

MAD  
PROJECT ID:  
WITH: N/A

5573-03-73

COUNTY:  
SAUK

JANUARY 2022

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 48

# STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

## PLAN OF PROPOSED IMPROVEMENT

# REEDSBURG - BARABOO

CTH PF TO V WEST BARABOO W LIMIT

STH 136

SAUK COUNTY

STATE PROJECT NUMBER

5573-03-73

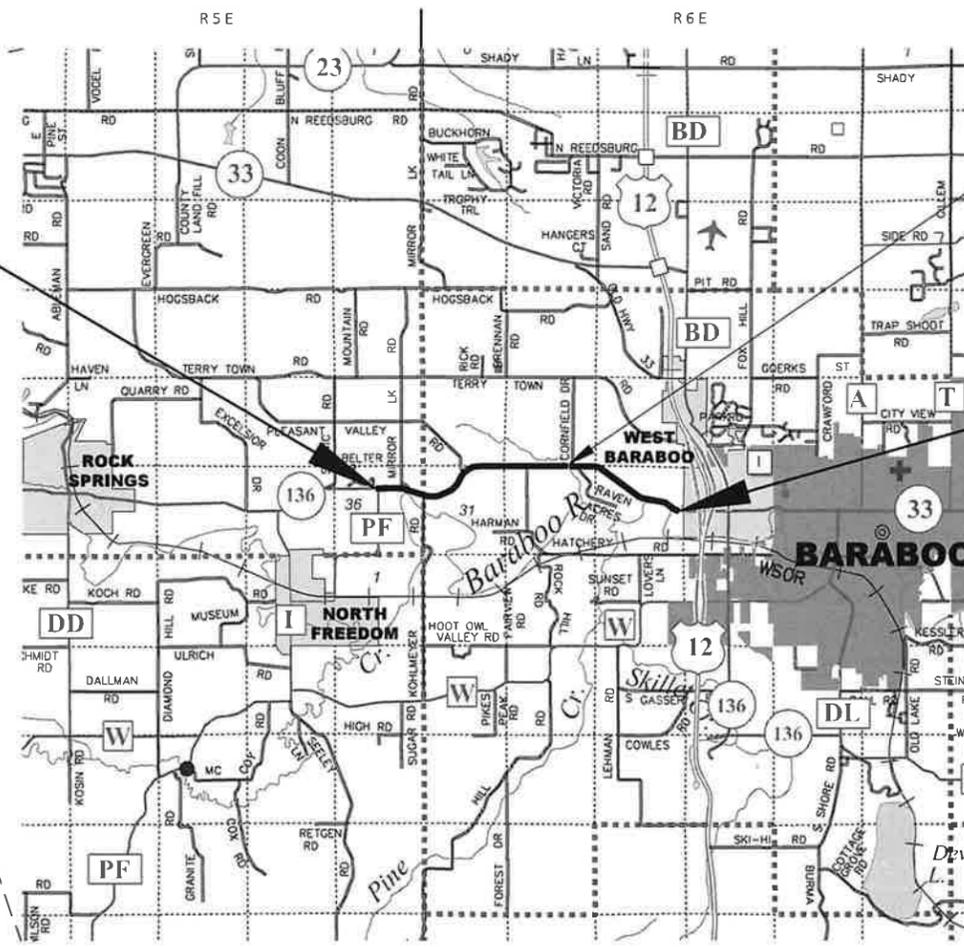
STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5573-03-73	WISC 2022116	1



**BEGIN PROJECT**  
STA 8+25.00  
Y=240,264.895  
X=622,893.962

EXCEPTION TO NET LENGTH OF CENTERLINE  
STA. 130+49.33 TO STA. 131+56.99  
STRUCTURE: B-56-81

**END PROJECT**  
STA 202+25.00  
Y=238,944.669  
X=640,714.533



TOTAL NET LENGTH OF CENTERLINE = 3.654 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), SAUK COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 18.

DESIGN DESIGNATION

A.A.D.T.	2022	=	3,075
A.A.D.T.	2042	=	3,425
D.H.V.		=	405
D.D. (DAILY SPLIT)		=	50/50
T.		=	10.9%
DESIGN SPEED		=	45/55 MPH
ESALS		=	770,000

CONVENTIONAL SYMBOLS

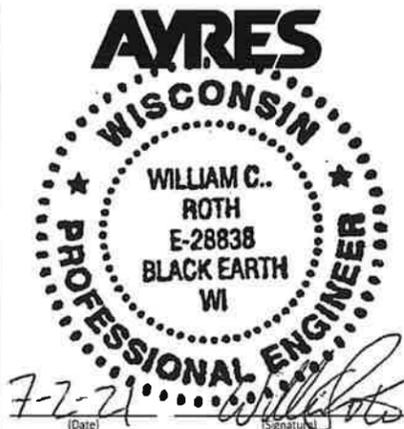
PLAN

CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE

GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

ORIGINAL PLANS PREPARED BY



STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

PREPARED BY	
Surveyor	SW REGION
Designer	AYRES ASSOCIATES
Project Manager	AMY COUGHLIN, P.E.
Regional Examiner	SW REGION
Regional Supervisor	ALEX HAGEN, P.E.

APPROVED FOR THE DEPARTMENT  
DATE: 7/06/2021  
*Amy Coughlin*  
(Signature)

E

**STANDARD ABBREVIATIONS**

A.D.T.	AVERAGE DAILY TRAFFIC
AECP	APRON ENDWALL FOR CULVERT PIPE
AECPRC	APRON ENDWALL FOR CULVERT PIPE REINFORCED CONCRETE
AECPS	APRON ENDWALL FOR CULVERT PIPE STEEL
AECPSAL	APRON ENDWALL FOR CULVERT PIPE SALVAGED
ATMS	ARTERIAL TRAFFIC MANAGEMENT SYSTEM
BM	BENCHMARK
BOC	BACK OF CURB
CBTP	CONCRETE BARRIER TEMPORARY PRECAST
CBSS	CONCRETE BARRIER SINGLE SLOPE
CL	CLASS
CONST	CONSTRUCTION
CP	CONTROL POINT
CPCS	CULVERT PIPE CORRUGATED STEEL
CPRC	CULVERT PIPE REINFORCED CONCRETE
D.D.	DIRECTIONAL DISTRIBUTION
D.H.V.	DESIGN HOURLY VOLUME
EB	EASTBOUND
EXIST	EXISTING
HMA	HOT MIX ASPHALT
H.S.	HIGH STRENGTH
ITS	INTELLIGENT TRAFFIC SYSTEM
MAX	MAXIMUM
MIN	MINIMUM
NB	NORTHBOUND
NOR	NORMAL
PC	POINT OF CURVATURE
PCC	POINT OF COMMON CURVATURE
PI	POINT OF INTERSECTION
PRC	POINT OF REVERSE CURVATURE
PT	POINT OF TANGENCY
R/L	REFERENCE LINE
REQ'D	REQUIRED
SB	SOUTHBOUND
SSPRC	STORM SEWER PIPE REINFORCED CONCRETE
T.	PERCENT TRUCKS
TYP	TYPICAL
VAR	VARIABLE
WB	WESTBOUND
WT.	WEIGHT

**GENERAL NOTES**

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS. THE CONTRACTOR SHALL COORDINATE HIS CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

ASPHALTIC SURFACE/HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN PER THE PAVEMENT REPORT.

CONSTRUCT THE PAVED SHOULDERS WITH THE SAME MIX TYPE AND THICKNESS AS THE MAINLINE.

APPLY TACK COAT BETWEEN THE MILLED SURFACE AND THE LAYER OF NEW HMA PAVEMENT AT A RATE OF 0.07 GAL/SY.

GRADING IS NOT ALLOWED IN WETLAND AREAS. DO NOT STORE EQUIPMENT OR MATERIAL IN ENVIRONMENTALLY SENSITIVE AREAS, WETLANDS OR WATERWAYS.

**ORDER OF SECTION 2 SHEETS**

- GENERAL NOTES
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- TRAFFIC CONTROL AND CONSTRUCTION STAGING

**UTILITY CONTACTS**

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 ATC Management Inc. - Electricity-Transmission  
 2489 Rinden Road  
 Cottage Grove, WI 53527  
 P: (608) 877-7650  
 dvosberg@atcllc.com

Neal Goodchild  
 Alliant Energy - Electricity  
 520 Commerce Avenue  
 Baraboo, WI 53913  
 P: (608) 356-0643  
 nealgoodchild@alliantenergy.com

Neal Goodchild  
 Alliant Energy - Gas/Petroleum  
 520 Commerce Avenue  
 Baraboo, WI 53913  
 P: (608) 356-0643  
 nealgoodchild@alliantenergy.com

Steve Bishop  
 CenturyLink - Communication Line  
 130 4th Street  
 Baraboo, WI 53913  
 P: (608) 355-7501  
 steven.bishop@lumen.com

Lenny Klaas  
 Northern Natural Gas Company - Gas/Petroleum  
 5557 County Highway D  
 Platteville, WI 53818  
 P: (608) 778-8514  
 Leonard.Klaas@nngco.com

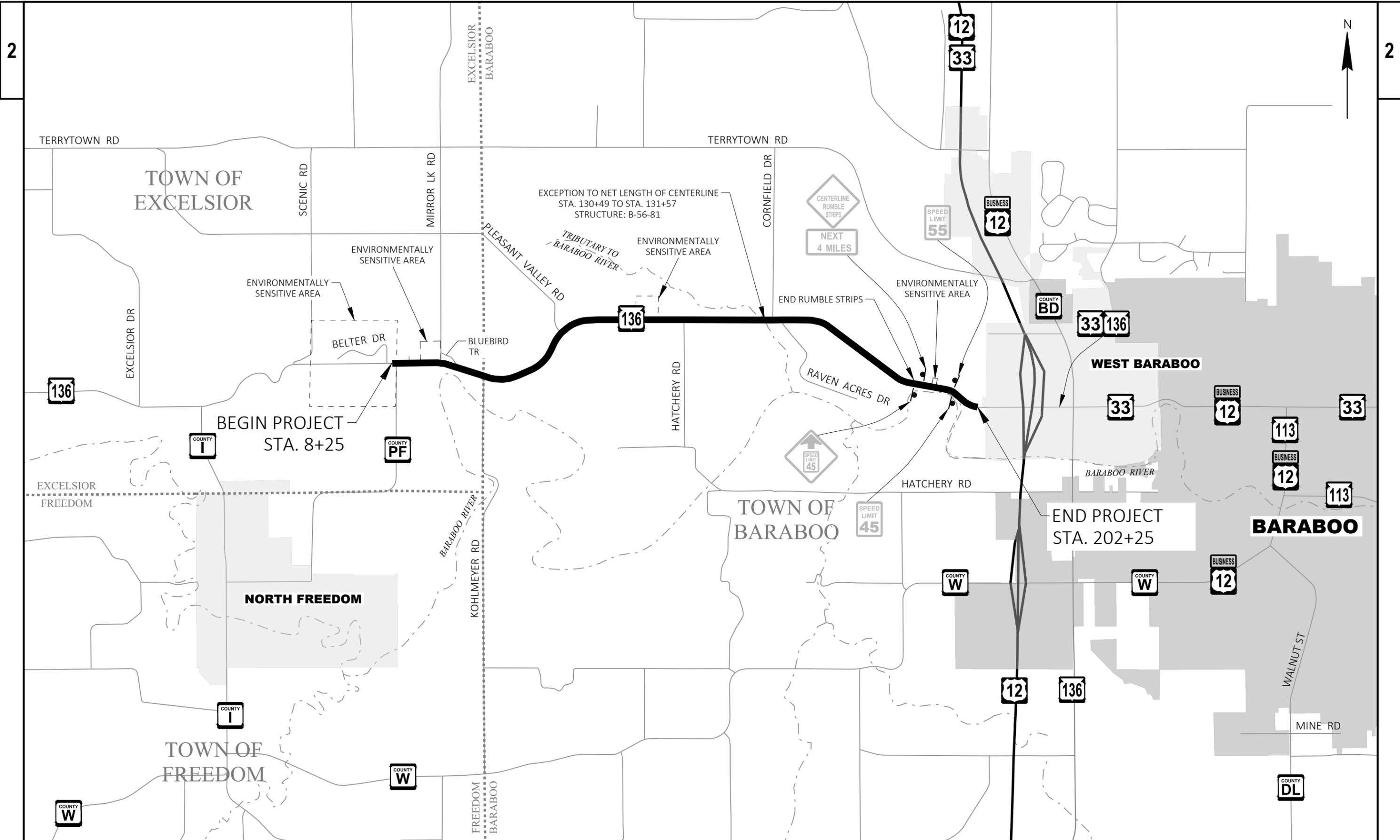
**OTHER CONTACTS**

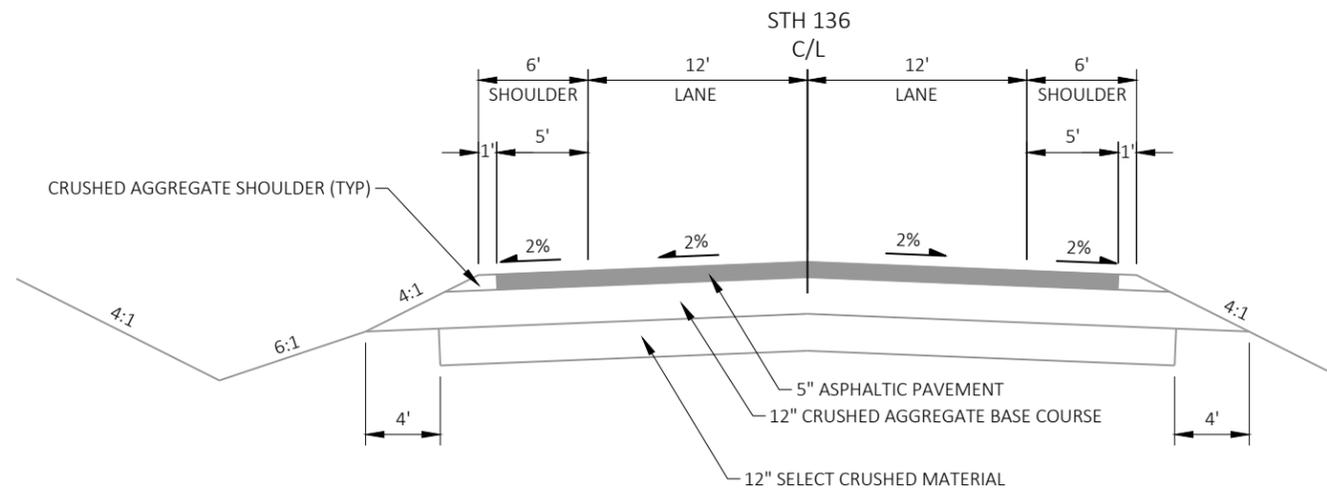
Amy Coughlin, P.E.  
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 2101 Wright Street  
 Madison, WI 53704  
 (608) 245-5358  
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 Madison, WI 53718  
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 rothw@ayresassociates.com

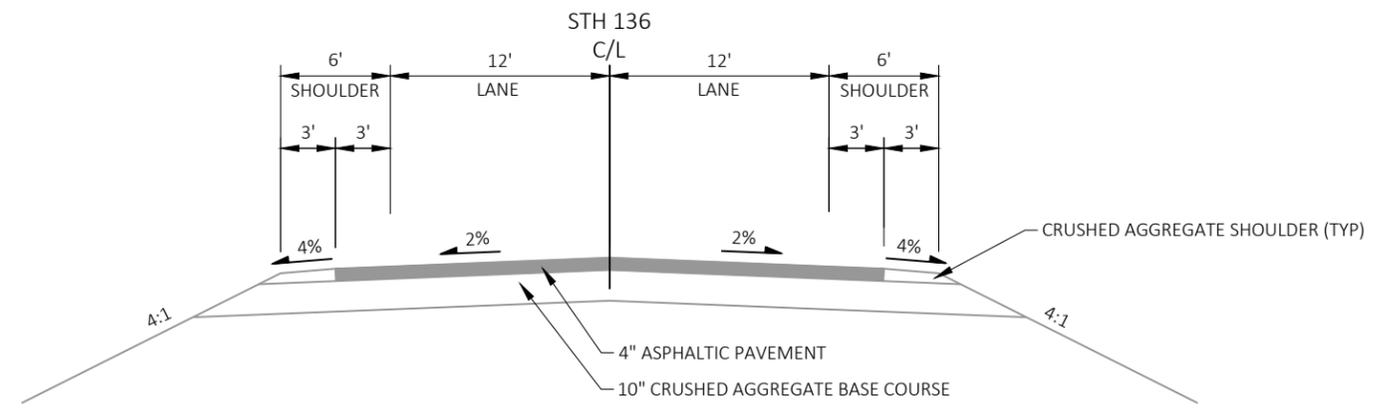
Andy Barta  
 Wisconsin DNR Liaison  
 3911 Fish Hatchery Rd  
 Fitchburg, WI 53711  
 (608) 235-2955  
 Andrew.Barta@wisconsin.gov

**DIGGERS HOTLINE**  
 Dial 311 or (800)242-8511  
 www.DiggersHotline.com



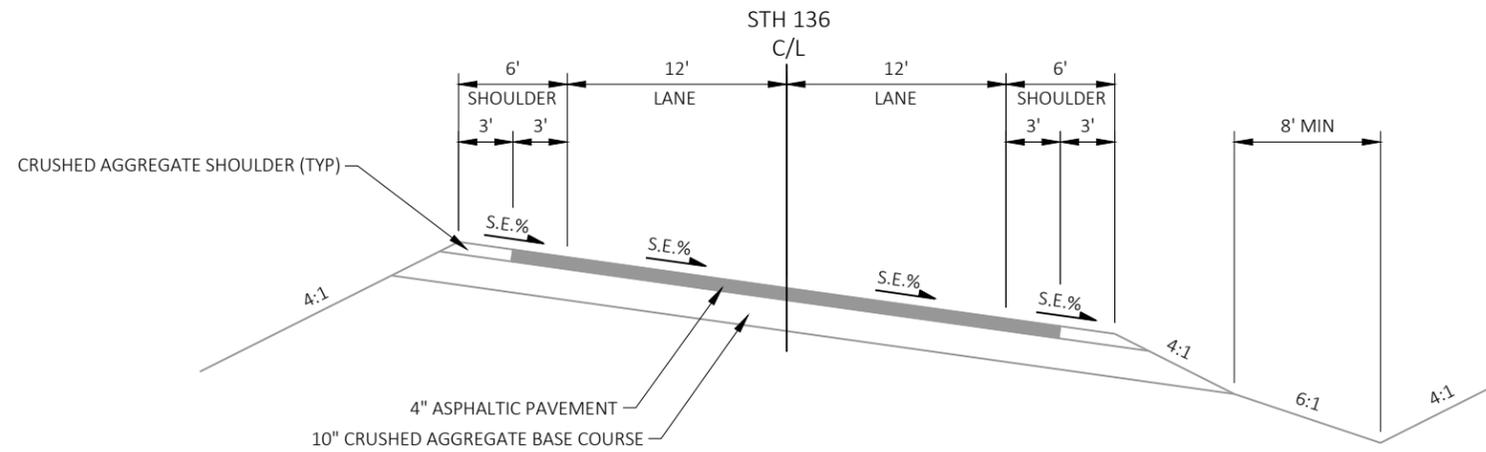


**TYPICAL EXISTING SECTION - STH 136**  
STA. 6+00 TO STA. 8+25



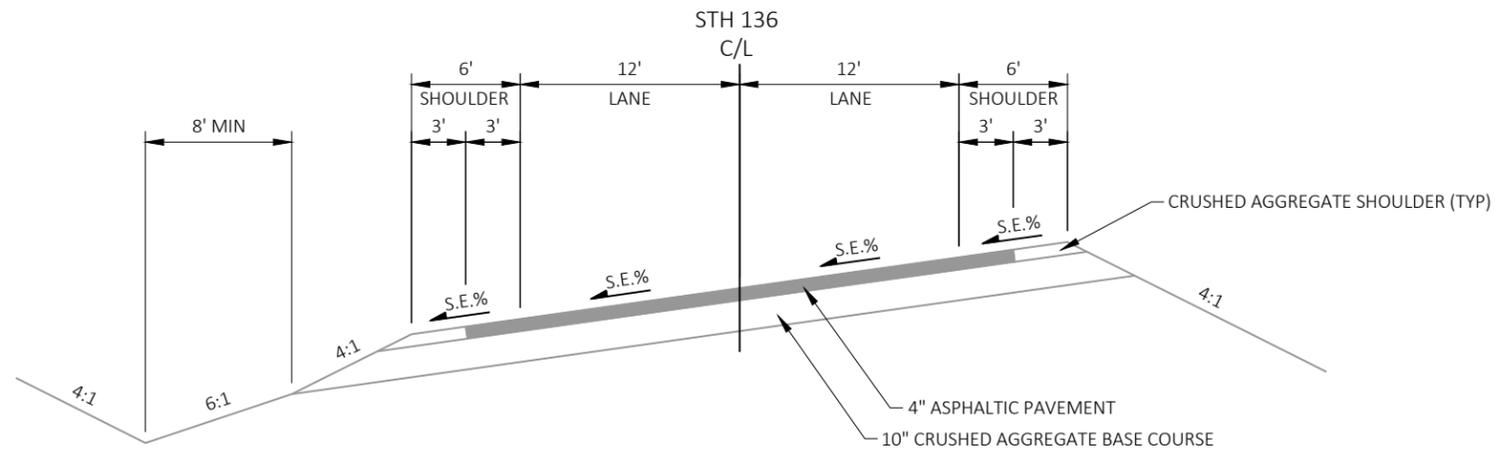
**TYPICAL EXISTING SECTION - STH 136**

- STA. 8+25 - STA. 22+02
- STA. 26+25 - STA. 38+69
- STA. 47+82 - STA. 52+22
- STA. 61+55 - STA. 63+48
- STA. 77+80 - STA. 144+00
- STA. 151+62 - STA. 176+69
- STA. 181+10 - STA. 192+49



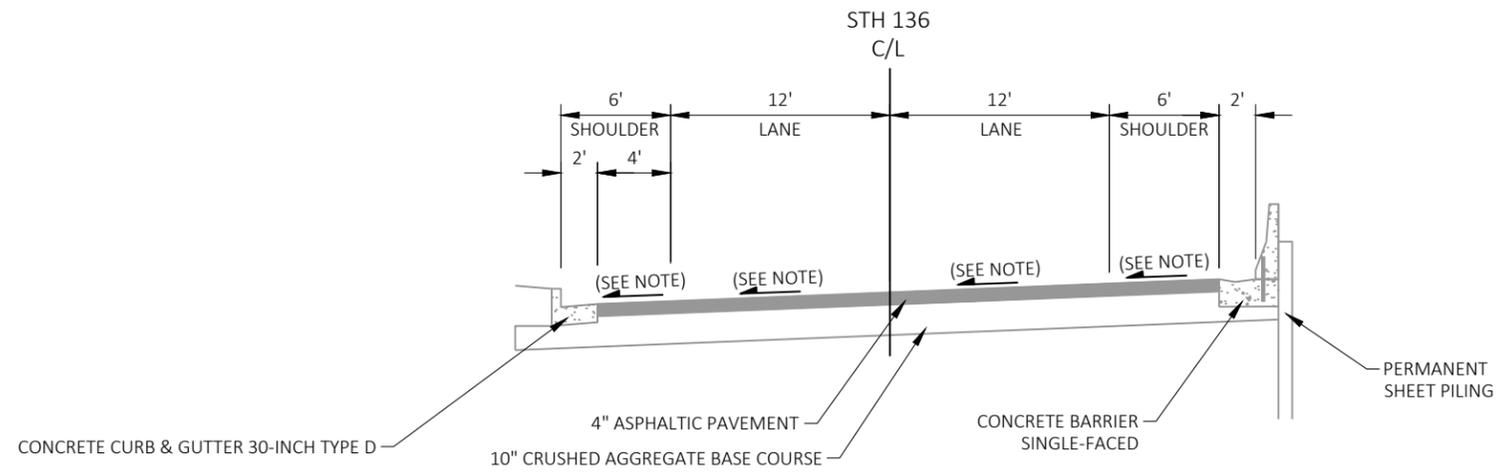
TYPICAL EXISTING SUPERELEVATED SECTION - STH 136

STA. - STA.	S.E. %
22+02 - 26+25	7.9%
63+48 - 77+80	7.9%
144+00 - 151+62	8.0%
192+49 - 196+15	7.45%



TYPICAL EXISTING SUPERELEVATED SECTION - STH 136

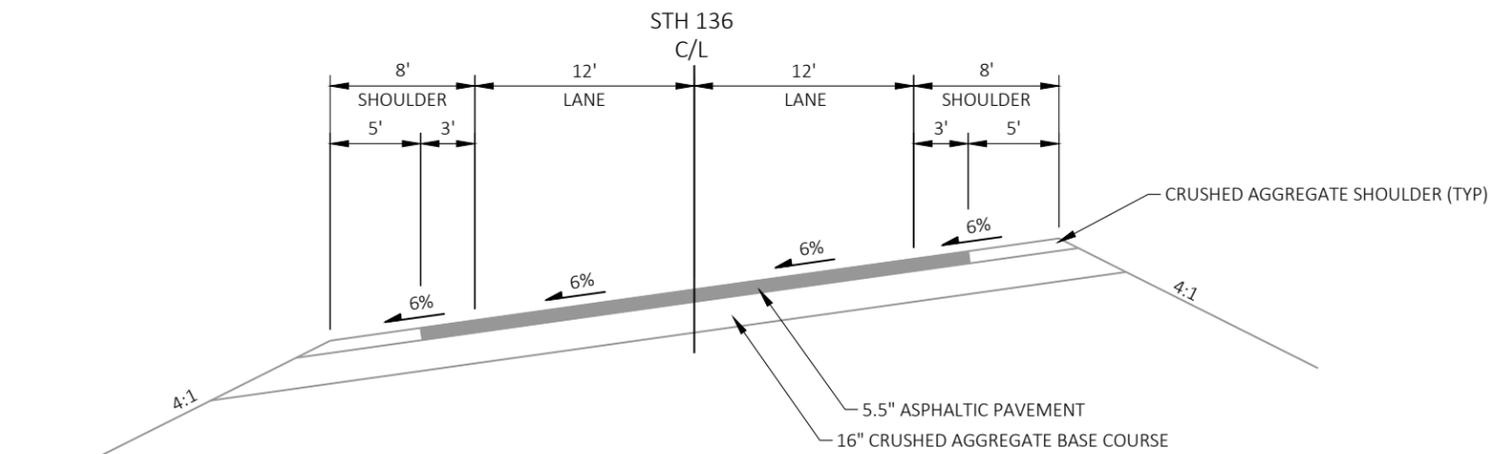
STA. - STA.	S.E. %
38+69 - 47+82	7.9%
52+22 - 61+55	7.9%
176+69 - 181+10	6.0%
200+25 - 202+25	6.0%



TYPICAL EXISTING SECTION - STH 136

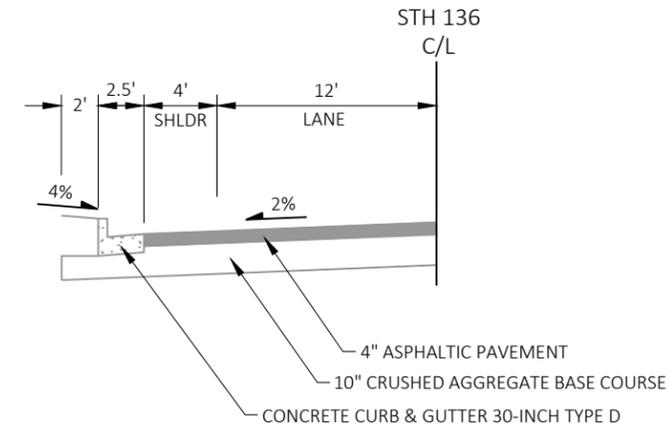
STA. 196+15 - STA. 200+25

NOTE: VARYING PAVEMENT SLOPE



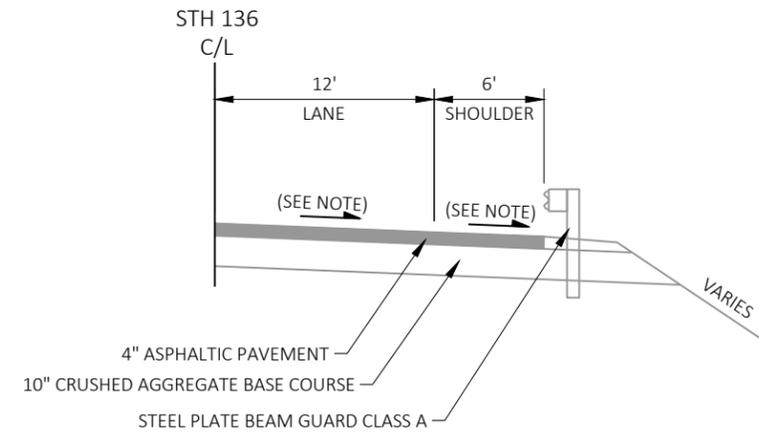
TYPICAL EXISTING SUPERELEVATED SECTION - STH 136

STA. 202+25 - STA. 204+88



TYPICAL EXISTING HALF-SECTION - STH 136

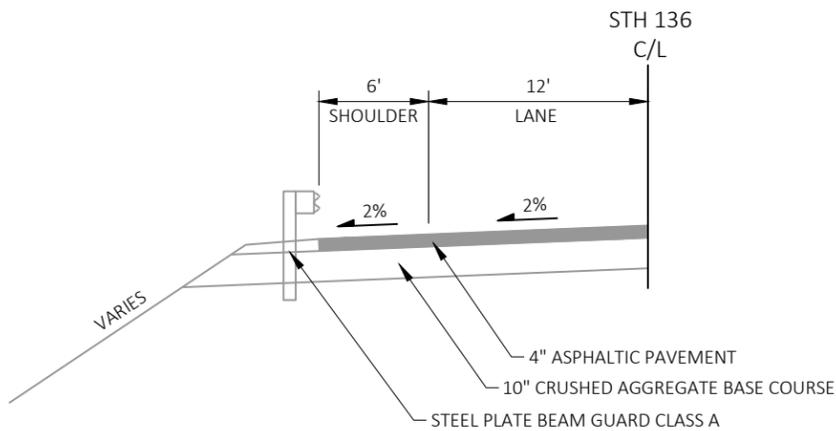
STA. 8+29 - STA. 10+60 LT  
STA. 78+90 - STA. 83+30 LT



TYPICAL EXISTING HALF-SECTION - STH 136

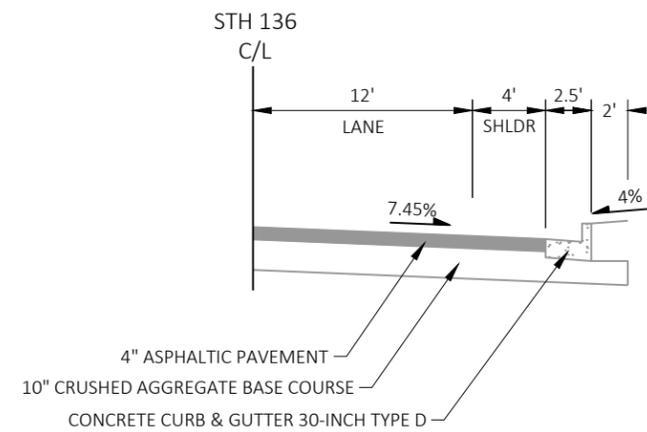
STA. 27+10 - STA. 30+60 RT  
STA. 129+00 - STA. 130+49 RT  
STA. 131+57 - STA. 132+70 RT  
STA. 145+70 - STA. 148+82 RT  
STA. 161+15 - STA. 163+90 RT  
STA. 188+90 - STA. 196+15 RT  
STA. 198+90 - STA. 199+90 RT

NOTE: VARYING PAVEMENT SLOPE  
6% TO 8%  
2% TYP



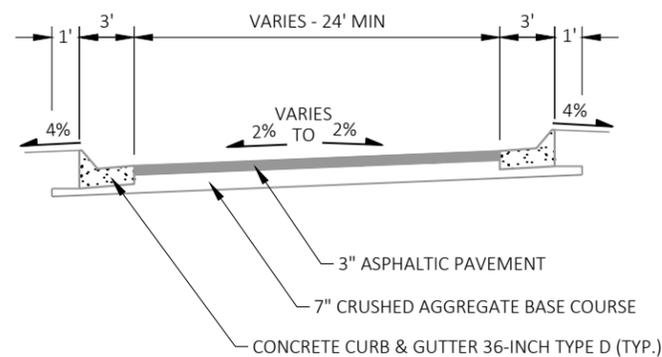
TYPICAL EXISTING HALF-SECTION - STH 136

STA. 128+90 - STA. 130+49 LT  
STA. 131+57 - STA. 132+30 LT  
STA. 161+35 - STA. 164+23 LT

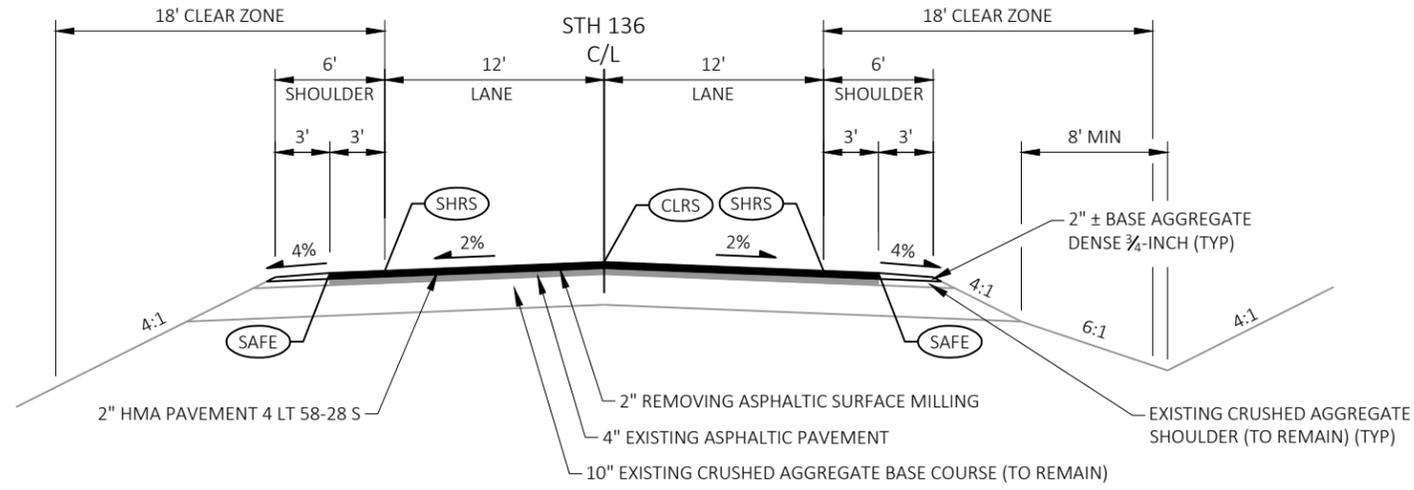


TYPICAL EXISTING HALF-SECTION - STH 136

STA. 192+05 - STA. 196+15 RT

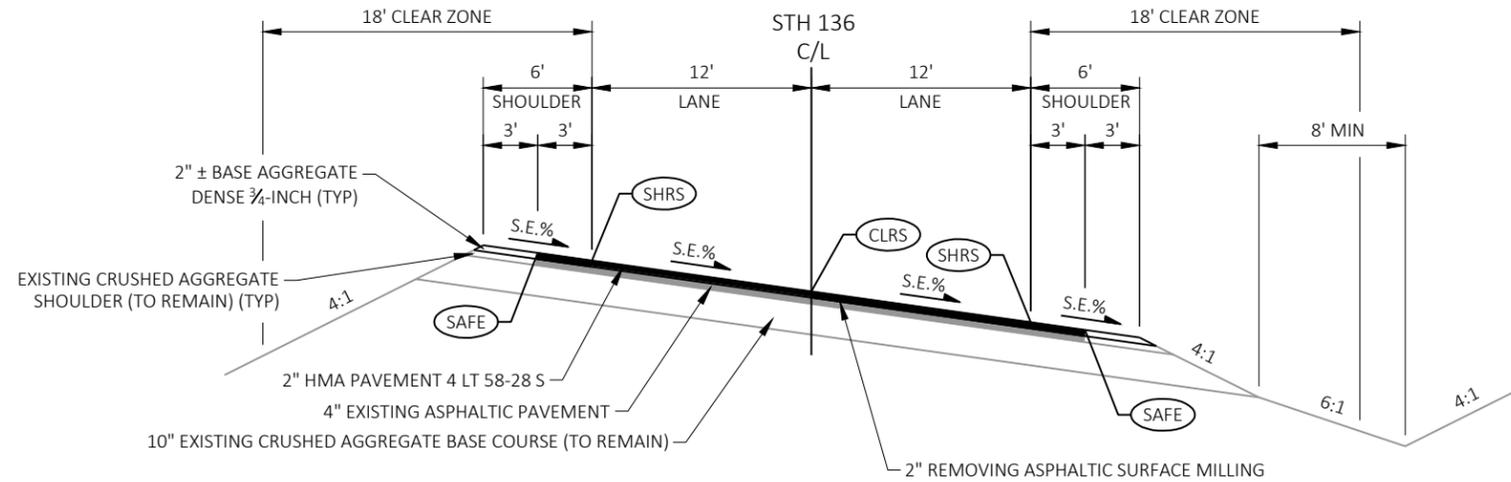


TYPICAL EXISTING SECTION - SIDEROADS



TYPICAL FINISHED SECTION - STH 136

STA. 8+25 - STA. 22+02  
 STA. 26+25 - STA. 38+69  
 STA. 47+82 - STA. 52+22  
 STA. 61+55 - STA. 63+48  
 STA. 77+80 - STA. 144+00  
 STA. 151+62 - STA. 176+69  
 STA. 181+10 - STA. 192+49



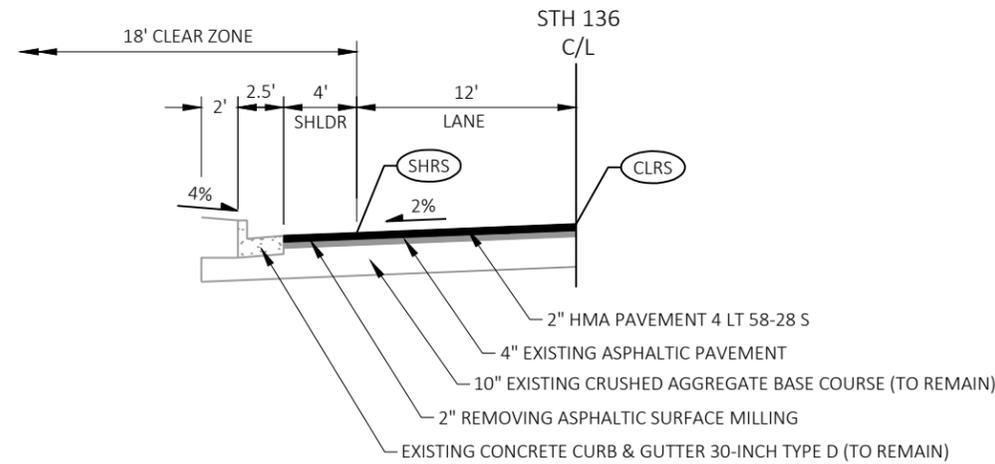
TYPICAL FINISHED SUPERELEVATED SECTION - STH 136

STA. - STA.	S.E.%
22+02 - 26+25	7.9%
63+48 - 77+80	7.9%
144+00 - 151+62	8.0%
192+49 - 196+15	7.45%

**LEGEND:**

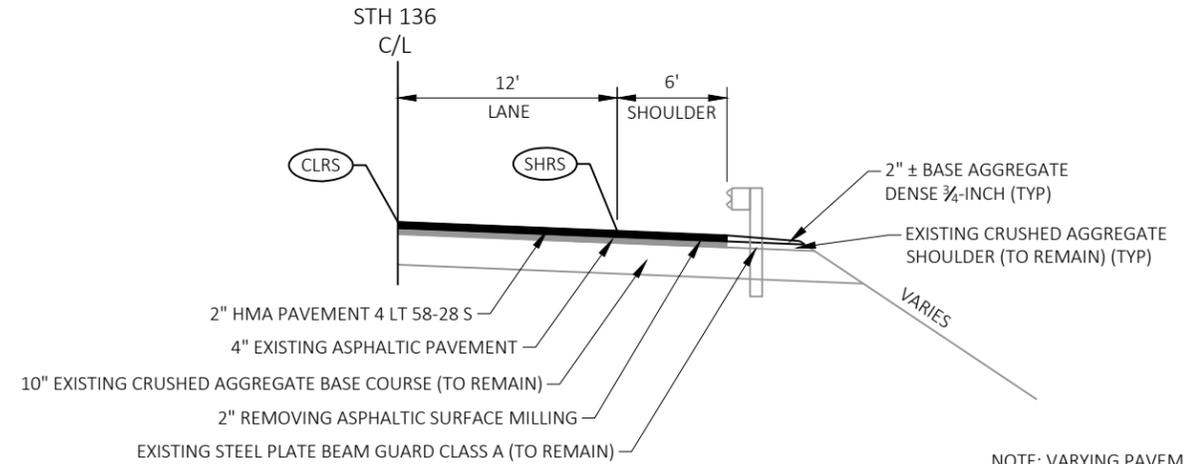
- SAFETY EDGE
- ASPHALTIC CENTERLINE RUMBLE STRIP 2-LANE RURAL
- ASPHALTIC SHOULDER RUMBLE STRIP 2-LANE RURAL (TYPE 2)





TYPICAL FINISHED HALF-SECTION - STH 136

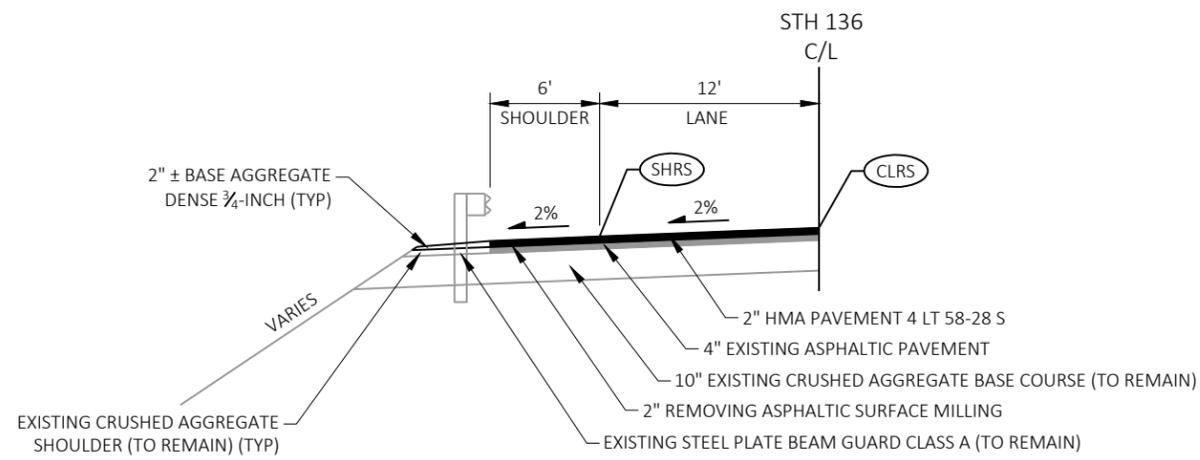
STA. 8+29 - STA. 10+60 LT  
STA. 78+90 - STA. 83+30 LT



TYPICAL FINISHED HALF-SECTION - STH 136

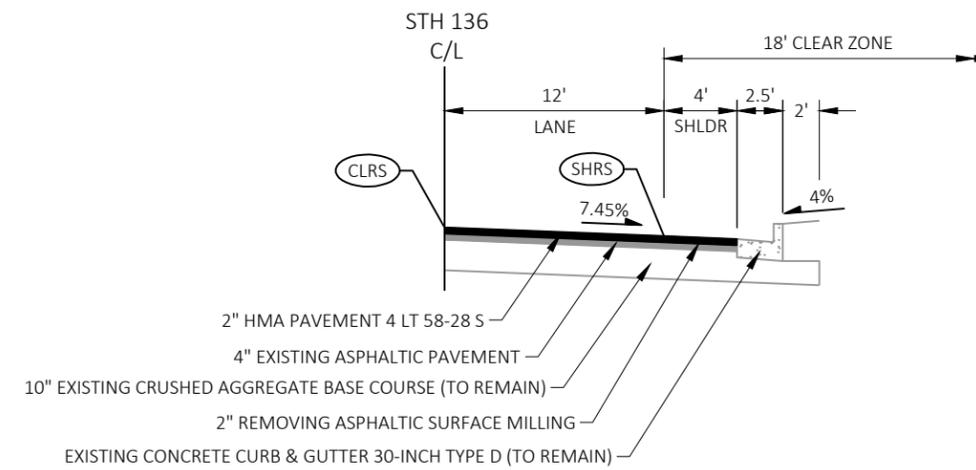
STA. 27+10 - STA. 30+60 RT  
STA. 129+00 - STA. 130+49 RT  
STA. 131+57 - STA. 132+70 RT  
STA. 145+70 - STA. 148+82 RT  
STA. 161+15 - STA. 163+90 RT  
STA. 188+90 - STA. 196+15 RT  
STA. 198+90 - STA. 199+90 RT

NOTE: VARYING PAVEMENT SLOPE  
6% TO 8%  
2% TYP



TYPICAL FINISHED HALF-SECTION - STH 136

STA. 128+90 - STA. 130+49 LT  
STA. 131+57 - STA. 132+30 LT  
STA. 161+35 - STA. 164+23 LT

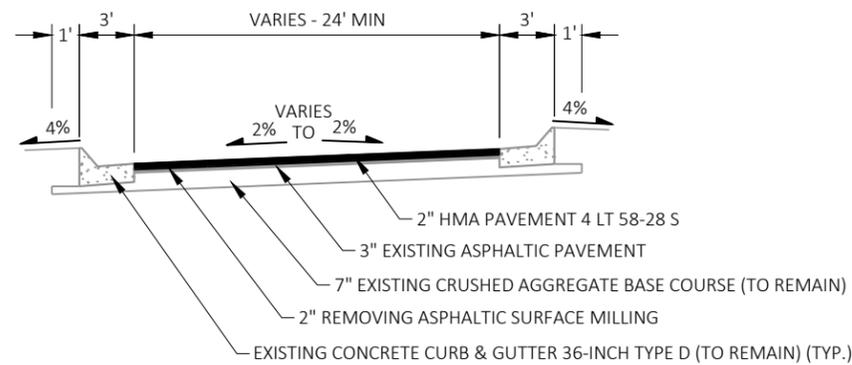


TYPICAL FINISHED HALF-SECTION - STH 136

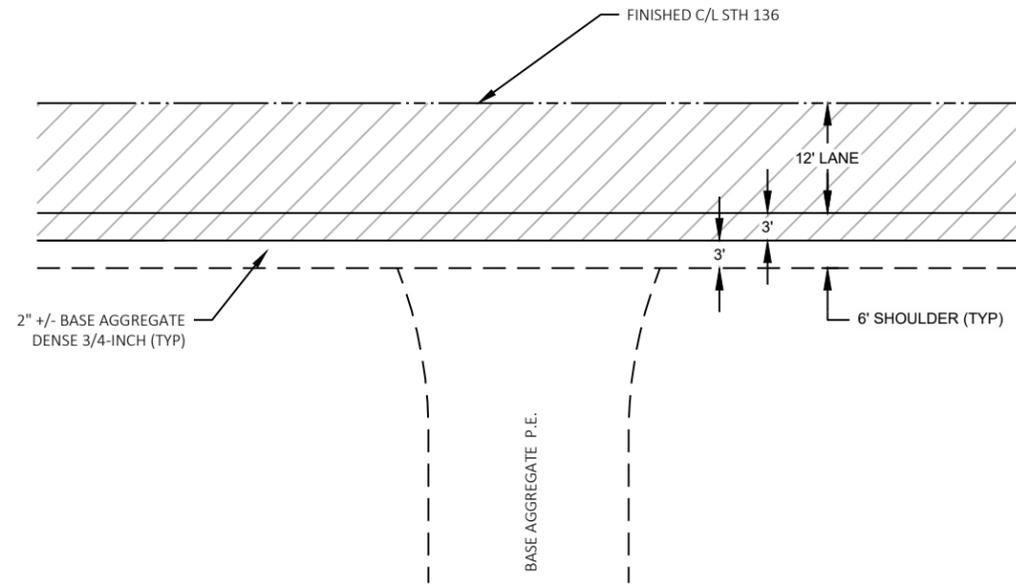
STA. 192+05 - STA. 196+15 RT

LEGEND:

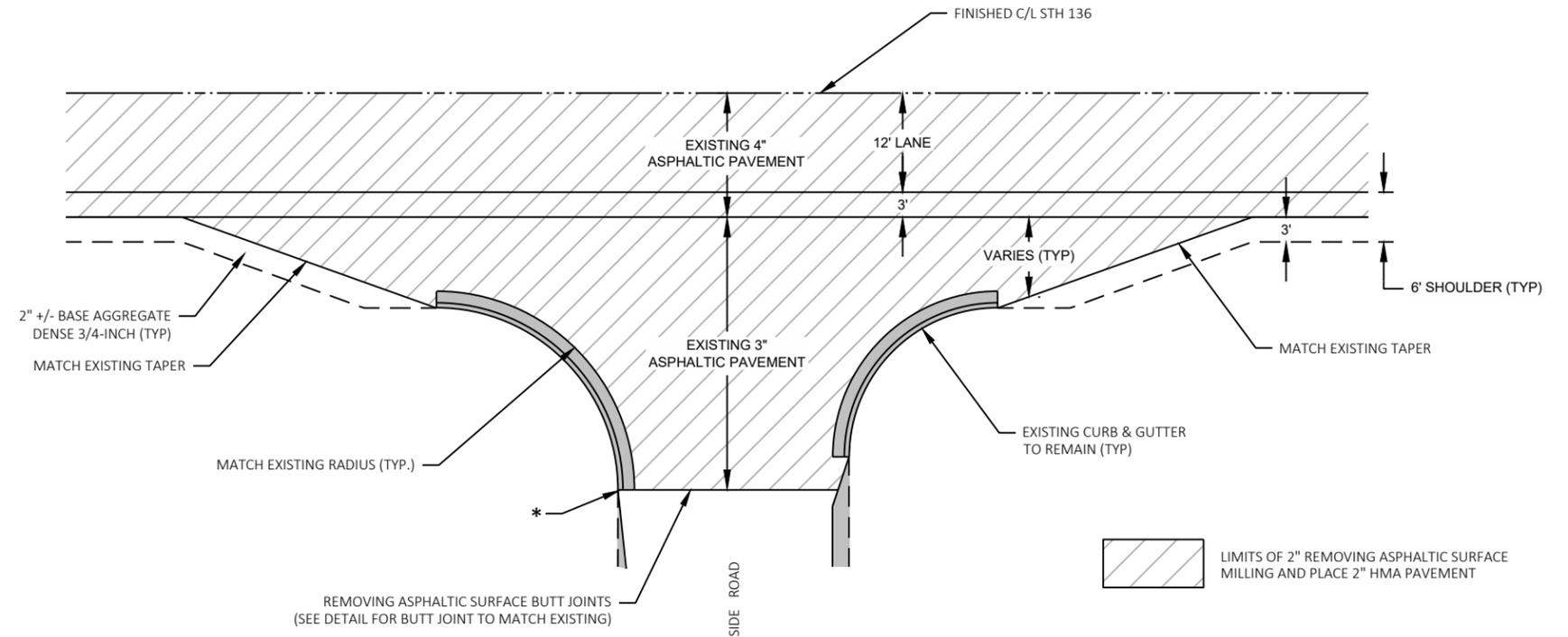
SAFE	SAFETY EDGE
CLRS	ASPHALTIC CENTERLINE RUMBLE STRIP 2-LANE RURAL
SHRS	ASPHALTIC SHOULDER RUMBLE STRIP 2-LANE RURAL (TYPE 2)



TYPICAL FINISHED SECTION - SIDEROADS

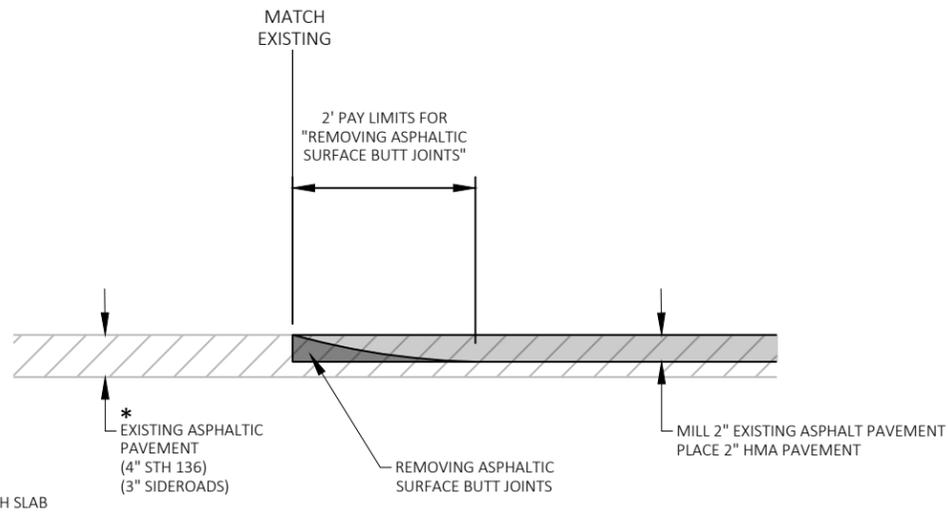


BASE AGGREGATE ENTRANCES

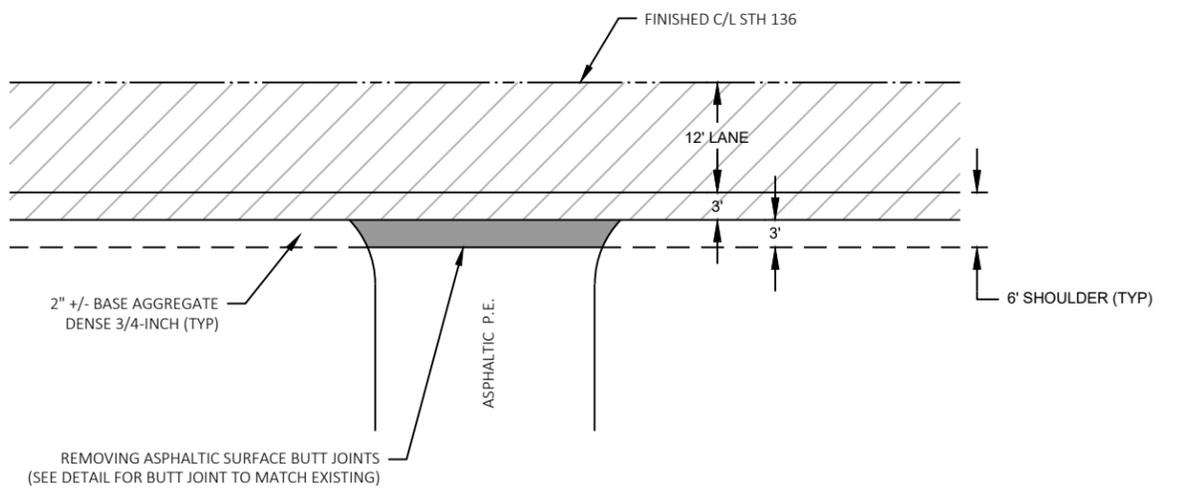


TYPICAL SIDE ROAD PAVING LIMITS

\* MILL AND OVERLAY TO RADIUS POINT FARTHEST FROM MAINLINE STH 136

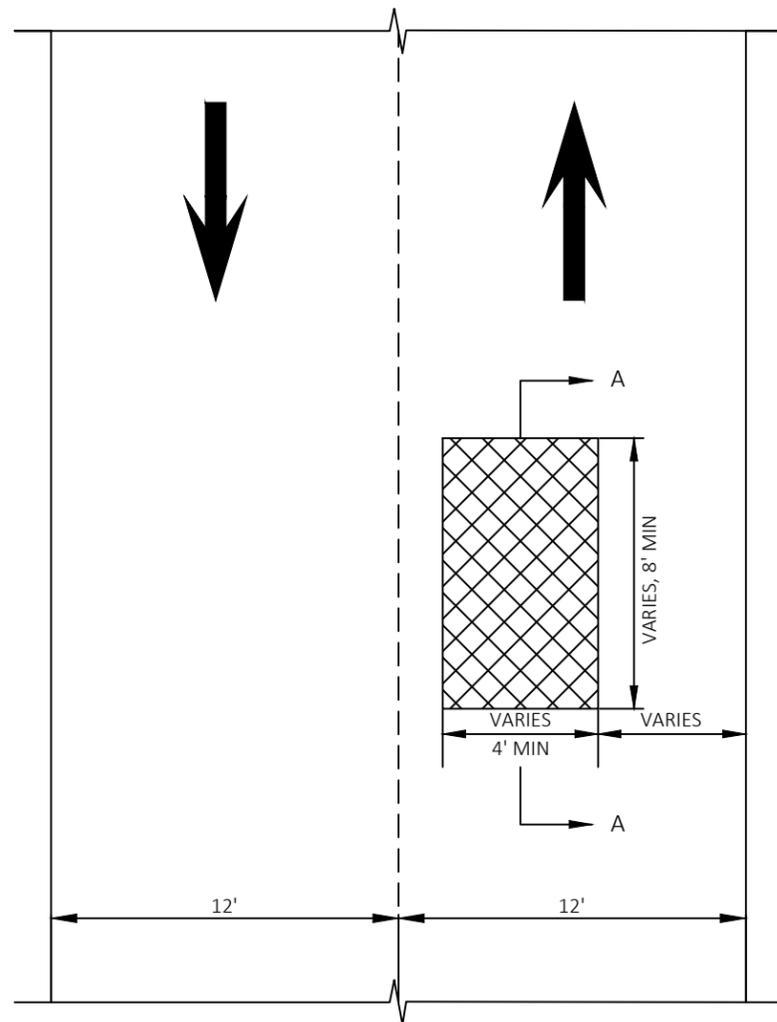


BUTT JOINT TO MATCH EXISTING - STH 136, SIDE ROADS, AND ASPHALTIC ENTRANCES



ASPHALTIC ENTRANCES

- LIMITS OF 2" REMOVING ASPHALTIC SURFACE MILLING, AND PLACE 2" HMA PAVEMENT.
- LIMITS OF 2" REMOVING ASPHALTIC SURFACE MILLING, AND PLACE 2" ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES.

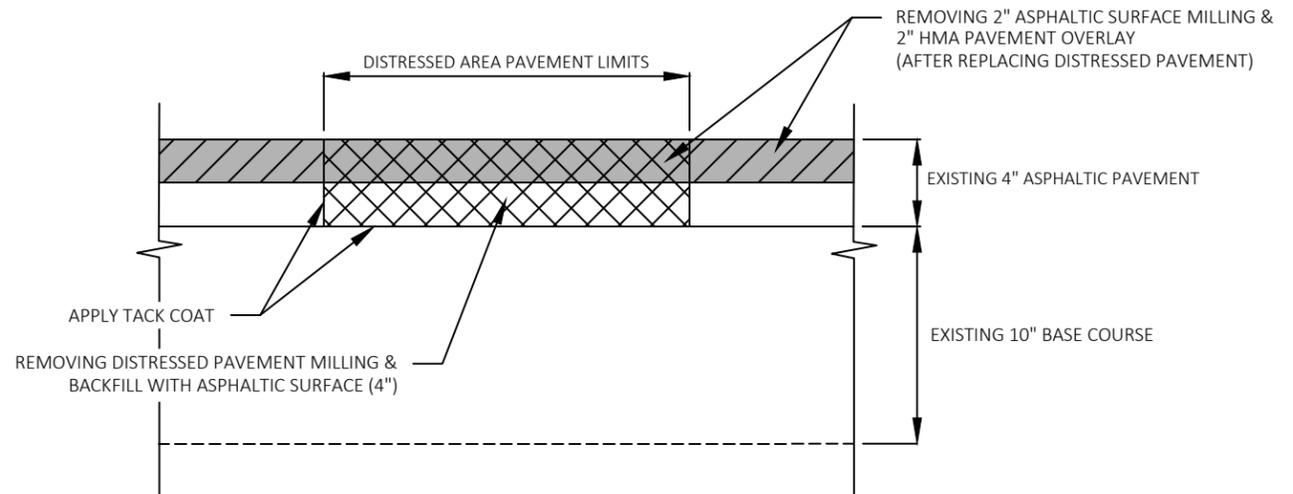


REMOVING DISTRESSED PAVEMENT MILLING



TRAVEL DIRECTION

PLAN VIEW



HMA PAVEMENT OVERLAY



REMOVING DISTRESSED PAVEMENT MILLING & BACKFILL WITH ASPHALTIC SURFACE (4")

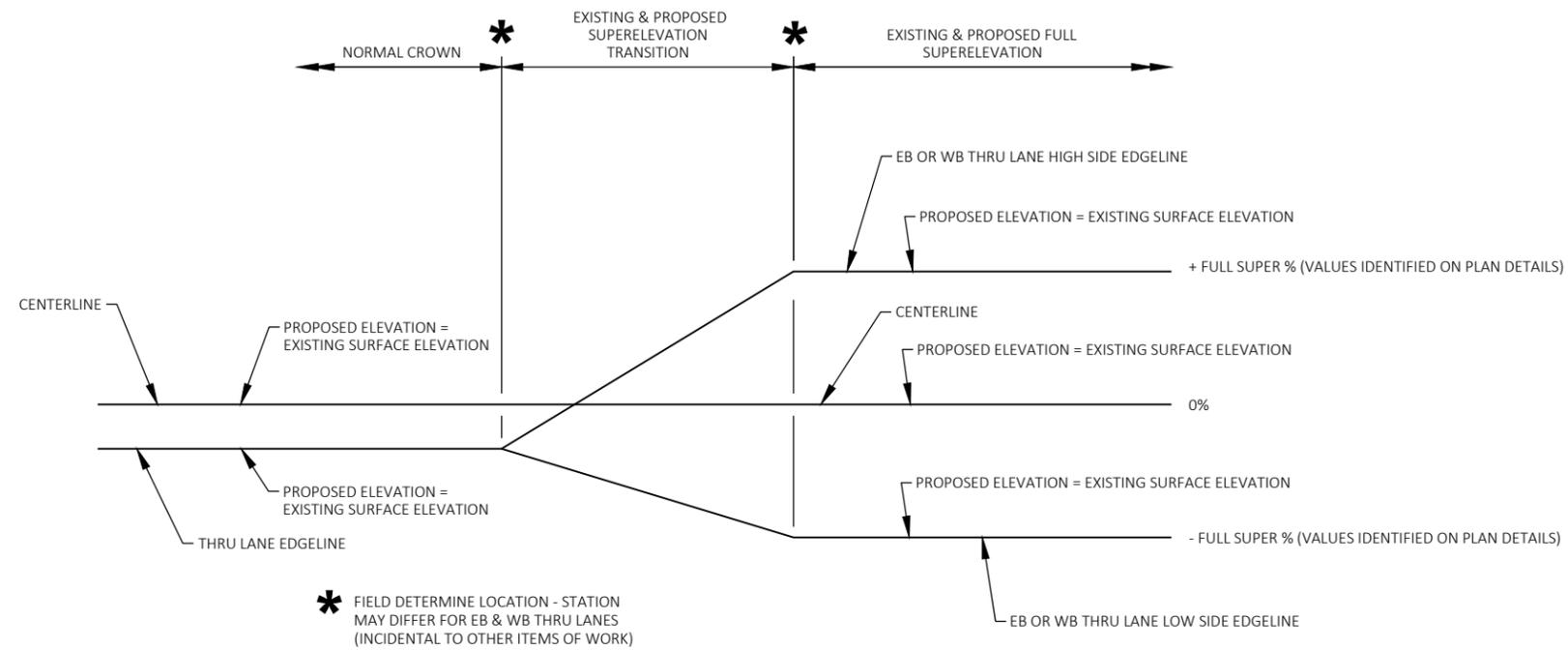


REMOVING 2" ASPHALTIC SURFACE MILLING & 2" PAVEMENT OVERLAY (AFTER REPLACING DISTRESSED PAVEMENT)

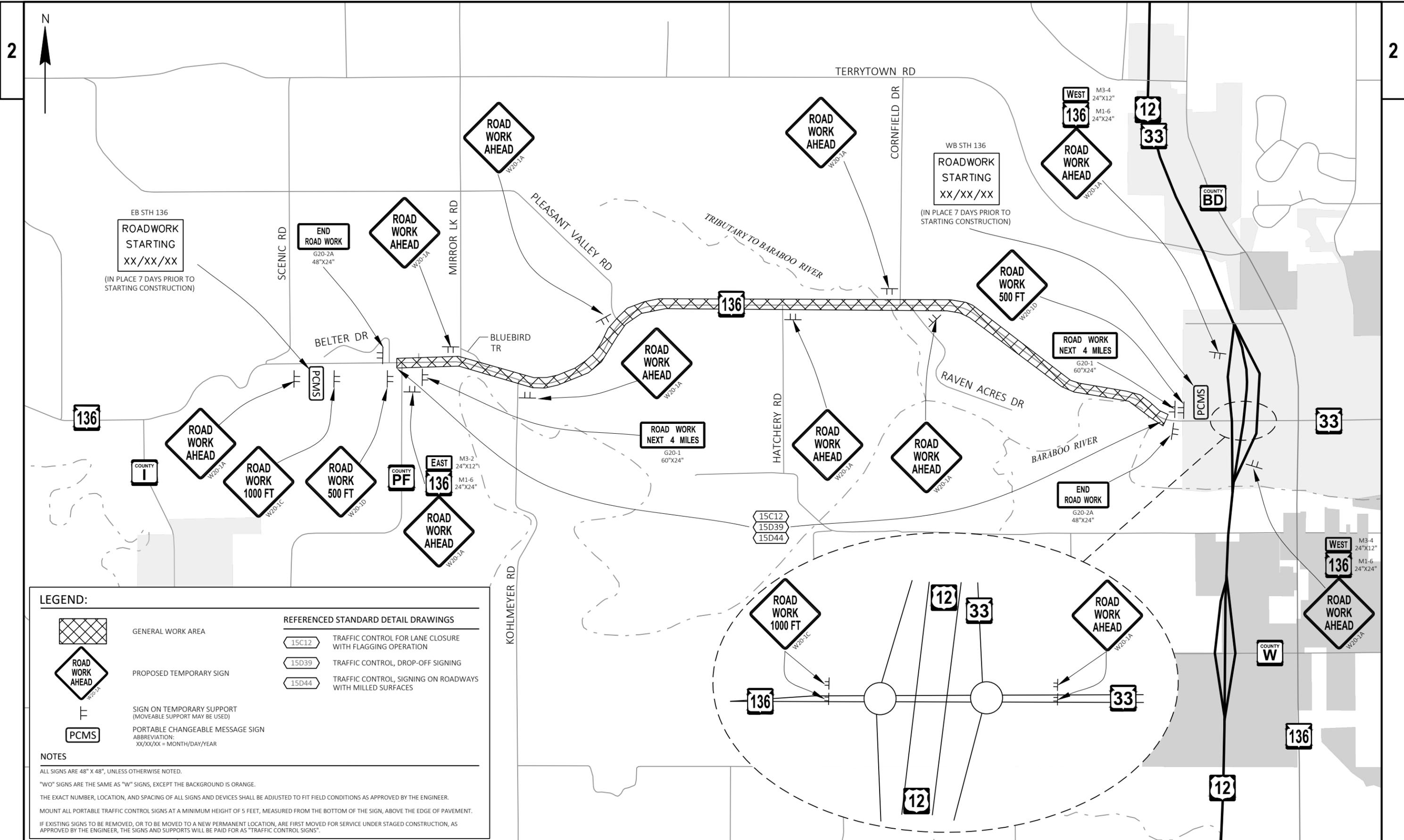
SECTION A-A

**REMOVING DISTRESSED PAVEMENT MILLING**

EXACT LOCATION AND LIMITS OF REMOVING DISTRESSED PAVEMENT MILLING TO BE DETERMINED BY THE ENGINEER IN THE FIELD



DETAIL FOR TYPICAL SUPERELEVATION TRANSITION  
 PROFILE VIEW



PROJECT NO: 5573-03-73

HWY: STH 136

COUNTY: SAUK

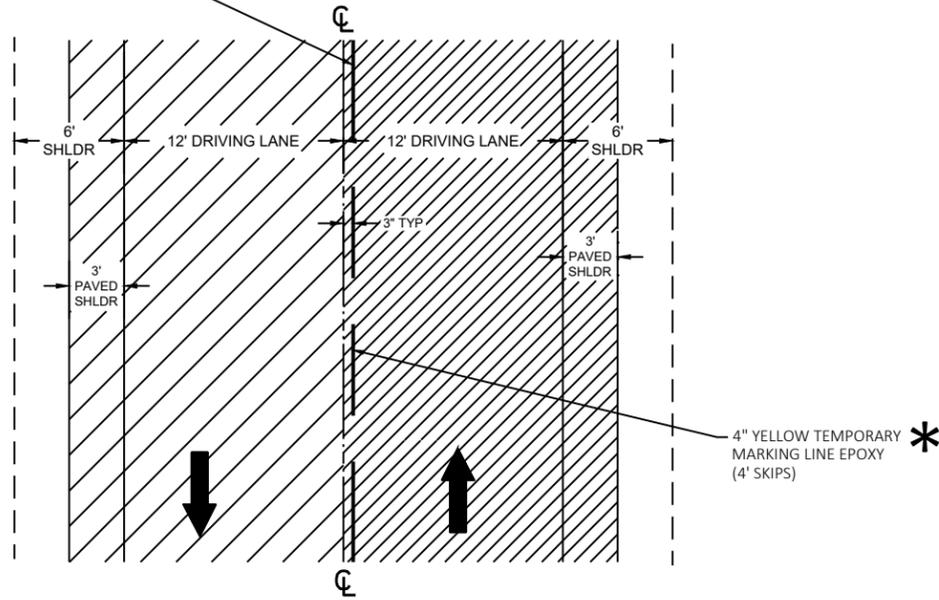
TRAFFIC CONTROL - WORK ZONE OVERVIEW

SHEET

E

\* 4" YELLOW TEMPORARY MARKING LINE EPOXY 4' SKIPS. WHERE EXISTING SIGNS ARE NOT IN PLACE, SUPPLEMENT WITH "NO PASSING ZONE" SIGNS (R4-1, 24"x30"), INSTALLED AT THE BEGINNING OF NO PASSING ZONES, AND WITHIN NO PASSING ZONES AT ONE MILE INTERVALS AND AFTER STATE AND COUNTY TRUNK HIGHWAY INTERSECTIONS.

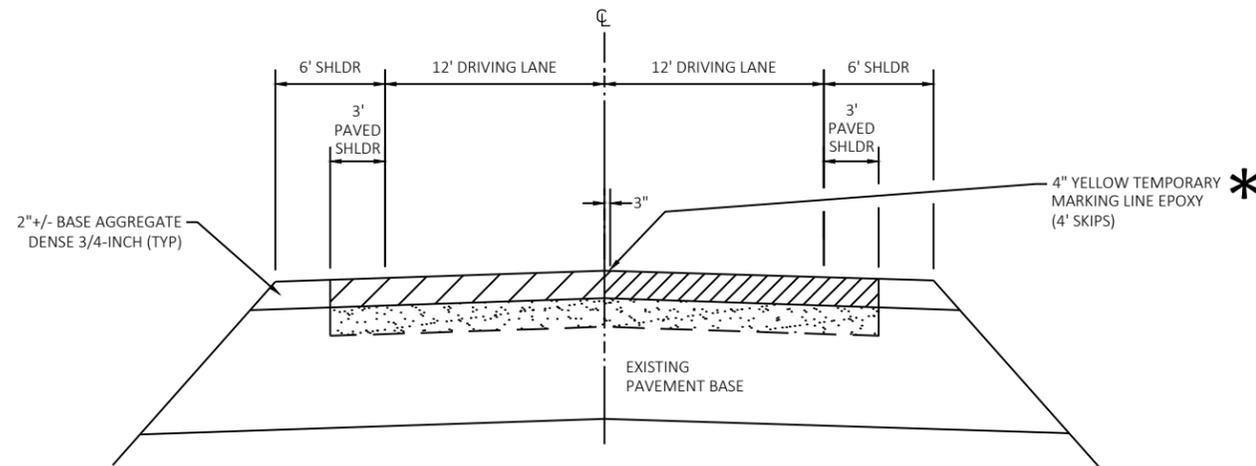
PERMANENT 4" YELLOW MARKING LINE SAME DAY EPOXY TO BE PLACED AFTER CENTERLINE RUMBLE STRIPS ARE INSTALLED.



PLAN VIEW

**NOTE:**  
\* = PLACE TEMPORARY YELLOW MARKING LINE EPOXY 4' SKIPS IN LOCATION OF PERMANENT YELLOW MARKING LINE (SEE SDD LONGITUDINAL MARKING (MAINLINE))

-  SECOND LANE (WB OR EB) PAVED WITH 2" HMA PAVEMENT 4 LT 58-28 S
-  EXISTING 2" ASPHALT TO REMAIN (BELOW 2" MILL)
-  FIRST LANE (EB OR WB) PAVED WITH 2" HMA PAVEMENT 4 LT 58-28 S



SECTION VIEW

MILLING, HMA PAVING, AND TEMPORARY MARKING LINE DETAIL

Estimate Of Quantities

5573-03-73

Line	Item	Item Description	Unit	Total	Qty
0002	204.0115	Removing Asphaltic Surface Butt Joints	SY	131.000	131.000
0004	204.0120	Removing Asphaltic Surface Milling	SY	69,110.000	69,110.000
0006	211.0100	Prepare Foundation for Asphaltic Paving (project) 01. 5573-03-73	LS	1.000	1.000
0008	213.0100	Finishing Roadway (project) 01. 5573-03-73	EACH	1.000	1.000
0010	305.0110	Base Aggregate Dense 3/4-Inch	TON	1,540.000	1,540.000
0012	455.0605	Tack Coat	GAL	5,311.000	5,311.000
0014	460.2000	Incentive Density HMA Pavement	DOL	4,890.000	4,890.000
0016	460.5224	HMA Pavement 4 LT 58-28 S	TON	7,635.000	7,635.000
0018	465.0105	Asphaltic Surface	TON	763.000	763.000
0020	465.0110	Asphaltic Surface Patching	TON	100.000	100.000
0022	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	96.000	96.000
0024	465.0425	Asphaltic Shoulder Rumble Strips 2-Lane Rural	LF	31,865.000	31,865.000
0026	465.0475	Asphalt Centerline Rumble Strips 2-Lane Rural	LF	15,593.000	15,593.000
0028	618.0100	Maintenance And Repair of Haul Roads (project) 01. 5573-03-73	EACH	1.000	1.000
0030	619.1000	Mobilization	EACH	1.000	1.000
0032	624.0100	Water	MGAL	15.000	15.000
0034	628.1905	Mobilizations Erosion Control	EACH	1.000	1.000
0036	628.1910	Mobilizations Emergency Erosion Control	EACH	1.000	1.000
0038	628.7015	Inlet Protection Type C	EACH	6.000	6.000
0040	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	2.000	2.000
0042	638.2102	Moving Signs Type II	EACH	2.000	2.000
0044	638.3000	Removing Small Sign Supports	EACH	2.000	2.000
0046	642.5201	Field Office Type C	EACH	1.000	1.000
0048	643.0900	Traffic Control Signs	DAY	800.000	800.000
0050	643.1050	Traffic Control Signs PCMS	DAY	14.000	14.000
0052	643.5000	Traffic Control	EACH	1.000	1.000
0054	646.1020	Marking Line Epoxy 4-Inch	LF	37,644.000	37,644.000
0056	646.4520	Marking Line Same Day Epoxy 4-Inch	LF	26,178.000	26,178.000
0058	648.0100	Locating No-Passing Zones	MI	3.650	3.650
0060	649.0120	Temporary Marking Line Epoxy 4-Inch	LF	1,550.000	1,550.000
0062	650.8000	Construction Staking Resurfacing Reference	LF	19,292.000	19,292.000
0064	650.9910	Construction Staking Supplemental Control (project) 01. 5573-03-73	LS	1.000	1.000
0066	740.0440	Incentive IRI Ride	DOL	14,600.000	14,600.000
0068	SPV.0060	Special 01. Landmark Reference Monuments Special	EACH	2.000	2.000
0070	SPV.0060	Special 02. Verify Landmark Reference Monuments	EACH	12.000	12.000
0072	SPV.0180	Special 01. Removing Distressed Pavement Milling	SY	3,510.000	3,510.000

**REMOVING ASPHALTIC SURFACE BUTT JOINTS**

204.0115  
REMOVING  
ASPHALTIC SURFACE  
BUTT JOINTS

CATEGORY	STATION	TO	STATION	LOCATION	SY	REMARKS
0010	8+25	-	8+27	CL	8	STH 136
0010		-	19+20	LT	6	PRIVATE ENTRANCE
0010		-	108+15	LT	15	PRIVATE ENTRANCE
0010		-	113+80	RT	6	PRIVATE ENTRANCE
0010	130+47	-	130+49	CL	8	STH 136
0010	131+57	-	131+59	CL	8	STH 136
0010		-	142+68	RT	6	PRIVATE ENTRANCE
0010		-	153+19	LT	8	PRIVATE ENTRANCE
0010		-	171+89	RT	6	PRIVATE ENTRANCE
0010		-	178+72	RT	7	PRIVATE ENTRANCE
0010		-	183+70	LT	7	PRIVATE ENTRANCE
0010	202+23	-	202+25	CL	7	STH 136
0010	9+94	-	10+25	RT	7	CTH PF
0010	23+61	-	23+80	LT	5	MIRROR LAKE RD
0010	37+77	-	38+01	RT	5	KOHLMEYER RD
0010	67+47	-	67+70	LT	5	PLEASANT VALLEY RD
0010	106+28	-	106+52	RT	6	HATCHERY RD
0010	133+22	-	133+44	LT	5	CORNFIELD DR
0010	136+17	-	136+43	RT	6	RAVEN ACRES DR

TOTAL 0010 131

**MILLING AND DISTRESSED PAVEMENT ITEMS**

CATEGORY	STATION	TO	STATION	LOCATION	SY	TACK COAT GAL	ASPHALTIC SURFACE TON	ASPHALTIC SURFACE PATCHING TON	SPECIAL (01. REMOVING DISTRESSED PAVEMENT MILLING) SY	REMARKS
0010	8+25	-	24+28	STH 136	6,310	44	70	-	320	BEGIN - MIRROR LAKE RD
0010	24+28	-	38+39	STH 136	5,220	36	58	-	270	MIRROR LAKE RD - KOHLMEYER RD
0010	38+39	-	68+28	STH 136	10,410	72	115	-	530	KOHLMEYER RD - PLEASANT VALLEY RD
0010	68+28	-	106+92	STH 136	13,440	93	149	-	680	PLEASANT VALLEY RD - HATCHERY RD
0010	106+92	-	130+49	STH 136	8,250	56	90	-	420	HATCHERY RD - BRIDGE APPROACH
0010	131+57	-	136+69	STH 136	2,450	17	27	-	130	BRIDGE APPROACH - RAVEN ACRES DR
0010	136+69	-	202+25	STH 136	23,030	159	254	-	1,160	RAVEN ACRES DR - END
0010			UNDISTRIBUTED		-	-	-	100	-	

TOTAL 0010 69,110 477 763 100 3,510

**NOTES:**

- \* ADDITIONAL TACK COAT QUANTITIES LISTED ELSEWHERE. APPLICATION RATE = 0.07 GAL/SY.
- \*\* ASSUMED ASPHALTIC SURFACE AT 112 LBS/SY/IN.

**MISCELLANEOUS ITEMS**

CATEGORY	LOCATION	LS	EACH	EACH	EACH	FIELD OFFICE TYPE C EACH
0010	PROJECT-WIDE	1	1	1	1	1
TOTAL 0010		1	1	1	1	1

**BASE AGG SHOULDERS & ENTRANCES**

CATEGORY	STATION	TO	STATION	LOCATION	DENSE 3/4-INCH TON	TACK COAT GAL	ENTRANCES TON	WATER MGAL	REMARKS
0010	8+25	-	37+90	LT/RT SHLDR AND PRIVATE ENTRANCES	230	5	9	2	BEGIN - KOHLMEYER RD
0010	37+90	-	67+59	LT/RT SHLDR AND PRIVATE ENTRANCES	260	7	12	3	KOHLMEYER RD - PLEASANT VALLEY RD
0010	67+59	-	106+40	LT/RT SHLDR AND PRIVATE ENTRANCES	320	8	13	3	PLEASANT VALLEY RD - HATCHERY RD
0010	106+40	-	136+30	LT/RT SHLDR AND PRIVATE ENTRANCES	230	14	24	2	HATCHERY RD - RAVEN ACRES DR
0010	136+30	-	202+25	LT/RT SHLDR AND PRIVATE ENTRANCES	500	23	38	5	RAVEN ACRES DR - END
TOTAL 0010					1,540	57	96	15	

- \* ADDITIONAL TACK COAT QUANTITIES LISTED ELSEWHERE. APPLICATION RATE = 0.07 GAL/SY.
- \*\* ASSUMED ASPHALTIC SURFACE AT 112 LBS/SY/IN.

HMA PAVEMENT

CATEGORY	STATION TO	STATION	LOCATION	HMA PAVEMENT		REMARKS
				TACK COAT GAL	4 LT 58-28 S TON	
0010	8+25 -	24+28	STH 136	436	697	BEGIN - MIRROR LAKE RD
0010	24+28 -	38+39	STH 136	365	584	MIRROR LAKE RD - KOHLMeyer RD
0010	38+39 -	68+28	STH 136	721	1,150	KOHLMEYER RD - PLEASANT VALLEY RD
0010	68+28 -	106+92	STH 136	933	1,490	PLEASANT VALLEY RD - HATCHERY RD
0010	106+92 -	130+49	STH 136	563	900	HATCHERY RD - BRIDGE APPROACH
0010	131+57 -	136+69	STH 136	171	274	BRIDGE APPROACH - RAVEN ACRES DR
0010	136+69 -	202+25	STH 136	1,588	2,540	RAVEN ACRES DR - END
TOTAL 0010				4,777	7,635	

\* ADDITIONAL TACK COAT QUANTITIES LISTED ELSEWHERE. APPLICATION RATE = 0.07 GAL/SY.  
 \*\* ASSUMED HMA AT 112 LBS/SY/IN

RUMBLE STRIPS

CATEGORY	STATION TO	STATION	LOCATION	RUMBLE STRIPS		REMARKS
				2-LANE RURAL LF	2-LANE RURAL LF	
0010	11+77 -	22+00	LT SHLDR	891	-	BEGIN - MIRROR LAKE RD
0010	11+77 -	22+89	RT SHLDR	1,111	-	BEGIN - MIRROR LAKE RD
0010	12+10 -	21+65	CENTERLINE	-	955	BEGIN - MIRROR LAKE RD
0010	23+64 -	36+20	RT SHLDR	1,254	-	MIRROR LAKE RD - KOHLMeyer RD
0010	25+28 -	65+96	LT SHLDR	4,054	-	MIRROR LAKE RD - PLEASANT VALLEY RD
0010	25+62 -	35+90	CENTERLINE	-	1,028	MIRROR LAKE RD - KOHLMeyer RD
0010	39+48 -	104+72	RT SHLDR	6,395	-	KOHLMEYER RD - HATCHERY RD
0010	69+27 -	130+24	LT SHLDR	5,716	-	PLEASANT VALLEY RD - BRIDGE
0010	39+89 -	65+60	CENTERLINE	-	2,572	KOHLMEYER RD - PLEASANT VALLEY RD
0010	108+02 -	130+24	RT SHLDR	1,907	-	HATCHERY RD - BRIDGE
0010	69+57 -	104+40	CENTERLINE	-	3,483	PLEASANT VALLEY RD - HATCHERY RD
0010	108+40 -	130+24	CENTERLINE	-	2,184	HATCHERY RD - BRIDGE
0010	131+82 -	134+48	RT SHLDR	200	-	BRIDGE - RAVEN ACRES DR
0010	134+93 -	192+00	LT SHLDR	5,323	-	CORNFIELD DR - SPEED LIMIT REDUCTION
0010	137+80 -	192+00	RT SHLDR	5,014	-	RAVEN ACRES DR - SPEED LIMIT REDUCTION
0010	138+30 -	192+00	CENTERLINE	-	5,371	RAVEN ACRES DR - SPEED LIMIT REDUCTION
TOTAL 0010				31,865	15,593	

\*\*\*TYPE 2 RUMBLE STRIPS.

INLET PROTECTION TYPE C

CATEGORY	STATION	LOCATION	EROSION CONTROL MOBILIZATION		REMARKS
			628.1905 MOBILIZATIONS	628.1910 EMERGENCY EROSION CONTROL	
0010	193+10	RT	1		628.7015 INLET PROTECTION TYPE C EACH
0010	193+95	RT	1		
0010	194+05	RT	1		
0010	196+70	RT	1		
0010	196+80	RT	1		
0010	197+80	LT	1		
TOTAL 0010			6		

MOVING SIGNS TYPE II

CATEGORY	LOCATION	MOVING SIGNS TYPE II			REMARKS
		634.0616 POSTS WOOD 4X6-INCH X 16-FT EACH	638.2102 MOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	
0010	UNDISTRIBUTED	2	2	2	NO PASSING ZONE SIGNS (W14-3)
TOTAL 0010		2	2	2	

TRAFFIC CONTROL SIGNS

STAGE	LOCATION	643.0900 APPROXIMATE SERVICE DAYS		643.1050 TRAFFIC CONTROL SIGNS PCMS		REMARKS
		NO. IN SERVICE	DAY	NO. IN SERVICE	DAY	
PRE-1	EB STH 136	7	-	1	7	APPROACHING WEST PROJECT LIMIT
PRE-1	WB STH 136	7	-	1	7	APPROACHING EAST PROJECT LIMIT
1	PROJECT-WIDE	6	23	-	-	FLAGGING
2	PROJECT-WIDE	9	23	-	-	FLAGGING
3	PROJECT-WIDE	16	23	-	-	FLAGGING
1,2,3	UNDISTRIBUTED	-	-	87	-	
TOTAL 0010			800		14	

**PERMANENT PAVEMENT MARKING**

CATEGORY	STATION TO STATION	LOCATION	MARKING LINE			MI	REMARKS
			MARKING LINE EPOXY 4-INCH (WHITE) LF	MARKING LINE EPOXY 4-INCH (YELLOW) LF	SAME DAY EPOXY 4-INCH (YELLOW) LF		
0010	8+25 - 38+39	CENTERLINE WITH LT/RT EDGELINE	5538	43	4,592	-	BEGIN - KOHLMAYER RD
0010	38+39 - 69+13	CENTERLINE WITH LT/RT EDGELINE	5994	-	6,155	-	KOHLMEYER RD - PLEASANT VALLEY RD
0010	69+13 - 108+72	CENTERLINE WITH LT/RT EDGELINE	7724	-	4,188	-	PLEASANT VALLEY RD - HATCHERY RD
0010	108+72 - 144+64	CENTERLINE WITH LT/RT EDGELINE	6824	-	2,595	-	HATCHERY RD - RAVEN ACRES DR
0010	144+64 - 202+25	CENTERLINE WITH LT/RT EDGELINE	11521	-	8,648	-	RAVEN ACRES DR - END
0010	PROJECT-WIDE		-	-	-	3.65	
TOTAL 0010				37,644	26,178	3.65	

**TRAFFIC CONTROL**

LOCATION	643.5000 TRAFFIC CONTROL EACH
PROJECT-WIDE	1
TOTAL 0010	1

**CONSTRUCTION STAKING**

CATEGORY	STATION TO STATION	LOCATION	CONSTRUCTION STAKING	
			CONSTRUCTION STAKING RESURFACING REFERENCE LF	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (01. 5573-03-73) LS
0010	8+25 - 130+49	STH 136	12,224	-
0010	131+57 - 202+25	STH 136	7,068	-
0010	8+25 - 202+25	PROJECT-WIDE	-	1
TOTAL 0010			19,292	1

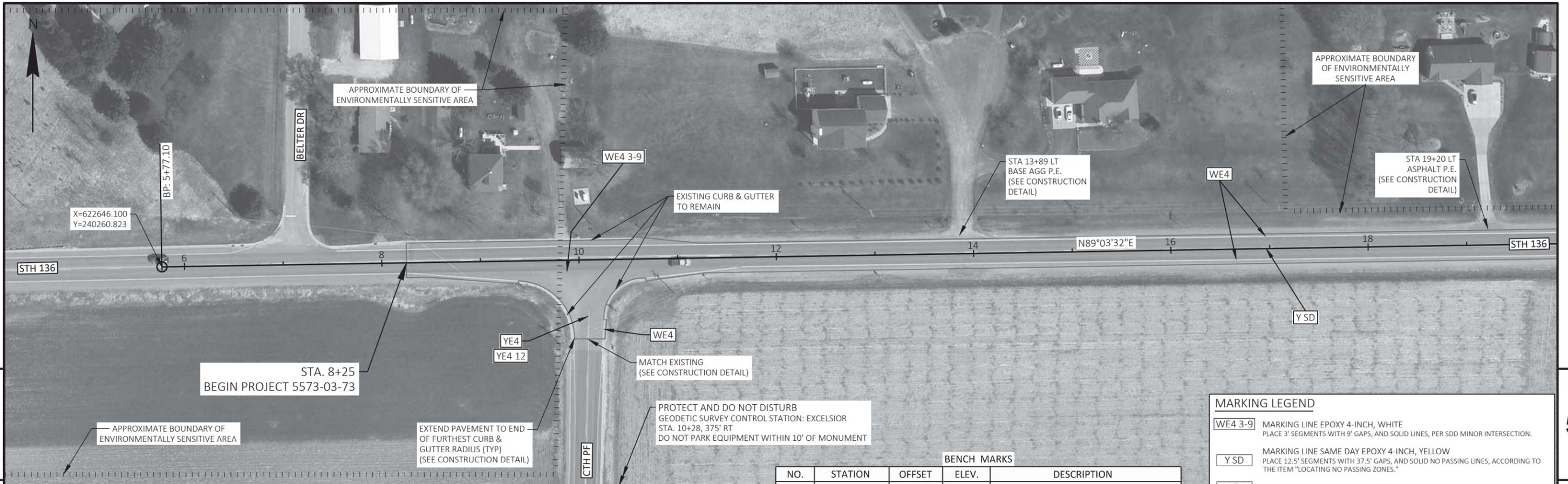
**TEMPORARY PAVEMENT MARKING\***

CATEGORY	STATION TO STATION	LOCATION	649.0120 TEMPORARY MARKING LINE EPOXY 4-INCH LF
0010	8+25 - 202+25	CENTERLINE	1,550
TOTAL 0010			1,550

\*PLACE BEFORE OPENING TO TWO-WAY TRAFFIC AND PRIOR TO RUMBLE STRIP INSTALLATION.

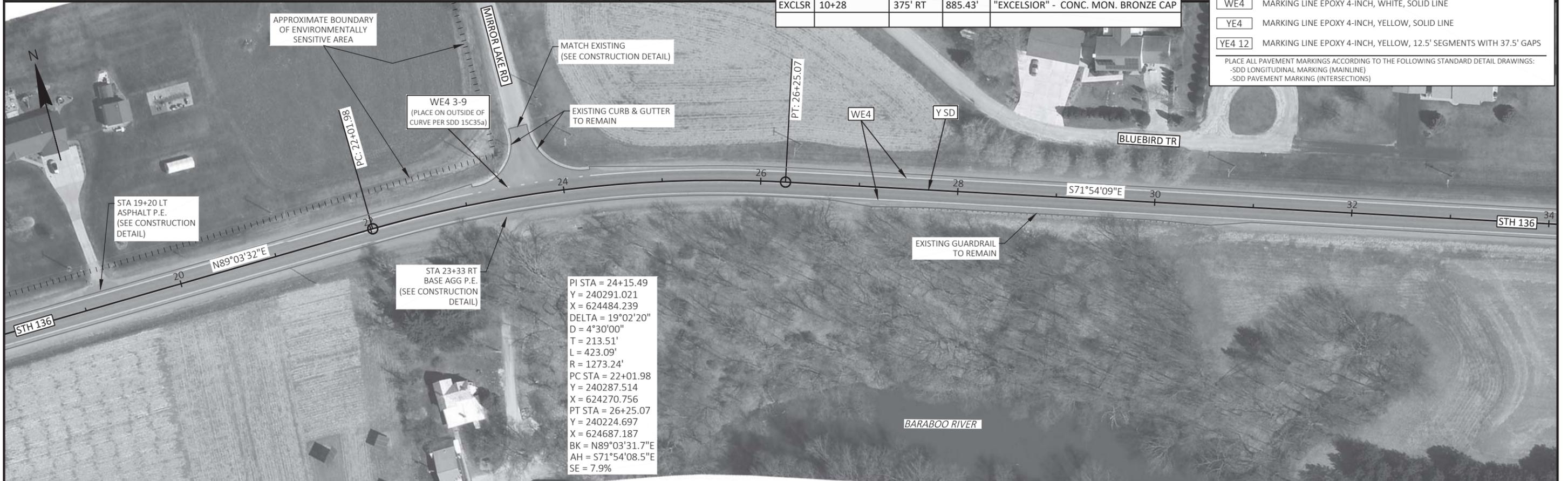
**LANDMARK REFERENCE MONUMENTS**

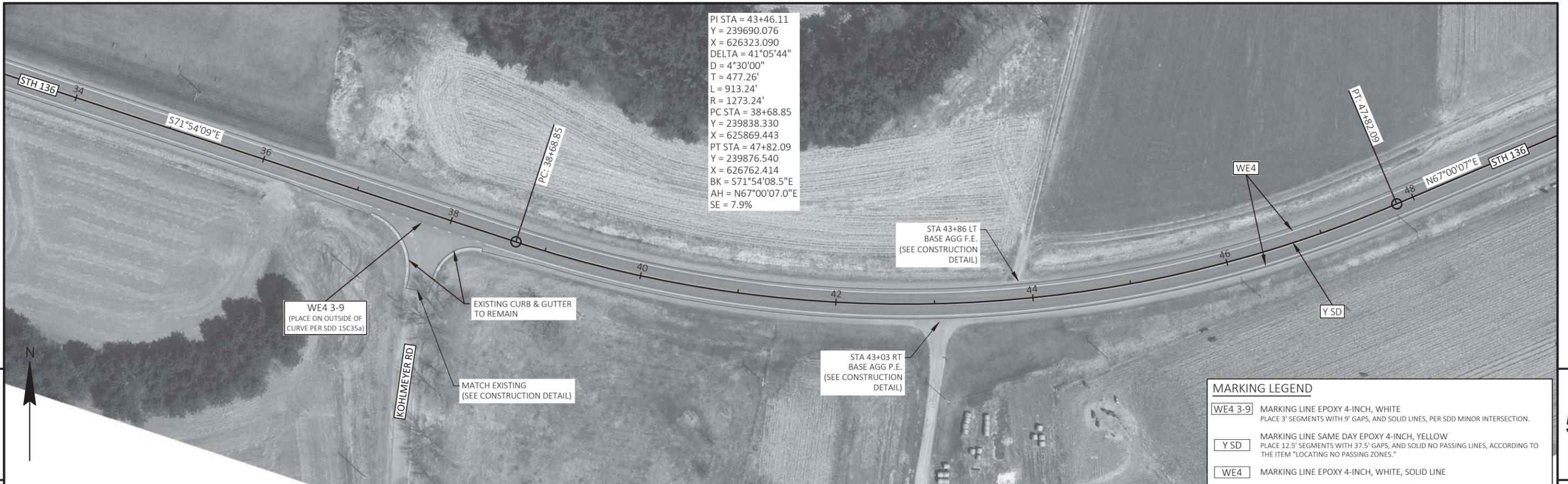
CATEGORY	STATION	LOCATION	SPV.0060 SPECIAL MONUMENTS		REMARKS
			SPV.0060.01 SPECIAL (01. LANDMARK REFERENCE MONUMENTS SPECIAL) EACH	SPV.0060.02 SPECIAL (02. VERIFY LANDMARK REFERENCE MONUMENTS) EACH	
0010	93+03	0.2' RT	1	4	T12N,R6E SW CORNER SECTION 29
0010	119+95	1' LT	1	4	T12N,R6E S 1/4 CONER SECTION 29
0010	146+81	35' LT	-	4	T12N, R6E SE CORNER SECTION 29
TOTAL 0010			2	12	



MARKING LEGEND	
WE4 3-9	MARKING LINE EPOXY 4-INCH, WHITE PLACE 3' SEGMENTS WITH 9' GAPS, AND SOLID LINES, PER SDD MINOR INTERSECTION.
Y SD	MARKING LINE SAME DAY EPOXY 4-INCH, YELLOW PLACE 12.5' SEGMENTS WITH 37.5' GAPS, AND SOLID NO PASSING LINES, ACCORDING TO THE ITEM "LOCATING NO PASSING ZONES."
WE4	MARKING LINE EPOXY 4-INCH, WHITE, SOLID LINE
YE4	MARKING LINE EPOXY 4-INCH, YELLOW, SOLID LINE
YE4 12	MARKING LINE EPOXY 4-INCH, YELLOW, 12.5' SEGMENTS WITH 37.5' GAPS

PLACE ALL PAVEMENT MARKINGS ACCORDING TO THE FOLLOWING STANDARD DETAIL DRAWINGS:  
 -SDD LONGITUDINAL MARKING (MAINLINE)  
 -SDD PAVEMENT MARKING (INTERSECTIONS)

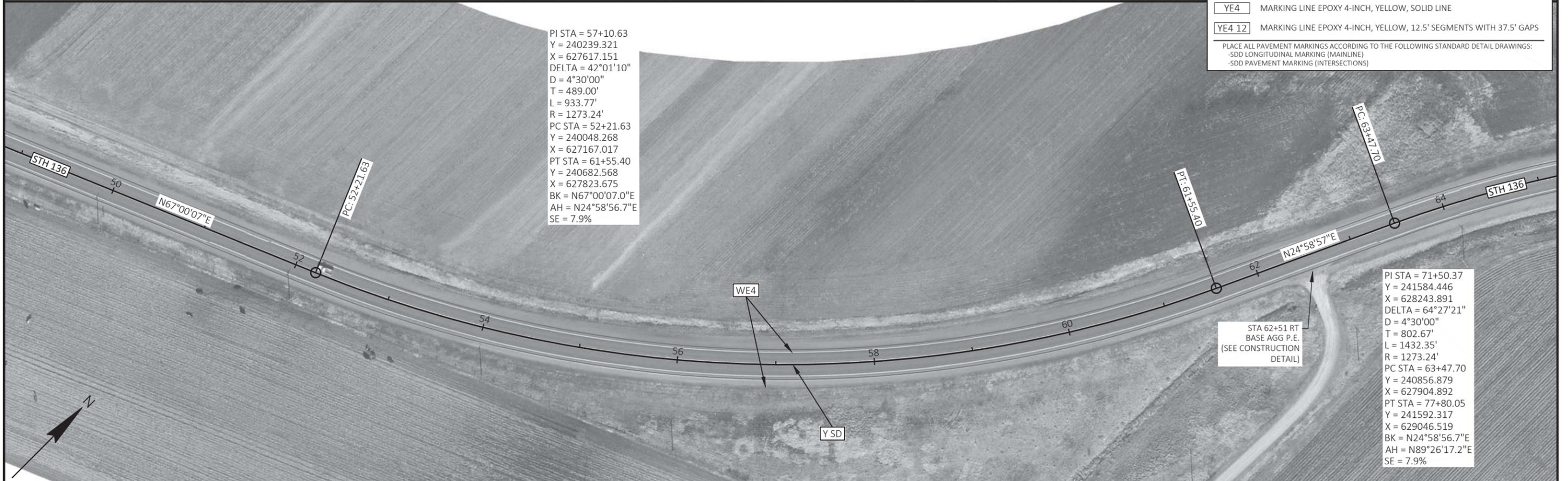




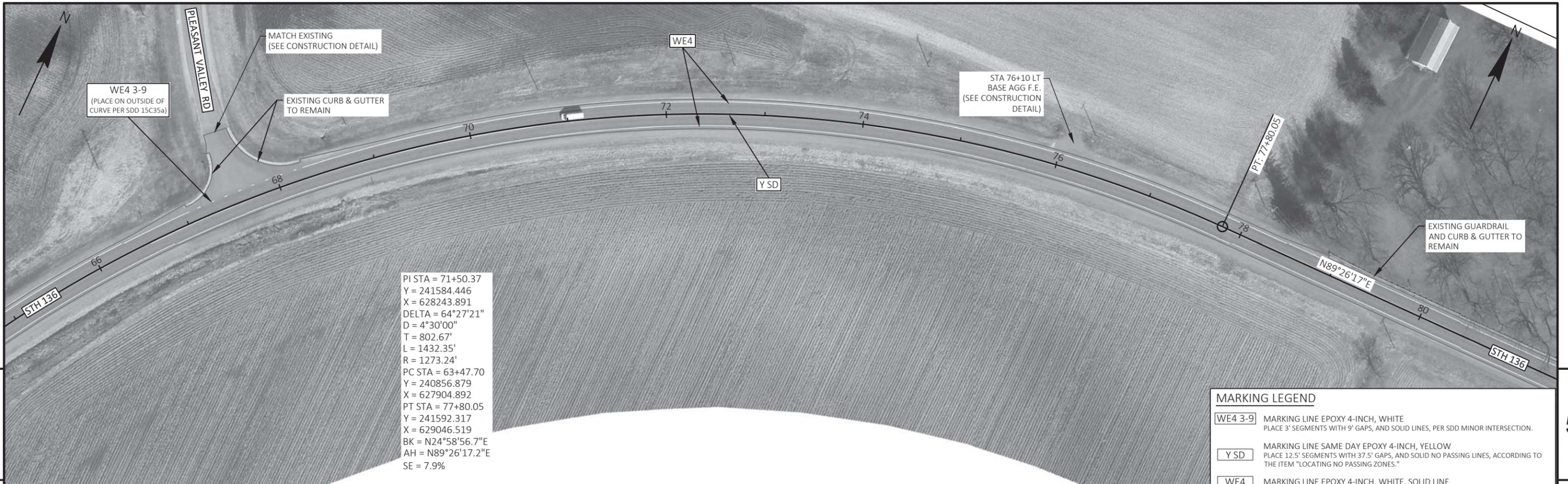
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MARKING LEGEND	
	MARKING LINE EPOXY 4-INCH, WHITE PLACE 3' SEGMENTS WITH 9' GAPS, AND SOLID LINES, PER SDD MINOR INTERSECTION.
	MARKING LINE SAME DAY EPOXY 4-INCH, YELLOW PLACE 12.5' SEGMENTS WITH 37.5' GAPS, AND SOLID NO PASSING LINES, ACCORDING TO THE ITEM "LOCATING NO PASSING ZONES."
	MARKING LINE EPOXY 4-INCH, WHITE, SOLID LINE
	MARKING LINE EPOXY 4-INCH, YELLOW, SOLID LINE
	MARKING LINE EPOXY 4-INCH, YELLOW, 12.5' SEGMENTS WITH 37.5' GAPS
PLACE ALL PAVEMENT MARKINGS ACCORDING TO THE FOLLOWING STANDARD DETAIL DRAWINGS: -SDD LONGITUDINAL MARKING (MAINLINE) -SDD PAVEMENT MARKING (INTERSECTIONS)	

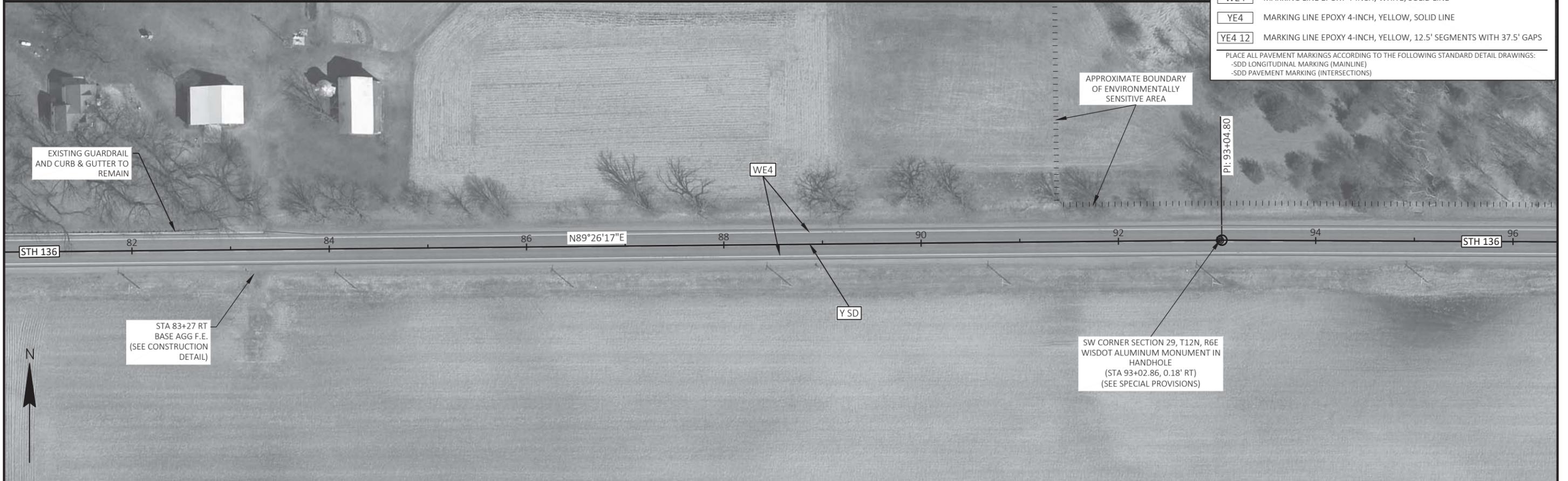


PROJECT NO: 5573-03-73	HWY: STH 136	COUNTY: SAUK	PLAN DETAILS	SHEET	E
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PI STA = 71+50.37
Y = 241584.446
X = 628243.891
DELTA = 64°27'21"
D = 4°30'00"
T = 802.67'
L = 1432.35'
R = 1273.24'
PC STA = 63+47.70
Y = 240856.879
X = 627904.892
PT STA = 77+80.05
Y = 241592.317
X = 629046.519
BK = N24°58'56.7"E
AH = N89°26'17.2"E
SE = 7.9%

MARKING LEGEND	
<b>WE4 3-9</b>	MARKING LINE EPOXY 4-INCH, WHITE PLACE 3' SEGMENTS WITH 9' GAPS, AND SOLID LINES, PER SDD MINOR INTERSECTION.
<b>Y SD</b>	MARKING LINE SAME DAY EPOXY 4-INCH, YELLOW PLACE 12.5' SEGMENTS WITH 37.5' GAPS, AND SOLID NO PASSING LINES, ACCORDING TO THE ITEM "LOCATING NO PASSING ZONES."
<b>WE4</b>	MARKING LINE EPOXY 4-INCH, WHITE, SOLID LINE
<b>YE4</b>	MARKING LINE EPOXY 4-INCH, YELLOW, SOLID LINE
<b>YE4 12</b>	MARKING LINE EPOXY 4-INCH, YELLOW, 12.5' SEGMENTS WITH 37.5' GAPS
PLACE ALL PAVEMENT MARKINGS ACCORDING TO THE FOLLOWING STANDARD DETAIL DRAWINGS: -SDD LONGITUDINAL MARKING (MAINLINE) -SDD PAVEMENT MARKING (INTERSECTIONS)	



PROJECT NO: 5573-03-73	HWY: STH 136	COUNTY: SAUK	PLAN DETAILS	SHEET	<b>E</b>
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APPROXIMATE BOUNDARY OF ENVIRONMENTALLY SENSITIVE AREA

STA 97+39 LT  
BASE AGG P.E.  
(SEE CONSTRUCTION  
DETAIL)

STA 98+16 LT  
BASE AGG P.E.  
(SEE CONSTRUCTION  
DETAIL)

STA 99+54 LT  
BASE AGG P.E.  
(SEE CONSTRUCTION  
DETAIL)

WE4

WE4 3-9

STA 107+92 LT  
ASPHALT P.E.  
(SEE CONSTRUCTION  
DETAIL)

STA 108+21 LT  
ASPHALT P.E.  
(SEE CONSTRUCTION  
DETAIL)

96

98

100

S89°43'13"E

102

104

106

108

110

STH 136

YSD

EXISTING CURB & GUTTER  
TO REMAIN

MATCH EXISTING  
(SEE CONSTRUCTION  
DETAIL)

HATCHERY RD

PROTECT AND DO NOT DISTURB  
GEODETIK SURVEY CONTROL STATION: 2F53  
STA. 106+22, 211' RT  
DO NOT PARK EQUIPMENT WITHIN 10' OF MONUMENT.

**MARKING LEGEND**

- WE4 3-9** MARKING LINE EPOXY 4-INCH, WHITE  
PLACE 3' SEGMENTS WITH 9' GAPS, AND SOLID LINES, PER SDD MINOR INTERSECTION.
  - YSD** MARKING LINE SAME DAY EPOXY 4-INCH, YELLOW  
PLACE 12.5' SEGMENTS WITH 37.5' GAPS, AND SOLID NO PASSING LINES, ACCORDING TO  
THE ITEM "LOCATING NO PASSING ZONES."
  - WE4** MARKING LINE EPOXY 4-INCH, WHITE, SOLID LINE
  - YE4** MARKING LINE EPOXY 4-INCH, YELLOW, SOLID LINE
  - YE4 12** MARKING LINE EPOXY 4-INCH, YELLOW, 12.5' SEGMENTS WITH 37.5' GAPS
- PLACE ALL PAVEMENT MARKINGS ACCORDING TO THE FOLLOWING STANDARD DETAIL DRAWINGS:  
-SDD LONGITUDINAL MARKING (MAINLINE)  
-SDD PAVEMENT MARKING (INTERSECTIONS)

**BENCH MARKS**

NO.	STATION	OFFSET	ELEV.	DESCRIPTION
2F53	106+22	211' RT	883.52'	CONC. MON. BRONZE CAP

N

Pl: 111+98.82

S1/4 CORNER SECTION 29, T12N, R6E  
(STA 119+95, 1' LT)  
(SEE SPECIAL PROVISIONS)

Pl: 123+98.30

112

114

116

S89°31'49"E

118

120

122

S89°38'18"E

124

126

STH 136

STA 113+81 RT  
ASPHALT P.E.  
(SEE CONSTRUCTION  
DETAIL)

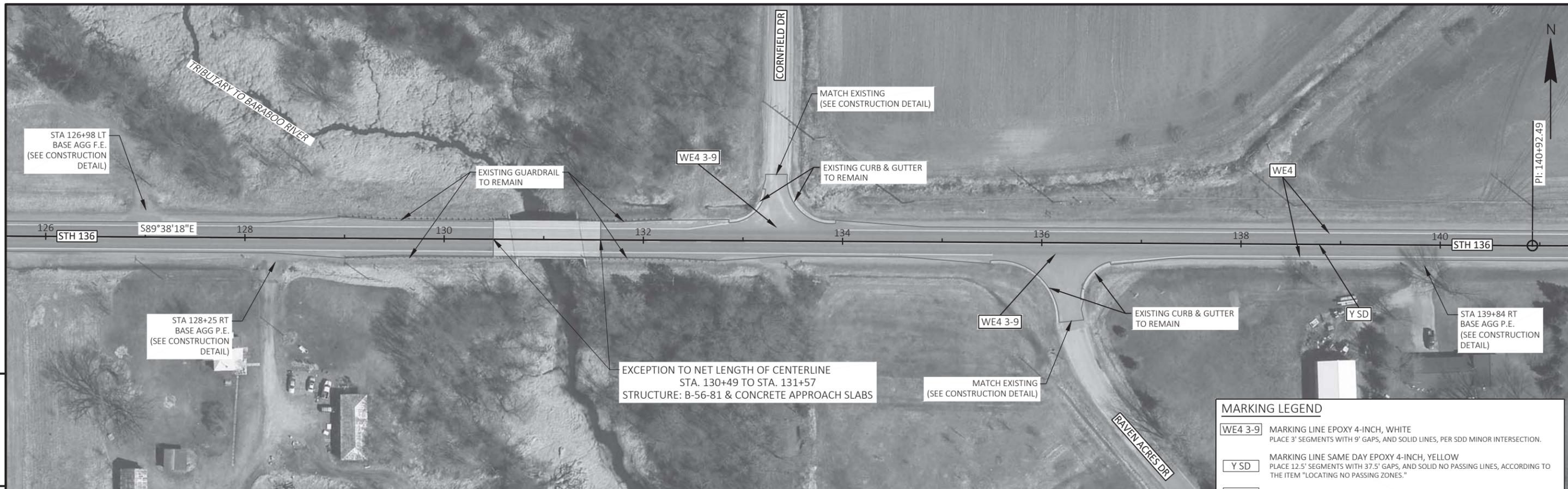
STA 115+35 RT  
BASE AGG P.E.  
(SEE CONSTRUCTION  
DETAIL)

YSD

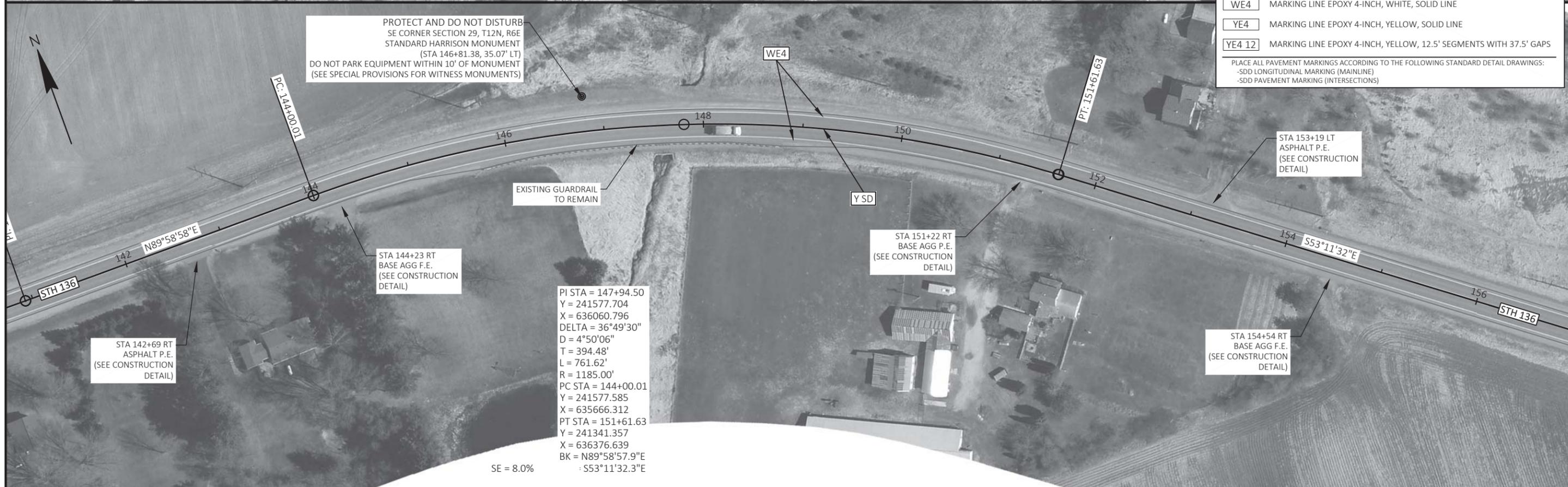
STA 120+26 RT  
BASE AGG P.E.  
(SEE CONSTRUCTION  
DETAIL)

STA 122+97 RT  
BASE AGG P.E.  
(SEE CONSTRUCTION  
DETAIL)

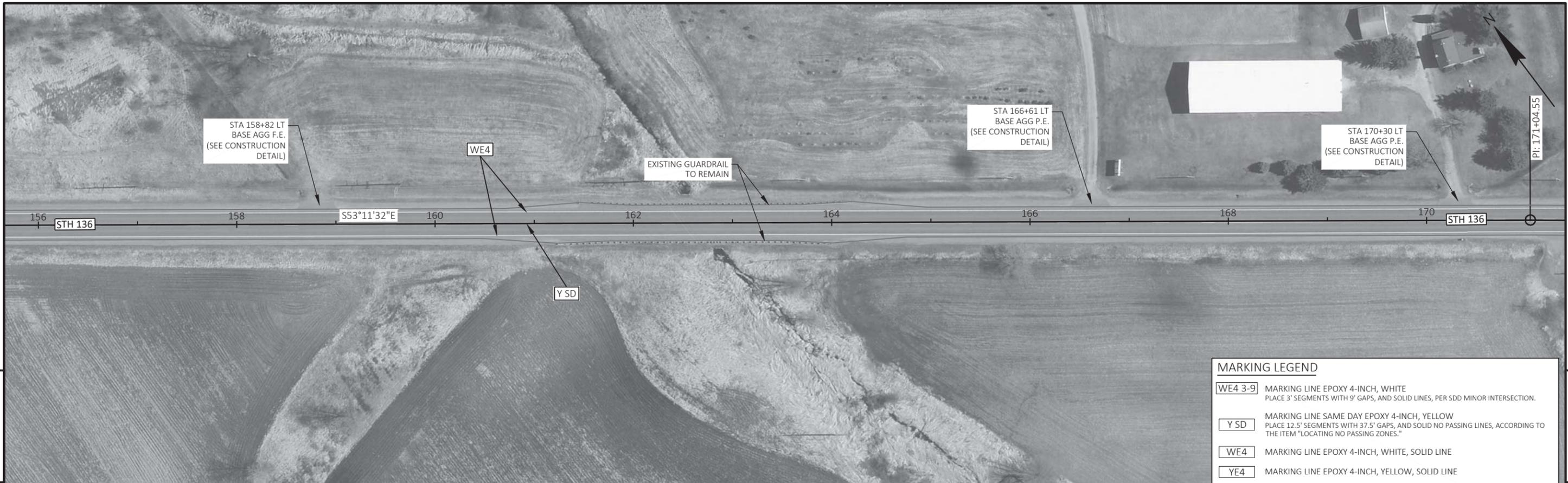
STA 124+91 RT  
BASE AGG P.E.  
(SEE CONSTRUCTION  
DETAIL)



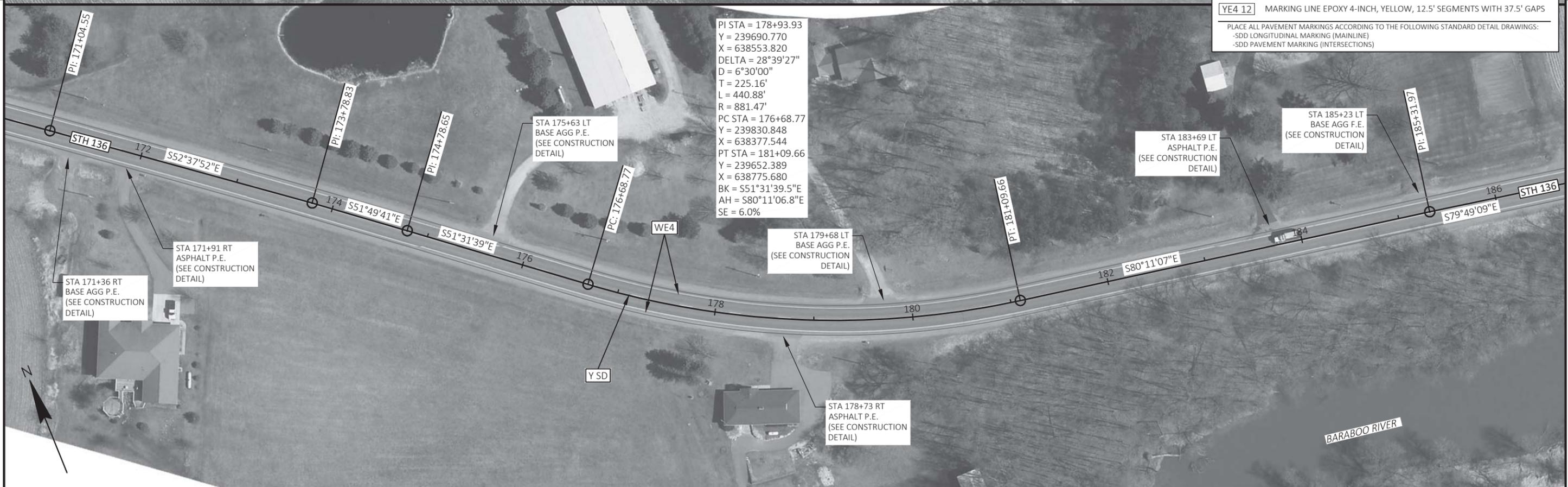
MARKING LEGEND	
WE4 3-9	MARKING LINE EPOXY 4-INCH, WHITE PLACE 3' SEGMENTS WITH 9' GAPS, AND SOLID LINES, PER SDD MINOR INTERSECTION.
Y SD	MARKING LINE SAME DAY EPOXY 4-INCH, YELLOW PLACE 12.5' SEGMENTS WITH 37.5' GAPS, AND SOLID NO PASSING LINES, ACCORDING TO THE ITEM "LOCATING NO PASSING ZONES."
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PLACE ALL PAVEMENT MARKINGS ACCORDING TO THE FOLLOWING STANDARD DETAIL DRAWINGS: -SDD LONGITUDINAL MARKING (MAINLINE) -SDD PAVEMENT MARKING (INTERSECTIONS)	



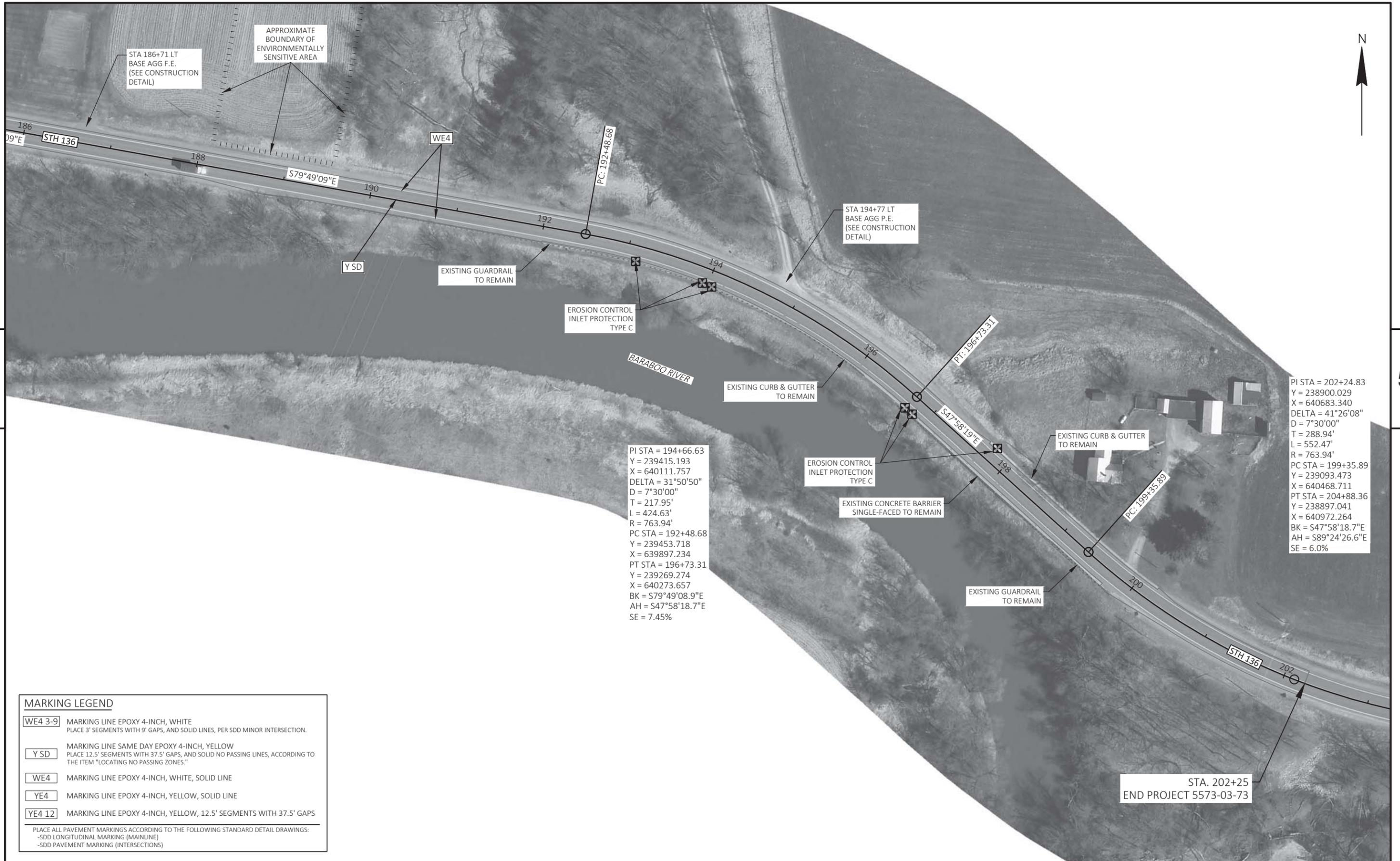
PI STA = 147+94.50  
 Y = 241577.704  
 X = 636060.796  
 DELTA = 36°49'30"  
 D = 4°50'06"  
 T = 394.48'  
 L = 761.62'  
 R = 1185.00'  
 PC STA = 144+00.01  
 Y = 241577.585  
 X = 635666.312  
 PT STA = 151+61.63  
 Y = 241341.357  
 X = 636376.639  
 BK = N89°58'57.9"E  
 S53°11'32.3"E  
 SE = 8.0%



MARKING LEGEND	
<b>WE4 3-9</b>	MARKING LINE EPOXY 4-INCH, WHITE PLACE 3' SEGMENTS WITH 9' GAPS, AND SOLID LINES, PER SDD MINOR INTERSECTION.
<b>Y SD</b>	MARKING LINE SAME DAY EPOXY 4-INCH, YELLOW PLACE 12.5' SEGMENTS WITH 37.5' GAPS, AND SOLID NO PASSING LINES, ACCORDING TO THE ITEM "LOCATING NO PASSING ZONES."
<b>WE4</b>	MARKING LINE EPOXY 4-INCH, WHITE, SOLID LINE
<b>YE4</b>	MARKING LINE EPOXY 4-INCH, YELLOW, SOLID LINE
<b>YE4 12</b>	MARKING LINE EPOXY 4-INCH, YELLOW, 12.5' SEGMENTS WITH 37.5' GAPS
PLACE ALL PAVEMENT MARKINGS ACCORDING TO THE FOLLOWING STANDARD DETAIL DRAWINGS: -SDD LONGITUDINAL MARKING (MAINLINE) -SDD PAVEMENT MARKING (INTERSECTIONS)	



PI STA = 178+93.93  
 Y = 239690.770  
 X = 638553.820  
 DELTA = 28°39'27"  
 D = 6°30'00"  
 T = 225.16'  
 L = 440.88'  
 R = 881.47'  
 PC STA = 176+68.77  
 Y = 239830.848  
 X = 638377.544  
 PT STA = 181+09.66  
 Y = 239652.389  
 X = 638775.680  
 BK = S51°31'39.5"E  
 AH = S80°11'06.8"E  
 SE = 6.0%



5

5

**MARKING LEGEND**

<b>WE4 3-9</b>	MARKING LINE EPOXY 4-INCH, WHITE PLACE 3' SEGMENTS WITH 9' GAPS, AND SOLID LINES, PER SDD MINOR INTERSECTION.
<b>Y SD</b>	MARKING LINE SAME DAY EPOXY 4-INCH, YELLOW PLACE 12.5' SEGMENTS WITH 37.5' GAPS, AND SOLID NO PASSING LINES, ACCORDING TO THE ITEM "LOCATING NO PASSING ZONES."
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<b>YE4 12</b>	MARKING LINE EPOXY 4-INCH, YELLOW, 12.5' SEGMENTS WITH 37.5' GAPS

PLACE ALL PAVEMENT MARKINGS ACCORDING TO THE FOLLOWING STANDARD DETAIL DRAWINGS:  
 -SDD LONGITUDINAL MARKING (MAINLINE)  
 -SDD PAVEMENT MARKING (INTERSECTIONS)

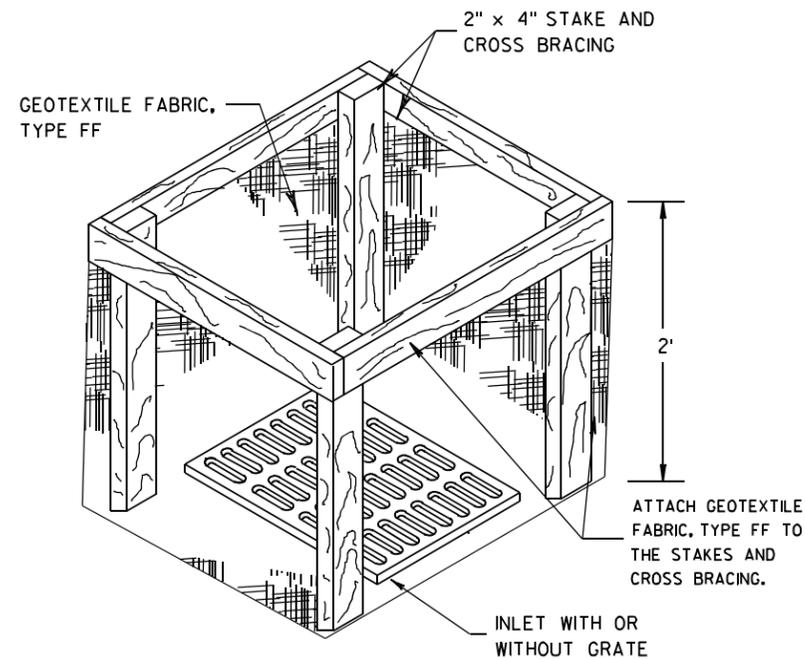
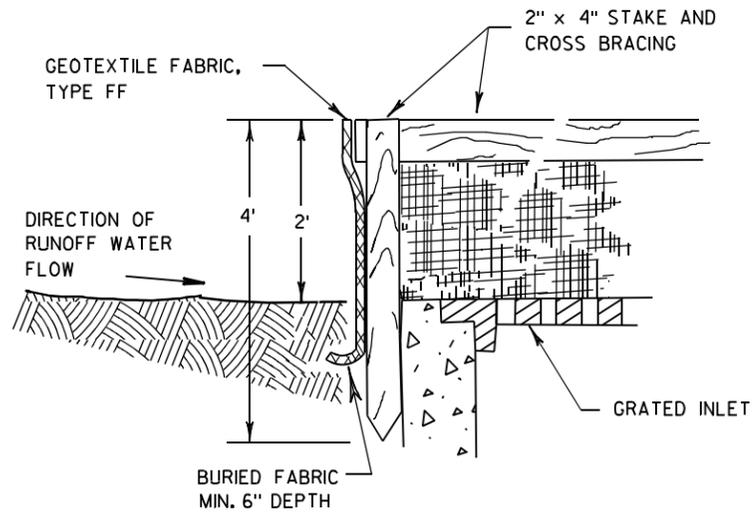
PI STA = 194+66.63  
 Y = 239415.193  
 X = 640111.757  
 DELTA = 31°50'50"  
 D = 7°30'00"  
 T = 217.95'  
 L = 424.63'  
 R = 763.94'  
 PC STA = 192+48.68  
 Y = 239453.718  
 X = 639897.234  
 PT STA = 196+73.31  
 Y = 239269.274  
 X = 640273.657  
 BK = S79°49'08.9"E  
 AH = S47°58'18.7"E  
 SE = 7.45%

PI STA = 202+24.83  
 Y = 238900.029  
 X = 640683.340  
 DELTA = 41°26'08"  
 D = 7°30'00"  
 T = 288.94'  
 L = 552.47'  
 R = 763.94'  
 PC STA = 199+35.89  
 Y = 239093.473  
 X = 640468.711  
 PT STA = 204+88.36  
 Y = 238897.041  
 X = 640972.264  
 BK = S47°58'18.7"E  
 AH = S89°24'26.6"E  
 SE = 6.0%

STA. 202+25  
 END PROJECT 5573-03-73

## Standard Detail Drawing List

08E10-02	INLET PROTECTION TYPE A, B, C AND D
13A10-02B	2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING
13A10-02C	2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING
13A11-03A	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13A11-03B	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13C19-03	HMA LONGITUDINAL JOINTS
14B29-01	SAFETY EDGE
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C08-20A	LONGITUDINAL MARKING (MAINLINE)
15C11-09B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-07	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C19-06A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C35-04A	PAVEMENT MARKING (INTERSECTIONS)
15D29-06	TRAFFIC CONTROL, VEHICLE ENTRANCE/EXIT OR HAUL ROAD
15D39-02	TRAFFIC CONTROL, DROP-OFF SIGNING
15D43-01	TRAFFIC CONTROL, SHORT DURATION MOBILE OPERATIONS
15D44-02	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES
15D51-01	TRAFFIC CONTROL, MOBILE OPERATIONS ON AN UNDIVIDED ROADWAY
16A01-07	LANDMARK REFERENCE MONUMENTS AND COVERS



**INLET PROTECTION, TYPE A**

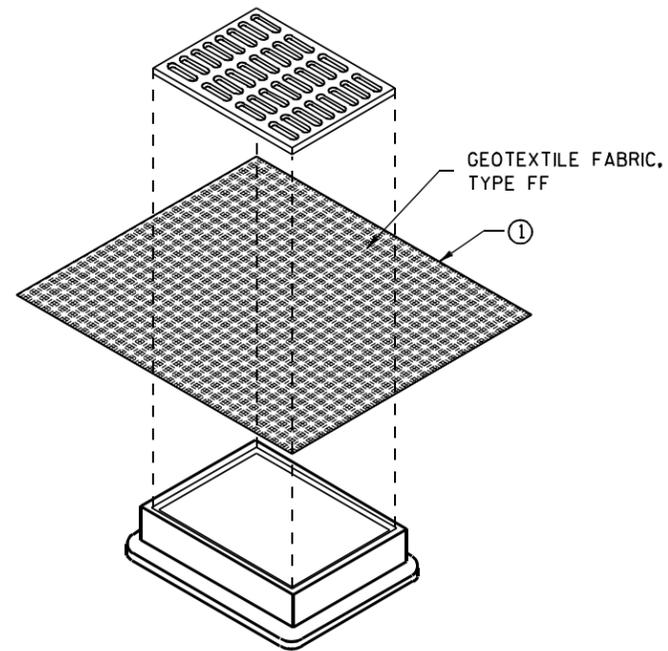
**GENERAL NOTES**

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

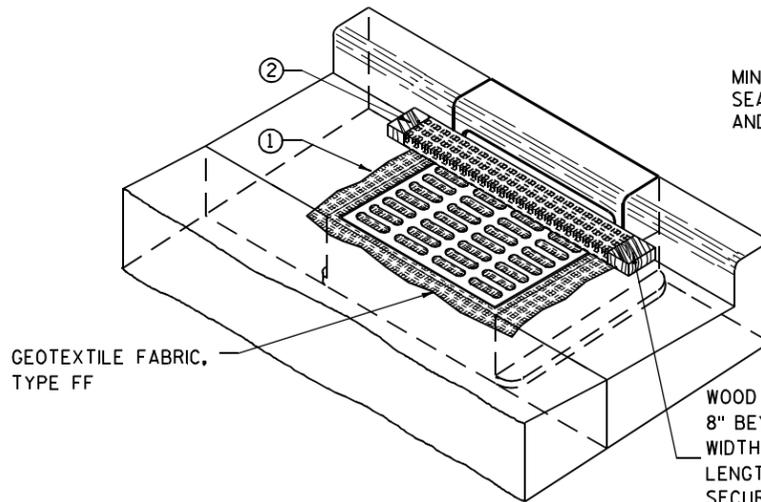
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B  
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



**INLET PROTECTION, TYPE C (WITH CURB BOX)**

**INSTALLATION NOTES**

**TYPE B & C**

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

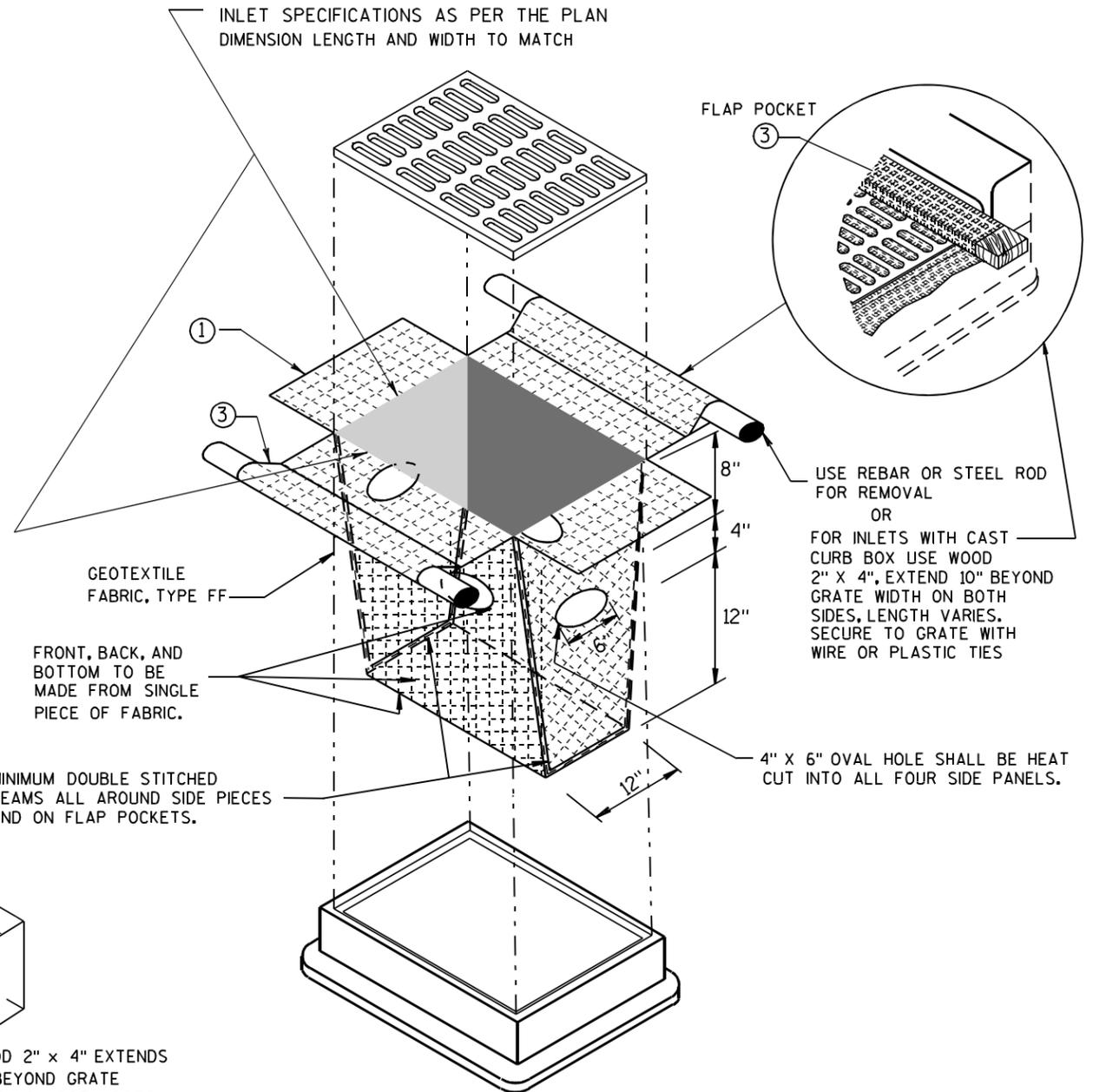
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

**TYPE D**

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



**INLET PROTECTION, TYPE D**

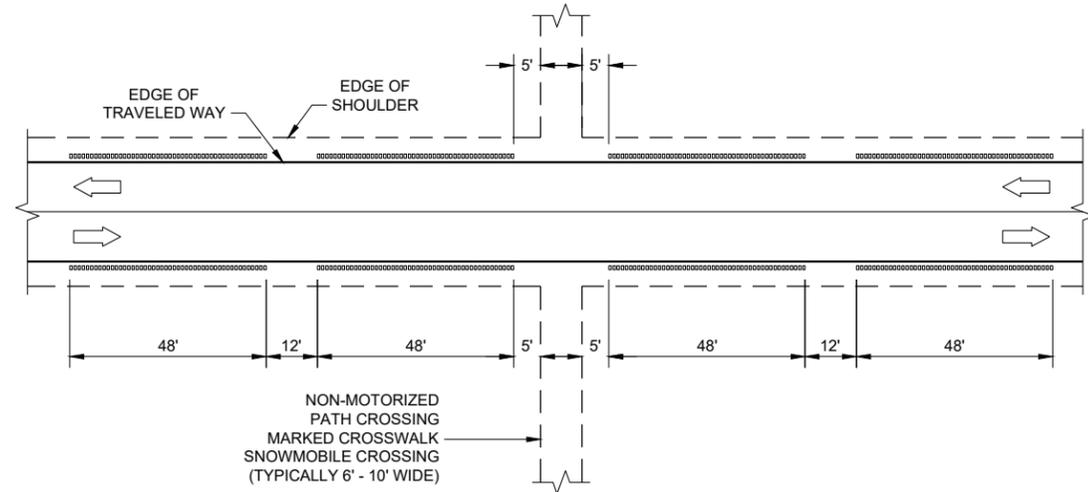
(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

**INLET PROTECTION  
TYPE A, B, C, AND D**

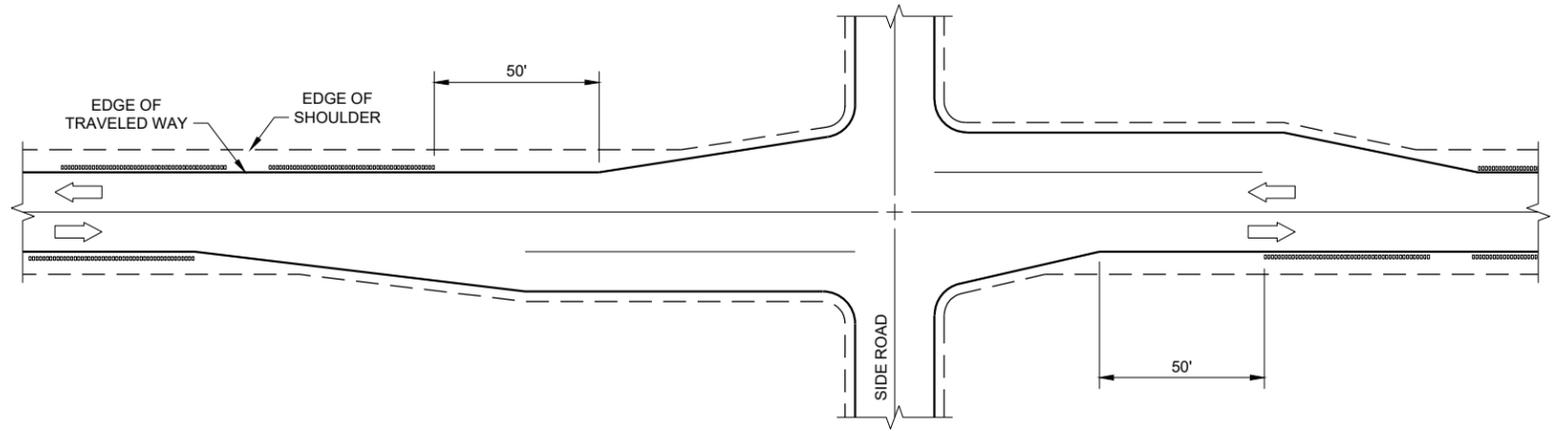
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
10/16/02 /S/ Beth Connestra  
DATE  
CHIEF ROADWAY DEVELOPMENT ENGINEER  
FHWA

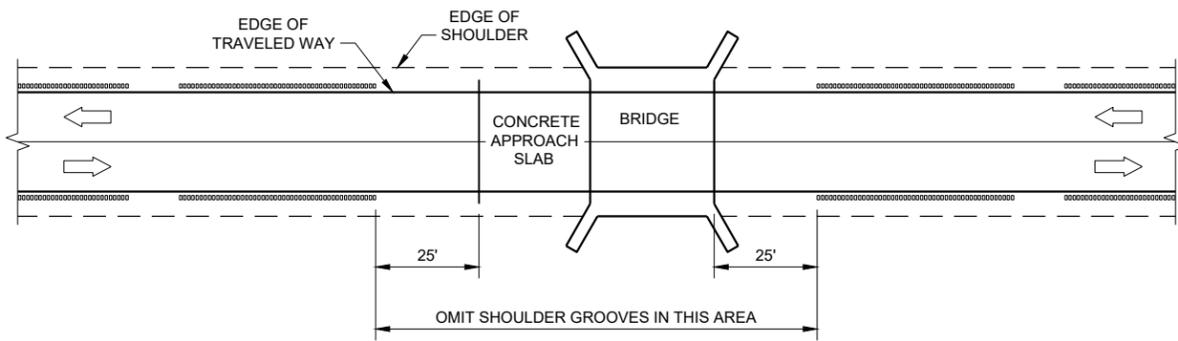




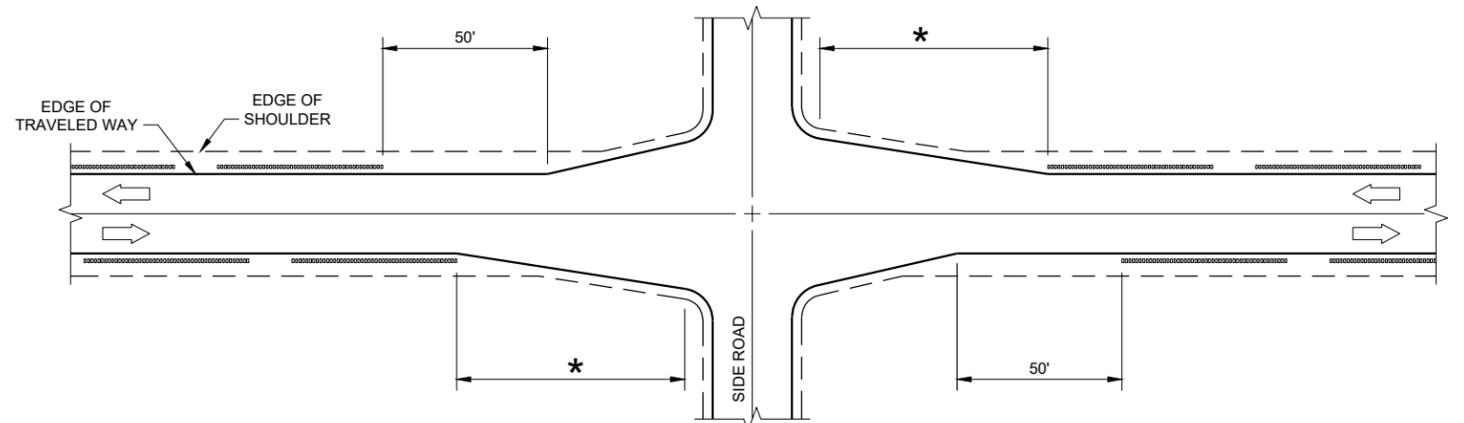
**SHOULDER GROOVES AT MISCELLANEOUS CROSSINGS**



**SHOULDER GROOVES AT RIGHT TURN LANE**

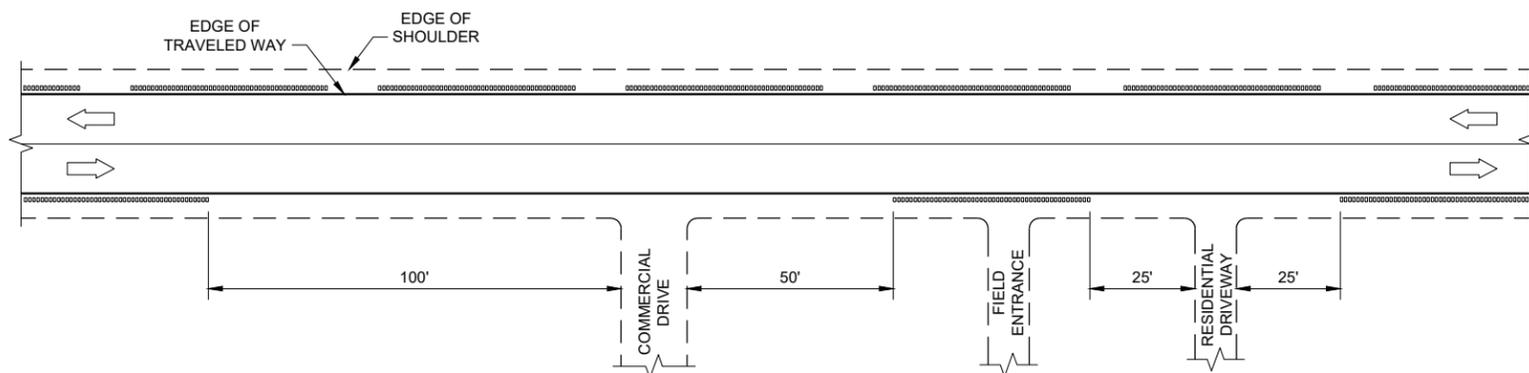


**SHOULDER GROOVES AT BRIDGES**



**SHOULDER GROOVES AT INTERSECTIONS WITH APPROACH TAPER**

\* GREATER OF 100' OR APPROACH TAPER LENGTH



**SHOULDER GROOVES AT DRIVEWAYS<sup>①</sup>**

**GENERAL NOTES**

- ① SHOULDER GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.

**2-LANE RURAL SHOULDER  
RUMBLE STRIP, MILLING**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

**GENERAL NOTES**

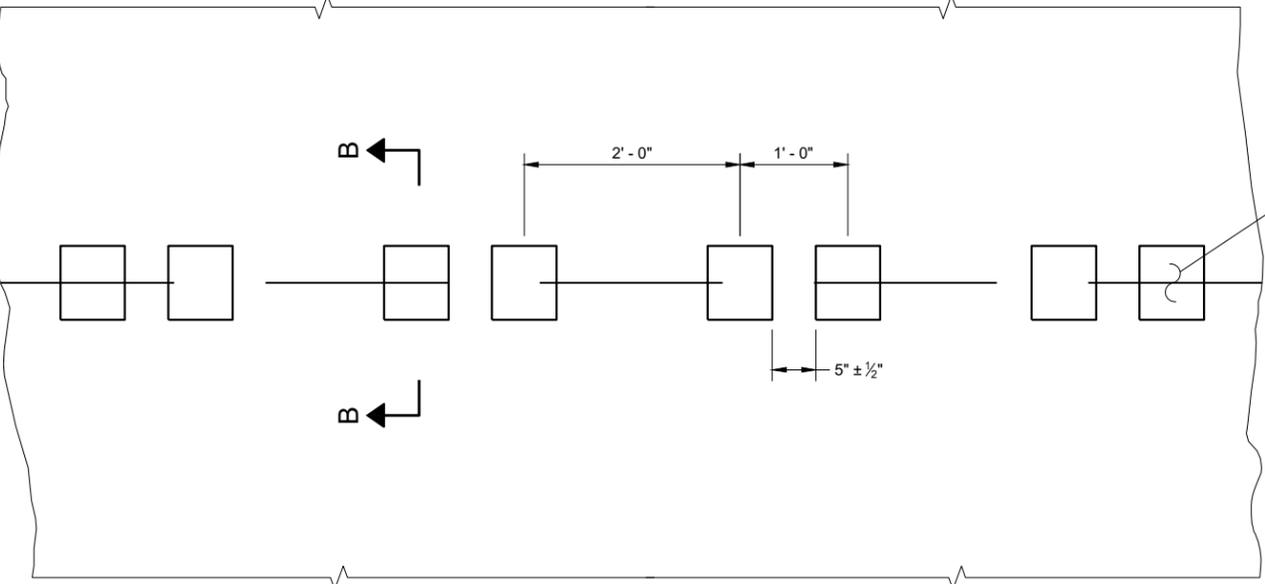
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL CENTERLINE GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

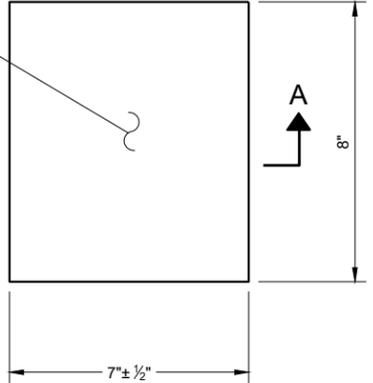
INSTALL PAVEMENT MARKING AFTER THE GROOVES ARE INSTALLED.

SEE SIGNING PLAN FOR SIGN REQUIREMENTS THAT MAY BE NEEDED.

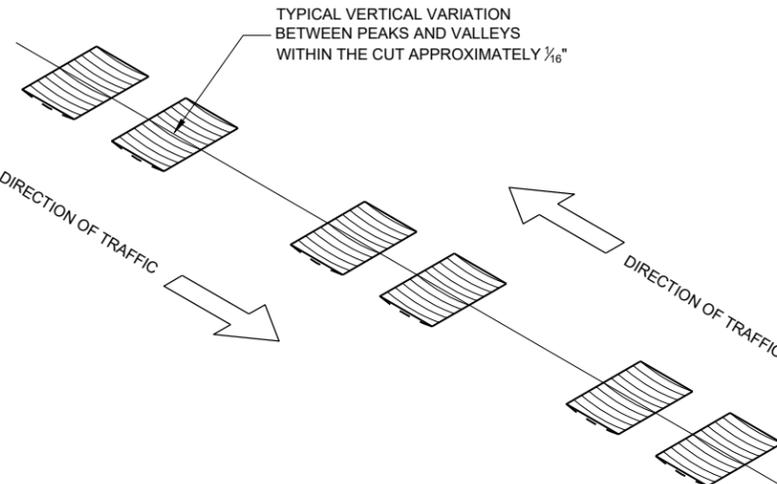
- ① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



**PLAN VIEW  
SHOULDER WITH GROOVES**

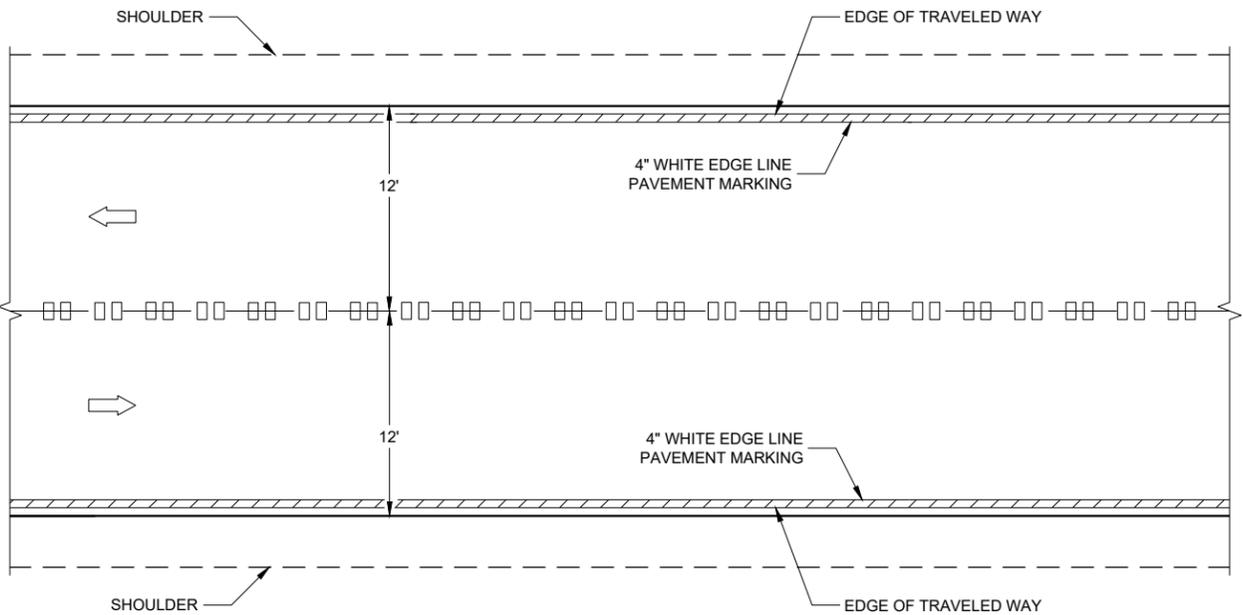


**PLAN VIEW  
(SINGLE GROOVE)**

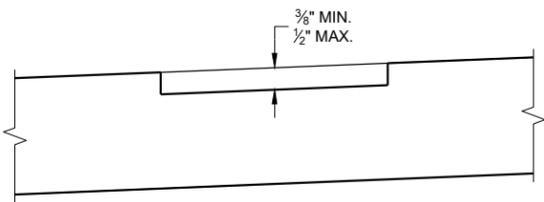


**ISOMETRIC**

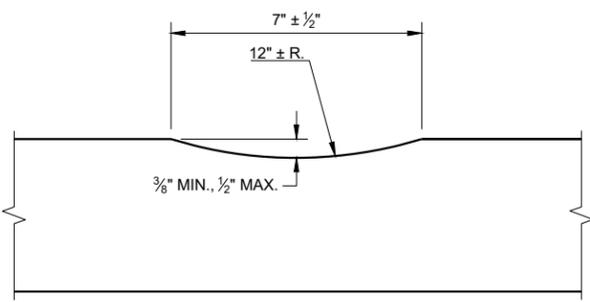
**PLACEMENT DETAIL FOR TYPE 1 MILLED RUMBLE STRIP**



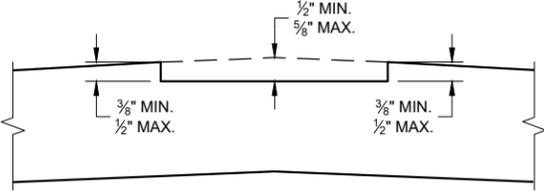
**CENTERLINE GROOVES ON TWO-WAY ROADWAYS**



**SECTION B - B  
SUPERELEVATED ROADWAY**

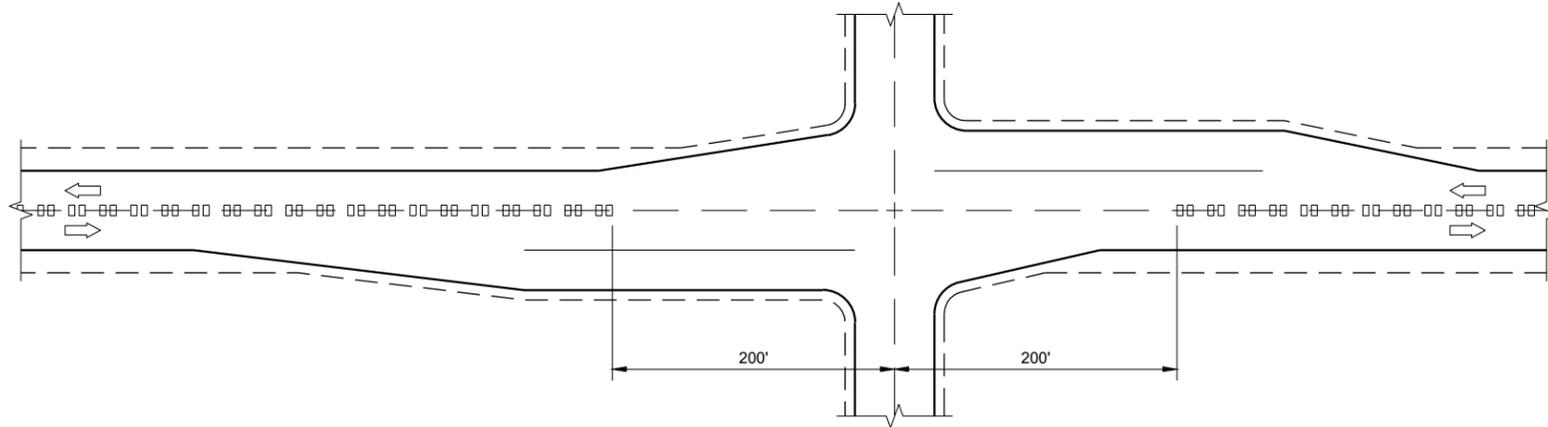


**SECTION A - A**

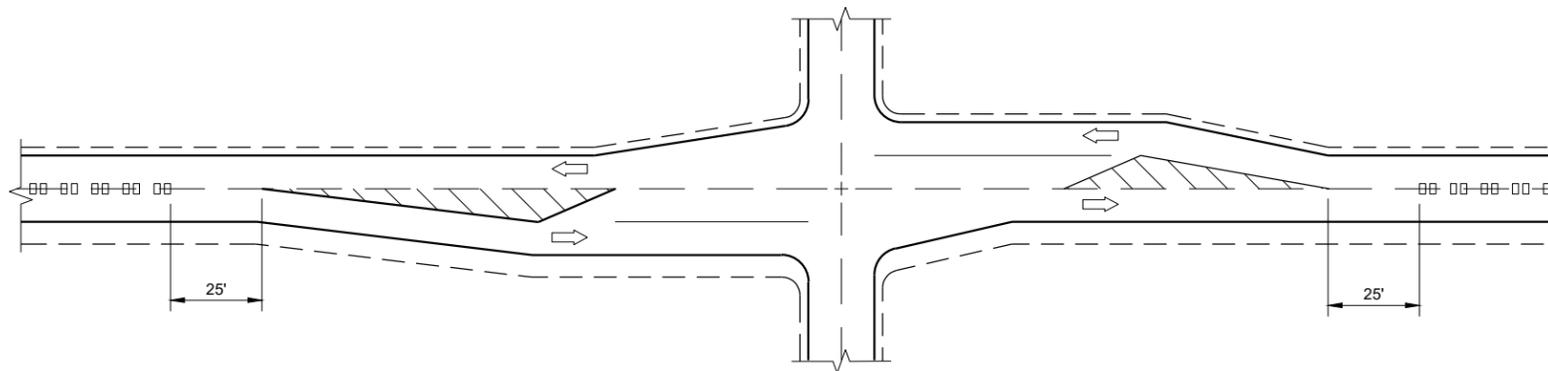


**SECTION B - B  
CROWNED ROADWAY**

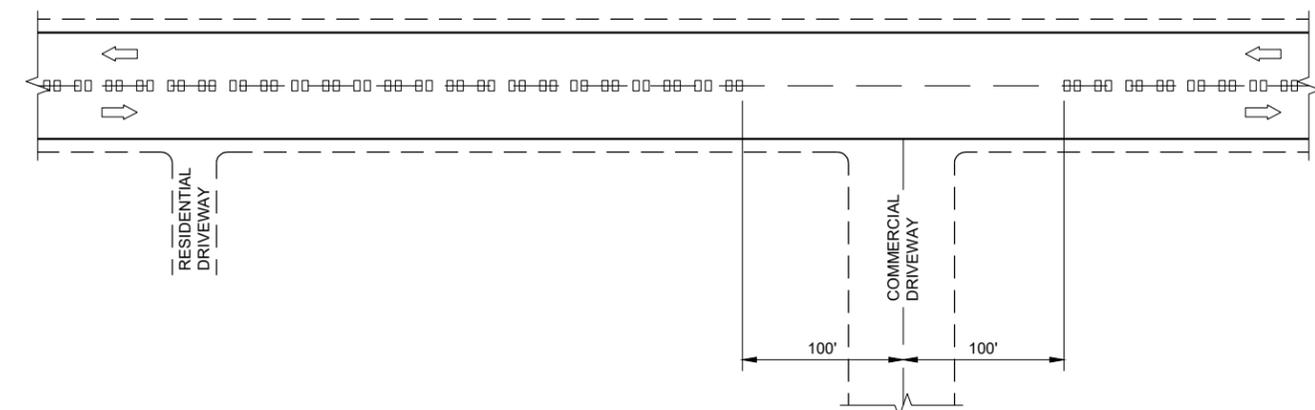
<p><b>2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING</b></p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>



**CENTERLINE GROOVES AT INTERSECTIONS**



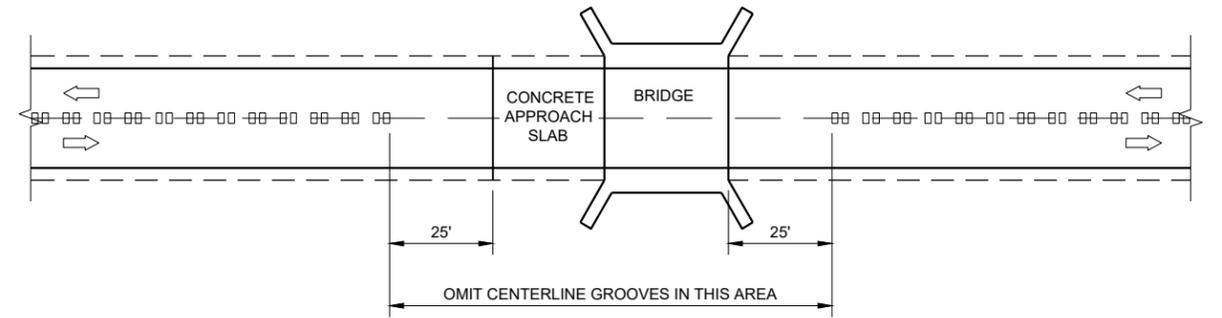
**CENTERLINE GROOVES AT INTERSECTIONS  
(WITH LEFT TURN LANES)**



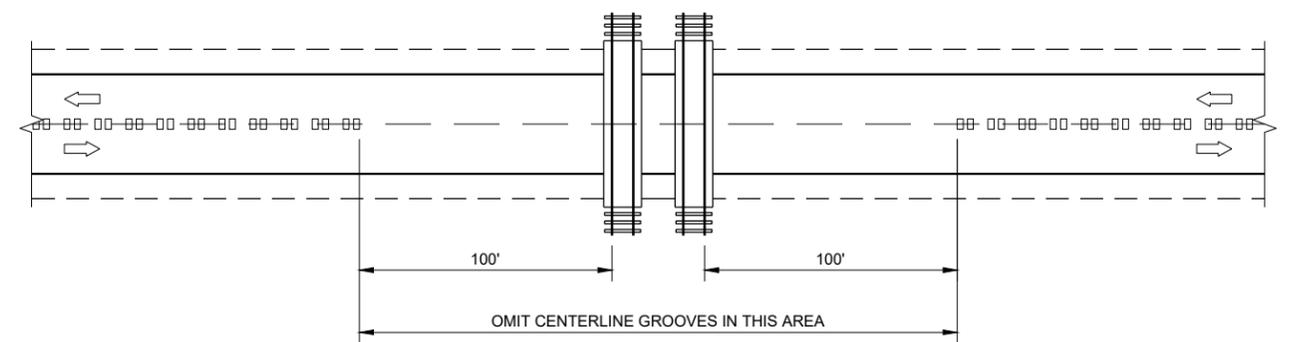
**CENTERLINE GROOVES AT DRIVEWAYS** ①

**GENERAL NOTES**

- ① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



**CENTERLINE GROOVES AT BRIDGES**

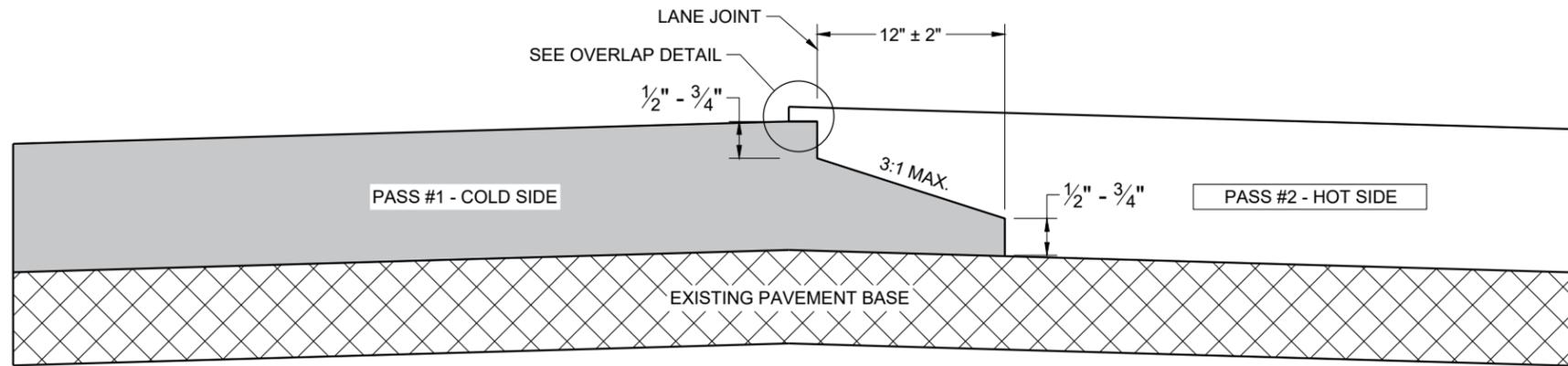


**CENTERLINE GROOVES AT RAILROADS**

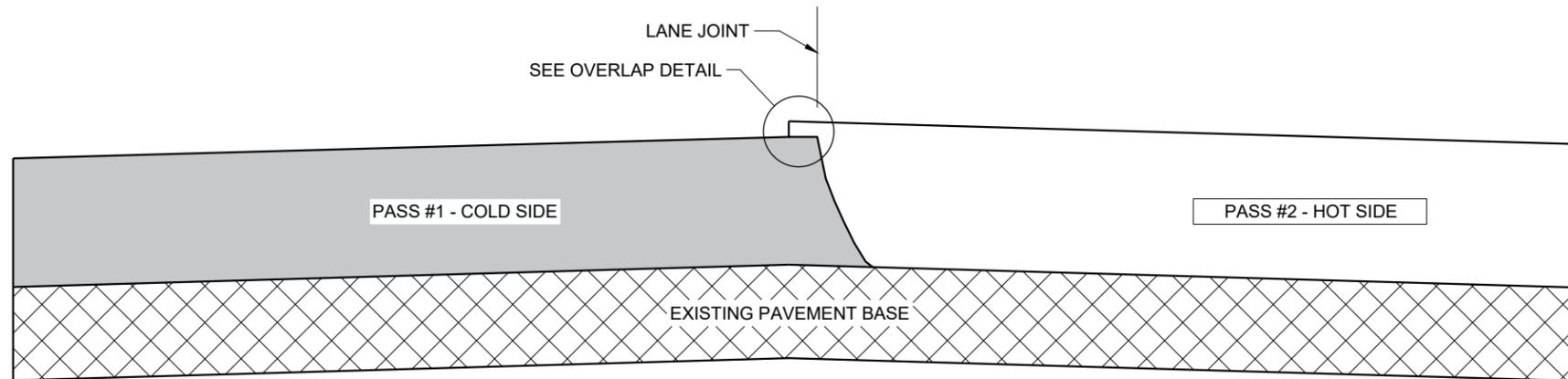
6

6

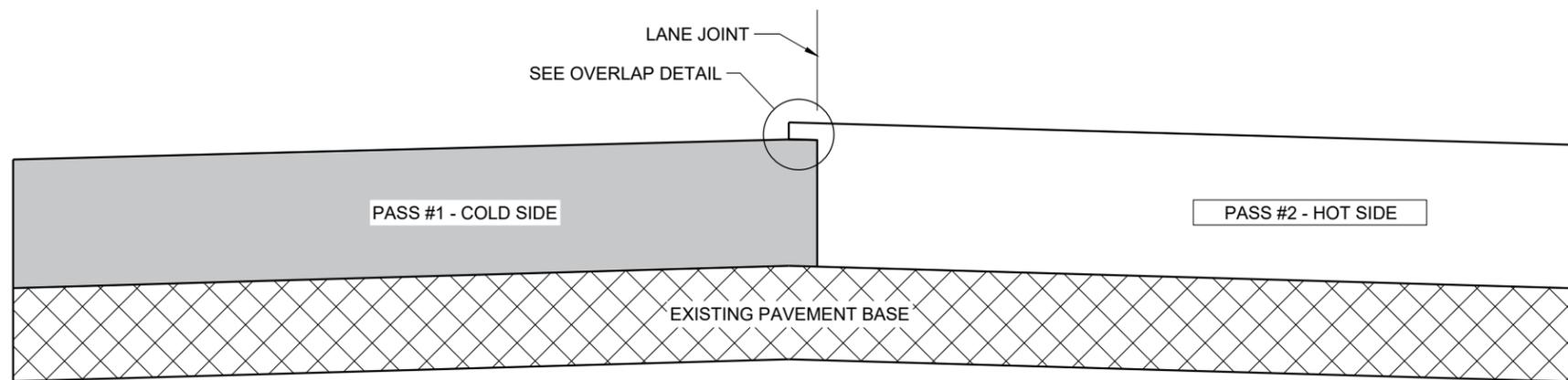
<b>2-LANE RURAL CENTERLINE RUMBLE STRIP, MILLING</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 7/2018 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



**TYPICAL PAVEMENT CROSS SECTION  
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION  
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION  
VERTICAL JOINT (MILLED)**

**GENERAL NOTES**

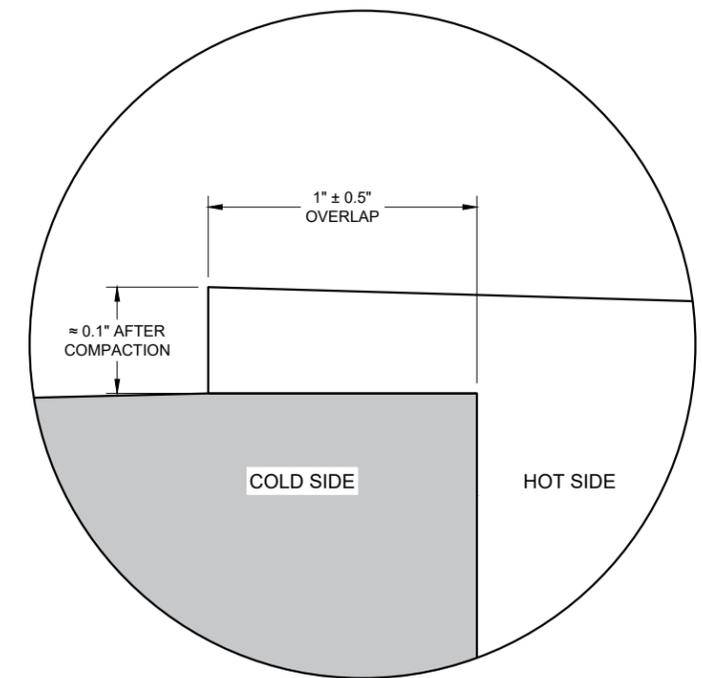
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY  $1" \pm 0.5"$  AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY  $0.1"$  AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO  $2"$  FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



**OVERLAP DETAIL (TYPICAL)**

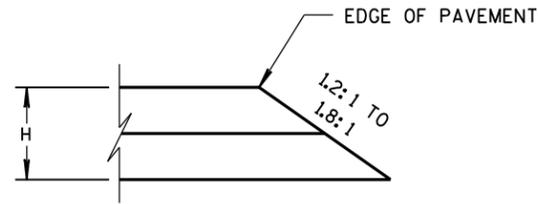
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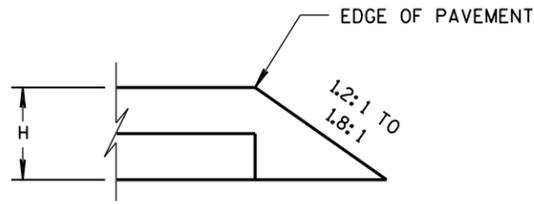
SDD 13C19 - 03

SDD 13C19 - 03

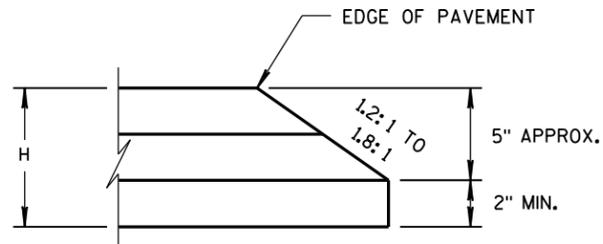
<b>HMA LONGITUDINAL JOINTS</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	



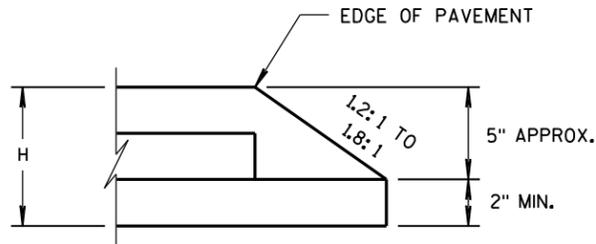
CONSTRUCTED WITH FINAL TWO LAYERS  
FOR H 5" OR LESS



CONSTRUCTED WITH FINAL LAYER  
FOR H 5" OR LESS

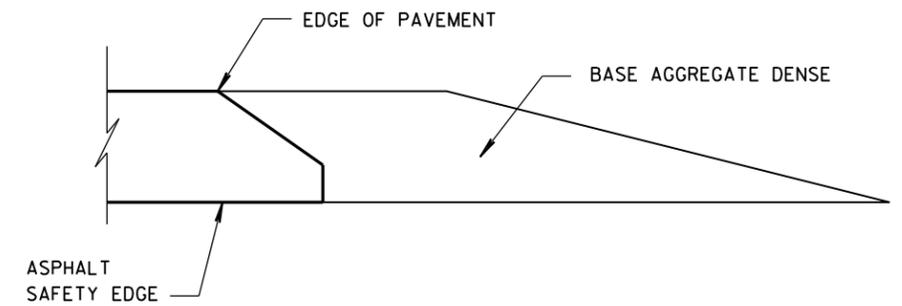


CONSTRUCTED WITH FINAL TWO LAYERS  
FOR H GREATER THAN 5"



CONSTRUCTED WITH FINAL LAYER  
FOR H GREATER THAN 5"

HMA PAVEMENT AND HMA OVERLAYS



FINISHED SHOULDER AGGREGATE PLACEMENT

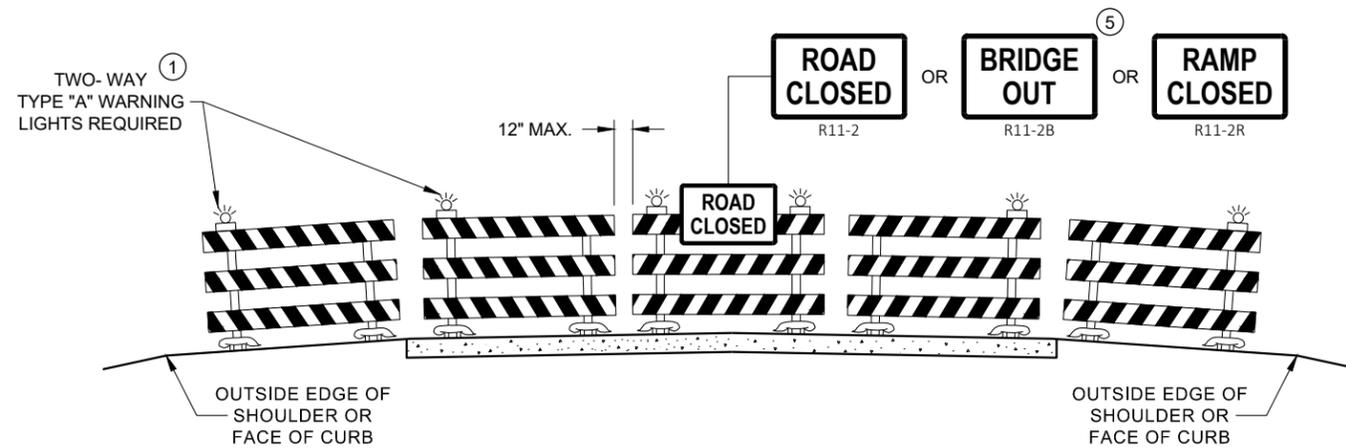
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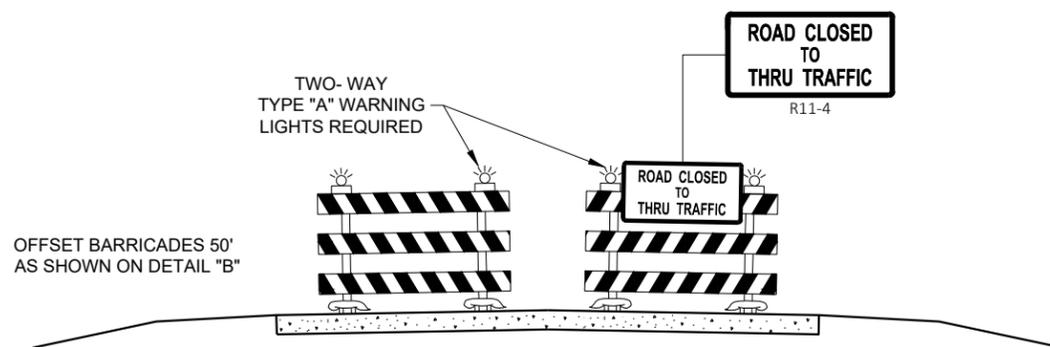
S.D.D. 14 B 29-1

S.D.D. 14 B 29-1

SAFETY EDGE <sub>SM</sub>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE	/s/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



**DETAIL D  
ROAD CLOSURE BARRICADE DETAIL  
APPROACH VIEW**



**DETAIL E  
LANE CLOSURE BARRICADE DETAIL  
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS  
FOR  
VARIOUS CLOSURES**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2020 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

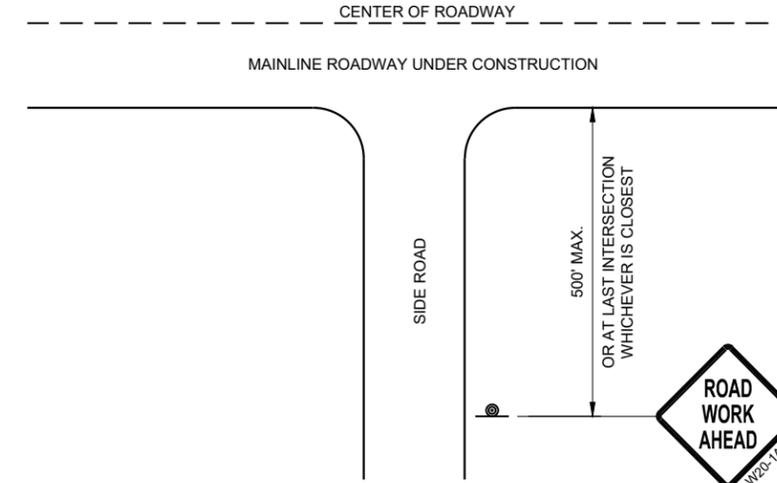
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

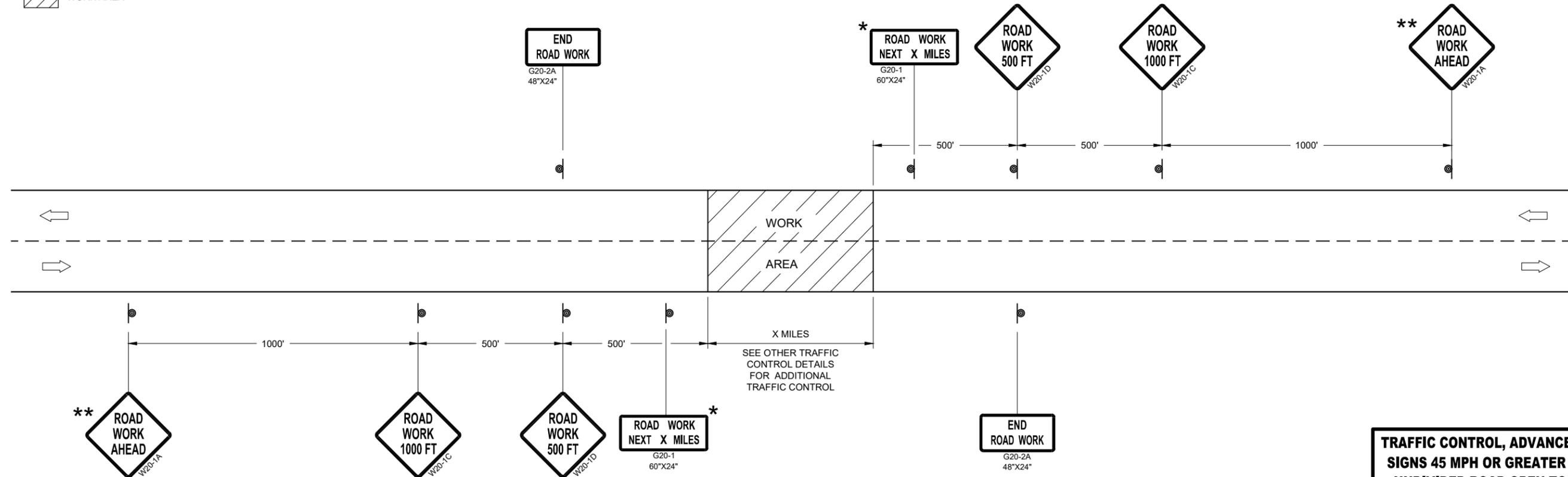
- \* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS
- \*\* PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

**LEGEND**

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH  
WARNING SIGN DETAIL**



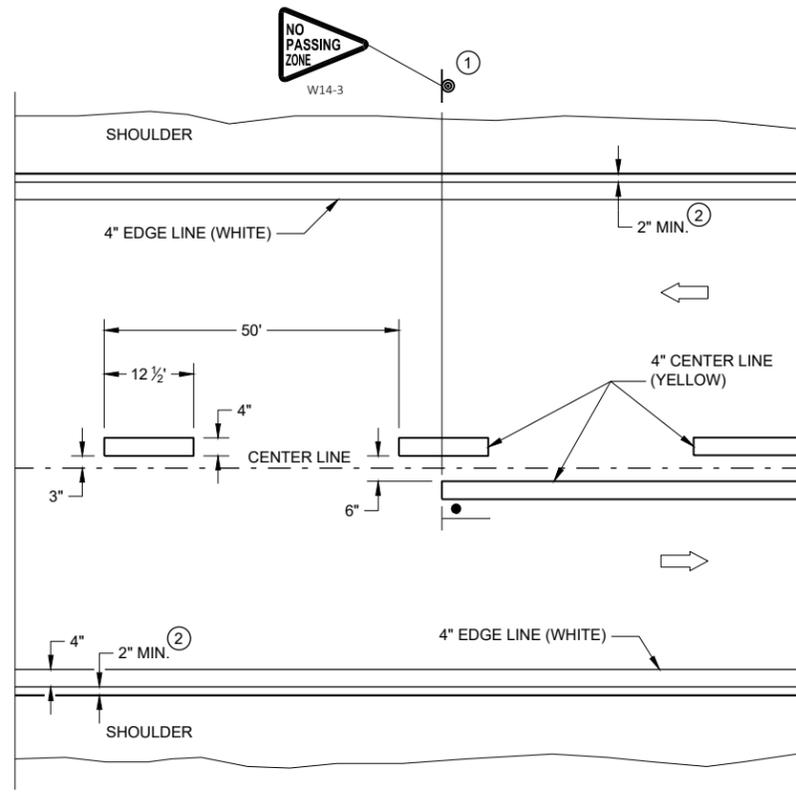
**TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER**

**TRAFFIC CONTROL, ADVANCE WARNING  
SIGNS 45 MPH OR GREATER TWO-WAY  
UNDIVIDED ROAD OPEN TO TRAFFIC**

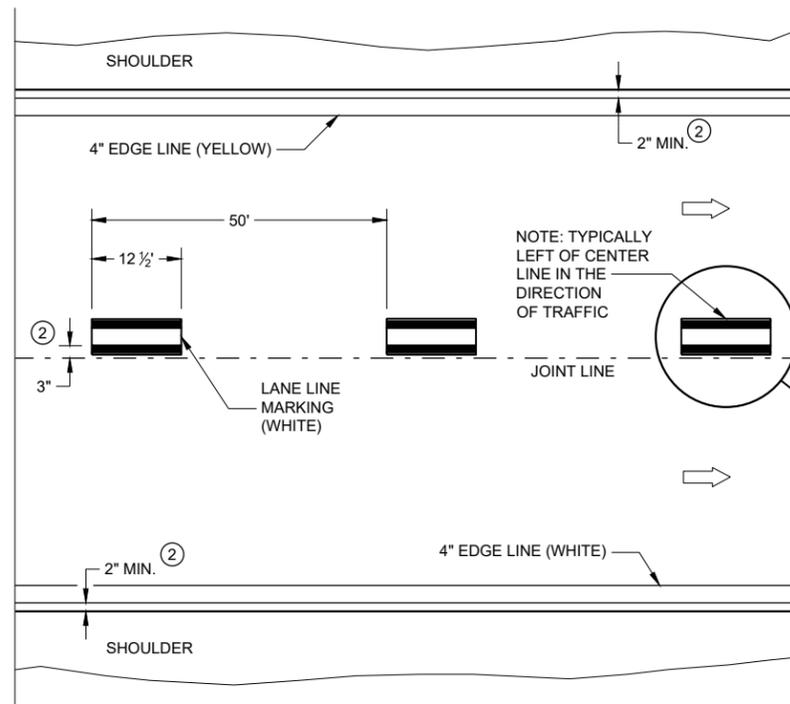
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED \_\_\_\_\_  
DATE July 2018 /S/ Andrew Heidtke  
WORK ZONE ENGINEER

FHWA

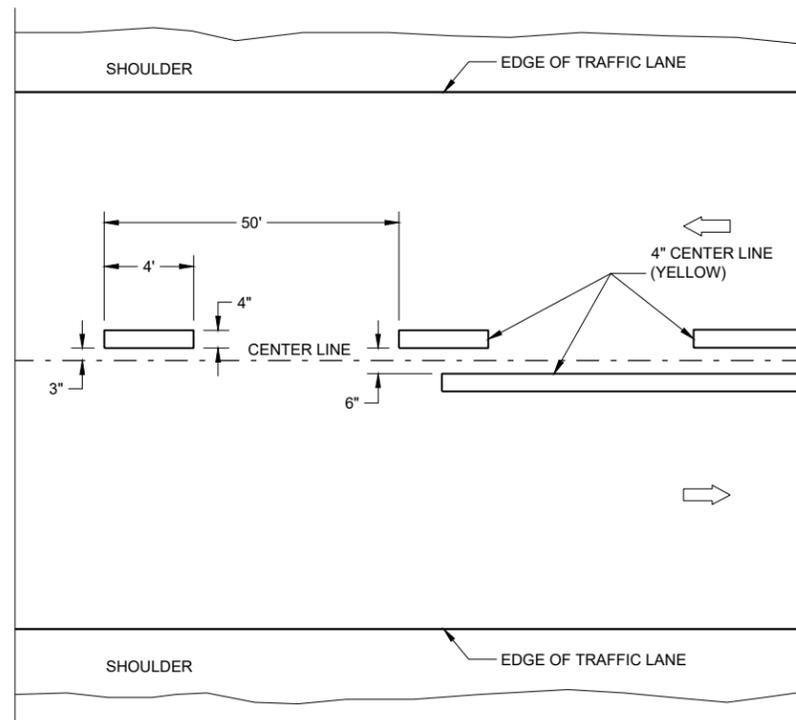


**TWO WAY TRAFFIC**

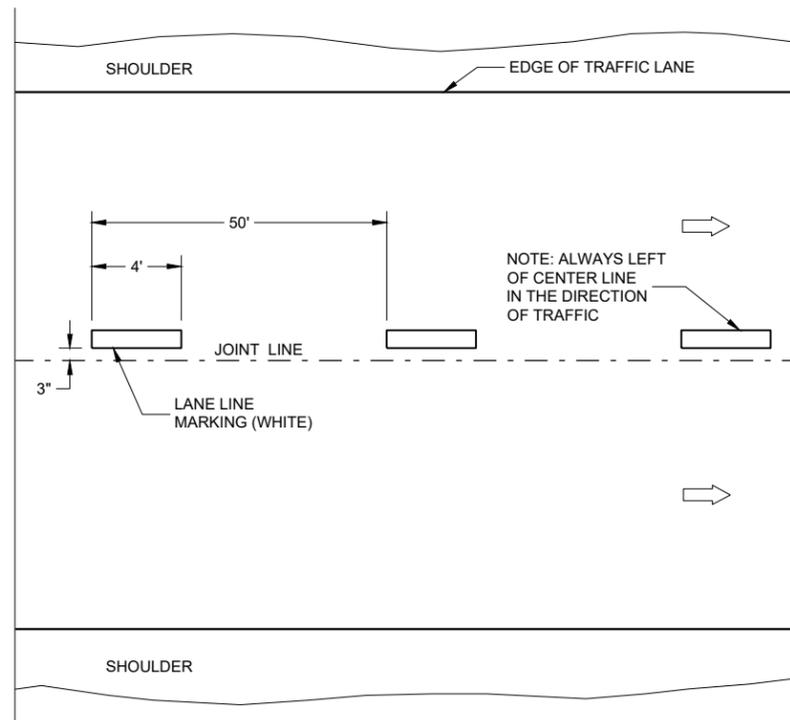


**ONE WAY TRAFFIC**

**PERMANENT PAVEMENT MARKING**



**TWO WAY TRAFFIC**



**ONE WAY TRAFFIC**

**TEMPORARY PAVEMENT MARKING**

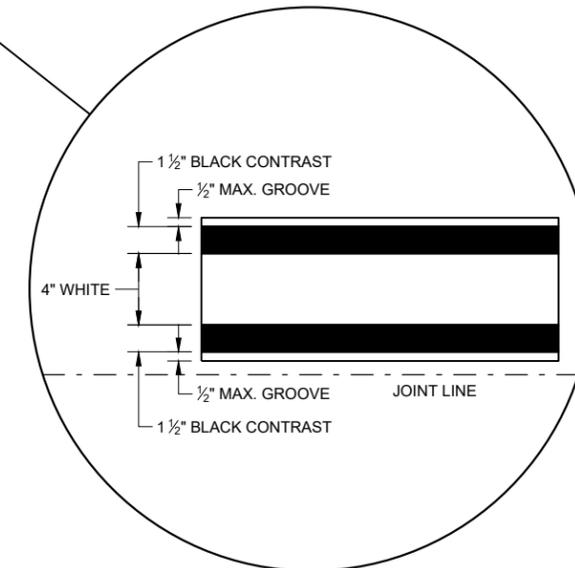
**GENERAL NOTES**

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

**LEGEND**

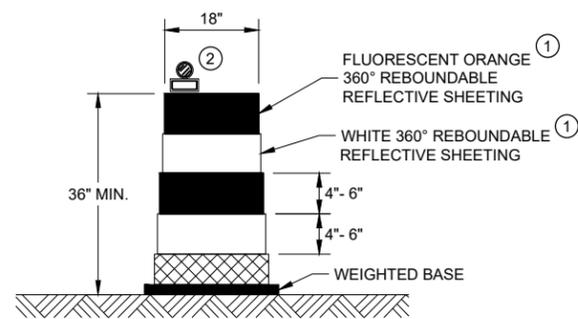
- |• "T" MARKING
- ⊙ SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC



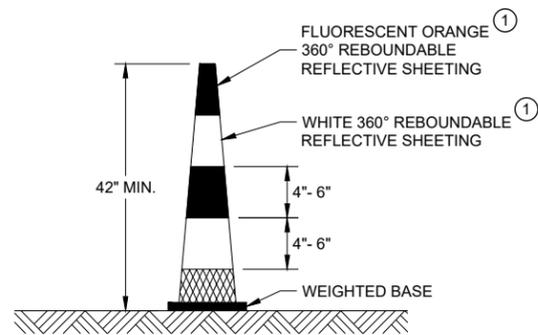
**LONGITUDINAL MARKING (MAINLINE)**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2020 /S/ Matthew Rauch  
DATE STATEWIDE SIGNING AND MARKING  
ENGINEER

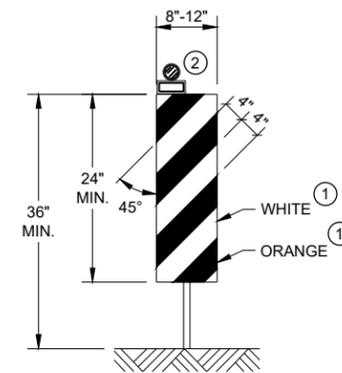


**DRUM**



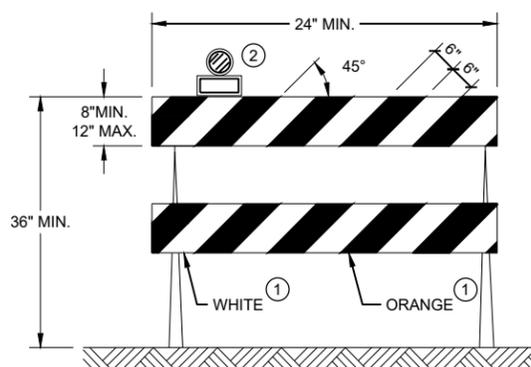
**42" CONE**

DO NOT USE IN TAPERS  
1/2 SPACING OF DRUMS



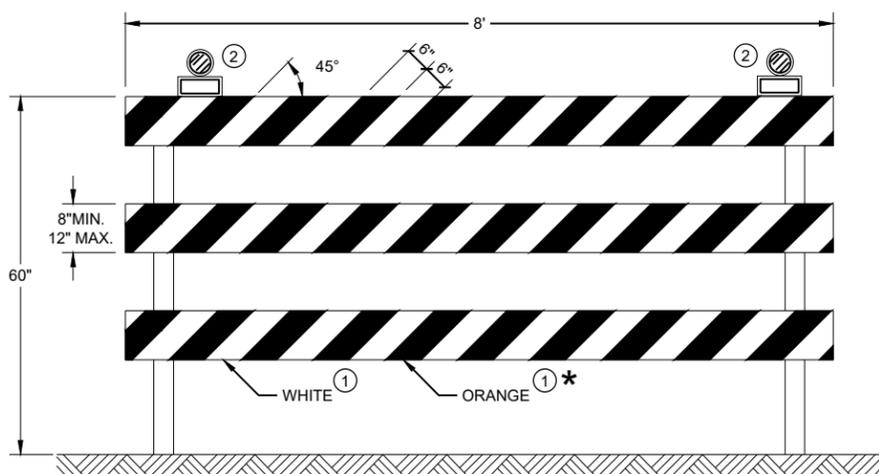
**VERTICAL PANEL**

THE STRIPES SHALL SLOPE DOWNWARD TO  
THE TRAFFIC SIDE FOR CHANNELIZATION.



**TYPE II BARRICADE**

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES  
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD  
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



**TYPE III BARRICADE**

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP  
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

\* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

**GENERAL NOTES**

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.

<b>CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2021 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

**LEGEND**

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

**GENERAL NOTES**

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

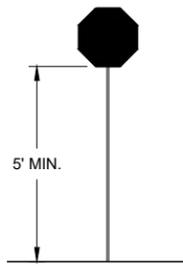
WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

**FLAGGING**

- FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.
- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
  - ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

**TEMPORARY PORTABLE RUMBLE STRIPS**

- UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.
- ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FOR THE APPROVED PRODUCTS LIST.
- INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.
- PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.
- DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



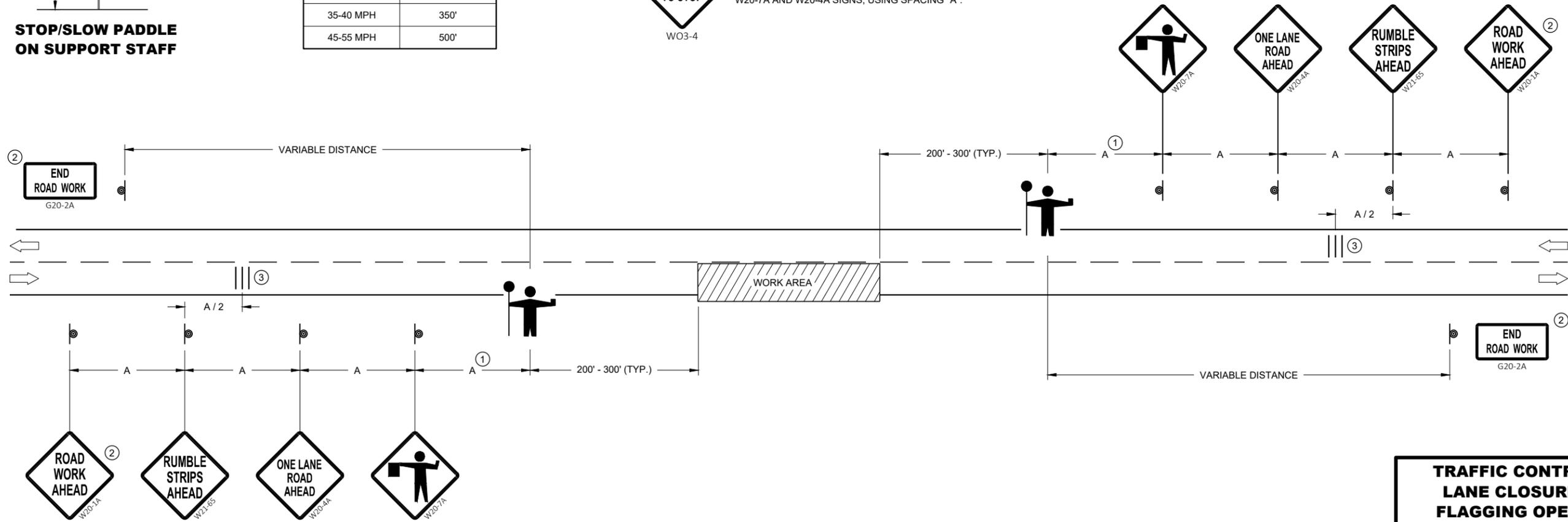
**STOP/SLOW PADDLE ON SUPPORT STAFF**

**SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE**

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



**TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION**

**TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE May 2019 /S/ Andrew Heidtke  
WORK ZONE ENGINEER

FHWA

**LEGEND**

- V1 LEAD VEHICLE
- V2 MARKING VEHICLE
- V3 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (CAUTION)

**GENERAL NOTES**

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

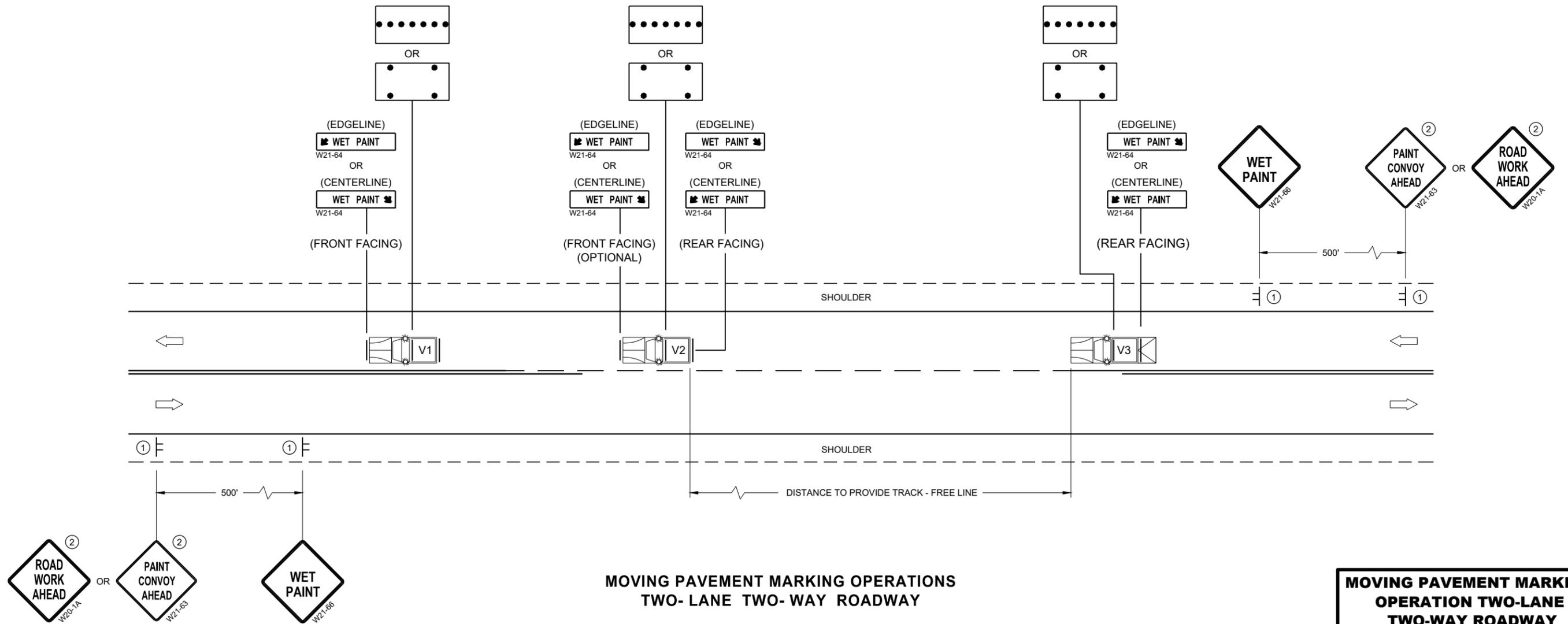
CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING.

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.

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**MOVING PAVEMENT MARKING OPERATIONS  
TWO-LANE TWO-WAY ROADWAY**

SDD 15C19 - 06a

SDD 15C19 - 06a

<b>MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2019 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

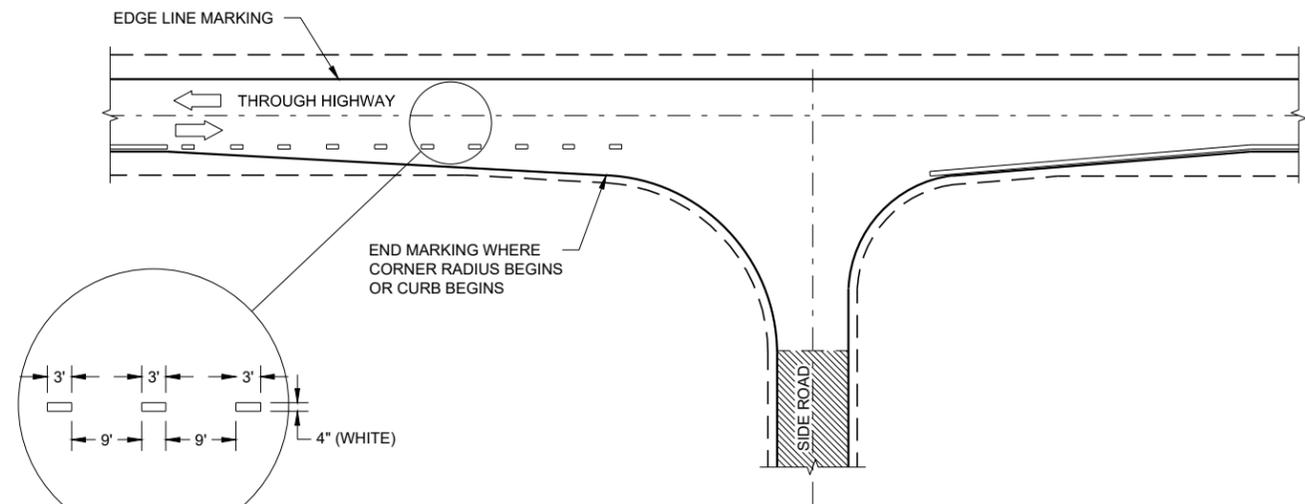
**GENERAL NOTES**

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

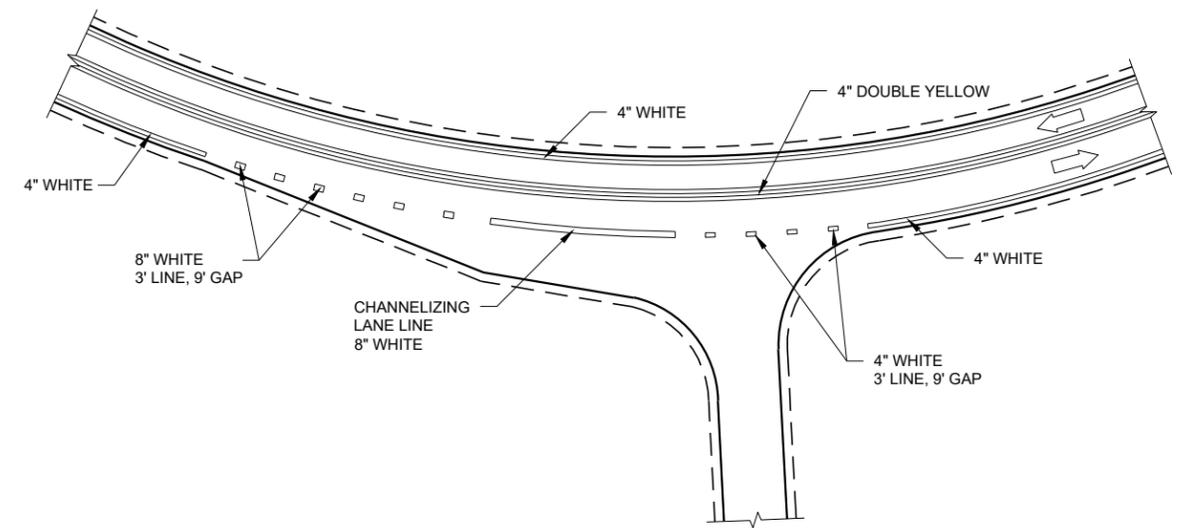
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.

**LEGEND**

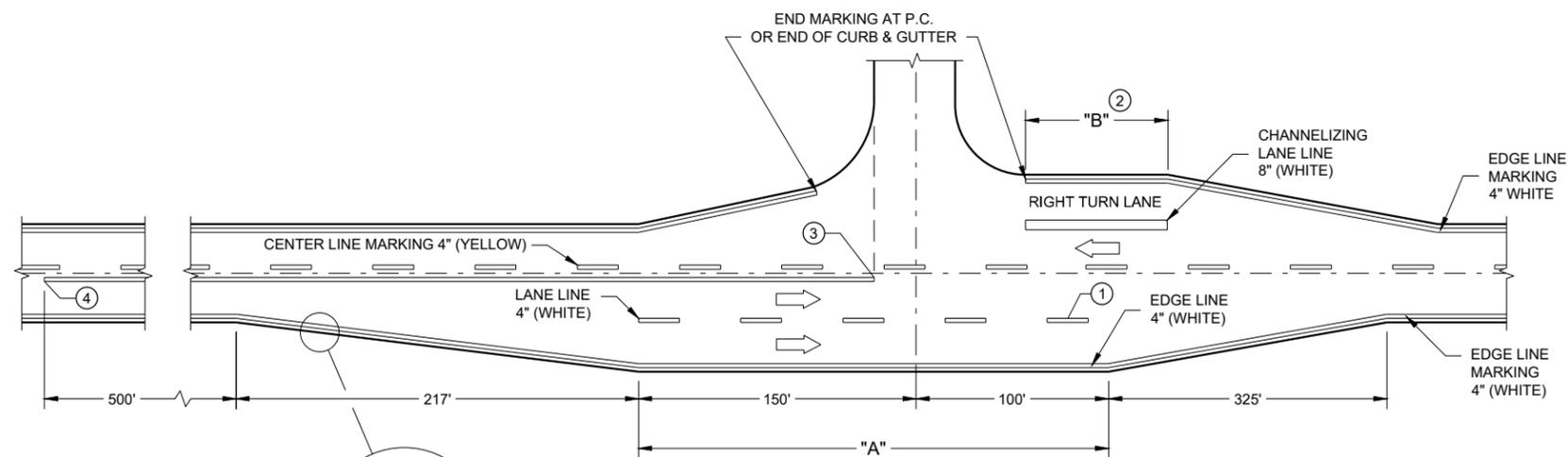
➡ DIRECTION OF TRAVEL



**MINOR INTERSECTION**



**INTERSECTION ON OUTSIDE OF CURVE**



**MAJOR INTERSECTIONS  
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**

**PAVEMENT MARKING  
(INTERSECTIONS)**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

**LEGEND**

-  SIGN ON TEMPORARY SUPPORT
-  SIGN ON PERMANENT SUPPORT
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  DIRECTION OF TRAFFIC
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

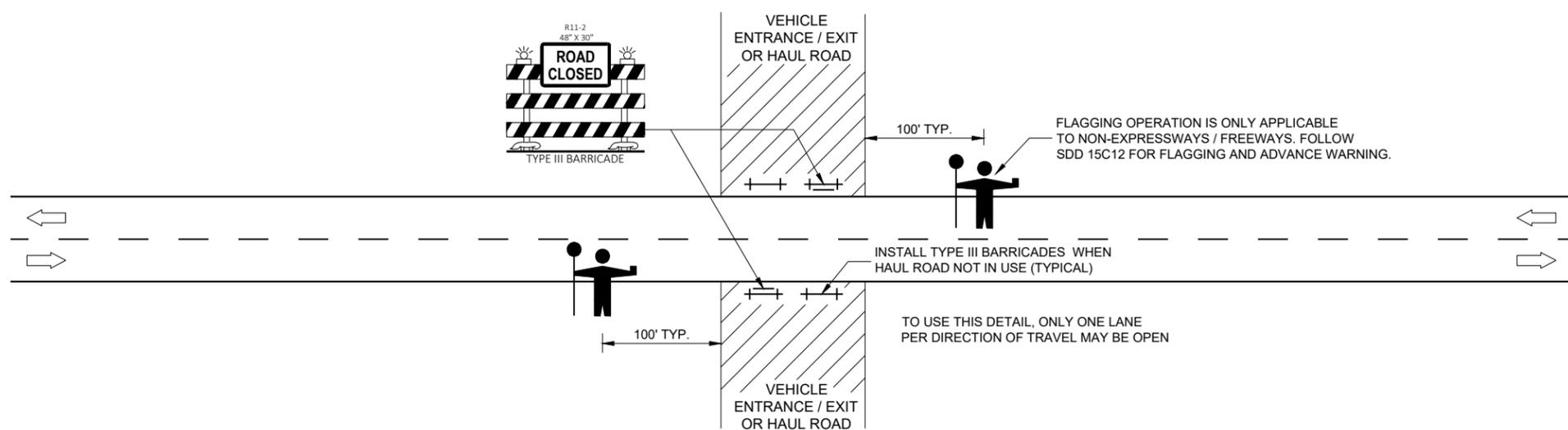
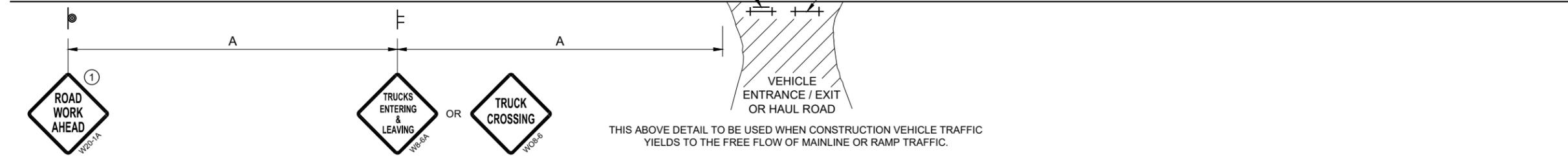
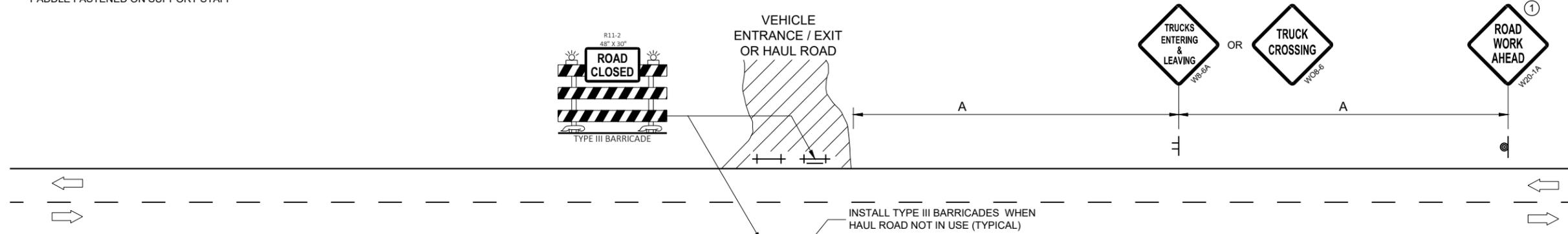
POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET
0-30	200'
35-40	350'
45-55	500'

**GENERAL NOTES**

- ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.
- "WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.
- THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- WARNING SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- WHEN ACTIVITY REFLECTED BY THE SIGN IS NOT CURRENTLY TAKING PLACE, THE HIGHWAY SHALL BE RESTORED TO NORMAL CONDITION AND THE SIGNS SHALL BE REMOVED, COVERED OR TURNED AWAY FROM TRAFFIC.
- WHEN A SIDE ROAD OR RAMP INTERSECTS WITHIN THE ADVANCE SIGNING AREA, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND / OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.
- PLACE SIGNS ON BOTH SIDES IF USED ON DIVIDED HIGHWAY.
- ① THESE SIGNS ARE TO BE USED ONLY WHEN VEHICLE ENTRANCE / EXIT CONDITIONS ARE SEPARATED BY MORE THAN TWO MILES FROM PREVIOUS WORK AREA OR SIGNING OR AS DIRECTED BY THE ENGINEER.

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THIS DETAIL TO BE USED WHEN CONSTRUCTION WORK INCLUDING TRUCKING ACTIVITY REQUIRES MAINLINE TRAFFIC TO BE TEMPORARILY STOPPED IN ONE OR BOTH DIRECTIONS. DELAY TO HIGHWAY TRAFFIC SHALL BE MINIMIZED.

FLAGGING OPERATION IS ONLY APPLICABLE TO NON-EXPRESSWAYS / FREEWAYS. FOLLOW SDD 15C12 FOR FLAGGING AND ADVANCE WARNING.

SDD 15D29 - 06

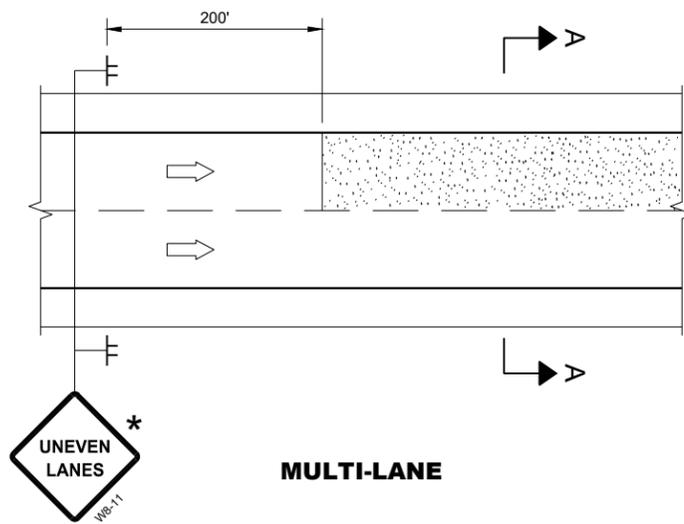
SDD 15D29 - 06

**TRAFFIC CONTROL,  
VEHICLE ENTRANCE/EXIT  
OR HAUL ROAD**

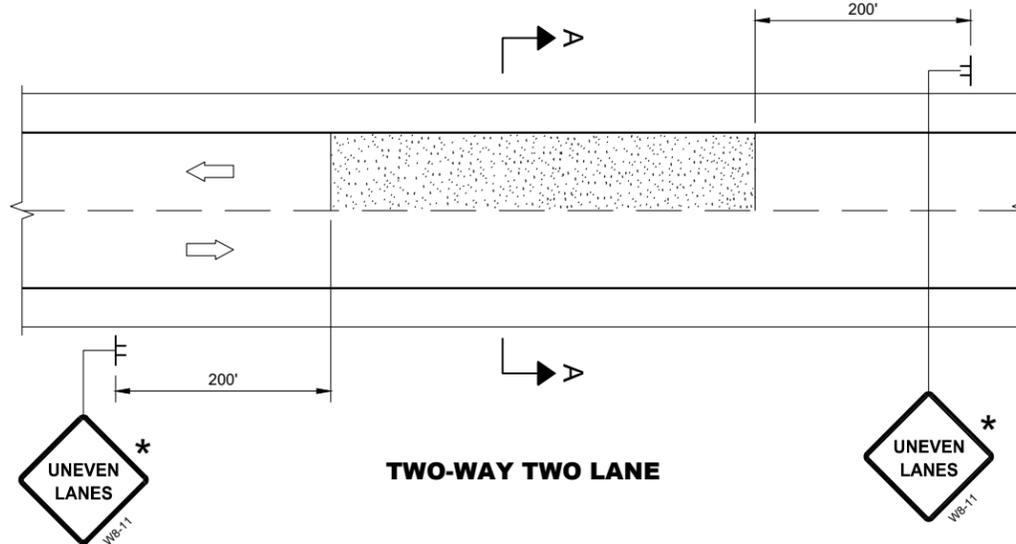
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED \_\_\_\_\_  
DATE May 2020 /S/ Andrew Heidtke  
WORK ZONE ENGINEER

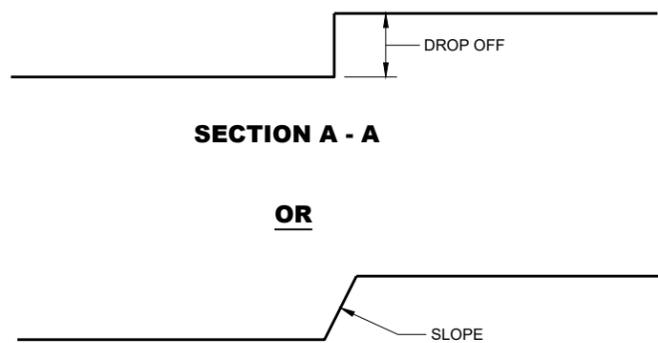
FHWA



**MULTI-LANE**



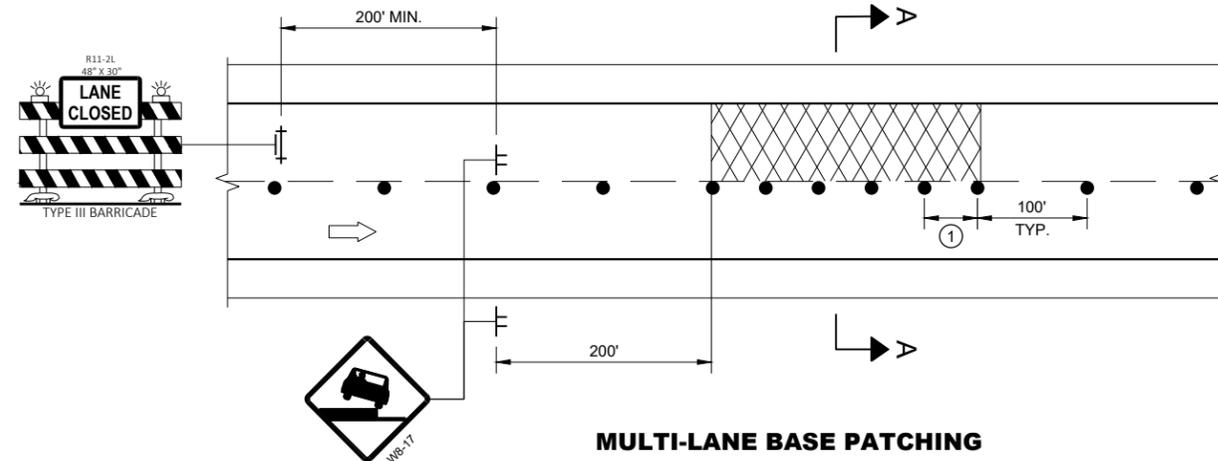
**TWO-WAY TWO LANE**



**SECTION A - A**

OR

**SECTION A - A**



**MULTI-LANE BASE PATCHING**

**ADJACENT LANE DROP-OFFS**

**GENERAL NOTES**

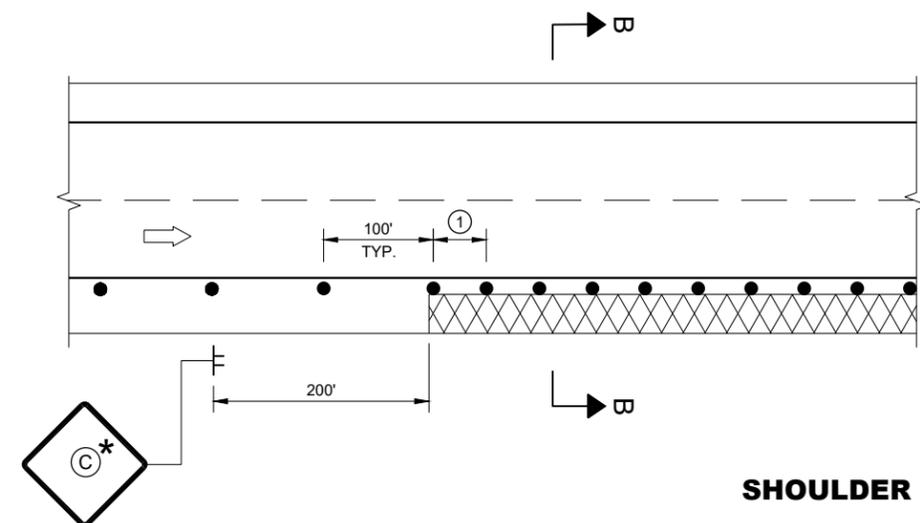
- FOR SPOT LOCATIONS USE ENGINEERING JUDGEMENT WHEN PLACING ADDITIONAL SIGNS.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.
- "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.
- \* IF THE DROP-OFF IS CONTINUOUS ALONG THE PROJECT, PLACE ADDITIONAL SIGNS EVERY 1 MILE AND AFTER EVERY ENTRANCE RAMP.
- ① USE CLOSER SPACING WHEN DELINEATING DROP-OFF.

**LEGEND**

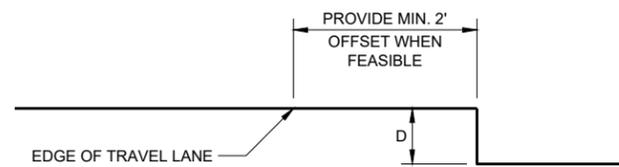
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- WORK AREA WITH DROP-OFF
- MILLED SURFACE

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**SHOULDER DROP-OFFS**



**SECTION B - B**

D	SIGN (C)
< 2" WITH A SLOPE STEEPER THAN 3:1	LOW SHOULDER WO8-9
2" < 6" WITH A SLOPE STEEPER THAN 3:1	SHOULDER DROP - OFF W8-9A PROVIDE A 3:1 OR FLATTER SLOPE OF MATERIAL ADJACENT TO THE PAVEMENT

SDD 15D39 - 02

SDD 15D39 - 02

**TRAFFIC CONTROL,  
DROP-OFF SIGNING**

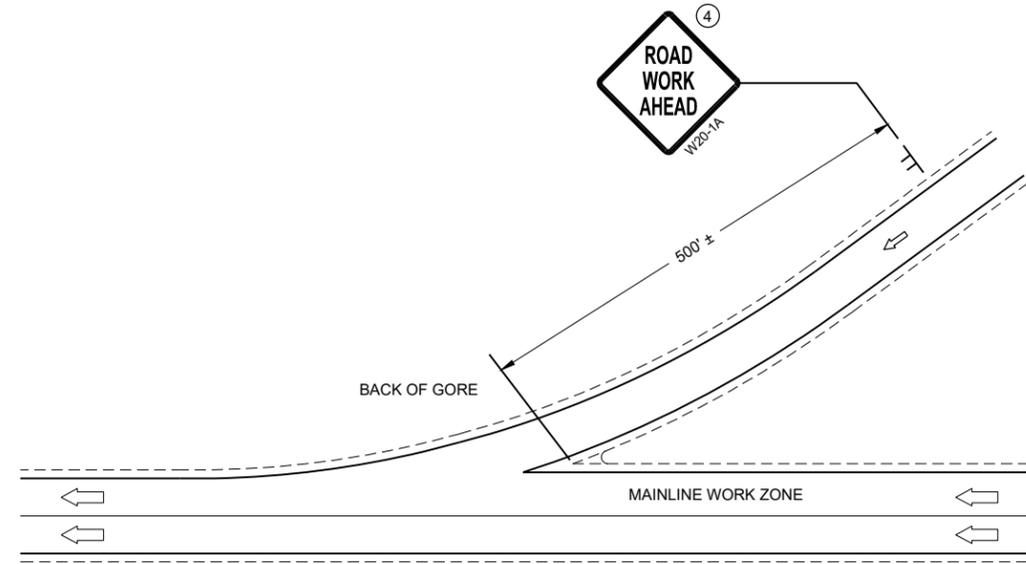
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
March 2018 /S/ Andrew Heidtke  
DATE DATE WORK ZONE ENGINEER

FHWA

**LEGEND**

- V1 MARKING VEHICLE
- V2 SHADOW VEHICLE
- V3 TRAIL VEHICLE
- TRAFFIC CONTROL DRUM
- ◻ TRUCK MOUNTED ATTENUATOR (TMA)
- ⊥ SIGN ON TEMPORARY SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ◻ FLASHING ARROW PANEL (MERGE)
- ◻ FLASHING ARROW PANEL (CAUTION)
- PCMS PORTABLE CHANGEABLE MESSAGE SIGN
- ▨ WORK AREA



**GENERAL NOTES**

SHORT DURATION IS WORK THAT OCCUPIES A LOCATION UP TO 1 HOUR.

MOBILE IS WORK THAT MOVES INTERMITTENTLY OR CONTINUOUSLY.

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

WHEN WORK ACTIVITY BLOCKS THE RIGHT LANE, REVERSE TRAFFIC CONTROL.

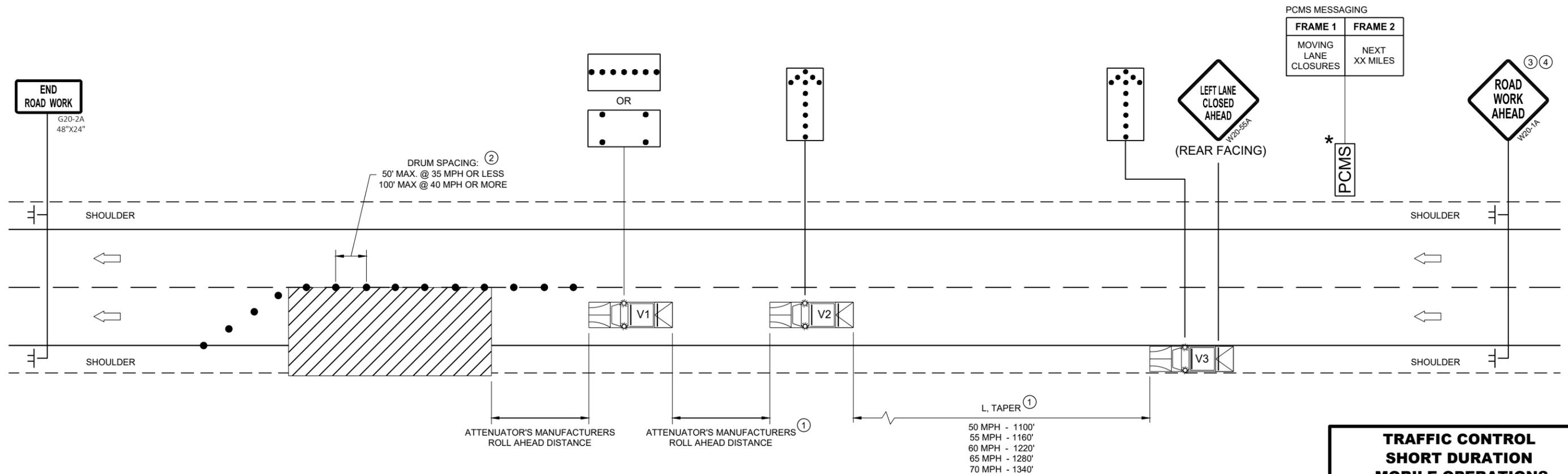
WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.

USE DOUBLE ARROWS WHEN CONVOY IS IN CENTER LANE ONLY.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC

- ① DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
  - ② DRUMS ARE TO BE USED FOR BRIDGE DECK SEALING AND OTHER PROJECTS THAT REQUIRE DELINEATION.
  - ③ WITHIN 5 MILES, RELOCATE SIGNS AS WORK PROGRESSES AND NECESSARY OR AS DIRECTED BY THE ENGINEER.
  - ④ SIGN NOT REQUIRED IF MOVING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- \* PCMS OPTIONAL

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SDD 15D43 - 01

SDD 15D43 - 01

**TRAFFIC CONTROL  
SHORT DURATION  
MOBILE OPERATIONS**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
August 2019 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

FHWA

**GENERAL NOTES**

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

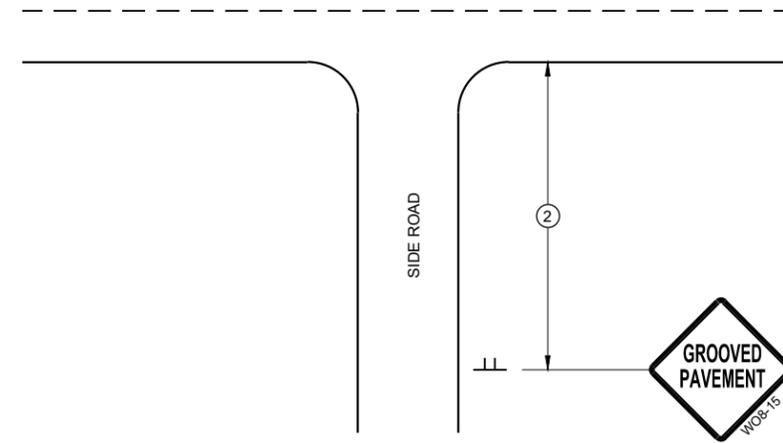
ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

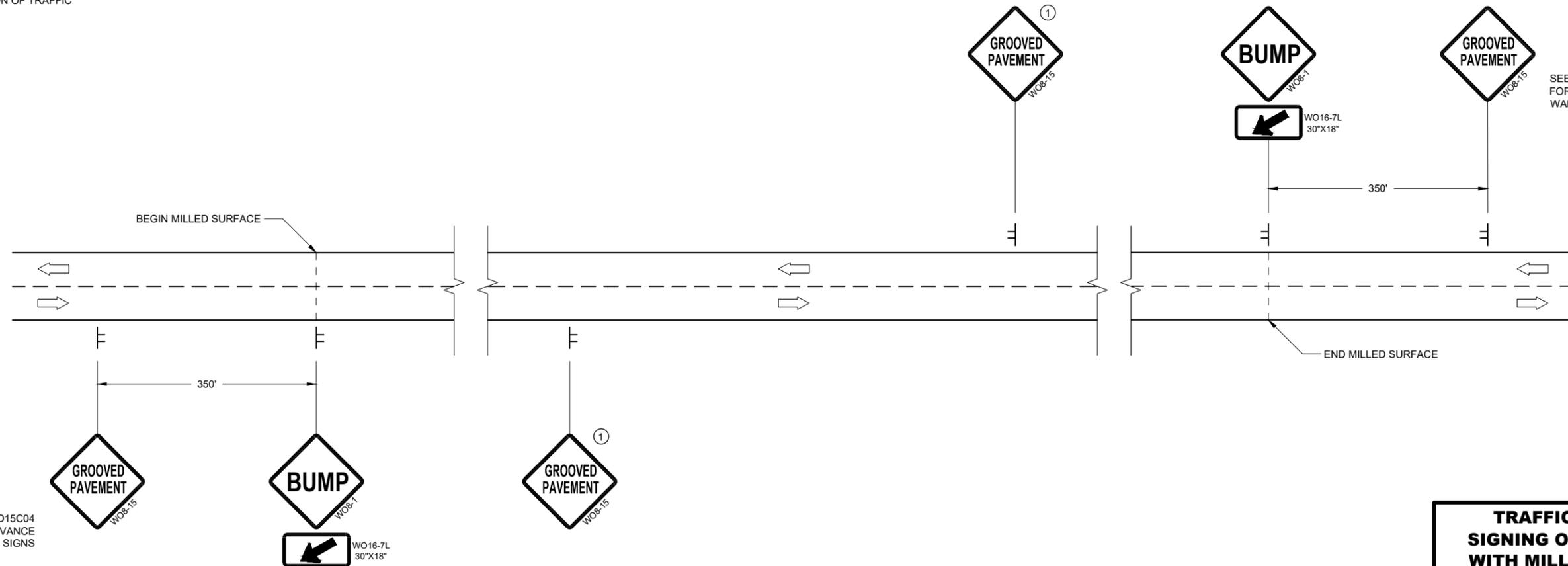
- ① PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- ② PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

**LEGEND**

-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC



**TYPICAL SIDE ROAD APPROACH SIGN DETAIL**



SEE SDD15C04 FOR ADVANCE WARNING SIGNS

SEE SDD15C04 FOR ADVANCE WARNING SIGNS

**DETAIL FOR SIGNING ON MILLED SURFACES**

**TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2020 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

FHWA

**LEGEND**

- V1 WORK VEHICLE
- V2 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  FLASHING ARROW PANEL (CAUTION)
-  WORK AREA
-  DIRECTION OF TRAFFIC

POSTED SPEED PRIOR TO WORK STARTING (MPH)	DECISION SIGHT DISTANCE (D)
0 - 25	550'
30	550'
35	700'
40	700'
45	900'
50	900'
55	1200'

**GENERAL NOTES**

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

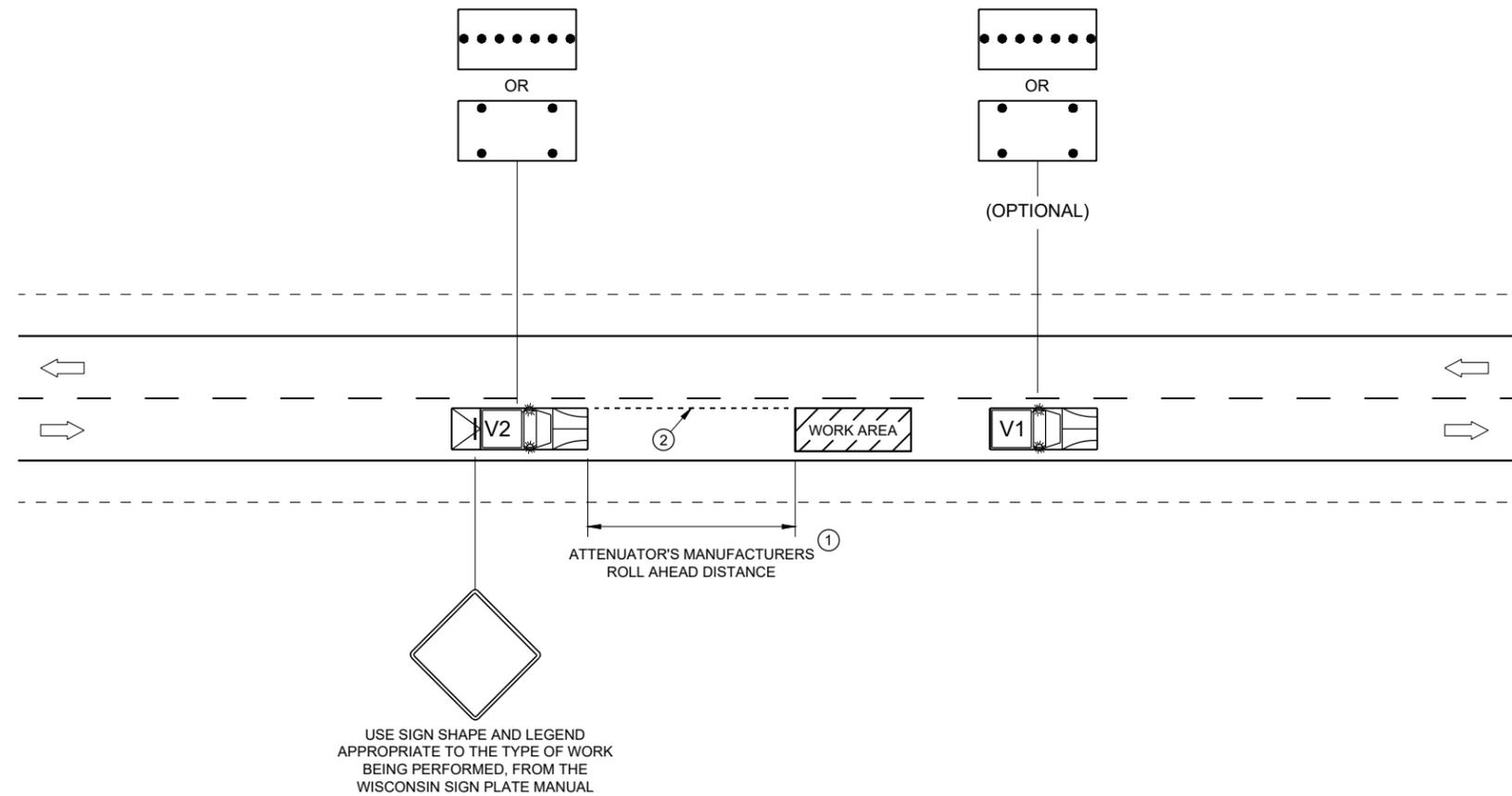
MOBILE IS WORK THAT MOVES CONTINUOUSLY OR MOVES AT LEAST THE DECISION SIGHT DISTANCE EVERY 15 MINUTES.

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL ARROW PANELS SHALL BE REAR FACING, TYPE "B" OR "C", AND DISPLAYING THE FLASHING CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

- ① DISTANCE BETWEEN VEHICLES MAY INCREASE FROM THE ATTENUATOR'S ROLL AHEAD BASED ON TERRAIN, SIGHT DISTANCE, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ② ALIGN LEFT SIDE OF SHADOW VEHICLE WITH EDGE OF WORK AREA.



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**TRAFFIC CONTROL,  
MOBILE OPERATIONS ON  
AN UNDIVIDED ROADWAY**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2021 /S/ Andrew Heidtke  
DATE STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

FHWA

### GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAILED DRAWINGS OF PROPOSED ALTERNATE DESIGNS FOR METAL MONUMENTS OR MONUMENT COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

PERMANENT MAGNETS SHALL BE INSERTED NEAR THE TOP AND BOTTOM OF ALL ALUMINUM MONUMENTS SO THE MONUMENT CAN EASILY BE DETECTED BY A METAL DETECTOR.

THE CAST IRON MONUMENT COVER SHALL BE A "NON-ROCKING" TYPE. ADJUSTMENT OF THE COVER TO GRADE MAY BE ACCOMPLISHED BY THE USE OF MORTAR AND BRICK, OR BY EITHER PRECAST OR CAST-IN-PLACE REINFORCED CONCRETE GRADE RINGS.

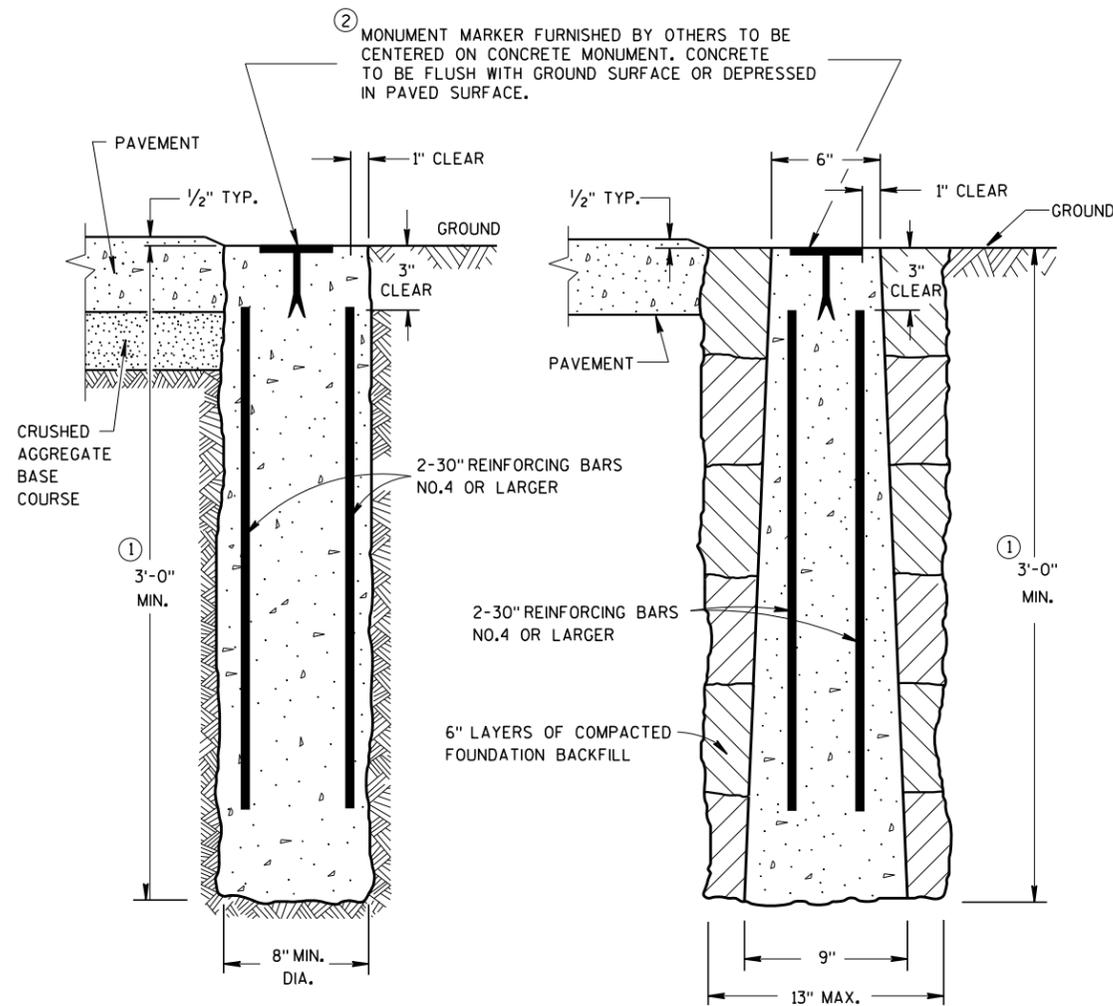
MONUMENTS SHALL BE LOCATED AND PLACED AT THE DIRECTION OF THE ENGINEER.

ALUMINUM MONUMENTS AND MONUMENT COVERS SHALL BE MADE FROM AN ALUMINUM AND MAGNESIUM ALLOY AS DETERMINED BY THE MANUFACTURER.

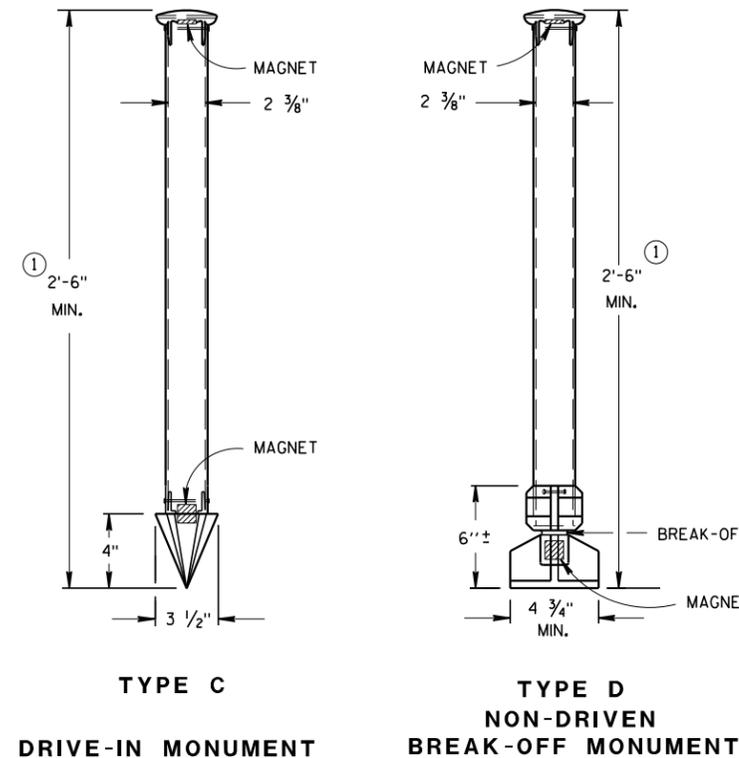
THE MONUMENT COVERS DETAILED ON THIS DRAWING ARE NOT EQUAL ALTERNATES. MONUMENT COVERS SHALL BE CAST IRON UNLESS ALUMINUM IS SPECIFIED ELSEWHERE IN THE CONTRACT.

MONUMENT SHALL BE CAST-IN-PLACE CONCRETE UNLESS PRECAST CONCRETE OR ALUMINUM MONUMENTS ARE SPECIFIED IN THE CONTRACT OR PERMITTED BY THE ENGINEER

- ① MINIMUM LENGTH SHALL BE 4'-0" FOR MONUMENTS INSTALLED IN PAVED AREAS.
- ② AN OFFICIAL COUNTY MONUMENT MARKER SUPPLIED BY A COUNTY MAY BE REQUIRED FOR SOME SECTION CORNERS AND WITNESS MONUMENTS INSTEAD OF THIS WIS DOT MARKER.



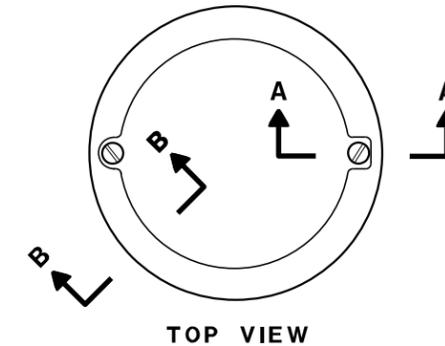
**CAST-IN-PLACE  
CONCRETE MONUMENTS  
TYPE A**



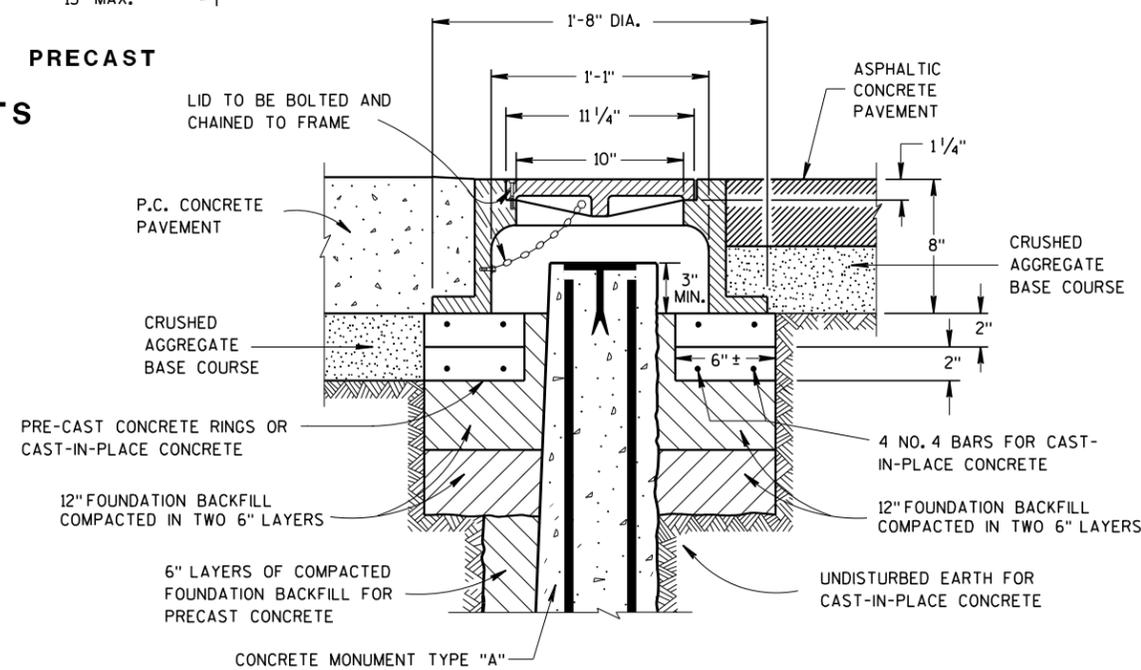
**TYPE C  
DRIVE-IN MONUMENT**

**TYPE D  
NON-DRIVEN  
BREAK-OFF MONUMENT**

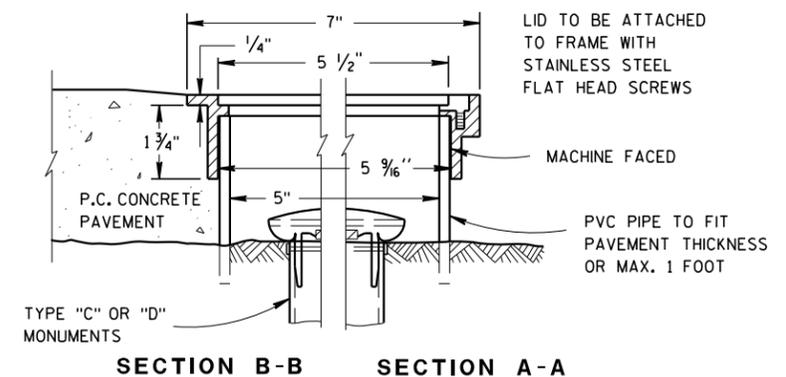
**ALUMINUM MONUMENTS**  
(INCLUDES MARKER)



**TOP VIEW**

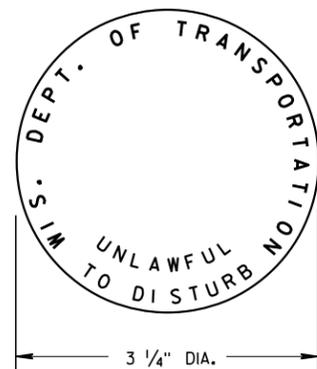


**CAST IRON MONUMENT COVER**  
(APPROXIMATE WEIGHT 95 LBS)



**ALUMINUM MONUMENT COVER**

(APPROXIMATE WEIGHT 2 LBS)  
(FOR CONCRETE PAVEMENT ONLY)

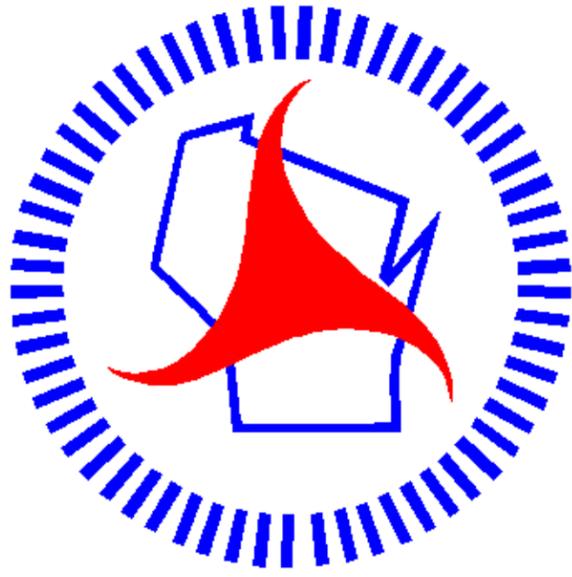


**WIS DOT MONUMENT  
MARKER LOGO**  
FOR TYPES "A", "C", & "D"

**LANDMARK REFERENCE  
MONUMENTS AND COVERS**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
March 2018 /S/ Raymond A. Kumapayi  
DATE CHIEF SURVEYING AND MAPPING ENGINEER  
FHWA



## ***Wisconsin Department of Transportation***

Dedicated people creating transportation solutions through innovation and exceptional service.

<http://www.dot.wisconsin.gov>