RHI APRIL 2023

PROJECT ID: 1595-09-73

WITH:

COUNTY: ONEIDA

ORDER OF SHEETS

TOTAL SHEETS = 128

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

BRADLEY - RHINELANDER

NORTH RIFLE ROAD TO STH 47

USH 8

ONEIDA COUNTY

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1595-09-73	WISC 2023290	1

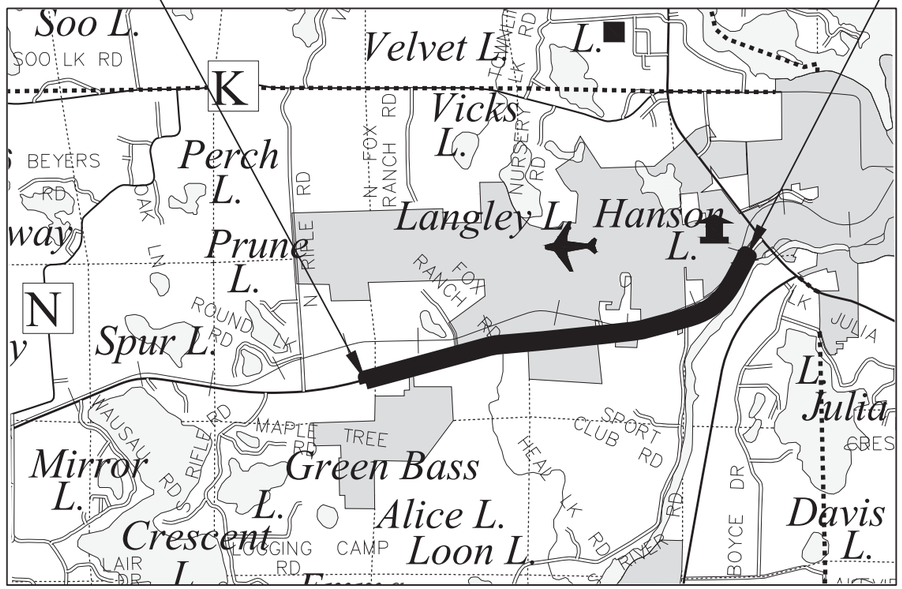
STATE PROJECT NUMBER

1595-09-73

N

BEGIN PROJECT 1595-09-73
STA 732+00
X = 45,617.320
Y = 89,493.251

END PROJECT 1595-09-73
STA 883+36.92 EB



CONVENTIONAL SYMBOLS

PLAN		PROFILE	
CORPORATE LIMITS		GRADE LINE	
PROPERTY LINE		ORIGINAL GROUND	
LOT LINE		MARSH OR ROCK PROFILE (To be noted as such)	ROCK
LIMITED HIGHWAY EASEMENT		SPECIAL DITCH	LABEL
EXISTING RIGHT OF WAY		GRADE ELEVATION	95.36
PROPOSED OR NEW R/W LINE		CULVERT (Profile View)	
SLOPE INTERCEPT		**UTILITIES**	
REFERENCE LINE	300'EB'	ELECTRIC	E
EXISTING CULVERT		FIBER OPTIC	FO
PROPOSED CULVERT (Box or Pipe)		GAS	G
COMBUSTIBLE FLUIDS	CAUTION	SANITARY SEWER	SAN
		STORM SEWER	SS
		TELEPHONE	T
MARSH AREA		WATER	W
		UTILITY PEDESTAL	
		POWER POLE	
WOODED OR SHRUB AREA		TELEPHONE POLE	

DESIGN DESIGNATION		North Rifle Rd To Airport Rd	Airport Rd To STH 47
A.A.D.T.	2024	= 8,090	= 12,660
A.A.D.T.	2044	= 8,670	= 13,550
D.H.V.		= 841	= 1,314
D.D.		= 59/41	= 59/41
T.		= 11.3	= 11.3
DESIGN SPEED		= 60MPH	= 50MPH
ESALS		= 210	= 210

LAYOUT

SCALE 0 1 MI

TOTAL NET LENGTH OF CENTERLINE = 2.83 Miles

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), ONEIDA COUNTY, NAD83 (1991), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (1991). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	SURVEYOR
Designer	TODD DELORIA
Project Manager	NICHOLAS VOS
Regional Examiner	ZACH GRULING
Regional Supervisor	DAN ERVA

APPROVED FOR THE DEPARTMENT

DATE: 1/6/2022 (Signature)

E

GENERAL NOTES:

- WHEN THE QUANTITY OF THE ITEMS BASE AGGREGATE OR HMA PAVEMENT IS MEASURED FOR PAYMENT BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE LAYERS SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

- THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

- CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING, SEEDING, AND EMATTING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY HIS OPERATION OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.

RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP- TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE: TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = 40 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.1 ACRES

ORDER OF DETAIL SHEETS:

DIGGERS HOTLINE
Dial 811 or (800)242-8511
www.DiggersHotline.com

UTILITY CONTACTS:

FRONTIER COMMUNICATION OF WI LLC
COMMUNICATION LINE
JEREMY ZAHM
154 E 2ND ST
NEW RICHMOND, WI 54017
JEREMY.ZEHM@FTR.COM
715-243-9243

SPECTRUM COMMUNICATIONS
COMMUNICATION LINE
JOSEPH NESSMAN
821 LINCOLN ST
RHINELANDER, WI 54501
JOSEPH.NESSMAN@CHARTER.COM
715-519-0039

RHINELANDER WATER DEPT
WATER
TOM ROESER
135 S. STEVENS STREET
TROESER@RHINELANDERUTILITIES.ORG
715-365-8624

RHINELANDER SEWER DEPT
SEWER
TOM ROESER
135 S. STEVENS STREET
TROESER@RHINELANDERUTILITIES.ORG
715-365-8624

WISCONSIN PUBLIC SERVICE CORPORATION
GAS
SHANE SARKKINEN
1700 SHERMAN ST.
WAUSAU, WI 54402
SHANE.SARKKINEN@WISCONSINPUBLICSERVICE.COM
715-369-7133

WISCONSIN PUBLIC SERVICE CORPORATION
ELECTRICITY
KEVIN TERMAAT
PO BOX 1166
WAUSAU, WI 54401
715-848-7353
KEVIN.TERMAAT@WISCONSINPUBLICSERVICE.COM

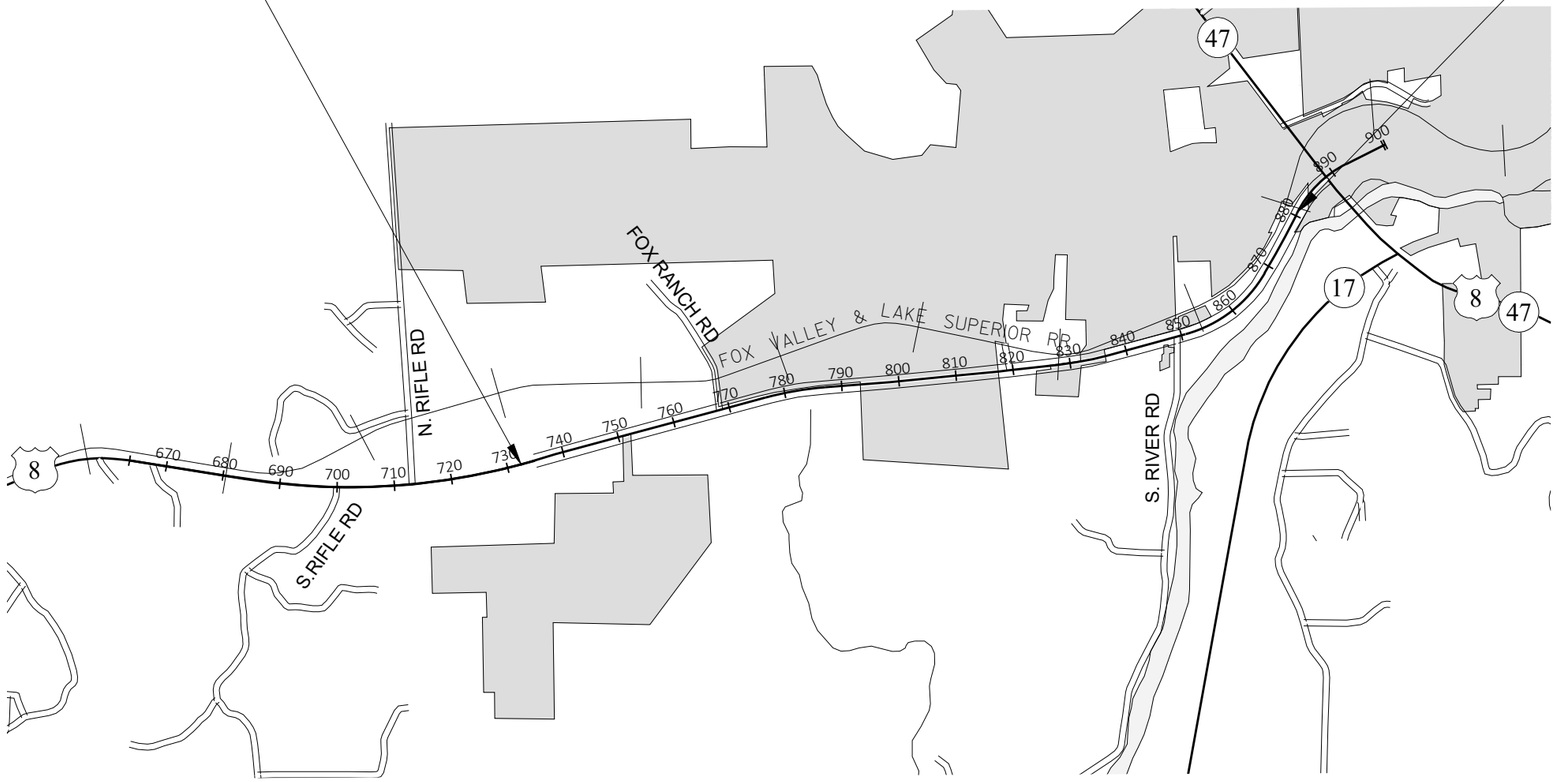
DNR CONTACT:

WENDY HENNIGES
107 SUTLIFF AVENUE
RHINELANDER, WI 54501
WENDY.HENNIGES@WISCONSIN.GOV
715-365-8916

2

BEGIN PROJECT 1595-09-73
STA 732+58.00

END PROJECT 1595-09-73
STA 883+36.92 EB

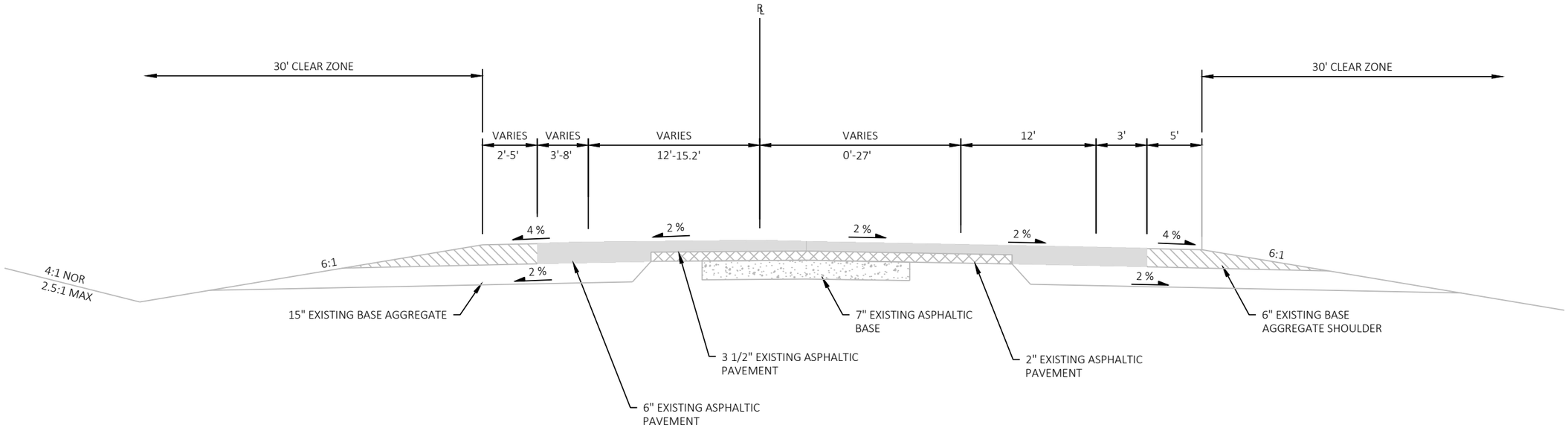


PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	PROJECT OVERVIEW	SHEET 3	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\020201-PO.DWG
LAYOUT NAME - 01

PLOT DATE : 10/3/2022 3:36 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:0.5 MI

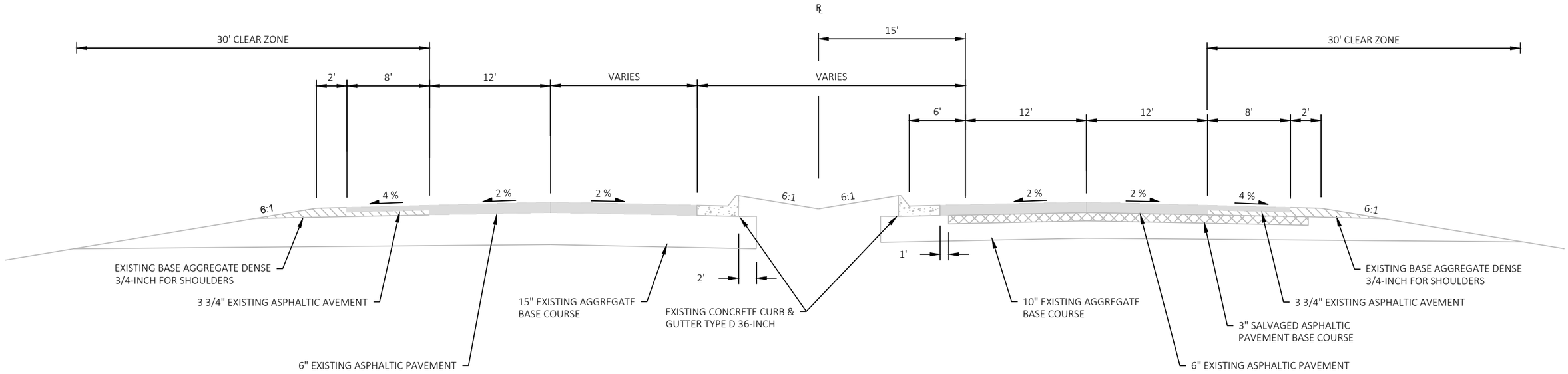
2



EXISTING TYPICAL SECTION

STA 732+00 - STA 738+62.30

2

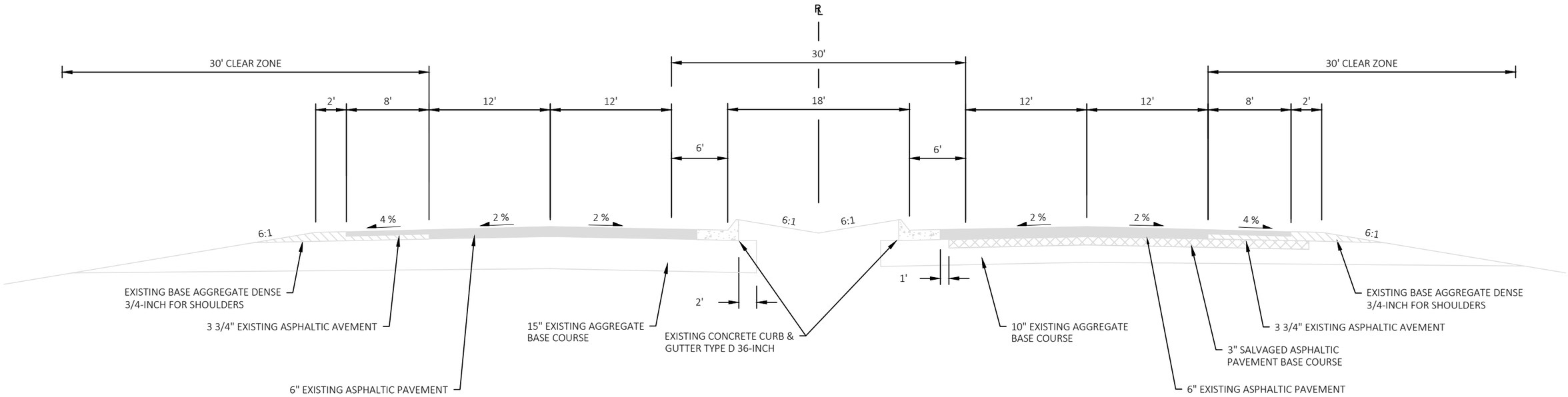


EXISTING TYPICAL SECTION

STA 738+62.30 - STA 745+86.65

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TYPICAL SECTION	SHEET 5 E

2

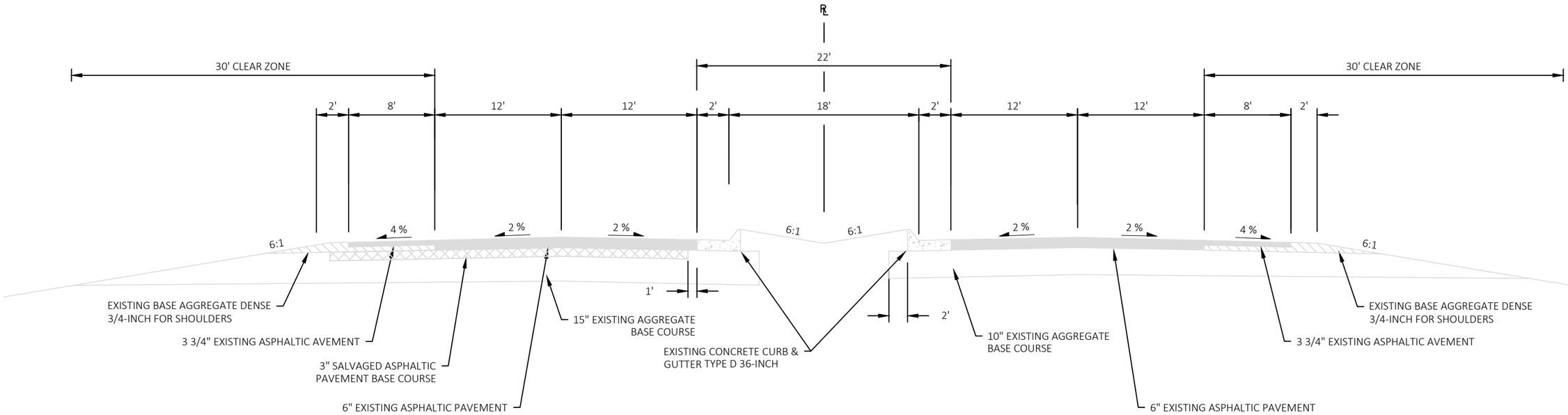


EXISTING TYPICAL SECTION

STA 745+86.65 - STA 823+00.00

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TYPICAL SECTION	SHEET 6	E

2

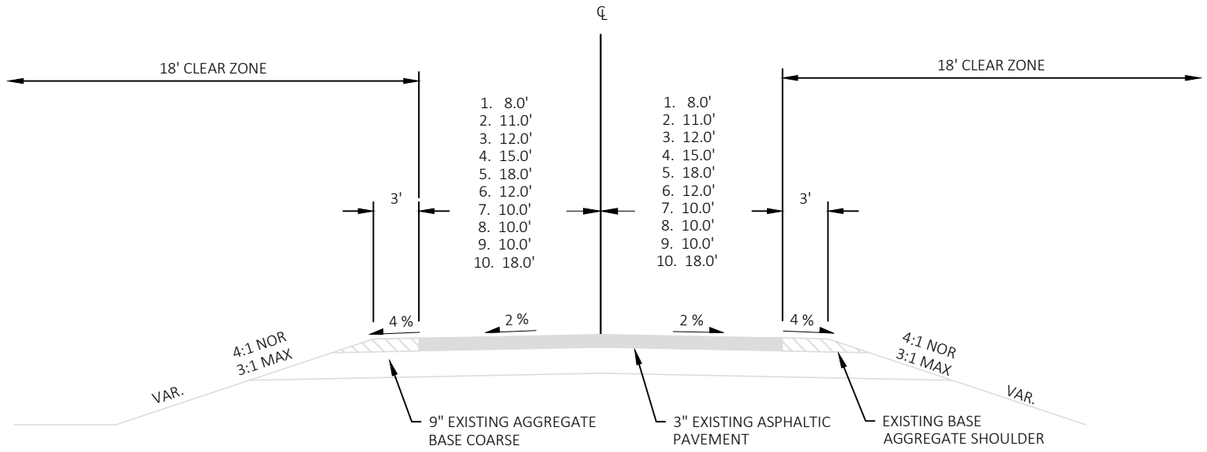


EXISTING TYPICAL SECTION

STA 823+00.00 - STA 883+36.92

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TYPICAL SECTION	SHEET 7	E

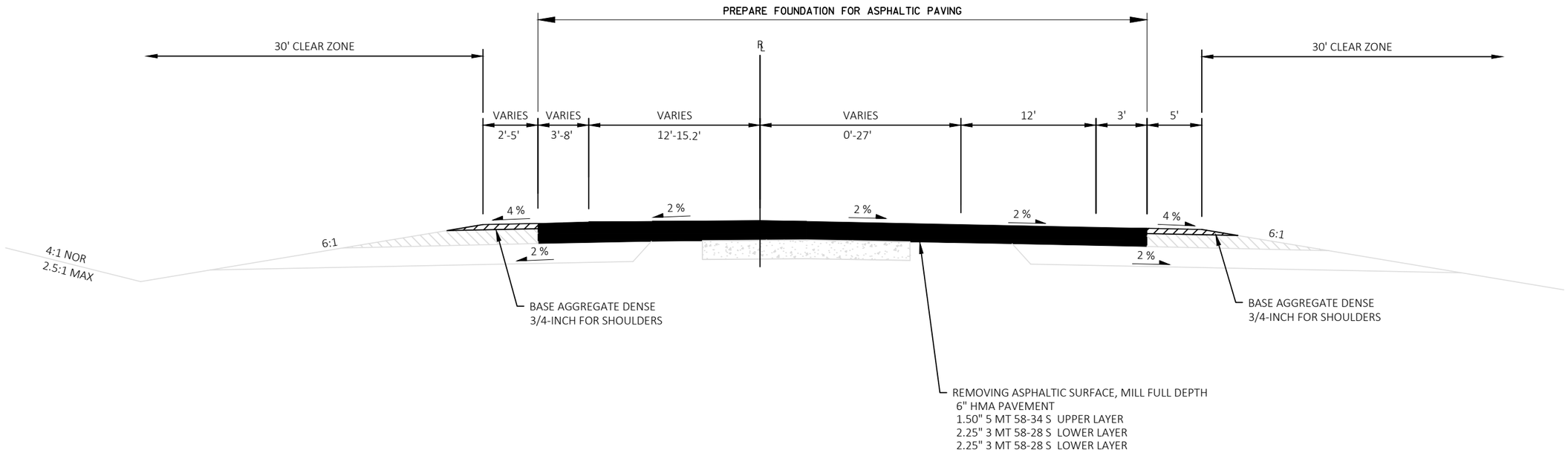
2



EXISTING TYPICAL SECTION SIDEROADS

1. CHARLES DENTON RD
2. GOLF COARSE RD
3. SOUTH FOX RANCH RD
4. RED ARROW DRIVE
5. LAKESHORE LANE
6. AIRPORT RD
7. BOYD DRIVEWAY
8. SOUTH RIVER RD
9. NORTH RIVER RD
10. UNITED RENTAL

2

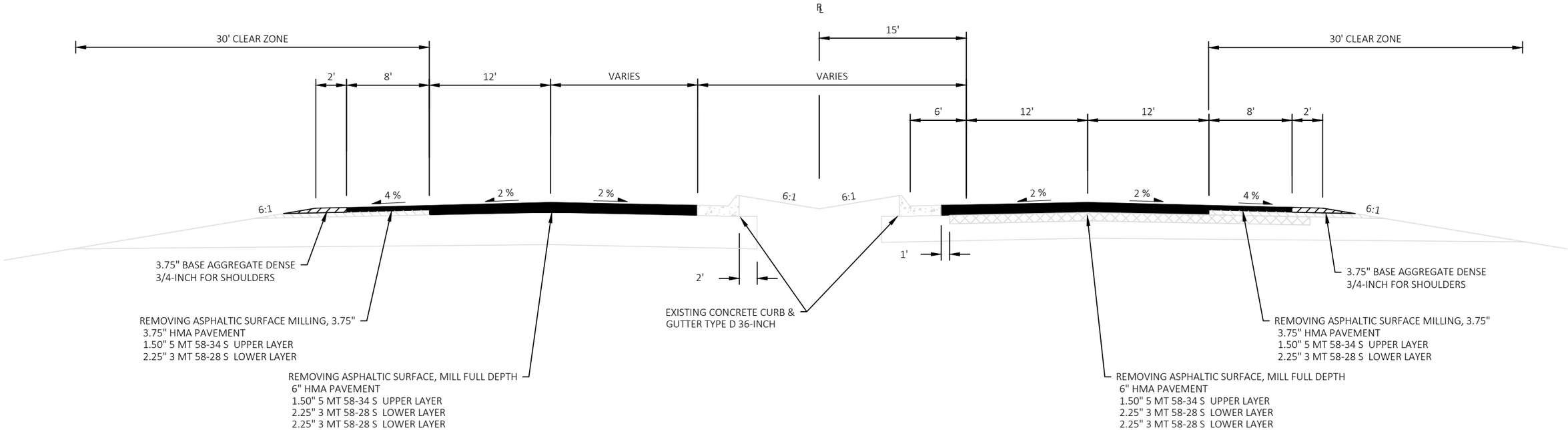


FINISHED TYPICAL SECTION

STA 732+00 - STA 738+62.30

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TYPICAL SECTION	SHEET 9	E

2

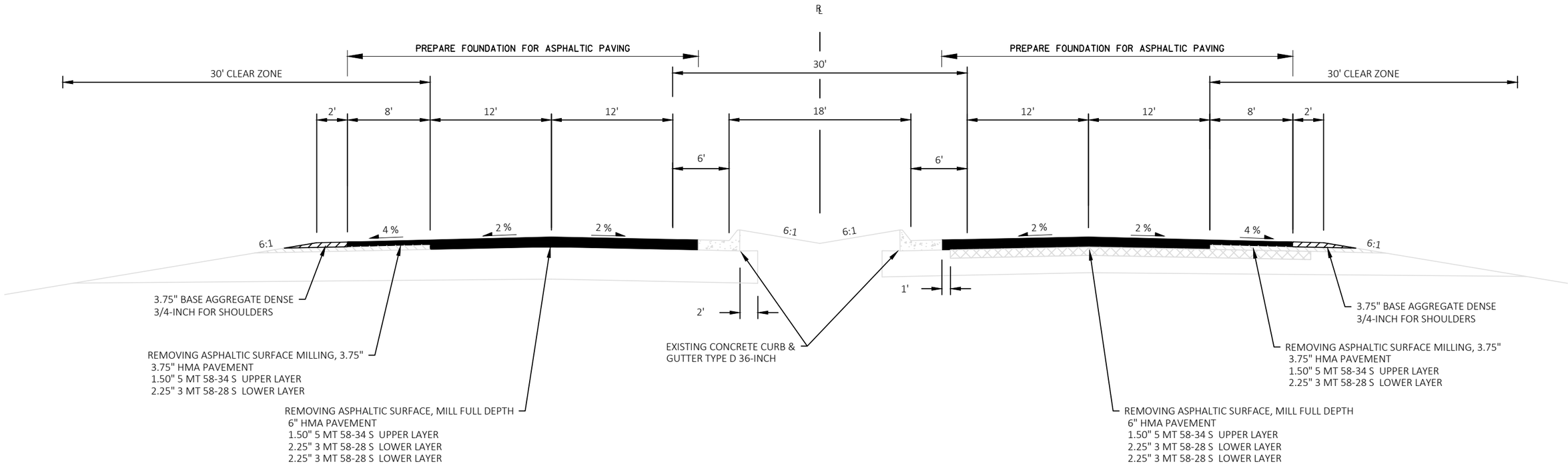


FINISHED TYPICAL SECTION

STA 738+62.30 - STA 745+86.65

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TYPICAL SECTION	SHEET 10	E

2

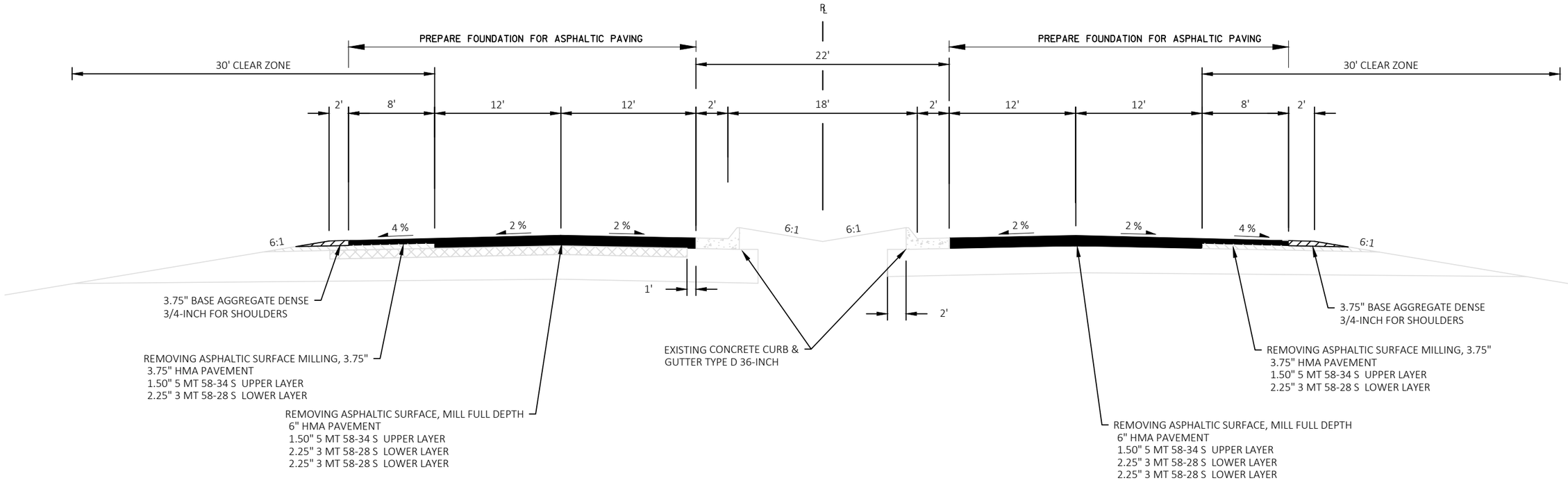


FINISHED TYPICAL SECTION

STA 745+86.65 - STA 823+00.00

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TYPICAL SECTION	SHEET 11	E

2

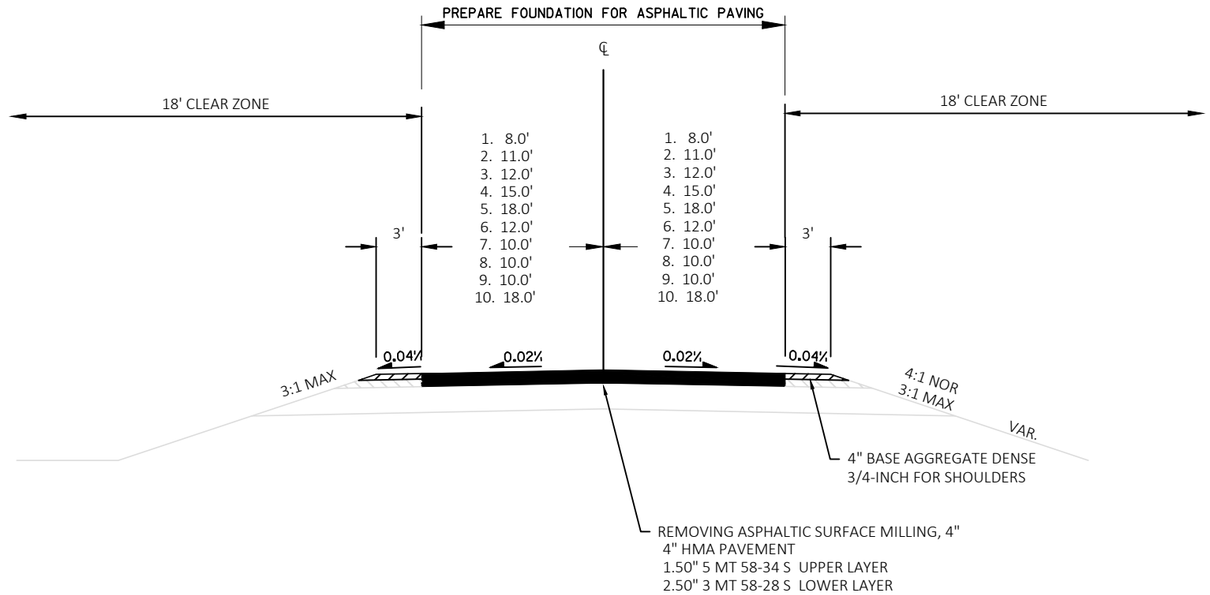


FINISHED TYPICAL SECTION

STA 823+00.00 - STA 883+36.92

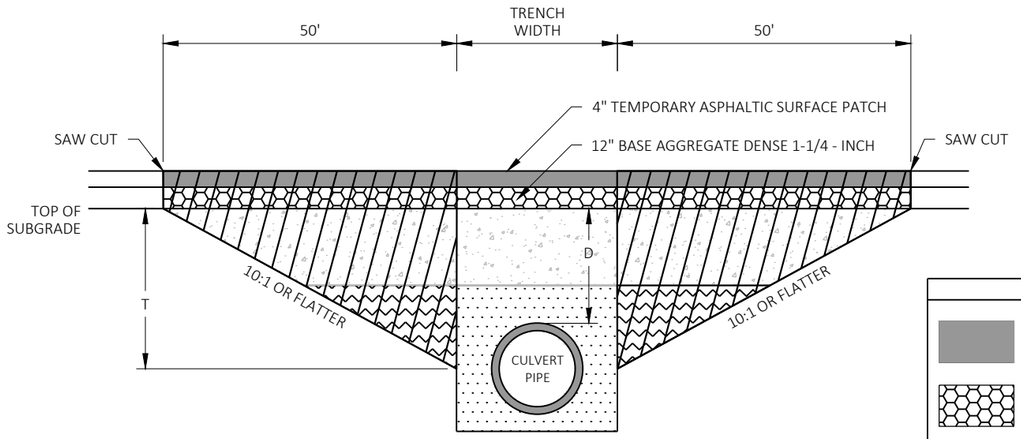
PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TYPICAL SECTION	SHEET 12	E

2



FINISHED TYPICAL SECTION SIDEROADS

1. CHARLES DENTON RD
2. GOLF COARSE RD
3. SOUTH FOX RANCH RD
4. RED ARROW DRIVE
5. LAKESHORE LANE
6. AIRPORT RD
7. BOYD DRIVEWAY
8. SOUTH RIVER RD
9. NORTH RIVER RD
10. UNITED RENTAL

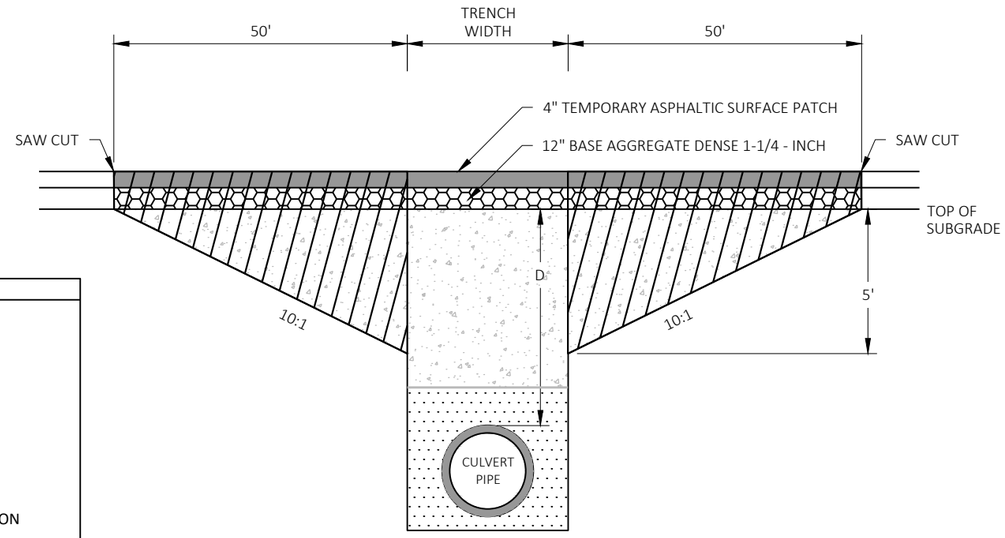


DEPTH D < 6 FT

TRANSITION CUT DEPTH (T) = THE LESSER OF DEPTH TO CENTER OF PIPE OR 5 FT.
DO NOT EXTEND TRANSITION CUT BELOW HORIZONTAL CENTER OF PIPE.

KEY

- PROPOSED SURFACE
- PROPOSED BASE
- TRENCH BACKFILL
- TRENCH OR FOUNDATION BACKFILL
- FOUNDATION BACKFILL
- TRANSITION CUT



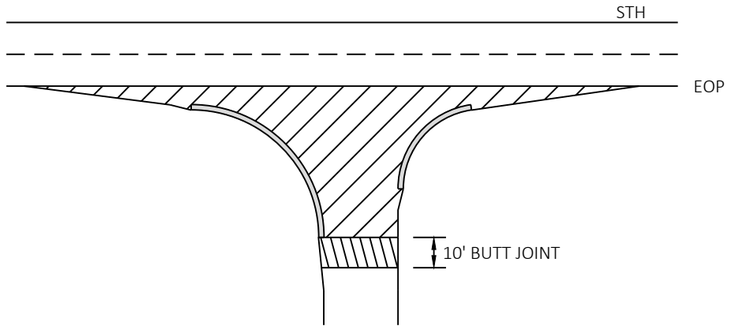
DEPTH D ≥ 6 FT

NOTES

TRANSITION CUT IS PAID AS EXCAVATION COMMON.
TRANSITION CUT WIDTH IS FROM SUBGRADE SHOULDER POINT TO SUBGRADE SHOULDER POINT.
BACKFILL THE TRANSITION CUT AREAS WITH FOUNDATION AND TRENCH BACKFILL AS SPECIFIED IN STANDARD SPEC 520.
PERFORM CULVERT PIPE INSTALLATION BEFORE *MILLING AND PAVING*

CULVERT PIPE TRANSITION

ROUTE	STA (CL)	DEPTH D (FT)	PIPE DIA (IN)	REMARKS
USH 8	785+26	>6FT	36	C 43008003062
USH 8	836+90	<6FT	19 X 30	C 43008003076



REMOVING ASPHALTIC SURFACE MILLING

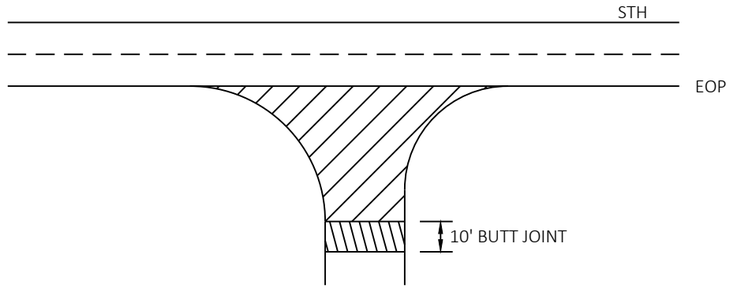
REMOVING ASPHALTIC SURFACE BUTT JOINTS
SEE BUTT JOINT DETAIL

NOTE: WHEN MATCHING TO AN UNPAVED SURFACE
BUTT JOINT IS NOT REQUIRED

SIDE ROADS

WITH CURB AND GUTTER

ROAD NAME
ROAD NAME



REMOVING ASPHALTIC SURFACE MILLING

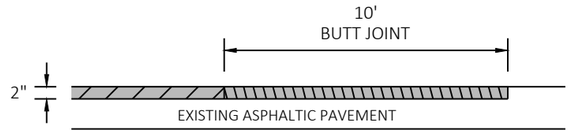
REMOVING ASPHALTIC SURFACE BUTT JOINTS
SEE BUTT JOINT DETAIL

NOTE: WHEN MATCHING TO AN UNPAVED SURFACE
BUTT JOINT IS NOT REQUIRED

SIDE ROADS

WITHOUT CURB AND GUTTER

ROAD NAME
ROAD NAME



HMA PAVEMENT

REMOVING ASPHALTIC SURFACE MILLING

REMOVING ASPHALTIC SURFACE BUTT JOINTS

BUTT JOINT

MAINLINE AND SIDE ROADS

2

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	CONSTRUCTION DETAILS	SHEET 15	E

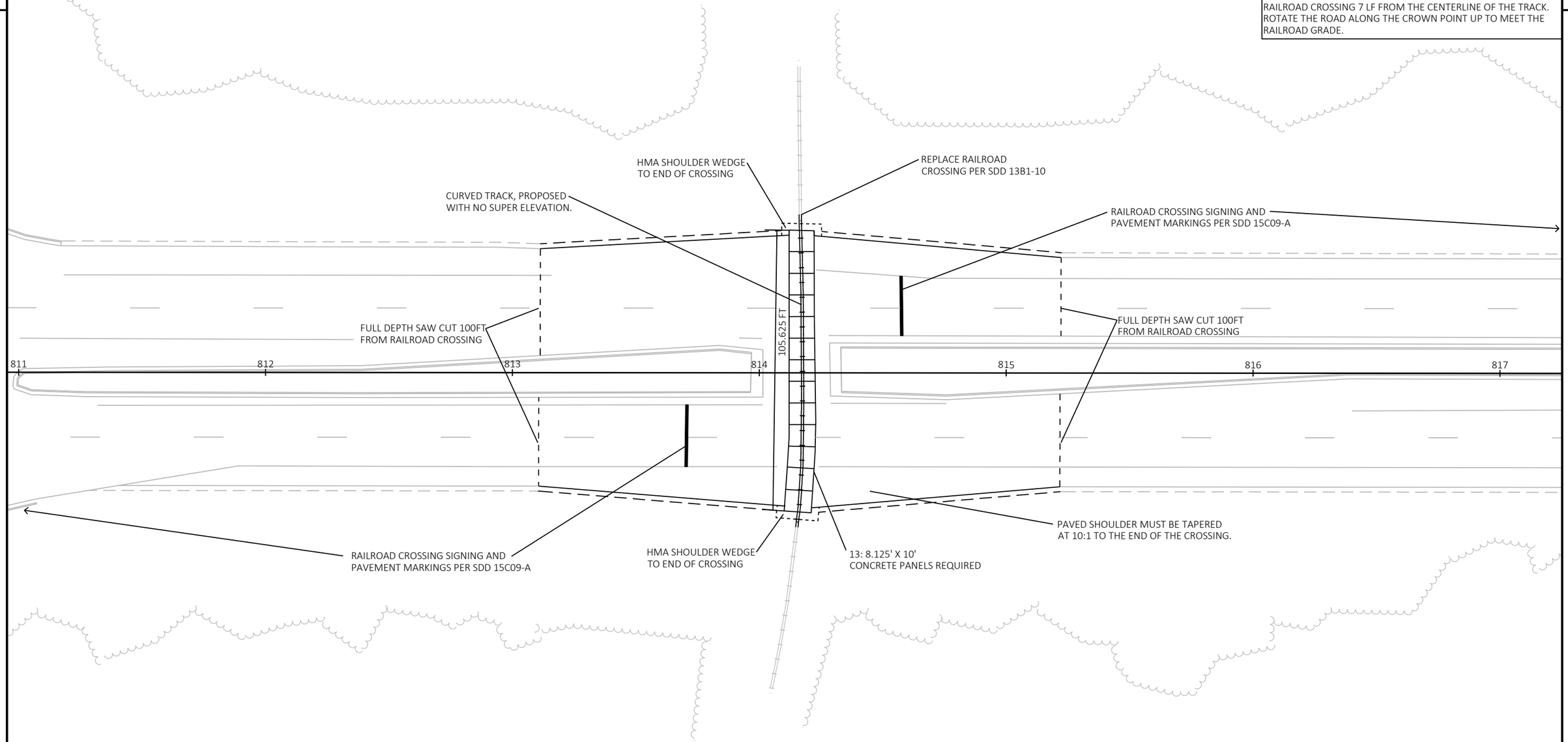
FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\021001-CD.DWG
LAYOUT NAME - JT01
PLOT DATE : 1/6/2023 11:53 AM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1:10
WISDOT/CADDS SHEET 42

2

CROSSING TO BE CONSTRUCTED BY OTHERS DURING 48 HOUR FULL ROAD CLOSURE, SEE DETOUR TRAFFIC CONTROL FOR DETAILS.

CONTRACTOR IS RESPONSIBLE FOR ALL WORK, UP TO AND INCLUDING, THE SAW CUTS ON THE EAST AND WEST SIDE OF THE RAILROAD CROSSING. CONTRACTOR IS NOT RESPONSIBLE FOR REPLACING THE RAILROAD CROSSING OR ANY PAVING BETWEEN THE SAW CUTS.

TRANSITION CROWN AT A RATE OF 1% IN 25 LF TO MATCH INTO RAILROAD CROSSING 7 LF FROM THE CENTERLINE OF THE TRACK. ROTATE THE ROAD ALONG THE CROWN POINT UP TO MEET THE RAILROAD GRADE.



PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	RAILROAD CROSSING LAYOUT DETAILS	SHEET 16	E

2

LEGEND

EROSION MAT
SILT FENCE
SLOPE INTERCEPT
INLET PROTECTION
CULVERT PIPE CHECK

N

EXISTING R/W

PIPE CHECK

BEGIN CURB
AND GUTTER
STA 784+65

END CURB
AND GUTTER
STA 786+93

783+00
784+00
785+00
786+00
787+00
788+00

BEGIN CURB
AND GUTTER
STA 784+47

END CURB
AND GUTTER
STA 787+78

2

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	EROSION CONTROL	SHEET 17	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\022001-EC.DWG
LAYOUT NAME - 022001-1

PLOT DATE : 1/9/2023 3:07 AM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:40 FT

2

LEGEND

EROSION MAT
SILT FENCE
SLOPE INTERCEPT
INLET PROTECTION
CULVERT PIPE CHECK

N

836+00
837+00
838+00

POST
GUY

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	EROSION CONTROL	SHEET 18	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\022001-EC.DWG
LAYOUT NAME - 022001-2

PLOT DATE : 1/9/2023 3:13 AM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:20 FT

WISDOT/CADDS SHEET 42

2

847+00

848+00

849+00

N

2

1+00

LEGEND

EROSION MAT

SILT FENCE

SLOPE INTERCEPT

INLET PROTECTION

CULVERT PIPE CHECK

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	EROSION CONTROL	SHEET 19	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\022001-EC.DWG
LAYOUT NAME - 022001-3

PLOT DATE : 1/9/2023 3:14 AM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : Custom

WISDOT/CADDS SHEET 42

2

2

N

BEGIN CURB
AND GUTTER
STA 879+44

END CURB
AND GUTTER
STA 882+61

878+00

879+00

880+00

881+00

882+00

883+00

L=564.727, R=1910.080

884+00

END CURB
AND GUTTER
STA 882+61

BEGIN CURB
AND GUTTER
STA 879+44

LEGEND

EROSION MAT

SILT FENCE

SLOPE INTERCEPT

INLET PROTECTION

CULVERT PIPE CHECK

EXISTING R/W

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	EROSION CONTROL	SHEET 20	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\022001-EC.DWG
LAYOUT NAME - 022001-4

PLOT DATE : 1/9/2023 3:16 AM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 42

2

2

N

RHINELANDER SEWER DEPARTMENT

SAN

FRONTIER COMMUNICATIONS

T

SPECTRUM COMMUNICATIONS

FO

OH

WISCONSIN PUBLIC SERVICE ELECTRICITY

INVERT ELEVATION
1554.43 FT

APRON ENDWALL
RCPSS 36-IN

EXCAVATION LIMITS

USH 8 WB

USH 8 EB

784+00

785+00

786+00

787+00

APRON ENDWALL
RCPSS 36-IN

STA 785+26
PIPE 43008003062
REMOVE EXISTING 36-IN, 225 LF RCPSS

INVERT ELEVATION
1551.58 FT

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	CULVERT	SHEET 21	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\022503-SS.DWG
LAYOUT NAME - 022503-1

PLOT DATE : 10/31/2022 10:50 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : Custom

WISDOT/CADDS SHEET 42

2

2

N

837+00

INVERT ELEVATION
1598.96 FT

EXCAVATION LIMITS

USH 8 EB

WISCONSIN PUBLIC SERVICE ELECTRICITY

FRONTIER COMMUNICATIONS

STA 836+90
PIPE 43008003076
REMOVE EXISTING 19-IN X 30,
55 LF RCHEPSS

INVERT ELEVATION
1598.68 FT

APRON ENDWALL
RCHEPSS 19-IN X 30

WISCONSIN PUBLIC SERVICE GAS

FRONTIER COMMUNICATIONS

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	STORM SEWER	SHEET 22	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\022503-SS.DWG
LAYOUT NAME - 022503-2

PLOT DATE : 10/31/2022 10:51 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:10 FT

2

847+00

848+00

849+00

N

USH 8 EB

1583.0
1582.5
1582.0
1581.5
1581.0
1580.5
1580.0
1579.5
1579.0
1578.5

EXCAVATION LIMITS

1575.0
1576.0
1576.5
1577.0
1577.5
1578.0

1578.0
1577.5

1577.0
1576.5

STA 847+57
REMOVE EXISTING STRUCTURE
INSTALL 3.5' X 9' TYPE 3 STRUCTURE
BOTTOM OF STRUCTURE ELEV: 1575.32
GRADE SURROUNDING AREA TO NEW INLET
ELEVATION AT A 4:1 SLOPE AS SHOWN.

WISCONSIN PUBLIC SERVICE GAS

WISCONSIN PUBLIC SERVICE ELECTRICITY

WISCONSIN PUBLIC SERVICE GAS

FRONTIER COMMUNICATIONS

WISCONSIN PUBLIC SERVICE GAS

SOUTH RIVER ROAD

1400

2

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	STORM SEWER	SHEET 23	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\022503-SS.DWG
LAYOUT NAME - 022503-3

PLOT DATE : 10/31/2022 10:52 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:20 FT

1 DETOUR M4-8 24"X12" / WEST M3-4 24"X12" / 8 M1-4 24"X24"

2 DETOUR M4-8 24"X12" / EAST M3-2 24"X12" / 8 M1-4 24"X24"

3 WEST M3-4 24"X12" / 8 M1-4 24"X24"

4 EAST M3-2 24"X12" / 8 M1-4 24"X24"

6 ROAD CLOSED TO THRU TRAFFIC 13 MILES AHEAD — R11-3 60"X30"

7 ROAD CLOSED TO THRU TRAFFIC 1.5 MILES AHEAD — R11-3 60"X30"

MO5-1L 21"X21" A | MO6-1 21"X21" B | MO6-1 21"X21" E | MO5-2R 21"X21" I

MO5-1R 21"X21" C | MO6-1 21"X21" D | END DETOUR M4-8A 24"X18" F | MO6-2 21"X21" H

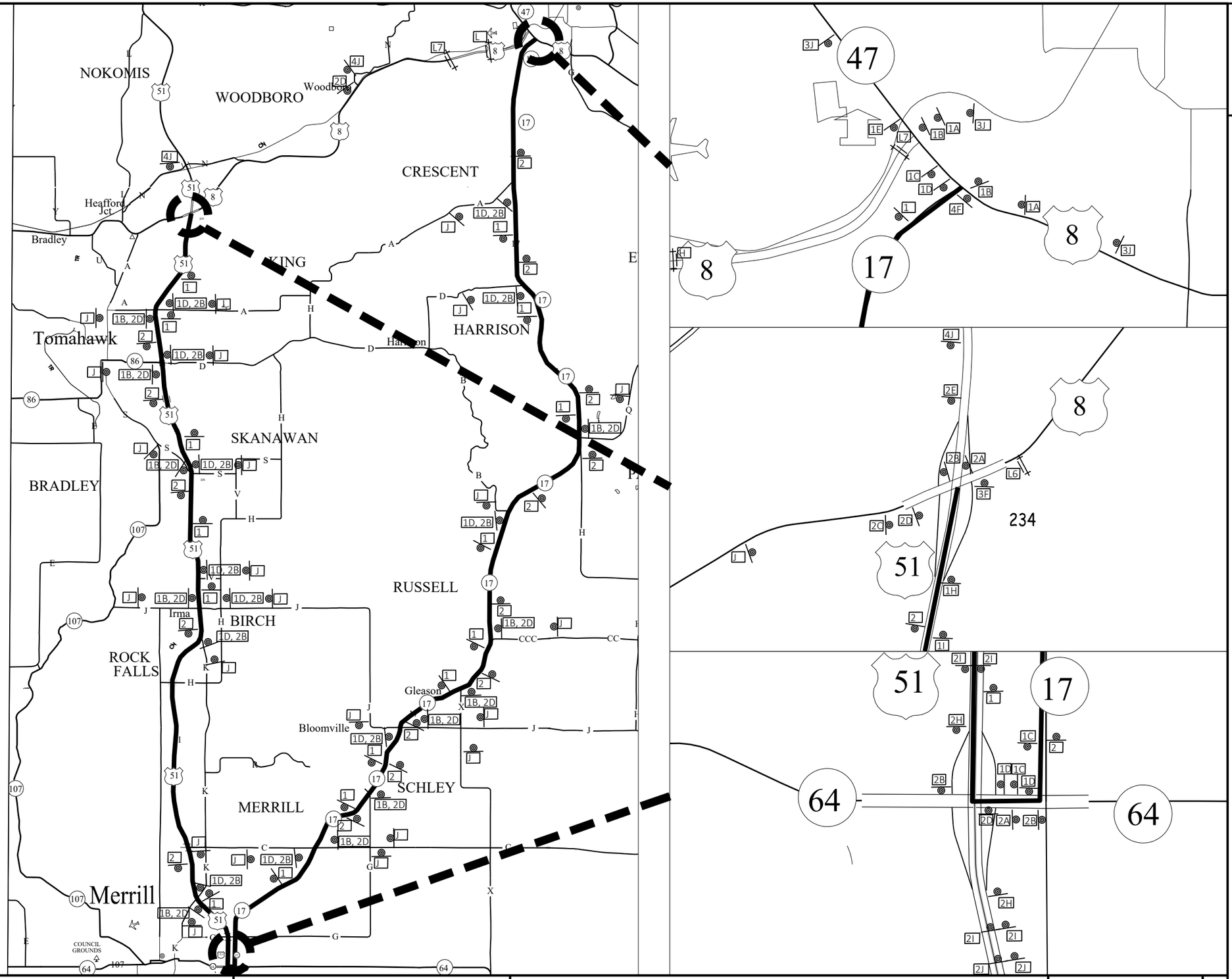
R11-4 60" X 30" ROAD CLOSED TO THRU TRAFFIC — TYPE III BARRICADE L

DETOUR AHEAD W20-2A J

LEGEND

- TRAFFIC CONTROL SIGN MOUNTED ON POST
- TC TYPE III BARRICADE WITH SIGN
- TC TYPE III BARRICADE
- X DENOTES SIGN NUMBER
- FLASHER TYPE A

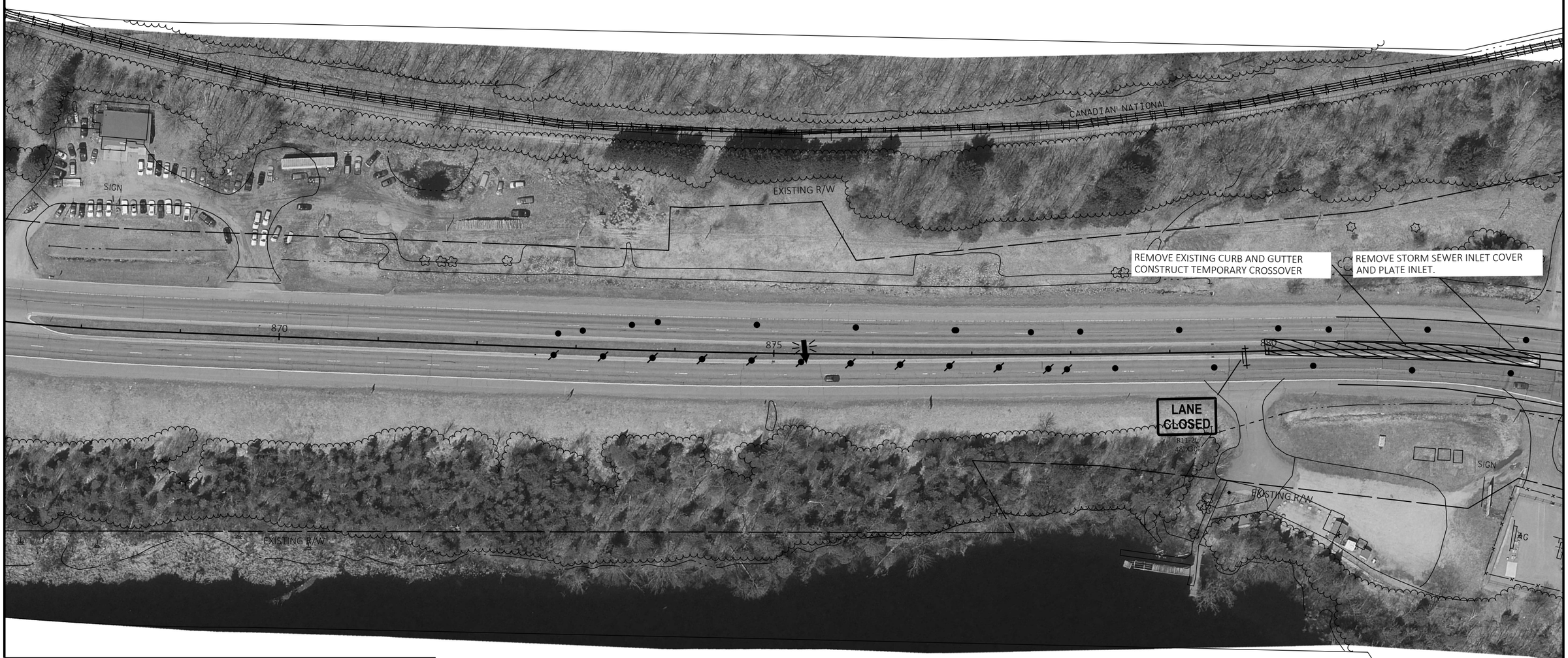
NOTE:
SPACING AND PLACEMENT OF SIGNS TO FOLLOW SDD "DETOUR SIGNING FOR MAINLINE CLOSURES", AND "BARRICADES AND SIGNS FOR MAINLINE CLOSURES".
ALL SIGNS ARE 48"X 48" UNLESS OTHERWISE NOTED.



PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	DETOUR ROUTE	SHEET 24 E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\DETOUR MAP.DWG PLOT DATE : 10/25/2022 3:58 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME :

2



LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WTH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- DELINEATOR FLEXIBLE/TUBULAR MARKER
- TYPE A WARNING LIGHT (FLASHING)
- TYPE C WARNING LIGHT (STEADY BURN)
- WORK AREA
- FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
- DIRECTION OF TRAFFIC

- CONSTRUCT SINGLE LANE CLOSURE AND PLACE ADVANCE WARNING SIGNS PER SDD15D20-06A.
- PLACE G20-57 (72"X36") SIGNS AT THE PROJECT TERMINI 7 DAYS BEFORE CONSTRUCTION AND REMOVED WHEN CONSTRUCTION BEGINS.

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 1	SHEET 25	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 1.DWG
LAYOUT NAME - 025100- 1-1

PLOT DATE : 10/31/2022 11:27 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

2

2

LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WTH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- DELINEATOR FLEXIBLE/TUBULAR MARKER
- TYPE A WARNING LIGHT (FLASHING)
- TYPE C WARNING LIGHT (STEADY BURN)
- WORK AREA
- FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
- DIRECTION OF TRAFFIC

N

LANE CLOSED
R11-2L
48"X30"

PLACE DRUMS SO THAT LEFT LANE IS NEVER ALLOWED TO FORM COMING OUT OF THE ROUNDABOUT

END ROAD WORK
G20-2A
48"X24"

885

890

895

POST

SIGN

SIGN 1

AG

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 1	SHEET 26	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 1.DWG
LAYOUT NAME - 025100- 1-2

PLOT DATE : 1/9/2023 3:57 AM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : Custom

2

LEGEND

	TYPE III BARRICADE	Ⓐ	TYPE A WARNING LIGHT (FLASHING)
	TYPE III BARRICADE WTH ATTACHED SIGN	Ⓒ	TYPE C WARNING LIGHT (STEADY BURN)
	TRAFFIC CONTROL DRUM		WORK AREA
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT		FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
	FLASHING ARROW BOARD		DIRECTION OF TRAFFIC
	SIGN ON PERMANENT SUPPORT		
	SIGN ON TEMPORARY SUPPORT		
	DELINEATOR FLEXIBLE/TUBULAR MARKER		

2

END ROAD WORK
G20-2A
48"X24"

EXISTING R/W

REMOVE PAVEMENT MARKINGS

WO1-6
48"X24"

WO1-6
48"X24"

25

730

740

TEMPORARY MARKING LINE PAINT 4-INCH

CHARLES DENTON LN.

EXISTING R/W

ROAD WORK AHEAD
W20-1A

EXISTING R/W

WO6-3

DO NOT DISTURB CORRUGATED CONCRETE MEDIAN.

WO1-4R

EXISTING R/W

ROAD CLOSED
R11-2
48"X30"

740

745

750

50.00

TEMPORARY MARKING LINE PAINT 4-INCH DOUBLE YELLOW

REMOVE EXISTING YELLOW LINE

EXISTING R/W

WO6-3

DO NOT PASS
R4-1
24"X30"

EXISTING R/W

GOLF COURSE RD.

ROAD WORK AHEAD
W20-1A

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 2	SHEET 27	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 2.DWG
LAYOUT NAME - 025100-2- 1

PLOT DATE : 10/13/2022 11:23 AM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

2

LEGEND

TYPE III BARRICADE	TYPE A WARNING LIGHT (FLASHING)
TYPE III BARRICADE WTH ATTACHED SIGN	TYPE C WARNING LIGHT (STEADY BURN)
TRAFFIC CONTROL DRUM	WORK AREA
TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT	FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
FLASHING ARROW BOARD	DIRECTION OF TRAFFIC
SIGN ON PERMANENT SUPPORT	
SIGN ON TEMPORARY SUPPORT	
DELINEATOR FLEXIBLE/TUBULAR MARKER	

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 2	SHEET 28	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 2.DWG
LAYOUT NAME - 025100-2- 2

PLOT DATE : 10/13/2022 11:27 AM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

2

LEGEND

TYPE III BARRICADE	TYPE A WARNING LIGHT (FLASHING)
TYPE III BARRICADE WTH ATTACHED SIGN	TYPE C WARNING LIGHT (STEADY BURN)
TRAFFIC CONTROL DRUM	WORK AREA
TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT	FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
FLASHING ARROW BOARD	
SIGN ON PERMANENT SUPPORT	DIRECTION OF TRAFFIC
SIGN ON TEMPORARY SUPPORT	
DELINEATOR FLEXIBLE/TUBULAR MARKER	

N

EXISTING R/W

785

790

795

50.00

REMOVE EXISTING YELLOW LINE

TEMPORARY MARKING LINE
PAINT 4-INCH DOUBLE YELLOW

EXISTING R/W

ROAD WORK AHEAD

RED ARROW DR.

COVER EXISTING STOP SIGN

STOP
R1-1
30"X30"

ROAD CLOSED
R11-2
48"X30"

EXISTING R/W

800

805

810

TEMPORARY MARKING
STOP LINE PAINT 18-INCH

ROAD WORK AHEAD

LAKE SHORE LN.

EXISTING R/W

2

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 2	SHEET 29	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 2.DWG
LAYOUT NAME - 025100-2- 3

PLOT DATE : 10/13/2022 11:38 AM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

2

LEGEND

TYPE III BARRICADE	TYPE A WARNING LIGHT (FLASHING)
TYPE III BARRICADE WTH ATTACHED SIGN	TYPE C WARNING LIGHT (STEADY BURN)
TRAFFIC CONTROL DRUM	WORK AREA
TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT	FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
FLASHING ARROW BOARD	
SIGN ON PERMANENT SUPPORT	DIRECTION OF TRAFFIC
SIGN ON TEMPORARY SUPPORT	
DELINEATOR FLEXIBLE/TUBULAR MARKER	

ROAD WORK AHEAD
W20-1A

COVER EXISTING STOP SIGN

AIRPORT RD.

STOP
R1-1
30"X30"

ROAD CLOSED
R11-2
48"X30"

MAINTAIN ACCESS DURING CONSTRUCTION

EXISTING R/W

815

820

825

50.00

TEMPORARY TAPE STOP BAR 18"

REMOVE EXISTING YELLOW LINE

TEMPORARY MARKING LINE PAINT 4-INCH DOUBLE YELLOW

EXISTING R/W

RAIL ROAD CROSSING
R15-1
48"X48"

EXEMPT
R15-3
24"X12"

YIELD
R1-2
36"X36"

MAINTAIN ACCESS DURING CONSTRUCTION

EXISTING R/W

ROAD CLOSED
R11-2
48"X30"

ROAD CLOSED
R11-2
48"X30"

ROAD CLOSED
R11-2
48"X30"

830

835

840

EXISTING R/W

EXISTING R/W

2

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 2	SHEET 30	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 2.DWG
LAYOUT NAME - 025100-2- 4

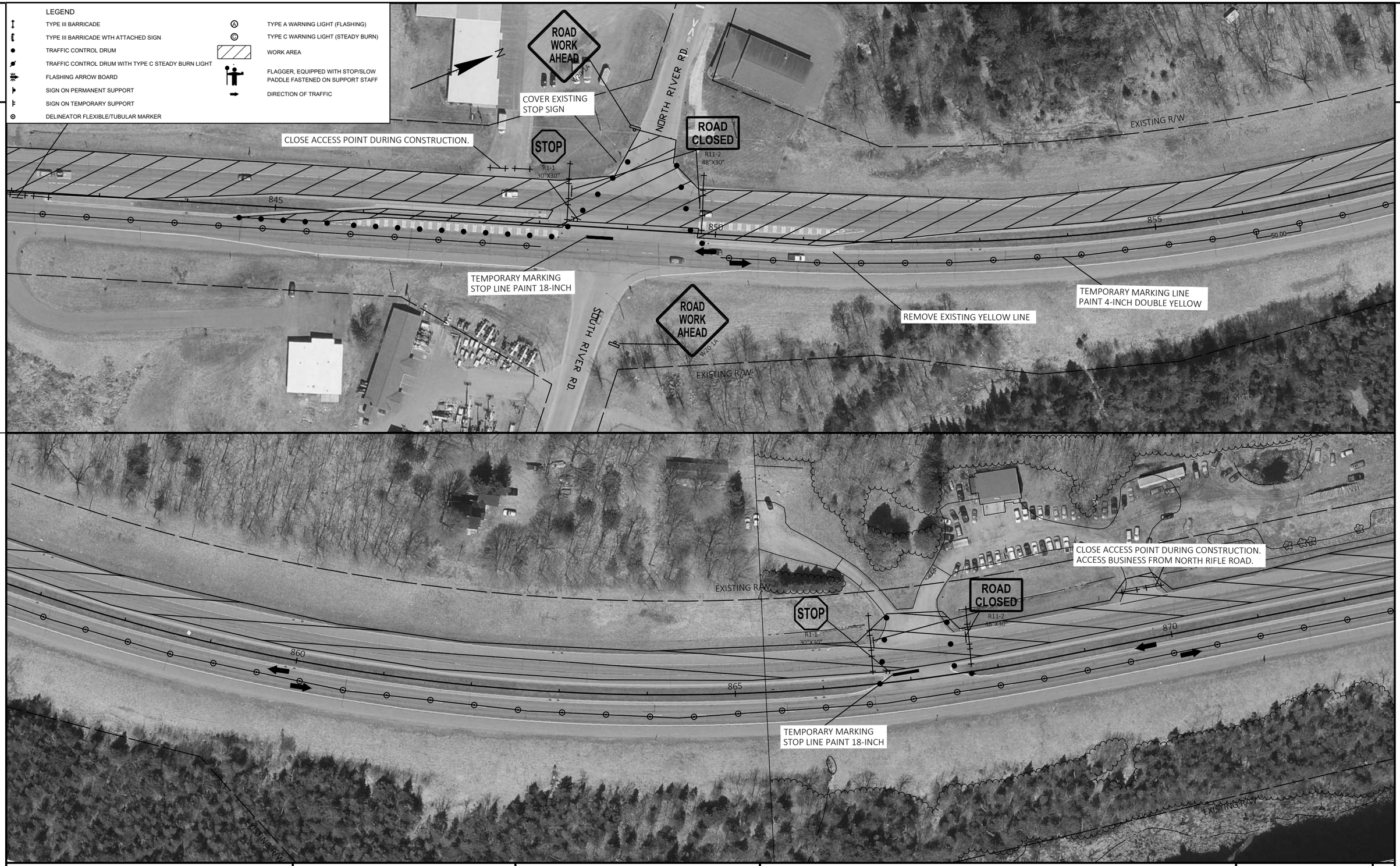
PLOT DATE : 10/13/2022 11:47 AM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : ##########

WISDOT/CADDS SHEET 44

2

LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WTH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- DELINEATOR FLEXIBLE/TUBULAR MARKER
- TYPE A WARNING LIGHT (FLASHING)
- TYPE C WARNING LIGHT (STEADY BURN)
- WORK AREA
- FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
- DIRECTION OF TRAFFIC



PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 2	SHEET 31 E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 2.DWG
LAYOUT NAME - 025100-2- 5

PLOT DATE : 10/13/2022 12:29 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

2

LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WTH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- DELINEATOR FLEXIBLE/TUBULAR MARKER
- TYPE A WARNING LIGHT (FLASHING)
- TYPE C WARNING LIGHT (STEADY BURN)
- WORK AREA
- FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
- DIRECTION OF TRAFFIC

DO NOT PASS
R4-1
24"X30"

ROAD CLOSED
R11-2
48"X30"

W9-2
48"X24"

MAINTAIN BUSINESS ACCESS DURING CONSTRUCTION

W9-4L

875

880

885

50.00

REMOVE EXISTING YELLOW LINE

TEMPORARY MARKING LINE
PAINT 4-INCH DOUBLE YELLOW

TEMPORARY MARKING LINE
PAINT 4-INCH SINGLE YELLOW

EXISTING R/W

AG

AG

AG

SIGN

END
ROAD WORK
G20-2A
48"X24"

2

PLACE ADVANCE WARNING SIGNS ON ALL 3 APPROACH LEGS TO THE ROUNDABOUT

WEST
M3-4
24"X12"

8
M1-4
24"X24"

ROAD WORK AHEAD
W20-1A

END
ROAD WORK
G20-2A
48"X24"

890

895

SIGN

SIGN

SIGN

SIGN

SIGN

POST

POST

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 2	SHEET 32	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 2.DWG
LAYOUT NAME - 025100-2- 6

PLOT DATE : 10/13/2022 12:32 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

2

2

LEGEND

	TYPE III BARRICADE	Ⓐ	TYPE A WARNING LIGHT (FLASHING)
	TYPE III BARRICADE WTH ATTACHED SIGN	Ⓒ	TYPE C WARNING LIGHT (STEADY BURN)
	TRAFFIC CONTROL DRUM		WORK AREA
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT		FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
	FLASHING ARROW BOARD		DIRECTION OF TRAFFIC
	SIGN ON PERMANENT SUPPORT		
	SIGN ON TEMPORARY SUPPORT		
	DELINEATOR FLEXIBLE/TUBULAR MARKER		

N

EXISTING R/W

END ROAD WORK
G20-2A
48"X24"

725

730

735

740

REMOVE PAVEMENT MARKINGS

TEMPORARY MARKING LINE TAPE 4-INCH

WO1-6
48"X24"

WO1-6
48"X24"

WO1-6
48"X24"

WO1-6
48"X24"

CHARLES DENTON LN

EXIST

ROAD WORK AHEAD
W20-1A

WO1-4L

EXISTING R/W

EXISTING R/W

WO1-4L

DO NOT DISTURB CORRUGATED CONCRETE MEDIAN.

740

50.00

745

750

TEMPORARY MARKING TAPE STOP LINE 18-INCH

STOP
R1-1
30"X30"

TEMPORARY MARKING LINE PAINT 4-INCH DOUBLE YELLOW

W6C3

EXISTING R/W

DO NOT PASS
R4-1
24"X30"

COVER EXISTING STOP SIGN

ROAD CLOSED
R11-2
48"X30"

GOLF COURSE RD.

EXISTING R/W

ROAD WORK AHEAD
W20-1A

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 3	SHEET 33	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 3.DWG
LAYOUT NAME - 025100-2- 1

PLOT DATE : 10/17/2022 9:27 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

2

LEGEND

	TYPE III BARRICADE	Ⓐ	TYPE A WARNING LIGHT (FLASHING)
	TYPE III BARRICADE WTH ATTACHED SIGN	Ⓒ	TYPE C WARNING LIGHT (STEADY BURN)
	TRAFFIC CONTROL DRUM		WORK AREA
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT		FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
	FLASHING ARROW BOARD		
	SIGN ON PERMANENT SUPPORT		DIRECTION OF TRAFFIC
	SIGN ON TEMPORARY SUPPORT		
	DELINEATOR FLEXIBLE/TUBULAR MARKER		

N

ROAD WORK AHEAD
W20-1A

SOUTH FOX RANCH RD.

ROAD CLOSED
R11-2
48"X30"

EXISTING R/W

TEMPORARY MARKING LINE
TAPE 4-INCH DOUBLE YELLOW

755 760 765

50.00

EXISTING R/W

SOUTH FOX RANCH RD.

ROAD CLOSED
R11-2
48"X30"

EXISTING R/W

770 775 780

EXISTING R/W

EXISTING R/W

2

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 3	SHEET 34	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 3.DWG
LAYOUT NAME - 025100-2- 2

PLOT DATE : 10/17/2022 9:29 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

2

LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WTH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- DELINEATOR FLEXIBLE/TUBULAR MARKER
- TYPE A WARNING LIGHT (FLASHING)
- TYPE C WARNING LIGHT (STEADY BURN)
- WORK AREA
- FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
- DIRECTION OF TRAFFIC

N

EXISTING R/W

TEMPORARY MARKING LINE
TAPE 4-INCH DOUBLE YELLOW

785 790 795

50.00

EXISTING R/W

EXISTING R/W

ROAD WORK AHEAD
W20-1

RED ARROW DR.

ROAD CLOSED
R11-2
48"X30"

STOP
R1-1
30"X30"

800 805 810

TEMPORARY MARKING
STOP LINE TAPE 18-INCH

COVER EXISTING
STOP SIGN

EXISTING R/W

LAKE SHORE LN.

ROAD WORK AHEAD
W20-1

2

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 3	SHEET 35	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 3.DWG
LAYOUT NAME - 025100-2- 3

PLOT DATE : 10/17/2022 9:32 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

2

LEGEND

TYPE III BARRICADE	TYPE A WARNING LIGHT (FLASHING)
TYPE III BARRICADE WTH ATTACHED SIGN	TYPE C WARNING LIGHT (STEADY BURN)
TRAFFIC CONTROL DRUM	WORK AREA
TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT	FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
FLASHING ARROW BOARD	
SIGN ON PERMANENT SUPPORT	DIRECTION OF TRAFFIC
SIGN ON TEMPORARY SUPPORT	
DELINEATOR FLEXIBLE/TUBULAR MARKER	

N

ROAD WORK AHEAD
W20-1A
48"X48"

AIRPORT RD.

ROAD CLOSED
R11-2
48"X30"

EXISTING R/W

TEMPORARY MARKING LINE
TAPE 4-INCH DOUBLE YELLOW

815 820 825

50.00

RAILROAD CROSSING
R15-1
48"X48"

EXEMPT
R15-3
24"X12"

R1-2
36"X31"

EXISTING R/W

TEMPORARY MARKING
STOP LINE TAPE 18-INCH

STOP
R1-1
30"X30"

ROAD CLOSED
R11-2
48"X30"

830 835 840

EXISTING R/W

TEMPORARY MARKING
STOP LINE TAPE 18-INCH

STOP
R1-1
30"X30"

ROAD CLOSED
R11-2
48"X30"

EXISTING R/W

2

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 3	SHEET 36	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 3.DWG
LAYOUT NAME - 025100-2- 4

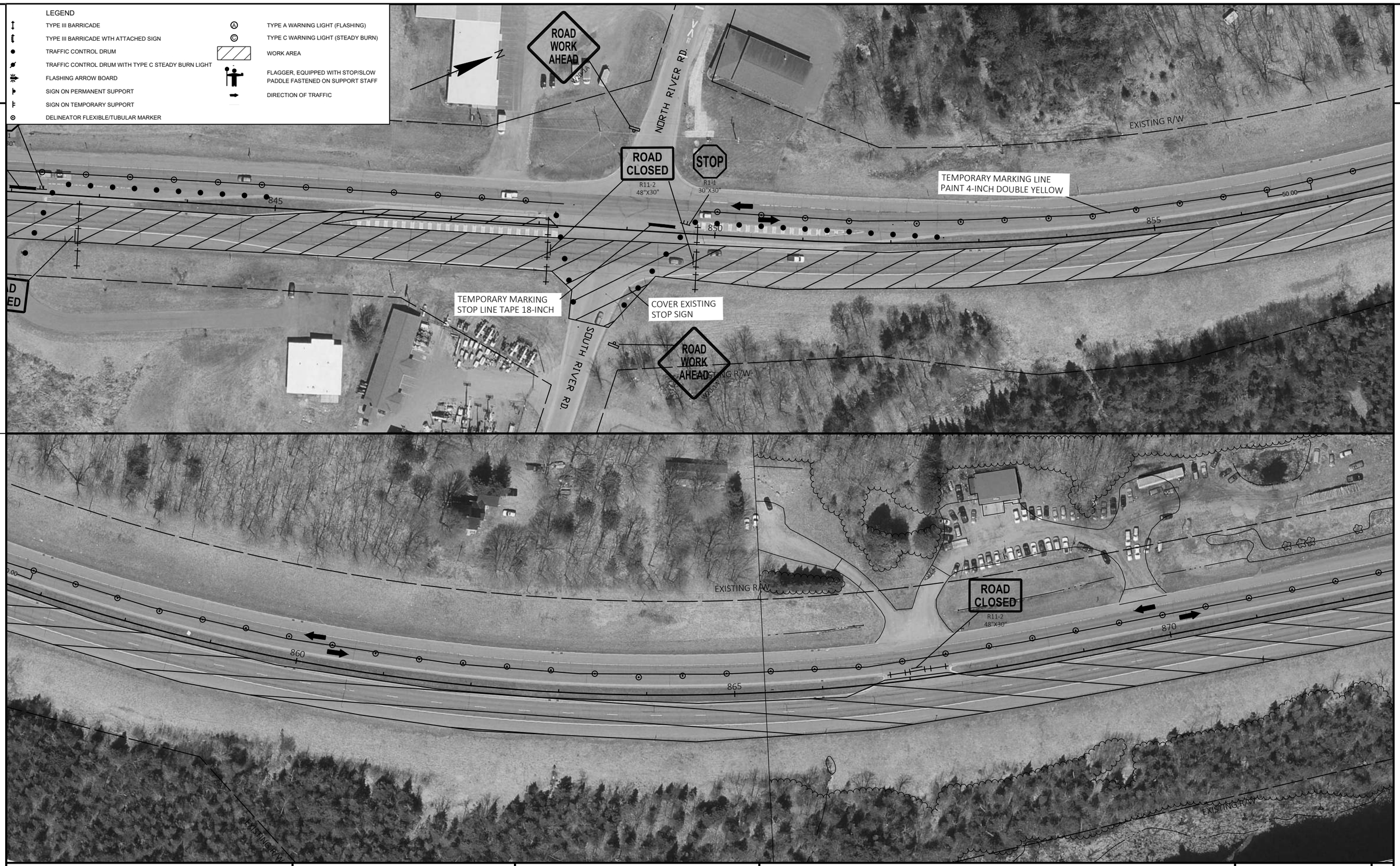
PLOT DATE : 10/17/2022 9:37 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : ##########

WISDOT/CADDS SHEET 44

2

LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WTH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- DELINEATOR FLEXIBLE/TUBULAR MARKER
- TYPE A WARNING LIGHT (FLASHING)
- TYPE C WARNING LIGHT (STEADY BURN)
- WORK AREA
- FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
- DIRECTION OF TRAFFIC



PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 3	SHEET 37	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 3.DWG
LAYOUT NAME - 025100-2- 5

PLOT DATE : 10/17/2022 9:39 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

2

LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WTH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- DELINEATOR FLEXIBLE/TUBULAR MARKER
- TYPE A WARNING LIGHT (FLASHING)
- TYPE C WARNING LIGHT (STEADY BURN)
- WORK AREA
- FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
- DIRECTION OF TRAFFIC

DO NOT PASS
R4-1
24"X30"

W06-3

TEMPORARY MARKING LINE
PAINT 4-INCH DOUBLE YELLOW

W01-6
48"X24"

875

50.00

880

885

TEMPORARY MARKING LINE
PAINT 4-INCH SINGLE YELLOW

W01-4R

EXISTING R/W

AG

AG

AG

SIGN

END
ROAD WORK
G20-2A
48"X24"

PLACE ADVANCE WARNING SIGNS ON ALL 3 APPROACH LEGS TO THE ROUNDABOUT

WEST
M3-4
24"X12"

8
M1-4
24"X24"

ROAD WORK AHEAD
W20-1A

END
ROAD WORK
G20-2A
48"X24"

890

SIGN

SIGN

SIGN

SIGN

POST

POST

SIGN

895

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 3	SHEET 38	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 3.DWG
LAYOUT NAME - 025100-2- 6

PLOT DATE : 10/18/2022 7:14 AM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

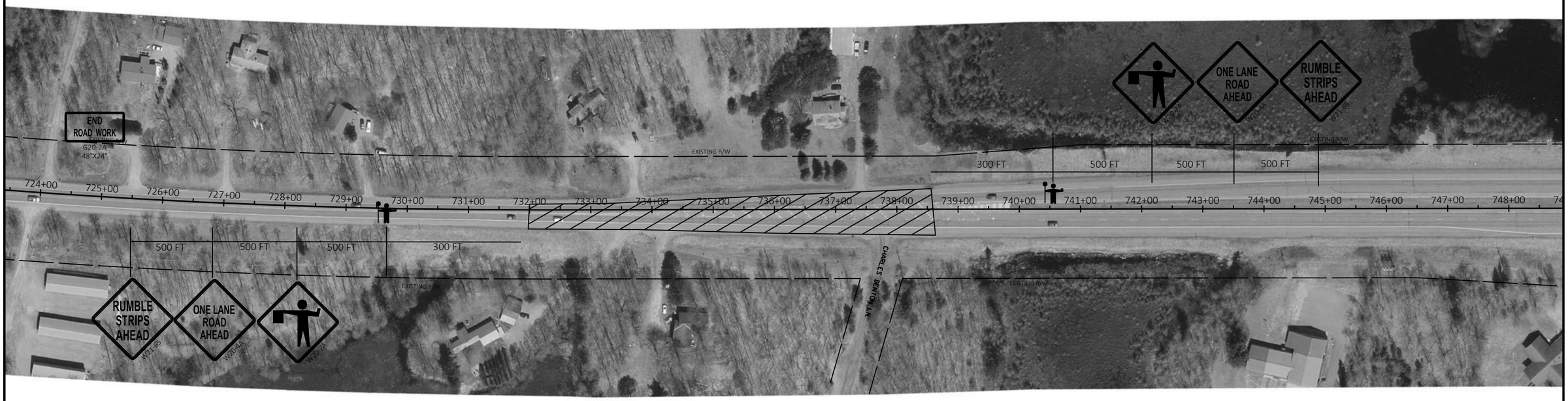
WISDOT/CADDS SHEET 44

2

LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WTH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- DELINEATOR FLEXIBLE/TUBULAR MARKER
- TYPE A WARNING LIGHT (FLASHING)
- TYPE C WARNING LIGHT (STEADY BURN)
- WORK AREA
- FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
- DIRECTION OF TRAFFIC

N



PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 4	SHEET 39	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 4.DWG
LAYOUT NAME - 025100-4-1

PLOT DATE : 10/18/2022 10:34 AM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : Custom

2

LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WTH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- DELINEATOR FLEXIBLE/TUBULAR MARKER
- TYPE A WARNING LIGHT (FLASHING)
- TYPE C WARNING LIGHT (STEADY BURN)
- WORK AREA
- FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
- DIRECTION OF TRAFFIC

N

CANADIAN NATIONAL

EXISTING R/W

END ROAD WORK
G20-2A
48"X24"

LANE CLOSED
R11-2L
48"X30"

REMOVE TEMPORARY CROSSOVER
REPLACE PAVEMENT INSIDE LANES
REPLACE CURB AND GUTTER
TOPSOIL AND SEED

873+00 874+00 875+00 876+00 877+00 878+00 879+00 880+00 881+00 882+00 883+00 884+00 885+00 886+00

LEFT LANE CLOSED AHEAD
W20-5L

W1-6R

EXISTING R/W

AG AG AG

SIGN SIGN

LANE CLOSED
R11-2L
48"X30"

PLACE DRUMS SO THAT LEFT LANE IS NOT ALLOWED TO FORM COMING OUT OF THE ROUNDABOUT

END ROAD WORK
G20-2A
48"X24"

PLACE W20-1A ROADWORK AHEAD SIGNS ON ALL 3 APPROACH LEGS TO THE ROUNDABOUT

2

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 4	SHEET 40	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 4.DWG
LAYOUT NAME - 025100-4-2

PLOT DATE : 10/18/2022 10:38 AM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 42

2

LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WTH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- DELINEATOR FLEXIBLE/TUBULAR MARKER
- TYPE A WARNING LIGHT (FLASHING)
- TYPE C WARNING LIGHT (STEADY BURN)
- WORK AREA
- FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
- DIRECTION OF TRAFFIC

N

CANADIAN NATIONAL

EXISTING R/W

END ROAD WORK
G20-2A
48"X24"

873+00 874+00 875+00 876+00 877+00 878+00 879+00 880+00 881+00 882+00 883+00 884+00 885+00 886+00

LANE CLOSED
R11-2L
48"X30"

RIGHT LANE CLOSED AHEAD
W20-5L
48"X48"

REPLACE PAVEMENT IN HATCHED AREA
DURING STAGE 5 TRAFFIC CONTROL

EXISTING R/W

AG

SIGN

SIGN

LANE CLOSED
R11-2L
48"X30"

PLACE DRUMS SO THAT RIGHT LANE IS NOT ALLOWED TO FORM COMING OUT OF THE ROUNDABOUT

END ROAD WORK
G20-2A
48"X24"

2

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL STAGE 5	SHEET 41	E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\025100-TC_STAGE 5TEMP.DWG
LAYOUT NAME - 025100-5-1

PLOT DATE : 10/18/2022 12:22 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 42

1595-09-73

Line	Item	Item Description	Unit	Total	Qty
0002	203.0100	Removing Small Pipe Culverts	EACH	2.000	2.000
0004	204.0115	Removing Asphaltic Surface Butt Joints	SY	400.000	400.000
0006	204.0120	Removing Asphaltic Surface Milling	SY	135,764.000	135,764.000
0008	204.0150	Removing Curb & Gutter	LF	6,640.000	6,640.000
0010	204.0215	Removing Catch Basins	EACH	1.000	1.000
0012	204.0220	Removing Inlets	EACH	1.000	1.000
0014	205.0100	Excavation Common	CY	4,874.000	4,874.000
0016	211.0101	Prepare Foundation for Asphaltic Paving (project) 01.1595-09-73	EACH	1.000	1.000
0018	211.0400	Prepare Foundation for Asphaltic Shoulders	STA	13.000	13.000
0020	213.0100	Finishing Roadway (project) 01.1595-09-73	EACH	1.000	1.000
0022	305.0110	Base Aggregate Dense 3/4-Inch	TON	4,834.000	4,834.000
0024	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	250.000	250.000
0026	450.4000	HMA Cold Weather Paving	TON	3,826.000	3,826.000
0028	455.0605	Tack Coat	GAL	15,070.000	15,070.000
0030	460.0105.S	HMA Percent Within Limits (PWL) Test Strip Volumetrics	EACH	2.000	2.000
0032	460.0110.S	HMA Percent Within Limits (PWL) Test Strip Density	EACH	3.000	3.000
0034	460.2005	Incentive Density PWL HMA Pavement	DOL	6,720.000	6,720.000
0036	460.2007	Incentive Density HMA Pavement Longitudinal Joints	DOL	8,200.000	8,200.000
0038	460.2010	Incentive Air Voids HMA Pavement	DOL	8,200.000	8,200.000
0040	460.6223	HMA Pavement 3 MT 58-28 S	TON	29,874.000	29,874.000
0042	460.6245	HMA Pavement 5 MT 58-34 S	TON	11,794.000	11,794.000
0044	465.0125	Asphaltic Surface Temporary	TON	335.000	335.000
0046	522.0136	Culvert Pipe Reinforced Concrete Class III 36-Inch	LF	225.000	225.000
0048	522.1036	Apron Endwalls for Culvert Pipe Reinforced Concrete 36-Inch	EACH	2.000	2.000
0050	522.2319	Culvert Pipe Reinforced Concrete Horizontal Elliptical Class HE-III 19x30-Inch	LF	55.000	55.000
0052	522.2619	Apron Endwalls for Culvert Pipe Reinforced Concrete Horizontal Elliptical 19x30-Inch	EACH	1.000	1.000
0054	601.0557	Concrete Curb & Gutter 6-Inch Sloped 36-Inch Type D	LF	6,640.000	6,640.000
0056	611.0430	Reconstructing Inlets	EACH	5.000	5.000
0058	611.0642	Inlet Covers Type MS	EACH	1.000	1.000
0060	611.3903	Inlets Median 3 Grate	EACH	1.000	1.000
0062	618.0100	Maintenance And Repair of Haul Roads (project) 01.1595-09-73	EACH	1.000	1.000
0064	619.1000	Mobilization	EACH	1.000	1.000
0066	624.0100	Water	MGAL	74.000	74.000
0068	625.0100	Topsoil	SY	497.000	497.000
0070	628.1504	Silt Fence	LF	585.000	585.000
0072	628.1520	Silt Fence Maintenance	LF	585.000	585.000
0074	628.1905	Mobilizations Erosion Control	EACH	6.000	6.000
0076	628.1910	Mobilizations Emergency Erosion Control	EACH	3.000	3.000
0078	628.2002	Erosion Mat Class I Type A	SY	497.000	497.000
0080	628.7010	Inlet Protection Type B	EACH	20.000	20.000
0082	628.7015	Inlet Protection Type C	EACH	75.000	75.000
0084	628.7555	Culvert Pipe Checks	EACH	2.000	2.000
0086	629.0210	Fertilizer Type B	CWT	0.313	0.313
0088	630.0130	Seeding Mixture No. 30	LB	8.900	8.900
0090	630.0500	Seed Water	MGAL	2.900	2.900
0092	642.5201	Field Office Type C	EACH	1.000	1.000
0094	643.0300	Traffic Control Drums	DAY	6,000.000	6,000.000
0096	643.0420	Traffic Control Barricades Type III	DAY	1,508.000	1,508.000
0098	643.0500	Traffic Control Flexible Tubular Marker Posts	EACH	600.000	600.000

1595-09-73

Line	Item	Item Description	Unit	Total	Qty
0100	643.0600	Traffic Control Flexible Tubular Marker Bases	EACH	600.000	600.000
0102	643.0705	Traffic Control Warning Lights Type A	DAY	2,010.000	2,010.000
0104	643.0715	Traffic Control Warning Lights Type C	DAY	1,680.000	1,680.000
0106	643.0800	Traffic Control Arrow Boards	DAY	40.000	40.000
0108	643.0900	Traffic Control Signs	DAY	2,876.000	2,876.000
0110	643.0910	Traffic Control Covering Signs Type I	EACH	98.000	98.000
0112	643.1000	Traffic Control Signs Fixed Message	SF	36.000	36.000
0114	643.3105	Temporary Marking Line Paint 4-Inch	LF	30,274.000	30,274.000
0116	643.3150	Temporary Marking Line Removable Tape 4-Inch	LF	34,674.000	34,674.000
0118	643.3605	Temporary Marking Word Paint	EACH	1.000	1.000
0120	643.3650	Temporary Marking Word Removable Tape	EACH	1.000	1.000
0122	643.3805	Temporary Marking Stop Line Paint 18-Inch	LF	60.000	60.000
0124	643.3850	Temporary Marking Stop Line Removable Tape 18-Inch	LF	60.000	60.000
0126	643.5000	Traffic Control	EACH	1.000	1.000
0128	646.1545	Marking Line Grooved Wet Ref Contrast Epoxy 4-Inch	LF	60,363.000	60,363.000
0130	646.3545	Marking Line Grooved Wet Ref Contrast Epoxy 8-Inch	LF	2,611.000	2,611.000
0132	646.5020	Marking Arrow Epoxy	EACH	12.000	12.000
0134	646.5120	Marking Word Epoxy	EACH	12.000	12.000
0136	646.5320	Marking Railroad Crossings Epoxy	EACH	4.000	4.000
0138	646.6120	Marking Stop Line Epoxy 18-Inch	LF	50.000	50.000
0140	646.6464	Cold Weather Marking Epoxy 4-Inch	LF	6,036.000	6,036.000
0142	646.6468	Cold Weather Marking Epoxy 8-Inch	LF	261.000	261.000
0144	646.7120	Marking Diagonal Epoxy 12-Inch	LF	40.000	40.000
0146	646.8220	Marking Island Nose Epoxy	EACH	23.000	23.000
0148	646.9000	Marking Removal Line 4-Inch	LF	4,267.000	4,267.000
0150	646.9100	Marking Removal Line 8-Inch	LF	2,120.000	2,120.000
0152	646.9300	Marking Removal Special Marking	EACH	4.000	4.000
0154	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	6,640.000	6,640.000
0156	650.6000	Construction Staking Pipe Culverts	EACH	2.000	2.000
0158	650.8000	Construction Staking Resurfacing Reference	LF	14,943.000	14,943.000
0160	650.9911	Construction Staking Supplemental Control (project) 01.1595-09-73	EACH	1.000	1.000
0162	690.0150	Sawing Asphalt	LF	520.000	520.000
0164	690.0250	Sawing Concrete	LF	108.000	108.000
0166	740.0440	Incentive IRI Ride	DOL	11,480.000	11,480.000
0168	ASP.1T0A	On-the-Job Training Apprentice at $5.00/HR	HRS	2,400.000	2,400.000
0170	ASP.1T0G	On-the-Job Training Graduate at $5.00/HR	HRS	1,980.000	1,980.000

3

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203- REMOVING CULVERTS

CATEGORY	STATION	TO	STATION	LOCATION	203.0100 REMOVING SMALL PIPE CULVERTS EACH	205.0100 EXCAVATION COMMON CY	REMARKS
0010	787+40	-	787+40	WB+LT	1	3,917	225FT 36-INCH RCP
0010	836+90	-	836+90	EB+RT	1	957	55FT 19X30-INCH RCHEP
0010	847+57	-	847+57	EB+RT	-	65	TYPE 3 FIELD INLET AND GRADING
				TOTAL 0010	2	4,874	

204- MILLING

CATEGORY	STATION	TO	STATION	LOCATION	204.0115 REMOVING ASPHALTIC SURFACE BUTT JOINTS SY	204.0120 REMOVING ASPHALTIC SURFACE MILLING SY	REMARKS
0010	732+58	-	732+58	BEGIN PROJECT	79	-	
0010	0+87	-	0+87	GOLF COURSE RD	33	412	
0010	1+13	-	1+13	S FOX RANCH RD	33	439	
0010	1+09	-	1+09	RED ARROW DR	33	389	
0010	0+85	-	0+85	LAKESHORE DR	44	576	
0010	814+15	-	814+15	RAILROAD CROSSING	-	-	
0010	1+09	-	1+09	AIRPORT RD	33	363	
0010	1+25	-	1+25	NORTH RIVER RD	33	742	
0010	0+90	-	0+90	SOUTH RIVER RD	33	612	
0010	883+37	-	883+37	END PROJECT	79	-	
0010	PROJECT	-	PROJECT	EASTBOUND SHOULDER	-	20455	
0010	732+00	-	740+00	EASTBOUND LANES	-	1740	
0010	740+00	-	745+86.85	EASTBOUND LANES	-	1925	
0010	745+86.85	-	823+00	EASTBOUND LANES	-	22395	
0010	823+00	-	883+36.92	EASTBOUND LANES	-	17640	
0010	PROJECT	-	PROJECT	WESTBOUND SHOULDER	-	19849	
0010	732+00	-	740+00	WESTBOUND LANES	-	1710	
0010	740+00	-	745+86.85	WESTBOUND LANES	-	1508	
0010	745+86.85	-	823+00	WESTBOUND LANES	-	22,195	
0010	823+00	-	883+36.92	WESTBOUND LANES	-	16,587	
0010	750+00	-	755+00	CENTER TURN LANES	-	670	
0010	764+00	-	769+00	CENTER TURN LANES	-	555	
0010	806+00	-	814+00	CENTER TURN LANES	-	938	
0010	815+00	-	820+00	CENTER TURN LANES	-	680	
0010	829+00	-	831+00	CENTER TURN LANES	-	375	
0010	833+00	-	835+00	CENTER TURN LANES	-	375	
0010	841+00	-	845+00	CENTER TURN LANES	-	564	
0010	844+50	-	853+00	CENTER TURN LANES	-	1,320	
0010	866+50	-	868+00	CENTER TURN LANES	-	375	
0010	882+50	-	883+50	CENTER TURN LANES	-	375	
				TOTAL 0010	400	135,764	

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305- BASE AGGREGATE

CATEGORY	STATION	TO	STATION	LOCATION	211.0400 PREPARE FOUNDATION FOR ASPHALTIC SHOULDERS STA	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	624.0100 WATER MGAL	REMARKS
0010	739+00	-	751+00	WB - LT	13	-	-	-	
0010	PROJECT	-	PROJECT	EB-RT, WB-LT	-	4834	250	74	
				TOTAL 0010	13	4,834	250	74	

450- HMA

CATEGORY	STATION	LOCATION	450.4000 HMA COLD WEATHER PAVING TON	455.0605 TACK COAT GAL	460.6223 HMA PAVEMENT 3 MT 58-28 S TON	460.6245 HMA PAVEMENT 5 MT 58-34 S TON	465.0125 ASPHALTIC SURFACE TEMPORARY TON	REMARKS
0010	848+94	SOUTH RIVER RD.	14	43	456	175	-	INTERSECTION, TURN LANES
0010	848+94	NORTH RIVER RD	17	52	474	186	-	INTERSECTION, TURN LANES
0010	819+30	AIRPORT RD	9	26	420	154	-	INTERSECTION, TURN LANES
0010	810+46	LAKE SHORE LN	13	41	449	172	-	INTERSECTION, TURN LANES
0010	810+46	RED ARROW DR	9	28	423	156	-	INTERSECTION, TURN LANES
0010	768+00	S FOX RANCH RD	10	31	430	161	-	INTERSECTION, TURN LANES
0010	751+49	GOLF COURSE RD	10	29	426	158	-	INTERSECTION, TURN LANES
0010	PROJECT	USH 8 EB	1,475	6,118	11,062	3,688	-	DRIVING LANES
0010	PROJECT	USH 8 WB	1,418	5,880	10,632	3,544	-	DRIVING LANES
0010	PROJECT	USH 8 EB SHOULDERS	432	1,432	2,589	1,726	-	EB SHOULDERS
0010	PROJECT	USH 8 WB SHOULDERS	419	1,390	2,513	1,675	-	WB SHOULDERS
0010	785+26	CULVERT	-	-	-	-	250	C43008003062
0010	836+90	CULVERT	-	-	-	-	85	C43008003076
		TOTAL 0010	3,826	15,070	29,874	11,794	335	

PWL MIXTURE USE TABLE

LOCATION	MIXTURE USE	UNDERLYING SURFACE	BID ITEM	THICKNESS	TONS*	QUALITY MANAGEMENT PROGRAM TO BE USED: MIXTURE ACCEPTANCE	DENSITY ACCEPTANCE
12 FOOT DRIVING LANE	LOWER LAYER	BASE AGGREGATE	460.6223 3MT 58-28S	2.25"	21,694	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
8 FOOT SHOULDER	LOWER LAYER	MILLED EXISTING HMA BASE	460.6223 3MT 58-28S	2.25"	5,102	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT, NOT ELIGIBLE FOR INCENTIVE
INTERSECTIONS, TURN LANES	LOWER LAYER	BASE AGGREGATE	460.6223 3MT 58-28S	2.25"	2,578	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
SIDEROADS	LOWER LAYER	MILLED EXISTING HMA BASE	460.6223 3MT 58-28S	2.50"	500	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
12 FOOT DRIVING LANE	UPPER LAYER	TACK COATED HMA	460.6245 5MT 58-34S	1.50"	7,232	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
8 FOOT SHOULDER	UPPER LAYER	TACK COATED HMA	460.6245 5MT 58-34S	1.50"	3,401	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT, NOT ELIGIBLE FOR INCENTIVE
INTERSECTIONS, TURN LANES	UPPER LAYER	TACK COATED HMA	460.6245 5MT 58-34S	1.50"	860	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
SIDEROADS	UPPER LAYER	TACK COATED HMA	460.6245 5MT 58-34S	1.50"	301	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005

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522 - CULVERTS

CATEGORY	STATION	TO	STATION	LOCATION	522.0136 CULVERT PIPE REINFORCED CONCRETE CLASS III 36-INCH LF	522.1036 APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 36-INCH EACH	522.2319 CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL CLASS HE-III 19X30-INCH LF	522.2619 APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL 19X30-INCH EACH	REMARKS
0010	787+40	-	787+40	WB+LT	225	2	-	-	CULVERT 43008003062
0010	836+90	-	836+90	EB+RT	-	-	55	1	CULVERT 43008003076
				TOTAL 0010	225	2	55	1	

601 - CURB & GUTTER

CATEGORY	STATION	TO	STATION	LOCATION	204.0150 REMOVING CURB & GUTTER LF	601.0557 CONCRETE CURB & GUTTER 6-INCH SLOPED 36-INCH TYPE D LF	REMARKS
0010	747+96	-	749+30	WB WEST OF GOLF COURSE RD	142	142	
0010	754+40	-	754+77	EB/WB EAST OF GOLF COURSE RD	565	565	
0010	762+92	-	763+80	EB/WB WEST OF FOX RANCH RD	861	861	
0010	768+44	-	768+95	FOX RANCH RD EAST SIDE	52	52	
0010	768+95	-	778+42	EB/WB EAST OF FOX RANCH	1060	1060	
0010	796+10	-	797+16	WB WEST OF RED ARROW DR	108	108	
0010	811+11	-	813+98	WB EAST OF RED ARROW DR	288	288	
0010	816+39	-	818+60	EB WEST OF AIRPORT RD	222	222	
0010	820+38	-	823+03	EB/WB EAST OF AIRPORT RD	447	447	
0010	823+10	-	829+39	EB EAST OF AIRPORT RD	629	629	
0010	830+85	-	833+36	EB EAST OF AIRPORT RD	252	252	
0010	834+40	-	836+66	EB EAST OF AIRPORT RD	226	226	
0010	843+06	-	846+34	EB WEST OF NORTH RIVER RD	330	330	
0010	844+25	-	847+94	WB WEST OF NORTH RIVER RD	370	370	
0010	845+36	-	848+00	EB SOUTH RIVER RD TURN LANE	263	263	
0010	864+89	-	866+30	EB EAST OF SOUTH RIVER RD	138	138	
0010	867+88	-	868+43	EB EAST OF SOUTH RIVER RD	57	57	
0010	879+44	-	882+61	WB WEST OF ROUNDABOUT	315	315	
0010	879+44	-	882+61	EB WEST OF ROUNDABOUT	315	315	
				TOTAL 0010	6,640	6,640	

611 - STORM SEWER ITEMS

CATEGORY	STATION	LOCATION	204.0215 REMOVING CATCH BASINS EACH	204.0220 REMOVING INLETS EACH	611.0430 RECONSTRUCTING INLETS EACH	611.0642 INLET COVERS TYPE MS EACH	611.3903 INLETS MEDIAN 3 GRATE EACH	628.7010 INLET PROTECTION TYPE B EACH	628.7015 INLET PROTECTION TYPE C EACH
0010	741+48	WB USH 8 MEDIAN	-	-	1	-	-	-	-
0010	741+48	EB USH 8 MEDIAN	-	-	1	-	-	-	-
0010	744+10	WB USH 8 MEDIAN	-	-	1	-	-	-	-
0010	749+38	WB USH 8 MEDIAN	-	-	1	-	-	-	-
0010	749+41	EB USH 8 MEDIAN	-	-	1	-	-	-	-
0010	847+57	EB USH 8 MEDIAN	1	1	-	1	1	-	-
0010	PROJECT		-	-	-	-	-	20	75
		TOTAL 0010	1	1	5	1	1	20	75

619- MOBILIZATION

CATEGORY	STATION	LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH	REMARKS
0010	PROJECT	PROJECT	6	3	
		TOTAL 0010	6	3	

625- EROSION CONTROL

CATEGORY	STATION	TO	STATION	LOCATION	625.0100 TOPSOIL SY	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF	628.2002 EROSION MAT CLASS I TYPE A SY	629.0210 FERTILIZER TYPE B CWT	630.0130 SEEDING MIXTURE NO. 30 LB	630.0500 SEED WATER MGAL	REMARKS
0010	PROJECT	-	PROJECT	EASTBOUND SHOULDER	-	-	-	-	-	-	-	
0010	PROJECT	-	PROJECT	WESTBOUND SHOULDER	-	-	-	-	-	-	-	
0010	751+49	-		GOLF COURSE RD	9	30	30	9	0.01	0.2	0.1	
0010	768+00	-		S FOX RANCH RD	9	30	30	9	0.01	0.2	0.1	
0010	810+46	-		RED ARROW DR	27	70	70	27	0.02	0.5	0.2	
0010	819+30	-		AIRPORT RD	23	60	60	23	0.01	0.4	0.1	
0010	848+94	-		NORTH RIVER RD	84	197	197	84	0.05	1.5	0.5	
0010	848+94	-		SOUTH RIVER RD	84	198	198	84	0.05	1.5	0.5	
0010	732+58	-	751+00	BOP TO GOLF COURSE RD	27	-	-	27	0.02	0.5	0.2	
0010	752+00	-	767+50	GOLF COURSE RD TO FOX RANCH RD	36	-	-	36	0.02	0.6	0.2	
0010	768+50	-	809+75	FOX RANCH RD TO RED ARROW RD	72	-	-	72	0.05	1.3	0.4	
0010	811+00	-	818+60	RED ARROW DR TO AIRPORT RD	36	-	-	36	0.02	0.6	0.2	
0010	819+50	-	848+00	AIRPORT RD TO RIVER RD	45	-	-	45	0.03	0.8	0.2	
0010	849+75	-	883+37	RIVER RD TO EOP	45	-	-	45	0.03	0.8	0.2	
				TOTAL 0010	497	585	585	497	0.31	8.9	2.9	

643- TEMPORARY MARKING

CATEGORY	STATION	TO	STATION	LOCATION	643.3105 TEMPORARY MARKING LINE PAINT 4-INCH LF	643.3150 TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH LF	643.3605 TEMPORARY MARKING WORD PAINT EACH	643.3650 TEMPORARY MARKING WORD REMOVABLE TAPE EACH	643.3805 TEMPORARY MARKING STOP LINE PAINT 18-INCH LF	643.3850 TEMPORARY MARKING STOP LINE REMOVABLE TAPE 18-INCH LF	REMARKS
0010	732+00	-	883+36.92		30,274	-	-	-	-	-	DBL YELLOW CENTERLINE STAGE 2
0010	732+00	-	883+36.92		-	30274	-	-	-	-	DBL YELLOW CENTERLINE STAGE 3
0010	732+00		883+36.92		-	-	-	-	60	-	STOP LINES STAGE 2
					-	-	-	-	-	60	STOP LINES STAGE 3
0010	732+00	-	740+00		-	3,200	-	-	-	-	WEST CROSSOVER
0010	879+44	-	883+36.92		-	1,200	1	1	-	-	EAST CROSSOVER
0010		-									
				TOTAL 0010	30,274	34,674	1	1	60	60	

3

643- TRAFFIC CONTROL

CATEGORY	STATION TO STATION	LOCATION	643.0300 TRAFFIC CONTROL DRUMS DAY	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAY	643.0500 TRAFFIC CONTROL FLEXIBLE TUBULAR MARKER POSTS EACH	643.0600 TRAFFIC CONTROL FLEXIBLE TUBULAR MARKER BASES EACH	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	643.0715 TRAFFIC CONTROL WARNING LIGHTS TYPE C DAY	643.0800 TRAFFIC CONTROL ARROW BOARDS DAY	643.0900 TRAFFIC CONTROL SIGNS DAY	643.0910 TRAFFIC CONTROL COVERING SIGNS TYPE I EACH	CYCLES	SIGNS	643.1000 TRAFFIC CONTROL SIGNS FIXED MESSAGE SF
0010	732+58 - 751+00	BOP TO GOLF COURSE RD	600	90	100	100	119	-	-	540	2	2	1	18
0010	752+00 - 767+50	GOLF COURSE RD TO FOX RANCH RD	1,070	180	100	100	238	-	-	120	2	2	1	
0010	768+50 - 809+75	FOX RANCH RD TO RED ARROW RD	1,070	180	100	100	238	-	-	150	4	2	2	
0010	811+00 - 818+60	RED ARROW DR TO AIRPORT RD	1,070	180	100	100	238	-	-	120	2	2	1	
0010	819+50 - 848+00	AIRPORT RD TO RIVER RD	970	510	100	100	673	-	-	240	4	2	2	
0010	849+75 - 883+37	RIVER RD TO EOP	1,220	340	100	100	449	1,680	40	440	-	-	-	
0010	DETOUR	DETOUR	-	28	-	-	56	-	-	1,266	84	2	42	18
		TOTAL 0010	6,000	1,508	600	600	2,010	1,680	40	2,876	98			36

646- MARKING REMOVAL

CATEGORY	STATION	TO	STATION	LOCATION	646.9000 MARKING REMOVAL LINE 4-INCH LF	646.9100 MARKING REMOVAL LINE 8-INCH LF	646.9300 MARKING REMOVAL SPECIAL MARKING EACH	REMARKS
0010	732+00	-	883+36.92		3,633	-	-	CENTERLINE STAGE 2
0010	732+00	-	740+00		-	2,120	-	WEST CROSSOVER
0010	805+40	-	806+10		-	-	4	REMOVE RAILROAD CROSSBUCKS
0010	879+44	-	882+61		634	-	-	EAST EDGELINES AT CROSSOVER
				TOTAL 0010	4,267	2,120	4	

646- PAVEMENT MARKING

CATEGORY	STATION	TO	STATION	LOCATION	646.1545 MARKING LINE GROOVED WET REF CONTRAST EPOXY 4-INCH LF	646.3545 MARKING LINE GROOVED WET REF CONTRAST EPOXY 8-INCH LF	646.5020 MARKING ARROW EPOXY EACH	646.5120 MARKING WORD EPOXY EACH	646.5320 MARKING RAILROAD CROSSINGS EPOXY EACH	646.6120 MARKING STOP LINE EPOXY 18-INCH LF	646.6464 COLD WEATHER MARKING EPOXY 4-INCH LF	646.6468 COLD WEATHER MARKING EPOXY 8-INCH LF	646.7120 MARKING DIAGONAL EPOXY 12-INCH LF	646.8220 MARKING ISLAND NOSE EPOXY EACH	REMARKS
0010	732+58	-	751+00	BOP TO GOLF COURSE RD	2752	-	-	-	-	-	-	-	20	2	WESTBOUND
0010	752+00	-	767+50	GOLF COURSE RD TO FOX RANCH RD	3251	-	1	1	-	-	-	-	-	2	WESTBOUND
0010	768+50	-	809+75	FOX RANCH RD TO RED ARROW RD	9297	196	1	1	-	-	-	-	-	2	WESTBOUND
0010	811+00	-	818+60	RED ARROW DR TO AIRPORT RD	1418	340	-	-	2	25	-	-	-	4	WESTBOUND
0010	819+50	-	848+00	AIRPORT RD TO RIVER RD	5950	300	2	2	-	-	-	-	-	8	WESTBOUND
0010	849+75	-	883+37	RIVER RD TO EOP	7388	250	2	2	-	-	-	-	-	5	WESTBOUND
											-	-			
0010	732+58	-	751+00	BOP TO GOLF COURSE RD	2905	190	1	1	-	-	-	-	20	-	EASTBOUND
0010	752+00	-	767+50	GOLF COURSE RD TO FOX RANCH RD	3214	203	1	1	-	-	-	-	-	-	EASTBOUND
0010	768+50	-	809+75	FOX RANCH RD TO RED ARROW RD	8975	400	1	1	2	-	-	-	-	-	EASTBOUND
0010	811+00	-	818+60	RED ARROW DR TO AIRPORT RD	1393	200	1	1	-	25	-	-	-	-	EASTBOUND
0010	819+50	-	848+00	AIRPORT RD TO RIVER RD	6428	532	2	2	-	-	-	-	-	-	EASTBOUND
0010	849+75	-	883+37	RIVER RD TO EOP	7393	-	-	-	-	-	-	-	-	-	EASTBOUND
0010				PROJECT	-	-	-	-	-	-	6,036	261	-	-	
				TOTAL 0010	60,363	2,611	12	12	4	50	6,036	261	40	23	

3

3

650- Staking Items

CATEGORY	LOCATION	650.5500 CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER LF	650.6000 CONSTRUCTION STAKING PIPE CULVERTS EACH	650.8000 CONSTRUCTION STAKING RESURFACING REFERENCE LF
0010	PROJECT	6640	2	14943
	TOTAL 0010	6,640	2	14,943

690- SAWING

CATEGORY	STATION	TO	STATION	LOCATION	690.0150 SAWING ASPHALT LF	690.0250 SAWING CONCRETE LF	REMARKS
0010	732+00	-	883+36.92		360		INTERSECTIONS
0010	814+00	-	814+30		160	108	RAILROAD CROSSING
				TOTAL 0010	520	108	

5

5

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	PLAN	SHEET 50 E

FILE NAME : N:\PDS\C3D\15950903\SHEETSPLAN\050501-PP.DWG
LAYOUT NAME - 050501- 1

PLOT DATE : 11/1/2022 4:11 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

5 5

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	PLAN	SHEET 51 E

FILE NAME : N:\PDS\C3D\15950903\SHEETSPLAN\050501-PP.DWG
LAYOUT NAME - 050501- 2

PLOT DATE : 11/1/2022 4:13 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

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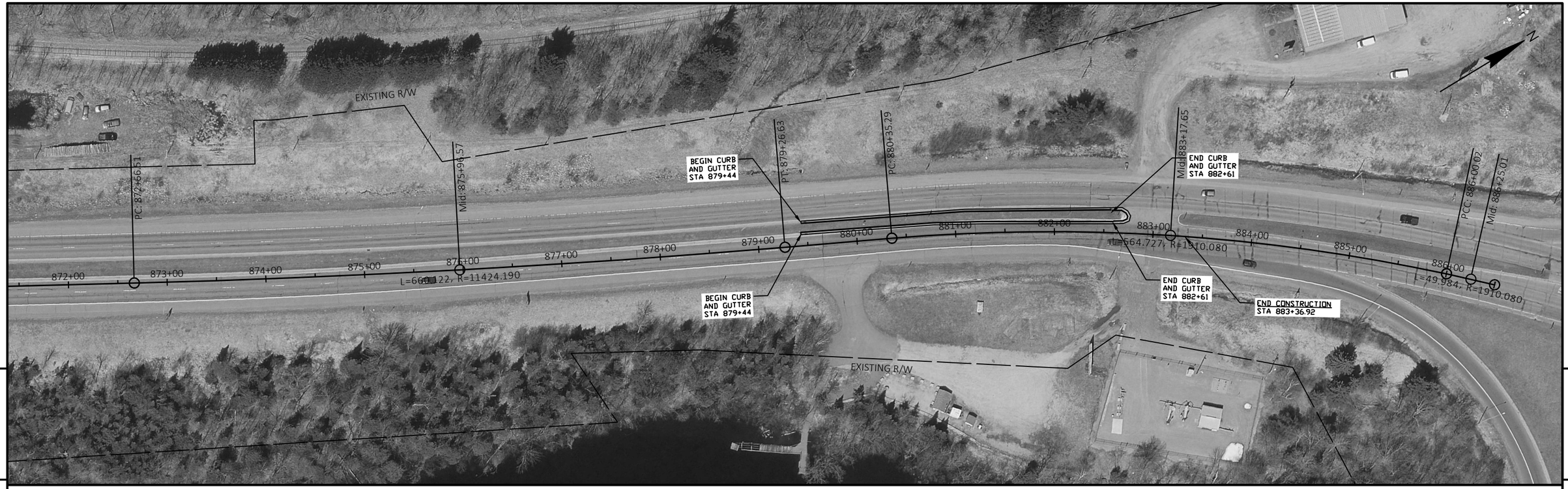
5

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	PLAN	SHEET 52	E

FILE NAME : N:\PDS\C3D\15950903\SHEETSPLAN\050501-PP.DWG
LAYOUT NAME - 050501- 3

PLOT DATE : 11/1/2022 4:15 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44



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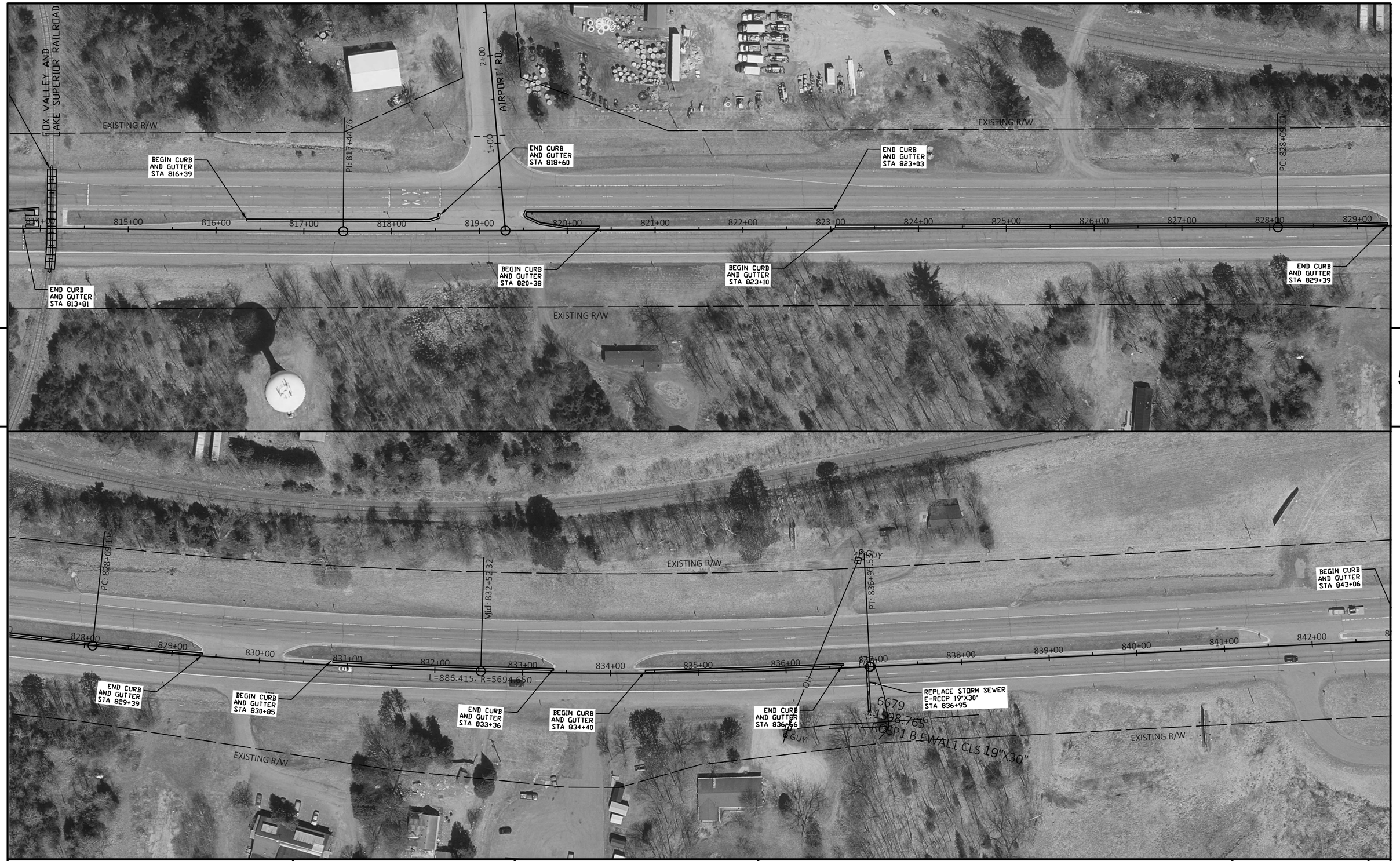


PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	PLAN	SHEET 54 E

FILE NAME : \\RHIRTOPFLPPI01\N3PUBLIC\PDS\C3D\15950903\SHEETSPLAN\050501-PP.DWG
LAYOUT NAME - 050501- 4

PLOT DATE : 1/18/2023 8:49 AM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

5

5

PROJECT NO: 1595-09-73	HWY: USH 8	COUNTY: ONEIDA	PLAN	SHEET 55	E

FILE NAME : N:\PDS\C3D\15950903\SHEETSPLAN\050501-PP.DWG
LAYOUT NAME - 050501- 5

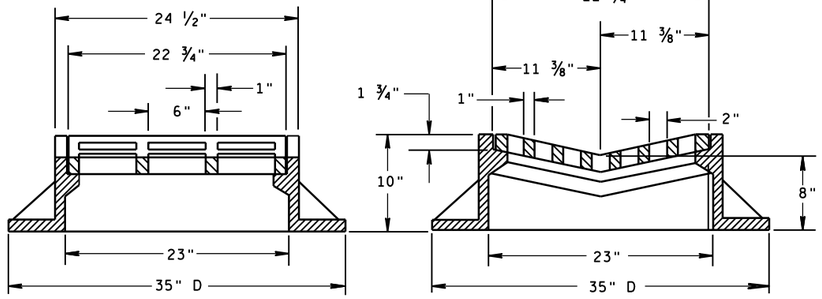
PLOT DATE : 11/1/2022 4:18 PM PLOT BY : DELORIA, TODD MICHAE PLOT NAME : PLOT SCALE : 1 IN:100 FT

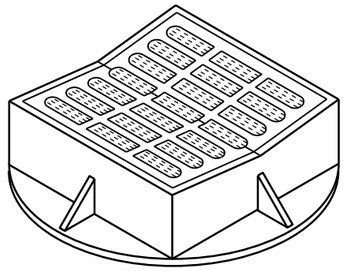
WISDOT/CADDS SHEET 44

Standard Detail Drawing List

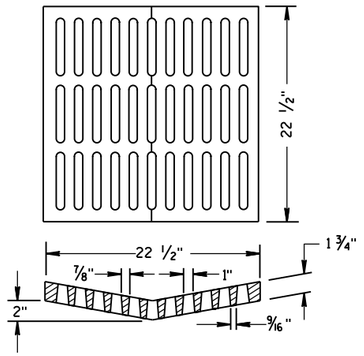
08A05-19B	INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM
08C09-02	INLETS MEDIAN 3 AND 4 GRATE
08D01-22A	CONCRETE CURB & GUTTER
08D01-22B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08F01-11	APRON ENDWALLS FOR CULVERT PIPE
08F02-01	APRON ENDWALLS FOR PIPE ARCH AND ELLIPTICAL PIPE
08F04-08	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
08F08-02	STEEL APRON ENDWALLS FOR CULVERT PIPE AND PIPE ARCH SLOPED CROSS DRAINS
13B01-10	PAVEMENT DETAILS FOR RAILROAD APPROACH
13C19-03	HMA LONGITUDINAL JOINTS
15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-08C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C02-08F	ADVANCED WIDTH RESTRICTION SIGNING
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C07-15B	PAVEMENT MARKING WORDS
15C07-15C	PAVEMENT MARKING ARROWS
15C08-22A	LONGITUDINAL MARKING (MAINLINE)
15C08-22B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C08-22C	PAVEMENT MARKING (TURN LANES)
15C08-22D	PAVEMENT MARKING (TURN LANES)
15C09-12A	SIGNING AND PAVEMENT MARKING DETAILS FOR RAILROAD-HIGHWAY GRADE CROSSINGS
15C09-12B	TRUCK STOPPING LANE PAVEMENT MARKINGS
15C11-09A	CHANNELIZING DEVICES FLEXIBLE TUBULAR MARKER POST
15C11-09B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C18-06B	MEDIAN ISLAND MARKING MEDIAN ISLAND NOSE
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15D06-05	TRAFFIC CONTROL, TWO LANE TWO WAY OPERATION
15D11-08	TRAFFIC CONTROL, SINGLE LANE CROSSOVER
15D20-06A	TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY
15D21-07A	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE
15D21-07B	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE
15D22-05	TRAFFIC CONTROL, TWO LANE CLOSURE, NON-FREEWAY/EXPRESSWAY
15D40-04A	TRAFFIC CONTROL, FULL LANE SHIFT NON-FREEWAY OR MULTILANE DIVIDED 45 MPH AND UNDER
15D40-04C	TRAFFIC CONTROL, PARTIAL LANE SHIFT NON-FREEWAY OR MULTILANE DIVIDED 45 MPH AND UNDER
15D44-02	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES
15D51-01	TRAFFIC CONTROL, MOBILE OPERATIONS ON AN UNDIVIDED ROADWAY

6



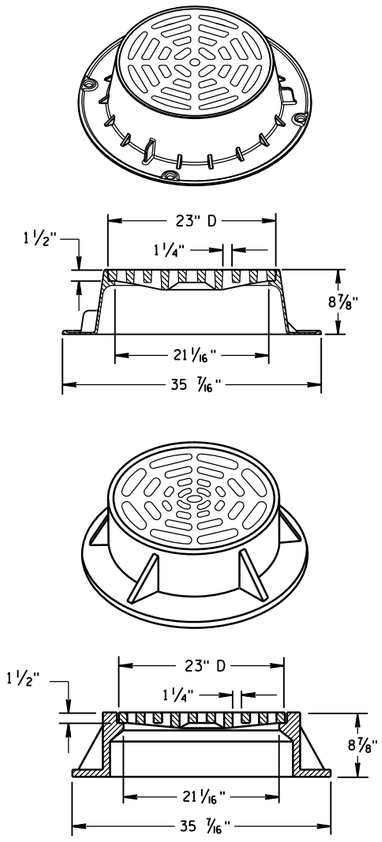


TYPE "B"



ALTERNATIVE GRATE FOR TYPE "B" COVER

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS POSSIBLE.
NOTED AS TYPE B-A ON THE DRAINAGE TABLE



TYPE "C"

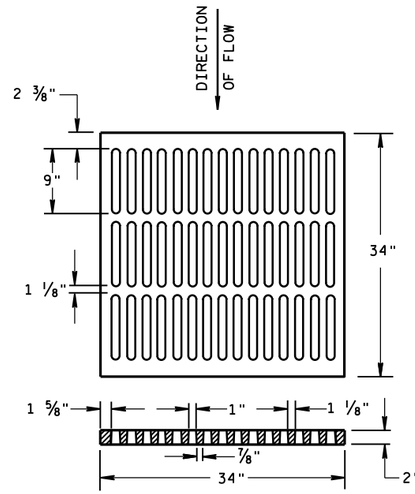
NOTE: EITHER CASTING IS ACCEPTABLE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

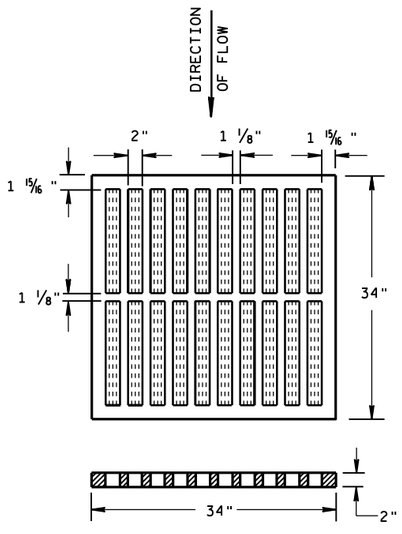
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



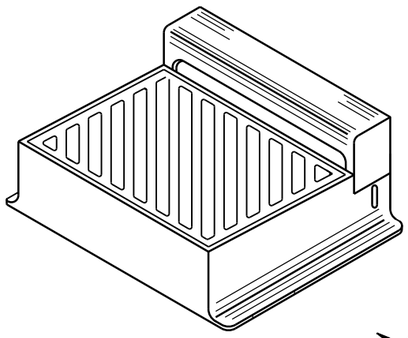
ALTERNATIVE TYPE "MS"

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS PERMITTED
NOTED AS TYPE MS-A ON THE DRAINAGE TABLE

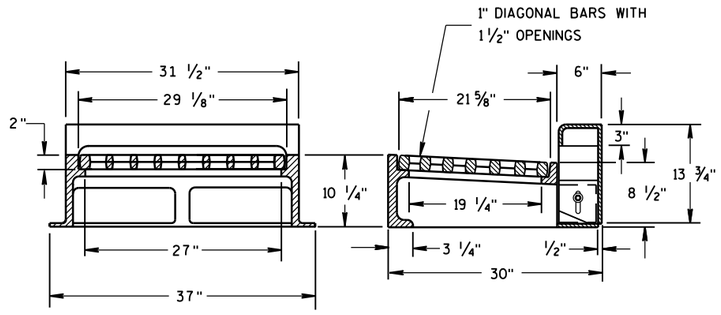


TYPE "MS"

USE ON FREEWAYS AND EXPRESSWAYS
NOTED AS TYPE MS ON DRAINAGE TABLE



DIAGONAL SLOTS, SHALL BE ORIENTED TO THE DIRECTION OF FLOW AS ILLUSTRATED. GRATES ARE MANUFACTURED TO BE REVERSIBLE.



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

TYPE "WM"

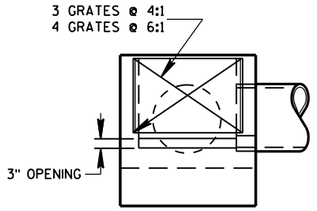
INLET COVERS
TYPE B, B-A, C,
MS, MS-A, & WM

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

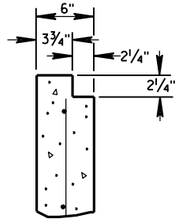
APPROVED
11/27/2013 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
FHWA

6

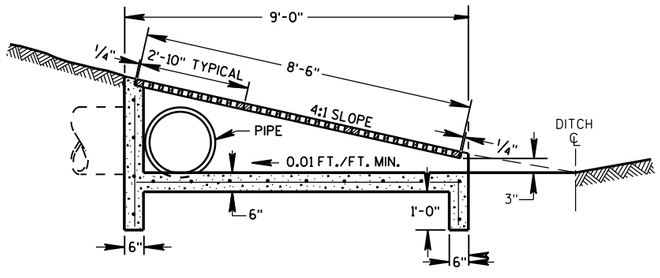
S.D.D. 8 A 5-19b



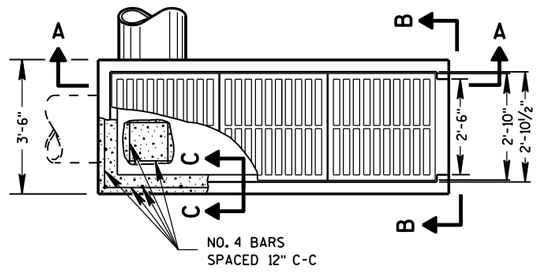
SECTION B-B



SECTION C-C

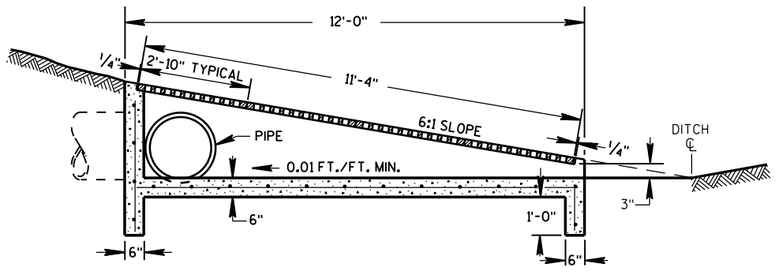


SECTION A-A

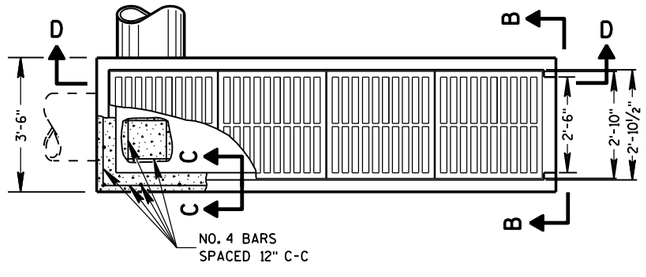


PLAN VIEW

INLETS MEDIAN 3 GRATE



SECTION D-D



PLAN VIEW

INLETS MEDIAN 4 GRATE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR INLETS WHICH MAY INCLUDE PRECAST REINFORCED CONCRETE INLETS, SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL MEDIAN INLETS ARE DESIGNATED ON THE PLANS AS "INLETS, 3G-MS", ETC. THE FIRST NUMBER AND LETTER DESIGNATE THE TYPE OF STRUCTURE, AND THE FOLLOWING LETTERS DESIGNATE THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

INLETS MEDIAN 3 AND 4 GRATE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept., 2016 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS UNIT SUPERVISOR
FHWA

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S.D.D. 8 C 9-2

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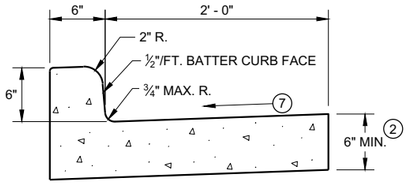
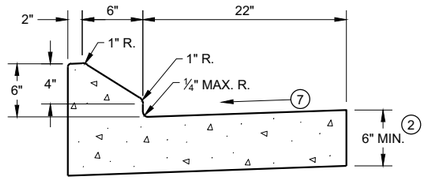
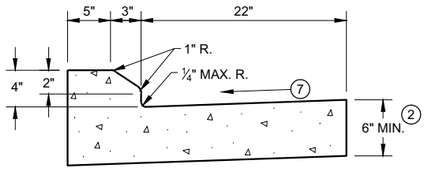
S.D.D. 8 C 9-2



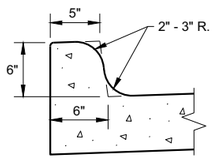
TYPES A (1) & D



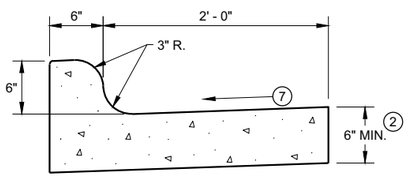
6" SLOPED CURB TYPES G (1) & J



4" SLOPED CURB TYPES G (1) & J

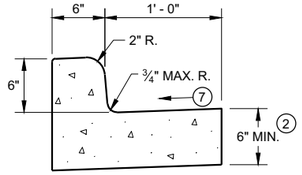


TYPES K (1) & L
(OPTIONAL CURB SHAPE)



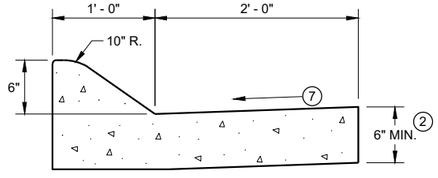
TYPES K (1) & L

CONCRETE CURB AND GUTTER 30"



TYPES A (1) & D

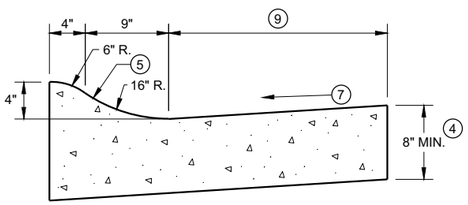
CONCRETE CURB AND GUTTER 18"



6" SLOPED CURB TYPES A (1) & D

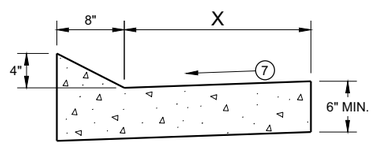
4" SLOPED CURB TYPES A (1) & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R (1) & T

TBT & TBTT	X
30"	22"
36"	28"

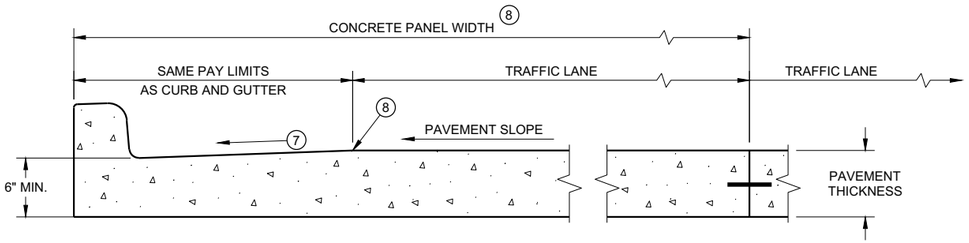


TYPES TBT & TBTT (1)

CONCRETE CURB AND GUTTER

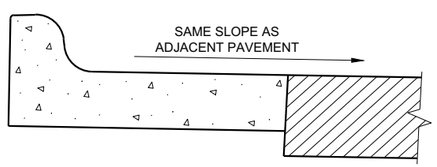
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT *
WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER (6)
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.

(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(3) USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.

(4) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(5) UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.

(6) WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.

(7) USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.

(8) INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.

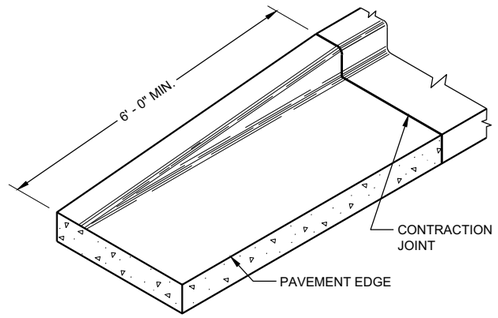
(9) CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES

CONCRETE CURB AND GUTTER

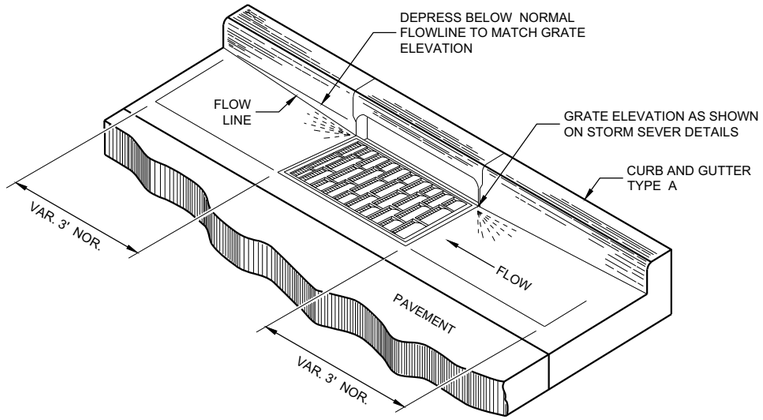
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 08D01 - 22a
SDD 08D01 - 22a



END SECTIONCURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS
(TYPICAL H INLET COVER SHOWN)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

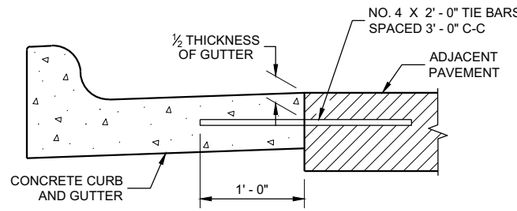
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

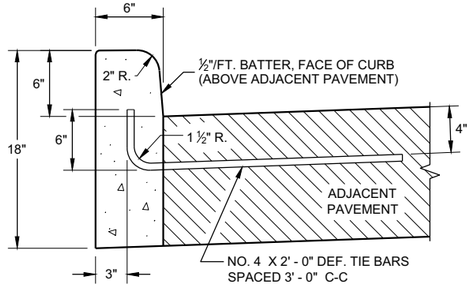
(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.

(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

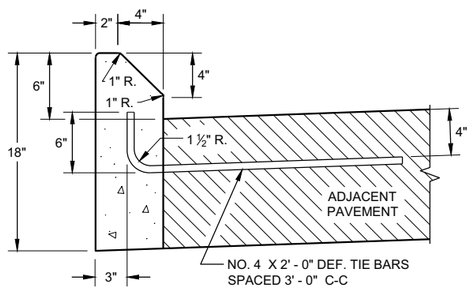
(9) REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.



TYPICAL TIE BAR LOCATION (1)

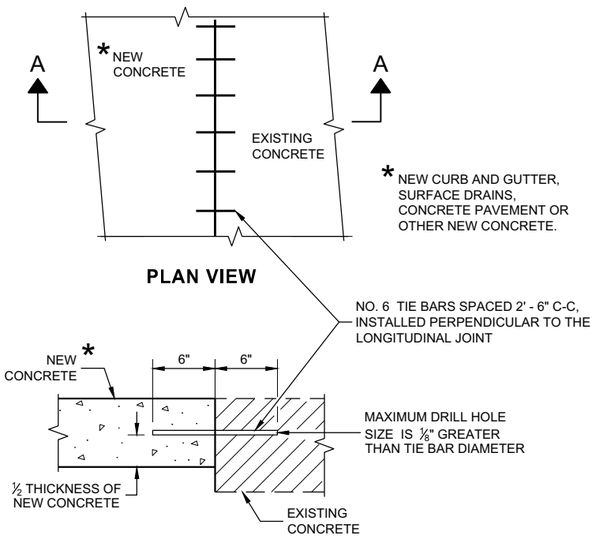


TYPES A (1) & D



TYPES G (1) & J

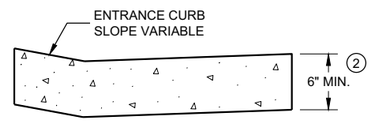
CONCRETE CURB



PLAN VIEW

SECTION A - A

TIE BARS DRILLED INTO EXISTING PAVEMENT



DRIVEWAY ENTRANCE CURB (9)
(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 — DATE
/S/ Rodnery Taylor — ROADWAY STANDARDS DEVE ENGINEER

FHWA

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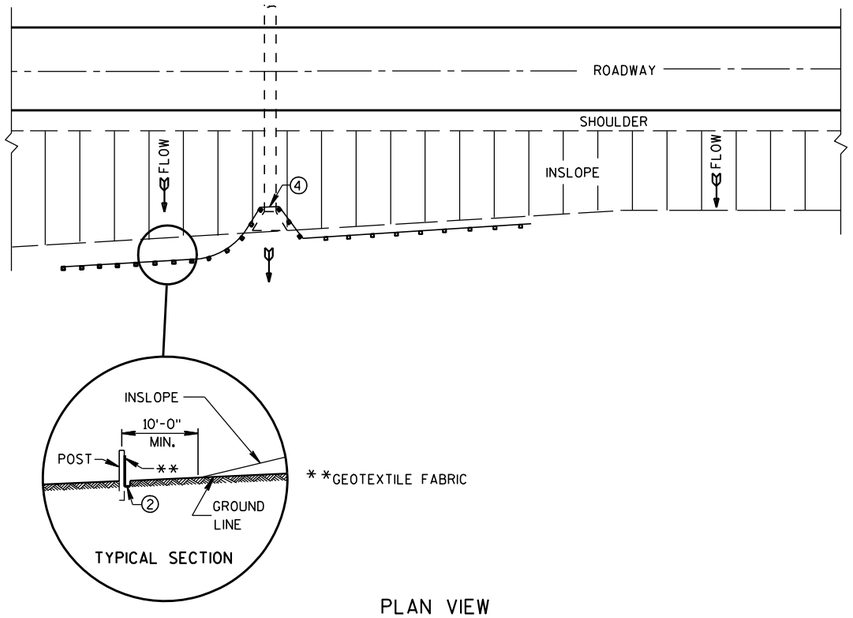
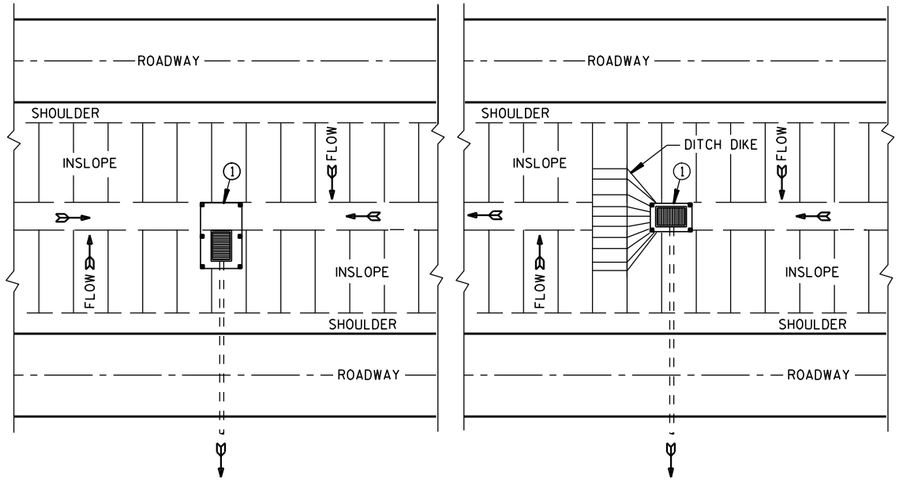
SDD08D01 - 22b



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



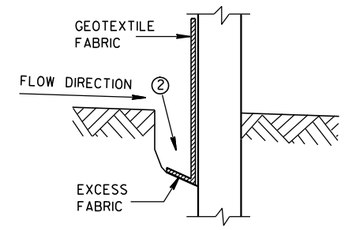
PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

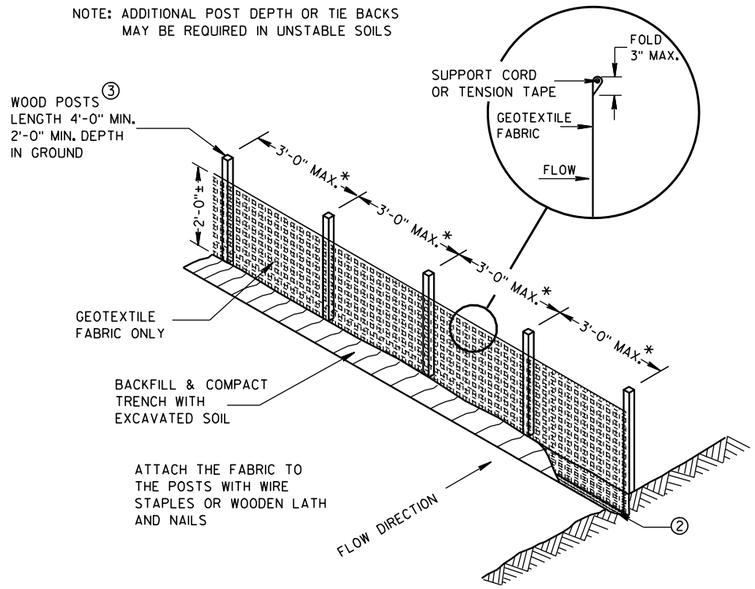
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

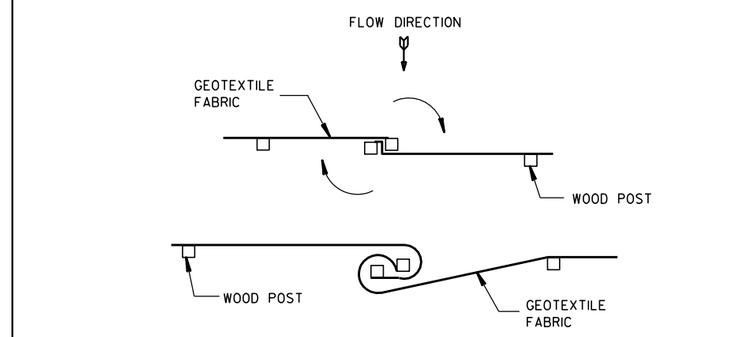
1. HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
2. FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
3. WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.
4. SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
5. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



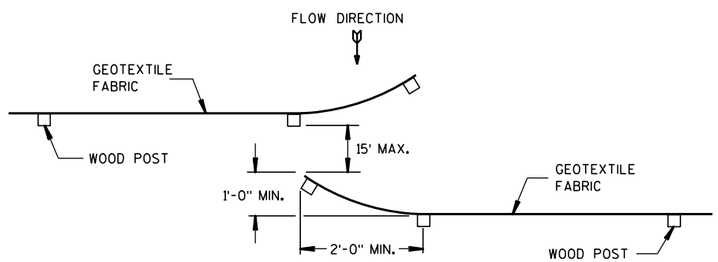
TRENCH DETAIL



SILT FENCE

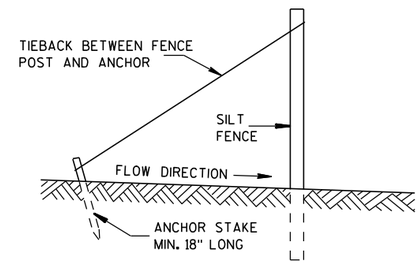


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE 5



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6

S.D.D. 8 E 9-6

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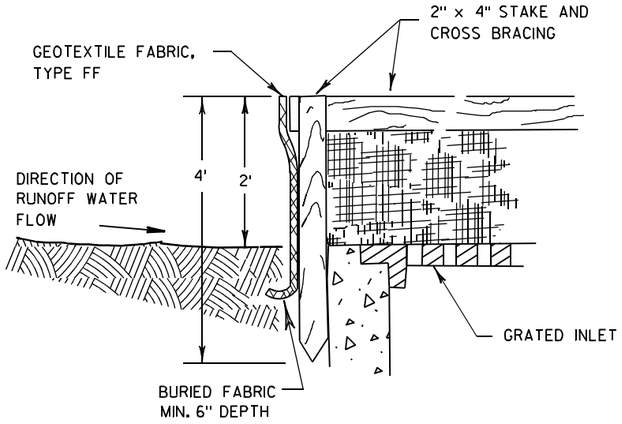
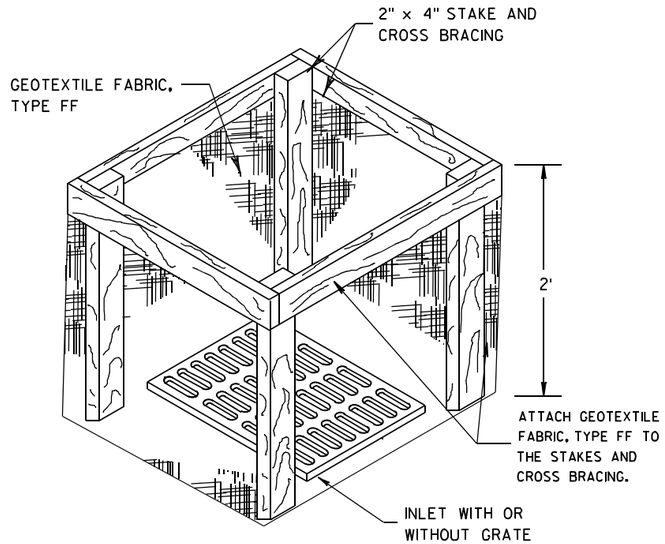
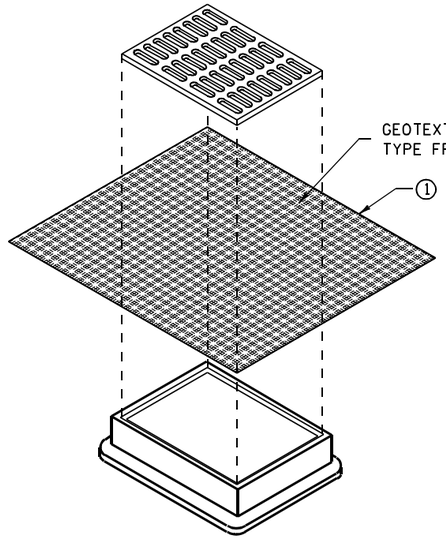
S.D.D. 8 E 9-6



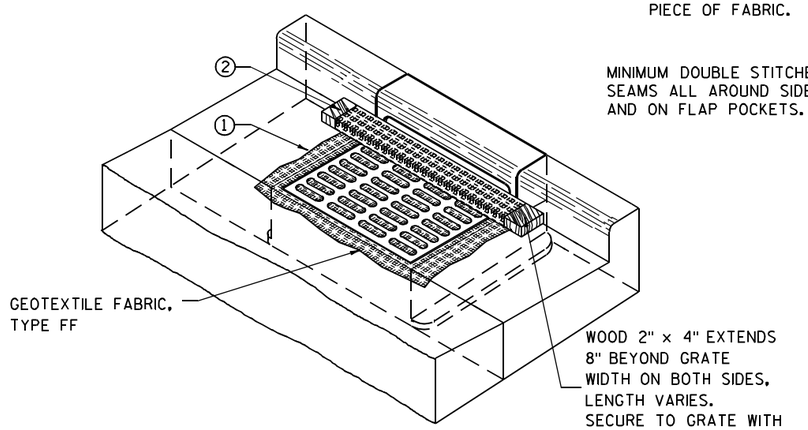


INLET PROTECTION, TYPE A

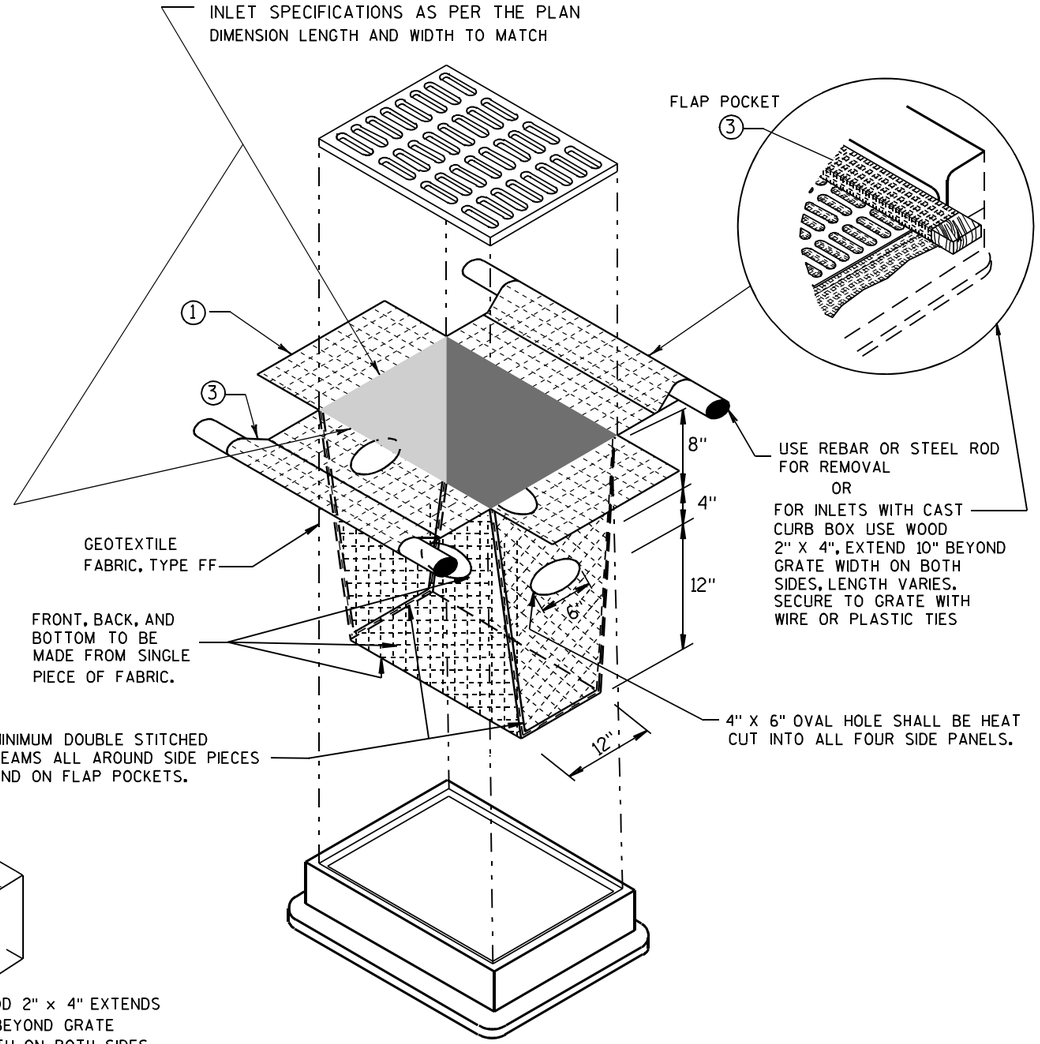


INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH
OR WITHOUT A CURB BOX AS PER NOTE ②)

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.

③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION
TYPE A, B, C, AND D

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

10/16/02
DATE

/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA

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S.D.D. 8 E 10-2

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S.D.D. 8 E 10-2

METAL APRON ENDWALLS

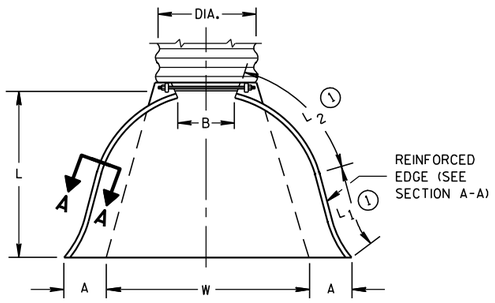
PIPE DIA. (IN.)	MIN. THICK. (Inches) STEEL	MIN. THICK. (Inches) ALUM.	DIMENSIONS (Inches) A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 ①	L2 ①	W (±2")	APPROX. SLOPE	BODY
12	.064	.060	6	6	6	21	12	17 1/2	24	2 1/2 to 1	1 Pc.
15	.064	.060	7	8	6	26	14	21 3/4	30	2 1/2 to 1	1 Pc.
18	.064	.060	8	10	6	31	15	28 1/4	36	2 1/2 to 1	1 Pc.
21	.064	.060	9	12	6	36	18	29 5/8	42	2 1/2 to 1	1 Pc.
24	.064	.075	10	13	6	41	18	37 1/4	48	2 1/2 to 1	1 Pc.
30	.079	.075	12	16	8	51	18	52 1/4	60	2 1/2 to 1	1 Pc.
36	.079	.105	14	19	9	60	24	59 3/4	72	2 1/2 to 1	2 Pc.
42	.109	.105	16	22	11	69	24	75 5/8	84	2 1/2 to 1	2 Pc.
48	.109	.105	18	27	12	78	24	81	90	2 1/4 to 1	3 Pc.
54	.109	.105	18	30	12	84	30	85 1/2	102	2 1/4 to 1	3 Pc.
60	.109×	.105×	18	33	12	87	—	—	114	2 to 1	3 Pc.
66	.109×	.105×	18	36	12	87	—	—	120	2 to 1	3 Pc.
72	.109×	.105×	18	39	12	87	—	—	126	2 to 1	3 Pc.
78	.109×	.105×	18	42	12	87	—	—	132	1 1/2 to 1	3 Pc.
84	.109×	.105×	18	45	12	87	—	—	138	1 1/2 to 1	3 Pc.
90	.109×	.105×	18	37	12	87	—	—	144	1 1/2 to 1	3 Pc.
96	.109×	.105×	18	35	12	87	—	—	150	1 1/2 to 1	3 Pc.

× EXCEPT CENTER PANEL SEE GENERAL NOTES

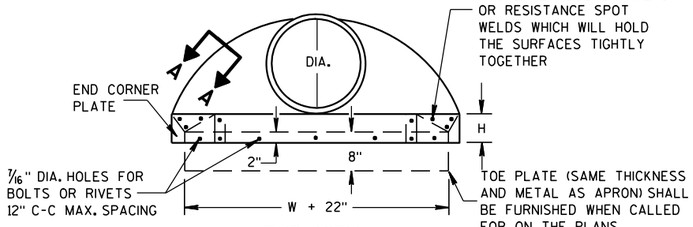
REINFORCED CONCRETE APRON ENDWALLS

PIPE DIA. (IN.)	DIMENSIONS (Inches) T	A	B	C	D	E	G	APPROX. SLOPE
12	2	4	24	48 7/8	72 1/8	24	2	3 to 1
15	2 1/4	6	27	46	73	30	2 1/4	3 to 1
18	2 1/2	9	27	46	73	36	2 1/2	3 to 1
21	2 3/4	9	36	37 1/2	73 1/2	42	2 3/4	3 to 1
24	3	9 1/2	43 1/2	30	73 1/2	48	3	3 to 1
27	3 1/4	10 1/2	49 1/2	24	73 1/2	54	3 1/4	3 to 1
30	3 1/2	12	54	19 3/4	73 1/2	60	3 1/2	3 to 1
36	4	15	63	34 3/4	97 3/4	72	4	3 to 1
42	4 1/2	21	63	35	98	78	4 1/2	3 to 1
48	5	24	72	26	98	84	5	3 to 1
54	5 1/2	27	65	* 33 1/4 - ** 35	* 98 1/4 - ** 100	90	5 1/2	2 2/5 to 1
60	6	* 30 - ** 35	60	39	99	96	5	2 to 1
66	6 1/2	* 24 - ** 30	* 72 - ** 78	* 21 - ** 27	99	102	5 1/2	2 to 1
72	7	* 24 - ** 36	78	21	99	108	6	2 to 1
78	7 1/2	* 24 - ** 36	78	21	99	114	6 1/2	2 to 1
84	8	36	90 1/2	21	111 1/2	120	6 1/2	1 1/2 to 1
90	8 1/2	41	87 1/2	24	111 1/2	132	6 1/2	1 1/2 to 1

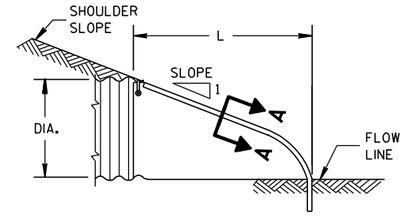
*MINIMUM
**MAXIMUM



PLAN VIEW

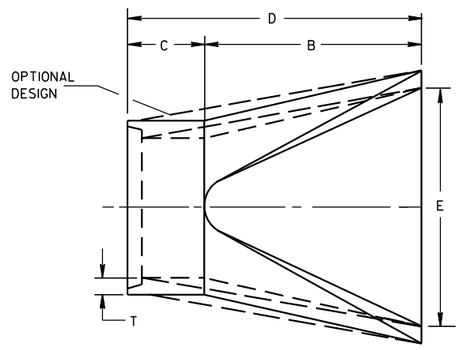


END VIEW

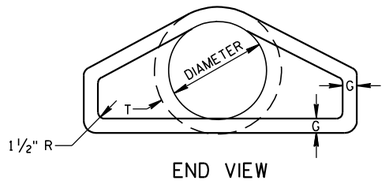


SIDE ELEVATION

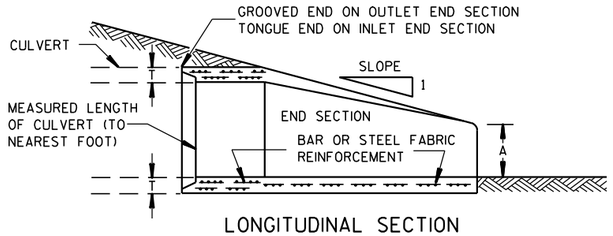
METAL ENDWALLS



PLAN

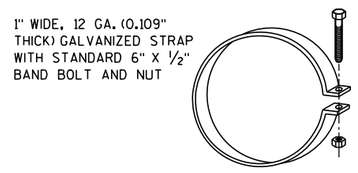


END VIEW



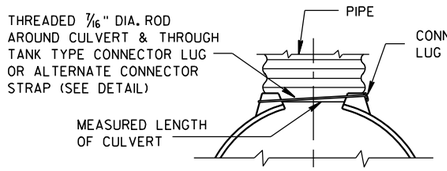
LONGITUDINAL SECTION

CONCRETE ENDWALLS

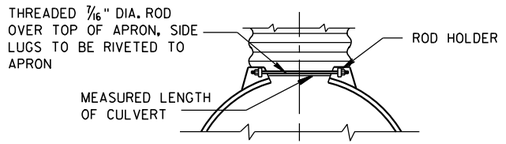


ALTERNATE FOR TYPE 1 CONNECTION

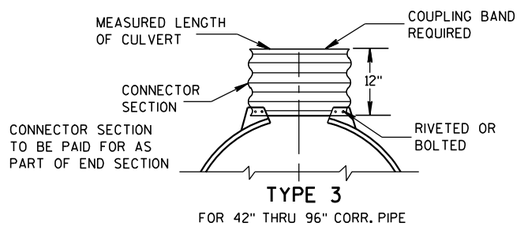
END SECTION CONNECTOR STRAP



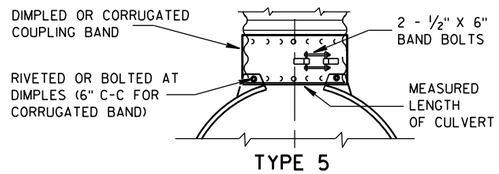
TYPE 1
FOR 12" THRU 24" CORR. PIPE



TYPE 2
FOR 30" THRU 96" CORR. PIPE



TYPE 3
FOR 42" THRU 96" CORR. PIPE



TYPE 5
ALTERNATE FOR:
ALL SIZES CORRUGATED CIRCULAR PIPE

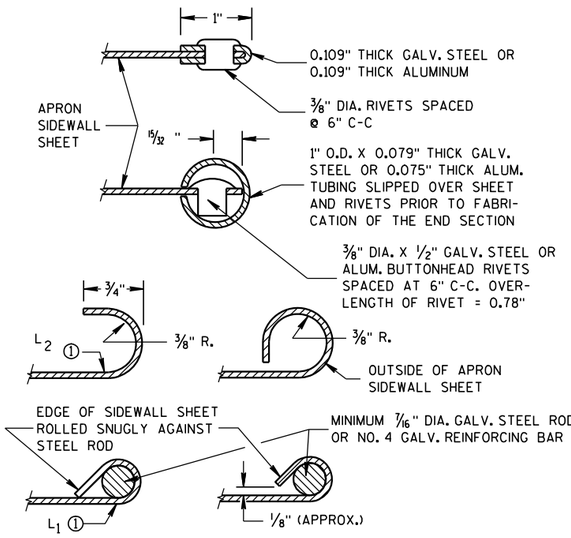
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL. AND CORRUGATED BAND FITS INSIDE ENDWALL. DIMPLED BAND MAY BE USED WITH HELICALLY CORRUGATED PIPE.

FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5 AS APPLICABLE.

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VISE VERSA. GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

① FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

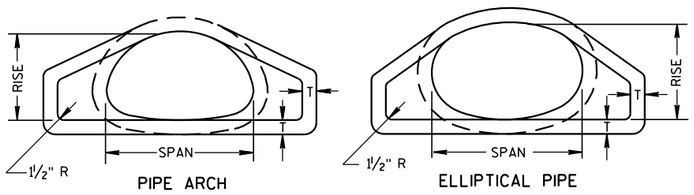
APRON ENDWALLS FOR CULVERT PIPE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

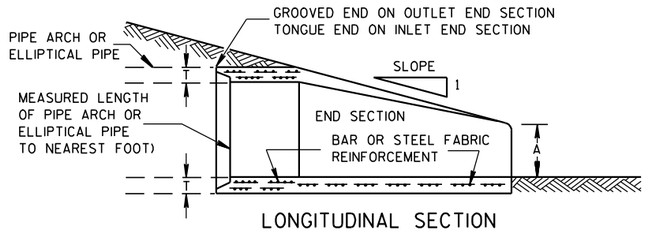
APPROVED
11/30/94 DATE
/S/ Rory L. Rhinesmith
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6

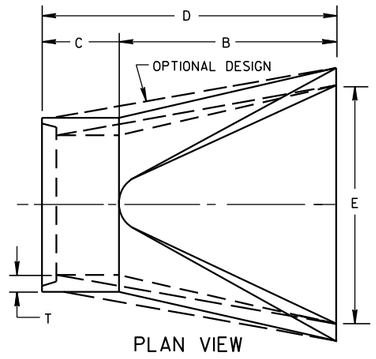
S.D.D. 8 F 1-11



END VIEW

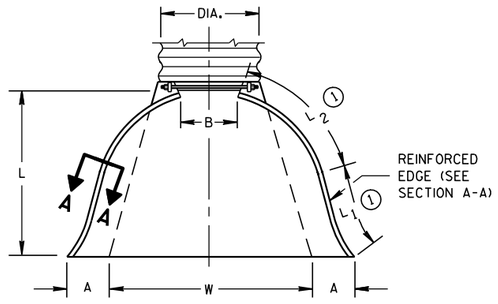


LONGITUDINAL SECTION

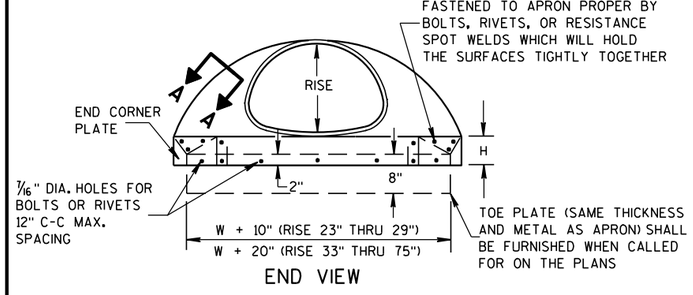


PLAN VIEW

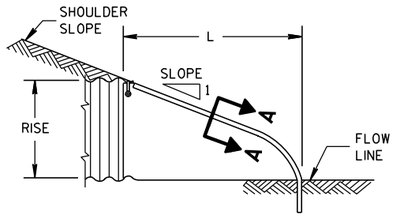
CONCRETE ENDWALLS



PLAN VIEW

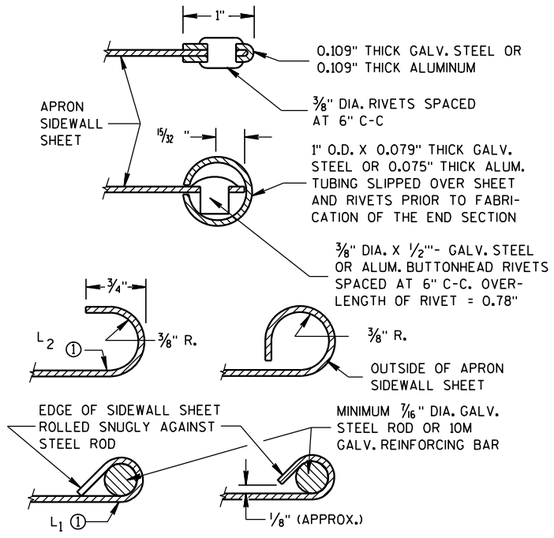


END VIEW



SIDE ELEVATION

METAL ENDWALLS



SECTION A-A

2- 2/3" X 1/2" CORRUGATIONS

EQUIV. DIA. (Inches)	(Inches) SPAN	(Inches) RISE	MIN. THICK. (Inches) STEEL	MIN. THICK. (Inches) ALUM.	DIMENSIONS (Inches) A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 ①	L2 ①	W (±2")	APPROX. SLOPE	BODY
15	17	13	.064	.060	7	9	6	19	14	16	30	2½ to 1	1 Pc.
18	21	15	.064	.060	7	10	6	23	14	19⅜	36	2½ to 1	1 Pc.
21	24	18	.064	.060	8	12	6	28	18	21¾	42	2½ to 1	1 Pc.
24	28	20	.064	.060	9	14	6	32	18	27½	48	2½ to 1	1 Pc.
30	35	24	.079	.075	10	16	6	39	18	37⅝	60	2½ to 1	1 Pc.
36	42	29	.079	.075	12	18	8	46	24	45⅜	75	2½ to 1	1 Pc.
42	49	33	.109	.105	13	21	9	53	24	54¾	85	2½ to 1	2 Pc.
48	57	38	.109	.105	18	26	12	63	24	68	90	2½ to 1	3 Pc.
54	64	43	.109	.105	18	30	12	70	24	72¾	102	2¼ to 1	3 Pc.
60	71	47	.109*	.105*	18	33	12	77	30	82¼	114	2¼ to 1	3 Pc.
66	77	52	.109*	.105*	18	36	12	77	—	—	126	2 to 1	3 Pc.
72	83	57	.109*	.105*	18	39	12	77	—	—	138	2 to 1	3 Pc.

3" X 1" CORRUGATIONS

EQUIV. DIA. (Inches)	(Inches) SPAN	(Inches) RISE	MIN. THICK. (Inches) STEEL	MIN. THICK. (Inches) ALUM.	DIMENSIONS (Inches) A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 ①	L2 ①	W (±2")	APPROX. SLOPE	BODY
48	53	41	.109	.105	18	26	12	63	24	72¾	90	2½ to 1	2 Pc.
54	60	46	.109	.105	18	30	12	70	30	82¼	102	2 to 1	2 Pc.
60	66	51	.109*	.105*	18	33	12	77	—	—	114	1½ to 1	3 Pc.
66	73	55	.109*	.105*	18	36	12	77	—	—	126	1½ to 1	3 Pc.
72	81	59	.109*	.105*	18	39	12	77	—	—	138	2 to 1	3 Pc.
78	87	63	.109*	.105*	22	38	12	77	—	—	148	1½ to 1	3 Pc.
84	95	67	.109*	.105*	22	34	12	77	—	—	162	1½ to 1	3 Pc.
90	103	71	.109*	.105*	22	38	12	77	—	—	174	1½ to 1	3 Pc.
96	112	75	.109*	.105*	24	40	12	77	—	—	174	1½ to 1	3 Pc.

NOTE: ALL SPLICES TO BE LAP RIVETED OR BOLTED.

* EXCEPT CENTER PANEL SEE GENERAL NOTES

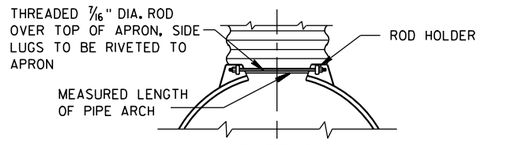
REINFORCED CONCRETE PIPE ARCH

EQUIV. DIA. (Inches)	DIMENSIONS (Inches) ** SPAN	** RISE	T	A	B	C	D	E	APPROX. SLOPE
24	29	18	3	8½	39	33	72	48	3 to 1
30	36	22	3½	9½	50	46	96	60	3 to 1
36	44	27	4	11⅛	60	36	96	72	3 to 1
42	51	31	4½	15 13/16	60	36	96	78	3 to 1
48	58	36	5	21	60	36	96	84	3 to 1
54	65	40	5½	25½	60	36	96	90	3 to 1
60	73	45	6	31	60	36	96	96	3 to 1
72	88	54	7	31	60	39	99	120	2 to 1
84	102	62	8	28½	83	19	102	144	2 to 1

REINFORCED CONCRETE ELLIPTICAL PIPE

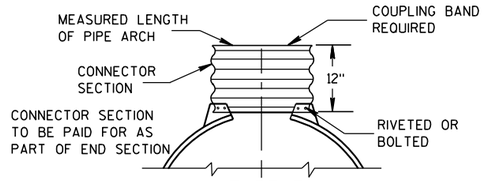
EQUIV. DIA. (Inches)	DIMENSIONS (Inches) ** SPAN	** RISE	T	A	B	C	D	E	APPROX. SLOPE
24	30	19	3¼	8½	39	33	72	48	3 to 1
30	38	24	3¾	9½	54	18	72	60	3 to 1
36	45	29	4½	11⅛	60	24	84	72	2½ to 1
42	53	34	5	15¾	60	36	96	78	2½ to 1
48	60	38	5½	21	60	36	96	84	2½ to 1
54	68	43	6	25½	60	36	96	90	2½ to 1
60	76	48	6½	30	60	36	96	96	2½ to 1

**NOMINAL SIZE



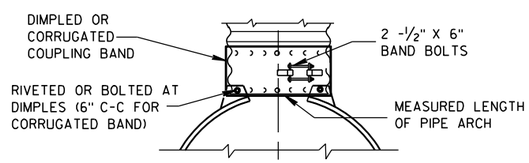
TYPE 2

FOR 17" X 13" THRU 112" X 75" PIPE ARCH



TYPE 3

FOR 64" X 43" THRU 112" X 75" PIPE ARCH



TYPE 5

ALTERNATE FOR:

ALL SIZES CORRUGATED PIPE ARCHES

NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL, AND CORRUGATED BAND FITS INSIDE ENDWALL.

CONNECTION DETAILS

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE APRON ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VISE VERSA. GALVANIZED STEEL OR ALUMINUM APRON ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 66" X 51" PIPE ARCH AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 66" X 51" PIPE ARCH AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE ARCH PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 77" X 52" THROUGH 112" X 75" APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

① FOR PIPE ARCH SIZES UP TO 73" X 55" A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

APRON ENDWALLS FOR PIPE ARCH AND ELLIPTICAL PIPE

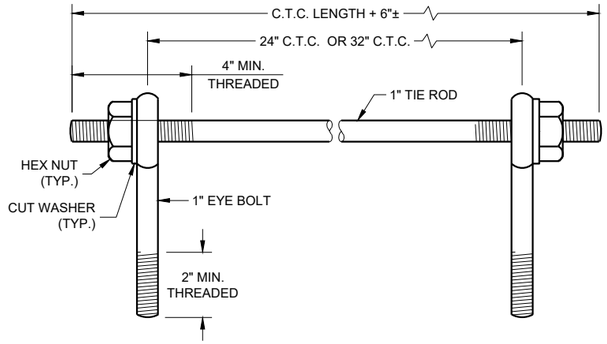
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

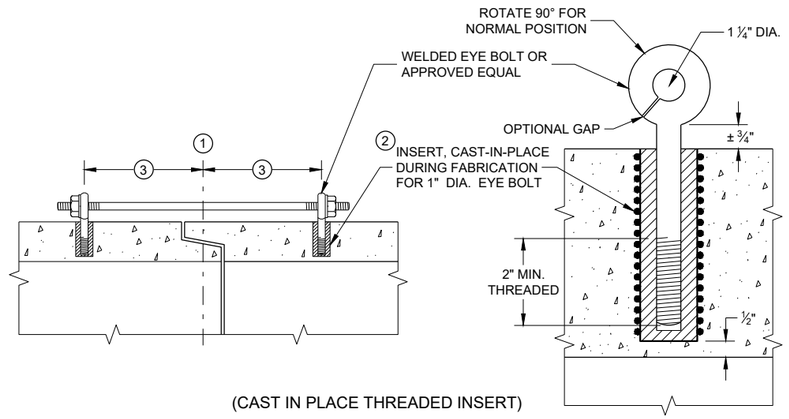
11/30/94 — DATE

/S/ Rory L. Rhinesmith — CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA



EYE BOLTS AND TIE ROD



(CAST IN PLACE THREADED INSERT)

LONGITUDINAL SECTIONS

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

(1) CENTER LINE OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.

(2) THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.

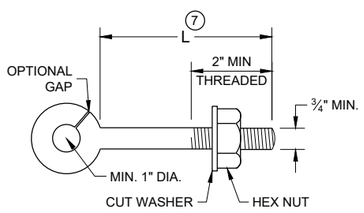
(3) HOLES SHALL BE CAST-IN-PLACE OR DRILLED PER THE APPLICABLE DETAIL, AND EQUAL DISTANCE FROM THE CENTERLINE OF THE JOINT.

(4) BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.

(5) OPENING TO BE ROD DIAMETER PLUS 1 INCH.

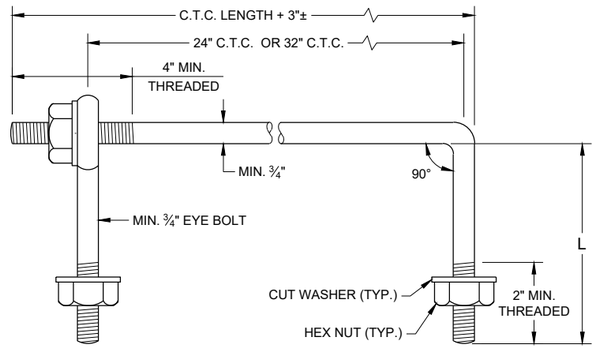
(6) LENGTH ADEQUATE TO EXTEND TO WITHIN ½ INCH OF THE INNER SURFACE OF THE PIPE.

(7) EYE BOLT LENGTH DETERMINED BY WALL THICKNESS, BELL THICKNESS AND BOLT PROJECTION INSIDE PIPE.

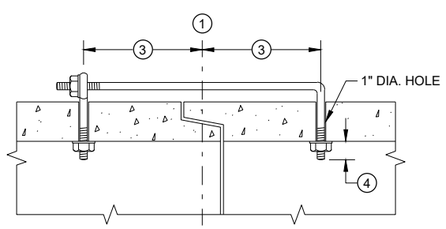


EYE BOLT (7)

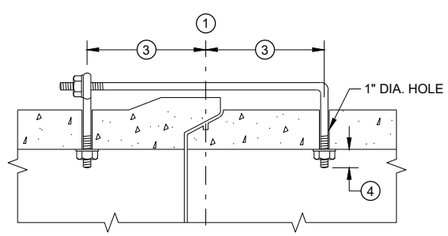
NOTE: TWO EYE BOLTS MAY BE USED WITH A 30" OR 38" LONG THREADED ROD IN LIEU OF THE 90° BENT TIE ROD.



EYE BOLT AND TIE ROD



(TONGUE AND GROOVE PIPE)



(MODIFIED BELL PIPE)

LONGITUDINAL SECTION

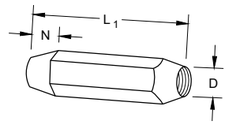
(JOINT TIES FOR 18" TO 66" DIA. CONCRETE PIPE)

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)

ADJUSTABLE TIE ROD TABLE

PIPE DIAMETER	TIE ROD DIAMETER	D	L_1	N
12 - 60	⅝	⅝	5	½
66 - 84	¾	¾	5	½
90 - 144	1	1	7	1 7/16

DIMENSIONS SHOWN ARE IN INCHES



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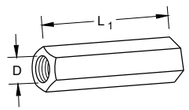
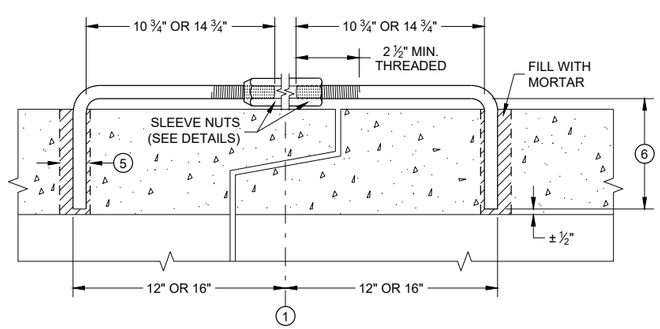


PLAIN

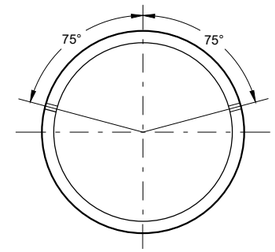
RIGHT AND LEFT THREADS

SLEEVE NUTS



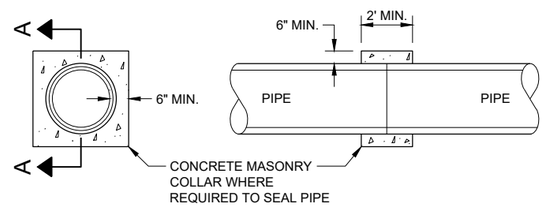
LONGITUDINAL SECTION

ADJUSTABLE TIE ROD (ALTERNATE NO. 3)



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION



SECTION A - A

CONCRETE COLLAR DETAIL

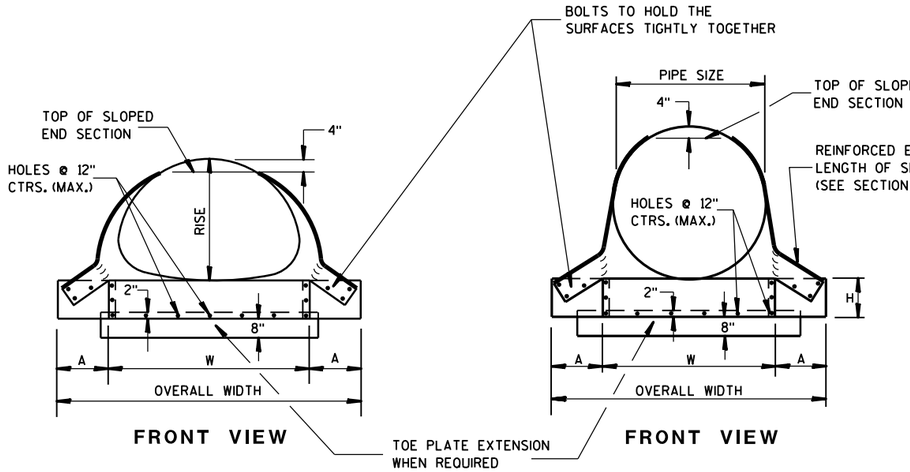
JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2021 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVE ENGINEER
FHWA

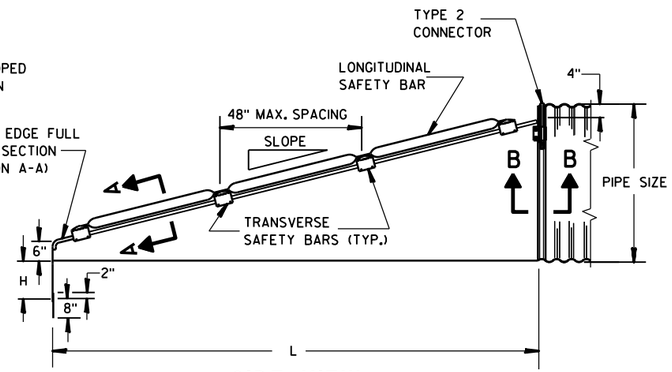
SDD 08F04 - 08

SDD 08F04 - 08



FRONT VIEW

FRONT VIEW



SIDE VIEW
(48" SIZE ILLUSTRATED)

GENERAL NOTES

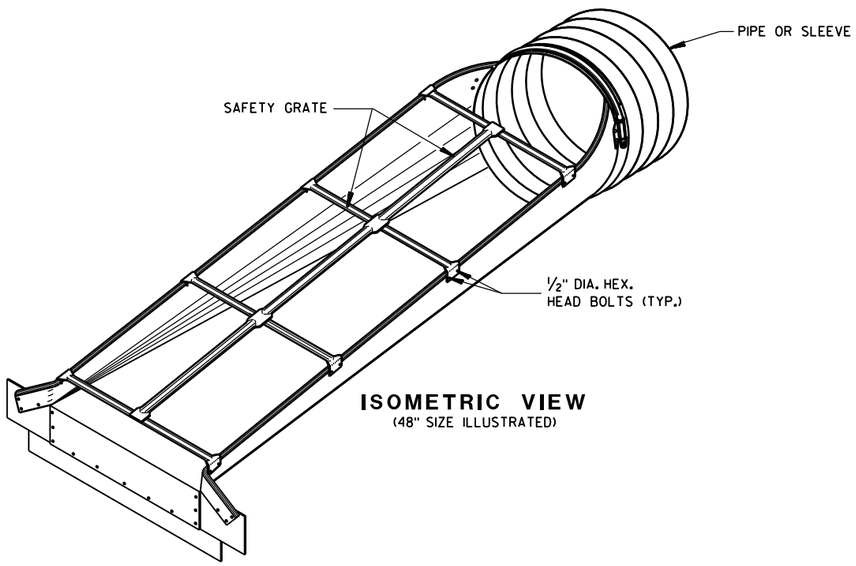
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

SAFETY GRATES SHALL BE FABRICATED FROM 3-INCH DIAMETER GALVANIZED PIPE MEETING THE REQUIREMENTS OF ASTM A-53, GRADE B, SCHEDULE 40 OR APPROVED EQUAL. THE LONGITUDINAL BAR SHALL BE WELDED TO THE TRANSVERSE BARS WHERE THE BARS CROSS. THE NUMBER OF TRANSVERSE BARS REQUIRED WILL VARY DEPENDING ON THE LENGTH OF THE END SECTION.

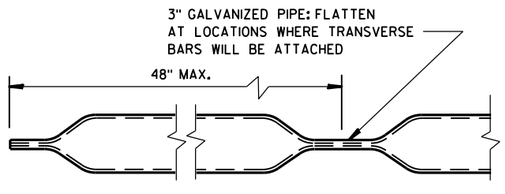
SLOPED STEEL ENDWALLS LOCATED AT THE ENDS OF CONCRETE CULVERT PIPE SHALL BE FURNISHED WITH STEEL ADAPTER SLEEVES.

STEEL APRON ENDWALLS FOR CULVERT PIPE CROSS DRAINS

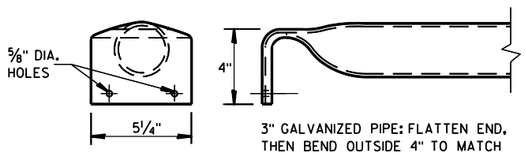
PIPE DIA. (IN.)	MIN. THICK.		DIMENSIONS (Inches)				L DIMENSIONS			
	IN.	GAGE	A	H	W	OVERALL WIDTH	SLOPE	LENGTH INCHES	SLOPE	LENGTH INCHES
36	.109	12	12	9	42	66	4:1	104	6:1	156
42	.109	12	16	12	48	80	4:1	128	6:1	192
48	.109	12	16	12	54	86	4:1	152	6:1	228
54	.109	12	16	12	60	92	4:1	176	6:1	264
60	.109	12	16	12	66	98	4:1	200	6:1	300



ISOMETRIC VIEW
(48" SIZE ILLUSTRATED)



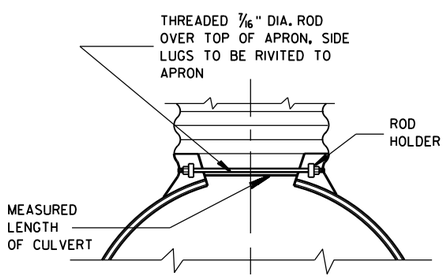
LONGITUDINAL SAFETY BAR



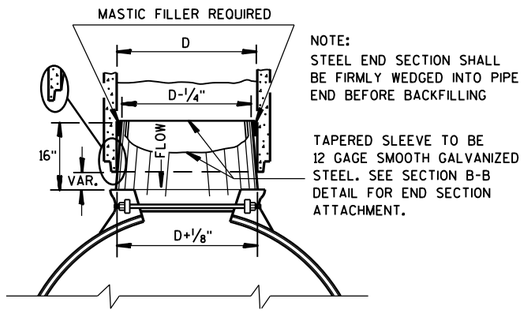
TRANSVERSE SAFETY BAR

STEEL APRON ENDWALLS FOR PIPE ARCH SLOPED CROSS DRAINS

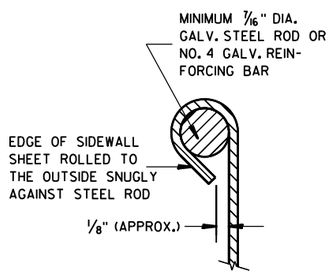
EQUIV. DIA. (IN.)	INCHES		MIN. THICK.		DIMENSIONS (Inches)				L DIMENSIONS			
	SPAN	RISE	IN.	GAGE	A	H	W	OVERALL WIDTH	SLOPE	LENGTH INCHES	SLOPE	LENGTH INCHES
30	35	24	.079	14	12	9	41	65	4:1	56	6:1	84
36	42	29	.109	12	12	9	48	72	4:1	76	6:1	114
42	49	33	.109	12	16	12	55	87	4:1	92	6:1	138
48	57	38	.109	12	16	12	63	95	4:1	112	6:1	168
54	64	43	.109	12	16	12	70	102	4:1	132	6:1	198
60	71	47	.109	12	16	12	77	109	4:1	148	6:1	222



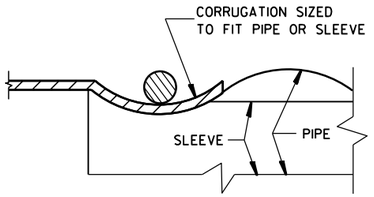
TYPE 2 CONNECTOR DETAIL



STEEL ADAPTER SLEEVE FOR CONCRETE PIPE



SECTION A-A



SECTION B-B

STEEL APRON ENDWALLS FOR CULVERT PIPE AND PIPE ARCH SLOPED CROSS DRAINS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

6/5/2012 — DATE

/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER

FHWA

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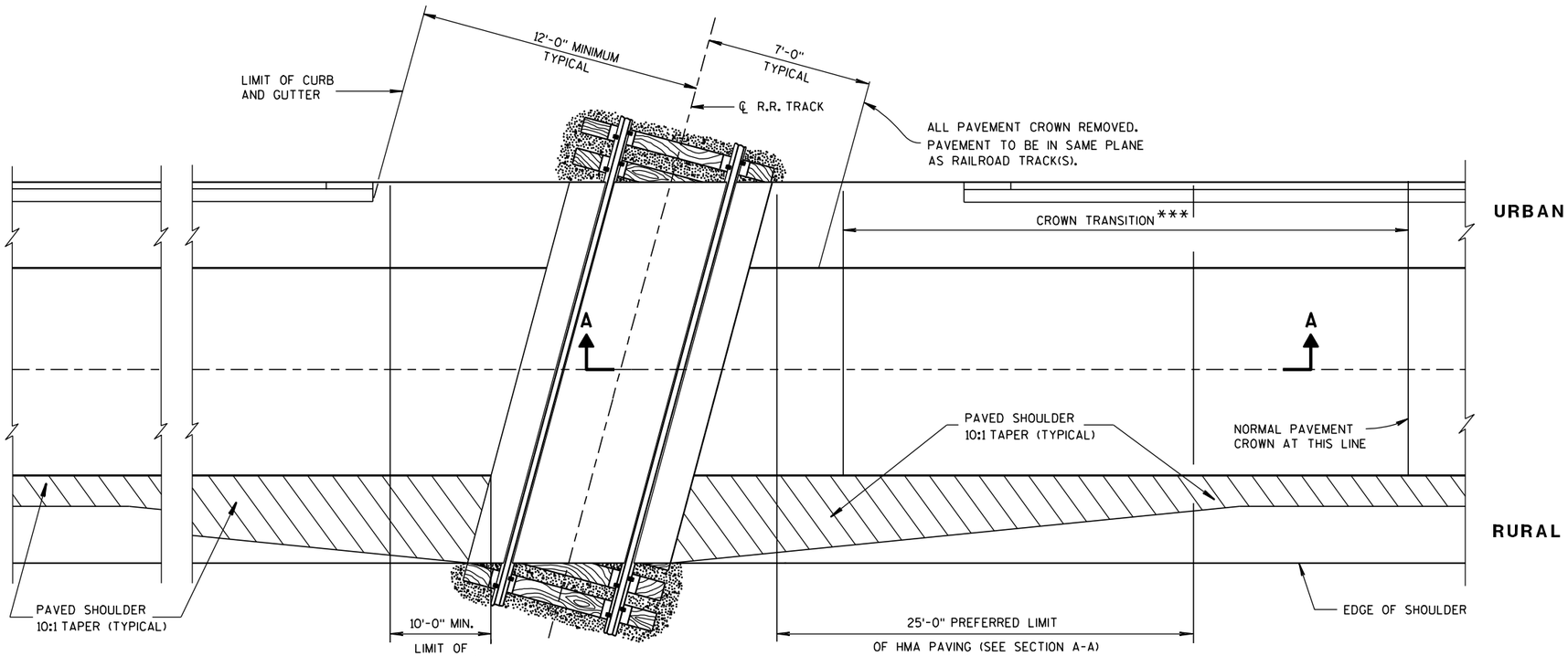
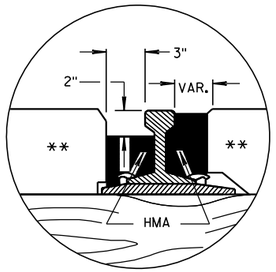
S.D.D. 8 F 8-2





DETAIL B

HMA FLANGEWAY AND FIELD FILLERS

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TIMBER, CONCRETE OR RUBBER CROSSING SURFACE MATERIAL, RAILS, TIES, BALLAST, GEOTEXTILE FABRIC AND CROSSING DRAINAGE SYSTEM BY OTHERS UNLESS OTHERWISE PROVIDED.

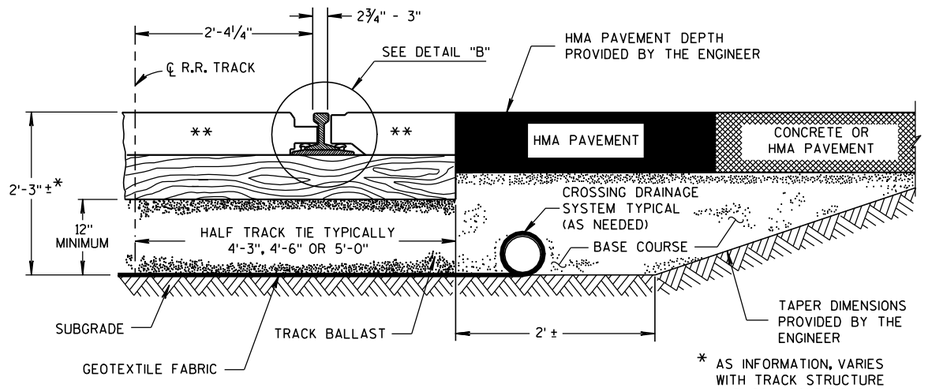
HMA PAVEMENT APPROACHES AND HMA PAVEMET CROSSING SURFACES TO BE PLACED BY CONTRACTOR UNLESS OTHERWISE PROVIDED.

HMA FLANGEWAY AND FIELD FILLERS TO BE PLACED AND THOROUGHLY HAND COMPACTED BY THE CONTRACTOR WHEN NOT PROVIDED BY OTHERS. SEE DETAIL B. HMA FILLERS NOT REQUIRED WHEN RUBBER FILLERS ARE PROVIDED.

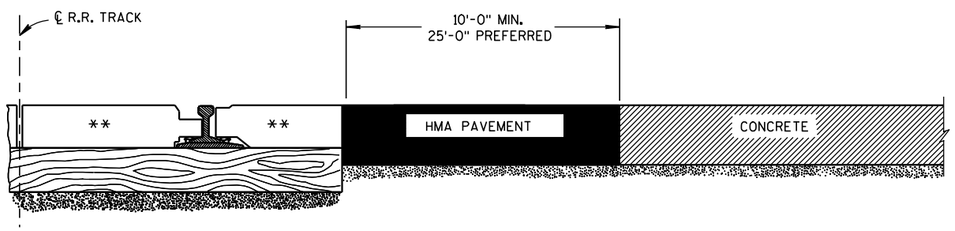
HMA PAVEMENT SHALL BE ROLLED PARALLEL TO THE TRACK.

** CROSSING SURFACE MAY BE TIMBER, RUBBER, CONCRETE, HMA PAVEMENT OR A COMBINATION OF SUCH MATERIALS.

*** CROWN TRANSITION LENGTH SHOWN ELSEWHERE IN THE PLAN.



TYPICAL HALF SECTION



SECTION A-A

CONCRETE PAVEMENT APPROACH

SECTION A-A

HMA PAVEMENT APPROACH

EXAMPLES OF PAVEMENT APPROACHES

PAVEMENT DETAILS FOR RAILROAD APPROACH

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8-28-09 DATE
/S/ Ronald E. Adams
CHIEF, RAILROADS & HAI CTION
FHWA

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S.D.D. 13 B 1-10

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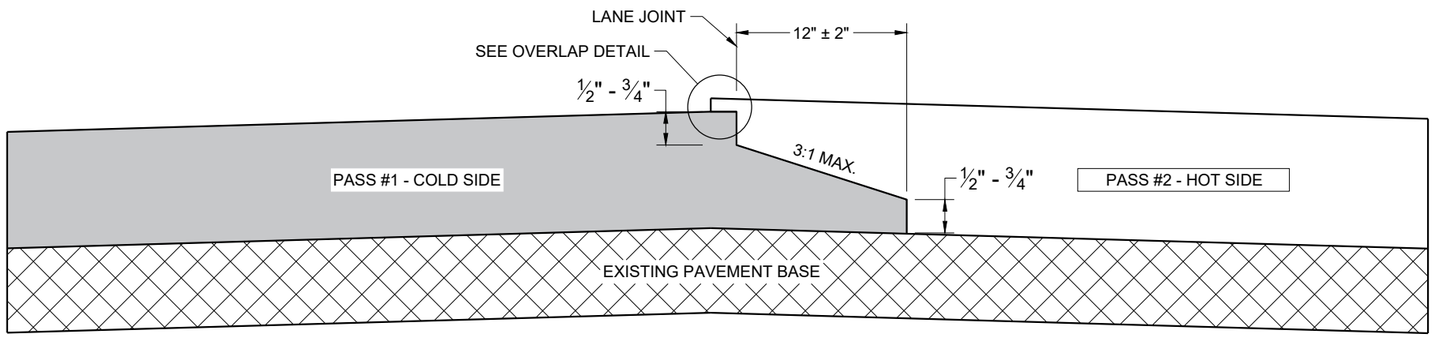
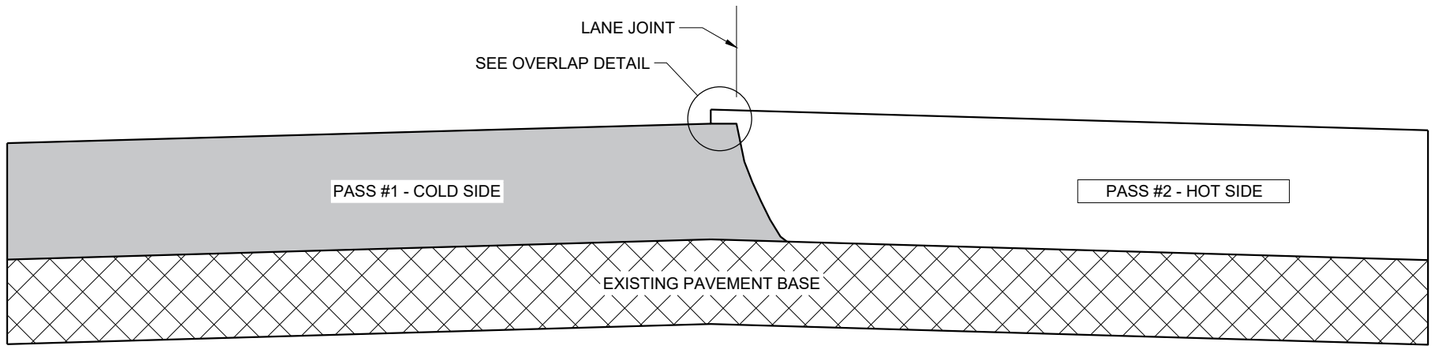
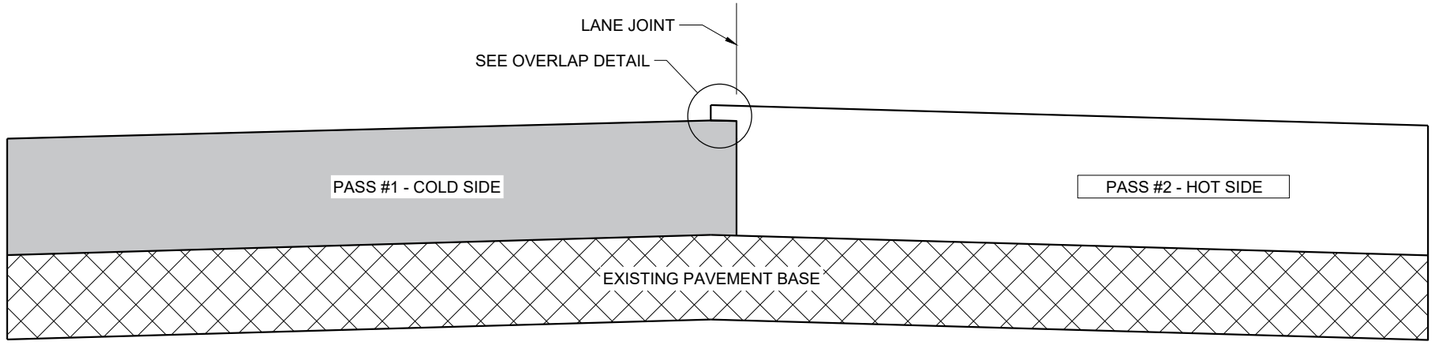
S.D.D. 13 B 1-10



TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)

GENERAL NOTES

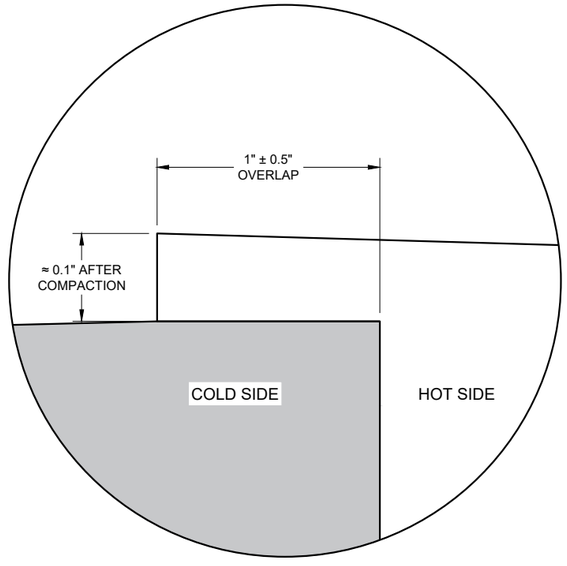
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY 1" ± 0.5" AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

6

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SDD 13C19 - 03

SDD 13C19 - 03

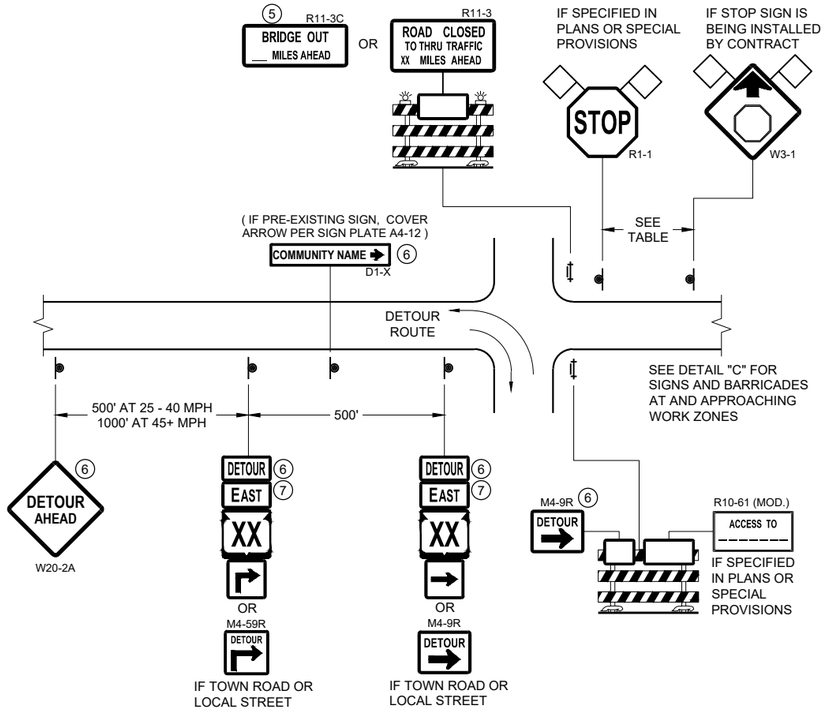
HMA LONGITUDINAL JOINTS

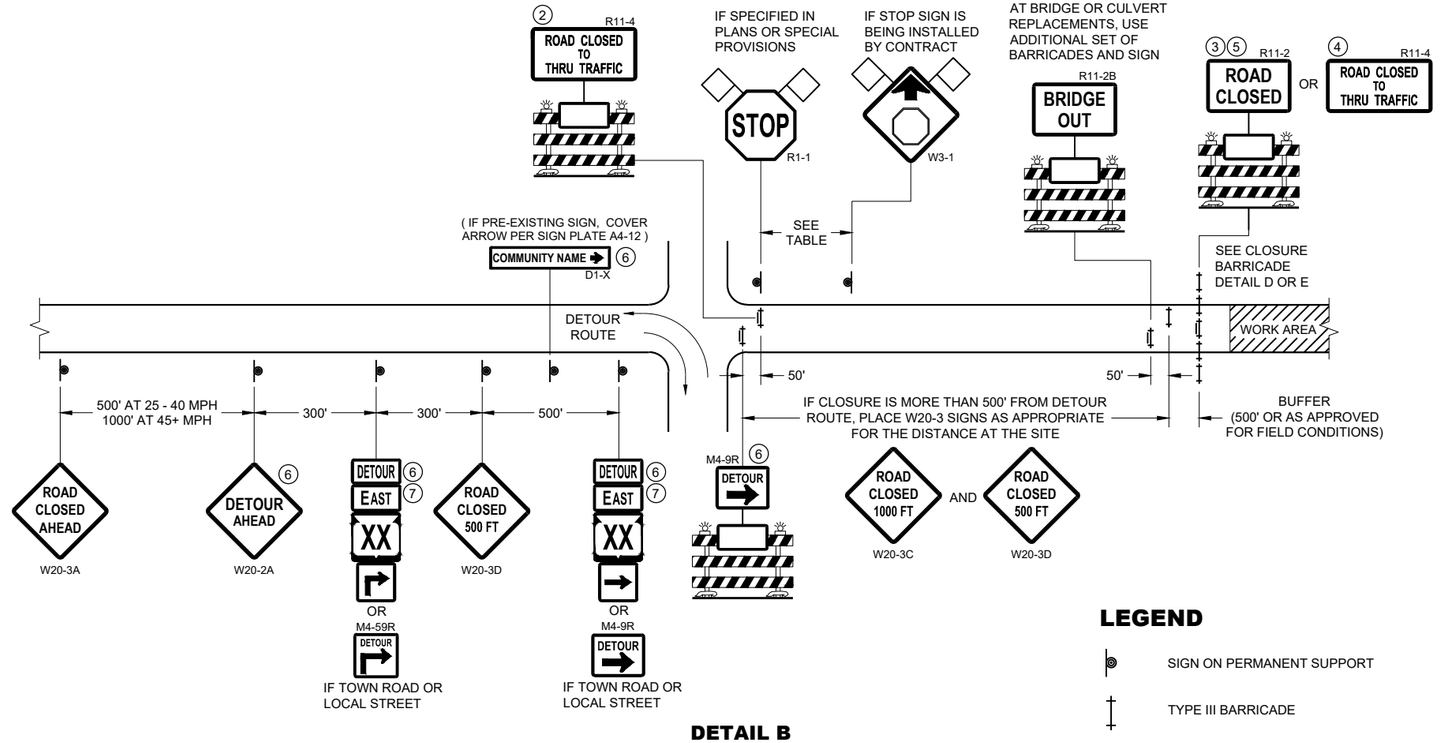
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2020
DATE

/S/ Steven Hefel
HMA PAVEMENT ENGIN

FHWA



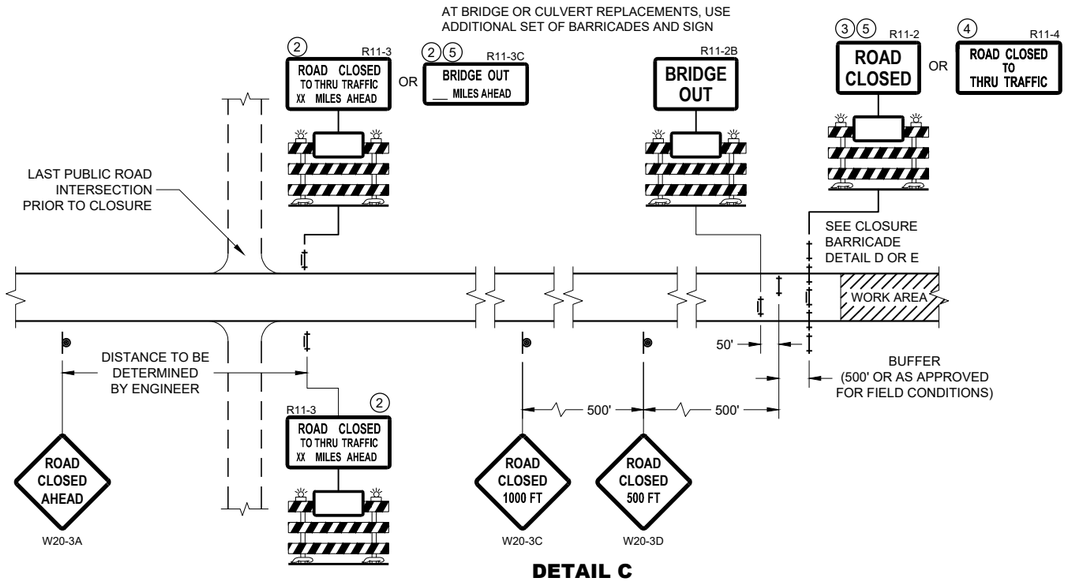


DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN OR EQUAL TO ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)

DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)
- DETOUR — M4 - 8
- EAST — M3 - X
- XX — M1 - 4 OR XX — M1 - 6 OR COUNTY X — M1 - 5A
- M05 - 1 OR M06 - 1

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

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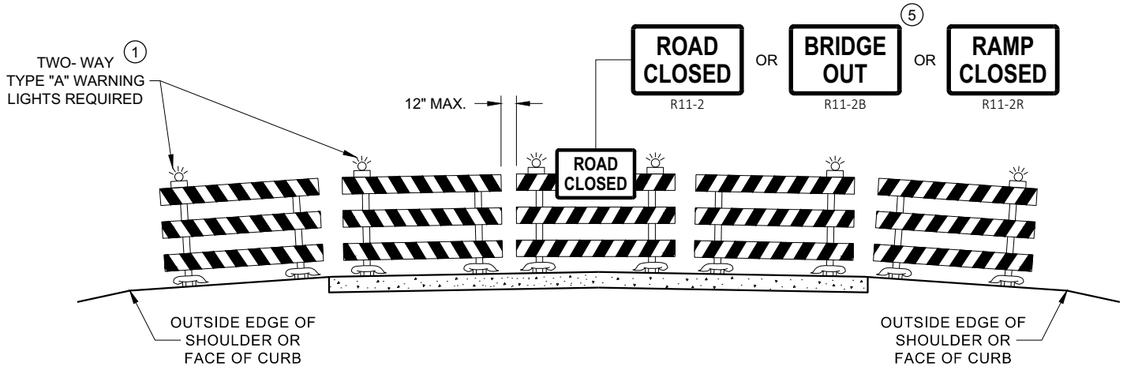
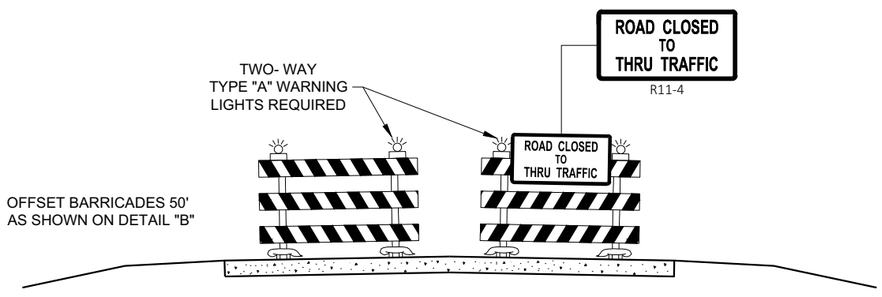
SDD 15C02 - 08a

SDD 15C02 - 08a



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60 " X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING.

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR VARIOUS CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEE
FHWA

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SDD 15C02 - 08b

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SDD 15C02 - 08b

THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- DETOUR / EAST — M4 - 8, M3 - X
- XX — M1 - 4
- OR XX — M1 - 6
- OR COUNTY X — M1 - 5A
- M05 - 1
- OR M06 - 1
- OR M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

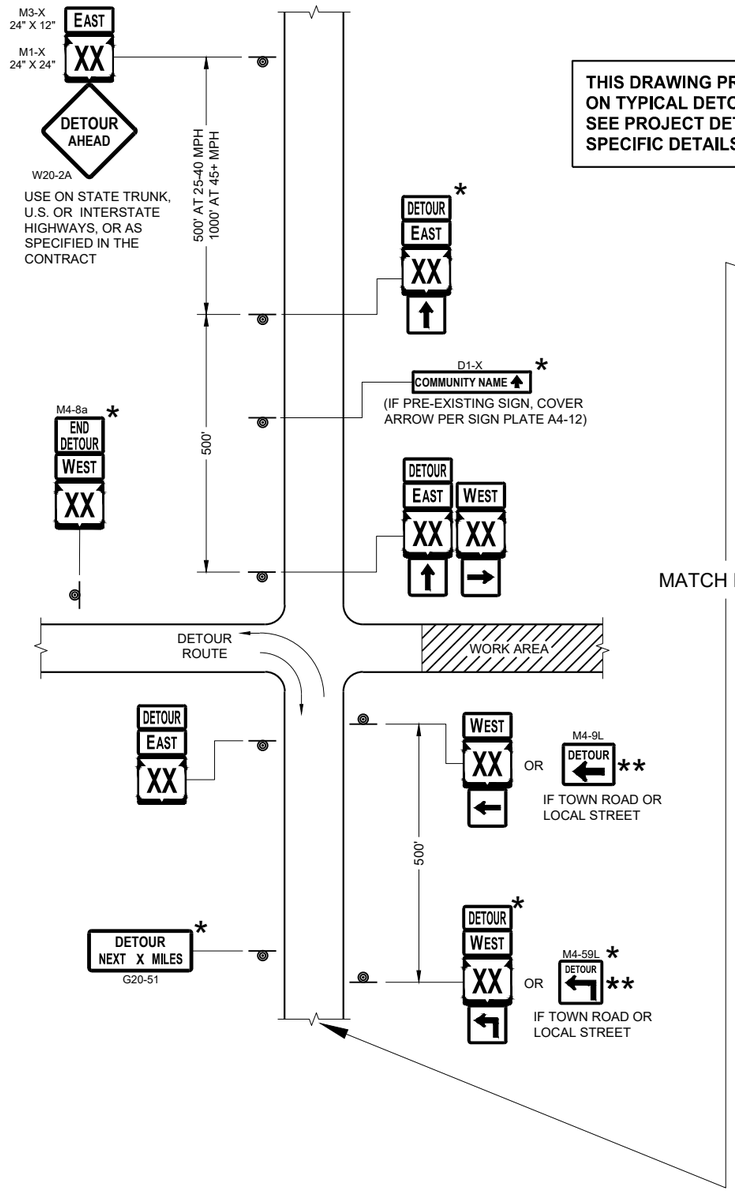
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

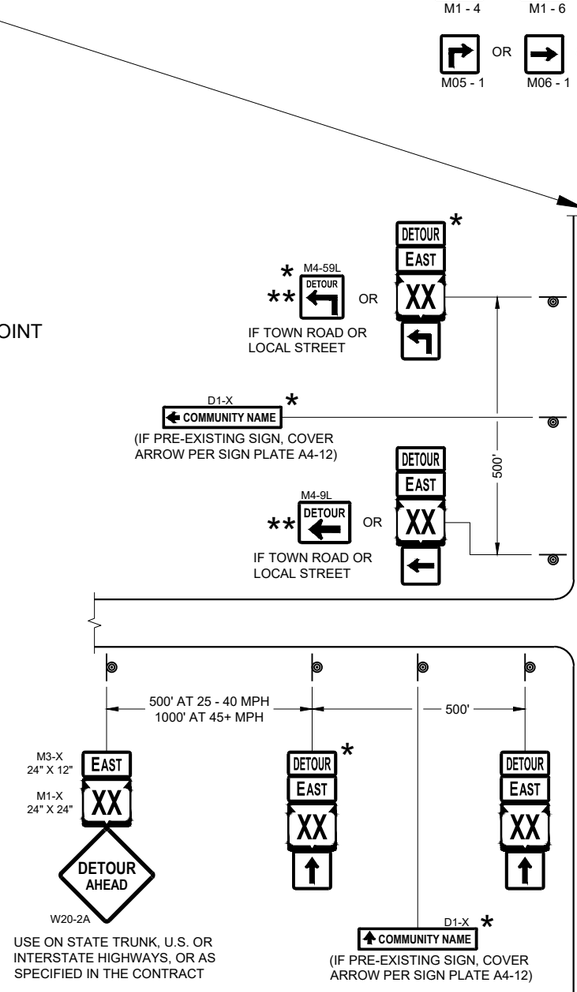
M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
M4-9 AND M4-59 SHALL BE 30" X 24"
M4-8a SHALL BE 24" X 18"
G20-51 SHALL BE 60" X 24"
W20-2A SHALL BE 48" X 48"
D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

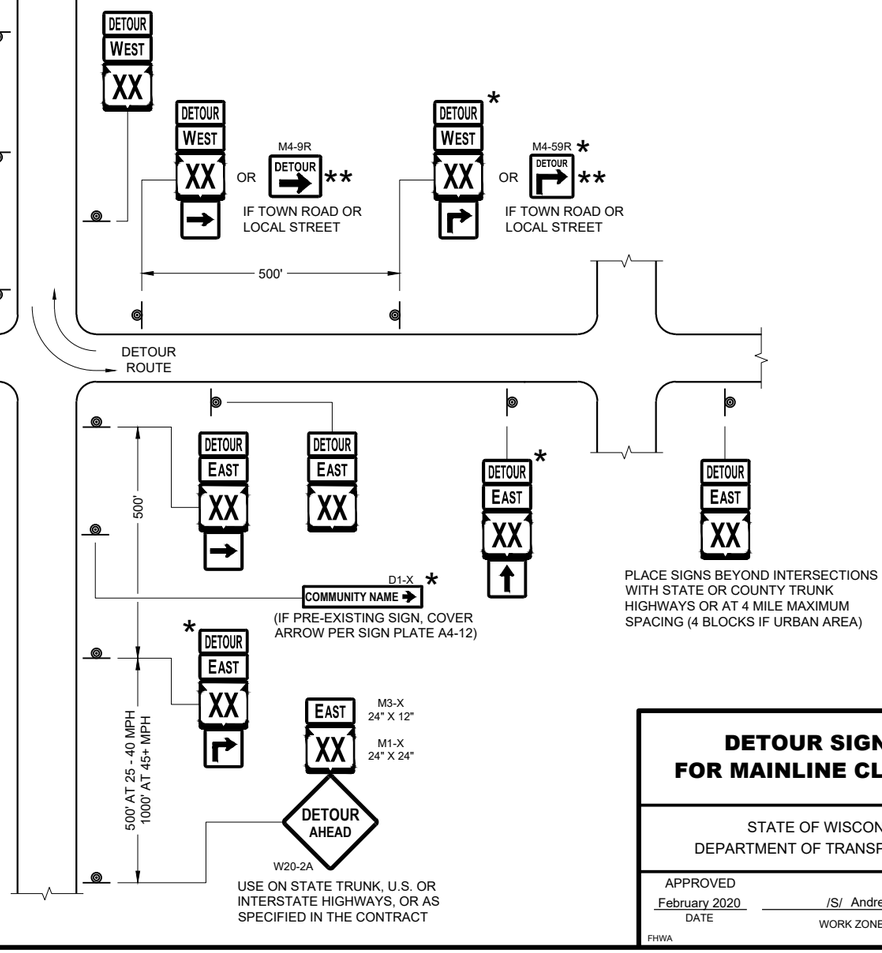
* OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.

** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.



MATCH POINT





SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

DETAIL F
DETOUR SIGNING

DETOUR SIGNING FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE

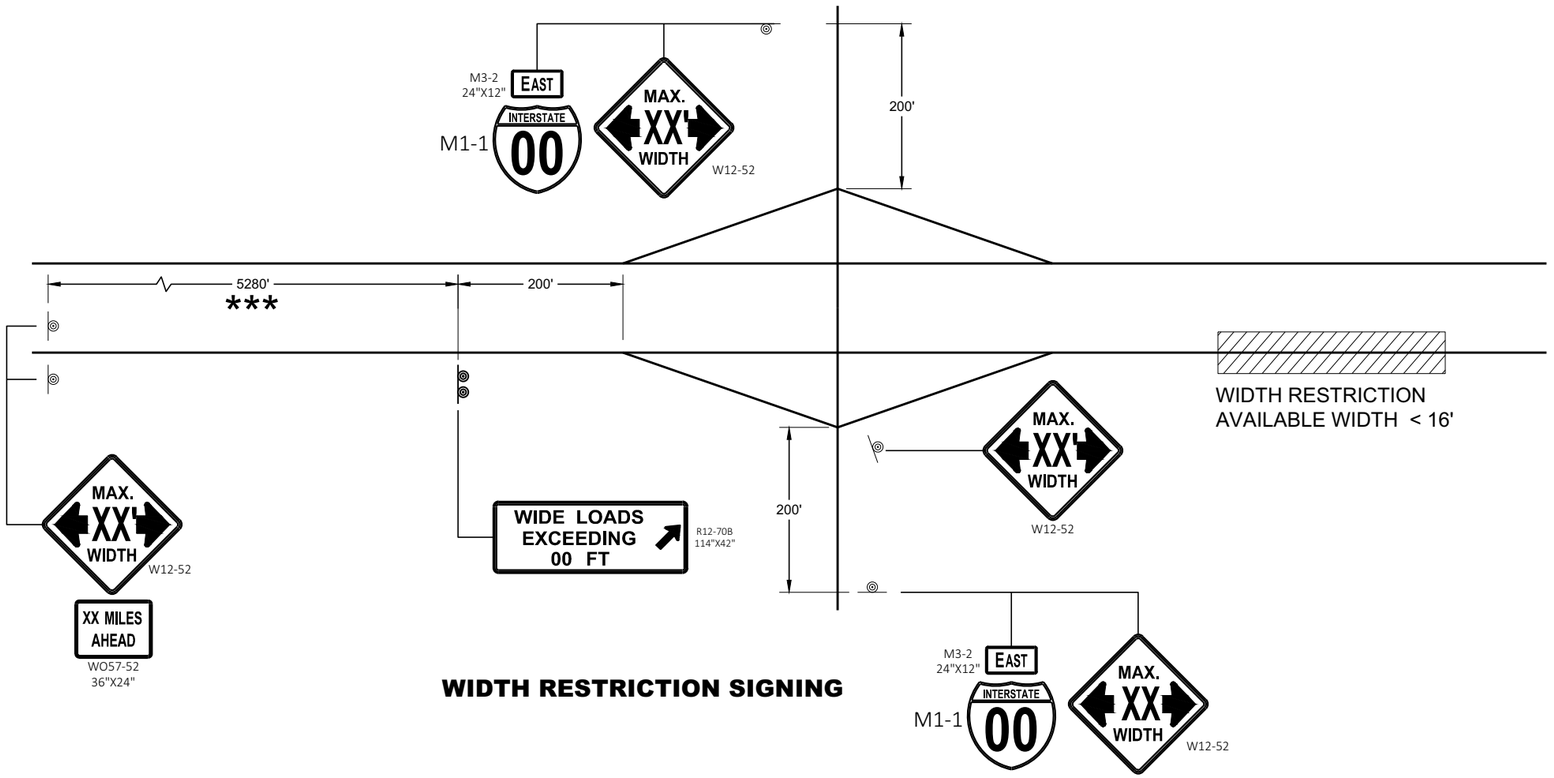
/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

SDD 15C02 - 08c

6

SDD 15C02 - 08c



WIDTH RESTRICTION SIGNING

LEGEND

SIGN ON PERMANENT SUPPORT

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL " IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

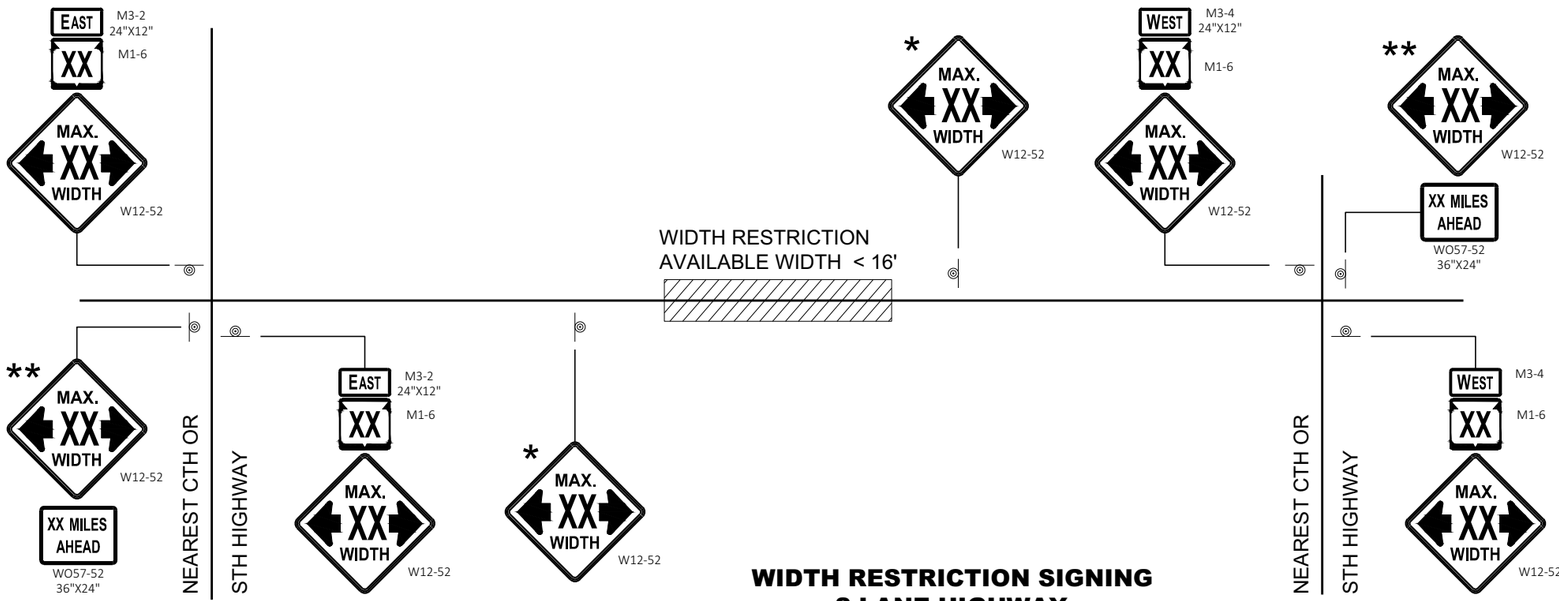
WIDTH ON SIGN TO BE APPROXIMATELY ONE FOOT LESS THAN AVAILABLE WIDTH.

* PLACE 500 FEET AFTER THE W20 - 1A AND 500 FEET BEFORE ADDITIONAL SIGNS FOR ROADWAYS WITH A PRE - CONSTRUCTION SPEED LIMIT OF 45 MPH OR MORE. FOR 35-40 MPH, USE 350 FOOT TYPICAL SPACING. FOR 25-30 MPH, USE 200 FOOT TYPICAL SPACING.

** SIGN SHALL BE VISIBLE FROM ROADWAY.

*** ADDITIONAL SIGNS NEEDED IF THERE IS AN ON RAMP BETWEEN SIGNS.

WIDTH ON SIGN TO BE APPROX. 1 - FOOT LESS THAN AVAILABLE WIDTH



WIDTH RESTRICTION SIGNING
2 LANE HIGHWAY

ADVANCED WIDTH RESTRICTION SIGNING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

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SDD 15C02 - 08f

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

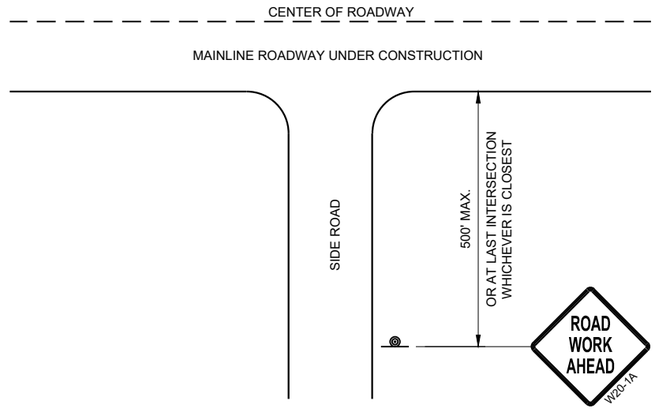
ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS

** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.



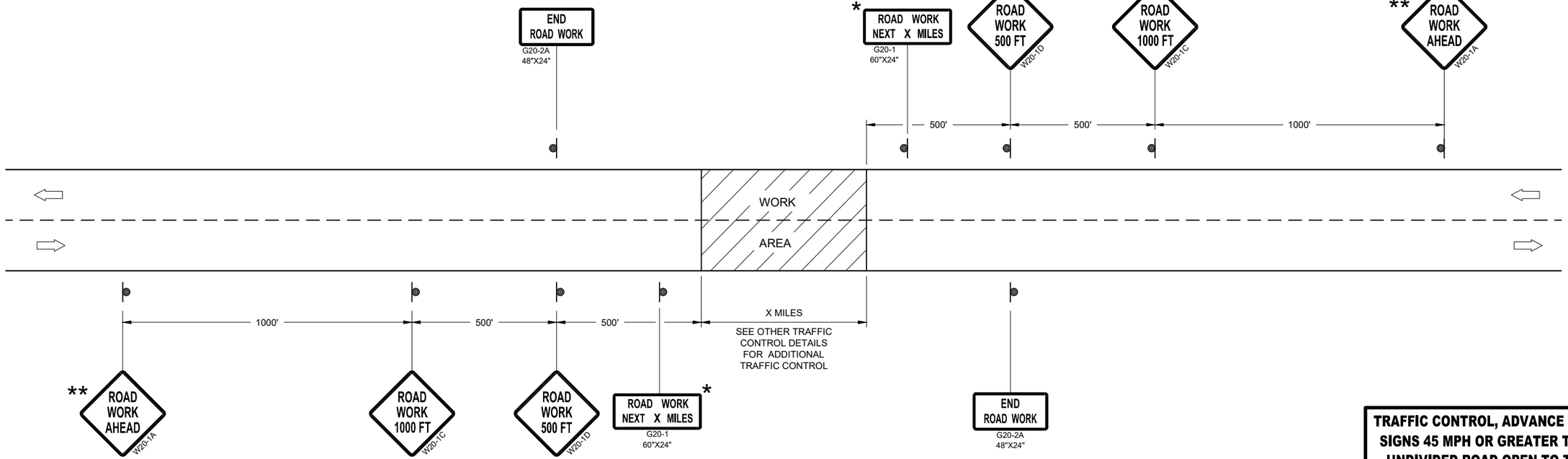
TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL

LEGEND

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC

WORK AREA



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 MPH OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

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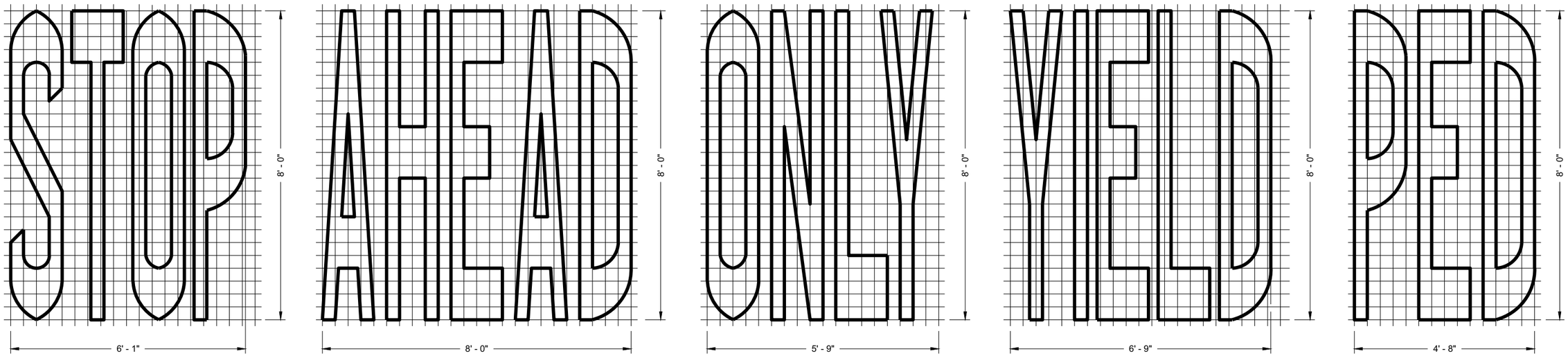
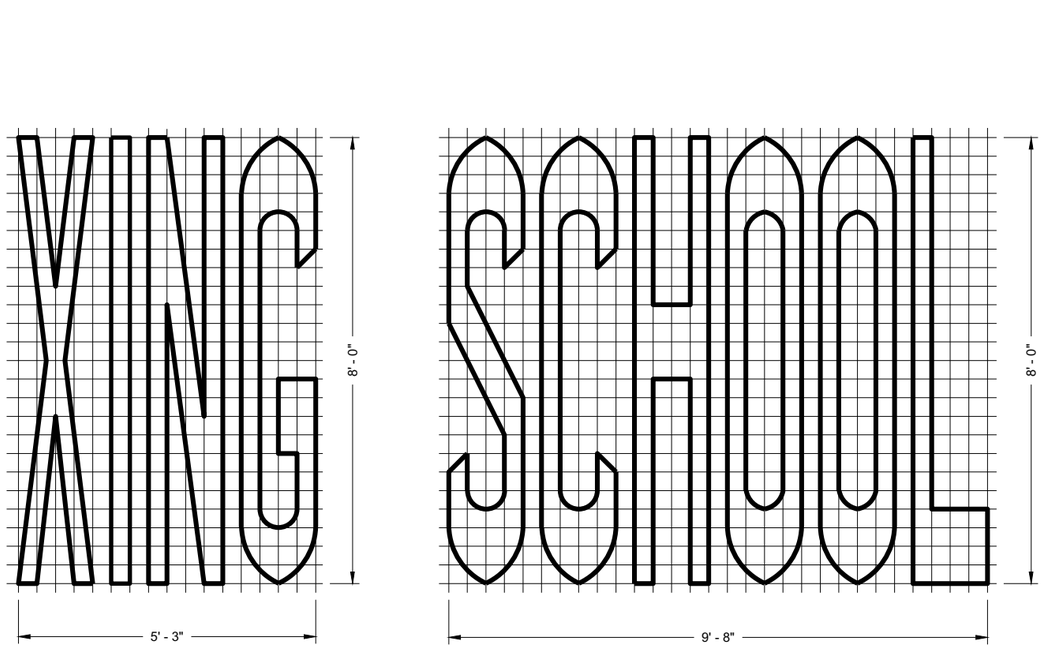
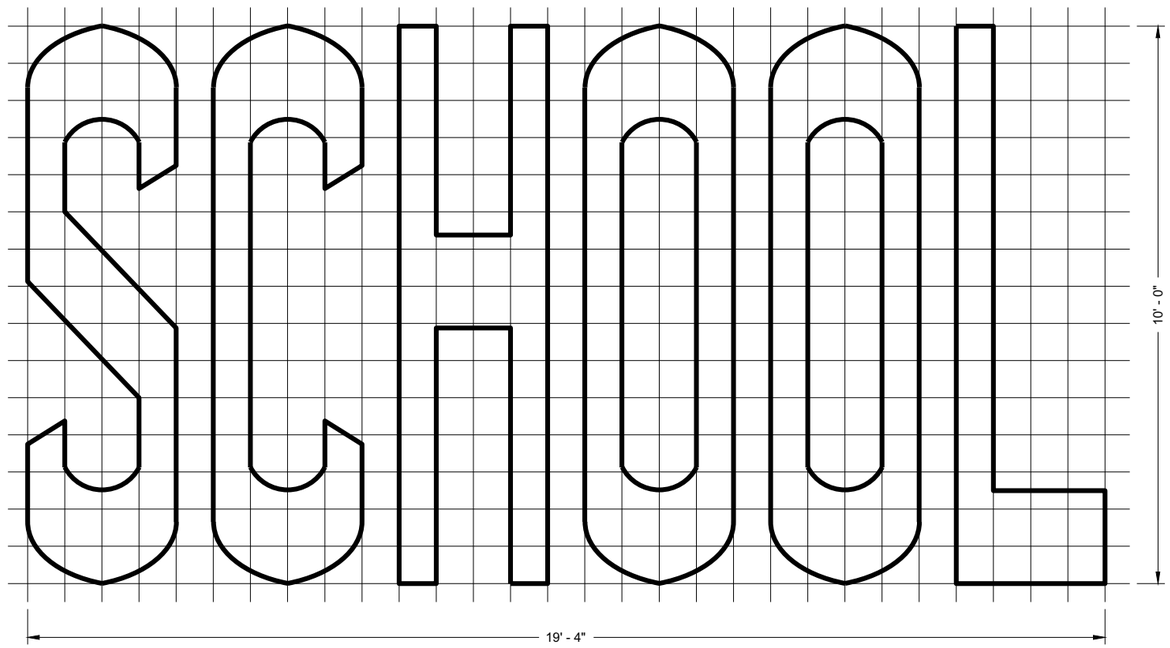
SDD 15C04 - 05





SINGLE LANE



TWO - LANE

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING WORDS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019
DATE

/S/ Matthew Rauch
STATE SIGNING AND MAF ENGINEER

FHWA

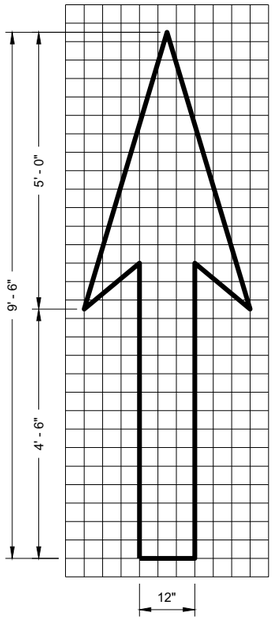
6

SDD 15C07 - 15b

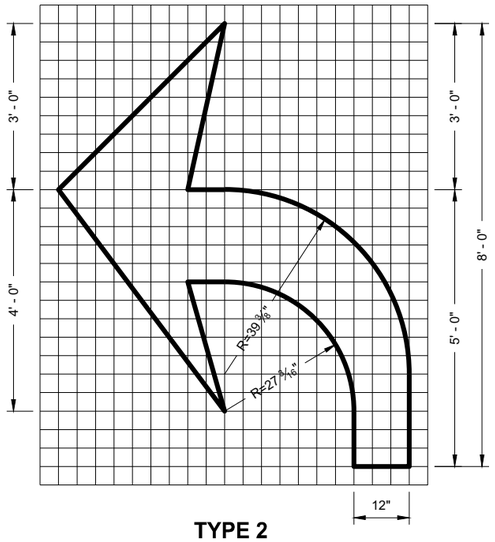
SDD 15C07 - 15b

GENERAL NOTES

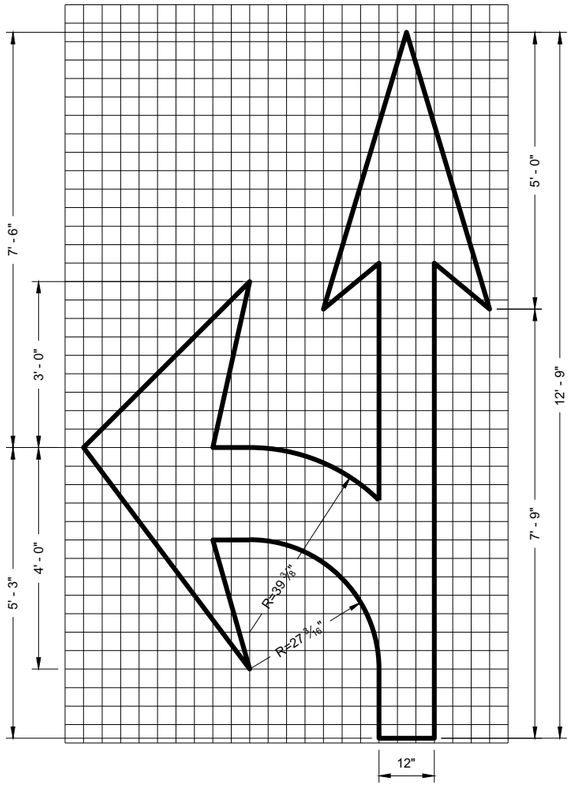
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



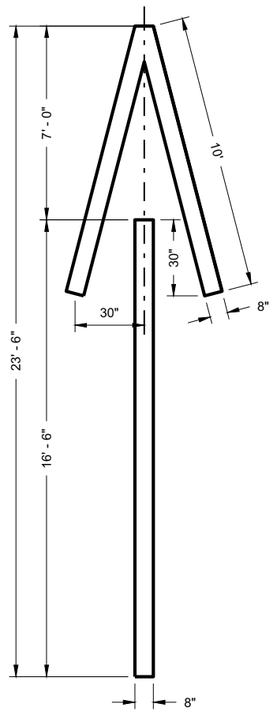
TYPE 1



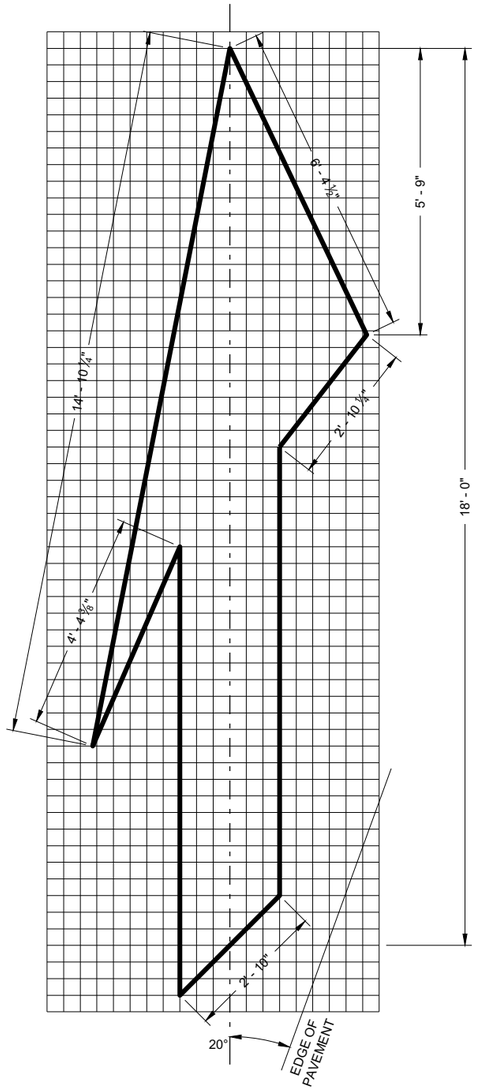
TYPE 2



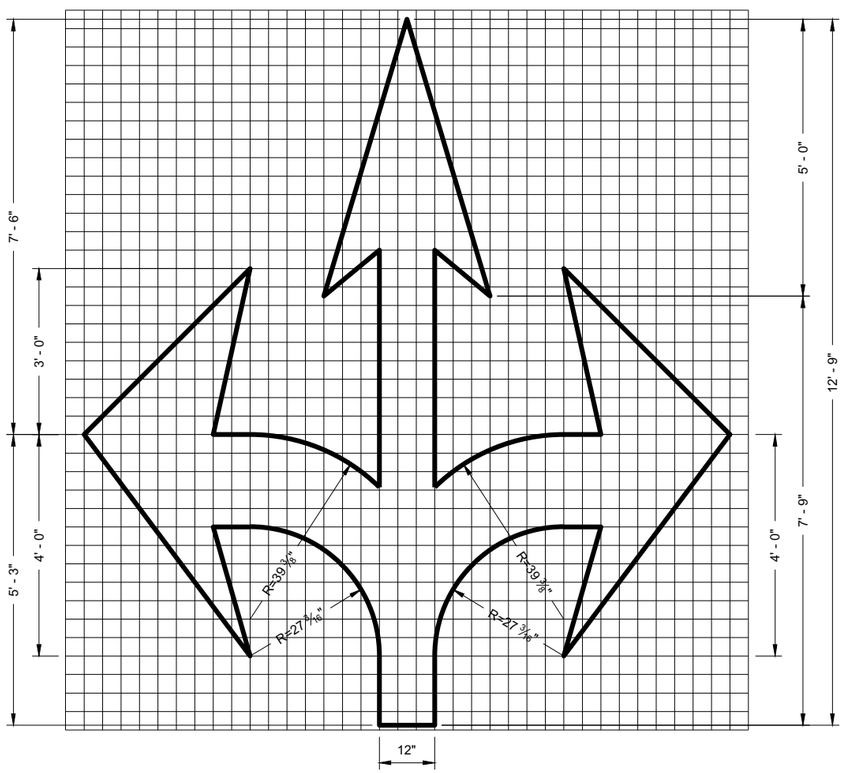
TYPE 3



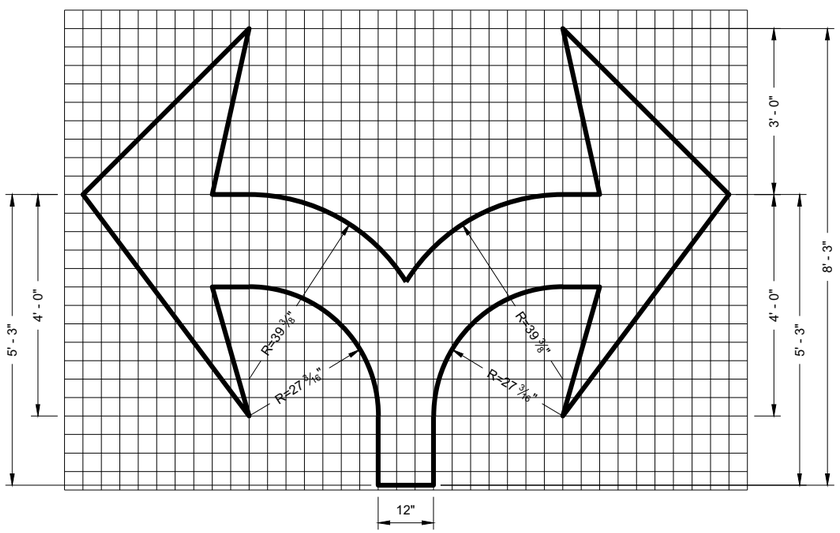
TYPE 4



TYPE 5 LANE DROP ARROW



TYPE 6



TYPE 7

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019
DATE

/S/ Matthew Rauch
STATE SIGNING AND MAF ENGINEER

FHWA

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SDD 15C07 - 15c

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SDD 15C07 - 15c

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

(1) LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING

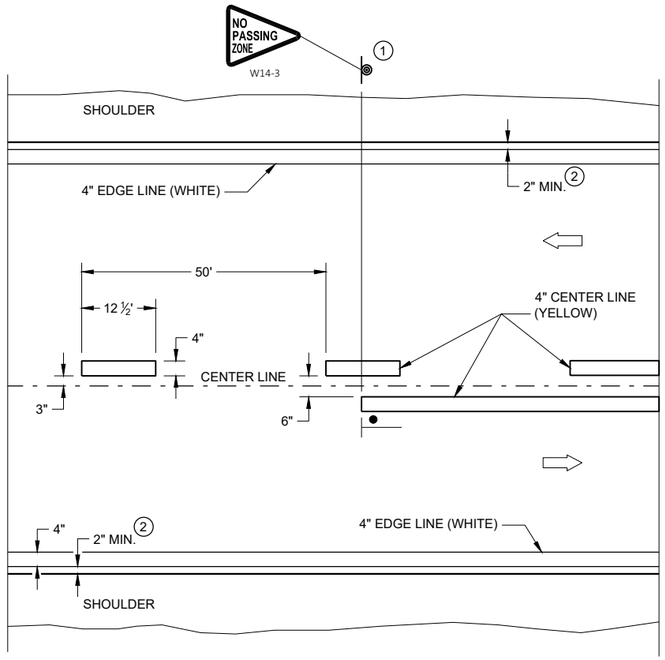
(2) MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

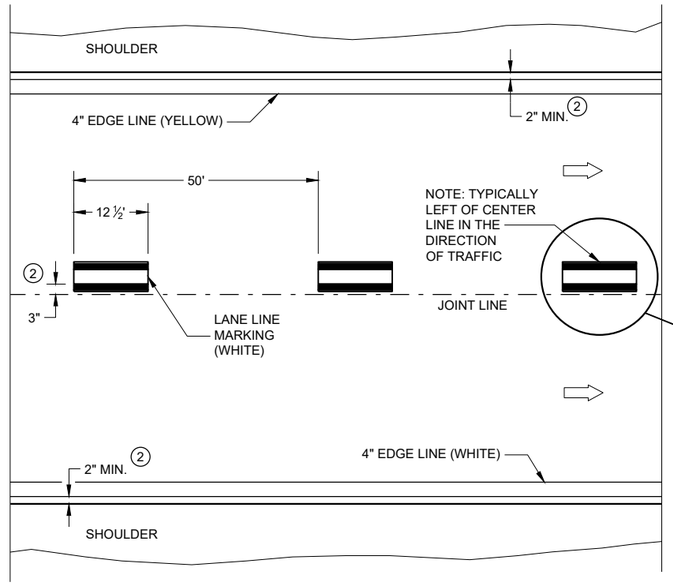
"T" MARKING

SIGN ON PERMANENT SUPPORT

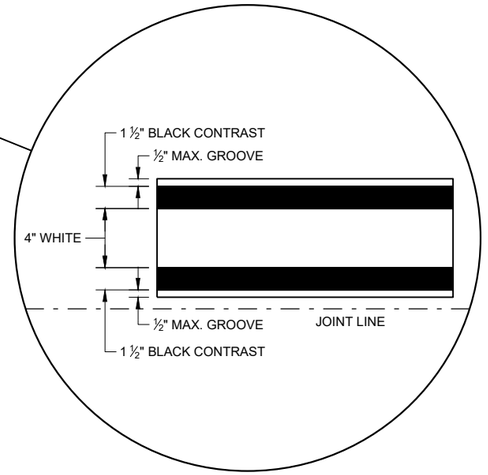
DIRECTION OF TRAFFIC



TWO WAY TRAFFIC



ONE WAY TRAFFIC



PERMANENT PAVEMENT MARKING

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022 — DATE
/S/ Jeannie Silver — STATEWIDE SIGNING AND M ENGINEER

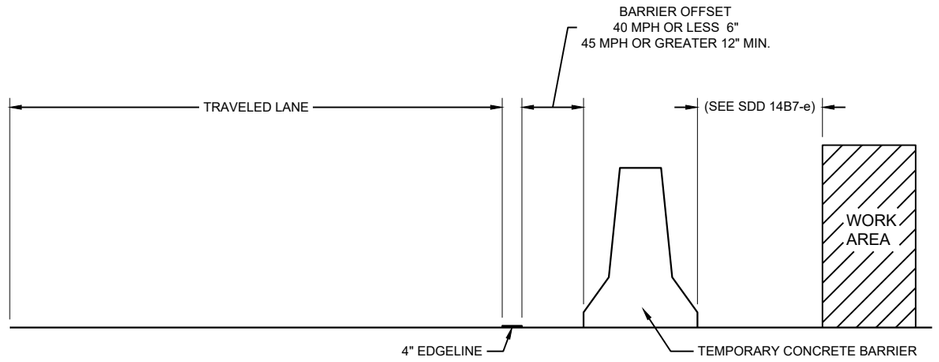
FHWA

6

SDD 15C08 - 22a

6

SDD 15C08 - 22a



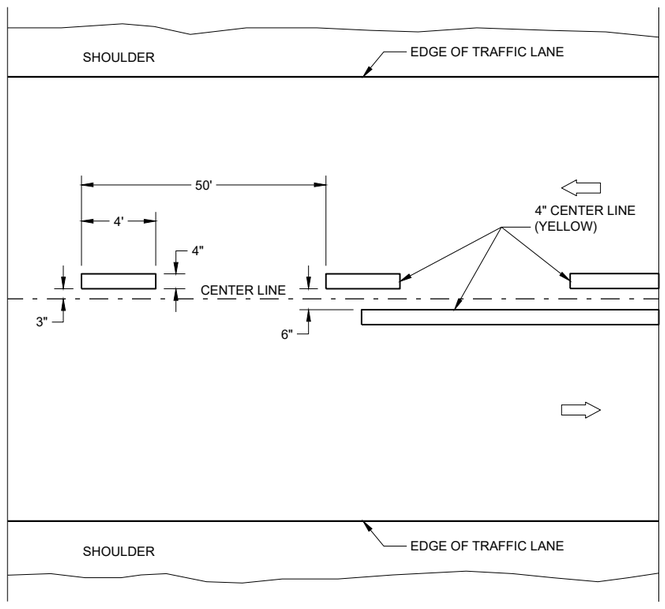
TEMPORARY BARRIER OFFSET FROM EDGELINE

GENERAL NOTES

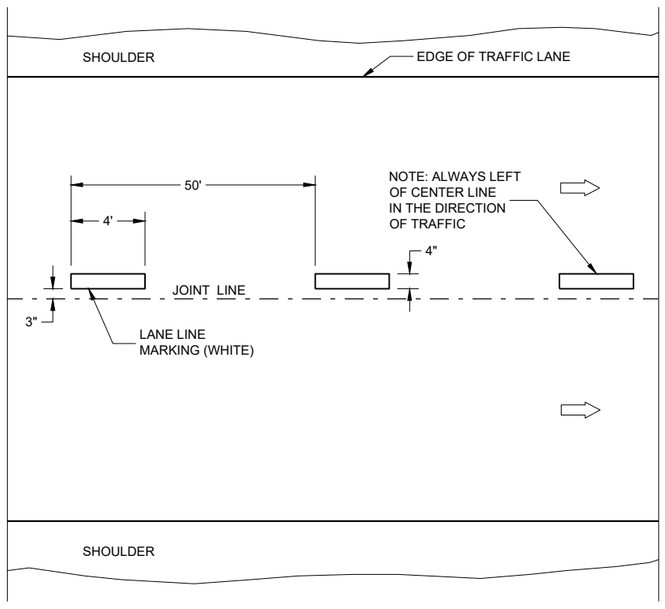
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

LEGEND

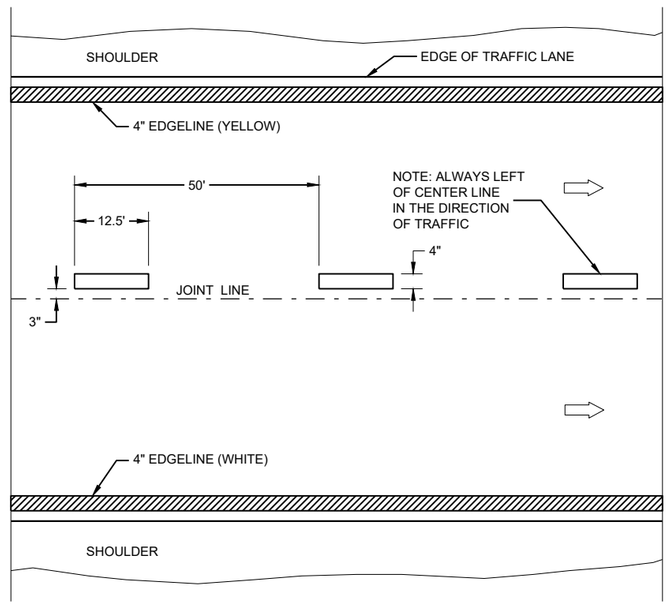
DIRECTION OF TRAFFIC



TWO WAY TRAFFIC



ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

May 2022
DATE

/S/ Jeannie Silver
STATEWIDE SIGNING AND M ENGINEER

FHWA

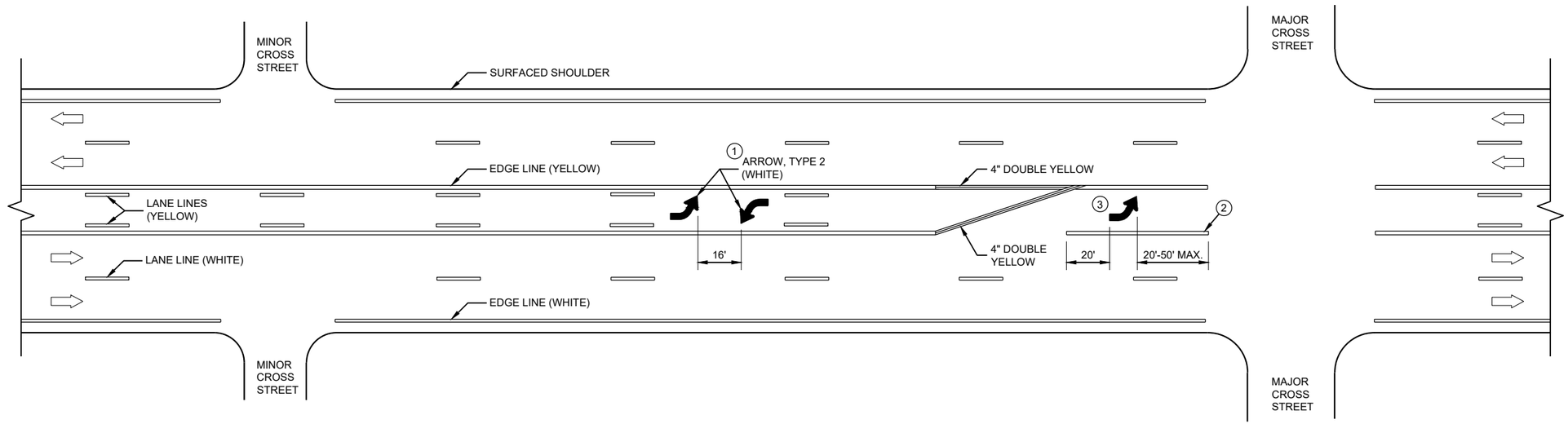
6

SDD 15C08 - 22b

GENERAL NOTES

1. A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
2. 8" WHITE
3. TURN BAY LENGTH OF LESS THAN 48' DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT.

DIRECTION OF TRAFFIC



TWO WAY LEFT TURN LANE

6

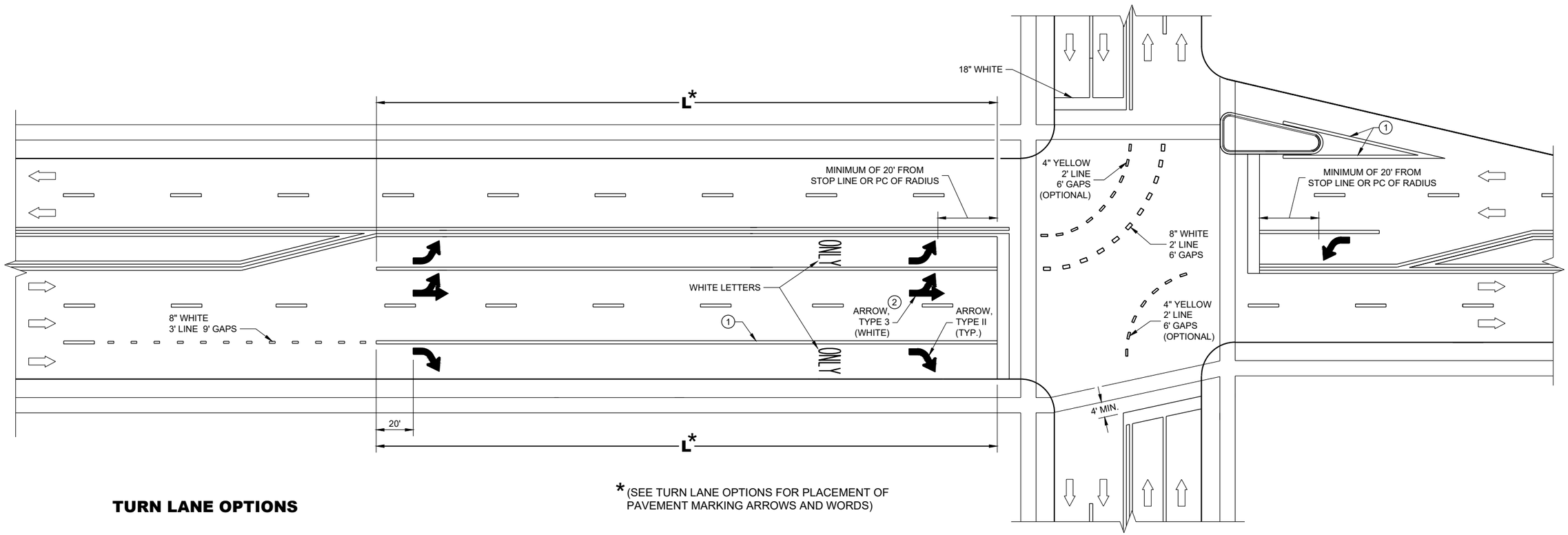
SDD 15C08 - 22c

PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

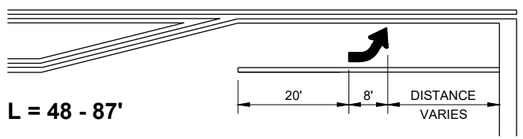
SDD 15C08 - 22c



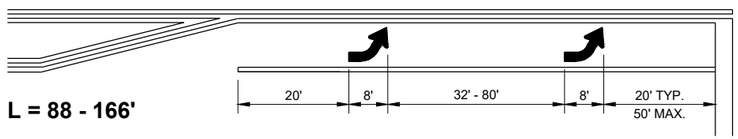
* (SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

TURN LANE OPTIONS

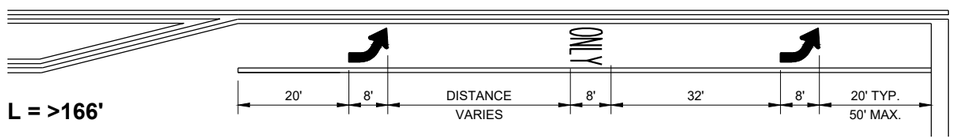
LENGTH OF TURN BAY (**L**) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



L = 48 - 87'



L = 88 - 166'



L = >166'

GENERAL NOTES

1. 8" WHITE
2. QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

DIRECTION OF TRAFFIC

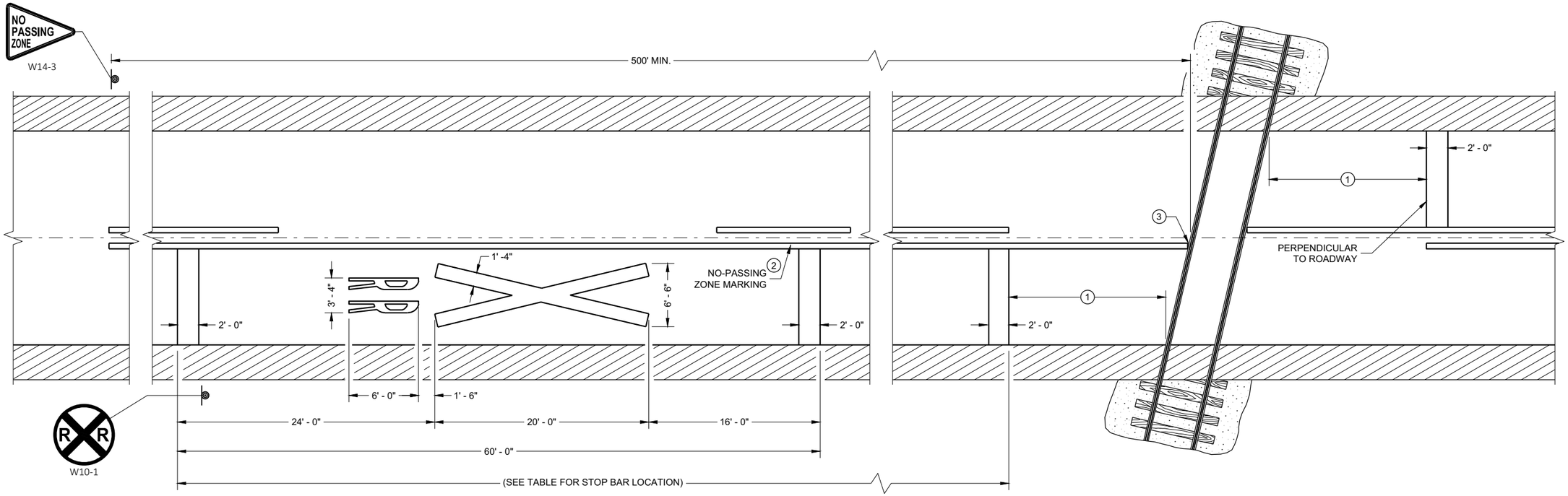
L = LENGTH OF TURN BAY

PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 15C08 - 22d



PAVEMENT MARKING

LEGEND

SIGN ON PERMANENT SUPPORT

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

ON MULTI-LANE ROADS THE TRANSVERSE BANDS SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.

CENTER OR LANE LINES AND NO-PASSING ZONE MARKINGS SHOWN ON THIS DRAWING ARE REQUIRED AND PAID FOR UNDER OTHER ITEMS IN THE CONTRACT.

TRACE EXISTING SYMBOL WHERE EXISTING SYMBOLS ARE PLACED.

(1) MINIMUM 8' FROM ANY RAILROAD WARNING DEVICES (SIGNAL , GATES, ETC.) OR 25' FROM THE NEAREST RAIL, WHICHEVER DISTANCE IS GREATER.

(2) 500' MINIMUM. MARKING LIMITS MAY BE EXTENDED AS DIRECTED BY THE ENGINEER TO MEET ADJACENT NO-PASSING ZONE MARKINGS.

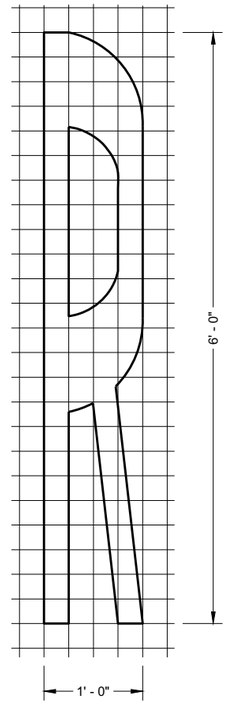
(3) FOR MULTIPLE TRACK CROSSINGS, THE BARRIER LINE SHALL EXTEND TO THE NEAR RAIL OF THE FURTHEST TRACK IN THE DIRECTION OF HIGHWAY TRAVEL.

DISTANCE TABLE

TABLE BASED UPON 2C-4 WISCONSIN SUPPLEMENT OF MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

POSTED SPEED (M.P.H.)	DIMENSION RANGE (FEET)
25	150* - 250'
30	200* - 300'
35	250* - 450'
40	300* - 500'
45	400* - 650'
50	550* - 800'
55	750* - 1000'
60	1000* - 1250'
65	1000* - 1250'

* THE MINIMUM DISTANCES IN THE TABLE ARE DESIRABLE AND SHOULD BE USED. THE DISTANCES MAY BE INCREASED UP TO THE MAXIMUM TO ALLOW FOR FIELD CONDITIONS SUCH AS THE CLOSED PROXIMITY OF DRIVEWAYS, BRIDGES, SIDE ROADS OR OTHER FEATURES THAT WOULD PROHIBIT THE MINIMUM DISTANCES FROM BEING USED.



SIGNING AND PAVEMENT MARKING DETAILS FOR RAILROAD - HIGHWAY GRADE CROSSINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021
DATE

/S/ Matthew R. Rauch
STATE SIGNING AND MAF ENGINEER

FHWA

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SDD 15C09 - 12a

SDD15C09 - 12a

LEGEND

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC

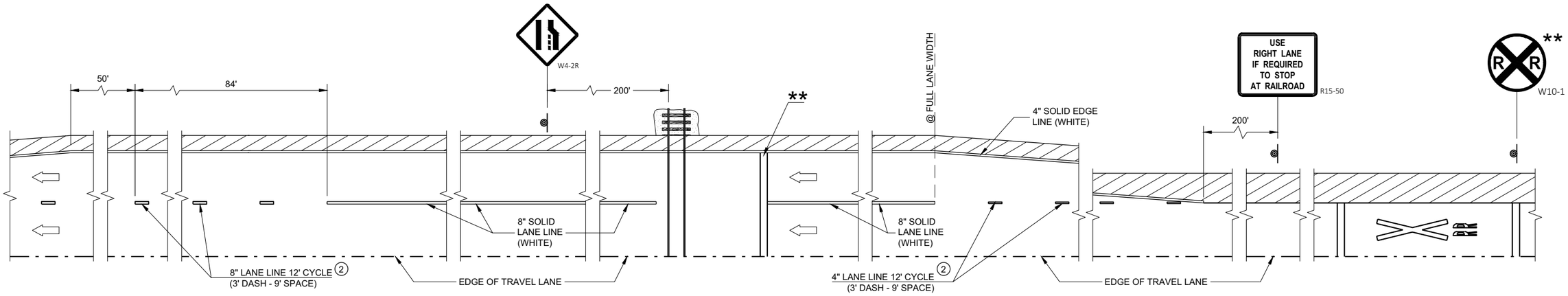
GENERAL NOTES

SIGNING AND MARKING IS SHOWN AS TYPICAL PLACEMENT. FIELD CONDITIONS MAY DICTATE CHANGES IN SIGNING AND MARKING PLACEMENT.

(1) INSTALLED FOR EXPRESSWAYS

(2) 3' LINE, 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.

** SEE SDD 15C09-a, "SIGNING AND PAVEMENT MARKING DETAILS FOR RAILROAD - HIGHWAY GRADE CROSSING" FOR LOCATION OF ADVANCE MARKINGS.

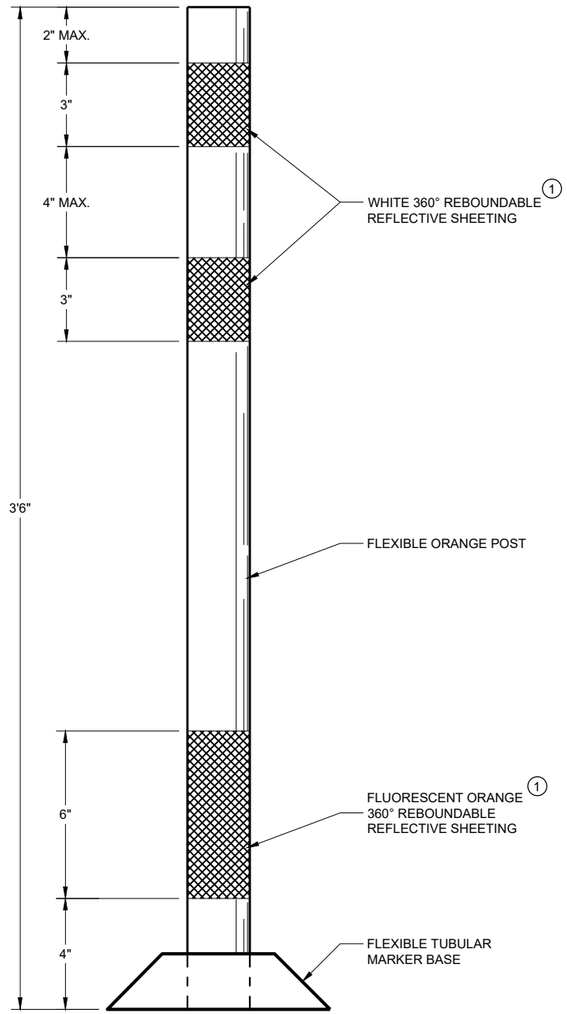


DETAIL FOR TRUCK STOPPING LANE PAVEMENT MARKINGS

TRUCK STOPPING LANE PAVEMENT MARKINGS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED	
February 2021 DATE	/S/ Matthew R. Rauch STATE SIGNING AND MAF ENGINEER
FHWA	81

SDD 15C09 - 12b

SDD 15C09 - 12b



FLEXIBLE TUBULAR MARKER POST WORK ZONE

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

SURFACE MOUNTED BASES SHALL BE FURNISHED IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS TO BE COMPATIBLE WITH FLEXIBLE TUBULAR MARKER POSTS TO A SIZE AND SHAPE THAT WILL PROVIDE A STABLE POST FOUNDATION WHEN SECURED TO THE PAVEMENT.

THE ASPHALTIC ADHESIVE OR BUTYL PAD FURNISHED SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS, UNLESS DIRECTED BY THE ENGINEER TO USE BOLTS.

(1) REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.

6

SDD 15C11 - 09a

CHANNELIZING DEVICES
FLEXIBLE TUBULAR MARKER POST

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2021
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

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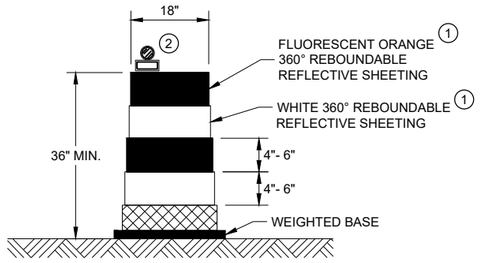
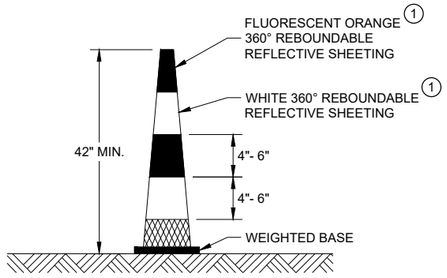
SDD 15C11 - 09a

GENERAL NOTES

1. REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
2. LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.

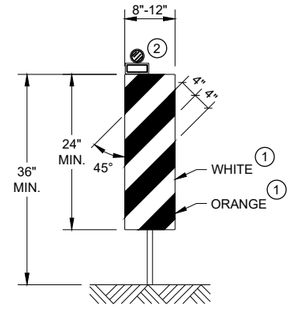


DRUM



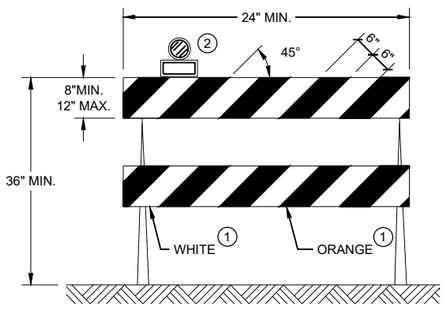
42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS



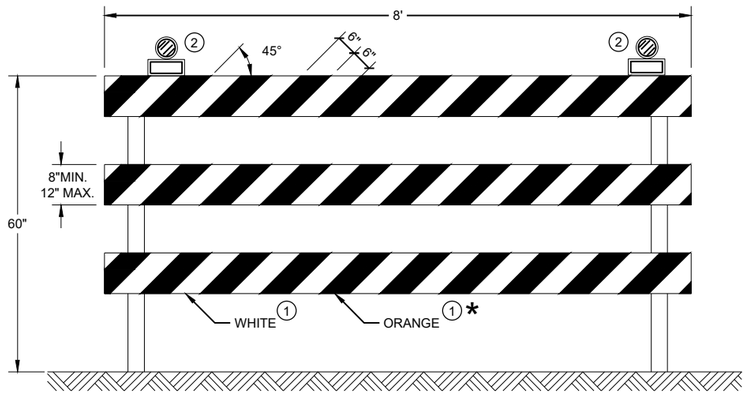
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2021 — DATE
/S/ Andrew Heidtke — WORK ZONE ENGINEER
FHWA

6

SDD15C11 - 09b

6

SDD15C11 - 09b

LEGEND

SIGN ON PORTABLE OR PERMANENT SUPPORT

TEMPORARY PORTABLE RUMBLE STRIP ARRAY

DIRECTION OF TRAFFIC

WORK AREA

FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

(1) FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.

(2) SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

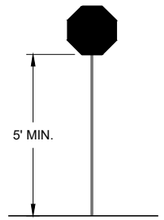
(3) EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



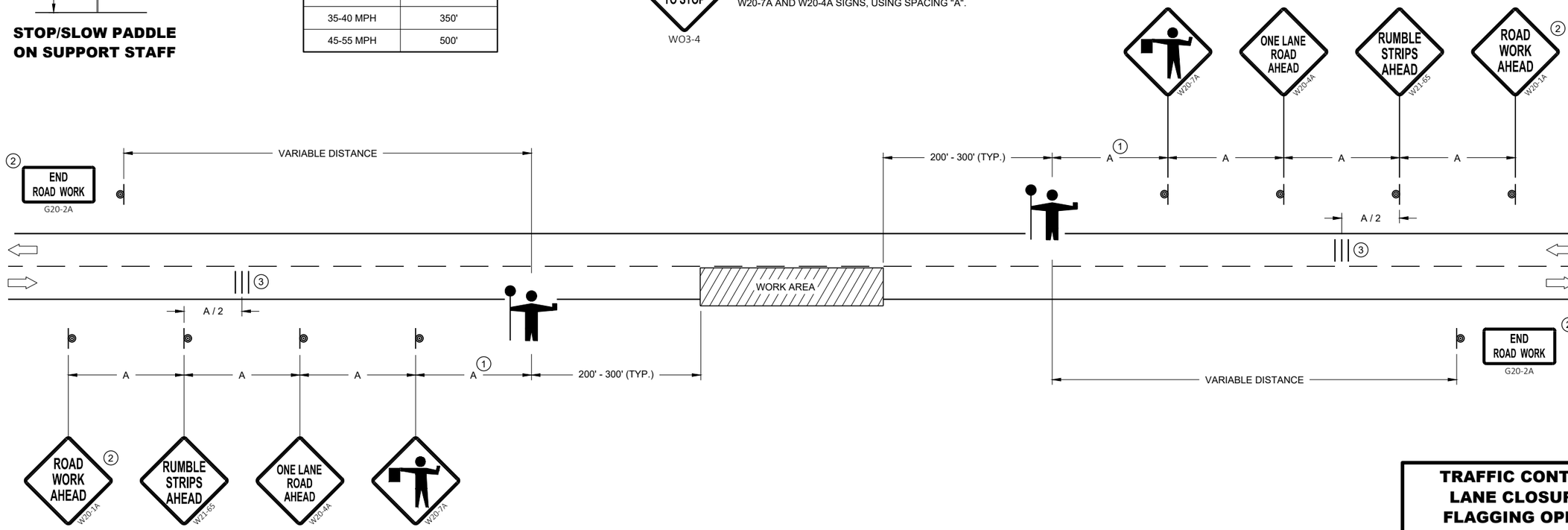
STOP/SLOW PADDLE ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'

WO3-4

USE OF WO3-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

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SDD 15C12 - 09a

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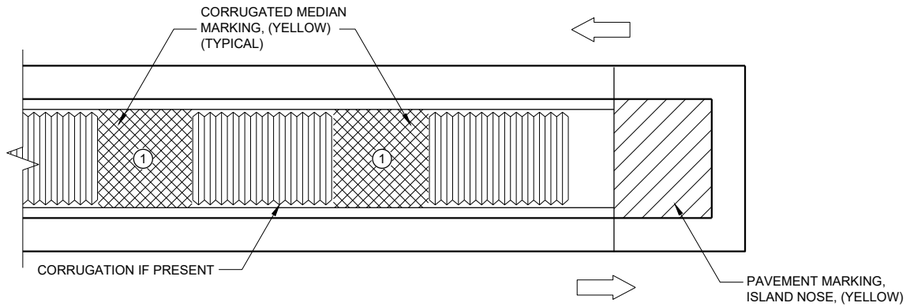
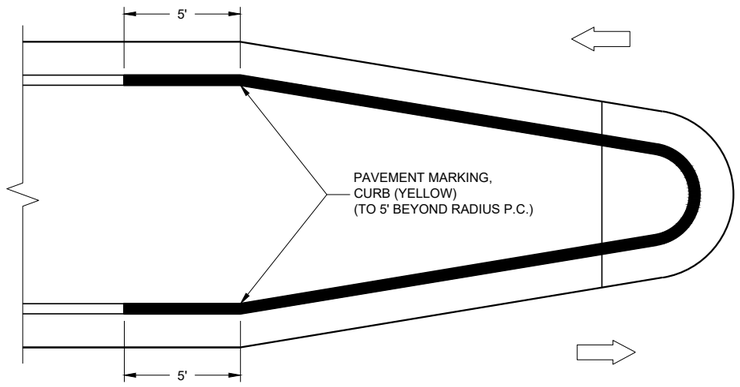
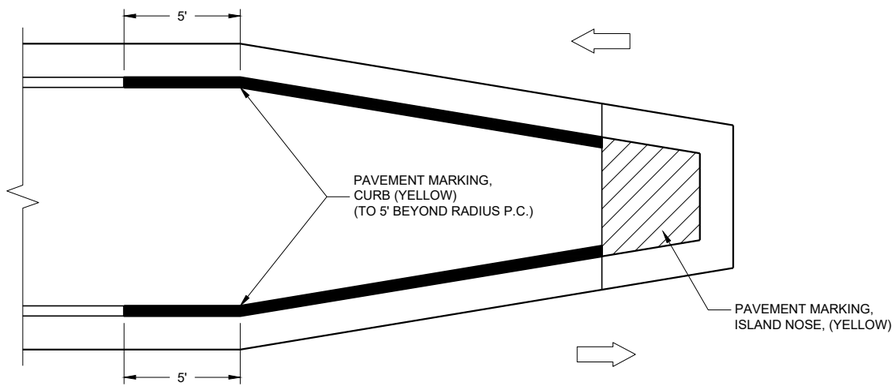
SDD 15C12 - 09a



MEDIAN ISLAND WITH SQUARE BLUNT NOSE



MEDIAN ISLAND WITH ROUND BLUNT NOSE



MEDIAN ISLAND WITH SLOPED NOSE

TYPICAL PLACEMENT OF PAVEMENT MARKING ON MEDIAN ISLANDS

GENERAL NOTES

WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION, YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN. THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.

(1) APPLY PAVEMENT MARKING TO THE FLAT PORTION OF CORRUGATED MEDIAN.

ISLAND NOSE MARKING

CURB MARKING

CORRUGATED MEDIAN MARKING

DIRECTION OF TRAVEL

PAVEMENT MARKINGS, MEDIAN ISLAND NOSE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

May 2022
DATE

/S/ Jeannie Silver
STATE SIGNING AND MAR ENGINEER

FHWA

6

SDD 15C18 - 06b

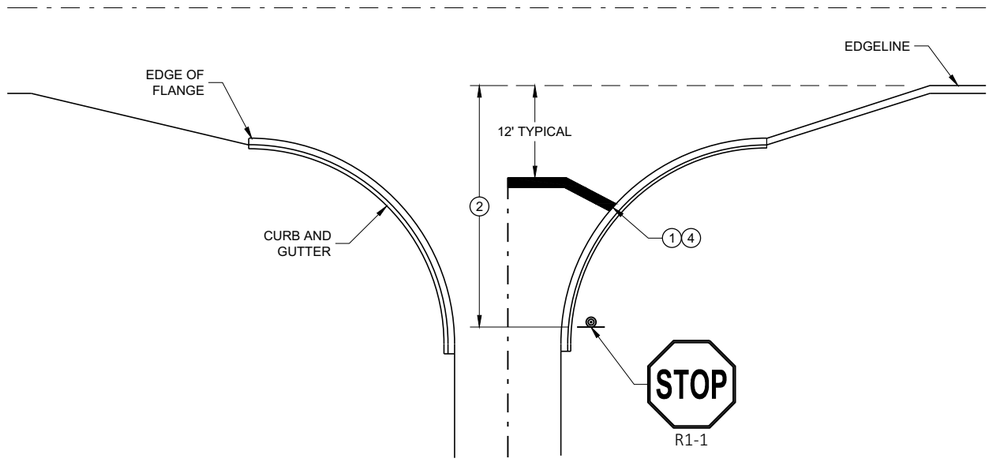
6

SDD15C18 - 06b

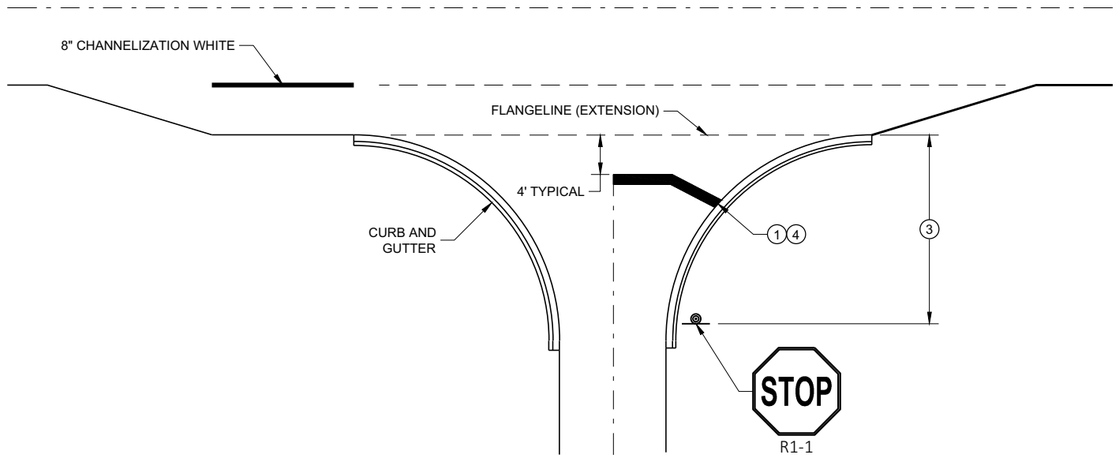
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

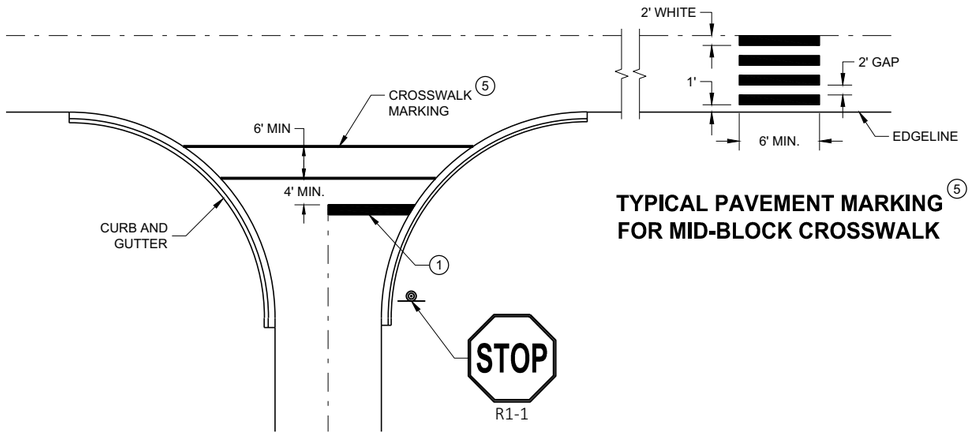
1. 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
2. NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
3. NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
4. MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
5. LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

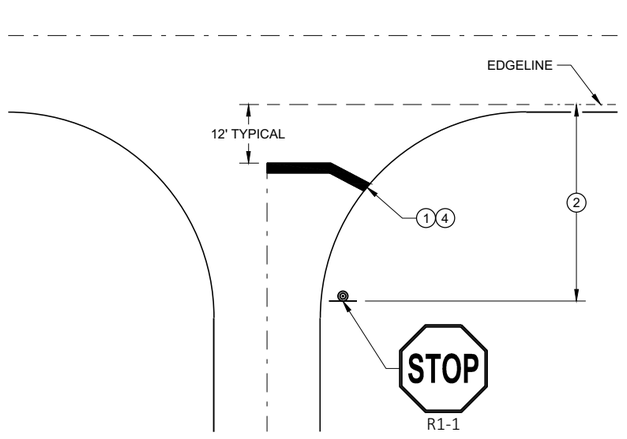


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING

TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK 5



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 — DATE
/S/ Matthew Rauch — STATE SIGNING AND MARKING ENGINEER

FHWA

6

SDD 15C33 - 04
SDD 15C33 - 04

LEGEND

SIGN ON PERMANENT SUPPORT

DELINEATOR, FLEXIBLE/TUBULAR MARKER

DIRECTION OF TRAFFIC

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS.

A SINGLE ROW OF FLEXIBLE TUBULAR MARKERS ON CENTERLINE EXTEND FOR THE ENTIRE LENGTH OF TWO-WAY TRAFFIC AT 50 FOOT SPACING.

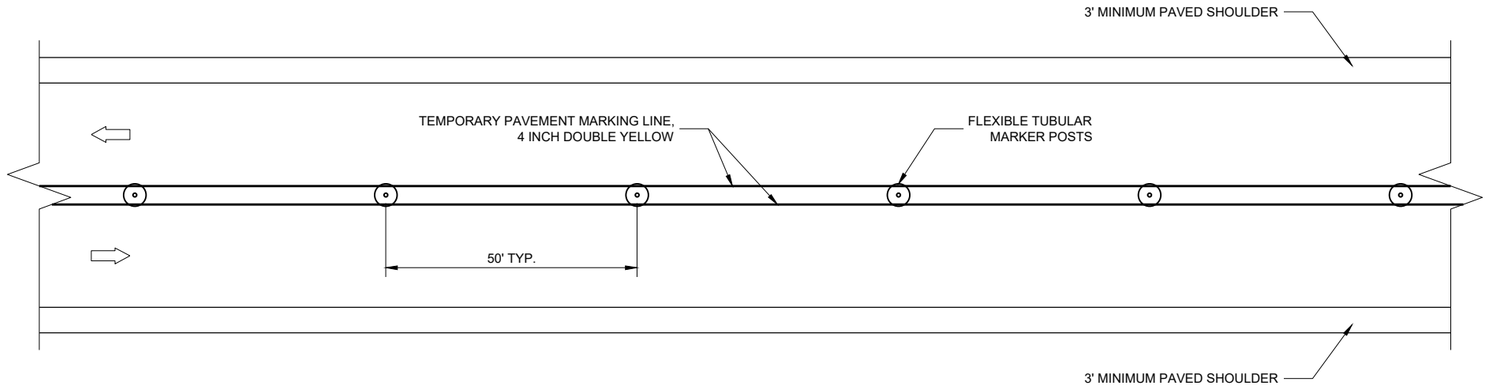
COVER EXISTING CENTERLINE STRIPE WITH TEMPORARY PAVEMENT MARKING LINE, 4 INCH DOUBLE YELLOW.

(1)

(2)

(1) THE WO6-3 AND WO57-51 SHALL BE LOCATED 200 FEET BEYOND THE END OF THE ACCELERATION LANE OF EACH ENTRANCE RAMP AND / OR 500 FEET BEYOND ANY SIDE ROAD. THE R4-1 SHALL BE LOCATED 1000 FEET BEYOND THE WO6-3 AND THE WO57-51 AND THE SIGNS SHALL BE ALTERNATED WITH ONE MILE INTERVALS BETWEEN THE SIGNS.

(2) CONVENTIONAL: 24" X 30"
FREEWAY AND EXPRESSWAY: 36" X 48"



TWO LANE, TWO WAY OPERATION

6

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SDD 15D06 - 05

SDD 15D06 - 05

TRAFFIC CONTROL
TWO LANE TWO WAY
OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

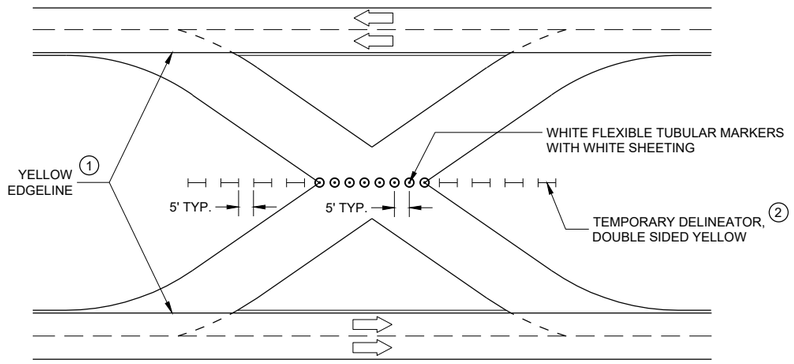
APPROVED
February 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- DELINEATOR FLEXIBLE / TUBULAR MARKER
- TEMPORARY DELINEATOR (STEEL POST WITH SINGLE DELINEATOR) COLOR OF DELINEATOR SHALL MATCH THE COLOR OF THE RESPECTIVE EDGELINE MARKING
- TEMPORARY DELINEATOR (DOUBLE SIDED)
- TYPE "A" WARNING LIGHT (FLASHING)
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- DIRECTION OF TRAFFIC
- REMOVE PAVEMENT MARKINGS
- WORK AREA



TRAFFIC CONTROL FOR CROSSOVER THAT IS NOT IN USE

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

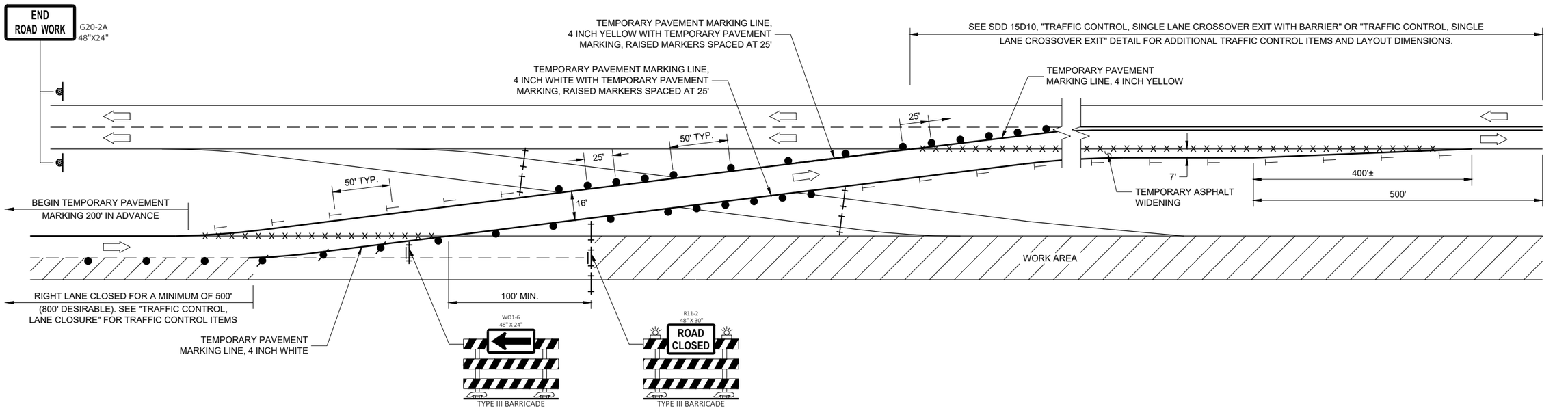
THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

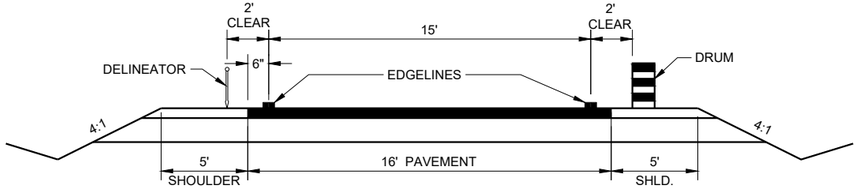
THE SPACING BETWEEN PROPOSED SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS.

REVERSE DEVICES WHEN OTHER LEG OF CROSSOVER IS IN USE.

(1) FOR PERMANENT CROSSOVER, PAVEMENT MARKING SHOULD CONFORM TO SECTION 646 OF THE STANDARD SPECIFICATIONS.

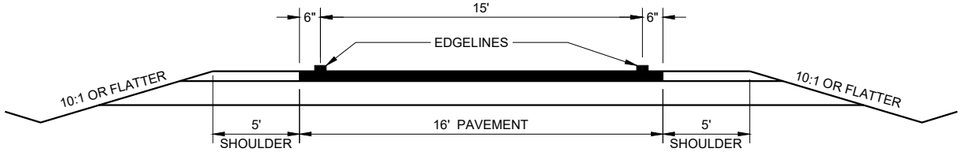
(2) FOR PERMANENT CROSSOVER, INSTALL PERMANENT DELINEATORS ACCORDING TO SECTION 633 OF THE STANDARD SPECIFICATIONS.





TYPICAL TEMPORARY CROSSOVER ROADWAY DIMENSIONS

(SEE PLAN FOR ROADWAY DESIGN ELEMENTS)



TYPICAL CROSSOVER TO REMAIN IN PLACE ROADWAY DIMENSIONS

(SEE PLAN FOR ROADWAY DESIGN ELEMENTS)

TRAFFIC CONTROL, SINGLE LANE CROSSOVER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

SDD 15D11 - 08

SDD 15D11 - 08

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- REMOVE PAVEMENT MARKING (SEE GENERAL NOTES)
- WORK AREA

GENERAL NOTES

FOR WORK ON ROADWAYS WITH SPEEDS GREATER THAN 45MPH, USE SDD 15D12.

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

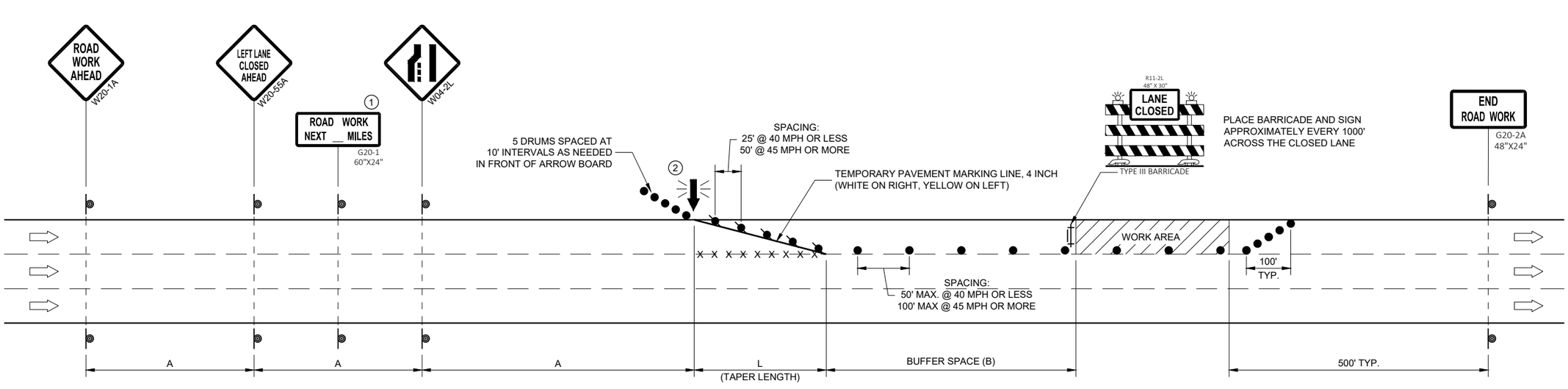
BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

(1) OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

(2) WHERE THE SHOULDER OR TERRACE HAS INSUFFICIENT SPACE TO PLACE THE ARROW BOARD AS SHOWN, PLACE THE ARROW BOARD AT THE END OF THE TAPER.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'

TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

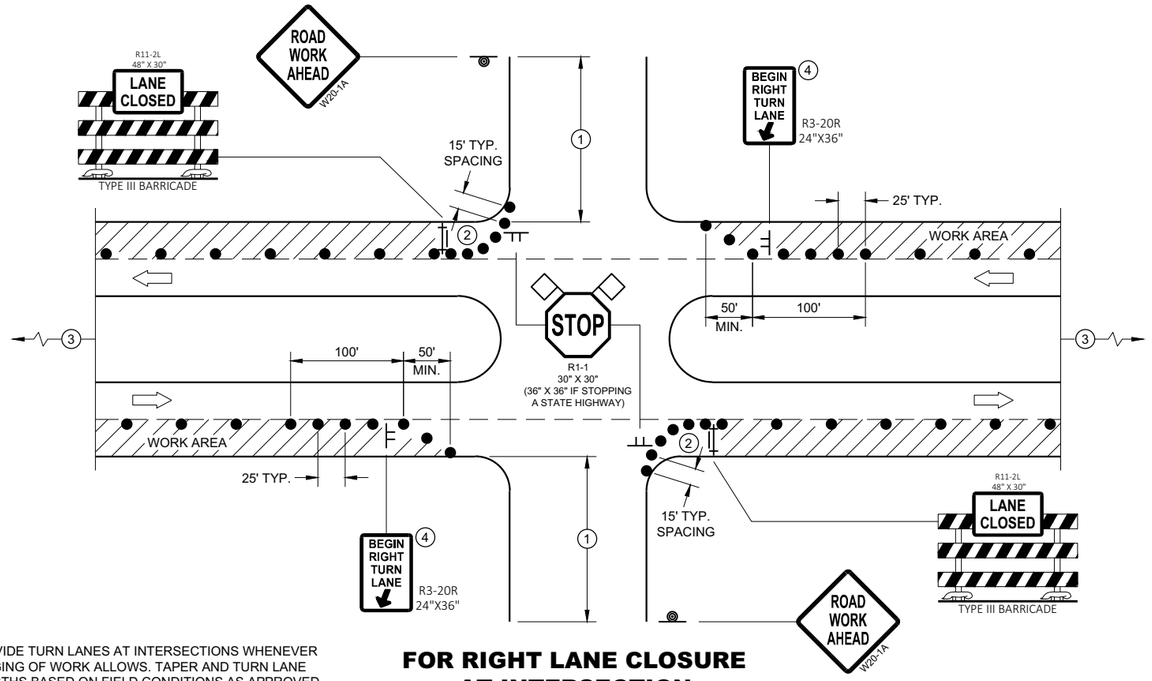
APPROVED
February 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

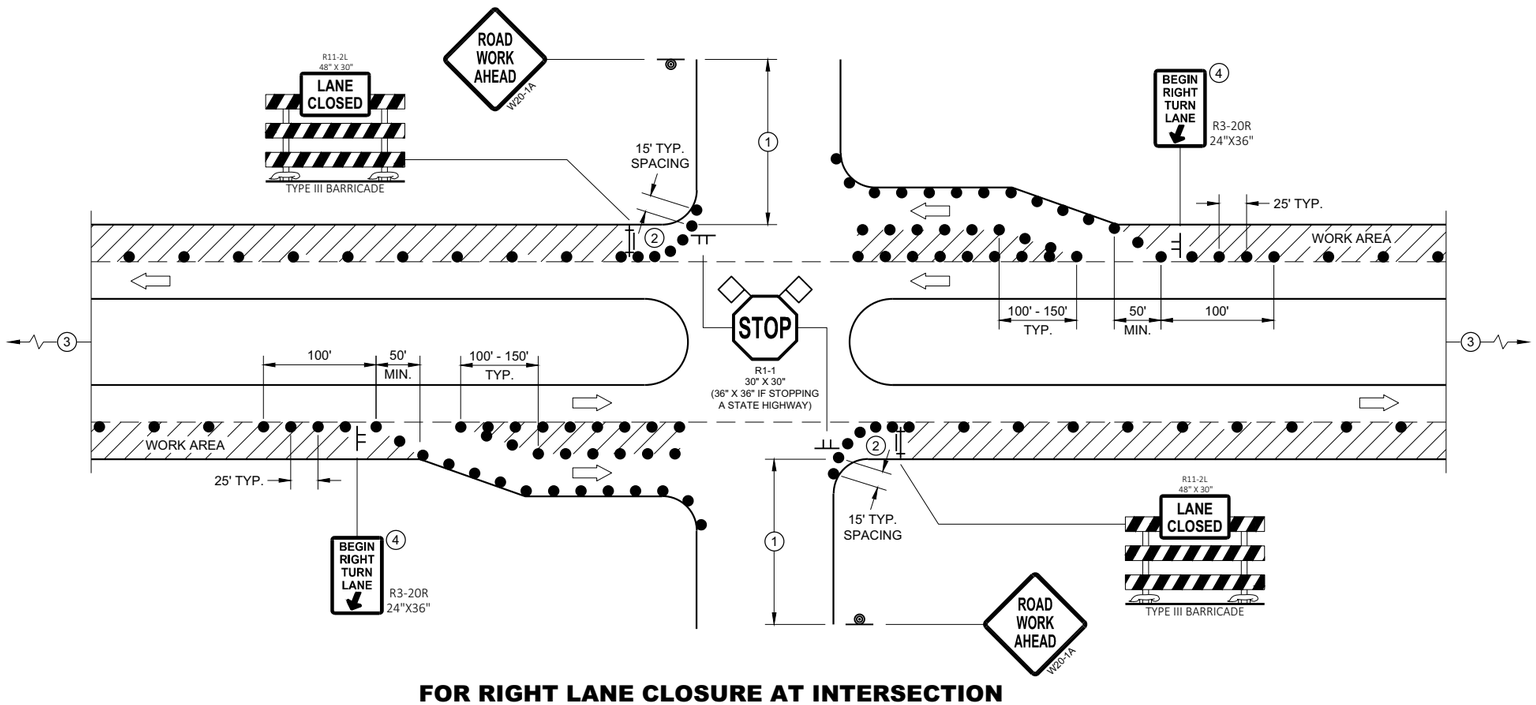
6

SDD 15D20 - 06a
SDD15D20 - 06a



PROVIDE TURN LANES AT INTERSECTIONS WHENEVER STAGING OF WORK ALLOWS. TAPER AND TURN LANE LENGTHS BASED ON FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

FOR RIGHT LANE CLOSURE AT INTERSECTION



FOR RIGHT LANE CLOSURE AT INTERSECTION (WITH RIGHT TURN BAY OPEN)

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" MAY BE USED IF APPROVED BY THE DISTRICT TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL REMAIN IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON PORTABLE SUPPORTS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

(1) 500' TYPICAL OR AT LAST INTERSECTION, WHICHEVER IS CLOSER.
350' IF 35 - 40 MPH.
200' IF 25 - 30 MPH.

(2) ALSO USE BARRICADE AND 15 FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS

(3) SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.

(4) MINIMUM MOUNTING HEIGHT OF 5 FEET FROM EDGE OF PAVEMENT (AT EDGE LINE LOCATION) TO BOTTOM OF SIGN.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- DIRECTION OF TRAFFIC
- FLAGS, 16" X 16" MIN., ORANGE
- WORK AREA

TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE RIGHT LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 15D21 - 07a

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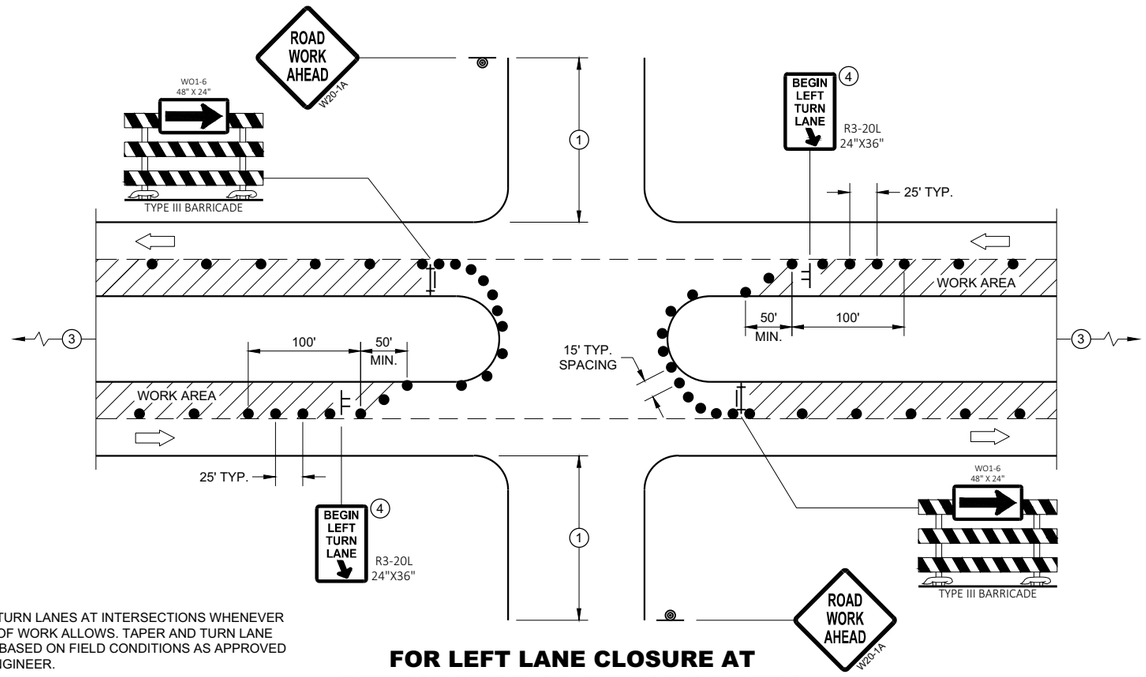
SDD 15D21 - 07a



PROVIDE TURN LANES AT INTERSECTIONS WHENEVER STAGING OF WORK ALLOWS. TAPER AND TURN LANE LENGTHS BASED ON FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

FOR LEFT LANE CLOSURE AT INTERSECTION OR MEDIAN OPENING

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" MAY BE USED IF APPROVED BY THE DISTRICT TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL REMAIN IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON PORTABLE SUPPORTS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

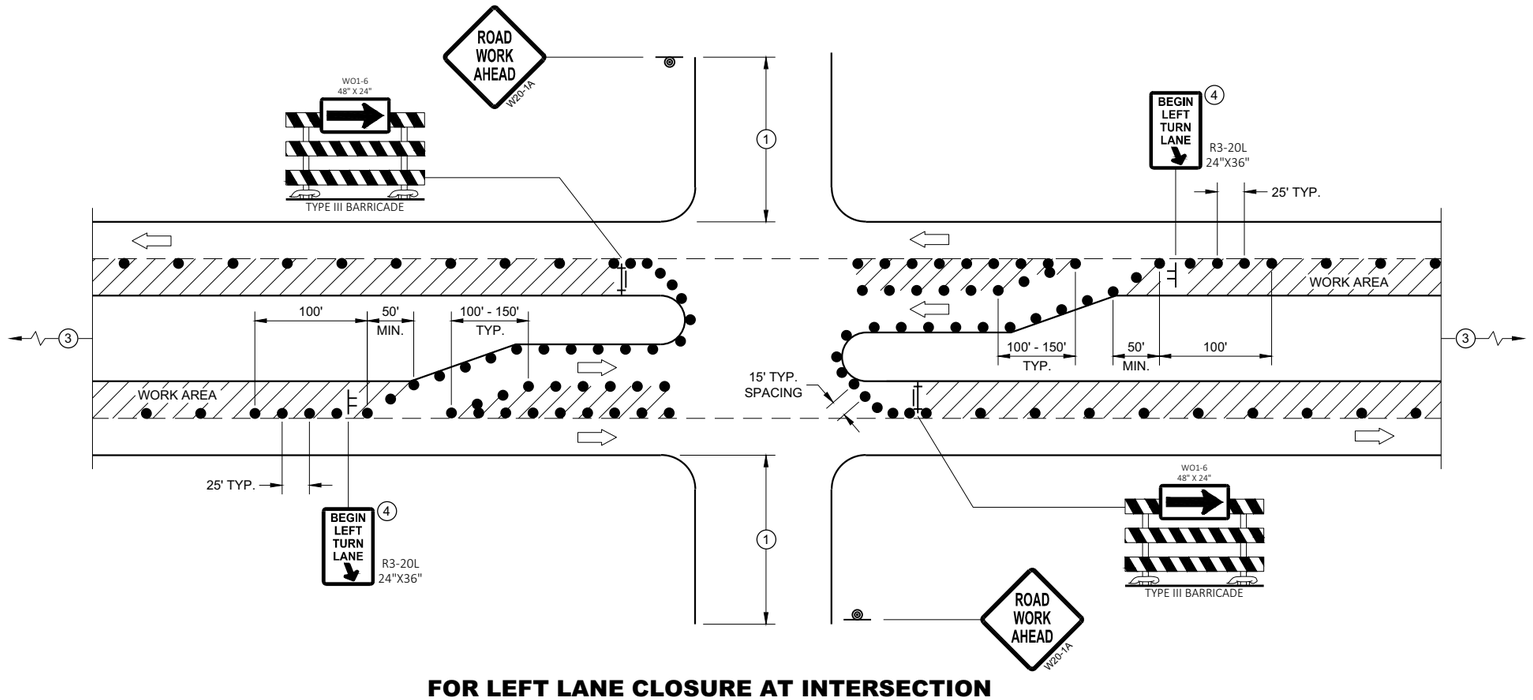
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

(1) 500' TYPICAL OR AT LAST INTERSECTION, WHICHEVER IS CLOSER.
350' IF 35 - 40 MPH.
200' IF 25 - 30 MPH.

(2) ALSO USE BARRICADE AND 15 FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS

(3) SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.

(4) MINIMUM MOUNTING HEIGHT OF 5 FEET FROM EDGE OF PAVEMENT (AT EDGE LINE LOCATION) TO BOTTOM OF SIGN.



FOR LEFT LANE CLOSURE AT INTERSECTION OR MEDIAN OPENING (WITH LEFT TURN BAY OPEN)

LEGEND

- SIGN ON TEMPORARY SUPPORT
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- DIRECTION OF TRAFFIC
- FLAGS, 16" X 16" MIN., ORANGE
- WORK AREA

TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LEFT LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
August 2020
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

SDD 15D21 - 07b

SDD 15D21 - 07b

LEGEND

- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- REMOVE PAVEMENT MARKING (SEE GENERAL NOTES)
- WORK AREA

GENERAL NOTES

FOR WORK ON ROADWAYS WITH SPEEDS GREATER THAN 45 MPH, USE SDD 15D14.

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE RIGHT TWO LANES.
FOR CLOSING LEFT TWO LANES, REVERSE THE TRAFFIC CONTROL.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINES IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

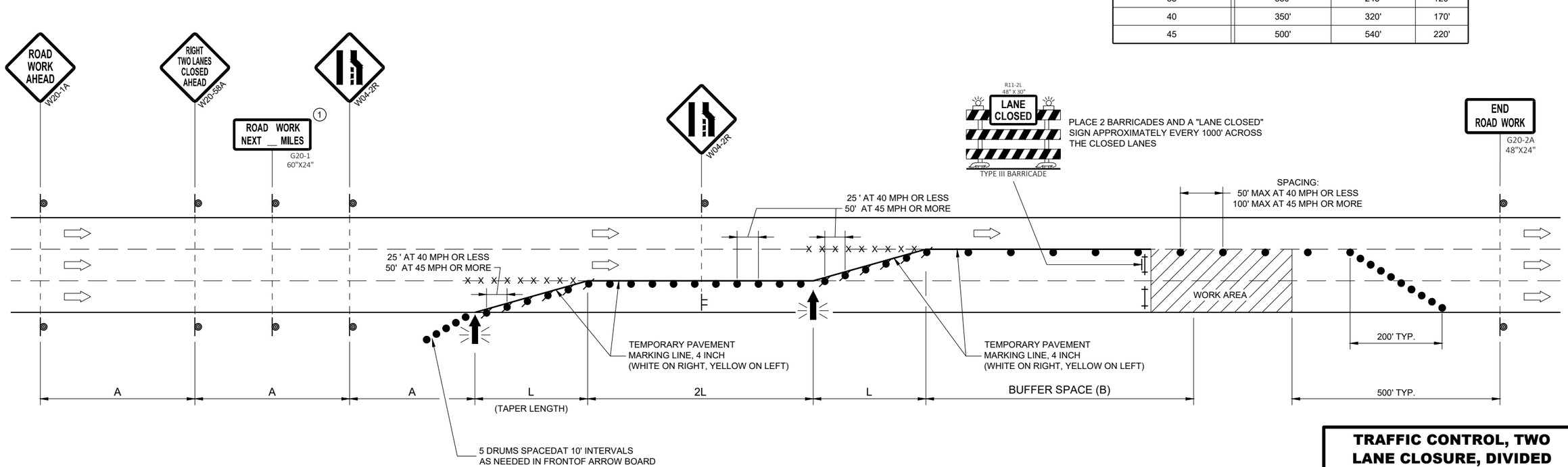
BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

(1) OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'



TRAFFIC CONTROL, TWO LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6

SDD 15D22 - 05

6

SDD 15D22 - 05

LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- WORK AREA
- REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- CONCRETE BARRIER TEMPORARY PRECAST

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR LANE SHIFT RIGHT - REVERSE FOR SHIFTING LEFT.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON ANY "COVERED" OR "DOWNED" SIGNS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINES IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

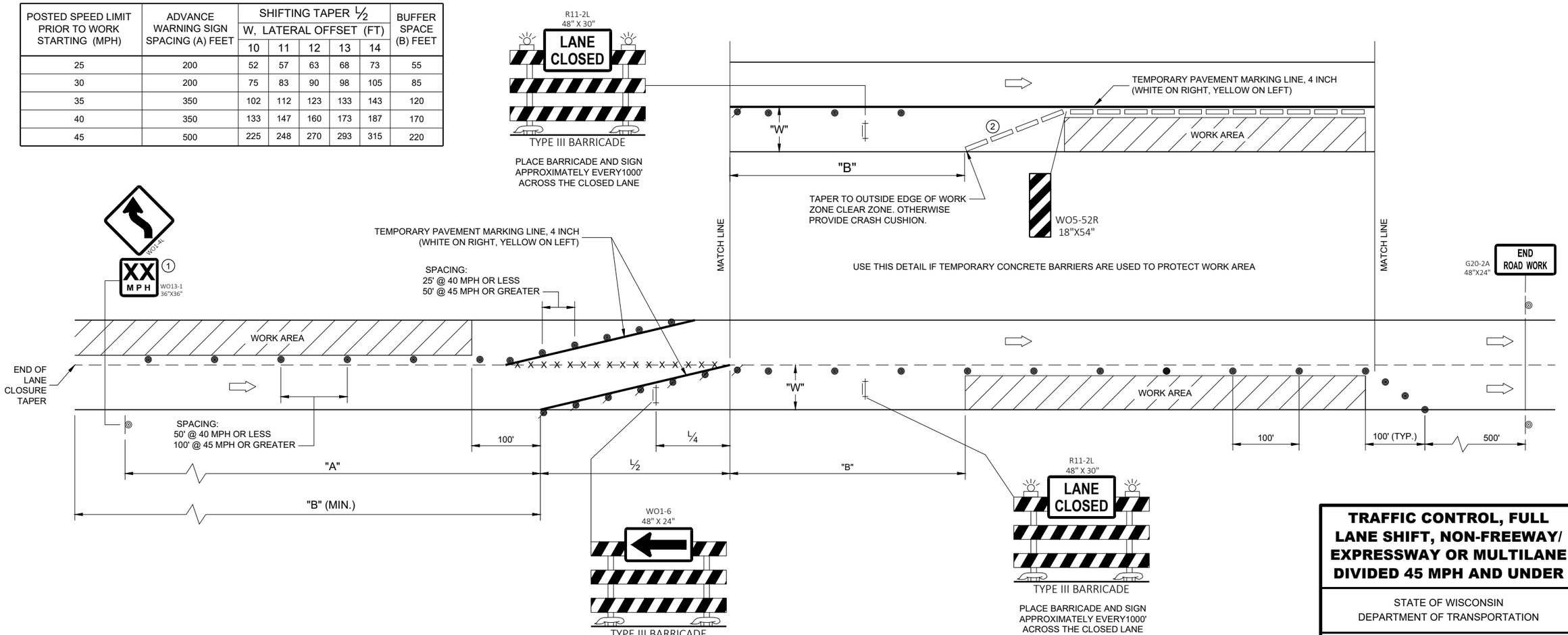
IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE SHIFT OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE SHIFT MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER
SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL
CHANGE SUCH AS A CROSSOVER MANEUVER.

(1) USE ONLY IF DESIGN SPEED IS 10 MPH BELOW POSTED SPEED.

(2) BARRIER FLARE RATE: 6:1 @ 40 MPH OR LESS
8:1 @ 45 MPH OR GREATER

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHIFTING TAPER L/2 W, LATERAL OFFSET (FT)					BUFFER SPACE (B) FEET
		10	11	12	13	14	
25	200	52	57	63	68	73	55
30	200	75	83	90	98	105	85
35	350	102	112	123	133	143	120
40	350	133	147	160	173	187	170
45	500	225	248	270	293	315	220



TRAFFIC CONTROL, FULL LANE SHIFT, NON-FREEWAY/ EXPRESSWAY OR MULTILANE DIVIDED 45 MPH AND UNDER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

6

SDD 15D40 - 04a

6

SDD 15D40 - 04a

LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- WORK AREA
- REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- CONCRETE BARRIER TEMPORARY PRECAST

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR LANE SHIFT LEFT - REVERSE FOR SHIFTING RIGHT.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON ANY "COVERED" OR "DOWNED" SIGNS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINES IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

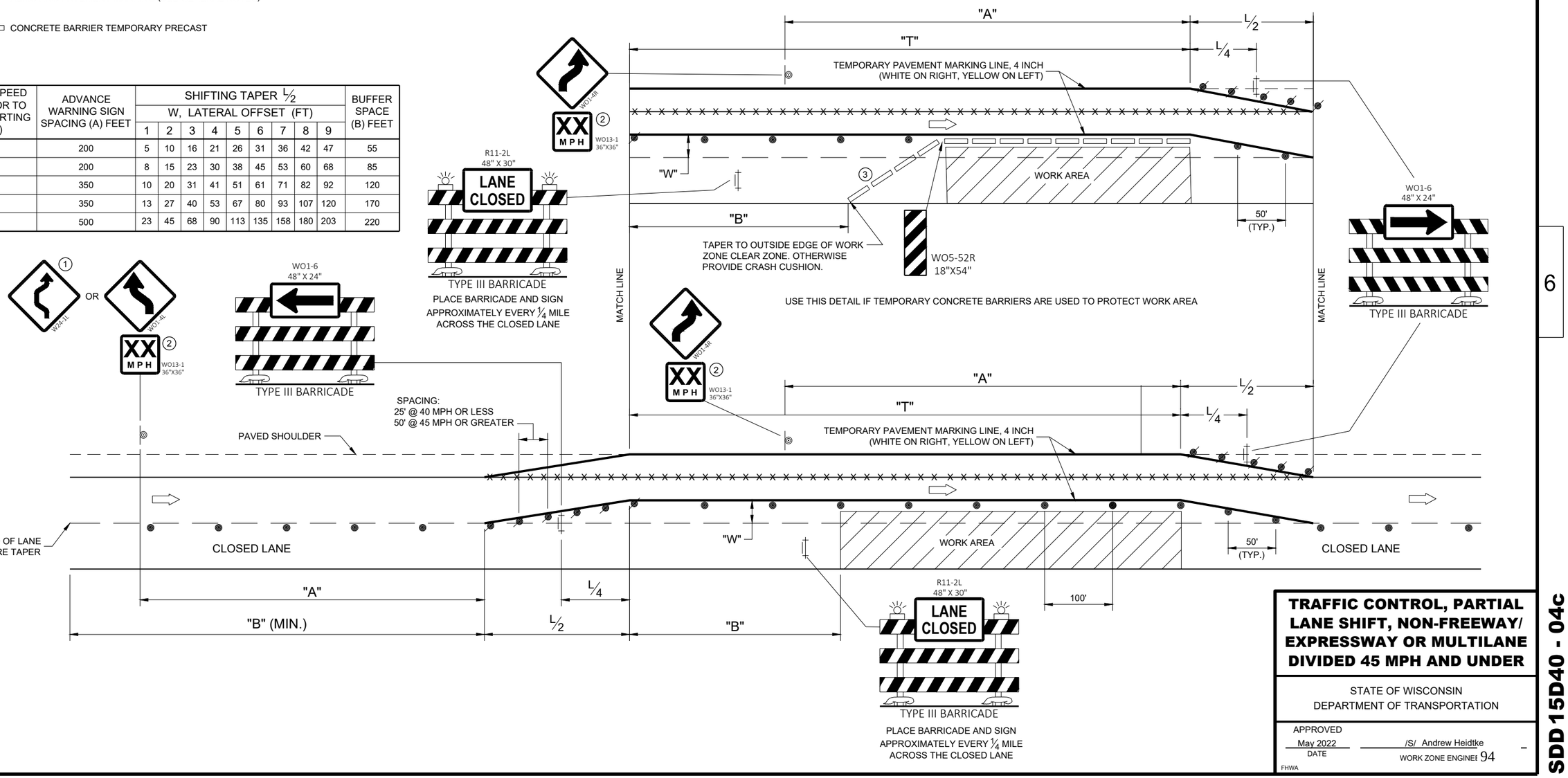
ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE SHIFT OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE SHIFT MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

(1) USE ONLY WHEN T<600', OMIT WO1-4R.

(2) IF NEEDED, USE ONLY IF DESIGN SPEED IS 10 MPH BELOW POSTED SPEED.

(3) BARRIER FLARE RATE: 6:1 @ 40 MPH OR LESS
8:1 @ 45 MPH OR GREATER

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHIFTING TAPER L/2 W, LATERAL OFFSET (FT)									BUFFER SPACE (B) FEET
		1	2	3	4	5	6	7	8	9	
25	200	5	10	16	21	26	31	36	42	47	55
30	200	8	15	23	30	38	45	53	60	68	85
35	350	10	20	31	41	51	61	71	82	92	120
40	350	13	27	40	53	67	80	93	107	120	170
45	500	23	45	68	90	113	135	158	180	203	220



TRAFFIC CONTROL, PARTIAL LANE SHIFT, NON-FREEWAY/ EXPRESSWAY OR MULTILANE DIVIDED 45 MPH AND UNDER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

SDD 15D40 - 04c

SDD 15D40 - 04c

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

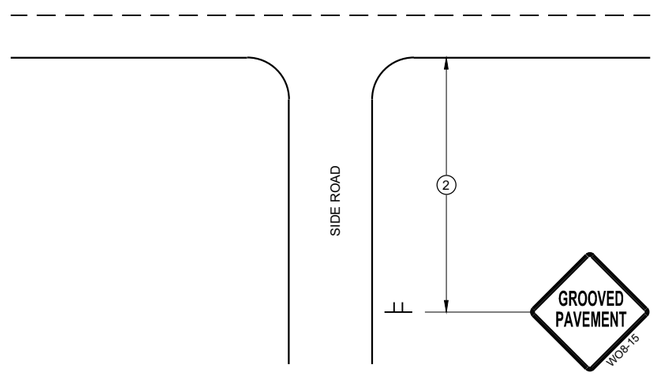
(1) PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.

(2) PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

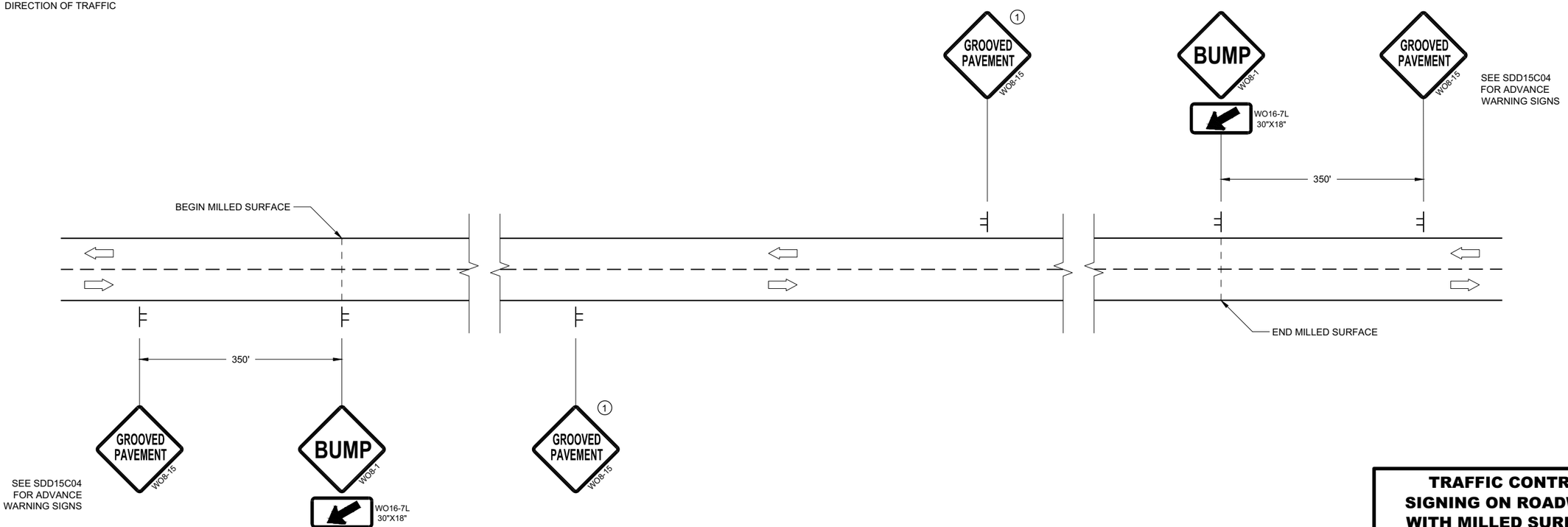
LEGEND

SIGN ON TEMPORARY SUPPORT

DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH SIGN DETAIL



DETAIL FOR SIGNING ON MILLED SURFACES

TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

SDD 15D44 - 02

SDD 15D44 - 02

LEGEND

V1 WORK VEHICLE

V2 SHADOW VEHICLE

TRUCK MOUNTED ATTENUATOR (TMA)

FLASHING ARROW PANEL (CAUTION)

WORK AREA

DIRECTION OF TRAFFIC

POSTED SPEED PRIOR TO WORK STARTING (MPH)	DECISION SIGHT DISTANCE (D)
0 - 25	550'
30	550'
35	700'
40	700'
45	900'
50	900'
55	1200'

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

MOBILE IS WORK THAT MOVES CONTINUOUSLY OR MOVES AT LEAST THE DECISION SIGHT DISTANCE EVERY 15 MINUTES.

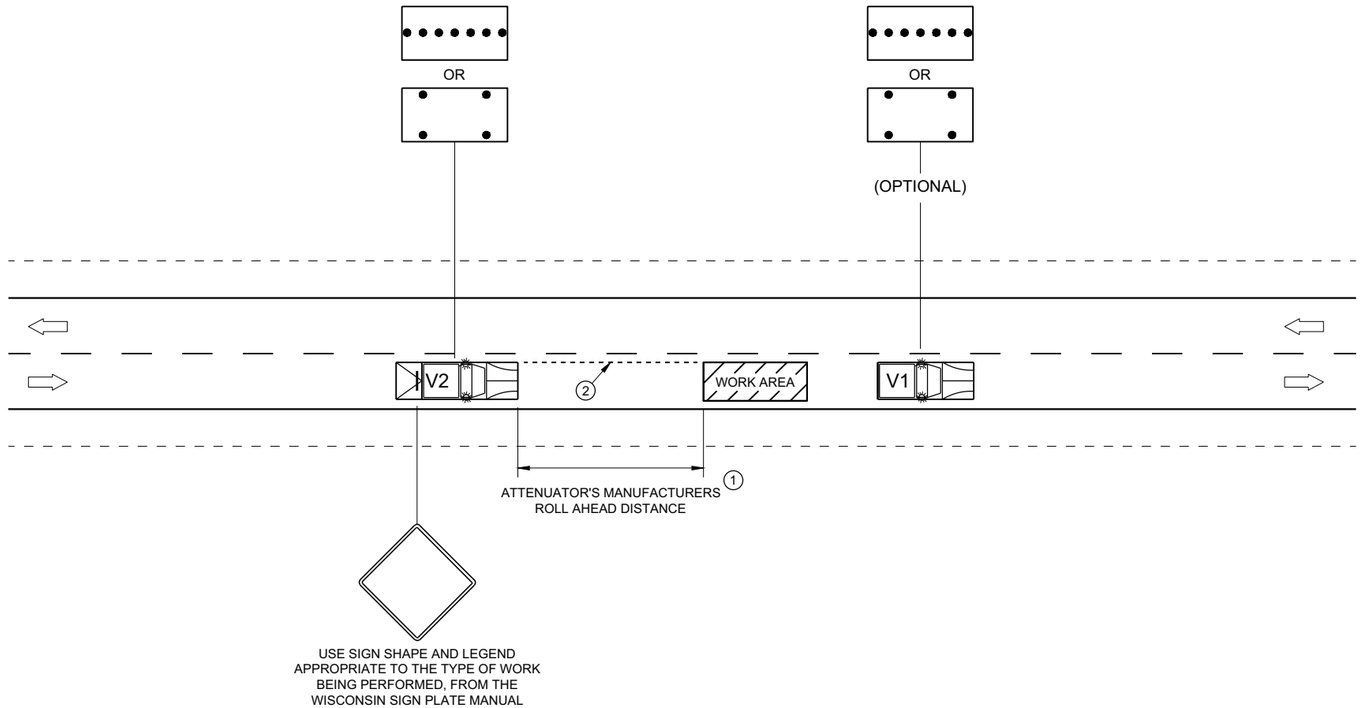
ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL ARROW PANELS SHALL BE REAR FACING, TYPE "B" OR "C", AND DISPLAYING THE FLASHING CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

(1) DISTANCE BETWEEN VEHICLES MAY INCREASE FROM THE ATTENUATOR'S ROLL AHEAD BASED ON TERRAIN, SIGHT DISTANCE, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

(2) ALIGN LEFT SIDE OF SHADOW VEHICLE WITH EDGE OF WORK AREA.



TRAFFIC CONTROL, MOBILE OPERATIONS ON AN UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 — DATE
/S/ Andrew Heidtke — STATEWIDE WORK ZONE T SAFETY ENGINEER
FHWA

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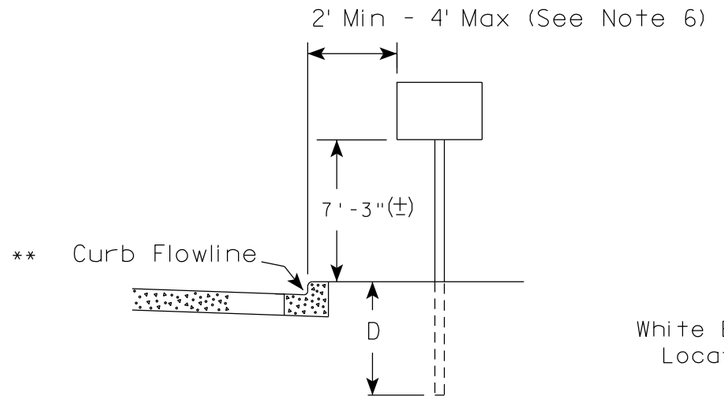
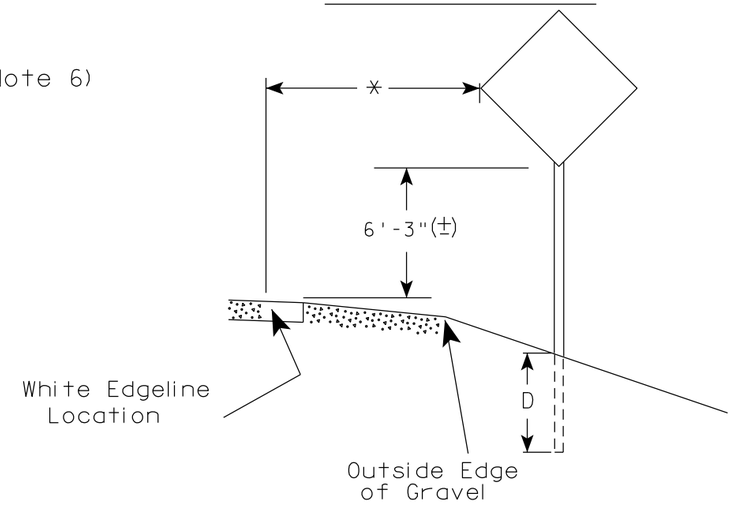
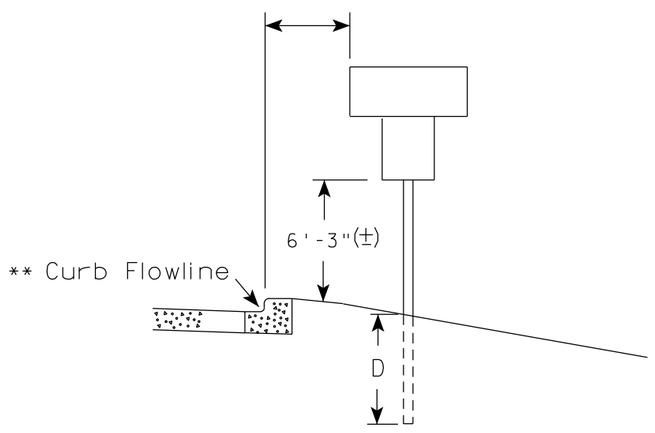
SDD 15D51 - 01

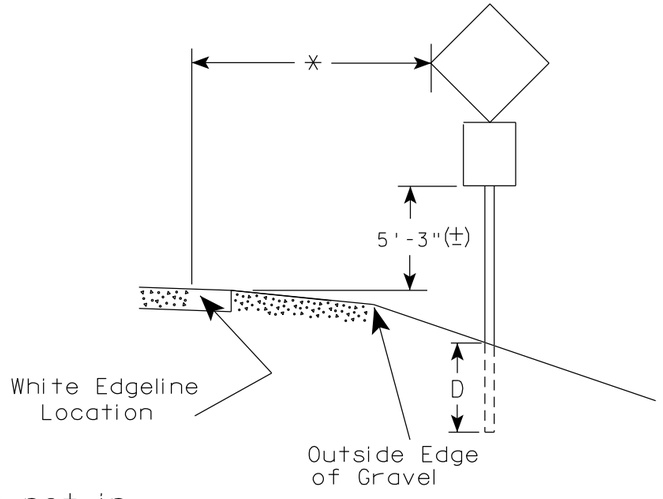
URBAN AREA



RURAL AREA (See Note 2)







** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
   The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directd by the Engineer.

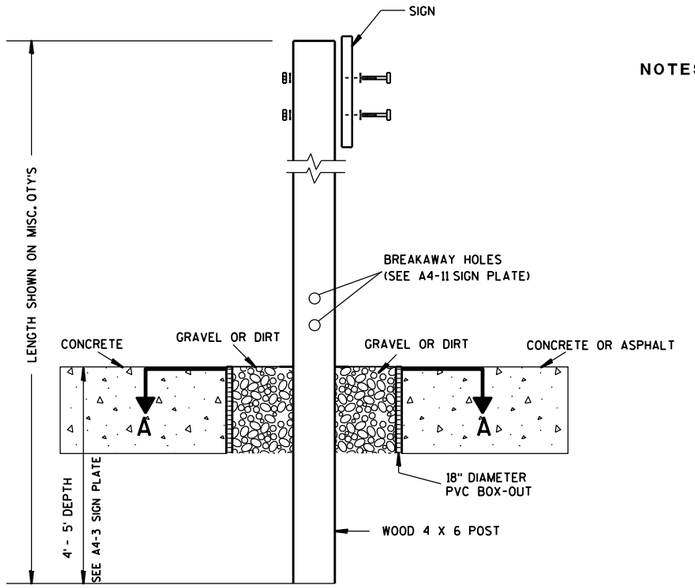
TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/13/2020 PLATE NO. A4-3.22

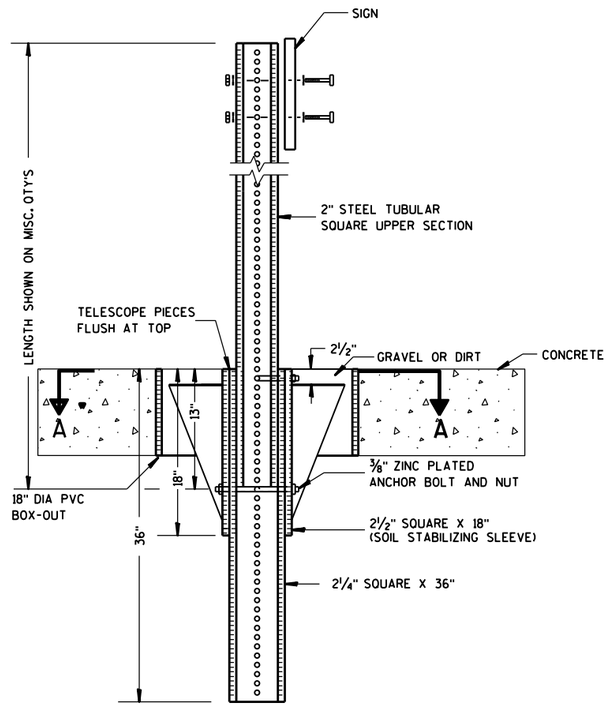
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 97	E



ELEVATION VIEW

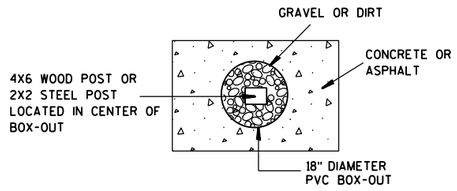
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

NOTES:
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

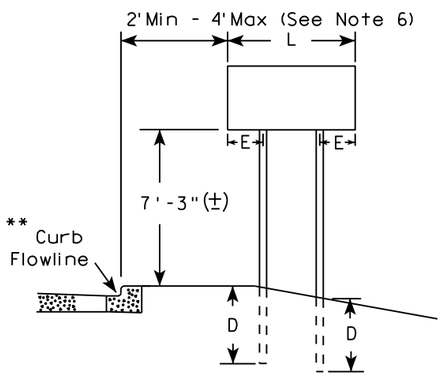
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

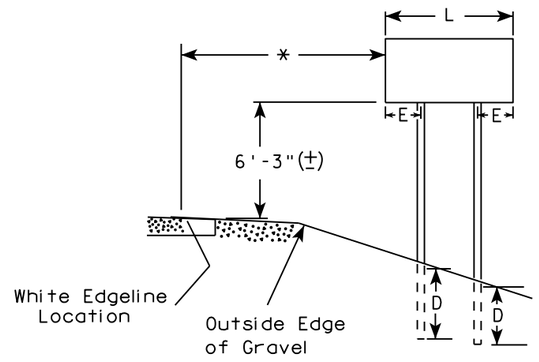
DATE 1/27/14 PLATE NO. A4-3B.1

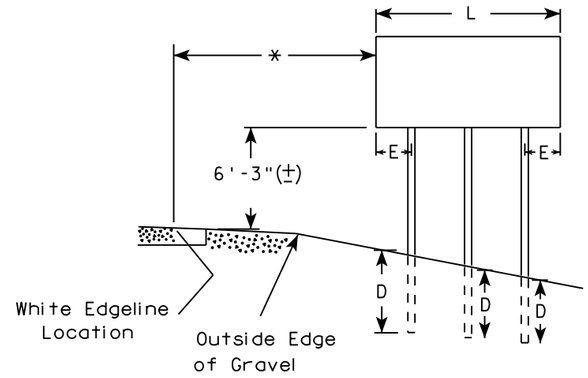
PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

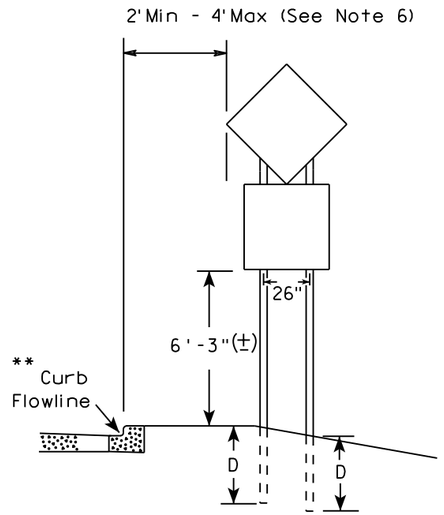
URBAN AREA



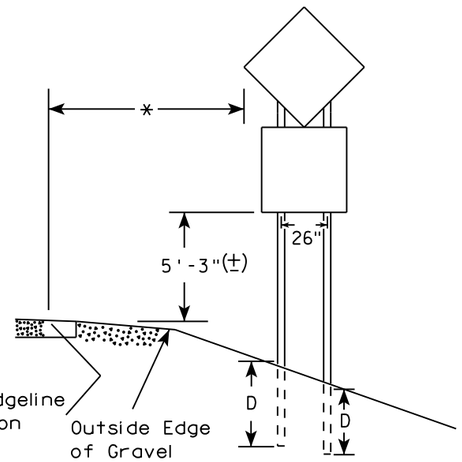
RURAL AREA (See Note 3)







48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)		
	L	E
***	Greater than 48" Less than 60"	12"
	60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

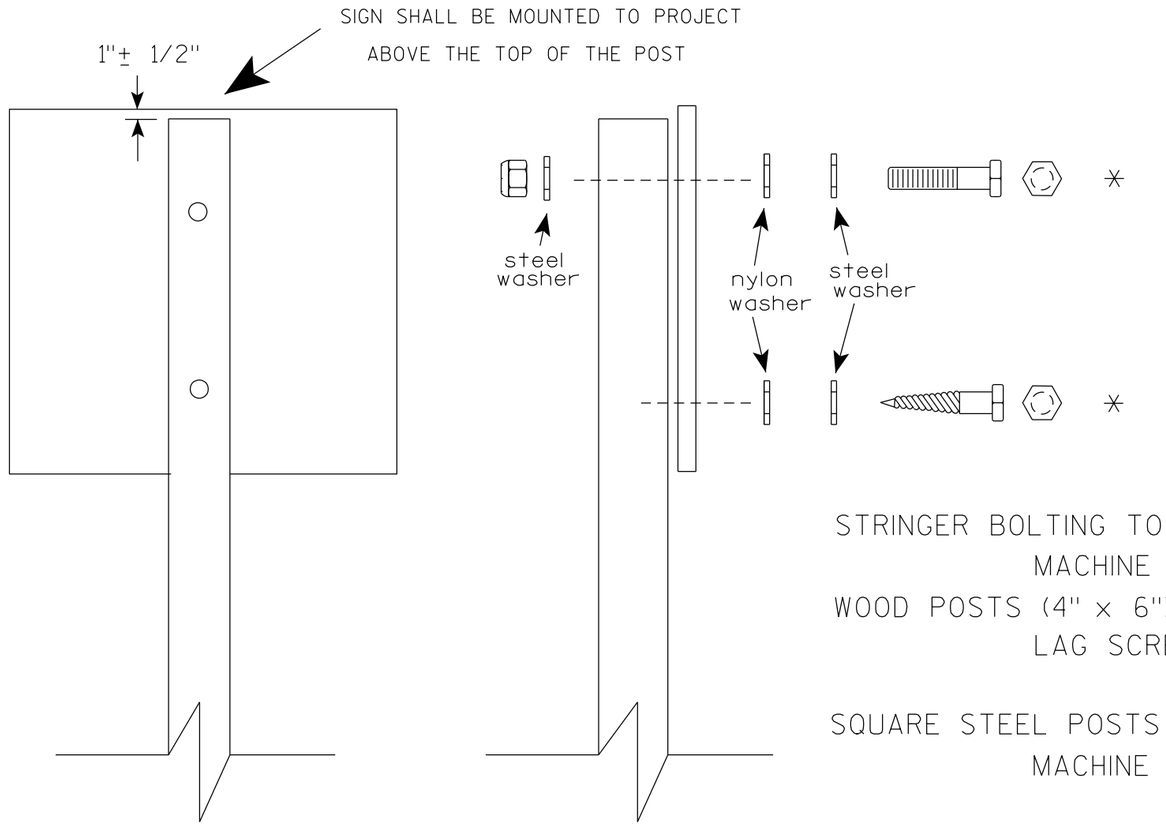
TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 8/21/17 PLATE NO. A4-4.15

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 99	E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3

b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

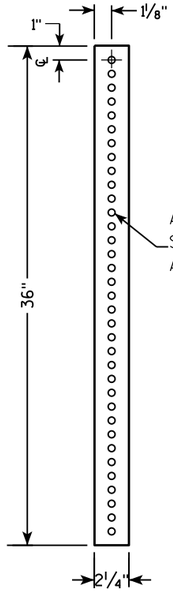
DATE 4/1/2020 PLATE NO. A4-8.9

PROJECT NO:

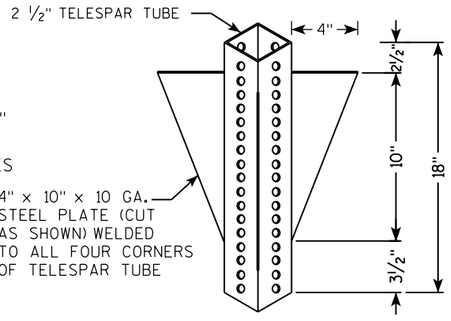
SHEET NO: 100 E

TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM

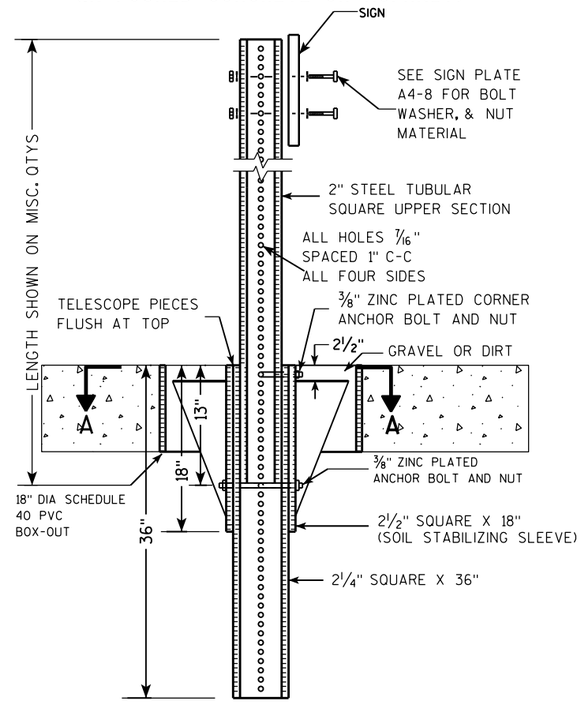
2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



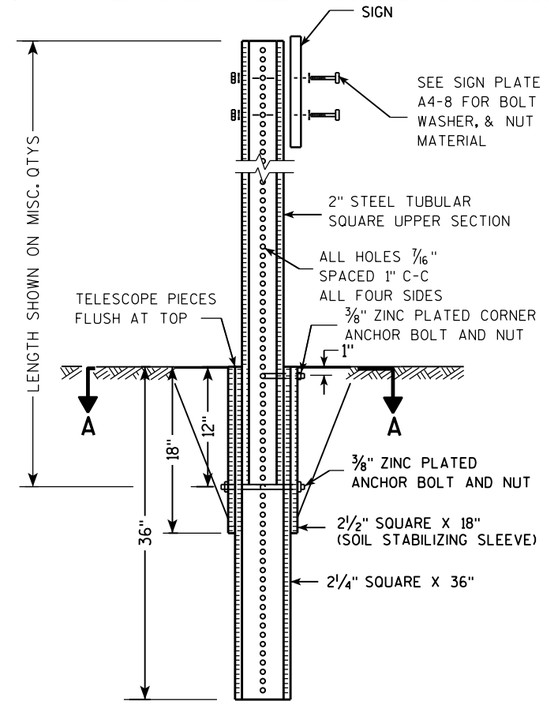
2 1/2 " SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH

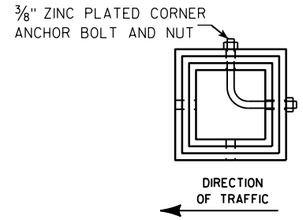


DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)





SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

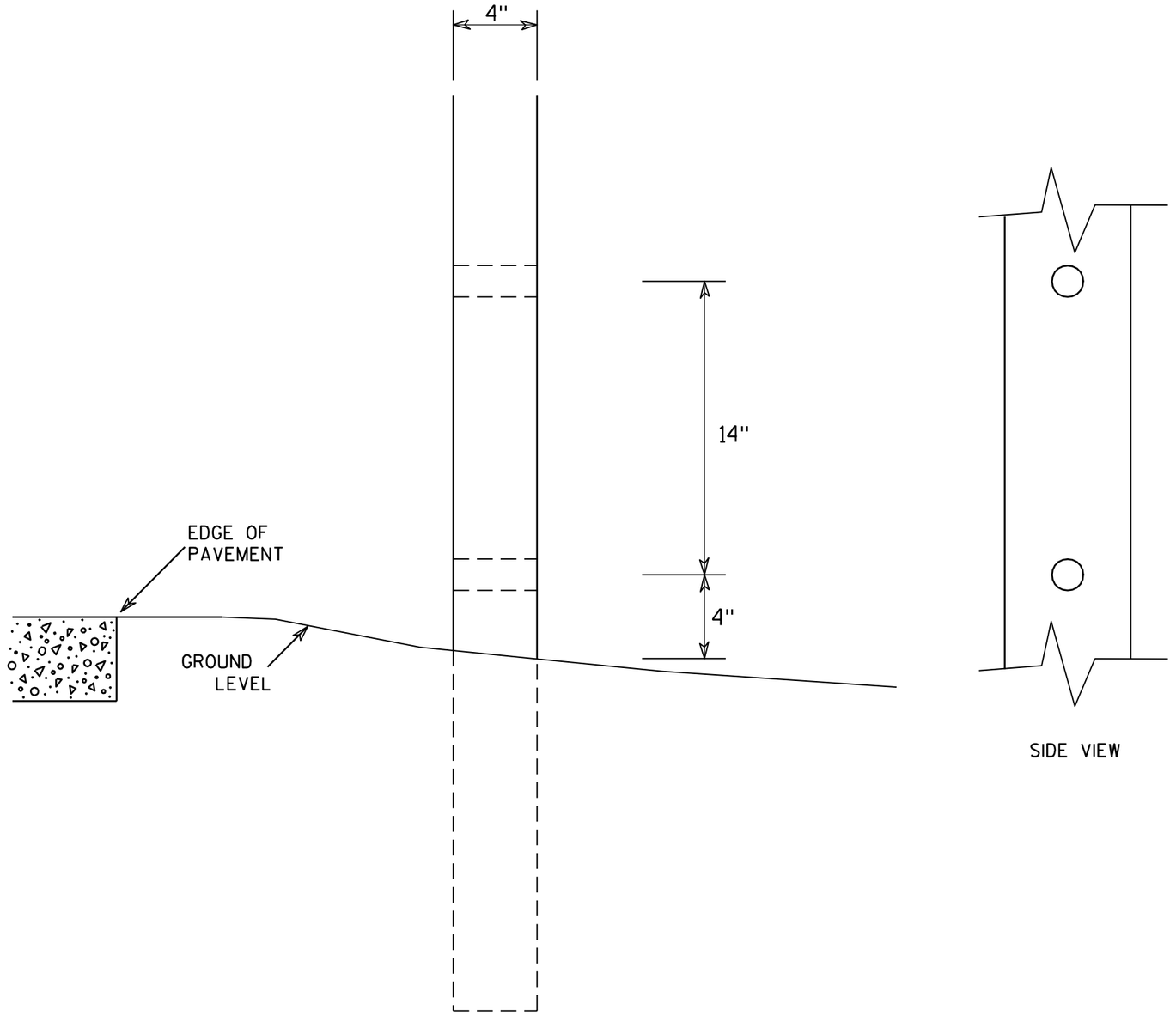
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/05/15 PLATE A4-9.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE : 05-FEB-2015 17:09 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

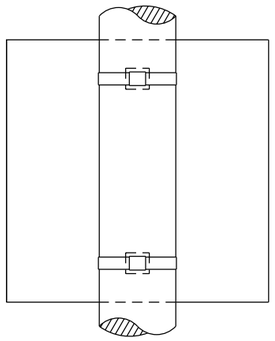
APPROVED Chester J Spang for State Traffic Engineer

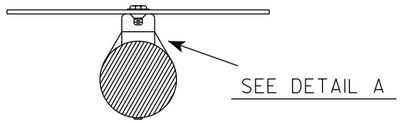
DATE 3/27/97 PLATE NO. A4-11.2

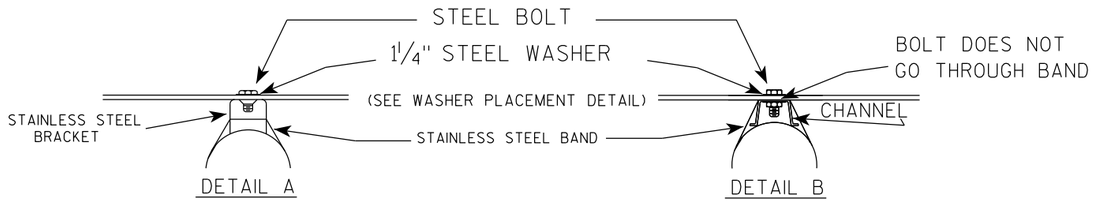
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 102	E

BANDING

SINGLE SIGN



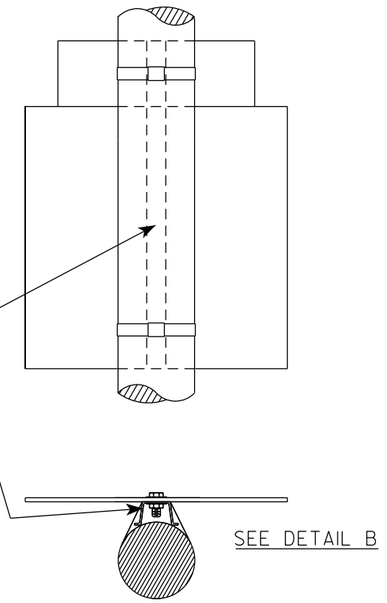




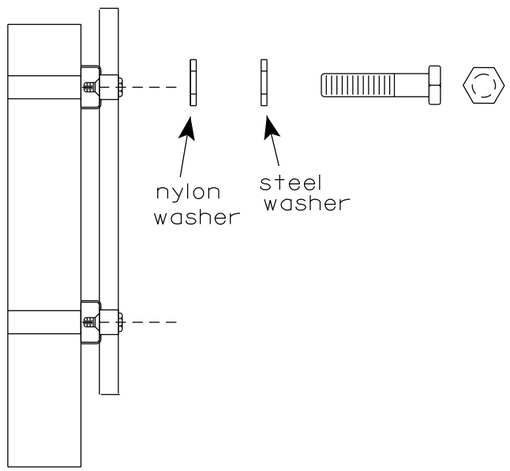
GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be ¾" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



WASHER PLACEMENT



WASHERS (ALL POSTS) -
1-1/4" O.D. X ⅜" I.D. X 1/16" STEEL
1-1/4" O.D. X ⅜" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

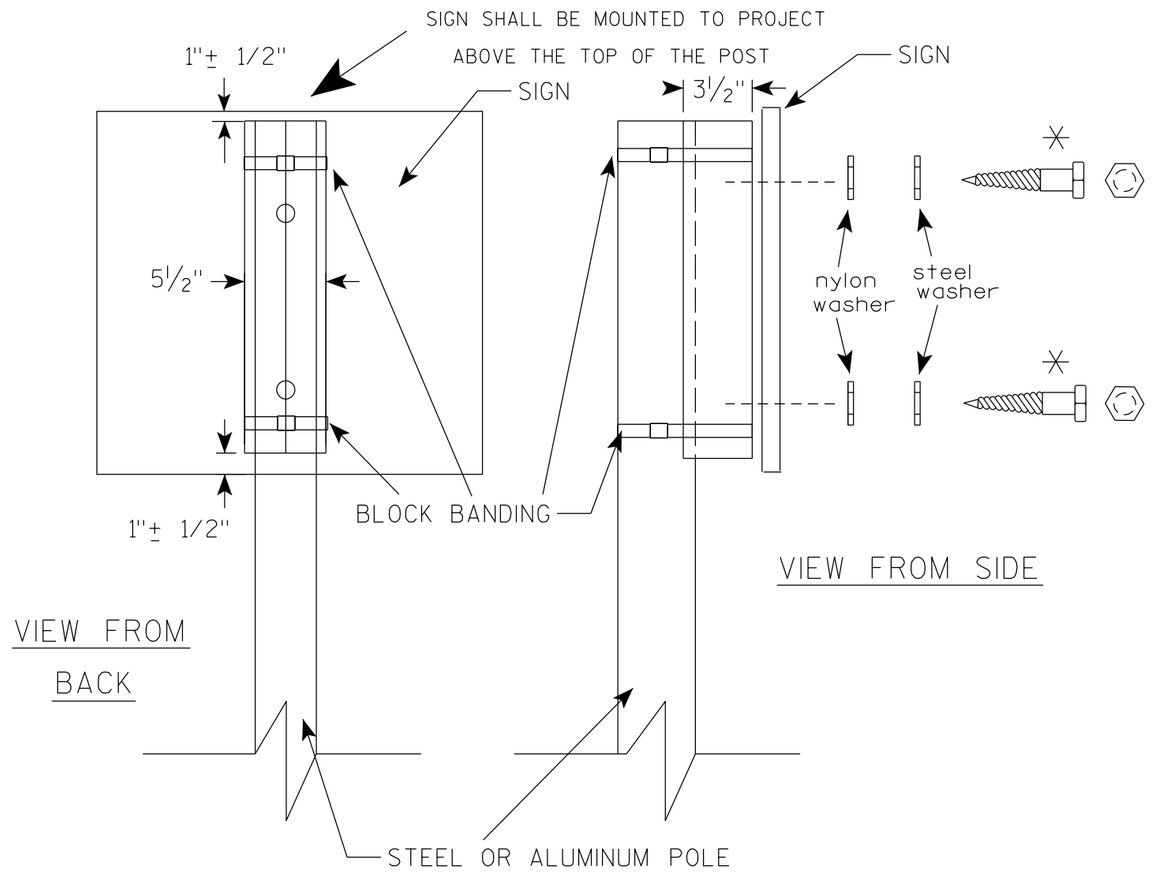
WISCONSIN DEPT OF TRANSPORTATION

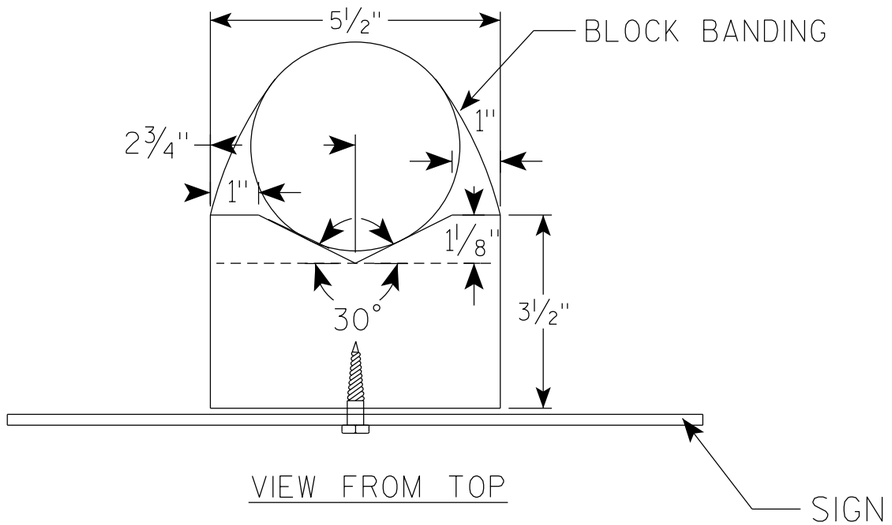
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 6/10/19 PLATE NO. A5-9.4

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 103	E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A59.dgn PLOT DATE : 10-JUN 2019 4:10 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : $$.....plotscale.....$$





GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WisDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, ¾" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORNALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✳ LAG BOLTS SHALL BE ⅜" X 2½"

BLOCK BANDING DETAIL (V-BLOCK OPTION)

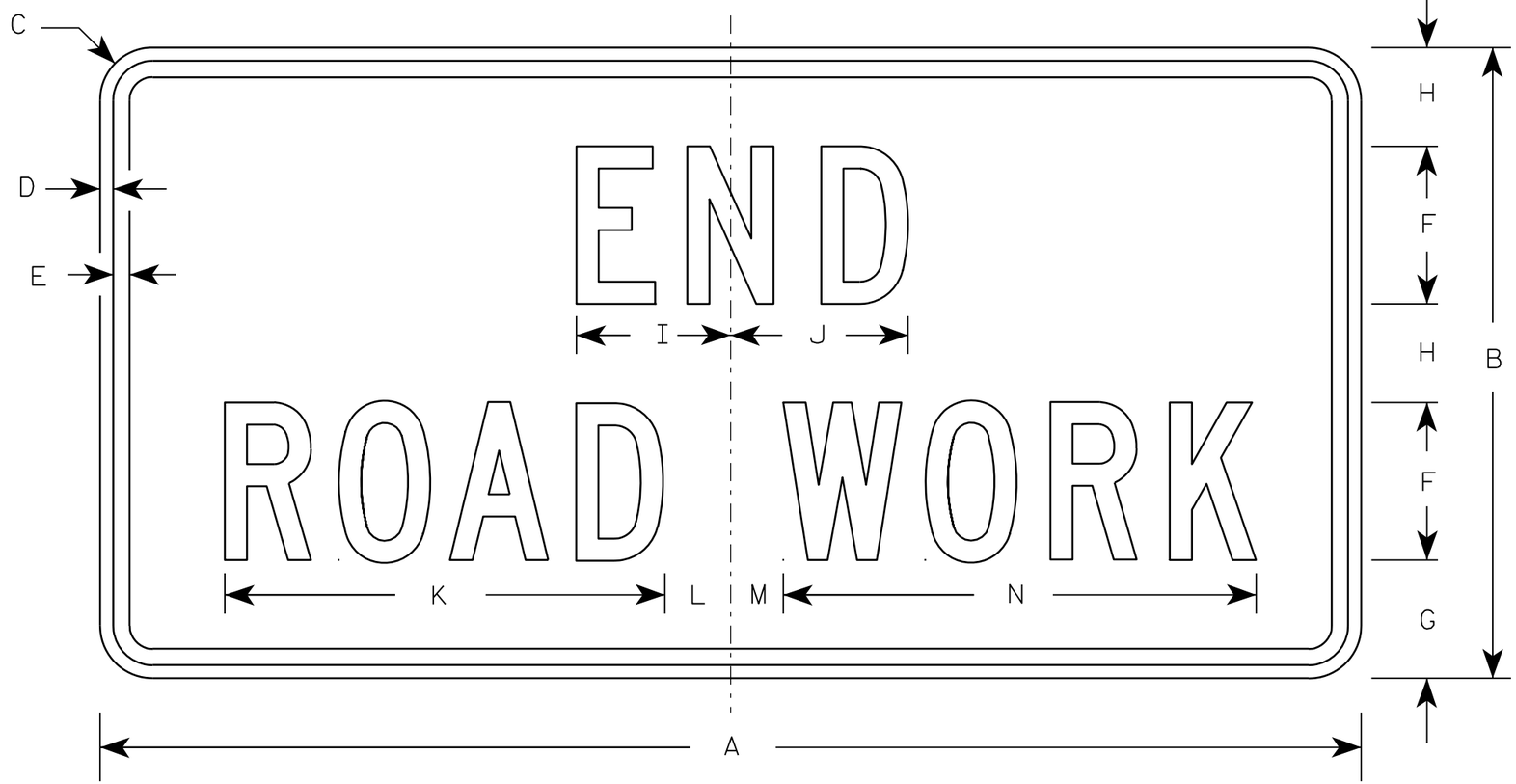
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 4/19/2022 PLATE NO. A5-10.3

PROJECT NO:

SHEET NO: 104 E



G20-2A

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

Metric equivalent for this sign is:

SIZE	
1	900 mm X 450 mm
2	1200 mm X 600 mm
3	1200 mm X 600 mm
4	1200 mm X 600 mm
5	1200 mm X 600 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m2
1	36	18	1 1/8	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5	0.41
2	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
3	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
4	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
5	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72

STANDARD SIGN
G20-2A

WISCONSIN DEPT OF TRANSPORTATION

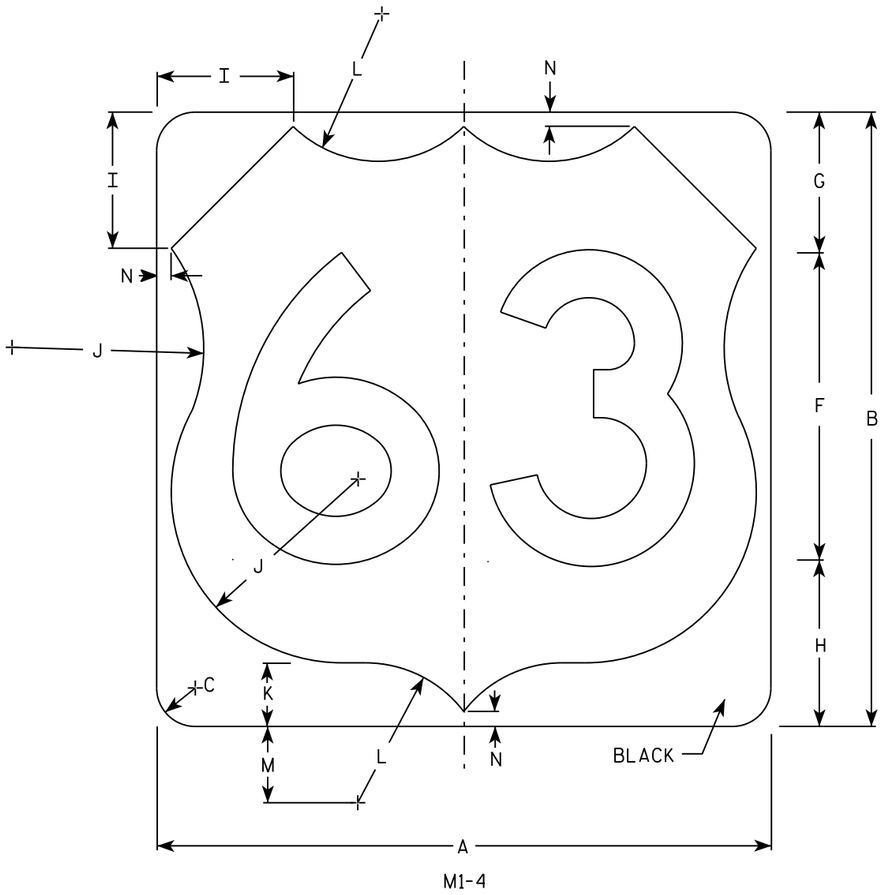
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 9/30/09 PLATE NO. G20-2A.8

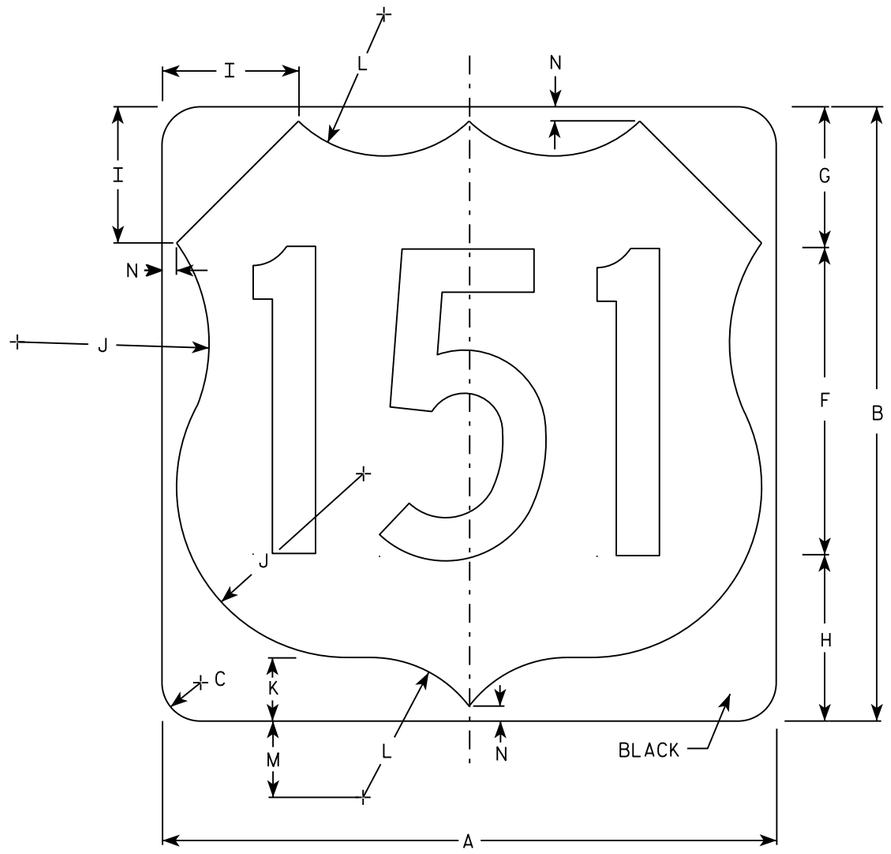
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 105	E

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - D except 3 number signs Series C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



M1-4



M1-4

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area Sq. ft.
1																											
2	24	24	1 1/2			12	5 1/2	6 1/2	5	7 1/2	2 1/2	5 1/2	3	1/2													4.0
3	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0
4	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0
5	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0

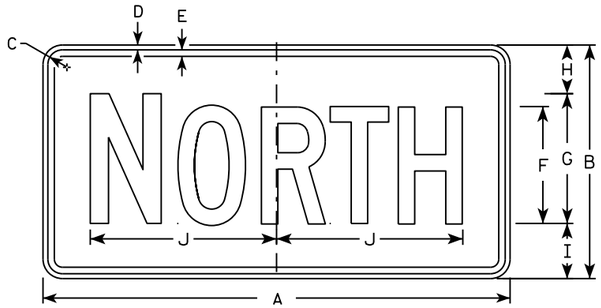
USH MARKER
M1-4 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

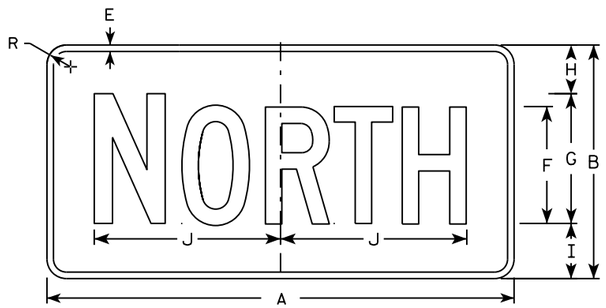
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/16/18 PLATE NO. M1-4.10

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 106	E



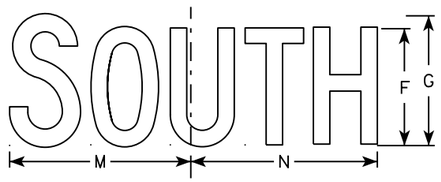
M3-1
MM3-1
MP3-1



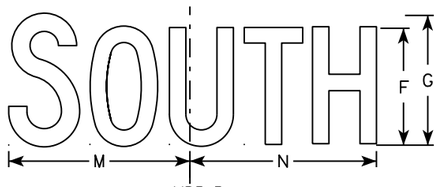
MB3-1
MK3-1
MN3-1

M3-2
MM3-2
MP3-2

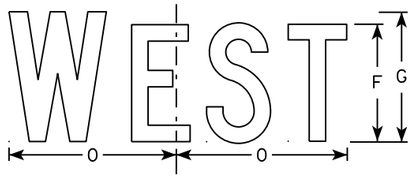
MB3-2
MK3-2
MN3-2



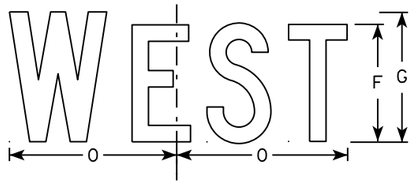
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

NOTES

1. All Signs Type II - Type H
2. Color:
   Background - See note 5
   Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M3-1 thru M3-4 Background - White
   Message - Black
   MB3-1 thru MB3-4 Background - Blue
   Message - White
   MK3-1 thru MK3-4 Background - Green
   Message - White
   MM3-1 thru MM3-4 Background - White
   Message - Green
   MN3-1 thru MN3-4 Background - Brown
   Message - White
   MP3-1 thru MP3-4 Background - White
   Message - Blue
6. Note the first letter of each direction is larger than the remainder of the message.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

STANDARD SIGNS
M3-1 thur M3-4
SERIES

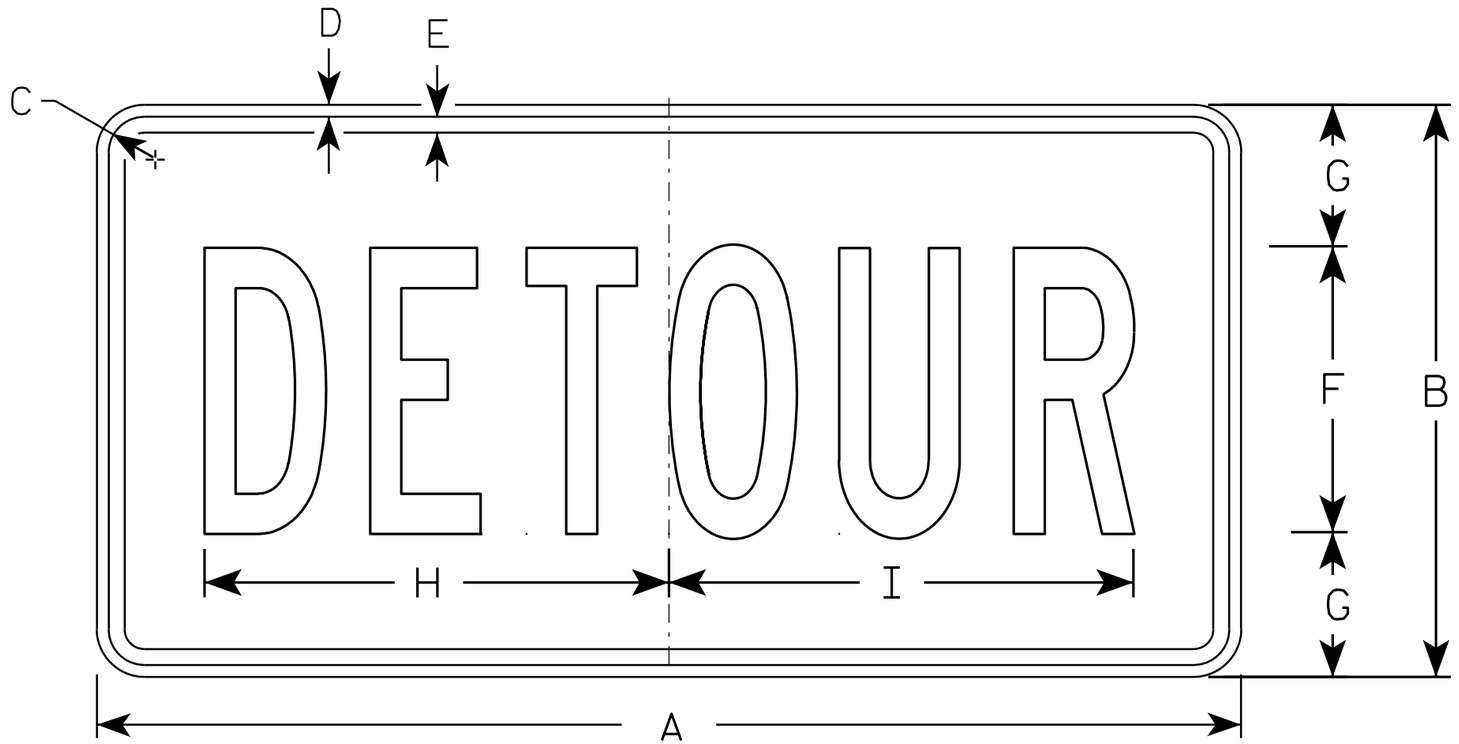
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew P Rauch for State Traffic Engineer

DATE 10/15/15 PLATE NO M3-1.14

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\M31.DGN PLOT DATE : 01-DEC-2015 17:54 PLOT BY : $$ plotuser $$ PLOT NAME : PLOT SCALE : 11.675051:1.000000



M4-8

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	3	10	10 1/4																		2.0
3	36	18	1 1/8	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
4																											
5																											

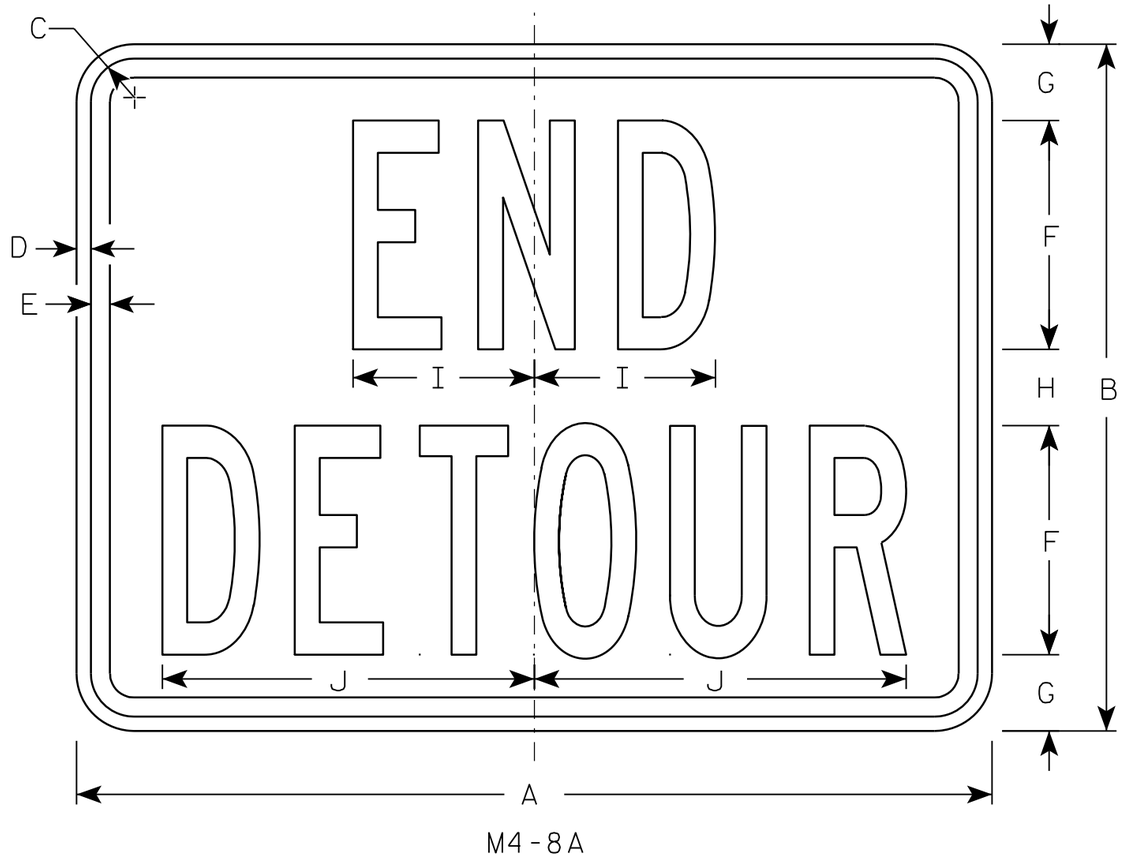
STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/10/10 PLATE NO. M4-8.2

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 108	E



M4-8A

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 1/8	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
3	30	24	1 1/8	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
4																											
5																											

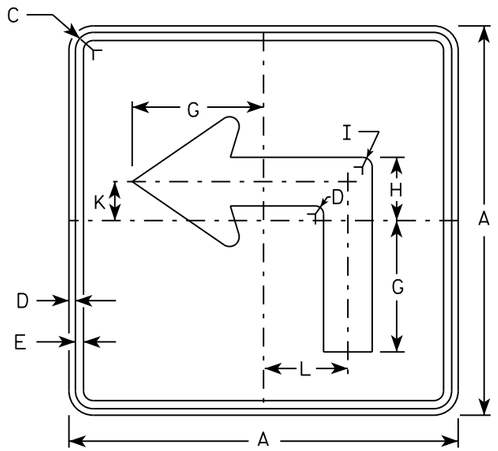
STANDARD SIGN
M4-8A

WISCONSIN DEPT OF TRANSPORTATION

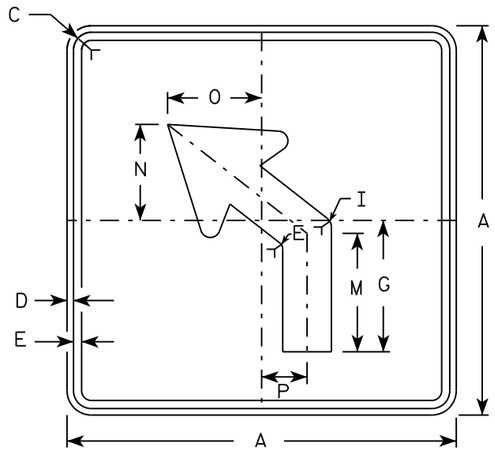
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-8A.2

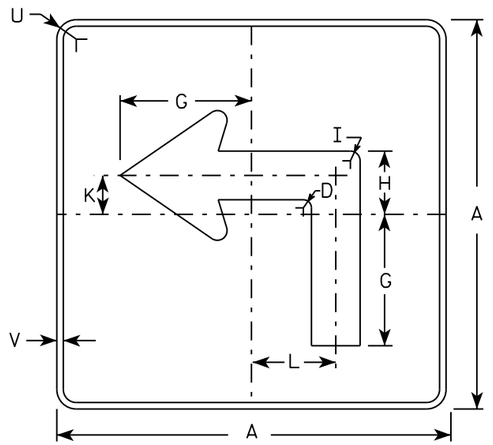
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 109	E



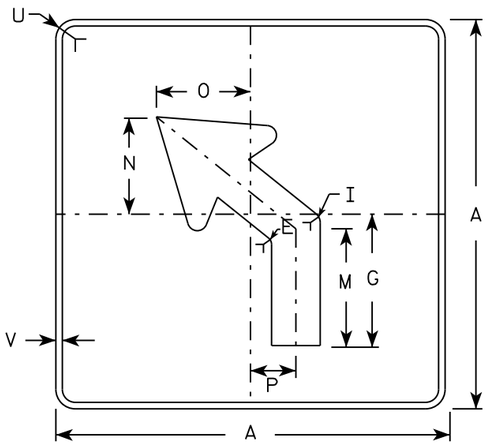
M5-1L
MM5-1L
MO5-1L
MP5-1L



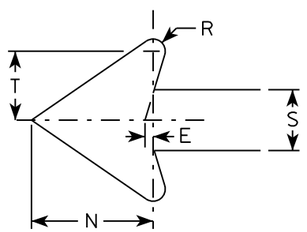
M5-2L
MM5-2L
MO5-2L
MP5-2L



MB5-1L
MK5-1L
MN5-1L
MR5-1L



MB5-2L
MK5-2L
MN5-2L
MR5-2L



NOTES

1. Signs are Type II - Type H reflective except as shown
2. Color:
   Background - See note 4
   Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M5-1 and M5-2 Background - White
   Message - Black
   MB5-1 and MB5-2 Background - Blue
   Message - White
   MK5-1 and MK5-2 Background - Green
   Message - White
   MM5-1 and MM5-2 Background - White
   Message - Green
   MN5-1 and MN5-2 Background - Brown
   Message - White
   MO5-1 and MO5-2 Background - Orange - Type F Reflective
   Message - Black
   MP5-1 and MP5-2 Background - White - Type H Reflective
   Message - Blue
   MR5-1 and MR5-2 Background - Brown
   Message - Yellow
5. M5-1R same as M5-1L except arrow points right.
6. M5-2R same as M5-2L except arrow tilts right.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25

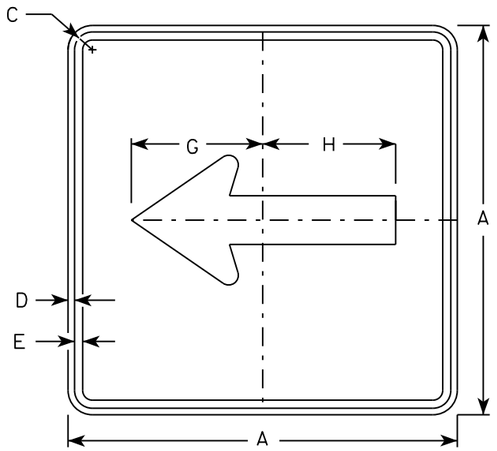
STANDARD SIGN
M5-1 & M5-2

WISCONSIN DEPT OF TRANSPORTATION

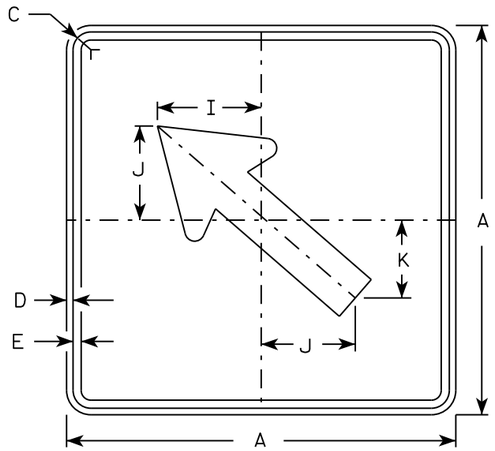
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M5-1.13

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E



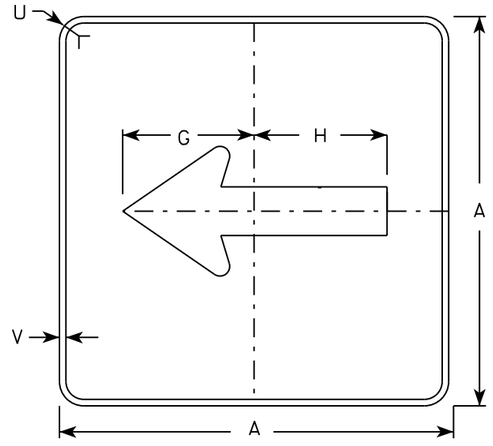
M6-1
MM6-1
MO6-1
MP6-1



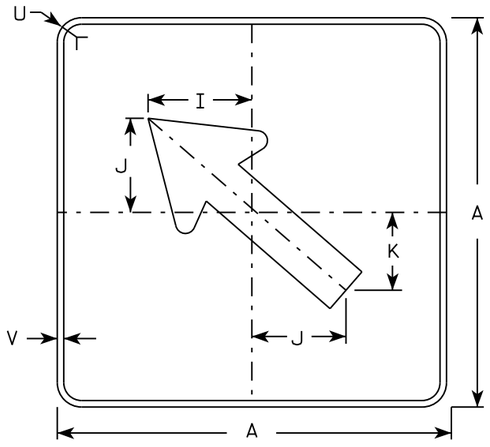
M6-2
MM6-2
MO6-2
MP6-2

NOTES

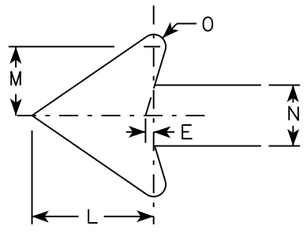
1. Signs are Type II - Type H except as Shown
2. Color:
   Background - See note 4
   Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M6-1 and M6-2 Background - White
   Message - Black
   MB6-1 and MB6-2 Background - Blue
   Message - White
   MK6-1 and MK6-2 Background - Green
   Message - White
   MM6-1 and MM6-2 Background - White
   Message - Green
   MN6-1 and MN6-2 Background - Brown
   Message - White
   MO6-1 and MO6-2 Background - Orange - Type F Reflective
   Message - Black
   MP6-1 and MP6-2 Background - White
   Message - Blue
   MR6-1 and MR6-2 Background - Brown
   Message - Yellow



MB6-1
MK6-1
MN6-1
MR6-1



MB6-2
MK6-2
MN6-2
MR6-2



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2						3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25

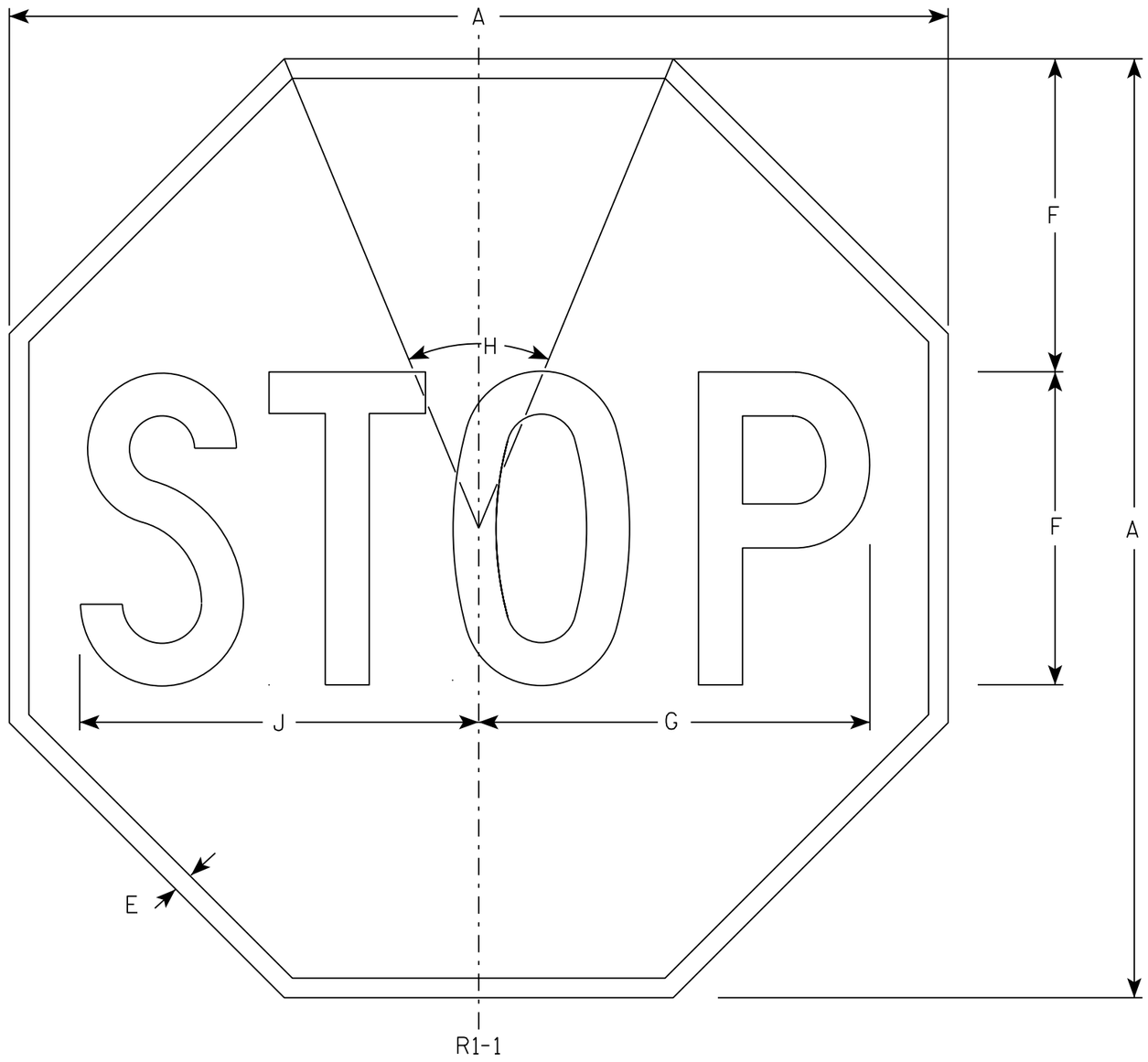
STANDARD SIGN
M6-1 & M6-2
SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/15/15 PLATE 111 M6-1.15

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Red
   Message - White
3. Message Series - C

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

STANDARD SIGN

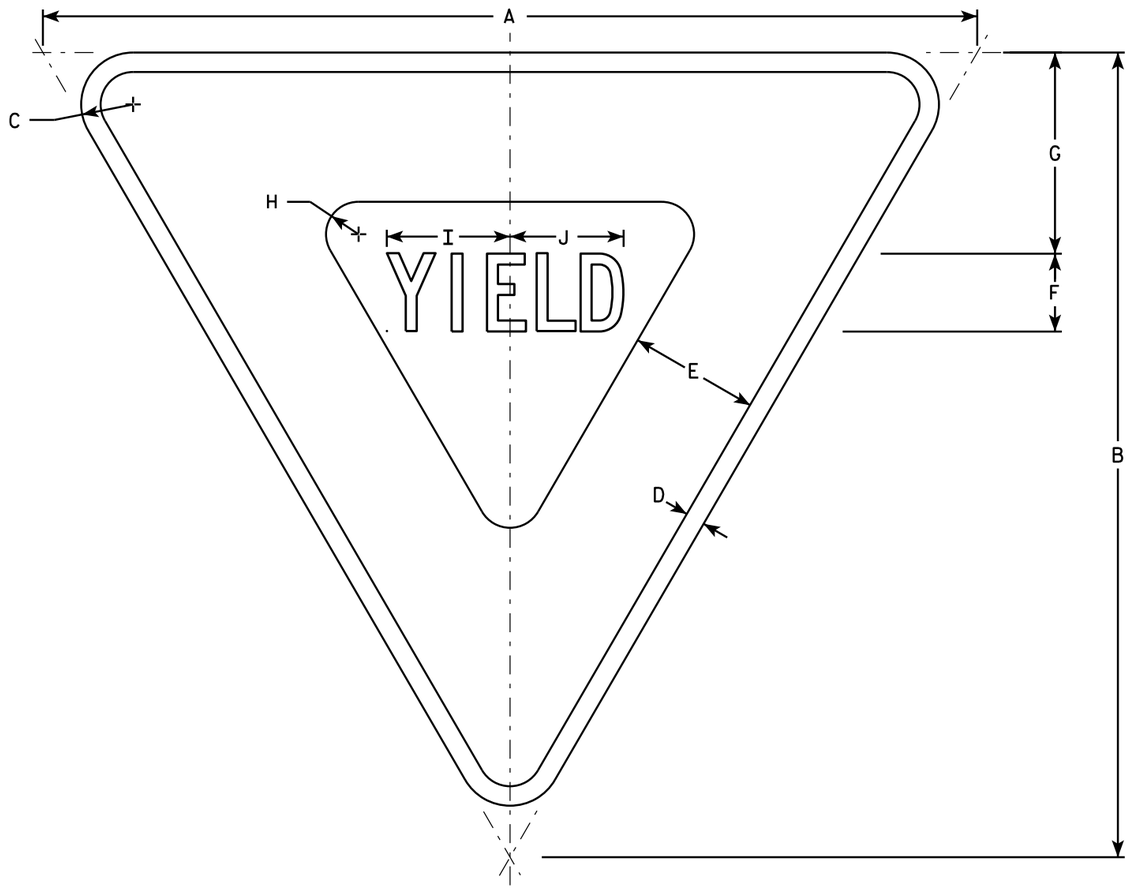
R1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/12/15 PLATE NO. R1-1.13

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 112	E



R1-2

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. The border strip and word message are reflectorized red.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30	26	1 1/2	5/8	4	2 1/2	6 3/8	7/8	4	3 5/8																	2.71
2S	36	31	2	3/4	5	3	7 3/4	1 1/4	4 3/4	4 3/8																	3.88
2M	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
3	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
4	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
5	60	52	3	1 1/2	8	5	13	2 1/2	7 7/8	7 1/4																	10.83
6																											
7	18	15 1/2	1	3/8	2 1/2	1 1/2	3 7/8	5/8	2 3/8	2 1/4																	0.97

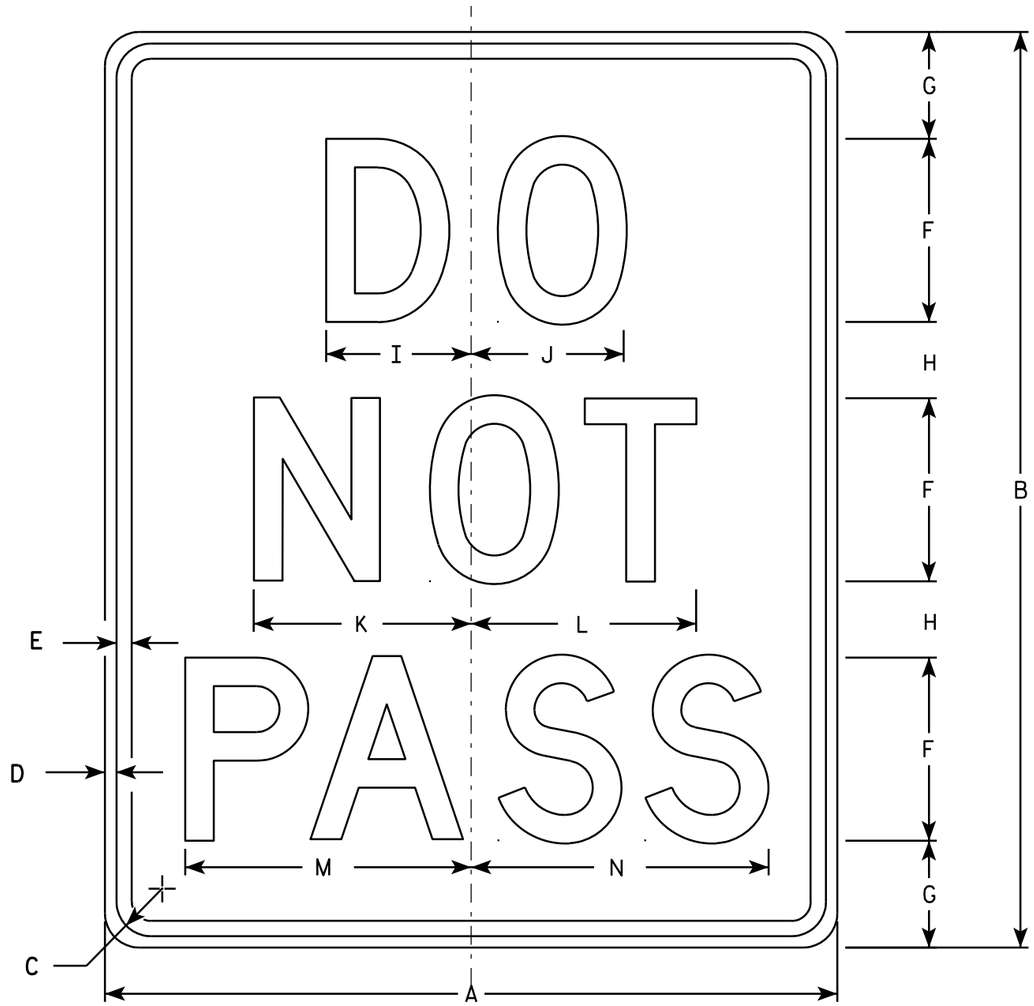
STANDARD SIGN
R1-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/13/14 PLATE P1-2.12

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



R4-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	4	3 1/2	2 1/2	3 1/8	3 1/4	4 3/4	4 7/8	6 1/4	6 1/2													3.0
2S	24	30	1 1/8	3/8	1/2	6	3 1/2	2 1/2	4 3/4	5	7 1/8	7 3/8	9 3/8	9 3/4													5.0
2M	24	30	1 1/8	3/8	1/2	6	3 1/2	2 1/2	4 3/4	5	7 1/8	7 3/8	9 3/8	9 3/4													5.0
3																											
4	36	48	1 5/8	5/8	3/4	8	7	5	6 1/4	6 5/8	9 1/2	9 3/4	12 1/2	13													12.0
5	48	60	2 1/4	3/4	1	10	8	7	7 3/4	8 3/8	11 7/8	12 1/4	15 5/8	16 1/4													20.0

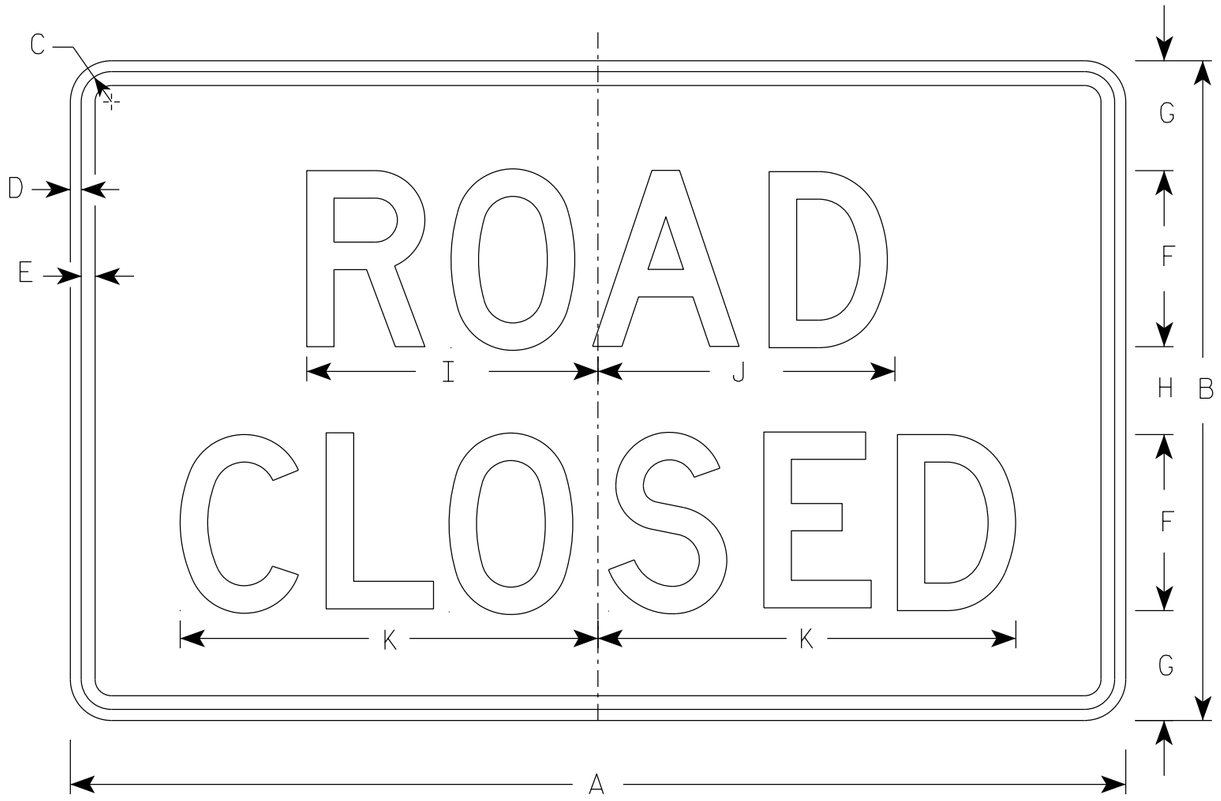
STANDARD SIGN
R4-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/25/2011 PLATE NO. R4-1.7

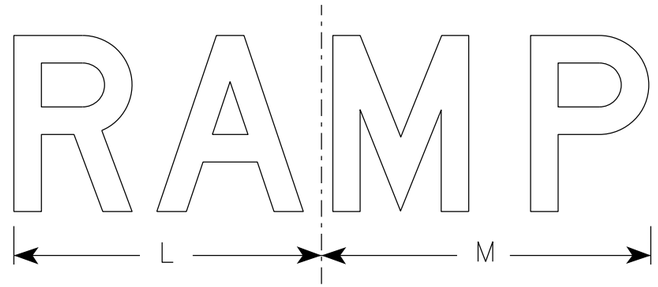
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 114 | E



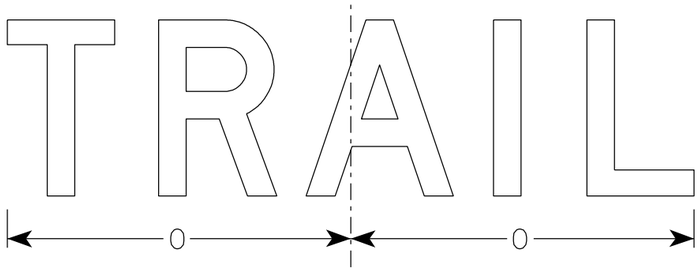
R11-2

NOTES

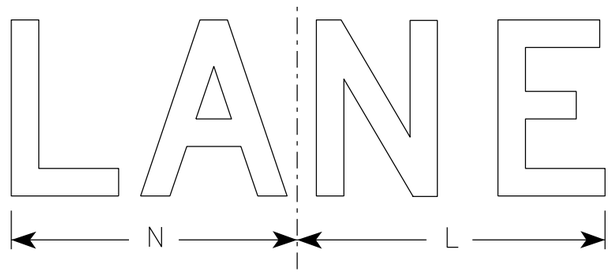
1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Modify the message as required.



R11-2R



R11-2T



R11-2L

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
2M	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
3	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
4	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
5	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0

STANDARD SIGN
R11-2

WISCONSIN DEPT OF TRANSPORTATION

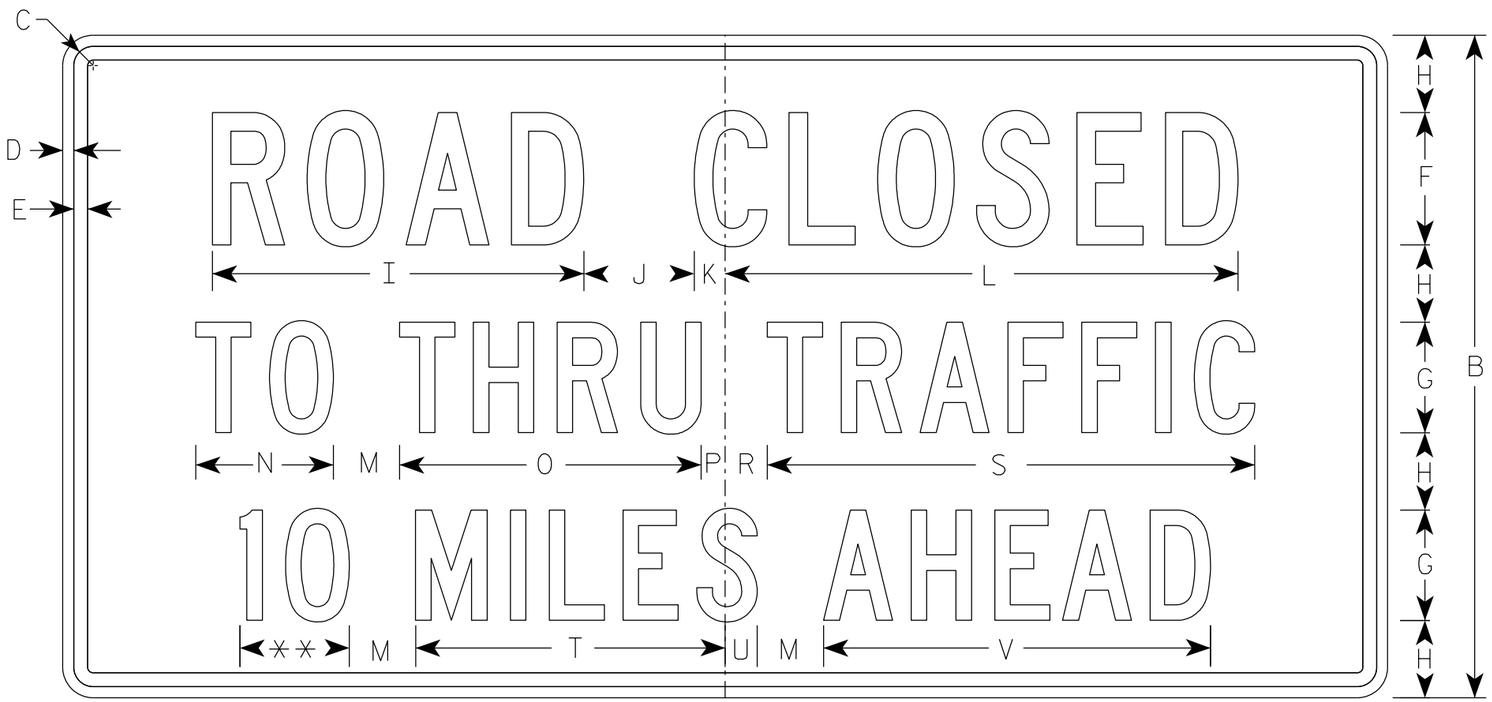
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/29/2021 PLATE NO. R11-2.11

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 115	E

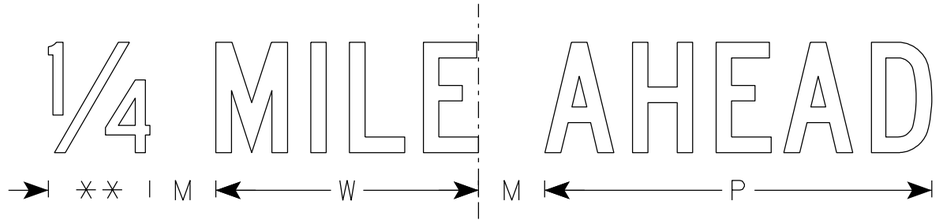
NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals to nearest quarter mile and optically adjust spacing to achieve proper balance.



R11-3

** See Note 5



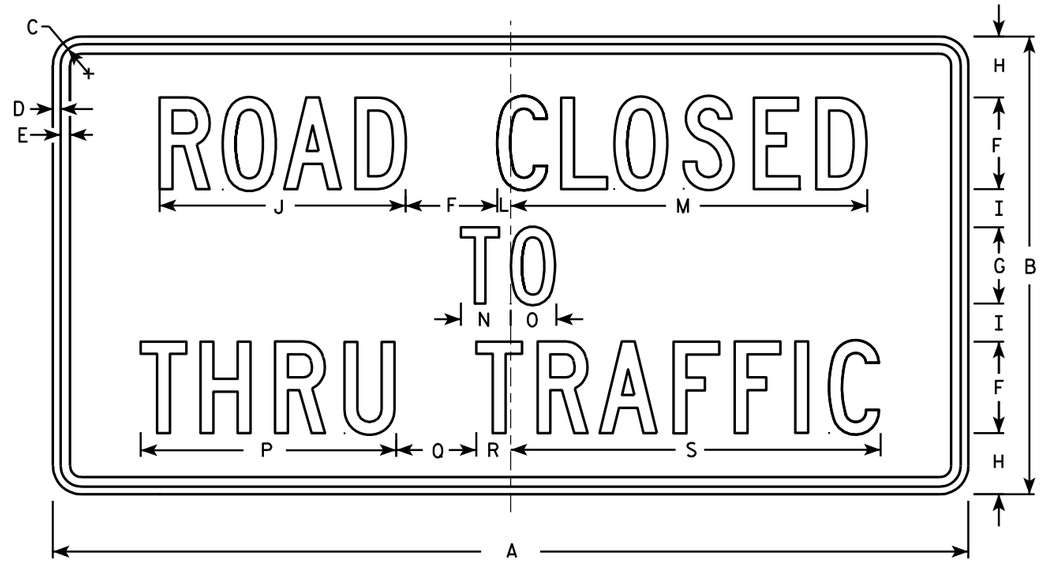
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/4	3/8	3/8	4	3	2	11 1/4	3	1 1/8	15 3/8	2	3 3/4	8 1/4	5/8		1 3/8	13 1/4	8 3/8	7/8	10 1/2	7 1/8				4.5
2S	60	30	1 3/8	1/2	5/8	6	5	3 1/2	16 7/8	5	1 3/8	23 1/4	3	6 1/4	13 5/8	1 1/8		1 7/8	22 1/8	14	1 1/2	17 1/2	11 7/8				12.5
2M	60	30	1 3/8	1/2	5/8	6	5	3 1/2	16 7/8	5	1 3/8	23 1/4	3	6 1/4	13 5/8	1 1/8		1 7/8	22 1/8	14	1 1/2	17 1/2	11 7/8				12.5
3																											
4																											
5																											

STANDARD SIGN
R11-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer
DATE 6/14/2021 PLATE NO. R11-3.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 116	E



R11-4

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
2M	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
3																											
4																											
5																											

STANDARD SIGN
R11 - 4

WISCONSIN DEPT OF TRANSPORTATION

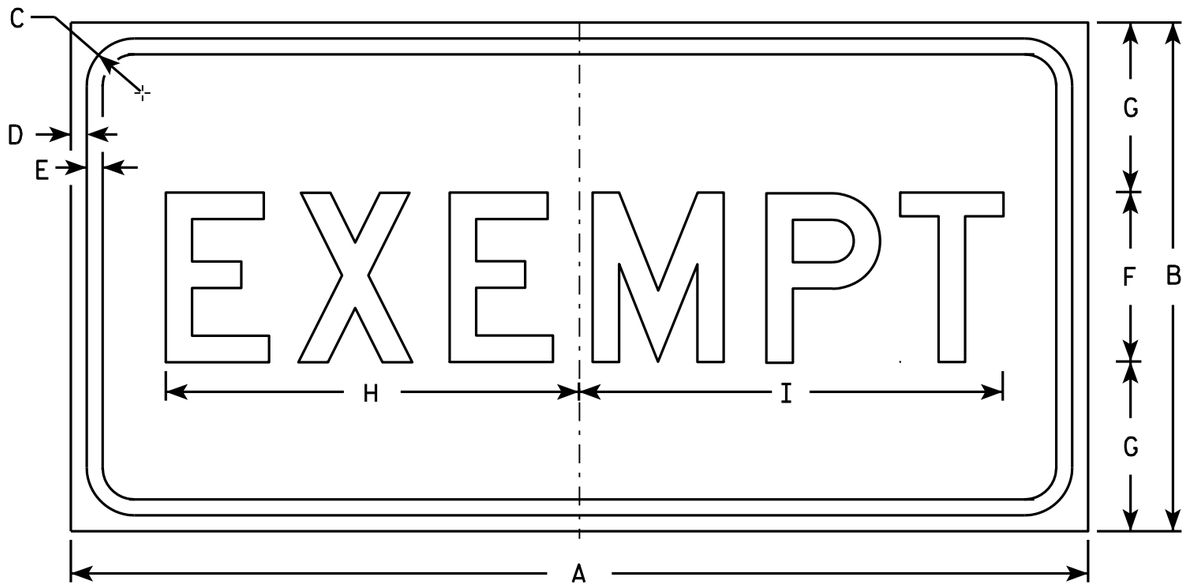
APPROVED Matthew R Rauch for State Traffic Engineer

DATE 4/1/11 PLATE NO. R11-4.3

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 117 | E

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - See Note 5
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Background - R15-3 is White.
   W10-1A is Yellow.



R15-3
and
W10-1A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/8	3/8	3/8	4	4	9 3/4	10																		2
2M	24	12	1 1/8	3/8	3/8	4	4	9 3/4	10																		2
3																											
4																											
5																											

STANDARD SIGN
R15-3 & W10-1A

WISCONSIN DEPT OF TRANSPORTATION

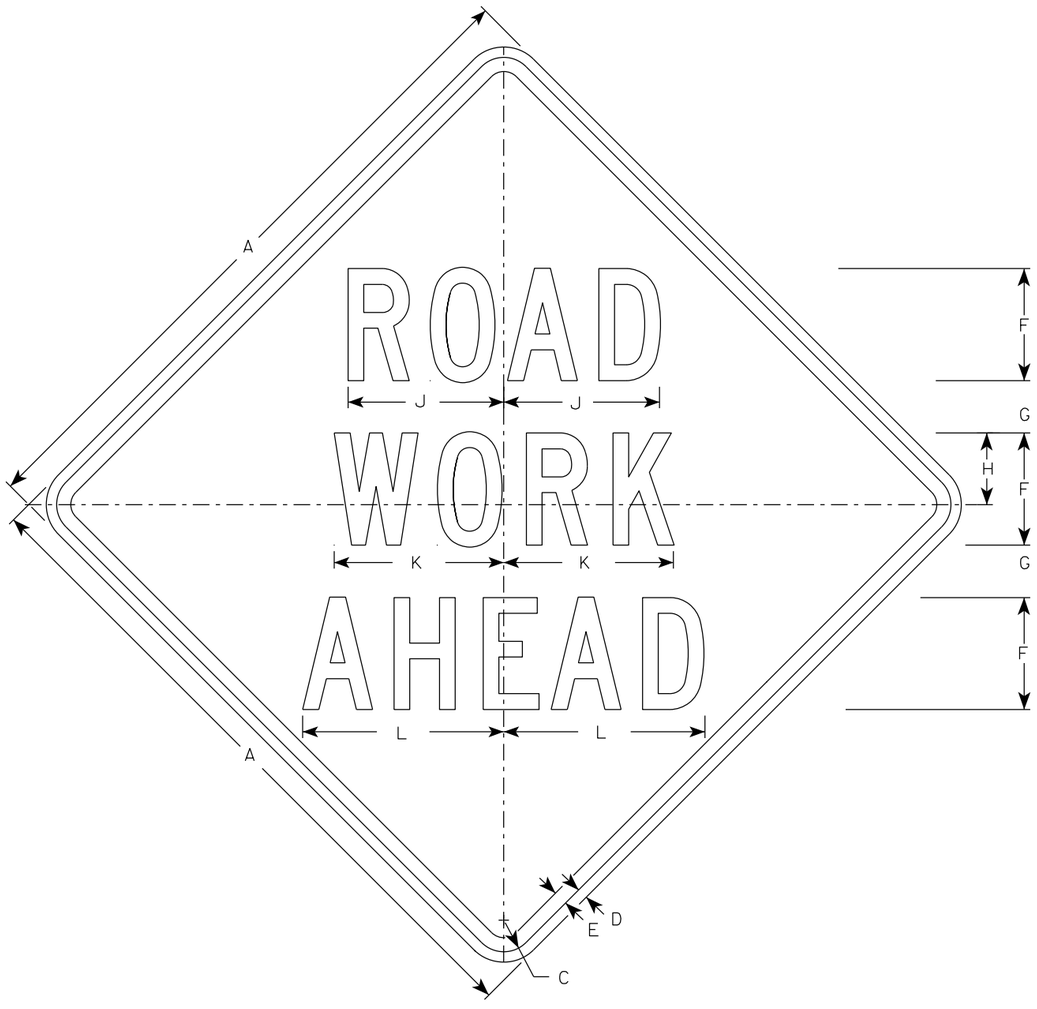
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/15/11 PLATE NO. R15-3.6

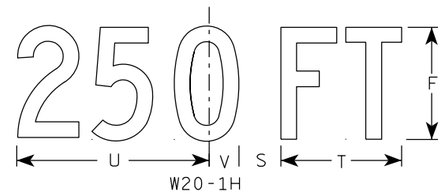
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 118	E

NOTES

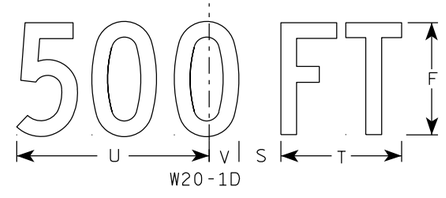
1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W20-1A

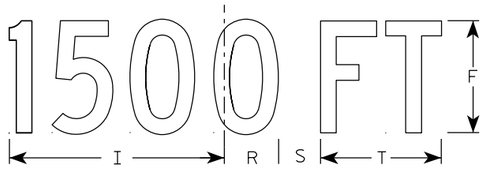


W20-1H

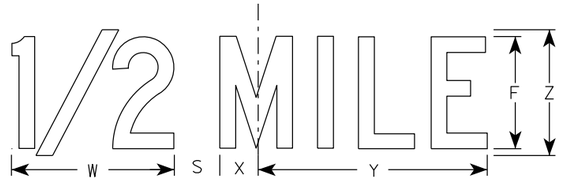


W20-1D

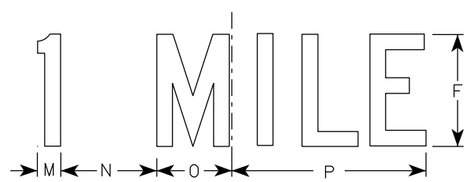
W20-1C



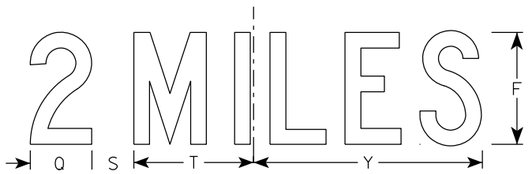
W20-1B



W20-1G



W20-1F



W20-1E

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9	3 1/4	2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN
W20-1A, B, C, D, E, F, G & H

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/25/2020 PLATE NO. W20-1.11

PROJECT NO:

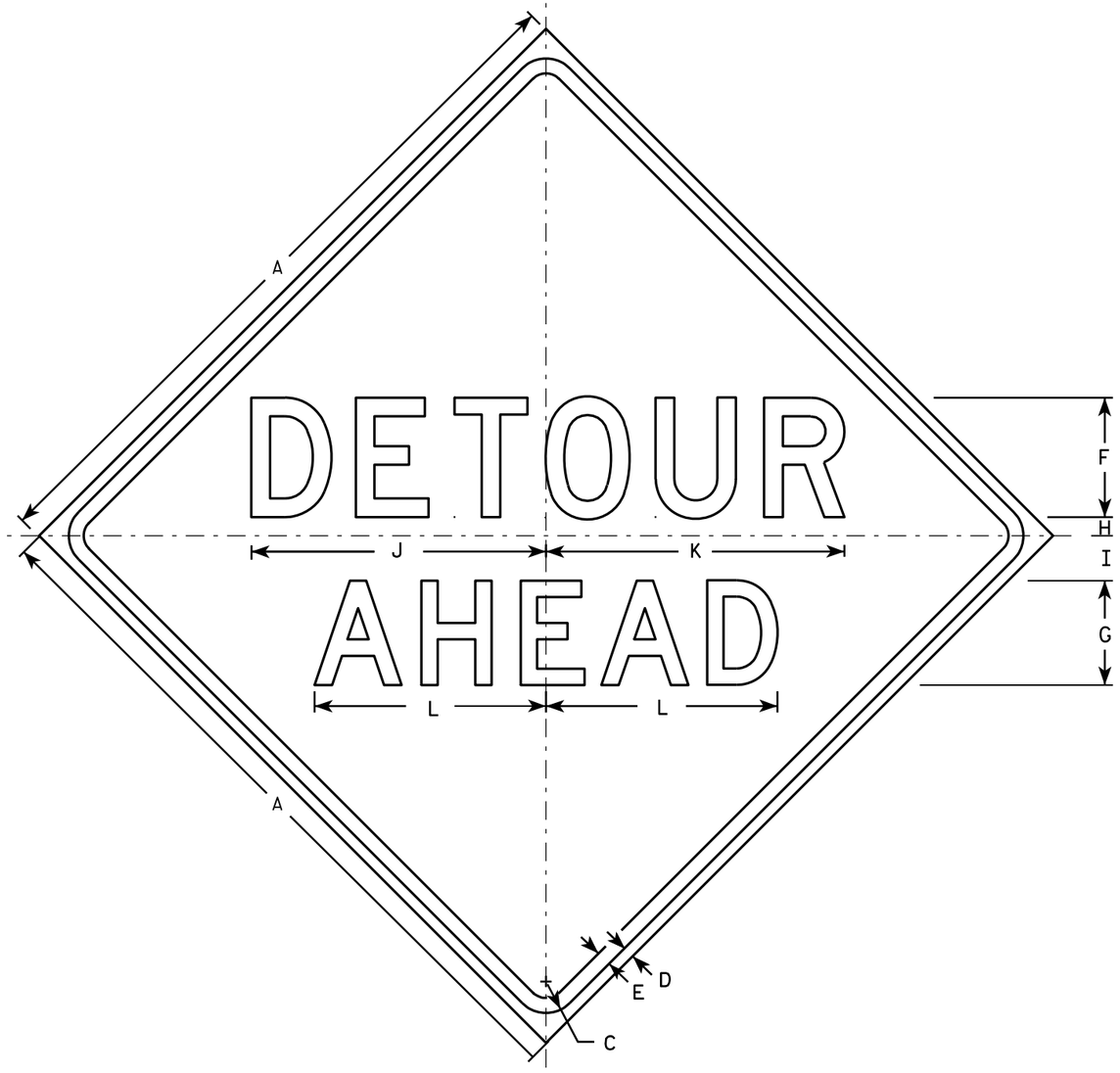
SHEET NO: E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\W201.DGN PLOT DATE : 25-MARCH-2020 PLOT BY : dotc4c

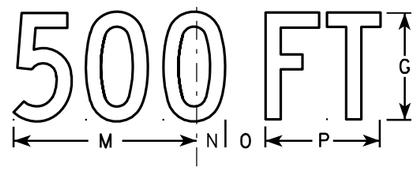
WISDOT/CADDS SHEET 42

7

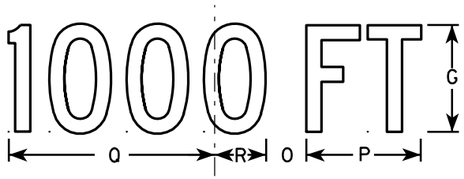
7



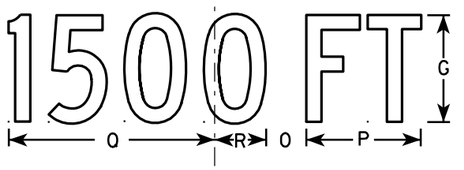
W20-2A



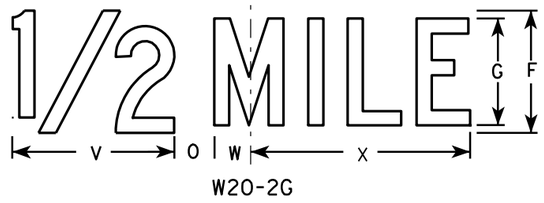
W20-2D



W20-2C



W20-2B



W20-2G

W20-2F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
   Line 2 is Series D for AHEAD and Series C for all other distances.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	O	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
2M	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
3	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
4	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
5	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0

STANDARD SIGN
W20-2A,B,C,D,F & G

WISCONSIN DEPT OF TRANSPORTATION

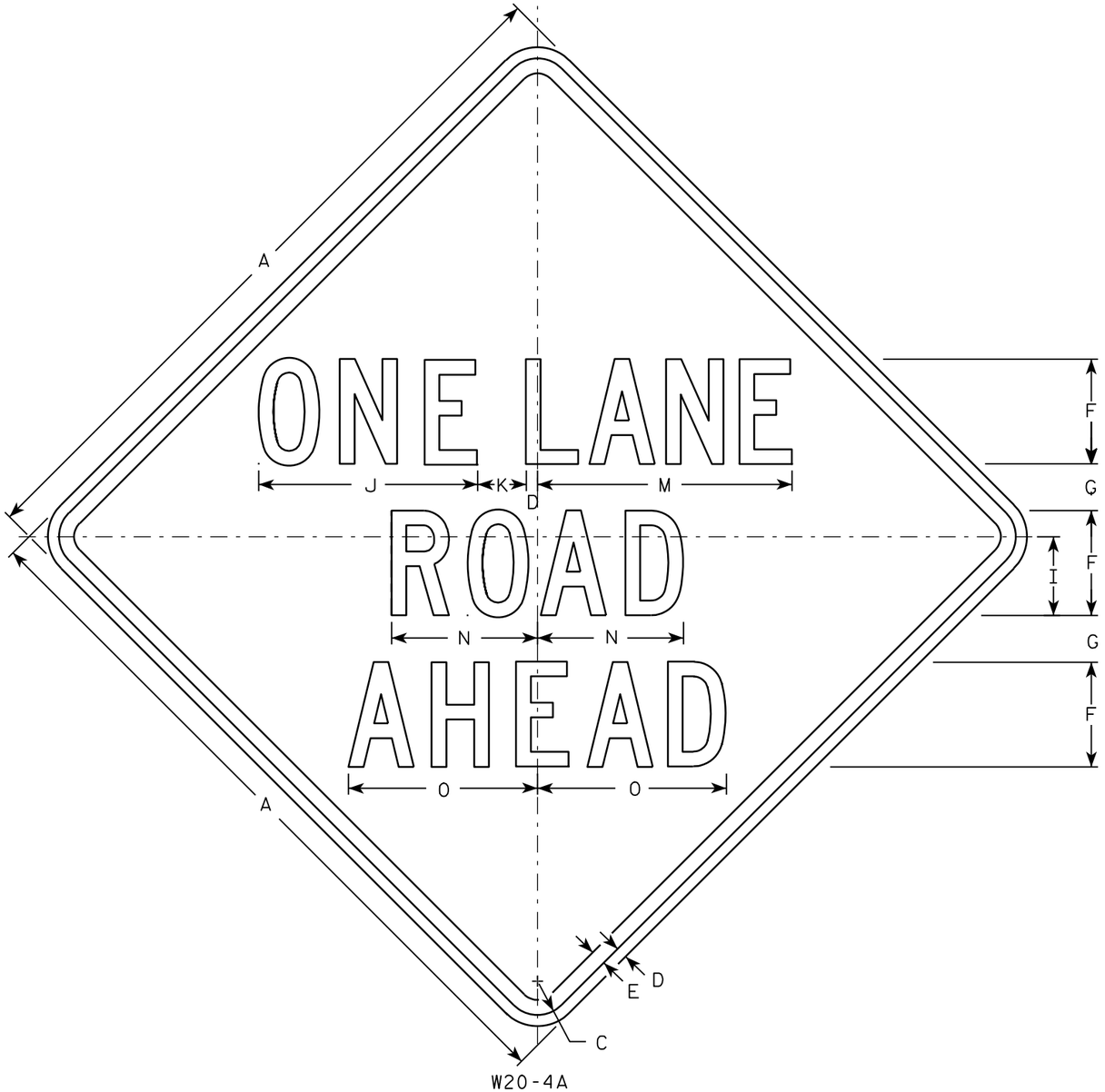
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-2.6

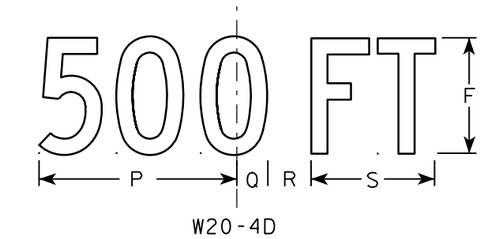
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 120 | E

7

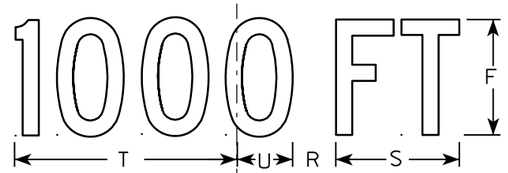
FILE NAME : C:\Users\PROJECTS\tr_stdplate\W202.DGN PLOT DATE : 18-MAR-2011 10:00 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 9.931739:1.000000
WISDOT/CADDS SHEET 42



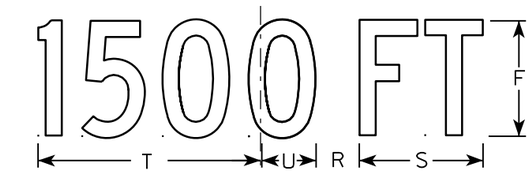
W20-4A



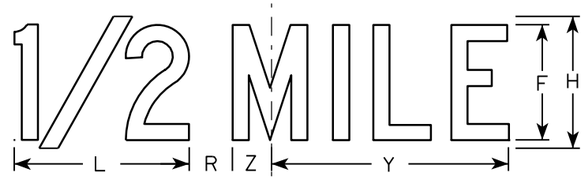
W20-4D



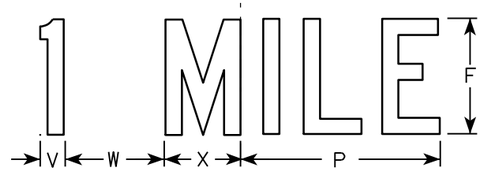
W20-4C



W20-4B



W20-4G



W20-4F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	2 3/8	6	3 3/4	10 3/8	2 3/8	8	13 1/2	7	8 7/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
3	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
4	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
5	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-4A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

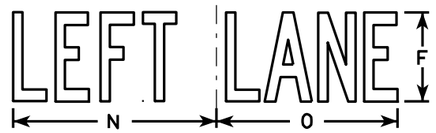
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-4.9

PROJECT NO:

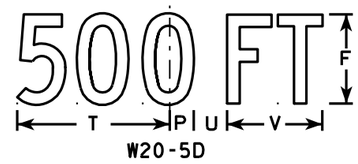
SHEET NO: 121 E

W20-56A

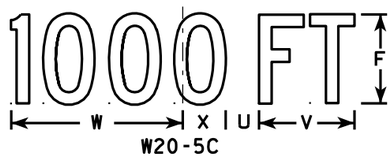


W20-55A

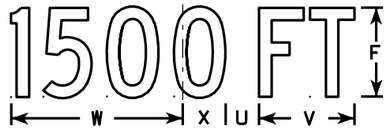
W20-5A



W20-5D



W20-5C



W20-5B

W20-5G

W20-5F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. " _____ LANE" is Series B.
   All other copy is Series C.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	6	1 5/8	5/8	3/4	5	1/8	2 1/2	13 1/8	10 3/4	9 1/2	14 1/4	13 5/8	12	12	1 3/8	1 1/8	4 1/2	3 1/2	9	1 1/8	5 5/8	10 1/8	2 1/2	1 3/4	8	9.0
2S	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
2M	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
3	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
4	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
5	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0

STANDARD SIGN
W20-5A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

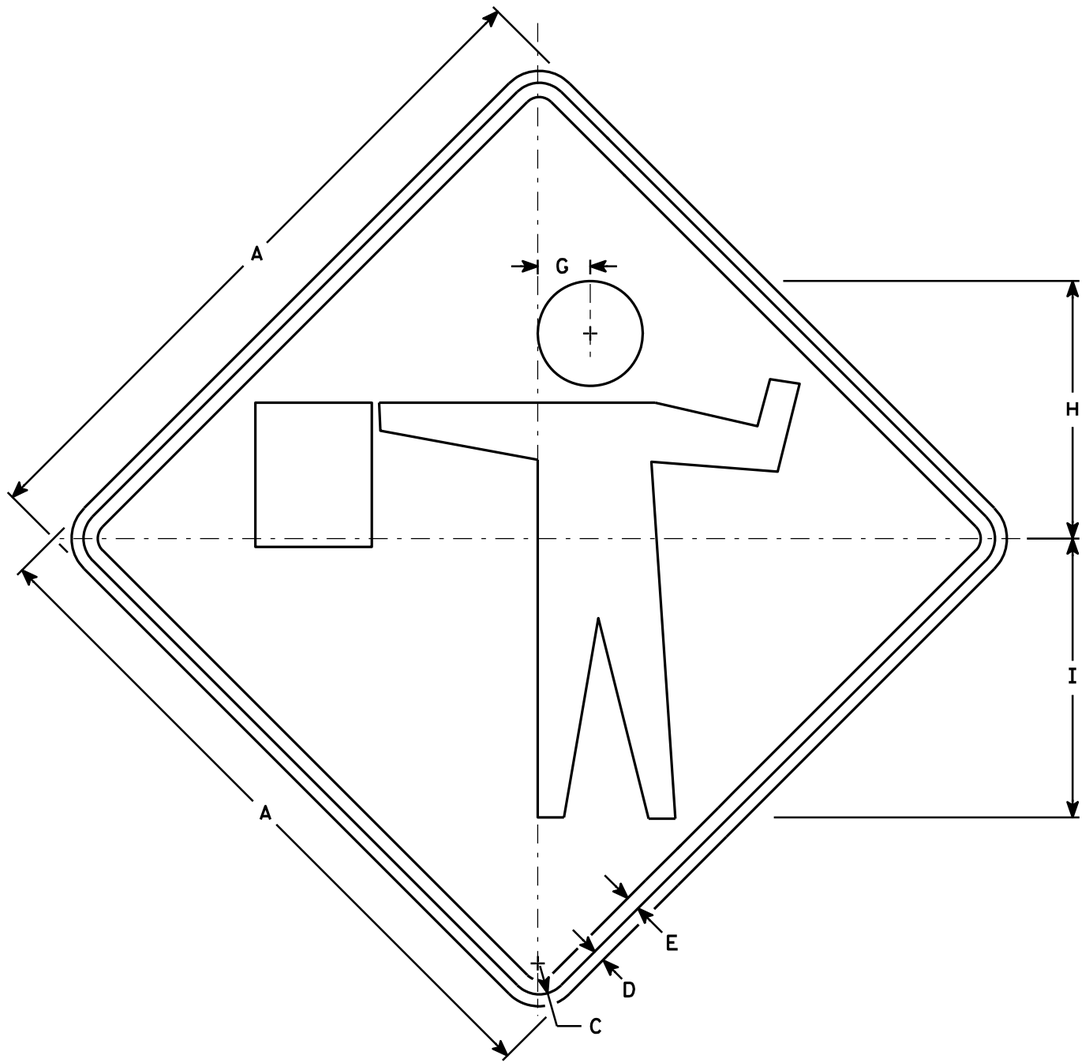
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-5.11

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 122 | E

7

FILE NAME : C:\Users\PROJECTS\tr_stdplate\W205.DGN PLOT DATE : 18-MAR-2011 12:15 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 11.918087:1.000000
WISDOT/CADDS SHEET 42



W20-7A

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4		2 3/4	13 1/2	14 5/8																		9.00
2S	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00
2M	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00
3	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00
4	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00
5	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00

STANDARD SIGN
W20-7A

WISCONSIN DEPT OF TRANSPORTATION

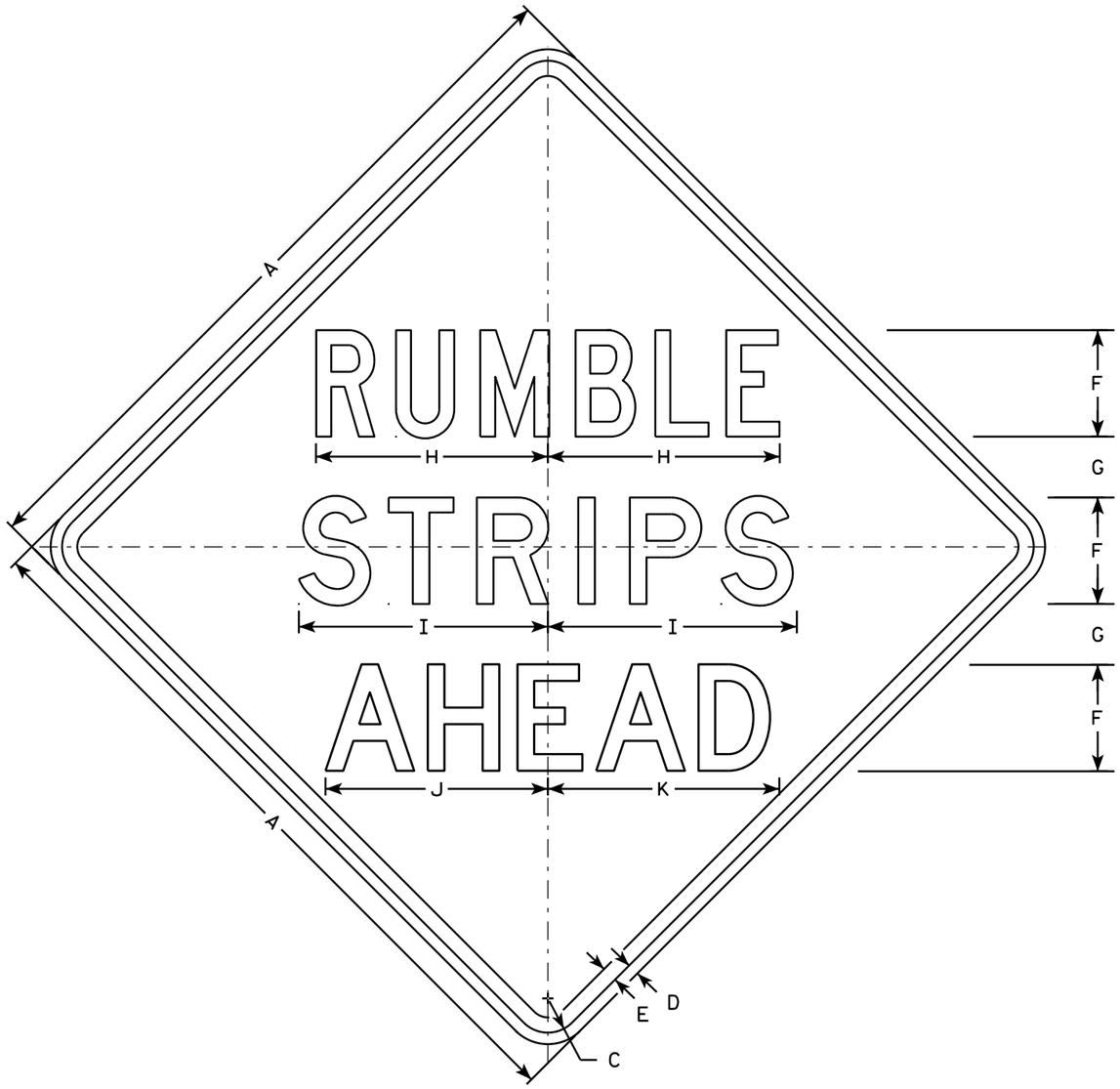
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-7A.5

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 123 | E

7

FILE NAME : C:\Users\PROJECTS\tr_stdplate\W207A.DGN PLOT DATE : 18-MAR-2011 13:14 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 7.945391:1.000000 WISDOT/CADDS SHEET 42



W21-65

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series C
   Lines 2 and 3 are Series D

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	3 1/4	10 7/8	11 5/8	11	11 5/8																9.0
2S	48		2 1/4	3/4	1	7	4	15 1/4	16 3/8	14 5/8	15 1/4																16.0
2M	48		2 1/4	3/4	1	7	4	15 1/4	16 3/8	14 5/8	15 1/4																16.0
3	48		2 1/4	3/4	1	7	4	15 1/4	16 3/8	14 5/8	15 1/4																16.0
4	48		2 1/4	3/4	1	7	4	15 1/4	16 3/8	14 5/8	15 1/4																16.0
5	48		2 1/4	3/4	1	7	4	15 1/4	16 3/8	14 5/8	15 1/4																16.0

STANDARD SIGN

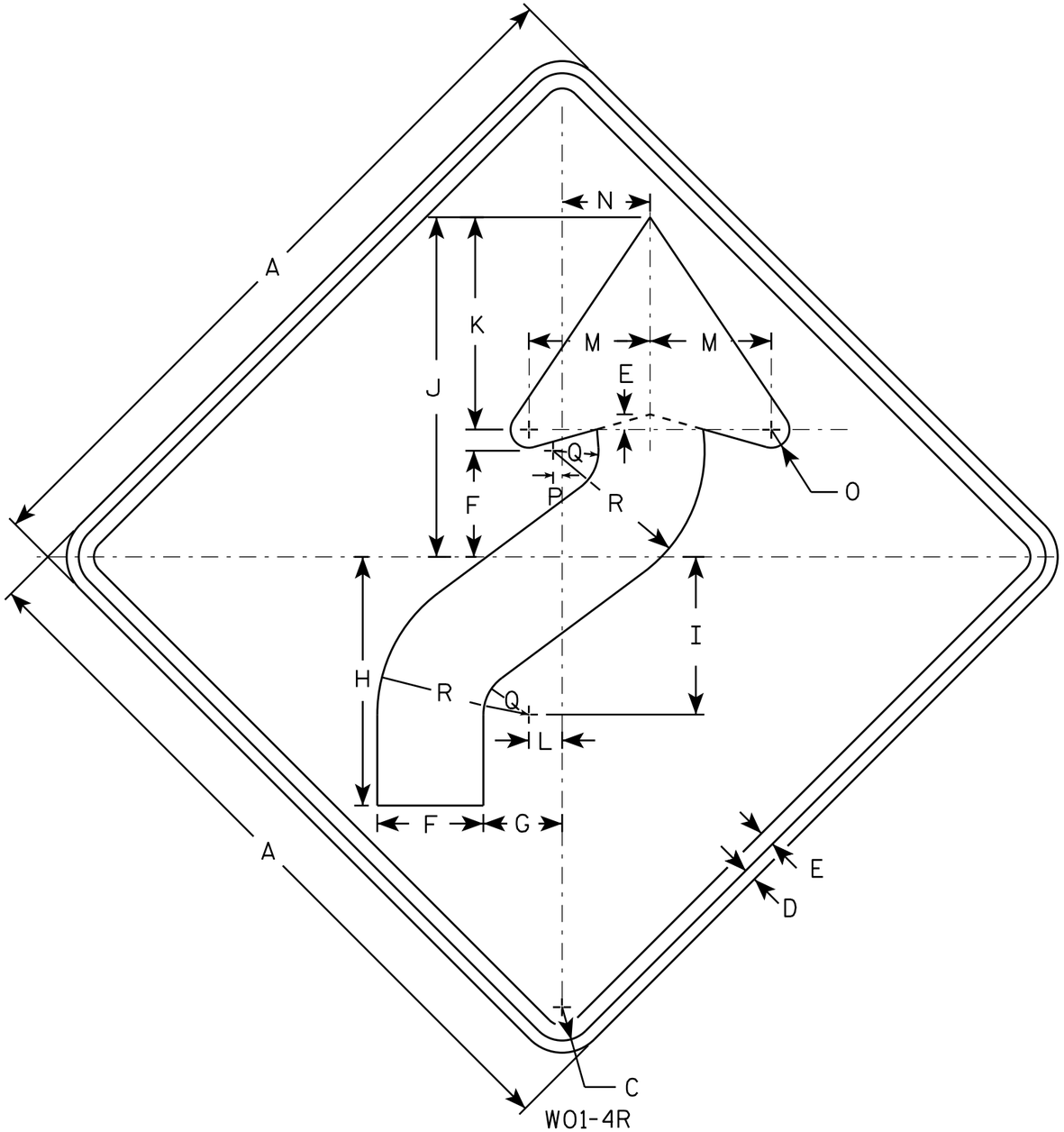
W21-65

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/28/14 PLATE W21-65.1

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E



W01-4R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W01-4L is the same as W01-4R except the arrow is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
2S	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
2M	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
3	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
4	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
5	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0

STANDARD SIGN
W01-4

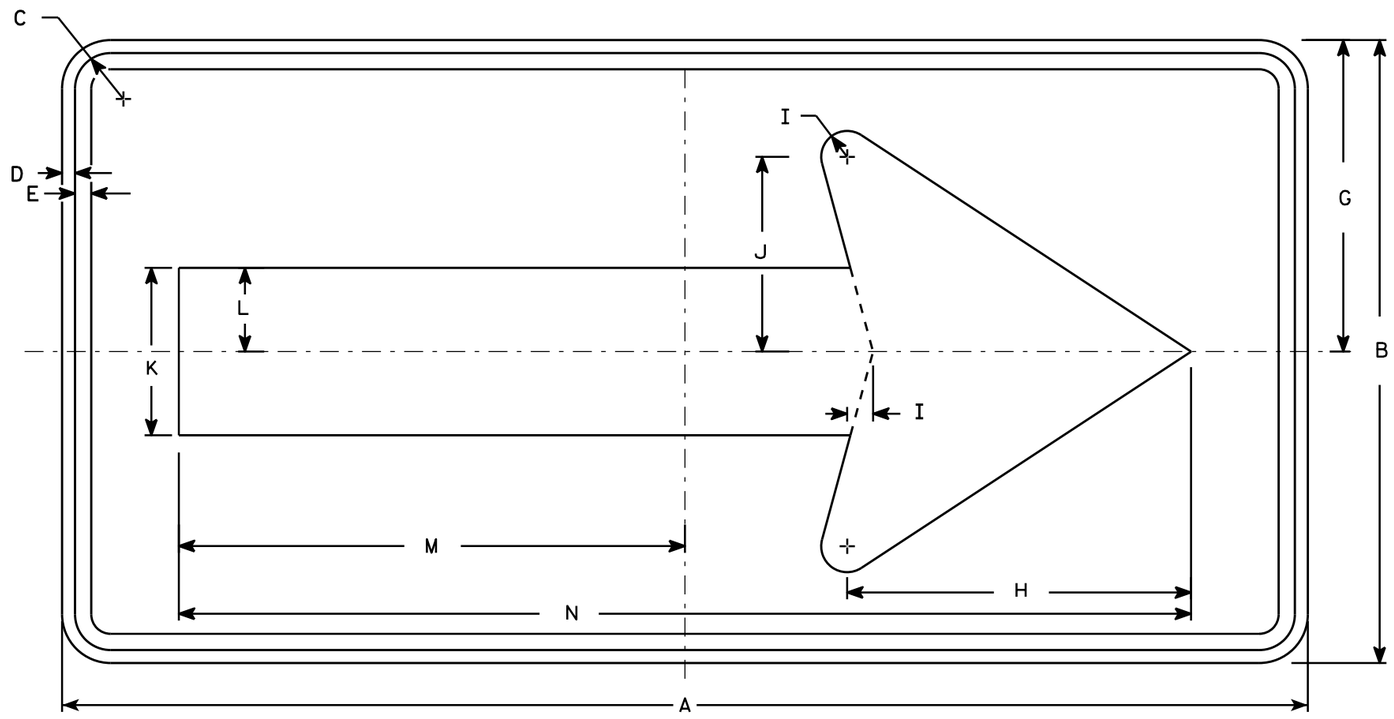
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/18/13 PLATE NO W01-4.1

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\W014.DGN PLOT DATE : 28-FEB-2014 11:35 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 6.755110:1.000000
WISDOT/CADDS SHEET 42



W01-6

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
5	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5

STANDARD SIGN
W01-6

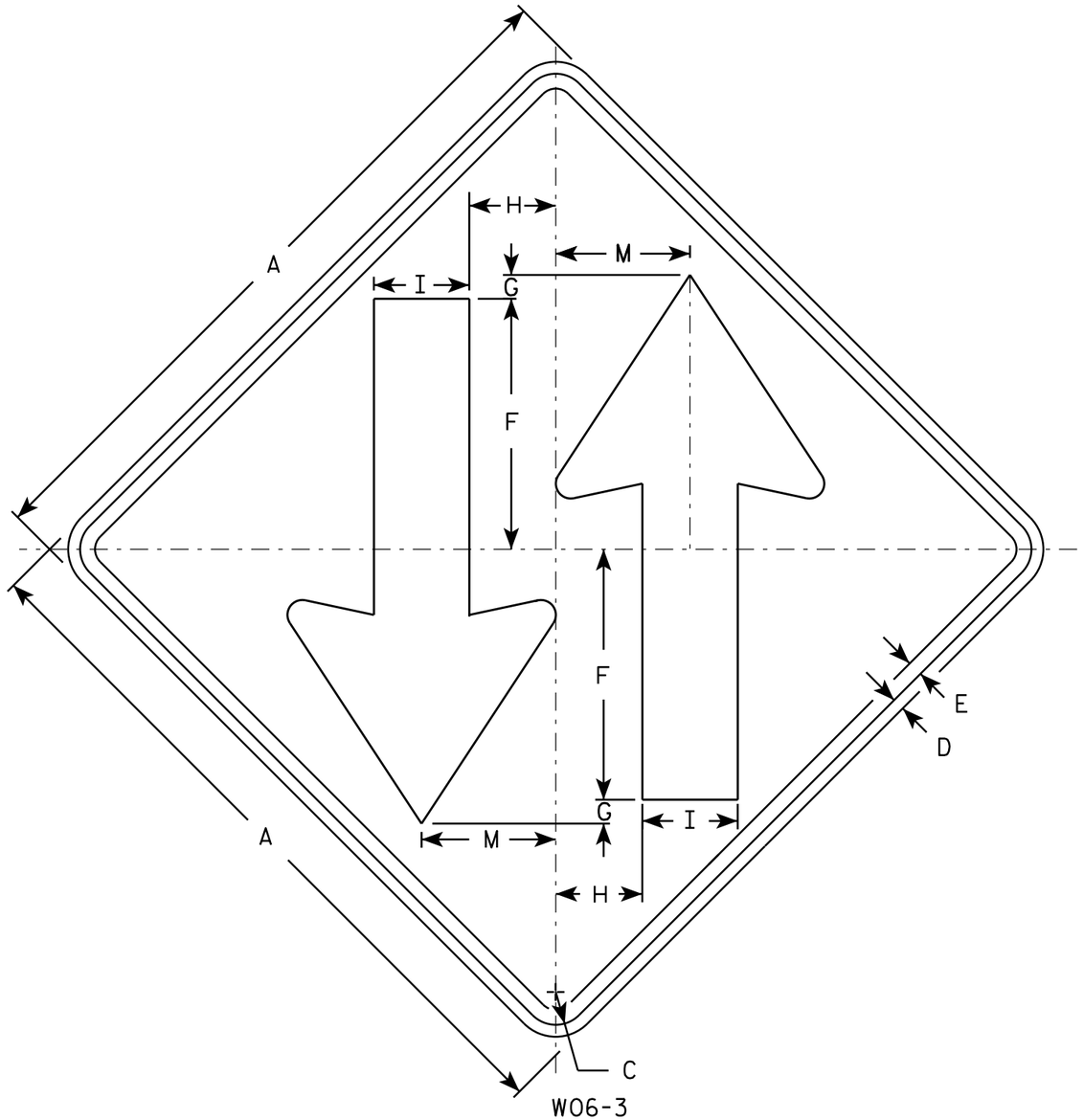
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/18/13 PLATE W01-6.1

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

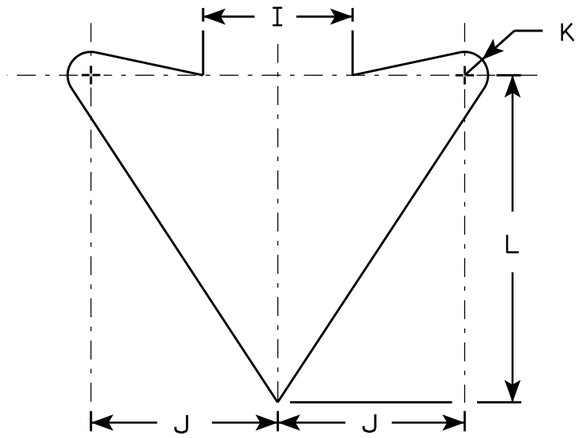
FILE NAME : C:\CAEfiles\Projects\tr_stdplate\W016.DGN PLOT DATE : 28-FEB-2014 11:37 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 5.837526:1.000000 WISDOT/CADDS SHEET 42



W06-3

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	12	1	4 1/4	5	6	3/4	10 1/2	6 3/4														9.0
2S	48		2 1/4	3/4	1	15 1/2	1	6	6	8	1	14	9														16.0
2M	48		2 1/4	3/4	1	15 1/2	1	6	6	8	1	14	9														16.0
3	48		2 1/4	3/4	1	15 1/2	1	6	6	8	1	14	9														16.0
4	48		2 1/4	3/4	1	15 1/2	1	6	6	8	1	14	9														16.0
5	48		2 1/4	3/4	1	15 1/2	1	6	6	8	1	14	9														16.0

STANDARD SIGN
W06-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/20/13 PLATE 127 W06-3.1

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

Wisconsin Department of Transportation

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