

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
PLAN OF PROPOSED IMPROVEMENT

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1009-33-27		
1009-33-34		

ORDER OF SHEETS

Section No.	Title
1	Title
2	Typical Sections and Details
3	Estimate of Quantities
3	Miscellaneous Quantities
4	Right of Way Plat
5	Plan and Profile
6	Standard Detail Drawings
7	Sign Plates
8	Structure Plans
9	Computer Earthwork Data
9	Cross Sections

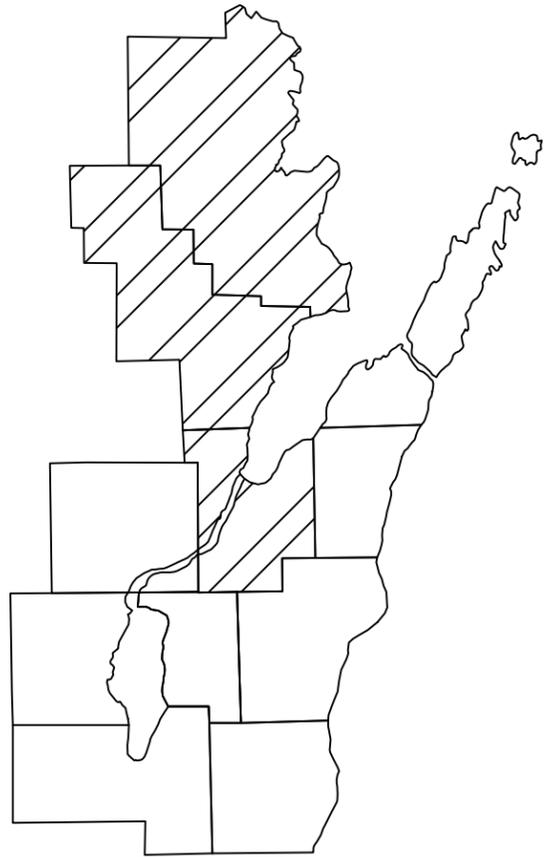
TOTAL SHEETS = 52

REGION WIDE DECK SEALING FY23
VARIOUS BACKBONE ROUTES STH
VAR HWY
NORTHEAST REGION WIDE

REGION WIDE DECK SEALING FY23
VARIOUS 3R/LCB ROUTES STH
VAR HWY
NORTHEAST REGION WIDE

STATE PROJECT NUMBER
1009-33-27

STATE PROJECT NUMBER
1009-33-34



LAYOUT
SCALE 0 30 MI
TOTAL NET LENGTH OF CENTERLINE = N/A

DESIGN DESIGNATION 1009-33-35

- A.A.D.T. =
- A.A.D.T. =
- D.H.V. =
- D.D. =
- T. =
- DESIGN SPEED =
- ESALS =

CONVENTIONAL SYMBOLS

- PLAN**
- CORPORATE LIMITS
 - PROPERTY LINE
 - LOT LINE
 - LIMITED HIGHWAY EASEMENT
 - EXISTING RIGHT OF WAY
 - PROPOSED OR NEW R/W LINE
 - SLOPE INTERCEPT
 - REFERENCE LINE
 - EXISTING CULVERT
 - PROPOSED CULVERT (Box or Pipe)
 - COMBUSTIBLE FLUIDS
 - MARSH AREA
 - WOODED OR SHRUB AREA

- PROFILE**
- GRADE LINE
 - ORIGINAL GROUND
 - MARSH OR ROCK PROFILE (To be noted as such)
 - SPECIAL DITCH
 - GRADE ELEVATION
 - CULVERT (Profile View)
- UTILITIES**
- ELECTRIC
 - FIBER OPTIC
 - GAS
 - SANITARY SEWER
 - STORM SEWER
 - TELEPHONE
 - WATER
 - UTILITY PEDESTAL
 - POWER POLE
 - TELEPHONE POLE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	_____
Designer	B. CIBULKA
Project Manager	P. BRAUER
Regional Examiner	_____
Regional Supervisor	D. Segerstrom

APPROVED FOR THE DEPARTMENT
DATE: 10/18/2022
(Signature)

E

GENERAL NOTES

THERE ARE NO KNOWN UTILITY FACILITIES WITHIN THE PROJECT AREA. HOWEVER, IT IS THE CONTRACTORS RESPONSIBILITY TO CONFIRM THIS.

THE CONTRACTOR WILL ENSURE THE DECK SEALING MATERIAL WILL NOT REACH SURFACE WATER. REPORT ANY INCIDENTAL SPILLS TO THE WDNR OFFICE WITHIN 24 HOURS OR THE TOLL FREE HOTLINE, 1-800-943-0003, IF WDNR CONTACT IS NOT AVAILABLE.

ANY INCIDENTAL GROUND DISTURBANCES WILL BE RESTORED AS SOON AS CONDITIONS PERMIT. USE APPROPRIATE EROSION CONTROL DEVICES UNTIL RESTORATION OF DISTURBED SOILS ARE COMPLETE.

ANY MATERIAL INADVERTENTLY FALLING ONTO THE STREAMBED AND BANKS AND/OR WETLANDS WILL BE REMOVED IMMEDIATELY

WASTE OR EXCESS MATERIALS WILL NOT BE DISPOSED OF IN FLOODPLAINS, WETLANDS OR WATERWAYS.

DNR AREA LIAISON

BROWN, MARINETTE & OCONTO COUNTIES
JIM DOPERALSKI
DEPARTMENT OF NATURAL RECOURCES
2984 SHAWANO AVE
GREEN BAY, WI 54313
PHONE: (920) 412-0165
EMAIL: JAMES.DOPERALSKI@WISCONSIN.GOV

Northeast Region Project Manager

PAUL BRAUER
944 VANDERPERREN WAY
GREEN BAY, WI 54304
PHONE: (920) 412-6381
EMAIL: PAUL.BRAUER@DOT.WI.GOV



COUNTY	BRIDGE NO.	ON	OVER	TRAFFIC CONTROL	WORK RESTRICTIONS
IH 41					
BROWN	B-05-0119	ASHLAND AVE TO IH 41 SB RAMP	IH 41	LANE CLOSURE	
BROWN	B-05-0120	IH 41 NB RAMP TO ASHLAND AVE	IH 41 RAMP FROM CTH G	LANE SHIFT	
BROWN	B-05-0601	IH41 NB RAMP TO CTH F	ASHWAUBENON CREEK	LANE SHIFT	
BROWN	B-05-0611	HANSEN ROAD	IH 41	FLAGGING	
BROWN	B-05-0615	GLORY ROAD	IH 41	FLAGGING	

USH 41					
BROWN	B-05-0312	BROWN ROAD	USH 141/USH 41	FLAGGING	
BROWN	B-05-0313	NORFIELD ROAD	USH 141/USH 41	FLAGGING	
BROWN	B-05-0684	CTH EB (LAKEVIEW DR)	USH 141/USH 41	FLAGGING	
BROWN	B-05-0685	CTH M (LINEVILLE RD)	USH 141/USH 41	LANE CLOSURE	Mon-Fri: 3pm-6pm
MARINETTE	B-38-0090	USH 41 (BRIDGE ST)	MENOMINEE RIVER	LANE CLOSURE	
MARINETTE	B-38-0101	CTH Y	USH 41	LANE CLOSURE	
MARINETTE	B-38-0104	HALE ROAD	USH 41	FLAGGING	
OCONTO	B-42-0014	USH 41 NB	PENSAUKEE RIVER	LANE CLOSURE	
OCONTO	B-42-0048	USH 41 SB	PENSAUKEE RIVER	LANE CLOSURE	
OCONTO	B-42-0067	CTH S	USH 141/USH 41	FLAGGING	
OCONTO	B-42-0068	ALLEN ROAD	USH 141/USH 41	FLAGGING	
OCONTO	B-42-0070	GEANO ROAD	USH 41	FLAGGING	
OCONTO	B-42-0071	STH D (SAMPSON ROAD)	USH 141/USH 41	FLAGGING	
OCONTO	B-42-0072	OAK ORCHARD ROAD	USH 141/USH 41	FLAGGING	
OCONTO	B-42-0079	USH141/USH 41 NB	LITTLE SUAMICO RIVER	LANE CLOSURE	Sun 11am-4pm, Mon-Tues 1pm-7pm, Wed 12pm-7pm, Thurs 11am-7pm, Fri 9am-9pm, Sat 9am-5pm
OCONTO	B-42-0110	BUS 41 RAMP TO USH 41 SB	USH 41	FULL CLOSURE	
OCONTO	B-42-0111	USH 41 SB	OCONTO RIVER	LANE CLOSURE	
OCONTO	B-42-0112	USH 41 NB	OCONTO RIVER	LANE CLOSURE	
OCONTO	B-42-0118	USH 41 SB RAMP TO BUS 41	USH 41	FULL CLOSURE	

COUNTY	BRIDGE NO.	ON	OVER	TRAFFIC CONTROL	WORK RESTRICTIONS
USH 8					
MARINETTE	B-38-0070	USH 8	K C CREEK	FLAGGING	
STH 29					
BROWN	B-05-0269-0001	STH 29 (WALNUT STREET)	FOX RIVER	LANE CLOSURE	M-F: 6am-9am, 3pm-6pm
BROWN	B-05-0269-0002	STH 29 (WALNUT STREET)	FOX RIVER	LANE CLOSURE	M-F: 6am-9am, 3pm-6pm
BROWN	B-05-0269-0003	STH 29 (WALNUT STREET)	FOX RIVER	LANE CLOSURE	M-F: 6am-9am, 3pm-6pm
BROWN	B-05-0339	STH 32	FOX RIVER	LANE CLOSURE	M-F: 6am-9am, 3pm-6pm
STH 32					
OCONTO	B-42-0025	STH 32-STH 64	WAUPEE CREEK	FLAGGING	Friday: Noon-6pm
OCONTO	B-42-0028	STH 32-STH 64	N BRANCH OCONTO RIVER	FLAGGING	Friday: Noon-6pm
STH 64					
MARINETTE	B-38-0045	STH 64	S BRANCH BEAVER CREEK	FLAGGING	
MARINETTE	B-38-0131	STH 64	PESHTIGO RIVER	FLAGGING	
OCONTO	B-42-0056	STH 64	PESHTIGO BROOK	FLAGGING	
OCONTO	B-42-0080	STH 64	HILLS POND CREEK	FLAGGING	
OCONTO	B-42-0085	STH 64	SECOND S BRANCH OCONTO RIVER	FLAGGING	
OCONTO	B-42-0086	STH 64	FIRST S BRANCH OCONTO RIVER	FLAGGING	

COUNTY	BRIDGE NO.	ON	OVER	TRAFFIC CONTROL	WORK RESTRICTIONS
USH 141					
MARINETTE	B-38-0035	USH 141	MIDDLE INLET CREEK	FLAGGING	
MARINETTE	B-38-0036	USH 141	UPPER MIDDLE INLET	FLAGGING	
MARINETTE	B-38-0037	USH 141	LOWER MIDDLE INLET	FLAGGING	
MARINETTE	B-38-0048	USH 141	PESHTIGO RIVER	FLAGGING	
MARINETTE	B-38-0082	USH 141	S BRANCH BEAVER CREEK	FLAGGING	
MARINETTE	B-38-0083	USH 141	PIKE RIVER	FLAGGING	
MARINETTE	B-38-0091	CTH B - BUS 141	USH 141	LANE SHIFT	
MARINETTE	B-38-0094	CTH Q	USH 141	FLAGGING	
MARINETTE	B-38-0095	CTH CP	USH 141	FLAGGING	
OCONTO	B-42-0042	USH 41 SB	USH 141 NB	LANE CLOSURE	
OCONTO	B-42-0045	USH 141 SB	PENSAUKEE RIVER	LANE CLOSURE	Sunday 10am-4pm
OCONTO	B-42-0046	USH 141 NB	PENSAUKEE RIVER	LANE CLOSURE	Friday: 2pm-7pm
OCONTO	B-42-0074	USH 141 NB	OCONTO RIVER	LANE CLOSURE	Friday: 2pm-7pm
OCONTO	B-42-0075	USH 141 SB	OCONTO RIVER	LANE CLOSURE	Sunday 10am-4pm
OCONTO	B-42-0076	USH 141 NB	STH 22	LANE CLOSURE	Friday: 2pm-7pm
OCONTO	B-42-0077	USH 141 SB	STH 22	LANE CLOSURE	Sunday 10am-4pm
OCONTO	B-42-0088	CTH A	USH 141	FLAGGING	
OCONTO	B-42-0089	USH 141 NB	KELLY BROOK	LANE CLOSURE	Friday: 2pm-7pm
OCONTO	B-42-0090	USH 141 SB	KELLY BROOK	LANE CLOSURE	Sunday 10am-4pm
OCONTO	B-42-0091	USH 141 NB	LITTLE RIVER	LANE CLOSURE	Friday: 2pm-7pm
OCONTO	B-42-0092	USH 141 SB	LITTLER RIVER	LANE CLOSURE	Sunday 10am-4pm

Estimate Of Quantities

Line	Item	Item Description	Unit	Total	1009-33-27	1009-33-34
					Qty	Qty
0002	502.3215	Protective Surface Treatment Reseal	SY	88,038.000	45,122.000	42,916.000
0004	619.1000	Mobilization	EACH	1.000	0.430	0.570
0006	643.1050	Traffic Control Signs PCMS	DAY	133.000	84.000	49.000
0008	SPV.0060	Special 01. Traffic Control 1009-33-27	EACH	1.000	1.000	
0010	SPV.0060	Special 02. Traffic Control 1009-33-34	EACH	1.000		1.000

Project ID	COUNTY	ROUTE	CORRIDOR	OVER	BRIDGE NO.	LENGTH [FT]	* PARAPETS [SY]	CONCRETE MEDIAN & SIDEWALK [SY]	DECK [SY]	502.3215 PROTECTIVE SURFACE TREATMENT RESEAL [SY]
1009-33-27	BROWN	ASHLAND AVE TO IH 41 SB RAMP	IH 41	IH 41	B-05-0119	441.2	358	0	1,961	2319
1009-33-27	BROWN	IH 41 NB RAMP TO ASHLAND AVE	IH 41	IH 41 RAMP FROM CTH G	B-05-0120	183.5	157	0	816	973
1009-33-27	BROWN	BROWN ROAD	USH 41	USH 141/USH 41	B-05-0312	220.5	179	321	1,254	1754
1009-33-27	BROWN	NORFIELD ROAD	USH 41	USH 141/USH 41	B-05-0313	214.6	173	134	658	965
1009-33-27	BROWN	IH41 NB RAMP TO CTH F	IH 41	ASHWAUBENON CREEK	B-05-0601	96.9	95	0	302	396
1009-33-27	BROWN	HANSEN ROAD	IH 41	IH 41	B-05-0611	375.5	384	501	1,836	2720
1009-33-27	BROWN	GLORY ROAD	IH 41	IH 41	B-05-0615	213.8	219	317	1,022	1557
1009-33-27	BROWN	CTH EB (LAKEVIEW DR)	USH 41	USH 141/USH 41	B-05-0684	175.8	180	0	1,172	1352
1009-33-27	BROWN	CTH M (LINEVILLE ROAD)	USH 41	USH 141/USH 41	B-05-0685	163.0	188	616	1,232	2036

BROWN COUNTY TOTAL 14071

* SURFACE AREA (INSIDE FACE AND TOP)

Project ID	COUNTY	ROUTE	CORRIDOR	OVER	BRIDGE NO.	LENGTH [FT]	* PARAPETS [SY]	CONCRETE MEDIAN & SIDEWALK [SY]	DECK [SY]	502.3215 PROTECTIVE SURFACE TREATMENT RESEAL [SY]
1009-33-27	MARINETTE	USH 41 (BRIDGE STREET)	USH 41	MENONMINEE RIVER	B-38-0090	868.2	733	1,479	5,595	7807
1009-33-27	MARINETTE	CTH Y	USH 41	USH 41	B-38-0101	265.6	221	600	2,007	2828
1009-33-27	MARINETTE	HALE ROAD	USH 41	USH 41	B-38-0104	265.3	215	0	1,061	1276

MARINETTE COUNTY TOTAL 11912

* SURFACE AREA (INSIDE FACE AND TOP)

LIMITS OF B-38-0090 ARE FROM SOUTH ABUTMENT TO NORTH ABUTMENT ACROSS THE MENOMINEE RIVER

Project ID	COUNTY	ROUTE	CORRIDOR	OVER	BRIDGE NO.	LENGTH [FT]	* PARAPETS [SY]	CONCRETE MEDIAN & SIDEWALK [SY]	DECK [SY]	502.3215 PROTECTIVE SURFACE TREATMENT RESEAL [SY]
1009-33-27	OCONTO	USH 41 NB	USH 41	PENSAUKEE RIVER	B-42-0014	130.8	106	0	625	731
1009-33-27	OCONTO	USH 41 SB	USH 41	PENSAUKEE RIVER	B-42-0048	133.9	109	0	595	704
1009-33-27	OCONTO	CTH S	USH 41	USH 141/USH 41	B-42-0067	220.5	179	426	1,149	1754
1009-33-27	OCONTO	ALLEN ROAD	USH 41	USH 141/USH 41	B-42-0068	214.6	173	135	658	966
1009-33-27	OCONTO	GEANO ROAD	USH 41	USH 41	B-42-0070	214.6	173	135	658	966
1009-33-27	OCONTO	CTH D (SAMPSON ROAD)	USH 41	USH 141/USH 41	B-42-0071	220.5	179	426	1,149	1754
1009-33-27	OCONTO	OAK ORCHARD ROAD	USH 41	USH 141/USH 41	B-42-0072	214.6	173	133	658	964
1009-33-27	OCONTO	USH 141/USH 41 NB	USH 41	LITTLE SUAMICO RIVER	B-42-0079	165.7	134	0	725	860
1009-33-27	OCONTO	BUS 41 RAMP TO USH 41 SB	USH 41	USH 41	B-42-0110	273.3	222	0	881	1102
1009-33-27	OCONTO	USH 41 SB	USH 41	OCONTO RIVER	B-42-0111	717.9	582	0	3,510	4092
1009-33-27	OCONTO	USH 41 NB	USH 41	OCONTO RIVER	B-42-0112	739.9	600	0	3,617	4217
1009-33-27	OCONTO	USH 41 SB RAMP TO BUS 41	USH 41	USH 41	B-42-0118	269.7	219	0	809	1028

OCONTO COUNTY TOTAL 19139

* SURFACE AREA (INSIDE FACE AND TOP)

BROWN COUNTY TOTAL 14071
MARINETTE COUNTY TOTAL 11912
OCONTO COUNTY TOTAL 19139
PROJECT 1009-33-27 TOTAL 45,122

3

3

Project ID	COUNTY	ROUTE	CORRIDOR	OVER	BRIDGE NO.	LENGTH [FT]	* PARAPETS [SY]	CONCRETE MEDIAN & SIDEWALK [SY]	DECK [SY]	502.3215 PROTECTIVE SURFACE TREATMENT RESEAL [SY]
1009-33-34	BROWN	STH 29 (WALNUT STREET)	STH 29	FOX RIVER	B-05-0269-0001	299.2	0	704	1,729	2433
1009-33-34	BROWN	STH 29 (WALNUT STREET)	STH 29	FOX RIVER	B-05-0269-0002	183.0	0	437	358	795
1009-33-34	BROWN	STH 29 (WALNUT STREET)	STH 29	FOX RIVER	B-05-0269-0003	176.5	0	422	1,020	1442
1009-33-34	BROWN	STH 32	STH 32	FOX RIVER	B-05-0339	1,674.0	0	4,557	10,788	15345

BROWN COUNTY TOTAL 20015

* SURFACE AREA (INSIDE FACE AND TOP)

Project ID	COUNTY	ROUTE	CORRIDOR	OVER	BRIDGE NO.	LENGTH [FT]	* PARAPETS [SY]	CONCRETE MEDIAN & SIDEWALK [SY]	DECK [SY]	502.3215 PROTECTIVE SURFACE TREATMENT RESEAL [SY]
1009-33-34	MARINETTE	USH 141	USH 141	MIDDLE INLET CREEK	B-38-0035	35.3	29	0	173	201
1009-33-34	MARINETTE	USH 141	USH 141	UPPER MIDDLE INLET	B-38-0036	35.3	29	0	173	201
1009-33-34	MARINETTE	USH 141	USH 141	LOWER MIDDLE INLET	B-38-0037	60.5	49	0	296	345
1009-33-34	MARINETTE	STH 64	STH 64	S BRANCH BEAVER CREEK	B-38-0045	35.1	0	0	148	148
1009-33-34	MARINETTE	USH 141	USH 141	PESHTIGO RIVER	B-38-0048	187.7	152	0	918	1070
1009-33-34	MARINETTE	USH 8	USH 8	K C CREEK	B-38-0070	53.4	43	0	210	253
1009-33-34	MARINETTE	USH 141	USH 141	S BRANCH BEAVER CREEK	B-38-0082	35.4	0	0	178	178
1009-33-34	MARINETTE	USH 141	USH 141	PIKE RIVER	B-38-0083	89.4	0	0	462	462
1009-33-34	MARINETTE	CTH B - BUS 141	USH 141	USH 141	B-38-0091	242.3	194	225	1,669	2088
1009-33-34	MARINETTE	CTH Q	USH 141	USH 141	B-38-0094	238.0	372	0	952	1324
1009-33-34	MARINETTE	CTH CP	USH 141	USH 141	B-38-0095	254.1	205	188	1,242	1635
1009-33-34	MARINETTE	STH 64	STH 64	PESHTIGO RIVER	B-38-0131	365.7	620	0	1,788	2408

MARINETTE COUNTY TOTAL 10312

* SURFACE AREA (INSIDE FACE AND TOP)

Project ID	COUNTY	ROUTE	CORRIDOR	OVER	BRIDGE NO.	LENGTH [FT]	* PARAPETS [SY]	CONCRETE MEDIAN & SIDEWALK [SY]	DECK [SY]	502.3215 PROTECTIVE SURFACE TREATMENT RESEAL [SY]
1009-33-34	OCONTO	STH 32 - STH 64	STH 32	WAUPEE CREEK	B-42-0025	55.0	45	0	244	289
1009-33-34	OCONTO	STH 32 - STH 64	STH 32	N BRANCH OCONTO RIVER	B-42-0028	119.8	97	0	532	630
1009-33-34	OCONTO	USH 41 SB	USH 141	USH 141 NB	B-42-0042	141.1	114	0	627	742
1009-33-34	OCONTO	USH 141 SB	USH 141	PENSAUKEE RIVER	B-42-0045	103.2	84	0	594	678
1009-33-34	OCONTO	USH 141 NB	USH 141	PENSAUKEE RIVER	B-42-0046	112.0	91	0	684	775
1009-33-34	OCONTO	STH 64	STH 64	PESHTIGO BROOK	B-42-0056	79.8	0	0	336	336
1009-33-34	OCONTO	USH 141 NB	USH 141	OCONTO RIVER	B-42-0074	253.3	206	0	1,109	1314
1009-33-34	OCONTO	USH 141 SB	USH 141	OCONTO RIVER	B-42-0075	177.8	144	0	778	923
1009-33-34	OCONTO	USH 141 NB	USH 141	STH 22	B-42-0076	184.4	150	0	820	969
1009-33-34	OCONTO	USH 141 SB	USH 141	STH 22	B-42-0077	184.4	150	0	820	969
1009-33-34	OCONTO	STH 64	STH 64	HILLS POND CREEK	B-42-0080	42.9	0	0	184	184
1009-33-34	OCONTO	STH 64	STH 64	SECOND S BRANCH OCONTO RIVER	B-42-0085	42.8	0	0	183	183
1009-33-34	OCONTO	STH 64	STH 64	FIRST S BRANCH OCONTO RIVER	B-42-0086	48.9	0	0	209	209
1009-33-34	OCONTO	CTH A	USH 141	USH 141	B-42-0088	242.0	194	547	1,667	2408
1009-33-34	OCONTO	USH 141 NB	USH 141	KELLY BROOK	B-42-0089	109.4	89	0	486	575
1009-33-34	OCONTO	USH 141 SB	USH 141	KELLY BROOK	B-42-0090	109.4	89	0	486	575
1009-33-34	OCONTO	USH 141 NB	USH 141	LITTLE RIVER	B-42-0091	84.2	0	0	393	393
1009-33-34	OCONTO	USH 141 SB	USH 141	LITTLE RIVER	B-42-0092	84.2	0	0	439	439

OCONTO COUNTY TOTAL 12589

* SURFACE AREA (INSIDE FACE AND TOP)

BROWN COUNTY TOTAL 20015

MARINETTE COUNTY TOTAL 10312

OCONTO COUNTY TOTAL 12589

PROJECT 1009-33-34 TOTAL 42,916

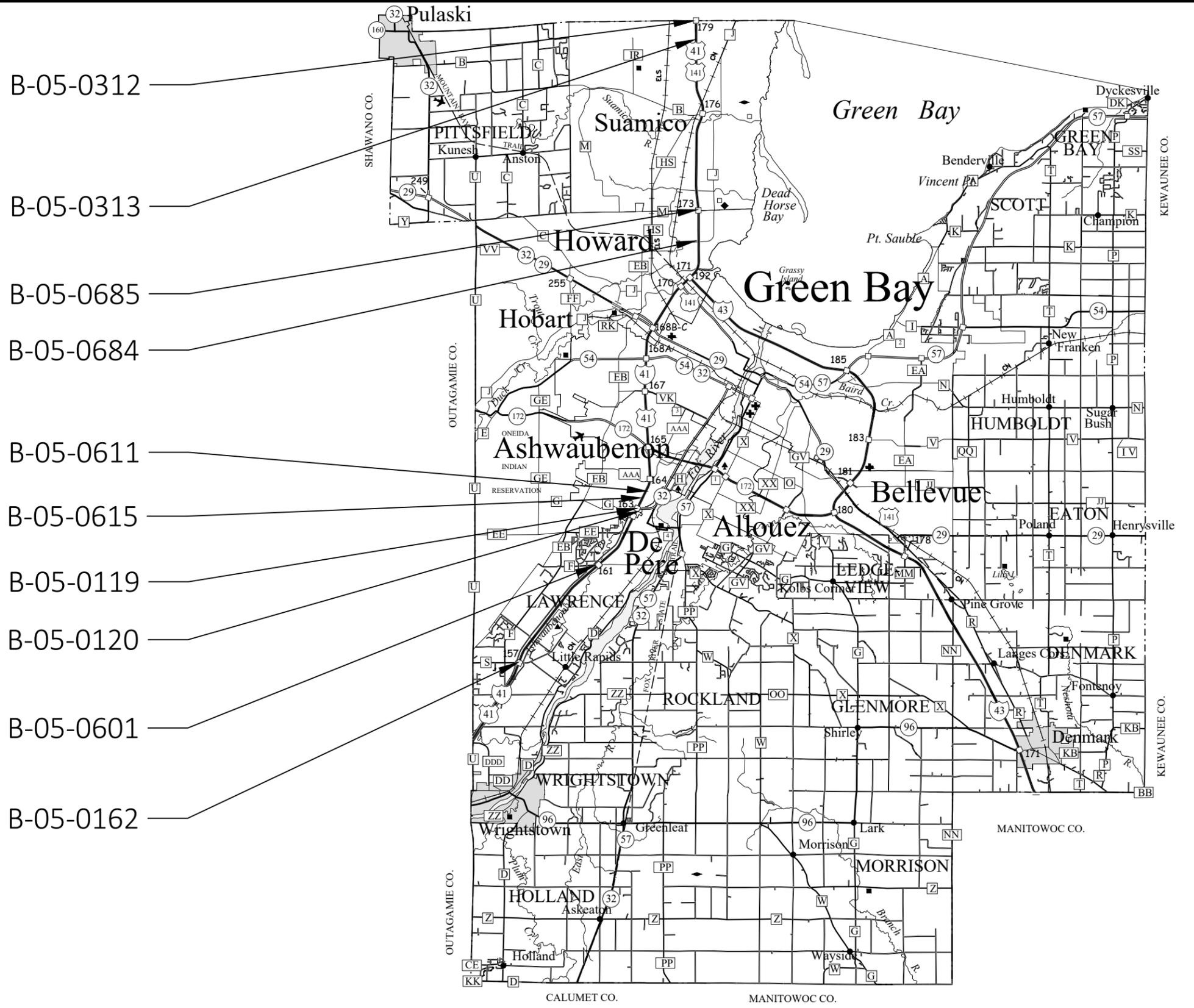
TRAFFIC CONTROL SIGNS PCMS

Project ID	ROADWAY	APPROX. SERVICE PERIOD DAYS	NUMBER IN SERVICE	643.1050 DAY	REMARKS
1009-33-27	IH 41	7	5	35	B-05-0119, B-05-0120, B-05-0601, B-05-0611
	USH 41	7	5	35	B-05-0685, B-38-0090, B-42-0110, B-42-0118
	USH 41/USH 141	7	2	14	B-42-0079
			1009-33-27 Total	84	
1009-33-34	STH 32	7	2	14	B-05-0339
	USH 41	7	1	7	B-42-0042
	USH 141	7	4	28	B-42-0046, B-38-0091
			1009-33-34 Total	49	
			TOTAL 0010	133	

NOTE: ADDITIONAL ADVANCED WARNING WILL BE NECESSARY IF LOCAL BRIDGES ARE COMPLETELY CLOSED.
SEE TRAFFIC CONTROL AND WORK RESTRICTIONS FOR LOCATIONS.

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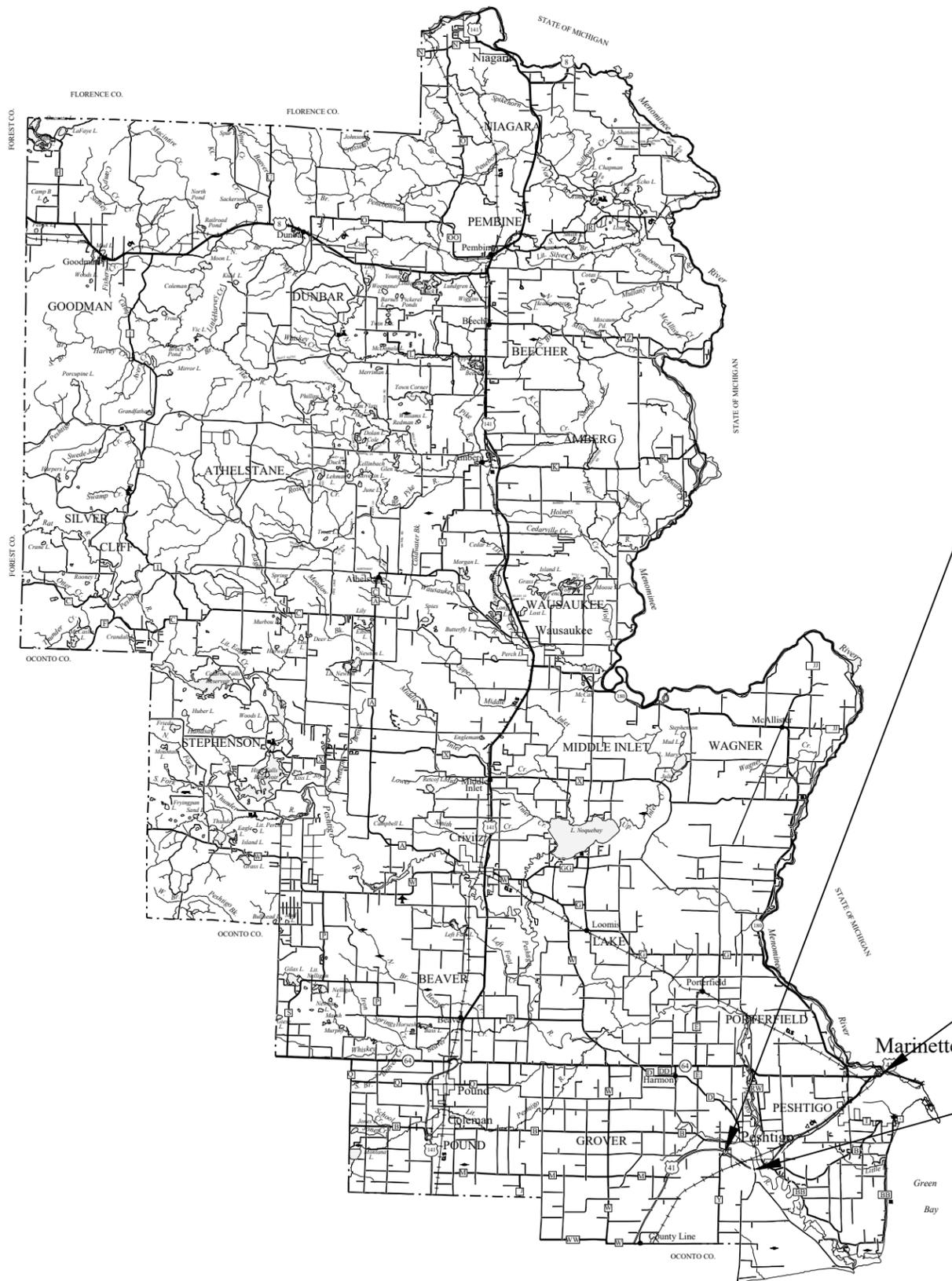
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- B-05-0313
- B-05-0685
- B-05-0684
- B-05-0611
- B-05-0615
- B-05-0119
- B-05-0120
- B-05-0601
- B-05-0162

BROWN COUNTY BACKBONE LOCATION MAP

PROJECT NO: 1009-33-27	HWY: VARIOUS	COUNTY: NORTHEAST REGION WIDE	PLAN	SHEET	E
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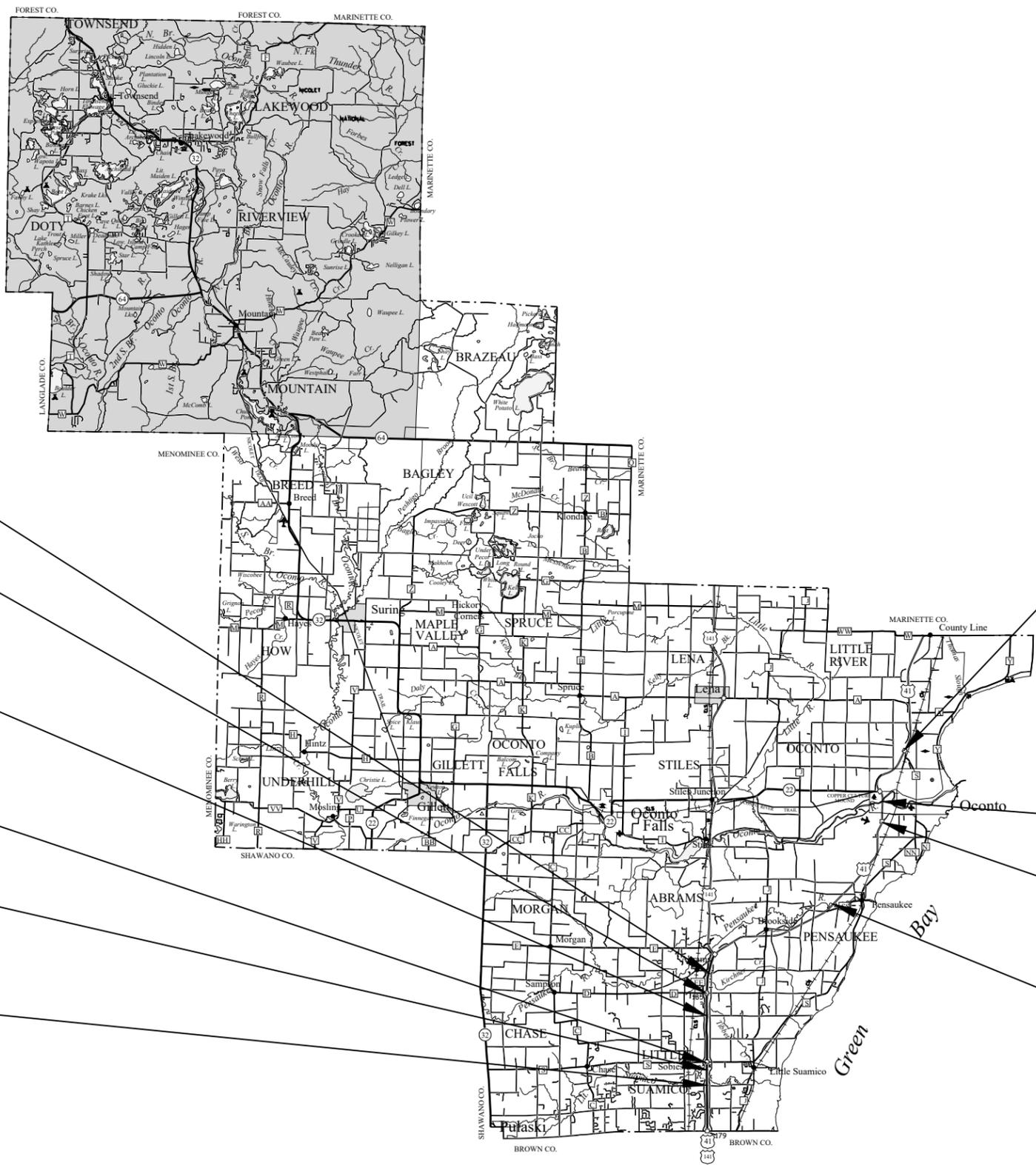


B-38-0101

B-38-0090

B-38-0104

MARINETTE COUNTY BACKBONE ROUTES MAP



B-42-0072

B-42-0071

B-42-0070

B-42-0067

B-42-0079

B-42-0068

B-42-0118

B-42-0111

B-42-0112

B-42-0110

B-42-0014

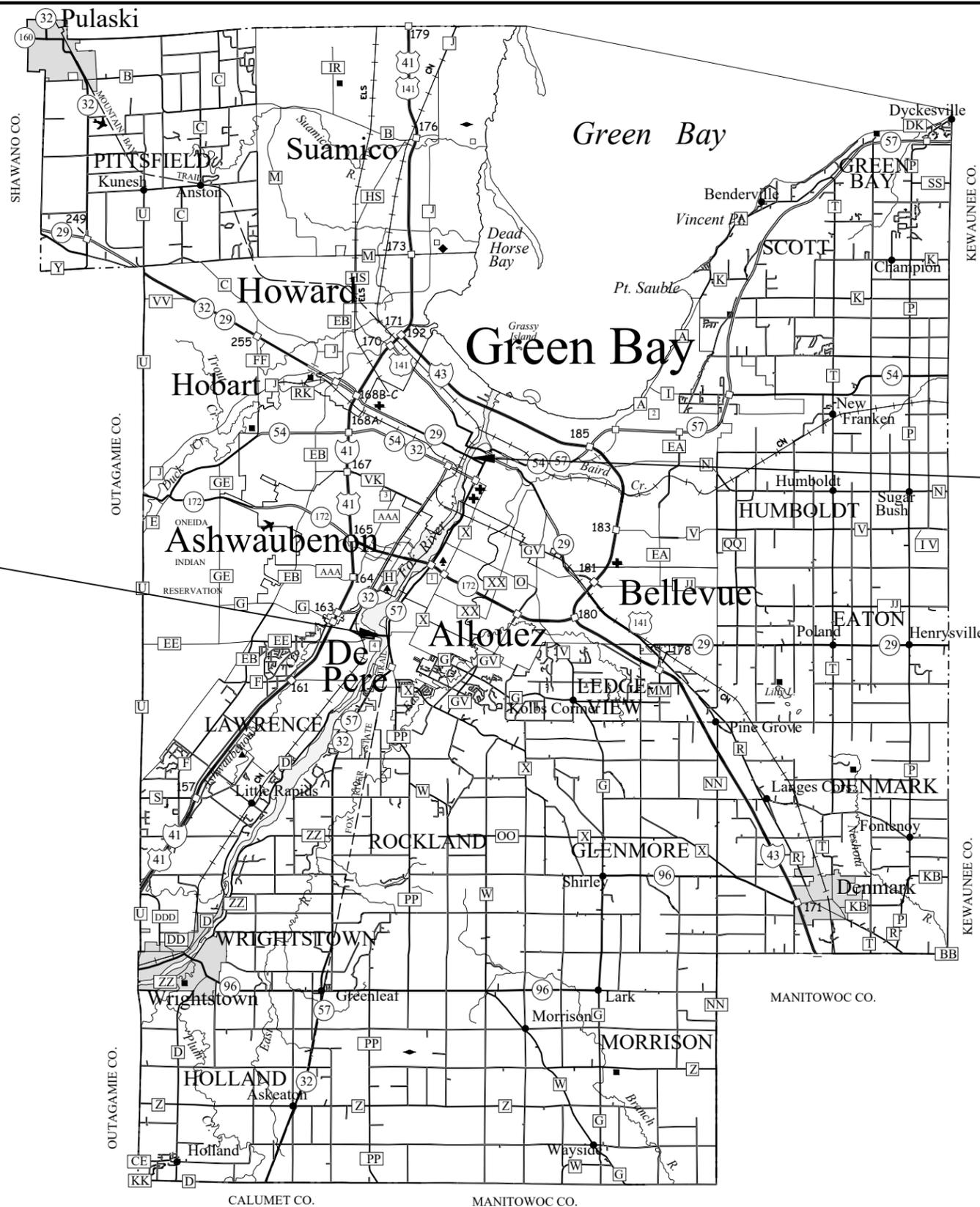
B-42-0048

OCONTO COUNTY BACKBONE ROUTES LOCATION MAP

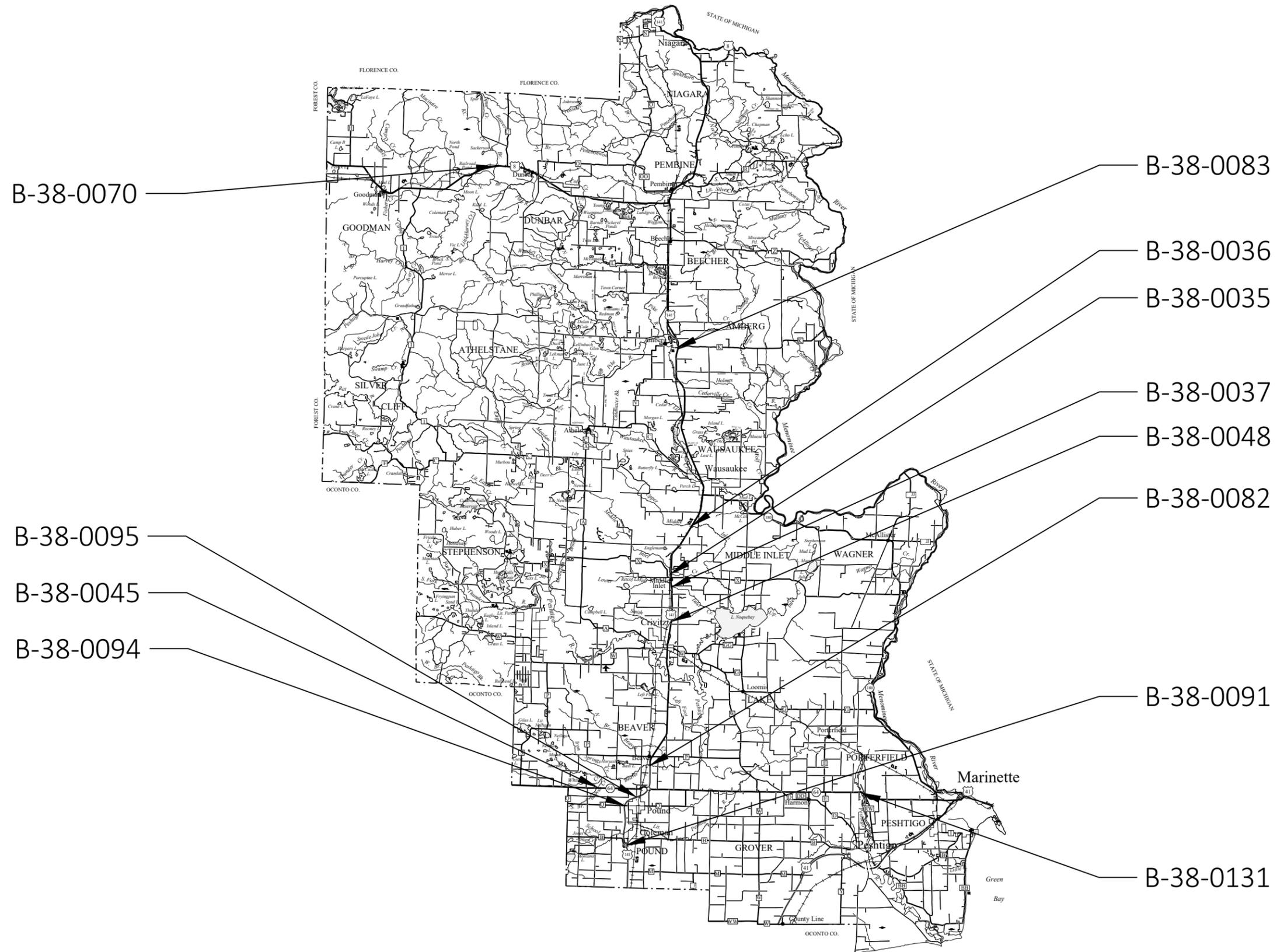
PROJECT NO: 1009-33-27	HWY: VARIOUS	COUNTY: NORTHEAST REGION WIDE	PLAN	SHEET	E
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B-05-0339

B-05-0269-0001
B-05-0269-0002
B-05-0269-0003



BROWN COUNTY 3R ROUTES LOCATION MAP



MARINETTE COUNTY 3R ROUTES MAP

PROJECT NO: 1009-33-34	HWY: VARIOUS	COUNTY: NORTHEAST REGION WIDE	PLAN	SHEET	E
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B-42-0086

B-42-0080

B-42-0085

B-42-0028

B-42-0025

B-42-0045

B-42-0046

B-42-0056

B-42-0091

B-42-0092

B-42-0089

B-42-0090

B-42-0088

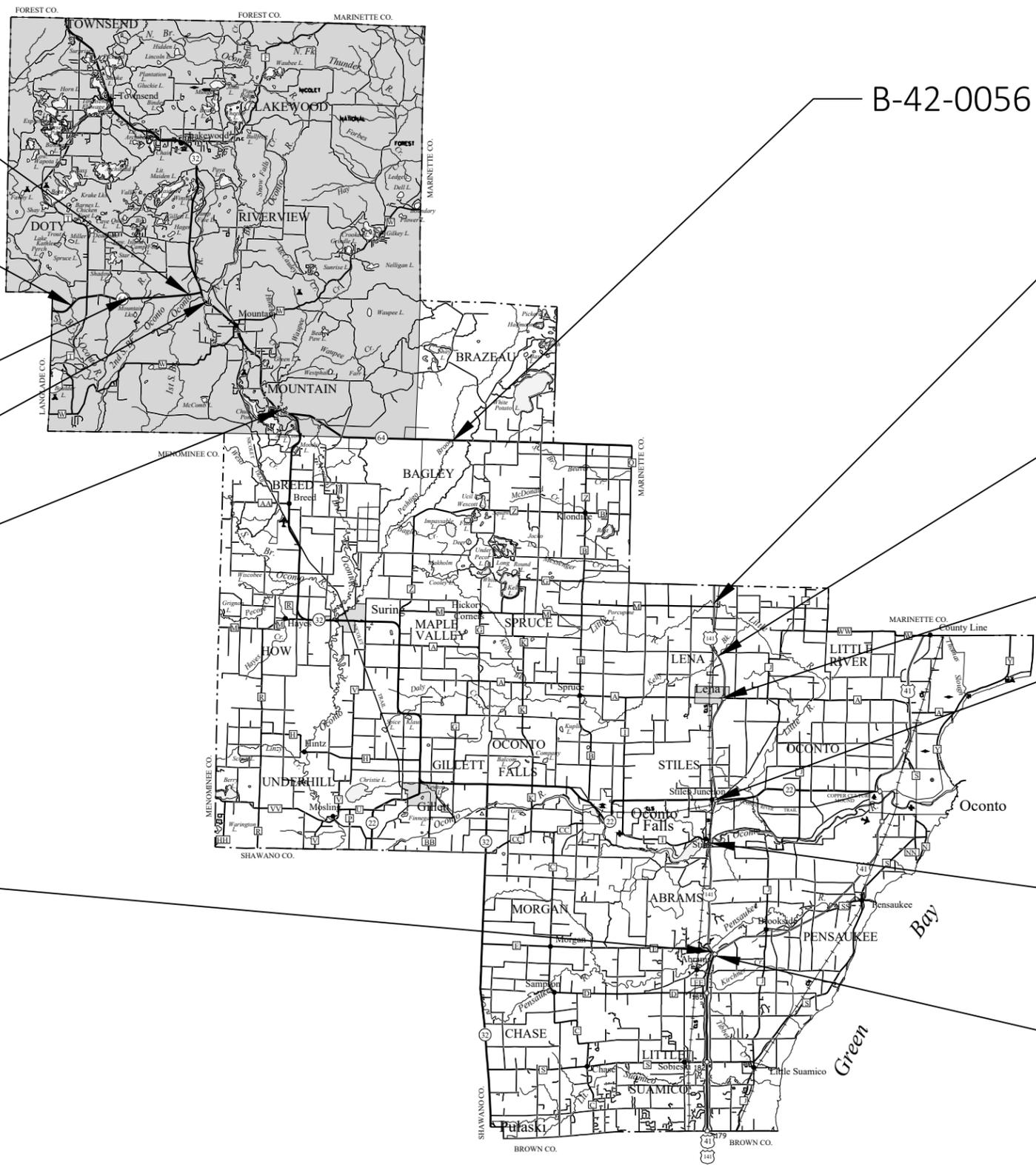
B-42-0076

B-42-0077

B-42-0074

B-42-0075

B-42-0042



OCONTO COUNTY 3R ROUTES LOCATION MAP

PROJECT NO: 1009-33-34	HWY: VARIOUS	COUNTY: NORTHEAST REGION WIDE	PLAN	SHEET	E
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Standard Detail Drawing List

15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS
15C11-09A	CHANNELIZING DEVICES FLEXIBLE TUBULAR MARKER POST
15C11-09B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15D12-10A	TRAFFIC CONTROL, LANE CLOSURE
15D12-10B	TRAFFIC CONTROL, LANE CLOSURE, SPEED REDUCTION
15D14-04	TRAFFIC CONTROL, TWO LANE CLOSURE ON FREEWAY OR EXPRESSWAY, SHORT-TERM (LESS THAN 24 HOURS)
15D15-06A	TRAFFIC CONTROL, PARALLEL ENTRANCE RAMP WITHIN LANE CLOSURE
15D15-06B	TRAFFIC CONTROL, ENTRANCE RAMP WITHIN LANE CLOSURE
15D15-06C	TRAFFIC CONTROL, TAPERED ENTRANCE RAMP WITHIN LANE CLOSURE
15D15-06D	TRAFFIC CONTROL, TAPERED ENTRANCE RAMP WITHIN LANE CLOSURE
15D15-06E	TRAFFIC CONTROL, PARALLEL EXIT RAMP WITHIN LANE CLOSURE
15D16-05	TRAFFIC CONTROL, EXIT RAMP CLOSURE
15D20-06A	TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY
15D20-06B	TRAFFIC CONTROL, SINGLE RIGHT LANE CLOSURE, UNDIVIDED NON-FREEWAY/EXPRESSWAY
15D20-06C	TRAFFIC CONTROL, SINGLE LEFT LANE CLOSURE, UNDIVIDED NON-FREEWAY/EXPRESSWAY
15D21-07A	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE
15D21-07B	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE
15D23-07A	TRAFFIC CONTROL, INTERSECTION WITHIN TWO LANE CLOSURE
15D23-07B	TRAFFIC CONTROL, INTERSECTION WITHIN TWO LANE CLOSURE
15D30-07A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07B	TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION
15D30-07C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07E	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07G	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07J	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D40-04C	TRAFFIC CONTROL, PARTIAL LANE SHIFT NON-FREEWAY OR MULTILANE DIVIDED 45 MPH AND UNDER
15D40-04D	TRAFFIC CONTROL, PARTIAL LANE SHIFT MULTILANE DIVIDED 50 MPH AND GREATER

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

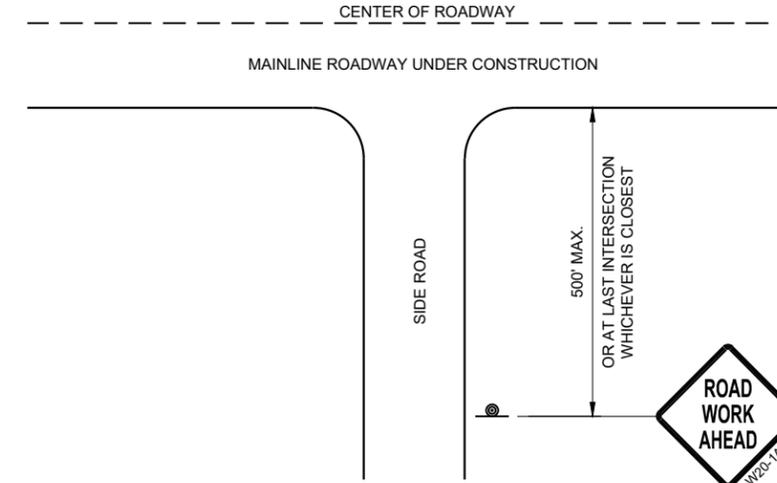
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

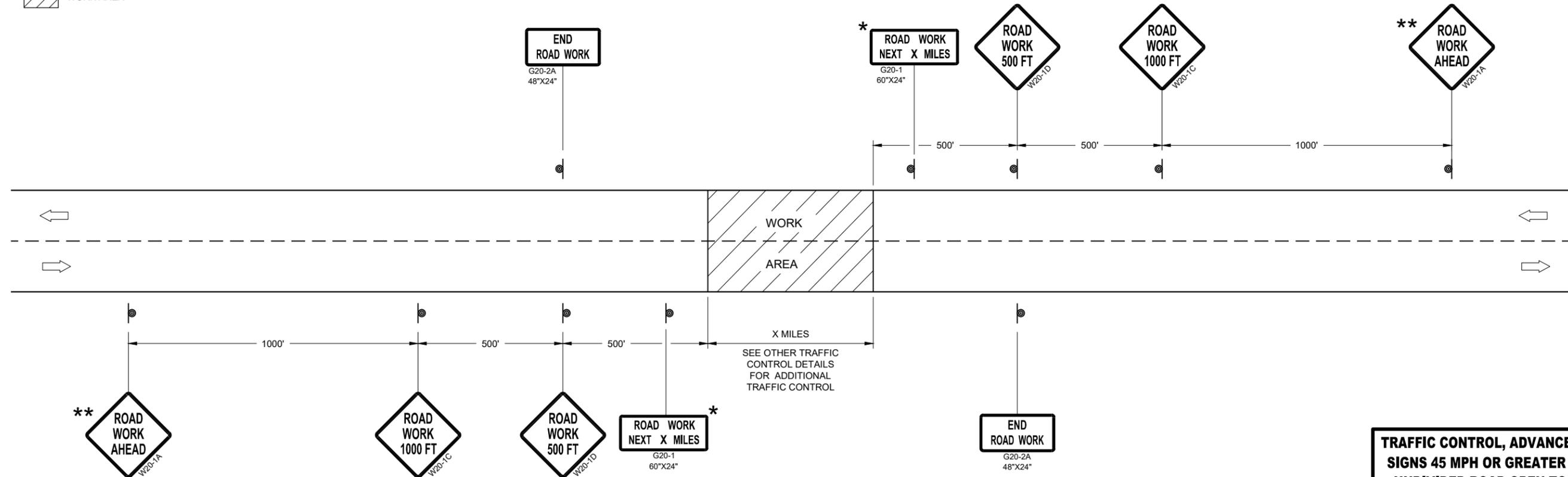
- * OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS
- ** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL**



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

**TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 45 MPH OR GREATER TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED _____
DATE July 2018 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

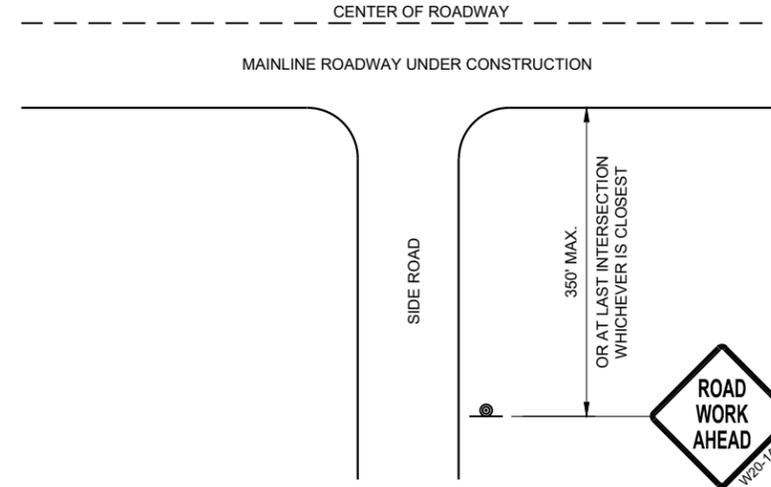
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

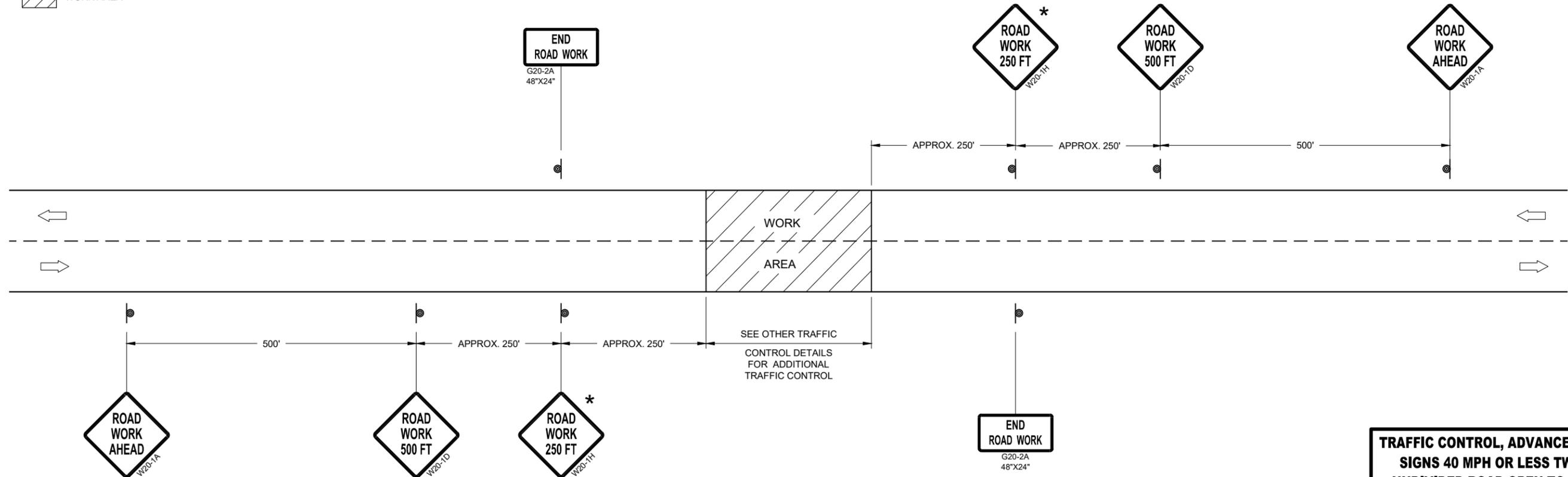
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL**



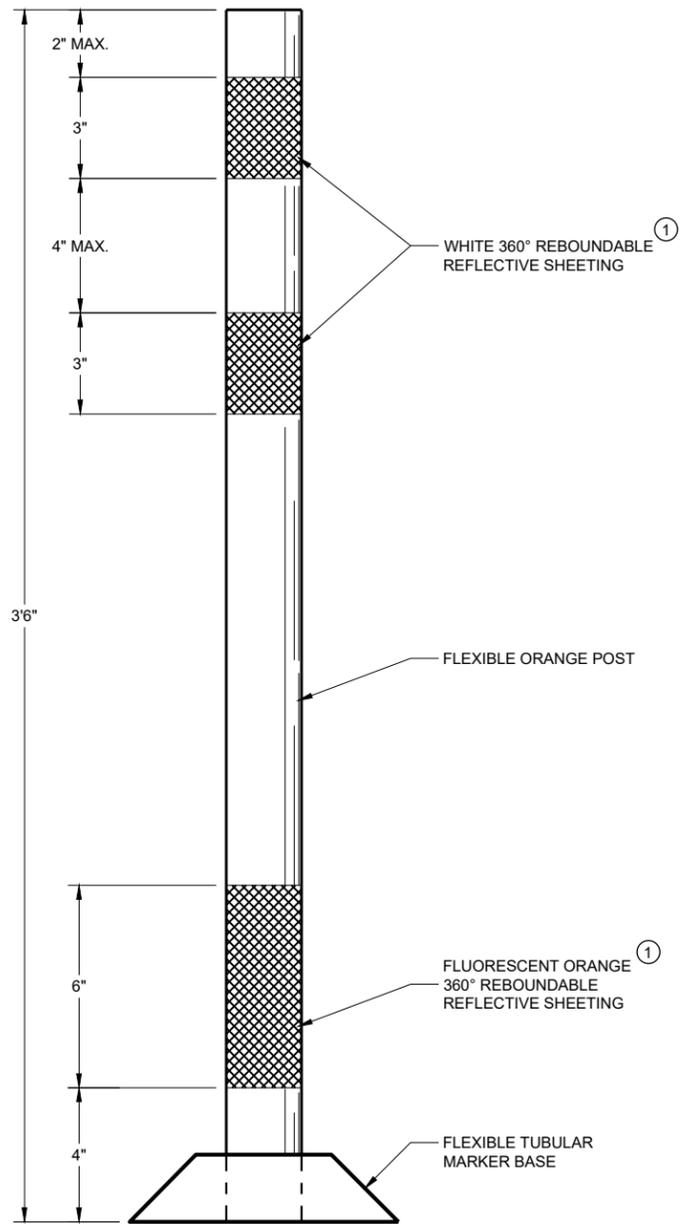
TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

**TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 40 MPH OR LESS TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA



FLEXIBLE TUBULAR MARKER POST WORK ZONE

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

SURFACE MOUNTED BASES SHALL BE FURNISHED IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS TO BE COMPATIBLE WITH FLEXIBLE TUBULAR MARKER POSTS TO A SIZE AND SHAPE THAT WILL PROVIDE A STABLE POST FOUNDATION WHEN SECURED TO THE PAVEMENT.

THE ASPHALTIC ADHESIVE OR BUTYL PAD FURNISHED SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS, UNLESS DIRECTED BY THE ENGINEER TO USE BOLTS.

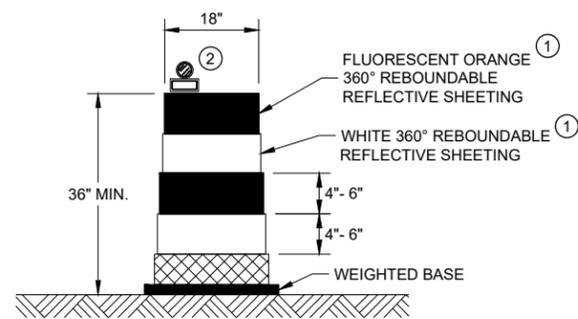
① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.

**CHANNELIZING DEVICES
FLEXIBLE TUBULAR
MARKER POST**

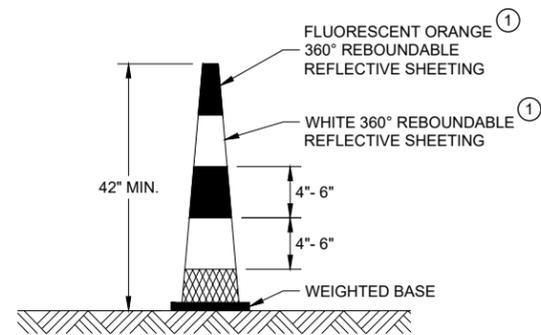
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2021 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

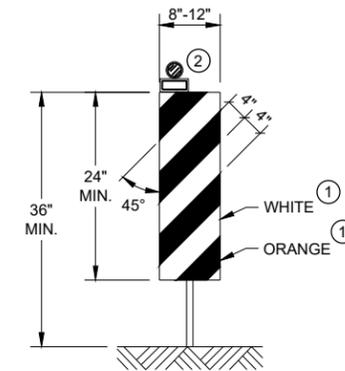


DRUM



42" CONE

DO NOT USE IN TAPERS
 1/2 SPACING OF DRUMS

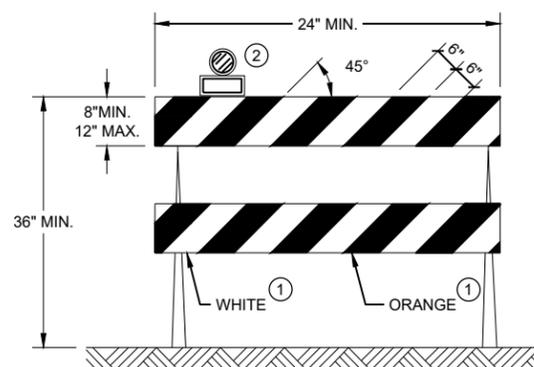


VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.

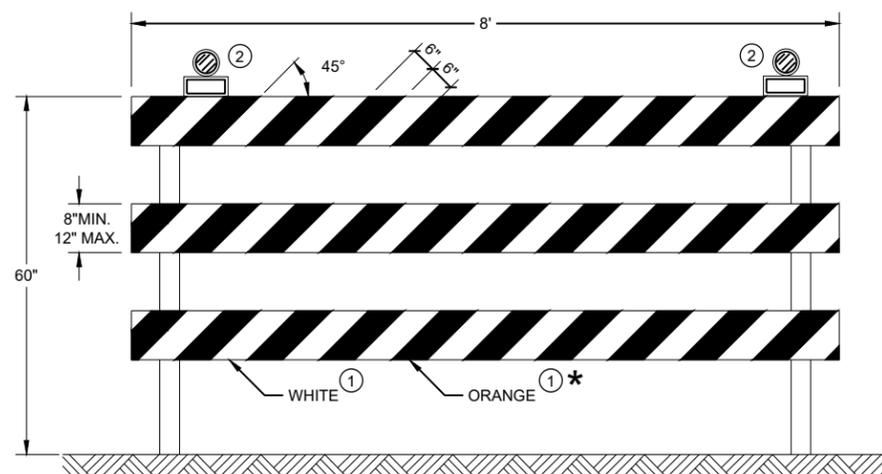
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2021 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
<small>FHWA</small>	

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

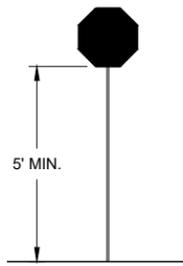
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



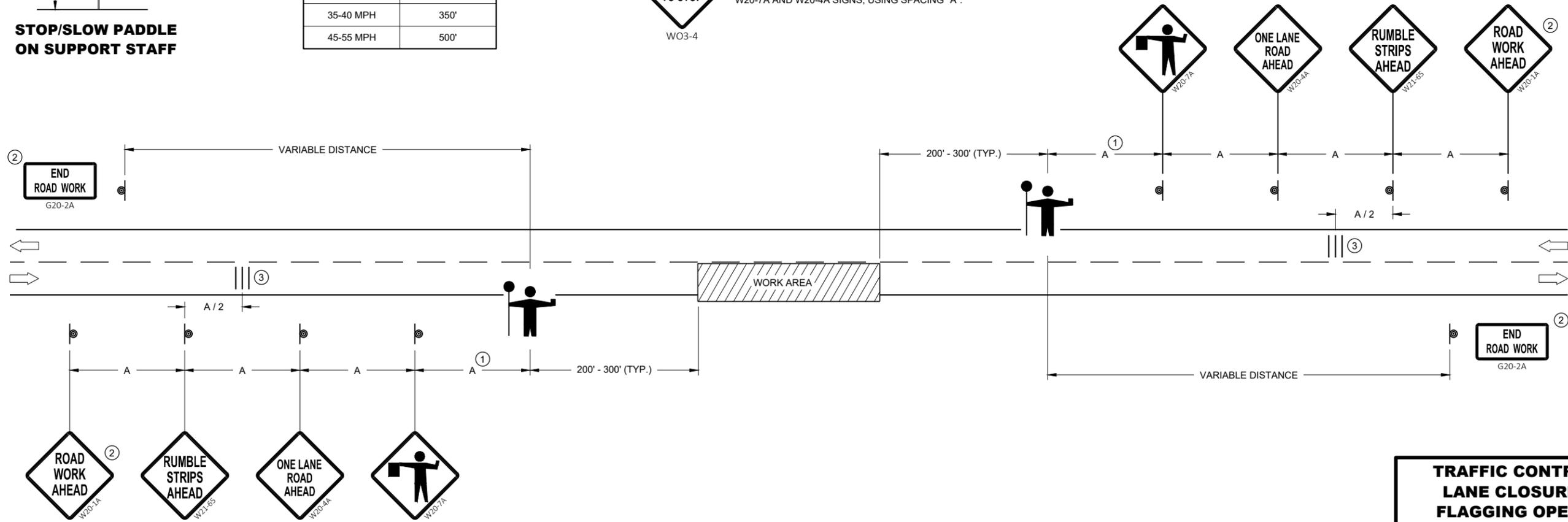
STOP/SLOW PADDLE ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



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TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE May 2022 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

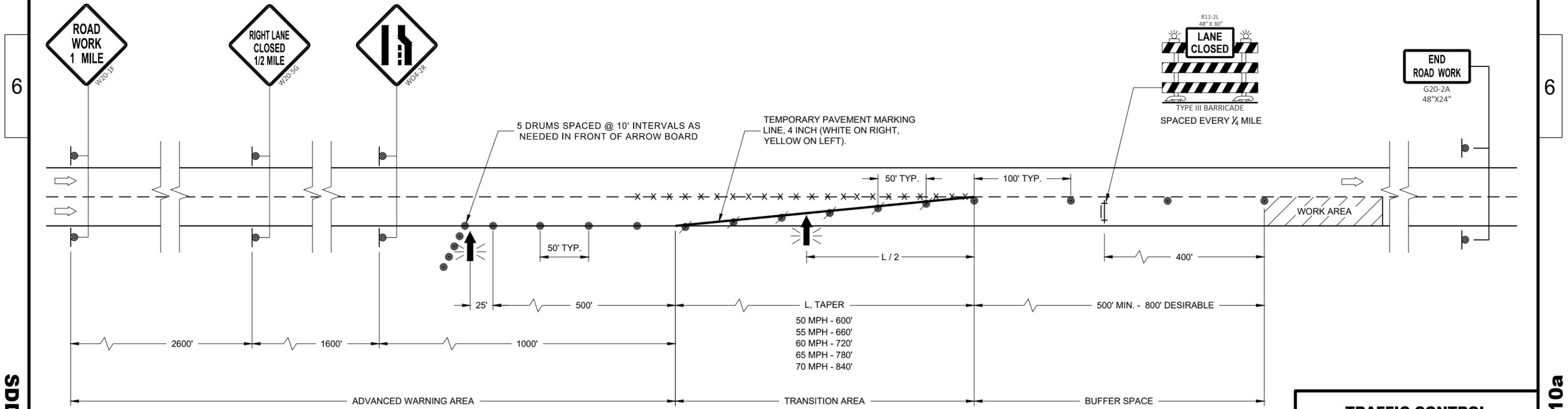
ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS

NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  REMOVING PAVEMENT MARKINGS
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLASHING ARROW BOARD



TRAFFIC CONTROL LANE CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

SDD 15D12 - 10a

SDD 15D12 - 10a

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

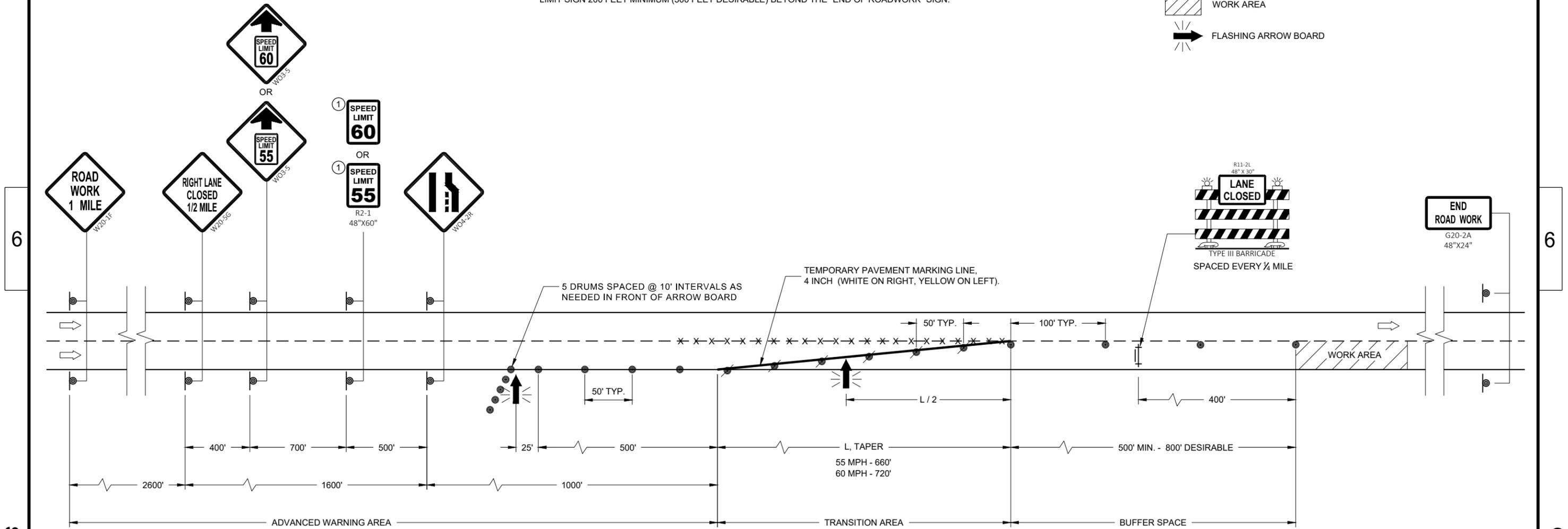
IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

- ① A SPEED LIMIT SIGN SHALL BE LOCATED 1500 FEET BEYOND THE END OF THE ACCELERATION LANE OF EACH ENTRANCE RAMP. PLACE A SPEED LIMIT SIGN A MINIMUM OF EVERY 3 MILES. INCLUDE A RESUME SPEED LIMIT SIGN 200 FEET MINIMUM (500 FEET DESIRABLE) BEYOND THE "END OF ROADWORK" SIGN.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  REMOVING PAVEMENT MARKINGS
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLASHING ARROW BOARD



TRAFFIC CONTROL, LANE CLOSURE, SPEED REDUCTION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

SDD 15D12 - 10b

SDD 15D12 - 10b

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  FLASHING ARROW BOARD
-  DIRECTION OF TRAFFIC
-  WORK AREA

GENERAL NOTES

THIS DETAIL IS TYPICAL FOR CLOSING THE RIGHT TWO LANES. FOR CLOSING THE LEFT TWO LANES, REVERSE THE TRAFFIC CONTROL.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1E AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS.

WHEN A RAMP OR SIDE ROAD INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

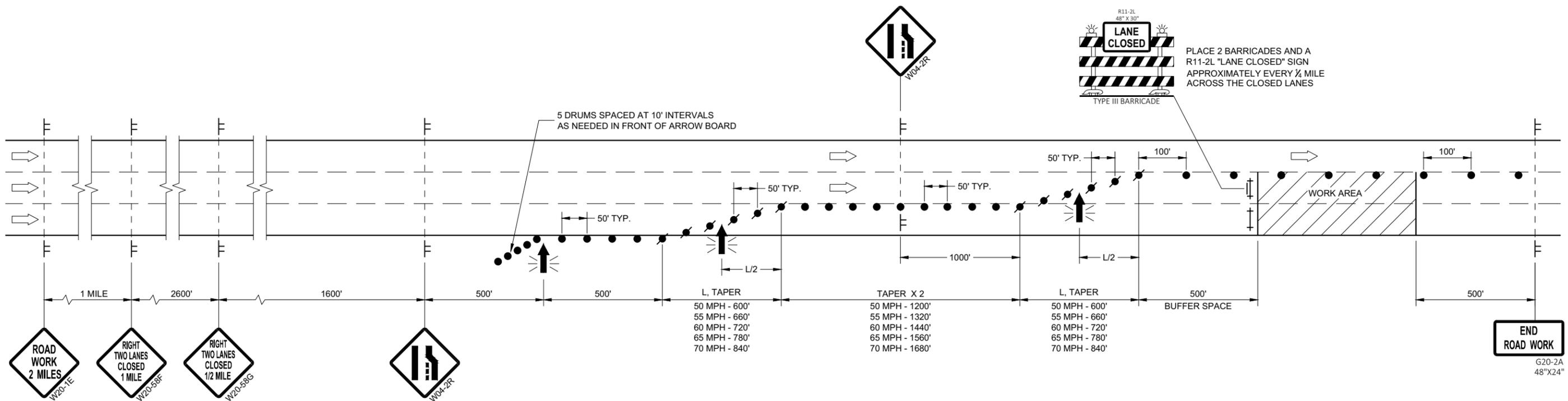
BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

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SDD 15D14 - 04

SDD 15D14 - 04

TRAFFIC CONTROL, TWO LANE CLOSURE ON FREEWAY OR EXPRESSWAY, SHORT TERM (LESS THAN 24 HOURS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2020 DATE /S/ Andrew Heidtke
STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

FHWA

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  DIRECTION OF TRAFFIC

GENERAL NOTES

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONSECUTIVE DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS. USE SUPPORTS THAT PROVIDE A MINIMUM OF 5 FEET FROM THE BOTTOM OF THE SIGN TO THE PAVEMENT.

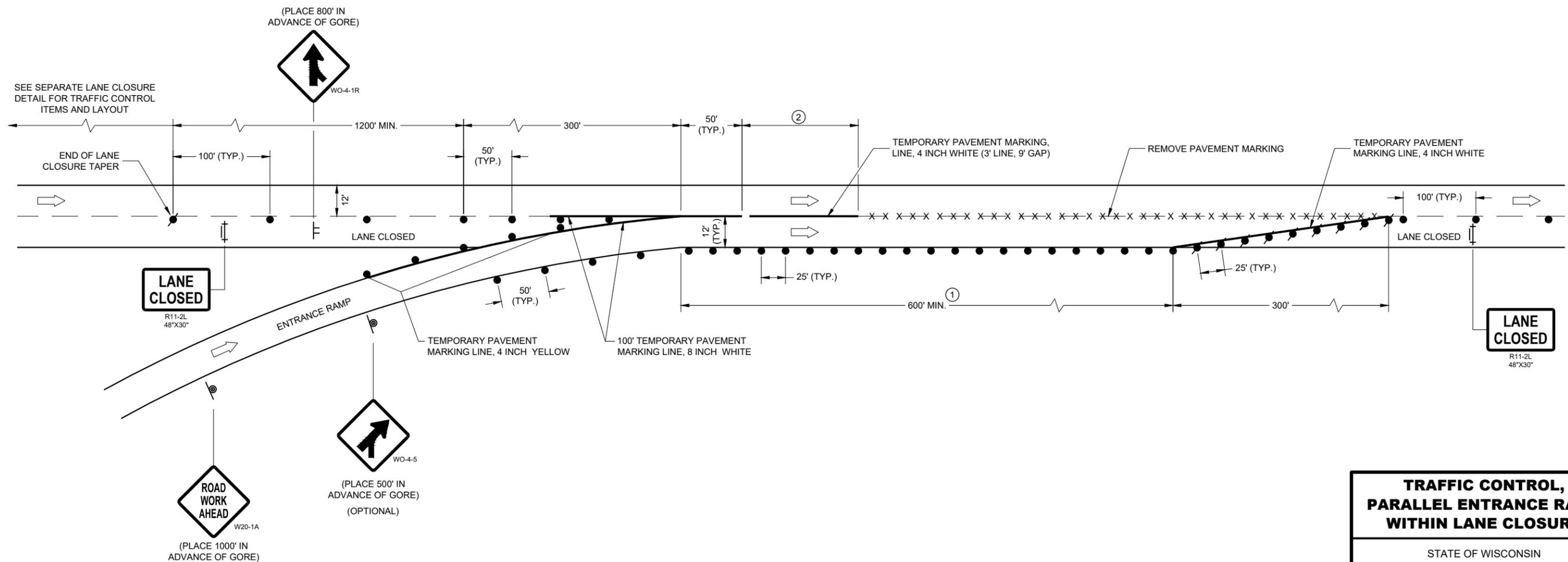
IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE ENTRANCE RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINES IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

- ① EXTEND THE LENGTH OF THE MERGE ARE IF THE ENTERING (DESIGN) SPEED IS LESS THAN 50MPH OR IF THE MAINLINE GRADE EXCEEDS ±2.2%.
- ② END TEMPORARY PAVEMENT MARKING LINE AT ½ THE LENGTH OF FULL WIDTH OF THE ACCELERATION LANE.



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SDD 15D15 - 06a

SDD 15D15 - 06a

TRAFFIC CONTROL, PARALLEL ENTRANCE RAMP WITHIN LANE CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  DIRECTION OF TRAFFIC

GENERAL NOTES

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONSECUTIVE DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS /OR DRUMS IN THE GORE BETWEEN THE EXIT RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINES IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

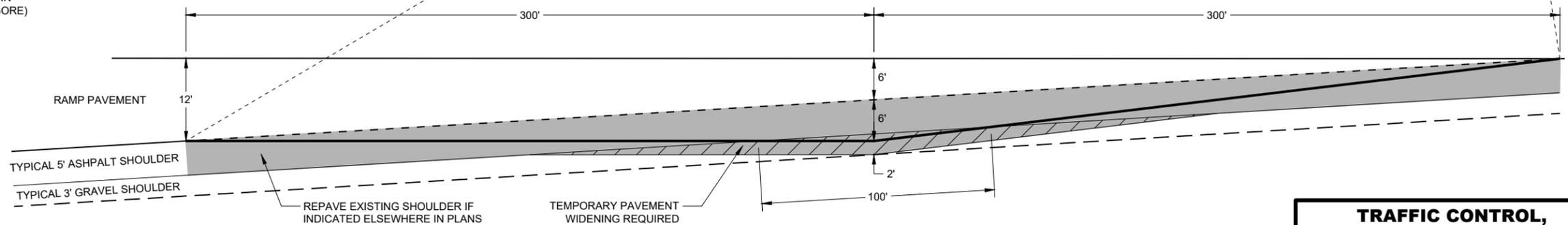
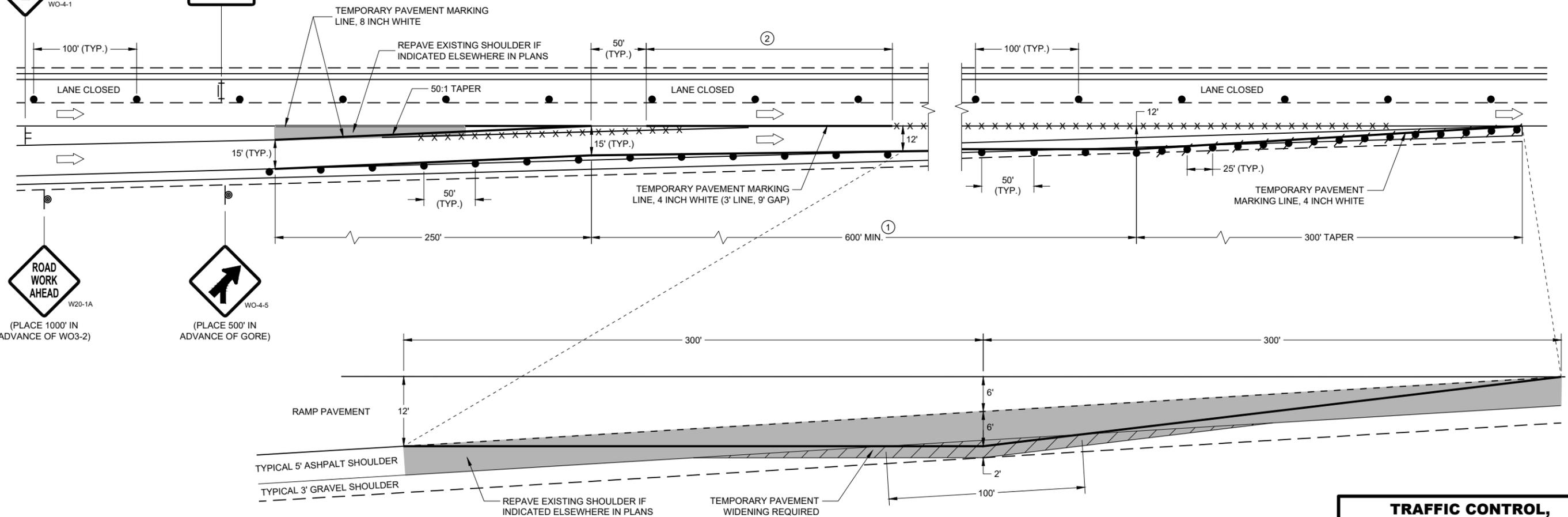
- ① EXTEND THE LENGTH OF THE MERGE AREA IF THE ENTERING (DESIGN) SPEED OF THE RAMP IS LESS THAN 50 MPH OR IF THE MAINLINE GRADE EXCEEDS ±2.2%.
- ② END TEMPORARY PAVEMENT MARKING LINE AT ½ THE LENGTH OF FULL WIDTH OF THE ACCELERATION LANE.

(PLACE 800' IN ADVANCE OF GORE)



(PLACE 1000' IN ADVANCE OF WO3-2)

(PLACE 500' IN ADVANCE OF GORE)



TEMPORARY PAVEMENT DETAIL
(EXISTING RAMP DIMENSIONS MAY VARY, ADJUST TEMPORARY PAVEMENT ACCORDINGLY)

TRAFFIC CONTROL, ENTRANCE RAMP WITHIN LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  FLAGS, 16" X 16" MIN., ORANGE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

YIELD SIGN AND WARNING SIGNS ON ENTRANCE RAMP ARE ALSO APPROPRIATE FOR CLOSURE OF THE MAINLINE LEFT LANE. OMIT THE YIELD SIGN IF MORE THAN ONE LANE REMAINS OPEN ON THE MAINLINE AND THE RAMP TAPER IS AT LEAST AS LONG AS THE NORMAL ENTRANCE RAMP TAPER AT THE SITE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONSECUTIVE DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS. USE SUPPORTS THAT PROVIDE A MINIMUM OF 5 FEET FROM THE BOTTOM OF THE SIGN TO THE PAVEMENT.

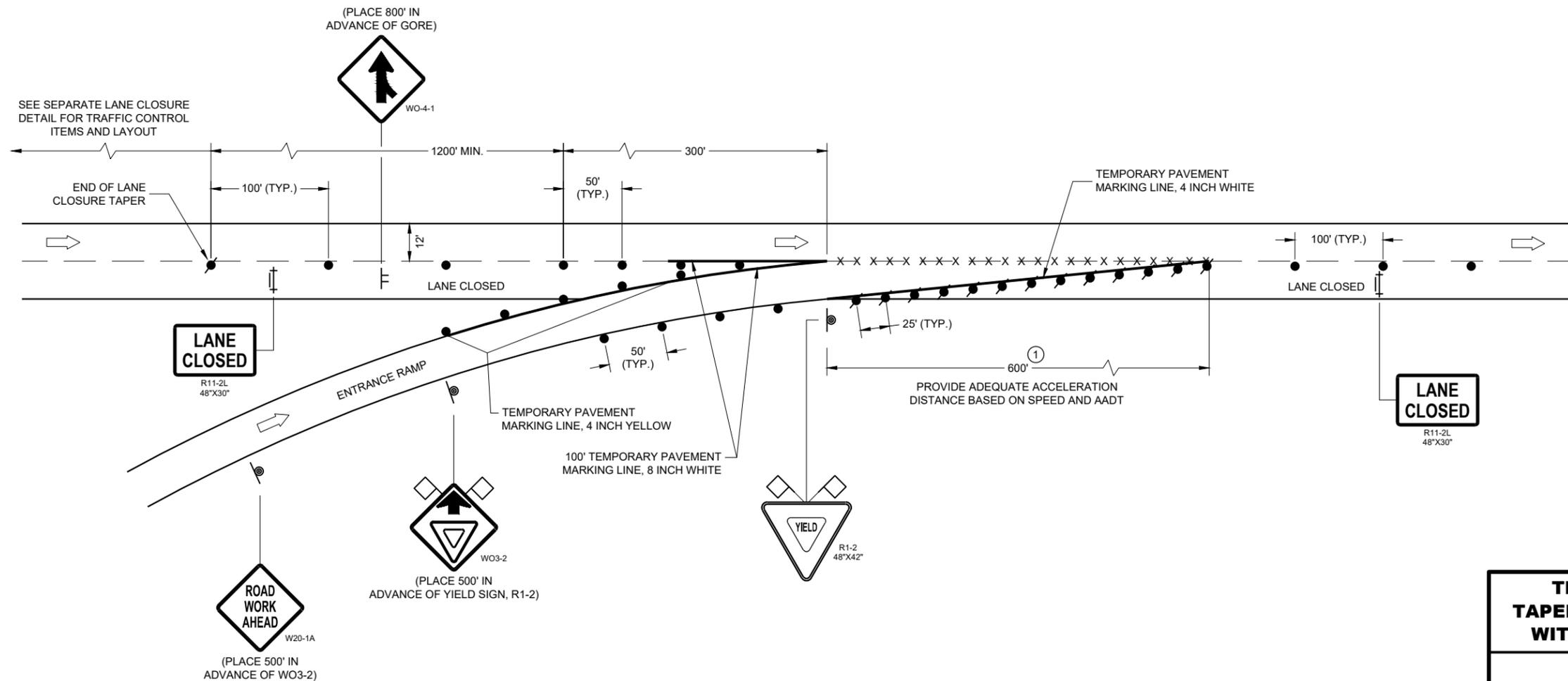
IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE ENTRANCE RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINES IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

① CONSULT WITH REGIONAL WORK ZONE ENGINEER IF NEED TO REDUCE LENGTH EXISTS.



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SDD 15D15 - 06C

SDD 15D15 - 06C

**TRAFFIC CONTROL,
TAPERED ENTRANCE RAMP
WITHIN LANE CLOSURE**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  FLAGS, 16" X 16" MIN., ORANGE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

YIELD SIGN AND WARNING SIGNS ON ENTRANCE RAMP ARE ALSO APPROPRIATE FOR CLOSURE OF THE MAINLINE LEFT LANE. OMIT THE YIELD SIGN IF MORE THAN ONE LANE REMAINS OPEN ON THE MAINLINE AND THE RAMP TAPER IS AT LEAST AS LONG AS THE NORMAL ENTRANCE RAMP TAPER AT THE SITE.

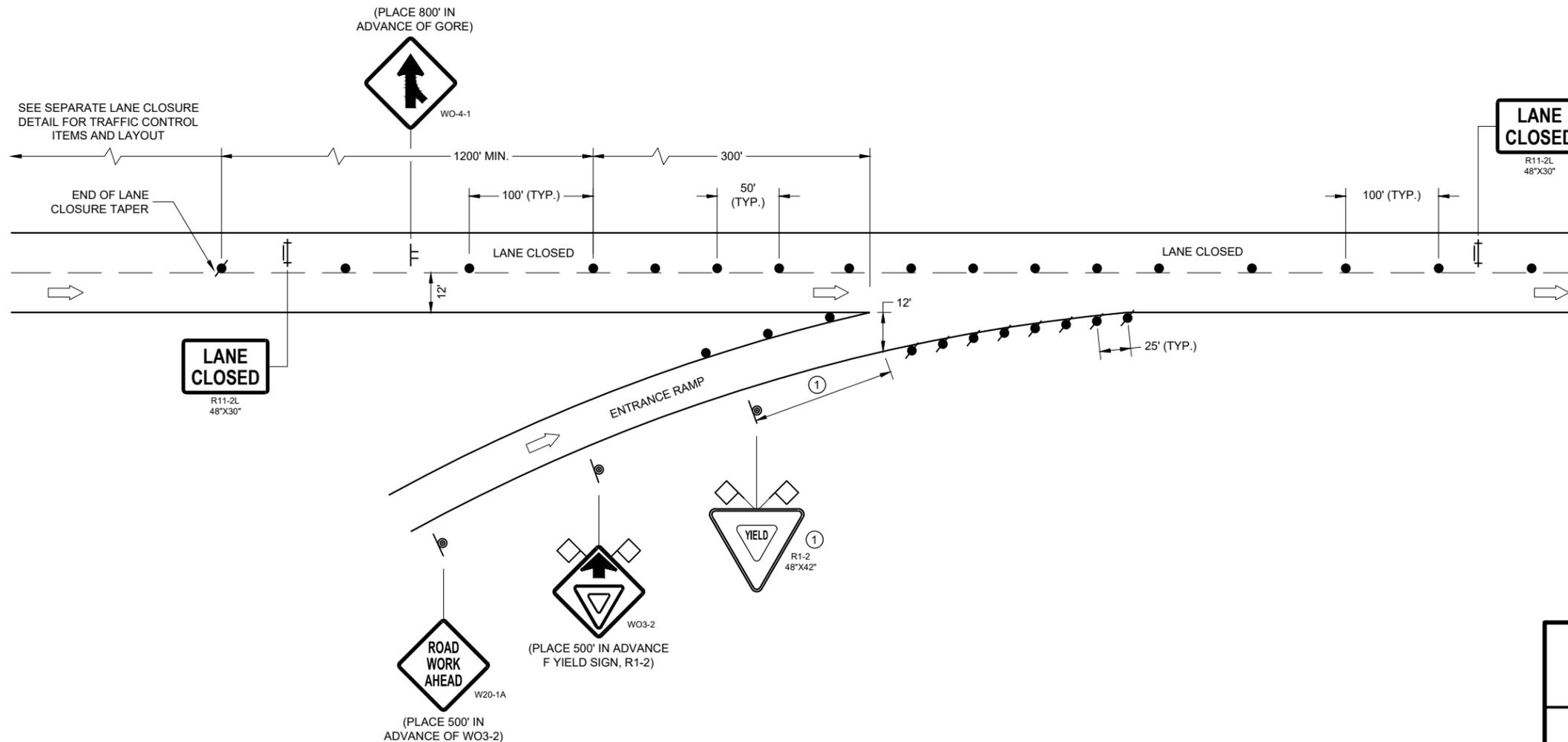
SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONSECUTIVE DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS. USE SUPPORTS THAT PROVIDE A MINIMUM OF 5 FEET FROM THE BOTTOM OF THE SIGN TO THE PAVEMENT.

IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE ENTRANCE RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

- ① PLACE YIELD SIGN TO PROVIDE ADEQUATE SIGHT DISTANCE AND ACCELERATION DISTANCE.



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SDD 15D15 - 06d

SDD 15D15 - 06d

TRAFFIC CONTROL, TAPERED ENTRANCE RAMP WITHIN LANE CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ⊞ SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- ⦿ TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- ×-×-× REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- ⊞ TYPE III BARRICADE WITH ATTACHED SIGN
- ➡ DIRECTION OF TRAFFIC

GENERAL NOTES

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

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THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

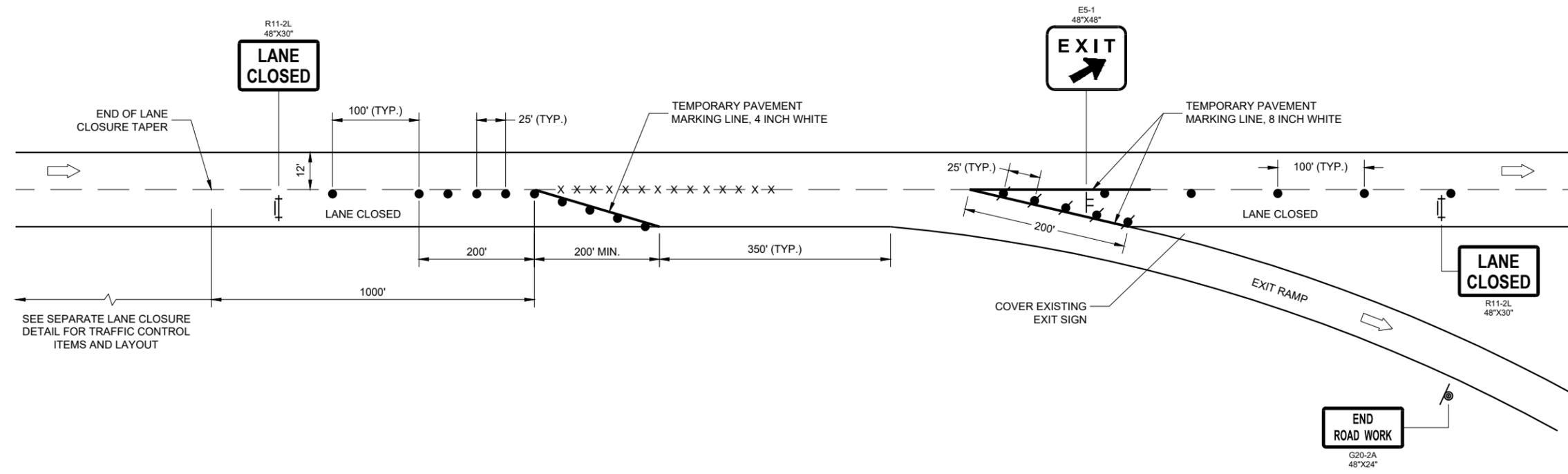
SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONSECUTIVE DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE EXIT RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINES IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.



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SDD 15D15 - 06e

SDD 15D15 - 06e

TRAFFIC CONTROL, PARALLEL EXIT RAMP WITHIN LANE CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

- † TYPE III BARRICADE
- †† TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- ⦿ SIGN ON PERMANENT SUPPORT
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ➡ DIRECTION OF TRAFFIC

GENERAL NOTES

THIS RAMP CLOSURE DETAIL IS TYPICAL FOR CLOSING A RIGHT SIDE EXIT RAMP. FOR A LEFT SIDE EXIT RAMP, REVERSE THE TRAFFIC CONTROL.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS, TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

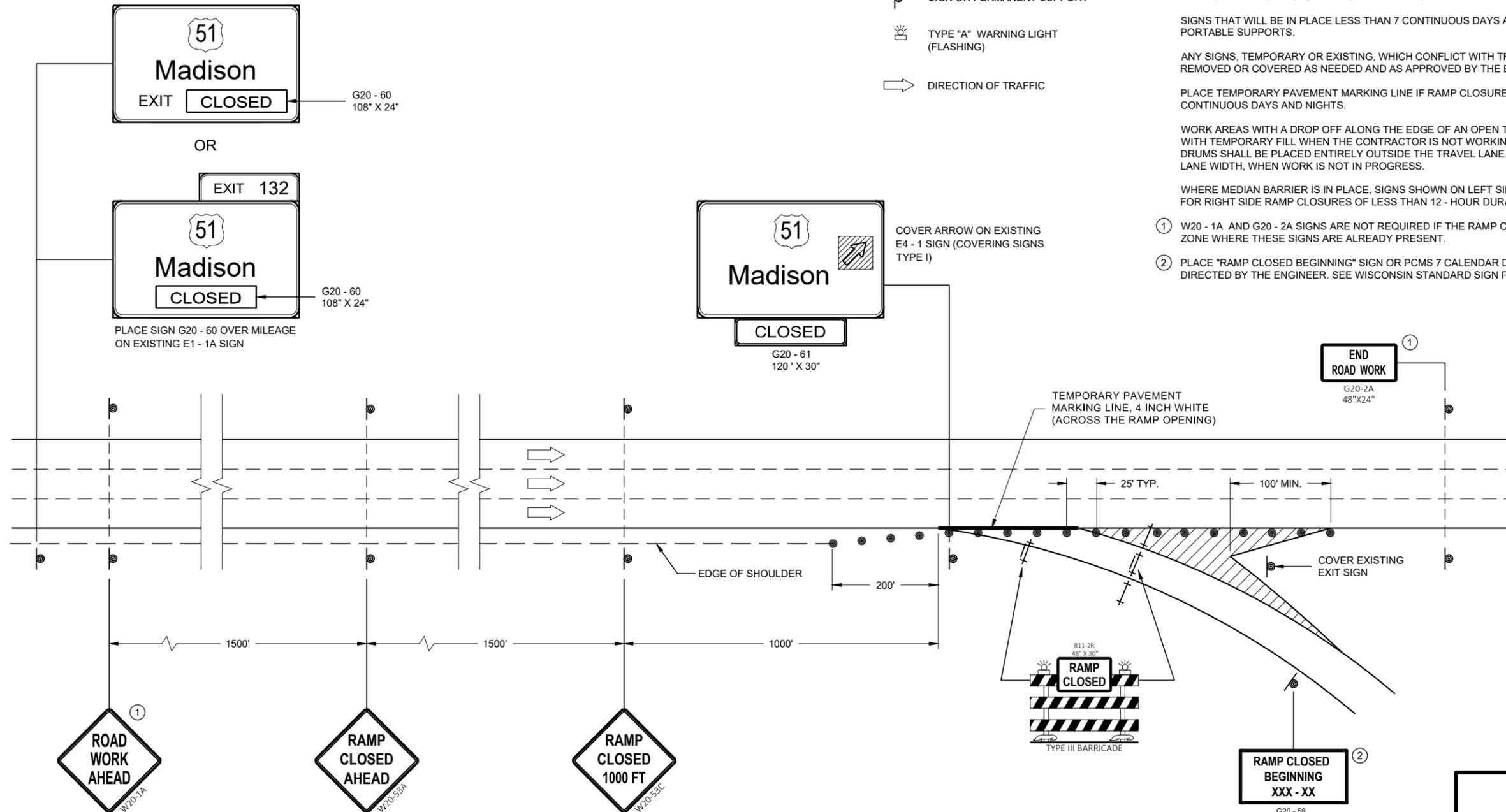
PLACE TEMPORARY PAVEMENT MARKING LINE IF RAMP CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WORK AREAS WITH A DROP OFF ALONG THE EDGE OF AN OPEN TRAVEL LANE SHALL BE LEVELED WITH TEMPORARY FILL WHEN THE CONTRACTOR IS NOT WORKING ADJACENT TO THE TRAVEL LANE. DRUMS SHALL BE PLACED ENTIRELY OUTSIDE THE TRAVEL LANE, ALLOWING THE FULL UNOBSTRUCTED LANE WIDTH, WHEN WORK IS NOT IN PROGRESS.

WHERE MEDIAN BARRIER IS IN PLACE, SIGNS SHOWN ON LEFT SIDE OF ROADWAY MAY BE OMITTED FOR RIGHT SIDE RAMP CLOSURES OF LESS THAN 12 - HOUR DURATION.

① W20 - 1A AND G20 - 2A SIGNS ARE NOT REQUIRED IF THE RAMP CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

② PLACE "RAMP CLOSED BEGINNING" SIGN OR PCMS 7 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR SIGN LAYOUT.



RAMP CLOSED BEGINNING
G20 - 58
OR
PCMS MESSAGING

FRAME 1	FRAME 2
RAMP TO CLOSE	XXXDAY XX XX XX

**TRAFFIC CONTROL,
EXIT RAMP CLOSURE**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2022 /S/ Andrew Heidtke
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

FHWA

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  FLASHING ARROW BOARD
-  DIRECTION OF TRAFFIC
-  REMOVE PAVEMENT MARKING (SEE GENERAL NOTES)
-  WORK AREA

GENERAL NOTES

FOR WORK ON ROADWAYS WITH SPEEDS GREATER THAN 45MPH, USE SDD 15D12.

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

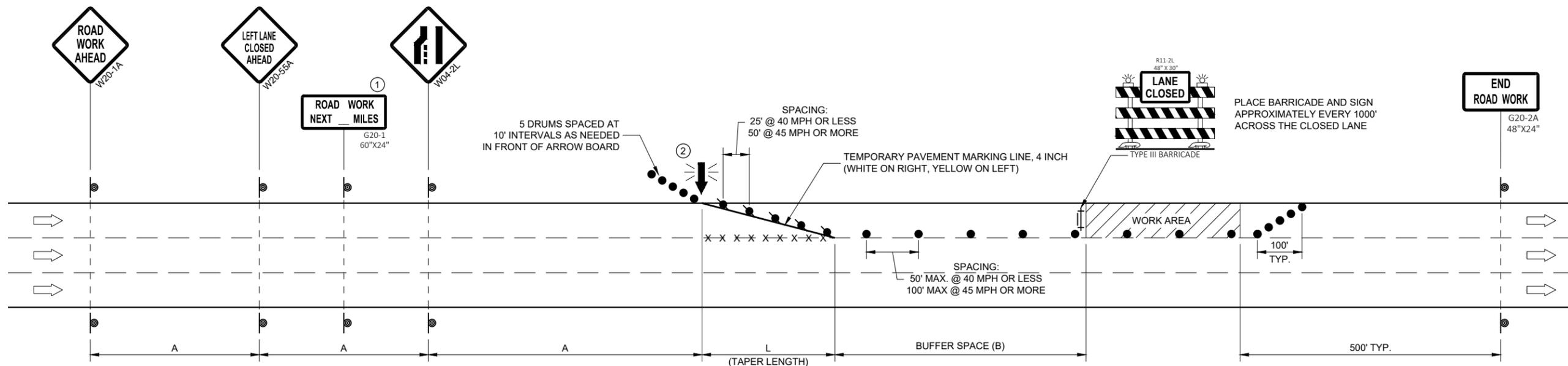
CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

- ① OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.
- ② WHERE THE SHOULDER OR TERRACE HAS INSUFFICIENT SPACE TO PLACE THE ARROW BOARD AS SHOWN, PLACE THE ARROW BOARD AT THE END OF THE TAPER.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'

TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  FLASHING ARROW BOARD
-  DIRECTION OF TRAFFIC
-  REMOVE PAVEMENT MARKING (SEE GENERAL NOTES)
-  WORK AREA

GENERAL NOTES

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

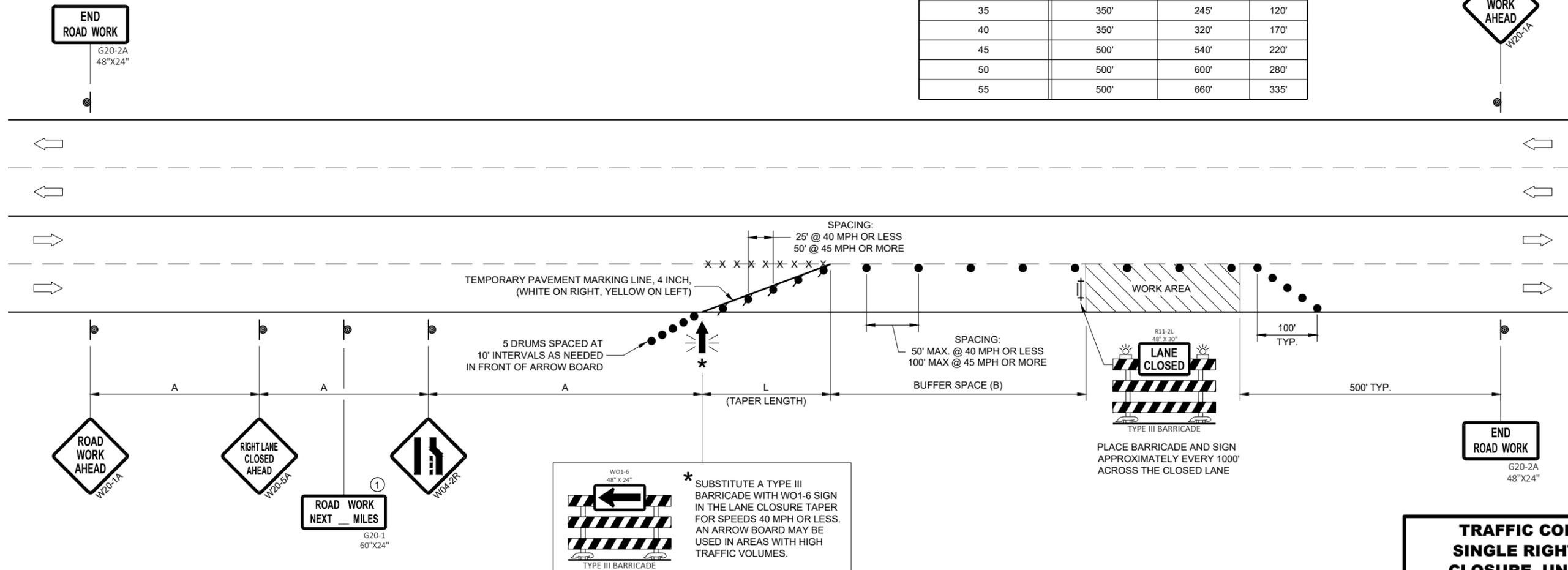
① OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'
50	500'	600'	280'
55	500'	660'	335'



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SDD 15D20 - 06b

SDD 15D20 - 06b

**TRAFFIC CONTROL,
SINGLE RIGHT LANE
CLOSURE, UNDIVIDED
NON-FREEWAY/EXPRESSWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  FLASHING ARROW BOARD
-  DIRECTION OF TRAFFIC
-  REMOVE PAVEMENT MARKING (SEE GENERAL NOTES)
-  WORK AREA

GENERAL NOTES

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

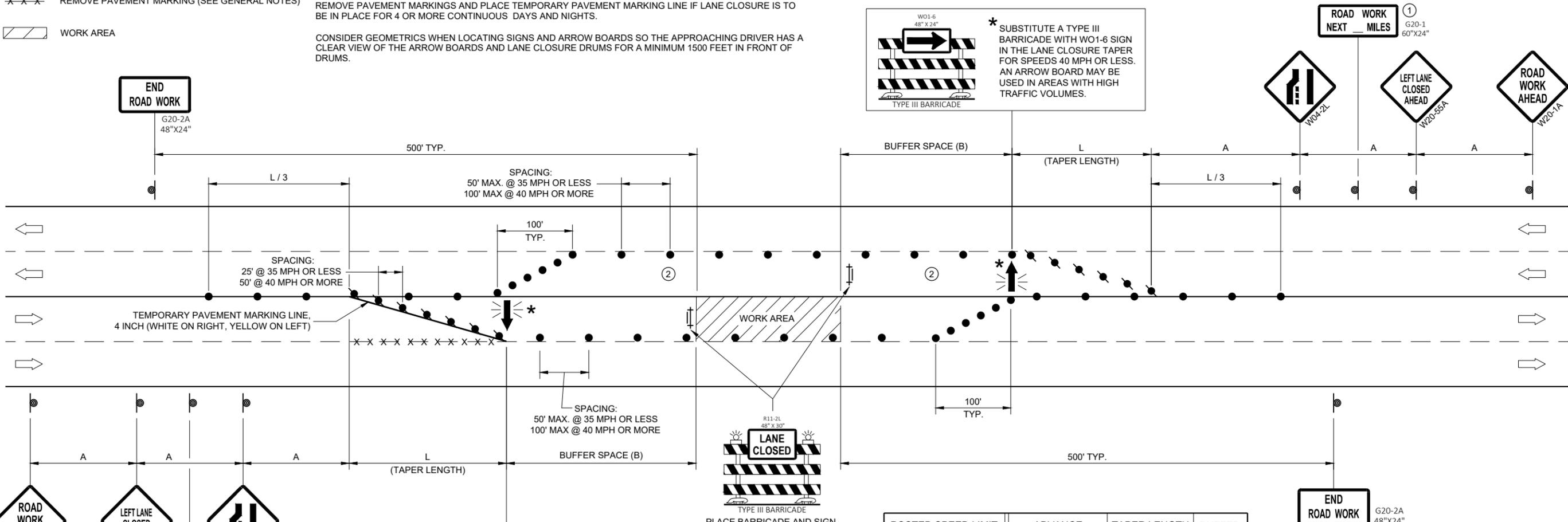
DUE TO LACK OF SHOULDER/MEDIAN, ARROW BOARD IS PLACED AT THE THE END OF THE TAPER.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

- ① OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.
- ② LANE MAY BE OPENED WHEN WORKERS ARE NOT PRESENT IN THE WORK AREA.



*** SUBSTITUTE A TYPE III BARRICADE WITH WO1-6 SIGN IN THE LANE CLOSURE TAPER FOR SPEEDS 40 MPH OR LESS. AN ARROW BOARD MAY BE USED IN AREAS WITH HIGH TRAFFIC VOLUMES.**

*** SUBSTITUTE A TYPE III BARRICADE WITH WO1-6 SIGN IN THE LANE CLOSURE TAPER FOR SPEEDS 40 MPH OR LESS. AN ARROW BOARD MAY BE USED IN AREAS WITH HIGH TRAFFIC VOLUMES.**

R11-2L 48"X30"

PLACE BARRICADE AND SIGN APPROXIMATELY EVERY 1000' ACROSS THE CLOSED LANE.

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'
50	500'	600'	280'
55	500'	660'	335'

**TRAFFIC CONTROL,
SINGLE LEFT LANE
CLOSURE, UNDIVIDED
NON-FREEWAY/EXPRESSWAY**

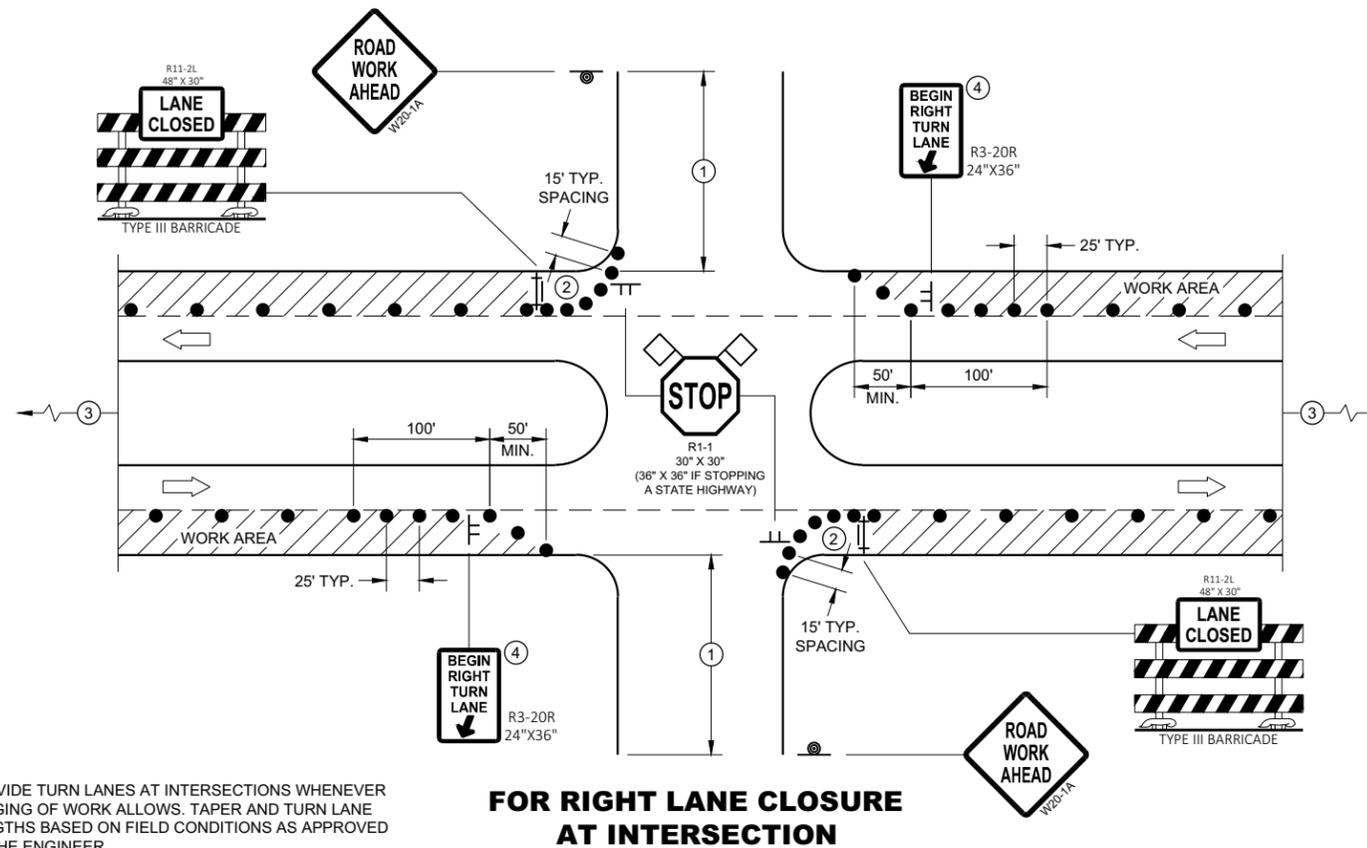
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

SDD 15D20 - 06C

SDD 15D20 - 06C



PROVIDE TURN LANES AT INTERSECTIONS WHENEVER STAGING OF WORK ALLOWS. TAPER AND TURN LANE LENGTHS BASED ON FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" MAY BE USED IF APPROVED BY THE DISTRICT TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

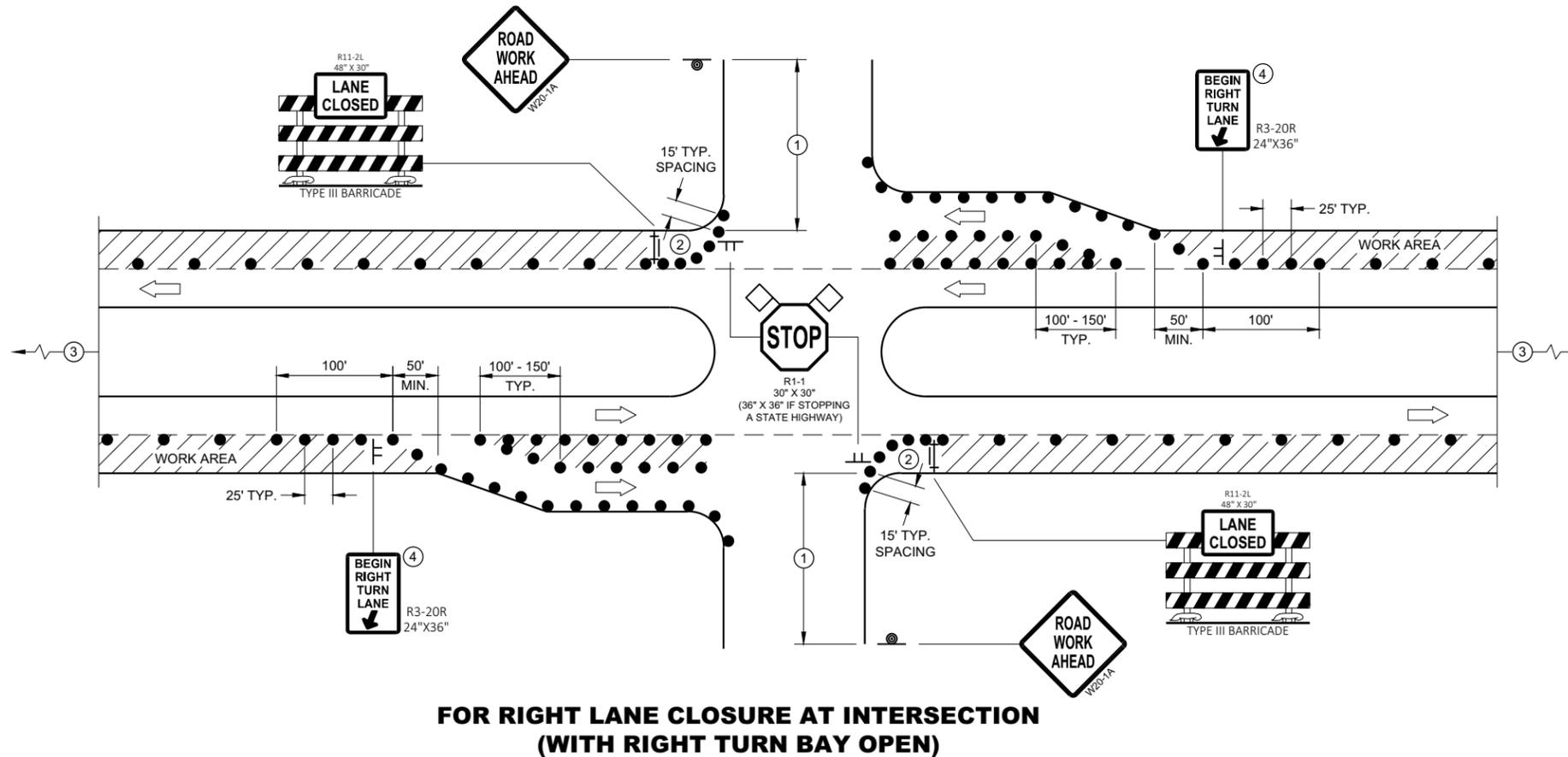
SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL REMAIN IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON PORTABLE SUPPORTS.

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- ① 500' TYPICAL OR AT LAST INTERSECTION, WHICHEVER IS CLOSER.
350' IF 35 - 40 MPH.
200' IF 25 - 30 MPH.
- ② ALSO USE BARRICADE AND 15 FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS
- ③ SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.
- ④ MINIMUM MOUNTING HEIGHT OF 5 FEET FROM EDGE OF PAVEMENT (AT EDGE LINE LOCATION) TO BOTTOM OF SIGN.

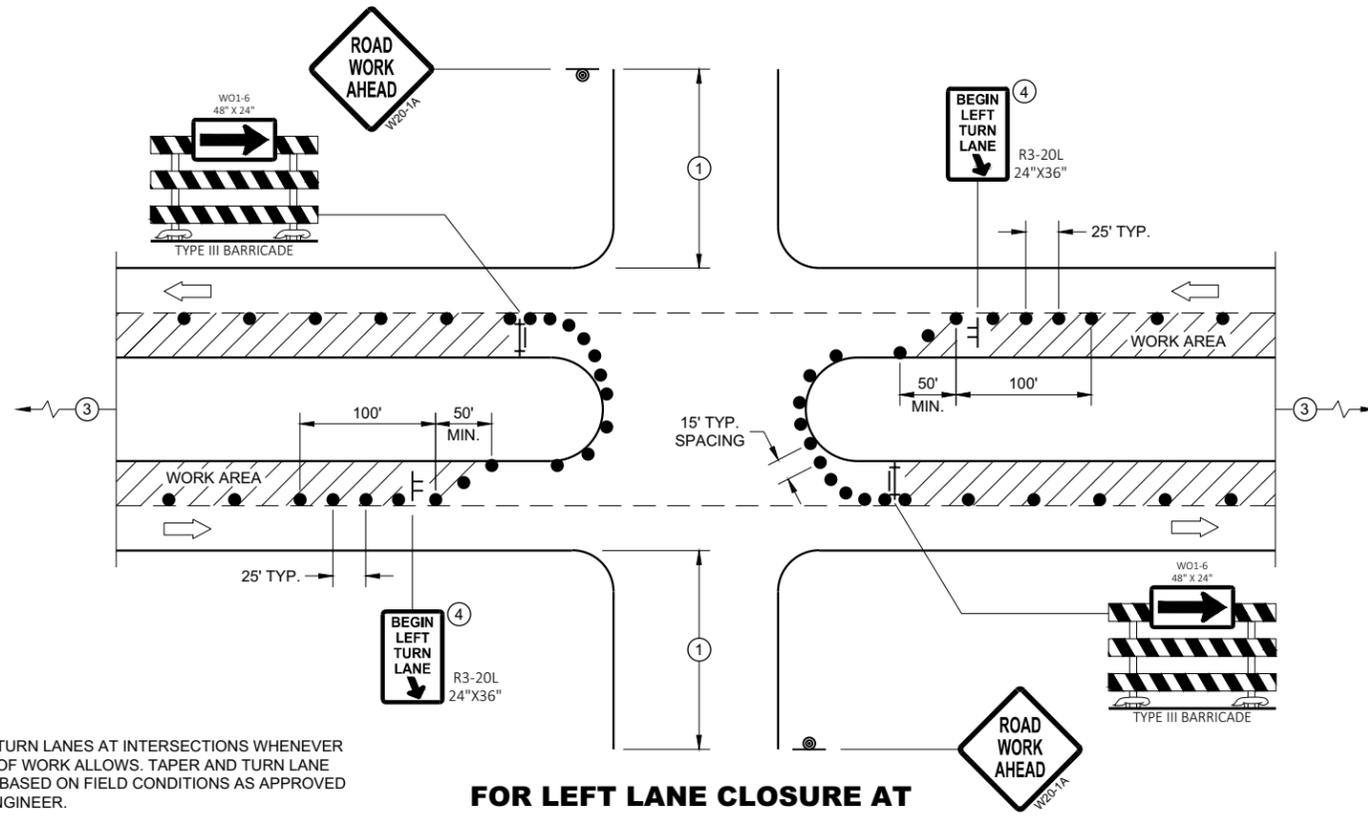


LEGEND

- SIGN ON TEMPORARY SUPPORT
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- DIRECTION OF TRAFFIC
- FLAGS, 16" X 16" MIN., ORANGE
- WORK AREA

TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE RIGHT LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PROVIDE TURN LANES AT INTERSECTIONS WHENEVER STAGING OF WORK ALLOWS. TAPER AND TURN LANE LENGTHS BASED ON FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

FOR LEFT LANE CLOSURE AT INTERSECTION OR MEDIAN OPENING

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" MAY BE USED IF APPROVED BY THE DISTRICT TRAFFIC UNIT.

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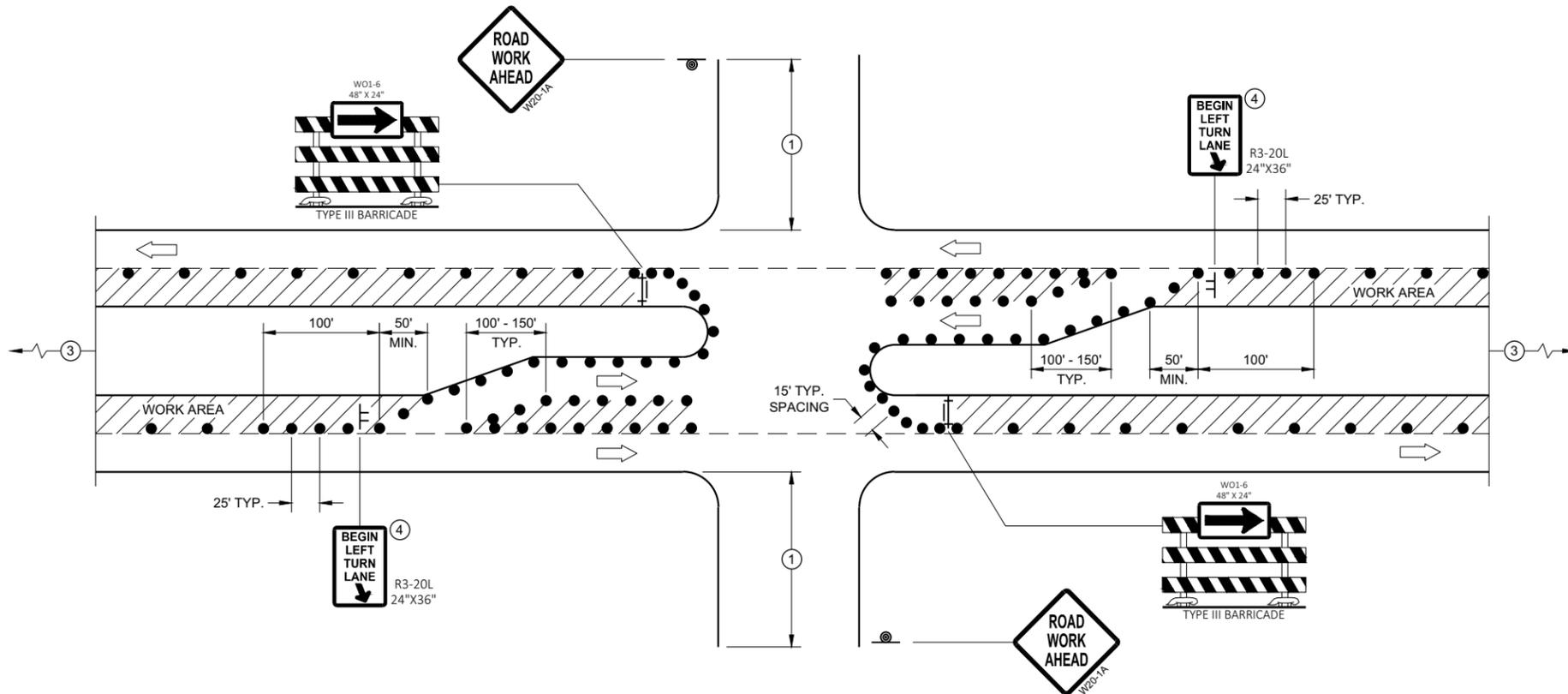
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350' IF 35 - 40 MPH.
200' IF 25 - 30 MPH.
- ② ALSO USE BARRICADE AND 15 FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS
- ③ SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.
- ④ MINIMUM MOUNTING HEIGHT OF 5 FEET FROM EDGE OF PAVEMENT (AT EDGE LINE LOCATION) TO BOTTOM OF SIGN.

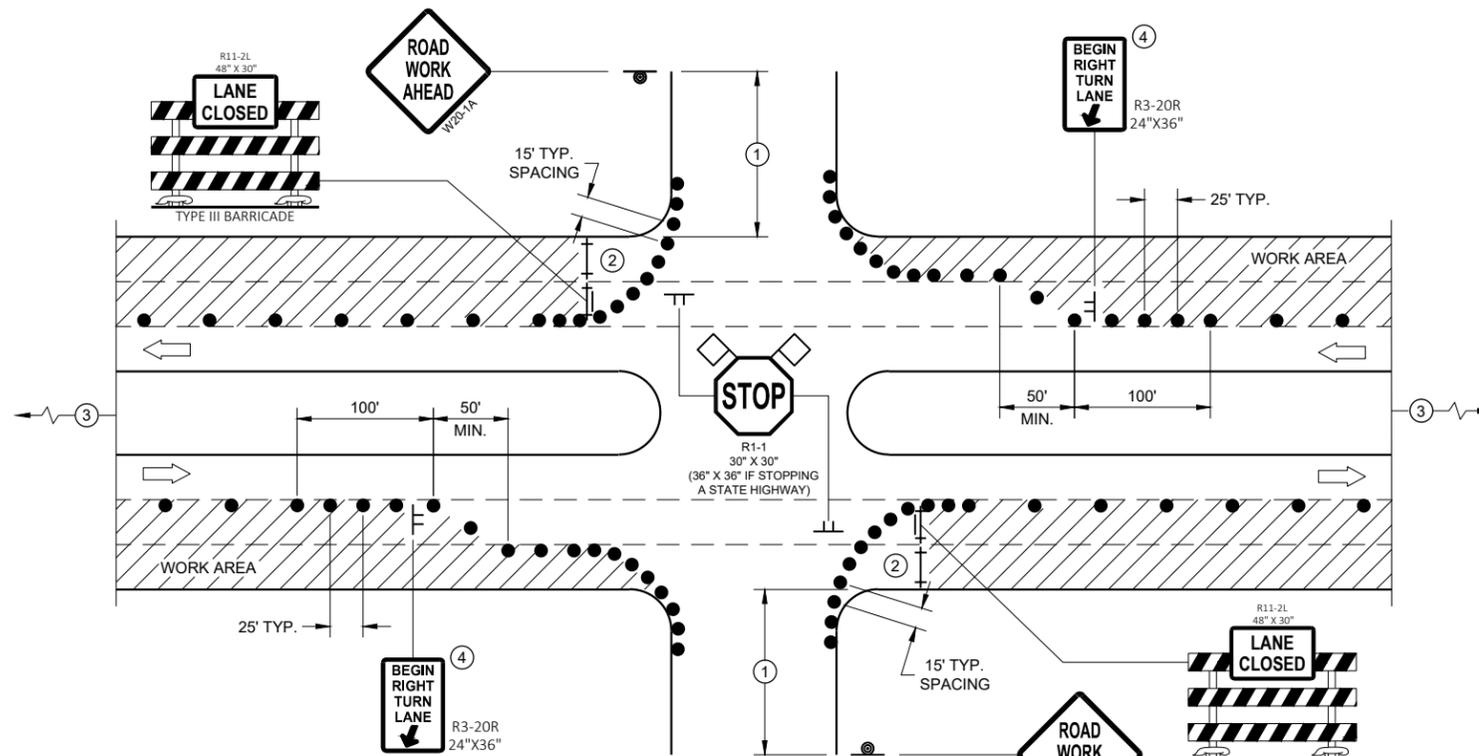


FOR LEFT LANE CLOSURE AT INTERSECTION OR MEDIAN OPENING (WITH LEFT TURN BAY OPEN)

LEGEND

- SIGN ON TEMPORARY SUPPORT
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- DIRECTION OF TRAFFIC
- FLAGS, 16" X 16" MIN., ORANGE
- WORK AREA

TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LEFT LANE CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED August 2020 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



PROVIDE TURN LANES AT INTERSECTIONS WHENEVER STAGING OF WORK ALLOWS. TAPER AND TURN LANE LENGTHS BASED ON FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

FOR RIGHT TWO LANES CLOSED AT INTERSECTION

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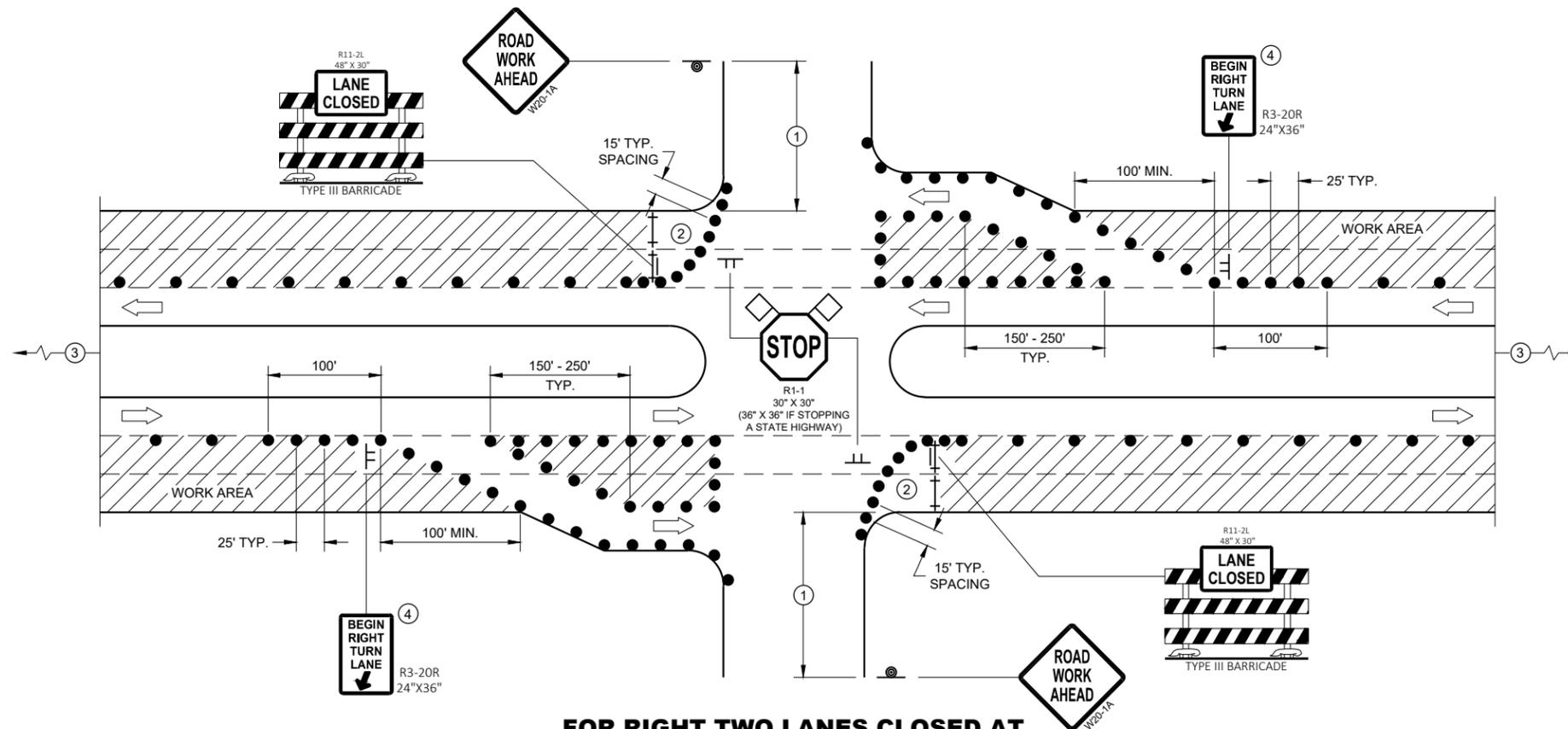
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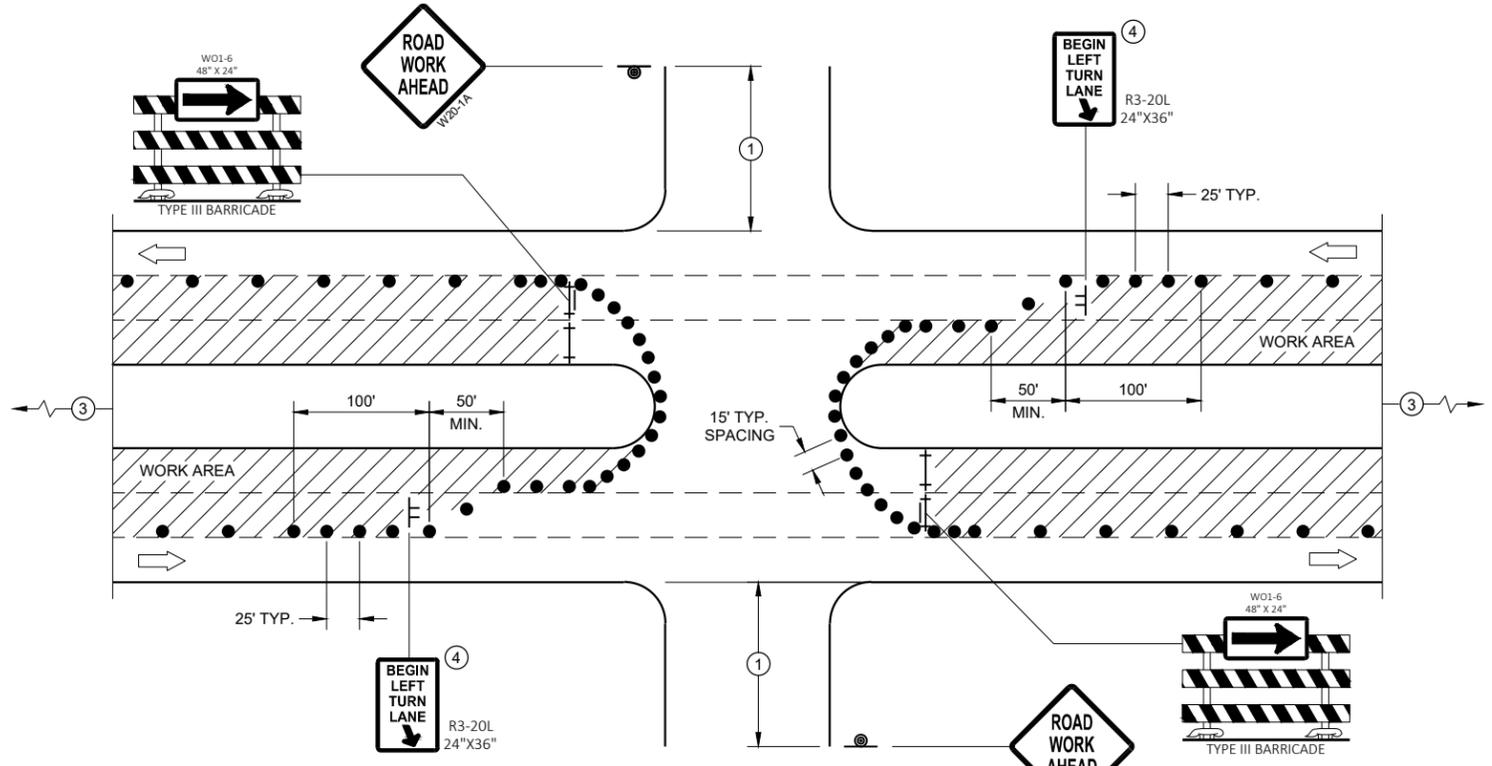
FOR RIGHT TWO LANES CLOSED AT INTERSECTION (WITH RIGHT TURN BAY OPEN)

LEGEND

- SIGN ON TEMPORARY SUPPORT
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- DIRECTION OF TRAFFIC
- FLAGS, 16" X 16" MIN., ORANGE
- WORK AREA

**TRAFFIC CONTROL,
INTERSECTION WITHIN
TWO RIGHT LANE CLOSURE**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

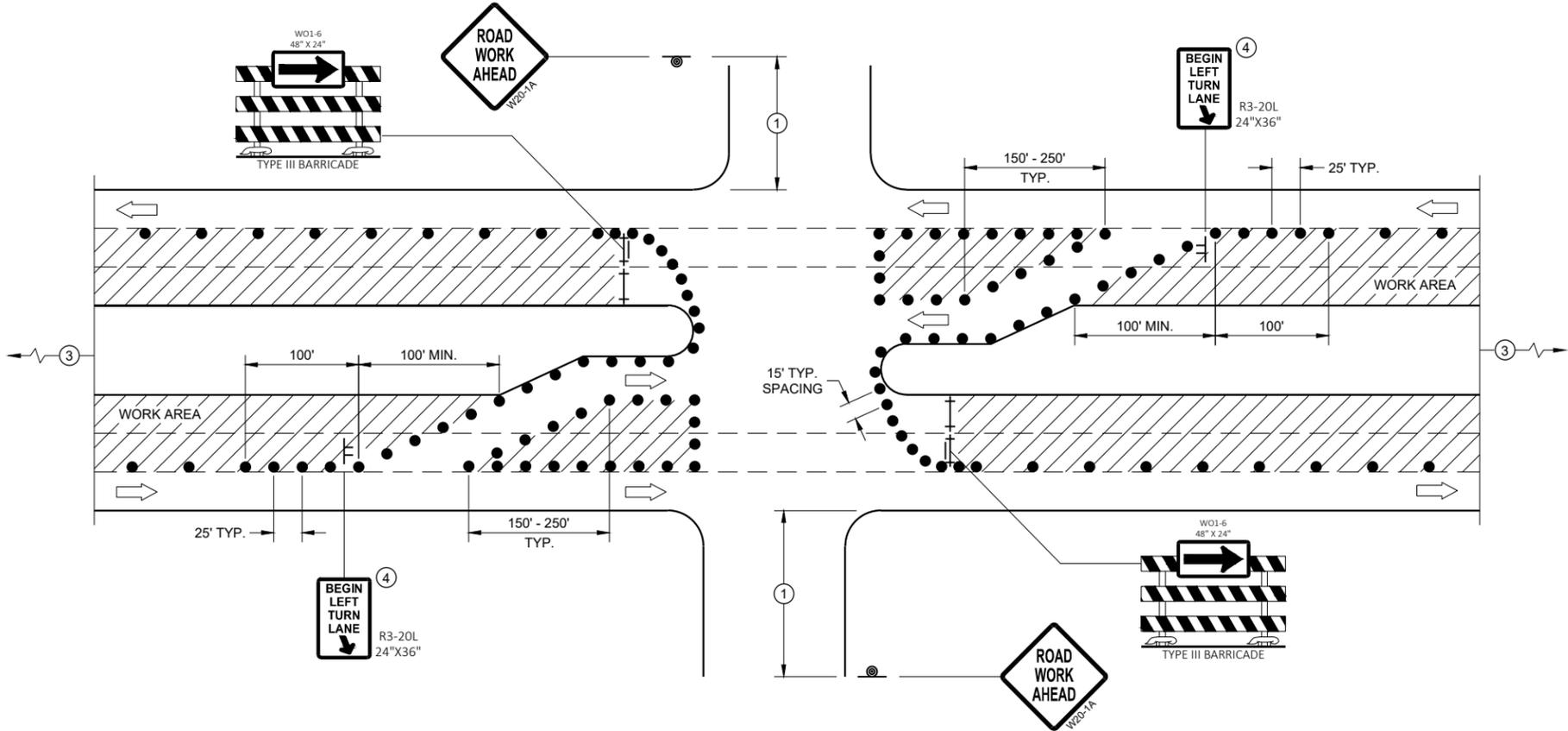


FOR LEFT TWO LANES CLOSED AT INTERSECTION OR MEDIAN BREAK

PROVIDE TURN LANES AT INTERSECTIONS WHENEVER STAGING OF WORK ALLOWS. TAPER AND TURN LANE LENGTHS BASED ON FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

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 - CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.
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350' IF 35 - 40 MPH.
200' IF 25 - 30 MPH.
 - 2 ALSO USE BARRICADE AND 15 FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS
 - 3 SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.
 - 4 MINIMUM MOUNTING HEIGHT OF 5 FEET FROM EDGE OF PAVEMENT (AT EDGE LINE LOCATION) TO BOTTOM OF SIGN.



FOR LEFT TWO LANES CLOSED AT INTERSECTION OR MEDIAN BREAK (WITH LEFT TURN BAY OPEN)

LEGEND

- SIGN ON TEMPORARY SUPPORT
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- DIRECTION OF TRAFFIC
- FLAGS, 16" X 16" MIN., ORANGE
- WORK AREA

6

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SDD 15D23 - 07b

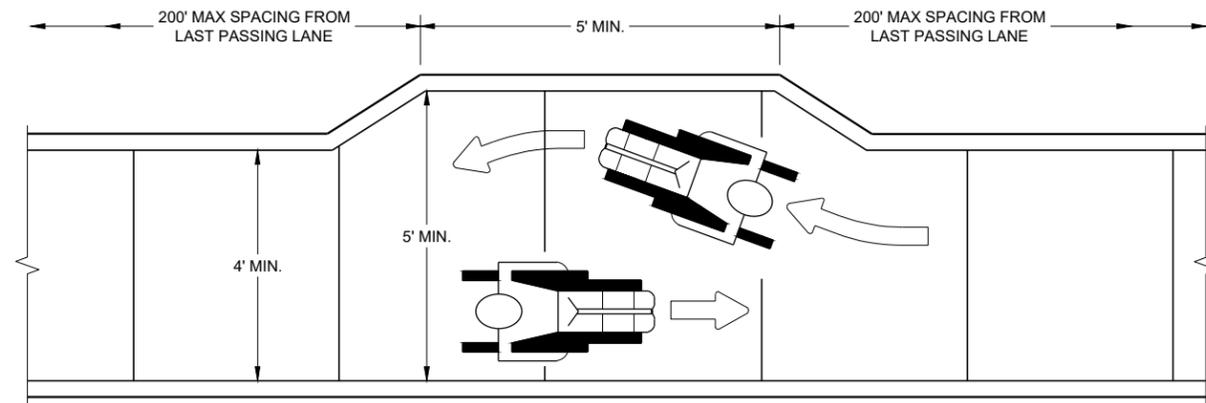
SDD 15D23 - 07b

**TRAFFIC CONTROL,
INTERSECTION WITHIN
TWO LEFT LANE CLOSURE**

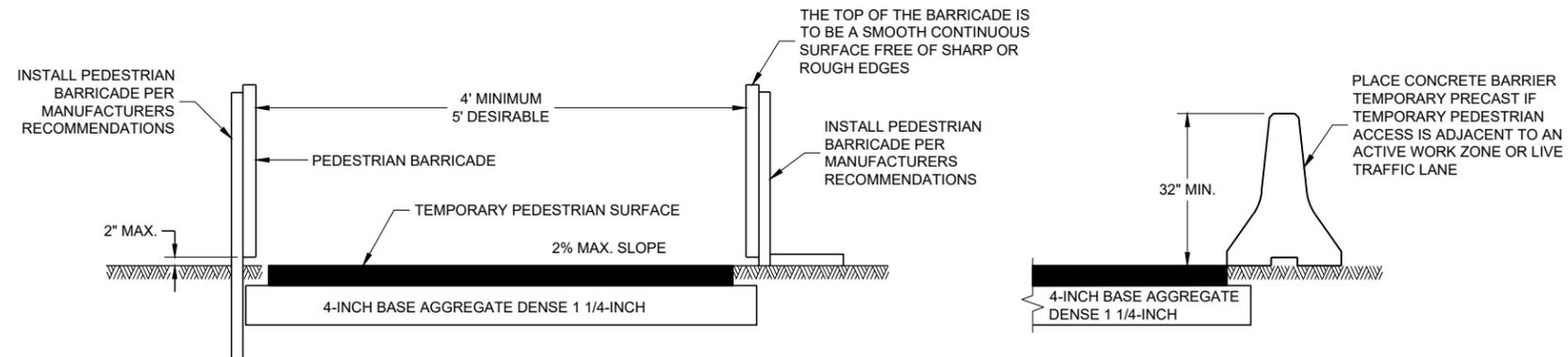
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
August 2021 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA



NARROW SIDEWALK PASSING DETAIL



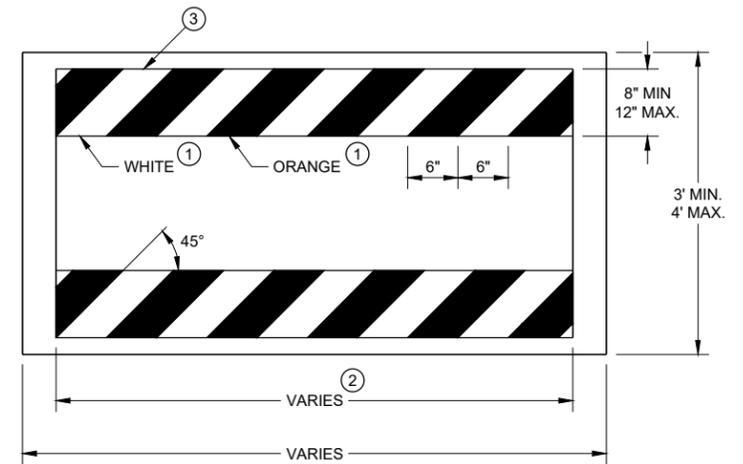
TEMPORARY PEDESTRIAN ACCESS

GENERAL NOTES

BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.

* USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



TEMPORARY PEDESTRIAN BARRICADE*

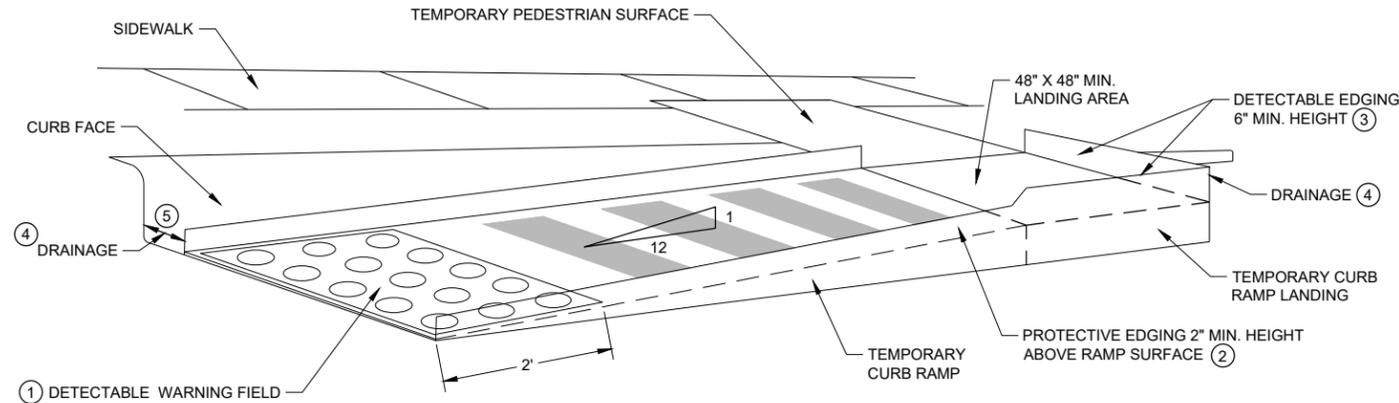
**TRAFFIC CONTROL,
PEDESTRIAN
ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

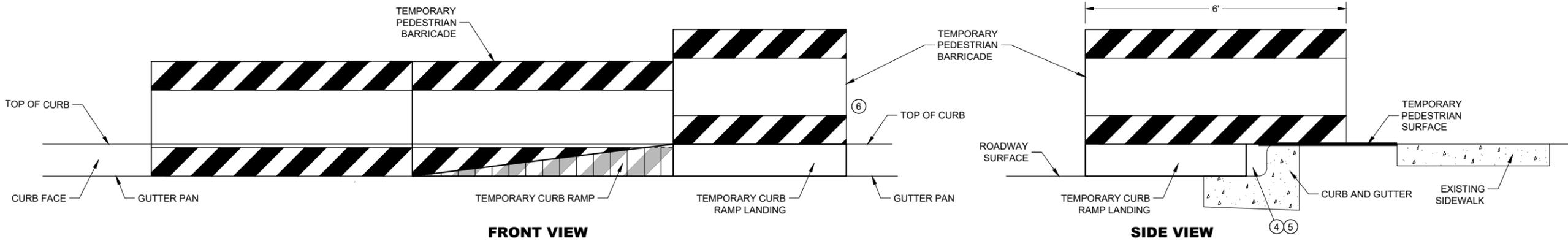
GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.
 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
 CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ 6" MINIMUM BETWEEN CURB FACE AND EDGE OF RAMP
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



PERSPECTIVE VIEW

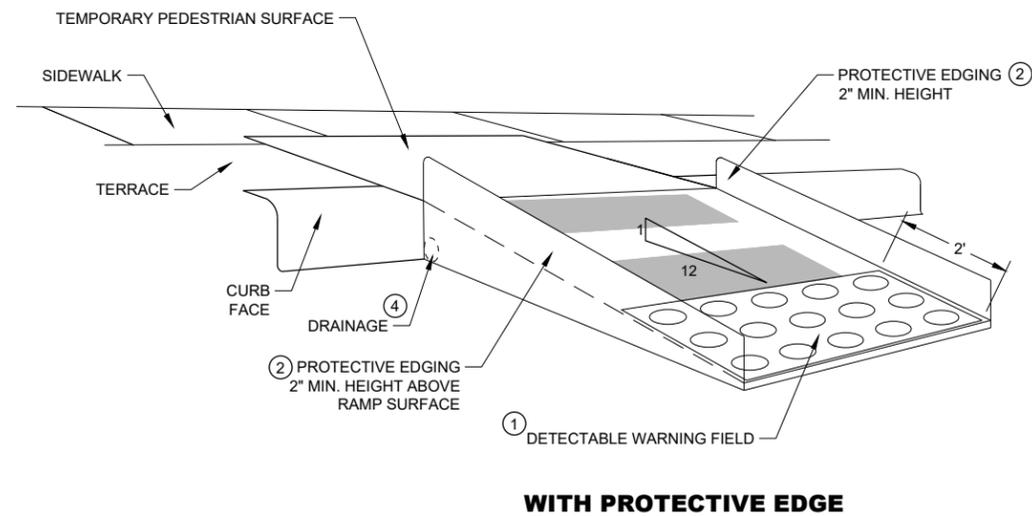
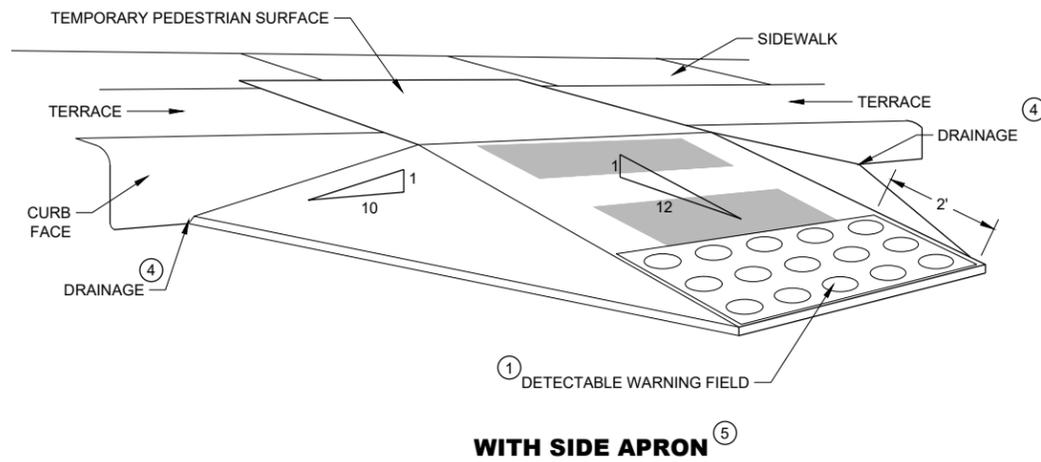


FRONT VIEW

SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

<p>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>



TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

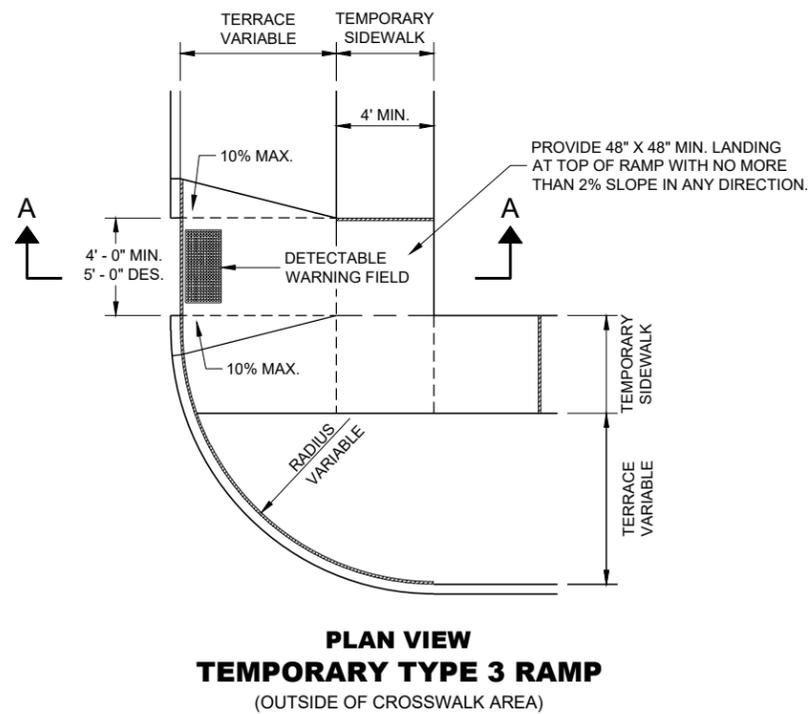
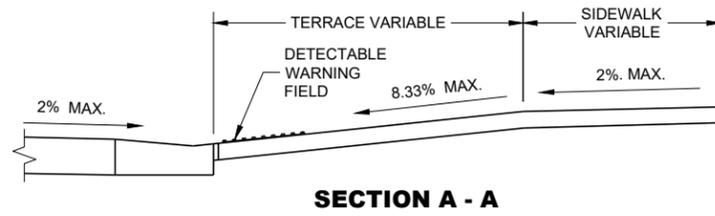
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- (3) DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- (4) DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- (5) CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
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- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- ★ USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



6

6

SDD 15D30 - 07d

SDD 15D30 - 07d

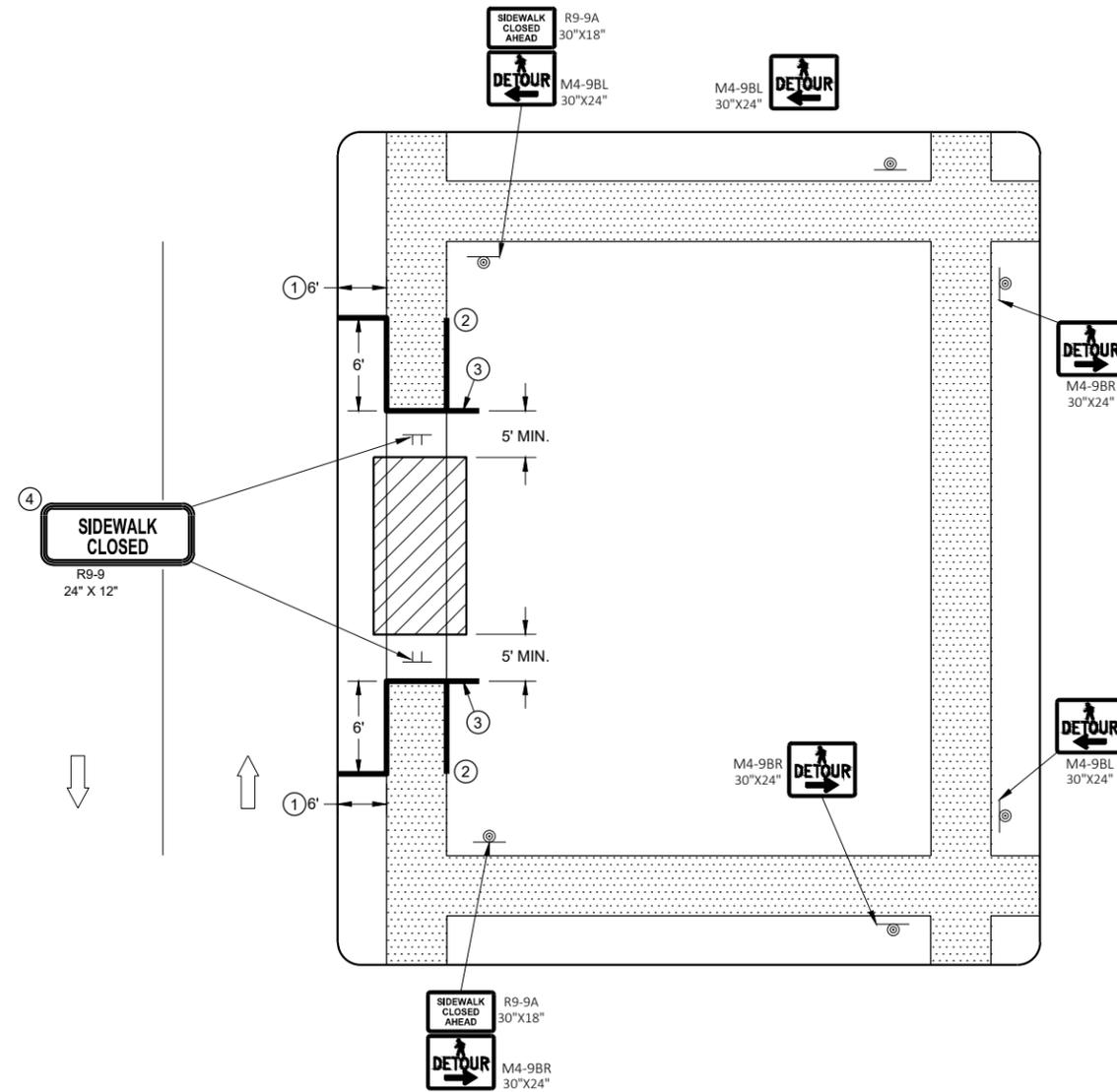
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
<small>FHWA</small>	

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
 - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
 - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

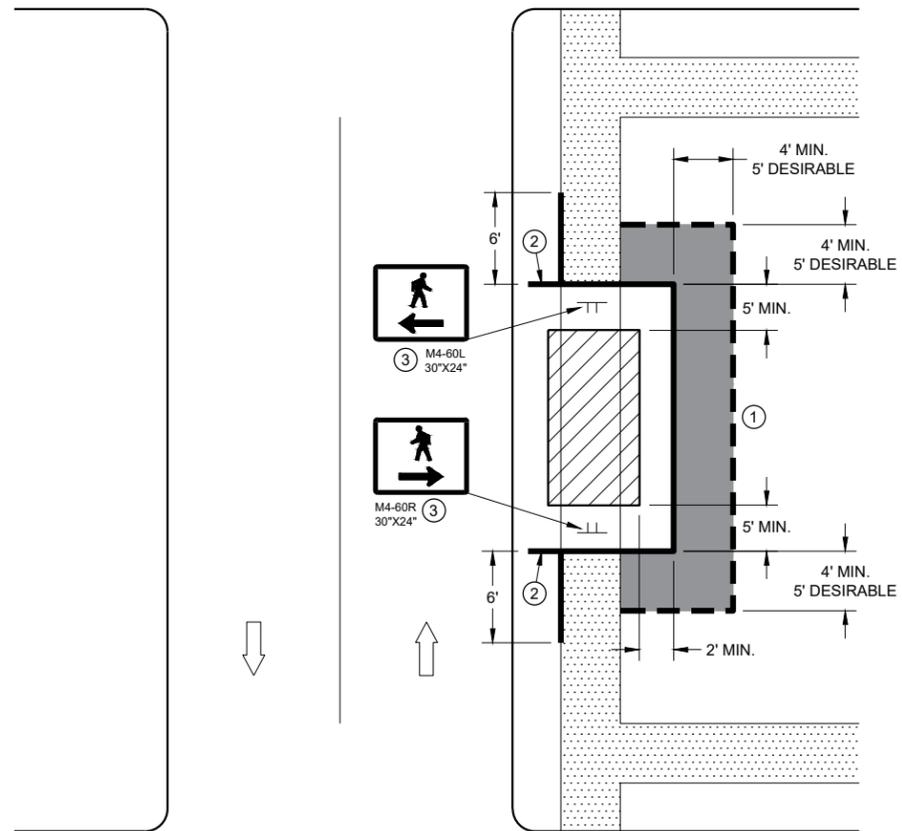
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY PEDESTRIAN SURFACE
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ① USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ② IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



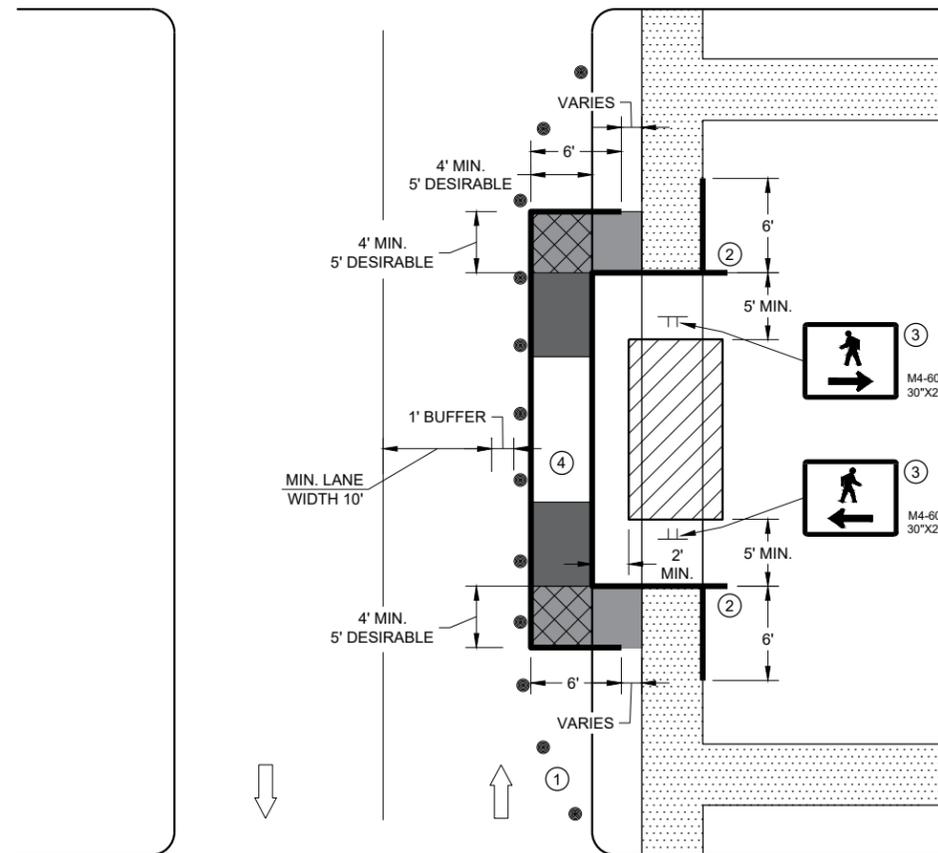
**SIDEWALK DIVERSION
SINGLE SIDE**

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
 - ② PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
 - ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
 - ④ USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURFACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE.



SIDEWALK DIVERSION, SINGLE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

6

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SDD 15D30 - 07h

SDD 15D30 - 07h

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

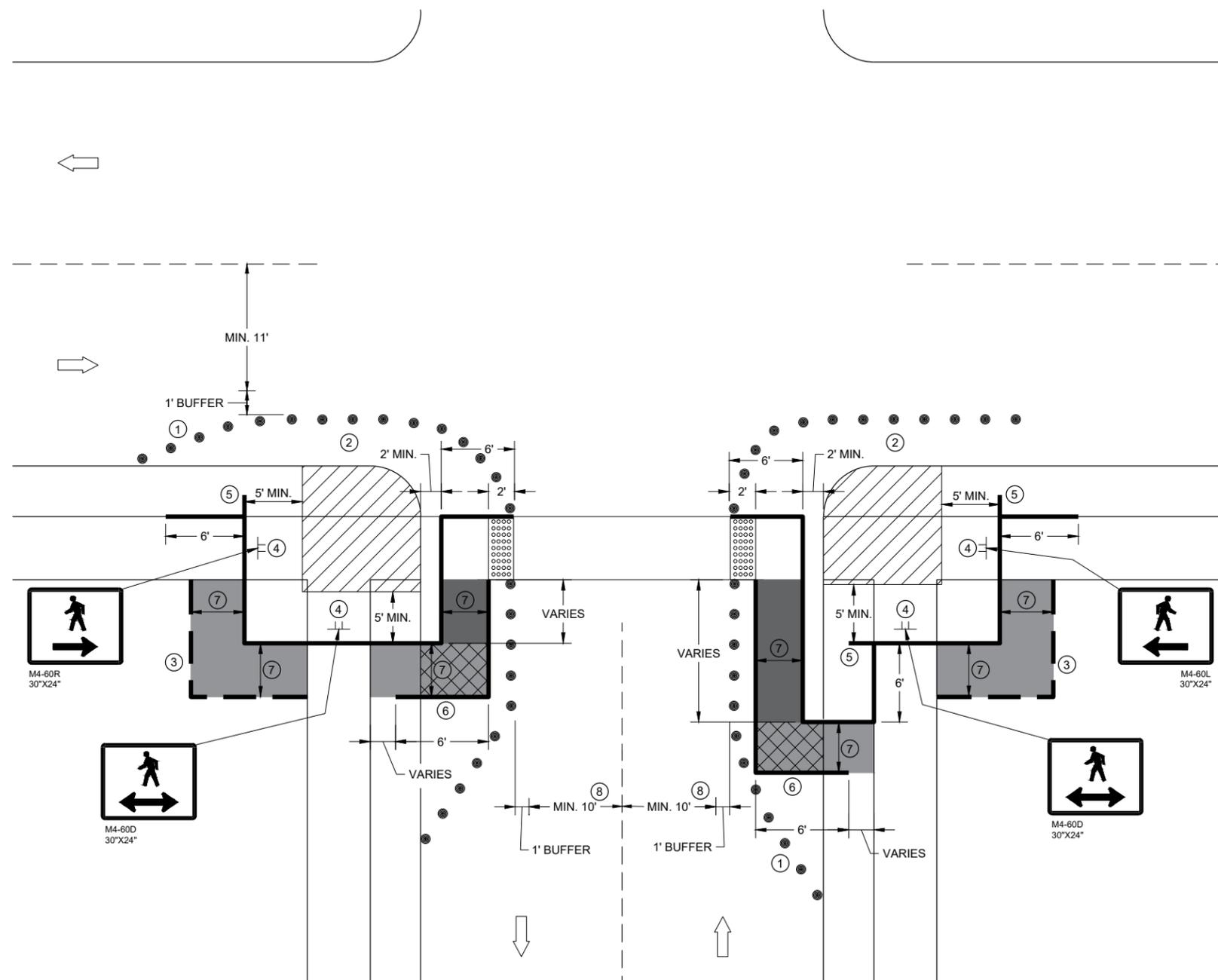
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑦ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑧ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



**CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE**

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

6

6

SDD 15D30 - 07i

SDD 15D30 - 07i

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

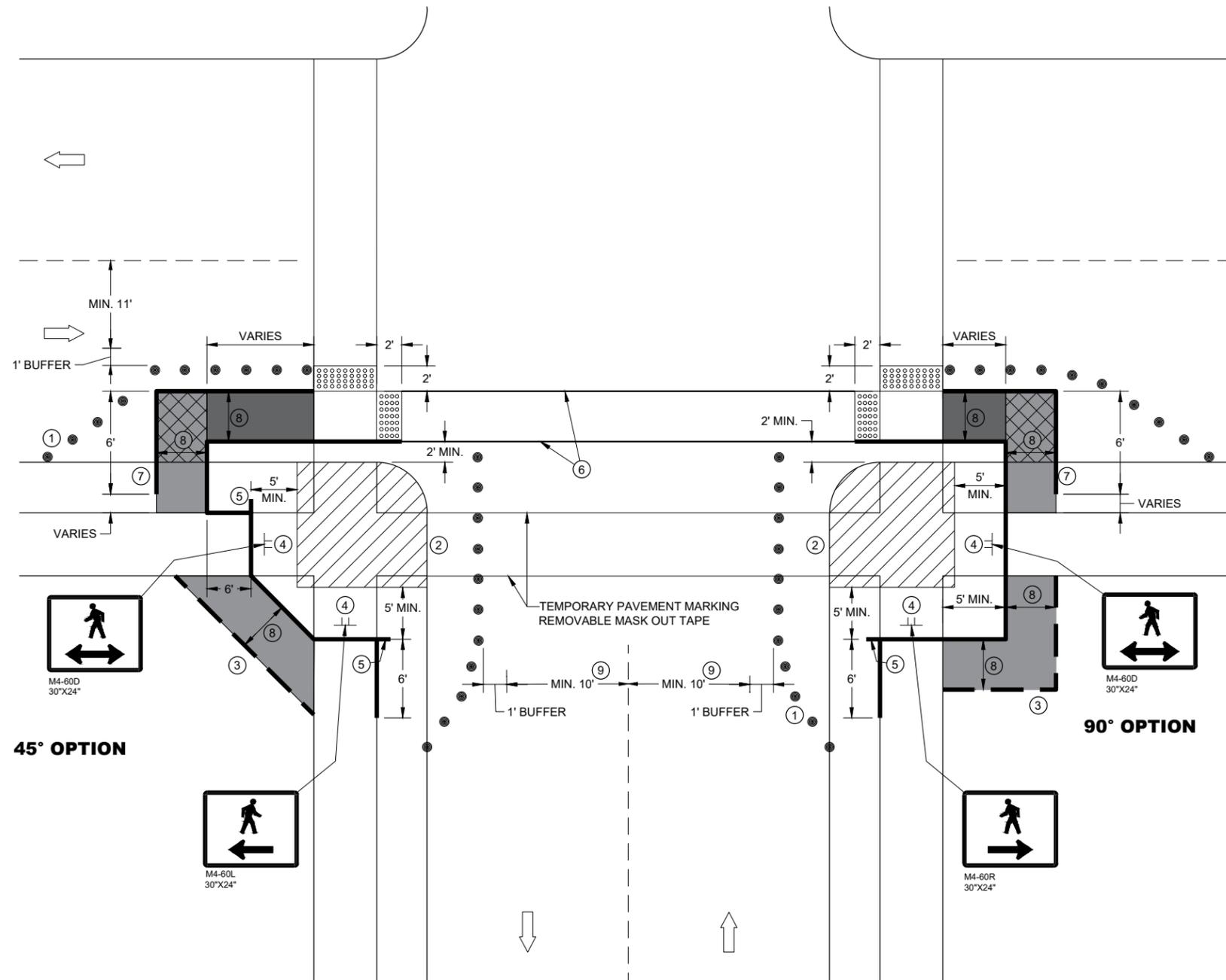
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ WHITE 6" TEMPORARY PAVEMENT MARKING
- ⑦ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑧ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑨ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



CURB RAMP PEDESTRIAN TRAFFIC CONTROL

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  TYPE III BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE "A" WARNING LIGHT (FLASHING)
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
-  CONCRETE BARRIER TEMPORARY PRECAST

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR LANE SHIFT LEFT - REVERSE FOR SHIFTING RIGHT.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON ANY "COVERED" OR "DOWNED" SIGNS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINES IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

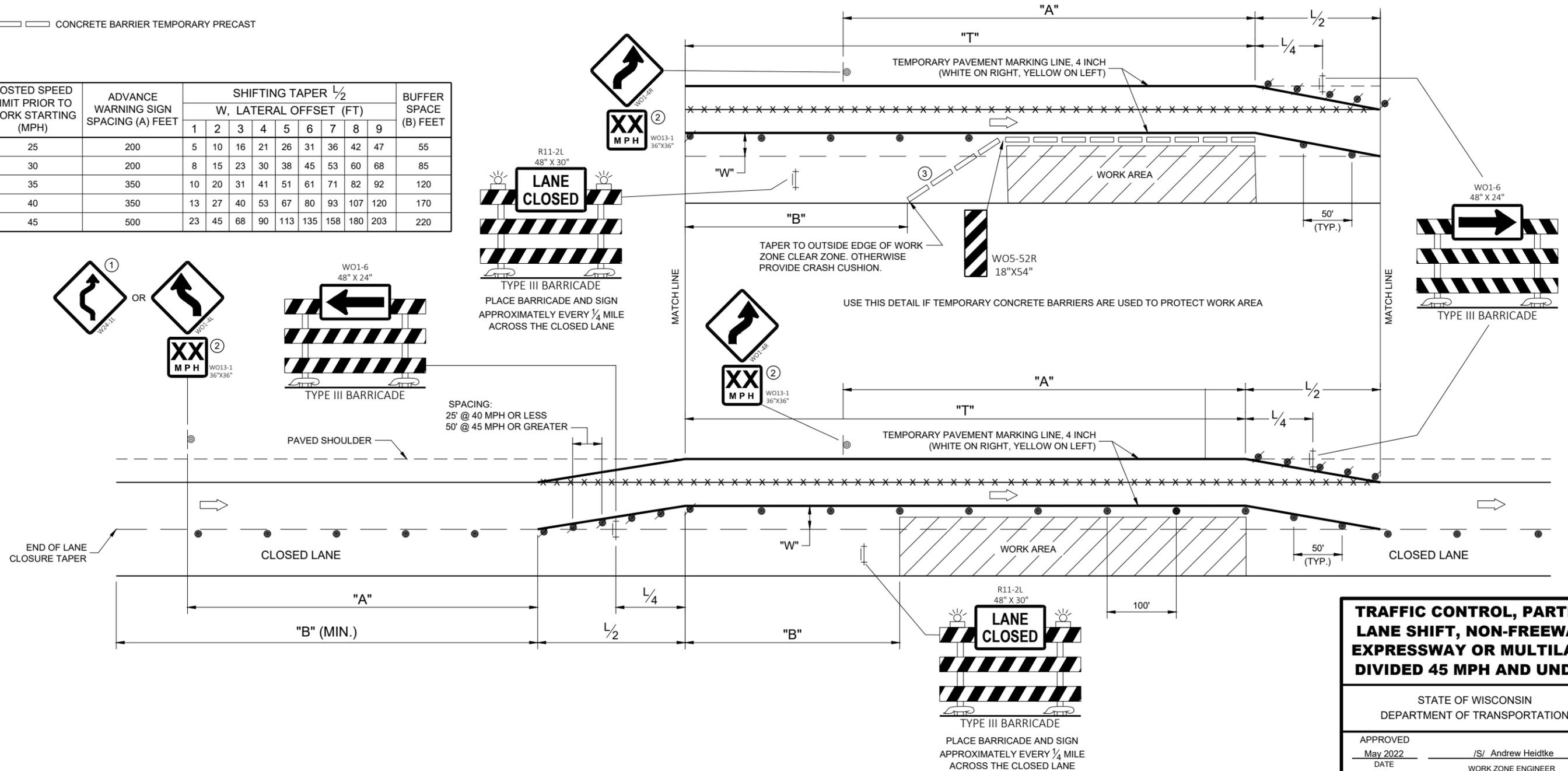
WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE SHIFT OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE SHIFT MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

- ① USE ONLY WHEN T<600', OMIT WO1-4R.
- ② IF NEEDED, USE ONLY IF DESIGN SPEED IS 10 MPH BELOW POSTED SPEED.
- ③ BARRIER FLARE RATE: 6:1 @ 40 MPH OR LESS
8:1 @ 45 MPH OR GREATER

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHIFTING TAPER 1/2									BUFFER SPACE (B) FEET
		W, LATERAL OFFSET (FT)									
		1	2	3	4	5	6	7	8	9	
25	200	5	10	16	21	26	31	36	42	47	55
30	200	8	15	23	30	38	45	53	60	68	85
35	350	10	20	31	41	51	61	71	82	92	120
40	350	13	27	40	53	67	80	93	107	120	170
45	500	23	45	68	90	113	135	158	180	203	220



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SDD 15D40 - 04c

SDD 15D40 - 04c

TRAFFIC CONTROL, PARTIAL LANE SHIFT, NON-FREEWAY/ EXPRESSWAY OR MULTILANE DIVIDED 45 MPH AND UNDER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

-  TYPE III BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE "A" WARNING LIGHT (FLASHING)
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
-  CONCRETE BARRIER TEMPORARY PRECAST

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR LANE SHIFT LEFT - REVERSE FOR SHIFTING RIGHT.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON ANY "COVERED" OR "DOWNED" SIGNS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINES IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

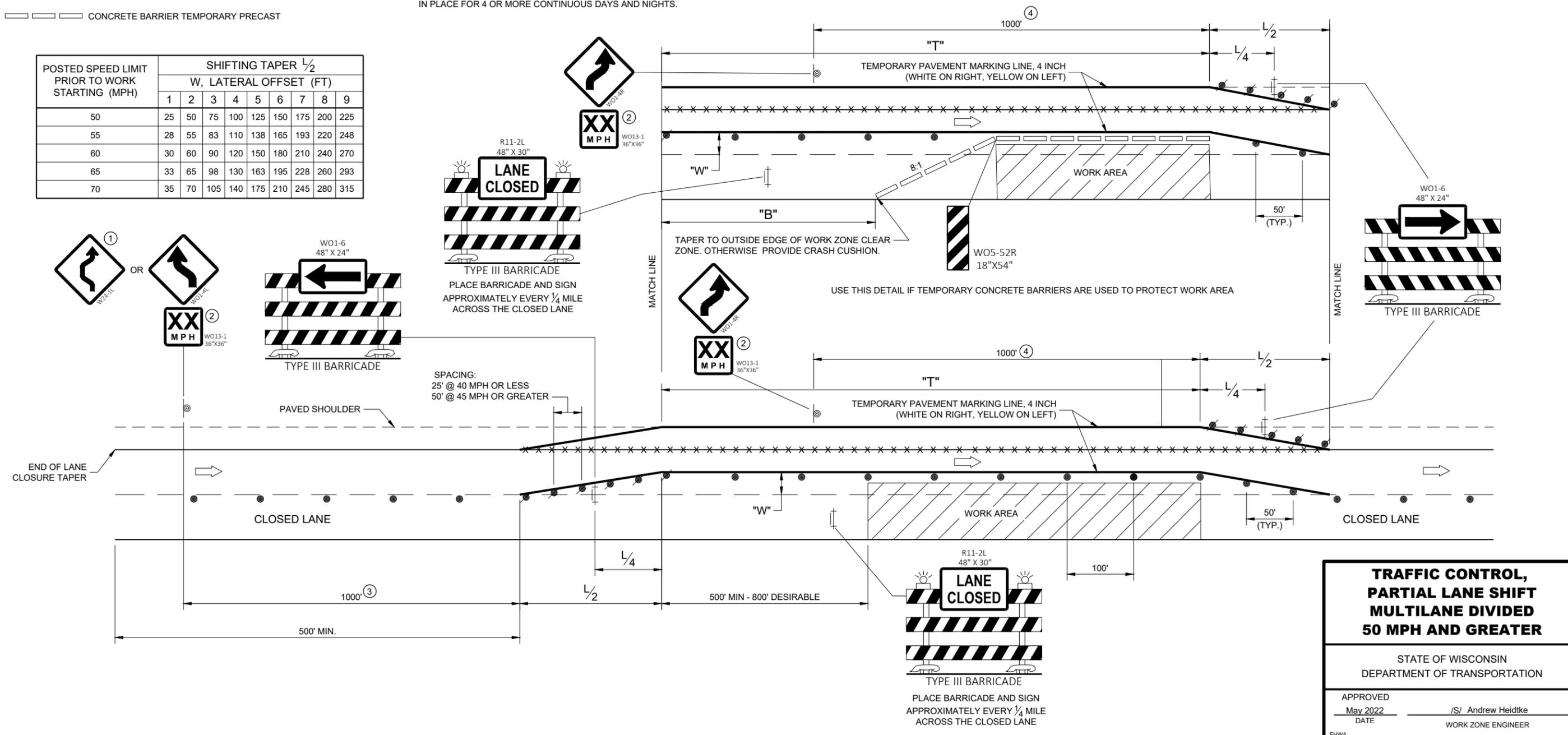
WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE SHIFT OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE SHIFT MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

- ① USE ONLY WHEN T<600', OMIT WO1-4R.
- ② IF NEEDED, USE ONLY IF DESIGN SPEED IS 10 MPH BELOW POSTED SPEED.
- ③ IF THE BEGINNING OF LANE SHIFT TAPER IS 1200 FEET OR LESS FROM END OF LANE CLOSURE TAPER, PLACE THE WO1-4L SIGN 200 FEET AFTER THE END OF THE LANE CLOSURE TAPER.
- ④ IF THE BEGINNING OF THE SECOND LANE SHIFT TAPER IS 1200 FEET OR LESS FROM END OF THE FIRST LANE CLOSURE TAPER, PLACE THE WO1-4L SIGN 200 FEET AFTER THE END OF THE FIRST LANE CLOSURE TAPER.

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	SHIFTING TAPER 1/2 W, LATERAL OFFSET (FT)								
	1	2	3	4	5	6	7	8	9
50	25	50	75	100	125	150	175	200	225
55	28	55	83	110	138	165	193	220	248
60	30	60	90	120	150	180	210	240	270
65	33	65	98	130	163	195	228	260	293
70	35	70	105	140	175	210	245	280	315



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SDD 15D40 - 04d

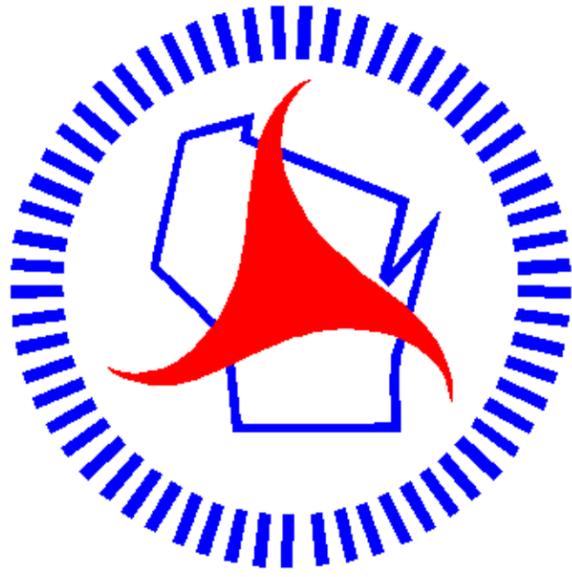
SDD 15D40 - 04d

**TRAFFIC CONTROL,
PARTIAL LANE SHIFT
MULTILANE DIVIDED
50 MPH AND GREATER**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED _____
DATE May 2022 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA



Wisconsin Department of Transportation

Dedicated people creating transportation solutions through innovation and exceptional service.

<http://www.dot.wisconsin.gov>