

PROJECT
WITH: N/A

1570-06-72

COUNTY:

RUSK

**STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION**

PLAN OF PROPOSED IMPROVEMENT

CAMERON - LADYSMITH

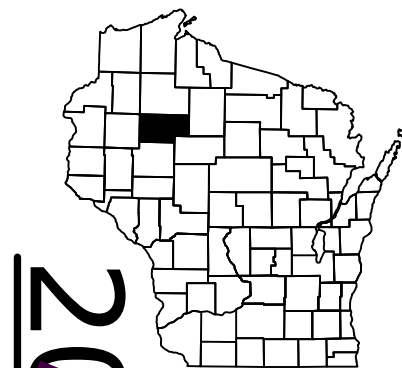
CEDAR SWAMP ROAD TO CHIPPEWA RIVER

USH 8

RUSK COUNTY

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1570-06-72	WISC 2022521	1

STATE PROJECT NUMBER
1570-06-72










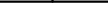



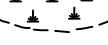

20

DESIGN DESIGNATION

A.A.D.T.	(2022)	=	6300
A.A.D.T.	(2042)	=	7300
D.H.V.		=	---
D.D.		=	60/40
T.		=	28.5%
DESIGN SPEED		=	55 MPH
ESALS		=	1,600,000

CONVENTIONAL SYMBOLS

PLAN

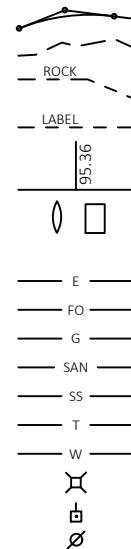
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE

GRADE LINE
ORIGINAL GROUND
MARSH OR ROCK PROFILE
(To be noted as such)
SPECIAL DITCH

GRADE ELEVATION

CULVERT (Profile View)
UTILITIES
ELECTRIC
FIBER OPTIC
GAS
SANITARY SEWER
STORM SEWER
TELEPHONE
WATER
UTILITY PEDESTAL
POWER POLE
TELEPHONE POLE



EXISTING STRUCTURE B-54-0408
PROPOSED STRUCTURE B-54-131
(PROJECT ID 1580-00-70 BY OTHERS)

STRUCTURE B-54-0731

BEGIN PROJECT
STA 485+50.00
Y = 544875.00
X = 727828.19

STRUCTURE B-54-0064

END PROJECT
STA 965+29.00

ORIGINAL PLANS PREPARED BY

AVRES

DATE: 06/13/2022 _____
(Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY	
Surveyor	_____ AYRES ASSOCIATES INC _____
Designer	_____ AYRES ASSOCIATES INC _____
Project Manager	_____ PROJECT MANAGER _____
Regional Examiner	_____ REGIONAL EXAMINER _____
Regional Supervisor	_____ REGIONAL SUPERVISOR _____

APPROVED FOR THE DEPARTMENT
6/13/2022 *Matthew J Dickenson*
DATE: _____ (Signature)

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GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLAN ARE APPROXIMATE. THERE MAY BE OTHER INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN. THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY UTILITY WHICH IS NOT A MEMBER OF THE DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.

EXCAVATION BELOW SUBGRADE (EBS) IS NOT USED TO BALANCE EARTHWORK. EBS IS MEASURED AND PAID FOR AS EXCAVATION COMMON, EXACT LOCATIONS OF EBS WILL BE DETERMINED BY THE ENGINEER.

SHRINKAGE OF EARTHWORK IS VARIABLE. AN AVERAGE FACTOR FOR EXCAVATION COMMON IS 25%.

SEED, INSTALL EROSION MAT, AND FERTILIZE ALL SALVAGED TOPSOIL AREAS WITHIN 7 WORKING DAYS AFTER GRADING WORK IS COMPLETED.

DO NOT APPLY FERTILIZER WITHIN 20 FEET OF A WATER BODY OR WETLAND.

TEMPORARY STORAGE OF ANY EXCAVATED MATERIAL WILL NOT BE PERMITTED IN WETLANDS, FLOODWAY OR FLOODPLAIN OF ANY WETLANDS.

NO TREES OR SHRUBS SHALL BE REMOVED WITHOUT APPROVAL BY THE ENGINEER.

THE EROSION CONTROL ITEMS SHOWN ON THE PLANS ARE AT SUGGESTED LOCATIONS. THE ENGINEER MAY MODIFY LOCATIONS AS NEEDED. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL SUCH TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NECESSARY.

FOR ALL WORK AT CULVERT PIPES APPLY SEED, TEMPORARY SEED, MULCH, AND FERTILIZER TO DISTURBED AREAS OR AS DIRECTED BY THE ENGINEER.

PRIOR TO ORDERING DRAINAGE PIPES AND STRUCTURES, VERIFY RELATED DRAINAGE INFORMATION IN THE PLAN AND PROVIDE DOCUMENTATION TO THE ENGINEER.

INLET AND DISCHARGE ELEVATIONS FOR DRAINAGE STRUCTURES SHOWN ON THE PLAN MAY BE ADJUSTED BY THE ENGINEER TO FIT FIELD CONDITIONS.

INTERSECTION RADII TO MATCH EXISTING. RADII SHOWN ON PLAN IS FOR INFORMATION ONLY. ALL RADII ARE MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE SHOWN OR NOTED ON THE PLAN.

CURB AND GUTTER ELEVATIONS AND RADIUS POINTS ARE ALONG THE FLAG LINE UNLESS OTHERWISE NOTED.

THE EXACT LOCATION AND WIDTH OF DRIVEWAY ENTRANCES WILL BE DETERMINED BY THE FIELD ENGINEER. ALL DRIVEWAYS ARE TO BE REPLACED IN KIND UNLESS OTHERWISE DIRECTED BY THE ENGINEER OR AS SHOWN ON THE PLANS.

EXISTING ELEVATIONS SHALL BE VERIFIED IN THE FIELD.

THE CONTRACTOR IS TO TAKE ALL REASONABLE MEASURES TO ASSURE THAT NO SURVEY MONUMENTS WILL BE DESTROYED, DISTURBED, REMOVED OR BURIED TO THE DEGREE THAT THEY ARE NO LONGER USABLE. THE CONTRACTOR MUST CONTACT A LICENSED LAND SURVEYOR OR LOCAL COUNTY LAND SURVEYOR TO PROTECT, PRESERVE, AND RE-MONUMENT ANY CORNER OF THE PUBLIC LAND SURVEY SYSTEM THAT WOULD BE DESTROYED, DISTURBED, REMOVED OR BURIED DURING THE CONSTRUCTION PROCESS.

PROPOSED ROADWAY IMPROVEMENTS SHALL MATCH EXISTING SUPERELEVATIONS AND ROADWAY CROSS SLOPES.

UTILITY CONTACTS

BRUCE TELEPHONE COMPANY INC - COMMUNICATION LINE
620 NORTH ALVEY STREET
BRUCE, WI 54819
ATTN: JOHN WARNER
OFFICE: 715-403-3267
Jwb101@brucetel.net

SPECTRUM - COMMUNICATION LINE
2304 SOUTH MAIN STREET
RICE LAKE, WI 54868
ATTN: JAMEY OLDEEN
OFFICE: 715-719-0561
CELL: 715-651-7488
jamey.oldeen@charter.com

BEVCOMM - COMMUNICATION LINE
N3767 4TH ATREET
WEYERHAUSER, WI 54895
ATTN: RANDY MONNIER
OFFICE: 715-353-2434
CELL: 715-492-5029
rmonnier@bevcomm.com

BRUCE MUN WATER & SEWER UTILITY - SEWER AND WATER
PO BOX 238
BRUCE, WI 54819-0238
ATTN: DARLENE WUNDROW
OFFICE: 715-868-2188
bruceutility@brucetel.net

WEYERHAEUSER MUNICIPAL WATER UTILITY - SEWER AND WATER
PO BOX 168
WEYERHAEUSER, WI 54895
ATTN: PAUL DACHEL
OFFICE: 715-353-2571
CELL: 715-642-1265
wey watu@bevcomm.net

WE ENERGIES - GAS/PETROLEUM
104 WEST SOUTH STREET
RICE LAKE, WI 54868
ATTN: STEVEN CHAVERS
OFFICE: 715-234-9605
CELL: 715-213-4327
steven.chavers@we-energies.com

BARRON ELECTRIC COOPERATIVE - ELECTRICITY
1434 NORTH STH 25
BARRON, WI 54812
ATTN: JEFF NELSON
OFFICE: 715-537-3171
CELL: 715-418-1167
jnelson@barronelectric.com

XCEL ENERGY - ELECTRICTY
310 HICKORY HILLS LANE
PHILLIPS, WI 54555
ATTN: ANDY HALOPKA
OFFICE: 715-737-1183
CELL: 715-316-1356
andrew.a.halopka@xcelenergy.com

DNR CONTACT

DNR WEST CENTRAL REGOIN HQ
1300 WEST CLAIRMONT AVENUE
EAU CLAIRE, WI 54701
ATTN: LEAH NICOL
715-934-9014
LEAH.NICOL@WISCONSIN.GOV

DESIGN CONTACT

AYRES ASSOCIATES INC
3433 OAKWOOD HILLS PARKWAY
EAU CLAIRE, WI 54701
ATTN: JEFF ABRAMSON
715-834-3161
ABRAMSONJ@AYRESASSOCIATES.COM

WISDOT DESIGN CONTACT

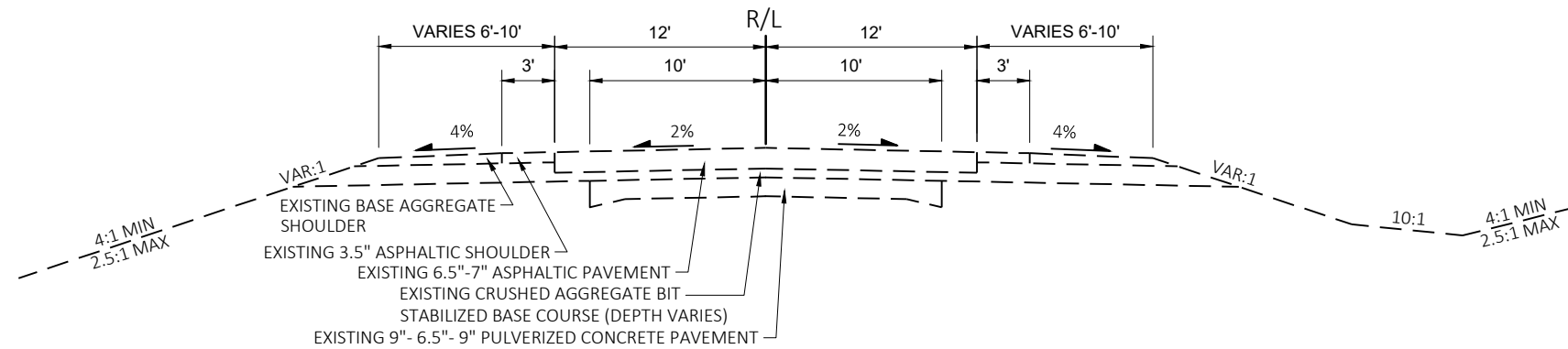
DEPARTMENT OF TRANSPORTATION
1701 NORTH 4TH STREET
SUPERIOR, WI 54880
ATTN: MATTHEW DICKENSON, PE
715-395-3022
MATTHEW.DICKENSON@DOT.WI.GOV

RAILROAD CONTACT

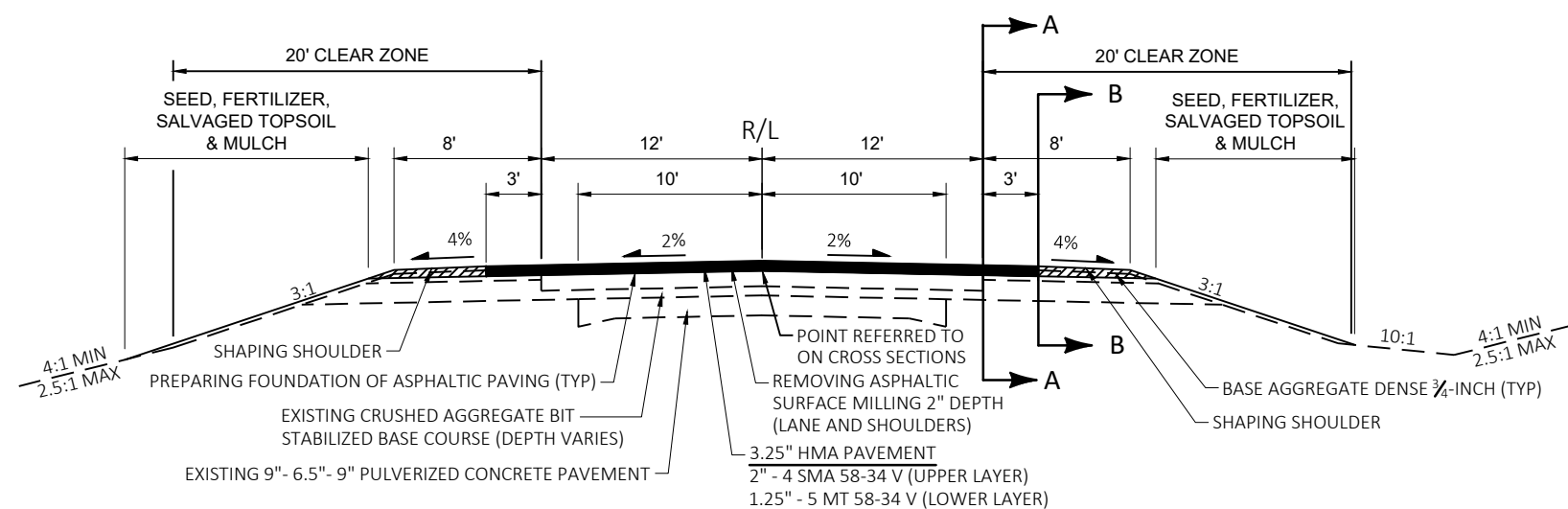
CN RAILROAD
3192 S. POKEGAMA ROAD
SUPERIOR, WI 54880
ATTN: JACKIE SAP, MANAGER PUBLIC WORKS
715-345-2503
JACKIE.SAPP@CN.CA



Dial 811 or (800)242-8511
www.DiggersHotline.com

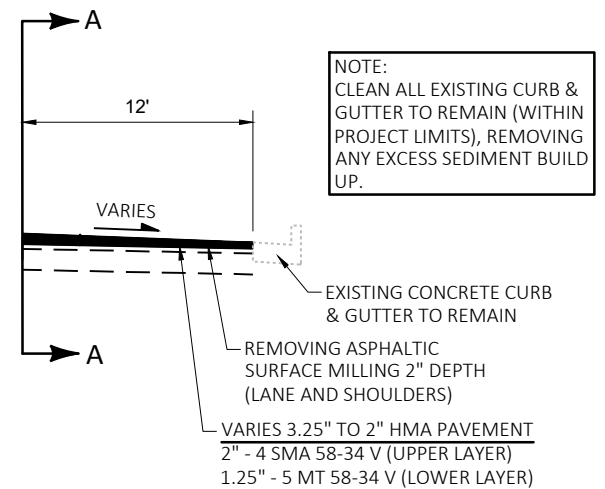


EXISTING TYPICAL SECTION - USH 8



FINISHED TYPICAL SECTION - USH 8

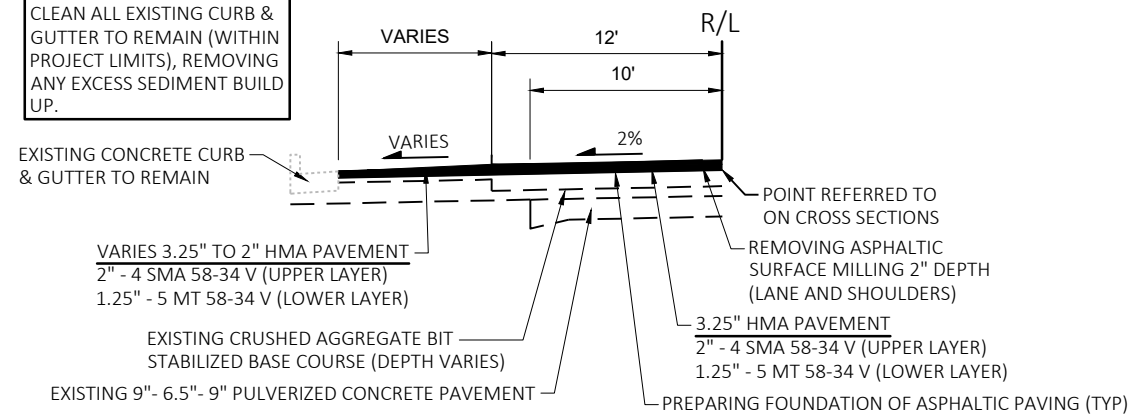
STA 485+50.00 - STA 922+75.00
STA 935+75.00 - STA 965+29.00



FINISHED TYPICAL SECTION - USH 8

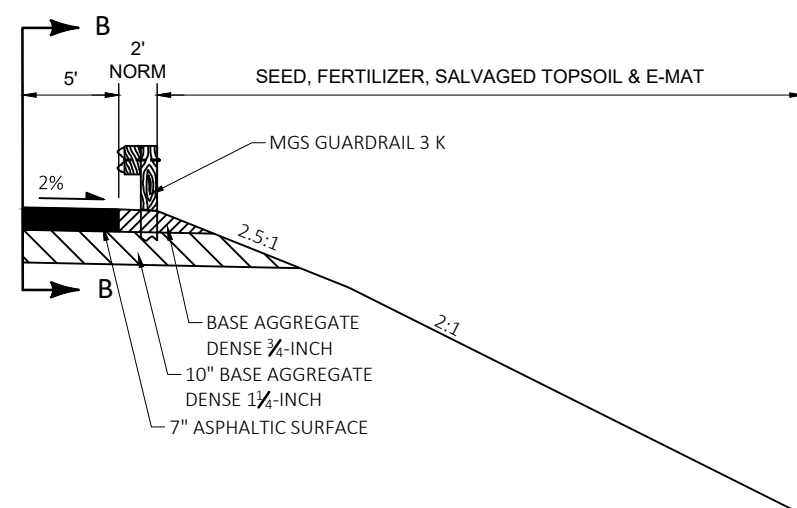
STA 549+50.00 - STA 551+00.00
(STATION RANGE IS APPROXIMATE,
CURB & GUTTER COVERED IN SAND)

NOTE:
CLEAN ALL EXISTING CURB &
GUTTER TO REMAIN (WITHIN
PROJECT LIMITS), REMOVING
ANY EXCESS SEDIMENT BUILD
UP.



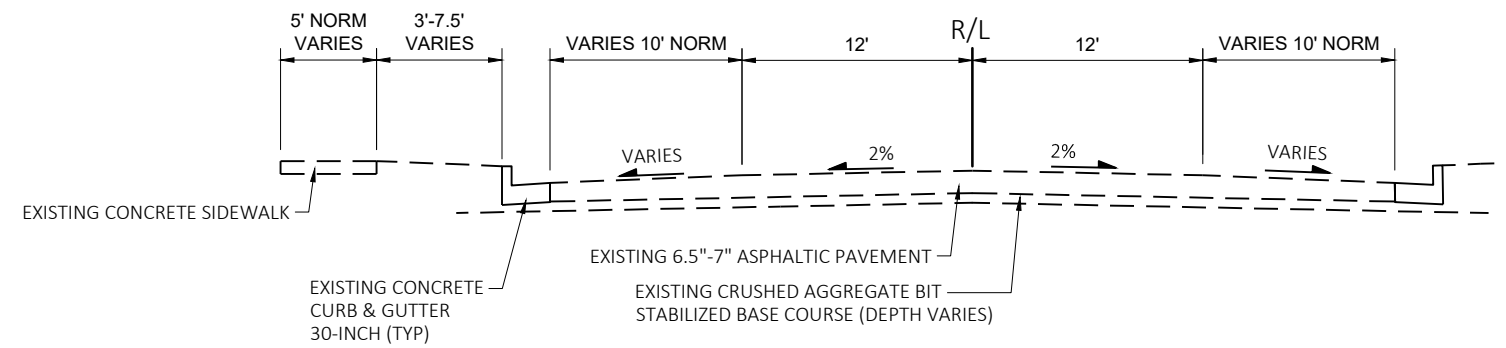
FINISHED TYPICAL SECTION - USH 8

STA 561+35.00 - STA 564+40.00, LT
USH 8 & NORWEIGAN RD (NW QUAD)
USH 8 & STH 40 NORTH RADII
STA 940+97.00 - STA 944+29.00, LT



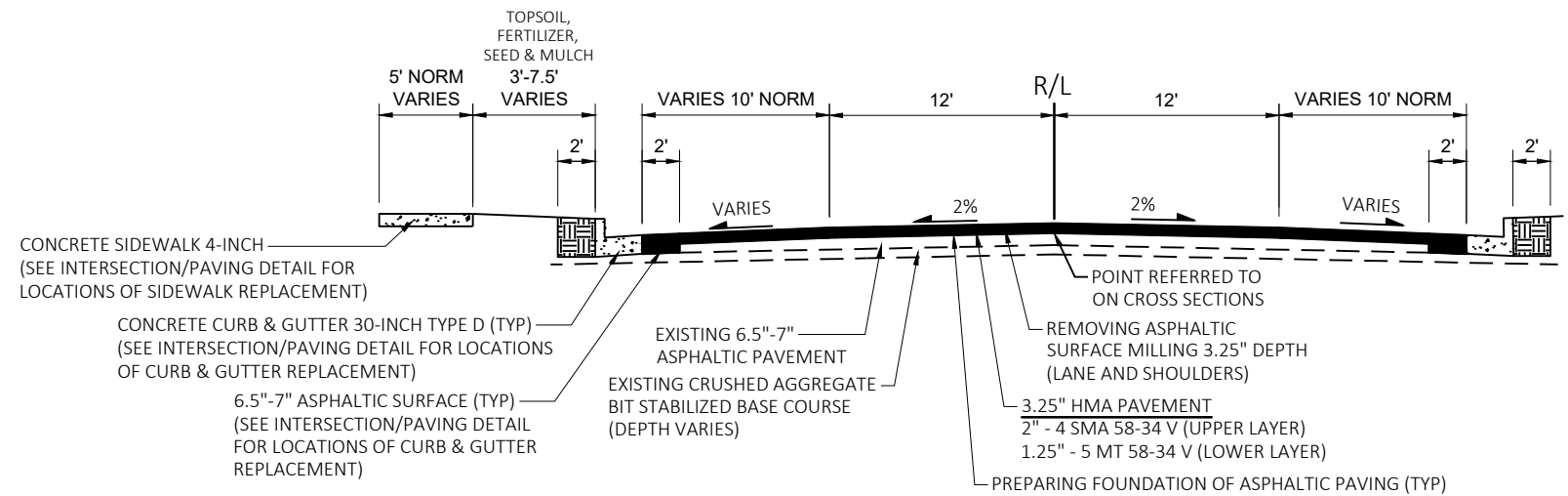
FINISHED GUARDRAIL TYPICAL SECTION - USH 8

STA 503+65.85 - STA 507+84.60



EXISTING TYPICAL URBAN SECTION - USH 8

STA 922+75.00 - STA 935+75.00



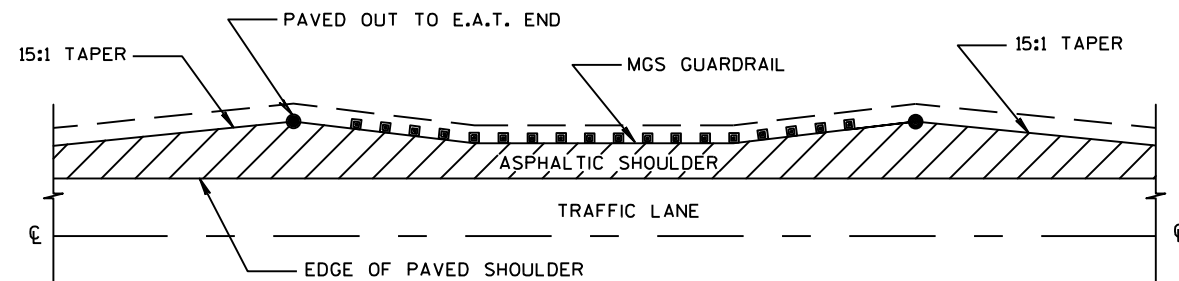
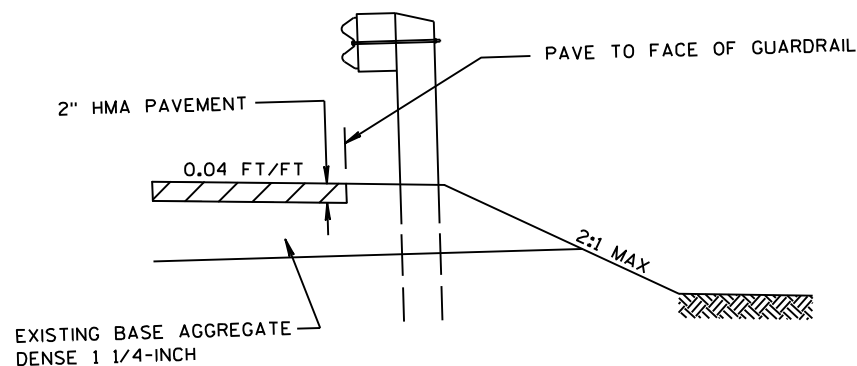
FINISHED TYPICAL URBAN SECTION - USH 8

STA 922+75.00 - STA 935+75.00

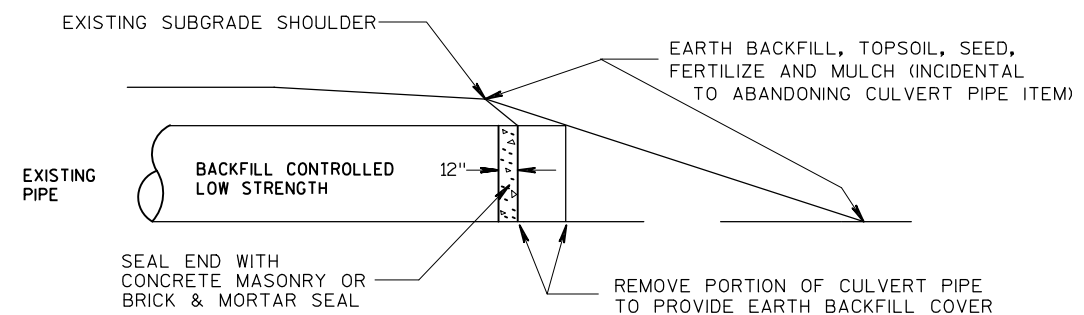
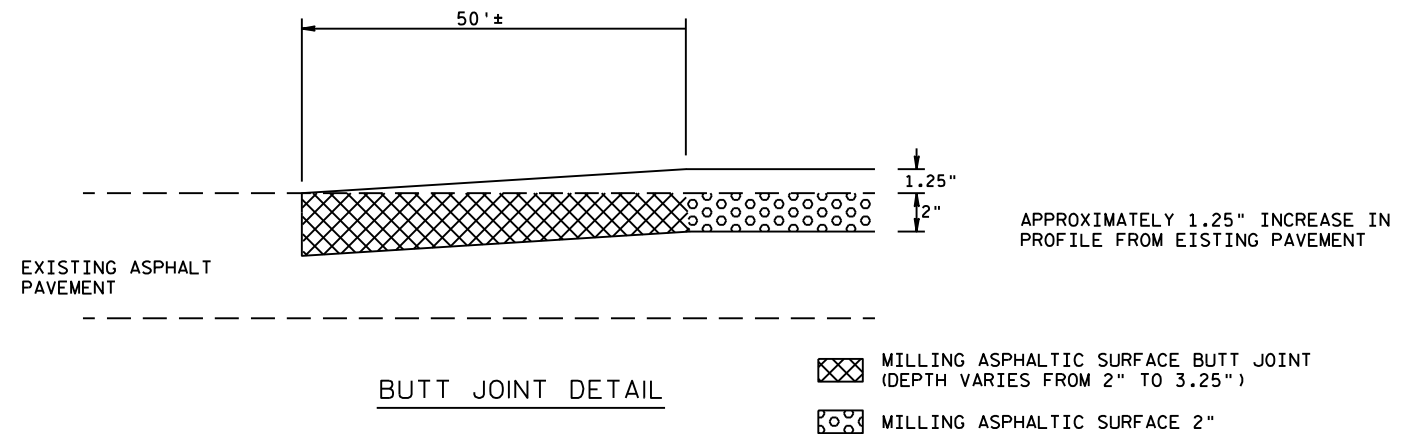
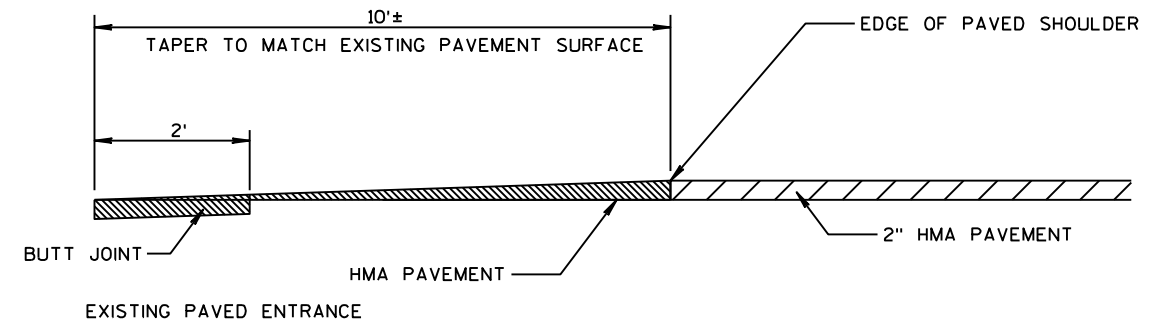
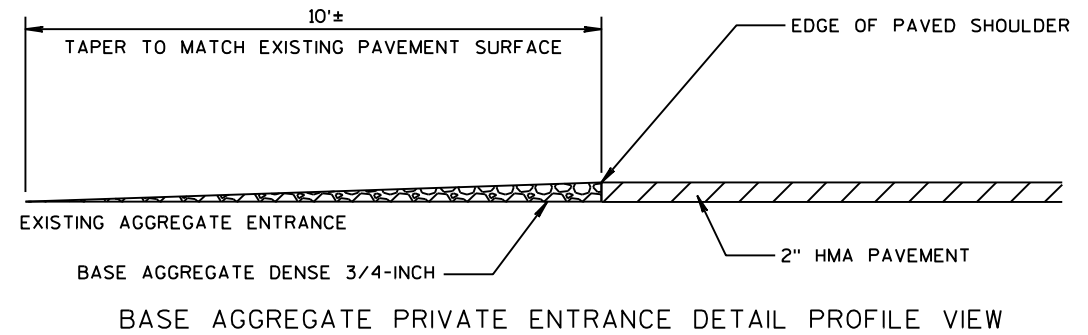
NOTE:
CLEAN ALL EXISTING CURB & GUTTER TO REMAIN (WITHIN PROJECT LIMITS), REMOVING ANY EXCESS SEDIMENT BUILD UP.

RUNOFF COEFFICIENT TABLE

A	HYDROLOGIC SOIL GROUP											
	B C						D					
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08	.16	.22	.12	.20	.27	.15	.24	.33	.19	.28	.38
	.22	.30	.38	.26	.34	.44	.30	.37	.50	.34	.41	.56
MEDIAN STRIP-TURF	.19	.20	.24	.19	.22	.26	.20	.23	.30	.20	.25	.30
	.24	.26	.30	.25	.28	.33	.26	.30	.37	.27	.32	.40
SIDE SLOPE: TURF			.25			.27			.28			.30
			.32			.34			.36			.38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

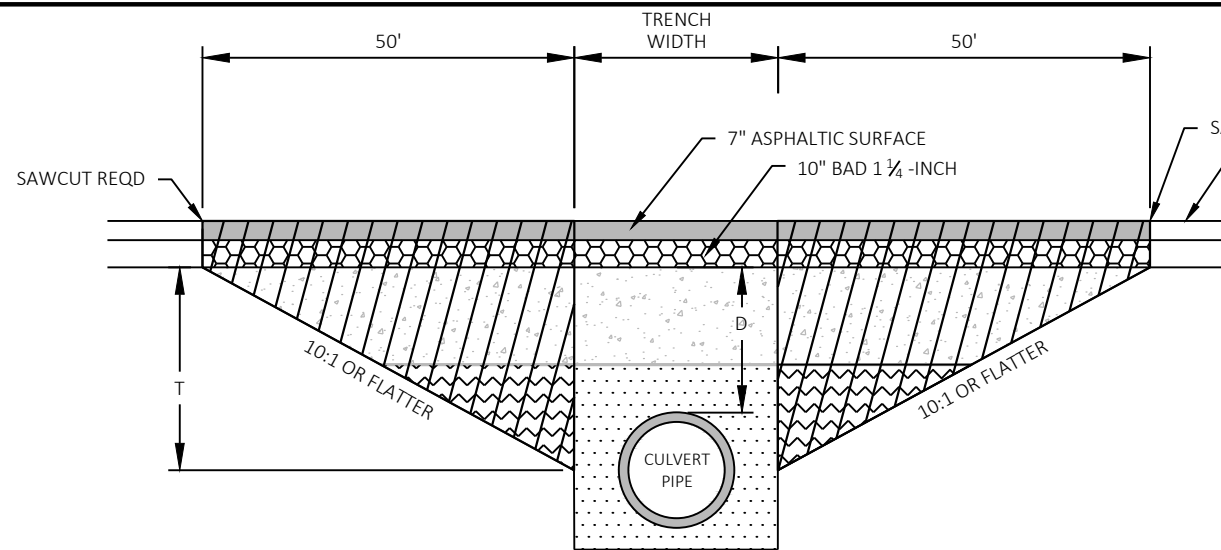
TOTAL PROJECT AREA = 55 ACRESTOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 8 ACRES

ASPHALTIC PAVING ALONG BEAMGUARD

STA 503+66 - STA 507+85, RT
STA 962+84 - STA 965+36, LT
STA 963+47 - STA 965+36, RT

ABANDONING CULVERT PIPE DETAIL

NOTE: SEE PLAN DETAILS FOR LOCATIONS.



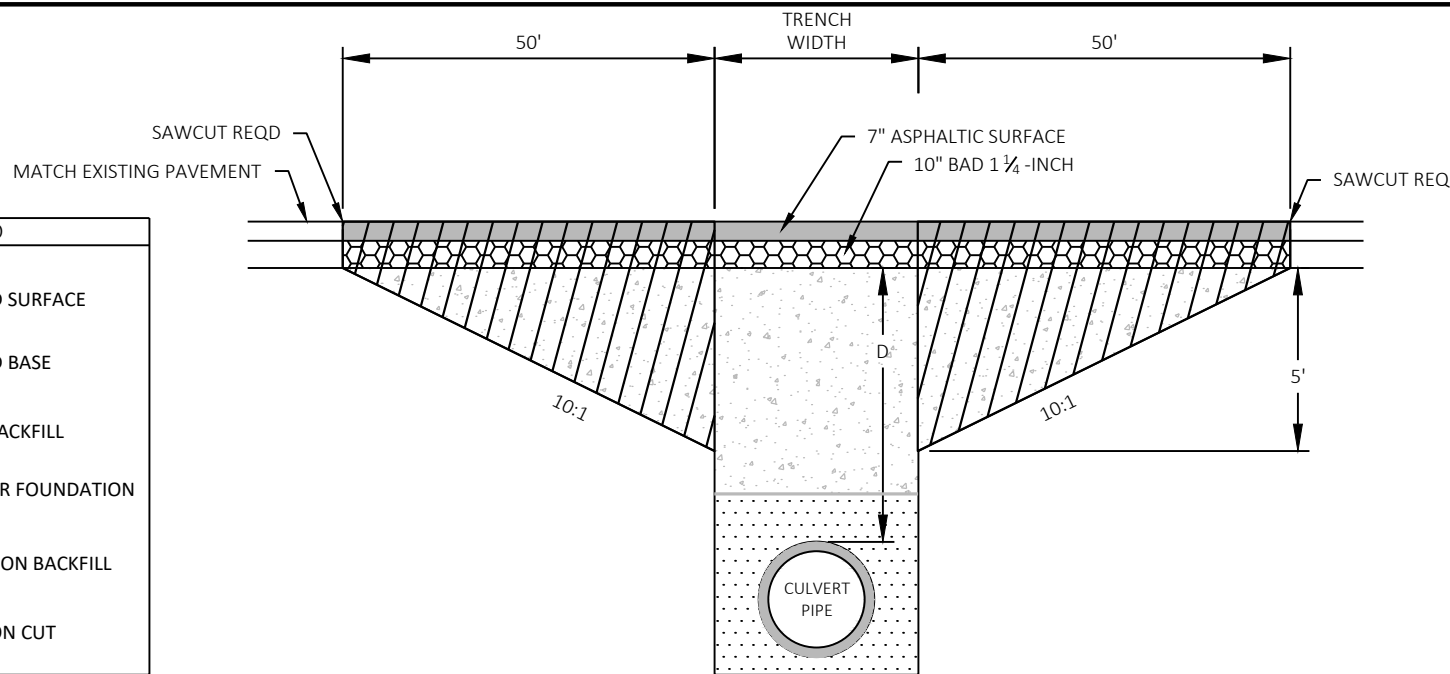
DEPTH D < 6 FT

TRANSITION CUT DEPTH (T) = THE LESSER OF DEPTH TO CENTER OF PIPE OR 5 FT.
DO NOT EXTEND TRANSITION CUT BELOW HORIZONTAL CENTER OF PIPE.

CULVERT PIPE TRANSITION

STA 493+21
STA 540+59
STA 559+00
STA 568+98
STA 593+98
STA 626+43
STA 626+48
STA 668+40
STA 708+12
STA 822+09
STA 938+00

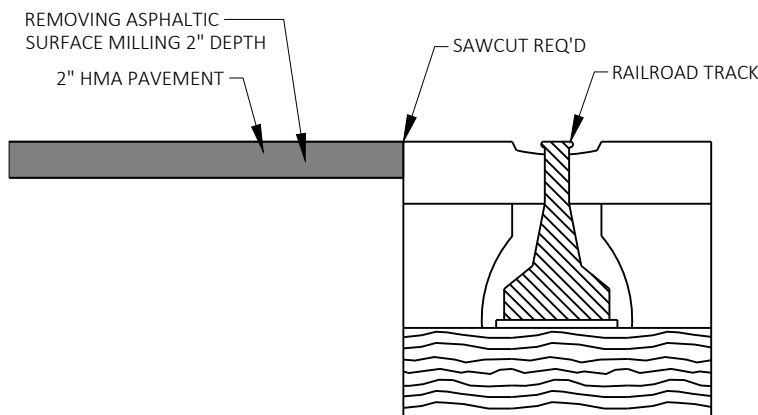
LEGEND	
	PROPOSED SURFACE
	PROPOSED BASE
	TRENCH BACKFILL
	TRENCH OR FOUNDATION BACKFILL
	FOUNDATION BACKFILL
	TRANSITION CUT



DEPTH D ≥ 6 FT

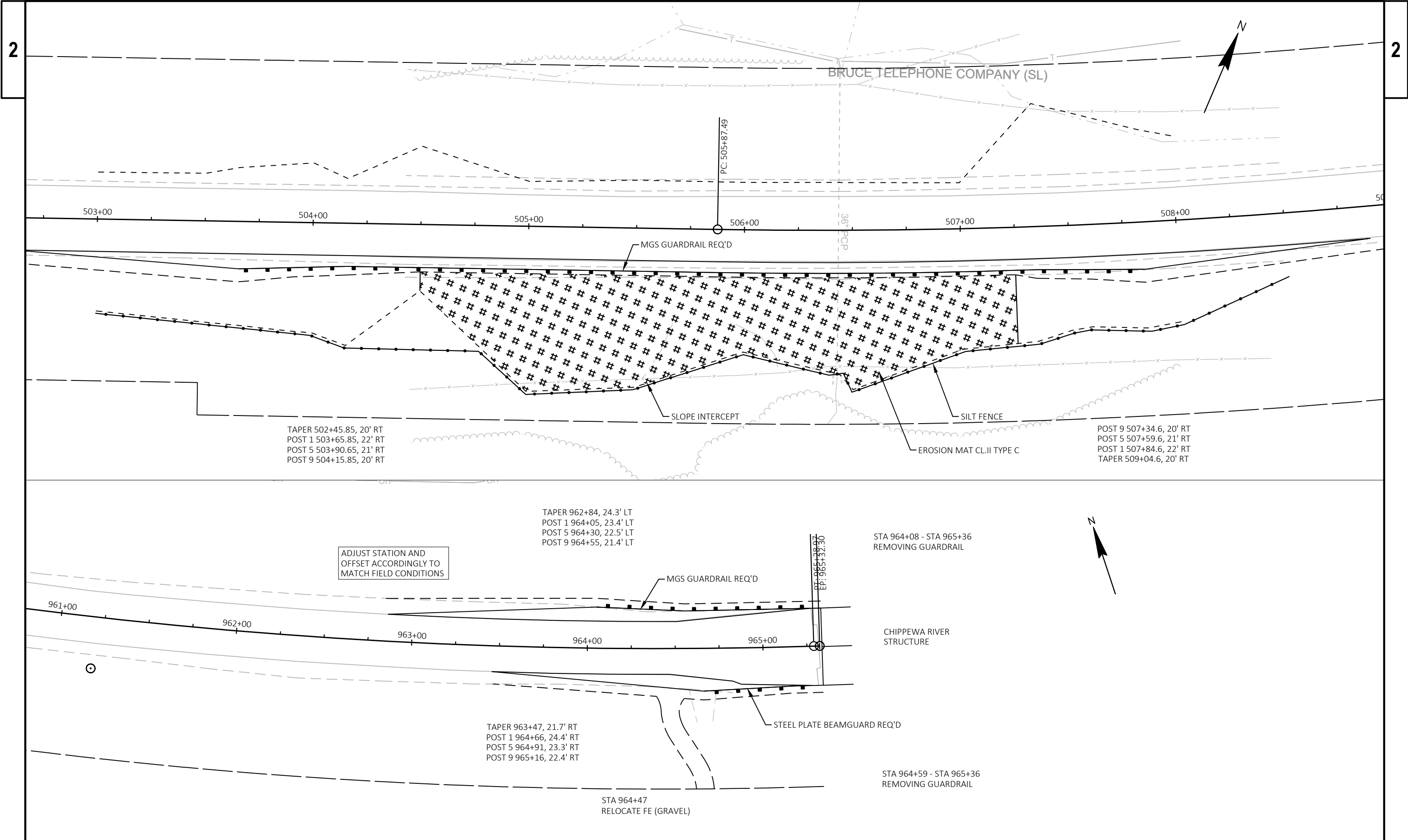
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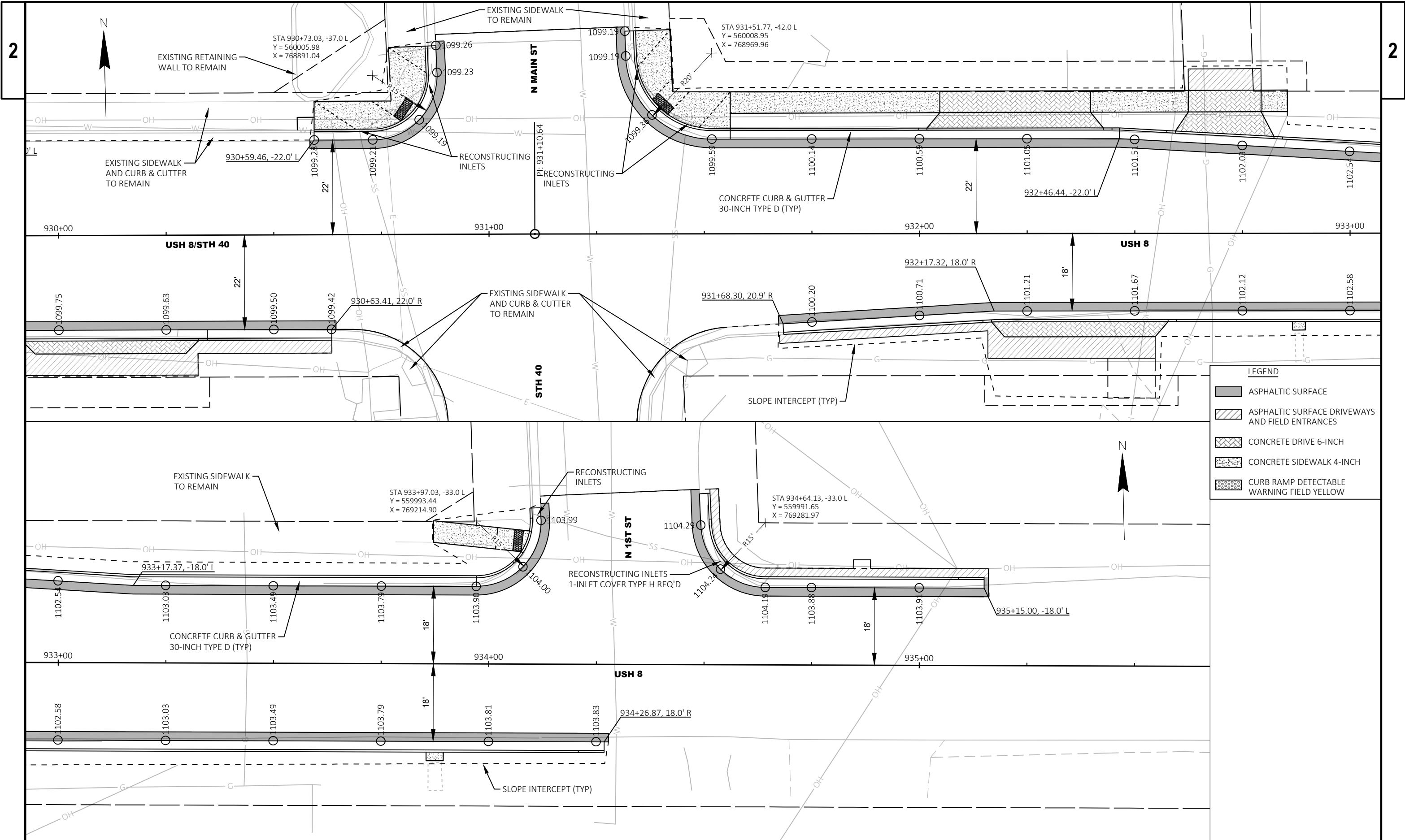
TRANSITION CUT IS PAID AS EXCAVATION COMMON.
TRANSITION CUT WIDTH IS FROM SUBGRADE SHOULDER POINT TO SUBGRADE SHOULDER POINT.
BACKFILL THE TRANSITION CUT AREAS WITH FOUNDATION AND TRENCH BACKFILL AS SPECIFIED IN STANDARD SPEC 520.
PERFORM CULVERT PIPE INSTALLATION BEFORE MILLING AND PAVING.
PLACE BASE AGGREGATE AND ASPHALTIC SURFACE AFTER CULVERT PIPE INSTALLATION AND BEFORE MILLING AND PAVING OPERATION.

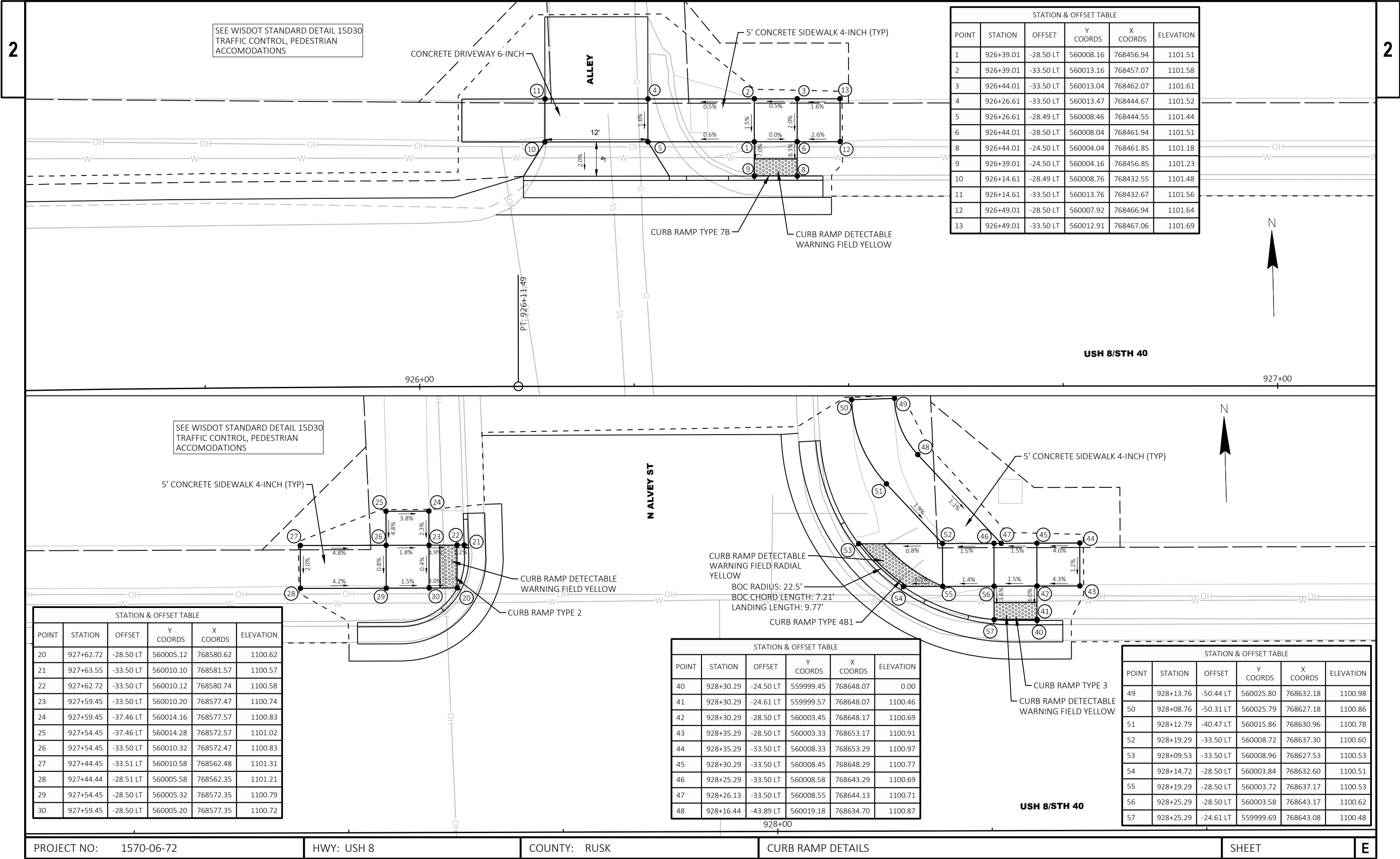


PAVING DETAIL AT RAILROAD CROSSING

HUTCHINSON RD
KIEF RD
GURESKI LN
ZEBRO RD







SEE WISDOT STANDARD DETAIL 15D30
TRAFFIC CONTROL, PEDESTRIAN
ACCOMMODATIONS

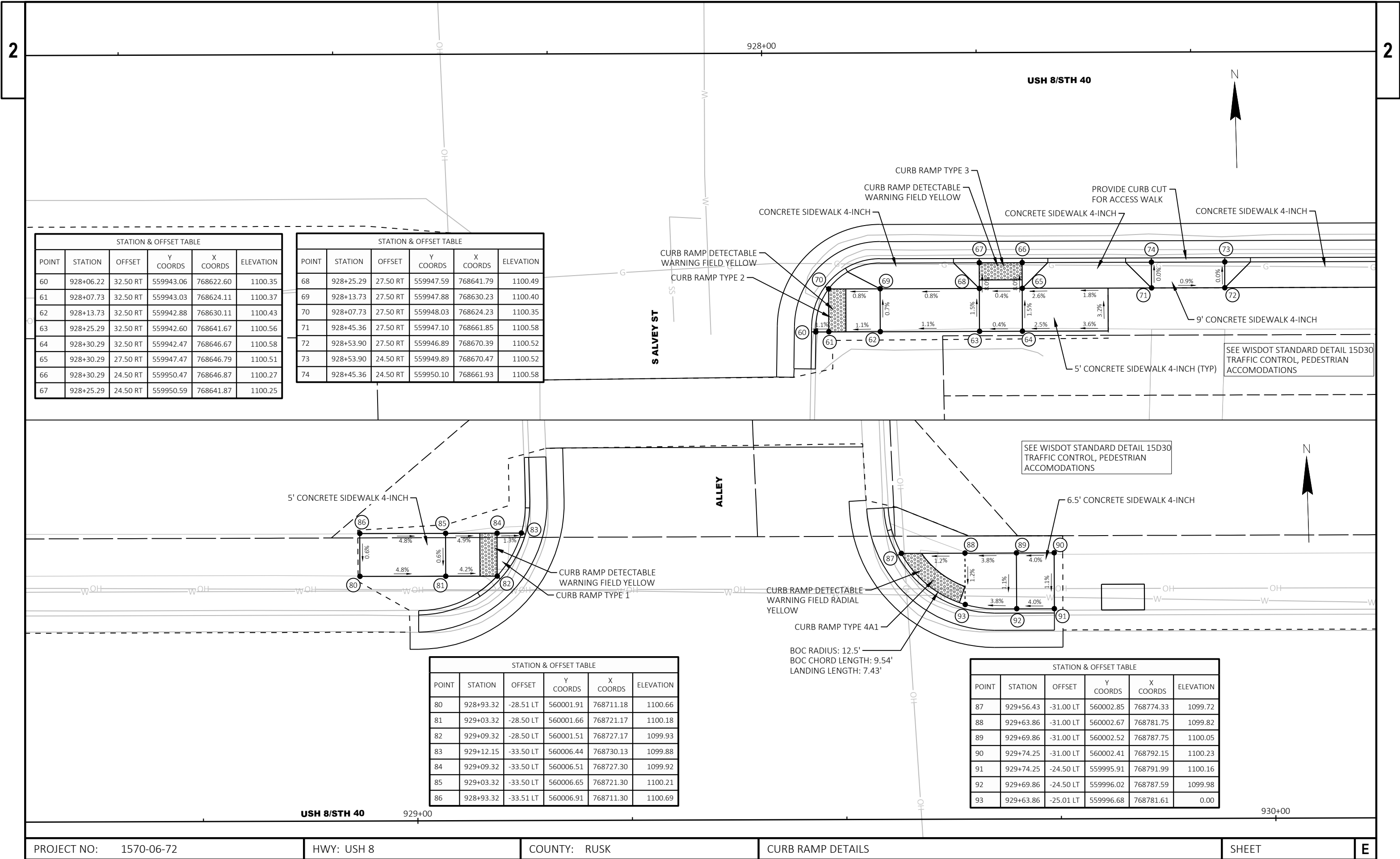
STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
1	926+39.01	-28.50 LT	560008.16	768456.94	1101.51
2	926+39.01	-33.50 LT	560013.16	768457.07	1101.58
3	926+44.01	-33.50 LT	560013.04	768462.07	1101.61
4	926+26.61	-33.50 LT	560013.47	768444.67	1101.52
5	926+26.61	-28.49 LT	560008.46	768444.55	1101.44
6	926+44.01	-28.50 LT	560008.04	768461.94	1101.51
8	926+44.01	-24.50 LT	560004.04	768461.85	1101.18
9	926+39.01	-24.50 LT	560004.16	768456.85	1101.23
10	926+14.61	-28.49 LT	560008.76	768432.55	1101.48
11	926+14.61	-33.50 LT	560013.76	768432.67	1101.56
12	926+49.01	-28.50 LT	560007.92	768466.94	1101.64
13	926+49.01	-33.50 LT	560012.91	768467.06	1101.69

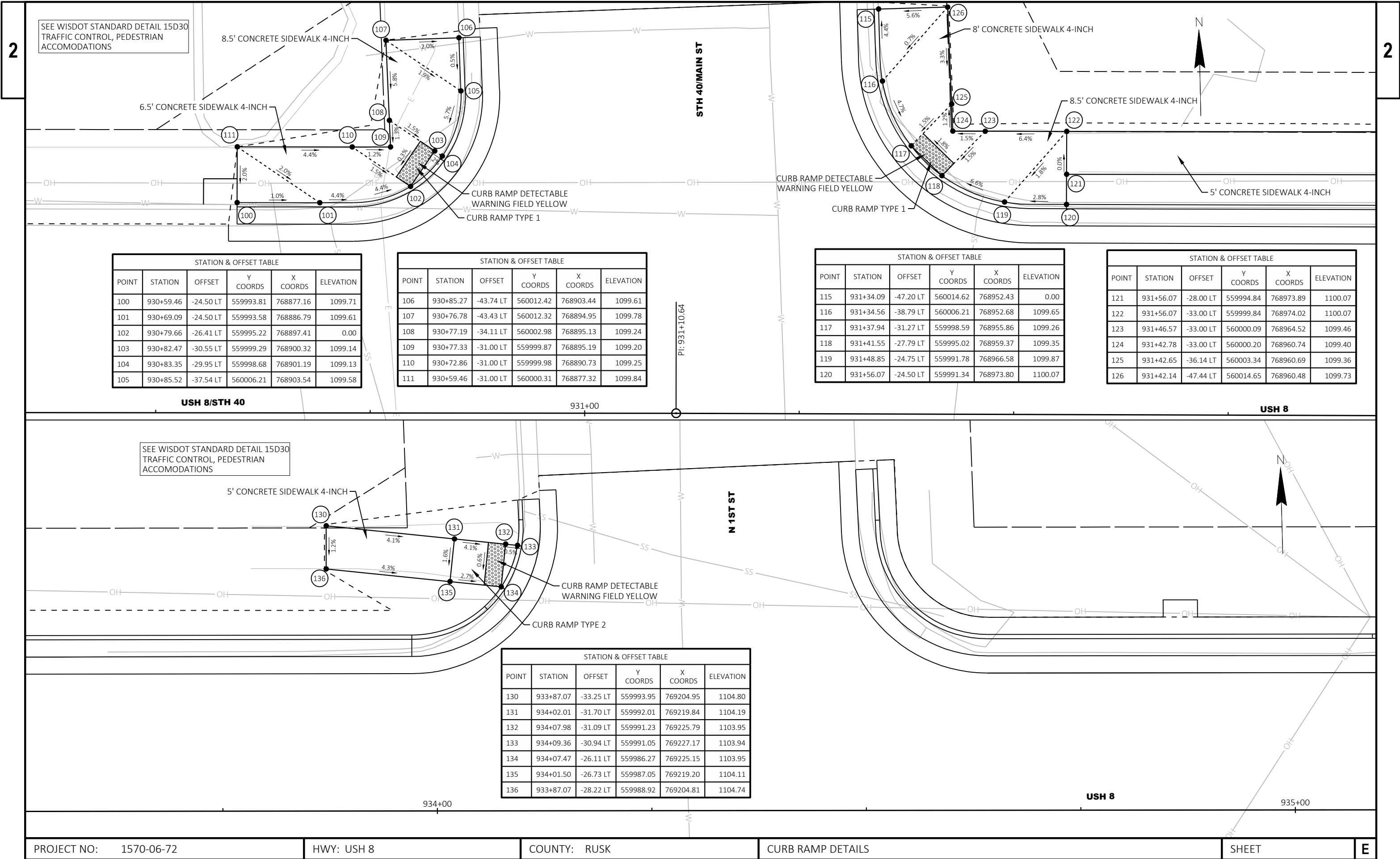
SEE WISDOT STANDARD DETAIL 15D30
TRAFFIC CONTROL, PEDESTRIAN
ACCOMMODATIONS

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
20	927+62.72	-28.50 LT	560005.12	768580.62	1100.62
21	927+63.55	-33.50 LT	560010.10	768581.57	1100.57
22	927+62.72	-33.50 LT	560010.12	768580.74	1100.58
23	927+59.45	-33.50 LT	560010.20	768577.47	1100.74
24	927+59.45	-37.46 LT	560014.16	768577.57	1100.83
25	927+54.45	-37.46 LT	560014.28	768572.57	1101.02
26	927+54.45	-33.50 LT	560010.32	768572.47	1100.83
27	927+44.45	-33.51 LT	560010.58	768562.48	1101.31
28	927+44.44	-28.51 LT	560005.58	768562.35	1101.21
29	927+54.45	-28.50 LT	560005.32	768572.35	1100.79
30	927+59.45	-28.50 LT	560005.20	768577.35	1100.72

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
40	928+30.29	-24.50 LT	559999.45	768648.07	0.00
41	928+30.29	-24.61 LT	559999.57	768648.07	1100.46
42	928+30.29	-28.50 LT	560003.45	768648.17	1100.69
43	928+35.29	-28.50 LT	560003.33	768653.17	1100.91
44	928+35.29	-33.50 LT	560008.33	768653.29	1100.97
45	928+30.29	-33.50 LT	560008.45	768648.29	1100.77
46	928+25.29	-33.50 LT	560008.58	768643.29	1100.69
47	928+26.13	-33.50 LT	560008.55	768644.13	1100.71
48	928+16.44	-43.89 LT	560019.18	768634.70	1100.87

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
49	928+13.76	-50.44 LT	560025.80	768632.18	1100.98
50	928+08.76	-50.31 LT	560025.79	768627.18	1100.86
51	928+12.79	-40.47 LT	560015.86	768630.96	1100.78
52	928+19.29	-33.50 LT	560008.72	768637.30	1100.60
53	928+09.53	-33.50 LT	560008.96	768627.53	1100.53
54	928+14.72	-28.50 LT	560003.84	768632.60	1100.51
55	928+19.29	-28.50 LT	560003.72	768637.17	1100.53
56	928+25.29	-28.50 LT	560003.58	768643.17	1100.62
57	928+25.29	-24.61 LT	559999.69	768643.08	1100.48





Estimate Of Quantities

1570-06-72

Line	Item	Item Description	Unit	Total	Qty
0002	201.0105	Clearing	STA	18.000	18.000
0004	201.0205	Grubbing	STA	18.000	18.000
0006	203.0100	Removing Small Pipe Culverts	EACH	9.000	9.000
0008	203.0220	Removing Structure (structure) 01. STA 493+21	EACH	1.000	1.000
0010	204.0100	Removing Concrete Pavement	SY	76.000	76.000
0012	204.0115	Removing Asphaltic Surface Butt Joints	SY	1,405.000	1,405.000
0014	204.0120	Removing Asphaltic Surface Milling	SY	163,485.000	163,485.000
0016	204.0150	Removing Curb & Gutter	LF	1,108.000	1,108.000
0018	204.0155	Removing Concrete Sidewalk	SY	223.000	223.000
0020	204.0165	Removing Guardrail	LF	205.000	205.000
0022	204.0170	Removing Fence	LF	225.000	225.000
0024	204.0270	Abandoning Culvert Pipes	EACH	3.000	3.000
0026	205.0100	Excavation Common	CY	2,027.000	2,027.000
0028	208.0100	Borrow	CY	6,097.000	6,097.000
0030	208.1500.S	Temporary Lane Shift During Culvert Work	EACH	20.000	20.000
0032	211.0100	Prepare Foundation for Asphaltic Paving (project) 01. 1570-06-72	LS	1.000	1.000
0034	211.0400	Prepare Foundation for Asphaltic Shoulders	STA	2.000	2.000
0036	213.0100	Finishing Roadway (project) 01. 1570-06-72	EACH	1.000	1.000
0038	305.0110	Base Aggregate Dense 3/4-Inch	TON	6,870.000	6,870.000
0040	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	2,756.000	2,756.000
0042	305.0500	Shaping Shoulders	STA	48.000	48.000
0044	416.0160	Concrete Driveway 6-Inch	SY	104.000	104.000
0046	455.0605	Tack Coat	GAL	11,550.000	11,550.000
0048	460.0105.S	HMA Percent Within Limits (PWL) Test Strip Volumetrics	EACH	1.000	1.000
0050	460.0110.S	HMA Percent Within Limits (PWL) Test Strip Density	EACH	1.000	1.000
0052	460.0115.S	HMA Pavement Test Strips Volumetrics	EACH	1.000	1.000
0054	460.0120.S	HMA Pavement Test Strips Density	EACH	1.000	1.000
0056	460.2000	Incentive Density HMA Pavement	DOL	12,780.000	12,780.000
0058	460.2005	Incentive Density PWL HMA Pavement	DOL	9,060.000	9,060.000
0060	460.2007	Incentive Density HMA Pavement Longitudinal Joints	DOL	48,450.000	48,450.000
0062	460.2010	Incentive Air Voids HMA Pavement	DOL	12,490.000	12,490.000
0064	460.6645	HMA Pavement 5 MT 58-34 V	TON	12,362.000	12,362.000
0066	460.8644	HMA Pavement 4 SMA 58-34 V	TON	19,760.000	19,760.000
0068	460.9000.S	Material Transfer Vehicle (project) 01. 1570-06-72	EACH	1.000	1.000
0070	465.0105	Asphaltic Surface	TON	1,578.000	1,578.000
0072	465.0110	Asphaltic Surface Patching	TON	200.000	200.000
0074	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	107.000	107.000
0076	465.0475	Asphalt Centerline Rumble Strips 2-Lane Rural	LF	31,350.000	31,350.000
0078	522.0124	Culvert Pipe Reinforced Concrete Class III 24-Inch	LF	532.000	532.000
0080	522.0130	Culvert Pipe Reinforced Concrete Class III 30-Inch	LF	308.000	308.000
0082	522.0148	Culvert Pipe Reinforced Concrete Class III 48-Inch	LF	110.000	110.000
0084	522.1024	Apron Endwalls for Culvert Pipe Reinforced Concrete 24-Inch	EACH	14.000	14.000
0086	522.1030	Apron Endwalls for Culvert Pipe Reinforced Concrete 30-Inch	EACH	6.000	6.000
0088	522.1048	Apron Endwalls for Culvert Pipe Reinforced Concrete 48-Inch	EACH	2.000	2.000
0090	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	1,100.000	1,100.000
0092	602.0405	Concrete Sidewalk 4-Inch	SF	2,364.000	2,364.000
0094	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	208.000	208.000
0096	611.0430	Reconstructing Inlets	EACH	7.000	7.000
0098	611.0624	Inlet Covers Type H	EACH	1.000	1.000

Estimate Of Quantities

1570-06-72

Line	Item	Item Description	Unit	Total	Qty
0100	614.0010	Barrier System Grading Shaping Finishing	EACH	2.000	2.000
0102	614.0200	Steel Thrie Beam Structure Approach	LF	20.600	20.600
0104	614.0370	Steel Plate Beam Guard Energy Absorbing Terminal	EACH	1.000	1.000
0106	614.2300	MGS Guardrail 3	LF	37.500	37.500
0108	614.2330	MGS Guardrail 3 K	LF	312.500	312.500
0110	614.2500	MGS Thrie Beam Transition	LF	39.400	39.400
0112	614.2610	MGS Guardrail Terminal EAT	EACH	3.000	3.000
0114	618.0100	Maintenance And Repair of Haul Roads (project) 01. 1570-06-72	EACH	1.000	1.000
0116	619.1000	Mobilization	EACH	1.000	1.000
0118	624.0100	Water	MGAL	140.000	140.000
0120	625.0100	Topsoil	SY	285.000	285.000
0122	625.0500	Salvaged Topsoil	SY	12,322.000	12,322.000
0124	627.0200	Mulching	SY	12,825.000	12,825.000
0126	628.1504	Silt Fence	LF	2,851.000	2,851.000
0128	628.1520	Silt Fence Maintenance	LF	2,851.000	2,851.000
0130	628.1905	Mobilizations Erosion Control	EACH	2.000	2.000
0132	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0134	628.2027	Erosion Mat Class II Type C	SY	2,290.000	2,290.000
0136	628.7555	Culvert Pipe Checks	EACH	32.000	32.000
0138	629.0210	Fertilizer Type B	CWT	10.000	10.000
0140	630.0120	Seeding Mixture No. 20	LB	418.000	418.000
0142	630.0140	Seeding Mixture No. 40	LB	6.000	6.000
0144	630.0500	Seed Water	MGAL	354.000	354.000
0146	633.5200	Markers Culvert End	EACH	22.000	22.000
0148	634.0612	Posts Wood 4x6-Inch X 12-FT	EACH	14.000	14.000
0150	637.2210	Signs Type II Reflective H	SF	42.000	42.000
0152	642.5001	Field Office Type B	EACH	1.000	1.000
0154	643.0300	Traffic Control Drums	DAY	300.000	300.000
0156	643.0900	Traffic Control Signs	DAY	10,950.000	10,950.000
0158	643.5000	Traffic Control	EACH	1.000	1.000
0160	644.1420	Temporary Pedestrian Surface Plywood	SF	1,600.000	1,600.000
0162	644.1601	Temporary Pedestrian Curb Ramp	DAY	230.000	230.000
0164	646.1020	Marking Line Epoxy 4-Inch	LF	53,858.000	53,858.000
0166	646.1040	Marking Line Grooved Wet Ref Epoxy 4-Inch	LF	86,940.000	86,940.000
0168	646.3020	Marking Line Epoxy 8-Inch	LF	725.000	725.000
0170	646.4520	Marking Line Same Day Epoxy 4-Inch	LF	48,060.000	48,060.000
0172	646.6020	Marking Stop Line Epoxy 12-Inch	LF	119.000	119.000
0174	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	260.000	260.000
0176	646.7520	Marking Crosswalk Epoxy Block Style 24-Inch	LF	216.000	216.000
0178	649.0120	Temporary Marking Line Epoxy 4-Inch	LF	38,000.000	38,000.000
0180	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	1,100.000	1,100.000
0182	650.6000	Construction Staking Pipe Culverts	EACH	11.000	11.000
0184	650.8000	Construction Staking Resurfacing Reference	LF	47,979.000	47,979.000
0186	650.9000	Construction Staking Curb Ramps	EACH	11.000	11.000
0188	650.9910	Construction Staking Supplemental Control (project) 01. 1570-06-72	LS	1.000	1.000
0190	690.0150	Sawing Asphalt	LF	3,623.000	3,623.000
0192	690.0250	Sawing Concrete	LF	686.000	686.000
0194	740.0440	Incentive IRI Ride	DOL	36,720.000	36,720.000
0196	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	2,400.000	2,400.000

Estimate Of Quantities

					1570-06-72
0198	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	2,000.000	2,000.000

CLEARING & GRUBBING

				201.0105 CLEARING	201.0205 GRUBBING
STATION	TO	STATION	LOCATION	STA	STA
555+00	-	560+00	USH 8	5	5
592+00	-	595+00	USH 8	3	3
625+00	-	629+00	USH 8	4	4
668+00	-	669+00	USH 8	1	1
821+00	-	823+00	USH 8	2	2
937+00	-	940+00	USH 8	3	3
TOTAL 0010				18	18

REMOVING SMALL PIPE CULVERTS

				203.0100 REMOVING SMALL PIPE CULVERTS
STATION	LOCATION	SIZE	MATERIAL	EACH
540+59	USH 8	48"	RCCP	1
568+98	USH 8	24"	RCCP	1
593+98	USH 8	24"	RCCP	1
626+43	USH 8	30"	RCCP	1
626+48	USH 8	30"	RCCP	1
668+40	USH 8	30"	RCCP	1
708+12	USH 8	24"	RCCP	1
822+09	USH 8	24"	RCCP	1
938+00	USH 8	24"	RCCP	1
TOTAL 0010				9

REMOVING STRUCTURE (STA 493+21)

		203.0220.01 REMOVING STRUCTURE (STRUCTURE) (01. STA 493+21)
STATION	LOCATION	EACH
493+21	USH 8	1
TOTAL 0010		1

REMOVING CONCRETE PAVEMENT

				204.0100 REMOVING CONCRETE PAVEMENT
STATION	TO	STATION	LOCATION	SY
931+57	-	932+43	USH 8, LT	76
TOTAL 0010				76

REMOVING ASPHALTIC SURFACE BUTT JOINTS

		204.0115 REMOVING ASPHALTIC SURFACE BUTT JOINTS
STATION	LOCATION	SY
485+50	USH 8	179
527+33	USH 8	245
528+77	USH 8	245
965+29	USH 8	271
DRIVEWAYS AND SIDE STREETS		465
TOTAL 0010		1,405

REMOVING ASPHALTIC SURFACE MILLING

				204.0120 REMOVING ASPHALTIC SURFACE MILLING
STATION	TO	STATION	LOCATION	SY
485+50	-	922+75	USH 8	145,750
922+75	-	935+75	USH 8	6,673
935+75	-	965+29	USH 8	11,062
TOTAL 0010				163,485

REMOVING CURB & GUTTER

				204.0150 REMOVING CURB & GUTTER
STATION	TO	STATION	LOCATION	LF
926+12	-	926+47	LT	35
927+51	-	927+66	LT	24
928+03	-	928+33	LT	42
928+04	-	930+64	RT	268
929+00	-	929+15	LT	27
929+53	-	929+74	LT	28
930+59	-	930+88	LT	41
931+32	-	934+12	LT	303
931+68	-	934+27	RT	260
934+49	-	935+15	LT	80
-	-	-	-	-
TOTAL 0010				1,108

REMOVING CONCRETE SIDEWALK

				204.0155 REMOVING CONCRETE SIDEWALK
STATION	TO	STATION	LOCATION	SY
BLACKBURN ST	-	ALVEY ST	USH 8, LT	33
ALVEY ST	-	MAIN ST	USH 8, LT/RT	159
MAIN ST	-	1ST ST	USH 8, LT/RT	31
1ST ST	-	2ND ST	USH 8, LT	14+2
TOTAL 0010				223

REMOVING GUARDRAIL

				204.0165 REMOVING GUARDRAIL
STATION	TO	STATION	LOCATION	LF
964+08	-	965+36	LT	128
964+59	-	965+36	RT	77
TOTAL 0010				205

REMOVING FENCE

				204.0170 REMOVING FENCE
STATION	TO	STATION	LOCATION	LF
504+75	-	507+00	RT	225
TOTAL 0010				225

ABANDONING CULVERT PIPES

		204.0270 ABANDONING CULVERT PIPES
STATION	LOCATION	EACH
555+72	USH 8	1
559+98	USH 8	1
908+12	USH 8	1
TOTAL 0010		3

TEMPORARY LANE SHIFT DURING CULVERT WORK

		208.1500.S TEMPORARY LANE SHIFT DURING CULVERT WORK
STATION		EACH
493+21		2
540+59		2
559+00		2
568+98		2
593+98		2
626+45		2
668+40		2
708+12		2
822+09		2
938+00		2
TOTAL 0010		20

PREPARE FOUNDATION FOR
ASPHALTIC PAVING (1570-06-72)

		211.0100.01 PREPARE FOUNDATION FOR ASPHALTIC PAVING (PROJECT) (01. 1570-06-72)
LOCATION		LS
1570-06-72		1
TOTAL 0010		1

FINISHING ROADWAY (1570-06-72)

		213.0100.01 FINISHING ROADWAY (PROJECT) (01. 1570-06-72)
LOCATION		EACH
1570-06-72		1
TOTAL 0010		1

PREPARE FOUNDATION FOR ASPHALTIC SHOULDERS

				211.0400 PREPARE FOUNDATION FOR ASPHALTIC SHOULDERS
STATION	TO	STATION	LOCATION	STA
925+04	-	926+12	USH 8, LT	2
TOTAL 0010				2

EARTHWORK SUMMARY

Division	From/To Station	Location	205.0100 Common Excavation (1)	Salvaged/Unusable Pavement Material (4)	Available Material (5)	205.0500 Marsh Excavation (6)	Reduced Marsh in Fill (8)	Expanded Marsh Backfill (10)	Unexpanded Fill	Expanded Fill (13)	Mass Ordinate +/- (14)	Waste	208.0100 Borrow
			Cut (2)				Factor 1.00	Factor 1.00		Factor 1.25			
Project - USH 8													
Graill 503+66-507+85	503+00/508+00	USH 8	215	0	215	0	0	0	915	1,144	-929	0	929
CP STA 493+21	492+50/494+00	USH 8	191	0	191	0	0	0	97	121	70	70	0
CP STA 540+59	540+00/541+00	USH 8	152	0	152	0	0	0	156	195	-42	0	42
CP STA 559+00	554+50/560+50	USH 8	386	0	386	0	0	0	2,180	2,725	-2,339	0	2,339
CP STA 568+98	568+50/569+50	USH 8	153	0	153	0	0	0	42	53	100	100	0
CP STA 593+98	593+50/594+50	USH 8	79	0	79	0	0	0	144	180	-100	0	100
CP STA 626+43	624+50/627+50	USH 8	155	0	155	0	0	0	744	930	-774	0	774
CP STA 668+40	667+00/670+00	USH 8	343	0	343	0	0	0	1,729	2,161	-1,818	0	1,818
CP STA 708+12	707+50/708+50	USH 8	122	0	122	0	0	0	118	148	-26	0	26
CP STA 822+09	821+50/823+00	USH 8	120	0	120	0	0	0	151	189	-69	0	69
CP STA 938+00	937+50/938+50	USH 8	109	0	109	0	0	0	38	48	62	62	0
Project Subtotal			2,027	0	2,027	0	0	0	6,313	7,892	-5,865	232	6,097
Undistributed Various Areas USH 8													
Grand Total			2,027	0	2,027	0	0	0	6,313	7,892	-5,865	232	6,097
Total Common Exc			2,027										

Notes:

- (1) Common Excavation is the sum of the Cut and EBS Excavation columns. Item number 205.0100
- (2) Salvaged/Unsuable Pavement Material is included in Cut.
- (5) Available Material = Cut - Salvaged/Unusuable Pavement Material
- (13) Expanded Fill Factor = 1.25
- (14) The Mass Ordinate + or - Qty calculated for the Division. Plus quantity indicates an excess of material within the Division. Minus indicates a shortage of material within the Division.

BASE AGGREGATE DENSE

STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	305.0500 SHAPING SHOULDERS STA
485+50	-	925+00	USH 8	5,704	---	44
935+15	-	965+29	USH 8	393	---	4
		PIPE REPLACEMENT AREAS (10)		336	1,863	---
		MGS GUARDRAIL AREAS		282	833	---
		TEMPORARY PEDESTRAIN SIDEWALK AND RAMPS		---	60	---
		DRIVEWAYS (52)		155	---	---
TOTAL 0010				6,870	2,756	48

HMA PAVEMENT TEST STRIPS

PROJECT	460.0115.S HMA PAVEMENT TEST STRIPS VOLUMETRICS EACH	460.0120.S HMA PAVEMENT TEST STRIPS DENSITY EACH
ENTIRE PROJECT	1	1
TOTAL 0010	1	1

HMA PERCENT WITHIN LIMITS (PWL)

PROJECT	460.0105.S HMA PERCENT WITHIN LIMITS (PWL) TEST STRIP VOLUMETRICS EACH	460.0110.S HMA PERCENT WITHIN LIMITS (PWL) TEST STRIP DENSITY EACH
ENTIRE PROJECT	1	1
TOTAL 0010	1	1

MATERIAL TRANSFER VEHICLE

PROJECT	460.9000.S.01 MATERIAL TRANSFER VEHICLE (PROJECT) (01. 1570-06-72) EACH
ENTIRE PORJECT	1
TOTAL 0010	1

ASPHALT CENTERLINE RUMBLE STRIPS 2-LANE RURAL

STATION	TO	STATION	LOCATION	465.0475 ASPHALT CENTERLINE RUMBLE STRIPS 2-LANE RURAL LF
485+50	-	672+50	USH 8	15,450
679+00	-	897+00	USH 8	15,900
TOTAL 0010				31,350

HMA PAVEMENT

STATION	TO	STATION	LOCATION	455.0605 TACK COAT GAL	460.6645 HMA PAVEMENT 5 MT 58-34 V TON	460.8644 HMA PAVEMENT 4 SMA 58-34 V TON	465.0105 ASPHALTIC SURFACE TON	465.0110 ASPHALTIC SURFACE PATCHING TON	465.0120 ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES TON
485+50	-	922+75	USH 8	10,270	11,056	17,683	---	---	---
922+75	-	935+75	USH 8	470	470	750	---	---	---
935+75	-	965+29	USH 8	770	800	1,270	---	---	---
		CURB REPLACEMENT AREAS		---	---	---	103	---	23
		PIPE REPLACEMENT AREAS (10)		---	---	---	475	---	---
		GUARDRAIL AREAS		40	36	57	---	---	---
		DRIVEWAYS (14)		---	---	---	---	---	84
480+80	-	965+29	PROJECT LIMITS	---	---	---	1,000	200	---
TOTAL 0010				11,550	12,362	19,760	1,578	200	107

CULVERT PIPE REINFORCED CONCRETE CLASS III & APRON ENDWALLS FOR CULVERT PIPE

STATION	LOCATION	522.0124	522.0130	522.0148	522.1024	522.1030	522.1048
		CULVERT PIPE REINFORCED CONCRETE CLASS III 24-INCH	CULVERT PIPE REINFORCED CONCRETE CLASS III 30-INCH	CULVERT PIPE REINFORCED CONCRETE CLASS III 48-INCH	APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 24- INCH	APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 30- INCH	APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 48- INCH
		LF	LF	LF	EACH	EACH	EACH
493+21	USH 8	82	---	---	2	---	---
540+59	USH 8	---	---	110	---	---	2
559+00	USH 8	66	---	---	2	---	---
568+98	USH 8	42	---	---	2	---	---
593+98	USH 8	86	---	---	2	---	---
626+43	USH 8	---	92	---	---	2	---
626+48	USH 8	---	92	---	---	2	---
668+40	USH 8	---	124	---	---	2	---
708+12	USH 8	92	---	---	2	---	---
822+09	USH 8	80	---	---	2	---	---
938+00	USH 8	84	---	---	2	---	---
TOTAL 0010		532	308	110	14	6	2

CONCRETE CURB & GUTTER 30-INCH TYPE D

STATION	TO	STATION	LOCATION	601.0411
				CONCRETE CURB & GUTTER 30-INCH TYPE D LF
926+12	-	926+47	LT	35
927+51	-	927+66	LT	23
928+03	-	928+33	LT	41
928+04	-	930+64	RT	267
929+00	-	929+15	LT	26
929+53	-	929+74	LT	27
930+59	-	930+88	LT	41
931+32	-	934+12	LT	303
931+68	-	934+27	RT	259
934+49	-	935+15	LT	78
TOTAL 0010				1,100

RECONSTRUCTING INLETS

STATION	LOCATION	611.0430 RECONSTRUCTING EACH
929+14	LT	1
930+70	LT	1
930+86	LT	1
931+34	LT	1
931+46	LT	1
934+11	LT	1
934+54	LT	1
TOTAL 0010		7

CONCRETE DRIVEWAY 6-INCH, CONCRETE SIDEWALK 4-INCH, & CURB RAMP DETECTABLE WARNING FIELD

STATION	TO	STATION	LOCATION	416.0160	602.0405	*
				CONCRETE DRIVEWAY 6-INCH SY	CONCRETE SIDEWALK 4-INCH SF	CURB RAMP DETECTABLE WARNING FIELD YELLOW SF
BLACKBURN ST	-	ALVEY ST	LT	14	296	20
ALVEY ST	-	MAIN ST	LT	---	681	60
ALVEY ST	-	MAIN ST	RT	19	637	20
MAIN ST	-	1ST ST	LT	56	736	20
MAIN ST	-	1ST ST	RT	15	14	---
TOTAL 0010				104	2,364	120

* QUANTITY LOCATED ELSEWHERE IN THE PLAN

INLET COVERS TYPE H

STATION	LOCATION	611.0624 INLET COVERS TYPE H EACH
634+54	LT	1
TOTAL 0010		1

MGS GUARDRAIL

STATION	TO	STATION	LOCATION	614.2300	614.2330	614.2500	614.2610
				MGS GUARDRAIL 3 LF	MGS GUARDRAIL 3 K LF	MGS THRIE BEAM TRANSITION LF	MGS GUARDRAIL TERMINAL EAT EACH
503+66	-	507+85	RT	---	312.5	---	2
964+05	-	965+36	LT	37.5	---	39.4	1
TOTAL 0010				37.5	312.5	39.4	3

BARRIER SYSTEM GRADING SHAPING FINISHING

STATION LOCATION	** EXCAVATION COMMON CY	** BORROW CY	** SALVAGED TOPSOIL SY	** FERTILIZER TYPE B CWT	** SEEDING NO.20 LB	** MULCHING SY	** SEED WATER MGAL	614.0010 BARRIER SYSTEM GRADING SHAPING FINISHING EACH
964+30, LT	160	80	120	0.10	3.50	120	0.75	1
964+66, RT	300	80	180	0.10	4.00	180	1.00	1
TOTAL 0010								2

** ITEMS & QUANTITIES LISTED FOR BID INFORMATION ONLY.

STEEL PLATE BEAM GUARD

STATION	TO	STATION	LOCATION	614.0200	614.0370
				STEEL THRIE BEAM STRUCTURE APPROACH LF	STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL EACH
964+66	-	965+36	RT	20.6	1
TOTAL 0010				20.6	1

<u>MAINTENANCE AND REPAIR OF HAUL ROADS (1570-06-72)</u>					<u>WATER</u>		<u>CULVERT PIPE CHECKS</u>			<u>TEMPORARY PEDESTRIAN CURB RAMP & CURB RAMP DETECTABLE WARNING FIELD</u>			<u>TRAFFIC CONTROL</u>			
618.0100.01 MAINTENANCE AND REPAIR OF HAUL ROADS (PROJECT) (01. 1570-06-72) EACH					624.0100 WATER MGAL		628.7555 CULVERT PIPE CHECKS EACH			* 602.0505 CURB RAMP DETECTABLE WARNING FIELD YELLOW SF			644.1601 TEMPORARY PEDESTRIAN CURB RAMP DAY			
LOCATION					LOCATION		STATION		LOCATION		LOCATION		PROJECT		EACH	
1570-06-72					1		TOTAL 0010		140		493+21		LT		3	
											540+59		LT		10	
											559+00		LT		3	
											593+98		RT		3	
											626+43		LT		5	
											626+48		LT		5	
											668+40		RT		3	
TOTAL 0010					1				32		TOTAL 0010		88		230	

POSTS AND SIGNING TYPE II

SIGN NO.	APPROX. STATION	SIGN CODE	SIGN SIZE WxH (INCHES)	634.0612 POSTS WOOD 4X6-INCH X 12-FT	637.2210 SIGNS TYPE II REFLECTIVE H
				EACH	SF
1-1	925+40	R-7-53D	18X24	1	3.0
1-2	927+12	R-7-53D	18X24	1	3.0
1-3	928+33	R-7-53D	18X24	1	3.0
1-4	928+85	R-7-53D	18X24	1	3.0
1-5	929+83	R-7-53D	18X24	1	3.0
1-6	928+35	R-7-53D	18X24	1	3.0
1-7	929+21	R-7-53D	18X24	1	3.0
1-8	929+82	R-7-53D	18X24	1	3.0
1-9	930+58	R-7-53D	18X24	1	3.0
1-10	931+67	R-7-53D	18X24	1	3.0
1-11	933+82	R-7-53D	18X24	1	3.0
1-12	934+87	R-7-53D	18X24	1	3.0
1-13	931+66	R-7-53D	18X24	1	3.0
1-14	934+01	R-7-53D	18X24	1	3.0
TOTAL 0010				14	42

CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER

STATION	TO	STATION	LOCATION	650.5500 CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER
				LF
926+12	-	926+47	LT	35
927+51	-	927+66	LT	23
928+03	-	928+33	LT	41
928+04	-	930+64	RT	267
929+00	-	929+15	LT	26
929+53	-	929+74	LT	27
930+59	-	930+88	LT	41
931+32	-	934+12	LT	303
931+68	-	934+27	RT	259
934+49	-	935+15	LT	78
TOTAL 0010				1,100

CONSTRUCTION STAKING SUPPLEMENTAL
CONTROL (1570-06-72)

650.9910.01 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (01. 1570-06-72)	
LOCATION	LS
1570-06-72	1
TOTAL 0010	1

MARKING STOP LINE EPOXY 12-INCH

LOCATION	646.6020 MARKING STOP LINE EPOXY 12-INCH
	LF
STH 40	25
BLACKBURN ST	18
ALVERY ST	16
STH 40/MAIN ST	20
MAIN ST	24
1ST ST	16
TOTAL 0010	119

MARKING CROSSWALK EPOXY

LOCATION	646.7420 MARKING CROSSWALK EPOXY TRANSVERSE LINE	646.7520 MARKING CROSSWALK EPOXY BLOCK STYLE
	6-INCH LF	24-INCH LF
USH 8	---	54
ALVERY ST	80	---
USH 8	---	54
TOWN RD	80	---
USH 8	---	54
STH 40/MAIN ST	100	---
USH 8	---	54
TOTAL 0010	260	216

CONSTRUCTION STAKING CURB RAMPS

STATION	LOCATION	650.9000 CONSTRUCTION STAKING CURB RAMPS
		EACH
926+42	LT	1
927+63	LT	1
928+08	RT	1
928+12	LT	1
928+28	RT	1
928+28	LT	1
929+09	LT	1
929+59	LT	1
930+81	LT	1
931+40	LT	1
934+08	LT	1
TOTAL 0010		11

CONSTRUCTION STAKING RESURFACING REFERENCE

STATION	TO	STATION	LOCATION	650.8000 CONSTRUCTION STAKING RESURFACING REFERENCE
				LF
485+50	-	965+29	USH 8	47,979
TOTAL 0010				47,979

CONSTRUCTION STAKING PIPE CULVERTS

STATION	LOCATION	650.6000 CONSTRUCTION STAKING PIPE CULVERTS
		EACH
493+21	USH 8	1
540+59	USH 8	1
559+00	USH 8	1
568+98	USH 8	1
593+98	USH 8	1
626+43	USH 8	1
626+48	USH 8	1
668+40	USH 8	1
708+12	USH 8	1
822+09	USH 8	1
938+00	USH 8	1
TOTAL 0010		11

SAWING

				690.0150	690.0250	
				SAWING	SAWING	
				ASPHALT	CONCRETE	
STATION	TO	STATION	LOCATION	LF	LF	
926+15	-	926+27	KIEF RD	35	---	
			HUTCHINSON RD	43	---	
			GURESKI LN	35	---	
			ZEBRO RD	36	---	
			NW QUAD, ALLEY	---	5	
			ALLEY	53	---	
			COLEMAN ST	22	---	
927+66	-	928+03	NE QUAD, ALLEY	---	7.5	
			NW QUAD, ALVEY ST	35	15	
			ALVEY ST, LT	37	---	
			NE QUAD, ALVEY ST	51	15	
			ALVEY ST. RT	30	---	
928+00	-	930+64	RT	418	102	
			NW QUAD, ALLEY	36	10	
929+15	-	929+52	ALLEY	37	---	
			NE QUAD, ALLEY	62	11.5	
930+88	-	931+32	NW QUAD, MAIN ST	54	20	
			MAIN ST, LT	44	---	
			MAIN ST, RT	44	---	
			LT	319	88	
931+30	-	934+14	RT	350	9.5	
931+67	-	933+28	1ST ST	37	---	
				NE QUAD, 1ST ST	171	2.5
				PIPE REPLACEMENT AREAS (10)	640	400
				SIDERoads & DRIVEWAYS	1,034	---
TOTAL 0010				3,623	686	

Location	Station	Mixture Use	Underlying Surface	Bid Item	Tons	Thickness	Quality Management Program to be used for:	
							Mixture Acceptance	Density Acceptance
12 Foot Driving Lane	480+80 - 922+75, 935+75 - 965+29	Surface Layer	5 MT 58-34 V	4 SMA 58-34 V	14,085	2"	QMP as per SS 465	Incentive Density HMA Pavement 460.2000
12 Foot Driving Lane	480+80 - 922+75, 935+75 - 965+29	Binder Layer	MILLED EXISTING HMA SURFACE	5 MT 58-34 V	8,805	1.25"	PWL Incentative Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
3 Foot Shoulder	480+80 - 922+75, 935+75 - 965+29	Surface Layer	5 MT 58-34 V	4 SMA 58-34 V	5,060	2"	QMP as per SS 465	Incentive Density HMA Pavement 460.2000
3 Foot Shoulder	480+80 - 922+75, 935+75 - 965+29	Binder Layer	MILLED EXISTING HMA SURFACE	5 MT 58-34 V	3,160	1.25"	PWL Incentative Air Voids HMA Pavement 460.2010	Acceptance testing by the department; Not eligible for incentative
12 Foot Driving Lane	922+75 - 935+75	Surface Layer	5 MT 58-34 V	4 SMA 58-34 V	400	2"	QMP as per SS 465	Incentive Density HMA Pavement 460.2000
12 Foot Driving Lane	922+75 - 935+75	Binder Layer	MILLED EXISTING HMA SURFACE	5 MT 58-34 V	250	1.25"	PWL Incentative Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
10 Foot Shoulder	922+75 - 935+75	Surface Layer	5 MT 58-34 V	4 SMA 58-34 V	350	2"	QMP as per SS 465	Incentive Density HMA Pavement 460.2000
10 Foot Shoulder	922+75 - 935+75	Binder Layer	MILLED EXISTING HMA SURFACE	5 MT 58-34 V	225	1.25"	PWL Incentative Air Voids HMA Pavement 460.2014	Acceptance testing by the department; Not eligible for incentative
Various		Culvert Patches	Base Aggregate	Asphaltic Surface	475	7" Total	QMP as per SS 465	Acceptance by ordinary compaction

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN
COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS)
RUSK COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES
ARE GRID COORDINATES, GRID BEARINGS, AND GRID
DISTANCES. GRID DISTANCES MAY BE USED AS GROUND
DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2
(TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE
NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF
THE PROJECT.

DIMENSIONING FOR THE RIGHT-OF-WAY IS MEASURED ALONG
AND PERPENDICULAR TO THE REFERENCE LINE.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF
THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO
THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS"
OF PUBLIC RECORD.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION
AREAS ARE INTENDED TO RE-ESTABLISH EXISTING
RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS
PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM
CENTERLINE OF EXISTING PAVEMENTS.

CONVENTIONAL SYMBOLS

SECTION LINE
QUARTER LINE
SIXTEENTH LINE
NEW REFERENCE LINE
NEW R/W LINE
EXISTING R/W OR HE LINE
PROPERTY LINE

LOT, TIE & OTHER
MINOR LINES

SLOPE INTERCEPT

CORPORATE LIMITS

UNDERGROUND FACILITY
(COMMUNICATIONS, ELECTRIC, ETC.)

NEW R/W (FEE OR HE)
(HATCHING VARIES BY OWNER)

TEMPORARY LIMITED
EASEMENT AREA

EASEMENT AREA
(PERMANENT LIMITED OR
RESTRICTED DEVELOPMENT)

TRANSMISSION STRUCTURES

BUILDING

BRIDGE

TO BE REMOVED

CULVERT

SECTION
CORNER
SYMBOL

SECTION
CORNER
MONUMENT

GEODETIC SURVEY MONUMENT

SIXTEENTH CORNER MONUMENT

SIGN

ELECTRIC POLE

TELEPHONE POLE

PEDESTAL (LABEL TYPE)

ACCESS RESTRICTED BY ACQUISITION

NO ACCESS (BY STATUTORY AUTHORITY)

ACCESS RESTRICTED (BY PREVIOUS
PROJECT OR CONTROL)

NO ACCESS (NEW HIGHWAY)

PARCEL NUMBER

PARALLEL OFFSETS

R/W MONUMENT
(TO BE SET)

NON-MONUMENTED
R/W POINT

FOUND IRON PIN
(1-INCH UNLESS NOTED)

OFF-PREMISE
SIGN

COMPENSABLE

NON-COMPENSABLE

ACCESS RESTRICTED BY ACQUISITION

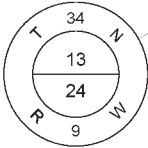
NO ACCESS (BY STATUTORY AUTHORITY)

ACCESS RESTRICTED (BY PREVIOUS
PROJECT OR CONTROL)

NO ACCESS (NEW HIGHWAY)

PARCEL NUMBER

UTILITY NUMBER



Y 547917.53
X 728863.85
STEEL SURVEY
NAIL

SE-NW
SEC. 24, T34N, R09W

SE-NW
SEC. 24, T34N, R09W

SE-NW
SEC. 24, T34N, R09W

SE-NW
SEC. 24, T34N, R09W

SE-NW
SEC. 24, T34N, R09W

SE-NW
SEC. 24, T34N, R09W

SE-NW
SEC. 24, T34N, R09W

SE-NW
SEC. 24, T34N, R09W

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SEC. 24, T34N, R09W

SE-NW
SEC. 24, T34N, R09W

SE-NW
SEC. 24, T34N, R09W

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SEC. 24, T34N, R09W

SE-NW
SEC. 24, T34N, R09W

SE-NW
SEC. 24, T34N, R09W

SE-NW
SEC. 24, T34N, R09W

SE-NW
SEC. 24, T34N, R09W

SE-NW
SEC. 24, T34N, R09W

TRANSPORTATION PROJECT PLAT NO: 1570-06-22 - 4.01

PART OF THE NW1/4 -SW1/4, PART OF THE NE1/4-SW1/4 AND PART OF THE SE1/4-NW1/4
OF SECTION 24, T34N, R09W, TOWN OF STRICKLAND, RUSK COUNTY, WISCONSIN

USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

THE STATE OF WISCONSIN HAS DEEMED IT NECESSARY TO MONUMENT RIGHT-OF-WAY INTERESTS PREVIOUSLY
ACQUIRED UNDER SECTIONS 82.01, 82.31(1), 82.31(2) AND 84.09 OF THE WISCONSIN STATE STATUTES AND DOES
HEREBY CERTIFY THE DEPARTMENT OF TRANSPORTATION HAS SURVEYED THE HIGHWAY RIGHT-OF-WAY; THAT THIS PLAT
IS A TRUE AND CORRECT REPRESENTATION OF THE EXTERIOR BOUNDARIES OF THE LAND SURVEYED AND SHOWS
ACCURATE MEASUREMENTS THEREOF.

GN

SCALE, FEET

0 100 200

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM
DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC
RECORD AND/OR EXISTING OCCUPANCY LINES. THIS
PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING
PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD
NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD
SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION,
CONTACT THE PLANNING UNIT OF THE WISCONSIN
DEPARTMENT OF TRANSPORTATION OFFICE IN SPOONER.

EXISTING USH 8 RIGHT-OF-WAY SHOWN HEREIN IS BASED
ON THE FOLLOWING POINTS OF REFERENCE:
R/W PROJECT NUMBER 8747 AND MAP OF SURVEY V-1360,
FILED 7-25-2007, IN THE RUSK COUNTY SURVEYOR'S OFFICE.
EXISTING CEDAR SWAMP ROAD AND OLESIAK LANE
RIGHT-OF-WAY SHOWN IS BASED ON PHYSICAL EVIDENCE
AND EXISTING CENTERLINE.

UTILITY INFORMATION IS NOT SHOWN ON THIS PLAT.

CONVENTIONAL ABBREVIATIONS

ACRES	AC	POINT OF TANGENCY	PT
AHEAD	AH	POINT OF BEGINNING	POB
ALUMINUM	ALUM	POINT OF CURVATURE	PC
BACK	BK	POINT OF COMPOUND CURVE	PCC
CENTERLINE	C/L	POINT OF INTERSECTION	PI
CERTIFIED SURVEY MAP	CSM	PROPERTY LINE	PL
COUNTY	CO	RECORDED AS	{100'}
COUNTY TRUNK HIGHWAY	CTH	REFERENCE LINE	R/L
DISTANCE	DIST	RIGHT	RT
CORNER	COR	RIGHT OF WAY	R/W
DOCUMENT NUMBER	DOC	SECTION	SEC
EASTBOUND	EB	SEPTIC VENT	SEPV
EXISTING	EX	SQUARE FEET	SF
GRID NORTH	GN	STATE TRUNK HIGHWAY	STH
LAND CONTRACT	LC	STATION	STA
LEFT	LT	TEMPORARY LIMITED	TLE
MONUMENT	MON	EASEMENT	
NUMBER	NO	TRANSPORTATION PROJECT	TPP
OUTLOT	OL	PLAT	
PAGE	P	VOLUME	V

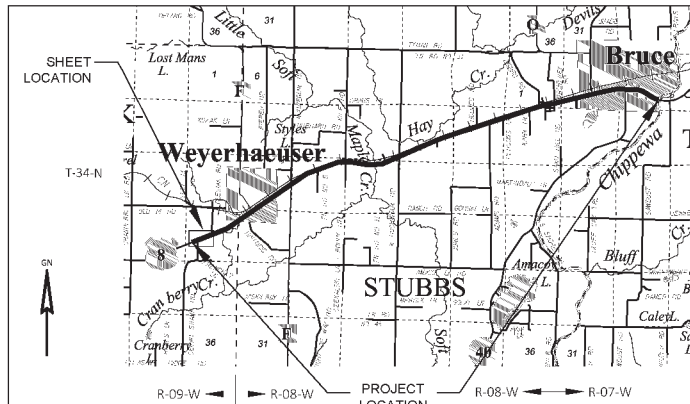
EXISTING MONUMENTS					
POINT	Y	X	DESCRIPTION	STA	OFFSET
6120	545199.97	728826.14	1" IRON PIPE	495+96.84	75.10' RT

NO TYPE 2 MONUMENT SET DUE TO CLOSE
PROXIMITY OF EXISTING MONUMENT.

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ / DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

Y 542768.33
X 728741.69
ALUMINUM MONUMENT



SIGNATURE: _____ DATE: 05/19/21

PRINT NAME: AARON PARKS

REGISTRATION NUMBER: S-2861

THIS PLAT IS APPROVED FOR THE WISCONSIN
DEPARTMENT OF TRANSPORTATION

SIGNATURE: _____ DATE: 12/01/21

PRINT NAME: HEATHER L. DRESEL

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN SPOONER.

EXISTING USH 8 RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:
R/W PROJECT NUMBER 8747 AND MAP OF SURVEY V-1360, FILED 7-25-2007, IN THE RUSK COUNTY SURVEYOR'S OFFICE.
EXISTING OLESIK LANE RIGHT-OF-WAY SHOWN HEREIN IS BASED ON PHYSICAL EVIDENCE AND EXISTING CENTERLINE.

UTILITY INFORMATION IS NOT SHOWN ON THIS PLAT.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS) RUSK COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

DIMENSIONING FOR THE RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE REFERENCE LINE.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

CONVENTIONAL ABBREVIATIONS

ACRES	AC	POINT OF TANGENCY	PT
AHEAD	AH	POINT OF BEGINNING	POB
ALUMINUM	ALUM	POINT OF CURVATURE	PC
BACK	BK	POINT OF COMPOUND CURVE	PCC
CENTERLINE	C/L	POINT OF INTERSECTION	PI
CERTIFIED SURVEY MAP	CSM	PROPERTY LINE	PL
COUNTY	CO	RECORDED AS	(100')
COUNTY TRUNK HIGHWAY	CTH	REFERENCE LINE	R/L
DISTANCE	DIST	RIGHT	RT
CORNER	COR	RIGHT OF WAY	R/W
DOCUMENT NUMBER	DOC	SECTION	SEC
EASTBOUND	EB	SEPTIC VENT	SEPV
EXISTING	EX	SQUARE FEET	SF
GRID NORTH	GN	STATE TRUNK HIGHWAY	STH
LAND CONTRACT	LC	STATION	STA
LEFT	LT	TEMPORARY LIMITED	TLE
MONUMENT	MON	EASEMENT	
NUMBER	NO	TRANSPORTATION PROJECT	TPP
OUTLOT	OL	PLAT	
PAGE	P	VOLUME	V

CONVENTIONAL SYMBOLS	
SECTION LINE	---
QUARTER LINE	---
SIXTEENTH LINE	---
NEW REFERENCE LINE	---
NEW R/W LINE	---
EXISTING R/W OR HE LINE	---
PROPERTY LINE	---
LOT, TIE & OTHER MINOR LINES	---
SLOPE INTERCEPT	---
CORPORATE LIMITS	---
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	---
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---
TEMPORARY LIMITED EASEMENT AREA	---
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---
TRANSMISSION STRUCTURES	---
BUILDING	---
BRIDGE	---
TO BE REMOVED	---
CULVERT	---
PARALLEL OFFSETS	---
SECTION CORNER SYMBOL	---
SECTION CORNER MONUMENT	---
GEODETIC SURVEY MONUMENT	---
SIXTEENTH CORNER MONUMENT	---
SIGN	---
OFF-PREMISE SIGN	---
ELECTRIC POLE	---
TELEPHONE POLE	---
PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)	---
ACCESS RESTRICTED BY ACQUISITION	---
NO ACCESS (BY STATUTORY AUTHORITY)	---
ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)	---
NO ACCESS (NEW HIGHWAY)	---
PARCEL NUMBER	---
UTILITY NUMBER	---

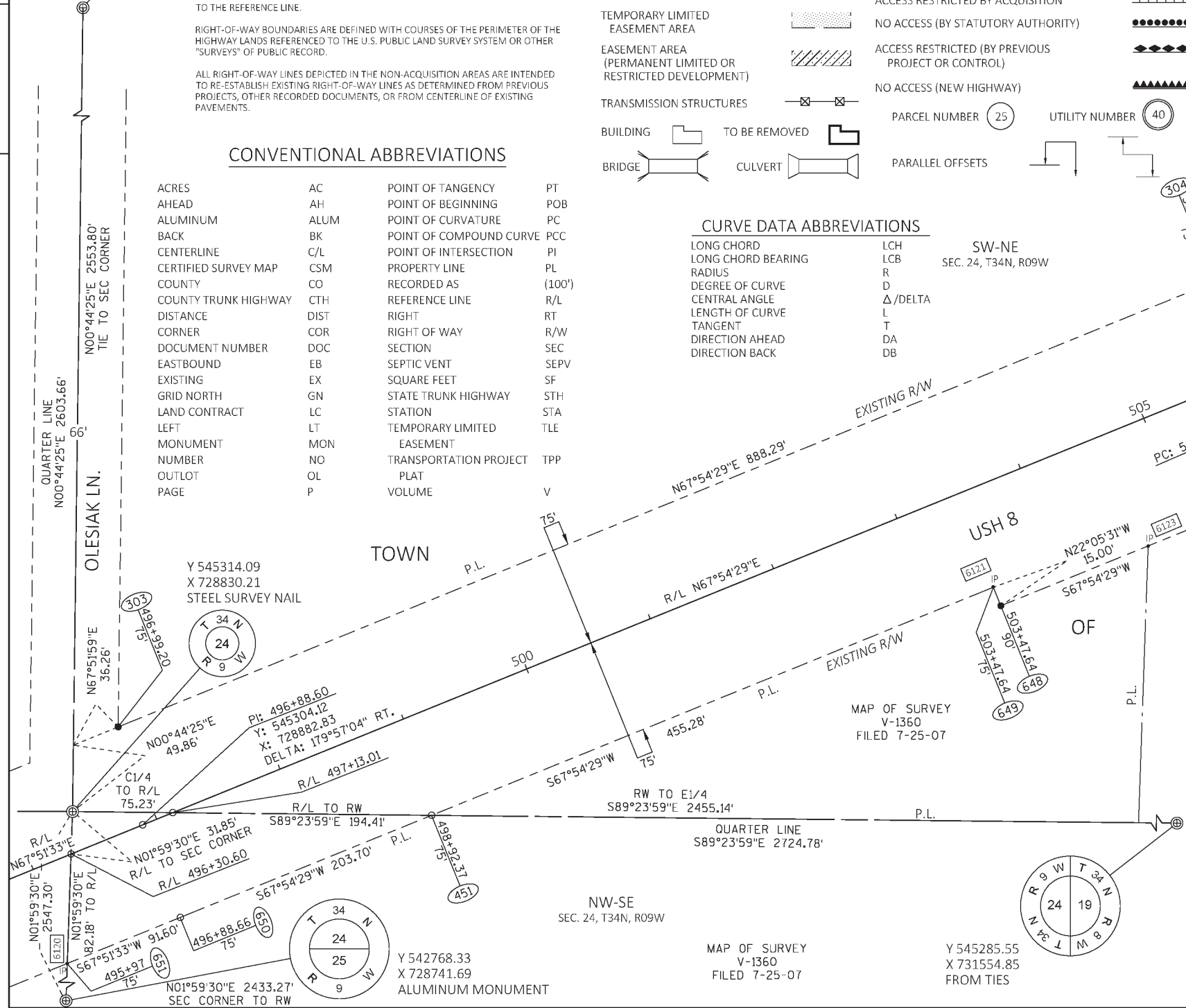
CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ / DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

SW-NE
SEC. 24, T34N, R09W

Y 547917.53
X 728863.85
STEEL SURVEY NAIL

4



TRANSPORTATION PROJECT PLAT NO: 1570-06-22 - 4.02

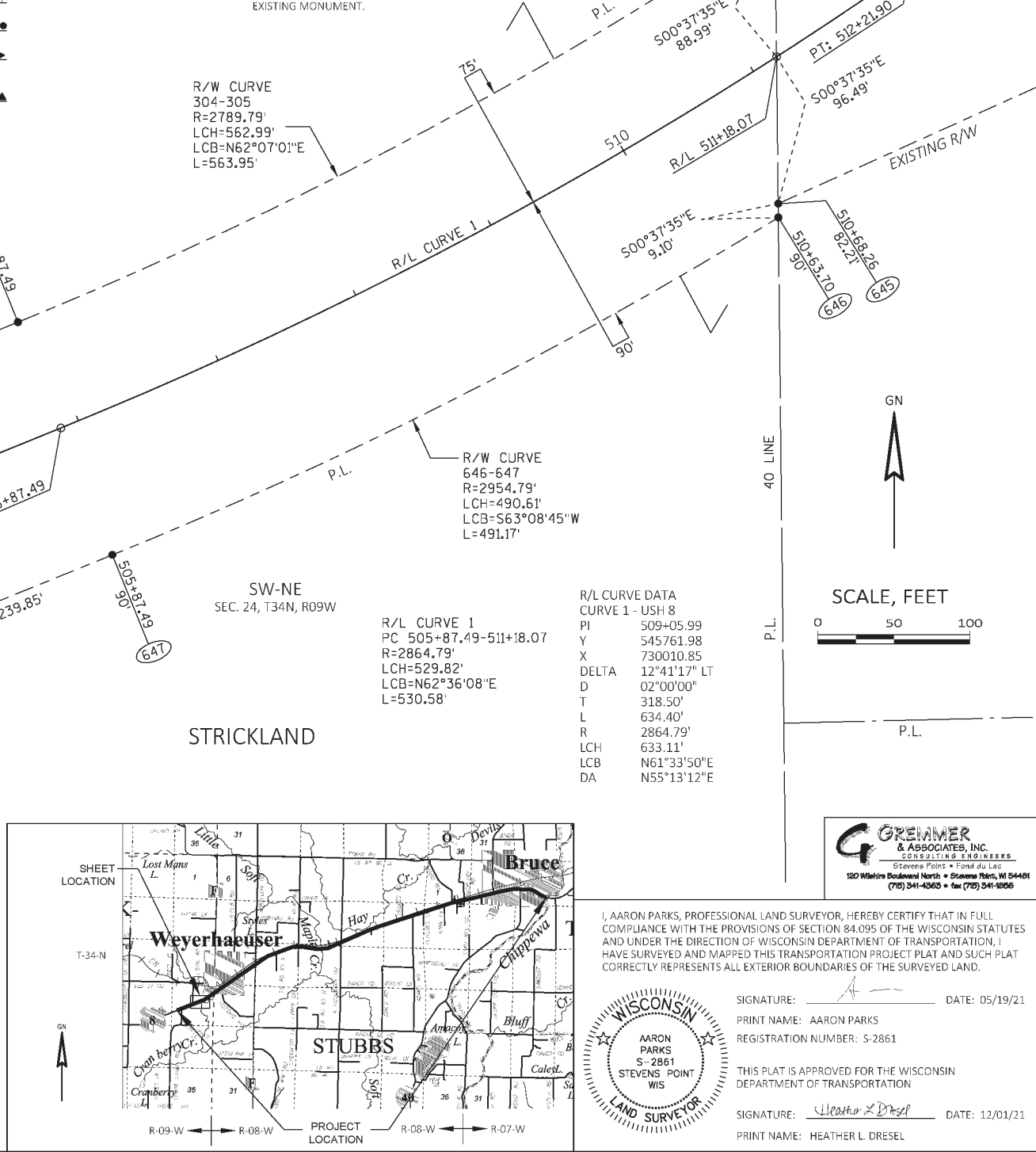
PART OF THE NW1/4 -SE1/4 AND PART OF THE SW1/4-NE1/4 OF SECTION 24, T34N, R09W, TOWN OF STRICKLAND, RUSK COUNTY, WISCONSIN

USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

THE STATE OF WISCONSIN HAS DEEMED IT NECESSARY TO MONUMENT RIGHT-OF-WAY INTERESTS PREVIOUSLY ACQUIRED UNDER SECTIONS 82.01, 82.31(1), 82.31(2) AND 84.09 OF THE WISCONSIN STATE STATUTES AND DOES HEREBY CERTIFY THE DEPARTMENT OF TRANSPORTATION HAS SURVEYED THE HIGHWAY RIGHT-OF-WAY; THAT THIS PLAT IS A TRUE AND CORRECT REPRESENTATION OF THE EXTERIOR BOUNDARIES OF THE LAND SURVEYED AND SHOWS ACCURATE MEASUREMENTS THEREOF.

EXISTING MONUMENTS					
POINT	Y	X	DESCRIPTION	STA	OFFSET
6120	545199.97	728826.14	1" IRON PIPE	495+96.84	75.10' RT
6121	545482.73	729521.60	1" IRON PIPE	503+47.64	74.74' RT
6123	545513.30	729637.96	1" IRON PIPE	504+66.96	90.18' RT

NO TYPE 2 MONUMENT SET DUE TO CLOSE PROXIMITY OF EXISTING MONUMENT.



362313
MARY BERG
REGISTER OF DEEDS
RUSK COUNTY, WI
RECORDED ON
12/08/2021 10:30 AM
CABINET:1 ENV:11 A

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 1570-06-22 - 4.02
AMENDMENT NO:___

SCALE, FEET
0 50 100

GREMMER & ASSOCIATES, INC.
CONSULTING ENGINEERS
Stevens Point • Fond du Lac
120 Watline Boulevard North • Stevens Point, WI 54481
(715) 341-4365 • Fax: (715) 341-4266

I, AARON PARKS, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

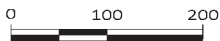
SIGNATURE: _____ DATE: 05/19/21
PRINT NAME: AARON PARKS
REGISTRATION NUMBER: S-2861
THIS PLAT IS APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION
SIGNATURE: _____ DATE: 12/01/21
PRINT NAME: HEATHER L. DRESEL

362307
MARY BERG
REGISTER OF DEEDS
RUSK COUNTY, WI
RECORDED ON
12/07/2021 12:05 PM
CABINET:1 ENV:98

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 1570-06-22 - 4.03
AMENDMENT NO.:



SCALE, FEET



POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS) RUSK COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

DIMENSIONING FOR THE RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE REFERENCE LINE.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

CONVENTIONAL ABBREVIATIONS

ACRES	AC	POINT OF TANGENCY	PT
AHEAD	AH	POINT OF BEGINNING	POB
ALUMINUM	ALUM	POINT OF CURVATURE	PC
BACK	BK	POINT OF COMPOUND CURVE	PCC
CENTERLINE	C/L	POINT OF INTERSECTION	PI
CERTIFIED SURVEY MAP	CSM	PROPERTY LINE	PL
COUNTY	CO	RECORDED AS	(100')
COUNTY TRUNK HIGHWAY	CTH	REFERENCE LINE	R/L
DISTANCE	DIST	RIGHT	RT
CORNER	COR	RIGHT OF WAY	R/W
DOCUMENT NUMBER	DOC	SECTION	SEC
EASTBOUND	EB	SEPTIC VENT	SEPV
EXISTING	EX	SQUARE FEET	SF
GRID NORTH	GN	STATE TRUNK HIGHWAY	STH
LAND CONTRACT	LC	STATION	STA
LEFT	LT	TEMPORARY LIMITED	TLE
MONUMENT	MON	EASEMENT	
NUMBER	NO	TRANSPORTATION PROJECT	TPP
OUTLOT	OL	PLAT	
PAGE	P	VOLUME	V

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN SPOONER.

EXISTING USH 8 RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:

R/W PROJECT NUMBER 8747 AND R/W PROJECT NUMBER 8641.

EXISTING HISTORIC ROAD RIGHT-OF-WAY SHOWN HEREIN IS BASED ON EXISTING CENTERLINE.

UTILITY INFORMATION IS NOT SHOWN ON THIS PLAT.



I, AARON PARKS, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: DATE: 05/19/21

PRINT NAME: AARON PARKS

REGISTRATION NUMBER: S-2861

THIS PLAT IS APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION

SIGNATURE:

PRINT NAME: HEATHER L. DRESEL



CONVENTIONAL SYMBOLS			
SECTION LINE	---	SECTION CORNER SYMBOL	
QUARTER LINE	---	SECTION CORNER MONUMENT	
SIXTEENTH LINE	---	GEODETIC SURVEY MONUMENT	
NEW REFERENCE LINE	---	SIXTEENTH CORNER MONUMENT	
NEW R/W LINE	---	SIGN	
EXISTING R/W OR HE LINE	---	OFF-PREMISE SIGN	
PROPERTY LINE	---	COMPENSABLE	
LOT, TIE & OTHER MINOR LINES	---	NON-COMPENSABLE	
SLOPE INTERCEPT	---	ELECTRIC POLE	
CORPORATE LIMITS	---	TELEPHONE POLE	
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	---	PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)	
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---	ACCESS RESTRICTED BY ACQUISITION	
TEMPORARY LIMITED EASEMENT AREA	---	NO ACCESS (BY STATUTORY AUTHORITY)	
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---	ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)	
TRANSMISSION STRUCTURES	---	NO ACCESS (NEW HIGHWAY)	
BUILDING		PARCEL NUMBER	
TO BE REMOVED		UTILITY NUMBER	
BRIDGE		PARALLEL OFFSETS	
CULVERT			



Y 547969.39
X 731463.40
ALUMINUM MONUMENT

PLAT

LOT 6

WEYERHAEUSER

FRAC. NW-NW
SEC. 19, T34N, R08W

Pl: 529+22.50
Y: 546913.57
X: 731669.35
DELTA: 179°56'43" RT.

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TRANSPORTATION PROJECT PLAT NO: 1570-06-22 - 4.04

PART OF LOT 1 OF CSM #1365 AND PART OF LOT 4 OF PLAT OF WEYERHAEUSER, LOCATED IN THE SE1/4-SW1/4 SECTION 18, AND PART OF THE SW1/4-SE1/4 OF SECTION 18, T34N, R08W, VILLAGE OF WEYERHAEUSER, RUSK COUNTY, WISCONSIN

RELOCATION ORDER: USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT: 1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT. 2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS) RUSK COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

EXISTING ACCESS CONTROL ALONG USH 8 ESTABLISHED FROM QUIT CLAIM DEED, VOLUME 186, PAGE 586, AS DOCUMENT #184634.

CONVENTIONAL ABBREVIATIONS

ACRES	AC	POINT OF TANGENCY	PT
AHEAD	AH	POINT OF BEGINNING	POB
ALUMINUM	ALUM	POINT OF CURVATURE	PC
BACK	BK	POINT OF COMPOUND CURVE	PCC
CENTERLINE	C/L	POINT OF INTERSECTION	PI
CERTIFIED SURVEY MAP	CSM	PROPERTY LINE	PL
COUNTY	CO	RECORDED AS	(100')
COUNTY TRUNK HIGHWAY	CTH	REFERENCE LINE	R/L
DISTANCE	DIST	RIGHT	RT
CORNER	COR	RIGHT OF WAY	R/W
DOCUMENT NUMBER	DOC	SECTION	SEC
EASTBOUND	EB	SEPTIC VENT	SEPV
EXISTING	EX	SQUARE FEET	SF
GRID NORTH	GN	STATE TRUNK HIGHWAY	STH
LAND CONTRACT	LC	STATION	STA
LEFT	LT	TEMPORARY LIMITED	TLE
MONUMENT	MON	EASEMENT	
NUMBER	NO	TRANSPORTATION PROJECT	TPP
OUTLOT	OL	PLAT	
PAGE	P	VOLUME	V

CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT	○ ● (SET)
QUARTER LINE	---	SECTION CORNER MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	IP ●
SIXTEENTH LINE	---	GEODETIC SURVEY MONUMENT		SIXTEENTH CORNER MONUMENT	
NEW REFERENCE LINE	---	SIGN		OFF-PREMISE SIGN	
NEW R/W LINE	---	COMPENSABLE		NON-COMPENSABLE	
EXISTING R/W OR HE LINE	---	ELECTRIC POLE		TELEPHONE POLE	
PROPERTY LINE	---	PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)		ACCESS RESTRICTED BY ACQUISITION	
LOT, TIE & OTHER MINOR LINES	---	NO ACCESS (BY STATUTORY AUTHORITY)		ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)	
SLOPE INTERCEPT	---	NO ACCESS (NEW HIGHWAY)		PARCEL NUMBER (25)	
CORPORATE LIMITS	---	TRANSMISSION STRUCTURES		UTILITY NUMBER (40)	
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	---	BUILDING		PARALLEL OFFSETS	
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---	BRIDGE			
TEMPORARY LIMITED EASEMENT AREA	---	CULVERT			
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---				

EXISTING MONUMENTS					
POINT	Y	X	DESCRIPTION	STA	OFFSET
*6166	547806.70	732882.29	1" IRON PIPE	544+28.16	43.55' LT
6165	547920.27	733046.66	1" IRON PIPE	546+27.94	43.32' LT
*6164	548189.91	733422.68	1" IRON PIPE	550+90.58	50.87' LT
6163	548470.47	733838.38	3/4" IRON REBAR	555+92.08	44.82' LT
6162	548501.81	733883.66	3/4" IRON REBAR	556+47.14	44.80' LT
*6132	548499.02	734079.51	3/4" IRON REBAR	558+06.55	69.00' RT
6131	548330.29	733836.51	3/4" IRON REBAR	555+10.72	69.35' RT
*6130	548211.11	733664.72	3/4" IRON REBAR	553+01.64	69.51' RT
*6128	547990.55	733303.91	1" IRON PIPE	548+79.44	45.39' RT
*6127	547784.73	733068.42	3/4" IRON REBAR	545+68.66	80.49' RT

* NO TYPE 2 MONUMENT SET DUE TO CLOSE PROXIMITY OF EXISTING MONUMENT.

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER(S)	INTERESTS REQUIRED	R/W ACRES REQUIRED			TLE ACRES
			NEW	EXISTING	TOTAL	
5	DANIELS JOINT REVOCABLE TRUST	TLE	0	0	0	0.144
6	WISCONSIN CENTRAL LTD	TLE	0	0	0	0.078
7	PATRICK GREENING	TLE	0	0	0	0.012
8	MICHAEL HAUSE	TLE	0	0	0	0.114

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT.

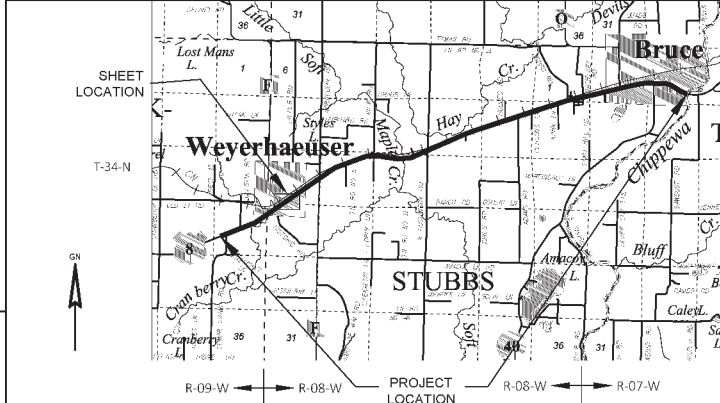
CONVENTIONAL UTILITY SYMBOLS

WATER	---	W
GAS	---	G
TELEPHONE	---	T
OVERHEAD TRANSMISSION LINES	---	OH
ELECTRIC	---	E
CABLE TELEVISION	---	TV
FIBER OPTIC	---	FO
SANITARY SEWER	---	SAN
STORM SEWER	---	SS
ELECTRIC TOWER	---	ET

UTILITY INTERESTS REQUIRED

UTILITY NUMBER	OWNER(S)	INTEREST REQUIRED
80	INDIANHEAD TELEPHONE CO.	RELEASE OF RIGHTS
81	XCEL ENERGY	RELEASE OF RIGHTS

- (81) XCEL ENERGY - ELECTRIC NO EASEMENT OF RECORD PARCEL 8
- (80) INDIANHEAD TELEPHONE CO. EASEMENTS V.269 P.25 DOC.226492 - PARCEL 8 V.269 P.27 DOC.226493 - PARCEL 8



SIGNATURE: [Signature] DATE: 07/15/21
PRINT NAME: AARON PARKS
REGISTRATION NUMBER: S-2861
THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION
SIGNATURE: [Signature] DATE: 10/18/21
PRINT NAME: HEATHER L. DRESEL

TRANSPORTATION PROJECT PLAT NO: 1570-06-22 - 4.05

PART OF THE NW1/4-SE1/4 OF SECTION 18, IN THE VILLAGE OF WEYERHAEUSER, PART OF THE NE1/4-SE1/4 AND THE SE1/4-NE1/4 OF SECTION 18, AND PART OF THE NW1/4-SW1/4 OF SECTION 17, TOWN OF STUBBS, ALL IN T34N, R08W, RUSK COUNTY, WISCONSIN

USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

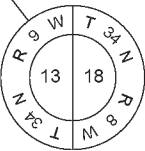
THE STATE OF WISCONSIN HAS DEEMED IT NECESSARY TO MONUMENT RIGHT-OF-WAY INTERESTS PREVIOUSLY ACQUIRED UNDER SECTIONS 82.01, 82.31(1), 82.31(2) AND 84.09 OF THE WISCONSIN STATE STATUTES AND DOES HEREBY CERTIFY THE DEPARTMENT OF TRANSPORTATION HAS SURVEYED THE HIGHWAY RIGHT-OF-WAY; THAT THIS PLAT IS A TRUE AND CORRECT REPRESENTATION OF THE EXTERIOR BOUNDARIES OF THE LAND SURVEYED AND SHOWS ACCURATE MEASUREMENTS THEREOF.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN SPOONER.

EXISTING USH 8 RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:
R/W PROJECT NUMBER 8641 AND CERTIFIED SURVEY MAP NUMBER 774.
EXISTING NORWEGIAN ROAD RIGHT-OF-WAY SHOWN HEREIN IS BASED ON CERTIFIED SURVEY MAP NUMBER 774, AND EXISTING CTH F RIGHT-OF-WAY SHOWN HEREIN IS BASED ON WARRANTY DEED, VOLUME 199, PAGE 393, AS DOCUMENT # 191989.

UTILITY INFORMATION IS NOT SHOWN ON THIS PLAT.



Y 550592.03
X 731501.14
ALUMINUM MONUMENT

CONVENTIONAL ABBREVIATIONS

ACRES	AC	POINT OF TANGENCY	PT
AHEAD	AH	POINT OF BEGINNING	POB
ALUMINUM	ALUM	POINT OF CURVATURE	PC
BACK	BK	POINT OF COMPOUND CURVE	PCC
CENTERLINE	C/L	POINT OF INTERSECTION	PI
CERTIFIED SURVEY MAP	CSM	PROPERTY LINE	PL
COUNTY	CO	RECORDED AS	(100')
COUNTY TRUNK HIGHWAY	CTH	REFERENCE LINE	R/L
DISTANCE	DIST	RIGHT	RT
CORNER	COR	RIGHT OF WAY	R/W
DOCUMENT NUMBER	DOC	SECTION	SEC
EASTBOUND	EB	SEPTIC VENT	SEPV
EXISTING	EX	SQUARE FEET	SF
GRID NORTH	GN	STATE TRUNK HIGHWAY	STH
LAND CONTRACT	LC	STATION	STA
LEFT	LT	TEMPORARY LIMITED	TLE
MONUMENT	MON	EASEMENT	
NUMBER	NO	TRANSPORTATION PROJECT	TPP
OUTLOT	OL	PLAT	
PAGE	P	VOLUME	

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

POINT	Y	X	DESCRIPTION	STA	OFFSET
6155	550314.17	736486.76	3/4" IRON REBAR	588+19.03	49.89' LT
6156	550510.26	736487.16	1" IRON PIPE	589+31.10	210.79' LT
6157	550151.20	736066.08	1" IRON PIPE	583+80.58	155.74' LT
6158	550201.39	736023.45	1" IRON PIPE	583+74.18	221.29' LT
6159	549829.95	735788.76	1" IRON PIPE	579+69.55	50.07' LT
6161	549743.85	735664.70	1" IRON PIPE	578+18.54	50.10' LT

NO TYPE 2 MONUMENT SET DUE TO CLOSE PROXIMITY OF EXISTING MONUMENT.

SCALE, FEET



TOWN

NE-SE
SEC. 18, T34N, R08W

STUBBS

NW-SW
SEC. 17, T34N, R08W

SW-NW
SEC. 17, T34N, R08W

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS) RUSK COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

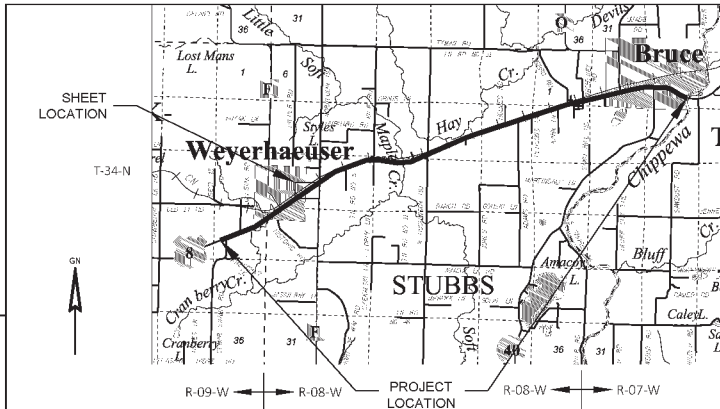
DIMENSIONING FOR THE RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE REFERENCE LINE.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

EXISTING ACCESS CONTROL ALONG USH 8 ESTABLISHED FROM QUIT CLAIM DEED VOLUME 185, PAGE 795, AS DOCUMENT #184184.

GREINER & ASSOCIATES, INC.
CONSULTING ENGINEERS
Stevens Point • Fond du Lac
1220 Wilshire Boulevard North • Stevens Point, WI 54481
(715) 541-4363 • fax (715) 341-1956



I, AARON PARKS, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: *Aaron Parks* DATE: 05/19/21
PRINT NAME: AARON PARKS
REGISTRATION NUMBER: S-2861
THIS PLAT IS APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION
SIGNATURE: *Heather L. Dreisel* DATE: 12/01/21
PRINT NAME: HEATHER L. DREISEL

362308
MARY BERG
REGISTER OF DEEDS
RUSK COUNTY, WI
RECORDED ON
12/07/2021 12:06 PM
CABINET:1 ENV:10A

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 1570-06-22 - 4.05
AMENDMENT NO: __

TRANSPORTATION PROJECT PLAT NO: 1570-06-22 - 4.06

PART OF THE SW1/4-NW1/4 AND THE SE1/4-NW1/4 OF SECTION 17, T34N, R08W, TOWN OF STUBBS, RUSK COUNTY, WISCONSIN

USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

THE STATE OF WISCONSIN HAS DEEMED IT NECESSARY TO MONUMENT RIGHT-OF-WAY INTERESTS PREVIOUSLY ACQUIRED UNDER SECTIONS 82.01, 82.31(1), 82.31(2) AND 84.09 OF THE WISCONSIN STATE STATUTES AND DOES HEREBY CERTIFY THE DEPARTMENT OF TRANSPORTATION HAS SURVEYED THE HIGHWAY RIGHT-OF-WAY; THAT THIS PLAT IS A TRUE AND CORRECT REPRESENTATION OF THE EXTERIOR BOUNDARIES OF THE LAND SURVEYED AND SHOWS ACCURATE MEASUREMENTS THEREOF.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN SPOONER.

EXISTING USH 8 RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:
R/W PROJECT NUMBER 8641.
EXISTING NORWEGIAN ROAD RIGHT-OF-WAY SHOWN HEREIN IS BASED ON CERTIFIED SURVEY MAP NUMBER 774.

UTILITY INFORMATION IS NOT SHOWN ON THIS PLAT.

MAP OF SURVEY
V-1855
RECEIVED 3-24-2014

R/W CURVE
327-328
R=2876.04'
LCH=712.12'
LCB=N61°50'26"E
L=713.95'

R/W CURVE
617-618
R=2799.79'
LCH=651.33'
LCB=S61°54'54"W
L=652.81'

R/L CURVE DATA
CURVE 2 - USH 8
PI 608+30.48
Y 551419.88
X 738167.76
DELTA 02°00'00"
T 335.50'
L 667.96'
R 2864.79'
LCH 666.45'
LCB N61°54'54"E

CONVENTIONAL ABBREVIATIONS

ACRES	AC	POINT OF TANGENCY	PT
AHEAD	AH	POINT OF BEGINNING	POB
ALUMINUM	ALUM	POINT OF CURVATURE	PC
BACK	BK	POINT OF COMPOUND CURVE	PCC
CENTERLINE	C/L	POINT OF INTERSECTION	PI
CERTIFIED SURVEY MAP	CSM	PROPERTY LINE	PL
COUNTY	CO	RECORDED AS	{100'}
COUNTY TRUNK HIGHWAY	CTH	REFERENCE LINE	R/L
DISTANCE	DIST	RIGHT	RT
CORNER	COR	RIGHT OF WAY	R/W
DOCUMENT NUMBER	DOC	SECTION	SEC
EASTBOUND	EB	SEPTIC VENT	SEPV
EXISTING	EX	SQUARE FEET	SF
GRID NORTH	GN	STATE TRUNK HIGHWAY	STH
LAND CONTRACT	LC	STATION	STA
LEFT	LT	TEMPORARY LIMITED	TLE
MONUMENT	MON	EASEMENT	
NUMBER	NO	TRANSPORTATION PROJECT	TPP
OUTLOT	OL	PLAT	
PAGE	P	VOLUME	V

EXISTING MONUMENTS					
POINT	Y	X	DESCRIPTION	STA	OFFSET
6170	550900.41	736655.33	1" IRON PIPE	592+91.63	435.56' LT
6171	551080.99	736948.80	1" IRON PIPE	596+35.87	416.65' LT
6172	551131.11	737046.71	1" IRON PIPE	597+44.88	402.00' LT

NO TYPE 2 MONUMENT SET DUE TO CLOSE PROXIMITY OF EXISTING MONUMENT.

QUARTER LINE
N88°02'25"W 5263.19'
SEC CORNER TO 621
N88°02'25"W 4991.34'

NW-SW
SEC. 17, T34N, R08W

Y 550243.66
X 736519.66
STEEL SURVEY NAIL

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB



SCALE, FEET



POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS) RUSK COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

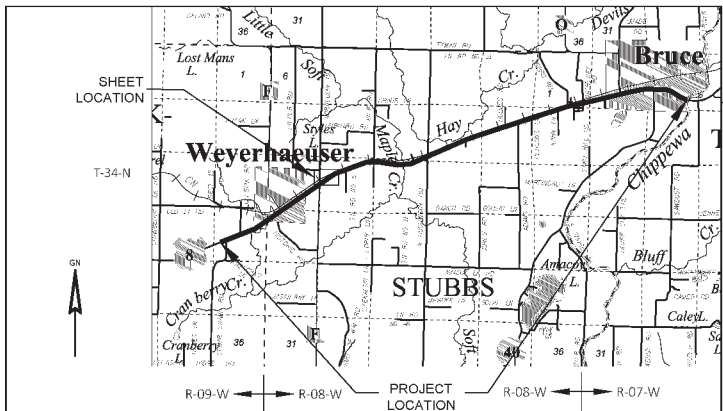
DIMENSIONING FOR THE RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE REFERENCE LINE.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	●
QUARTER LINE	---	SECTION CORNER MONUMENT		NON-MONUMENTED R/W POINT	○
SIXTEENTH LINE	---	GEODETIC SURVEY MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	IP ●
NEW REFERENCE LINE	---	SIXTEENTH CORNER MONUMENT		OFF-PREMISE SIGN	
NEW R/W LINE	---	SIGN		COMPENSABLE	
EXISTING R/W OR HE LINE	---	ELECTRIC POLE		NON-COMPENSABLE	
PROPERTY LINE	---	TELEPHONE POLE			
LOT, TIE & OTHER MINOR LINES	---	PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)			
SLOPE INTERCEPT	---	ACCESS RESTRICTED BY ACQUISITION			
CORPORATE LIMITS	---	NO ACCESS (BY STATUTORY AUTHORITY)			
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	---	ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)			
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---	NO ACCESS (NEW HIGHWAY)			
TEMPORARY LIMITED EASEMENT AREA	---	PARCEL NUMBER 25		UTILITY NUMBER 40	
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---	PARALLEL OFFSETS			
TRANSMISSION STRUCTURES	---				
BUILDING					
BRIDGE					



I, AARON PARKS, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: DATE: 05/19/21
PRINT NAME: AARON PARKS
REGISTRATION NUMBER: S-2861
THIS PLAT IS APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION
SIGNATURE: DATE: 02/07/22
PRINT NAME: HEATHER L. DREISEL

GREMMER & ASSOCIATES, INC.
SURVEYING ENGINEERS
Stevens Point • Fond du Lac
120 Main Street North • Stevens Point, WI 54481
(715) 341-4365 • Fax (715) 341-4366



CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT	○ ● (SET)
QUARTER LINE	---	SECTION CORNER MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	IP
SIXTEENTH LINE	---	GEODETIC SURVEY MONUMENT		SIXTEENTH CORNER MONUMENT	
NEW REFERENCE LINE	---	SIGN		OFF-PREMISE SIGN	
NEW R/W LINE	---	COMPENSABLE		NON-COMPENSABLE	
EXISTING R/W OR HE LINE	---	ELECTRIC POLE		TELEPHONE POLE	
PROPERTY LINE	---	PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)		ACCESS RESTRICTED BY ACQUISITION	
LOT, TIE & OTHER MINOR LINES	---	NO ACCESS (BY STATUTORY AUTHORITY)		NO ACCESS (BY PREVIOUS PROJECT OR CONTROL)	
SLOPE INTERCEPT	---	NO ACCESS (NEW HIGHWAY)		PARCEL NUMBER (25)	UTILITY NUMBER (40)
CORPORATE LIMITS	---	PARALLEL OFFSETS			
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	---				
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---				
TEMPORARY LIMITED EASEMENT AREA	---				
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---				
TRANSMISSION STRUCTURES	---				
BUILDING		TO BE REMOVED			
BRIDGE		CULVERT			

TOWN

TRANSPORTATION PROJECT PLAT NO: 1570-06-22 - 4.07

PART OF THE NW1/4-NE1/4 OF SECTION 17, T34N, R08W, TOWN OF STUBBS, RUSK COUNTY, WISCONSIN

RELOCATION ORDER: USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

CONVENTIONAL UTILITY SYMBOLS

WATER
GAS
TELEPHONE
OVERHEAD TRANSMISSION LINES
ELECTRIC
CABLE TELEVISION
FIBER OPTIC
SANITARY SEWER
STORM SEWER
ELECTRIC TOWER

W
G
T
OH
E
TV
FO
SAN
SS
X

TLE-GRADING 0.034 AC

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	R/W ACRES REQUIRED			TLE ACRES
			NEW	EXISTING	TOTAL	
6	WISCONSIN CENTRAL LTD.	TLE	0	0	0	0.034

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT.

CONVENTIONAL ABBREVIATIONS

ACRES	AC	POINT OF TANGENCY	PT
AHEAD	AH	POINT OF BEGINNING	POB
ALUMINUM	ALUM	POINT OF CURVATURE	PC
BACK	BK	POINT OF COMPOUND CURVE	PCC
CENTERLINE	C/L	POINT OF INTERSECTION	PI
CERTIFIED SURVEY MAP	CSM	PROPERTY LINE	PL
COUNTY	CO	RECORDED AS	(100')
COUNTY TRUNK HIGHWAY	CTH	REFERENCE LINE	R/L
DISTANCE	DIST	RIGHT	RT
CORNER	COR	RIGHT OF WAY	R/W
DOCUMENT NUMBER	DOC	SECTION	SEC
EASTBOUND	EB	SEPTIC VENT	SEPV
EXISTING	EX	SQUARE FEET	SF
GRID NORTH	GN	STATE TRUNK HIGHWAY	STH
LAND CONTRACT	LC	STATION	STA
LEFT	LT	TEMPORARY LIMITED EASEMENT	TLE
MONUMENT	MON	TRANSPORTATION PROJECT	TPP
NUMBER	NO	PLAT	P
OUTLOT	OL	VOLUME	V
PAGE	P		

CURVE DATA

LONG CHORD LCH
LONG CHORD BEARING LCB
RADIUS R
DEGREE OF CURVE D
CENTRAL ANGLE Δ/DELTA
LENGTH OF CURVE L
TANGENT T
DIRECTION AHEAD DA
DIRECTION BACK DB

SE-NW
SEC. 17, T34N, R08WY 547684.69
X 736461.17
STEEL SURVEY NAILSECTION LINE
S88°56'36"E 2642.94'Y 547635.94
X 739103.66
ALUMINUM MONUMENTNE-NW
SEC. 17, T34N, R08WNW-NE
SEC. 17, T34N, R08WSW-NE
SEC. 17, T34N, R08W

STUBBS

NE-NE
SEC. 17, T34N, R08W

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS) RUSK COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN SPOONER.

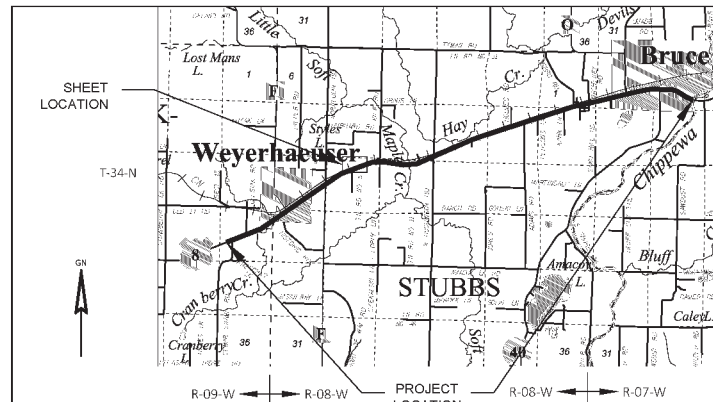
PARCEL AND UTILITY IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE TPP DETAIL PAGES.

EXISTING USH 8 RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE: R/W PROJECT NUMBERS 8641. EXISTING HWY 8 ROAD RIGHT-OF-WAY SHOWN HEREIN IS BASED ON STATE STATUTES AND EXISTING PAVEMENT.

GN

SCALE, FEET

0 100 200



GOREMME & ASSOCIATES, INC.
CONSULTING ENGINEERS
Steven Point • Fond du Lac
120 Wiskind Boulevard North • Stevens Point, WI 54481
(715) 341-4262 • Fax: (715) 341-1960

I, AARON PARKS, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

WISCONSIN
AARON PARKS
S-2861
STEVENS POINT
WIS
LAND SURVEYOR

SIGNATURE: DATE: 07/15/21
PRINT NAME: AARON PARKS
REGISTRATION NUMBER: S-2861

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION
SIGNATURE: DATE: 09/28/21
PRINT NAME: HEATHER L. DREISEL

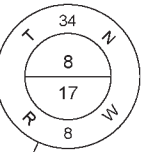
TRANSPORTATION PROJECT PLAT NO: 1570-06-22 - 4.08

PART OF THE NW1/4-NW1/4 OF SECTION 16 AND THE NE1/4-NE1/4 OF SECTION 17, ALL IN T34N, R08W, TOWN OF STUBBS, RUSK COUNTY, WISCONSIN

USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

THE STATE OF WISCONSIN HAS DEEMED IT NECESSARY TO MONUMENT RIGHT-OF-WAY INTERESTS PREVIOUSLY ACQUIRED UNDER SECTIONS 82.01, 82.31(1), 82.31(2) AND 84.09 OF THE WISCONSIN STATE STATUTES AND DOES HEREBY CERTIFY THE DEPARTMENT OF TRANSPORTATION HAS SURVEYED THE HIGHWAY RIGHT-OF-WAY; THAT THIS PLAT IS A TRUE AND CORRECT REPRESENTATION OF THE EXTERIOR BOUNDARIES OF THE LAND SURVEYED AND SHOWS ACCURATE MEASUREMENTS THEREOF.

Y 552902.82
X 739143.96
ALUMINUM MONUMENT



PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPANCY LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN SPOONER.

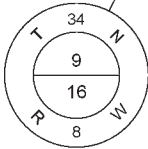
EXISTING USH 8 RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:
R/W PROJECT NUMBER 8641.
EXISTING KIEF ROAD RIGHT-OF-WAY SHOWN HEREIN IS BASED ON PHYSICAL EVIDENCE AND EXISTING CENTERLINE.

UTILITY INFORMATION IS NOT SHOWN ON THIS PLAT.

363070
MARY BERG
REGISTER OF DEEDS
RUSK COUNTY, WI
RECORDED ON
02/18/2022 09:14 AM
CABINET:1 ENV:

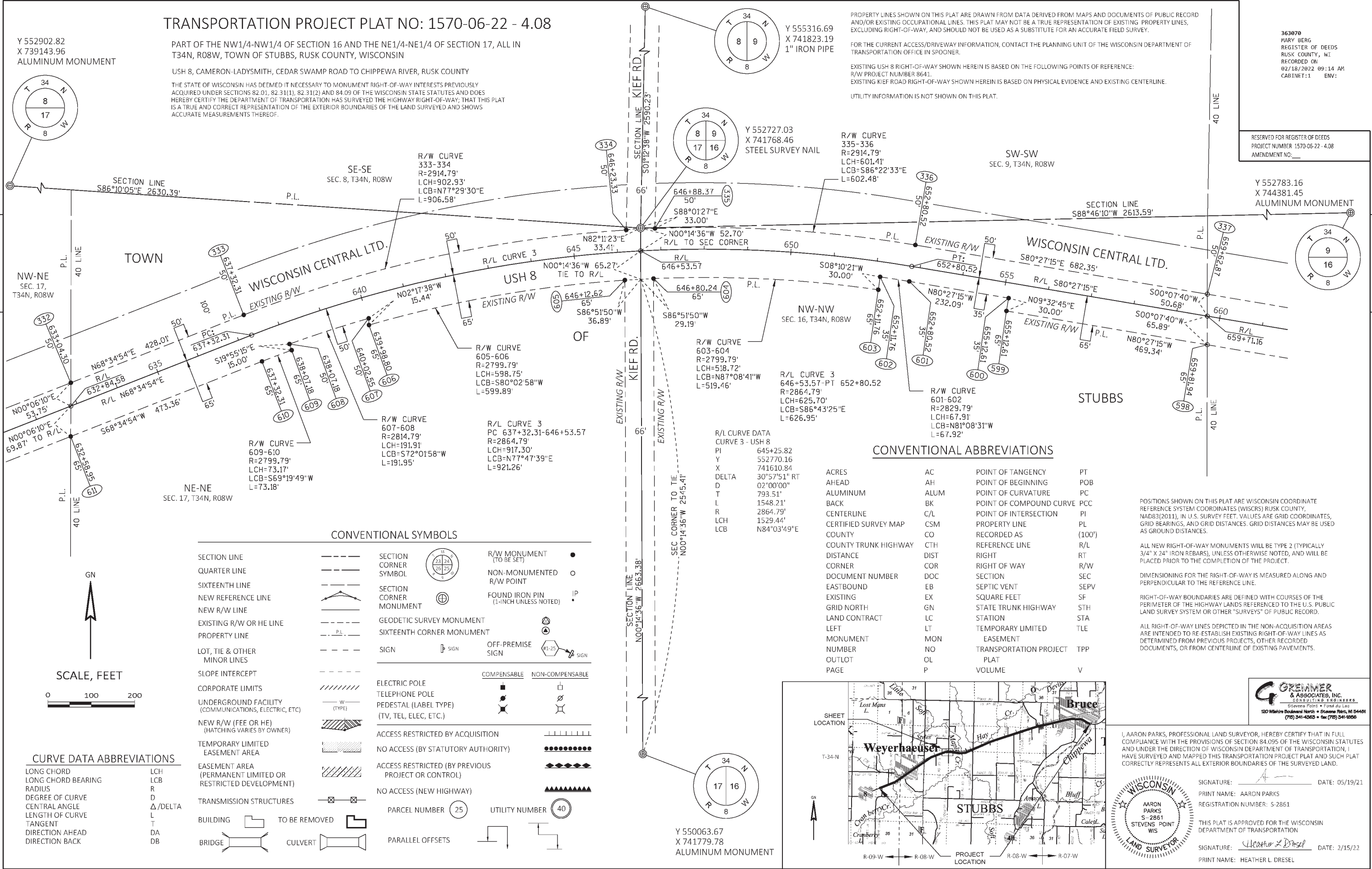
RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 1570-06-22 - 4.08
AMENDMENT NO.:

Y 552783.16
X 744381.45
ALUMINUM MONUMENT



4

4



CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	●
QUARTER LINE	---	SECTION CORNER MONUMENT		NON-MONUMENTED R/W POINT	○
SIXTEENTH LINE	---	SECTION CORNER MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	•
NEW REFERENCE LINE	---	GEODETIC SURVEY MONUMENT		OFF-PREMISE SIGN	
NEW R/W LINE	---	SIXTEENTH CORNER MONUMENT		COMPENSABLE	
EXISTING R/W OR HE LINE	---	SIGN		NON-COMPENSABLE	
PROPERTY LINE	---				
LOT, TIE & OTHER MINOR LINES	---				
SLOPE INTERCEPT	---				
CORPORATE LIMITS	---				
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	---				
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---				
TEMPORARY LIMITED EASEMENT AREA	---				
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---				
TRANSMISSION STRUCTURES	---				
BUILDING	---				
BRIDGE	---				
TO BE REMOVED	---				
CULVERT	---				
PARCEL NUMBER	---				
PARALLEL OFFSETS	---				

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL ABBREVIATIONS

ACRES	AC	POINT OF TANGENCY	PT
AHEAD	AH	POINT OF BEGINNING	POB
ALUMINUM	ALUM	POINT OF CURVATURE	PC
BACK	BK	POINT OF COMPOUND CURVE	PCC
CENTERLINE	C/L	POINT OF INTERSECTION	PI
CERTIFIED SURVEY MAP	CSM	PROPERTY LINE	PL
COUNTY	CO	RECORDED AS	(100')
COUNTY TRUNK HIGHWAY	CTH	REFERENCE LINE	R/L
DISTANCE	DIST	RIGHT	RT
CORNER	COR	RIGHT OF WAY	R/W
DOCUMENT NUMBER	DOC	SECTION	SEC
EASTBOUND	EB	SEPTIC VENT	SEPV
EXISTING	EX	SQUARE FEET	SF
GRID NORTH	GN	STATE TRUNK HIGHWAY	STH
LAND CONTRACT	LC	STATION	STA
LEFT	LT	TEMPORARY LIMITED	TLE
MONUMENT	MON	EASEMENT	
NUMBER	NO	TRANSPORTATION PROJECT	TPP
OUTLOT	OL	PLAT	
PAGE	P	VOLUME	V

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS) RUSK COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

DIMENSIONING FOR THE RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE REFERENCE LINE.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

GREMMER & ASSOCIATES, INC.
SURVEYING ENGINEERS
Stevens Point • Fond du Lac
120 Main Street North • Stevens Point, WI 54481
(715) 341-4365 • Fax (715) 341-4366

I, AARON PARKS, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: DATE: 05/19/21
PRINT NAME: AARON PARKS
REGISTRATION NUMBER: S-2861
THIS PLAT IS APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION
SIGNATURE: DATE: 2/15/22
PRINT NAME: HEATHER L. DREISEL



CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	●
QUARTER LINE	---	NON-MONUMENTED R/W POINT	○		
SIXTEENTH LINE	---	SECTION CORNER MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	IP
NEW REFERENCE LINE	---	GEODETIC SURVEY MONUMENT			
NEW R/W LINE	---	SIXTEENTH CORNER MONUMENT			
EXISTING R/W OR HE LINE	---	SIGN		OFF-PREMISE SIGN	
PROPERTY LINE	---				
LOT, TIE & OTHER MINOR LINES	---				
SLOPE INTERCEPT	---				
CORPORATE LIMITS	---	ELECTRIC POLE		COMPENSABLE	
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	---	TELEPHONE POLE		NON-COMPENSABLE	
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---	PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)			
TEMPORARY LIMITED EASEMENT AREA	---	ACCESS RESTRICTED BY ACQUISITION			
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---	NO ACCESS (BY STATUTORY AUTHORITY)			
TRANSMISSION STRUCTURES	---	ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)			
BUILDING	---	NO ACCESS (NEW HIGHWAY)			
BRIDGE	---	PARCEL NUMBER (25)		UTILITY NUMBER (40)	
CULVERT	---	PARALLEL OFFSETS			

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

TOWN

R/W CURVE
341-342
R=4700.00'
LCH=595.88'
LCB=N72°42'47"E
L=596.28'

WISCONSIN CENTRAL LTD.

CONVENTIONAL ABBREVIATIONS

ACRES	AC	POINT OF TANGENCY	PT
AHEAD	AH	POINT OF BEGINNING	POB
ALUMINUM	ALUM	POINT OF CURVATURE	PC
BACK	BK	POINT OF COMPOUND CURVE	PCC
CENTERLINE	C/L	POINT OF INTERSECTION	PI
CERTIFIED SURVEY MAP	CSM	PROPERTY LINE	PL
COUNTY	CO	RECORDED AS	(100')
COUNTY TRUNK HIGHWAY	CTH	REFERENCE LINE	R/L
DISTANCE	DIST	RIGHT	RT
CORNER	COR	RIGHT OF WAY	R/W
DOCUMENT NUMBER	DOC	SECTION	SEC
EASTBOUND	EB	SEPTIC VENT	SEPV
EXISTING	EX	SQUARE FEET	SF
GRID NORTH	GN	STATE TRUNK HIGHWAY	STH
LAND CONTRACT	LC	STATION	STA
LEFT	LT	TEMPORARY LIMITED EASEMENT	TLE
MONUMENT	MON	TRANSPORTATION PROJECT	TPP
NUMBER	NO	PLAT	
OUTLOT	OL	VOLUME	V
PAGE	P		

R/W CURVE
593-594
R=3919.72'
LCH=322.18'
LCB=S67°54'02"W
L=322.27'

R/L CURVE 4
686+23.65-PT 689+01.10
R=3819.72'
LCH=277.39'
LCB=N67°37'34"E
L=277.45'

R/L CURVE DATA
CURVE 4 - USH 8
PI 678+02.23
Y 552220.38
X 744880.15
DELTA 34°00'02" LT
D 01°30'00"
T 1167.83'
L 2266.70
R 3819.72'
LCH 2233.59'
LCB N82°32'44"E
PC 666+34.40
DB S80°27'15"E

TRANSPORTATION PROJECT PLAT NO: 1570-06-22 - 4.10

PART OF THE SE1/4-SE1/4 OF SECTION 9, THE SW1/4-SW1/4 OF SECTION 10 AND THE NE1/4-NE1/4 OF SECTION 16, ALL IN T34N, R08W, TOWN OF STUBBS, RUSK COUNTY, WISCONSIN

USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

THE STATE OF WISCONSIN HAS DEEMED IT NECESSARY TO MONUMENT RIGHT-OF-WAY INTERESTS PREVIOUSLY ACQUIRED UNDER SECTIONS 82.01, 82.31(1), 82.31(2) AND 84.09 OF THE WISCONSIN STATE STATUTES AND DOES HEREBY CERTIFY THE DEPARTMENT OF TRANSPORTATION HAS SURVEYED THE HIGHWAY RIGHT-OF-WAY; THAT THIS PLAT IS A TRUE AND CORRECT REPRESENTATION OF THE EXTERIOR BOUNDARIES OF THE LAND SURVEYED AND SHOWS ACCURATE MEASUREMENTS THEREOF.

363072
MARY BERG
REGISTER OF DEEDS
RUSK COUNTY, WI
RECORDED ON
02/18/2022 09:18 AM
CABINET:1 ENV:15 B

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 1570-06-22 - 4.10
AMENDMENT NO:___



SCALE, FEET



POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS) RUSK COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

DIMENSIONING FOR THE RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE REFERENCE LINE.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER 'SURVEYS' OF PUBLIC RECORD.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

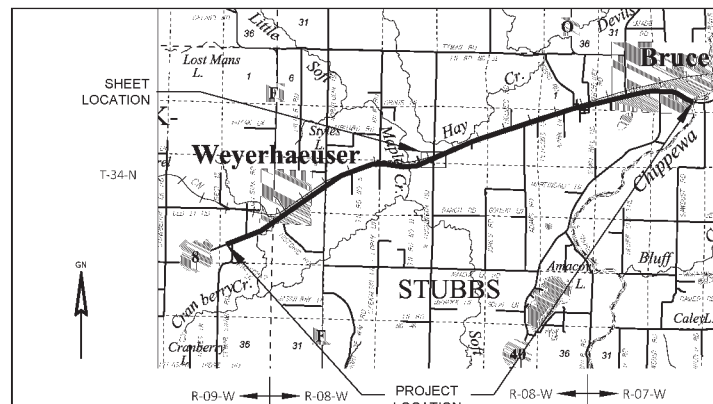
FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN SPOONER.

EXISTING USH 8 RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE: R/W PROJECT NUMBER 8641. EXISTING N. AND S. HUTCHINSON ROAD RIGHT-OF-WAY SHOWN HEREIN IS BASED ON EXISTING CENTERLINE.

UTILITY INFORMATION IS NOT SHOWN ON THIS PLAT.

Y 552839.57
X 746994.33
COMPUTED FROM TIES

Y 550208.53
X 746987.93
COMPUTED FROM TIES



I, AARON PARKS, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.



SIGNATURE: _____ DATE: 05/19/21

PRINT NAME: AARON PARKS

REGISTRATION NUMBER: S-2861

THIS PLAT IS APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION

SIGNATURE: _____ DATE: 2/15/22

PRINT NAME: HEATHER L. DRESEL

CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	●
QUARTER LINE	---	SECTION CORNER MONUMENT		NON-MONUMENTED R/W POINT	○
SIXTEENTH LINE	---	GEODETIC SURVEY MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	IP
NEW REFERENCE LINE	---	SIXTEENTH CORNER MONUMENT		OFF-PREMISE SIGN	
NEW R/W LINE	---	SIGN		COMPENSABLE	
EXISTING R/W OR HE LINE	---	PARCEL NUMBER	25	NON-COMPENSABLE	
PROPERTY LINE	---	UTILITY NUMBER	40		
LOT, TIE & OTHER MINOR LINES	---	PARALLEL OFFSETS			
SLOPE INTERCEPT	---				
CORPORATE LIMITS	---				
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)	---				
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---				
TEMPORARY LIMITED EASEMENT AREA	---				
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---				
TRANSMISSION STRUCTURES	---				
BUILDING	---				
BRIDGE	---				
CULVERT	---				

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS) RUSK COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

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ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN SPOONER.

EXISTING USH 8 RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE: R/W PROJECT NUMBER 8641. EXISTING LARGER ROAD RIGHT-OF-WAY SHOWN HEREIN IS BASED ON PHYSICAL EVIDENCE AND EXISTING CENTERLINE.

UTILITY INFORMATION IS NOT SHOWN ON THIS PLAT.

TRANSPORTATION PROJECT PLAT NO: 1570-06-22 - 4.11

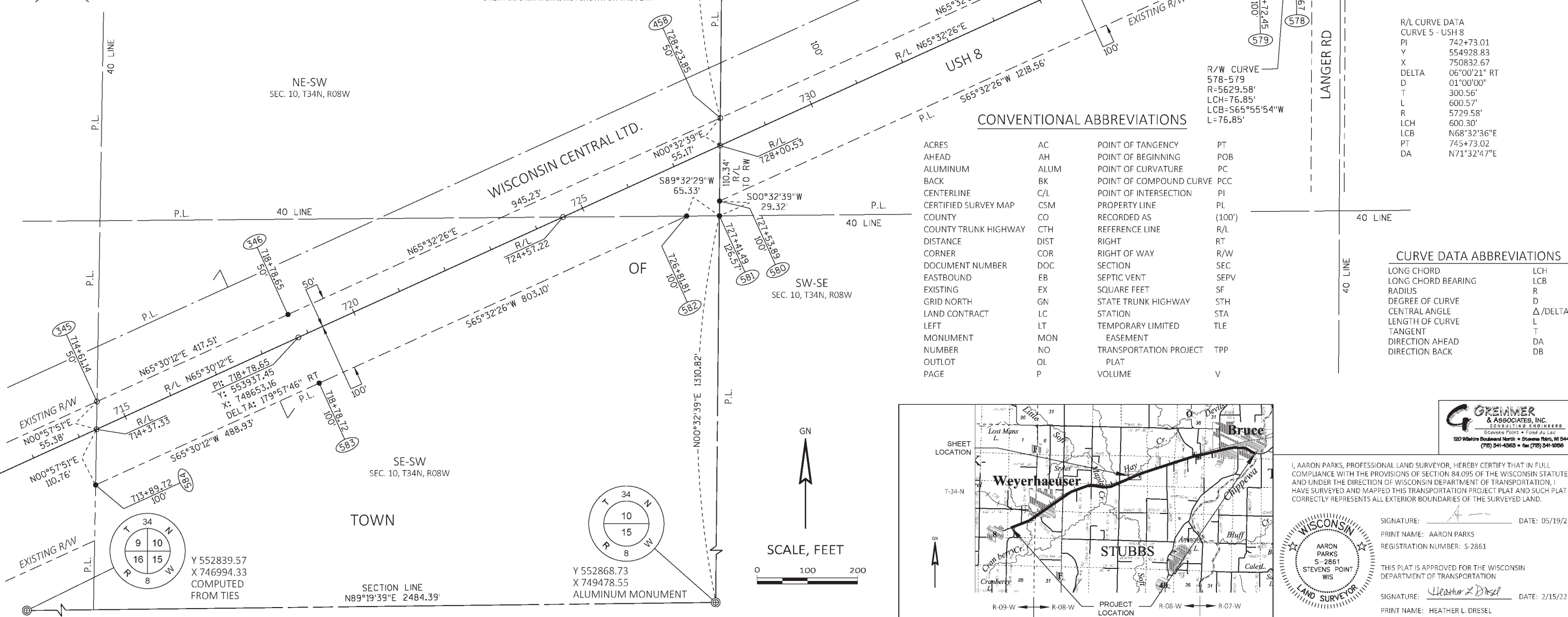
PART OF THE SE1/4-SW1/4, THE NE1/4-SW1/4 AND THE NW1/4-SE1/4 OF SECTION 10, ALL IN T34N, R08W, TOWN OF STUBBS, RUSK COUNTY, WISCONSIN

USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

THE STATE OF WISCONSIN HAS DEEMED IT NECESSARY TO MONUMENT RIGHT-OF-WAY INTERESTS PREVIOUSLY ACQUIRED UNDER SECTIONS 82.01, 82.31(1), 82.31(2) AND 84.09 OF THE WISCONSIN STATE STATUTES AND DOES HEREBY CERTIFY THE DEPARTMENT OF TRANSPORTATION HAS SURVEYED THE HIGHWAY RIGHT-OF-WAY, THAT THIS PLAT IS A TRUE AND CORRECT REPRESENTATION OF THE EXTERIOR BOUNDARIES OF THE LAND SURVEYED AND SHOWS ACCURATE MEASUREMENTS THEREOF.

363073
MARY BERG
REGISTER OF DEEDS
RUSK COUNTY, WI
RECORDED ON
02/18/2022 09:19 AM
CABINET:1 ENV:15 B

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 1570-06-22 - 4.11
AMENDMENT NO.:



SECTION LINE		SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	
QUARTER LINE				NON-MONUMENTED R/W POINT	
SIXTEENTH LINE		SECTION CORNER MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	IP •
NEW REFERENCE LINE		GEODETTIC SURVEY MONUMENT			
NEW R/W LINE		SIXTEENTH CORNER MONUMENT			
EXISTING R/W OR HE LINE		SIGN		OFF-PREMISE SIGN	
PROPERTY LINE					
LOT, TIE & OTHER MINOR LINES					
SLOPE INTERCEPT					
CORPORATE LIMITS					
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)		ELECTRIC POLE		COMPENSABLE	NON-COMPENSABLE
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)		TELEPHONE POLE			
TEMPORARY LIMITED EASEMENT AREA		PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)			
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)		ACCESS RESTRICTED BY ACQUISITION			
TRANSMISSION STRUCTURES		NO ACCESS (BY STATUTORY AUTHORITY)			
BUILDING		ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)			
BRIDGE		NO ACCESS (NEW HIGHWAY)			
CULVERT		PARCEL NUMBER		UTILITY NUMBER	
		PARALLEL OFFSETS			

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ / DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

SE-NW
SEC. 11, T34N, R08W

Y 558055.51
X 754484.57
ALUMINUM MONUMENT

PART OF THE SE1/4-NW1/4 AND THE SW1/4-NE1/4 OF SECTION 11, T34N, R08W,
TOWN OF STUBBS, RUSK COUNTY, WISCONSIN

USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

THE STATE OF WISCONSIN HAS DEEMED IT NECESSARY TO MONUMENT RIGHT-OF-WAY INTERESTS PREVIOUSLY ACQUIRED UNDER SECTIONS 82.01, 82.31(1), 82.31(2) AND 84.09 OF THE WISCONSIN STATE STATUTES AND DOES HEREBY CERTIFY THE DEPARTMENT OF TRANSPORTATION HAS SURVEYED THE HIGHWAY RIGHT-OF-WAY; THAT THIS PLAT IS A TRUE AND CORRECT REPRESENTATION OF THE EXTERIOR BOUNDARIES OF THE LAND SURVEYED AND SHOWS ACCURATE MEASUREMENTS THEREOF.

362368
MARY BERG
REGISTER OF DEEDS
RUSK COUNTY, WI
RECORDED ON
12/13/2021 12:53 PM
CABINET:1 ENV:11 B

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 1570-06-22 - 4.13
AMENDMENT NO: _____

GN

SCALE, FEET



CONVENTIONAL ABBREVIATIONS

ACRES	AC	POINT OF TANGENCY	PT
AHEAD	AH	POINT OF BEGINNING	POB
ALUMINUM	ALUM	POINT OF CURVATURE	PC
BACK	BK	POINT OF COMPOUND CURVE	PCC
CENTERLINE	C/L	POINT OF INTERSECTION	PI
CERTIFIED SURVEY MAP	CSM	PROPERTY LINE	PL
COUNTY	CO	RECORDED AS	(100')
COUNTY TRUNK HIGHWAY	CTH	REFERENCE LINE	R/L
DISTANCE	DIST	RIGHT	RT
CORNER	COR	RIGHT OF WAY	R/W
DOCUMENT NUMBER	DOC	SECTION	SEC
EASTBOUND	EB	SEPTIC VENT	SEPV
EXISTING	EX	SQUARE FEET	SF
GRID NORTH	GN	STATE TRUNK HIGHWAY	STH
LAND CONTRACT	LC	STATION	STA
LEFT	LT	TEMPORARY LIMITED	TLE
MONUMENT	MON	EASEMENT	
NUMBER	NO	TRANSPORTATION PROJECT	TPP
OUTLOT	OL	PLAT	
PAGE	P	VOLUME	V

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN SPOONER.

EXISTING USH 8 RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING
POINTS OF REFERENCE:

EXISTING GURESKI LANE AND ADAMS ROAD RIGHT-OF-WAY SHOWN HEREIN IS
BASED ON EXISTING CENTERLINES.

UTILITY INFORMATION IS NOT SHOWN ON THIS PLAT.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS) RUSK COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

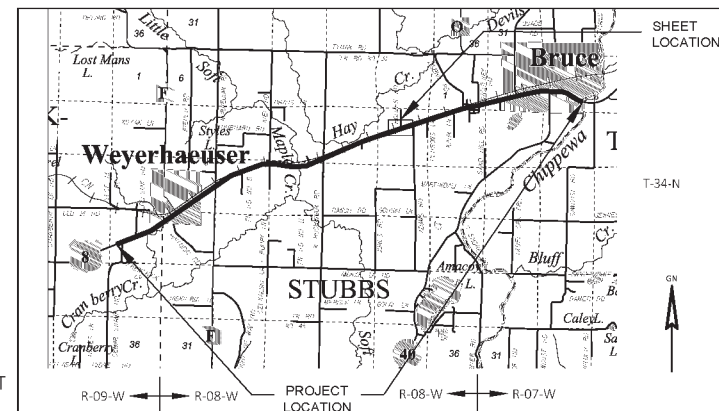
ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

DIMENSIONING FOR THE RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE REFERENCE LINE

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

Y 552871.63
X 754525.78
ALUMINUM MONUMENT



I, AARON PARKS, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: A DATE: 05/19/21

PRINT NAME: AARON PARKS

REGISTRATION NUMBER: S-2861

THIS PLAT IS APPROVED FOR THE WISCONSIN
DEPARTMENT OF TRANSPORTATION

Wagner & Daniel

SIGNATURE: Heather L. Dreisel DATE: 12/06/21
PRINT NAME: HEATHER L. DRESEL

FILE NAME : P:\PROJECTS CURRENT\RUSK\WISDOT\USH 8 TPP - CAMERON TO LADYSMITH\SURVEY\USH8 413-RP.DWG

PLOT DATE : 12/7/2021 3:02 PM

PLOT BY : AARON PARKS

PLOT NAME :

PLOT SCALE : 1 IN:200 FT

570-06-22 - 4.13

SECTION LINE		SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	
QUARTER LINE		SECTION CORNER MONUMENT		NON-MONUMENTED R/W POINT	
SIXTEENTH LINE		GEODETIC SURVEY MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	
NEW REFERENCE LINE		SIXTEENTH CORNER MONUMENT			
NEW R/W LINE		SIGN		OFF-PREMISE SIGN	
EXISTING R/W OR HE LINE					
PROPERTY LINE					
LOT, TIE & OTHER MINOR LINES					
SLOPE INTERCEPT					
CORPORATE LIMITS					
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)					
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)					
TEMPORARY LIMITED EASEMENT AREA					
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)					
TRANSMISSION STRUCTURES					
BUILDING					
TO BE REMOVED					
BRIDGE					
CULVERT					
		PARCEL NUMBER		UTILITY NUMBER	
		PARALLEL OFFSETS			

ACRES	AC	POINT OF TANGENCY	PT
AHEAD	AH	POINT OF BEGINNING	POB
ALUMINUM	ALUM	POINT OF CURVATURE	PC
BACK	BK	POINT OF COMPOUND CURVE	PCC
CENTERLINE	C/L	POINT OF INTERSECTION	PI
CERTIFIED SURVEY MAP	CSM	PROPERTY LINE	PL
COUNTY	CO	RECORDED AS	(100'
COUNTY TRUNK HIGHWAY	CTH	REFERENCE LINE	R/L
DISTANCE	DIST	RIGHT	RT
CORNER	COR	RIGHT OF WAY	R/W
DOCUMENT NUMBER	DOC	SECTION	SEC
EASTBOUND	EB	SEPTIC VENT	SEPV
EXISTING	EX	SQUARE FEET	SF
GRID NORTH	GN	STATE TRUNK HIGHWAY	STH
LAND CONTRACT	LC	STATION	STA
LEFT	LT	TEMPORARY LIMITED	TLE
MONUMENT	MON	EASEMENT	
NUMBER	NO	TRANSPORTATION PROJECT	TPP
OUTLOT	OL	PLAT	
PAGE	P	VOLUME	V

EXISTING MONUMENTS					
POINT	Y	X	DESCRIPTION	STA	OFFSET
1003	558067.45	759761.28	1" IRON PIPE	837+35.46	150.68' LT
6173	557370.72	758353.17	1" IRON PIPE	821+79.19	64.25' RT
6174	557803.15	759650.05	1" IRON PIPE	835+46.28	64.81' RT
*6175	557792.25	759617.43	1" IRON PIPE	835+11.89	64.82' RT
6191	558322.55	760525.52	1" IRON PIPE	845+37.77	153.57' LT
6192	558198.63	760154.90	1" IRON PIPE	841+50.37	150.50' LT

Y 558009.10
X 757026.49
3" IRON PIPE

40 LINE

SE-SW
SEC. 1, T34N, R08W

SECTION LINE
N88°45'40"E 2612.99'

P.L.

Y 558065.60
X 759638.86
1" IRON PIPE

POINT	Y	X	DESCRIPTION	STA	OFFSET
1003	558067.45	759761.28	1" IRON PIPE	837+35.46	150.68' LT
6173	557370.72	758353.17	1" IRON PIPE	821+79.19	64.25' RT
6174	557803.15	759650.05	1" IRON PIPE	835+46.28	64.81' RT
*6175	557792.25	759617.43	1" IRON PIPE	835+11.89	64.82' RT
6191	558322.55	760525.52	1" IRON PIPE	845+37.77	153.57' LT
6192	558198.63	760154.90	1" IRON PIPE	841+50.37	150.50' LT

*NO TYPE 2 MONUMENT SET DUE TO CLOSE PROXIMITY OF EXISTING MONUMENT.

OF

US 8

NE-NW
SEC. 12, T34N, R08W

KAHN RD

QUARTER LINE

MAP OF SURVEY
V-1901
DATED 11-19-2014

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

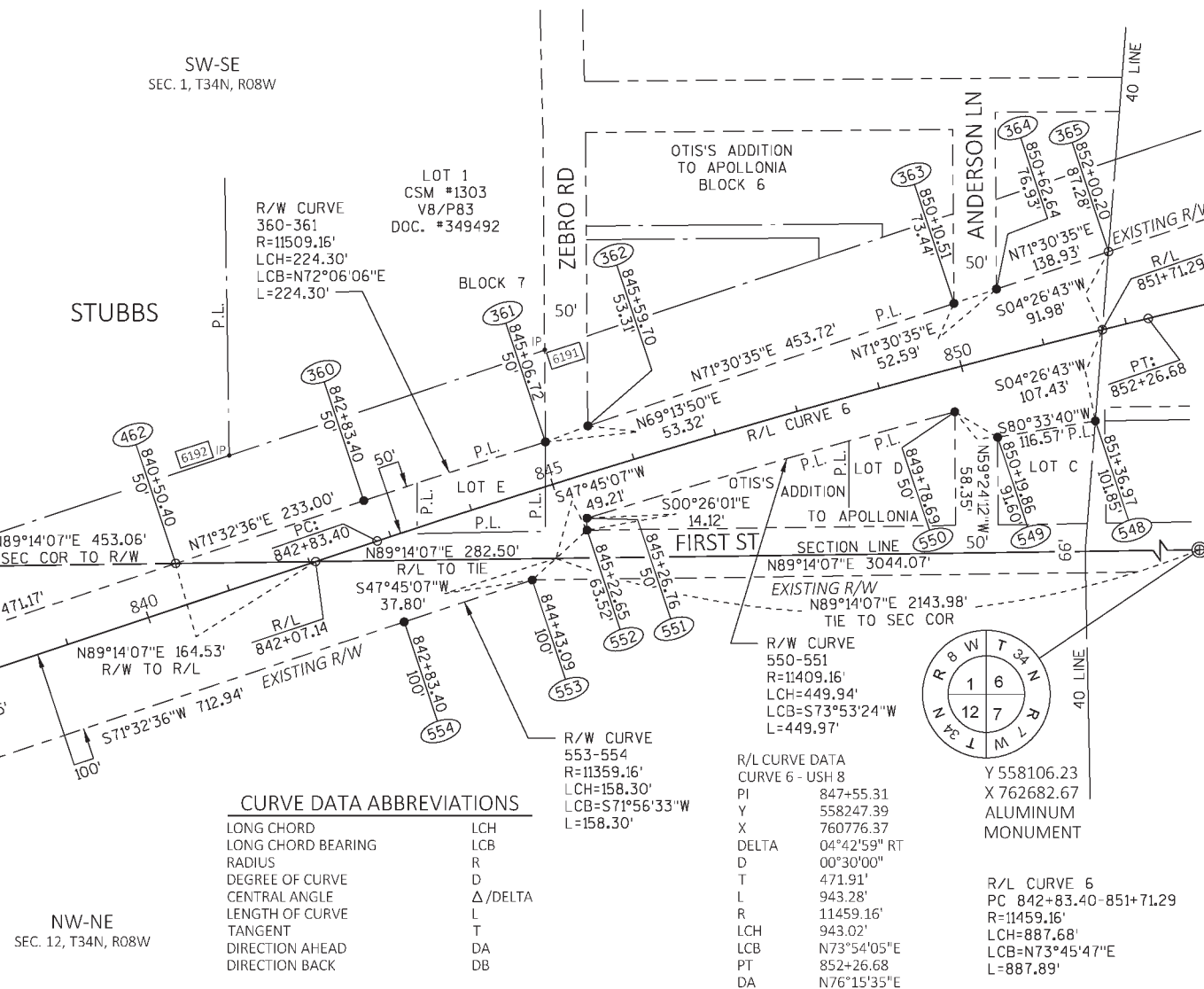
FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN SPOONER.

EXISTING US 8 RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE: R/W PROJECT NUMBER 8641, CERTIFIED SURVEY MAP NUMBER 1303 AND MAP OF SURVEY NUMBER V-1901.
EXISTING KAHN ROAD RIGHT-OF-WAY SHOWN HEREIN IS BASED ON MAP OF SURVEY NUMBER V-1901.
EXISTING ADDITIONAL TOWN ROADS RIGHT-OF-WAY SHOWN HEREIN ARE BASED ON OTIS'S ADDITION TO APOLLONIA AND CERTIFIED SURVEY MAP NUMBER 1303.

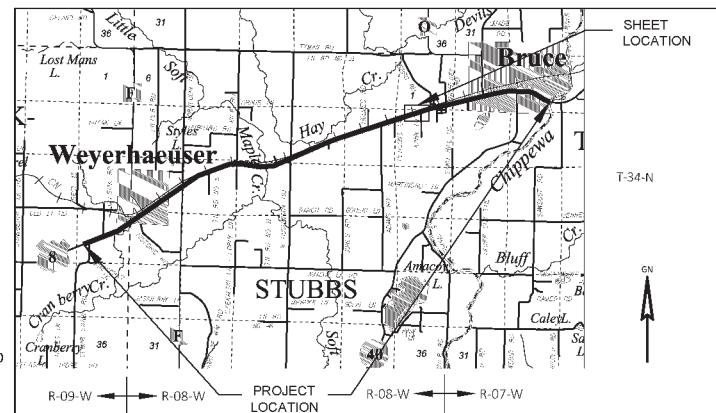
UTILITY INFORMATION IS NOT SHOWN ON THIS PLAT.

THE STATE OF WISCONSIN HAS DEEMED IT NECESSARY TO MONUMENT RIGHT-OF-WAY INTERESTS PREVIOUSLY ACQUIRED UNDER SECTIONS 82.01, 82.31(1), 82.31(2) AND 84.09 OF THE WISCONSIN STATE STATUTES AND DOES HEREBY CERTIFY THE DEPARTMENT OF TRANSPORTATION HAS SURVEYED THE HIGHWAY RIGHT-OF-WAY; THAT THIS PLAT IS A TRUE AND CORRECT REPRESENTATION OF THE EXTERIOR BOUNDARIES OF THE LAND SURVEYED AND SHOWS ACCURATE MEASUREMENTS THEREOF.

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 1570-06-22 - 4.15
AMENDMENT NO: _____



LONG CHORD	LCH	L=158.30'
LONG CHORD BEARING	LCB	
RADIUS	R	
DEGREE OF CURVE	D	
CENTRAL ANGLE	Δ /DELTA	
LENGTH OF CURVE	L	
TANGENT	T	
DIRECTION AHEAD	DA	
DIRECTION BACK	DB	



I, AARON PARKS, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

PRINT NAME: HEATHER L. DRESEL

TRANSPORTATION PROJECT PLAT NO: 1570-06-22 - 4.16

BEING PART OF ORIGINAL PLAT OF APPOLONIA, LOCATED IN THE SE1/4-SE1/4 OF SECTION 1, T34N, R08W, TOWN OF STUBBS, RUSK COUNTY, WISCONSIN

USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

THE STATE OF WISCONSIN HAS DEEMED IT NECESSARY TO MONUMENT RIGHT-OF-WAY INTERESTS PREVIOUSLY ACQUIRED UNDER SECTIONS 82.01, 82.31(1), 82.31(2) AND 84.09 OF THE WISCONSIN STATE STATUTES AND DOES HEREBY CERTIFY THE DEPARTMENT OF TRANSPORTATION HAS SURVEYED THE HIGHWAY RIGHT-OF-WAY; THAT THIS PLAT IS A TRUE AND CORRECT REPRESENTATION OF THE EXTERIOR BOUNDARIES OF THE LAND SURVEYED AND SHOWS ACCURATE MEASUREMENTS THEREOF.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS) RUSK COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

DIMENSIONING FOR THE RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE REFERENCE LINE.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

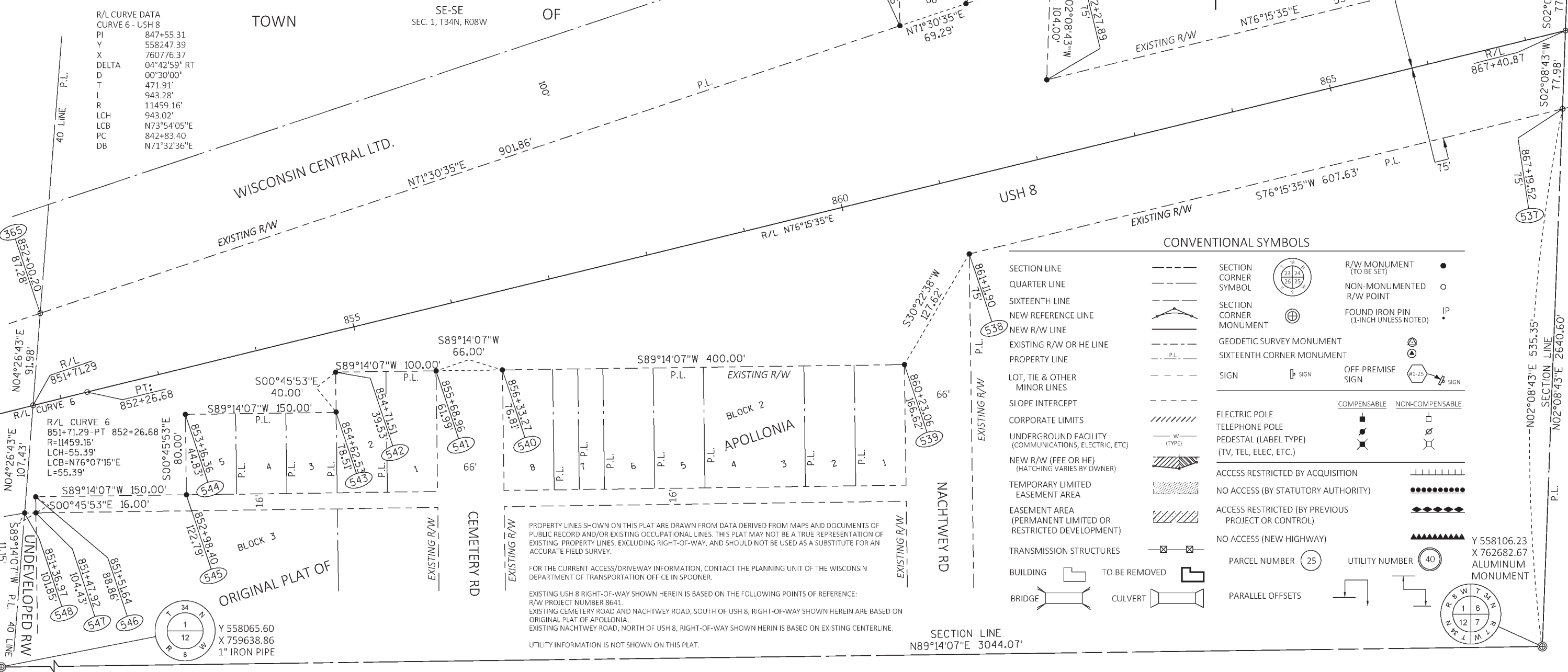
CONVENTIONAL ABBREVIATIONS

AC	POINT OF TANGENCY	PT
AHEAD	POINT OF BEGINNING	POB
ALUMINUM	POINT OF CURVATURE	PC
BACK	POINT OF COMPOUND CURVE	PCC
CENTERLINE	POINT OF INTERSECTION	PI
CERTIFIED SURVEY MAP	PROPERTY LINE	PL
COUNTY	RECORDED AS	(100')
COUNTY TRUNK HIGHWAY	REFERENCE LINE	R/L
DISTANCE	RIGHT	RT
CORNER	RIGHT OF WAY	R/W
DOCUMENT NUMBER	SECTION	SEC
EASTBOUND	SEPTIC VENT	SEPV
EXISTING	SQUARE FEET	SF
GRID NORTH	STATE TRUNK HIGHWAY	STH
LAND CONTRACT	STATION	STA
LEFT	TEMPORARY LIMITED	TLE
MONUMENT	EASEMENT	
NUMBER	TRANSPORTATION PROJECT	TPP
OUTLOT	PLAT	
PAGE	VOLUME	V

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

R/L CURVE DATA	
CURVE 6 - USH 8	
PI	847+55.31
Y	558247.39
X	760776.37
DELTA	04°42'59" RT
D	00°30'00"
T	471.91'
L	943.28'
R	11459.16'
LCH	943.02'
LCB	N73°54'05"E
PC	842+83.40
DB	N71°32'36"E



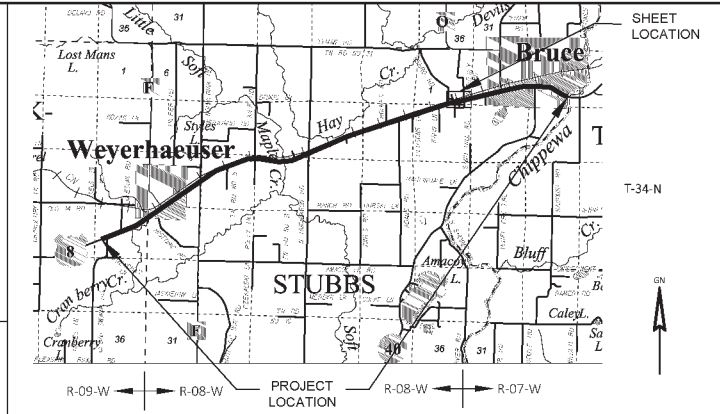
I, AARON PARKS, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.09S OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.



SIGNATURE: [Signature] DATE: 05/19/21
PRINT NAME: AARON PARKS
REGISTRATION NUMBER: S-2861

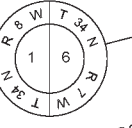
THIS PLAT IS APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION

SIGNATURE: [Signature] DATE: 2/07/22
PRINT NAME: HEATHER L. DRESEL



363023
MARY BERG
REGISTER OF DEEDS
RUSK COUNTY, WI
RECORDED ON
02/11/2022 08:03 AM
CABINET:1 ENV:

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 1570-06-22 - 4.16
AMENDMENT NO.:



Y 560744.97
X 762781.51
ALUMINUM
MONUMENT

867+62.21
75'

S02°08'43"W
77.98'

S02°08'43"W
77.98'

S02°08'43"W
77.98'

S02°08'43"W
77.98'

S02°08'43"W
77.98'

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S02°08'43"W
77.98'

S02°08'43"W
77.98'

USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

THE STATE OF WISCONSIN HAS DEEMED IT NECESSARY TO MONUMENT RIGHT-OF-WAY INTERESTS PREVIOUSLY ACQUIRED UNDER SECTIONS 82.01, 82.31(1), 82.31(2) AND 84.09 OF THE WISCONSIN STATE STATUTES AND DOES HEREBY CERTIFY THE DEPARTMENT OF TRANSPORTATION HAS SURVEYED THE HIGHWAY RIGHT-OF-WAY; THAT THIS PLAT IS A TRUE AND CORRECT REPRESENTATION OF THE EXTERIOR BOUNDARIES OF THE LAND SURVEYED AND SHOWS ACCURATE MEASUREMENTS THEREOF.

A detailed map of the project location in the Stubb's area. The map shows a network of roads and trails, with the project route highlighted in a thick black line. Key locations labeled include Weyerhaeuser, Bruce, Hay, Spies, Little, Lost Mans L., Devil, Chippewa, Bluff, Caleb, and Stubb's. The map also shows various landmarks such as Crayberry Cr., Cranberry L., and Amos L. A legend at the bottom indicates the project location with arrows pointing to R-09-W, R-08-W, and R-07-W.

— SHEET
LOCATION

A circular professional seal for Aaron Parks, a Wisconsin Land Surveyor. The seal features a five-pointed star on the left side. The text "WISCONSIN" is arched across the top, and "LAND SURVEYOR" is arched across the bottom. In the center, the text reads "AARON PARKS", "S-2861", and "STEVENS POINT WIS".

SIGNATURE: _____ DATE: 05/19/21

PRINT NAME: AARON PARKS
REGISTRATION NUMBER: 5-2861

THIS PLAT IS APPROVED FOR THE WISCONSIN
DEPARTMENT OF TRANSPORTATION

SIGNATURE: Heather L. Dase DATE: 2/15/22

PRINT NAME: HEATHER L. DRESEL

NE-SW
SEC. 6, T34N, R07W

Y 563438.18
X 765502.84
F 7/8" REBAR

363078
MARY BERG
REGISTER OF DEEDS
RUSK COUNTY, WI
RECORDED ON
02/18/2022 11:13 AM
CABINET:1 ENV:18 A

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 1570-06-22 - 4.17
AMENDMENT NO: _____

4

4

TOWN

VILLAGE

VILLAGE OF BRUCE
INDUSTRIAL PARK
MAP OF SURVEY

MAP OF SURVEY
L-581
FILED 10-1-1997
P.L. [6]

OF CURVE DATA ABBREVIATIONS

LONG CHORD
LONG CHORD BEARING
RADIUS
DEGREE OF CURVE
CENTRAL ANGLE
LENGTH OF CURVE
TANGENT
DIRECTION AHEAD
DIRECTION BACK

LCH
LCB
R
D
 Δ /DELTA
L
T
DA
DB

SE-SW
SEC. 6, T34N, R07W

BRUCE

GN



SCALE, FEET

0	100	200
---	-----	-----

Y 557987.41
X 765559.16
ALUMINUM
MONUMENT

CONVENTIONAL ABBREVIATIONS

ACRES	AC	POINT OF TANGENCY	PT
AHEAD	AH	POINT OF BEGINNING	POB
ALUMINUM	ALUM	POINT OF CURVATURE	PC
BACK	BK	POINT OF COMPOUND CURVE	PCC
CENTERLINE	C/L	POINT OF INTERSECTION	PI
CERTIFIED SURVEY MAP	CSM	PROPERTY LINE	PL
COUNTY	CO	RECORDED AS	(100')
COUNTY TRUNK HIGHWAY	CTH	REFERENCE LINE	R/L
DISTANCE	DIST	RIGHT	RT
CORNER	COR	RIGHT OF WAY	R/W
DOCUMENT NUMBER	DOC	SECTION	SEC
EASTBOUND	EB	SEPTIC VENT	SEPV
EXISTING	EX	SQUARE FEET	SF
GRID NORTH	GN	STATE TRUNK HIGHWAY	STH
LAND CONTRACT	LC	STATION	STA
LEFT	LT	TEMPORARY LIMITED	TLE
MONUMENT	MON	EASEMENT	
NUMBER	NO	TRANSPORTATION PROJECT	TPP
OUTLOT	OL	PLAT	
PAGE	P	VOLUME	V

CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	○
QUARTER LINE	---	SECTION CORNER MONUMENT		NON-MONUMENTED R/W POINT	○
SIXTEENTH LINE	---	GEODETIC SURVEY MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	•
NEW REFERENCE LINE		SIXTEENTH CORNER MONUMENT			
NEW R/W LINE	---	SIGN		OFF-PREMISE SIGN	
EXISTING R/W OR HE LINE	---				
PROPERTY LINE	---				
LOT, TIE & OTHER MINOR LINES	---				
SLOPE INTERCEPT	---				
CORPORATE LIMITS	---				
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	---				
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)					
TEMPORARY LIMITED EASEMENT AREA					
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)					
TRANSMISSION STRUCTURES					
BUILDING					
TO BE REMOVED					
BRIDGE					
CULVERT					
		PARCEL NUMBER		UTILITY NUMBER	
		PARALLEL OFFSETS			

SECTION LINE
N02°08'43"E 2640.60'

370
 867+62.21
 15
 EXISTING R/W
 N02°08'43"E
 77.98'
 N02°08'43"E
 77.98'
 EXISTING R/W
 867

N02°08'43"E 535.35'
SEC CORNER TO R/W
P.L.

Y 558106.23
X 762682.67
ALUMINUM
MONUMENT

SECTION LINE
N87°38'05"W 2878.94'

FILE NAME : P:\PROJECTS CURRENT\RUSK\WISDOT\USH 8 TPP - CAMERON TO LADYSMITH\SURVEY\USH8 417-RP.DWG

PLOT DATE : 2/15/2022 2:26 PM

PLOT BY : AARON PARKS

PLOT NAME :

PLOT SCALE : 1 IN:200 FT

570-06-22 - 4.17

CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	●
QUARTER LINE	---	SECTION CORNER MONUMENT		NON-MONUMENTED R/W POINT	○
SIXTEENTH LINE	---	GEODETIC SURVEY MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	IP
NEW REFERENCE LINE	---	SIXTEENTH CORNER MONUMENT		OFF-PREMISE SIGN	
NEW R/W LINE	---	SIGN		COMPENSABLE	
EXISTING R/W OR HE LINE	---			NON-COMPENSABLE	
PROPERTY LINE	---				
LOT, TIE & OTHER MINOR LINES	---				
SLOPE INTERCEPT	---				
CORPORATE LIMITS	---				
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)	---	ELECTRIC POLE			
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---	TELEPHONE POLE			
TEMPORARY LIMITED EASEMENT AREA	---	PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)			
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---	ACCESS RESTRICTED BY ACQUISITION			
TRANSMISSION STRUCTURES	---	NO ACCESS (BY STATUTORY AUTHORITY)			
BUILDING		ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)			
TO BE REMOVED		NO ACCESS (NEW HIGHWAY)			
BRIDGE		PARCEL NUMBER (25)			
CULVERT		UTILITY NUMBER (40)			
		PARALLEL OFFSETS			



PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN SPOONER.

EXISTING USH 8 RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:
R/W PROJECT NUMBER 8641, R/W PROJECT 8590-15-21 AND ASSESSOR PLAT NO. 1 TO THE VILLAGE OF BRUCE.
EXISTING STH 40 RIGHT-OF-WAY SHOWN HEREIN IS BASED ON R/W PROJECT 8590-15-21.
EXISTING S. RICHARD LANE RIGHT-OF-WAY SHOWN HEREIN IS BASED ON ASSESSOR PLAT NO. 1 TO VILLAGE OF BRUCE.
EXISTING BRUCE LAKE STREET RIGHT-OF-WAY SHOWN HEREIN IS BASED ON CSM #221-2-20.

UTILITY INFORMATION IS NOT SHOWN ON THIS PLAT.

EXISTING ACCESS CONTROL ALONG USH 8 IS ESTABLISHED FROM R/W PROJECT 8590-15-21.

TRANSPORTATION PROJECT PLAT NO: 1570-06-22 - 4.18

PART OF THE NE1/4-SW1/4, THE NW1/4-SE1/4 AND THE SW1/4-SE1/4 OF SECTION 6, ALL IN T34N, R07W, VILLAGE OF BRUCE, RUSK COUNTY, WISCONSIN

USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

THE STATE OF WISCONSIN HAS DEEMED IT NECESSARY TO MONUMENT RIGHT-OF-WAY INTERESTS PREVIOUSLY ACQUIRED UNDER SECTIONS 82.01, 82.31(1), 82.31(2) AND 84.09 OF THE WISCONSIN STATE STATUTES AND DOES HEREBY CERTIFY THE DEPARTMENT OF TRANSPORTATION HAS SURVEYED THE HIGHWAY RIGHT-OF-WAY; THAT THIS PLAT IS A TRUE AND CORRECT REPRESENTATION OF THE EXTERIOR BOUNDARIES OF THE LAND SURVEYED AND SHOWS ACCURATE MEASUREMENTS THEREOF.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS) RUSK COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

363024
MARY BERG
REGISTER OF DEEDS
RUSK COUNTY, WI
RECORDED ON
02/11/2022 08:04 AM
CABINET:1 ENV:13 A

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 1570-06-22 - 4.18
AMENDMENT NO.:

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB



SCALE, FEET



WISCONSIN CENTRAL LTD.

VILLAGE OF BRUCE INDUSTRIAL PARK

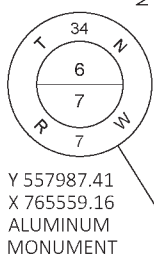
VILLAGE OF BRUCE INDUSTRIAL PARK

MAP OF SURVEY
L-581
FILED 10-1-1997

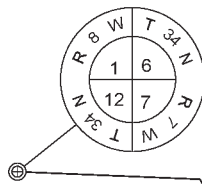
MAP OF SURVEY
L-581
FILED 10-1-1997

CONVENTIONAL ABBREVIATIONS

ACRES	AC	POINT OF TANGENCY	PT
AHEAD	AH	POINT OF BEGINNING	POB
ALUMINUM	ALUM	POINT OF CURVATURE	PC
BACK	BK	POINT OF COMPOUND CURVE	PCC
CENTERLINE	C/L	POINT OF INTERSECTION	PI
CERTIFIED SURVEY MAP	CSM	PROPERTY LINE	PL
COUNTY	CO	RECORDED AS	(100')
COUNTY TRUNK HIGHWAY	CTH	REFERENCE LINE	R/L
DISTANCE	DIST	RIGHT	RT
CORNER	COR	RIGHT OF WAY	R/W
DOCUMENT NUMBER	DOC	SECTION	SEC
EASTBOUND	EB	SEPTIC VENT	SEPV
EXISTING	EX	SQUARE FEET	SF
GRID NORTH	GN	STATE TRUNK HIGHWAY	STH
LAND CONTRACT	LC	STATION	STA
LEFT	LT	TEMPORARY LIMITED	TLE
MONUMENT	MON	EASEMENT	
NUMBER	NO	TRANSPORTATION PROJECT	TPP
OUTLOT	OL	PLAT	
PAGE	P	VOLUME	V



Y 558106.23
X 762682.67
ALUMINUM
MONUMENT



SECTION LINE
S87°38'05"E 2878.94'

TRANSPORTATION PROJECT PLAT NO: 1570-06-22 - 4.19

PART OF THE NE1/4-SE1/4 OF SECTION 6, T34N, R07W, VILLAGE OF BRUCE, RUSK COUNTY, WISCONSIN

USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

THE STATE OF WISCONSIN HAS DEEMED IT NECESSARY TO MONUMENT RIGHT-OF-WAY INTERESTS PREVIOUSLY ACQUIRED UNDER SECTIONS 82.01, 82.31(1), 82.31(2) AND 84.09 OF THE WISCONSIN STATE STATUTES AND DOES HEREBY CERTIFY THE DEPARTMENT OF TRANSPORTATION HAS SURVEYED THE HIGHWAY RIGHT-OF-WAY, THAT THIS PLAT IS A TRUE AND CORRECT REPRESENTATION OF THE EXTERIOR BOUNDARIES OF THE LAND SURVEYED AND SHOWS ACCURATE MEASUREMENTS THEREOF.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN SPOONER.

EXISTING USH 8 AND OTHER VILLAGE ROAD RIGHT-OF-WAYS SHOWN HEREIN ARE BASED ON THE FOLLOWING POINTS OF REFERENCE: R/W PROJECT NUMBER 8641, MAP OF SURVEY V-1258 AND V-1903, CSM #1059-6-314, ASSESSOR PLAT NO. 1 TO THE VILLAGE OF BRUCE AND STAHPMAN'S ADDITION TO VILLAGE OF BRUCE.

UTILITY INFORMATION IS NOT SHOWN ON THIS PLAT.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS) RUSK COUNTY, MAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

DIMENSIONING FOR THE RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE REFERENCE LINE.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

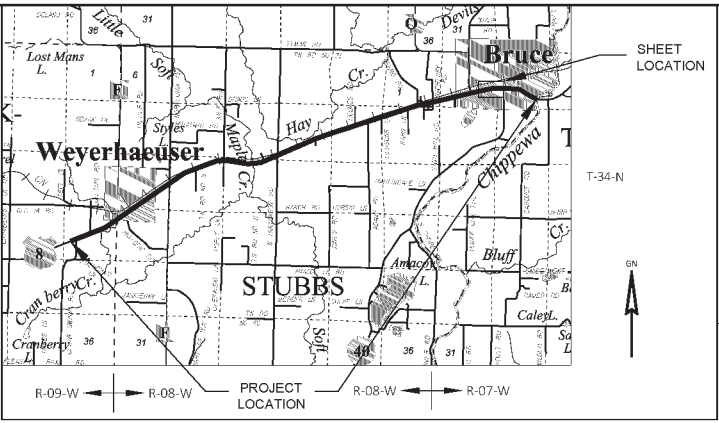
GN

SCALE, FEET

0 50 100

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB



I, AARON PARKS, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

WISCONSIN LAND SURVEYOR

AARON PARKS
S-2861
STEVENS POINT
WIS

SIGNATURE: _____ DATE: 05/19/21

PRINT NAME: AARON PARKS

REGISTRATION NUMBER: S-2861

THIS PLAT IS APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION

SIGNATURE: _____ DATE: 2/07/22

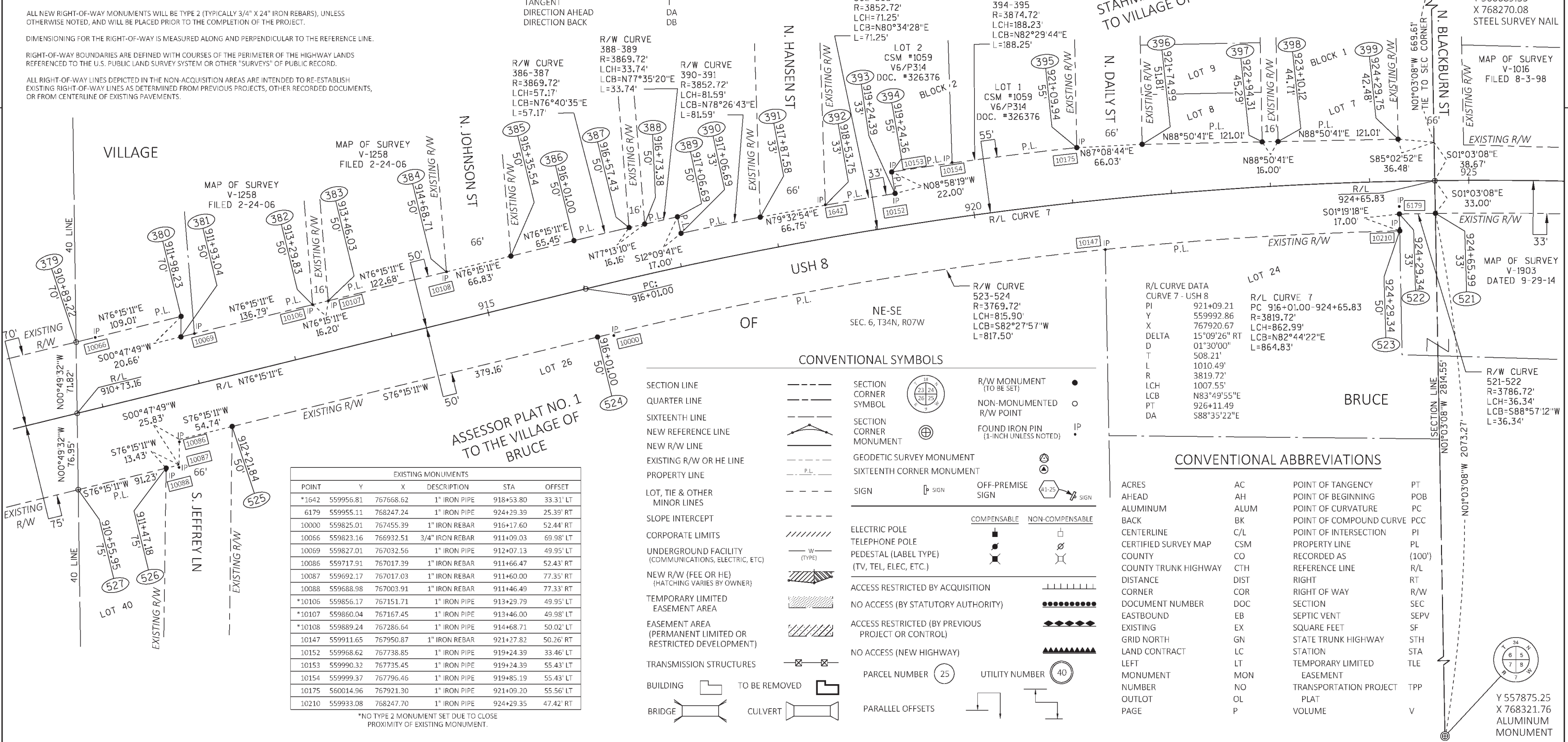
PRINT NAME: HEATHER L. DRESEL

363025
MARY BERG
REGISTER OF DEEDS
RUSK COUNTY, WI
RECORDED ON
02/11/2022 08:05 AM
CABINET:1 ENW:13 B

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 1570-06-22-4.19
AMENDMENT NO. _____



STAHPMAN'S ADDITION
TO VILLAGE OF BRUCE



Y 560689.33
X 768270.08
STEEL SURVEY NAIL

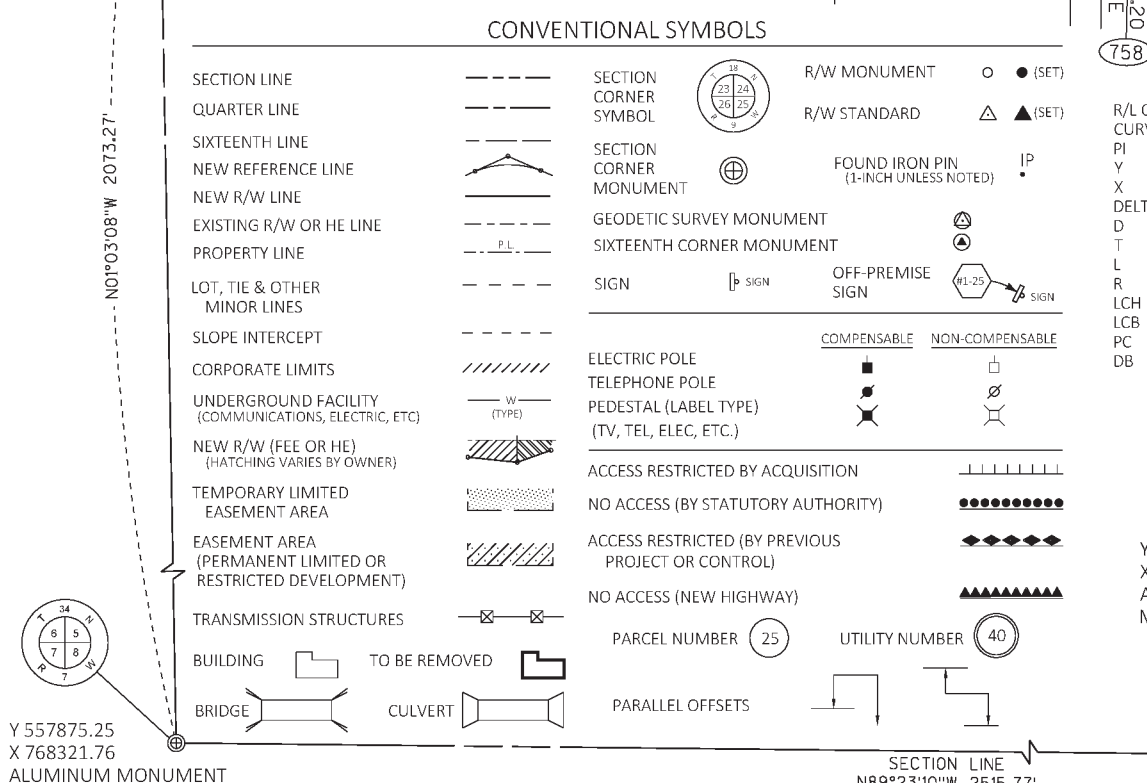
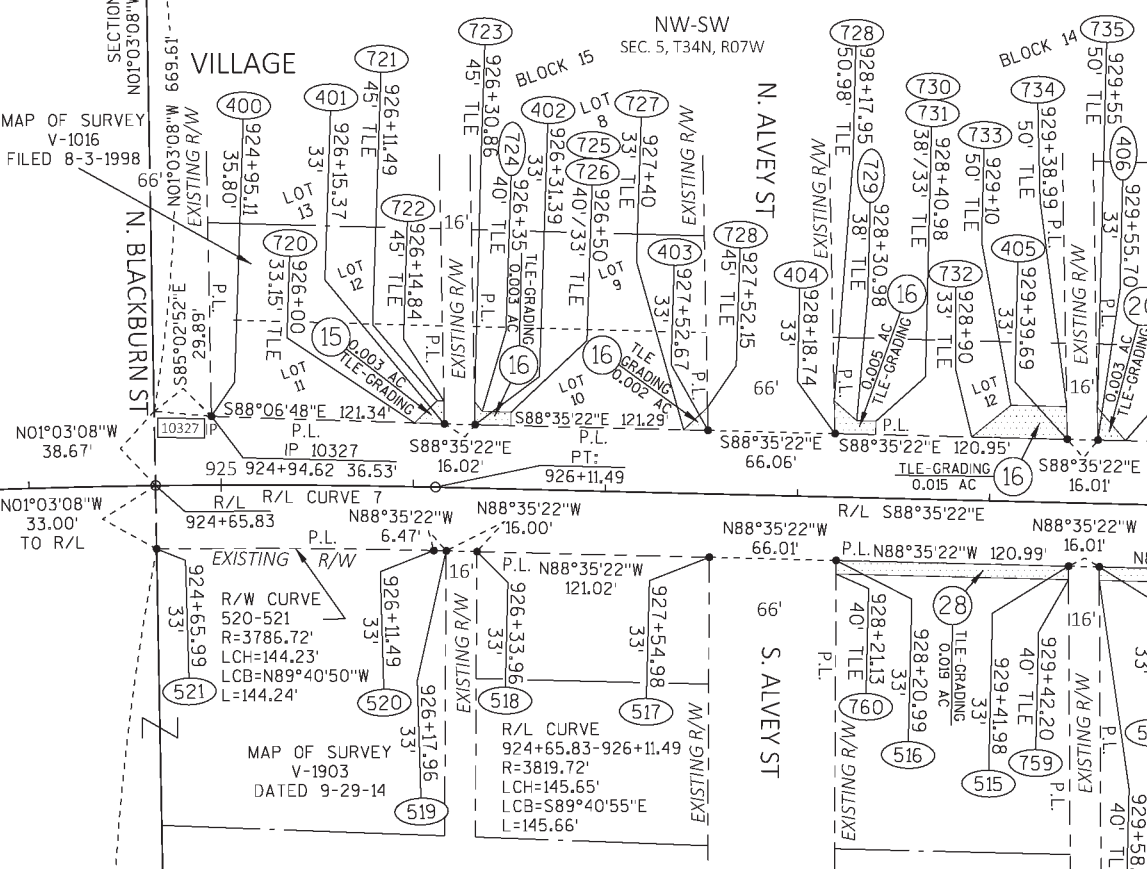
TRANSPORTATION PROJECT PLAT NO: 1570-06-22 - 4.20

PART OF LOTS 10 AND 11 OF BLOCK 15, LOTS 11 AND 12 OF BLOCK 14 AND LOTS 13 AND 14 OF BLOCK 13 OF PLAT OF BRUCE, AND THE NW1/4 - SW1/4 OF SECTION 5, T34N, R07W, VILLAGE OF BRUCE, RUSK COUNTY, WISCONSIN
RELOCATION ORDER: USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.



Y 557875.25
X 768321.76
ALUMINUM MONUMENT

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS) RUSK COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

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DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

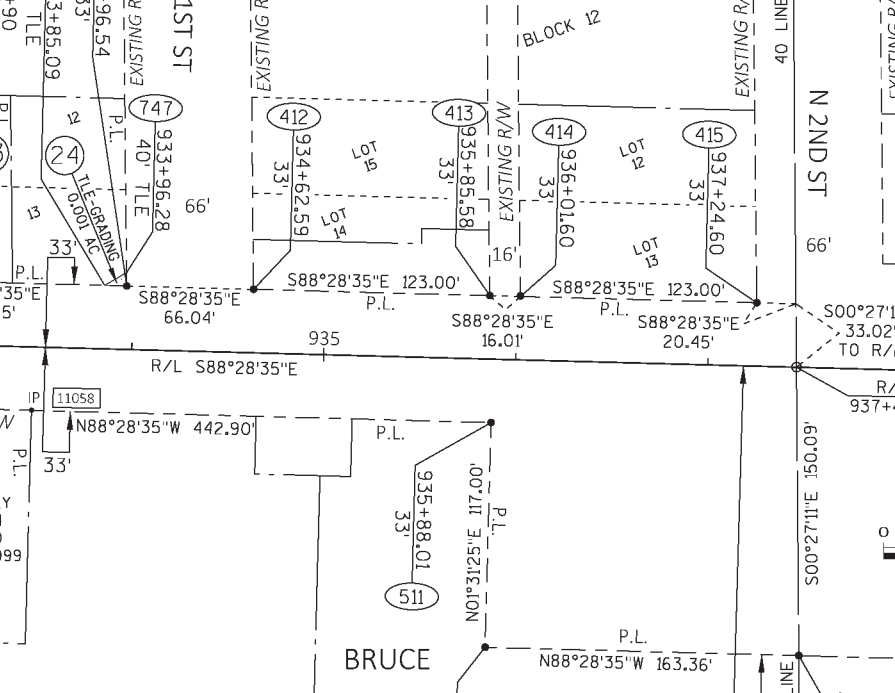
FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN SPOONER.

PARCEL AND UTILITY IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE TPP DETAIL PAGES.

EXISTING USH 8, STH 40 AND VILLAGE STREETS RIGHT-OF-WAYS SHOWN HEREIN ARE BASED ON THE FOLLOWING POINTS OF REFERENCE: R/W PROJECT NUMBER 1440-15-01, R/W PROJECT NUMBER 1440-15-22 - 4.01 LATEST AMENDMENT, MAP OF SURVEY V-1016, V-1903, L-613 AND L-614, DIVISION JOB NO. 8518 AND PLAT OF BRUCE.

EXISTING MONUMENTS					
POINT	Y	X	DESCRIPTION	STA	OFFSET
*10327	560017.97	768311.65	1" IRON PIPE	924+94.62	36.53' LT
11071	559931.36	769059.86	3/4" IRON REBAR	932+43.70	33.18' RT
11058	559928.76	769164.84	3/4" IRON REBAR	933+48.71	32.98' RT

* NO TYPE 2 MONUMENT SET DUE TO CLOSE PROXIMITY OF EXISTING MONUMENT.



BRUCE

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	R/W ACRES REQUIRED			TLE ACRES
			NEW	EXISTING	TOTAL	
15	JOHN DIAZUK	TLE	0	0	0	0.003
16	BRUCE TELEPHONE COMPANY	TLE	0	0	0	0.025
20	CHIPPEWA VALLEY BANK	TLE	0	0	0	0.008
22	ERIC OLSON	TLE	0	0	0	0.021
23	CAROLYN RAKOW	TLE	0	0	0	0.002
24	LINDA VAUGHN	TLE	0	0	0	0.001
25	R & S WILLIAMS PROPERTIES LLC	TLE	0	0	0	0.003
26	DOROTHY HANSON	TLE	0	0	0	0.005
27	J & S SALES OF CHIPPEWA FALLS, LLC	TLE	0	0	0	0.012
28	NASH-JACKAN INC.	TLE	0	0	0	0.019

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT.

CONVENTIONAL ABBREVIATIONS

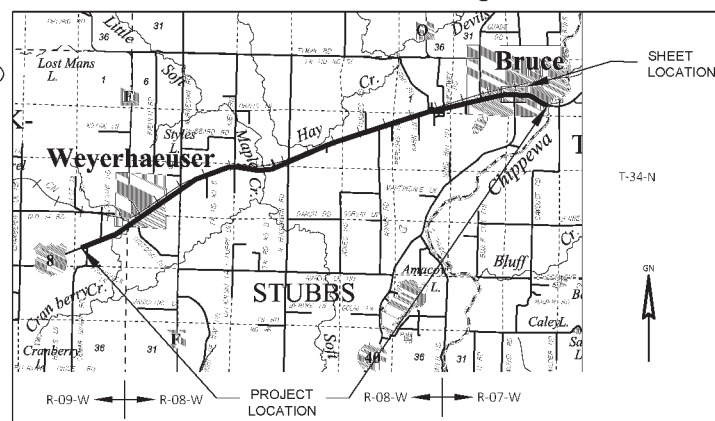
ACCESS POINT/ DRIVEWAY CONNECTION	AP	RELEASE OF RIGHTS	ROR
ACCESS RIGHTS	AR	REMAINING	REM.
ACRES	AC	RIGHT-OF-WAY	R/W
AND OTHERS	ET AL	SECTION	SEC.
CENTERLINE	C/L	STATION	STA.
CERTIFIED SURVEY MAP	CSM	TEMPORARY LIMITED EASEMENT	TLE
CORNER	COR.	VOLUME	V.
DOCUMENT	DOC.		
EASEMENT	EASE.		
HIGHWAY EASEMENT	H.E.	LONG CHORD	LCH
LAND CONTRACT	LC	LONG CHORD BEARING	LCB
MONUMENT	MON.	RADIUS	R
PAGE	P.	DEGREE OF CURVE	D
PERMANENT LIMITED EASEMENT	P.L.E.	CENTRAL ANGLE OR DELTA	DELTA
PROPERTY LINE	PL	LENGTH OF CURVE	L
RECORDED AS	(100')	TANGENT	TAN
REFERENCE LINE	R/L	DIRECTION AHEAD	DA
		DIRECTION BACK	DB

CURVE DATA

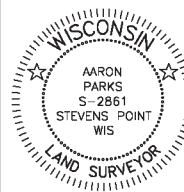
LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE OR DELTA	DELTA
LENGTH OF CURVE	L
TANGENT	TAN
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL UTILITY SYMBOLS

WATER	W
GAS	G
TELEPHONE	T
OVER-HEAD TRANSMISSION LINES	OH
ELECTRIC	E
CABLE TELEVISION	TV
FIBER OPTIC	FO
SANITARY SEWER	SAN
STORM SEWER	SS
ELECTRIC TOWER	ET



I, AARON PARKS, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.



SIGNATURE: *Aaron Parks* DATE: 07/15/21
PRINT NAME: AARON PARKS
REGISTRATION NUMBER: S-2861
THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION
SIGNATURE: *Heather L. Dreisel* DATE: 09/20/21
PRINT NAME: HEATHER L. DRESEL

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

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FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN SPOONER.

PARCEL AND UTILITY IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE TPP DETAIL PAGES.

EXISTING USH 8 RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:
DIVISION JOB NUMBER 8518 AND R/W PROJECT NUMBER 1582-03-21.
EXISTING VILLAGE STREET RIGHT-OF-WAYS SHOWN HEREIN ARE BASED ON PLAT OF BRUCE.

TRANSPORTATION PROJECT PLAT NO: 1570-06-22 - 4.21

PART OF LOT 14, BLOCK 11 OF PLAT OF BRUCE LOCATED IN THE NE1/4 -SW1/4 OF SECTION 5, T34N, R07W, VILLAGE OF BRUCE, RUSK COUNTY, WISCONSIN

RELOCATION ORDER: USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

TO PROPERLY ESTABLISH LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:
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POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS) RUSK COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

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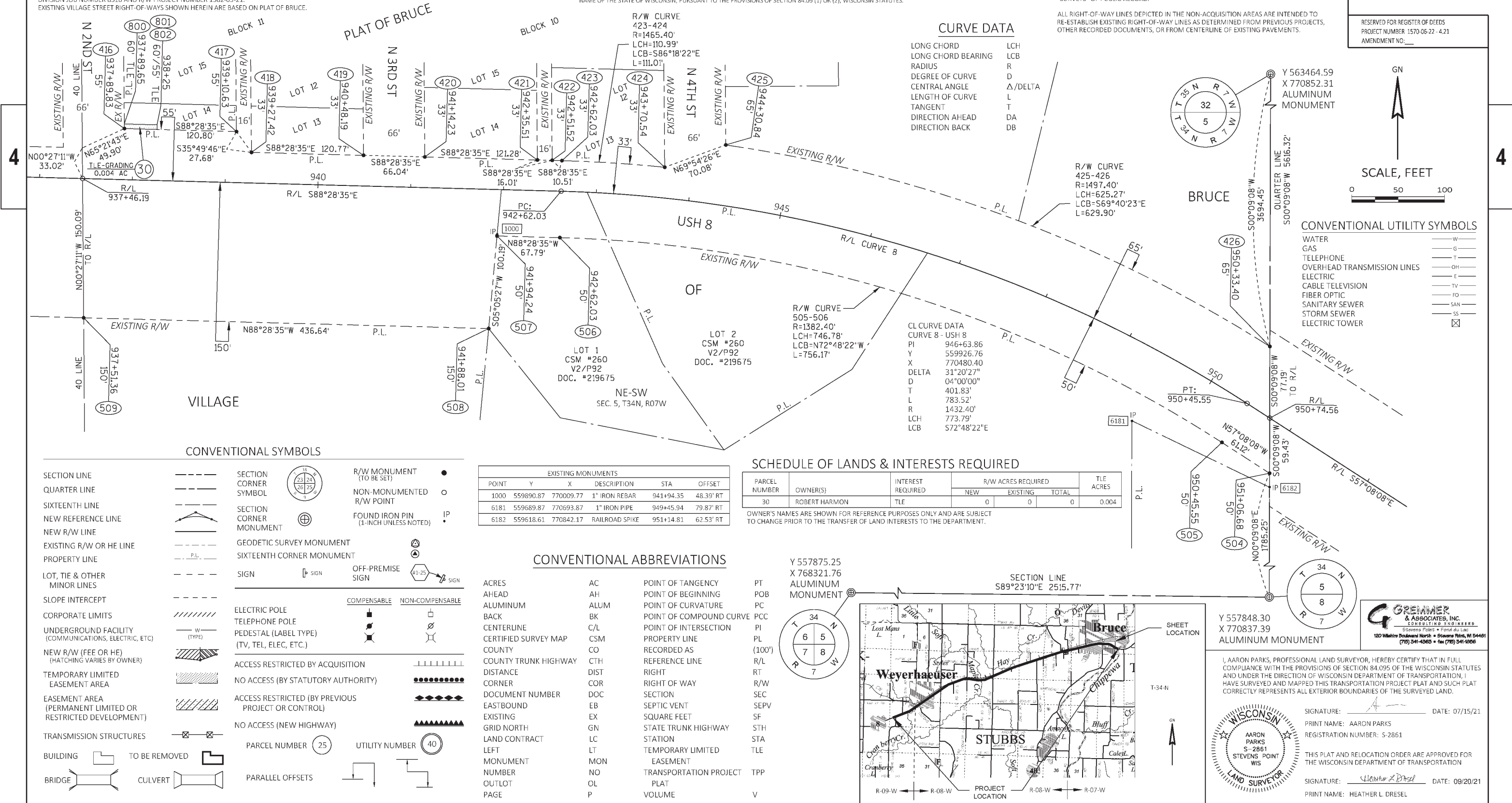
DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

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ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

361348
MARY BERG
REGISTER OF DEEDS
RUSK COUNTY, WI
RECORDED ON
09/24/2021 08:47 AM
CABINET:1 ENW:6

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 1570-06-22 - 4.21
AMENDMENT NO: _____



TRANSPORTATION PROJECT PLAT NO: 1570-06-22 - 4.22

PART OF THE NW1/4-SE1/4 AND GOVERNMENT LOT 3 OF SECTION 5, ALL IN T34N, R07W,
VILLAGE OF BRUCE, RUSK COUNTY, WISCONSIN

USH 8, CAMERON-LADYSMITH, CEDAR SWAMP ROAD TO CHIPPEWA RIVER, RUSK COUNTY

THE STATE OF WISCONSIN HAS DEEMED IT NECESSARY TO MONUMENT RIGHT-OF-WAY INTERESTS PREVIOUSLY
ACQUIRED UNDER SECTIONS 82.01, 82.31(1), 82.31(2) AND 84.09 OF THE WISCONSIN STATE STATUTES AND DOES
HEREBY CERTIFY THE DEPARTMENT OF TRANSPORTATION HAS SURVEYED THE HIGHWAY RIGHT-OF-WAY; THAT THIS PLAT
IS A TRUE AND CORRECT REPRESENTATION OF THE EXTERIOR BOUNDARIES OF THE LAND SURVEYED AND SHOWS
ACCURATE MEASUREMENTS THEREOF.

EXISTING MONUMENTS					
POINT	Y	X	DESCRIPTION	STA	OFFSET
6182	559618.61	770842.17	RAILROAD SPIKE	951+14.81	62.53' RT
6183	559154.60	772111.56	1" IRON PIPE	964+78.89	139.15' LT
*6184	559394.63	771539.51	1" IRON PIPE	958+27.23	125.49' LT
*6185	559403.83	771539.31	1" IRON PIPE	958+22.15	133.31' LT
6186	559713.51	770957.80	1" IRON PIPE	951+60.44	79.94' LT

*NO TYPE 2 MONUMENT SET DUE TO CLOSE PROXIMITY OF EXISTING MONUMENT.

NW-SE
SEC. 5, T34N, R07W

BRUCE

ALTA/ACSM
L-770
FILED 8-26-10

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	●
QUARTER LINE	---	SIXTEENTH LINE	---	NON-MONUMENTED R/W POINT	○
NEW REFERENCE LINE	---	NEW R/W LINE	---	FOUND IRON PIN (1-INCH UNLESS NOTED)	IP
EXISTING R/W OR HE LINE	---	PROPERTY LINE	---	GEODETIC SURVEY MONUMENT	
LOT, TIE & OTHER MINOR LINES	---	SIXTEENTH CORNER MONUMENT	---	OFF-PREMISE SIGN	
SLOPE INTERCEPT	---	SIGN		UTILITY NUMBER	
CORPORATE LIMITS	---	ELECTRIC POLE		PARCEL NUMBER	
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	---	TELEPHONE POLE		UTILITY NUMBER	
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---	PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)		PARALLEL OFFSETS	
TEMPORARY LIMITED EASEMENT AREA	---	ACCESS RESTRICTED BY ACQUISITION			
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---	NO ACCESS (BY STATUTORY AUTHORITY)			
TRANSMISSION STRUCTURES	---	ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)			
BUILDING		NO ACCESS (NEW HIGHWAY)			
BRIDGE		TO BE REMOVED			
CULVERT					

GOVERNMENT
LOT 2
SEC. 5, T34N, R07W

CONVENTIONAL ABBREVIATIONS

ACRES	AC	POINT OF TANGENCY	PT
AHEAD	AH	POINT OF BEGINNING	POB
ALUMINUM	ALUM	POINT OF CURVATURE	PC
BACK	BK	POINT OF COMPOUND CURVE	PCC
CENTERLINE	C/L	POINT OF INTERSECTION	PI
CERTIFIED SURVEY MAP	CSM	PROPERTY LINE	PL
COUNTY	CO	RECORDED AS	(100')
COUNTY TRUNK HIGHWAY	CTH	REFERENCE LINE	R/L
DISTANCE	DIST	RIGHT	RT
CORNER	COR	RIGHT OF WAY	R/W
DOCUMENT NUMBER	DOC	SECTION	SEC
EASTBOUND	EB	SEPTIC VENT	SEPV
EXISTING	EX	SQUARE FEET	SF
GRID NORTH	GN	STATE TRUNK HIGHWAY	STH
LAND CONTRACT	LC	STATION	STA
LEFT	LT	TEMPORARY LIMITED EASEMENT	TLE
MONUMENT	MON	TRANSPORTATION PROJECT	TPP
NUMBER	NO	PLAT	V
OUTLOT	OL	VOLUME	
PAGE	P		

363879
MARY BERG
REGISTER OF DEEDS
RUSK COUNTY, WI
RECORDED ON
02/18/2022 11:14 AM
CABINET:1 ENV:18 B

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 1570-06-22 - 4.22
AMENDMENT NO.:

Y 560579.62
X 773393.36
ALUMINUM MONUMENT

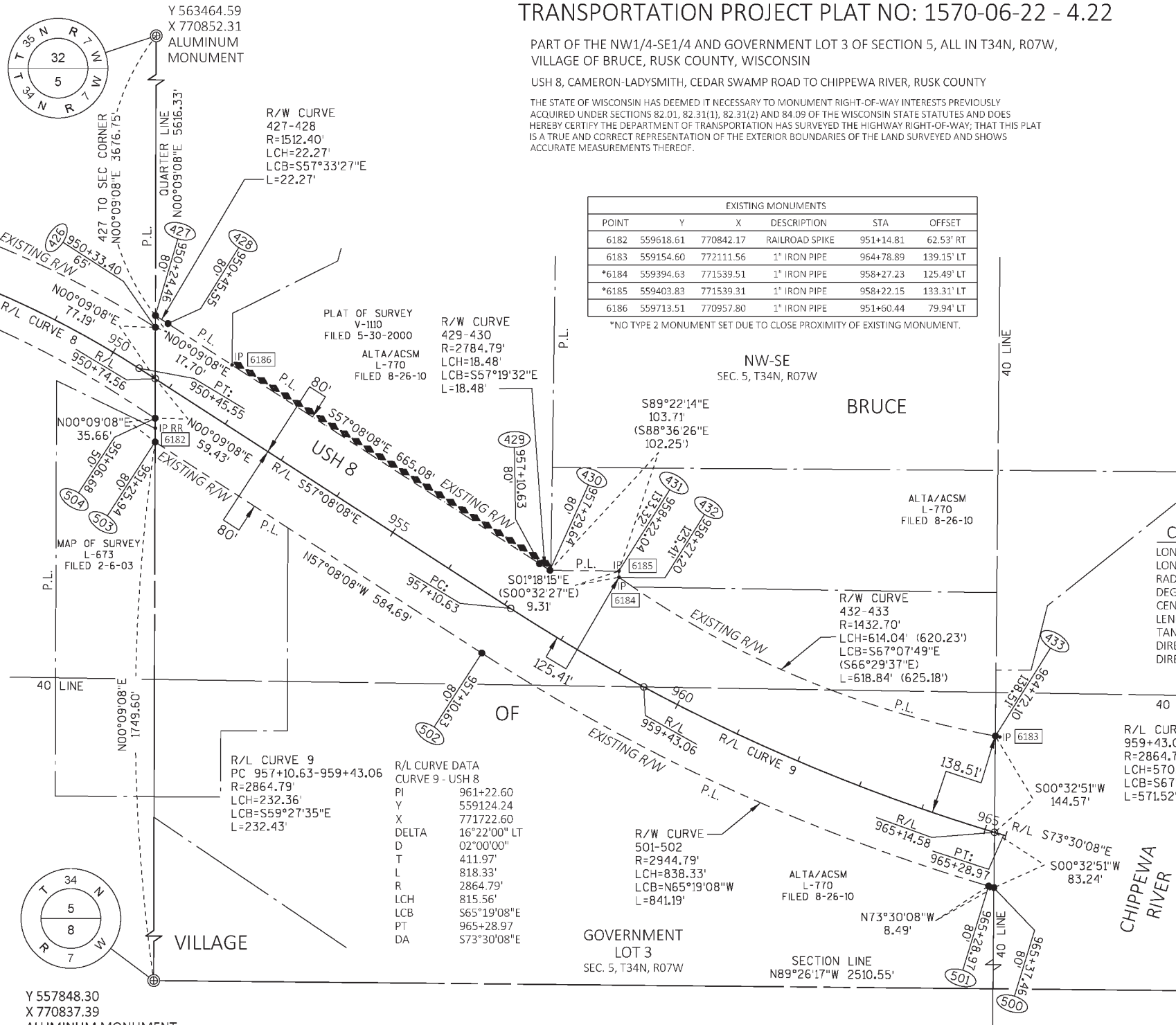
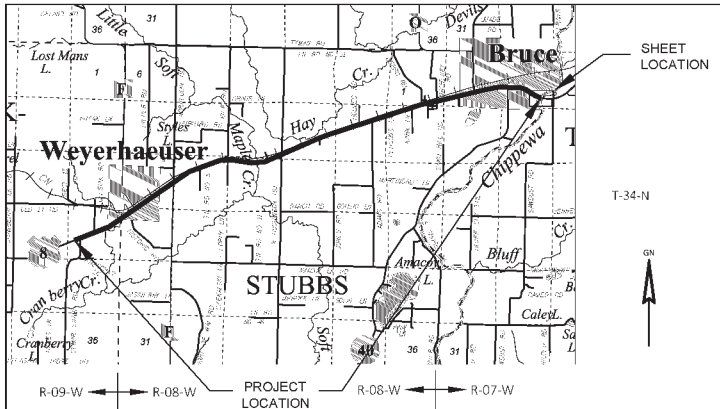
SCALE, FEET
0 100 200

Y 557823.67
X 773347.81
ALUMINUM MONUMENT

GREMMER & ASSOCIATES, INC.
CONSULTING ENGINEERS
Stevens Point • Fond du Lac
120 Main Street North • Stevens Point, WI 54481
(715) 341-4365 • Fax (715) 341-4366

WISCONSIN
AARON PARKS
S-2861
STEVENS POINT
WIS
LAND SURVEYOR

SIGNATURE: _____ DATE: 05/19/21
PRINT NAME: AARON PARKS
REGISTRATION NUMBER: S-2861
THIS PLAT IS APPROVED FOR THE WISCONSIN
DEPARTMENT OF TRANSPORTATION
SIGNATURE: _____ DATE: 2/16/22
PRINT NAME: HEATHER L. DRESEL



POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS) RUSK COUNTY,
NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE
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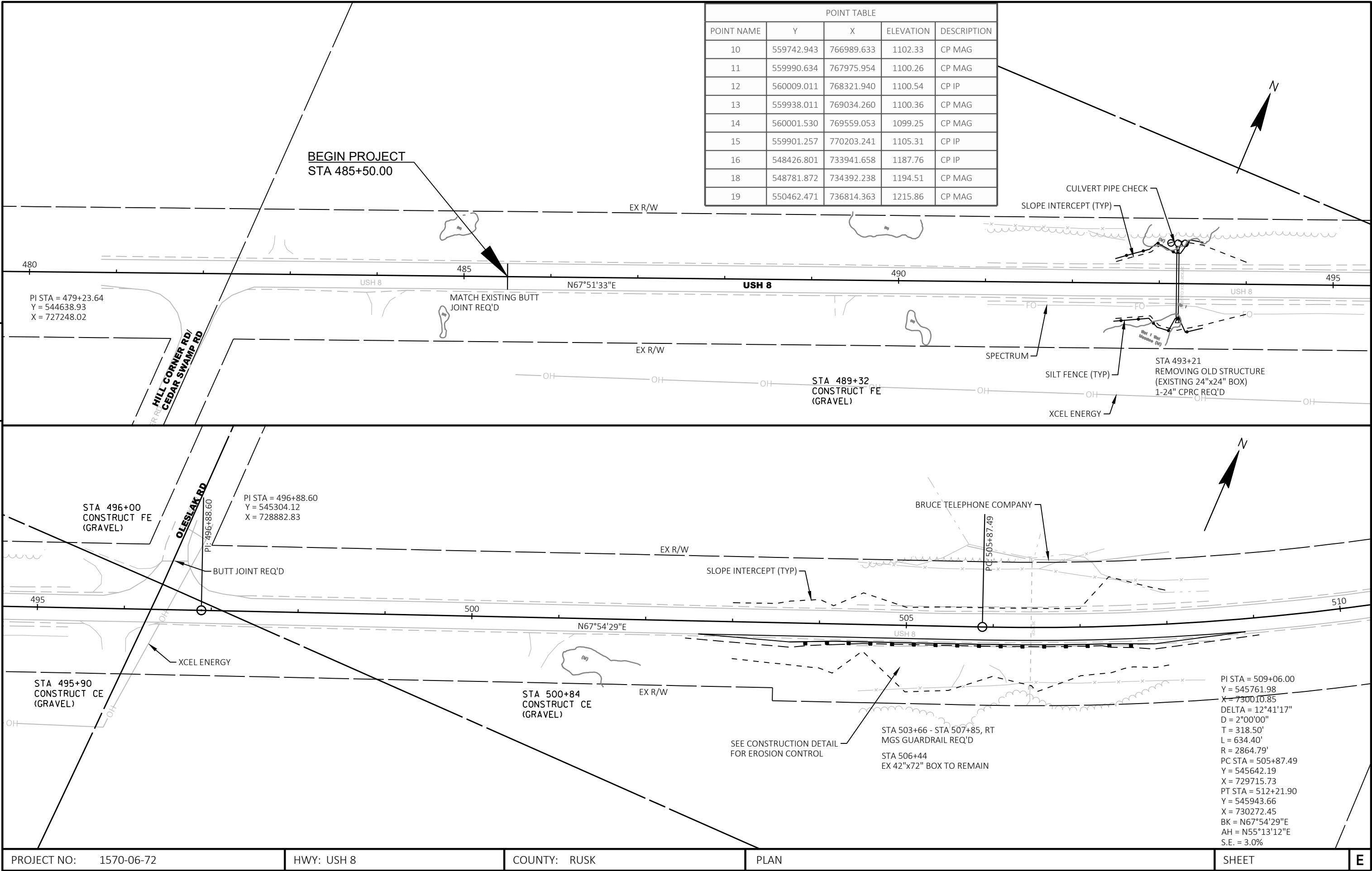
UTILITY INFORMATION IS NOT SHOWN ON THIS PLAT.

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EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

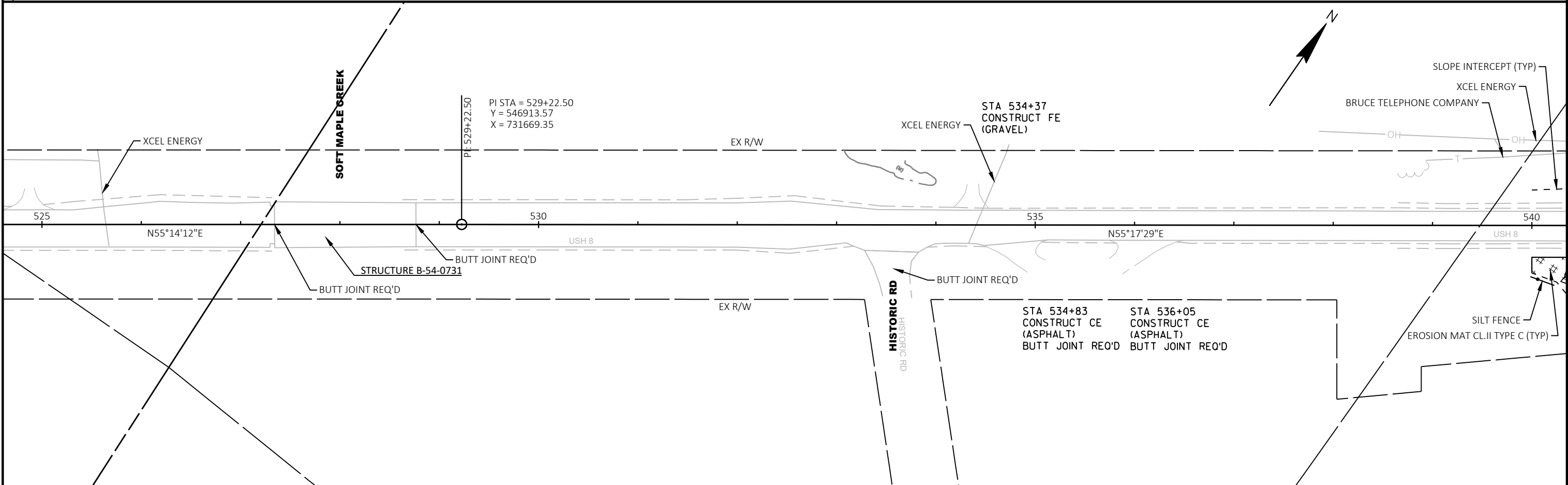
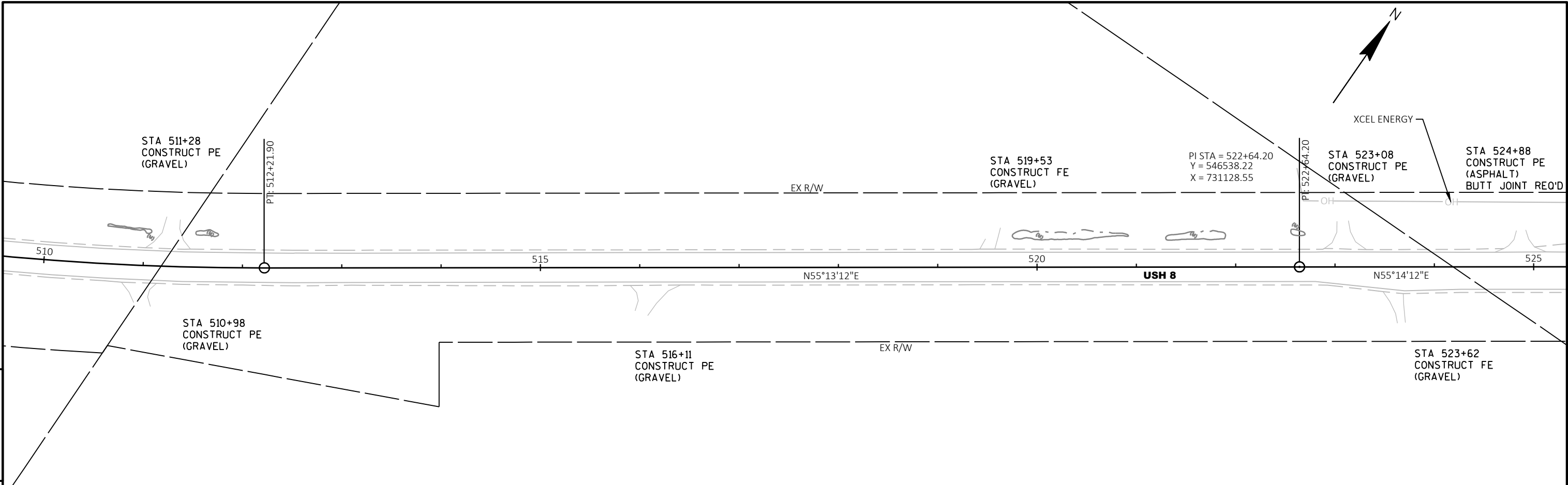
FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF
TRANSPORTATION OFFICE IN SPOONER.

EXISTING USH 8 RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:
DIVISION JOB NUMBER 8518, R/W PROJECT NUMBER 1582-03-21, PLAT OF SURVEY V-1110, MAP OF SURVEY L-673 AND
ALTA/ACSM LAND TITLE SURVEY (L-770)

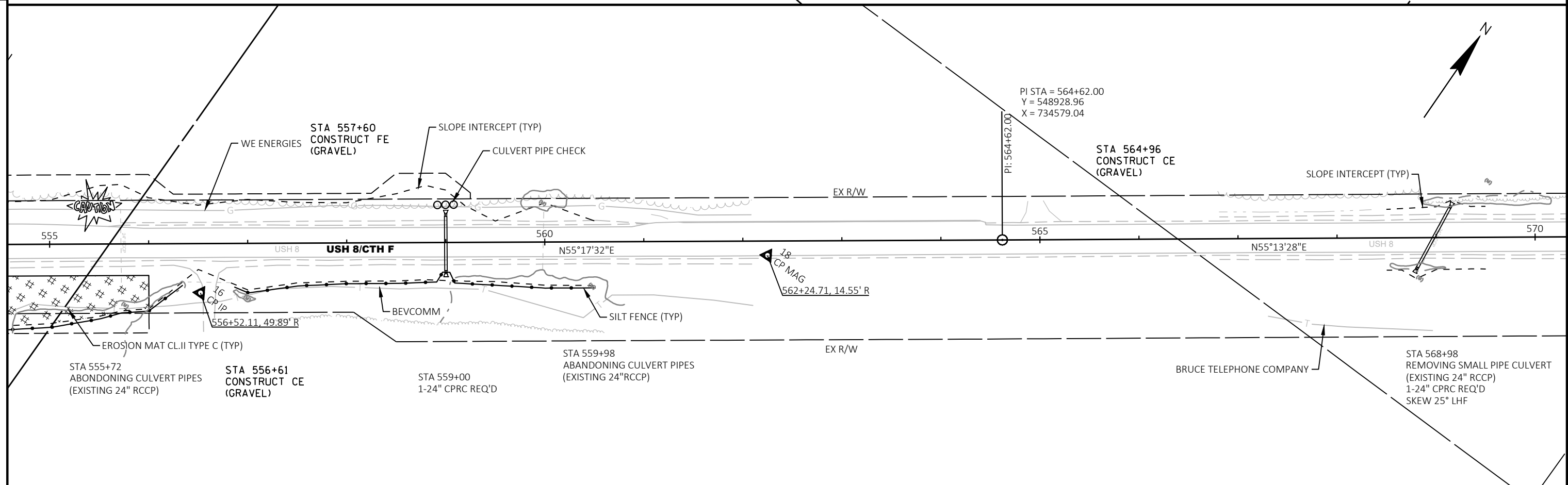
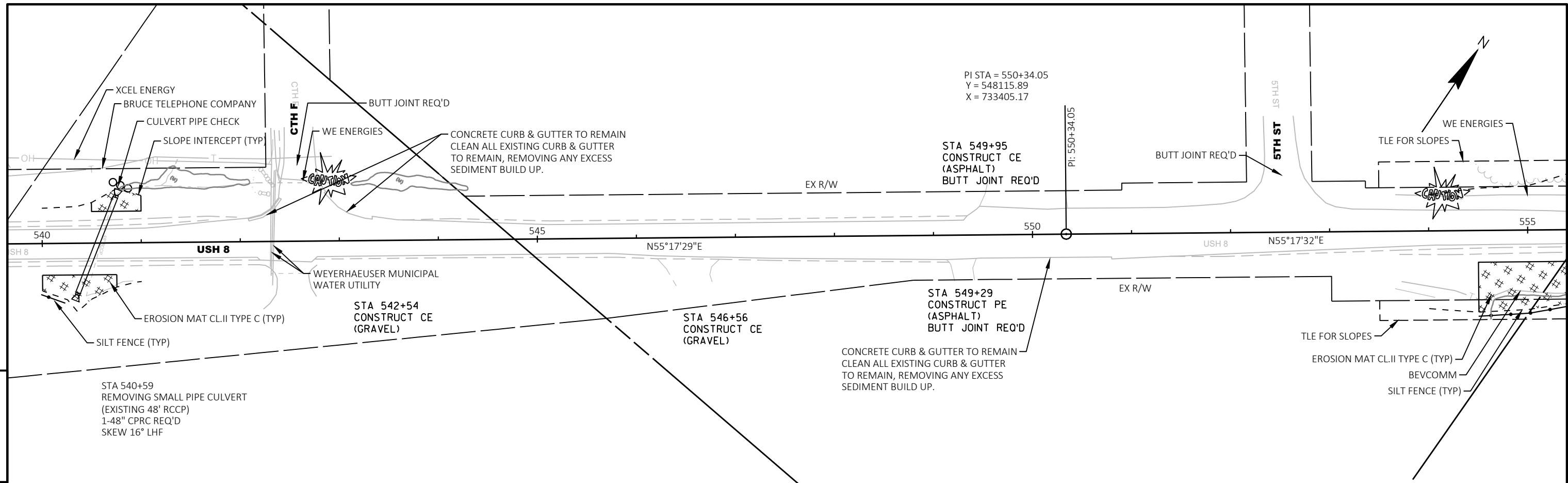
EXISTING ACCESS CONTROL ALONG USH 8 ESTABLISHED FROM PLAT OF SURVEY, V-1110.



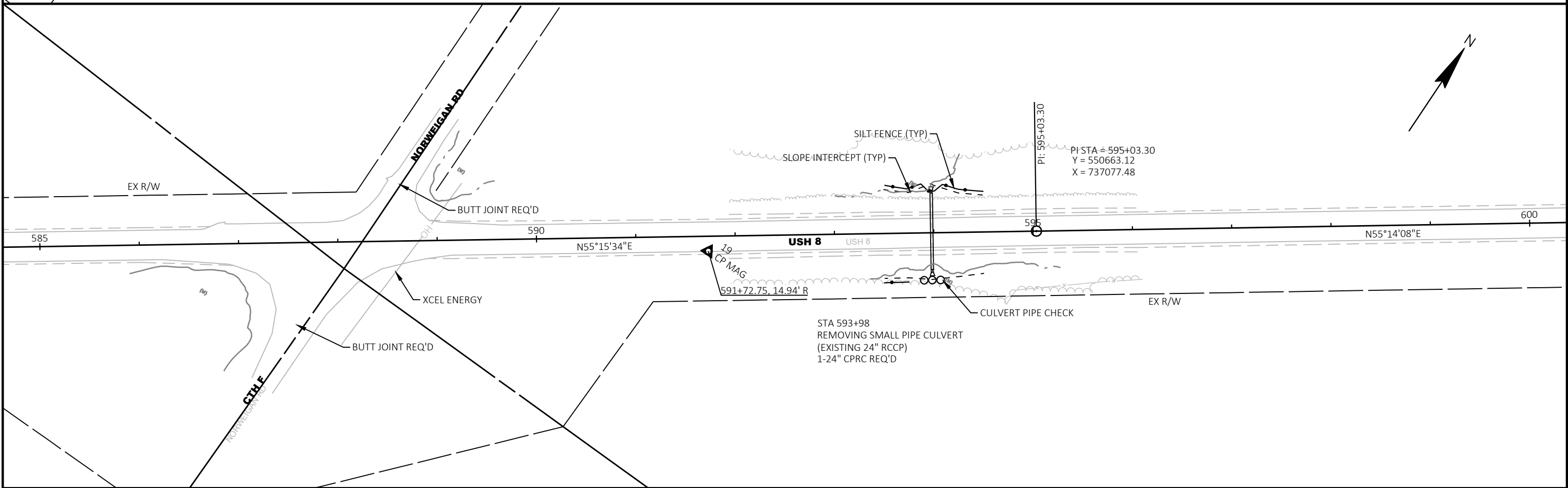
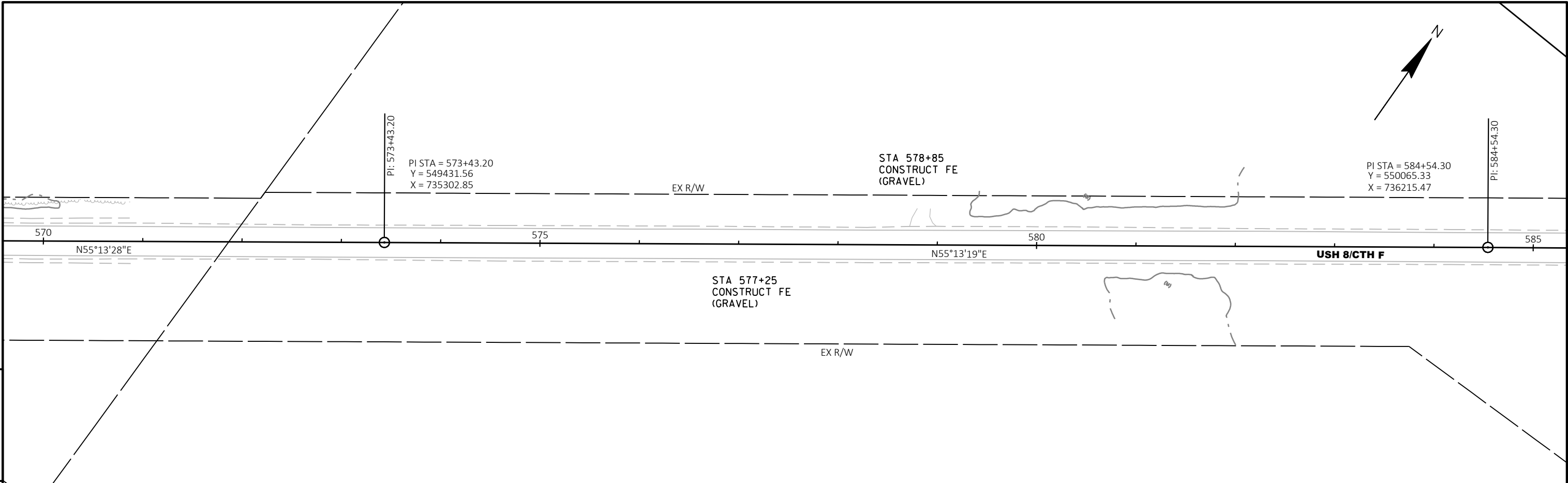
POINT TABLE				
POINT NAME	Y	X	ELEVATION	DESCRIPTION
10	559742.943	766989.633	1102.33	CP MAG
11	559990.634	767975.954	1100.26	CP MAG
12	560009.011	768321.940	1100.54	CP IP
13	559938.011	769034.260	1100.36	CP MAG
14	560001.530	769559.053	1099.25	CP MAG
15	559901.257	770203.241	1105.31	CP IP
16	548426.801	733941.658	1187.76	CP IP
18	548781.872	734392.238	1194.51	CP MAG
19	550462.471	736814.363	1215.86	CP MAG



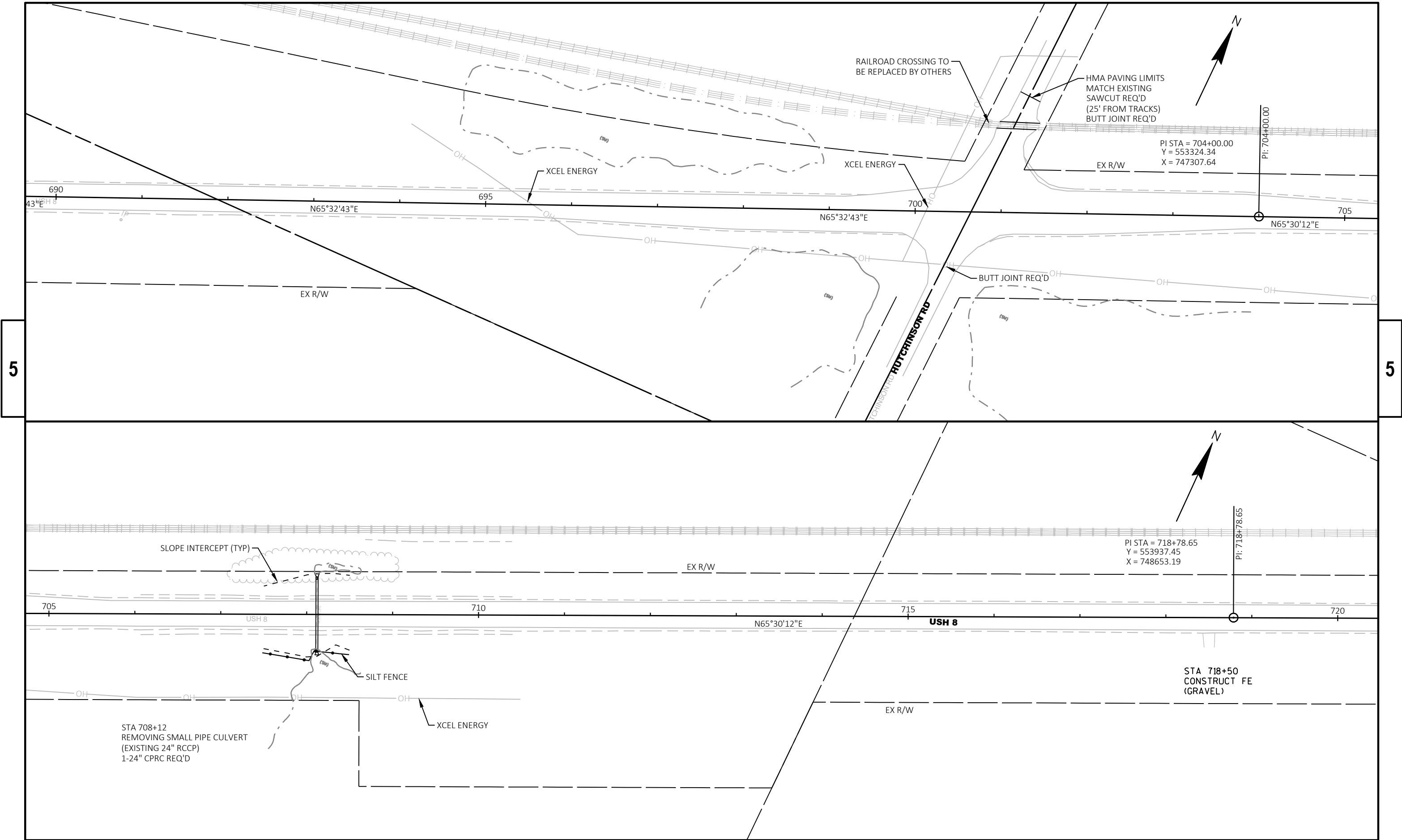
PROJECT NO: 1570-06-72	HWY: USH 8	COUNTY: RUSK	PLAN	SHEET	E
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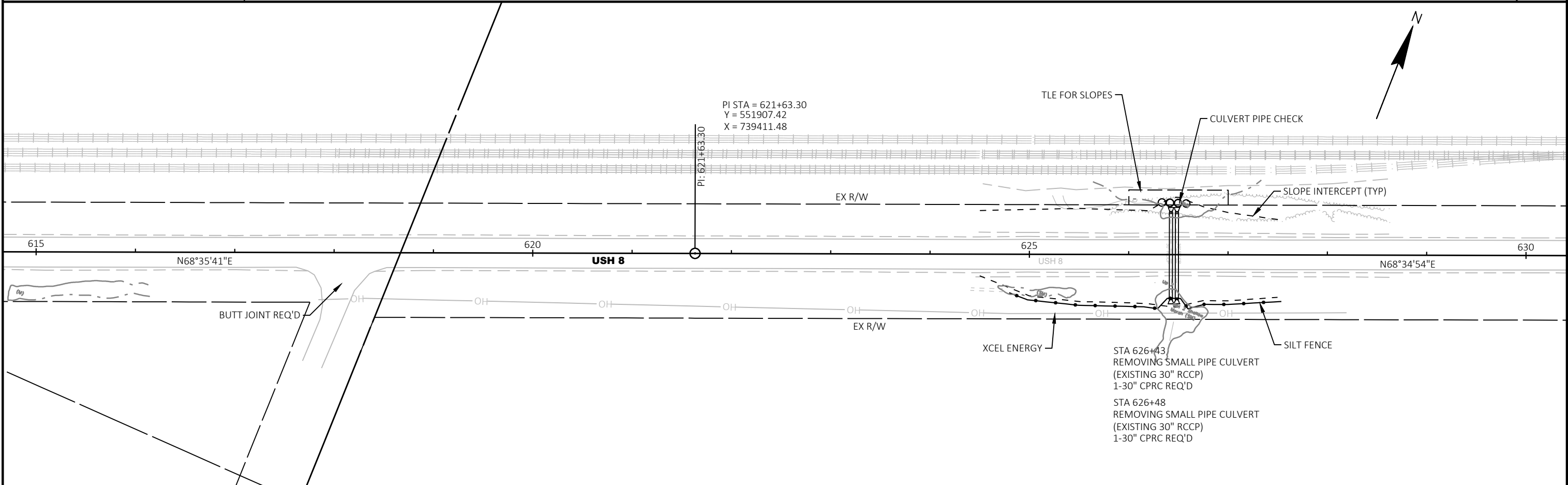
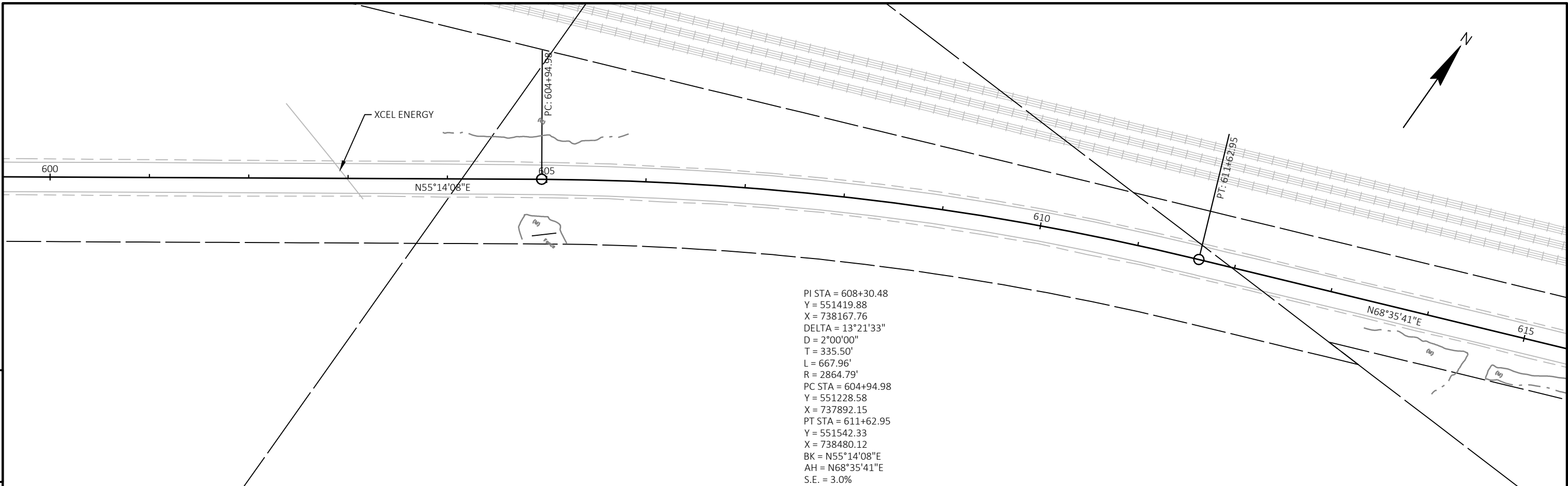


PROJECT NO: 1570-06-72	HWY: USH 8	COUNTY: RUSK	PLAN	SHEET	E
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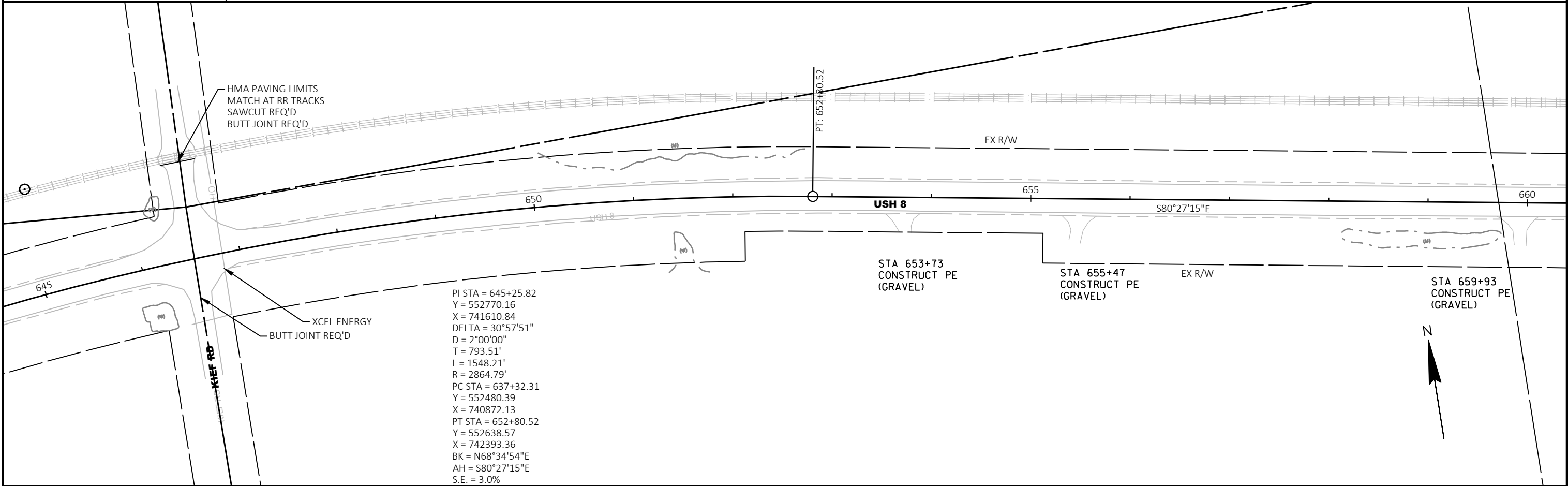
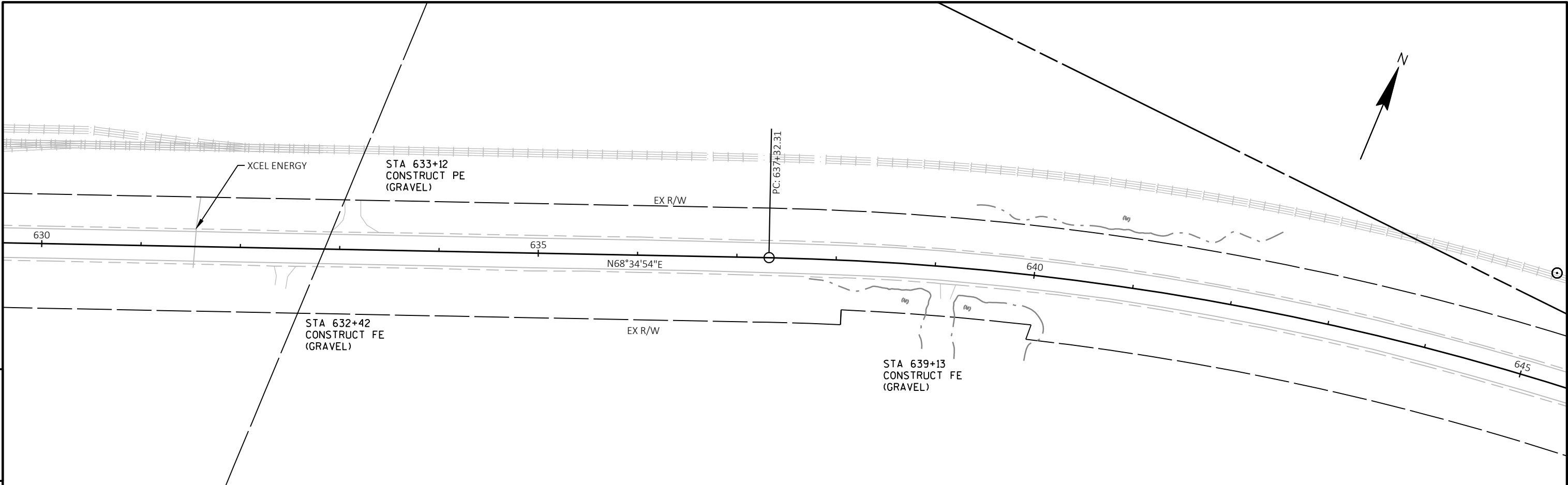


PROJECT NO: 1570-06-72	HWY: USH 8	COUNTY: RUSK	PLAN	SHEET	E
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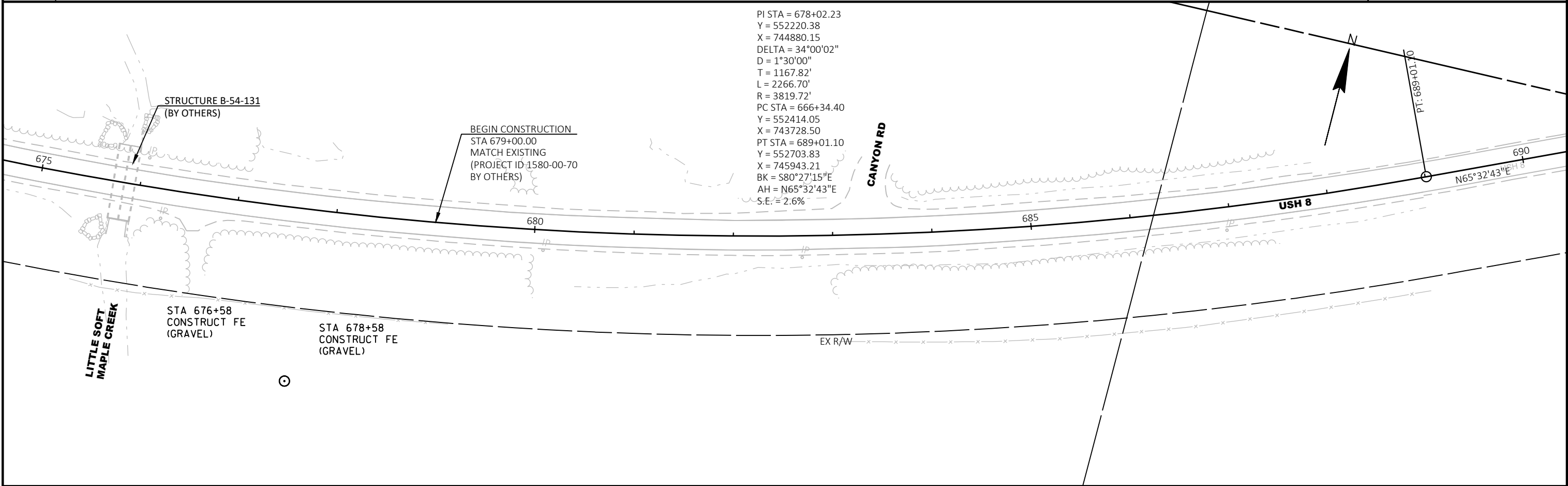
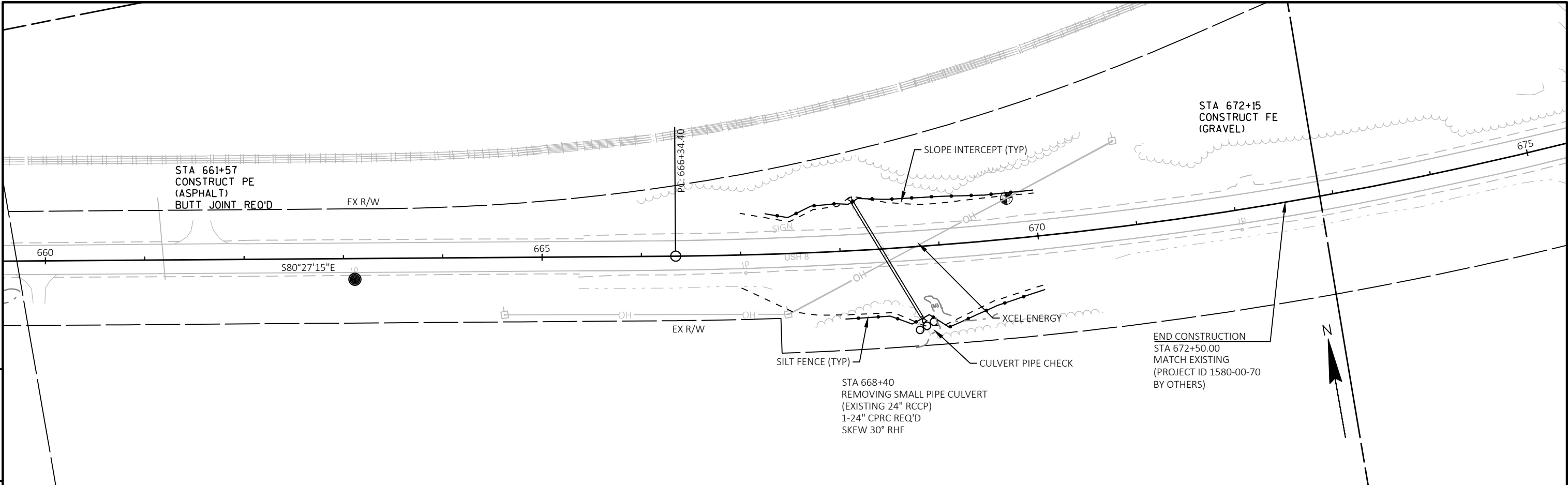


PROJECT NO: 1570-06-72	HWY: USH 8	COUNTY: RUSK	PLAN	SHEET	E
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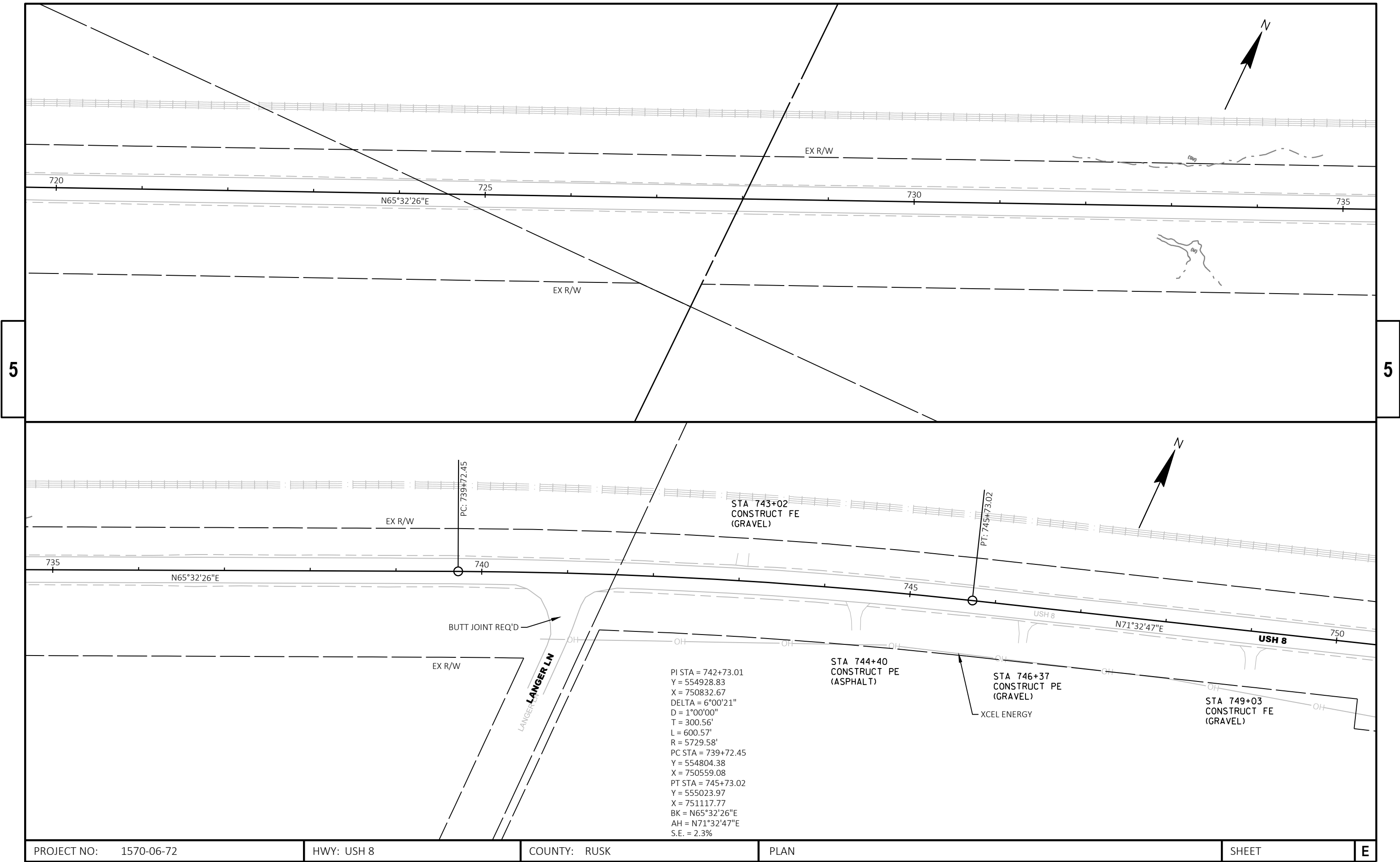


PI STA = 645+25.82
Y = 552770.16
X = 741610.84
DELTA = 30°57'51"
D = 2°00'00"
T = 793.51'
L = 1548.21'
R = 2864.79'
PC STA = 637+32.31
Y = 552480.39
X = 740872.13
PT STA = 652+80.52
Y = 552638.57
X = 742393.36
BK = N68°34'54"E
AH = S80°27'15"E
S.E. = 3.0%

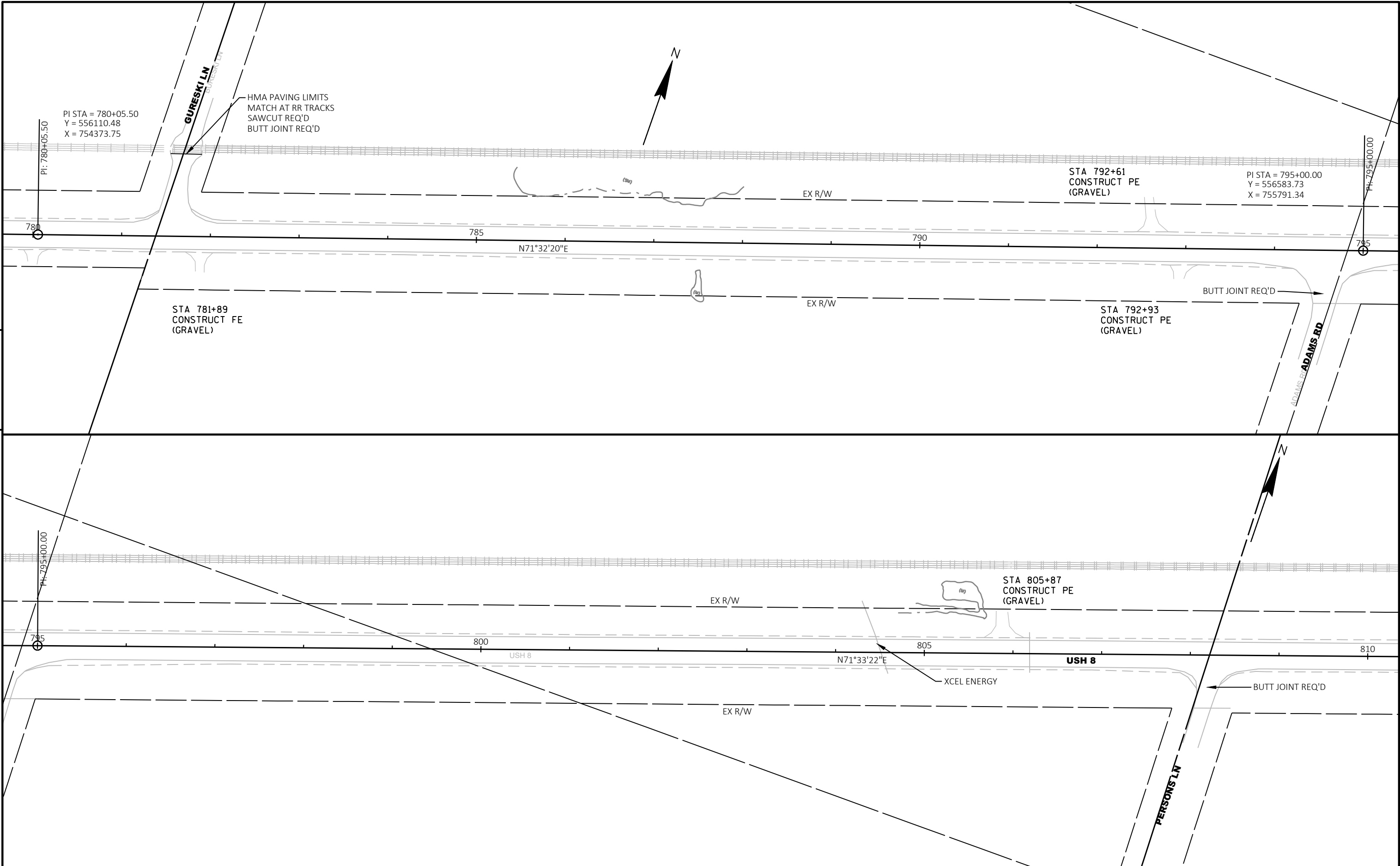
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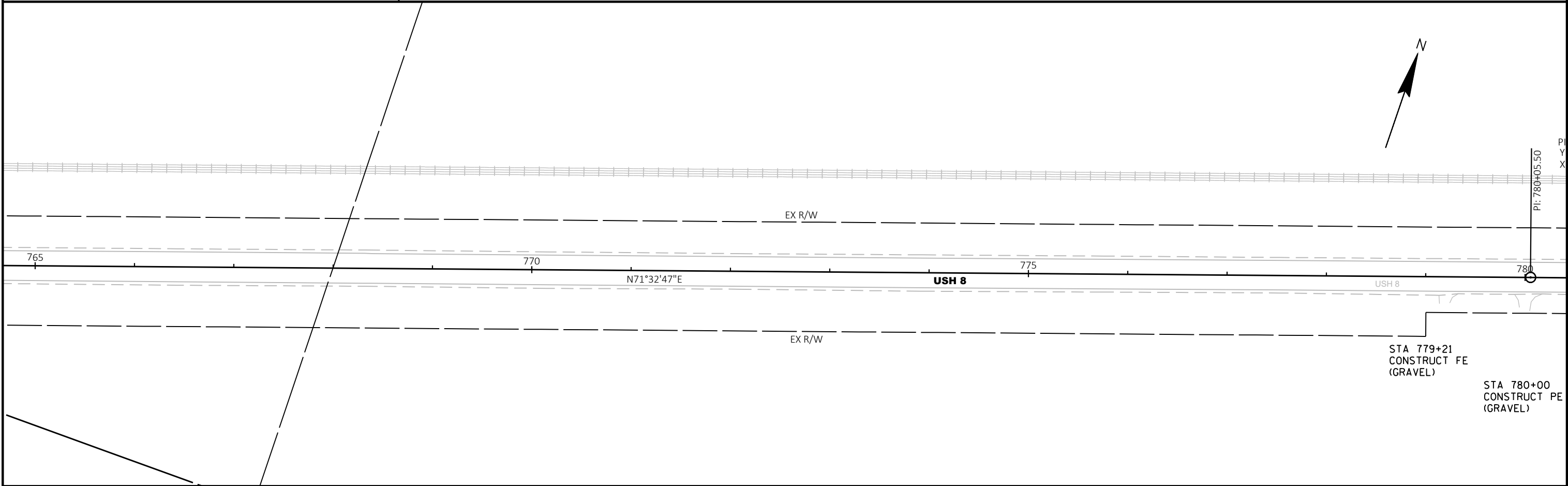
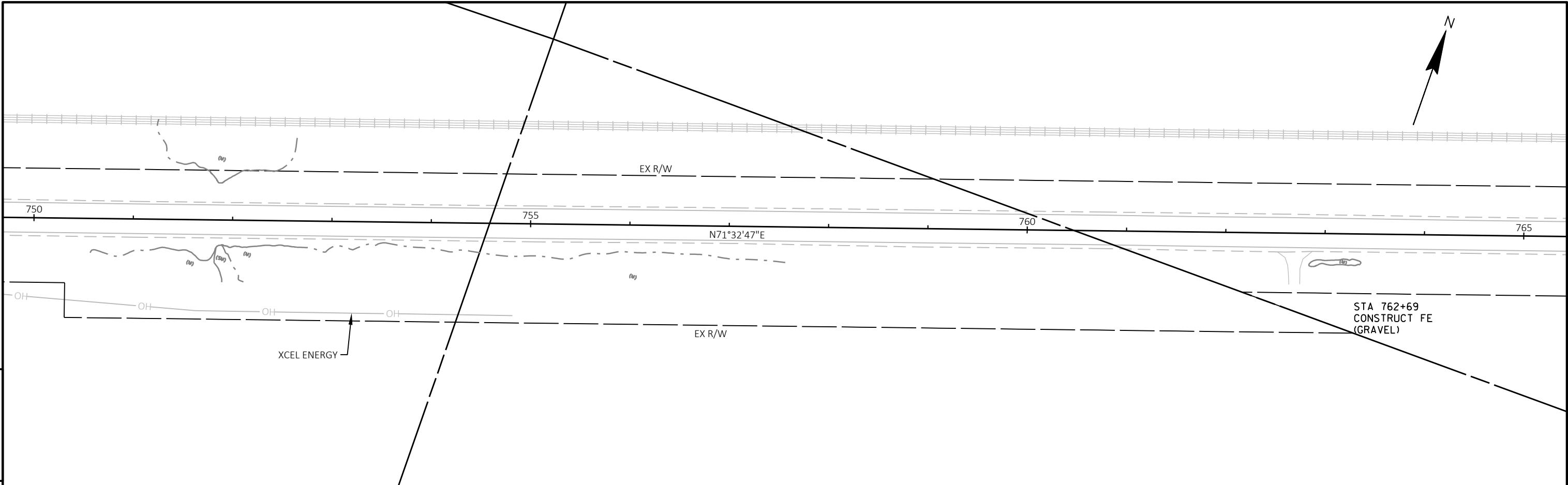
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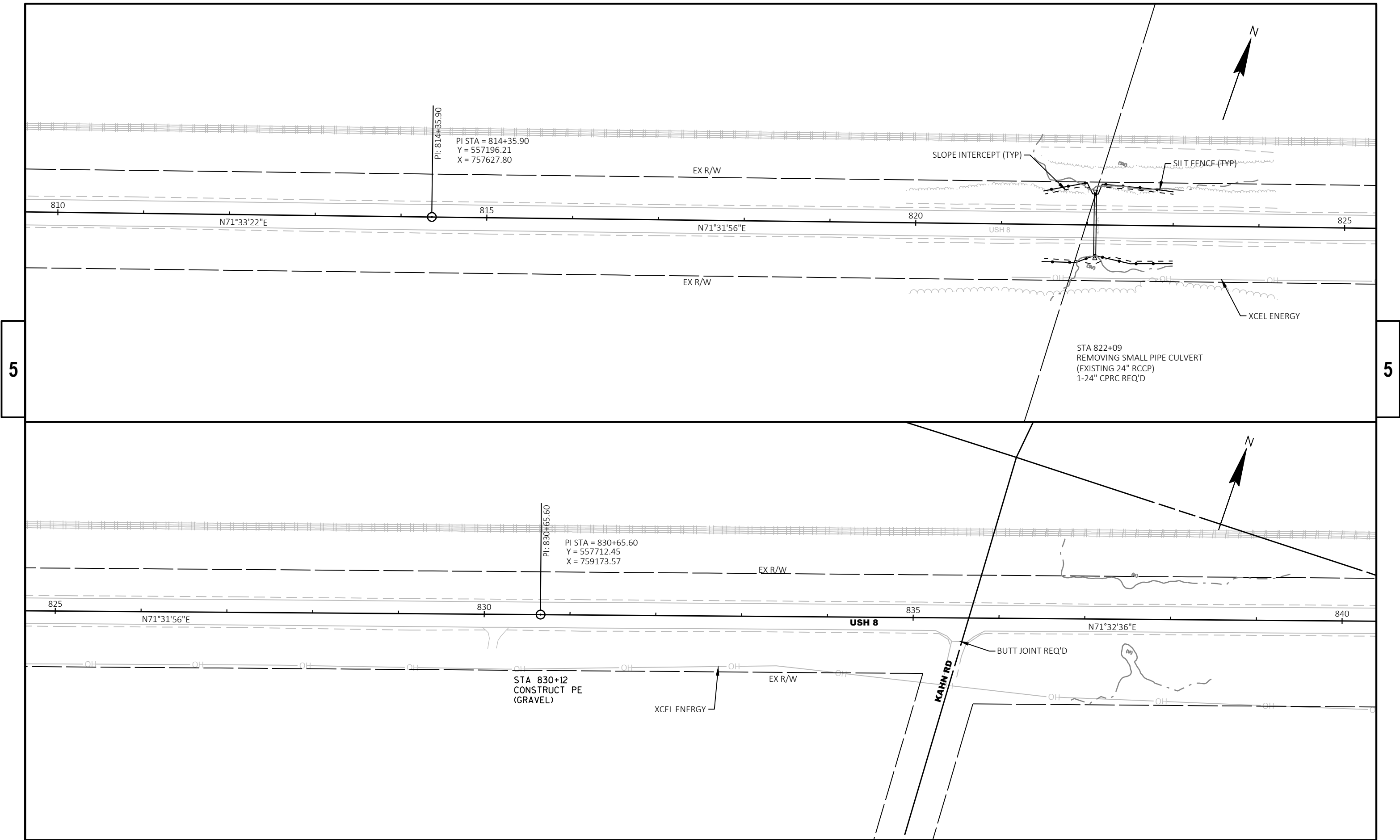
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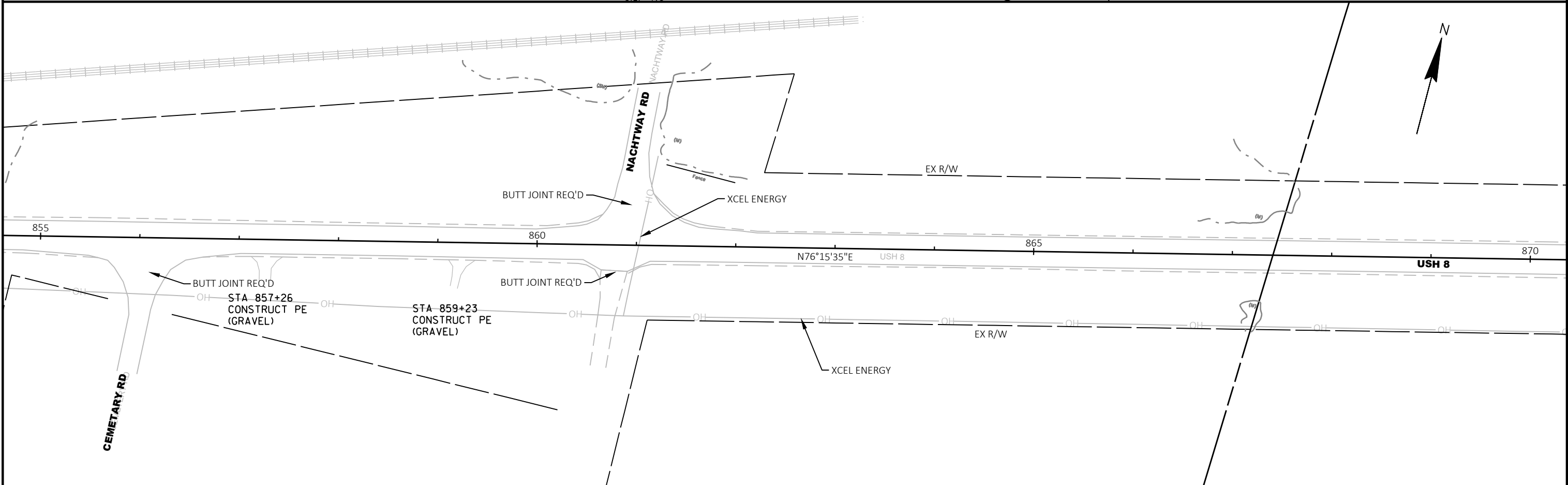
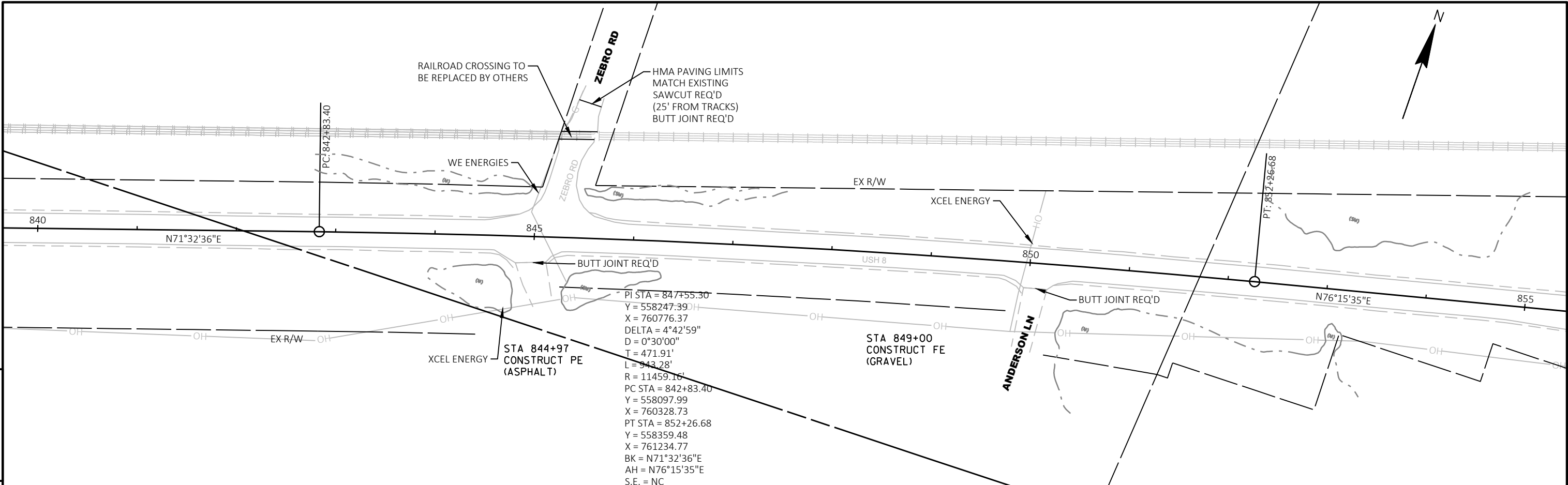
PROJECT NO: 1570-06-72	HWY: USH 8	COUNTY: RUSK	PLAN	SHEET	E
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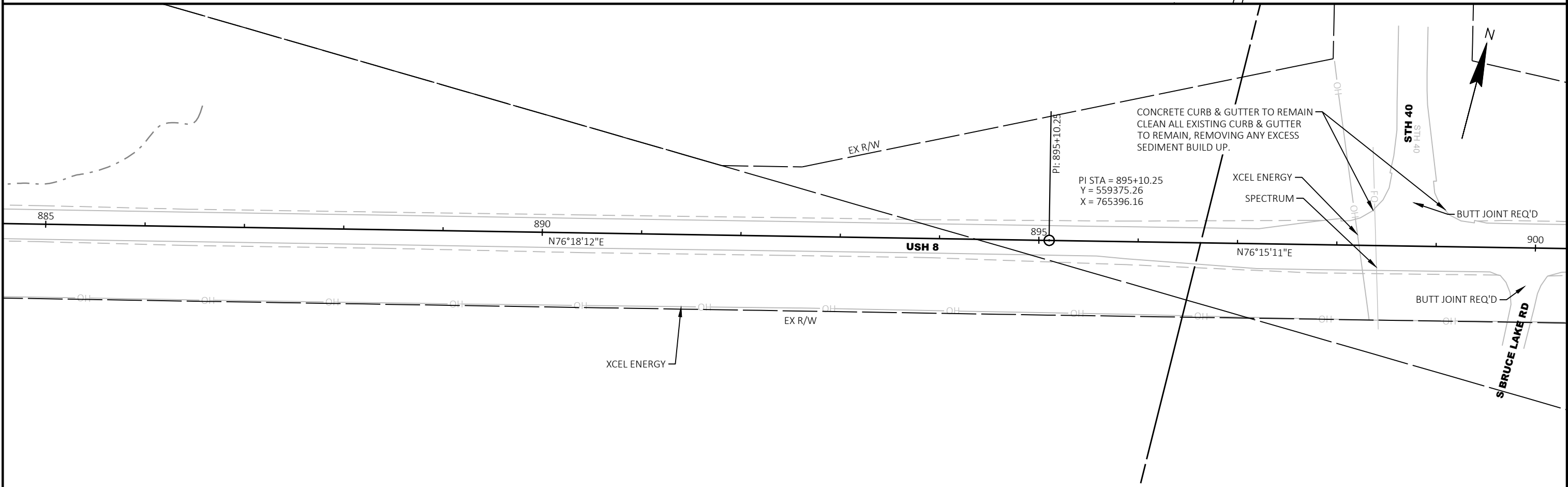
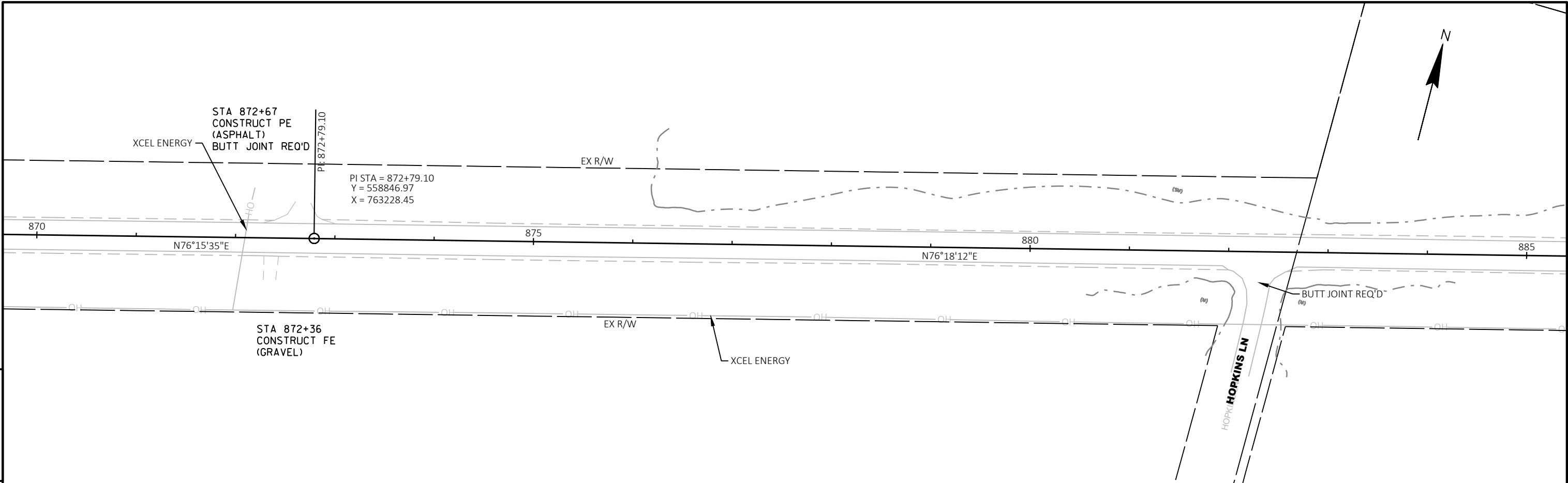
PROJECT NO: 1570-06-72	HWY: USH 8	COUNTY: RUSK	PLAN	SHEET	E
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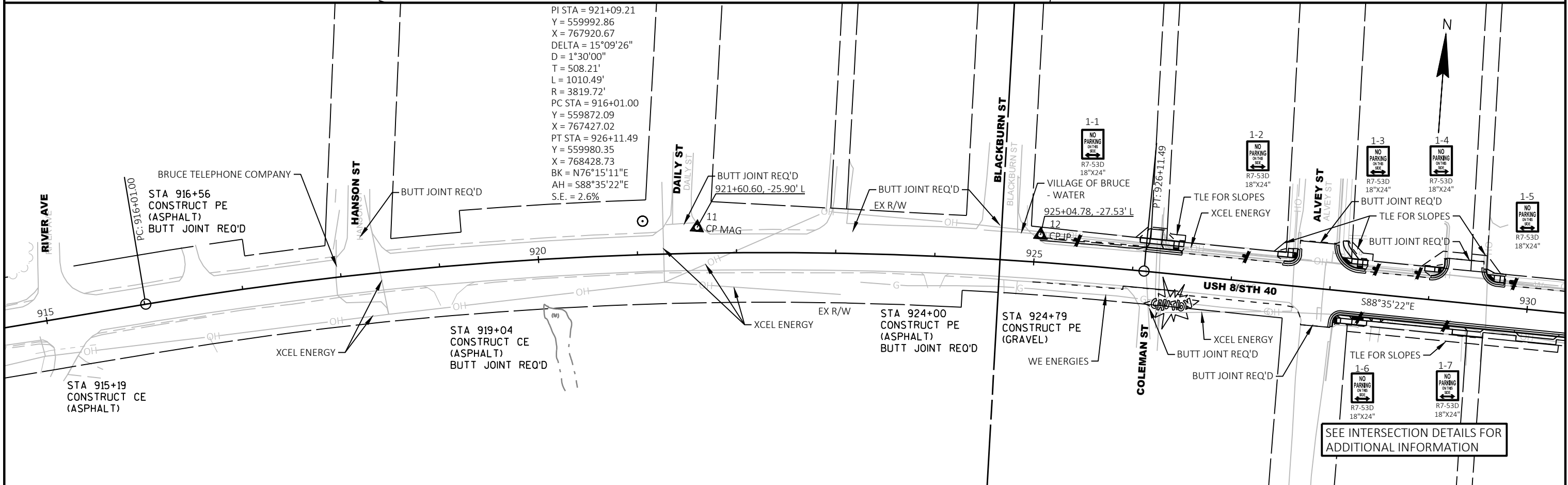
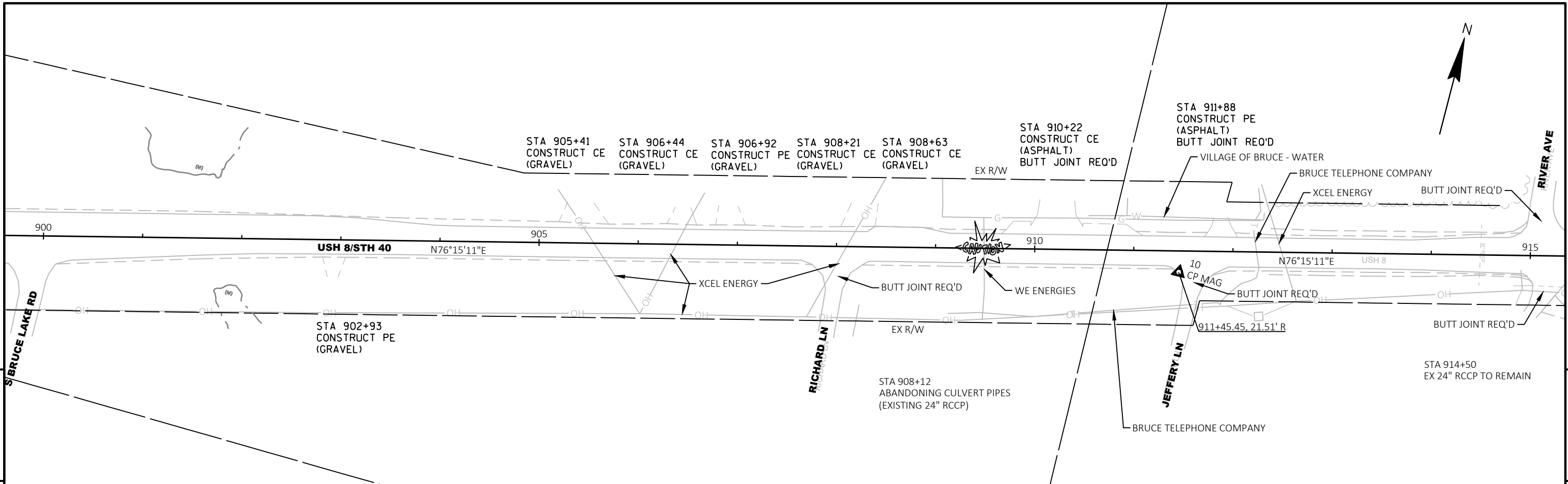
PROJECT NO: 1570-06-72	HWY: USH 8	COUNTY: RUSK	PLAN	SHEET	E
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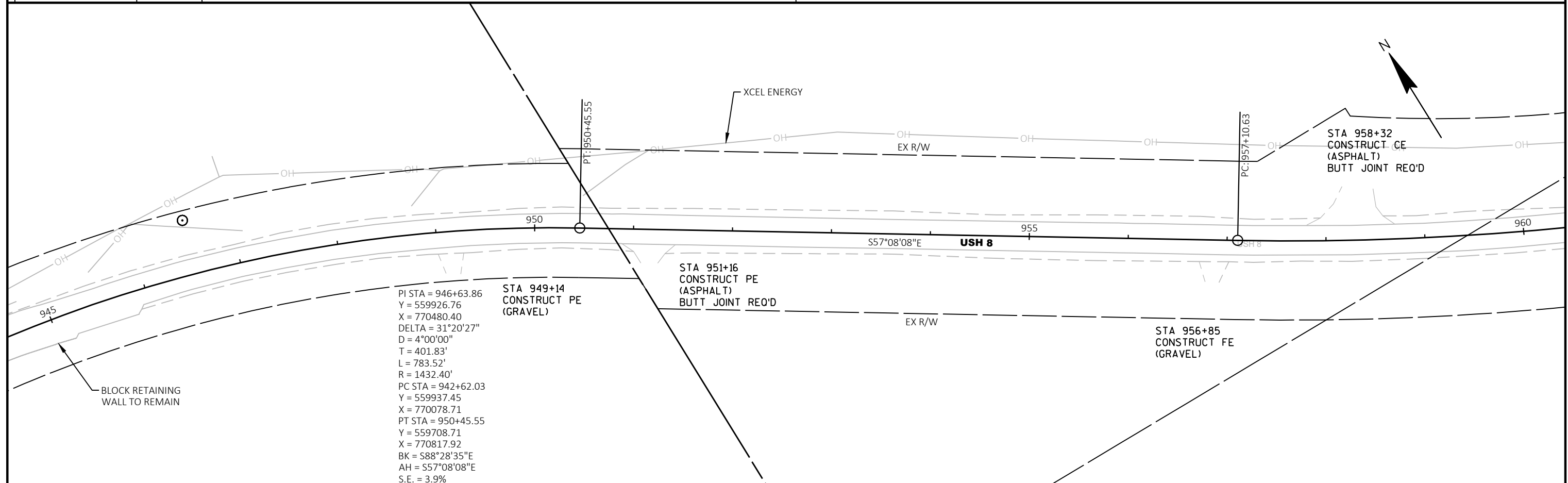
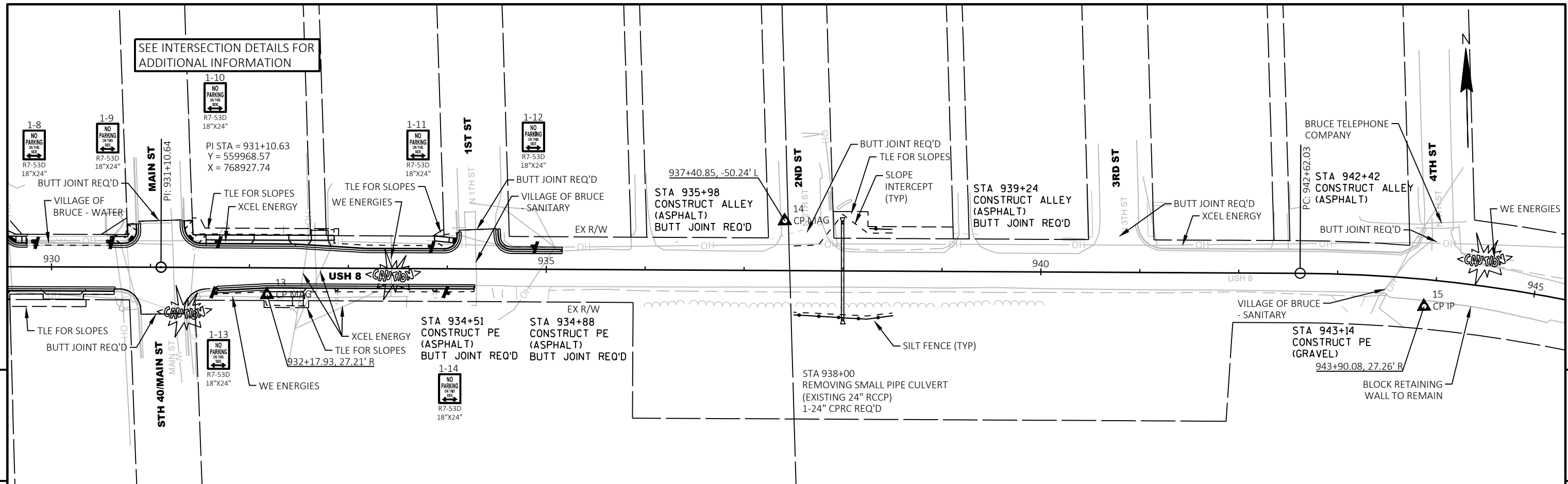


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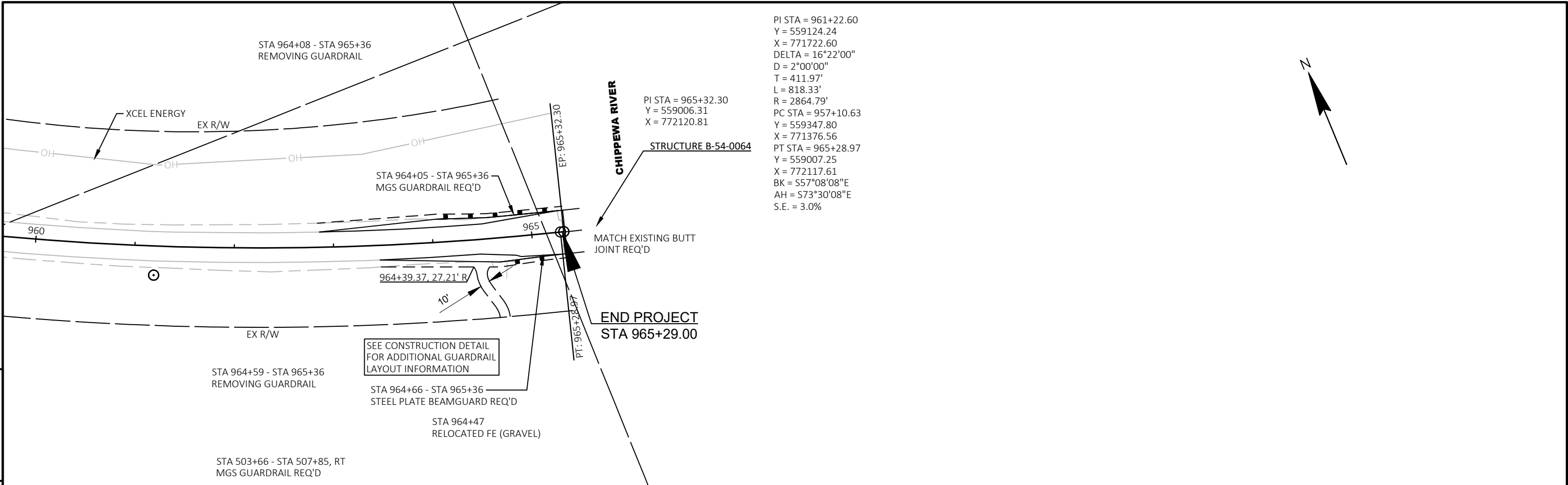


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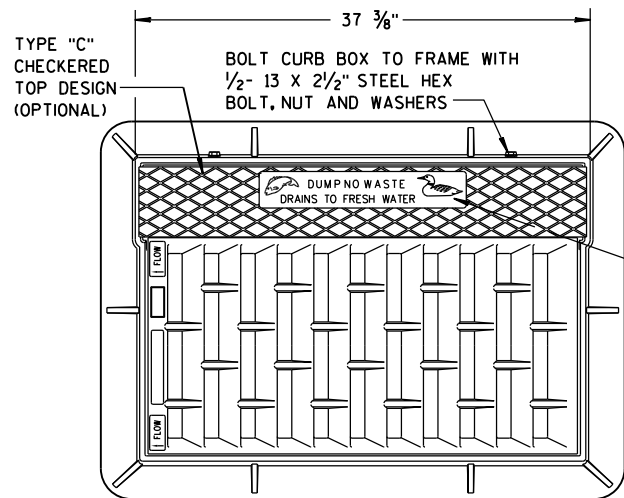


PROJECT NO: 1570-06-72	HWY: USH 8	COUNTY: RUSK	PLAN	SHEET	E
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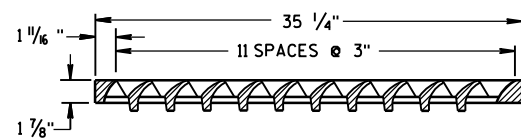
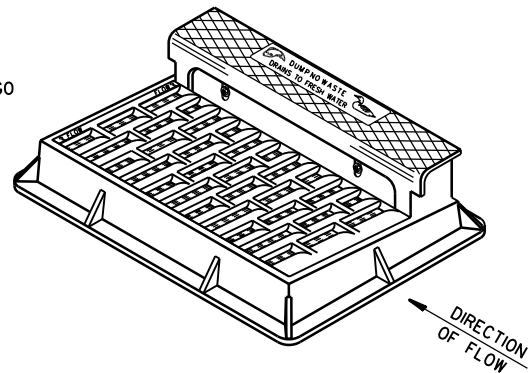


Standard Detail Drawing List

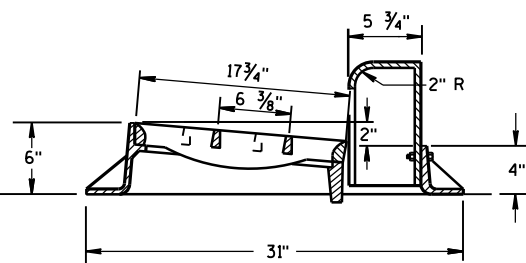
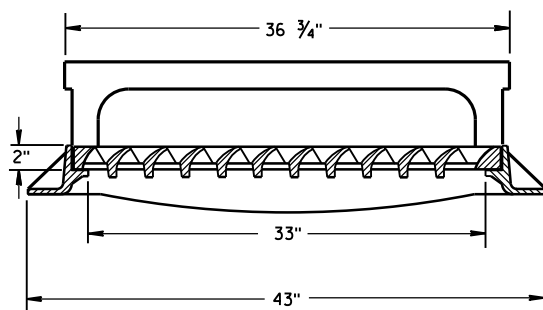
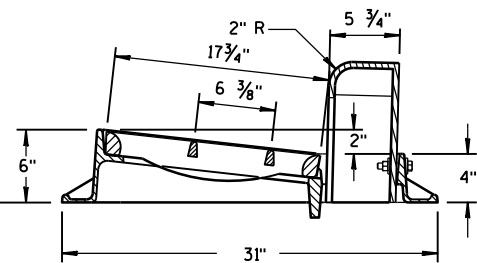
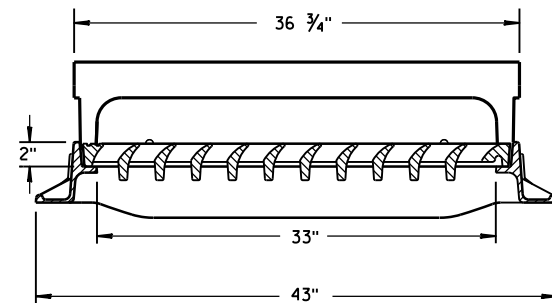
08A05-19A	I NLET COVERS TYPE A, H, A-S, H-S & Z
08D01-22A	CONCRETE CURB & GUTTER
08D01-22B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLI CATIONS
08E09-06	SILT FENCE
08E15-01	CULVERT PIPE CHECK
08F01-11	APRON ENDWALLS FOR CULVERT PIPE
08F04-08	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
13C19-03	HMA LONGITUDI NAL JOINTS
14B42-07A	MIDWEST GUARDRAI L SYSTEM (MGS) GUARDRAI L
14B42-07B	MIDWEST GUARDRAI L SYSTEM (MGS) GUARDRAI L
14B42-07C	MIDWEST GUARDRAI L SYSTEM (MGS) GUARDRAI L
14B42-07D	MIDWEST GUARDRAI L SYSTEM (MGS) GUARDRAI L
14B44-04A	MIDWEST GUARDRAI L SYSTEM ENERGY ABSORBING TERMIN AL (MGS)
14B44-04B	MIDWEST GUARDRAI L SYSTEM ENERGY ABSORBING TERMIN AL (MGS)
14B44-04C	MIDWEST GUARDRAI L SYSTEM ENERGY ABSORBING TERMIN AL (MGS)
14B45-05A	MIDWEST GUARDRAI L SYSTEM THRI E BEAM TRANSI TION (MGS)
14B45-05B	MIDWEST GUARDRAI L SYSTEM THRI E BEAM TRANSI TION (MGS)
14B45-05C	MIDWEST GUARDRAI L SYSTEM THRI E BEAM TRANSI TION (MGS)
14B45-05D	MIDWEST GUARDRAI L SYSTEM THRI E BEAM TRANSI TION (MGS)
14B45-05E	MIDWEST GUARDRAI L SYSTEM THRI E BEAM TRANSI TION (MGS)
14B45-05F	MIDWEST GUARDRAI L SYSTEM THRI E BEAM TRANSI TION (MGS)
14B45-05G	MIDWEST GUARDRAI L SYSTEM THRI E BEAM TRANSI TION (MGS)
14B45-05H	MIDWEST GUARDRAI L SYSTEM THRI E BEAM TRANSI TION (MGS)
14B45-05I	MIDWEST GUARDRAI L SYSTEM THRI E BEAM TRANSI TION (MGS)
14B45-05J	MIDWEST GUARDRAI L SYSTEM THRI E BEAM TRANSI TION (MGS)
14B45-05K	MIDWEST GUARDRAI L SYSTEM THRI E BEAM TRANSI TION (MGS)
14B45-05L	MIDWEST GUARDRAI L SYSTEM THRI E BEAM TRANSI TION (MGS)
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS
15C08-20A	LONGI TUDINAL MARKING (MAINLINE)
15C11-09A	CHANNELIZING DEVICES FLEXIBLE TUBULAR MARKER POST
15C11-09B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-08	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C19-06A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C35-04A	PAVEMENT MARKING (INTERSECTIONS)
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D30-06A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-06B	TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATI ON
15D30-06C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D39-02	TRAFFIC CONTROL, DROP-OFF SIGNING
15D44-02	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES
15D45-03	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH LOOSE GRAVEL
15D48-01	TRAFFIC CONTROL, LANE SHI FT IN FLAGGING OPERATION
15D51-01	TRAFFIC CONTROL, MOBILE OPERATIONS ON AN UNDIVIDED ROADWAY



NOTE:
GRATE IS REVERSIBLE.

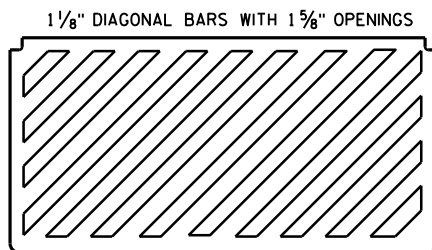


NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"



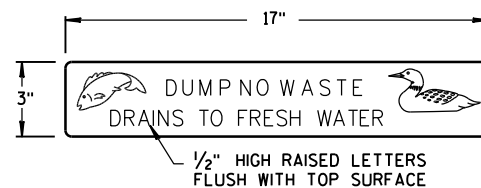
TYPE "H"

NOTE: EITHER CASTING IS ACCEPTABLE

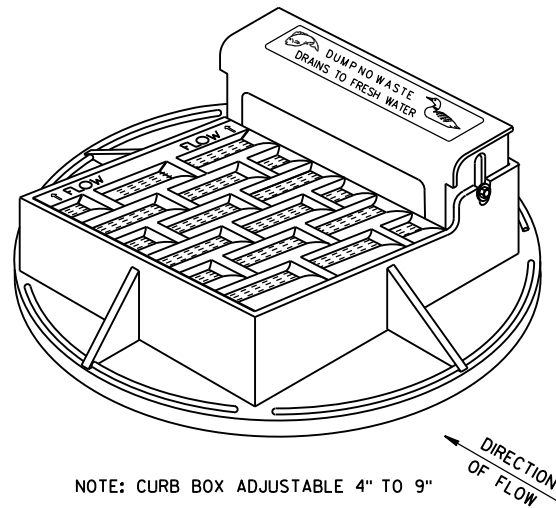


SPECIAL GRATE FOR
TYPE "H" COVER

(MEASURES 35 1/4" X 17 3/4" X 2")
(NOTED AS TYPE H-S ON DRAINAGE TABLE)

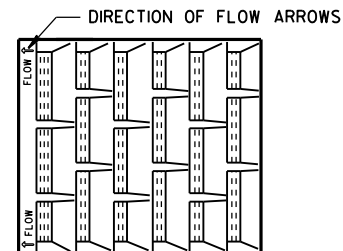


LOGO DETAIL

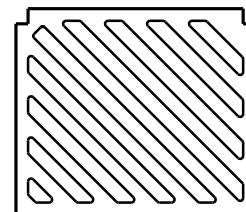


NOTE: CURB BOX ADJUSTABLE 4" TO 9"

NOTE:
GRATE IS REVERSIBLE.

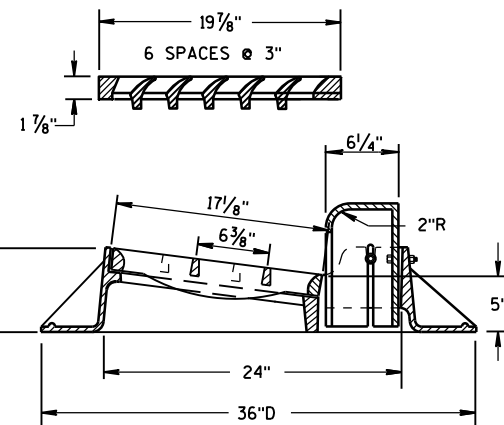
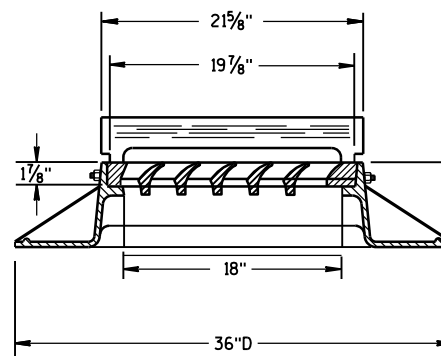


1" DIAGONAL BARS
WITH 1 1/2" OPENINGS

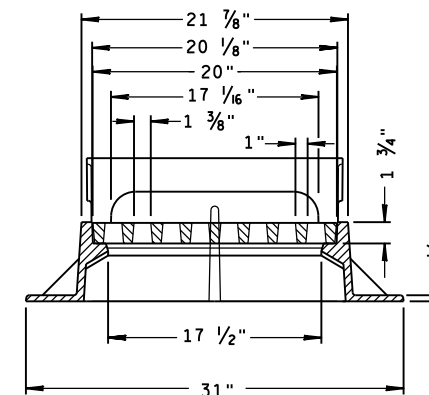
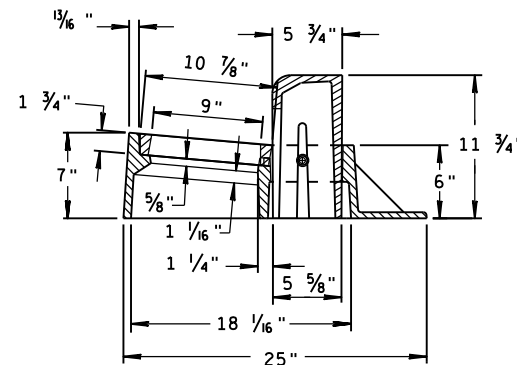


SPECIAL GRATE FOR
TYPE "A" COVER

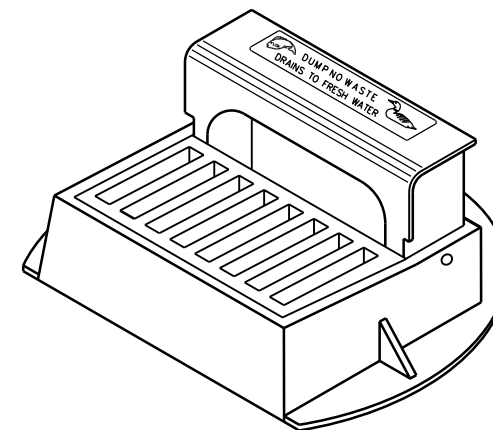
(MEASURES 19 3/4" X 17" X 1 1/8")
(NOTED AS TYPE A-S ON DRAINAGE TABLE)



TYPE "A"



TYPE "Z"

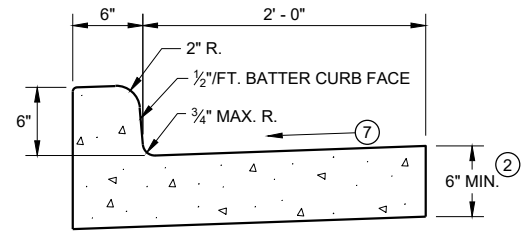


INLET COVERS
TYPE A, H, A-S, H-S & Z

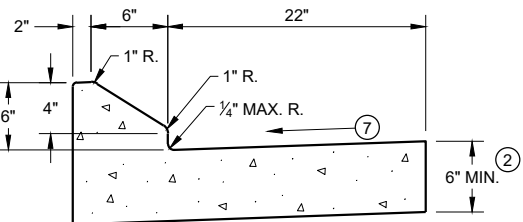
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11-27-13
DATE
FHWA

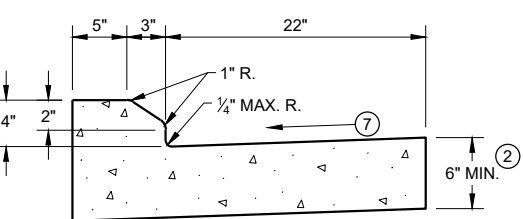
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER



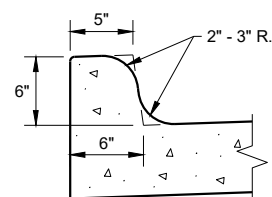
TYPES A^① & D



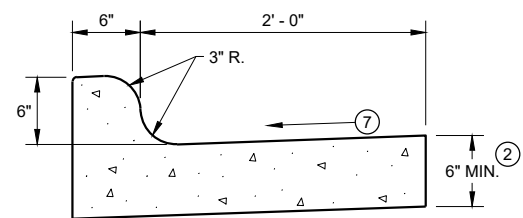
6" SLOPED CURB TYPES G^① & J



4" SLOPED CURB TYPES G^① & J

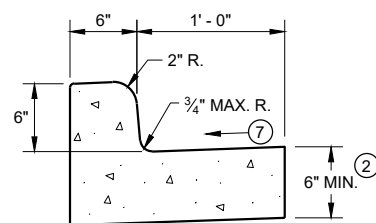


TYPES K^① & L
(OPTIONAL CURB SHAPE)



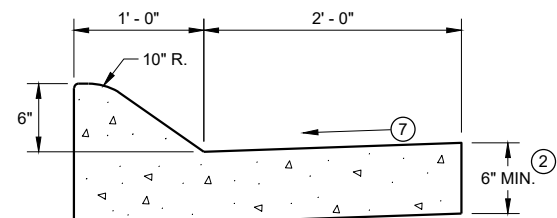
TYPES K^① & L

CONCRETE CURB AND GUTTER 30"

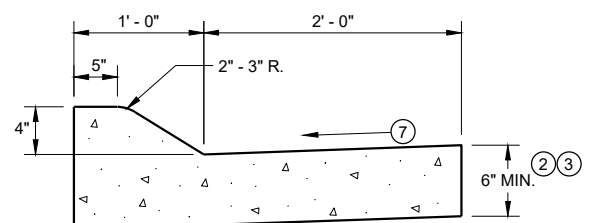


TYPES A^① & D

CONCRETE CURB AND GUTTER 18"

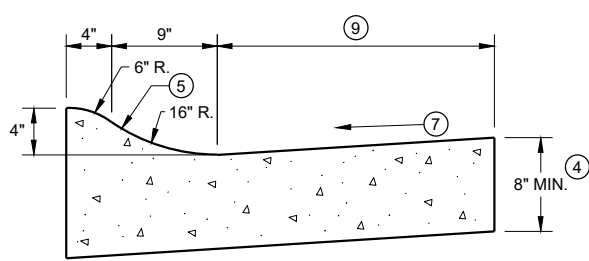


6" SLOPED CURB TYPES A^① & D



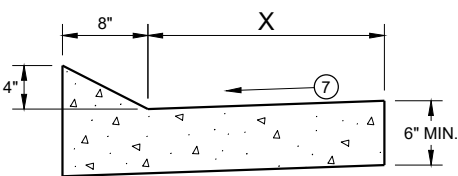
4" SLOPED CURB TYPES A^① & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R^① & T

TBT & TBTT	X
30"	22"
36"	28"

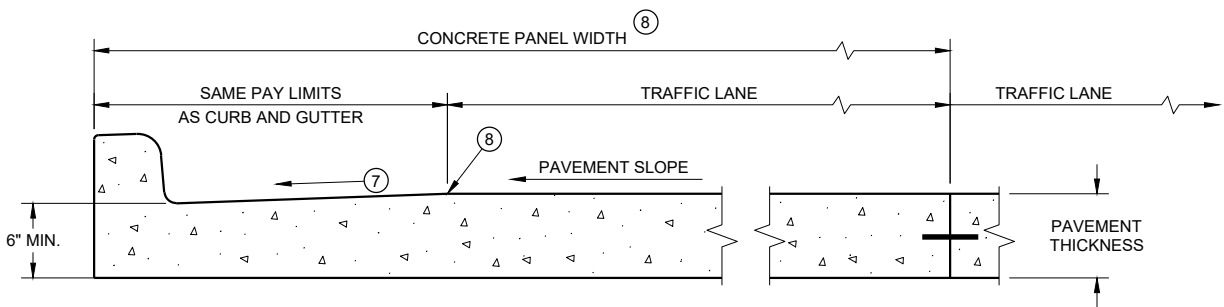


TYPES TBT & TBTT^①

CONCRETE CURB AND GUTTER

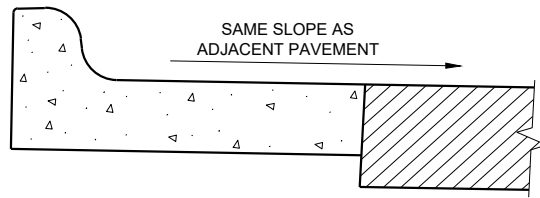
PAVEMENT THICKNESS
AND MAXIMUM CONCRETE
PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT *
WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER^⑥
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

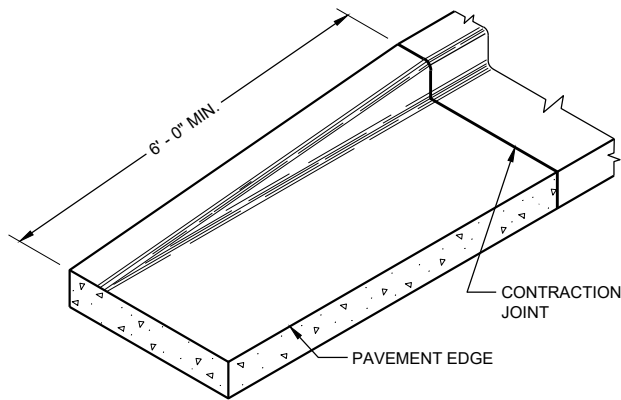
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

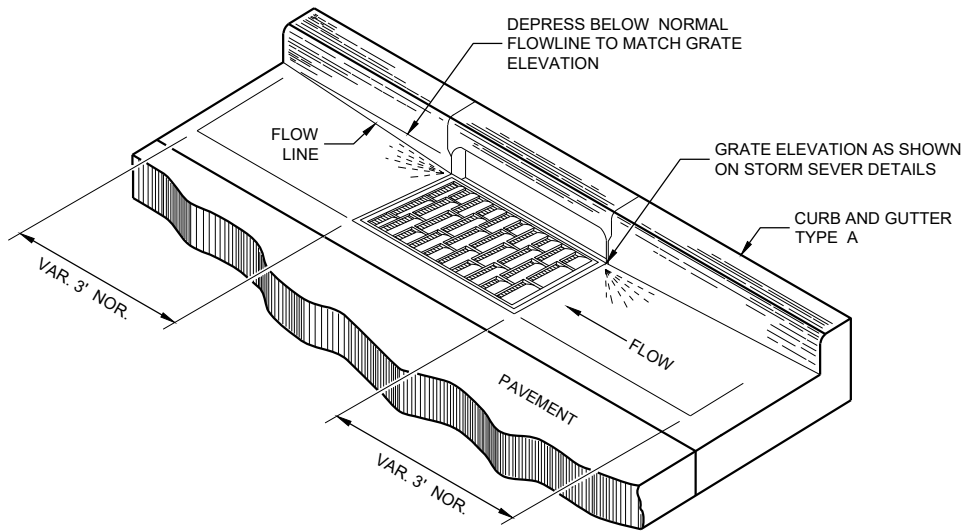
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES

CONCRETE CURB AND GUTTER

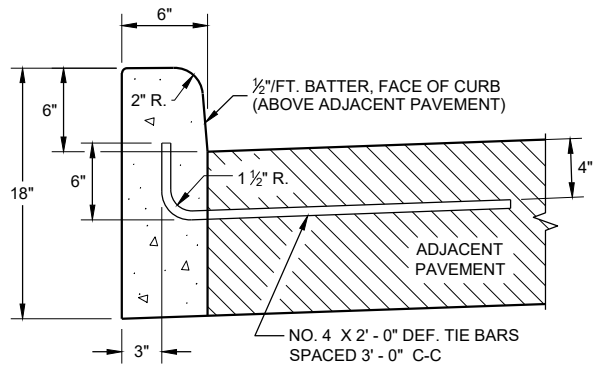
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



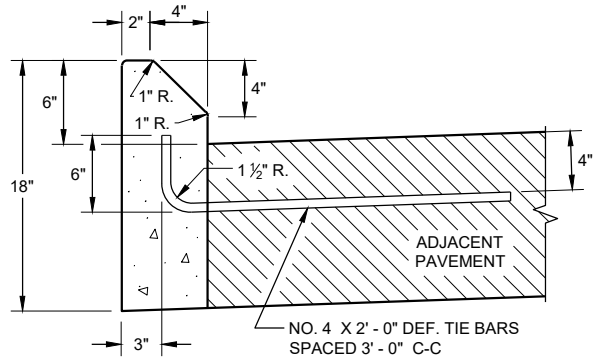
END SECTION CURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS
(TYPICAL H INLET COVER SHOWN)

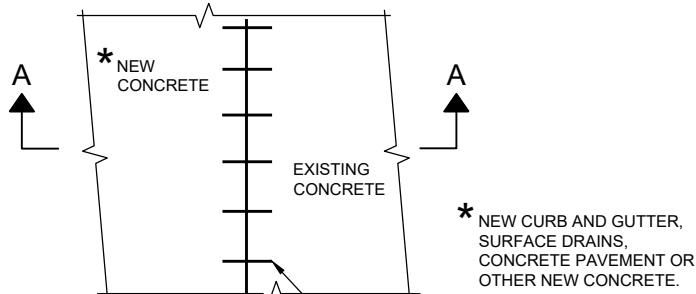


TYPES A^① & D

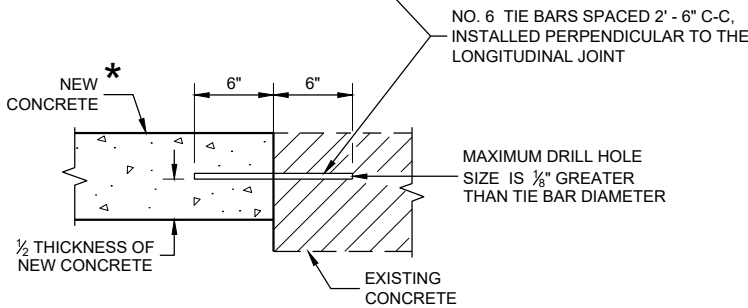


TYPES G^① & J

CONCRETE CURB



PLAN VIEW



SECTION A - A

TIE BARS DRILLED
INTO EXISTING PAVEMENT

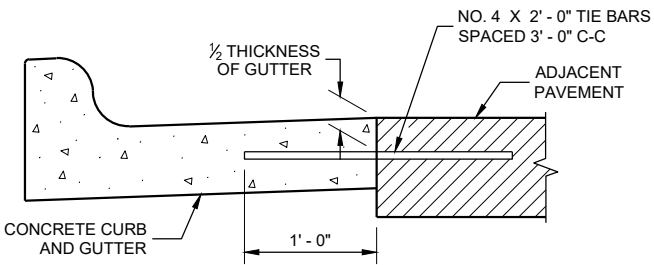
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

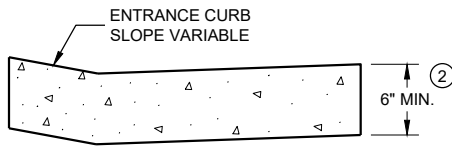
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑨ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.



TYPICAL TIE BAR LOCATION^①



DRIVEWAY ENTRANCE CURB^⑨
(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES
AND CURB AND GUTTER
APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021
DATE /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
ENGINEER

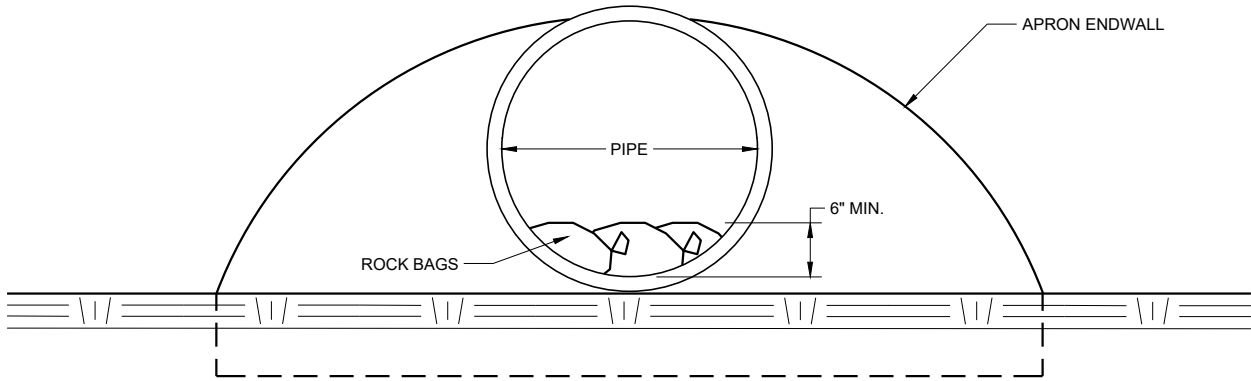
FHWA



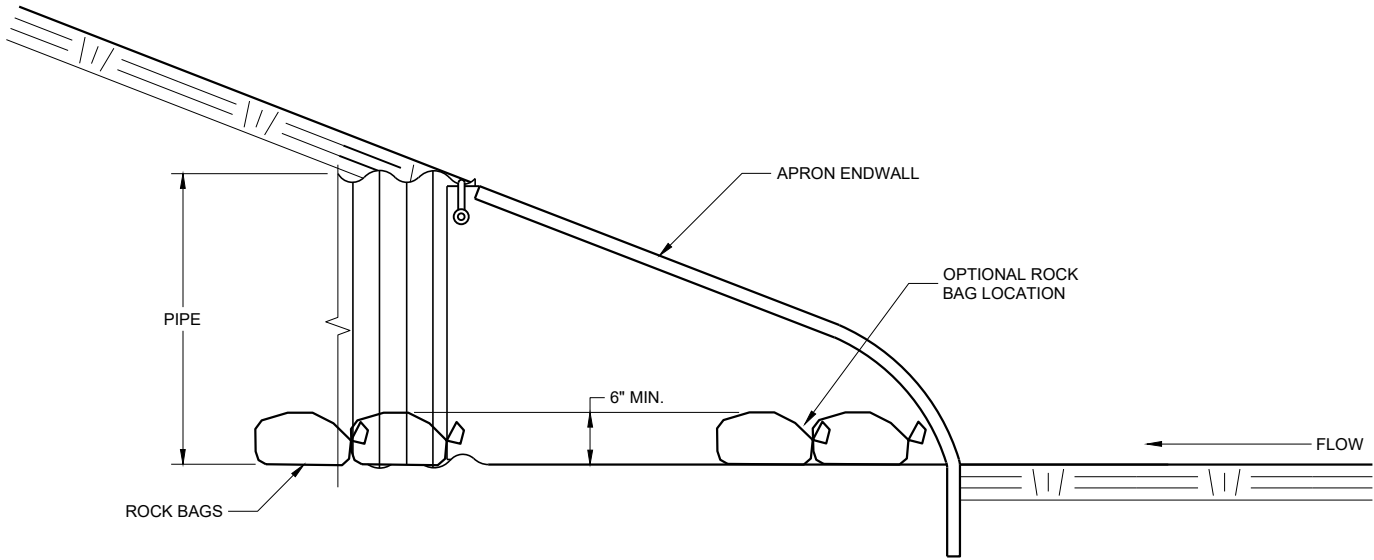
- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1½" X 1½" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



<p style="text-align: center;">SILT FENCE</p>	
<p style="text-align: center;">STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>	
<p>APPROVED</p> <p><u>4-29-05</u></p> <p><u>DATE</u></p>	<p><u>/S/ Beth Canestra</u></p> <p>CHIEF ROADWAY DEVELOPMENT ENGINEER</p>



END VIEW



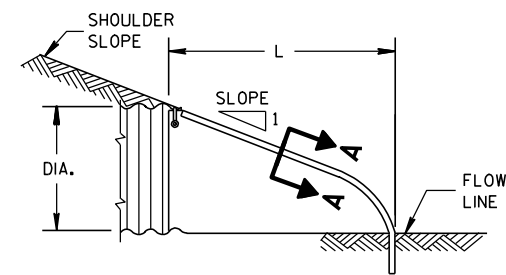
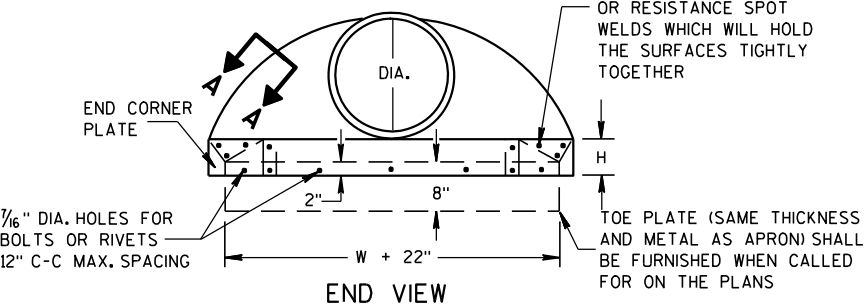
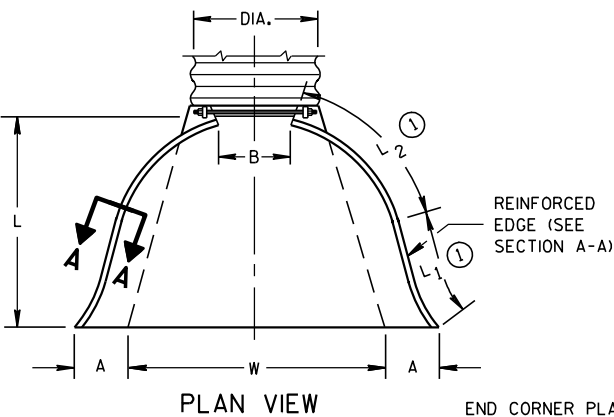
SIDE VIEW

CULVERT PIPE CHECK
(INSTALL ON INLET END ONLY)

CULVERT PIPE CHECK	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2019 DATE	/S/ Daniel Schave EROSION CONTROL ENGINEER
FHWA	

METAL APRON ENDWALLS											
PIPE DIA. (IN.)	MIN. THICK. (Inches)		DIMENSIONS (Inches)							APPROX. SLOPE	BODY
	STEEL	ALUM.	A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 ①	L2 ①	W (±2")		
12	.064	.060	6	6	6	21	12	17 1/2	24	2 1/2 to 1	1 Pc.
15	.064	.060	7	8	6	26	14	21 3/4	30	2 1/2 to 1	1 Pc.
18	.064	.060	8	10	6	31	15	28 1/4	36	2 1/2 to 1	1 Pc.
21	.064	.060	9	12	6	36	18	29 5/8	42	2 1/2 to 1	1 Pc.
24	.064	.075	10	13	6	41	18	37 1/4	48	2 1/2 to 1	1 Pc.
30	.079	.075	12	16	8	51	18	52 1/4	60	2 1/2 to 1	1 Pc.
36	.079	.105	14	19	9	60	24	59 3/4	72	2 1/2 to 1	2 Pc.
42	.109	.105	16	22	11	69	24	75 5/8	84	2 1/2 to 1	2 Pc.
48	.109	.105	18	27	12	78	24	81	90	2 1/4 to 1	3 Pc.
54	.109	.105	18	30	12	84	30	85 1/2	102	2 1/4 to 1	3 Pc.
60	.109x	.105x	18	33	12	87	—	—	114	2 to 1	3 Pc.
66	.109x	.105x	18	36	12	87	—	—	120	2 to 1	3 Pc.
72	.109x	.105x	18	39	12	87	—	—	126	2 to 1	3 Pc.
78	.109x	.105x	18	42	12	87	—	—	132	1 1/2 to 1	3 Pc.
84	.109x	.105x	18	45	12	87	—	—	138	1 1/2 to 1	3 Pc.
90	.109x	.105x	18	37	12	87	—	—	144	1 1/2 to 1	3 Pc.
96	.109x	.105x	18	35	12	87	—	—	150	1 1/2 to 1	3 Pc.

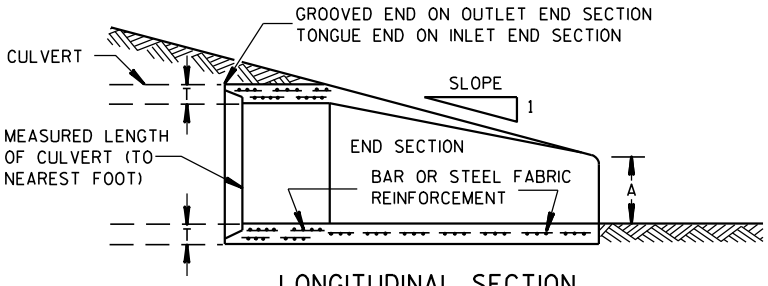
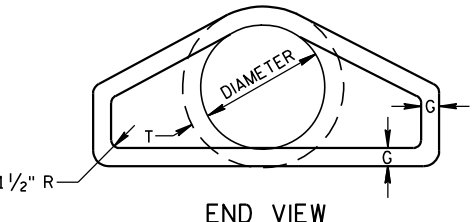
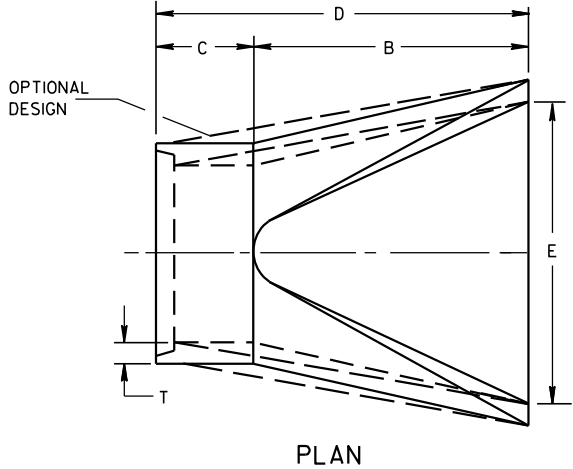
* EXCEPT CENTER PANEL
SEE GENERAL NOTES



METAL ENDWALLS

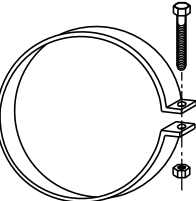
REINFORCED CONCRETE APRON ENDWALLS											
PIPE DIA. (IN.)	DIMENSIONS (Inches)							APPROX. SLOPE			
	T	A	B	C	D	E	G				
12	2	4	24	48 7/8	72 7/8	24	2	3 to 1			
15	2 1/4	6	27	46	73	30	2 1/4	3 to 1			
18	2 1/2	9	27	46	73	36	2 1/2	3 to 1			
21	2 3/4	9	36	37 1/2	73 1/2	42	2 3/4	3 to 1			
24	3	9 1/2	43 1/2	30	73 1/2	48	3	3 to 1			
27	3 1/4	10 1/2	49 1/2	24	73 1/2	54	3 1/4	3 to 1			
30	3 1/2	12	54	19 3/4	73 1/2	60	3 1/2	3 to 1			
36	4	15	63	34 3/4	97 3/4	72	4	3 to 1			
42	4 1/2	21	63	35	98	78	4 1/2	3 to 1			
48	5	24	72	26	98	84	5	3 to 1			
54	5 1/2	27	65	33 1/4-35	98 1/4-100	90	5 1/2	2 1/2 to 1			
60	6	30-35	60	39	99	96	5	2 to 1			
66	6 1/2	24-30	72-78	21-27	99	102	5 1/2	2 to 1			
72	7	24-36	78	21	99	108	6	2 to 1			
78	7 1/2	24-36	78	21	99	114	6 1/2	2 to 1			
84	8	36	90 1/2	21	111 1/2	120	6 1/2	1 1/2 to 1			
90	8 1/2	41	87 1/2	24	111 1/2	132	6 1/2	1 1/2 to 1			

* MINIMUM
** MAXIMUM

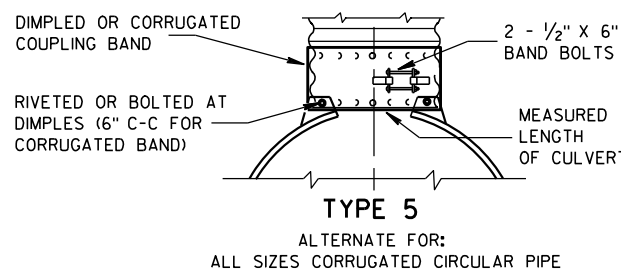
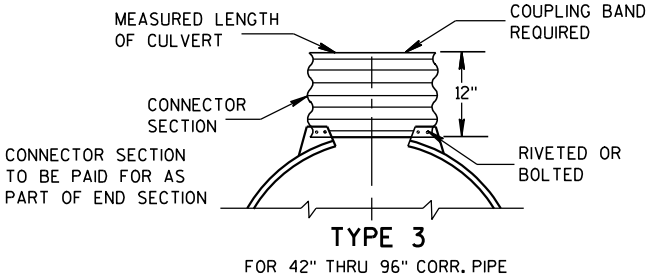
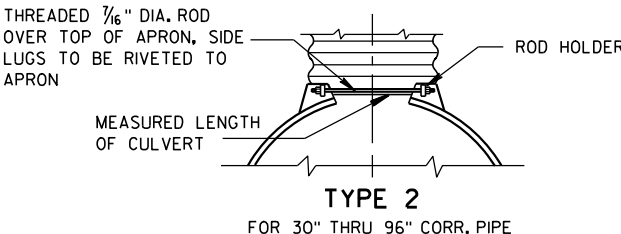
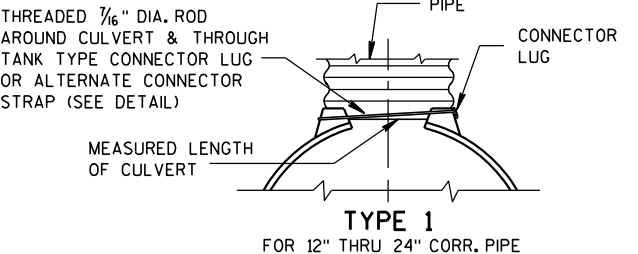


CONCRETE ENDWALLS

1" WIDE, 12 GA. (0.109" THICK) GALVANIZED STRAP WITH STANDARD 6" X 1/2" BAND BOLT AND NUT



ALTERNATE FOR TYPE 1 CONNECTION
END SECTION CONNECTOR STRAP



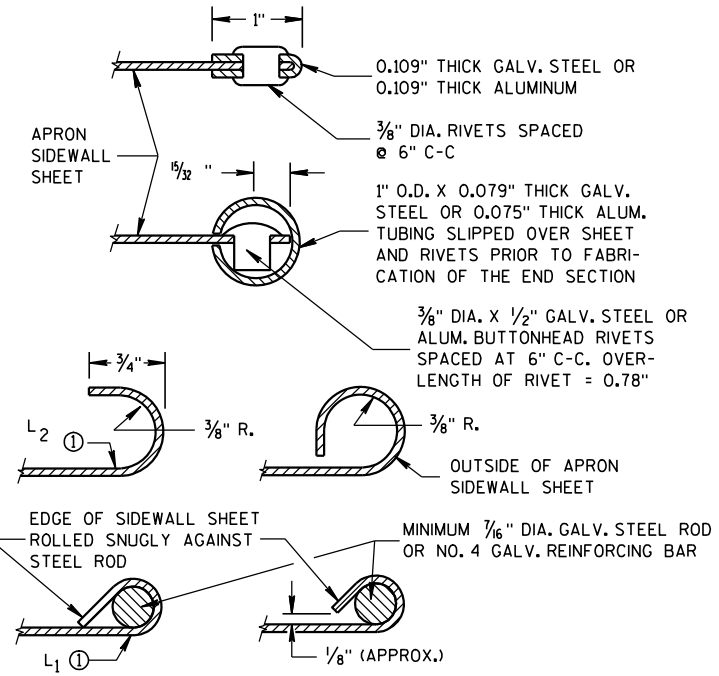
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL, AND CORRUGATED BAND FITS INSIDE ENDWALL. DIMPLED BAND MAY BE USED WITH HELICALLY CORRUGATED PIPE.

FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5 AS APPLICABLE.

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VISE VERSA. GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

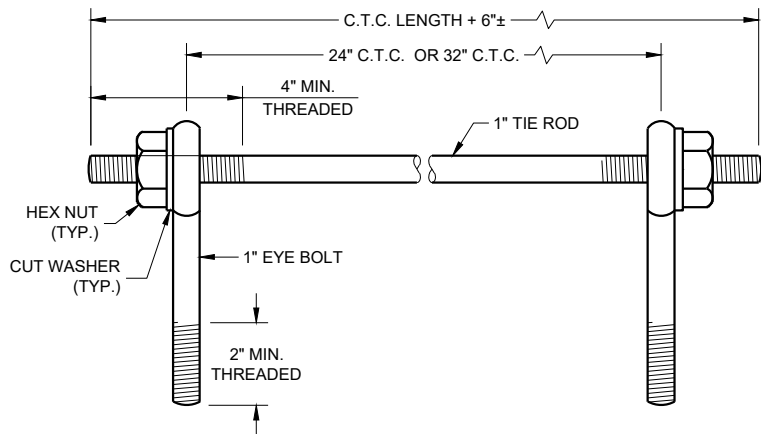
ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

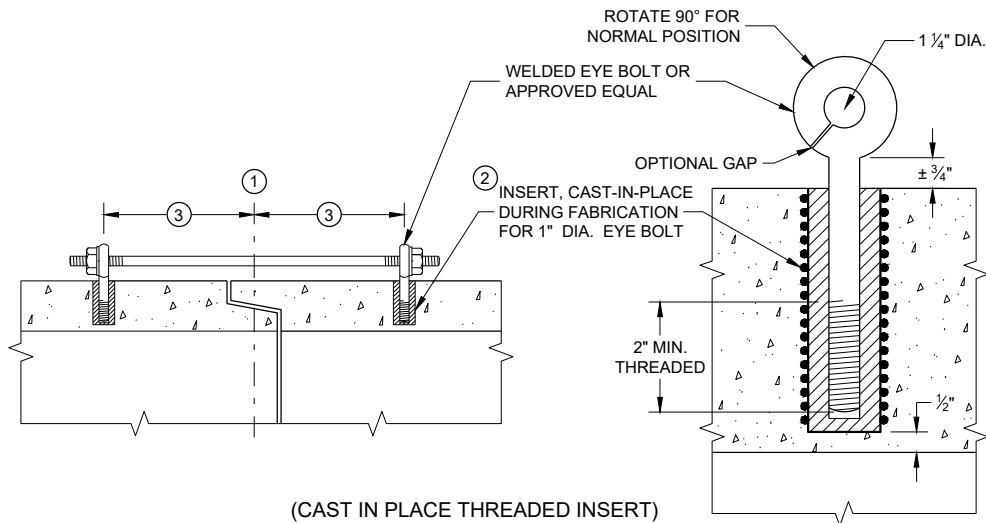
① FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

APRON ENDWALLS FOR CULVERT PIPE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 11/30/94 DATE	/S/ Rory L. Rhinesmith CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



EYE BOLTS AND TIE ROD

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)



(CAST IN PLACE THREADED INSERT)

LONGITUDINAL SECTIONS

GENERAL NOTES

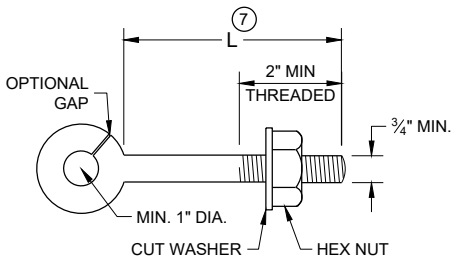
DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

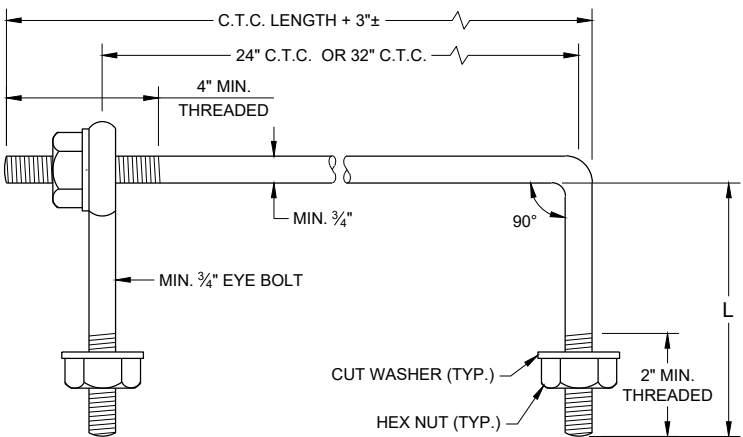
JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

- 1 CENTER LINE OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.
- 2 THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.
- 3 HOLES SHALL BE CAST-IN-PLACE OR DRILLED PER THE APPLICABLE DETAIL, AND EQUAL DISTANCE FROM THE CENTERLINE OF THE JOINT.
- 4 BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.
- 5 OPENING TO BE ROD DIAMETER PLUS 1 INCH.
- 6 LENGTH ADEQUATE TO EXTEND TO WITHIN 1/2 INCH OF THE INNER SURFACE OF THE PIPE.
- 7 EYE BOLT LENGTH DETERMINED BY WALL THICKNESS, BELL THICKNESS AND BOLT PROJECTION INSIDE PIPE.

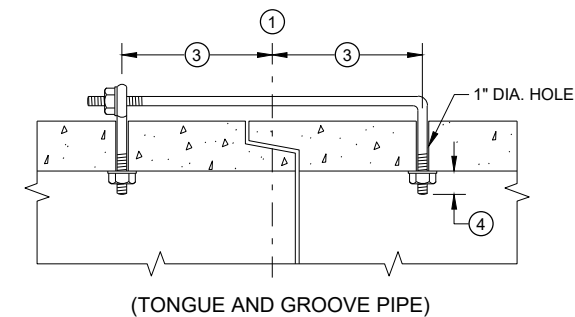


EYE BOLT 7

NOTE: TWO EYE BOLTS MAY BE USED WITH A 30" OR 38" LONG THREADED ROD IN LIEU OF THE 90° BENT TIE ROD.



EYE BOLT AND TIE ROD

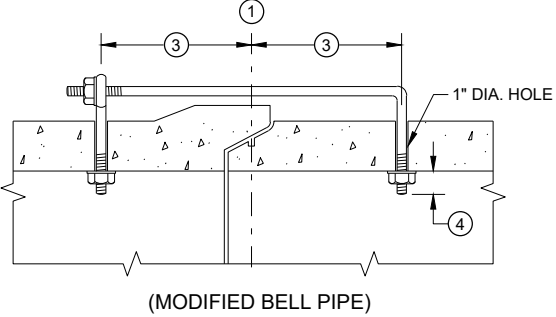


(TONGUE AND GROOVE PIPE)

LONGITUDINAL SECTION

(JOINT TIES FOR 18" TO 66" DIA. CONCRETE PIPE)

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)

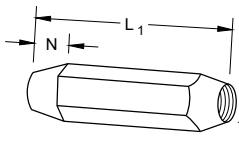


(MODIFIED BELL PIPE)

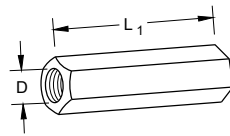
ADJUSTABLE TIE ROD TABLE

PIPE DIAMETER	TIE ROD DIAMETER	D	L ₁	N
12 - 60	5/8	5/8	5	1/2
66 - 84	3/4	3/4	5	1/2
90 - 144	1	1	7	1 1/16

DIMENSIONS SHOWN ARE IN INCHES

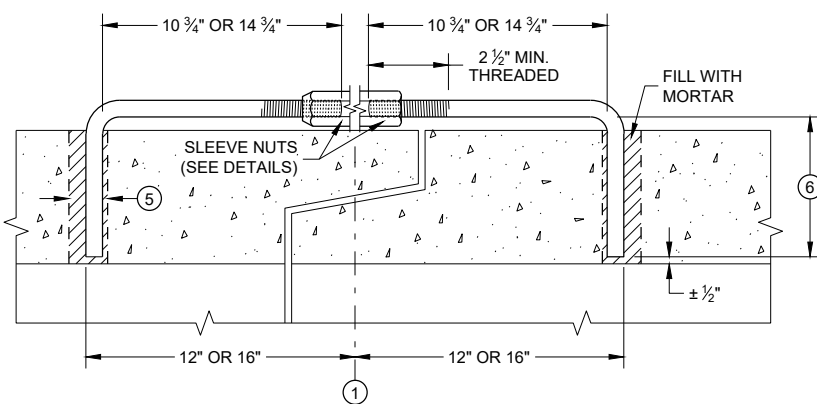


TAPERED



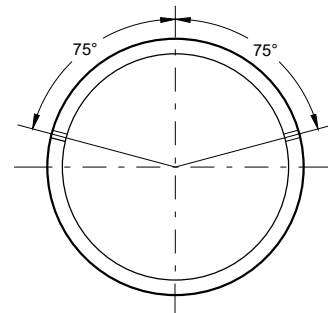
PLAIN

RIGHT AND LEFT THREADS
SLEEVE NUTS



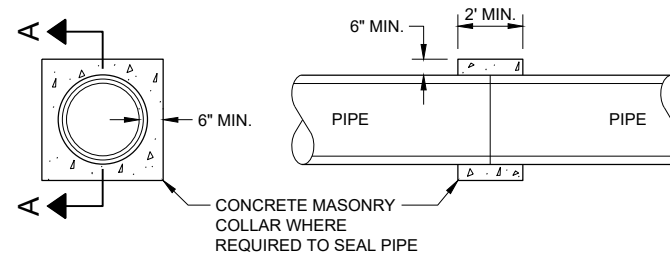
LONGITUDINAL SECTION

ADJUSTABLE TIE ROD (ALTERNATE NO. 3)



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION



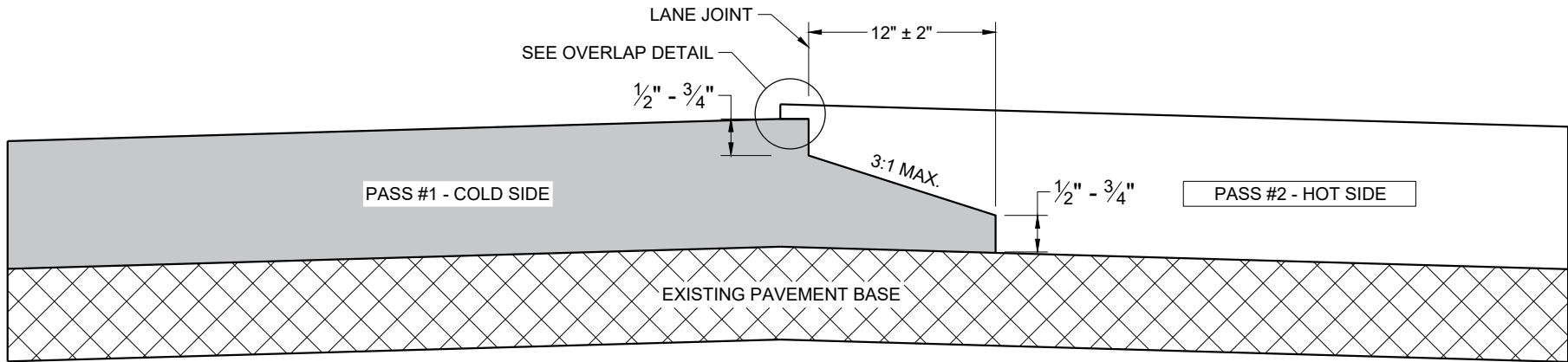
SECTION A - A

CONCRETE COLLAR DETAIL

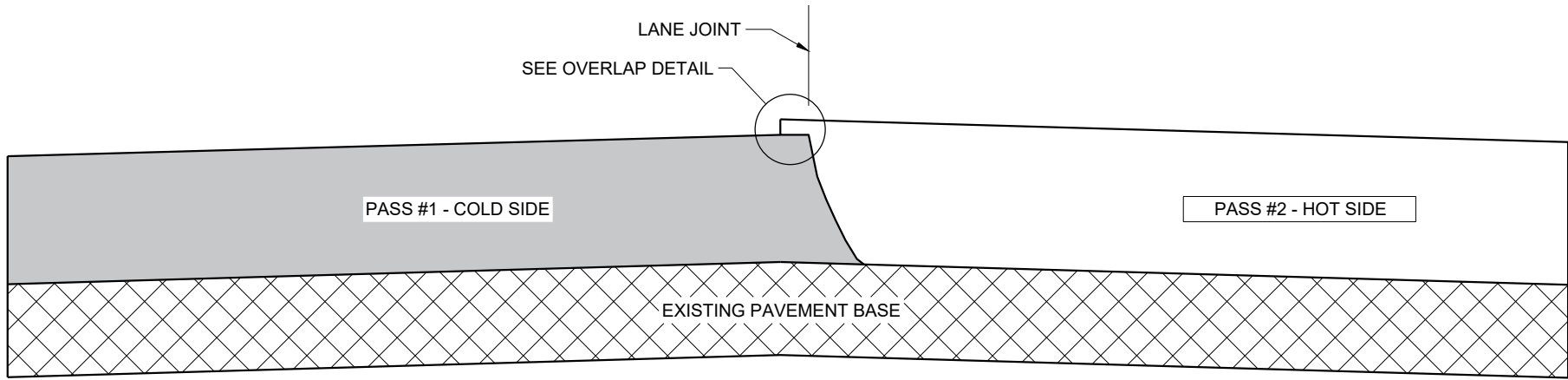
JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

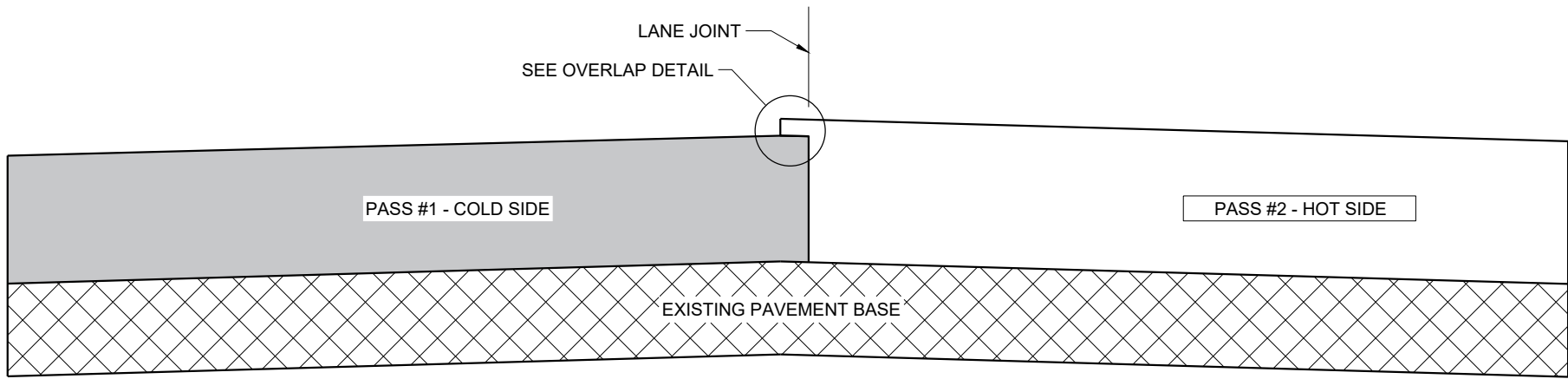
APPROVED
November 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA ENGINEER



TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)

GENERAL NOTES

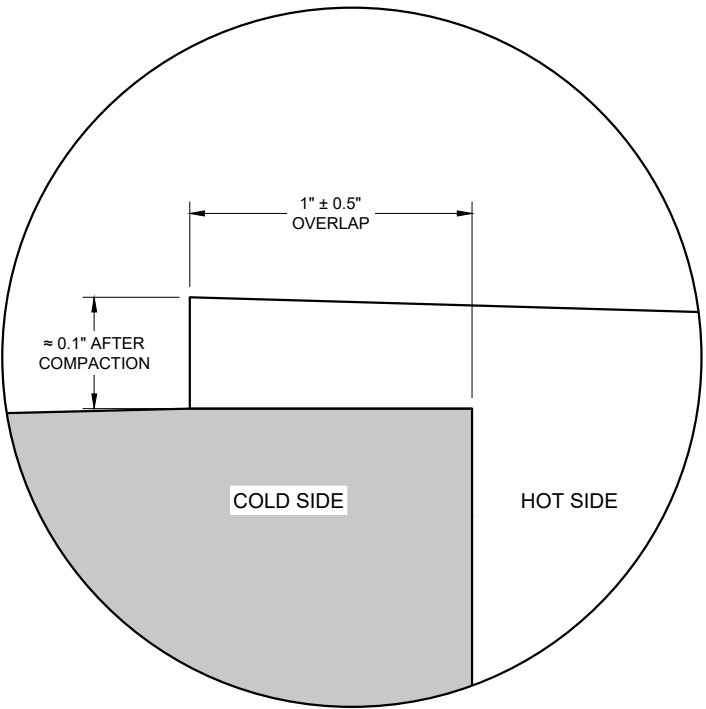
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY 1" ± 0.5" AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



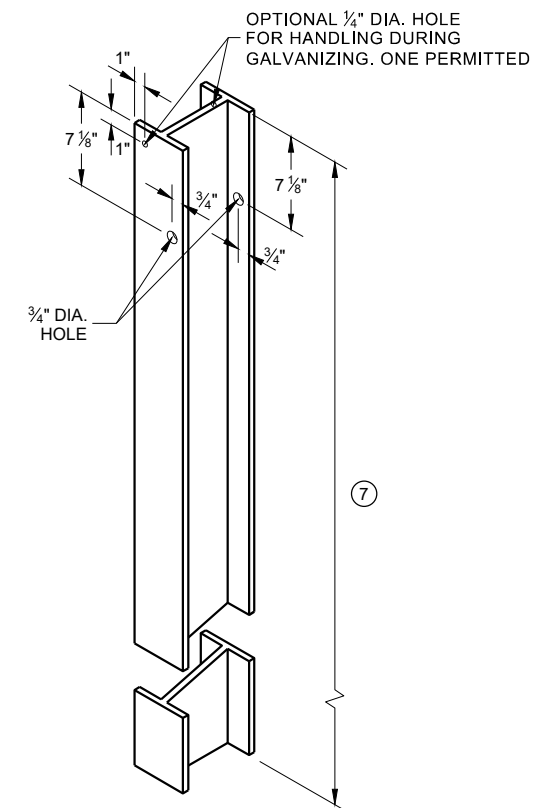
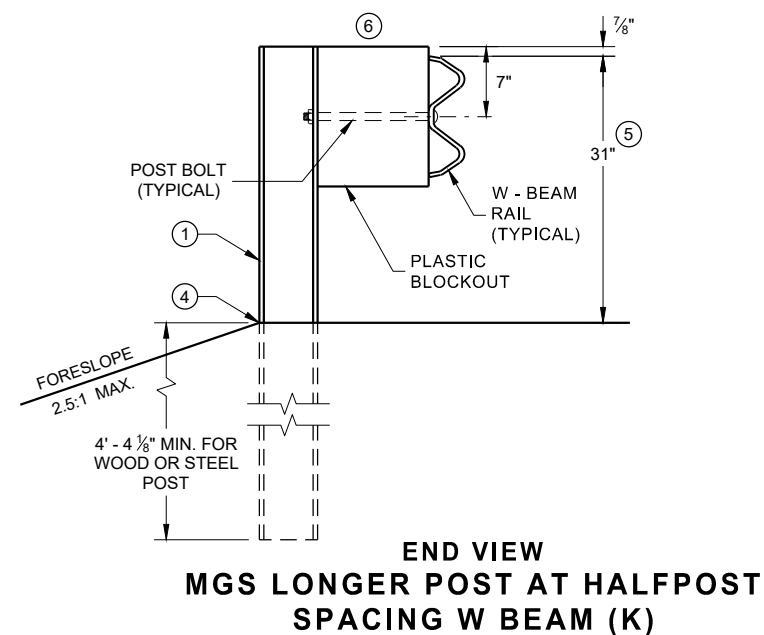
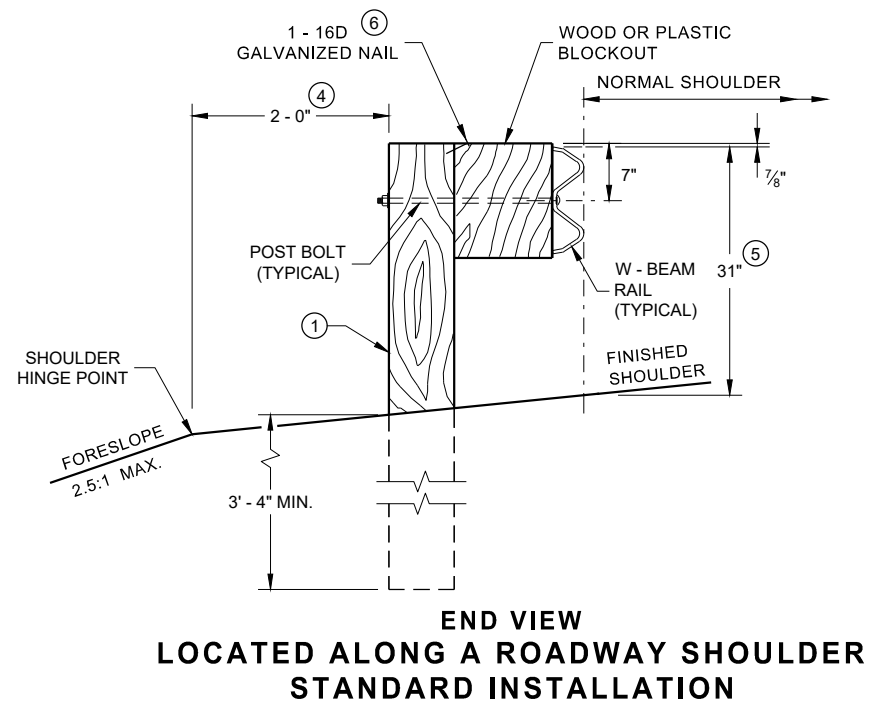
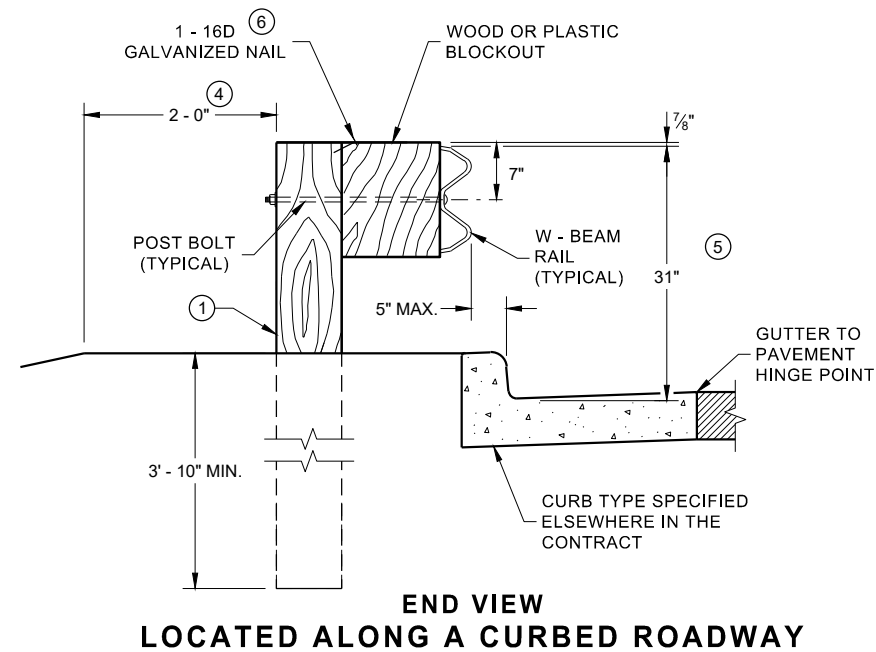
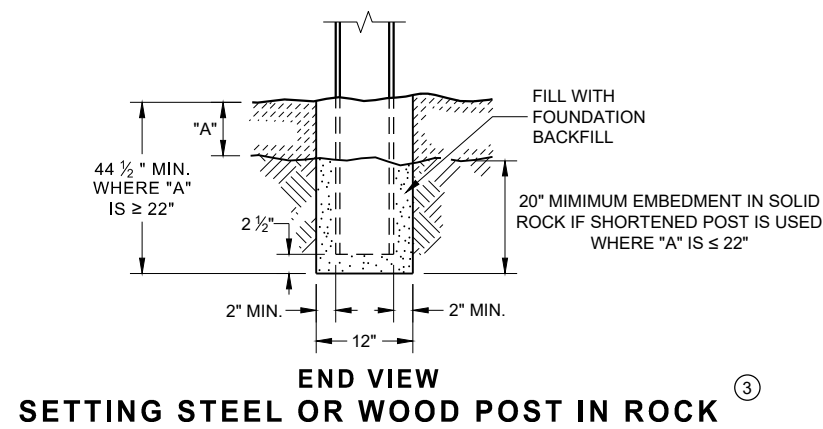
OVERLAP DETAIL (TYPICAL)

HMA LONGITUDINAL JOINTS

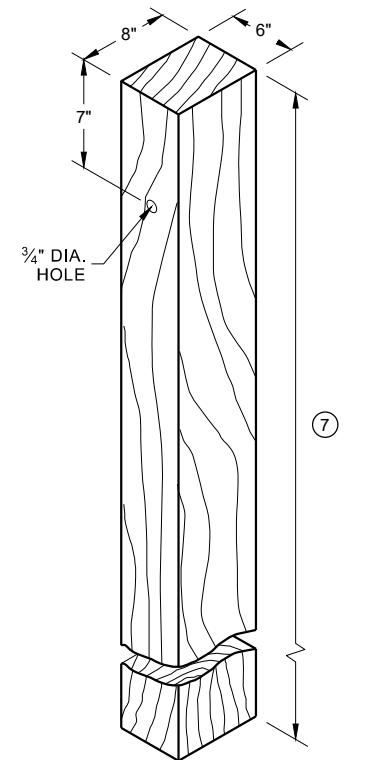
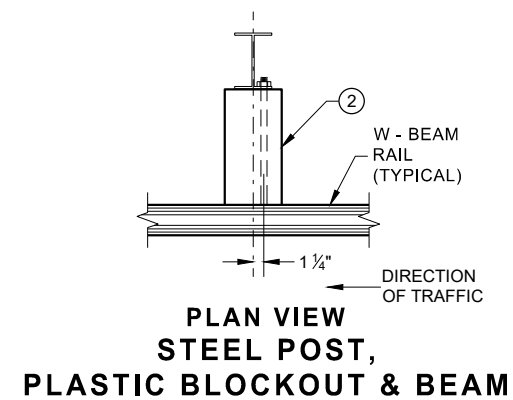
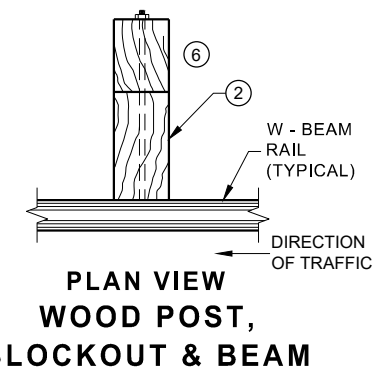
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2020 /S/ Steven Hefel
DATE HMA PAVEMENT ENGINEER
FHWA

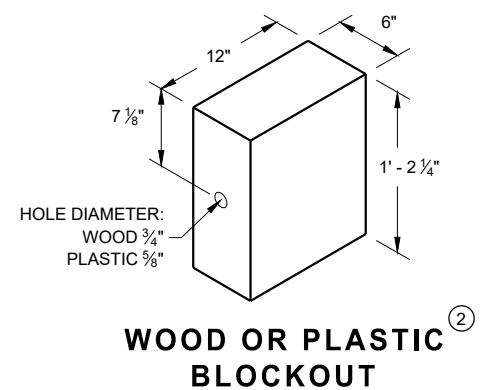
- ① WOOD OR STEEL POSTS (w6X9 OR w6X8.5) MAY BE USED. DO NOT INTERMIX WOOD AND STEEL POSTS. INSTALL STEEL POSTS WITH HOLES ON APPROACHING TRAFFIC SIDE.
- ② USE WOOD OR APPROVED PLASTIC BLOCKOUTS. WOOD BLOCKOUTS MAY BE CONSTRUCTED OUT OF TWO OR MORE WOOD BLOCKOUTS. SEE ALTERNATE WOOD BLOCKOUT DETAIL. DIMENSIONS OF APPROVED PLASTIC BLOCKOUTS MAY VARY.
- ③ IF ROCK IS ENCOUNTERED DURING EXCAVATION, PROVIDE A HOLE 12 INCHES IN DIAMETER EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE APPROXIMATELY 2 1/2" INCHES OF GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE. CUT THE POSTS THE TO LENGTH AND INSTALL. BACKFILL WITH EXCAVATED MATERIAL AND COMPACT. BACKFILL IS TO BE FREE OF LARGE ROCKS.
- ④ WHEN THE DISTANCE FROM BACK OF POST TO SHOULDER HINGE POINT IS LESS THAN 2 FEET INSTALL LONGER POST AT HALF POST SPACING (K).
- ⑤ FOR NEW MGS INSTALLATION TOP OF W-BEAM RAIL TOLERANCE IS $\pm 1"$. FOR EXISTING MGS INSTALLATION TOP OF W-BEAM IS BETWEEN 27 3/4" TO 32".
- ⑥ WHEN USING STEEL POST AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.
- ⑦ TOTAL POST LENGTH FOR TYPE K IS 7' - 0".
TOTAL POST LENGTH FOR OTHER MGS TYPES IS 6' - 0".

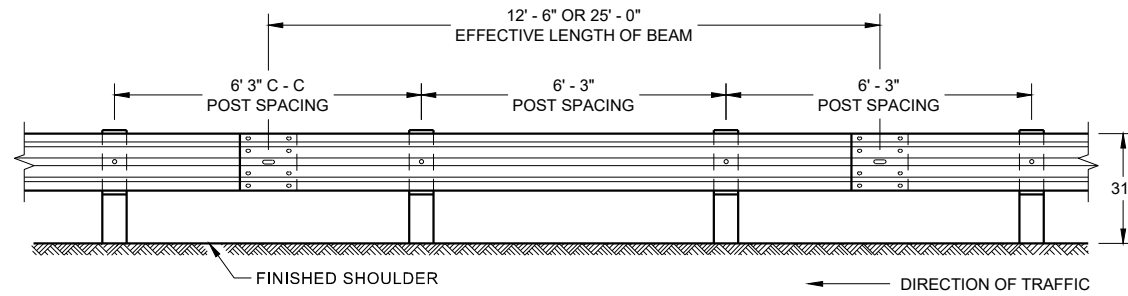


**STEEL POST & HOLE
PUNCHING DETAIL
(W 6 X 9) ①**

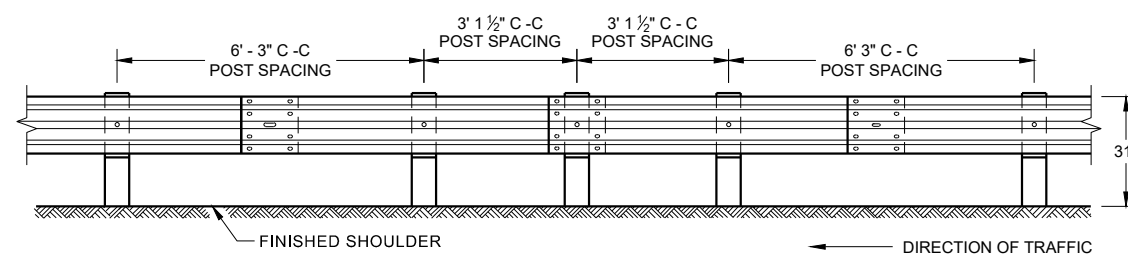


WOOD POST (6" X 8") NOMINAL ⁽¹⁾

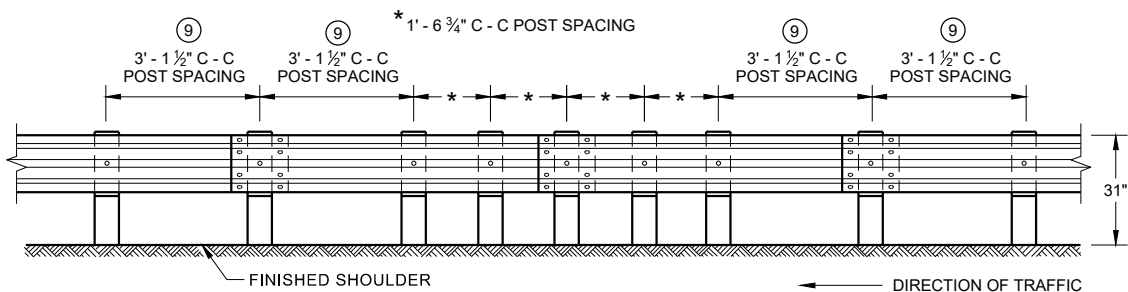




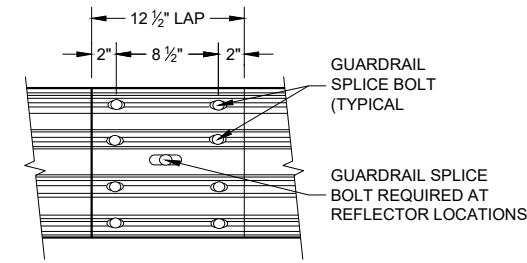
**FRONT VIEW
POST SPACING STANDARD INSTALLATION**



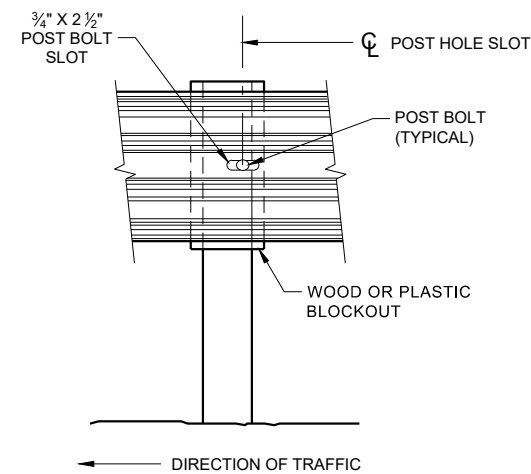
**FRONT VIEW
HALF POST SPACING (HS) AND
HALF POST SPACING WITH LONGER POSTS (K)**



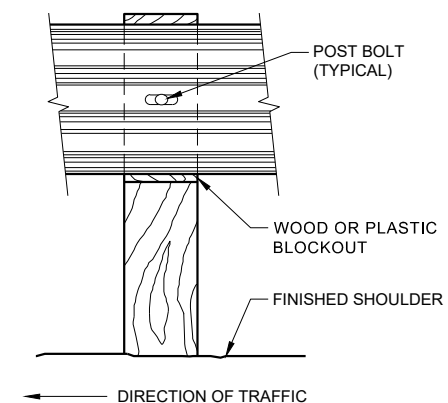
**FRONT VIEW
QUARTER POST SPACING (QS)**



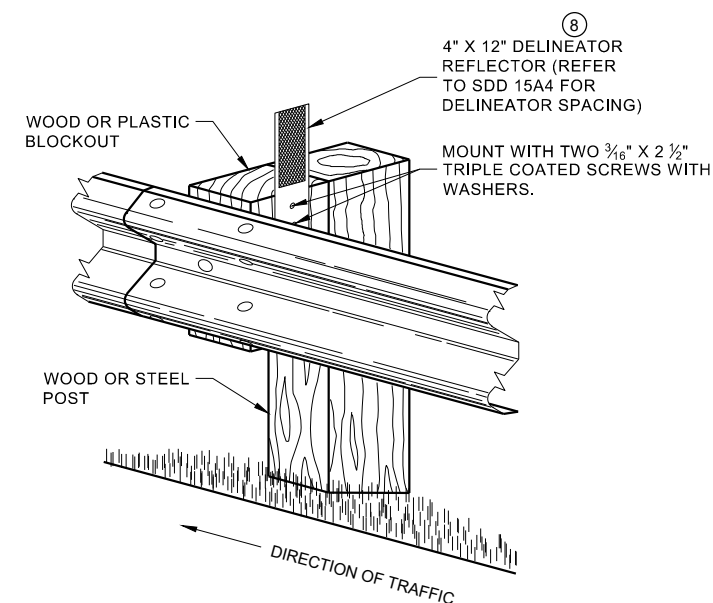
**FRONT VIEW
MID-SPAN BEAM SPLICE**



FRONT VIEW AT STEEL POST



FRONT VIEW AT WOOD POST



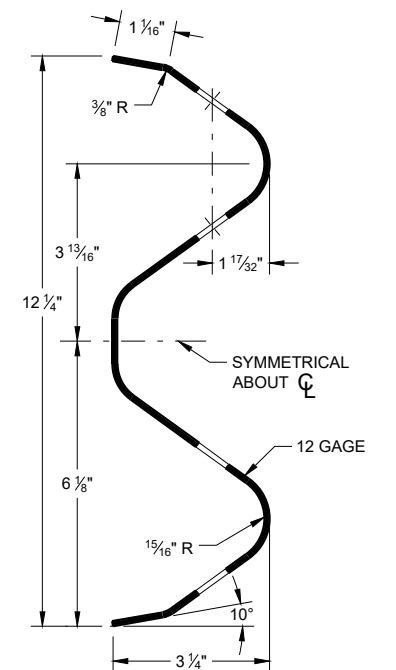
**ONE SIDED REFLECTOR DETAIL
AND TYPICAL INSTALLATION**

GENERAL NOTES

- 8 DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.
- 9 25 FEET OF HALF POST SPACING IS REQUIRED ON APPROACH AND DEPARTURE ENDS OF QUARTER POST SPACING.

POST BOLTS ARE A 3/8" DIAMETER ASTM A307 GUARDRAIL BOLT. A POST BOLT REQUIRES 3/4" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT AND 3/8" DIAMETER F844 FLAT WASHER. POST BOLTS MAY BE LONGER IF MULTIPLE BLOCKOUTS ARE BEING USED.

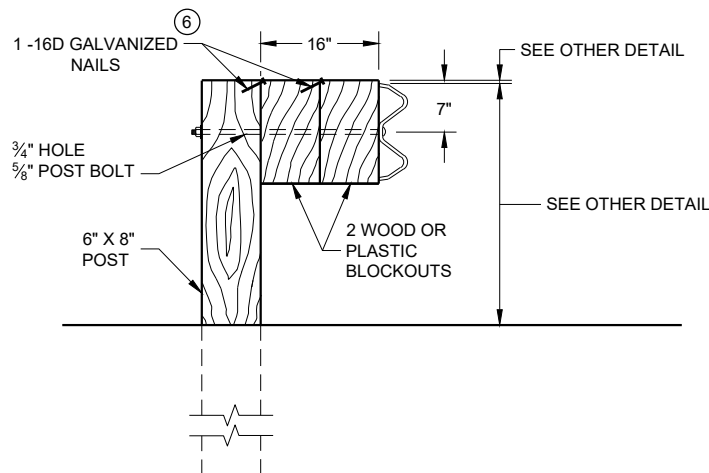
GUARD RAIL SPLICE BOLTS ARE A 3/8" DIAMETER ASTM A307 GUARDRAIL HEAD BOLT. A GUARDRAIL SPLICE BOLT REQUIRES 3/4" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT.



SECTION THRU W-BEAM RAIL

**MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL**

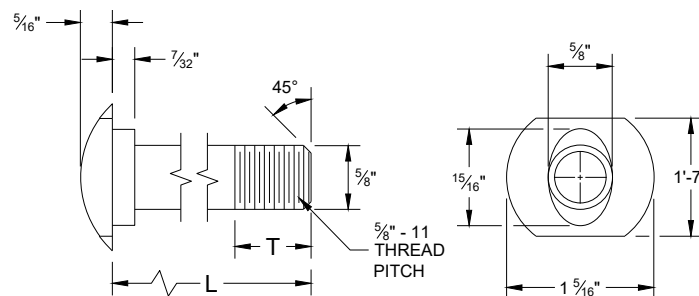
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



DETAIL FOR 16" BLOCKOUT DEPTH

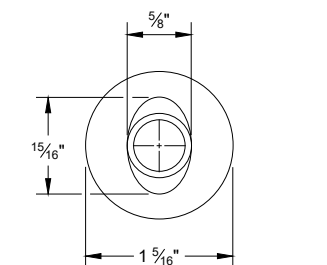
IT IS ACCEPTABLE TO USE BLOCKOUTS UP TO 16" DEEP TO INCREASE THE POST OFFSET TO AVOID UNDERGROUND OBSTACLES. THERE IS NO LIMIT TO THE NUMBER OF POSTS THAT CAN HAVE ADDITIONAL BLOCKOUTS UP TO 16" DEEP.

- NOTE:
1. ALL FILLETS SHALL HAVE A MINIMUM RADIUS OF $\frac{3}{16}$ ".
 2. IF THE BOLT EXTENDS MORE THAN $\frac{1}{4}$ " FROM THE NUT THE BOLT SHOULD BE TRIMMED BACK.

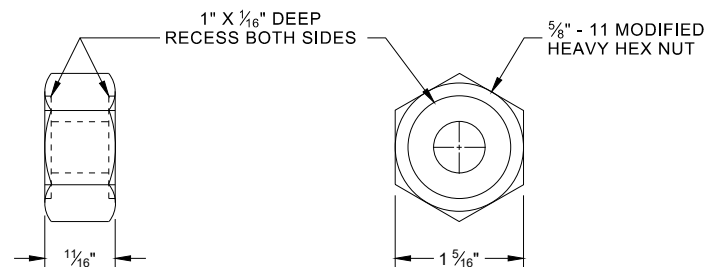


POST BOLT TABLE

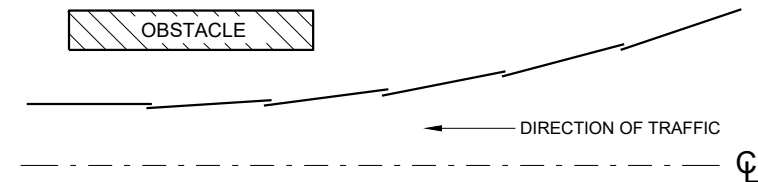
L	T (MIN.)
1 1/4"	1 1/8"
2"	1 3/4"
10"	4"
14"	4 1/16"
18"	4"
21"	4 1/16"
25"	4"



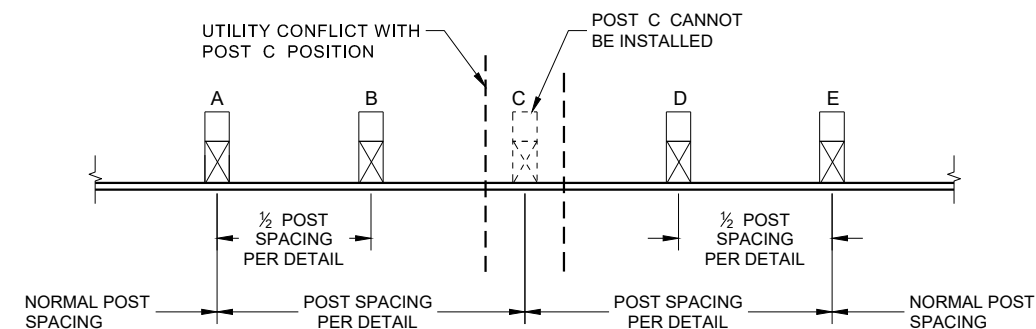
ALTERNATE BOLT HEAD



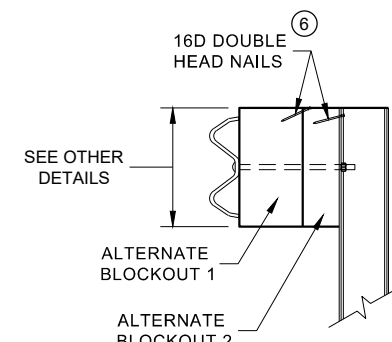
POST BOLT, SPLICE BOLT AND RECESS NUT



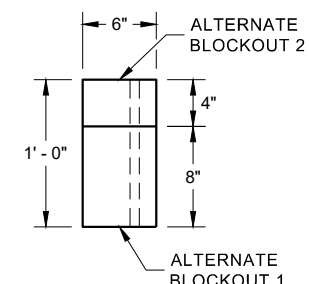
PLAN VIEW
BEAM LAPPING DETAIL



POST DRIVING FOR CONTINUOUS
UNDERGROUND OBSTRUCTION



SIDE VIEW



PLAN VIEW

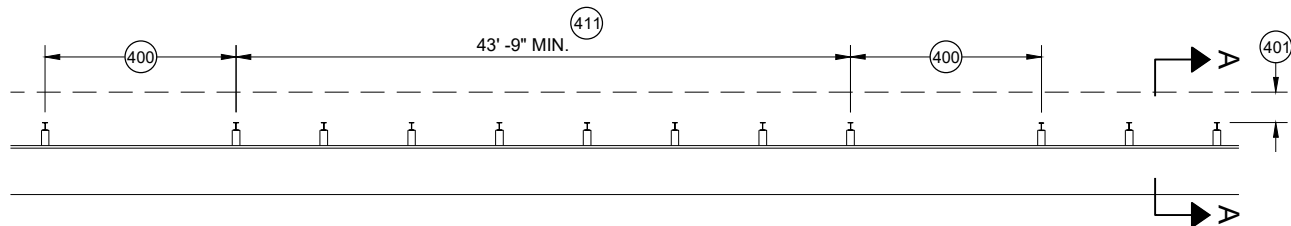
ALTERNATE WOOD
BLOCKOUT DETAIL

- NOTES:
- UNDER SPECIAL CIRCUMSTANCES, SUCH AS AVOIDING OBSTACLES THAT ARE NOT RELOCATED, IT IS ACCEPTABLE TO INSTALL ADDITIONAL BLOCKOUTS TO OBTAIN UP TO 36" DEPTH FOR ONE OR TWO POSTS IN A SECTION OF GUARDRAIL.
- DO NOT USE 16" OR 36" BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.

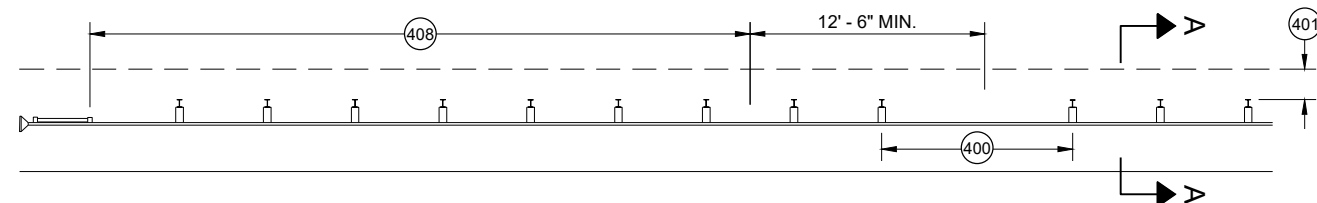
- 6 WHEN USING STEEL POST AD WOOD BLOCKOUTS, INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL

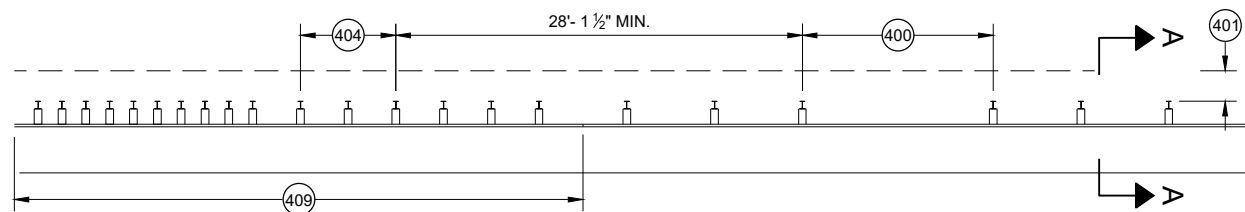
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



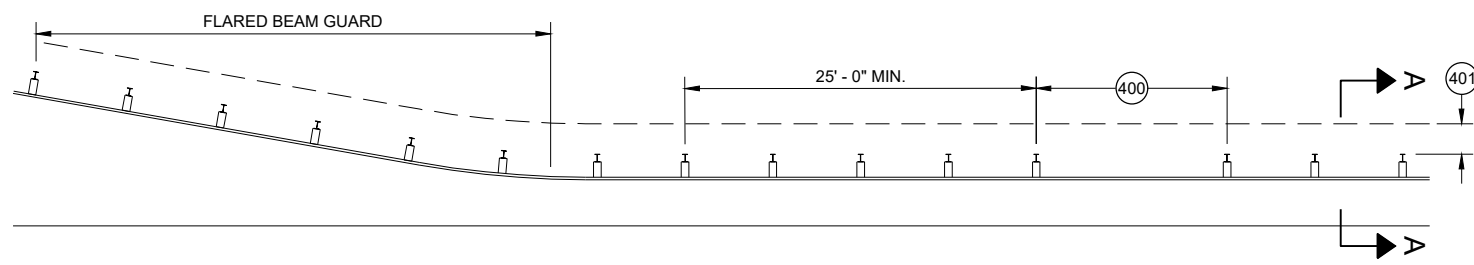
MISSING POST IN MGS GUARDRAIL



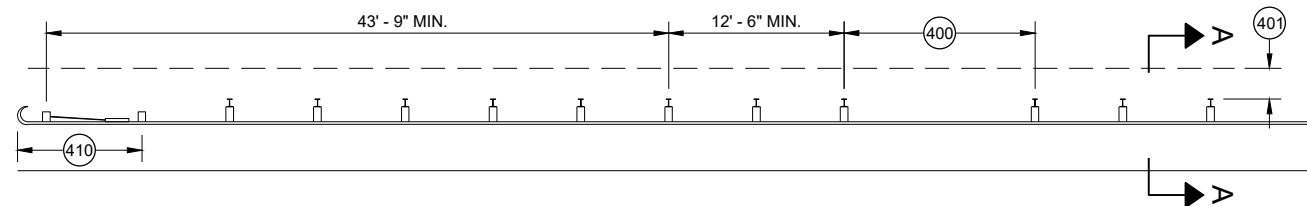
MISSING POST IN MGS GUARDRAIL NEAR EAT



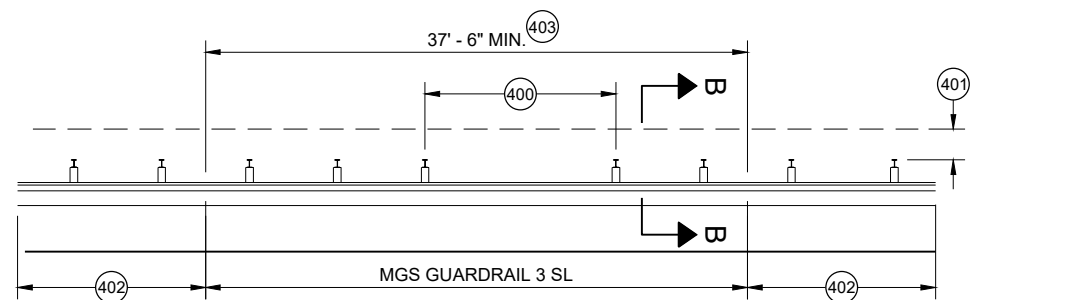
MISSING POST IN MGS GUARDRAIL NEAR AN APPROACH TRANSITION



MISSING POST IN MGS GUARDRAIL NEAR FLARED BEAM GUARD

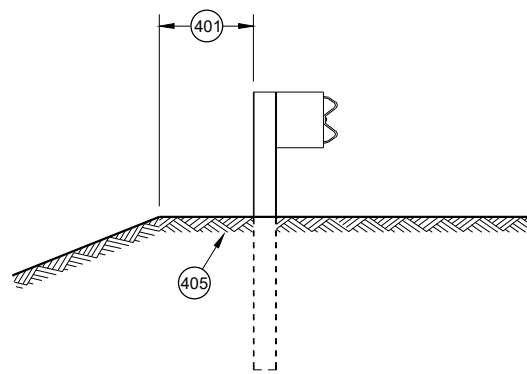


MISSING POST IN MGS GUARDRAIL NEAR A TYPE 2 END TERMINAL

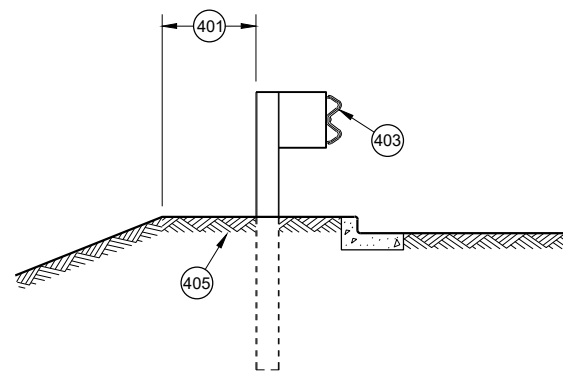


MISSING POST IN SHORT SPAN MGS GUARDRAIL NEAR CURB (SL)

- (400) MAX SPAN 12' - 6"
- (401) 2' MIN.
- (402) MGS GUARDRAIL 3
- (403) NESTING BEAM GUARD
- (404) ASYMMETRIC TRANSITION
- (405) SOIL WELL DRAINED AND COMPACTED
- (406) SEE OTHER DRAWINGS IN THIS SDD
- (407) SEE OTHER DRAWINGS FOR MIN. SPACING BETWEEN SPANS
- (408) SEE SDD 14B44
- (409) SEE SDD 14B45
- (410) SEE SDD 14B47
- (411) MINIMUM DISTANCE BETWEEN MISSING POST SPANS.



SECTION A - A



SECTION B - B

**MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2021
DATE
/S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR

FHWA

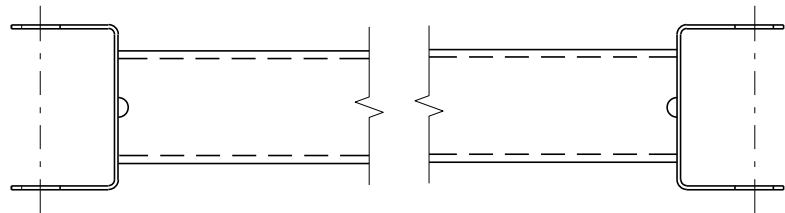
- (A) THE SLOPE IN THE AREA BOUNDED BY THE GRADELINE, THE HINGE POINT LINE (HPL) AND THE CLEAR ZONE LIMITS (CZL) SHALL BE 4:1 OR FLATTER.
- (B) AFTER FINAL ASSEMBLY, RECHECK CABLE TO BE SURE IT IS TAUT AND HAS NOT RELAXED
- (C) DIFFERENT MANUFACTURERS REQUIRE DIFFERENT PERFORATED W - BEAM RAIL END PANELS. SEE MANUFACTURER'S INFORMATION.
- (D) ATTACH ALUMINUM SHEET TO E.A.T. HEAD USING 4 STAINLESS STEEL SELF - TAPPING SCREWS. ONE SCREW PER CORNER.
- (E) HARDWARE MAY VARY BETWEEN MANUFACTURER. SEE MANUFACTURER'S DRAWING FOR INFORMATION.

DIMENSIONS MAY VARY, MANUFACTURER'S INFORMATION.

THE CENTER OF THE UPPER 3 1/2" DIAMETER HOLE ON POST NUMBER 3 THROUGH POST 9 IS TO BE FLUSH WITH THE GROUND LINE UP TO A MAXIMUM OF 2" ABOVE GROUND LINE. WOOD BLOCKS ON POSTS NUMBERED 3 THROUGH 9 MAY BE ADJUSTED UP TO 3" ABOVE THE TOP OF POST.

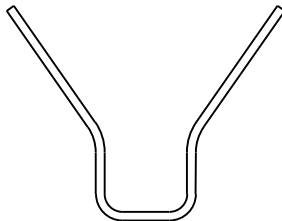
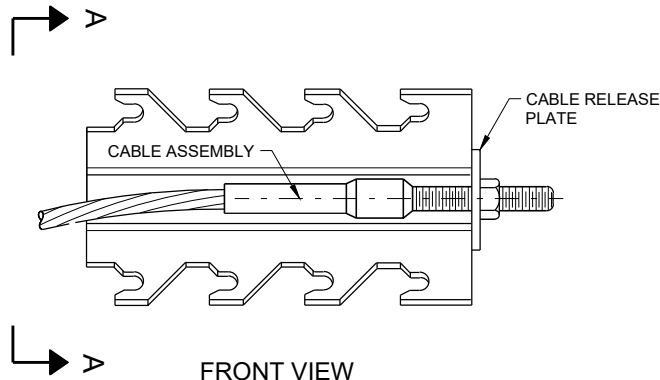


STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

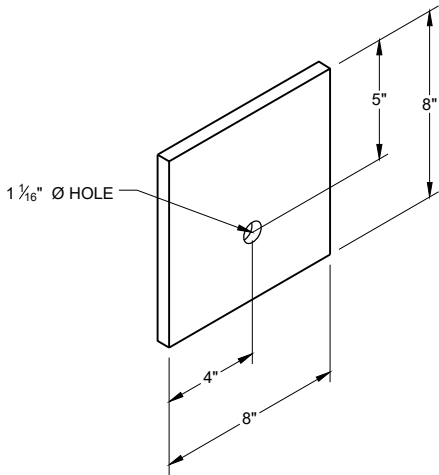


GENERIC GROUND STRUT⁹ ^E

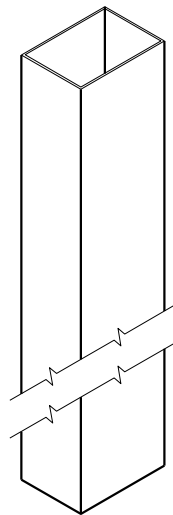
BILL OF MATERIALS	
PART NO.	DESCRIPTION MATERIALS PROVIDED BY MGS EAT MANUFACTURER. SEE MANUFACTURER'S DETAILS FOR MORE INFORMATION.
①	UPPER POST NO. 1 6" X 6" TUBE
②	LOWER POST NO. 1
③	WOOD CRT
④	WOOD BLOCKOUT
⑤	PIPE SLEEVE
⑥	BEARING PLATE
⑦	BCT CABLE ASSEMBLY
⑧	ANCHOR CABLE BOX
⑨	GROUND STRUT
⑩	PERFORATED W-BEAM RAIL END PANEL, 12'-6" LONG.
⑪	STANDARD W-BEAM RAIL. MULTIPLE SECTIONS REQUIRED. SECTIONS VARY IN LENGTH.
⑫	IMPACT HEAD
⑬	EAT MARKER POST - YELLOW (SEE APPROVED PRODUCTS LIST)
⑭	SOIL PLATE
⑮	UPPER POST NO. 2
⑯	LOWER POST NO. 2



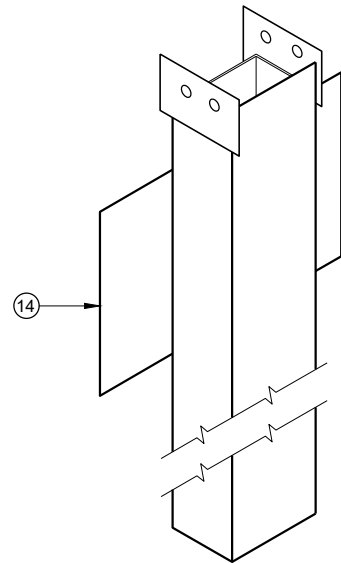
GENERIC ANCHOR CABLE BOX⁹ ^E



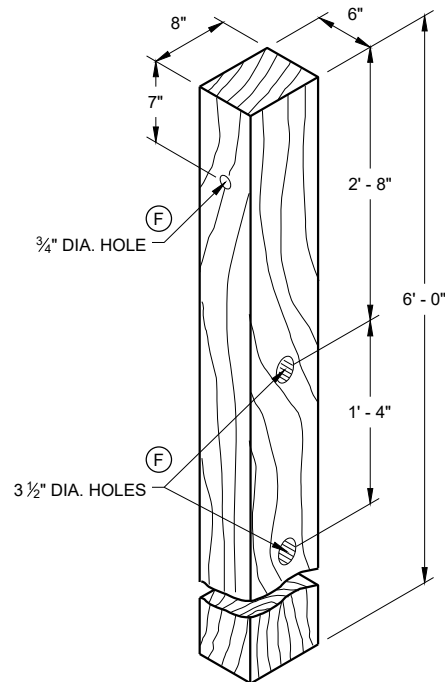
BEARING PLATE⁶ ^E



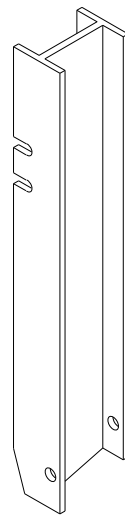
UPPER POST NO. 1 ⁽¹⁾ (E)



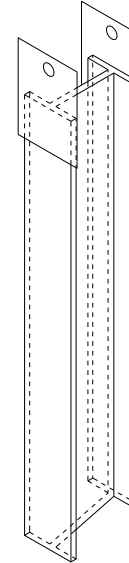
LOWER POST NO. 1 ⁽²⁾ (E)



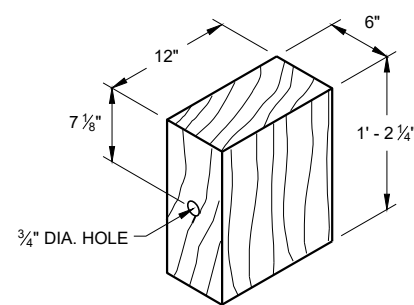
WOOD CRT POST ⁽³⁾ (E)
POSTS NUMBER 3-9



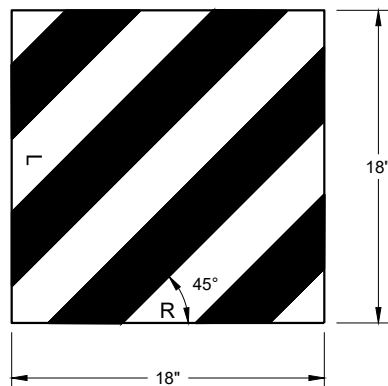
UPPER POST NO. 2 ⁽¹⁵⁾ (E)



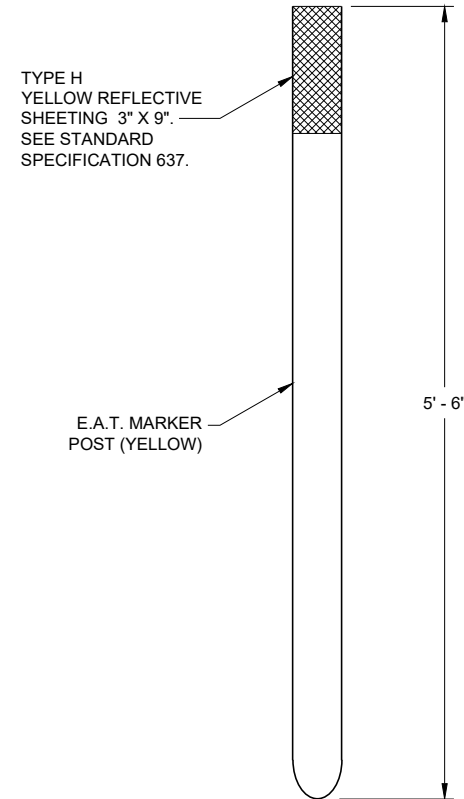
LOWER POST NO. 2 ⁽¹⁶⁾ (E)



WOOD BLOCKOUT ⁽⁴⁾
REQ'D. AT ALL POSTS EXCEPT POST NO'S 1 & 2



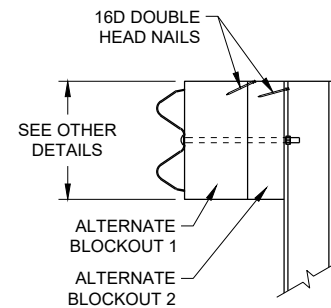
REFLECTIVE SHEETING DETAIL ^(E)



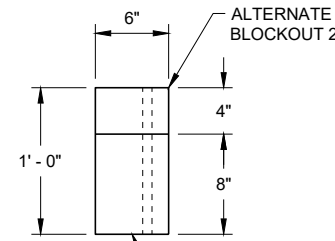
FRONT VIEW

SIDE VIEW

E.A.T. MARKER POST ⁽¹³⁾



SIDE VIEW



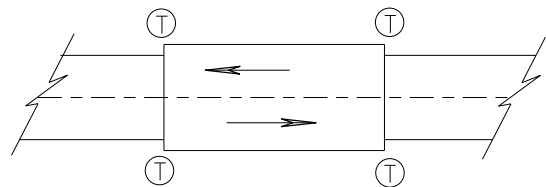
TOP VIEW

ALTERNATE WOOD
BLOCKOUT DETAIL

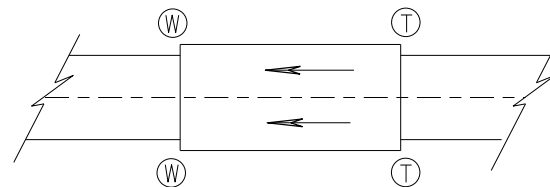
**MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL
(MGS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018 DATE /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR
FHWA



TWO WAY TRAFFIC



ONE WAY TRAFFIC

(T) THRIE BEAM CONNECTION

(W) W-BEAM CONNECTION WHEN REQUIRED

TYPICAL LOCATIONS OF THRIE BEAM AND W-BEAM CONNECTIONS TO BRIDGE

GENERAL NOTES

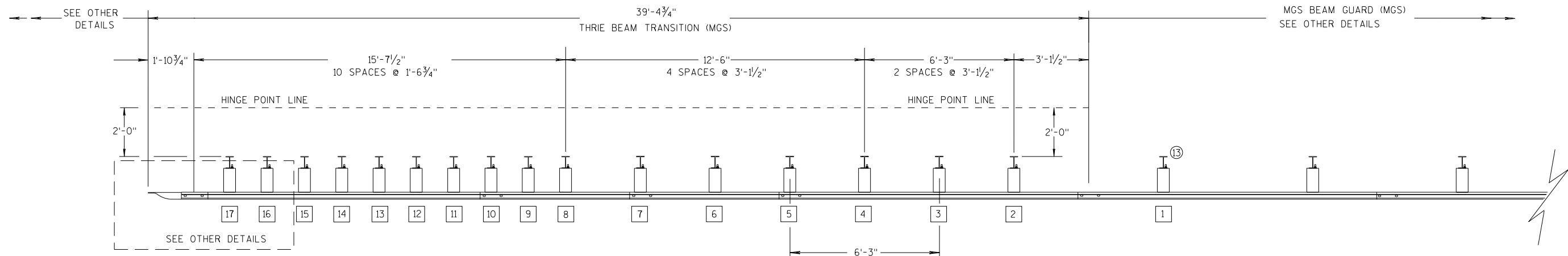
IF ROCK IS ENCOUNTERED, REMOVE ROCK TO FULL DEPTH OF POST PLUS 2 1/2", AND 12" DIAMETER AROUND POST. SEE 14B42 FOR MORE DETAILS.

TRANSITION USES STEEL POSTS ONLY.

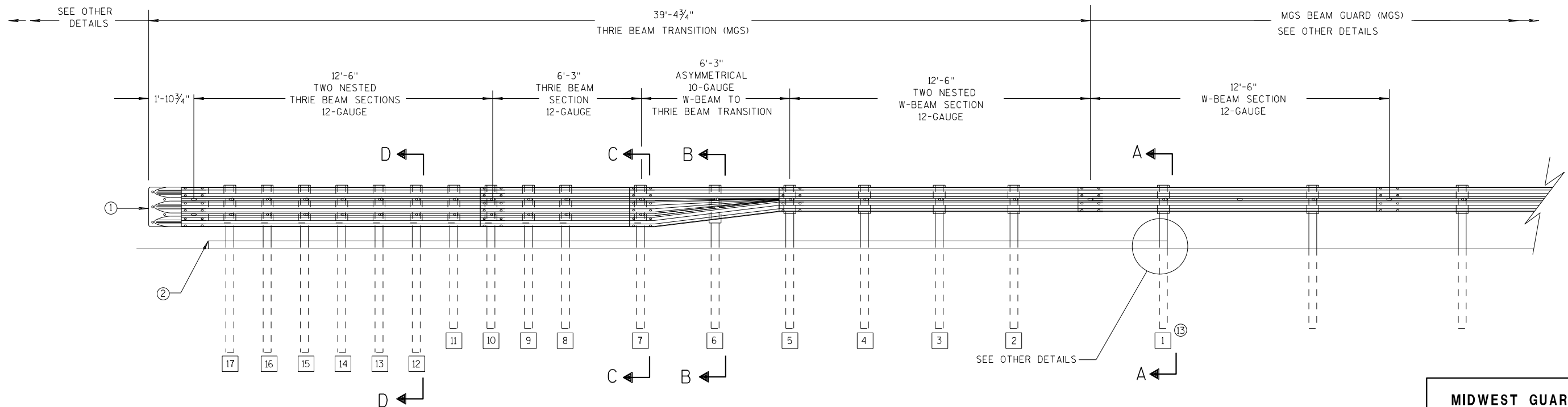
SEE STANDARD DETAIL DRAWING 14 B 42 FOR MORE INFORMATION.

POST 2 THROUGH 17 USES STEEL POST ONLY

- ① BRIDGE RAILING TYPE "W" DOES NOT REQUIRE A TERMINAL CONNECTOR.
- ② OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.
- ⑬ STEEL OR WOOD POST IS ACCEPTABLE AT POST 1. SEE SDD14B42



PLAN VIEW



ELEVATION VIEW

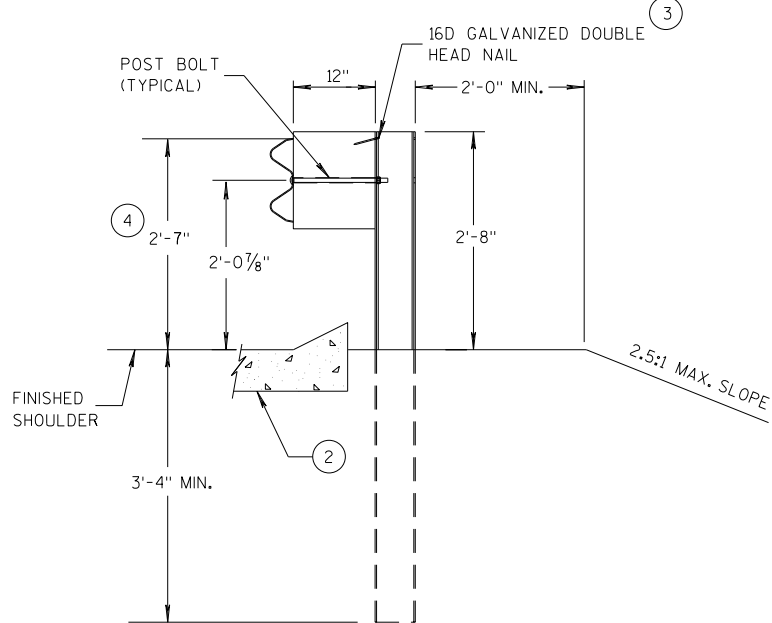
MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

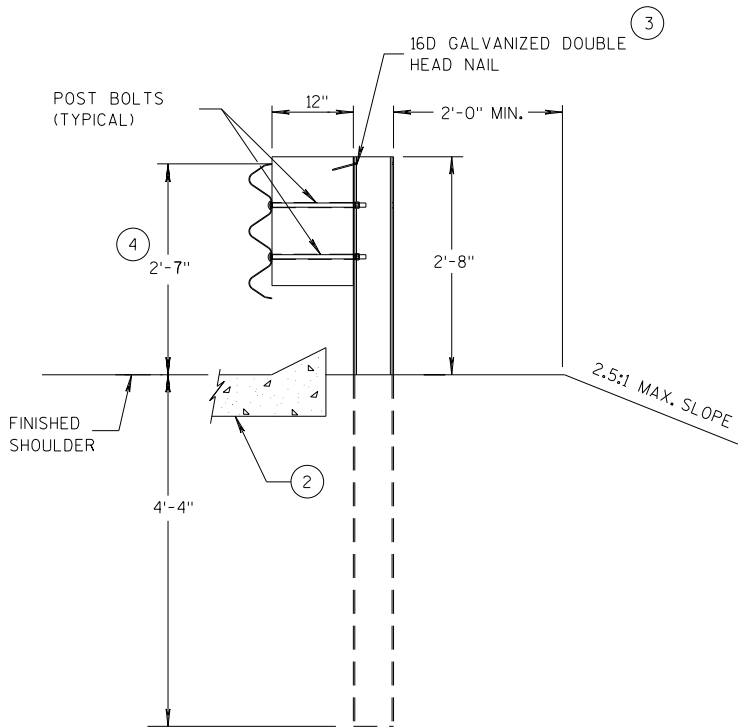
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

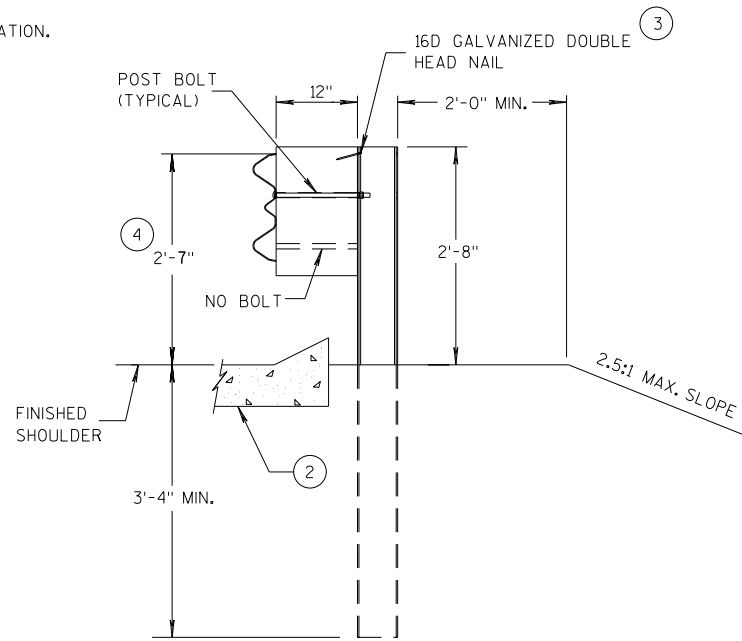
- 2 OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.
- 3 WHEN USING STEEL POSTS AND WOOD BLOCKOUTS INSTALL FOUR 10D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.
- 4 TOLERANCE FOR TOP OF W-BEAM RAIL IS $\pm 1"$.
- 13 STEEL OR WOOD POST IS ACCEPTABLE AT POST 1. SEE SDD 14B42



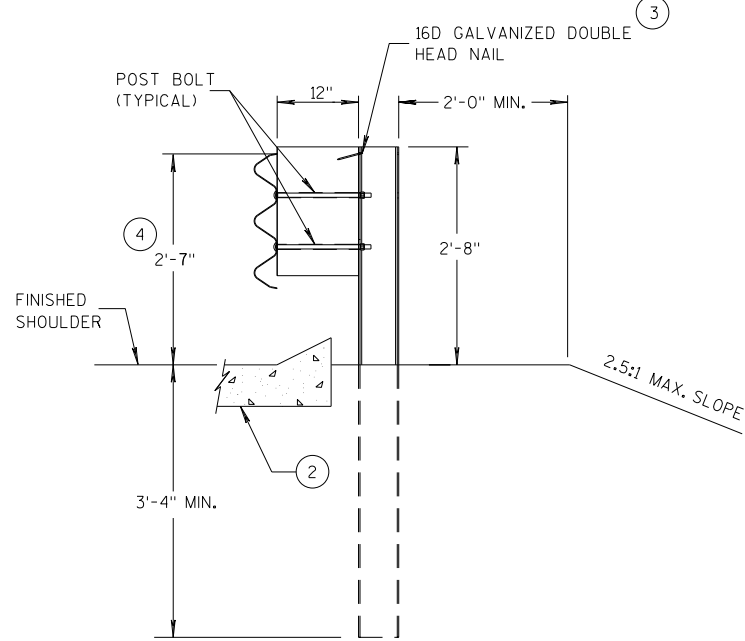
SECTION A-A
POSTS 1-5



SECTION D-D
POSTS 12-17



SECTION B-B
POST 6



SECTION C-C
POSTS 7-11

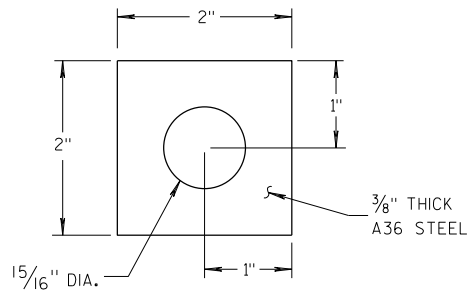
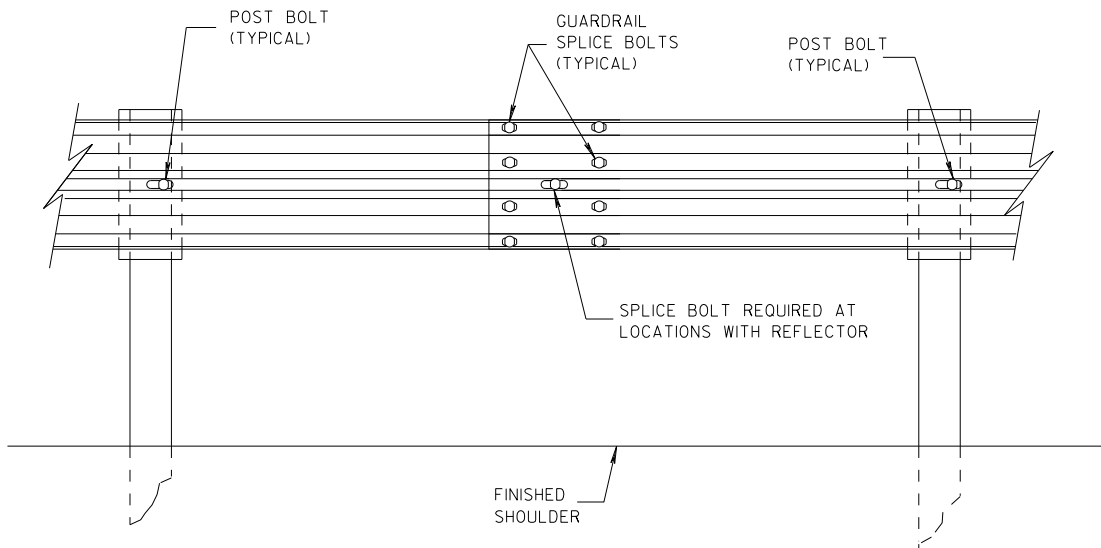
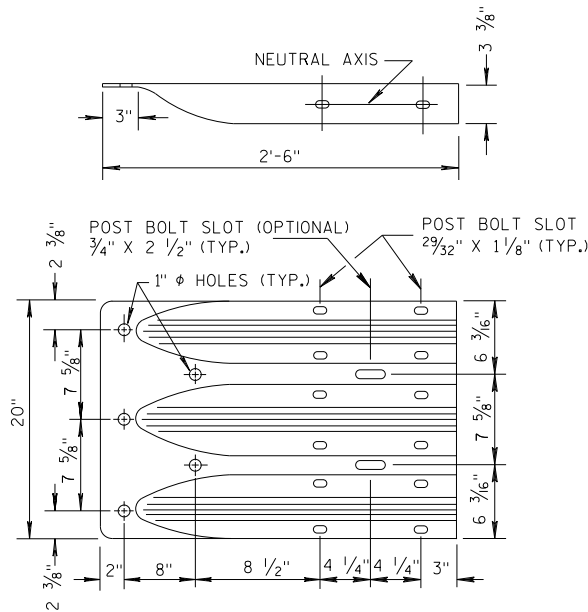


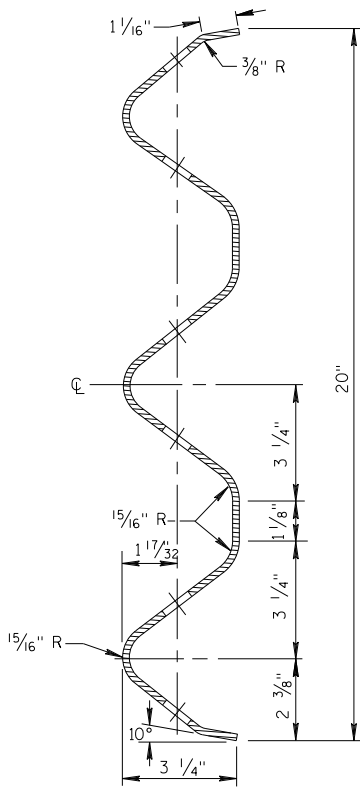
PLATE WASHER DETAIL



SPLICE DETAIL



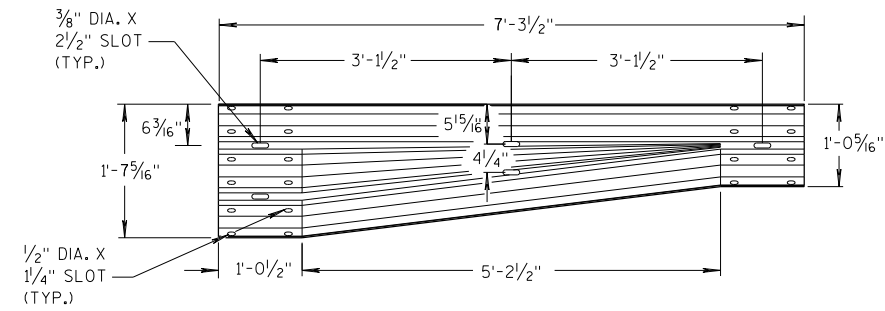
THRIE BEAM
TERMINAL CONNECTOR



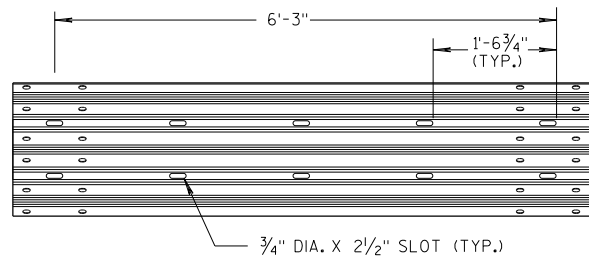
SECTION THRU THRIE
BEAM RAIL ELEMENT

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

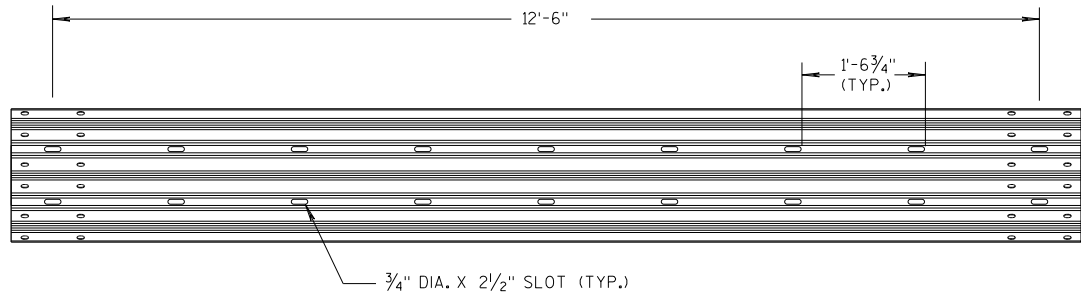
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



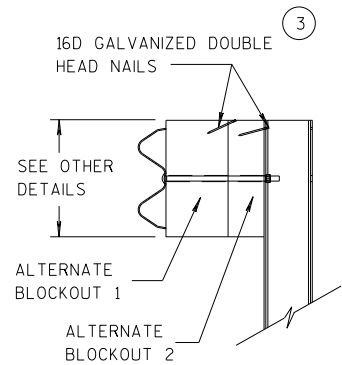
W-BEAM TO THRIE BEAM TRANSITION SECTION



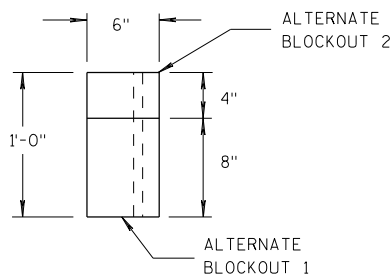
6'-3" THRIE BEAM SECTION



12'-6" THRIE BEAM SECTION

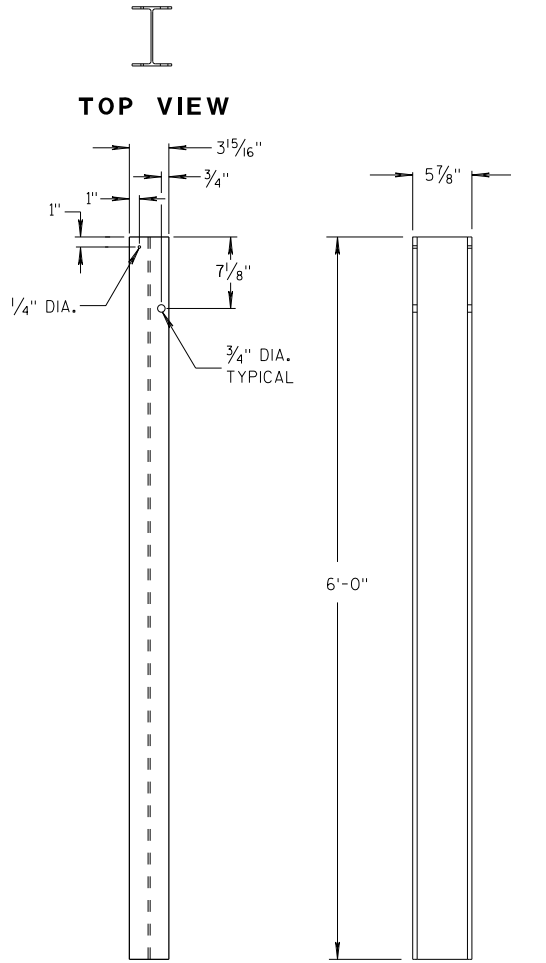


SIDE VIEW



TOP VIEW

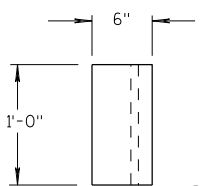
ALTERNATE WOOD BLOCKOUT DETAIL



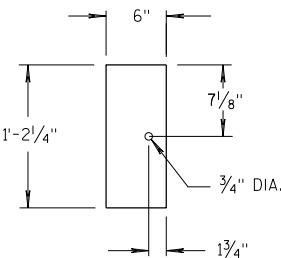
FRONT VIEW

SIDE VIEW

STEEL POSTS 1-5

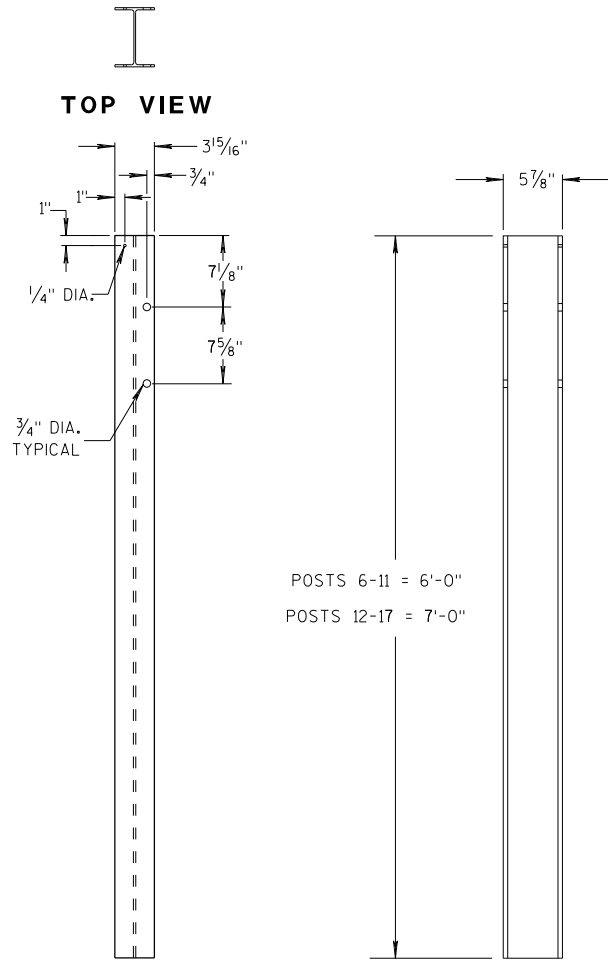


TOP VIEW



FRONT VIEW

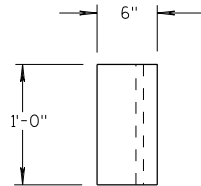
**BLOCKOUT
POSTS 1-5**



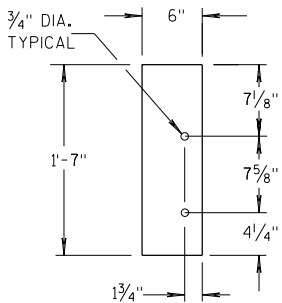
FRONT VIEW

SIDE VIEW

STEEL POSTS 6-17



TOP VIEW



FRONT VIEW

**BLOCKOUT
POSTS 6-17**

GENERAL NOTES

STEEL POSTS ARE W6X9 OR W6X8.5.

BOLT HOLES FOR POST ARE ON FRONT AND OF SIDE OF POST.

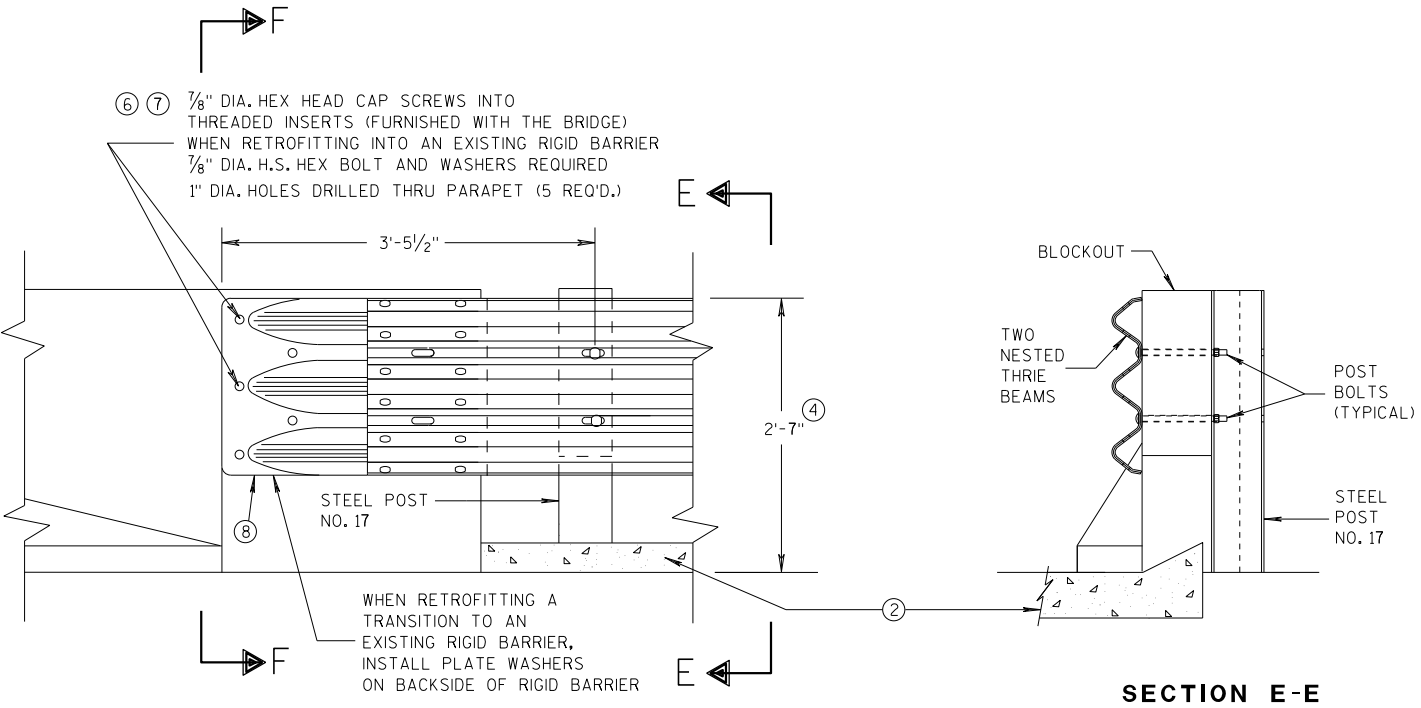
③ WHEN USING STEEL POSTS AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

⑤ WOOD BLOCKS MAY BE CONSTRUCTED OUT OF 2 WOOD BLOCKS. SEE ALTERNATE WOOD BLOCK DETAIL.

⑬ STEEL OR WOOD POST IS ACCEPTABLE AT POST 1. SEE SDD 14B42.

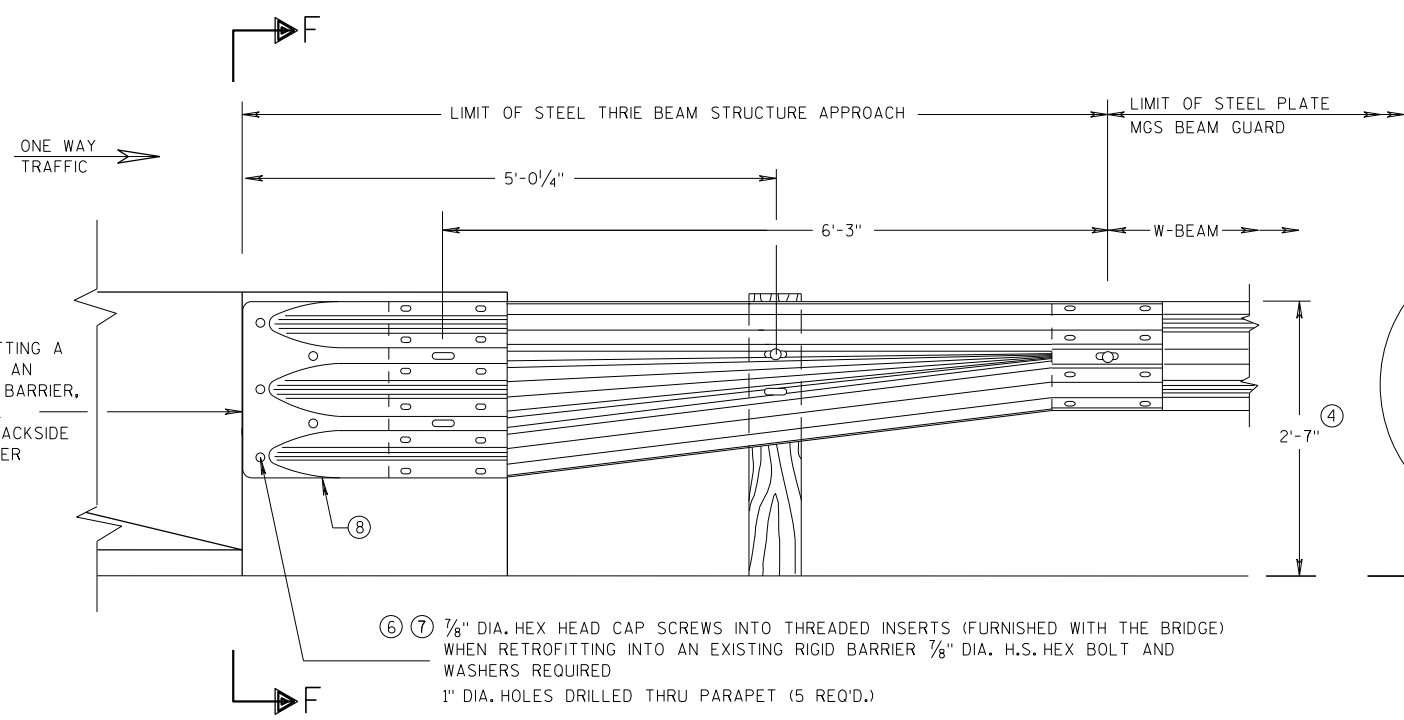
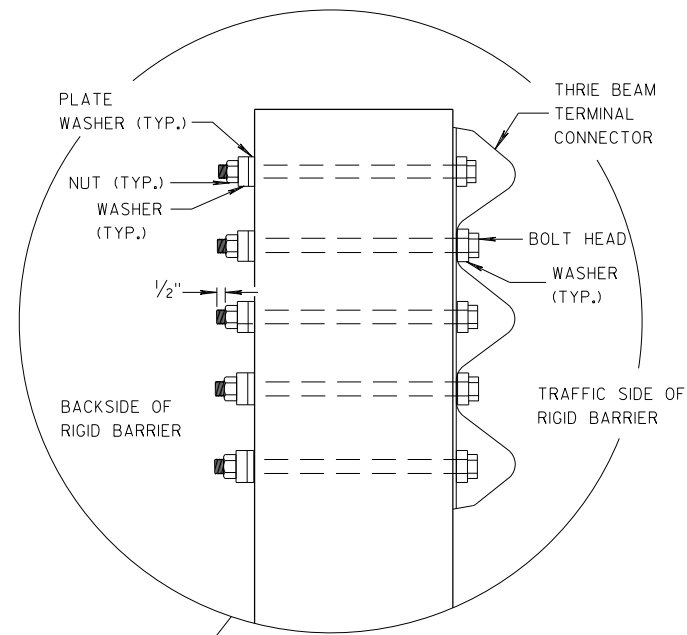
**MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)**

STATE OF WISCONSIN
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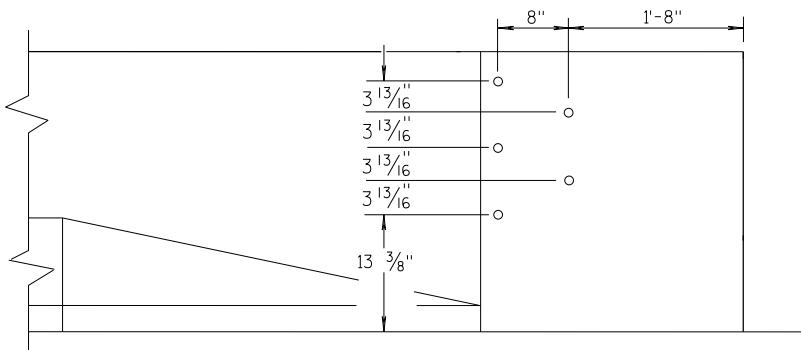


GENERAL NOTES

- THESE ARE TYPICAL CONNECTION DETAILS. ADJUST THE POSTION OF CONNECTIONS TO EXISTING BRIDGES TO FIT THE ACTUAL BRIDGE AND SITE DIMENSIONS.
- ② OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.
- ④ TOLERANCE FOR TOP OF BEAM IS $\pm 1"$.
- ⑥ DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.
- ⑦ BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X 5/32" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.
- ⑧ THE RECESS FOR A W-BEAM CONNECTION, WHICH EXISTS ON SOME PARAPETS OF THIS TYPE, SHALL BE FILLED WITH A TREATED TIMBER BLOCKOUT. BLOCKOUT SIZE IS 1'-6" X 2'-0" X 3 1/2".



SECTION F-F



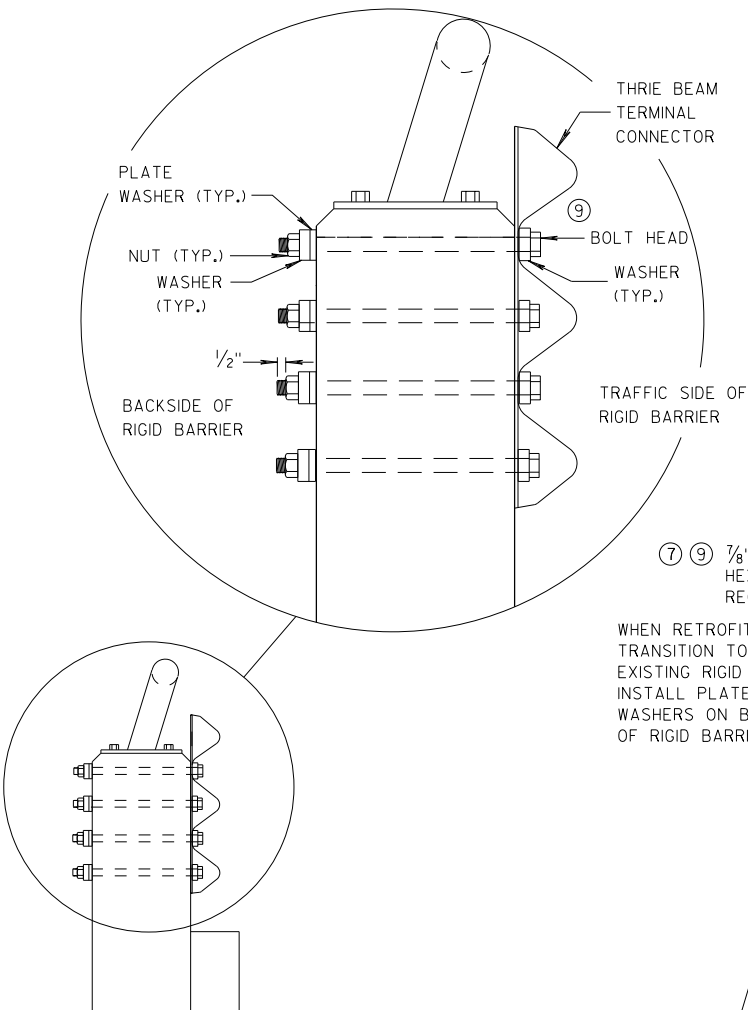
DRILL HOLE LOCATION

MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 07/2018 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	

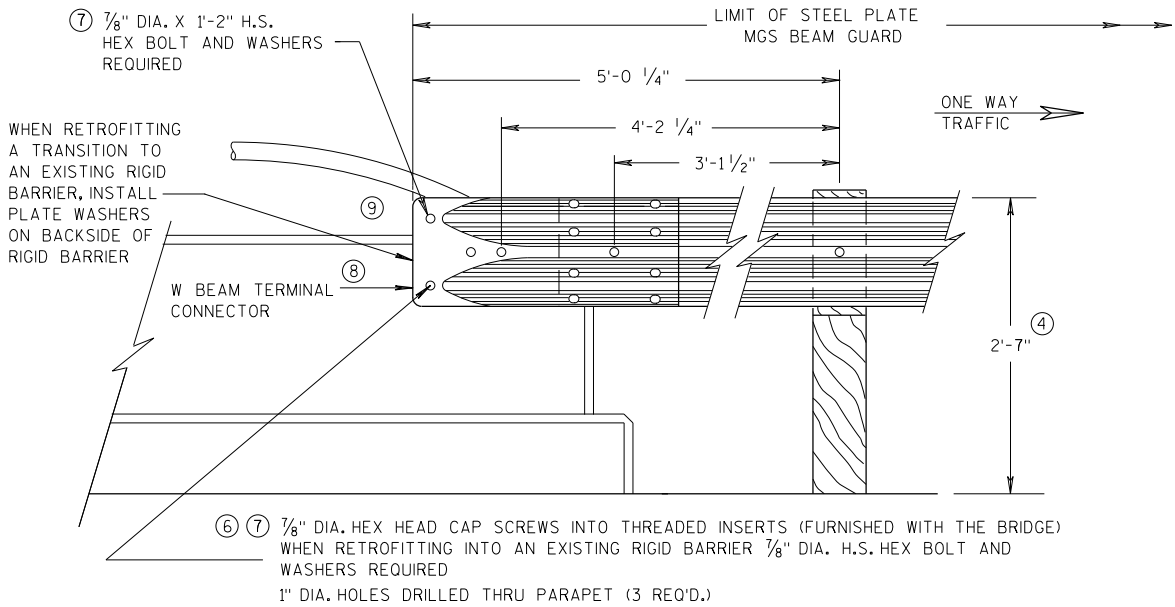
GENERAL NOTES

THESE ARE TYPICAL CONNECTION DETAILS. ADJUST THE POSTION OF CONNECTIONS TO EXISTING BRIDGES TO FIT THE ACTUAL BRIDGE AND SITE DIMENSIONS.

- ②
- OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.
- ④
- TOLERANCE FOR TOP OF BEAM IS $\pm 1"$.
- ⑥
- DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.
- ⑦
- BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X $\frac{5}{32}"$ THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.
- ⑧
- THE RECESS FOR A W-BEAM CONNECTION, WHICH EXISTS ON SOME PARAPETS OF THIS TYPE, SHALL BE FILLED WITH A TREATED TIMBER BLOCKOUT. BLOCKOUT SIZE IS 1'-6" X 2'-0" X 3 $\frac{1}{2}"$.
- ⑨
- BOLT, NUT AND WASHERS NOT REQUIRED FOR THIS LOCATION WHEN RETROFITTING AN EXISTING PAPAPET AND THE HOLE IS EITHER ABOVE PARAPET OR WITHIN 4 INCHES OF THE EDGE OF PARAPET.

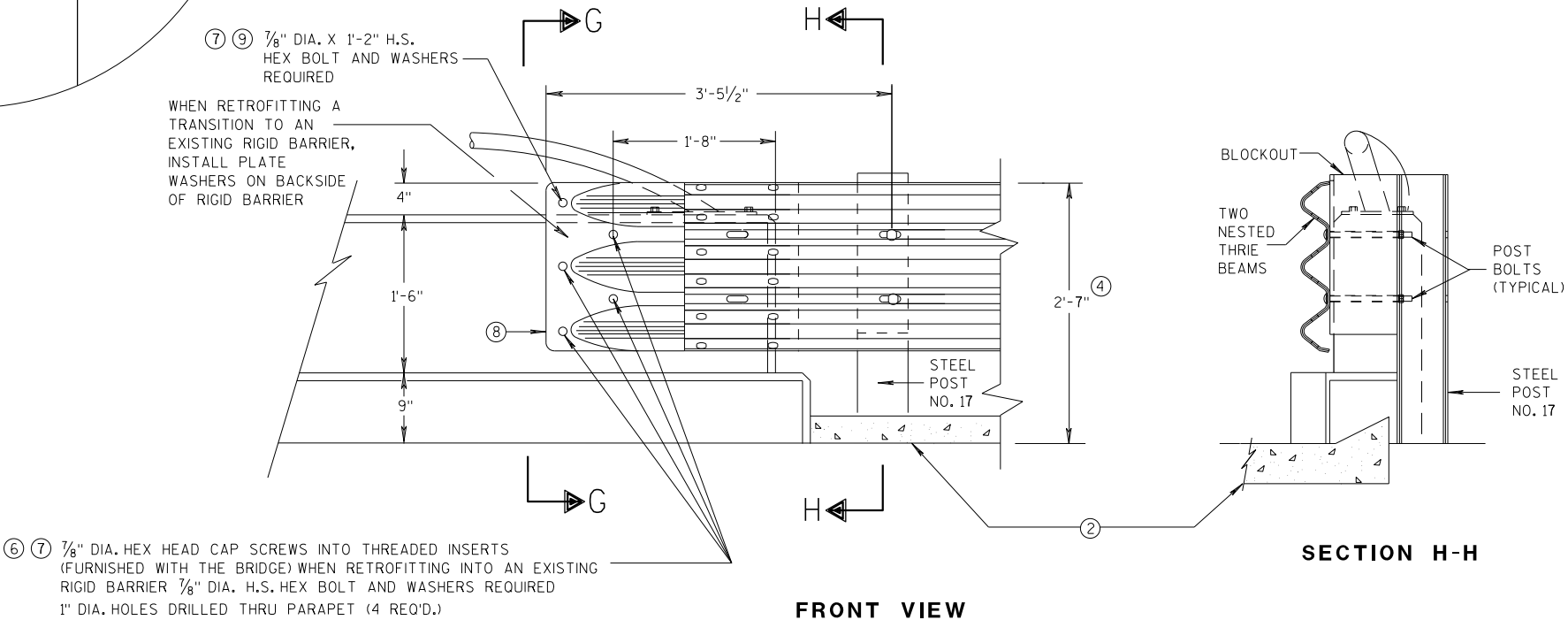


SECTION G-G



FRONT VIEW

W BEAM CONNECTION TO VERTICAL FACE PARAPET
(USE ONLY ON THE TRAFFIC EXIT END OF ONE WAY BRIDGES)



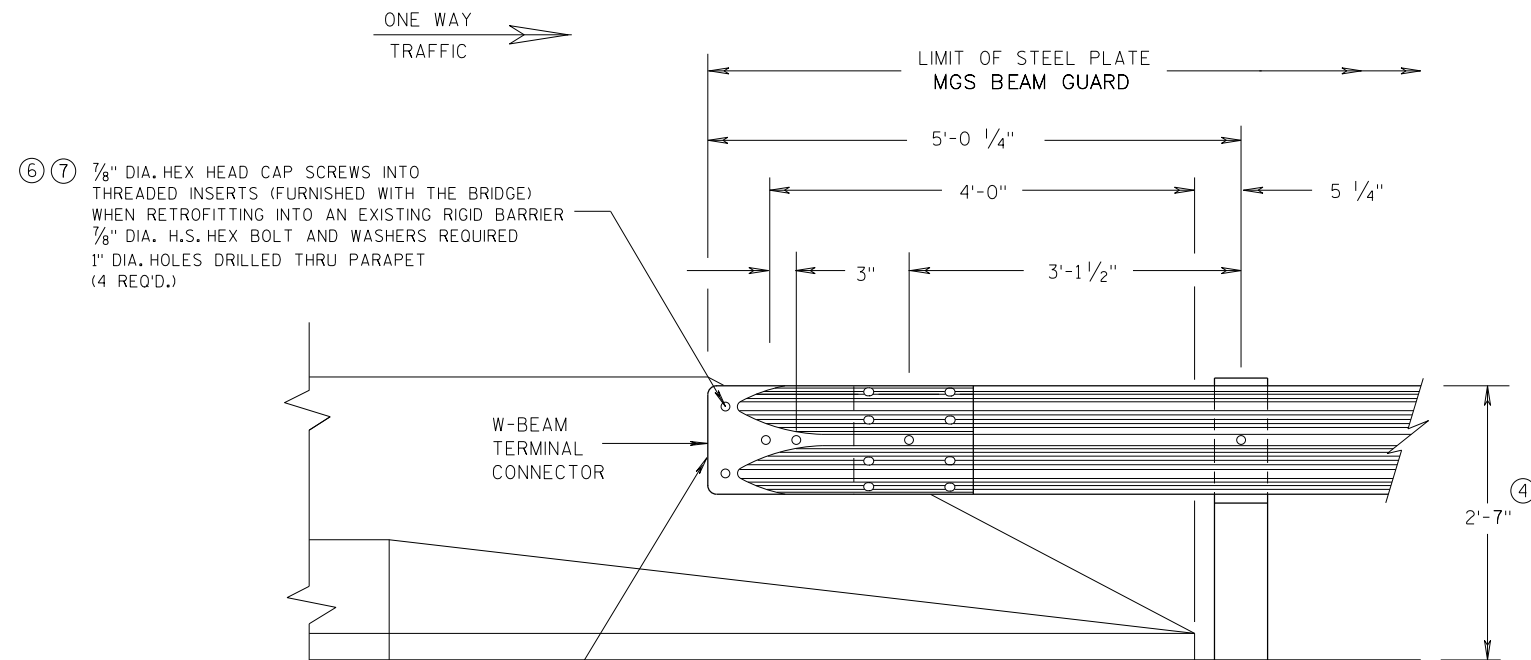
FRONT VIEW

THRIE BEAM CONNECTION TO VERTICAL FACED PARAPETS

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

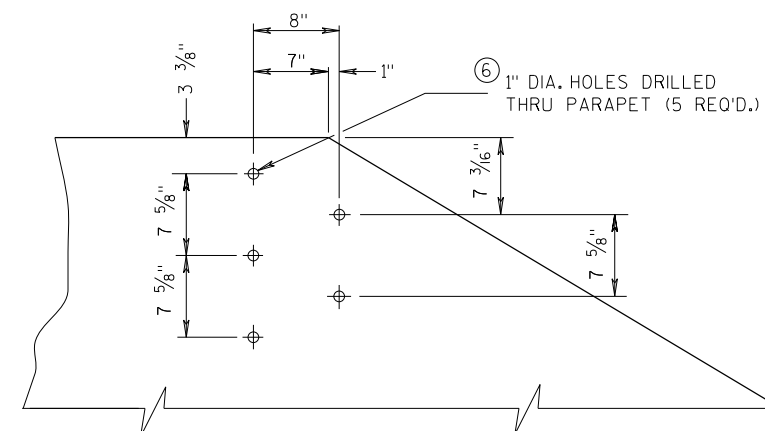
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UNIT SUPERVISOR

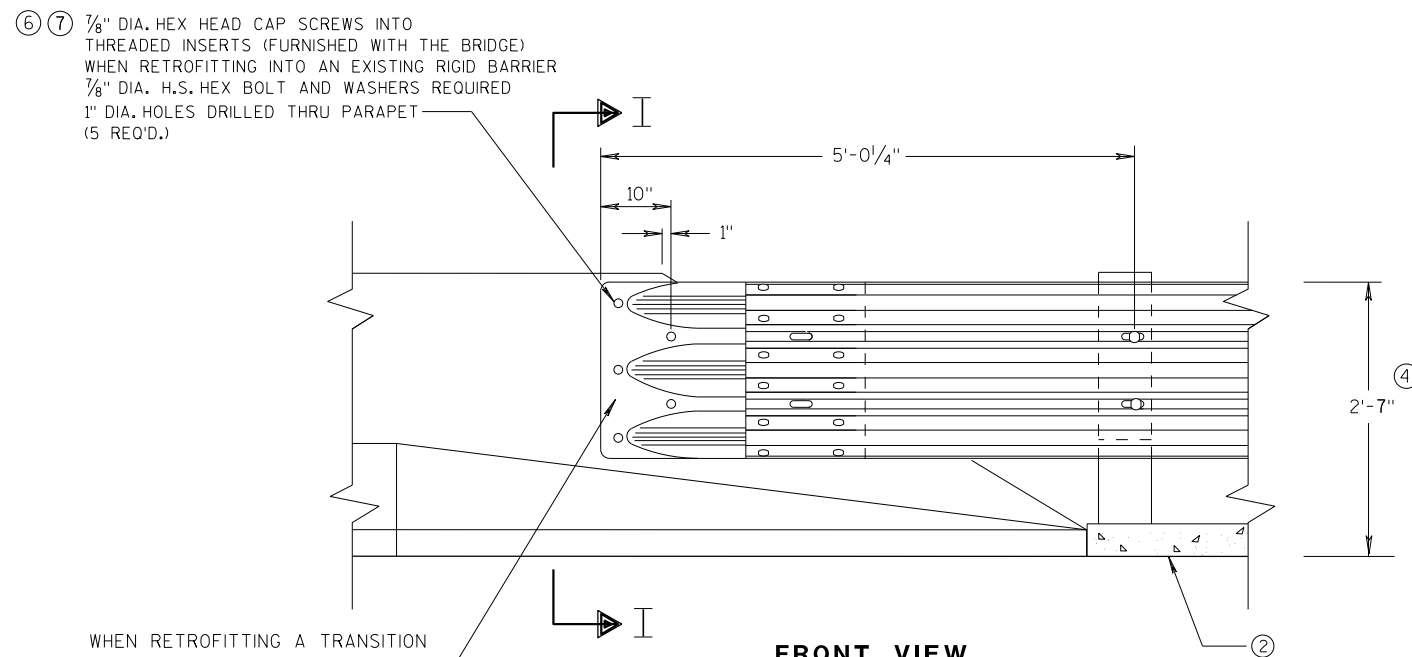


FRONT VIEW

**W BEAM CONNECTION TO
PARAPETS WITH SLOPED ENDS
(USE ONLY AT TRAFFIC EXIT END OF ONE WAY BRIDGE)**

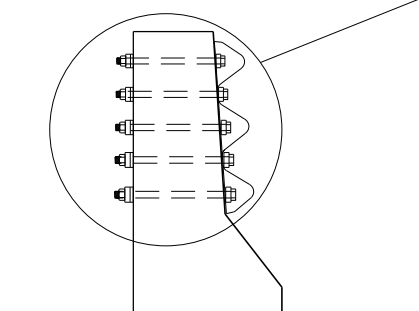


**DRILL HOLE LOCATION AND PATTERN
FOR THRIE BEAM CONNECTION**

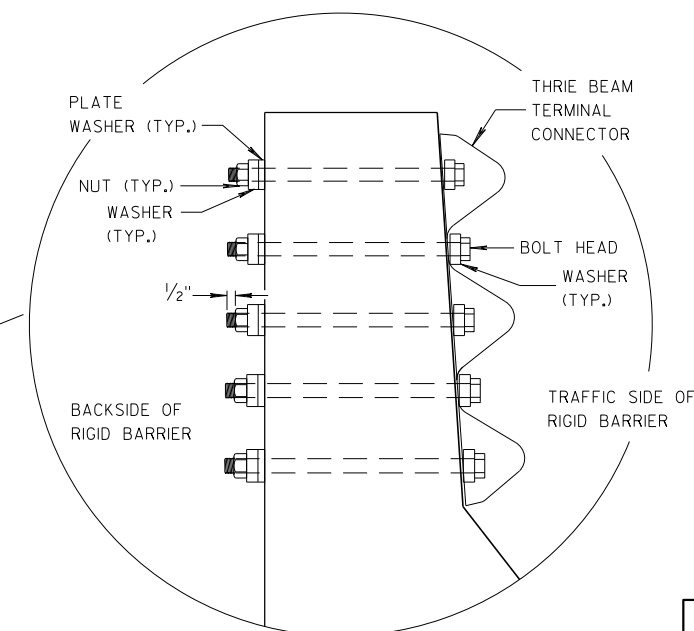


FRONT VIEW

**THRIE BEAM CONNECTION TO BRIDGE
PARAPETS WITH SLOPED ENDS**



SECTION I-I

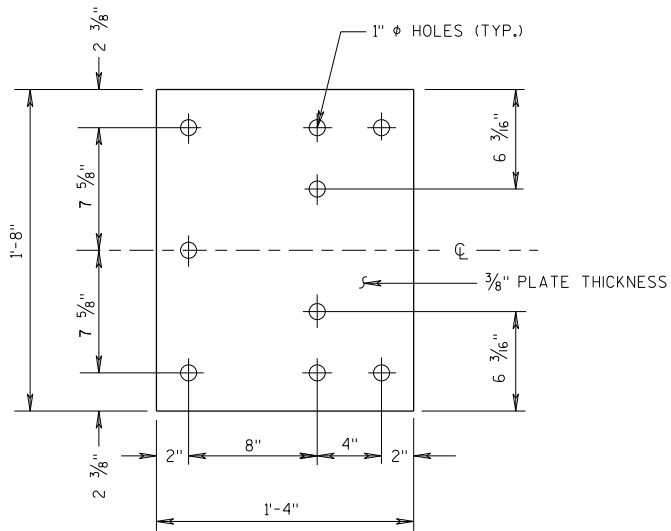
**GENERAL NOTES**

- ② OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.
- ④ TOLERANCE FOR TOP OF BEAM IS $\pm 1"$.
- ⑥ DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.
- ⑦ BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X 5/32" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.

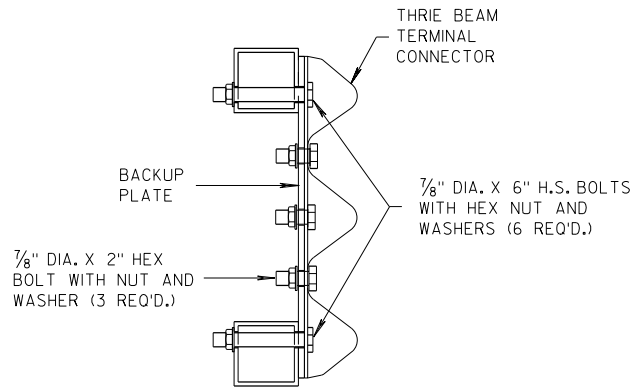
**MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

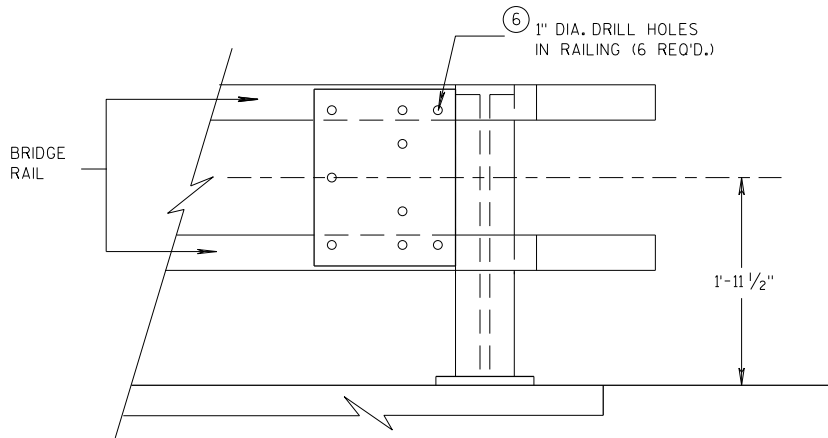
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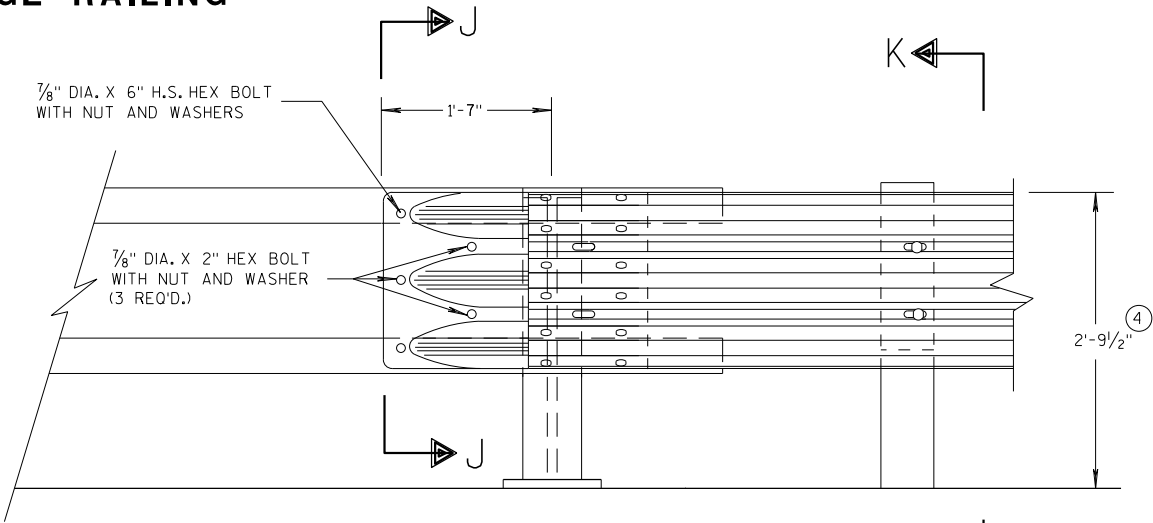
BACK-UP PLATE DETAIL



SECTION J-J

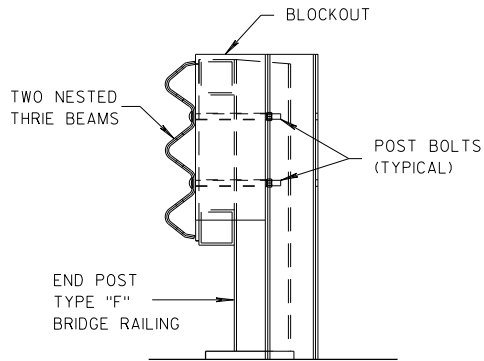


BACK-UP PLATE MOUNTING ONTO BRIDGE RAILING



FRONT VIEW

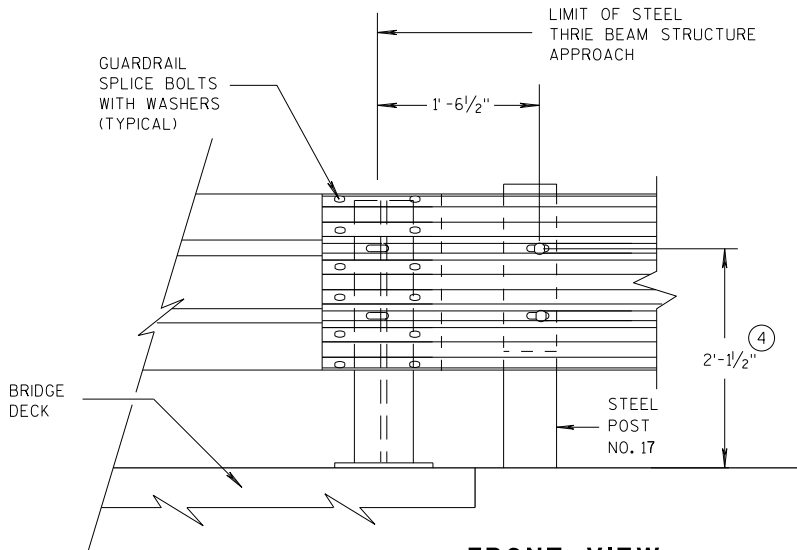
THRIE BEAM CONNECTION TO TUBULAR RAILING TYPE "F"



SECTION K-K

GENERAL NOTES

- ④ TOLERANCE FOR TOP OF BEAM IS $\pm 1"$.
- ⑥ DRILLING HOLES THROUGH THE PAPER, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.



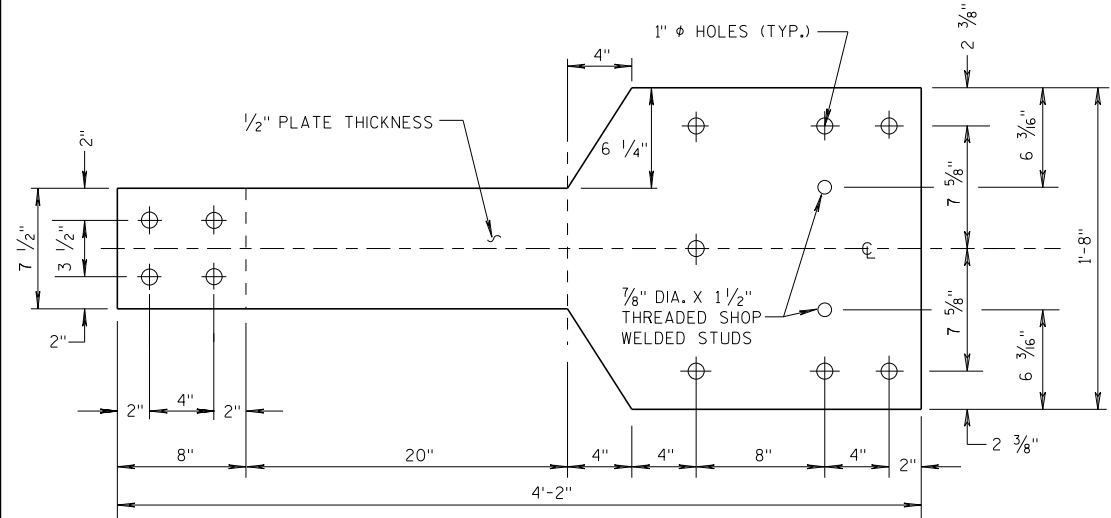
FRONT VIEW

THRIE BEAM CONNECTION TO STEEL RAILING TYPE "W"

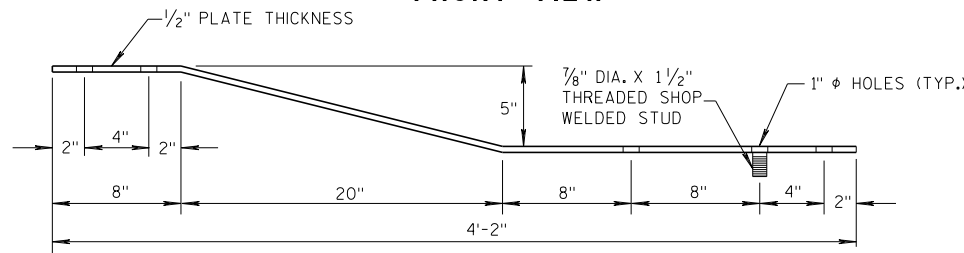
MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 07/2018 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	

GENERAL NOTES

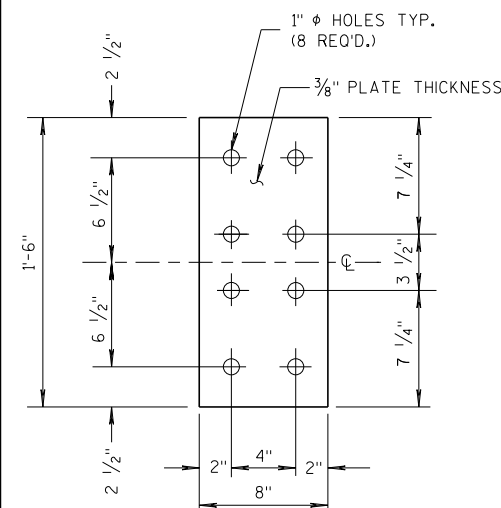
④ TOLERANCE FOR TOP OF W-BEAM RAIL IS $\pm 1"$.



FRONT VIEW

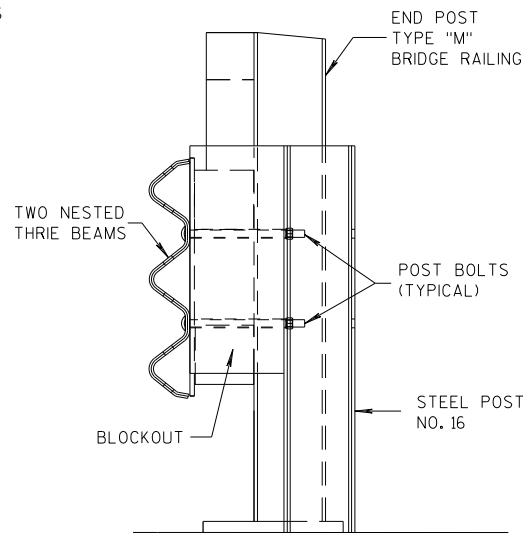


PLAN VIEW
BACK-UP PLATE DETAIL, TYPE "M"

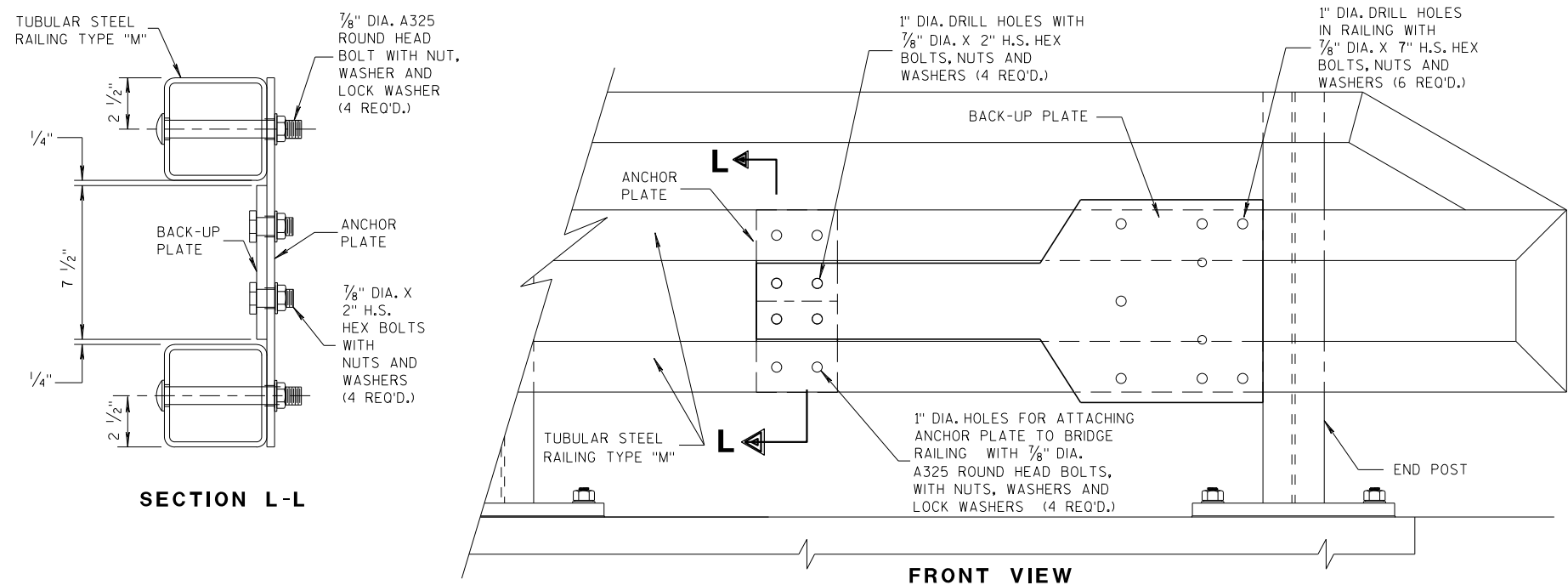


FRONT VIEW

ANCHOR
PLATE DETAIL,
TYPE "M"



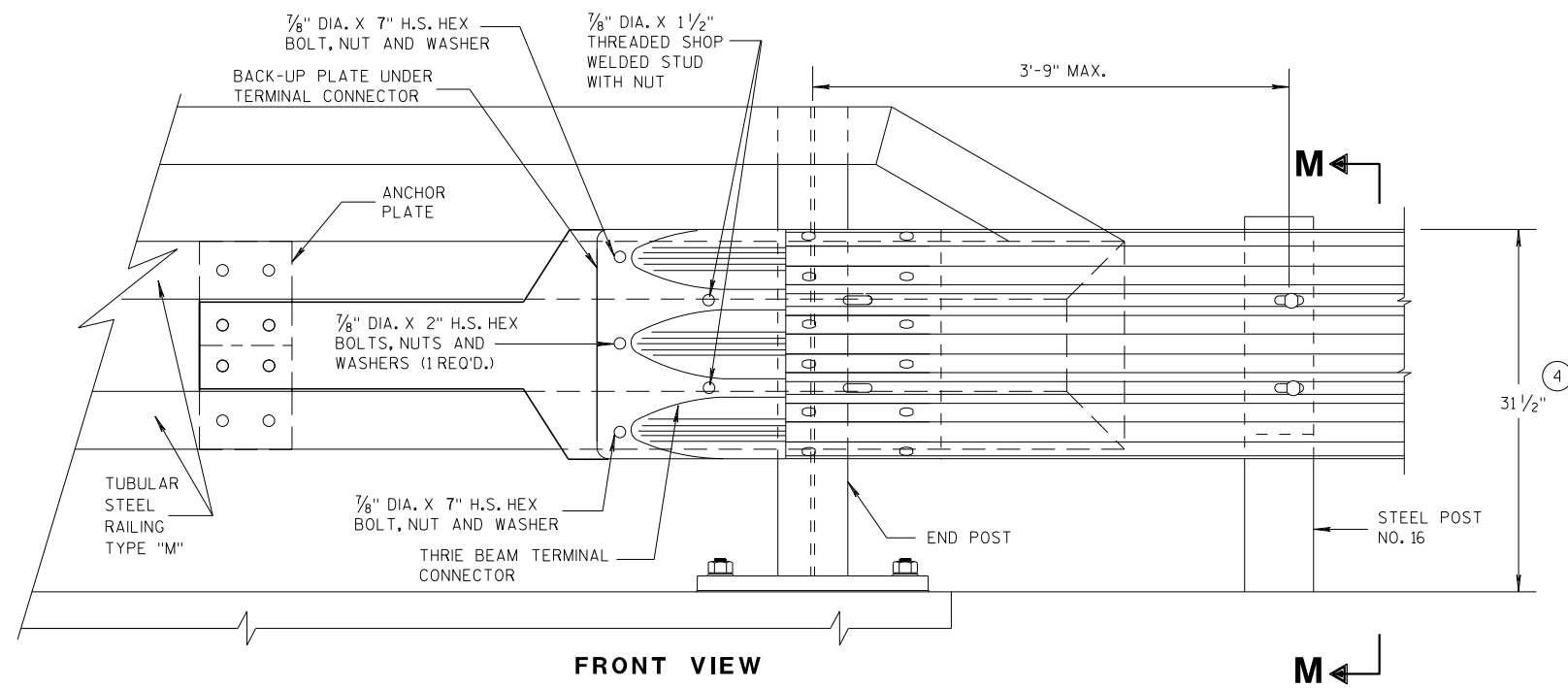
SECTION M-M



SECTION L-L

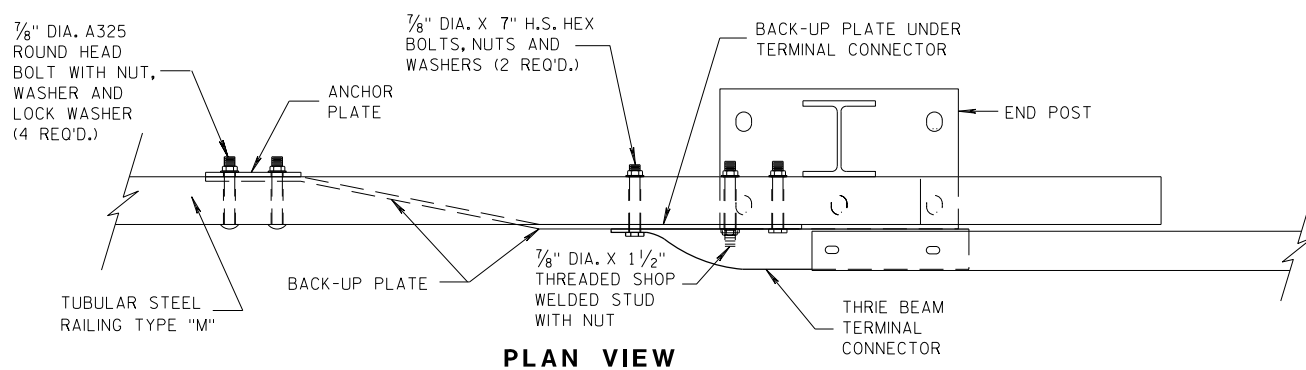
FRONT VIEW

ANCHOR AND BACK-UP PLATE MOUNTING TO BRIDGE RAILING, TYPE "M"



FRONT VIEW

M



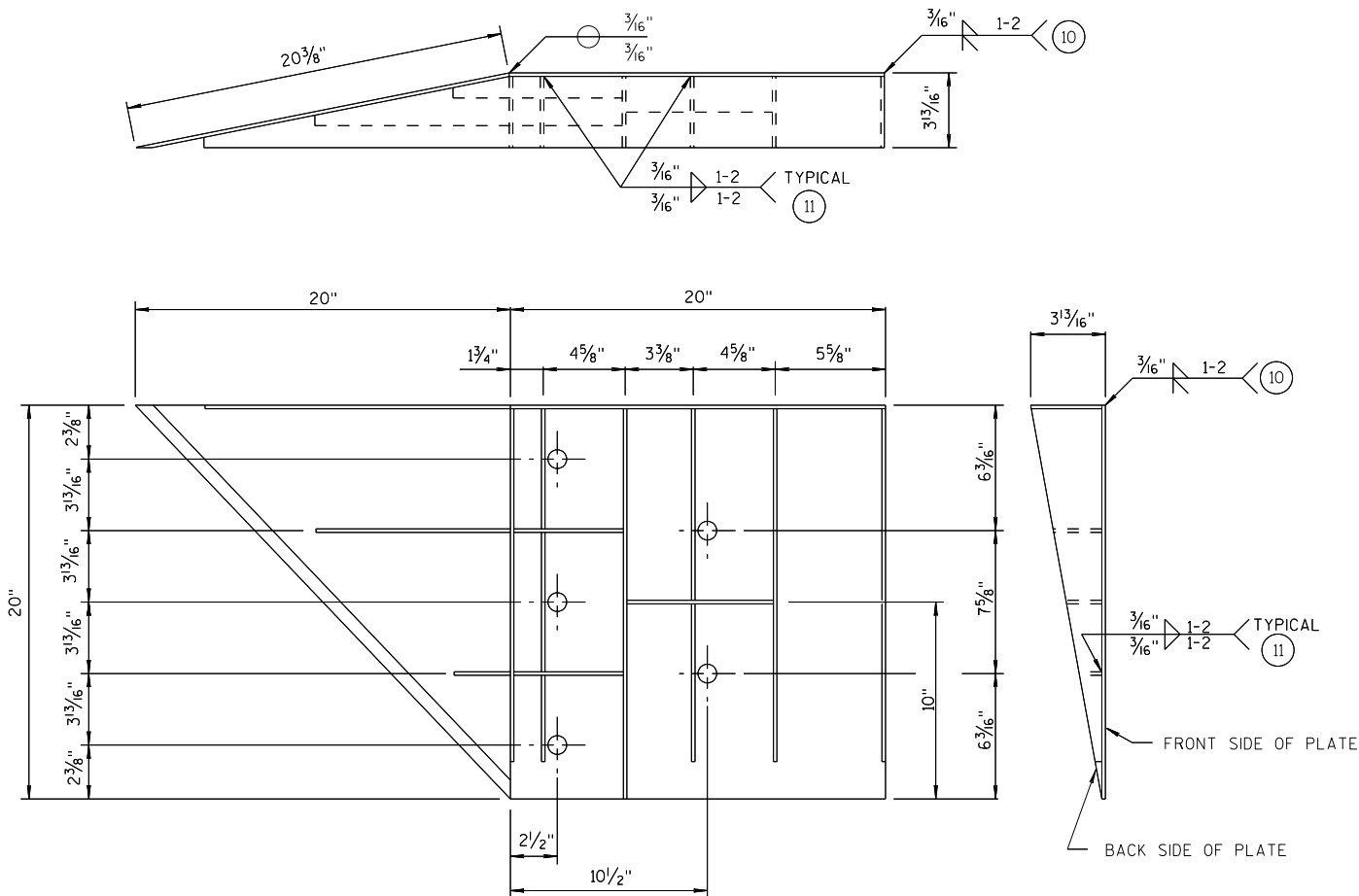
PLAN VIEW

THREE BEAM CONNECTION TO TUBULAR RAILING, TYPE "M"

MIDWEST GUARDRAIL SYSTEM
THREE BEAM TRANSITION (MGS)

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WELDING INSTRUCTION
(VIEWED FROM BACK SIDE OF PLATE)

CONNECTOR PLATE DIMENSION (PER ASSEMBLY)				
PLATE	QUANTITY	SHAPE	SIZE (A x B x C x D)	THICKNESS
P1	1		20" x 20"	3/16"
P2	1		20" x 20" x 28 3/16"	3/16"
P3	1		39" x 3 5/8" x 20" x 19 5/16"	3/16"
S1	4		18 7/16" x 3 5/8" x 18 3/4"	1/4"
S2	1		10 1/4" x 2 1/16" x 10 3/8" x 1/2"	1/4"
S3	1		3" x 1 1/16" x 3 3/8" x 1/2"	1/4"
S4	1		6 1/8" x 2 7/16"	1/4"
S5	1		6 1/8" x 1 1/16"	1/4"
S6	1		7 3/4" x 1 3/4"	1/4"
S7	1		2 3/16" x 6" x 3 5/8" x 5 7/8"	1/4"
S8	1		1 5/32" x 7 1/2" x 2 1/2" x 7 3/8"	1/4"
S9	1		6 1/16" x 6 3/16" x 1 3/32"	1/4"
S10	1		1 7/8" x 9 7/8" x 3 5/8" x 9 11/16"	1/4"
S11	1		8 1/2" x 8 3/4" x 1 3/16"	1/4"

SINGLE SLOPE CONNECTION PLATE

GENERAL NOTES

- COVER PLATE PANELS ARE 3/16" THICK.
- ALL STIFFENERS ARE 1/4" THICK.
- CONNECTOR PLATE SHALL BE FABRICATED FROM ASTM GRADE A36 STEEL AND GALVANIZED.
- FOR GALVANIZED REQUIREMENTS, SEE SECTION 614 OF THE STANDARD SPECIFICATIONS.
- ALL HOLE DIAMETERS SHALL BE 1".
- FOR OPPOSITE SIDE INSTALLATION MIRROR DRAWINGS.

- 10 STIFFENERS LOCATED AT THE OUTSIDE EDGES OF THE COVER PLATES SHALL BE WELDED AS FOLLOWS:
SINGLE BEVEL GROOVE WELD ON EXTERNAL SIDES AND 3/16" FILLET WELD BY 1" LONG SPACED AT 2" ON INTERNAL SIDES.
- 11 STIFFENERS LOCATED ON THE INSIDE OF THE COVER PLATE SHALL BE WELDED AS FOLLOWS:
3/16" FILLET WELD BY 1" LONG SPACED AT 2".

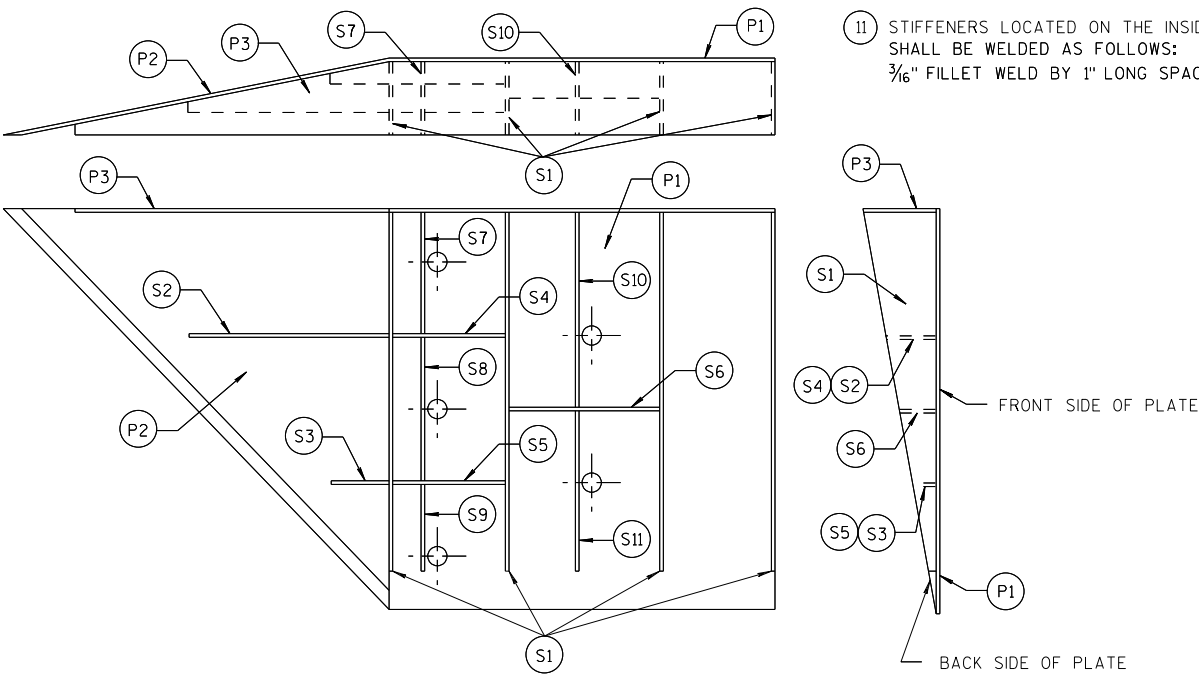
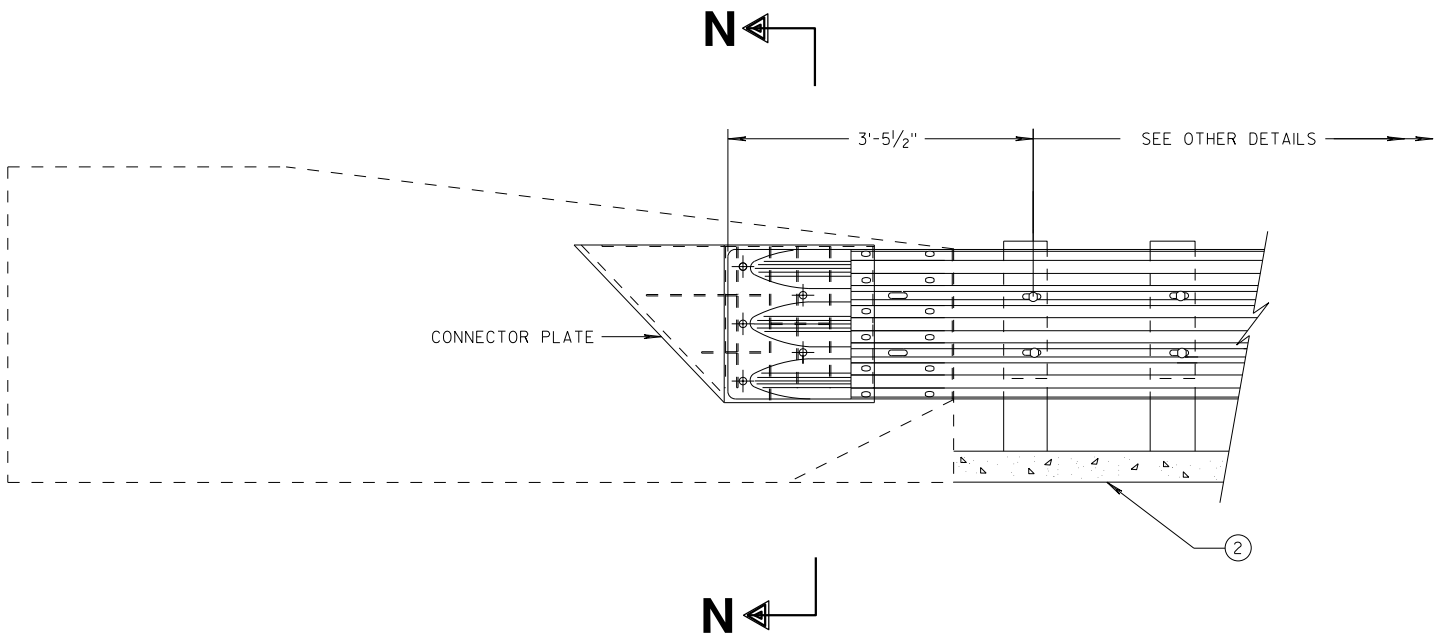


PLATE AND STIFFENER IDENTIFICATION
(VIEWED FROM BACK SIDE OF PLATE)

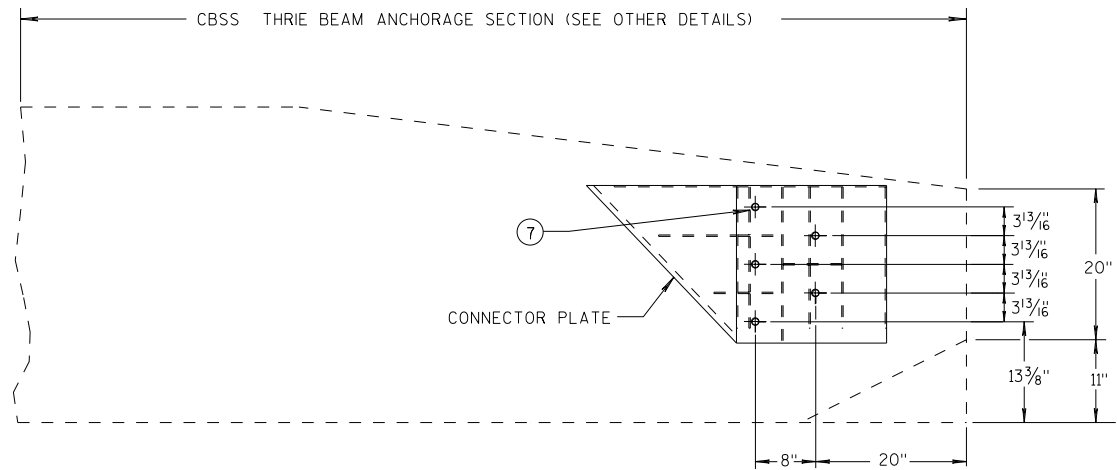
**MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)**

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THRIE BEAM CONNECTION TO SINGLE SLOPE BARRIER



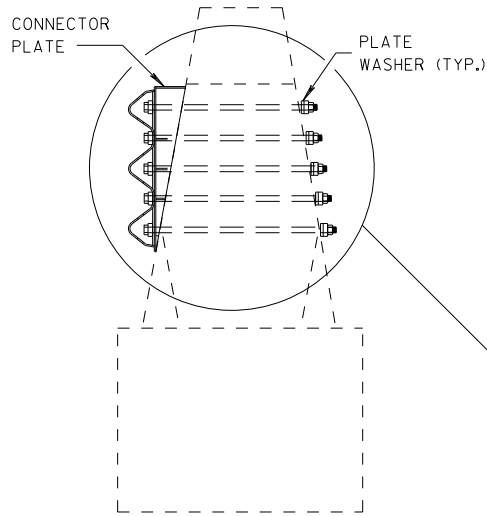
SINGLE SLOPE CONNECTION PLATE PLACEMENT

GENERAL NOTES

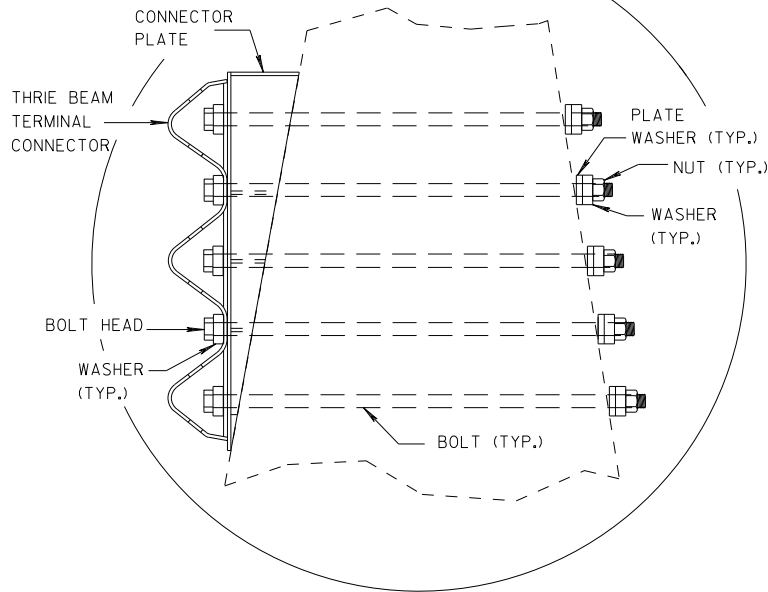
CONNECTOR PLATE, DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

(2) OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

(7) BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTION PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X 5/32" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.



SECTION N-N



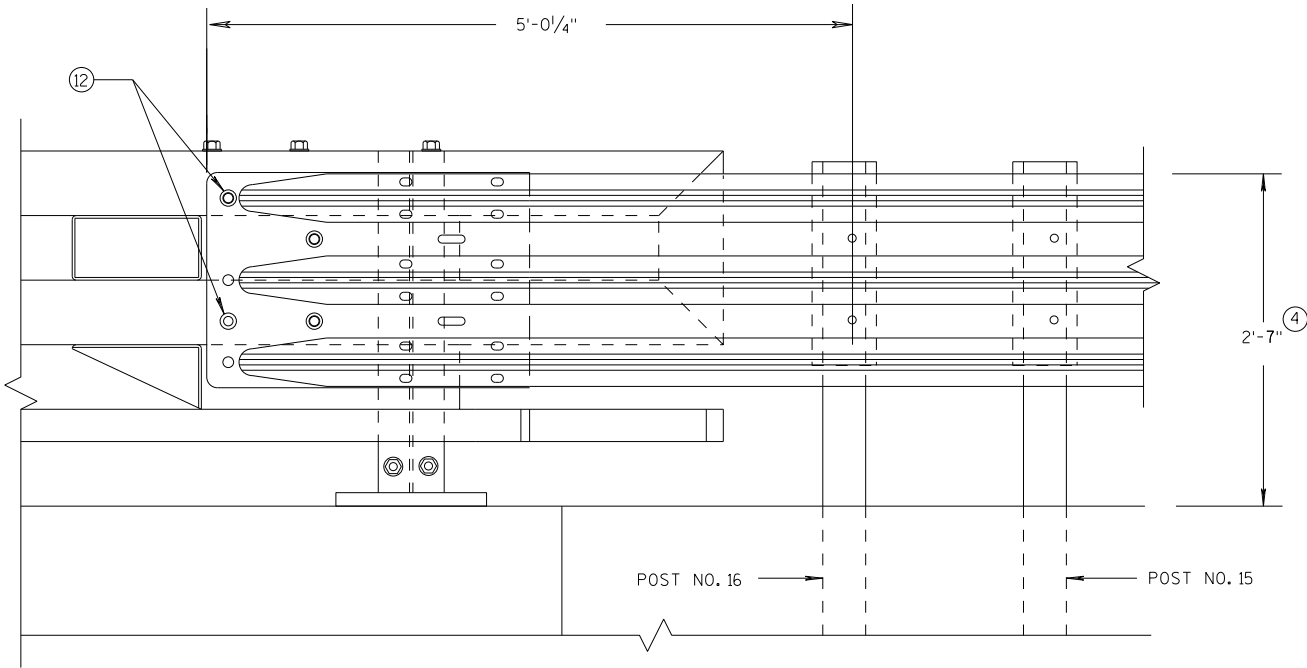
MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
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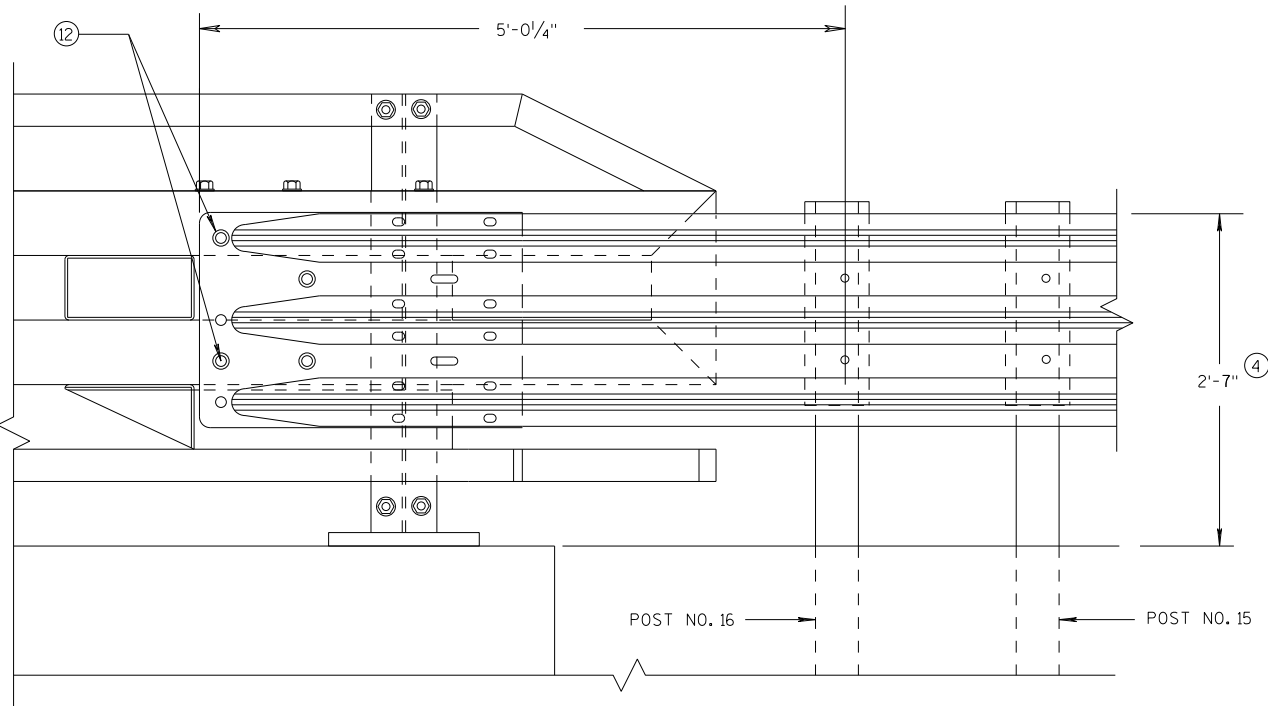
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ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR
FHWA

GENERAL NOTES

- ④ TOLERANCE FOR TOP OF BEAM IS $\pm 1"$.
- ⑫ BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. ON BACKSIDE OF PARAPET ONE ROUND WASHER, AND NUT REQUIRED. BOLT THREAD IS TO EXTEND $\frac{1}{2}$ -INCH BEYOND NUT.



ELEVATION OF DETAIL AT NY3 END POST
THRIE BEAM RAIL ATTACHMENT

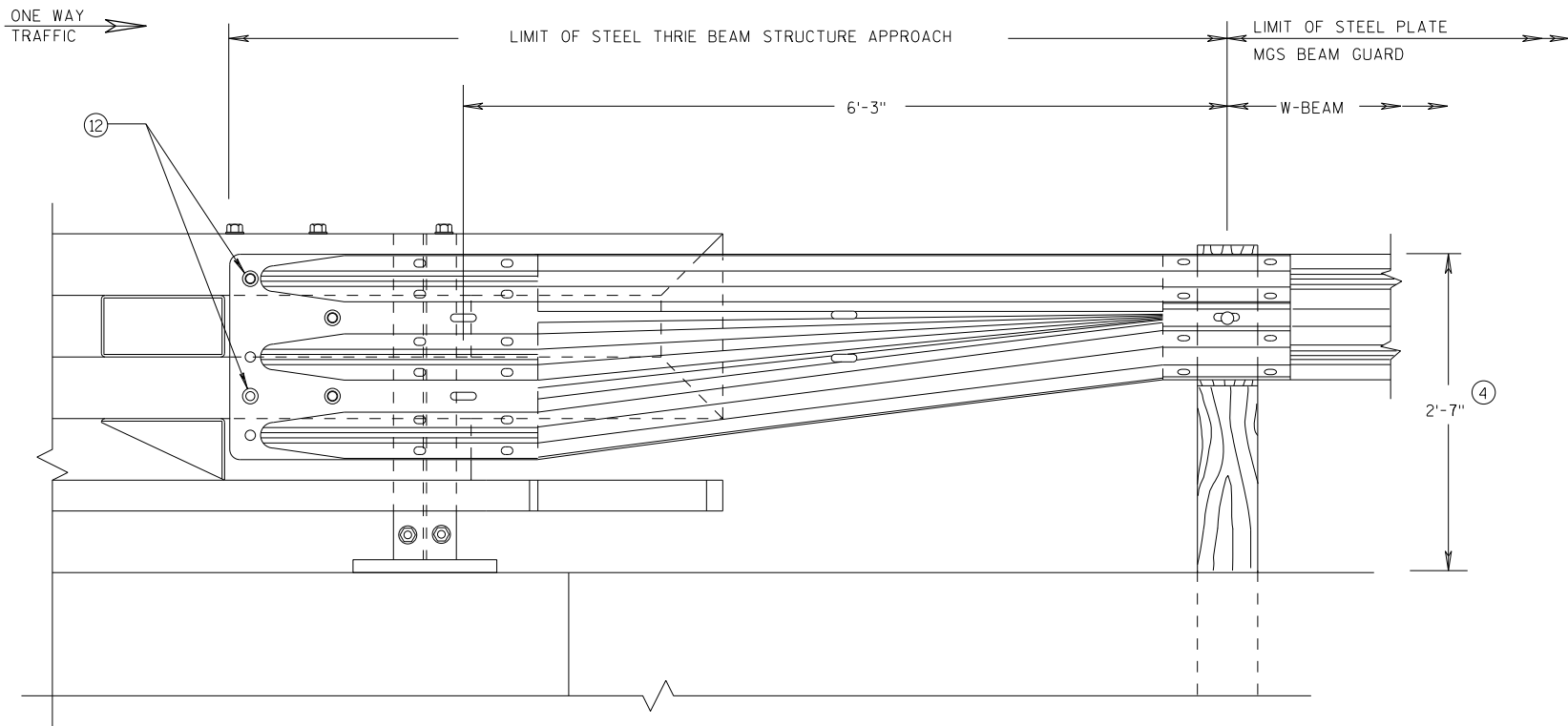


ELEVATION OF DETAIL AT NY4 END POST
THRIE BEAM RAIL ATTACHMENT

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

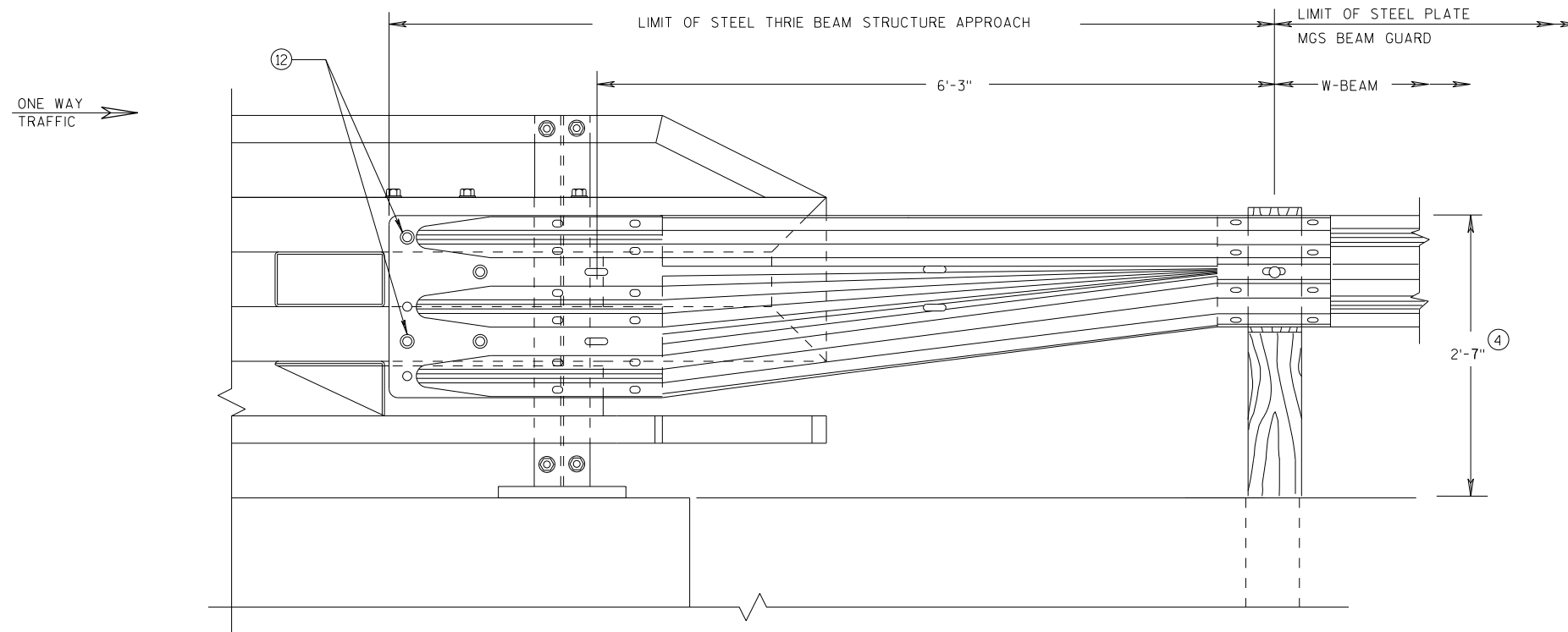
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DATE	UNIT SUPERVISOR
FHWA	



FRONT VIEW
W BEAM TRANSITION AND
CONNECTION TO BRIDGE RAILING TYPE "NY3"
 (USE ONLY ON THE TRAFFIC EXIT END OF ONE WAY BRIDGES)

GENERAL NOTES

- (4) TOLERANCE FOR TOP OF BEAM IS $\pm 1"$.
- (12) BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. ON BACKSIDE OF PARAPET ONE ROUND WASHER, AND NUT REQUIRED. BOLT THREAD IS TO EXTEND $\frac{1}{2}$ -INCH BEYOND NUT.



FRONT VIEW
W BEAM TRANSITION AND
CONNECTION TO BRIDGE RAILING TYPE "NY4"
 (USE ONLY ON THE TRAFFIC EXIT END OF ONE WAY BRIDGES)

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

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 DATE
 /S/ Rodney Taylor
 ROADWAY STANDARDS DEVELOPMENT
 UNIT SUPERVISOR
 FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

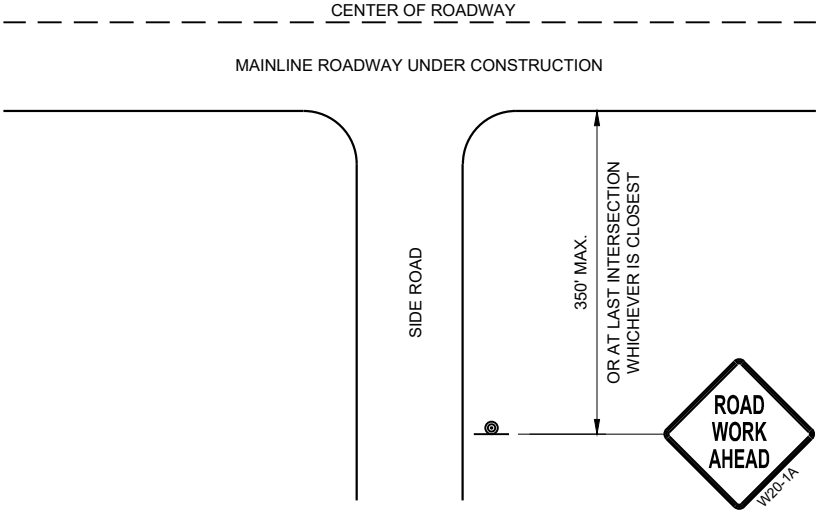
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

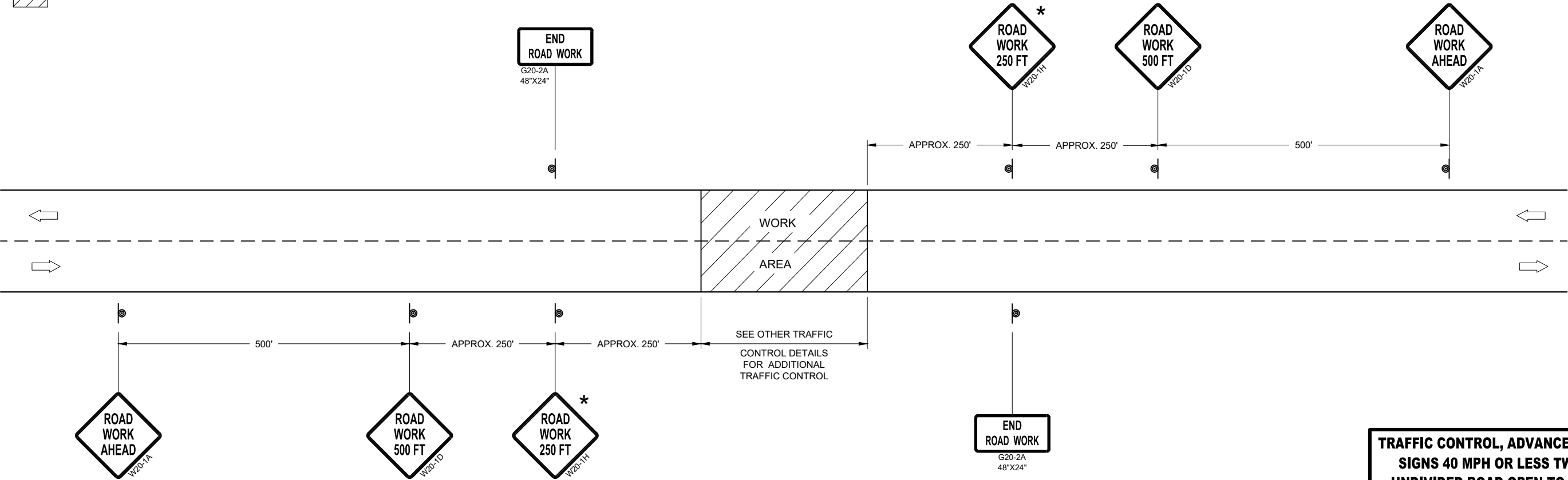
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL

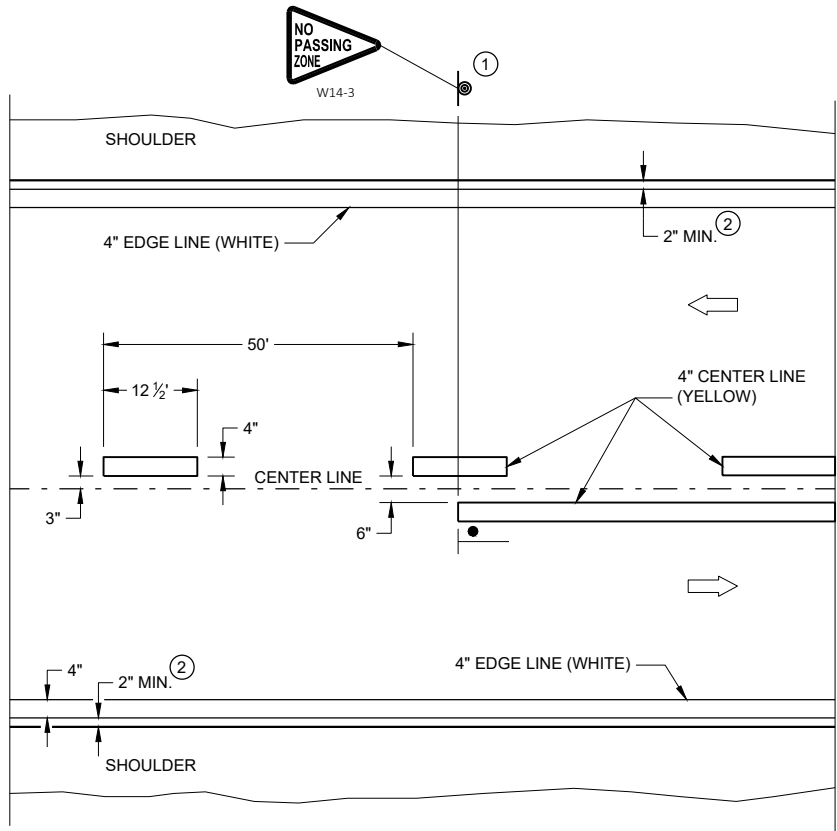


TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

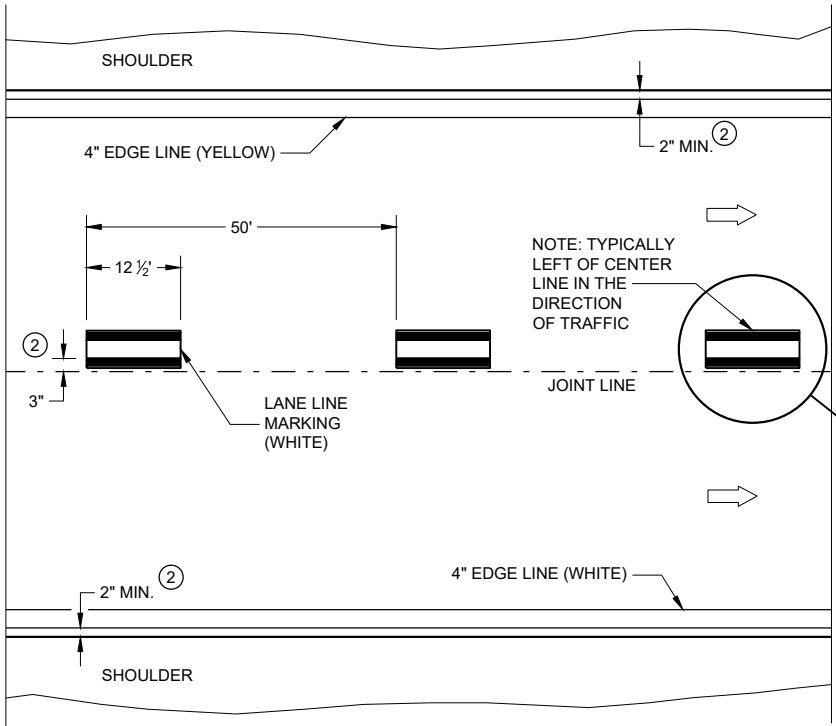
TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 40 MPH OR LESS TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA

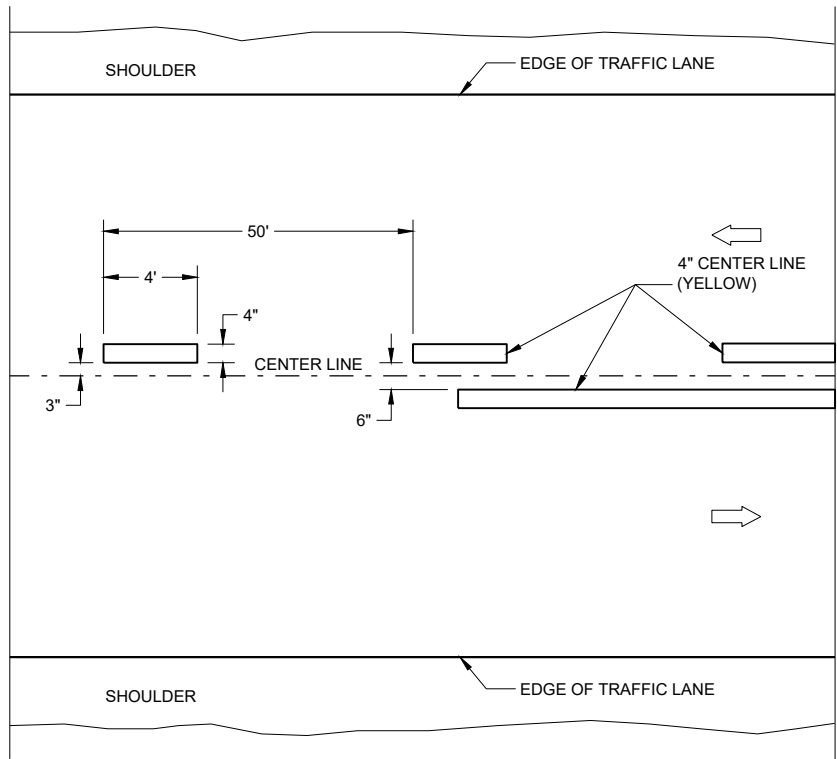


TWO WAY TRAFFIC

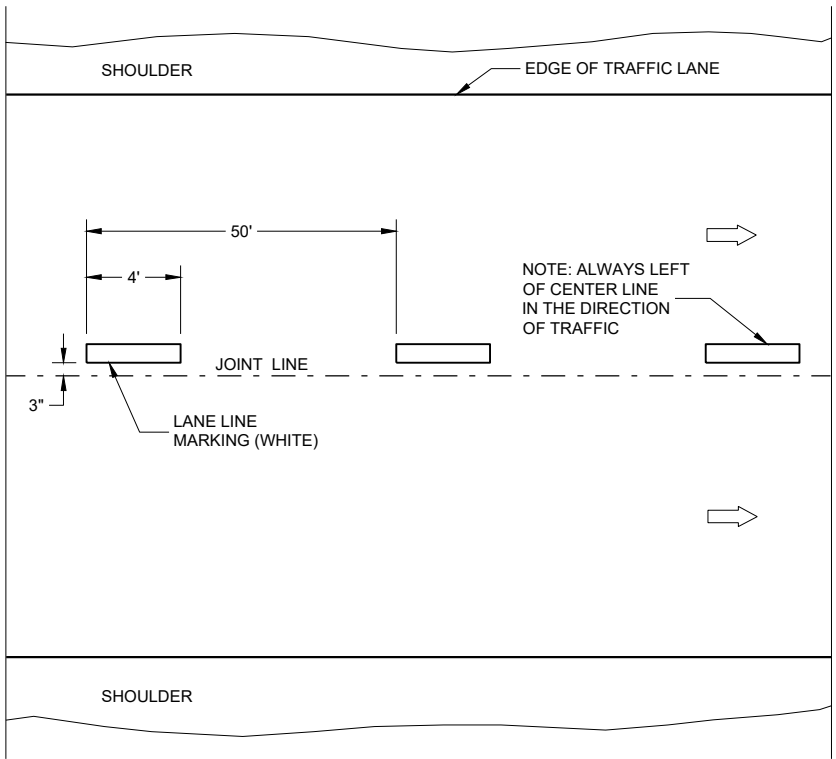


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

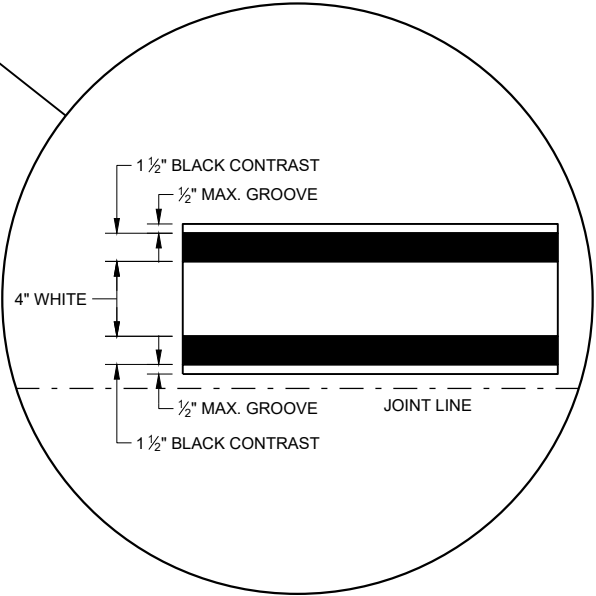
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

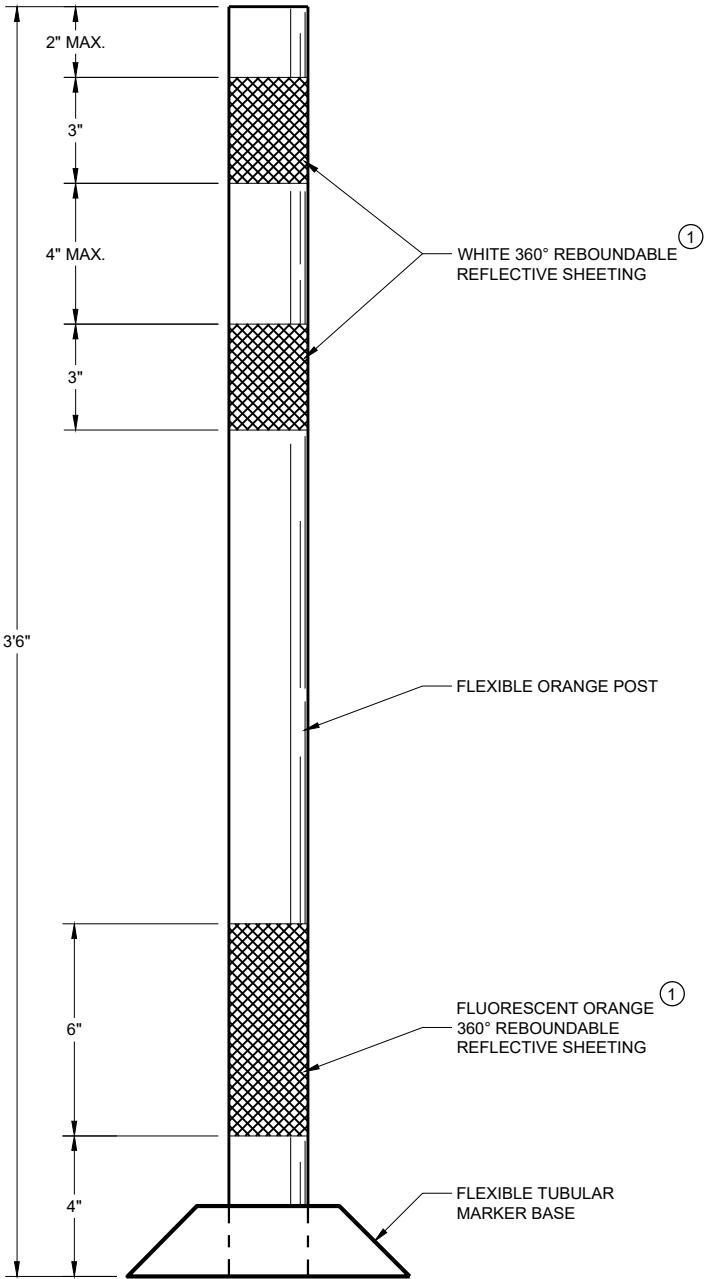
- "T" MARKING
- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC



LONGITUDINAL MARKING (MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE
/S/ Matthew Rauch
STATEWIDE SIGNING AND MARKING
ENGINEER
FHWA



FLEXIBLE TUBULAR
MARKER POST
WORK ZONE

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

SURFACE MOUNTED BASES SHALL BE FURNISHED IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS TO BE COMPATIBLE WITH FLEXIBLE TUBULAR MARKER POSTS TO A SIZE AND SHAPE THAT WILL PROVIDE A STABLE POST FOUNDATION WHEN SECURED TO THE PAVEMENT.

THE ASPHALTIC ADHESIVE OR BUTYL PAD FURNISHED SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS, UNLESS DIRECTED BY THE ENGINEER TO USE BOLTS.

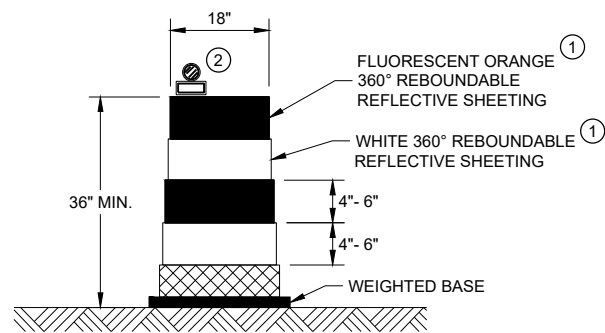
① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.

CHANNELIZING DEVICES
FLEXIBLE TUBULAR
MARKER POST

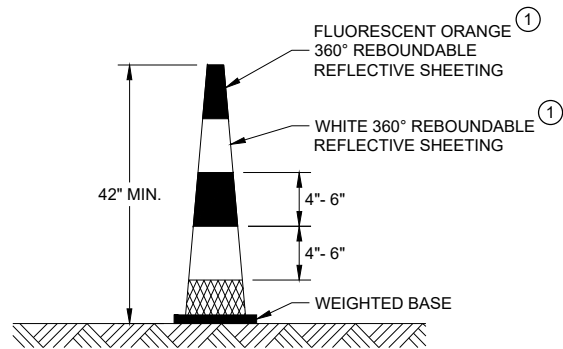
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2021 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

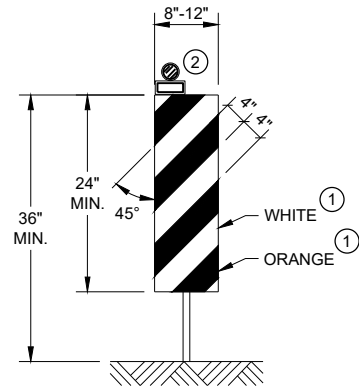


DRUM



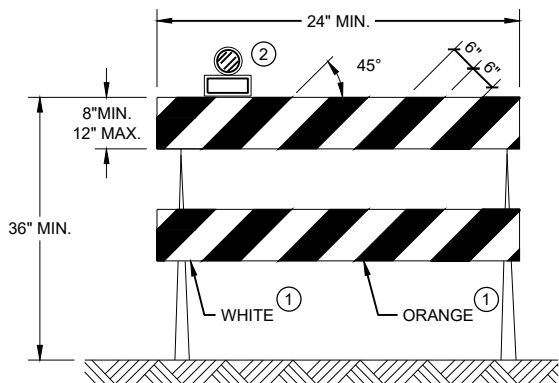
42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS



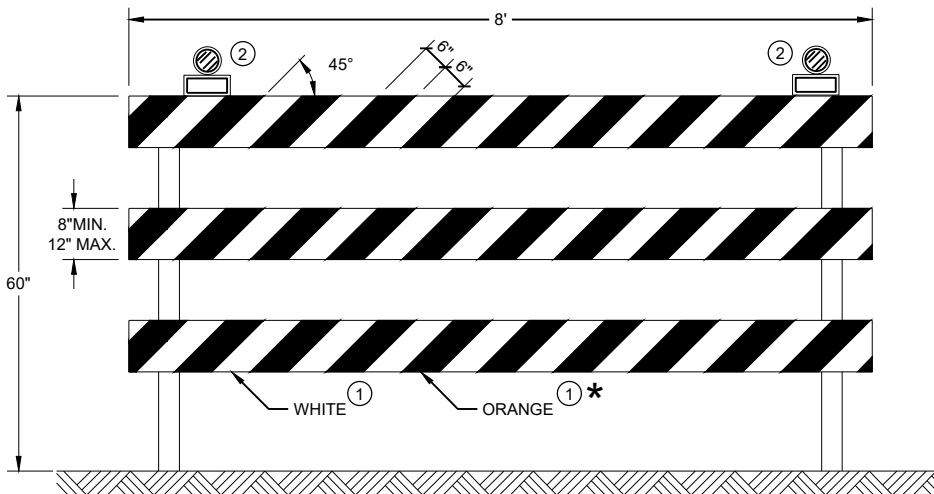
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.






CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2021 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

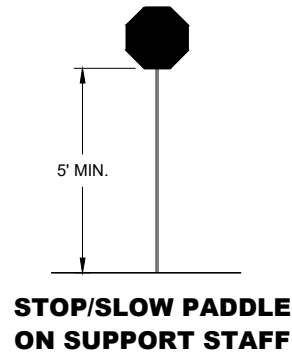
FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

- UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER
- ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.
- INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.
- PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.
- DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

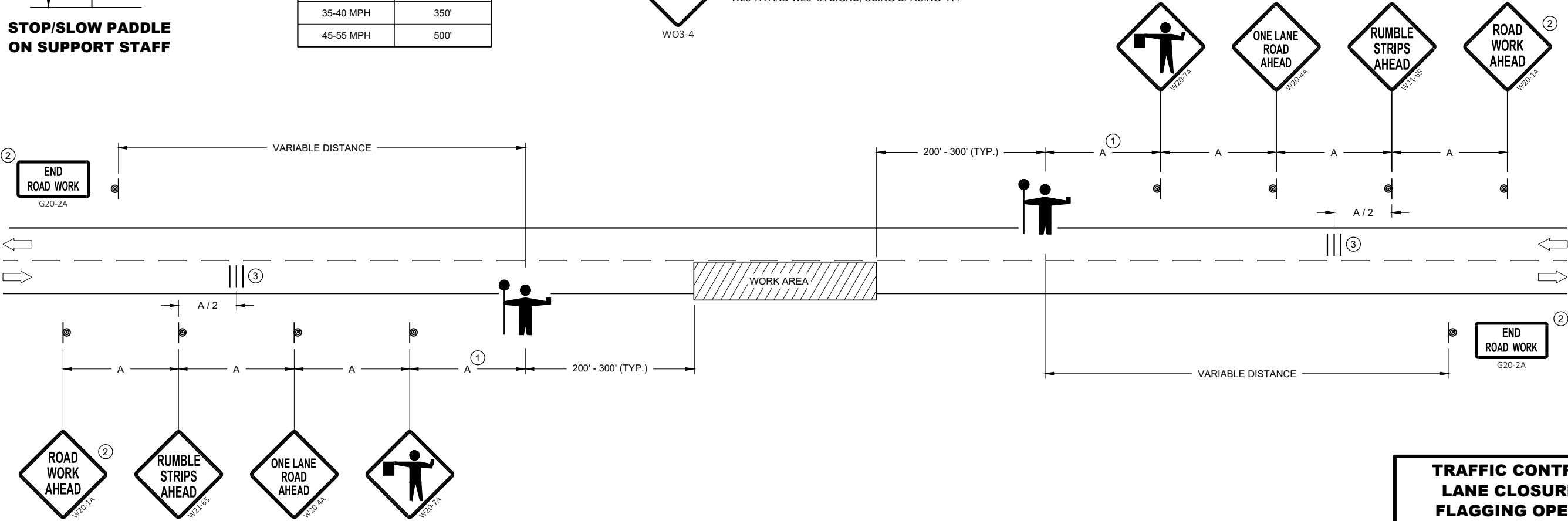


SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'







USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2021 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

- V1 LEAD VEHICLE
- V2 MARKING VEHICLE
- V3 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (CAUTION)

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

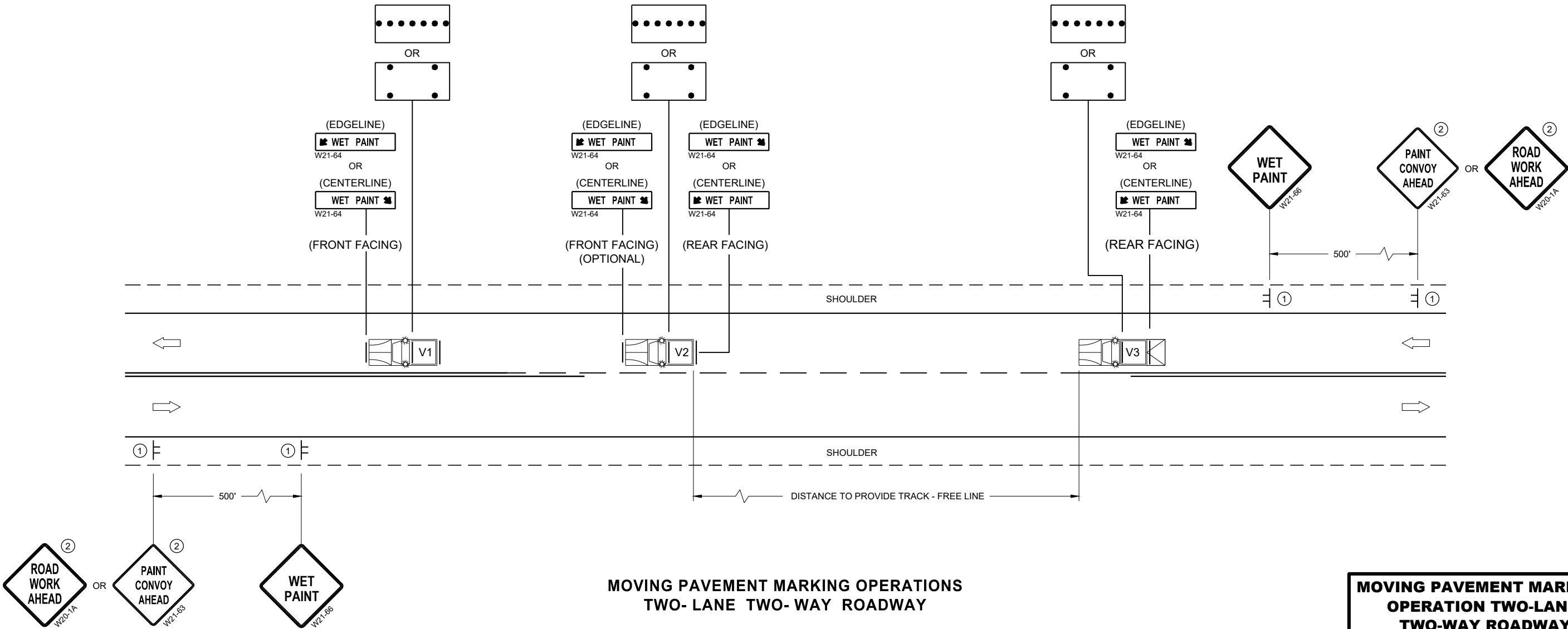
THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING .

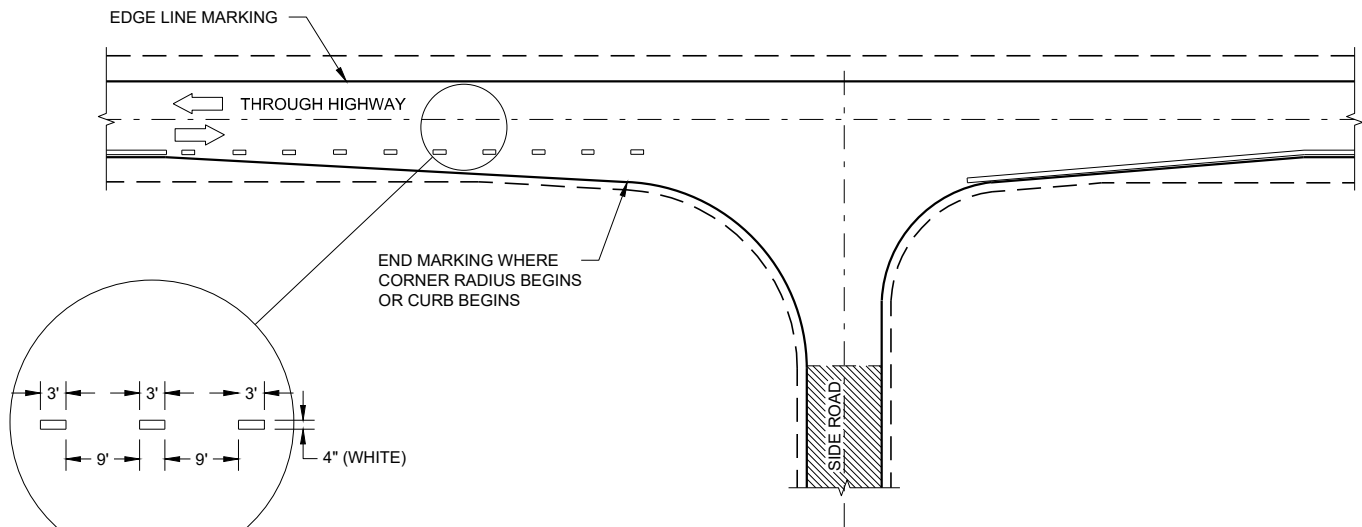
- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.



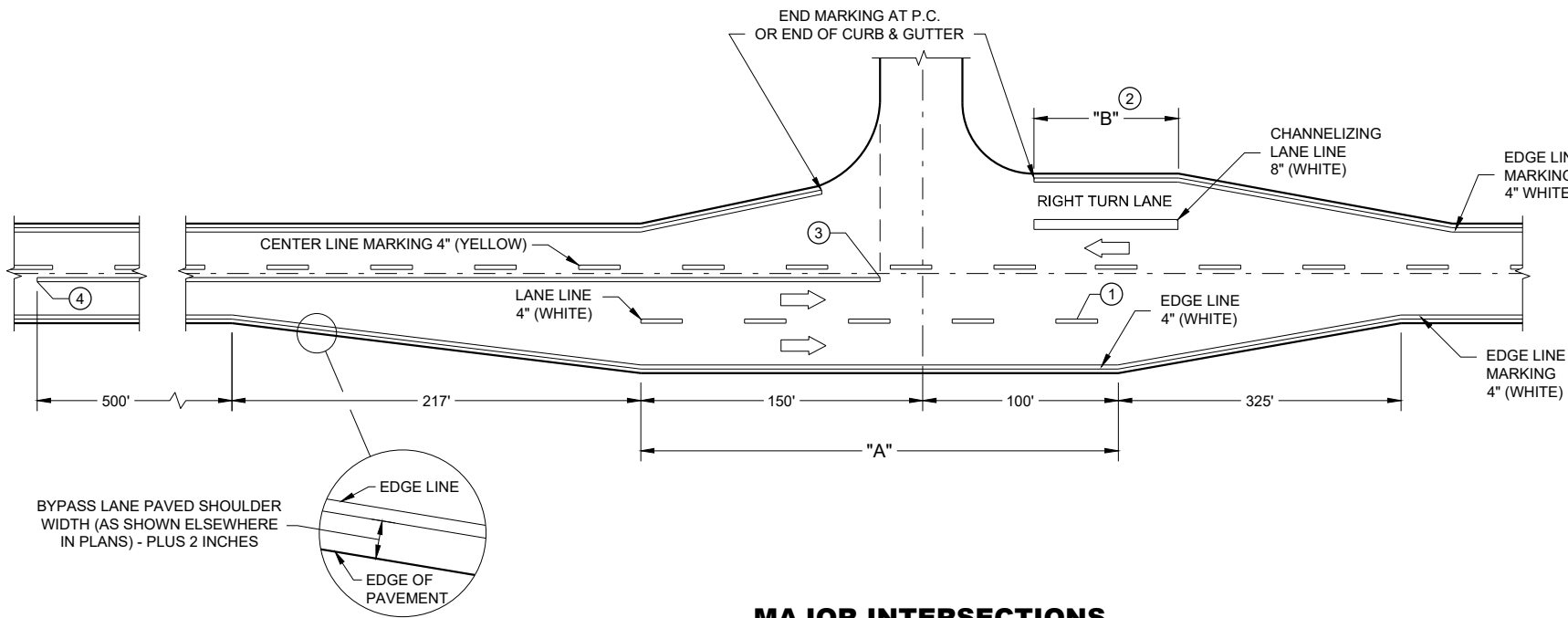
MOVING PAVEMENT MARKING
OPERATION TWO-LANE
TWO-WAY ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA



MINOR INTERSECTION



MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)

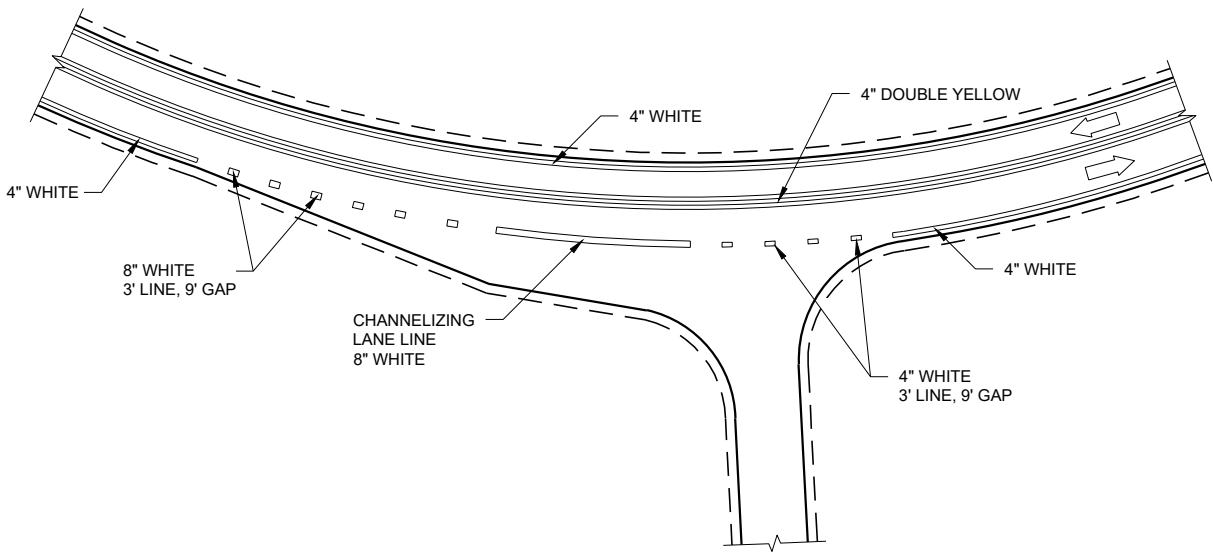
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.

LEGEND

➡ DIRECTION OF TRAVEL







INTERSECTION ON OUTSIDE OF CURVE

PAVEMENT MARKING
(INTERSECTIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  DIRECTION OF TRAFFIC
-  WORK ZONE

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

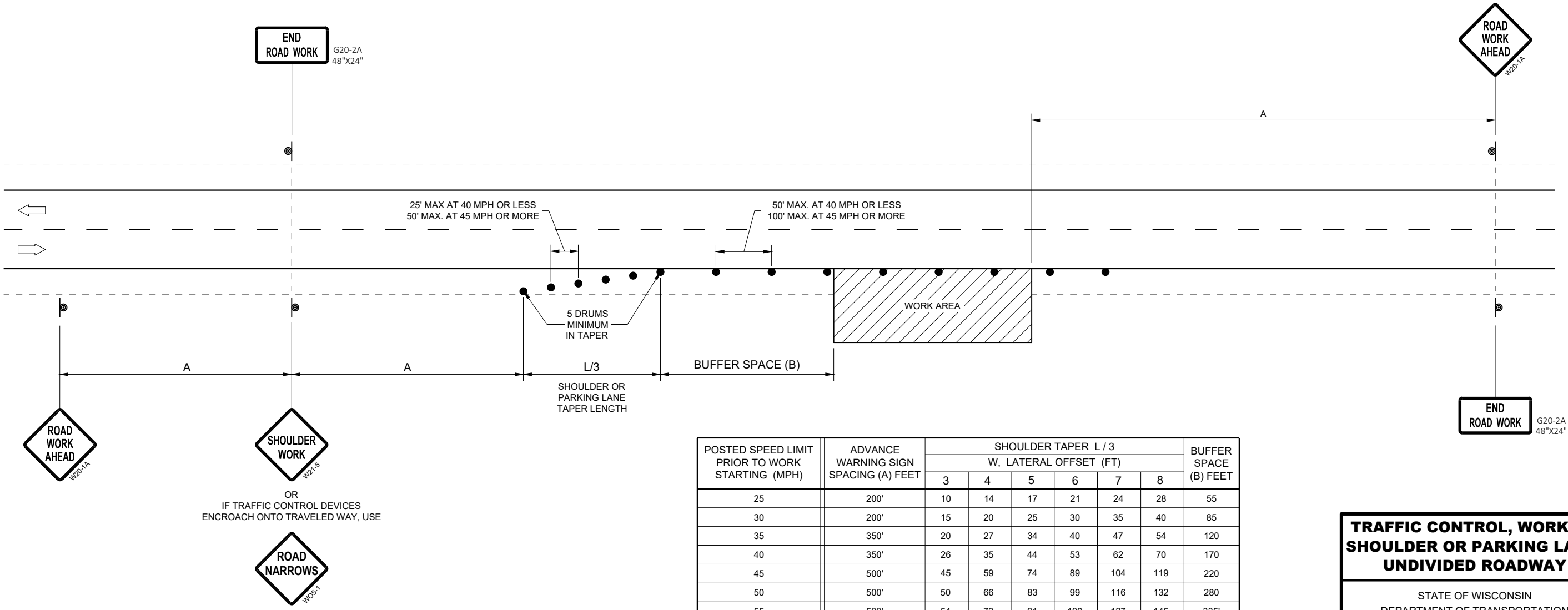
ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.



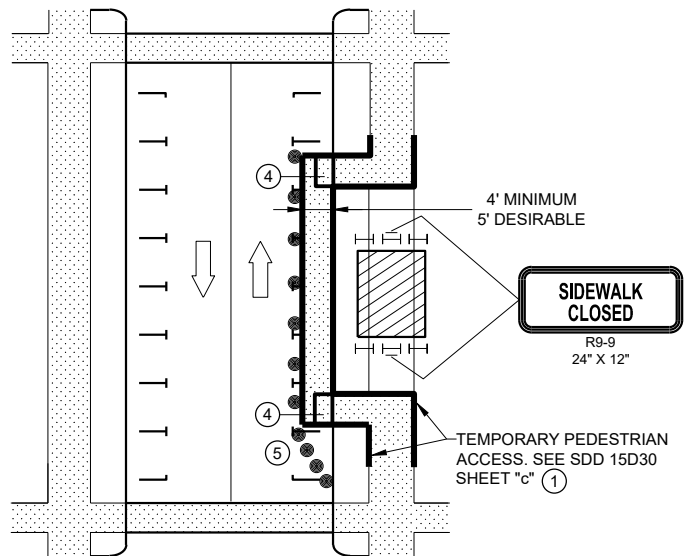
POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHOULDER TAPER L / 3 W, LATERAL OFFSET (FT)						BUFFER SPACE (B) FEET
		3	4	5	6	7	8	
25	200'	10	14	17	21	24	28	55
30	200'	15	20	25	30	35	40	85
35	350'	20	27	34	40	47	54	120
40	350'	26	35	44	53	62	70	170
45	500'	45	59	74	89	104	119	220
50	500'	50	66	83	99	116	132	280
55	500'	54	73	91	109	127	145	335'

TRAFFIC CONTROL, WORK ON
SHOULDER OR PARKING LANE,
UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

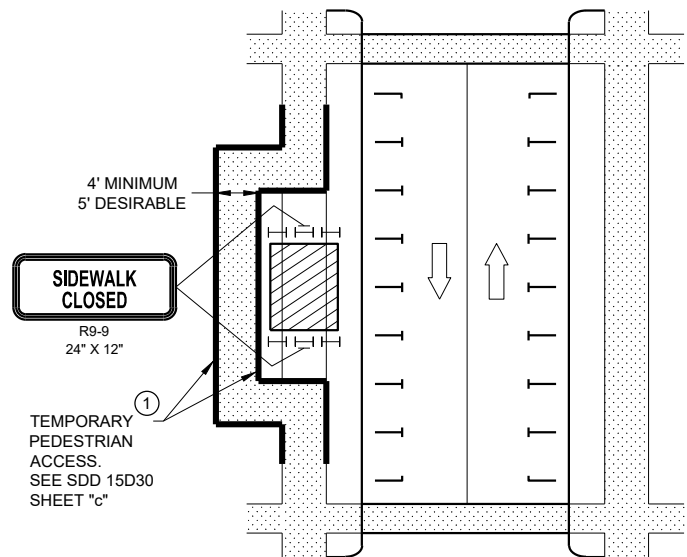
APPROVED
May 2020 /S/ Andrew Heidtke
DATE STATEWIDE WORK ZONE TRAFFIC
SAFETY ENGINEER
FHWA

NOTE: MAY BE USED ON ROADWAY WITH POSTED SPEED OF LESS THAN 40 MPH.

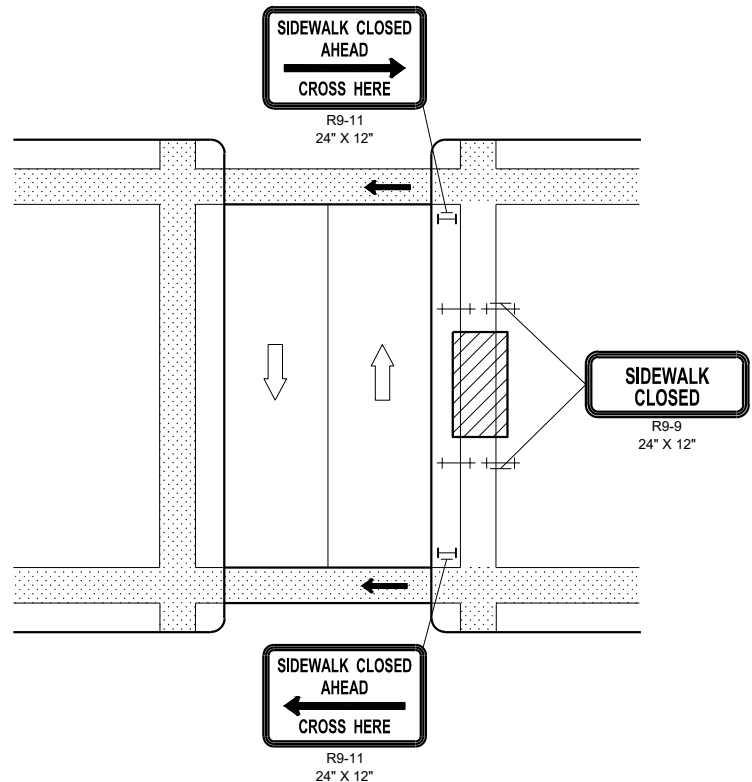


MID-BLOCK SIDEWALK CLOSURE
IN PARKING LANE

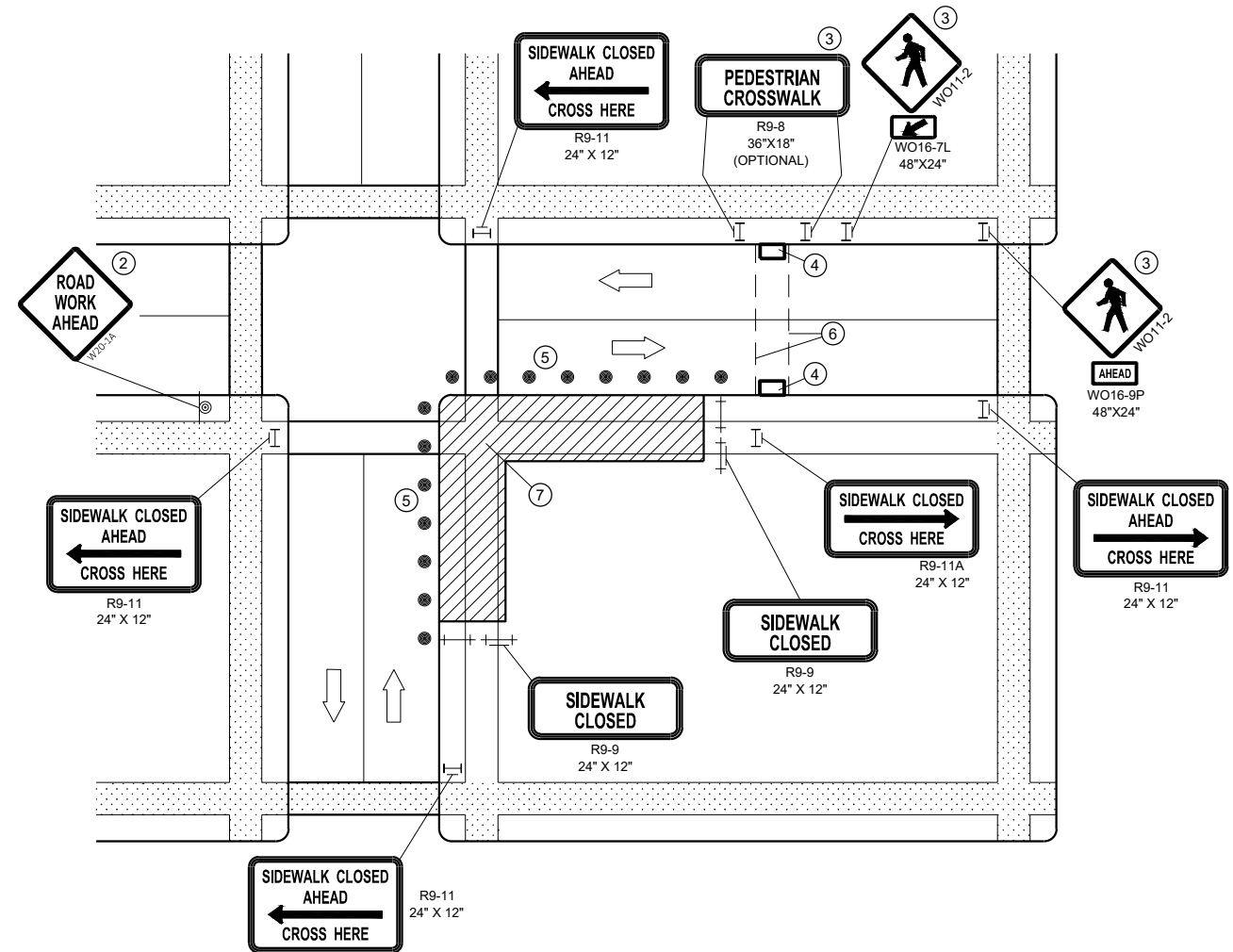
NOTE: LAYOUT SAME AS ABOVE.



SIDEWALK DIVERSION



MID-BLOCK SIDEWALK
CLOSURE



CORNER SIDEWALK CLOSURE WITH TEMPORARY CROSSWALK

GENERAL NOTES

WHEN CLOSING OR RELOCATING CROSSWALKS OR SIDEWALKS, PROVIDE DETECABLE TEMPORARY FACILITIES AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH EXISTING PEDESTRIAN FACILITIES.

TEMPORARY TRAFFIC CONTROL DEVICES FOR PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL VEHICULAR TRAFFIC. STAGE WORK AS NECESSARY, TO PROVIDE A TEMPORARY PEDESTRIAN ACCESS ROUTE AT ALL TIMES. FOR ROADWAYS WITH NO AVAILABLE DETOURS, MAINTAIN ONE OPEN SIDEWALK AT ALL TIMES.

"WO" SIGN IS THE SAME AS "W" SIGN, EXCEPT THE BACKGROUND IS ORANGE.

FOR NIGHTTIME CLOSURE, USE TYPE "A" FLASHING WARNING LIGHTS ON BARRICADES, SUPPORTING SIGNS AND CLOSING SIDEWALK. USE TYPE "C" STEADY BURN LIGHTS ON CHANNELIZING DEVICES SEPARATING THE WORK AREA FROM VEHICULAR TRAFFIC.

PEDESTRIAN TRAFFIC SIGNAL DISPLAY CONTROLLING CLOSED CROSSWALK SHALL BE COVERED OR DEACTIVATED.

POST MOUNTED SIGNS LOCATED ADJACENT TO A SIDEWALK SHALL HAVE A 7 FOOT MINIMUM CLEARANCE FROM THE BOTTOM OF THE SIGN TO THE SIDEWALK SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

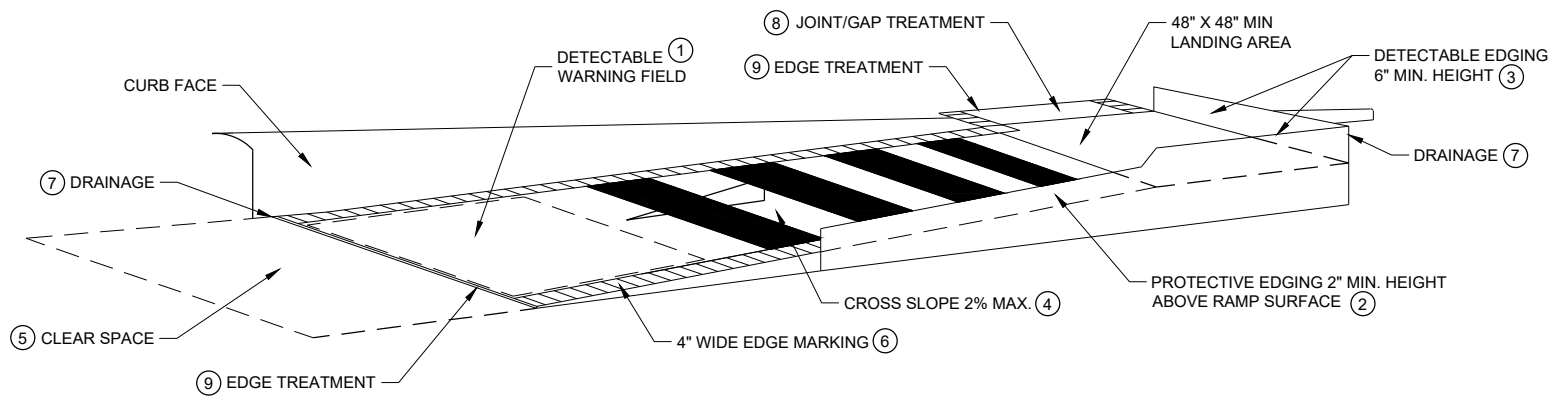
- ① IF SIDEWALK CLOSURE AFFECTS AN ACCESSIBLE AND DETECTABLE FACILITY, MAINTAIN ACCESSIBILITY AND DETECTABILITY ALONG THE ALTERNATE PEDESTRIAN ROUTE
- ② "ROAD WORK AHEAD" SIGNS ARE NOT REQUIRED IF THE SIDEWALK CLOSURE OCCURS WITHIN A LARGER WORK ZONE WHERE ADVANCE WARNING SIGNS ARE ALREADY PRESENT, OR IF THE WORK AREA AND EQUIPMENT ARE MORE THAN 2 FEET BEHIND THE CURB.
- ③ IF TEMPORARY PEDESTRIAN CROSSWALK IS NOT PROVIDED, OMIT R9-8 AND WO11-2 SIGN ASSEMBLIES. IF PROVIDED INCLUDE ON BOTH SIDES OF THE CROSSWALK.
- ④ TEMPORARY CURB RAMPS. SEE SDD 15D30 SHEET "b".
- ⑤ DRUMS OR BARRICADES AT 25 FOOT SPACING. STREET PARKING SHALL BE PROHIBITED FOR AT LEAST 50 FEET IN ADVANCE OF THE MID-BLOCK CROSSWALK.
- ⑥ TEMPORARY PAVEMENT MARKING FOR CROSSWALK LINES.
- ⑦ LIMIT WORK TO ONE QUADRANT AT A TIME TO MINIMIZE PEDESTRIAN DISRUPTION.

LEGEND

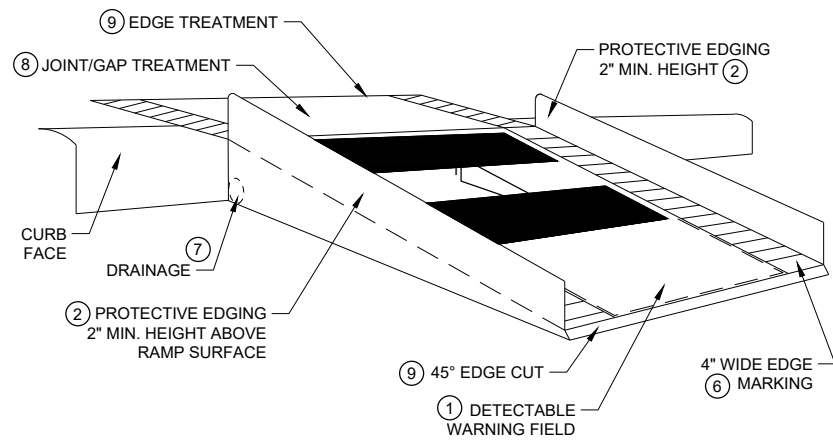
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)
- TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)
- UNDER PEDESTRIAN TRAFFIC
- WORK AREA
- PEDESTRIAN CHANNELIZATION DEVICE
- DIRECTION OF TRAFFIC

TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION

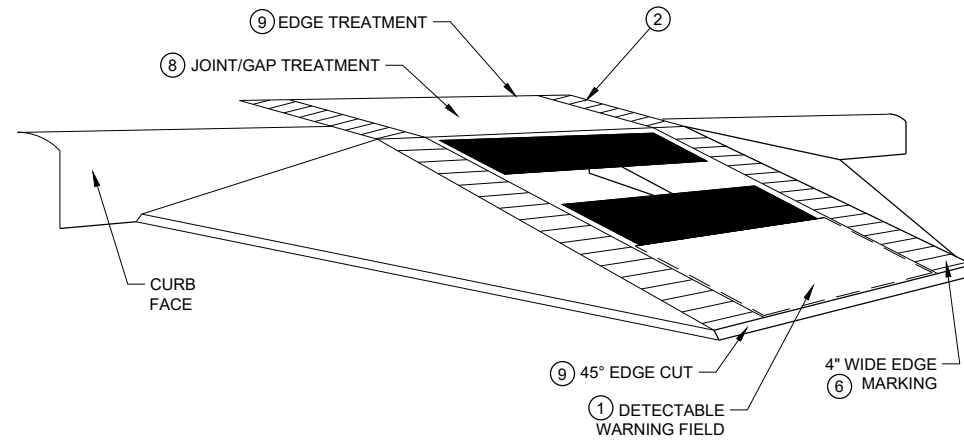
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



TEMPORARY CURB RAMP PARALLEL TO CURB



WITH PROTECTIVE EDGE



WITH SIDE APRON

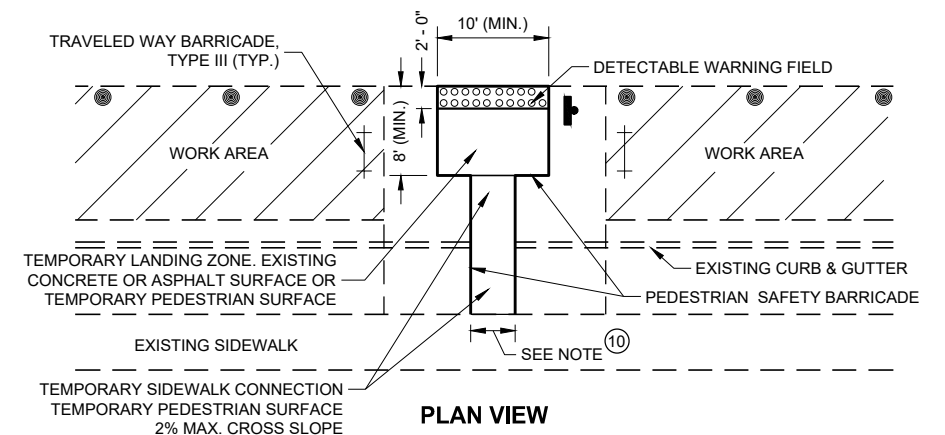
TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

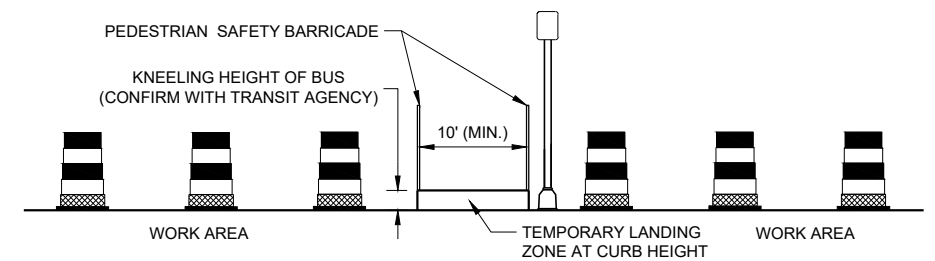
NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

- 1 CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE. INSTALL CONTRASTING DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS. REFER TO SDD 08D05, SHEET "e".
- 2 PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- 3 DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- 4 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
- 5 CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
- 6 THE CURB RAMP WALKWAY EDGE SHALL BE MARKED WITH A YELLOW COLOR, 4" WIDE MARKING, UNLESS A CONTRASTING DETECTABLE WARNING FIELD IS PROVIDED.
- 7 DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- 8 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
- 9 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES SHALL BE VERTICAL UP TO 1/4" HIGH AND BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".
- 10 5" WIDE MIN. WITH PEDESTRIAN SAFETY BARRICADE, 10' WIDE MIN. WITHOUT PEDESTRIAN SAFETY BARRICADE.



PLAN VIEW



PROFILE VIEW

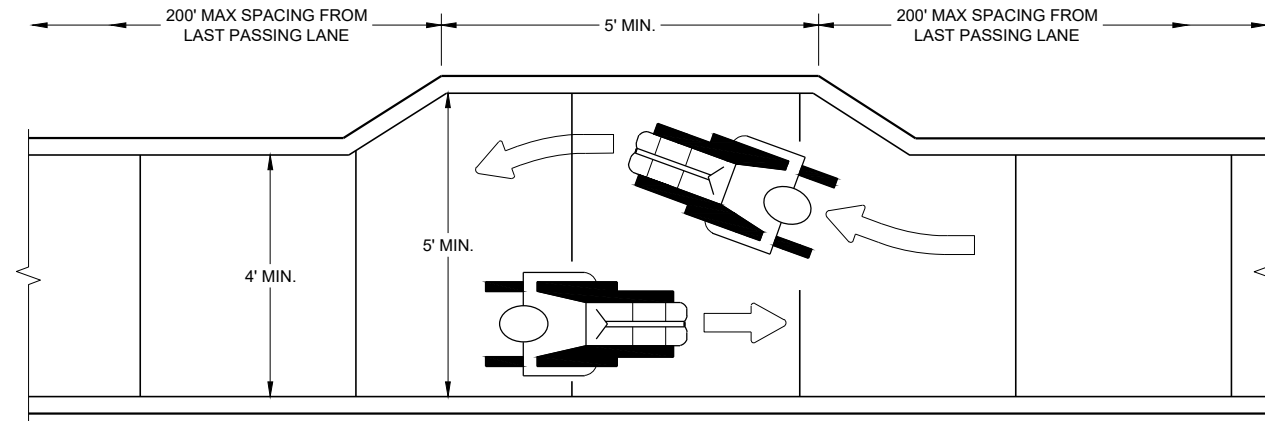
TEMPORARY BUS STOP PAD

LEGEND

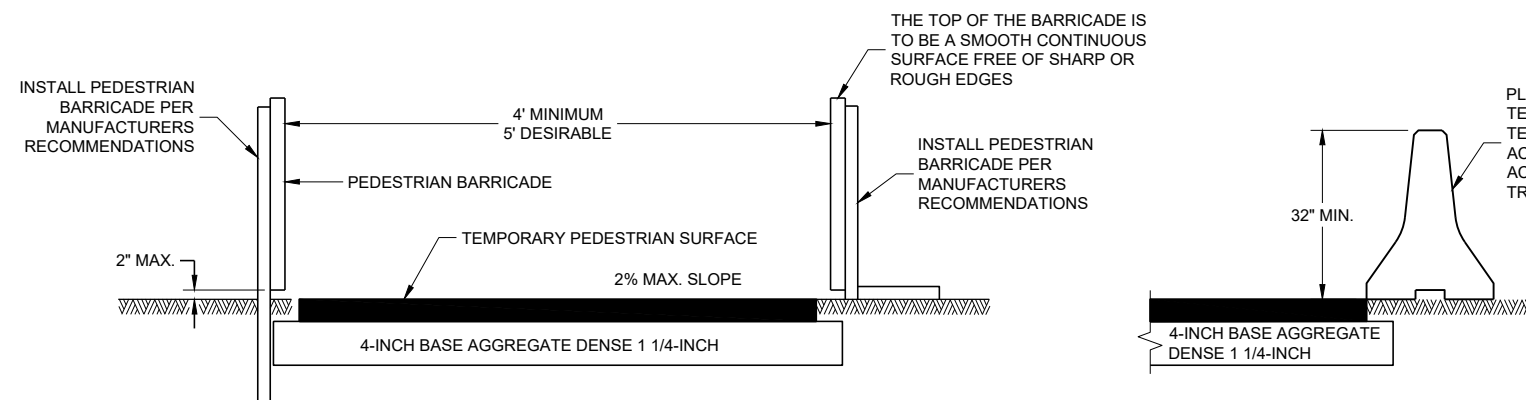
- TRAFFIC CONTROL DRUM
- † TYPE III BARRICADE
- ▨ WORK AREA

TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION

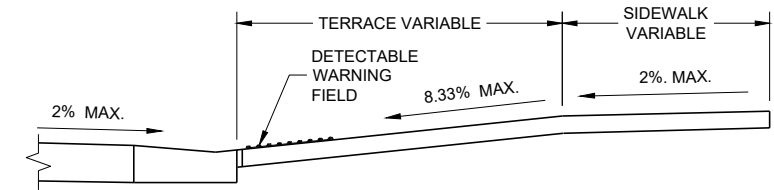
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



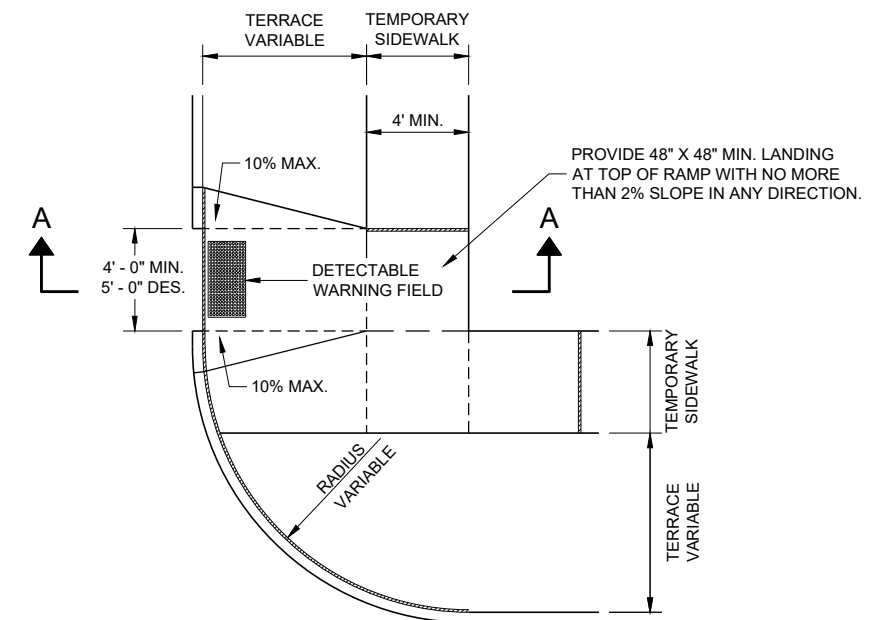
NARROW SIDEWALK PASSING DETAIL



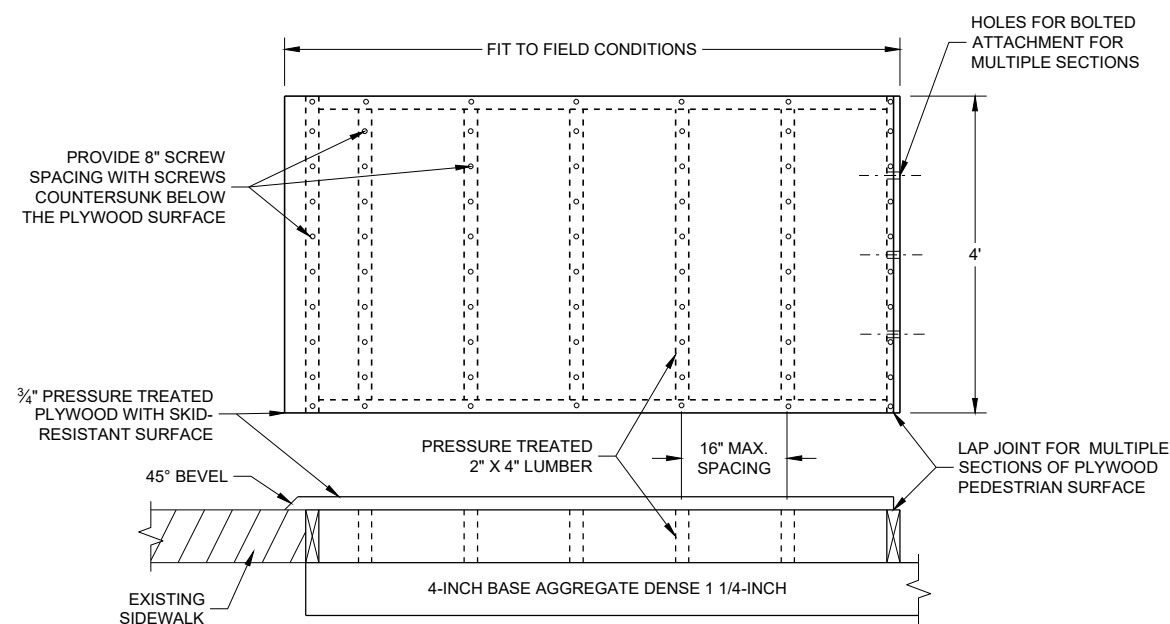
TEMPORARY PEDESTRIAN ACCESS



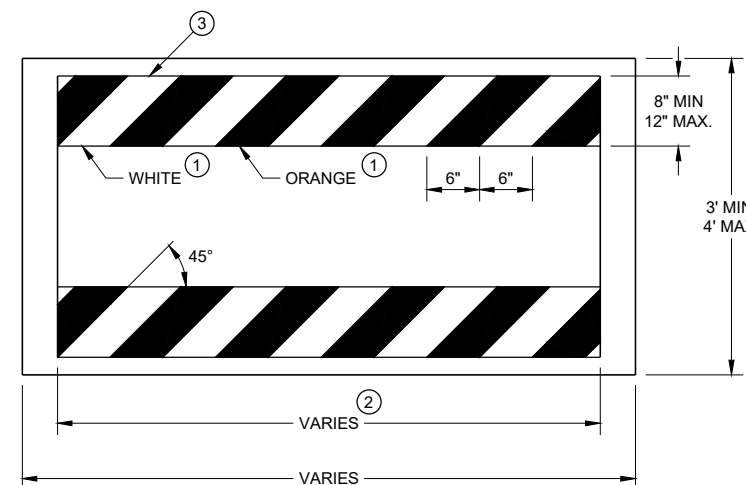
SECTION A - A



**PLAN VIEW
TEMPORARY TYPE 3 RAMP
(OUTSIDE OF CROSSWALK AREA)**



TEMPORARY PEDESTRIAN SURFACE PLYWOOD

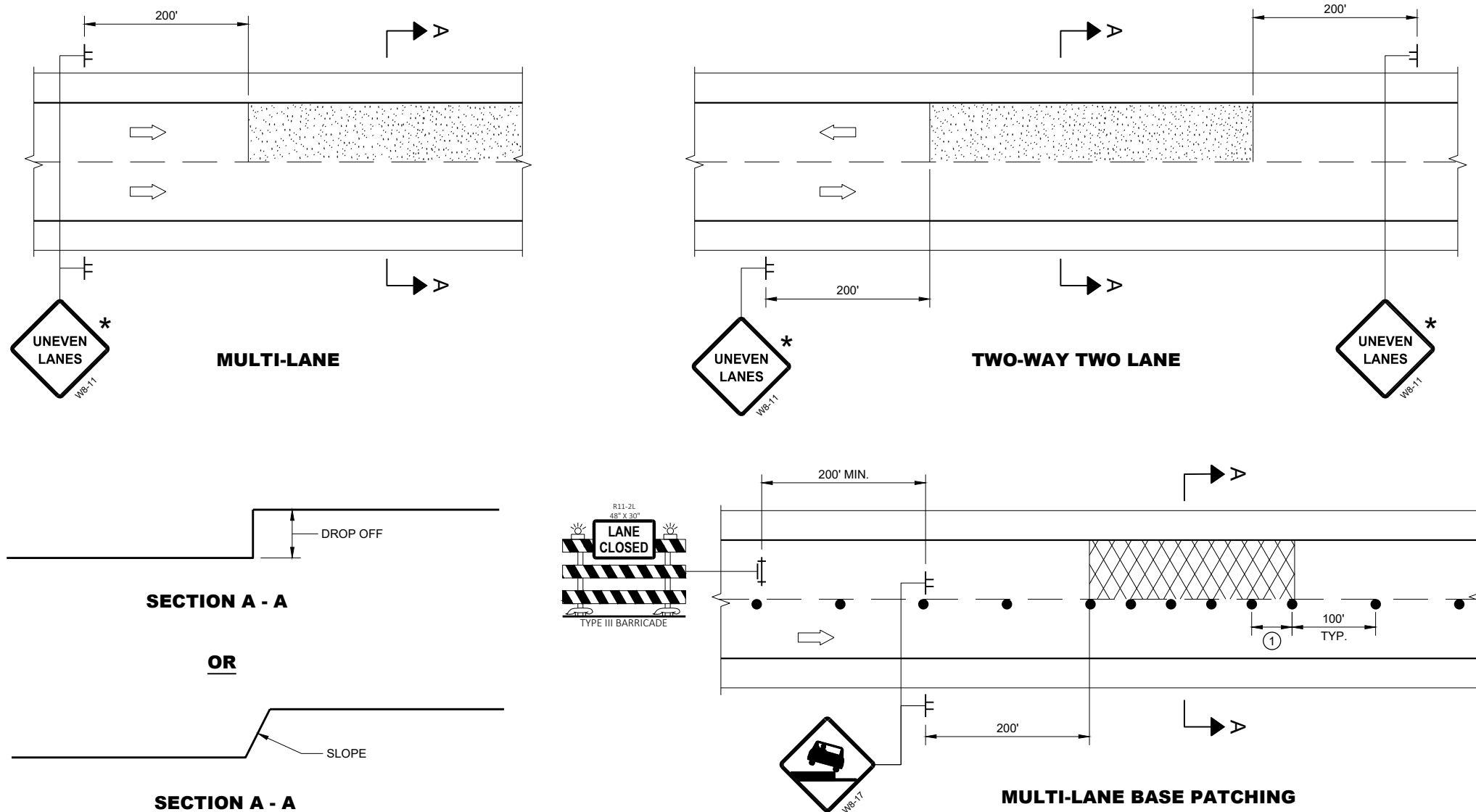


TEMPORARY PEDESTRIAN BARRICADE *

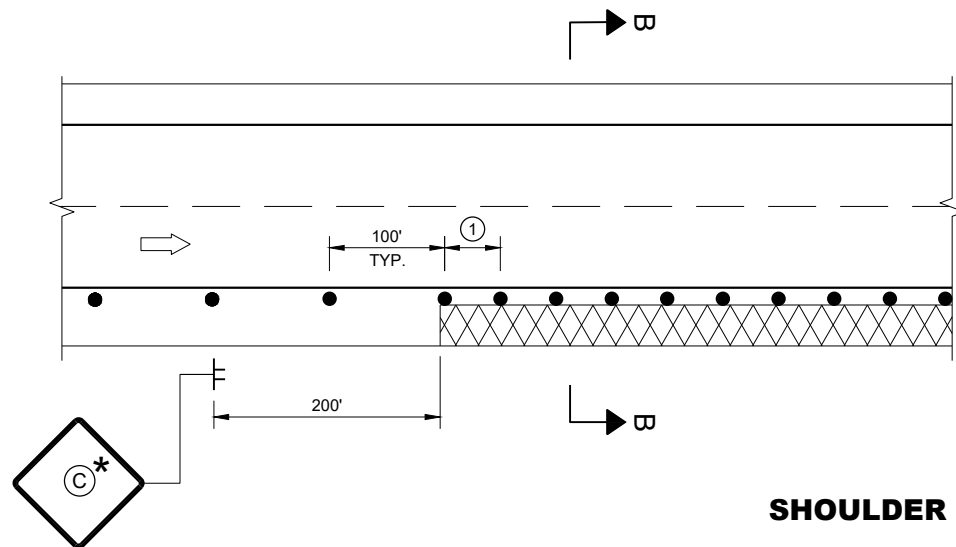
GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- * USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2019 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



ADJACENT LANE DROP-OFFS



SHOULDER DROP-OFFS

SECTION B - B

D	SIGN (C)
< 2" WITH A SLOPE STEEPER THAN 3:1	LOW SHOULDER WO8-9
2" < 6" WITH A SLOPE STEEPER THAN 3:1	SHOULDER DROP - OFF W8-9A PROVIDE A 3:1 OR FLATTER SLOPE OF MATERIAL ADJACENT TO THE PAVEMENT

GENERAL NOTES

FOR SPOT LOCATIONS USE ENGINEERING JUDGEMENT WHEN PLACING ADDITIONAL SIGNS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

* IF THE DROP-OFF IS CONTINUOUS ALONG THE PROJECT, PLACE ADDITIONAL SIGNS EVERY 1 MILE AND AFTER EVERY ENTRANCE RAMP.

① USE CLOSER SPACING WHEN DELINEATING DROP-OFF.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- WORK AREA WITH DROP-OFF
- MILLED SURFACE

TRAFFIC CONTROL, DROP-OFF SIGNING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.



THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

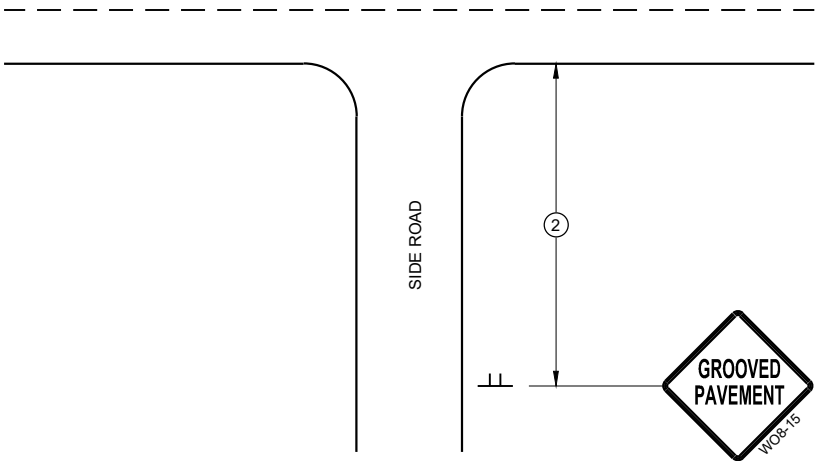
ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

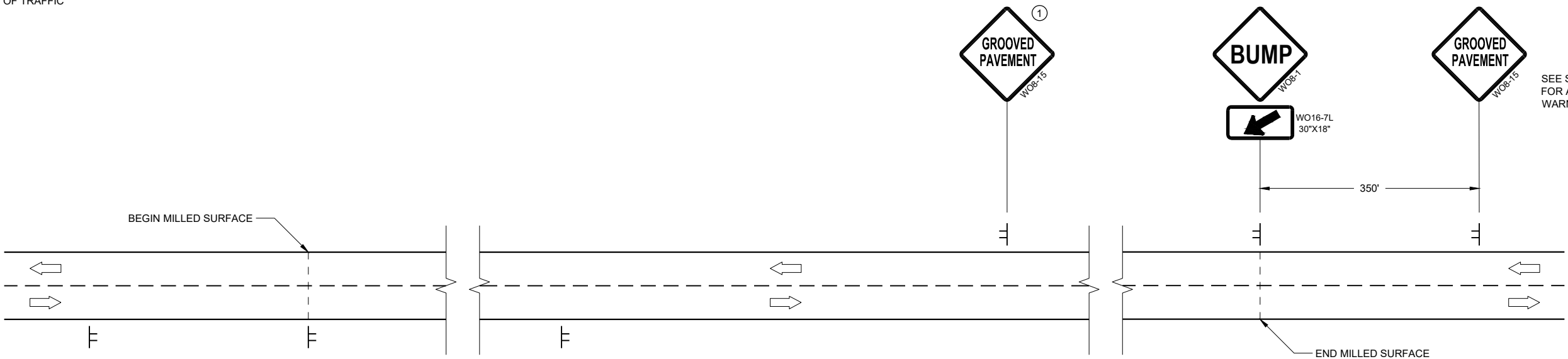
- 1 PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- 2 PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH
SIGN DETAIL



SEE SDD15C04
FOR ADVANCE
WARNING SIGNS

SEE SDD15C04
FOR ADVANCE
WARNING SIGNS

DETAIL FOR SIGNING ON MILLED SURFACES

TRAFFIC CONTROL,
SIGNING ON ROADWAYS
WITH MILLED SURFACES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.



THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

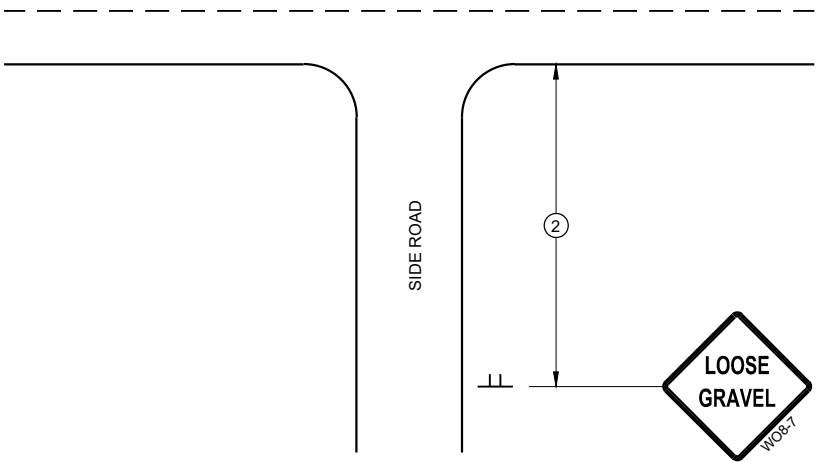
ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

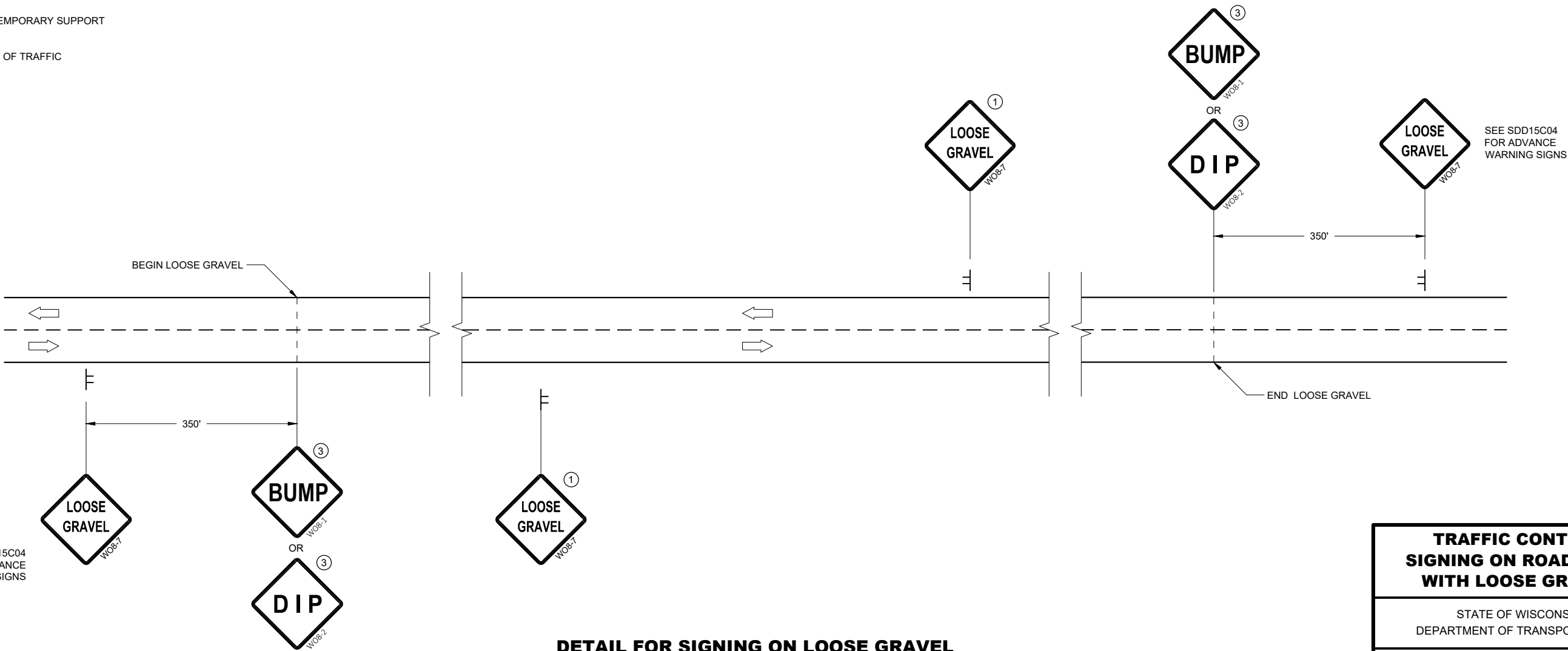
- 1 PLACE SIGNS 350' IN ADVANCE OF CHIP SEALED OR LOOSE GRAVEL SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- 2 PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.
- 3 ADD WO8-1 OR WO8-2 SIGN WHEN THE CONDITION IS PRESENT.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH
SIGN DETAIL

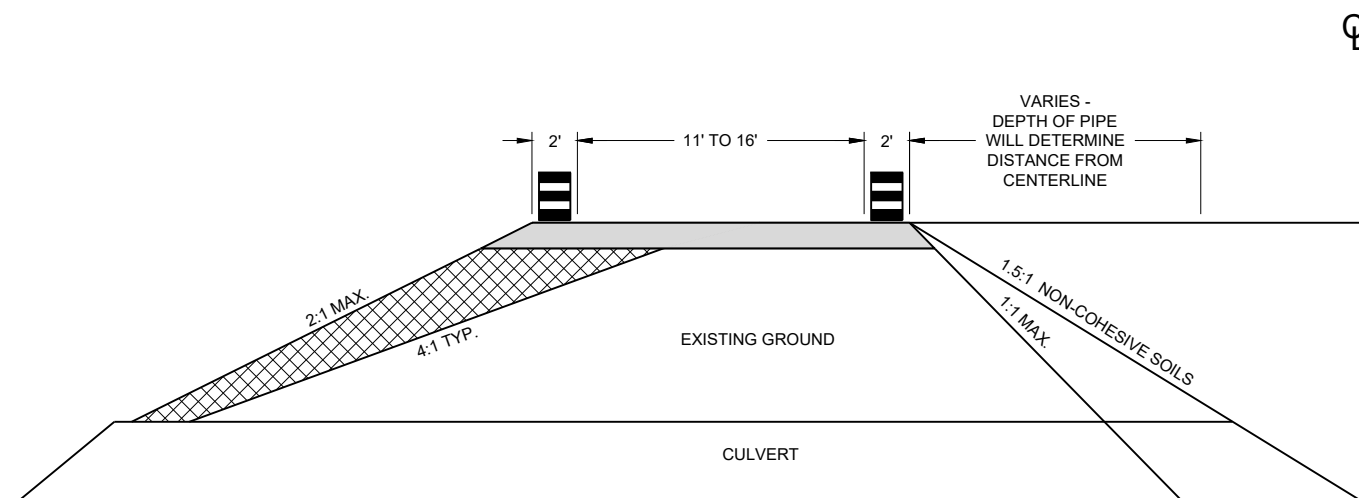


DETAIL FOR SIGNING ON LOOSE GRAVEL
OR CHIP SEALED SURFACES

TRAFFIC CONTROL
SIGNING ON ROADWAYS
WITH LOOSE GRAVEL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA



CROSS SECTION

GENERAL NOTES

USE 1:1 FOR COHESIVE CLAYS AND SILTS, LOAMS, SANDY CLAYS AND ANGULAR GRAVEL SOILS.
USE 1.5:1 FOR NON-COHESIVE SOILS.

THE TAPER SHOULD EXTEND ACROSS THE SHOULDER UNLESS DOING SO WOULD GREATLY CONFLICT WITH THE WORK OPERATION.






ALL LANE CLOSURE SIGNS SHALL BE REMOVED OR COVERED AND ALL DEVICES REMOVED BEYOND THE SHOULDER WHEN WORK IS NOT IN PROGRESS AND THE LANE IS RESTORED TO A SAFE OPERATING CONDITION.

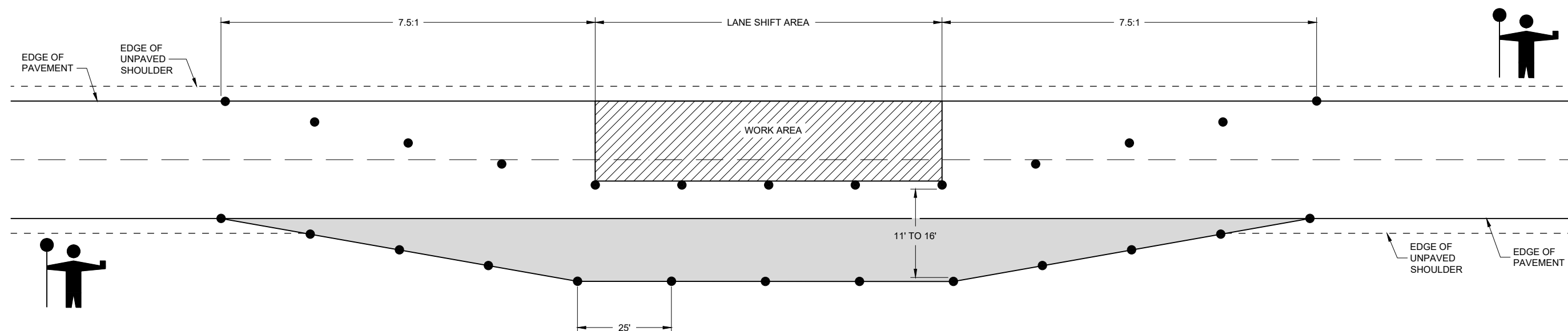
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

USE WITH SDD 15C12 "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATIONS"

USE WITH SDD 15D45 "SIGNING ON ROADWAYS WITH LOOSE GRAVEL"

LEGEND

- | | |
|---|--|
|  | DRUM WITHOUT WARNING LIGHT |
|  | 6" BASE AGGREGATE DENSE 1 1/4" -INCIDENTAL TO LANE SHIFT ITEM |
|  | FILL - INCIDENTAL TO LANE SHIFT ITEM |
|  | WORK AREA |
|  | FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE
FASTENED ON SUPPORT STAFF |



LANE SHIFT IN FLAGGING OPERATION

TRAFFIC CONTROL, TEMPORARY LANE SHIFT DURING CULVERT WORK

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER


FHWA


WORK ZONE ENGINEER


LEGEND


- V1

WORK VEHICLE
- V2

SHADOW VEHICLE
- 

TRUCK MOUNTED ATTENUATOR (TMA)
- 

FLASHING ARROW PANEL (CAUTION)
- 

WORK AREA
- 

DIRECTION OF TRAFFIC

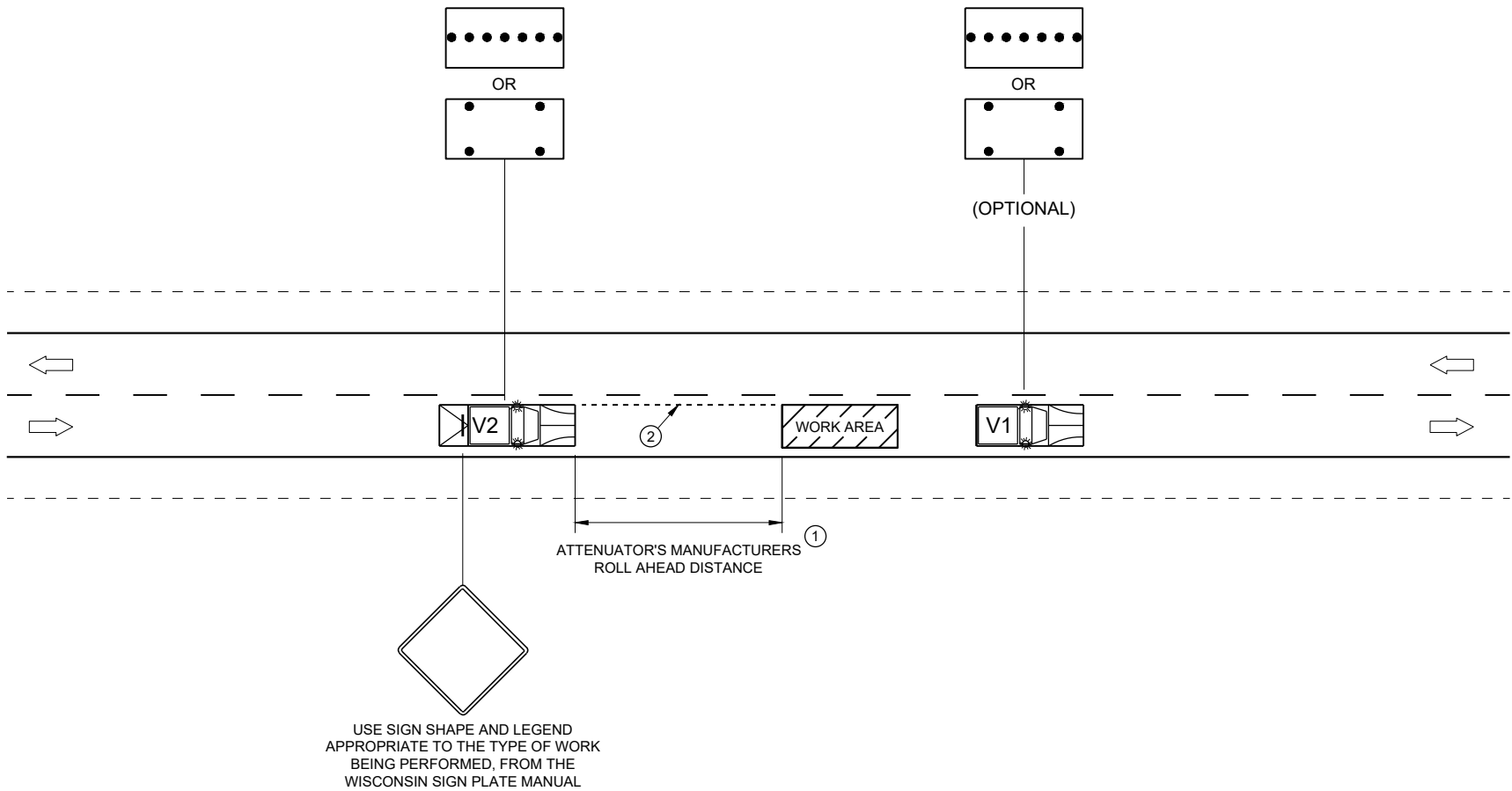
POSTED SPEED PRIOR TO WORK STARTING (MPH)	DECISION SIGHT DISTANCE (D)
0 - 25	550'
30	550'
35	700'
40	700'
45	900'
50	900'
55	1200'

GENERAL NOTES

- ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.
- MOBILE IS WORK THAT MOVES CONTINUOUSLY OR MOVES AT LEAST THE DECISION SIGHT DISTANCE EVERY 15 MINUTES.
- ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.
- ALL ARROW PANELS SHALL BE REAR FACING, TYPE "B" OR "C", AND DISPLAYING THE FLASHING CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.
- USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.
- ①

DISTANCE BETWEEN VEHICLES MAY INCREASE FROM THE ATTENUATOR'S ROLL AHEAD BASED ON TERRAIN, SIGHT DISTANCE, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ②

ALIGN LEFT SIDE OF SHADOW VEHICLE WITH EDGE OF WORK AREA.

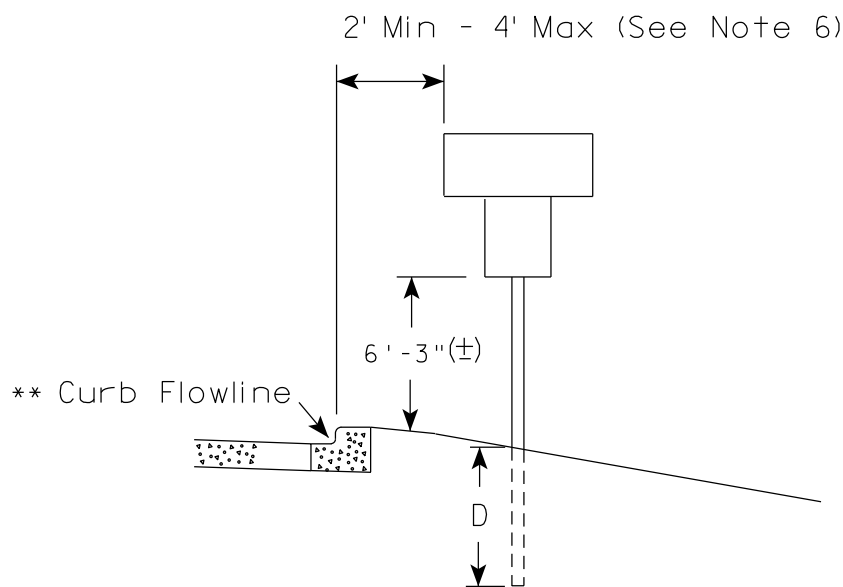


TRAFFIC CONTROL,
MOBILE OPERATIONS ON
AN UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

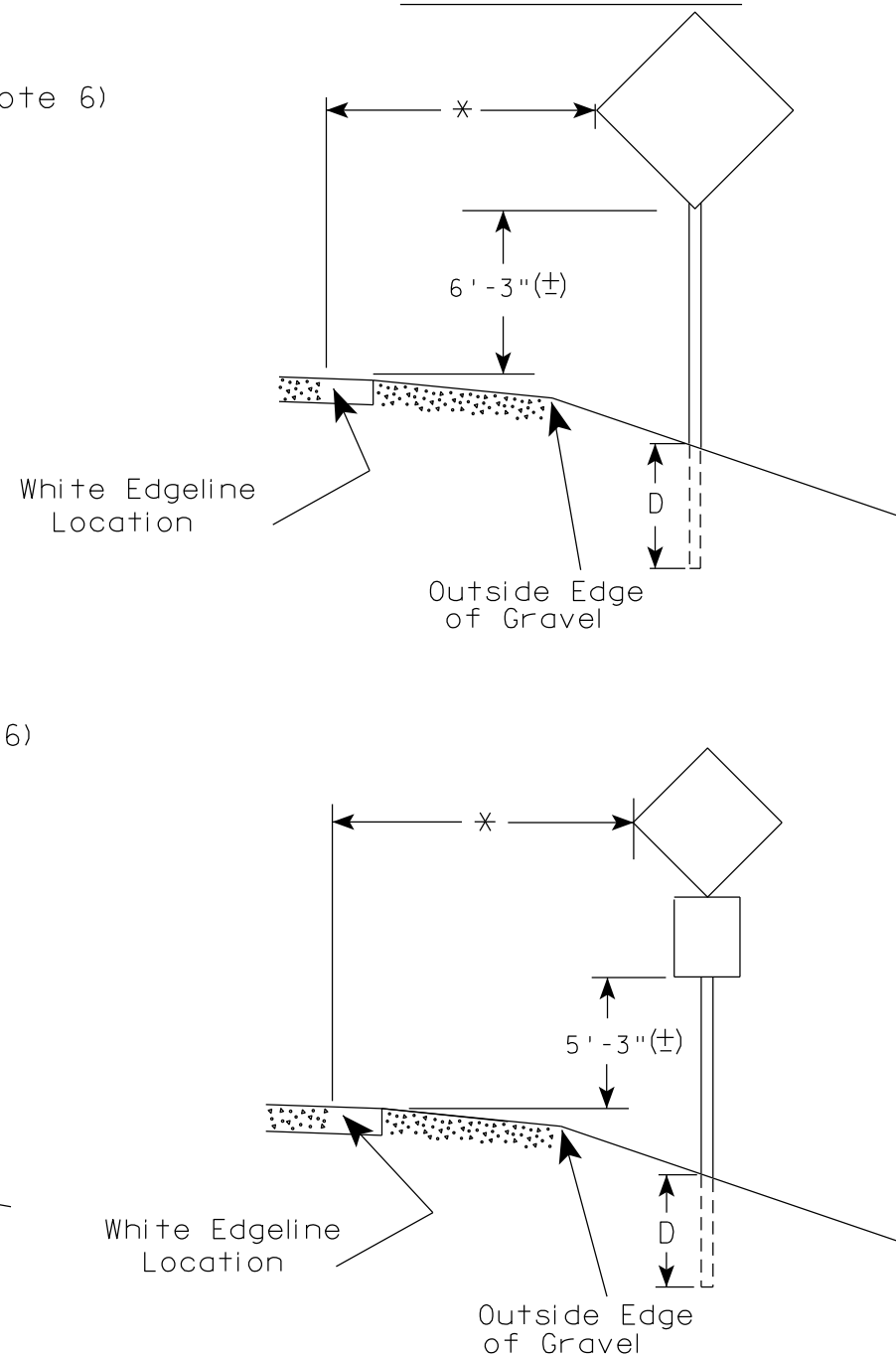
APPROVED
February 2021 /S/ Andrew Heidtke
DATE STATEWIDE WORK ZONE TRAFFIC
SAFETY ENGINEER
FHWA

7



* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

7



* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.

The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (\pm). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (\pm).

3. For expressways and freeways, mounting height is 7'- 3" (\pm) or 6'-3" (\pm) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (\pm).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (\pm) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (\pm) or as directed by the Engineer.

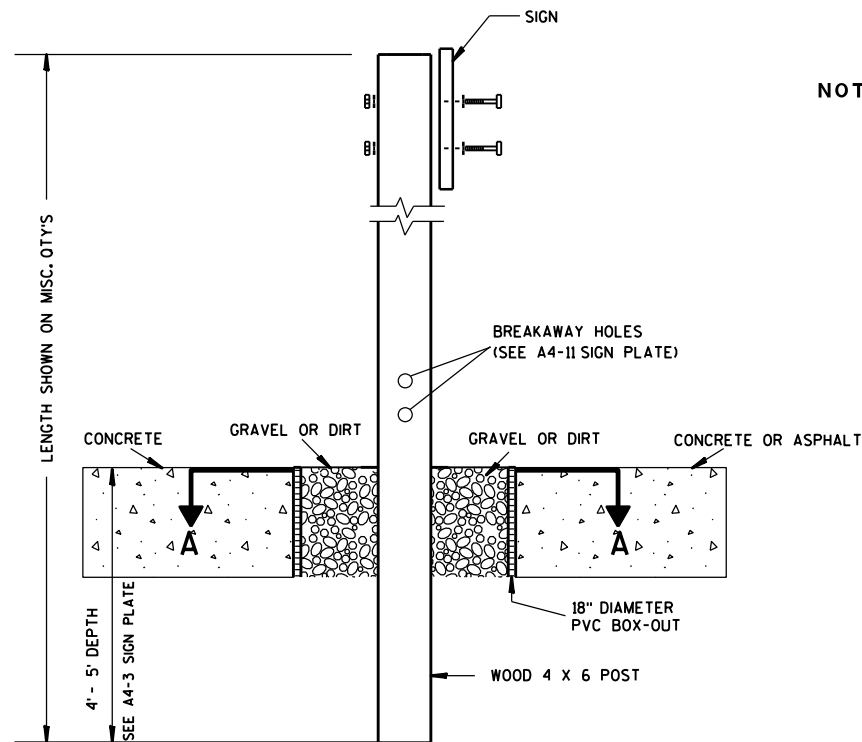
POST EMBEDMENT DEPTH	
Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4"
Greater than 20	5'

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

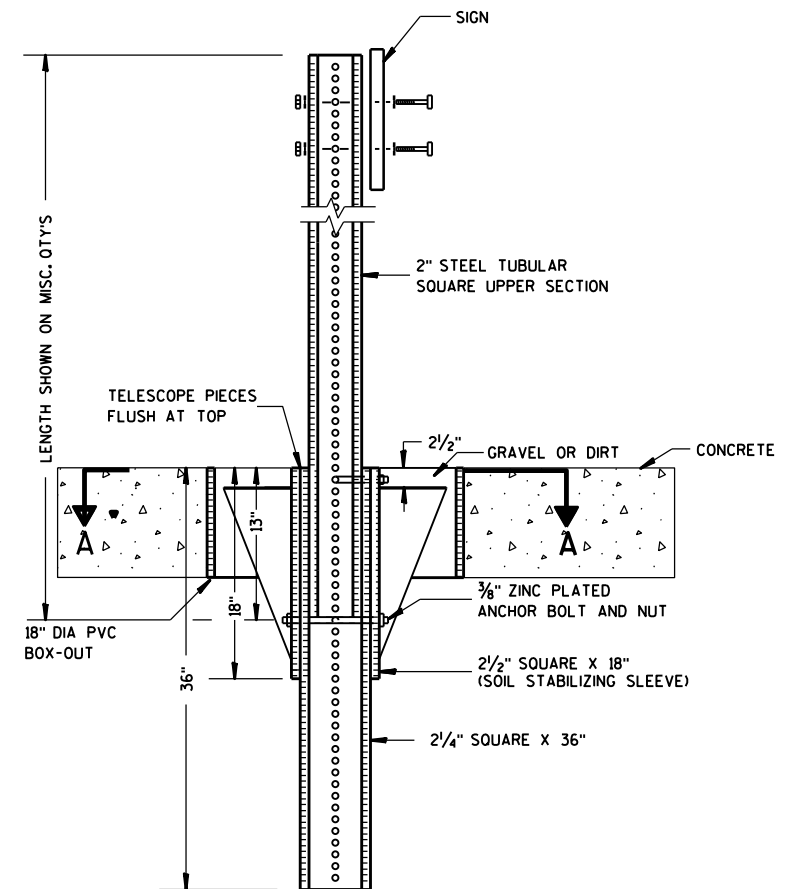
DATE 5/13/2020 PLATE NO. A4-3.22



ELEVATION VIEW

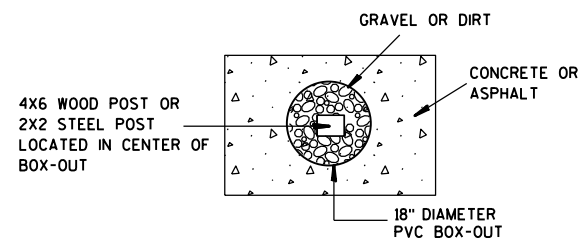
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES: 1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

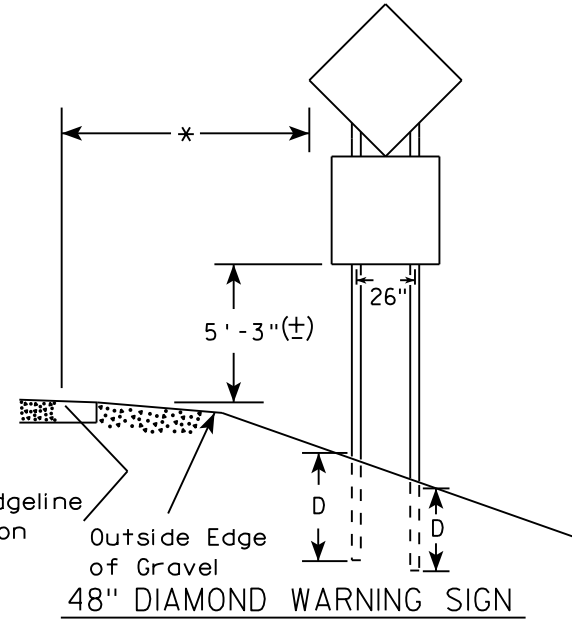
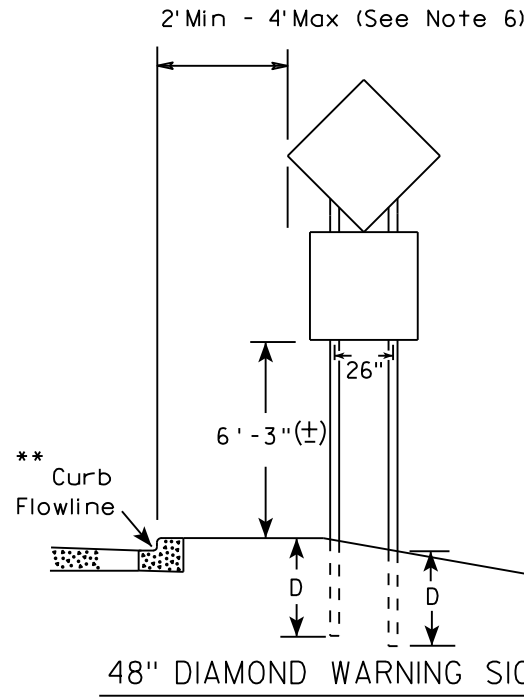
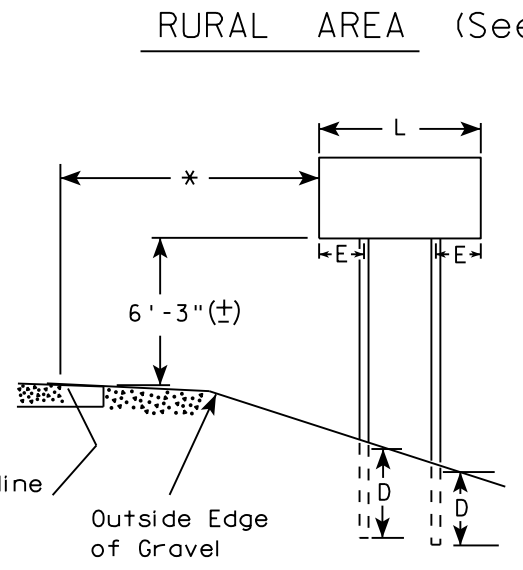
FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST
BOX-OUTS
A4-3B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1



- GENERAL NOTES
1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
 2. See tables below for required number of posts.
 3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
 4. The (±) tolerance for mounting height is 3 inches.
 5. J-Assemblies are considered to be one sign for mounting height.
 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
 7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

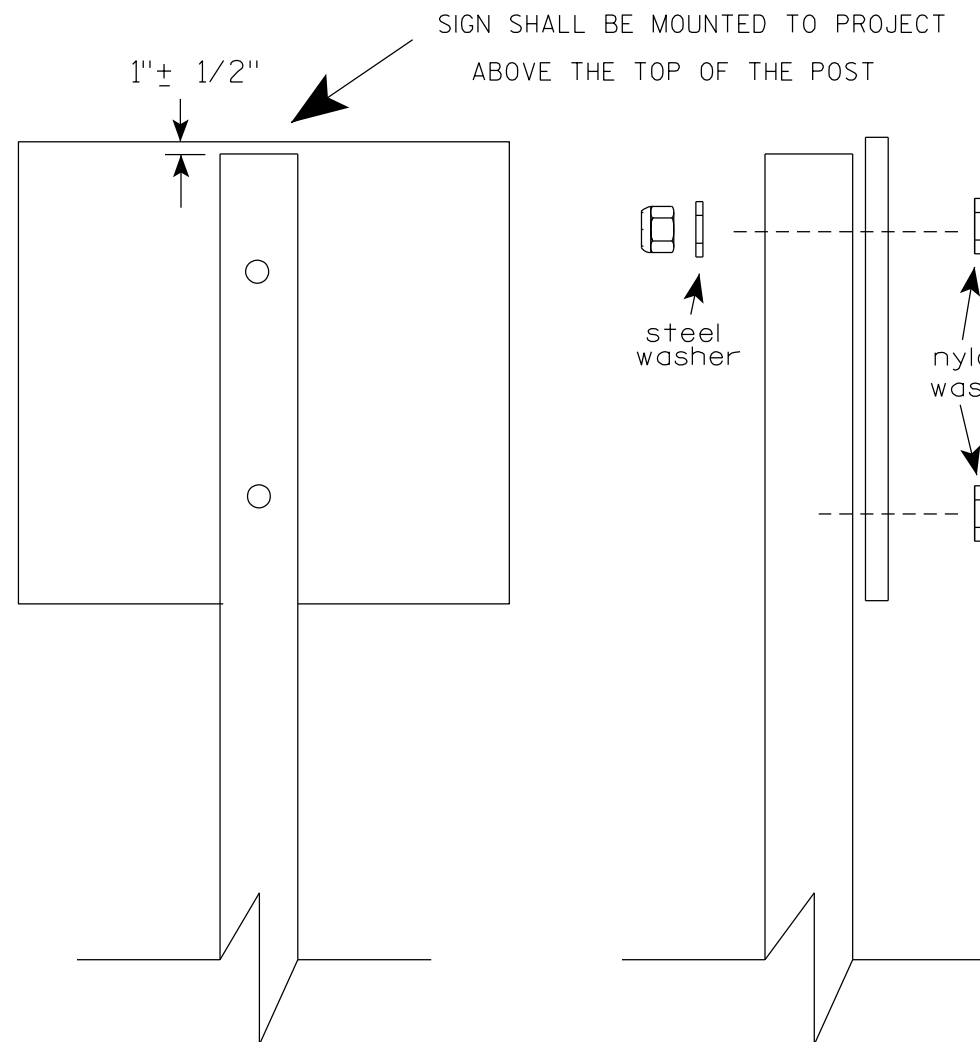
Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION
OF TYPE II SIGNS
ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 8/21/17 PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

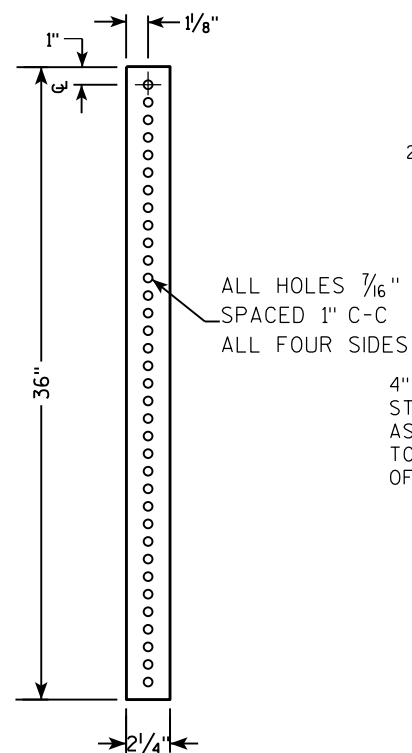
Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS - $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)
- $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
- $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
- O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON

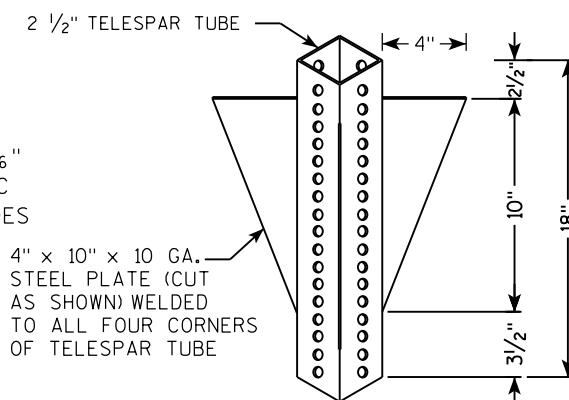
* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 4/1/2020	PLATE NO. A4-8.9

**2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH**



**2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH**



TECHNICAL DRAWING OF A VERTICAL SIGN POST ASSEMBLY.

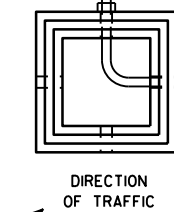
Labels and Dimensions:

- 18" DIA SCHEDULE 40 PVC BOX-OUT**: The base of the post.
- 36"**: Total height of the post assembly.
- 18"**: Height of the box-out section.
- 13"**: Height of the gravel/dirt section.
- 2 1/2" GRAVEL OR DIRT**: The base layer of the box-out.
- 2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)**: The sleeve around the post in the box-out.
- 2 1/4" SQUARE X 36"**: The main post section.
- 2" STEEL TUBULAR SQUARE UPPER SECTION**: The upper part of the post.
- ALL HOLES 7/16" SPACED 1" C-C ALL FOUR SIDES**: Specification for the post holes.
- 3/8" ZINC PLATED CORNER ANCHOR BOLT AND NUT**: Hardware for the sleeve.
- 3/16" ZINC PLATED ANCHOR BOLT AND NUT**: Hardware for the post.
- SIGN**: The sign plate at the top.
- SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL**: Reference to a sign plate for hardware details.
- TELESCOPE PIECES FLUSH AT TOP**: Note about the top of the post sections.

TECHNICAL DRAWING OF A SIGNPOST ASSEMBLY:

- TELESCOPE PIECES FLUSH AT TOP**: Indicated by a dimension line on the left.
- 2" STEEL TUBULAR SQUARE UPPER SECTION**: The main vertical support.
- ALL HOLES $\frac{7}{16}$ " SPACED 1" C-C ALL FOUR SIDES**: Specification for the perforations in the upper section.
- SIGN**: The top horizontal component.
- SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL**: Reference to a sign plate for hardware details.
- 3/8" ZINC PLATED CORNER ANCHOR BOLT AND NUT**: Hardware used to secure the post to the base.
- 1"**: Dimension for the offset of the anchor bolt from the post face.
- 3/8" ZINC PLATED ANCHOR BOLT AND NUT**: Hardware used to secure the post to the base.
- 2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)**: The base section of the post.
- 2 1/4" SQUARE X 36"**: The base section of the post.
- 36"**: Total height of the base section.
- 18"**: Height of the upper part of the base section.
- 12"**: Height of the lower part of the base section.
- A**: Downward arrows indicating load or weight.

3/8" ZINC PLATED CORNER
ANCHOR BOLT AND NUT



SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthaeus R. Rausch

for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

PROJECT NO:

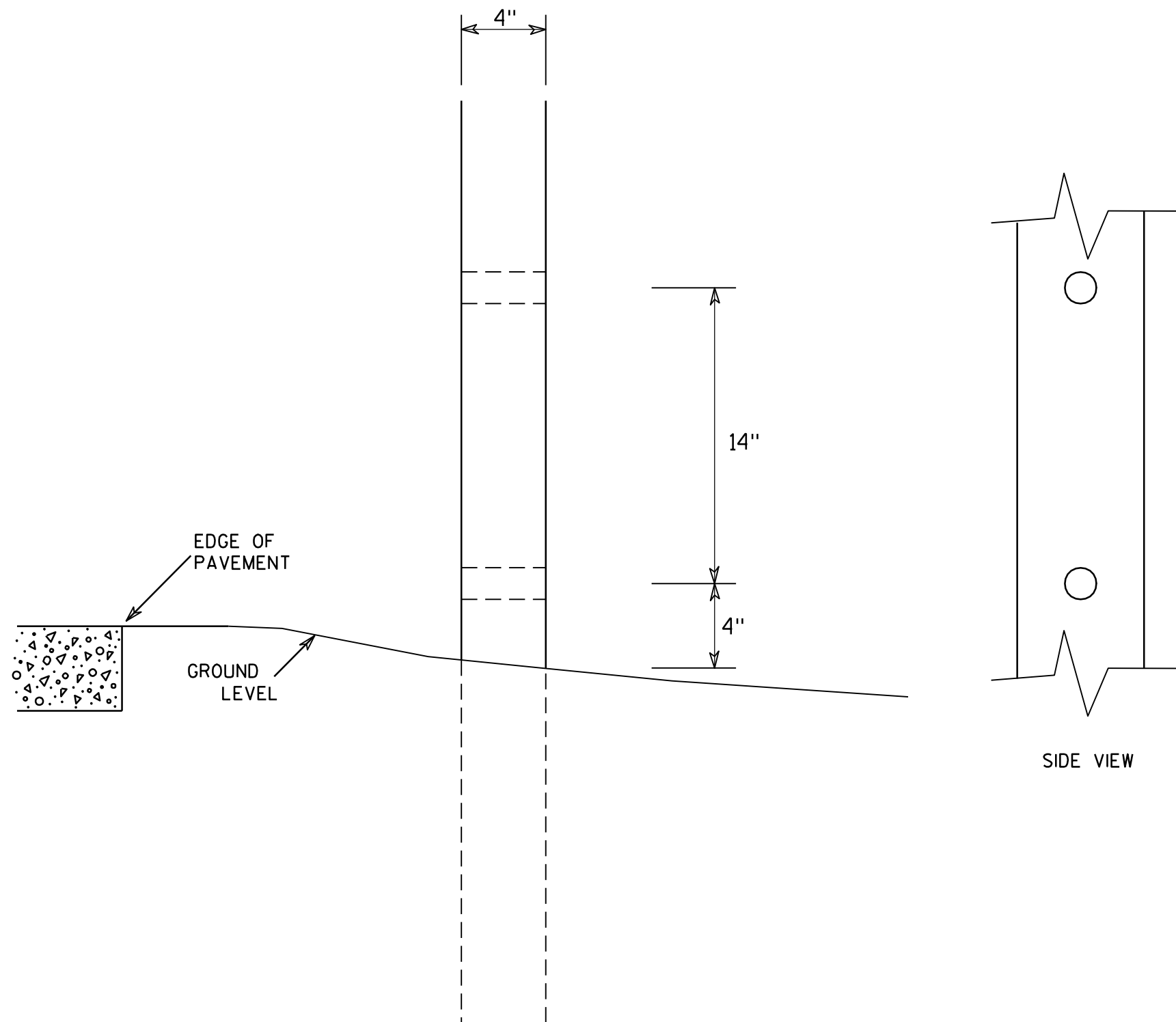
HWY:

COUNTY:

SHEET NO:

E

7

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

**4 X 6 WOOD POST
MODIFICATIONS**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Chester J. Spang
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

PROJECT NO:

HWY:

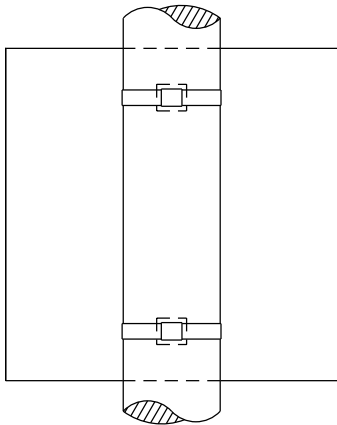
COUNTY:

SHEET NO:

E

BANDING

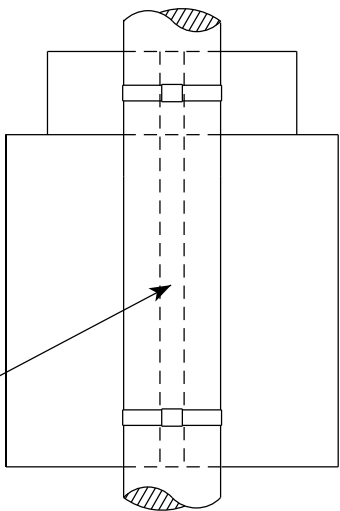
SINGLE SIGN



GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be $\frac{3}{4}$ " in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

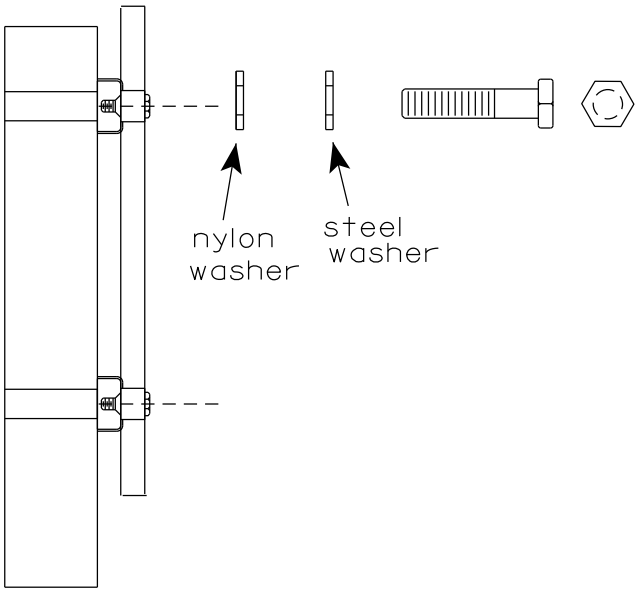
"J" ASSEMBLY



CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET

SEE DETAIL B

WASHER PLACEMENT

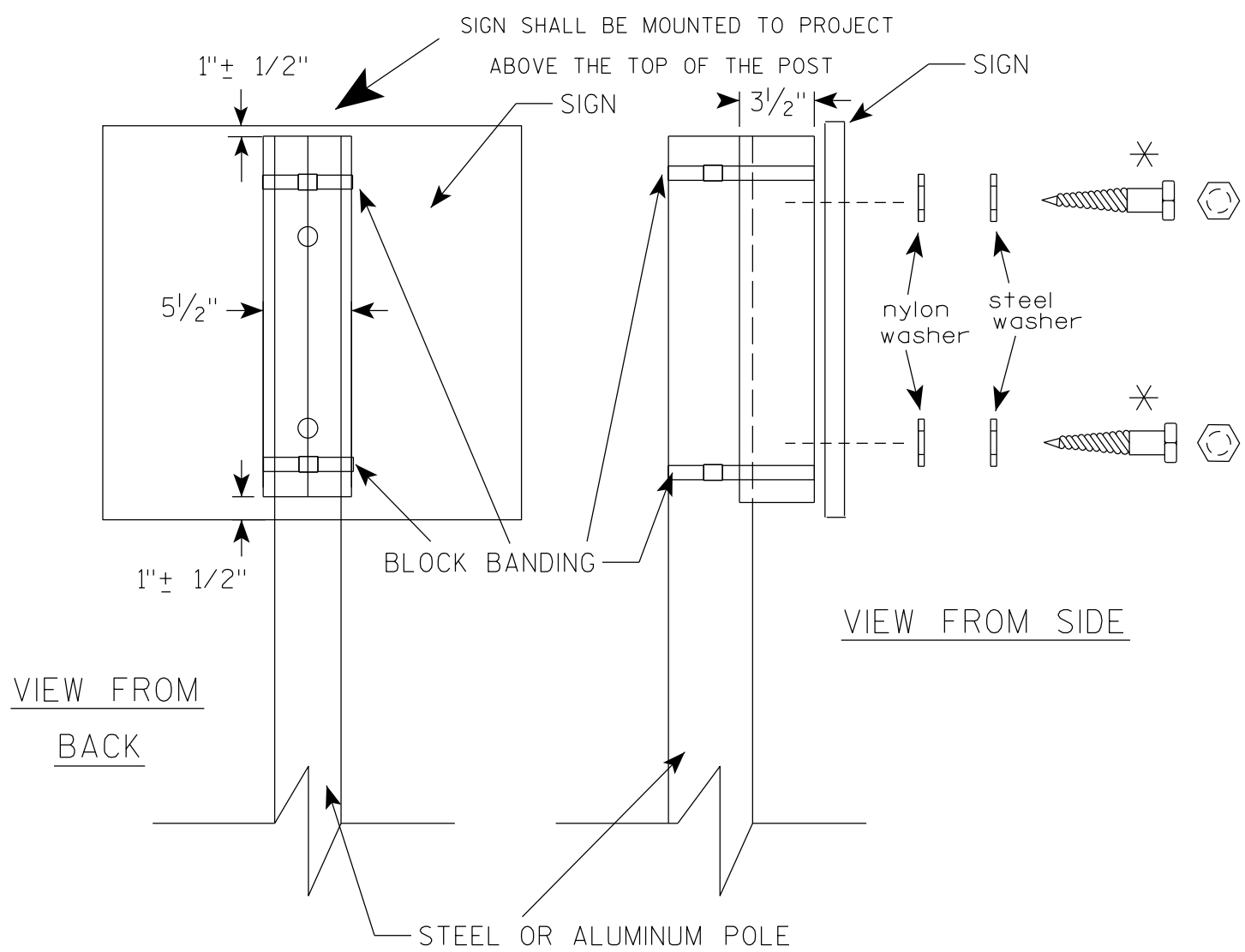


WASHERS (ALL POSTS) -
1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

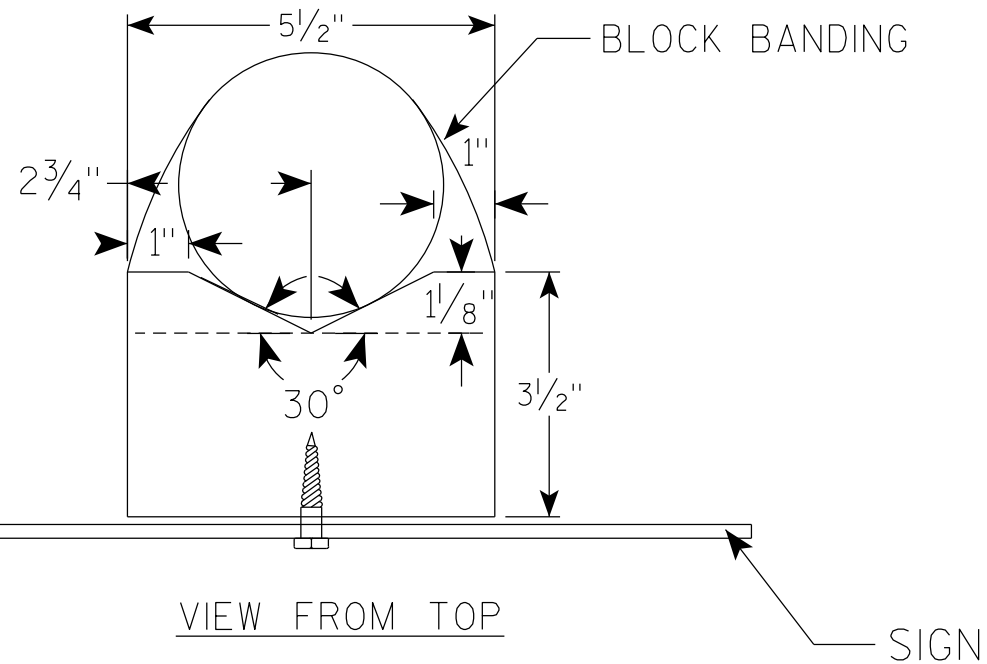
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 6/10/19 PLATE NO. A5-9.4



VIEW FROM
BACK

VIEW FROM SIDE



VIEW FROM TOP

GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, $\frac{3}{4}$ " WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE $\frac{1}{4}$ " O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ "
8. NYLON WASHERS SHALL BE $\frac{1}{4}$ " O.D. X $\frac{3}{8}$ " I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✱ LAG BOLTS SHALL BE $\frac{3}{8}$ " X $2\frac{1}{2}$ "

BLOCK BANDING DETAIL
(V-BLOCK OPTION)

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 4/19/2022 PLATE NO. A5-10.3

PROJECT NO:

SHEET NO:

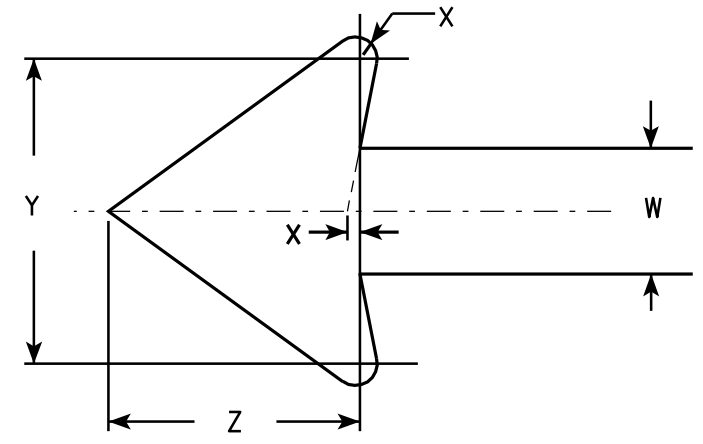
E



R7-53

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Red
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1, 3 and 4 are series C, line 2 is series B.
6. R7-53D (double arrow)
R7-53L (left arrow)
R7-53R (right arrow)



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/8	3/8	3/8	3	1 7/8	2	7/8	5/8	1 1/2	2 1/2	2	2	4 7/8	4 7/8	2 5/8	1 3/4	1/4	4 1/2	2 3/8	3 7/8	3/4	1/8	1 3/4	1 1/2	1.5
2S	18	24	1 1/8	3/8	1/2	4	2 1/2	2 1/2	1 1/4	1	2	3 1/4	2 3/4	2 5/8	7 1/8	7	3 3/8	1 1/2	5/8	5 3/8	3	5 7/8	1 1/8	1/4	2 5/8	2 1/4	3.0
2M	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	4	2	5/8	6 5/8	3 5/8	7 3/4	1 1/2	1/4	3 1/2	3	5.0
3	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	4	2	5/8	6 5/8	3 5/8	7 3/4	1 1/2	1/4	3 1/2	3	5.0
4																											
5																											

STANDARD SIGN R7-53	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> for State Traffic Engineer
DATE 3/31/2011	PLATE NO. R7-53.6

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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GUARDRAIL STA 50366-50785 - Alignment - US 8

STATION	AREA (SF)		Incremental Vol (CY) (Unadjusted)		Cumulative Vol (CY)		
	Cut	Fill	Cut	Fill	Expanded		Mass Ordinate
					Cut 1.00	Fill 1.25	
503+00	15.00	13.05	0	0	0	0	0
503+50	16.20	25.32	29	36	29	44	-16
503+65.85	16.13	28.75	9	16	38	64	-26
503+90.85	16.71	26.40	15	26	54	96	-43
504+00	16.76	26.07	6	9	59	107	-48
504+15.85	16.73	35.29	10	18	69	130	-61
504+50	17.05	0.87	21	23	90	158	-68
505+00	10.95	119.03	26	111	116	297	-181
505+50	8.76	82.79	18	187	135	531	-396
506+00	8.36	57.89	16	130	150	694	-543
506+44	5.91	61.18	12	97	162	815	-653
507+00	9.93	14.85	16	79	179	913	-735
507+34.6	10.40	58.95	13	47	192	972	-781
507+59.6	9.88	55.88	9	53	201	1,039	-838
507+84.6	9.24	60.34	9	54	210	1,106	-896
508+00	8.88	45.04	5	30	215	1,144	-929

CP STA 49321 - Alignment - US 8

STATION	AREA (SF)		Incremental Vol (CY) (Unadjusted)		Cumulative Vol (CY)		
	Cut	Fill	Cut	Fill	Expanded		Mass Ordinate
					Cut 1.00	Fill 1.25	
492+50	2.65	12.01	0	0	0	0	0
493+00	52.06	14.21	51	24	51	30	20
493+21	61.50	47.37	44	24	95	60	35
493+50	40.00	13.84	55	33	149	101	48
494+00	5.02	2.90	42	15	191	121	70

CP STA 54059 - Alignment - US 8

STATION	AREA (SF)		Incremental Vol (CY) (Unadjusted)		Cumulative Vol (CY)		
	Cut	Fill	Cut	Fill	Expanded		Mass Ordinate
					Cut 1.00	Fill 1.25	
540+00	5.02	29.74	0	0	0	0	0
540+35	60.12	26.71	42	37	42	46	-4
540+50	55.03	46.20	32	20	74	71	3
540+75	54.46	62.67	51	50	125	134	-9
541+00	4.84	42.26	27	49	152	195	-42

CP STA 55900 - Alignment - US 8

STATION	AREA (SF)		Incremental Vol (CY) (Unadjusted)		Cumulative Vol (CY)		
	Cut	Fill	Cut	Fill	Expanded		Mass Ordinate
					Cut 1.00	Fill 1.25	
554+50	5.58	230.95	0	0	0	0	0
555+00	1.27	240.53	6	437	6	546	-539
555+50	0.77	277.86	2	480	8	1,146	-1,137
555+72	0.80	305.33	1	238	9	1,443	-1,434
556+00	1.30	169.54	1	246	10	1,750	-1,741
556+50	0.64	38.73	2	193	12	1,992	-1,980
557+00	0.51	65.61	1	97	13	2,112	-2,099
557+50	0.60	61.05	1	117	14	2,259	-2,245
558+00	5.24	39.74	5	93	19	2,376	-2,356
558+50	53.47	23.96	54	59	74	2,449	-2,376
559+00	99.67	12.84	142	34	215	2,492	-2,276
559+50	13.29	21.80	105	32	320	2,532	-2,212
560+00	17.42	62.28	28	78	348	2,629	-2,281
560+50	22.81	20.15	37	76	386	2,725	-2,339

CP STA 56898 - Alignment - US 8

STATION	AREA (SF)		Incremental Vol (CY) (Unadjusted)		Cumulative Vol (CY)		
	Cut	Fill	Cut	Fill	Expanded		Mass Ordinate
					Cut 1.00	Fill 1.25	
568+50	5.53	10.21	0	0	0	0	0
568+80	61.51	10.55	37	12	37	14	23
569+00	54.87	11.84	43	8	80	25	56
569+15	58.72	12.60	32	7	112	33	79
569+50	4.94	11.42	41	16	153	53	100

CP STA 59398 - Alignment - US 8

STATION	AREA (SF)		Incremental Vol (CY) (Unadjusted)		Cumulative Vol (CY)		
	Cut	Fill	Cut	Fill	Expanded		Mass Ordinate
					Cut 1.00	Fill 1.25	
593+50	3.70	34.03	0	0	0	0	0
593+98	37.70	56.76	37	81	37	101	-64
594+00	38.04	56.80	3	4	40	106	-67
594+50	4.71	6.64	40	59	79	180	-100

CP STA 62643 - Alignment - US 8

STATION	AREA (SF)		Incremental Vol (CY) (Unadjusted)		Cumulative Vol (CY)		
	Cut	Fill	Cut	Fill	Expanded		Mass Ordinate
					Cut 1.00	Fill 1.25	
624+50	5.70	27.92	0	0	0	0	0
625+00	4.04	50.82	9	73	9	91	-82
625+50	4.96	80.55	8	122	17	243	-226
626+00	4.41	69.35	9	139	26	417	-391
626+43	56.84	111.03	49	144	75	596	-521
626+48	59.95	96.83	11	19	86	620	-535
627+00	4.25	58.88	62	150	147	808	-660
627+50	4.40	46.61	8	98	155	930	-774

CP STA 66840 - Alignment - US 8

STATION	AREA (SF)		Incremental Vol (CY) (Unadjusted)		Cumulative Vol (CY)		
	Cut	Fill	Cut	Fill	Expanded		Mass Ordinate
					Cut 1.00	Fill 1.25	
667+00	5.31	20.37	0	0	0	0	0
667+50	5.47	154.65	10	162	10	203	-193
668+00	52.73	160.74	54	292	64	568	-504
668+14	80.74	156.03	35	82	98	670	-572
668+40	54.39	174.02	65	159	164	869	-705
668+80	52.19	272.26	79	331	242	1,282	-1,040
669+00	52.11	238.45	39	189	281	1,519	-1,237
669+50	4.82	119.67	53	332	334	1,933	-1,599
670+00	5.08	77.39	9	182	343	2,161	-1,818

CP STA 70812 - Alignment - US 8

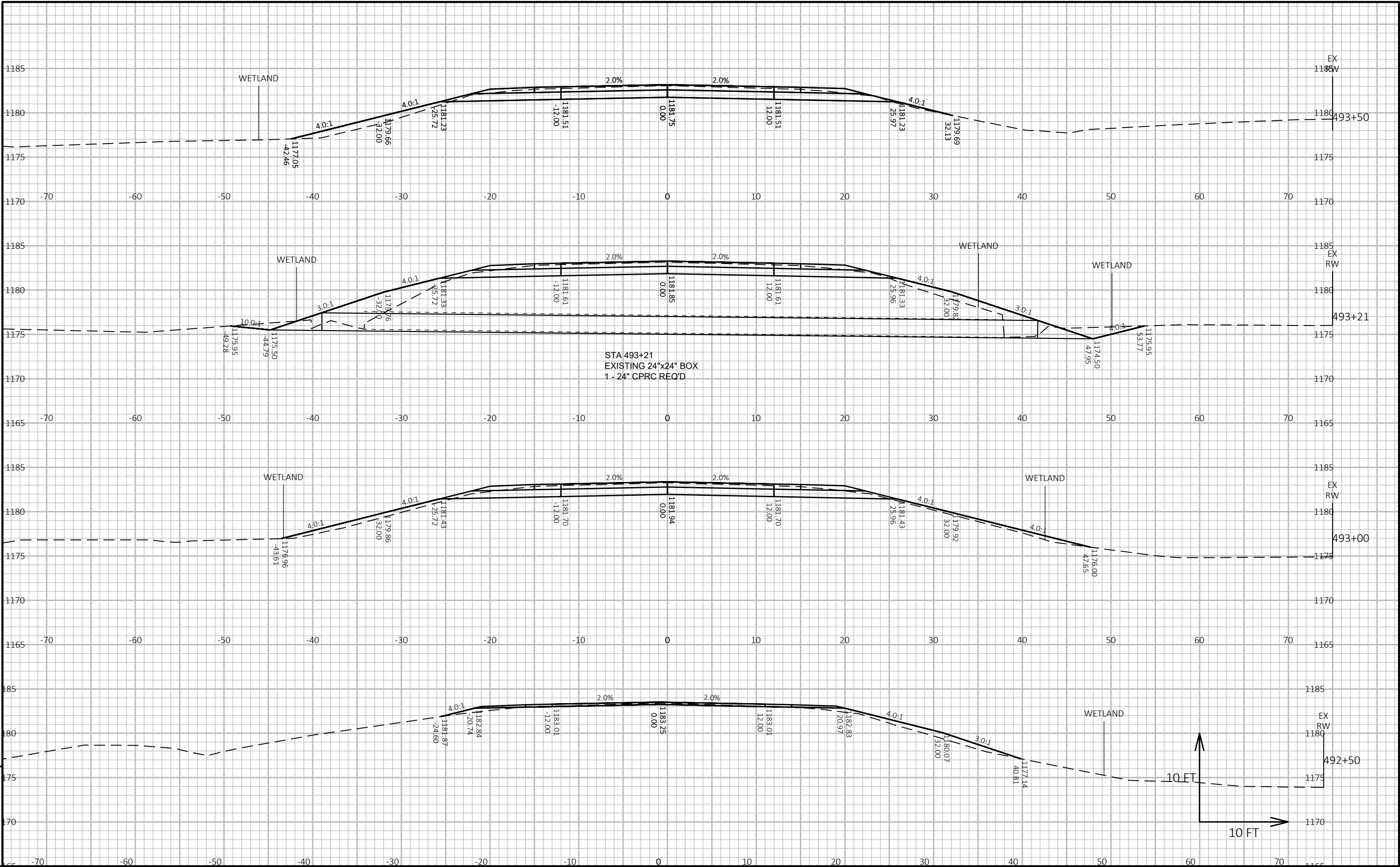
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	Cut	Fill	Cut	Fill	Expanded		Mass Ordinate
					Cut 1.00	Fill 1.25	
707+50	6.32	15.16	0	0	0	0	0
708+00	53.66	45.99	56	57	56	71	-15
708+12	53.47	41.12	24	19	79	95	-16
708+50	7.37	18.93	43	42	122	148	-26

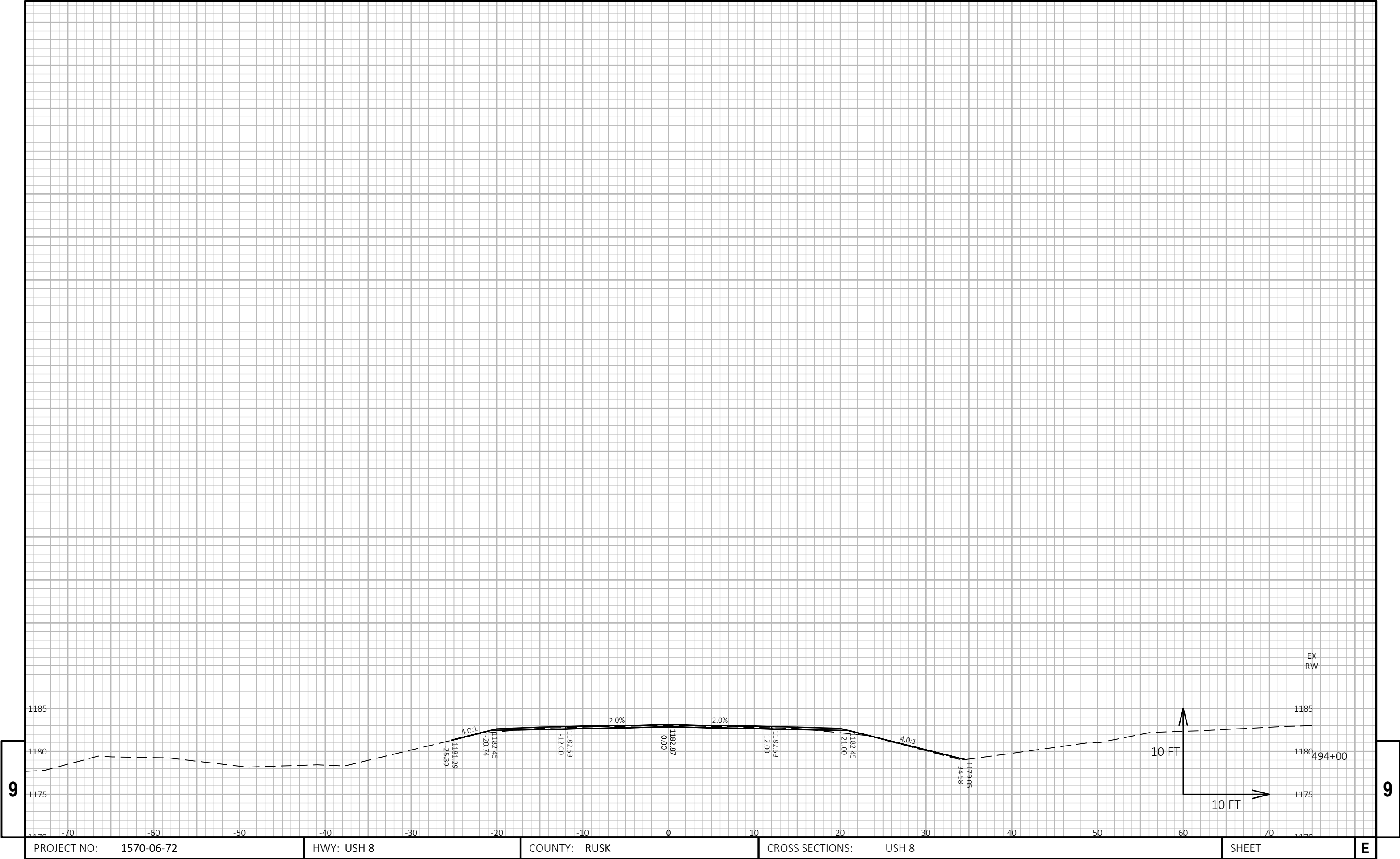
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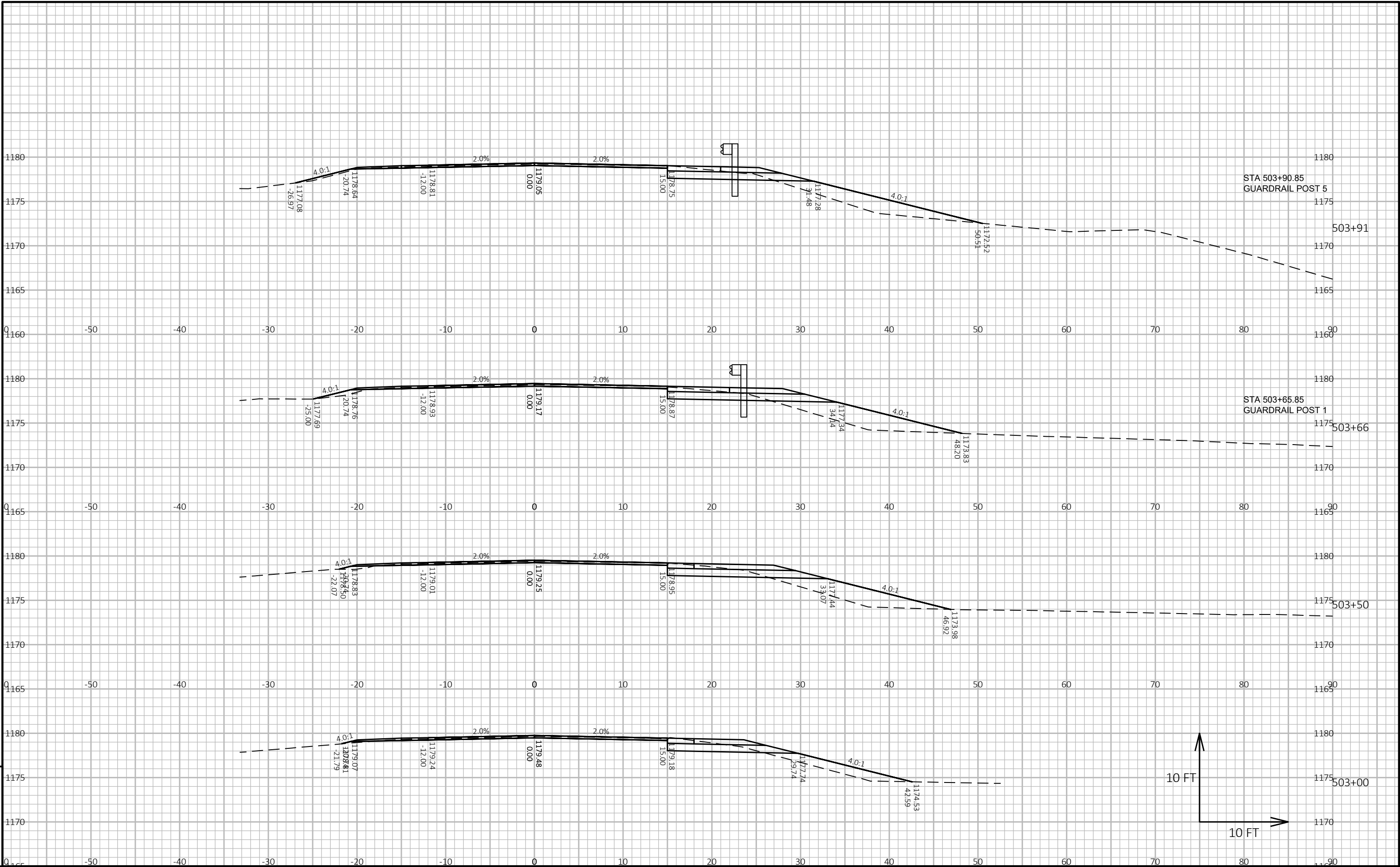
STATION	AREA (SF)		Incremental Vol (CY) (Unadjusted)		Cumulative Vol (CY)		
	Cut	Fill	Cut	Fill	Expanded		Mass Ordinate
					Cut 1.00	Fill 1.25	
821+50	4.10	22.93	0	0	0	0	0
822+00	52.78	23.67	53	43	53	54	-1
822+09	52.54	41.53	18	11	70	68	3
822+50	3.49	28.85	43	53	113	134	-22
823+00	4.48	18.30	7	44	120	189	-69

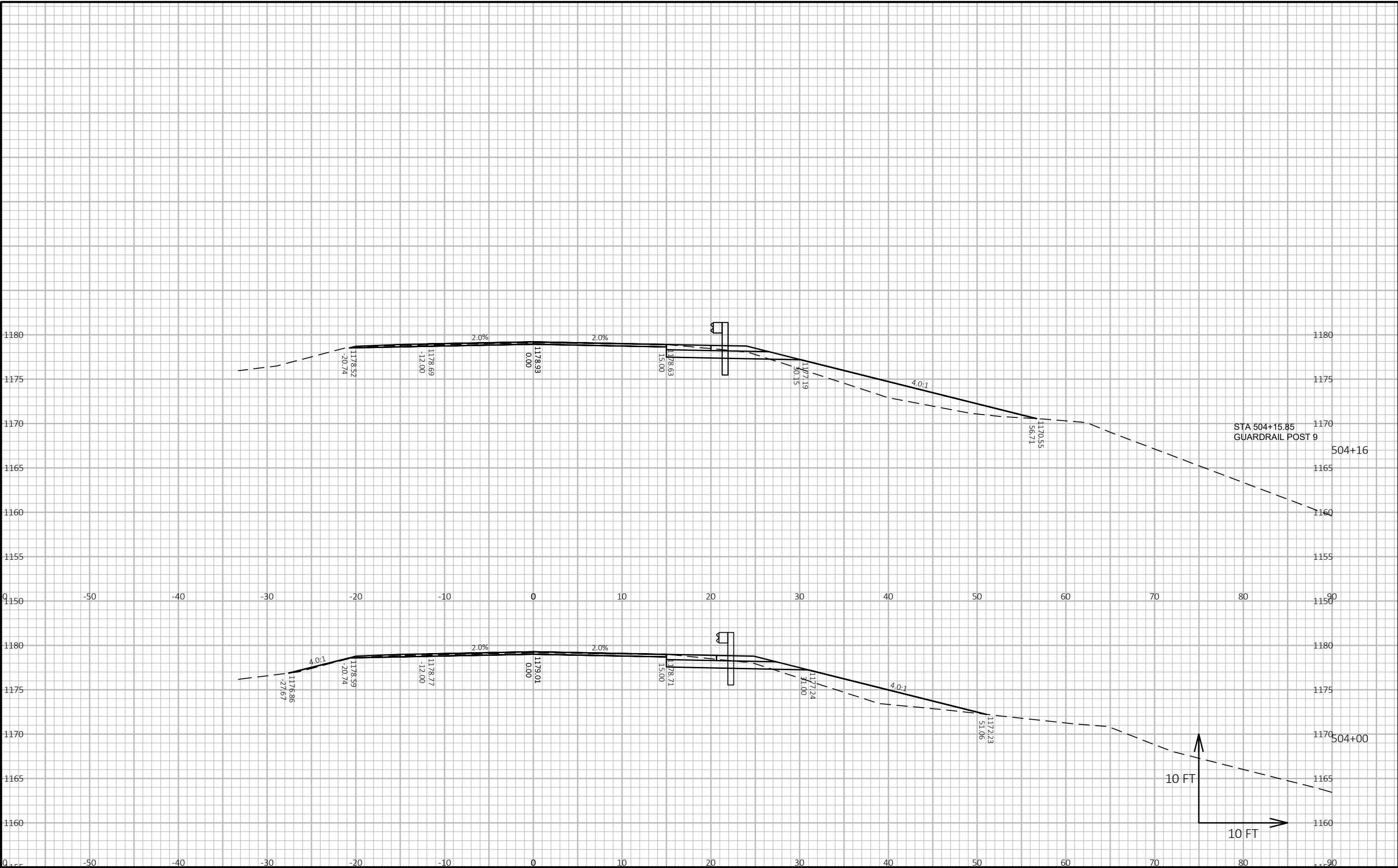
CP STA 93800 - Alignment - US 8

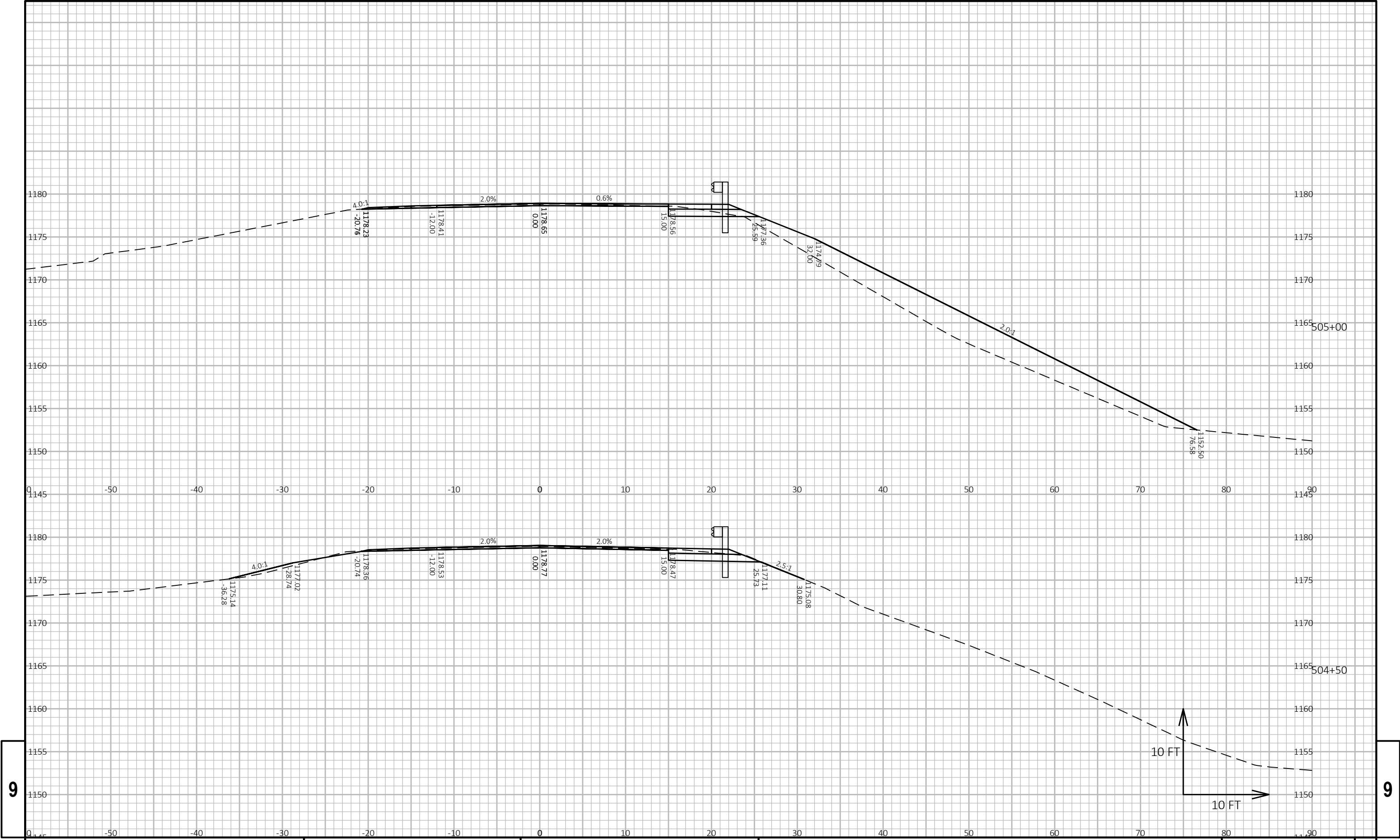
STATION	AREA (SF)		Incremental Vol (CY) (Unadjusted)		Cumulative Vol (CY)		
	Cut	Fill	Cut	Fill	Expanded		Mass Ordinate
					Cut 1.00	Fill 1.25	
937+50	3.34	3.70	0	0	0	0	0
938+00	55.10	6.03	54	9	54	11	43
938+50	4.67	25.46	55	29	109	48	62





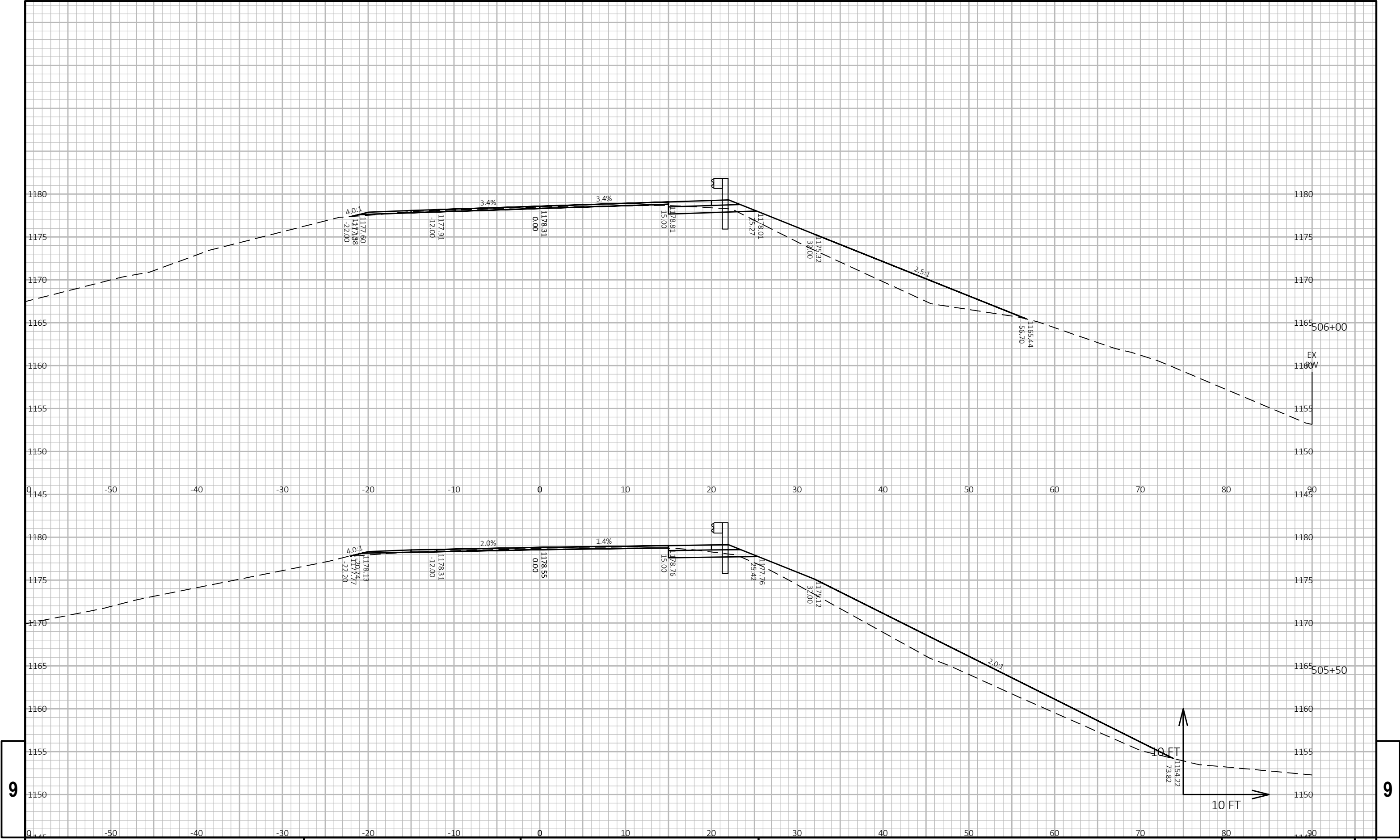


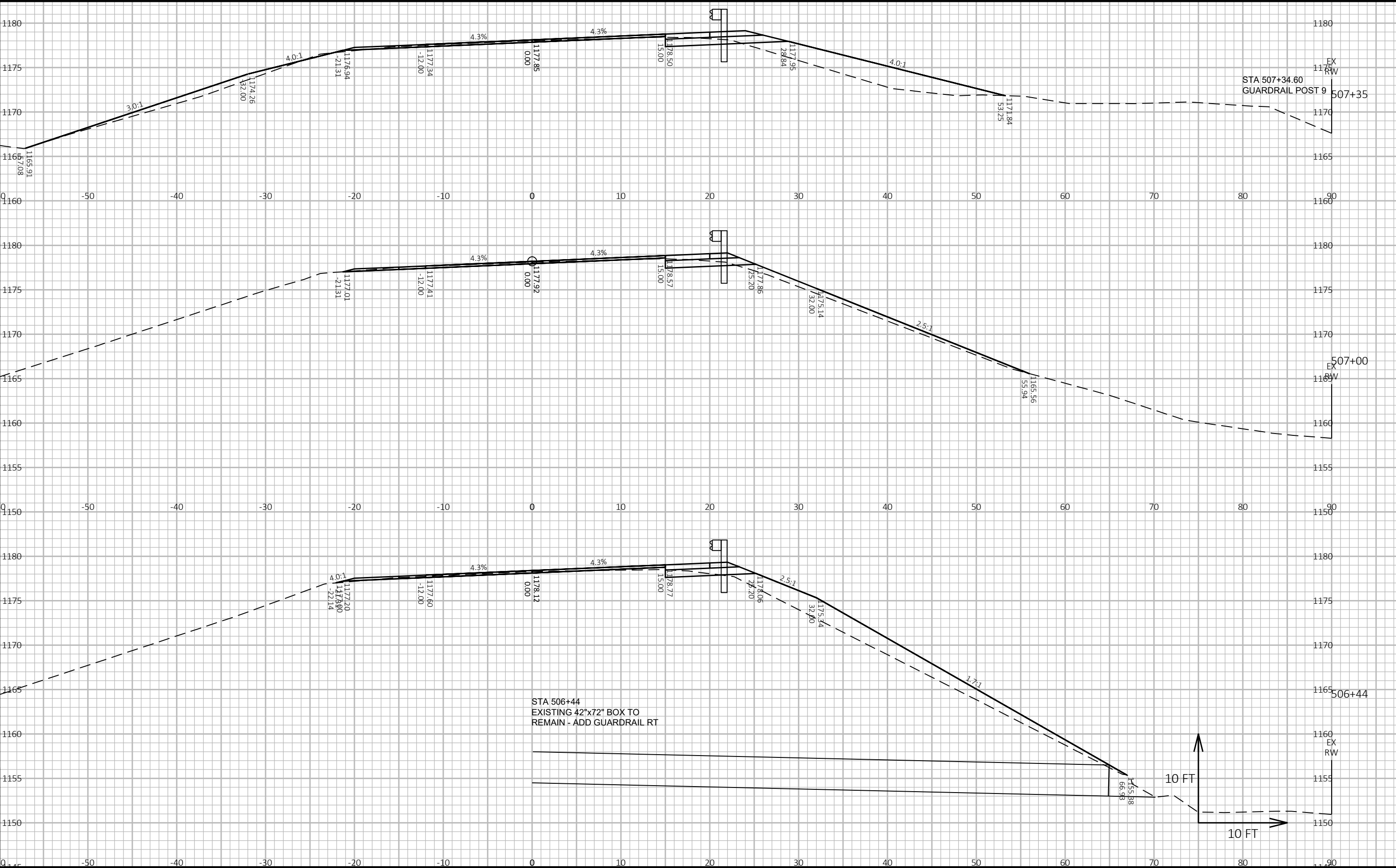


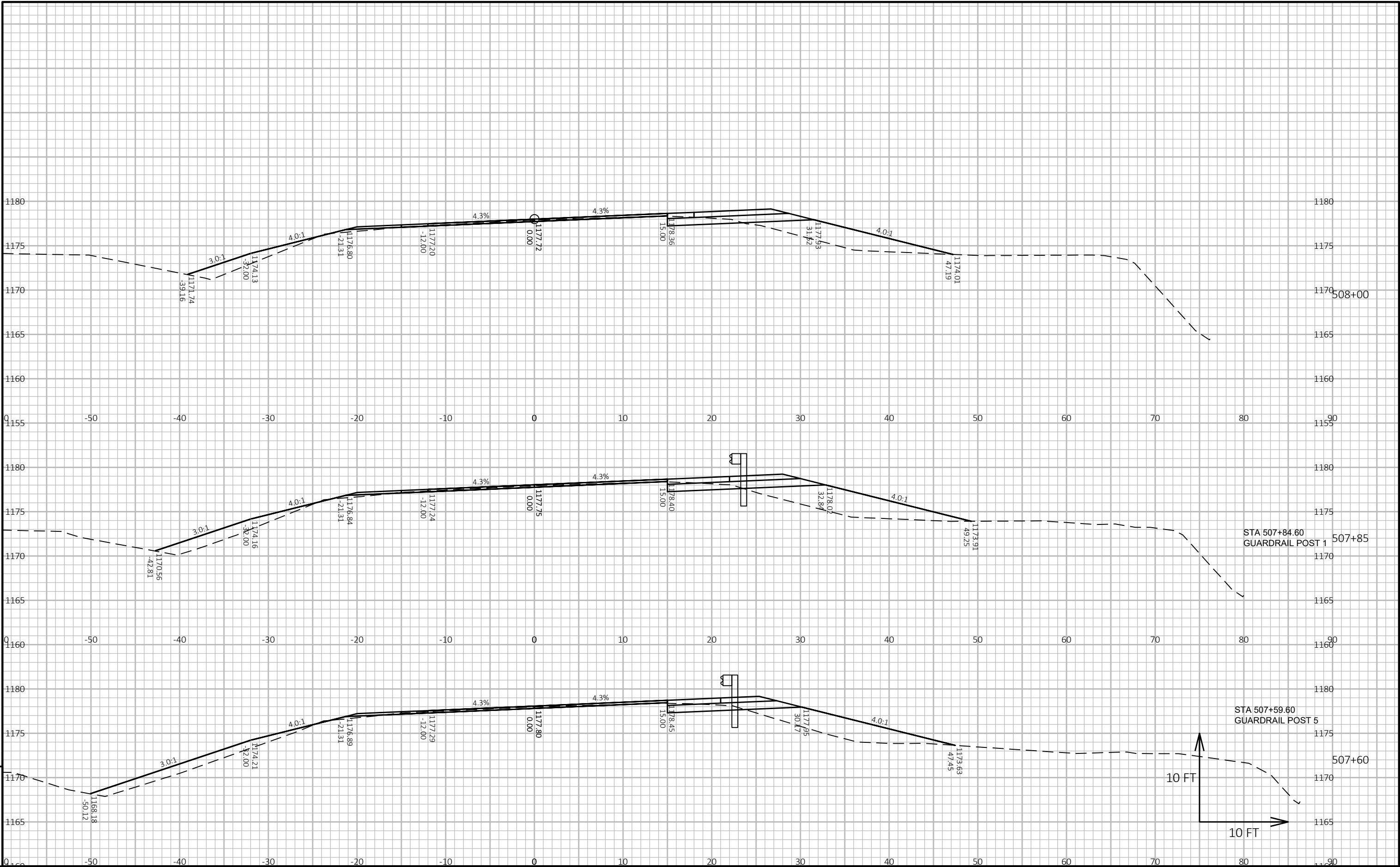


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9

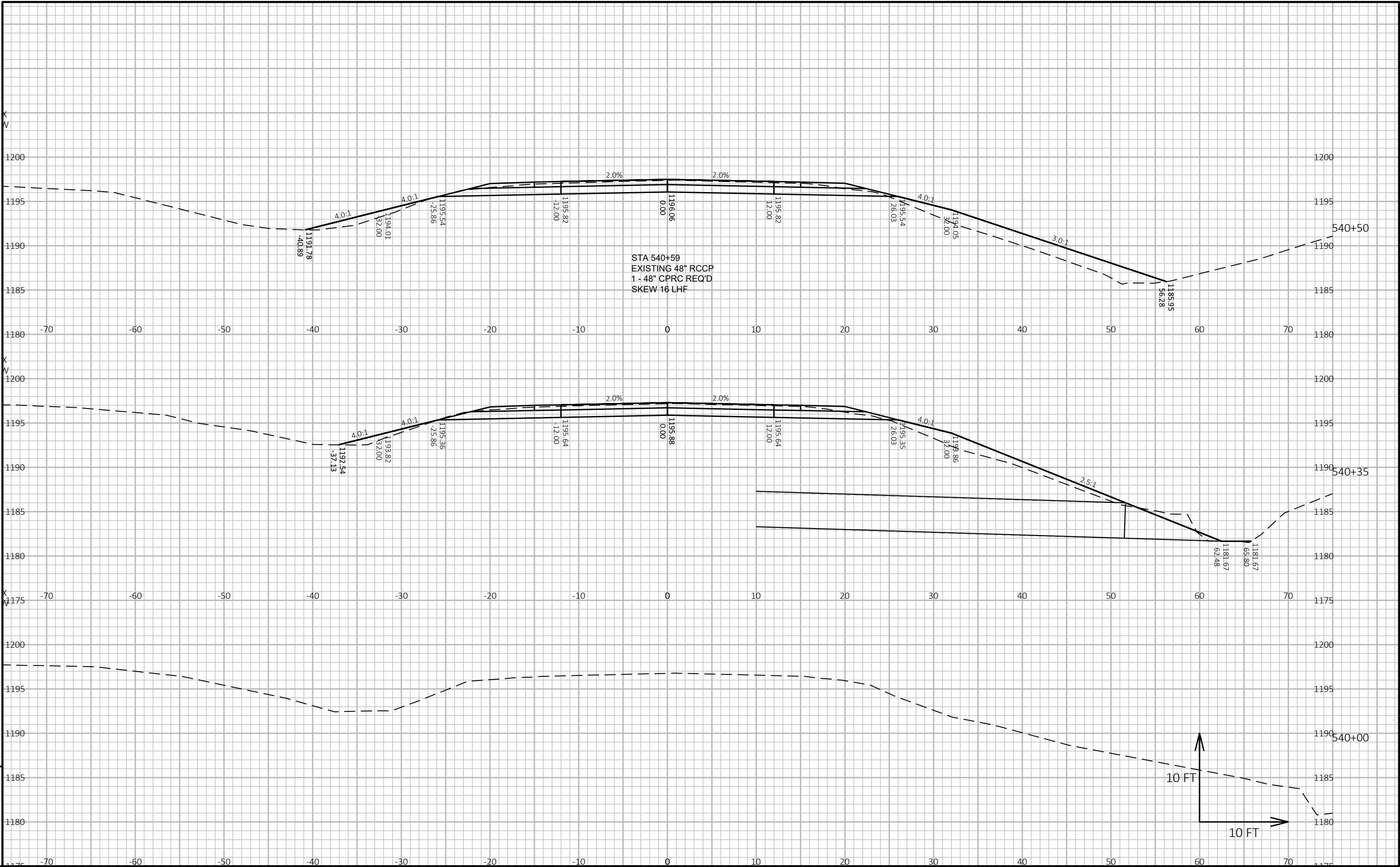


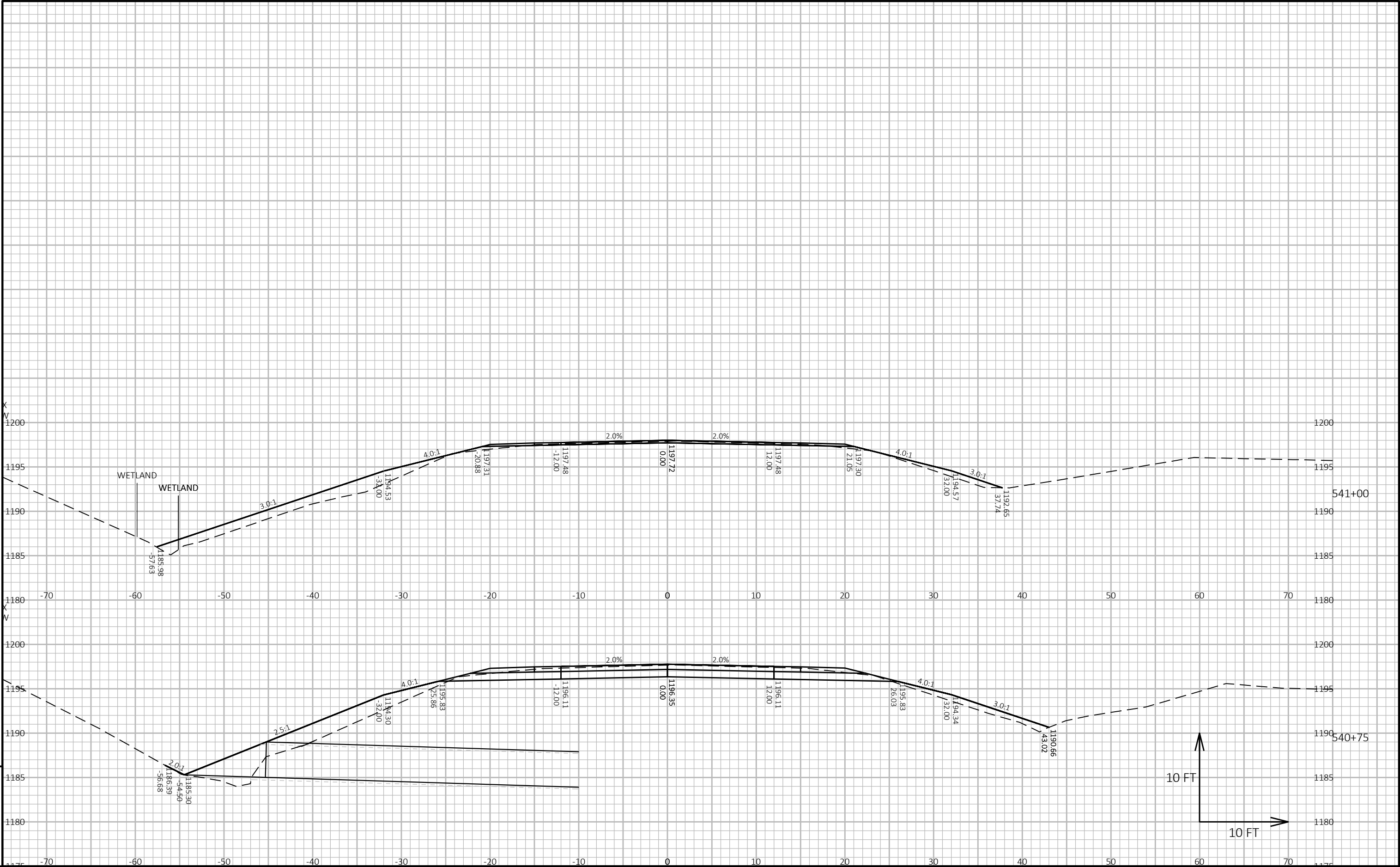


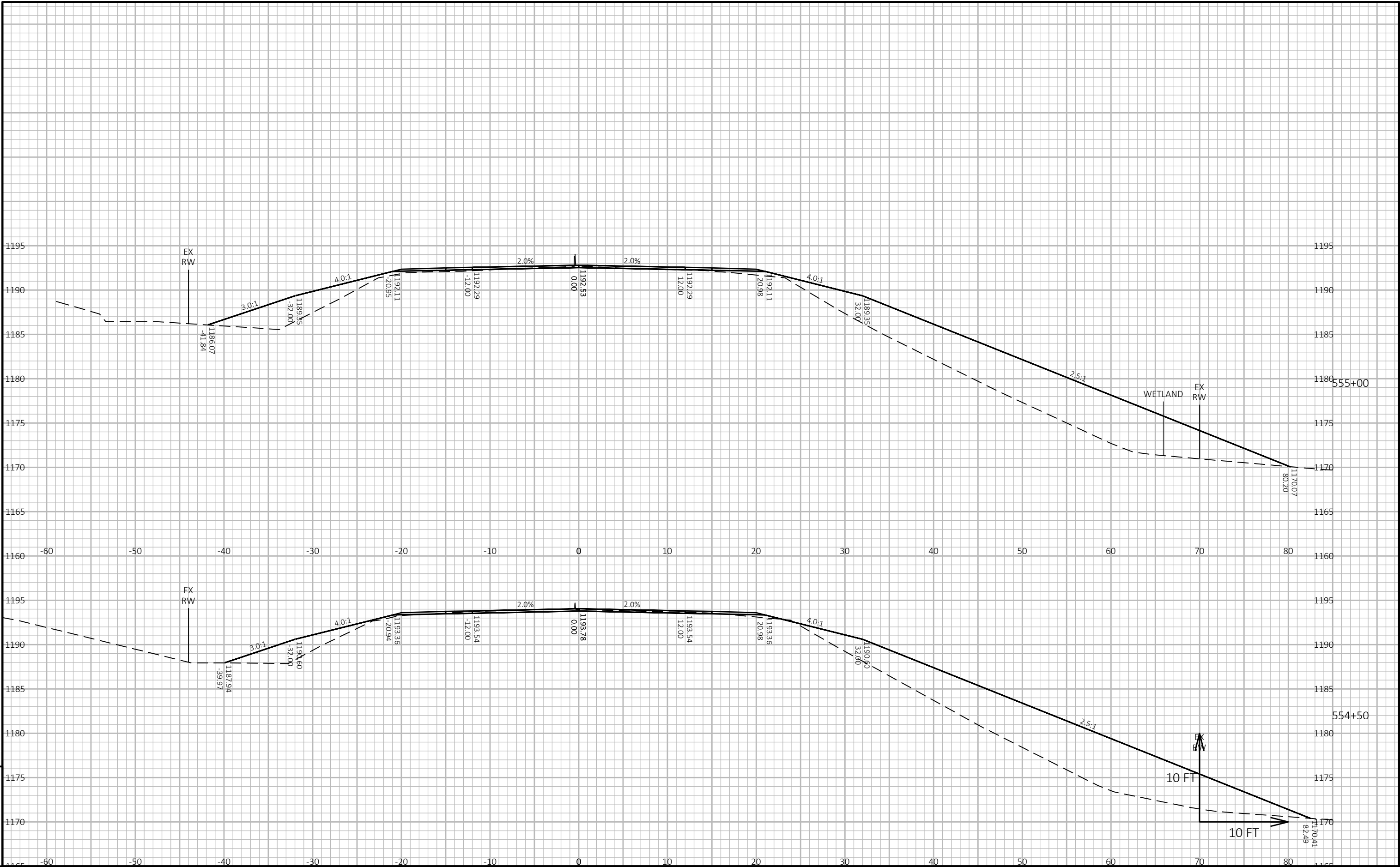


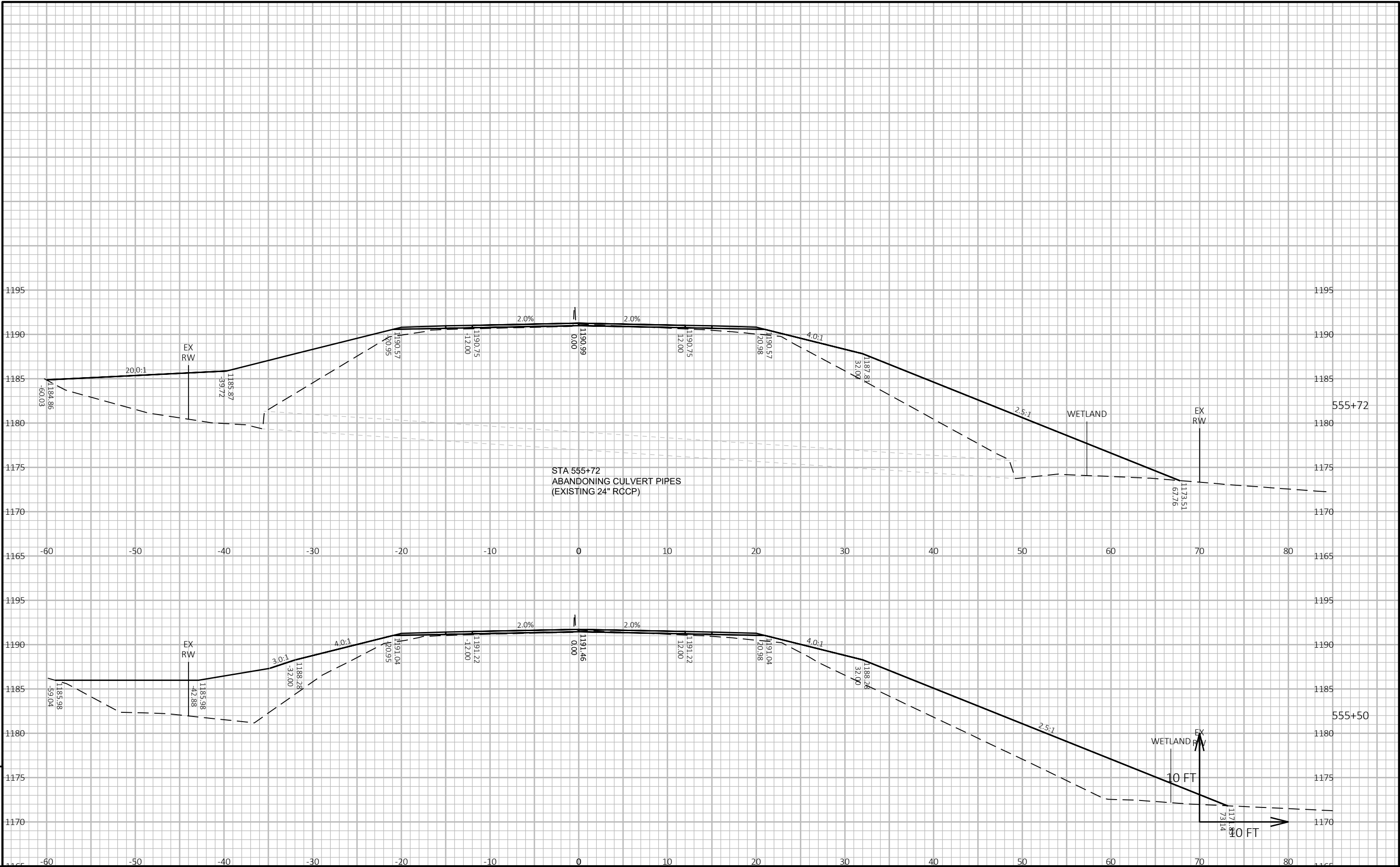
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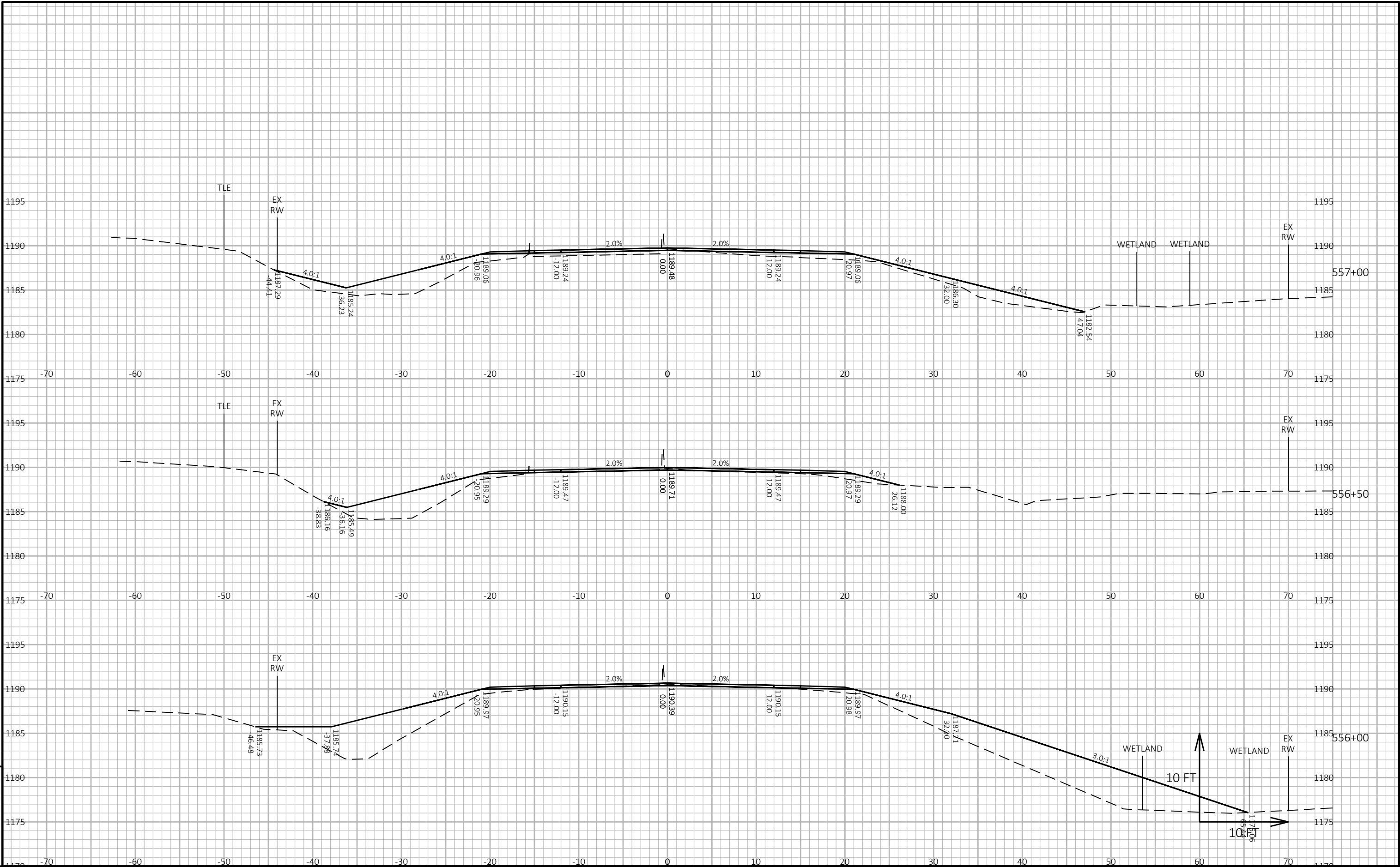


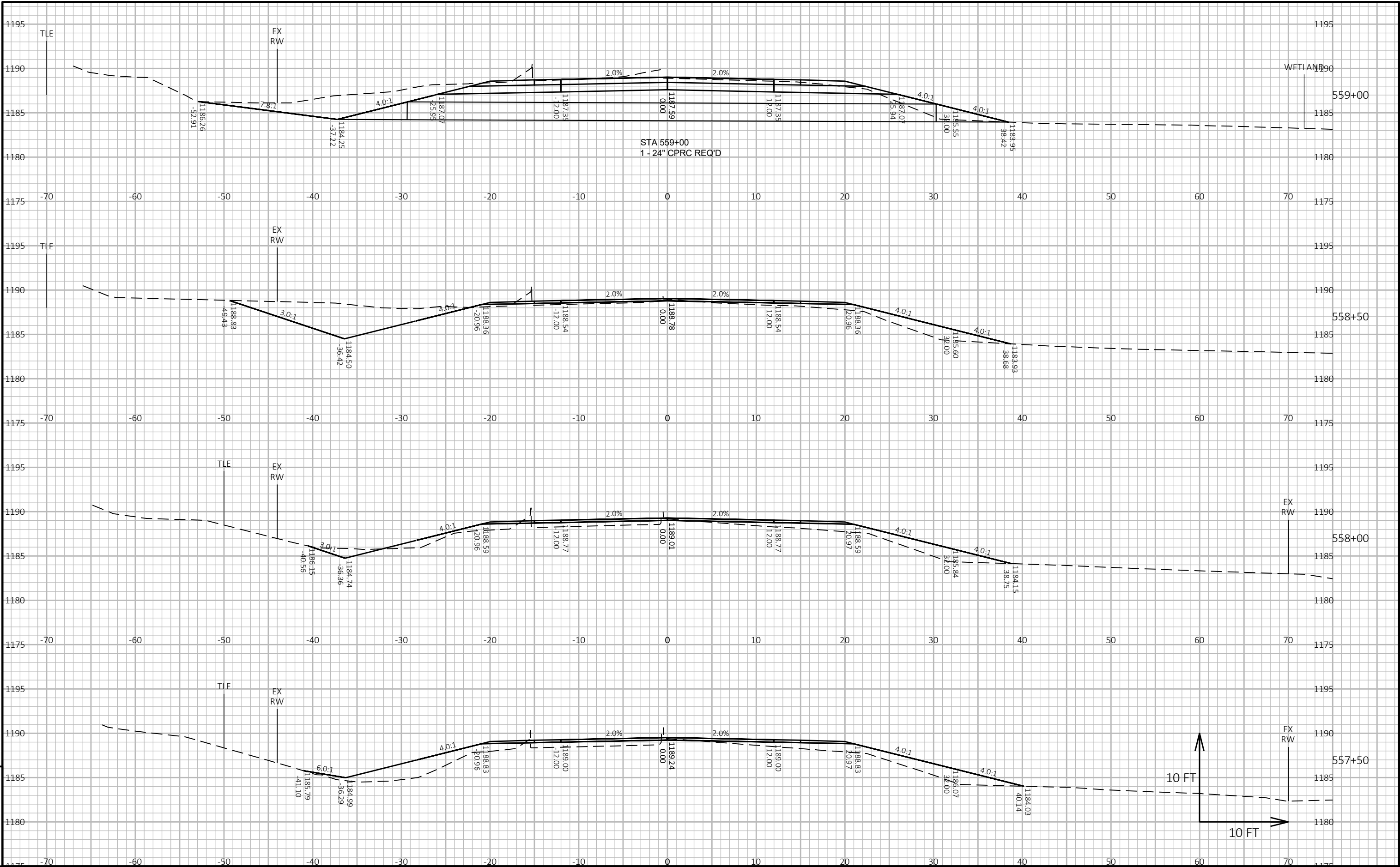


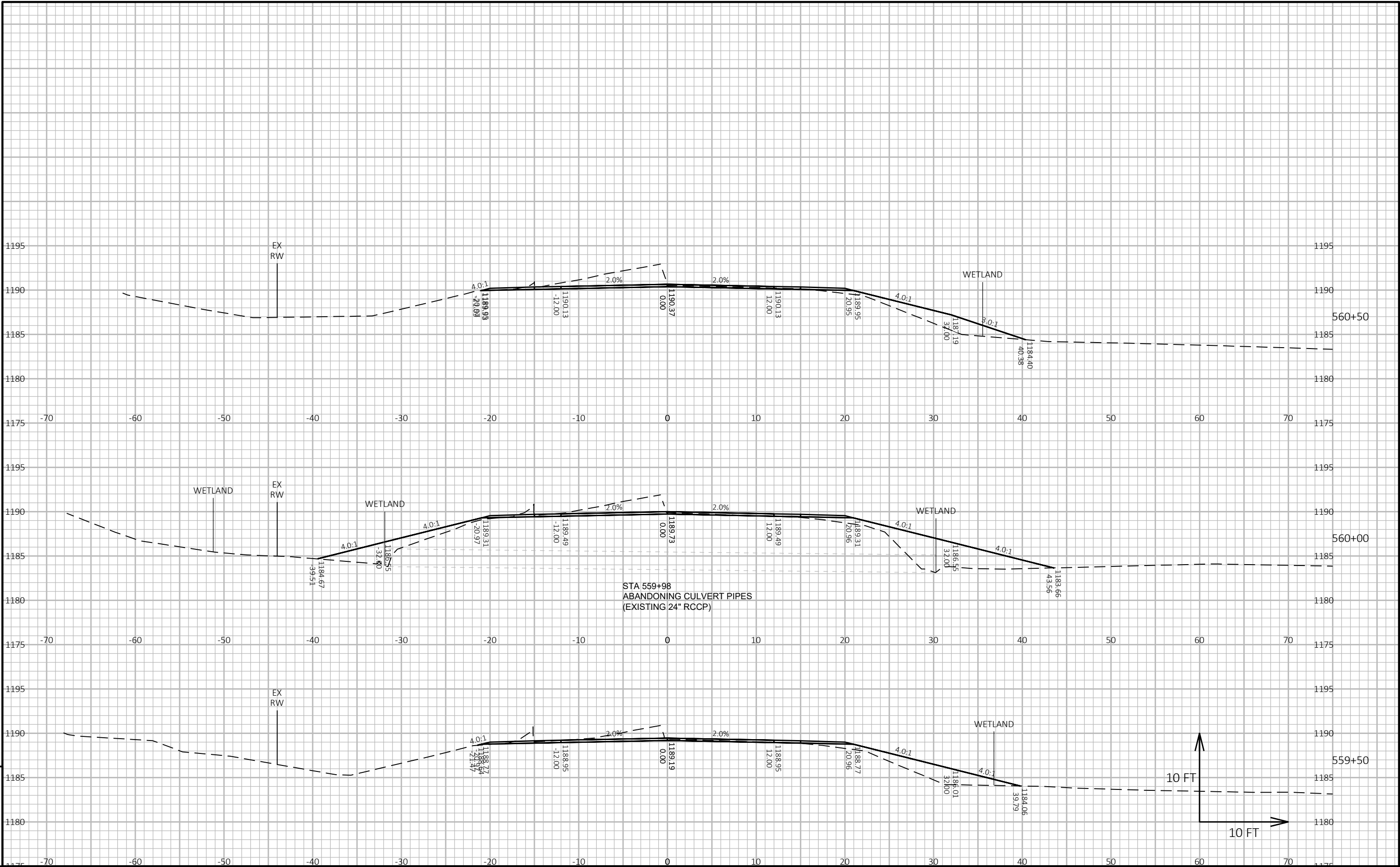
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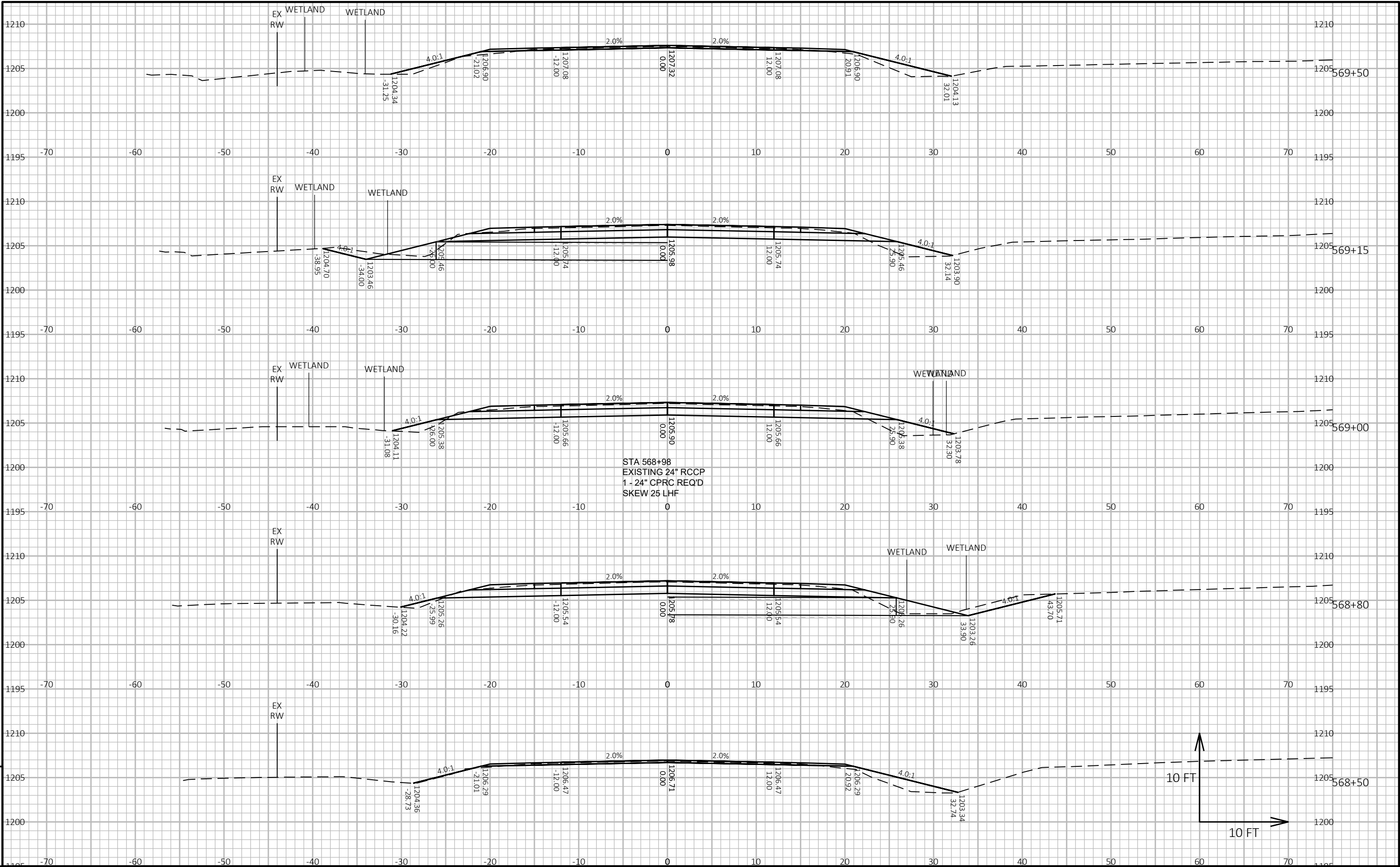
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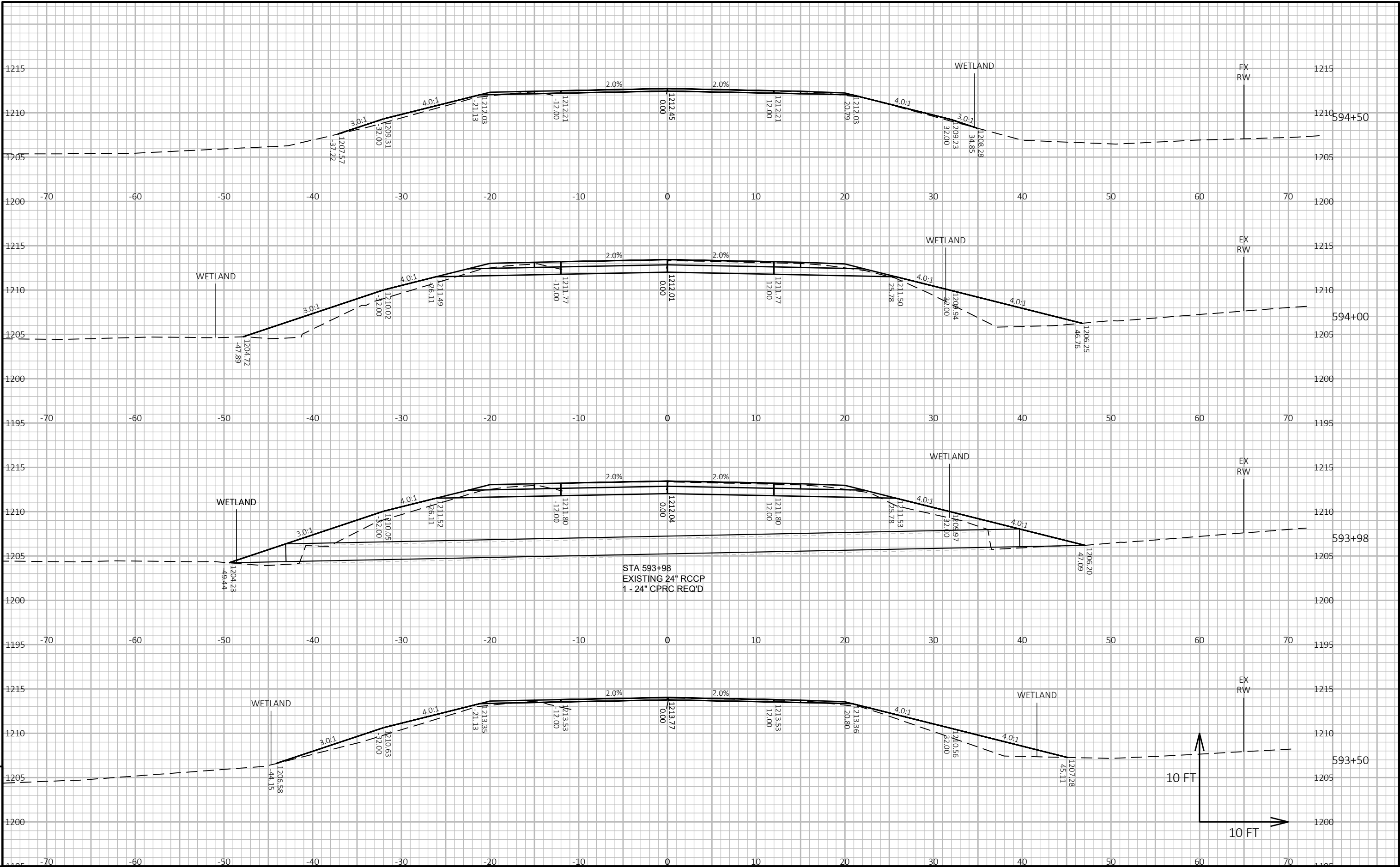
PROJECT NO: 1570-06-72	HWY: USH 8	COUNTY: RUSK	CROSS SECTIONS: USH 8	SHEET E
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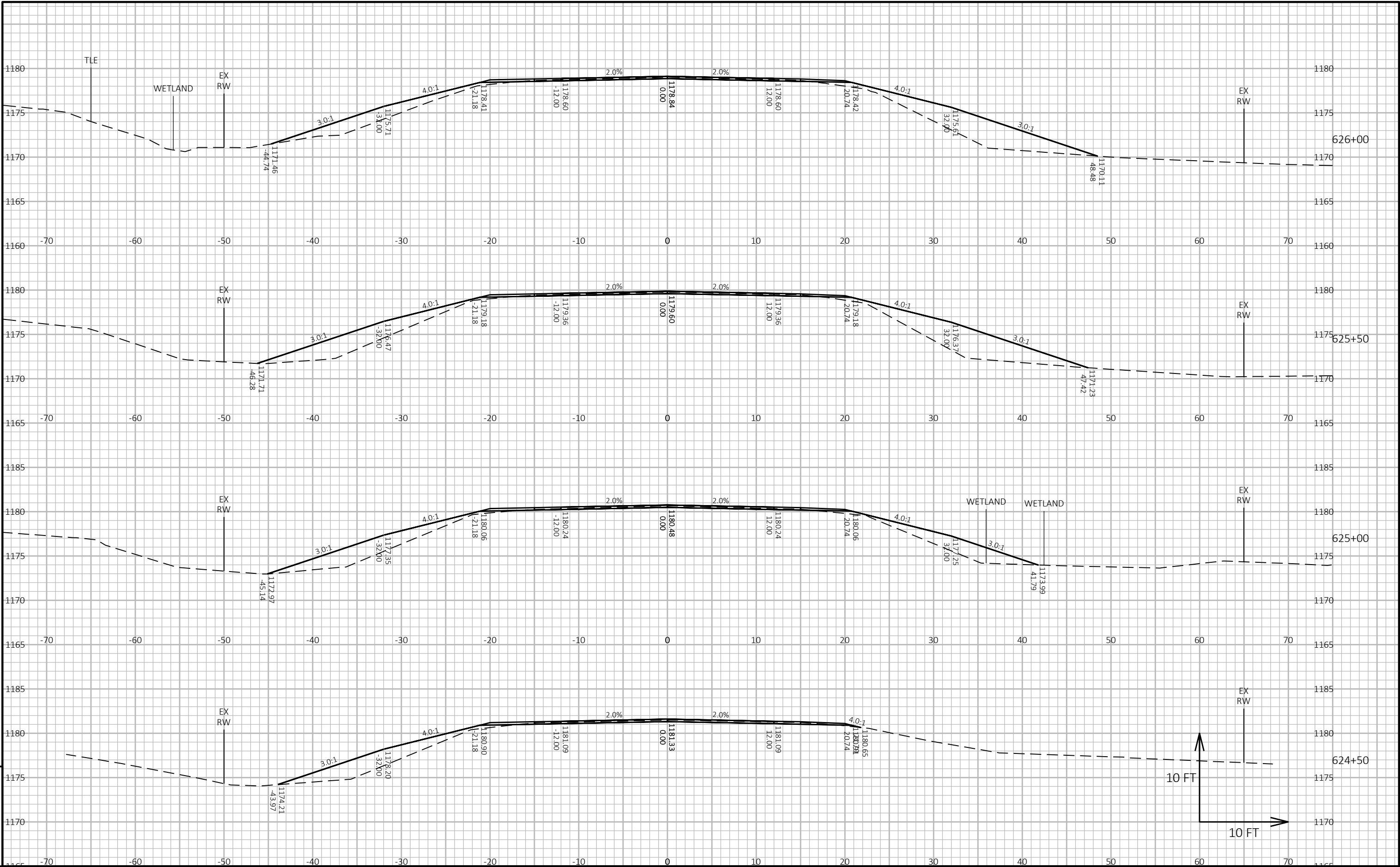


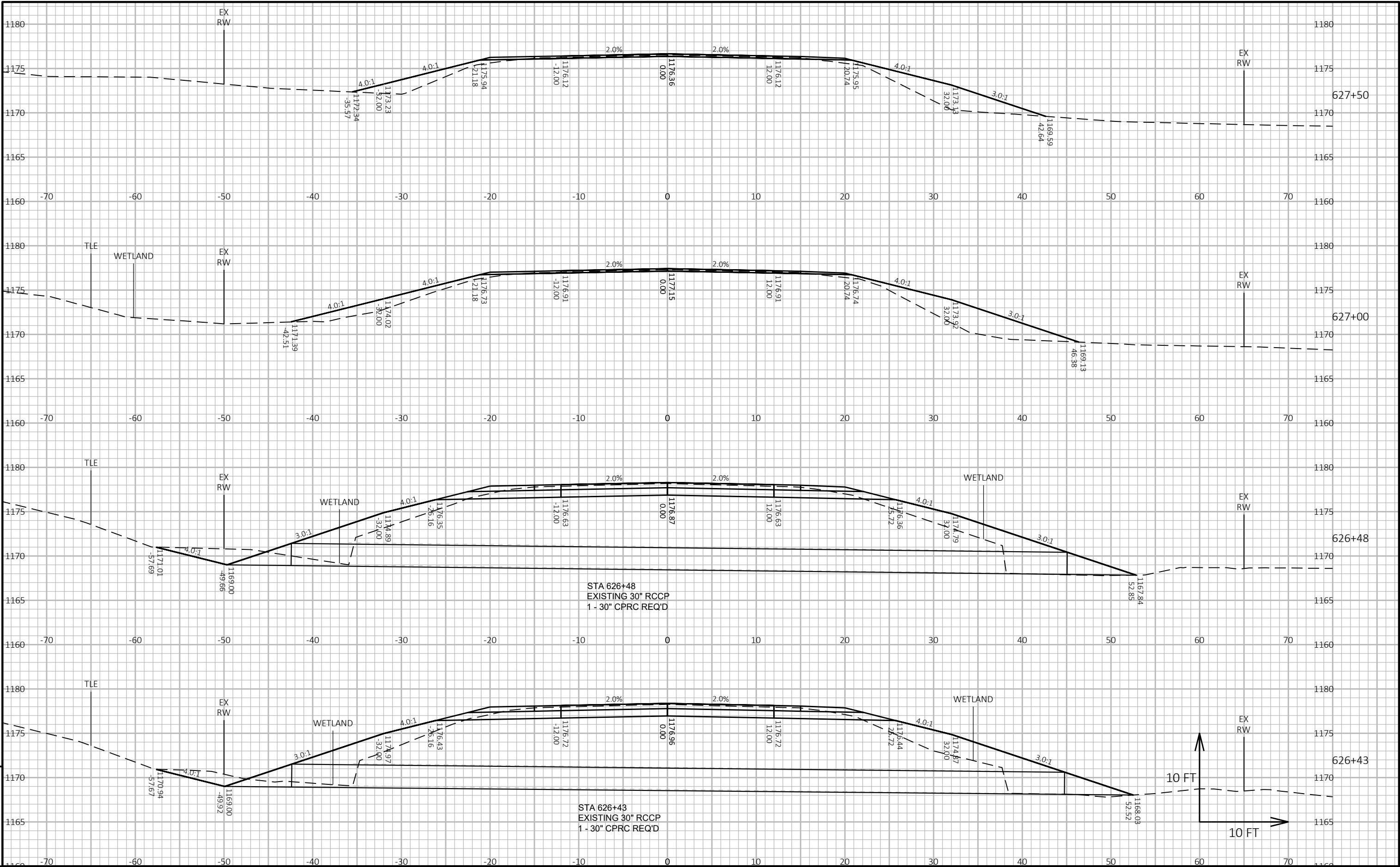


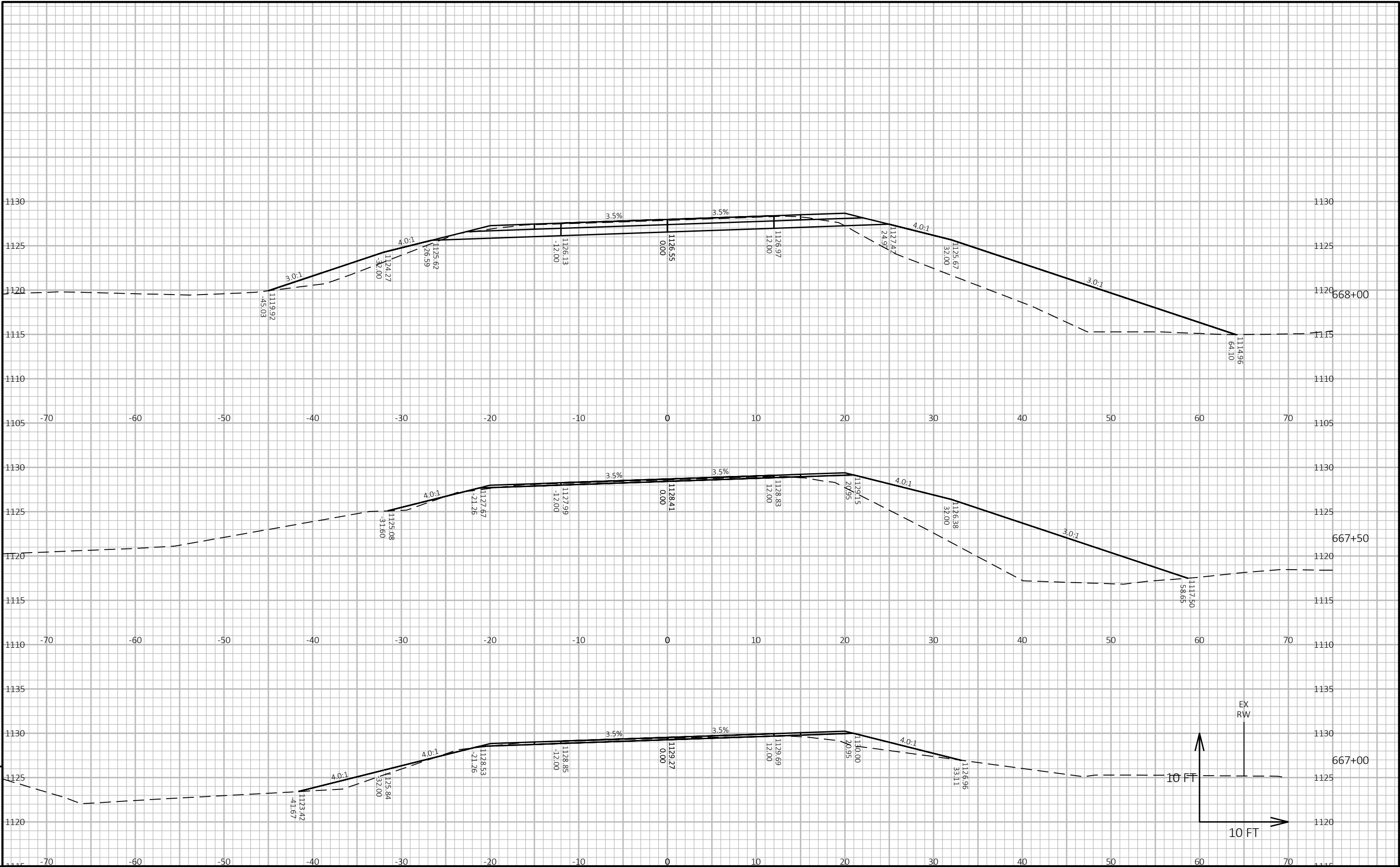


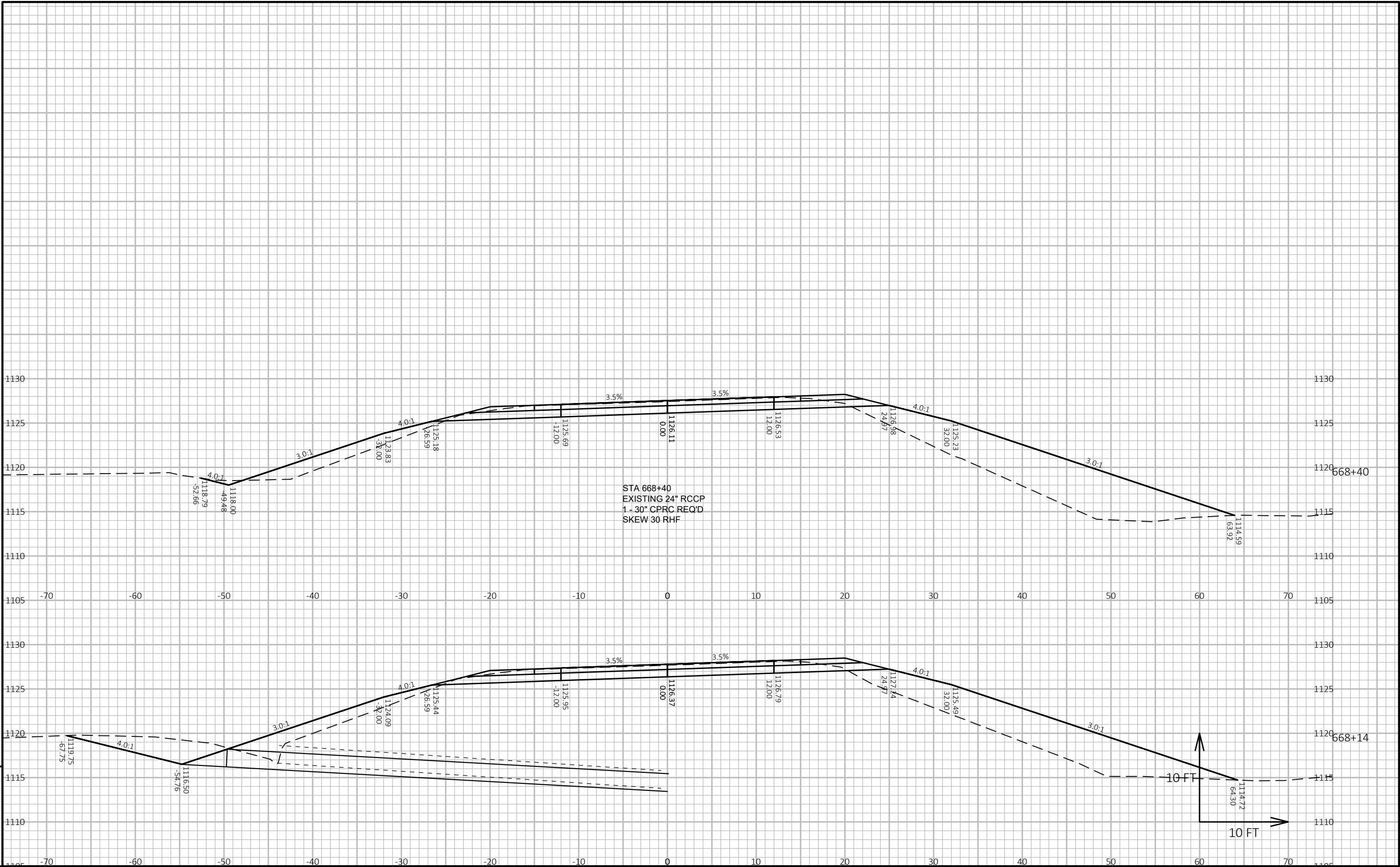








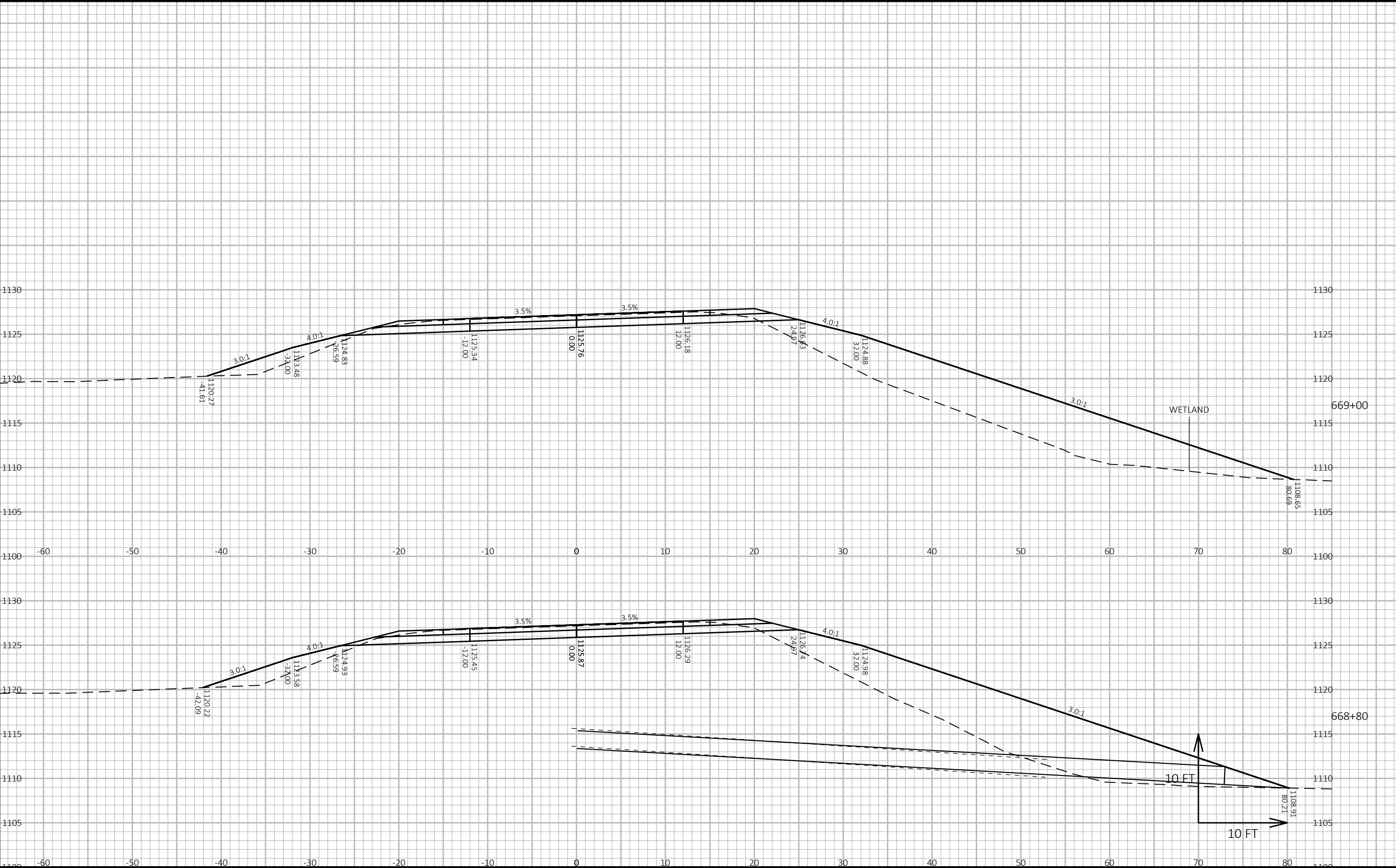


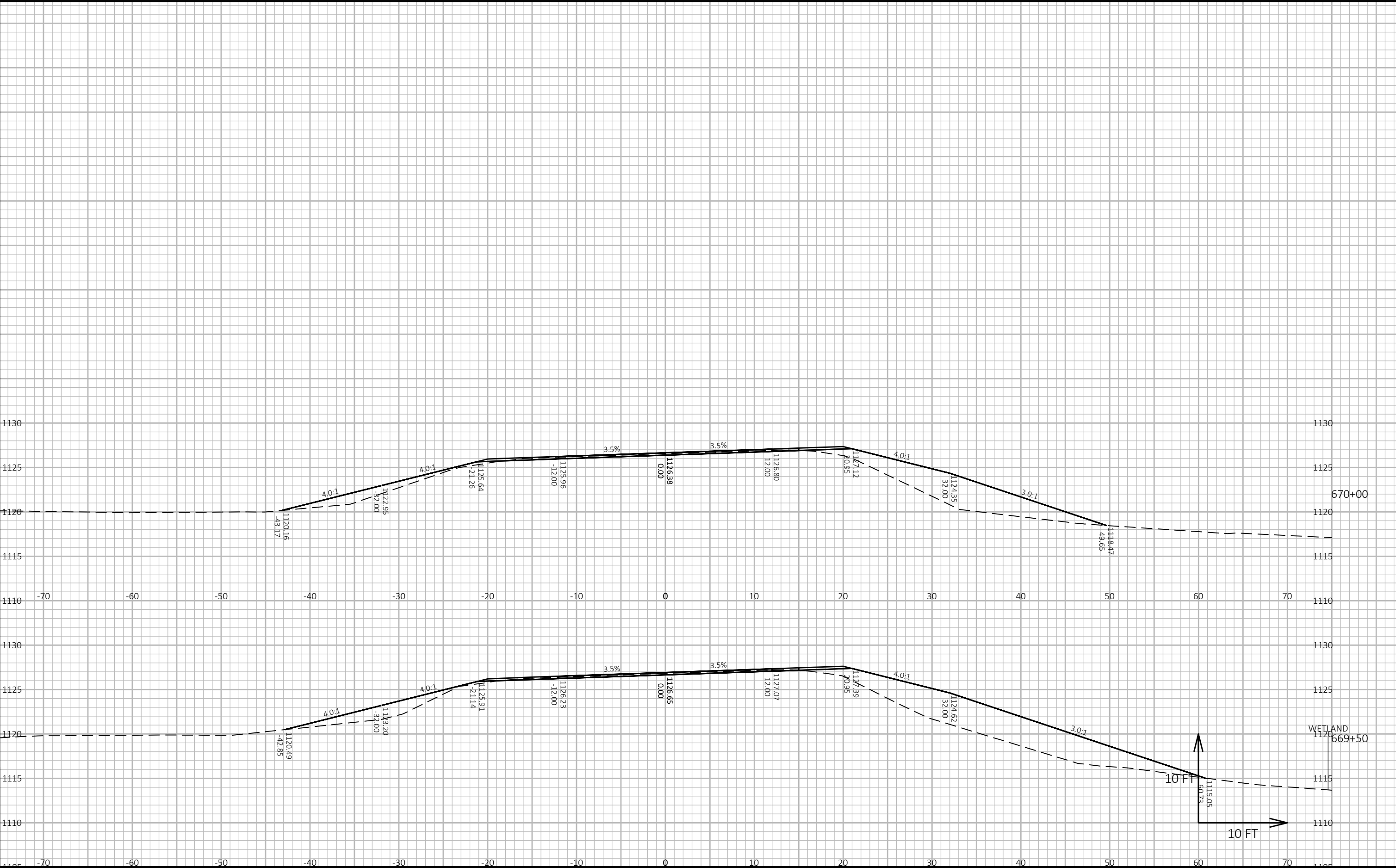


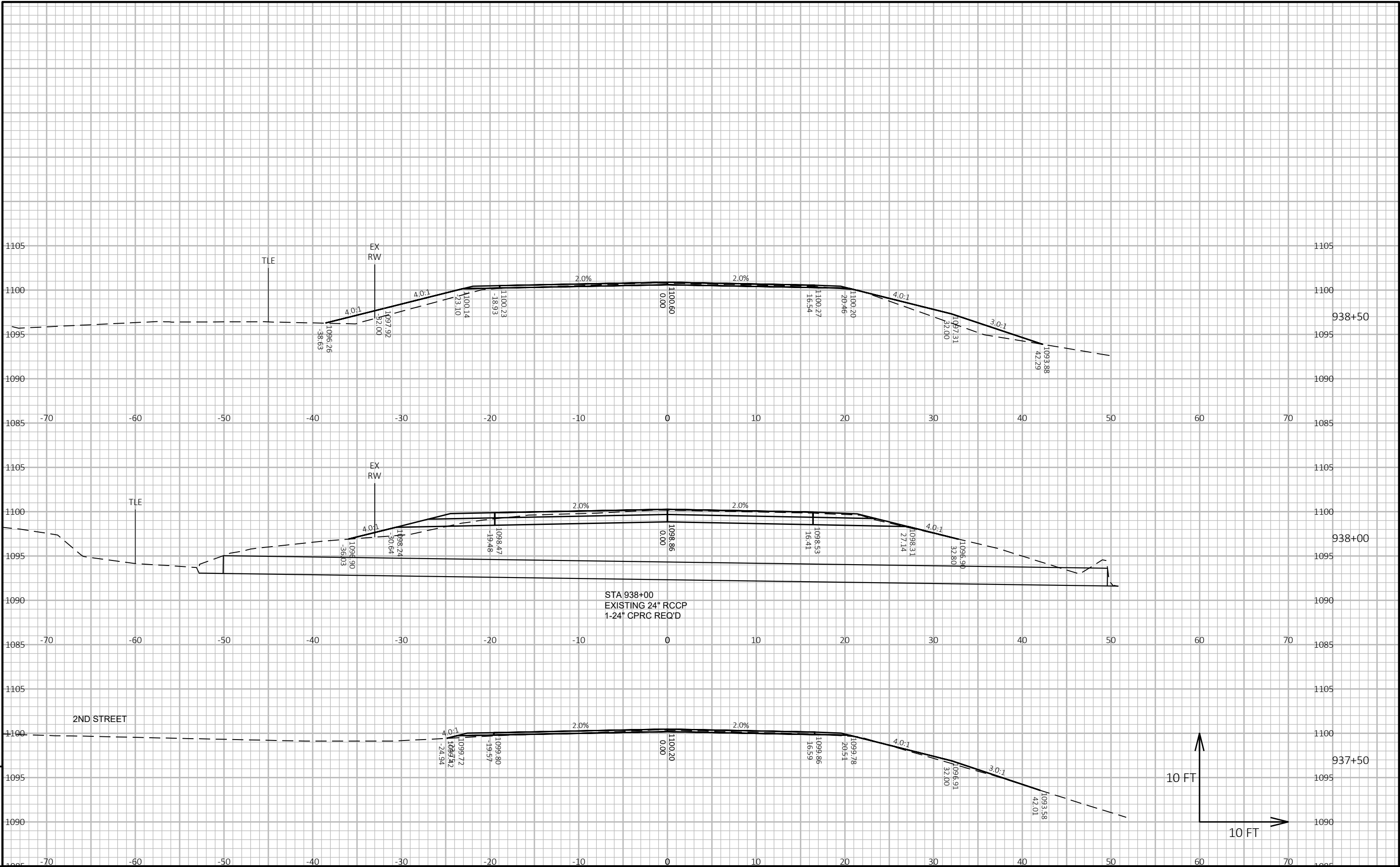
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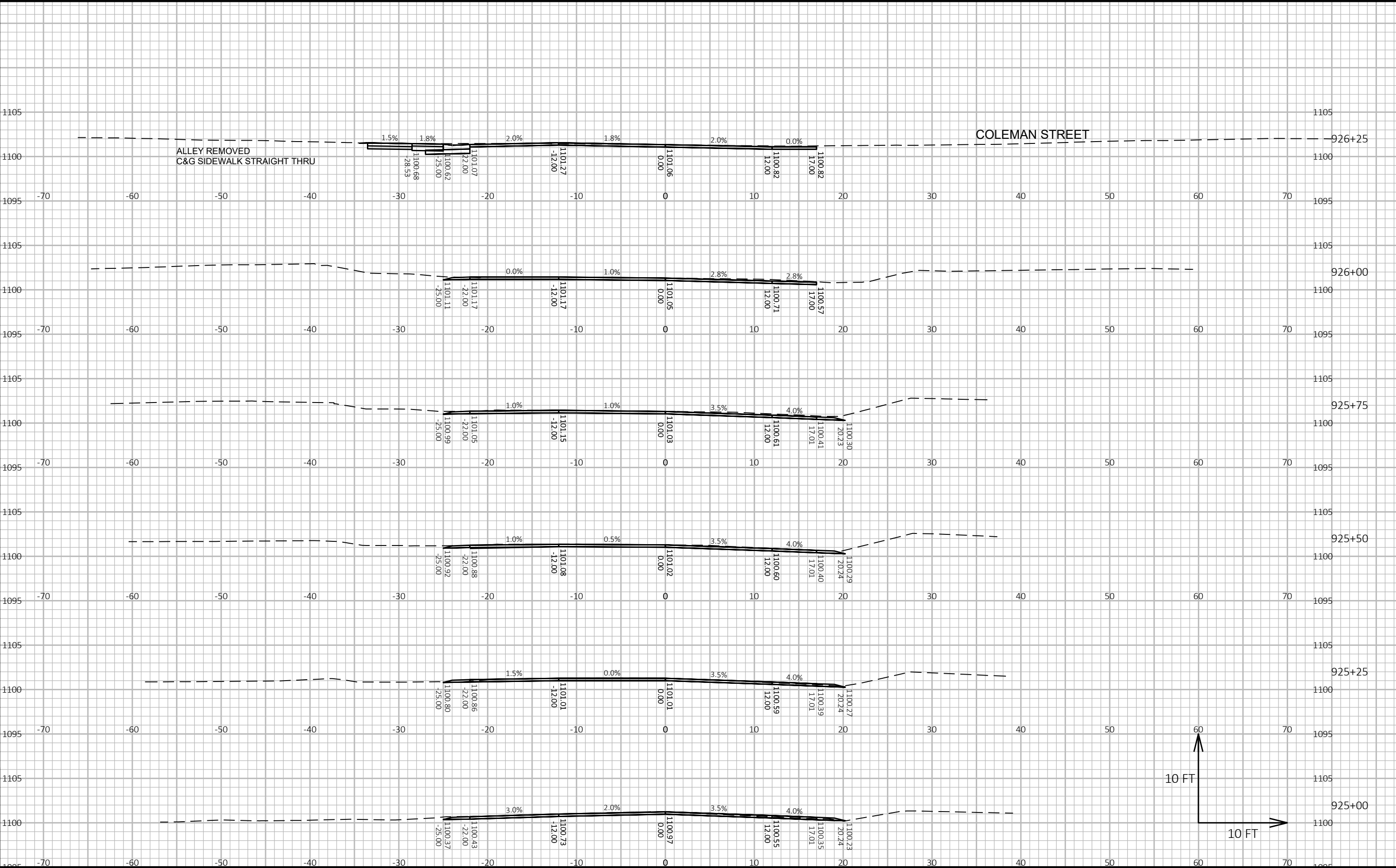
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PROJECT NO: 1570-06-72	HWY: USH 8	COUNTY: RUSK	CROSS SECTIONS: USH 8	SHEET E
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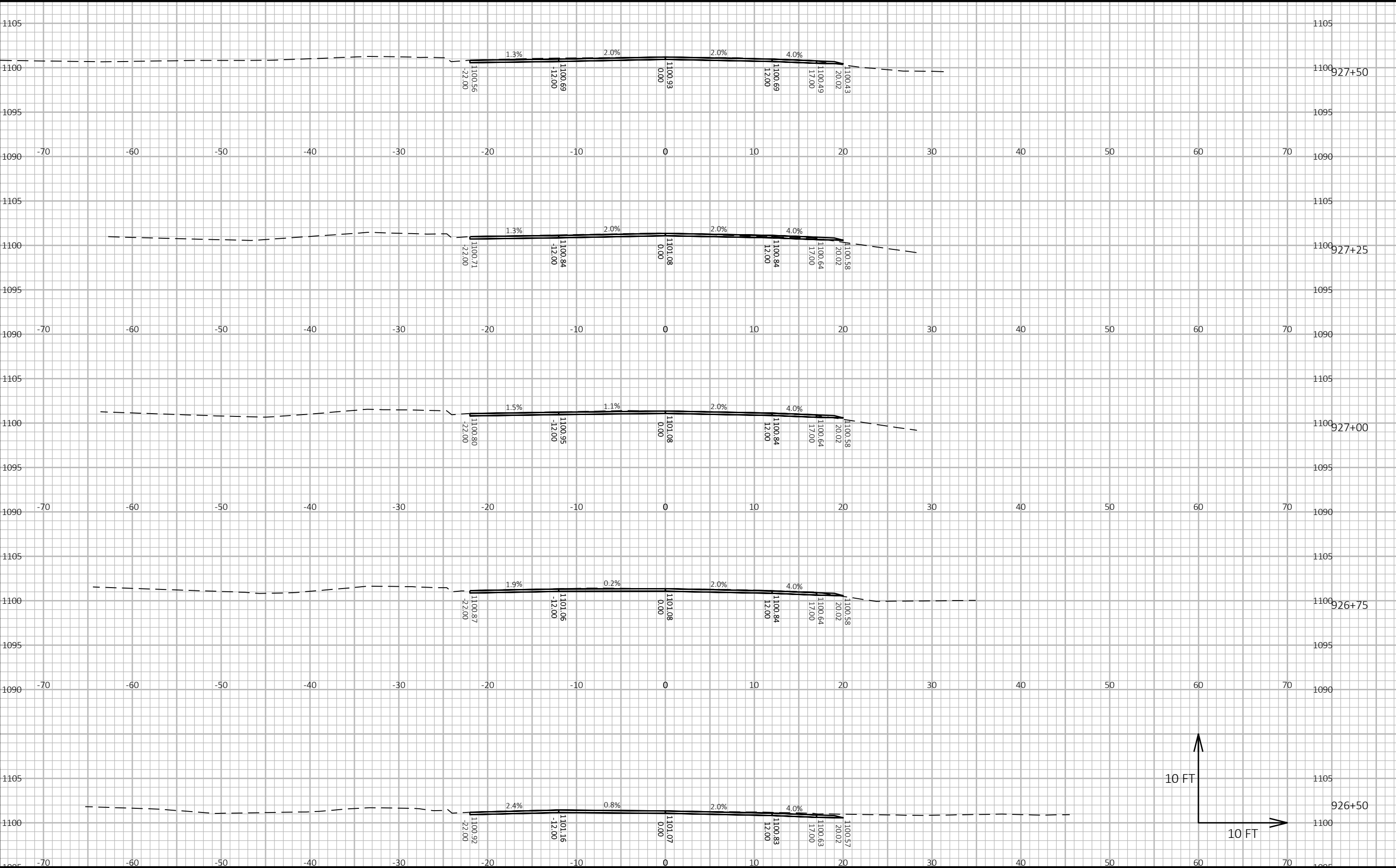


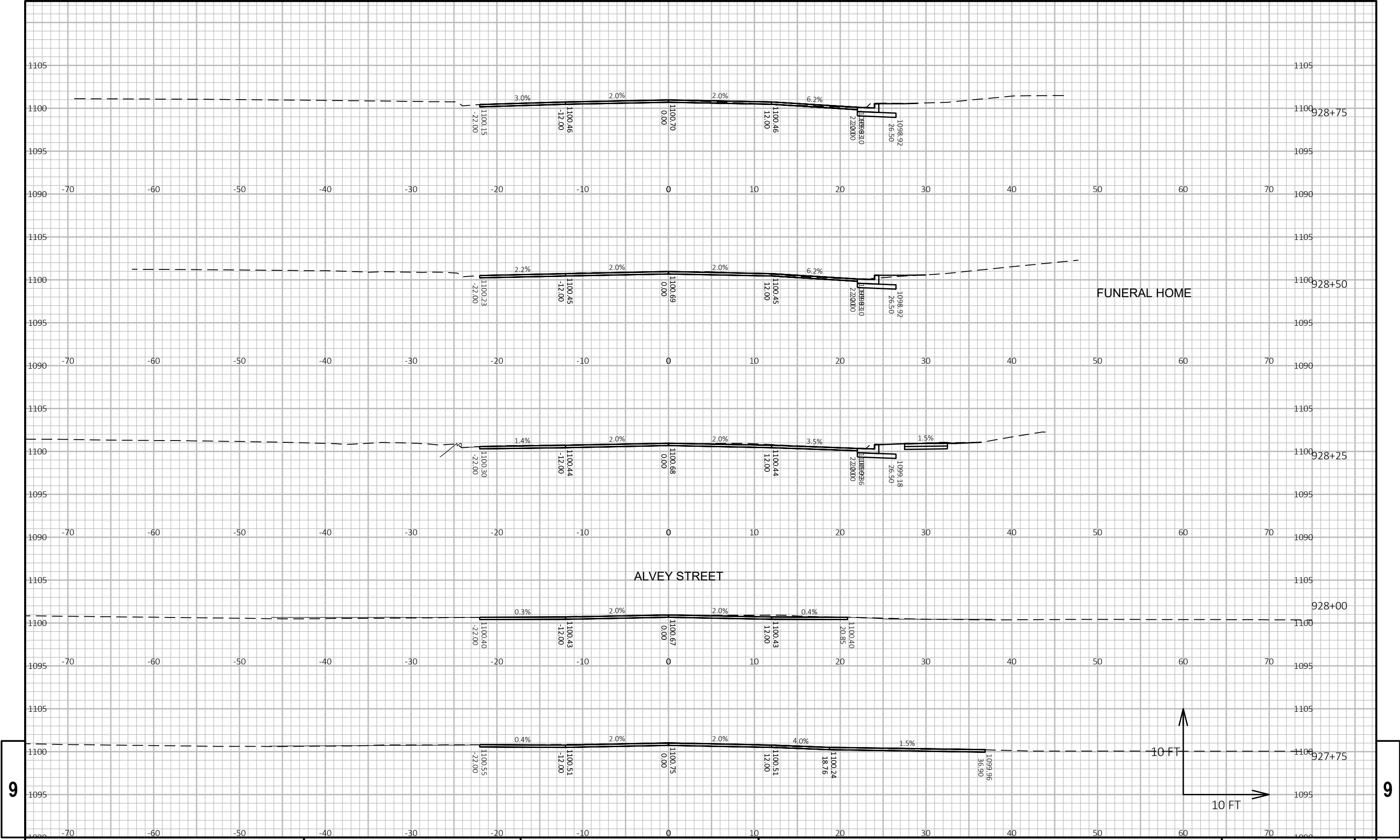


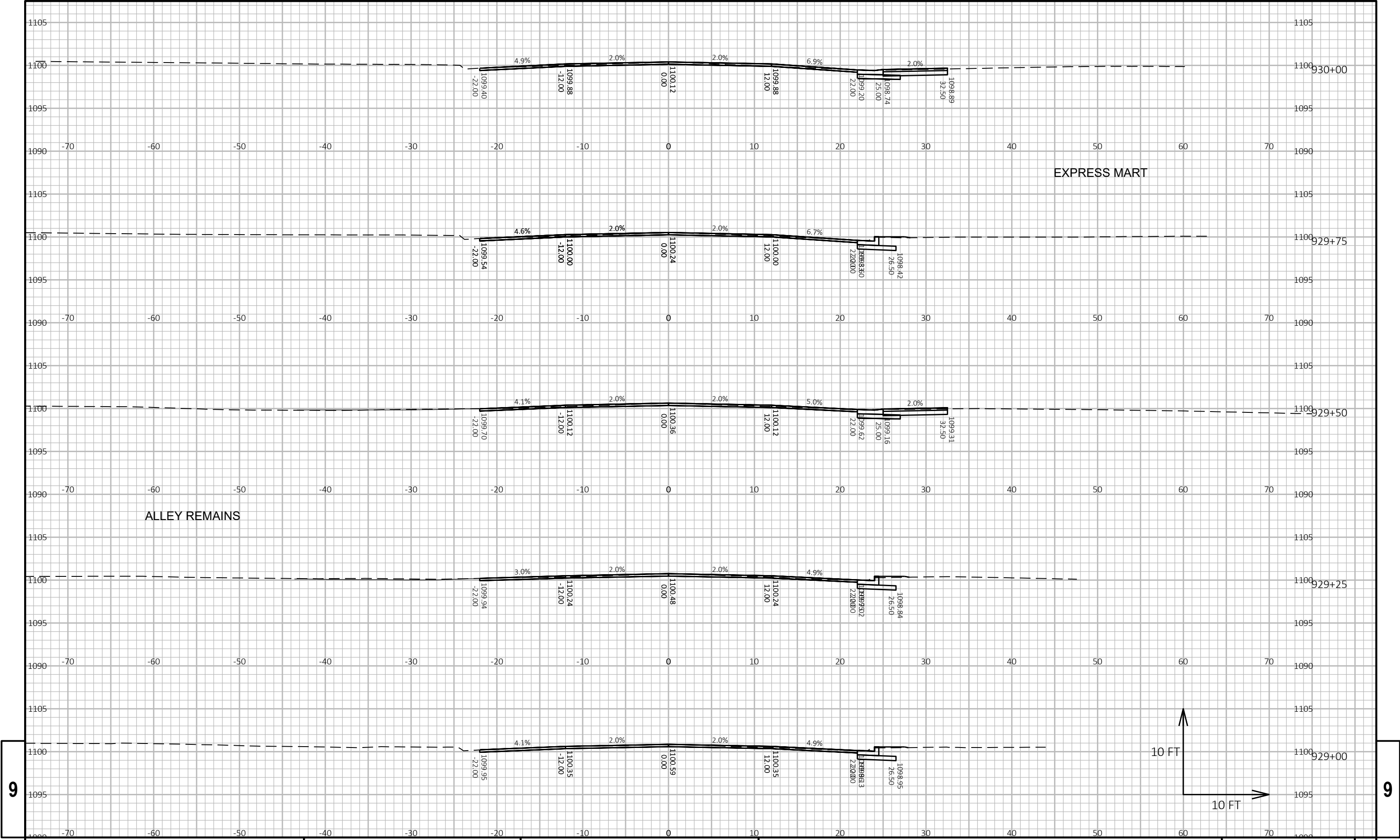
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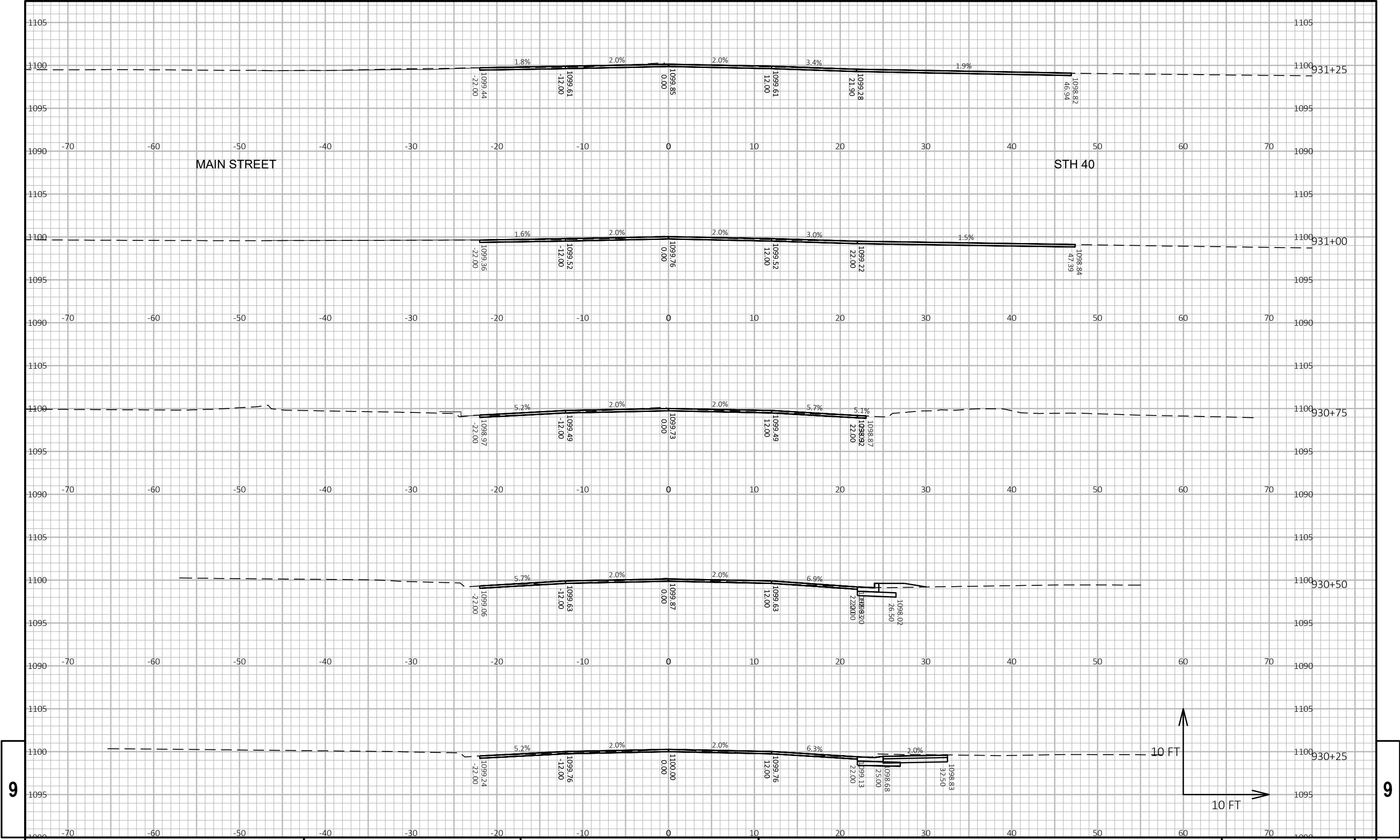
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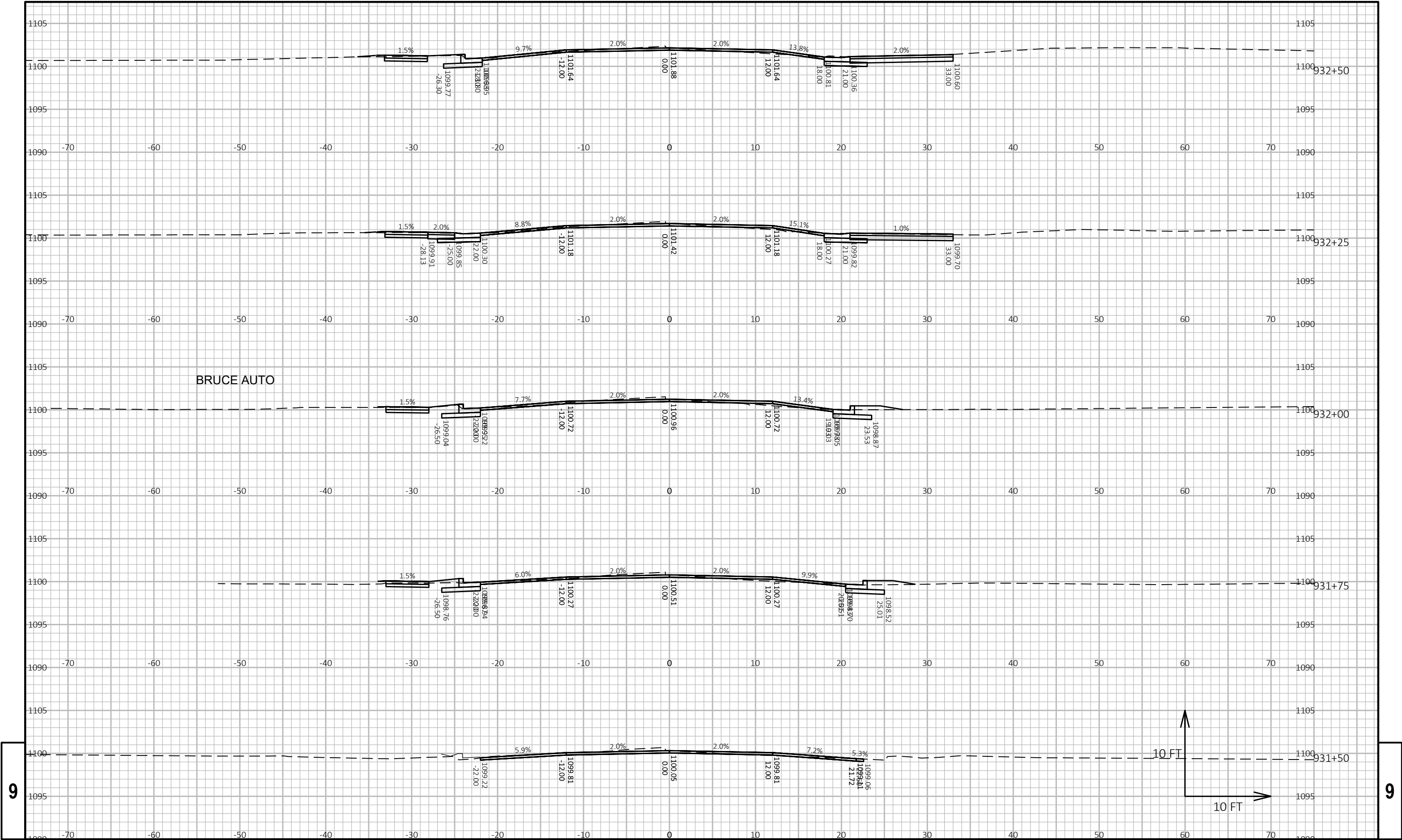
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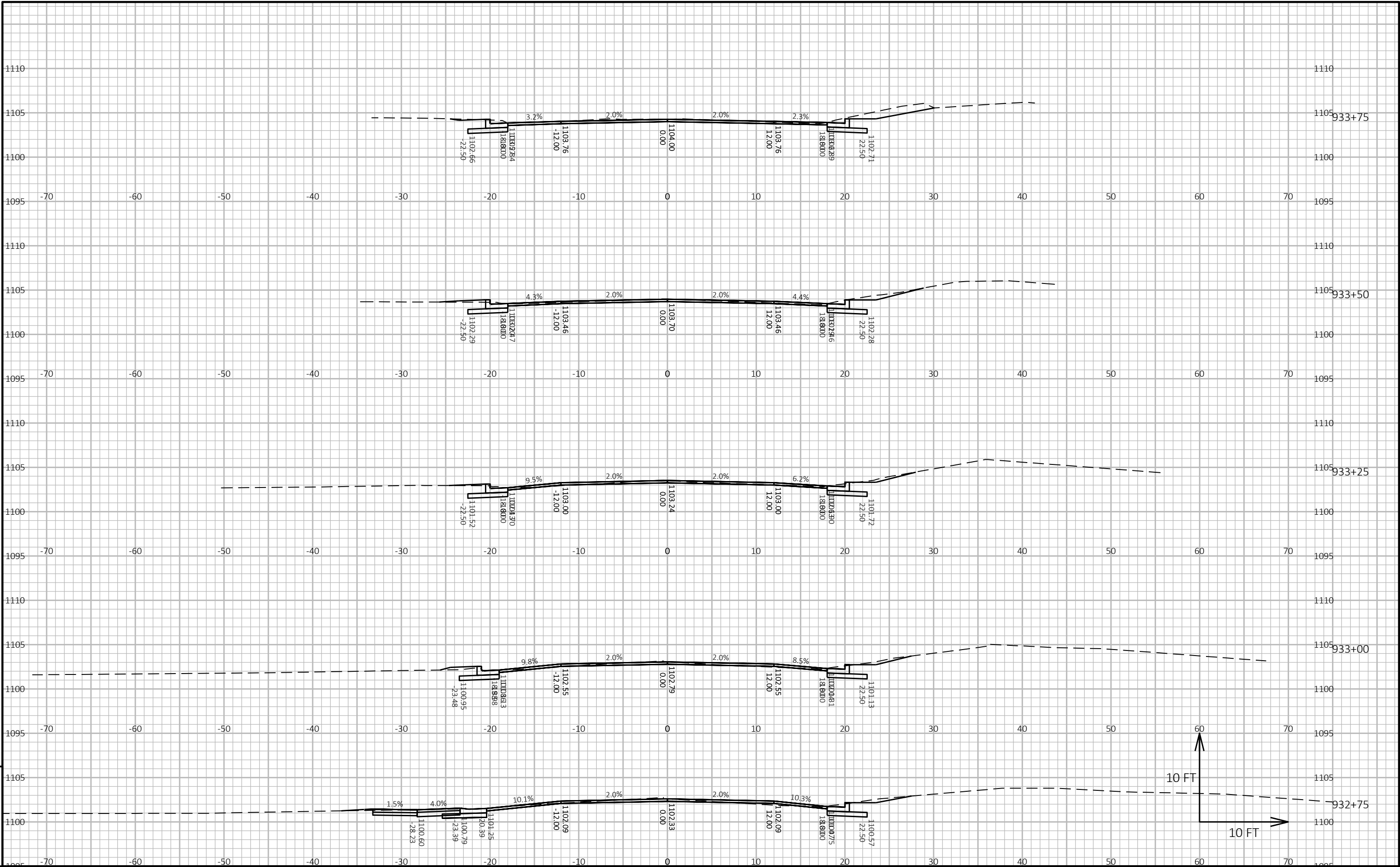


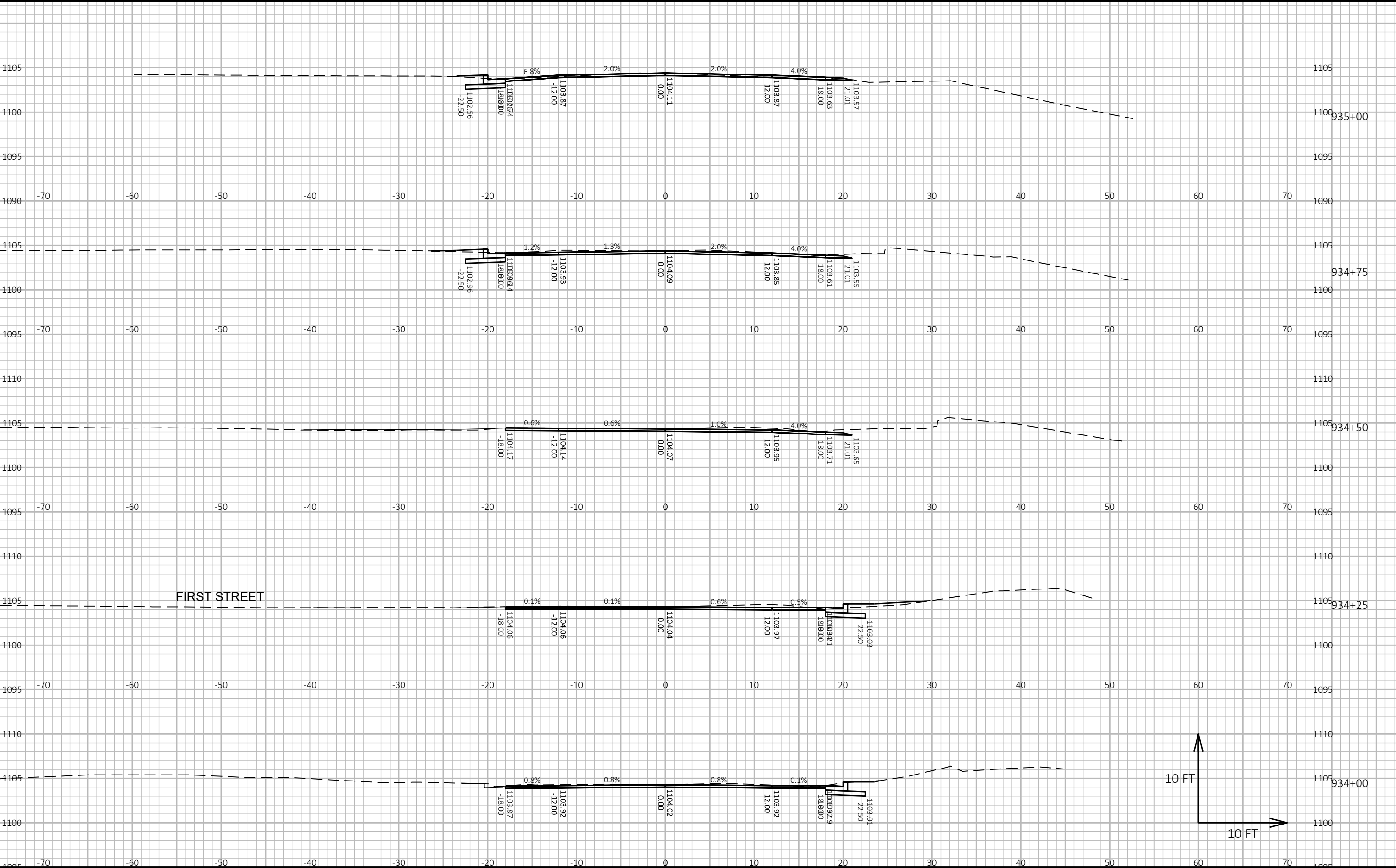














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