAPETS OF THIS TYPE, SHALL BE FILLED WITH A TREATED TIMBER BLOCKOUT. BLOCKOUT SIZE IS 1'-6" X 2'-0" X 3 1/2".

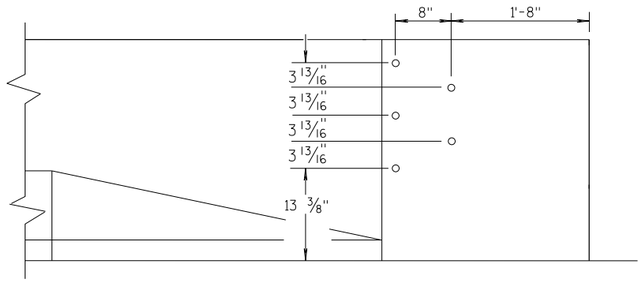




FRONT VIEW



SECTION F-F



DRILL HOLE LOCATION

W BEAM TRANSITION AND CONNECTION TO BRIDGE PARAPETS WITH SQUARE ENDS

(USE ONLY ON THE TRAFFIC EXIT END OF ONE WAY BRIDGES)

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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07/2018 DATE
/S/ Rodney Taylor
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GENERAL NOTES

THESE ARE TYPICAL CONNECTION DETAILS. ADJUST THE POSTION OF CONNECTIONS TO EXISTING BRIDGES TO FIT THE ACTUAL BRIDGE AND SITE DIMENSIONS.

(2) OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

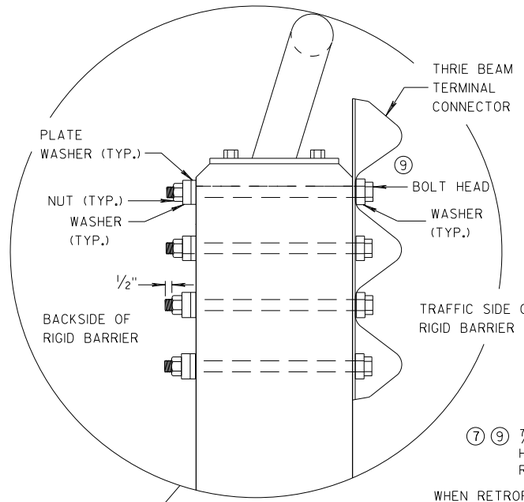
(4) TOLERANCE FOR TOP OF BEAM IS ± 1".

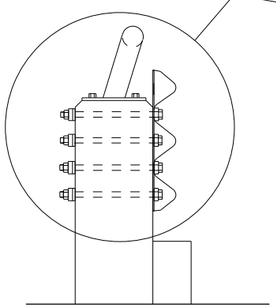
(6) DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

(7) BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X 5/32" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.

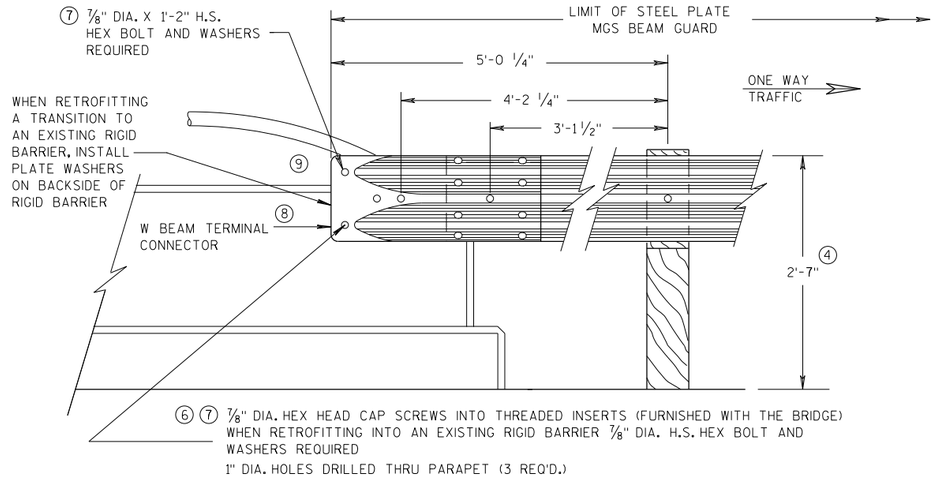
(8) THE RECESS FOR A W-BEAM CONNECTION, WHICH EXISTS ON SOME PARAPETS OF THIS TYPE, SHALL BE FILLED WITH A TREATED TIMBER BLOCKOUT. BLOCKOUT SIZE IS 1'-6" X 2'-0" X 3 ½".

(9) BOLT, NUT AND WASHERS NOT REQUIRED FOR THIS LOCATION WHEN RETROFITTING AN EXISTING PAPAPET AND THE HOLE IS EITHER ABOVE PARAPET OR WITHIN 4 INCHES OF THE EDGE OF PARAPET.





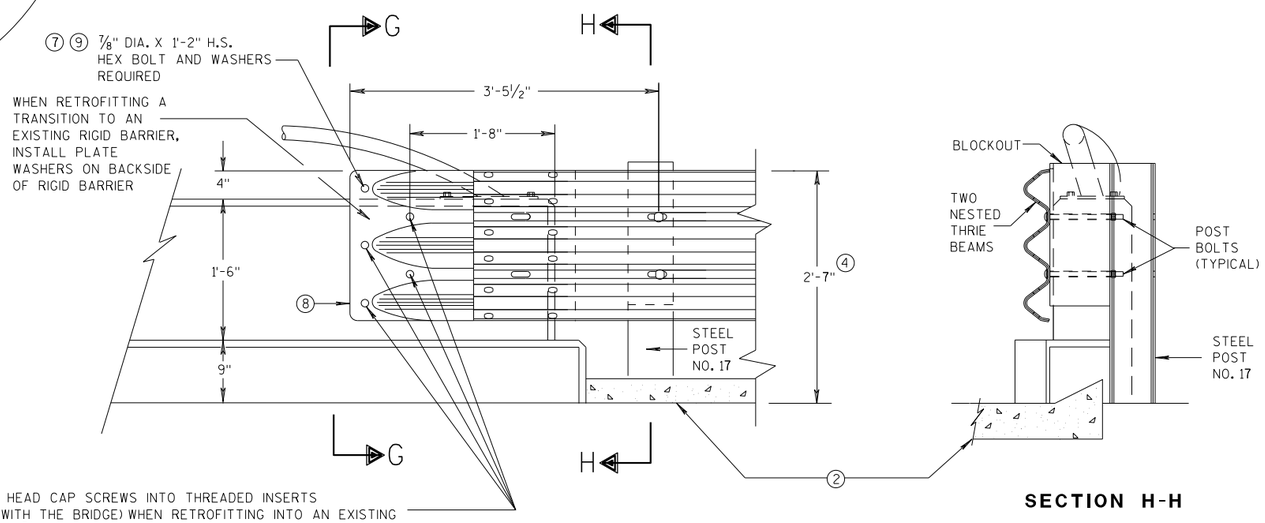
SECTION G-G



FRONT VIEW

W BEAM CONNECTION TO VERTICAL FACE PARAPET

(USE ONLY ON THE TRAFFIC EXIT END OF ONE WAY BRIDGES)



FRONT VIEW

SECTION H-H

THRIE BEAM CONNECTION TO VERTICAL FACED PARAPETS

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
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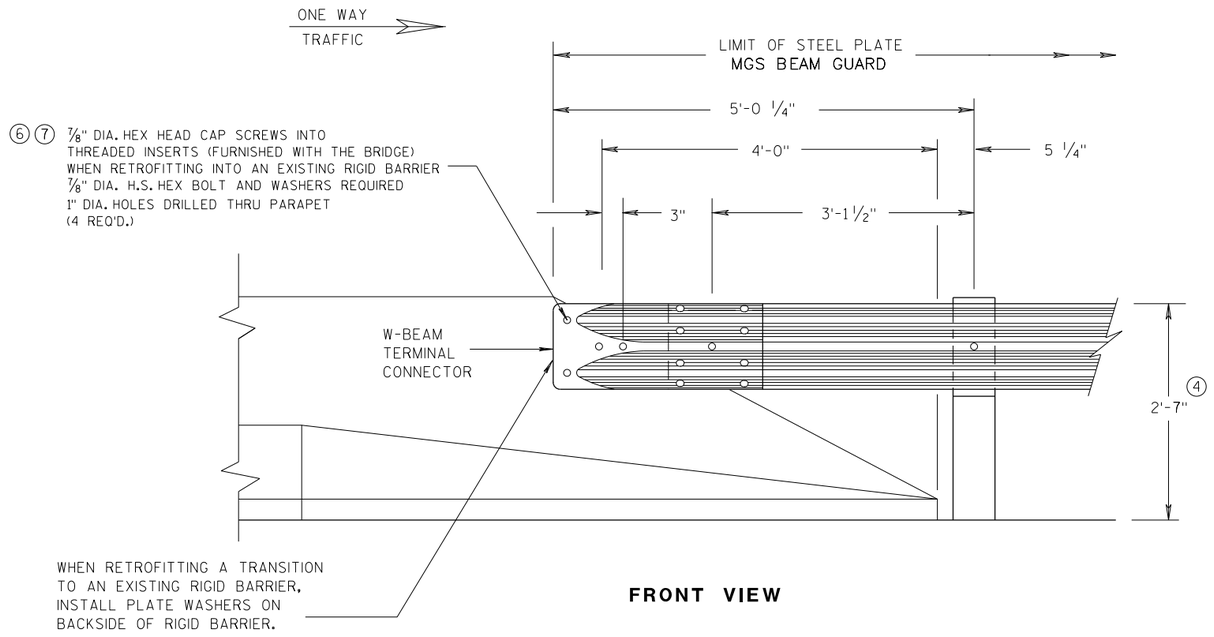


FRONT VIEW

W BEAM CONNECTION TO PARAPETS WITH SLOPED ENDS

(USE ONLY AT TRAFFIC EXIT END OF ONE WAY BRIDGE)

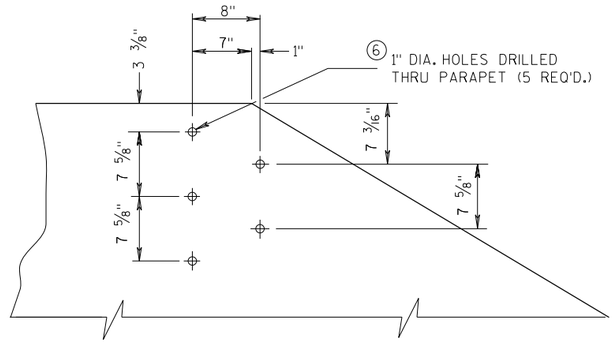
GENERAL NOTES

② OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

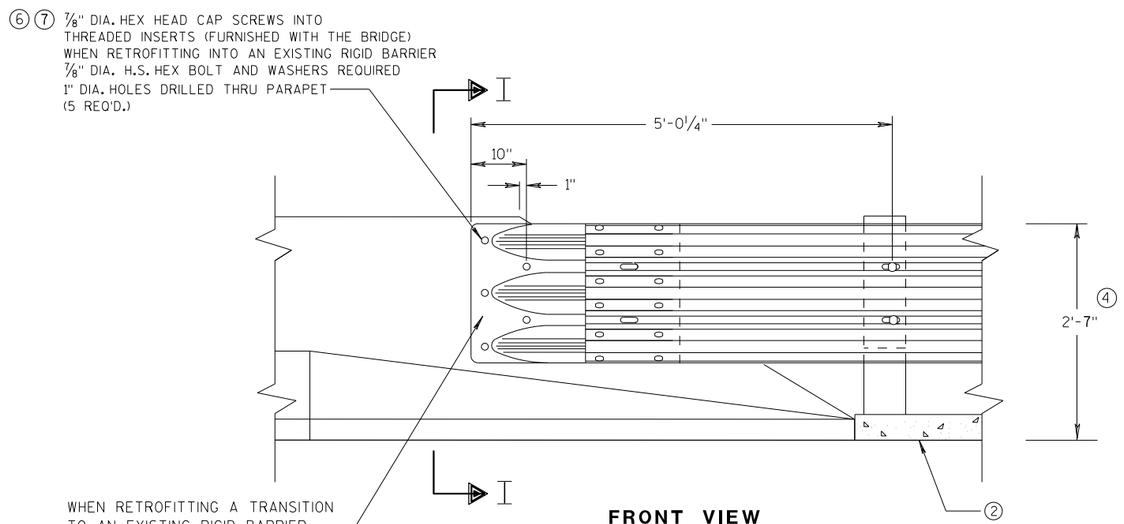
④ TOLERANCE FOR TOP OF BEAM IS ± 1".

⑥ DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

⑦ BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X 5/32" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.

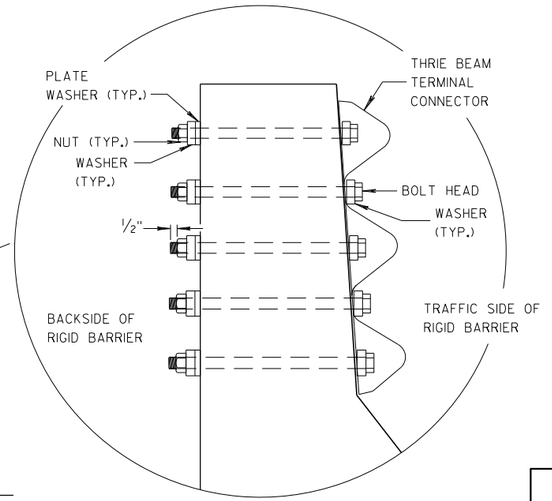


DRILL HOLE LOCATION AND PATTERN
FOR THRIE BEAM CONNECTION



FRONT VIEW

SECTION I-I



THRIE BEAM CONNECTION TO BRIDGE PARAPETS WITH SLOPED ENDS

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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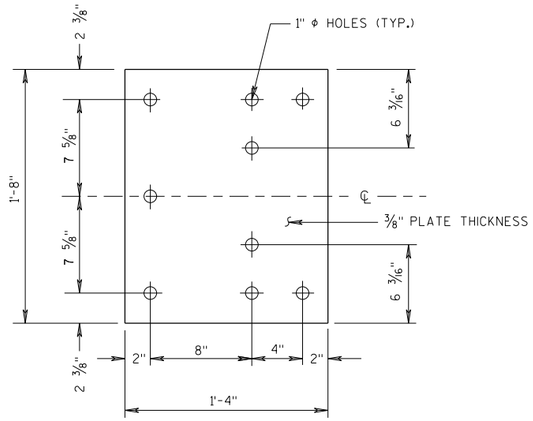
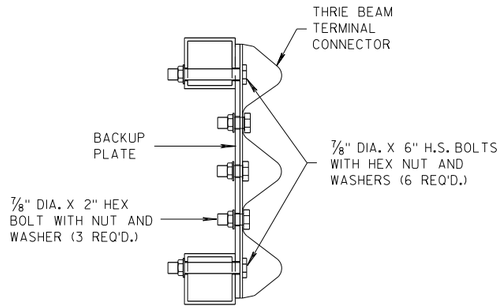
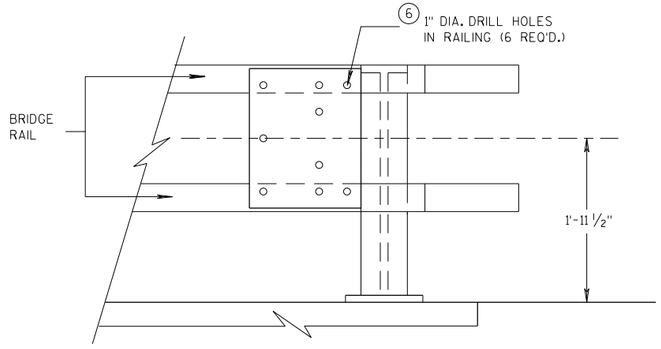


BACK-UP PLATE DETAIL



SECTION J-J

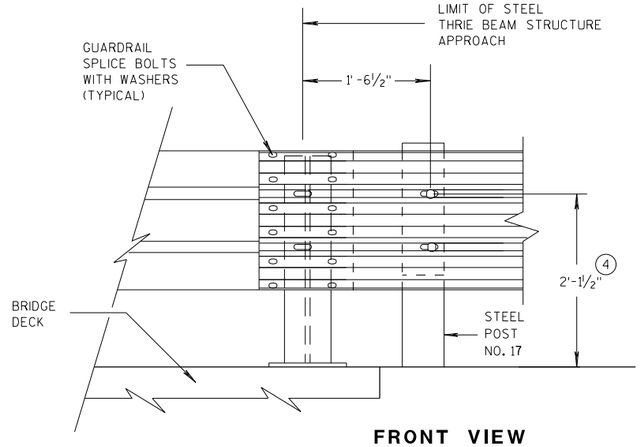


BACK-UP PLATE MOUNTING ONTO BRIDGE RAILING

GENERAL NOTES

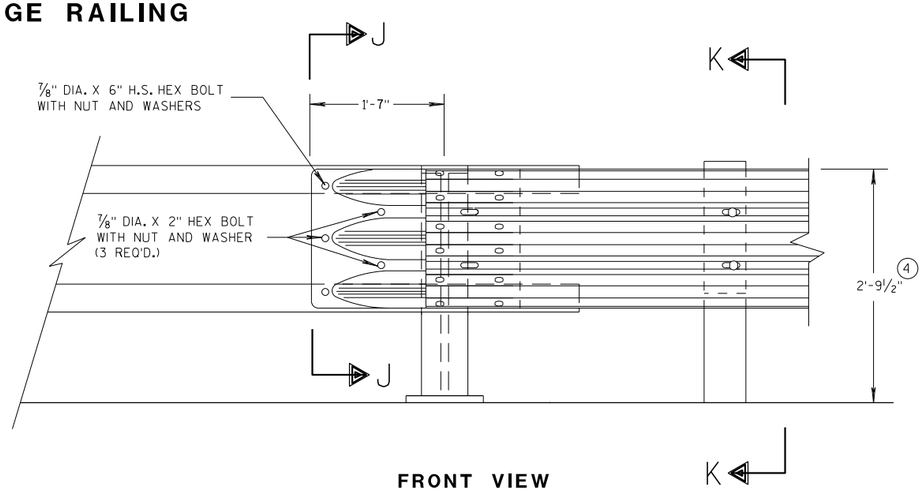
④ TOLERANCE FOR TOP OF BEAM IS ± 1".

⑥ DRILLING HOLES THROUGH THE PAPRPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

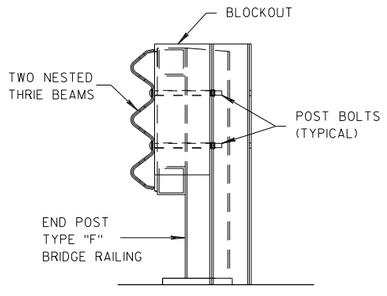


FRONT VIEW

THRIE BEAM CONNECTION TO STEEL RAILING TYPE "W"



FRONT VIEW



SECTION K-K

THRIE BEAM CONNECTION TO TUBULAR RAILING TYPE "F"

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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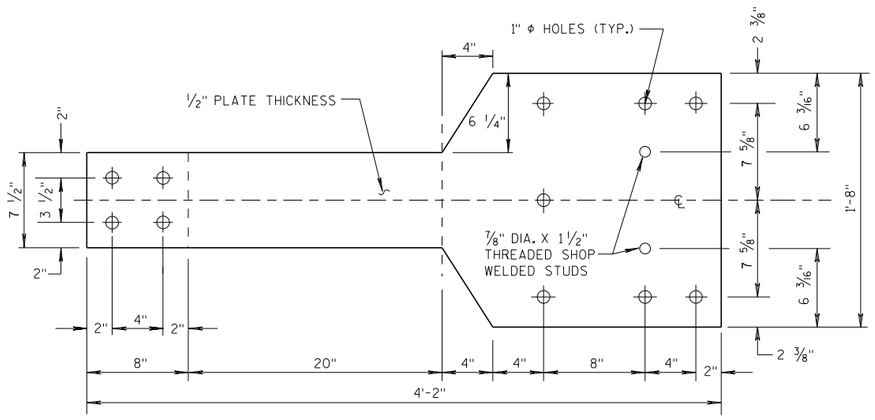
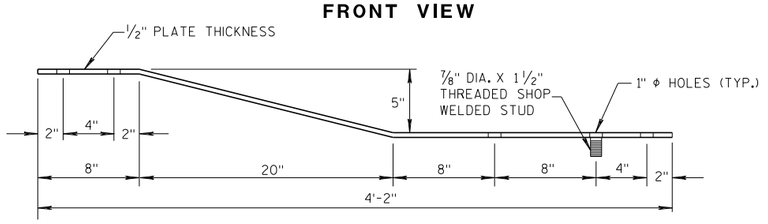
GENERAL NOTES

(4) TOLERANCE FOR TOP OF W-BEAM RAIL IS ± 1".

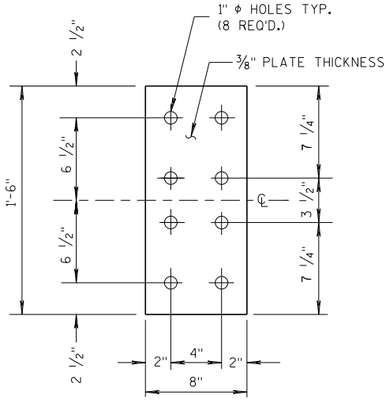


FRONT VIEW



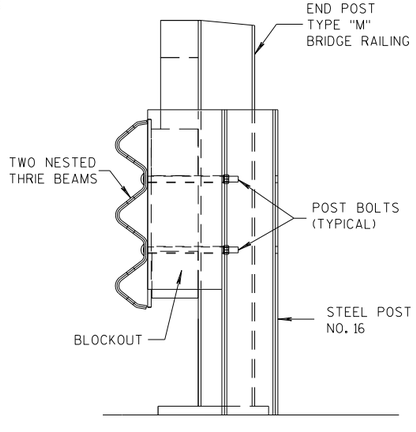
PLAN VIEW

BACK-UP PLATE DETAIL, TYPE "M"

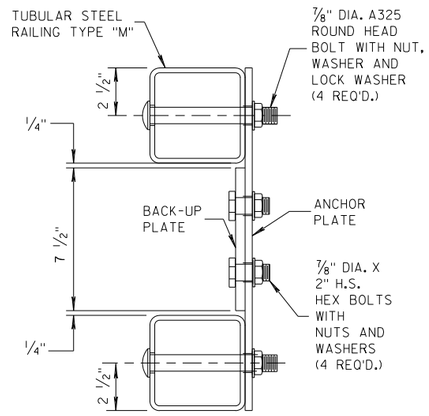


FRONT VIEW

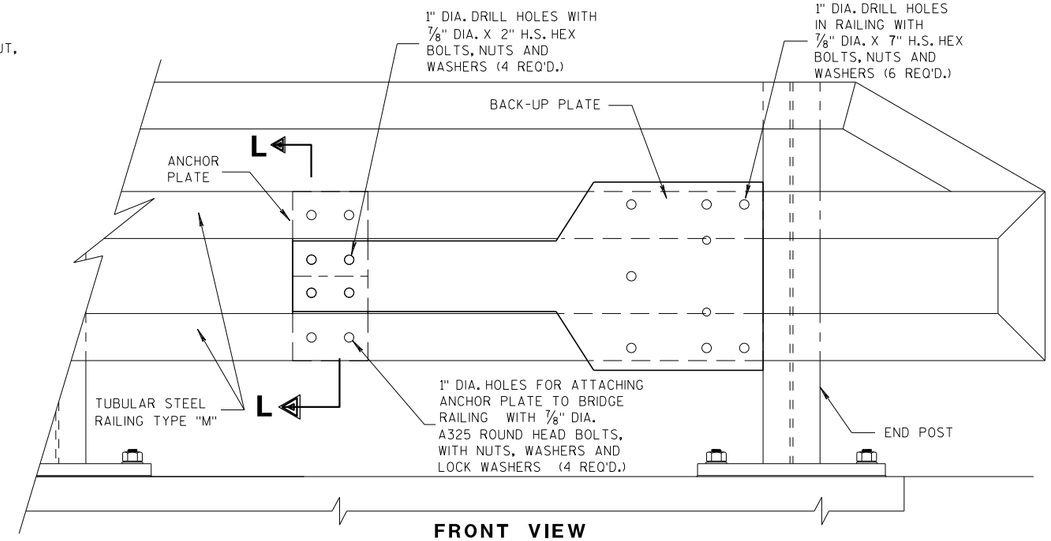
ANCHOR PLATE DETAIL, TYPE "M"



SECTION M-M

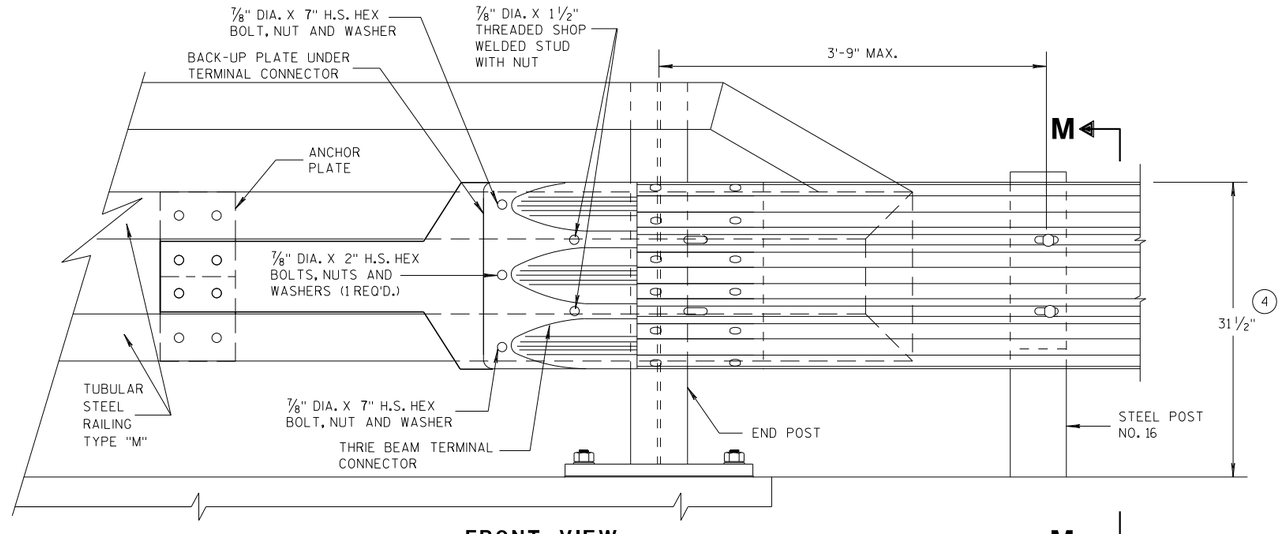


SECTION L-L

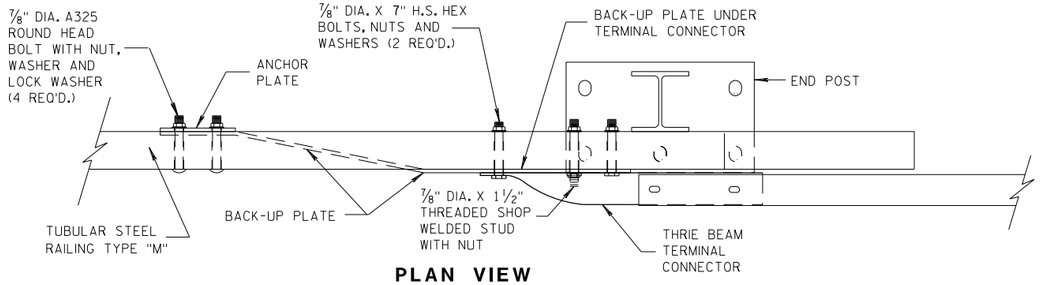


FRONT VIEW

ANCHOR AND BACK-UP PLATE MOUNTING TO BRIDGE RAILING, TYPE "M"



FRONT VIEW



PLAN VIEW

THRIE BEAM CONNECTION TO TUBULAR RAILING, TYPE "M"

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
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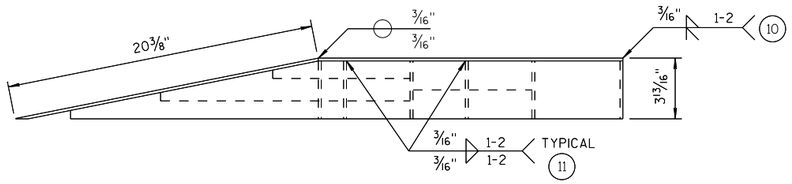

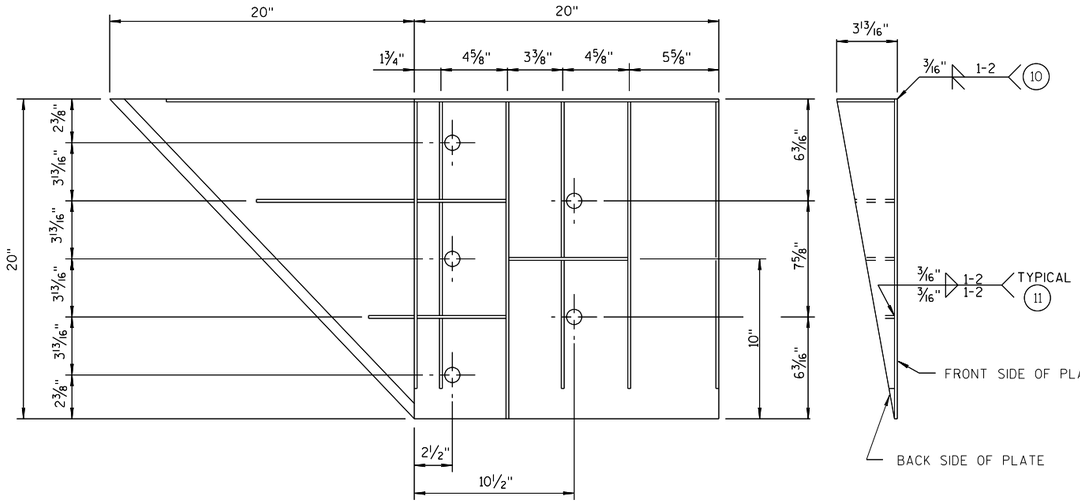

WELDING INSTRUCTION

(VIEWED FROM BACK SIDE OF PLATE)

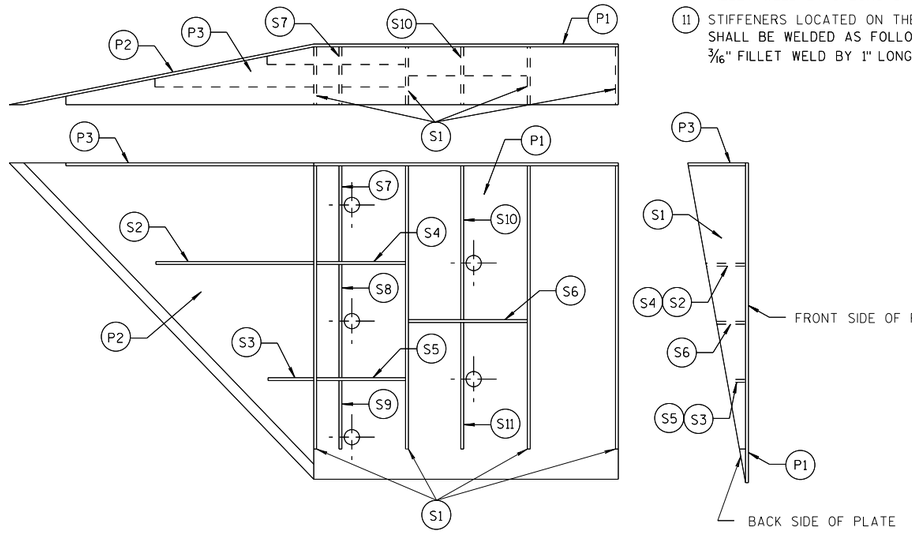

PLATE AND STIFFENER IDENTIFICATION

(VIEWED FROM BACK SIDE OF PLATE)

GENERAL NOTES

COVER PLATE PANELS ARE 3/16" THICK.

ALL STIFFENERS ARE 1/4" THICK.

CONNECTOR PLATE SHALL BE FABRICATED FROM ASTM GRADE A36 STEEL AND GALVANIZED.

FOR GALVANIZED REQUIREMENTS, SEE SECTION 614 OF THE STANDARD SPECIFICATIONS.

ALL HOLE DIAMETERS SHALL BE 1".

FOR OPPOSITE SIDE INSTALLATION MIRROR DRAWINGS.

(10) STIFFENERS LOCATED AT THE OUTSIDE EDGES OF THE COVER PLATES SHALL BE WELDED AS FOLLOWS:
SINGLE BEVEL GROOVE WELD ON EXTERNAL SIDES AND 3/16" FILLET WELD BY 1" LONG SPACED AT 2" ON INTERNAL SIDES.

(11) STIFFENERS LOCATED ON THE INSIDE OF THE COVER PLATE SHALL BE WELDED AS FOLLOWS:
3/16" FILLET WELD BY 1" LONG SPACED AT 2".

CONNECTOR PLATE DIMENSION (PER ASSEMBLY)

PLATE	QUANTITY	SHAPE	SIZE (A x B x C x D)	THICKNESS
P1	1		20" x 20"	3/16"
P2	1		20" x 20" x 28 9/16"	3/16"
P3	1		39" x 3 5/8" x 20" x 19 5/16"	3/16"
S1	4		18 7/16" x 3 5/8" x 18 3/4"	1/4"
S2	1		10 1/4" x 2 7/16" x 10 3/8" x 1/2"	1/4"
S3	1		3" x 1 1/16" x 3 1/8" x 1/2"	1/4"
S4	1		6 1/8" x 2 7/16"	1/4"
S5	1		6 1/8" x 1 1/16"	1/4"
S6	1		7 3/4" x 1 3/4"	1/4"
S7	1		2 9/16" x 6" x 3 5/8" x 5 7/8"	1/4"
S8	1		1 5/32" x 7 1/2" x 2 1/2" x 7 3/8"	1/4"
S9	1		6 1/16" x 6 3/16" x 1 3/32"	1/4"
S10	1		1 7/8" x 9 7/8" x 3 5/8" x 9 11/16"	1/4"
S11	1		8 1/2" x 8 3/4" x 1 13/16"	1/4"

SINGLE SLOPE CONNECTION PLATE

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

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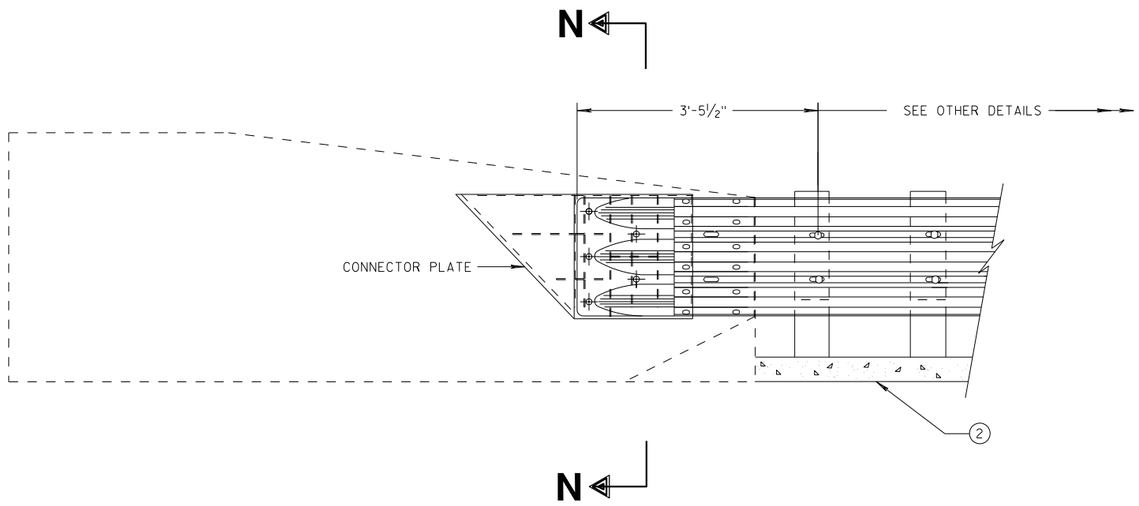
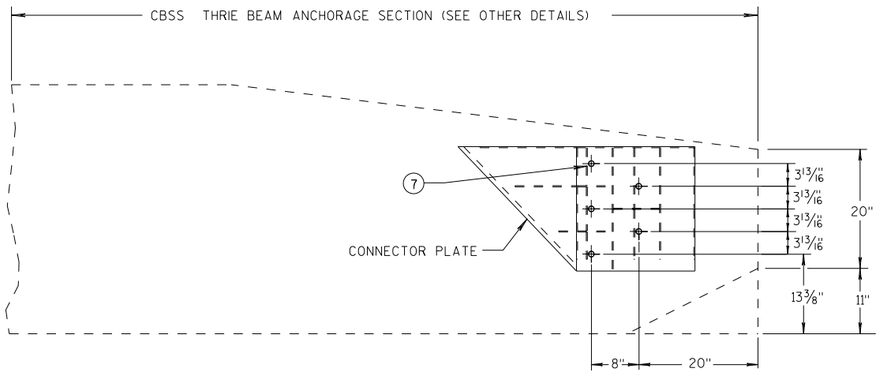


THRIE BEAM CONNECTION TO SINGLE SLOPE BARRIER



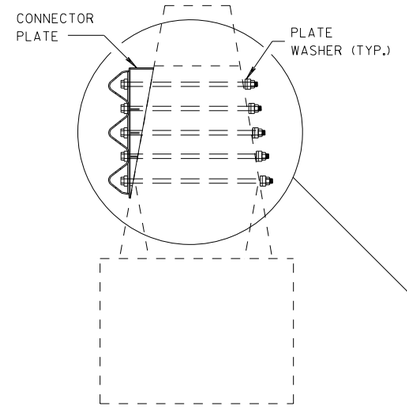
SINGLE SLOPE CONNECTION PLATE PLACEMENT

GENERAL NOTES

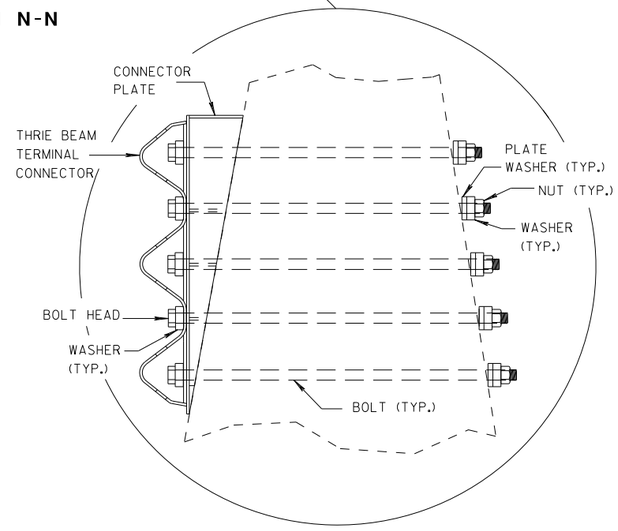
CONNECTOR PLATE, DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

(2) OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

(7) BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X 5/32" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.



SECTION N-N



MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
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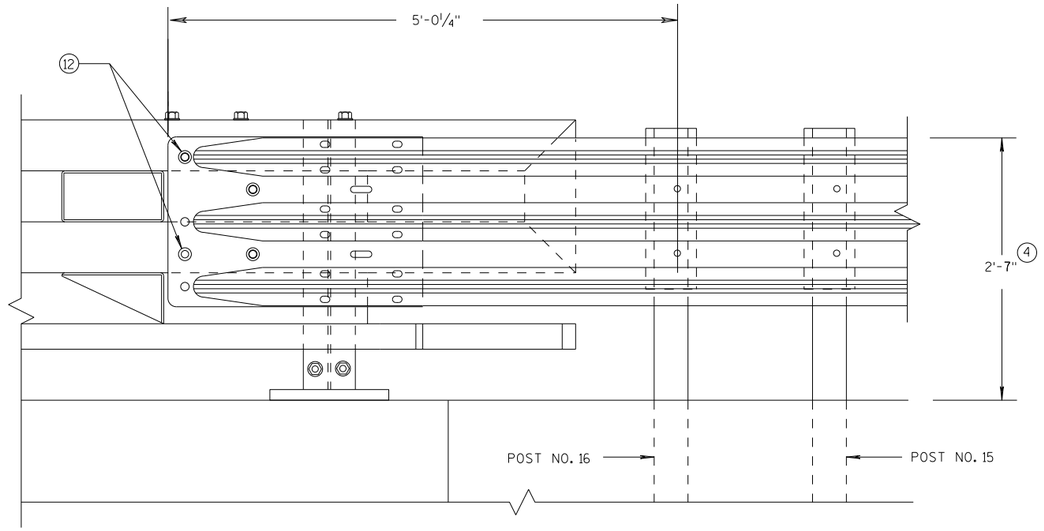
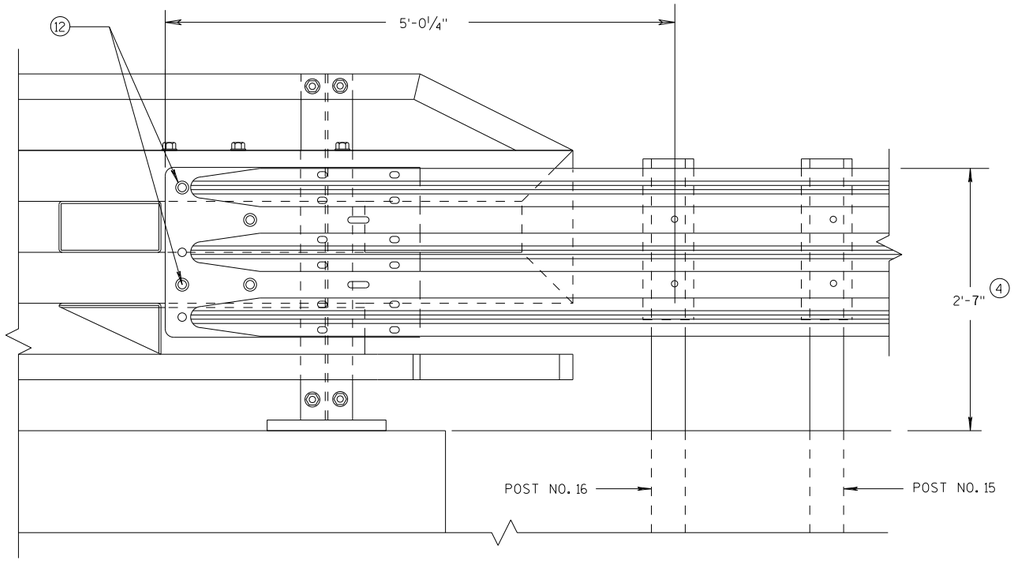


ELEVATION OF DETAIL AT NY3 END POST

THRIE BEAM RAIL ATTACHMENT



ELEVATION OF DETAIL AT NY4 END POST

THRIE BEAM RAIL ATTACHMENT

GENERAL NOTES

(4) TOLERANCE FOR TOP OF BEAM IS ± 1".

(12) BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. ON BACKSIDE OF PARAPET ONE ROUND WASHER, AND NUT REQUIRED. BOLT THREAD IS TO EXTEND 1/2-INCH BEYOND NUT.

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

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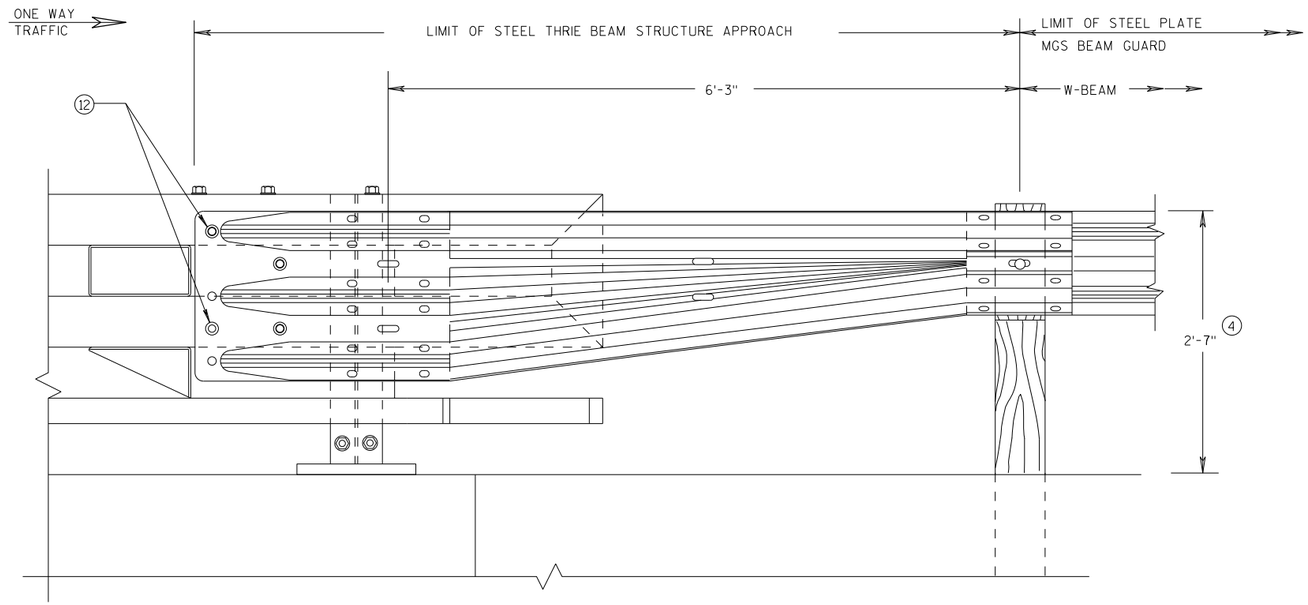
S.D.D. 14 B 45-5k



FRONT VIEW

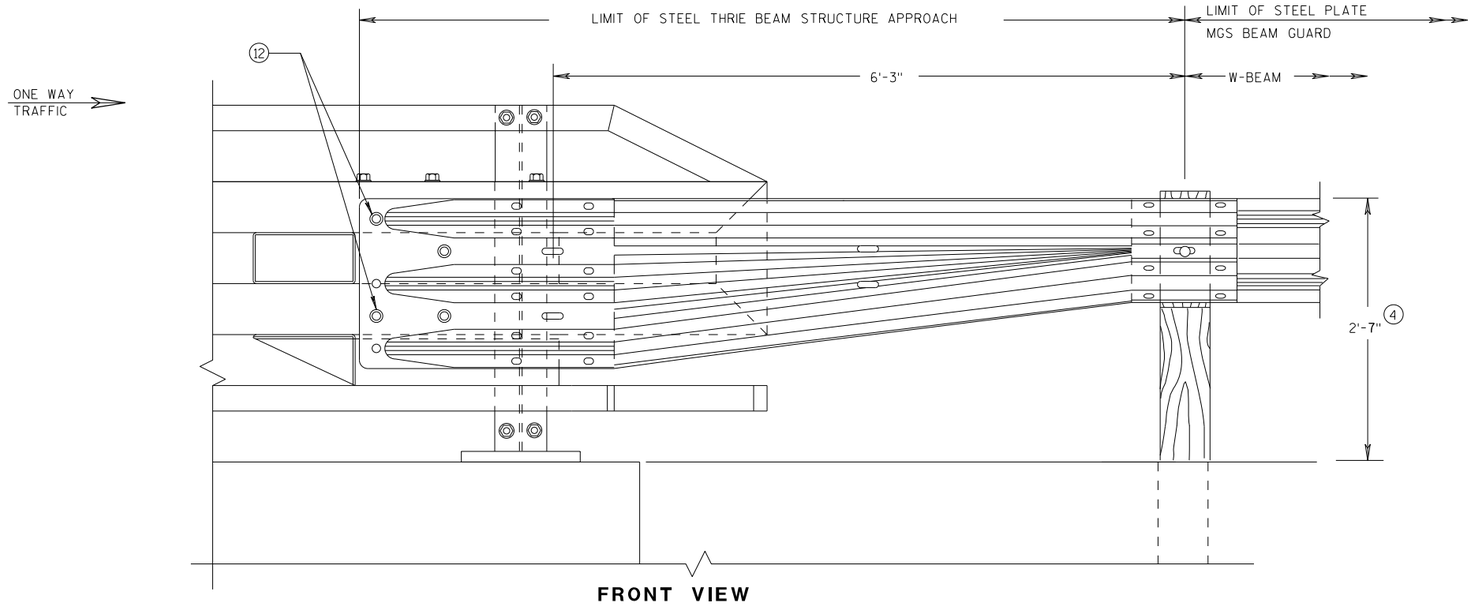
W BEAM TRANSITION AND CONNECTION TO BRIDGE RAILING TYPE "NY3"

(USE ONLY ON THE TRAFFIC EXIT END OF ONE WAY BRIDGES)

GENERAL NOTES

(4) TOLERANCE FOR TOP OF BEAM IS ± 1".

(12) BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. ON BACKSIDE OF PARAPET ONE ROUND WASHER, AND NUT REQUIRED. BOLT THREAD IS TO EXTEND 1/2-INCH BEYOND NUT.



FRONT VIEW

W BEAM TRANSITION AND CONNECTION TO BRIDGE RAILING TYPE "NY4"

(USE ONLY ON THE TRAFFIC EXIT END OF ONE WAY BRIDGES)

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

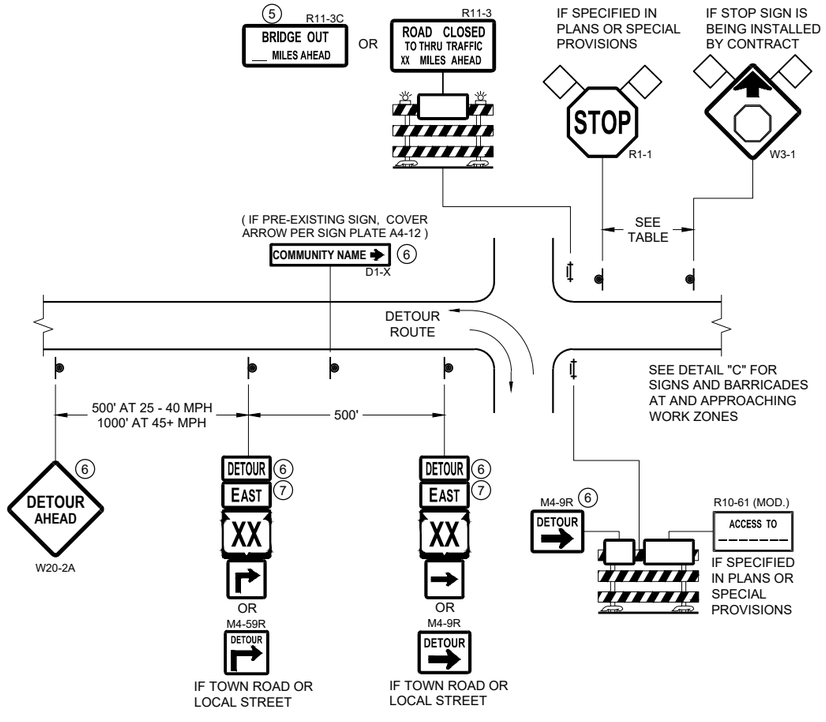
APPROVED
7/2018 DATE
/S/ Rodney Taylor
ROADWAY STANDARDS D UNIT SUPERVIS
NT
FHWA

6

S.D.D. 14 B 45-5L

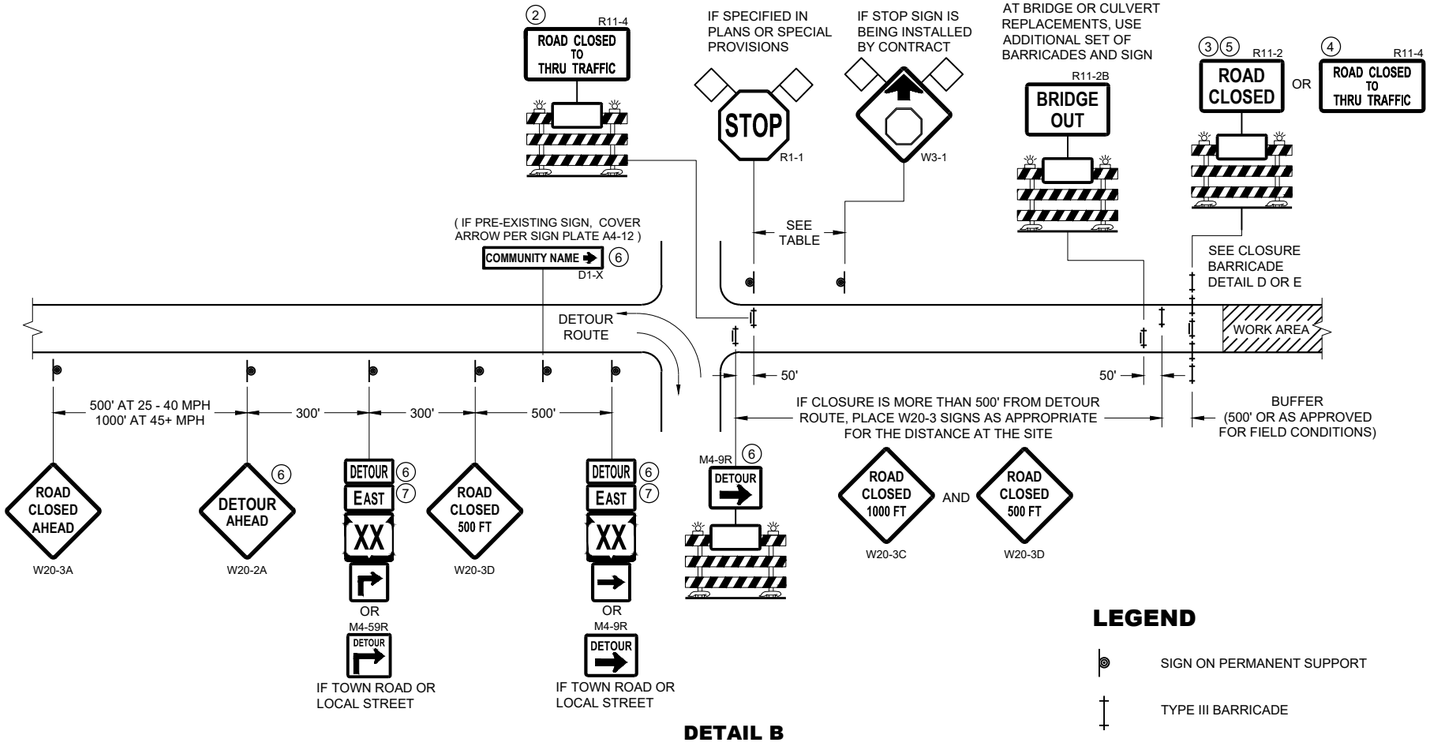
6

S.D.D. 14 B 45-5L



DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN OR EQUAL TO ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

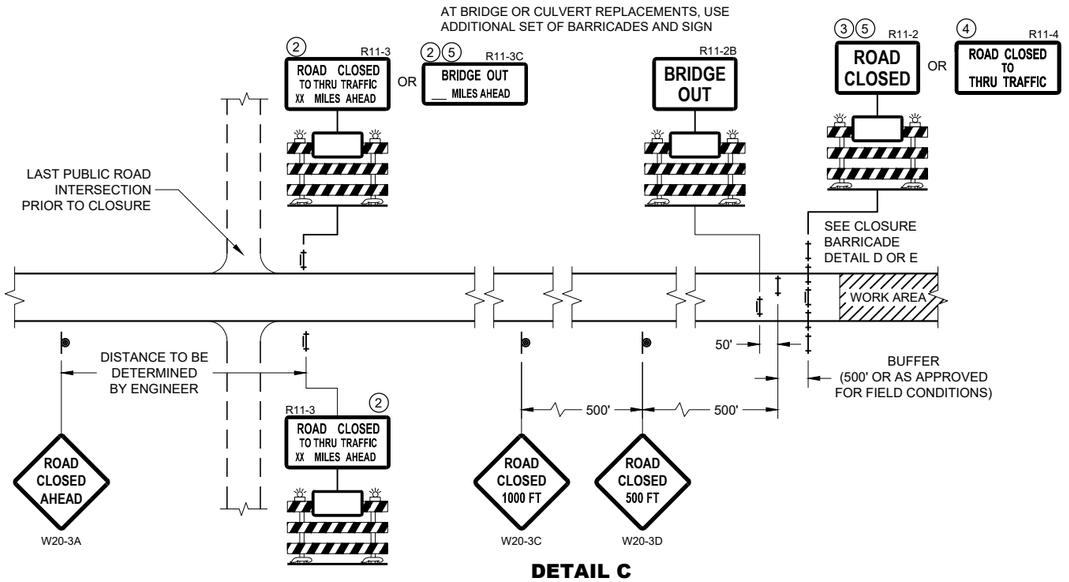
WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b" FOR GENERAL NOTES AND FOOTNOTES ① THROUGH ⑦

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)
- DETOUR — M4 - 8
- EAST — M3 - X
- XX — M1 - 4 OR XX — M1 - 6 OR COUNTY X — M1 - 5A
- M05 - 1 OR M06 - 1



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEER

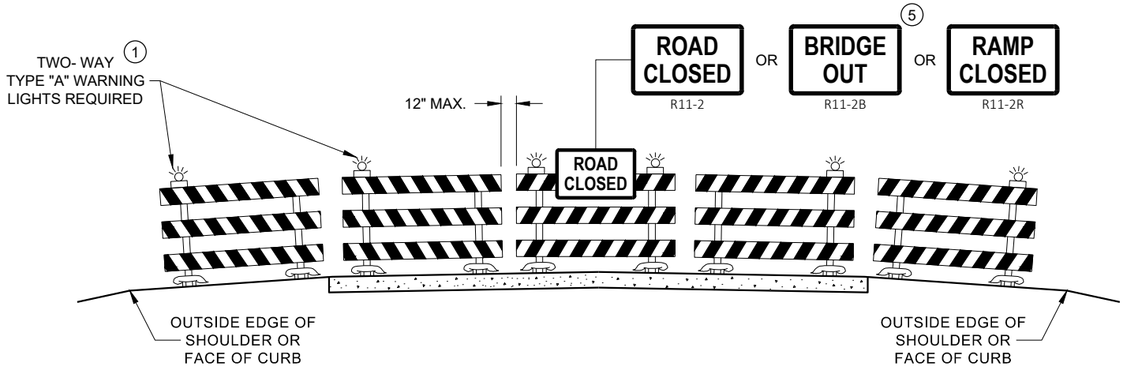
FHWA

6

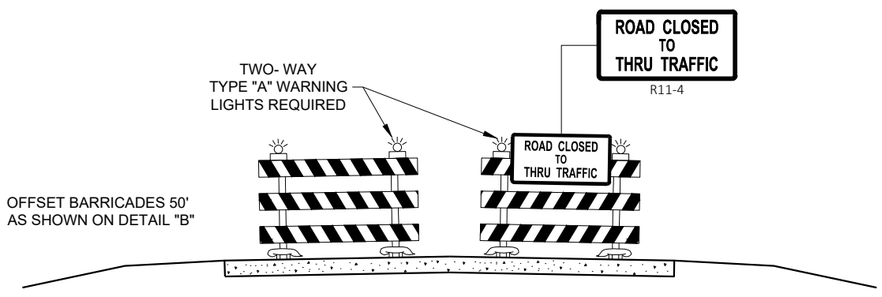
SDD 15C02 - 09a

6

SDD 15C02 - 09a



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60 " X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING.

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

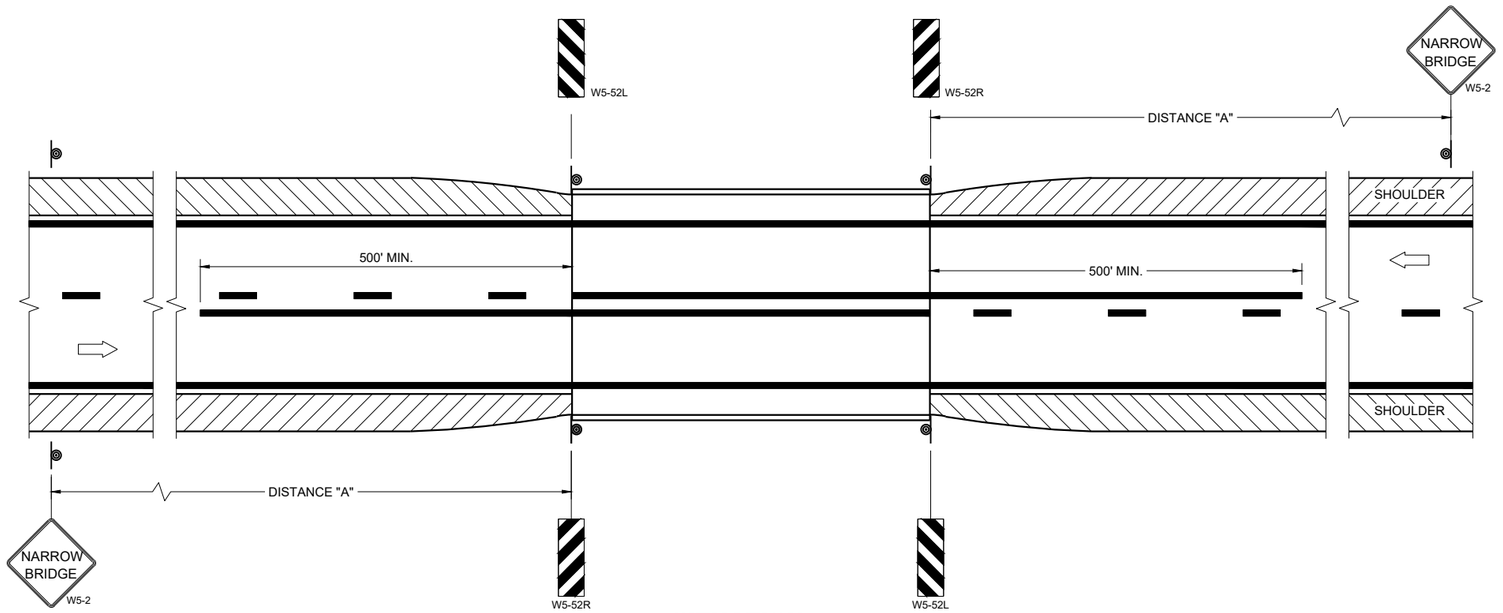
BARRICADES AND SIGNS FOR VARIOUS CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Andrew Heidtke — WORK ZONE ENGINEER
FHWA

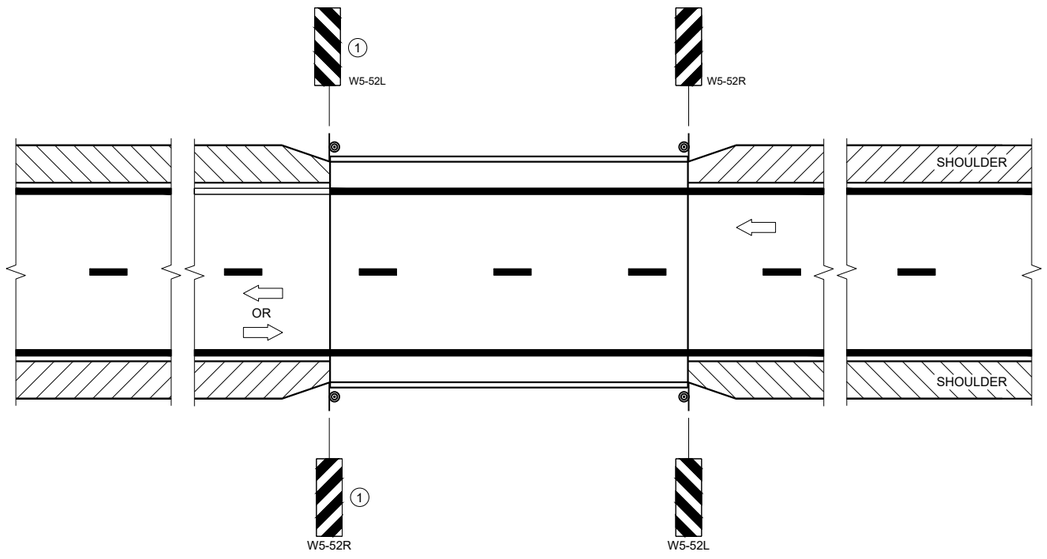
6

SDD 15C02 - 09b



SITUATION 1

WARRANTING CRITERIA:
BRIDGE WIDTH IS AT LEAST 16 FEET BUT LESS THAN 24 FEET.



SITUATION 2

WARRANTING CRITERIA:
1. BRIDGE WIDTH IS AT LEAST 24 FEET AND
2. BRIDGE SHOULDER WIDTH IS LESS THAN 6 FEET

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THE DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

LOCATE W5-52 SIGN POST(S) BEHIND GUARDRAIL WHEN PRESENT.

PLACE THE EDGE OF THE W5-52 SIGN IN LINE WITH FACE OF CURB OR PARAPET.

ON BRIDGE ONLY PROJECTS, PLACE 300 FEET OF EDGELINE.

OMIT EDGELINES ON ROADWAYS WITHOUT EXISTING EDGELINES.

(1) OMIT ON ONE-WAY TRAVELED WAYS.

LEGEND

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC

DISTANCE TABLE

POSTED OR 85TH PERCENTILE SPEED	DISTANCE "A"
25	150'
30	200'
35	250'
40	300'
45	400'
50	550'
55	700'

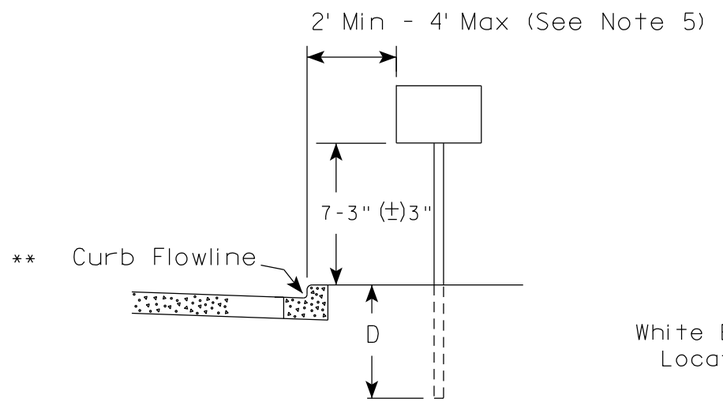
SIGNING AND MARKING FOR TWO LANE BRIDGES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

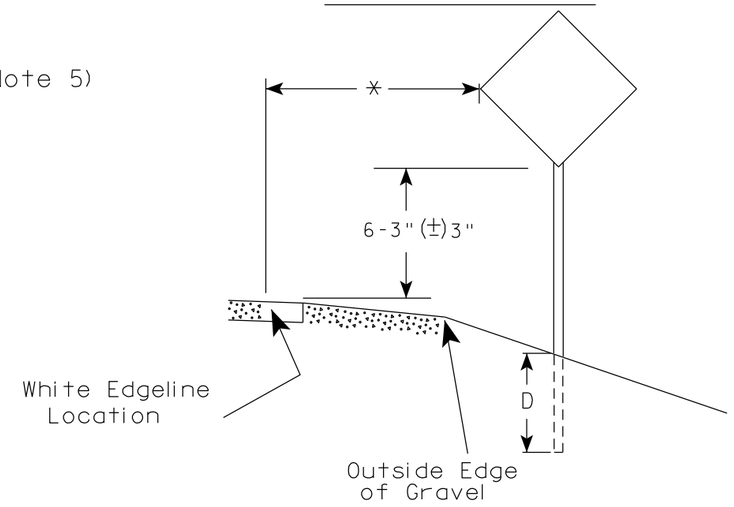
APPROVED
May 2023
DATE
/S/ Jeannie Silver
Statewide Pavement Marking Engineer
FHWA

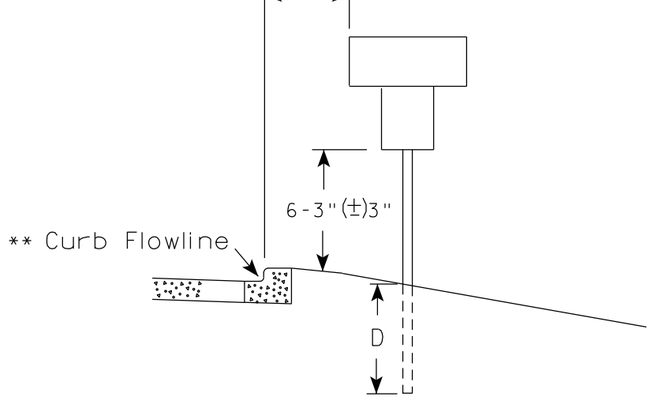
6
SDD 15C06-12

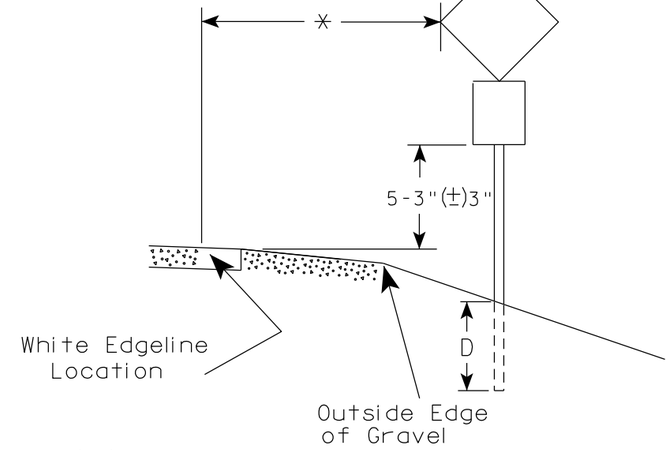
URBAN AREA



RURAL AREA (See Note 2)







** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
   The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".
3. For expressways and freeways, mounting height is 7'- 3" (±) 3" or 6'-3" (±) 3" depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±) 3".
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directd by the Engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

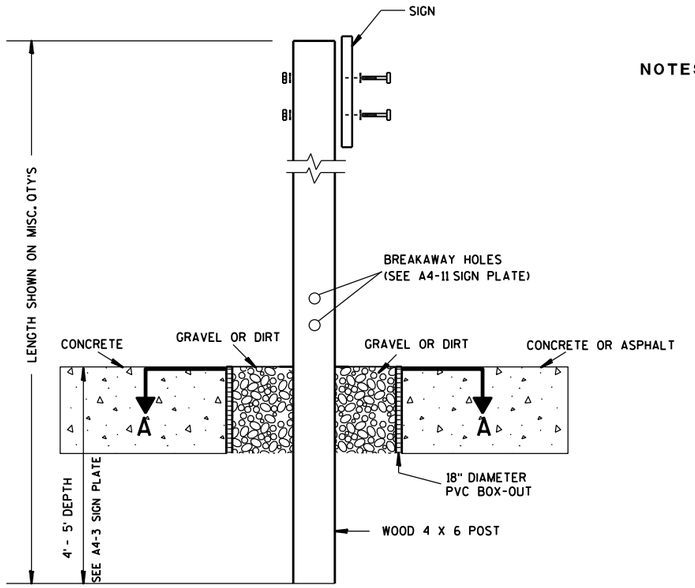
TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-3.23

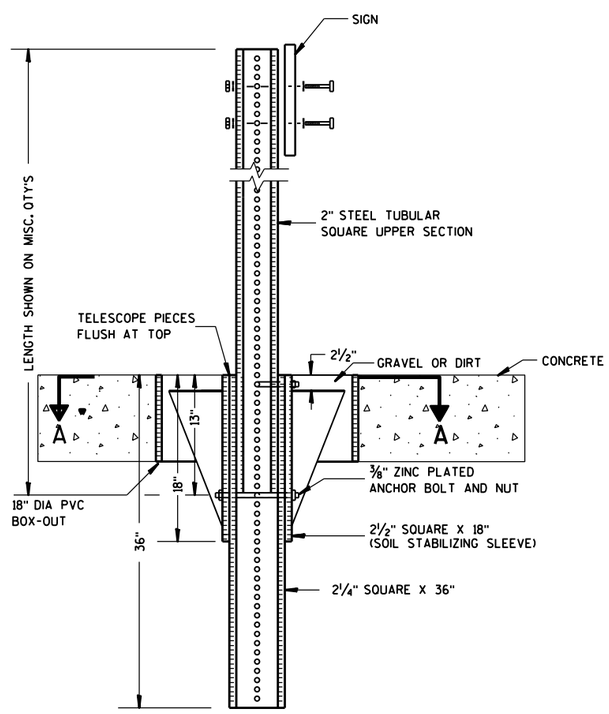
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 39	E



ELEVATION VIEW

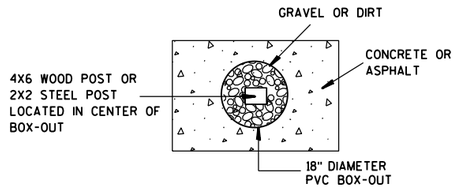
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

NOTES:
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

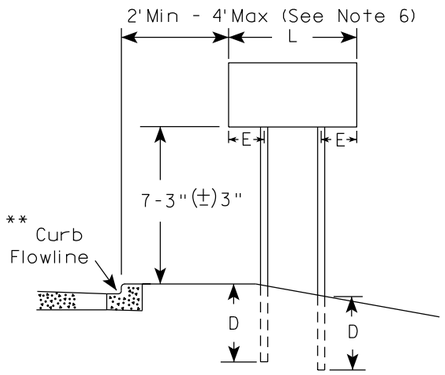
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

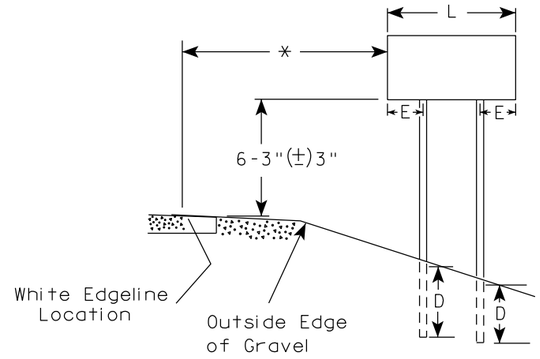
PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

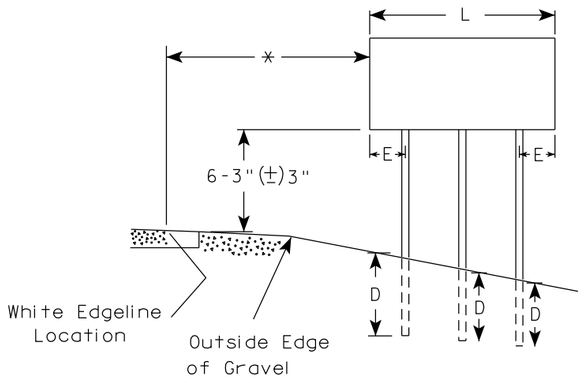
FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN PLOT DATE : 27-JAN-2014 09:48 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42

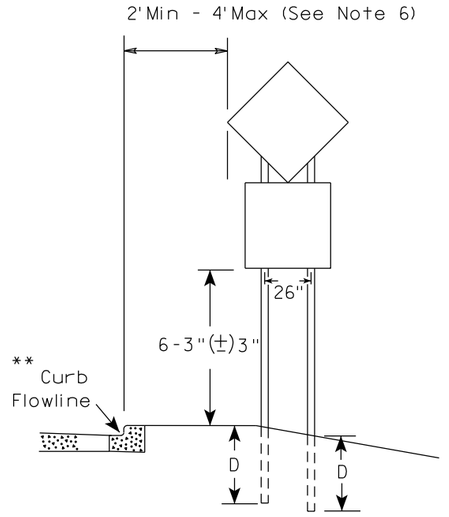
URBAN AREA



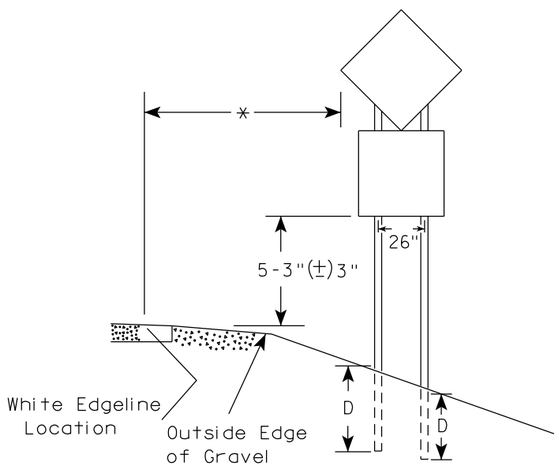
RURAL AREA (See Note 3)







48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)		
	L	E
***	Greater than 48" Less than 60"	12"
	60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) 3" or 6'-3" (±) 3" depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±) 3".

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

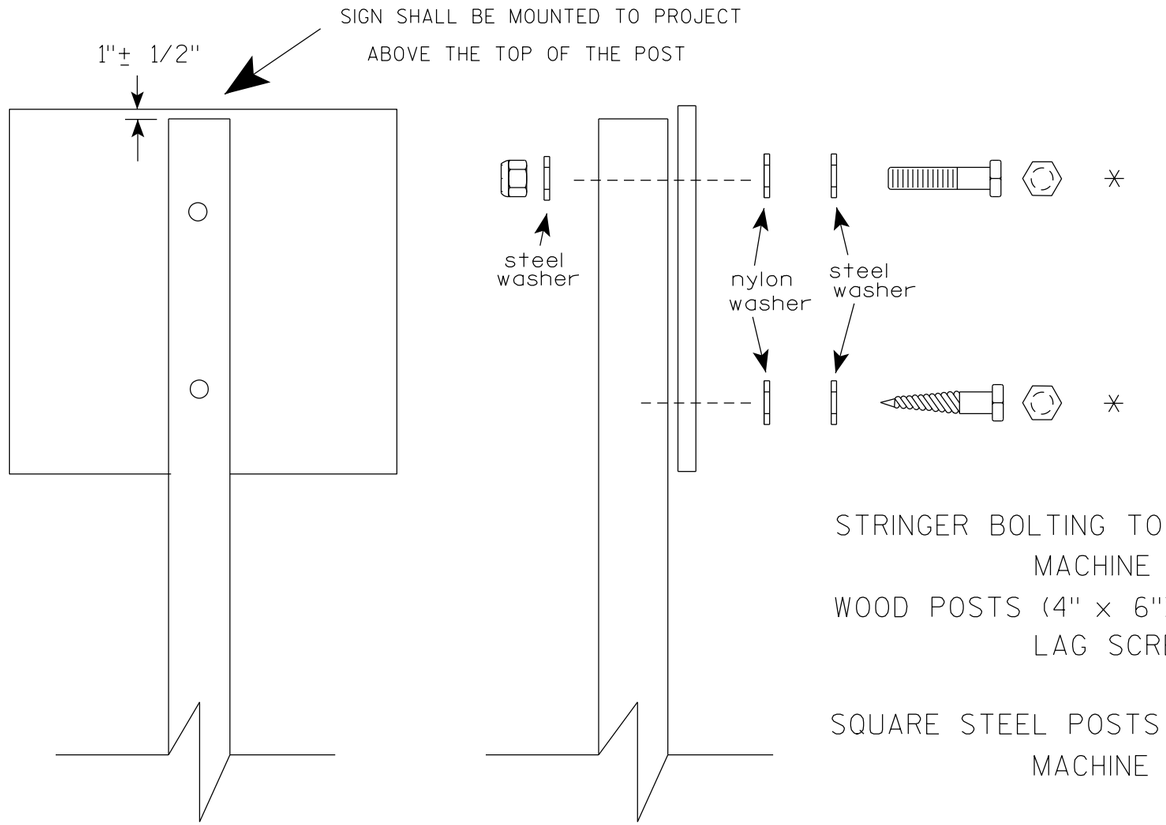
TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-4.16

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 41	E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3

b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

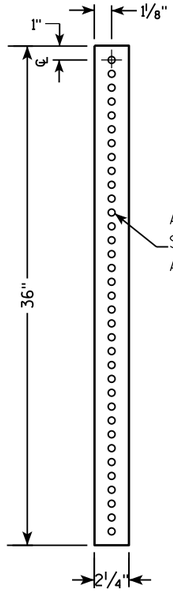
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

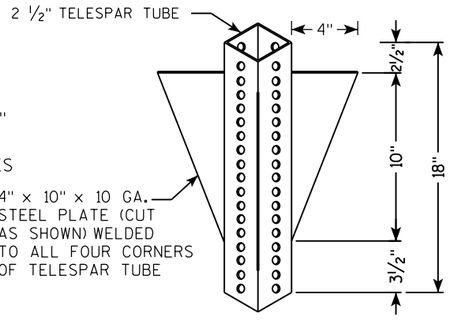
DATE 4/1/2020 PLATE NO. A4-8.9

TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM

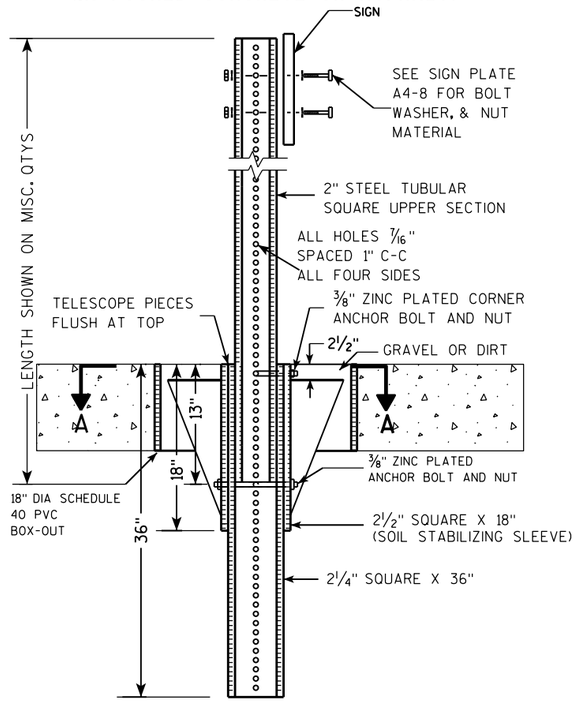
2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



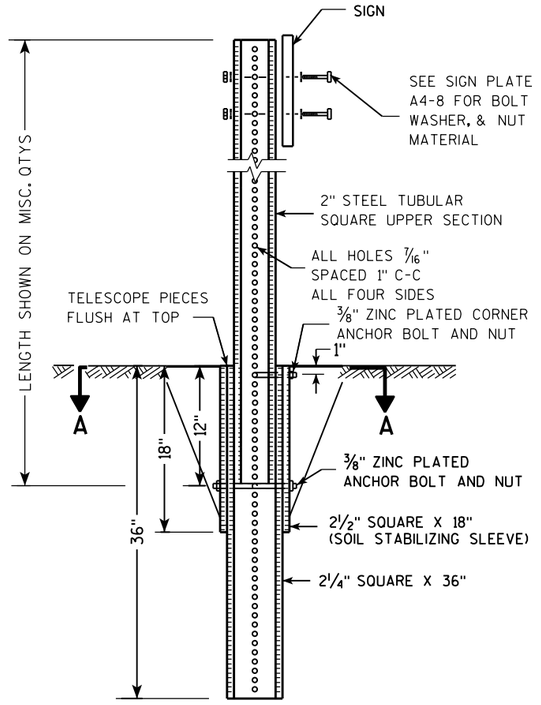
2 1/2 " SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH

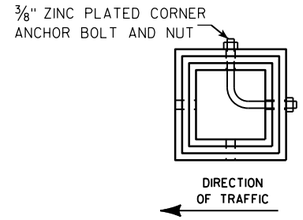


DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)





SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

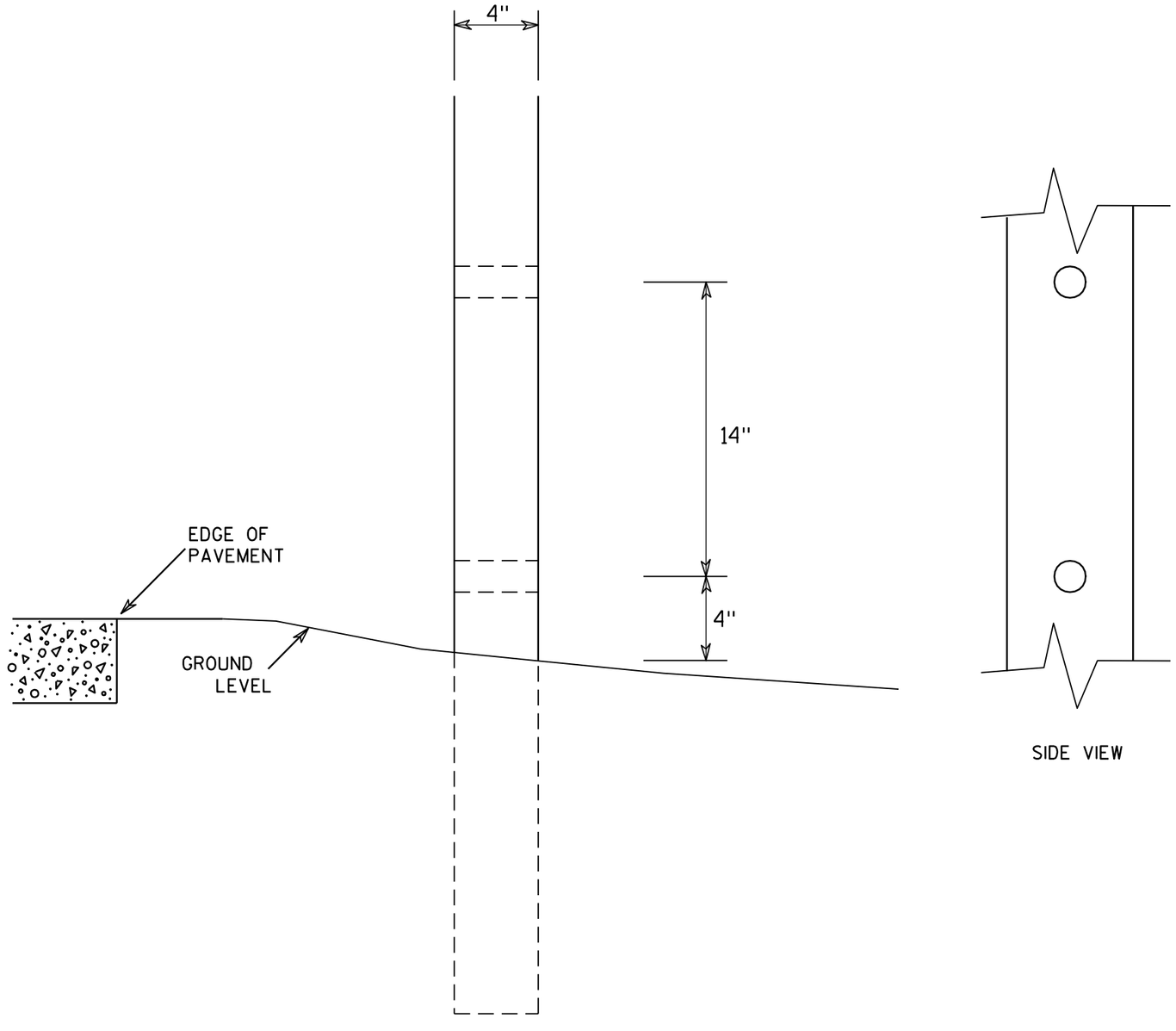
DATE 2/05/15 PLATE A4-9.9

43

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE : 05-FEB-2015 17:09 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000

WISDOT/CADDS SHEET 42



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

7

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

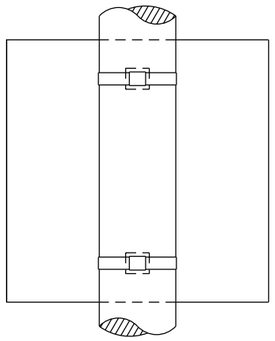
APPROVED Chester J Spang for State Traffic Engineer

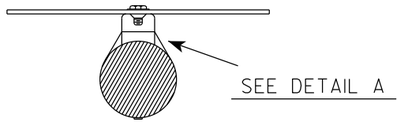
DATE 3/27/97 PLATE NO. A4-11.2

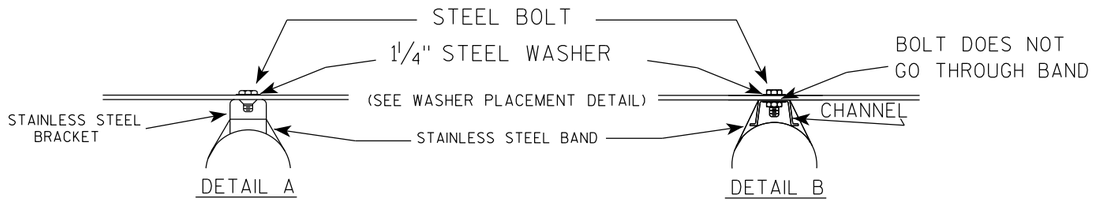
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 44	E

BANDING

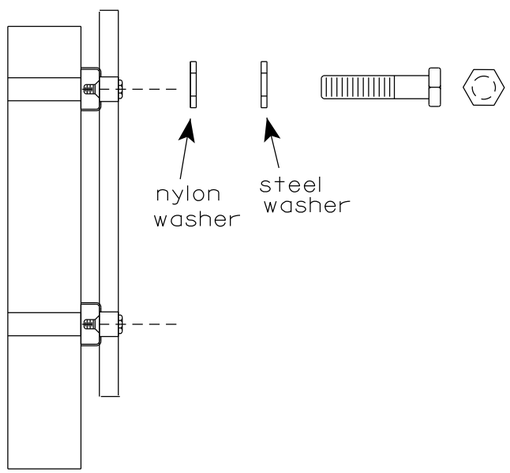
SINGLE SIGN







WASHER PLACEMENT

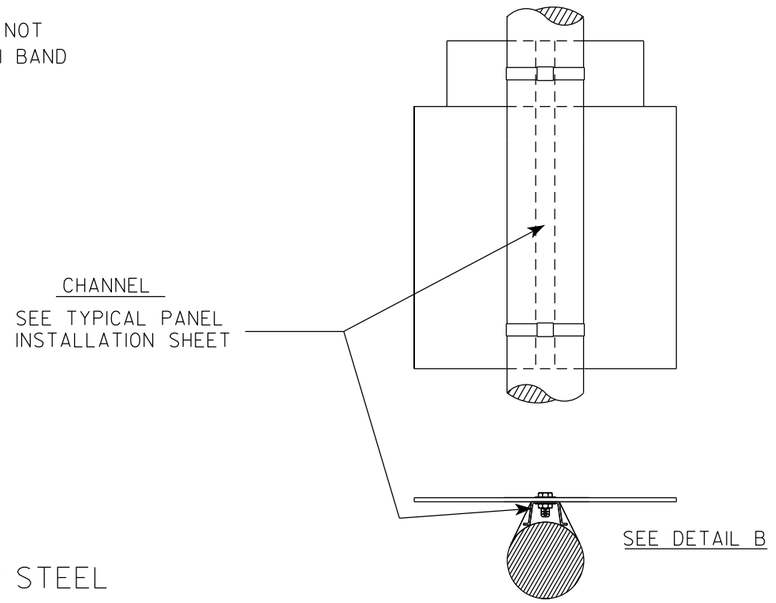


WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be ¾" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



STANDARD SIGN
SIGN BANDING DETAILS

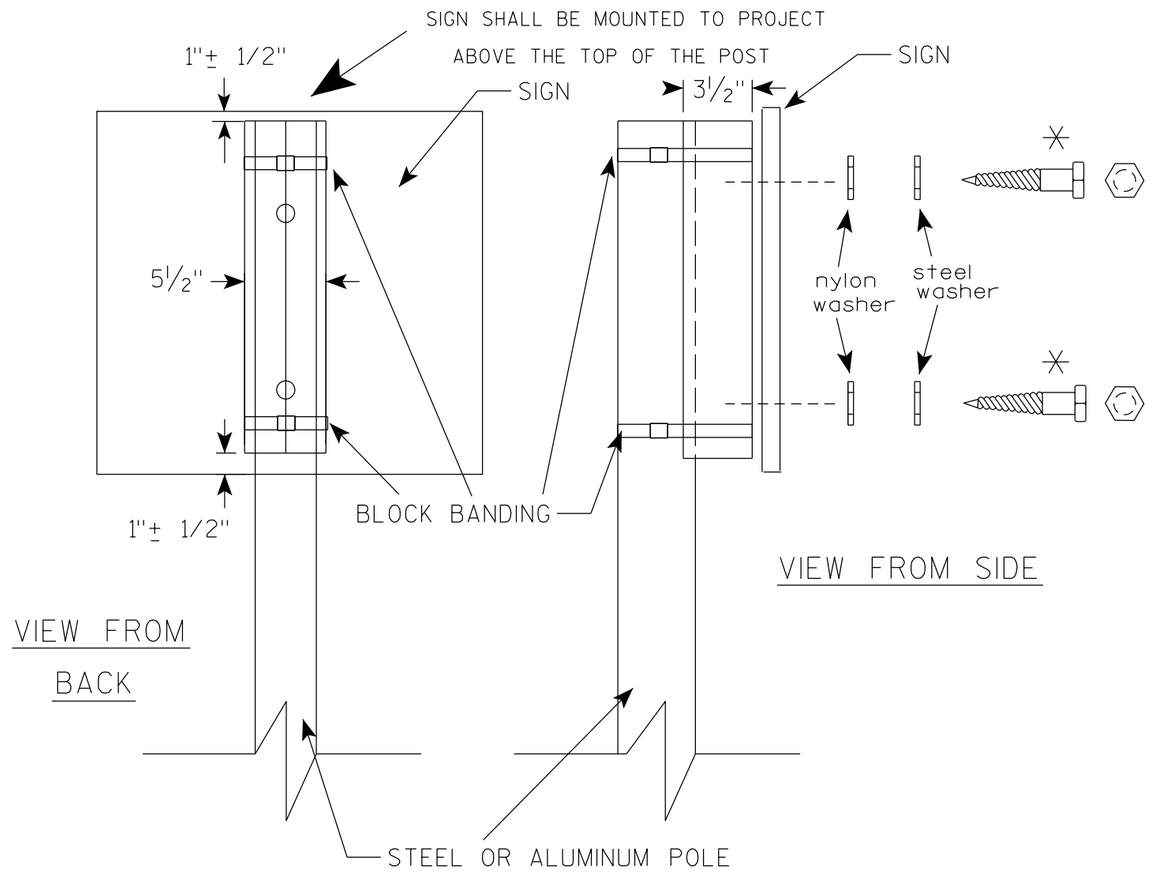
WISCONSIN DEPT OF TRANSPORTATION

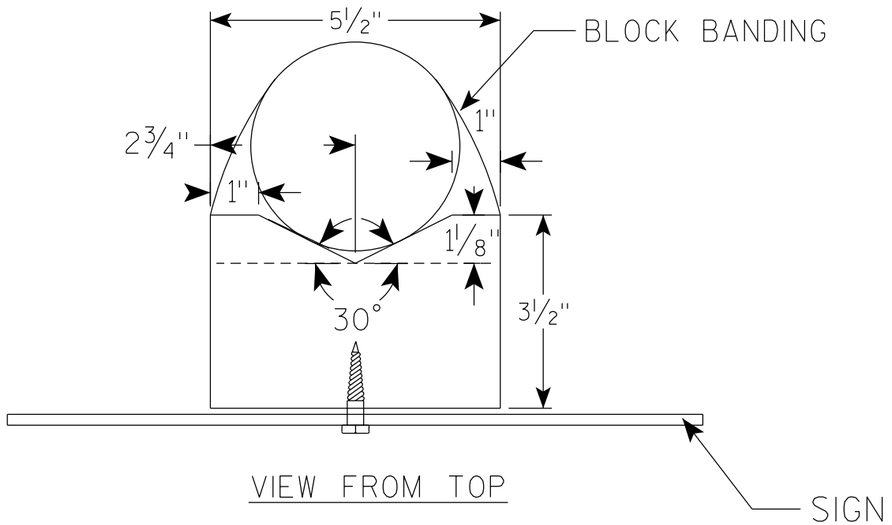
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 6/10/19 PLATE NO. A5-9.4

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 45	E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A59.dgn PLOT DATE : 10-JUN 2019 4:10 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : $$.....plotscale.....$$





GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WisDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, ¾" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORNALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✳ LAG BOLTS SHALL BE ⅜" X 2½"

BLOCK BANDING DETAIL (V-BLOCK OPTION)

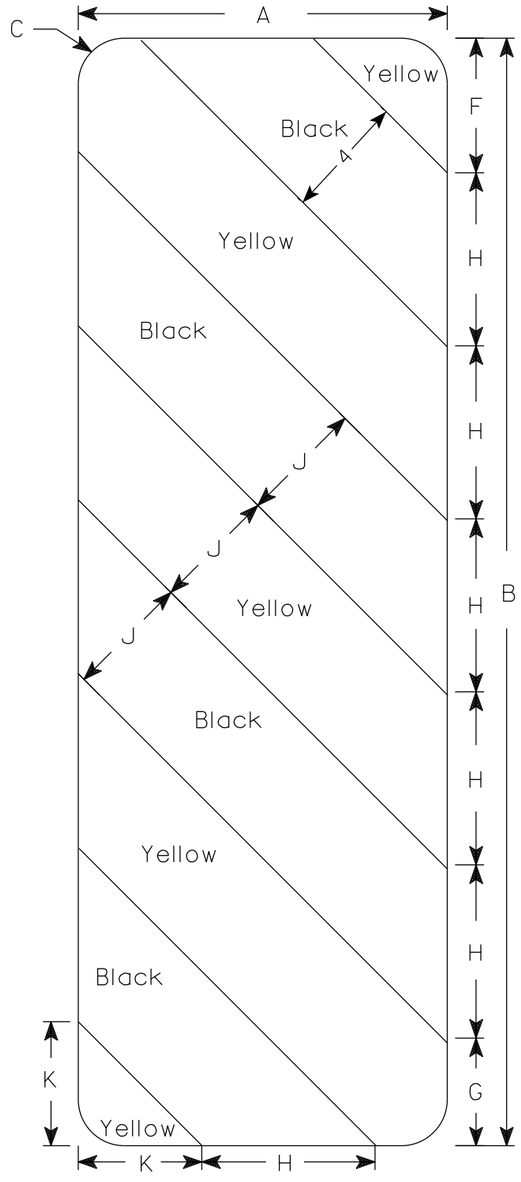
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

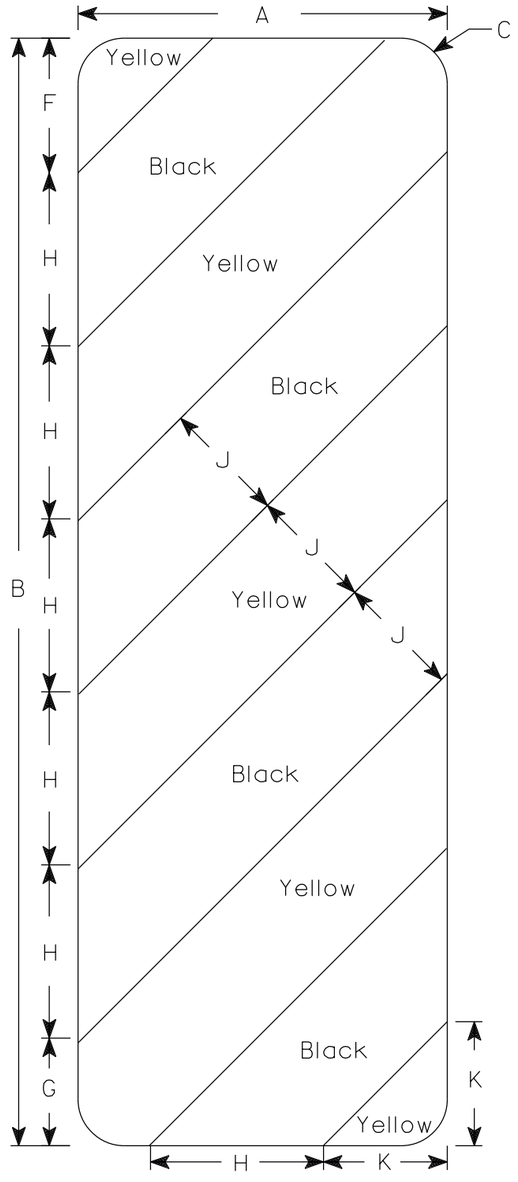
DATE 4/19/2022 PLATE NO. A5-10.3

PROJECT NO:

SHEET NO: 46 E



W5-52L



W5-52R

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Yellow
   Message - Black
3. Alternate colors of stripes as shown.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	36	1 1/2			4 3/8	3 1/2	5 5/8	45°	4	4																3.0
2M	12	36	1 1/2			4 3/8	3 1/2	5 5/8	45°	4	4																3.0
3	18	54	1 1/2			6	5 1/2	8 1/2	45°	6	6 9/16																6.75
4																											
5																											

STANDARD SIGN

W5-52L & W5-52R

WISCONSIN DEPT OF TRANSPORTATION

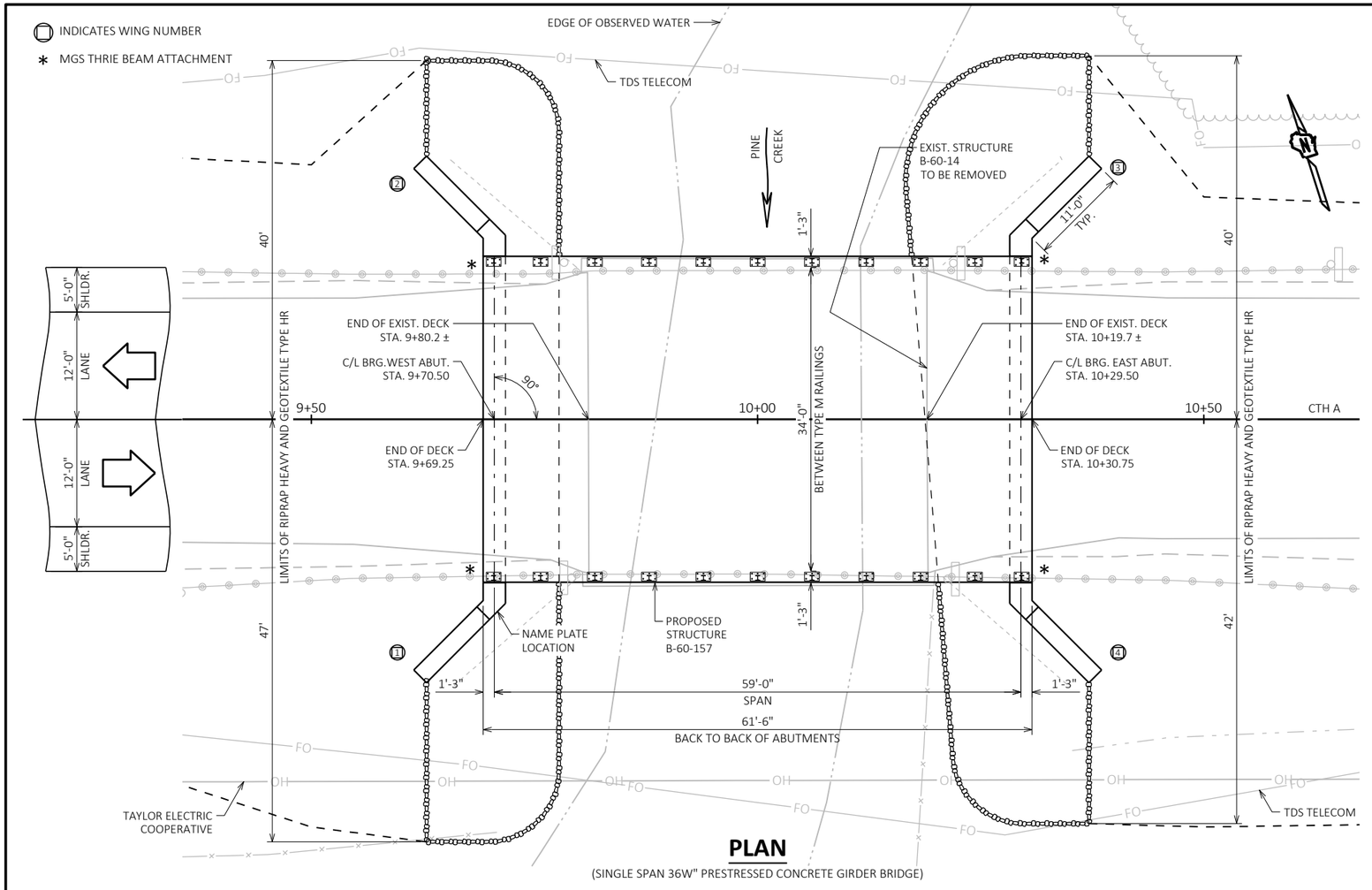
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/4/2024 PLATE NO. W5-52.10

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 47 | E

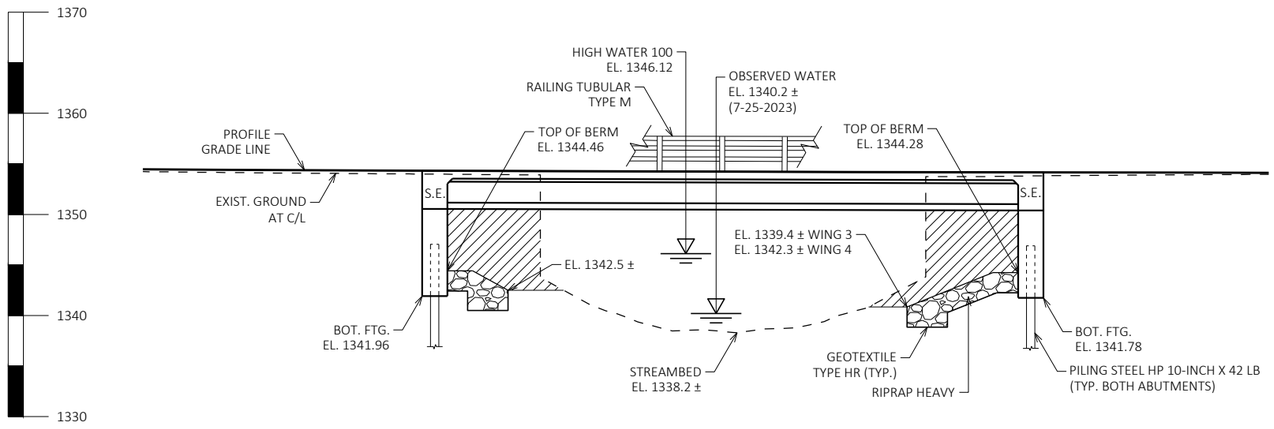
INDICATES WING NUMBER

* MGS THRIE BEAM ATTACHMENT



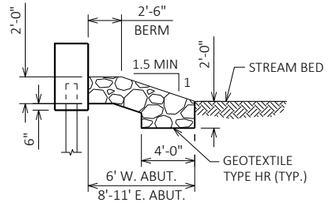
PLAN

(SINGLE SPAN 36W" PRESTRESSED CONCRETE GIRDER BRIDGE)



ELEVATION

(LOOKING NORTH)



RIPRAP DETAIL

AREA TO EXCAVATE INCLUDED IN "EXCAVATION FOR STRUCTURES BRIDGES B-60-157"

BENCH MARKS

NO.	STATION	DESCRIPTION	ELEV.
CP903	12+22.0, 11.92' LT.	MAG NAIL	1354.14
CP904	8+06.5, 11.38' LT.	MAG NAIL	1355.31

DESIGN DATA

STATE PROJECT NUMBER

8887-03-76

LIVE LOAD:

DESIGN LOADING: HL-93
INVENTORY RATING: RF = 1.09
OPERATING RATING: RF = 1.42
WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV): 250 (KIPS)

STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 POUNDS PER SQUARE FOOT.

MATERIAL PROPERTIES:

CONCRETE MASONRY:
SUPERSTRUCTURE —— f'_c = 4,000 PSI
ALL OTHER —— f'_c = 3,500 PSI

BAR STEEL REINFORCEMENT
GRADE 60 —— f_y = 60,000 PSI

36W" PRESTRESSED GIRDERS:
CONCRETE MASONRY —— f'_c = 8,000 PSI
STRANDS: 0.6" DIA. WITH ULTIMATE TENSILE STRENGTH OF 270,000 P.S.I.

FOUNDATION DATA

ABUTMENTS TO BE SUPPORTED ON HP 10-INCH X 42 LB PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 150 TONS ** PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA.
ESTIMATED 30' LONG AT WEST ABUT.
ESTIMATED 30' LONG AT EAST ABUT.

**THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS THE REQUIRED DRIVING RESISTANCE MULTIPLIED BY A RESISTANCE FACTOR OF 0.5 USING MODIFIED GATES TO DETERMINE PILE CAPACITY.

HYDRAULIC DATA

100-YEAR FREQUENCY:

Q_{100}= 1,845 C.F.S.
V_{100}= 7.9 F.P.S.
HW_{100}= EL. 1346.12
WATERWAY AREA = 233 SQ. FT.
DRAINAGE AREA = 15.0 SQ. MI.
ROADWAY OVERTOPPING = N/A
SCOUR CRITICAL CODE = 5

2-YEAR FREQUENCY:

Q_2= 535 C.F.S.
V_2= 3.5 F.P.S.
HW_2= EL. 1343.86

TRAFFIC DATA

CTH A

ADT = 640 (2025)
ADT = 608 (2045)
R.D.S. = 55 MPH

LIST OF DRAWINGS:

1. GENERAL PLAN
2. CROSS SECTION & QUANTITIES
3. SUBSURFACE EXPLORATION
4. WEST ABUTMENT
5. WEST ABUTMENT DETAILS
6. EAST ABUTMENT
7. EAST ABUTMENT DETAILS
8. 36" PRESTRESSED GIRDER DETAILS - 1
9. 36" PRESTRESSED GIRDER DETAILS - 2
10. STEEL DIAPHRAGMS
11. SUPERSTRUCTURE
12. SUPERSTRUCTURE DETAILS
13. RAILING TUBULAR TYPE M

WISCONSIN
ERIC T. PRICE
E-39027
MADISON
WI
PROFESSIONAL ENGINEER

May 31, 2024

NO.	DATE	REVISION	BY

CORRE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORATION

ACCEPTED SDR 08/14/24
CHIEF STRUCTURES DESIGN ENGINEER — DATE

STRUCTURE B-60-157

CTH A OVER PINE CREEK

COUNTY: TAYLOR | TOWN/~~CITY/VILLAGE~~: HOLWAY

DESIGN SPEC.
AASHTO LRFD BRIDGE DESIGN SPECIFICATION

DESIGNED BY	DESIGNED CK'D	DRAWN BY	PLANS CK'D
SMS	SAE	SMS	ETP

GENERAL PLAN

SHEET 1 OF 13
48

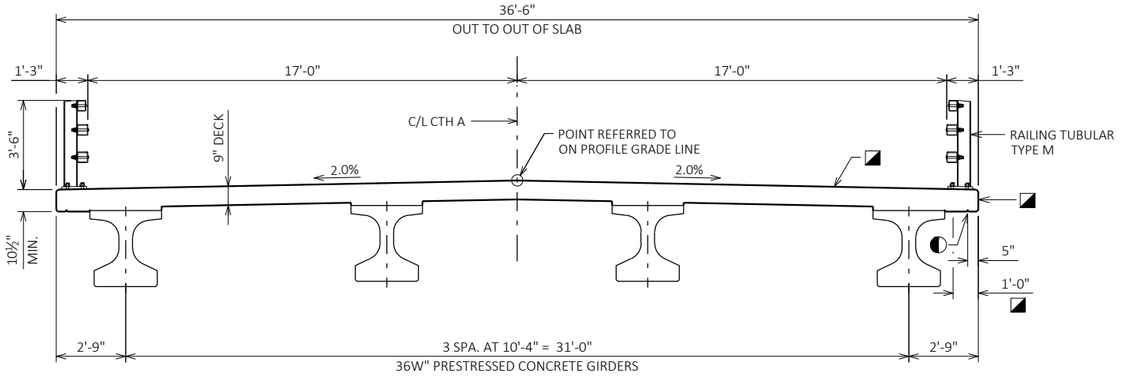
SCALE =

STRUCTURE DESIGN CONTACTS:

ERIC PRICE, P.E. 608-826-6146
AARON BONK, P.E. 608-261-0261

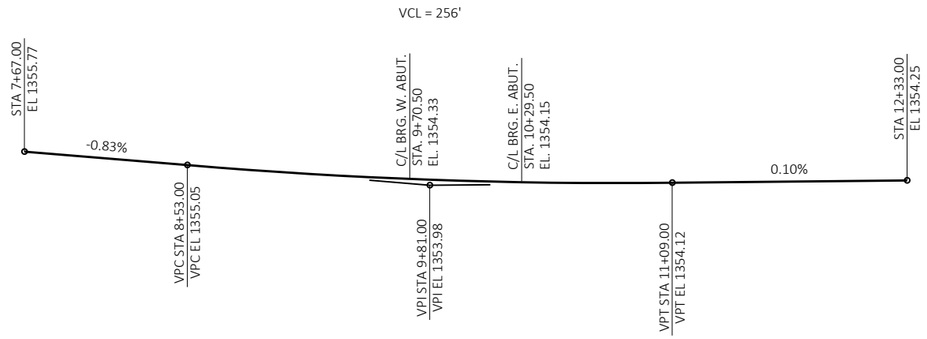
STATE PROJECT NUMBER

8887-03-76



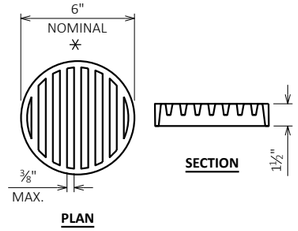
CROSS SECTION THRU BRIDGE

(LOOKING EAST)



PROFILE GRADE LINE

(CTH A)

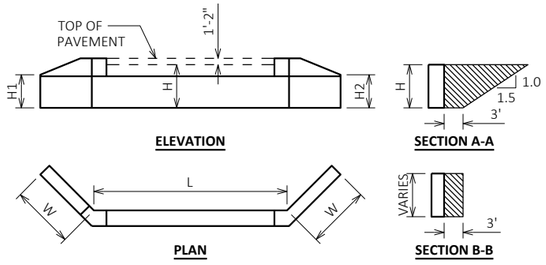


RODENT SHIELD DETAIL

⚹ DIMENSIONS ARE APPROXIMATE. THE GRATE IS SIZED TO FIT INTO A PIPE COUPLING. ORIENT SO SLOTS ARE VERTICAL.

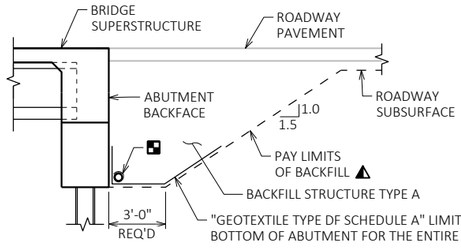
THE RODENT SHIELD, PIPE COUPLING AND SCREWS SHALL BE CONSIDERED INCIDENTAL TO THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6-INCH".

THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALLY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE EXPOSED END OF THE PIPE UNDERDRAIN. THE SHIELD SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1-INCH STAINLESS STEEL SHEET METAL SCREWS.



ABUTMENT BACKFILL DIAGRAM

L = ABUTMENT BODY LENGTH AT BACKFACE (FT)
H = AVERAGE ABUTMENT FILL HEIGHT (FT)
H1 = WING 1 HEIGHT AT TIP (FT)
H2 = WING 2 HEIGHT AT TIP (FT)
W = WING LENGTH (FT)
EF = EXPANSION FACTOR (1.20 FOR CY BID ITEMS AND 1.00 FOR TON BID ITEMS)
V_{CF} = (L)(3.0')(H) + (L)(0.5)(1.5H)(H) + (3')(0.5)(H1+H2+H+H)(W)
V_{CY} = V_{CF}(EF)/27
V_{TON} = V_{CY}(2.0)



TYPICAL SECTION THRU ABUTMENT

⚠ BACKFILL PAY LIMITS. BACKFILL BEYOND PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.

▪ PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN.

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEM	UNIT	WEST ABUTMENT	EAST ABUTMENT	SUPER.	TOTALS
203.0260	REMOVING STRUCTURE OVER WATERWAY MINIMAL DEBRIS B-60-14	EACH	--	--	--	1
206.1001	EXCAVATION FOR STRUCTURES BRIDGES B-60-157	EACH	--	--	--	1
210.1500	BACKFILL STRUCTURE TYPE A	TON	430	430	--	860
502.0100	CONCRETE MASONRY BRIDGES	CY	50.5	50.5	88.3	189
502.3200	PROTECTIVE SURFACE TREATMENT	SY	28	28	273	329
503.0137	PRESTRESSED GIRDER TYPE I 36W-INCH	LF	--	--	240	240
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	3,010	3,010	--	6,020
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	1,940	1,940	16,940	20,820
506.2605	BEARING PADS ELASTOMERIC NON-LAMINATED	EACH	4	4	--	8
506.4000	STEEL DIAPHRAGMS B-60-157	EACH	--	--	3	3
513.4061	RAILING TUBULAR TYPE M	LF	--	--	128	128
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	9	9	--	18
550.0500	PILE POINTS	EACH	8	8	--	16
550.1100	PILING STEEL HP 10-INCH X 42 LB	LF	230	230	--	460
606.0300	RIPRAP HEAVY	CY	80	105	--	185
612.0406	PIPE UNDERDRAIN WRAPPED 6-INCH	LF	110	110	--	220
645.0111	GEOTEXTILE TYPE DF SCHEDULE A	SY	60	60	--	120
645.0120	GEOTEXTILE TYPE HR	SY	140	175	--	315
	NON-BID ITEMS					
	FILLER	SIZE	--	--	--	1/2", 3/4"

ALL ITEMS ARE CATEGORY 0020

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BEVEL EXPOSED EDGES OF CONCRETE ¾" UNLESS OTHERWISE NOTED.

THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES B-60-157" SHALL BE THE EXISTING GROUNDLINE.

AT THE BACK FACE OF ABUTMENT ALL VOLUME WHICH CANNOT BE PLACED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL TYPE A.

EXCAVATION BELOW THE ABUTMENT AND ABUTMENT BEDDING MATERIALS REQUIRES ENGINEER APPROVAL. GEOTEXTILE SHALL BE SET AT THE BOTTOM OF EXCAVATION AND EXTEND 2'-0" ABOVE BOTTOM OF ABUTMENT.

THE QUANTITY FOR BACKFILL STRUCTURE IS CALCULATED BASED ON THE DETAIL SHOWN IN THE PLANS.

ELASTOMERIC BEARING PADS NEED NOT BE INDIVIDUALLY MOLDED PROVIDED THE CUT EDGES ARE SMOOTH AND TRUE.

THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH HEAVY RIPRAP AND GEOTEXTILE TYPE "HR" TO THE EXTENT SHOWN ON SHEET 1 AND THE ABUTMENT DETAILS.

THE HAUNCH CONCRETE QUANTITY IS BASED ON THE AVERAGE HAUNCH SHOWN ON THE "36W" PRESTRESSED GIRDER DETAILS" SHEETS.

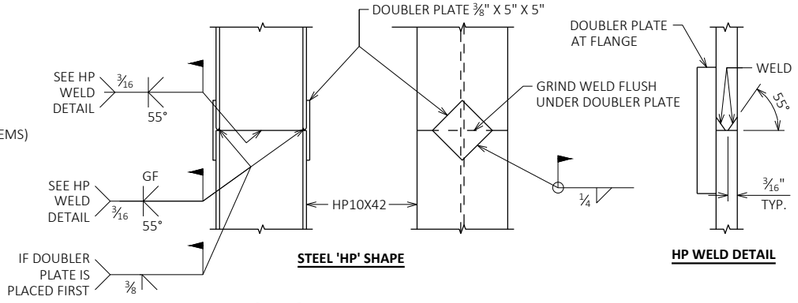
PROTECTIVE SURFACE TREATMENT IS TO BE APPLIED TO THE TOP OF DECK AND WINGS, THE DECK EDGE AND UNDERSIDE OF DECK AS SHOWN, THE EXTERIOR EXPOSED FACE OF WINGS, AND THE END 1'-0' OF THE ABUTMENT FRONT FACES.

THE EXISTING STRUCTURE B-60-14, TO BE REMOVED, IS A SINGLE SPAN CONCRETE FLAT SLAB BRIDGE, 39.5 FT. LONG WITH A 34.0 FT. CLEAR ROADWAY WIDTH.

LEGEND

◐ ¾" V-GROOVE REQ'D. EXTEND 6" FROM F.F. OF ABUTMENT BODY.

◪ COAT WITH "PROTECTIVE SURFACE TREATMENT" AS PER THE STANDARD SPECIFICATIONS. PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO THE TOP AND EXTERIOR EXPOSED FACE OF WINGS, AND THE END 1'-0" OF THE FRONT FACE OF ABUTMENT.



'HP' PILE DETAILS

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-60-157

DRAWN BY PKF | PLANS CK'D ETP

CROSS SECTION & QUANTITIES

SHEET 2 OF 13

49

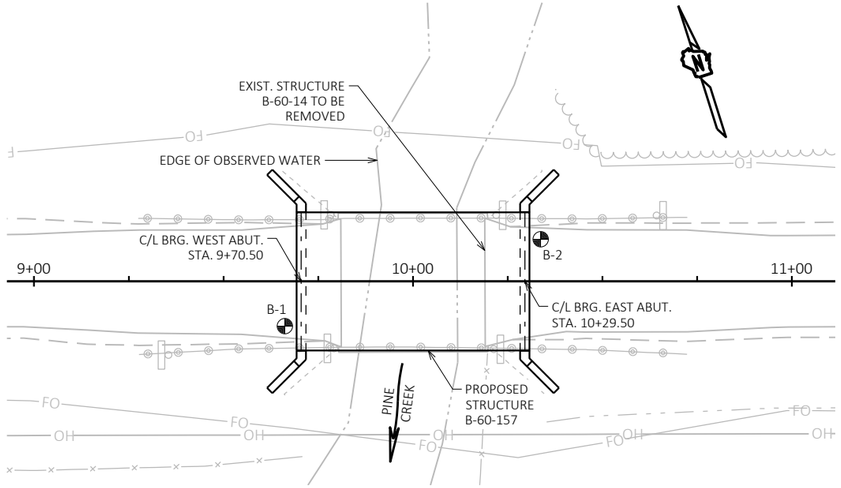
SCALE =

8

8

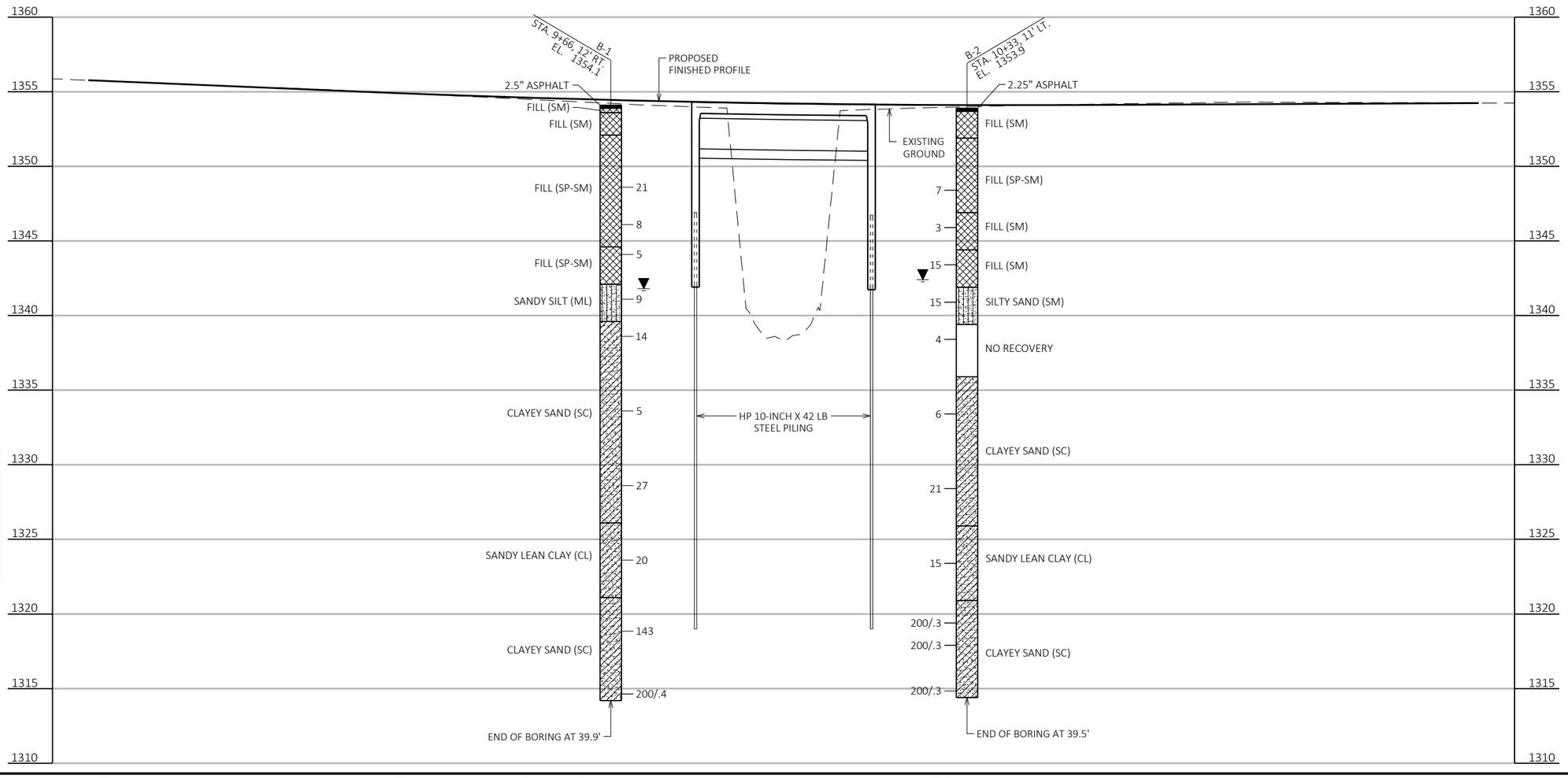
STATE PROJECT NUMBER

8887-03-76



SOIL BORINGS COMPLETED BY:

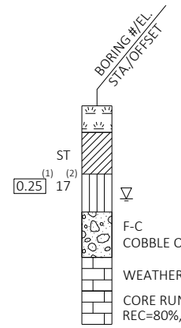
AMERICAN ENGINEERING TESTING
1837 COUNTY HIGHWAY OO
CHIPPEWA FALLS, WI 54729
(715) 861-5045
MARCH 8, 2023 (B-1)
MARCH 8, 2023 (B-2)



MATERIAL SYMBOLS

ASPHALT	TOPSOIL	PEAT
CONCRETE	FILL	GRAVEL
SAND	CLAY	SILT
BOULDERS OR COBBLES	LIMESTONE	BEDROCK (UNKNOWN)
SHALE	SANDSTONE	IGNEOUS/ META

LEGEND OF BORING



(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)

(2) UNLESS OTHERWISE, SPECIFIED THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION

- AT TIME OF DRILLING
- END OF DRILLING
- AFTER DRILLING

ABBREVIATIONS

F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-60-157

DRAWN BY PFK | PLANS CK'D ETP

SUBSURFACE EXPLORATION

SHEET 3 OF 13

50

SCALE =

8

8

FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\503_STRUCTURES\503.3_FINAL\080301_SX_CTH A.DWG PLOT DATE : 5/16/2024 8:24 AM PLOT BY : SAM SWITTEL

STATE PROJECT NUMBER

8887-03-76

LEGEND

(1) INDICATES WING NUMBER

(2) INDICATES GIRDER NUMBER

(A02) OPTIONAL CONST. JOINT: KEYWAY FORMED BY A BEVELED 2 x 6 (18" RMW @ B.F. & "V" GROOVE @ F.F. IF JOINT IS USED, INCIDENTAL TO "CONCRETE MASONRY").

(A05) ¾" CORK FILLER UP VERT. BEAM SEAT FACES THAT RUN PARALLEL WITH GIRDER.

(A08) SEE BEARING PAD DETAIL ON SUPERSTRUCTURE DETAILS SHEET.

(A09) SUPPORT ABUTMENT ON HP 10-INCH X 42 LB PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 150 TONS PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED 30' LONG.

(A10) PULL UP TO 2" CLEAR

(A11) ELEVATION SHOWN AT TOP OF CONCRETE AT C/L OF BRG.

(A15) PIPE UNDERDRAIN WRAPPED 6-INCH. SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SCREEN AT ENDS OF PIPE UNDERDRAIN. RODENT SHIELD TO BE INCLUDED IN BID PRICE OF "PIPE UNDERDRAIN WRAPPED 6-INCH".

(A17) ½" FILLER: SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF ½" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE).

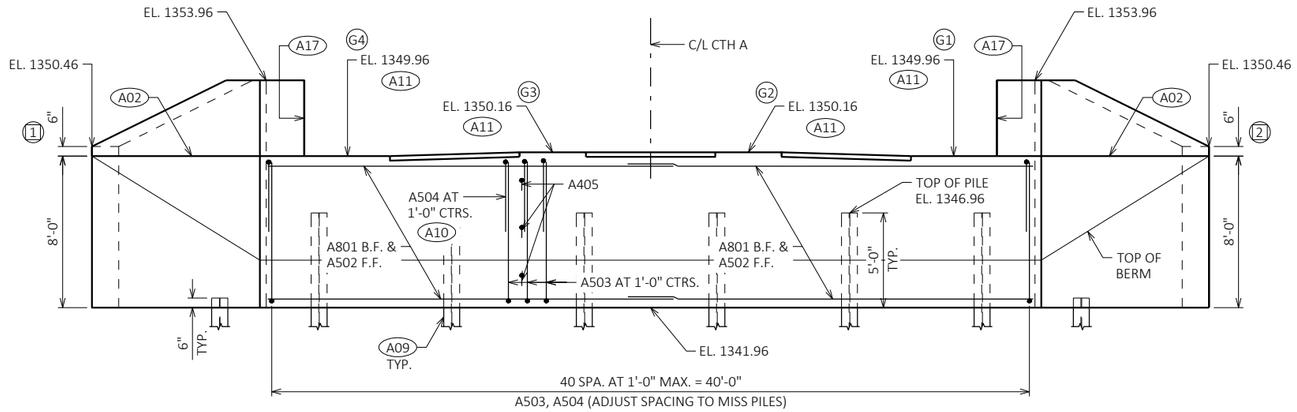
(A19) 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACKFACE.

(A20) STEEL TROWEL TOP SURFACE OF ABUTMENT. PLACE MULTIPLE LAYERS OF POLYETHYLENE SHEETS OVER ENTIRE ABUTMENT TOP BEFORE PLACING BEARING PADS. TOTAL THICKNESS OF SHEETS SHALL BE AT LEAST 0.03".

B.F. DENOTES BACK FACE
F.F. DENOTES FRONT FACE
E.F. DENOTES EACH FACE

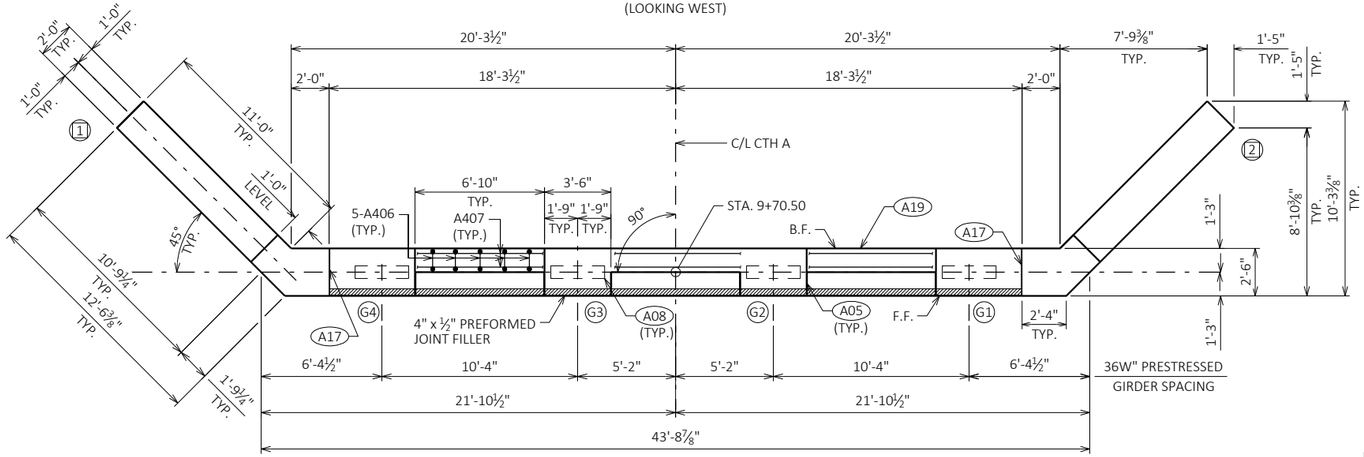
NOTE

DO NOT PLACE FILL ABOVE 3'-0" FROM BOTTOM OF ABUTMENT UNTIL SUPERSTRUCTURE IS IN PLACE.

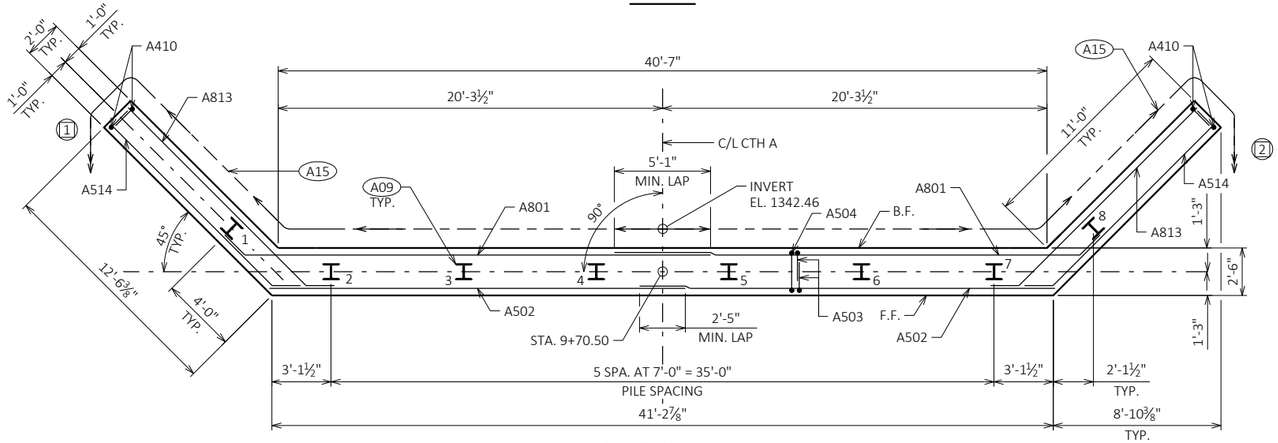


ELEVATION

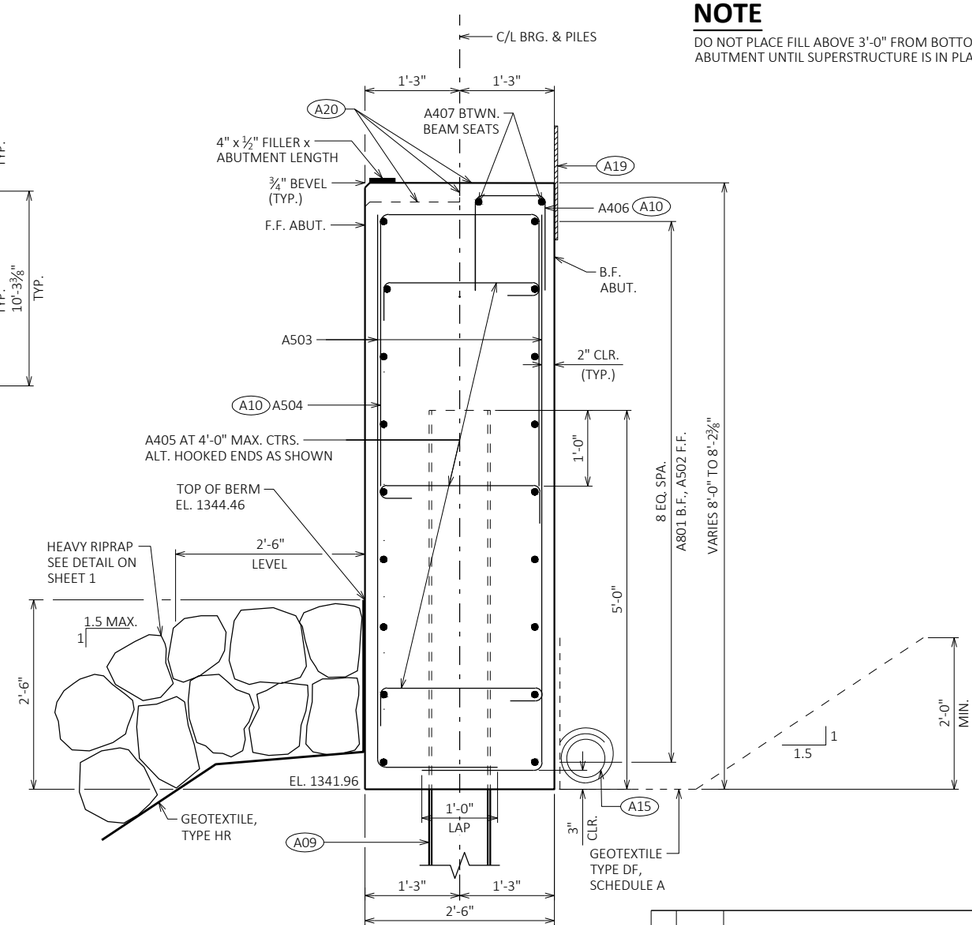
(LOOKING WEST)



PLAN



PILE PLAN



SECTION THRU BODY

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-60-157

DRAWN BY SMS | PLANS CK'D ETP

WEST ABUTMENT

SHEET 4 OF 13

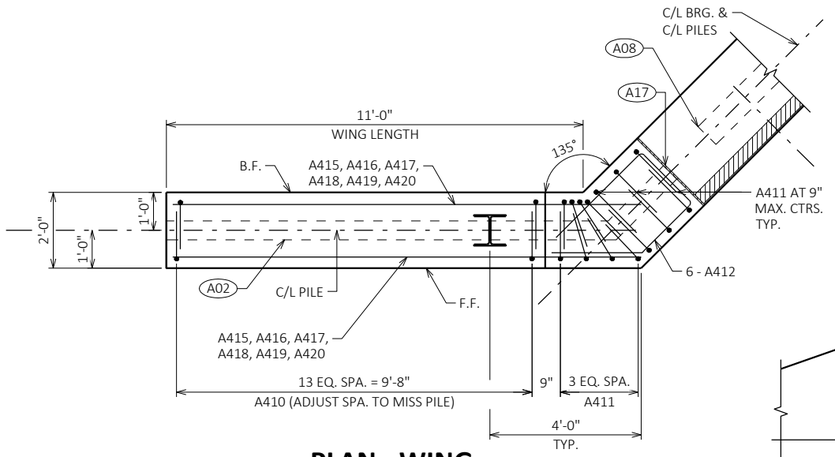
51

SCALE =

FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\503_STRUCTURES\503.3_FINAL\080401 ABT_CTH A.DWG PLOT DATE : 5/16/2024 8:24 AM PLOT BY : SAM SWITTEL

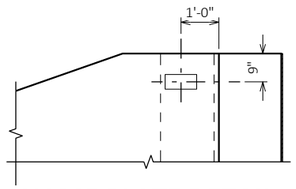
STATE PROJECT NUMBER

8887-03-76



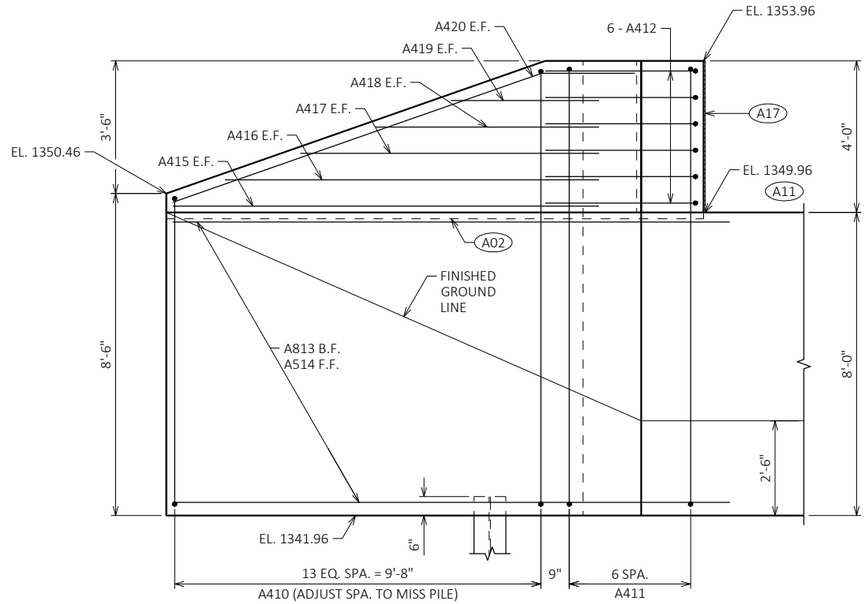
PLAN - WING

(WING 1 SHOWN, WING 2 SIMILAR)
HORIZONTAL REINF. BELOW SEAT NOT SHOWN
FOR CLARITY. SEE PILE PLAN.



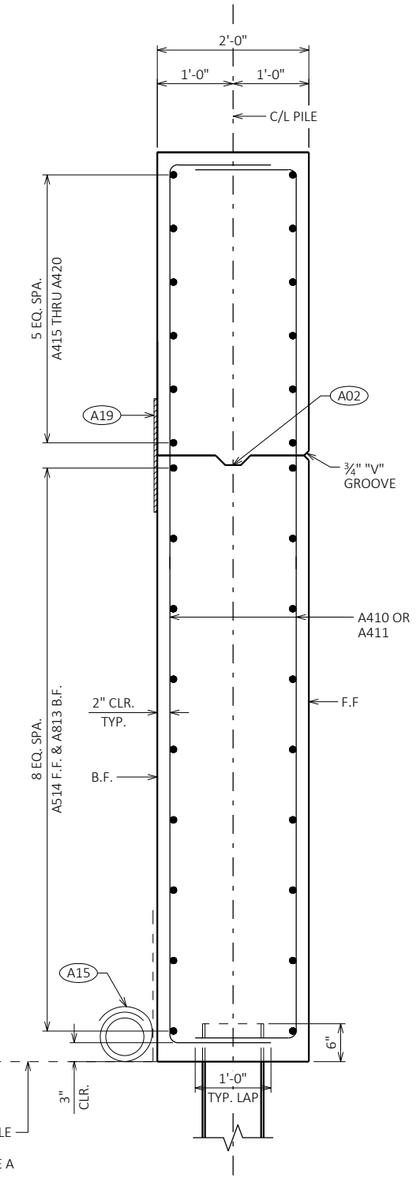
NAME PLATE DETAIL

(WING 1 ONLY)



ELEVATION - WING

(WING 1 SHOWN, WING 2 SIMILAR)



SECTION THRU WING

BILL OF BARS - WEST ABUTMENT

BAR MARK	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION	
NON-COATED BARS					TOTAL WEIGHT = 3,010 LBS	
A801	18	26'-7"	X		BODY - B.F.	HORIZ.
A502	18	22'-1"			BODY - F.F.	HORIZ.
A503	82	9'-1"	X		BODY - STIRRUPS	VERT.
A504	41	9'-7"	X		BODY - TOP	VERT.
A405	33	3'-0"	X		BODY - TIES	TRANS.
A406	15	3'-3"	X		BODY - NOTCH	VERT.
A407	6	8'-10"			BODY - NOTCH	HORIZ.
COATED BARS					TOTAL WEIGHT = 1,940 LBS	
A410	56	12'-4"	X	X	WINGS - E.F.	VERT.
A411	28	14'-7"	X		WINGS - E.F.	VERT.
A412	12	9'-3"	X		WINGS - CORNERS	HORIZ.
A813	18	15'-4"	X		WINGS - B.F.	HORIZ.
A514	18	13'-10"	X		WINGS - F.F.	HORIZ.
A415	4	11'-4"			WINGS - B.F. & F.F.	HORIZ.
A416	4	9'-8"			WINGS - B.F. & F.F.	HORIZ.
A417	4	7'-8"			WINGS - B.F. & F.F.	HORIZ.
A418	4	5'-8"			WINGS - B.F. & F.F.	HORIZ.
A419	4	3'-8"			WINGS - B.F. & F.F.	HORIZ.
A420	4	12'-9"	X		WINGS - TOP	VERT.

THE FIRST DIGIT OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.
DIMENSIONS IN BENDING DETAILS ARE OUT-TO-OUT OF BAR.

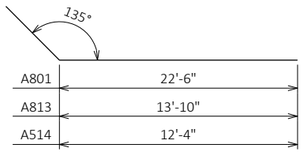
BAR SERIES TABLE

BAR MARK	NO. REQ'D	LENGTH
A410	4 SERIES OF 14	10'-8" TO 14'-0"

BUNDLE AND TAG EACH SERIES SEPARATELY.

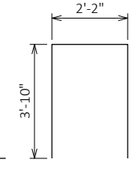
LEGEND

- A02 — OPTIONAL CONST. JOINT: KEYWAY FORMED BY A BEVELED 2 x 6 (18" RMW @ B.F. & "V" GROOVE @ F.F. IF JOINT IS USED, INCIDENTAL TO "CONCRETE MASONRY").
- A08 — SEE BEARING PAD DETAIL ON SUPERSTRUCTURE DETAILS SHEET.
- A11 — ELEVATION SHOWN AT TOP OF CONCRETE AT C/L OF BRG.
- A15 — PIPE UNDERDRAIN WRAPPED 6-INCH. SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SCREEN AT ENDS OF PIPE UNDERDRAIN. RODENT SHIELD TO BE INCLUDED IN BID PRICE OF "PIPE UNDERDRAIN WRAPPED 6-INCH".
- A17 — ½" FILLER: SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF ½" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE).
- A19 — 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACKFACE.



A801, A813, A514

A503



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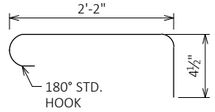

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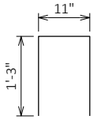

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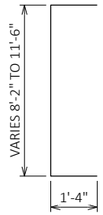
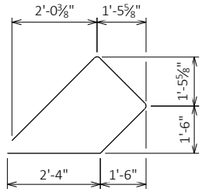


A410

A411



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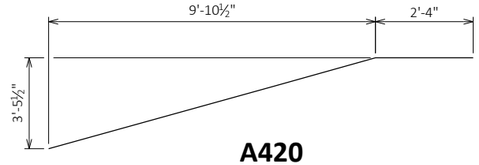


A420

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-60-157

DRAWN BY SMS | PLANS CK'D ETP

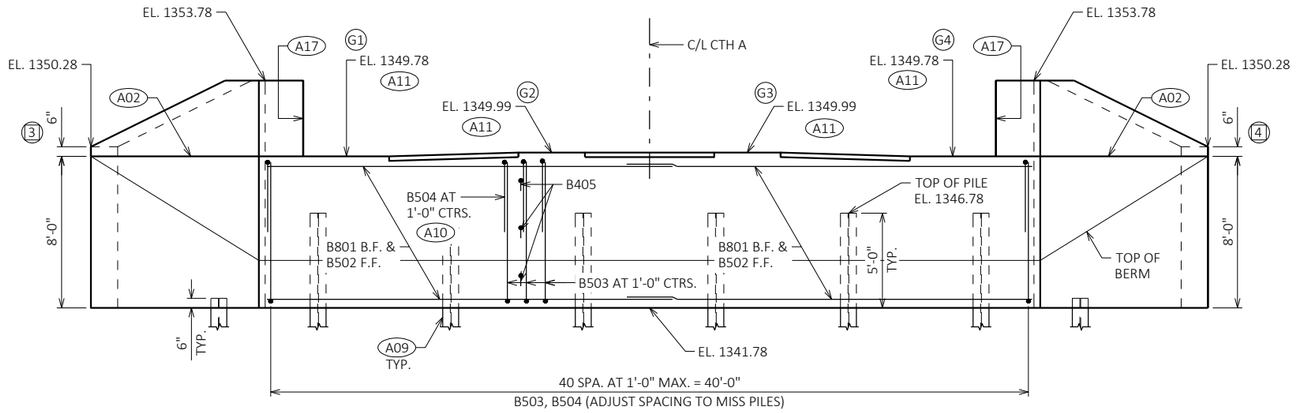
WEST ABUTMENT DETAILS

SHEET 5 OF 13
52

SCALE =

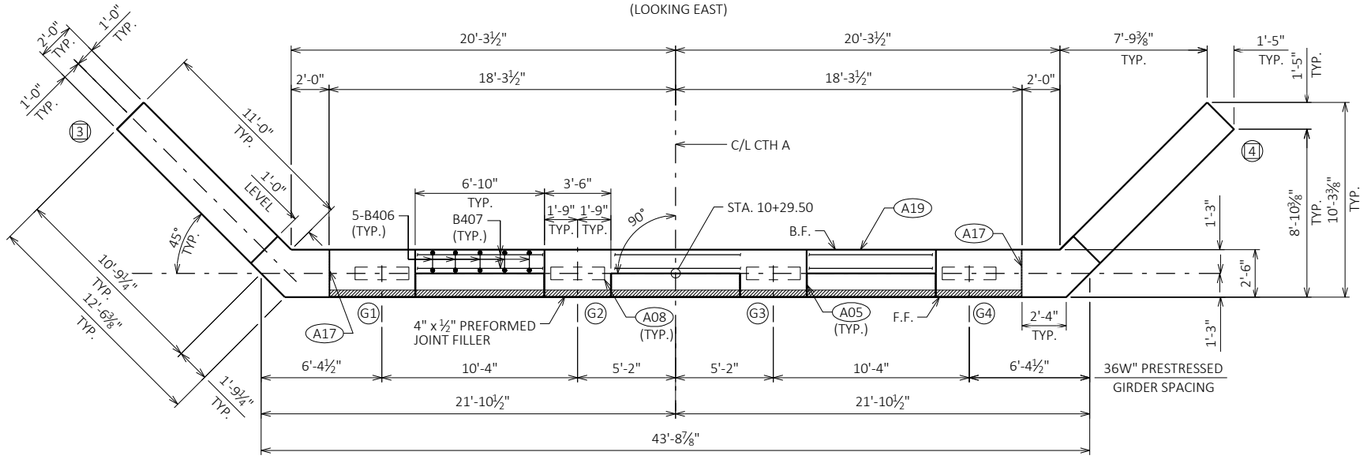
8

8

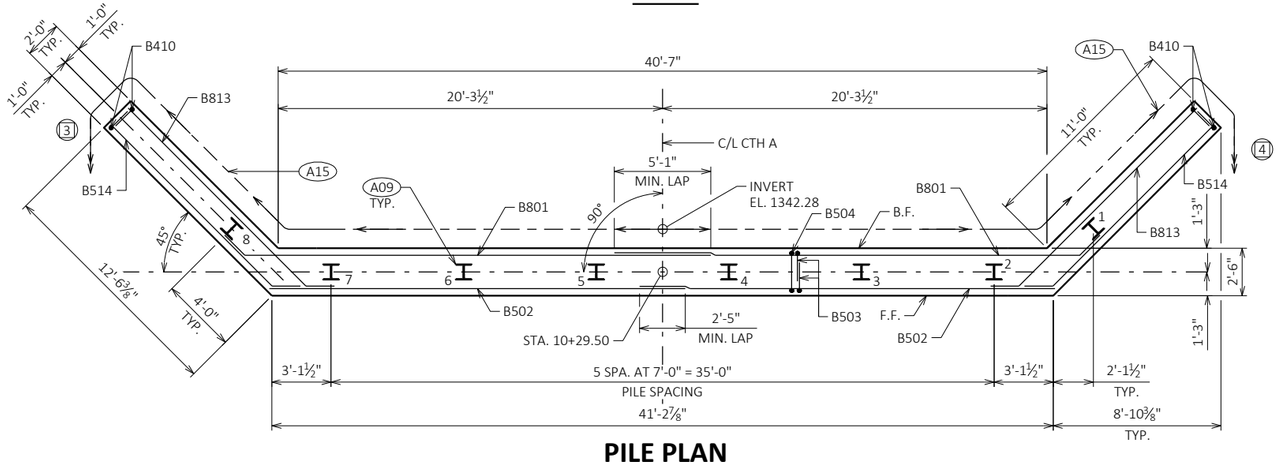


ELEVATION

(LOOKING EAST)



PLAN



PILE PLAN

LEGEND

- INDICATES WING NUMBER
- INDICATES GIRDER NUMBER

(A02) OPTIONAL CONST. JOINT: KEYWAY FORMED BY A BEVELED 2 x 6 (18" RMW @ B.F. & "V" GROOVE @ F.F. IF JOINT IS USED, INCIDENTAL TO "CONCRETE MASONRY").

(A05) ¾" CORK FILLER UP VERT. BEAM SEAT FACES THAT RUN PARALLEL WITH GIRDER.

(A08) SEE BEARING PAD DETAIL ON SUPERSTRUCTURE DETAILS SHEET.

(A09) SUPPORT ABUTMENT ON HP 10-INCH X 42 LB PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 150 TONS PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED 30' LONG.

(A10) PULL UP TO 2" CLEAR

(A11) ELEVATION SHOWN AT TOP OF CONCRETE AT C/L OF BRG.

(A15) PIPE UNDERDRAIN WRAPPED 6-INCH. SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SCREEN AT ENDS OF PIPE UNDERDRAIN. RODENT SHIELD TO BE INCLUDED IN BID PRICE OF "PIPE UNDERDRAIN WRAPPED 6-INCH".

(A17) ½" FILLER: SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF ½" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE).

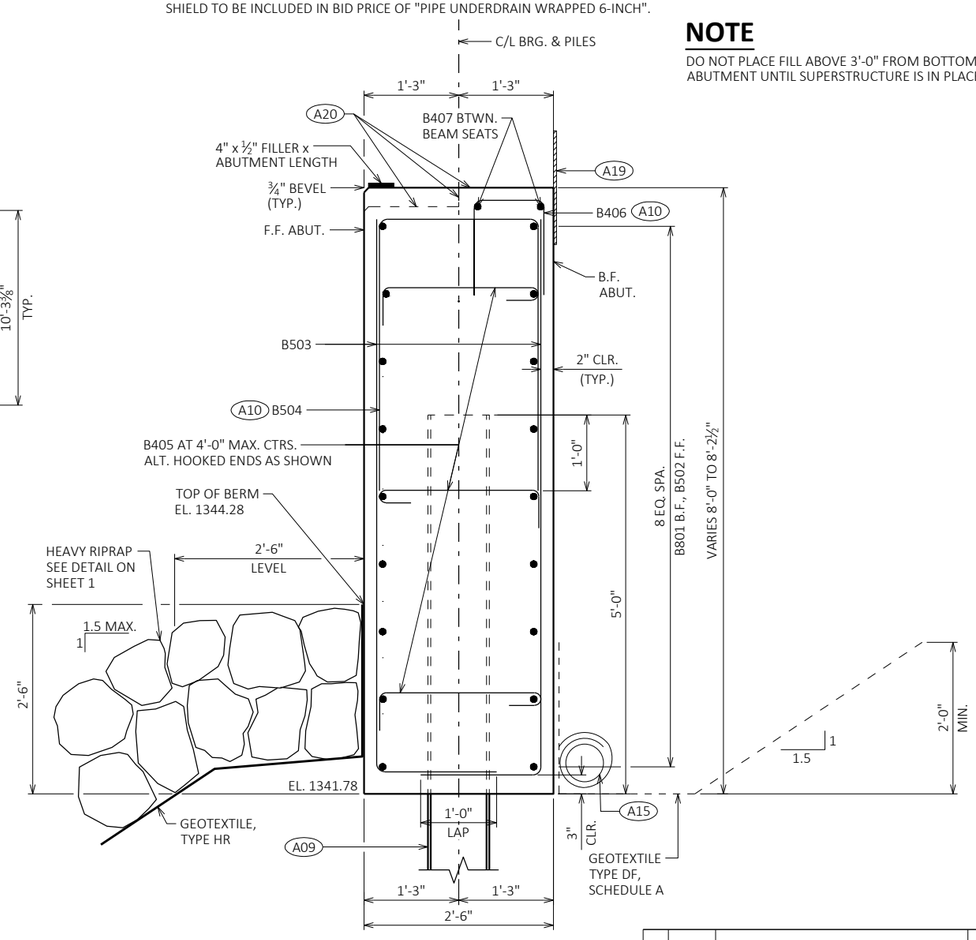
(A19) 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACKFACE.

(A20) STEEL TROWEL TOP SURFACE OF ABUTMENT. PLACE MULTIPLE LAYERS OF POLYETHYLENE SHEETS OVER ENTIRE ABUTMENT TOP BEFORE PLACING BEARING PADS. TOTAL THICKNESS OF SHEETS SHALL BE AT LEAST 0.03".

B.F. DENOTES BACK FACE
F.F. DENOTES FRONT FACE
E.F. DENOTES EACH FACE

NOTE

DO NOT PLACE FILL ABOVE 3'-0" FROM BOTTOM OF ABUTMENT UNTIL SUPERSTRUCTURE IS IN PLACE.



SECTION THRU BODY

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-60-157

DRAWN BY SMS | PLANS CK'D ETP

EAST ABUTMENT

SHEET 6 OF 13
53

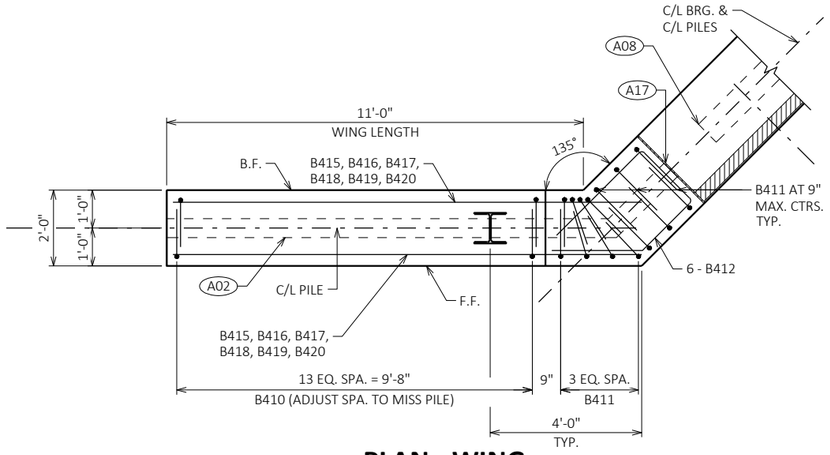
SCALE =

8

8

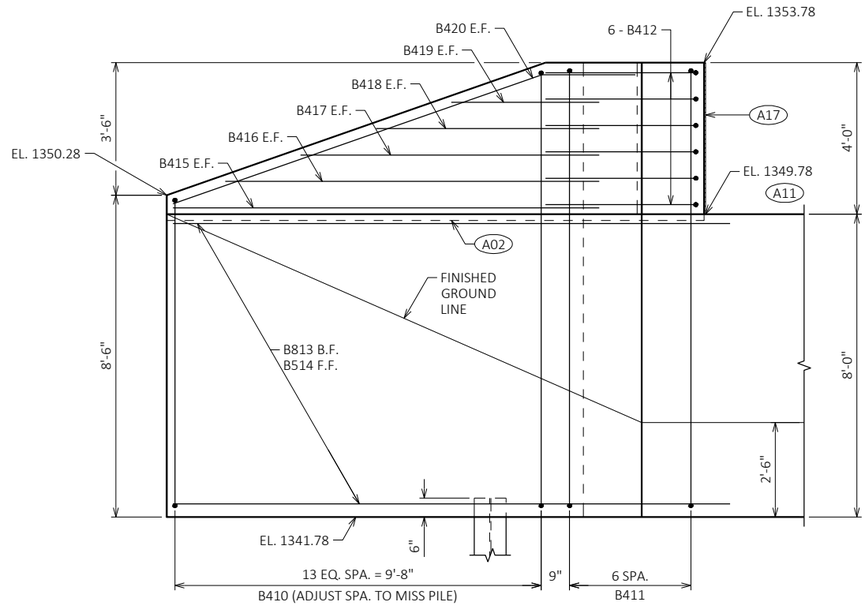
STATE PROJECT NUMBER

8887-03-76



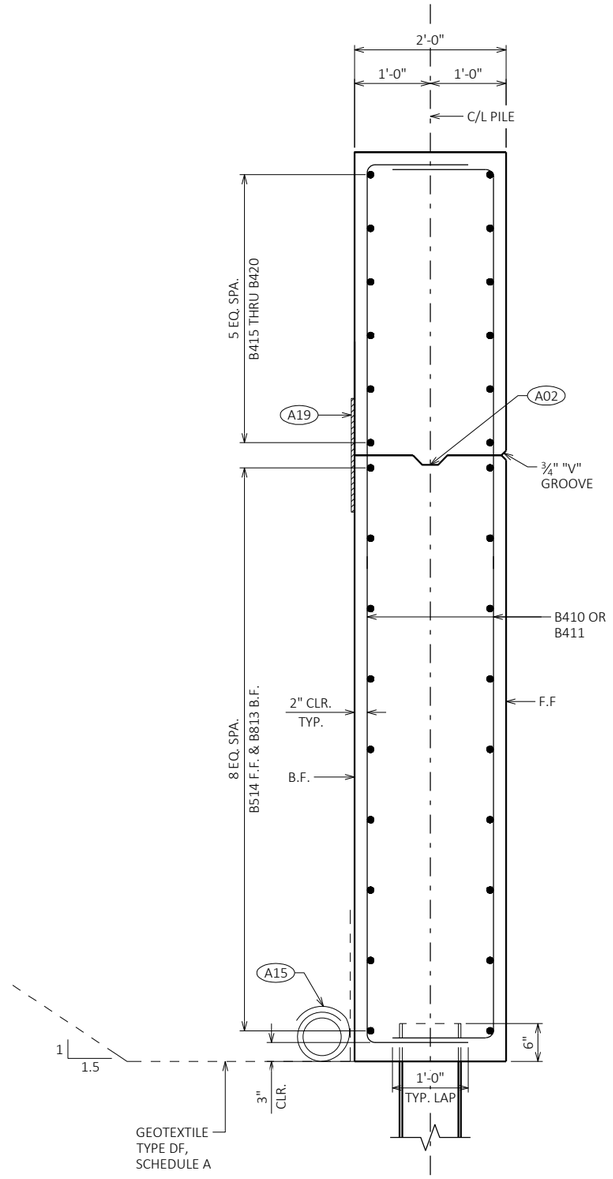
PLAN - WING

(WING 3 SHOWN, WING 4 SIMILAR)
HORIZONTAL REINF. BELOW SEAT NOT SHOWN
FOR CLARITY. SEE PILE PLAN.



ELEVATION - WING

(WING 3 SHOWN, WING 4 SIMILAR)



SECTION THRU WING

BILL OF BARS - EAST ABUTMENT

BAR MARK	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION	
NON-COATED BARS					TOTAL WEIGHT = 3,010 LBS	
B801	18	26'-7"	X		BODY - B.F.	HORIZ.
B502	18	22'-1"			BODY - F.F.	HORIZ.
B503	82	9'-1"	X		BODY - STIRRUPS	VERT.
B504	41	9'-7"	X		BODY - TOP	VERT.
B405	33	3'-0"	X		BODY - TIES	TRANS.
B406	15	3'-3"	X		BODY - NOTCH	VERT.
B407	6	8'-10"			BODY - NOTCH	HORIZ.
COATED BARS					TOTAL WEIGHT = 1,940 LBS	
B410	56	12'-4"	X	X	WINGS - E.F.	VERT.
B411	28	14'-7"	X		WINGS - E.F.	VERT.
B412	12	9'-3"	X		WINGS - CORNERS	HORIZ.
B813	18	15'-4"	X		WINGS - B.F.	HORIZ.
B514	18	13'-10"	X		WINGS - F.F.	HORIZ.
B415	4	11'-4"			WINGS - B.F. & F.F.	HORIZ.
B416	4	9'-8"			WINGS - B.F. & F.F.	HORIZ.
B417	4	7'-8"			WINGS - B.F. & F.F.	HORIZ.
B418	4	5'-8"			WINGS - B.F. & F.F.	HORIZ.
B419	4	3'-8"			WINGS - B.F. & F.F.	HORIZ.
B420	4	12'-9"	X		WINGS - TOP	VERT.

THE FIRST DIGIT OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.
DIMENSIONS IN BENDING DETAILS ARE OUT-TO-OUT OF BAR.

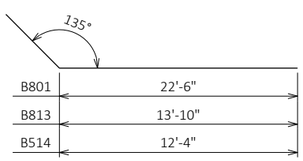
BAR SERIES TABLE

BAR MARK	NO. REQ'D	LENGTH
B410	4 SERIES OF 14	10'-8" TO 14'-0"

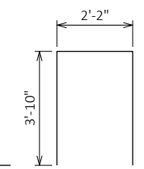
BUNDLE AND TAG EACH SERIES SEPARATELY.

LEGEND

- A02 OPTIONAL CONST. JOINT: KEYWAY FORMED BY A BEVELED 2 x 6 (18" RMW @ B.F. & "V" GROOVE @ F.F. IF JOINT IS USED, INCIDENTAL TO "CONCRETE MASONRY").
- A08 SEE BEARING PAD DETAIL ON SUPERSTRUCTURE DETAILS SHEET.
- A11 ELEVATION SHOWN AT TOP OF CONCRETE AT C/L OF BRG.
- A15 PIPE UNDERDRAIN WRAPPED 6-INCH. SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SCREEN AT ENDS OF PIPE UNDERDRAIN. RODENT SHIELD TO BE INCLUDED IN BID PRICE OF "PIPE UNDERDRAIN WRAPPED 6-INCH".
- A17 ½" FILLER: SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF ½" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE).
- A19 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACKFACE.

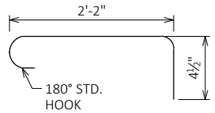


B801, B813, B514

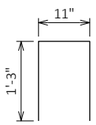


B503

B504



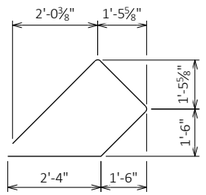
B405



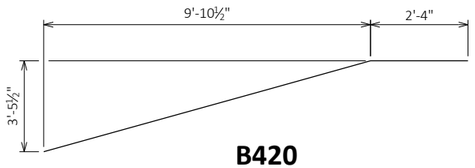
B406

B410

B411



B412



B420

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STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-60-157

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EAST ABUTMENT DETAILS

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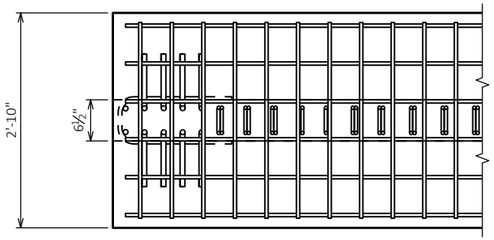
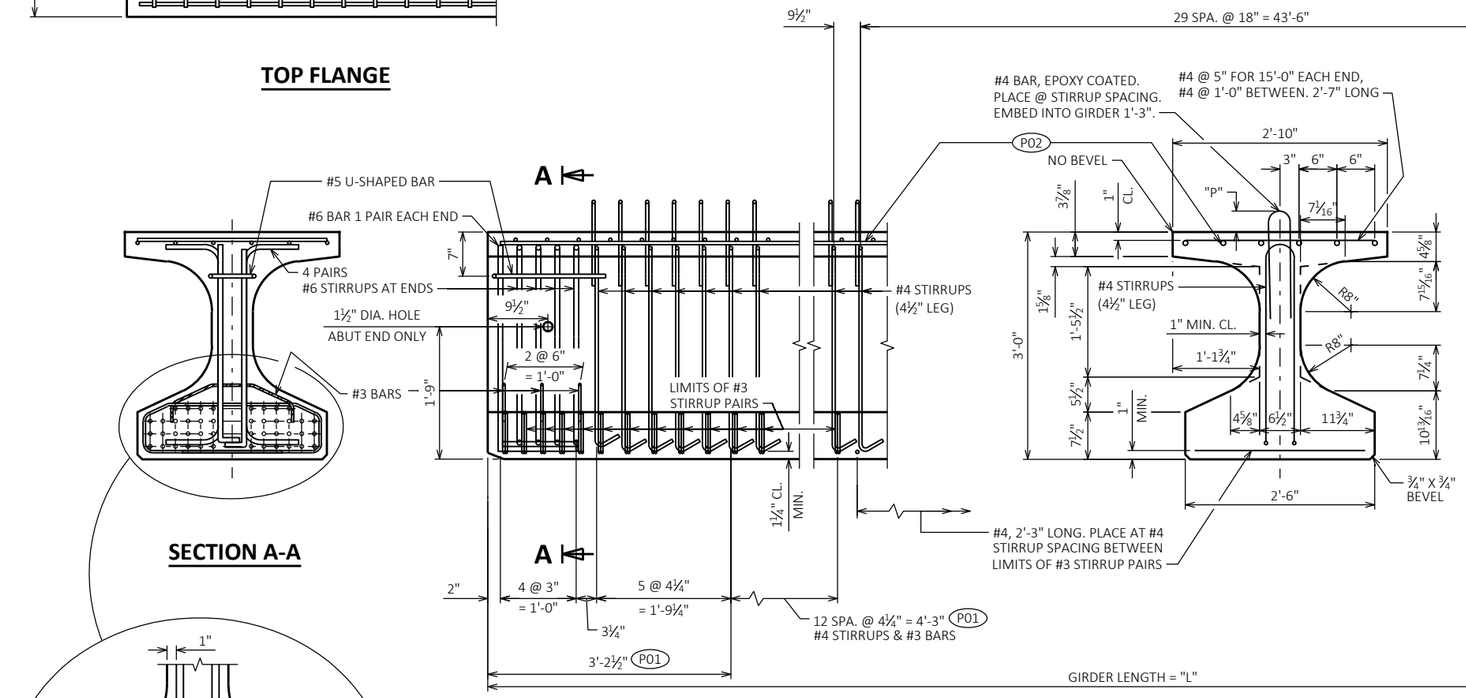
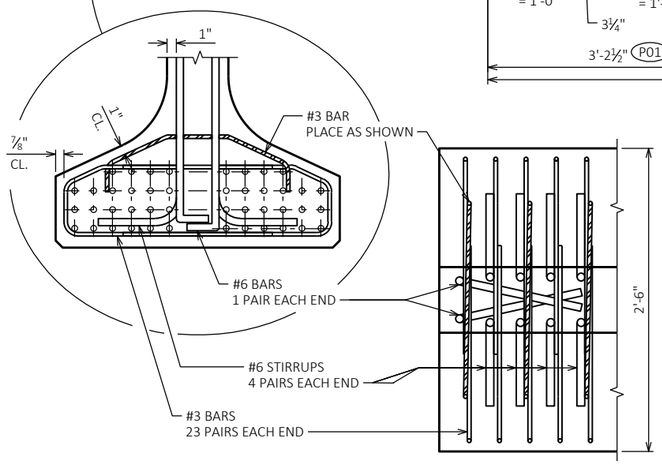


TOP FLANGE



SECTION A-A



BOTTOM FLANGE

SIDE VIEW & TYPICAL SECTION IN SPAN

P01 DETAIL TYP. AT EACH END

P02 6 #4 BARS, FULL LENGTH, MIN. LAP = 1'-11"

NOTES

TOP OF GIRDER TO BE ROUGH FLOATED AND BROOMED TRANSVERSELY, EXCEPT THE OUTSIDE 8" OF GIRDER, WHICH SHALL RECEIVE A SMOOTH FINISH. AN APPROVED CONCRETE SEALER SHALL BE APPLIED TO ALL SMOOTH SURFACES INCLUDING THE OUTSIDE 8" OF THE TOP FLANGE.

DO NOT APPLY CONCRETE SEALER OR EPOXY TO SURFACES RECEIVING APPLICATION OF CONCRETE STAINING.

THE GIRDERS SHALL BE PROVIDED WITH A SUITABLE LIFTING DEVICE FOR HANDLING AND ERECTING THE GIRDERS. SEE SECT. 503.3.4 OF STANDARD SPECIFICATIONS FOR GUIDANCE.

STRANDS SHALL BE FLUSH WITH END OF GIRDER. FOR GIRDER ENDS EMBEDDED COMPLETELY IN CONCRETE, END OF STRANDS SHALL BE COATED WITH NON-BITUMINOUS JOINT SEALER. FOR GIRDER ENDS THAT ARE FINALLY EXPOSED, COAT THE GIRDER ENDS, EXPOSED STRAND ENDS AND ALL NON-BONDING SURFACES WITHIN 2 FEET OF THE GIRDER ENDS WITH A NON-PIGMENTED EPOXY CONFORMING TO AASHTO M-235 TYPE III, GRADE 2, CLASS B OR C. THE EPOXY SHALL BE APPLIED AT LEAST 3 DAYS AFTER MOIST CURING HAS CEASED AND PRIOR TO THE APPLICATION OF THE SEALER.

ALL GIRDERS SHALL BE CAST FULL LENGTH AS SHOWN.

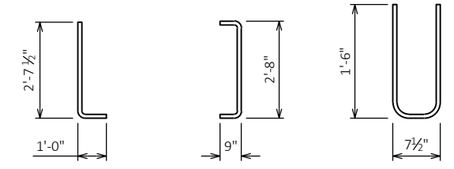
SPACING SHOWN FOR #4 STIRRUPS IS FOR GRADE 60 REINFORCEMENT.

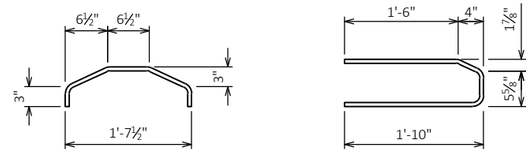
AN EQUIVALENT OF WELDED WIRE FABRIC (WWF) ASTM A1064 MAY BE SUBSTITUTED FOR THE STIRRUP REINFORCEMENT SHOWN, UPON APPROVAL OF THE STRUCTURES DESIGN SECTION. IF USED, WWF SUBSTITUTION DETAILS SHALL BE SUBMITTED ELECTRONICALLY TO THE WISDOT FABRICATION LIBRARY AND ACCEPTED PRIOR TO SHOP DRAWING SUBMITTAL.

PRESTRESSING STRANDS SHALL BE (0.6" DIA.)-7 WIRE LOW-RELAXATION STRANDS WITH AN ULTIMATE STRENGTH OF 270,000 PSI.

FOR DIAPHRAGM INSERT & CONNECTION DETAILS SEE "STEEL DIAPHRAGMS" SHEET.

FOR STORAGE, HANDLING, AND TRANSPORTING, THIS GIRDER IS REINFORCED TO ALLOW A MAXIMUM OVERHANG FROM THE LIFTING LOCATION OR POINT OF SUPPORT OF UP TO 1/10 THE GIRDER LENGTH. THE CONTRACTOR IS RESPONSIBLE FOR LATERAL STABILITY OF THE GIRDER UNTIL THE DECK IS CURED.





✳ MINIMUM CYLINDER STRENGTH OF CONCRETE @ TIME OF TRANSFER OF PRESTRESS FORCE.

GIRDER DATA

SPAN	GIRDER	GIRDER LENGTH "L" (FEET)	DEAD LOAD DEFL. (IN.)									CONC. STRGTH. f'c (P.S.I.)	"P" (IN.)			DIA. OF STRAND (IN.)	DRAPED PATTERN						UNDRAPED PATTERN	
			1/10	2/10	3/10	4/10	5/10	6/10	7/10	8/10	9/10		1ST ⅓ OF GIRDER	MID ⅓ OF GIRDER	END ⅓ OF GIRDER		TOTAL NO. OF STRANDS	f'ci (P.S.I.) ✳	(IN.) "A"	"B" MIN.	"B" MAX.	"C"	TOTAL NO. OF STRANDS	f'ci (P.S.I.) ✳
1	ALL	60.0	0.2	0.3	0.5	0.5	0.6	0.5	0.5	0.3	0.2	8,000	7.00	7.00	7.00	0.600	18	6,800	28	10	13	4		

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36" PRESTRESSED GIRDER DETAILS - 1

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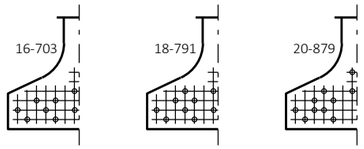
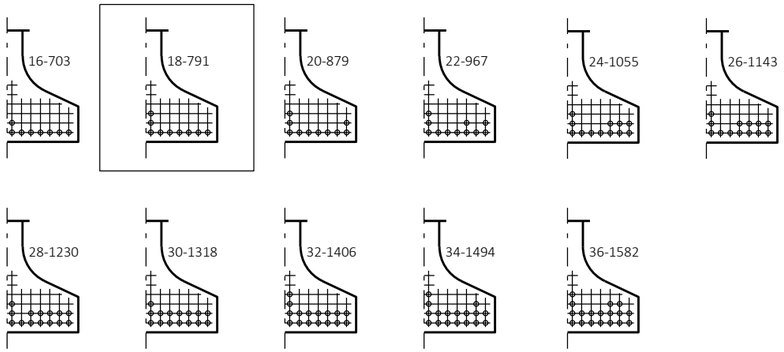


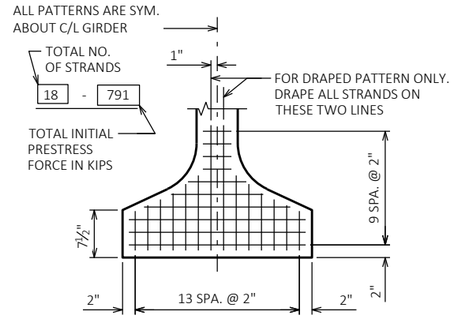
STANDARD ARRANGEMENTS TO RAISE CENTER OF GRAVITY TO AVOID DRAPING OF STRANDS

0.6" DIA. STRANDS

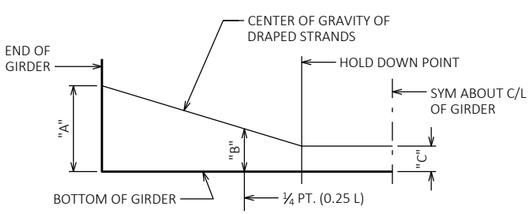


ARRANGEMENT AT C/L SPAN - FOR GIRDERS WITH DRAPED STRANDS

0.6" DIA. STRANDS



TYP. STRAND PATTERN



"A" TO BE GIVEN TO THE NEAREST 1"

"B_{MIN}" = ¼("A" + 3"C")

"B_{MAX}" = "B_{MIN}" + 3

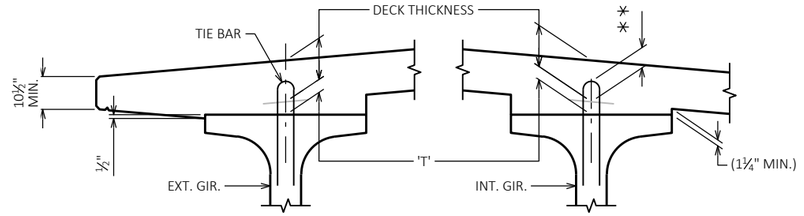
DRAPED STRAND PROFILE

⚹ THE THEORETICAL INITIAL CAMBER VALUE AT THE TIME OF STRAND RELEASE AT MIDSPAN MULTIPLIED BY A FACTOR OF 1.4 TO ACCOUNT FOR CAMBER GROWTH FROM THE TIME OF STRAND RELEASE TO JOBSITE PLACEMENT.

SPAN	CAMBER (IN.) ⚹
1	1.2

THESE VALUES ARE NOT TO BE USED IN DETERMINING 'T', USE ACTUAL GIRDER SHOTS.

THESE VALUES ARE FOR INFORMATIONAL PURPOSES ONLY.



DECK HAUNCH DETAIL

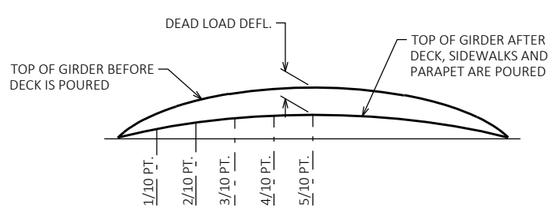
IF 1¼" MINIMUM HAUNCH HEIGHT AT EDGE OF GIRDER CANNOT BE MAINTAINED, THE GRADE LINE MAY BE REVISED BY THE ENGINEER AT THE OPTION OF THE CONTRACTOR, THE PLAN DECK THICKNESS SHALL BE HELD. NOTIFY THE STRUCTURES SECTION IF THE GRADE LINE IS RAISED FROM THE PLAN PROFILE BY MORE THAN ½" OR,

⚹⚹ IF 3" MINIMUM DECK EMBEDMENT OF TIE BAR CANNOT BE OBTAINED.

TO DETERMINE 'T', ELEV. OF TOP OF GIR'S. AT C/L OF SUBSTRUCTURE UNITS & AT 1/10 POINTS OF EACH SPAN SHALL BE TAKEN. THEN FOLLOW THIS PROCESS:

	TOP OF DECK ELEV. AT FINAL GRADE
-	TOP OF GIRDER ELEVATION
+	DEAD LOAD DEFLECTION
-	DECK THICKNESS
=	HAUNCH HEIGHT 'T'

NOTE: AN AVERAGE HAUNCH ('T') OF 2½" WAS USED IN THE QUANTITY "CONCRETE MASONRY BRIDGES".



DEAD LOAD DEFLECTION DIAGRAM

8

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DEPARTMENT OF TRANSPORTATION

STRUCTURE B-60-157

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36" PRESTRESSED GIRDER DETAILS - 2

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NOTES

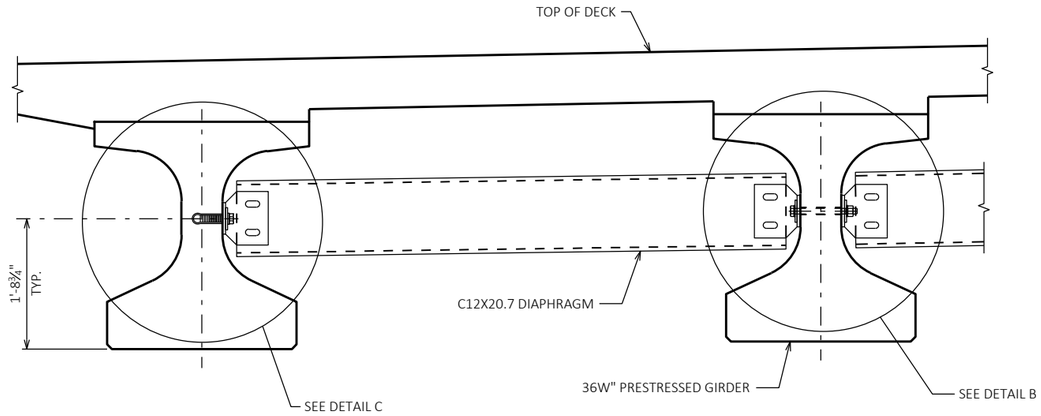
ALL DIAPHRAGM MATERIAL NOT EMBEDDED IN THE CONCRETE GIRDER SHALL BE PAID FOR AT THE UNIT PRICE BID FOR "STEEL DIAPHRAGMS B-60-157", EACH.

EACH DIAPHRAGM BETWEEN GIRDERS SHALL CONSTITUTE ONE UNIT.

ALL DIAPHRAGM STRUCTURAL STEEL SHALL BE ASTM A709 GRADE 36.

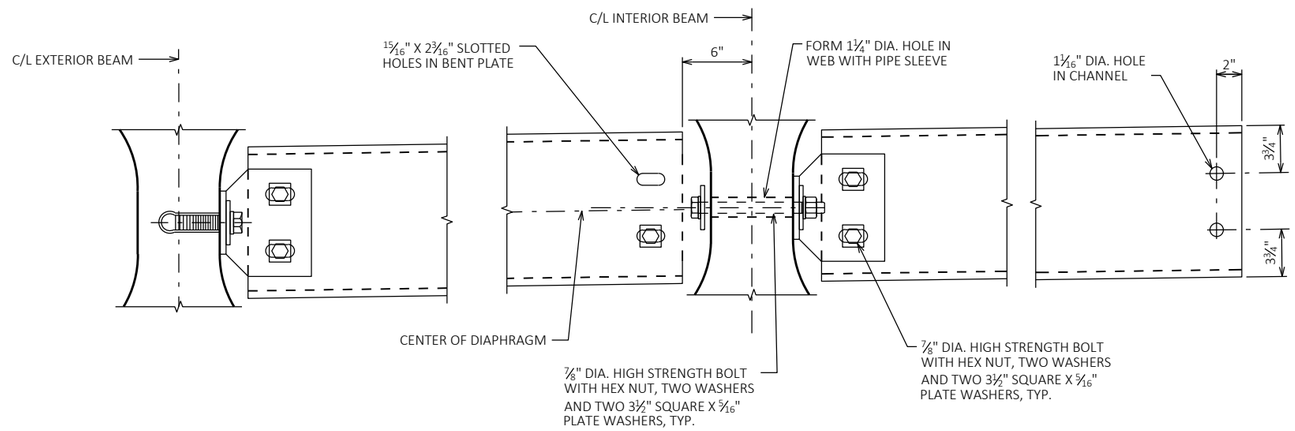
ALL DIAPHRAGM MATERIAL INCLUDING BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED AFTER FABRICATION.

STEEL DIAPHRAGM TO CONCRETE WEB CONNECTION SHALL BE SNUG-TIGHT PLUS ¼ TURN, UNLESS NOTED OTHERWISE. HIGH STRENGTH BOLTS FOR WEB CONNECTION SHALL MEET THE REQUIREMENTS FOR ASTM A325 OR ASTM A449.

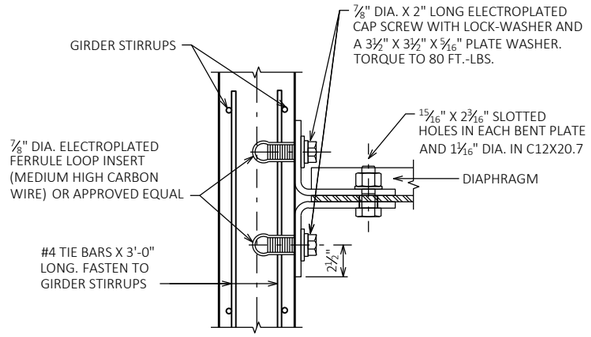


EXTERIOR GIRDER **INTERIOR GIRDER**

PART TRANSVERSE SECTION AT DIAPHRAGM

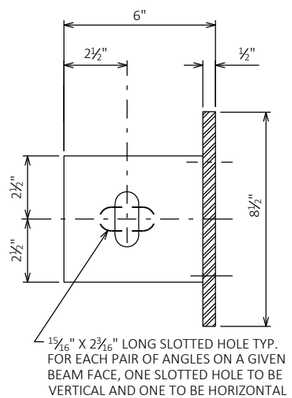


DETAIL C **DETAIL B**

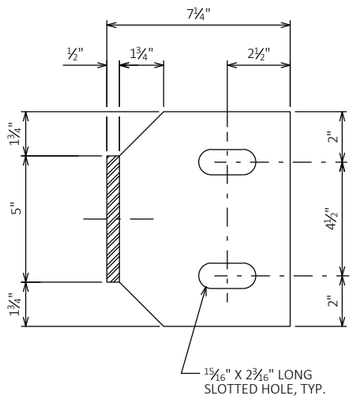


SECTION A-A

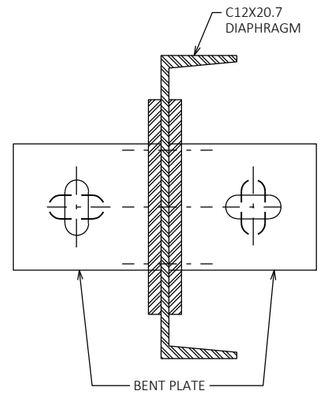
(FOR EXTERIOR ATTACHMENT)



BEAM FACE



DIAPHRAGM FACE



ATTACHMENT TO CHANNEL

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STEEL DIAPHRAGMS

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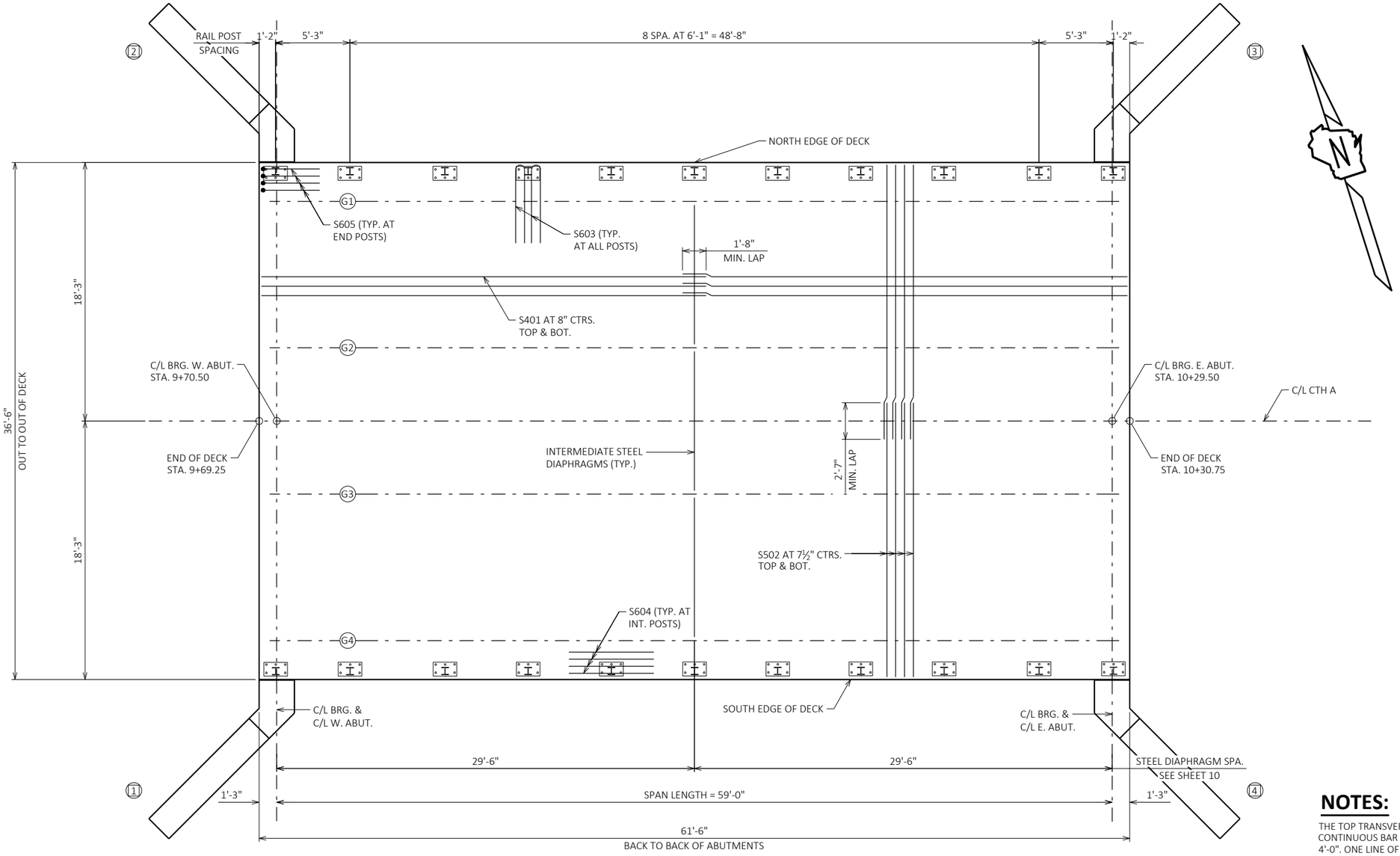

PLAN

NOTES:

THE TOP TRANSVERSE BAR STEEL REINFORCEMENT SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS WITH A CENTER TO CENTER SPACING NOT TO EXCEED 4'-0". ONE LINE OF CONTINUOUS BAR CHAIRS SHALL BE PLACED NEAR EACH EDGE OF SLAB TO SUPPORT THE ENDS OF THE BOTTOM TRANSVERSE BAR STEEL.

THE TOP LONGITUDINAL BAR STEEL REINFORCEMENT SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS IN TRANSVERSE DIRECTION ON 4'-0" CENTERS.

(n) INDICATES WING NUMBER

TOP OF DECK ELEVATIONS

LOCATION	W. ABUT.	1/10	2/10	3/10	4/10	5/10	6/10	7/10	8/10	9/10	E. ABUT.
EDGE OF DECK	1353.96	1353.94	1353.92	1353.90	1353.88	1353.86	1353.84	1353.83	1353.81	1353.80	1353.79
GIRDER 1	1354.02	1353.99	1353.97	1353.95	1353.93	1353.91	1353.90	1353.88	1353.87	1353.85	1353.84
GIRDER 2	1354.22	1354.20	1354.18	1354.16	1354.14	1354.12	1354.10	1354.09	1354.07	1354.06	1354.05
CROWN	1354.33	1354.30	1354.28	1354.26	1354.24	1354.22	1354.21	1354.19	1354.18	1354.16	1354.15
GIRDER 3	1354.22	1354.20	1354.18	1354.16	1354.14	1354.12	1354.10	1354.09	1354.07	1354.06	1354.05
GIRDER 4	1354.02	1353.99	1353.97	1353.95	1353.93	1353.91	1353.90	1353.88	1353.87	1353.85	1353.84
EDGE OF DECK	1353.96	1353.94	1353.92	1353.90	1353.88	1353.86	1353.84	1353.83	1353.81	1353.80	1353.79

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SUPERSTRUCTURE

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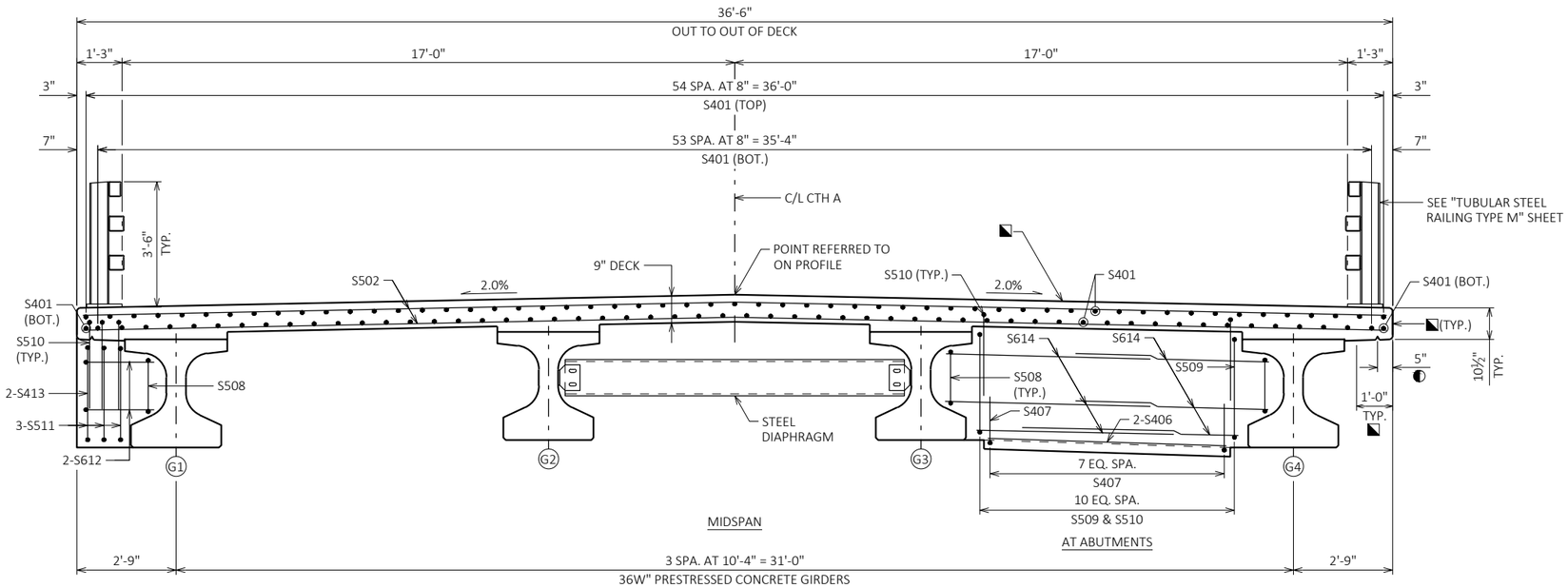
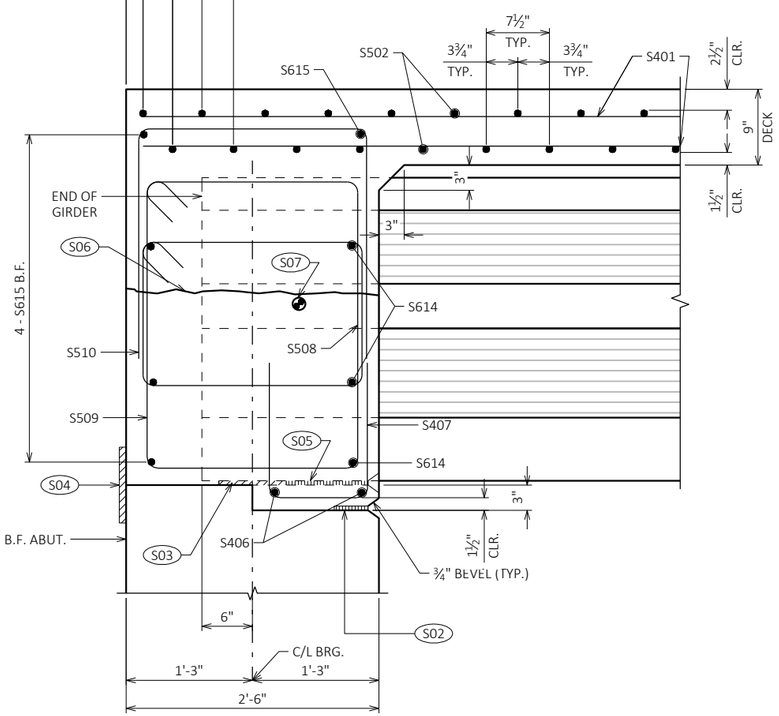
FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\503_STRUCTURES\503.3_FINAL\081101_SUP_CTH A.DWG PLOT DATE : 5/16/2024 8:24 AM PLOT BY : SAM SWITTEL

STATE PROJECT NUMBER

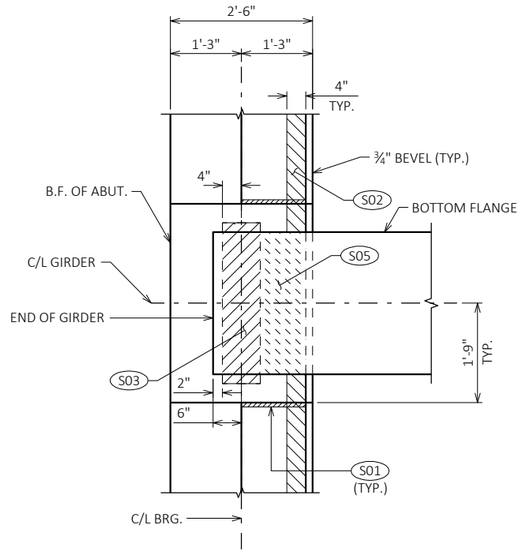
8887-03-76



CROSS SECTION THRU BRIDGE

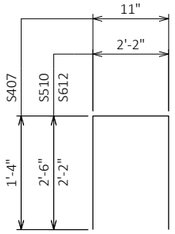


PART LONGITUDINAL SECTION AT ABUTMENTS

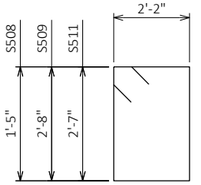


BEARING PAD DETAIL

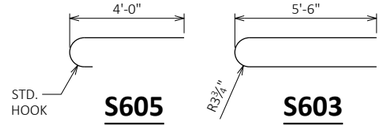
AT ABUTMENTS



S407, S510, S612



S508, S509, S511



S605 S603

BILL OF BARS - SUPERSTRUCTURE

BAR MARK	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION	
COATED BARS					TOTAL WEIGHT = 16,940 LBS	
S401	222	31'-5"			DECK - TOP & BTM.	LONGIT.
S502	394	19'-5"			DECK - TOP & BTM.	TRANS.
S603	44	11'-4"	X		DECK - TOP - AT RAIL POSTS	TRANS.
S604	72	6'-0"			DECK - TOP - AT INT. RAIL POSTS	LONGIT.
S605	16	4'-8"	X		DECK - TOP - AT EXT. RAIL POSTS	LONGIT.
S406	12	6'-6"			DIAPHRAGMS - BTM.	HORIZ.
S407	48	3'-5"	X		DIAPHRAGMS - BTM.	VERT.
S508	16	7'-10"	X		DIAPHRAGMS	VERT.
S509	66	10'-4"	X		DIAPHRAGMS	VERT.
S510	78	6'-11"	X		DIAPHRAGMS - TOP	VERT.
S511	12	10'-2"	X		DIAPHRAGMS - EXT.	VERT.
S612	8	6'-2"	X		DIAPHRAGMS - EXT.	HORIZ.
S413	8	2'-9"			DIAPHRAGMS - EXT.	VERT.
S614	36	5'-9"			DIAPHRAGMS - F.F.	HORIZ.
S615	20	19'-10"			DIAPHRAGMS - B.F.	HORIZ.
S516	16	6'-0"			DIAPHRAGMS - GIRDERS	HORIZ.

THE FIRST DIGIT OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.
DIMENSIONS IN BENDING DETAILS ARE OUT-TO-OUT OF BAR.

LEGEND

S01 ¾" CORK FILLER UP VERT. BEAM SEAT FACES THAT RUN PARALLEL WITH THE GIRDER.

S02 ½" x 4" PREFORMED JOINT FILLER TO EXTEND BETWEEN EDGES OF DIAPHRAGM EXCEPT UNDER THE GIRDER (TYP.).

S03 ½" x 8" x 2'-10" NON-LAMINATED ELASTOMERIC BEARING PAD. SEE BEARING PAD DETAIL.

S04 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACK FACE.

S05 ½" PREFORMED JOINT FILLER UNDER GIRDER FLANGE IN FRONT OF BEARING PAD.

S06 OPTIONAL CONSTRUCTION JOINT 1'-2" BELOW TOP OF GIRDER. IF USED, DECK POUR MUST BE WITHIN 2 WEEKS FROM TIME OF THE DIAPHRAGM POUR. SEAL CONSTRUCTION JOINT ON B.F. ABUTMENT WITH 18" RUBBERIZED MEMBRANE WATERPROOFING (COST INCIDENTAL TO "CONCRETE MASONRY BRIDGES").

S07 (1) - 1½" DIA. HOLE IN GIRDER WEB FOR (2) S516 BARS. PLACE SYMMETRICAL ABOUT C/L OF GIRDERS.

¾" V-GROOVE REQ'D. EXTEND 6" FROM F.F. OF ABUTMENT DIAPHRAGM.

COAT WITH "PROTECTIVE SURFACE TREATMENT" AS PER THE STANDARD SPECIFICATIONS.

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STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-60-157

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SUPERSTRUCTURE DETAILS

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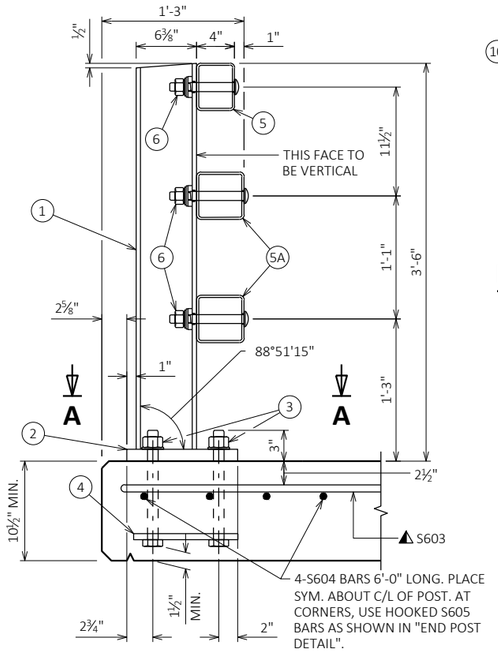
SCALE =

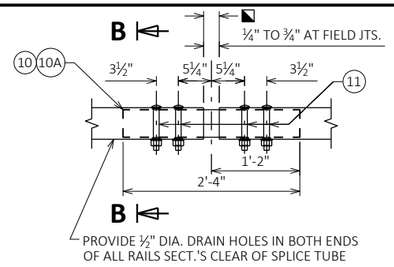
FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\503_STRUCTURES\503.3_FINAL\081101_SUP_CTH A.DWG PLOT DATE : 5/16/2024 8:24 AM PLOT BY : SAM SWITTEL

STATE PROJECT NUMBER

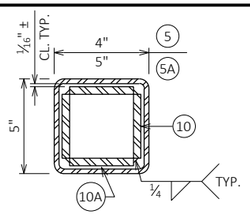
8887-03-76



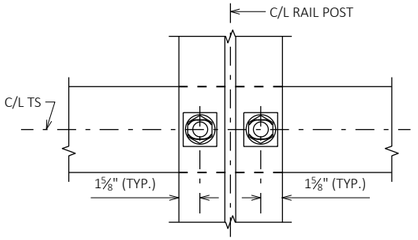
SECTION THRU RAILING ON DECK



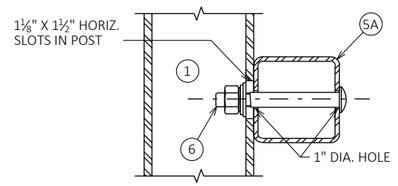
FIELD ERECTION JOINT DETAIL



SECTION B-B



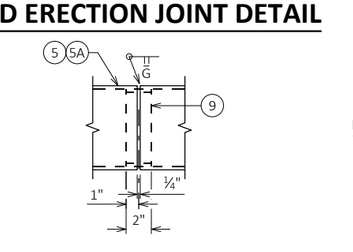
SECTION THRU POST WEB



SECTION THRU RAIL

NOTE: CONNECTIONS AT LOWER RAILS SHOWN. CONNECTIONS AT TOP RAIL SIMILAR.

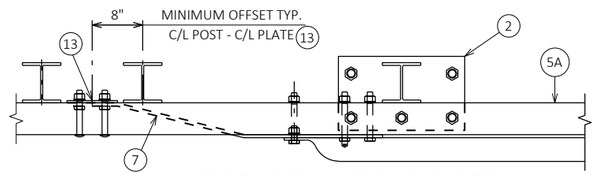
TYPICAL RAIL TO POST CONNECTIONS



ANCHOR BOLTS

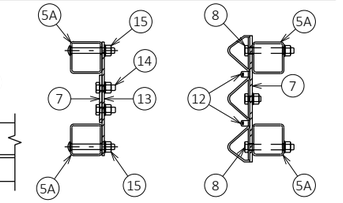
SHOP RAIL SPLICE DETAIL

LOCATION MUST BE SHOWN ON SHOP DRAWINGS



TOP VIEW AT END POST

THRIE BEAM RAIL ATTACHMENT



SECTION C-C

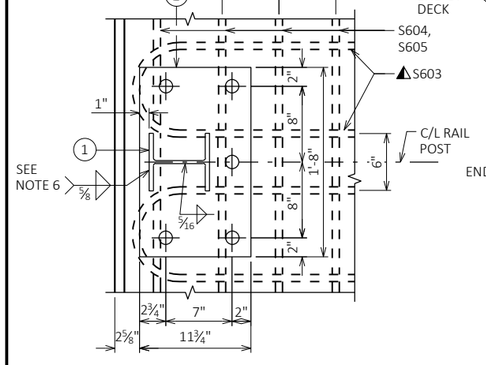
SECTION D-D

LEGEND

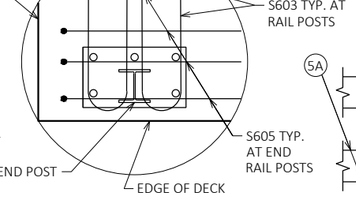
1. W6 X 25 WITH 1⅛" X 1½" HORIZ. SLOTS ON EACH SIDE OF POST FOR BOLT NO. 6. CUT BOTTOM OF POST TO MATCH CROSS SLOPE OF ROADWAY. PLACE POST VERTICAL. PLACE POSTS NORMAL TO GRADE LINE.
2. PLATE 1¼" X 11¾" X 1'-8" WITH 1⁷⁄₁₆" DIA. OVERSIZED HOLES FOR ANCHOR BOLTS NO. 3. WELD TO NO. 1 AS SHOWN.
3. ASTM A449 - 1⅛" DIA. ANCHOR BOLTS WITH NUT AND HARDENED WASHER (ALL GALVANIZED). 5 REQ'D. PER POST. THREAD 3" AND PLACE NORMAL TO PLATE NO. 2. CHAMFER TOP OF BOLTS BEFORE THREADING. USE 10¾" LONG AT ALL LOCATIONS. (AN EQUIVALENT THREADED ROD WITH NUTS AND HARDENED WASHERS MAY BE SUBSTITUTED FOR ANCHOR BOLTS IN WINGS IF REQ'D. FOR CONSTRUCTABILITY.)
4. ⅝" X 11" X 1'-8" ANCHOR PLATE (GALVANIZED) WITH 1³⁄₁₆" DIA. HOLES FOR ANCHOR BOLTS NO. 3
5. TS 5 X 4 X 0.25 STRUCTURAL TUBING. ATTACH TO NO. 1 WITH NO. 6.
5A. TS 5 X 5 X 0.25 STRUCTURAL TUBING. ATTACH TO NO. 1 WITH NO. 6.
6. ⅞" DIA. A325 SLOTTED ROUND HEAD BOLT WITH NUT, ³⁄₁₆" X 1⅝" X 1⅝" MIN. WASHER, AND LOCK WASHER (2 REQ'D. AT EACH RAIL TO POST LOCATION.)
7. ½" THK. BACK-UP PLATE WITH 2 - ⅞" X 1½" THREADED SHOP WELDED STUDS (NO. 12). BOLT TO RAIL AS SHOWN IN DETAIL. REQUIRED AT THRIE BEAM GUARD RAIL ATTACHMENTS ONLY. PLACE SYMMETRICALLY ABOUT TUBES NO. 5A.
8. 1" DIA. HOLES IN PLATE NO. 7 & TUBES NO. 5A FOR ⅞" DIA. A325 BOLTS WITH HEX NUTS AND WASHERS. 6 HOLES IN TUBES AND PLATE NO. 7.
9. SPLICE SLEEVE FABRICATED FROM ¼" PLATE. PROVIDE "SLIDING FIT".
10. ⅜" X 3⅝" X 2'-4" PLATE. 2 PER RAIL. USED IN NO. 5 & 5A.
10A. ⅜" X 2⅝" X 2'-4" PLATE USED IN NO. 5, ⅜" X 3⅝" X 2'-4" PLATE USED IN NO. 5A. 2 PER RAIL.
11. ⅞" DIA. A325 ROUND HEAD BOLT WITH NUT, WASHER, AND LOCK WASHER. USE ¹⁵⁄₁₆" X 1¼" LONGIT. SLOTTED HOLES IN PLATE NO. 10A AT FIELD JOINTS AND ¹⁵⁄₁₆" X 2¼" MIN. LONGIT. SLOTTED HOLES AT EXP. JOINTS IN PLATE NO. 10A.
12. ⅞" DIA. X 1½" LONG THREADED SHOP WELDED STUDS (2 REQ'D).
13. ⅜" X 8" X 1'-6" PLATE. BOLT TO RAIL AS SHOWN IN DETAIL. REQ'D. AT THRIE BEAM GUARD RAIL ATTACHMENTS ONLY. PLACE SYM. ABOUT TUBES NO. 5A.
14. ⅞" DIA. X 2" LONG A325 HEX BOLT WITH NUT AND WASHER (5 REQ'D.).
15. 1" DIA. HOLES IN TUBES NO. 5A FOR ⅞" DIA. A325 ROUND HEAD BOLT WITH NUT, WASHER AND LOCK WASHER (4 REQ'D.). 4 HOLES IN TUBES.

GENERAL NOTES

1. BID ITEM SHALL BE "RAILING TUBULAR TYPE M" WHICH INCLUDES ALL ITEMS SHOWN.
2. RAIL POST AND BASE PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A709 GRADE 50. HOLLOW RAILING STRUCTURAL TUBING SHALL CONFORM TO THE REQUIREMENTS OF ASTM A500 GRADE B OR C WITH A CERTIFIED FY = 50 KSI. ANCHOR PLATES, AND SPLICE TUBE PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A709 GRADE 36.
3. THE NUT SECURING THE POST BASE PLATE TO THE CONCRETE SHALL BE TIGHTENED TO A SNUG FIT AND GIVEN AN ADDITIONAL ⅛ TURN.
4. RAILS SHALL BE CONTINUOUS OVER A MINIMUM OF THREE (3) POSTS WITHOUT SPLICES WHERE POSSIBLE. RAILS SHALL BE SPLICED IN A PANEL OVER EXPANSION JOINTS.
5. ENDS OF TUBE SECTIONS SHALL BE SAWED. GRIND SMOOTH EXPOSED EDGES. ALL CUT ENDS SHALL BE TRUE AND SMOOTH.
6. WELD IS THE SAME ON BOTH FLANGES. FLANGE WELD DOES NOT REQUIRE MAGNETIC PARTICLE TESTING.
7. FILL BOLT SLOT OPENINGS IN POST SHIMS AND PLATE NO. 2 AND CAULK AROUND PERIMETER OF PLATE NO. 2 WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. STEEL POST SHIMS MAY BE USED UNDER POSTS WHERE REQ'D. FOR ALIGNMENT.
8. POST BASE PLATES SHALL BE FLAT WITH ALL SURFACES SMOOTH AND FREE FROM WARP AND ALL EDGES SMOOTH, STRAIGHT AND VERTICAL. ALL PLATE CUTS SHALL BE MACHINE OR MACHINE FLAME CUT.
9. ALL MATERIAL SHALL BE GALVANIZED AFTER FABRICATION. PRIOR TO GALVANIZING, ALL STEEL RAILING POSTS & STEEL TUBING SHALL BE GIVEN A NO. 6 BLAST CLEANING BY SSPC SPECIFICATIONS.

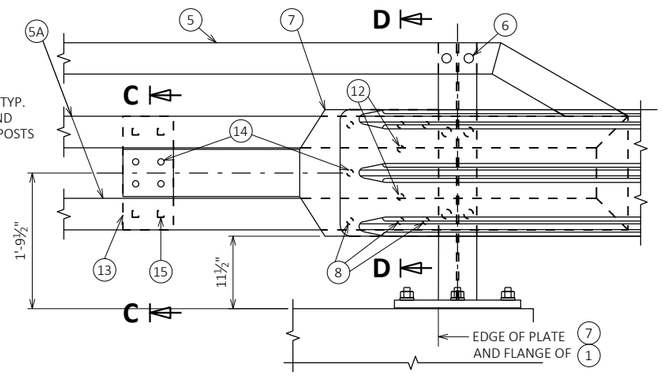


SECTION A-A



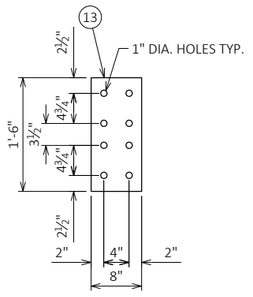
END POST DETAIL

REINFORCEMENT AT CORNERS



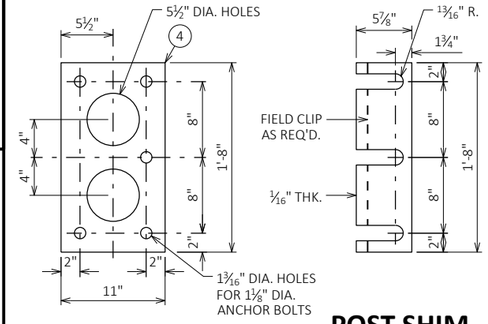
DETAIL AT END POST

THRIE BEAM RAIL ATTACHMENT



ANCHOR PLATE

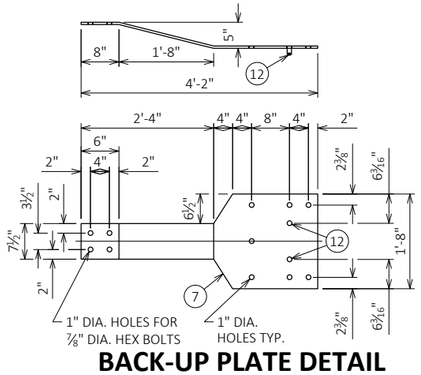
AT BEAM GUARD ATTACHMENT



ANCHOR PLATE

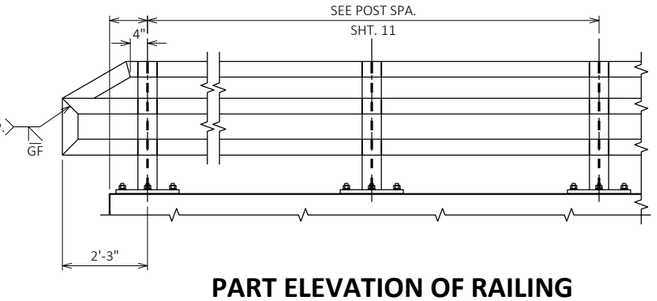
AT RAIL TO DECK CONNECTION

POST SHIM DETAIL



BACK-UP PLATE DETAIL

AT BEAM GUARD ATTACHMENT



PART ELEVATION OF RAILING

▲ TIE TO TOP MAT OF STEEL.

✳ ANCHOR BOLT ASSEMBLY MAY BE TACK WELDED, EITHER IN THE SHOP, OR IN THE FIELD AFTER THE ANCHOR PLATE IS PLACED.

◥ ¼" TO ¾"

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-60-157

DRAWN BY SMS | PLANS CK'D ETP

RAILING TUBULAR TYPE M

SHEET 13 OF 13

60

SCALE =

8

8

DIVISION 1 -- CTH A

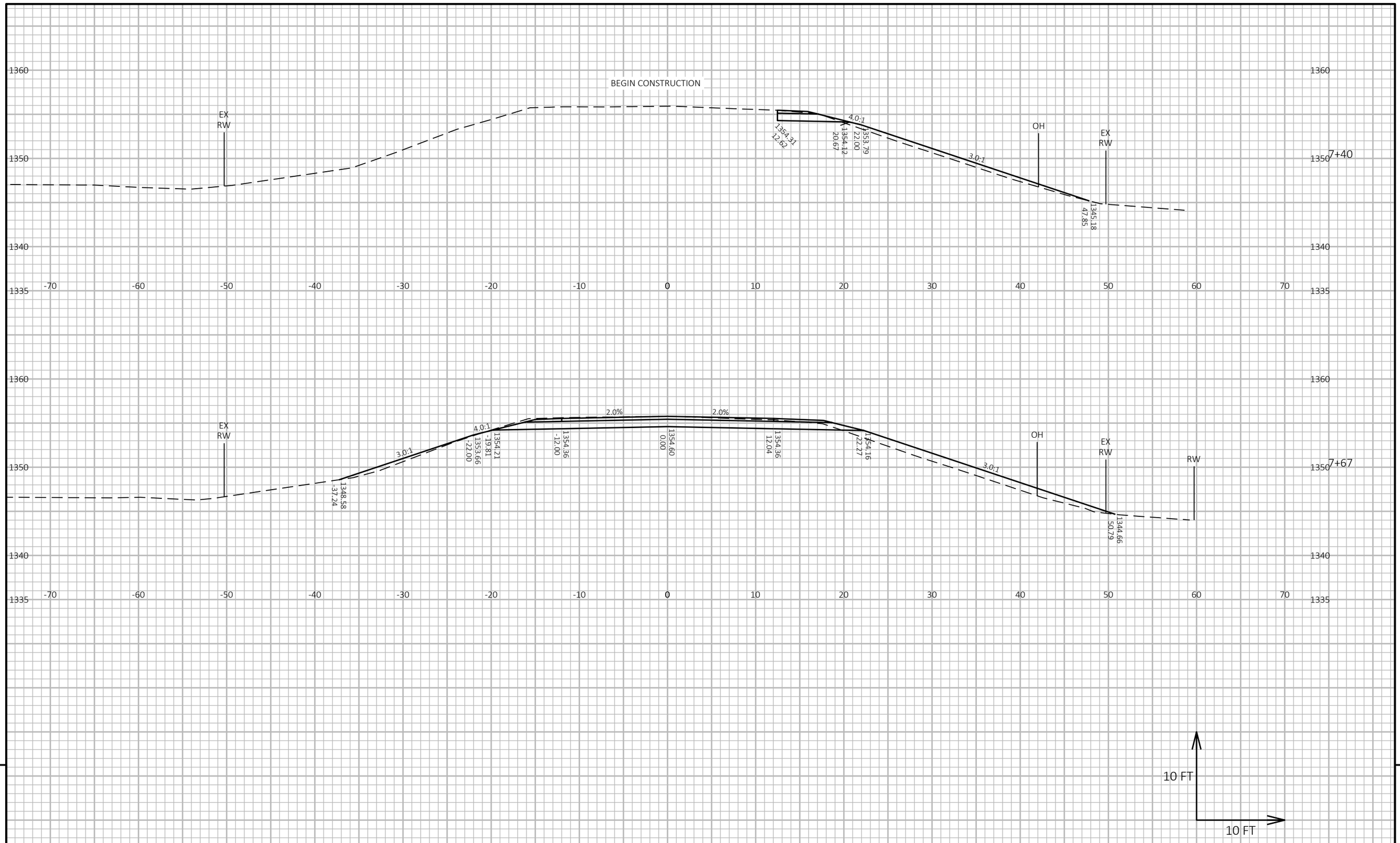
STATION	DISTANCE	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)		
		CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	EXPANDED FILL	MASS ORDINATE
								1.00	1.25	
					NOTE 1	NOTE 2	NOTE 3	NOTE 1		NOTE 4
7+40	0.00	0.00	0.00	11.32	0	0	0	0	0	0
7+67	27.00	35.68	5.00	23.10	18	3	17	18	21	-6
8+00	33.00	40.72	5.00	47.05	47	6	43	65	75	-19
8+17	16.60	42.26	5.00	65.08	26	3	34	91	118	-39
8+42	24.98	44.49	5.00	65.93	40	5	61	131	194	-80
8+67	25.02	43.67	5.00	46.12	41	5	52	172	259	-109
8+92	24.98	41.81	5.00	29.17	40	5	35	212	303	-118
9+00	8.42	41.20	5.00	18.90	13	2	7	225	311	-115
9+17	16.56	40.24	5.00	10.38	25	3	9	250	323	-105
9+50	33.44	36.31	5.00	14.14	47	6	15	297	341	-82
9+61	11.47	35.10	5.00	37.05	15	2	11	312	355	-83
10+39	0.00	39.57	5.00	28.08	0	0	0	312	355	-83
10+50	11.47	44.17	5.00	22.02	18	2	11	330	369	-81
10+83	33.44	53.68	5.00	21.92	61	6	27	391	403	-60
11+00	16.56	57.20	5.00	29.49	34	3	16	425	423	-49
11+08	8.42	57.70	5.00	34.66	18	2	10	443	435	-45
11+33	24.98	58.83	5.00	38.72	54	5	34	497	478	-39
11+58	25.02	58.04	5.00	40.08	54	5	37	551	524	-36
11+83	24.98	52.30	5.00	52.05	51	5	43	602	578	-44
12+00	16.60	48.82	5.00	39.85	31	3	28	633	613	-51
12+33	33.00	43.95	5.00	12.61	57	6	32	690	653	-40

Notes:	
1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL
2 - SALVAGED/UNUSABLE PAVEMENT MATERIAL	THIS DOES NOT SHOW UP IN CROSS SECTIONS
3 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME
4 - MASS ORDINATE	(CUT - SALVAGED PAVT) - (FILL * FILL FACTOR)

9

9

PROJECT NO: 8887-03-76 | HWY: CTH A | COUNTY: TAYLOR | EARTHWORK DATA | SHEET 61 | E



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9

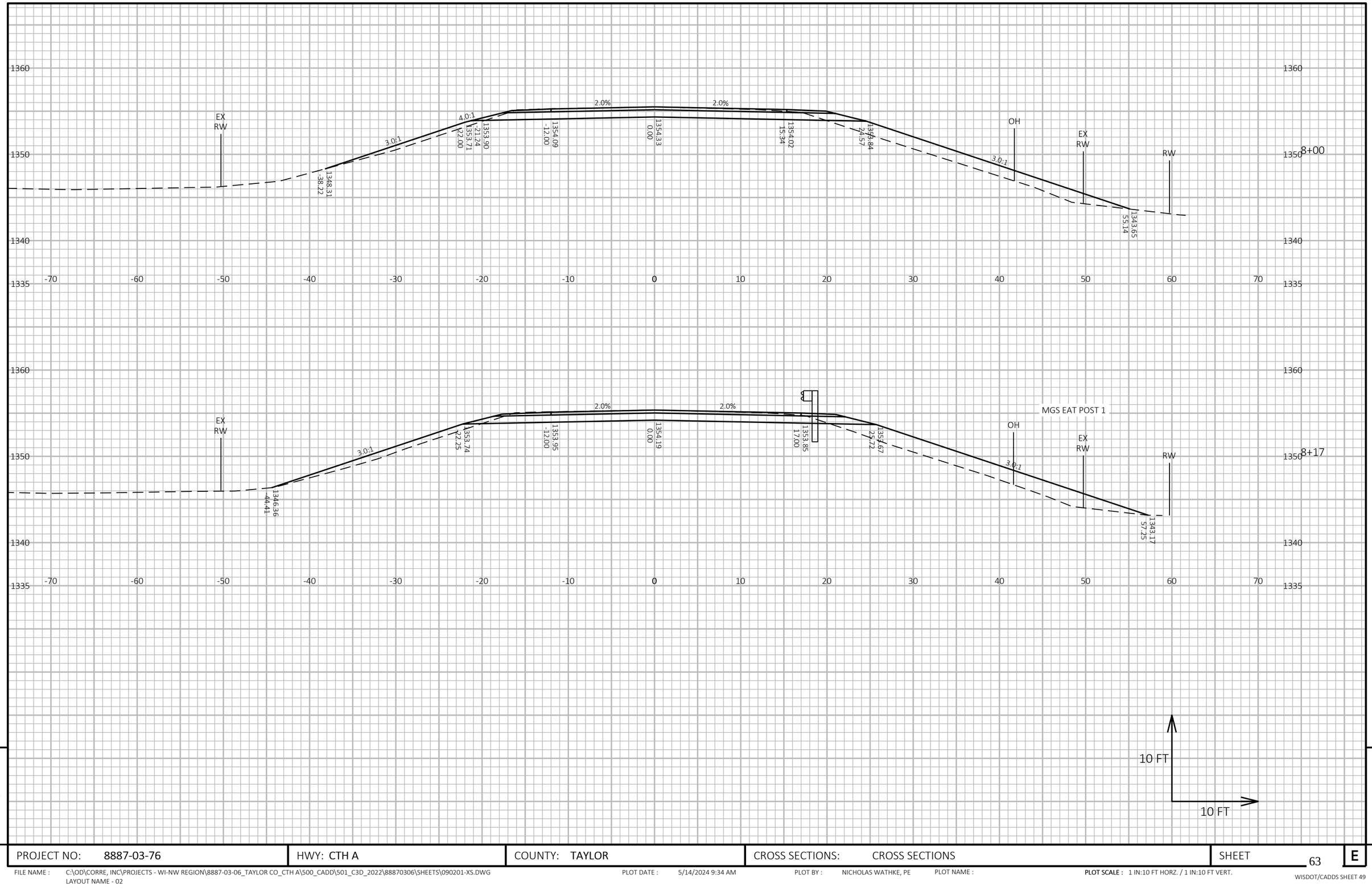
PROJECT NO: 8887-03-76	HWY: CTH A	COUNTY: TAYLOR	CROSS SECTIONS: CROSS SECTIONS	SHEET 62	E

FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\501_C3D_2022\88870306\SHEETS\090201-XS.DWG
LAYOUT NAME - 01

PLOT DATE : 5/14/2024 9:33 AM PLOT BY : NICHOLAS WATHKE, PE PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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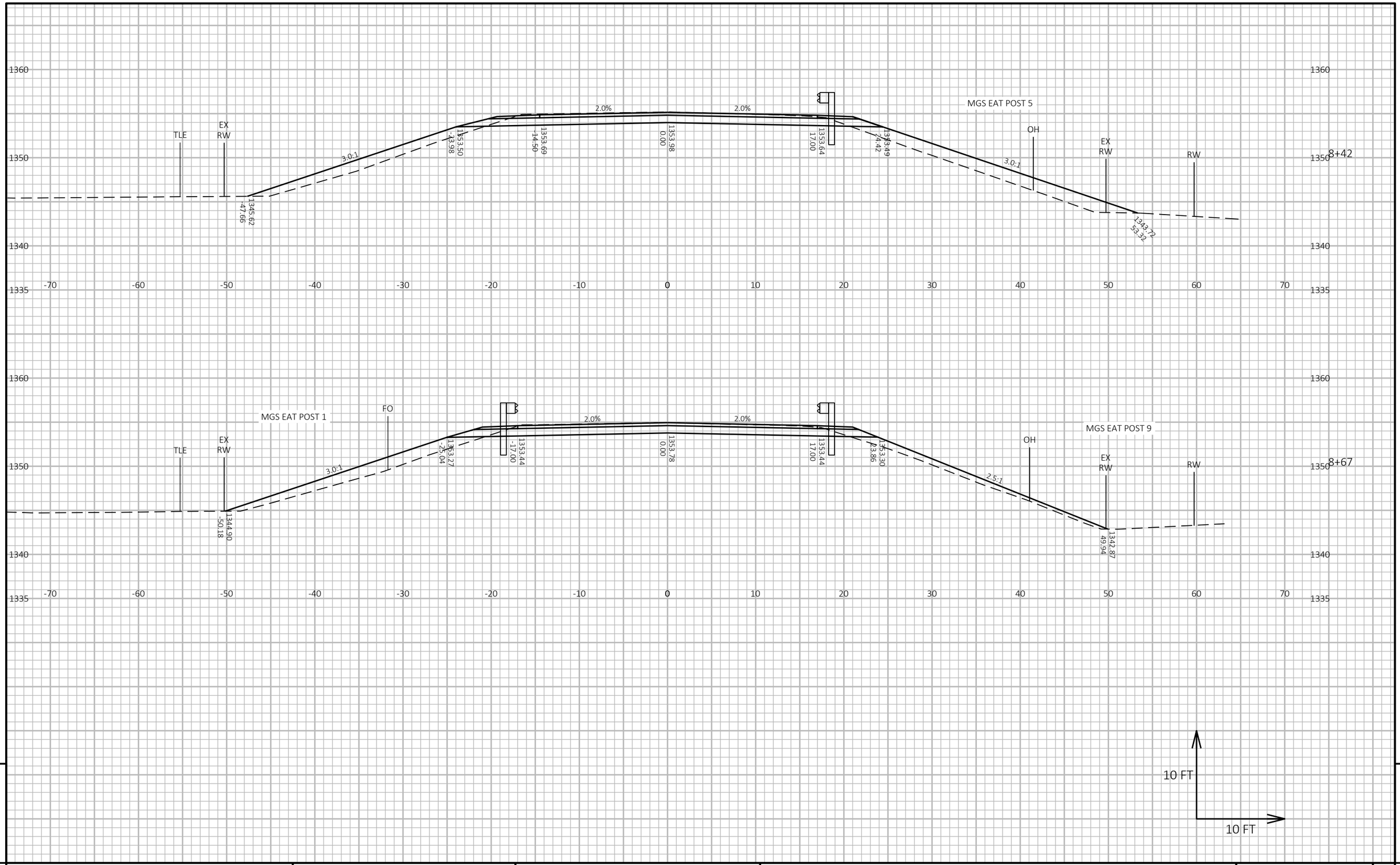
PROJECT NO: 8887-03-76	HWY: CTH A	COUNTY: TAYLOR	CROSS SECTIONS: CROSS SECTIONS	SHEET 63	E

FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\501_C3D_2022\88870306\SHEETS\090201-XS.DWG
LAYOUT NAME - 02

PLOT DATE : 5/14/2024 9:34 AM PLOT BY : NICHOLAS WATHKE, PE PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49

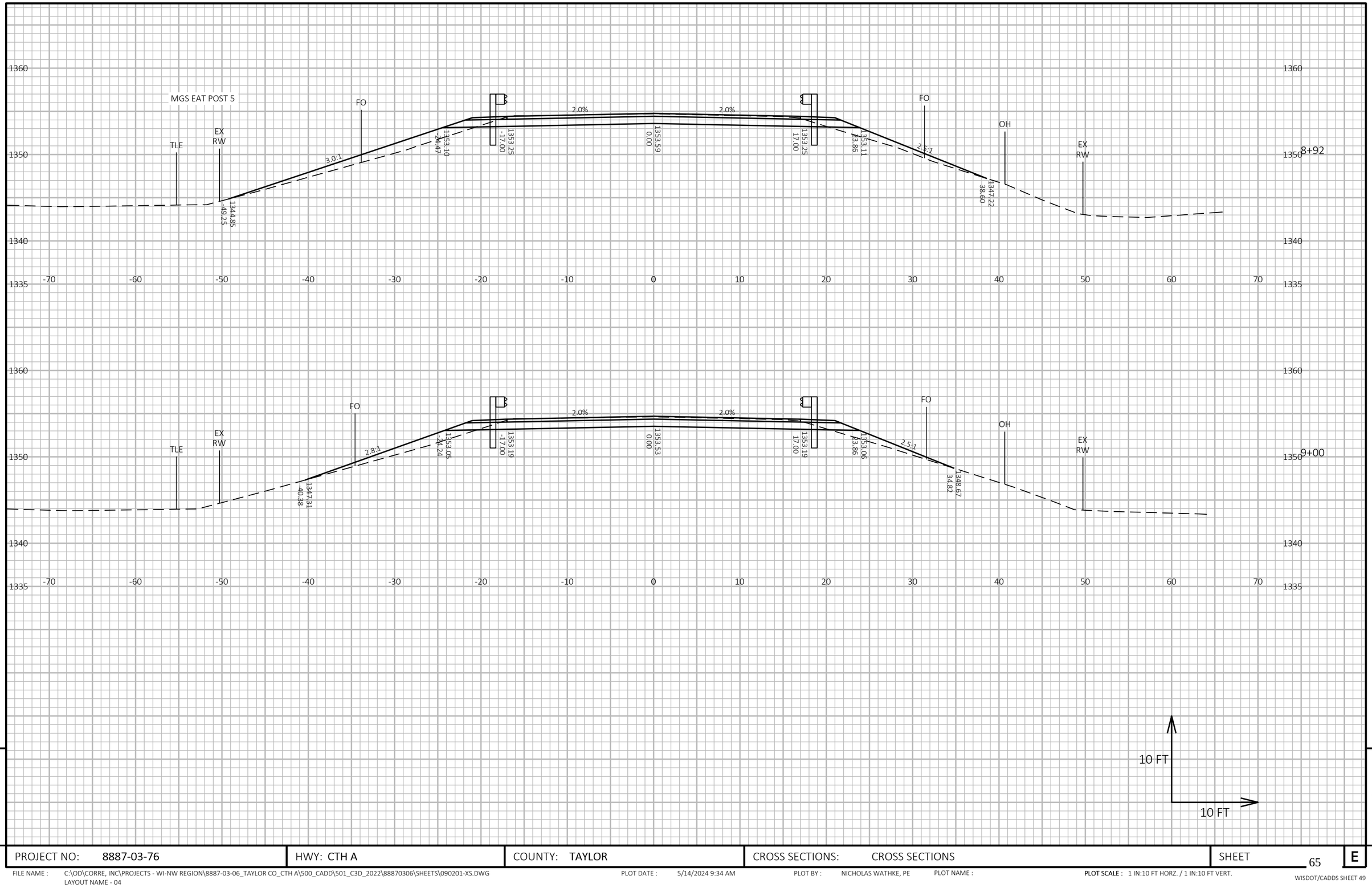


PROJECT NO: 8887-03-76	HWY: CTH A	COUNTY: TAYLOR	CROSS SECTIONS: CROSS SECTIONS	SHEET 64	E

FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\501_C3D_2022\88870306\SHEETS\090201-XS.DWG
LAYOUT NAME - 03

PLOT DATE : 5/14/2024 9:34 AM PLOT BY : NICHOLAS WATHKE, PE PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

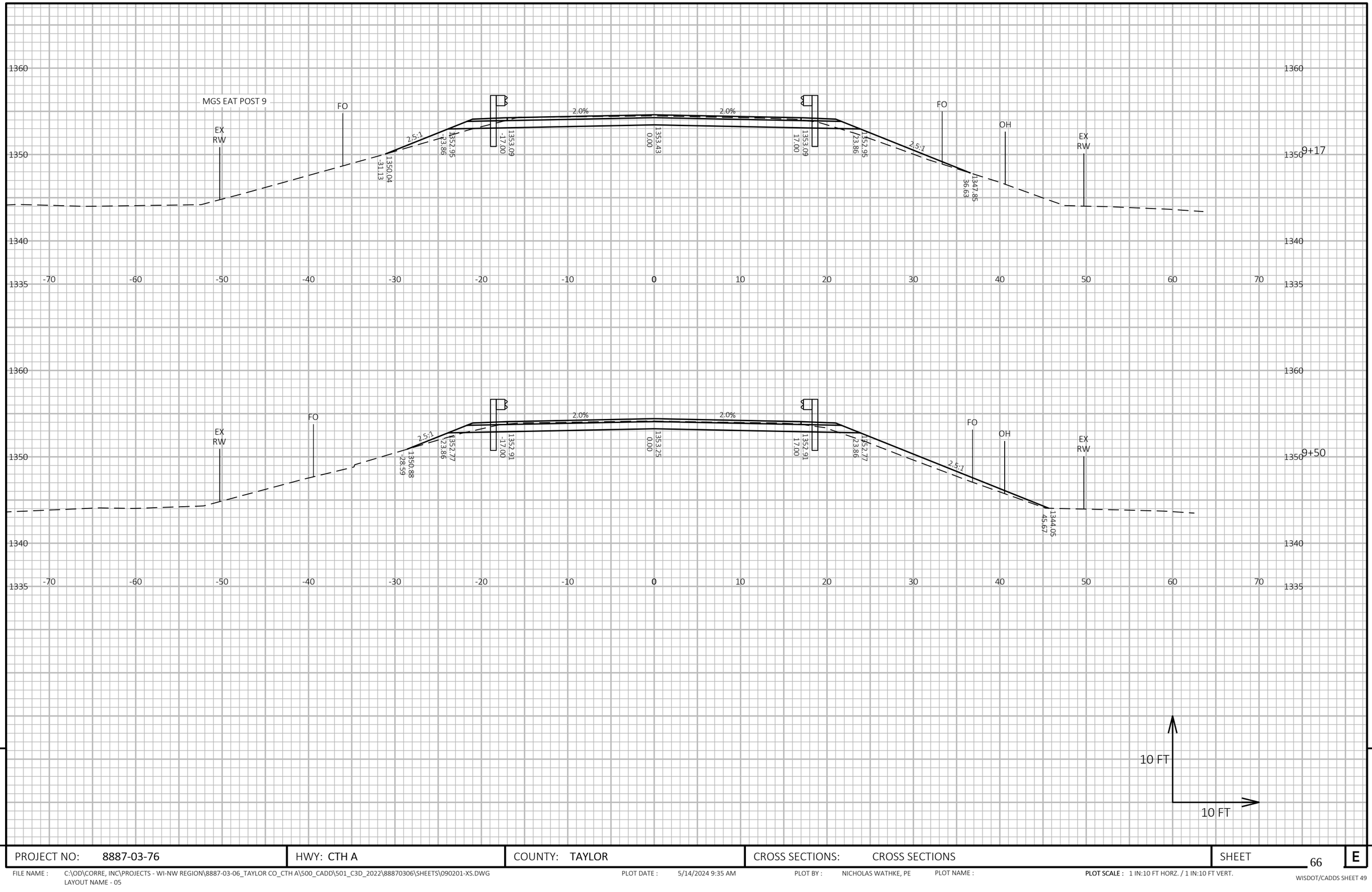
WISDOT/CADDS SHEET 49



PROJECT NO: 8887-03-76	HWY: CTH A	COUNTY: TAYLOR	CROSS SECTIONS: CROSS SECTIONS	SHEET 65	E

FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\501_C3D_2022\88870306\SHEETS\090201-XS.DWG
LAYOUT NAME - 04

PLOT DATE : 5/14/2024 9:34 AM PLOT BY : NICHOLAS WATHKE, PE PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.



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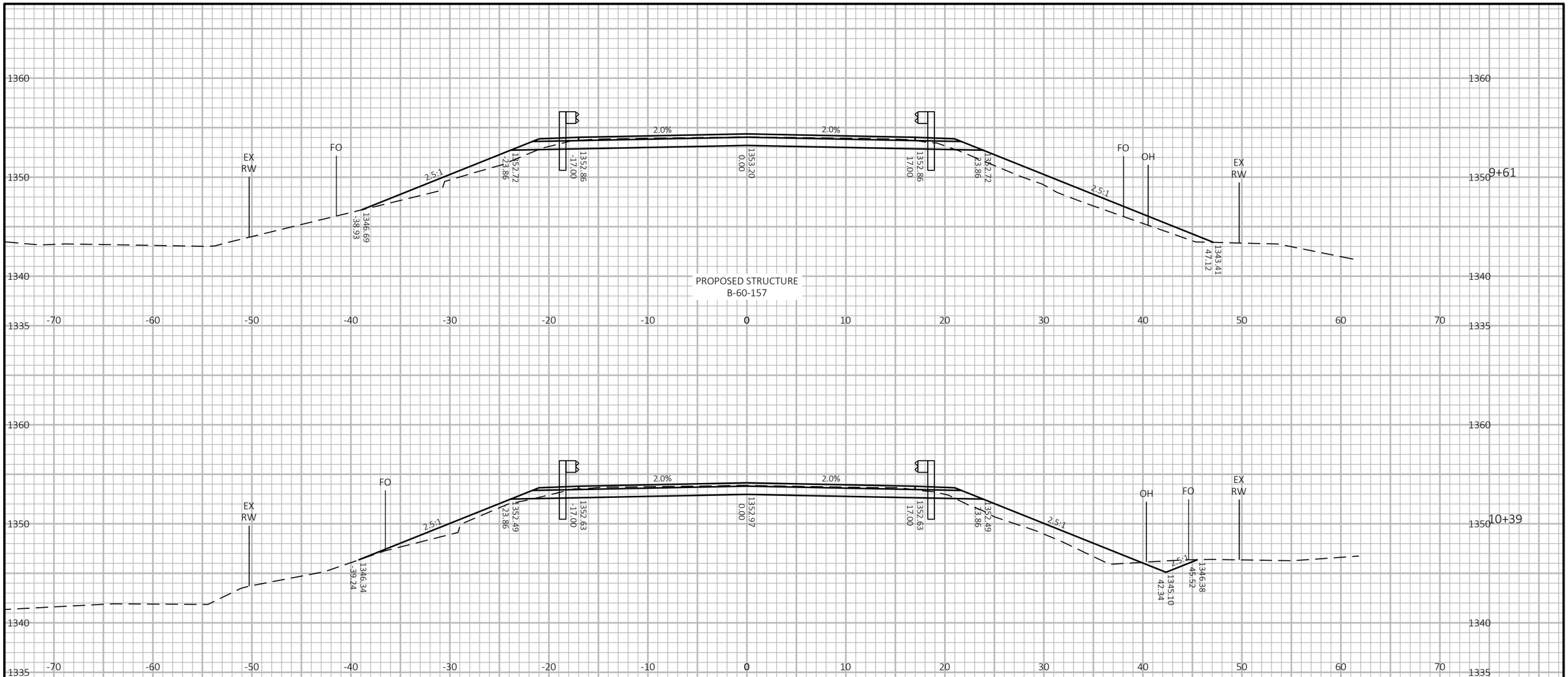
PROJECT NO: 8887-03-76	HWY: CTH A	COUNTY: TAYLOR	CROSS SECTIONS: CROSS SECTIONS	SHEET 66	E

FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\501_C3D_2022\88870306\SHEETS\090201-XS.DWG
LAYOUT NAME - 05

PLOT DATE : 5/14/2024 9:35 AM PLOT BY : NICHOLAS WATHKE, PE PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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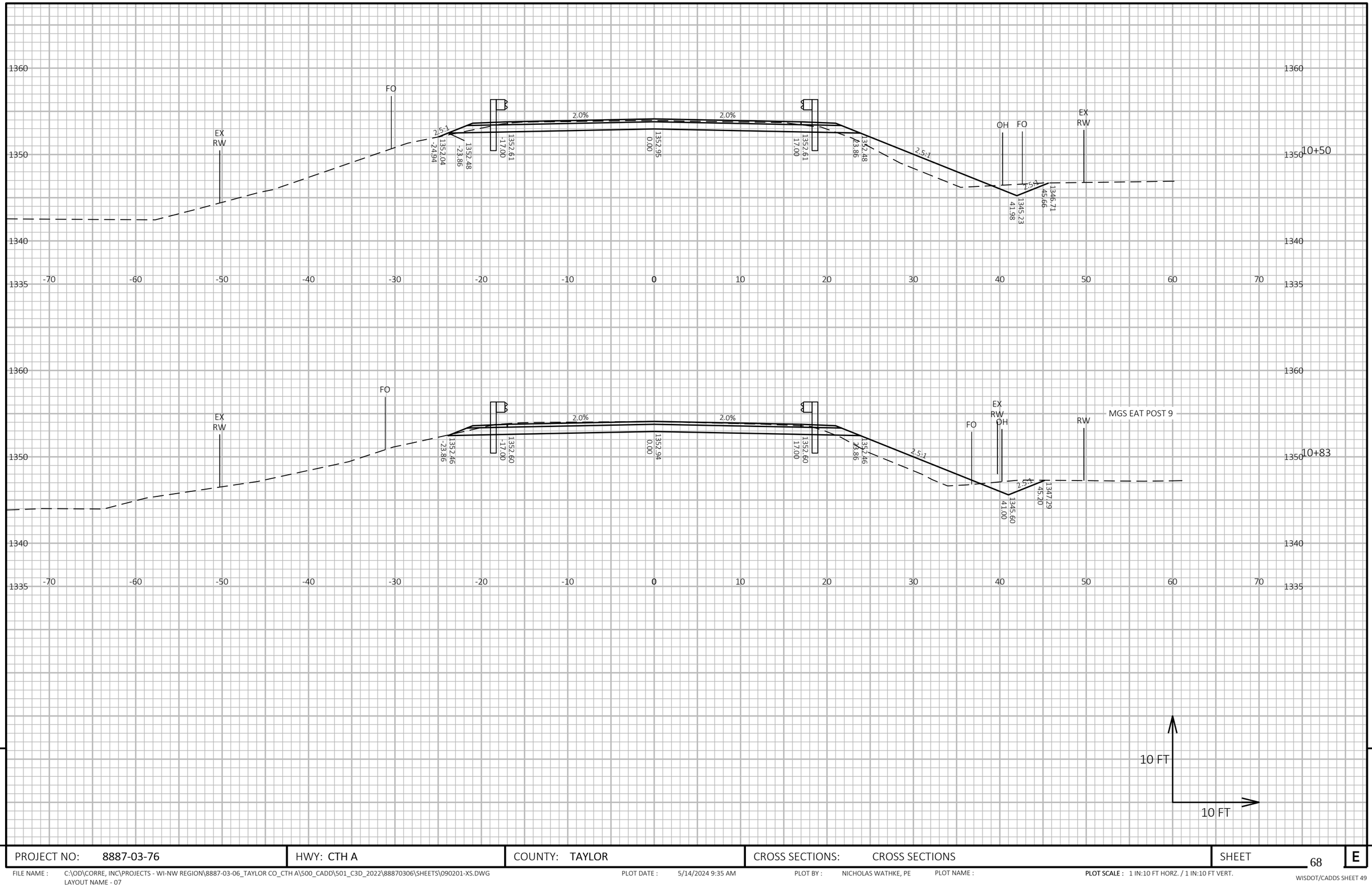
PROJECT NO: 8887-03-76	HWY: CTH A	COUNTY: TAYLOR	CROSS SECTIONS: CROSS SECTIONS	SHEET 67	E

FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\501_C3D_2022\88870306\SHEETS\090201-XS.DWG
LAYOUT NAME - 06

PLOT DATE : 5/14/2024 9:35 AM PLOT BY : NICHOLAS WATHKE, PE PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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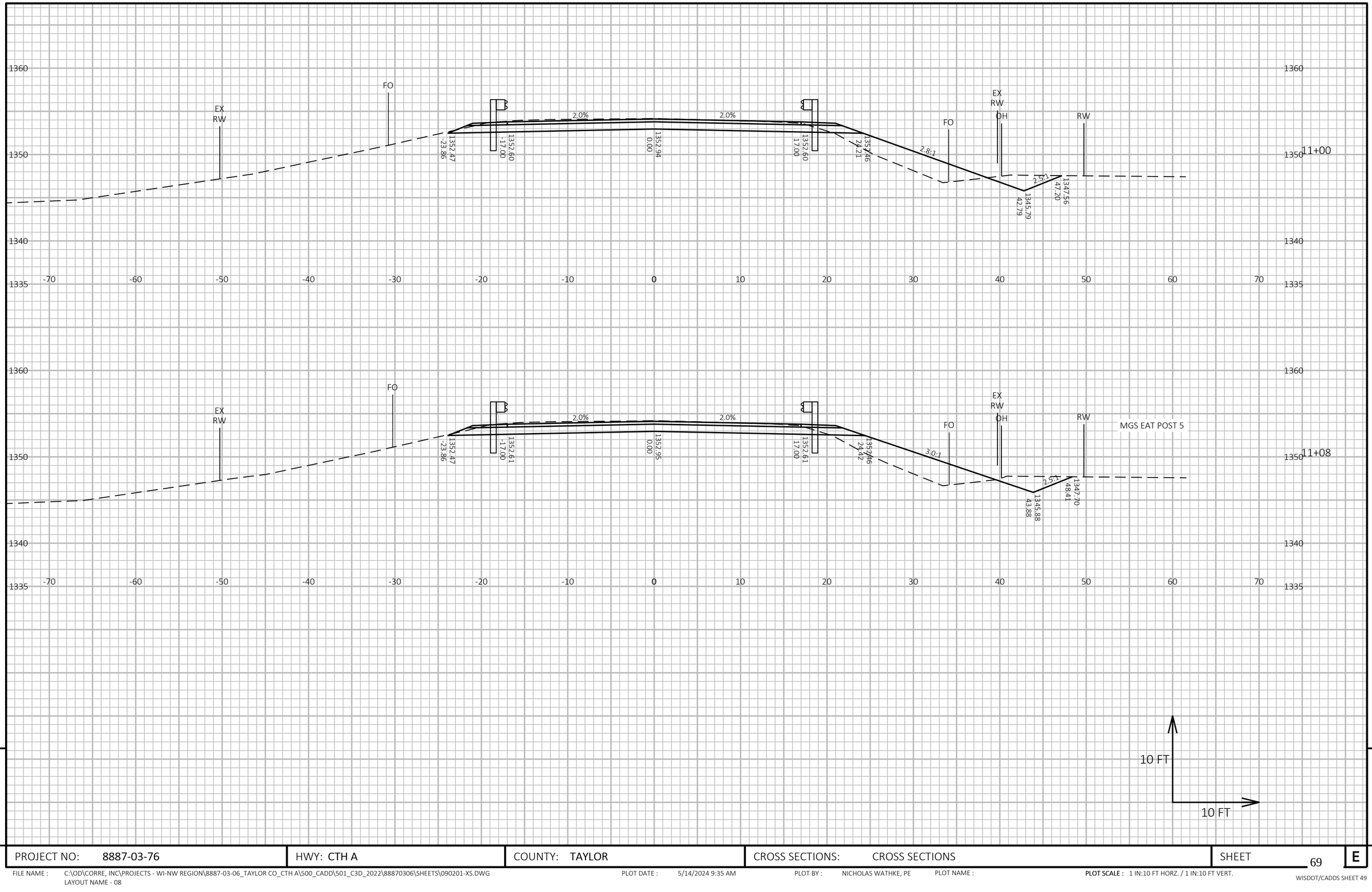
PROJECT NO: 8887-03-76	HWY: CTH A	COUNTY: TAYLOR	CROSS SECTIONS: CROSS SECTIONS	SHEET 68	E

FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\501_C3D_2022\88870306\SHEETS\090201-XS.DWG
LAYOUT NAME - 07

PLOT DATE : 5/14/2024 9:35 AM PLOT BY : NICHOLAS WATHKE, PE PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.



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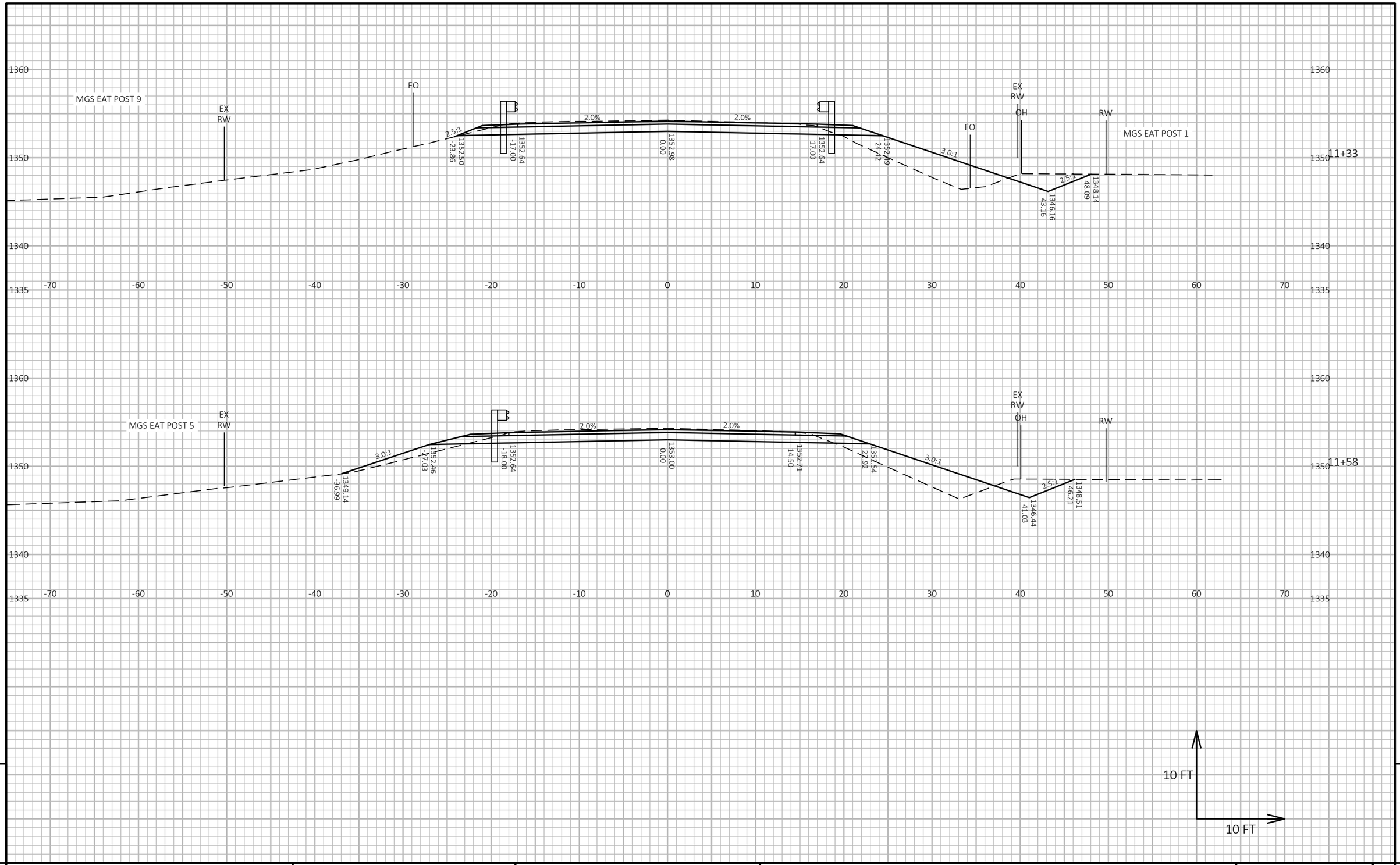
PROJECT NO: 8887-03-76	HWY: CTH A	COUNTY: TAYLOR	CROSS SECTIONS: CROSS SECTIONS	SHEET 69	E

FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\501_C3D_2022\88870306\SHEETS\090201-XS.DWG
LAYOUT NAME - 08

PLOT DATE : 5/14/2024 9:35 AM PLOT BY : NICHOLAS WATHKE, PE PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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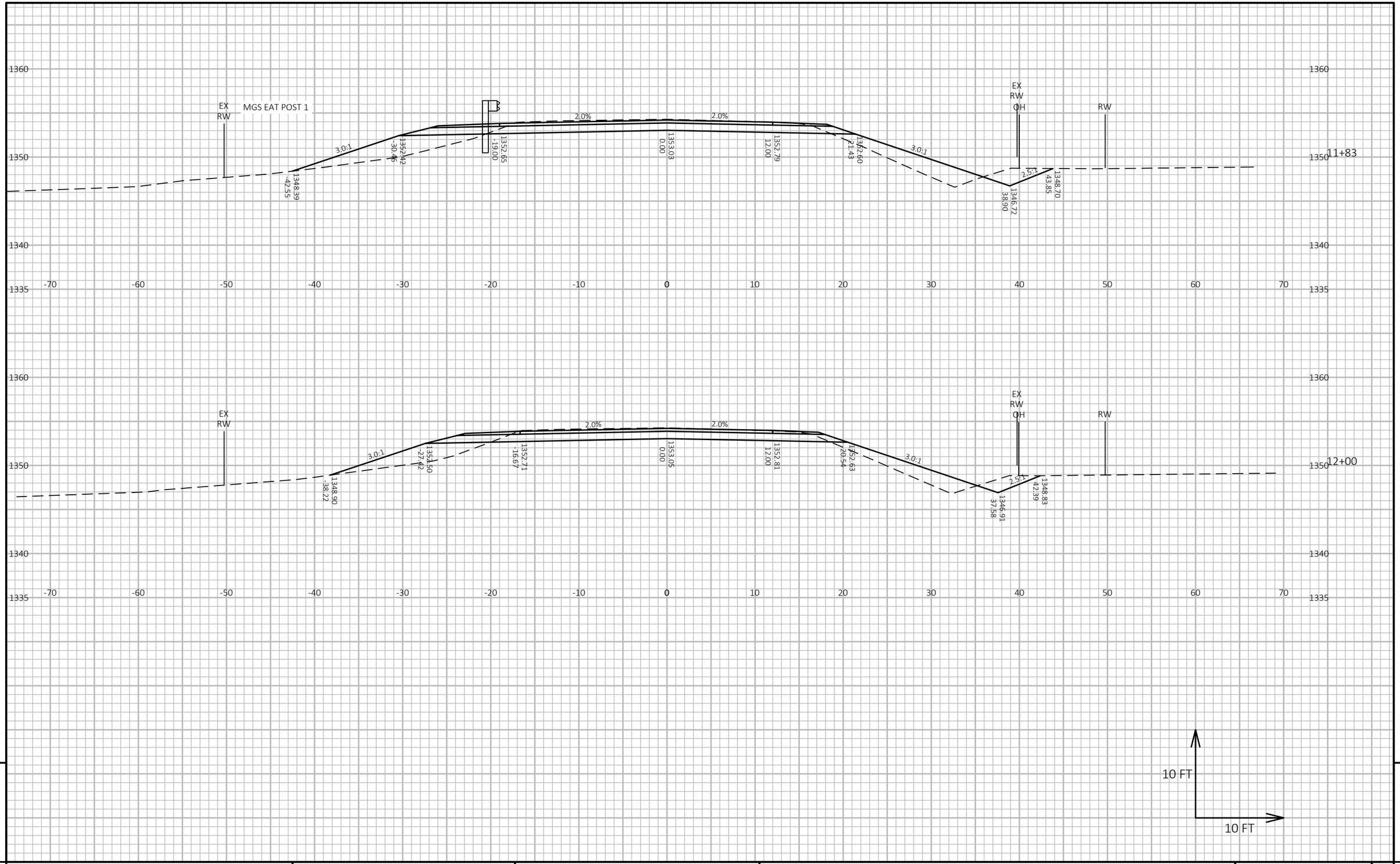
PROJECT NO: 8887-03-76	HWY: CTH A	COUNTY: TAYLOR	CROSS SECTIONS: CROSS SECTIONS	SHEET 70	E

FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\501_C3D_2022\88870306\SHEETS\090201-XS.DWG
LAYOUT NAME - 09

PLOT DATE : 5/14/2024 9:36 AM PLOT BY : NICHOLAS WATHKE, PE PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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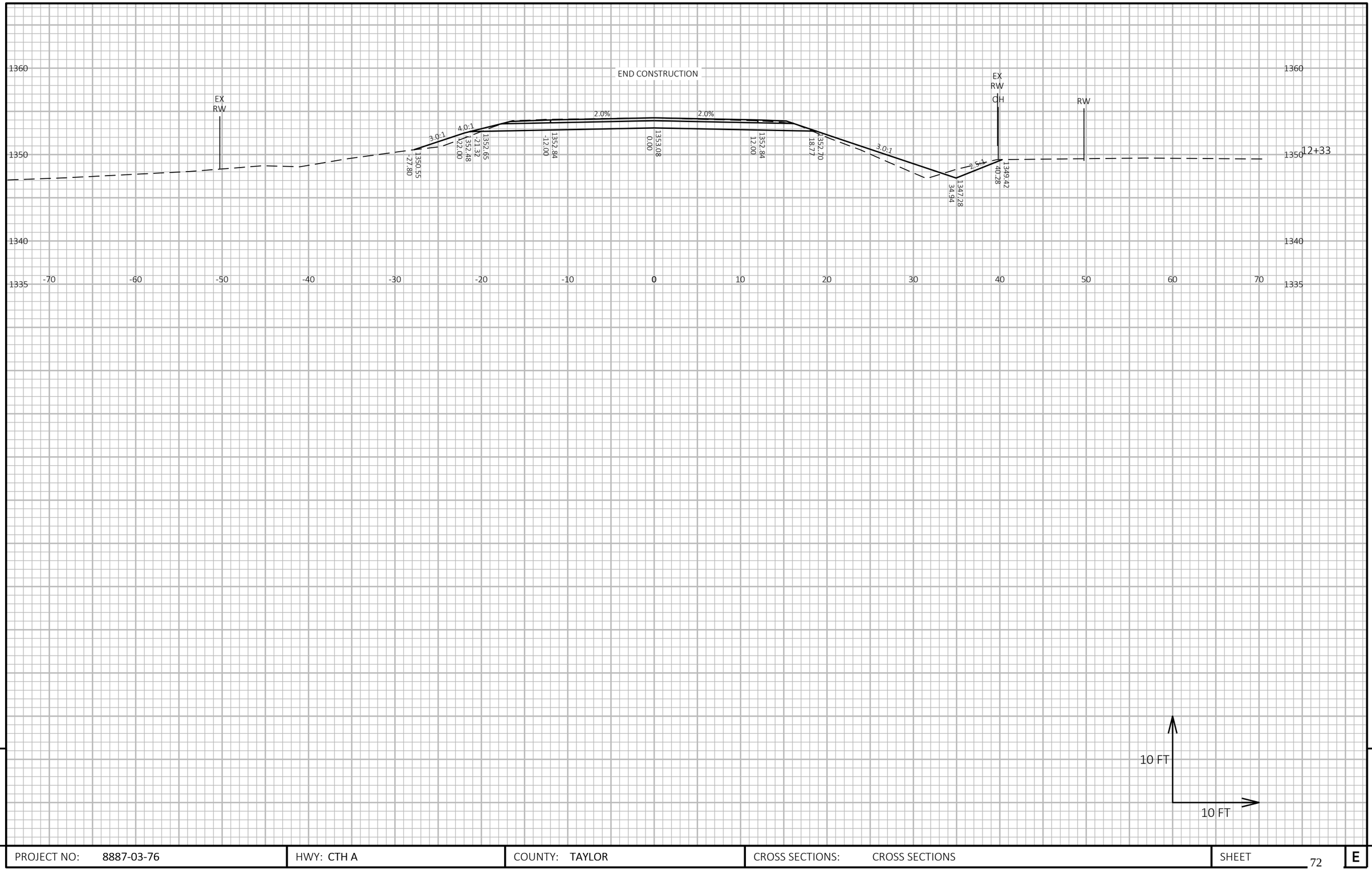
PROJECT NO: 8887-03-76	HWY: CTH A	COUNTY: TAYLOR	CROSS SECTIONS: CROSS SECTIONS	SHEET 71	E

FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\501_C3D_2022\88870306\SHEETS\090201-XS.DWG
LAYOUT NAME - 10

PLOT DATE : 5/14/2024 9:36 AM PLOT BY : NICHOLAS WATHKE, PE PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



PROJECT NO: 8887-03-76	HWY: CTH A	COUNTY: TAYLOR	CROSS SECTIONS: CROSS SECTIONS	SHEET 72	E

FILE NAME : C:\OD\CORRE, INC\PROJECTS - WI-NW REGION\8887-03-06_TAYLOR CO_CTH A\500_CADD\501_C3D_2022\88870306\SHEETS\090201-XS.DWG
LAYOUT NAME - 11

PLOT DATE : 5/14/2024 9:36 AM PLOT BY : NICHOLAS WATHKE, PE PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49

Notes

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