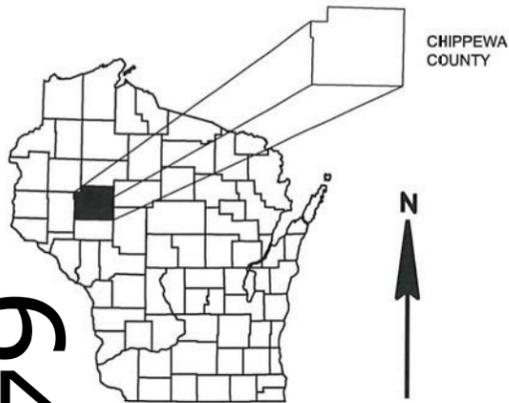


ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plat
Section No. 5	Plan and Profile
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 16

64



# STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

## PLAN OF PROPOSED IMPROVEMENT

### V LAKE HALLIE, 120TH STREET

30TH AVENUE/119TH STREET TO 35TH AVENUE

### CHIPPEWA COUNTY

STATE PROJECT NUMBER
8996-01-13

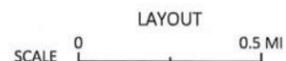
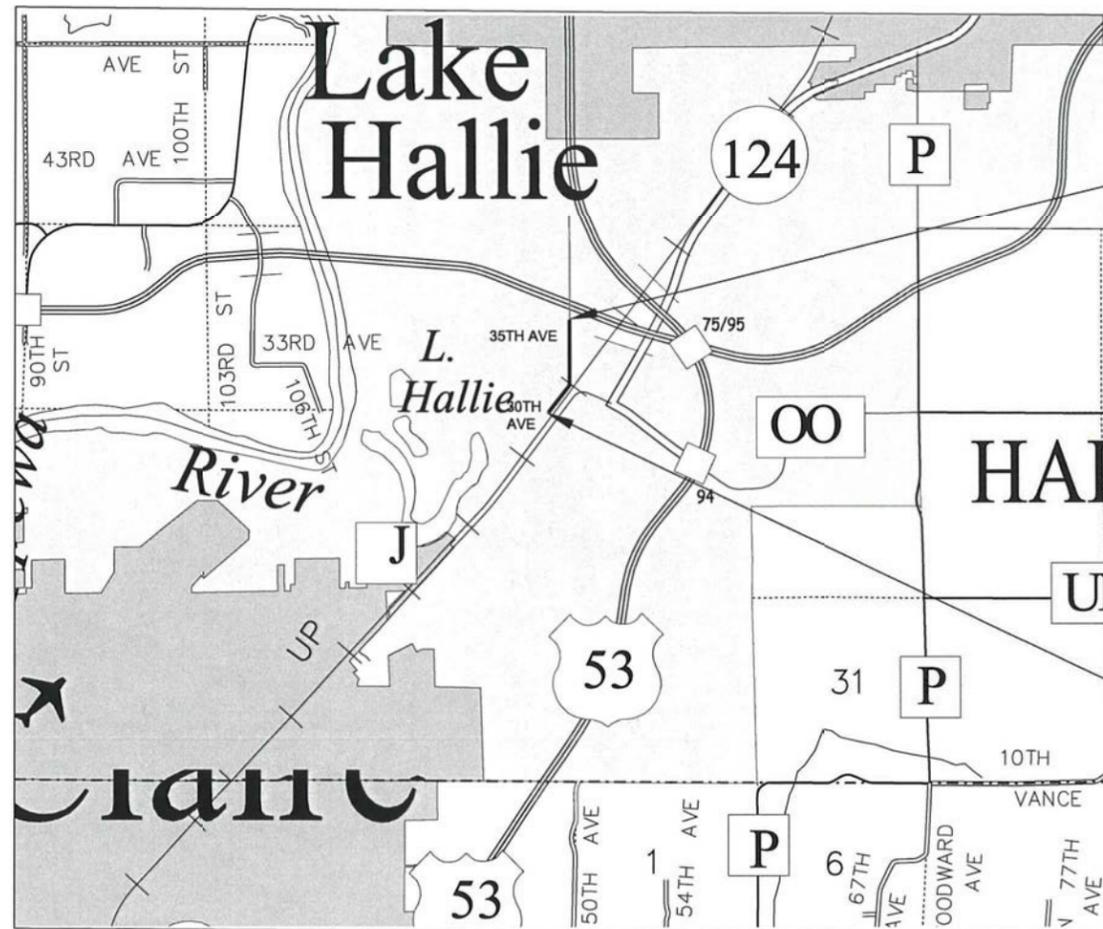
STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
8996-01-13	WISC 2023061	1

DESIGN DESIGNATION

A.A.D.T.	=	890
A.A.D.T.	=	N/A
D.H.V.	=	N/A
D.D.	=	N/A
T.	=	N/A
DESIGN SPEED	=	35 MPH
ESALS	=	73,000

CONVENTIONAL SYMBOLS

<b>PLAN</b>	<b>PROFILE</b>
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	<b>UTILITIES</b>
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
MARSH AREA	STORM SEWER
WOODED OR SHRUB AREA	TELEPHONE
	WATER
	UTILITY PEDESTAL
	POWER POLE
	TELEPHONE POLE



TOTAL NET LENGTH OF CENTERLINE = 0.532 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), CHIPPEWA COUNTY, NAD83 ( 1983 ), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 ( 1929 ). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

END PROJECT  
STA 132+00.00

BEGIN PROJECT  
STA 103+93.00  
Y = 111514.70  
X = 161949.31

ACCEPTED FOR

VILLAGE OF LAKE HALLIE

Date: 07/29/22  
*[Signature]*  
(Signature and Title of Official)

VILLAGE PRESIDENT

ORIGINAL PLANS PREPARED BY

DATE: \_\_\_\_\_  
(Professional Engineer Signature)

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	N/A
Designer	AYRES ASSOCIATES INC
Project Manager	MATTHEW THORSEN
Regional Examiner	TOU YANG
Regional Supervisor	TYLER RONGSTAD

APPROVED FOR THE DEPARTMENT

DATE: 8/1/2022  
*[Signature]*  
(Signature)

E

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLAN ARE APPROXIMATE. THERE MAY BE OTHER INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY UTILITY WHICH IS NOT A MEMBER OF THE DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.

NO TREES OR SHRUBS SHALL BE REMOVED WITHOUT APPROVAL BY THE ENGINEER.

THE EROSION CONTROL ITEMS SHOWN ON THE PLANS ARE AT SUGGESTED LOCATIONS. THE ENGINEER MAY MODIFY LOCATIONS AS NEEDED. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL SUCH TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NECESSARY.

TEMPORARY STORAGE OF ANY EXCAVATED MATERIAL WILL NOT BE PERMITTED IN WETLANDS, FLOODWAY OR FLOODPLAIN OF ANY WETLANDS.

TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

DETAILS OF CONSTRUCTION NOT SHOWN SHALL BE IN ACCORDANCE WITH THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS.

A SAWED JOINT WILL BE REQUIRED WHERE NEW PAVEMENT IS TO MEET AN EXISTING PAVED SURFACE.

INTERSECTION RADII TO MATCH EXISTING. RADII SHOWN ON PLAN IS FOR INFORMATION ONLY. ALL RADII ARE MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE SHOWN OR NOTED ON THE PLAN.

THE EXACT LOCATION AND WIDTH OF DRIVEWAY ENTRANCES WILL BE DETERMINED BY THE FIELD ENGINEER. ALL DRIVEWAYS ARE TO BE REPLACED IN KIND UNLESS OTHERWISE DIRECTED BY THE ENGINEER OR AS SHOWN ON THE PLANS.

EXISTING ELEVATIONS SHALL BE VERIFIED IN THE FIELD.

PROPOSED ROADWAY IMPROVEMENTS SHALL MATCH EXISTING SUPERELEVATIONS AND ROADWAY CROSS SLOPES IN MILLED AREAS.

UTILITY CONTACTS

AT & T  
304 S DEWEY ST  
EAU CLAIRE, WI 54701  
ATTN: RICK PODOLAK  
715-410-0656  
RP4514@ATT.COM

SPECTRUM  
1201 MCCANN DRIVE  
ALTOONA, WI 54720  
ATTN: SUNNY RICHARDSON  
715-896-6503  
SUNNY.RICHARDSON@CHARTER.COM

XCEL ENERGY (NATURAL GAS)  
1400 WESTERN AVE  
EAU CLAIRE, WI 54702  
ATTN: MELISSA GOETTL  
715-271-6883  
MELISSA.A.EWINGS@XCELENERGY.COM

\*DENOTES NOT A MEMBER OF DIGGERS HOTLINE



VILLAGE OF LAKE HALLIE  
ATTN: DEREK SCHAD  
PUBLIC WORKS SUPERVISOR  
OFFICE: 715.726.2660  
CELL: 715.559.7019  
DSCHAD@LAKEHALLIE.US

XCEL ENERGY (ELECTRIC)  
320 HELLER ROAD  
MENOMONIE, WI 54751  
ATTN: TRAVIS WERLEIN  
CELL: 715-495-3441  
TRAVIS.A.WERLEIN@XCELENERGY.COM

DNR CONTACT

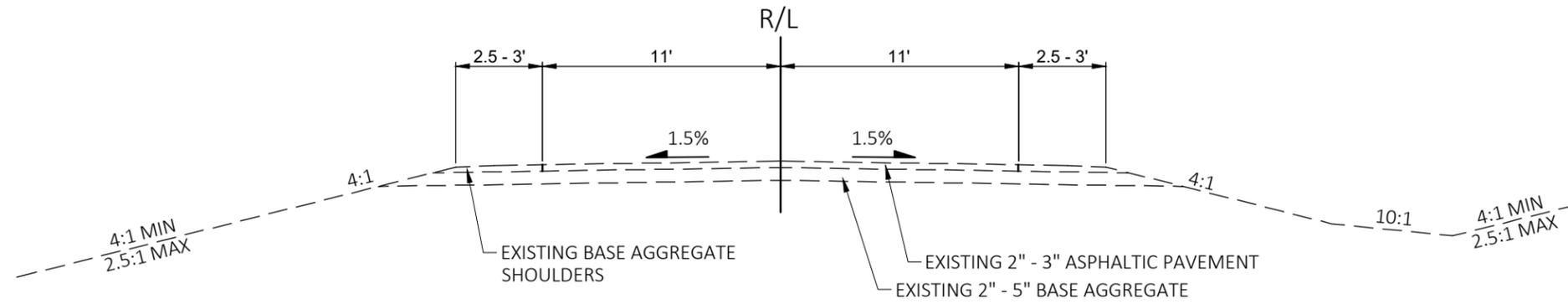
DNR NORTHERN REGION HQ  
1300 WEST CLAIREMONT AVENUE  
EAU CLAIRE, WI 54701  
ATTN: LEAH NICOL  
715-934-9014  
LEAH.NICOL@WISCONSIN.GOV

DESIGN CONTACT

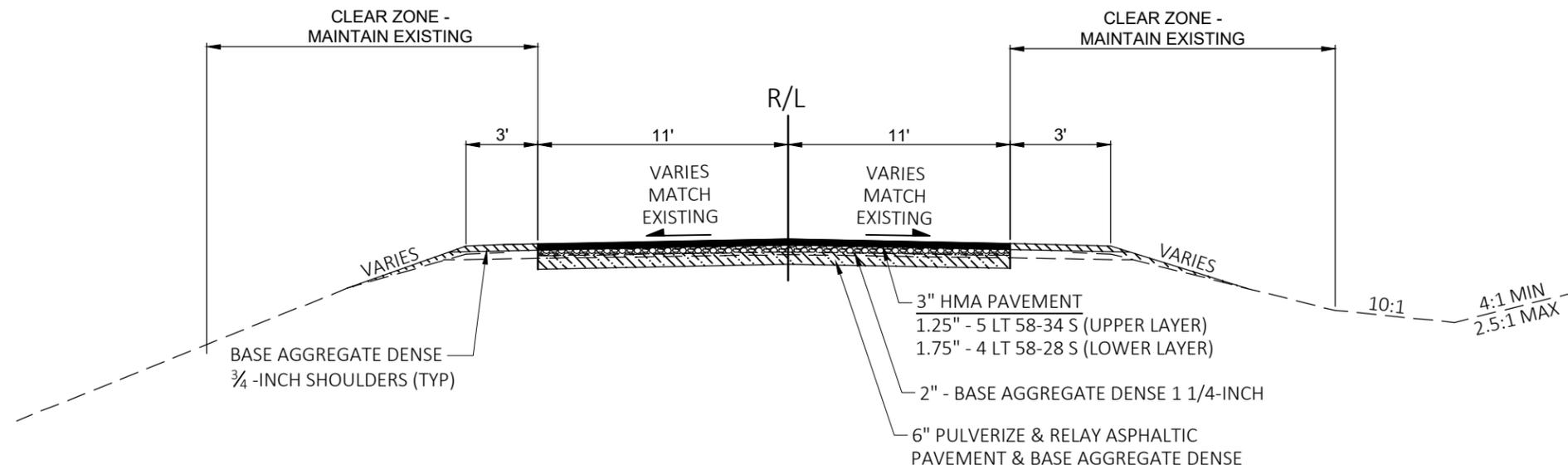
AYRES ASSOCIATES INC  
3433 OAKWOOD HILLS PARKWAY  
EAU CLAIRE, WI 54701  
ATTN: BRIAN LAMBERT  
715-834-3161  
LAMBERTB@AYRESASSOCIATES.COM

WISDOT DESIGN CONTACT

DEPARTMENT OF TRANSPORTATION  
718 W. CLAIREMONT AVE  
EAU CLAIRE, WI 54701  
ATTN: MATTHEW THORNSEN, P.E.  
715-225-4159  
MATTHEW.THORNSEN@DOT.WI.GOV

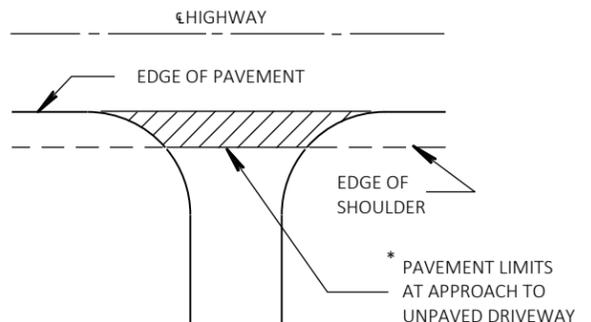


**EXISTING TYPICAL SECTION**



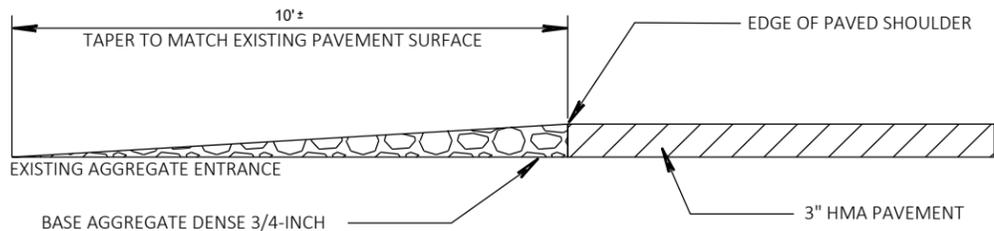
**FINISHED TYPICAL SECTION**

STA 103+90 - STA 132+00

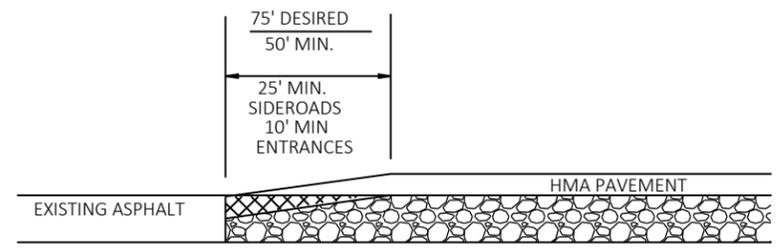


PLAN VIEW

RURAL DRIVEWAY INTERSECTION DETAIL



BASE AGGREGATE PRIVATE ENTRANCE DETAIL PROFILE VIEW

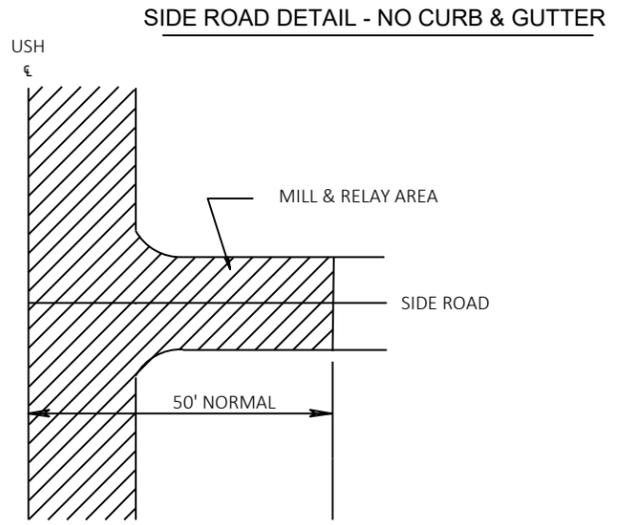


BUTT JOINT

PAID FOR AS REMOVING ASPHALTIC SURFACE BUTT JOINTS

REQUIRED AT BEGIN AND END PAVING LOCATIONS

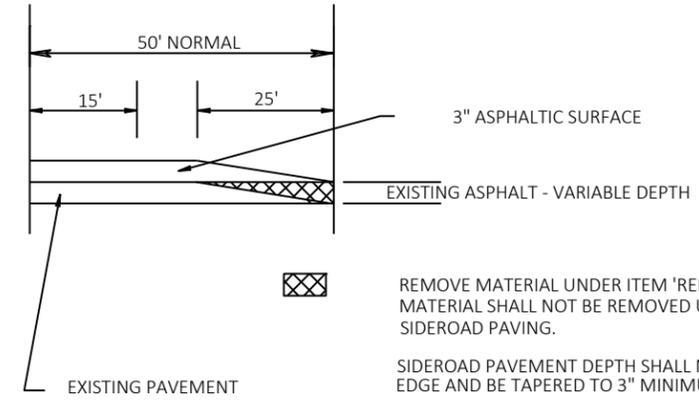
NOTE: ANY SAWCUT USED IN THIS OPERATION CONSIDERED INCIDENTAL TO THIS ITEM



SIDE ROAD DETAIL - NO CURB & GUTTER

NOTE: IF THE EXISTING SIDEROAD CONSISTS OF A BASE COURSE SURFACE, THE NEW ASPHALT SHALL BE PLACED TO THE ENDS OF THE EXISTING SIDEROAD RADIUS'

NOT TO SCALE



REMOVE MATERIAL UNDER ITEM 'REMOVING ASPHALT SURFACE , BUTT JOINTS' MATERIAL SHALL NOT BE REMOVED UNDER THIS ITEM UNTIL 24 HOURS BEFORE SIDEROAD PAVING.

SIDEROAD PAVEMENT DEPTH SHALL MATCH AT MAINLINE PAVEMENT EDGE AND BE TAPERED TO 3\"/>

NOTE: ANY SAWCUT USED WILL BE CONSIDERED INCIDENTAL TO THE ITEM "REMOVING ASPHALTIC SURFACE, BUTT JOINTS."

ROAD CLOSED TO THRU TRAFFIC AT  
120TH ST AND 35TH AVE  
SEE SDD 15C2, BARRICADES AND SIGNS FOR MAINLINE CLOSURES

35TH AVE

ERICKSON ST

ROAD CLOSED TO THRU TRAFFIC AT  
120TH ST AND 30TH AVE  
SEE SDD 15C2, BARRICADES AND SIGNS FOR MAINLINE CLOSURES

30TH AVE

120TH ST

**LEGEND**

- ① SEE SDD (TYP), TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC
- ①S SIDEROAD APPROACH CONFIGURATION
- ② SEE SDD 15C12, TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

Estimate Of Quantities By Plan Sets

8996-01-13

Line	Item	Item Description	Unit	Total	Qty
0002	204.0100	Removing Concrete Pavement	SY	10.000	10.000
0004	204.0110	Removing Asphaltic Surface	SY	278.000	278.000
0006	204.0115	Removing Asphaltic Surface Butt Joints	SY	367.000	367.000
0008	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 8996-01-13	EACH	1.000	1.000
0012	213.0100	Finishing Roadway (project) 01. 8996-01-13	EACH	1.000	1.000
0016	305.0110	Base Aggregate Dense 3/4-Inch	TON	323.000	323.000
0018	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	798.000	798.000
0020	325.0100	Pulverize and Relay	SY	7,308.000	7,308.000
0022	416.0160	Concrete Driveway 6-Inch	SY	10.000	10.000
0024	455.0605	Tack Coat	GAL	439.000	439.000
0026	460.2000	Incentive Density HMA Pavement	DOL	810.000	810.000
0028	460.5224	HMA Pavement 4 LT 58-28 S	TON	725.000	725.000
0030	460.5245	HMA Pavement 5 LT 58-34 S	TON	570.000	570.000
0032	619.1000	Mobilization	EACH	0.350	0.350
0034	624.0100	Water	MGAL	22.500	22.500
0036	642.5001	Field Office Type B	EACH	0.500	0.500
0038	643.0420	Traffic Control Barricades Type III	DAY	100.000	100.000
0040	643.0705	Traffic Control Warning Lights Type A	DAY	160.000	160.000
0042	643.0900	Traffic Control Signs	DAY	130.000	130.000
0044	643.5000	Traffic Control	EACH	0.500	0.500
0046	646.1020	Marking Line Epoxy 4-Inch	LF	11,040.000	11,040.000
0048	648.0100	Locating No-Passing Zones	MI	0.530	0.530
0050	650.5000	Construction Staking Base	LF	2,807.000	2,807.000
0052	650.8000	Construction Staking Resurfacing Reference	LF	2,807.000	2,807.000
0054	650.9911	Construction Staking Supplemental Control (project) 01. 8996-01-13	EACH	1.000	1.000
0058	690.0150	Sawing Asphalt	LF	296.000	296.000
0060	690.0250	Sawing Concrete	LF	30.000	30.000
0062	740.0440	Incentive IRI Ride	DOL	2,129.000	2,129.000
0064	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	300.000	300.000
0066	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	300.000	300.000

3

3

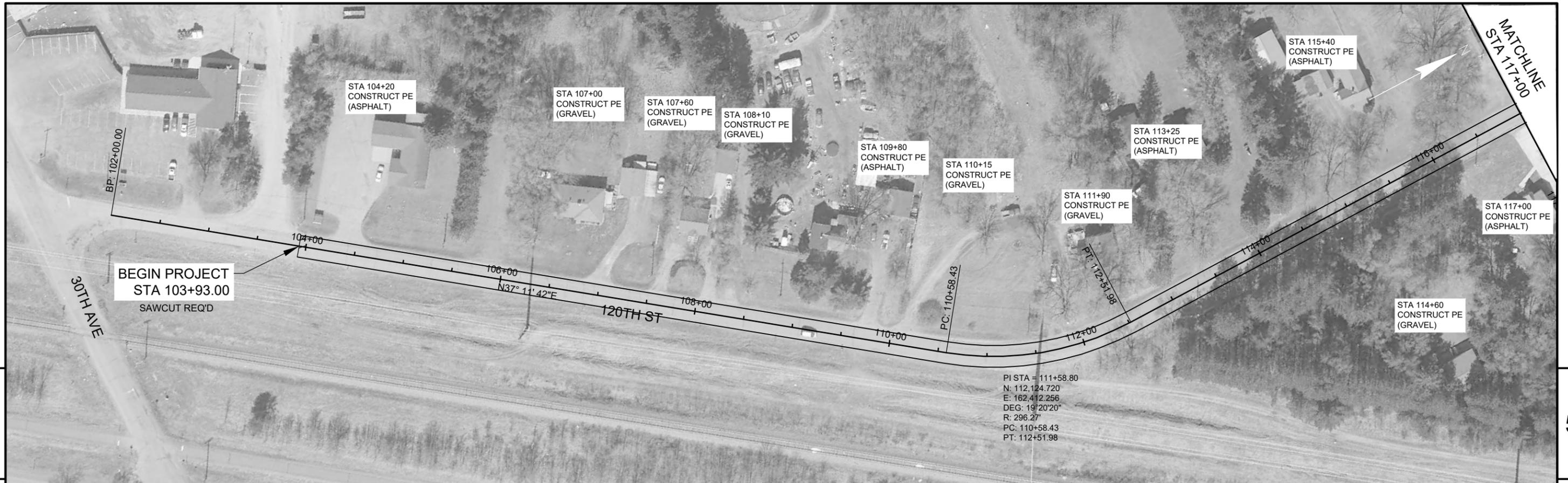
STATION	TO	STATION	LOCATION	204.0100 REMOVING CONCRETE PAVEMENT SY	204.0110 REMOVING ASPHALTIC SURFACE SY	STATION	TO	STATION	LOCATION	204.0115 REMOVING ASPHALTIC SURFACE BUTT JOINTS SY	STATION	TO	STATION	LOCATION	211.0101.01 PREPARE FOUNDATION FOR ASPHALTIC PAVING (01. 8995-01-13) EACH
103+93	-	132+00	ENTRANCES	10	278	103+90	-	132+00	PROJECT START	122	103+93	-	132+00	PROJECT	1
			TOTAL	10	278	103+90	-	132+00	PROJECT END	122				TOTAL	1
						118+75	-		33RD AVE	61					
						126+90	-		35TH AVE	61					
									TOTAL 0010	367					

STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	624.0100 WATER MGAL	STATION	TO	STATION	LOCATION	455.0605 TACK COAT GAL	460.5224 HMA PAVEMENT 4 LT 58-28 S TON	460.5245 HMA PAVEMENT 5 LT 58-34 S TON
103+93	-	132+00	120TH ST MAINLINE	312	762	21.5	103+90	-	132+00	1210TH ST MAINLINE	412	675	490
118+75	-		33RD AVE	6	18	0.5	118+75	-		33RD AVE	13	25	20
126+90	-		35TH AVE	6	18	0.5	126+90	-		35TH AVE	13	25	20
			TOTAL	323	798	22.5	103+90	-	132+00	DRIVEWAYS			40
									TOTAL	439	725	570	

STATION	TO	STATION	LOCATION	325.0100 PULVERIZE AND RELAY SY	STATION	TO	STATION	LOCATION	646.1020 MARKING LINE EPOXY 4-INCH LF	REMARKS	STATION	TO	STATION	LOCATION	642.5001 FIELD OFFICE TYPE B EACH
103+92	-	132+00	120TH ST MAINLINE	6,864	103+90	-	132+00	MAINLINE	5,620	CENTERLINE (YELLOW)				PROJECT (8996-01-13)	0.5
118+75	-	118+75	33RD AVE	222	103+90	-	132+00	MAINLINE	5,420	EDGE LINE (WHITE)				TOTAL	0.5
126+90	-	126+90	35TH AVE	222				TOTAL	11,040						
			TOTAL	7,308											

STATION	LOCATION	416.0160 CONCRETE DRIVEWAY 6-INCH SY	STATION	TO	STATION	LOCATION	650.5000 CONSTRUCTION STAKING BASE LF	650.8000 CONSTRUCTION STAKING RESURFACING REFERENCE LF	STATION	LOCATION	690.0150 SAWING ASPHALT LF	690.0250 CONCRETE LF
117+25	PE LT	10	103+93	-	132+00	PROJECT	2,807	2,807	118+75	33RD AVE INTERSECTION	24	
	TOTAL	10				TOTAL	2,807	2,807	126+90	35TH AVE INTERSECTION	24	
									103+90	120TH ST DRIVEWAYS	248	30
										TOTAL	296	30

LOCATION	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAY	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	643.0900 TRAFFIC CONTROL SIGNS DAY	643.5000 TRAFFIC CONTROL EACH	STATION	TO	STATION	LOCATION	650.9911 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (8996-01-13) LS	STATION	TO	STATION	LOCATION	648.0100 LOCATING NO- PASSING ZONES MI
PROJECT (8996-01-13)	100	160	130	0.5	103+93	-	132+00	PROJECT	1	103+93	-	132+00	120TH STREET	0.53
TOTAL	100	160	130	0.5				TOTAL	1				TOTAL	0.53



5

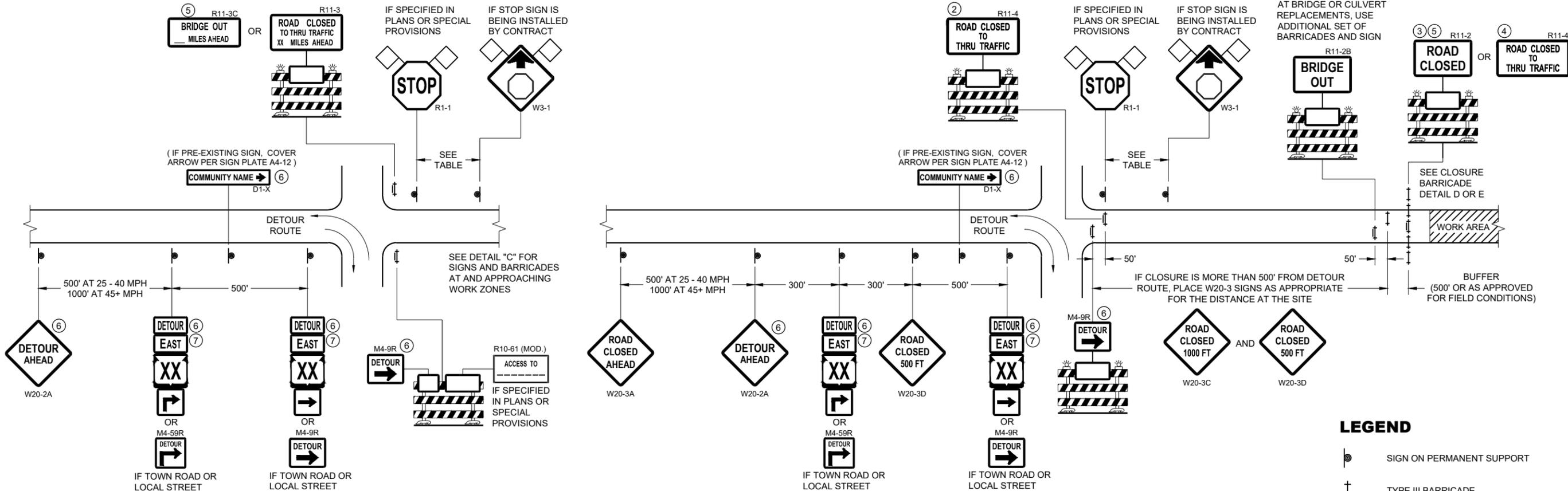
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PROJECT NO: 8996-01-13	HWY: 120TH ST	COUNTY: CHIPPEWA	120TH STREET - PLAN LAYOUT	SHEET	E
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## Standard Detail Drawing List

15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS
15C08-21A	LONGITUDINAL MARKING (MAINLINE)
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION



**DETAIL A  
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM  
DETOUR ROUTE ( 1000 FEET IF URBAN )

**DETAIL B  
MAINLINE CLOSURE WITH POSTED DETOUR**

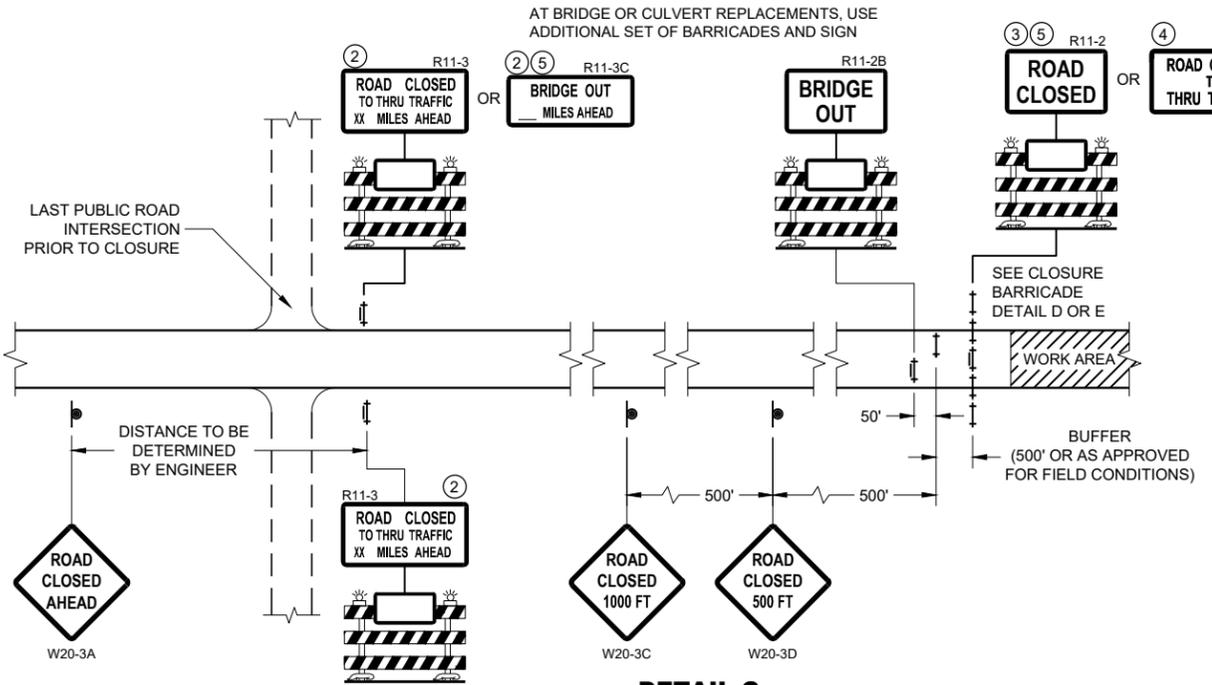
WORK ZONE LESS THAN 1/2 MILE FROM  
DETOUR ROUTE ( 1000 FEET IF URBAN )

**LEGEND**

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



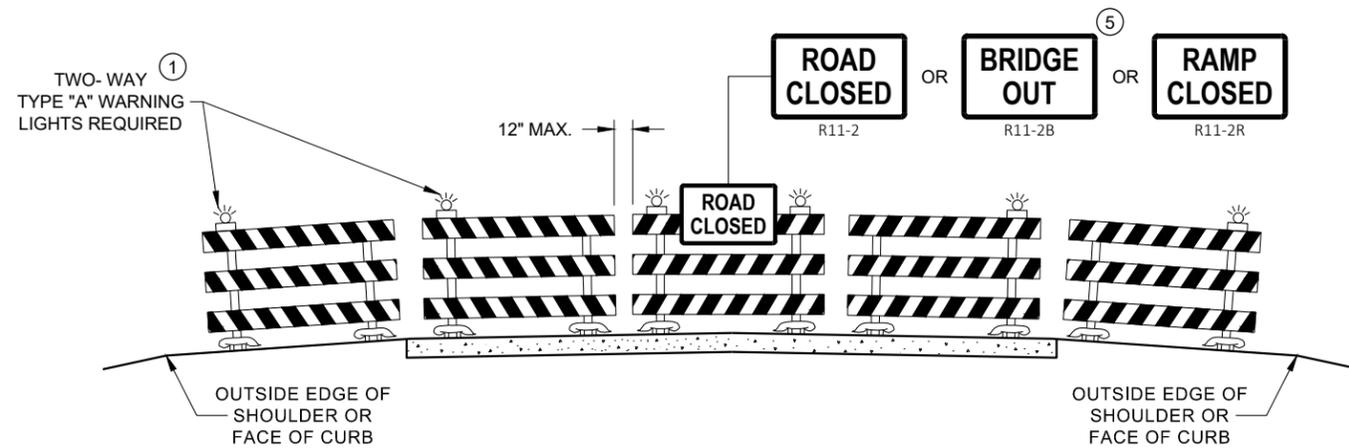
**DETAIL C  
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"  
FOR GENERAL NOTES  
AND FOOTNOTES ① THROUGH ⑦

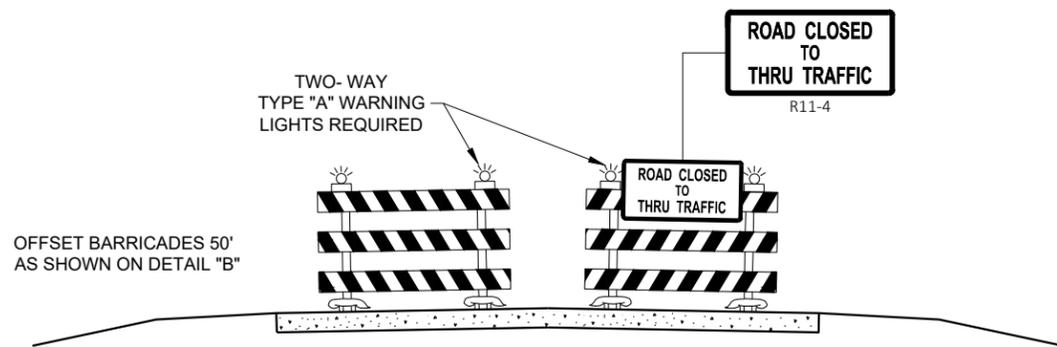
**BARRICADES AND SIGNS  
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2020 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER  
FHWA



**DETAIL D  
ROAD CLOSURE BARRICADE DETAIL  
APPROACH VIEW**



**DETAIL E  
LANE CLOSURE BARRICADE DETAIL  
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS  
FOR  
VARIOUS CLOSURES**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2020 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

FHWA

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

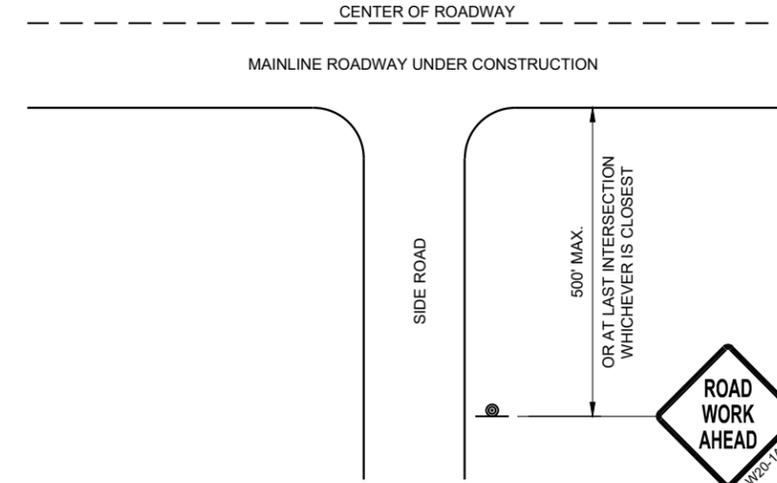
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

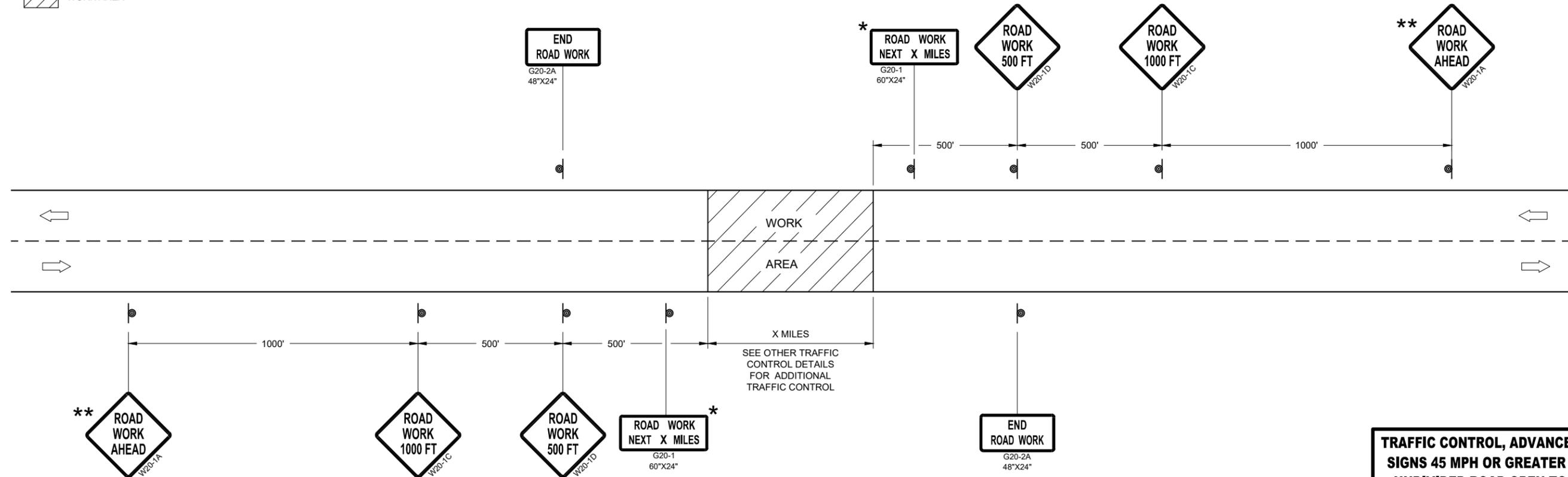
- \* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS
- \*\* PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

**LEGEND**

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL**



**TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER**

**TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 MPH OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE July 2018 /S/ Andrew Heidtke  
WORK ZONE ENGINEER

FHWA

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

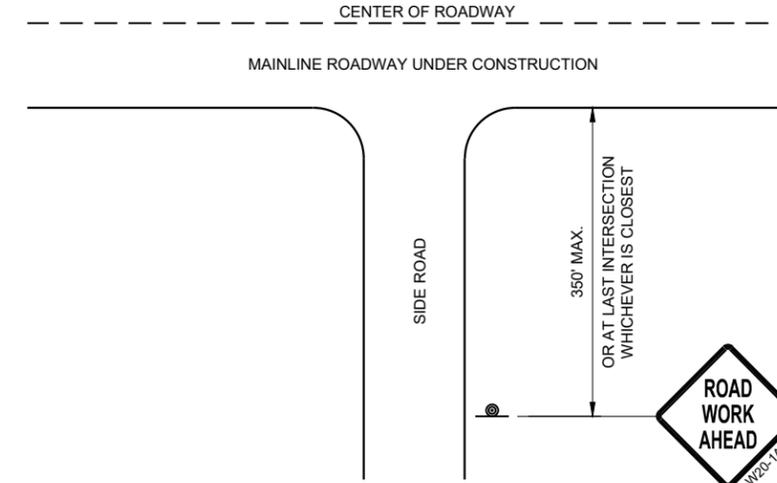
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

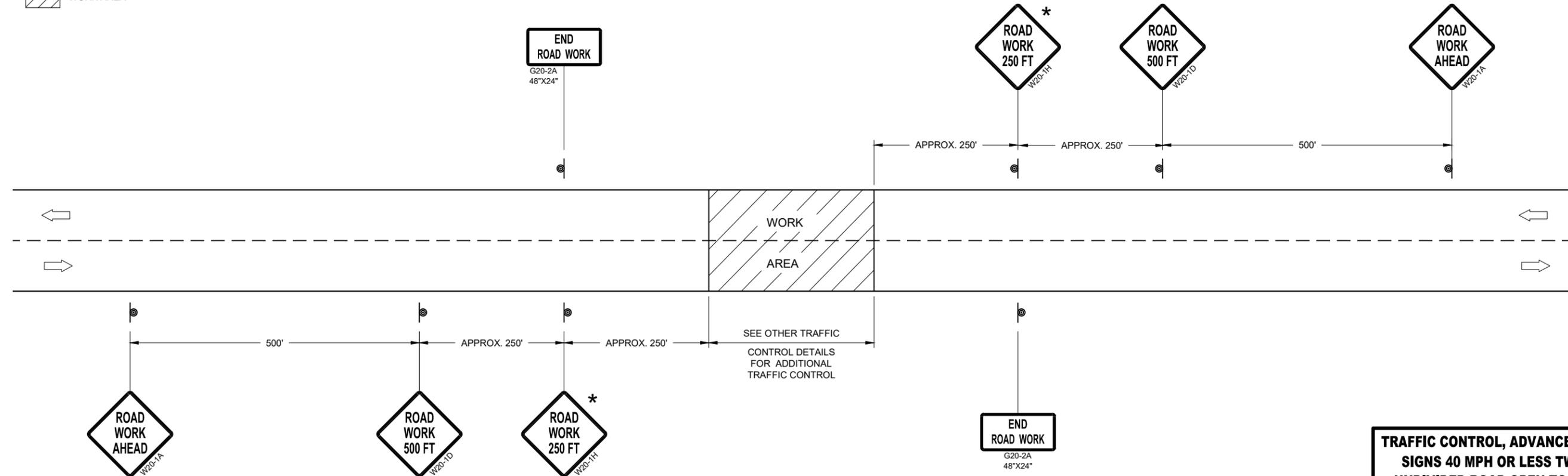
\* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



**TYPICAL SIDE ROAD APPROACH  
WARNING SIGN DETAIL**

**LEGEND**

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA



**TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS**

**TRAFFIC CONTROL, ADVANCE WARNING  
SIGNS 40 MPH OR LESS TWO-WAY  
UNDIVIDED ROAD OPEN TO TRAFFIC**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
July 2018 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

FHWA

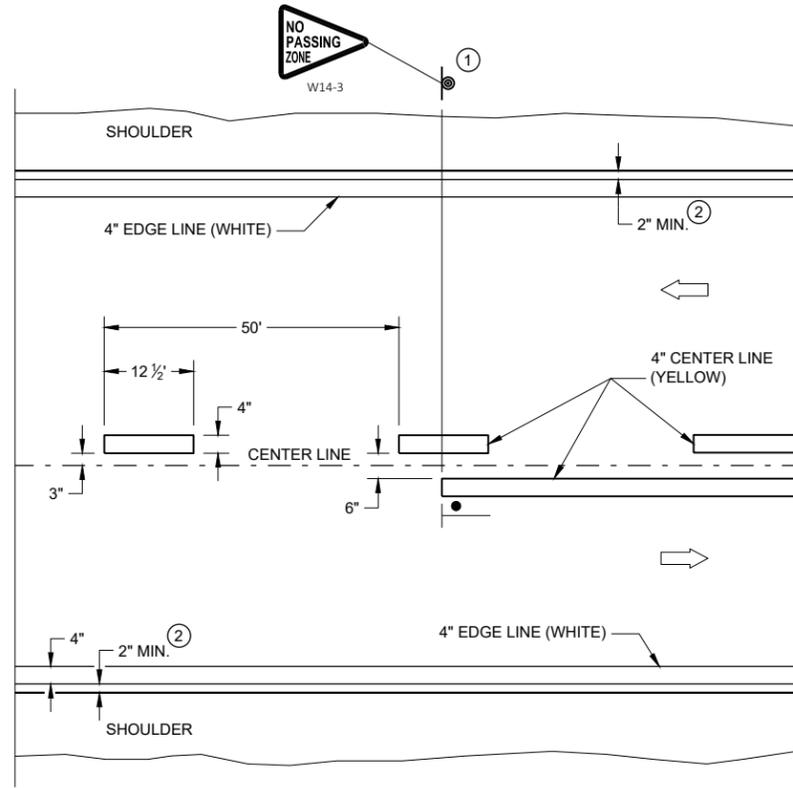
**GENERAL NOTES**

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

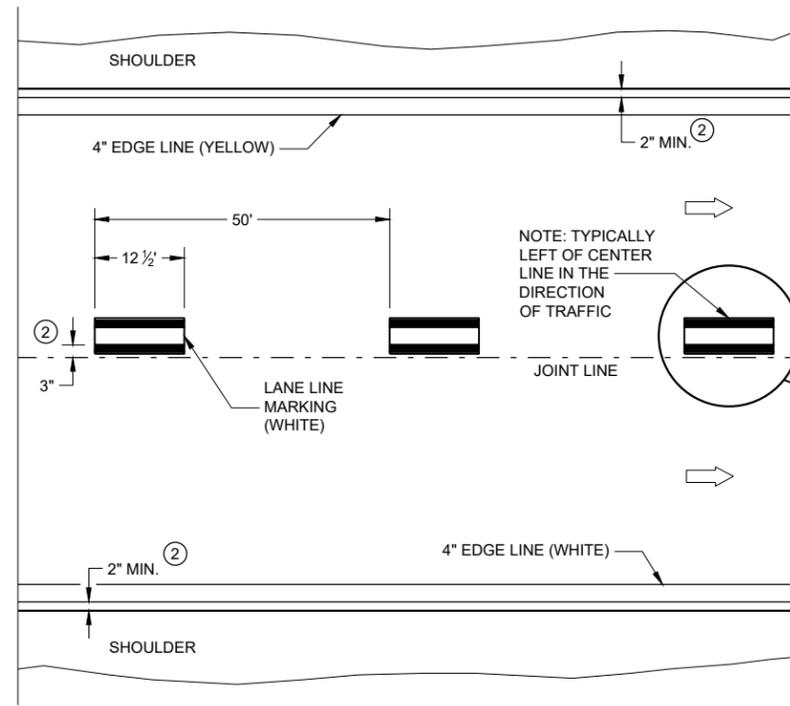
- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

**LEGEND**

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

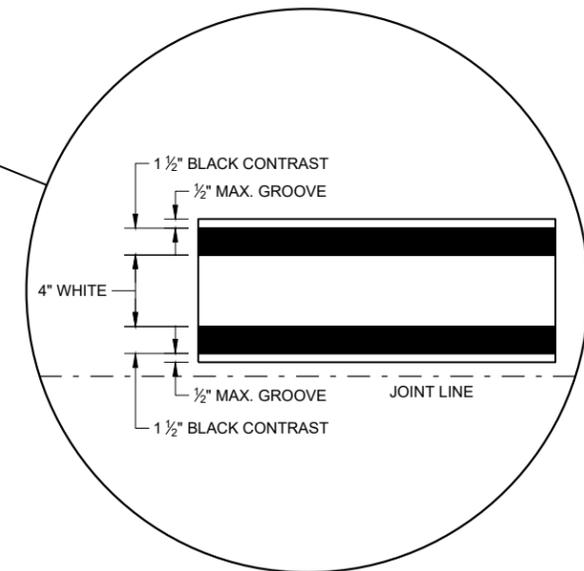


**TWO WAY TRAFFIC**



**ONE WAY TRAFFIC**

**PERMANENT PAVEMENT MARKING**



**PERMANENT LONGITUDINAL PAVEMENT MARKINGS**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE: May 2022 /S/ Jeannie Silver  
STATEWIDE SIGNING AND MARKING ENGINEER

**LEGEND**

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

**GENERAL NOTES**

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

**FLAGGING**

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

**TEMPORARY PORTABLE RUMBLE STRIPS**

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

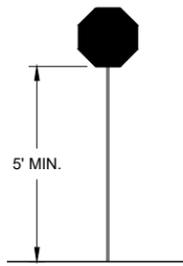
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



**STOP/SLOW PADDLE ON SUPPORT STAFF**

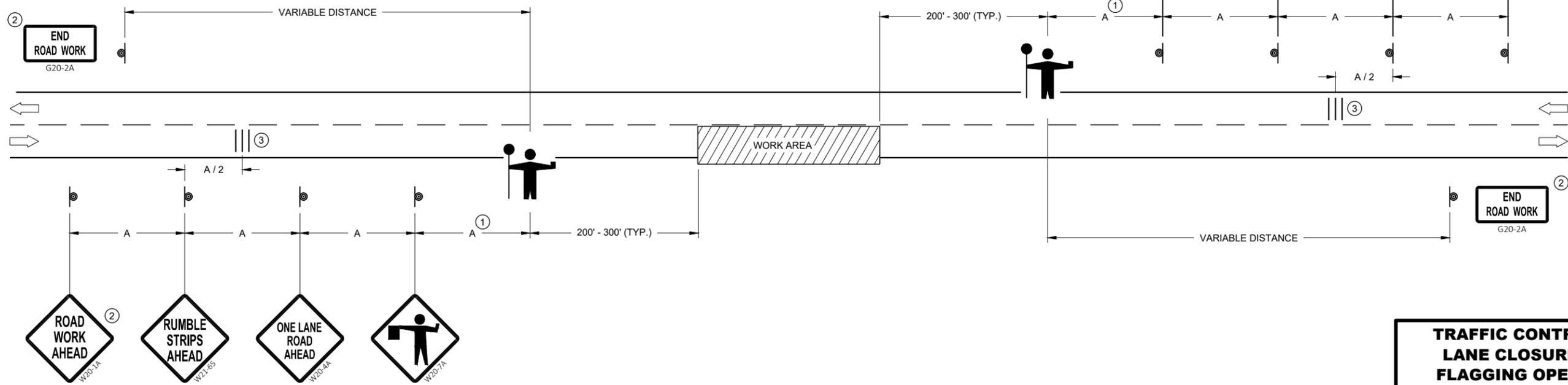
**SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE**

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



W03-4

USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".

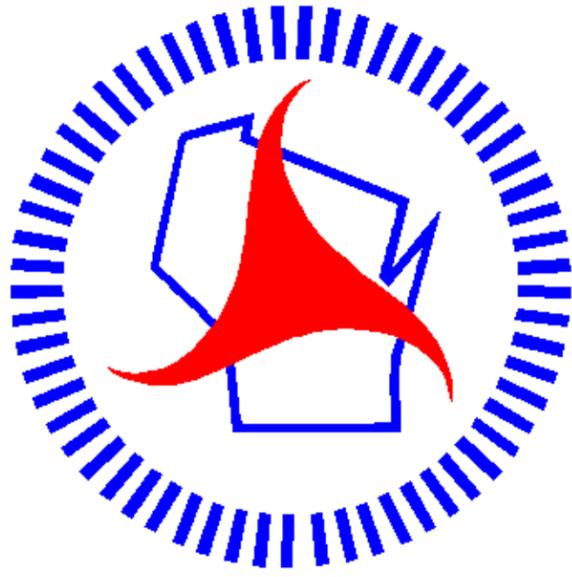


**TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE: May 2022 /S/ Andrew Heidtke  
WORK ZONE ENGINEER

FHWA



## ***Wisconsin Department of Transportation***

Dedicated people creating transportation solutions through innovation and exceptional service.

<http://www.dot.wisconsin.gov>

EAU NOVEMBER 2022  
 PROJECT ID: 8996-01-15  
 WITH: N/A

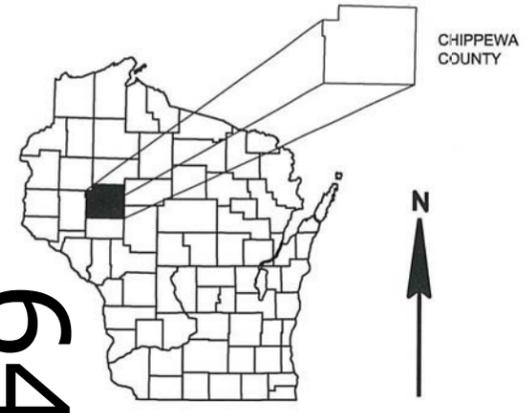
NOVEMBER 2022

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plan
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plans
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Gross Sections

TOTAL SHEETS = 20

64



DESIGN DESIGNATION

A.A.D.T.	=	490
A.A.D.T.	=	N/A
D.H.V.	=	N/A
D.D.	=	N/A
T.	=	N/A
DESIGN SPEED	=	35 MPH
ESALS	=	73,000

CONVENTIONAL SYMBOLS

<b>PLAN</b>	<b>P/PROFILE</b>
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	<b>UTILITIES</b>
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
	STORM SEWER
	TELEPHONE
MARSH AREA	WATER
	UTILITY PEDESTAL
	POWER POLE
WOODED OR SHRUB AREA	TELEPHONE POLE

STATE OF WISCONSIN  
 DEPARTMENT OF TRANSPORTATION

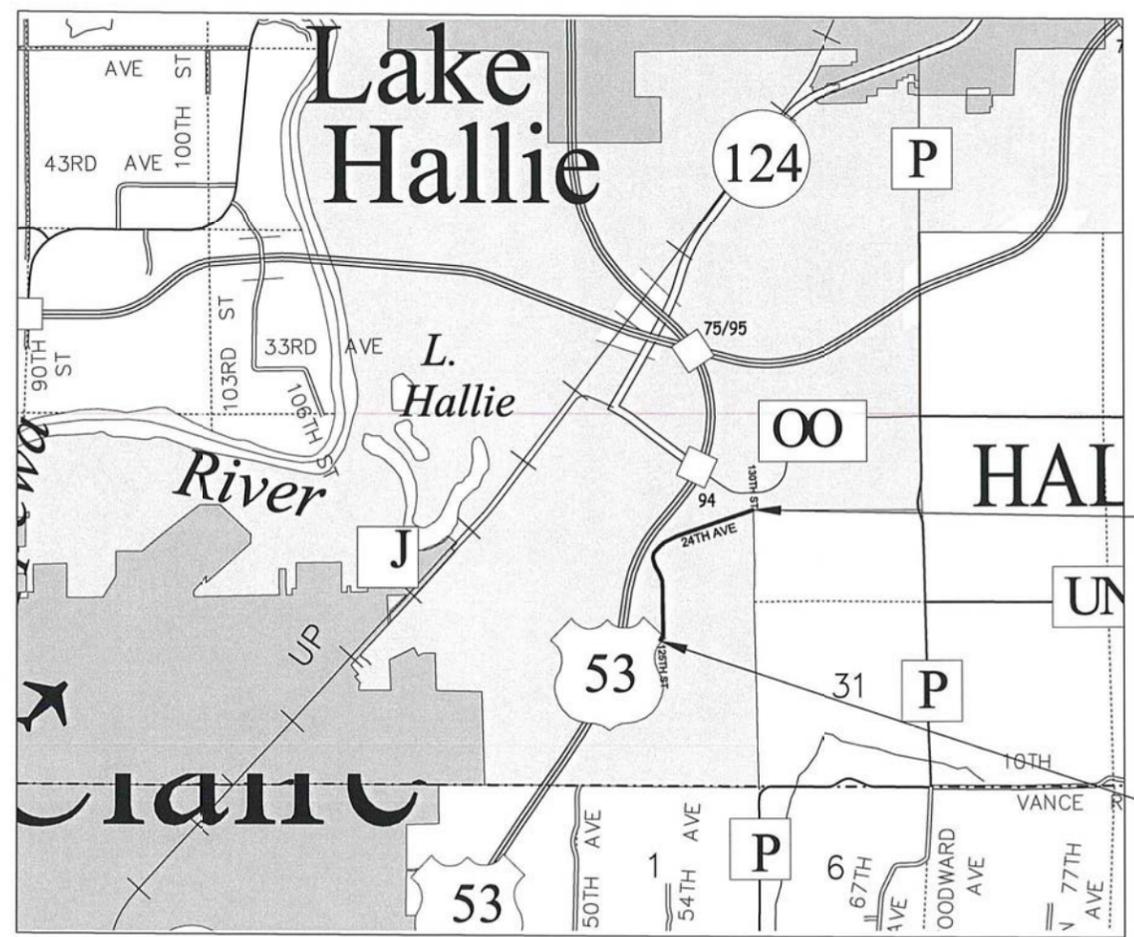
PLAN OF PROPOSED IMPROVEMENT

V LAKE HALLIE, 24TH AVE

130TH STREET TO 125TH STREET

CHIPPEWA COUNTY

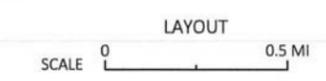
STATE PROJECT NUMBER  
 8996-01-15



END PROJECT  
 STA 73+68.22

BEGIN PROJECT  
 STA 16+59.34

Y = 104936.73  
 X = 164966.64



TOTAL NET LENGTH OF CENTERLINE = 1.081 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), CHIPPEWA COUNTY, NAD83 ( 1983 ), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 ( 1929 ). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
8996-01-15	WISC 2023062	1

ACCEPTED FOR  
 VILLAGE OF LAKE HALLIE  
 Date: 07/29/22  
 (Signature and Title of Official)  
 VILLAGE PRESIDENT

ORIGINAL PLANS PREPARED BY  
  
 DATE: 07/29/2022  
 (Professional Engineer Signature)

STATE OF WISCONSIN  
 DEPARTMENT OF TRANSPORTATION  
 PREPARED BY  
 Surveyor: N/A  
 Designer: AYRES ASSOCIATES INC  
 Project Manager: MATTHEW THORSEN  
 Regional Examiner: TOU YANG  
 Regional Supervisor: TYLER RONGSTAD

APPROVED FOR THE DEPARTMENT  
 DATE: 8/1/2022  
 (Signature)

E

**GENERAL NOTES**

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLAN ARE APPROXIMATE. THERE MAY BE OTHER INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY UTILITY WHICH IS NOT A MEMBER OF THE DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.

NO TREES OR SHRUBS SHALL BE REMOVED WITHOUT APPROVAL BY THE ENGINEER.

TEMPORARY STORAGE OF ANY EXCAVATED MATERIAL WILL NOT BE PERMITTED IN WETLANDS, FLOODWAY OR FLOODPLAIN OF ANY WETLANDS.

TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

DETAILS OF CONSTRUCTION NOT SHOWN SHALL BE IN ACCORDANCE WITH THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS.

A SAWED JOINT WILL BE REQUIRED WHERE NEW PAVEMENT IS TO MEET AN EXISTING PAVED SURFACE.

INTERSECTION RADII TO MATCH EXISTING. RADII SHOWN ON PLAN IS FOR INFORMATION ONLY. ALL RADII ARE MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE SHOWN OR NOTED ON THE PLAN.

THE EXACT LOCATION AND WIDTH OF DRIVEWAY ENTRANCES WILL BE DETERMINED BY THE FIELD ENGINEER. ALL DRIVEWAYS ARE TO BE REPLACED IN KIND UNLESS OTHERWISE DIRECTED BY THE ENGINEER OR AS SHOWN ON THE PLANS.

EXISTING ELEVATIONS SHALL BE VERIFIED IN THE FIELD.

THE CONTRACTOR IS TO WORK WITH UTMOST CARE AND PROTECT ALL SURVEY MARKERS. SURVEY MARKERS SHALL NOT BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

PROPOSED ROADWAY IMPROVEMENTS SHALL MATCH EXISTING SUPERELEVATIONS AND ROADWAY CROSS SLOPES IN MILLED AREAS.

**UTILITY CONTACTS**

AT & T  
304 S DEWEY ST  
EAU CLAIRE, WI 54701  
ATTN: RICK PODOLAK  
715-410-0656  
RP4514@ATT.COM

XCEL ENERGY (NATURAL GAS)  
1400 WESTERN AVE  
EAU CLAIRE, WI 54702  
ATTN: MELISSA GOETTL (EWINGS)  
CELL 715-271-6883  
MELISSA.A.EWINGS@XCELENERGY.COM

SPECTRUM / CHARTER COMMUNICATIONS  
1201 MCCANN DRIVE  
ALTOONA, WI 54720  
ATTN: SUNNY RICHARDSON  
715-896-6503  
SUNNY.RICHARDSON@CHARTER.COM

VILLAGE OF LAKE HALLIE WATER MAIN  
13136 30TH AVE  
LAKE HALLIE, WI 54729  
ATTN: DEREK S  
CELL: 715.559.7019  
DSCHAD@LAKEHALLIE.US

EAU CLAIRE ENERGY COOPERATIVE  
8214 HWY 12  
FALL CREEK, WI 54742  
ATTN: PATRICK BETHKE  
715-836-6474  
PBETHKE@ECEC.COM

\*DENOTES NOT A MEMBER OF DIGGERS HOTLINE



**DNR CONTACT**

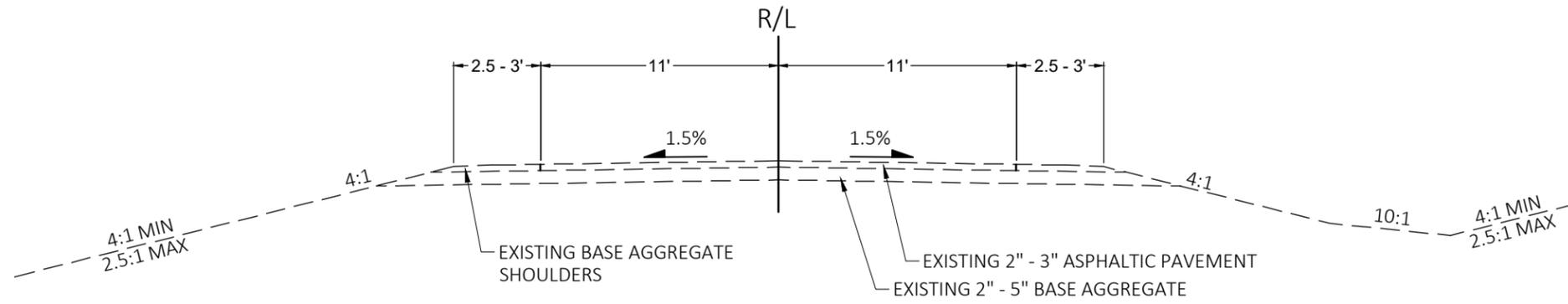
DNR NORTHERN REGION HQ  
1300 WEST CLAIREMONT AVENUE  
EAU CLAIRE, WI 54701  
ATTN: LEAH NICOL  
715-934-9014  
LEAH.NICOL@WISCONSIN.GOV

**DESIGN CONTACT**

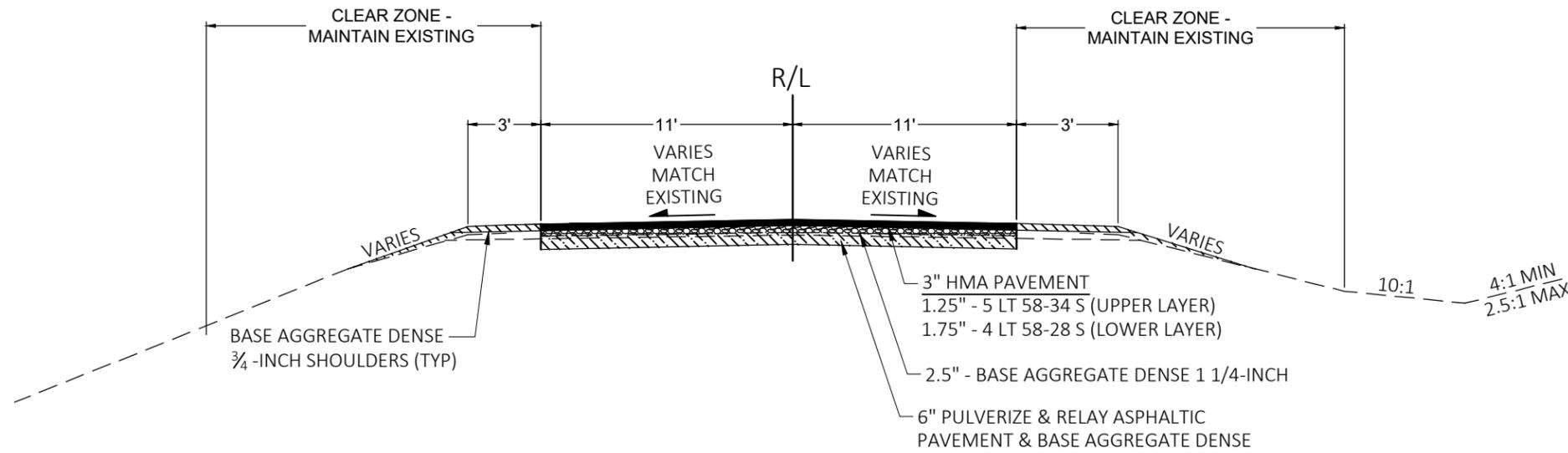
AYRES ASSOCIATES INC  
3433 OAKWOOD HILLS PARKWAY  
EAU CLAIRE, WI 54701  
ATTN: LISA FLEMING P.E.  
715-834-3161  
FLEMINGL@AYRESASSOCIATES.COM

**WISDOT DESIGN CONTACT**

DEPARTMENT OF TRANSPORTATION  
718 W. CLAIREMONT AVE  
EAU CLAIRE, WI 54701  
ATTN: MATTHEW THORNSEN, P.E.  
715-225-4159  
MATTHEW.THORNSEN@DOT.WI.GOV

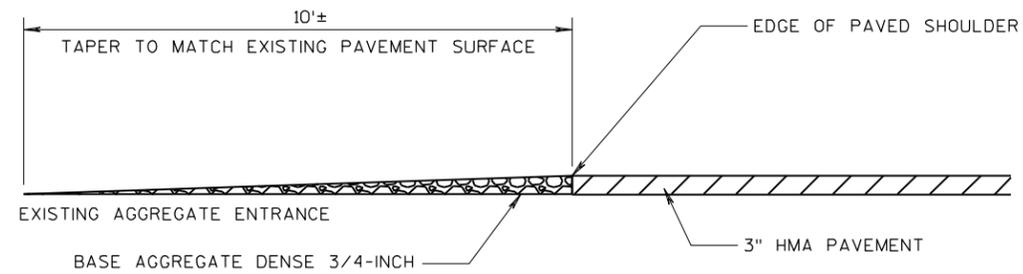


**EXISTING TYPICAL SECTION**

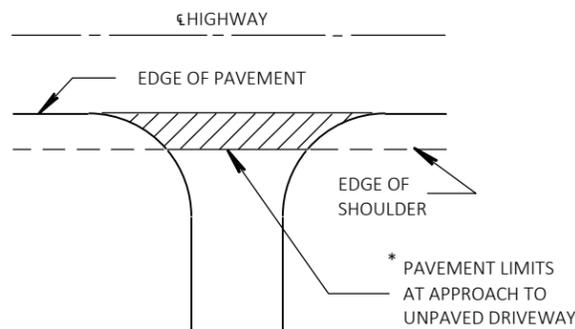


**FINISHED TYPICAL SECTION**

STA 16+00 - STA 73+68



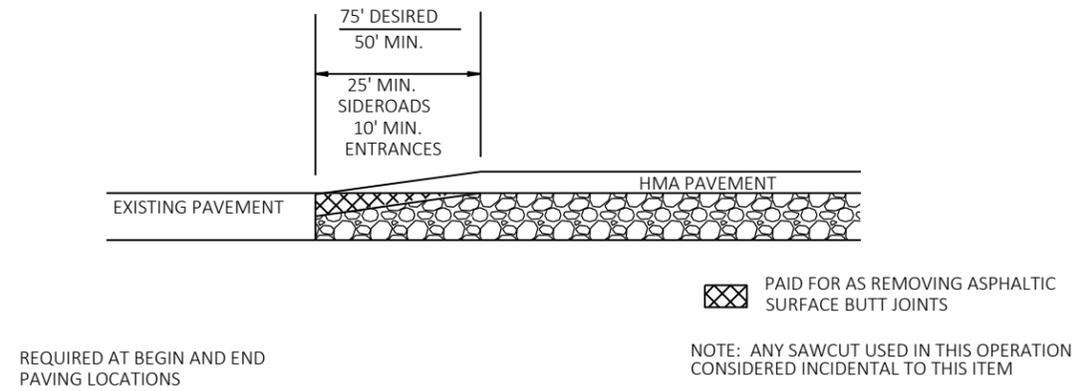
**BASE AGGREGATE PRIVATE ENTRANCE DETAIL PROFILE VIEW**



\*WHERE DRIVEWAY IS PAVED, APPROACH PAVEMENT SHOULD BE EXTENDED TO MATCH DRIVEWAY PAVEMENT.

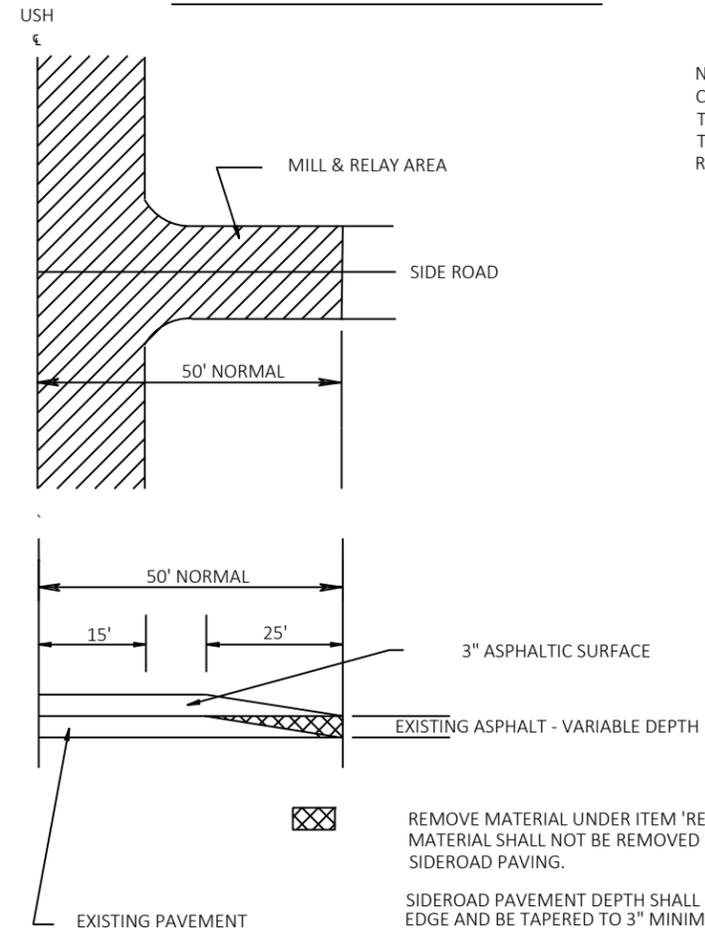
**PLAN VIEW**

**RURAL DRIVEWAY INTERSECTION DETAIL**



**BUTT JOINT**

**SIDE ROAD DETAIL - NO CURB & GUTTER**



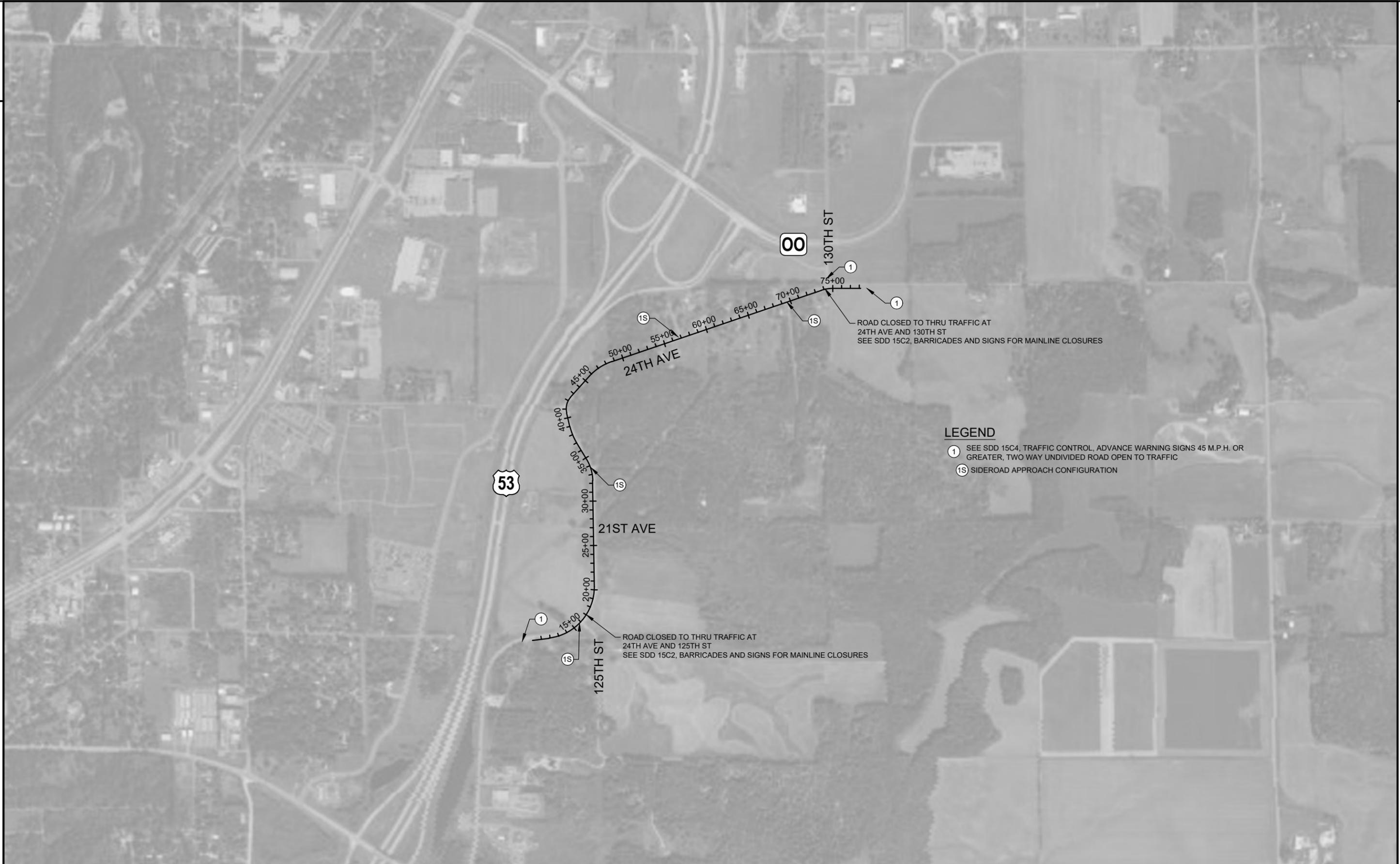
NOTE: IF THE EXISTING SIDEROAD CONSISTS OF A BASE COURSE SURFACE, THE NEW ASPHALT SHALL BE PLACED TO THE ENDS OF THE EXISTING SIDEROAD RADIUS'.

NOT TO SCALE

REMOVE MATERIAL UNDER ITEM 'REMOVING ASPHALT SURFACE , BUTT JOINTS' MATERIAL SHALL NOT BE REMOVED UNDER THIS ITEM UNTIL 24 HOURS BEFORE SIDEROAD PAVING.

SIDEROAD PAVEMENT DEPTH SHALL MATCH AT MAINLINE PAVEMENT EDGE AND BE TAPERED TO 3" MINIMUM AT JOINT

NOTE: ANY SAWCUT USED WILL BE CONSIDERED INCIDENTAL TO THE ITEM "REMOVING ASPHALTIC SURFACE, BUTT JOINTS."



**LEGEND**

- ① SEE SDD 15C4, TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC
- ①S SIDEROAD APPROACH CONFIGURATION

PROJECT NO: 8996-01-15	HWY: 24TH AVENUE	COUNTY: CHIPPEWA	TRAFFIC CONTROL	SHEET	<b>E</b>
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Estimate Of Quantities By Plan Sets

8996-01-15

Line	Item	Item Description	Unit	Total	Qty
0002	204.0100	Removing Concrete Pavement	SY	97.000	97.000
0004	204.0110	Removing Asphaltic Surface	SY	361.000	361.000
0006	204.0115	Removing Asphaltic Surface Butt Joints	SY	856.000	856.000
0010	211.0101	Prepare Foundation for Asphaltic Paving (project) 02. 8996-01-15	EACH	1.000	1.000
0014	213.0100	Finishing Roadway (project) 02. 8996-01-15	EACH	1.000	1.000
0016	305.0110	Base Aggregate Dense 3/4-Inch	TON	650.000	650.000
0018	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	2,170.000	2,170.000
0020	325.0100	Pulverize and Relay	SY	14,939.000	14,939.000
0022	416.0160	Concrete Driveway 6-Inch	SY	97.000	97.000
0024	455.0605	Tack Coat	GAL	896.000	896.000
0026	460.2000	Incentive Density HMA Pavement	DOL	1,630.000	1,630.000
0028	460.5224	HMA Pavement 4 LT 58-28 S	TON	1,464.000	1,464.000
0030	460.5245	HMA Pavement 5 LT 58-34 S	TON	1,117.000	1,117.000
0032	619.1000	Mobilization	EACH	0.650	0.650
0034	624.0100	Water	MGAL	57.000	57.000
0036	642.5001	Field Office Type B	EACH	0.500	0.500
0038	643.0420	Traffic Control Barricades Type III	DAY	180.000	180.000
0040	643.0705	Traffic Control Warning Lights Type A	DAY	300.000	300.000
0042	643.0900	Traffic Control Signs	DAY	225.000	225.000
0044	643.5000	Traffic Control	EACH	0.500	0.500
0046	646.1020	Marking Line Epoxy 4-Inch	LF	22,436.000	22,436.000
0048	648.0100	Locating No-Passing Zones	MI	1.080	1.080
0050	650.5000	Construction Staking Base	LF	5,709.000	5,709.000
0052	650.8000	Construction Staking Resurfacing Reference	LF	5,709.000	5,709.000
0056	650.9911	Construction Staking Supplemental Control (project) 02. 8996-01-15	EACH	1.000	1.000
0058	690.0150	Sawing Asphalt	LF	180.000	180.000
0060	690.0250	Sawing Concrete	LF	87.000	87.000
0062	740.0440	Incentive IRI Ride	DOL	4,325.000	4,325.000
0064	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	300.000	300.000
0066	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	300.000	300.000

3

STATION TO STATION	LOCATION	204.0100 REMOVING CONCRETE PAVEMENT SY	204.0110 REMOVING ASPHALTIC SURFACE SY	STATION TO STATION	LOCATION	204.0115 REMOVING ASPHALTIC SURFACE BUTT JOINTS SY
58+90 -	LT	36	-	16+59 -	PROJECT START	183
59+50 -	LT	22	-	73+68 -	PROJECT END	183
70+30 -	LT	39	-	33+50 -	21ST AVE	122
16+58 -	PRIVATE ENTRANCES	-	361	41+00	22ND AVE	122
-				56+60 -	126TH ST	122
				69+25 -	129TH ST	122
				-		
	TOTAL	97	361		TOTAL	856

STATION TO STATION	LOCATION	211.0101 PREPARE FOUNDATION FOR ASPHALTIC PAVING (8996-01-15) EACH
16+59 - 73+68	24TH AVE	1
	TOTAL	1

STATION TO STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	624.0100 WATER MGAL
16+59 - 73+68	24TH AVE	635	2,114	55
33+50	21ST AVE	5	19	1
56+60	126TH ST	5	19	1
69+25	129TH ST	5	19	1
16+59 73+68	PRIVATE ENTRANCES	28		1
16+59 - 73+68	FIELD ENTRANCES	30		1
	TOTAL	650	2,170	57

STATION	LOCATION	416.0160 CONCRETE DRIVEWAY 6-INCH SY
58+90	LT	36
59+50	LT	22
70+30	LT	39
	TOTAL	97

STATION TO STATION	LOCATION	455.0605 TACK COAT GAL	460.5224 HMA PAVEMENT 4 LT 58-28 S TON	460.5245 HMA PAVEMENT 5 LT 58-34 S TON
16+59 - 73+68	24TH AVE	837	1,368	977
33+50	21ST AVE	15	24	20
41+00	22ND AVE	15	24	20
56+60	126TH ST	15	24	20
69+25	129TH ST	15	24	20
16+59 - 73+68	PRIVATE ENTRANCES			60
	TOTAL	896	1,464	1,117

LOCATION	642.5001 FIELD OFFICE TYPE B EACH
PROJECT (8996-01-15)	0.5
TOTAL	0.5

STATION TO STATION	LOCATION	325.0100 PULVERIZE AND RELAY SY
16+59 - 73+68	24TH AVE	13,958
33+50	21ST AVE	245
41+00	22ND AVE	245
56+60	126TH ST	245
69+25	129TH ST	245
	TOTAL	14,939

STATION TO STATION	LOCATION	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAY	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	643.0900 TRAFFIC CONTROL SIGNS DAY	643.5000 TRAFFIC CONTROL EACH
16+59 - 73+68	PROJECT (8996-01-15)	180	300	225	0.5
	TOTAL	180	300	225	0.5

3

3

				646.1020 MARKING LINE EPOXY 4-INCH LF
STATION	TO	STATION	LOCATION	
16+59	-	73+68	24TH AVE	11,418
16+59	-	73+68	24TH AVE	11,018
TOTAL				<u>22,436</u>

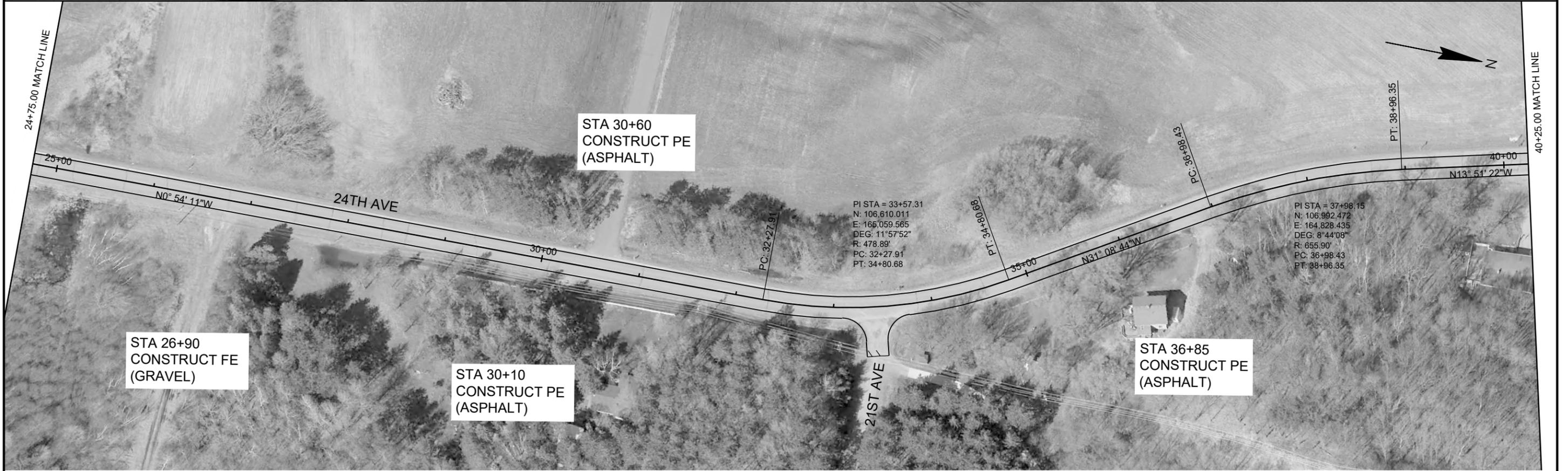
					648.0100 LOCATING NO- PASSING ZONES MI
STATION	TO	STATION	LOCATION		
16+59	-	73+68	24TH AVE	1.08	
TOTAL				<u>1.08</u>	

				650.5000 CONSTRUCTION STAKING BASE LF	650.8000 CONSTRUCTION STAKING RESURFACING REFERENCE LF
STATION	TO	STATION	LOCATION		
16+59	-	73+68	24TH AVE	5,709	5,709
TOTAL				<u>5,709</u>	<u>5,709</u>

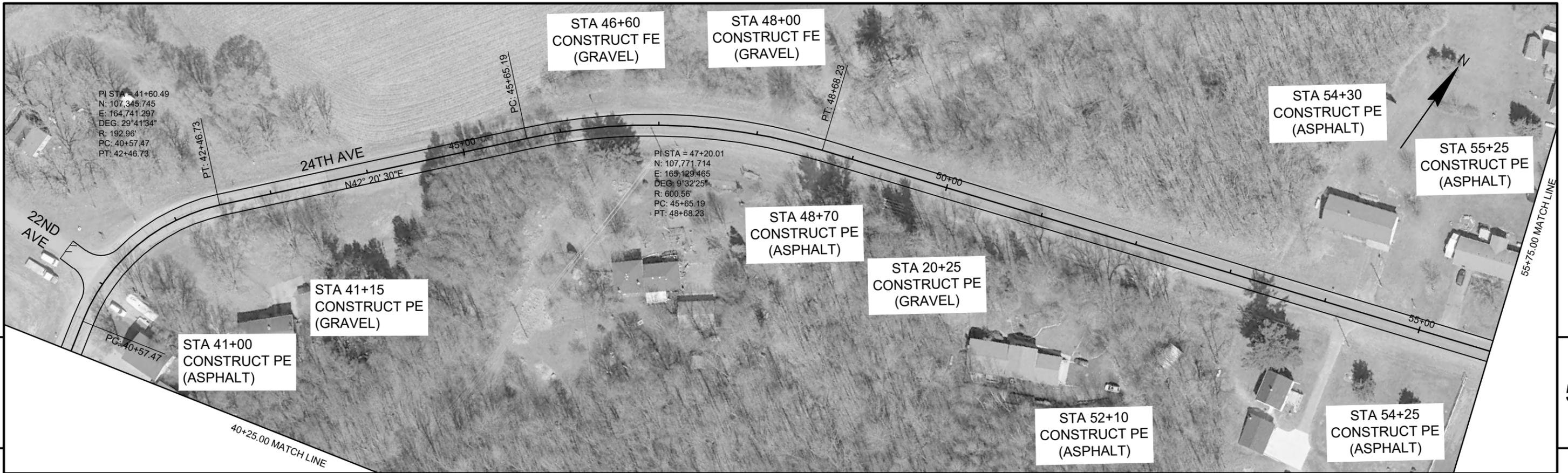
				650.9911.01 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (8996-01-15) EACH
STATION	TO	STATION	LOCATION	
16+59	-	73+68	24TH AVE	1
TOTAL				<u>1</u>

				690.0150 SAWING ASPHALT LF	690.0250 SAWING CONCRETE LF
STATION	TO	STATION	LOCATION		
16+59	-	73+68	PRIVATE ENTRANCES	180	87
TOTAL				<u>180</u>	<u>87</u>

		213.0100.02 FINISHING ROADWAY (PROJECT) (02. 8996-01-15) EACH
LOCATION		
24TH AVE		1
TOTAL		<u>1</u>



PROJECT NO: 8996-01-15	HWY: 24TH AVENUE	COUNTY: CHIPPEWA	24TH AVE PLAN LAYOUT	SHEET 06	E
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PROJECT NO: 8996-01-15	HWY: 24TH AVENUE	COUNTY: CHIPPEWA	24TH AVE PLAN LAYOUT	SHEET	E
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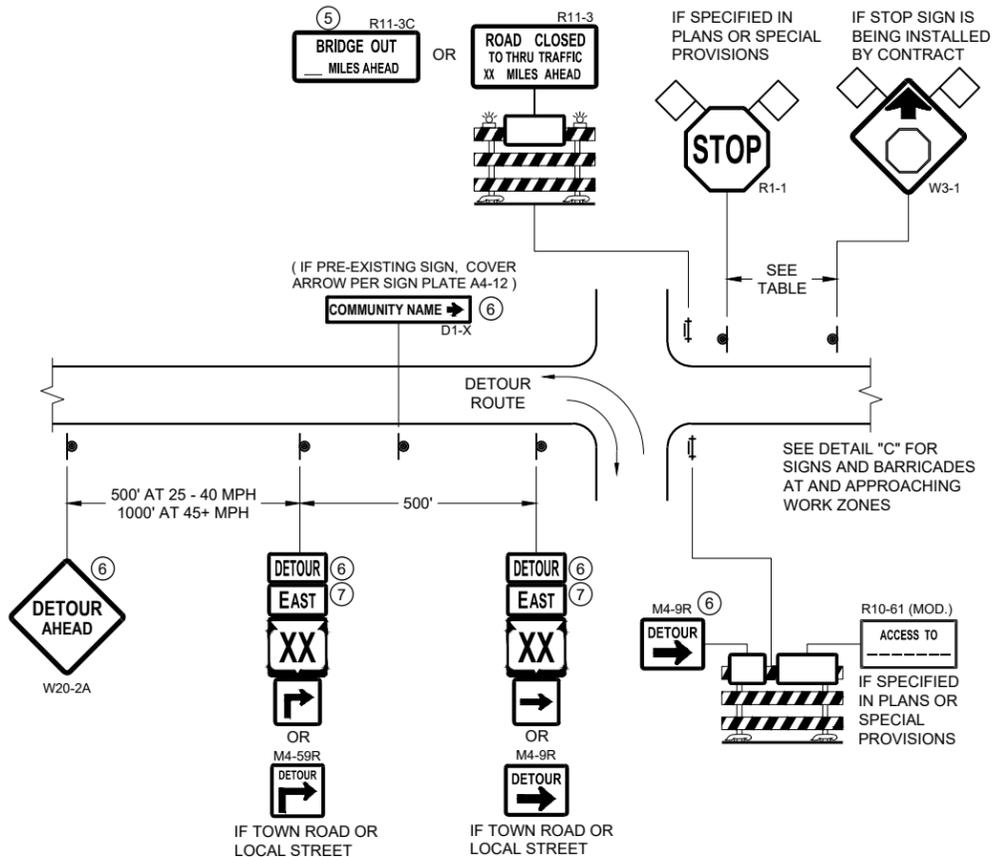


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5

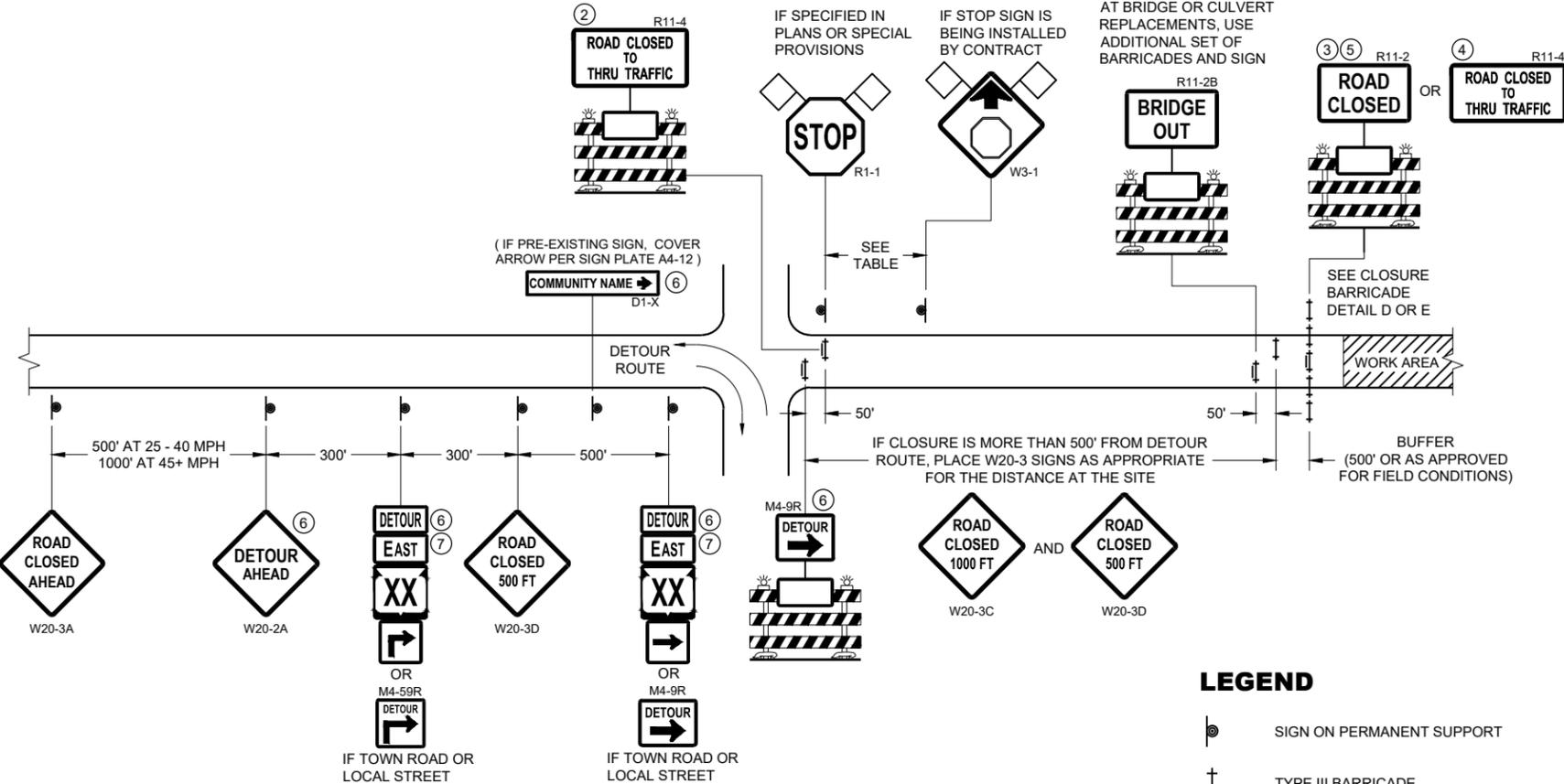
## Standard Detail Drawing List

15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS
15C08-21A	LONGITUDINAL MARKING (MAINLINE)
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION



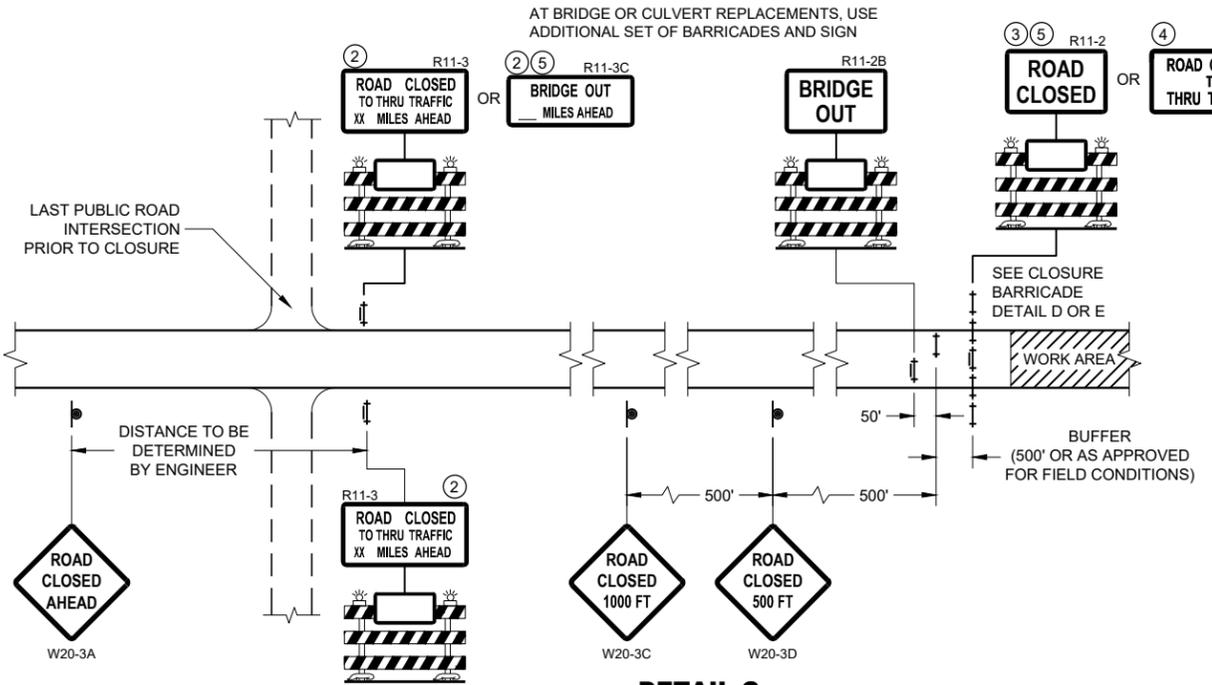
**DETAIL A  
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM  
DETOUR ROUTE ( 1000 FEET IF URBAN )



**DETAIL B  
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE LESS THAN 1/2 MILE FROM  
DETOUR ROUTE ( 1000 FEET IF URBAN )



**DETAIL C  
MAINLINE CLOSURE, NO POSTED DETOUR**

**LEGEND**

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)
- M4 - 8
- M3 - X
- M1 - 4 OR M1 - 6 OR M1 - 5A
- M05 - 1 OR M06 - 1

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

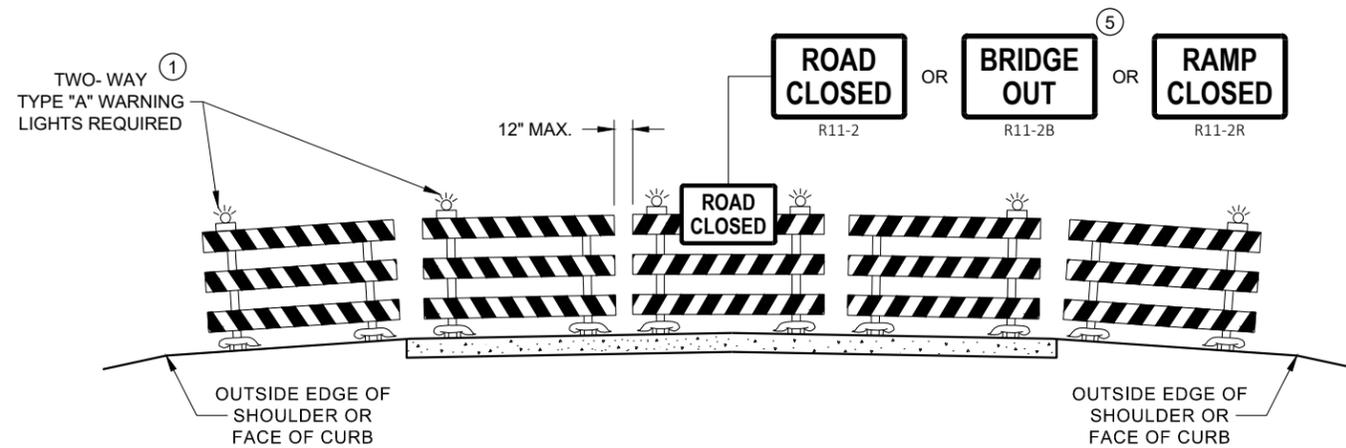
SEE SDD 15C2-SHEET "b"  
FOR GENERAL NOTES  
AND FOOTNOTES ① THROUGH ⑦

**BARRICADES AND SIGNS  
FOR MAINLINE CLOSURES**

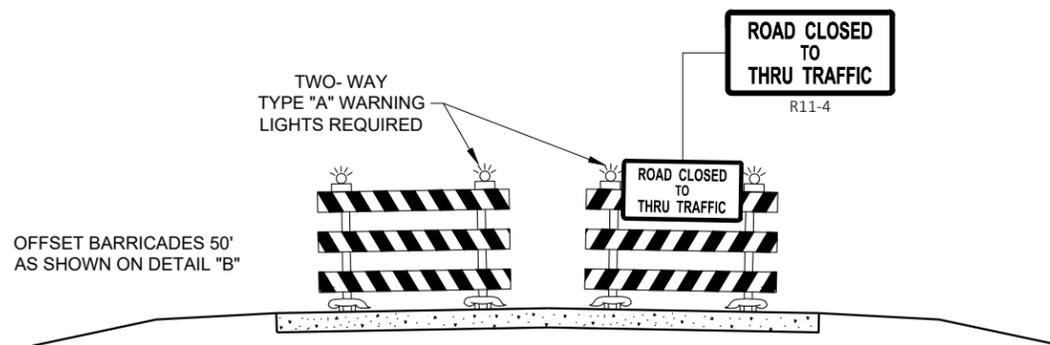
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2020 /S/ Andrew Heidtke  
DATE DATE WORK ZONE ENGINEER

FHWA



**DETAIL D  
ROAD CLOSURE BARRICADE DETAIL  
APPROACH VIEW**



**DETAIL E  
LANE CLOSURE BARRICADE DETAIL  
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS  
FOR  
VARIOUS CLOSURES**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2020 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER  
FHWA

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

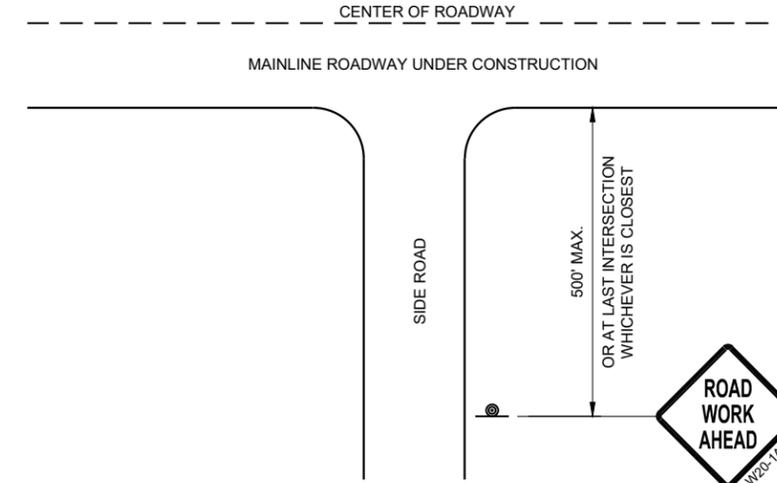
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

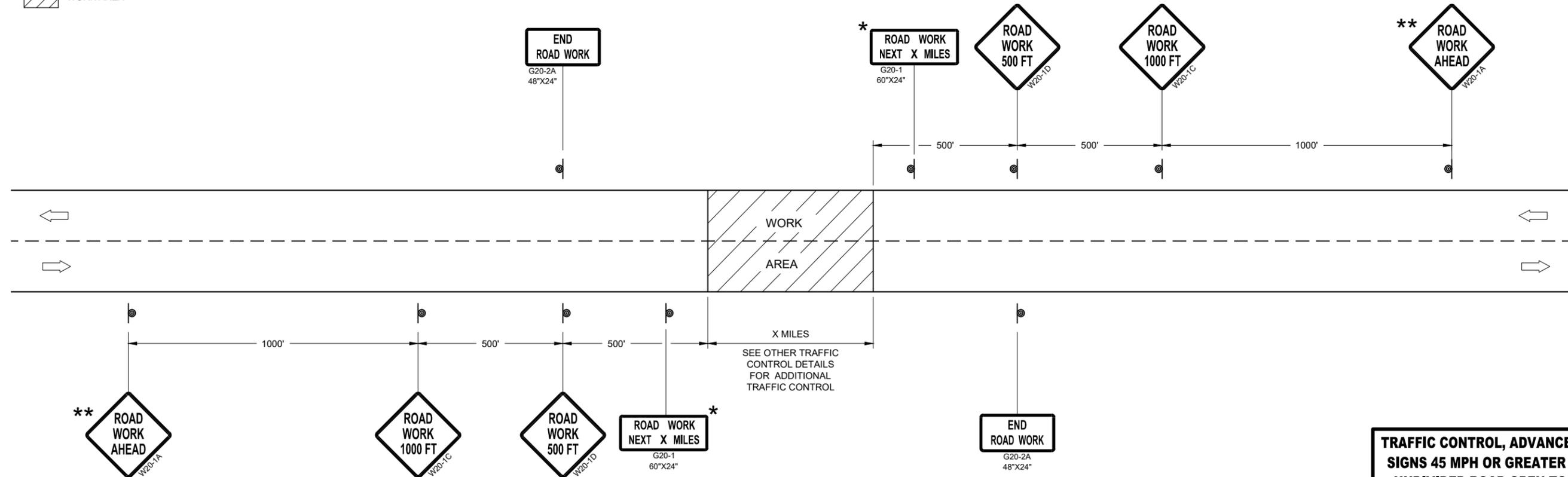
- \* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS
- \*\* PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

**LEGEND**

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH  
WARNING SIGN DETAIL**



**TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER**

**TRAFFIC CONTROL, ADVANCE WARNING  
SIGNS 45 MPH OR GREATER TWO-WAY  
UNDIVIDED ROAD OPEN TO TRAFFIC**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE July 2018 /S/ Andrew Heidtke  
WORK ZONE ENGINEER

FHWA

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

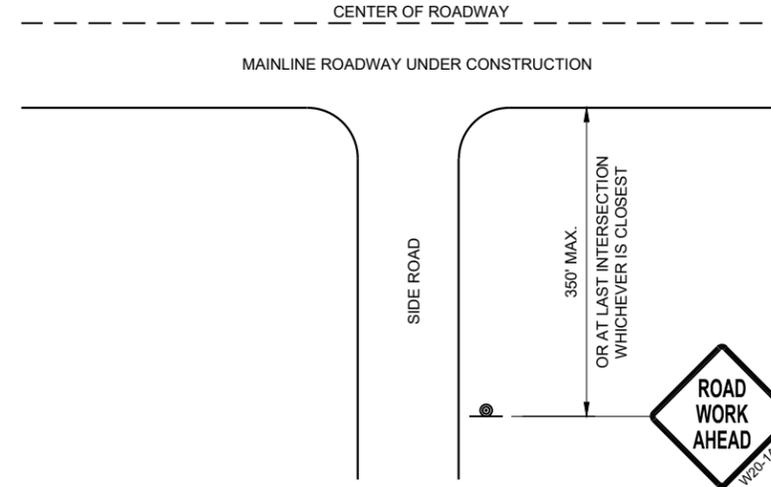
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

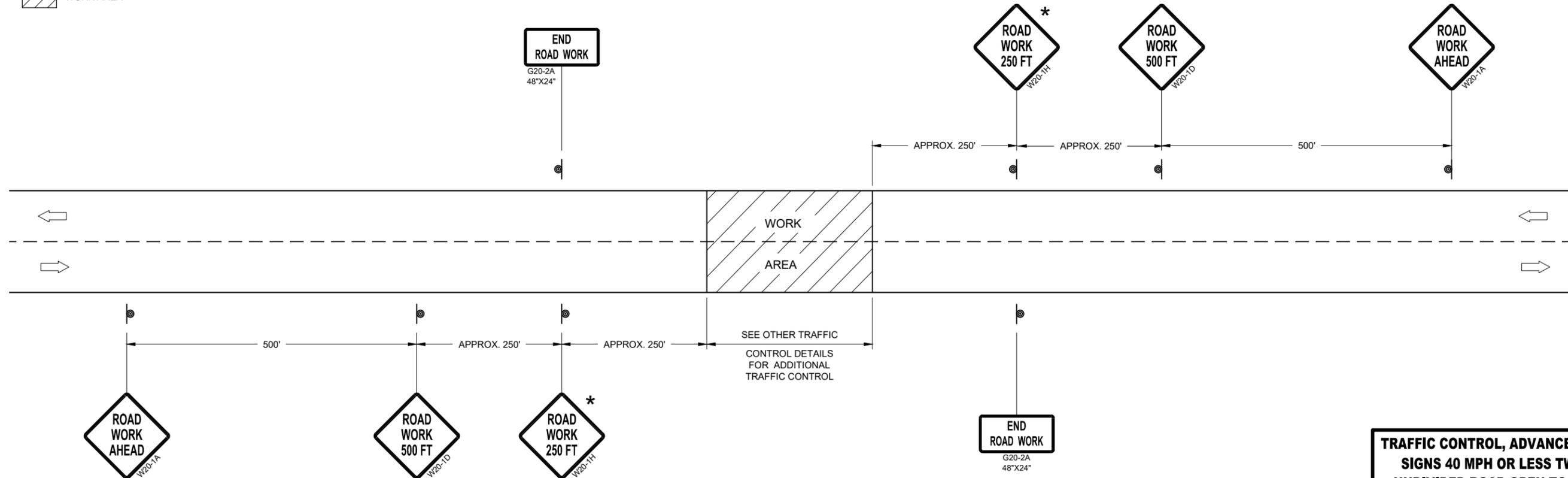
\* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

**LEGEND**

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH  
WARNING SIGN DETAIL**



**TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS**

**TRAFFIC CONTROL, ADVANCE WARNING  
SIGNS 40 MPH OR LESS TWO-WAY  
UNDIVIDED ROAD OPEN TO TRAFFIC**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
July 2018 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

FHWA

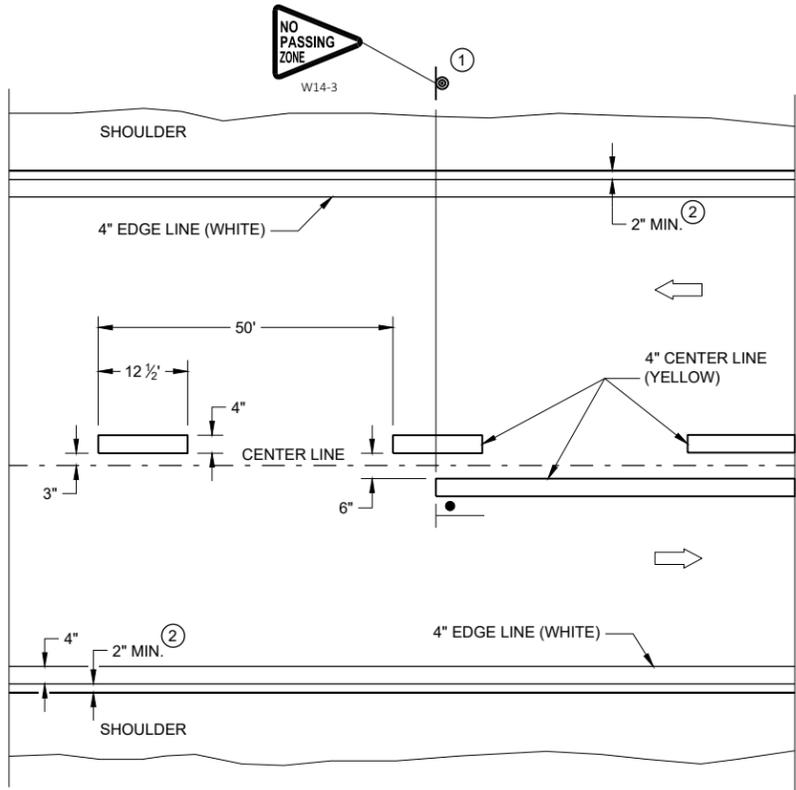
**GENERAL NOTES**

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

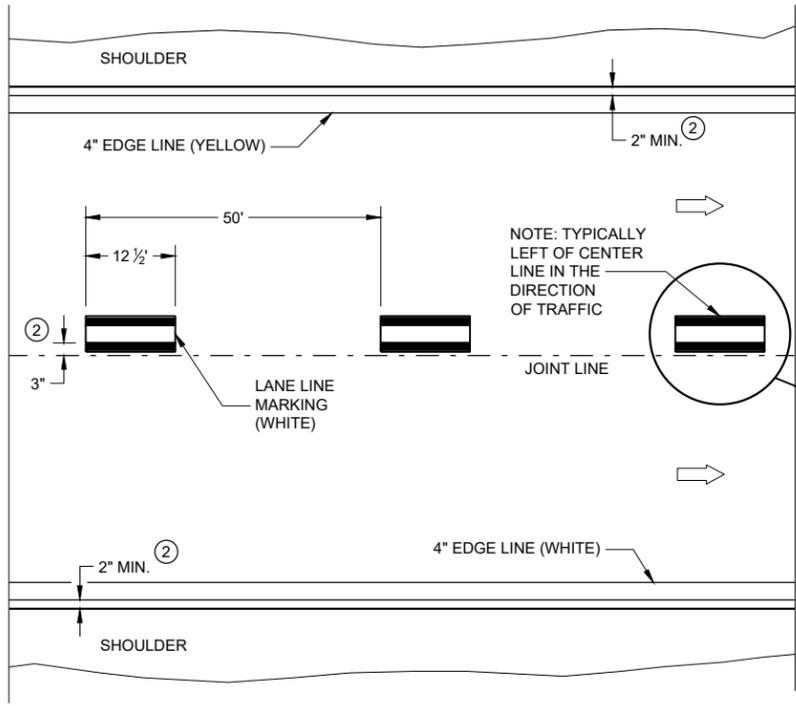
- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

**LEGEND**

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

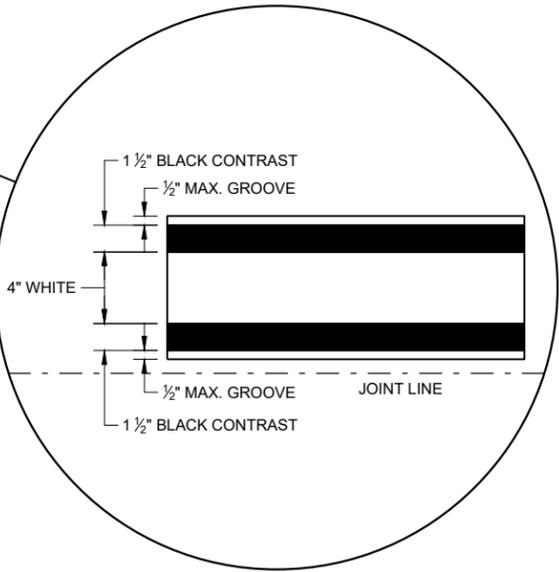


**TWO WAY TRAFFIC**



**ONE WAY TRAFFIC**

**PERMANENT PAVEMENT MARKING**



**PERMANENT LONGITUDINAL PAVEMENT MARKINGS**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE: May 2022 /S/ Jeannie Silver  
STATEWIDE SIGNING AND MARKING ENGINEER

**LEGEND**

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

**GENERAL NOTES**

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

**FLAGGING**

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

**TEMPORARY PORTABLE RUMBLE STRIPS**

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

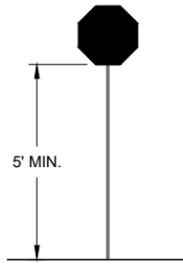
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



**STOP/SLOW PADDLE ON SUPPORT STAFF**

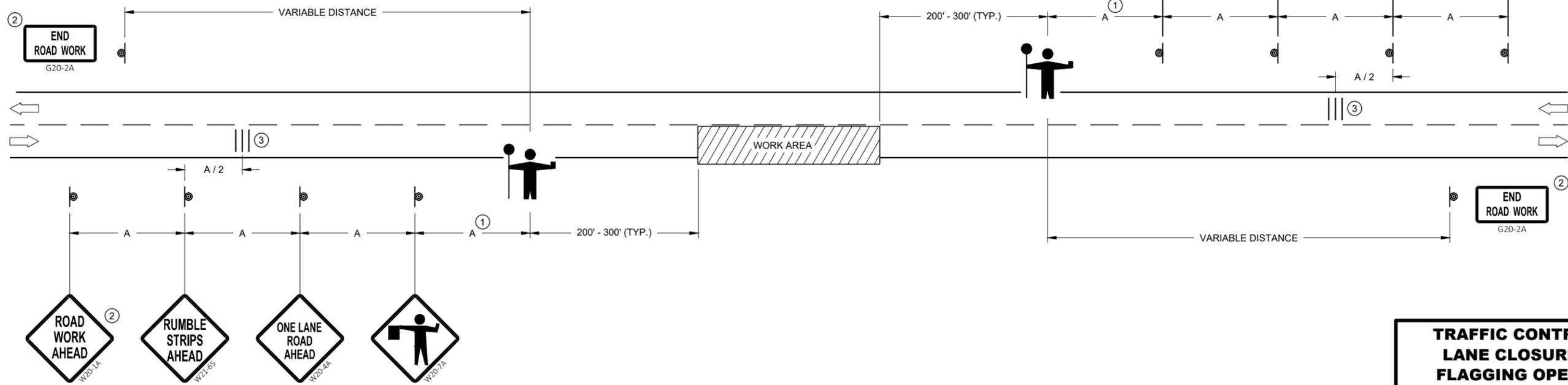
**SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE**

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



W03-4

USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



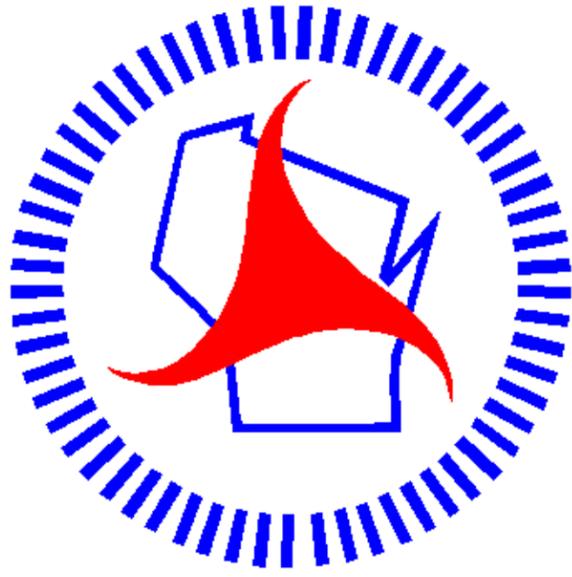
**TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE: May 2022 /S/ Andrew Heidtke  
WORK ZONE ENGINEER

FHWA

Notes



## ***Wisconsin Department of Transportation***

Dedicated people creating transportation solutions through innovation and exceptional service.

<http://www.dot.wisconsin.gov>