Jan 09, 2024

PROJECT ID: 5280-03-70

WITH: N/A

COUNTY: DANE COUNTY

ORDER OF SHEETS

TOTAL SHEETS = 202

N

DESIGN DESIGNATION

A.A.D.T.	2023	=	4900
A.A.D.T.	2043	=	6300
D.H.V.		=	
D.D.		=	
T.		=	12.8%
DESIGN SPEED		=	55 MPH (35 MPH S. OF KOPP RD.)
ESALS		=	1,500,000

CONVENTIONAL SYMBOLS

PLAN

- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE (300'EB')
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE

- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES

- ELECTRIC (E)
- FIBER OPTIC (FO)
- GAS (G)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

MADISON - LODI

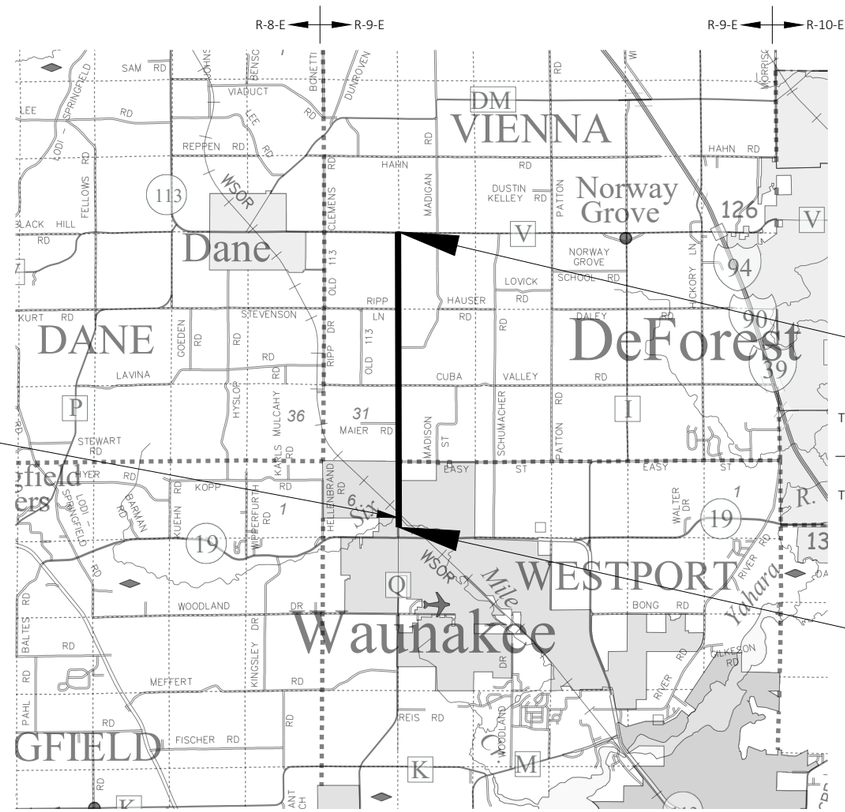
SUNSET LANE TO CTH V

STH 113

DANE COUNTY

STATE PROJECT NUMBER
5280-03-70

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5280-03-70	WISC 2024109	1



LAYOUT

SCALE 0 2 MI

TOTAL NET LENGTH OF CENTERLINE = 3.804 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), DANE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	MSA PROFESSIONAL SERVICES
Designer	MEGAN SCHERER, P.E.
Project Manager	AMY COUGHLIN, P.E.
Regional Examiner	SW REGION
Regional Supervisor	ALEX HAGEN, P.E.

APPROVED FOR THE DEPARTMENT

DATE: 10/12/2022 Amy Coughlin (Signature)

E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\010101- TI.DWG PLOT DATE : 10/12/2022 12:45 PM PLOT BY : SCHERER, MEGAN S PLOT NAME :

UTILITY CONTACTS

ELECTRIC TRANSMISSION

ATC MANAGEMENT INC.
DOUG VOSBERG
2489 RINDEN ROAD
COTTAGE GROVE, WI 53527
(608) 877-7650
dvosberg@atcllc.com

COMMUNICATIONS

SPECTRUM
BRAD TEMPLETON
2701 DANIELS STREET
MADISON, WI 53718
(608) 225-2529
brad.templeton@charter.com

COMMUNICATIONS

BRIGHTSPEED OF WESTERN WISCONSIN, LLC
JAMES WINTER
224 INDUSTRIAL DRIVE
NORTH PRAIRIE, WI 53153
(262) 392-5210
james.winter@brightspeed.com

ELECTRIC

ALLIANT ENERGY
KAI GRAFF
6462 BLANCHAR'S CROSSING
WINDSOR, WI 53598
(608) 459-5797
kaigraff@alliantenergy.com

COMMUNICATIONS

MCI
RANDOLPH CICATELLO JR.
15725 WEST RYERSON ROAD
NEW BERLIN, WI 53151
(262) 232-1323
randy.cicatello@verizon.com

GAS

MADISON GAS & ELECTRIC CO.
ROGER AHLES
623 RAILROAD STREET
MADISON, WI 53701
(608) 252-5682
rahles@mge.com

ELECTRIC

WAUNAKEE UTILITIES
JOHN MCLAIN
322 MORAVIAN VALLEY RD
P.O. BOX 70
WAUNAKEE, WI 53597
(608) 235-6441
jmclain@waunakeeutilities.com

SEWER & WATER

WAUNAKEE UTILITIES
RANDY DORN
322 MORAVIAN VALLEY RD
P.O. BOX 70
WAUNAKEE, WI 53597
(608) 849-4107
rdorn@waunakeeutilities.org

COMMUNICATIONS

SPRINT COMMUNCATIONS
DAN HILLIARD
849 EARL STREET
SAINT PAUL, MN 55106
(612) 217-3526
dan.j.hilliard@t-mobile.com

COMMUNICATIONS

TDS TELECOM
JERRY MYERS
525 JUNCTION RD
MADISON, WI 53717
(608) 664-4404
jerry.myers@tdstelecom.com

SEWER

VILLAGE OF DANE
SHANE CLAPPER
102 W MAIN ST
P.O. BOX 168
DANE, WI 53529-0168
(608) 849-5425
publicworks@villageofdane.org

COMMUNICATIONS

EVERSTREAM
JIM SAGAITIS
324 E WISCONSIN AVE
STE 370
MILWAUKEE, WI 53202
(608) 201-5586
jsagaitis@everstream.net

DIGGERS HOTLINE
Dial 811 or (800)242-8511
www.DiggersHotline.com

SECTION 2 ORDER OF SHEETS

RAILROAD

WISCONSIN & SOUTHERN RAILROAD
BEN MEIGHAN
1890 E. JOHNSON ST.
MADISON, WI 53704
(608) 620-2037

DNR LIAISON

ERIC HEGGELUND
DEPARTMENT OF NATURAL RESOURCES
3911 FISH HATCHERY ROAD
FITCHBURG, WI 53711
(608) 228-7927
eric.heggelund@wisconsin.gov

DESIGN CONTACTS

WISDOT DESIGNER

MEGAN SCHERER
WISDOT NORTHEAST REGION
944 VANDERPERREN WAY
GREEN BAY, WI 54304
(920) 492-7702
megan.scherer@dot.wi.gov

WISDOT PROJECT MANAGER

AMY COUGHLIN
WISDOT SOUTHWEST REGION
2101 WRIGHT STREET
MADISON, WI 54704
(608) 245-5358
amy.coughlin@dot.wi.gov

TOTAL LAYER PAVEMENT THICKNESS	PAVEMENT TYPE	LAYERS
6.25" HMA PAVEMENT	4 MT 58-28 S UPPER 3 MT 58-28 S LOWER	1.75" UPPER LAYER 2.25" LOWER LAYER 2.25" LOWER LAYER
4" HMA PAVEMENT	4 MT 58-28 S UPPER 3 MT 58-28 S LOWER	1.75" UPPER LAYER 2.25" LOWER LAYER

GENERAL NOTES

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN. COORDINATE CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

EROSION CONTROL FEATURES AS SHOWN IN THE PLANS ARE AT SUGGESTED LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE CONTRACTORS'S EROSION CONTROL IMPLEMENTATION PLAN (ECIP) AND APPROVED BY THE ENGINEER IN CONSULTATION WITH THE WISCONSIN DEPARTMENT OF NATURAL RESOURCES. MAINTAIN ALL EROSION CONTROL MEASURES UNTIL SUCH TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NECESSARY.

EXISTING SIGNS SHALL REMAIN IN PLACE UNLESS MOVED AS PART OF THE PLAN OR THE ENGINEER APPROVES THEIR REMOVAL.

SIGN LOCATIONS SHALL BE REVIEWED BY THE ENGINEER PRIOR TO INSTALLATION.

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY SHALL BE RESTORED AS DIRECTED BY THE ENGINEER.

REMOVAL ITEMS SHALL BE REMOVED TO AN EXISTING JOINT, SAWCUT WHERE SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER.

THE LOCATION AND WIDTH OF DRIVEWAYS WILL BE DETERMINED BY THE ENGINEER.

RADIUS DIMENSIONS FOR THE CURB AND GUTTER ARE TO THE FLANGE LINE UNLESS OTHERWISE NOTED.

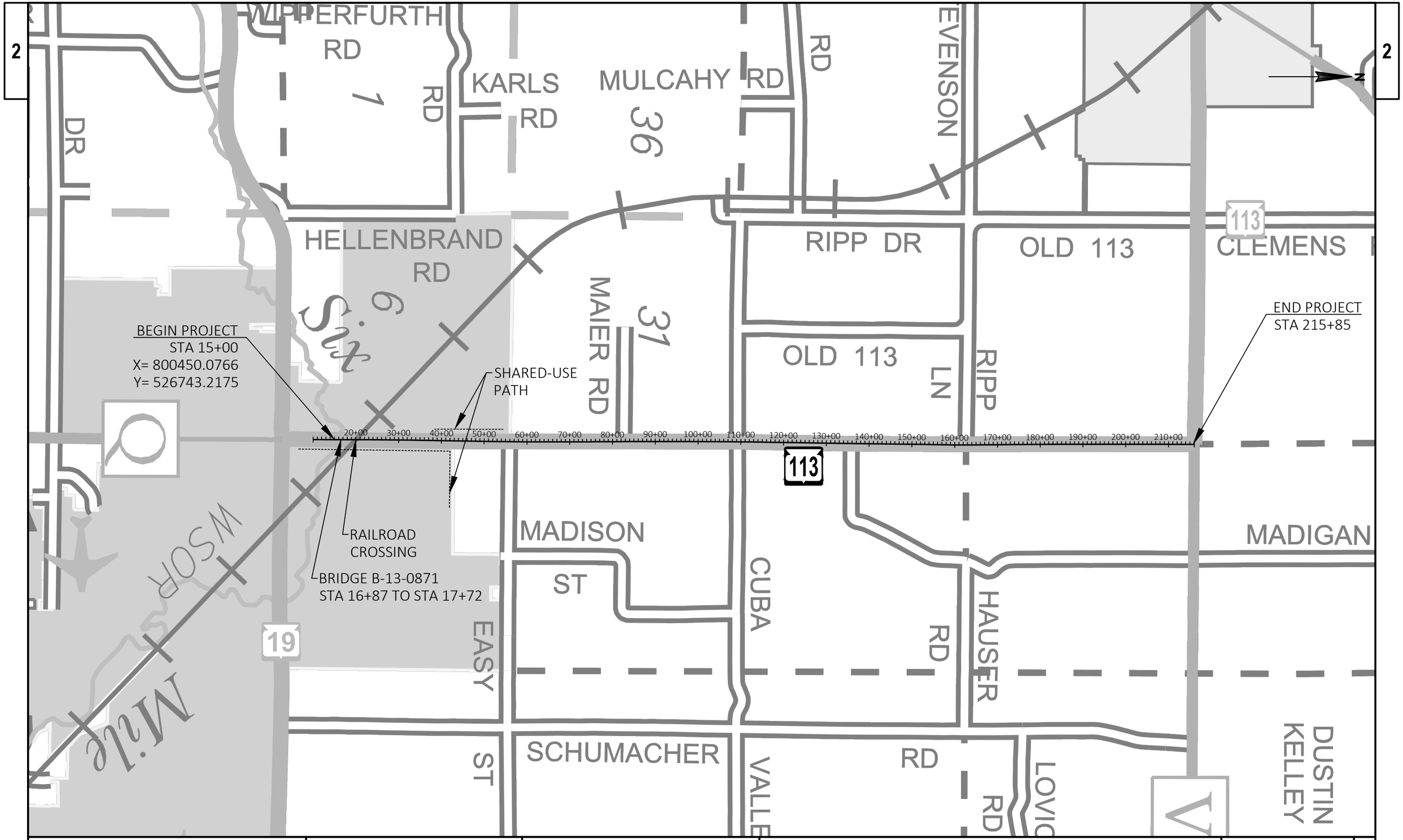
THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING OR TURNING LANE.

HMA PAVEMENT CALCULATIONS ARE BASED ON 112 LB/SY/IN.

EXISTING ASPHALT THICKNESS

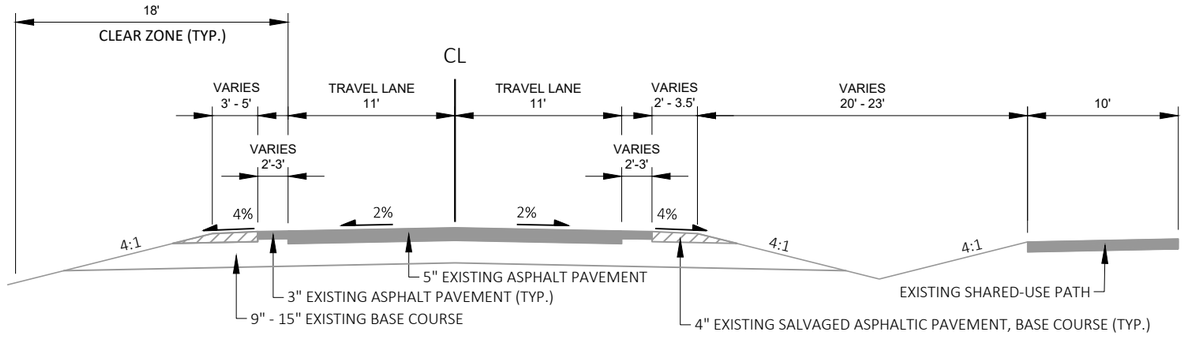
APPROXIMATE STATION	LANE MILE	OFFSET	THICKNESS (INCHES)
10+35	0.00	15' RT	6.00
23+55	0.25	6' LT	4.25
38+33	0.53	11' RT	3.50
51+00	0.77	9' LT	4.25
63+15	1.00	6' RT	2.75
76+35	1.25	10' LT	3.50
89+55	1.50	9' RT	3.50
102+75	1.75	6' LT	4.00
115+95	2.00	9' RT	8.50
130+73	2.28	9' LT	4.50
142+35	2.50	11' RT	8.00
158+19	2.80	11' LT	5.50
168+75	3.00	6' RT	7.25
182+48	3.26	9' LT	7.00
199+90	3.59		7.00
208+35	3.75	11' LT	4.75

NOTE: RED TOPS FOR REESTABLISHING GRADES ARE AVAILABLE IN THE EVENT BASE IS ENCOUNTERED

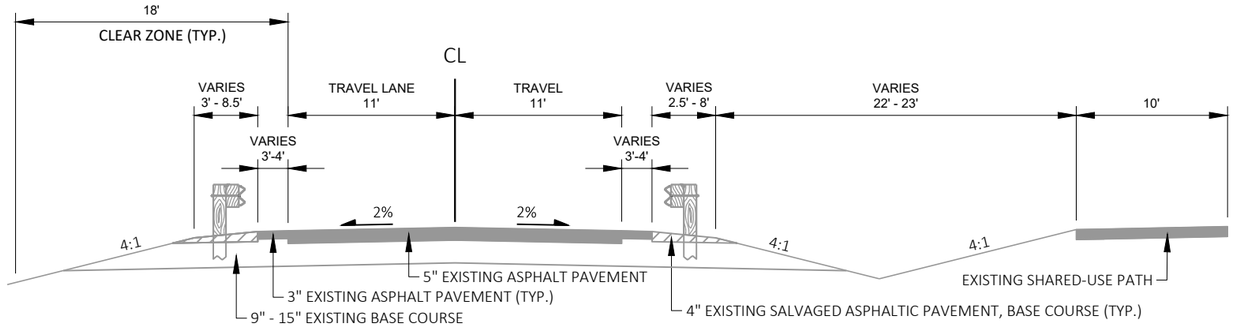


PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PROJECT OVERVIEW	SHEET 3	E

2



TYPICAL EXISTING SECTION
STA 15+00 - STA 16+32

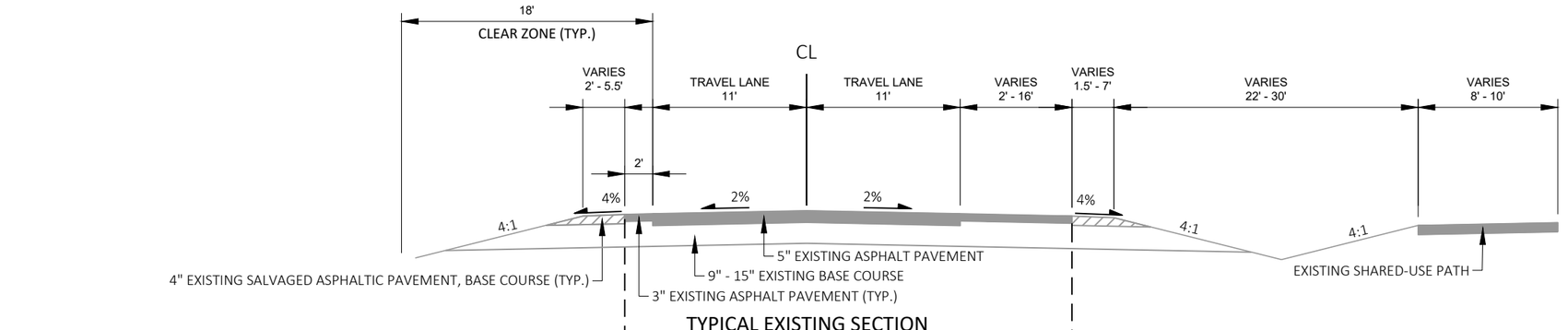


TYPICAL EXISTING SECTION
STA 16+32 - STA 18+27

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	EXISTING TYPICAL SECTIONS	SHEET 4	E

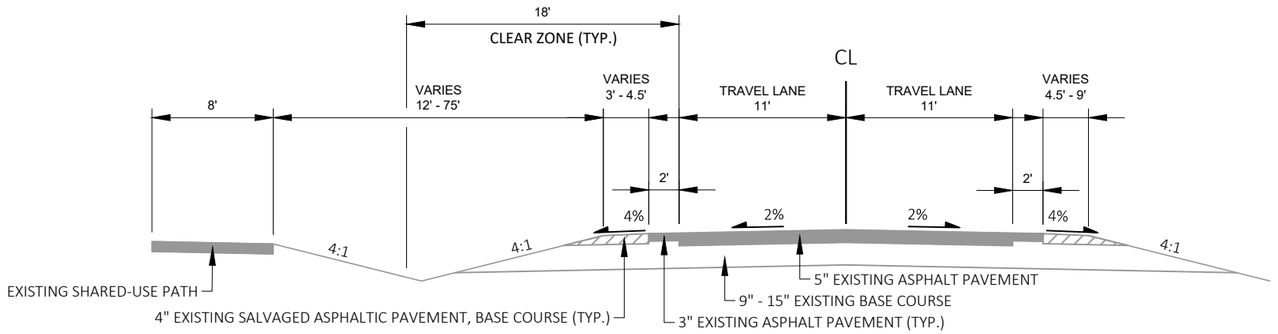
FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\020301-TS.DWG
LAYOUT NAME - 020301-ts
PLOT DATE : 10/21/2022 7:02 AM
PLOT BY : SCHERER, MEGAN S
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42

2



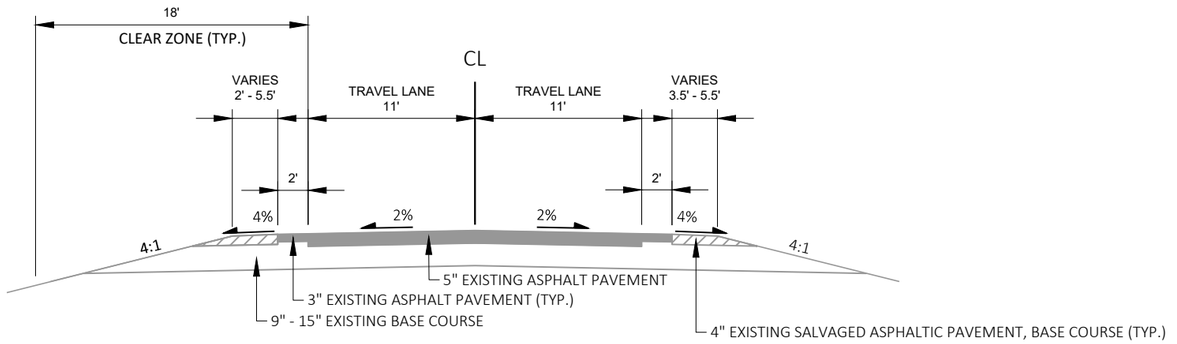
TYPICAL EXISTING SECTION
STA 18+27 - STA 38+86

TYPICAL EXISTING SECTION
STA 38+86 - STA 43+38

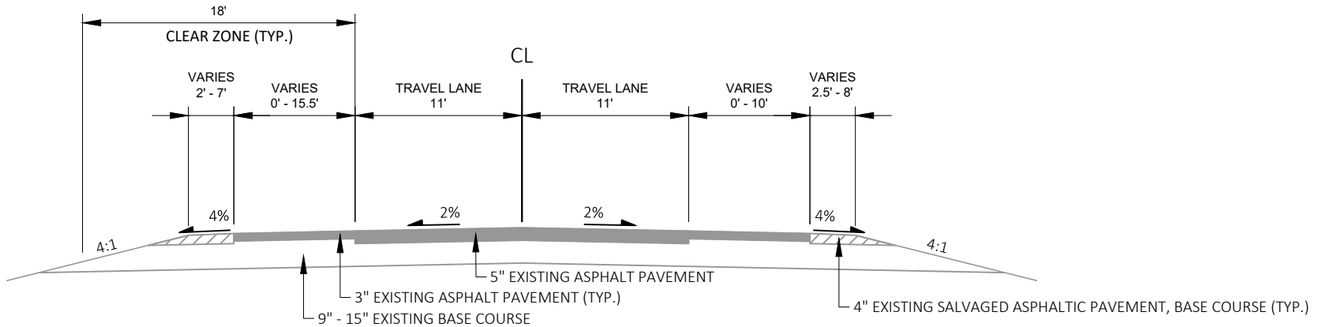


TYPICAL EXISTING SECTION
STA 43+38 - STA 56+60

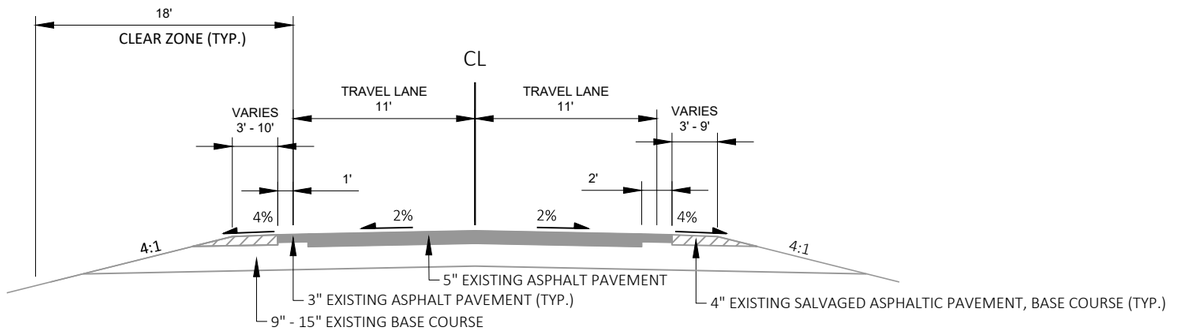
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	EXISTING TYPICAL SECTIONS	SHEET 5	E



TYPICAL EXISTING SECTION
STA 56+60 - STA 105+44

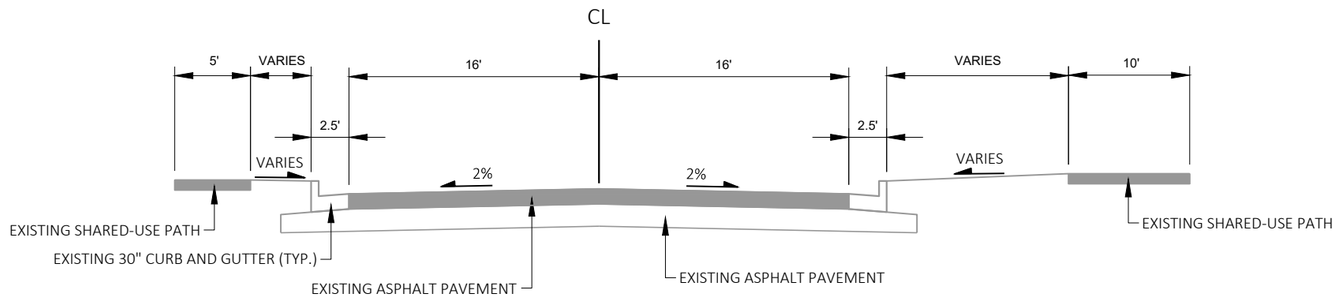


TYPICAL EXISTING SECTION
STA 105+44 - STA 114+39



TYPICAL EXISTING SECTION
STA 114+39 - STA 215+85

2

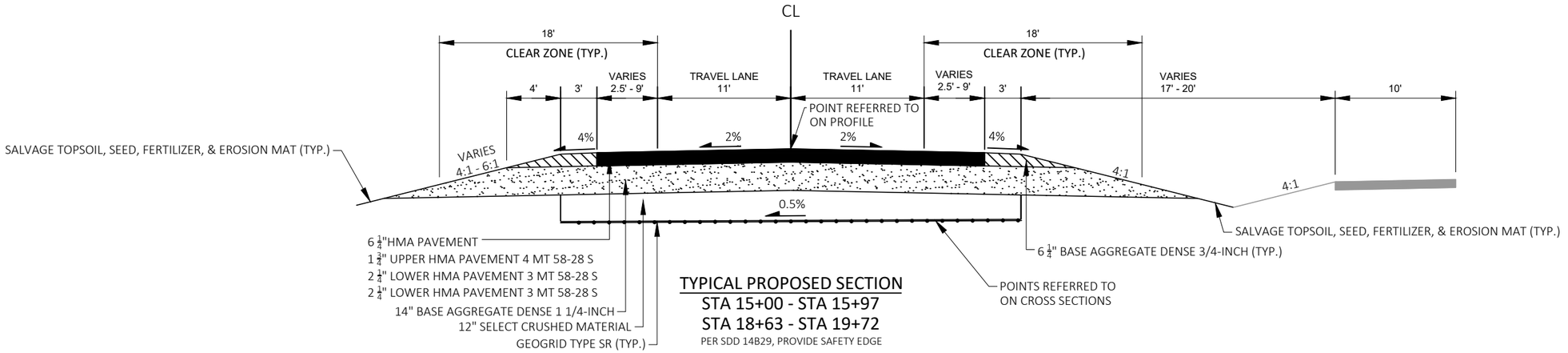


TYPICAL EXISTING SECTION
KOPP ROAD

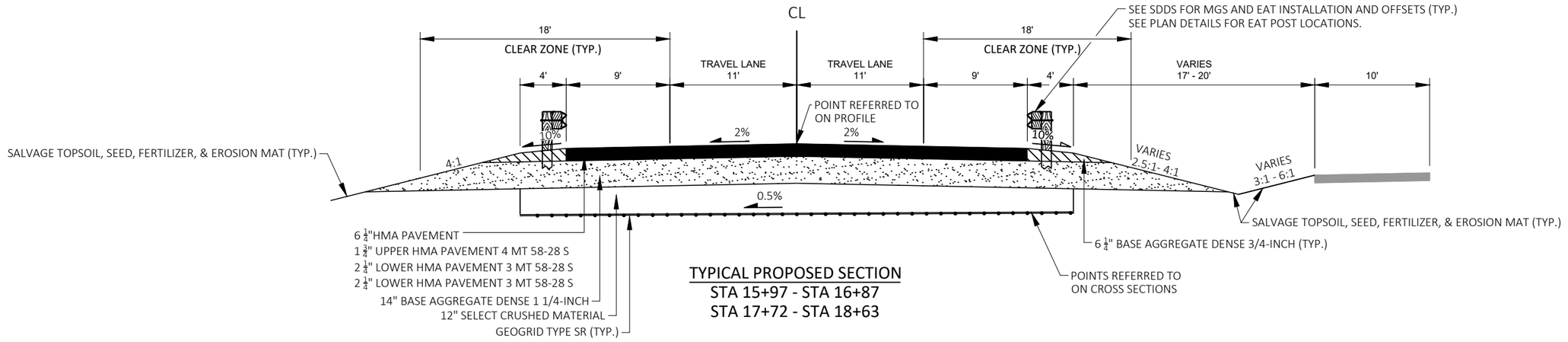
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	EXISTING TYPICAL SECTION - KOPP ROAD	SHEET 7	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\020301-TS.DWG
LAYOUT NAME - 020304-ts
PLOT DATE : 10/21/2022 7:02 AM
PLOT BY : SCHERER, MEGAN S
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42

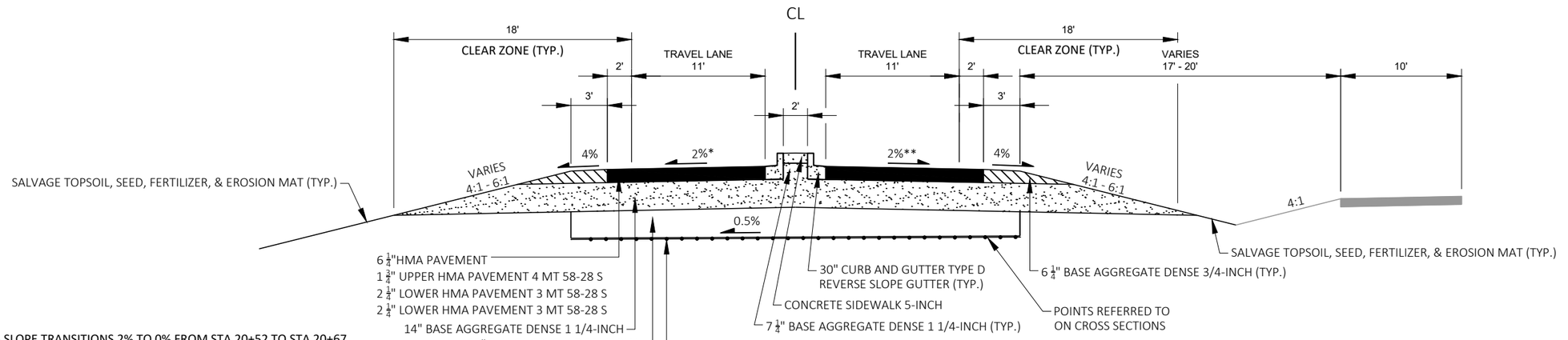
2



TYPICAL PROPOSED SECTION
STA 15+00 - STA 15+97
STA 18+63 - STA 19+72
PER SDD 14B29, PROVIDE SAFETY EDGE



TYPICAL PROPOSED SECTION
STA 15+97 - STA 16+87
STA 17+72 - STA 18+63

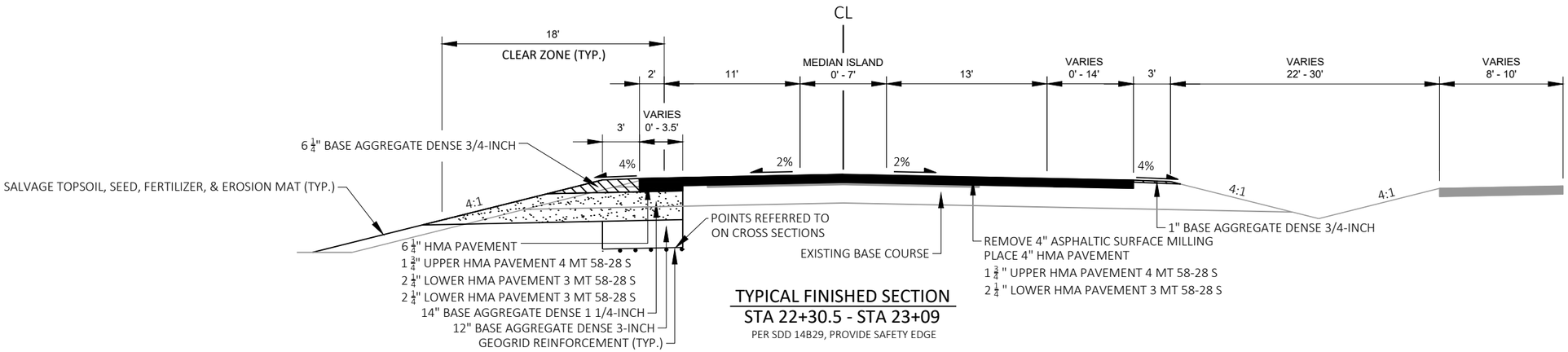


* CROSS SLOPE TRANSITIONS 2% TO 0% FROM STA 20+52 TO STA 20+67
AND 0% TO 2% FROM STA 21+07 TO STA 21+47.

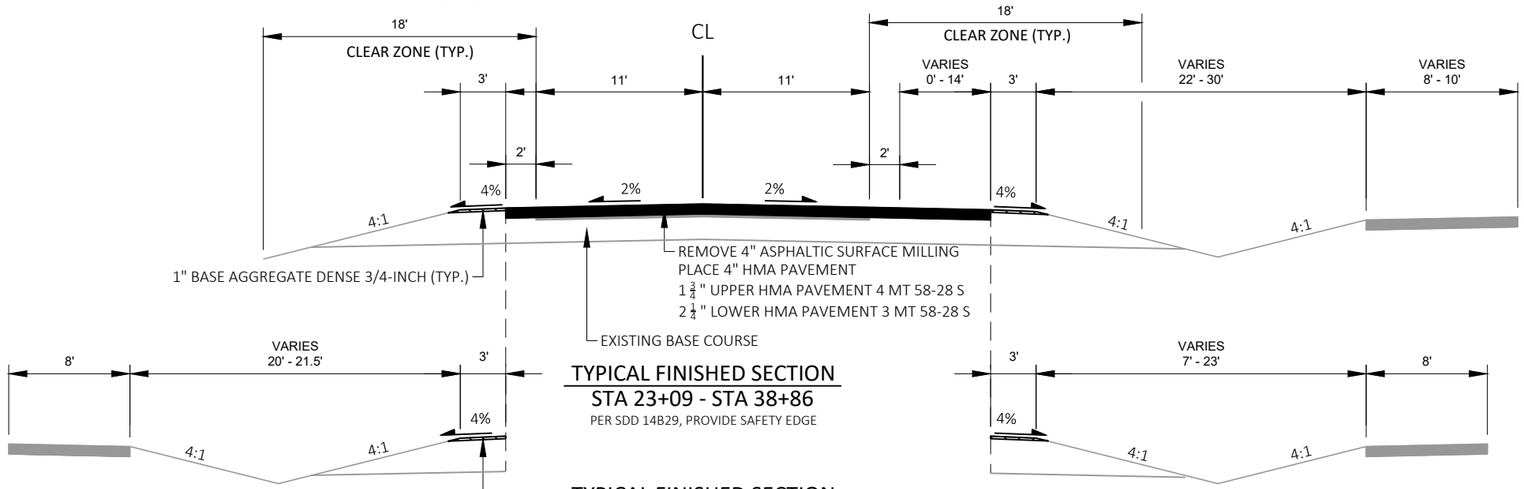
** CROSS SLOPE TRANSITIONS 2% TO 0% FROM STA 20+06 TO STA 20+46
AND 0% TO 2% FROM STA 20+87 TO STA 21+27.

TYPICAL PROPOSED SECTION
STA 19+72 - STA 22+30.5
PER SDD 14B29, PROVIDE SAFETY EDGE

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PROPOSED TYPICAL SECTIONS	SHEET 8	E

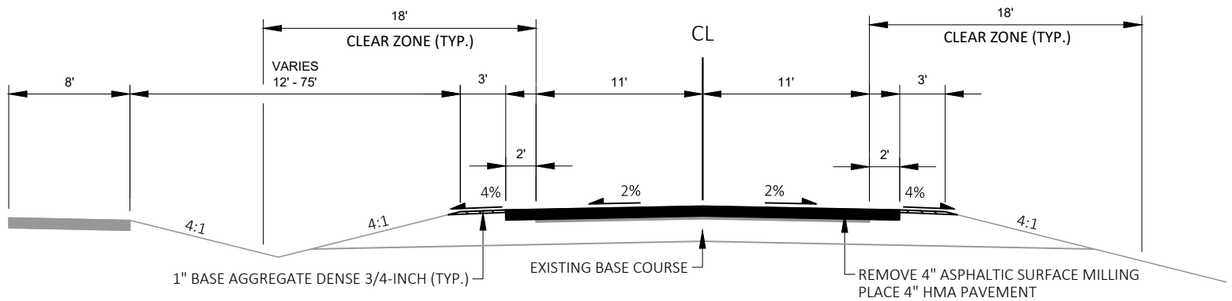


TYPICAL FINISHED SECTION
STA 22+30.5 - STA 23+09
PER SDD 14B29, PROVIDE SAFETY EDGE



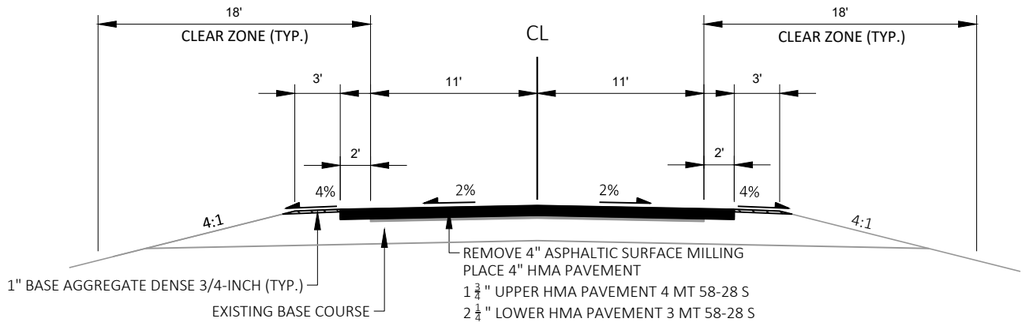
TYPICAL FINISHED SECTION
STA 23+09 - STA 38+86
PER SDD 14B29, PROVIDE SAFETY EDGE

TYPICAL FINISHED SECTION
STA 38+86 - STA 43+38
PER SDD 14B29, PROVIDE SAFETY EDGE

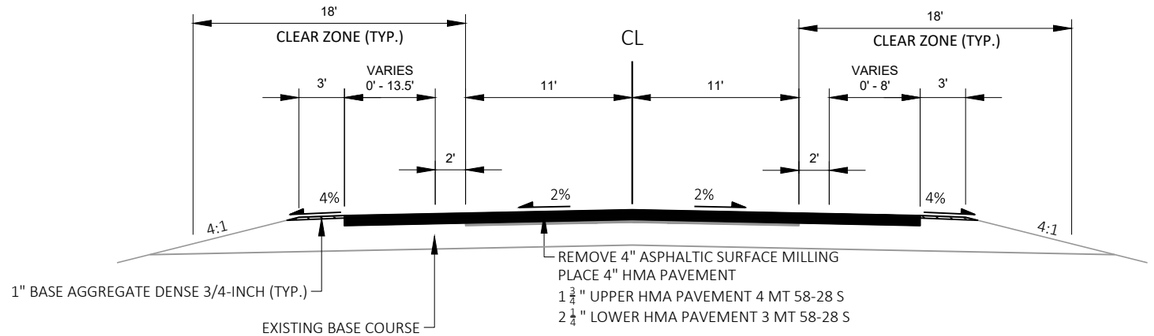


TYPICAL FINISHED SECTION
STA 43+38 - STA 56+60
PER SDD 14B29, PROVIDE SAFETY EDGE

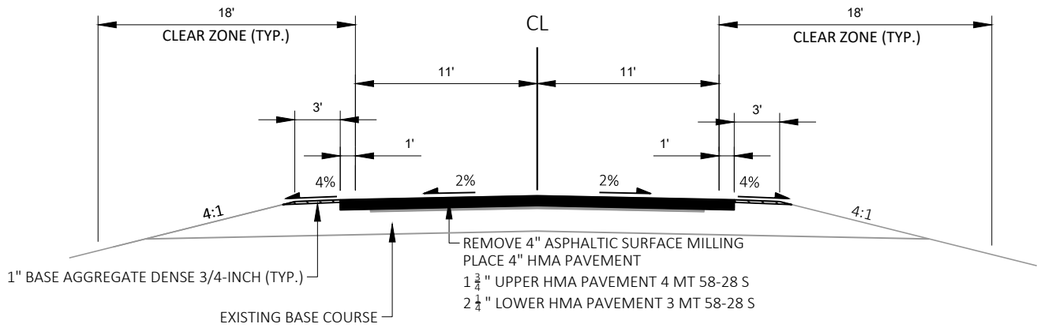
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	FINISHED TYPICAL SECTIONS	SHEET 9	E



TYPICAL FINISHED SECTION
STA 56+60 - STA 105+44
PER SDD 14B29, PROVIDE SAFETY EDGE

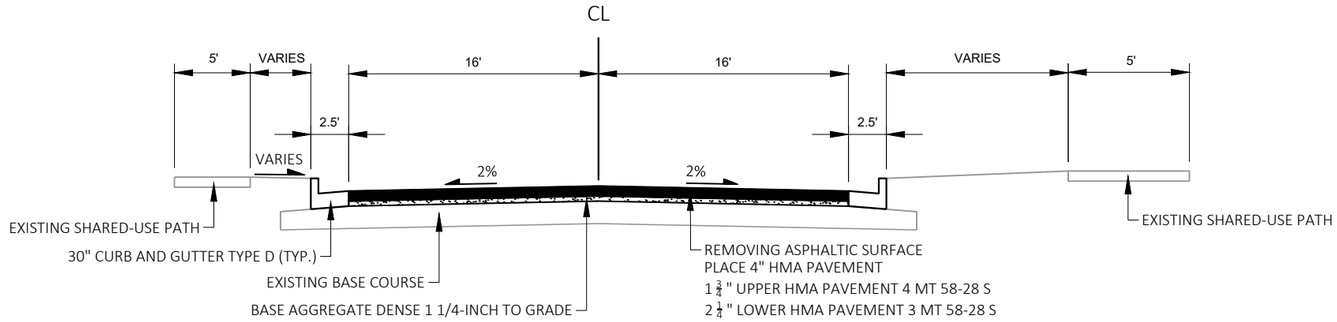


TYPICAL FINISHED SECTION
STA 105+44 - STA 114+39
PER SDD 14B29, PROVIDE SAFETY EDGE

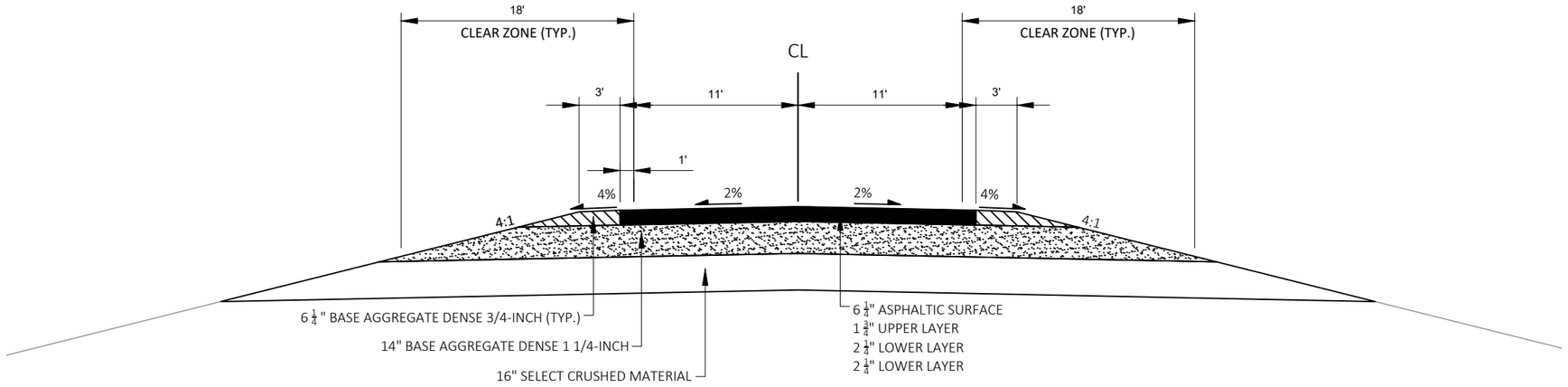


TYPICAL FINISHED SECTION
STA 114+39 - STA 215+85
PER SDD 14B29, PROVIDE SAFETY EDGE

2



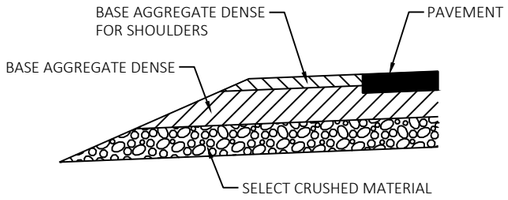
TYPICAL PROPOSED SECTION
KOPP ROAD



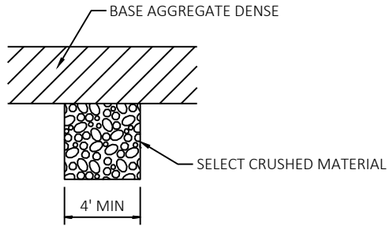
TYPICAL PROPOSED SECTION
AT CULVERT REPLACEMENT
SEE PLAN DETAILS: CULVERTS FOR LIMITS
EBS AREAS TO USE THIS PAVEMENT STRUCTURE

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PROPOSED TYPICAL SECTIONS	SHEET 11	E

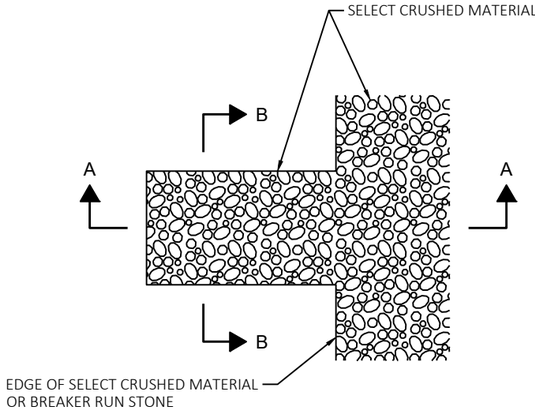
FILE NAME : \\FIWMAD7P3158\N3PUBLIC\PDS\C3D\52800300\SHEETSPLAN\020301-TS.DWG
LAYOUT NAME - 020308-ts
PLOT DATE : 11/14/2023 9:02 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42



SECTION A-A



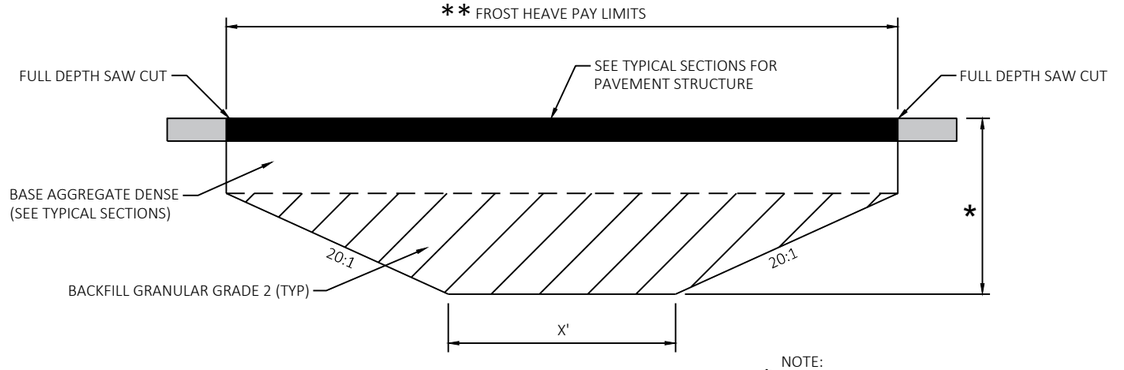
SECTION B-B



DETAIL FOR FRENCH DRAINS

DRAINS ARE TO BE CONSTRUCTED AT THE FOLLOWING STATIONS: STA 16+50 LT, STA 18+00 LT, & STA 22+30 LT OR AS DIRECTED BY ENGINEER.

EXCAVATION REQUIRED TO CONSTRUCT FRENCH DRAINS SHALL BE CONSIDERED INCIDENTAL TO THE ITEM SELECT CRUSHED MATERIAL.

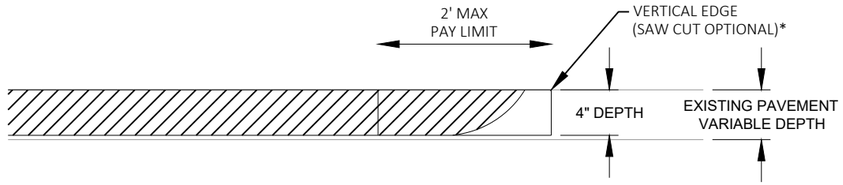


* NOTE:
* EXCAVATE 5' BELOW TOP OF FINISHED PAVEMENT.

** PAID AS COMMON EXCAVATION.

LONGITUDINAL DETAIL FOR FROST HEAVE REPAIR AREA WITHOUT PIPE

STA 22+07



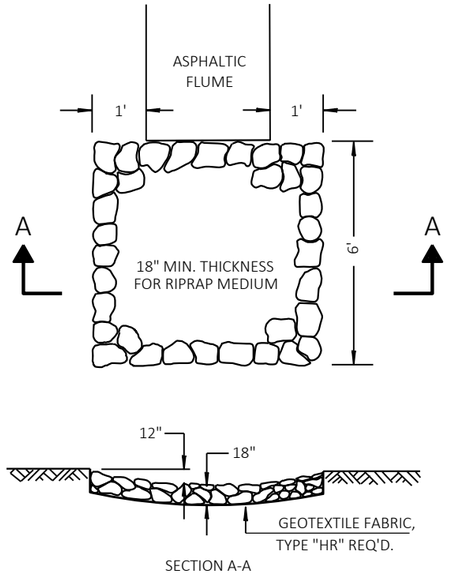
REMOVING ASPHALTIC SURFACE MILLING

REMOVING ASPHALTIC SURFACE BUTT JOINT

*INCIDENTAL TO "REMOVING ASPHALTIC SURFACE BUTT JOINT" ITEM

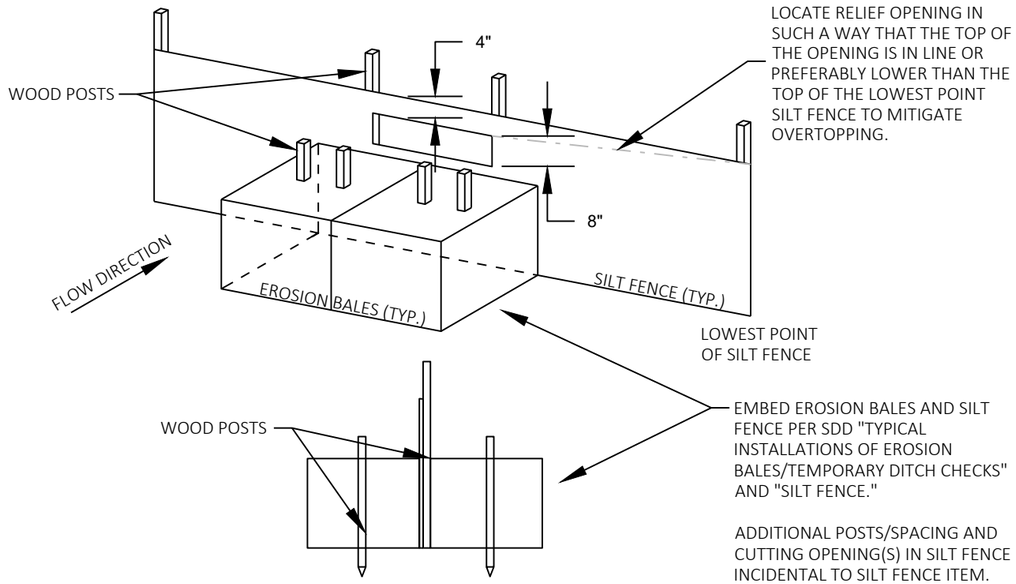
BUTT JOINT DETAIL

W VERLEEN AVENUE (RT)
LOCHMOORE DRIVE (RT)
LEGENDS DRIVE (RT)
KOPP ROAD (LT)
EASY STREET (RT)
MAIER ROAD (LT)
CUBA VALLEY ROAD (LT/RT)
MADIGAN ROAD (RT)
RIPP LANE (LT)
END OF PROJECT (STA 215+85)



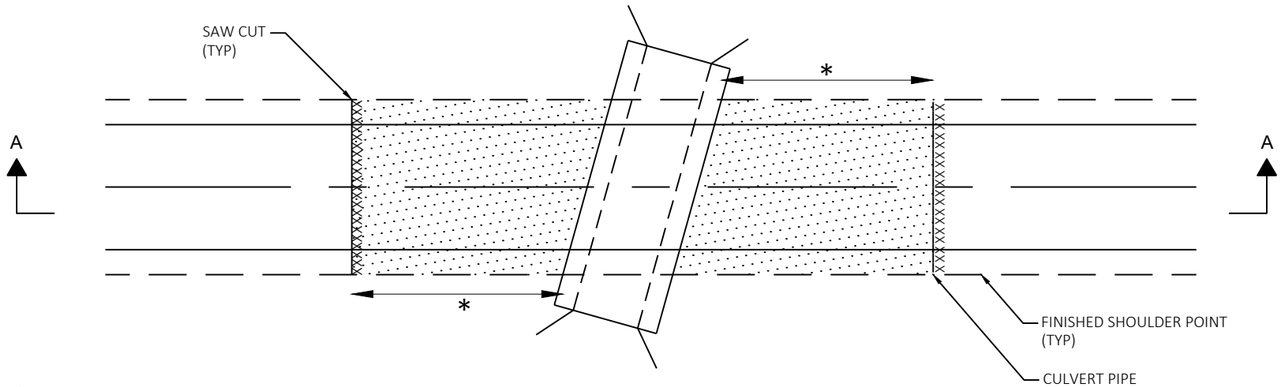
RIPRAP MEDIUM TREATMENT AT FLUMES DETAIL

SEE EROSION CONTROL PLAN FOR LOCATIONS

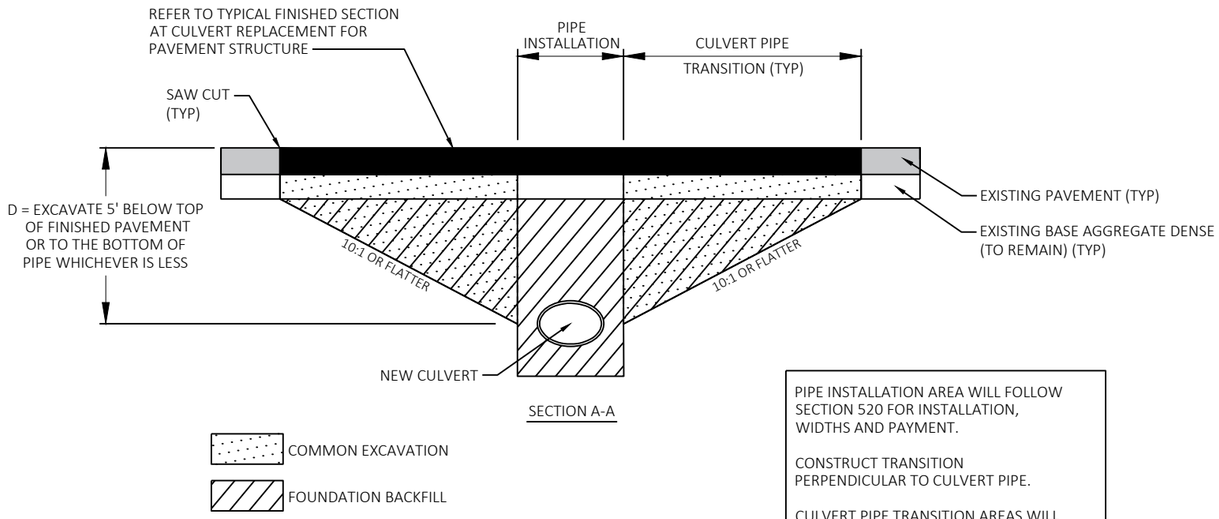


BALE & SILT FENCE RELIEF DETAIL

SEE EROSION CONTROL PLAN FOR LOCATIONS



* SEE MISCELLANEOUS QUANTITIES FOR CULVERT PIPE TRANSITION LIMITS.



SECTION A-A

PIPE INSTALLATION AREA WILL FOLLOW SECTION 520 FOR INSTALLATION, WIDTHS AND PAYMENT.

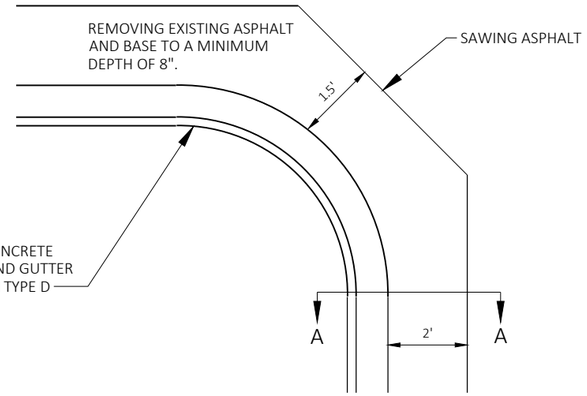
CONSTRUCT TRANSITION PERPENDICULAR TO CULVERT PIPE.

CULVERT PIPE TRANSITION AREAS WILL BE PAID BY COMMON EXCAVATION & SPV FOUNDATION BACKFILL.

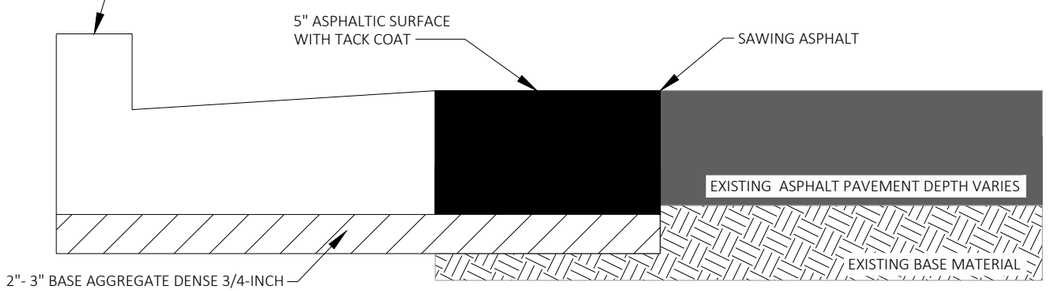
PAVEMENT SAW CUT TO BE PERPENDICULAR TO ROADWAY ALIGNMENT.

SHALLOW PIPES WITH TRANSITION

STA 210+41

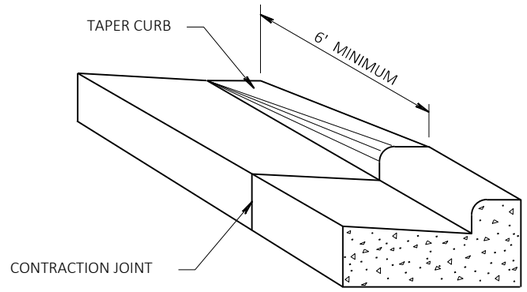


PLAN VIEW



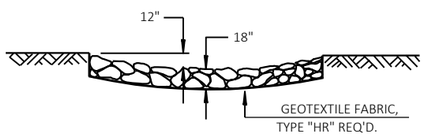
SECTION A-A

INTERMEDIATE PAVING AROUND NEW CURB AND GUTTER



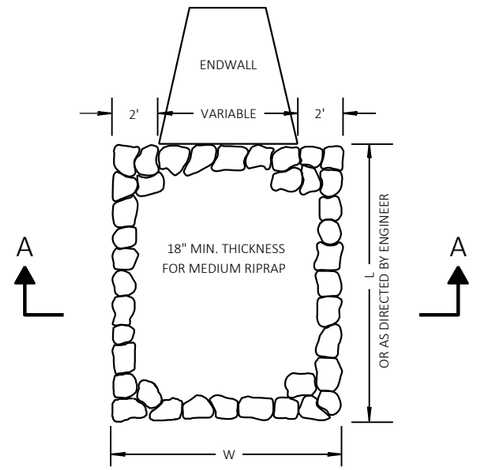
DETAIL OF CURB & GUTTER TERMINI

STA 16+28 RT
STA 16+28 LT
STA 20+56 RT
STA 20+58 LT
STA 20+95 RT
STA 20+98 LT
STA 42+80 LT
STA 44+14 LT



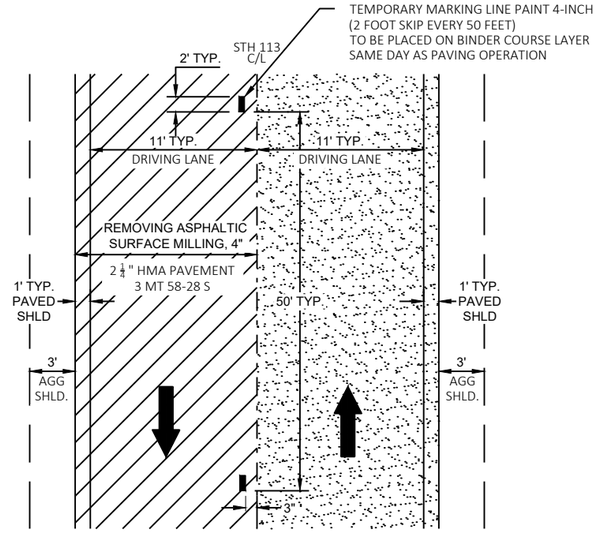
SECTION A-A

ENDWALL LOCATION	W	L
STA 170+33 RT	10'	8'
STA 210+41 LT	11'	4'

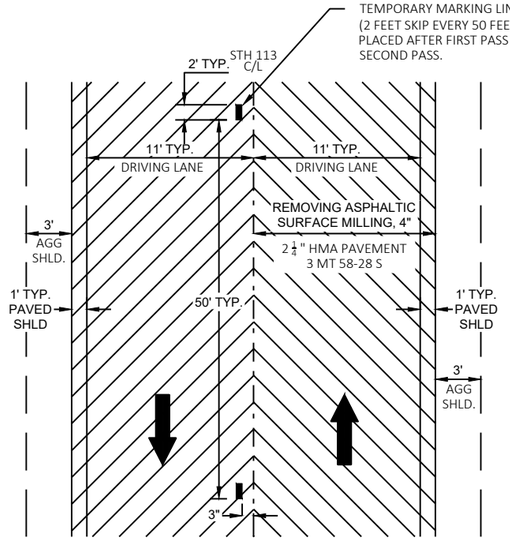


RIPRAP TREATMENT AT CULVERTS

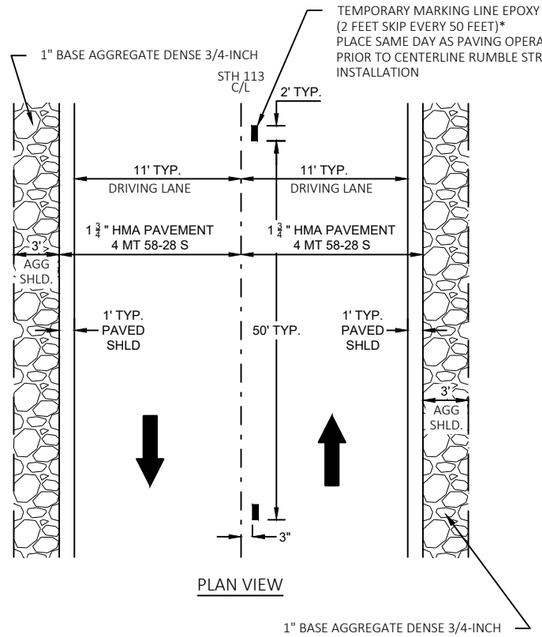
STA 170+33
STA 210+41



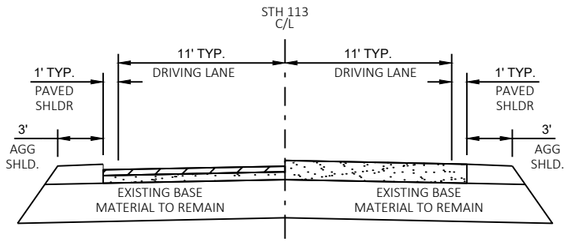
PLAN VIEW



PLAN VIEW



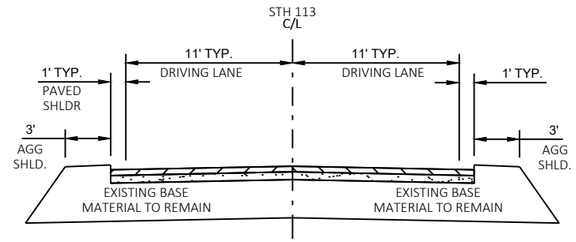
PLAN VIEW



CROSS SECTION VIEW

LONGITUDINAL PAVEMENT JOINT DETAIL - FIRST PASS

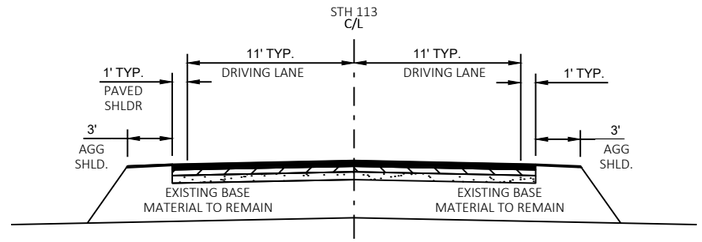
STA 22+30.5 - STA 215+85



CROSS SECTION VIEW

LONGITUDINAL PAVEMENT JOINT DETAIL - SECOND PASS

STA 22+30.5 - STA 215+85



CROSS SECTION VIEW

LONGITUDINAL PAVEMENT JOINT DETAIL - THIRD PASS

STA 22+30.5 - STA 215+85

EXISTING ASPHALTIC SURFACE TO REMAIN

FIRST PASS 2-1/4" HMA PAVEMENT 3 MT 58-28 S PAVING LIMITS

SECOND PASS 2-1/4" HMA PAVEMENT 3 MT 58-28 S PAVING LIMITS

THIRD PASS 1-3/4" HMA PAVEMENT 4 MT 58-28 S PAVING LIMITS

1" BASE AGGREGATE DENSE 3/4-INCH

NOTES

BINDER LAYER MUST BE PAVED PRIOR TO MILLING ADJACENT TRAVELED LANE.

DIFFERING HEIGHT GREATER THAN 2" BETWEEN ADJACENT TRAVELED LANES WILL NOT BE PERMITTED OVERNIGHT.

SEE STANDARD DETAIL DRAWING 15C8 PAVEMENT MARKING, MAINLINE AND TURN LANES AND 15C33 STOP LINE AND CROSSWALK PAVEMENT MARKING FOR PERMANENT PAVEMENT MARKING DETAILS.

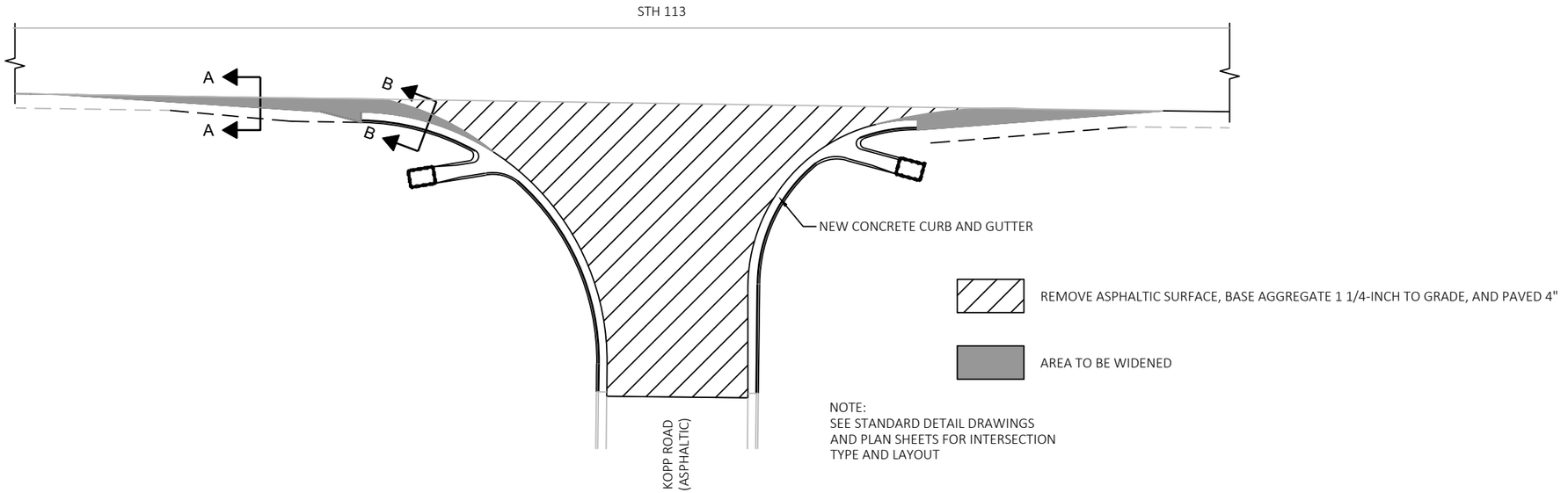
DRAINAGE TO BE MAINTAINED DURING CONSTRUCTION.

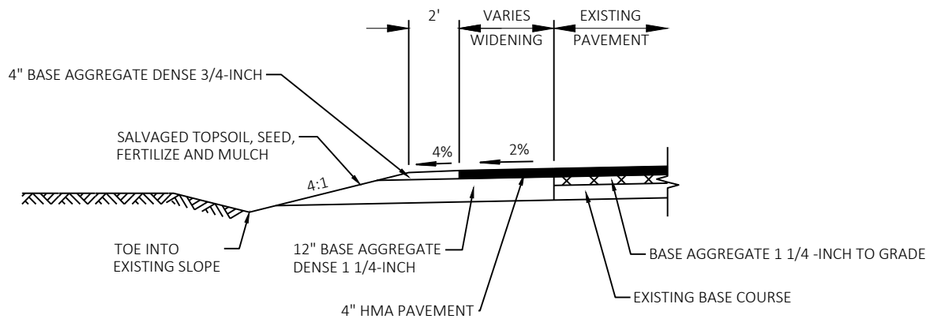
NOTCH WEDGE JOINT TO BE USED ON SURFACE LAYER.

ONLY ONE SET OF PAVEMENT MARKINGS WILL BE VISIBLE TO MOTORIST WITH EACH PAVEMENT PASS.

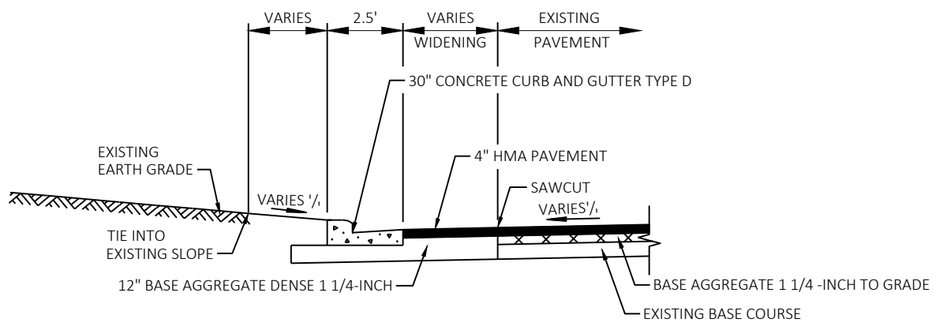
*TEMPORARY PAVEMENT MARKING TO BE PLACED IN LOCATION OF PERMANENT MARKINGS. MARKING LINE EPOXY 4-INCH REQUIRED AFTER INSTALLING CENTERLINE RUMBLE STRIPS.

2





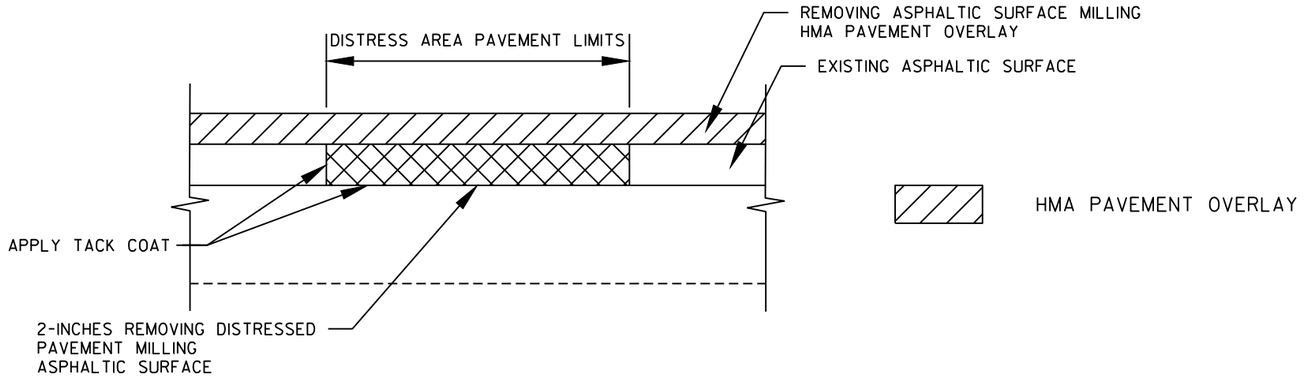
SECTION FOR WIDENING ALONG MAINLINE TAPERS
(A-A SECTION)



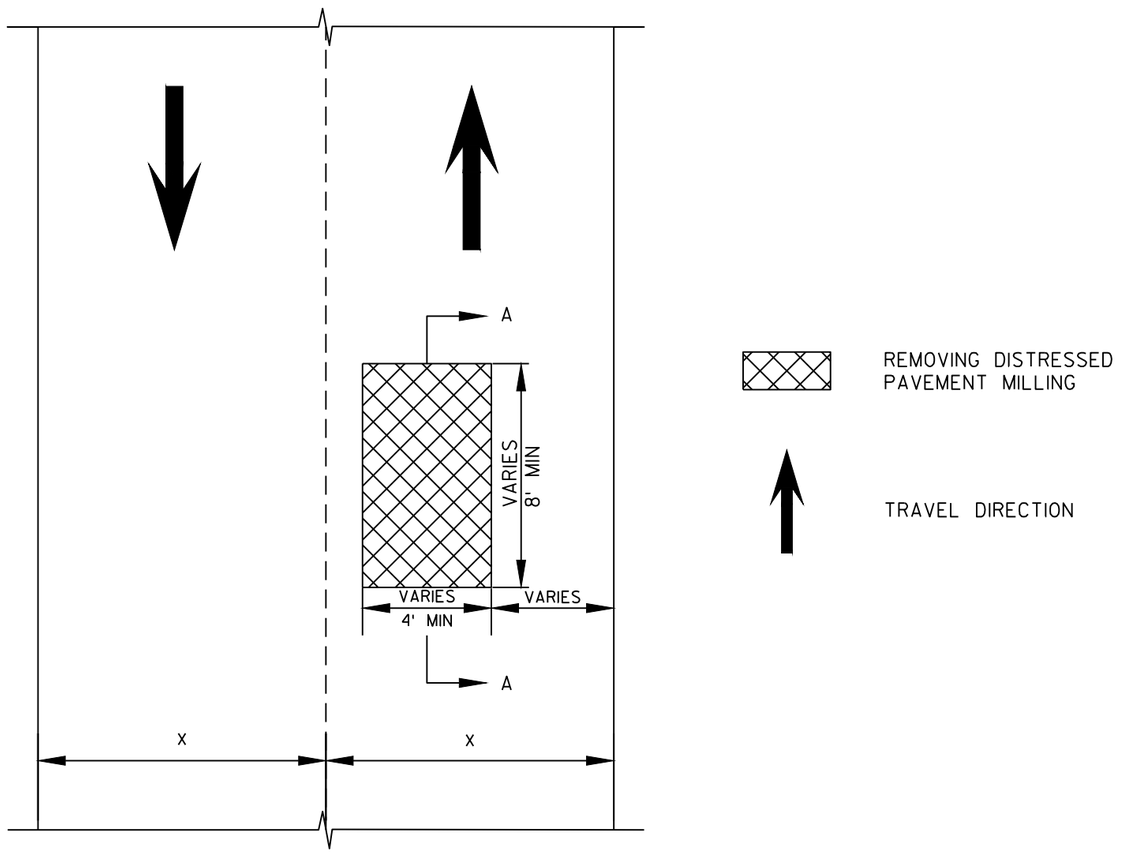
DETAIL FOR INTERSECTION WIDENING IN CURB SECTIONS
(B-B SECTION)

KOPP ROAD INTERSECTION WIDENING DETAIL

2

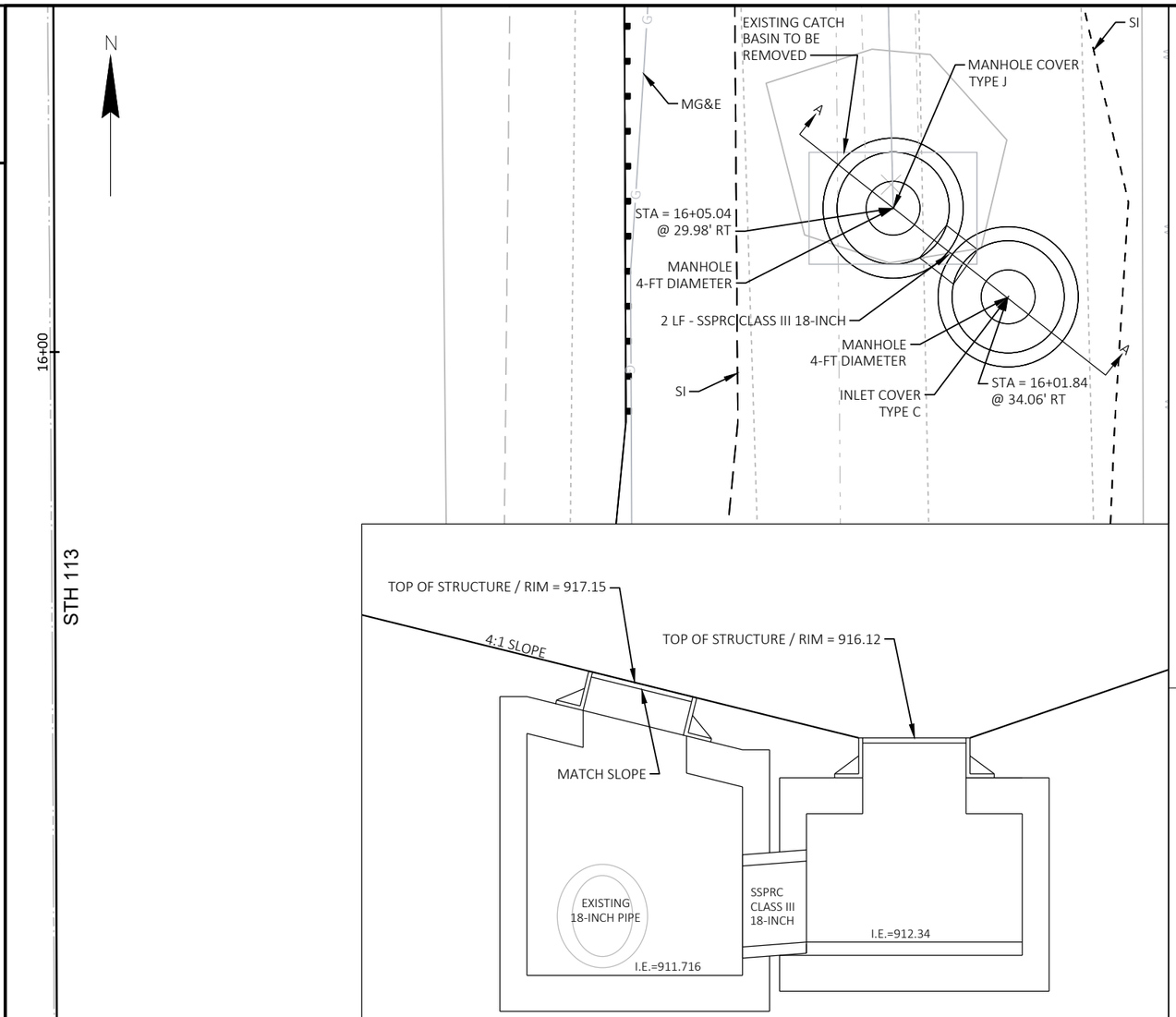


REMOVING DISTRESS PAVEMENT MILLING
SECTION A-A



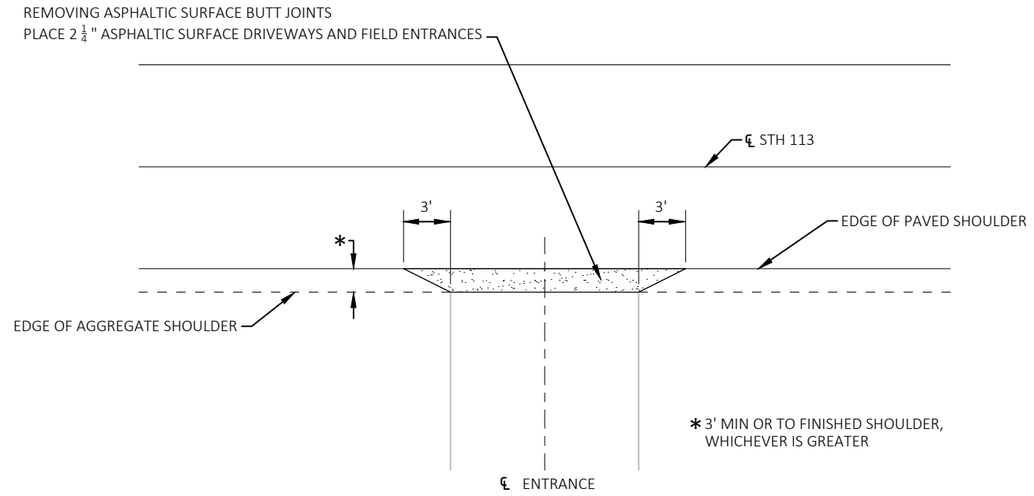
PLAN VIEW

EXACT LOCATION AND LIMITS OF REMOVING DISTRESSED PAVEMENT MILLING TO BE DETERMINED BY THE ENGINEER IN THE FIELD

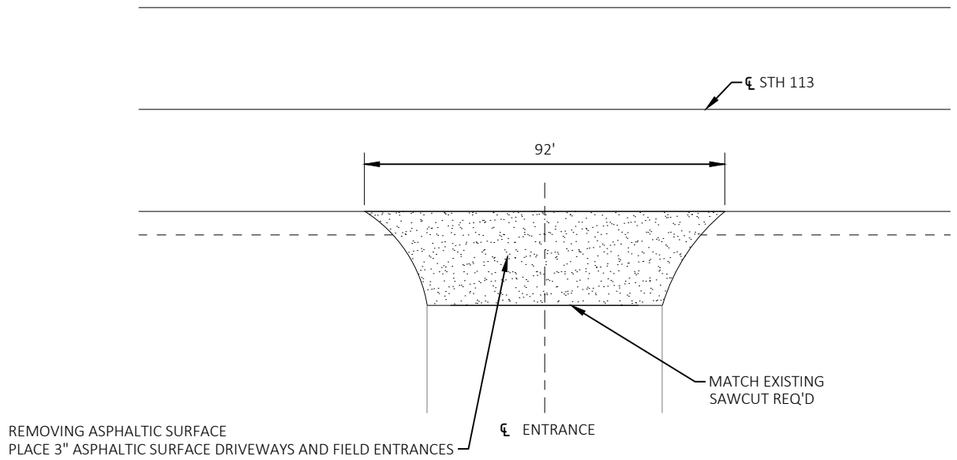


SECTION A - A

STORM SEWER MODIFICATIONS



RURAL EXISTING ASPHALT DRIVEWAY DETAIL



COMMERCIAL DRIVEWAY DETAIL

19+56 LT

2

SI

ATC

C/L

SEE EROSION CONTROL PLAN AND RIPRAP MEDIUM TREATMENT AT FLUMES FOR RIPRAP MEDIUM LOCATIONS AND DIMENSIONS

CAUTION

N

FINAL PLACEMENT OF THE FLUME MUST MAINTAIN THE MINIMUM POST SEPERATION DISTANCES SHOWN IN THE STANDARD DETAIL. SEE SDD "CONCRETE SURFACE DRAIN FLUME TYPE AT STRUCTURE" FOR DETAIL.

SHOULDER HINGE POINT

16+44.26, 20.41' LT

CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE TBTT MODIFIED

CONCRETE PAVEMENT 8-INCH (PAVED SHOULDER)

6' 7"

12'

$6\frac{1}{2}$" HMA PAVEMENT OVER
14" BASE AGGREGATE DENSE 3/4-INCH OVER
12" SELECT CRUSHED MATERIAL OVER GEOGRID

BRIDGE APPROACH (TYP.)
SEE SDD "CONCRETE BRIDGE APPROACH SLAB" FOR DETAILS.

STRUCTURAL APPROACH SLAB
SEE SDD "STRUCTURAL APPROACH SLAB AND CONCRETE APPROACH SLAB" FOR DETAILS.

16+00

STH 113

17+00

VILLAGE OF WAUNAKEE - SEWER

VILLAGE OF WAUNAKEE - WATER

CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE TBTT MODIFIED

16+44.26, 20.41' RT

SHOULDER HINGE POINT

FINAL PLACEMENT OF THE FLUME MUST MAINTAIN THE MINIMUM POST SEPERATION DISTANCES SHOWN IN THE STANDARD DETAIL. SEE SDD "CONCRETE SURFACE DRAIN FLUME TYPE AT STRUCTURE" FOR DETAIL.

MG&E

C/L

CAUTION

SEE STORM SEWER MODIFICATION CONSTRUCTION DETAIL FOR ADDITIONAL INFORMATION

SI

SEE EROSION CONTROL PLAN AND RIPRAP MEDIUM TREATMENT AT FLUMES FOR RIPRAP MEDIUM LOCATIONS AND DIMENSIONS

SALVAGE APRON ENDWALL FOR CULVERT PIPE 18-INCH AND SALVAGE 16' OF CULVERT PIPE 18-INCH
STA 17+09.7, 27.8' RT
INVERT ELEVATION = 911.496'

STRUCTURE DRAINAGE

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	CONSTRUCTION DETAILS - STRUCTURE DRAINAGE	SHEET 19	E

2

N

VILLAGE OF DANE

SI

ATC

SIXMILE CREEK

SHOULDER HINGE POINT

+28.39
-18.57' L
EL 919.11

CONCRETE CURB AND GUTTER 4-INCH
SLOPED 30-INCH TYPE TBTT MODIFIED

+72.41
-18.58' L
EL 919.24

CONCRETE PAVEMENT
8-INCH
(PAVED SHOULDER)

+87.41
-20.42' L
EL 919.27

+72.41
-12.00' L
EL 919.38

BRIDGE APPROACH (TYP.)
SEE SDD "CONCRETE
BRIDGE APPROACH SLAB"
FOR DETAILS.

+87.41
-12.00' L
EL 919.44

STRUCTURAL APPROACH SLAB
SEE SDD "STRUCTURAL
APPROACH SLAB AND
CONCRETE APPROACH SLAB"
FOR DETAILS.

17+00

STH 113

BRIDGE
B-13-0871

+72.41
0.00'
EL 919.62

+87.41
0.00'
EL 919.68

+72.58
-20.42' L
EL 919.71

CONCRETE PAVEMENT
8-INCH
(PAVED SHOULDER)

+87.58
-20.41' L
EL 919.81

STRUCTURAL APPROACH SLAB
SEE SDD "STRUCTURAL
APPROACH SLAB AND
CONCRETE APPROACH SLAB"
FOR DETAILS.

+72.58
-12.00' L
EL 919.88

+87.58
-12.00' L
EL 919.98

BRIDGE APPROACH (TYP.)
SEE SDD "CONCRETE
BRIDGE APPROACH SLAB"
FOR DETAILS.

18+00

+72.58
0.00' L
EL 920.12

+87.58
0.00' L
EL 920.22

CONCRETE CURB AND GUTTER 4-INCH
SLOPED 30-INCH TYPE TBTT MODIFIED

+72.41
12.00' R
EL 919.38

+87.41
12.00' R
EL 919.44

CONCRETE PAVEMENT
8-INCH
(PAVED SHOULDER)

+28.39
18.57' R
EL 919.11

+72.41
18.58' R
EL 919.24

+87.41
20.42' R
EL 919.27

+72.58
12.00' R
EL 919.88

+87.58
12.00' R
EL 919.98

CONCRETE PAVEMENT
8-INCH
(PAVED SHOULDER)

+72.58
20.42' R
EL 919.71

+87.58
20.42' R
EL 919.81

SHOULDER HINGE POINT

SHOULDER HINGE POINT

MG&E

SI

SI

VILLAGE OF WAUNAKEE - WATER

VILLAGE OF WAUNAKEE - SEWER

VILLAGE OF WAUNAKEE - ELECTRIC

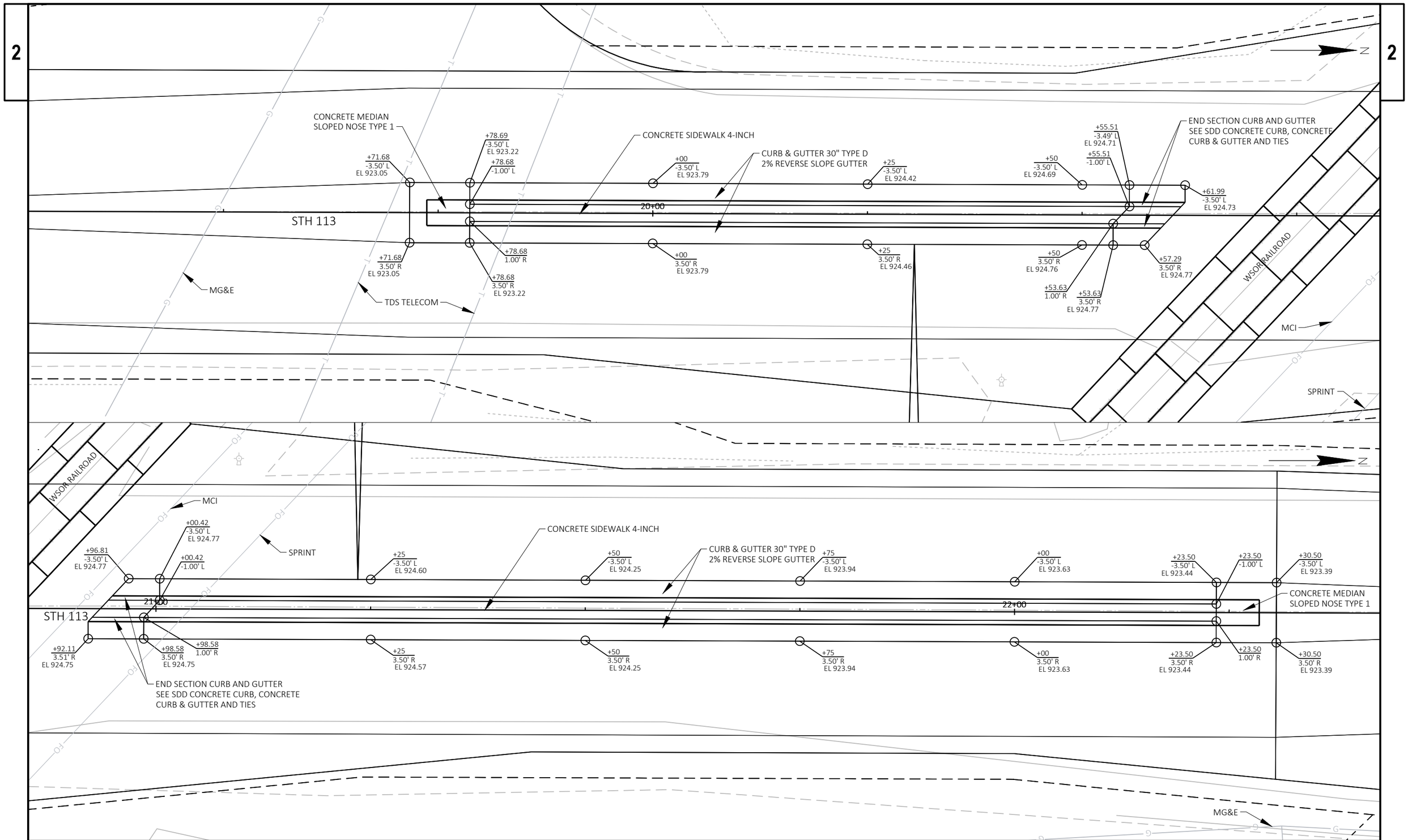
SIXMILE CREEK

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	CONSTRUCTION DETAILS - APPROACH DETAILS	SHEET 20	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\020401-CD.DWG
LAYOUT NAME - 08

PLOT DATE : 10/21/2022 7:03 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : Custom

WISDOT/CADDS SHEET 42



PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	CONSTRUCTION DETAILS - MEDIAN DETAILS	SHEET 21	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\020401-CD.DWG
LAYOUT NAME - 09

PLOT DATE : 10/21/2022 7:03 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 42

2

N

SIXMILE CREEK

TLE

PROPOSED R/W

VILLAGE OF DANE

EXISTING R/W

TLE

SI

ATC

CAUTION

MGS EAT

MGS THRIE BEAM TRANSITION

MGS THRIE BEAM TRANSITION

MGS EAT

15+97.40
-20.40' LT
POST #1

16+22.40
-20.40' LT
POST #5

16+47.40
-20.40' LT
POST #9

REMOVING GUARDRAIL

18+12.60
-20.40' LT
POST #9

18+37.60
-20.40' LT
POST #5

18+62.60
-20.40' LT
POST #1

REMOVING GUARDRAIL

VILLAGE OF WAUNAKEE - SEWER

16+00

STH 113

17+00

B-13-871

18+00

REMOVING GUARDRAIL

16+47.41
20.40' RT
POST #9

15+97.40
20.40' RT
POST #1

16+22.40
20.40' RT
POST #5

REMOVING GUARDRAIL

18+12.60
20.40' RT
POST #9

18+37.60
20.40' RT
POST #5

18+62.60
20.40' RT
POST #1

MGS EAT

MGS THRIE BEAM TRANSITION

MGS THRIE BEAM TRANSITION

MGS EAT

MG&E

SI

SI

SHARED USE PATH

PEDESTRIAN BRIDGE
OVER SIXMILE CREEK

SHARED USE PATH

EXISTING R/W

VILLAGE OF WAUNAKEE - WATER

SALVAGE APRON ENDWALL FOR CULVERT PIPE 18-INCH
AND SALVAGE 16' OF CULVERT PIPE 18-INCH

VILLAGE OF WAUNAKEE - ELECTRIC

SIXMILE CREEK

NOTE: SEE SDD 14B44 MIDWEST GUARDRAIL SYSTEM (MGS)
ENERGY ABSORBING TERMINAL FOR INSTALLATION INSTRUCTIONS.

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN DETAILS: BEAM GUARD	SHEET 22	E

2

2

WETLANDS

N

WSOR RAILROAD

WORK BY RAILROAD
GATE AND MAST MOUNTED LIGHTS

SHALLOW DITCH
GRADE TO DRAIN

21+56.06
-62.31' LT
923.212'

FILL AND BUNGELOW APPROXIMATE SIZE. EXACT SIZE TO BE DETERMINED BY RAILROAD. BUNGELOW PAD TO BE CONSTRUCTED WITH 1 1/4-INCH BASE AGGREGATE DENSE.

WETLANDS

PROPOSED R/W

EXISTING R/W

SI

ATC

CAUTION

EXISTING RR SIGNAL

WORK BY RAILROAD
5' WIDTH X 65' LONG, 8 PANEL

20+93.30
-24.88' LT

WORK BY RAILROAD
2.5' WIDTH X 65' LONG, 8 PANEL
TWO TIE STAGER

21+68.45
-50.66' LT
922.532'

EXISTING R/W

PROPOSED R/W

TLE

VILLAGE OF DANE

PROPOSED R/W

TLE

EXISTING R/W

SHOULDER BREAK
POINT EXTENDED

20+61.04
-19.50' LT

6:1 TAPER

21+28.36
-32.84' LT
923.833'

21+46.02
-26.81' LT
923.650'

21+67.40
-19.51' LT

3:1 TAPER

21+23.50
-27.75' LT

10:1 TAPER

20+49.11
-16.50' LT

21+04.01
-21.56' LT

21+54.44
-16.50' LT

22+30.52
-16.49' LT

TDS TELECOM

CURB & GUTTER 30" TYPE D
REVERSE SLOPE GUTTER

CONCRETE MEDIAN SLOPED NOSE TYPE 1

19+78.69
-3.50' LT

WORK BY RAILROAD
2.5' WIDTH X 65' LONG, 8 PANEL
ONE TIE STAGER

STH 113

20+00

21+00

22+00

END SECTION CURB AND GUTTER
SEE SDD CONCRETE CURB, CONCRETE
CURB & GUTTER AND TIES

CURB & GUTTER 30" TYPE D
REVERSE SLOPE GUTTER

22+23.49
3.39' RT

CONCRETE MEDIAN SLOPED NOSE TYPE 1

22+00.00
16.50' RT

WORK BY RAILROAD
GATE AND MAST MOUNTED LIGHTS

MG&E

MCI

RAIL

20+48.86
22.59' RT

20+30.48
27.75' RT

19+87.33
16.50' RT

10:1 TAPER

19+74.14
19.51' RT

3:1 TAPER

SI

20+27.12
32.75' RT

RECONSTRUCTING CURB RAMPS
SEE CURB RAMP DETAIL SHEETS

SHOULDER BREAK
POINT EXTENDED

SPRINT

EXISTING RR SIGNAL

20+59.58
25.91' RT

CAUTION

SI

21+23.76
19.52' RT

21+43.76
16.50' RT

MG&E

CENTERLINE OF TRACKS

EXISTING RR BUNGALOW

VILLAGE OF WAUNAKEE

SHARED USE PATH

20+15.95
57.68' RT

20+23.24
64.52' RT

TDS TELECOM

19+94.81
69.57' RT

20+33.35
65.07' RT

WORK BY RAILROAD
POST MOUNTED
RAILROAD SIGNAL

WORK BY RAILROAD
POST MOUNTED
RAILROAD SIGNAL

SHARED USE PATH

VILLAGE OF WAUNAKEE

TDS TELECOM
SPECTRUM

EXISTING R/W

EXISTING R/W

20+04.81
69.50' RT

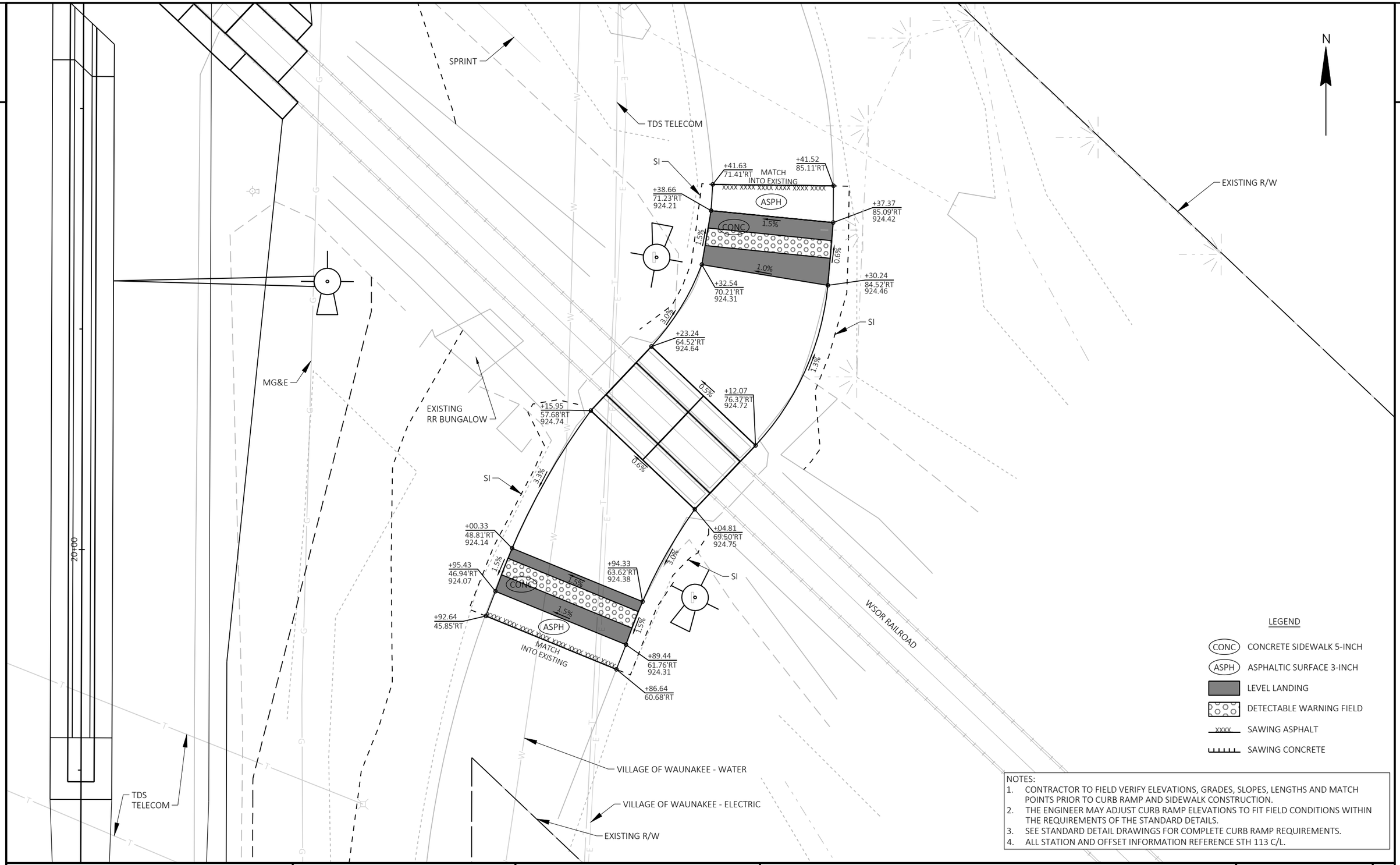
EXISTING R/W

WORK BY RAILROAD
5' WIDTH X 16.25' LONG, 2 PANEL

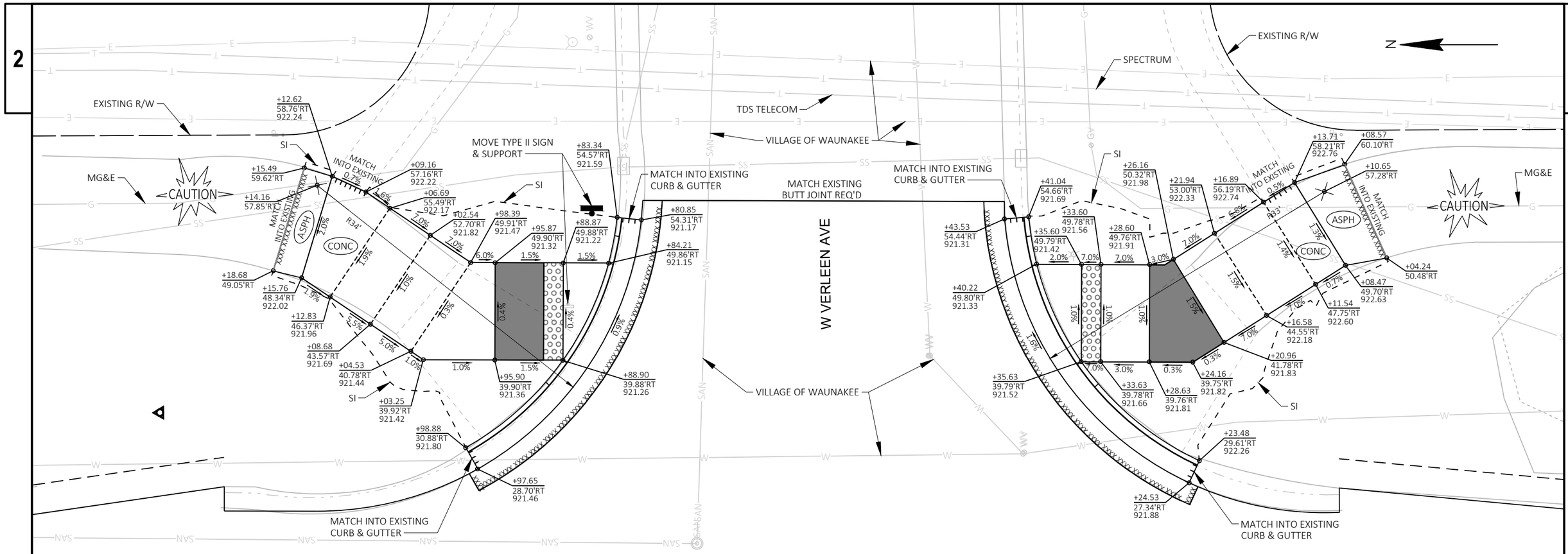
20+12.07
76.37' RT

WETLANDS

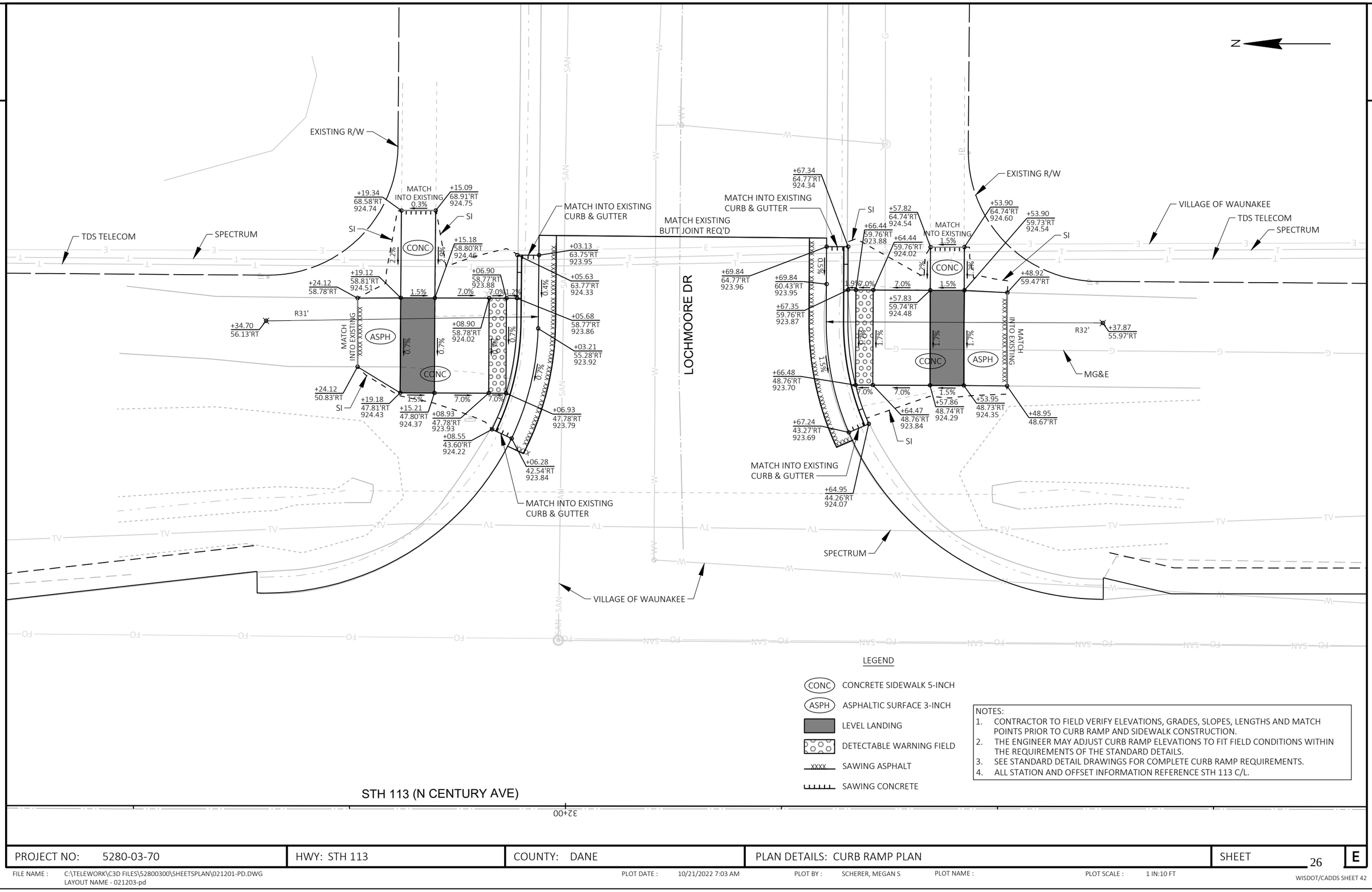
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN DETAILS: RAILROAD CROSSING	SHEET 23	E



PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN DETAILS: CURB RAMP PLANS	SHEET 24	E

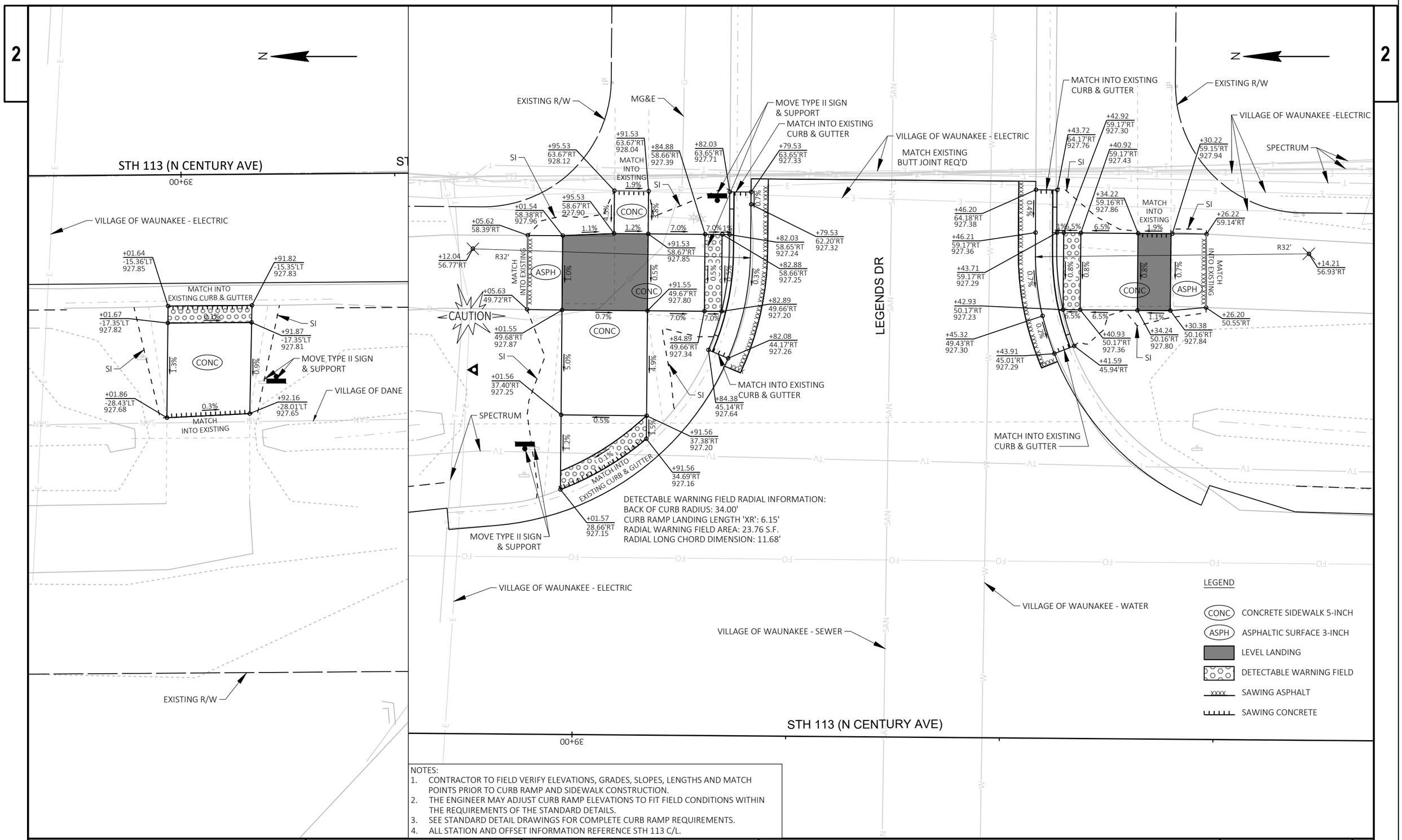


PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN DETAILS: CURB RAMP PLAN	SHEET 25	E



PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN DETAILS: CURB RAMP PLAN	SHEET 26	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\021201-PD.DWG
LAYOUT NAME - 021203-pd
PLOT DATE : 10/21/2022 7:03 AM
PLOT BY : SCHERER, MEGAN S
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42



NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
2. THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.
3. SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 113 C/L.

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN DETAILS: CURB RAMP PLAN	SHEET 27	E

STH 113 (N CENTURY AVE)

EP: 1+81.36

44+00

43+00

SPECTRUM

MG&E

VILLAGE OF DANE

TDS TELECOM

+14.08
-16.00'LT
930.13

+14.08
-18.50'LT
930.06

CONCRETE SURFACE DRAIN
SEE SDD FOR DETAILS

NOTE:
DITCHING REQUIRED TO DRAIN FROM FLUME TO EXISTING DITCH TO THE NORTH. MAINTAIN A MINIMUM OF 0.3% SLOPE IN THE DITCH.

+86.15
-22.89'LT

+81.03
-25.92'LT
929.94
LOW POINT

+74.90
-30.56'LT

+70.45
-38.55'LT
929.94

+67.71
-37.92'LT
930.02

+84.68
-38.54'LT
930.15

+75.00
-38.54'LT
930.01

DETECTABLE WARNING FIELD RADIAL INFORMATION:
BACK OF CURB RADIUS: 57.50'
CURB RAMP LANDING LENGTH 'XR': 15.56'
RADIAL WARNING FIELD AREA: 16.65 S.F.
RADIAL LONG CHORD DIMENSION: 9.86'

SIGN AND POST TO BE REMOVED BY LOCALS

+64.69
-46.55'LT
930.07

SI

+81.25
-46.54'LT
930.22

CONC

+95.23
-49.64'LT
930.82

MATCH INTO EXISTING

+89.43
-55.15'LT
930.87

CAUTION

+74.99
-46.54'LT
930.12

EXISTING R/W

MG&E

SI

+54.08
-75.85'LT
930.59

+54.06
-82.61'LT
930.70

KOPP RD

1+00

MATCH EXISTING SAW CUT REQ'D

MATCH INTO EXISTING
CURB & GUTTER

+94.09
-18.52'LT

+99.66
-21.09'LT
929.79
LOW POINT

+80.13
-16.00'LT
930.02

+80.12
-18.50'LT
929.95

+05.72
-25.26'LT

CONCRETE SURFACE DRAIN
SEE SDD FOR DETAILS

SI

+13.33
-38.58'LT
929.91

+11.33
-38.58'LT
929.94

+03.62
-38.57'LT
930.05

+78.30
-39.02'LT
929.58

MOVE TYPE II SIGN

+16.42
-46.57'LT
929.98

+98.07
-44.17'LT
930.29

ASPH

MATCH INTO EXISTING

+19.43
-48.62'LT
930.45

+13.33
-46.58'LT
930.01

+06.91
-46.57'LT
930.11

CONC

+04.51
-48.94'LT
930.35

+78.09
-46.60'LT
929.73

+94.50
-48.99'LT
930.38

+11.33
-46.57'LT
930.04

+00.93
-53.76'LT
930.44

SI

+20.12
-56.10'LT
930.08

CONC

CAUTION

EXISTING R/W

+13.64
-69.23'LT
931.03

+08.62
-69.61'LT
931.10

CONC

+13.70
-74.22'LT
931.10

MATCH INTO EXISTING

+09.22
-74.23'LT
931.25

+20.06
-82.52'LT
930.79

SI

MATCH INTO EXISTING
CURB & GUTTER

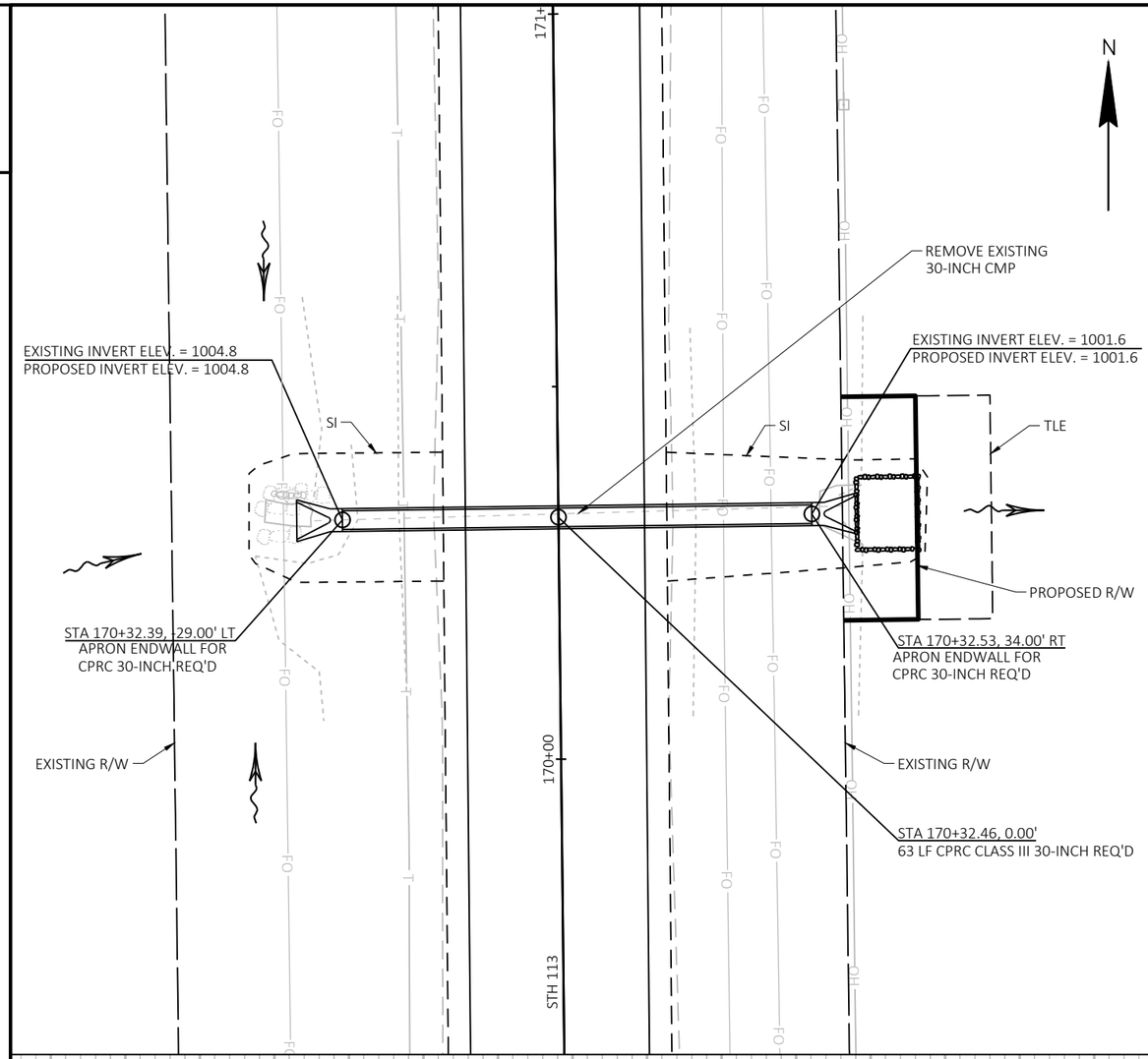
N

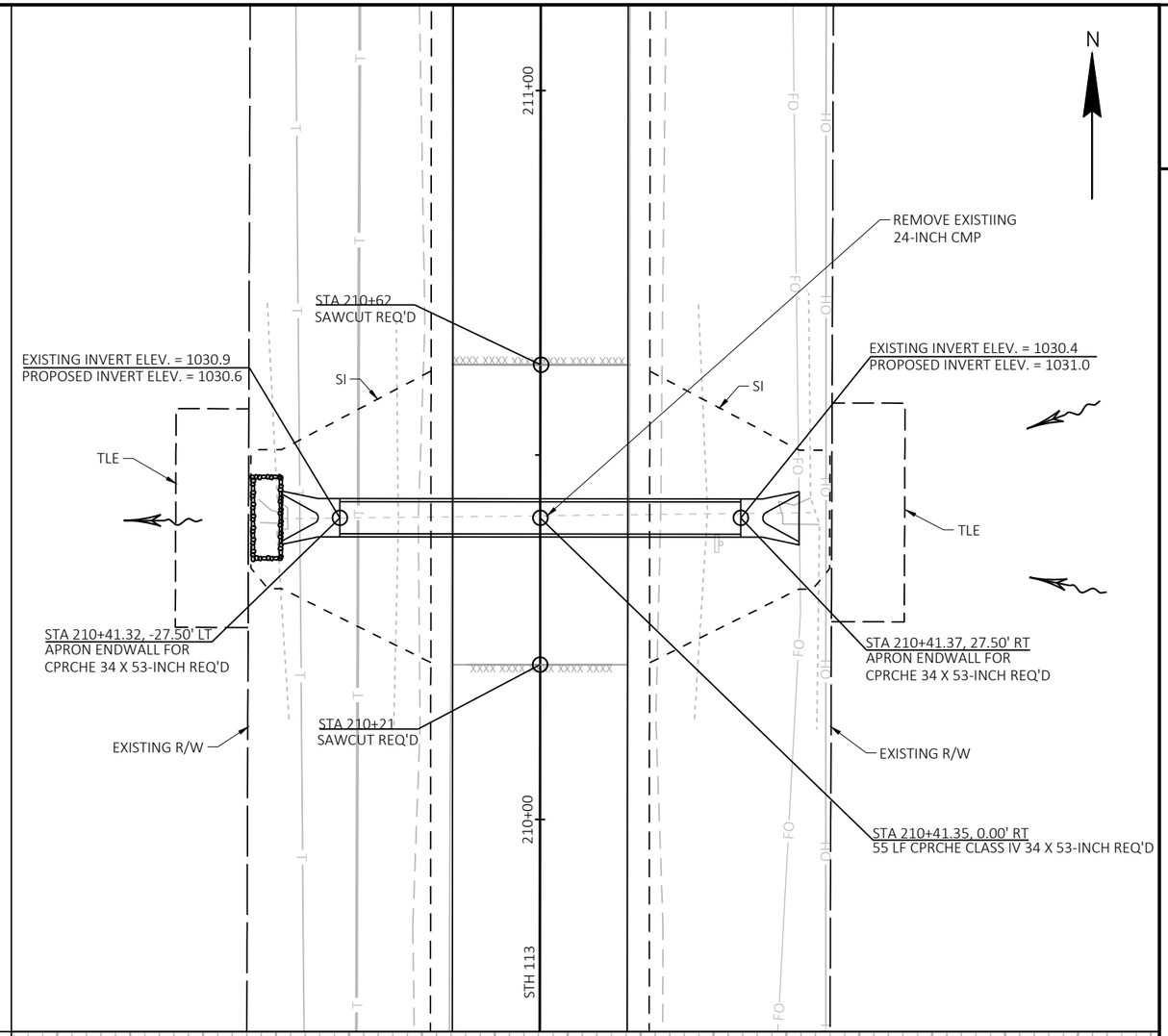
LEGEND

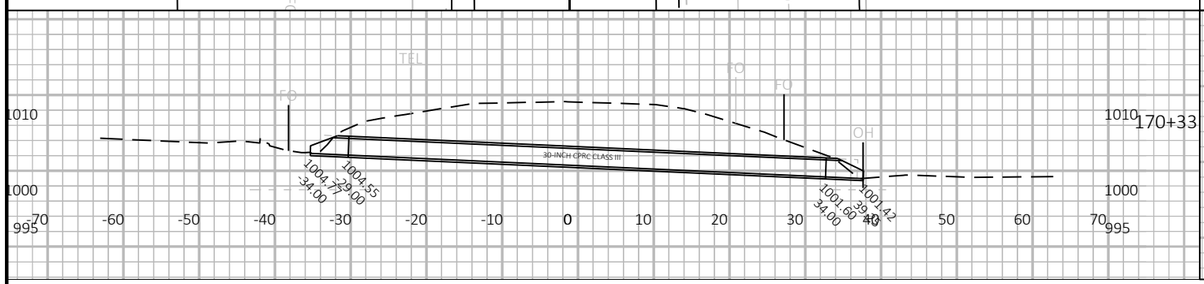
- CONC — CONCRETE SIDEWALK 5-INCH
- ASPH — ASPHALTIC SURFACE 3-INCH
- LEVEL LANDING
- DETECTABLE WARNING FIELD
- xxxx — SAWING ASPHALT
- SAWING CONCRETE

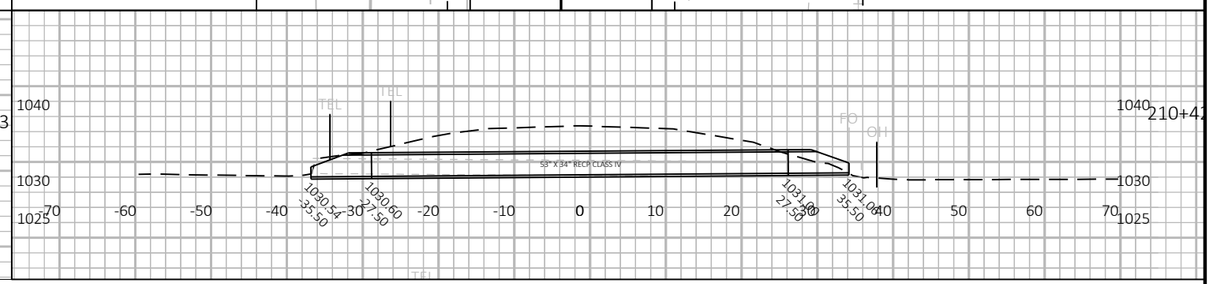
NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
2. THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.
3. SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
4. ALL STATION AND OFFSET INFORMATION REFERENCE STH 113 C/L.

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN DETAILS: CURB RAMP PLAN	SHEET 28	E









PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN DETAILS: CULVERTS	SHEET 29	E

2

SEE CONSTRUCTION DETAIL FOR BALE & SILT FENCE RELIEF

RIP RAP MEDIUM 6.0' X 6.0' X 1.5'

TLE

EXISTING R/W

SIXMILE CREEK

SEE CONSTRUCTION DETAIL FOR BALE & SILT FENCE RELIEF

TLE

PROPOSED R/W

EXISTING R/W

N

PI 15+00.00

15+00

16+00

17+00

18+00

19+00

20+00

STH 113

SEE RIPRAP TREATMENT AT FLUMES CONSTRUCTION DETAIL FOR RIPRAP AND GEOTEXTILE FABRIC HR DETAIL

RIP RAP MEDIUM 6.0' X 6.0' X 1.5'

SIXMILE CREEK

TURBIDITY BARRIER

EXISTING R/W

EXISTING R/W

LEGEND

- EROSION MAT URBAN CLASS I TYPE B
- SEED MIXTURE NO. 70A
- EROSION BALES
- SILT FENCE
- INLET PROTECTION TYPE A
- INLET PROTECTION TYPE C
- CULVERT PIPE CHECK
- SURFACE WATER FLOW

NOTES

1. DISTURBED SLOPES SHALL BE RESTORED WITH SALVAGED TOPSOIL NO. 30, FERTILIZER TYPE B, AND EROSION MAT URBAN CLASS I TYPE B.
2. SEE SDD "SILT FENCE" FOR INSTALLATION INSTRUCTIONS.

NOTES

1. DISTURBED SLOPES SHALL BE RESTORED WITH SALVAGED TOPSOIL NO. 30, FERTILIZER TYPE B, AND EROSION MAT URBAN CLASS I TYPE B.
2. SEE SDD "SILT FENCE" FOR INSTALLATION INSTRUCTIONS.

N

PROPOSED R/W

TLE

23+00.00

EXISTING R/W

21+00

22+00

23+00

24+00

25+00

26+00

STH 113

EXISTING R/W

W VERLEEN AVE

EXISTING R/W

2

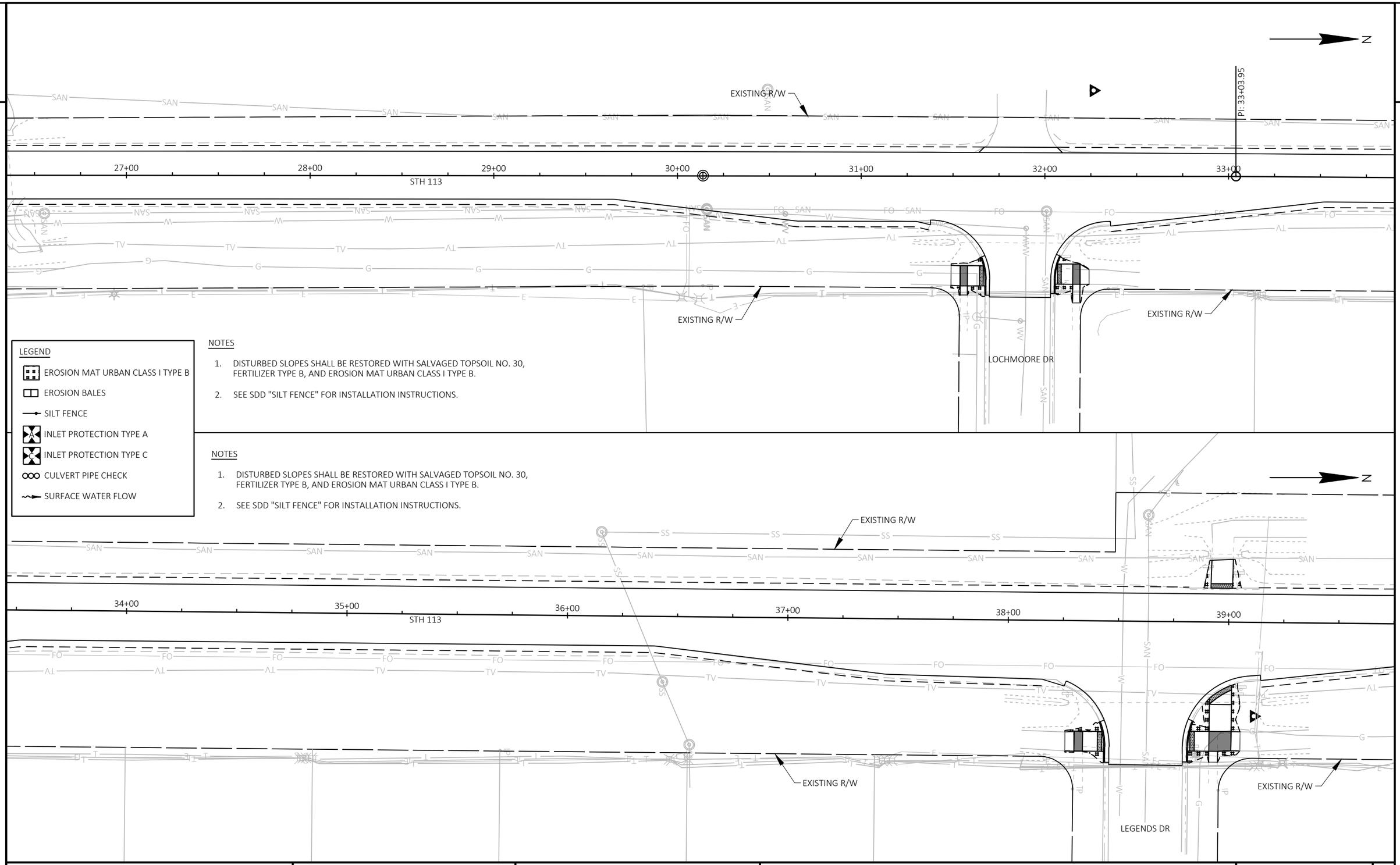
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	EROSION CONTROL	SHEET 30	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\022001-EC.DWG
LAYOUT NAME - 1

PLOT DATE : 10/31/2022 10:01 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 41

2



LEGEND

- EROSION MAT URBAN CLASS I TYPE B
- EROSION BALES
- SILT FENCE
- INLET PROTECTION TYPE A
- INLET PROTECTION TYPE C
- CULVERT PIPE CHECK
- SURFACE WATER FLOW

NOTES

1. DISTURBED SLOPES SHALL BE RESTORED WITH SALVAGED TOPSOIL NO. 30, FERTILIZER TYPE B, AND EROSION MAT URBAN CLASS I TYPE B.
2. SEE SDD "SILT FENCE" FOR INSTALLATION INSTRUCTIONS.

NOTES

1. DISTURBED SLOPES SHALL BE RESTORED WITH SALVAGED TOPSOIL NO. 30, FERTILIZER TYPE B, AND EROSION MAT URBAN CLASS I TYPE B.
2. SEE SDD "SILT FENCE" FOR INSTALLATION INSTRUCTIONS.

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	EROSION CONTROL	SHEET 31	E

2

KOPP RD

EXISTING R/W

SEE RIPRAP TREATMENT AT FLUMES CONSTRUCTION DETAIL FOR RIPRAP AND GEOTEXTILE FABRIC HR DETAIL

EXISTING R/W

RIP RAP MEDIUM 6.0' X 6.0' X 1.5'

RIP RAP MEDIUM 6.0' X 6.0' X 1.0'

PI: 44+70.30

41+00 42+00 43+00 44+00 45+00 46+00

STH 113

EXISTING R/W

LEGEND

- EROSION MAT URBAN CLASS I TYPE B
- EROSION BALES
- SILT FENCE
- INLET PROTECTION TYPE A
- INLET PROTECTION TYPE C
- CULVERT PIPE CHECK
- SURFACE WATER FLOW

NOTES

1. DISTURBED SLOPES SHALL BE RESTORED WITH SALVAGED TOPSOIL NO. 30, FERTILIZER TYPE B, AND EROSION MAT URBAN CLASS I TYPE B.
2. SEE SDD "SILT FENCE" FOR INSTALLATION INSTRUCTIONS.

CUBA VALLEY RD

RIP RAP MEDIUM 6.0' X 6.0' X 1.5'

PI: 109+33.08

107+00 108+00 109+00 110+00 111+00 112+00

STH 113

SEE RIPRAP TREATMENT AT FLUMES CONSTRUCTION DETAIL FOR RIPRAP AND GEOTEXTILE FABRIC HR DETAIL

CUBA VALLEY RD

NOTES

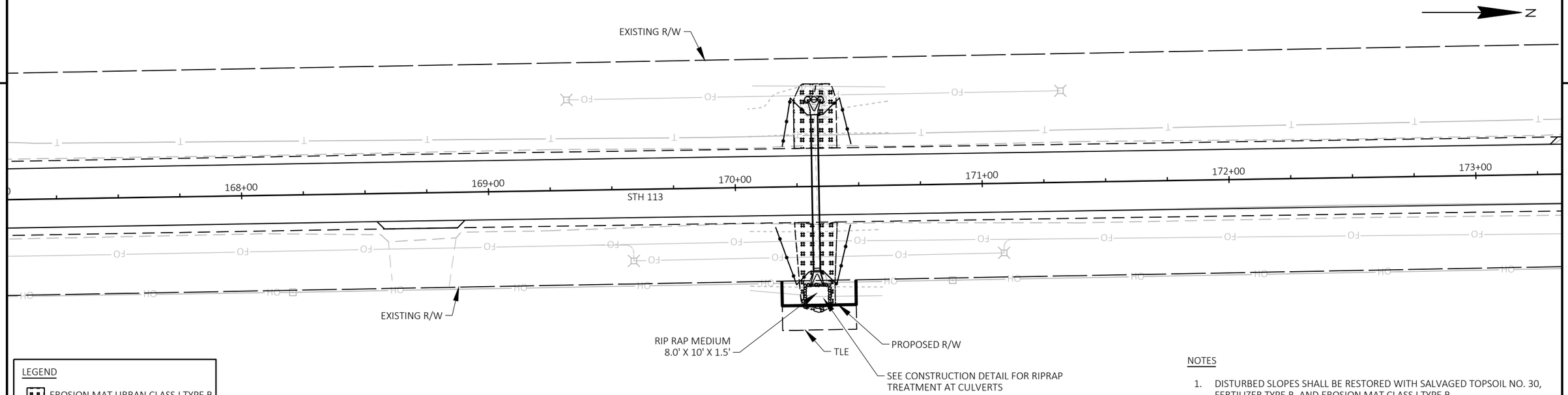
1. DISTURBED SLOPES SHALL BE RESTORED WITH SALVAGED TOPSOIL NO. 30, FERTILIZER TYPE B, AND EROSION MAT URBAN CLASS I TYPE B.
2. SEE SDD "SILT FENCE" FOR INSTALLATION INSTRUCTIONS.

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	EROSION CONTROL	SHEET 32	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\022001-EC.DWG
LAYOUT NAME - 3

PLOT DATE : 10/21/2022 7:05 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 41

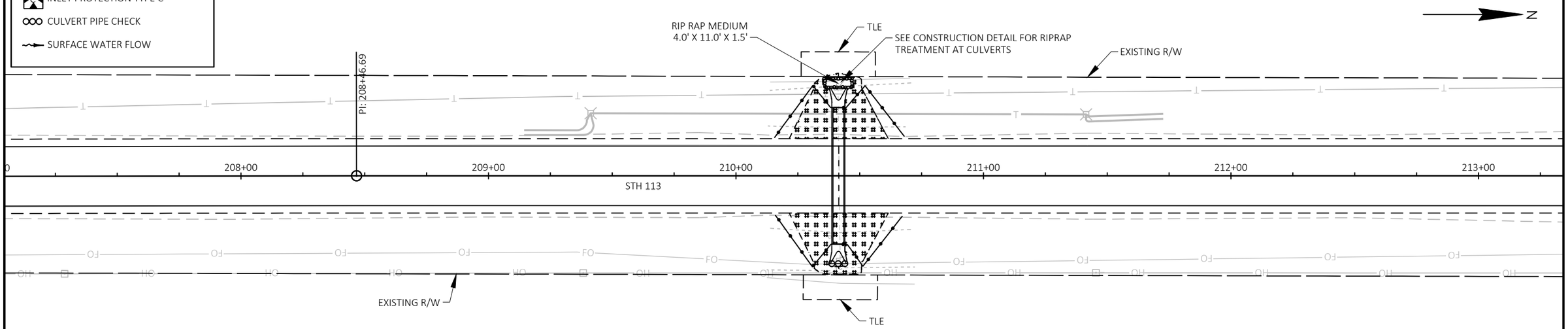


LEGEND

- EROSION MAT URBAN CLASS I TYPE B
- EROSION BALES
- SILT FENCE
- INLET PROTECTION TYPE A
- INLET PROTECTION TYPE C
- CULVERT PIPE CHECK
- SURFACE WATER FLOW

NOTES

1. DISTURBED SLOPES SHALL BE RESTORED WITH SALVAGED TOPSOIL NO. 30, FERTILIZER TYPE B, AND EROSION MAT CLASS I TYPE B.
2. SEE SDD "SILT FENCE" FOR INSTALLATION INSTRUCTIONS.



NOTES

1. DISTURBED SLOPES SHALL BE RESTORED WITH SALVAGED TOPSOIL, FERTILIZER TYPE B, AND EROSION MAT CLASS I TYPE B.
2. SEE SDD "SILT FENCE" FOR INSTALLATION INSTRUCTIONS.

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	EROSION CONTROL	SHEET 33	E

2

MARKING ISLAND NOSE EPOXY, YELLOW

REMOVE TYPE II SIGN AND SUPPORT

MARKING LINE EPOXY 6-INCH, DOUBLE YELLOW CENTERLINE

MARKING ISLAND NOSE EPOXY, YELLOW

BEGIN PROJECT
STA 15+00

DO NOT ENTER
R5-1
30"X30"

NARROW BRIDGE
REMOVE

SIGN

SIGN

SIGN

PI: 15+00.00

PI: 19+00.00

15 16 17 18 19 20 21

11' 11' 11' 11'

STH 113

REMOVE TYPE II SIGN AND SUPPORT

MARKING LINE EPOXY 6-INCH, WHITE EDGELINE

SEE STATION
12+75 RT

NARROW BRIDGE
REMOVE

SIXMILE CREEK

MOVE TYPE II SIGN AND INSTALL
ON NEW SIGN SUPPORT

R4-7
24"X30"

MARKING RAILROAD CROSSING EPOXY

MOVE TYPE II SIGN AND INSTALL
ON NEW SIGN SUPPORT

MARKING RAILROAD CROSSING EPOXY

R4-7
24"X30"

MARKING LINE EPOXY 6-INCH, YELLOW CENTERLINE, LEFT

MARKING LINE EPOXY 6-INCH, YELLOW CENTERLINE (12.5' LINE, 37.5' GAP), RIGHT

SIGN

SIGN

MARKING LINE EPOXY 6-INCH, DOUBLE YELLOW CENTERLINE

MARKING ISLAND NOSE EPOXY, YELLOW

22 23 24 25 26 27

11' 11' 11' 11'

STH 113

+00.80
14.50' RT

+09.09
24.49' RT

+62.19
54.58' RT

+00
0.00'

+15.20
11.00' RT

MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH

MARKING LINE EPOXY 6-INCH, WHITE EDGELINE

STREET NAME SIGNS TO
BE REMOVED BY LOCALS

DO NOT ENTER
R5-1
30"X30"

MARKING LINE, EPOXY, 6-INCH, WHITE EDGELINE

MARKING STOP LINE EPOXY 18-INCH

STOP

MOVE TYPE II SIGN AND INSTALL
ON NEW SIGN SUPPORT

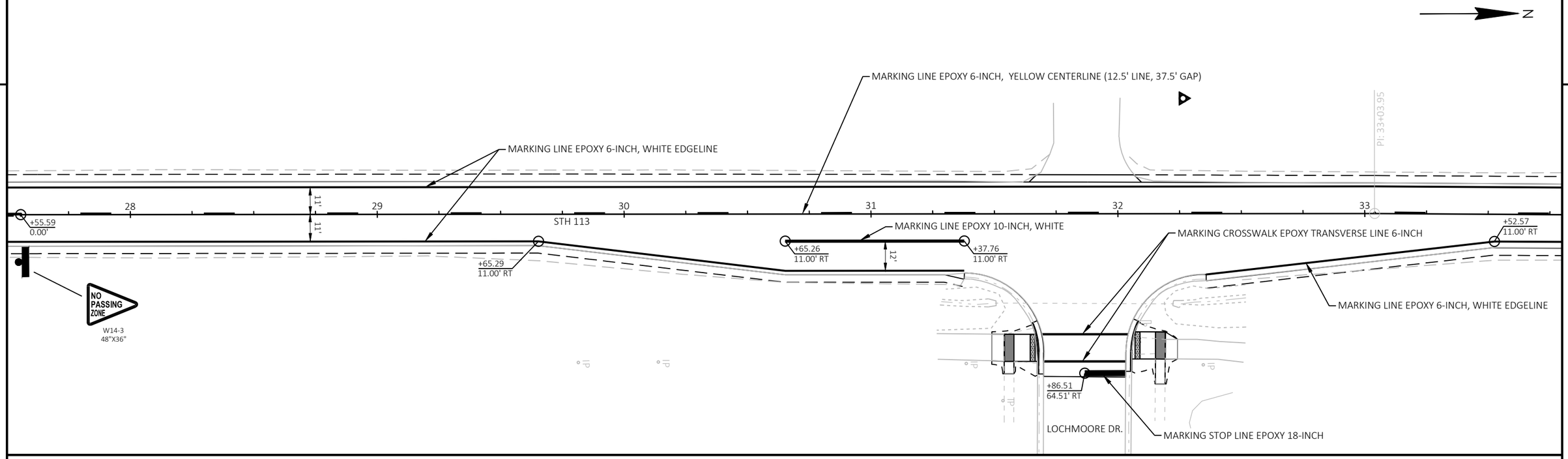
W VERLEEN AVE

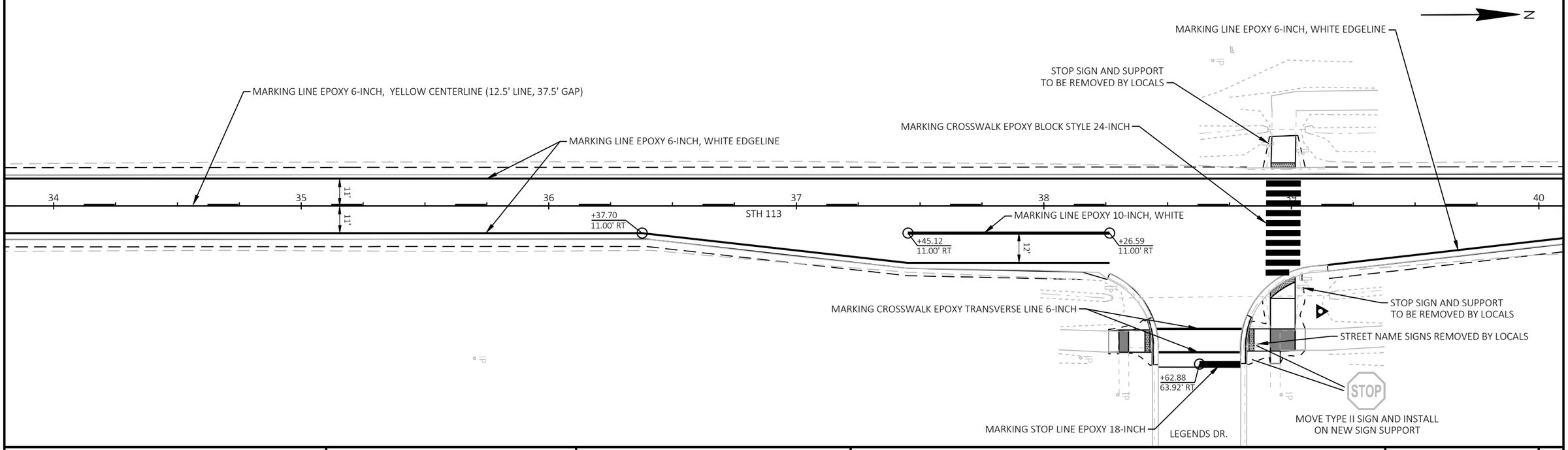
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	SIGNING AND PAVEMENT MARKING	SHEET 34	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\024501-PM.DWG
LAYOUT NAME - 01

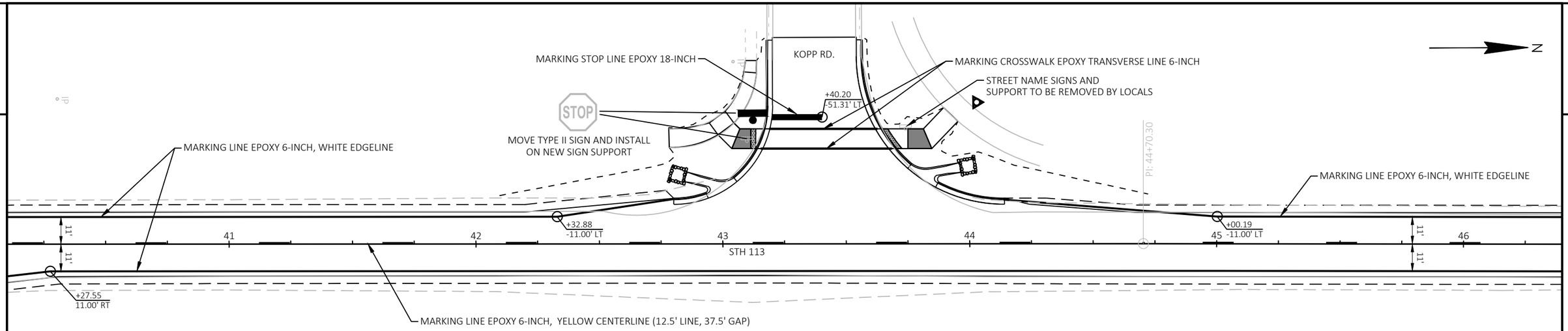
PLOT DATE : 5/19/2023 1:35 PM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : 1 IN:40 FT

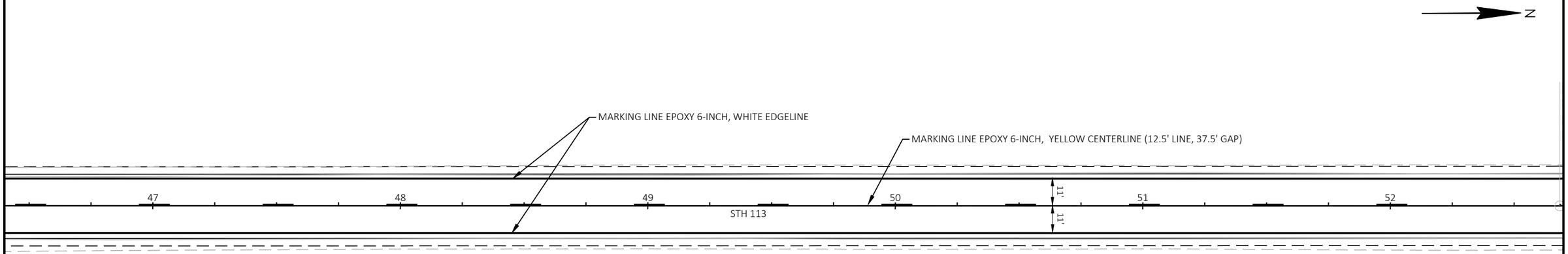
2





PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	SIGNING AND PAVEMENT MARKING	SHEET 35	E



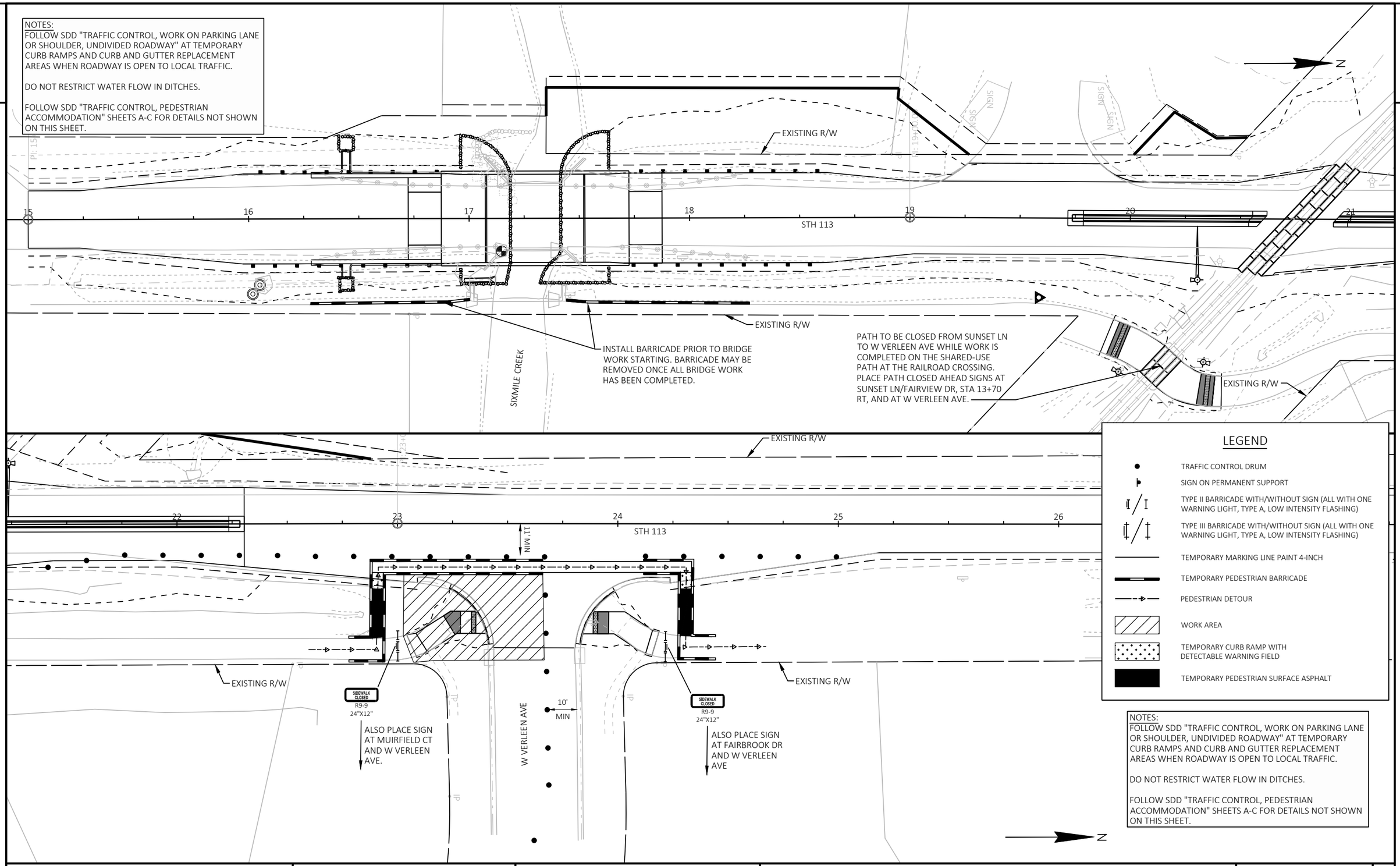


PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	SIGNING AND PAVEMENT MARKING	SHEET 36	E

NOTES:
FOLLOW SDD "TRAFFIC CONTROL, WORK ON PARKING LANE OR SHOULDER, UNDIVIDED ROADWAY" AT TEMPORARY CURB RAMPS AND CURB AND GUTTER REPLACEMENT AREAS WHEN ROADWAY IS OPEN TO LOCAL TRAFFIC.

DO NOT RESTRICT WATER FLOW IN DITCHES.

FOLLOW SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" SHEETS A-C FOR DETAILS NOT SHOWN ON THIS SHEET.



LEGEND

- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)
- TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)
- TEMPORARY MARKING LINE PAINT 4-INCH
- TEMPORARY PEDESTRIAN BARRICADE
- PEDESTRIAN DETOUR
- WORK AREA
- TEMPORARY CURB RAMP WITH DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN SURFACE ASPHALT

NOTES:
FOLLOW SDD "TRAFFIC CONTROL, WORK ON PARKING LANE OR SHOULDER, UNDIVIDED ROADWAY" AT TEMPORARY CURB RAMPS AND CURB AND GUTTER REPLACEMENT AREAS WHEN ROADWAY IS OPEN TO LOCAL TRAFFIC.

DO NOT RESTRICT WATER FLOW IN DITCHES.

FOLLOW SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" SHEETS A-C FOR DETAILS NOT SHOWN ON THIS SHEET.

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	TEMPORARY PEDESTRIAN ACCESS - PHASE 1	SHEET 37	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\021301-PE.DWG
LAYOUT NAME - 01
PLOT DATE : 10/21/2022 7:06 AM
PLOT BY : SCHERER, MEGAN S
PLOT NAME :
PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 44

2

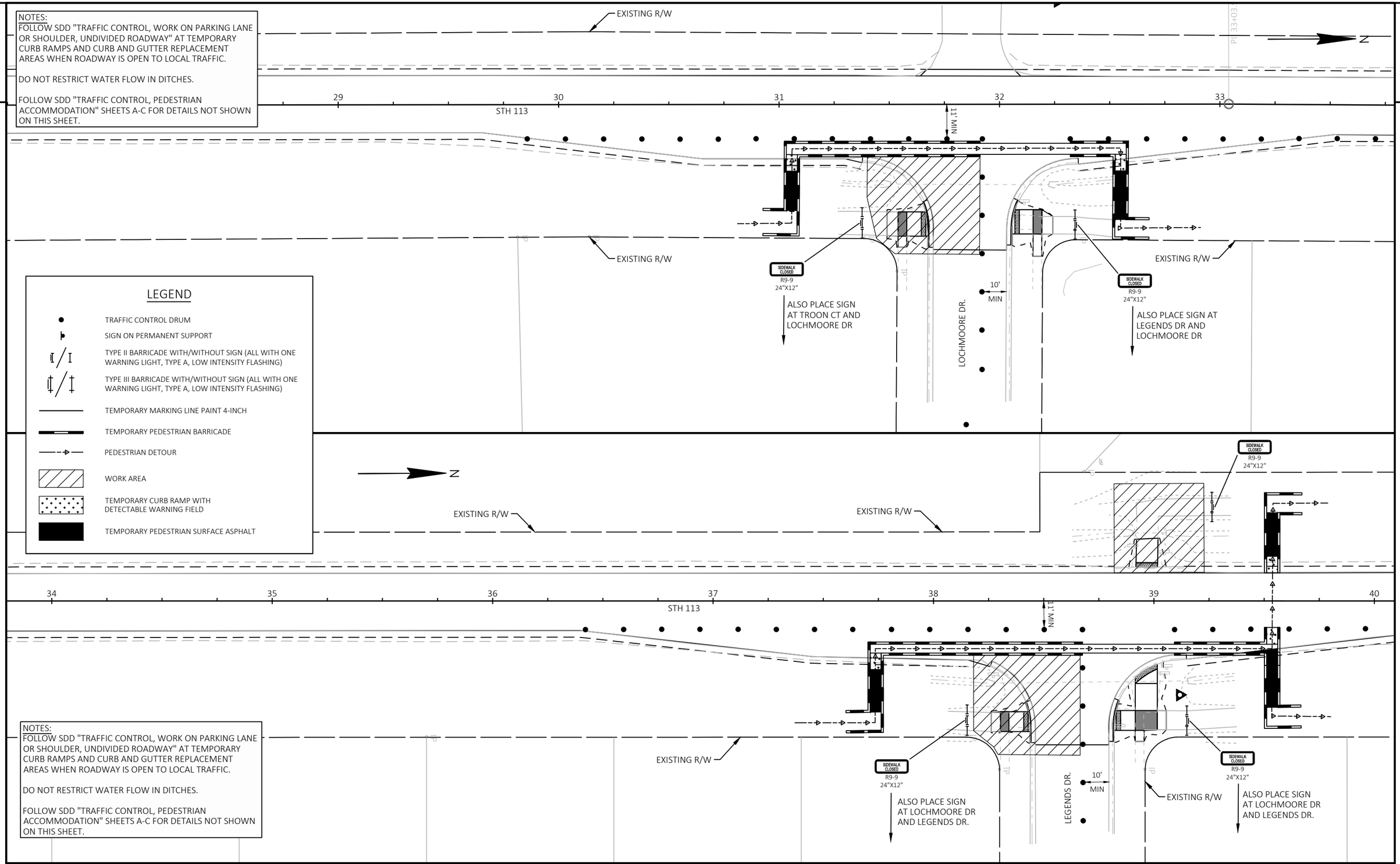
NOTES:
FOLLOW SDD "TRAFFIC CONTROL, WORK ON PARKING LANE OR SHOULDER, UNDIVIDED ROADWAY" AT TEMPORARY CURB RAMPS AND CURB AND GUTTER REPLACEMENT AREAS WHEN ROADWAY IS OPEN TO LOCAL TRAFFIC.

DO NOT RESTRICT WATER FLOW IN DITCHES.

FOLLOW SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" SHEETS A-C FOR DETAILS NOT SHOWN ON THIS SHEET.

LEGEND

- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)
- TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)
- TEMPORARY MARKING LINE PAINT 4-INCH
- TEMPORARY PEDESTRIAN BARRICADE
- PEDESTRIAN DETOUR
- WORK AREA
- TEMPORARY CURB RAMP WITH DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN SURFACE ASPHALT



2

NOTES:
FOLLOW SDD "TRAFFIC CONTROL, WORK ON PARKING LANE OR SHOULDER, UNDIVIDED ROADWAY" AT TEMPORARY CURB RAMPS AND CURB AND GUTTER REPLACEMENT AREAS WHEN ROADWAY IS OPEN TO LOCAL TRAFFIC.

DO NOT RESTRICT WATER FLOW IN DITCHES.

FOLLOW SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" SHEETS A-C FOR DETAILS NOT SHOWN ON THIS SHEET.

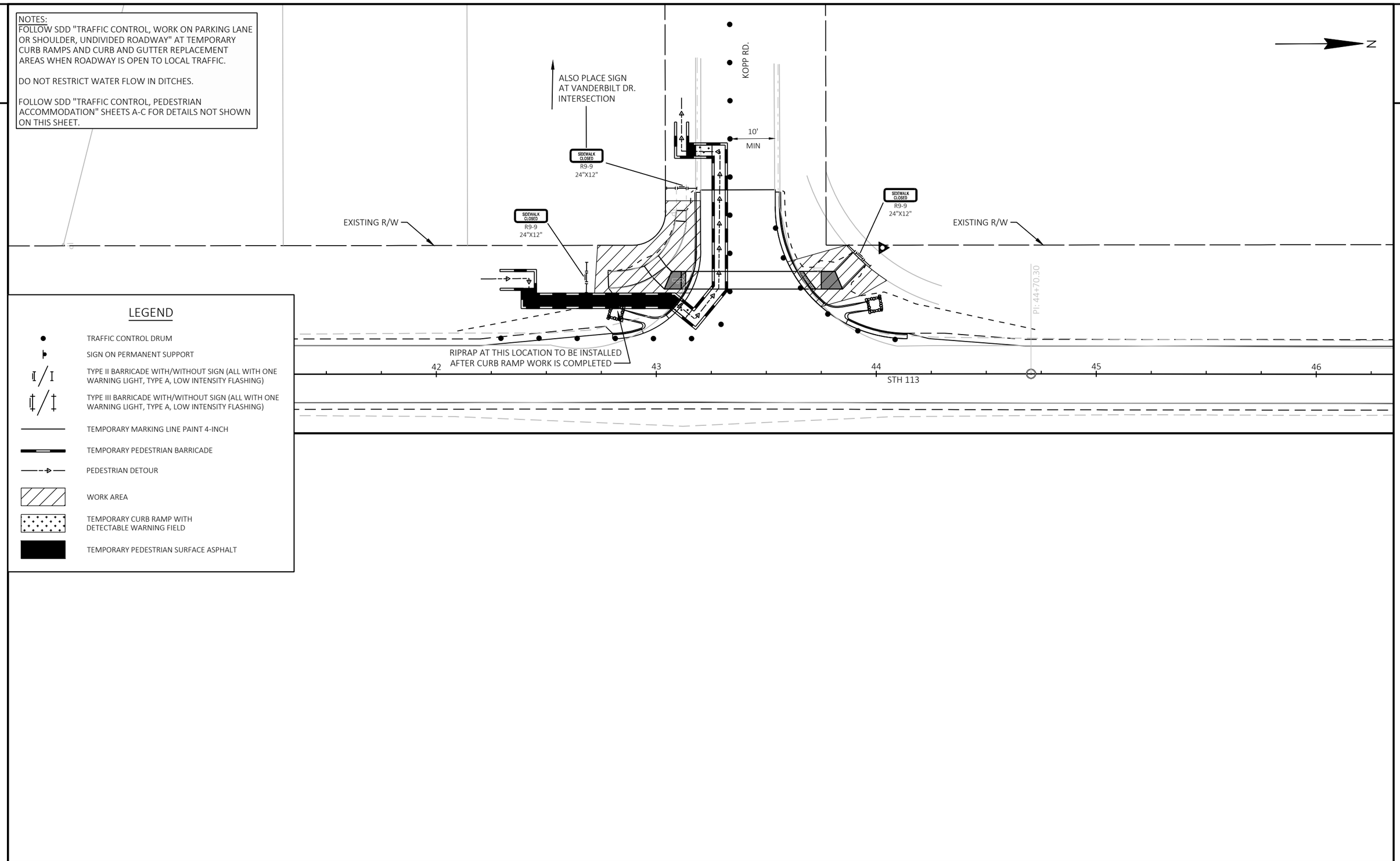
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	TEMPORARY PEDESTRIAN ACCESS - PHASE 1	SHEET 38 E

2

NOTES:
FOLLOW SDD "TRAFFIC CONTROL, WORK ON PARKING LANE OR SHOULDER, UNDIVIDED ROADWAY" AT TEMPORARY CURB RAMPS AND CURB AND GUTTER REPLACEMENT AREAS WHEN ROADWAY IS OPEN TO LOCAL TRAFFIC.

DO NOT RESTRICT WATER FLOW IN DITCHES.

FOLLOW SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" SHEETS A-C FOR DETAILS NOT SHOWN ON THIS SHEET.



LEGEND

- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)
- TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)
- TEMPORARY MARKING LINE PAINT 4-INCH
- TEMPORARY PEDESTRIAN BARRICADE
- PEDESTRIAN DETOUR
- WORK AREA
- TEMPORARY CURB RAMP WITH DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN SURFACE ASPHALT

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	TEMPORARY PEDESTRIAN ACCESS - PHASE 1	SHEET 39	E

NOTES:
FOLLOW SDD "TRAFFIC CONTROL, WORK ON PARKING LANE OR SHOULDER, UNDIVIDED ROADWAY" AT TEMPORARY CURB RAMPS AND CURB AND GUTTER REPLACEMENT AREAS WHEN ROADWAY IS OPEN TO LOCAL TRAFFIC.

DO NOT RESTRICT WATER FLOW IN DITCHES.

FOLLOW SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" SHEETS A-C FOR DETAILS NOT SHOWN ON THIS SHEET.

N

EXISTING R/W

STH 113

15 16 17 18 19 20 21

INSTALL BARRICADE PRIOR TO BRIDGE WORK STARTING. BARRICADE MAY BE REMOVED ONCE ALL BRIDGE WORK HAS BEEN COMPLETED.

SIXMILE CREEK

EXISTING R/W

PATH TO BE CLOSED FROM SUNSET LN TO W VERLEEN AVE WHILE WORK IS COMPLETED ON THE SHARED-USE PATH AT THE RAILROAD CROSSING. PLACE PATH CLOSED AHEAD SIGNS AT SUNSET LN/FAIRVIEW DR, STA 13+70 RT, AND AT W VERLEEN AVE.

EXISTING R/W

EXISTING R/W

22 23 24 25 26

STH 113

11' MIN

EXISTING R/W

EXISTING R/W

SIDEWALK CLOSED
R9-9
24"X12"

ALSO PLACE SIGN AT MUIRFIELD CT AND W VERLEEN AVE.

10' MIN

W VERLEEN AVE

SIDEWALK CLOSED
R9-9
24"X12"

ALSO PLACE SIGN AT FAIRBROOK DR AND W VERLEEN AVE

LEGEND

- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)
- TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)
- TEMPORARY MARKING LINE PAINT 4-INCH
- TEMPORARY PEDESTRIAN BARRICADE
- PEDESTRIAN DETOUR
- WORK AREA
- TEMPORARY CURB RAMP WITH DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN SURFACE ASPHALT

NOTES:
FOLLOW SDD "TRAFFIC CONTROL, WORK ON PARKING LANE OR SHOULDER, UNDIVIDED ROADWAY" AT TEMPORARY CURB RAMPS AND CURB AND GUTTER REPLACEMENT AREAS WHEN ROADWAY IS OPEN TO LOCAL TRAFFIC.

DO NOT RESTRICT WATER FLOW IN DITCHES.

FOLLOW SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" SHEETS A-C FOR DETAILS NOT SHOWN ON THIS SHEET.

N

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	TEMPORARY PEDESTRIAN ACCESS - PHASE 2	SHEET 40	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\021302-PE.DWG
LAYOUT NAME - 01
PLOT DATE : 10/21/2022 7:06 AM
PLOT BY : SCHERER, MEGAN S
PLOT NAME :
PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 44

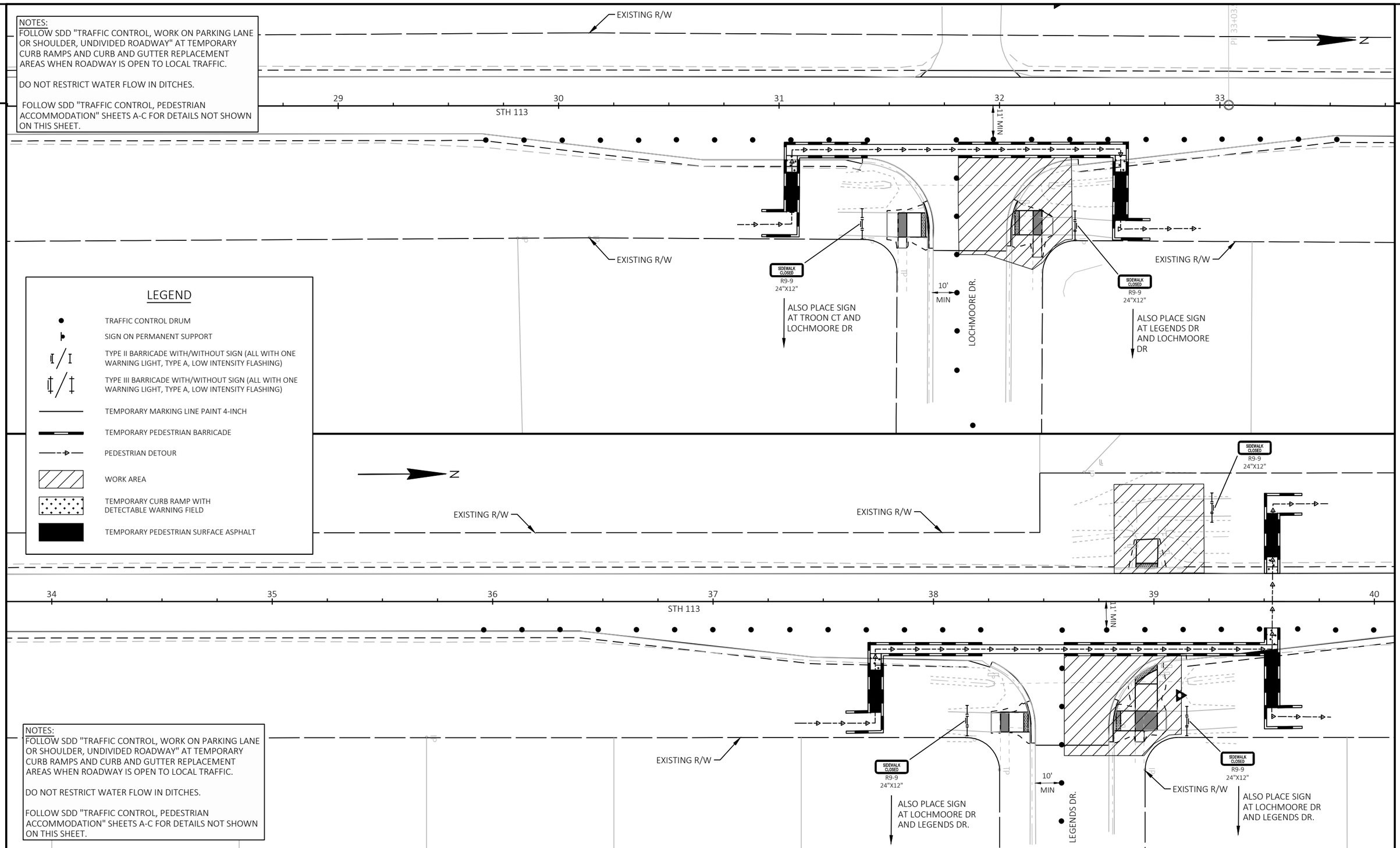
NOTES:
FOLLOW SDD "TRAFFIC CONTROL, WORK ON PARKING LANE OR SHOULDER, UNDIVIDED ROADWAY" AT TEMPORARY CURB RAMPS AND CURB AND GUTTER REPLACEMENT AREAS WHEN ROADWAY IS OPEN TO LOCAL TRAFFIC.

DO NOT RESTRICT WATER FLOW IN DITCHES.

FOLLOW SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" SHEETS A-C FOR DETAILS NOT SHOWN ON THIS SHEET.

LEGEND

- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)
- TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)
- TEMPORARY MARKING LINE PAINT 4-INCH
- TEMPORARY PEDESTRIAN BARRICADE
- PEDESTRIAN DETOUR
- WORK AREA
- TEMPORARY CURB RAMP WITH DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN SURFACE ASPHALT



NOTES:
FOLLOW SDD "TRAFFIC CONTROL, WORK ON PARKING LANE OR SHOULDER, UNDIVIDED ROADWAY" AT TEMPORARY CURB RAMPS AND CURB AND GUTTER REPLACEMENT AREAS WHEN ROADWAY IS OPEN TO LOCAL TRAFFIC.

DO NOT RESTRICT WATER FLOW IN DITCHES.

FOLLOW SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" SHEETS A-C FOR DETAILS NOT SHOWN ON THIS SHEET.

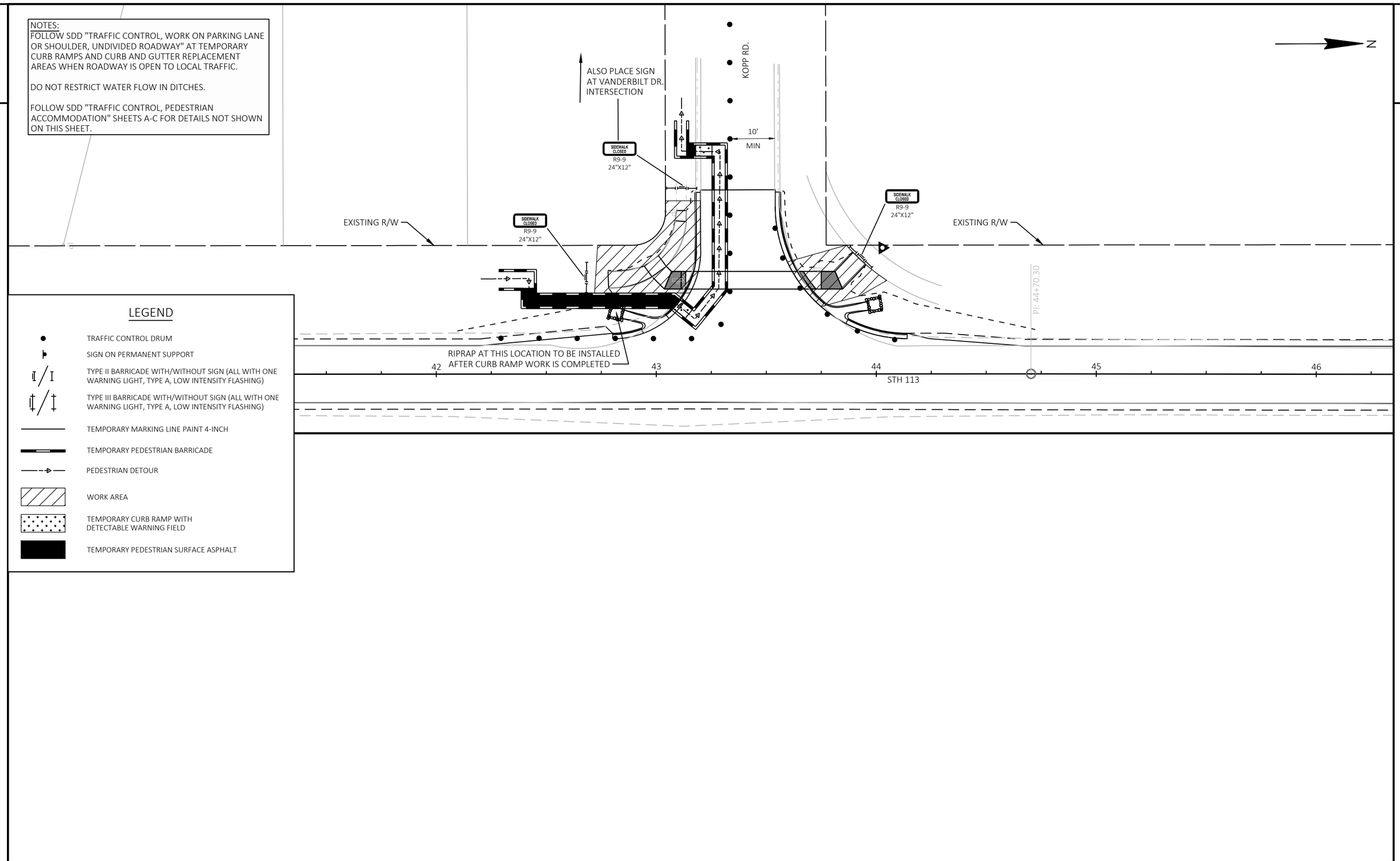
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	TEMPORARY PEDESTRIAN ACCESS - PHASE 2	SHEET 41	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\021302-PE.DWG
LAYOUT NAME - 02
PLOT DATE : 10/21/2022 7:06 AM
PLOT BY : SCHERER, MEGAN S
PLOT NAME :
PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 44

NOTES:
FOLLOW SDD "TRAFFIC CONTROL, WORK ON PARKING LANE OR SHOULDER, UNDIVIDED ROADWAY" AT TEMPORARY CURB RAMPS AND CURB AND GUTTER REPLACEMENT AREAS WHEN ROADWAY IS OPEN TO LOCAL TRAFFIC.

DO NOT RESTRICT WATER FLOW IN DITCHES.

FOLLOW SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" SHEETS A-C FOR DETAILS NOT SHOWN ON THIS SHEET.



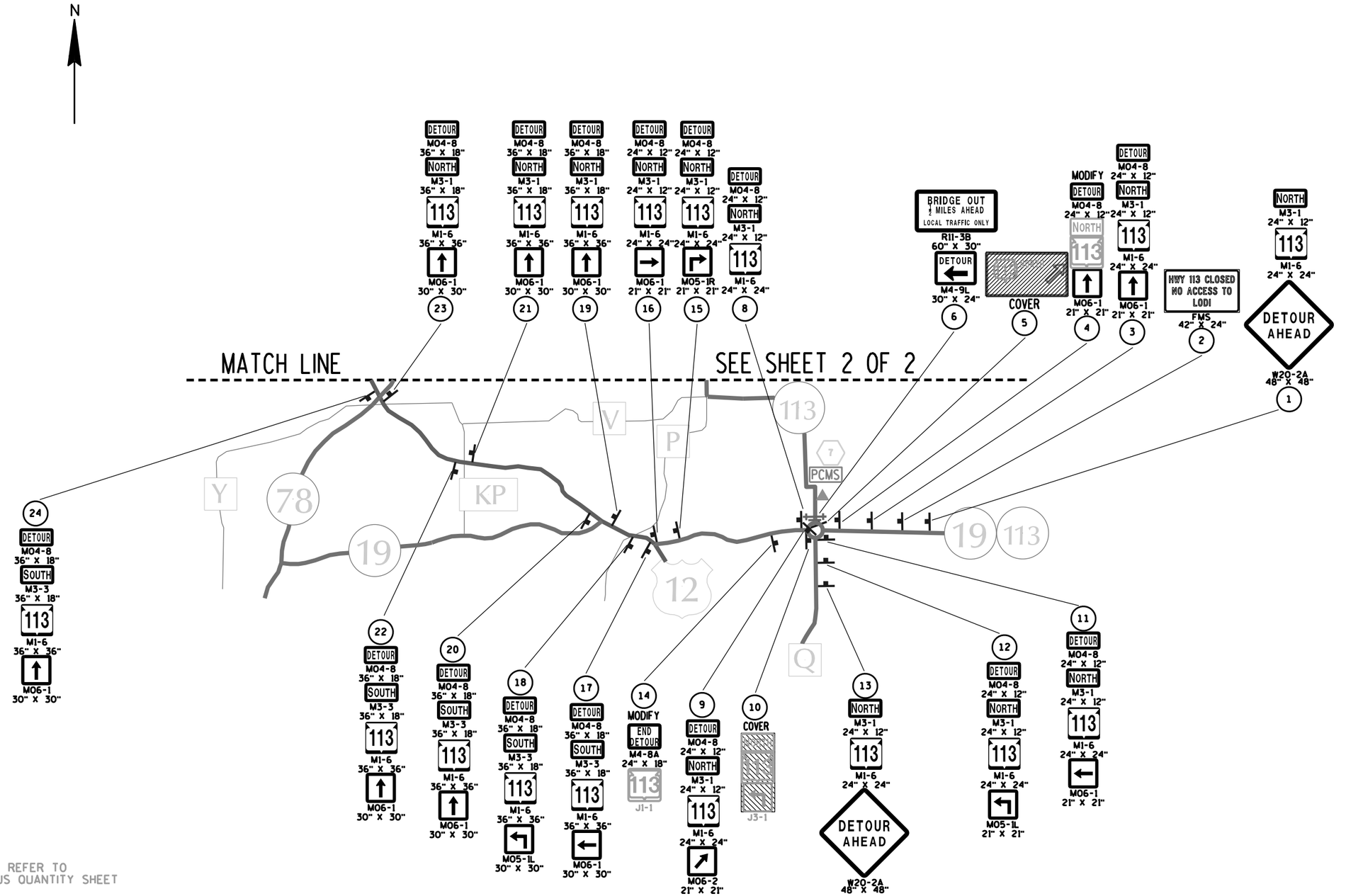
LEGEND

- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)
- TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)
- TEMPORARY MARKING LINE PAINT 4-INCH
- TEMPORARY PEDESTRIAN BARRICADE
- PEDESTRIAN DETOUR
- WORK AREA
- TEMPORARY CURB RAMP WITH DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN SURFACE ASPHALT

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	TEMPORARY PEDESTRIAN ACCESS - PHASE 2	SHEET 42	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\021302-PE.DWG
LAYOUT NAME - 03
PLOT DATE : 10/21/2022 7:06 AM
PLOT BY : SCHERER, MEGAN S
PLOT NAME :
PLOT SCALE : ##########
WISDOT/CADDS SHEET 44

2



LEGEND

- (X) SIGN NUMBER. REFER TO MISCELLANEOUS QUANTITY SHEET
- PCMS (X) PORTABLE CHANGEABLE MESSAGE SIGN
- SIGN MOUNTED ON TYPE III BARRICADE
- POST MOUNTED SIGN

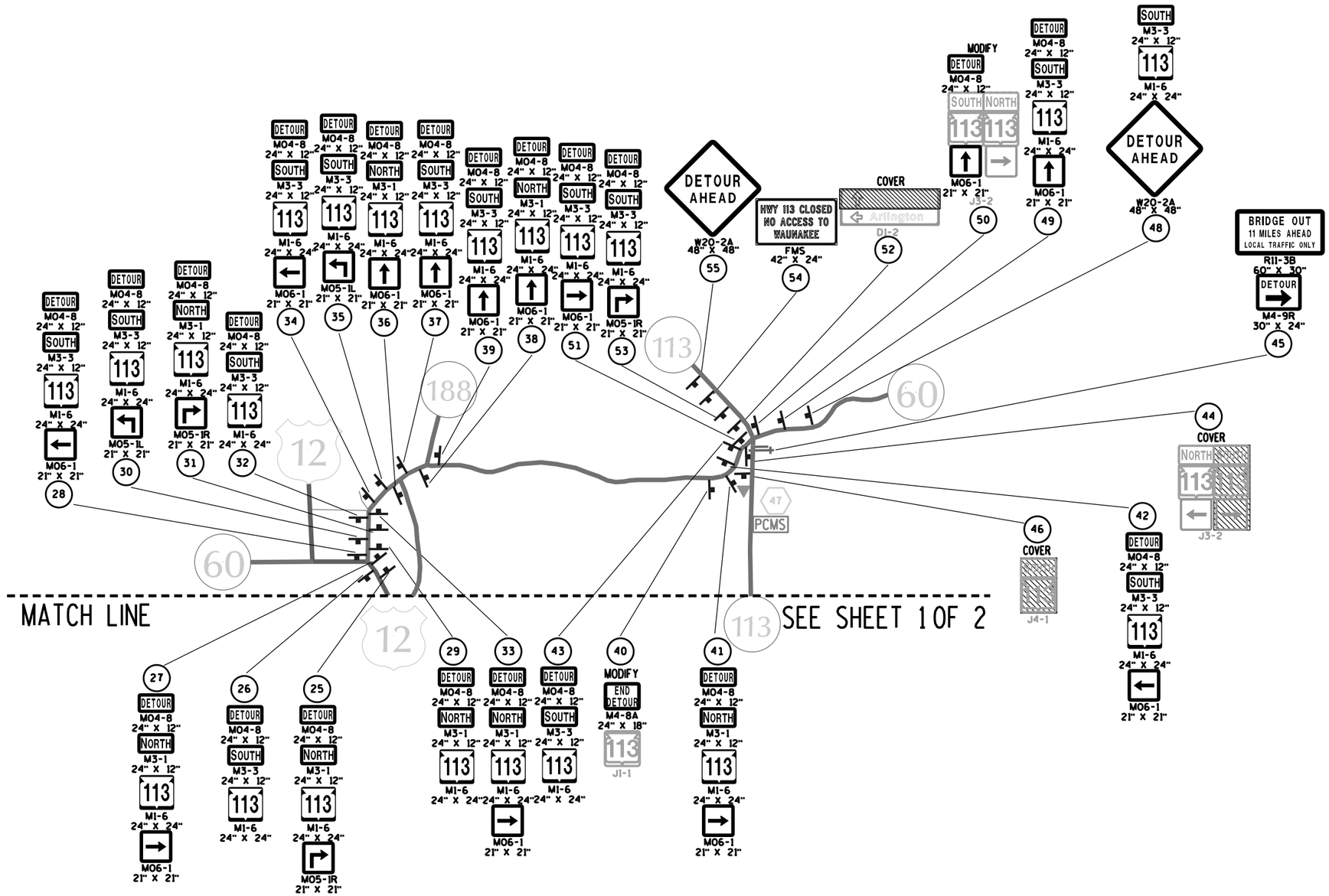
SHEET 1 OF 2
PLAN SHEET PRODUCED BY WisDOT-NE REGION

PROJECT NO:5280-03-70	HWY: STH 113	COUNTY: DANE	DETOUR SIGNING DETAIL	SHEET	E

FILE NAME : $$....designfile....$$ PLOT DATE : $$...plottingdate...$$ PLOT BY : $$...plotuser...$$ PLOT NAME : PLOT SCALE : $$.....plotscale.....$$

2

N



LEGEND

- (X) SIGN NUMBER. REFER TO MISCELLANEOUS QUANTITY SHEET
- PCMS (X) PORTABLE CHANGEABLE MESSAGE SIGN
- SIGN MOUNTED ON TYPE III BARRICADE
- POST MOUNTED SIGN

SHEET 2 OF 2
PLAN SHEET PRODUCED BY WisDOT-NE REGION

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	DETOUR SIGNING DETAIL	SHEET	E

FILE NAME : $$....designfile....$$ PLOT DATE : $$...plottingdate...$$ PLOT BY : $$...plotuser...$$ PLOT NAME : PLOT SCALE : $$.....plotscale.....$$

Estimate Of Quantities

5280-03-70

Line	Item	Item Description	Unit	Total	Qty
0002	203.0100	Removing Small Pipe Culverts	EACH	2.000	2.000
0004	203.0220	Removing Structure (structure) 01. STA 22+07	EACH	1.000	1.000
0006	203.0260	Removing Structure Over Waterway Minimal Debris (structure) 01. B-13-945	EACH	1.000	1.000
0008	204.0110	Removing Asphaltic Surface	SY	520.000	520.000
0010	204.0115	Removing Asphaltic Surface Butt Joints	SY	253.000	253.000
0012	204.0120	Removing Asphaltic Surface Milling	SY	58,643.000	58,643.000
0014	204.0150	Removing Curb & Gutter	LF	146.000	146.000
0016	204.0155	Removing Concrete Sidewalk	SY	141.000	141.000
0018	204.0165	Removing Guardrail	LF	340.000	340.000
0020	204.0280	Sealing Pipes	EACH	1.000	1.000
0022	205.0100	Excavation Common	CY	5,898.000	5,898.000
0024	206.1001	Excavation for Structures Bridges (structure) 01. B-13-871	EACH	1.000	1.000
0026	209.2500	Backfill Granular Grade 2	TON	297.000	297.000
0028	210.1500	Backfill Structure Type A	TON	180.000	180.000
0030	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 5280-03-70	EACH	1.000	1.000
0032	211.0400	Prepare Foundation for Asphaltic Shoulders	STA	387.000	387.000
0034	213.0100	Finishing Roadway (project) 01. 5280-03-70	EACH	1.000	1.000
0036	305.0110	Base Aggregate Dense 3/4-Inch	TON	991.000	991.000
0038	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	3,809.000	3,809.000
0040	305.0130	Base Aggregate Dense 3-Inch	TON	44.000	44.000
0042	312.0110	Select Crushed Material	TON	2,100.000	2,100.000
0044	415.0080	Concrete Pavement 8-Inch	SY	52.000	52.000
0046	415.0410	Concrete Pavement Approach Slab	SY	83.000	83.000
0048	450.4000	HMA Cold Weather Paving	TON	4,180.000	4,180.000
0050	455.0605	Tack Coat	GAL	7,729.000	7,729.000
0052	460.2000	Incentive Density HMA Pavement	DOL	9,400.000	9,400.000
0054	460.6223	HMA Pavement 3 MT 58-28 S	TON	8,615.000	8,615.000
0056	460.6224	HMA Pavement 4 MT 58-28 S	TON	6,008.000	6,008.000
0058	465.0105	Asphaltic Surface	TON	741.000	741.000
0060	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	91.000	91.000
0062	465.0315	Asphaltic Flumes	SY	39.000	39.000
0064	502.0100	Concrete Masonry Bridges	CY	361.000	361.000
0066	502.3200	Protective Surface Treatment	SY	403.000	403.000
0068	502.3210	Pigmented Surface Sealer	SY	85.000	85.000
0070	505.0400	Bar Steel Reinforcement HS Structures	LB	5,400.000	5,400.000
0072	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	53,090.000	53,090.000
0074	505.0800.S	Bar Steel Reinforcement HS Stainless Structures	LB	390.000	390.000
0076	516.0500	Rubberized Membrane Waterproofing	SY	22.000	22.000
0078	522.0130	Culvert Pipe Reinforced Concrete Class III 30-Inch	LF	63.000	63.000
0080	522.1030	Apron Endwalls for Culvert Pipe Reinforced Concrete 30-Inch	EACH	2.000	2.000
0082	522.2434	Culvert Pipe Reinforced Concrete Horizontal Elliptical Class HE-IV 34x53-Inch	LF	55.000	55.000
0084	522.2634	Apron Endwalls for Culvert Pipe Reinforced Concrete Horizontal Elliptical 34x53-Inch	EACH	2.000	2.000
0086	524.0118	Culvert Pipe Salvaged 18-Inch	LF	16.000	16.000
0088	524.0618	Apron Endwalls for Culvert Pipe Salvaged 18-Inch	EACH	1.000	1.000
0090	550.1100	Piling Steel HP 10-Inch X 42 Lb	LF	595.000	595.000
0092	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	754.000	754.000
0094	601.0590	Concrete Curb & Gutter 4-Inch Sloped 36-Inch Type TBTT	LF	118.000	118.000
0096	602.0410	Concrete Sidewalk 5-Inch	SF	3,228.000	3,228.000
0098	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	218.000	218.000
0100	602.0605	Curb Ramp Detectable Warning Field Radial Yellow	SF	41.000	41.000

5280-03-70

Line	Item	Item Description	Unit	Total	Qty
0102	602.3010	Concrete Surface Drains	CY	19.000	19.000
0104	606.0200	Riprap Medium	CY	27.000	27.000
0106	606.0300	Riprap Heavy	CY	185.000	185.000
0108	608.0318	Storm Sewer Pipe Reinforced Concrete Class III 18-Inch	LF	2.000	2.000
0110	611.0530	Manhole Covers Type J	EACH	1.000	1.000
0112	611.0612	Inlet Covers Type C	EACH	1.000	1.000
0114	611.2004	Manholes 4-FT Diameter	EACH	2.000	2.000
0116	612.0406	Pipe Underdrain Wrapped 6-Inch	LF	160.000	160.000
0118	614.0150	Anchor Assemblies for Steel Plate Beam Guard	EACH	4.000	4.000
0120	614.2500	MGS Thrie Beam Transition	LF	156.000	156.000
0122	614.2610	MGS Guardrail Terminal EAT	EACH	4.000	4.000
0124	618.0100	Maintenance and Repair of Haul Roads (project) 01. 5280-03-70	EACH	1.000	1.000
0126	619.1000	Mobilization	EACH	1.000	1.000
0128	620.0300	Concrete Median Sloped Nose	SF	98.000	98.000
0130	624.0100	Water	MGAL	77.000	77.000
0132	625.0500	Salvaged Topsoil	SY	2,906.000	2,906.000
0134	628.1104	Erosion Bales	EACH	13.000	13.000
0136	628.1504	Silt Fence	LF	1,545.000	1,545.000
0138	628.1520	Silt Fence Maintenance	LF	1,545.000	1,545.000
0140	628.1905	Mobilizations Erosion Control	EACH	7.000	7.000
0142	628.1910	Mobilizations Emergency Erosion Control	EACH	4.000	4.000
0144	628.2008	Erosion Mat Urban Class I Type B	SY	2,906.000	2,906.000
0146	628.6005	Turbidity Barriers	SY	195.000	195.000
0148	628.7005	Inlet Protection Type A	EACH	3.000	3.000
0150	628.7015	Inlet Protection Type C	EACH	4.000	4.000
0152	628.7555	Culvert Pipe Checks	EACH	23.000	23.000
0154	629.0210	Fertilizer Type B	CWT	3.200	3.200
0156	630.0130	Seeding Mixture No. 30	LB	65.000	65.000
0158	630.0171	Seeding Mixture No. 70A	LB	1.000	1.000
0160	630.0500	Seed Water	MGAL	79.000	79.000
0162	633.5200	Markers Culvert End	EACH	4.000	4.000
0164	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	9.000	9.000
0166	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	1.000	1.000
0168	637.2210	Signs Type II Reflective H	SF	26.000	26.000
0170	638.2102	Moving Signs Type II	EACH	5.000	5.000
0172	638.2602	Removing Signs Type II	EACH	2.000	2.000
0174	638.3000	Removing Small Sign Supports	EACH	7.000	7.000
0176	642.5001	Field Office Type B	EACH	1.000	1.000
0178	643.0300	Traffic Control Drums	DAY	1,916.000	1,916.000
0180	643.0410	Traffic Control Barricades Type II	DAY	838.000	838.000
0182	643.0420	Traffic Control Barricades Type III	DAY	4,536.000	4,536.000
0184	643.0705	Traffic Control Warning Lights Type A	DAY	8,064.000	8,064.000
0186	643.0900	Traffic Control Signs	DAY	24,860.000	24,860.000
0188	643.0920	Traffic Control Covering Signs Type II	EACH	5.000	5.000
0190	643.1000	Traffic Control Signs Fixed Message	SF	16.000	16.000
0192	643.1050	Traffic Control Signs PCMS	DAY	14.000	14.000
0194	643.3165	Temporary Marking Line Paint 6-Inch	LF	1,554.000	1,554.000
0196	643.3170	Temporary Marking Line Epoxy 6-Inch	LF	1,554.000	1,554.000
0198	643.5000	Traffic Control	EACH	1.000	1.000
0200	644.1410	Temporary Pedestrian Surface Asphalt	SF	1,317.000	1,317.000

5280-03-70

Line	Item	Item Description	Unit	Total	Qty
0202	644.1601	Temporary Pedestrian Curb Ramp	DAY	154.000	154.000
0204	644.1605	Temporary Pedestrian Detectable Warning Field	SF	110.000	110.000
0206	644.1810	Temporary Pedestrian Barricade	LF	2,511.000	2,511.000
0208	645.0111	Geotextile Type DF Schedule A	SY	76.000	76.000
0210	645.0120	Geotextile Type HR	SY	410.000	410.000
0212	645.0220	Geogrid Type SR	SY	3,155.000	3,155.000
0214	646.2020	Marking Line Epoxy 6-Inch	LF	28,835.000	28,835.000
0216	646.2040	Marking Line Grooved Wet Ref Epoxy 6-Inch	LF	33,594.000	33,594.000
0218	646.4020	Marking Line Epoxy 10-Inch	LF	541.000	541.000
0220	646.5320	Marking Railroad Crossing Epoxy	EACH	2.000	2.000
0222	646.6120	Marking Stop Line Epoxy 18-Inch	LF	103.000	103.000
0224	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	320.000	320.000
0226	646.7520	Marking Crosswalk Epoxy Block Style 24-Inch	LF	136.000	136.000
0228	646.8220	Marking Island Nose Epoxy	EACH	4.000	4.000
0230	650.4500	Construction Staking Subgrade	LF	724.000	724.000
0232	650.5000	Construction Staking Base	LF	1,596.000	1,596.000
0234	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	608.000	608.000
0236	650.6000	Construction Staking Pipe Culverts	EACH	2.000	2.000
0238	650.6501	Construction Staking Structure Layout (structure) 01. B-13-871	EACH	1.000	1.000
0240	650.8000	Construction Staking Resurfacing Reference	LF	19,276.000	19,276.000
0242	650.9000	Construction Staking Curb Ramps	EACH	11.000	11.000
0244	650.9500	Construction Staking Sidewalk (project) 01. 5280-03-70	EACH	1.000	1.000
0246	650.9911	Construction Staking Supplemental Control (project) 01. 5280-03-70	EACH	1.000	1.000
0248	690.0150	Sawing Asphalt	LF	1,125.000	1,125.000
0250	690.0250	Sawing Concrete	LF	81.000	81.000
0252	715.0502	Incentive Strength Concrete Structures	DOL	2,166.000	2,166.000
0254	715.0720	Incentive Compressive Strength Concrete Pavement	DOL	500.000	500.000
0256	740.0440	Incentive IRI Ride	DOL	15,280.000	15,280.000
0258	999.2000.S	Installing and Maintaining Bird Deterrent System (station) 01. 17+25	EACH	1.000	1.000
0260	ASP.1T0A	On-the-Job Training Apprentice at $5.00/HR	HRS	1,200.000	1,200.000
0262	ASP.1T0G	On-the-Job Training Graduate at $5.00/HR	HRS	600.000	600.000
0264	SPV.0035	Special 01. Foundation Backfill	CY	80.000	80.000
0266	SPV.0060	Special 01. Verify and Replace Existing Land Parcel Monuments	EACH	6.000	6.000
0268	SPV.0060	Special 02. Research and Locate Existing Land Parcel Monuments	EACH	6.000	6.000
0270	SPV.0060	Special 03. Verify Landmark Reference Monuments	EACH	8.000	8.000
0272	SPV.0090	Special 01. Pre-Boring Special	LF	32.000	32.000
0274	SPV.0180	Special 01. Removing Distressed Pavement Milling	SY	5,877.000	5,877.000
0276	SPV.0195	Special 01. Select Crushed Material for Travel Corridor	TON	68.000	68.000

3

REMOVING STRUCTURE AND CULVERTS

CATEGORY	STATION	203.0100 REMOVING SMALL PIPE CULVERTS EACH	203.0220.01 REMOVING STRUCTURE (STRUCTURE) (01. 22+07) EACH	REMARKS
0010	STA 22+07	-	1	REMOVE 52' OF 72" CMP
0010	STA 170+32	1	-	30" CMP
0010	STA 210+40	1	-	24" CMP
	TOTAL 0010	2	1	

EXCAVATION COMMON

CATEGORY	STATION	TO	STATION	LOCATION	205.0100 EXCAVATION COMMON CY	UNUSABLE PAVEMENT MATERIAL	UNEXPANDED FILL	EXPANDED FILL (1)	MASS ORDINATE +/- (2)	REMARKS
0010	15+00	-	23+50	STH 113	3,101	165	116	166	2,770	RECONSTRUCTION SEGMENT
0010	21+58		22+56	STH 113	135	-	-	-	135	72" CULVERT REMOVAL
0010	19+85	-	44+15	CURB RAMP LOCATIONS	133	-	-	-	133	
0010	42+20	-	44+66	STH 113/KOPP RD	35	-	-	-	35	SHOULDER WIDENING
0010	210+21	-	210+62	STH 113	182	-	-	-	182	CULVERT REPLACEMENT
0010				UNDISTRIBUTED	2,312	-	-	-	2,312	EBS AREAS
				TOTAL 0010	5,898					

NOTES:
1) EXPANDED FILL FACTOR = 1.43
2) THE MASS ORDINATE + OR - QT CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL

SEALING PIPES

CATEGORY	STATION	OFFSET	204.0280 SEALING PIPES EACH	REMARKS
0010	22+07	29' RT	1	72" CULVERT PIPE
	TOTAL 0010		1	

PREPARE FOUNDATION ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	211.0101.01 PREPARE FOUNDATION FOR ASPHALTIC PAVING (PROJECT) (01. 5280-03-70) EACH	211.0400 PREPARE FOUNDATION FOR ASPHALTIC SHOULDERS STA
0010	23+09	-	215+85	STH 113 - LT	-	193
0010	22+00	-	215+85	STH 113 - RT	-	194
0010				STH 113	1	-
				TOTAL 0010	1	387

BRIDGE APPROACH ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	415.0080 CONCRETE PAVEMENT 8-INCH SY	415.0410 CONCRETE PAVEMENT APPROACH SLAB SY	601.0590 CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE TBTT LF
0010	16+28	-	16+87	SOUTH OF BRIDGE - LT	11	21	59
0010	16+28	-	16+87	SOUTH OF BRIDGE - RT	11	21	59
0010	17+72	-	17+88	NORTH OF BRIDGE - LT	15	21	0
0010	17+72	-	17+88	NORTH OF BRIDGE - RT	15	20	0
				TOTAL 0010	52	83	118

CULVERT ITEMS

CATEGORY	STATION	LOCATION	209.2500 BACKFILL GRANULAR GRADE 2 TON	522.0130 CULVERT PIPE REINFORCED CONCRETE CLASS III 30-INCH LF	522.1030 APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 30-INCH EACH	522.2434 CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL CLASS HE-IV 34X53-INCH LF	522.2634 APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL 34X53-INCH EACH	524.0118 CULVERT PIPE SALVAGED 18-INCH LF	524.0618 APRON ENDWALLS FOR CULVERT PIPE SALVAGED 18-INCH EACH	* 624.0100 WATER MGAL	633.5200 MARKERS CULVERT END EACH	SPV.0035.01 SPECIAL (01. FOUNDATION BACKFILL) CY	REMARKS
0010		STH 113	-	-	-	-	-	16	1	-	-	-	SE OF BRIDGE IN RIPRAP
0010	22+07	STH 113	297	-	-	-	-	-	-	3	-	-	72" CULVERT REMOVAL FROST HEAVE REPAIR AREA
0010	170+33	STH 113	-	63	2	-	-	-	-	-	2	-	CULVERT REPLACMENT
0010	210+42	STH 113	-	-	-	55	2	-	-	1	2	80	CULVERT REPLACMENT
		TOTAL 0010	297	63	2	55	2	16	1	4	4	80	

* ADDITIONAL QUANTITIES LISTED ELSEWHERE

3

ASPHALT ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	204.0110 REMOVING ASPHALTIC SURFACE SY	204.0115 REMOVING ASPHALTIC SURFACE BUTT JOINTS SY	204.0120 REMOVING ASPHALTIC SURFACE MILLING SY	450.4000 HMA COLD WEATHER PAVING TON	455.0605 TACK COAT GAL	460.6223 HMA PAVEMENT 3 MT 58-28 S TON	460.6224 HMA PAVEMENT 4 MT 58-28 S TON	* 465.0105 ASPHALTIC SURFACE TON	465.0120 ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES TON	SPV.0180.01 SPECIAL (01. REMOVING DISTRESSED PAVEMENT MILLING) SY
0010	15+00	-	22+30	STH 113 - RECONSTRUCTION	-	-	-	227	233	610	228	-	-	-
0010	22+30	-	25+00	STH 113 - MILL & OVERLAY	-	-	879	50	101	107	77	-	-	-
0010	25+00	-	35+00	STH 113 - MILL & OVERLAY	-	-	2,908	183	348	393	284	-	-	-
0010	35+00	-	45+00	STH 113 - MILL & OVERLAY	-	-	2,920	183	349	393	284	-	-	-
0010	45+00	-	55+00	STH 113 - MILL & OVERLAY	-	-	2,905	183	348	393	284	-	-	-
0010	55+00	-	65+00	STH 113 - MILL & OVERLAY	-	-	2,963	183	352	393	284	-	-	-
0010	65+00	-	75+00	STH 113 - MILL & OVERLAY	-	-	2,909	183	349	393	284	-	-	-
0010	75+00	-	85+00	STH 113 - MILL & OVERLAY	-	-	2,879	183	346	393	284	-	-	-
0010	85+00	-	95+00	STH 113 - MILL & OVERLAY	-	-	2,859	183	345	393	284	-	-	-
0010	95+00	-	105+00	STH 113 - MILL & OVERLAY	-	-	2,905	183	348	393	284	-	-	-
0010	105+00	-	115+00	STH 113 - MILL & OVERLAY	-	-	3,019	180	354	386	278	-	-	-
0010	115+00	-	125+00	STH 113 - MILL & OVERLAY	-	-	2,678	169	321	363	262	-	-	-
0010	125+00	-	135+00	STH 113 - MILL & OVERLAY	-	-	2,652	169	319	363	262	-	-	-
0010	135+00	-	145+00	STH 113 - MILL & OVERLAY	-	-	2,656	169	320	363	262	-	-	-
0010	145+00	-	155+00	STH 113 - MILL & OVERLAY	-	-	2,642	169	319	363	262	-	-	-
0010	155+00	-	165+00	STH 113 - MILL & OVERLAY	-	-	2,648	169	319	363	262	-	-	-
0010	165+00	-	175+00	STH 113 - MILL & OVERLAY	-	-	2,654	169	320	363	262	-	-	-
0010	175+00	-	185+00	STH 113 - MILL & OVERLAY	-	-	2,651	169	319	363	262	-	-	-
0010	185+00	-	195+00	STH 113 - MILL & OVERLAY	-	-	2,643	169	319	363	262	-	-	-
0010	195+00	-	205+00	STH 113 - MILL & OVERLAY	-	-	2,646	169	319	363	262	-	-	-
0010	205+00	-	215+85	STH 113 - MILL & OVERLAY	-	-	2,934	184	351	394	284	-	-	-
0010				W VERLEEN AVE	-	9	502	29	58	61	44	-	-	-
0010				LOCHMOORE DR	-	8	566	36	68	77	56	-	-	-
0010				LEGENDS DR	-	8	563	38	69	80	58	-	-	-
0010				KOPP RD	454	-	-	30	23	63	45	-	-	-
0010				EASY ST	-	6	268	17	32	36	26	-	-	-
0010				MAIER RD	-	5	244	16	30	33	24	-	-	-
0010				CUBA VALLEY RD (LT)	-	7	959	64	117	136	98	-	-	-
0010				CUBA VALLEY RD (RT)	-	7	799	52	97	111	80	-	-	-
0010				MADIGAN RD	-	6	175	12	21	24	18	-	-	-
0010				RIPP LN	-	5	234	15	28	32	23	-	-	-
0010				CTH V (LT)	-	17	215	15	27	31	22	-	-	-
0010				CTH V (RT)	-	17	168	11	21	23	17	-	-	-
0010	170+23	-	170+41	CULVERT REPLACEMENT	-	-	-	5	5	-	-	18	-	-
0010	210+21	-	210+62	CULVERT REPLACEMENT	-	-	-	11	11	-	-	40	-	-
0010				DRIVEWAYS	66	158	-	25	11	-	-	-	91	-
0010				DISTRESSED PAVEMENT AREAS	-	-	-	178	412	-	-	659	-	5,877
				TOTAL 0010	520	253	58,643	4,180	7,729	8,615	6,008	717	91	5,877

* ADDITIONAL QUANTITIES LISTED ELSEWHERE

ASPHALTIC FLUMES

CATEGORY	LOCATION	465.0315 ASPHALTIC FLUMES SY
0010	CUBA VALLEY RD - SW	11
0010	CUBA VALLEY RD - SW	9
0010	CUBA VALLEY RD - SE	19
	TOTAL 0010	39

RIPRAP & GEOTEXTILE FABRIC

CATEGORY	STATION	LOCATION	606.0200 RIPRAP MEDIUM CY	645.0120 GEOTEXTILE TYPE HR SY
0010	16+44	BRIDGE SW	3	12
0010	16+44	BRIDGE SE	3	12
0010	43+98	KOPP RD LT	2	12
0010	42+82	KOPP RD RT	3	12
0010	108+92	CUBA VALLEY RD - SW	2	11
0010	109+43	CUBA VALLEY RD - SW	3	12
0010	108+76	CUBA VALLEY RD - SE	3	12
0010	170+32	CULVERT RT	5	19
0010	210+41	CULVERT LT	3	14
		TOTAL 0010	27	116

GUARDRAIL ITEMS

CATEGORY	LOCATION	204.0165 REMOVING GUARDRAIL LF	614.2500 MGS THRIE BEAM TRANSITION LF	614.2610 MGS GUARDRAIL TERMINAL EAT EACH
0010	SW OF B-13-945	85	39	1
0010	SE OF B-13-946	85	39	1
0010	NW OF B-13-947	85	39	1
0010	NE OF B-13-948	85	39	1
	TOTAL 0010	340	156	4

CONCRETE SURFACE DRAINS

CATEGORY	STATION	TO	STATION	LOCATION	602.3010 CONCRETE SURFACE DRAINS CY
0010	16+28	-	16+87	SOUTH OF BRIDGE - LT	8
0010	16+28	-	16+87	SOUTH OF BRIDGE - RT	7
0010	42+94	-	43+06	KOPP ROAD - SOUTH DRAIN	2
0010	43+74	-	43+86	KOPP ROAD - NORTH DRAIN	2
				TOTAL 0010	19

STORM SEWER ITEMS

CATEGORY	STATION	OFFSET	LOCATION	608.0318 STORM SEWER PIPE REINFORCED CONCRETE CLASS III 18-INCH LF	611.0530 MANHOLE COVERS TYPE J EACH	611.0612 INLET COVERS TYPE C EACH	611.2004 MANHOLES 4-FT DIAMETER EACH
0010	16+02	34' RT	STH 113	2	-	1	1
0010	16+05	30' RT	STH 113	-	1	-	1
			TOTAL 0010	2	1	1	2

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	MISCELLANEOUS QUANTITIES	SHEET 49	E

CURB RAMP RECONSTRUCTION ITEMS

CATEGORY	LOCATION	204.0150 REMOVING CURB & GUTTER LF	204.0155 REMOVING CONCRETE SIDEWALK SY	* 305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	* 465.0105 ASPHALTIC SURFACE TON	601.0411 CONCRETE CURB & GUTTER 30-INCH TYPE D LF	602.0410 CONCRETE SIDEWALK 5-INCH SF	602.0505 CURB RAMP DETECTABLE WARNING FIELD YELLOW SF	602.0605 CURB RAMP DETECTABLE WARNING FIELD RADIAL YELLOW SF	620.0300 CONCRETE MEDIAN SLOPED NOSE SF	* 624.0100 WATER MGAL
0010	RR CROSSING - S PED CROSSING	-	-	10	1	-	325	32	-	-	0.2
0010	RR CROSSING - N PED CROSSING	-	-	11	1	-	340	29	-	-	0.2
0010	RR CROSSING - S MEDIAN	-	-	9	-	164	164	-	-	49	0.2
0010	RR CROSSING - N MEDIAN	-	-	14	-	260	261	-	-	49	0.2
0010	W VERLEEN AVE - SE	33	21	18	4	33	299	21	-	-	0.2
0010	W VERLEEN AVE - NE	31	20	18	3	30	322	20	-	-	0.2
0010	LOCHMOORE DR - SE	22	21	12	3	22	166	22	-	-	0.2
0010	LOCHMOORE DR - NE	21	16	12	3	21	185	22	-	-	0.2
0010	LEGENDS DR - SE	19	11	10	3	19	119	18	-	-	0.2
0010	LEGENDS DR - NE	20	38	16	3	20	379	18	24	-	0.2
0010	LEGENDS DR- NW	-	14	4	-	-	126	20	-	-	0.2
0010	KOPP RD - SW	-	-	37	3	87	308	16	-	-	0.4
0010	KOPP RD - NW	-	-	33	-	98	234	-	17	-	0.4
	TOTAL 0010	146	141	204	24	754	3,228	218	41	98	3.0

* ADDITIONAL QUANTITIES LISTED ELSEWHERE

NOTE 1: CONSTRUCT CONCRETE SIDEWALK OVER 4" OF BASE AGGREGATE DENSE 1 1/4-INCH

NOTE 2: CONSTRUCT ASPHALTIC SURFACE SIDEWALK OVER 6" OF BASE AGGREGATE DENSE 1 1/4-INCH

EROSION CONSTROL MOBILIZATON

CATEGORY	LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH
0010	STH 113	7	4
	TOTAL 0010	7	4

AGGREGATE ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	* 305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	305.0130 BASE AGGREGATE DENSE 3-INCH TON	312.0110 SELECT CRUSHED MATERIAL TON	* 624.0100 WATER MGAL	645.0220 GEOGRID TYPE SR SY	REMARKS
0010	15+00	-	16+88	STH 113	56	786	-	577	15	912	RECONSTRUCTION SEGMENT
0010	17+72	-	22+31	STH 113	121	2012	-	1381	36	2180	RECONSTRUCTION SEGMENT
0010	22+31	-	23+50	STH 113 - LT	25	49	44	-	2	63	WIDENING SEGMENT
0010	23+50	-	215+36	STH 113 - RT/LT SHOULDERS	747	-	-	-	8	-	SHOULDERS
0010				KOPP RD	-	175	-	-	2	-	
0010	170+23	-	170+23	STH 113	17	24	-	26	1	-	CULVERT REPLACEMENT
0010	210+21	-	210+62	STH 113	25	265	-	116	5	-	CULVERT REPLACEMENT
0010	21+27	-	21+67	RR BUNGELOW PAD	-	26	-	-	1	-	BUNGELOW PAD AT RAILROAD
				TOTAL 0010	991	3,337	44	2,100	70	3,155	

* ADDITIONAL QUANTITIES LISTED ELSEWHERE

TURBIDITY BARRIER

CATEGORY	LOCATION	628.6005 TURBIDITY BARRIER SY
0010	SIXMILE CREEK BRIDGE SOUTH ABUTMENT	95
0010	SIXMILE CREEK BRIDGE NORTH ABUTMENT	100
	TOTAL 0010	195

EROSION CONTROL ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	628.1104 EROSION BALES EACH	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF	628.7005 INLET PROTECTION TYPE A EACH	628.7015 INLET PROTECTION TYPE C EACH	628.7555 CULVERT PIPE CHECKS EACH	REMARKS
0010	15+00	-	16+96	STH 113 - LT	5	207	207	1	-	-	
0010	17+62	-	19+22	STH 113 - LT	5	182	182	-	-	-	
0010	20+05	-	20+97	STH 113 - LT	-	109	109	-	-	-	
0010	20+60	-	22+45	STH 113 - RT	-	185	185	-	-	3	
0010	22+75	-	24+00	STH 113 - RT	-	-	-	1	2	3	
0010	21+45	-	23+70	STH 113 - LT	-	255	255	-	-	-	
0010	110+00	-	110+00	STH 113 - RT	-	-	-	-	1	-	
0010	169+83	-	170+85	STH 113	-	135	135	-	-	4	CULVERT REPLACEMENT
0010	210+20	-	210+62	STH 113	-	163	163	-	-	8	CULVERT REPLACEMENT
				UNDISTRIBUTED	3	309	309	1	1	5	
				TOTAL 0010	13	1,545	1,545	3	4	23	

RESTORATION ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	625.0500 SALVAGED TOPSOIL SY	628.2008 EROSION MAT URBAN CLASS I TYPE B SY	629.0210 FERTILIZER TYPE B CWT	630.0130 SEEDING MIXTURE NO. 30 LB	630.0171 SEEDING MIXTURE NO. 70A LB	630.0500 SEED WATER MGAL
0010	15+00	-	17+00	STH 113 - RT	211	211	0.2	4	0.2	5
0010	15+00	-	16+95	STH 113 - LT	199	199	0.2	4	0.2	5
0010	17+62	-	20+35	STH 113 - RT	244	244	0.2	5	0.2	6
0010	17+64	-	21+05	STH 113 - LT	661	661	0.5	12	0.2	15
0010	20+62	-	22+38	STH 113 - RT	247	247	0.2	5	-	6
0010	21+28	-	23+65	STH 113 - LT	318	318	0.2	6	-	8
0010				PATH RAILROAD CROSSING SOUTH	9	9	0.1	1	-	1
0010				PATH RAILROAD CROSSING NORTH	13	13	0.1	1	-	1
0010				W VERLEEN AVE - SOUTH CURB RAMP	26	26	0.1	1	-	1
0010				W VERLEEN AVE - NORTH CURB RAMP	24	24	0.1	1	-	1
0010				LOCHMOORE DR - SOUTH CURB RAMPS	10	10	0.1	1	-	1
0010				LOCHMOORE DR - NORTH CURB RAMPS	10	10	0.1	1	-	1
0010				LEGENDS DR - SOUTH CURB RAMP	6	6	0.1	1	-	1
0010				LEGENDS DR - NORTH CURB RAMP	20	20	0.1	1	-	1
0010				LEGENDS DR - NORTHWEST CURB RAMP	7	7	0.1	1	-	1
0010				KOPP RD - SOUTH CURB RAMP	138	138	0.1	3	-	4
0010				KOPP RD - NORTH CURB RAMP	128	128	0.1	3	-	3
0010				CUBA VALLEY RD - FLUMES	145	145	0.1	3	-	4
0010				CULVERT 170+32	89	89	0.1	2	-	2
0010				CULVERT 210+41	136	136	0.1	3	-	4
0010				UNDISTRIBUTED	265	265	0.3	6	0.2	8
				TOTAL 0010	2,906	2,906	3.2	65	1	79

TRAFFIC CONTROL ITEMS

CATEGORY	LOCATION	DAYS	* 643.0420 TRAFFIC CONTROL BARRICADES TYPE III NO. IN SERVICE	DAY	* 643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A NO. IN SERVICE	DAY	* 643.0900 TRAFFIC CONTROL SIGNS NO. IN SERVICE	DAY
0010	START OF PROJECT	126	2	252	4	504	3	378
0010	SOUTH OF BRIDGE	126	5	630	6	756	1	126
0010	NORTH OF BRIDGE	126	5	630	6	756	1	126
0010	W VERLEEN AVE	126	2	252	4	504	3	378
0010	LOCHMOORE DR	126	2	252	4	504	3	378
0010	LEGENDS DR	126	2	252	4	504	3	378
0010	KOPP RD	126	2	252	4	504	3	378
0010	EASY ST	126	2	252	4	504	3	378
0010	MAIER RD	126	2	252	4	504	3	378
0010	CUBA VALLEY RD - LT	126	2	252	4	504	3	378
0010	CUBA VALLEY RD - RT	126	2	252	4	504	3	378
0010	MADIGAN RD	126	2	252	4	504	3	378
0010	RIPP LN	126	2	252	4	504	3	378
0010	END OF PROJECT	126	2	252	4	504	1	126
	TOTAL 0010			4,284		7,560		4,536

* ADDITIONAL QUANTITIES LISTED ELSEWHERE

3

PAVEMENT MARKING ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	646.2020 MARKING LINE EPOXY 6-INCH YELLOW LF	646.2020 MARKING LINE EPOXY 6-INCH WHITE LF	646.2040 MARKING LINE GROOVED WET REF EPOXY 6-INCH WHITE LF	646.4020 MARKING LINE EPOXY 10-INCH LF	646.5320 MARKING RAILROAD CROSSINGS EPOXY EACH	646.6120 MARKING STOP LINE EPOXY 18-INCH LF	646.7420 MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH LF	646.7520 MARKING CROSSWALK EPOXY BLOCK STYLE 24-INCH LF	646.8220 MARKING ISLAND NOSE EPOXY EACH	643.3170 TEMPORARY MARKING LINE EPOXY 6-INCH YELLOW LF	643.3165 TEMPORARY MARKING LINE PAINT 6-INCH YELLOW LF
0010	15+00	-	19+72	STH 113	1,096	944	-	-	1	-	-	-	-	-	-
0010	22+30	-	23+06	STH 113	304	152	-	-	-	-	-	-	-	7	7
0010	23+06	-	25+48	STH 113	484	365	-	-	1	-	-	-	2	20	20
0010	25+48	-	27+92	STH 113	305	488	-	-	-	-	-	-	2	20	20
0010	27+92	-	64+60	STH 113	917	3,105	2,992	154	-	-	-	136	-	294	294
0010	64+60	-	75+05	STH 113	1,307	-	1,808	-	-	-	-	-	-	84	84
0010	75+05	-	88+53	STH 113	2,696	-	2,696	-	-	-	-	-	-	108	108
0010	88+53	-	100+15	STH 113	1,453	-	2,300	-	-	-	-	-	-	93	93
0010	100+15	-	117+62	STH 113	437	-	2,737	387	-	-	-	-	-	140	140
0010	117+62	-	129+04	STH 113	1,428	-	4,502	-	-	-	-	-	-	92	92
0010	129+04	-	154+27	STH 113	5,046	-	4,287	-	-	-	-	-	-	202	202
0010	154+27	-	165+57	STH 113	1,413	-	2,260	-	-	-	-	-	-	91	91
0010	165+57	-	169+10	STH 113	89	-	706	-	-	-	-	-	-	29	29
0010	169+10	-	179+50	STH 113	1,300	-	2,080	-	-	-	-	-	-	84	84
0010	179+50	-	191+00	STH 113	1,438	-	2,300	-	-	-	-	-	-	92	92
0010	191+00	-	202+45	STH 113	1,432	-	2,290	-	-	-	-	-	-	92	92
0010	202+45	-	215+63	STH 113	2,636	-	2,636	-	-	30	-	-	-	106	106
0010				W VERLEEN AVE	-	-	-	-	-	19	86	-	-	-	-
0010				LOCHMOORE DR	-	-	-	-	-	17	68	-	-	-	-
0010				LEGENDS DR	-	-	-	-	-	17	68	-	-	-	-
0010				KOPP RD	-	-	-	-	-	20	98	-	-	-	-
				SUBTOTAL	23,781	5,054									
				TOTAL 0010	28,835		33,594	541	2	103	320	136	4	1,554	1,554

SIGN ITEMS

CATEGORY	STATION	LOCATION	634.0614 POSTS WOOD 4X6-INCH X 14-FT EACH	634.0616 POSTS WOOD 4X6-INCH X 16-FT EACH	637.2210 SIGNS TYPE II REFLECTIVE H SF	638.2102 MOVING SIGNS TYPE II EACH	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	REMARKS
0010	12+75	STH 113 RT	-	-	-	-	1	1	NARROW BRIDGE
0010	17+74	STH 113 RT	-	1	-	1	-	1	R X R
0010	19+85	STH 113 CL	1	-	3	-	-	-	KEEP LEFT/RIGHT
0010	20+00	STH 113 LT	1	-	7	-	1	1	INSTALL DO NOT ENTER/ REMOVE NARROW BRIDGE
0010	22+18	STH 113 CL	1	-	3	-	-	-	KEEP LEFT/RIGHT
0010	22+25	STH 113 RT	1	-	7	-	-	-	DO NOT ENTER
0010	23+77	STH 113 LT	1	-	-	1	-	1	R X R
0010	23+86	STH 113 RT	1	-	-	1	-	1	STOP SIGN
0010	27+56	STH 113 RT	1	-	6	-	-	-	W14-3 NO PASSING ZONE
0010	38+83	STH 113 RT/LEGENDS DR	1	-	-	1	-	1	STOP SIGN
0010	43+15	STH 113 LT/KOPP RD	1	-	-	1	-	1	STOP SIGN
		TOTAL 0010	9	1	26	5	2	7	

SAWING ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	690.0150 SAWING ASPHALT LF	690.0250 SAWING CONCRETE LF
0010	15+00	-	15+00	START PROJECT	27	-
0010	19+86	-	20+41	PATH AT RR CROSSING	30	-
0010	22+30	-	22+30	END OF RECONSTRUCT 22+30	36	-
0010	22+30	-	23+09	SHOULDER WIDENING AREA	109	-
0010	23+04	-	24+18	W VERLEEN AVE - CURB RAMPS	107	18
0010	31+48	-	32+24	LOCHMOORE DR - CURB RAMPS	104	18
0010	38+26	-	39+05	LEGENDS DR - CURB RAMPS	67	40
0010	42+20	-	44+68	KOPP RD & STH 113	250	-
0010	43+20	-	43+54	KOPP RD	34	-
0010	42+78	-	43+95	KOPP RD - CURB RAMPS	204	5
0010	169+82	-	170+83	CULVERT REPLACEMENT	48	-
0010	209+90	-	210+92	CULVERT REPLACEMENT	48	-
0010	19+26	-	19+87	COMMERCIAL DRIVEWAY	61	-
				TOTAL 0010	1,125	81

TRAFFIC CONTROL DETOUR SIGN SUMMARY

SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 126 DAYS	643.0900 * SIGNS DAYS	643.0420 * BARRICADES TYPE III DAYS	643.0705 * WARNING LIGHTS TYPE A DAYS	643.1000 SIGNS FIXED MESSAGE SF	643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	NO OF CYCLES	643.0920 COVERING SIGNS TYPE II EACH	REMARKS
1	STH 19/113, E. OF CTH Q, PLACE 1500' E. OF CTH Q RAB INTERSECTION	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	W 20-2A	48"X48"	1	126	126							
2	STH 19/113, E. OF CTH Q, PLACE 1000' E. OF CTH Q RAB INTERSECTION	FMS	48"X24"	1					8				SEE SIGN DETAIL SHEET
3	STH 19/113, E. OF CTH Q, PLACE 500' E. OF CTH Q RAB INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 6-1	21"X21"	1	126	126							AHEAD
4	STH 19/113, AT CTH Q, MODIFY EXISTING J3-1 SIGN AS SHOWN AT CTH Q RAB INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	MO 6-1	21"X21"	1	126	126							AHEAD
5	STH 19/113, AT CTH Q, COVER EXISTING SPLITTER ISLAND SIGN AT NORTH LEG EXIT										1	1	COVER ENTIRE SIGN
6	STH 113, AT STH 19, PLACE ON RIGHT SHOULDER AT STH 19 RAB INTERSECTION	R 11-3B	60"X30"	1	126	126	126	252					1/2 MILE AHEAD
	"	M 4-9L	30"X24"	1	126	126							
7	STH 113, N. OF STH 19, PLACE ON RIGHT SHOULDER, FIELD DETERMINE LOCATION	PCMS		1						7			PLACE IN ADVANCE OF CLOSURE
8	STH 19, W. OF STH 113, PLACE 250' W. OF STH 113 RAB INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
9	STH 19, AT STH 113, PLACE ABOVE EXISTING SPLITTER ISLAND SIGN ON WEST LEG EXIT	MO 4-8	24"X12"	1	126	126							
	"	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 6-2	21"X21"	1	126	126							TILT RIGHT
10	STH 19, AT STH 113, COVER EXISTING J3-1 SIGN AS SHOWN										1	1	COVER ENTIRE SIGN
11	CTH Q, S. OF STH 19/113, PLACE 100' S. OF STH 19/113 RAB INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 6-1	21"X21"	1	126	126							LEFT
12	CTH Q, S. OF STH 19/113, PLACE 600' S. OF STH 19/113 RAB INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 5-1L	21"X21"	1	126	126							
13	CTH Q, S. OF STH 19/113, PLACE 1000' S. OF STH 19/113 RAB INTERSECTION	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	W 20-2A	48"X48"	1	126	126							
14	STH 19, W. OF STH 113, MODIFY EXISTING J1-1 SIGN AS SHOWN	M 4-8A	24"X18"	1	126	126							
15	STH 19, E. OF USH 12, PLACE 500' E. OF USH 12 INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 5-1R	21"X21"	1	126	126							
16	STH 19, AT USH 12, PLACE RIGHT OF EXISTING J3-3 SIGN AT USH 12 INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 6-1	21"X21"	1	126	126							RIGHT
17	USH 12/STH 19, AT STH 19, PLACE AT BEGIN OF LEFT TURN LANE IN MEDIAN TO STH 19	MO 4-8	36"X18"	1	126	126							
	"	M 3-3	36"X18"	1	126	126							
	"	M 1-6	36"X36"	1	126	126							113
	"	MO 6-1	30"X30"	1	126	126							LEFT
	PAGE SUBTOTALS			44		5,292	126	252	8	7		2	

* ADDITIONAL QUANTITIES LISTED ELSEWHERE

TRAFFIC CONTROL DETOUR SIGN SUMMARY

* * *

SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 126 DAYS	643.0900 SIGNS DAYS	643.0420 BARRICADES TYPE III DAYS	643.0705 WARNING LIGHTS TYPE A DAYS	643.1000 SIGNS FIXED MESSAGE SF	643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	NO OF CYCLES	643.0920 COVERING SIGNS TYPE II EACH	REMARKS
18	USH 12/STH 19, W. OF STH 19, PLACE 1000' W. OF STH 19 INTERSECTION IN MEDIAN	MO 4-8	36"X18"	1	126	126							
	"	M 3-3	36"X18"	1	126	126							
	"	M 1-6	36"X36"	1	126	126							113
	"	MO 5-1L	30"X30"	1	126	126							
19	USH 12/STH 19, E. OF STH 19, PLACE 250' E. OF STH 19 INTERSECTION	MO 4-8	36"X18"	1	126	126							
	"	M 3-1	36"X18"	1	126	126							
	"	M 1-6	36"X36"	1	126	126							113
	"	MO 6-1	30"X30"	1	126	126							AHEAD
20	USH 12, W. OF STH 19, PLACE 250' W. OF STH 19 INTERSECTION	MO 4-8	36"X18"	1	126	126							
	"	M 3-3	36"X18"	1	126	126							
	"	M 1-6	36"X36"	1	126	126							113
	"	MO 6-1	30"X30"	1	126	126							AHEAD
21	USH 12, E. OF CTH KP, PLACE 250' E. OF CTH KP INTERSECTION	MO 4-8	36"X18"	1	126	126							
	"	M 3-1	36"X18"	1	126	126							
	"	M 1-6	36"X36"	1	126	126							113
	"	MO 6-1	30"X30"	1	126	126							AHEAD
22	USH 12, W. OF CTH KP, PLACE 250' W. OF CTH KP INTERSECTION	MO 4-8	36"X18"	1	126	126							
	"	M 3-3	36"X18"	1	126	126							
	"	M 1-6	36"X36"	1	126	126							113
	"	MO 6-1	30"X30"	1	126	126							AHEAD
23	USH 12, E. OF STH 78, PLACE 250' E. OF STH 78 INTERSECTION	MO 4-8	36"X18"	1	126	126							
	"	M 3-1	36"X18"	1	126	126							
	"	M 1-6	36"X36"	1	126	126							113
	"	MO 6-1	30"X30"	1	126	126							AHEAD
24	USH 12, W. OF STH 78, PLACE 250' W. OF STH 78 INTERSECTION	MO 4-8	36"X18"	1	126	126							
	"	M 3-3	36"X18"	1	126	126							
	"	M 1-6	36"X36"	1	126	126							113
	"	MO 6-1	30"X30"	1	126	126							AHEAD
25	USH 12, E. OF STH 60, PLACE 750' E. OF STH 60 INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 5-1R	21"X21"	1	126	126							
26	USH 12, E. OF STH 60, PLACE 250' E. OF STH 60 INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-3	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
27	USH 12, AT STH 60, PLACE RIGHT OF EXISTING J3-3 SIGN AT STH 60 INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 6-1	21"X21"	1	126	126							RIGHT
28	STH 60, AT USH 12, PLACE ABOVE EXISTING J3-2 SIGN AT USH 12 INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-3	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 6-1	21"X21"	1	126	126							LEFT
29	STH 60, N. OF USH 12, PLACE 250' N. OF USH 12 INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	PAGE SUBTOTALS			46		5,796	0	0	0	0		0	

* ADDITIONAL QUANTITIES LISTED ELSEWHERE

TRAFFIC CONTROL DETOUR SIGN SUMMARY

SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 126 DAYS	643.0900 * SIGNS DAYS	643.0420 * BARRICADES TYPE III DAYS	643.0705 * WARNING LIGHTS TYPE A DAYS	643.1000 SIGNS FIXED MESSAGE SF	643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	NO OF CYCLES	643.0920 COVERING SIGNS TYPE II EACH	REMARKS
30	STH 60, N. OF USH 12, PLACE 500' N. OF USH 12 INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-3	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 5-1L	21"X21"	1	126	126							
31	STH 60, S. OF PRAIRIE ST, PLACE 500' S. OF PRAIRIE ST INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 5-1R	21"X21"	1	126	126							
32	STH 60, S. OF PRAIRIE ST, PLACE 250' S. OF PRAIRIE ST INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-3	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
33	STH 60, AT PRAIRIE ST, PLACE RIGHT OF EXISTING R1-1 SIGN AT PRAIRIE ST INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 6-1	21"X21"	1	126	126							RIGHT
34	STH 60, AT PRAIRIE ST, PLACE RIGHT OF EXISTING J3-2 SIGN AT PRAIRIE ST INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-3	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 6-1	21"X21"	1	126	126							LEFT
35	STH 60, N. OF PRAIRIE ST, PLACE 500' N. OF PRAIRIE ST INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-3	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 5-1L	21"X21"	1	126	126							
36	STH 60, W. OF STH 78, PLACE 250' W. OF STH 78 INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 6-1	21"X21"	1	126	126							AHEAD
37	STH 60, E. OF STH 78, PLACE 250' E. OF STH 78 INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-3	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 6-1	21"X21"	1	126	126							AHEAD
38	STH 60, W. OF STH 188, PLACE 250' W. OF STH 188 INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 6-1	21"X21"	1	126	126							AHEAD
39	STH 60, E. OF STH 188, PLACE 250' E. OF STH 188 INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-3	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 6-1	21"X21"	1	126	126							AHEAD
40	STH 60, W. OF STH 113, MODIFY EXISTING J1-1 SIGN AS SHOWN	M 4-8A	24"X18"	1	126	126							
41	STH 60, AT LODI ST, PLACE 100' PRIOR TO LODI ST INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-1	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 6-1	21"X21"	1	126	126							RIGHT
	PAGE SUBTOTALS			44		5,544	0	0	0	0		0	

* ADDITIONAL QUANTITIES LISTED ELSEWHERE

3

TRAFFIC CONTROL DETOUR SIGN SUMMARY

* * *

SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 126 DAYS	643.0900 SIGNS DAYS	643.0420 BARRICADES TYPE III DAYS	643.0705 WARNING LIGHTS TYPE A DAYS	643.1000 SIGNS FIXED MESSAGE SF	643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	NO OF CYCLES	643.0920 COVERING SIGNS TYPE II EACH	REMARKS
42	STH 60, AT WATER ST, PLACE 100' PRIOR TO WATER ST INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-3	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 6-1	21"X21"	1	126	126							LEFT
43	STH 60, W. OF STH 113, PLACE 100' W. OF STH 113 INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-3	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
44	STH 60, AT STH 113, COVER EXISTING J3-2 SIGN AS SHOWN										1	1	COVER" SOUTH 113 RT"
45	STH 113, AT STH 60, PLACE ON RIGHT SHOULDER IN SW QUADRANT OF INTERSECTION	R 11-3B	60"X30"	1	126	126	126	252					11 MILES AHEAD
	"	M 4-9R	30"X24"	1	126	126							
46	STH 113, S. OF STH 60, COVER EXISTING J4-1 SIGN AS SHOWN										1	1	COVER ENTIRE SIGN
47	STH 113, S. OF STH 60, PLACE ON RIGHT SHOULDER, FIELD DETERMINE LOCATION	PCMS		1						7			PLACE IN ADVANCE OF CLOSURE
48	STH 60, E. OF STH 113, PLACE 1000' E. OF STH 113 INTERSECTION	M 3-3	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	W 20-2A	48"X48"	1	126	126							
49	STH 60, E. OF STH 113, PLACE 500' E. OF STH 113 INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-3	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 6-1	21"X21"	1	126	126							AHEAD
50	STH 60, AT STH 113, MODIFY EXISTING J3-2 SIGN AS SHOWN	MO 4-8	24"X12"	1	126	126							
	"	MO 6-1	21"X21"	1	126	126							AHEAD
51	STH 113, AT STH 60, PLACE RIGHT OF EXISTING R1-1 SIGN	MO 4-8	24"X12"	1	126	126							
	"	M 3-3	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 6-1	21"X21"	1	126	126							RIGHT
52	STH 113, N. OF STH 60, COVER EXISTING D1-2 SIGN AS SHOWN										1	1	COVER "AHEAD-MADISON"
53	STH 113, N. OF STH 60, PLACE 500' N. OF STH 60 INTERSECTION	MO 4-8	24"X12"	1	126	126							
	"	M 3-3	24"X12"	1	126	126							
	"	M 1-6	24"X24"	1	126	126							113
	"	MO 5-1R	21"X21"	1	126	126							
54	STH 113, N. OF STH 60, PLACE 1000' N. OF STH 60 INTERSECTION	FMS	48"X24"	1					8				SEE SIGN DETAIL
55	STH 113, N. OF STH 60, PLACE 1500' N. OF STH 60 INTERSECTION	W 20-2A	48"X48"	1	126	126							
	PAGE SUBTOTALS			29		3,402	126	252	8	7		3	
	DETOUR TOTALS			**163**		**20,034**	**252**	**504**	**16**	**14**		**5**	

* ADDITIONAL QUANTITIES LISTED ELSEWHERE

TEMPORARY PEDESTRIAN FACILITIES

CATEGORY	STATION	TO	STATION	LOCATION	* 643.0900 TRAFFIC CONTROL SIGNS NO. IN SERVICE	643.0900 TRAFFIC CONTROL SIGNS DAY	643.0300 TRAFFIC CONTROL DRUMS NO. IN SERVICE	643.0300 TRAFFIC CONTROL DRUMS DAY	643.0410 TRAFFIC CONTROL BARRICADES TYPE II NO. IN SERVICE	643.0410 TRAFFIC CONTROL BARRICADES TYPE II DAY	644.1410 TEMPORARY PEDESTRIAN SURFACE ASPHALT SF	644.1601 TEMPORARY PEDESTRIAN CURB RAMP NO. IN SERVICE	644.1601 TEMPORARY PEDESTRIAN CURB RAMP DAY	644.1810 TEMPORARY PEDESTRIAN BARRICADE LF	644.1605 TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD SF	REMARKS
0010	16+28	-	18+28	STH 113	3	42	-	-	7	98	-	-	-	155	-	AT BRIDGE ALONG PEDESTRIAN PATH
0010	22+84	-	24+38	STH 113/W VERLEEN AVE	4	56	30	420	12	168	218	2	28	480	20	
0010	30+99	-	32+62	STH 113/LOCHMOORE DR	4	56	32	448	12	168	201	2	28	488	20	
0010	37+67	-	39+61	STH 113/LEGENDS	4	56	32	448	12	168	240	2	28	502	20	
0010	39+48	-	39+61	STH 113 LT	1	14	-	-	3	42	100	1	14	135	10	
0010	42+34	-	43+35	STH 113/KOPP RD	2	28	25	350	6	84	386	2	28	423	20	
0010				UNDISTRIBUTED		38		250		110	172	2	28	328	20	
				TOTAL 0010		290		1,916		838	1,317		154	2,511	110	

* ADDITIONAL QUANTITIES LISTED ELSEWHERE

STAKING ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	650.4500 CONSTRUCTION STAKING SUBGRADE LF	650.5000 CONSTRUCTION STAKING BASE LF	650.5500 CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER LF	650.6000 CONSTRUCTION STAKING PIPE CULVERTS EACH	650.8000 CONSTRUCTION STAKING RESURFACING REFERENCE LF	650.9000 CONSTRUCTION STAKING CURB RAMPS EACH	650.9500.01 CONSTRUCTION STAKING SIDEWALK (PROJECT) (01. 5280-03-70) EACH	650.9911.01 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (01. 5280-03-70) EACH	650.6501.01 CONSTRUCTION STAKING STRUCTURE LAYOUT (STRUCTURE) (01.B-13-871) EACH
0010	15+00	-	16+87	STH 113	187	187	--	--	--	--	--	--	--
0010	17+72	-	23+09	STH 113	537	537	424	--	--	--	--	--	--
0010	23+09	-	215+85	STH 113	--	--	--	--	19,276	--	--	--	--
0010	169+83	-	170+83	CULVERT REPLACEMENT	--	--	--	1	--	--	--	--	--
0010	209+90	-	210+92	CULVERT REPLACEMENT	--	--	--	1	--	--	--	--	--
0010	19+86	-	20+41	RAILROAD CROSSING	--	--	--	--	--	2	--	--	--
0010	23+04	-	24+15	W VERLEEN AVE	--	--	--	--	--	2	--	--	--
0010	31+49	-	32+24	LOCHMOORE DR	--	--	--	--	--	2	--	--	--
0010	38+26	-	39+05	LEGENDS DR	--	--	--	--	--	3	--	--	--
0010	0+97	-	1+69	KOPP RD	--	72	184	--	--	2	--	--	--
				UNDISTRIBUTED	--	800	--	--	--	--	1	1	1
				TOTAL 0010	724	1,596	608	2	19,276	11	1	1	1

BIRD DETERRENT SYSTEM

CATEGORY	STATION	LOCATION	999.2000.S.01 INSTALLING AND MAINTAINING BIRD DETERRENT SYSTEM (STATION) (01.17+25) EACH
0010	17+25	STH 113 Bridge over Sixmile Creek	1
		TOTAL 0010	1

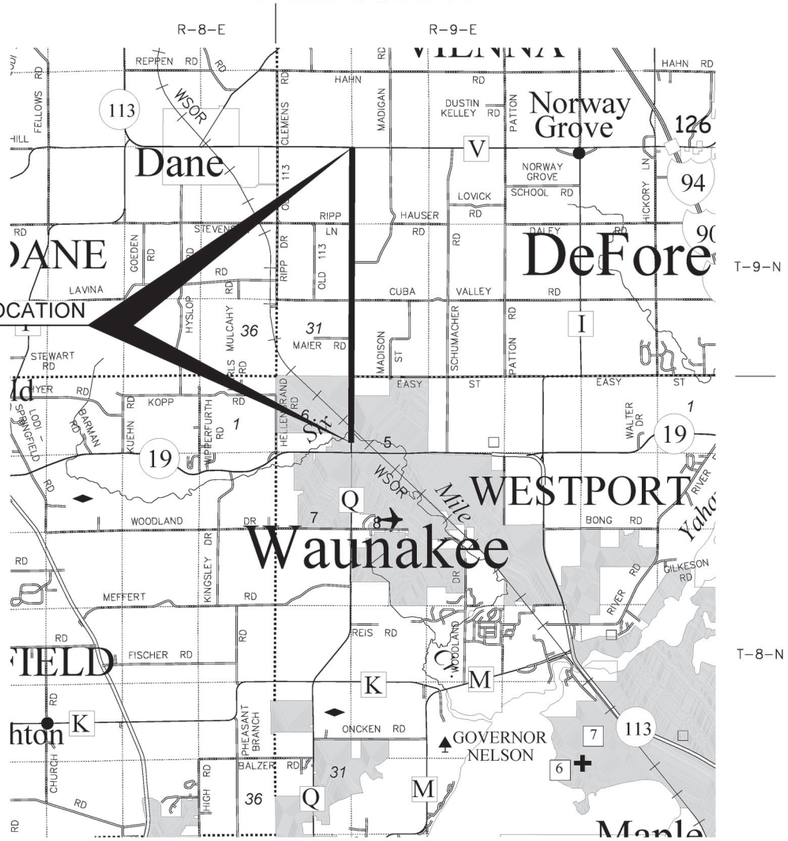
LAND PARCEL MONUMENTS

CATEGORY	LOCATION	SPV.0060.01 VERIFY AND REPLACE EXISTING LAND PARCEL MONUMENTS EACH	SPV.0060.02 RESEARCH AND LOCATE EXISTING LAND PARCEL MONUMNETS EACH
0010	STH 113 PROJECT	6	6
	TOTAL 0010	6	6

REFERENCE MONUMENTS

CATEGORY	STATION	OFFSET	SPV.0060.03 LANDMARK REFERENCE MONUMENTS EACH	REMARKS
0010	30+13.9	0.0’	1	T8N, R9E, S6/5
0010	56+60.8	1.2’ RT	1	T9N / T8N, R9E, S6/5/31/32
0010	83+16.5	4.8’ RT	1	T9N, R9E, S31/32
0010	109+73.4	3.2’ LT	1	T9N, R9E, S30/29/31/32
0010	136+19.5	1.0’ LT	1	T9N, R9E, S30/29
0010	162+86.0	0.8’ RT	1	T9N, R9E, S30/29/19/20
0010	189+42.7	0.1’ RT	1	T9N, R9E, S19/20
0010	215+99.5	0.0’	1	T9N, R9E, S19/20/18/17
		TOTAL 0010	8	

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
TRANSPORTATION PROJECT PLAT TITLE SHEET
5280-03-20
MADISON - LODI
SUNSET LANE TO CTH V
STH 113
DANE COUNTY



CONVENTIONAL SYMBOLS

- SECTION LINE
- QUARTER LINE
- SIXTEENTH LINE
- NEW REFERENCE LINE
- NEW R/W LINE
- EXISTING R/W OR HE LINE
- PROPERTY LINE
- LOT, TIE & OTHER MINOR LINES
- SLOPE INTERCEPT
- CORPORATE LIMITS
- UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)
- NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)
- TEMPORARY LIMITED EASEMENT AREA
- EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)
- TRANSMISSION STRUCTURES
- BUILDING — TO BE REMOVED
- BRIDGE — CULVERT
- SECTION CORNER SYMBOL
- SECTION CORNER MONUMENT
- R/W MONUMENT (TO BE SET)
- NON-MONUMENTED R/W POINT
- FOUND IRON PIN (1-INCH UNLESS NOTED)
- GEODETIC SURVEY MONUMENT
- SIXTEENTH CORNER MONUMENT
- SIGN
- OFF-PREMISE SIGN
- ELECTRIC POLE
- TELEPHONE POLE
- PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)
- ACCESS RESTRICTED BY ACQUISITION
- NO ACCESS (BY STATUTORY AUTHORITY)
- ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)
- NO ACCESS (NEW HIGHWAY)
- PARCEL NUMBER 25
- UTILITY NUMBER 40
- PARALLEL OFFSETS

CONVENTIONAL ABBREVIATIONS

ACCESS RIGHTS	AR	POINT OF INTERSECTION	PI
ACRES	AC	PROPERTY LINE	PL
AHEAD	AH	RECORDED AS	(100')
ALUMINUM	ALUM	REEL / IMAGE	R/I
AND OTHERS	ET AL	REFERENCE LINE	R/L
BACK	BK	REMAINING	REM
BLOCK	BLK	RESTRICTIVE DEVELOPMENT EASEMENT	RDE
CENTERLINE	C/L	RIGHT	RT
CERTIFIED SURVEY MAP	CSM	RIGHT OF WAY	R/W
CONCRETE	CONC	SECTION	SEC
COUNTY	CO	SEPTIC VENT	SEPV
COUNTY TRUNK HIGHWAY	CTH	SQUARE FEET	SF
DISTANCE	DIST	STATE TRUNK HIGHWAY	STH
CORNER	COR	STATION	STA
DOCUMENT NUMBER	DOC	TELEPHONE PEDESTAL	TP
EASEMENT	EASE	TEMPORARY LIMITED EASEMENT	TLE
EXISTING	EX	TRANSPORTATION PROJECT PLAT	TPP
GAS VALVE	GV	UNITED STATES HIGHWAY	USH
GRID NORTH	GN	VOLUME	V
HIGHWAY EASEMENT	HE		
IDENTIFICATION	ID		
LAND CONTRACT	LC		
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY	NGS		
NUMBER	NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		
POINT OF COMPOUND CURVE	PCC		

CURVE DATA

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL UTILITY SYMBOLS

- W — WATER
- G — GAS
- T — TELEPHONE
- OH — OVERHEAD TRANSMISSION LINES
- E — ELECTRIC
- TV — CABLE TELEVISION
- FO — FIBER OPTIC
- SAN — SANITARY SEWER
- SS — STORM SEWER

THE NOTES, CONVENTIONAL SIGNS, AND ABBREVIATIONS ARE ASSOCIATED WITH EACH TRANSPORTATION PROJECT PLAT FOR PROJECT 5280-03-20

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), DANE COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY ¾" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

A PERMANENT LIMITED EASEMENT (PLE) IS A RIGHT FOR CONSTRUCTION AND MAINTENANCE PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE, BUT WITHOUT PREJUDICE TO THE OWNER'S RIGHTS TO MAKE OR CONSTRUCT IMPROVEMENTS ON SAID LANDS OR TO FLATTEN THE SLOPES, PROVIDING SAID ACTIVITIES WILL NOT IMPAIR OR OTHERWISE ADVERSELY AFFECT THE HIGHWAY FACILITIES.

AN EASEMENT FOR HIGHWAY PURPOSES (HE), AS LONG AS SO USED, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN MADISON.

PARCEL AND UTILITY IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE TPP DETAIL SHEETS.

INFORMATION FOR THE BASIS OF EXISTING HIGHWAY RIGHT OF WAY POINTS OF REFERENCE AND ACCESS CONTROL ARE LISTED ON DETAIL SHEETS.

PROJECT NUMBER 5280-03-20 -4. 01
SHEET 2 OF 2
AMENDMENT NO:

FILE NAME : G:\00\00093\00093602\CADD\RW\5280-03-20_STH 113_WAUNAKEE_TPP.DWG
LAYOUT NAME: 5280-03-20_Title Sheet
PLOT DATE : 9/16/2015 3:04 PM PLOT BY : BRAD TISDALE PLOT NAME : _____
APPRAISAL PLAT DATE : _____ _____4.___

TRANSPORTATION PROJECT PLAT NO: 5280-03-20 - 4.01

PART OF LOT 1 OF CSM NO. 5320 RECORDED AS DOCUMENT NO. 2037727 LOCATED IN AND INCLUDING PART OF THE NE 1/4 OF THE SE 1/4 AND PART OF THE SE 1/4 OF THE SE 1/4 ALL BEING LOCATED IN SECTION 6, T8N, R9E, VILLAGE OF WAUNAKEE, DANE COUNTY, WISCONSIN.

RELOCATION ORDER: STH 113, MADISON-LODI, (SUNSET LANE TO CTH V), DANE COUNTY, WISCONSIN

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE: PREVIOUS PROJECT 5290-01-21, FIRST ADDITION SIXMILE CREEK SUBDIVISION, CSM NO. 5320, CSM NO. 8801, CSM NO. 12679 AND SCHNEEBERGER PLAT OF SURVEY M783-L.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN MADISON.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), DANE COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

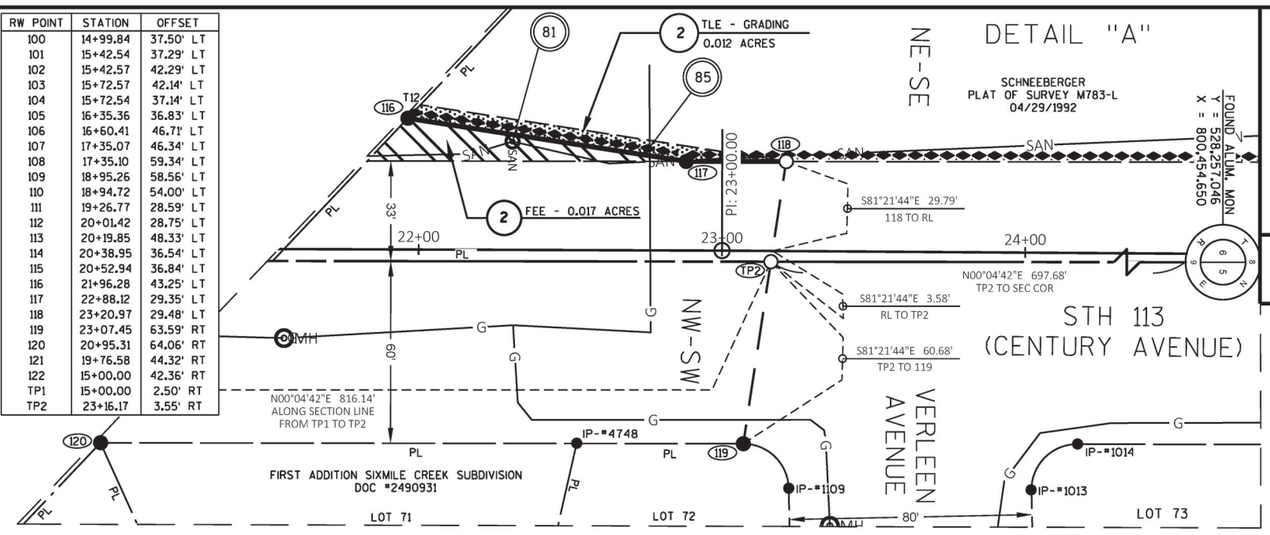
ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4"X24" IRON REBARS) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

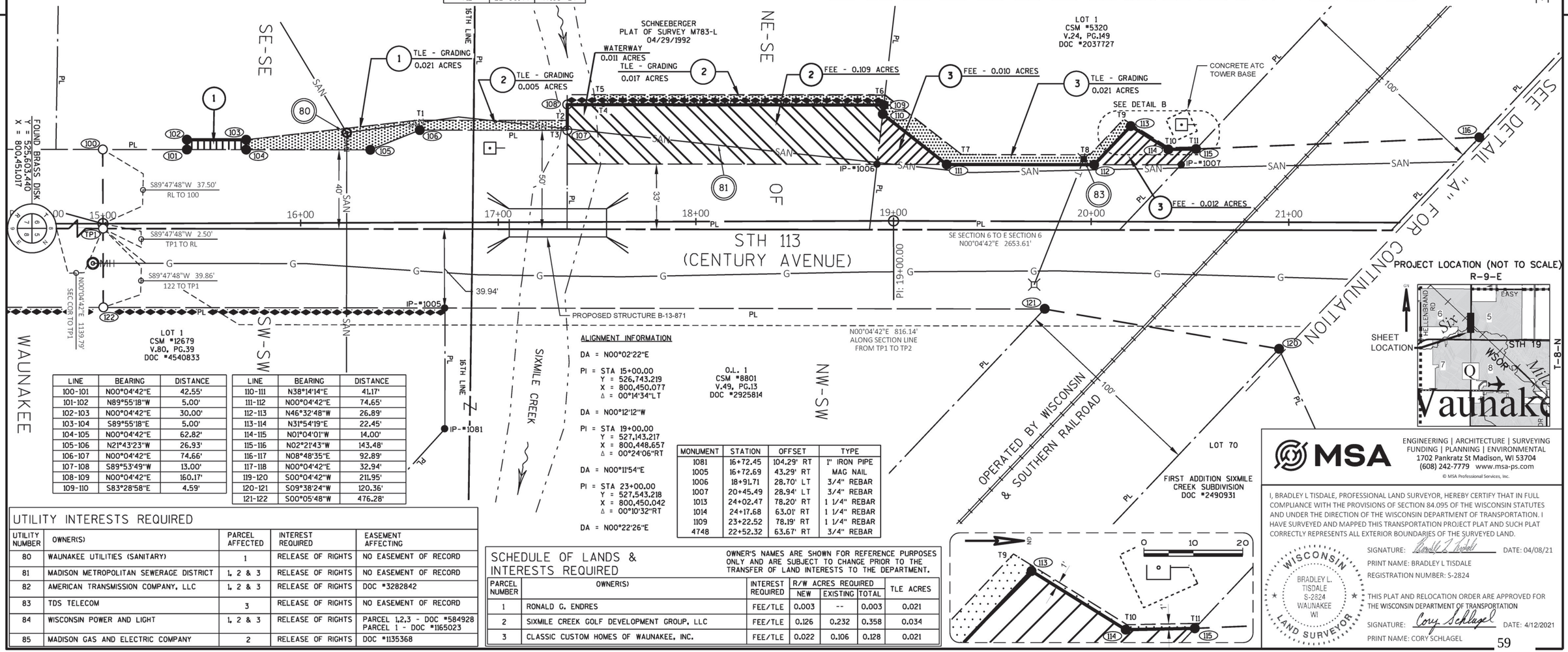
FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED IN THE OFFICE OF THE REGISTER OF DEEDS, IN DANE COUNTY, AS SHEET 2 OF 2 OF THIS DOCUMENT.

EXISTING ACCESS CONTROL ALONG STH 113 HAS BEEN ESTABLISHED FROM CSM NO. 12679 AND ACCESS COVENANT (PERMIT) RECORDED AS DOCUMENT NO. 2732910

TLE POINT	STATION	OFFSET
T1	16+59.48	51.72' LT
T2	17+32.51	51.36' LT
T3	17+32.08	46.36' LT
T4	17+49.25	59.27' LT
T5	17+48.73	64.28' LT
T6	18+95.85	63.55' LT
T7	19+34.43	33.60' LT
T8	19+99.26	33.74' LT
T9	20+16.21	51.76' LT
T10	20+39.22	37.54' LT
T11	20+53.90	37.87' LT
T12	22+00.44	47.68' LT

RW POINT	STATION	OFFSET
100	14+99.84	37.50' LT
101	15+42.54	37.29' LT
102	15+42.57	42.29' LT
103	15+72.57	42.14' LT
104	15+72.54	37.14' LT
105	16+35.36	36.83' LT
106	16+60.41	46.71' LT
107	17+35.07	46.34' LT
108	17+35.10	59.34' LT
109	18+95.26	58.56' LT
110	18+94.72	54.00' LT
111	19+26.77	28.59' LT
112	20+01.42	28.75' LT
113	20+19.85	48.33' LT
114	20+38.95	36.54' LT
115	20+52.94	36.84' LT
116	21+96.28	43.25' LT
117	22+88.12	29.35' LT
118	23+20.97	29.48' LT
119	23+07.45	63.59' RT
120	20+95.31	64.06' RT
121	19+76.58	44.32' RT
122	15+00.00	42.36' RT
TP1	15+00.00	2.50' RT
TP2	23+16.17	3.55' RT





LINE	BEARING	DISTANCE
100-101	N00°04'42"E	42.55'
101-102	N89°55'18"W	5.00'
102-103	N00°04'42"E	30.00'
103-104	S89°55'18"E	5.00'
104-105	N00°04'42"E	62.82'
105-106	N21°43'23"W	26.93'
106-107	N00°04'42"E	74.66'
107-108	S89°53'49"W	13.00'
108-109	N00°04'42"E	160.17'
109-110	S83°28'58"E	4.59'

LINE	BEARING	DISTANCE
110-111	N38°14'14"E	41.17'
111-112	N00°04'42"E	74.65'
112-113	N46°32'48"W	26.89'
113-114	N31°54'19"E	22.45'
114-115	N01°04'01"W	14.00'
115-116	N02°21'43"W	143.48'
116-117	N08°48'35"E	92.89'
117-118	N00°04'42"E	32.94'
119-120	S00°04'42"W	211.95'
120-121	S09°38'24"W	120.36'
121-122	S00°05'48"W	476.28'

MONUMENT	STATION	OFFSET	TYPE
1081	16+72.45	104.29' RT	1" IRON PIPE
1005	16+72.69	43.29' RT	MAG NAIL
1006	18+91.71	28.70' LT	3/4" REBAR
1007	20+45.49	28.94' LT	3/4" REBAR
1013	24+02.47	78.20' RT	1 1/4" REBAR
1014	24+17.68	63.01' RT	1 1/4" REBAR
1109	23+22.52	78.19' RT	1 1/4" REBAR
4748	22+52.32	63.67' RT	3/4" REBAR

UTILITY INTERESTS REQUIRED

UTILITY NUMBER	OWNER(S)	PARCEL AFFECTED	INTEREST REQUIRED	EASEMENT AFFECTING
80	WAUNAKEE UTILITIES (SANITARY)	1	RELEASE OF RIGHTS	NO EASEMENT OF RECORD
81	MADISON METROPOLITAN SEWERAGE DISTRICT	1, 2 & 3	RELEASE OF RIGHTS	NO EASEMENT OF RECORD
82	AMERICAN TRANSMISSION COMPANY, LLC	1, 2 & 3	RELEASE OF RIGHTS	DOC #3282842
83	TDS TELECOM	3	RELEASE OF RIGHTS	NO EASEMENT OF RECORD
84	WISCONSIN POWER AND LIGHT	1, 2 & 3	RELEASE OF RIGHTS	PARCEL 1,2,3 - DOC #584928 PARCEL 1 - DOC #1165023
85	MADISON GAS AND ELECTRIC COMPANY	2	RELEASE OF RIGHTS	DOC #1135368

SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT.

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	R/W ACRES REQUIRED NEW	EXISTING	TOTAL	TLE ACRES
1	RONALD G. ENDRES	FEE/TLE	0.003	--	0.003	0.021
2	SIXMILE CREEK GOLF DEVELOPMENT GROUP, LLC	FEE/TLE	0.126	0.232	0.358	0.034
3	CLASSIC CUSTOM HOMES OF WAUNAKEE, INC.	FEE/TLE	0.022	0.106	0.128	0.021

PROJECT LOCATION (NOT TO SCALE)

KRISTI CHLEBOWSKI
DANE COUNTY
REGISTER OF DEEDS

DOCUMENT # 5716525
04/13/2021 10:25 AM
Trans Fee:
Exempt #:
Rec. Fee: 25.00
Pages:

The above recording information verifies that this document has been electronically recorded and returned to the submitter.

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 5280-03-20 - 4.01
SHEET 1 OF 2
AMENDMENT NO:___

GN
0 25 50
SCALE, FEET

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ENGINEERING | ARCHITECTURE | SURVEYING
FUNDING | PLANNING | ENVIRONMENTAL
1702 Pankratz St Madison, WI 53704
(608) 242-7779 www.msa-ps.com

I, BRADLEY L TISDALE, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION. I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: _____ DATE: 04/08/21
PRINT NAME: BRADLEY L TISDALE
REGISTRATION NUMBER: S-2824

WISCONSIN BRADLEY L. TISDALE S-2824 WAUNAKEE WI LAND SURVEYOR

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION

SIGNATURE: _____ DATE: 4/12/2021
PRINT NAME: CORY SCHLAGEL

FILE NAME : G:\00\00093\00093602\CADD\RW\5280-03-20_STH 113_WAUNAKEE_TPP.DWG
PLOT DATE : 4/9/2021 1:50 PM
PLOT BY : BRAD TISDALE
PLOT NAME :
PLOT SCALE : ##########
APPRAISAL PLAT DATE : 04/08/21

TRANSPORTATION PROJECT PLAT NO: 5280-03-20 - 4.02

PART OF LOT 1 OF CSM NO. 10221 RECORDED IN V.60, P.38-40 AS DOCUMENT NO. 3393332 LOCATED IN THE SW 1/4 OF THE SW 1/4 OF SECTION 20, T9N, R9E, TOWN OF VIENNA, DANE COUNTY, WISCONSIN.

RELOCATION ORDER: STH 113, MADISON-LODI, (SUNSET LANE TO CTH V), DANE COUNTY, WISCONSIN

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE: PREVIOUS PROJECT - DANE COUNTY 522-32, CSM #6041 AND CSM #10221.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN MADISON.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), DANE COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4"X24" IRON REBAR) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

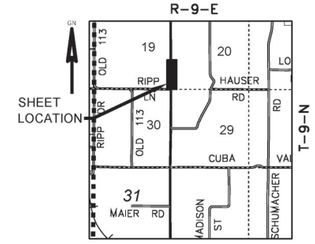
FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED IN THE OFFICE OF THE REGISTER OF DEEDS, IN DANE COUNTY, AS SHEET 2 OF 2 OF DOCUMENT 5716525.

RW POINT	STATION	OFFSET
300	162+86.01	0.00'
301	162+86.81	-49.28'
302	163+19.81	-49.62'
303	176+15.51	-47.01'
304	176+14.42	0.00'
305	176+14.36	2.92'
306	176+13.87	42.92'
307	170+48.23	41.52'
308	170+48.23	48.02'
309	170+18.23	47.95'
310	170+18.23	41.50'
311	162+85.96	40.72'

TLE POINT	STATION	OFFSET
T1	170+48.23	58.02'
T2	170+18.23	57.95'

LINE	BEARING	DISTANCE
300-301	N89°16'11"W	49.29'
301-302	N00°29'08"W	33.01'
303-304	S89°19'04"E	47.09'
304-305	S89°19'04"E	2.92'
305-306	S89°56'51"E	40.00'
307-308	N89°22'47"E	6.50'
308-309	S00°29'08"E	30.00'
309-310	S89°22'47"W	6.50'
311-SE 1/4 SEC 19	S89°54'12"W	40.00'
SE 1/4 SEC 19-300	N89°16'11"W	0.72'

PROJECT LOCATION (NOT TO SCALE)
R-9-E



KRISTI CHLEBOWSKI
DANE COUNTY
REGISTER OF DEEDS

DOCUMENT #
5743452
06/23/2021 01:13 PM
Trans Fee:
Exempt #:
Rec. Fee: 25.00
Pages:
The above recording information verifies that this document has been electronically recorded and returned to the submitter.

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 5280-03-20 - 4.02
AMENDMENT NO:___

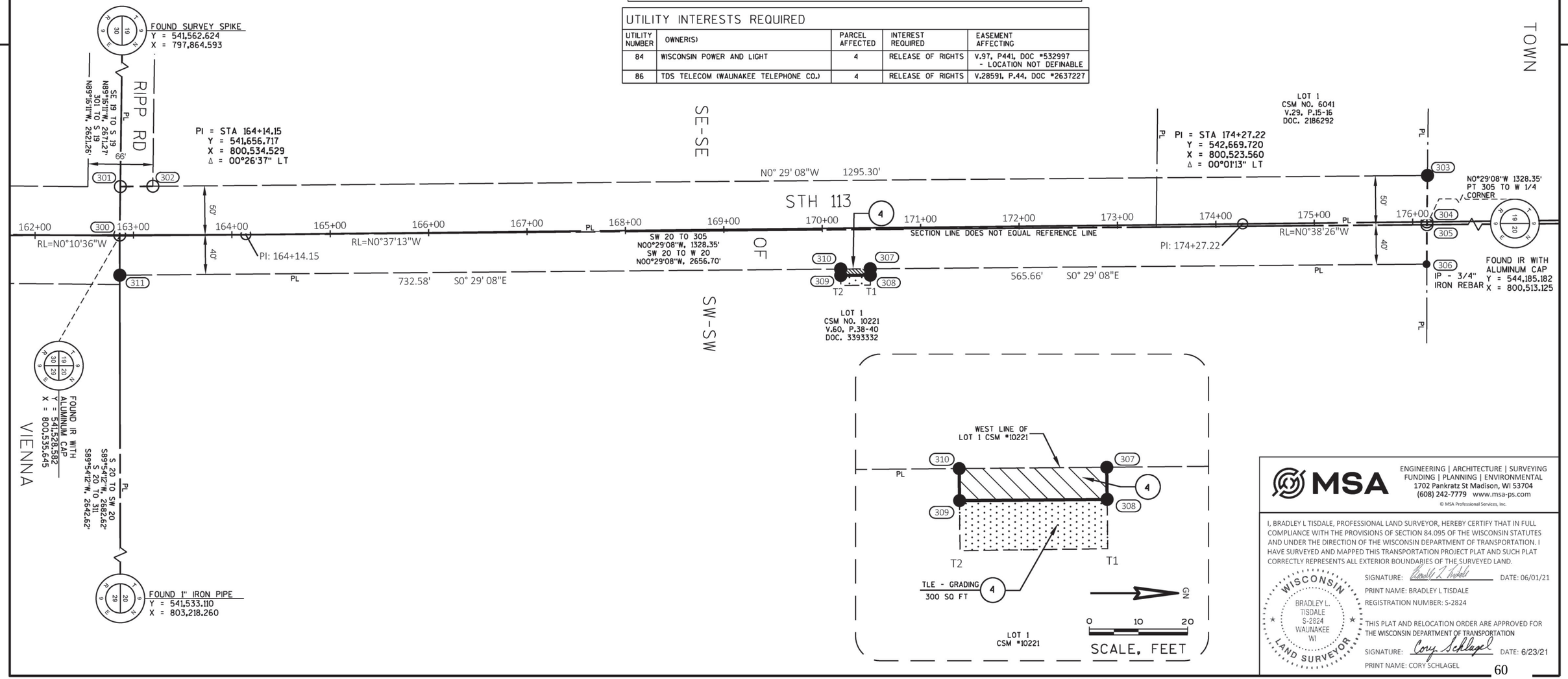
SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT.

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	R/W SQ FT REQUIRED			TLE SQ FT
			NEW	EXISTING	TOTAL	
4	RIPP LIVING TRUST AND TRICOR TRANSIT INC, AS LESSEE	FEE/TLE	195	--	195	300

UTILITY INTERESTS REQUIRED

UTILITY NUMBER	OWNER(S)	PARCEL AFFECTED	INTEREST REQUIRED	EASEMENT AFFECTING
84	WISCONSIN POWER AND LIGHT	4	RELEASE OF RIGHTS	V.97, P441, DOC #532997 - LOCATION NOT DEFINABLE
86	TDS TELECOM (WAUNAKEE TELEPHONE CO.)	4	RELEASE OF RIGHTS	V.28591, P.44, DOC #2637227



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1702 Pankratz St Madison, WI 53704
(608) 242-7779 www.msa-ps.com

I, BRADLEY L TISDALE, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION. I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

WISCONSIN BRADLEY L. TISDALE S-2824 WAUNAKEE WI LAND SURVEYOR

SIGNATURE: Bradley L Tisdale DATE: 06/01/21
PRINT NAME: BRADLEY L TISDALE
REGISTRATION NUMBER: S-2824

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION
SIGNATURE: Cory Schlagel DATE: 6/23/21
PRINT NAME: CORY SCHLAGEL

R/W PROJECT NUMBER: 5280-03-20	EXHIBIT NUMBER: 1

TLE ACQUISITION EXHIBIT
MADISON - LODI
(SUNSET LN - CTH V)

STH 113 — DANE COUNTY

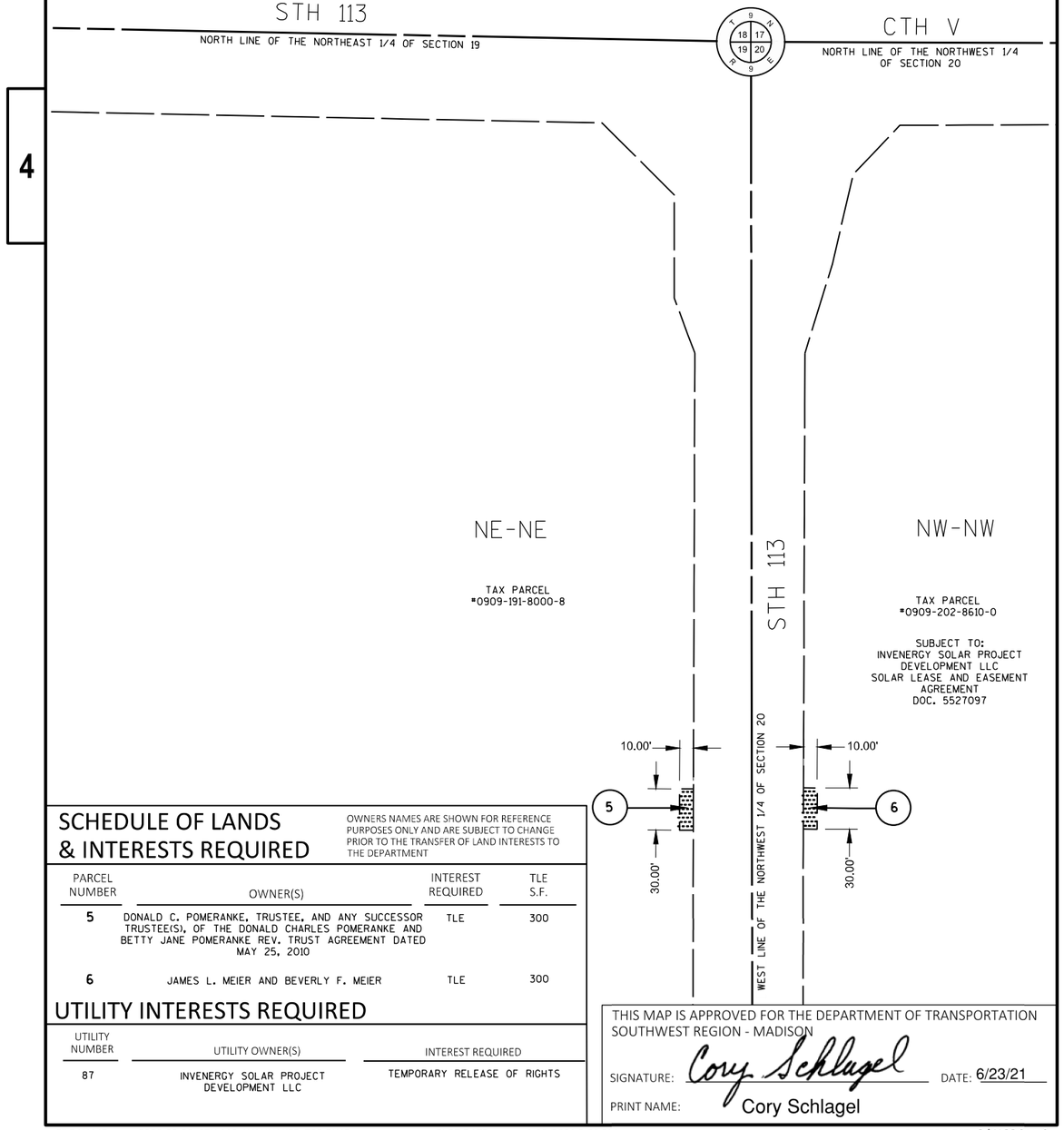
PART OF THE NE 1/4 OF THE NE 1/4 OF SECTION 19 AND PART OF THE NW 1/4 OF THE NW 1/4 OF SECTION 20, ALL LOCATED IN T9N, R9E, TOWN OF VIENNA, DANE COUNTY, WISCONSIN

GN

SCALE, FEET
0 50 100

NOTES:
THIS EXHIBIT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSES ONLY.
REFER TO THE CONVEYANCE DOCUMENT FOR PARCEL RELATED DETAILS.

THE PURPOSE OF TLEs IS FOR GRADING.



SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE S.F.
5	DONALD C. POMERANKE, TRUSTEE, AND ANY SUCCESSOR TRUSTEE(S), OF THE DONALD CHARLES POMERANKE AND BETTY JANE POMERANKE REV. TRUST AGREEMENT DATED MAY 25, 2010	TLE	300
6	JAMES L. MEIER AND BEVERLY F. MEIER	TLE	300

UTILITY INTERESTS REQUIRED

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
87	INVENERGY SOLAR PROJECT DEVELOPMENT LLC	TEMPORARY RELEASE OF RIGHTS

THIS MAP IS APPROVED FOR THE DEPARTMENT OF TRANSPORTATION
SOUTHWEST REGION - MADISON

SIGNATURE: Cory Schlagel DATE: 6/23/21

PRINT NAME: Cory Schlagel

FILE NAME : 5280-03-20_STH 113_WAUNAKEE_TPP.DWG PLOT DATE : 4/29/2021 3:51 PM R/W PROJECT: R/W PROJ NO

BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
900	15+16.79 LT	918.430	CP 900 FENO
901	19+58.58	921.206	CP 901 0.75" IRON ROD

N

SIXMILE CREEK

CONTROL POINT 900
X=800404.261
Y=526759.851

ATC

P.I. 15+00.00

SI

SEE BEAM GUARD
PLAN DETAIL SHEET

EXISTING R/W

TLE

VILLAGE OF DANE

SI

ATC

CAUTION

PROPOSED R/W

TLE

P.I. 19+00.00

ASPHALT CE
STA 19+56, 25' LT
SAWCUT REQ'D

CAUTION

PROPOSED R/W

RECONSTRUCT CROSSING
SEE RAILROAD CROSSING
PLAN DETAIL SHEET

BUNGELOW PAD
SEE RAILROAD CROSSING
PLAN DETAIL SHEET

EXISTING R/W

WETLANDS

MG&E

TDS TELECOM

SPRINT

14 15 16 17 18 19 20 21

STH 113 (N CENTURY AVE)

MG&E

MCI

SHARED-USE PATH

VILLAGE OF WAUNAKEE

BEGIN PROJECT
STA 15+00
MATCH EXISTING
SAW CUT REQ'D
Y = 800450.0766
X = 526743.2175

EXISTING R/W

SEE CONSTRUCTION DETAIL "STORM
SEWER MODIFICATIONS" FOR DETAILS

VILLAGE OF WAUNAKEE - SEWER

SALVAGE APRON ENDWALL FOR CULVERT PIPE 18-INCH
AND SALVAGE 16' OF CULVERT PIPE 18-INCH
STA 16+97.9, 28 FT

SI

SIXMILE CREEK

REMOVE EXISTING STRUCTURE B-13-945,
PROPOSED STRUCTURE B-13-871

SI

SEE BEAM GUARD
PLAN DETAIL SHEET

MG&E

VILLAGE OF WAUNAKEE - WATER

VILLAGE OF WAUNAKEE - ELECTRIC

CONTROL POINT 901
X=800484.916
Y=527201.677

TDS TELECOM

RECONSTRUCT CURB RAMPS
SEE CURB RAMP DETAIL SHEETS

SI

TDS TELECOM
SPECTRUM

SHARED-USE PATH

WETLANDS

VILLAGE OF WAUNAKEE - ELECTRIC

EXISTING R/W

14+00.00 15+00.00 16+00.00 17+00.00 18+00.00 19+00.00 20+00.00 21+00.00 21+50.00

935 930 925 920 915

VCL = 186.00
K = 164.02

STA 15+00.00
EL 919.76

VCL = 237.00
K = 100.92

VPC STA 20+09.50
VPC EL 924.13

VPI STA 20+32.00
VPI EL 924.77

VPT STA 20+54.50
VPT EL 924.77

STA 20+73.74
EL 924.77

STA 20+80.43
EL 924.77

VPC STA 21+04.00
VPC EL 924.77

VPI STA 21+14.00
VPI EL 924.77

VPT STA 21+24.00
VPT EL 924.65

0.00% 0.00% 0.00%

0.52%

VPC STA 15+00.00
VPC EL 919.76

VPI STA 15+93.00
VPI EL 919.19

VPC STA 17+72.50
VPC EL 920.12

VPI STA 18+91.00
VPI EL 920.73

VCL = 45.00
K = 15.71

VCL = 20.00
K = 16.12

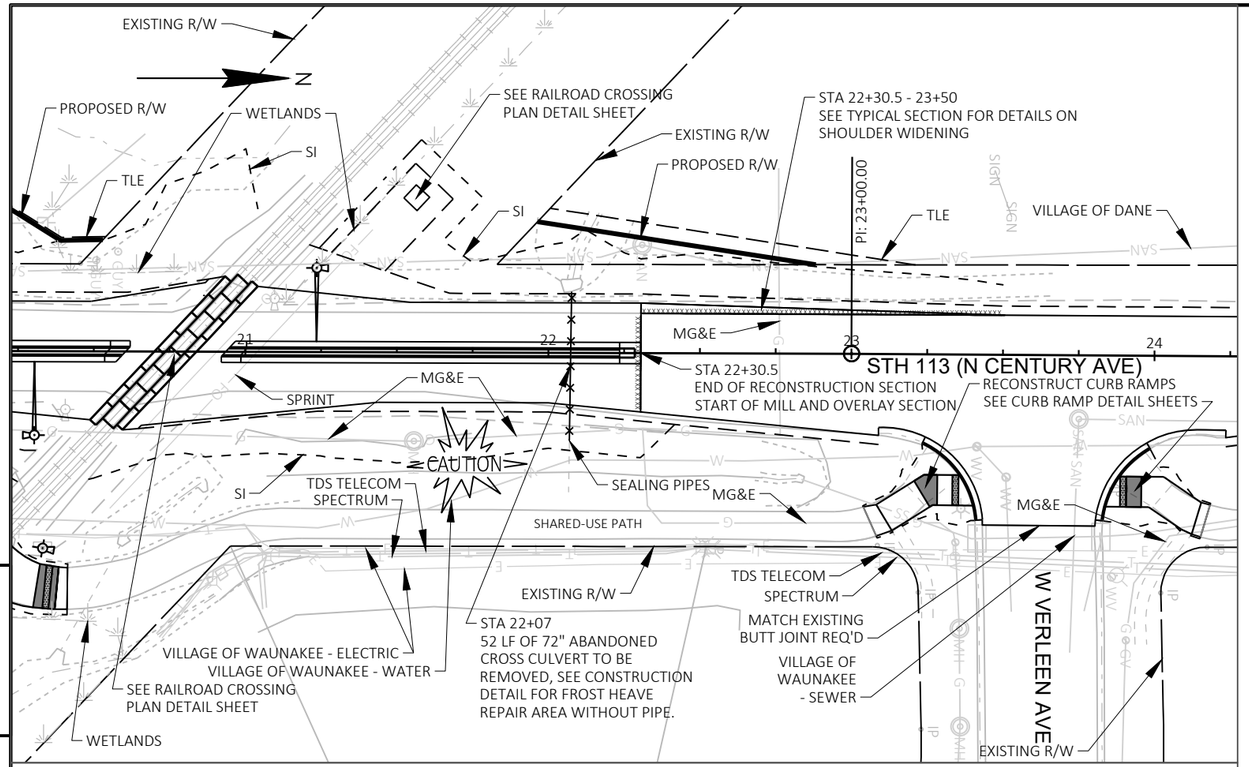
920.24 919.99 919.76 919.50 919.28 919.40 919.56 919.77 920.06 920.55 921.29 922.16 923.32 924.58 924.83 924.23

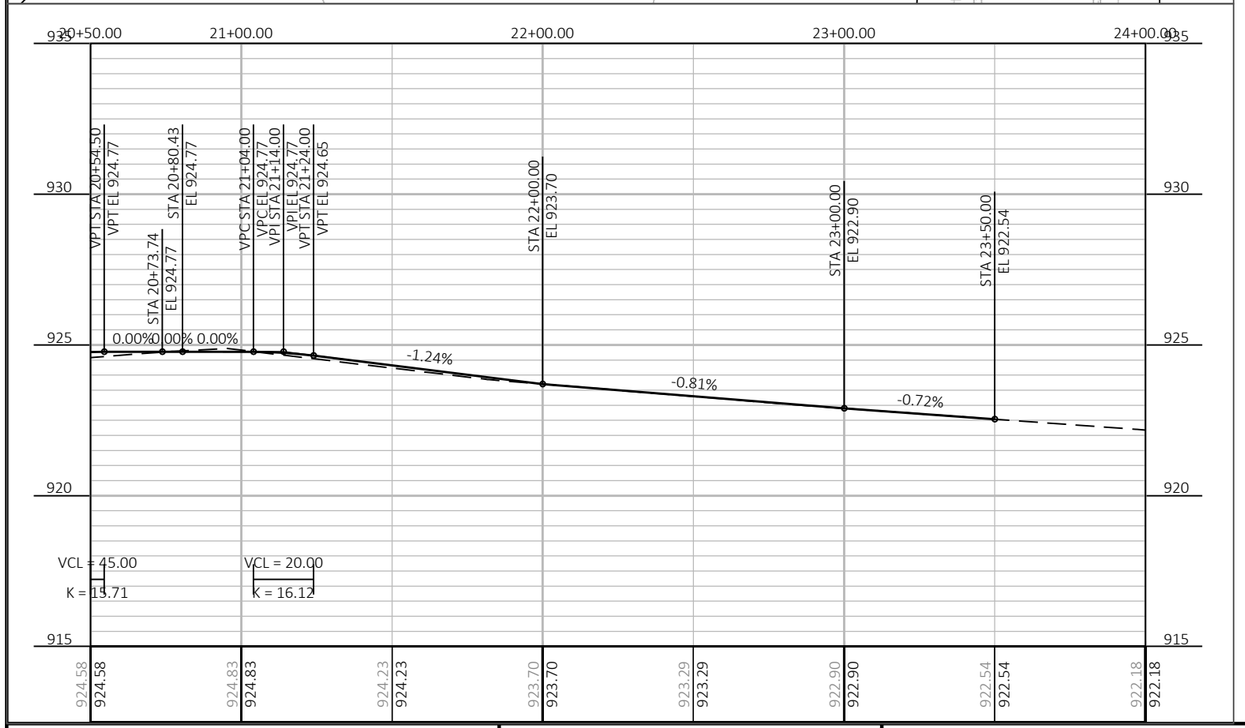
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN AND PROFILE: STH 113	SHEET 62 E

FILE NAME : \\FIWMAD7P3158\N3PUBLIC\PDS\C3D\52800300\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - 050101_pp

PLOT DATE : 11/14/2023 11:47 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : 1 IN:50 FT

WISDOT/CADDS SHEET 44





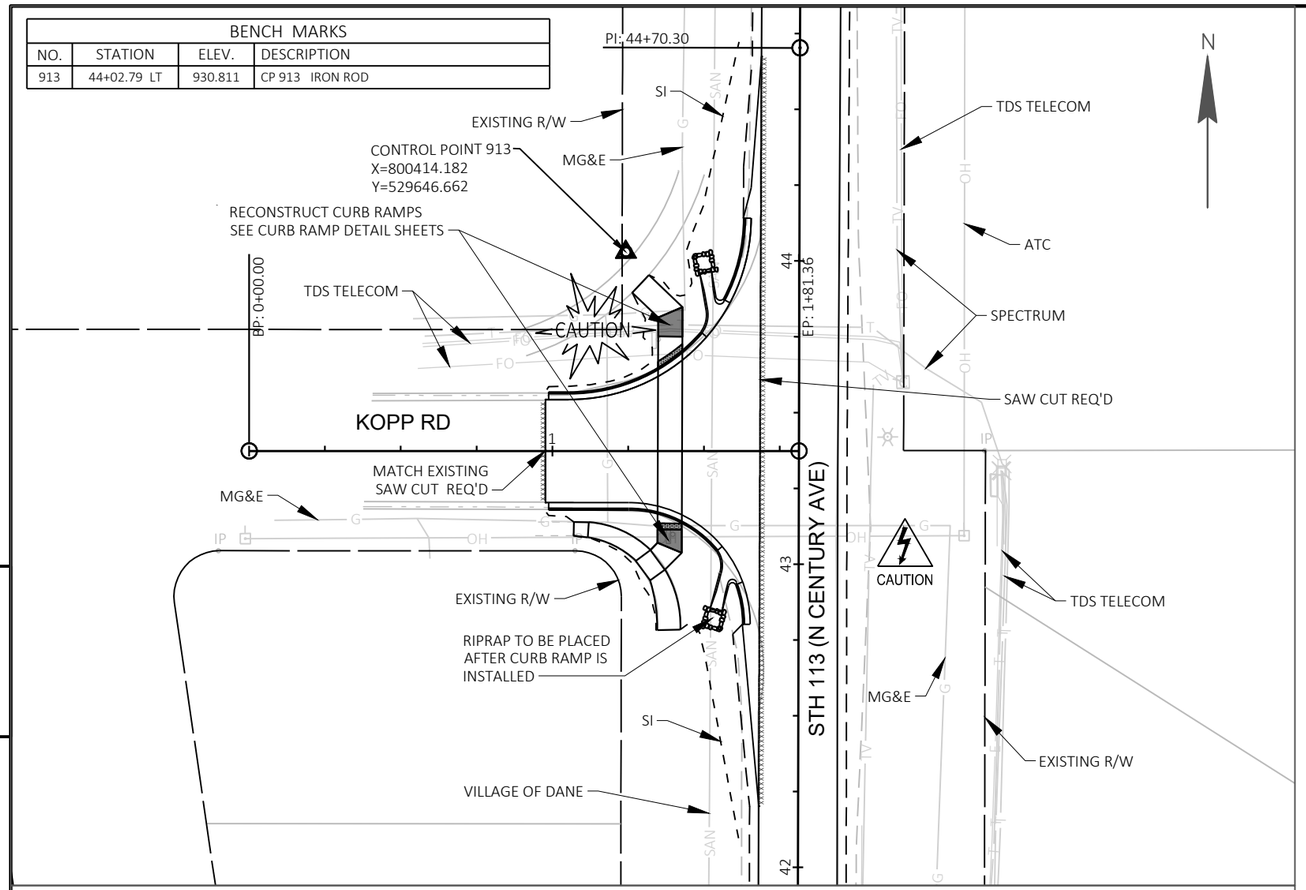
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN AND PROFILE: STH 113	SHEET 63	E

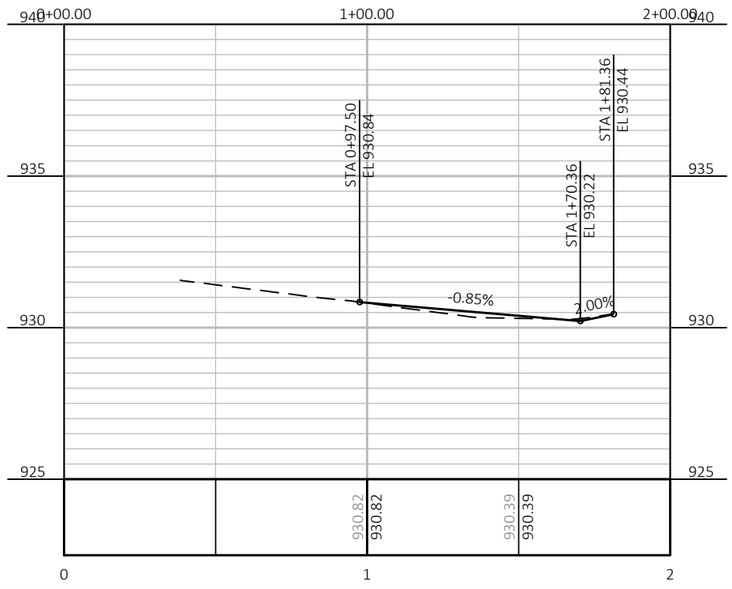
FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - 050101B_pp

PLOT DATE : 11/1/2022 7:04 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : Custom

WISDOT/CADDS SHEET 44

BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
913	44+02.79 LT	930.811	CP 913 IRON ROD





PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN AND PROFILE: KOPP RD	SHEET 64	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - 050101C_pp

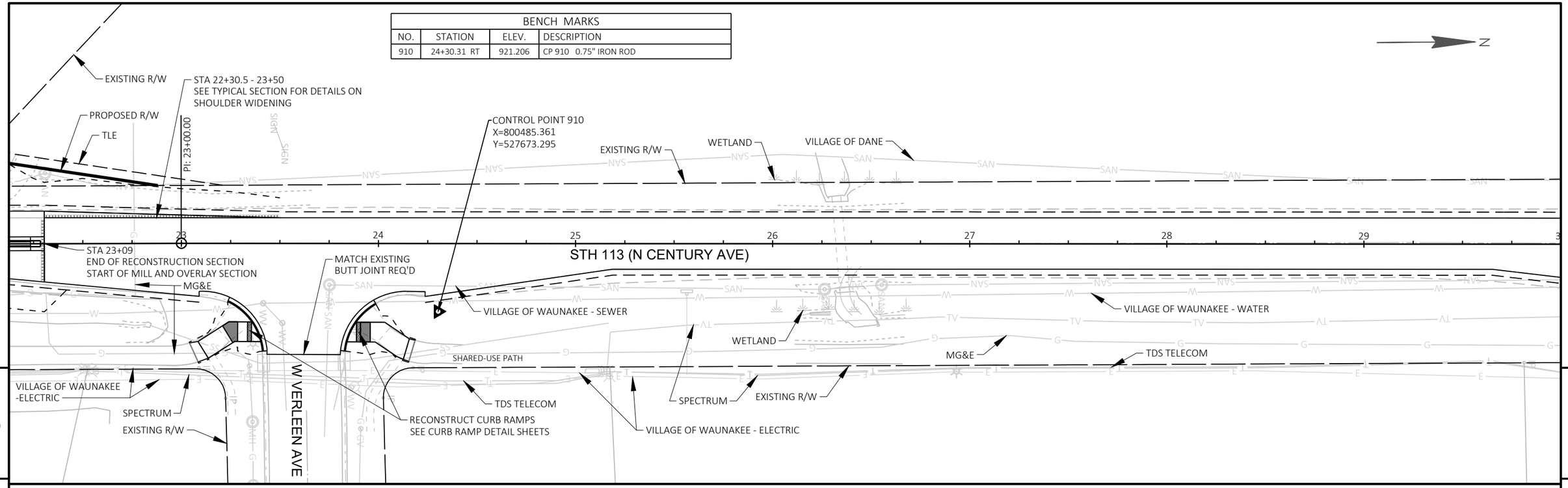
PLOT DATE : 11/1/2022 7:04 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : 1 IN:50 FT

WISDOT/CADDS SHEET 44

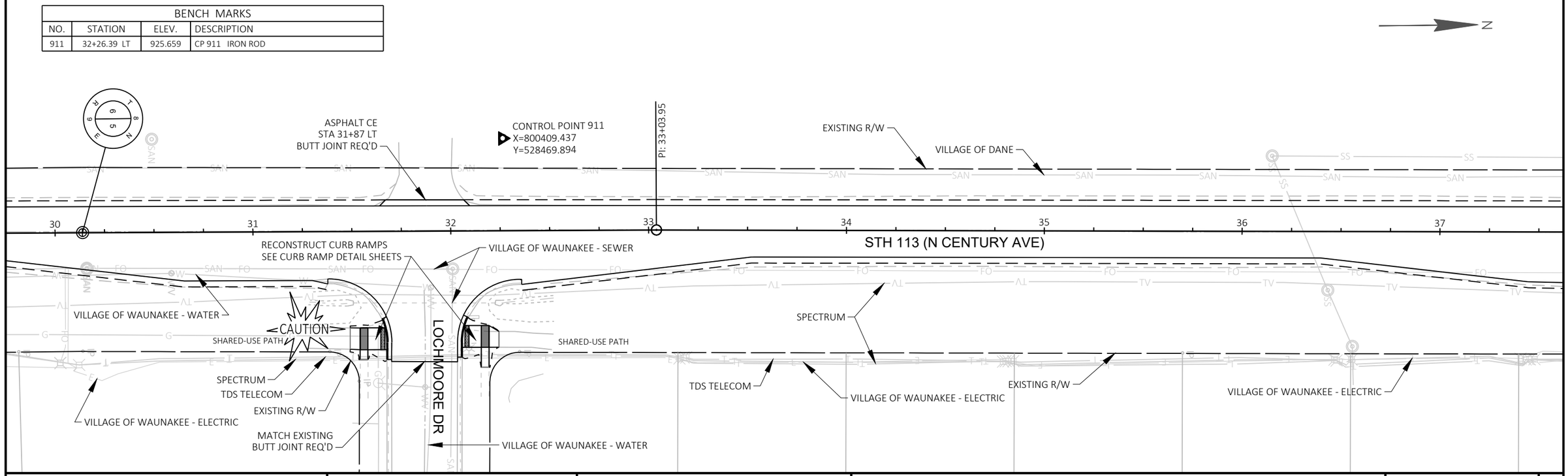
5

5

BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
910	24+30.31 RT	921.206	CP 910 0.75" IRON ROD

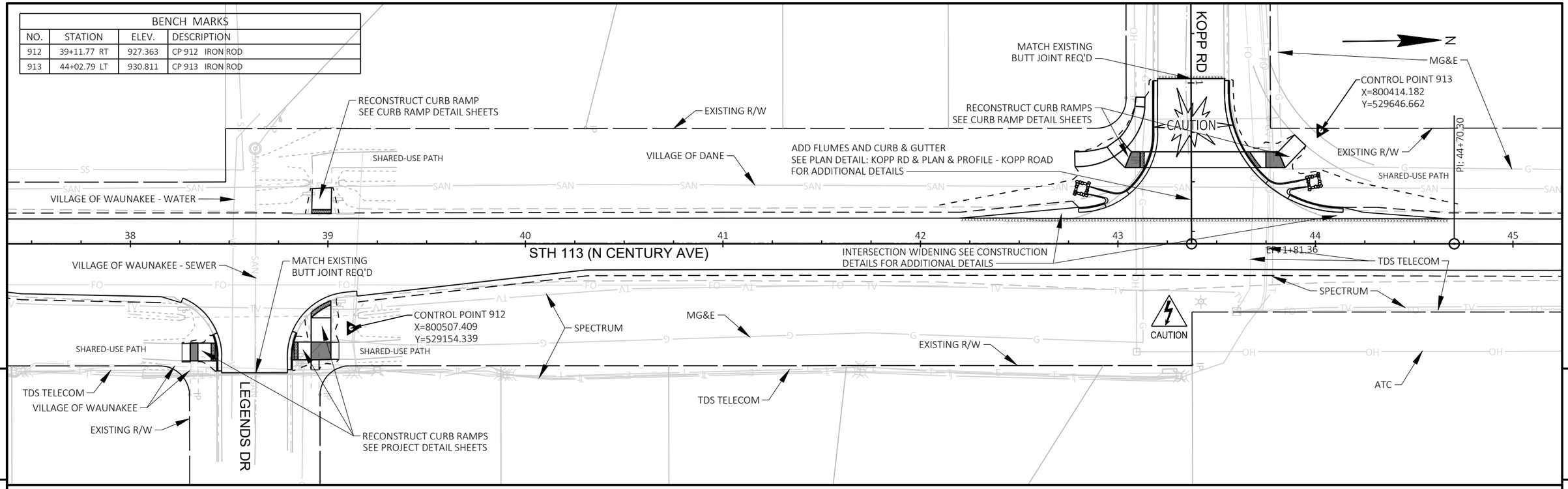


BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
911	32+26.39 LT	925.659	CP 911 IRON ROD



PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN SHEETS	SHEET 65	E

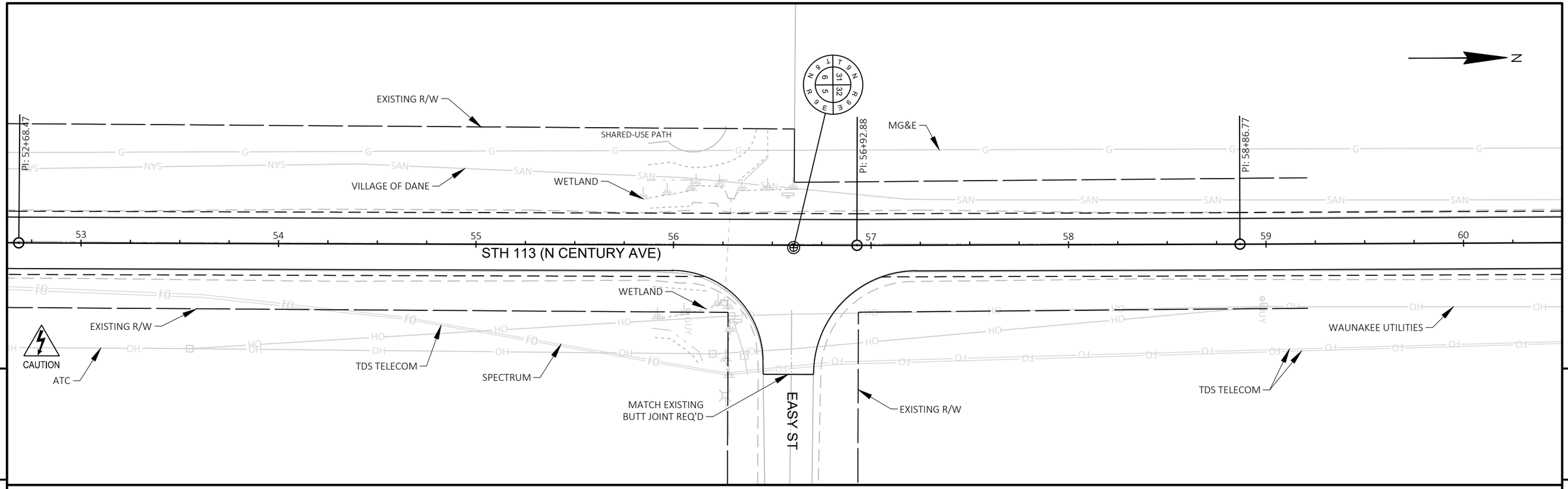
BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
912	39+11.77 RT	927.363	CP 912 IRON ROD
913	44+02.79 LT	930.811	CP 913 IRON ROD

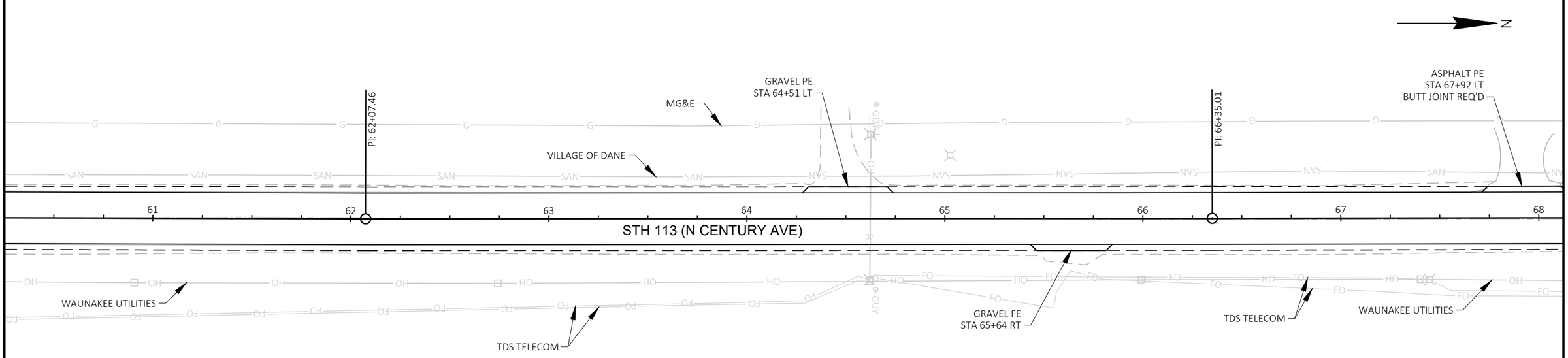


PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN SHEETS	SHEET 66	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - 050103_pp

PLOT DATE : 11/1/2022 7:04 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : 1 IN:50 FT



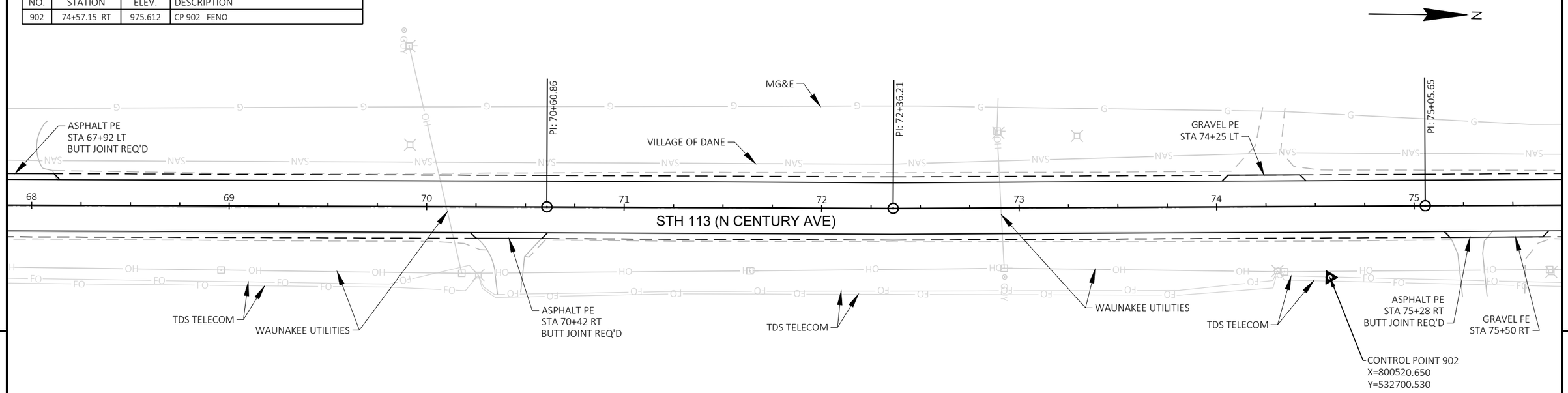


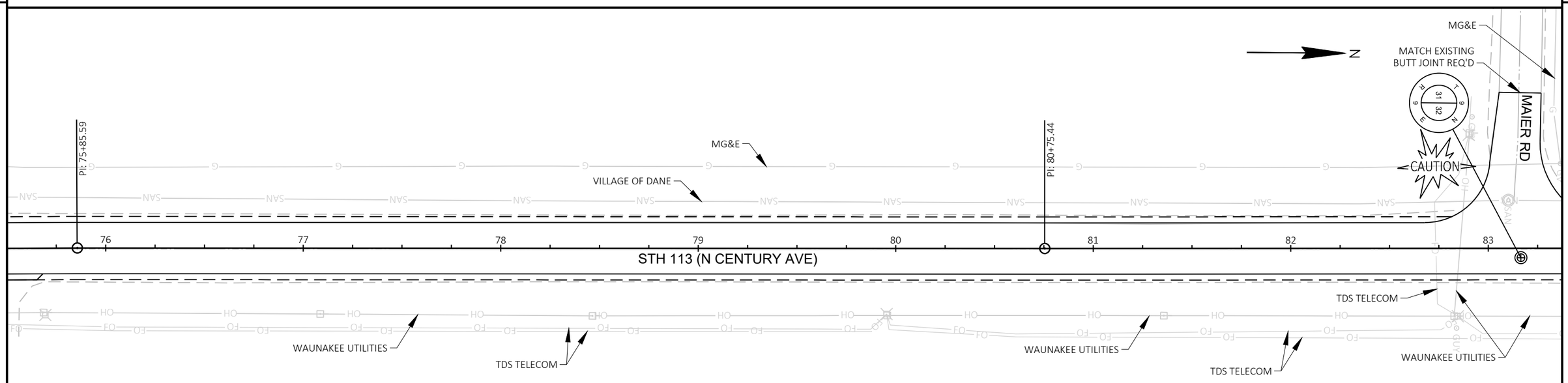
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN SHEETS	SHEET 67	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - 050104_pp

PLOT DATE : 11/1/2022 7:04 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : 1 IN:50 FT

BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
902	74+57.15 RT	975.612	CP 902 FENO

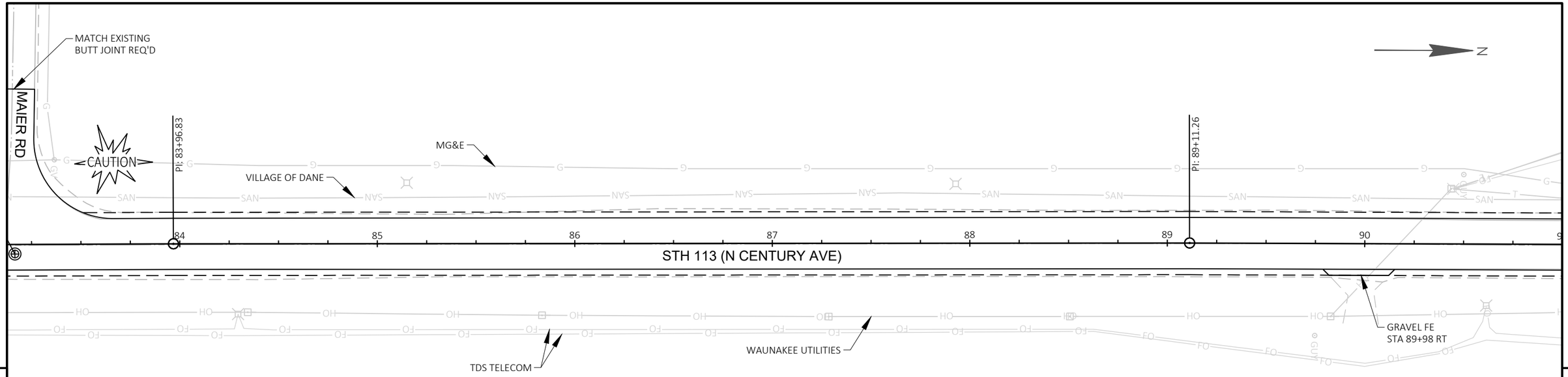




PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN SHEETS	SHEET 68	E

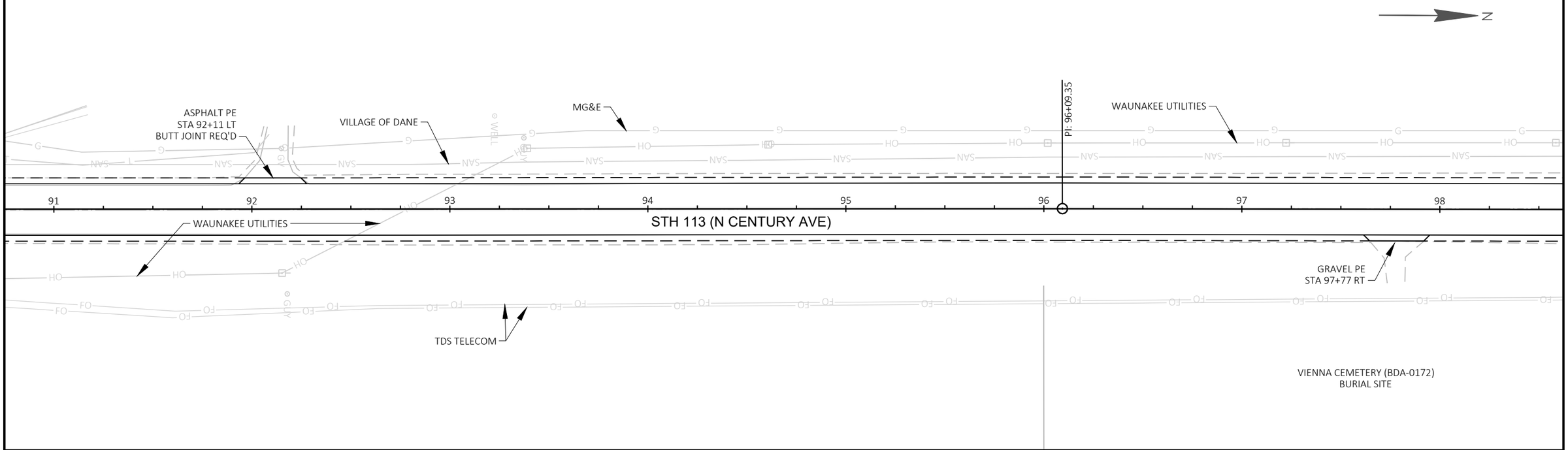
FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - 050105_pp

PLOT DATE : 11/1/2022 7:04 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : 1 IN:50 FT

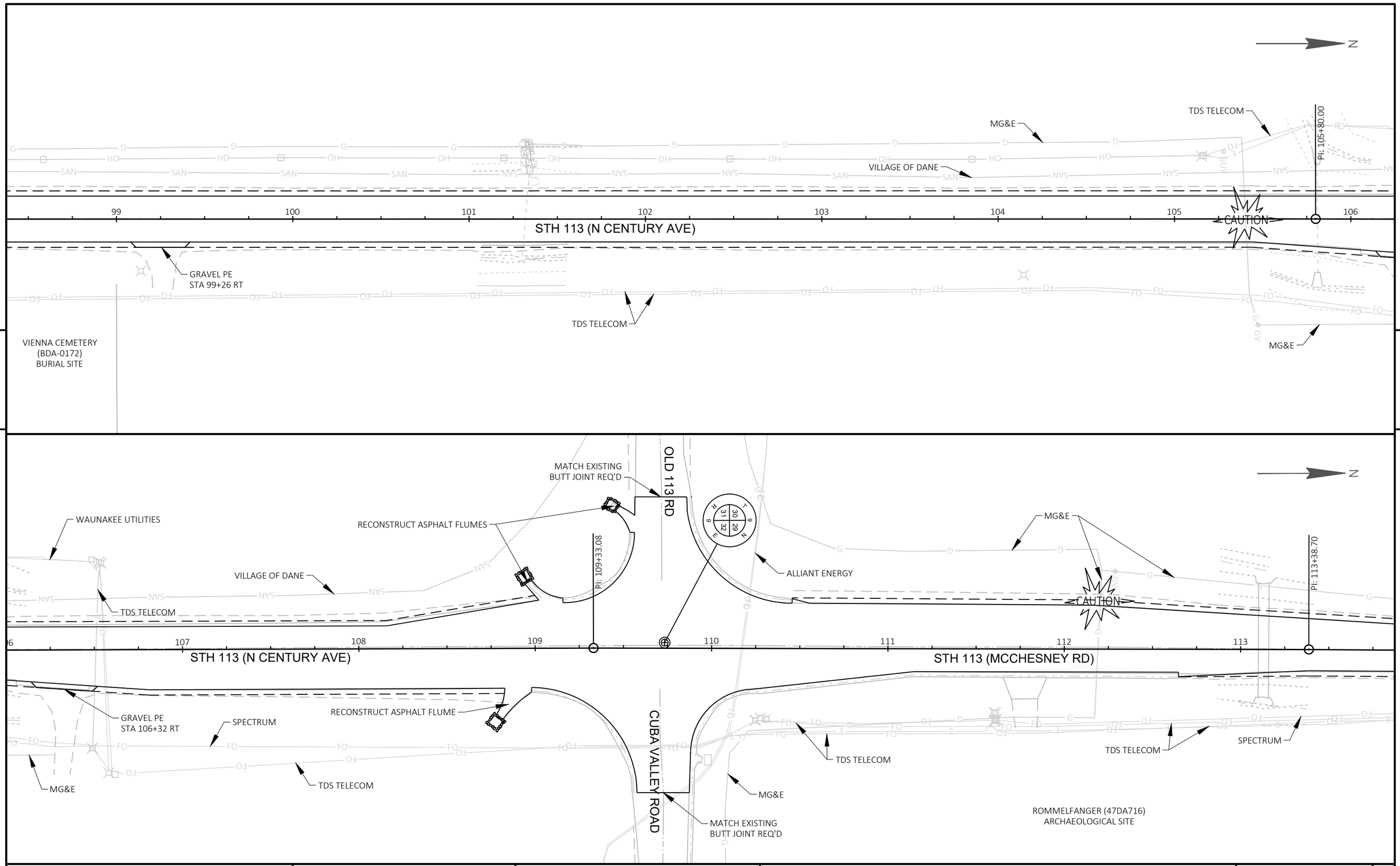


5

5



PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN SHEETS	SHEET 69	E

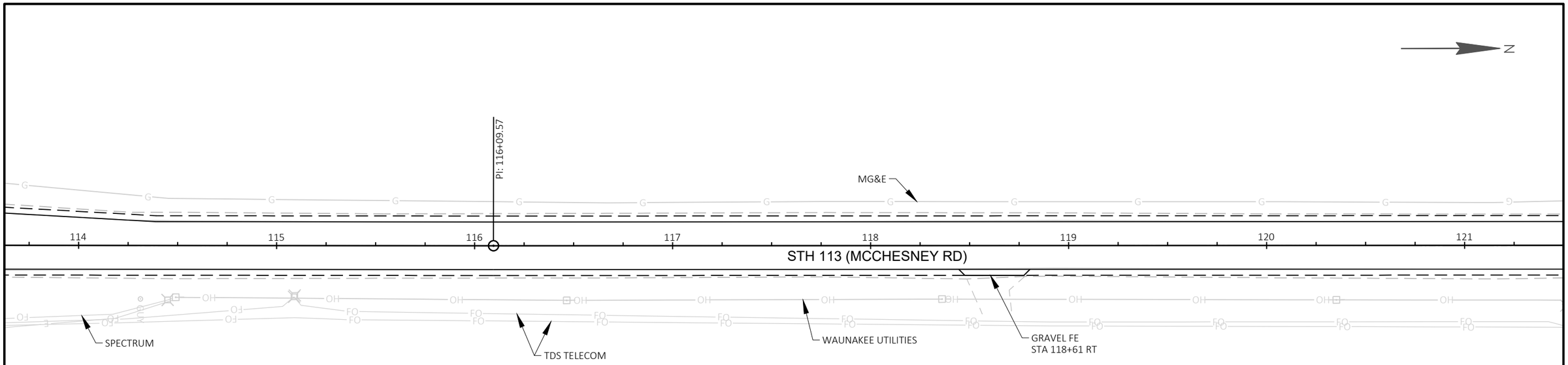


5 5

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN SHEETS	SHEET 70	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - 050107_pp

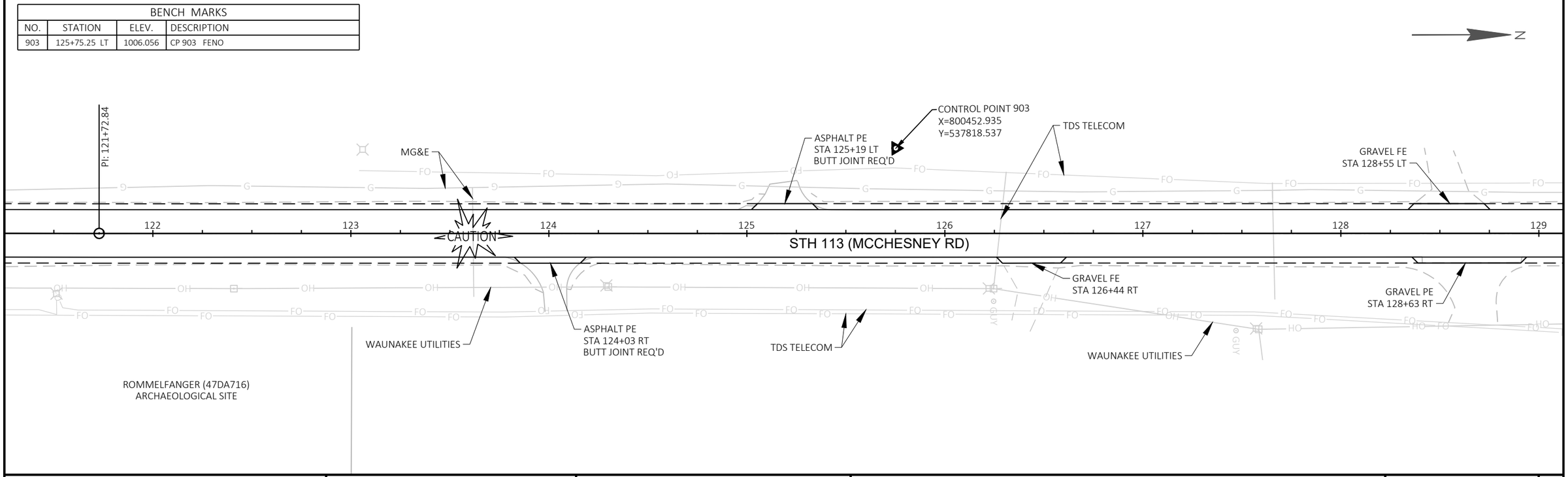
PLOT DATE : 11/1/2022 7:05 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : 1 IN:50 FT



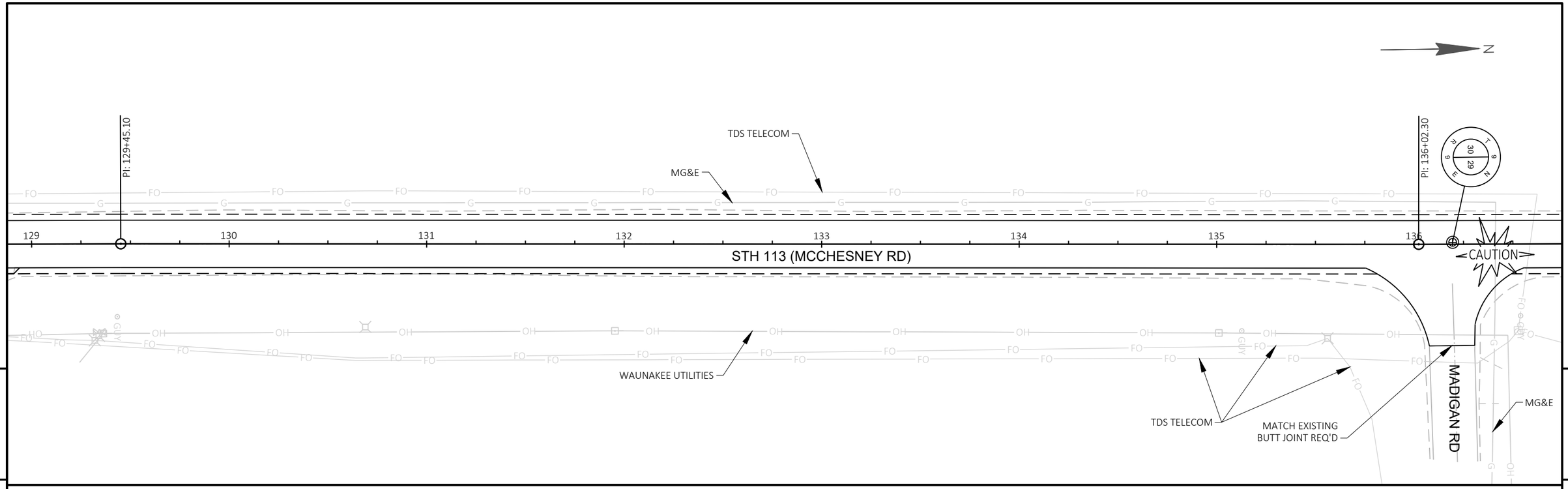
5

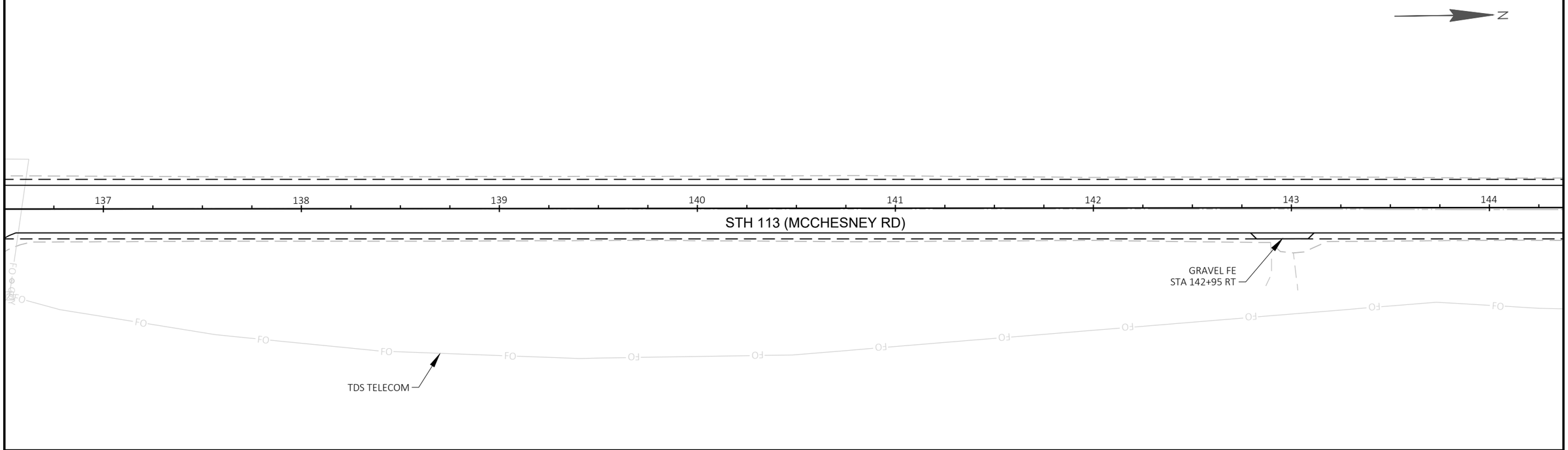
5

BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
903	125+75.25 LT	1006.056	CP 903 FENO

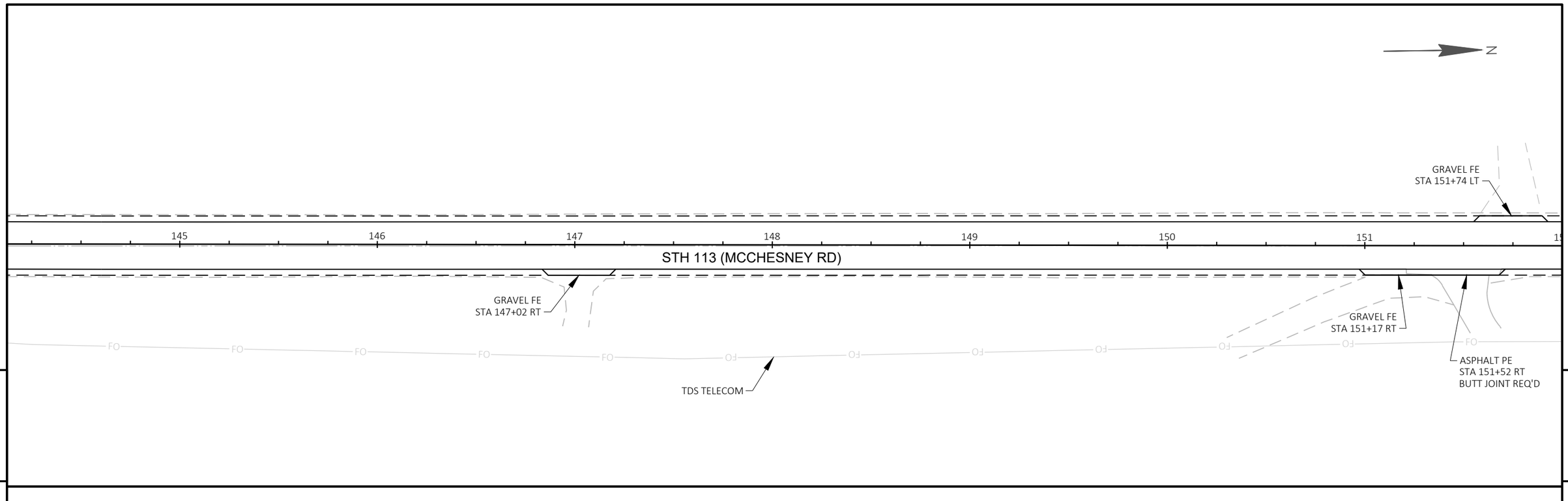


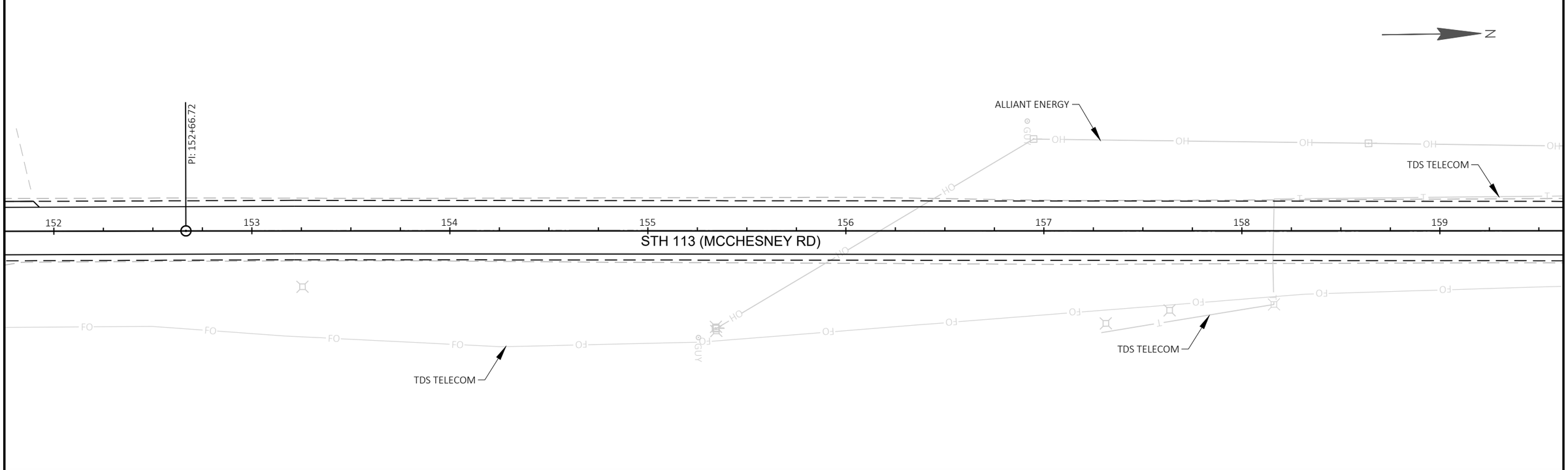
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN SHEETS	SHEET 71	E





PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN SHEETS	SHEET 72	E

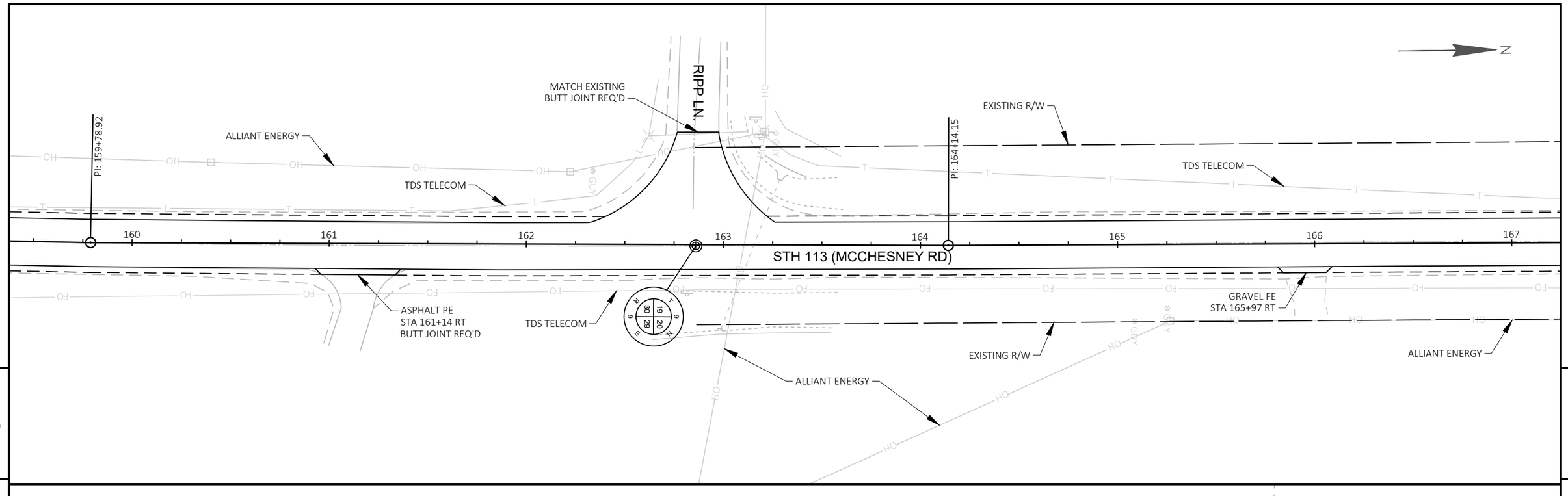


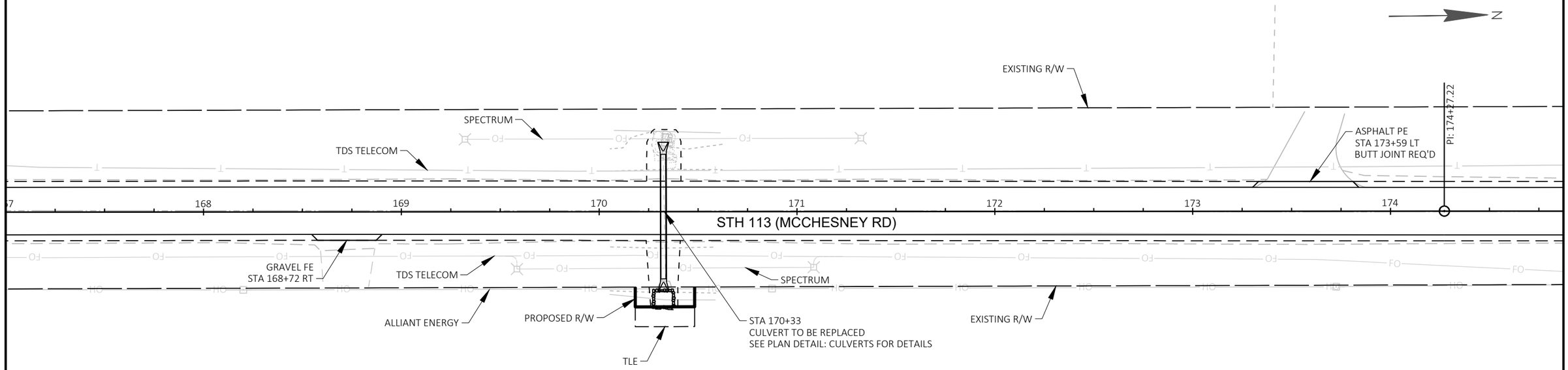


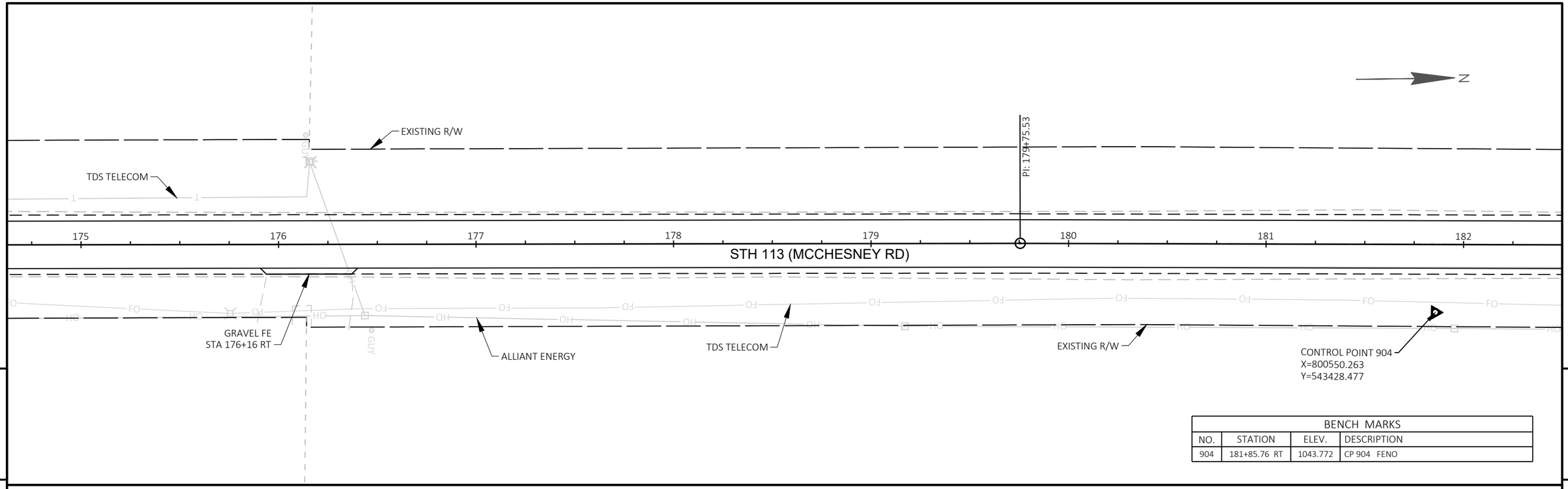
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN SHEETS	SHEET 73	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - 050110_pp

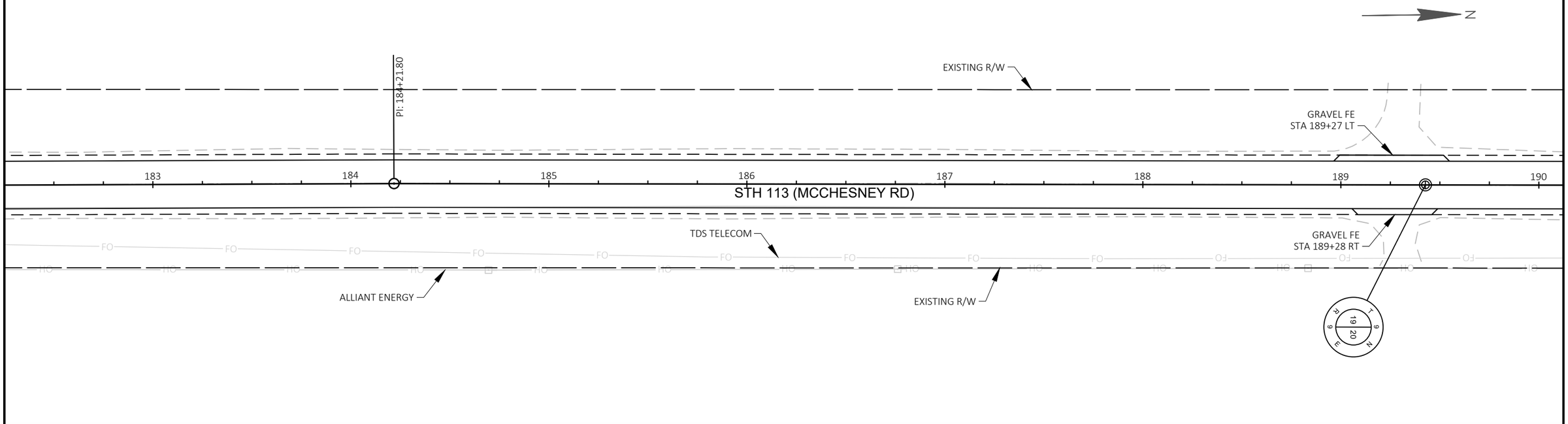
PLOT DATE : 11/1/2022 7:05 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : 1 IN:50 FT


N
MATCH EXISTING
BUTT JOINT REQ'D
RIPP LN
ALLIANT ENERGY
TDS TELECOM
PI: 159+78.92
EXISTING R/W
PI: 164+14.15
TDS TELECOM
160
161
162
163
164
165
166
167
STH 113 (MCCHESNEY RD)
ASPHALT PE
STA 161+14 RT
BUTT JOINT REQ'D
TDS TELECOM
GRAVEL FE
STA 165+97 RT
EXISTING R/W
ALLIANT ENERGY
ALLIANT ENERGY
5
5


N
EXISTING R/W
SPECTRUM
TDS TELECOM
ASPHALT PE
STA 173+59 LT
BUTT JOINT REQ'D
PI: 174+27.22
168
169
170
171
172
173
174
STH 113 (MCCHESNEY RD)
GRAVEL FE
STA 168+72 RT
TDS TELECOM
SPECTRUM
ALLIANT ENERGY
PROPOSED R/W
STA 170+33
CULVERT TO BE REPLACED
SEE PLAN DETAIL: CULVERTS FOR DETAILS
EXISTING R/W
TLE



BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
904	181+85.76 RT	1043.772	CP 904 FENO

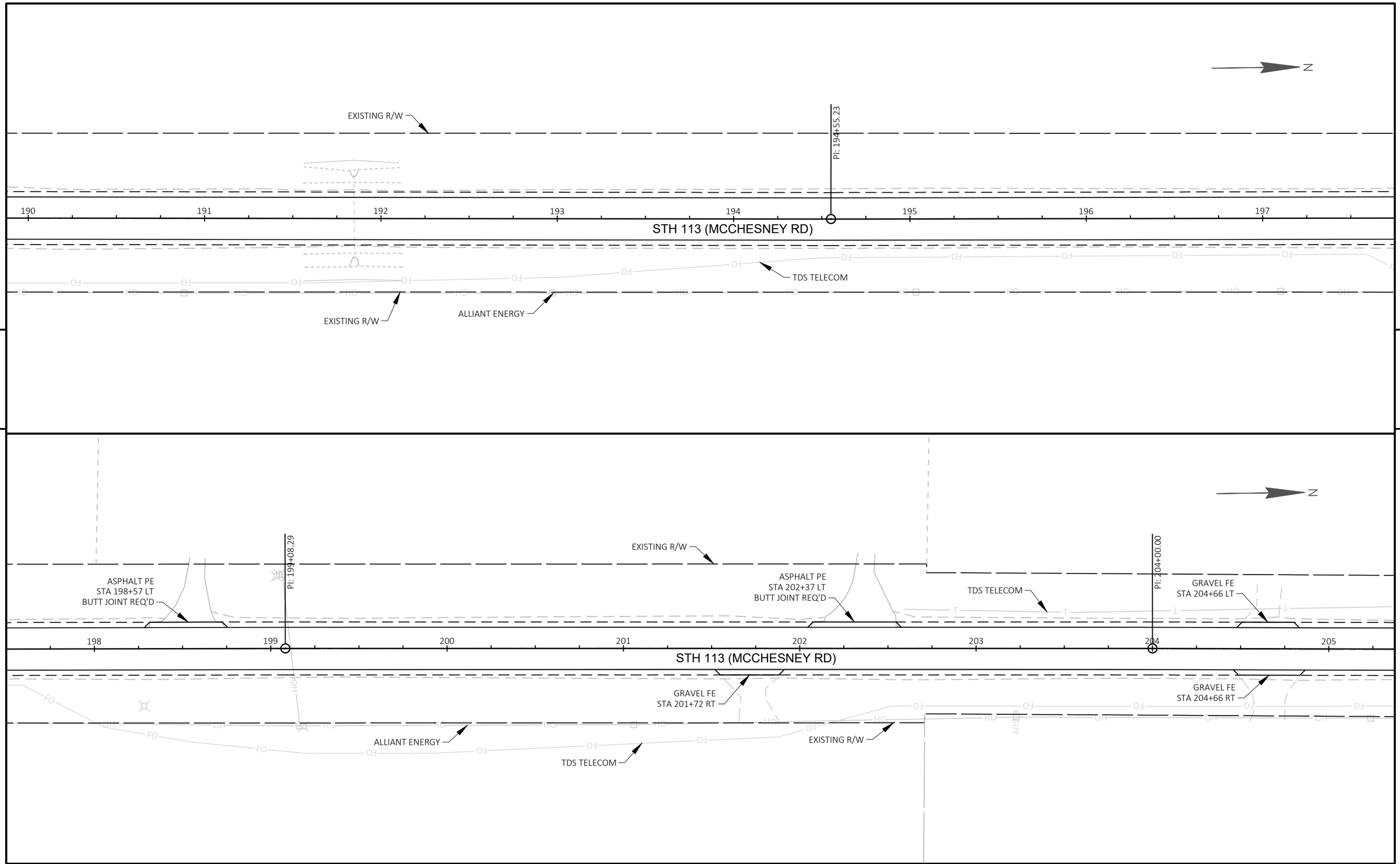


PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN SHEETS	SHEET 75	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - 050112_pp

PLOT DATE : 11/1/2022 7:05 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : 1 IN:50 FT

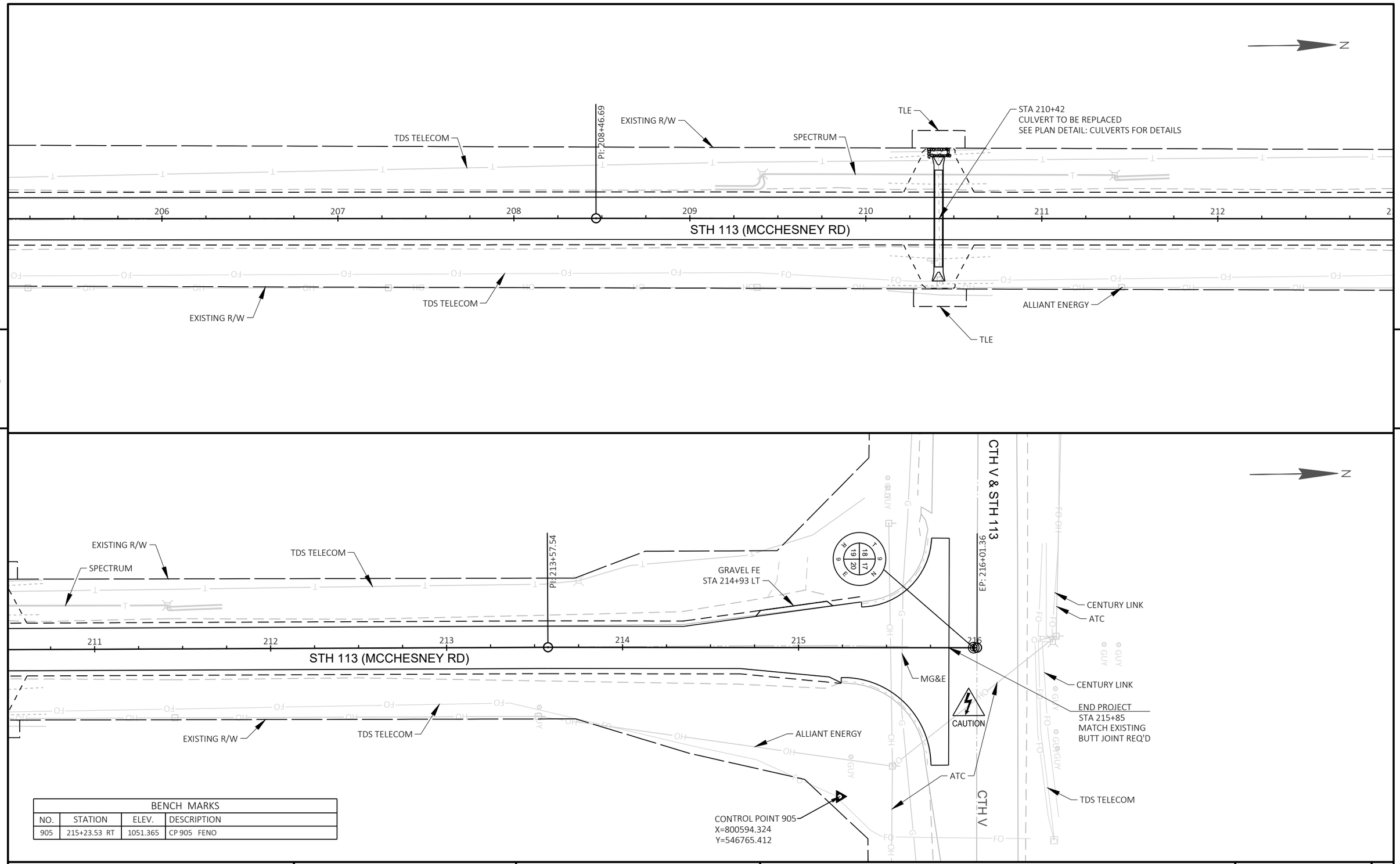
WISDOT/CADDS SHEET 44



PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN SHEETS	SHEET 76	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - 050113_pp

PLOT DATE : 11/1/2022 7:05 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : 1 IN:50 FT



BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
905	215+23.53 RT	1051.365	CP 905 FENO

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	PLAN SHEETS	SHEET 77 E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - 050114_pp

PLOT DATE : 11/1/2022 7:06 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : PLOT SCALE : 1 IN:50 FT

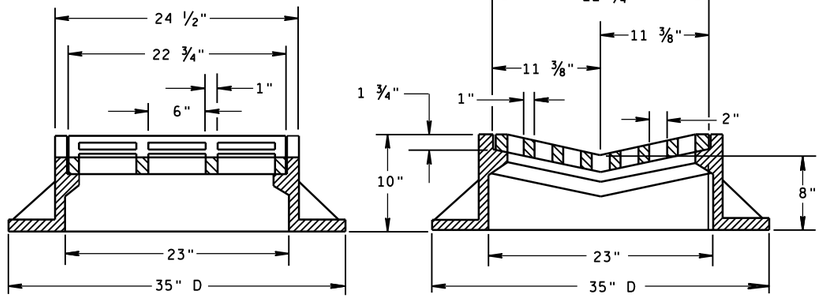
Standard Detail Drawing List

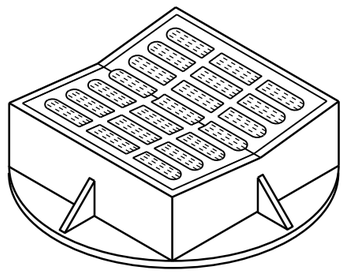
08A05-19B	INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM
08A05-19D	INLET COVER TYPE BW, MANHOLE COVERS, TYPE K, J, J-S, L & M
08B09-03	MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT, 8-FT, 9-FT, 10-FT DIAMETER
08D01-23A	CONCRETE CURB & GUTTER
08D01-23B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D02-08A	CONCRETE SURFACE DRAINS FLUME TYPE AT STRUCTURES
08D02-08B	CONCRETE SURFACE DRAINS FLUME TYPE AT STRUCTURES
08D02-08C	CONCRETE SURFACE DRAINS FLUME TYPE AT STRUCTURES
08D05-20A	CURB RAMPS TYPES 1 AND 1-A
08D05-20B	CURB RAMPS TYPES 2 AND 3
08D05-20C	CURB RAMPS TYPES 4A AND 4A1
08D05-20D	CURB RAMPS TYPE 4B AND 4B1
08D05-20E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-20F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-20G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D22-01	DRIVEWAYS WITHOUT CURB & GUTTER RESURFACING PROJECTS RURAL
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E11-02	TURBIDITY BARRIER
08E15-01	CULVERT PIPE CHECK
08F01-11	APRON ENDWALLS FOR CULVERT PIPE
08F02-01	APRON ENDWALLS FOR PIPE ARCH AND ELLIPTICAL PIPE
08F04-08	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
09A01-14A	AT-GRADE SIDE ROAD INTERSECTION, TYPES "B1", "B2", "C" AND D AND TEE INTERSECTION BYPASS LANE
09A01-14B	AT-GRADE SIDE ROAD INTERSECTION, TYPE "A1" & "A2"
11B02-02	CONCRETE MEDIAN NOSE
12A03-10	NAME PLATE (STRUCTURES)
13B01-11A	PAVEMENT DETAILS FOR RAILROAD APPROACH
13B02-09A	CONCRETE PAVEMENT APPROACH SLAB
13B02-09B	STRUCTURAL APPROACH SLAB AND CONCRETE PAVEMENT APPROACH SLAB
13C01-19	CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES
13C18-07A	CONCRETE PAVEMENT JOINTING
13C18-07B	CONCRETE PAVEMENT STEEL REINFORCEMENT
13C18-07C	CONCRETE PAVEMENT JOINT TYPES
13C18-07D	CONCRETE PAVEMENT JOINT TYPES AT UTILITY FIXTURES
13C18-07F	CONCRETE PAVEMENT INTERSECTION BOXOUT FOR INTEGRAL CURB AND GUTTER
13C19-03	HMA LONGITUDINAL JOINTS
14B29-01	SAFETY EDGE
14B42-07A	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07B	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07C	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07D	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B44-04A	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-04B	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-04C	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B45-05A	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05B	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05C	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05D	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-09C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C08-23B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C08-23C	PAVEMENT MARKING (TURN LANES)
15C08-23D	PAVEMENT MARKING (TURN LANES)
15C09-13A	SIGNING AND PAVEMENT MARKING DETAILS FOR RAILROAD-HIGHWAY GRADE CROSSINGS
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C12-09B	TRAFFIC CONTROL, LANE CLOSURE WITH AUTOMATED FLAGGER ASSISTANCE DEVICE
15C18-08A	MEDIAN ISLAND MARKING PAVEMENT MARKINGS
15C18-08B	MEDIAN ISLAND MARKING MEDIAN ISLAND NOSE
15C18-08C	MEDIAN PAVEMENT MARKINGS DOUBLE ARROW WARNING SIGN PLACEMENT

6

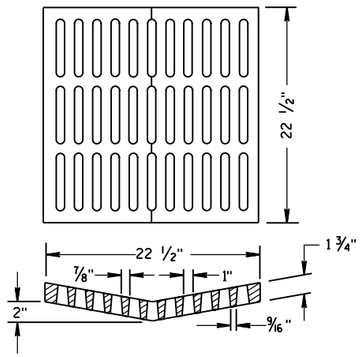
Standard Detail Drawing List

15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D30-09A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09B	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09G	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D44-02	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES



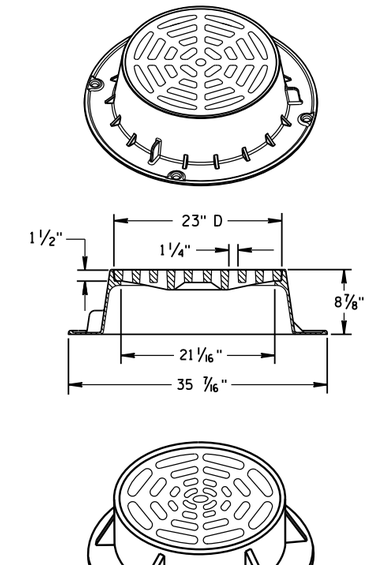


TYPE "B"



ALTERNATIVE GRATE FOR TYPE "B" COVER

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS POSSIBLE.
NOTED AS TYPE B-A ON THE DRAINAGE TABLE



TYPE "C"

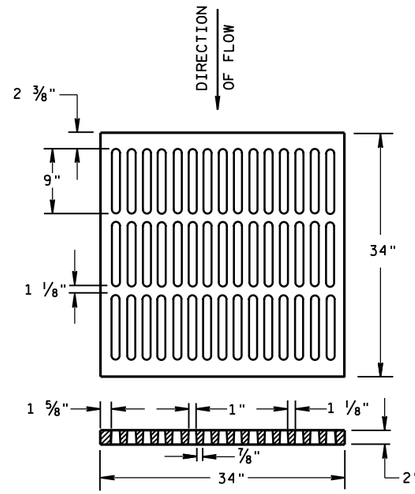
NOTE: EITHER CASTING IS ACCEPTABLE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

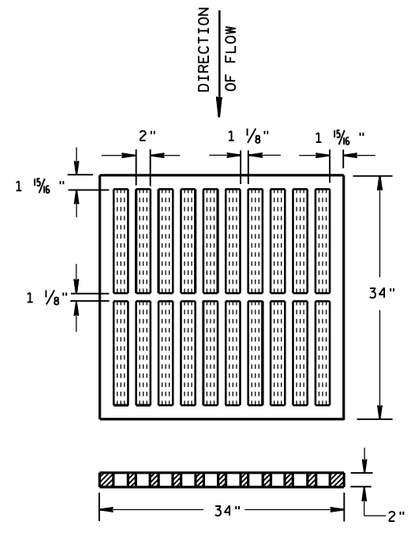
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



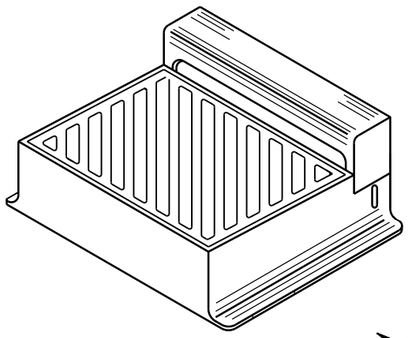
ALTERNATIVE TYPE "MS"

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS PERMITTED
NOTED AS TYPE MS-A ON THE DRAINAGE TABLE

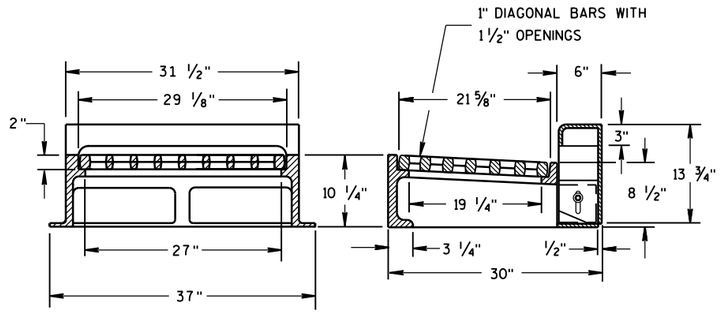


TYPE "MS"

USE ON FREEWAYS AND EXPRESSWAYS
NOTED AS TYPE MS ON DRAINAGE TABLE



DIAGONAL SLOTS, SHALL BE ORIENTED TO THE DIRECTION OF FLOW AS ILLUSTRATED. GRATES ARE MANUFACTURED TO BE REVERSIBLE.



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

TYPE "WM"

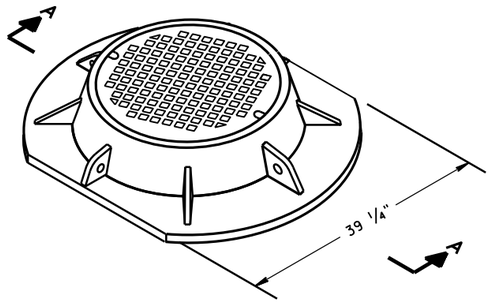
INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM

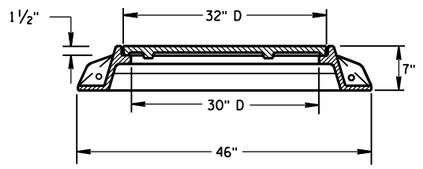
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/27/2013 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

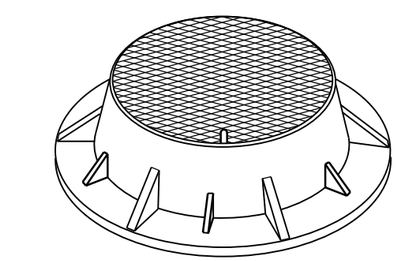
6

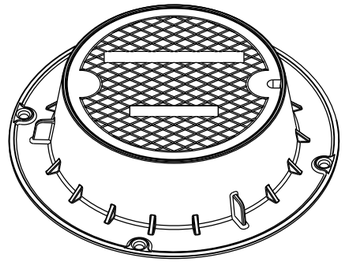
S.D.D. 8 A 5-19b

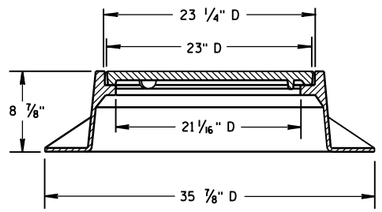


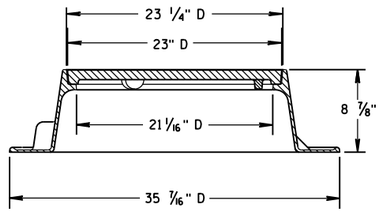


SECTION A-A
TYPE "K"









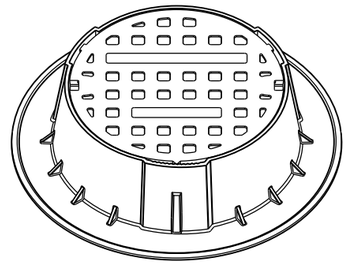
TYPE "J"
NOTE: EITHER CASTING IS ACCEPTABLE

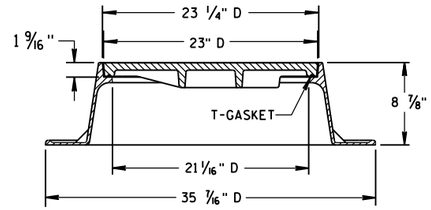
GENERAL NOTES

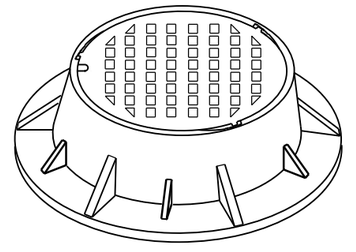
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

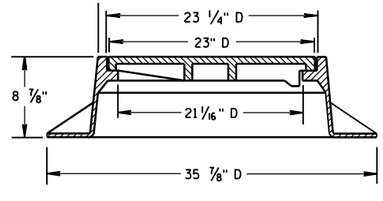
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR MANHOLE COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

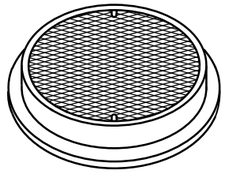


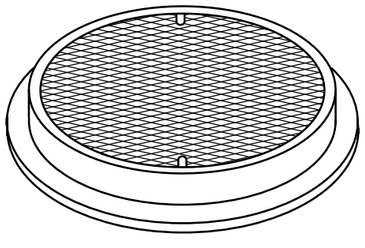


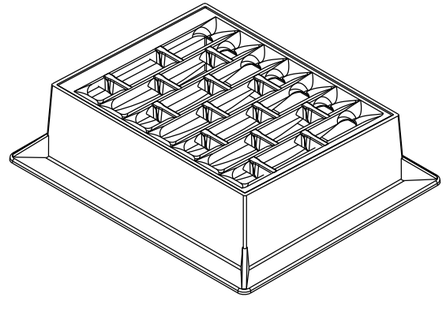


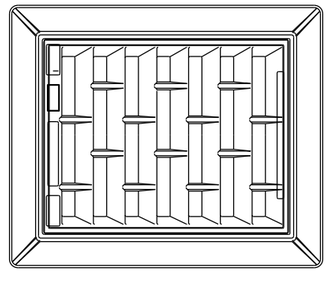


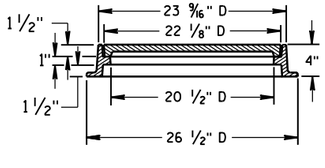
TYPE "J" SPECIAL
TYPE "B" NON-ROCKING SELF-SEAL LID
(NOTED AS TYPE J-S ON THE DRAINAGE TABLE)
NOTE: EITHER CASTING IS ACCEPTABLE



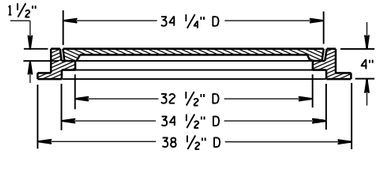




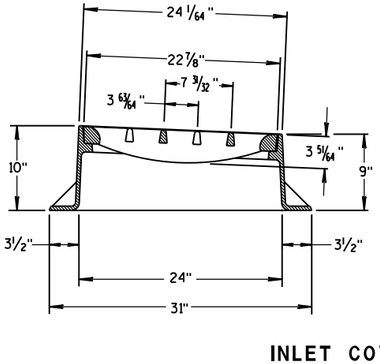


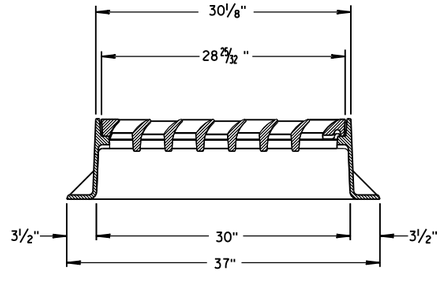


TYPE "L"



TYPE "M"





INLET COVER TYPE "BW"

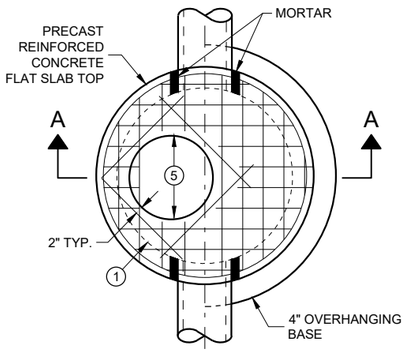
INLET COVER TYPE BW
MANHOLE COVERS, TYPE K, J, J-S, L & M

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

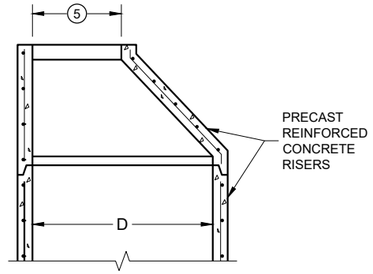
APPROVED
11/27/2013
DATE
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

6

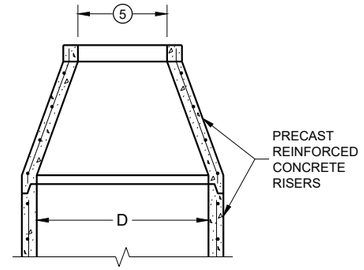
S.D.D. 8 A 5-19d



PLAN VIEW
CIRCULAR OPENING



OPTIONAL PRECAST REINFORCED CONCRETE ECCENTRIC TOP



OPTIONAL PRECAST REINFORCED CONCRETE CONCENTRIC TOP

MANHOLE COVER OPENING MATRIX

OPENING SIZE (FT.) \ MANHOLE COVER TYPE	C	ALL J'S	K	L	M
2 DIA.	X	X		X	
3 DIA.			X		X

PIPE MATRIX

MANHOLE SIZE (DIA.)	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES		MINIMUM WALL THICKNESS (IN)	MINIMUM PRECAST FLAT SLAB TOP AND BASE THICKNESS
	180° SEPARATION (IN)	90° SEPARATION (IN)		
3-FT	15	12	4	6
4-FT	24	18	4	6
5-FT	36	24	5	8
6-FT	42	36	6	8
7-FT	48	36/42*	7	8
8-FT	60	42	8	8
9-FT	66	54	9	10
10-FT	72	60	10	10

*A 36" PIPE AND A 42" PIPE CAN BE PLACED WITHIN 90 DEGREES. SEE MINIMUM HORIZONTAL PIPE SEPARATION DETAIL.

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST MANHOLE UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONCRETE CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED CONCRETE FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES. CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH: 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF ½ INCH AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST MANHOLE UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

CONCRETE BLOCK WILL NOT BE PERMITTED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "D".

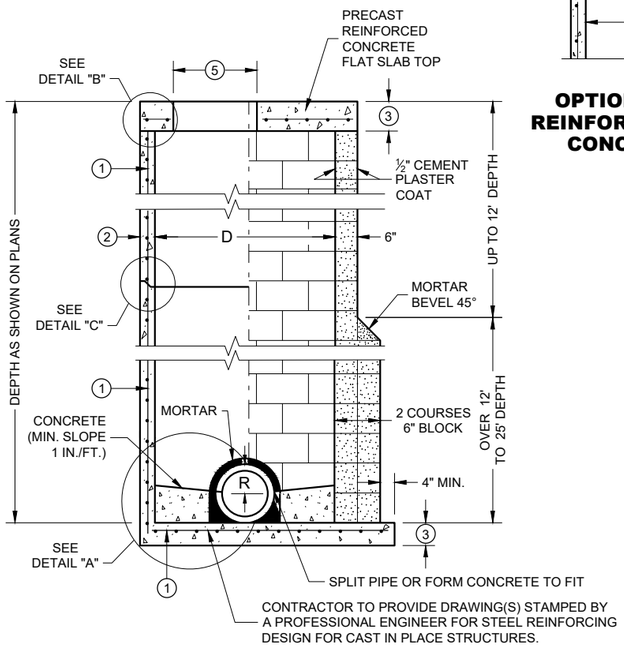
(1) FOR PRECAST MANHOLES PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.

(2) SEE PIPE MATRIX TABLE FOR MINIMUM WALL THICKNESS FOR PRECAST MANHOLES

(3) SEE PIPE MATRIX TABLE FOR MINIMUM THICKNESS OF PRECAST FLAT SLAB TOPS AND BASES.

(4) JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP.).

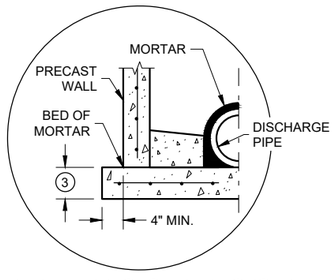
(5) SEE MANHOLE COVER OPENING MATRIX.



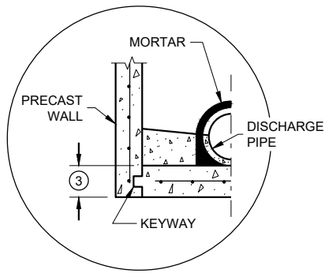
SECTION A - A

PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

CONCRETE BLOCK WITH CAST IN PLACE OR PRECAST REINFORCED CONCRETE BASE (1)

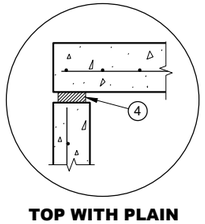


SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

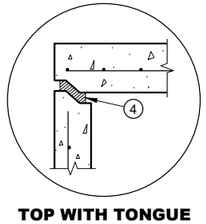


PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION

DETAIL "A"

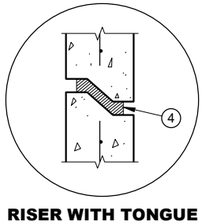


TOP WITH PLAIN END JOINT



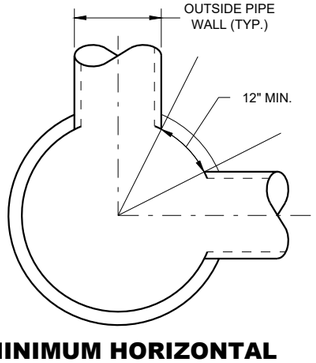
TOP WITH TONGUE AND GROOVE JOINT

DETAIL "B"



RISER WITH TONGUE AND GROOVE JOINT

DETAIL "C"



MINIMUM HORIZONTAL PIPE SEPARATION

MANHOLES, 3-FT, 4-FT 5-FT, 6-FT, 7-FT, 8-FT, 9-FT AND 10-FT DIAMETER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

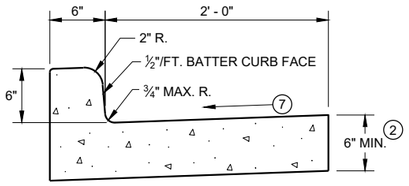
APPROVED
November 2021 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVE ENGINEER

FHWA

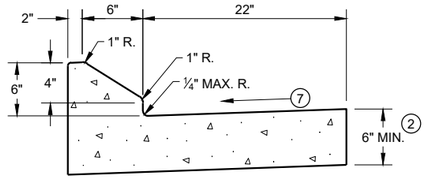
MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT, 8-FT, 9-FT AND 10-FT DIAMETER

6

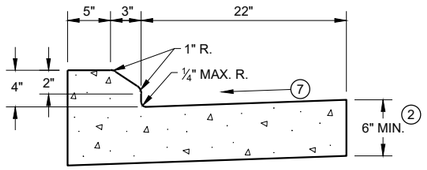
SDD 08B09 - 03



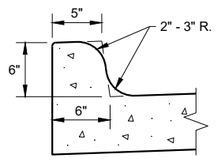
TYPES A (1) & D



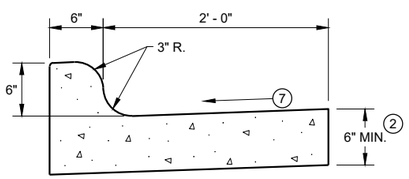
6" SLOPED CURB TYPES G (1) & J



4" SLOPED CURB TYPES G (1) & J

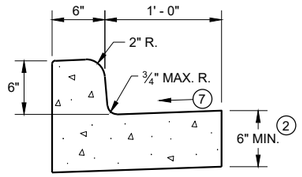


TYPES K (1) & L
(OPTIONAL CURB SHAPE)



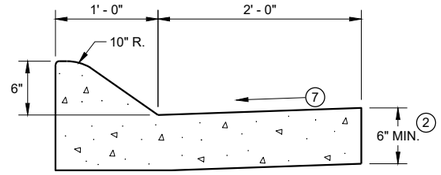
TYPES K (1) & L

CONCRETE CURB AND GUTTER 30"

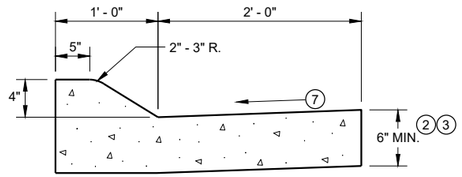


TYPES A (1) & D

CONCRETE CURB AND GUTTER 18"

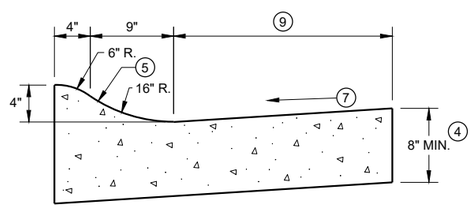


6" SLOPED CURB TYPES A (1) & D



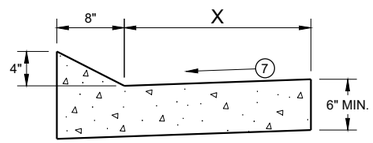
4" SLOPED CURB TYPES A (1) & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R (1) & T

TBT & TBTT	X
30"	22"
36"	28"

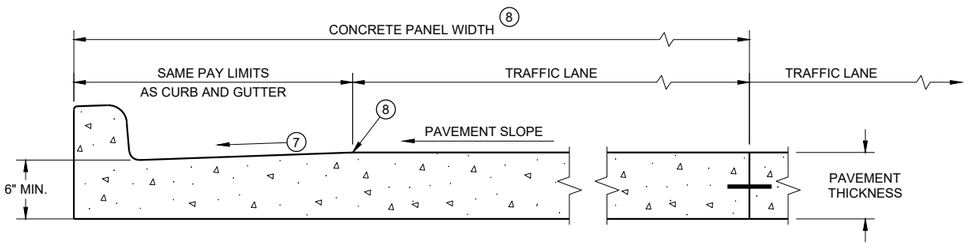


TYPES TBT & TBTT (1)

CONCRETE CURB AND GUTTER

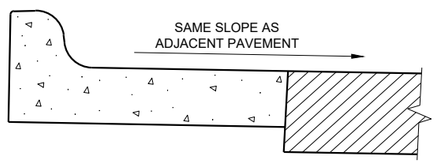
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT * WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER (6)
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.

(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(3) USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.

(4) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(5) UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.

(6) WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.

(7) USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.

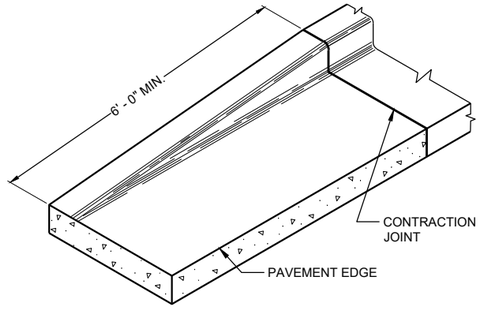
(8) INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.

(9) CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES

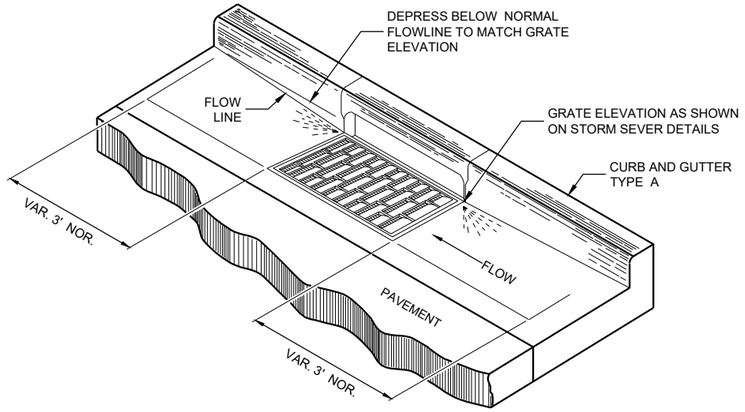
CONCRETE CURB AND GUTTER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6
SDD 08D01 - 23a



END SECTIONCURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS

(TYPICAL H INLET COVER SHOWN)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

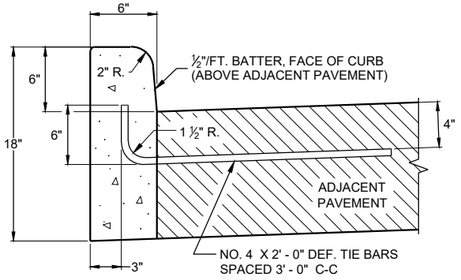
UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.

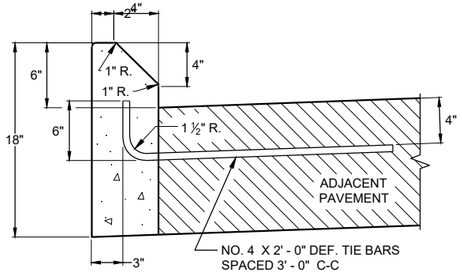
(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(10) REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.

(11) PLACE 1" THICK EXPANSION JOINT MATERIAL BETWEEN VERTICAL FACE CURB TYPES EXTENDING FROM THE TOP OF CURB TO 1 INCH BELOW THE ADJOINING CONCRETE SURFACE. RIGID CONCRETE STRUCTURES INCLUDE RAISED CONCRETE MEDIANS, CONCRETE SAFETY ISLANDS, SPLITTER ISLANDS, OR LOCATIONS IDENTIFIED ON THE PLANS.

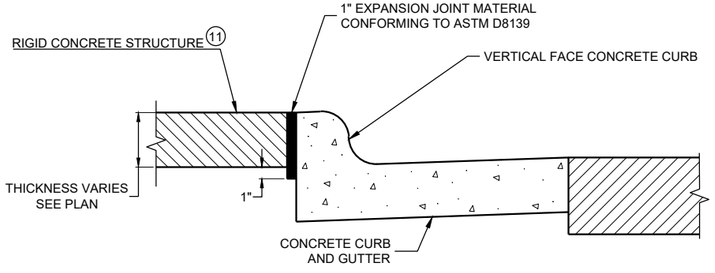


TYPES A (1) & D

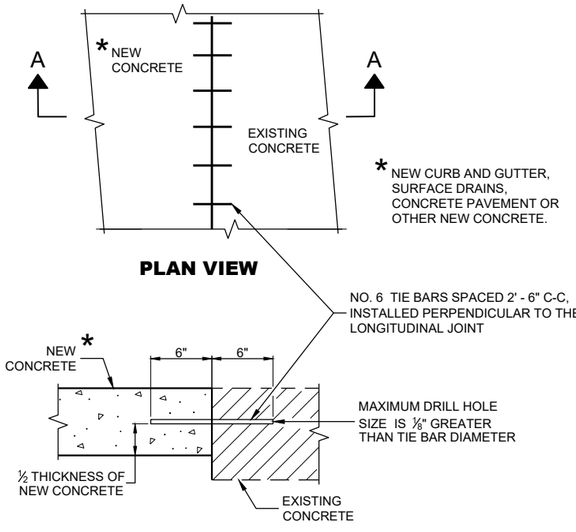


TYPES G (1) & J

CONCRETE CURB



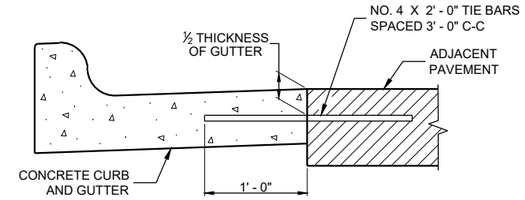
EXPANSION JOINT DETAIL FOR VERTICAL CURB ABUTTING A RIGID STRUCTURE (11)



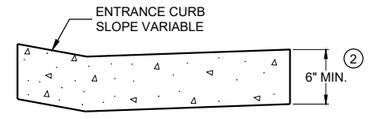
PLAN VIEW

SECTION A - A

TIE BARS DRILLED INTO EXISTING PAVEMENT



TYPICAL TIE BAR LOCATION (1)



DRIVEWAY ENTRANCE CURB (10)

(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Rodnery Taylor — ROADWAY STANDARDS DEVE ENGINEER

FHWA

6

SDD08D01 - 23b

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

ALL STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

(1) USE A JOINT SEALANT CONFORMING TO STANDARD SPECIFICATION 415.2.6.

(2) NO. 4 X 2' - 0" TIE BARS SPACED AT 12" C-C TO BE PLACED BY BRIDGE CONTRACTOR OR DRILLED TIE BARS PLACED AS DIRECTED BY THE ENGINEER.

(3) PAVED CONCRETE SHOULDER (SDD 13A03) OR CONCRETE DRAINAGE SLAB.

(4) CONCRETE PAVEMENT APPROACH SLAB (SHOWN) OR STRUCTURE APPROACH SLAB AND CONCRETE PAVEMENT APPROACH SLAB. SEE SDD 13B02.

(5) PAVED CONCRETE SHOULDER (SDD 13A03) OR ASPHALT SHOULDER.

(6) CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE TBT OR TBTT. USE TYPE TBTT CURB WITH NO. 4 X 2' -0" TIE BARS SPACED AT 3' - 0" C-C ONLY WHEN ADJACENT TO CONCRETE PAVEMENTS.

(7) PLACE FLUME BEFORE MSG THRIE BEAM TRANSITION POST 1 (SEE SDD 14B45)

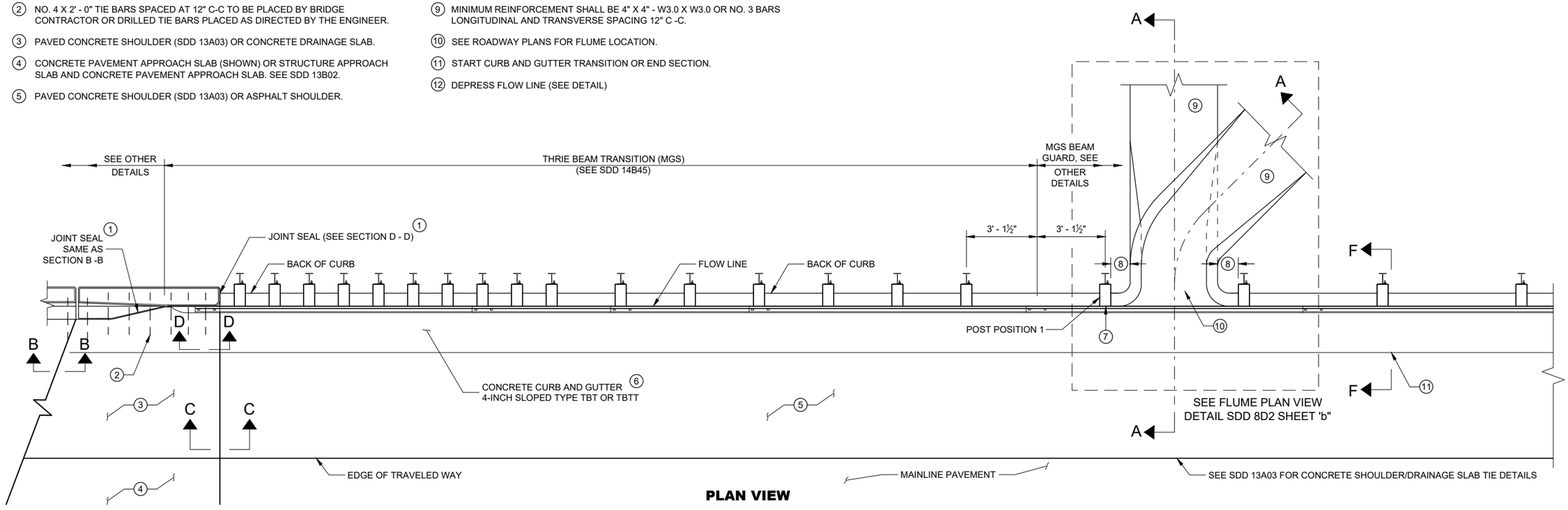
(8) CENTER FLUME BETWEEN POSTS. 6-INCH MINIMUM SEPARATION FROM OUTSIDE EDGE OF FLUME TO POSTS.

(9) MINIMUM REINFORCEMENT SHALL BE 4" X 4" - W3.0 X W3.0 OR NO. 3 BARS LONGITUDINAL AND TRANSVERSE SPACING 12" C -C.

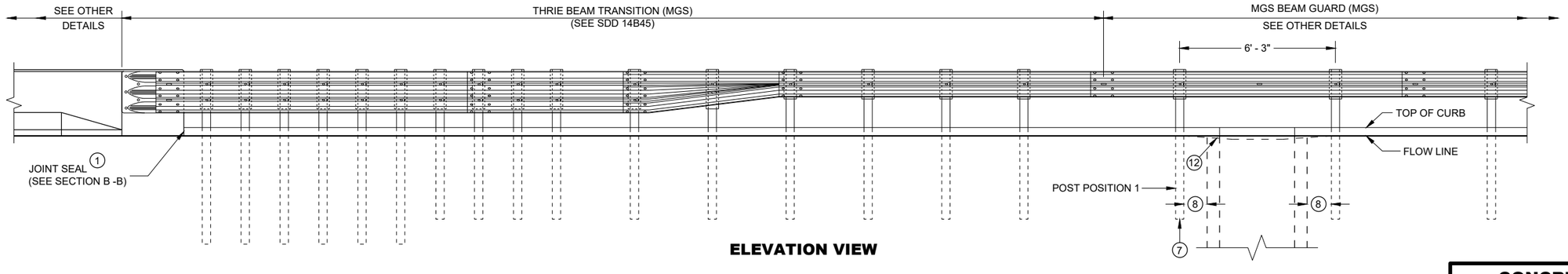
(10) SEE ROADWAY PLANS FOR FLUME LOCATION.

(11) START CURB AND GUTTER TRANSITION OR END SECTION.

(12) DEPRESS FLOW LINE (SEE DETAIL)



PLAN VIEW



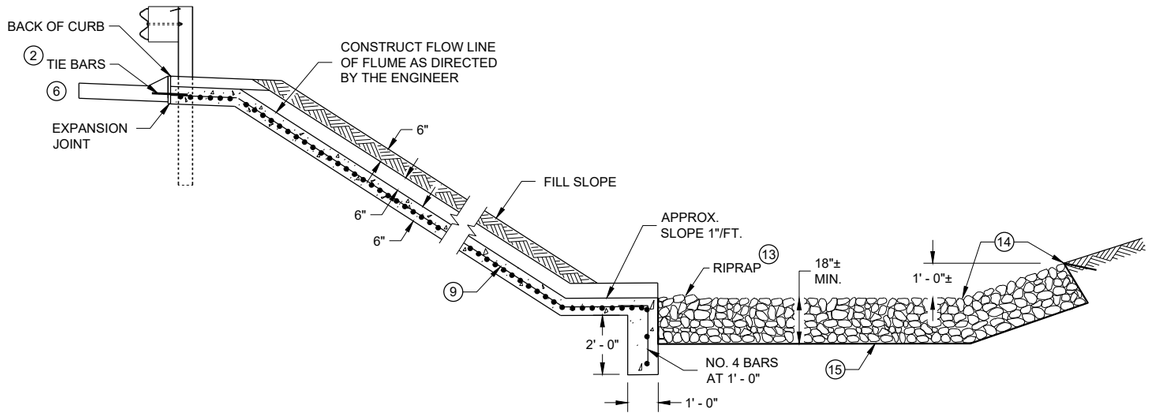
ELEVATION VIEW

CONCRETE SURFACE DRAINS FLUME TYPE AT STRUCTURES

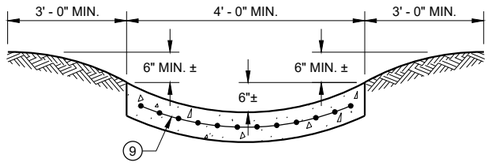
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

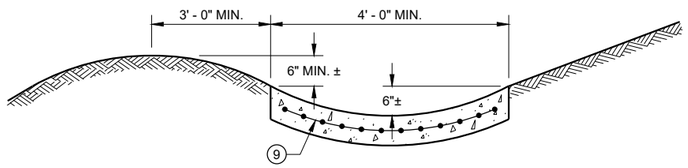
SDD 08D02 - 08a



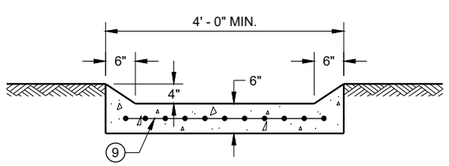
SECTION A - A



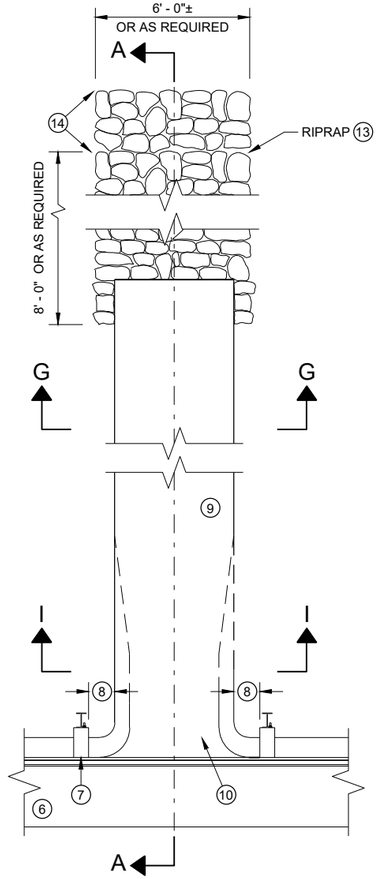
SECTION G - G



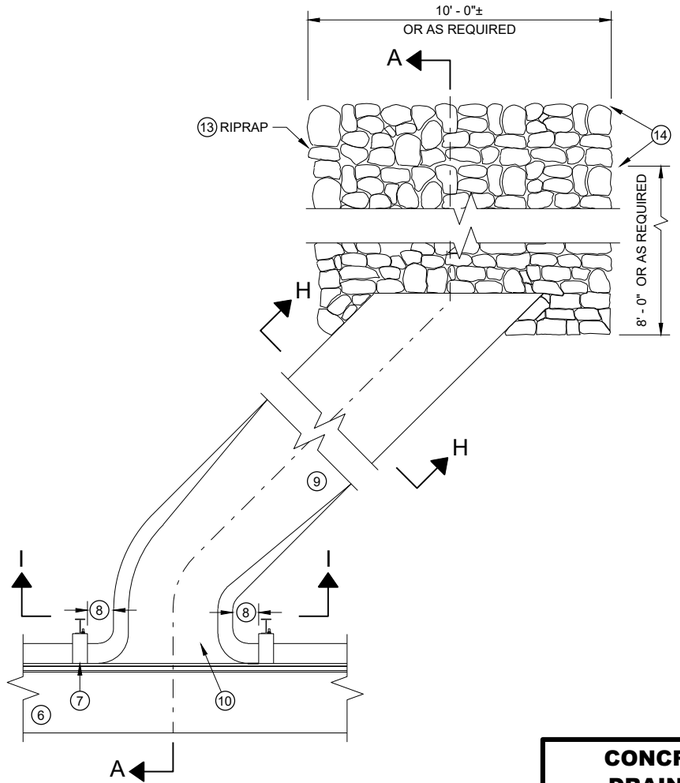
SECTION H - H



SECTION I - I



PLAN VIEW PERPENDICULAR FLUME



PLAN VIEW SKEWED FLUME

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

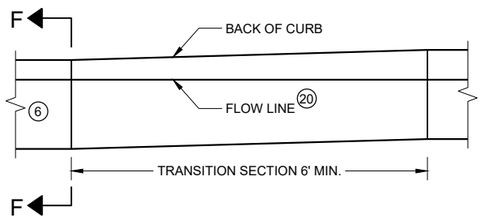
ALL STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

1. USE A JOINT SEALANT CONFORMING TO STANDARD SPECIFICATION 415.2.6.
2. NO. 4 X 2' - 0" TIE BARS SPACED AT 12" C-C TO BE PLACED BY BRIDGE CONTRACTOR OR DRILLED TIE BARS PLACED AS DIRECTED BY THE ENGINEER.
3. PAVED CONCRETE SHOULDER (SDD 13A03) OR CONCRETE DRAINAGE SLAB.
4. CONCRETE PAVEMENT APPROACH SLAB (SHOWN) OR STRUCTURE APPROACH SLAB AND CONCRETE PAVEMENT APPROACH SLAB. SEE SDD 13B02 AND STRUCTURE PLANS.
5. PAVED CONCRETE SHOULDER (SDD 13A03) OR ASPHALT SHOULDER.
6. CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE TBT OR TBTT. USE TYPE TBTT CURB WITH NO. 4 X 2' -0" TIE BARS SPACED AT 3' - 0" C-C ONLY WHEN ADJACENT TO CONCRETE PAVEMENTS.
7. PLACE FLUME BEFORE MSG THRIE BEAM TRANSITION POST 1 (SEE SDD 14B45)
8. CENTER FLUME BETWEEN POSTS. 6-INCH MINIMUM SEPARATION FROM OUTSIDE EDGE OF FLUME TO POSTS.
9. MINIMUM REINFORCEMENT SHALL BE 4" X 4" - W3.0 X W3.0 OR NO. 3 BARS LONGITUDINAL AND TRANSVERSE SPACING 12" C -C.
10. SEE ROADWAY PLANS FOR FLUME LOCATION.
11. START CURB AND GUTTER TRANSITION OR END SECTION.
12. DEPRESS FLOW LINE (SEE DETAIL)
13. MEDIUM RIPRAP UNLESS OTHERWISE SPECIFIED.
14. LIMITS OF ADDITIONAL RIPRAP WHEN SPECIAL DITCH AS REQUIRED.
15. GEOTEXTILE TYPE HR.

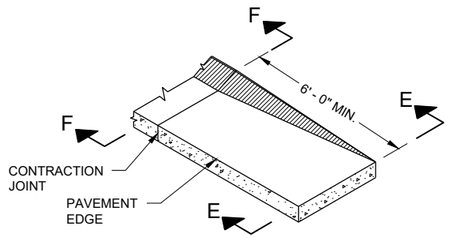
CONCRETE SURFACE DRAINS FLUME TYPE AT STRUCTURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

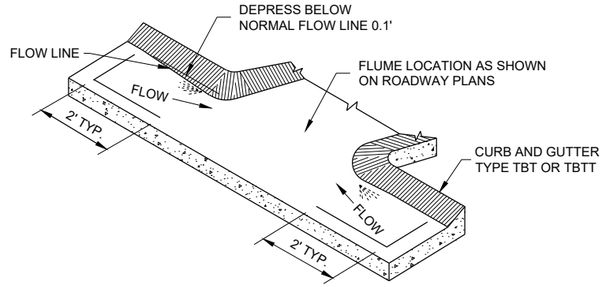
6
SDD 08D02 - 08b



CURB AND GUTTER TRANSITION SECTION
CONCRETE CURB AND GUTTER 4-INCH SLOPED
36 INCH TYPE TBT OR TBTT



CURB AND GUTTER END SECTION
CONCRETE CURB AND GUTTER 4-INCH SLOPED
36 INCH TYPE TBT OR TBTT



CURB AND GUTTER FLOW LINE DEPRESSION
AT FLUMES CONCRETE CURB AND GUTTER
4-INCH SLOPED 36 INCH TYPE TBT OR TBTT

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

ALL STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

(1) USE A JOINT SEALANT CONFORMING TO STANDARD SPECIFICATION 415.2.6.

(2) NO. 4 X 2' - 0" TIE BARS SPACED AT 12" C-C TO BE PLACED BY BRIDGE CONTRACTOR OR DRILLED TIE BARS PLACED AS DIRECTED BY THE ENGINEER.

(3) PAVED CONCRETE SHOULDER (SDD 13A03) OR CONCRETE DRAINAGE SLAB.

(4) CONCRETE PAVEMENT APPROACH SLAB (SHOWN) OR STRUCTURE APPROACH SLAB AND CONCRETE PAVEMENT APPROACH SLAB. SEE SDD 13B02 AND STRUCTURE PLANS.

(5) PAVED CONCRETE SHOULDER (SDD 13A03) OR ASPHALT SHOULDER.

(6) CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE TBT OR TBTT. USE TYPE TBTT CURB WITH NO. 4 X 2' -0" TIE BARS SPACED AT 3' - 0" C-C ONLY WHEN ADJACENT TO CONCRETE PAVEMENTS.

(7) PLACE FLUME BEFORE MSG THRIE BEAM TRANSITION POST 1 (SEE SDD 14B45)

(8) CENTER FLUME BETWEEN POSTS. 6-INCH MINIMUM SEPARATION FROM OUTSIDE EDGE OF FLUME TO POSTS.

(9) MINIMUM REINFORCEMENT SHALL BE 4" X 4" - W3.0 X W3.0 OR NO. 3 BARS LONGITUDINAL AND TRANSVERSE SPACING 12" C -C.

(10) SEE ROADWAY PLANS FOR FLUME LOCATION.

(11) START CURB AND GUTTER TRANSITION OR END SECTION.

(12) DEPRESS FLOW LINE (SEE DETAIL)

(13) MEDIUM RIPRAP UNLESS OTHERWISE SPECIFIED.

(14) LIMITS OF ADDITIONAL RIPRAP WHEN SPECIAL DITCH IS REQUIRED.

(15) GEOTEXTILE TYPE HR.

(16) MINIMUM REINFORCEMENT SHALL BE 6" X 6" - W4.0 X W4.0 OR NO. 3 BARS LONGITUDINAL AND TRANSVERSE SPACING 12" C - C.

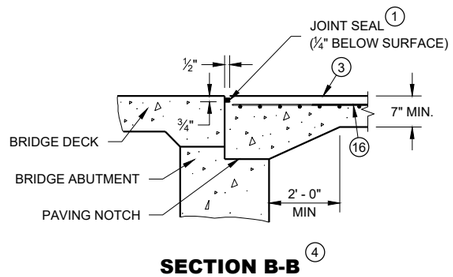
(17) MSG THRIE BEAM TRANSITION POST 1. SEE SDD 14B45 FOR ADDITIONAL CONSTRUCTION DETAILS AND ACCEPTABLE MATERIALS.

(18) MAINTAIN WIDTH, THICKNESS AND CROSS SLOPE OF ADJACENT TYPE TBT OR TBTT CURB. SEE NOTE 6 FOR TIE BAR SPACING.

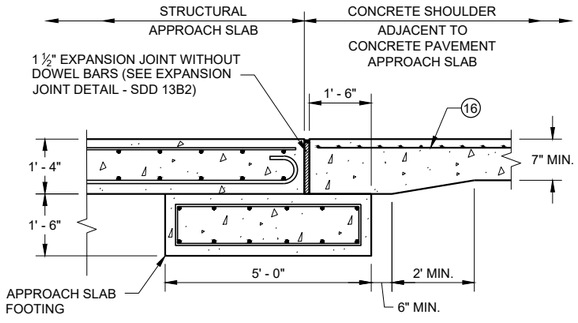
(19) ALIGN FACE OF POST BLOCK WITH FLOW LINE.

(20) MAINTAIN FLOW LINE AT EDGE OF PAVEMENT/FACE OF BEAM GUARD AS APPLICABLE.

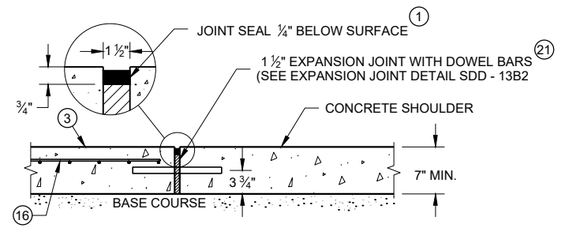
(21) DO NOT CONSTRUCT AN EXPANSION JOINT OR INSTALL DOWEL BARS WHEN ABUTTING HMA PAVEMENTS.



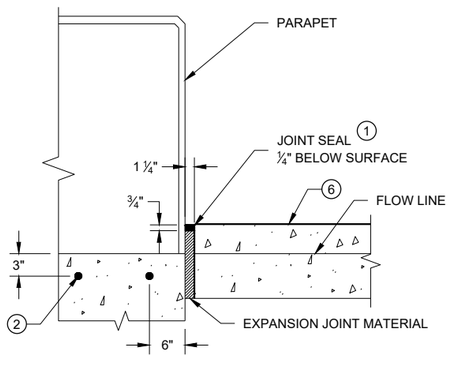
SECTION B-B (4)



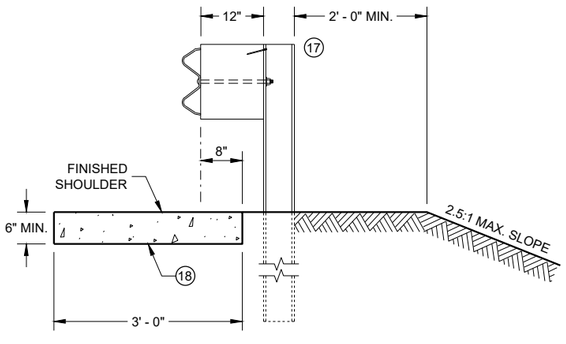
SECTION C - C
JOINT DETAIL FOR BRIDGE WITH STRUCTURAL APPROACH SLAB AND CONCRETE APPROACH SLAB



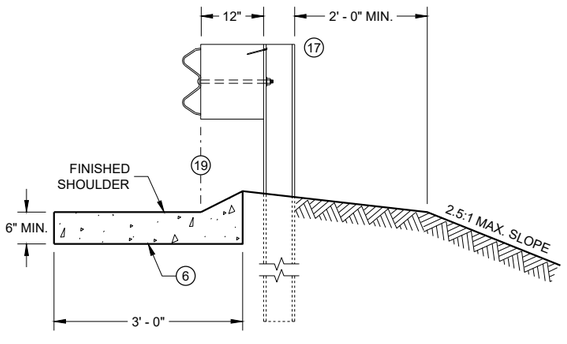
SECTION C - C
JOINT DETAIL FOR BRIDGE APPROACH WITH CONCRETE SHOULDERS



SECTION D - D



SECTION E - E



SECTION F - F

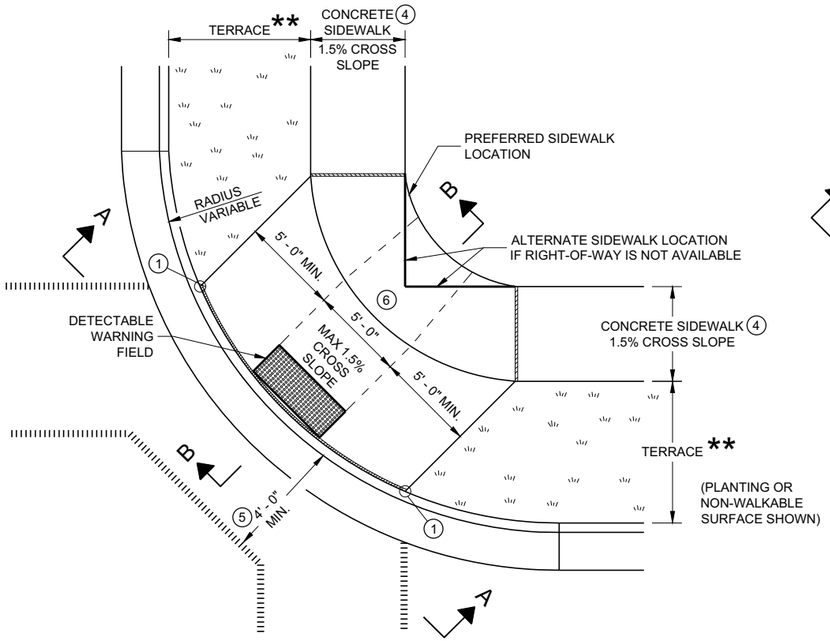
CONCRETE SURFACE DRAINS FLUME TYPE AT STRUCTURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVE ENGINEER

FHWA

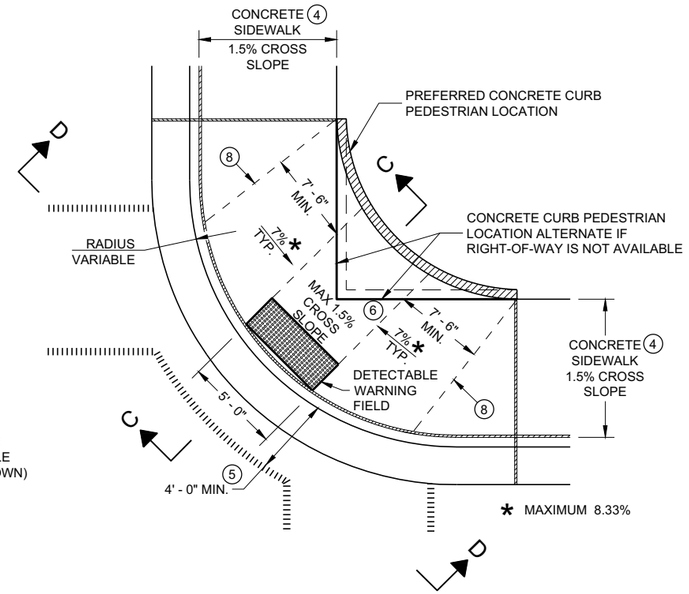
6
SDD 08D02 - 08c



PLAN VIEW

CURB RAMP TYPE 1

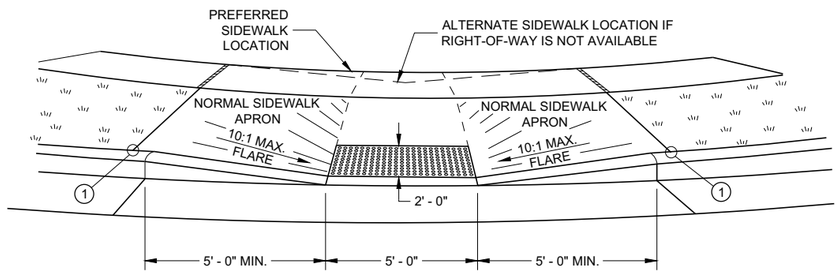
(CENTER OF CORNER RADIUS)



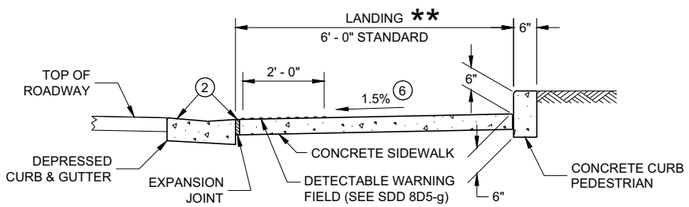
PLAN VIEW

CURB RAMP TYPE 1 - A

(NO TERRACE)

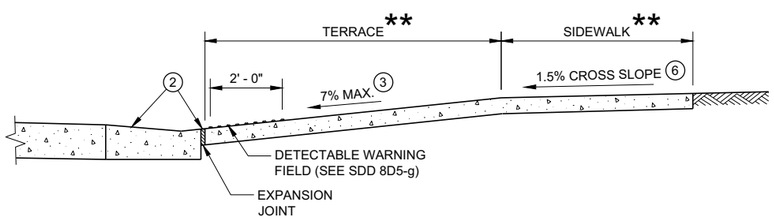


VIEW A - A FOR TYPE 1

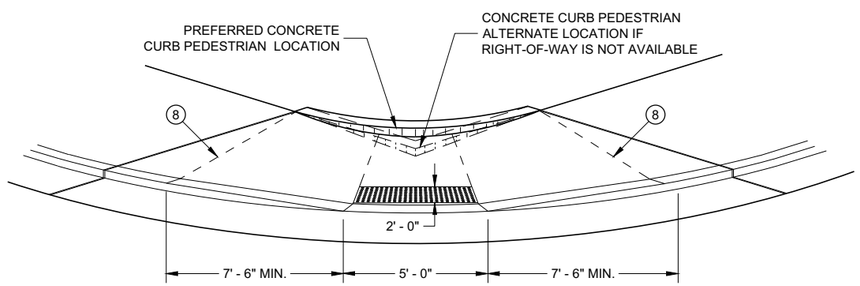


SECTION C - C FOR TYPE 1 - A

** WIDTH SHOWN ELSEWHERE IN THE PLANS



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.

TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FILED SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

(1) THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(5) PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.

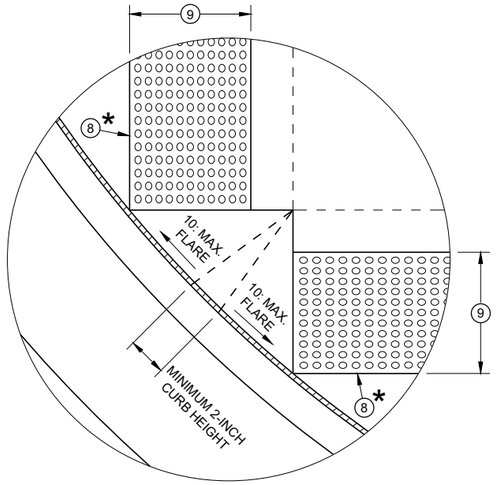
(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

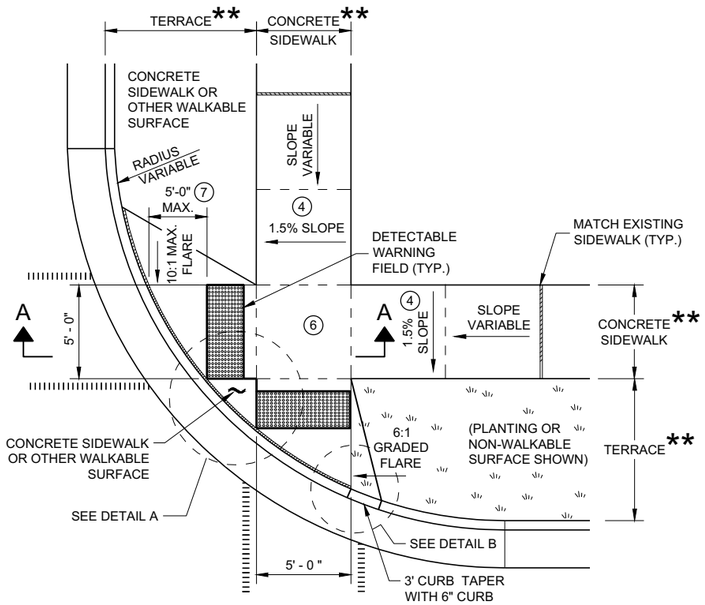
LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS
TYPE 1 AND 1-A



DETAIL A



PLAN VIEW
CURB RAMP TYPE 2
(CENTER OF CORNER RADIUS)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.

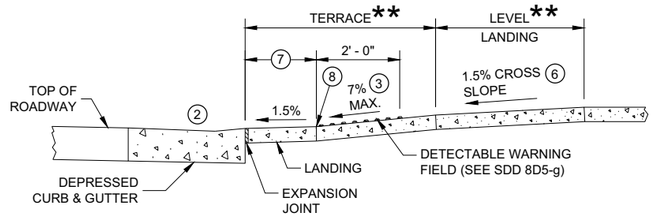
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.

(7) WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

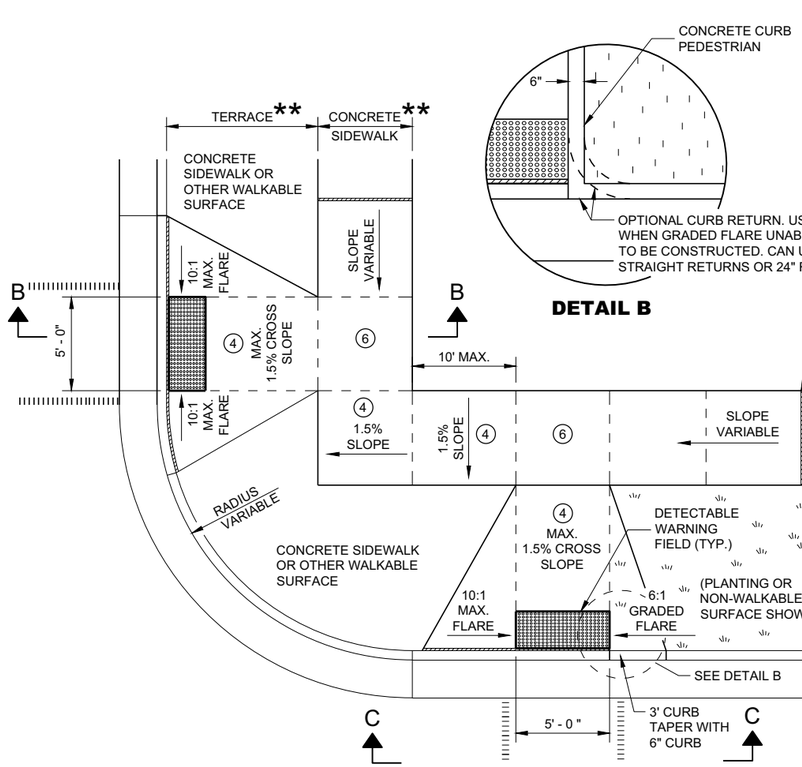
(9) WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.



SECTION A - A FOR TYPE 2

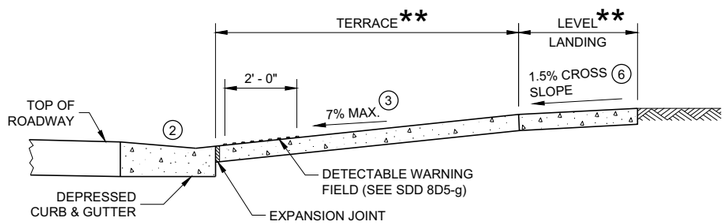
* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

** WIDTH SHOWN ELSEWHERE IN THE PLANS



DETAIL B

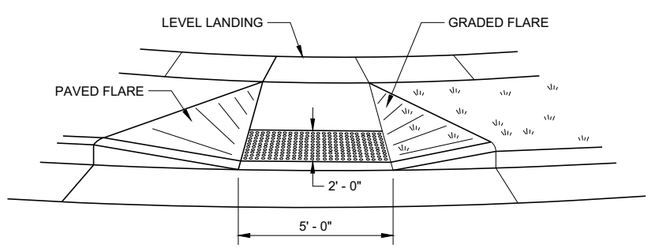
PLAN VIEW
CURB RAMP TYPE 3
(OUTSIDE OF CROSSWALK AREA)



SECTION B - B FOR TYPE 3

LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)



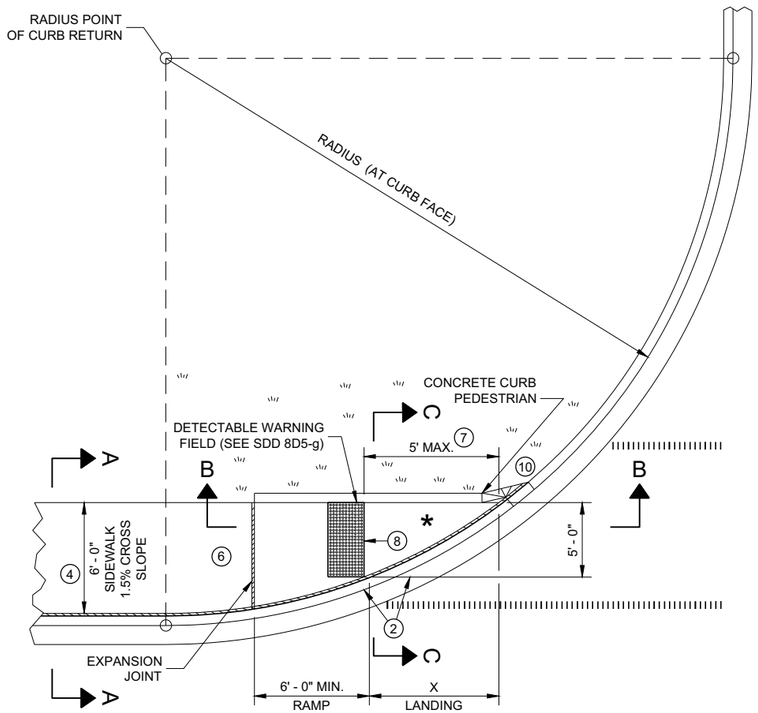
VIEW C - C FOR TYPE 3

CURB RAMPS TYPE 2 AND 3

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 08D05 - 20b

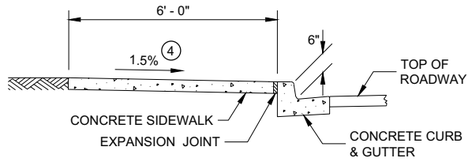


PLAN VIEW

CURB RAMP TYPE 4A

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"
15 FEET	6' - 5½"

INTERMEDIATE RADII CAN BE INTERPOLATED



SECTION A - A FOR TYPE 4A

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

(7) WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

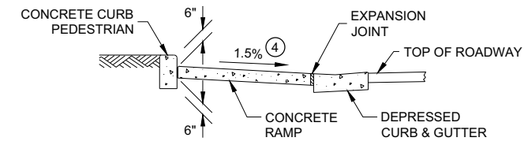
(10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

LEGEND

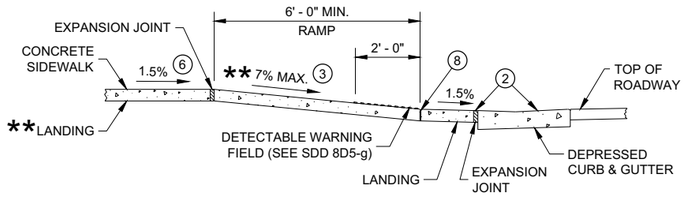
½" EXPANSION JOINT SIDEWALK

CONTRACTION JOINT SIDEWALK

PAVEMENT MARKING CROSSWALK (WHITE)



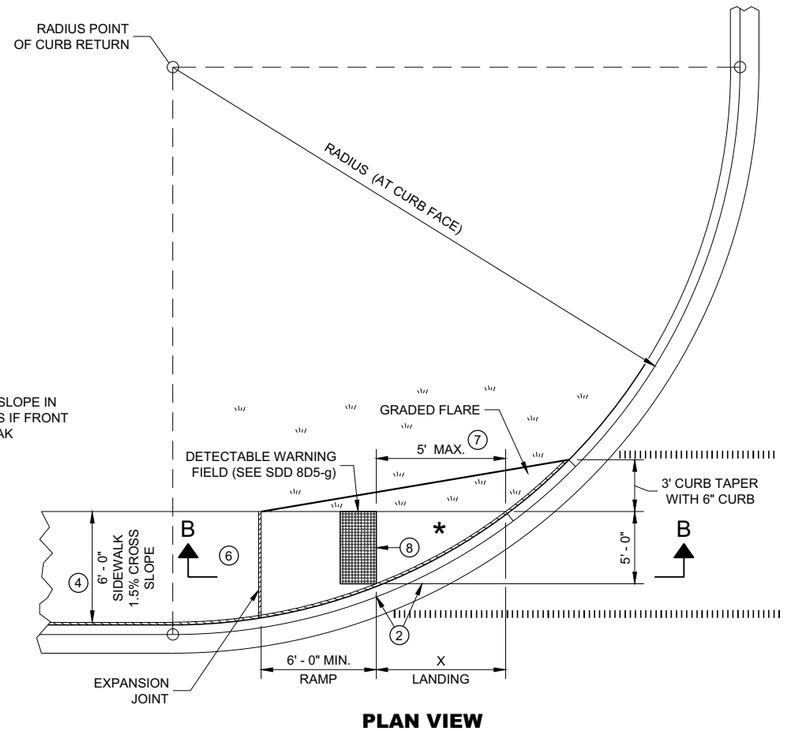
SECTION C - C FOR TYPE 4A



SECTION B - B FOR TYPE 4A AND TYPE 4A1

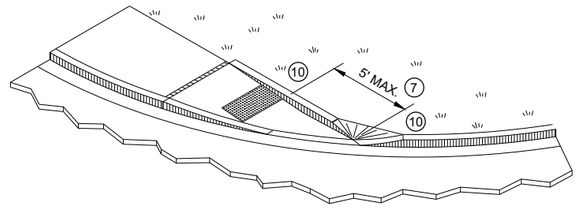
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

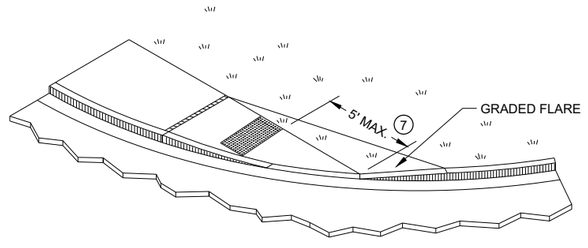


PLAN VIEW

CURB RAMP TYPE 4A1



ISOMETRIC VIEW FOR TYPE 4A



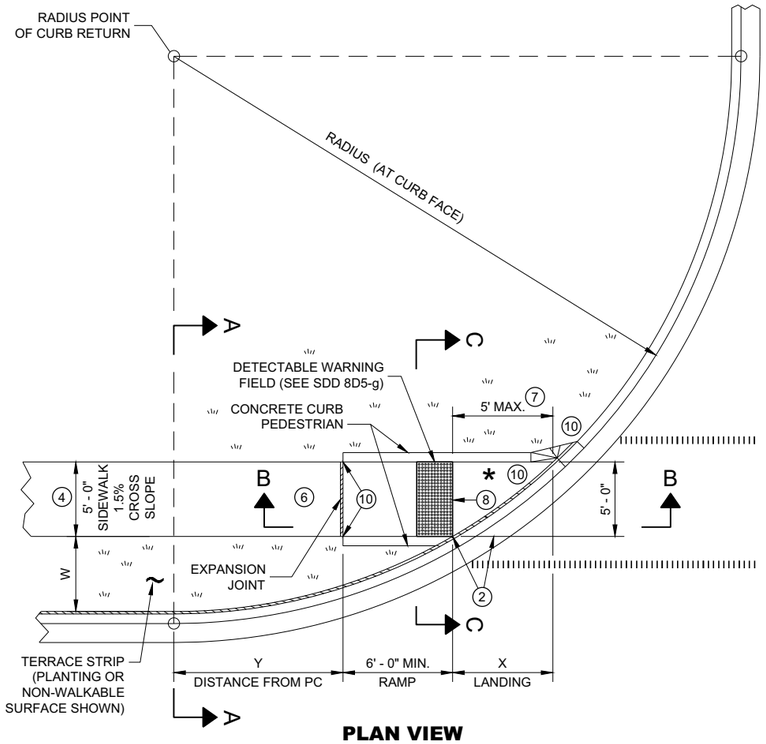
ISOMETRIC VIEW FOR TYPE 4A1

CURB RAMPS
TYPE 4A AND 4A1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 08D05 - 20c

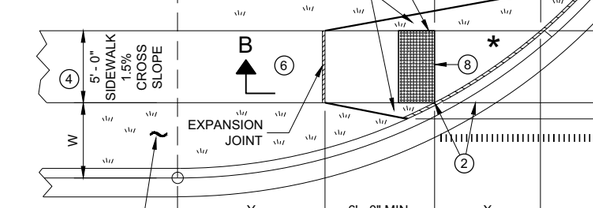


PLAN VIEW

CURB RAMP TYPE 4B

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 ¼"	0' - 5"	2' - 1"	1' - 4 ½"	1' - 5"	2' - 1"	0' - 10"	2' - 7 ½"	0' - 3 ¼"	3' - 0 ¼"						
15 FEET	4' - 6 ¾"	2' - 1 ¾"	3' - 9"	3' - 5 ¾"	3' - 1 ¼"	4' - 6"	2' - 6 ¾"	5' - 4 ½"	2' - 1"	6' - 1"	1' - 8"	6' - 8 ½"	1' - 3 ¼"	7' - 2 ½"	0' - 10 ¾"	7' - 7 ¼"
20 FEET	5' - 9 ¾"	3' - 6 ½"	4' - 11 ½"	5' - 1 ¾"	4' - 3 ¼"	6' - 5 ½"	3' - 8 ¾"	7' - 7"	3' - 3"	8' - 6 ½"	2' - 10"	9' - 4 ½"	2' - 5 ½"	10' - 1 ¼"	2' - 1 ¼"	10' - 9"
30 FEET			6' - 9 ¼"	7' - 11 ¼"	6' - 0 ¼"	9' - 8"	5' - 5"	11' - 1 ¾"	4' - 10 ¾"	12' - 5 ¾"	4' - 5 ½"	13' - 7 ¾"	4' - 0 ¾"	14' - 8 ½"	3' - 8 ½"	15' - 8 ¼"
40 FEET									6' - 1 ¾"	15' - 8 ½"	5' - 8"	17' - 2"	5' - 3"	18' - 5 ¾"	4' - 10 ¾"	19' - 8 ¼"
50 FEET															5' - 10 ¼"	23' - 2"

INTERMEDIATE RADII CAN BE INTERPOLATED
DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH



SECTION C - C FOR TYPE 4B

LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

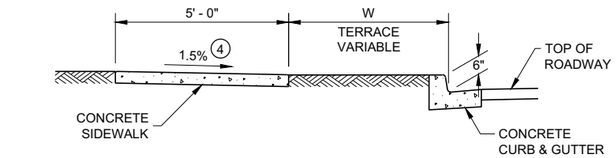
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

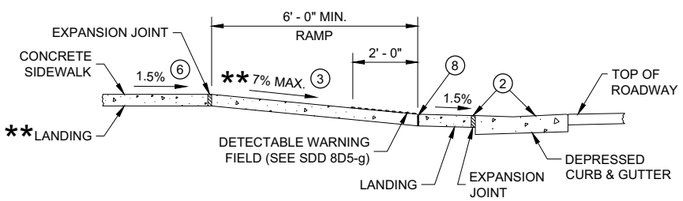
(7) WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

(10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.



SECTION A - A FOR TYPE 4B



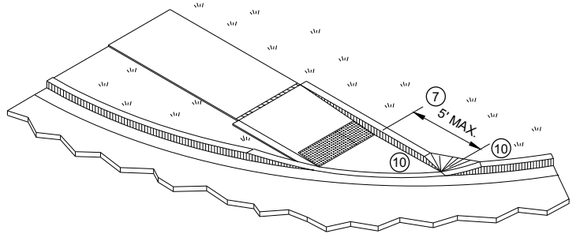
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

SECTION B - B FOR TYPE 4B AND TYPE 4B1

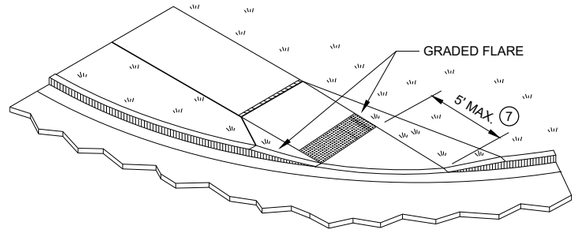
* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

PLAN VIEW

CURB RAMP TYPE 4B1



ISOMETRIC VIEW FOR TYPE 4B



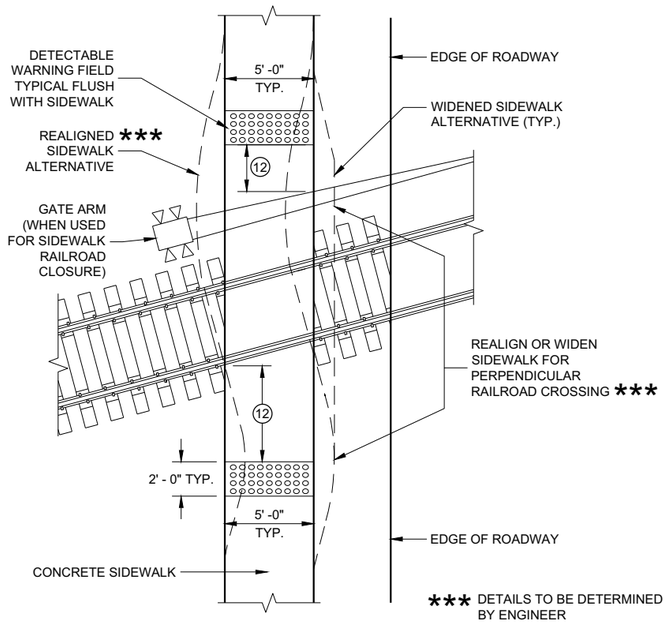
ISOMETRIC VIEW FOR TYPE 4B1

CURB RAMPS TYPE 4B AND 4B1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

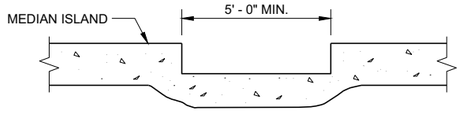
6

SDD 08D05 - 20d

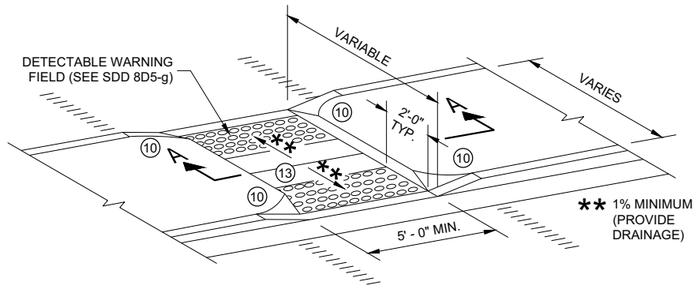


CURB RAMP TYPE 8

DETECTABLE WARNINGS AT RAILROAD CROSSING



SECTION A - A FOR TYPE 5



CURB RAMP TYPE 5

MEDIAN ISLAND NON-ELEVATED PEDESTRIAN CROSSING

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ½ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

(10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

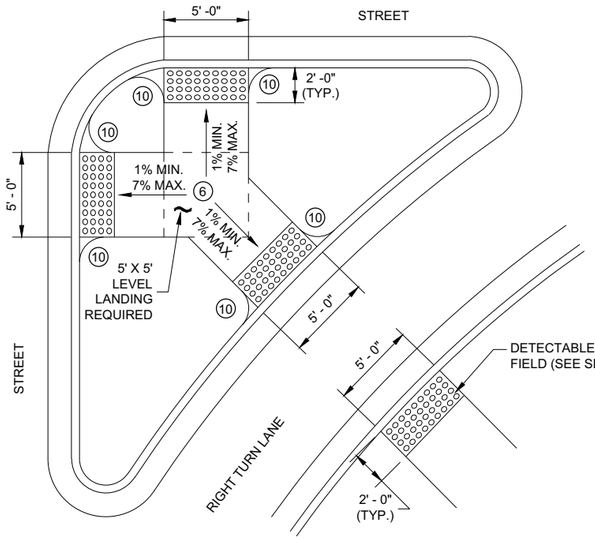
(11) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.

(12) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.

(13) DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STEET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

LEGEND

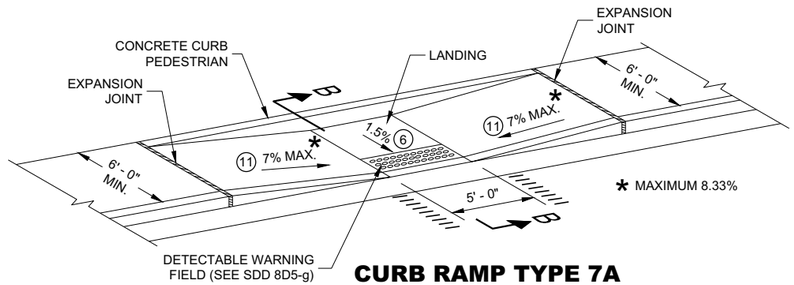
- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)



CURB RAMP TYPE 6

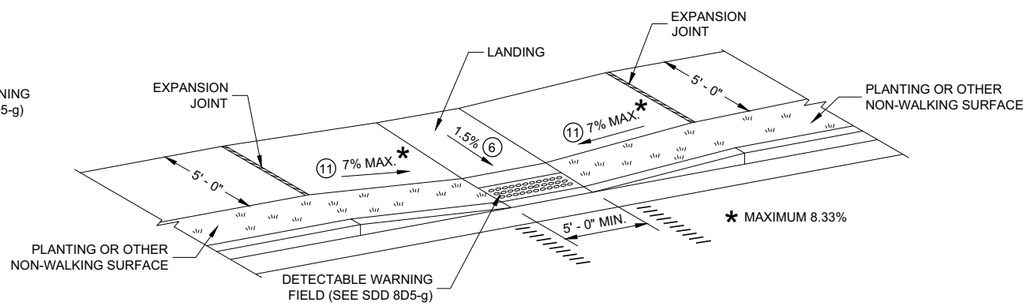
DETECTABLE WARNING AT ISLANDS

REFER TO GENERAL NOTES (2) AND (3) FOR ALL ISLAND CURB RAMPS



CURB RAMP TYPE 7A

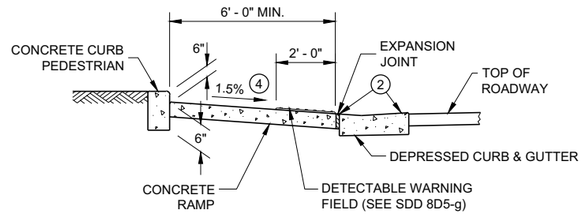
MID BLOCK CROSSING



CURB RAMP TYPE 7B

MID BLOCK CROSSING

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.



SECTION B - B FOR TYPE 7A

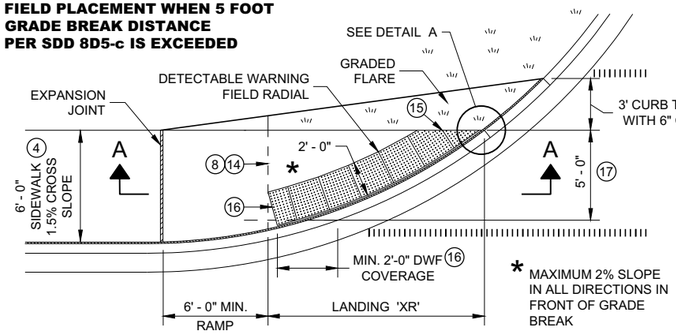
CURB RAMPS
TYPE 5, 6, 7A, 7B & 8

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

SDD 08D05 - 20e

SDD 08D05 - 20e

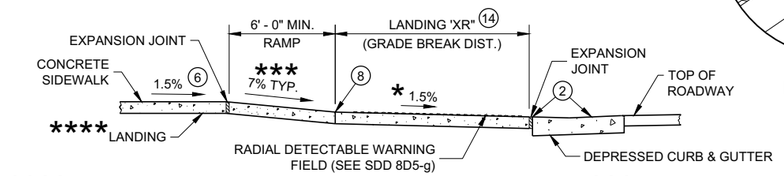
RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED



PLAN VIEW

CURB RAMP TYPE 4A1

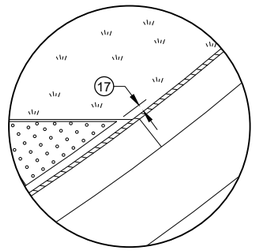
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)



**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

SECTION A - A FOR TYPE 4A1



DETAIL A

LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B CURB RAMPS ARE NOT SHOWN.

REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AD ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

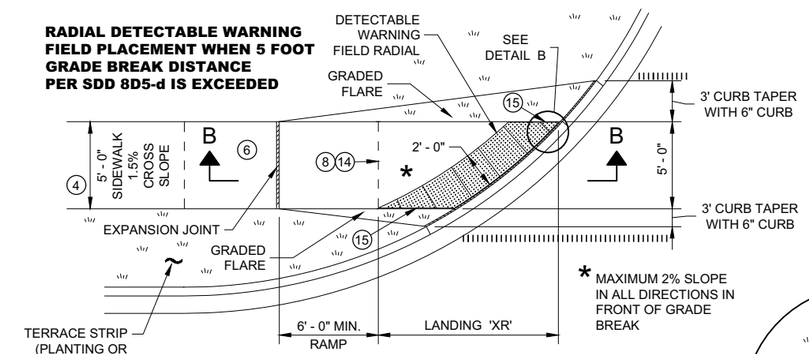
(14) CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.

(15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN ⅛" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

(16) USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.

(17) A MAXIMUM 3 INCH CONCRETE BORDER WITH IS ALLOWABLE IN FROM OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

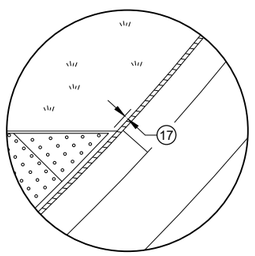
RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED



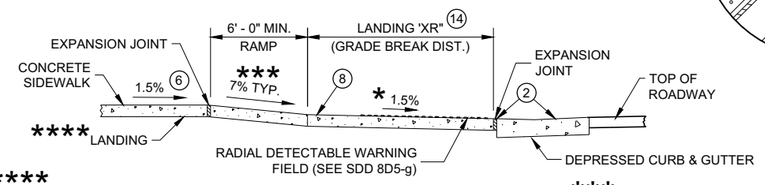
PLAN VIEW

CURB RAMP TYPE 4B1

(GRADE BREAK DISTANCE GREATER THAN 5 FEET)



DETAIL B

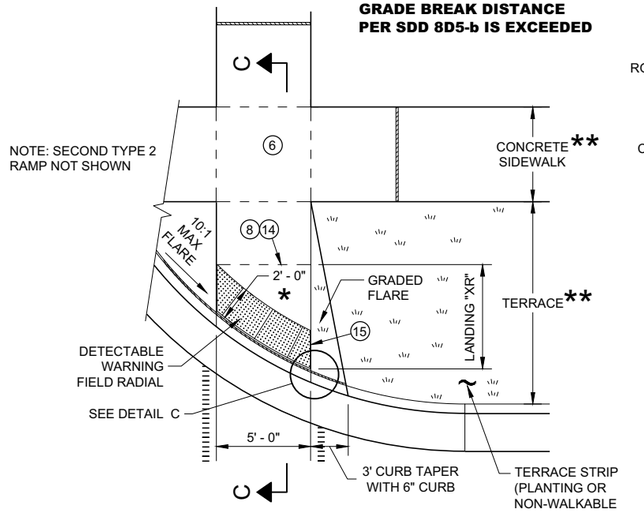


**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

SECTION B - B FOR TYPE 4B1

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED

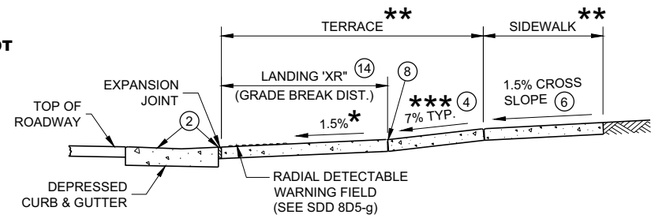


PLAN VIEW

CURB RAMP TYPE 2

(GRADE BREAK DISTANCE GREATER THAN 5 FEET)

(ON LINE WITH SIDEWALK)

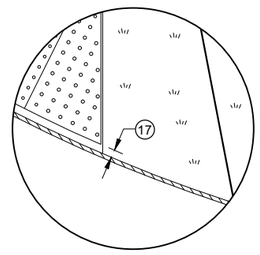


SECTION C - C FOR TYPE 2

* MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

** WIDTH SHOWN ELSEWHERE IN THE PLANS

*** MAXIMUM 8.33%



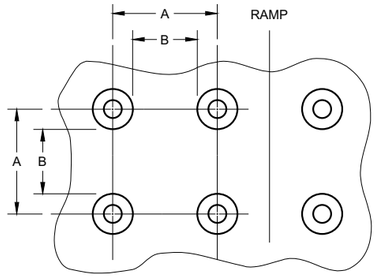
DETAIL C

CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS

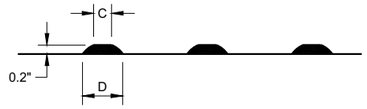
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

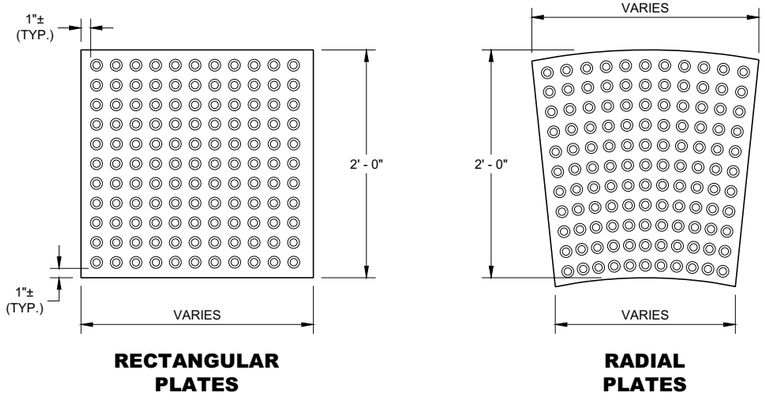


PLAN VIEW



ELEVATION VIEW

TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL



RECTANGULAR PLATES

RADIAL PLATES

PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.

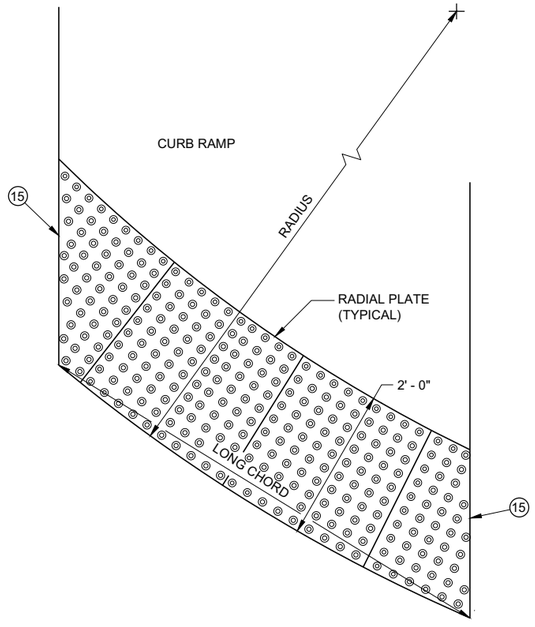
DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

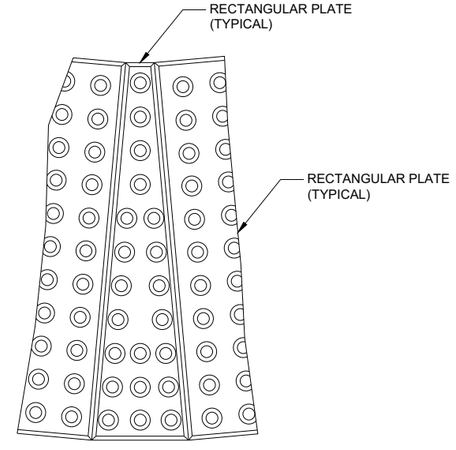
REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

(15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN ⅛" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.



PLAN VIEW
RADIAL DETECTABLE WARNING FIELD ATTRIBUTES



PLAN VIEW
RADIAL WEDGE PLATE CONNECTION DETAIL

CURB RAMPS
RECTANGULAR AND RADIAL
DETECTABLE WARNING PLATES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019
DATE
/S/ Rodney Taylor
ROADWAY STANDARDS DEVE
UNIT SUPERVISOR

FHWA

6

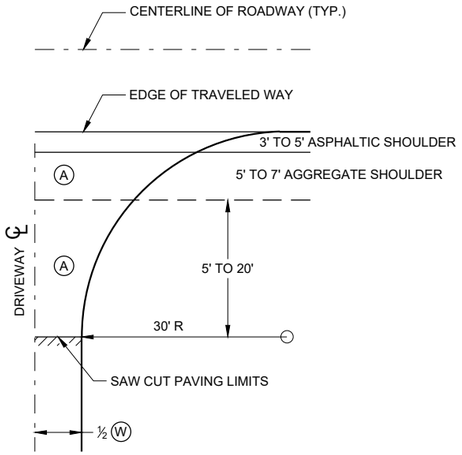
SDD 08D05 - 20g

6

SDD 08D05 - 20g

GENERAL NOTES

(1) DESIGN WILL DETERMINE FINAL DRIVEWAY ASPHALTIC THICKNESS BASED ON TYPE OF USAGE AND LOADINGS.

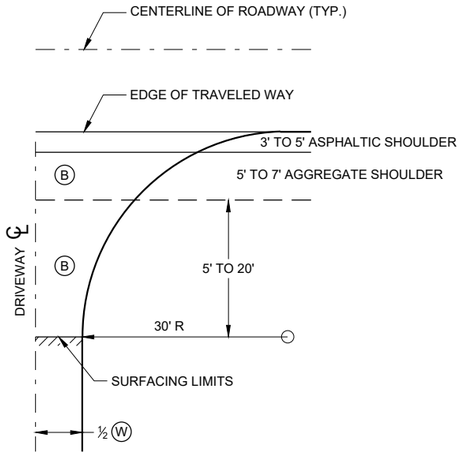


PLAN VIEW

Ⓐ : PAID FOR AS ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES. (TON)

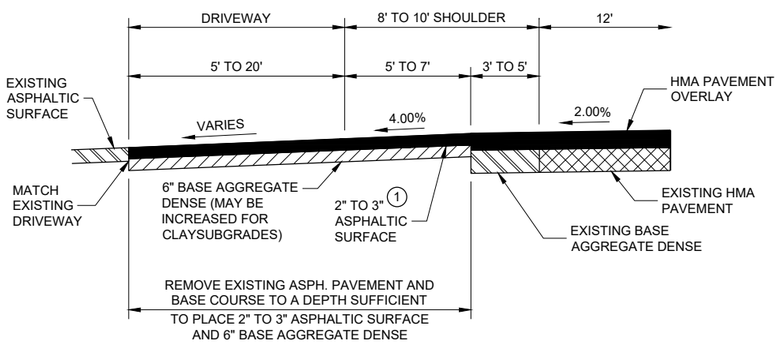
Ⓑ : PAID FOR AS BASE AGGREGATE DENSE 1 ¼" (TON)

Ⓦ : DRIVEWAY WIDTH 16' MIN. - 24' MAX.



PLAN VIEW

HALF SECTION

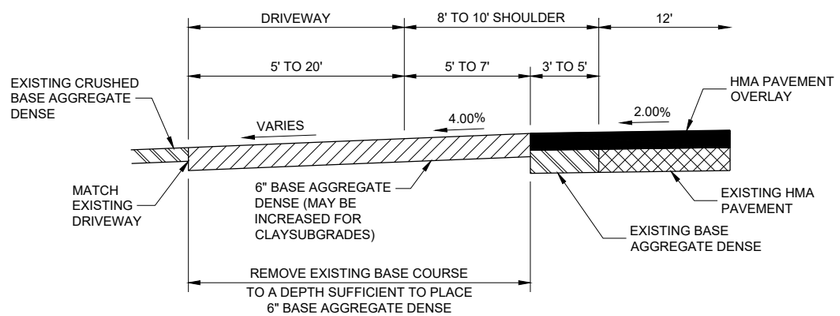


PROFILE VIEW

RURAL ENTRANCE
WITH ASPHALTIC SURFACE
RESURFACING PROJECTS

PLAN VIEW

HALF SECTION



PROFILE VIEW

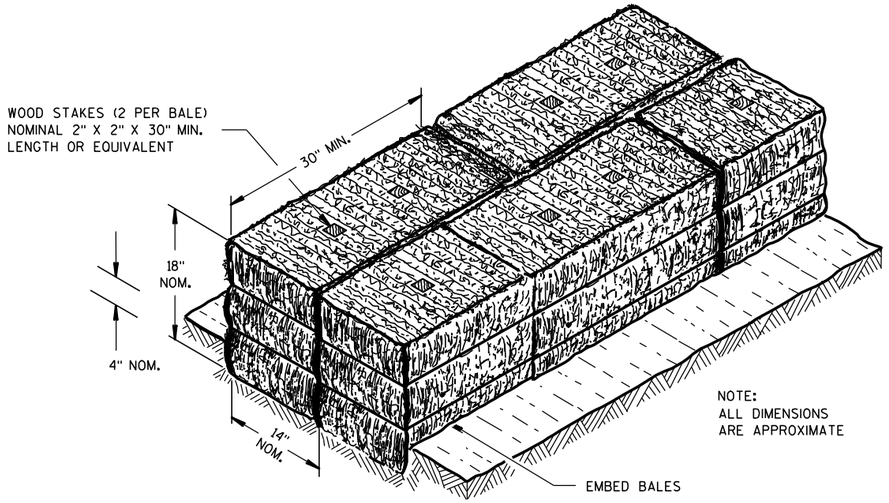
RURAL ENTRANCE
WITH AGGREGATE SURFACE
6" BASE AGGREGATE DENSE
RESURFACING PROJECTS

DRIVEWAYS WITHOUT CURB AND GUTTER RESURFACING PROJECTS RURAL

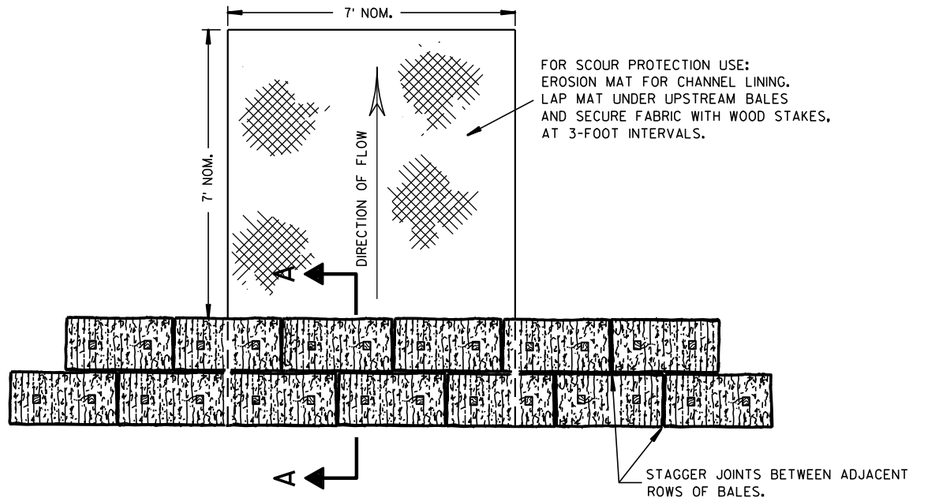
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2016 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVE ENGINEER
FHWA

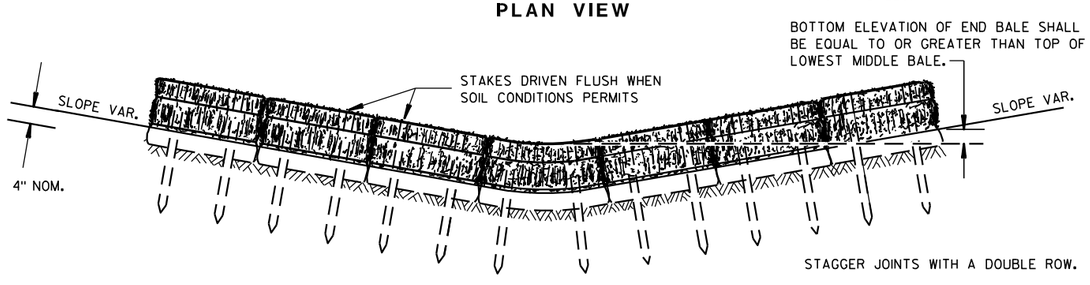
6
SDD 08D22 - 01



SECTION A-A



PLAN VIEW



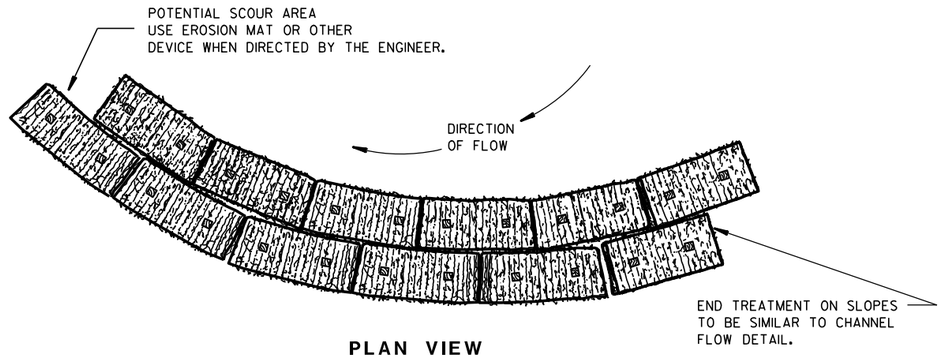
FRONT ELEVATION

TEMPORARY DITCH CHECK USING EROSION BALES (1)

GENERAL NOTES

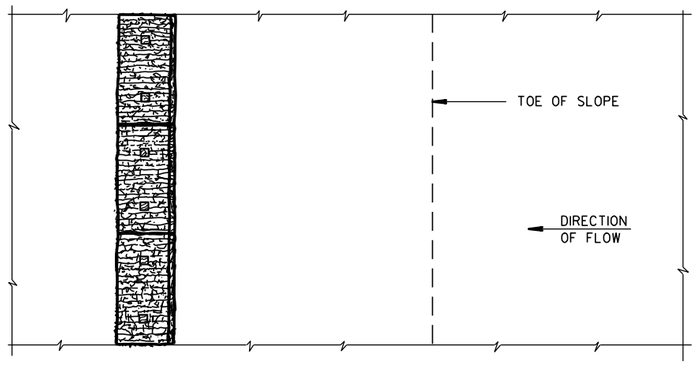
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

(1) TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

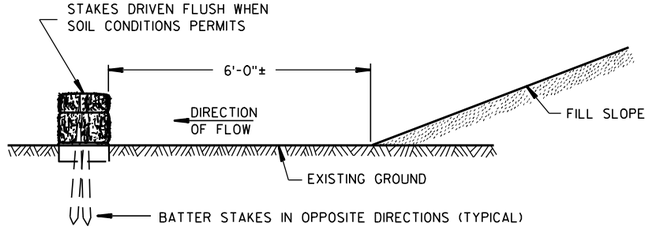


PLAN VIEW

WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW



FRONT ELEVATION

WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

EROSION BALES FOR SHEET FLOW

TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

6/04/02 — DATE

/S/ Beth Cannestra — CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA

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S.D.D. 8 E 8-3

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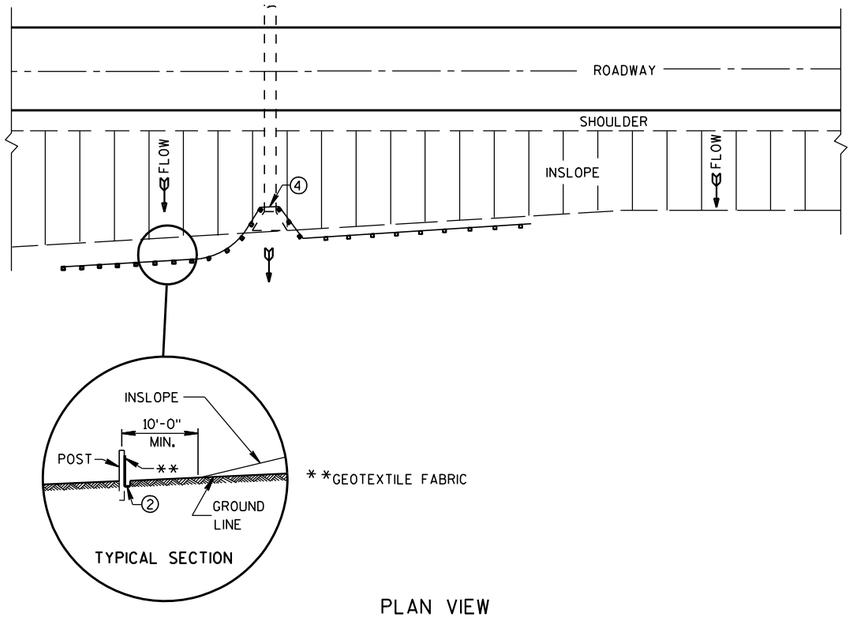
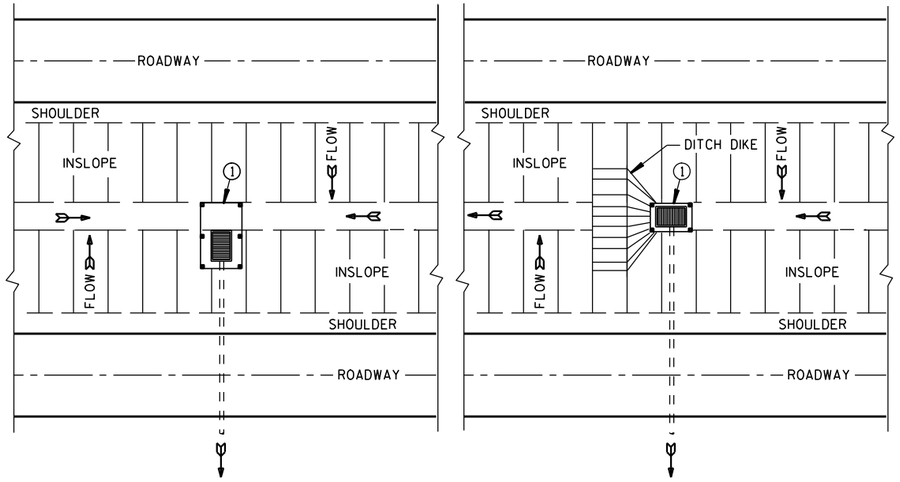
S.D.D. 8 E 8-3



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



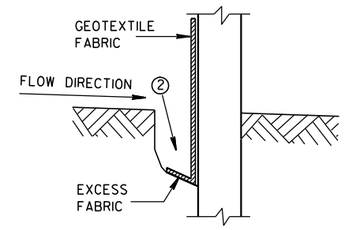
PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

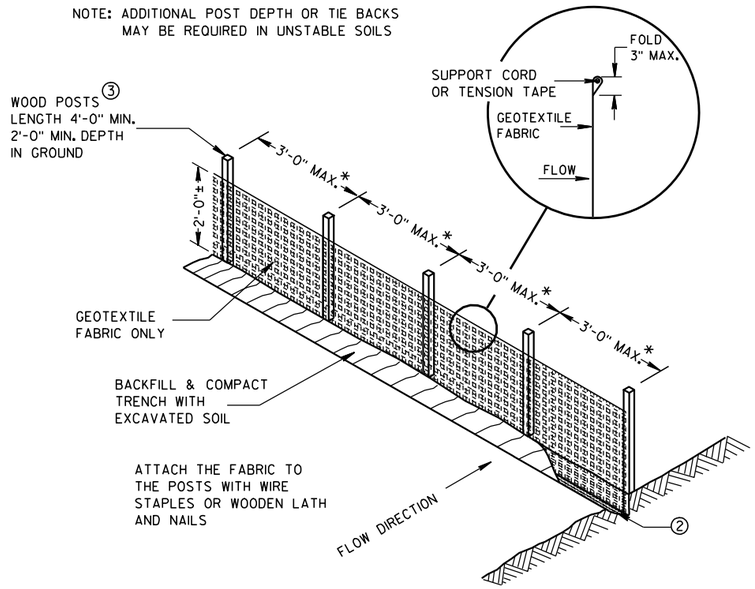
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

1. HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
2. FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
3. WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.
4. SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
5. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

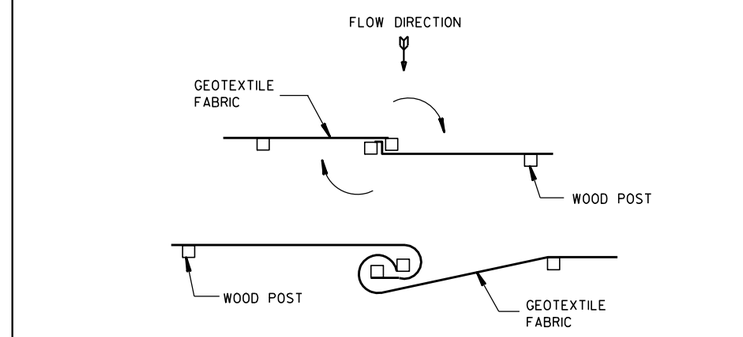


TRENCH DETAIL

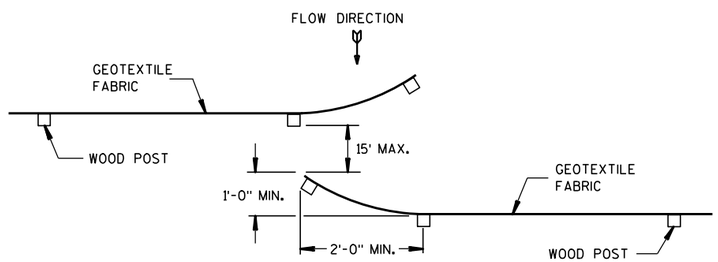


*NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.

SILT FENCE

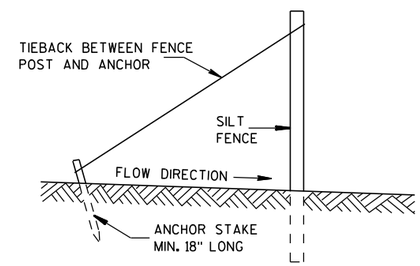


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE 5

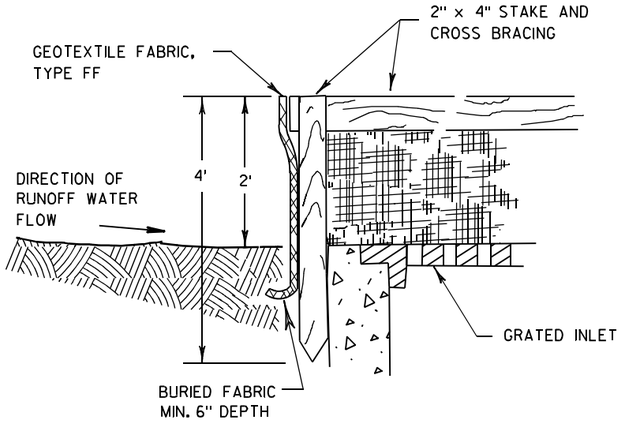


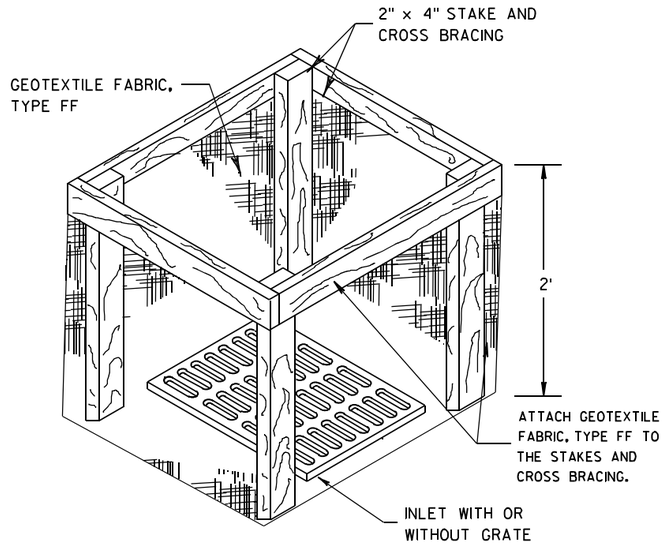
SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

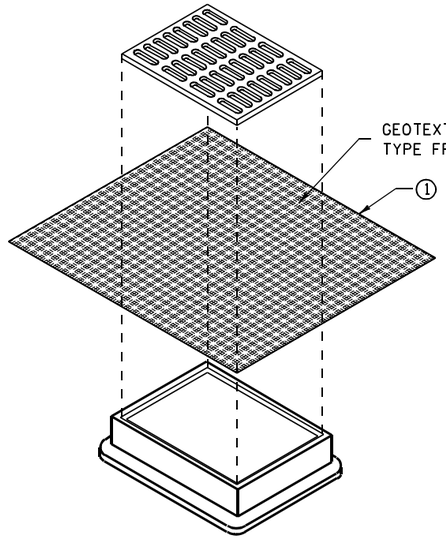
SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED	
4-29-05 DATE	/S/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	

6
S.D.D. 8 E 9-6



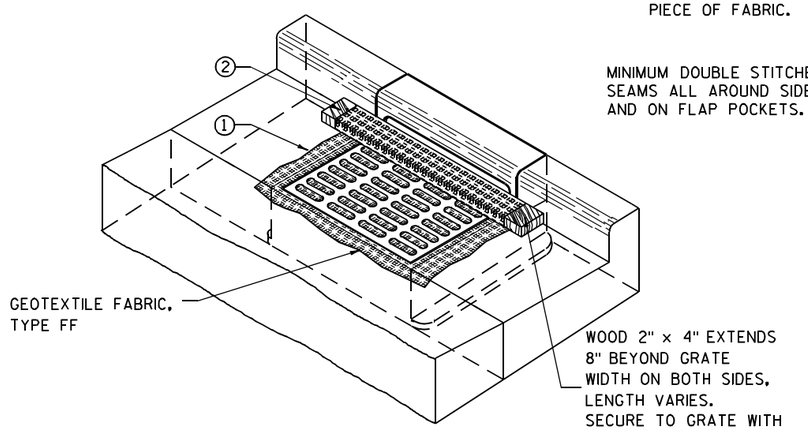


INLET PROTECTION, TYPE A

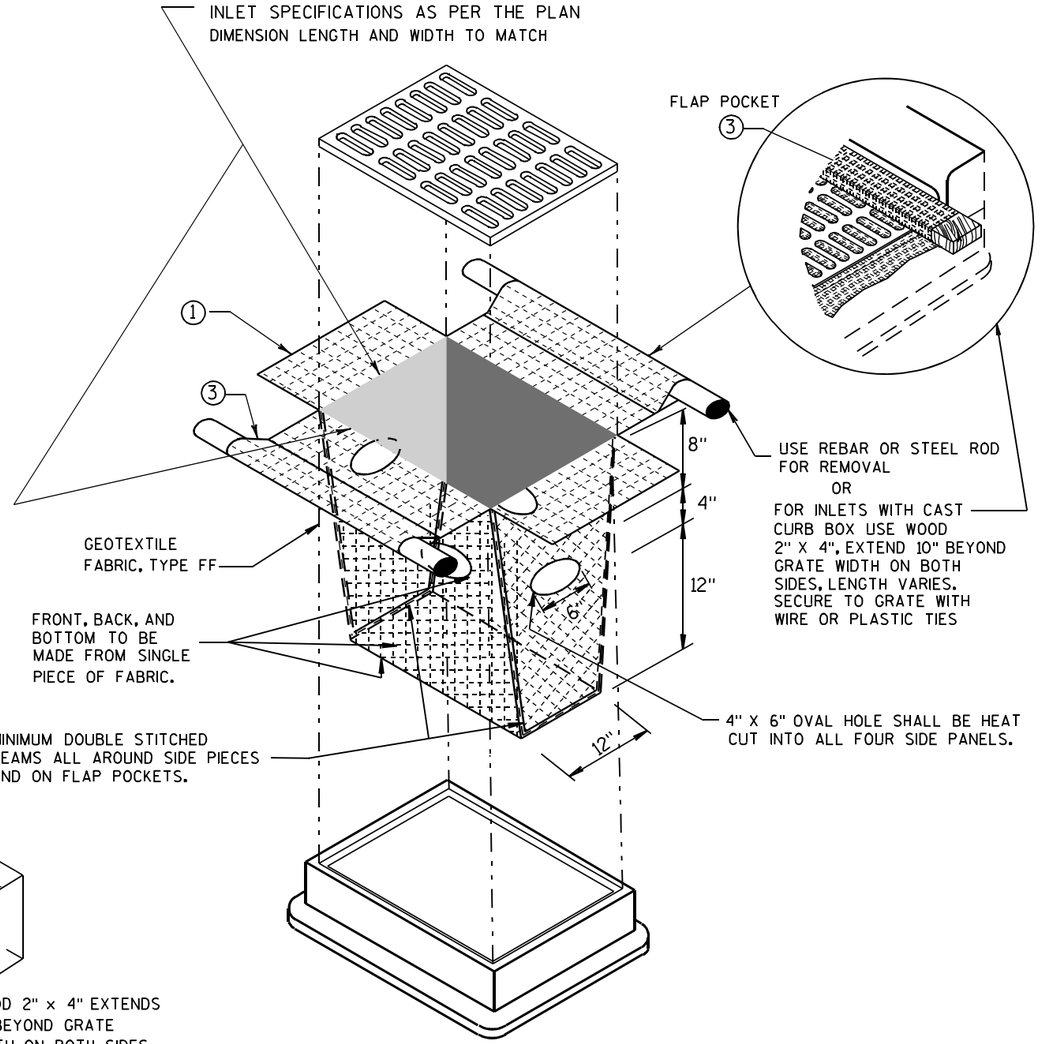


INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH
OR WITHOUT A CURB BOX AS PER NOTE ②)

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.

③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION
TYPE A, B, C, AND D

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

10/16/02
DATE

/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA

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S.D.D. 8 E 10-2

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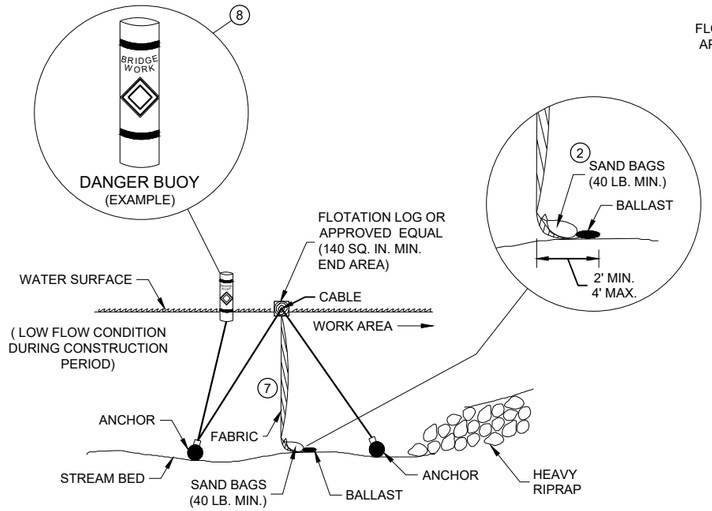
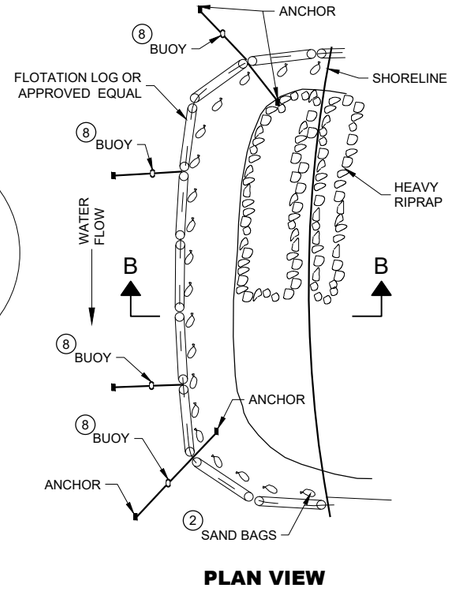
S.D.D. 8 E 10-2

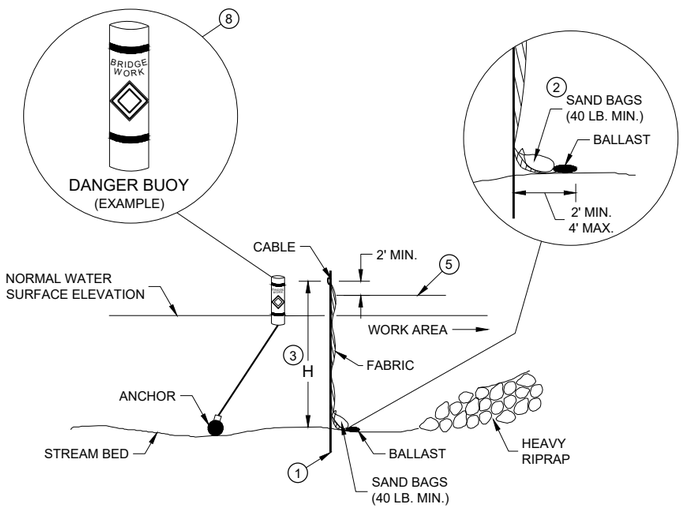


SECTION B - B

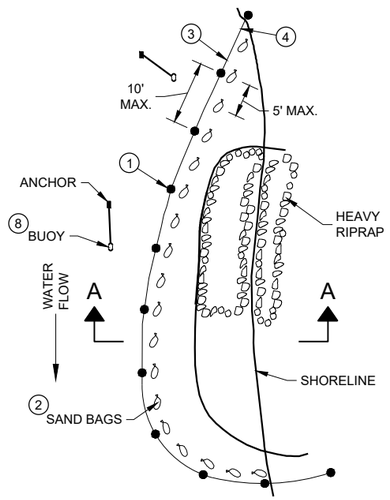


PLAN VIEW

TURBIDITY BARRIER - FLOAT ALTERNATIVE
CAUTION - SEE NOTE 6



SECTION A - A



PLAN VIEW

TURBIDITY BARRIER - STANDARD POST INSTALLATION

TURBIDITY BARRIER PLACEMENT DETAILS

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TURBIDITY BARRIER MAY BE REMOVED AT THE ENGINEERS DISCRETION, WHEN PERMANENT EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

(1) DRIVEN STEEL POSTS, PIPES, OR CHANNELS. LENGTH SHALL BE SUFFICIENT TO SECURELY SUPPORT BARRIER AT HIGH WATER ELEVATIONS.

(2) SAND BAGS TO BE USED AS ADDITIONAL BALLAST WHEN ORDERED BY THE ENGINEER TO MEET ADVERSE FIELD CONDITIONS. SPACE AS APPROPRIATE FOR SITE CONDITIONS.

(3) WHEN BARRIER HEIGHT "H" EXCEEDS 8 FEET, POST SPACING MAY NEED TO BE DECREASED.

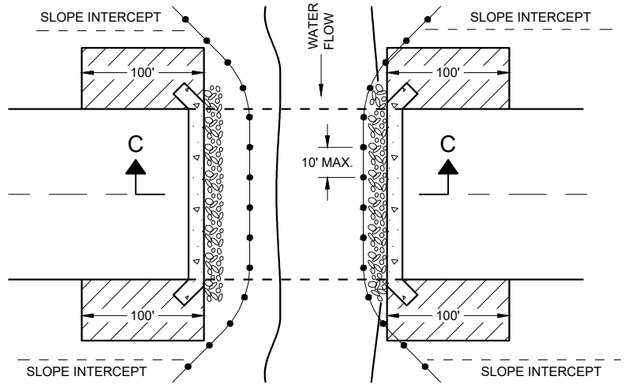
(4) IN WATERWAYS SUBJECT TO FLUCTUATING WATER ELEVATIONS, PROVISIONS SHOULD BE MADE TO ALLOW THE WATER TO EQUALIZE ON EACH SIDE OF THE BARRIER. THIS MAY BE ACCOMPLISHED BY LEAVING A PORTION OF THE BARRIER OPEN ON THE UPSTREAM END.

(5) ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION PERIOD. MINIMUM BARRIER HEIGHT SHALL BE 2' GREATER THAN EITHER THE Q2 ELEVATION OR THE ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION, WHICHEVER IS GREATER.

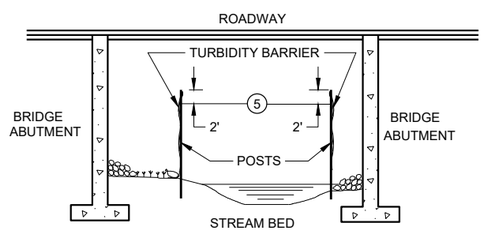
(6) FLOAT ALTERNATIVE WILL ONLY BE ALLOWED WITH WRITTEN APPROVAL OF THE ENGINEER, AND IS MEANT FOR LOCATIONS WHERE BEDROCK PREVENTS THE INSTALLATION OF POSTS.

(7) ALLOW SUFFICIENT SLACK VERTICALLY AND HORIZONTALLY SO THAT SEDIMENT BUILD UP WILL NOT SEPARATE OR LOWER THE TURBIDITY BARRIER.

(8) USE AS DIRECTED BY COAST GUARD OR DNR PERMIT WHEN WORKING IN NAVIGABLE WATERWAYS.



PLAN VIEW



SECTION C - C

TURBIDITY BARRIER DETAIL SHOWING TYPICAL PLACEMENT AT STRUCTURES

TURBIDITY BARRIER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/4/02
DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA

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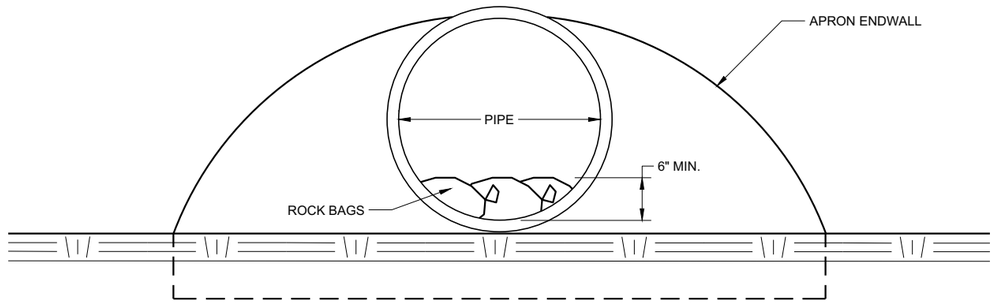
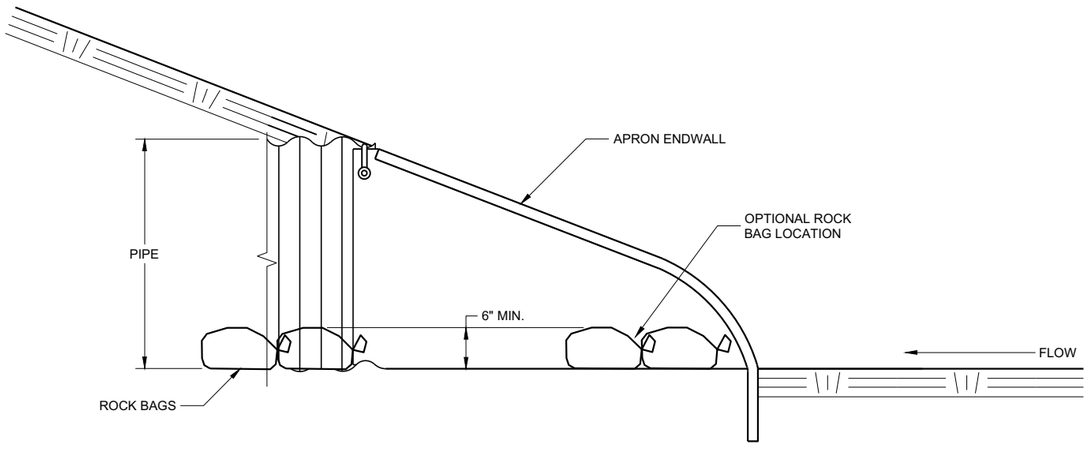
SDD 08E11 - 02



END VIEW



SIDE VIEW

CULVERT PIPE CHECK

(INSTALL ON INLET END ONLY)

CULVERT PIPE CHECK	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2019 DATE	/S/ Daniel Schave EROSION CONTROL ENGI
FHWA	

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SDD 08E15 - 01

METAL APRON ENDWALLS

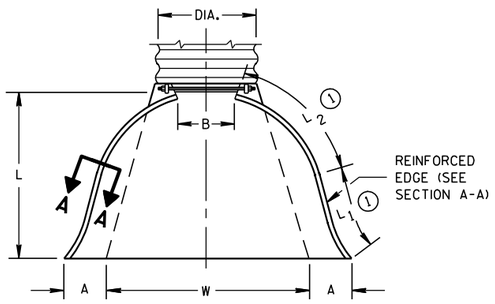
PIPE DIA. (IN.)	MIN. THICK. (Inches) STEEL	MIN. THICK. (Inches) ALUM.	DIMENSIONS (Inches) A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 ①	L2 ①	W (±2")	APPROX. SLOPE	BODY
12	.064	.060	6	6	6	21	12	17 1/2	24	2 1/2 to 1	1 Pc.
15	.064	.060	7	8	6	26	14	21 3/4	30	2 1/2 to 1	1 Pc.
18	.064	.060	8	10	6	31	15	28 1/4	36	2 1/2 to 1	1 Pc.
21	.064	.060	9	12	6	36	18	29 5/8	42	2 1/2 to 1	1 Pc.
24	.064	.075	10	13	6	41	18	37 1/4	48	2 1/2 to 1	1 Pc.
30	.079	.075	12	16	8	51	18	52 1/4	60	2 1/2 to 1	1 Pc.
36	.079	.105	14	19	9	60	24	59 3/4	72	2 1/2 to 1	2 Pc.
42	.109	.105	16	22	11	69	24	75 5/8	84	2 1/2 to 1	2 Pc.
48	.109	.105	18	27	12	78	24	81	90	2 1/4 to 1	3 Pc.
54	.109	.105	18	30	12	84	30	85 1/2	102	2 1/4 to 1	3 Pc.
60	.109×	.105×	18	33	12	87	—	—	114	2 to 1	3 Pc.
66	.109×	.105×	18	36	12	87	—	—	120	2 to 1	3 Pc.
72	.109×	.105×	18	39	12	87	—	—	126	2 to 1	3 Pc.
78	.109×	.105×	18	42	12	87	—	—	132	1 1/2 to 1	3 Pc.
84	.109×	.105×	18	45	12	87	—	—	138	1 1/2 to 1	3 Pc.
90	.109×	.105×	18	37	12	87	—	—	144	1 1/2 to 1	3 Pc.
96	.109×	.105×	18	35	12	87	—	—	150	1 1/2 to 1	3 Pc.

× EXCEPT CENTER PANEL SEE GENERAL NOTES

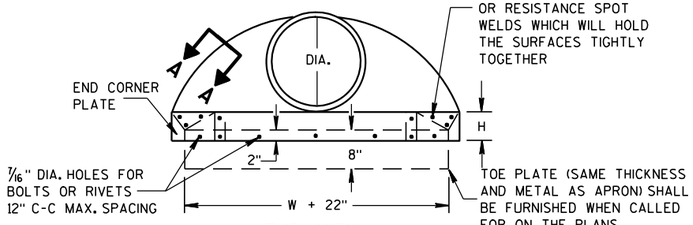
REINFORCED CONCRETE APRON ENDWALLS

PIPE DIA. (IN.)	DIMENSIONS (Inches) T	A	B	C	D	E	G	APPROX. SLOPE
12	2	4	24	48 7/8	72 1/8	24	2	3 to 1
15	2 1/4	6	27	46	73	30	2 1/4	3 to 1
18	2 1/2	9	27	46	73	36	2 1/2	3 to 1
21	2 3/4	9	36	37 1/2	73 1/2	42	2 3/4	3 to 1
24	3	9 1/2	43 1/2	30	73 1/2	48	3	3 to 1
27	3 1/4	10 1/2	49 1/2	24	73 1/2	54	3 1/4	3 to 1
30	3 1/2	12	54	19 3/4	73 1/2	60	3 1/2	3 to 1
36	4	15	63	34 3/4	97 3/4	72	4	3 to 1
42	4 1/2	21	63	35	98	78	4 1/2	3 to 1
48	5	24	72	26	98	84	5	3 to 1
54	5 1/2	27	65	* 33 1/4 - ** 35	* 98 1/4 - ** 100	90	5 1/2	2 2/5 to 1
60	6	* 30 - ** 35	60	39	99	96	5	2 to 1
66	6 1/2	* 24 - ** 30	* 72 - ** 78	* 21 - ** 27	99	102	5 1/2	2 to 1
72	7	* 24 - ** 36	78	21	99	108	6	2 to 1
78	7 1/2	* 24 - ** 36	78	21	99	114	6 1/2	2 to 1
84	8	36	90 1/2	21	111 1/2	120	6 1/2	1 1/2 to 1
90	8 1/2	41	87 1/2	24	111 1/2	132	6 1/2	1 1/2 to 1

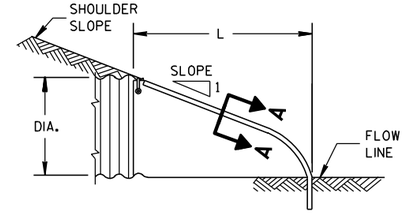
*MINIMUM
**MAXIMUM



PLAN VIEW

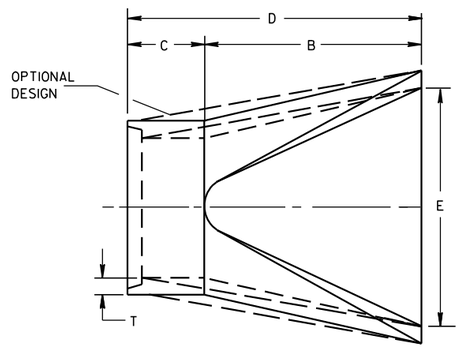


END VIEW

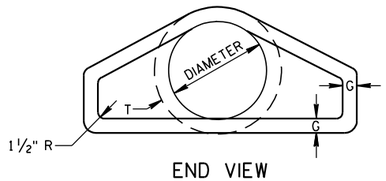


SIDE ELEVATION

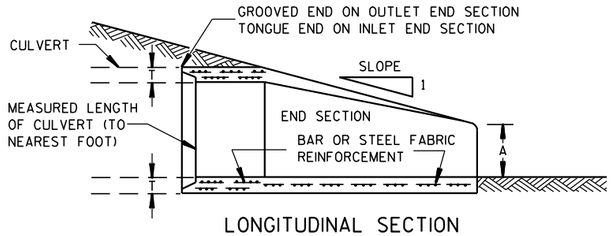
METAL ENDWALLS



PLAN

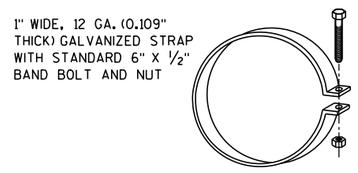


END VIEW



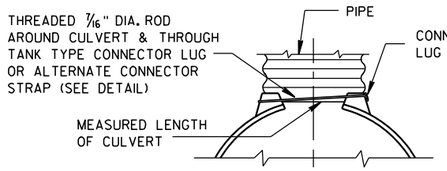
LONGITUDINAL SECTION

CONCRETE ENDWALLS

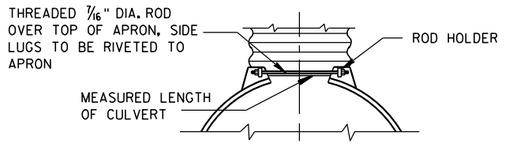


ALTERNATE FOR TYPE 1 CONNECTION

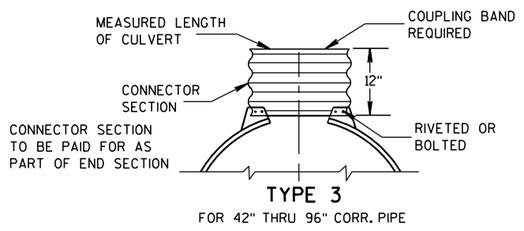
END SECTION CONNECTOR STRAP



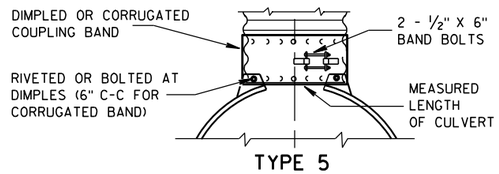
TYPE 1
FOR 12" THRU 24" CORR. PIPE



TYPE 2
FOR 30" THRU 96" CORR. PIPE



TYPE 3
FOR 42" THRU 96" CORR. PIPE



TYPE 5
ALTERNATE FOR:
ALL SIZES CORRUGATED CIRCULAR PIPE

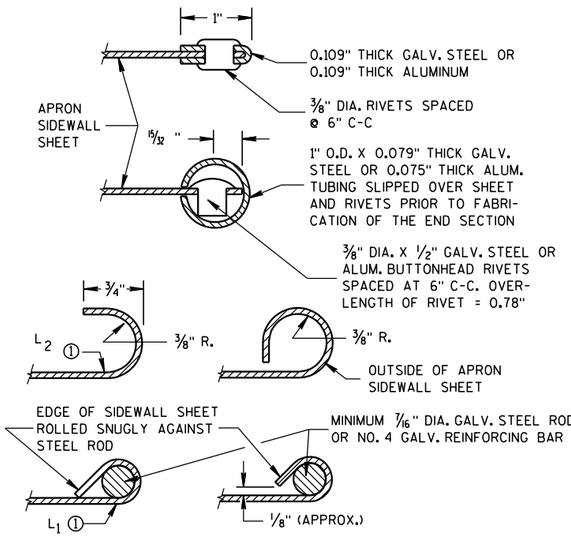
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL. AND CORRUGATED BAND FITS INSIDE ENDWALL. DIMPLED BAND MAY BE USED WITH HELICALLY CORRUGATED PIPE.

FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5 AS APPLICABLE.

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VISE VERSA. GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

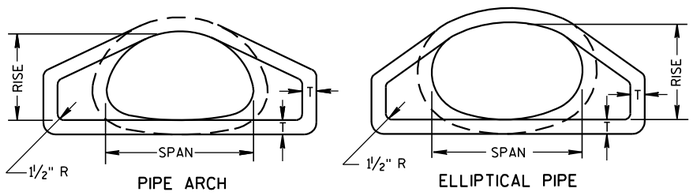
① FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

APRON ENDWALLS FOR CULVERT PIPE

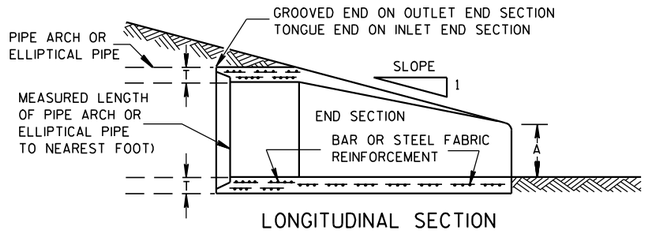
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/30/94 DATE
/S/ Rory L. Rhinesmith
CHIEF ROADWAY DEVELOP 101 NEER
FHWA

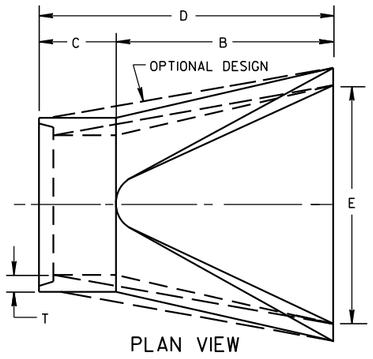
6
S.D.D. 8 F 1-11



END VIEW

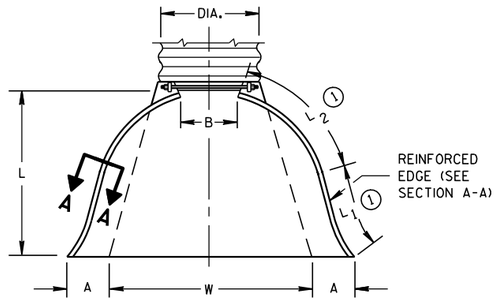


LONGITUDINAL SECTION

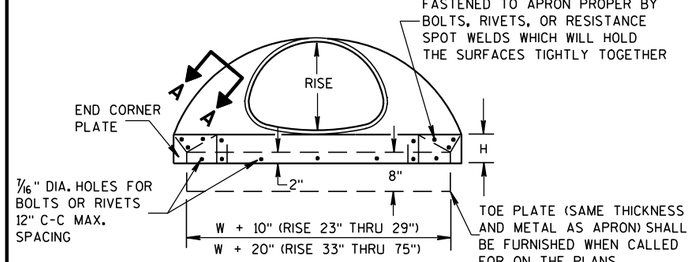


PLAN VIEW

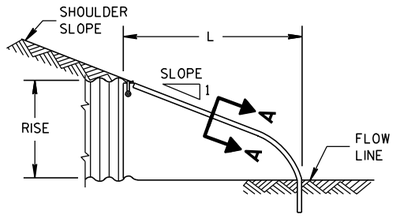
CONCRETE ENDWALLS



PLAN VIEW

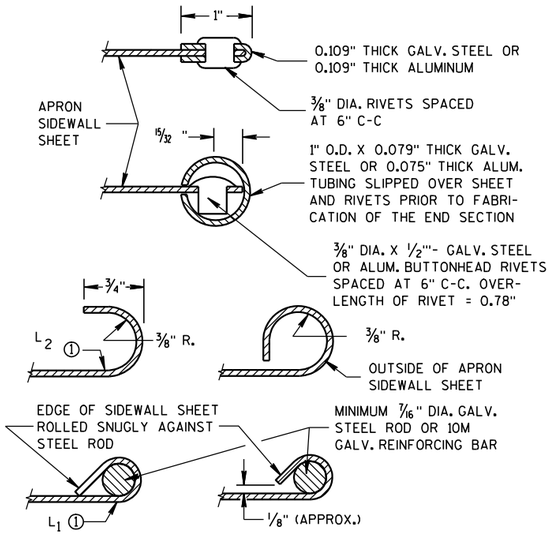


END VIEW



SIDE ELEVATION

METAL ENDWALLS



SECTION A-A

2- 2/3" X 1/2" CORRUGATIONS

EQUIV. DIA. (Inches)	(Inches) SPAN	(Inches) RISE	MIN. THICK. (Inches) STEEL	MIN. THICK. (Inches) ALUM.	DIMENSIONS (Inches) A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 ①	L2 ①	W (±2")	APPROX. SLOPE	BODY
15	17	13	.064	.060	7	9	6	19	14	16	30	2½ to 1	1 Pc.
18	21	15	.064	.060	7	10	6	23	14	19⅜	36	2½ to 1	1 Pc.
21	24	18	.064	.060	8	12	6	28	18	21¾	42	2½ to 1	1 Pc.
24	28	20	.064	.060	9	14	6	32	18	27½	48	2½ to 1	1 Pc.
30	35	24	.079	.075	10	16	6	39	18	37⅝	60	2½ to 1	1 Pc.
36	42	29	.079	.075	12	18	8	46	24	45⅜	75	2½ to 1	1 Pc.
42	49	33	.109	.105	13	21	9	53	24	54¾	85	2½ to 1	2 Pc.
48	57	38	.109	.105	18	26	12	63	24	68	90	2½ to 1	3 Pc.
54	64	43	.109	.105	18	30	12	70	24	72¾	102	2¼ to 1	3 Pc.
60	71	47	.109*	.105*	18	33	12	77	30	82¼	114	2¼ to 1	3 Pc.
66	77	52	.109*	.105*	18	36	12	77	—	—	126	2 to 1	3 Pc.
72	83	57	.109*	.105*	18	39	12	77	—	—	138	2 to 1	3 Pc.

3" X 1" CORRUGATIONS

EQUIV. DIA. (Inches)	(Inches) SPAN	(Inches) RISE	MIN. THICK. (Inches) STEEL	MIN. THICK. (Inches) ALUM.	DIMENSIONS (Inches) A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 ①	L2 ①	W (±2")	APPROX. SLOPE	BODY
48	53	41	.109	.105	18	26	12	63	24	72¾	90	2½ to 1	2 Pc.
54	60	46	.109	.105	18	30	12	70	30	82¼	102	2 to 1	2 Pc.
60	66	51	.109*	.105*	18	33	12	77	—	—	114	1½ to 1	3 Pc.
66	73	55	.109*	.105*	18	36	12	77	—	—	126	1½ to 1	3 Pc.
72	81	59	.109*	.105*	18	39	12	77	—	—	138	2 to 1	3 Pc.
78	87	63	.109*	.105*	22	38	12	77	—	—	148	1½ to 1	3 Pc.
84	95	67	.109*	.105*	22	34	12	77	—	—	162	1½ to 1	3 Pc.
90	103	71	.109*	.105*	22	38	12	77	—	—	174	1½ to 1	3 Pc.
96	112	75	.109*	.105*	24	40	12	77	—	—	174	1½ to 1	3 Pc.

NOTE: ALL SPLICES TO BE LAP RIVETED OR BOLTED.

* EXCEPT CENTER PANEL SEE GENERAL NOTES

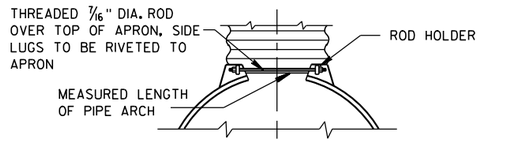
REINFORCED CONCRETE PIPE ARCH

EQUIV. DIA. (Inches)	DIMENSIONS (Inches) ** SPAN	** RISE	T	A	B	C	D	E	APPROX. SLOPE
24	29	18	3	8½	39	33	72	48	3 to 1
30	36	22	3½	9½	50	46	96	60	3 to 1
36	44	27	4	11⅛	60	36	96	72	3 to 1
42	51	31	4½	15 13/16	60	36	96	78	3 to 1
48	58	36	5	21	60	36	96	84	3 to 1
54	65	40	5½	25½	60	36	96	90	3 to 1
60	73	45	6	31	60	36	96	96	3 to 1
72	88	54	7	31	60	39	99	120	2 to 1
84	102	62	8	28½	83	19	102	144	2 to 1

REINFORCED CONCRETE ELLIPTICAL PIPE

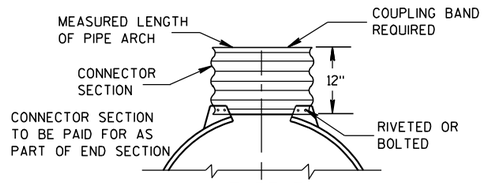
EQUIV. DIA. (Inches)	DIMENSIONS (Inches) ** SPAN	** RISE	T	A	B	C	D	E	APPROX. SLOPE
24	30	19	3¼	8½	39	33	72	48	3 to 1
30	38	24	3¾	9½	54	18	72	60	3 to 1
36	45	29	4½	11⅛	60	24	84	72	2½ to 1
42	53	34	5	15¾	60	36	96	78	2½ to 1
48	60	38	5½	21	60	36	96	84	2½ to 1
54	68	43	6	25½	60	36	96	90	2½ to 1
60	76	48	6½	30	60	36	96	96	2½ to 1

**NOMINAL SIZE



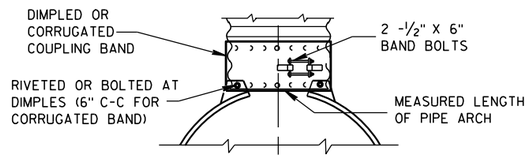
TYPE 2

FOR 17" X 13" THRU 112" X 75" PIPE ARCH



TYPE 3

FOR 64" X 43" THRU 112" X 75" PIPE ARCH



TYPE 5

ALTERNATE FOR:

ALL SIZES CORRUGATED PIPE ARCHES

NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL, AND CORRUGATED BAND FITS INSIDE ENDWALL.

CONNECTION DETAILS

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE APRON ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VISE VERSA. GALVANIZED STEEL OR ALUMINUM APRON ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 66" X 51" PIPE ARCH AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 66" X 51" PIPE ARCH AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE ARCH PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 77" X 52" THROUGH 112" X 75" APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

① FOR PIPE ARCH SIZES UP TO 73" X 55" A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

APRON ENDWALLS FOR PIPE ARCH AND ELLIPTICAL PIPE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

11/30/94 — DATE

/S/ Rory L. Rhinesmith — CHIEF ROADWAY DEVELOPMENT ENGINEER

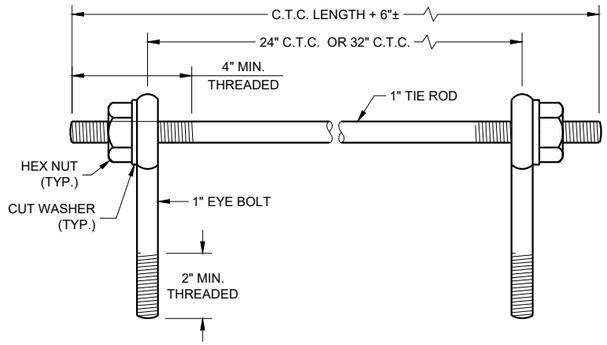
FHWA

6

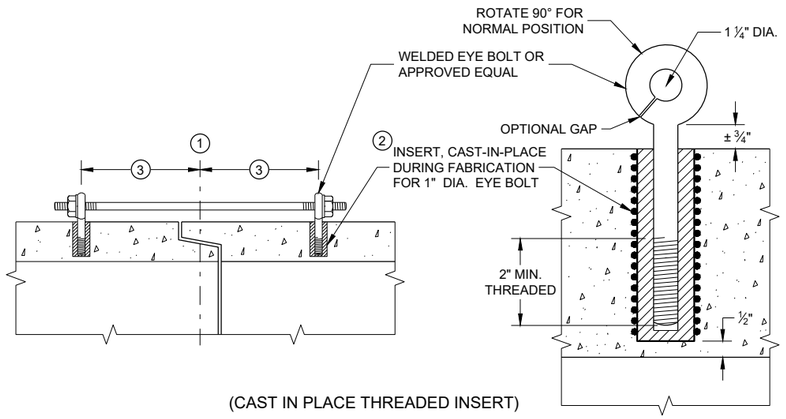
S.D.D. 8 F 2-1

6

S.D.D. 8 F 2-1



EYE BOLTS AND TIE ROD



(CAST IN PLACE THREADED INSERT)

LONGITUDINAL SECTIONS

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

(1) CENTER LINE OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.

(2) THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.

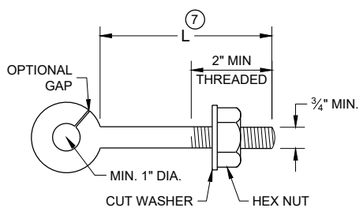
(3) HOLES SHALL BE CAST-IN-PLACE OR DRILLED PER THE APPLICABLE DETAIL, AND EQUAL DISTANCE FROM THE CENTERLINE OF THE JOINT.

(4) BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.

(5) OPENING TO BE ROD DIAMETER PLUS 1 INCH.

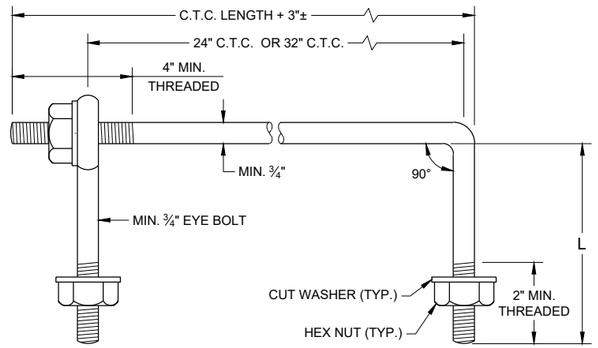
(6) LENGTH ADEQUATE TO EXTEND TO WITHIN ½ INCH OF THE INNER SURFACE OF THE PIPE.

(7) EYE BOLT LENGTH DETERMINED BY WALL THICKNESS, BELL THICKNESS AND BOLT PROJECTION INSIDE PIPE.

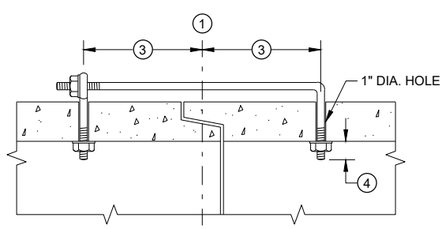


EYE BOLT (7)

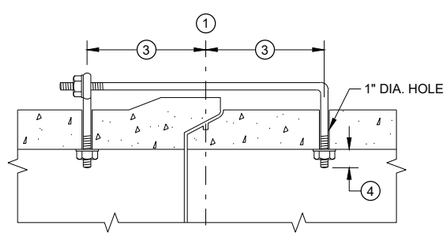
NOTE: TWO EYE BOLTS MAY BE USED WITH A 30" OR 38" LONG THREADED ROD IN LIEU OF THE 90° BENT TIE ROD.



EYE BOLT AND TIE ROD



(TONGUE AND GROOVE PIPE)



(MODIFIED BELL PIPE)

LONGITUDINAL SECTION

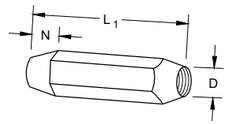
(JOINT TIES FOR 18" TO 66" DIA. CONCRETE PIPE)

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)

ADJUSTABLE TIE ROD TABLE

PIPE DIAMETER	TIE ROD DIAMETER	D	L_1	N
12 - 60	⅝	⅝	5	½
66 - 84	¾	¾	5	½
90 - 144	1	1	7	1 7/16

DIMENSIONS SHOWN ARE IN INCHES



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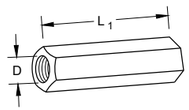
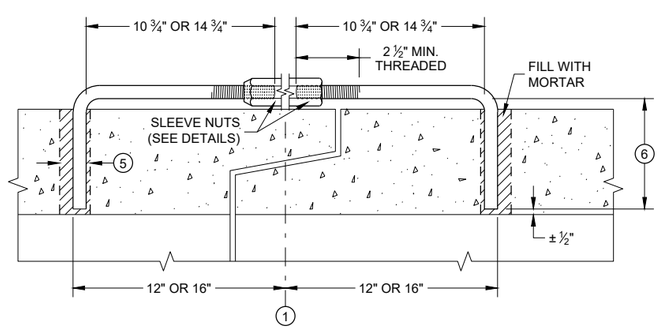


PLAIN

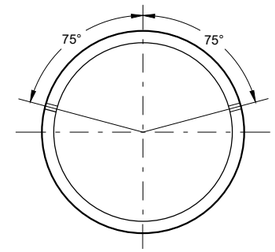
RIGHT AND LEFT THREADS

SLEEVE NUTS



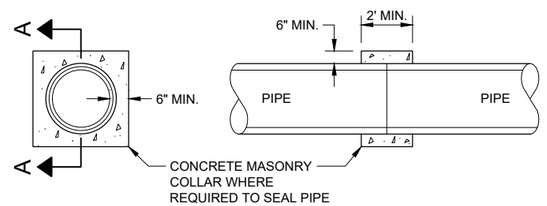
LONGITUDINAL SECTION

ADJUSTABLE TIE ROD (ALTERNATE NO. 3)



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION



SECTION A - A

CONCRETE COLLAR DETAIL

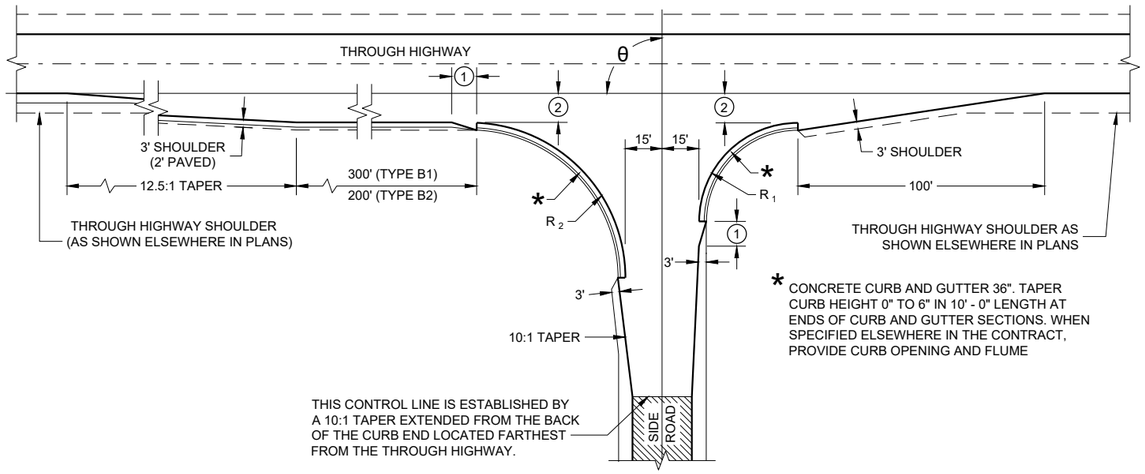
JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2021 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVE ENGINEER
FHWA

SDD 08F04 - 08

SDD 08F04 - 08



* CONCRETE CURB AND GUTTER 36". TAPER CURB HEIGHT 0" TO 6" IN 10' - 0" LENGTH AT ENDS OF CURB AND GUTTER SECTIONS. WHEN SPECIFIED ELSEWHERE IN THE CONTRACT, PROVIDE CURB OPENING AND FLUME

TYPE "B1" AND "B2"

RADII DIMENSIONS FOR TYPES "B1", "B2", "C" AND "D" INTERSECTIONS

θ	R_1	R_2
65 - 70	35	70
71 - 80	40	70
81 - 90	40	60
91 - 100	50	55
101 - 110	60	45

GENERAL NOTES

DESIGNS MAY BE USED INTERCHANGEABLY IN COMBINATION OR SEPARATELY FOR ANY ONE COMPLETE INTERSECTION DEPENDING UPON INTERSECTION ANGLE AND SURFACING OF EACH APPROACH ROADWAY.

SIDE ROAD SURFACING NOTE

WHEN THE SIDE ROAD IS NOT PRESENTLY PAVED, PAVEMENT SHALL BE PLACED TO THE LIMITS SHOWN UNLESS OTHERWISE PROVIDED IN THE CONTRACT. WHERE THE CONSTRUCTION LIMITS ARE BEYOND THE PAVING LIMITS, CRUSHED AGGREGATE SURFACING SHALL BE PLACED BETWEEN THE PAVING LIMITS AND CONSTRUCTION LIMITS.

WHEN THE SIDE ROAD IS PRESENTLY PAVED, NEW PAVEMENT SHALL BE PLACED TO THE LIMITS OF DESIGN AS SHOWN AND BEYOND, IF NECESSARY, TO MEET EXISTING PAVEMENT.

WHEN THE SIDE ROAD IS THE CONSTRUCTION PROJECT, THE INTERSECTION SURFACING SHALL BE THE SAME AS FOR THE PROJECT.

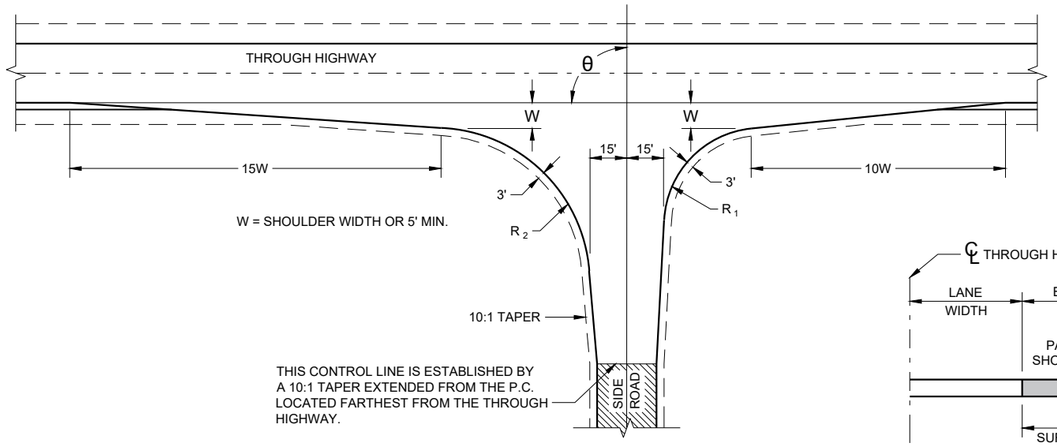
(1) 10-FT TYPICAL.

(2) 12-FT** PLUS ADDITIONAL WIDTH FOR BIKE LANE IF SHOWN ELSEWHERE IN THE PLAN.

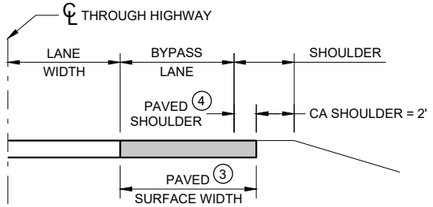
**10-FT MAY BE USED ON TYPE B2 ON RESURFACING PROJECTS IF SPECIFIED IN THE CONTRACT.

(3) BYPASS LANE PAVED SURFACE WIDTH OUTSIDE OF TRAVEL LANE
- ASPHALT = 12-FT PLUS PAVED SHOULDER WIDTH
- PC CONCRETE = 13-FT PLUS PAVED SHOULDER WIDTH

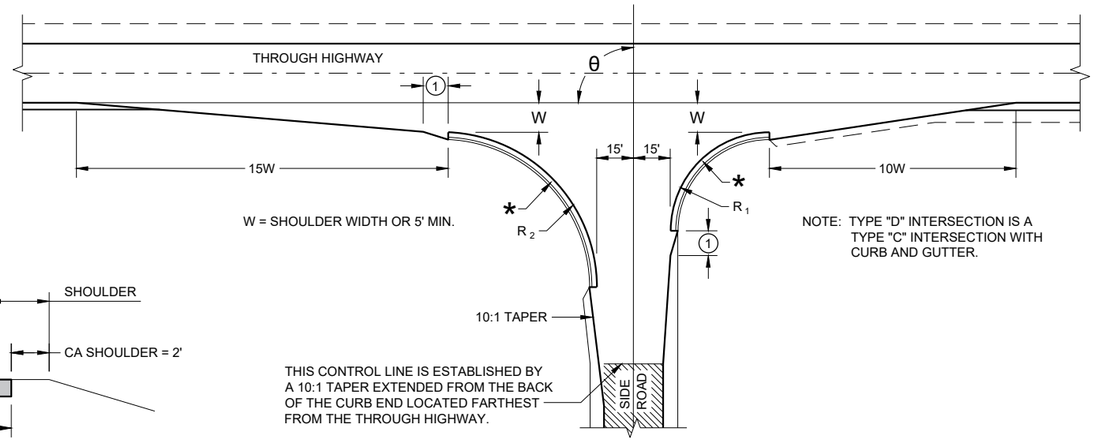
(4) BYPASS LANE PAVED SHOULDER WIDTH = THE GREATER OF 1-FT OR THE PAVED SHOULDER WIDTH OF THE THROUGH HIGHWAY.



TYPE "C"

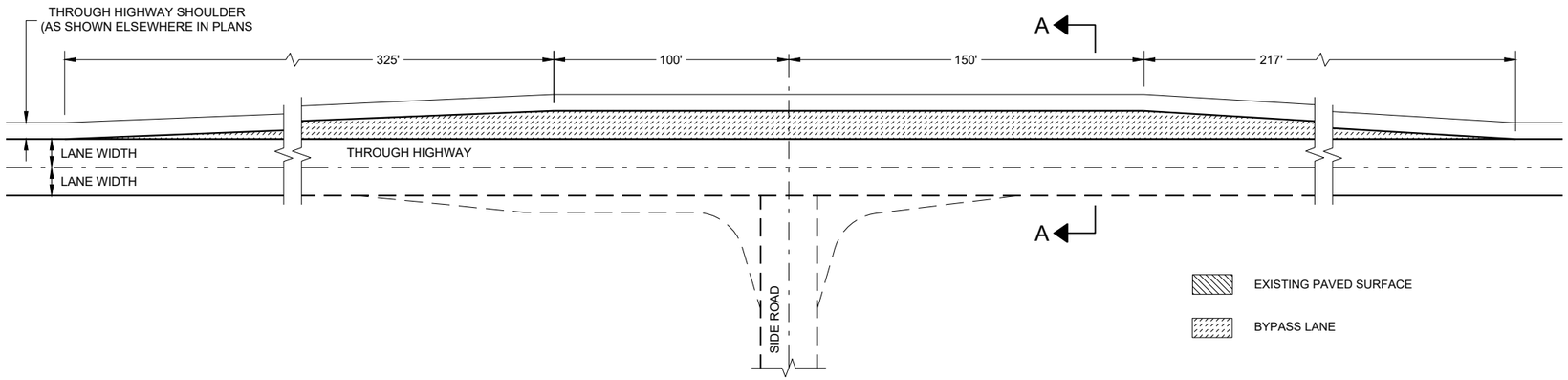


SECTION A - A
(SHOWING BYPASS LANE AND SHOULDER)



NOTE: TYPE "D" INTERSECTION IS A TYPE "C" INTERSECTION WITH CURB AND GUTTER.

TYPE "D"



TEE INTERSECTION BYPASS LANE DETAIL

AT GRADE SIDE ROAD INTERSECTION TYPES "B1", "B2", "C", "D" AND TEE INTERSECTION BYPASS LANE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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SDD 09A01 - 14a

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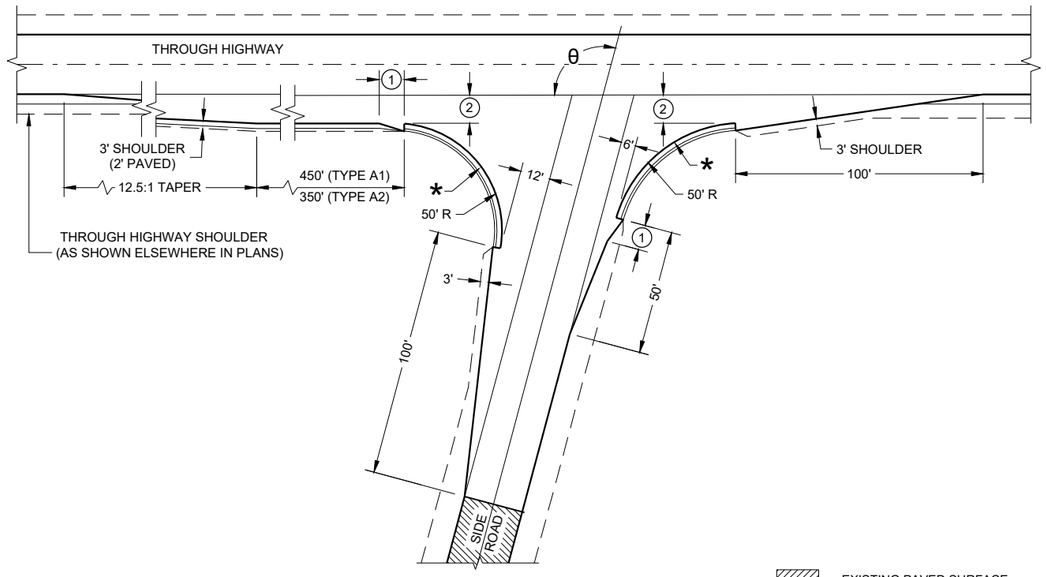
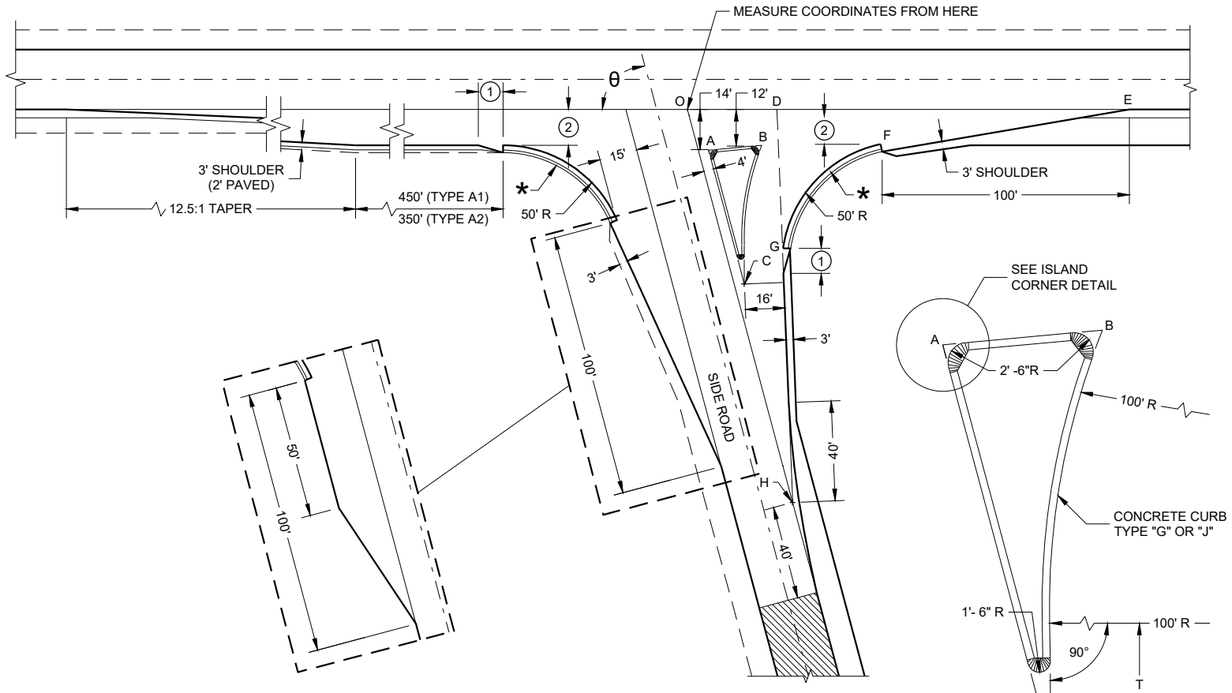
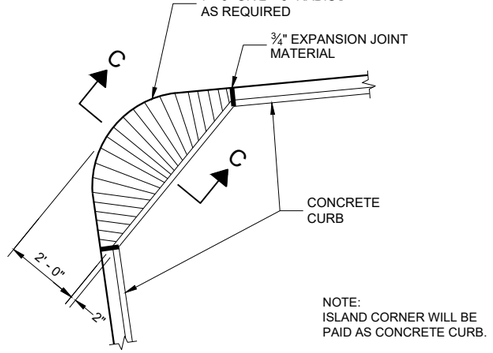


θ = MORE THAN 80°



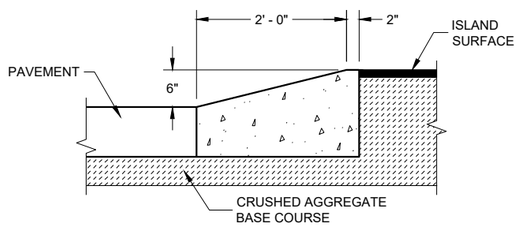
SIDE ROAD WIDENING AND TAPER REQUIRED WHERE THE THROUGH HIGHWAY CARRIES TWO-WAY TRAFFIC
θ = ACUTE ANGLES 70° OR LESS

θ = ACUTE ANGLES 80° OR LESS



PLAN VIEW

NOTE:
ISLAND CORNER WILL BE
PAID AS CONCRETE CURB.



SECTION C - C

ISLAND CORNER DETAIL

(TO BE CONSTUCTED AT ALL ISLAND CORNERS)

TABLE OF DIMENSIONS FOR VARIABLE SIDE ROAD INTERSECTION ANGLES

(INTERPOLATE VALUES FOR ANGLES NOT SHOWN)

ANGLE θ DEGREES	COORDINATES IN FEET (MEASURED FROM POINT 'O') X / Y								LENGTH IN FEET				
	A	B	C	D	E	F	G	H	AB	AC	T	OJ	OH
60	12.7 / -14.0	44.9 / -12.0	46.4 / -72.4	41.9 / 0.0	205.0 / 0.0	104.6 / -12.0	64.0 / -75.5	85.0 / -147.1	32.3	67.4	4.9	85.9	169.9
65	10.9 / -14.0	39.0 / -12.0	37.8 / -71.6	39.4 / 0.0	196.1 / 0.0	95.7 / -12.0	54.1 / -71.5	70.5 / -151.3	28.2	63.6	8.5	80.9	166.9
70	9.4 / -14.0	33.9 / -12.0	29.8 / -70.1	37.4 / 0.0	188.3 / 0.0	87.8 / -12.0	45.6 / -67.5	56.1 / -154.2	24.6	59.7	11.5	76.1	164.1
75	7.9 / -14.0	29.3 / -12.0	22.3 / -67.9	35.7 / 0.0	181.2 / 0.0	80.7 / -12.0	38.2 / -63.4	41.8 / -155.9	21.5	55.8	13.8	71.4	161.4
80	6.5 / -14.0	25.4 / -12.0	15.6 / -65.2	34.4 / 0.0	174.8 / 0.0	74.4 / -12.0	31.8 / -59.3	27.6 / -156.5	18.9	52.0	15.6	66.9	158.9

TYPE 'A1" AND "A2" SIDE ROAD INTERSECTION DETAILS

AT GRADE SIDE ROAD INTERSECTIONS TYPES "A1" AND "A2"

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022 — DATE
/S/ John Jenkins — ROADWAY STANDARDS DEVE ENGINEER

FHWA

SDD 09A01 - 14b

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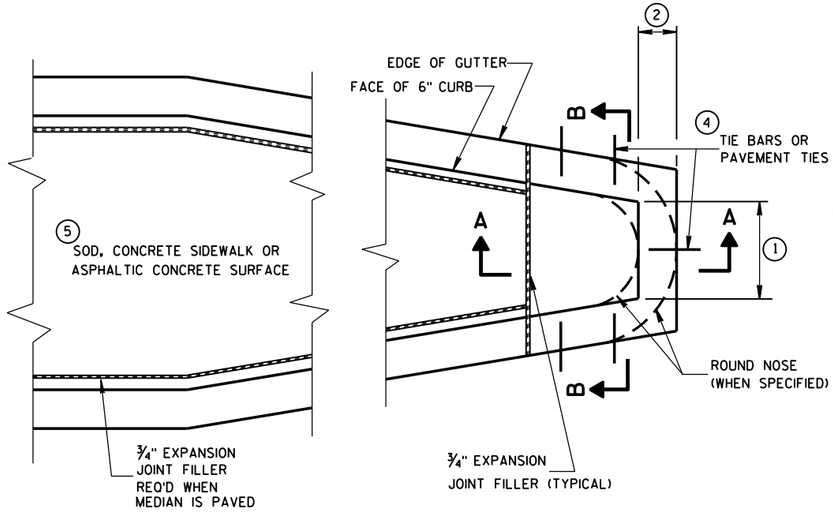
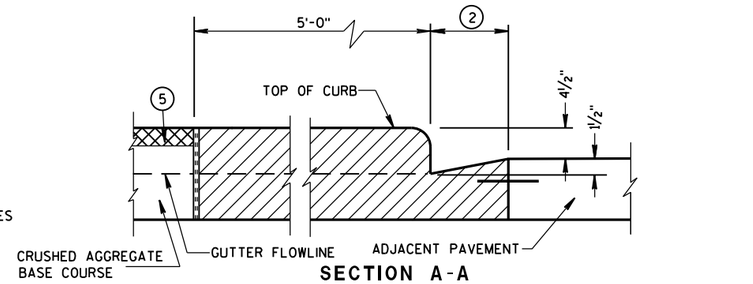
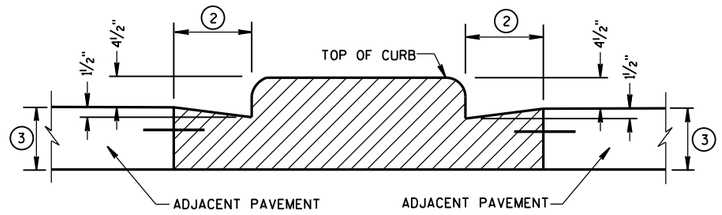
SDD 09A01 - 14b





SECTION A-A



SECTION B-B

CONCRETE MEDIAN BLUNT NOSE DETAIL

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

(1) SEE PLAN FOR MEDIAN NOSE WIDTH AND RADIUS (FOR ROUND NOSE ALTERNATE).

(2) WIDTH OF GUTTER TO MATCH EXISTING ADJACENT GUTTER OR AS SPECIFIED ELSEWHERE IN THE PLAN.

(3) DEPTH EQUAL TO ADJACENT PAVEMENT. ADJACENT PAVEMENT STRUCTURE DETAILS ARE SHOWN ON THE PLAN. TYPICAL OPTIONS ARE:

(1) NEW OR EXISTING CONCRETE PAVEMENT.

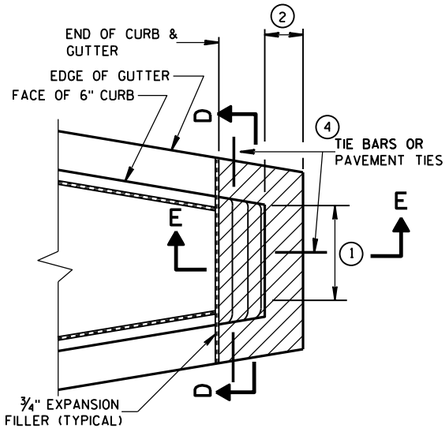
(2) ASPHALTIC CONCRETE PAVEMENT OVER NEW OR EXISTING CONCRETE BASE COURSE.

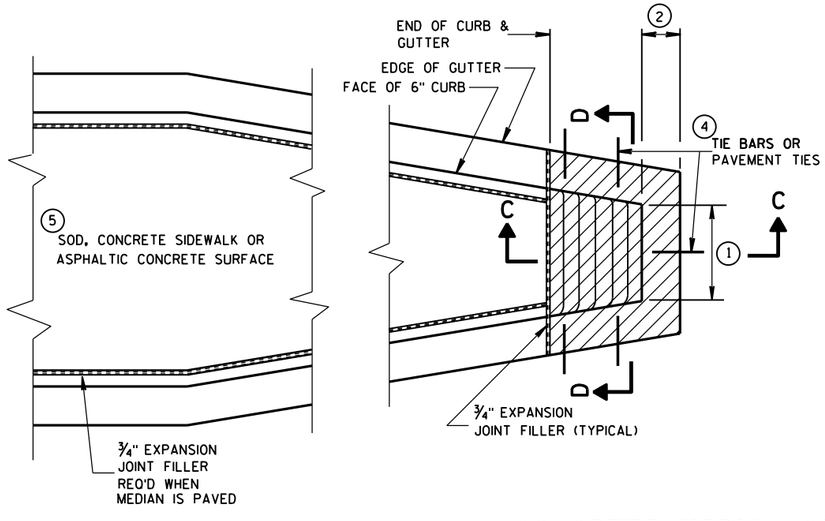
(3) ASPHALTIC CONCRETE PAVEMENT OVER CRUSHED AGGREGATE BASE COURSE.

(4) TIE BARS OR PAVEMENT TIES REQUIRED IN NEW CONCRETE PAVEMENT OR CONCRETE BASE COURSE. TIE BARS SHALL BE NO. 4 X 2'-0" SPACED AT 2'-0" C-C.

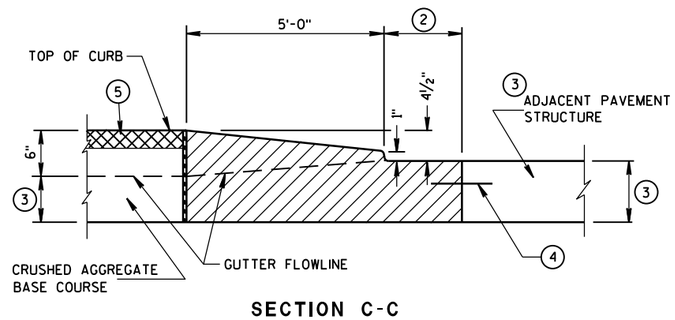
PAVEMENT TIES REQUIRED IN EXISTING CONCRETE BASE COURSE. PAVEMENT TIES SHALL BE NO. 6 X 1'-0" SPACED AT 3'-0" C-C INSTALLED ON A HORIZONTAL SKEW OF 6:1. THE DIRECTION OF SKEW SHALL ALTERNATE AFTER EVERY ONE OR TWO BARS.

(5) SURFACE TYPE AND DETAILS ARE SHOWN ELSEWHERE IN THE PLAN.

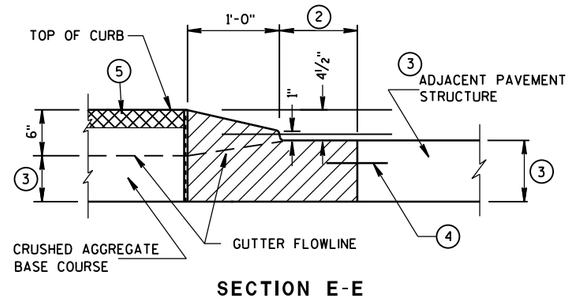




CONCRETE MEDIAN SLOPED NOSE TYPE 1

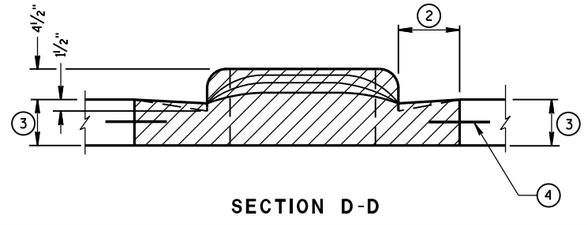


SECTION C-C



SECTION E-E

CONCRETE MEDIAN SLOPED NOSE TYPE 2



SECTION D-D

CONCRETE MEDIAN NOSE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/8/2006 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS D ENGINEER
FHWA

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S.D.D. 11 B 2-2

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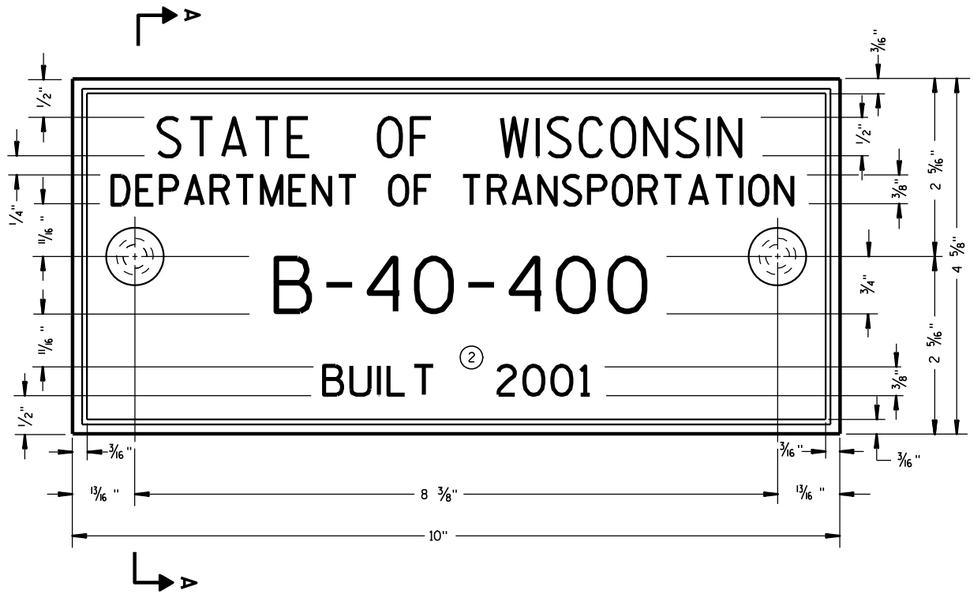
S.D.D. 11 B 2-2



TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)

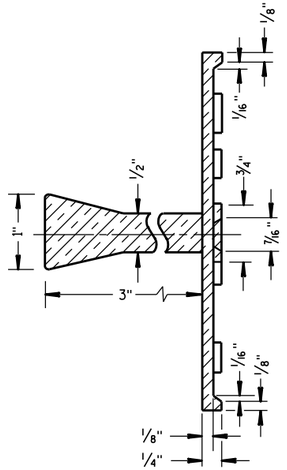
GENERAL NOTES

NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

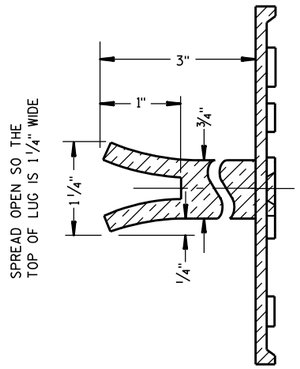
THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

(1) EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.

(2) REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.



SECTION A-A



ALTERNATE LUG

FOR MULTI-UNIT STRUCTURES LINE 3 ABOVE SHALL READ

B = BRIDGE
C = CULVERT
R = RETAINING WALL

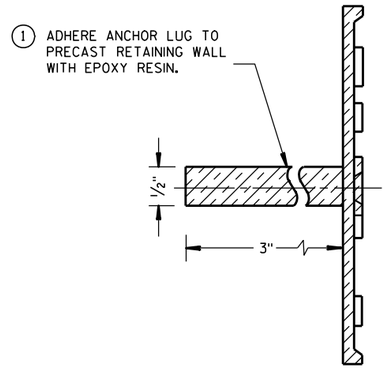
COUNTY NO.

BRIDGE NO.

UNIT NO. FOR MULTIPLE UNIT BRIDGE

B-40-400-1A

NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES



ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

NAME PLATE (STRUCTURES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

3/26/10 DATE

/S/ Scot Beck-- CHIEF STRUCTURAL DEVELOP ENGINEER

FHWA

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S.D.D. 12 A 3-10

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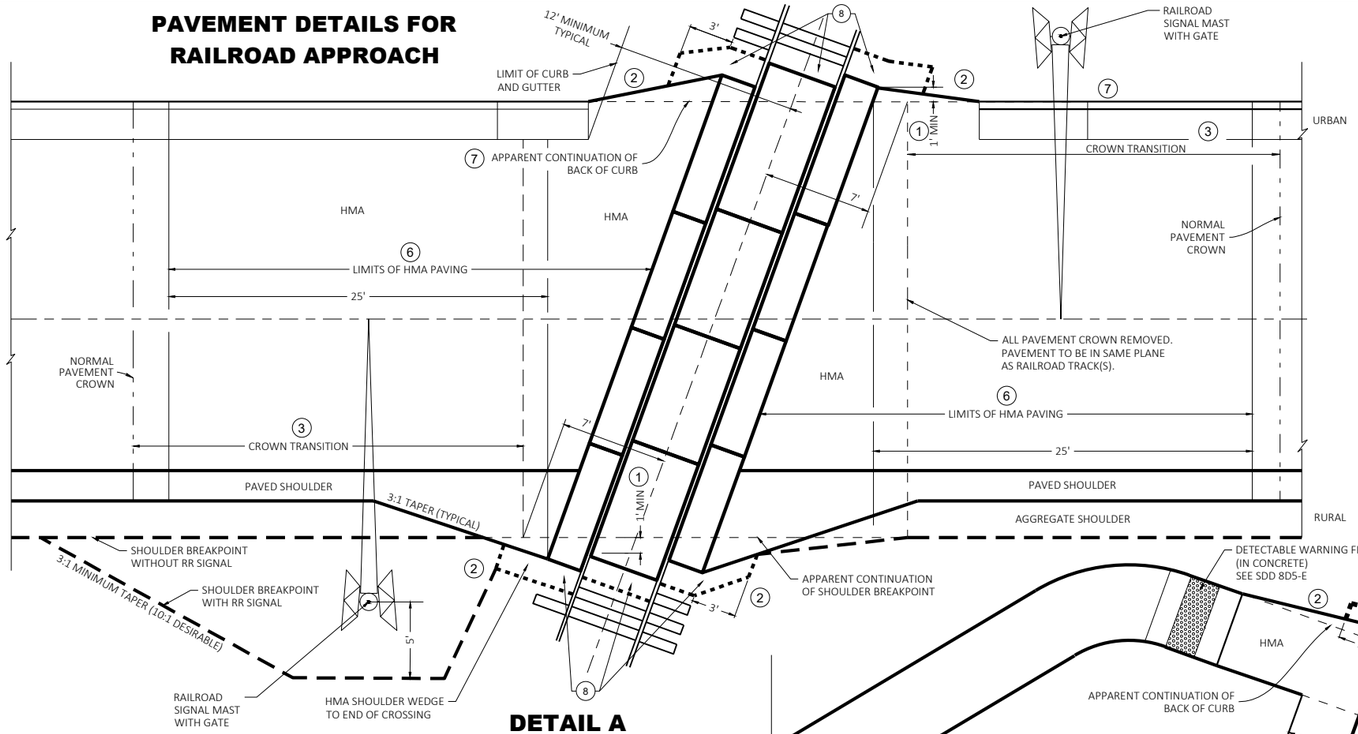
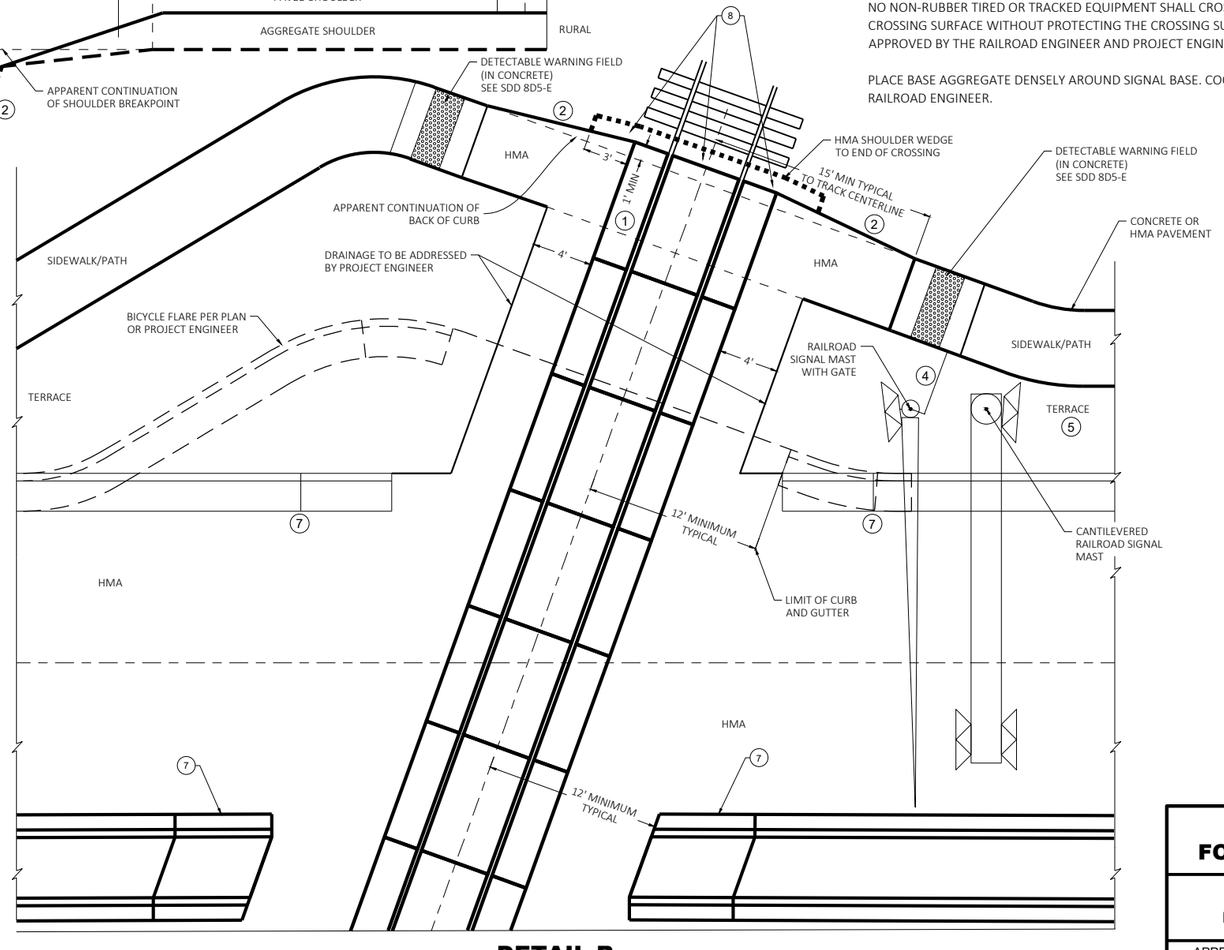
S.D.D. 12 A 3-10

PAVEMENT DETAILS FOR RAILROAD APPROACH



DETAIL A
RAILROAD APPROACH



DETAIL B
MEDIAN AND SIDEWALK/SHARED-USE PATH APPROACH

GENERAL NOTES

PLANS AND SECTIONS ARE TYPICAL. DIMENSIONS VARY PER PROJECT.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, PROJECT PLANS, AND THE APPLICABLE SPECIAL PROVISIONS.

CROSSING SURFACE MATERIAL, RAILS, TIES, BALLAST, AND CROSSING DRAINAGE SYSTEM BY OTHERS UNLESS DIRECTED OTHERWISE. IF THE FINAL GRADES DON'T MATCH TO THE PLAN GRADES THEN GRADE ADJUSTMENTS WILL BE NECESSARY. CONFIRM NEW GRADES WITH PROJECT ENGINEER.

HMA PAVEMENT APPROACHES, HMA PAVEMENT CROSSING SURFACES, AND HMA FLANGEWAY/FIELD FILLERS TO BE REPLACED BY ROADWAY CONTRACTOR UNLESS DIRECTED OTHERWISE BY THE PLANS, SPECIAL PROVISIONS, RAILROAD ENGINEER, OR PROJECT ENGINEER.

HMA PAVEMENT SHALL BE ROLLED PARALLEL TO THE TRACK.

WHEN THERE IS A SIDEWALK OR SHARED-USE PATH, ADD DETECTABLE WARNING FIELDS PER CURRENT STANDARD DETAIL DRAWING 8D5-E.

THE CROSSING SHALL NOT BE OPENED TO ANY TYPE OF TRAFFIC UNTIL IT IS FULLY PAVED AND COOLED SUFFICIENTLY UNLESS OTHERWISE APPROVED BY THE RAILROAD ENGINEER AND THE PROJECT ENGINEER.

NO NON-RUBBER TIRED OR TRACKED EQUIPMENT SHALL CROSS OR SIT ON THE CROSSING SURFACE WITHOUT PROTECTING THE CROSSING SURFACE WITH A METHOD APPROVED BY THE RAILROAD ENGINEER AND PROJECT ENGINEER.

PLACE BASE AGGREGATE DENSELY AROUND SIGNAL BASE. COORDINATE WITH THE RAILROAD ENGINEER.

GENERAL NOTES CONTINUED

(1) 1' MINIMUM CROSSING SURFACE COVERAGE PAST THE APPARENT CONTINUATION OF SHOULDER BREAKPOINT, BACK OF CURB, OR OUTSIDE EDGE OF SIDEWALK/PATH. INDIVIDUAL RAILROADS MAY HAVE DIFFERENT MINIMUM STANDARDS.

(2) HMA FLARE FROM OUTSIDE EDGE OF SIDEWALK/PATH, BACK OF CURB, OR AGGREGATE SHOULDER BREAKPOINT TO THE END OF CROSSING SURFACE MATERIAL.

(3) CROWN TRANSITION LENGTH SHOWN ELSEWHERE IN THE PLAN.

(4) NEAR EDGE OF PATH TO THE CENTER OF SIGNAL OR GATE MAST SHOULD BE A MINIMUM OF 5'-0". FOR SIDEWALK, THE NEAR EDGE SHOULD BE A MINIMUM OF 3'-0" TO THE CENTER OF SIGNAL OR GATE. NEAR EDGE OF SIDEWALK TO A NON-GATED MAST OR CANTILEVER SHOULD BE A MINIMUM OF 2'-6". SEE PLAN FOR RAILROAD SIGNAL AND GATE LOCATION IF THEY ARE NOT ALREADY INSTALLED.

(5) TERRACE WIDTH VARIES. SEE PLAN FOR RAILROAD SIGNAL AND GATE LOCATIONS. PER PLAN OR PROJECT ENGINEER THE TERRACE AND SIDEWALK/PATH GRADES SHALL BE TRANSITIONED TO MATCH THE GRADE OF THE TRACK. FIELD FIT TO AVOID PONDING.

(6) 25' MINIMUM HMA PAVING MEASURED PARALLEL TO THE ROAD OR 10' MINIMUM MEASURED PERPENDICULAR TO THE TRACK FROM THE EDGE OF THE CROSSING SURFACE, WHICHEVER IS GREATER.

(7) REFERENCE SDD 8-D-01 END SECTION CURB AND GUTTER. MEDIAN END NEAR THE TRACK SHOULD BE PARALLEL TO THE TRACK. 6'-0" TAPER FOR A MEDIAN SHOULD BE REDUCED TO GET FULL HEIGHT CURB WHERE THE GATE COMES DOWN. DESIGN OPTION TO POUR MEDIAN TAPER IN ONE PIECE. BUILD PER PLAN UNLESS OTHERWISE APPROVED BY THE RAILROAD ENGINEER AND THE PROJECT ENGINEER.

(8) IF METAL END PLATES ARE NOT INSTALLED BY THE RAILROAD THEN HMA PAVEMENT WEDGE SHALL BE PLACED AT THE END OF THE LAST PANEL TAPERED TO BACK EDGE OF NEXT TIE AND THOROUGHLY COMPACTED. SEE DETAIL G.

PAVEMENT DETAILS FOR RAILROAD APPROACH

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE

/S/ Kristen Sommers
STATE RAILROAD ENGINEER AND SAFETY SUPERVISO

FHWA

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SDD 13B01-11a

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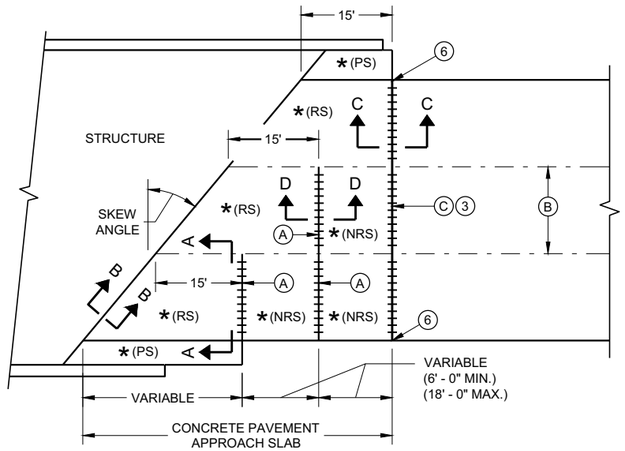
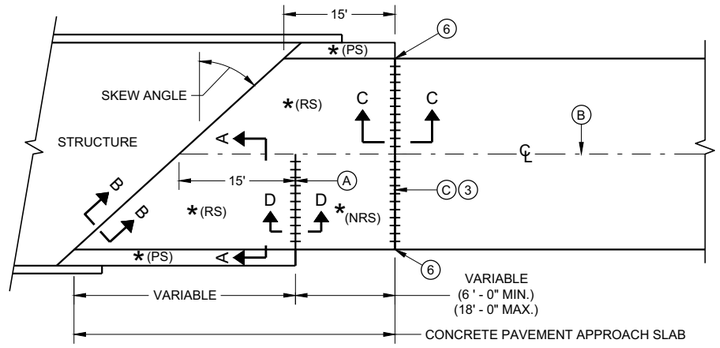
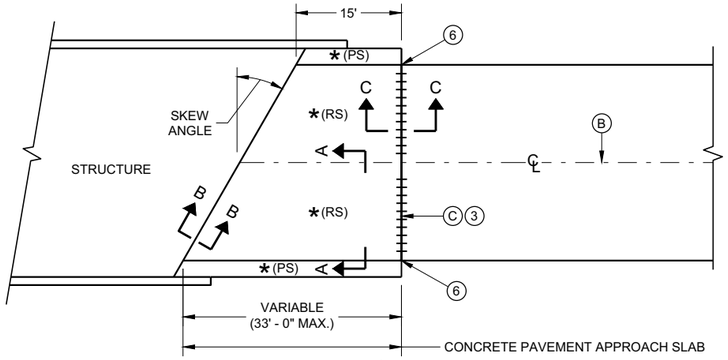
SDD 13B01-11a



SKEWED APPROACH
(PAVEMENT MORE THAN TWO LANES)



SKEWS > 20°
(PAVEMENT WIDTH ≤ 30')



SKEWS ≤ 20°
(PAVEMENT WIDTH ≤ 30')

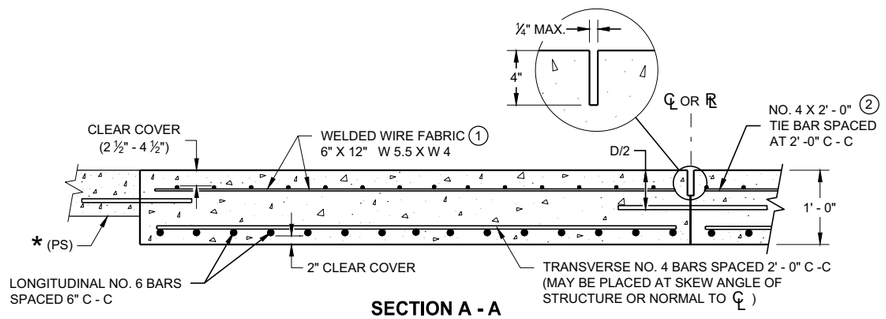
APPROACH SLAB AND ADJACENT PAVEMENT

* (RS) = REINFORCED CONCRETE SLAB

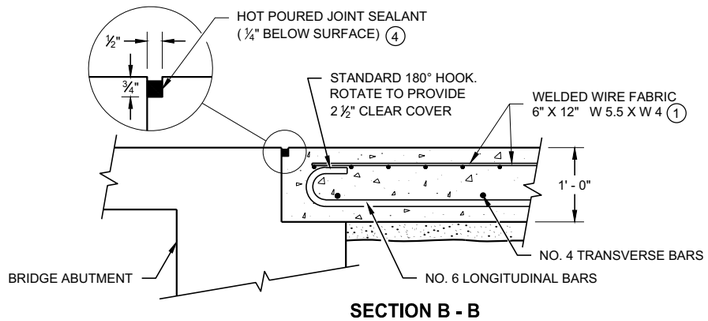
* (PS) = PAVED CONCRETE SHOULDER OR CONCRETE DRAINAGE SLAB

* (NRS) - NON - REINFORCED CONCRETE SLAB

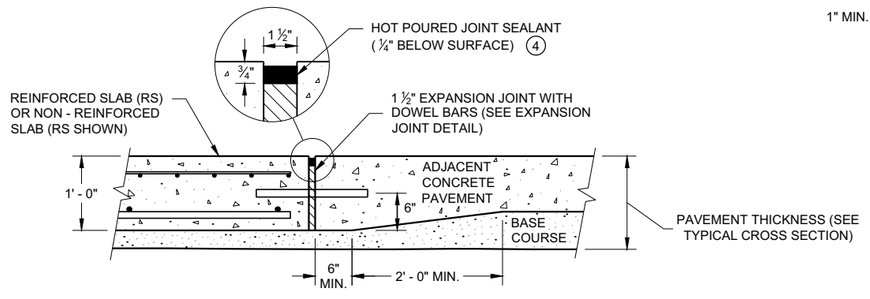
*** STANDARD DOWEL BAR DIAMETER (SEE SDD 13C11 AND SDD 13C13)



SECTION A - A
REINFORCEMENT POSITIONING DETAIL



SECTION B - B
BEND DETAIL
BOTTOM REINFORCEMENT



SECTION C - C
TRANSITION DETAIL
APPROACH SLAB TO ADJACENT PAVEMENT

GENERAL NOTES

THE CONTRACTOR MAY SPLICE NO. 6 BARS IN THE APPROACH SLAB FOR SKEWED STRUCTURES ONLY. STAGGER SPLICES WITH A MAXIMUM OF ONE SPLICE PER BAR. THE LENGTH OF LAP IS 20 INCHES.

TACK WELD DOWEL BARS TO THE BASKETS ON ALTERNATE ENDS.

(1) THE CONTRACTOR MAY USE NO. 4 BARS SPACED AT 2' - 0 " C - C IN BOTH THE LONGITUDINAL AND TRANSVERSE DIRECTIONS FOR TOP REINFORCEMENT AS AN ALTERNATIVE TO THE WELDED WIRE FABRIC.

(2) THE CONTRACTOR MAY OMIT THE BARS BETWEEN REINFORCED SLABS WHERE SLAB REINFORCEMENT BARS EXTEND ACROSS THE CENTERLINE OR REFERENCE LINE.

(3) DO NOT CONSTRUCT AN EXPANSION JOINT OR INSTALL DOWEL BARS WHEN ABUTTING AN HMA PAVEMENT.

(4) USE A JOINT SEALANT CONFORMING TO STANDARD SPECIFICATION 415.2.6.

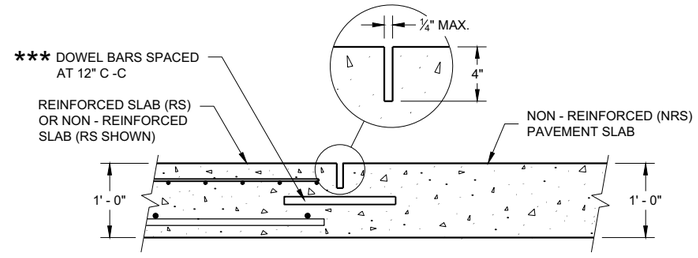
(5) PLACE EXPANSION CAP ON THE END OF THE DOWEL THAT IS NOT TACK WELDED TO THE BASKET. DO NOT FORCE DOWEL BAR PAST THE DOWEL STOP.

(6) EXTEND EXPANSION JOINT THROUGH ANY ADJACENT TIED CONCRETE.

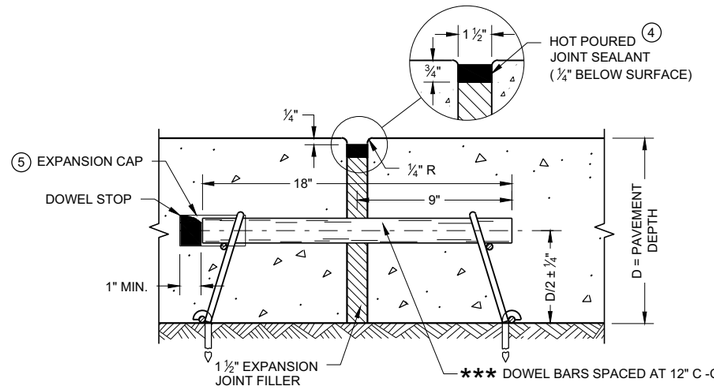
(A) STANDARD CONTRACTION JOINT NORMAL TO ℄ OR ℞.

(B) STANDARD LONGITUDINAL JOINT WITH TIE BARS.

(C) 1 ½" EXPANSION JOINT WITH DOWEL BARS NORMAL TO ℄ OR ℞ .



SECTION D - D
CONTRACTION JOINT



EXPANSION JOINT DETAIL

CONCRETE PAVEMENT APPROACH SLAB

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018
DATE

/S/ Peter Kemp, P.E.
PAVEMENT SUPERVIS

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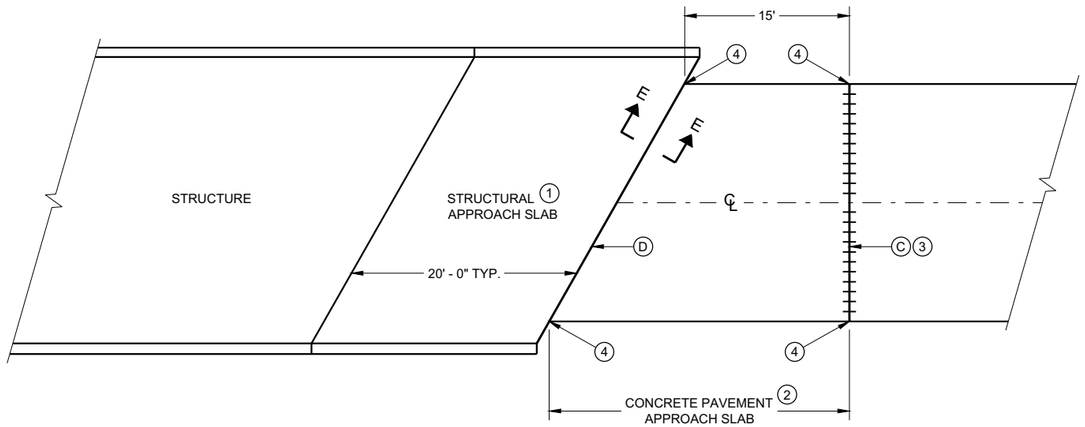
SDD 13B02 - 09a



BRIDGE APPROACHES

GENERAL NOTES

ALL PROJECTS THAT INVOLVE A STRUCTURAL APPROACH SLAB WILL ALSO HAVE A CONCRETE PAVEMENT APPROACH SLAB.

(1) SEE BRIDGE PLAN.

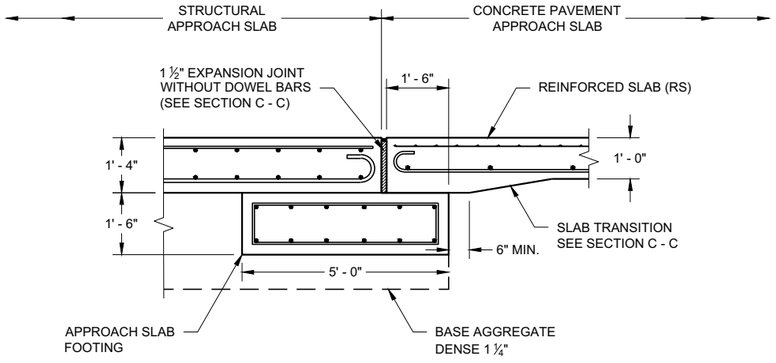
(2) CONFORM TO SDD 13B02 SHEET A FOR CONCRETE PAVEMENT APPROACH SLAB DETAILS

(3) DO NOT CONSTRUCT AN EXPANSION JOINT OR INSTALL DOWEL BARS WHEN ABUTTING AN HMA PAVEMENT.

(4) EXTEND EXPANSION JOINT THROUGH ANY ADJACENT TIED CONCRETE.

(C) 1 ½" EXPANSION JOINT WITH DOWEL BARS NORMAL TO ℄ OR ℞.

(D) 1 ½" EXPANSION JOINT (NO DOWELS)



SECTION E - E
FOOTING DETAIL
STRUCTURAL APPROACH SLAB TO CONCRETE BRIDGE APPROACH

STRUCTURAL APPROACH SLAB AND CONCRETE PAVEMENT APPROACH SLAB

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018
DATE
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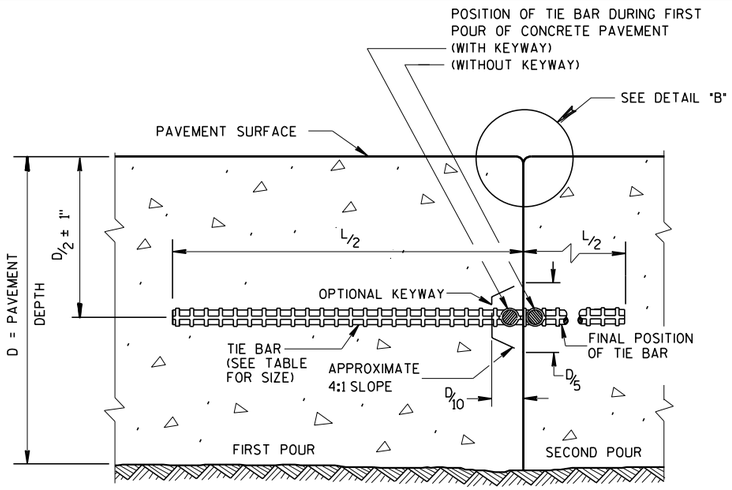
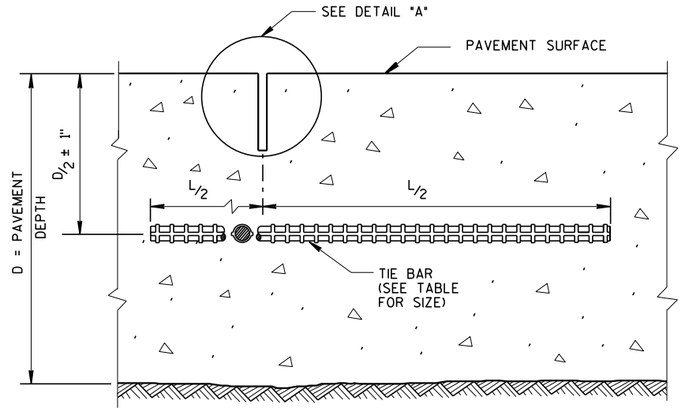
SDD 13B02 - 09b



CONSTRUCTION JOINT



SAWED JOINT

GENERAL NOTES

CREATE A LONGITUDINAL JOINT FOR PAVEMENT WIDTHS GREATER THAN 15 FEET.

CORRELATE LONGITUDINAL JOINTS WITH LANE LINES WHEN POSSIBLE.

(1) ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.

(2) PAVEMENT THAT WAS IN PLACE PRIOR TO THE CONTRACT.

PLAN VIEW

SECTION A-A

LONGITUDINAL CONSTRUCTION JOINT TIE BARS ANCHORED INTO EXISTING PAVEMENT

DETAIL "A"

DETAIL "B"

DETAIL "C"

TIE BAR TABLE

PAVEMENT DEPTH (D)	TIE BAR SIZE	TIE BAR LENGTH (L)	MAX. TIE BAR SPACING
< 10 1/2"	NO. 4	30"	36"
≥ 10 1/2"	NO. 5	36"	36"
	NO. 4 *	30"	24" **

* SUBSTITUTE BENT BARS AT LONGITUDINAL JOINTS WHEN EQUIPMENT LIMITATIONS DURING CONSTRUCTION WARRANT (e.g. AUXILIARY LANES OR TURN LANES)

** CONFORM TO 15" MINUMUM SPACING FROM TRANSVERSE JOINTS; SPACING BETWEEN TIE BARS WILL BE 30" AT TRANSVERSE JOINTS.

PLAN VIEW
SHOWING LOCATION OF TIE BARS

CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018 — DATE
/S/ Peter Kemp PE — PAVEMENT SUPE
FHWA

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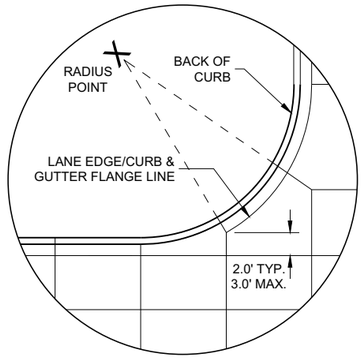
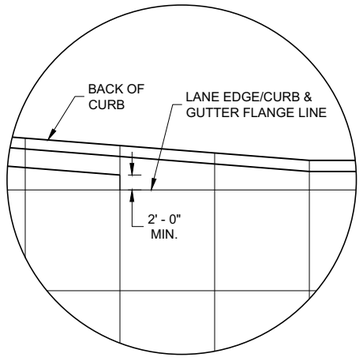
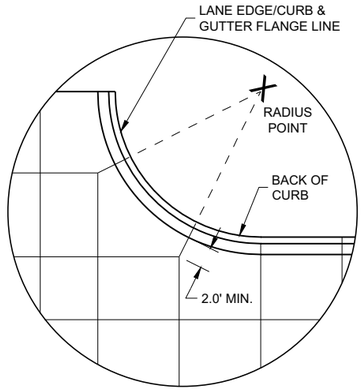
S.D.D. 13 C 1-19



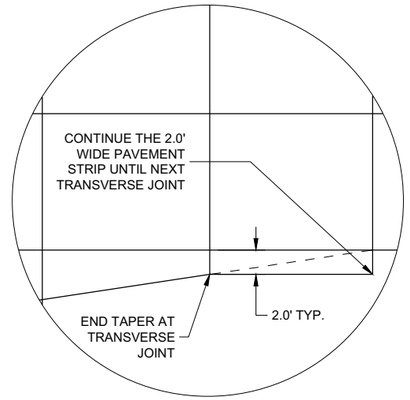
DETAIL "A"



DETAIL "B"



DETAIL "C"



DETAIL "D"

GENERAL NOTES

THE PRIMARY ROADWAY CONTROLS THE TRANSVERSE JOINT PATTERN.

ALIGN NEW JOINTS WITH EXISTING JOINTS OR CRACKS.

CONSTRUCT TRANSVERSE JOINTS PERPENDICULAR TO THE ROADWAY.

ADJUST TRANSVERSE JOINTS TO ALIGN WITH UTILITY FIXTURES (E.G MANHOLES AND INLETS) IN THE PAVEMENT STRUCTURE WHEN POSSIBLE. WATER VALVES DO NOT REQUIRE JOINT ADJUSTMENT.

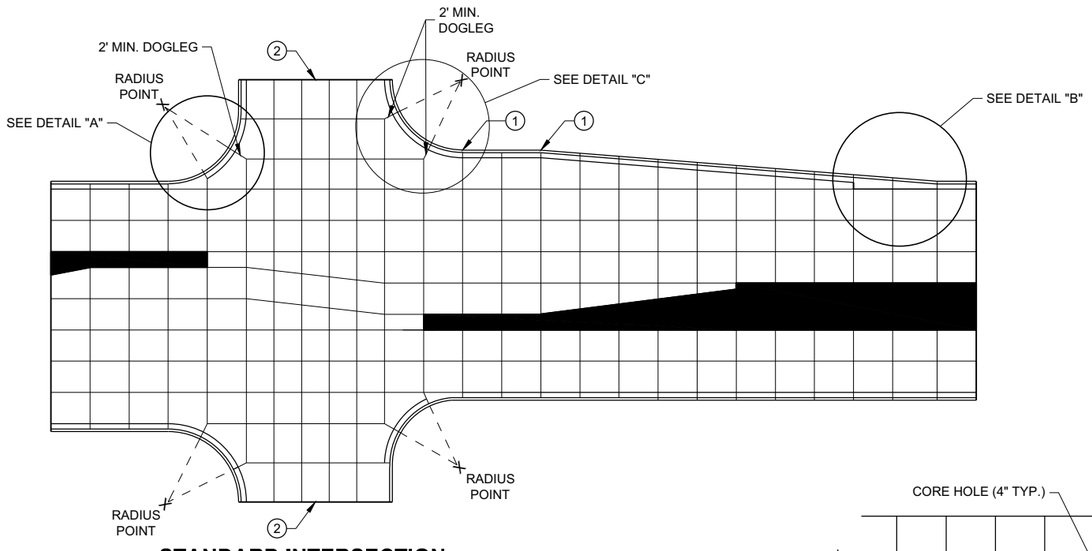
AVOID SLABS LESS THAN 2 FEET WIDE OR GREATER THAN 15 FEET WIDE.

SEE TABLE FOR TRANSVERSE JOINT SPACING. JOINT SPACING SPECIFIED IS MAXIMUM AND ACTUAL SPACING CAN BE ADJUSTED TO ACCOMMODATE INTERSECTIONS.

AVOID ANGLES LESS THAN 60° BY DOGLEGGING JOINTS THROUGH CURVE RADIUS POINTS. USE 90° ANGLES WHEN POSSIBLE.

CORRELATE LONGITUDINAL JOINTS WITH LANE LINES WHEN POSSIBLE.

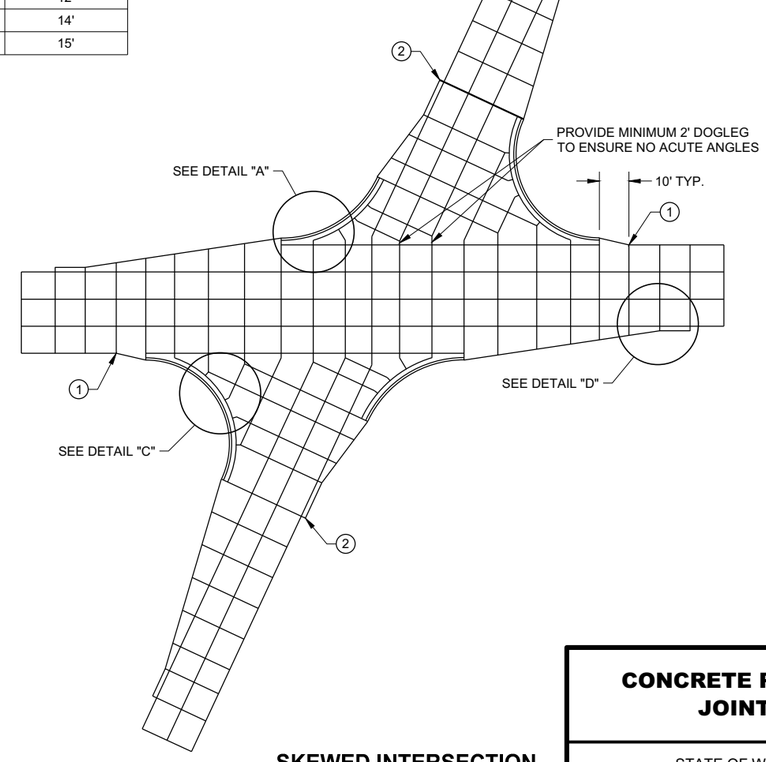
1. PROVIDE TRANSVERSE JOINTS AT ALL PAVEMENT WIDTH CHANGES.
2. CONSTRUCT DOWELED EXPANSION JOINT ON THE SIDE ROAD OF AN INTERSECTION IF THE SIDE ROAD IS CONCRETE PAVEMENT AND GREATER THAN 300 FEET IN LENGTH. ALIGN EXPANSION JOINT WITHEDGE OF RADIUS.
3. THE ENGINEER MAY APPROVE SLIGHT VARIATIONS FROM THESE JOINTING DETAILS.



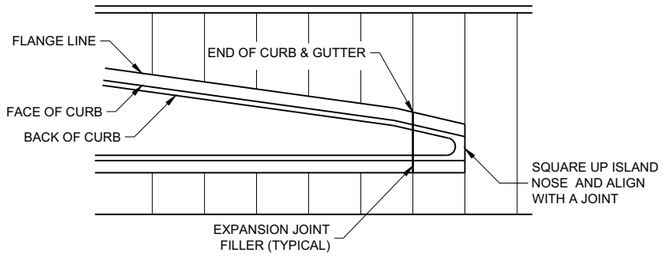
STANDARD INTERSECTION

PAVEMENT DEPTH AND JOINT SPACING TABLE

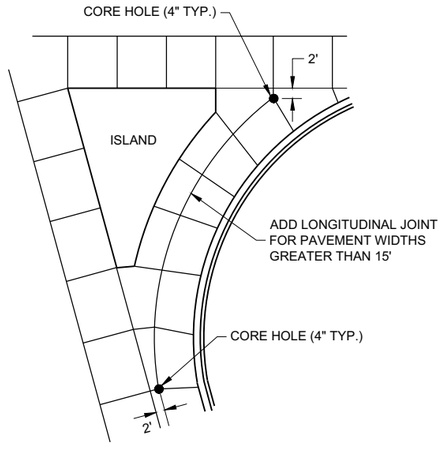
PAVEMENT DEPTH (D)	CONTRACTION JOINT SPACING
6", 6 ½"	12'
7", 7 ½"	14'
8" & ABOVE	15'



SKEWED INTERSECTION



APPROACH TO MEDIAN



LARGE RIGHT TURN

CONCRETE PAVEMENT JOINTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 13C18 - 07a

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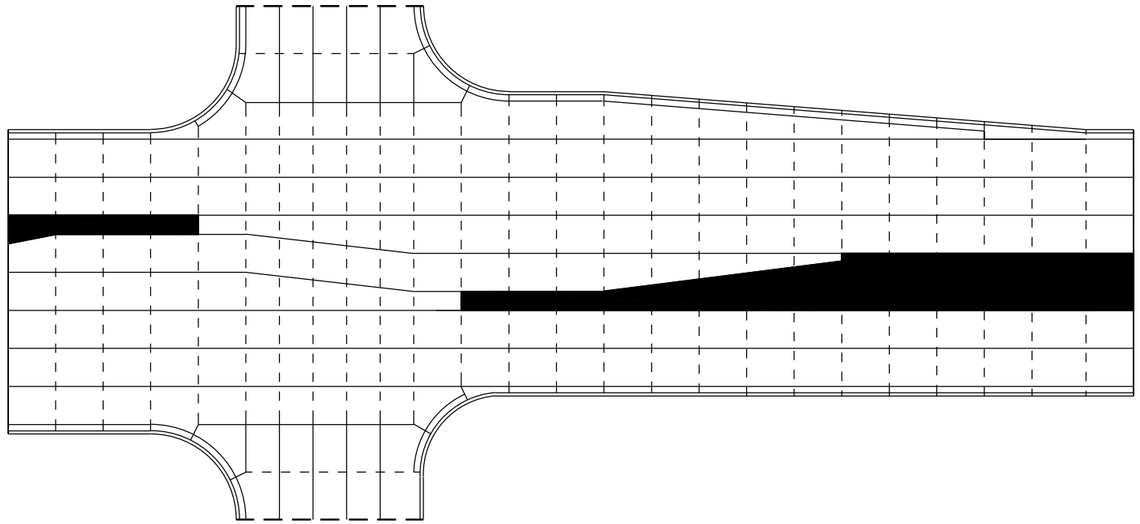
SDD 13C18 - 07a

LEGEND

POTENTIAL DOWELED EXPANSION JOINT

DOWELED JOINT

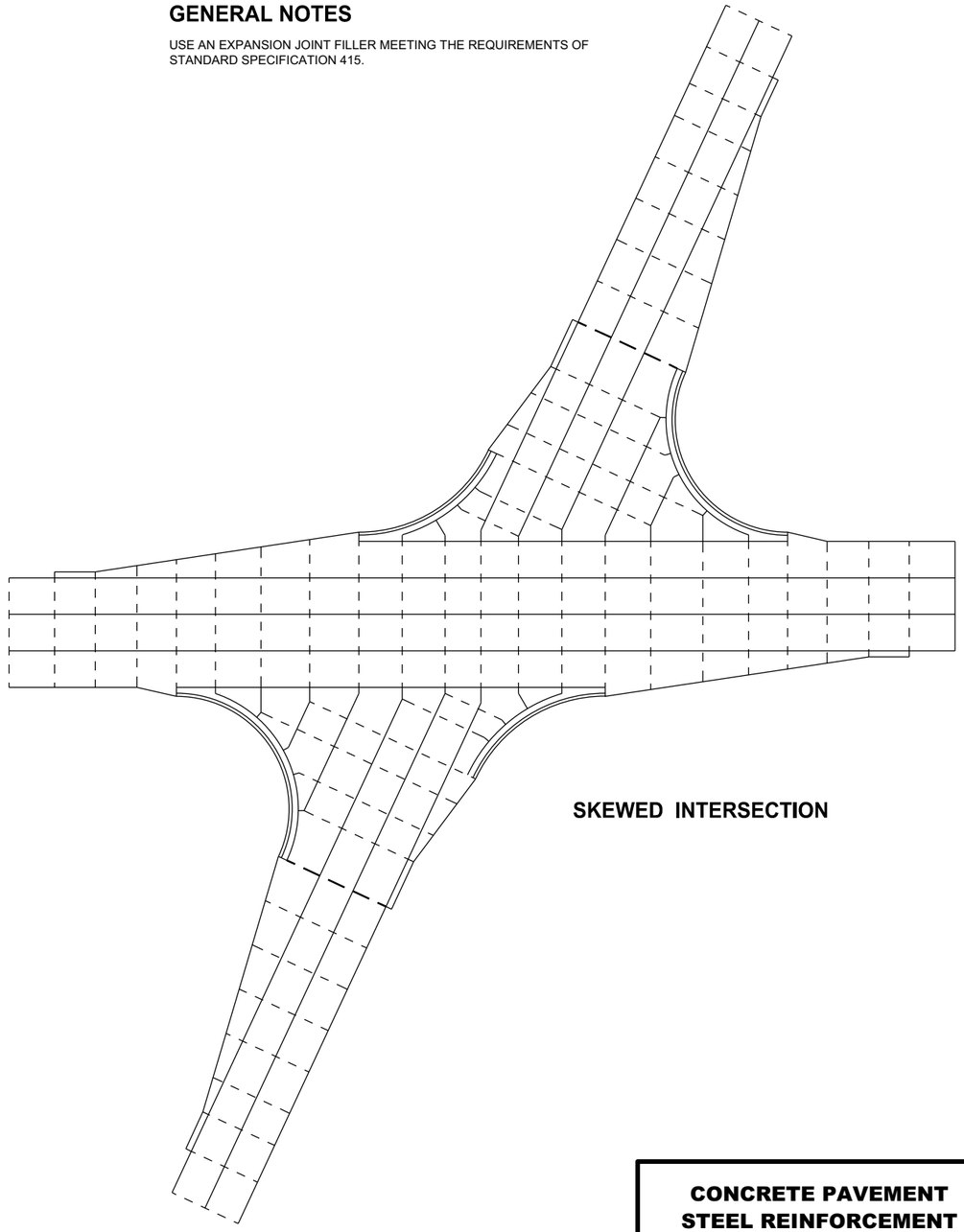
TIED JOINT



STANDARD INTERSECTION

GENERAL NOTES

USE AN EXPANSION JOINT FILLER MEETING THE REQUIREMENTS OF STANDARD SPECIFICATION 415.



SKEWED INTERSECTION

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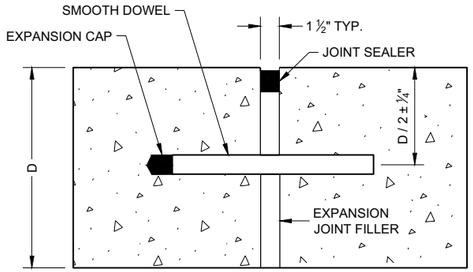
SDD 13C18 - 07b

CONCRETE PAVEMENT STEEL REINFORCEMENT

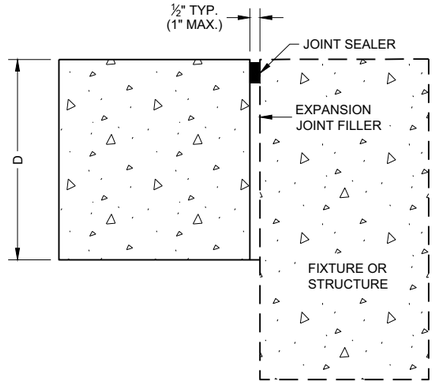
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 13C18 - 07b



DOWELED TRANSVERSE (1)



UNTIED - LONGITUDINAL

EXPANSION JOINTS

TIE BAR TABLE

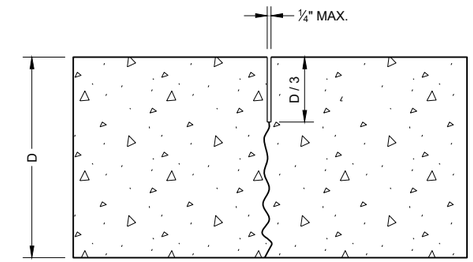
PAVEMENT DEPTH (D)	TIE BAR SIZE	TIE BAR LENGTH (L)	MAX. TIE BAR SPACING
< 10 ½"	NO. 4	30"	36"
≥ 10 ½"	NO. 5	36"	36"
	NO. 4*	30"	24"**

* SUBSTITUTE BENT BARS AT LONGITUDINAL JOINTS WHEN EQUIPMENT LIMITATIONS DURING CONSTRUCTION WARRANT (e.g. AUXILIARY LANES OR TURN LANES)

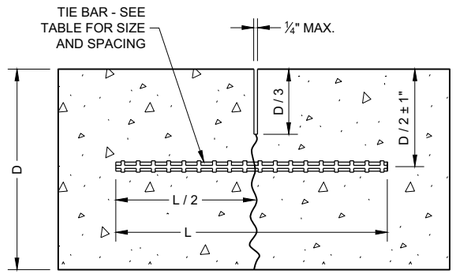
** CONFORM TO 15" MINIMUM SPACING FROM TRANSVERSE JOINTS; SPACING BETWEEN TIE BARS WILL BE 30" AT TRANSVERSE JOINTS.

GENERAL NOTES

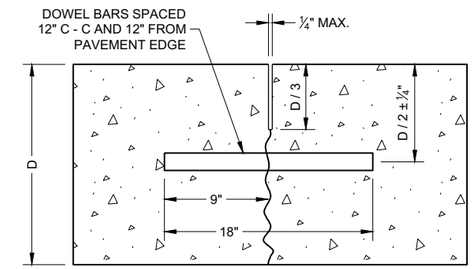
1. USE DOWELED EXPANSION JOINTS ON SIDE ROADS AT INTERSECTIONS (TO ISOLATETHE SIDE ROAD FROM THE THROUGH STREET) IF THE SIDE ROAD IS CONCRETE PAVEMENT AND GREATER THAN 300 FEET IN LENGTH.
2. SPACE CONTRACTION JOINTS IN ACCORDANCE WITH SDD 13C4, 13C11 OR 13C13.
3. LOCATE CONSTRUCTION JOINTS A MINIMUM OF 6 FEET FROM THE NEAREST CONTRACTION JOINT AND ALIGN PARALLEL TO CONTRACTION JOINTS.
4. CONSTRUCTION JOINTS CAN BE FORMED OR SAWED.
5. IF JOINT IS FORMED, PROVIDE A ¼" RADIUS.
6. ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.



UNDOWELED TRANSVERSE

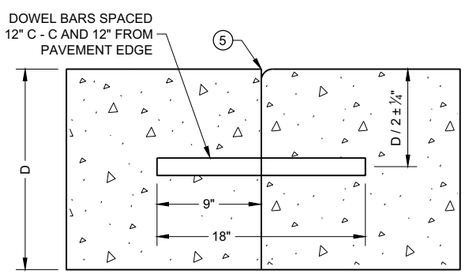


TIED LONGITUDINAL

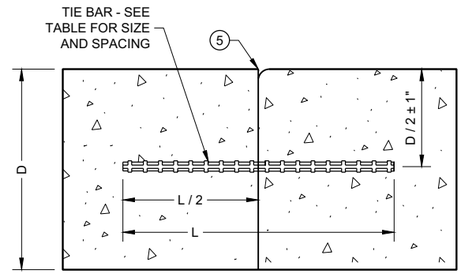


DOWELED TRANSVERSE

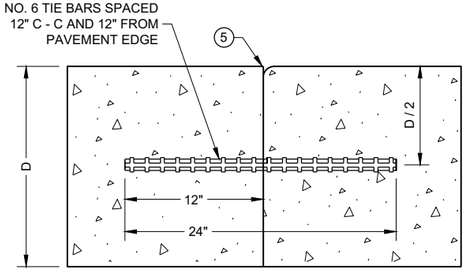
CONTRACTION JOINTS (2)



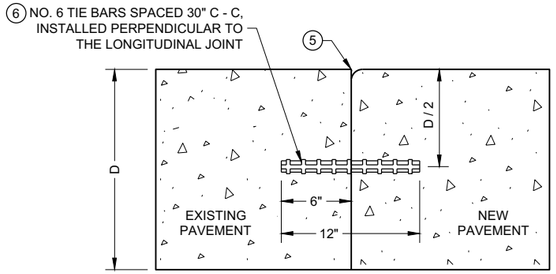
DOWELED TRANSVERSE (3)



TIED LONGITUDINAL



TIED TRANSVERSE (3)
(FOR USE ON NON-DOWELED PAVEMENTS ONLY)



TIED LONGITUDINAL TO EXISTING

CONSTRUCTION JOINTS (4)

CONCRETE PAVEMENT JOINT TYPES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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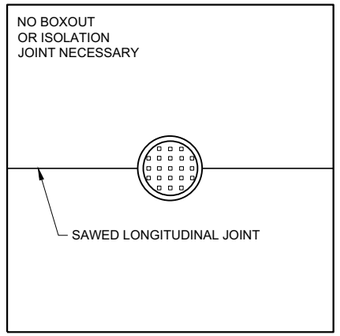
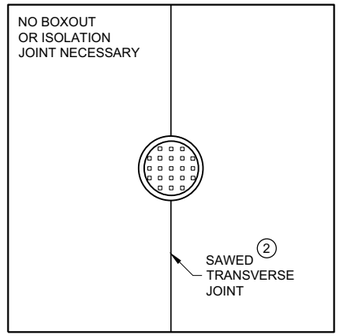
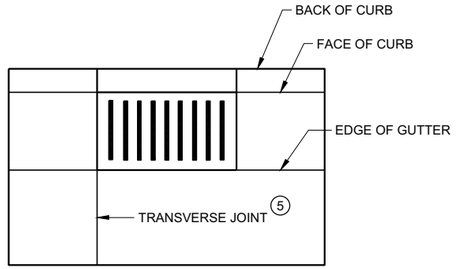
SDD 13C18 - 07c



MANHOLE WITH LONGITUDINAL JOINT



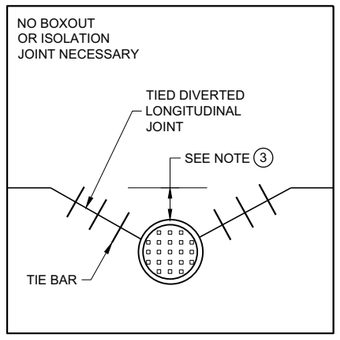
MANHOLE WITH TRANSVERSE JOINT



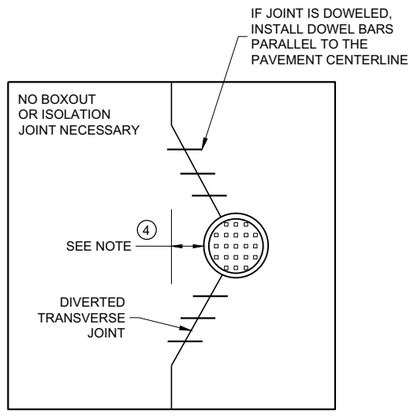
INLET WITH TRANSVERSE JOINT

GENERAL NOTES

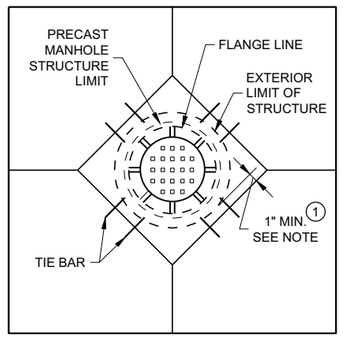
1. USE BOXOUTS WHEN UTILITY STRUCTURE IS IN THE PATH OF CONSTRUCTION JOINTS. PROVIDE A 1 FOOT MINIMUM CLEARANCE BETWEEN THE EXTERIOR LIMIT OF THE STRUCTURE TO THE DIAMOND BOXOUT.
2. ADJUST TRANSVERSE JOINT TO INTERSECT MANHOLE IF POSSIBLE.
3. IF DISTANCE BETWEEN THE LONGITUDINAL JOINT AND THE EDGE OF MANHOLE IS 2 FEET OR LESS, DIVERT THE LONGITUDINAL JOINT AT A 2:1 TAPER RATE TO THE CENTER OF THE MANHOLE. IF THE DISTANCE IS GREATER THAN 2 FEET, DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REINFORCEMENT REBAR AROUND THE MANHOLE.
4. IF THE DISTANCE FROM THE EDGE OF THE MANHOLE TO THE NEAREST TRANSVERSE JOINT IS LESS 4 FEET OR LESS, REDIRECT JOINT TO INTERSECT THE CENTER OF THE MANHOLE. IF DISTANCE IS GREATER THAN 4 FEET, DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REINFORCEMENT REBAR AROUND THE MANHOLE.
5. ALIGN TRANSVERSE JOINT WITH ONE EDGE OF INLET WHEN PRACTICAL.



MANHOLE WITH DIVERTED LONGITUDINAL CONTRACTION JOINT



MANHOLE WITH DIVERTED TRANSVERSE CONTRACTION JOINT



DIAGONAL MANHOLE BOXOUT FOR CONSTRUCTION JOINTS

CONCRETE PAVEMENT JOINTING AT UTILITY FIXTURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018
DATE

/S/ Peter Kemp P.E.
PAVEMENT SUPERVIS

FHWA

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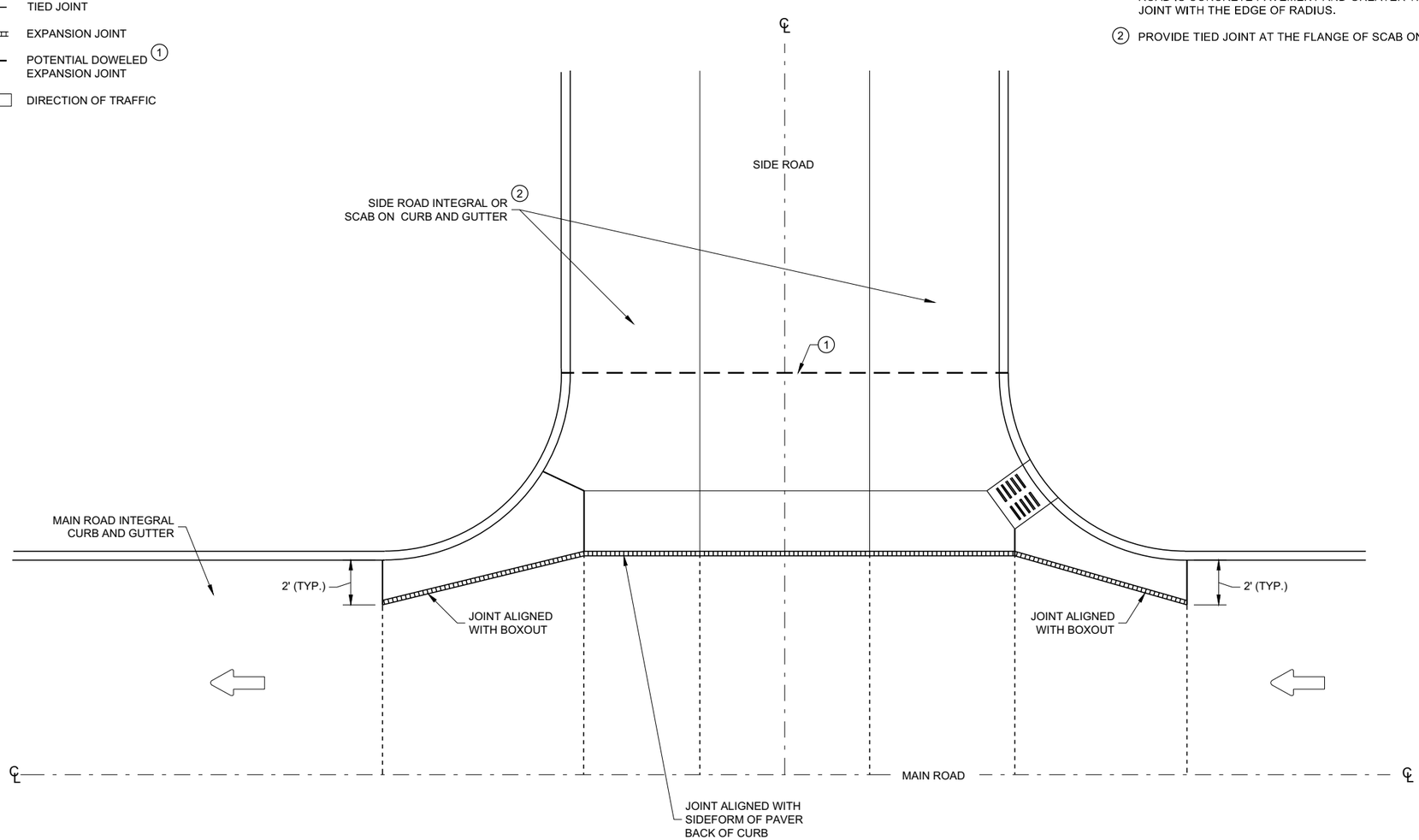
SDD 13C18 - 07d

LEGEND

- DOWELED JOINT
- TIED JOINT
- EXPANSION JOINT
- POTENTIAL DOWELED EXPANSION JOINT (1)
- DIRECTION OF TRAFFIC



GENERAL NOTES

(1) CONSTRUCT DOWELED EXPANSION JOINT ON THE SIDE ROAD OF AN INTERSECTION IF THE SIDE ROAD IS CONCRETE PAVEMENT AND GREATER THAN 300 FEET IN LENGTH. ALIGN EXPANSION JOINT WITH THE EDGE OF RADIUS.

(2) PROVIDE TIED JOINT AT THE FLANGE OF SCAB ON CURB IF SCAB ON CURB AND GUTTER IS USE.

INTERSECTION BOXOUT FOR INTEGRAL CURB AND GUTTER

CONCRETE PAVEMENT INTERSECTION BOXOUT FOR INTEGRAL CURB AND GUTTER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018
DATE

/S/ Peter Kemp P.E.
ROADWAY STANDARDS DEVE ENGINEER

FHWA

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SDD 13C18 - 07f

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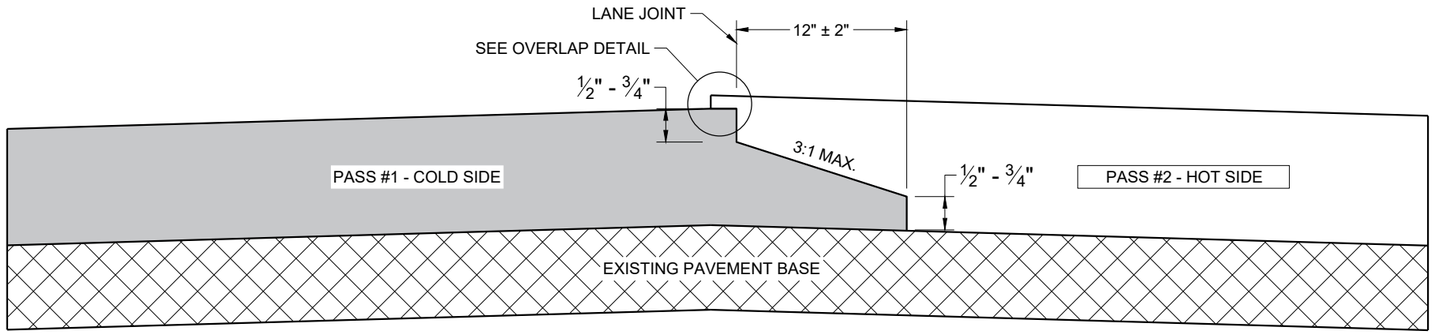
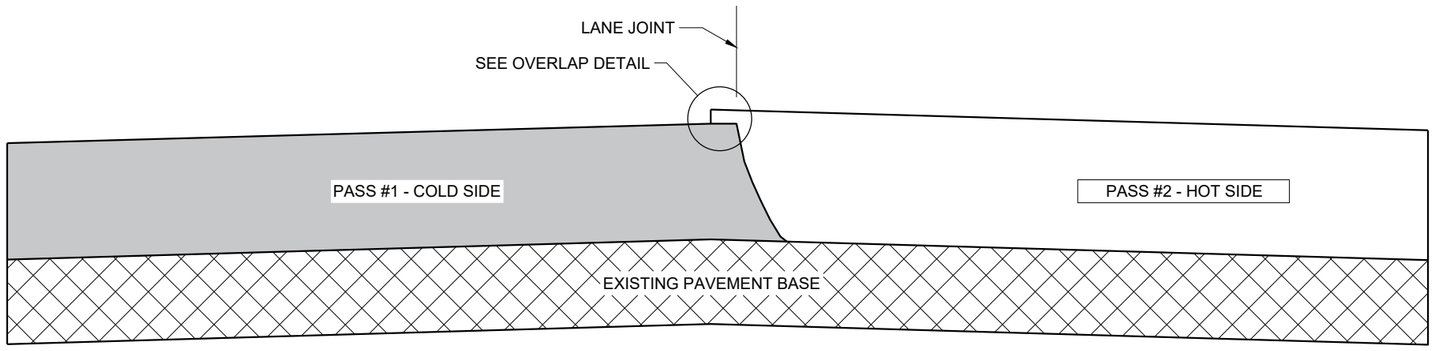
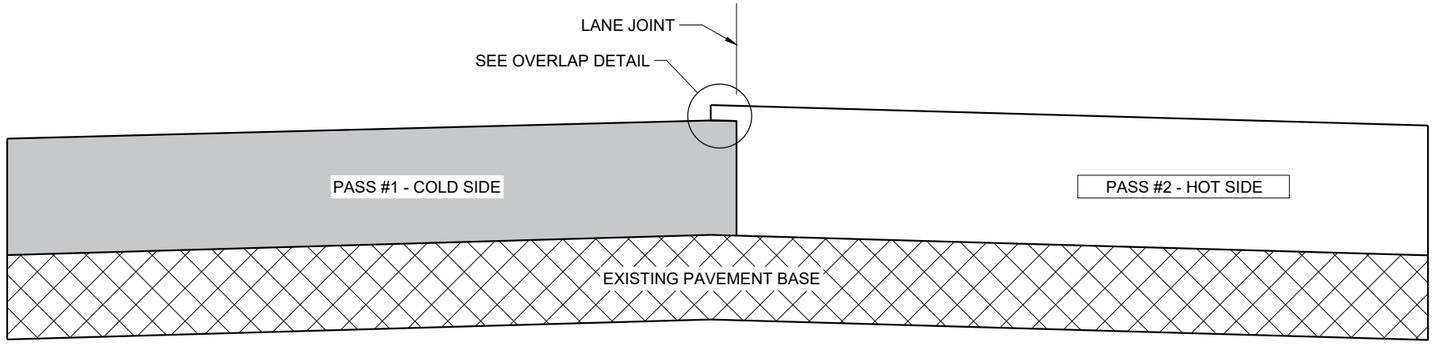
SDD 13C18 - 07f



TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)

GENERAL NOTES

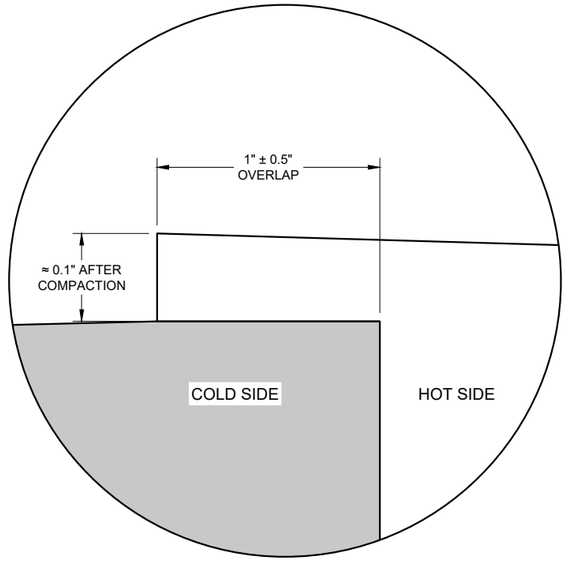
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY 1" ± 0.5" AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

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SDD 13C19 - 03

SDD 13C19 - 03

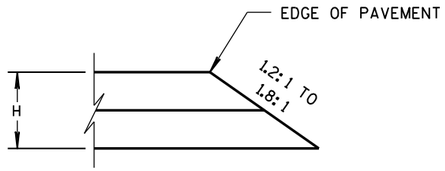
HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2020
DATE

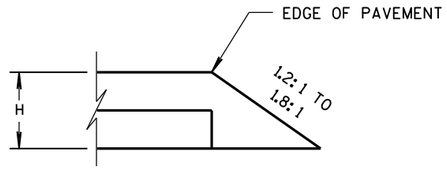
/S/ Steven Hefel
HMA PAVEMENT ENGIN

FHWA



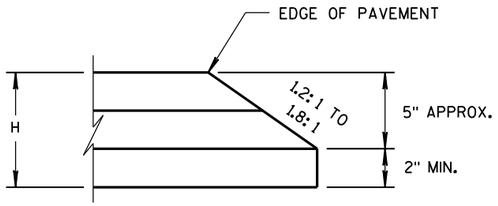
CONSTRUCTED WITH FINAL TWO LAYERS

FOR H 5" OR LESS



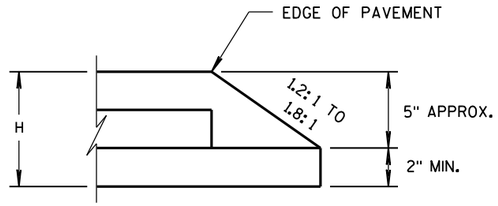
CONSTRUCTED WITH FINAL LAYER

FOR H 5" OR LESS



CONSTRUCTED WITH FINAL TWO LAYERS

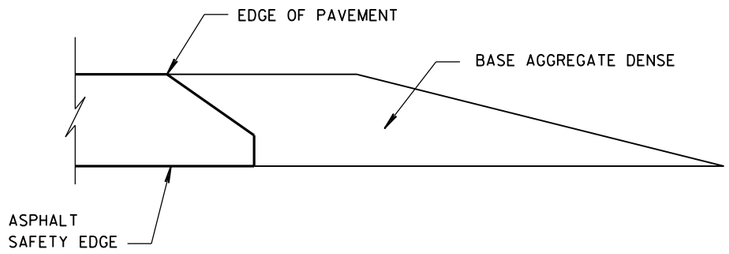
FOR H GREATER THAN 5"



CONSTRUCTED WITH FINAL LAYER

FOR H GREATER THAN 5"

HMA PAVEMENT AND HMA OVERLAYS



FINISHED SHOULDER AGGREGATE PLACEMENT

6

6

S.D.D. 14 B 29-1

S.D.D. 14 B 29-1

SAFETY EDGE SM

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

11/30/2012
DATE

/S/ Jerry H. Zoda
ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA

(1) WOOD OR STEEL POSTS (w6X9 OR w6X8.5) MAY BE USED. DO NOT INTERMIX WOOD AND STEEL POSTS. INSTALL STEEL POSTS WITH HOLES ON APPROACHING TRAFFIC SIDE.

(2) USE WOOD OR APPROVED PLASTIC BLOCKOUTS. WOOD BLOCKOUTS MAY BE CONSTRUCTED OUT OF TWO OR MORE WOOD BLOCKOUTS. SEE ALTERNATE WOOD BLOCKOUT DETAIL. DIMENSIONS OF APPROVED PLASTIC BLOCKOUTS MAY VARY.

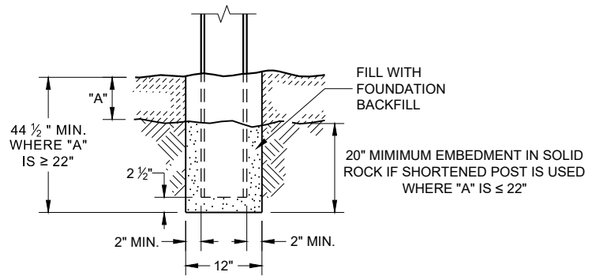
(3) IF ROCK IS ENCOUNTERED DURING EXCAVATION, PROVIDE A HOLE 12 INCHES IN DIAMETER EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE APPROXIMATELY 2 1/2" INCHES OF GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE. CUT THE POSTS THE TO LENGTH AMD INSTALL. BACKFILL WITH EXCAVATED MATERIAL AND COMPACT. BACKFILL IS TO BE FREE OF LARGE ROCKS.

(4) WHEN THE DISTANCE FROM BACK OF POST TO SHOULDER HINGE POINT IS LESS THAN 2 FEET INSTALL LONGER POST AT HALF POST SPACING (K).

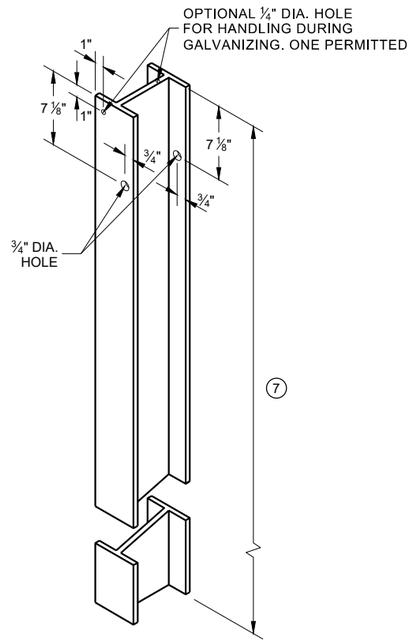
(5) FOR NEW MGS INSTALLATION TOP OF W-BEAM RAIL TOLERANCE IS ±1". FOR EXISTING MGS INSTALLATION TOP OF W-BEAM IS BETWEEN 27 ¾" TO 32".

(6) WHEN USING STEEL POST AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

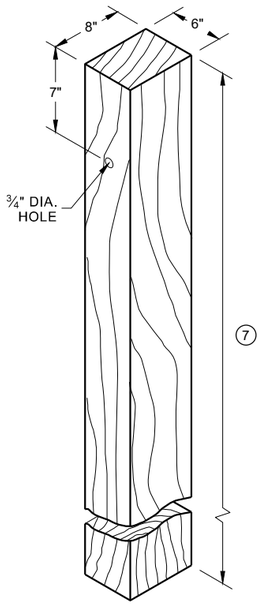
(7) TOTAL POST LENGTH FOR TYPE K IS 7' - 0".
TOTAL POST LENGTH FOR OTHER MGS TYPES IS 6' - 0".



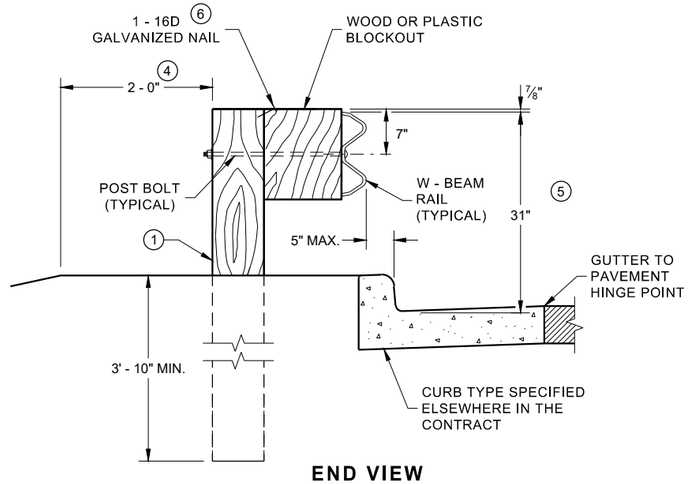
END VIEW
SETTING STEEL OR WOOD POST IN ROCK (3)



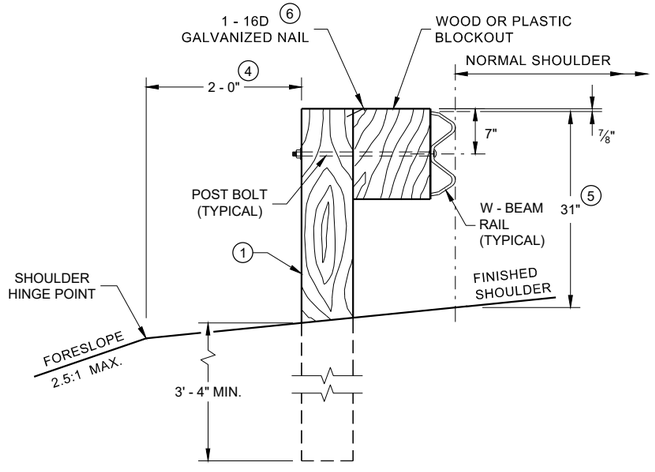
STEEL POST & HOLE PUNCHING DETAIL (W 6 X 9) (1)



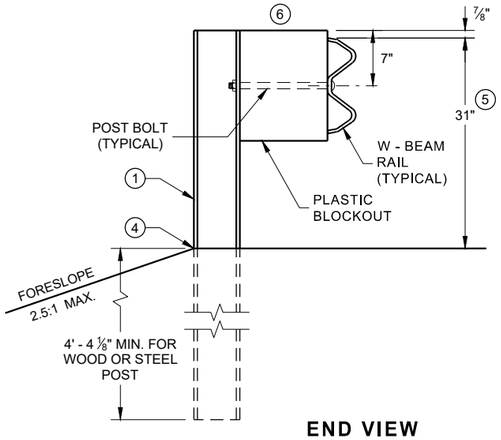
WOOD POST (6" X 8") NOMINAL (1)



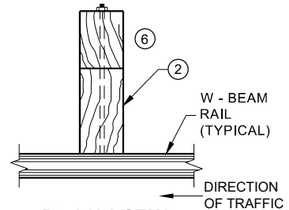
END VIEW
LOCATED ALONG A CURBED ROADWAY



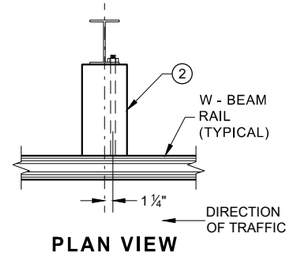
END VIEW
LOCATED ALONG A ROADWAY SHOULDER STANDARD INSTALLATION



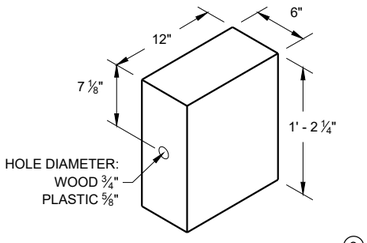
END VIEW
MGS LONGER POST AT HALFPOST SPACING W BEAM (K)



PLAN VIEW
WOOD POST, BLOCKOUT & BEAM



PLAN VIEW
STEEL POST, PLASTIC BLOCKOUT & BEAM



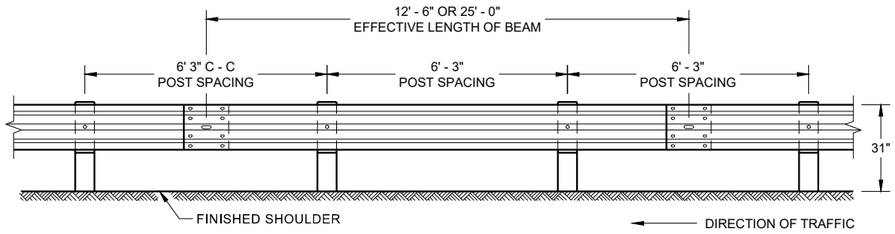
WOOD OR PLASTIC BLOCKOUT (2)

MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

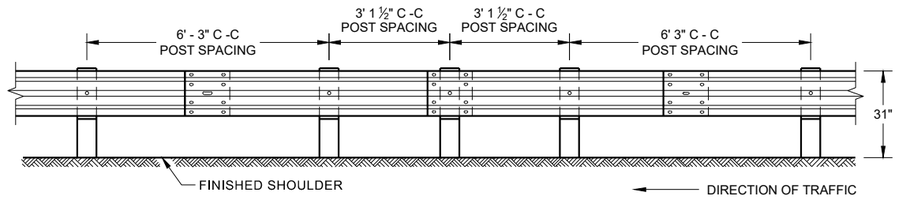
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

SDD 14B42 - 07a

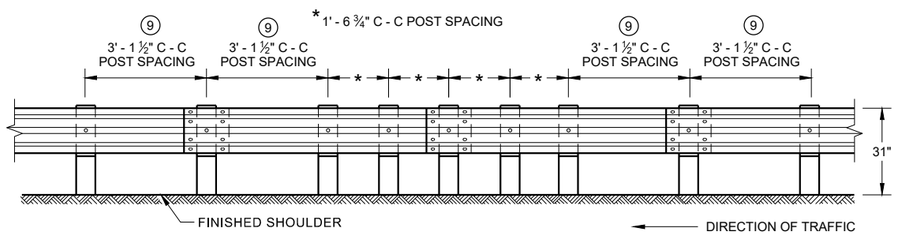
SDD 14B42 - 07a



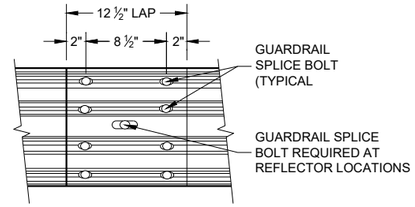
FRONT VIEW
POST SPACING STANDARD INSTALLATION



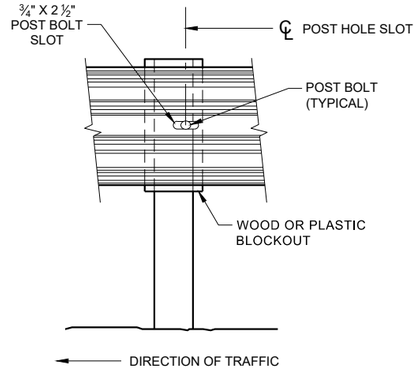
FRONT VIEW
HALF POST SPACING (HS) AND HALF POST SPACING WITH LONGER POSTS (K)



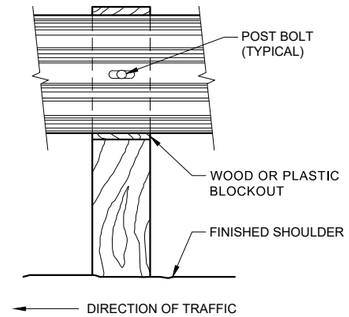
FRONT VIEW
QUARTER POST SPACING (QS)



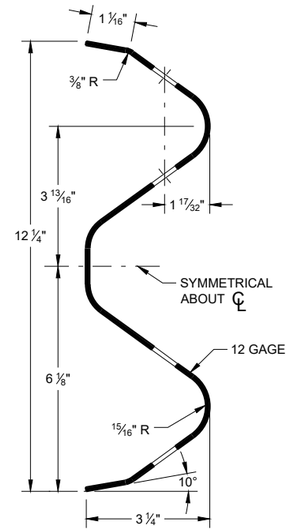
FRONT VIEW
MID-SPAN BEAM SPLICE



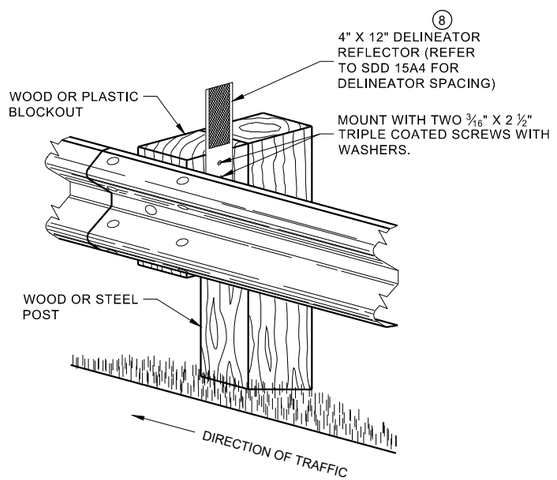
FRONT VIEW AT STEEL POST



FRONT VIEW AT WOOD POST



SECTION THRU W-BEAM RAIL



ONE SIDED REFLECTOR DETAIL AND TYPICAL INSTALLATION

GENERAL NOTES

⑧ DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.

⑨ 25 FEET OF HALF POST SPACING IS REQUIRED ON APPROACH AND DEPARTURE ENDS OF QUARTER POST SPACING.

POST BOLTS ARE A ⅝" DIAMETER ASTM A307 GUARDRAIL BOLT. A POST BOLT REQUIRES ⅝" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT AND ⅝" DIAMETER F844 FLAT WASHER. POST BOLTS MAY BE LONGER IF MULTIPLE BLOCKOUTS ARE BEING USED.

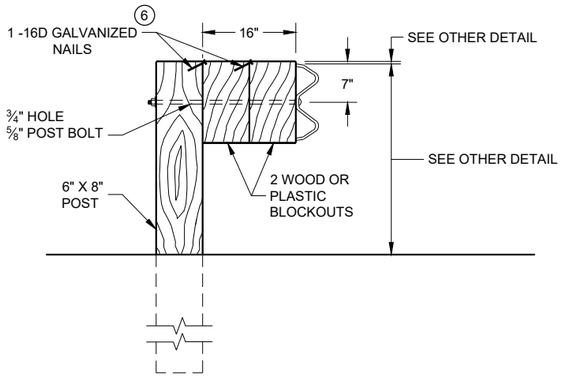
GUARD RAIL SPLICE BOLTS ARE A ⅝" DIAMETER ASTM A307 GUARDRAIL HEAD BOLT. A GUARDRAIL SPLICE BOLT REQUIRES ⅝" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT.

MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 14B42 - 07b
SDD 14B42 - 07b

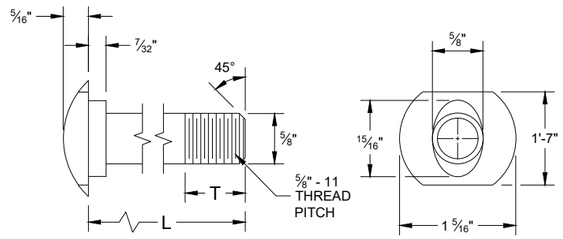


DETAIL FOR 16" BLOCKOUT DEPTH

IT IS ACCEPTABLE TO USE BLOCKOUTS UP TO 16" DEEP TO INCREASE THE POST OFFSET TO AVOID UNDERGROUND OBSTACLES. THERE IS NO LIMIT TO THE NUMBER OF POSTS THAT CAN HAVE ADDITIONAL BLOCKOUTS UP TO 16" DEEP.

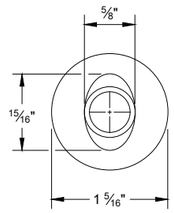
NOTE:

1. ALL FILLETS SHALL HAVE A MINIMUM RADIUS OF 3⁄16".
2. IF THE BOLT EXTENDS MORE THAN ¼" FROM THE NUT THE BOLT SHOULD BE TRIMMED BACK.

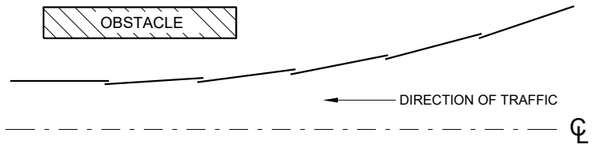


POST BOLT TABLE

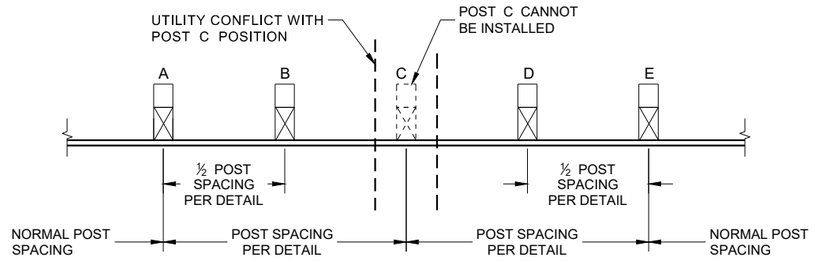
L	T (MIN.)
1 ¼"	1 ⅛"
2"	1 ¾"
10"	4"
14"	4 1⁄16"
18"	4"
21"	4 1⁄16"
25"	4"



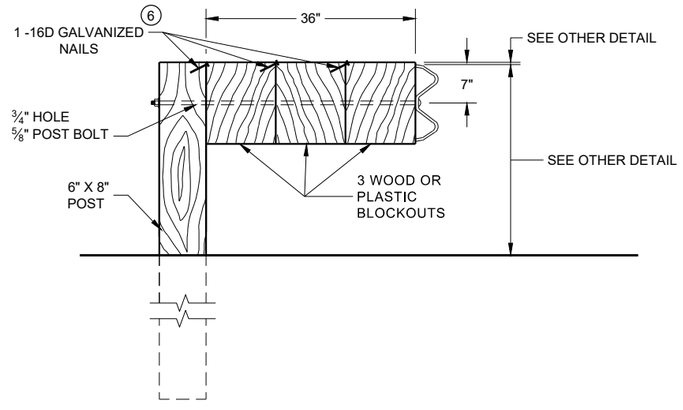
ALTERNATE BOLT HEAD



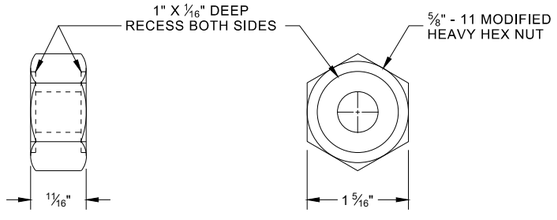
PLAN VIEW
BEAM LAPPING DETAIL



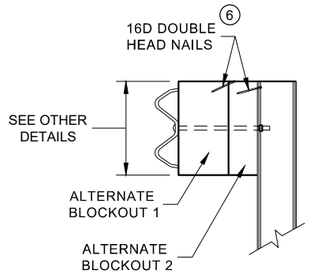
POST DRIVING FOR CONTINUOUS UNDERGROUND OBSTRUCTION



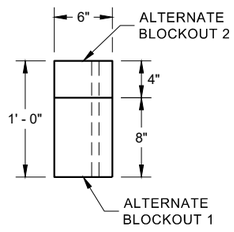
DETAIL FOR 36" BLOCKOUT DEPTH



POST BOLT, SPLICE BOLT AND RECESS NUT



SIDE VIEW



PLAN VIEW

ALTERNATE WOOD BLOCKOUT DETAIL

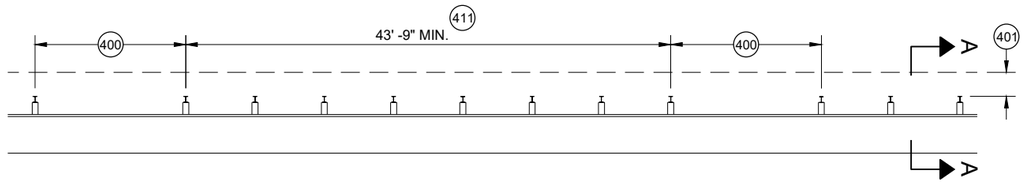
NOTES: UNDER SPECIAL CIRCUMSTANCES, SUCH AS AVOIDING OBSTACLES THAT ARE NOT RELOCATED, IT IS ACCEPTABLE TO INSTALL ADDITIONAL BLOCKOUTS TO OBTAIN UP TO 36" DEPTH FOR ONE OR TWO POSTS IN A SECTION OF GUARDRAIL.

DO NOT USE 16" OR 36" BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.

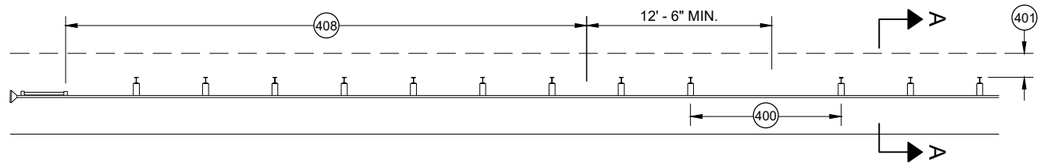
(6) WHEN USING STEEL POST AD WOOD BLOCKOUTS, INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

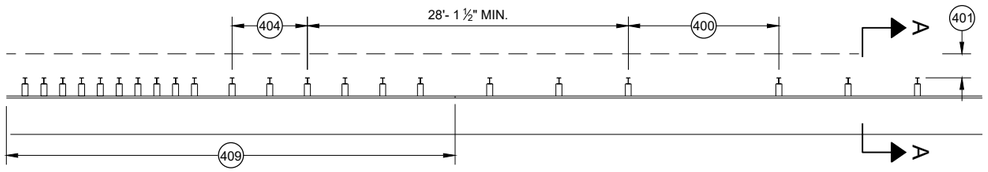
6
SDD 14B42 - 07c



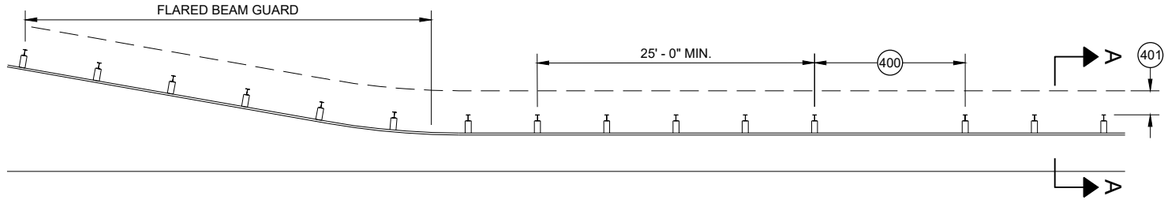
MISSING POST IN MGS GUARDRAIL



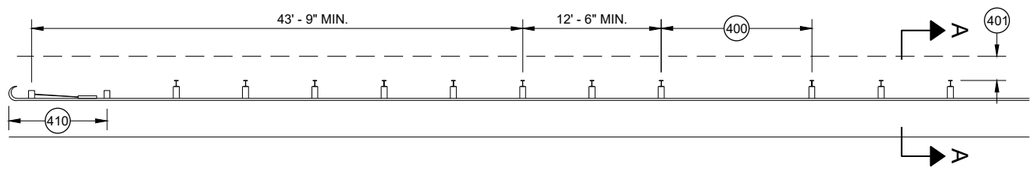
MISSING POST IN MGS GUARDRAIL NEAR EAT



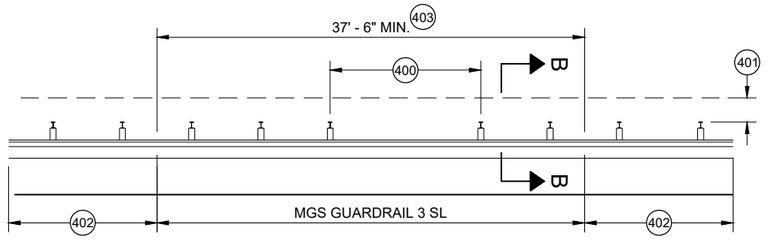
MISSING POST IN MGS GUARDRAIL NEAR AN APPROACH TRANSITION



MISSING POST IN MGS GUARDRAIL NEAR FLARED BEAM GUARD

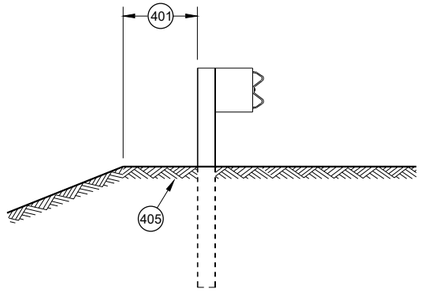


MISSING POST IN MGS GUARDRAIL NEAR A TYPE 2 END TERMINAL

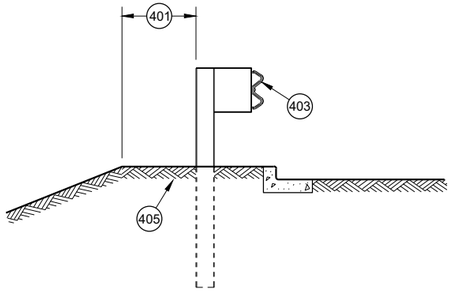


MISSING POST IN SHORT SPAN MGS GUARDRAIL NEAR CURB (SL) 407

(400) MAX SPAN 12' - 6"
(401) 2' MIN.
(402) MGS GUARDRAIL 3
(403) NESTING BEAM GUARD
(404) ASYMMETRIC TRANSITION
(405) SOIL WELL DRAINED AND COMPACTED
(406) SEE OTHER DRAWINGS IN THIS SDD
(407) SEE OTHER DRAWINGS FOR MIN. SPACING BETWEEN SPANS
(408) SEE SDD 14B44
(409) SEE SDD 14B45
(410) SEE SDD 14B47
(411) MINIMUM DISTANCE BETWEEN MISSING POST SPANS.



SECTION A - A 406



SECTION B - B 406

MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2021
DATE
/S/ Rodney Taylor
ROADWAY STANDARDS DEVE
UNIT SUPERVISOR

FHWA

6

SDD 14B42 - 07d

GENERAL NOTES

(A) THE SLOPE IN THE AREA BOUNDED BY THE GRADELINE, THE HINGE POINT LINE (HPL) AND THE CLEAR ZONE LIMITS (CZL) SHALL BE 4:1 OR FLATTER.

(B) AFTER FINAL ASSEMBLY, RECHECK CABLE TO BE SURE IT IS TAUT AND HAS NOT RELAXED

(C) DIFFERENT MANUFACTURERS REQUIRE DIFFERENT PERFORATED W - BEAM RAIL END PANELS. SEE MANUFACTURER'S INFORMATION.

(D) ATTACH ALUMINUM SHEET TO E.A.T. HEAD USING 4 STAINLESS STEEL SELF - TAPPING SCREWS. ONE SCREW PER CORNER.

(E) HARDWARE MAY VARY BETWEEN MANUFACTURER. SEE MANUFACTURER'S DRAWING FOR INFORMATION.

DIMENSIONS MAY VARY, MANUFACTURER'S INFORMATION.

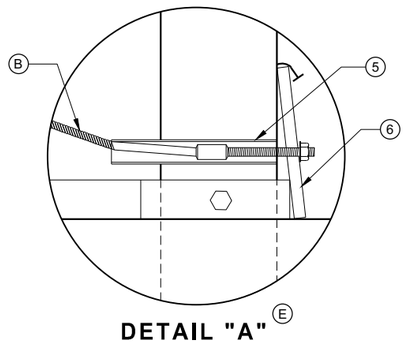
SEE SDD 14B42 FOR MORE INFORMATION.

* DO NOT ATTACH BLOCKOUTS TO POST 1 AND 2.

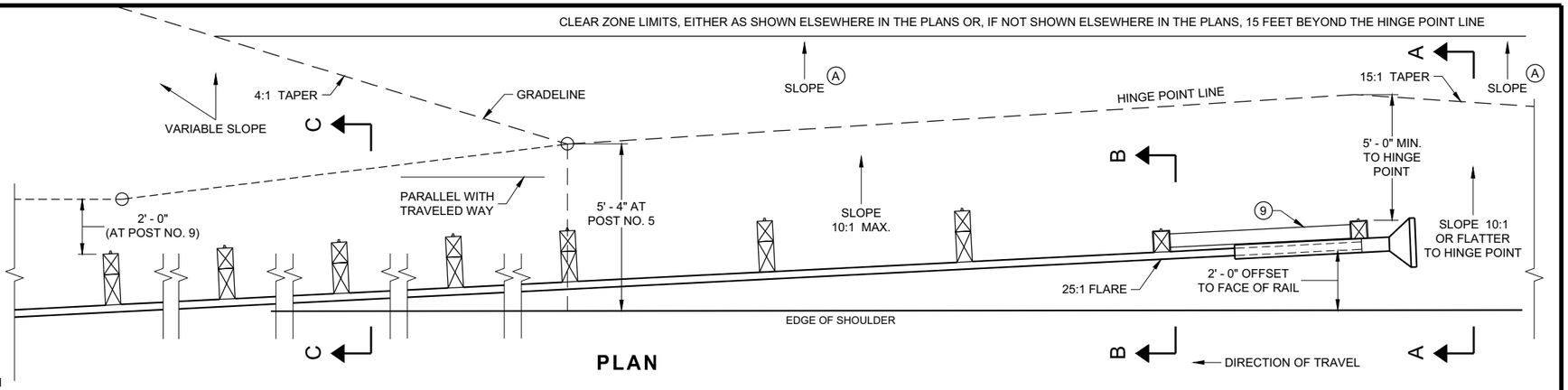
DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.

SEE MANUFACTURER'S DRAWING FOR SPLICE LOCATION, HARDWARE DIMENSIONS AND INSTALLATION INSTRUCTIONS.

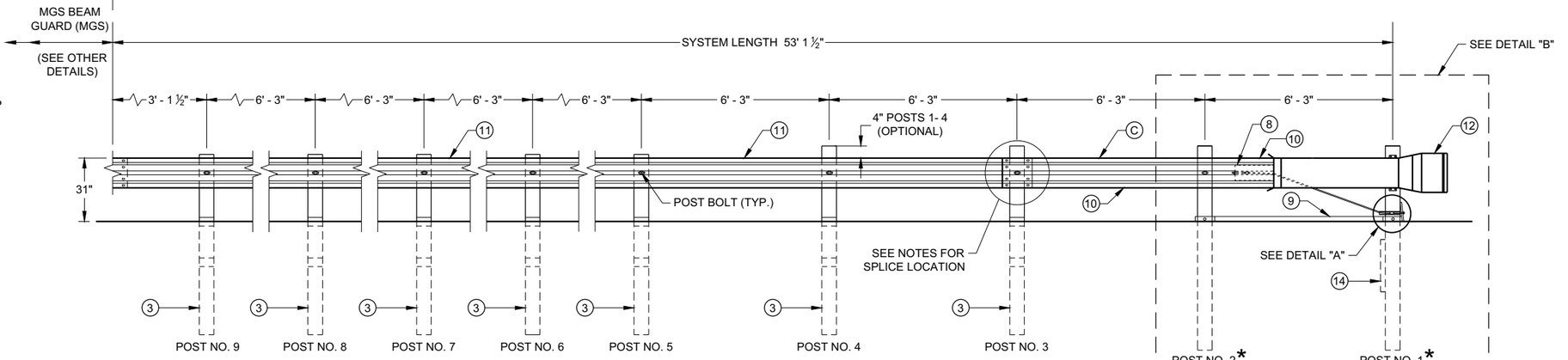
THE CENTER OF THE UPPER 3 ½" DIAMETER HOLE ON POST NUMBER 3 THROUGH POST 9 IS TO BE FLUSH WITH THE GROUND LINE UP TO A MAXIMUM OF 2" ABOVE GROUND LINE. WOOD BLOCKS ON POSTS NUMBERED 3 THROUGH 9 MAY BE ADJUSTED UP TO 3" ABOVE THE TOP OF POST.



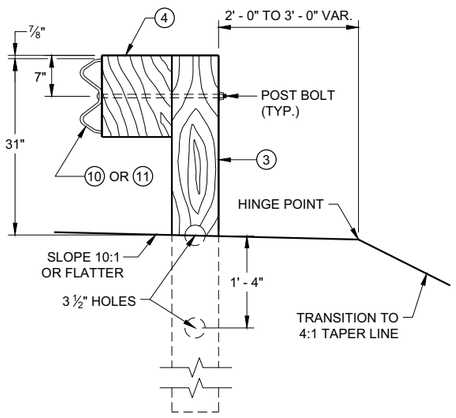
DETAIL "A" (E)



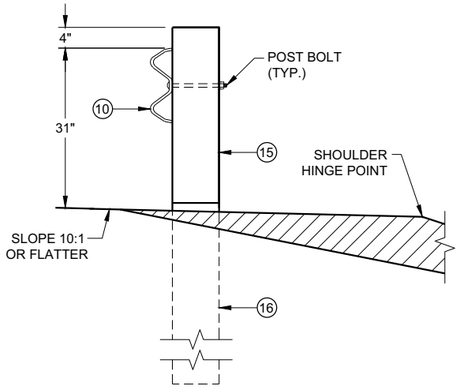
PLAN



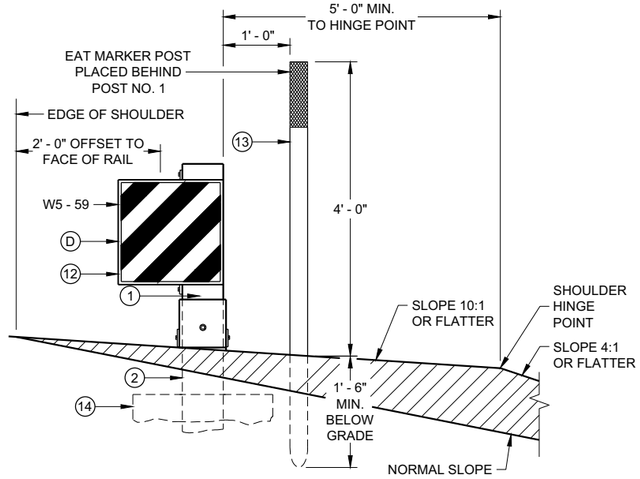
ELEVATION



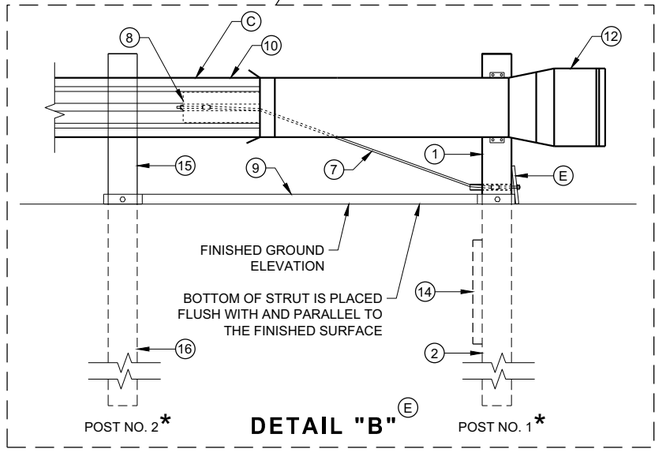
SECTION C - C
TYPICAL AT POST NOS. 3 - 9



SECTION B - B
TYPICAL AT POST NO. 2*



SECTION A - A
TYPICAL AT POST NO. 1*



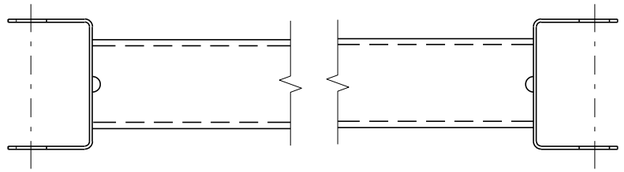
DETAIL "B" (E)

MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

SDD 14B44 - 04a

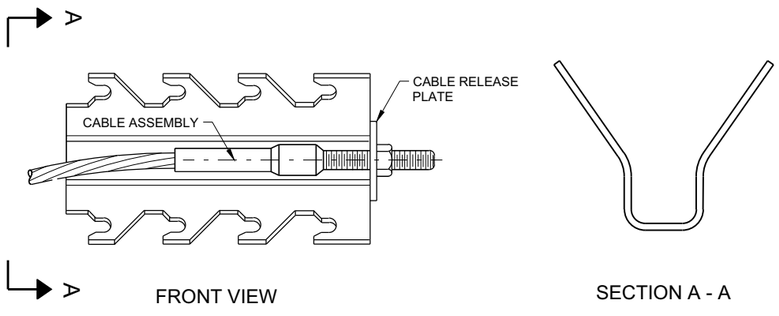
SDD 14B44 - 04a



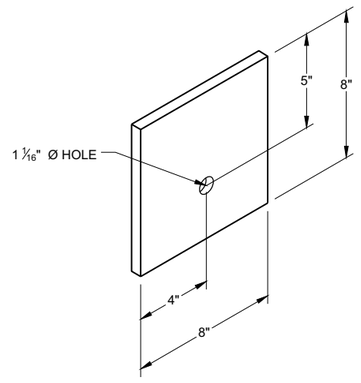
GENERIC GROUND STRUT ⑨ Ⓔ

BILL OF MATERIALS

PART NO.	DESCRIPTION MATERIALS PROVIDED BY MGS EAT MANUFACTURER. SEE MANUGACTURER'S DETAILS FOR MORE INFORMATION.
①	UPPER POST NO. 1 6" X 6" TUBE
②	LOWER POST NO. 1
③	WOOD CRT
④	WOOD BLOCKOUT
⑤	PIPE SLEEVE
⑥	BEARING PLATE
⑦	BCT CABLE ASSEMBLY
⑧	ANCHOR CABLE BOX
⑨	GROUND STRUT
⑩	PERFORATED W-BEAM RAIL END PANEL, 12'-6" LONG.
⑪	STANDARD W-BEAM RAIL. MULTIPLE SECTIONS REQUIRED. SECTIONS VARY IN LENGTH.
⑫	IMPACT HEAD
⑬	EAT MARKER POST - YELLOW (SEE APPROVED PRODUCTS LIST)
⑭	SOIL PLATE
⑮	UPPER POST NO. 2
⑯	LOWER POST NO. 2



GENERIC ANCHOR CABLE BOX ⑨ Ⓔ



BEARING PLATE ⑥ Ⓔ

6

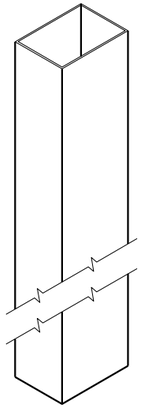
6

SDD 14B44 - 04b

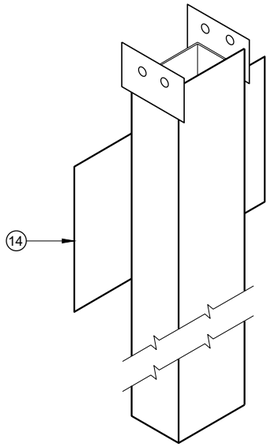
SDD 14B44 - 04b

MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL
(MGS)

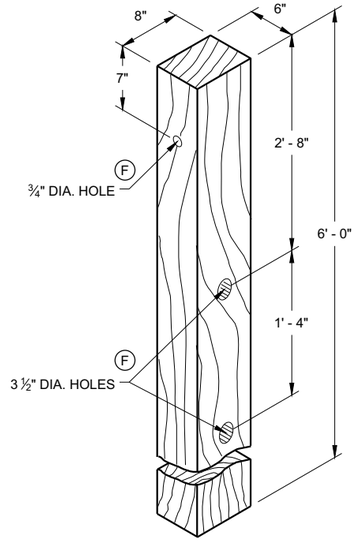
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



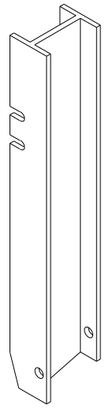
UPPER POST NO. 1 (1) (E)



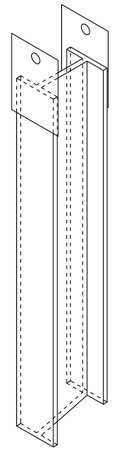
LOWER POST NO. 1 (2) (E)



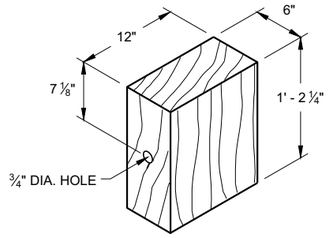
WOOD CRT POST (3) (E)
POSTS NUMBER 3-9



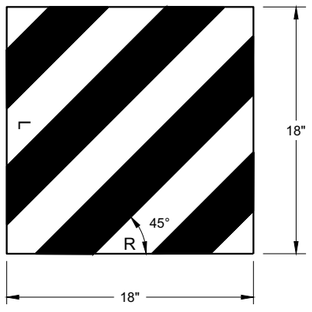
UPPER POST NO. 2 (15) (E)



LOWER POST NO. 2 (16) (E)

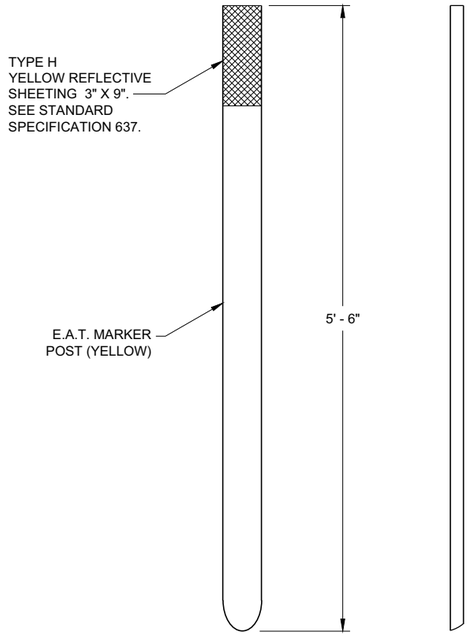


WOOD BLOCKOUT (4)
REQ'D. AT ALL POSTS EXCEPT POST NO'S 1 & 2



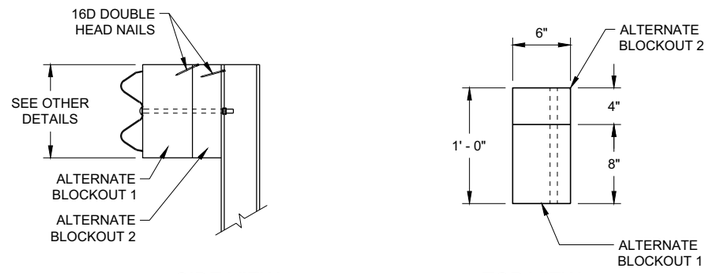
W5 - 59

REFLECTIVE SHEETING DETAIL (E)



FRONT VIEW SIDE VIEW

E.A.T. MARKER POST (13)



SIDE VIEW TOP VIEW

ALTERNATE WOOD BLOCKOUT DETAIL

MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)

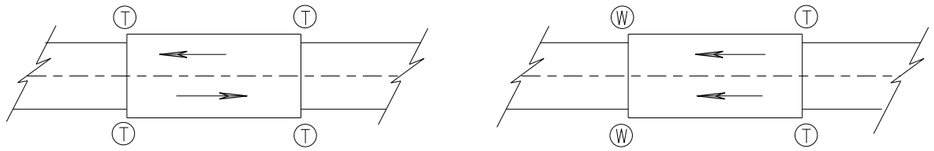
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018 DATE
/S/ Rodney Taylor
ROADWAY STANDARDS DEVE UNIT SUPERVISOR

FHWA

6

SDD 14B44 - 04c



TWO WAY TRAFFIC

ONE WAY TRAFFIC

(T) THRIE BEAM CONNECTION

(W) W-BEAM CONNECTION WHEN REQUIRED

TYPICAL LOCATIONS OF THRIE BEAM AND W-BEAM CONNECTIONS TO BRIDGE

GENERAL NOTES

IF ROCK IS ENCOUNTERED, REMOVE ROCK TO FULL DEPTH OF POST PLUS 2½", AND 12" DIAMETER AROUND POST. SEE 14B42 FOR MORE DETAILS.

TRANSITION USES STEEL POSTS ONLY.

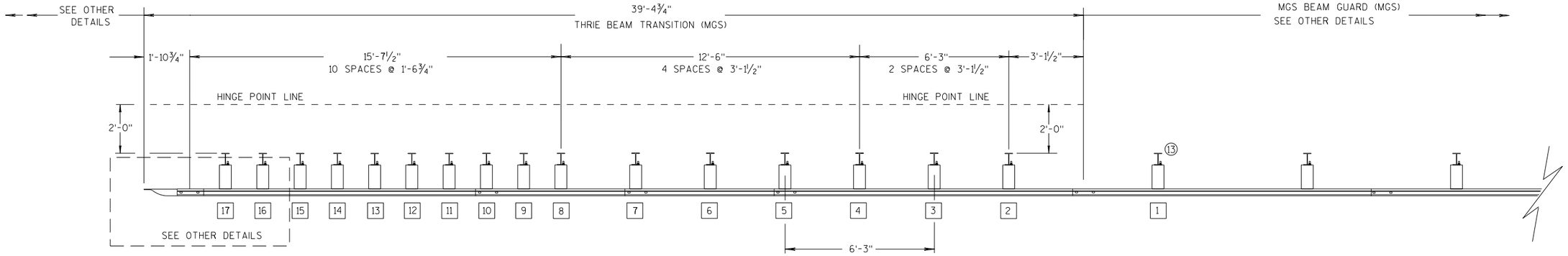
SEE STANDARD DETAIL DRAWING 14 B 42 FOR MORE INFORMATION.

POST 2 THROUGH 17 USES STEEL POST ONLY

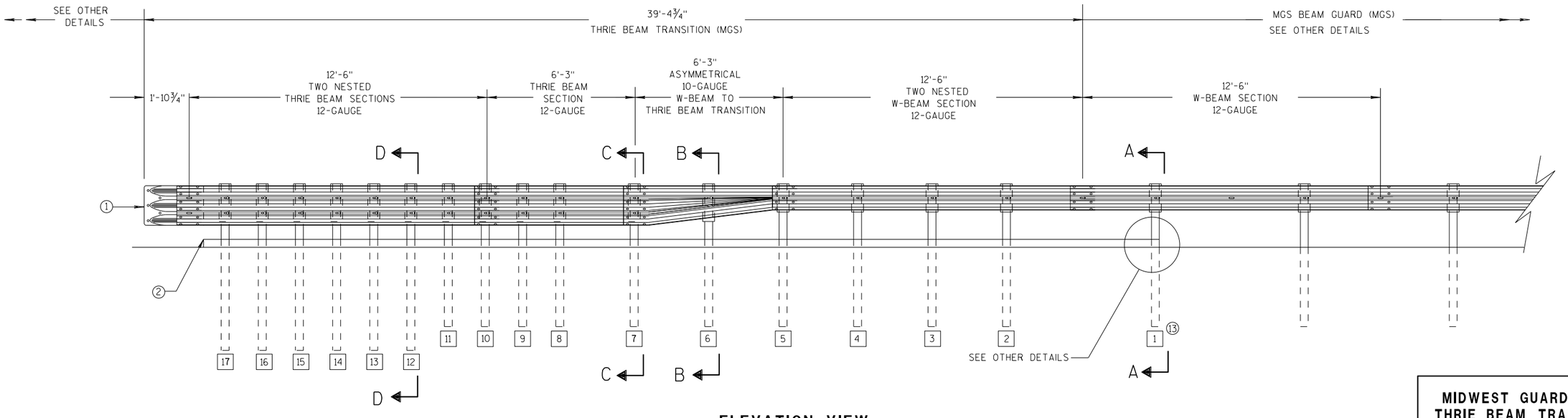
(1) BRIDGE RAILING TYPE "W" DOES NOT REQUIRE A TERMINAL CONNECTOR.

(2) OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

(13) STEEL OR WOOD POST IS ACCEPTABLE AT POST 1. SEE SDD14B42



PLAN VIEW



ELEVATION VIEW

MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 14 B 45-5a

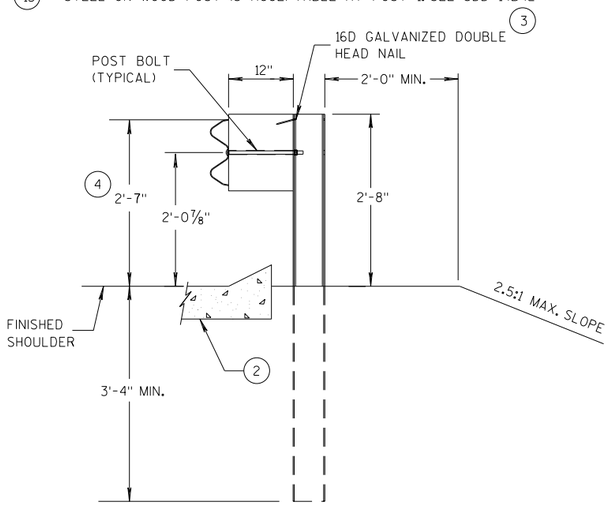
GENERAL NOTES

(2) OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

(3) WHEN USING STEEL POSTS AND WOOD BLOCKOUTS INSTALL FOUR 10D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

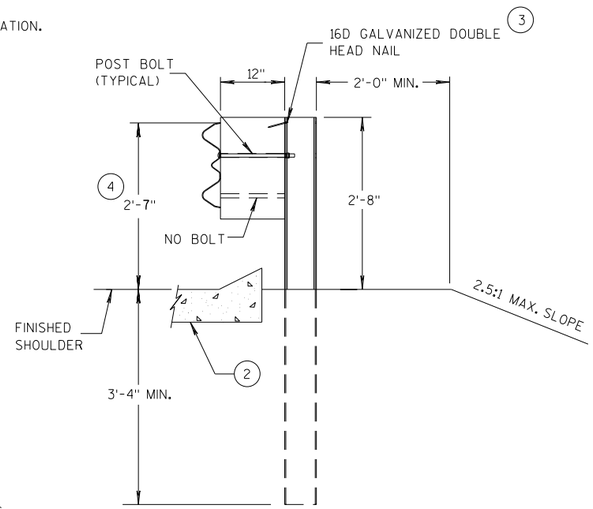
(4) TOLERANCE FOR TOP OF W-BEAM RAIL IS ± 1".

(13) STEEL OR WOOD POST IS ACCEPTABLE AT POST 1. SEE SDD 14B42



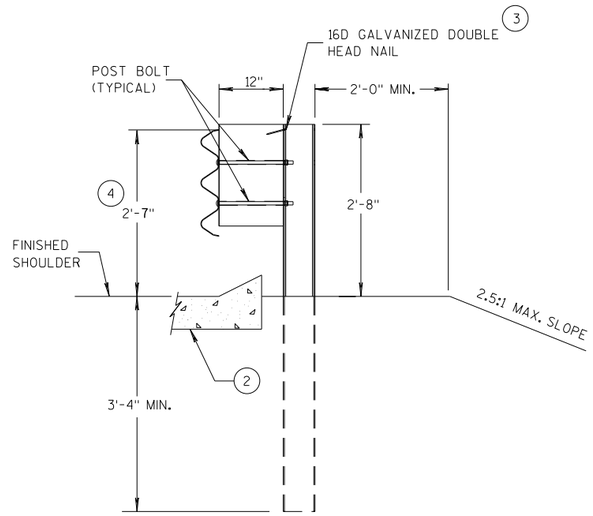
SECTION A-A (13)

POSTS 1-5



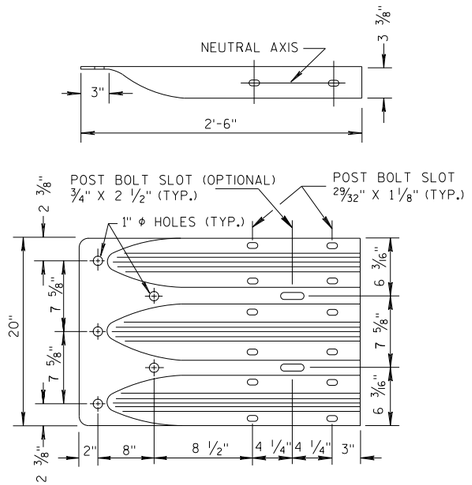
SECTION B-B

POST 6

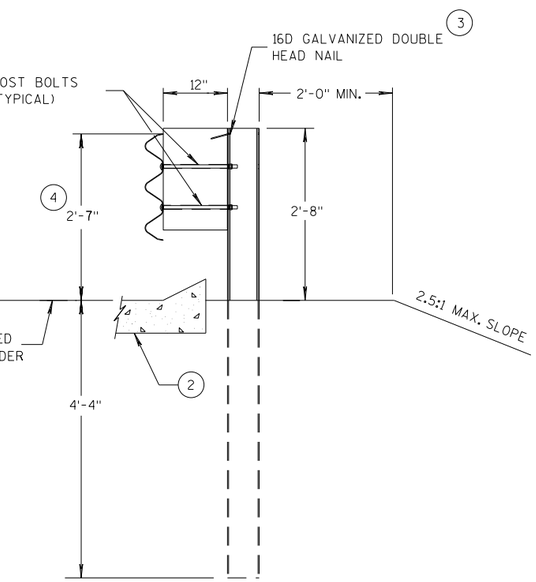


SECTION C-C

POSTS 7-11

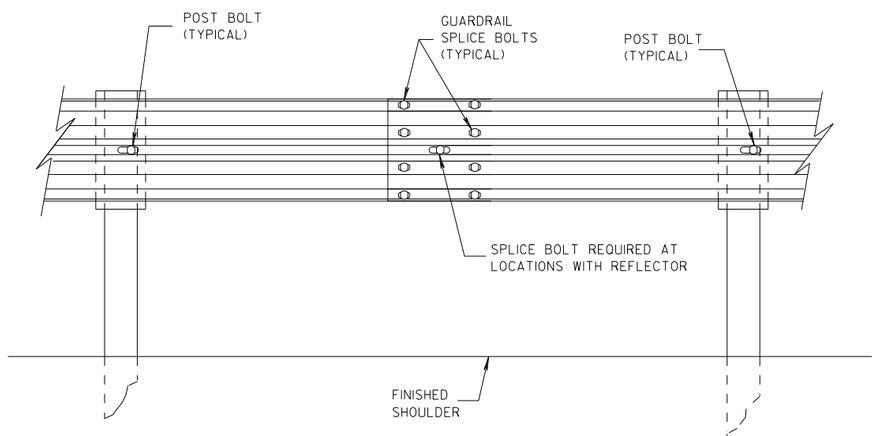


THRIE BEAM TERMINAL CONNECTOR



SECTION D-D

POSTS 12-17



SPLICE DETAIL

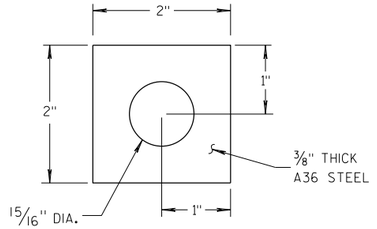
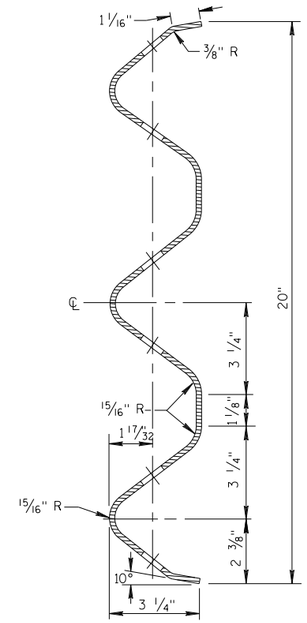


PLATE WASHER DETAIL



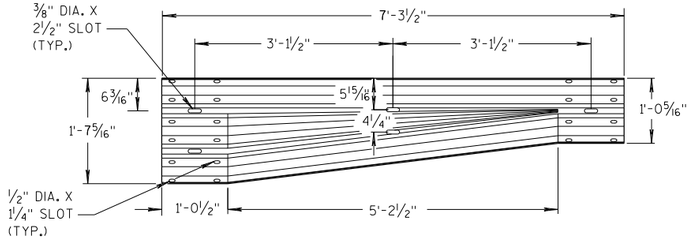
SECTION THRU THRIE BEAM RAIL ELEMENT

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

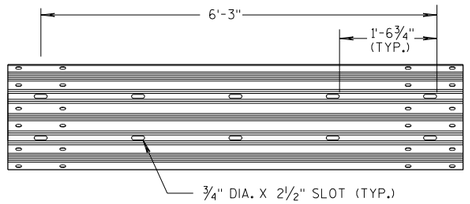
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

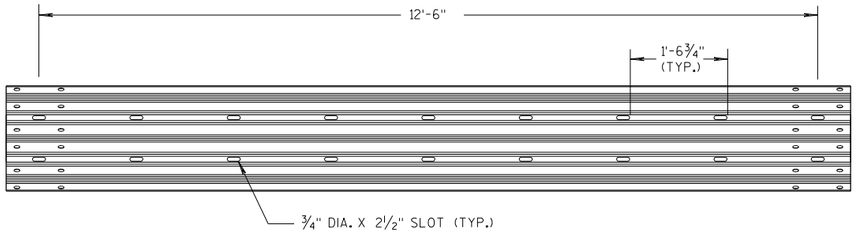
S.D.D. 14 B 45-5b



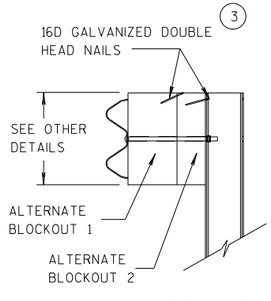
W-BEAM TO THRIE BEAM TRANSITION SECTION



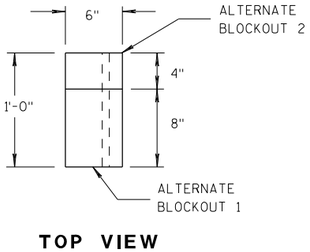
6'-3" THRIE BEAM SECTION



12'-6" THRIE BEAM SECTION

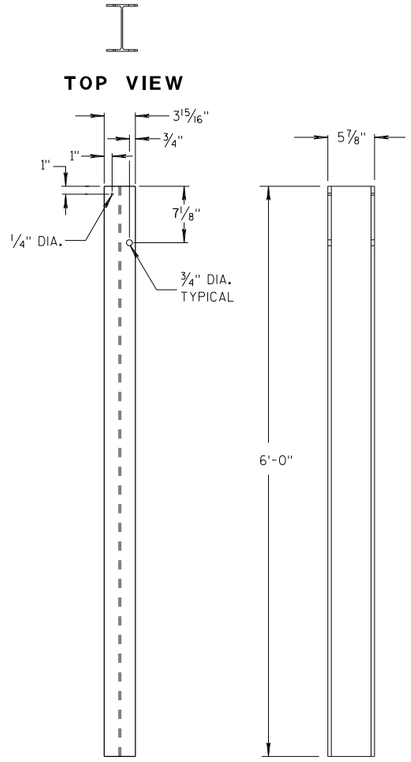


SIDE VIEW



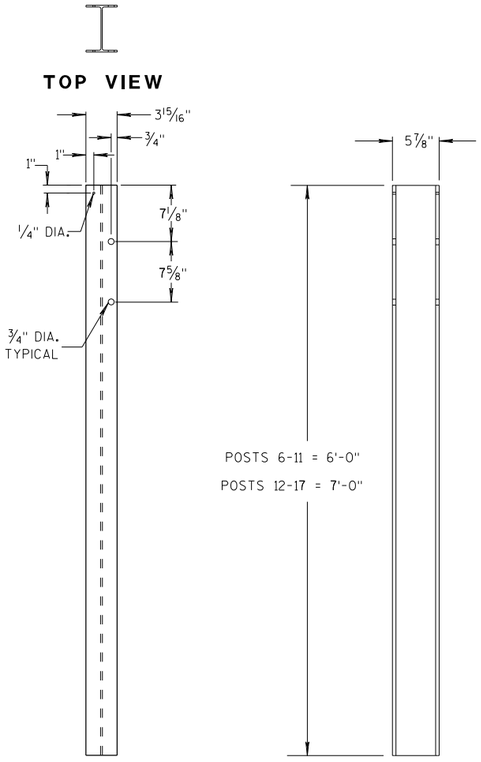
TOP VIEW

ALTERNATE WOOD BLOCKOUT DETAIL



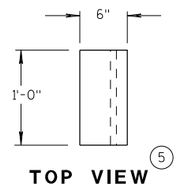
FRONT VIEW SIDE VIEW

STEEL POSTS 1-5 (13)

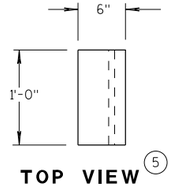


FRONT VIEW SIDE VIEW

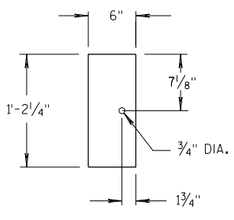
STEEL POSTS 6-17



TOP VIEW (5)

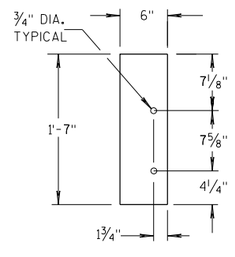


TOP VIEW (5)



FRONT VIEW

BLOCKOUT POSTS 1-5



FRONT VIEW

BLOCKOUT POSTS 6-17

GENERAL NOTES

STEEL POSTS ARE W6X9 OR W6X8.5.

BOLT HOLES FOR POST ARE ON FRONT AND OF SIDE OF POST.

(3) WHEN USING STEEL POSTS AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

(5) WOOD BLOCKS MAY BE CONSTRUCTED OUT OF 2 WOOD BLOCKS. SEE ALTERNATE WOOD BLOCK DETAIL.

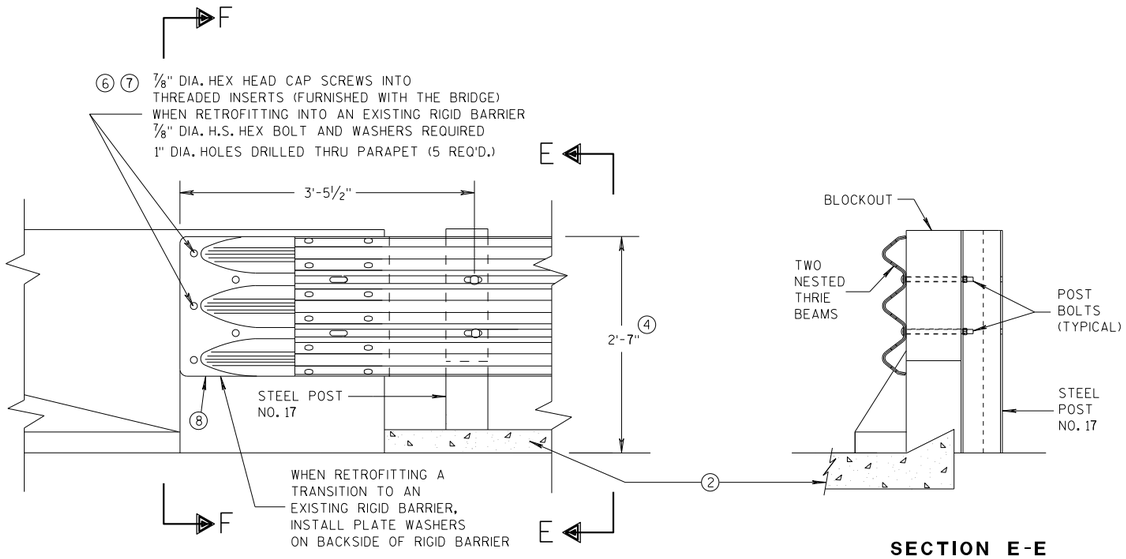
(13) STEEL OR WOOD POST IS ACCEPTABLE AT POST 1. SEE SDD 14B42.

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 14 B 45-5c



FRONT VIEW

SECTION E-E

THRIE BEAM CONNECTION TO BRIDGE PARAPET WITH SQUARE ENDS

GENERAL NOTES

THESE ARE TYPICAL CONNECTION DETAILS. ADJUST THE POSTION OF CONNECTIONS TO EXISTING BRIDGES TO FIT THE ACTUAL BRIDGE AND SITE DIMENSIONS.

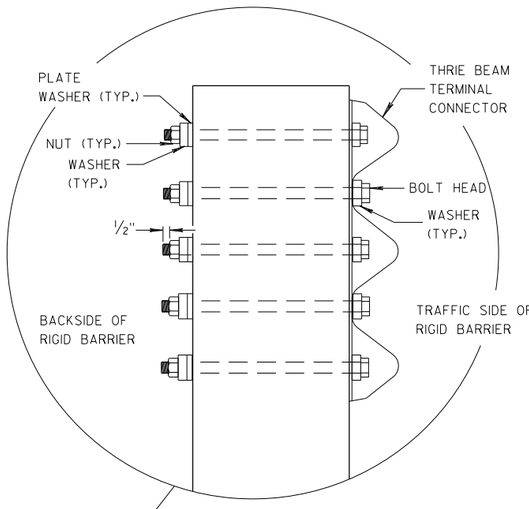
② OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

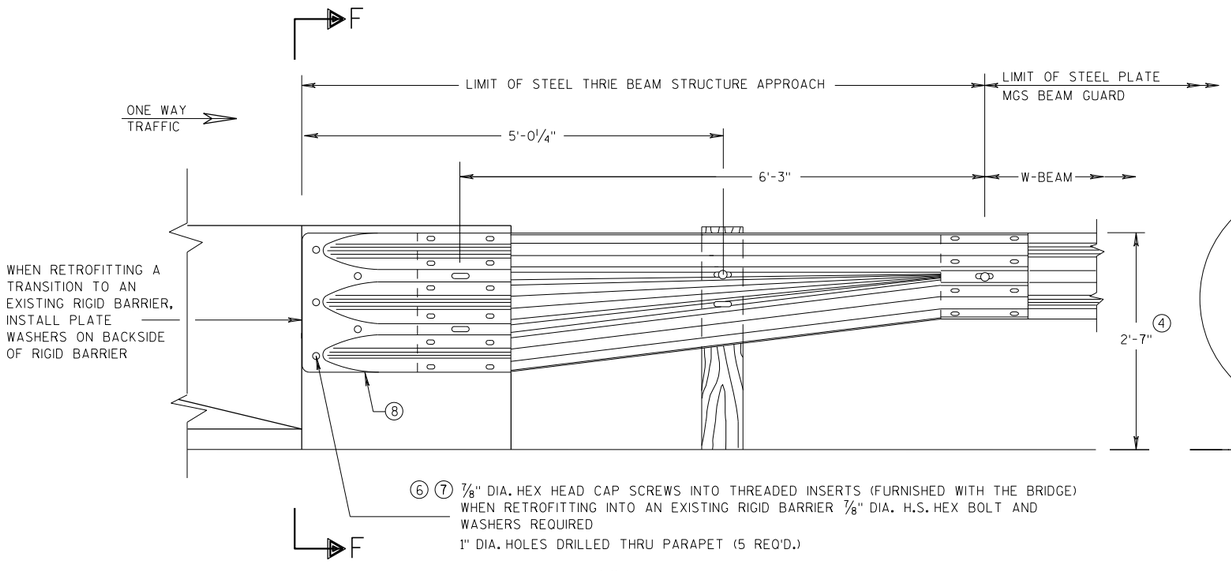
④ TOLERANCE FOR TOP OF BEAM IS ± 1".

⑥ DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

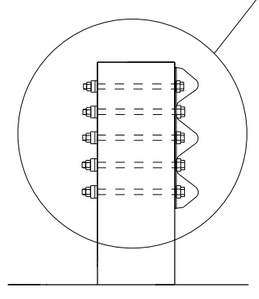
⑦ BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X 5/32" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.

⑧ THE RECESS FOR A W-BEAM CONNECTION, WHICH EXISTS ON SOME PARAPETS OF THIS TYPE, SHALL BE FILLED WITH A TREATED TIMBER BLOCKOUT. BLOCKOUT SIZE IS 1'-6" X 2'-0" X 3 1/2".

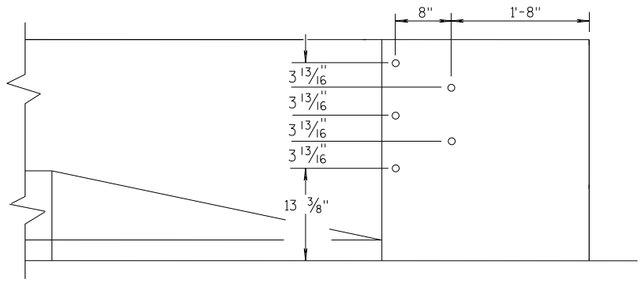




FRONT VIEW



SECTION F-F



DRILL HOLE LOCATION

W BEAM TRANSITION AND CONNECTION TO BRIDGE PARAPETS WITH SQUARE ENDS

(USE ONLY ON THE TRAFFIC EXIT END OF ONE WAY BRIDGES)

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
07/2018 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS D[illegible] UNIT SUPERVIS[illegible]NT
FHWA

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S.D.D. 14 B 45-5d

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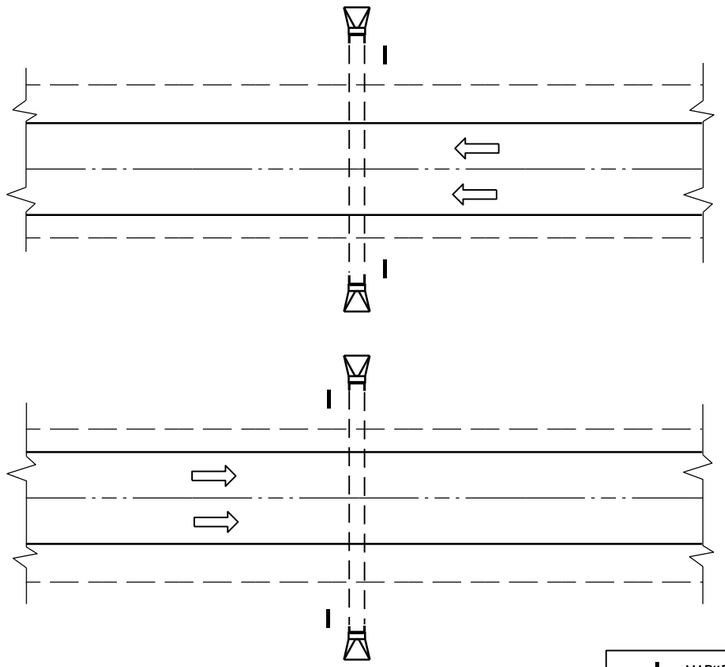
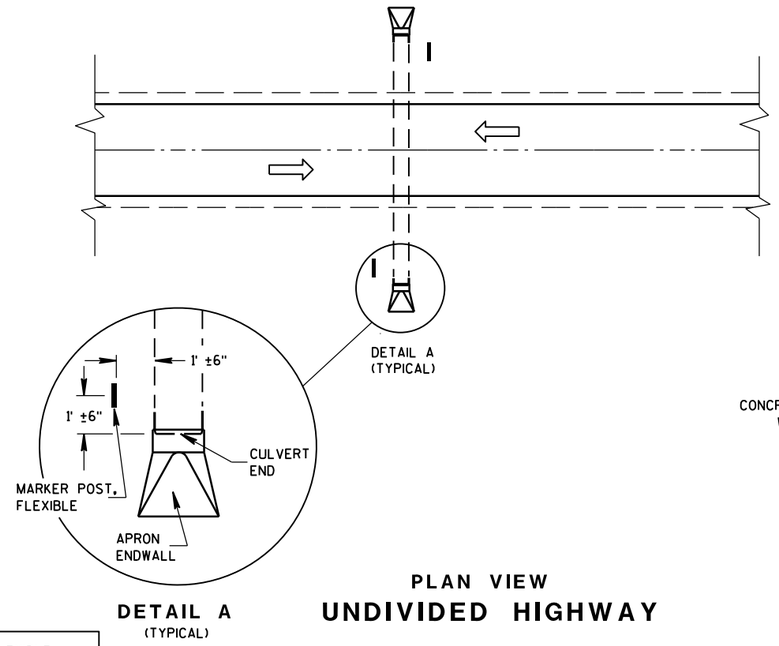
S.D.D. 14 B 45-5d



PLAN VIEW

DIVIDED HIGHWAY



DETAIL A
(TYPICAL)

PLAN VIEW

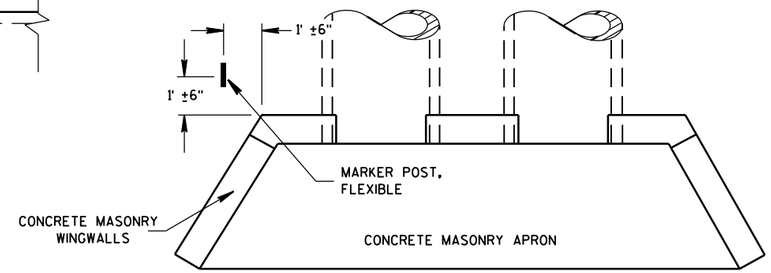
UNDIVIDED HIGHWAY

MARKER POST, FLEXIBLE

DIRECTION OF TRAFFIC FLOW

GENERAL NOTES

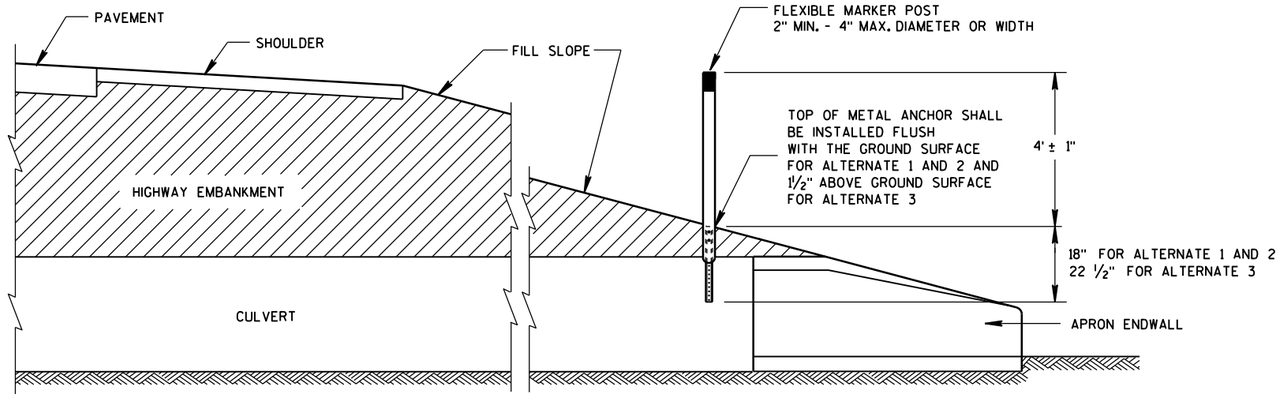
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



PLAN VIEW

CONCRETE MASONRY ENDWALLS FOR CULVERT PIPE AND PIPE ARCH

FLEXIBLE MARKER POST LOCATION



CROSS SECTION

FLEXIBLE MARKER POST

FLEXIBLE MARKER POST
FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

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S.D.D. 15 A 3-2a

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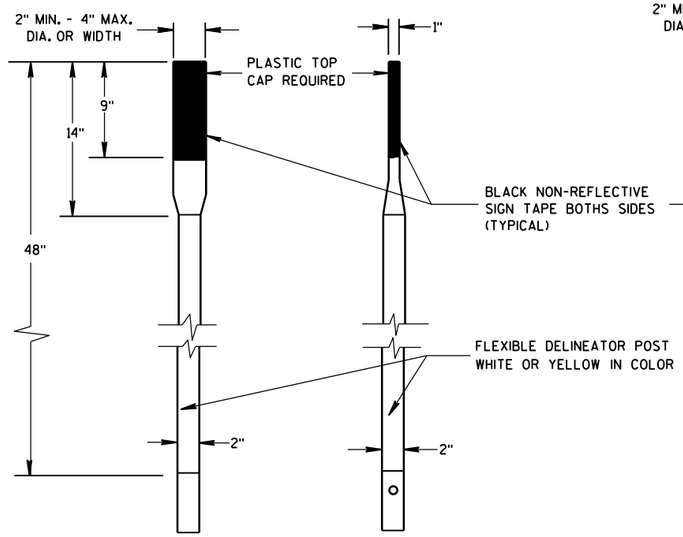
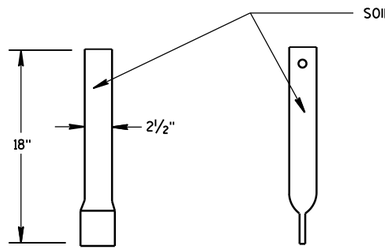
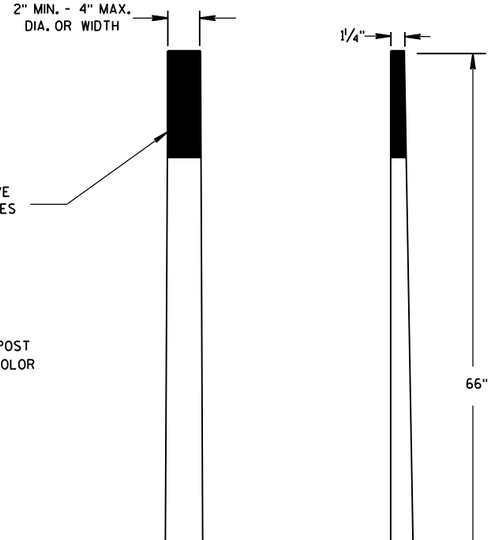
S.D.D. 15 A 3-2a





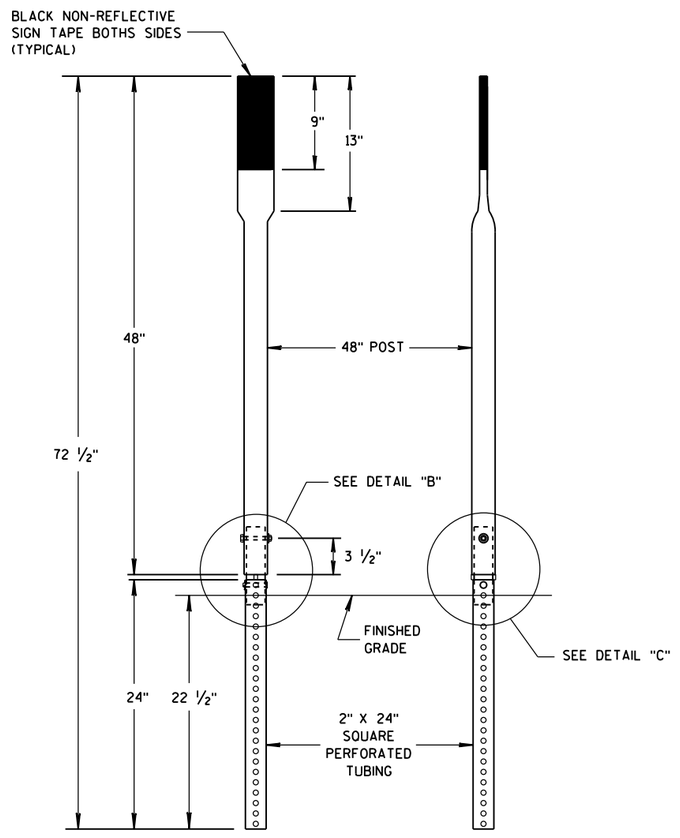
FRONT VIEW SIDE VIEW

ALTERNATE 1



FRONT VIEW SIDE VIEW

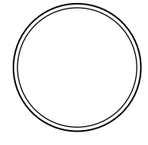
ALTERNATE 2



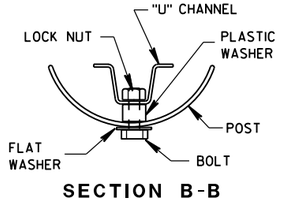
FRONT VIEW SIDE VIEW

ALTERNATE 3

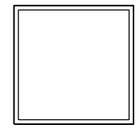
FLEXIBLE MARKER POSTS



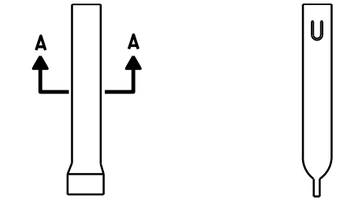
SECTION A-A



SECTION B-B

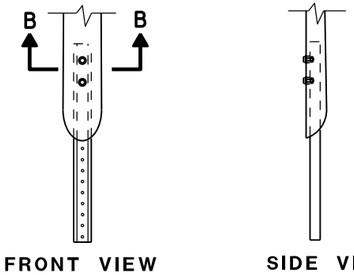


SECTION C-C



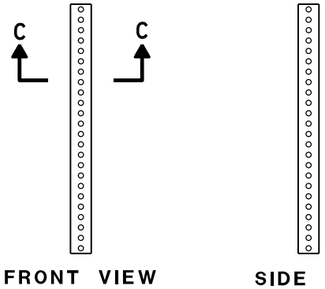
FRONT VIEW SIDE VIEW

ALTERNATE 1



FRONT VIEW SIDE VIEW

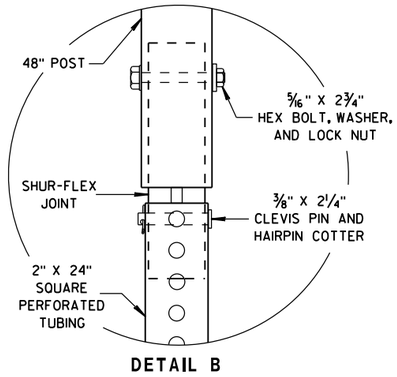
ALTERNATE 2



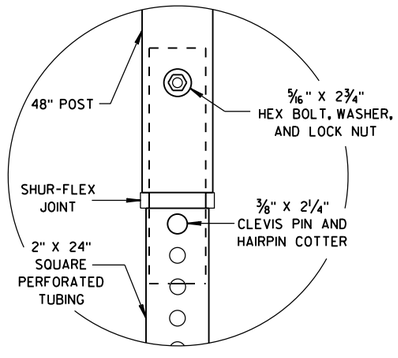
FRONT VIEW SIDE VIEW

ALTERNATE 3

FLEXIBLE MARKER POST ANCHORS



DETAIL B



DETAIL C

FLEXIBLE MARKER POST
FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/1/2012
DATE
/S/ Travis Feltes
STATE TRAFFIC ENGINEER
FHWA

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S.D.D. 15 A 3-2b

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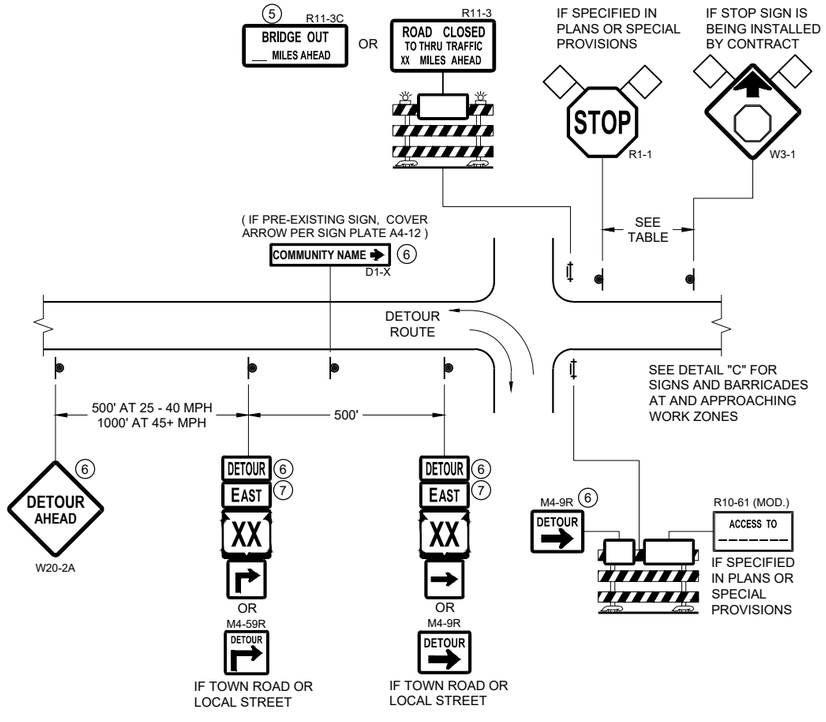
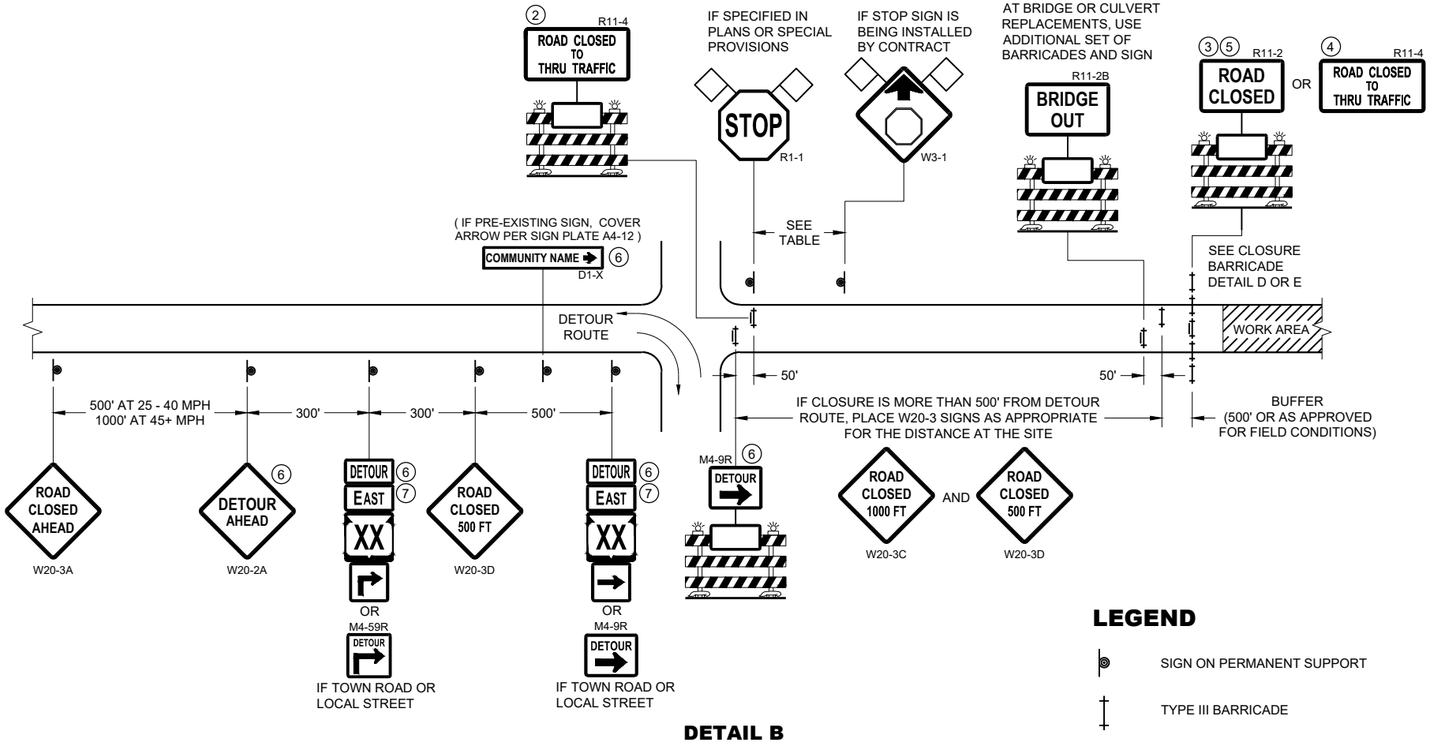
S.D.D. 15 A 3-2b



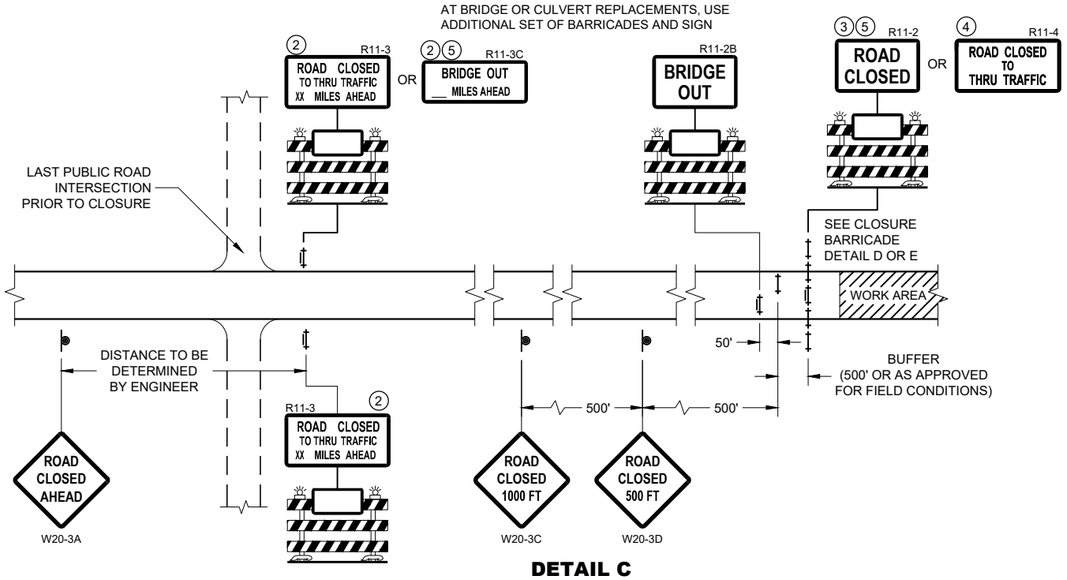
DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN OR EQUAL TO ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

DETOUR M4 - 8
EAST M3 - X
XX M1 - 4 OR XX M1 - 6 OR COUNTY X M1 - 5A
M05 - 1 OR M06 - 1

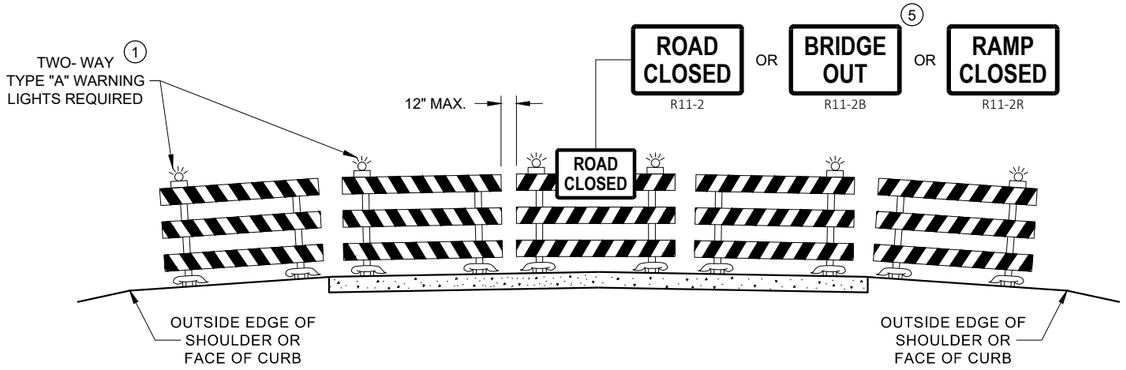
BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

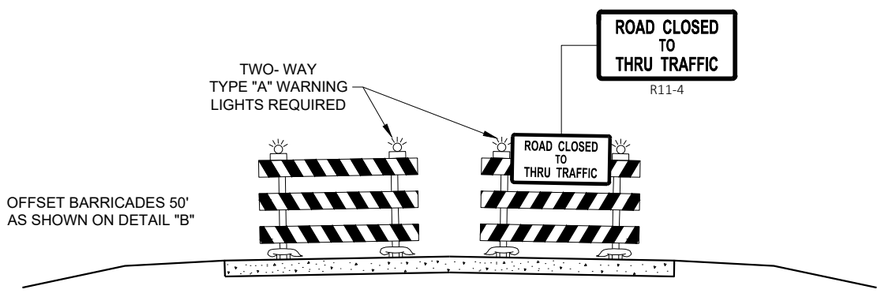
APPROVED
May 2023
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

SDD 15C02 - 09a



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60 " X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING.

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR VARIOUS CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEE
FHWA

6

SDD 15C02 - 09b

6

SDD 15C02 - 09b

THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- DETOUR / EAST — M4 - 8, M3 - X
- XX — M1 - 4; OR XX — M1 - 6; OR COUNTY X — M1 - 5A
- M05 - 1; OR M06 - 1; OR M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-59 SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

* OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.

** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT

M3-X 24" X 12"
M1-X 24" X 24"
DETOUR AHEAD
W20-2A
USE ON STATE TRUNK, U.S. OR INTERSTATE HIGHWAYS, OR AS SPECIFIED IN THE CONTRACT

500' AT 25-40 MPH
1000' AT 45+ MPH

500'

DETOUR EAST XX *

D1-X COMMUNITY NAME *
(IF PRE-EXISTING SIGN, COVER ARROW PER SIGN PLATE A4-12)

M4-8a END DETOUR WEST XX *

DETOUR EAST XX / WEST XX

DETOUR ROUTE

WORK AREA

DETOUR EAST XX

WEST XX OR M4-9L DETOUR ** IF TOWN ROAD OR LOCAL STREET

500'

DETOUR NEXT X MILES * G20-51

DETOUR WEST XX * OR M4-59L DETOUR ** IF TOWN ROAD OR LOCAL STREET

MATCH POINT

* M4-59L DETOUR ** OR DETOUR EAST XX * IF TOWN ROAD OR LOCAL STREET

D1-X COMMUNITY NAME * (IF PRE-EXISTING SIGN, COVER ARROW PER SIGN PLATE A4-12)

500'

M4-9L DETOUR ** OR DETOUR EAST XX IF TOWN ROAD OR LOCAL STREET

DETOUR WEST XX

DETOUR WEST XX OR M4-9R DETOUR ** IF TOWN ROAD OR LOCAL STREET

DETOUR WEST XX * OR M4-59R * DETOUR ** IF TOWN ROAD OR LOCAL STREET

500'

DETOUR ROUTE

500' AT 25 - 40 MPH
1000' AT 45+ MPH

500'

M3-X 24" X 12"
M1-X 24" X 24"
DETOUR AHEAD
W20-2A
USE ON STATE TRUNK, U.S. OR INTERSTATE HIGHWAYS, OR AS SPECIFIED IN THE CONTRACT

DETOUR EAST XX *

D1-X COMMUNITY NAME *
(IF PRE-EXISTING SIGN, COVER ARROW PER SIGN PLATE A4-12)

DETOUR EAST XX

500'

DETOUR EAST XX

D1-X COMMUNITY NAME *
(IF PRE-EXISTING SIGN, COVER ARROW PER SIGN PLATE A4-12)

DETOUR EAST XX *

PLACE SIGNS BEYOND INTERSECTIONS WITH STATE OR COUNTY TRUNK HIGHWAYS OR AT 4 MILE MAXIMUM SPACING (4 BLOCKS IF URBAN AREA)

500' AT 25 - 40 MPH
1000' AT 45+ MPH

DETOUR EAST XX *

M3-X 24" X 12"
M1-X 24" X 24"
DETOUR AHEAD
W20-2A
USE ON STATE TRUNK, U.S. OR INTERSTATE HIGHWAYS, OR AS SPECIFIED IN THE CONTRACT

SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

DETAIL F
DETOUR SIGNING

DETOUR SIGNING FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE

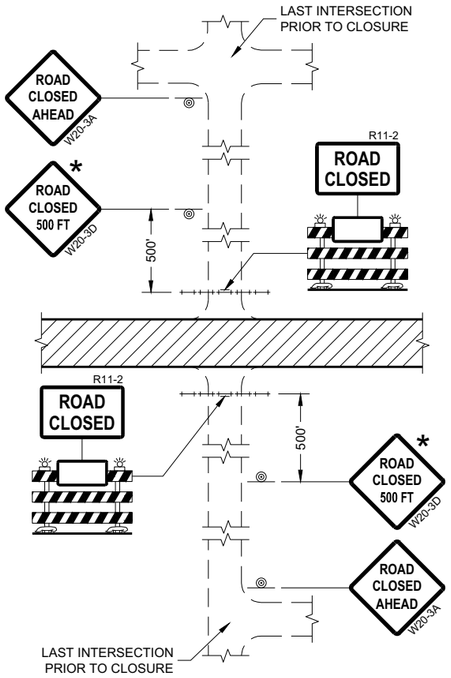
/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

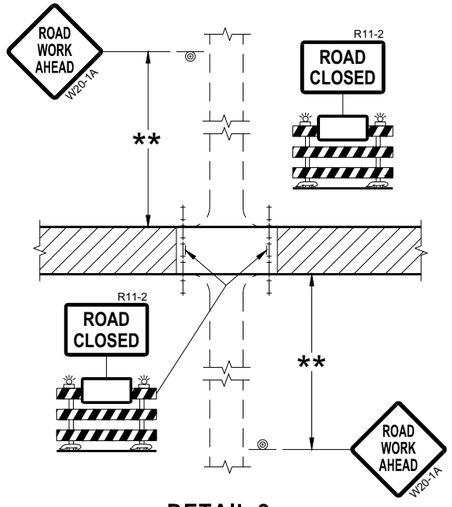
6

SDD 15C02 - 09c

SDD 15C02 - 09c



DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

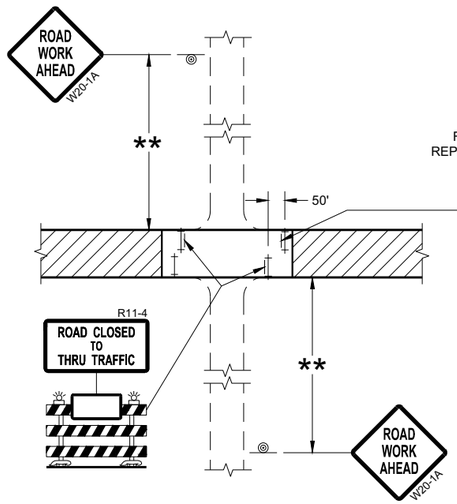
TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

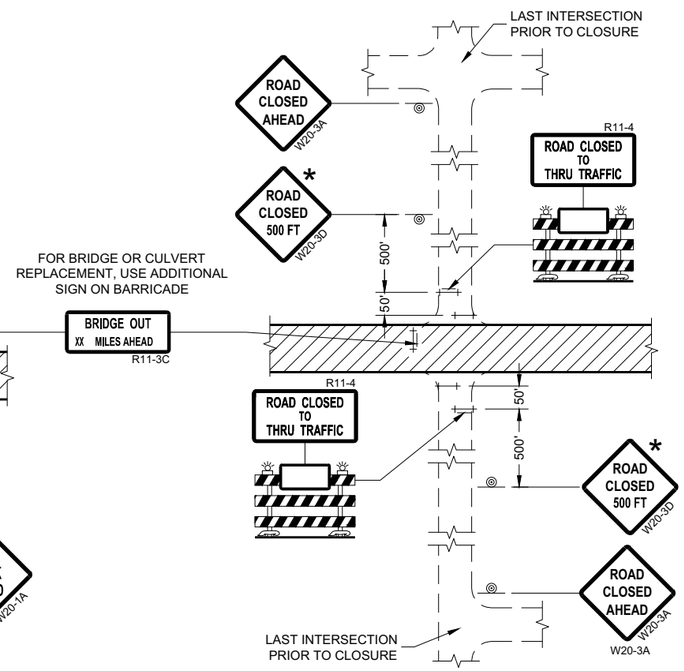
ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

* OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.

** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6

SDD 15C03 - 05

SDD 15C03 - 05

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

(1) LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING

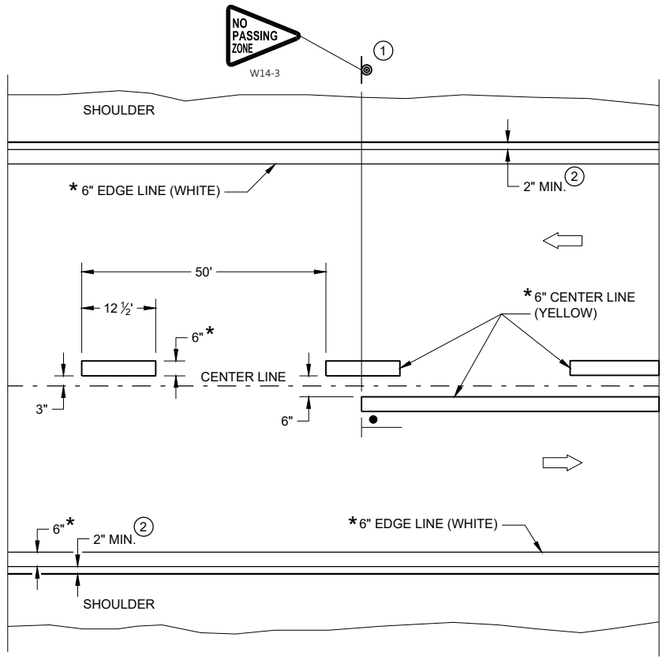
(2) MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

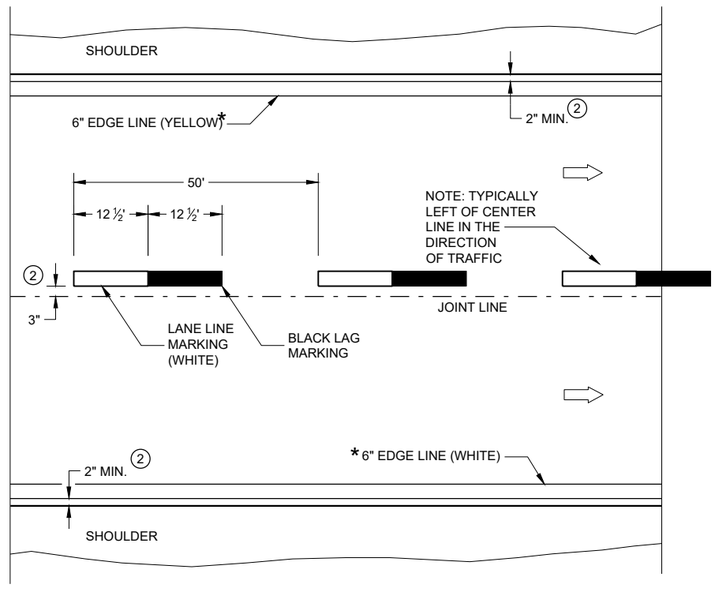
"T" MARKING

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

May 2023 — DATE

/S/ Jeannie Silver — STATEWIDE SIGNING AND M ENGINEER

FHWA

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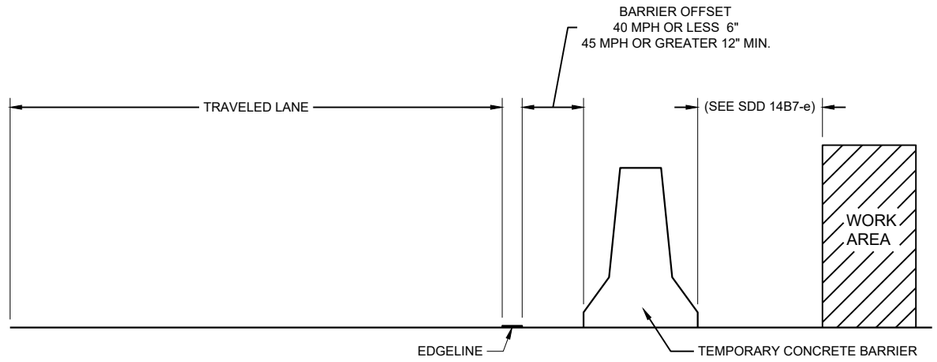


TEMPORARY BARRIER OFFSET FROM EDGELINE

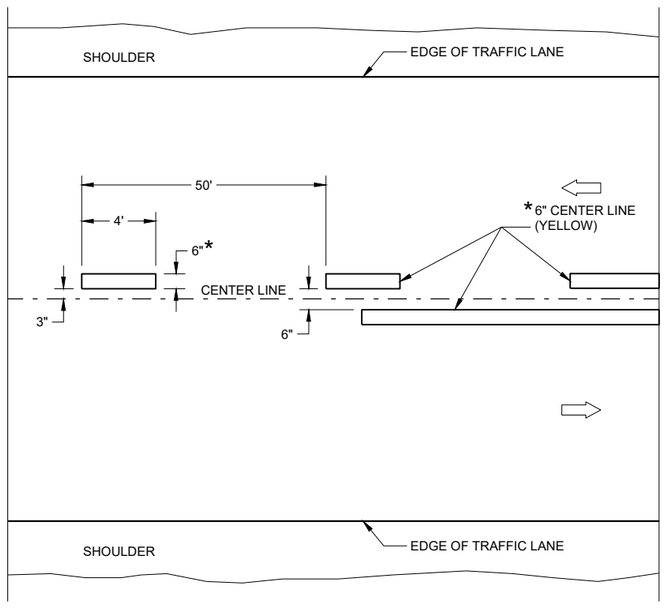
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

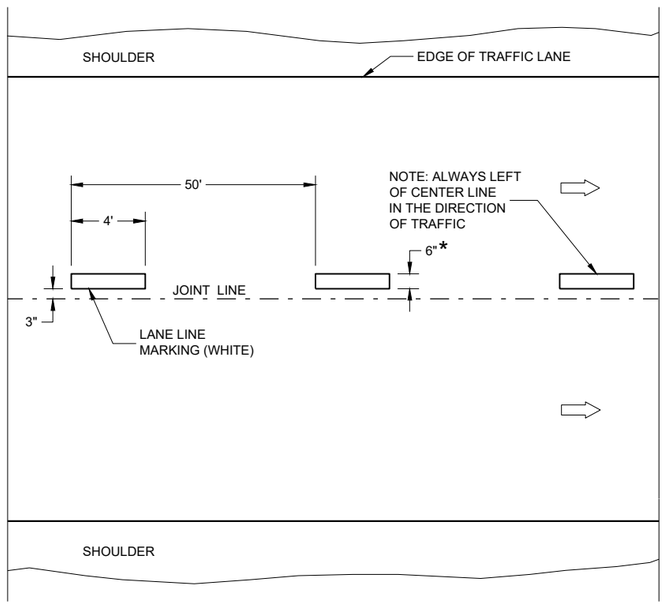
LEGEND

⇨ DIRECTION OF TRAFFIC

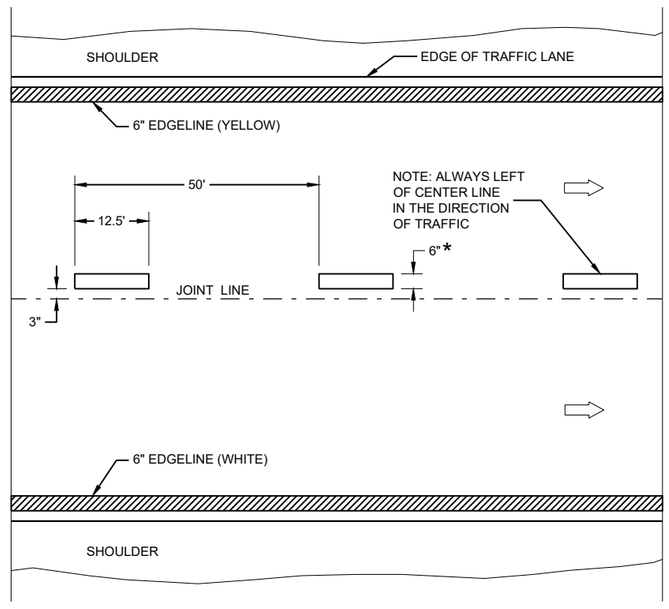
* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Jeannie Silver — STATEWIDE SIGNING AND M ENGINEER

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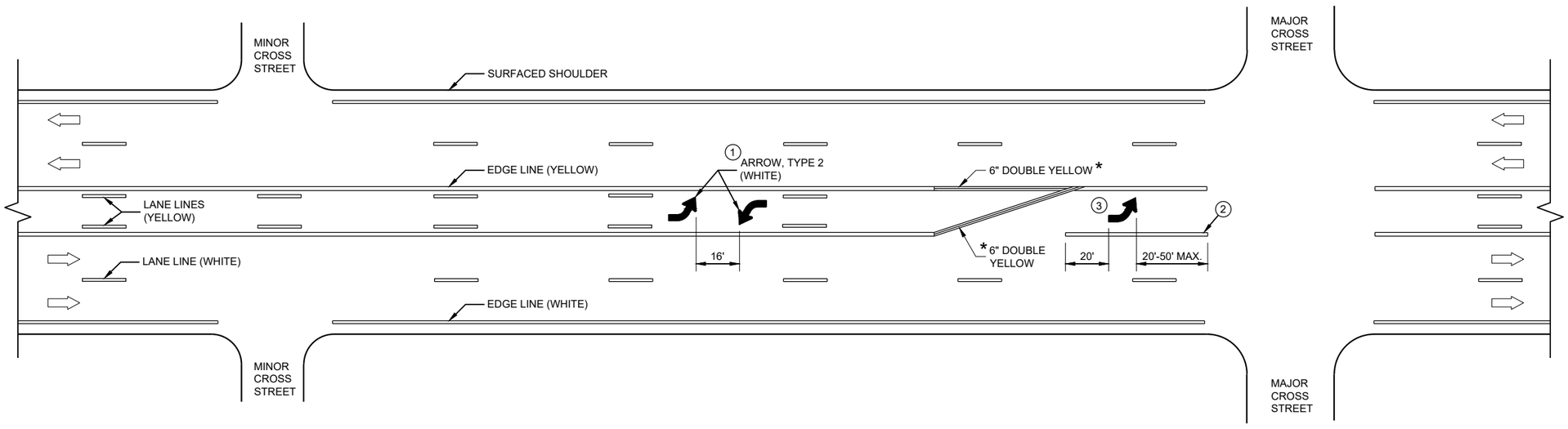
GENERAL NOTES

1. A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
2. 10" WHITE
3. TURN BAY LENGTH OF LESS THAN 48' DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT.

DIRECTION OF TRAFFIC

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY LEFT TURN LANE

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PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTA· 138

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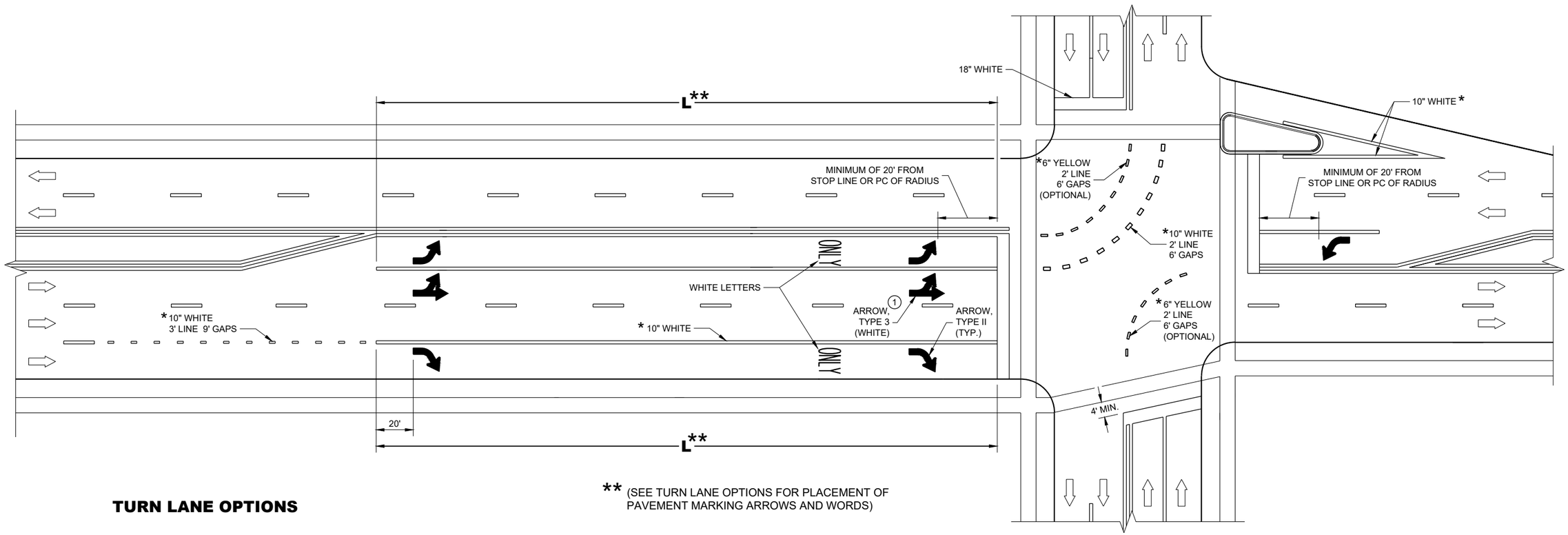
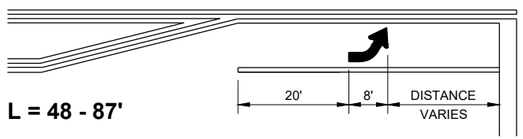
SDD 15C08-23c



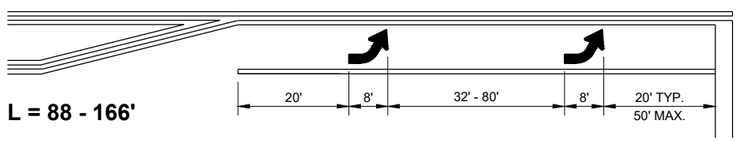
** (SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

TURN LANE OPTIONS

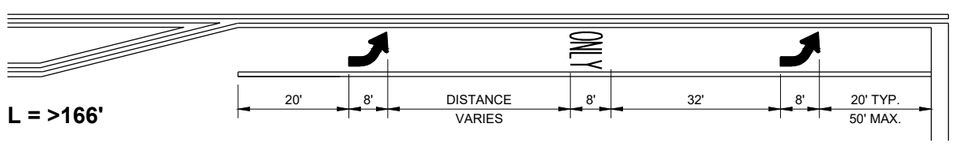
LENGTH OF TURN BAY (**L**) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



L = 48 - 87'



L = 88 - 166'



L = >166'

GENERAL NOTES

(1) QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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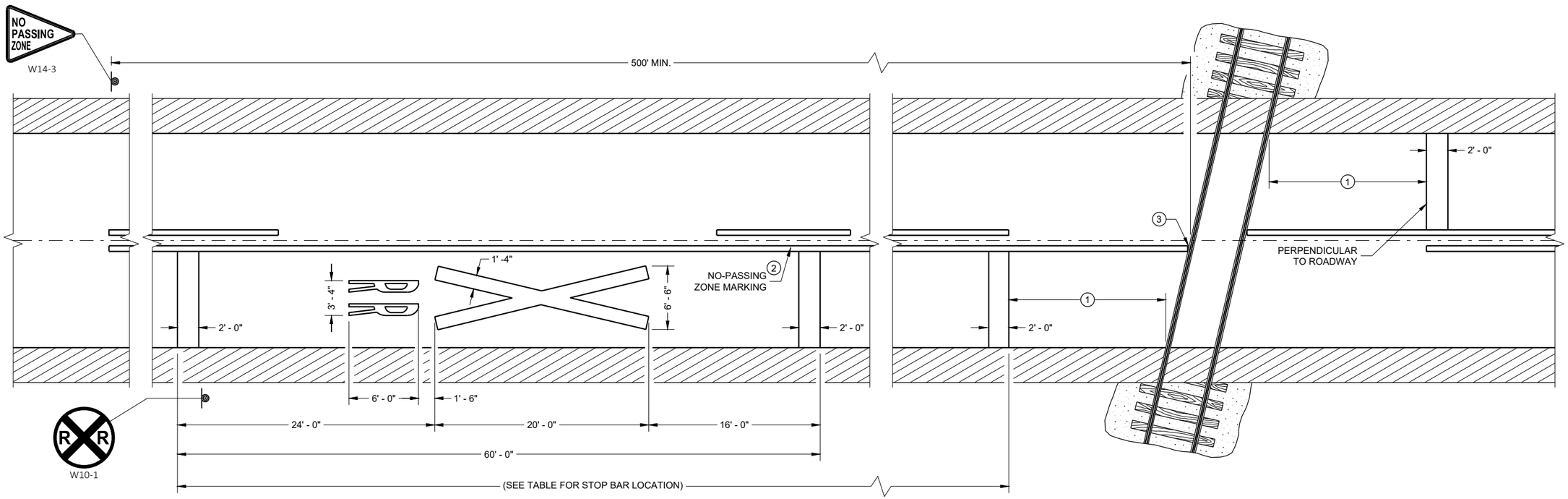
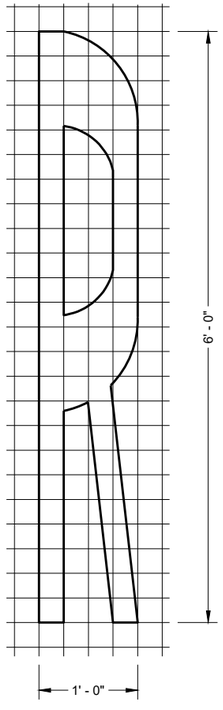
SDD 15C08-23d

SDD 15C08-23d



PAVEMENT MARKING



LEGEND

SIGN ON PERMANENT SUPPORT

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

ON MULTI-LANE ROADS THE TRANSVERSE BANDS SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.

CENTER OR LANE LINES AND NO-PASSING ZONE MARKINGS SHOWN ON THIS DRAWING ARE REQUIRED AND PAID FOR UNDER OTHER ITEMS IN THE CONTRACT.

TRACE EXISTING SYMBOL WHERE EXISTING SYMBOLS ARE PLACED.

(1) PLACE STOP BAR APPROXIMATELY 8 FEET IN ADVANCE OF THE GATE (IF PRESENT), BUT NO CLOSER THAN 15 FEET IN ADVANCE OF THE NEAREST RAIL. FIELD-FIT STOP BAR TO MAXIMIZE VIEW OF APPROACHING TRAIN.

(2) 500' MINIMUM. MARKING LIMITS MAY BE EXTENDED AS DIRECTED BY THE ENGINEER TO MEET ADJACENT NO-PASSING ZONE MARKINGS.

(3) FOR MULTIPLE TRACK CROSSINGS, THE BARRIER LINE SHALL EXTEND TO THE NEAR RAIL OF THE FURTHEST TRACK IN THE DIRECTION OF HIGHWAY TRAVEL.

DISTANCE TABLE

TABLE BASED UPON 2C-4 WISCONSIN SUPPLEMENT OF MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

POSTED SPEED (M.P.H.)	DIMENSION RANGE (FEET)
25	150* - 250'
30	200* - 300'
35	250* - 450'
40	300* - 500'
45	400* - 650'
50	550* - 800'
55	750* - 1000'
60	1000* - 1250'
65	1000* - 1250'

* THE MINIMUM DISTANCES IN THE TABLE ARE DESIRABLE AND SHOULD BE USED. THE DISTANCES MAY BE INCREASED UP TO THE MAXIMUM TO ALLOW FOR FIELD CONDITIONS SUCH AS THE CLOSED PROXIMITY OF DRIVEWAYS, BRIDGES, SIDE ROADS OR OTHER FEATURES THAT WOULD PROHIBIT THE MINIMUM DISTANCES FROM BEING USED.

SIGNING AND PAVEMENT MARKING DETAILS FOR RAILROAD - HIGHWAY GRADE CROSSINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Matthew R. Rauch — STATE SIGNING AND MAF ENGINEER

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SDD 15C09-13a

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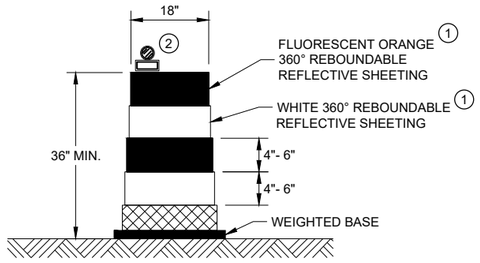
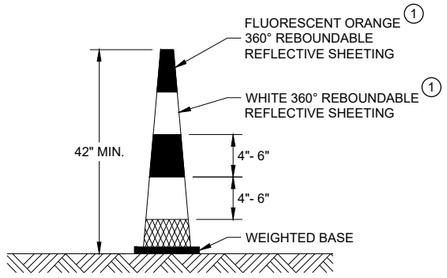
GENERAL NOTES

1. REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
2. LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



DRUM

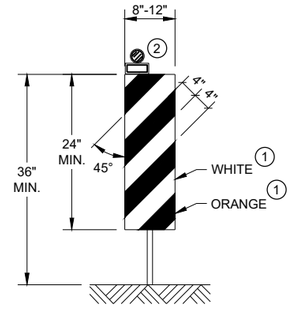
BALLAST WIDTHS
RANGE FROM 24"-36"



42" CONE

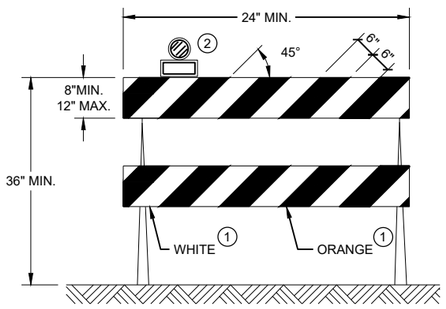
DO NOT USE IN TAPERS
½ SPACING OF DRUMS

BALLAST WIDTHS
RANGE FROM 14"-20"



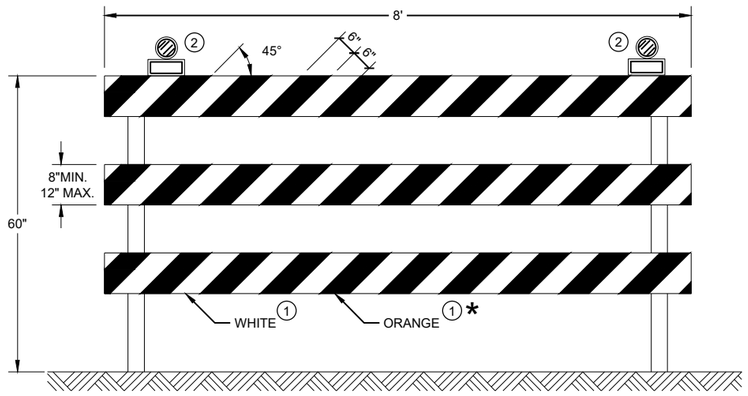
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

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SDD 15C11 - 10b

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LEGEND

SIGN ON PORTABLE OR PERMANENT SUPPORT

TEMPORARY PORTABLE RUMBLE STRIP ARRAY

DIRECTION OF TRAFFIC

WORK AREA

FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

(1) FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.

(2) SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

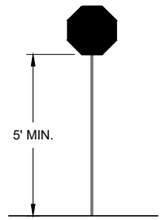
(3) EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



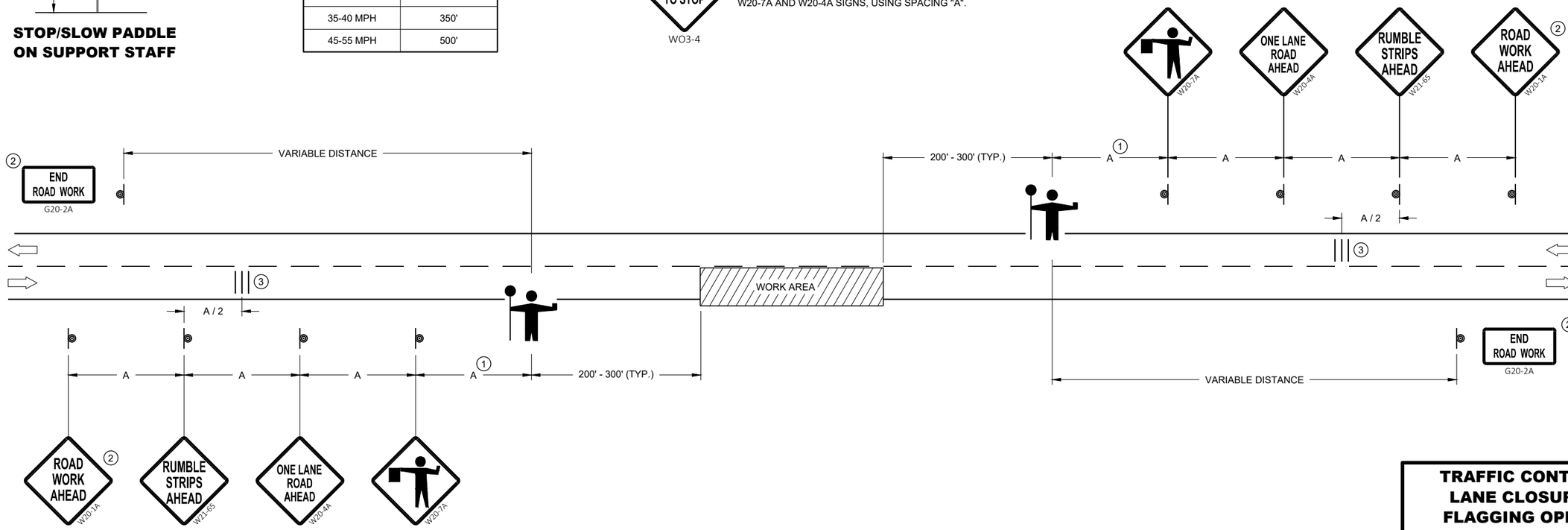
STOP/SLOW PADDLE ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'

WO3-4

USE OF WO3-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

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SDD 15C12 - 09a

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SDD 15C12 - 09a

GENERAL NOTES

SIGN ON PERMANENT SUPPORT

TRAFFIC CONTROL CONE 42-INCH

TRAFFIC CONTROL DRUM

TEMPORARY PORTABLE RUMBLE STRIP ARRAY

DIRECTION OF TRAFFIC

WORK AREA

AFAD — AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD)

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

IF THE AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD) STOPS WORKING, FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

(1) SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

(2) IF FLAGGERS ARE PHYSICALLY NEEDED TO FLAG, REPLACE WO3-4 SIGNS WITH W20-7A SIGNS.

TEMPORARY PORTABLE RUMBLE STRIPS

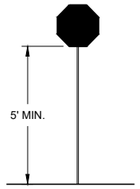
UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

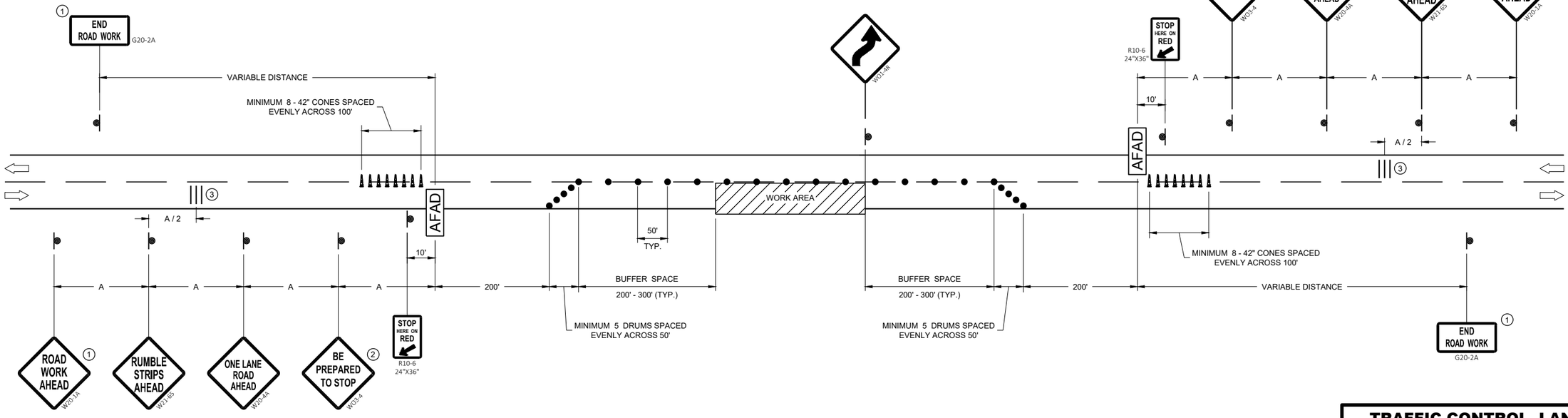
(3) EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSELY AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER.



STOP/SLOW PADDLE ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



TRAFFIC CONTROL, LANE CLOSURE WITH AUTOMATED FLAGGER ASSISTANCE DEVICE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

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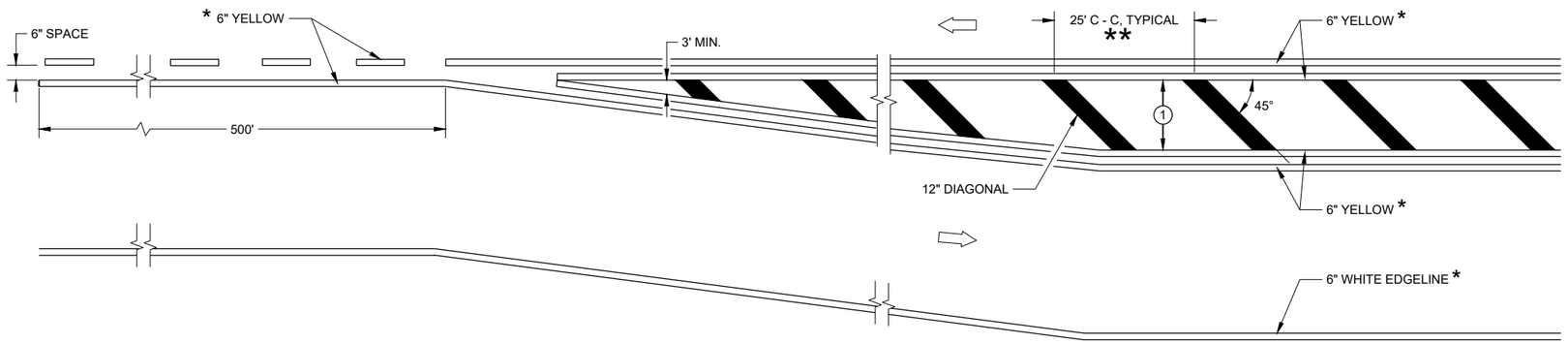
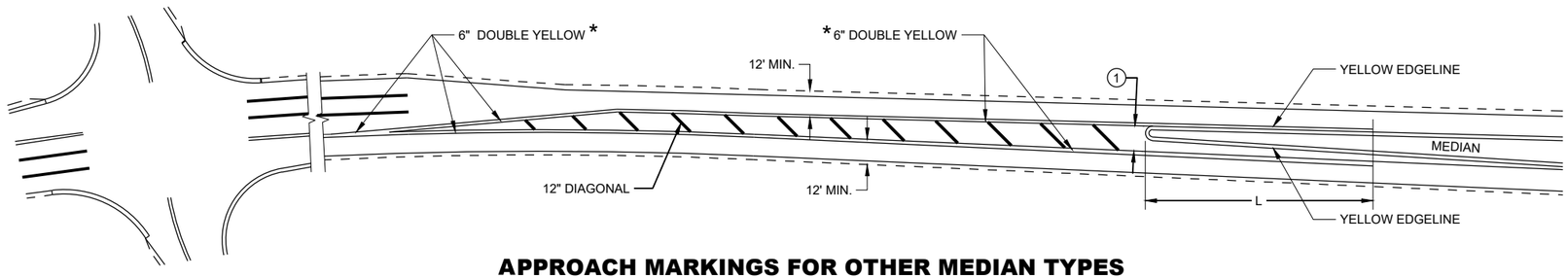
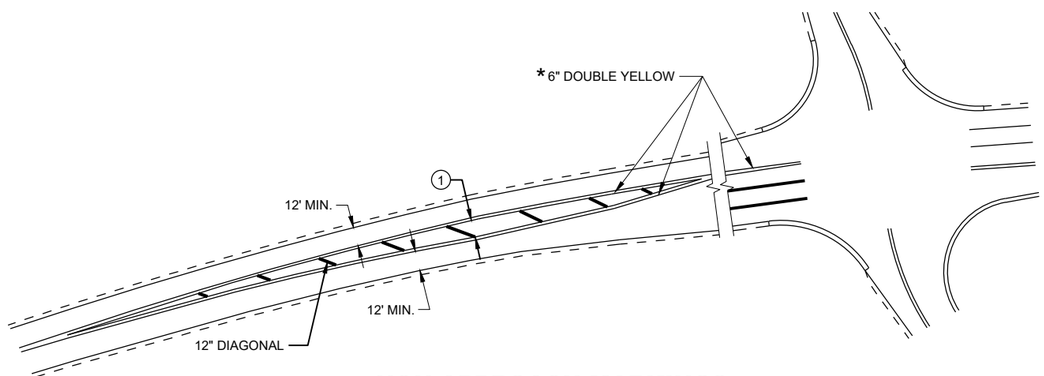


MEDIAN ISLAND DETAIL



APPROACH MARKINGS FOR OTHER MEDIAN TYPES



NON-APPROACH MARKINGS

GENERAL NOTES

1. DIAGONALS ARE OPTIONAL WHEN PAINTED ISLAND IS LESS THAN 6 FEET AT THE WIDEST POINT. OMIT DIAGONALS IF WIDTH IS LESS THAN 4 FEET.

⇨ DIRECTION OF TRAVEL

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

SPEED LIMIT	L
<35 MPH	5'
35> MPH	50'

MEDIAN ISLAND PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Jeannie Silver — STATE SIGNING AND MAF ENGINEER

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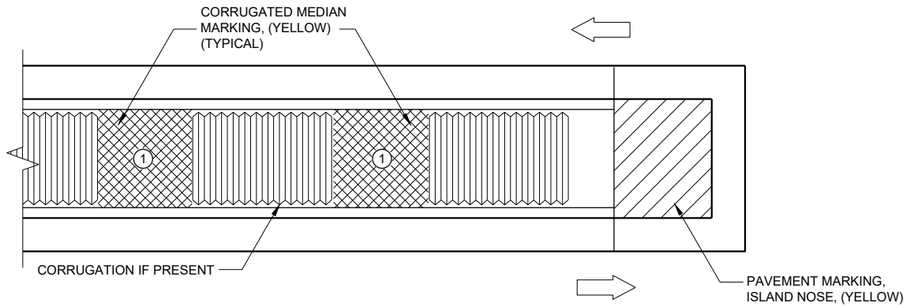
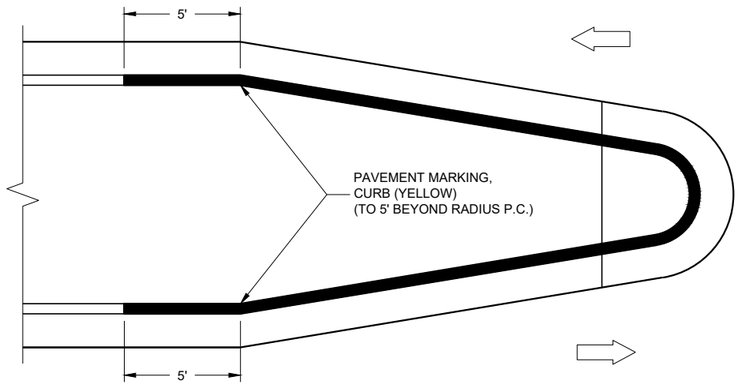
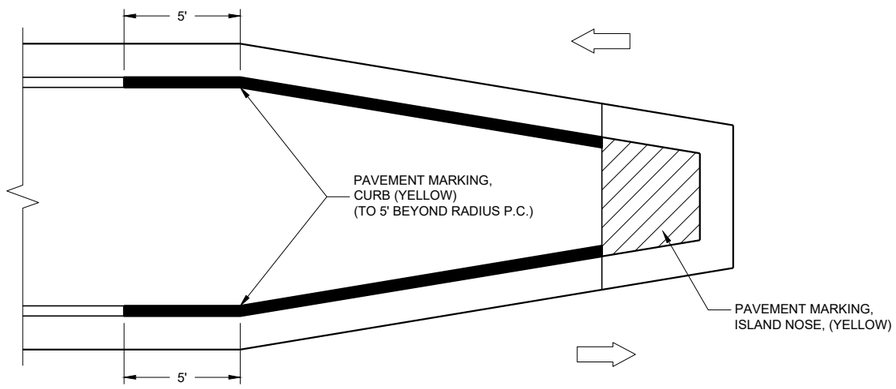


MEDIAN ISLAND WITH SQUARE BLUNT NOSE



MEDIAN ISLAND WITH ROUND BLUNT NOSE



MEDIAN ISLAND WITH SLOPED NOSE

TYPICAL PLACEMENT OF PAVEMENT MARKING ON MEDIAN ISLANDS

GENERAL NOTES

WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION, YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN. THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.

(1) APPLY PAVEMENT MARKING TO THE FLAT PORTION OF CORRUGATED MEDIAN.

ISLAND NOSE MARKING

CURB MARKING

CORRUGATED MEDIAN MARKING

DIRECTION OF TRAVEL

PAVEMENT MARKINGS, MEDIAN ISLAND NOSE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

May 2023
DATE

/S/ Jeannie Silver
STATE SIGNING AND MAR ENGINEER

FHWA

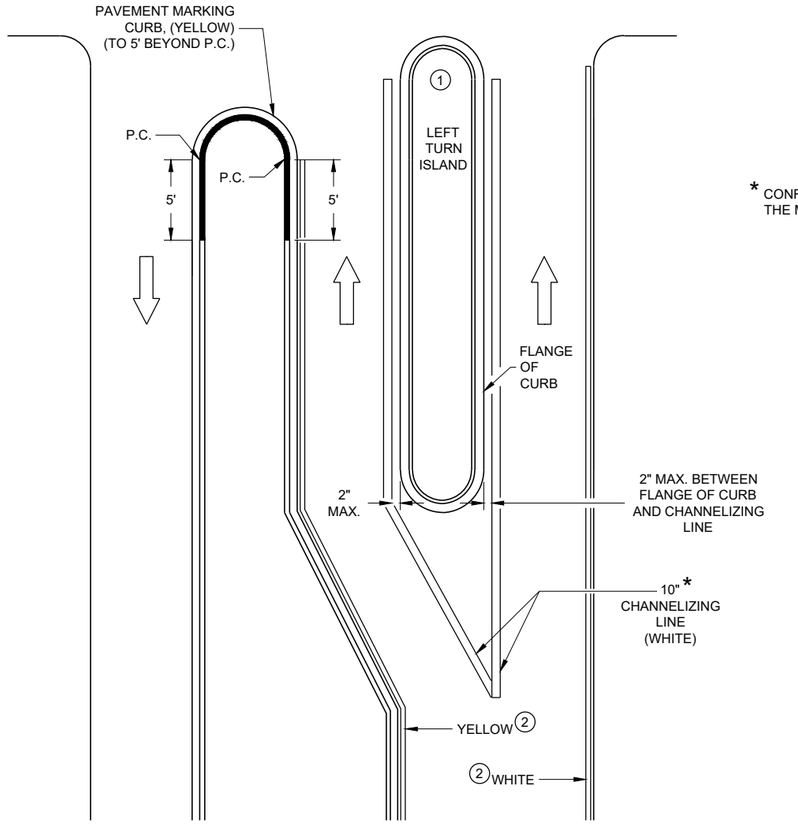
6

SDD 15C18-08b

6

SDD 15C18-08b

REQUIREMENTS FOR EDGE LINES		
POSTED SPEED	IS THERE CONTINUOUS LIGHTING?	
	YES	NO
≤ 30 MPH	NO	OPTIONAL
35 OR 40 MPH	OPTIONAL	RECOMMENDED
≥ 45 MPH	RECOMMENDED	REQUIRED



* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

LEFT TURN & MEDIAN ISLAND

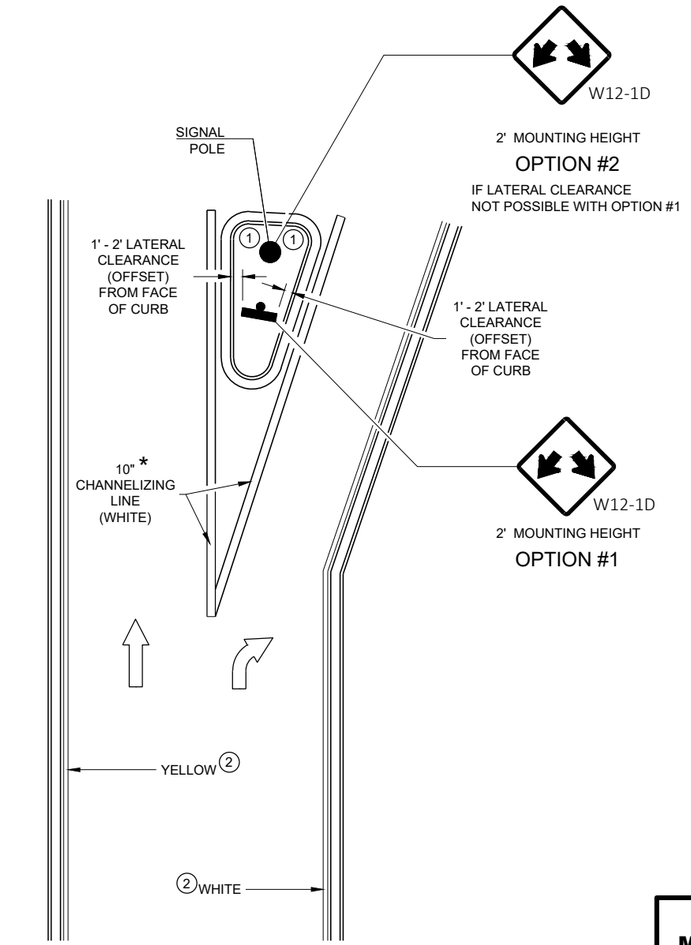
GENERAL NOTES

APPLIES TO ISLANDS AT LEFT TURNS AT ONE WAY ROADWAYS AS WELL.

SEE MISCELLANEOUS QUANTITIES FOR SIGN SIZE.

1. MARK CURB NOSES YELLOW.
2. MARK ACCORDING TO TABLE.

DIRECTION OF TRAVEL



RIGHT TURN ISLAND

MEDIAN PAVEMENT MARKINGS, DOUBLE ARROW WARNING SIGN PLACEMENT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

May 2023 — DATE

/S/ Jeannie Silver — STATE SIGNING AND MAF ENGINEER

FHWA

6

SDD 15C18-08c

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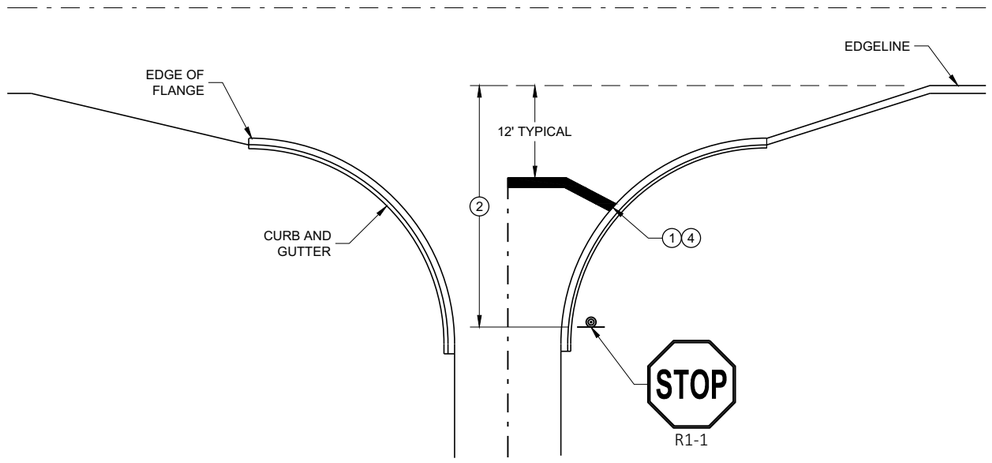
SDD 15C18-08c

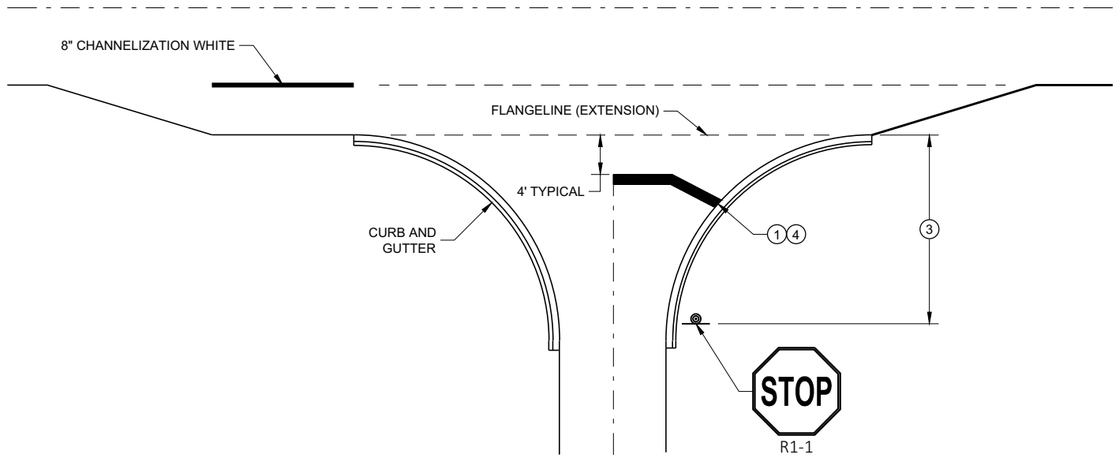
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

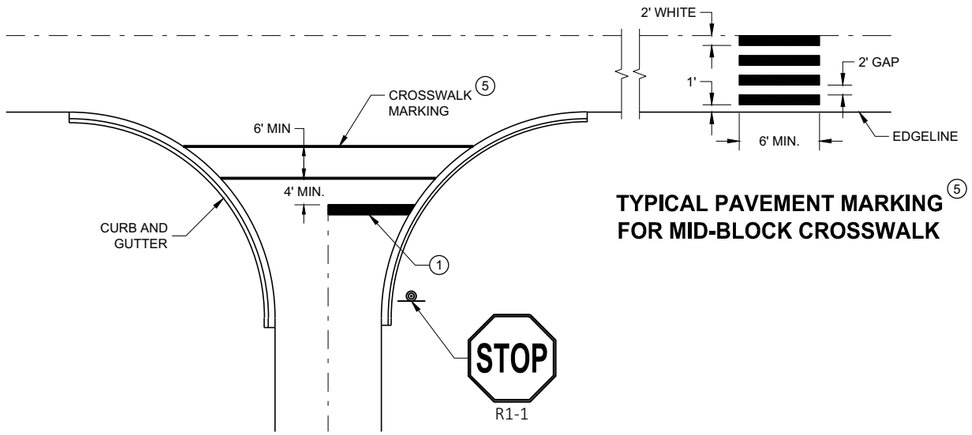
1. 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
2. NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
3. NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
4. MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
5. LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

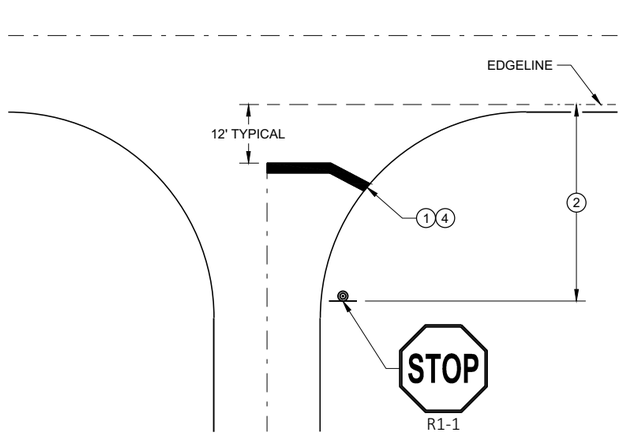


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING

TYPICAL PAVEMENT MARKING 5 FOR MID-BLOCK CROSSWALK



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 — DATE
/S/ Matthew Rauch — STATE SIGNING AND MARKING ENGINEER

FHWA

SDD 15C33 - 04

SDD 15C33 - 04

LEGEND

SIGN ON PERMANENT SUPPORT

TRAFFIC CONTROL DRUM

DIRECTION OF TRAFFIC

WORK ZONE

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

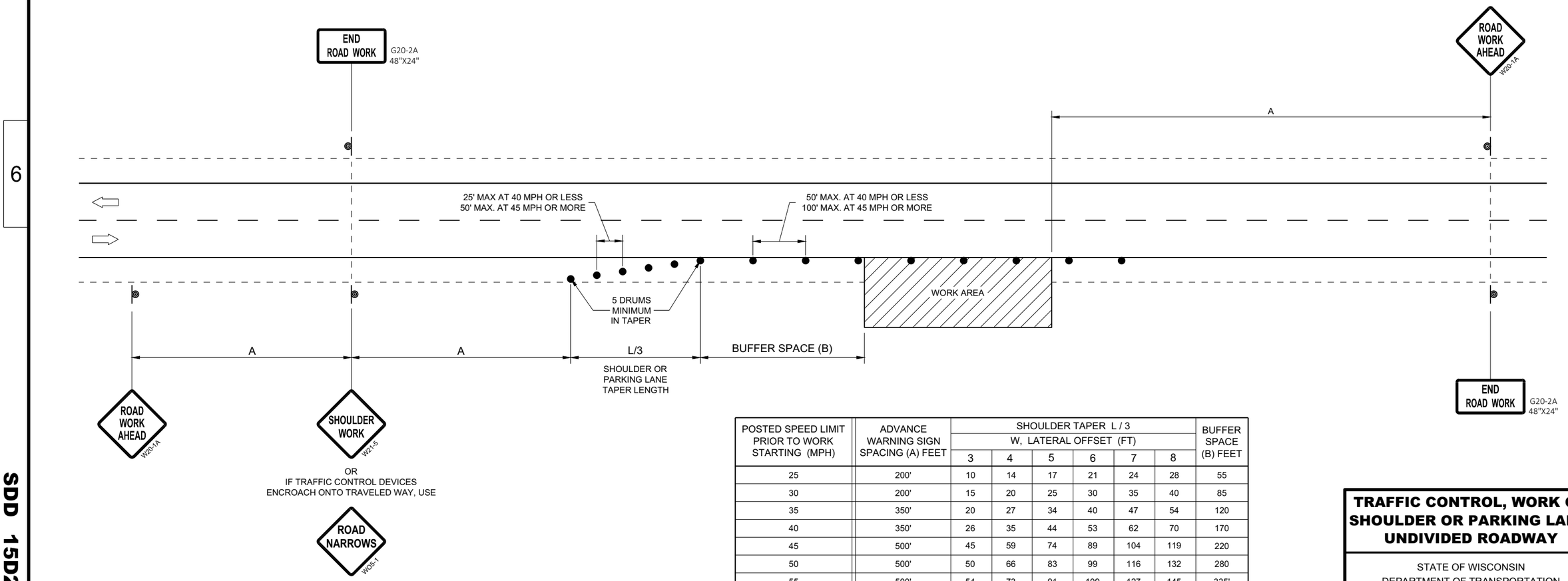
ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHOULDER TAPER L / 3 W, LATERAL OFFSET (FT)						BUFFER SPACE (B) FEET
		3	4	5	6	7	8	
25	200'	10	14	17	21	24	28	55
30	200'	15	20	25	30	35	40	85
35	350'	20	27	34	40	47	54	120
40	350'	26	35	44	53	62	70	170
45	500'	45	59	74	89	104	119	220
50	500'	50	66	83	99	116	132	280
55	500'	54	73	91	109	127	145	335'

TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2020
DATE

/S/ Andrew Heidtke
STATEWIDE WORK ZONE T SAFETY ENGINEER

FHWA

SDD 15D28 - 04

SDD 15D28 - 04

GENERAL NOTES

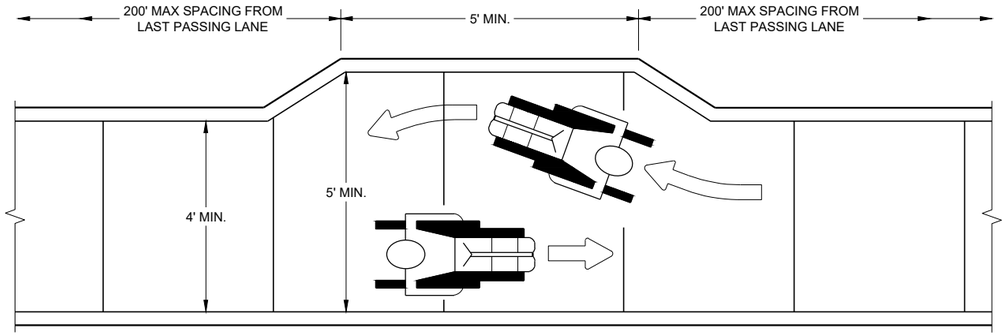
BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST

(1) REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.

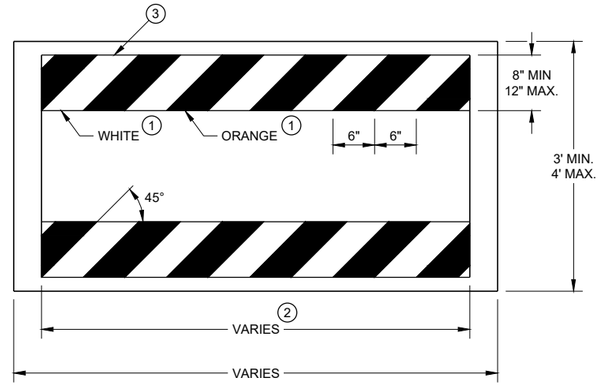
(2) SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.

(3) PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.

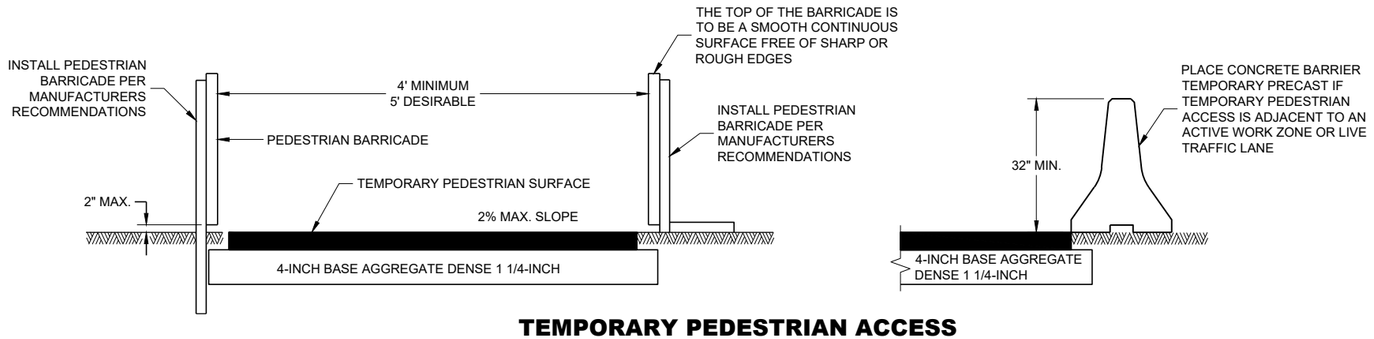
* USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



NARROW SIDEWALK PASSING DETAIL



TEMPORARY PEDESTRIAN BARRICADE*

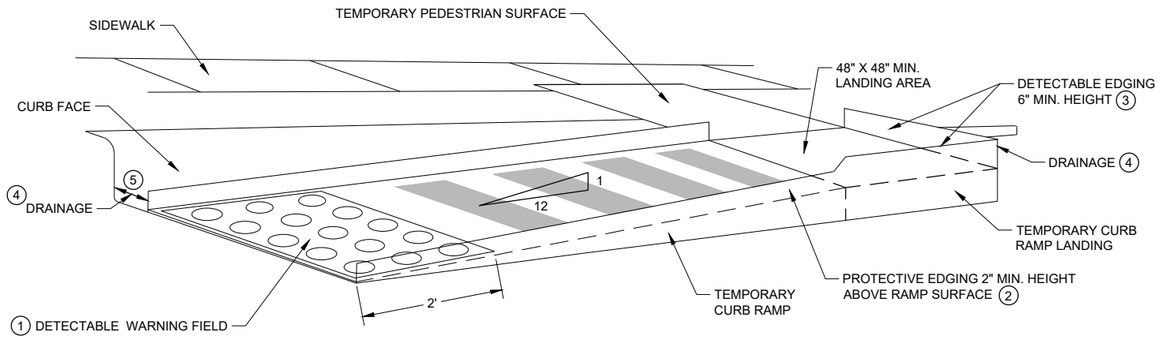


TEMPORARY PEDESTRIAN ACCESS

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

6

SDD 15D30 - 09a



PERSPECTIVE VIEW

GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP. LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN ½" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED ½". LATERAL EDGES MAY BE VERTICAL UP TO ¼" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN ¼" AND ½".

(1) INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.

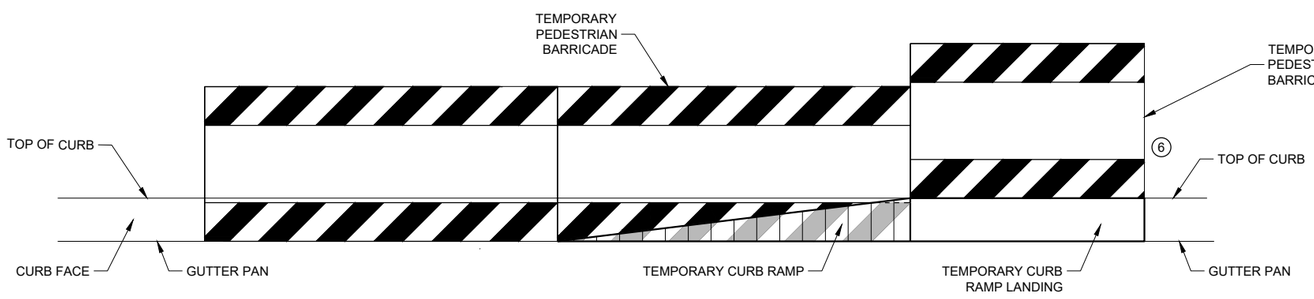
(2) PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.

(3) DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).

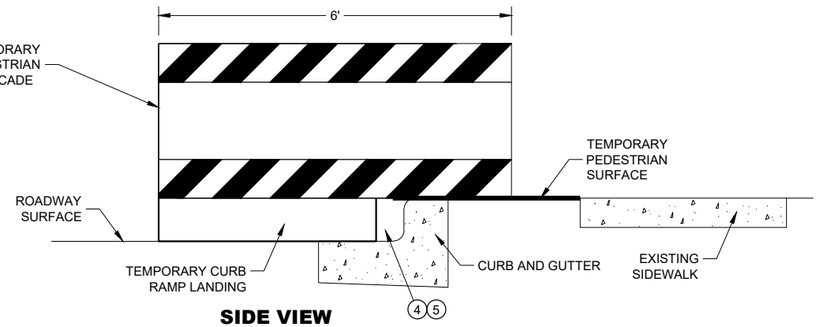
(4) DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.

(5) ENSURE CURB RAMP IS OUT OF THE GUTTER PAN.

(6) IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



FRONT VIEW



SIDE VIEW

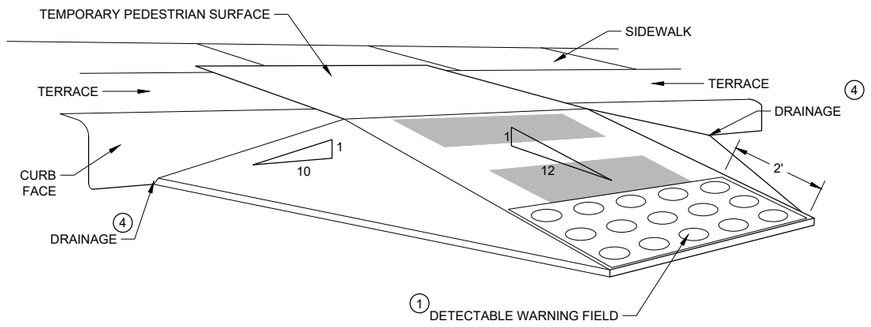
TEMPORARY CURB RAMP PARALLEL TO CURB

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

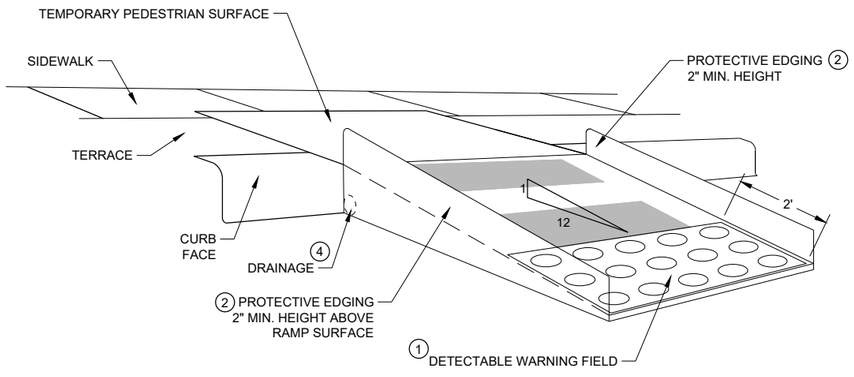
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 15D30 - 09b



WITH SIDE APRON (5)



WITH PROTECTIVE EDGE

TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN ½" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED ½". LATERAL EDGES MAY BE VERTICAL UP TO ¼" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN ¼" AND ½".

(1) INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS

(2) PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.

(3) DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).

(4) DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.

(5) CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

GENERAL NOTES

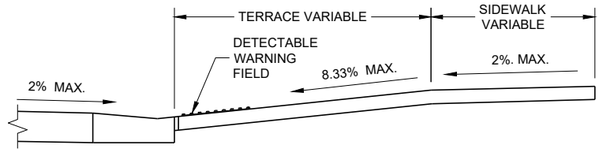
BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST

(1) REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.

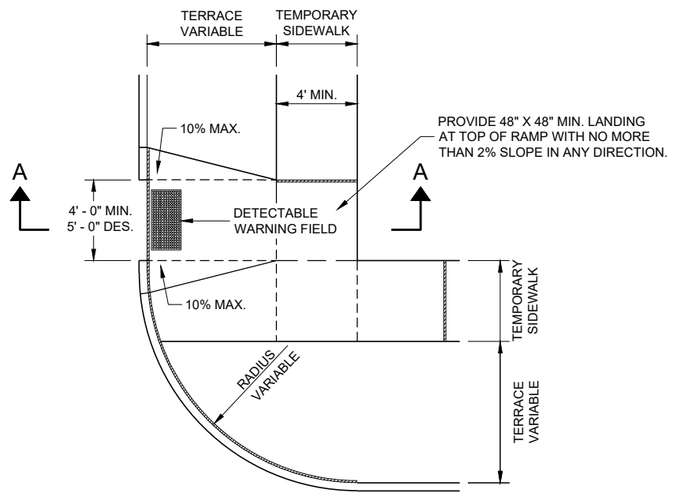
(2) SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.

(3) PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.

* USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



SECTION A - A



PLAN VIEW

TEMPORARY TYPE 3 RAMP

(OUTSIDE OF CROSSWALK AREA)

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

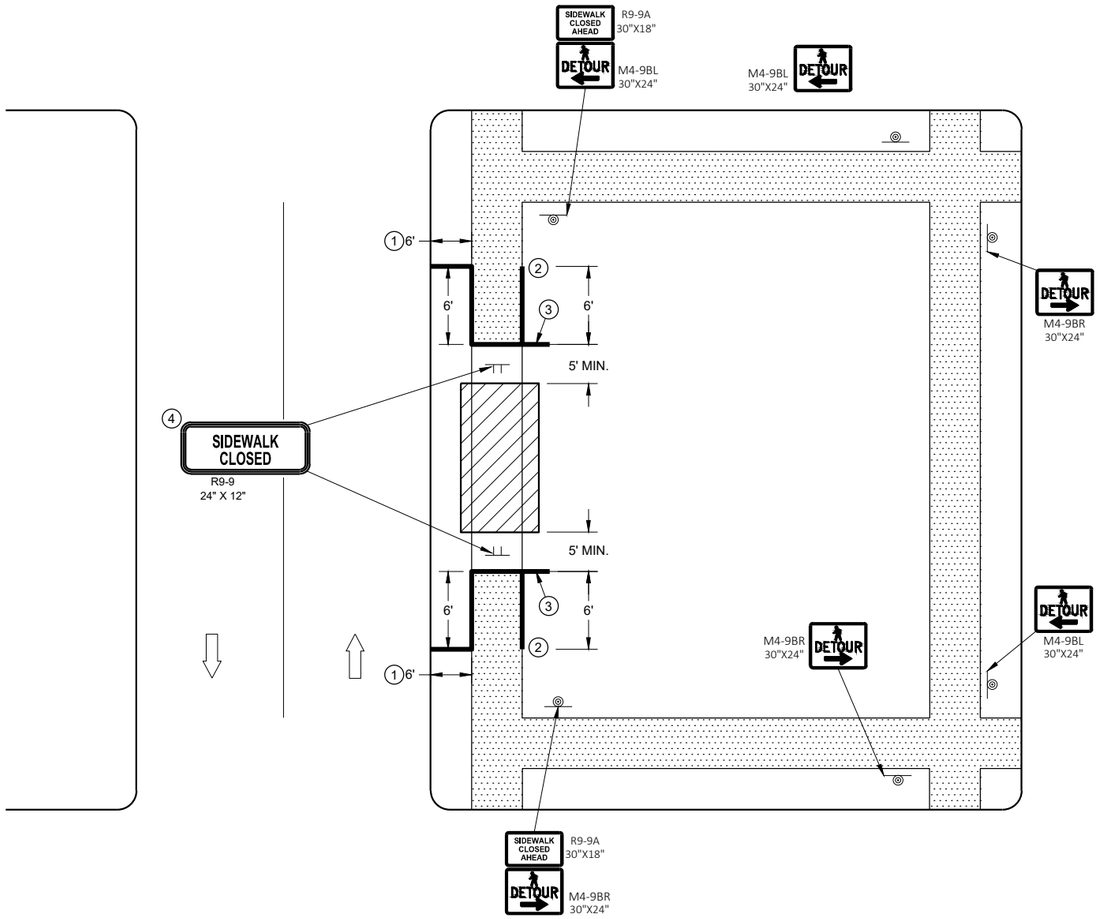
FHWA

6

SDD 15D30-09d

LEGEND

- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- UNDER PEDESTRIAN TRAFFIC
- WORK AREA
- TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC



GENERAL NOTES

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICTS WITH DRIVEWAYS AND OTHER EXISTING FEATURES.

1. IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
2. PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
3. IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
4. MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.

SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

6

SDD 15D30 - 09f

LEGEND

- SIGN ON TEMPORARY SUPPORT
- WORK AREA
- UNDER PEDESTRIAN TRAFFIC
- TEMPORARY PEDESTRIAN SURFACE
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

GENERAL NOTES

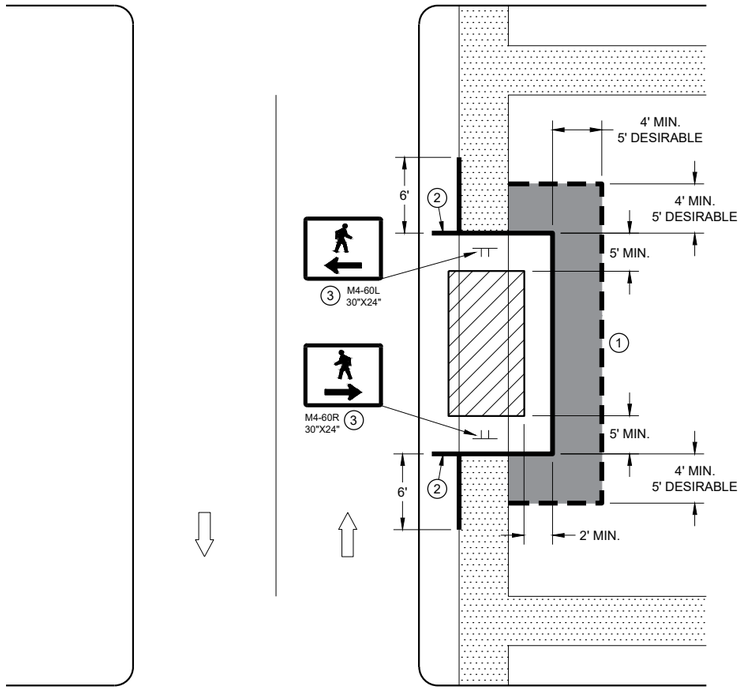
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

1. USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
2. IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
3. MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DIVERSION
SINGLE SIDE

6

SDD 15D30 - 09g

6

SDD15D30 - 09g

TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- UNDER PEDESTRIAN TRAFFIC
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

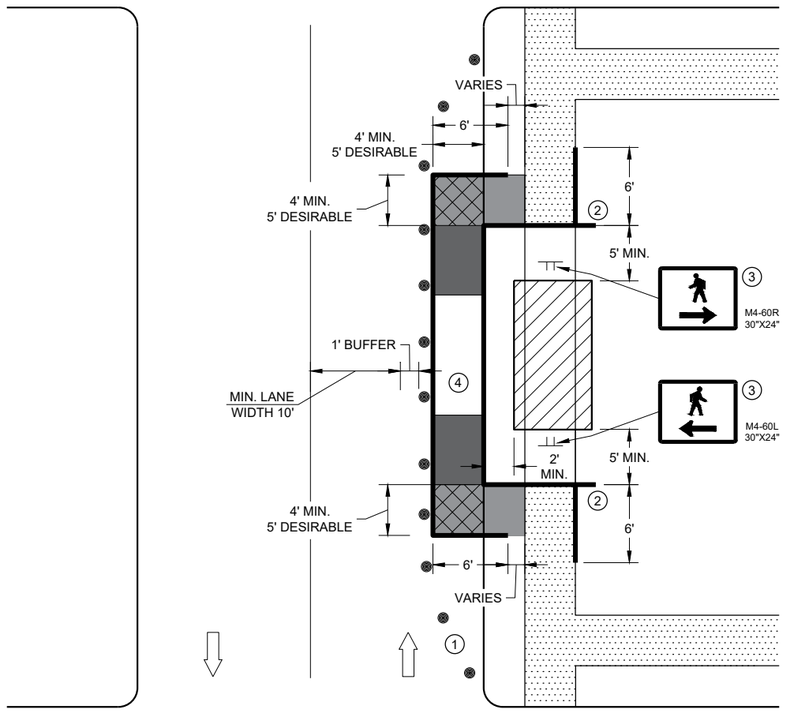
GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

1. SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
2. PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
3. MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
4. USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE.



SIDEWALK DIVERSION, SINGLE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 15D30 - 09h

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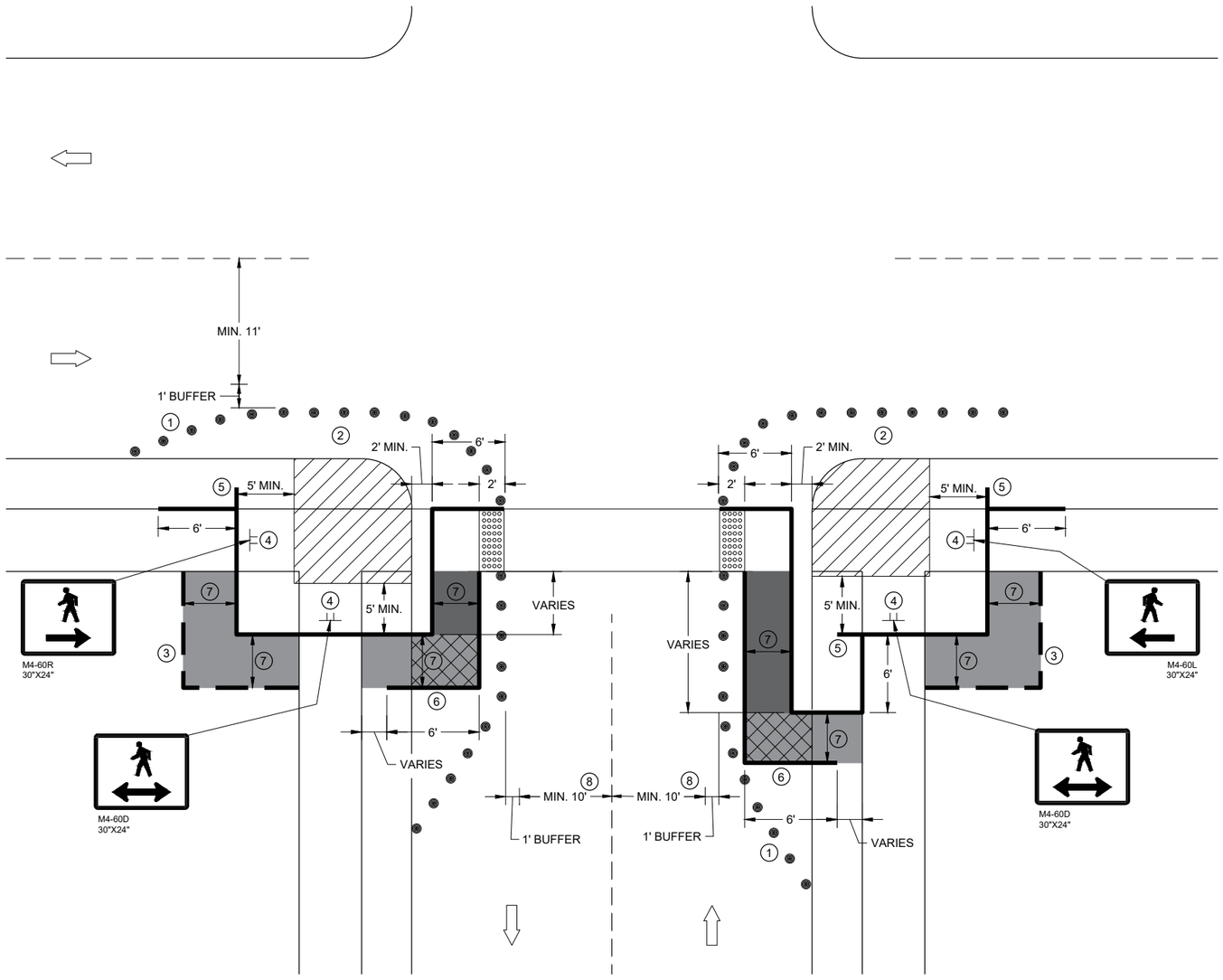
SDD 15D30 - 09h



CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

(1) SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.

(2) PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS

(3) USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.

(4) MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.

(5) PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.

(6) IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.

(7) 4 FEET MINIMUM, 5 FEET DESIRABLE

(8) IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION

SDD 15D30 - 09i

6

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

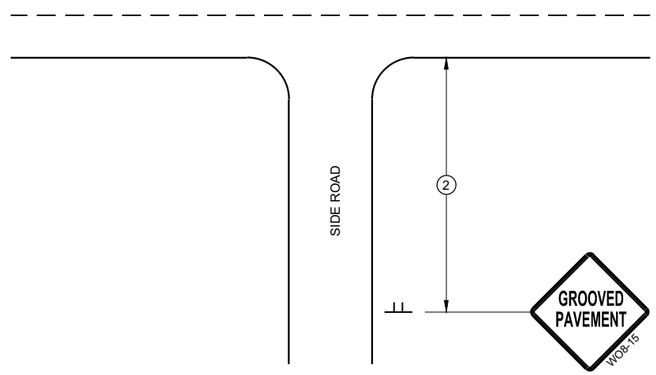
(1) PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.

(2) PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

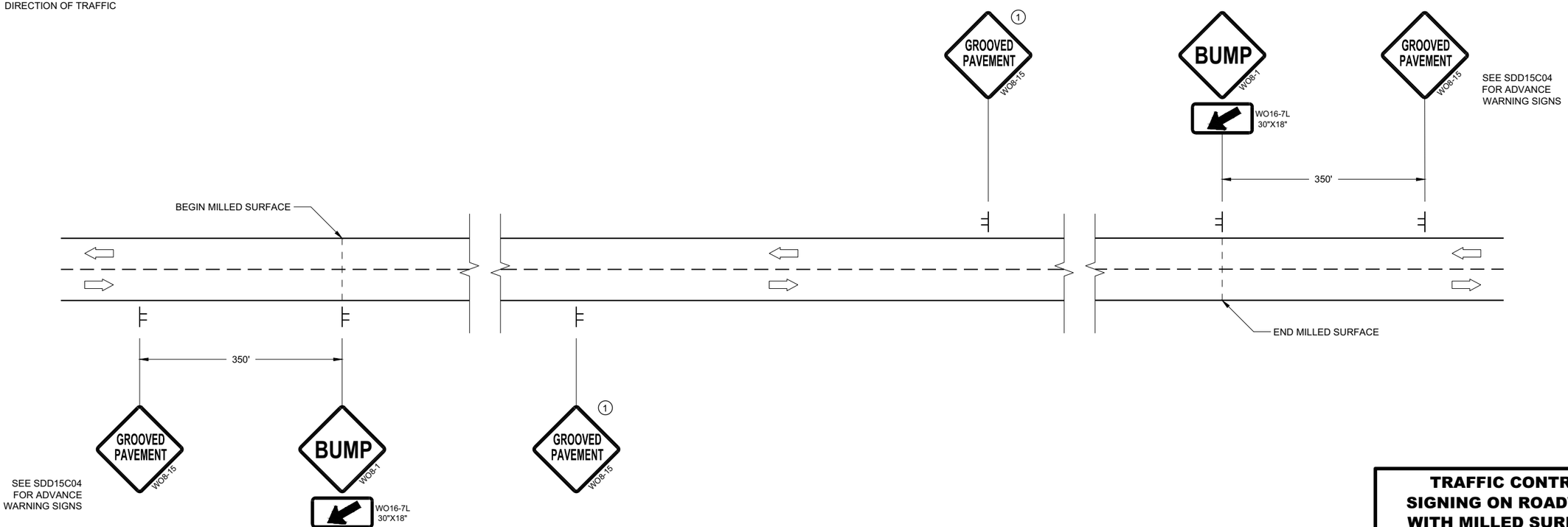
LEGEND

SIGN ON TEMPORARY SUPPORT

DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH SIGN DETAIL



DETAIL FOR SIGNING ON MILLED SURFACES

TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE

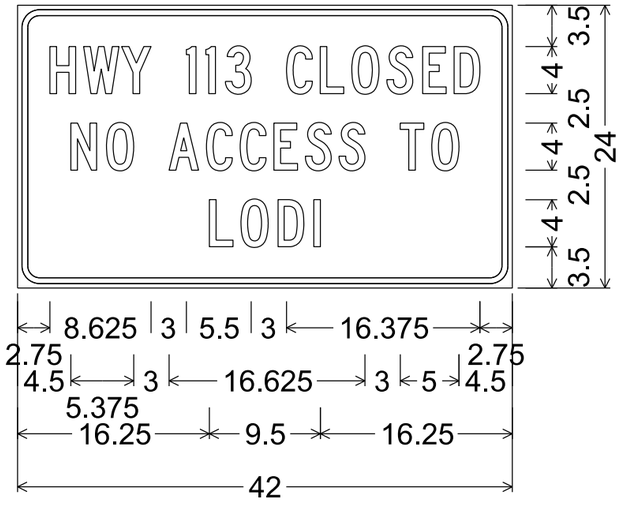
/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

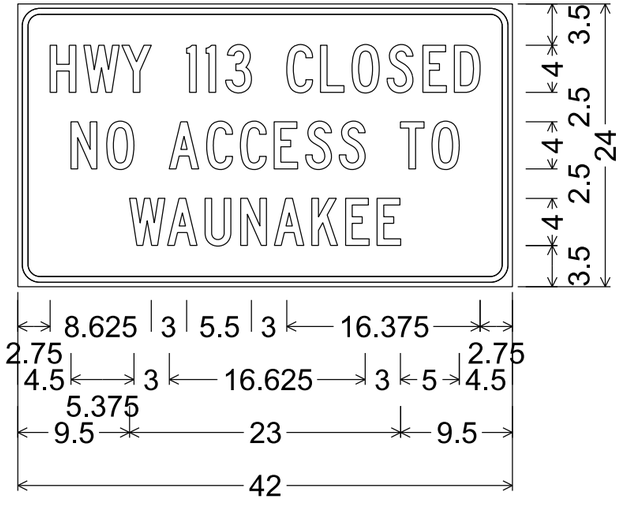
6
SDD 15D44 - 02

NOTES

1. Fixed Message Signs - Type II Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C

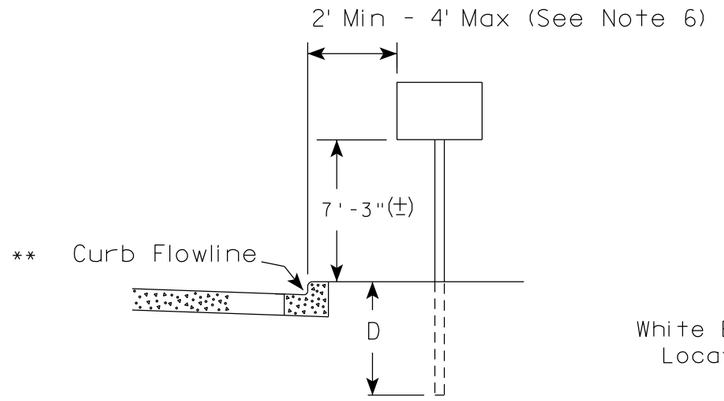


1.875" Radius, 0.500" Border, 0.375" Indent

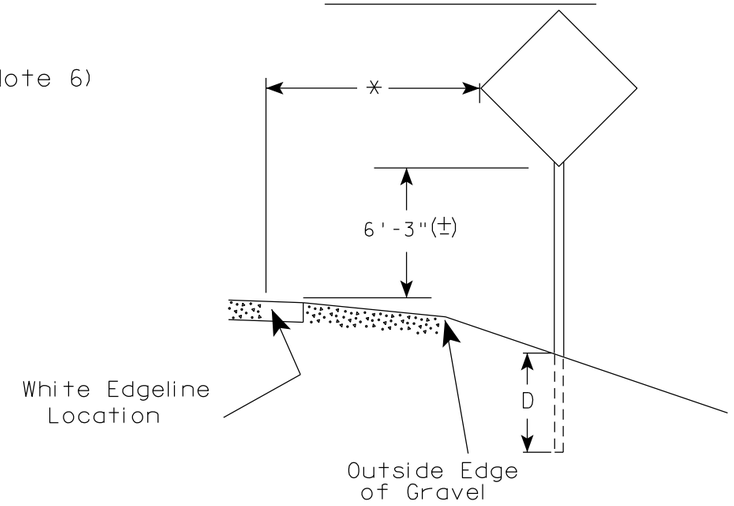


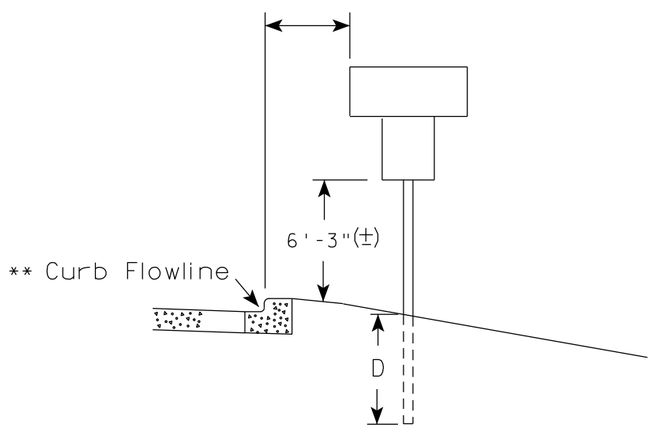
1.875" Radius, 0.500" Border, 0.375" Indent

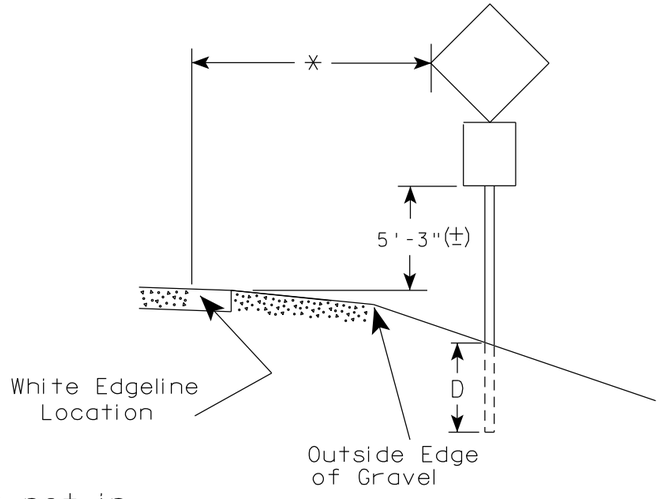
URBAN AREA



RURAL AREA (See Note 2)







** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
   The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directd by the Engineer.

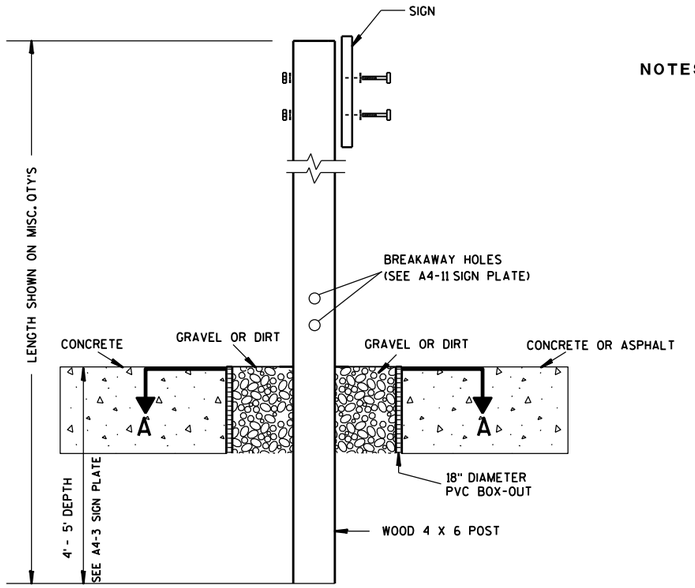
TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/13/2020 PLATE NO. A4-3.22

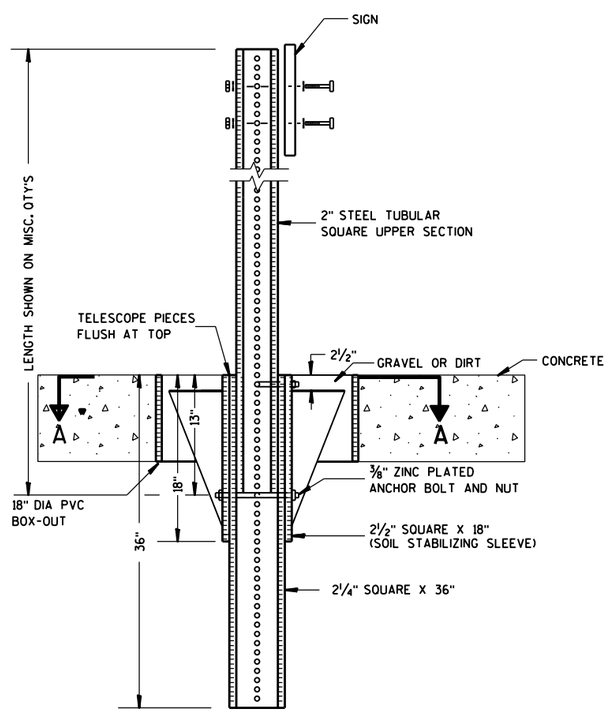
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 159	E



ELEVATION VIEW

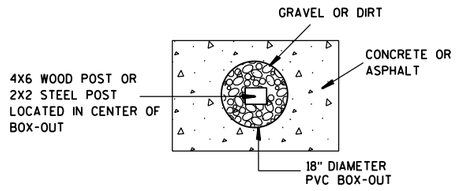
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

NOTES:
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

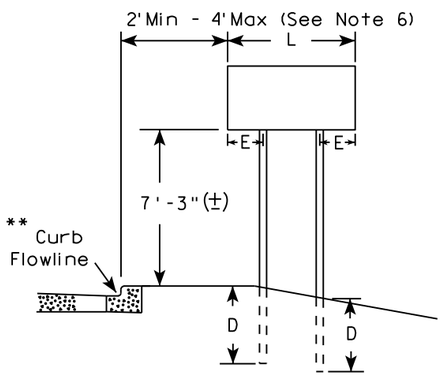
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

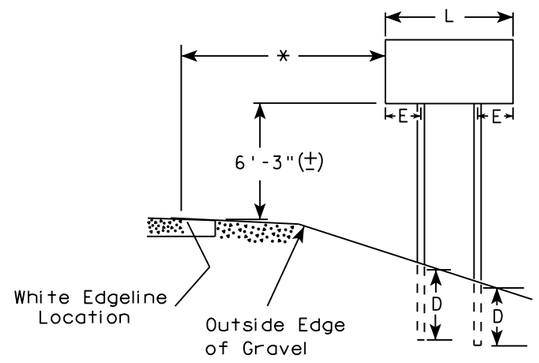
PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

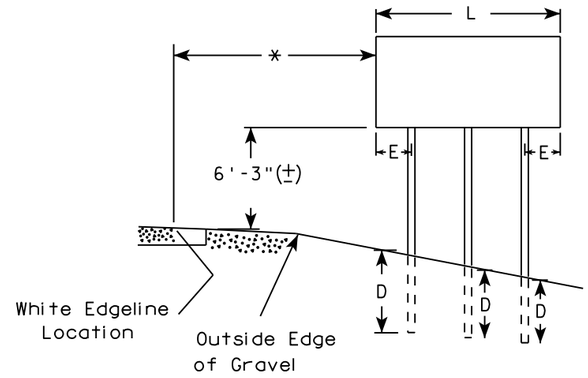
FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN PLOT DATE : 27-JAN-2014 09:48 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42

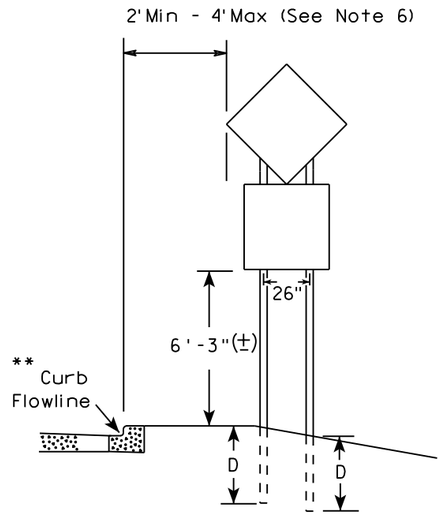
URBAN AREA



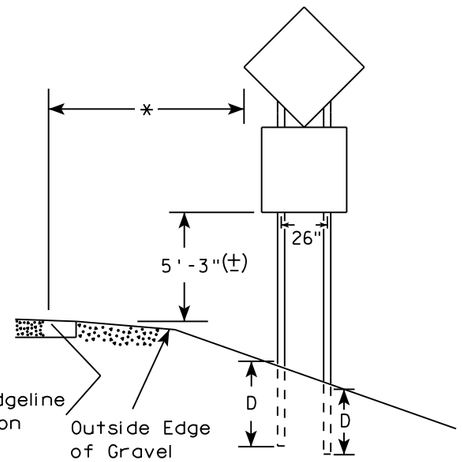
RURAL AREA (See Note 3)







48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)		
	L	E
***	Greater than 48" Less than 60"	12"
	60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

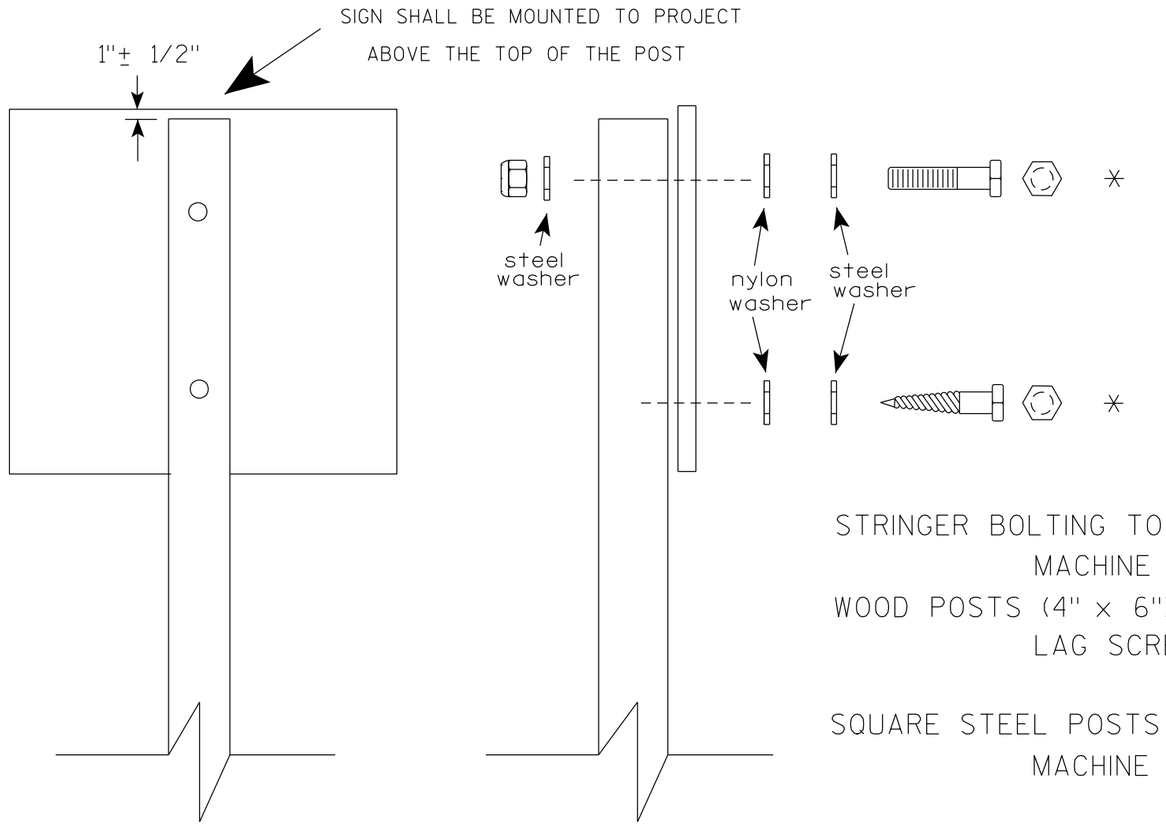
TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 8/21/17 PLATE NO. A4-4.15

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 161	E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3

b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

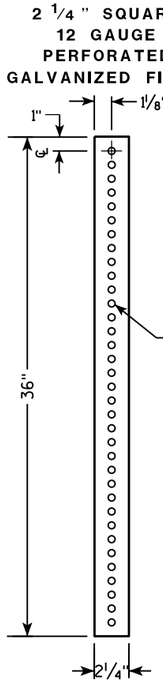
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

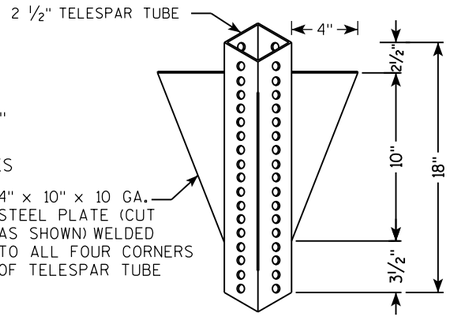
DATE 4/1/2020 PLATE NO. A4-8.9

TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM

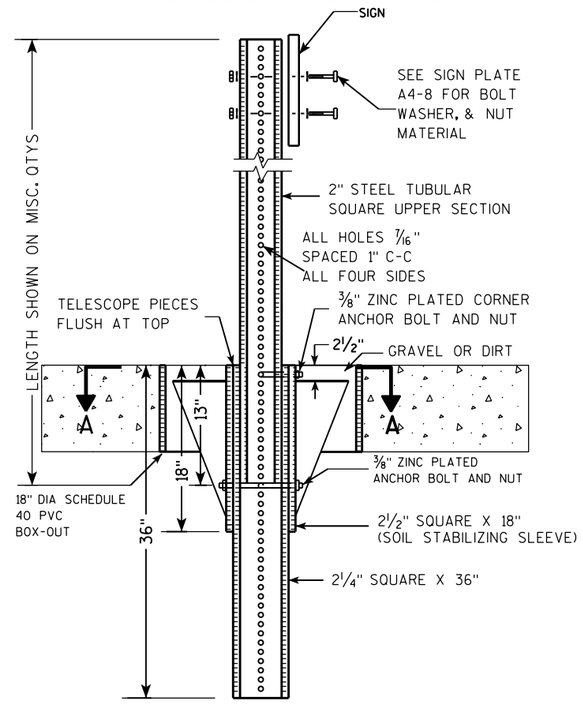
2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



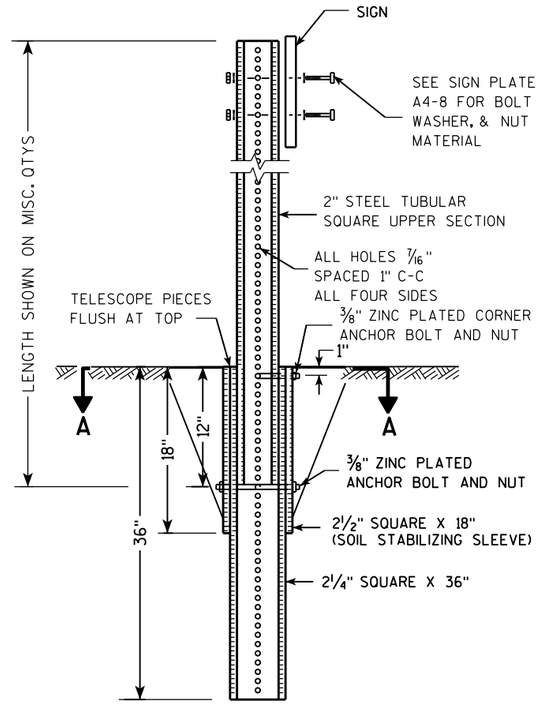
2 1/2 " SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH

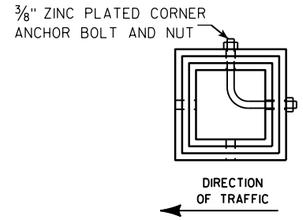


DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)





SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

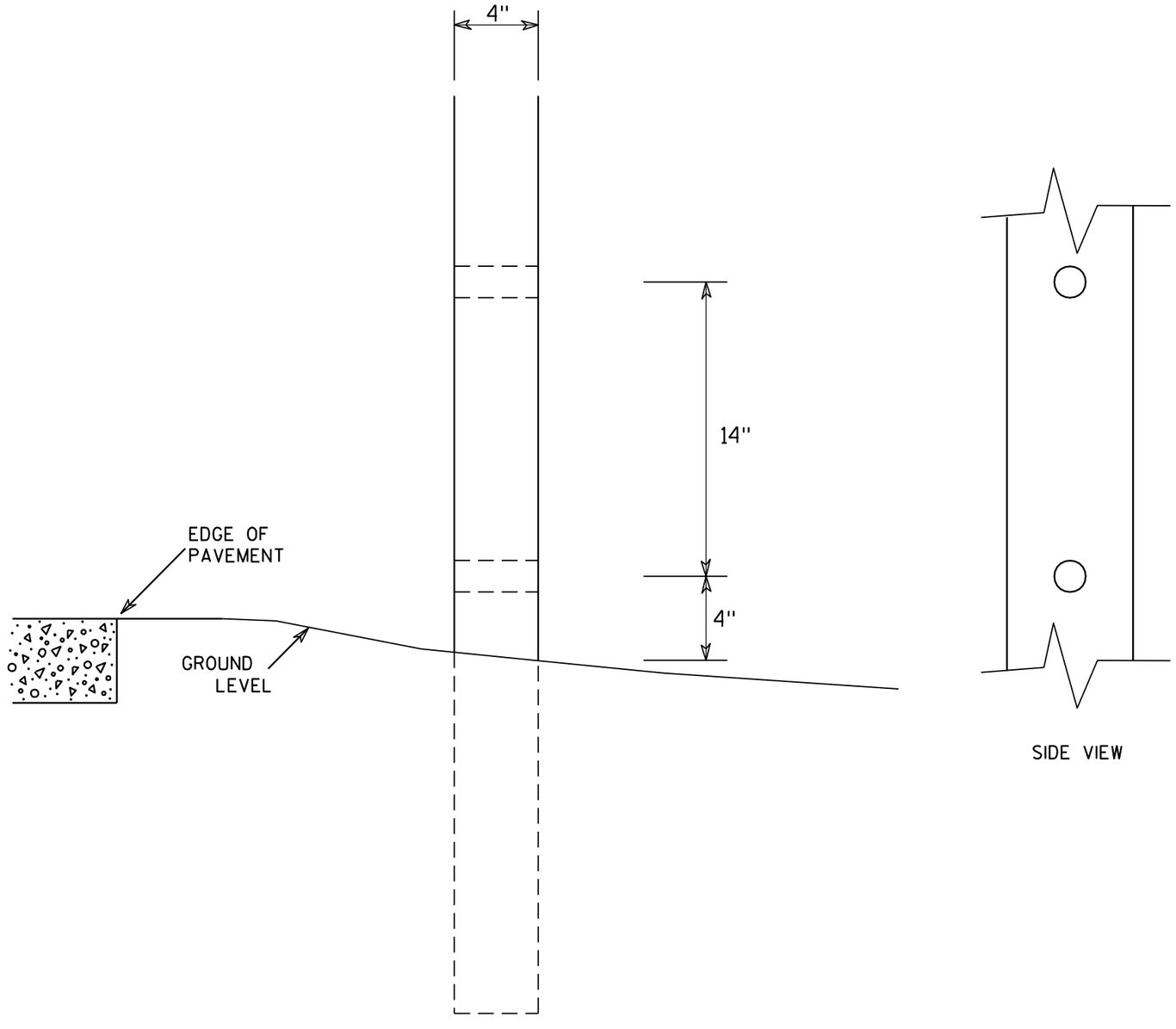
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/05/15 PLATE A4-9.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE : 05-FEB-2015 17:09 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000
WISDOT/CADDS SHEET 42



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

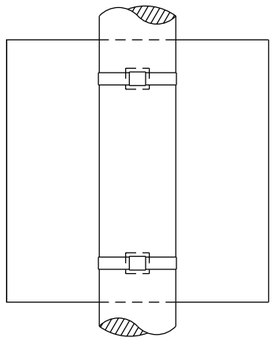
APPROVED Chester J Spang for State Traffic Engineer

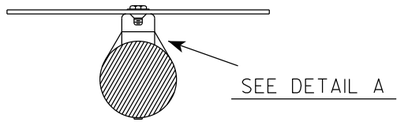
DATE 3/27/97 PLATE NO. A4-11.2

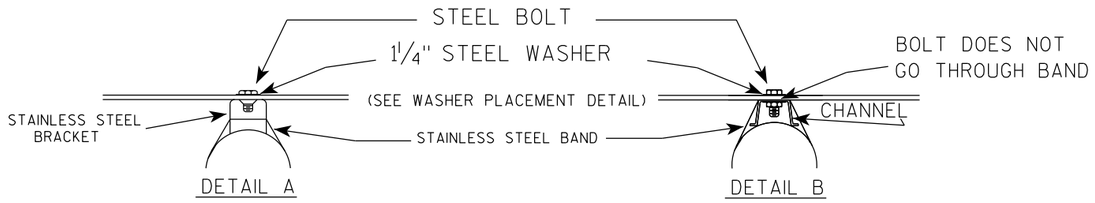
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 164	E

BANDING

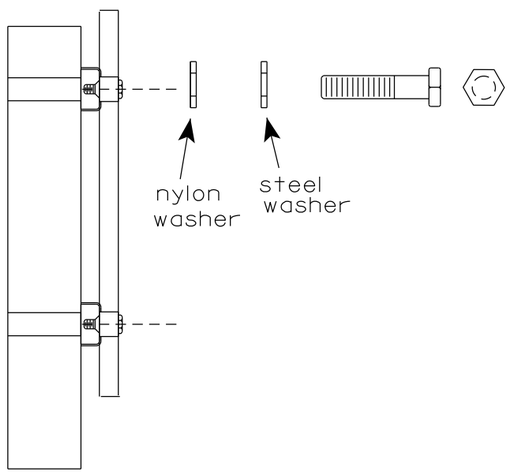
SINGLE SIGN







WASHER PLACEMENT

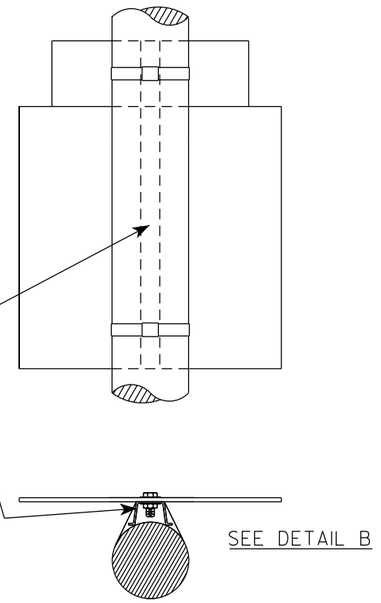


WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be ¾" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

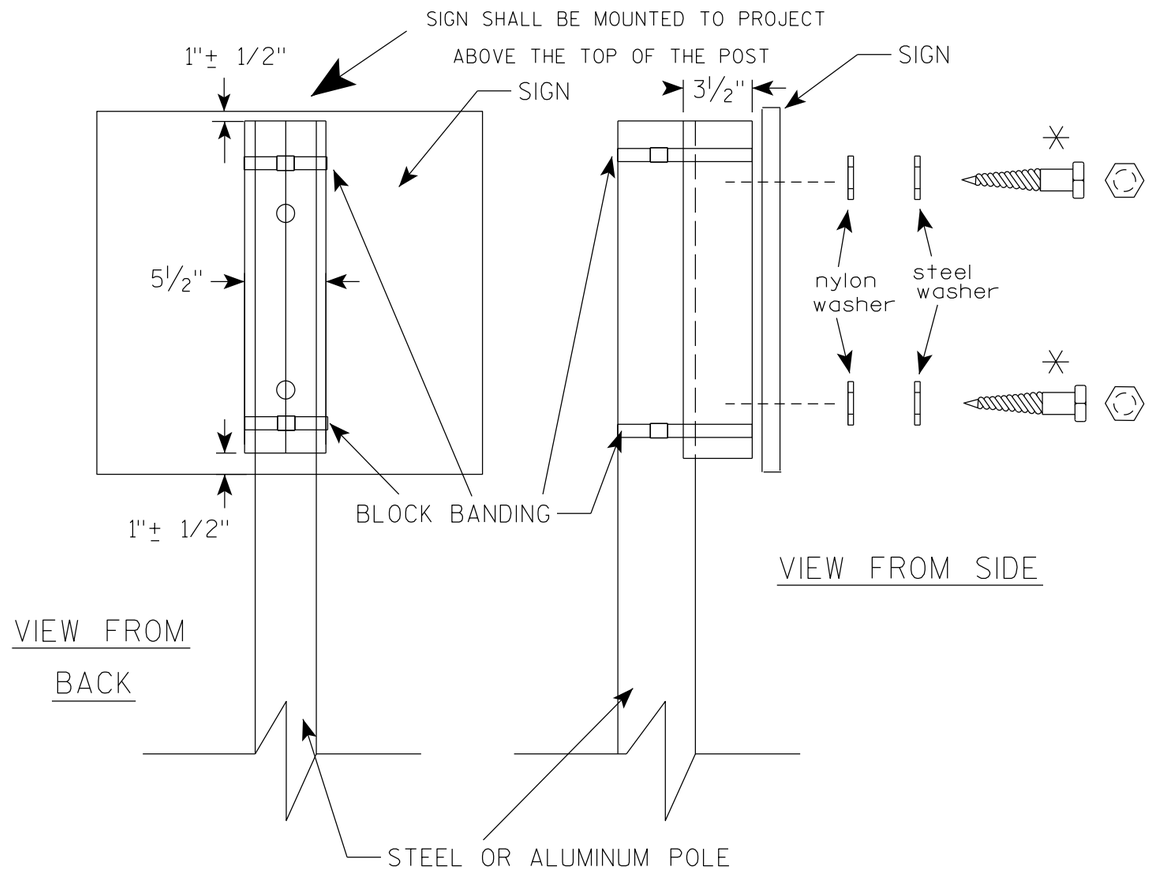
DATE 6/10/19 PLATE NO. A5-9.4

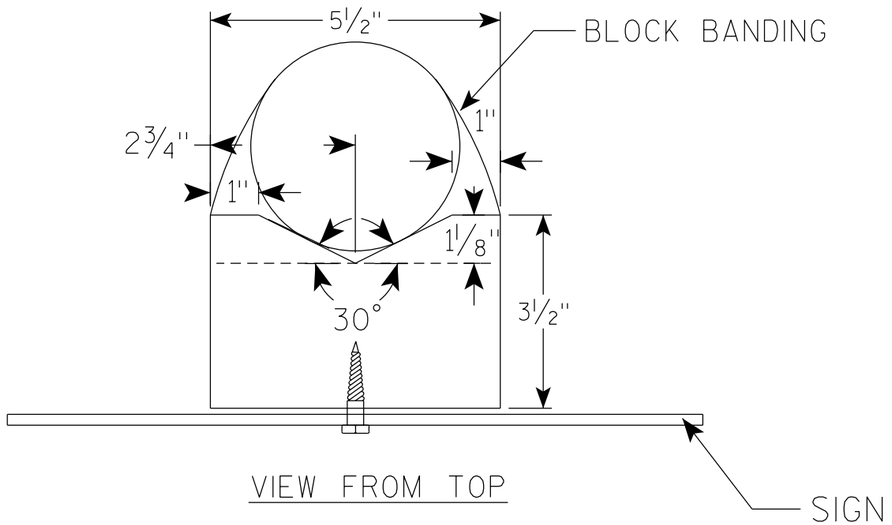
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 165	E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A59.dgn PLOT DATE : 10-JUN 2019 4:10 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : $$.....plotscale.....$$ WISDOT/CADDS SHEET 42

7

7





GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WisDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, ¾" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORNALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✳ LAG BOLTS SHALL BE ⅜" X 2½"

BLOCK BANDING DETAIL (V-BLOCK OPTION)

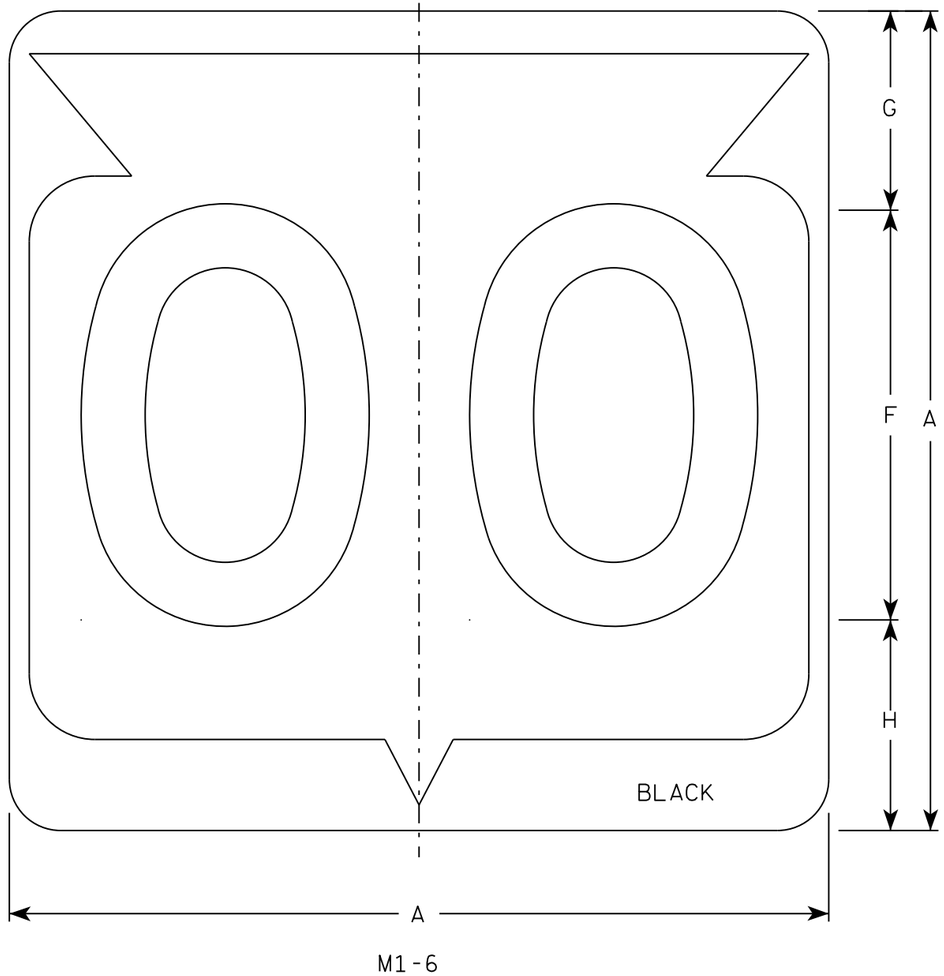
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 4/19/2022 PLATE NO. A5-10.3

PROJECT NO:

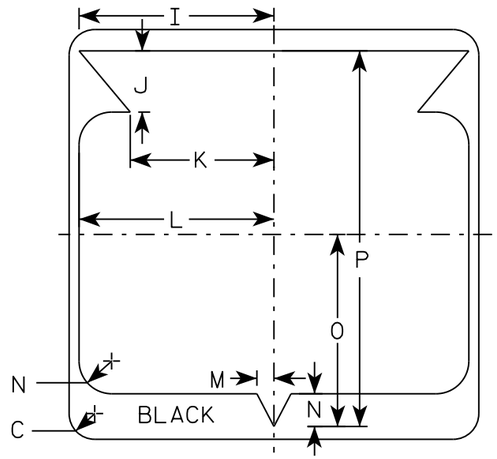
SHEET NO: 166 E



M1-6

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - D except 3 number signs Series C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0

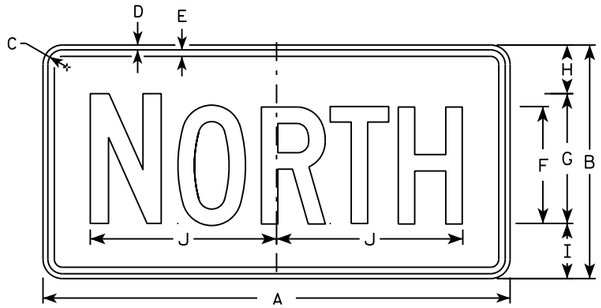
STATE ROUTE MARKER M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

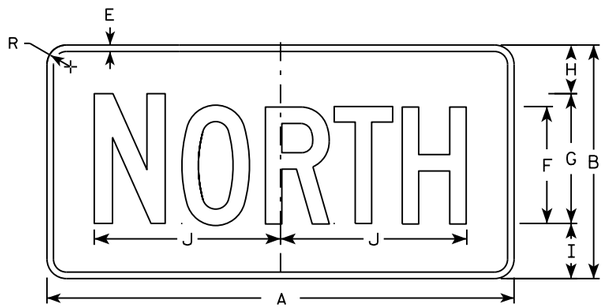
APPROVED Matthew R Rauch for State Traffic Engineer

DATE 3/16/18 PLATE NO. M1-6.10

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 167 | E



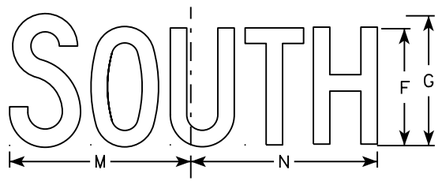
M3-1
MM3-1
MP3-1



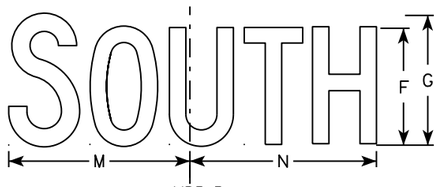
MB3-1
MK3-1
MN3-1

M3-2
MM3-2
MP3-2

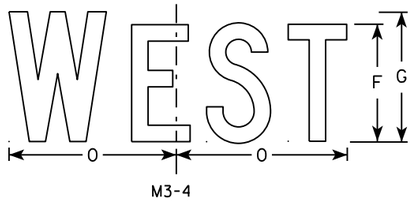
MB3-2
MK3-2
MN3-2



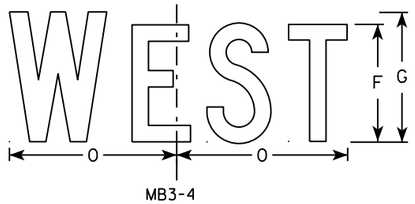
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

NOTES

1. All Signs Type II - Type H
2. Color:
   Background - See note 5
   Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M3-1 thru M3-4 Background - White
   Message - Black
   MB3-1 thru MB3-4 Background - Blue
   Message - White
   MK3-1 thru MK3-4 Background - Green
   Message - White
   MM3-1 thru MM3-4 Background - White
   Message - Green
   MN3-1 thru MN3-4 Background - Brown
   Message - White
   MP3-1 thru MP3-4 Background - White
   Message - Blue
6. Note the first letter of each direction is larger than the remainder of the message.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

STANDARD SIGNS
M3-1 thur M3-4
SERIES

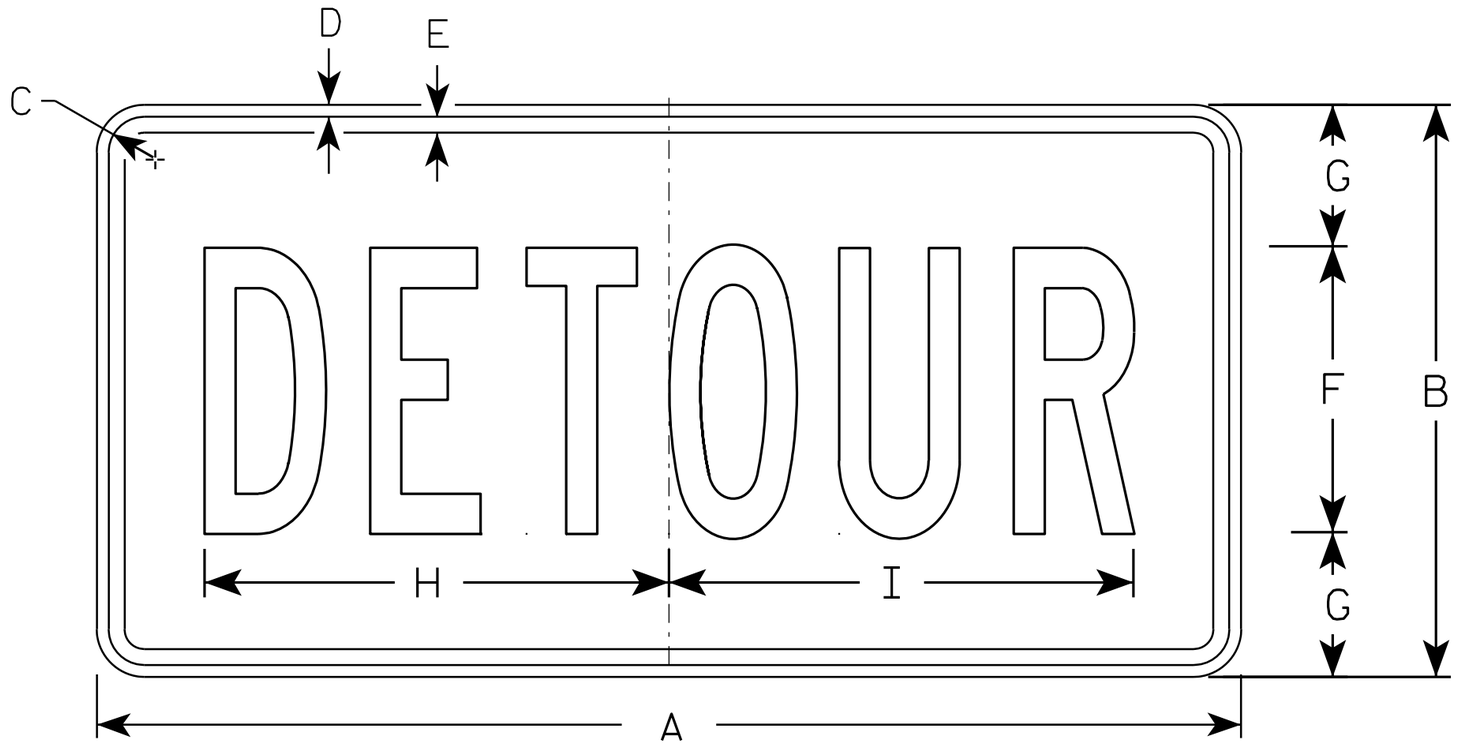
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew P Rauch for State Traffic Engineer

DATE 10/15/15 PLATE NO M3-1.14

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\M31.DGN PLOT DATE : 01-DEC-2015 17:54 PLOT BY : $$ plotuser $$ PLOT NAME : PLOT SCALE : 11.675051:1.000000



M4-8

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	3	10	10 1/4																		2.0
3	36	18	1 1/8	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
4																											
5																											

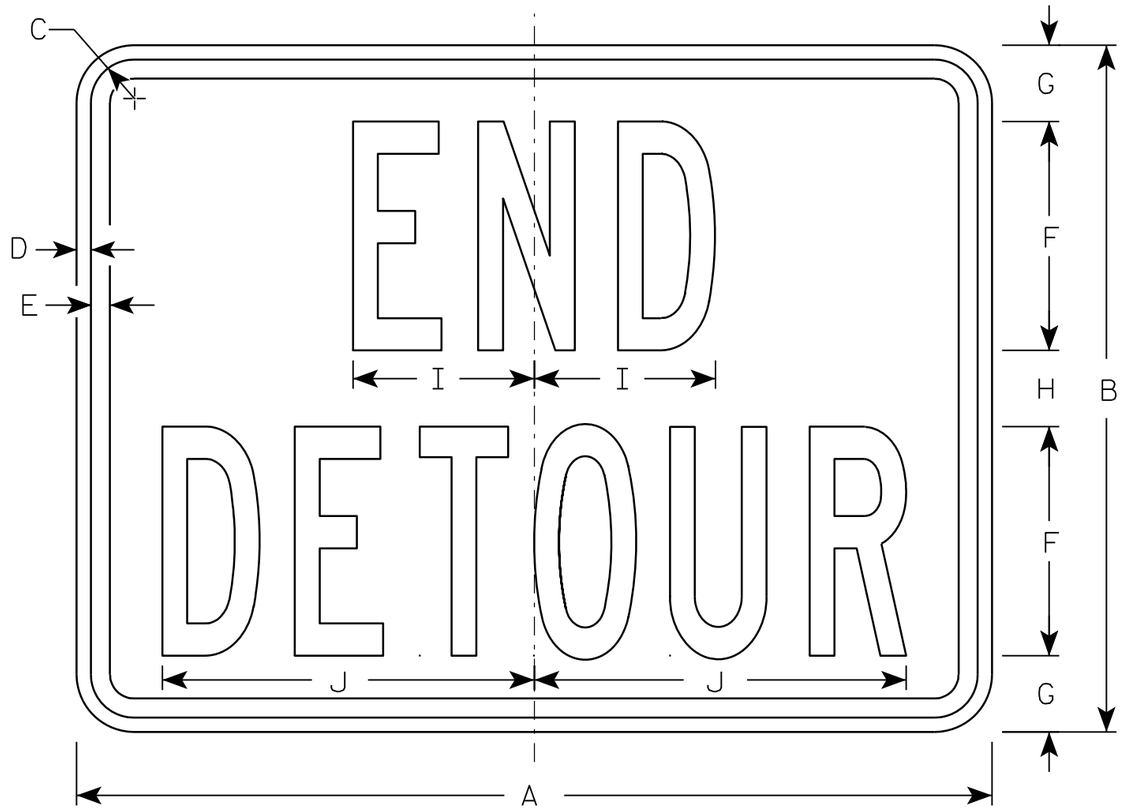
STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/10/10 PLATE NO. M4-8.2

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 169	E



M4-8A

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 1/8	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
3	30	24	1 1/8	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
4																											
5																											

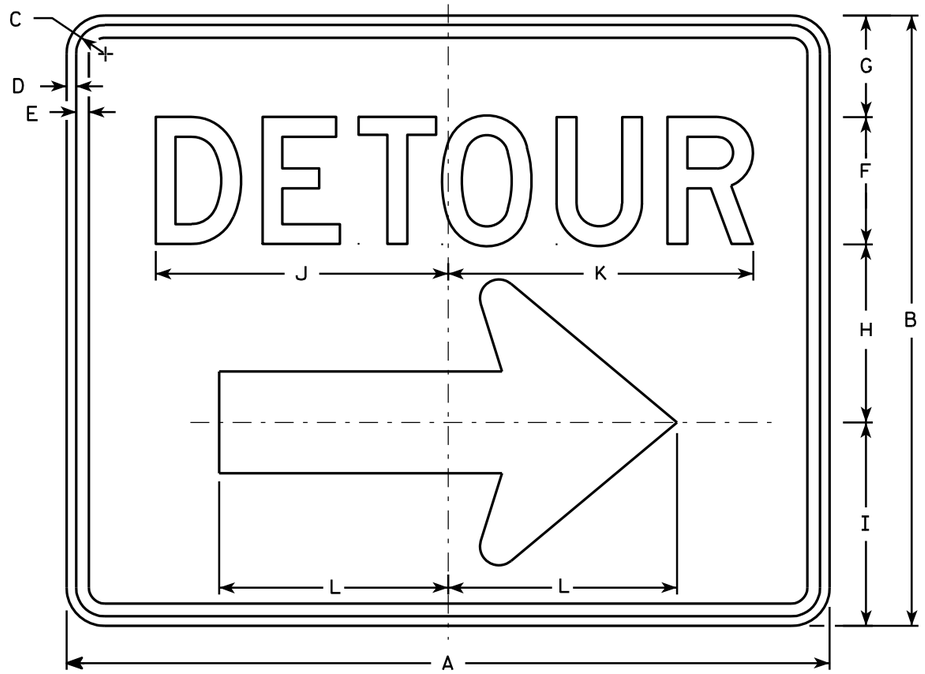
STANDARD SIGN
M4-8A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-8A.2

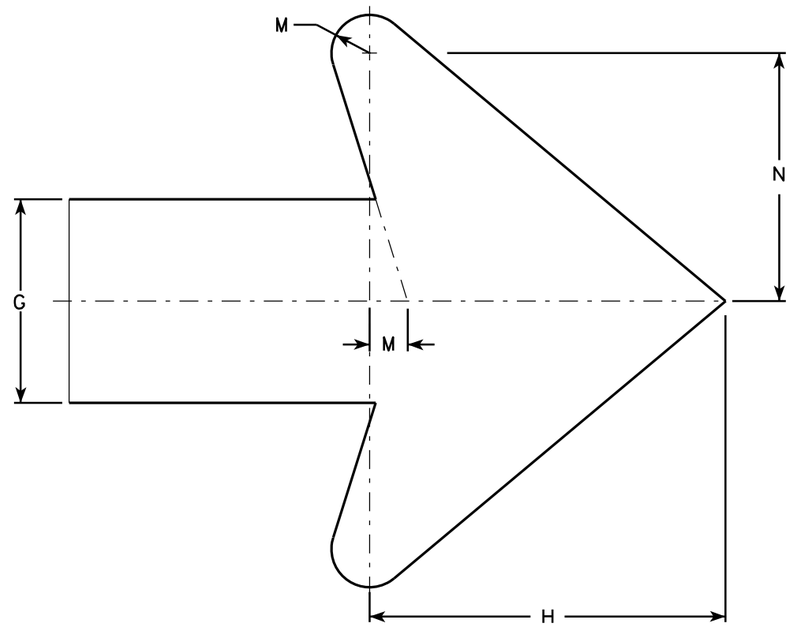
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 170	E



M4-9R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9L is the same as M4-9R except the arrow is reversed.



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
3	30	24	1 1/8	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
4	48	36	1 3/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0
5	48	36	1 3/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0

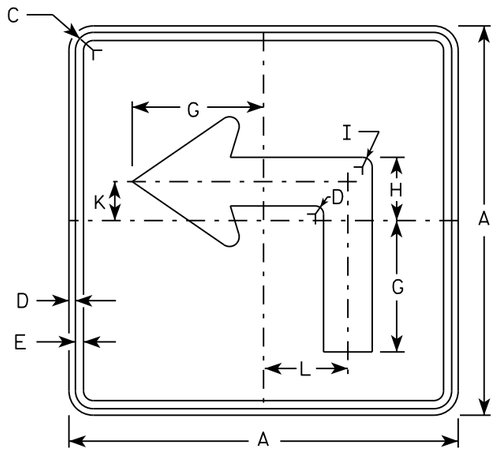
STANDARD SIGN
M4-9 R & L

WISCONSIN DEPT OF TRANSPORTATION

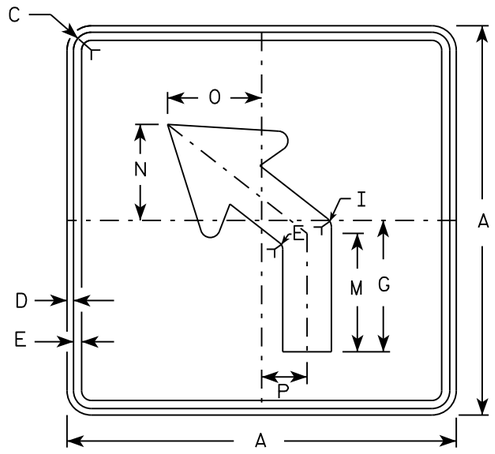
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-9R.4

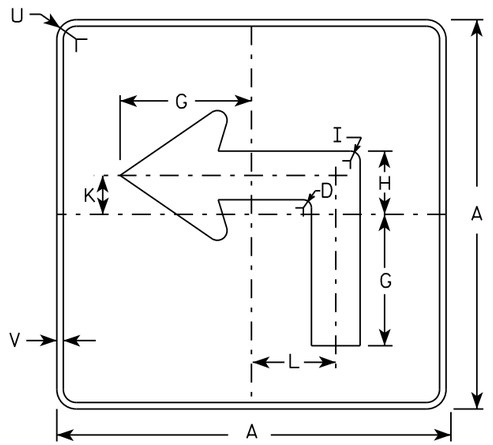
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 171 | E



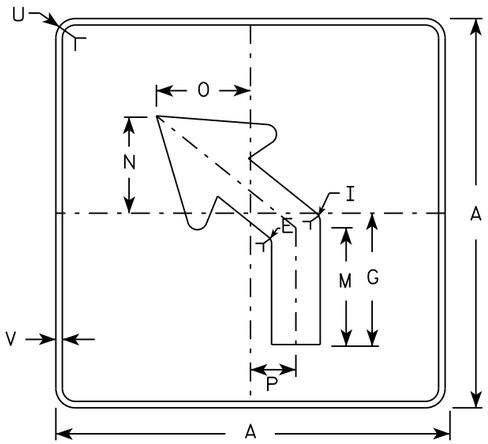
M5-1L
MM5-1L
MO5-1L
MP5-1L



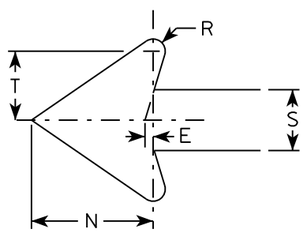
M5-2L
MM5-2L
MO5-2L
MP5-2L



MB5-1L
MK5-1L
MN5-1L
MR5-1L



MB5-2L
MK5-2L
MN5-2L
MR5-2L



NOTES

1. Signs are Type II - Type H reflective except as shown
2. Color:
   Background - See note 4
   Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M5-1 and M5-2 Background - White
   Message - Black
   MB5-1 and MB5-2 Background - Blue
   Message - White
   MK5-1 and MK5-2 Background - Green
   Message - White
   MM5-1 and MM5-2 Background - White
   Message - Green
   MN5-1 and MN5-2 Background - Brown
   Message - White
   MO5-1 and MO5-2 Background - Orange - Type F Reflective
   Message - Black
   MP5-1 and MP5-2 Background - White - Type H Reflective
   Message - Blue
   MR5-1 and MR5-2 Background - Brown
   Message - Yellow
5. M5-1R same as M5-1L except arrow points right.
6. M5-2R same as M5-2L except arrow tilts right.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25

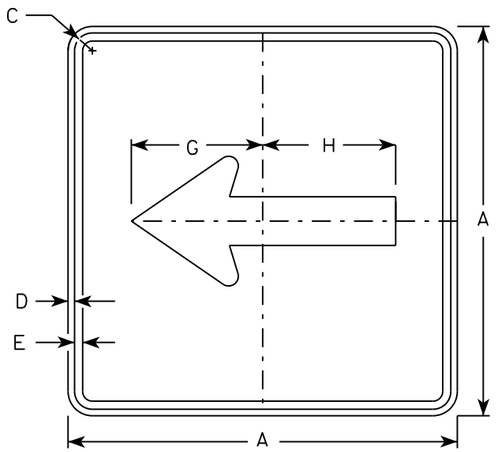
STANDARD SIGN
M5-1 & M5-2

WISCONSIN DEPT OF TRANSPORTATION

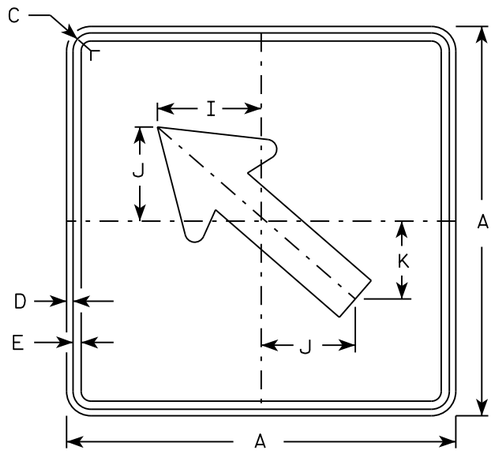
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M5-1.13

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E



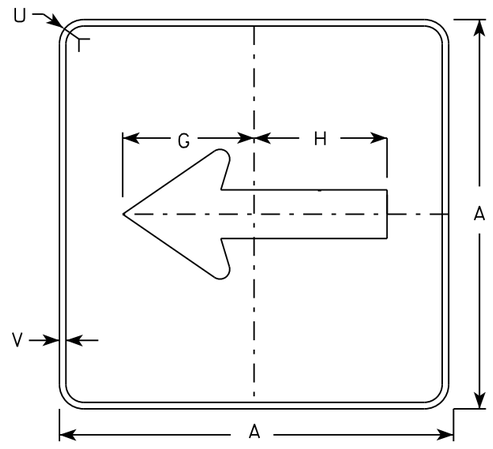
M6-1
MM6-1
MO6-1
MP6-1



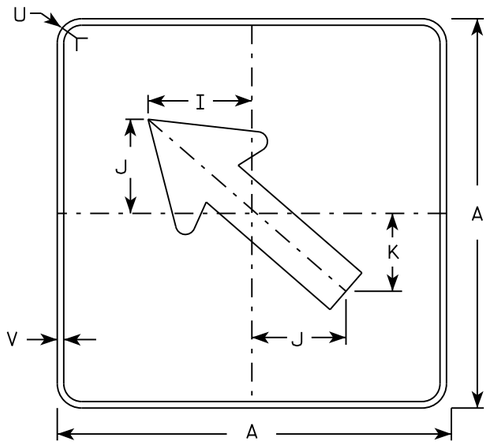
M6-2
MM6-2
MO6-2
MP6-2

NOTES

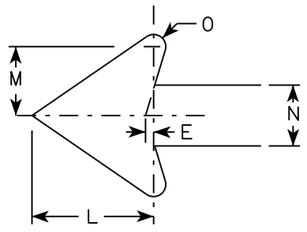
1. Signs are Type II - Type H except as Shown
2. Color:
   Background - See note 4
   Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M6-1 and M6-2 Background - White
   Message - Black
   MB6-1 and MB6-2 Background - Blue
   Message - White
   MK6-1 and MK6-2 Background - Green
   Message - White
   MM6-1 and MM6-2 Background - White
   Message - Green
   MN6-1 and MN6-2 Background - Brown
   Message - White
   MO6-1 and MO6-2 Background - Orange - Type F Reflective
   Message - Black
   MP6-1 and MP6-2 Background - White
   Message - Blue
   MR6-1 and MR6-2 Background - Brown
   Message - Yellow



MB6-1
MK6-1
MN6-1
MR6-1



MB6-2
MK6-2
MN6-2
MR6-2



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2						3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25

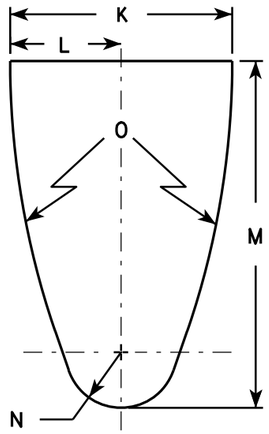
STANDARD SIGN M6-1 & M6-2 SERIES

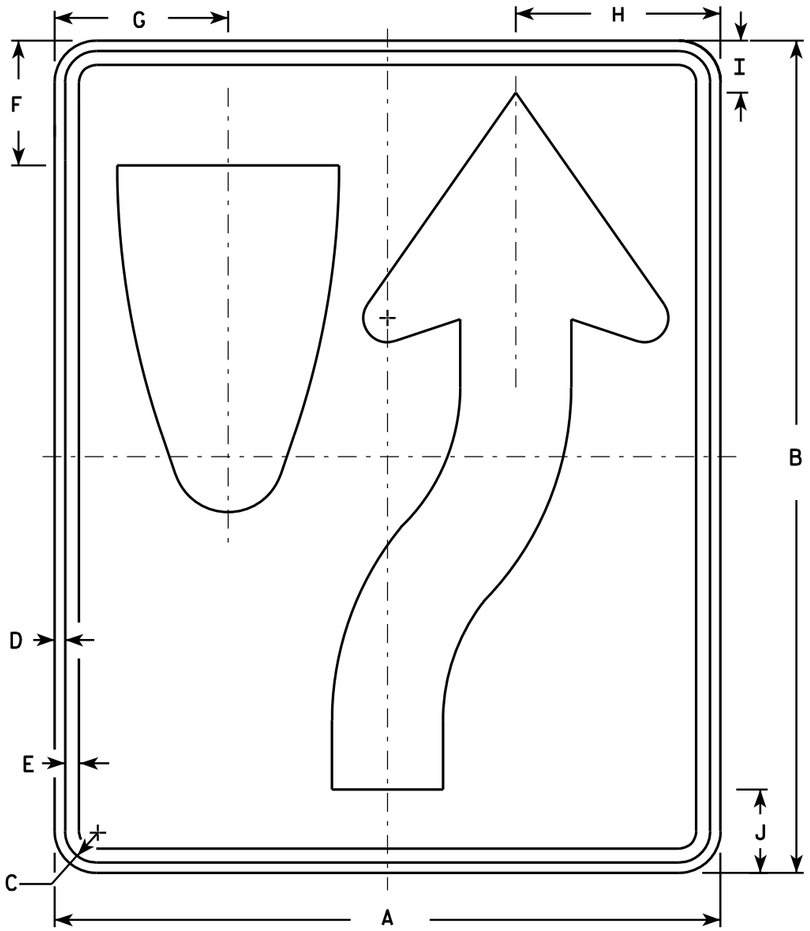
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/15/15 PLATE 173 M6-1.15

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

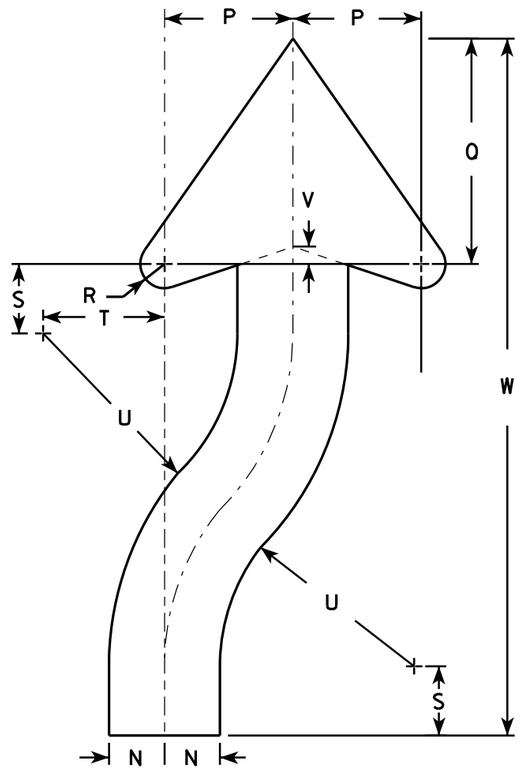




R4-7

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition. material is plywood but borders shall be rounded
2. Color:
   Background - White
   Message - Black
3. Corners may be square or rounded when base as shown. When base material is metal, the corners and borders shall be rounded.
4. R4-8 is the same as R4-7 except Legend is reversed.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3 3/8	4 3/4	5 1/2	1 3/8	2 1/4	6	3	9 3/8	1 1/2	22 1/2	3 1/2	6 1/8	5/8	1 7/8	3 1/4	6 3/4	1/2	20 3/8				3.0
2S	24	30	1 1/8	3/8	1/2	4 1/2	6 1/4	7 3/8	1 7/8	3	8	4	12 1/2	2	30	4 5/8	8 1/8	7/8	2 1/2	4 3/8	9	5/8	25 1/8				5.0
2M	24	30	1 1/8	3/8	1/2	4 1/2	6 1/4	7 3/8	1 7/8	3	8	4	12 1/2	2	30	4 5/8	8 1/8	7/8	2 1/2	4 3/8	9	5/8	25 1/8				5.0
3	36	48	1 3/4	1/2	5/8	6 3/4	9 3/8	11 1/8	2 7/8	4 1/2	12	6	18 3/4	3	45	6 7/8	12 1/4	1 1/4	3 3/4	6 5/8	13 1/2	1	40 3/4				12.0
4	36	48	1 3/4	1/2	5/8	6 3/4	9 3/8	11 1/8	2 7/8	4 1/2	12	6	18 3/4	3	45	6 7/8	12 1/4	1 1/4	3 3/4	6 5/8	13 1/2	1	40 3/4				12.0
5	48	60	2 1/4	3/4	1	9	12 1/2	14 3/4	3 3/4	6	16	8	25	4	60	9 1/4	16 1/4	1 5/8	5	8 3/4	18	1 1/4	50 1/4				20.0

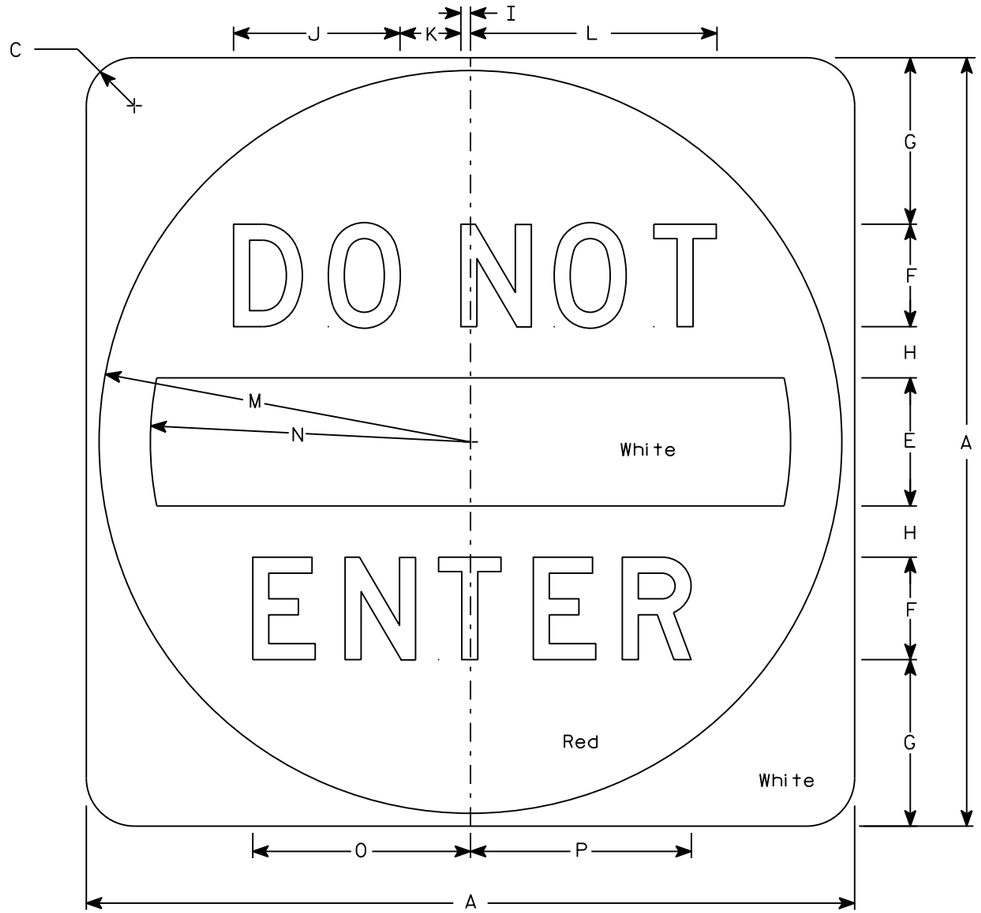
STANDARD SIGN
R4-7 & R4-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch for State Traffic Engineer

DATE 3/25/2011 PLATE NO. R4-7.8

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 174	E



R5-1

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - See detail
   Message - White
3. Message Series - D

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30		1 7/8		5	4	6 1/2	2	3/8	6 1/2	2 3/8	9 5/8	14 1/2	12 1/2	8 1/2	8 5/8											6.25
2M	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4											9.0
3	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4											9.0
4	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4											9.0
5	48		3		8	6	11	3	5/8	9 3/4	3 5/8	14 1/2	23 1/2	20	12 3/4	12 7/8											16.0

STANDARD SIGN
R5-1

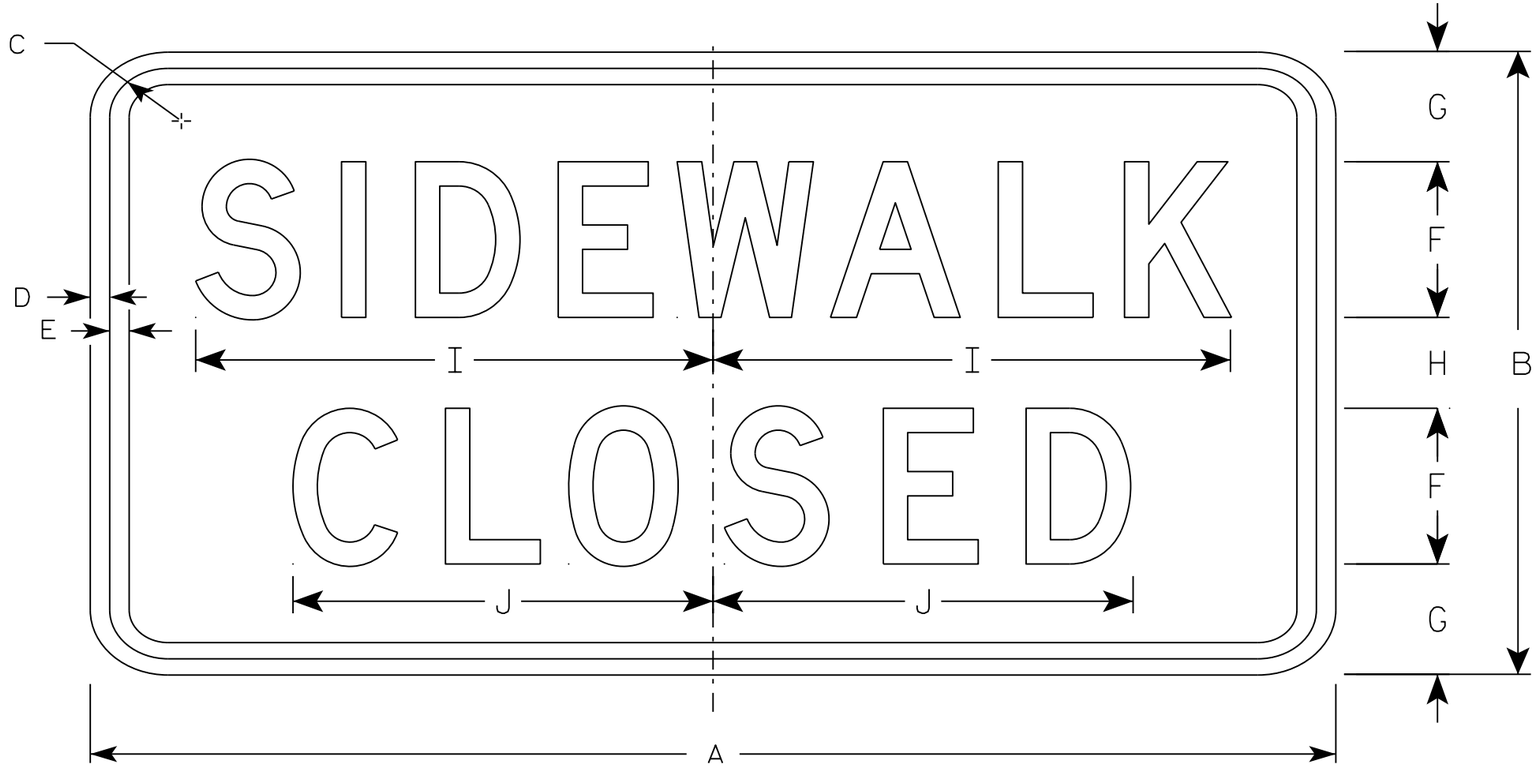
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/15/18 PLATE NO. R5-1.16

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 175 | E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\R51.DGN PLOT DATE : 15-MAR-2018 14:31 PLOT BY : $$...plotuser...$$ PLOT NAME : PLOT SCALE : 5.914594:1.000000 WISDOT/CADDS SHEET 42



R9-9

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
2M	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
3	30	18	1 3/4	1/2	1/2	4	3 1/2	3	12 1/2	10 1/4																	3.75
4																											
5																											

STANDARD SIGN
R9-9

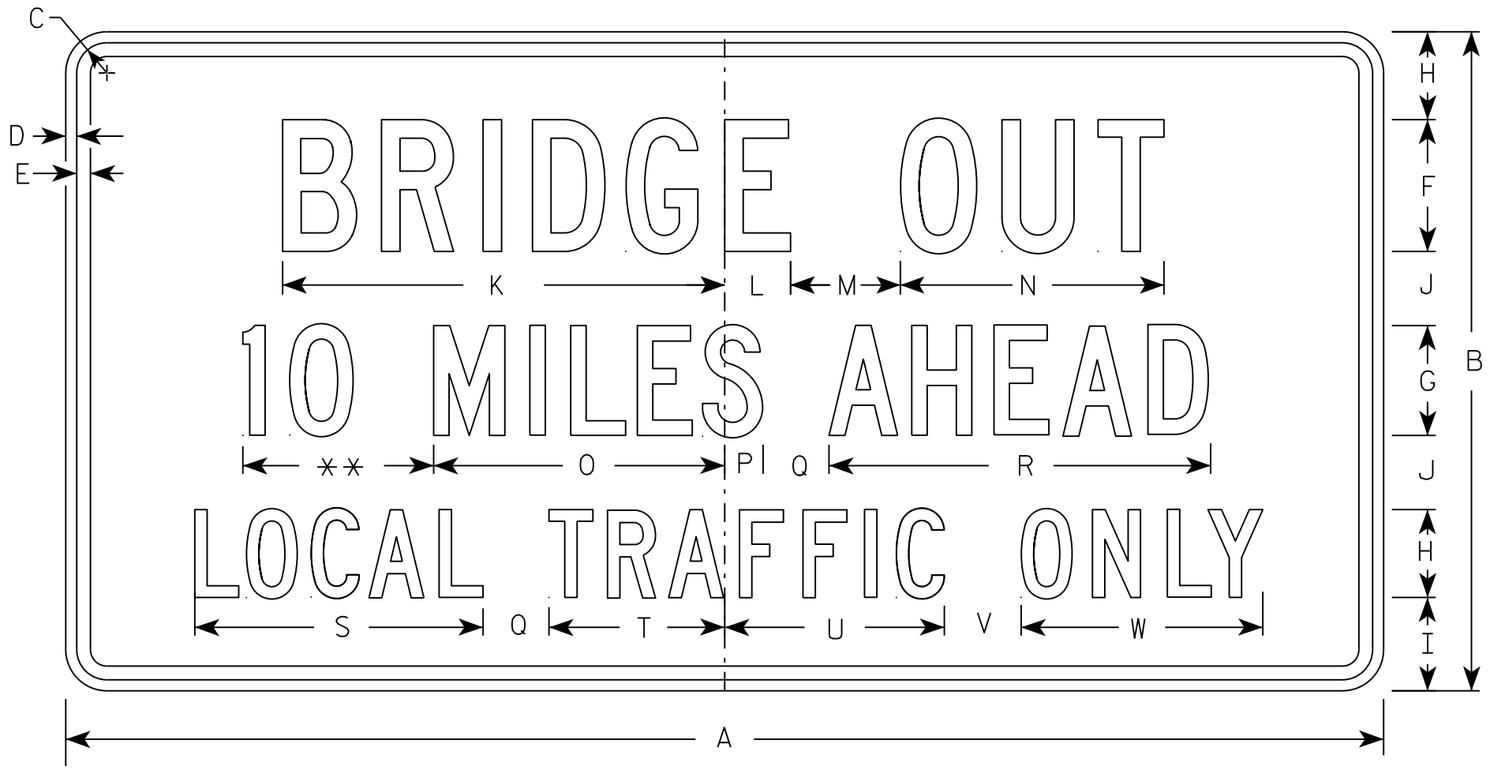
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 8/11/16 PLATE 176 R9-9.6

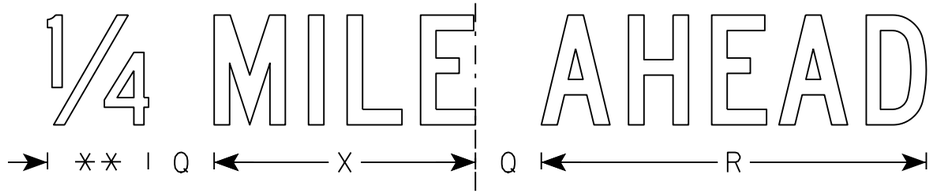
PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\R99.DGN PLOT DATE : 11-AUG-2016 11:33 PLOT BY : $$ plotuser $$ PLOT NAME : PLOT SCALE : 2.918761:1.000000



R11-3B

** See Note 5



NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals to nearest quarter mile and optically adjust spacing to achieve proper balance.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 3/8	1/2	5/8	4	3	2 1/2	2	2	13 1/4	2 1/4	3	8	8	1 1/2	2	10 3/4	8 3/8	4 3/4	6 1/2	2	6 3/4	7 1/8			4.5
2S	60	30	1 3/8	1/2	5/8	6	5	4	4 1/4	3 3/8	20 1/8	3	5	12	13 1/4	1 3/4	3	17 3/8	13 1/8	8	10	3 1/2	11	11 7/8			12.5
2M	60	30	1 3/8	1/2	5/8	6	5	4	4 1/4	3 3/8	20 1/8	3	5	12	13 1/4	1 3/4	3	17 3/8	13 1/8	8	10	3 1/2	11	11 7/8			12.5
3																											
4																											
5																											

STANDARD SIGN
R11-3B

WISCONSIN DEPT OF TRANSPORTATION

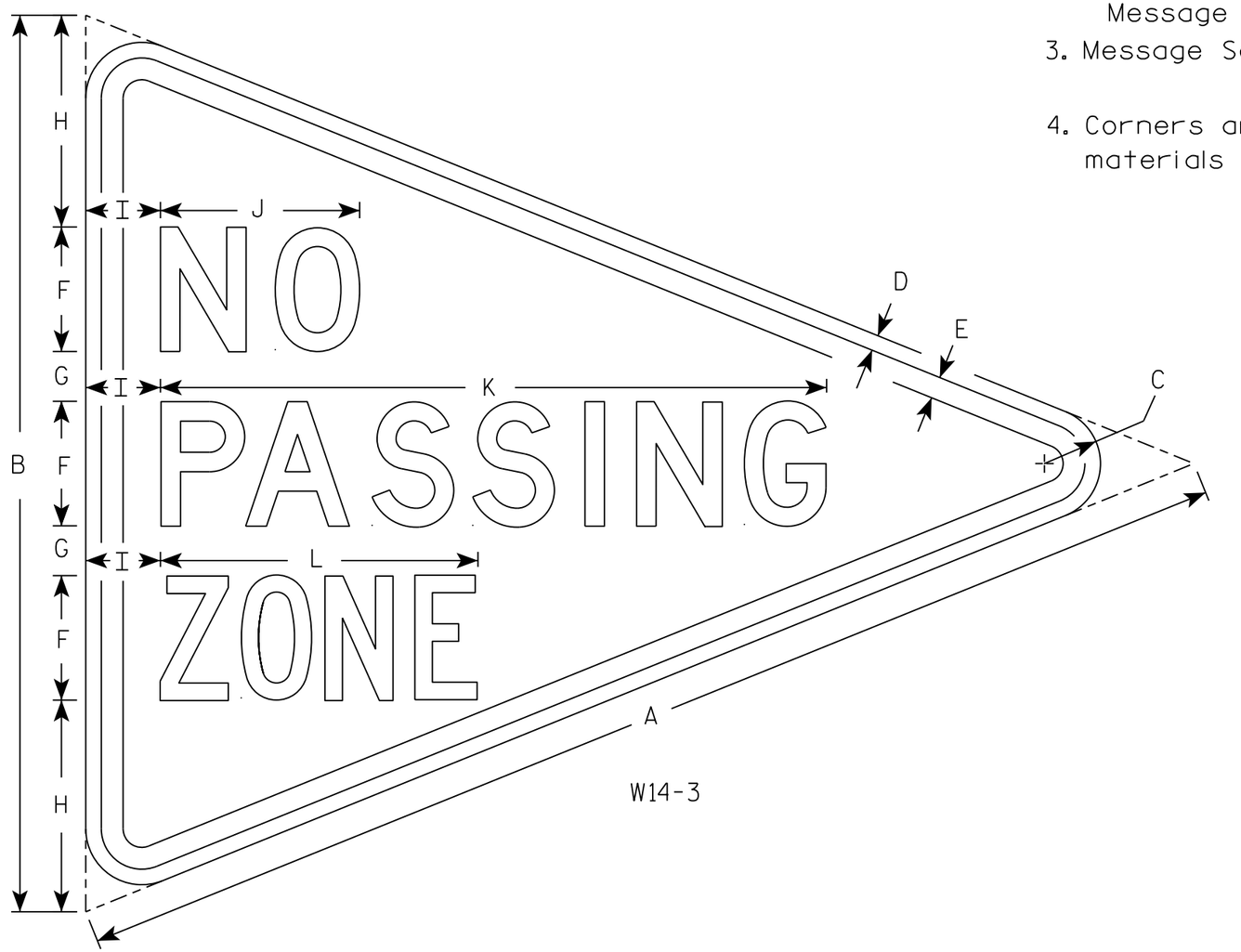
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/21/17 PLATE NO. R11-3B.3

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 177 | E

NOTES

1. Sign is Type II- Type F Reflective
2. Color:
   Background - Yellow
   Message - Black
3. Message Series - Lines 1 and 2 are Series D.
   Line 3 is series C.
4. Corners and borders shall be rounded on all base materials for this sign.



W14-3

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															5.56
2M																											
3																											
4																											
5																											

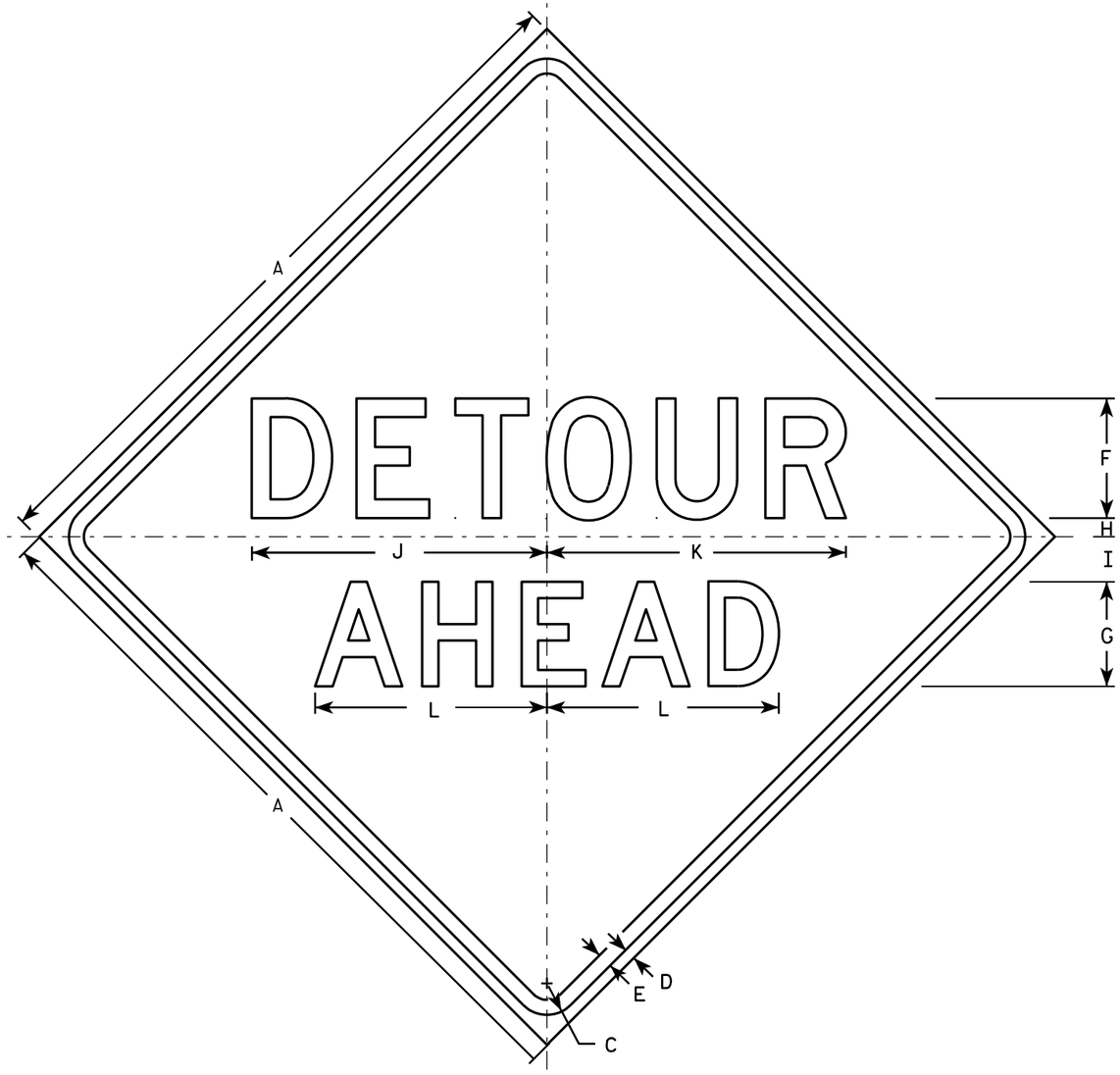
STANDARD SIGN
W14-3

WISCONSIN DEPT OF TRANSPORTATION

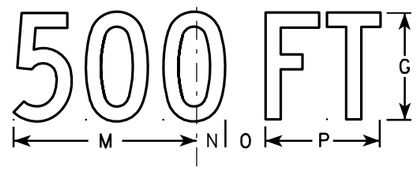
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/21/17 PLATE NO. W14-3.10

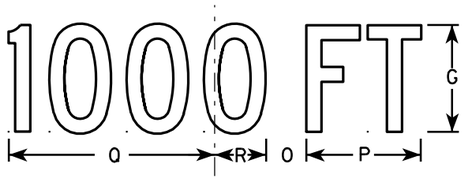
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 178 | E



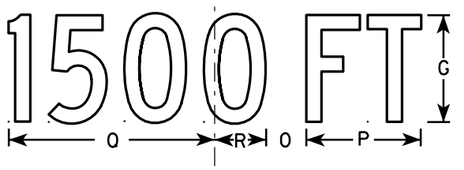
W20-2A



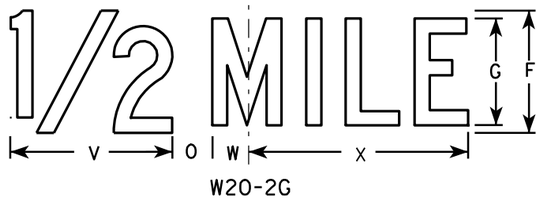
W20-2D



W20-2C



W20-2B



W20-2G

W20-2F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
   Line 2 is Series D for AHEAD and Series C for all other distances.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	O	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
2M	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
3	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
4	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
5	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0

STANDARD SIGN
W20-2A,B,C,D,F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-2.6

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 179 | E

7

FILE NAME : C:\Users\PROJECTS\tr_stdplate\W202.DGN PLOT DATE : 18-MAR-2011 10:00 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 9.931739:1.000000
WISDOT/CADDS SHEET 42

STATE PROJECT NUMBER

5280-03-70

* PROVIDE FOR THRIE BEAM GUARD RAIL ATTACHMENT. AT UNUSED ANCHOR ASSEMBLIES CAULK HOLES SHUT WITH "100% SILICONE CAULK".

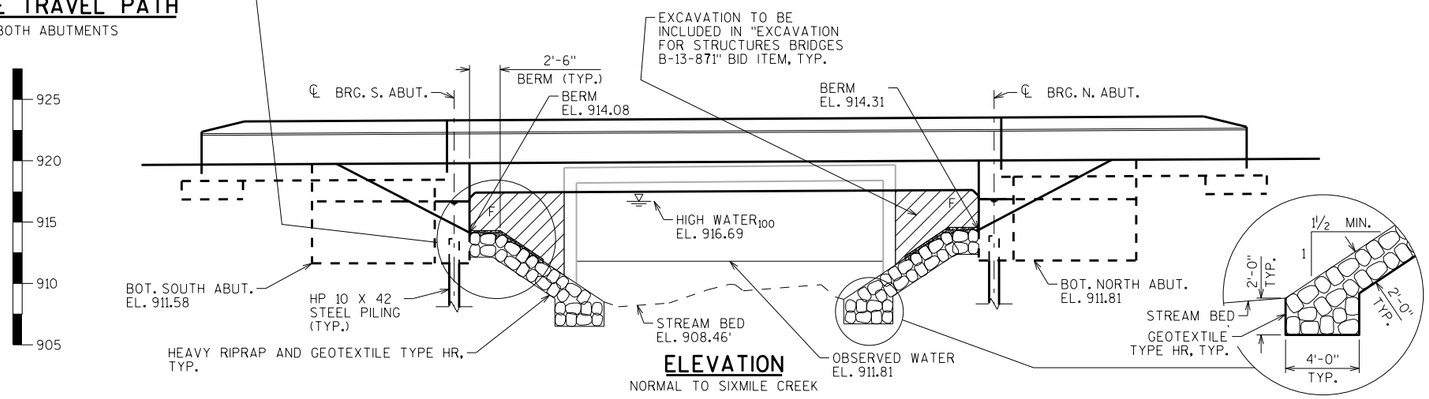
(2) INDICATES WING NUMBER

PLAN

SINGLE SPAN FLAT CONCRETE SLAB

WILDLIFE TRAVEL PATH

TYP. @ BOTH ABUTMENTS



ELEVATION

NORMAL TO SIXMILE CREEK

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HL-93
INVENTORY RATING FACTOR: RF = 1.24
OPERATING RATING FACTOR: RF = 1.61
WISCONSIN STANDARD PERMIT VEHICLE (WIS.-SPV): 250(KIPS)

STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 POUNDS PER SQUARE FOOT.

MATERIAL PROPERTIES:

CONCRETE MASONRY:
SUPERSTRUCTURE & STRUCTURAL APPROACH SLAB — f'c = 4,000 P.S.I.
ALL OTHER — f'c = 3,500 P.S.I.

BAR STEEL REINFORCEMENT:
GRADE 60 — fy = 60,000 P.S.I.
STAINLESS, GRADE 60 — fy = 60,000 P.S.I.

FOUNDATION DATA

ABUTMENTS TO BE SUPPORTED ON HP 10 X 42 PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 165 TONS ** PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA.
ESTIMATED 45'-0" LONG AT SOUTH ABUTMENT.
ESTIMATED 40'-0" LONG AT NORTH ABUTMENT.

** THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS THE REQUIRED DRIVING RESISTANCE MULTIPLIED BY A RESISTANCE FACTOR OF 0.5 USING MODIFIED GATES TO DETERMINE DRIVEN PILE CAPACITY.

HYDRAULIC DATA

100 YEAR FREQUENCY

Q_{100} = 1329 C.F.S.
$VEL._{100}$ = 10.1 F.P.S.
$HW._{100}$ = EL. 916.69
WATERWAY AREA = 131 SQ. FT.
DRAINAGE AREA = 36.7 SQ. MI.
ROADWAY OVERTOPPING = N/A
SCOUR CRITICAL CODE = 8

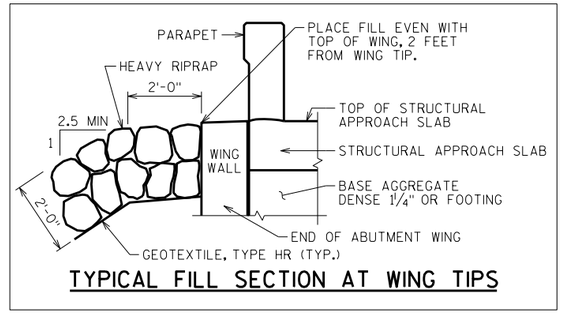
2 YEAR FREQUENCY

Q_2 = 600 C.F.S.
$VEL._2$ = 5.8 F.P.S.
$HW._2$ = EL. 914.34

TRAFFIC VOLUME

FEATURE ON

ADT = 4,900 (2023)
R.D.S. = 35 M.P.H.



TYPICAL FILL SECTION AT WING TIPS

STRUCTURE DESIGN CONTACTS:

CHRISTOPHER DOLL (608) 266-3229
KYLE BUSCH (608) 267-0465

NO.	DATE	REVISION	BY

BUREAU OF STRUCTURES

ACCEPTED KHB 7/24/23
CHIEF STRUCTURES DESIGN ENGINEER — DATE

LIST OF DRAWINGS

STRUCTURE B-13-871

STH 113 OVER SIXMILE CREEK

COUNTY	VILLAGE
DANE	WAUNAKEE

DESIGN SPEC.
AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS

DESIGNED BY	CAD	DESIGNED CK'D.	IDL	DRAWN BY	CAD	PLANS CK'D.	IDL

GENERAL PLAN

SHEET 1 OF 11

180

I.D. 5280-03-00A

DATE: JULY 2023

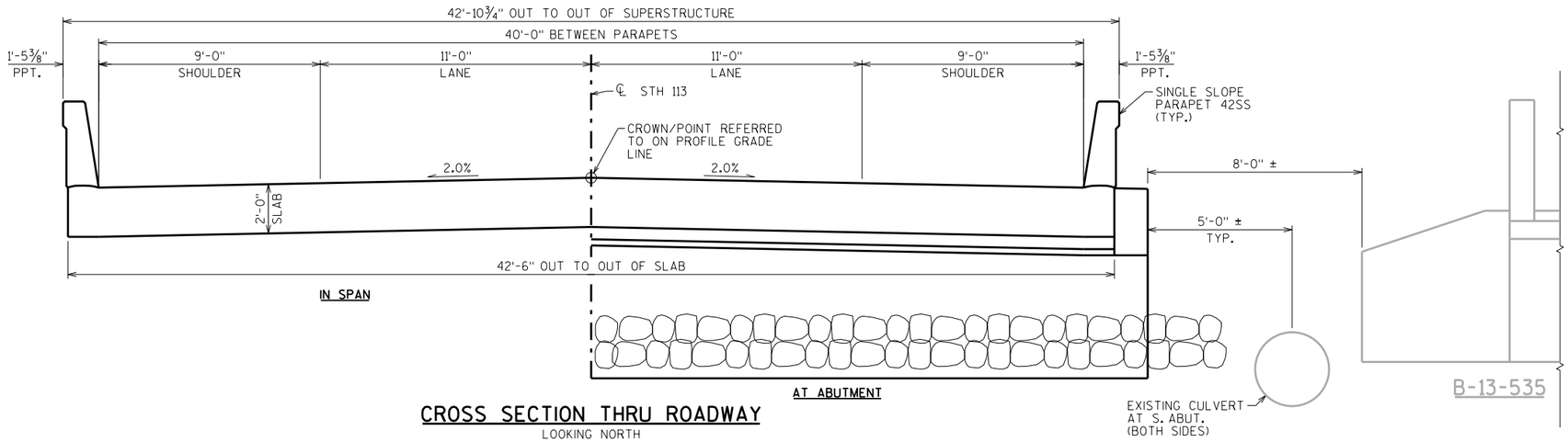
SCALE = 6.00

8

8

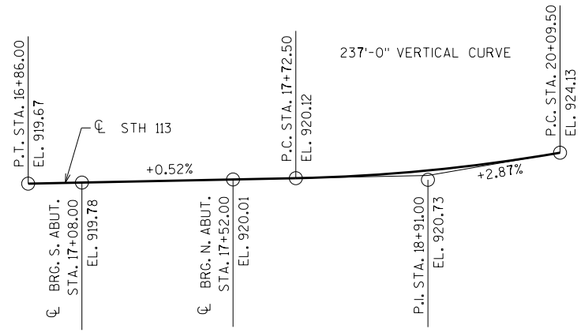
STATE PROJECT NUMBER

5280-03-70



CROSS SECTION THRU ROADWAY

LOOKING NORTH

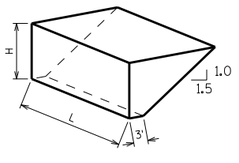


PROFILE GRADE LINE - STH 113

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	SUPER	S. STRUCT. APP. SLAB	SOUTH ABUT.	NORTH ABUT.	N. STRUCT. APP. SLAB	TOTALS
203.0260	REMOVING STRUCTURE OVER WATERWAY MINIMAL DEBRIS B-13-945	EACH	———	———	———	———	———	1
206.1001	EXCAVATION FOR STRUCTURES BRIDGES B-13-871	EACH	———	———	———	———	———	1
210.1500	BACKFILL STRUCTURE TYPE A	TON	———	———	90	90	———	180
305.0120	BASE AGGREGATE DENSE 1 1/4-INCH	TON	———	134	———	———	134	268
502.0100	CONCRETE MASONRY BRIDGES	CY	164.7	58.7	39.4	39.4	58.7	361
502.3200	PROTECTIVE SURFACE TREATMENT	SY	223	90	———	———	90	403
502.3210	PIGMENTED SURFACE SEALER	SY	45	20	———	———	20	85
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	———	———	2700	2700	———	5400
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	30,780	9890	1260	1270	9890	53,090
505.0800.S	BAR STEEL REINFORCEMENT HS STAINLESS STRUCTURES	LB	390	———	———	———	———	390
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	———	———	11	11	———	22
550.1100	PILING STEEL HP 10-INCH X 42 LB	LF	———	———	315	280	———	595
606.0300	RIPRAP HEAVY	CY	———	———	87	98	———	185
612.0406	PIPE UNDERDRAIN WRAPPED 6-INCH	LF	———	———	80	80	———	160
614.0150	ANCHOR ASSEMBLIES FOR STEEL PLATE BEAM GUARD	EACH	———	2	———	———	2	4
645.0111	GEOTEXTILE TYPE DF SCHEDULE A	SY	———	———	38	38	———	76
645.0120	GEOTEXTILE TYPE HR	SY	———	———	139	155	———	294
SPV.0090	PRE-BORING SPECIAL	LF	———	———	16 ☆	16 ☆	———	32 ☆
SPV.0195	SELECT CRUSHED MATERIAL FOR TRAVEL CORRIDOR	TON	———	———	34	34	———	68
	NON-BID ITEMS							
	FILLER	SIZE						½", ¾", 1½"

☆ QUANTITY SHOWN ASSUMES PRE-BORING WILL BE REQUIRED AT 2 PILES PER ABUTMENT (WORST CASE). PREBORING MAY ONLY BE REQUIRED AT 1 PILE PER ABUTMENT, REDUCING THE PAY QUANTITY BY 50%.



ABUTMENT BACKFILL DIAGRAM FOR WINGS PARALLEL TO ROADWAY

L = OUT TO OUT OF ABUTMENT, INCLUDING WINGS (FT)
H = AVERAGE ABUTMENT FILL HEIGHT (FT)
EF = EXPANSION FACTOR (1.20 FOR CY BID ITEMS AND 1.00 FOR TON BID ITEMS)
V_{CF} = (L)(3.0')(H) + (L)(0.5)(1.5H)(H)
V_{CY} = V_{CF} (EF)/27
V_{TON} = V_{CY} (2.0)

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BEVEL EXPOSED EDGES OF CONCRETE ¾" UNLESS OTHERWISE NOTED.

THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES B-13-871" SHALL BE THE EXISTING GROUNDLINE.

AT THE BACK FACE OF ABUTMENT ALL VOLUME WHICH CANNOT BE PLACED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL TYPE A. ALSO EXCLUDED IS THE "BASE AGGREGATE DENSE 1 1/4-INCH" AS DETAILED ON THE STRUCTURAL APPROACH SLAB SHEETS.

EXCAVATION BELOW THE ABUTMENT AND ABUTMENT BEDDING MATERIALS REQUIRES ENGINEER APPROVAL. GEOTEXTILE SHALL BE SET AT THE BOTTOM OF EXCAVATION AND EXTEND 2'-0" ABOVE BOTTOM OF ABUTMENT.

THE QUANTITY FOR BACKFILL STRUCTURE IS CALCULATED BASED ON THE DETAIL SHOWN IN THE PLANS.

PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO THE ENTIRE EXPOSED TOP OF DECK AND APPROACH SLAB SURFACES AND TO THE VERTICAL AND HORIZONTAL SURFACES OF THE PAVING NOTCHES AT ABUTMENT DIAPHRAGMS.

PIGMENTED SURFACE SEALER TO BE APPLIED TO THE FRONT FACE AND THE TOP OF THE PARAPETS, INCLUDING PARAPETS ON APPROACH SLABS.

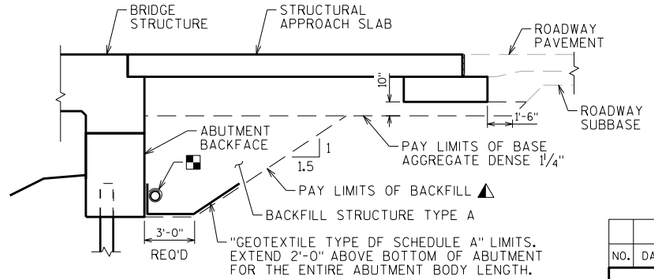
THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH HEAVY RIPRAP AND GEOTEXTILE TYPE "HR" TO THE EXTENT SHOWN ON SHEET 1 AND THE ABUTMENT DETAILS.

SLAB FALSEWORK SHALL BE SUPPORTED ON PILES OR THE SUBSTRUCTURE, UNLESS AN ALTERNATE METHOD IS APPROVED BY THE ENGINEER.

AT ABUTMENTS, CONCRETE POURED UNDER WATER WILL BE ALLOWED AND SHALL BE DONE IN ACCORDANCE WITH SECTION 502.3.5.3 OF THE STANDARD SPECIFICATIONS.

THE UTILITY INFORMATION SHOWN ON THESE DRAWINGS CONCERNING TYPE AND LOCATION OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL-INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE FOR MAKING THEIR OWN DETERMINATION AS TO TYPE AND LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE.

CONSTRUCTION OF BRIDGE B-13-871 MAY NEED TO BE ACCOMPLISHED BENEATH AND/OR ADJACENT TO ENERGIZED OVERHEAD 69 KV ELECTRIC TRANSMISSION LINES OWNED AND OPERATED BY THE AMERICAN TRANSMISSION COMPANY (ATC). THE HORIZONTAL AND VERTICAL LOCATION OF THESE LINES CAN VARY WITH WIND, AMBIENT TEMPERATURE, AND POWER LOADING. THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING IF IT IS SAFE TO WORK NEAR THE 69 KV FACILITIES BASED ON THE LATEST OSHA REQUIREMENTS. ATC MANAGEMENT OUTAGE REQUESTS CAN BE MADE ON A ONE-TO-THREE DAY INTERVAL AND ARE FIRST COME, FIRST SERVE BASED ON DEMAND, WEATHER, AND TIME OF YEAR. ATC MANAGEMENT WILL NEED 6 TO 8 WEEKS NOTICE TO DETERMINE IF AN OUTAGE CAN BE OBTAINED AT THE TIME REQUESTED. IF AN OUTAGE IS APPROVED, THE CONTRACTOR SHALL COMPLETE A THIRD-PARTY OUTAGE AGREEMENT WITH ATC MANAGEMENT.



TYPICAL SECTION THRU ABUTMENT

◭ BACKFILL PAY LIMITS. BACKFILL BEYOND BACKFILL PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.

▪ PIPE UNDERDRAIN WRAPPED (6 INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

STRUCTURE B-13-871

DRAWN BY CAD | PLANS CK'D. IDL

CROSS SECTION & QUANTITIES

SHEET 2

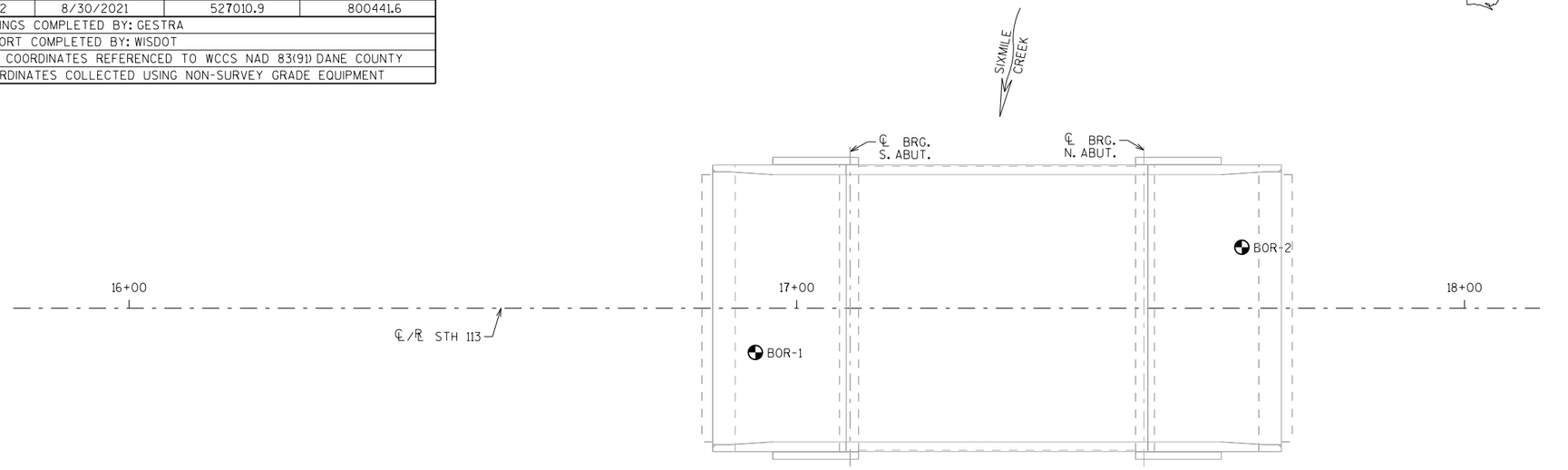
181

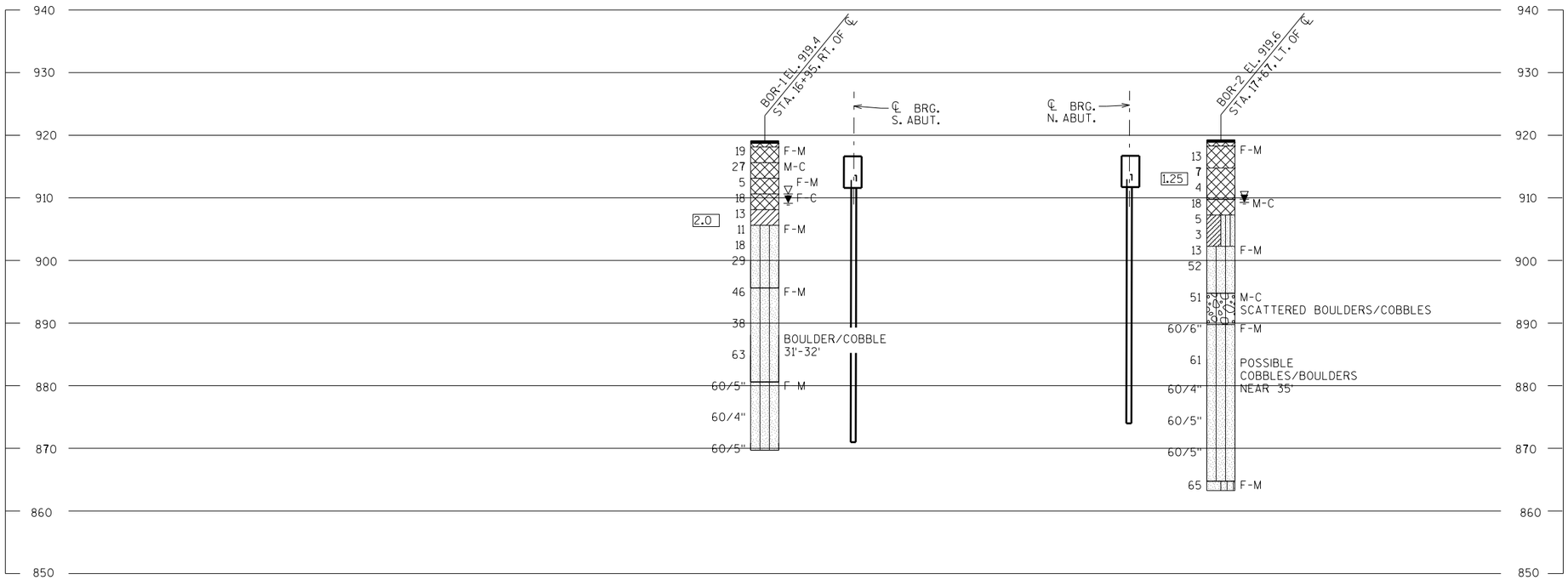
SCALE = 2.75

8

8

BORING #	DATE COMPLETED	NORTHING (Y)	EASTING (X)
1	8/27/2021	526938.1	800457.6
2	8/30/2021	527010.9	800441.6
BORINGS COMPLETED BY: GESTRA			
REPORT COMPLETED BY: WISDOT			
ALL COORDINATES REFERENCED TO WCCS NAD 83(91) DANE COUNTY			
COORDINATES COLLECTED USING NON-SURVEY GRADE EQUIPMENT			





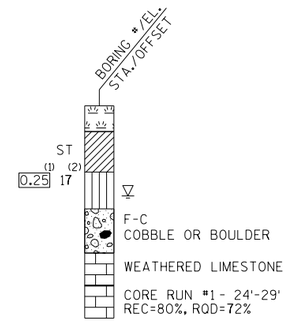
STATE PROJECT NUMBER

5280-03-70

MATERIAL SYMBOLS

ASPHALT	TOPSOIL	PEAT
CONCRETE	FILL	GRAVEL
SAND	CLAY	SILT
BOULDERS OR COBBLES	LIMESTONE	BEDROCK (UNKNOWN)
SHALE	SANDSTONE	IGNEOUS/ META

LEGEND OF BORING



(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)

(2) UNLESS OTHERWISE, SPECIFIED THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION

- ▽ AT TIME OF DRILLING
- ▼ END OF DRILLING
- ▽ AFTER DRILLING

ABBREVIATIONS

F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

STRUCTURE B-13-871

DRAWN BY JSJ/CAD | PLANS CK'D. IDL

SUBSURFACE EXPLORATION

SHEET 3

182

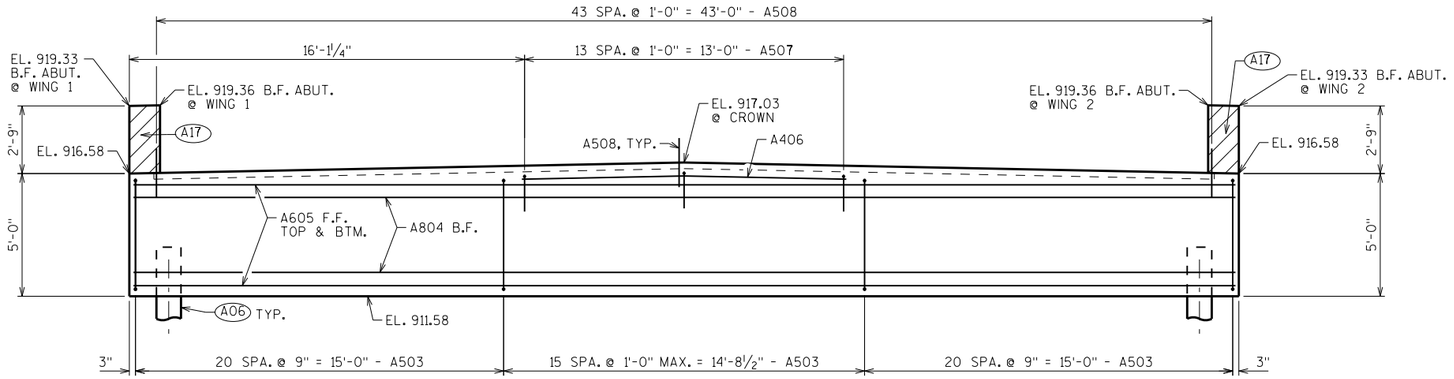
8

8

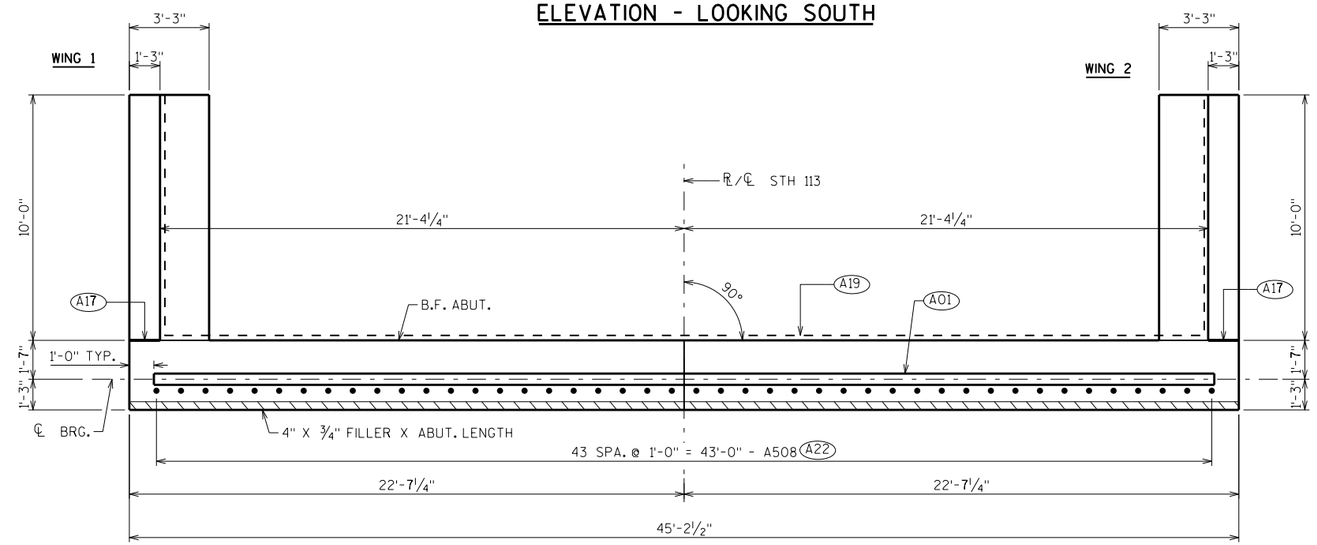
SCALE =

STATE PROJECT NUMBER

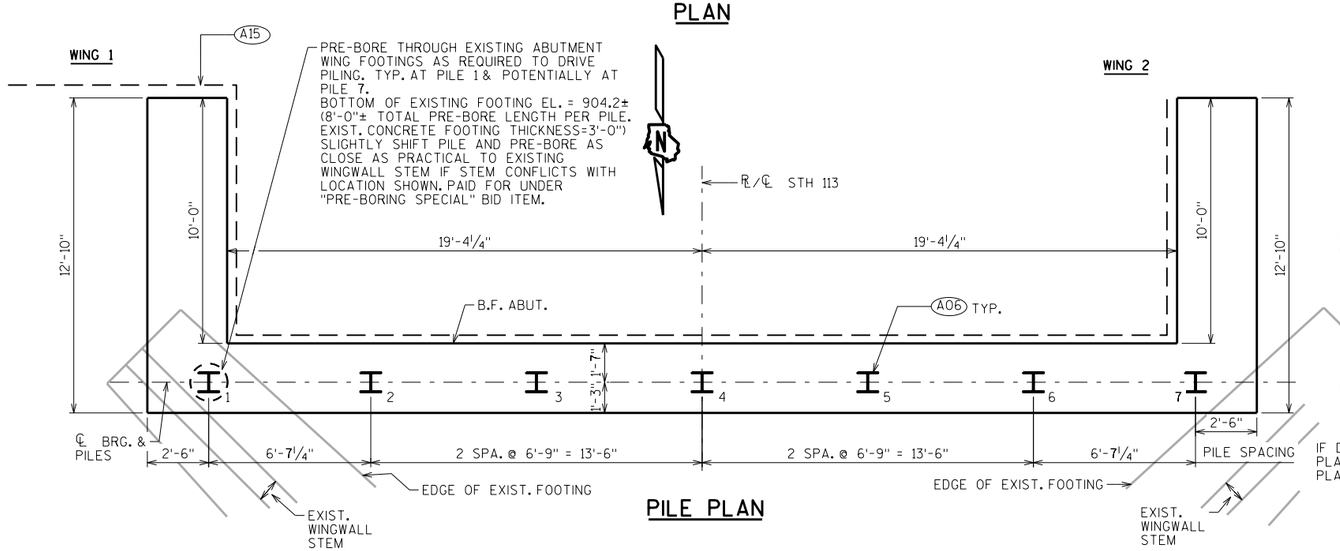
5280-03-70



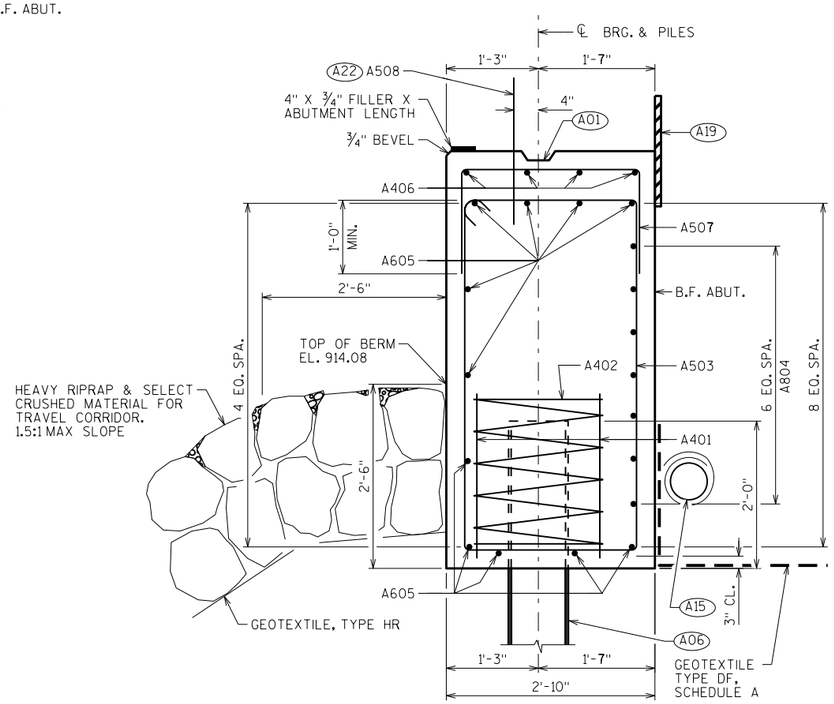
ELEVATION - LOOKING SOUTH



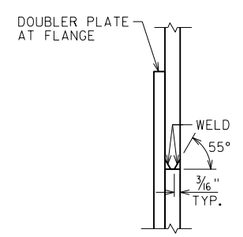
PLAN



PILE PLAN

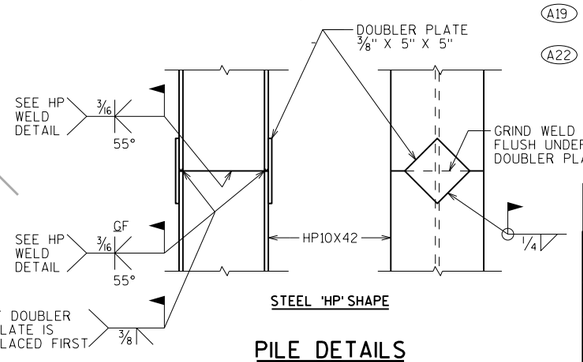


SECTION THRU BODY



HP WELD DETAIL

FLANGE SHOWN, WEB SIMILAR



STEEL 'HP' SHAPE

PILE DETAILS

(A01) CONST. JOINT: KEYWAY FORMED BY A BEVELED 2 x 6.

(A06) SUPPORT ABUTMENT ON HP 10 x 42 STEEL PILING, ESTIMATED 45'-0" LONG WITH A REQUIRED DRIVING RESISTANCE OF 165 TONS PER PILE.

(A15) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED. MINIMUM BTM. OF PIPE ELEVATION = 912.81

(A17) 1/2" FILLER (INCLUDED IN WING LENGTH); SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.

(A19) 18" (RMW) RUBBERIZED MEMBRANE WATERPROOFING SEAL ALL HORIZ. & VERT. JOINTS AT BACKFACE.

(A22) BARS @ 1'-0" CTRS. MAY BE PLACED AFTER CONCRETE IS POURED BUT BEFORE INITIAL SET HAS TAKEN PLACE. (EMBED 1'-0" INTO CONC.)

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

STRUCTURE B-13-871

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SOUTH ABUTMENT

SHEET 4

183

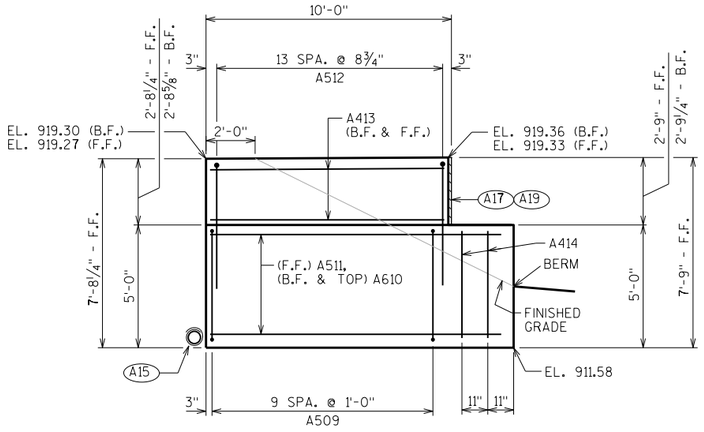
8

8

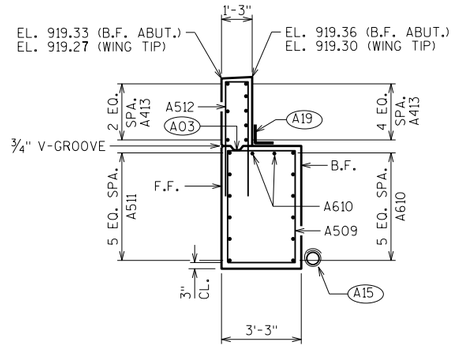
SCALE = 3.00

STATE PROJECT NUMBER

5280-03-70



WING 1 ELEVATION



WING 1 SECTION

(A03) OPTIONAL CONST. JOINT: KEYWAY FORMED BY BEVELED 2 × 6. (18" R.M.W. @ B.F. & 3/4" "V" GROOVE @ F.F. IF JOINT IS USED).

(A15) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.

(A17) 1/2" FILLER (INCLUDED IN WING LENGTH): SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.

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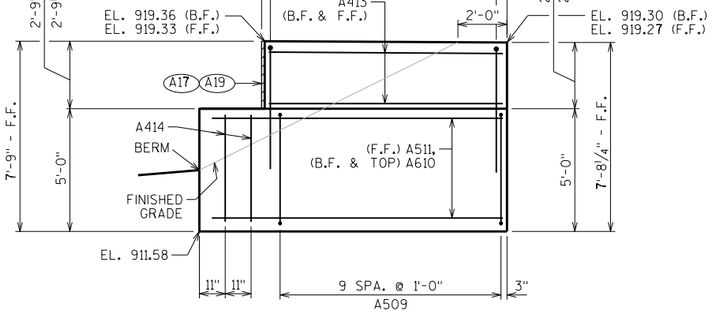
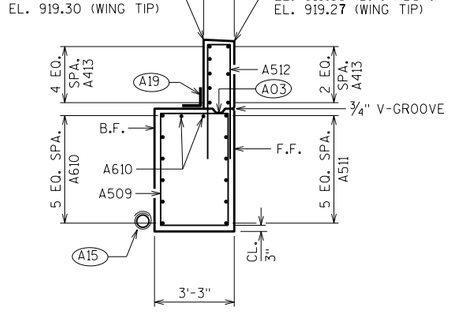
BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BAR MARK	COAT	NO. REQ'D.	LENGTH	BENT	LOCATION
A401		14	2'-3"		PILES - 2 PER BODY PILE
A402		7	28'-0"	X	PILES - 1 PER BODY PILE
A503		56	14'-6"	X	BODY STIRRUPS
A804		7	44'-10"		BODY - HORIZ. - B.F.
A605		11	44'-9"		BODY - HORIZ. - TOP, F.F. & BTM.
A406		4	13'-6"		BODY - HORIZ. - TOP
A507		14	5'-3"	X	BODY - TOP
A508	X	44	2'-0"		BODY - VERT. - TOP - STAB BARS
A509	X	20	15'-8"	X	WING 1 & 2 - STIRRUPS
A610	X	16	12'-4"		WING 1 & 2 - HORIZ. - B.F. & TOP
A511	X	12	12'-6"		WING 1 & 2 - HORIZ. - F.F.
A512	X	28	9'-10"	X	WING 1 & 2 - VERT. - TOP
A413	X	16	9'-8"		WING 1 & 2 - HORIZ. - TOP - F.F. & B.F.
A414		4	4'-6"		BODY VERT. - ENDS

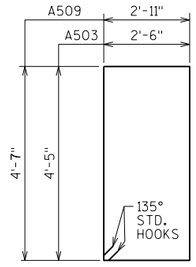


WING 2 ELEVATION

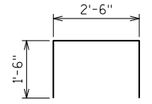


WING 2 SECTION

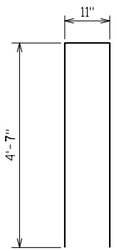
A402



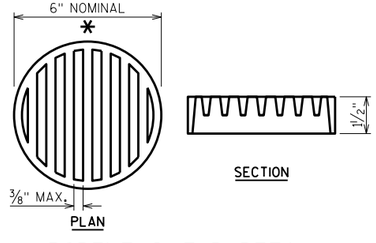
A503, A509



A507



A512



RODENT SHIELD DETAIL

* DIMENSIONS ARE APPROXIMATE. THE GRATE IS SIZED TO FIT INTO A PIPE COUPLING. ORIENT SO SLOTS ARE VERTICAL.

THE RODENT SHIELD, PIPE COUPLING AND SCREWS SHALL BE CONSIDERED INCIDENTAL TO THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6-INCH".

THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALLY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE EXPOSED END OF THE PIPE UNDERDRAIN. THE SHIELD SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1-INCH STAINLESS STEEL SHEET METAL SCREWS.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

STRUCTURE B-13-871

DRAWN BY CAD | PLANS CK'D. IDL

SOUTH ABUTMENT DETAILS

SHEET 5

184

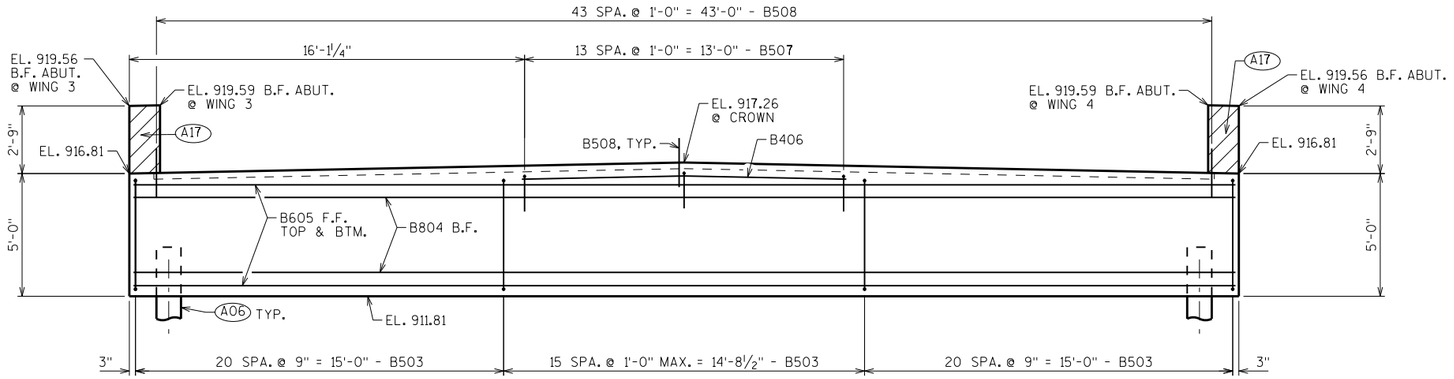
8

8

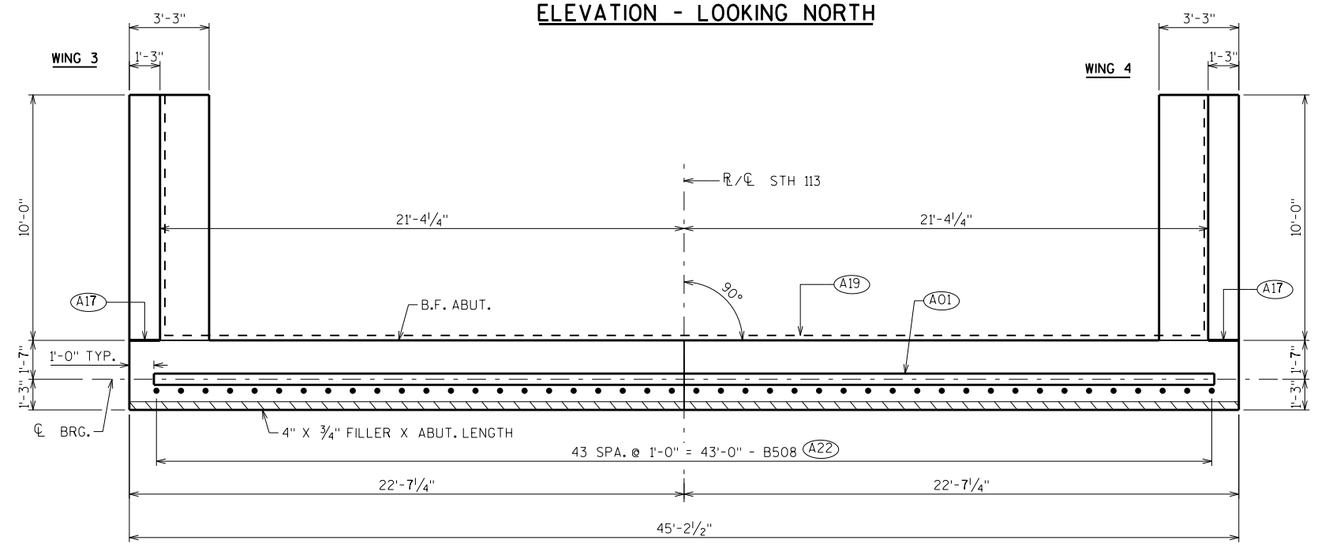
SCALE = 3.00

STATE PROJECT NUMBER

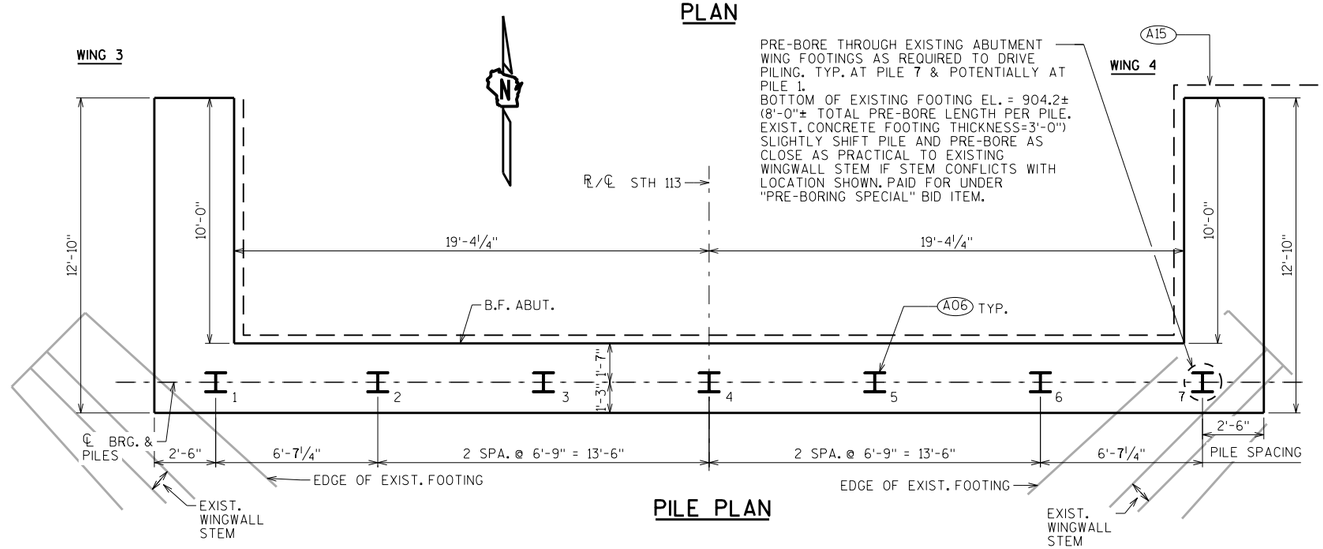
5280-03-70



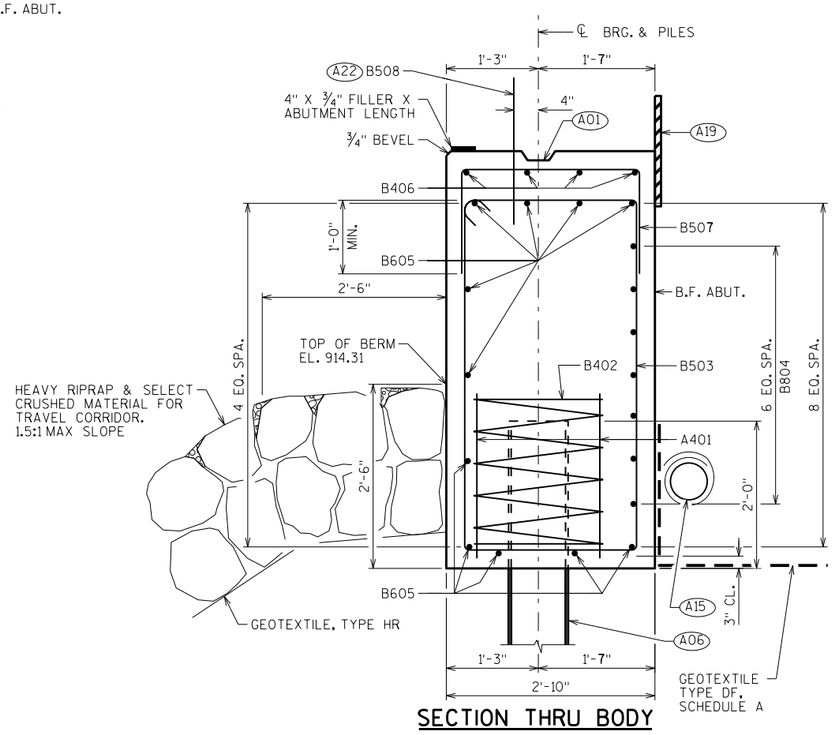
ELEVATION - LOOKING NORTH



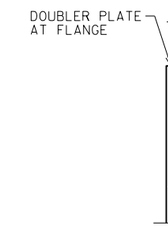
PLAN



PILE PLAN

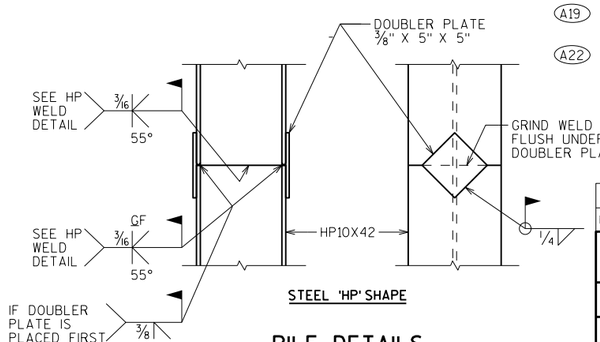


SECTION THRU BODY



HP WELD DETAIL

FLANGE SHOWN, WEB SIMILAR



STEEL 'HP' SHAPE

PILE DETAILS

(A01) CONST. JOINT: KEYWAY FORMED BY A BEVELED 2 x 6.

(A06) SUPPORT ABUTMENT ON HP 10 x 42 STEEL PILING. ESTIMATED 40'-0" LONG WITH A REQUIRED DRIVING RESISTANCE OF 165 TONS PER PILE.

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STRUCTURES DESIGN SECTION

STRUCTURE B-13-871

DRAWN BY CAD | PLANS CK'D. IDL

NORTH ABUTMENT

SHEET 6

185

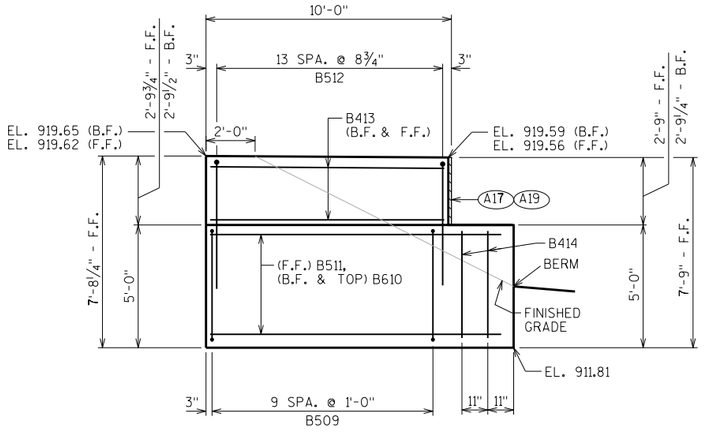
8

8

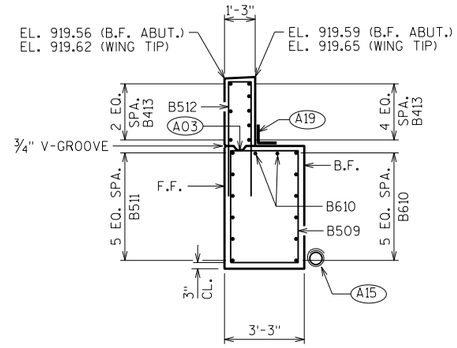
SCALE = 3.00

STATE PROJECT NUMBER

5280-03-70



WING 3 ELEVATION



WING 3 SECTION

(A03) OPTIONAL CONST. JOINT: KEYWAY FORMED BY BEVELED 2 × 6. (18" R.M.W. @ B.F. & 3/4" "V" GROOVE @ F.F. IF JOINT IS USED).

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BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BAR MARK	COAT	NO. REQ'D.	LENGTH	BENT	LOCATION
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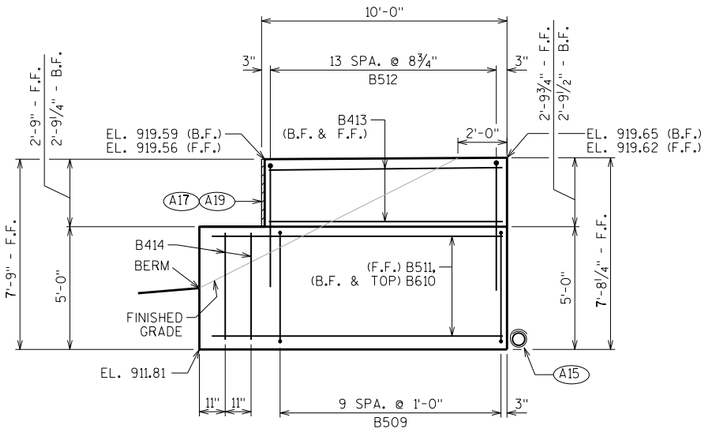
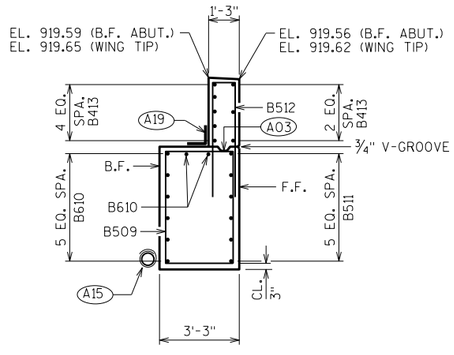
B503		56	14'-6"	X	BODY STIRRUPS
B804		7	44'-10"		BODY - HORIZ. - B.F.
B605		11	44'-9"		BODY - HORIZ. - TOP, F.F. & BTM.
B406		4	13'-6"		BODY - HORIZ. - TOP
B507		14	5'-3"	X	BODY - TOP
B508	X	44	2'-0"		BODY - VERT. - TOP - STAB BARS
B509	X	20	15'-8"	X	WING 3 & 4 - STIRRUPS
B610	X	16	12'-4"		WING 3 & 4 - HORIZ. - B.F. & TOP
B511	X	12	12'-6"		WING 3 & 4 - HORIZ. - F.F.
B512	X	28	10'-0"	X	WING 3 & 4 - VERT. - TOP
B413	X	16	9'-8"		WING 3 & 4 - HORIZ. - TOP - F.F. & B.F.
B414		4	4'-6"		BODY VERT. - ENDS

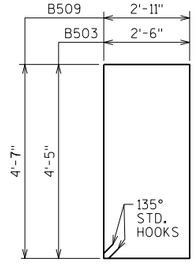


WING 4 ELEVATION

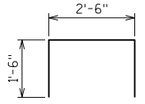


WING 4 SECTION

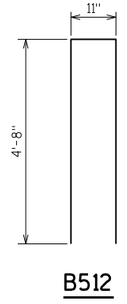
B402



B503, B509



B507



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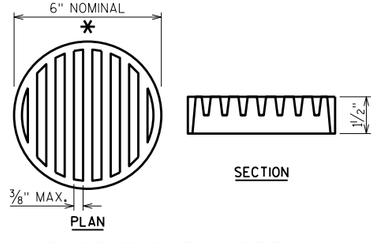

RODENT SHIELD DETAIL

* DIMENSIONS ARE APPROXIMATE. THE GRATE IS SIZED TO FIT INTO A PIPE COUPLING. ORIENT SO SLOTS ARE VERTICAL.

THE RODENT SHIELD, PIPE COUPLING AND SCREWS SHALL BE CONSIDERED INCIDENTAL TO THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6-INCH".

THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALLY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE EXPOSED END OF THE PIPE UNDERDRAIN. THE SHIELD SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1-INCH STAINLESS STEEL SHEET METAL SCREWS.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
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STRUCTURE B-13-871

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NORTH ABUTMENT DETAILS

SHEET 7

186

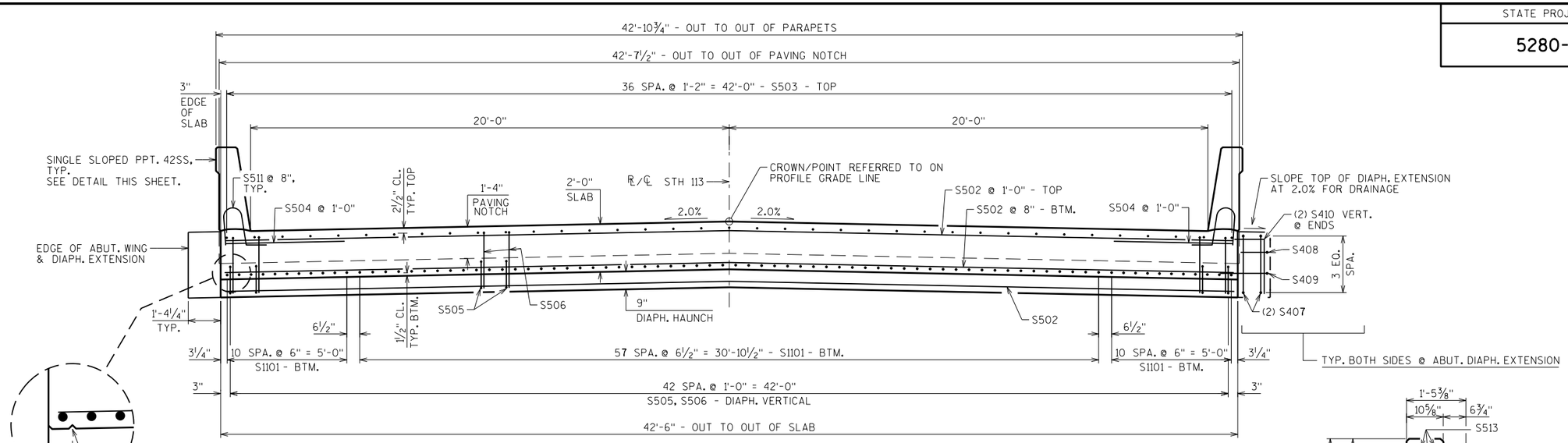
SCALE = 3.00

8

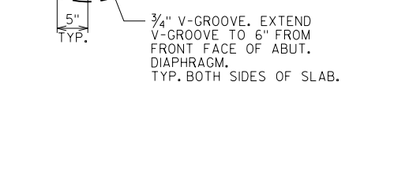
8

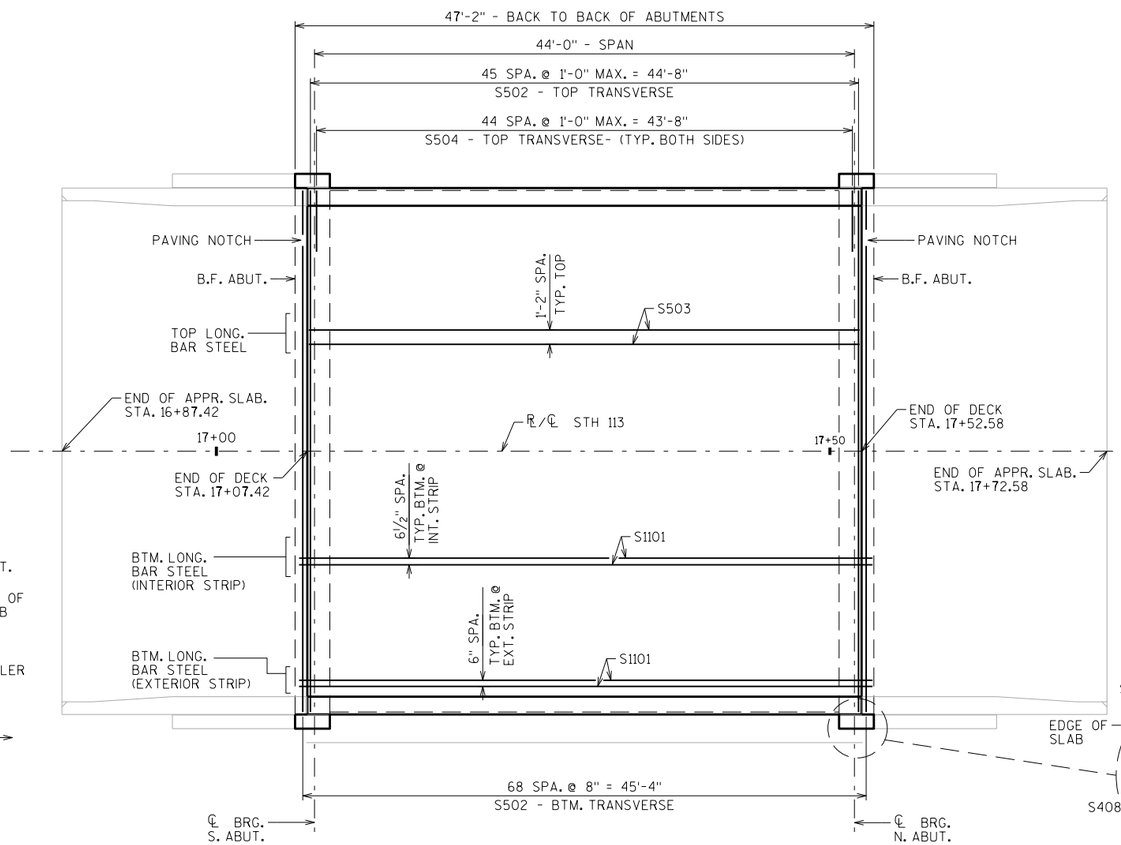
STATE PROJECT NUMBER

5280-03-70

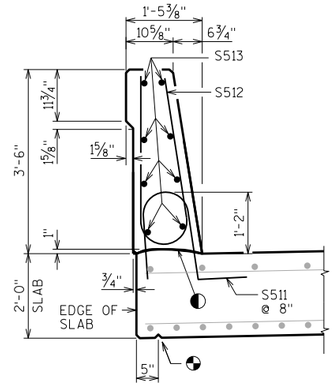


CROSS SECTION THRU BRIDGE (℄ ABUT. DIAPH.)





PLAN



SECTION DETAIL OF PARAPET ON BRIDGE

◐ CONSTRUCTION JOINT - STRIKE OFF AS SHOWN

◒ ¾" V-GROOVE. EXTEND V-GROOVE TO 6" FROM FRONT FACE OF ABUT. DIAPHRAGM. V-GROOVES ARE REQUIRED.

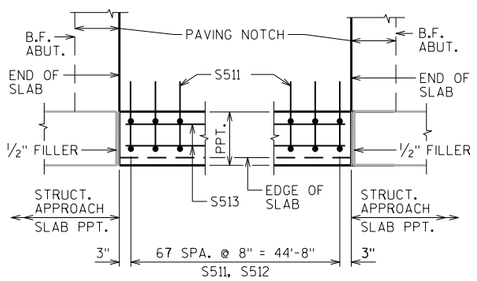
NOTES

TOP TRANVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 3'-0" CENTERS EACH WAY. BOTTOM LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 4'-0" CENTERS.

ALL SLAB THICKNESS DIMENSIONS ARE MINIMUM. ANY TOLERANCES NECESSARY TO CORRECT CONSTRUCTION DISCREPANCIES ARE TO BE (+).

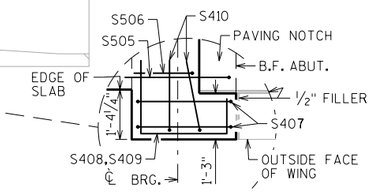
PARAPETS PLACED ON TOP OF THE SLAB SHALL BE POURED AFTER FALSEWORK HAS BEEN RELEASED.

CAMBER SPANS AS SHOWN TO PROVIDE FOR DEAD LOAD DEFLECTION AND FUTURE CREEP. CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT. (SEE "SUPERSTRUCTURE DETAILS" SHEET FOR CAMBER VALUES.)



PARTIAL PLAN DETAIL OF PARAPET REINF.

(SHOWING S.E. & N.E. CORNERS - DETAIL TYP. @ BOTH SIDES OF SUPERSTRUCTURE)



PARTIAL PLAN DETAIL OF DIAPH. EXTENSION

TYP. ALL (4) CORNERS

NO.	DATE	REVISION	BY

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SUPERSTRUCTURE

SHEET 8

187

SCALE = 6.00

8

8

STATE PROJECT NUMBER

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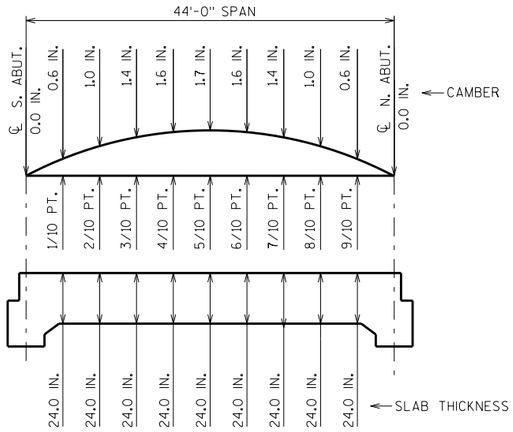


CAMBER AND SLAB THICKNESS DIAGRAM

CAMBER IS BASED ON 3 TIMES DEAD LOAD DEFLECTIONS.

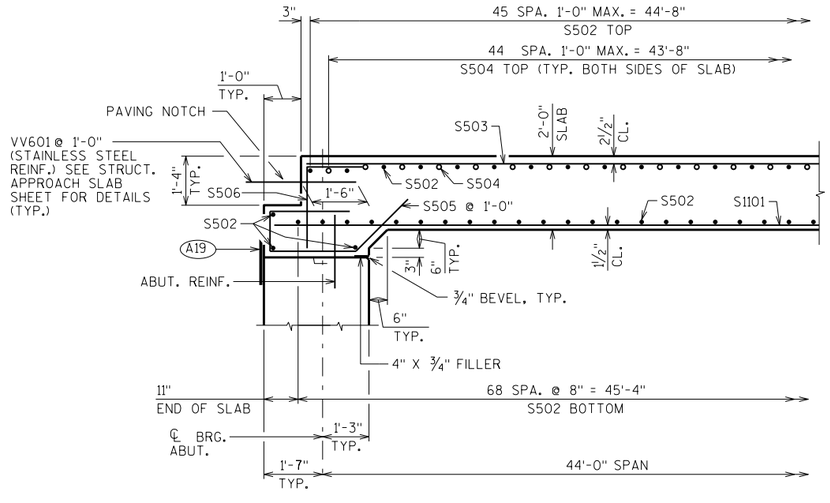
CAMBER SPANS AS SHOWN TO PROVIDE FOR DEADLOAD DEFLECTION & FUTURE CREEP.
CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT.

PARAPETS SHOWN ABOVE THE HORIZ. CONST. JT. SHALL BE POURED AFTER FALSEWORK HAS BEEN RELEASED.

TO DETERMINE FALSEWORK ELEVATION AT EDGE OF SLAB, CROWN OR CENTERLINE FOLLOW THIS PROCEDURE:

	TOP OF DECK ELEVATION AT FINAL GRADE
LESS	SLAB THICKNESS
PLUS	CAMBER
PLUS	FORM SETTLEMENT/DEFLECTION DUE TO PLACEMENT OF SLAB CONCRETE (TO BE COMPUTED BY THE CONTRACTOR)
EQUALS	TOP OF SLAB FALSEWORK ELEVATION.

PRIOR TO RELEASING SLAB FALSEWORK, TAKE TOP OF DECK ELEVATIONS AT THE ℄ OF ABUTMENTS, THE ℄ OF PIERS AND AT 5/10 PTS. TO VERIFY CAMBER. TAKE ELEVATIONS ALONG GUTTER LINES AND CROWN OR ℄.

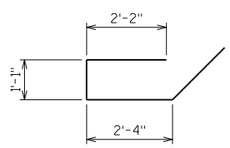


LONGITUDINAL SECTION THRU ROADWAY

BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE

BAR MARK	COAT	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
S1101	X	80	46'-8"			LONG. - BTM.
S502	X	121	42'-2"			TRANS. - TOP & BTM.
S503	X	37	44'-10"			LONG. - TOP
S504	X	90	5'-0"			TRANS. - TOP - @ EDGE OF SLAB
S505	X	86	7'-4"	X		DIAPHRAGM VERT.
S506	X	86	3'-9"	X		DIAPHRAGM VERT.
S407	X	8	10'-0"	X		VERT. - DIAPHRAGM EXTENSIONS
S408	X	4	5'-5"	X		HORIZ. - DIAPHRAGM EXTENSIONS
S409	X	4	6'-10"	X		HORIZ. - DIAPHRAGM EXTENSIONS
S410	X	8	4'-4"	X		VERT. - DAIPHRAGM EXTENSIONS - @ENDS
S511	X	136	4'-5"	X		PARAPET - VERT.
S512	X	136	6'-8"	X		PARAPET - VERT.
S513	X	16	44'-10"			PARAPET - HORIZ.
STAINLESS STEEL BAR →	VV601		86	3'-0"			ABUT. DIAPH. TO APPROACH SLAB



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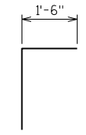

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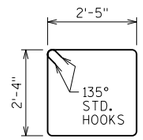

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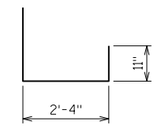

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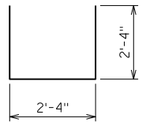

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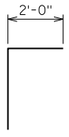

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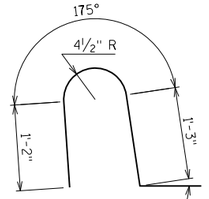

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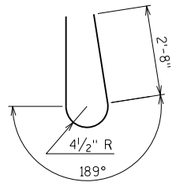

S512

TOP OF DECK ELEVATIONS

	℄ BRG. S. ABUT.	1/10 PT.	2/10 PT.	3/10 PT.	4/10 PT.	5/10 PT.	6/10 PT.	7/10 PT.	8/10 PT.	9/10 PT.	℄ BRG. N. ABUT.
W. GUTTER	919.38	919.40	919.43	919.45	919.47	919.50	919.52	919.54	919.56	919.59	919.61
CL/RL/CROWN	919.78	919.80	919.83	919.85	919.87	919.90	919.91	919.94	919.96	919.99	920.01
E. GUTTER	919.38	919.40	919.43	919.45	919.47	919.50	919.52	919.54	919.56	919.59	919.61

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

STRUCTURE B-13-871

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SUPERSTRUCTURE DETAILS

SHEET 9

188

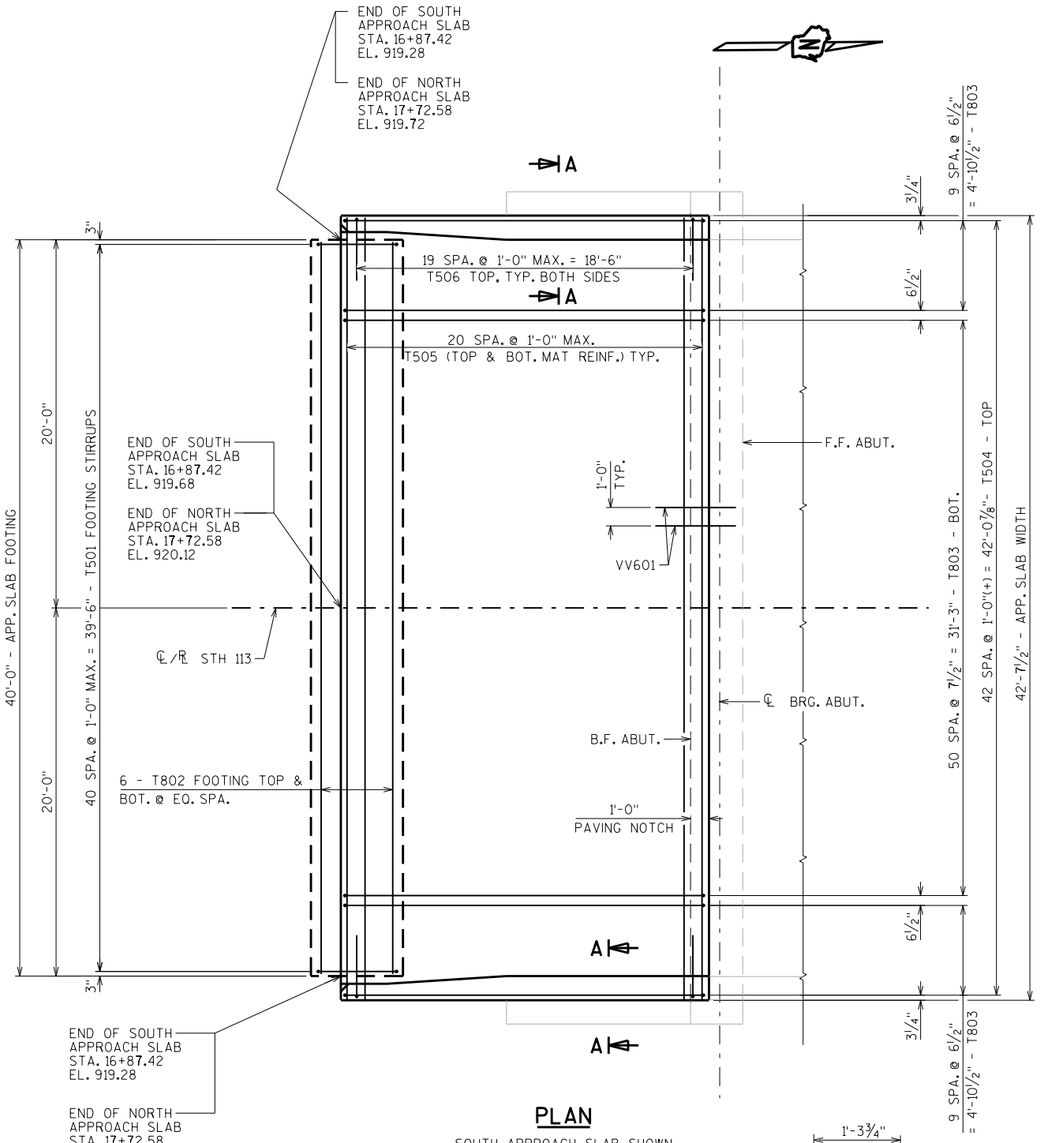
SCALE = 2.00

8

8

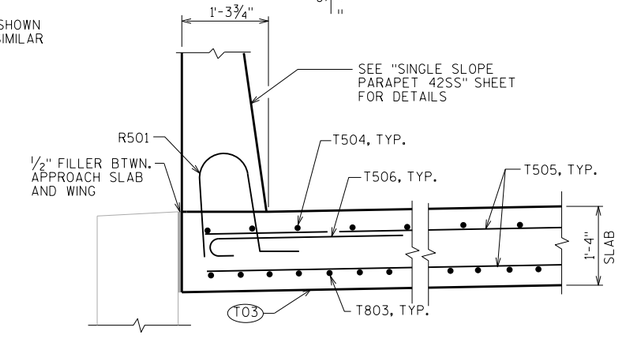
STATE PROJECT NUMBER

5280-03-70



PLAN

SOUTH APPROACH SLAB SHOWN
NORTH APPROACH SLAB SIMILAR

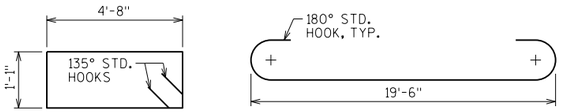


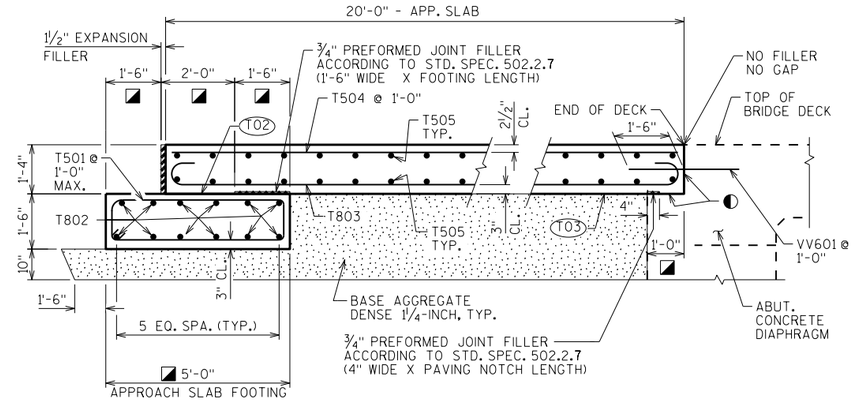
SECTION A-A

BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE

BAR MARK	COAT	NO. REQ'D. SOUTH	NO. REQ'D. NORTH	LENGTH	BENT	BAR SERIES	LOCATION
T501	X	41	41	12'-2"	X		APP. SLAB - FTG. - STIRRUP
T802	X	12	12	39'-8"			APP. SLAB - FTG. - TRANS.
T803	X	71	71	21'-4"	X		APP. SLAB - LONG. - BOT.
T504	X	43	43	19'-6"			APP. SLAB - LONG. - TOP
T505	X	42	42	42'-3"			APP. SLAB - TRANS. - TOP & BOT.
T506	X	40	40	4'-1"	X		APP. SLAB - TRANS. - EDGES





SECTION THRU APPROACH SLAB

◐ APPLY PROTECTIVE SURFACE TREATMENT TO PAVING NOTCH SURFACES PRIOR TO POURING STRUCTURAL APPROACH SLAB.

◪ MEASURED NORMAL TO ABUTMENT

(T02) STEEL TROWEL TOP SURFACE OF FOOTING AND PLACE MULTIPLE LAYERS (0.03" MIN. TOTAL THK.) OF POLYETHYLENE SHEETS OVER THE ENTIRE TOP OF FOOTING.

(T03) PLACE MULTIPLE LAYERS (0.03" MIN. TOTAL THK.) OF POLYETHYLENE SHEETS OVER THE ENTIRE SUBGRADE BENEATH SLAB.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
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STRUCTURE B-13-871

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STRUCTURAL APPROACH SLABS

SHEET 10

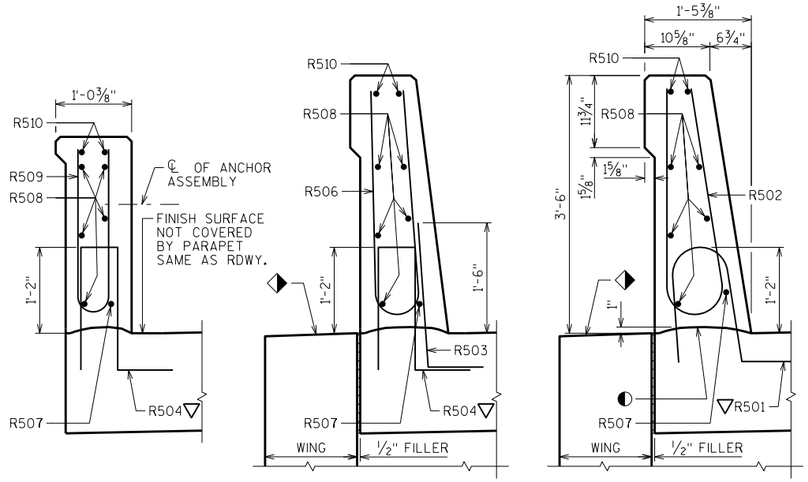
189

SCALE = 4.00

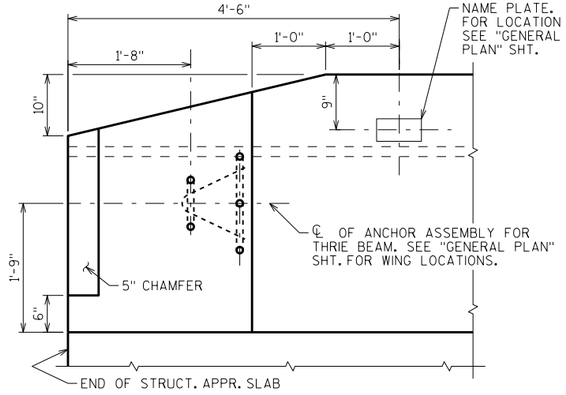
8

8

STATE PROJECT NUMBER
5280-03-70



SECTION A-A SECTION B-B SECTION C-C



PARAPET END TREATMENT DETAIL

LOOKING AT INSIDE FACE OF PARAPET

BILL OF BARS

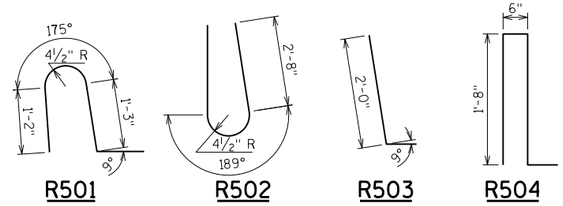
BAR MARK	COAT	S. ABUT.	N. ABUT.	LENGTH	BENT	BAR SERIES	LOCATION
R501	X	34	34	4'-5"	X		PARAPET VERT.
R502	X	34	34	6'-8"	X		PARAPET VERT.
R503	X	24	24	2'-9"	X		PARAPET VERT.
R504	X	34	34	4'-4"	X		PARAPET VERT.
R505	X	10	10	6'-5"	X		PARAPET VERT.
R506	X	12	12	6'-6"	X		PARAPET VERT.
R507	X	2	2	19'-6"	X		PARAPET HORIZ.
R508	X	10	10	19'-6"			PARAPET HORIZ.
R509	X	12	12	5'-5"	X	⚠	PARAPET VERT.
R510	X	4	4	19'-6"	X		PARAPET HORIZ.

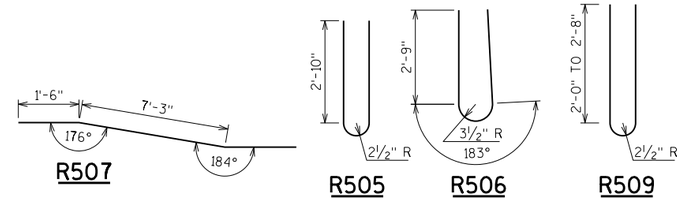
⚠ LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.

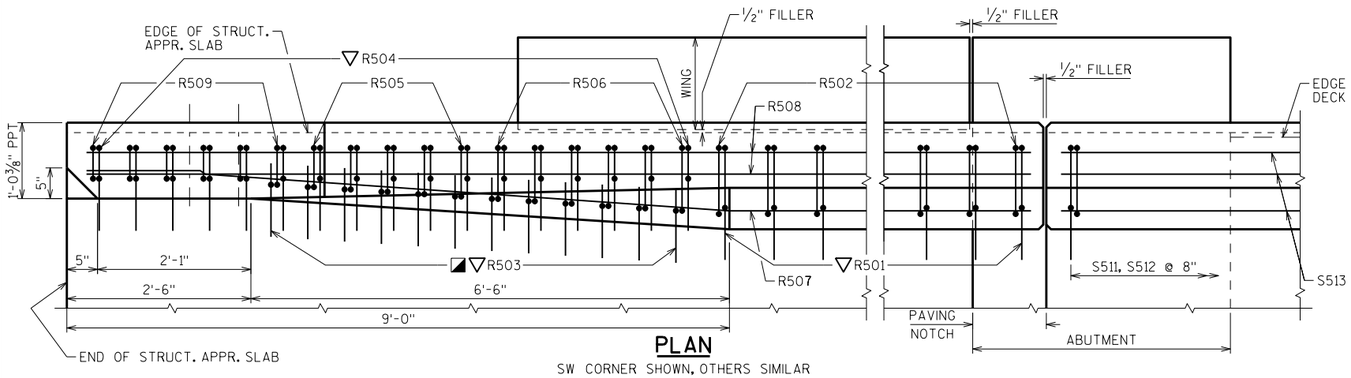
BAR SERIES TABLE

BAR MARK	NO. REQ'D	LENGTH
R509	4 SERIES OF 6	4'-9" TO 6'-1"

BUNDLE AND TAG EACH SERIES SEPARATELY.

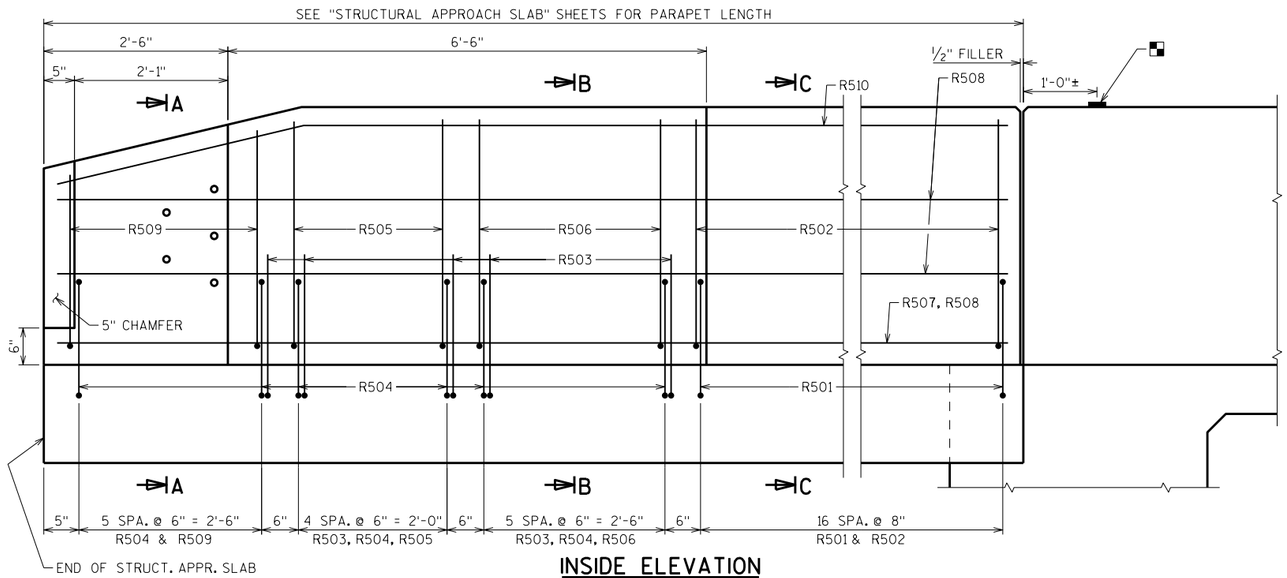






PLAN

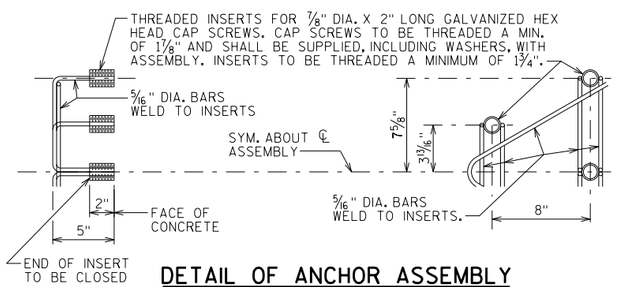
SW CORNER SHOWN, OTHERS SIMILAR



INSIDE ELEVATION

SW CORNER SHOWN, OTHERS SIMILAR
WING & STRUCTURAL APPROACH SLAB FOOTING NOT SHOWN FOR CLARITY

- BENCH MARK CAP (WHEN SUPPLIED). AVOID PLACING A BENCH MARK CAP BELOW A RAIL OR FENCE SYSTEM THAT IS ATTACHED TO THE TOP OF THE PARAPET.



DETAIL OF ANCHOR ASSEMBLY

NOTE: HEX HEAD CAP SCREWS & WASHERS TO BE GALVANIZED IN ACCORDANCE WITH AASHTO M232 CLASS C.

ASSEMBLY SHALL BE BID ITEM "ANCHOR ASSEMBLIES FOR STEEL PLATE BEAM GUARD", EACH.

- ◐ CONST. JOINT - STRIKE OFF AS SHOWN
- ◆ SLOPE FOR DRAINAGE
- ◪ USE CARE TO PLACE R503 BARS CORRECTLY ALONG TRANSITION OF PARAPET.
- ▽ R501, R503, AND R504 BARS TO BE TIED TO STRUCTURAL APPROACH SLAB STEEL BEFORE STRUCTURAL APPROACH SLAB IS POURED.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

STRUCTURE B-13-871

DRAWN BY	CAD	PLANS CK'D.	IDL

SINGLE SLOPE PARAPET 42SS

SHEET 11

190

SCALE = 1.00

8

8

DIVISION	FROM/TO STATION	LOCATION	205.0100 COMMON EXCAVATION (1) CUT (2)	205.0100 COMMON EXCAVATION (1) EBS EXCAVATION (3)	SALVAGED/UNUSABLE PAVEMENT MATERIAL (4)	AVAILABLE MATERIAL (5)	UNEXPANDED FILL	EXPANDED FILL (13) FACTOR 1.43	MASS ORDINATE +/- (14)	WASTE	208.0100 BORROW	COMMENT
DIVISION 1												
QR-113	15+00/23+25	STH 113 RECONSTRUCTION AREA	3,101	0	165	2,936	116	166	2,770	2,770	0	
DIVISION 1 SUBTOTAL			3,101	0	165	2,936	116	166	2,770	2,770	0	
CULVERT REMOVAL	21+58/22+56	STH 112	135	0	0	135	0	0	135	135	0	
CURB RAMPS	19+85/44+15	STH 113	133	0	0	133	0	0	133	133	0	
SHOULDER WIDENING	42+20/44+66	STH 113 & KOPP ROAD	35	0	0	35	0	0	35	35	0	
CULVERT REPLACEMENT	210+21/210+62	STH 113	182	0	0	182	0	0	182	182	0	
UNDISTRIBUTED EBS		STH 113	0	2,312	0	0	0	0	0	0	0	
GRAND TOTAL			3,586	2,312	165	3,421	116	166	3,255	3,255	0	
TOTAL COMMON EXC			5,898									

NOTES:
(1) COMMON EXCAVATION IS THE SUM OF THE CUT AND EBS EXCAVATION COLUMNS. ITEM NUMBER 205.0100
(2) SALVAGED/UNSUABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.
(3) EBS EXCAVATION TO BE BACKFILLED WITH 16" OF SELECT CRUSHED MATERIAL AND 14" BASE AGGREGATE DENSE MATERIAL 1 1/4-INCH.
(4) SALVAGED/UNUSABLE PAVEMENT MATERIAL
(5) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSUABLE PAVEMENT MATERIAL
(9) REDUCED EBS IN FILL - EXCAVATED EBS MATERIAL IS NOT USUABLE IN FILLS OUTSIDE THE 1:1 SLOPE.
(11) EXPANDED EBS BACKFILL - THIS IS TO BE FILLED WITH SELECT BORROW MATERIAL. EBS BACKFILL FACTOR = X.4X. ITEM NUMBER 208.1100
(13) EXPANDED FILL FACTOR = 1.15
(14) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.
(15) FACTORS USED TO COMPUTE ANTICIPATED WASTE AND THE COMPUTED WASTE VOLUME IDENTIFIED ARE FOR GENERAL INFORMATION ONLY.

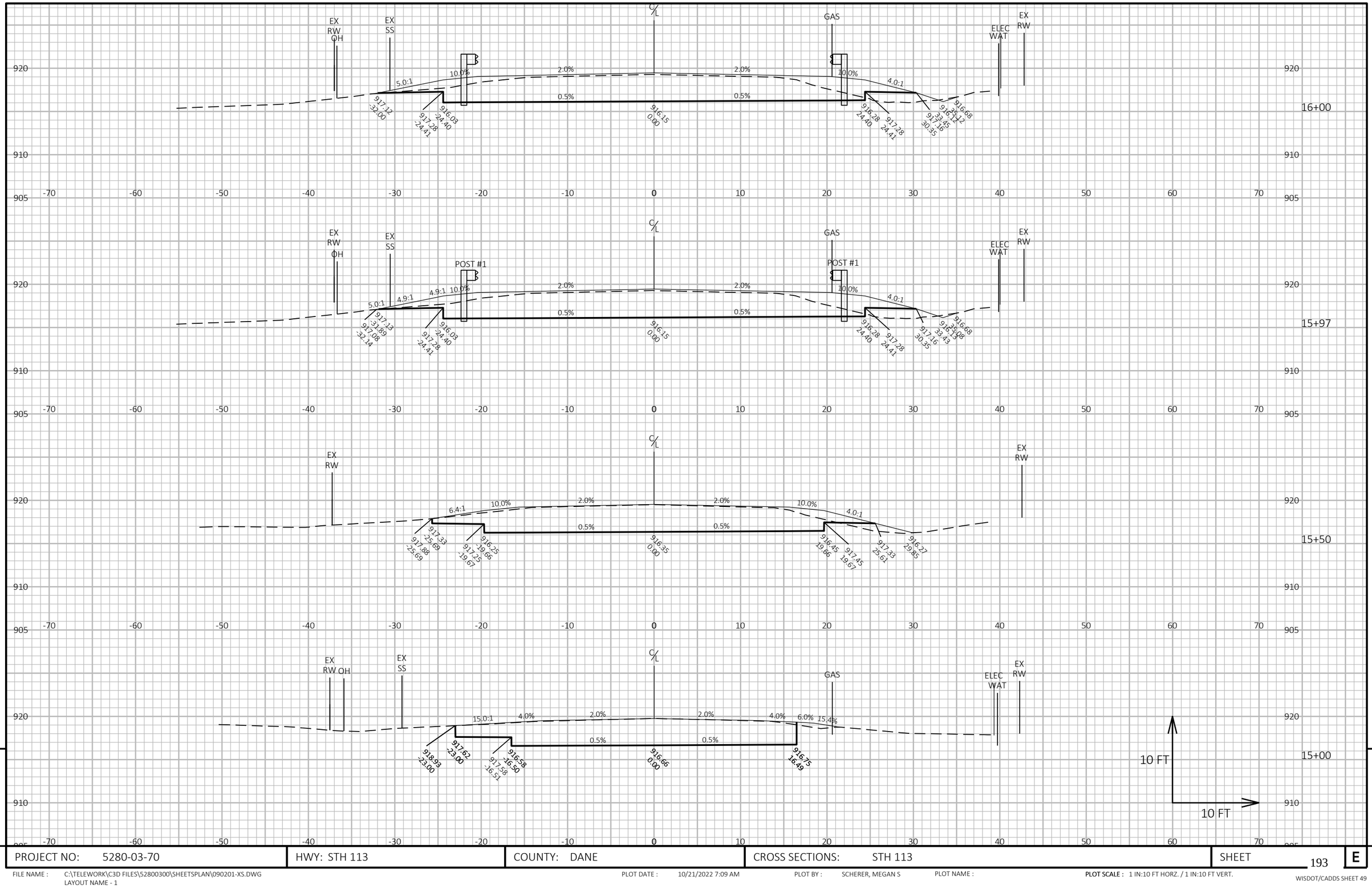
9

9

DIVISION -- STH 113 RECONSTRUCTION AREA

STATION	REAL STATION	DISTANCE	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)		
			CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT 1.43	EXPANDED FILL 1.43	MASS ORDINATE
						NOTE 1	NOTE 2	NOTE 3	NOTE 1		NOTE 8
15+00	1500.00	0.00	104.78	5.42	0.00	0	0	0	0	0	0
15+25	1525.00	25.00	109.31	5.42	0.00	99	5	0	142	0	94
15+50	1550.00	25.00	115.71	5.42	1.99	104	5	1	290	1	192
15+75	1575.00	25.00	120.94	5.42	4.63	110	5	3	448	6	292
16+00	1600.00	25.00	125.71	5.42	6.38	114	5	5	611	13	394
16+25	1625.00	25.00	129.15	5.42	0.29	118	5	3	779	17	503
16+50	1650.00	25.00	126.76	5.42	1.56	118	5	1	948	19	614
16+75	1675.00	25.00	122.20	5.42	2.62	115	5	2	1,113	21	722
17+00	1700.00	25.00	16.75	5.42	0.00	64	5	1	1,204	23	779
17+25	1725.00	25.00	18.84	5.42	0.00	16	5	0	1,227	23	790
17+50	1750.00	25.00	20.62	5.42	0.00	18	5	0	1,253	23	803
17+75	1775.00	25.00	122.90	5.42	10.08	66	5	5	1,347	30	857
18+00	1800.00	25.00	124.42	5.42	11.85	115	5	10	1,512	44	953
18+25	1825.00	25.00	123.41	5.42	10.24	115	5	10	1,676	59	1,048
18+50	1850.00	25.00	118.09	5.42	19.53	112	5	14	1,836	79	1,135
18+75	1875.00	25.00	106.11	5.42	22.70	104	5	20	1,985	107	1,206
19+00	1900.00	25.00	103.15	5.42	6.69	97	5	14	2,124	127	1,278
19+25	1925.00	25.00	102.69	5.42	0.00	95	5	3	2,259	132	1,363
19+50	1950.00	25.00	102.26	5.42	0.00	95	5	0	2,395	132	1,453
19+75	1975.00	25.00	100.50	5.42	0.00	94	5	0	2,530	132	1,542
20+00	2000.00	25.00	112.33	5.42	0.76	99	5	0	2,671	132	1,636
20+25	2025.00	25.00	115.01	5.42	1.98	105	5	1	2,821	133	1,735
20+50	2050.00	25.00	113.21	5.42	3.37	106	5	2	2,973	136	1,833
20+75	2075.00	25.00	126.75	5.42	5.30	111	5	4	3,132	142	1,933
21+00	2100.00	25.00	150.80	5.42	2.22	128	5	3	3,315	146	2,052
21+25	2125.00	25.00	152.35	5.42	0.00	140	5	1	3,515	147	2,186
21+50	2150.00	25.00	137.17	5.42	0.80	134	5	0	3,707	147	2,315
21+75	2175.00	25.00	123.54	5.42	1.30	121	5	1	3,880	149	2,429
22+00	2200.00	25.00	124.67	5.42	8.39	115	5	4	4,044	154	2,534
22+25	2225.00	25.00	131.74	5.42	1.94	119	5	5	4,214	162	2,640
22+50	2250.00	25.00	30.11	5.42	0.97	75	5	1	4,321	163	2,709
22+75	2275.00	25.00	29.68	5.42	0.83	28	5	1	4,362	164	2,731
23+00	2300.00	25.00	28.43	5.42	0.49	27	5	1	4,400	166	2,751
23+25	2325.00	25.00	24.45	5.42	0.40	24	5	0	4,434	166	2,770

Notes:		
1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL	
2 - SALVAGED/UNUSABLE PAVEMENT MATERIAL	THIS DOES NOT SHOW UP IN CROSS SECTIONS	
3 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME	
4 - EXPANDED MARSH BACKFILL	WILL BE BACKFILLED WITH GRANULAR BACKFILL (OR CUT, OR BORROW)	NOTE 4 - SELECT ONE BASED ON INPUT DIALOG SELECTION
5 - EXPANDED EBS	WILL BE BACKFILLED WITH GRANULAR BACKFILL (OR CUT, OR BORROW)	NOTE 5 - SELECT ONE BASED ON INPUT DIALOG SELECTION
6 - REDUCED MARSH IN FILL	REDUCED MARSH EXCAVATION THAT CAN BE USED IN FILL	NOTE 6 - IF EXCAVATED MARSH CAN BE USED IN FILL
7 - REDUCED EBS IN FILL	REDUCED EBS EXCAVATION THAT CAN BE USED IN FILL	NOTE 7 - IF EXCAVATED EBS CAN BE USED IN FILL



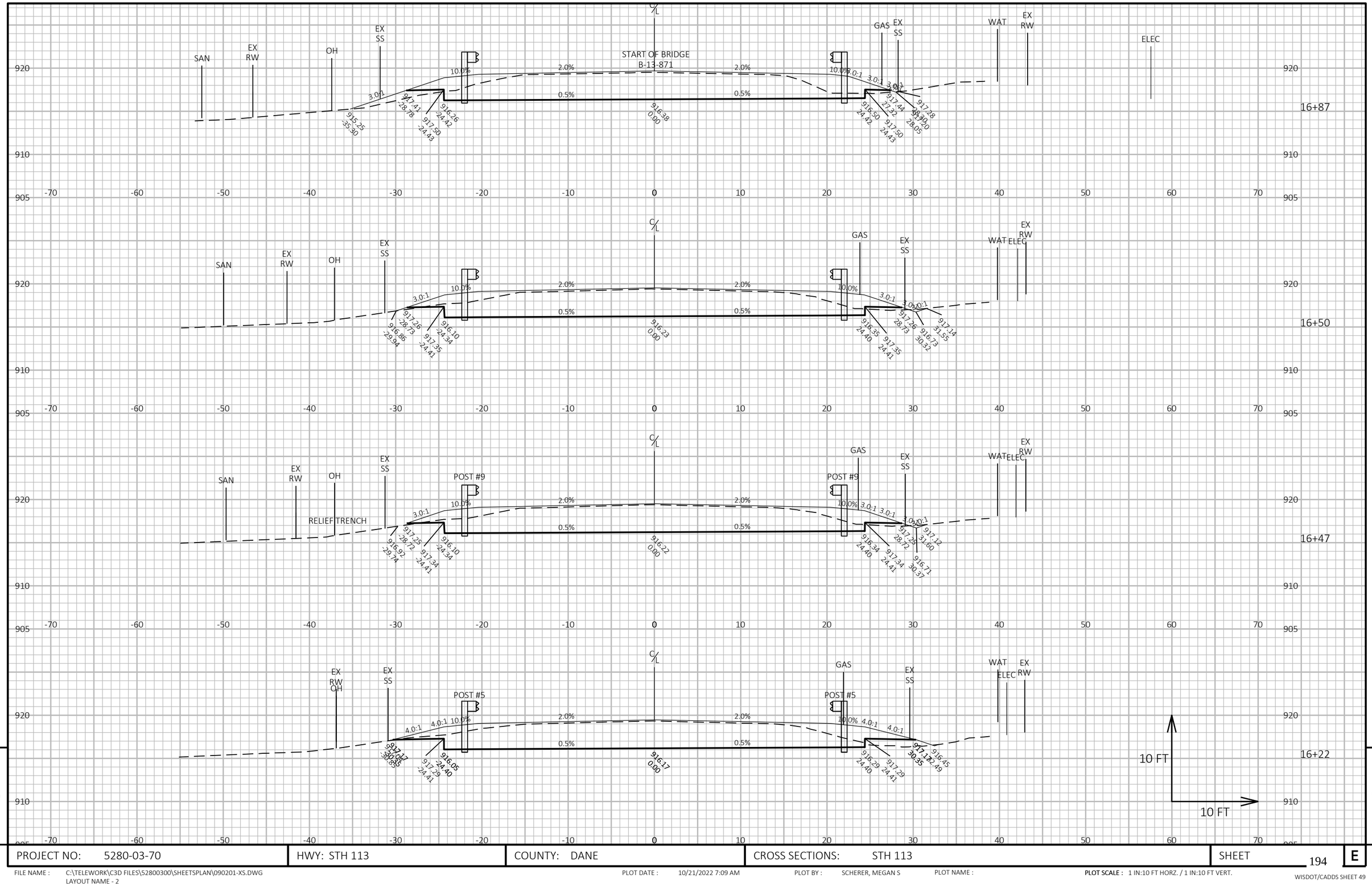
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	CROSS SECTIONS: STH 113	SHEET 193	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 1

PLOT DATE : 10/21/2022 7:09 AM PLOT BY : SCHERER, MEGAN S PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	CROSS SECTIONS: STH 113	SHEET 194	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 2

PLOT DATE : 10/21/2022 7:09 AM

PLOT BY : SCHERER, MEGAN S

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

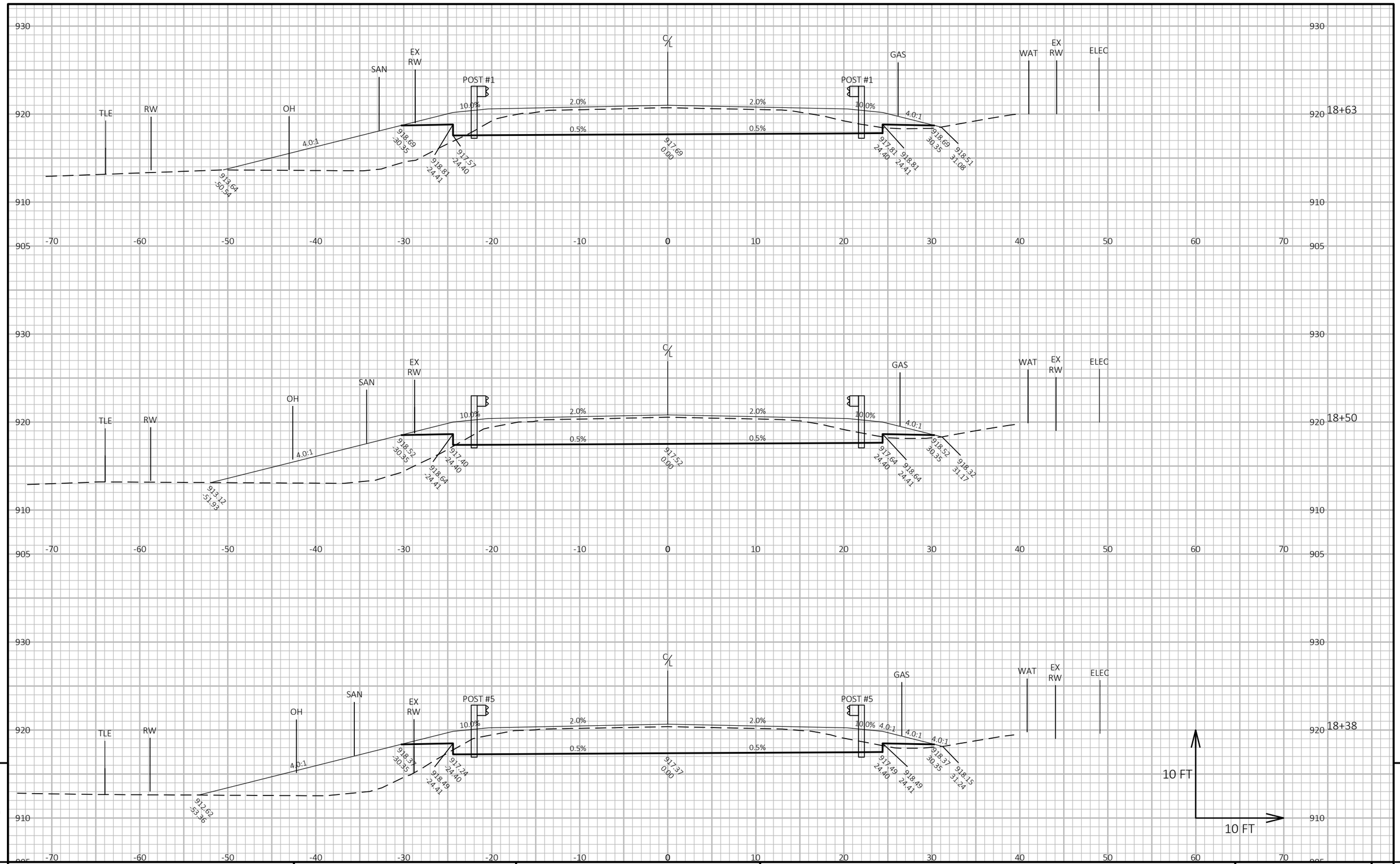
WISDOT/CADDS SHEET 49

PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: STH 113	CROSS SECTIONS: STH 113	SHEET 195	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 3

PLOT DATE : 10/21/2022 7:09 AM PLOT BY : SCHERER, MEGAN S PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



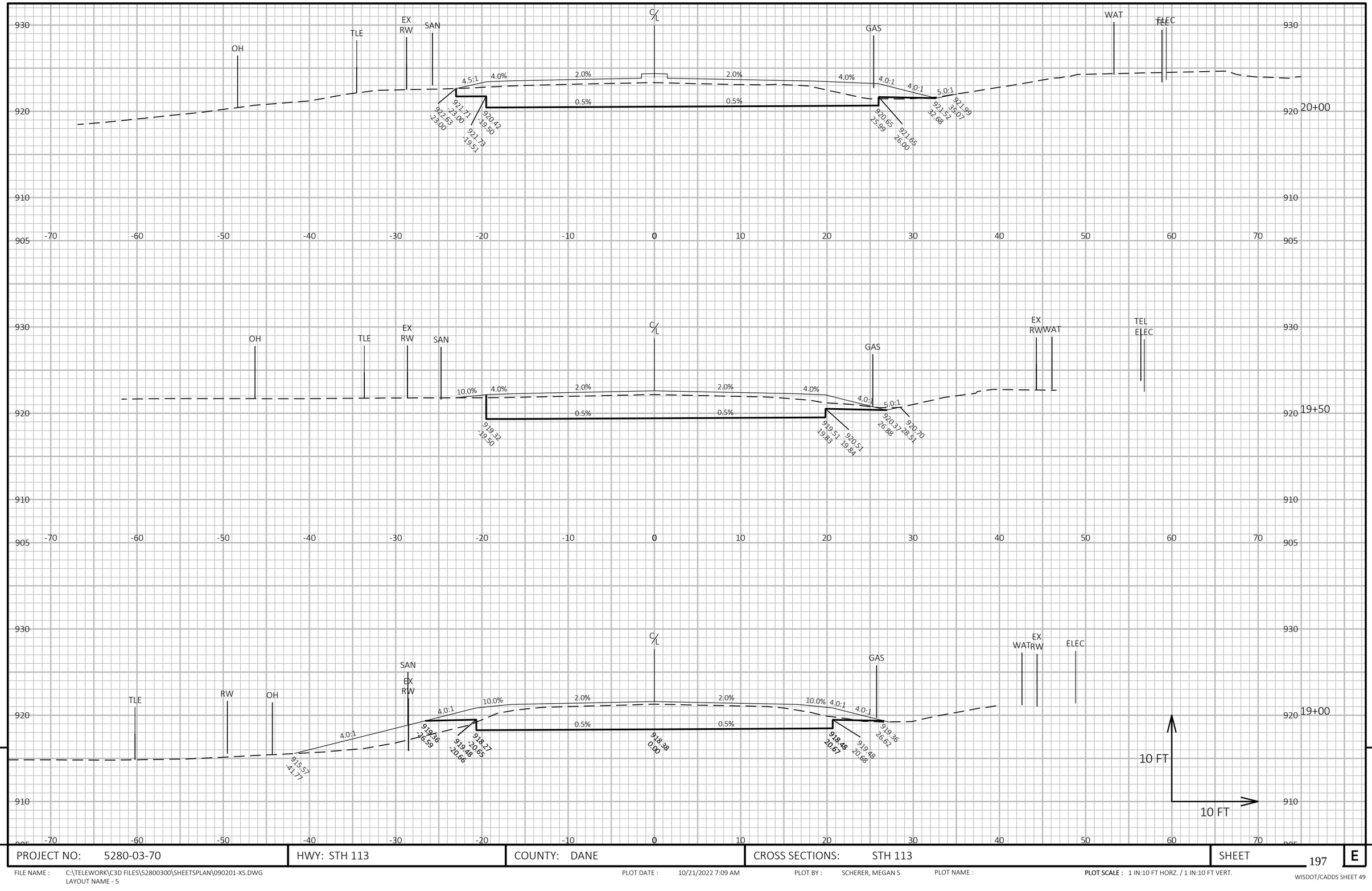
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	CROSS SECTIONS: STH 113	SHEET 196	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 4

PLOT DATE : 10/21/2022 7:09 AM PLOT BY : SCHERER, MEGAN S PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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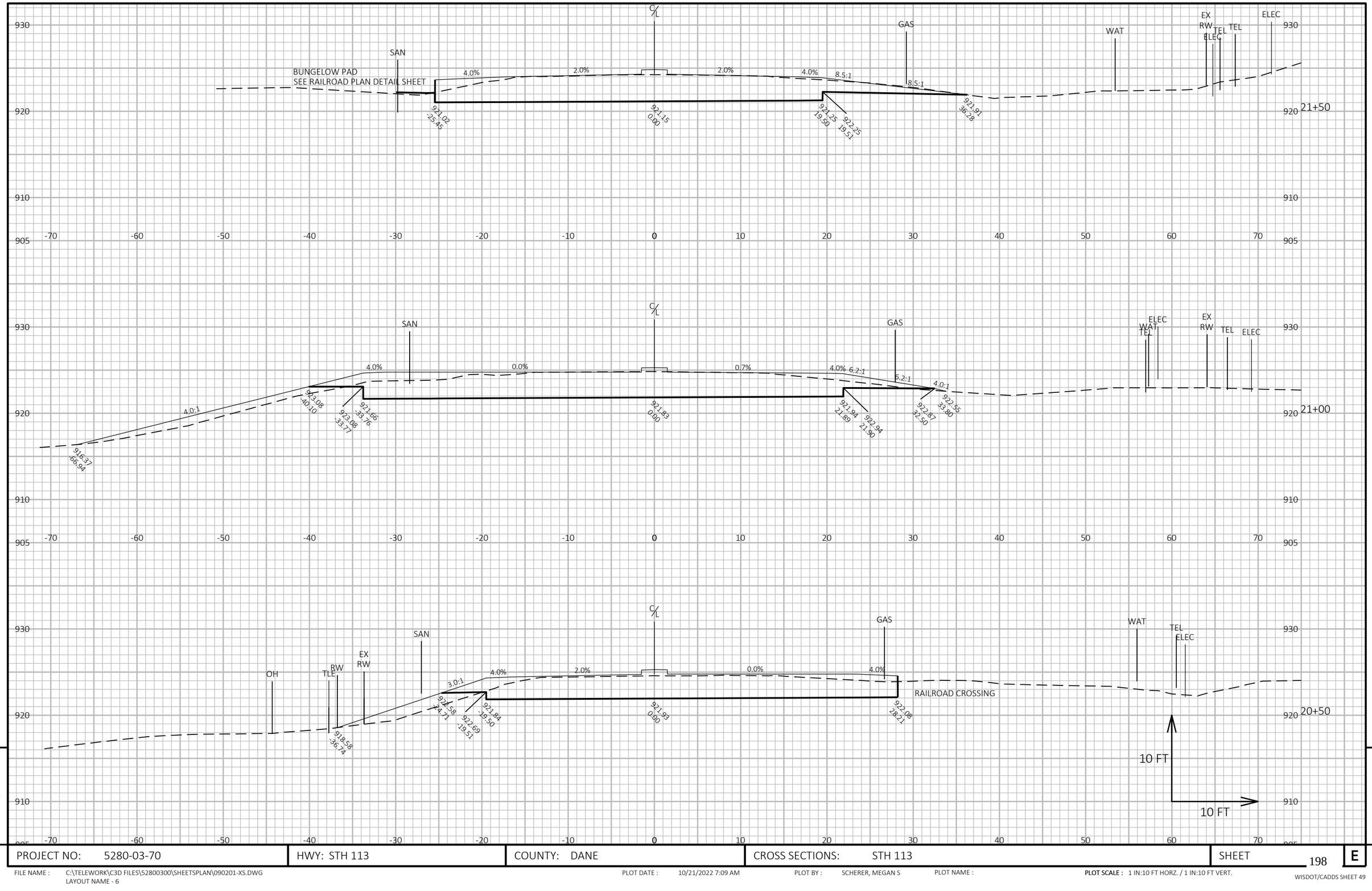
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	CROSS SECTIONS: STH 113	SHEET 197	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 5

PLOT DATE : 10/21/2022 7:09 AM PLOT BY : SCHERER, MEGAN S PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



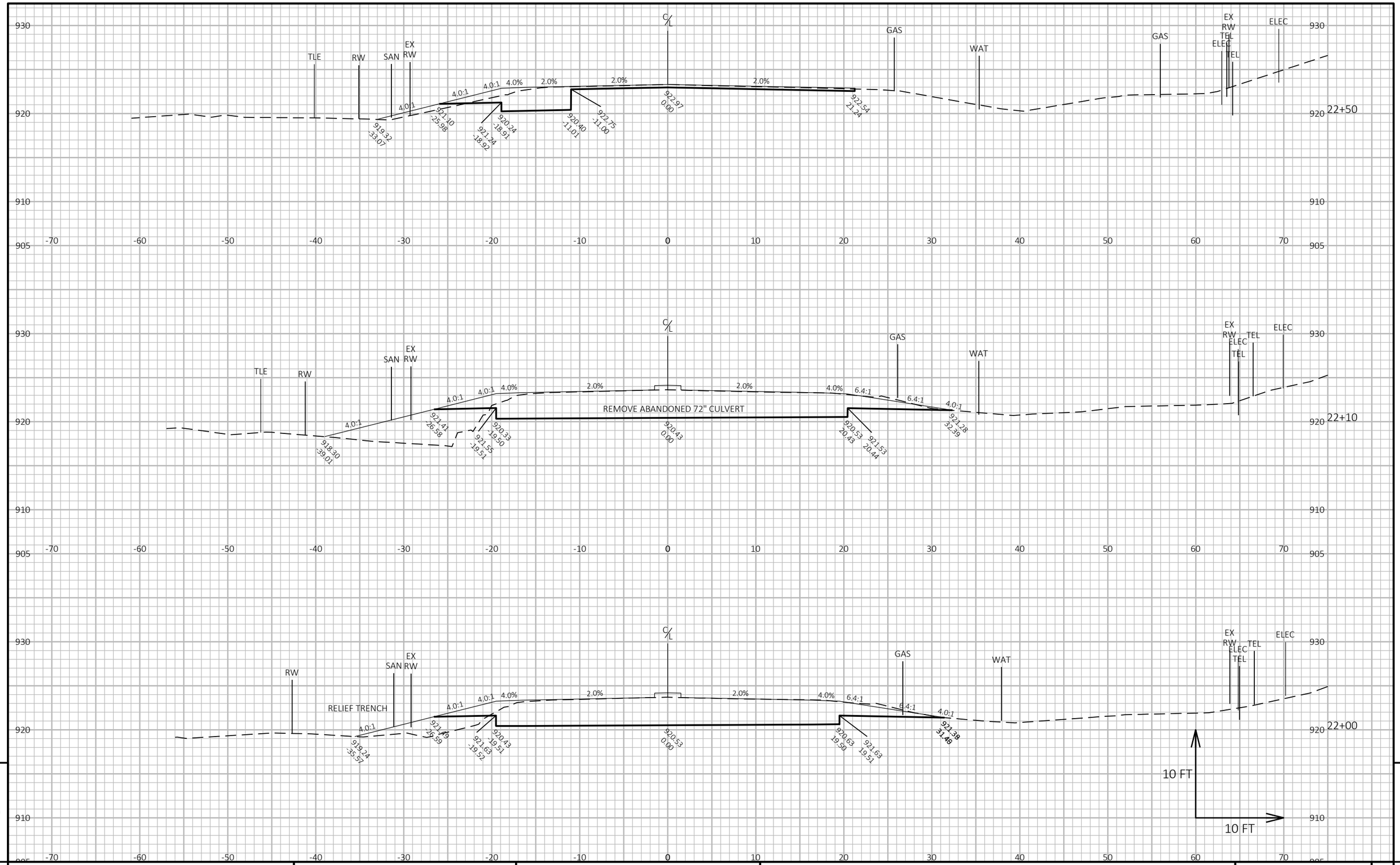
PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	CROSS SECTIONS: STH 113	SHEET 198	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 6

PLOT DATE : 10/21/2022 7:09 AM PLOT BY : SCHERER, MEGAN S PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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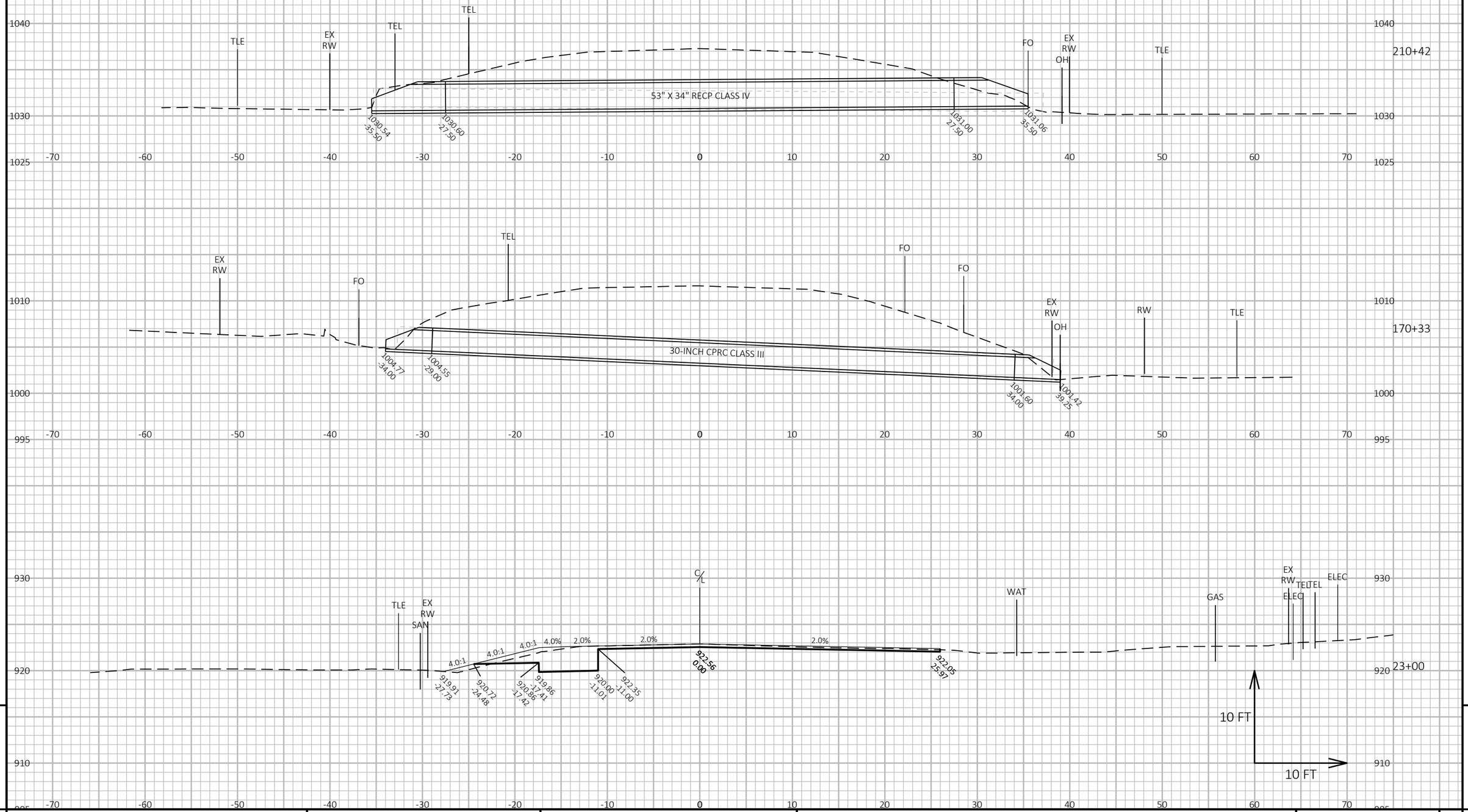
FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 7

PLOT DATE : 10/21/2022 7:09 AM PLOT BY : SCHERER, MEGAN S PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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PROJECT NO: 5280-03-70	HWY: STH 113	COUNTY: DANE	CROSS SECTIONS: STH 113	SHEET 200	E

FILE NAME : C:\TELEWORK\C3D FILES\52800300\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 8

PLOT DATE : 10/21/2022 7:09 AM PLOT BY : SCHERER, MEGAN S PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49

Notes

Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

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