

HIGHWAY WORK PROPOSAL

Wisconsin Department of Transportation
DT1502 01/2020 s.66.0901(7) Wis. Stats

Proposal Number: **009**

<u>COUNTY</u>	<u>STATE PROJECT</u>	<u>FEDERAL</u>	<u>PROJECT DESCRIPTION</u>	<u>HIGHWAY</u>
Waukesha	2025-15-71	N/A	Pewaukee To Milwaukee; Pilgrim Rd/Capitol Dr B-67-0086	CTH YY

ADDENDUM REQUIRED ATTACHED AT BACK

This proposal, submitted by the undersigned bidder to the Wisconsin Department of Transportation, is in accordance with the advertised request for proposals. The bidder is to furnish and deliver all materials, and to perform all work for the improvement of the designated project in the time specified, in accordance with the appended Proposal Requirements and Conditions.

Proposal Guaranty Required: \$20,000.00 Payable to: Wisconsin Department of Transportation	Attach Proposal Guaranty on back of this PAGE.
Bid Submittal Date: August 9, 2022 Time (Local Time): 11:00 am	Firm Name, Address, City, State, Zip Code
Contract Completion Time November 18, 2022	SAMPLE NOT FOR BIDDING PURPOSES
Assigned Disadvantaged Business Enterprise Goal 0%	This contract is exempt from federal oversight.

This certifies that the undersigned bidder, duly sworn, is an authorized representative of the firm named above; that the bidder has examined and carefully prepared the bid from the plans, Highway Work Proposal, and all addenda, and has checked the same in detail before submitting this proposal or bid; and that the bidder or agents, officer, or employees have not, either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action in restraint of free competitive bidding in connection with this proposal bid.

Do not sign, notarize, or submit this Highway Work Proposal when submitting an electronic bid on the Internet.

Subscribed and sworn to before me this date _____

(Signature, Notary Public, State of Wisconsin)

(Bidder Signature)

(Print or Type Name, Notary Public, State Wisconsin)

(Print or Type Bidder Name)

(Date Commission Expires)

(Bidder Title)

Notary Seal

Type of Work:	For Department Use Only
Bridge Repair, Bridge Painting Pavement Markings, Signs	
Notice of Award Dated	Date Guaranty Returned

**PLEASE ATTACH
PROPOSAL GUARANTY HERE**

Effective with November 2007 Letting

PROPOSAL REQUIREMENTS AND CONDITIONS

The bidder, signing and submitting this proposal, agrees and declares as a condition thereof, to be bound by the following conditions and requirements.

If the bidder has a corporate relationship with the proposal design engineering company, the bidder declares that it did not obtain any facts, data, or other information related to this proposal from the design engineering company that was not available to all bidders.

The bidder declares that they have carefully examined the site of, and the proposal, plans, specifications and contract forms for the work contemplated, and it is assumed that the bidder has investigated and is satisfied as to the conditions to be encountered, as to the character, quality, and quantities of work to be performed and materials to be furnished, and as to the requirements of the specifications, special provisions and contract. It is mutually agreed that submission of a proposal shall be considered conclusive evidence that the bidder has made such examination.

The bidder submits herewith a proposal guaranty in proper form and amount payable to the party as designated in the advertisement inviting proposals, to be retained by and become the property of the owner of the work in the event the undersigned shall fail to execute the contract and contract bond and return the same to the office of the engineer within fourteen (14) days after having been notified in writing to do so; otherwise to be returned.

The bidder declares that they understand that the estimate of quantities in the attached schedule is approximate only and that the attached quantities may be greater or less in accordance with the specifications.

The bidder agrees to perform the said work, for and in consideration of the payment of the amount becoming due on account of work performed, according to the unit prices bid in the following schedule, and to accept such amounts in full payment of said work.

The bidder declares that all of the said work will be performed at their own proper cost and expense, that they will furnish all necessary materials, labor, tools, machinery, apparatus, and other means of construction in the manner provided in the applicable specifications and the approved plans for the work together with all standard and special designs that may be designed on such plans, and the special provisions in the contract of which this proposal will become a part, if and when accepted. The bidder further agrees that the applicable specifications and all plans and working drawings are made a part hereof, as fully and completely as if attached hereto.

The bidder, if awarded the contract, agrees to begin the work not later than ten (10) days after the date of written notification from the engineer to do so, unless otherwise stipulated in the special provisions.

The bidder declares that if they are awarded the contract, they will execute the contract agreement and begin and complete the work within the time named herein, and they will file a good and sufficient surety bond for the amount of the contract for performance and also for the full amount of the contract for payment.

The bidder, if awarded the contract, shall pay all claims as required by Section 779.14, Statutes of Wisconsin, and shall be subject to and discharge all liabilities for injuries pursuant to Chapter 102 of the Statutes of Wisconsin, and all acts amendatory thereto. They shall further be responsible for any damages to property or injury to persons occurring through their own negligence or that of their employees or agents, incident to the performance of work under this contract, pursuant to the Standard Specifications for Road and Bridge Construction applicable to this contract.

In connection with the performance of work under this contract, the contractor agrees to comply with all applicable state and federal statutes relating to non-discrimination in employment. No otherwise qualified person shall be excluded from employment or otherwise be subject to discrimination in employment in any manner on the basis of age, race, religion, color, gender, national origin or ancestry, disability, arrest or conviction record (in keeping with s.111.32), sexual orientation, marital status, membership in the military reserve, honesty testing, genetic testing, and outside use of lawful products. This provision shall include, but not be limited to the following: employment, upgrading, demotion or transfer; recruitment or recruitment advertising; layoff or termination; rates of pay or other forms of compensation, and selection for training, including apprenticeship. The contractor further agrees to ensure equal opportunity in employment to all applicants and employees and to take affirmative action to attain a representative workforce.

The contractor agrees to post notices and posters setting forth the provisions of the nondiscrimination clause, in a conspicuous and easily accessible place, available for employees and applicants for employment.

If a state public official (section 19.42, Stats.) or an organization in which a state public official holds at least a 10% interest is a party to this agreement, this contract is voidable by the state unless appropriate disclosure is made to the State of Wisconsin Ethics Board.

Effective with August 2015 Letting

BID PREPARATION

Preparing the Proposal Schedule of Items

A General

- (1) Obtain bidding proposals as specified in section 102 of the standard specifications prior to 11:45 AM of the last business day preceding the letting. Submit bidding proposals using one of the following methods:
 1. Electronic bid on the internet.
 2. Electronic bid on a printout with accompanying diskette or CD ROM.
 3. Paper bid under a waiver of the electronic submittal requirements.

- (2) Bids submitted on a printout with accompanying diskette or CD ROM or paper bids submitted under a waiver of the electronic submittal requirements govern over bids submitted on the internet.

- (3) The department will provide bidding information through the department's web site at:
<https://wisconsindot.gov/Pages/doing-bus/contractors/hcci/bid-let.aspx>

The contractor is responsible for reviewing this web site for general notices as well as information regarding proposals in each letting. The department will also post special notices of all addenda to each proposal through this web site no later than 4:00 PM local time on the Thursday before the letting. Check the department's web site after 5:00 PM local time on the Thursday before the letting to ensure all addenda have been accounted for before preparing the bid. When bidding using methods 1 and 2 above, check the Bid Express™ on-line bidding exchange at <http://www.bidx.com/> after 5:00 PM local time on the Thursday before the letting to ensure that the latest schedule of items Expedite file (*.ebs or *.00x) is used to submit the final bid.

- (4) Interested parties can subscribe to the Bid Express™ on-line bidding exchange by following the instructions provided at the www.bidx.com web site or by contacting:

Info Tech Inc.
5700 SW 34th Street, Suite 1235
Gainesville, FL 32608-5371
email: <mailto:customer.support@bidx.com>

- (5) The department will address equipment and process failures, if the bidder can demonstrate that those failures were beyond their control.
- (6) Contractors are responsible for checking on the issuance of addenda and for obtaining the addenda. Notice of issuance of addenda is posted on the department's web site at:
<https://wisconsindot.gov/Pages/doing-bus/contractors/hcci/bid-let.aspx>

or by calling the department at (608) 266-1631. Addenda can ONLY be obtained from the departments web site listed above or by picking up the addenda at the Bureau of Highway Construction, 4th floor, 4822 Madison Yards Way, Madison, WI, during regular business hours.

- (7) Addenda posted after 5:00 PM on the Thursday before the letting will be emailed to the eligible bidders for that proposal. All eligible bidders shall acknowledge receipt of the addenda whether they are bidding on the proposal or not. Not acknowledging receipt may jeopardize the awarding of the project.

B Submitting Electronic Bids

B.1 On the Internet

- (1) Do the following before submitting the bid:
 1. Have a properly executed annual bid bond on file with the department.

2. Have a digital ID on file with and enabled by Info Tech Inc. Using this digital ID will constitute the bidder's signature for proper execution of the bidding proposal.
- (2) In lieu of preparing, delivering, and submitting the proposal as specified in 102.6 and 102.9 of the standard specifications, submit the proposal on the internet as follows:
 1. Download the latest schedule of items reflecting all addenda from the Bid Express™ web site.
 2. Use Expedite™ software to enter a unit price for every item in the schedule of items.
 3. Submit the bid according to the requirements of Expedite™ software and the Bid Express™ web site. Do not submit a bid on a printout with accompanying diskette or CD ROM or a paper bid. If the bidder does submit a bid on a printout with accompanying diskette or a paper bid in addition to the internet submittal, the department will disregard the internet bid.
 4. Submit the bid before the hour and date the Notice to Contractors designates.
 5. Do not sign, notarize, and return the bidding proposal described in 102.2 of the standard specifications.
- (3) The department will not consider the bid accepted until the hour and date the Notice to Contractors designates.

B.2 On a Printout with Accompanying Diskette or CD ROM

- (1) Download the latest schedule of items from the Wisconsin pages of the Bid Express™ web site reflecting the latest addenda posted on the department's web site at:
<https://wisconsindot.gov/Pages/doing-bus/contractors/hcci/bid-let.aspx>
Use Expedite™ software to prepare and print the schedule of items. Provide a valid amount for all price fields. Follow instructions and review the help screens provided on the Bid Express™ web site to assure that the schedule of items is prepared properly.
- (2) Staple an 8 1/2 by 11 inch printout of the Expedite™ generated schedule of items to the other proposal documents submitted to the department as a part of the bidder's sealed bid. As a separate submittal, not in the sealed bid envelop but due at the same time and place as the sealed bid, also provide the Expedite™ generated schedule of items on a 3 1/2 inch computer diskette or CD ROM. Label each diskette or CD ROM with the bidder's name, the 4 character department-assigned bidder identification code from the top of the bidding proposal, and a list of the proposal numbers included on that diskette or CD ROM as indicated in the following example:

Bidder Name

BN00

Proposals: 1, 12, 14, & 22

- (3) If bidding on more than one proposal in the letting, the bidder may include all proposals for that letting on one diskette or CD ROM. Include only submitted proposals with no incomplete or other files on the diskette or CD ROM.
- (4) The bidder-submitted printout of the Expedite™ generated schedule of items is the governing contract document and must conform to the requirements of section 102 of the standard specifications. If a printout needs to be altered, cross out the printed information with ink or typewriter and enter the new information and initial it in ink. If there is a discrepancy between the printout and the diskette or CD ROM, the department will analyze the bid using the printout information.
- (5) In addition to the reasons specified in section 102 of the standard specifications, proposals are irregular and the department may reject them for one or more of the following:
 1. The check code printed on the bottom of the printout of the Expedite™ generated schedule of items is not the same on each page.
 2. The check code printed on the printout of the Expedite™ generated schedule of items is not the same as the check code for that proposal provided on the diskette or CD ROM.

3. The diskette or CD ROM is not submitted at the time and place the department designates.

C Waiver of Electronic Submittal

- (1) The bidder may request a waiver of the electronic submittal requirements. Submit a written request for a waiver in lieu of bids submitted on the internet or on a printout with accompanying diskette or CD ROM. Use the waiver that was included with the paper bid document sent to the bidder or type up a waiver on the bidder's letterhead. The department will waive the electronic submittal requirements for a bidding entity (individual, partnership, joint venture, corporation, or limited liability company) for up to 4 individual proposals in a calendar year. The department may allow additional waivers for equipment malfunctions.
- (2) Submit a schedule of items on paper conforming to section 102 of the standard specifications. The department charges the bidder a \$75 administrative fee per proposal, payable at the time and place the department designates for receiving bids, to cover the costs of data entry. The department will accept a check or money order payable to: "Wisconsin, Dept. of Transportation."
- (3) In addition to the reasons specified in section 102 of the standard specifications, proposals are irregular and the department may reject them for one or more of the following:
 1. The bidder fails to provide the written request for waiver of the electronic submittal requirements.
 2. The bidder fails to pay the \$75 administrative fee before the time the department designates for the opening of bids unless the bidder requests on the waiver that they be billed for the \$75.
 3. The bidder exceeds 4 waivers of electronic submittal requirements within a calendar year.
- (4) In addition to the reasons specified in section 102 of the standard specifications, the department may refuse to issue bidding proposals for future contracts to a bidding entity that owes the department administrative fees for a waiver of electronic submittal requirements.

PROPOSAL BID BOND

DT1303 1/2006

Wisconsin Department of Transportation

Proposal Number	Project Number	Letting Date
Name of Principal		
Name of Surety	State in Which Surety is Organized	

We, the above-named Principal and the above-named Surety, are held and firmly bound unto the State of Wisconsin in the sum equal to the Proposal Guaranty for the total bid submitted for the payment to be made; we jointly and severally bind ourselves, our heirs, executors, administrators, successors and assigns. The condition of this obligation is that the Principal has submitted a bid proposal to the State of Wisconsin acting through the Department of Transportation for the improvement designated by the Proposal Number and Letting Date indicated above.

If the Principal is awarded the contract and, within the time and manner required by law after the prescribed forms are presented for signature, enters into a written contract in accordance with the bid, and files the bond with the Department of Transportation to guarantee faithful performance and payment for labor and materials, as required by law, or if the Department of Transportation shall reject all bids for the work described, then this obligation shall be null and void; otherwise, it shall be and remain in full force and effect. In the event of failure of the Principal to enter into the contract or give the specified bond, the Principal shall pay to the Department of Transportation **within 10 business days of demand** a total equal to the Proposal Guaranty as liquidated damages; the liability of the Surety continues for the full amount of the obligation as stated until the obligation is paid in full.

The Surety, for value received, agrees that the obligations of it and its bond shall not be impaired or affected by any extension of time within which the Department of Transportation may accept the bid; and the Surety does waive notice of any such extension.

IN WITNESS, the Principal and Surety have agreed and have signed by their proper officers and have caused their corporate seals to be affixed this date: **(DATE MUST BE ENTERED)**

PRINCIPAL

(Company Name) **(Affix Corporate Seal)**

(Signature and Title)

(Company Name)

(Signature and Title)

(Company Name)

(Signature and Title)

(Company Name)

(Signature and Title)

(Name of Surety) **(Affix Seal)**

(Signature of Attorney-in-Fact)

NOTARY FOR PRINCIPAL

(Date)

State of Wisconsin)
) ss.
_____ County)

On the above date, this instrument was acknowledged before me by the named person(s).

(Signature, Notary Public, State of Wisconsin)

(Print or Type Name, Notary Public, State of Wisconsin)

(Date Commission Expires)

Notary Seal

NOTARY FOR SURETY

(Date)

State of Wisconsin)
) ss.
_____ County)

On the above date, this instrument was acknowledged before me by the named person(s).

(Signature, Notary Public, State of Wisconsin)

(Print or Type Name, Notary Public, State of Wisconsin)

(Date Commission Expires)

Notary Seal

IMPORTANT: A certified copy of Power of Attorney of the signatory agent must be attached to the bid bond.

CERTIFICATE OF ANNUAL BID BOND

DT1305 8/2003

Wisconsin Department of Transportation

Time Period Valid (From/To)
Name of Surety
Name of Contractor
Certificate Holder Wisconsin Department of Transportation

This is to certify that an annual bid bond issued by the above-named Surety is currently on file with the Wisconsin Department of Transportation.

This certificate is issued as a matter of information and conveys no rights upon the certificate holder and does not amend, extend or alter the coverage of the annual bid bond.

Cancellation: Should the above policy be cancelled before the expiration date, the issuing surety will give thirty (30) days written notice to the certificate holder indicated above.

(Signature of Authorized Contractor Representative)

(Date)

DECEMBER 2000

**CERTIFICATION REGARDING DEBARMENT, SUSPENSION, AND OTHER
RESPONSIBILITY MATTERS - PRIMARY COVERED TRANSACTIONS**

Instructions for Certification

1. By signing and submitting this proposal, the prospective contractor is providing the certification set out below.
2. The inability of a person to provide the certification required below will not necessarily result in denial of participation in this covered transaction. The prospective contractor shall submit an explanation of why it cannot provide the certification set out below. The certification or explanation will be considered in connection with the department or agency's determination whether to enter into this transaction. However, failure of the prospective contractor to furnish a certification or an explanation shall disqualify such person from participation in this transaction.
3. The certification in this clause is a material representation of fact upon which reliance was placed when the department determined to enter into this transaction. If it is later determined that the contractor knowingly rendered an erroneous certification in addition to other remedies available to the Federal Government the department may terminate this transaction for cause or default.
4. The prospective contractor shall provide immediate written notice to the department to whom this proposal is submitted if at any time the prospective contractor learns that its certification was erroneous when submitted or has become erroneous by reason of changed circumstances.
5. The terms "covered transaction," "debarred," "suspended," "ineligible," "lower tier covered transaction," "participant," "person," "primary covered transaction," "principal," "proposal," and "voluntarily excluded," as used in this clause, have the meanings set out in the Definitions and Coverage sections of the rules implementing Executive Order 12549. You may contact the department to which this proposal is being submitted for assistance in obtaining a copy of those regulations.
6. The prospective contractor agrees by submitting this proposal that, should this contract be entered into, it shall not knowingly enter into any lower tier covered transaction with a person who is debarred, suspended, declared ineligible, or voluntarily excluded from participation in this covered transaction, unless authorized by the department entering into this transaction.
7. The prospective contractor further agrees by submitting this proposal that it will include the clause titled "Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion-Lower Tier Covered Transaction," which is included as an addendum to PR-1273 - "Required Contract Provisions Federal Aid Construction Contracts," without

modification, in all lower tier covered transactions and in all solicitations for lower tier covered transactions.

8. The contractor may rely upon a certification of a prospective subcontractor/materials supplier that it is not debarred, suspended, ineligible, or voluntarily excluded from the covered transaction, unless it knows that the certification is erroneous. A contractor may decide the method and frequency by which it determines the eligibility of its principals. Each contractor may, but is not required to, check the Disapproval List (telephone # 608/266/1631).
9. Nothing contained in the foregoing shall be construed to require establishment of a system of records in order to render in good faith the certification required by this clause. The knowledge and information of a contractor is not required to exceed that which is normally possessed by a prudent person in the ordinary course of business dealings.
10. Except for transactions authorized under paragraph 6 of these instructions, if a contractor in a covered transaction knowingly enters into a lower tier covered transaction with a person who is suspended, debarred, ineligible or voluntarily excluded from participation in this transaction, in addition to other remedies available to the Federal Government, the department may terminate this transaction for cause or default.

Certification Regarding Debarment, Suspension, and Other Responsibility Matters - Primary Covered Transactions

- (1) The prospective contractor certifies to the best of its knowledge and belief, that it and its principals:
 - (a) Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any Federal department or agency;
 - (b) Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements or receiving stolen property;
 - (c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offense enumerated in paragraph (1)(b) of this certification; and
 - (d) Have not within a three-year period preceding this proposal had one or more public transactions (Federal, State or local) terminated for cause or default.
- (2) Where the prospective contractor is unable to certify to any of the statements in this certification, such prospective contractor shall attach an explanation to this proposal.

Special Provisions

Table of Contents

Article	Description	Page #
1.	General.....	2
2.	Scope of Work.....	2
3.	Prosecution and Progress.....	2
4.	Lane Rental Fee Assessment.....	3
5.	Traffic.....	3
6.	Holiday and Special Event Work Restrictions.....	4
7.	Utilities.....	4
8.	Public Convenience and Safety.....	5
9.	Hauling Restrictions.....	5
10.	Erosion Control.....	5
11.	Available Documents.....	6
12.	Notice to Contractor, Return of Existing Traffic Control Devices.....	6
13.	Notice to Contractor, Notification of Demolition and/or Renovation No Asbestos Found.....	6
14.	Removing Structure B-67-86, Item 203.0220.....	7
15.	Expansion Device, Item 502.3101.....	7
16.	Preparation and Coating of Top Flanges B-67-86, Item 517.0901.S.....	8
17.	Structure Overcoating Cleaning and Priming B-67-86, Item 517.3001.S.....	9
18.	Containment and Collection of Waste Materials B-67-86, Item 517.4001.S.....	10
19.	Nighttime Work Lighting-Stationary.....	11
20.	Heat Straightening of Damaged Girders, Item SPV.0060.01.....	12
21.	Counterweight Structure, Item SPV.0060.02.....	15

STSP'S Revised January 7, 2022

SPECIAL PROVISIONS

1. General.

Perform the work under this construction contract for Project 2025-15-71, Pewaukee to Milwaukee, Pilgrim Rd/Capitol Dr B-67-0086, CTH YY, Waukesha County, Wisconsin as the plans show and execute the work as specified in the State of Wisconsin, Department of Transportation, Standard Specifications for Highway and Structure Construction, 2022 Edition, as published by the department, and these special provisions.

If all or a portion of the plans and special provisions are developed in the SI metric system and the schedule of prices is developed in the US standard measure system, the department will pay for the work as bid in the US standard system.

100-005 (20220107)

2. Scope of Work.

The work under this contract shall consist of partial bridge deck removal and replacement (B-67-86, Pilgrim Rd. over STH 190), girder replacement, heat straightening of damaged girder, and all incidental items necessary to complete the work as shown on the plans and included in the proposal and contract.

104-005 (20090901)

3. Prosecution and Progress.

Begin work within 10 calendar days after the engineer issues a written notice to do so.

Provide the start date to the engineer in writing within a month after executing the contract but at least 14 calendar days before the preconstruction conference. Upon approval, the engineer will issue the notice to proceed within ten calendar days before the approved start date.

To revise the start date, submit a written request to the engineer at least two weeks before the intended start date. The engineer will approve or deny that request based on the conditions cited in the request and its effect on the department's scheduled resources.

Lane Closure Restrictions

The hours allowed for lane closures on STH 190 shall be as follows:

Deck Removal/Heat Straightening/Girder Erection

A full closure of STH 190 (all eastbound lanes) is allowed only for bridge removal, heat straightening, and girder erection:

Weekdays, nightly closures, 8:00 PM to 6:00 AM

Weekends, nightly closures, 8:00 PM to 9:00 AM

Forming, Stripping, Painting

Weekdays (Monday – Friday)

- Single lane closures allowed anytime
- Double lane closures allowed daily from 8:30 AM to 3:00 PM and nightly from 7:00 PM to 6:00 AM

Weekends (Saturday – Sunday)

- Double lane closures allowed anytime

Notifications

Notify the City of Brookfield Fire Department, David Mason, at (262) 787-3645, and the City of Brookfield Chief of Police, James Adlam, at (262) 787-3700, two weeks prior to the beginning of construction to discuss issues caused by construction.

4. Lane Rental Fee Assessment.

A General

The contract designates some lane closures to perform the work. The contractor will not incur a Lane Rental Fee Assessment for closing lanes during the allowable lane closure times. The contractor will incur a Lane Rental Fee Assessment for each lane closure outside of the allowable lane closure times. If a lane is obstructed at any time due to contractor operations, it is considered a closure. The purpose of lane rental is to enforce compliance of lane restrictions and discourage unnecessary closures.

The allowable lane closure times are shown in the Prosecution and Progress article.

Submit the dates of the proposed lane, ramp, and roadway restrictions to the engineer as part of the progress schedule.

B Lane Rental Fee Assessment

The Lane Rental Fee Assessment incurred for each lane closure, each ramp closure, and each full closure of a roadway, per direction of travel, is as follows:

Lane Rental for STH 190 full closure during deck demolition

- \$600 per lane, per direction of travel, per hour broken into 15-minute increments

Lane Rental for double lane closures along STH 190

- \$1,200 per lane, per direction of travel, per hour broken into 15-minute increments

The Lane Rental Fee Assessment represents a portion of the cost of the interference and inconvenience to the road users for each closure. All lane, roadway, or ramp closure event increments 15 minutes and less will be assessed as a 15-minute increment.

The engineer, or designated representative, will be the sole authority in determining time period length for the Lane Rental Fee Assessment.

Lane Rental Fee Assessments will not be assessed for closures due to crashes, accidents, or emergencies not initiated by the contractor.

The department will assess Lane Rental Fee Assessment by the dollar under the administrative item Failing to Open Road to Traffic. The total dollar amount of Lane Rental Fee Assessment will be computed by multiplying the Lane Rental Assessment Rate by the number of 15-minute increments of each lane closure event as described above.

Lane Rental Fee Assessment will be in effect from the time of the Notice to Proceed until the department issues final acceptance. If interim completion time or contract time expires before the completion of specified work in the contract, additional liquidated damages will be assessed as specified in standard spec 108.11 or as specified within this contract.

stp-108-065 (20161130)

5. Traffic.

General

Prior to beginning operations under this contract, provide in writing the proposed schedule of operations and methods of coordination and handling traffic, to the engineer.

Construct the project using the construction staging and traffic control shown in the plans and standard detail drawings.

Keep open travel lanes free from mud, sand, and other construction debris at all times.

Access

The contractor shall provide access to all businesses and residences within the project limits at all times during construction.

Traffic Control

No operations shall proceed until all traffic control devices for such work are in the proper location.

Drums or barricades shall be used to delineate local traffic and protect hazards in the work zone, such as exposed manholes or drop-offs for vehicles and pedestrians. The use of such devices shall be incidental to the operation that creates the hazard.

Conflicting signs shall be completely covered by the contractor.

Prior to all lane closures, traffic control devices and signs shall be completely installed according to the traffic control staging sheets or as directed by the engineer.

Wisconsin Lane Closure System Advance Notification

Provide the following advance notification to the engineer for incorporation into the Wisconsin Lane Closure System (LCS).

TABLE 108-1 CLOSURE TYPE AND REQUIRED MINIMUM ADVANCE NOTIFICATION

Closure type with height, weight, or width restrictions (available width, all lanes in one direction < 16 feet)	MINIMUM NOTIFICATION
Lane and shoulder closures	7 calendar days
Full roadway closures	7 calendar days
Ramp closures	7 calendar days
Detours	7 calendar days
Closure type without height, weight, or width restrictions (available width, all lanes in one direction ≥ 16 feet)	MINIMUM NOTIFICATION
Lane and shoulder closures	3 business days
Ramp closures	3 business days
Modifying all closure types	3 business days

Discuss LCS completion dates and provide changes in the schedule to the engineer at weekly project meetings in order to manage closures nearing their completion date.

6. Holiday and Special Event Work Restrictions.

Do not perform work on, nor haul materials of any kind along or across any portion of the highway carrying STH 190 (Capitol Drive) or CTH YY (Pilgrim Road) traffic, and entirely clear the traveled way and shoulders of such portions of the highway of equipment, barricades, signs, lights, and any other material that might impede the free flow of traffic during the following holiday and special event periods:

- From noon Friday, September 2, 2022 to 6:00 AM Tuesday, September 6, 2022 for Labor Day;
- From noon Wednesday, November 23, 2022 to 6:00 AM Monday, November 28, 2022 for Thanksgiving.

stp-107-005 (20210113)

7. Utilities.

This contract comes under the provision of Administrative Rule Trans 220.

Some of the utility work described below is dependent on prior work being performed by the contractor at a specific site. In such situations, provide the engineer and the affected utility a good faith notice of when the utility is to start work at the site. Provide this notice 14 to 16 calendar days in advance of when the prior work will be completed, and the site will be available to the utility owner. Follow-up with a confirmation notice to the engineer and the utility owner not less than three working days before the site will be ready for the utility owner to begin its work.

Coordinate construction activities with a call to Diggers Hotline or a direct call to the utilities that have facilities in the area as required per statutes. Use caution to ensure the integrity of underground facilities and maintain code clearances from overhead facilities at all times.

Contact each utility company listed in the plans prior to preparing bids to obtain current information on the status of existing and any newly relocated utility facilities within the project limits.

The following utility companies have facilities within the project area that need adjustments:

Spectrum (Charter) – Communications Spectrum will transfer facilities to new WE-Energies pole at Station 18+38, 95' LT prior to construction.

WE Energies – Electric has facilities within the project limits. 20 working days will be required to complete the following work prior to construction:

A new pole will be installed at Station 18+38, 95' LT. Pole at Station 18+38, 91' LT will be removed. The electrical span between the poles at Station 18+38, 91' LT and Station 18+37, 100' RT will be removed and reinstalled following constructions.

The following utility companies have facilities within the project area; however, no adjustments are anticipated:

AT&T Wisconsin – Communications

Brookfield Water Utility - Water

City of Brookfield – Road Facility

City of Brookfield – Sewer

Everstream - Communications

Level 3 – Communications

WisDOT - Signal

WE Energies – Gas

8. Public Convenience and Safety.

Revise standard spec 107.8(6) as follows:

Check for and comply with local ordinances governing the hours of operation of construction equipment. Do not operate motorized construction equipment from 7:00 PM until the following 7:00 AM, unless prior written approval is obtained from the engineer or as stated in Prosecution and Progress. The City of Brookfield local zoning noise ordinances is affective from 12:00 AM to 7:00 AM. Coordinate with Jason Herzog (262) 787-7541 at the City of Brookfield two weeks for an exemption to the ordinance.

9. Hauling Restrictions.

At all times, conduct operations in a manner that will cause a minimum of inconvenience to the free flow of vehicles on roadways carrying STH 190 (Capitol Drive) and side road traffic. Do not haul on local roads without prior approval from the engineer.

10. Erosion Control.

Supplement standard spec 107.20 with the following:

Erosion control best management practices (BMP's) consists of Inlet Protection Type D at engineer identified locations. There are no anticipated ground disturbing activities as part of this project.

Install erosion control as the engineer directs. Do not begin construction operations until erosion control is installed.

Maintain Erosion Control BMP's until permanent vegetation is established or until the engineer determines that the BMP is no longer required.

Stockpile excess materials or spoils on upland areas away from wetlands, floodplains, and waterways. Immediately install perimeter silt fence protection around stockpiles. If stockpiled materials will be left for more than 14 days, install temporary seed or other temporary erosion control measures the engineer orders.

When performing sawcutting operations, concrete or asphalt slurry shall be squeegeed off to the shoulder gravel or shoveled behind the curb into the gravel base. Slurry of any kind shall not be allowed into storm sewers, ditches, waterways, or wetlands. The contractor shall cover storm sewer inlets in the work zone during the operation and provide a method of removing and disposing of this material. This shall be described in the ECIP.

When performing roadway cleaning operations, the contractor shall use equipment having vacuum or water spray mechanism to eliminate the dispersion of dust. If vacuum equipment is employed, it shall have suitable self-contained particulate collectors to prevent discharge from the collection bin in the atmosphere.

SER-107.3 (20161220) EROC

11. Available Documents.

The department will make its information available to bidding contractors. The list of documents that are available for contractors' information includes:

- Structure Inspection Reports

These documents are available from Janet Cannon at 141 NW Barstow Street, Waukesha, WI 53187, (262) 347-5323.

Reproduction costs will be applied to all copies requested.

sef-102-005 (20170310)

12. Notice to Contractor, Return of Existing Traffic Control Devices.

Waukesha County Department of Public Works has installed traffic control devices on Structure B-67-86 to prevent additional loading on the damaged girders. These devices shall be returned to Waukesha County prior to the start of the project.

Contact Gary Wolf, WisDOT County Maintenance Coordinator, (414) 750-2715, gary1.wolf@dot.wi.gov or Hans Guderyon, Waukesha County Patrol Supt., (262) 424-9003, hguderyon@waukeshacounty.gov at least three days prior to the start of the project to coordinate pick up of existing traffic control devices. Contractor must have replacement traffic control devices on site prior to removing Waukesha County owned traffic control devices.

13. Notice to Contractor, Notification of Demolition and/or Renovation No Asbestos Found.

John Roelke, License Number All-119523, inspected Structure B-67-86 for asbestos on April 18, 2022. No regulated Asbestos Containing Material (RACM) was found on this structure. A copy of the inspection report is available from Janet Cannon, (262) 347-5323.

According to NR447 and DHS159, ensure that DNR or DHS receives a completed Notification of Demolition and/or Renovation (DNR Form 4500-113 (R 4/11), or subsequent revision) via U.S. mail, hand-delivery, or using the online notification system at least 10 working days before beginning any construction or demolition. Pay all associated fees. Provide a copy of the completed 4500-113 form to Janet Cannon (262) 347-5323 and DOT BTS-ESS attn: Hazardous Materials Specialist, PO Box 7965, Madison, WI 53707-7965. In addition, comply with all local or municipal asbestos requirements.

Use the following information to complete WisDNR form 4500-113:

- Site Name: Structure B-67-86, CTH YY (Pilgrim Rd) over STH-190 (Capitol Dr)
- Site Address: 1.0 MI S JCT CTH K and 2.6 MI W JCT USH 45 TO N
- Ownership Information: WisDOT Transportation Southeast Region, 141 NW Barstow St, PO Box 798, Waukesha, WI, 53187-0798
- Contact: Kurt Flierl
- Phone: (414) 750-3085
- Age: 57 years old. This structure was constructed in 1965.
- Area: 8895 SF of deck

Insert the following paragraph in Section 6.g.:

If asbestos not previously identified is found or previously non-friable asbestos becomes crumbled, pulverized, or reduced to a powder, stop work immediately, notify the engineer, and the engineer will notify the department's Bureau of Technical Services at (608) 266-1476 for an emergency response as specified in standard spec 107.24. Keep material wet until it is abated or until it is determined to be non-asbestos containing material.

stp-107-125 (20120615)

14. Removing Structure B-67-86, Item 203.0220.

Provide a mat, sand, or another material approved by the engineer to protect the existing State Trunk Highway 190 (Capitol Drive) concrete pavement from falling debris during the removal of the existing structure.

All costs associated with the work described in this specification shall be included in the Removing Structure bid item.

15. Expansion Device, Item 502.3101.

A Description

This special provision describes furnishing and installing an expansion device as the plans show conforming to standard spec 502 as modified in this special provision.

B Materials

Furnish components for the Expansion Device System from an approved fabricator selected from the department's approved product list of Fabricated Bridge Components – Expansion Devices.

To be eligible for this project, expansion devices from other manufacturers must be pre-approved before the bid closing date. Applications for pre-approval may be submitted at any time. Prepare the application according to the department requirements. If needed, obtain information and assistance with the pre-approval process from the Structures Maintenance Section in the Bureau of Structures, by sending an email to the following address: DOTDLStructuresFabrication@dot.wi.gov

Expansion device strip seal gland size requirement of 4-inches, 5-inches, and 6-inches shall be as the plans show.

The minimum thickness of the polychloroprene strip seal shall be 1/4 inch for non-reinforced elastomeric glands and 1/8 inch for reinforced glands. Furnish the strip seal gland in lengths suitable for a continuous one-piece installation at each individual expansion joint location. Provide preformed polychloroprene strip seals that conform to the requirements ASTM D3542, and have the following physical properties:

Property Requirements	Value	Test Method
Tensile Strength, min.	2000 psi	ASTM D412
Elongation @ Break, min	250%	ASTM D412
Hardness, Type A, Durometer	55 ± 5 pts.	ASTM D2240
Compression Set, 70 hours @212°F, max.	35%	D395 Method B Modified
Ozone Resistance, after 70 hrs. at 100°F under 20% Strain with 100 pphm ozone	No Cracks	ASTM D1149 Method A
Mass Change in Oil 3 after 70 hr. 212°F Mass Change, max.	45%	ASTM D471

Install the elastomeric strip seal gland with tools and a lubricant adhesive recommended by the manufacturer.

Furnish manufacturer's certification for production of polychloroprene represented showing test results for the cured material supplied and certifying that it meets all specified requirements.

The steel extrusion or retainer shall conform to ASTM designation A 709 grade 36 steel. After fabrication, steel shall be galvanized conforming to the requirements ASTM A123.

Manufacturer's certifications for adhesive and steel shall attest that the materials meet the specification requirements.

stp-502-020 (20210113)

16. Preparation and Coating of Top Flanges B-67-86, Item 517.0901.S.

A Description

This special provision describes thoroughly cleaning and coating the top surface and edges of the top flanges, removing loose paint, rust, mill scale, dirt, oil, grease, or other foreign substances until the specified finish is obtained.

B (Vacant)

C Construction

For top flanges and edges that have no paint on them and according to the department's Pre-Qualified Paint Systems for Structure Overcoating Cleaning and Priming, clean the top surface and edges of the top flanges and paint them with one coat of an approved zinc rich primer. Paint for Solvent Cleaning for Overcoat-minimum Cleaning (SP-1) is not allowed.

For top flanges and edges that have paint on them and according to the department's Pre-Qualified Paint Systems for Structure Overcoating Cleaning and Priming, clean all areas of rust and loose paint on the top surface and edges of the top flanges. Wash the top surface and edges of the top flanges and paint them with one coat of an approved zinc-rich primer according to paint manufacture's recommendations. If flash rusting occurs before the application of the primer, stop painting application, remove the flash rusting and paint cleaned surface. Paint for Solvent Cleaning for Overcoat-minimum Cleaning (SP-1) is not allowed.

Where plans call for the cleaning of other painted structural steel including hanger assemblies, bearings, field splices, and connections, clean areas of loose paint and rust according to the department's Pre-Qualified Paint Systems for Structure Overcoating Cleaning and Priming, or and according to paint manufacture's cleaning recommendations. Sound paint need not be removed with the exception of an area 12 inch on either side of hanger assembly centerlines. Clean this area to base metal according to the paint manufacture's cleaning recommendations and paint them one coat of an approved zinc-rich primer according to paint manufacture's recommendations. Paint for Solvent Cleaning for Overcoat-minimum Cleaning (SP-1) is not allowed.

For areas of exposed steel members that are to be imbedded in new concrete and according to the department's Pre-Qualified Paint Systems for Structure Overcoating Cleaning and Priming, thoroughly clean the surface area of exposed steel members that are to be imbedded in the new concrete and solvent wash and paint one coat of an approved zinc rich primer according to paint manufacture's recommendations to these areas. Paint for Solvent Cleaning for Overcoat-minimum Cleaning (SP-1) is not allowed.

According to the approved project specific hazardous material containment plan, furnish and erect tarpaulins or other materials to collect all of the spent paint containing material resulting from blasting or hand and power tool cleaning and coating. Minimize dust during all clean-up activities. Collect and store waste material at the end of each work day or more often if needed. Store waste materials in the hazardous waste containers provided. Lock and secure all waste containers at the end of each work day. Cover containers at all times except when adding or removing waste material. Store the containers in an accessible and secured area, not located in a storm water runoff course, flood plain or exposed to standing water. Transportation and disposal of such waste material will be the responsibility of the department.

Damage to existing painted surfaces as a result of construction operations, shall be restored to the approval of the engineer at the contractor's expense.

D Measurement

The department will measure Preparation and Coating of Top Flanges (B-67-86) as a single unit for each structure, acceptably completed.

E Payment

The department will pay for measured quantities at the contract unit price under the following bid item:

ITEM NUMBER	DESCRIPTION	UNIT
517.0901.S	Preparation and Coating of Top Flanges B-67-86	EACH

Payment is full compensation for preparing and cleaning the designated surfaces; and for furnishing and applying the coating.

stp-517-010 (20210708)

17. Structure Overcoating Cleaning and Priming B-67-86, Item 517.3001.S.

A Description

This special provision describes cleaning and painting with two or three coats of paint the metal surfaces.

A.1 Areas to be Cleaned and Painted

Structure B-67-86

1. Two Coat Area: 0 SF with SP 1 cleaning.
2. Three Coat Area:
 - 0 SF with SP 2 cleaning.
 - 200 SF with SP 3 cleaning.
 - 0 SF with SP 11 cleaning.
 - 0 SF with SP 15 cleaning.
 - 200 SF total three-coat area.

B Materials

Furnish an epoxy coating system from the department's APL for Paint- structure maintenance.

C Construction

C.1 Surface Preparation

Before overcoating or power tool cleaning, solvent clean all surfaces to be coated according to SSPC-SP1. A SSPC-SP 3 power Tool Cleaning according to Steel Structures Painting Council Specification 3 will be required on all metal surfaces to be painted with a three-coat system. Prime the same day, or re-clean before application, all metal surfaces receiving a No. 3 cleaning.

Remove all abrasive or paint residue from steel surfaces with a High Efficiency Particulate Abatement (HEPA-VAC) vacuum cleaner equipped with a brush-type cleaning tool, or by double blowing. If the double blowing method is used, vacuum the exposed top surfaces of all structural steel, including flanges, longitudinal stiffeners, splices, plates, and hangers, after the double blowing operations are completed. The air line used for blowing the steel clean shall have an inline water trap and the air shall be free of oil and water as it leaves the air line.

Take care to protect freshly coated surfaces from subsequent cleaning operations. Thoroughly wire brush damaged primed surfaces with a non-rusting tool. Clean and re-prime the brushed surfaces within the time recommended by the manufacturer.

C.2 Painting

Paint by applying two or three coats of an approved coating system as specified herein to the surfaces as described in A.1 from the department's approved products list.

C.3 Coating Application

Apply paint in a neat, workmanlike manner. The resultant paint film shall be smooth and uniform without skips or areas of excessive paint. Apply coating according to the manufacturer's recommendations.

Before applying the prime coat, coat with primer all edges, rivet and bolt heads, nuts and washers by using either a brush, roller, or spray application.

Dry Film Thickness per coat shall be a minimum of 3-mil. The dry film thickness shall be determined by use of a magnetic film thickness gage. The gage shall be calibrated for dry film thickness measurement according to SSPC-PA 2.

During surface preparation and coating application, the ambient and steel temperature shall be between 39 and 100 degrees F. The steel temperature shall be at least 5 degrees F above the dew point temperature, and the relative humidity shall not exceed 85%.

D Measurement

The department will measure Structure Overcoating Cleaning and Priming B-67-86 as a single unit for each structure, acceptably completed.

E Payment

The department will pay for measured quantities at the contract unit price under the following bid item:

ITEM NUMBER	DESCRIPTION	UNIT
517.3001.S	Structure Overcoating Cleaning and Priming B-67-86	EACH

Payment is full compensation for preparing and cleaning the designated surfaces; and for furnishing and applying the paint.

stp-517-036 (20210708)

18. Containment and Collection of Waste Materials B-67-86, Item 517.4001.S.

A Description

This special provision describes furnishing and erecting tarpaulins to contain, collect and store the spent material from surface preparation of steel surfaces, collecting such spent material, and labeling and storing the spent material in waste containers.

B Materials

Provide 5-gallon lidded plastic containers for containing the spent material.

C Construction

Erect tarpaulins or other materials to collect all of the spent material from power tool cleaning. Consider and treat all spent material as hazardous waste because it contains lead.

Collect and store all waste material collected by this operation at the bridge site for disposal. Collect and store all waste materials at the end of each workday or more often if needed. Store materials in 5-gallon lidded plastic containers.

Label each container with the date the first waste was placed in the container and the words "Hazardous Waste – EPA Waste Code D008." Lock and secure all containers at the end of each workday. Keep the containers covered at all times except to add or remove waste material. Store the containers in an accessible and secured area, not located in a storm water runoff course, flood plain or exposed to standing water.

Collect the spent debris by vacuuming, shoveling, sweeping, or by channeling it directly to disposal containers. The enclosure shall be thoroughly cleaned at the end of each work day.

D Measurement

The department will measure Containment and Collection of Waste Materials B-67-86 as a single unit for each structure, acceptably completed.

E Payment

The department will pay for measured quantities at the contract unit price under the following bid item:

ITEM NUMBER	DESCRIPTION	UNIT
517.4001.S	Containment and Collection of Waste Materials B-67-86	EACH

Payment is full compensation for designing, erecting, operating, maintaining and disassembling the containment devices; collecting, labeling and storing spent materials in appropriate containers.

stp-517-037 (20210708)

19. Nighttime Work Lighting-Stationary.

A Description

This special provision describes furnishing portable lighting as necessary to complete nighttime work. Nighttime operations consist of work specifically scheduled to occur after sunset and before sunrise.

B (Vacant)

C Construction

C.1 General

This provision shall apply when providing, maintaining, moving, and removing portable light towers and equipment-mounted lighting fixtures for nighttime stationary work operations, for the duration of nighttime work on the contract.

At least 14 days before the nighttime work, furnish a lighting plan to the engineer for review and acceptance. Address the following in the plan:

1. Layout, including location of portable lighting – lateral placement, height, and spacing. Clearly show on the layout the location of all lights necessary for every aspect of work to be done at night.
2. Specifications, brochures, and technical data of all lighting equipment to be used.
3. The details on how the luminaires will be attached.
4. Electrical power source information.
5. Details on the louvers, shields, or methods to be employed to reduce glare.
6. Lighting calculations. Provide illumination with average to minimum uniformity ratio of 5:1 or less throughout the work area.
7. Detail information on any other auxiliary equipment.

C.2 Portable Lighting

Provide portable lighting that is sturdy and free standing and does not require any guy wires, braces, or any other attachments. Furnish portable lighting capable of being moved as necessary to keep up with the construction project. Position the portable lighting and trailers to minimize the risk of being impacted by traffic on the roadway or by construction traffic or equipment. Provide lightning protection for the portable lighting. Portable lighting shall withstand up to 60 mph wind velocity.

If portable generators are used as a power source, furnish adequate power to operate all required lighting equipment without any interruption during the nighttime work. Provide wiring that is weatherproof and installed according to local, state, federal (NECA and OSHA) requirements. Equip all power sources with a ground-fault circuit interrupter to prevent electrical shock.

C.3 Light Level and Uniformity

Position (spacing and mounting height) the luminaires to provide illumination with an average to minimum uniformity ratio of 5:1 or less throughout the work area.

Illuminate the area as necessary to incorporate construction vehicles, equipment, and personnel activities.

C.4 Glare Control

Design, install, and operate all lighting supplied under these specifications to minimize or avoid glare that interferes with all traffic on the roadway or that causes annoyance or discomfort for properties adjoining the roadway. Locate, aim, and adjust the luminaires to provide the adequate level of illumination and the specified uniformity in the work area without the creation of objectionable glare.

Provide louvers, shields, or visors, as needed, to reduce any objectionable levels of glare. As a minimum, ensure the following requirements are met to avoid objectionable glare on the roadways open to traffic in either direction or for adjoining properties:

1. Aim tower-mounted luminaires, either parallel or perpendicular to the roadway, so as to minimize light aimed toward approaching traffic.
2. Aim all luminaires such that the center of beam axis is no greater than 60 degrees above vertical (straight down).

If lighting does not meet above-mentioned criteria, adjust the lighting within 24 hours.

C.5 Continuous Operation

Provide and have available sufficient fuel, spare lamps, generators, and qualified personnel to ensure that the lights will operate continuously during nighttime operation. In the event of any failure of the lighting system, discontinue the operation until the adequate level of illumination is restored. Move and remove lighting as necessary.

D (Vacant)

E Payment

Costs for furnishing a lighting plan, and for providing, maintaining, moving, and removing portable lighting, tower mounted lighting, and equipment-mounted lighting required under this special provision are incidental to the contract.

stp-643-010 (20100709)

20. Heat Straightening of Damaged Girders, Item SPV.0060.01.

A Description

This special provision describes heat straightening portions of bent or damaged girders which are left in place, back to their original shape. Straighten the girders back into their original position and shape within the tolerances listed below or as necessary to mate the existing and new work.

B (Vacant)

C Construction

C.1 Contractor Qualifications

The contractor's personnel performing the heat straightening shall have at least 5 years of experience in conducting heat-straightening repairs to primary structural members of in-service damaged steel structures. During the immediately preceding three-year period, the personnel shall have conducted at least three successful heat-straightening projects. The contractor's personnel performing NDT testing shall have at least 5 years of experience in inspection and repairs of structural steel.

A minimum of 15 working days prior to the pre-construction meeting, submit to the engineer for approval a report documenting the experience of the personnel and the projects worked on including the date, location, bridge owner, number and type of members straightened, and duration of each project, along with contact names, current phone numbers and e-mail addresses. Include relevant information, experience and qualifications of the firm completing NDT testing for acceptance by the department.

C.2 Existing Paint Removal

Remove existing paint according to SSPC-SP15 Commercial Grade Power Tool Cleaning or equal. Remove all existing paint, mill scale, and rust within the heat affected zone except that up to 33% staining from rust and mill scale is permitted to remain. Remove paint as necessary to perform inspections and straightening work and to the engineer's satisfaction. Feather the edges of remaining old paint so that the repainted surface has a reasonably smooth appearance.

C.3 Grinding Flange Edges

Round all damaged/impacted exposed corners of main members as necessary to achieve a 1/16-inch radius or equivalent flat surface at a 45 degree angle. Grind edges at all locations of planned work to prevent edge cracking during the straightening work and to the engineer's satisfaction

C.4 Damage Inspection

Visually inspect all areas of damage, suspected damage, yield lines and zones of plastic bending. Also inspect all secondary members and connections between main and secondary members that potentially distributed forces causing damage. Perform this work with inspected surfaces being within approximately 24 inches from the inspector. Use access equipment, illumination, and nondestructive testing as necessary to identify, measure and document the location and details of buckling; crimps; misalignment; twists; tears; burrs; damaged edges; punched holes; pull out of secondary members; cracks or other physical distress. Remove existing paint and test using magnetic particle testing all areas of detected and suspected hairline cracking according to the procedures and techniques for dry powder magnetic particle examination using the yoke method, ASTM E709, Practices for Magnetic Particle Examination.

C.5 Straightening Work Plan

Use field data from the damage inspection to develop a straightening work plan. The work plan should include documentation of the contractor's means and methods including jacking or bracing plans; surface preparation methods; calculation and control of allowable jacking and pulling forces; heating methods and shapes; heating equipment; and temperature indicating devices. Submit the work plan to the engineer a minimum of 5 days prior to beginning heat straightening.

C.6 Straightening Damaged Members

Perform straightening using methods which will not permanently damage the metal's material properties. Heat members using controlled jacking, pulling or restraining forces; specified heating patterns; and controlled temperatures that result in controlled shrinkage to straighten the member. Do not heat members then use large jacks or pullers which mechanically hot work the material. Mechanical hot working permanently damages the metal's material properties. Prior to straightening a damaged compression member, install adequate bracing to support loads and prevent buckling.

C.7 Restraints or Preloads

Apply jacking, pulling or restraining forces to the damaged member in the direction that tends to straighten the member. Position jacks, pullers, or restraining forces such that heat straightening shrinkage will relieve the force during the cooling cycle. Do not allow jacks, pullers or restraining forces to subject any part of the structure to unit stresses that exceed 50 percent of the material's nominal yield (F_y) at ambient temperature. Provide pressure gages or load cells to control jacks, pullers or restraining forces. Secure jacks, pullers or restraining forces so they do not dislodge during cooling. Apply jacks, pullers or restraining forces prior to heating. Do not apply additional jacking, pulling or restraining forces after beginning the application of heat. Do not apply the next cycle of jacking, pulling or restraining forces until the steel has cooled below 250 °F.

C.8 Application of Heat

Heat opposite faces of a plate or rolled shape concurrently when the material thickness equals or exceeds 1-1/4 inch. When heating thick plates, it may be necessary to interrupt heating for periods of less than one minute to allow the heat to soak into the flange and avoid surface over-heating. Perform heating using single and multi-orifice (rosebud) heating torches sized according to the following table. Manipulate the torches to avoid overheating. Heat using propane, natural gas, or acetylene unless other methods are accepted by the engineer.

Limits on Torch Tip Size		
Steel Thickness	Orifice type	Orifice Size
less than 1/4inch (6mm)	Single	3
3/8 inches (9.5mm)	Single	4
1/2 inch (13mm)	Single	5
5/8 inch (16mm)	Single	7
3/4 inch(19mm)	Single	8
1 inch (25mm)	Single or Rosebud	8 single, 3 rosebud
1 1/4 inch (32mm)	Single or Rosebud: on both sides*	8 single, 3 rosebud
2 inch (51mm)	Single or Rosebud: on both sides*	8 single, 4 rosebud
3 inch (76mm) or greater	Rosebud: on both* sides	5

* - Heat applied concurrently to both sides

C.9 Shape of Heating Patterns

Perform heating using four basic heating patterns: Strip, Line, Spot or "V".

C.10 Temperature Control

Control heat so the internal temperature of the steel does not exceed 1200 °F. The internal temperature of the steel is the surface temperature approximately five seconds after passage of the torch. Control the application of heating so it is confined inside the limits of the four basic heating patterns. Bring the steel within the pattern to the desired temperature as rapidly as possible without overheating.

Control the application of heat by checking the internal temperature of the steel by frequent use of appropriate temperature range indicating crayons or an infrared, non-contact thermometer. The department will require investigative testing for damage to the metal's material properties for any procedure which causes the internal temperature of the steel to exceed the specified maximum heating temperature. Provide the inspector with access to infrared thermometers or heat-indicating crayons as necessary to document and verify compliance with the temperature restrictions as stated in these specifications.

Do not accelerate cooling with water, water mist or other cooling accelerants. After the steel surface temperature is less than 600 °F (315 °C) cooling may be accelerated with dry compressed air. After completing a planned set of heat patterns along the member, do not apply additional heat until the entire member has cooled below 250 °F and the straightening movement has been verified.

C.11 Tolerances

Do not measure dimensional tolerances for final acceptance until all heating and welding operations are completed and the member has cooled to 160 °F or less.

Girder straightness: The difference between the original as-built position and the final repair position when measured from a string line stretched along the member shall conform to the minimum tolerances as stated in the table below:

Tolerances for Heat Straightening Repair

Member Type	Minimum Tolerance ^{1,2}	
	English (in)	SI (mm)
Beams, Truss members, or Columns overall	1/2-in over 20 feet	13 mm over 6 meters
	at impact point	19 mm over 6 meters
Local Web Deviations	d/100 but not less than 1/4-in	d/100 but not less than 6 mm
Local Flange Deviations	b/100 but not less than 1/4-in	b/100 but not less than 6 mm

¹ Units of member depth, d, and flange width, b, are inches and millimeters, respectively, for English and SI units

² Tolerances for curved or cambered members should account for the original shape of the member.

C.12 Final Inspection

Perform a final arms-length inspection of all surfaces that were repaired or heated. Perform the inspection after the work is complete and cooled to 160 °F or less. Perform non-destructive testing at locations of detected or suspected hairline cracking as part of this inspection. Test these areas using magnetic particle testing. Immediately notify the engineer of any cracking found.

D Measurement

The department will measure Heat Straightening of Damaged Girders as a single unit for each project.

E Payment

The department will pay for measured quantities at the contract unit price under the following bid item:

ITEM NUMBER	DESCRIPTION	UNIT
SPV.0060.01	Heat Straightening of Damaged Girders	EACH

Payment is full compensation for heat straightening portions of bent or damaged girders and diaphragm members to the original tolerances as specified; inspecting all suspected areas of cracking by visual and non-destructive testing (NDT); providing temperature-indicating devices to the inspector.

21. Counterweight Structure, Item SPV.0060.02.

A Description

This special provision describes furnishing, placing and removing counterweights on the bridge structure in order to replace damaged sections of the structure according to the pertinent plan details, as directed by the engineer, and as hereinafter provided. The counterweights are required to prevent high stresses from accumulating in the field splice plates when a large portion of dead weight is removed from the structure and also to limit permanent redistribution of dead loads to other portions of the structure when a portion of the continuous-span steel superstructure is removed and replaced.

B Materials

Furnish stationary weights composed of steel plates or crane weights, concrete blocks, or other materials approved by the engineer. Provide elastomeric-rubber or tempered-hardboard-wood bearing pads or strips to level counterweights and provide a uniform contact stress on the existing concrete bridge surfaces. If required, provide a steel base/distribution plate between counterweights and pads of sufficient area, thickness, and strength to limit the contact stress on the existing concrete surfaces to 500 pounds per square inch (PSI) maximum.

C Construction

C.1 Placement

Locate individual counterweights at the locations shown in the plans. Weights may be placed by crane from below/off the existing bridge structure or trucked onto the structure and craned from it, provided all truck loads and cranes remain on the eastern half of the structure east of girder line 3. If the existing structure is used to deliver/place counterweights, contractor shall independently verify the structure can safely support all loads and is not overstressed/damaged during placement given their specific placement procedures. Place counterweights, and portions thereof, according to the sequence specified in the plans.

C.2 Size and Weight

Each individual counterweight shall be contained within the maximum footprint indicated on the plans. Each counterweight shall be within (+/-) 500 pounds (LBS) of the design values indicated on the plans.

C.3 General

Secure the counterweights to existing structure as required to safely stabilize their load and prevent their accidental displacement. Provide any necessary bracing, support framing, or scaffolding required to locate the counterweights at the locations shown in the plans. Utilize bearing pads and base/distribution plates as required to limit the contact stress on the existing concrete surfaces to 500 pounds per square inch and account for any surface irregularities in the existing concrete surfaces.

C.4 Removal

Remove the counterweights at the point in the work sequence specified in the plan. Weights may be removed via a crane located off the structure or loaded and trucked via the existing structure, provided all truck loads and cranes remain on the eastern half of the structure, east of girder line 3. If the existing structure is used to remove counterweights, the contractor shall independently verify the structure can safely support all loads and is not overstressed /damaged during their removal given their specific removal procedures.

D Measurement

The department will measure Counterweight Structure by each counterweight structure, acceptably completed.

E Payment

The department will pay for measured quantities at the contract unit price under the following bid item:

ITEM NUMBER	DESCRIPTION	UNIT
SPV.0060.02	Counterweight Structure	EACH

Payment is full compensation for providing, placing, and removing all counterweights; and for transporting to and from the jobsite.

ADDITIONAL SPECIAL PROVISION 4

This special provision does not limit the right of the department, prime contractor, or subcontractors at any tier to withhold payment for work not acceptably completed or work subject to an unresolved contract dispute.

Payment to First-Tier Subcontractors

Within 10 calendar days of receiving a progress payment for work completed by a subcontractor, pay the subcontractor for that work. The prime contractor may withhold payment to a subcontractor if, within 10 calendar days of receipt of that progress payment, the prime contractor provides written notification to the subcontractor and the department documenting "just cause" for withholding payment.

The prime contractor is not allowed to withhold retainage from payments due subcontractors.

Payment to Lower-Tier Subcontractors

Ensure that subcontracting agreements at all tiers provide prompt payment rights to lower-tier subcontractors that parallel those granted first-tier subcontractors in this provision.

Additional Special Provision 6
ASP 6 - Modifications to the standard specifications

Make the following revisions to the standard specifications:

415.3.16 Tolerance in Pavement Thickness

Replace the entire text with the following effective with the November 2021 letting:

415.3.16.1 General

- (1) Construct the plan thickness or thicker. The department will accept pavement thickness based on the results of department-performed acceptance testing conforming to:

Magnetic Pulse Induction	CMM 870: ASTM E3209 WTM
Probing.....	CMM 870: WTP C-002
Preplacement Measurement	CMM 870: WTP C-003

415.3.16.2 Pavement Units

415.3.16.2.1 Basic Units

- (1) Basic unit is defined as a slip formed, single lane, with a minimum lane width of 10 feet, measured, from the pavement edge to the adjacent longitudinal joint; from one longitudinal joint to the next; or between pavement edges if there is no longitudinal joint.

415.3.16.2.2 Special Units

- (2) Establish special units for areas of fillets, intersections, gaps, gores, shoulders, ramps, pavement lanes less than 10 feet wide and other areas not included in basic units.

415.3.16.3 Test Plate Locations

- (1) Place department-furnished test plates. Within 5 business days after paving, enter the sequential number and associated position data into MRS available at:

<http://www.atwoodsystems.com/>

- (2) Contractor will maintain plate location markings for 10 business days after paving.

415.3.16.4 Acceptance Testing

415.3.16.4.1 Basic Units

415.3.16.4.1.2 Magnetic Pulse Induction

- (1) The department will measure thickness within 10 business days of paving. Upon completion of the project thickness testing, the department will provide the test results to the contractor within 5 business days.
- (2) Department will establish a project reference plate at the start of each paving stage. Project reference plate will be measured before each day of testing. Department will notify the contractor of project reference plate locations before testing.
- (3) If the random plate test result falls within 80 to 50 percent pay range specified in 415.5.2, the department will measure the second plate in that unit. The department will notify the contractor immediately if the average of the 6 readings falls within the 80 to 50 percent pay range.
- (4) If an individual random plate test result is more than 1 inch thinner than contract plan thickness, the pavement is unacceptable. Department will determine limits of unacceptable pavement by performing the following:
- The engineer will test each consecutive plate stationed ahead and behind until the thickness test result is plan thickness or greater.
 - The engineer will direct the contractor to core the hardened concrete to determine the extent of the unacceptable area. In each direction, the contractor shall take cores at points approximately 20 feet from the furthest out of specification plate towards the plate that is plan thickness of greater. Once a core is within 80 to 100 percent pay range, the coring is complete and the limits of unacceptable pavement extend from the stationing between the core test results of 80 to 100 percent payment, inclusive of all unacceptable core and plate test results.
 - The contractor shall perform coring according to AASHTO T24. The department will evaluate the results according to AASHTO T148
 - The contractor shall fill core holes with concrete or mortar.

415.3.16.4.2 Special Units

415.3.16.4.2.1 Magnetic Pulse Induction

- (1) The department will measure thickness within 10 business days of paving. Upon completion of the project thickness testing, the department will provide the test results to the contractor within 5 business days.
- (2) Department will establish a project reference plate at the start of each paving stage. Project reference plate will be measured before each day of testing. Department will notify the contractor of project reference plate locations before testing.
- (3) If the random plate test result falls within 80 to 50 percent pay range specified in 415.5.2, the department will measure the second plate in that unit. The department will notify the contractor immediately if the average of the 6 readings falls within the 80 to 50 percent pay range.
- (4) If an individual random plate test result is more than 1 inch thinner than contract plan thickness, the department will measure the second plate in that unit. If both plates are required to be measured, then all six thickness measurements will be averaged for that unit. If the average of the six measurements is more than 1 inch thinner than contract plan thickness, the pavement is unacceptable.

415.3.16.4.2.2 Probing

- (1) The department will measure slip form special units during concrete placement. Upon completion of the project thickness testing, the department will provide the test results to the contractor within 5 business days.
- (2) Department will probe 2 random locations within the special unit. The average of the two readings will be the reported measurement for the special unit.

415.3.16.4.2.3 Preplacement Measurement

- (1) The department will measure non-slip form special units before concrete placement.
- (2) Thickness corrections will be made to a conforming thickness by reshaping the base aggregate before the pavement is placed.

415.5.2 Adjusting Pay for Thickness

Replace the entire text with the following effective with the November 2021 letting:

- (1) The department will adjust pay for pavement thickness under the Nonconforming Thickness Concrete Pavement administrative item as follows:

FOR PAVEMENT THINNER THAN PLAN THICKNESS BY:	PERCENT OF THE CONTRACT UNIT PRICE
> 1/4 inch but <= 1/2 inch	80
> 1/2 inch but <= 3/4 inch	60
> 3/4 inch but <= 1 inch	50

- (2) When pavement of unacceptable final thickness is determined, as specified in 415.3.16.4, the department will direct the contractor to either:
 - 1. Remove and replace unacceptable concrete pavement to the nearest joint with new concrete pavement of conforming thickness. The department will pay once for the area at the full contract price.
 - 2. If the unacceptable pavement is less than 100 LF, the department may allow the concrete to remain in place without payment for the unacceptable area.

460.2.6 Recovered Asphaltic Binders

Replace paragraph two with the following effective with the November 2021 letting:

- (2) The contractor may replace virgin binder with recovered binder up to the maximum percentage allowed under 460.2.5 without further testing. When the design percent asphalt binder replaced exceeds the allowable limits in 460.2.5, the contractor must:
 - Document adjustments made to the mix design in the mix design submittal.
 - Submit test results that indicate the mixture's asphaltic binder meets or exceeds the upper and lower temperature grade requirements the bid item designates.
 - If only one recycled asphaltic material source is used, furnish one of the following:
 - Test results from extracted and recovered binder from the resultant mixture.
 - Blending charts that indicate the resultant mixture's high and low temperature PG as an interpolation of the percent binder replaced between the virgin binder's and the recycled asphaltic material source binder's high and low temperature PG.
 - If two or more recycled asphaltic material sources are used, furnish test results from extracted and

recovered binder from the resultant mixture.

501.2.6 Water

Retitle with the following effective with the November 2021 letting:

501.2.6 Mixing Water

501.2.6.2 Requirements

Replace paragraph two with the following effective with the November 2021 letting:

- (2) Water from other sources must comply with the following:

Acidity, maximum of 0.1N NaOH to neutralize 200 mL of water; CMM 870: WTP C-001.....	2 mL
Alkalinity, maximum of 0.1N HCL to neutralize 200 mL of water; CMM 870: WTP C-001.....	15 mL
Maximum sulphate (SO ₄); CMM 870: WTP C-001.....	0.05 percent
Maximum chloride; CMM 870: WTP C-001.....	0.10 percent
Maximum total solids; CMM 870: WTP C-001	
Organic.....	0.04 percent
Inorganic.....	0.15 percent

501.3.2.2.2 Supplementary Cementitious Material

Replace the entire text with the following effective with the May 2022 letting:

- (1) Replace 15 to 30 percent by weight of the total cementitious material content with approved SCMs for class I concrete as specified in 715.
- (2) Replace a maximum of 30 percent by weight of the total cementitious material content with approved SCMs for class II and class III concrete as specified in 716.
- (3) Limit Class F fly ash sources not on the APL to maximum 15 percent.
- (4) Minimum SCM content may be waived by the engineer.

501.3.2.4.2 Air Entrainment

Replace paragraph two with the following effective with the November 2021 letting:

- (2) Test fresh concrete air content according to AASHTO T152 or AASHTO TP118 at the contract-required frequency and as the engineer directs. Test concrete placed by pumping or belting at the point of discharge from the pump line or belt.

501.3.7.1 Slump

Replace paragraph one with the following effective with the November 2021 letting:

- (1) Use a 1-inch to 4-inch slump for concrete used in structures or placed in forms, except as follows:
 - Do not exceed a slump of 2 inches for grade E concrete.
 - Increase slump as specified in 502.3.5.3 for concrete placed underwater.
 - If BTS approves a concrete mixture using a superplasticizer, the contractor may increase slump for that mixture to a maximum of 9 inches without exceeding the maximum mix water allowed for that grade.

531.5 Payment

Replace paragraph two with the following effective with the November 2021 letting:

- (2) Payment for Concrete Masonry Ancillary Structures Type NS is full compensation for providing concrete for non-standard sign structure foundations; and for anchor rod assemblies. The department will pay separately for excavating and backfilling drilled shafts under the Drilling Shafts bid items.

Replace paragraph five with the following effective with the November 2021 letting:

- (5) Payment for the Foundation bid items is full compensation for providing concrete foundations; for anchor rod assemblies; for reinforcing steel; and for embedded conduit and electrical components. The department will pay separately for excavating and backfilling drilled shafts under the Drilling Shafts bid items.

642.2.2.1 General

Replace paragraph one with the following effective with the November 2021 letting:

- (1) Provide each field office with two rooms, separated by an interior door with a padlock. Ensure that each room has a separate exterior door and its own air conditioner. Locate the office where a quality internet connection can be achieved. Ensure quality cell phone reception is achievable inside the field office.

701.3.1 General

Replace table 701-1 with the following effective with the November 2021 letting:

TABLE 701-1 TESTING AND CERTIFICATION STANDARDS

TEST	TEST STANDARD	MINIMUM REQUIRED CERTIFICATION (any one of the certifications listed for each test)
Random Sampling	CMM 830.9.2	Transportation Materials Sampling Technician (TMS) TMS Assistant Certified Technician (ACT-TMS) Aggregate Technician I (AGGTEC-I) AGGTEC-I Assistant Certified Technician (ACT-AGG) PCC Technician I (PCCTEC-I) PCCTEC-I Assistant Certified Technician (ACT-PCC) Grading Technician I (GRADINGTEC-I) Grading Assistant Certified Technician (ACT-GRADING)
Sampling Aggregates	AASHTO T2 ^[1] ^[4]	TMS, ACT-TMS, AGGTEC-1, ACT-AGG
Percent passing the No. 200 sieve	AASHTO T11 ^[1]	AGGTEC-I, ACT-AGG
Fine & coarse aggregate gradation	AASHTO T27 ^[1]	
Aggregate moisture content	AASHTO T255 ^[1]	
Fractured faces	ASTM D5821 ^[1]	
Liquid limit	AASHTO T89	Aggregate Testing for Transportation Systems (ATTS)
Plasticity index	AASHTO T90 ^[3]	GRADINGTEC-I, or ACT-GRADING
Sampling freshly mixed concrete	AASHTO R60	PCCTEC-1 ACT-PCC
Air content of fresh concrete	AASHTO T152 ^[2] AASHTO TP118 ^[5]	
Air void system of fresh concrete	AASHTO TP118 ^[5]	
Concrete slump	AASHTO T119 ^[2]	
Concrete temperature	ASTM C1064	
Making and curing concrete specimens	AASHTO T23	
Moist curing for concrete specimens	AASHTO M201	
Concrete compressive strength	AASHTO T22	Concrete Strength Tester (CST) CST Assistant Certified Technician (ACT-CST)
Concrete flexural strength	AASHTO T97	
Concrete surface resistivity ^[2]	AASHTO T358	
Voids in aggregate	AASHTO T19	PCCTEC-II
Profiling	—	PROFILER

^[1] As modified in CMM 860.

^[2] As modified in CMM 870.

^[3] A plasticity check, if required under individual QMP specifications, may be performed by an AGGTEC-I in addition to the certifications listed for liquid limit and plasticity index tests.

^[4] Plant personnel may operate equipment to obtain samples under the direct observation of a TMS or higher.

^[5] Consolidate by rodding.

710.2 Small Quantities

Replace the entire text with the following effective with the November 2021 letting:

- (1) The department defines small quantities as follows:
 - As specified in 715.1.1.2 for class I concrete.
 - Less than 50 cubic yards of class II ancillary concrete placed under a single bid item.
- (2) For contracts with only small quantities of material subject to testing, modify the requirements of 710 as follows:

1. The contractor may submit an abbreviated quality control plan as allowed in 701.1.2.3.
2. Provide one of the following for aggregate process control:
 - Documented previous testing dated within 120 calendar days. Provide gradation test results to the engineer before placing material.
 - Non-random start-up gradation testing.

710.4 Concrete Mixes

Replace paragraph two with the following effective with the November 2021 letting:

- (2) At least 7 business days before producing concrete, document that materials conform to 501 unless the engineer allows or individual QMP specifications provide otherwise. Include the following:
 1. For mixes: quantities per cubic yard expressed as SSD weights and net water, water to cementitious material ratio, air content, and SAM number.
 2. For cementitious materials and admixtures: type, brand, and source.
 3. For aggregates: absorption, SSD bulk specific gravity, wear, soundness, freeze thaw test results if required, and air correction factor. Also include aggregate production records dated within 2 years if using those results in the design. Submit component aggregate gradations, aggregate proportions, and target combined blended aggregate gradations using the following:
 - DT2220 for combined aggregate gradations.
 - DT2221 for optimized aggregate gradations.
 4. For optimized concrete mixtures:
 - Complete the worksheets within DT2221 according to the directions.
 - Ensure the optimized aggregate gradations and the optimized mix design conform to WisDOT specifications and pass the built-in tests within DT2221.
 - Verify slip-form mixture workability according to AASHTO TP137 and conformance to specifications through required trial batching.
 - Submit the completed DT2221 to the engineer electronically. Include the trial batch test results with the mix design submittal.

Replace paragraph four with the following effective with the November 2021 letting:

- (4) Prepare and submit modifications to a concrete mix to the engineer for approval 3 business days before using that modified mix. Modifications requiring the engineer's approval include changes in:
 1. Source of any material. For paving and barrier mixes, a source change for fly ash of the same class does not constitute a mix design change.
 2. Quantities of cementitious materials.
 3. Addition or deletion of admixtures. Minor admixture dosage adjustments required to maintain air content or slump do not require engineer review or approval.

710.5.5 Strength

Replace paragraph one with the following effective with the November 2021 letting:

- (1) Cast all 6" x 12" cylinders or all 6" x 6" x 21" beams in a set from the same sample. Do not cast more than one set of specimens from a single truckload of concrete. Mark each specimen to identify the lot and subplot or location on the project it represents.

710.5.6 Aggregate Testing

Retitle and replace the entire text with the following effective with the November 2021 letting:

710.5.6 Aggregate Testing During Concrete Production

710.5.6.1 General

- (1) The department will accept gradation based on the results of department-performed acceptance testing.
- (2) The department and contractor will obtain samples using the same method. When belt sampling, contractor personnel shall obtain samples for the department under the direct observation of the department personnel. Contractor will define sampling method in the QMP or abbreviated QMP.

710.5.6.2 Contractor Control Charts

710.5.6.2.1 General

- (1) Test aggregate gradations during concrete production except as allowed for small quantities under 710.2. Required contractor testing will be performed using non-random samples.

- (2) Sample aggregates from either the conveyor belt or from the working face of the stockpiles.
- (3) Sample aggregates within 2 business days before placement for each mix design. Include this gradation on the control charts.
- (4) Report gradation test results and provide control charts to the engineer within 1 business day of obtaining the sample. Submit results to the engineer and electronically into MRS as specified in 701.1.2.7.
- (5) Conduct aggregate testing at the minimum frequency shown based on the anticipated daily cumulative plant production for each mix design. The contractor’s concrete production tests can be used for the same mix design on multiple contracts.

TABLE 710-1 CONTRACTOR GRADATION TESTING FREQUENCY - CLASS I

DAILY PLANT PRODUCTION RATE FOR WisDOT WORK	MINIMUM FREQUENCY
Gradation Report Before Placement	
1000 cubic yards or less	one test per day
more than 1000 cubic yards	two tests per day

TABLE 710-2 CONTRACTOR GRADATION TESTING FREQUENCY - CLASS II

MINIMUM FREQUENCY
Gradation Report Before Placement
One test per calendar week of production

710.5.6.2.2 Optimized Aggregate Gradation Control Charts

- (1) Determine the complete gradation using a washed analysis for both fine and coarse aggregates. Report results for the following:
 - 1 1/2", 1", 3/4", 1/2", 3/8", #4, #8, #16, #30, #50, #100, and #200 sieves.
 - Sum of volumetric percentages retained on No. 8, No. 16, and No. 30 sieves.
 - Sum of volumetric percentages retained on No. 30, No. 50, No. 100, and No. 200 sieves.
- (2) Calculate blended aggregate gradations using the mix design batch percentages for the component aggregates. Ensure the blended aggregate gradation conforms to the volumetric percent retained of the optimized aggregate gradation limits specified in table 501-4.
- (3) Throughout the contract, construct a 4-point running average of the volumetric percent retained for each sieve to determine if the blended aggregate gradation is within the tarantula curve limits specified in table 501-4.

710.5.6.2.3 Combined Aggregate Gradation Control Charts

- (1) Determine the complete gradation using a washed analysis for both fine and coarse aggregates. Report results for the 1 1/2", 1", 3/4", 1/2", 3/8", #4, #8, #16, #30, #50, #100, and #200 sieves.
- (2) Calculate blended aggregate gradations using the mix design batch percentages for the component aggregates. Ensure the blended aggregate gradation conforms to the percent passing by weight requirements of the combined aggregate gradation limits specified in table 501-4.
- (3) Throughout the contract, construct a 4-point running average of the percent passing by weight for each sieve to determine if the blended aggregate gradation is within the combined aggregate gradation limits specified in table 501-4.

710.5.6.3 Department Acceptance Testing

- (1) Department testing frequency is based on the quantity of each mix design placed under each individual WisDOT contract.
- (2) The department will split each sample, test for acceptance, and retain the remainder for a minimum of 10 calendar days.
- (3) The department will obtain the sample and deliver to regional testing lab in the same day. Department will report gradation test results to the contractor within 1 business day of being delivered to the lab. Department and contractor can agree to an alternative test result reporting timeframe; alternative timeframe is required to be documented in the QMP.
- (4) Additional samples may be taken at the engineer’s discretion due to change in condition.

TABLE 710-3 DEPARTMENT GRADATION TESTING FREQUENCY

CONCRETE CLASSIFICATION	MINIMUM DEPARTMENT FREQUENCY
Class I: Pavement	1 test per placement day for first 5 days of placement. If all samples are passing, reduced frequency is applied.
	Reduced frequency: 1 test per calendar week of placement
Class I: Structures	1 test per 250 CY placed <ul style="list-style-type: none"> - Minimum of 1 test per substructure - Minimum of 1 test per superstructure
Class I: Cast-in-Place Barrier	1 test per 500 CY placed
Class II	No minimum testing

710.5.7 Corrective Action

Replace the entire text with the following effective with the November 2021 letting:

710.5.7.1 Optimized Aggregate Gradations

- (1) If the contractor's 4-point running average or a department test result of the volumetric percent retained exceeds the tarantula curve limits by less than or equal to 1.0 percent on a single sieve size, do the following:
 1. Notify the other party immediately.
 2. Perform corrective action documented in the QC plan or as the engineer approves.
 3. Document and provide corrective action results to the engineer as soon as they are available.
 4. Department will conduct two tests within the next business day after corrective action is complete.
 5. If blended aggregate gradations are within the tarantula curve limits by the second department test:
 - Continue with concrete production.
 - Contractor will include a break in the 4-point running average.
 - For Class I: Pavements, department will discontinue reduced frequency testing and will test at a frequency of 1 test per placement day. Once 5 consecutive samples are passing at the 1 test per placement day frequency, the reduced frequency testing will be reapplied.
 6. If blended aggregate gradations are not within the tarantula curve limits by the second department test:
 - Provide a new mix design with an increased cementitious content.
 - If the mix design already has a cementitious content of 565 or more pounds per cubic yard, provide a new mix design.
 - If the contract requires optimized aggregate gradations under 501.2.7.4.2.1(2), stop concrete production and submit a new mix design.
- (2) If the contractor's 4-point running average or a department test result of the volumetric percent retained exceeds the tarantula curve limits by more than 1.0 percent on one or more sieves, stop concrete production and submit a new mix design.
- (3) Department and contractor will sample and test aggregate of the new mix design at the frequency defined in 710.5.6.1.

710.5.7.2 Combined Aggregate Gradations

- (1) If the contractor's 4-point running average or a department test result of the percent passing by weight exceeds the combined aggregate gradation limits by less than or equal to 1.0 percent on a single sieve size, do the following:
 1. Notify the other party immediately.
 2. Perform corrective action documented in the QC plan or as the engineer approves.
 3. Document and provide corrective action results to the engineer as soon as they are available.
 4. Department will conduct two tests within the next business day after corrective action is complete.
 5. If blended aggregate gradations are within the combined aggregate gradation limits by the second department test:
 - Continue with concrete production.
 - Contractor will include a break in the 4-point running average.

- For Class I: Pavements, department will discontinue reduced frequency testing and will test at a frequency of 1 test per placement day. Once 5 consecutive samples are passing at the 1 test per placement day frequency, the reduced frequency testing will be reapplied.
- 6. If blended aggregate gradations are not within the combined aggregate gradation limits by the second department test, stop concrete production and submit a new mix design.
- (2) If the contractor's 4-point running average or a department test result of the percent passing by weight exceeds the combined aggregate gradation limits by more than 1.0 percent on one or more sieves, stop concrete production and submit a new mix design.
- (3) Department and contractor will sample and test aggregate of the new mix design at the frequency defined in 710.5.6.1.

715.3.1.1 General

Replace paragraphs three and four with the following effective with the November 2021 letting:

- (3) Cast a set of 3 additional 6"x12" cylinders and test the concrete surface resistivity according to AASHTO T358. Perform this testing at least once per lot if total contract quantities are greater than or equal to the following:

- 20,000 square yards for pavements.
- 5,000 linear feet for barriers.
- 500 cubic yards for structure concrete.

Submit the resistivity to the nearest tenth into MRS for information only. Resistivity testing is not required for the following:

- Lot with less than 3 sublots.
- Concrete items classified as ancillary.
- Concrete placed under the following bid items:
 - Concrete Pavement Approach Slab
 - Concrete Masonry Culverts
 - Concrete Masonry Retaining Walls

- (4) Test the air void system at least once per lot and enter the SAM number in MRS for information only. SAM testing is not required for the following:

- For lots with less than 3 sublots.
- High early strength (HES) concrete.
- Special high early strength (SHES) concrete.
- Concrete placed under the following bid items:
 - Concrete Pavement Approach Slab
 - Concrete Masonry Culverts
 - Concrete Masonry Retaining Walls
 - Steel Grid Floor Concrete Filled
 - Crash Cushions Permanent
 - Crash Cushions Permanent Low Maintenance
 - Crash Cushions Temporary

715.3.1.2.3 Lots by Cubic Yard

Replace the entire text with the following effective with the November 2021 letting:

- (1) Define standard lots and sublots conforming to the following:

TABLE 715-1 CLASS I - LOT AND SUBLOT SIZES

CONCRETE CLASSIFICATION	LOT SIZE	SUBLOT SIZE	NUMBER OF SUBLOTS PER LOT
Class I: Pavement	1250 cubic yards	250 cubic yards	5
Class I: Structures	250 cubic yards	50 cubic yards	5
Class I: Cast-in-Place Barrier	500 cubic yards	100 cubic yards	5

- (2) The contractor may include sublots less than or equal to 25 percent of the standard volume in the previous subplot. For partial sublots exceeding 25 percent of the standard volume, notify the engineer who will direct additional testing to represent that partial subplot.
- (3) An undersized lot is eligible for incentive payment under 715.5 if the lot has 3 or more sublots for that lot.

715.3.2 Strength Evaluation

Replace the entire text with the following effective with the November 2021 letting:

715.3.2.1 General

- (1) The department will make pay adjustments for strength on a lot-by-lot basis using the compressive strength of contractor QC cylinders or the flexural strength of contractor QC beams.
- (2) Randomly select 2 QC specimens to test at 28 days for percent within limits (PWL). Compare the strengths of the 2 randomly selected QC specimens and determine the 28-day subplot average strength as follows:
 - If the lower strength divided by the higher strength is 0.9 or more, average the 2 QC specimens.
 - If the lower strength divided by the higher strength is less than 0.9, break one additional specimen and average the 2 higher strength specimens.

715.3.2.2 Removal and Replacement

715.3.2.2.1 Pavement

- (1) If a subplot strength is less than 2500 psi in compressive strength or 500 psi in flexural strength, the department may direct the contractor to core that subplot to determine its structural adequacy and whether to direct removal.
- (2) If the engineer directs coring, obtain three cores from the subplot in question. Have an HTCP-certified PCC technician I perform or observe core sampling according to AASHTO T24.
- (3) Have an independent consultant test cores according to AASHTO T24.
- (4) The department will assess concrete for removal and replacement based on a subplot-by-subplot analysis of core strength. Perform coring and testing, fill core holes with an engineer-approved non-shrink grout or concrete, and provide traffic control during coring.
- (5) The subplot pavement is conforming if the compressive strengths of all cores from the subplot are 2500 psi or greater.
- (6) The subplot pavement is nonconforming if the compressive strengths of any core from the subplot is less than 2500 psi. The department may direct removal and replacement or otherwise determine the final disposition of nonconforming material as specified in 106.5.

715.3.2.2.2 Structures and Cast-in-Place Barrier

- (1) The department will evaluate the subplot for possible removal and replacement if the 28-day subplot average compressive strength is lower than f'_c minus 500 psi. The value of f'_c is the design stress the plans show. The department may assess further strength price reductions or require removal and replacement only after coring the subplot.
- (2) The engineer may initially evaluate the subplot strength using a non-destructive method. Based on the results of non-destructive testing, the department may accept the subplot at the previously determined pay for the lot, or direct the contractor to core the subplot.
- (3) If the engineer directs coring, obtain three cores from the subplot in question. Have an HTCP-certified PCC technician I perform or observe core sampling according to AASHTO T24. Determine core locations, subject to the engineer's approval, that do not interfere with structural steel.
- (4) Have an independent consultant test cores according to AASHTO T24.
- (5) The department will assess concrete for removal and replacement based on a subplot-by-subplot analysis of core strength. Perform coring and testing, fill core holes with an engineer-approved non-shrink grout or concrete, and provide traffic control during coring.
- (6) If the 3-core average is greater than or equal to 85 percent of f'_c , and no individual core is less than 75 percent of f'_c , the engineer will accept the subplot at the previously determined pay for the lot. If the 3-core average is less than 85 percent of f'_c , or an individual core is less than 75 percent of f'_c , the engineer may require the contractor to remove and replace the subplot. The department may direct removal and replacement or otherwise determine the final disposition of nonconforming material as specified in 106.5.

715.3.3 Aggregate

Replace the entire text with the following effective with the November 2021 letting:

715.3.3.1 General

- (1) Except as allowed for small quantities in 710.2, test aggregate conforming to 710.5.6.

715.3.3.2 Structures

- (1) In addition to the aggregate testing required under 710.5.6, determine the fine and coarse aggregate moisture content for each sample.
- (2) Calculate target batch weights for each mix when production of that mix begins. Whenever the moisture content of the fine or coarse aggregate changes by more than 0.5 percent, adjust the batch weights to maintain the design w/cm ratio.

716.2.1 Class II Concrete

Replace paragraph two with the following effective with the May 2022 letting:

- (2) Perform random QC testing at the following frequencies:
 1. Test air content, temperature, and slump a minimum of once per 100 cubic yards for each mix design and placement method.
 2. Cast one set of 2 cylinders per 200 cubic yards for each mix design and placement method. Cast a minimum of one set of 2 cylinders per contract for each mix design and placement method. Random 28-day compressive strength cylinders are not required for HES or SHES concrete.
 3. For deck overlays, perform tests and cast cylinders once per 50 cubic yards of grade E concrete placed.
 4. For concrete base, one set of tests and one set of cylinders per 250 cubic yards.

The department will allow concrete startup test results for quantities under 50 cubic yards. Cast one set of 2 cylinders if using startup testing for acceptance.

ERRATA

460.2.2.3 Aggregate Gradation Master RangeCorrect errata by adding US Standard equivalent sieve sizes.

- (1) Ensure that the aggregate blend, including recycled material and mineral filler, conforms to the gradation requirements in table 460-1. The values listed are design limits; production values may exceed those limits.

TABLE 460-1 AGGREGATE GRADATION MASTER RANGE AND VMA REQUIREMENTS

SIEVE	PERCENT PASSING DESIGNATED SIEVES							
	NOMINAL SIZE							
	No. 1 (37.5 mm) (1 1/2 inch)	No. 2 (25.0 mm) (1 inch)	No.3 (19.0 mm) (3/4 inch)	No. 4 (12.5 mm) (1/2 inch)	No. 5 (9.5 mm) (3/8 inch)	No. 6 (4.75 mm) (3/16 inch)	SMA No. 4 (12.5 mm) (1/2 inch)	SMA No. 5 (9.5 mm) (3/8 inch)
50.0-mm (2-inch)	100							
37.5-mm (1 1/2-inch)	90 - 100	100						
25.0-mm (1-inch)	90 max	90 - 100	100					
19.0-mm (3/4-inch)	—	90 max	90 - 100	100			100	
12.5-mm (1/2-inch)	—	—	90 max	90 - 100	100		90 - 97	100
9.5-mm (3/8-inch)	—	—	—	90 max	90 - 100	100	58 - 80	90 - 100
4.75-mm (No. 4)	—	—	—	—	90 max	90 - 100	25 - 35	35 - 45
2.36-mm (No. 8)	15 - 41	19 - 45	23 - 49	28 - 58	32 - 67	90 max	15 - 25	18 - 28
1.18-mm (No. 16)	—	—	—	—	—	30 - 55	—	—
0.60-mm (No. 30)	—	—	—	—	—	—	18 max	18 max
0.075-mm (No. 200)	0 - 6.0	1.0 - 7.0	2.0 - 8.0	2.0 - 10.0	2.0 - 10.0	6.0 - 13.0	8.0 - 11.0	8.0 - 12.0
% VMA	11.0 min	12.0 min	13.0 min	14.0 min ^[1]	15.0 min ^[2]	16.0 - 17.5	16.0 min	17.0 min

^[1] 14.5 for LT and MT mixes.

^[2] 15.5 for LT and MT mixes.

715.5.1 GeneralCorrect the bid item number for Incentive Compressive Strength Concrete Pavement.

- (1) The department will pay incentive for compressive strength under the following bid items:

<u>ITEM NUMBER</u>	<u>DESCRIPTION</u>	<u>UNIT</u>
715.0502	Incentive Strength Concrete Structures	DOL
715.0603	Incentive Strength Concrete Barrier	DOL
715.0715	Incentive Flexural Strength Concrete Pavement	DOL
715.0720	Incentive Compressive Strength Concrete Pavement	DOL

ADDITIONAL SPECIAL PROVISION 7

- A. Reporting 1st Tier and DBE Payments During Construction
1. Comply with reporting requirements specified in the department's Civil Rights Compliance, Contractor's User Manual, Sublets and Payments.
 2. Report payments to all DBE firms within 10 calendar days of receipt of a progress payment by the department or a contractor for work performed, materials furnished, or materials stockpiled by a DBE firm. Report the payment as specified in A(1) for all work satisfactorily performed and for all materials furnished or stockpiled.
 3. Report payments to all first tier subcontractor relationships within 10 calendar days of receipt of a progress payment by the department for work performed. Report the payment as specified in A(1) for all work satisfactorily performed.
 4. All tiers shall report payments as necessary to comply with the DBE payment requirement as specified in A(2).
 5. DBE firms must enter all payments to DBE and non-DBE firms regardless of tier.
 6. Require all first tier relationships, DBE firms and all other tier relationships necessary to comply with the DBE payment requirement in receipt of a progress payment by contractor to acknowledge receipt of payment as specified in A(1), (2), (3) and (4).
 7. All agreements made by a contractor shall include the provisions in A(1), (2), (3), (4), (5), and (6), and shall be binding on all first tier subcontractor relationships, all contractors and subcontractors utilizing DBE firms on the project, and all payments from DBE firms.
- B. Costs for conforming to this special provision are incidental to the contract.

NOTE: CRCS Prime Contractor payment is currently not automated and will need to be manually loaded into the Civil Rights Compliance System. Copies of prime contractor payments received (check or ACH) will have to be forwarded to paul.ndon@dot.wi.gov within 5 days of payment receipt to be logged manually.

***Additionally, for information on Subcontractor Sublet assignments, Subcontractor Payments and Payment Tracking, please refer to the CRCS Payment and Sublets manual at:

<https://wisconsindot.gov/Documents/doing-bus/civil-rights/labornwage/crcs-payments-sublets-manual.pdf>

ADDITIONAL SPECIAL PROVISION 9

Electronic Certified Payroll or Labor Data Submittal

(1) Use the department's Civil Rights Compliance System (CRCS) to electronically submit certified payroll reports for contracts with federal funds and labor data for contracts with state funds only. Details are available online through the department's highway construction contractor information (HCCI) site on the Labor, Wages, and EEO Information page at:

<https://wisconsindot.gov/Pages/doing-bus/civil-rights/labornwage/default.aspx>

(2) Ensure that all tiers of subcontractors, including all trucking firms, either submit their weekly certified payroll reports (contracts with federal funds) or labor data (contracts with state funds only) electronically through CRCS. These payrolls or labor data are due within seven calendar days following the close of the payroll period. Every firm providing physical labor towards completing the project is a subcontractor under this special provision.

(3) Upon receipt of contract execution, promptly make all affected firms aware of the requirements under this special provision and arrange for them to receive CRCS training as they are about to begin their submittals. The department will provide training either in a classroom setting at one of our regional offices or by telephone. Contact Paul Ndon at (414) 438-4584 to schedule the training.

(4) The department will reject all paper submittals for information required under this special provision. All costs for conforming to this special provision are incidental to the contract.

(5) Firms wishing to export payroll/labor data from their computer system into CRCS should have their payroll coordinator contact Paul Ndon at paul.ndon@dot.wi.gov. Not every contractor's payroll system is capable of producing export files. For details, see Section 4.8 CPR Auto Submit (Data Mapping) on pages 49-50; 66-71 of the CRCS Payroll Manual at:

<https://wisconsindot.gov/Documents/doing-bus/civil-rights/labornwage/crcs-payroll-manual.pdf>

Non-discrimination Provisions

During the performance of this contract, the contractor, for itself, its assignees, and successors in interest (hereinafter referred to as the "contractor") agrees as follows:

1. Compliance with Regulations: The contractor (hereinafter includes consultants) will comply with the Acts and the Regulations relative to Non-discrimination in Federally-assisted programs of the U.S. Department of Transportation, Federal Highway Administration, as they may be amended from time to time, which are herein incorporated by reference and made a part of this contract.

2. Non-discrimination: The contractor, with regard to the work performed by it during the contract, will not discriminate on the grounds of race, color, or national origin in the selection and retention of subcontractors, including procurements of materials and leases of equipment. The contractor will not participate directly or indirectly in the discrimination prohibited by the Acts and the Regulations, including employment practices when the contract covers any activity, project, or program set forth in Appendix B of 49 CFR Part 21.

3. Solicitations for Subcontracts, Including Procurements of Materials and Equipment: In all solicitations, either by competitive bidding, or negotiation made by the contractor for work to be performed under a subcontract, including procurements of materials, or leases of equipment, each potential subcontractor or supplier will be notified by the contractor of the contractor's obligations under this contract and the Acts and the Regulations relative to Non-discrimination on the grounds of race, color, or national origin.

4. Information and Reports: The contractor will provide all information and reports required by the Acts, the Regulations, and directives issued pursuant thereto and will permit access to its books, records, accounts, other sources of information, and its facilities as may be determined by the Recipient or the Federal Highway Administration to be pertinent to ascertain compliance with such Acts, Regulations, and instructions. Where any information required of a contractor is in the exclusive possession of another who fails or refuses to furnish the information, the contractor will so certify to the Recipient or the Federal Highway Administration, as appropriate, and will set forth what efforts it has made to obtain the information.

5. Sanctions for Noncompliance: In the event of a contractor's noncompliance with the Non-discrimination provisions of this contract, the Recipient will impose such contract sanctions as it or the Federal Highway Administration may determine to be appropriate, including, but not limited to:

- a. Withholding payments to the contractor under the contract until the contractor complies; and/or
- b. Cancelling, terminating, or suspending a contract, in whole or in part.

6. Incorporation of Provisions: The contractor will include the provisions of paragraphs one through six in every subcontract, including procurements of materials and leases of equipment, unless exempt by the Acts, the Regulations and directives issued pursuant thereto. The contractor will take action with respect to any subcontract or procurement as the Recipient or the Federal Highway Administration may direct as a means of enforcing such provisions including sanctions for noncompliance. Provided, that if the contractor becomes involved in, or is threatened with litigation by a subcontractor, or supplier because of such direction, the contractor may request the Recipient to enter into any litigation to protect the interests of the Recipient. In addition, the contractor may request the United States to enter into the litigation to protect the interests of the United States.

During the performance of this contract, the contractor, for itself, its assignees, and successors in interest (hereinafter referred to as the "contractor") agrees to comply with the following non-discrimination statutes and authorities; including but not limited to:

Pertinent Non-Discrimination Authorities:

- Title VI of the Civil Rights Act of 1964 (42 U.S.C. § 2000d et seq., 78 stat. 252), (prohibits discrimination on the basis of race, color, national origin); and 49 CFR Part 21.
- The Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, (42 U.S.C. § 4601), (prohibits unfair treatment of persons displaced or whose property has been acquired because of Federal or Federal-aid programs and projects);
- Federal-Aid Highway Act of 1973, (23 U.S.C. § 324 et seq.), (prohibits discrimination on the basis of sex);
- Section 504 of the Rehabilitation Act of 1973, (29 U.S.C. § 794 et seq.), as amended, (prohibits discrimination on the basis of disability); and 49 CFR Part 27;
- The Age Discrimination Act of 1975, as amended, (42 U.S.C. § 6101 et seq.), (prohibits discrimination on the basis of age);
- Airport and Airway Improvement Act of 1982, (49 USC § 471, Section 47123), as amended, (prohibits discrimination based on race, creed, color, national origin, or sex);
- The Civil Rights Restoration Act of 1987, (PL 100-209), (Broadened the scope, coverage and applicability of Title VI of the Civil Rights Act of 1964, The Age Discrimination Act of 1975 and Section 504 of the Rehabilitation Act of 1973, by expanding the definition of the terms "programs or activities" to include all of the programs or activities of the Federal-aid recipients, sub-recipients and contractors, whether such programs or activities are Federally funded or not);
- Titles II and III of the Americans with Disabilities Act, which prohibit discrimination on the basis of disability in the operation of public entities, public and private transportation systems, places of public accommodation, and certain testing entities (42 U.S.C. §§ 12131-12189) as implemented by Department of Transportation regulations at 49 C.F.R. parts 37 and 38;
- The Federal Aviation Administration's Non-discrimination statute (49 U.S.C. § 47123) (prohibits discrimination on the basis of race, color, national origin, and sex);

- Executive Order 12898, Federal Actions to Address Environmental Justice in Minority Populations and Low-Income Populations, which ensures Non-discrimination against minority populations by discouraging programs, policies, and activities with disproportionately high and adverse human health or environmental effects on minority and low-income populations;
- Executive Order 13166, Improving Access to Services for Persons with Limited English Proficiency, and resulting agency guidance, national origin discrimination includes discrimination because of Limited English proficiency (LEP). To ensure compliance with Title VI, you must take reasonable steps to ensure that LEP persons have meaningful access to your programs (70 Fed. Reg. at 74087 to 74100);
- Title IX of the Education Amendments of 1972, as amended, which prohibits you from discriminating because of sex in education programs or activities (20 U.S.C. 1681 et seq).

Effective November 2020 letting

BUY AMERICA PROVISION

All steel and iron materials permanently incorporated in this project shall be domestic products and all manufacturing and coating processes for these materials from smelting forward in the manufacturing process must have occurred within the United States. Coating includes epoxy coating, galvanizing, painting and any other coating that protects or enhances the value of a material subject to the requirements of Buy America. The exemption of this requirement is the minimal use of foreign materials if the total cost of such material permanently incorporated in the product does not exceed one-tenth of one percent (1/10 of 1%) of the total contract cost or \$2,500.00, whichever is greater. For purposes of this paragraph, the cost is that shown to be the value of the subject products as they are delivered to the project. The contractor shall take actions and provide documentation conforming to CMM 2-28.5 to ensure compliance with this "Buy America" provision.

<https://wisconsindot.gov/rdwy/cmm/cm-02-28.pdf>

Upon completion of the project certify to the engineer, in writing using department form DT4567, that all steel, iron, and coating processes for steel or iron incorporated into the contract work conform to these "Buy America" provisions. Attach a list of exemptions and their associated costs to the certification form. Department form DT4567 is available at:

<https://wisconsindot.gov/Documents/formdocs/dt4567.docx>



Proposal Schedule of Items

Proposal ID: 20220809009 Project(s): 2025-15-71

Federal ID(s): N/A

SECTION: 0001

Contract Items

Alt Set ID:

Alt Mbr ID:

Proposal Line Number	Item ID Description	Approximate Quantity and Units	Unit Price	Bid Amount
0002	203.0220 Removing Structure (structure) 01.B-67-86	1.000 EACH	_____.	_____.
0004	213.0100 Finishing Roadway (project) 01. 2025-15-71	1.000 EACH	_____.	_____.
0006	502.0100 Concrete Masonry Bridges	23.000 CY	_____.	_____.
0008	502.3101 Expansion Device	6.000 LF	_____.	_____.
0010	502.3200 Protective Surface Treatment	42.000 SY	_____.	_____.
0012	502.3210 Pigmented Surface Sealer	33.000 SY	_____.	_____.
0014	502.4106 Adhesive Anchors 3/4-inch	1.000 EACH	_____.	_____.
0016	505.0600 Bar Steel Reinforcement HS Coated Structures	4,960.000 LB	_____.	_____.
0018	506.0105 Structural Steel Carbon	11,630.000 LB	_____.	_____.
0020	506.3025 Welded Stud Shear Connectors 7/8x8-Inch	264.000 EACH	_____.	_____.
0022	517.0600 Painting Epoxy System (structure) 01.B-67-86	LS	LUMP SUM	_____.
0024	517.0901.S Preparation and Coating of Top Flanges (structure) 01.B-67-86	1.000 EACH	_____.	_____.
0026	517.3001.S Structure Overcoating Cleaning and Priming (structure) 01.B-67-86	1.000 EACH	_____.	_____.
0028	517.4001.S Containment and Collection of Waste Materials (structure) 01.B-67-86	1.000 EACH	_____.	_____.



Proposal Schedule of Items

Proposal ID: 20220809009 Project(s): 2025-15-71

Federal ID(s): N/A

SECTION: 0001

Contract Items

Alt Set ID:

Alt Mbr ID:

Proposal Line Number	Item ID Description	Approximate Quantity and Units	Unit Price	Bid Amount
0030	603.8000 Concrete Barrier Temporary Precast Delivered	383.000 LF	_____.	_____.
0032	603.8125 Concrete Barrier Temporary Precast Installed	383.000 LF	_____.	_____.
0034	618.0100 Maintenance And Repair of Haul Roads (project) 01. 2025-15-71	1.000 EACH	_____.	_____.
0036	619.1000 Mobilization	1.000 EACH	_____.	_____.
0038	628.7020 Inlet Protection Type D	2.000 EACH	_____.	_____.
0040	634.0618 Posts Wood 4x6-Inch X 18-FT	4.000 EACH	_____.	_____.
0042	637.2210 Signs Type II Reflective H	20.000 SF	_____.	_____.
0044	637.2230 Signs Type II Reflective F	20.000 SF	_____.	_____.
0046	638.2102 Moving Signs Type II	4.000 EACH	_____.	_____.
0048	638.2602 Removing Signs Type II	1.000 EACH	_____.	_____.
0050	638.3000 Removing Small Sign Supports	4.000 EACH	_____.	_____.
0052	642.5201 Field Office Type C	1.000 EACH	_____.	_____.
0054	643.0300 Traffic Control Drums	6,189.000 DAY	_____.	_____.
0056	643.0420 Traffic Control Barricades Type III	1,632.000 DAY	_____.	_____.
0058	643.0500 Traffic Control Flexible Tubular Marker Posts	15.000 EACH	_____.	_____.



Proposal Schedule of Items

Proposal ID: 20220809009 Project(s): 2025-15-71

Federal ID(s): N/A

SECTION: 0001

Contract Items

Alt Set ID:

Alt Mbr ID:

Proposal Line Number	Item ID Description	Approximate Quantity and Units	Unit Price	Bid Amount
0060	643.0600 Traffic Control Flexible Tubular Marker Bases	15.000 EACH	_____.	_____.
0062	643.0705 Traffic Control Warning Lights Type A	3,264.000 DAY	_____.	_____.
0064	643.0715 Traffic Control Warning Lights Type C	1,568.000 DAY	_____.	_____.
0066	643.0800 Traffic Control Arrow Boards	47.000 DAY	_____.	_____.
0068	643.0900 Traffic Control Signs	9,985.000 DAY	_____.	_____.
0070	643.0920 Traffic Control Covering Signs Type II	15.000 EACH	_____.	_____.
0072	643.1000 Traffic Control Signs Fixed Message	90.000 SF	_____.	_____.
0074	643.1050 Traffic Control Signs PCMS	175.000 DAY	_____.	_____.
0076	643.5000 Traffic Control	1.000 EACH	_____.	_____.
0078	646.1020 Marking Line Epoxy 4-Inch	796.000 LF	_____.	_____.
0080	646.5020 Marking Arrow Epoxy	1.000 EACH	_____.	_____.
0082	646.5120 Marking Word Epoxy	1.000 EACH	_____.	_____.
0084	646.9300 Marking Removal Special Marking	2.000 EACH	_____.	_____.
0086	649.0150 Temporary Marking Line Removable Tape 4-Inch	3,064.000 LF	_____.	_____.
0088	649.0250 Temporary Marking Line Removable Tape 8-Inch	53.000 LF	_____.	_____.
0090	649.0960 Temporary Marking Removable Mask Out Tape 6-Inch	1,816.000 LF	_____.	_____.



Proposal Schedule of Items

Proposal ID: 20220809009 Project(s): 2025-15-71

Federal ID(s): N/A

SECTION: 0001

Contract Items

Alt Set ID:

Alt Mbr ID:

Proposal Line Number	Item ID Description	Approximate Quantity and Units	Unit Price	Bid Amount
0092	649.0970 Temporary Marking Removable Mask Out Tape 10-Inch	245.000 LF	_____.	_____.
0094	715.0502 Incentive Strength Concrete Structures	500.000 DOL	1.00000	500.00
0096	SPV.0060 Special 01. Heat Straightening of Damaged Girders	1.000 EACH	_____.	_____.
0098	SPV.0060 Special 02. Counterweight Structure	1.000 EACH	_____.	_____.
Section: 0001			Total:	_____.
			Total Bid:	_____.

PLEASE ATTACH ADDENDA HERE



Wisconsin Department of Transportation

July 18, 2022

Division of Transportation Systems Development

Bureau of Project Development
4822 Madison Yards Way, 4th Floor South
Madison, WI 53705

Telephone: (608) 266-1631
Facsimile (FAX): (608) 266-8459

NOTICE TO ALL CONTRACTORS:

**Proposal #09: 2025-15-71
Pewaukee to Milwaukee
Pilgrim Rd/Capitol Dr B-67-0086
CTH YY
Waukesha County**

Letting of August 9, 2022

This is Addendum No. 01, which provides for the following:

Schedule of Items:

Revised Bid Item Quantities					
Bid Item	Item Description	Unit	Old Quantity	Revised Quantity	Proposal Total
643.0300	Traffic Control Drums	DAYS	6,189	322	6,511
643.0420	Traffic Control Barricades Type III	DAYS	1,632	9	1,641
643.0705	Traffic Control Warning Lights Type A	DAYS	3,264	18	3,282
643.0900	Traffic Control Signs	DAYS	9,985	121	10,106
643.1050	Traffic Control Signs PCMS	DAYS	175	10	185

Plan Sheets:

Revised Plan Sheets	
Plan Sheet	Plan Sheet Title (brief description of changes to sheet)
5	Traffic Control – Legend removed. Sign code updated to designate orange sign sheeting.
6	Traffic Control – Legend removed.
7	Traffic Control – Legend removed. Sign code updated to designate orange sign sheeting.
8	Traffic Control – Legend removed. Advance warning signs updated.
9	Traffic Control – Legend removed. Drum spacing updated. Lane drop sign added.
10	Traffic Control – Legend removed. Drum spacing updated.
11	Traffic Control – Legend removed.
12	Traffic Control – Legend removed. PCMS added. Advance warning signs updated.
13	Traffic Control – Legend removed. Drum spacing updated. Lane drop sign added.
14	Traffic Control – Legend removed. Drum spacing updated. Signs added.
15	Traffic Control – Legend removed. Sidewalk signing updated.
16	Detour Plan – PCMS abbreviation updated to match bid item.

17	Detour Plan – PCMS abbreviation updated to match bid item.
18	Detour Plan – PCMS abbreviation updated to match bid item. Time frame for pre-warn updated.
21	Miscellaneous Quantities – updated Traffic Control tables to match revisions.

Added Plan Sheets	
Plan Sheet	Plan Sheet Title (brief description of why sheet was added)
4A	Traffic Control – added a Traffic Control General Notes sheet, removed legends from each Stage Construction sheet.
45A	SDD – Traffic Control for Lane Closure with Flagging Operation
47A	SDD – Traffic Control, Two Lane Closure on Freeway or Expressway, Short Term (Less than 24 hours)
47B	Traffic Control, Intersection Within Single Right Lane Closure
47C	Traffic Control, Intersection Within Single Left Lane Closure
48A	Traffic Control Two Lane Full Freeway Closure
62A	Standard Sign M4-9B L&R
69A	Standard Sign R9-9A
72A	Standard Sign W1-2

The responsibility for notifying potential subcontractors and suppliers of these changes remains with the prime contractor.

Sincerely,

Mike Coleman

Proposal Development Specialist
Proposal Management Section

ADDENDUM NO. 01

2025-15-71

July 18, 2022

Schedule of Items

Attached, dated July 18, 2022, are the revised Schedule of Items Pages 1 – 4.

Plan Sheets

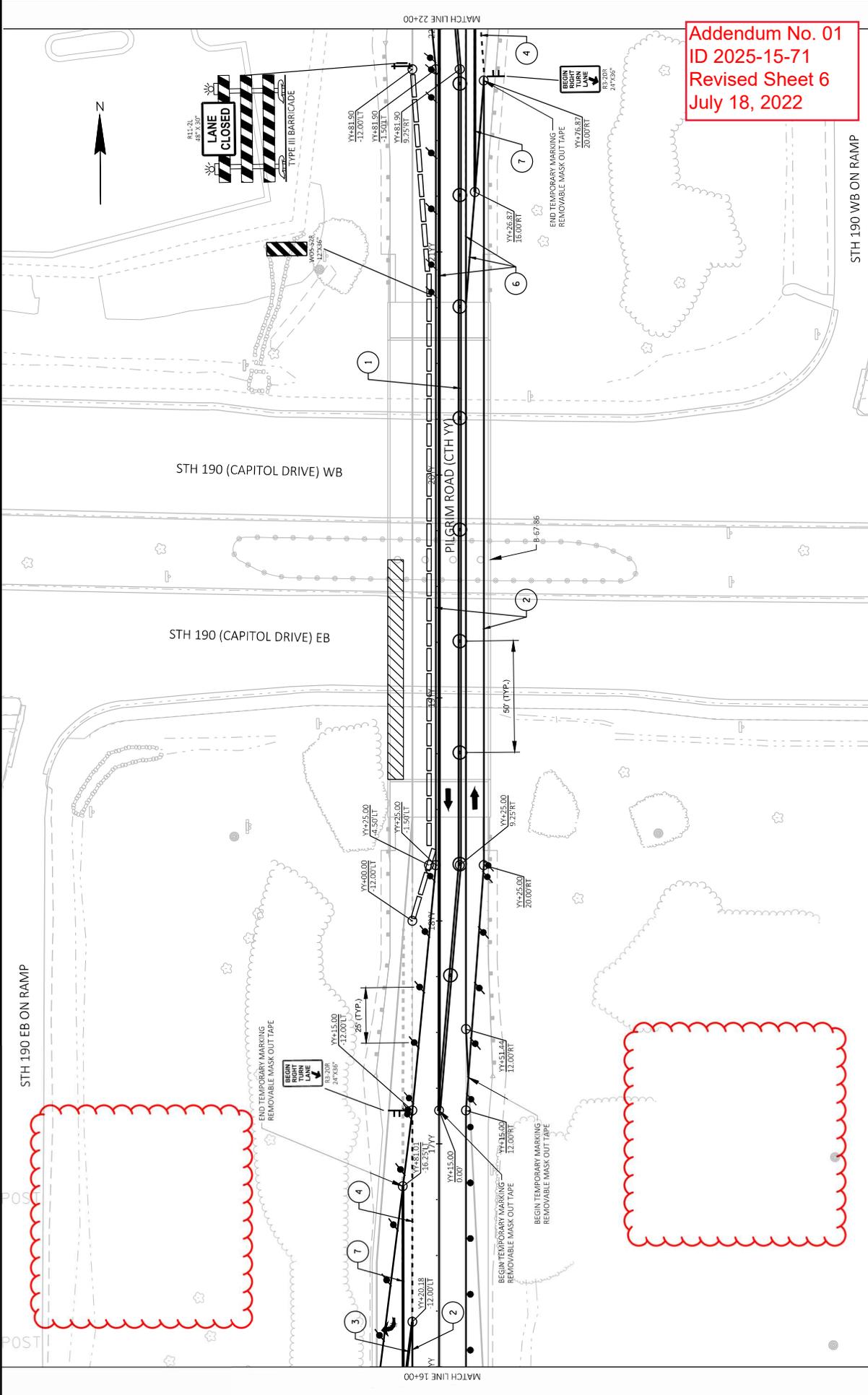
The following 8½ x 11-inch sheets are attached and made part of the plans for this proposal:

Revised: 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, and 21.

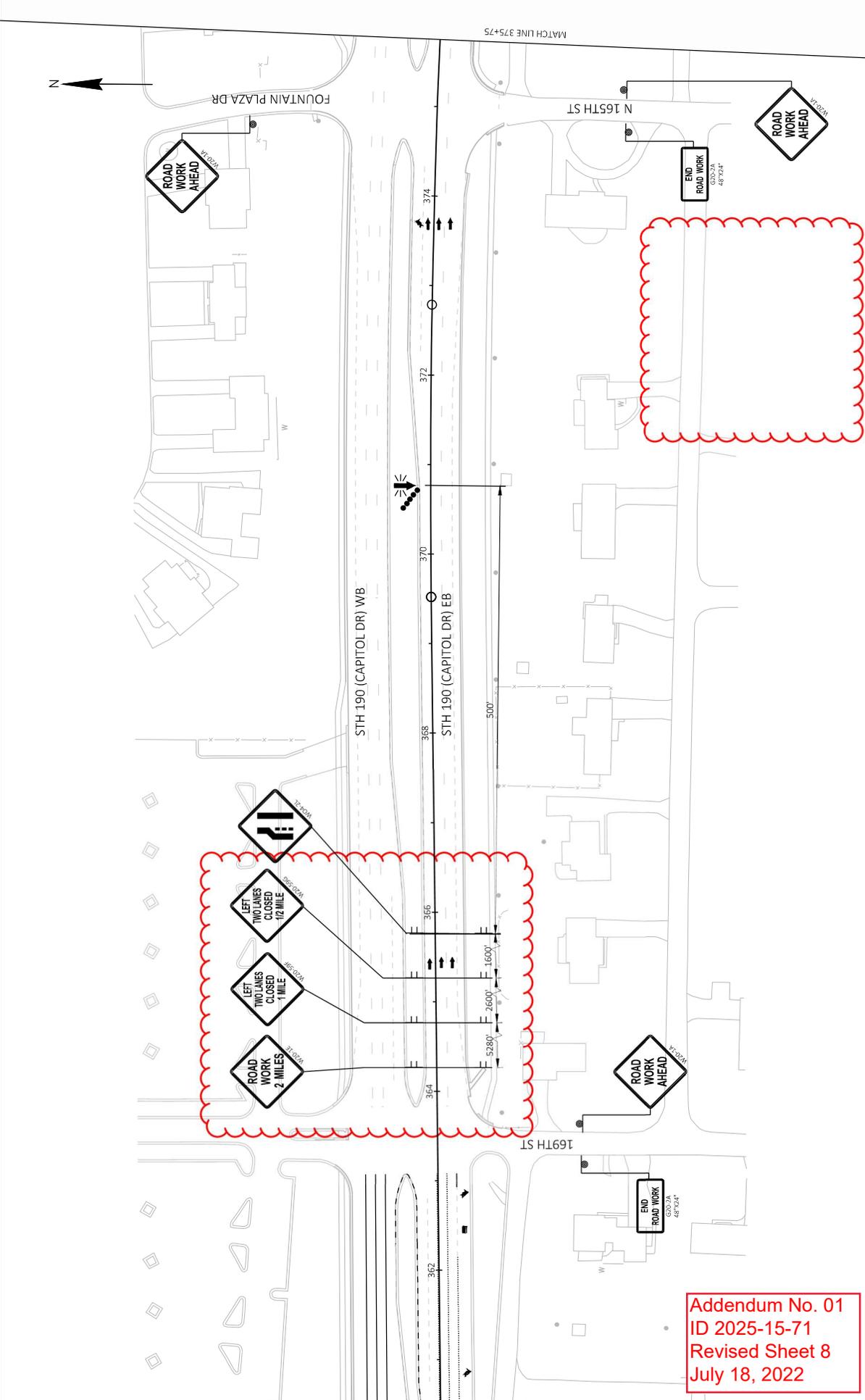
Added 4A, 45A, 47A-C, 48A, 62A, 69A, and 72A.

END OF ADDENDUM

Addendum No. 01
 ID 2025-15-71
 Revised Sheet 6
 July 18, 2022

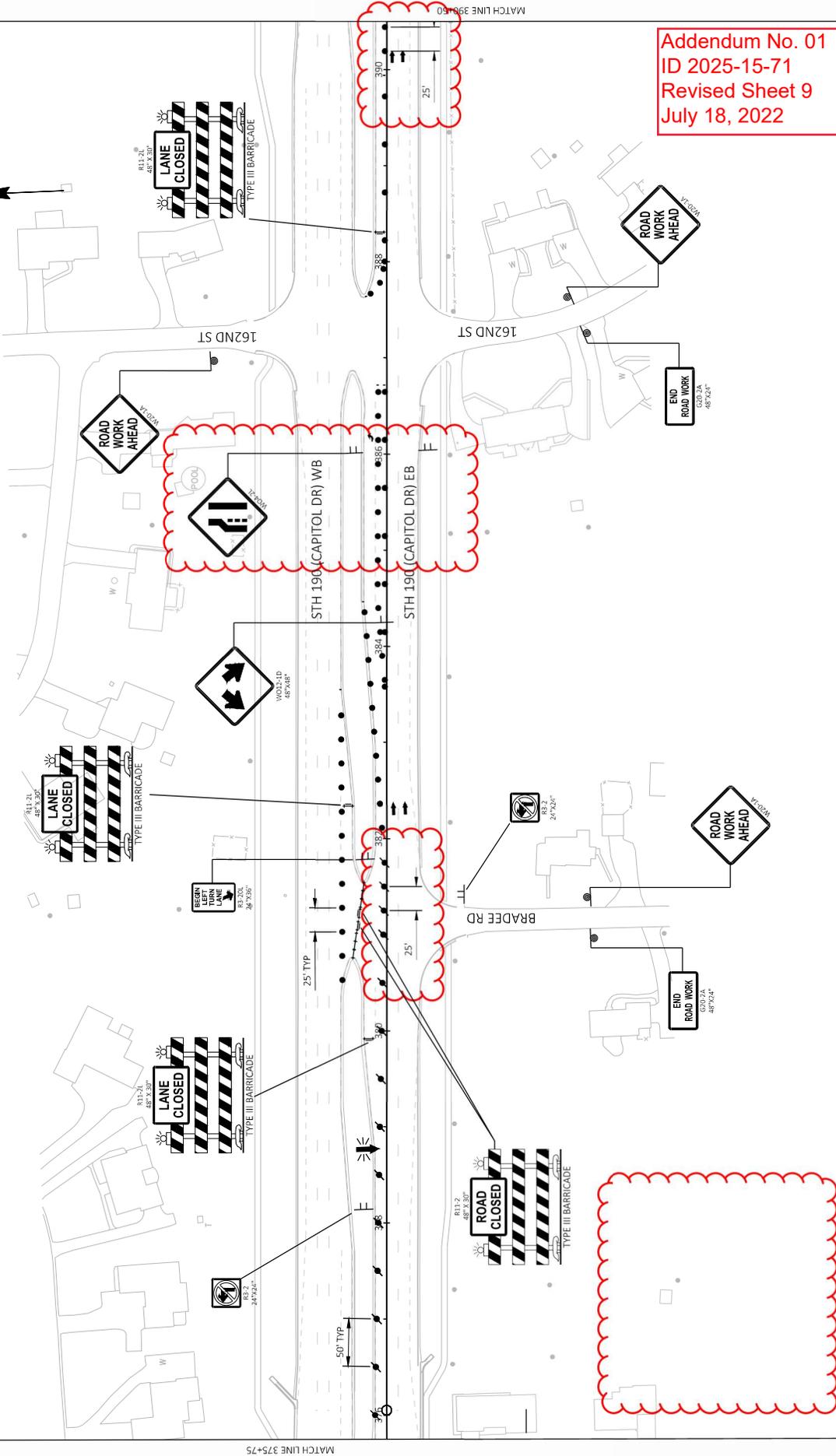


PROJECT NO: 2025-15-71	FILE NAME: U:\ST\WORK\CON\20251501\SHETS\PLAN\0600_01_PILGRIM.DWG	COUNTY: WAUKESHA	PILOT DATE: 7/17/2022 8:37 AM	PILOT BY: DAN BAUMANN	PILOT NAME: 1 INK(0) FT	SHEET: 6	E
LAYOUT NAME: _000	HWY: CTH YY	TRAFFIC CONTROL - STAGE 1 - PILGRIM RD	PILOT SCALE: 1 INK(0) FT	WISDOT/CADDS SHEET 42			



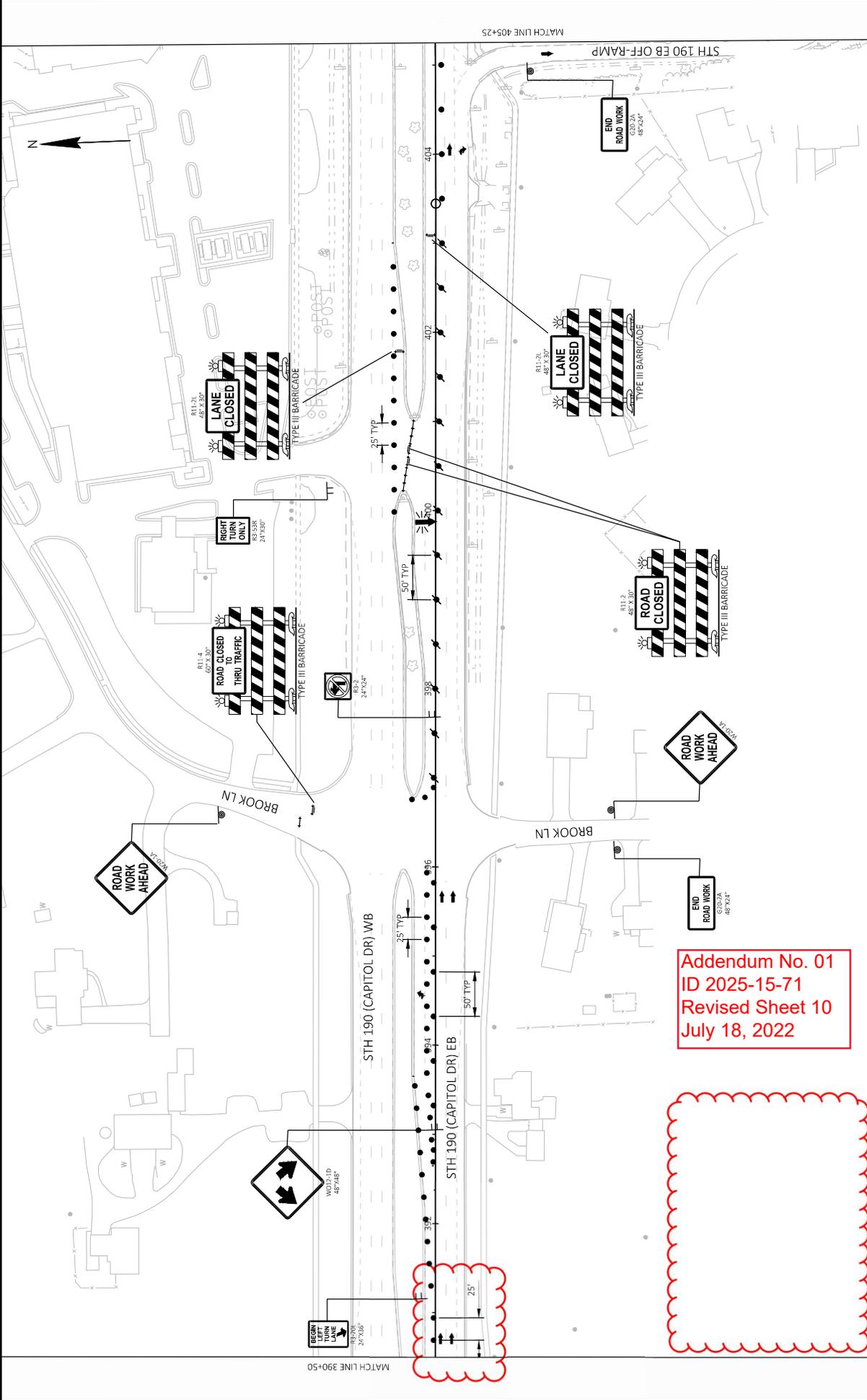
Addendum No. 01
 ID 2025-15-71
 Revised Sheet 8
 July 18, 2022

PROJECT NO: 2025-15-71	COUNTY: WAUKESHA	TRAFFIC CONTROL - CAPITOL DR S1 PARTIAL CLOSURE	SHEET 8
FILE NAME: I:\317\WIS07\CDR\20251571\SHIFTS\PLAN\026001_S1-CAPITOL.DWG	HWY: CTH YY	PLOT BY: DARR SUHMANN	PLOT DATE: 7/17/2022 9:05 AM
LAYOUT NAME: _201		PLOT NAME:	PLOT SCALE: 1 IN=100 FT
			WIS07/CARDS SHEET 42

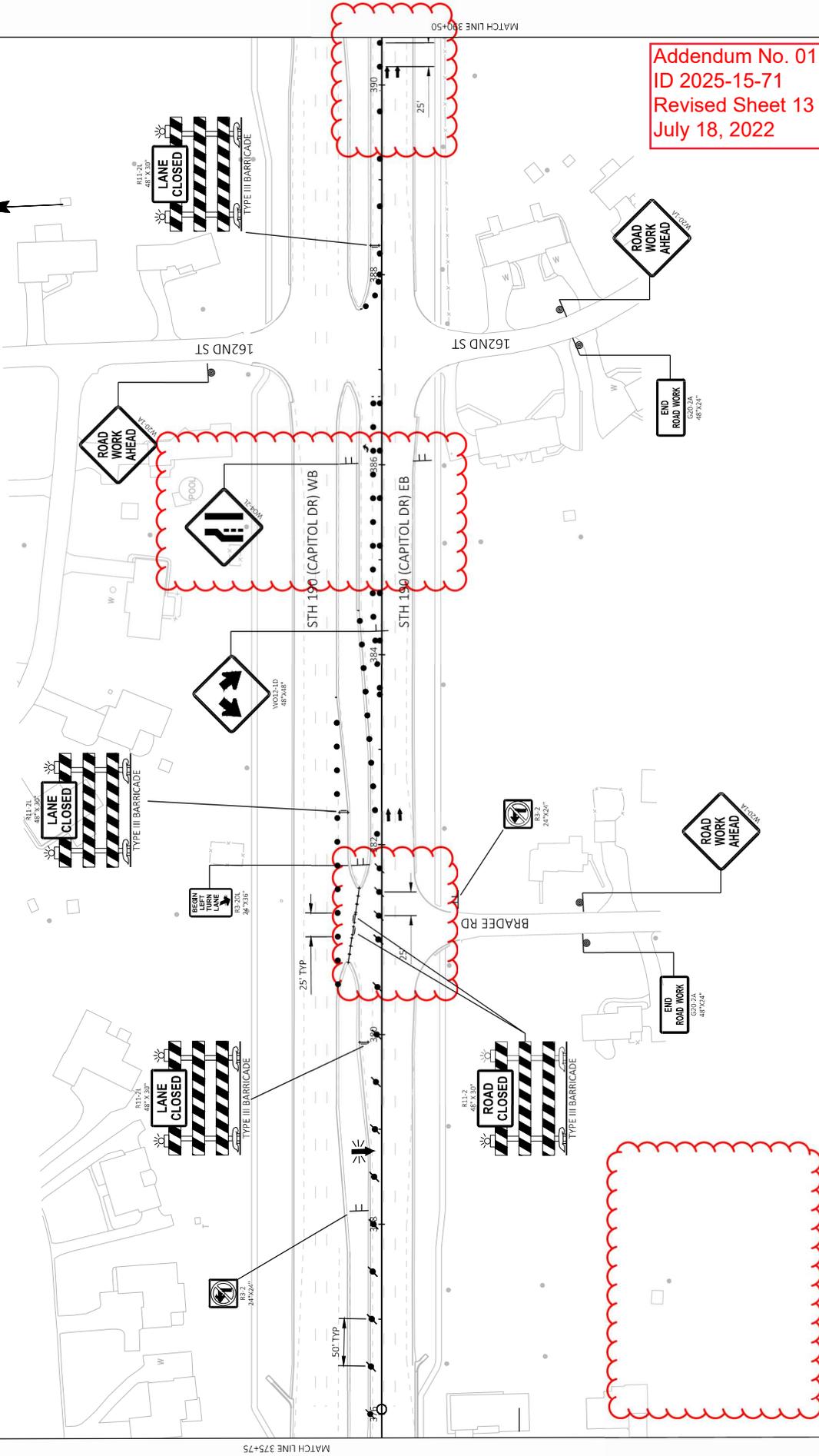


Addendum No. 01
 ID 2025-15-71
 Revised Sheet 9
 July 18, 2022

PROJECT NO: 2025-15-71	COUNTY: WAUKESHA	TRAFFIC CONTROL - CAPITOL DR S1 PARTIAL CLOSURE	SHEET 9
FILE NAME: I:\S1\WORK\PROJECTS\2025\1501\SHETS\PLAN\06001_S1-CAPITOL.DWG	HWY: CTH YY	PLOT DATE: 7/17/2022 9:06 AM	PLOT SCALE: 1 IN=100 FT
LAYOUT NAME: _002		PLOT BY: DAN SUMMANN	PLOT NAME:
			WISDOT/CADDS SHEET 42

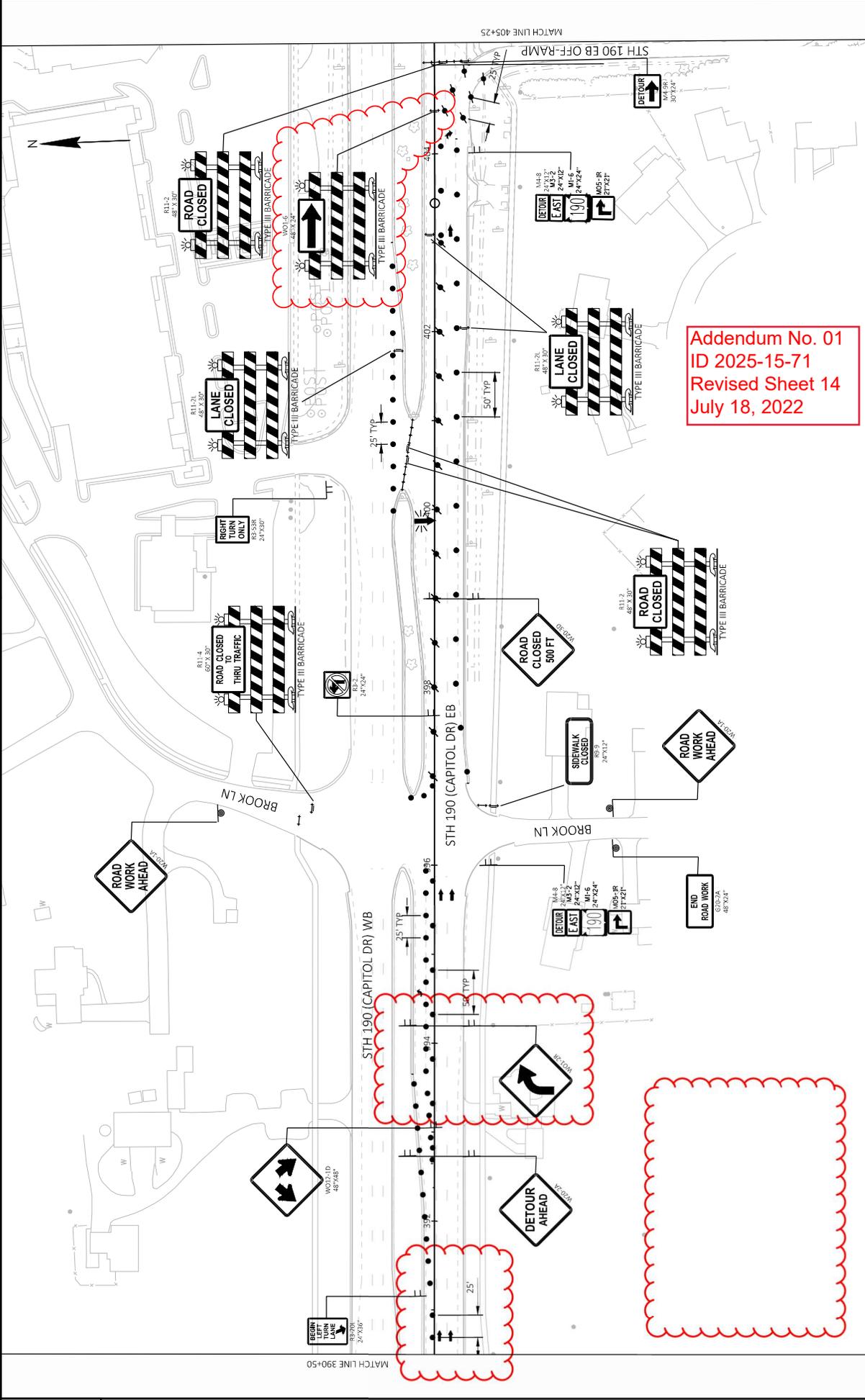


Addendum No. 01
ID 2025-15-71
Revised Sheet 10
July 18, 2022



Addendum No. 01
 ID 2025-15-71
 Revised Sheet 13
 July 18, 2022

PROJECT NO: 2025-15-71	COUNTY: WAUKESHA	TRAFFIC CONTROL - CAPITOL DR S1 FULL CLOSURE	SHEET 13	E
FILE NAME: I:\S1\WORKS\GSD\20251501\SHETS\AN\06001_S1-CAPITOL-FULLCLOSURE.DWG	HWY: CTH YY	PLOT DATE: 7/17/2022 2:14 PM	PLOT NAME: 1 IN 100 FT	WISDOT/CADDS SHEET 42
LAYOUT NAME: 0E				

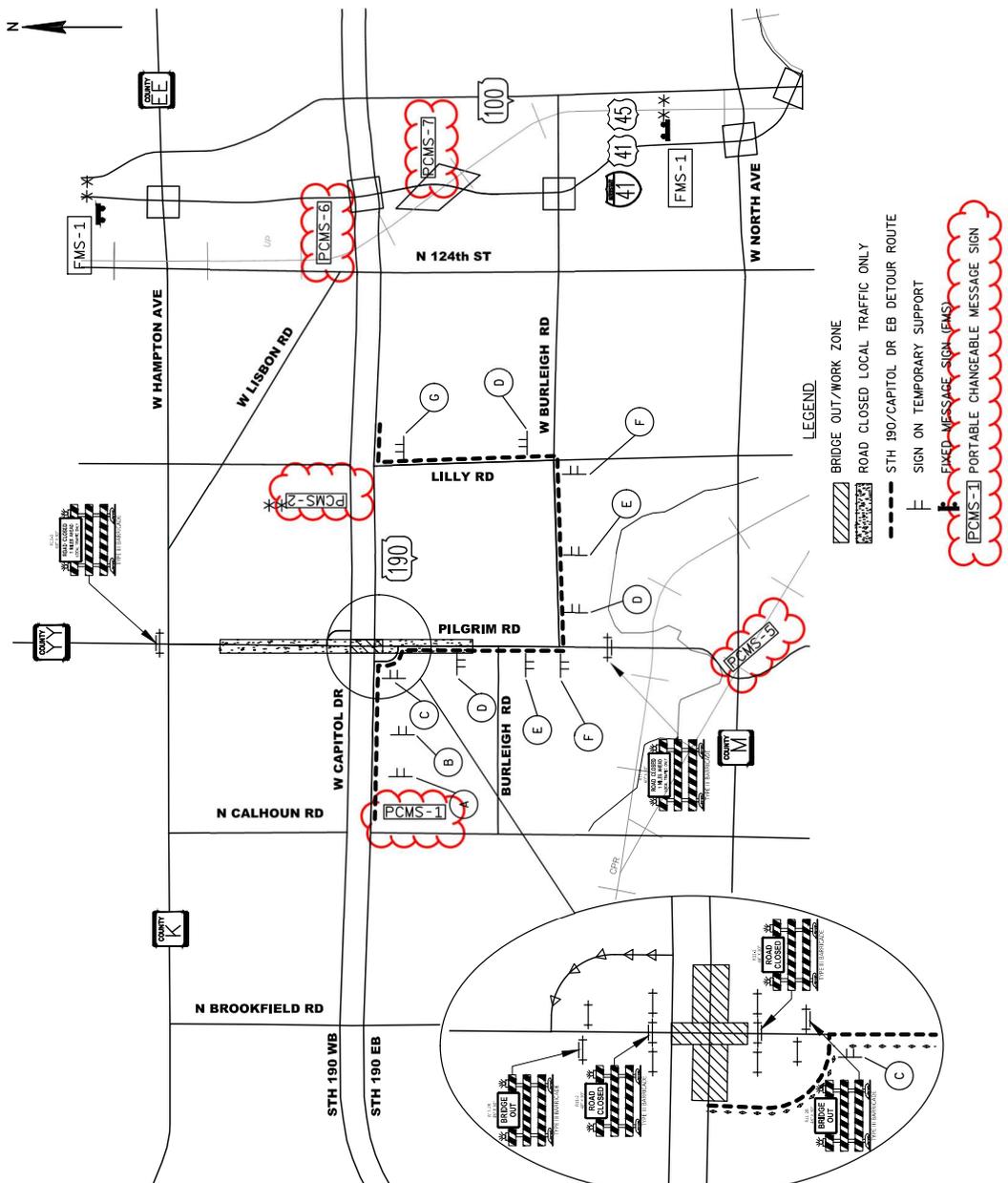


Addendum No. 01
 ID 2025-15-71
 Revised Sheet 14
 July 18, 2022

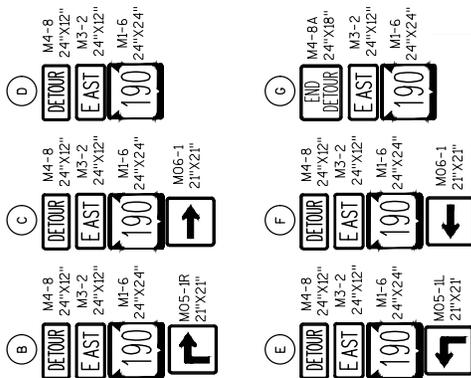
CAPITOL DR CLOSURE

DETOUR DURING DECK DEMO

- NOTES:**
- SIGN LOCATIONS ARE APPROXIMATE. THE ACTUAL LOCATION AND SPACING MAY BE ADJUSTED AS DIRECTED BY THE ENGINEER TO MEET FIELD CONDITIONS.
 - AFTER SIGNS HAVE BEEN LOCATED IN THE FIELD, BUT BEFORE INSTALLATION, THE PROJECT ENGINEER SHALL VERIFY EACH SIGN LOCATION.
 - CONTRACTOR SHOULD LOCATE FIXED MESSAGE SIGNS A MINIMUM OF 200' FROM EXISTING TYPE "M" SIGN.
 - SEE TEMPORARY SIGNING SHEETS FOR MORE INFORMATION.
 - THE MONTH AND DAY SHALL BE AS DIRECTED BY THE PROJECT ENGINEER FOR EACH SIGN.
 - EXISTING SIGNS IN CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE COVERED. THE COVERING OF EXISTING WOOD POST MOUNTED SIGNS IS INCLUDED UNDER ITEM 643.0920 TRAFFIC CONTROL COVERING SIGNS TYPE II.
 - ANY "STOP" SIGNS THAT ARE REMOVED FOR A CONSTRUCTION OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED.
 - SEE TRAFFIC CONTROL PLAN FOR ADDITIONAL TRAFFIC CONTROL SIGNS.
 - DO NOT PLACE ANY SIGNS OR TRAFFIC CONTROL DEVICES WITHIN 50' OF RAILROAD RIGHT OF WAY.



Addendum No. 01
ID 2025-15-71
Revised Sheet 17
July 18, 2022



MESSAGE PRIOR TO DECK DEMOLITION

FRAME 1

WIS 190
CLOSED
XXXXDAY

FRAME 2

WIS 190
REOPENS
XXXXDAY

PCMS 1
(7 DAYS PRIOR)

WIS 190
CLOSED
XXXXDAY

PCMS 2
(7 DAYS PRIOR)

WIS 190
REOPENS
XXXXDAY

MESSAGE DURING DECK DEMOLITION

FRAME 1

WIS 190
CLOSED
AHEAD

FRAME 2

FOLLOW
DETOUR

PCMS 1

PCMS 2

WIS 190
CLOSED
AHEAD

FOLLOW
DETOUR

MESSAGE DURING DECK DEMOLITION

FRAME 1

BRIDGE
CLOSED
2 MILES

FRAME 2

FOLLOW
DETOUR

PCMS 5

PCMS 6

WB 190
CLOSED
2 MILES

FOLLOW
DETOUR

WB 190
CLOSED
2 MILES

PCMS 7

FOLLOW
DETOUR

Addendum No. 01
ID 2025-15-71
Revised Sheet 18
July 18, 2022

PROJECT NO: 2025-15-71

FILE NAME: I:\317\WIS190\PCMS\SHIFTS\SPAN027001_01.DWG
LAYOUT NAME: 027002_P1.PCM5

HWY: CTH YY

COUNTY: WAUKESHA

PLOT DATE: 7/17/2022 2:03 PM

DETOUR SIGNS - PCMS

PLOT BY: DAN BAUMANN

PLOT NAME:

PLOT SCALE: 1 IN=20 FT

SHEET 18

E

WIS190/PCMS SHEET 18

TRAFFIC CONTROL ITEMS

ROADWAY	STAGE DURATION DAYS	TRAFFIC CONTROL DRUMS EACH* DAYS	TRAFFIC CONTROL BARRICADES TYPE II EACH* DAYS	TRAFFIC CONTROL FLEXIBLE TUBULAR MARKER POSTS EACH	TRAFFIC CONTROL FLEXIBLE TUBULAR MARKER TUBULAR MARKER BASES EACH	TRAFFIC CONTROL WARNING LIGHTS TYPE A EACH* DAYS	TRAFFIC CONTROL WARNING LIGHTS TYPE C EACH* DAYS	TRAFFIC CONTROL ARROW BOARDS EACH* DAYS	TRAFFIC CONTROL SIGNS EACH* DAYS	TRAFFIC CONTROL SIGNS COVERING TYPE II EACH*	643.1050* TRAFFIC CONTROL SIGNS PCMS EACH* DAYS
PILGRIM ROAD (CTH YY)	56	57	2	12	12	4	28	--	--	--	--
STH 190 (CAPTOL DRVE) FULL CLOSURE	9	171	69	--	--	138	--	3	74	--	18
STH 190 (CAPTOL DRVE) PARTIAL CLOSURE	20	89	1	--	--	2	--	1	22	--	--
UNDISTRIBUTED				3	3					10	
TRAFFIC CONTROL ITEMS SUBTOTAL		6,511	753	15	15	1,506	1,568	47	2,114	10	18

^ FOR INFORMATION ONLY
 * ADDITIONAL QUANTITIES ELSEWHERE

Addendum No. 01
 ID 2025-15-71
 Revised Sheet 21
 July 18, 2022

DETOUR ITEMS

ROADWAY	STAGE DURATION DAYS	TRAFFIC CONTROL BARRICADES TYPE III EACH* DAYS	TRAFFIC CONTROL WARNING LIGHTS TYPE A EACH* DAYS	TRAFFIC CONTROL WARNING LIGHTS TYPE II EACH* DAYS	TRAFFIC CONTROL SIGNS TYPE II EACH* DAYS	TRAFFIC CONTROL SIGNS COVERING TYPE II EACH* DAYS	TRAFFIC CONTROL SIGNS SF	643.1050 TRAFFIC CONTROL SIGNS PCMS EACH* DAYS
PILGRIM ROAD (CTH YY)	65	12	24	1,560	117	7,605	--	2
STH 190 (CAPITOL DRIVE)	9	12	108	24	216	43	90	5
UNDISTRIBUTED								
DETOUR ITEMS TOTAL		888	1,776	7,992	5	5	90	175

^ FOR INFORMATION ONLY
 * ADDITIONAL QUANTITIES ELSEWHERE

ALL ITEMS CATEGORY 0010
 UNLESS OTHERWISE NOTED

GENERAL NOTES
 DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.
 "W" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
 THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
 THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.
 WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL SIGNS SHOULD BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

LEGEND
 SIGN ON PORTABLE OR PERMANENT SUPPORT
 TEMPORARY PORTABLE RUMBLE STRIP ARRAY
 DIRECTION OF TRAFFIC
 WORK AREA
 FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

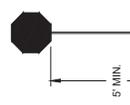
FLAGGING
 FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.
 FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
 SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
 WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS
 UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.
 EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 16 FEET ON CENTER. ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.
 INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.
 PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.
 DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

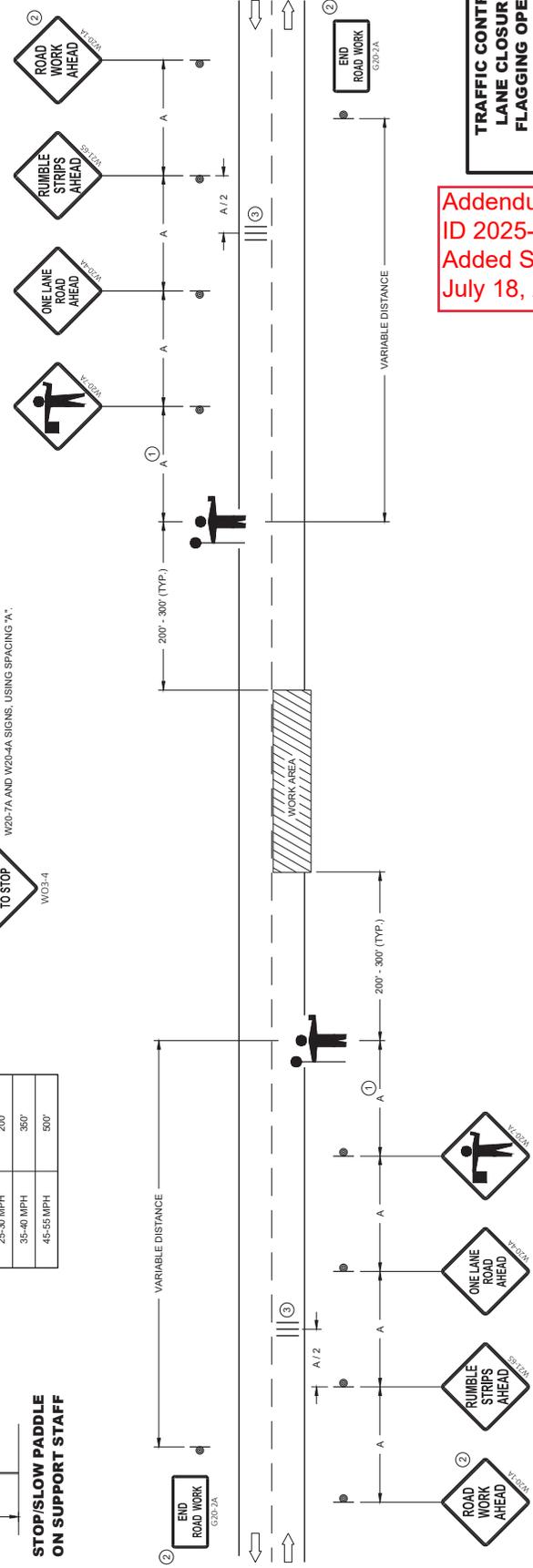
SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'

BE PREPARED TO STOP W03-4
 USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-7A SIGNS, USING SPACING "A".



STOP/SLOW PADDLE ON SUPPORT STAFF



Addendum No. 01
 ID 2025-15-71
 Added Sheet 45A
 July 18, 2022

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

APPROVED
 November 2021 DATE
 /S/ Andrew Hedling WORK ZONE ENGINEER
 E:PHM

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" MAY BE USED IF APPROVED BY THE DISTRICT TRAFFIC UNIT.

W SIGNS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR DOWNED SIGNS. THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (50W DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS ON THE WISCONSIN STANDARD SIGN PLATES.

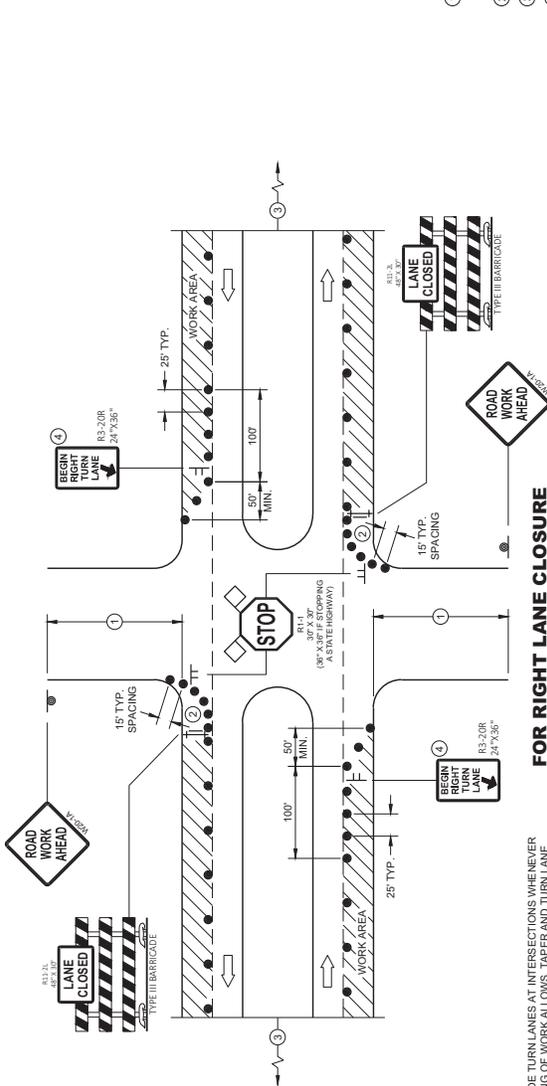
SIGNS THAT WILL REMAIN IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON PORTABLE SUPPORTS. BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

- ① 500' TYPICAL OR AT LAST INTERSECTION, WHICHEVER IS CLOSER, 350' IF 35-40 MPH, 200' IF 25-30 MPH.
- ② ALSO USE BARRICADE AND 15 FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS
- ③ SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.
- ④ MINIMUM MOUNTING HEIGHT OF 5 FEET FROM EDGE OF PAVEMENT (AT EDGE LINE LOCATION) TO BOTTOM OF SIGN.

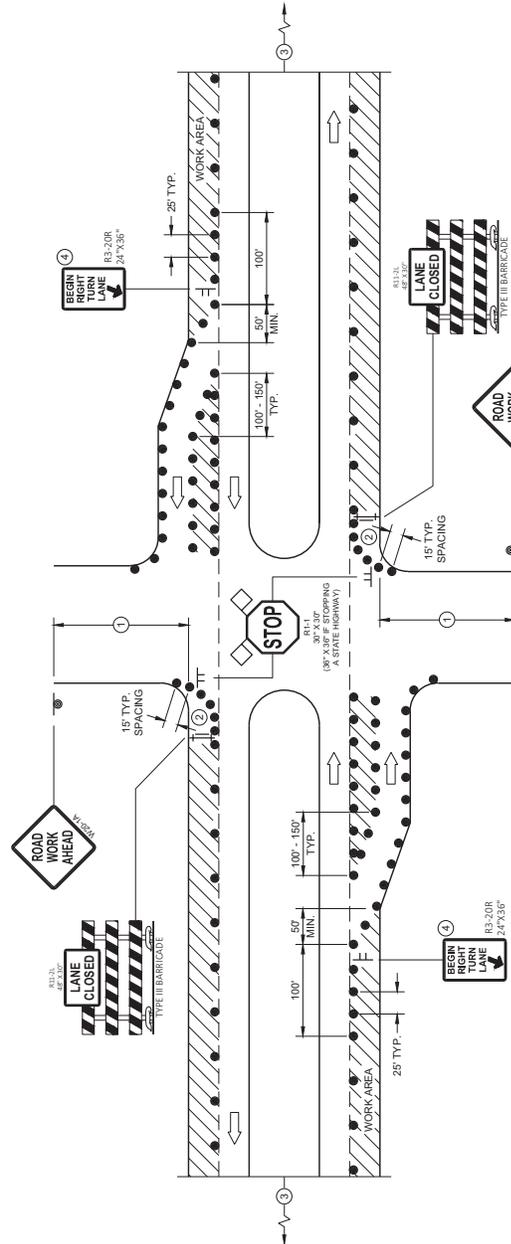
LEGEND

- ⊥ SIGN ON TEMPORARY SUPPORT
- ⊥ SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- ⇄ TYPE III BARRICADE WITH ATTACHED SIGN
- ➡ DIRECTION OF TRAFFIC
- ◇ FLAGS, 16" X 16" MIN., ORANGE
- ▨ WORK AREA



FOR RIGHT LANE CLOSURE AT INTERSECTION

PROVIDE TURN LANES AT INTERSECTIONS WHENEVER STAGING OF WORK ALLOWS. TAPER AND TURN LANE SIGNAGE ON FIELD CONDITIONS AS APPROVED BY THE ENGINEER.



FOR RIGHT LANE CLOSURE AT INTERSECTION (WITH RIGHT TURN BAY OPEN)

Addendum No. 01
ID 2025-15-71
Added Sheet 47B
July 18, 2022

**TRAFFIC CONTROL,
INTERSECTION WITHIN SINGLE
RIGHT LANE CLOSURE**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" MAY BE USED IF APPROVED BY THE DISTRICT TRAFFIC UNIT.

"W" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS. THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT EXCEED THE MINIMUM PROVIDE (MINIMUM OF 250 FEET (80' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL REMAIN IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON PORTABLE SUPPORTS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

- 1 500 TYPICAL OR AT LAST INTERSECTION, WHICHEVER IS CLOSER.
350' IF 35 - 40 MPH,
200' IF 25 - 30 MPH.
- 2 ALSO USE BARRICADE AND 15 FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS
- 3 SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.
- 4 MINIMUM MOUNTING HEIGHT OF 5 FEET FROM EDGE OF PAVEMENT (AT EDGE LINE LOCATION) TO BOTTOM OF SIGN.

6

LEGEND

— SIGN ON TEMPORARY SUPPORT

— SIGN ON PERMANENT SUPPORT

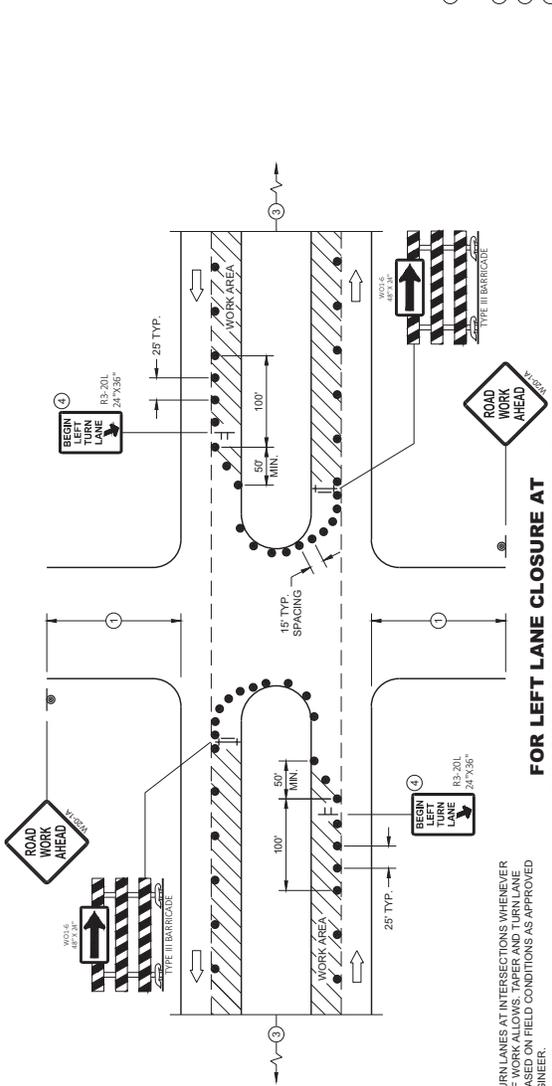
● TRAFFIC CONTROL DRUM

⇄ TYPE III BARRICADE WITH ATTACHED SIGN

⇨ DIRECTION OF TRAFFIC

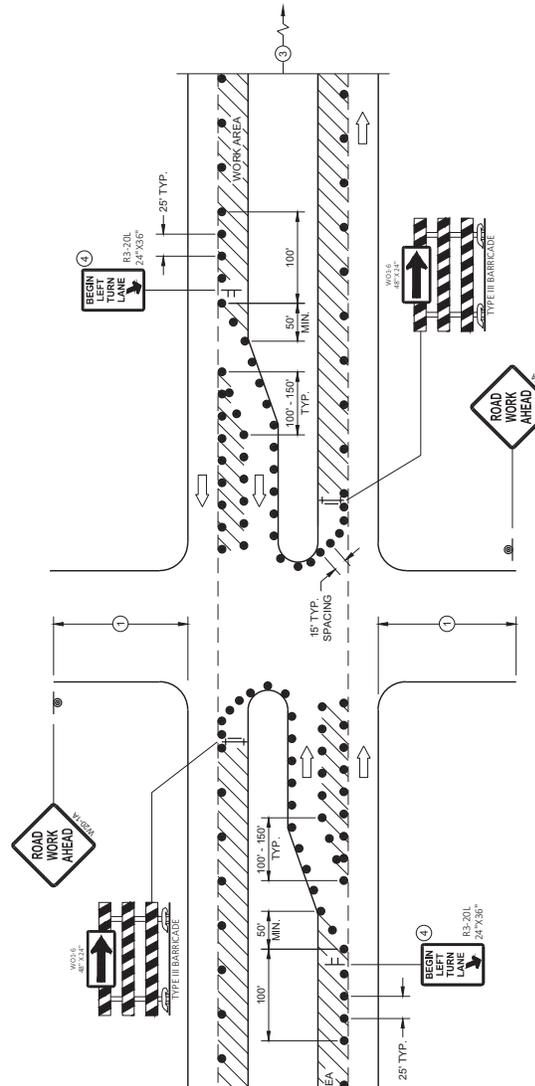
◇ FLAGS, 16" X 16" MIN., ORANGE

▨ WORK AREA



FOR LEFT LANE CLOSURE AT INTERSECTION OR MEDIAN OPENING

PROVIDE TURN LANES AT INTERSECTIONS WHENEVER STAGING OF WORK ALLOWS. TAPER AND TURN LANE LENGTHS BASED ON FIELD CONDITIONS AS APPROVED BY THE ENGINEER.



FOR LEFT LANE CLOSURE AT INTERSECTION OR MEDIAN OPENING (WITH LEFT TURN BAY OPEN)

6

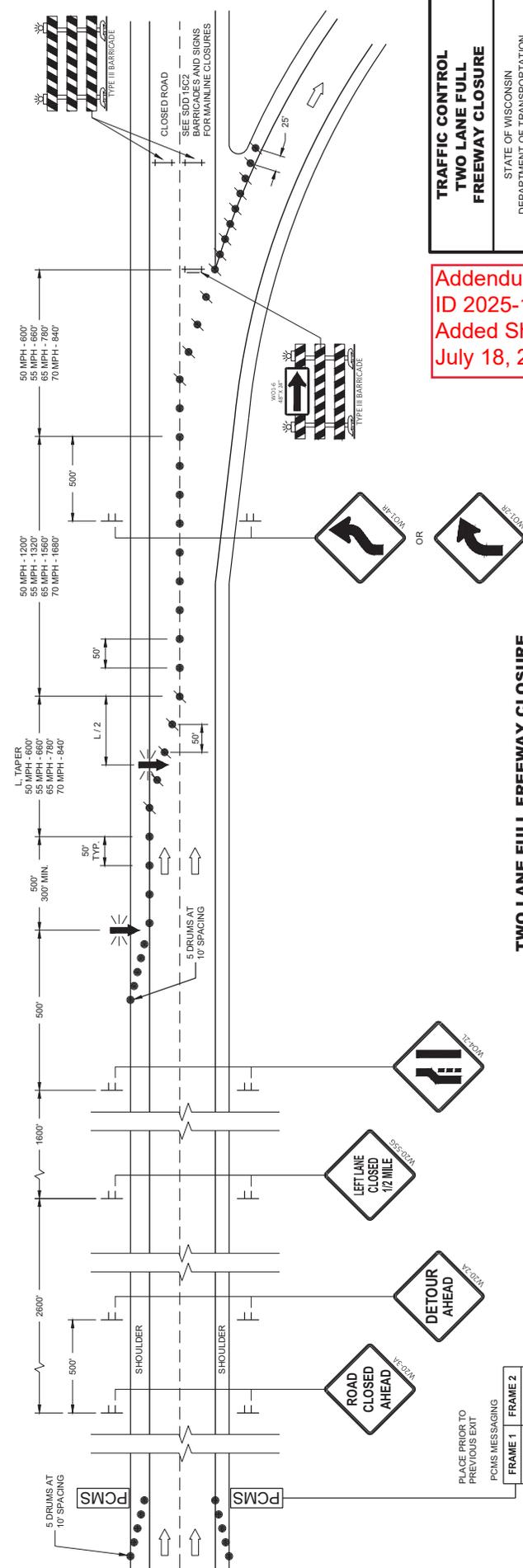
Addendum No. 01
ID 2025-15-71
Added Sheet 47C
July 18, 2022

TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LEFT LANE CLOSURE
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION
APPROVED _____ /S/ Andrew Hedling DATE August 2020 WORK ZONE ENGINEER E.P.M.A.

GENERAL NOTES

- RIGHT CLOSURE SHOWN (LEFT LANE CLOSURE SIMILAR)
- THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS PART OF A DAYTIME ONLY OPERATION.
- ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.
- "W" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.
- WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROL SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND / OR THE SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.
- PLACE A TYPE III BARRICADE APPROXIMATELY EVERY 1000' ACROSS EACH CLOSED LANE TO HELP REINFORCE THE DRUM LINE.
- CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.
- REFER TO DETOUR ROUTES FOR TRAFFIC GUIDANCE SIGNING.
- ANY SIGNS TEMPORARILY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON "COVERED" OR "DOWNED" SIGNS.

- LEGEND**
- TYPE III BARRICADE
 - TYPE III BARRICADE WITH ATTACHED SIGN
 - SIGN ON TEMPORARY SUPPORT
 - SIGN ON PERMANENT SUPPORT
 - TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
 - TRAFFIC CONTROL DRUM
 - TYPE "A" WARNING LIGHT (FLASHING)
 - DIRECTION OF TRAFFIC
 - WORK AREA
 - FLASHING ARROW BOARD
 - PORTABLE CHANGEABLE MESSAGE SIGN



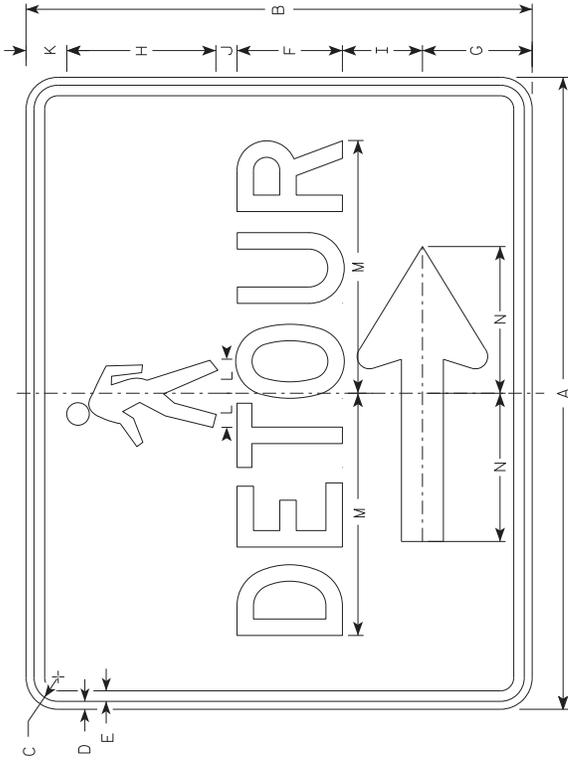
TRAFFIC CONTROL TWO LANE FULL FREEWAY CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED AUGUST 2019	DATE
_____/S/ Andrew Hedding	WORK ZONE ENGINEER

Addendum No. 01
 ID 2025-15-71
 Added Sheet 48A
 July 18, 2022

TWO LANE FULL FREEWAY CLOSURE

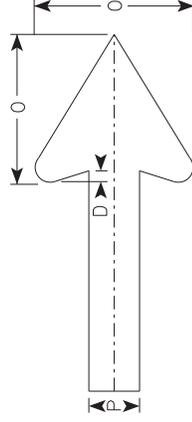
NOTES

1. Sign is Type II-Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9BL is the same as M4-9BR except the arrow is reversed.



M4-9BR

Addendum No. 01
ID 2025-15-71
Added Sheet 62A
July 18, 2022



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Acq. qt.
1																											
2	30	24	1 1/8	3/8	1/2	5	5 1/4	7 1/8	3 3/4	1	1 1/8	1 5/8	11 3/4	7	6	2											5.00
3																											
4																											
5																											

STANDARD SIGN
M4-9B L&R
WISCONSIN DEPT. OF TRANSPORTATION
APPROVED *Matthew R. Raush*
For state Traffic Engineer
DATE 7/1/19 PLATE NO. M4-9B.2

NOTES

1. Sign is Type II - Type H Reflective

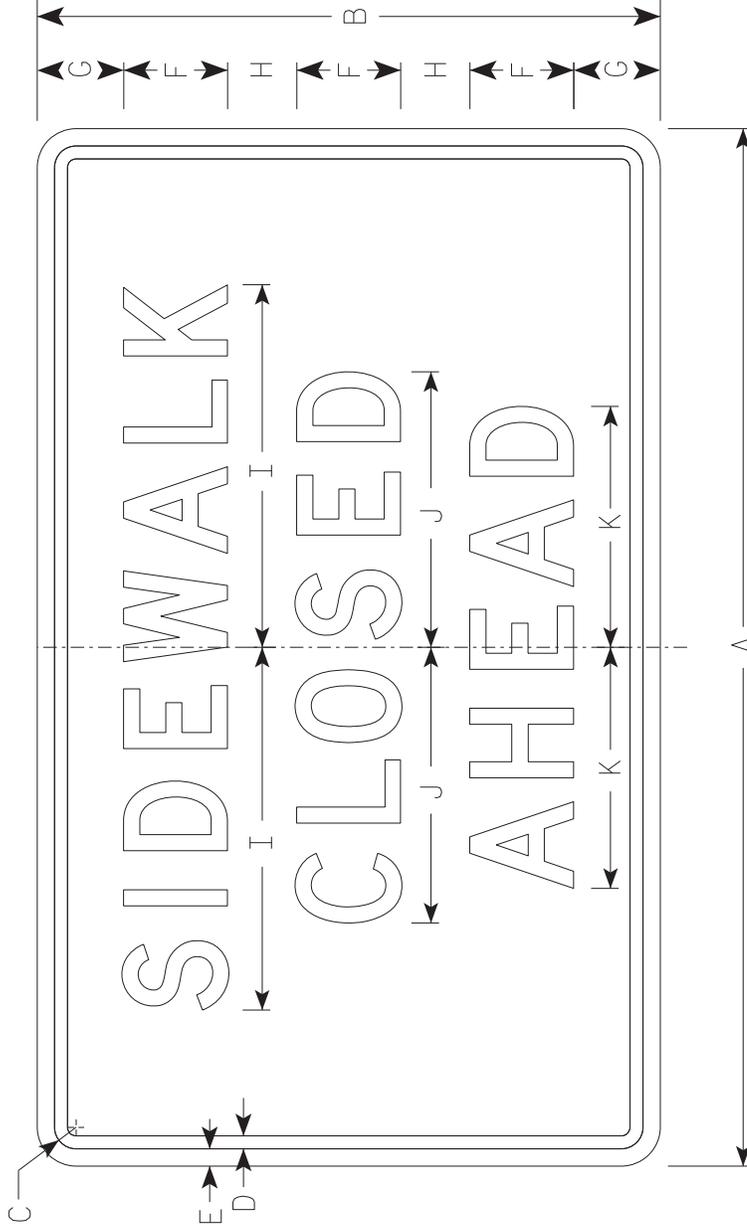
2. Color:

Background - White

Message - Black

3. Message Series - D

4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



Addendum No. 01
ID 2025-15-71
Added Sheet 69A
July 18, 2022

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	SPR. NO.
1																											
2S	30	18	1 1/8	3/8	1/2	3	2 1/2	2	10 1/2	8	7																3.75
2M	30	18	1 1/8	3/8	1/2	3	2 1/2	2	10 1/2	8	7																3.75
3																											
4																											
5																											

R9-9A

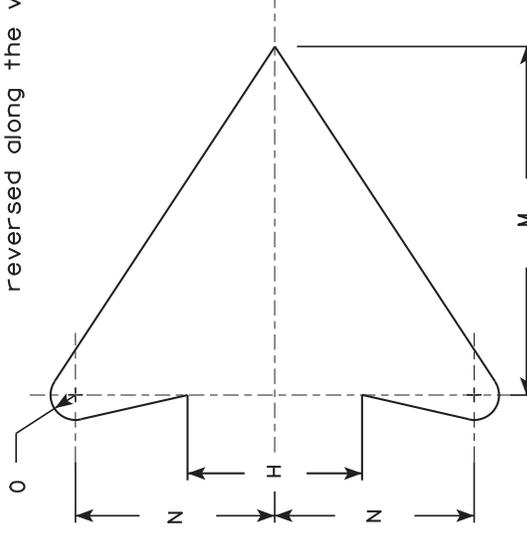
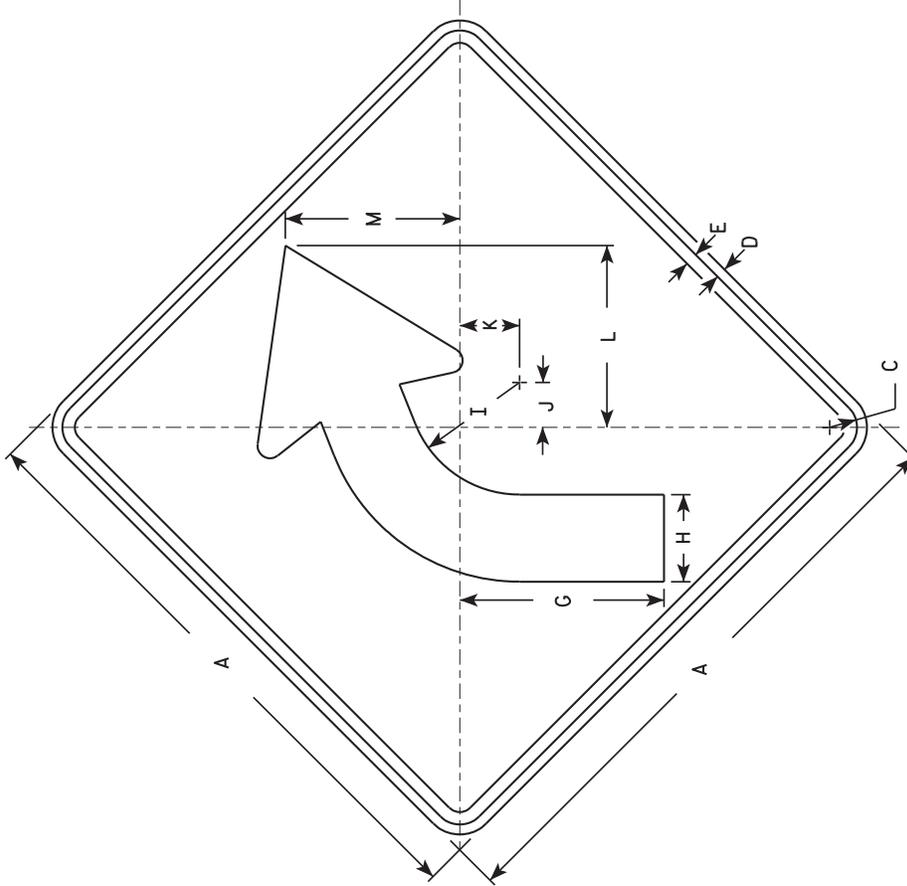
STANDARD SIGN
R9-9A
WISCONSIN DEPT OF TRANSPORTATION
APPROVED <i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 8/31/2020 PLATE NO. R9-9A.1

PROJECT NO:	COUNTY:	HWY:	SHEET NO: 69A	E
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NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-2L is the same as W1-2R except the arrow is reversed along the vertical centerline.

Addendum No. 01
ID 2025-15-71
Added Sheet 72A
July 18, 2022



ARROW DETAIL

W1-2R

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	R	S	T	U	V	W	X	Y	Z	W ₁₀₀ sq. ft.
1	24	1 1/8	3/8	1/2	1/2	8 1/4	3 1/2	4 1/2	1 3/4	2 3/8	7 1/4	7	4	1/2												4.0
2S	30	1 3/8	1/2	5/8	3/4	10 1/4	4 3/8	5 5/8	2 1/4	3	9 1/8	8 3/4	5	5/8												6.25
2M	36	1 5/8	5/8	3/4	3/4	12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
3	36	1 5/8	5/8	3/4	3/4	12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
4	36	1 5/8	5/8	3/4	3/4	12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
5	48	2 1/4	3/4	1	1	16 1/2	7	9	3 1/2	4 5/8	14 1/2	14	8	1												16.0

STANDARD SIGN
W1-2

WISCONSIN DEPT. OF TRANSPORTATION
APPROVED
Matthew R. Rauch
State Traffic Engineer
DATE 5/15/12 PLATE NO. WI-2-10



Proposal Schedule of Items

Proposal ID: 20220809009 Project(s): 2025-15-71

Federal ID(s): N/A

SECTION: 0001

Contract Items

Alt Set ID:

Alt Mbr ID:

Proposal Line Number	Item ID Description	Approximate Quantity and Units	Unit Price	Bid Amount
0002	203.0220 Removing Structure (structure) 01.B-67-86	1.000 EACH	_____.	_____.
0004	213.0100 Finishing Roadway (project) 01. 2025-15-71	1.000 EACH	_____.	_____.
0006	502.0100 Concrete Masonry Bridges	23.000 CY	_____.	_____.
0008	502.3101 Expansion Device	6.000 LF	_____.	_____.
0010	502.3200 Protective Surface Treatment	42.000 SY	_____.	_____.
0012	502.3210 Pigmented Surface Sealer	33.000 SY	_____.	_____.
0014	502.4106 Adhesive Anchors 3/4-inch	1.000 EACH	_____.	_____.
0016	505.0600 Bar Steel Reinforcement HS Coated Structures	4,960.000 LB	_____.	_____.
0018	506.0105 Structural Steel Carbon	11,630.000 LB	_____.	_____.
0020	506.3025 Welded Stud Shear Connectors 7/8x8-Inch	264.000 EACH	_____.	_____.
0022	517.0600 Painting Epoxy System (structure) 01.B-67-86	LS	LUMP SUM	_____.
0024	517.0901.S Preparation and Coating of Top Flanges (structure) 01.B-67-86	1.000 EACH	_____.	_____.
0026	517.3001.S Structure Overcoating Cleaning and Priming (structure) 01.B-67-86	1.000 EACH	_____.	_____.
0028	517.4001.S Containment and Collection of Waste Materials (structure) 01.B-67-86	1.000 EACH	_____.	_____.



Proposal Schedule of Items

Proposal ID: 20220809009 Project(s): 2025-15-71

Federal ID(s): N/A

SECTION: 0001

Contract Items

Alt Set ID:

Alt Mbr ID:

Proposal Line Number	Item ID Description	Approximate Quantity and Units	Unit Price	Bid Amount
0030	603.8000 Concrete Barrier Temporary Precast Delivered	383.000 LF	_____.	_____.
0032	603.8125 Concrete Barrier Temporary Precast Installed	383.000 LF	_____.	_____.
0034	618.0100 Maintenance And Repair of Haul Roads (project) 01. 2025-15-71	1.000 EACH	_____.	_____.
0036	619.1000 Mobilization	1.000 EACH	_____.	_____.
0038	628.7020 Inlet Protection Type D	2.000 EACH	_____.	_____.
0040	634.0618 Posts Wood 4x6-Inch X 18-FT	4.000 EACH	_____.	_____.
0042	637.2210 Signs Type II Reflective H	20.000 SF	_____.	_____.
0044	637.2230 Signs Type II Reflective F	20.000 SF	_____.	_____.
0046	638.2102 Moving Signs Type II	4.000 EACH	_____.	_____.
0048	638.2602 Removing Signs Type II	1.000 EACH	_____.	_____.
0050	638.3000 Removing Small Sign Supports	4.000 EACH	_____.	_____.
0052	642.5201 Field Office Type C	1.000 EACH	_____.	_____.
0054	643.0300 Traffic Control Drums	6,511.000 DAY	_____.	_____.
0056	643.0420 Traffic Control Barricades Type III	1,641.000 DAY	_____.	_____.
0058	643.0500 Traffic Control Flexible Tubular Marker Posts	15.000 EACH	_____.	_____.



Proposal Schedule of Items

Proposal ID: 20220809009 Project(s): 2025-15-71

Federal ID(s): N/A

SECTION: 0001

Contract Items

Alt Set ID:

Alt Mbr ID:

Proposal Line Number	Item ID Description	Approximate Quantity and Units	Unit Price	Bid Amount
0060	643.0600 Traffic Control Flexible Tubular Marker Bases	15.000 EACH	_____.	_____.
0062	643.0705 Traffic Control Warning Lights Type A	3,282.000 DAY	_____.	_____.
0064	643.0715 Traffic Control Warning Lights Type C	1,568.000 DAY	_____.	_____.
0066	643.0800 Traffic Control Arrow Boards	47.000 DAY	_____.	_____.
0068	643.0900 Traffic Control Signs	10,106.000 DAY	_____.	_____.
0070	643.0920 Traffic Control Covering Signs Type II	15.000 EACH	_____.	_____.
0072	643.1000 Traffic Control Signs Fixed Message	90.000 SF	_____.	_____.
0074	643.1050 Traffic Control Signs PCMS	185.000 DAY	_____.	_____.
0076	643.5000 Traffic Control	1.000 EACH	_____.	_____.
0078	646.1020 Marking Line Epoxy 4-Inch	796.000 LF	_____.	_____.
0080	646.5020 Marking Arrow Epoxy	1.000 EACH	_____.	_____.
0082	646.5120 Marking Word Epoxy	1.000 EACH	_____.	_____.
0084	646.9300 Marking Removal Special Marking	2.000 EACH	_____.	_____.
0086	649.0150 Temporary Marking Line Removable Tape 4-Inch	3,064.000 LF	_____.	_____.
0088	649.0250 Temporary Marking Line Removable Tape 8-Inch	53.000 LF	_____.	_____.
0090	649.0960 Temporary Marking Removable Mask Out Tape 6-Inch	1,816.000 LF	_____.	_____.



Proposal Schedule of Items

Proposal ID: 20220809009 Project(s): 2025-15-71

Federal ID(s): N/A

SECTION: 0001

Contract Items

Alt Set ID:

Alt Mbr ID:

Proposal Line Number	Item ID Description	Approximate Quantity and Units	Unit Price	Bid Amount
0092	649.0970 Temporary Marking Removable Mask Out Tape 10-Inch	245.000 LF	_____.	_____.
0094	715.0502 Incentive Strength Concrete Structures	500.000 DOL	1.00000	500.00
0096	SPV.0060 Special 01. Heat Straightening of Damaged Girders	1.000 EACH	_____.	_____.
0098	SPV.0060 Special 02. Counterweight Structure	1.000 EACH	_____.	_____.
Section: 0001			Total:	_____.
			Total Bid:	_____.

