MAD

JANUARY 2022

PROJECT ID: 3030-02-72

WITH: N/A

COUNTY: DODGE

07

ORDER OF SHEETS

TOTAL SHEETS = 106

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

OCONOMOWOC - MAYVILLE

WEST JCT CTH S TO RUEDEBUSCH AVE

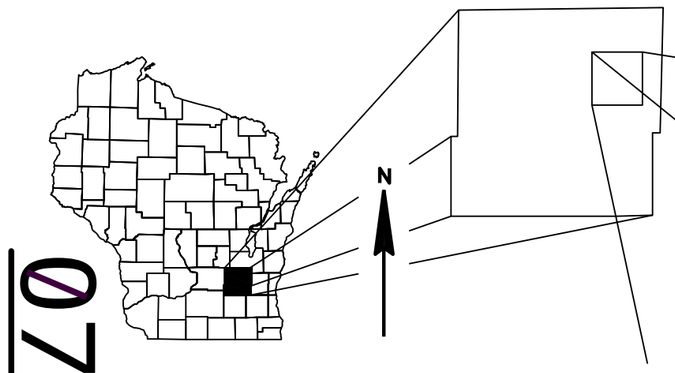
STH 67

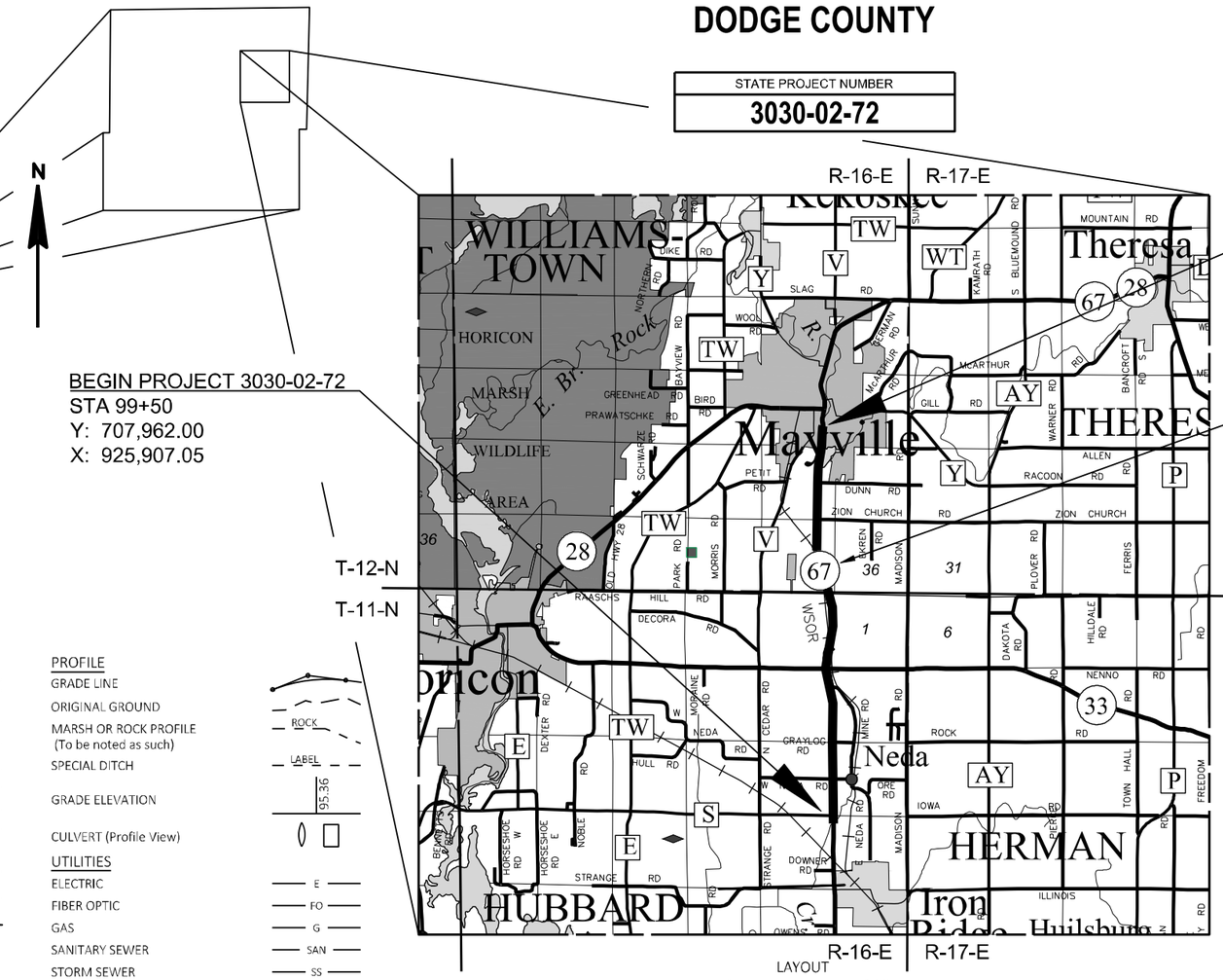
DODGE COUNTY

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
3030-02-72	WISC 2022111	1

STATE PROJECT NUMBER

3030-02-72





BEGIN PROJECT 3030-02-72
STA 99+50
Y: 707,962.00
X: 925,907.05

END PROJECT 3030-02-72
STA 377+15.2

RAILROAD CONSTRUCTION 3030-02-54
STA 223+59
STH 67 CL & WSOR RR CL

DESIGN DESIGNATION 3030-02-02

A.A.D.T.	(2022)	=	4,700
A.A.D.T.	(2042)	=	5,600
D.H.V.		=	700
D.D.		=	59/41
T.		=	11.5%
DESIGN SPEED		=	60 MPH
ESALS		=	1,100,000

CONVENTIONAL SYMBOLS

PLAN

- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE

- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES

ELECTRIC	E
FIBER OPTIC	FO
GAS	G
SANITARY SEWER	SAN
STORM SEWER	SS
TELEPHONE	T
WATER	W
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

LAYOUT SCALE 0 2 MI

TOTAL NET LENGTH OF CENTERLINE = 5.259 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS)DODGE COUNTY COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

ORIGINAL PLANS PREPARED BY

MSA

1702 Pankratz Street, Madison, WI 53704
608-242-7779 1-800-446-0679 Fax: 608-242-5664

WISCONSIN CHASE C. KIELER E-43621 MADISON WI PROFESSIONAL ENGINEER

DATE: 1/12/21 (Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	MSA PROFESSIONAL SERVICES, INC.
Designer	MSA PROFESSIONAL SERVICES , INC.
Project Manager	JEREMY HALL, P.E.
Regional Examiner	SOUTHWEST REGION
Regional Supervisor	JAMES OETTINGER, P.E.

APPROVED FOR THE DEPARTMENT

DATE: Digitally signed by Jeremy Hall DN: C=US, E=Jeremy.Hall@dot.wi.gov, O=WisDOT, OU=DTSD SW DOS East, CN=Jeremy Hall Reason: I am approving this document Date: 2021.06.28 14:33:36-05'00'

E

APPLY TACK COAT AT A MINIMUM RATE OF 0.07 GAL/SY TO MILLED SURFACES, AND 0.05 GAL/SY BETWEEN HMA LIFTS. HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/INCH.

SAWCUTS , AS SHOWN ON PLANS, ARE APPROXIMATE LOCATIONS AND MAY BE ADJUSTED BY THE FIELD ENGINEER BASED ON FIELD CONDITIONS.

THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A TRAVEL OR TURN LANE.

INCORPORATE EXISTING SHOULDER AGGREGATE INTO NEW SHOULDERS UNLESS OTHERWISE DIRECTED BY THE FIELD ENGINEER.

DIMENSIONS GIVEN FOR THE EXISTING FEATURES SHALL BE CONSIDERED APPROXIMATE AND MEASURED IN THE FIELD FOR MATCHING PURPOSES.

FLAGGERS KEEP IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. EQUIP FLAGGERS WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, COVER OR REMOVE ALL SIGNS RELATING TO THE OPERATION AND THE FACILITY RESTORED TO NORMAL OPERATION.

SIGN LOCATIONS ON THE PLAN ARE APPROXIMATE, AND MAY BE ADJUSTED BY THE FIELD ENGINEER AS NEEDED TO FIT CONDITIONS.

THE LIMITS OF PAYMENT REMOVAL ON SIDE STREETS ARE APPROXIMATE AND WILL BE VERIFIED IN THE FIELD BY THE ENGINEER.

TYPICAL FINISHED SECTIONS SHOW THE GENERAL ROADWAY FEATURES THROUGHOUT THE PROJECT. PAVEMENT SLOPES, BORDER SLOPES, ETC., MAY VARY WITHIN THE STATION LIMITS OF EACH SECTION.

THE LOCATIONS OF EXISTING UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. UTILITIES ARE ONLY SHOWN NEAR THE STH 33 INTERSECTION. THERE ARE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN IN THE PLANS.

COORDINATE CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

THE EROSION CONTROL ITEMS SHOWN ON THE PLANS ARE AT SUGGESTED LOCATIONS. THE ENGINEER WILL DETERMINE THE EXACT LOCATIONS OF EROSION CONTROL ITEMS. MAINTAIN ALL EROSION CONTROL UNTIL SUCH TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NECESSARY. REMOVE ITEMS AT THE ENGINEERS DISCRETION.

ADJUST NUMBER, LOCATION, AND SPACING OF TRAFFIC CONTROL SIGNS AND DEVICES, AS SHOWN ON THE PLANS, TO FIT FIELD CONDITIONS.

EXISTING RIGHT OF WAY LINES ARE APPROXIMATE.

STANDARD ABBREVIATIONS

AC	ACRES
AEW	APRON ENDWALL
AGG	AGGREGATE
AH	AHEAD
ALUM.	ALUMINUM
A.P.	ACCESS POINT
ASPH	ASPHALT
AVE	AVENUE
BAD	BASE AGGREGATE DENSE
BK	BACK
BLK	BLOCK
BOC	BACK OF CURB
BOW	BACK OF SIDEWALK
BM	BENCHMARK
CABC	CRUSHED AGGREGATE BASE COURSE
CL or ℄	CENTERLINE
Δ	CENTRAL ANGLE or DELTA
CMCP	CORRUGATED METAL CULVERT PIPE
CONC	CONCRETE
CP	CONTROL POINT
CPCS	CULVERT PIPE CORRUGATED STEEL
CSM	CERTIFIED SURVEY MAP
CTH	COUNTY TRUNK HIGHWAY
D	DEGREE OF CURVATURE
DES	DESIRABLE
E	EAST
EB	EASTBOUND
EBS	EXCAVATION BELOW SUBGRADE
EOP	EDGE OF PAVEMENT
ET AL	AND OTHERS
EW	ENDWALL
EXIST	EXISTING
FT	FOOT
FT2	SQUARE FEET
GN	GRID NORTH
GV	GAS VALVE
HERCP	HORIZONTAL ELLIPTICAL REINFORCED CONCRETE PIPE
HYD	HYDRANT
IN	INCH
INL	INLET
INV	INVERT
IP	IRON PIPE
L	LENGTH
L	LENGTH OF CURVE
LC	LONG CHORD
LCP	LONG CHORD BEARING
LF	LINEAR FEET
LT	LEFT
MH	MANHOLE
MON	MONUMENT
N	NORTH
NB	NORTHBOUND
N.C.	NORMAL CROWN
NO	NUMBER
PB	PULLBOX
PC	POINT OF CURVATURE
PI	POINT OF INTERSECTION
PL	PROPERTY LINE
PLE	PERMANENT LIMITED EASEMENT
POB	POINT OF BEGINNING
PT	POINT OF TANGENCY
R	RADIUS
R	RANGE
RCP	REINFORCED CONCRETE PIPE
RD	ROAD
REQ'D	REQUIRED
RL or R/L	REFERENCE LINE
RP	RADIUS POINT
RT	RIGHT
R/W	RIGHT OF WAY
S	SOUTH
SAN	SANITARY SEWER
SB	SOUTHBOUND
S.E.	SUPERELEVATION
SEC	SECTION
SSPRC	STORM SEWER PIPE REINFORCED CONCRETE
SSPRCHE	STORM SEWER PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL
SQ	SQUARE
ST	STREET
STA	STATION
STD	STANDARD
STH	STATE TRUNK HIGHWAY
STM	STORM SEWER
STR	STRUCTURE
T	TANGENT
TAN	TANGENT
TEMP	TEMPORARY
TLE	TEMPORARY LIMITED EASEMENT
T or TN	TOWN
TYP.	TYPICAL
WM	WATERMIN
WV	WATER VALVE
W	WEST
WB	WESTBOUND
X	EAST GRID COORDINATE
Y	NORTH GRID COORDINATE

BORING SUMMARY

BORING NUMBER	STATION	OFFSET	EXISTING ASPHALT DEPTH	BASE DEPTH	SUBGRADE TYPE
B-1	100+48	12' RT	6"	9"	SILT
B-2	109+29	10' RT	6"	15"	SILTY CLAY
B-3	119+61	3' RT	7"	11"	SILT
B-4	124+40	10' RT	8"	13"	SILT
B-5	148+88	3' RT	6"	16.5"	SILT
B-6	141+72	3' LT	8"	10"	SANDY SILT
B-7	124+46	12' LT	9"	16"	SANDY SILT
B-8	112+96	10' LT	8"	9"	SILT

DNR LIAISON

WISCONSIN DEPARTMENT OF NATURAL RESOURCES
ATTN: ERIC HEGGELUND
3911 FISH HATCHERY ROAD
FITCHBURG, WI 53711
PHONE: (608) 275-3301
EMAIL: eric.heggelund@wisconsin.gov

RAILROAD CONTACT

WISCONSIN DEPARTMENT OF TRANSPORTATION
SOUTHWEST REGION
ATTN: TERI BECKMAN
2101 WRIGHT STREET
MADISON, WI 53704
PHONE: (608) 733-1923
EMAIL: teri.beckman@dot.wi.gov

DESIGN CONTACTS

WISCONSIN DEPARTMENT OF TRANSPORTATION
SOUTHWEST REGION
ATTN: JEREMY HALL, P.E.
2101 WRIGHT STREET
MADISON, WI 53704
PHONE: (608) 245-2655
EMAIL: jeremy.hall@dot.wi.gov

MSA PROFESSIONAL SERVICES, INC.
ATTN: NATHAN COOK, P.E.
1702 PANKRATZ ST
MADISON, WI 53704
PHONE: (608) 216-2058
E-MAIL: ncook@msa-ps.com

UTILITY CONTACTS

AT&T Wisconsin - Comm Line
Chuck Bartelt
70 E Division St
Fond Du Lac, WI
(920) 929-1013
cb1461@att.com

Alliant Energy - Electricity
Perry Boeck
120 East Maple Ave
Beaver Dam, WI 53916
(920) 887-6061
PerryBoeck@alliantenergy.com

Alliant Energy - Gas/Petroleum
Perry Boeck
120 East Maple Ave
Beaver Dam, WI 53916
(920) 887-6061
PerryBoeck@alliantenergy.com

Charter Comm - Comm Line
Nick Frase
1515 West Washington St
West Bend, WI 53095
(920) 304-6797
nick.frase@charter.com

City of Mayville-Sewer
Courtney Steger
400 Kekoskee St
Mayville, WI 53050
920-387-7906 ext. 1223
csteger@mayvillecity.com

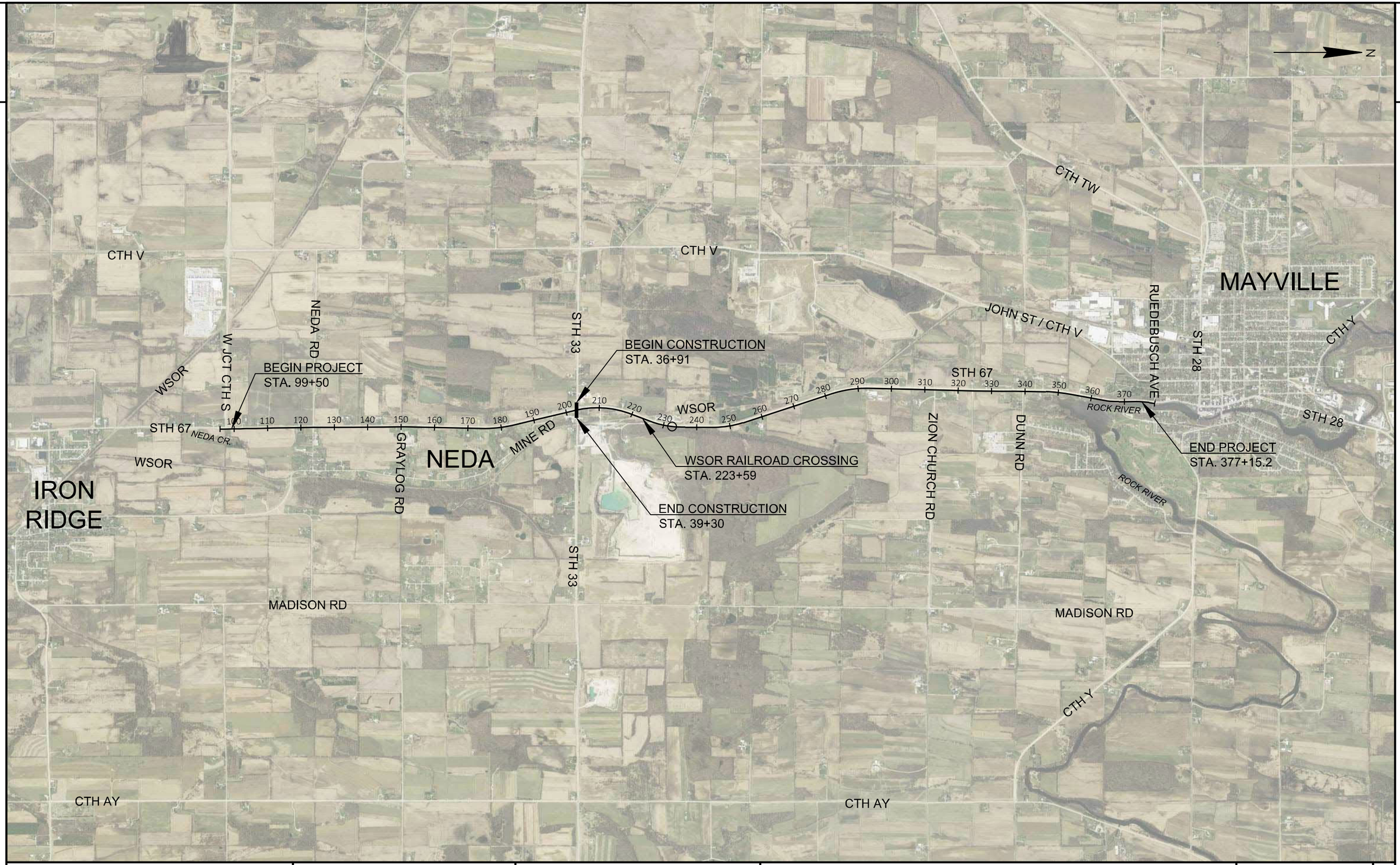
City of Mayville-Water
Courtney Steger
400 Kekoskee St
Mayville, WI 53050
920-387-7906 ext. 1223
csteger@mayvillecity.com

We Energies - Electricity
Gregory Boerner
500 S 116th St
West Allis, WI 53214
(618) 409-5861
gregory.boerner@we-energies.com

We Energies - Gas/Petroleum
Jacob Hulbert
500 S 116th St
West Allis, WI 53214
(414) 944-5575
jacob.hulbert@we-energies.com

DIGGERS HOTLINE
Dial 811 or (800)242-8511
www.DiggersHotline.com

INDEX OF TYPICAL SECTIONS & DETAIL SHEETS



PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PROJECT OVERVIEW	SHEET 3	E

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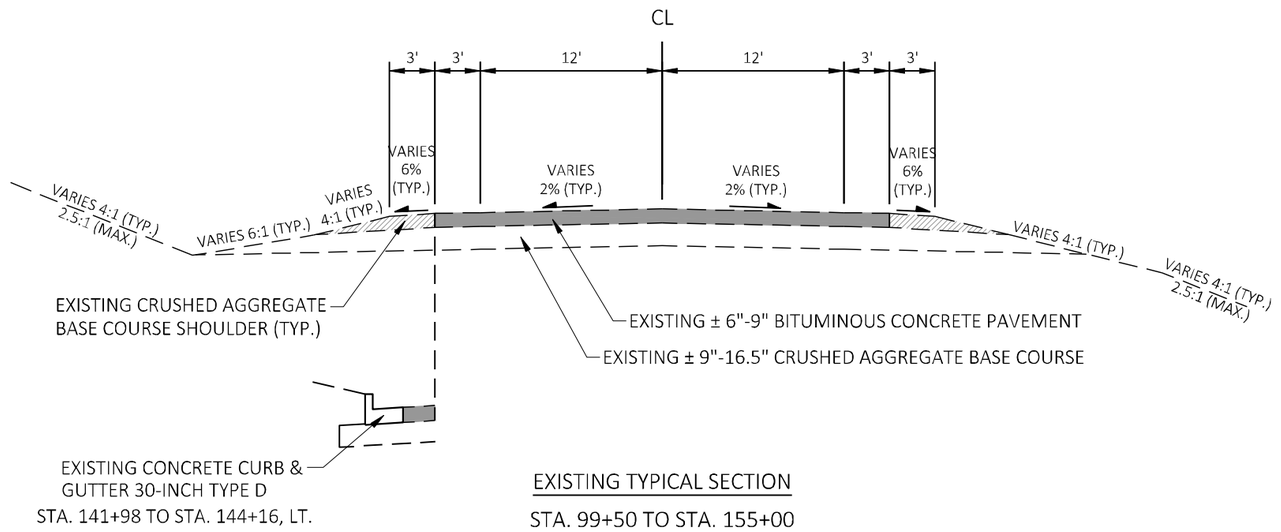
LAYOUT NAME - 020201_po

PLOT DATE : 4/12/2021 1:48 PM PLOT BY : KEVIN KLOCKZIEM PLOT NAME : PLOT SCALE : 1 IN:0.5 MI

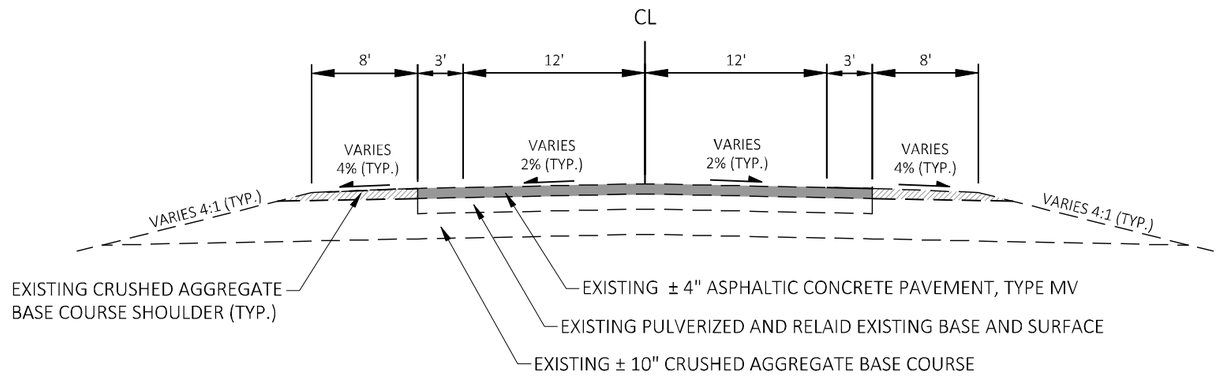
WISDOT/CADDS SHEET 42

2



EXISTING TYPICAL SECTION

STA. 99+50 TO STA. 155+00



EXISTING TYPICAL SECTION

STA. 155+00 TO STA. 357+62

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	TYPICAL SECTIONS	SHEET 4	E

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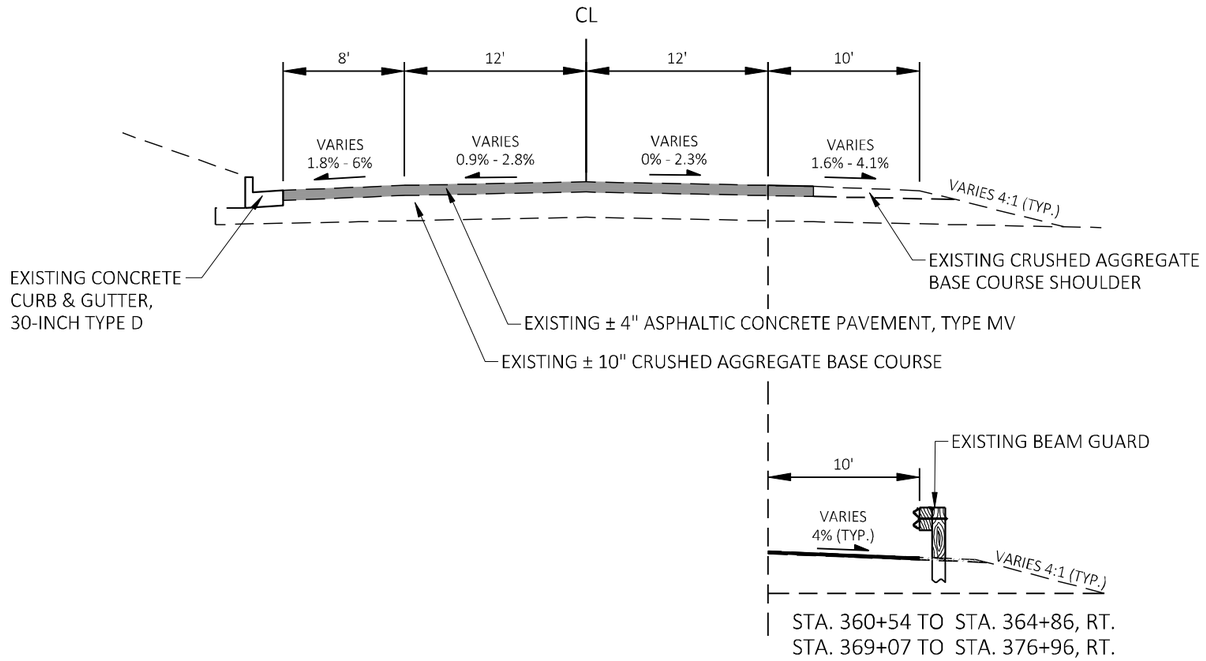
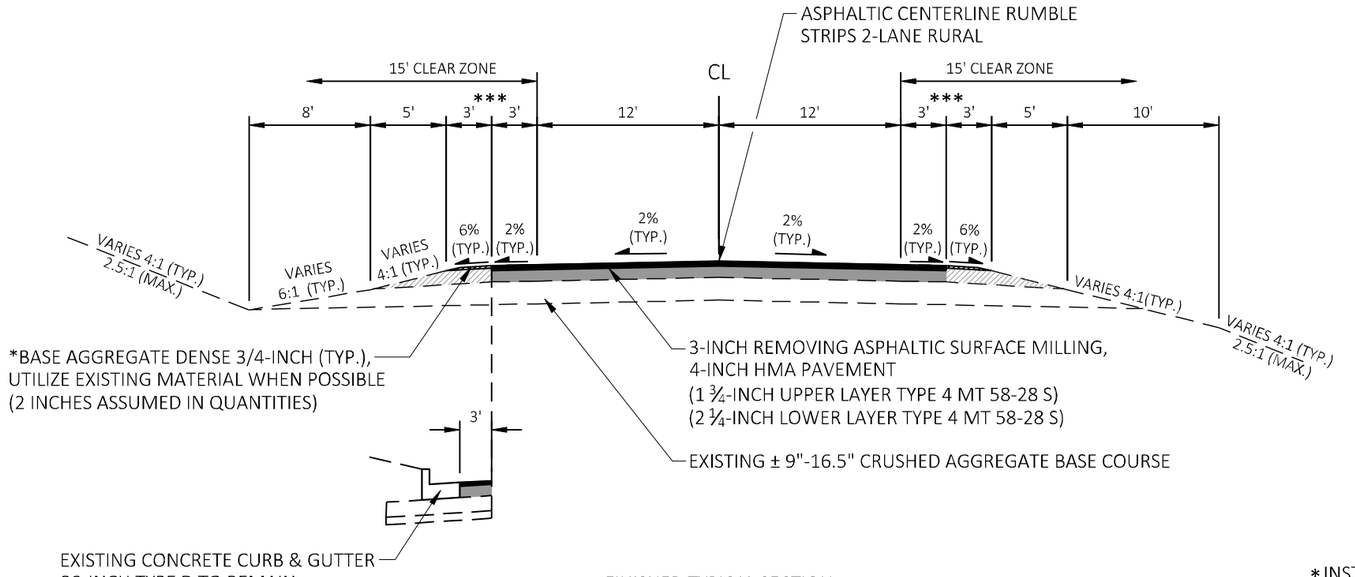


STA. 360+54 TO STA. 364+86, RT.
STA. 369+07 TO STA. 376+96, RT.

EXISTING TYPICAL SECTION

STA. 357+62 TO STA. 377+15 (HEADER)

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	TYPICAL SECTIONS	SHEET 5	E



FINISHED TYPICAL SECTION

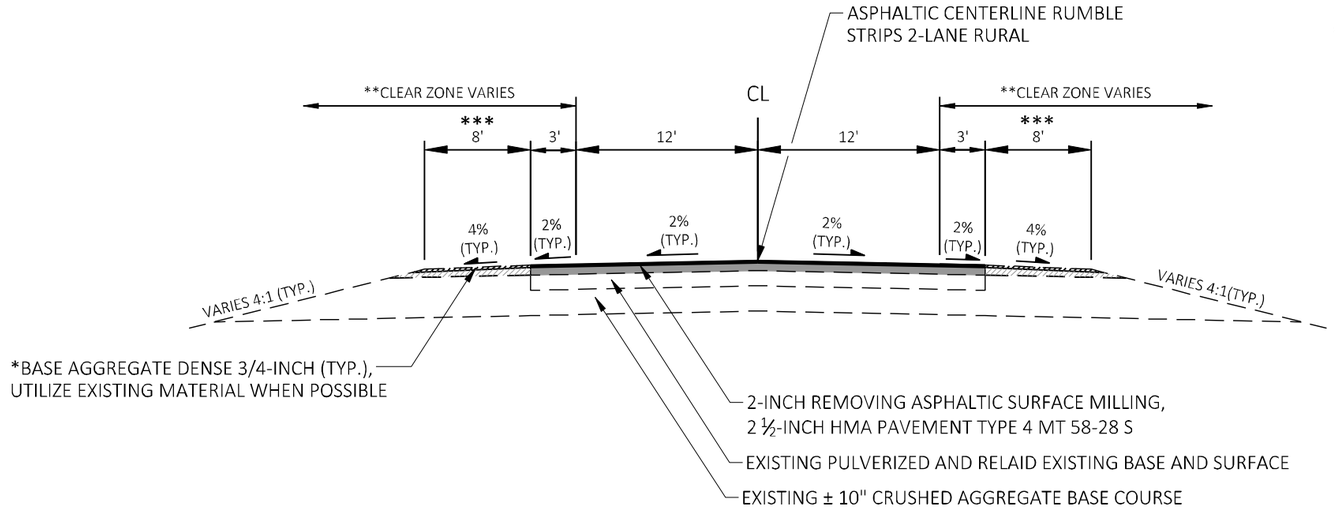
STA. 99+50 TO STA. 155+00

* INSTALL BASE AGGREGATE DENSE 3/4-INCH AS NECESSARY FOR LOW SHOULDERS. 2-INCHES ASSUMED FOR QUANTITY PURPOSES.

** CLEAR ZONE IS 15' SOUTH OF STH 33 AND 30' NORTH OF STH 33 ACCORDING TO ASBUILT INFORMATION

*** STOP MILL AND OVERLAY AT RECONSTRUCTED LIMITS OF RAILROAD APPROACHES (CONSTRUCTED BY OTHERS) FIELD ENGINEER TO DETERMINE LIMITS OF BUTT JOINT.

NOTE: INSTALL SAFETY EDGE ON ALL HMA SHOULDERS. REFER TO SDD "SAFETY EDGE".

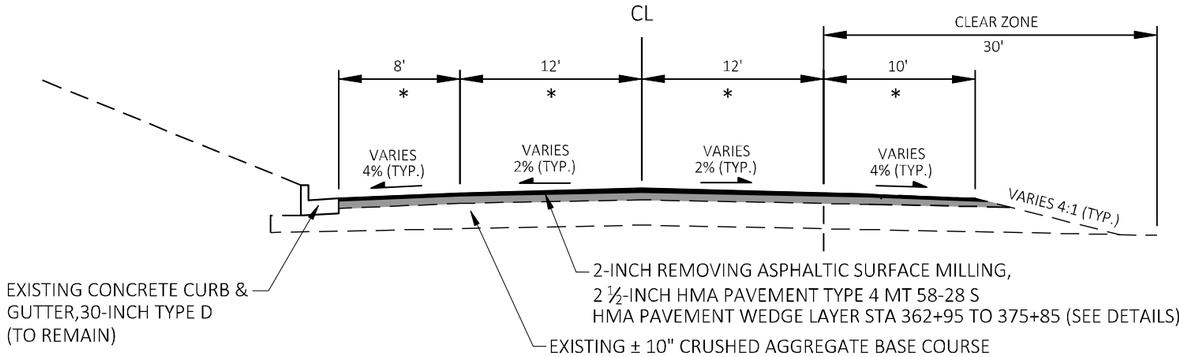


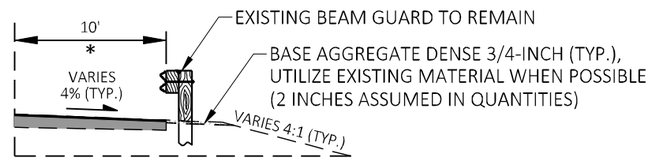
FINISHED TYPICAL SECTION

GRAYLOG ROAD TO NORTH OF DUNN ROAD
STA. 155+00 TO STA. 357+62 ***

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	TYPICAL SECTIONS	SHEET 6	E

2





STA. 360+54 TO STA. 364+86, RT.
STA. 369+07 TO STA. 376+96, RT.

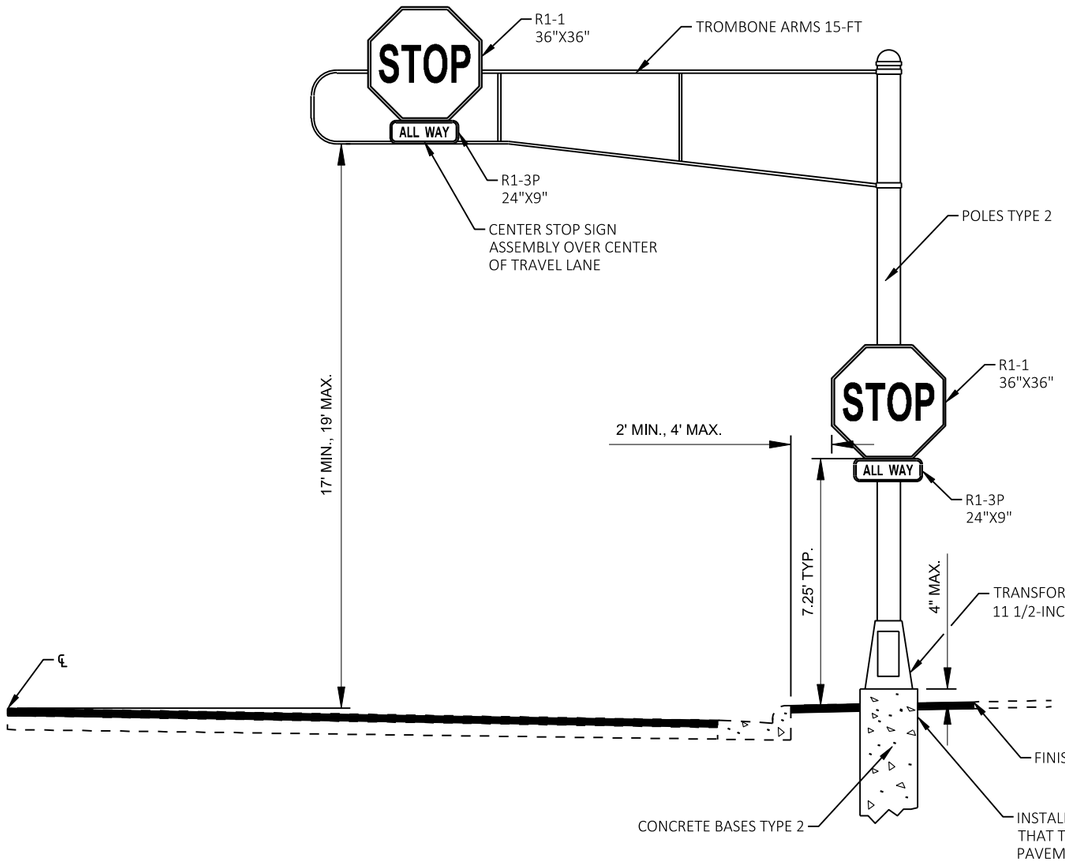
* SEE CONSTRUCTION DETAIL FOR LANE AND SHOULDER SLOPES FOR CROSS SLOPE CORRECTION AREA FROM STA. 362+95 TO STA. 375+85

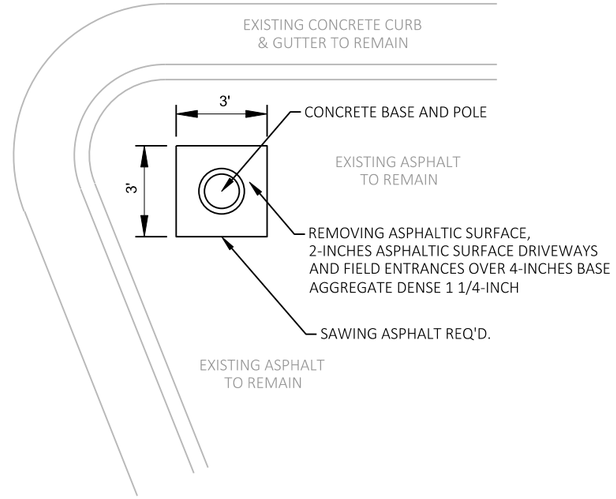
NOTE: INSTALL SAFETY EDGE ON ALL HMA SHOULDERS. REFER TO SDD "SAFETY EDGE".

FINISHED TYPICAL SECTION

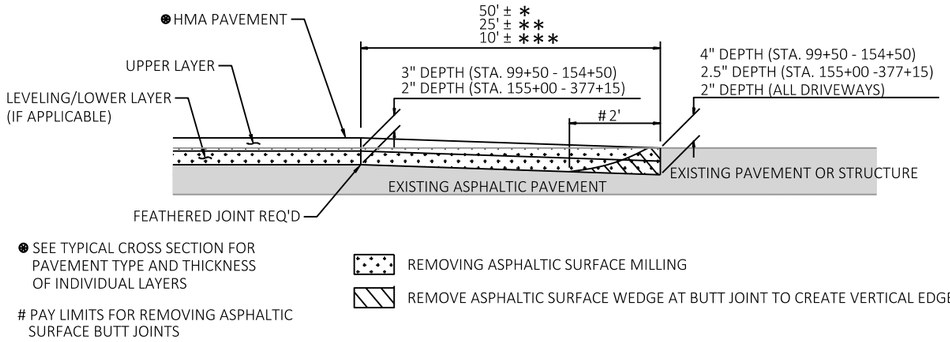
NORTH OF DUNN ROAD TO RUEDEBUSCH AVENUE
STA. 357+62 TO STA. 377+15 (HEADER)

2



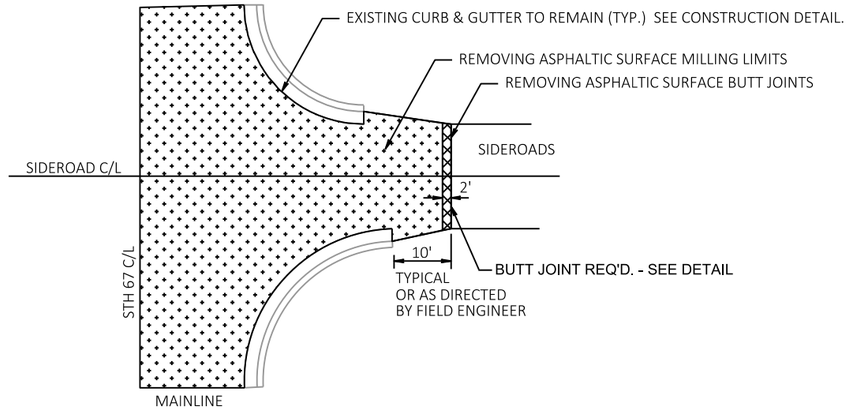


TROMBONE ARM MOUNTING DETAIL
(STH 67 & STH 33 INTERSECTION)

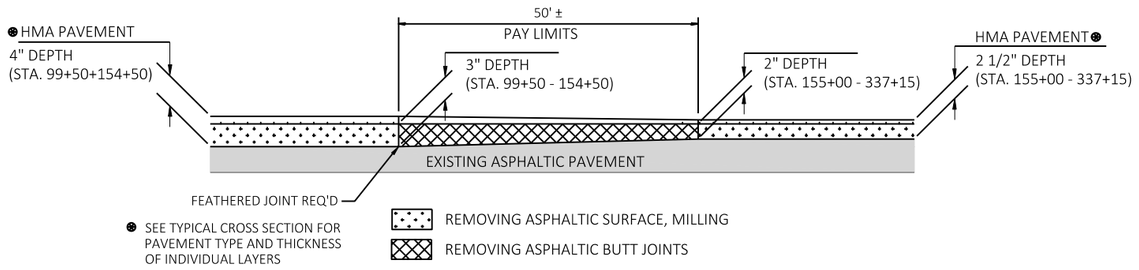


BUTT JOINT DETAIL FOR MILLED ASPHALTIC PAVEMENTS (PROFILE CHANGE)

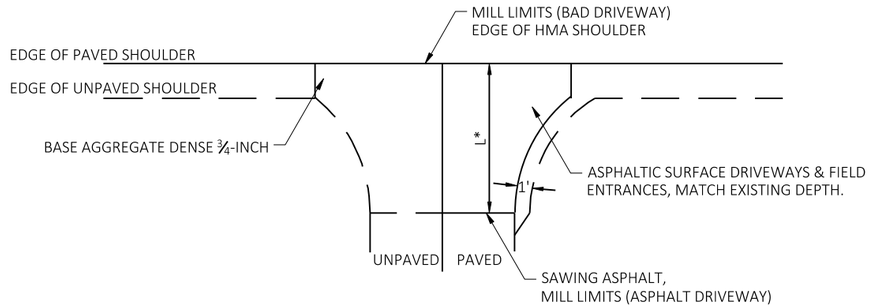
* MAINLINE
** SIDEROADS
*** DRIVEWAY ENTRANCES



SIDEROAD PLAN

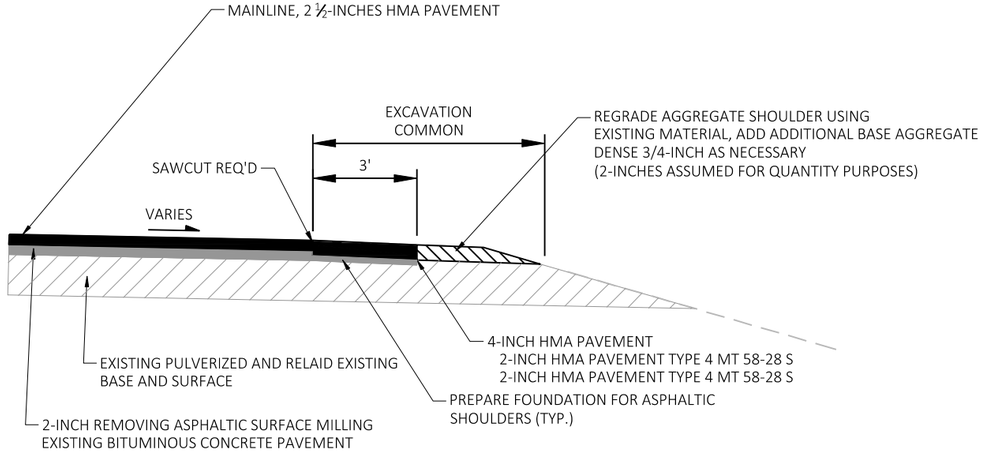


BUTT JOINT DETAIL FOR MILLED PAVEMENT TRANSITION (PROFILE CHANGE)
STA 154+50 TO 155+00



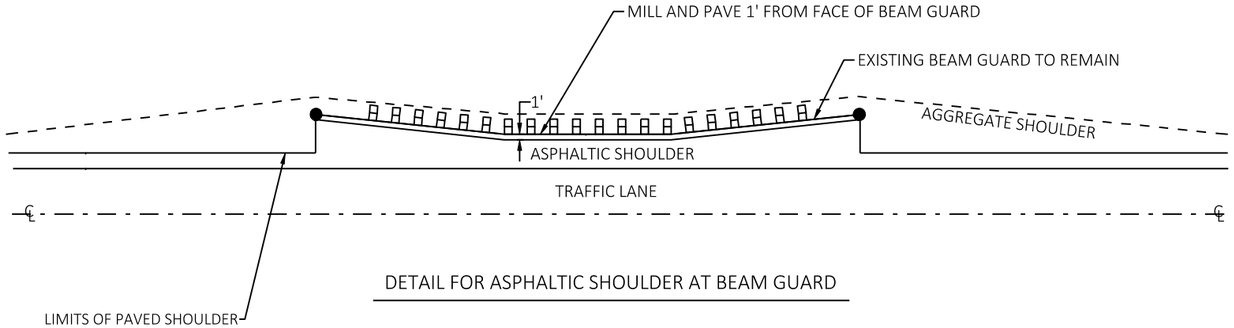
*L=10' TYPICAL: EXACT LENGTH TO BE DETERMINED IN THE FIELD BY THE ENGINEER.

DRIVEWAY PLAN
ASPHALT (ASPH.) AND BASE AGGREGATE DENSE (BAD)

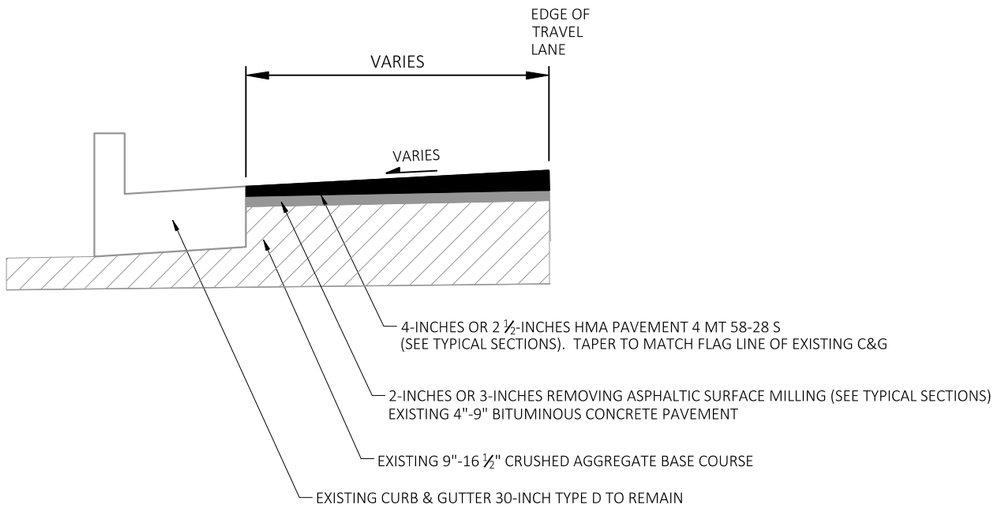


HMA PAVEMENT WIDENING DETAIL

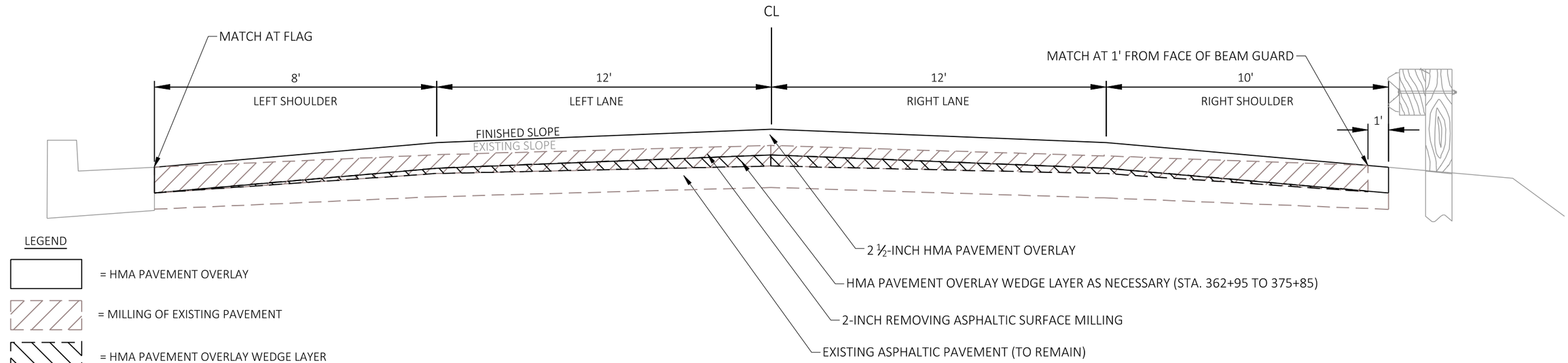
STA. 202+17 TO STA. 39+30, RT
STA. 202+28 TO STA. 39+61, LT & RT
STA. 36+91 TO STA. 204+58, LT
STA. 39+30 TO STA. 204+52, LT & RT
STA. 336+81 TO STA. 342+60, LT



DETAIL FOR ASPHALTIC SHOULDER AT BEAM GUARD



SIDEROAD HMA PAVING DETAIL AT EXISTING CURB & GUTTER



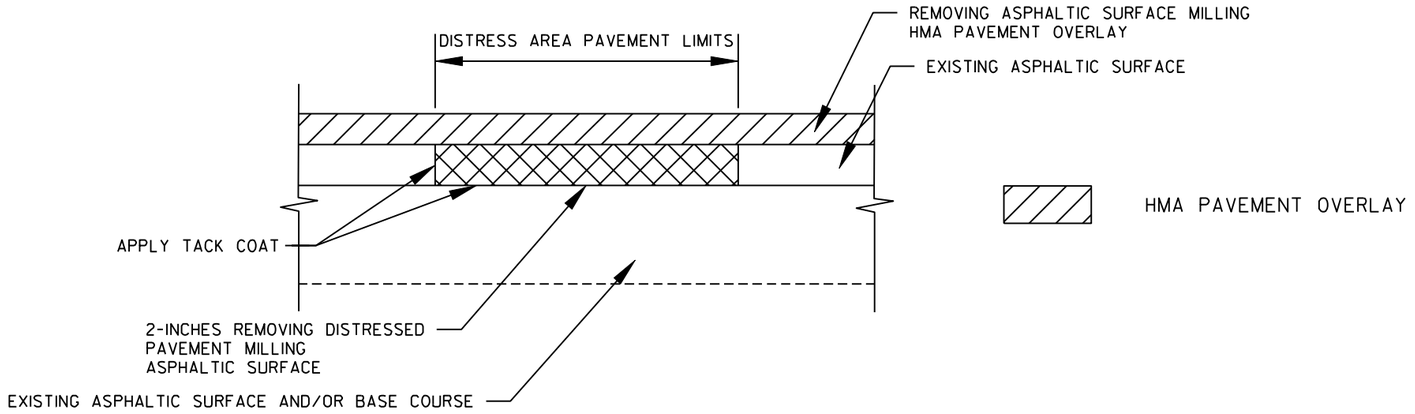
SHOULDER AND LANE SLOPE CORRECTION TABLE

STA	EXISTING LEFT SHOULDER SLOPE	FINISHED LEFT SHOULDER SLOPE	EXISTING LEFT LANE SLOPE	FINISHED LEFT LANE SLOPE	EXISTING RIGHT LANE SLOPE	FINISHED RIGHT LANE SLOPE	EXISTING RIGHT SHOULDER SLOPE	FINISHED RIGHT SHOULDER SLOPE	NOTES*
362+95	-6.00%	-6.20%	-2.80%	-3.00%	-0.60%	-1.00%	-1.60%	-0.72%	RAISE CENTERLINE BY 0.04'
364+60	-5.20%	-6.00%	-2.40%	-2.20%	-2.30%	-2.00%	-1.70%	-1.66%	RAISE CENTERLINE BY 0.04'
367+95	-5.60%	-5.95%	-2.70%	-2.80%	-0.10%	-2.00%	-4.00%	-1.32%	RAISE CENTERLINE BY 0.04'
371+10	-3.40%	-2.50%	-1.10%	-2.00%	-1.70%	-2.00%	-4.10%	-4.10%	RAISE CENTERLINE BY 0.04'
372+50	-2.30%	-3.40%	-1.10%	-2.00%	-1.40%	-2.00%	-2.80%	-4.00%	RAISE CENTERLINE BY 0.19'
373+75	-1.90%	-2.00%	-1.10%	-2.00%	-2.20%	-2.00%	-3.30%	-4.70%	RAISE CENTERLINE BY 0.12'
374+50	-1.80%	-2.00%	-0.90%	-2.00%	-2.10%	-2.00%	-3.50%	-5.10%	RAISE CENTERLINE BY 0.15'
375+85	-1.30%	-3.73%	-2.00%	-2.00%	-1.30%	-2.00%	-2.90%	-4.00%	RAISE CENTERLINE BY 0.19'

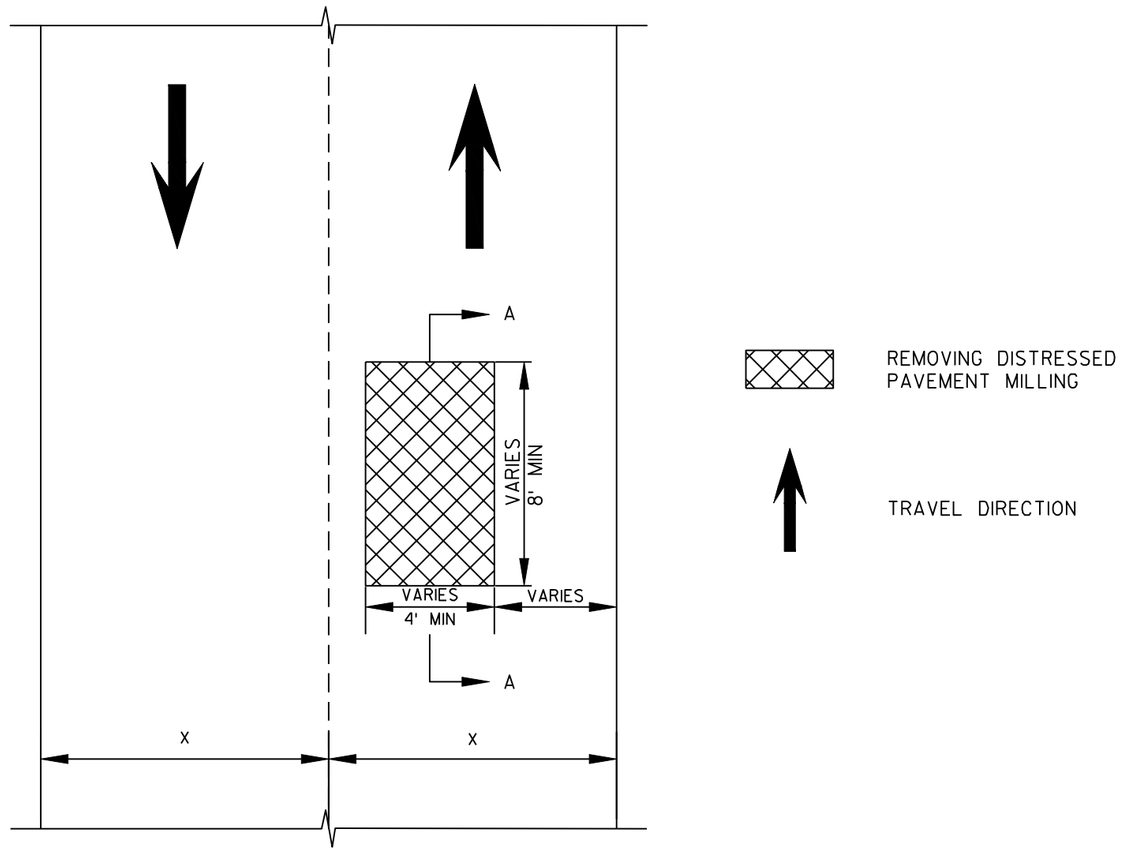
* THE FINISHED ROADWAY CENTERLINE WILL BE BE RAISED AT A MINIMUM OF ½-INCH BASED ON THICKER OVERLAY (SEE TYPICAL SECTIONS).

CROSS SLOPE CORRECTION STA. 362+95 TO STA. 375+85

2



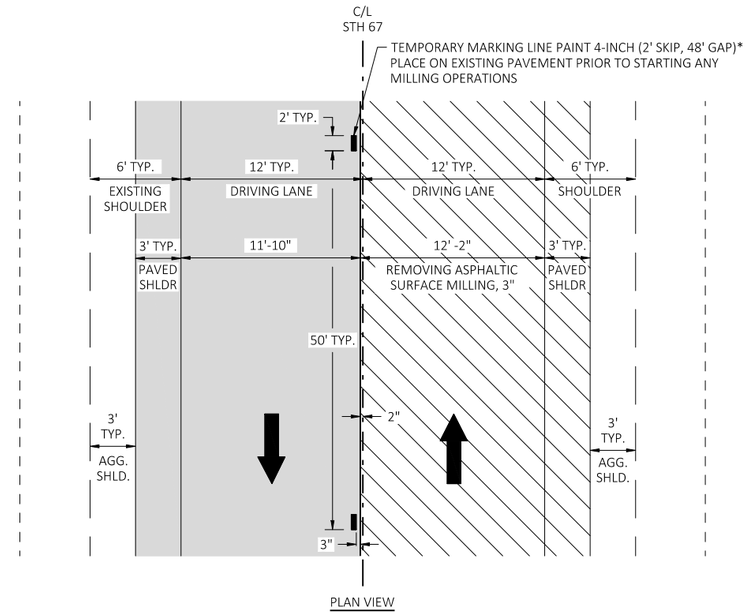
REMOVING DISTRESS PAVEMENT MILLING
SECTION A-A

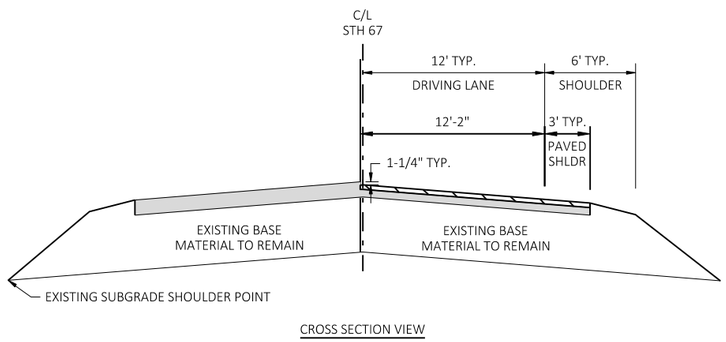


PLAN VIEW

EXACT LOCATION AND LIMITS OF REMOVING DISTRESSED PAVEMENT MILLING TO BE DETERMINED BY THE ENGINEER IN THE FIELD

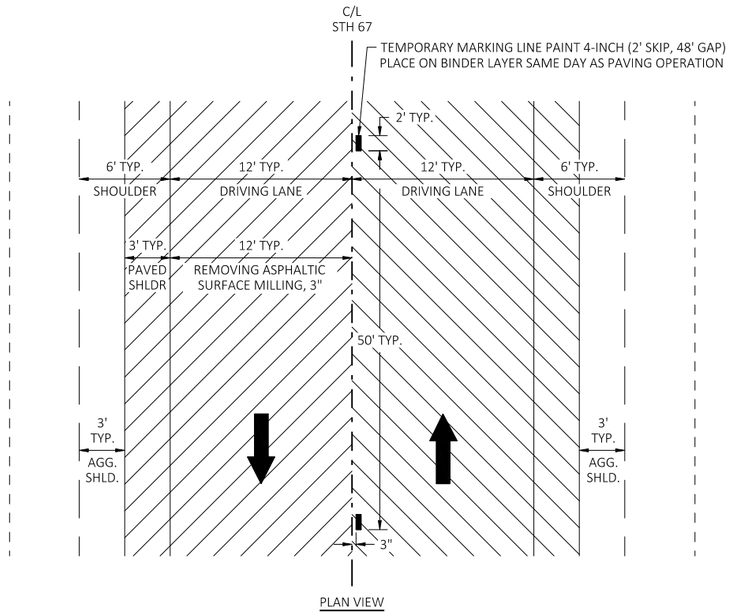
PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN: CONSTRUCTION DETAILS	SHEET 12	E

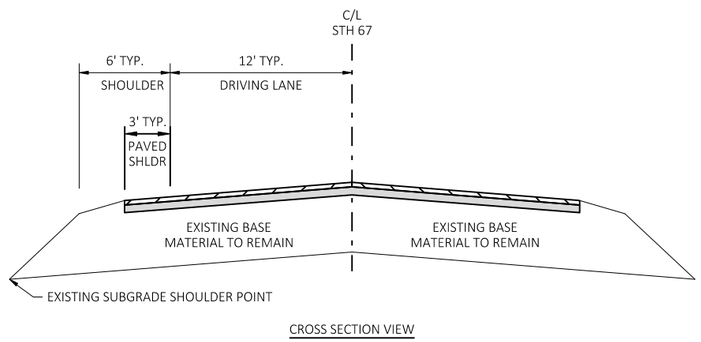




LONGITUDINAL PAVEMENT JOINT DETAIL - FIRST PASS

STA. 99+50 - STA. 155+00





LONGITUDINAL PAVEMENT JOINT DETAIL - SECOND PASS

STA. 99+50 -155+00

EXISTING SINGLE AGGREGATE BITUMINOUS PAVEMENT TO REMAIN

FIRST PASS 1-3/4" HMA PAVEMENT
4 MT 58-28 S PAVING LIMITS

SECOND PASS 1-3/4" HMA PAVEMENT
4 MT 58-28 S PAVING LIMITS

1" TYP. BASE AGGREGATE
DENSE 3/4-INCH

CROSS SECTION VIEW

BINDER LAYER MUST BE PAVED PRIOR TO MILLING ADJACENT TRAVELED LANE.

NORTHBOUND LANE TO BE MILLED AND BINDER PAVED PRIOR TO MILLING SOUTHBOUND LANE.

FOR DROP OFF 1 3/4" OR GREATER, INSTALL A NOTCHED WEDGE JOINT ACCORDING TO THE STANDARD DETAIL DRAWING.

*REQUIRED ONLY IN LOCATIONS WHERE EXISTING PAVEMENT MARKING WILL NOT BE VISIBLE FOR MORE THAN 50' AFTER MILLING.

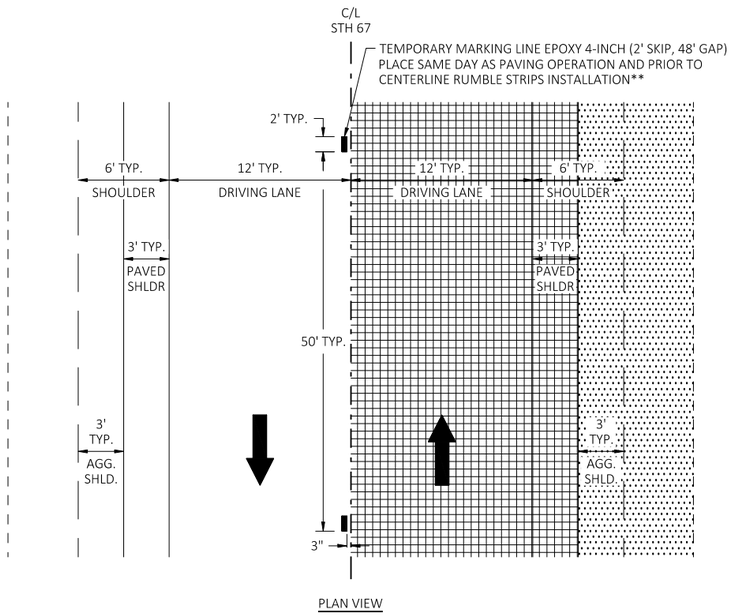
R4-1
24"X30"
DO NOT PASS

R4-1 SIGNS SHALL BE INSTALLED AT THE BEGINNING OF EXISTING NO PASSING ZONES (OPPOSITE SIDE OF W14-3 SIGNS). ADDITIONAL R4-1 SIGNS SHALL BE INSTALLED AT ONE MILE INTERVALS WITHIN THE NO PASSING ZONE IF APPLICABLE.

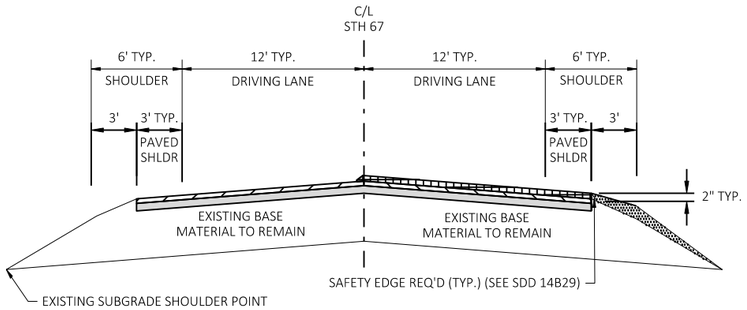
W8-12
48"X48"
NO CENTER LINE

W8-12 SIGNS SHALL BE PLACED AT THE START OF THE ROADWAY SEGMENT THAT HAS NO MARKING. ADDITIONAL W8-12 SIGNS SHALL BE PLACED AT TWO MILE INTERVALS IF APPLICABLE.

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN: CONSTRUCTION DETAILS	SHEET 13	E



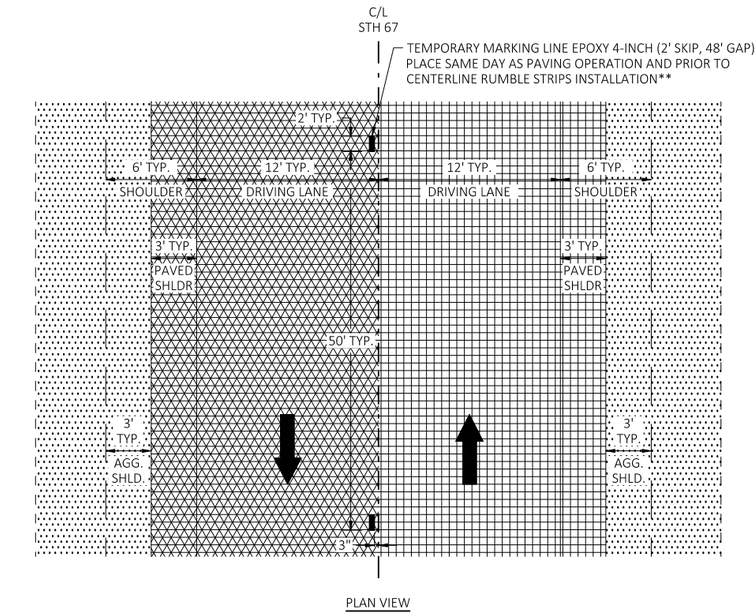
PLAN VIEW



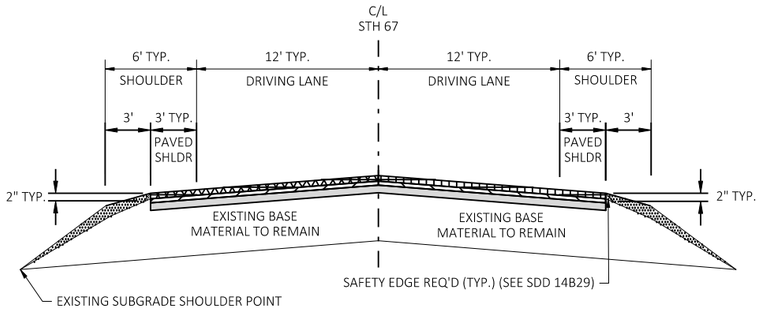
CROSS SECTION VIEW

LONGITUDINAL PAVEMENT JOINT DETAIL - THIRD PASS

STA. 99+50 TO 155+00



PLAN VIEW



CROSS SECTION VIEW

LONGITUDINAL PAVEMENT JOINT DETAIL - FOURTH PASS

STA. 99+50 TO 155+00

EXISTING SINGLE AGGREGATE
BITUMINOUS PAVEMENT TO REMAIN

FIRST PASS 1-3/4" HMA PAVEMENT
4 MT 58-28 S PAVING LIMITS

SECOND PASS 1-3/4" HMA PAVEMENT
4 MT 58-28 S PAVING LIMITS

THIRD PASS 2-1/4" HMA PAVEMENT
4 MT 58-28 S PAVING LIMITS

FOURTH PASS 2-1/4" HMA PAVEMENT
4 MT 58-28 S PAVING LIMITS

2" TYP. BASE AGGREGATE
DENSE 3/4-INCH

CROSS SECTION VIEW

FOR DROP OFFS 1 3/4" OR GREATER, INSTALL A NOTCHED WEDGE JOINT ACCORDING TO THE STANDARD DETAIL DRAWING.

SEE THE STANDARD DETAIL DRAWING FOR PERMANENT PAVEMENT MARKING DETAILS.

**LOCATING NO PASSING ZONES REQUIRED PRIOR TO PLACEMENT OF TEMPORARY MARKING LINE EPOXY 4-INCH. MARKING LINE EPOXY 4-INCH REQUIRED AFTER INSTALLING CENTERLINE RUMBLE STRIPS.

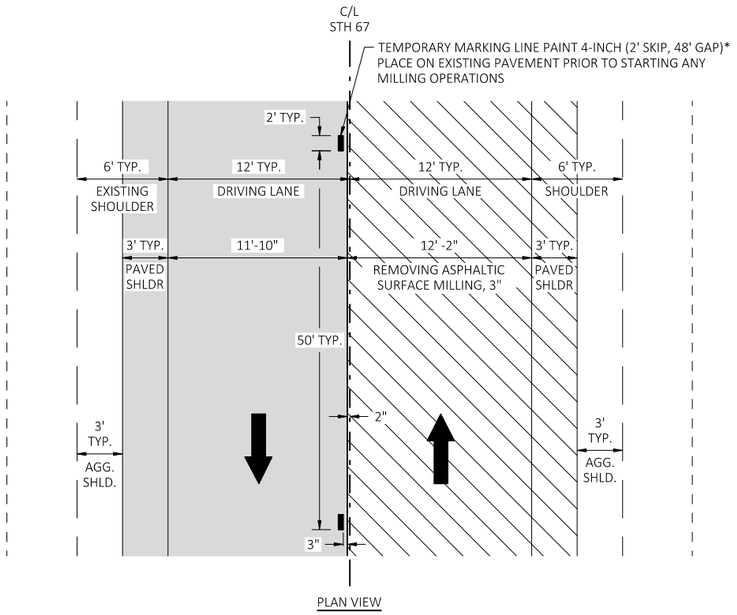
R4-1
24"X30"

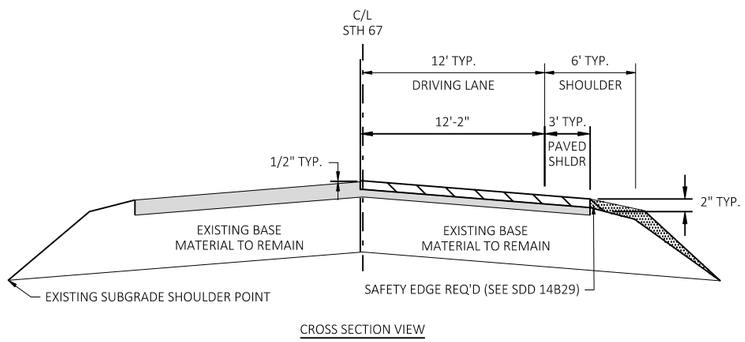
R4-1 SIGNS SHALL BE INSTALLED AT THE BEGINNING OF EXISTING NO PASSING ZONES (OPPOSITE SIDE OF W14-3 SIGNS). ADDITIONAL R4-1 SIGNS SHALL BE INSTALLED AT ONE MILE INTERVALS WITHIN THE NO PASSING ZONE IF APPLICABLE.

W8-12
48"X48"

W8-12 SIGNS SHALL BE PLACED AT THE START OF THE ROADWAY SEGMENT THAT HAS NO MARKING. ADDITIONAL W8-12 SIGNS SHALL BE PLACED AT TWO MILE INTERVALS IF APPLICABLE.

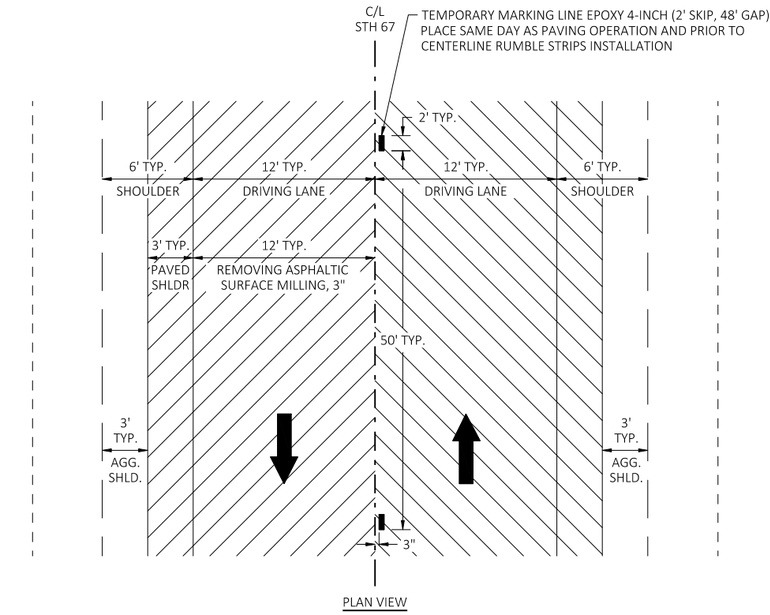
PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN: CONSTRUCTION DETAILS	SHEET 14	E

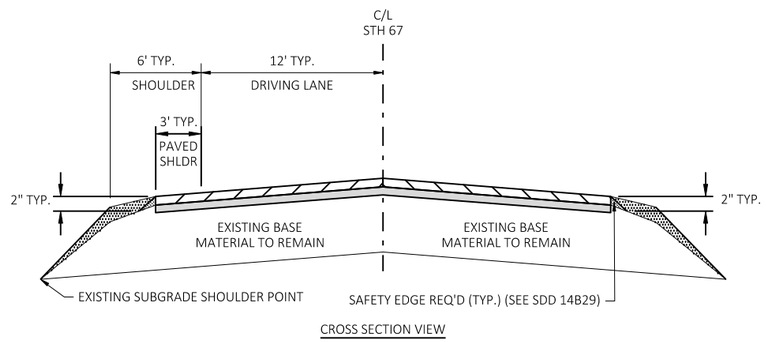




LONGITUDINAL PAVEMENT JOINT DETAIL - FIRST PASS

STA. 155+00 - STA. 357+62





LONGITUDINAL PAVEMENT JOINT DETAIL - SECOND PASS

STA 155+00 - 357+62

EXISTING SINGLE AGGREGATE BITUMINOUS PAVEMENT TO REMAIN

FIRST PASS 2-1/2" HMA PAVEMENT
4 MT 58-28 S PAVING LIMITS

SECOND PASS 2-1/2" HMA PAVEMENT
4 MT 58-28 S PAVING LIMITS

2" TYP. BASE AGGREGATE
DENSE 3/4-INCH

CROSS SECTION VIEW

FOR DROP OFFS 1 3/4" OR GREATER, INSTALL A NOTCHED WEDGE JOINT ACCORDING TO THE STANDARD DETAIL DRAWING.

SEE THE STANDARD DETAIL DRAWING FOR PERMANENT PAVEMENT MARKING DETAILS.

*REQUIRED ONLY IN LOCATIONS WHERE EXISTING PAVEMENT MARKING WILL NOT BE VISIBLE FOR MORE THAN 50' AFTER MILLING.

**LOCATING NO PASSING ZONES REQUIRED PRIOR TO PLACEMENT OF TEMPORARY MARKING LINE EPOXY 4-INCH. MARKING LINE EPOXY 4-INCH REQUIRED AFTER INSTALLING CENTERLINE RUMBLE STRIPS.

R4-1
24"X30"

R4-1 SIGNS SHALL BE INSTALLED AT THE BEGINNING OF EXISTING NO PASSING ZONES (OPPOSITE SIDE OF W14-3 SIGNS). ADDITIONAL R4-1 SIGNS SHALL BE INSTALLED AT ONE MILE INTERVALS WITHIN THE NO PASSING ZONE IF APPLICABLE.

W8-12
48"X48"

W8-12 SIGNS SHALL BE PLACED AT THE START OF THE ROADWAY SEGMENT THAT HAS NO MARKING. ADDITIONAL W8-12 SIGNS SHALL BE PLACED AT TWO MILE INTERVALS IF APPLICABLE.

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN: CONSTRUCTION DETAILS	SHEET 15	E

2

PI = STA. 97+23.44
Y = 707735.45
X = 925908.93
DELTA = 000°35'24"

PI = STA. 99+55.52
Y = 707967.52
X = 925907.00
DELTA = 000°29'22"

PI = STA. 101+77.66
Y = 708189.62
X = 925903.26
DELTA = 000°21'23"

PI = STA. 103+58.96
Y = 708370.88
X = 925899.07
DELTA = 000°07'45"

PI = STA. 106+57.38
Y = 708669.23
X = 925892.86
DELTA = 000°00'02"

N

11
T N
14 13
23 24
R E
16

ENVIRONMENTALLY SENSITIVE
AREA BDO-0112
(TOWNSHIP CEMETERY)

SECTION CORNER MONUMENT 36142
DO NOT DISTRUB

W JCT CTH S

BP: 95+76.93
Y = 707,588.93
X = 925,908.64

PI: 97+23.44

PI: 99+55.52

PI: 101+77.66

PI: 103+58.96

PI: 106+57.38

EXISTING RIGHT-OF-WAY

ASPH PE

ASPH PE

96+00 97+00 98+00 99+00 100+00 101+00 102+00 103+00 104+00 105+00 106+00 107+00 108+00 109+00 110+00

N0° 06' 48"E N0° 28' 36"W N0° 57' 57"W N1° 19' 21"W N1° 11' 36"W N1° 11' 38"W

BORE #1 BORE #2

STH 67

MATCHLINE- STA. 110+50

BEGIN PROJECT
STA. 99+50
Y = 707,962.00
X = 925,907.05
MATCH EXISTING
BUTT JOINT REQ'D
(SEE CONSTRUCTION
DETAIL)

EXISTING RIGHT-OF-WAY

NEDA CREEK

2

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 16	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021201_pd

PLOT DATE : 6/24/2021 5:09 PM PLOT BY : JASON DOLENS PLOT NAME : PLOT SCALE : 1 IN:100 FT

2

PI = STA. 111+78.20
Y = 709189.94
X = 925882.01
DELTA = 000°02'26"

PI = STA. 118+79.96
Y = 709891.56
X = 925867.88
DELTA = 000°38'45"

PI = STA. 123+75.72
Y = 710387.30
X = 925863.49
DELTA = 000°05'38"

N

2

ENVIRONMENTALLY SENSITIVE
AREA BDO-0112
(TOWNSHIP CEMETERY)

T 11 N
R 14 13 E
14 13
16

EXISTING CURB AND GUTTER
TO REMAIN

SECTION CORNER MONUMENT 36143
DO NOT DISTURB

LANDMARK REFERENCE MONUMENTS
SPECIAL

VERIFY LANDMARK REFERENCE
MONUMENTS

W NEDA RD

CULVERT TO BE
REPLACED BY
OTHERS PRIOR TO
CONSTRUCTION

PI: 111+78.20

PI: 118+79.96

PI: 123+75.72

EXISTING RIGHT-OF-WAY

CULVERT TO BE REPLACED BY
OTHERS PRIOR TO CONSTRUCTION

MATCHLINE- STA. 110+50

ASPH FE

BAD FE

BORE #8

ASPH PE

111+00 112+00 113+00 114+00 115+00 116+00 117+00 118+00 119+00 120+00 121+00 122+00 123+00 124+00 125+00

N1° 11' 38"W

STH 67

N1° 09' 12"W

BORE #3

N0° 30' 26"W

BORE #7

BORE #4

N0° 24' 49"W

ASPH FE

ASPH PE

ASPH FE

MATCHLINE- STA. 125+75

EXISTING RIGHT-OF-WAY

EXISTING CULVERT
TO REMAIN

EXISTING CURB AND GUTTER TO
REMAIN

W NEDA RD

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 17	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021202_pd

PLOT DATE : 6/24/2021 5:10 PM PLOT BY : JASON DOLENS PLOT NAME : PLOT SCALE : 1 IN:100 FT

2

PI = STA. 131+30.89
Y = 711142.45
X = 925858.04
DELTA = 000°05'00"

N

PI: 131+30.89

EXISTING RIGHT-OF-WAY

EXISTING CULVERT
TO REMAIN

ASPH PE

126+00 127+00 128+00 129+00 130+00 131+00 132+00 133+00 134+00 135+00 136+00 137+00 138+00 139+00 140+00 141

STH 67

N0° 24' 49"W

N0° 19' 49"W

MATCHLINE- STA. 125+75

MATCHLINE- STA. 141+00

EXISTING RIGHT-OF-WAY

2

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 18	E

2

PI = STA. 144+20.74
Y = 712432.28
X = 925850.61
DELTA = 000°04'22"

PI = STA. 152+76.31
Y = 713287.83
X = 925846.01
DELTA = 1°49'16"
D = 0°28'44"
T = 190.17'
L = 380.31'
R = 11965.49'
PC= STA. 150+86.14
Y = 713097.66
X = 925845.93
PT = STA. 154+66.44
Y = 713477.90
X = 925852.13

N

T 11 N
11 12
14 13
P 16 E

SECTION CORNER MONUMENT 36112
DO NOT DISTURB

LANDMARK REFERENCE MONUMENTS
SPECIAL

VERIFY LANDMARK REFERENCE
MONUMENTS

EXISTING CURB AND GUTTER TO
REMAIN, MATCH FLAG LINE

PI: 144+20.74

EXISTING RIGHT-OF-WAY

PC: 150+86.14

PT: 154+66.44

BAD PE

MATCHLINE- STA. 141+00

142+00 143+00 144+00 145+00 146+00 147+00 148+00 149+00 150+00 151+00 152+00 153+00 154+00 155+00 156+00

BORE #6
N0° 19' 49"W

STH 67

N0° 24' 10"W

BORE #5

N1° 14' 10"E

MATCHLINE- STA. 156+25

BAD PE

ASPH PE

EXISTING RIGHT-OF-WAY

EXISTING CULVERT
TO REMAIN

EXISTING FLUME TO REMAIN, FIELD FIT
MATCH AS DIRECTED BY THE FIELD
ENGINEER.

EXISTING CURB AND GUTTER
TO REMAIN

EXISTING RIGHT-OF-WAY

GRAYLOG RD

2

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 19	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021204_pd

PLOT DATE : 6/24/2021 5:12 PM PLOT BY : JASON DOLENS PLOT NAME : PLOT SCALE : 1 IN:100 FT

2

2

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 20	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021205_pd

PLOT DATE : 6/24/2021 5:14 PM PLOT BY : JASON DOLENS PLOT NAME : PLOT SCALE : 1 IN:100 FT

2

PI = STA. 179+80.58
Y = 715991.47
X = 925888.07
DELTA = 12°21'12"
D = 1°24'01"
T = 442.79'
L = 882.15'
R = 4091.45'
PC= STA. 175+37.79
Y = 715548.76
X = 925896.82
PT = STA. 184+19.94
Y = 716422.05
X = 925784.81

ENVIRONMENTALLY SENSITIVE
AREA 47DO186
(L. LUEBKE)

N

T 11 N
R 16 E
11 12
11 12

SECTION CORNER MONUMENT 36113

VERIFY LANDMARK REFERENCE MONUMENTS

OLIVER CREEK

EXISTING RIGHT-OF-WAY

PC: 175+37.79

PT: 184+19.94

EXISTING CULVERTS TO REMAIN

BAD PE

ASPH PE

STH 67

MATCHLINE- STA. 171+50

MATCHLINE- STA. 186+75

172+00 173+00 174+00 175+00 176+00 177+00 178+00 179+00 180+00 181+00 182+00 183+00 184+00 185+00 186+00

N1° 14' 10"E

N13° 31' 03"W

EXISTING RIGHT-OF-WAY

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 21	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021206_pd
PLOT DATE : 6/24/2021 5:15 PM PLOT BY : JASON DOLENS PLOT NAME : PLOT SCALE : 1 IN:100 FT
WISDOT/CADDS SHEET 44

PI = STA. 190+95.57
Y = 717078.97
X = 925626.89
DELTA = 000°06'04"

BENCHMARK/CONTROL POINT						
NO.	STATION	OFFSET	NORTHING	EASTING	ELEV.	DESCRIPTION
BM#1	196+13.9	94.4' RT.	717,605.037	925,598.459	971.28	FENO MONUMENT

N

ENVIRONMENTALLY SENSITIVE
AREA 47DO186
(L. LUEBKE)

ENVIRONMENTALLY SENSITIVE
AREA 47DO577
(TOBY LUEBKE FARM)

EXISTING RIGHT-OF-WAY

8' X 10' BOX CULVERT
STRUCTURE C-14-017
TO REMAIN

AT&T (TELEPHONE)

PI: 190+95.57

PC: 201+71.68

MATCHLINE- STA. 186+75

187+00 188+00 189+00 190+00 191+00 192+00 193+00 194+00 195+00 196+00 197+00 198+00 199+00 200+00 201+00 202

STH 67

N13° 31' 03"W

N13° 25' 00"W

MATCHLINE- STA. 202+00

BAD PE

BM#1

INSTALL 'ASPHALTIC
INTERSECTION RUMBLE
STRIPS' PER SDD 13A8 (TYP.)

EXISTING CULVERT
TO REMAIN

EXISTING RIGHT-OF-WAY

MINE RD

EXISTING RIGHT-OF-WAY

WISCONSIN & SOUTHERN RAILROAD

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 22	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021207_pd
PLOT DATE : 6/24/2021 5:16 PM PLOT BY : JASON DOLENS PLOT NAME : PLOT SCALE : 1 IN:100 FT
WISDOT/CADDS SHEET 44

2

2

BENCHMARK/CONTROL POINT

NO.	STATION	OFFSET	NORTHING	EASTING	ELEV.	DESCRIPTION
BM#2	204+38.4	126.7' LT.	718,363.951	925,194.063	973.04	FENO MONUMENT

PI = STA. 211+20.61
Y = 719040.98
X = 925126.71
DELTA = 36°36'10"
D = 1°59'49"
T = 948.92'
L = 1832.86'
R = 2869.03'
PC= STA. 201+71.68
Y = 718125.71
X = 925377.20
PT = STA. 220+04.54
Y = 719925.10
X = 925471.36

N

BP: 33+00.00
EXISTING CULVERT TO REMAIN
ALLIANT ENERGY (OVERHEAD - ELEC)
ENVIRONMENTALLY SENSITIVE AREA 47DO186 (L. LUEBKE)
WE ENERGIES (GAS)
AT&T (TELEPHONE)
STRUCTURE RESTORATION
HMA PAVEMENT WIDENING (SEE CONSTRUCTION DETAILS)
BM#2
EXISTING CULVERT TO REMAIN
INSTALL 'ASPHALTIC INTERSECTION RUMBLE STRIPS' PER SDD 13A8 (TYP.)
CULVERT TO BE REPLACED BY OTHERS PRIOR TO CONSTRUCTION
EXISTING RIGHT-OF-WAY
AT&T (TELEPHONE)
ALLIANT ENERGY (OVERHEAD - ELEC)
BAD PE
EXISTING CULVERT TO REMAIN
BAD PE
EXISTING CULVERTS TO REMAIN
BAD PE
STH 33
STH 67
34+00
35+00
36+00
37+00
38+00
39+00
40+00
41+00
42+00
203+00
204+00
205+00
206+00
207+00
208+00
209+00
210+00
211+00
212+00
213+00
214+00
215+00
216+00
217+00
MATCHLINE- STA. 202+00
MATCHLINE- STA. 217+25
SAWING ASPHALT REQ'D. (TYP.)
STA. 203+33.35 = STA. 38+00.04
Y = 718,282.76
X = 925,338.94
HMA PAVEMENT WIDENING (SEE CONSTRUCTION DETAILS)
6' X 6' BOX CULVERT STRUCTURE C-14-019 TO REMAIN
SEE CONSTRUCTION DETAIL SHEET FOR ADDITIONAL OVERHEAD SIGN INSTALLATION DETAILS.
SEE ADDITIONAL PLAN SHEET FOR INTERSECTION DETAILS.
EXISTING RIGHT-OF-WAY
AT&T (TELEPHONE)
WISCONSIN & SOUTHERN RAILROAD

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 23	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021208_pd
PLOT DATE : 6/24/2021 5:18 PM PLOT BY : JASON DOLENS PLOT NAME : PLOT SCALE : 1 IN:100 FT
WISDOT/CADDS SHEET 44

2

BENCHMARK/CONTROL POINT						
NO.	STATION	OFFSET	NORTHING	EASTING	ELEV.	DESCRIPTION
BM#3	220+79.4	65.8' LT.	720,018.521	925,436.642	982.00	HMOD MONUMENT

PI = STA. 229+38.97
Y = 720800.20
X = 925798.90
DELTA = 13°13'12"
D = 2°02'09"
T = 326.11'
L = 649.32'
R = 2814.20'
PC= STA. 226+12.86
Y = 720493.38
X = 925688.41
PT = STA. 232+62.19
Y = 721124.16
X = 925836.30

N

MATCHLINE- STA. 217+25

MATCHLINE- STA. 232+50

WISCONSIN & SOUTHERN RAILROAD

RAILROAD RIGHT-OF-WAY

EXISTING RIGHT-OF-WAY

EXISTING CULVERT TO REMAIN

ALLIANT ENERGY (OVERHEAD - ELEC)

AT&T (TELEPHONE)

EXACT BUTT JOINT LIMITS TO BE DETERMINED BY FIELD ENGINEER

EXISTING CULVERT TO REMAIN

EXISTING CULVERT TO REMAIN

PT: 220+04.54

PC: 226+12.86

BM#3

BAD PE

BAD PE

STH 67

N20° 54' 16"E

218+00 219+00 220+00 221+00 222+00 223+00 224+00 225+00 226+00 227+00 228+00 229+00 230+00 231+00 232+00

EXISTING RIGHT-OF-WAY

EXISTING CULVERT TO REMAIN

RAILROAD RECONSTRUCTION (BY OTHERS) BUTT JOINT REQ'D

EXISTING CULVERT TO REMAIN

WISCONSIN & SOUTHERN RAILROAD

LIMESTONE RD

EXISTING RIGHT-OF-WAY

T 11 N
R 16 E
2 1
2 1

SECTION CORNER MONUMENT 36023 DO NOT DISTURB

LANDMARK REFERENCE MONUMENTS SPECIAL

VERIFY LANDMARK REFERENCE MONUMENTS

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 24	E

FILE NAME : G:\00\00093\000093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021209_pd

PLOT DATE : 6/24/2021 5:19 PM PLOT BY : JASON DOLENS PLOT NAME : PLOT SCALE : 1 IN:100 FT

2

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 25	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021210_pd
PLOT DATE : 6/24/2021 5:20 PM
PLOT BY : JASON DOLENS
PLOT NAME :
PLOT SCALE : 1 IN:100 FT
WISDOT/CADDS SHEET 44

2

PI = STA. 249+73.34
Y = 722834.49
X = 925855.93
DELTA = 16°42'19"
D = 1°57'04"
T = 431.14'
L = 856.17'
R = 2936.46'
PC= STA. 245+42.20
Y = 722403.39
X = 925861.73
PT = STA. 253+98.36
Y = 723245.74
X = 925726.46

WISCONSIN & SOUTHERN RAILROAD

N

EXISTING RIGHT-OF-WAY

PT: 253+98.36

MATCHLINE- STA. 247+75

248+00 249+00 ASPH PE 250+00 251+00 252+00 253+00 254+00 255+00 256+00 257+00 258+00 259+00 260+00 261+00 262+00 263

BAD PE

STH 67

N18° 07' 30"W

BAD FE

BAD PE

MATCHLINE- STA. 263+00

EXISTING RIGHT-OF-WAY

T 11 N
R 16 E
35 36
2 1

SECTION CORNER MONUMENT 36024
DO NOT DISTRUB

VERIFY LANDMARK REFERENCE
MONUMENTS

T 11 N
R 16 E
35 36
TOWN OF HUBBARD

SECTION CORNER MONUMENT 46352
DO NOT DISTRUB

VERIFY LANDMARK REFERENCE
MONUMENTS

2

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 26	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021211_pd

PLOT DATE : 6/24/2021 5:21 PM PLOT BY : JASON DOLENS PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

2

2

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 27	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021212_pd

PLOT DATE : 6/24/2021 5:23 PM PLOT BY : JASON DOLENS PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

2

PI = STA. 287+46.46
Y = 726428.37
X = 924686.96
DELTA = 17°58'35"
D = 1°32'22"
T = 588.71'
L = 1167.75'
R = 3721.95'
PC= STA. 281+57.74
Y = 725868.20
X = 924868.04
PT = STA. 293+25.50
Y = 727017.08
X = 924687.61

N

EXISTING RIGHT-OF-WAY

WISCONSIN & SOUTHERN RAILROAD

EXISTING RIGHT-OF-WAY

EXISTING CULVERT
TO REMAIN

PC: 281+57.74

PT: 293+25.50

BAD PE

279+00 280+00 281+00 282+00 283+00 284+00 285+00 286+00 287+00 288+00 289+00 290+00 291+00 292+00 293+00

N18° 07' 30"W

STH 67

BAD PE

BAD PE

EXISTING RIGHT-OF-WAY

MATCHLINE- STA. 278+25

MATCHLINE- STA. 293+50

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 28	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021213_pd
PLOT DATE : 6/24/2021 5:24 PM
PLOT BY : JASON DOLENS
PLOT NAME :
PLOT SCALE : 1 IN:100 FT
WISDOT/CADDS SHEET 44

2

WISCONSIN & SOUTHERN RAILROAD

PI = STA. 304+57.19
Y = 728148.45
X = 924714.59
DELTA = 000°15'14"

N

EXISTING RIGHT-OF-WAY

PI: 304+57.19

ASPH PE

294+00 295+00 296+00 297+00 298+00 299+00 300+00 301+00 302+00 303+00 304+00 305+00 306+00 307+00 308+00

STH 67

N1° 21' 59"E

N1° 37' 13"E

ASPH PE

BAD PE

EXISTING RIGHT-OF-WAY

MATCHLINE - STA. 293+50

MATCHLINE - STA. 308+75

2

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 29	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021214_pd

PLOT DATE : 6/24/2021 5:25 PM PLOT BY : JASON DOLENS PLOT NAME : PLOT SCALE : 1 IN:100 FT

2

PI = STA. 318+09.22
Y = 729499.94
X = 924752.82
DELTA = 000°01'34"

N

EXISTING RIGHT-OF-WAY

CULVERT TO BE REPLACED
BY OTHERS PRIOR
TO CONSTRUCTION

PI: 318+09.22

BAD PE

MATCHLINE- STA. 308+75

309+00 310+00 311+00 312+00 313+00 314+00 315+00 316+00 317+00 318+00 319+00 320+00 321+00 322+00 323+00

STH 67

N1° 37' 13"E

N1° 38' 47"E

MATCHLINE- STA. 323+75

BAD PE

EXISTING RIGHT-OF-WAY

EXISTING RIGHT-OF-WAY

ZION CHURCH RD

2

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 30	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021215_pd

PLOT DATE : 6/24/2021 5:26 PM PLOT BY : JASON DOLENS PLOT NAME : PLOT SCALE : 1 IN:100 FT

2

PI = STA. 323+87.46
Y = 730077.94
X = 924769.44
DELTA = 000°41'59"

PI = STA. 326+08.76
Y = 730299.21
X = 924773.09
DELTA = 000°27'48"

PI = STA. 330+38.68
Y = 730729.12
X = 924776.72
DELTA = 000°02'53"

PI = STA. 331+39.98
Y = 730830.42
X = 924777.49
DELTA = 000°07'32"

PI = STA. 333+05.19
Y = 730995.62
X = 924779.11
DELTA = 000°20'52"

PI = STA. 334+92.48
Y = 731182.90
X = 924779.81
DELTA = 000°19'03"

UNNAMED STREAM

PI = STA. 337+01.46
Y = 731391.88
X = 924781.74
DELTA = 000°06'59"

N

SAWING ASPHALT REQ'D. (TYP.)

HMA PAVEMENT WIDENING
(SEE CONSTRUCTION DETAILS)

EXISTING RIGHT-OF-WAY

PI: 323+87.46 | PI: 326+08.76 | PI: 330+38.68 | PI: 331+39.98 | PI: 333+05.19 | PI: 334+92.48 | PI: 337+01.46

ASPH PE

BAD PE

ASPH PE

324+00 | 325+00 | 326+00 | 327+00 | 328+00 | 329+00 | 330+00 | 331+00 | 332+00 | 333+00 | 334+00 | 335+00 | 336+00 | 337+00 | 338+00

N0° 56' 48"E | N0° 29' 01"E | N0° 26' 08"E | N0° 33' 41"E | N0° 12' 48"E | N0° 31' 51"E | N0° 38' 51"E

STH 67

MATCHLINE- STA. 323+75

MATCHLINE- STA. 338+75

ASPH PE

BAD PE

EXISTING RIGHT-OF-WAY

2

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 31	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021216_pd
PLOT DATE : 6/24/2021 5:27 PM
PLOT BY : JASON DOLENS
PLOT NAME :
PLOT SCALE : 1 IN:100 FT
WISDOT/CADDS SHEET 44

2

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 32	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021217_pd
PLOT DATE : 6/24/2021 5:28 PM PLOT BY : JASON DOLENS PLOT NAME : PLOT SCALE : 1 IN:100 FT
WISDOT/CADDS SHEET 44

2

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 33	E

2

2

PI = STA. 375+54.22
Y = 735220.62
X = 925091.87
DELTA = 7°15'24"
D = 1°55'38"
T = 188.52'
L = 376.53'
R = 2972.89'
PC= STA. 373+65.71
Y = 735032.12
X = 925089.60
PT = STA. 377+42.24
Y = 735407.32
X = 925117.94

ENVIRONMENTALLY SENSITIVE
AREA BDO-0116
(GRACELAND CEMETERY)

EXISTING INLET TO REMAIN
INLET PROTECTION TYPE D

EXISTING CURB & GUTTER TO REMAIN
TIE INTO FLAG LINE (TYP.)

PT: 368+34.31

PC: 373+65.71

PT: 377+42.24

RUEDEBUSCH AVE

EP: 379+12.80
Y = 735,575.92
X = 925,143.81

S SCHOOL ST

STH 67

MAIN STREET

N0° 11' 47"W

N8° 43' 23"E

MATCHLINE- STA. 367+00

368+00 369+00 370+00 371+00 372+00 373+00 374+00 375+00 376+00 377+00 378+00 379+00

STA. 362+95 - STA. 375+85
CROSS SLOPE CORRECTION
(SEE CONSTRUCTION DETAIL)

EXISTING MGS BEAM GUARD TO REMAIN,
(SEE CONSTRUCTION DETAIL)

END PROJECT
STA. 377+15.20
Y = 735,380.53
X = 925,114.33
MATCH EXISTING
CONCRETE HEADER
BUTT JOINT REQ'D

ROCK RIVER

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 34	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0212_PD - PLAN DETAILS\0212_PD.DWG
LAYOUT NAME - 021219_pd

PLOT DATE : 6/24/2021 5:31 PM PLOT BY : JASON DOLENS PLOT NAME : PLOT SCALE : 1 IN:100 FT

2

2

BENCHMARK/CONTROL POINT						
NO.	STATION	OFFSET	NORTHING	EASTING	ELEV.	DESCRIPTION
BM#2	204+38.4	126.7' LT.	718,363.951	925,194.063	973.04	FENO MONUMENT

N

ALLIANT ENERGY (OVERHEAD - ELEC)

AT&T (TELEPHONE)

EXISTING RIGHT-OF-WAY

WENNINGER'S CORNER LLC
W2956 STATE ROAD 33

HMA PAVEMENT WIDENING (SEE CONSTRUCTION DETAILS)

EXISTING INLET TO REMAIN, INLET PROTECTION TYPE D

ALLIANT ENERGY (OVERHEAD - ELEC)

BM#2

EXISTING CULVERTS AND STORM SEWER TO REMAIN

BEGIN CONSTRUCTION
STA. 36+91
Y = 718,283.77
X = 925,230.49
MATCH EXISTING BUTT JOINT REQ'D
(SEE CONSTRUCTION DETAIL)

STH 67

205+00

204+00

SAWING ASPHALT REQ'D. (TYP.)

HMA PAVEMENT WIDENING (SEE CONSTRUCTION DETAILS)

SEE CONSTRUCTION DETAILS FOR OVERHEAD PERMANENT SIGN INSTALLATION (TYP.)

EXISTING RIGHT-OF-WAY

JOHN & TAMMY SCHWEITZER
W2912 STATE ROAD 33

6' X 6' BOX CULVERT STRUCTURE C-14-019 TO REMAIN

EXISTING INLET TO REMAIN INLET PROTECTION TYPE D

END CONSTRUCTION
STA. 39+30
Y = 718,281.56
X = 925,469.26
MATCH EXISTING BUTT JOINT REQ'D
(SEE CONSTRUCTION DETAIL)

AT&T (TELEPHONE)

BAD PE

STA. 38+00.04 = STA. 203+33.35
Y = 718,282.76
X = 925,338.94

S89°28'09"E

35+00 36+00 37+00 38+00 39+00 40+00 41+00

STH 33

STH 33

STRUCTURE RESTORATION

HMA PAVEMENT WIDENING (SEE CONSTRUCTION DETAILS)

203+00

ASPH PE

EXISTING RIGHT-OF-WAY

HMA PAVEMENT WIDENING (SEE CONSTRUCTION DETAILS)

EXISTING CURB & GUTTER, TO REMAIN, TIE INTO FLAG LINE (TYP.)

EXISTING INLETS TO REMAIN INLET PROTECTION TYPE D

CAUTION

WE ENERGIES (GAS)

EXISTING RIGHT-OF-WAY

ALLIANT ENERGY (OVERHEAD -ELEC)

JAMES PERSHAR
W2911 STATE ROAD 33

HORTENSE LUEBKE
W2969N STATE ROAD 33

202+00

STH 67

PC: 201+71.68

AT&T (TELEPHONE)

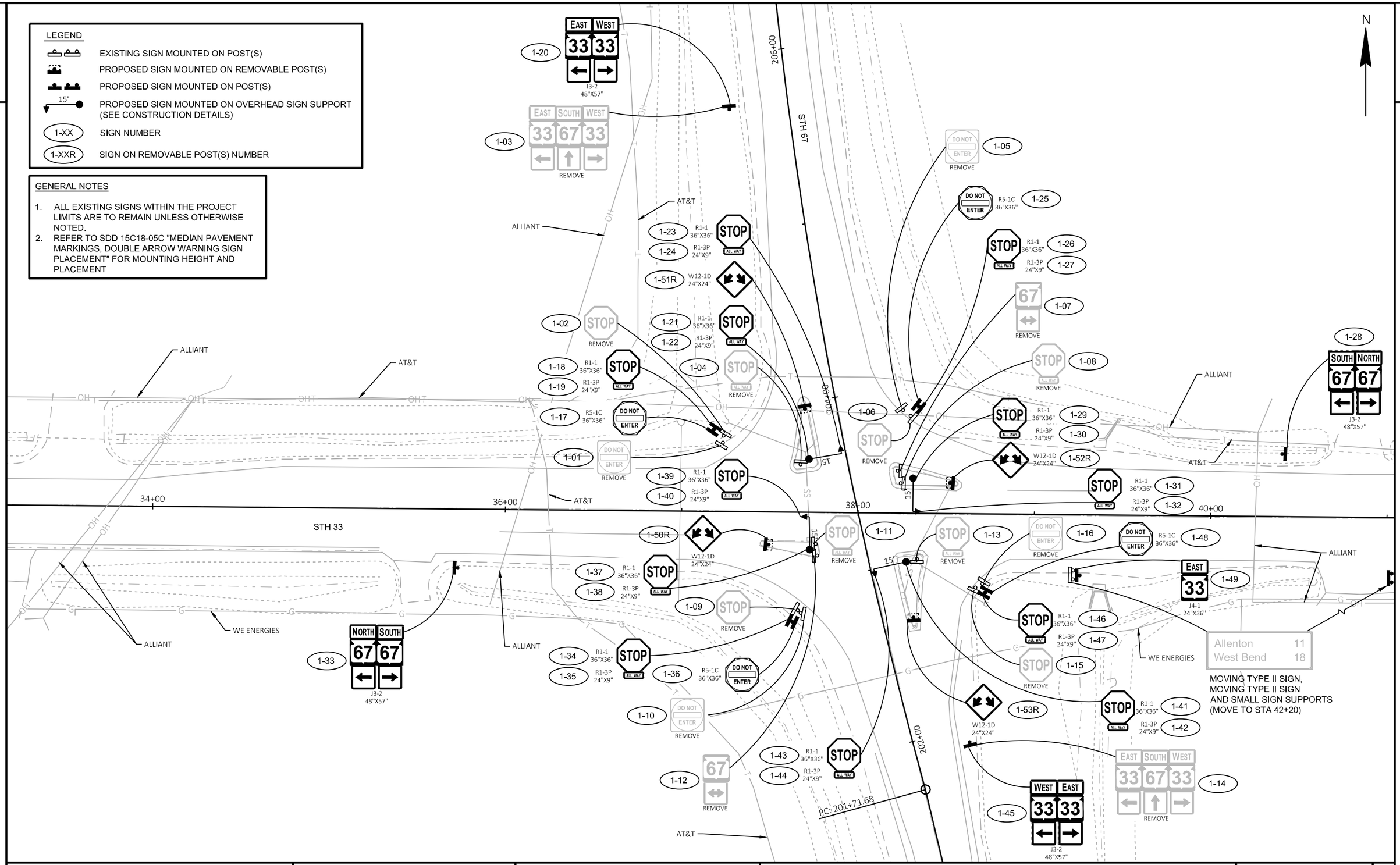
PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PLAN DETAILS	SHEET 35	E

LEGEND

- EXISTING SIGN MOUNTED ON POST(S)
- PROPOSED SIGN MOUNTED ON REMOVABLE POST(S)
- PROPOSED SIGN MOUNTED ON POST(S)
- 15' — PROPOSED SIGN MOUNTED ON OVERHEAD SIGN SUPPORT (SEE CONSTRUCTION DETAILS)
- 1-XX — SIGN NUMBER
- 1-XXR — SIGN ON REMOVABLE POST(S) NUMBER

GENERAL NOTES

1. ALL EXISTING SIGNS WITHIN THE PROJECT LIMITS ARE TO REMAIN UNLESS OTHERWISE NOTED.
2. REFER TO SDD 15C18-05C "MEDIAN PAVEMENT MARKINGS, DOUBLE ARROW WARNING SIGN PLACEMENT" FOR MOUNTING HEIGHT AND PLACEMENT



PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	PERMANENT SIGNING	SHEET 36	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0232_PS - SIGNING\SIGNING.DWG
LAYOUT NAME - Plan 1 IN 50 FT
PLOT DATE : 7/8/2021 2:15 PM
PLOT BY : KEVIN KLOCKZIEM
PLOT NAME :
PLOT SCALE : 1 IN:50 FT

1. ROAD WORK AHEAD W20-1A
2. RUMBLE STRIPS AHEAD W21-6S
3. ONE LANE ROAD AHEAD W20-4A
4. BE PREPARED TO STOP W03-4
5. W20-7A
6. END ROAD WORK G20-2A 48"X24"

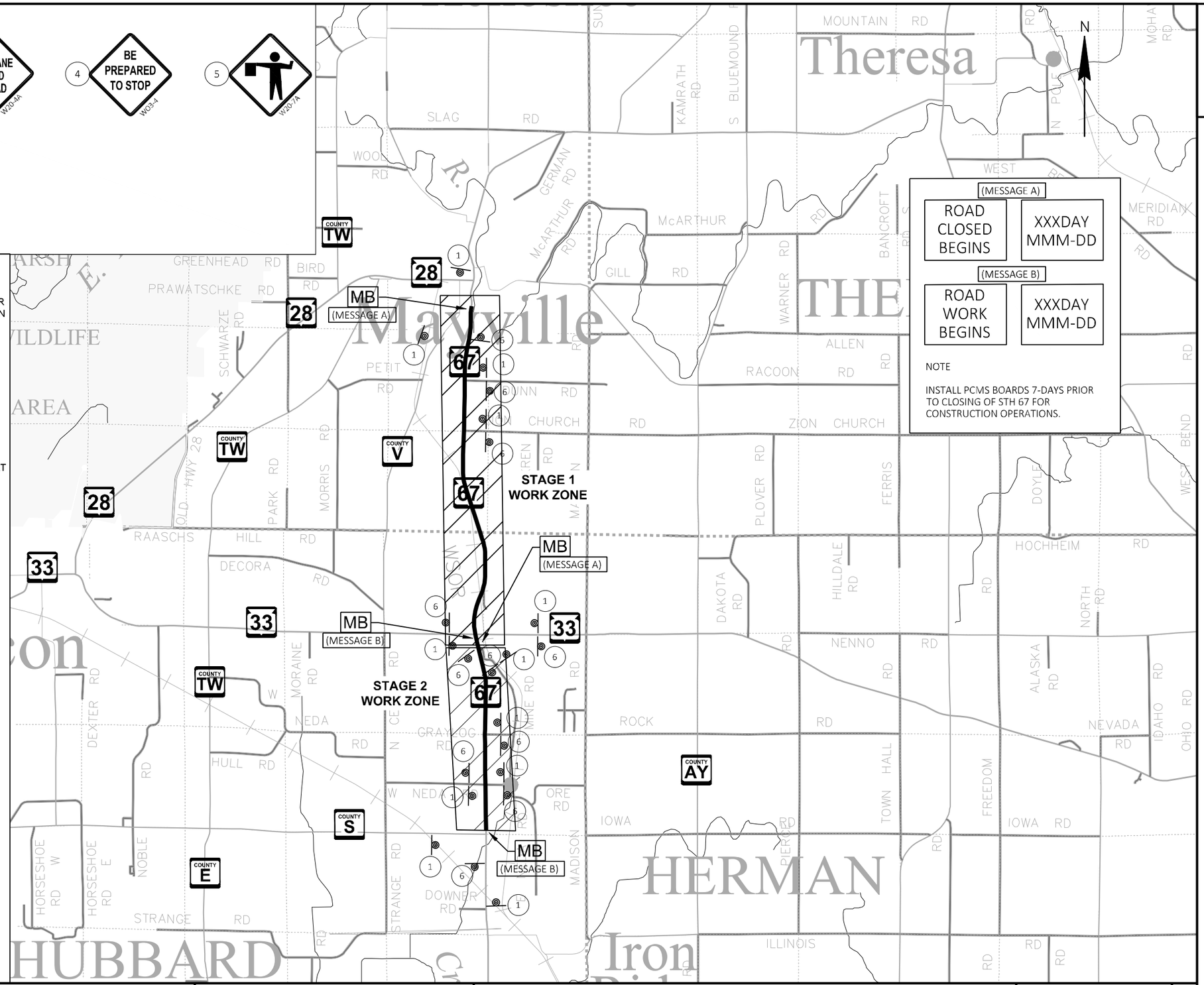
GENERAL NOTES:

- TRAFFIC CONTROL FOR ALL MILLING, PAVING, AGGREGATE SHOULDER AND RUMBLE STRIP WORK SHALL FOLLOW S.D.D. "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION". (TRAFFIC CONTROL SHOWN AT THIS LOCATION FOR PLAN SHEET PURPOSES ONLY)
- TRAFFIC CONTROL FOR ALL PAVEMENT MARKING WORK SHALL FOLLOW S.D.D. "MOVING PAVEMENT MARKING OPERATION-TWO LANE TWO-WAY ROADWAY."
- TRAFFIC CONTROL FOR ANY WORK REQUIRING A SHOULDER CLOSURE SHALL FOLLOW S.D.D. "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY."
- FLAGGERS ARE REQUIRED WHEN WORK IS BEING COMPLETED AT INTERSECTIONS TO MAINTAIN VEHICULAR ACCESS TO STH 67.
- SIGN FACES SHALL BE IN ACCORDANCE WITH THE FEDERAL HIGHWAY ADMINISTRATION MANUAL OF STANDARD HIGHWAY SIGNS, UNLESS OTHERWISE NOTED.
- DO NOT ALLOW TRAFFIC TO STOP BETWEEN THE STOP LINE MARKINGS AT ANYTIME. ADDITIONAL FLAGGERS MAY BE REQUIRED WHEN WORKING NEAR RAILROAD TRACKS.
- TRAFFIC CONTROL PLANS ARE NOT TO SCALE.
- MAINTAIN ACCESS TO DRIVEWAYS AND SIDE ROADS AT ALL TIMES,
- WO SIGNS ARE THE SAME AS W SIGNS EXCEPT THE BACKGROUND IS ORANGE.

5
4 SIGNS TO BE PLACED DURING FLAGGING OPERATIONS ON BOTH ENDS OF THE ACTIVE WORK ZONE, PER SDD 15C12
3
2

LEGEND

- SIGN ON PORTABLE OR PERMANENT SUPPORT
- MB PCMS MESSAGE BOARDS
- FLAGGER
- WORK ZONE



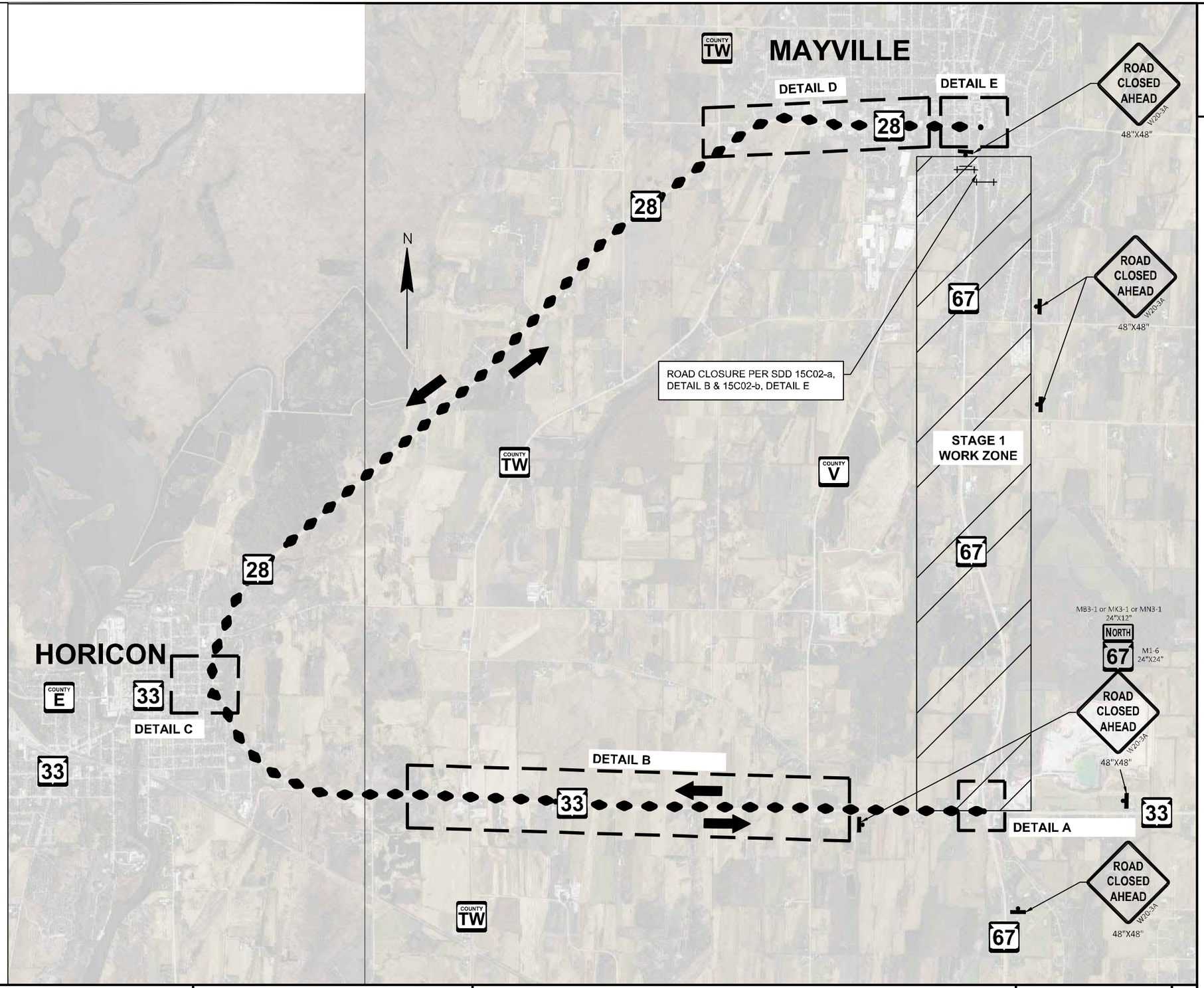
PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	TRAFFIC CONTROL OVERVIEW	SHEET 37	E

GENERAL NOTES:

1. ALL TC COVERING SIGNS TYPE I/II SHALL BE PAID AS EACH CONTIGUOUS SIGN(S) COVERED PER LOCATION PER CYCLE NOT AS INDIVIDUAL CLUSTER.
2. REFER TO SDD "DETOUR SIGNING FOR MAINLINE CLOSURES" FOR SIGN SIZE AND COLOR SPECIFICATIONS.
3. REFER TO PLATE A4-12 WHEN COVERING ARROWS FOR DESTINATION DIRECTIONAL ARROWS.
4. DRAWINGS SHOW TC DETOUR FOR A TYPICAL SITUATION. ADDITIONAL TC DEVICES MAY BE REQUIRED AND/OR LAYOUT DETAILS MODIFIED DEPENDING ON SITE CONDITIONS AS DIRECTED BY THE ENGINEER. ALL CHANGES TO THE TC DETOUR PLAN SHALL BE REVIEWED WITH THE PROJECT ENGINEER.
5. SIGN FACES SHALL BE IN ACCORDANCE WITH THE FEDERAL HIGHWAY ADMINISTRATION MANUAL OF STANDARD HIGHWAY SIGNS, UNLESS OTHERWISE NOTED.
6. TRAFFIC CONTROL PLANS ARE NOT TO SCALE.
7. COVER, REMOVE, OR ALTER ANY EXISTING OR PROPOSED SIGNS THAT DISPLAY A CONFLICTING MESSAGE WITH THE PROPOSED DETOUR ROUTE FOR THE PHASE THAT IS ACTIVE.
8. MAINTAIN ACCESS TO DRIVEWAYS SIDE ROADS AT ALL TIMES.
9. WO SIGNS ARE THE SAME AS W SIGNS EXCEPT THE BACKGROUND IS ORANGE.

LEGEND

- DETOUR ROUTE
- EXISTING / PROPOSED TC SIGNS
- TC COVERING SIGNS TYPE II
- TC BARRICADES TYPE III WITH MOUNTED TC SIGNS
- TC DRUM / TYPE A/C LIGHT
- DIRECTION OF TRAFFIC
- FLAGGER
- WORK ZONE



PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	DETOUR - STAGE 1 OVERVIEW	SHEET 38	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0270_DT - DETOUR\0270_DT - DETOUR.DWG
LAYOUT NAME - 027001_dt
PLOT DATE : 6/24/2021 2:23 PM PLOT BY : KEVIN KLOCKZIEM PLOT NAME : PLOT SCALE : Custom

2

2

N

STAGE 1 WORK ZONE

ROAD CLOSURE PER SDD 15C02-a, DETAIL B & 15C02-b, DETAIL E

INTERSECTION TO BE CONSTRUCTED IN HALVES UNDER FLAGGING IN STAGE 1A/1B. FLAGGING PER SDD 15C12.

DETOUR NEXT 10 MILES
G20-51
60"X24"

M4-8
24"X12"
M3-1
24"X12"
M1-6
24"X24"
DETOUR
NORTH
67

WEST
33
Horicon 4
Beaver Dam 15

67
MO6-6L
21"X21"
(COVER THESE SIGN WHEN WB LANE IS CLOSED)

WO1-6
48" X 24"
TYPE III BARRICADE
BARRICADE AND DRUMS WHEN WB LANE IS CLOSED

500'

W20-7A
48"X48"

STH 33

STAGE 1B UTILIZE ROAD CLOSURE PER SDD 15C02-b, DETAIL D

M4-8
24"X12"
DETOUR
SOUTH WEST NORTH
67 33 67
MO6-1
21"X21"

M3-1
24"X12"
NORTH
67
M1-6
24"X24"
DETOUR AHEAD
W20-2A
48"X48"

500'
300'

Horicon
Iron Ridge
Mayville
REFER TO PLATE A4-12

M4-8A
24"X18"
END DETOUR
NORTH EAST SOUTH
67 33 67

Allenton
Mayville
Iron Ridge

W20-7A
48"X48"

WO1-6
48" X 24"
TYPE III BARRICADE
BARRICADE AND DRUMS WHEN EB LANE IS CLOSED.

SOUTH
67
M3-3
24"X12"
MO6-1
21"X21"
(COVER THESE SIGNS WHEN EB LANE IS CLOSED)

M4-8
24"X12"
DETOUR
WEST NORTH EAST
33 67 33
MO6-1
21"X21"

STH 67

W20-7A
48"X48"

REFER TO PLATE A4-12
Mayville
Horicon
West Bend

ROAD CLOSED 500 FT
W20-3D
48"X48"

M4-8
24"X12"
DETOUR
WEST NORTH EAST
33 67 33
MO5-1L
21"X21"

300'

M3-1
24"X12"
NORTH
67
M1-6
24"X24"
DETOUR AHEAD
W20-2A
48"X48"

LEGEND

- WORK AREA
- TRAFFIC CONTROL DRUM WITH/WITHOUT LIGHT TYPE C
- NEW TRAFFIC CONTROL SIGN ON PERMANENT SUPPORT
- EXISTING TRAFFIC CONTROL SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE WITH ATTACHED TRAFFIC CONTROL SIGN
- TRAFFIC FLOW ARROW

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	DETOUR - STAGE 1 - DETAIL A	SHEET 39	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0270_DT - DETOUR\0270_DT - DETOUR.DWG
LAYOUT NAME - 027002_dt

PLOT DATE : 6/24/2021 2:24 PM PLOT BY : KEVIN KLOCKZIEM PLOT NAME : PLOT SCALE : 1 IN:200 FT

2

2

N

DECORA RD

CTH TW

CTH V

N CEDAR RD

STH 33

DETOUR NORTH 67
M4-8 24"X12"
M3-1 24"X12"
M1-6 24"X24"

DETOUR SOUTH 67
EAST 33
M4-8 24"X12"
M3-3 24"X12"
M1-6 24"X24"

LEGEND

- WORK AREA
- TRAFFIC CONTROL DRUM WITH/WITHOUT LIGHT TYPE C
- NEW TRAFFIC CONTROL SIGN ON PERMANENT SUPPORT
- EXISTING TRAFFIC CONTROL SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE WITH ATTACHED TRAFFIC CONTROL SIGN
- TRAFFIC FLOW ARROW

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	DETOUR - STAGE 1 - DETAIL B	SHEET 40	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0270_DT - DETOUR\0270_DT - DETOUR.DWG
LAYOUT NAME - 027003_dt

PLOT DATE : 6/24/2021 2:26 PM PLOT BY : KEVIN KLOCKZIEM PLOT NAME : PLOT SCALE : Custom

2

2

N

WALNUT ST

WALNUT ST

WALNUT ST

PALMATORY ST

PALMATORY ST

STH 28 / CLASON ST

STH 33 / W LAKE ST

STH 33 / E LAKE ST

WIND TRAIL

DETOUR
NEXT 5 MILES
G20-51
60"X24"

M4-8
24"X12"
M3-3
24"X12"
M1-6
24"X24"
MO5-1L
21"X21"

DETOUR
SOUTH
67

M4-8
M3-1
24"X12"
M1-6
24"X24"

DETOUR
NORTH
67

WEST
33

EAST
28

M4-8
24"X12"
M3-1
24"X12"
M1-6
24"X24"
MO6-1
21"X21"

DETOUR
NORTH
67

500'

M3-1
24"X12"
NORTH
67
M1-6
24"X24"

DETOUR
AHEAD
W20-2A
48"X48"

M4-8
24"X12"
M3-1
24"X12"
M1-6
24"X24"

DETOUR
NORTH
67

SECOND
LEFT
MO4-22L
24"X21"

EAST
28
33

M4-8
24"X12"
M3-1
24"X12"
M1-6
24"X24"
MO6-1
21"X21"

DETOUR
NORTH
67

EAST
28

EAST
33

M4-8
24"X12"
M3-3
24"X12"
M1-6
24"X24"
MO6-1
21"X21"

DETOUR
SOUTH
67

EAST
33
33
WEST

M4-8
24"X12"
M3-3
24"X12"
M1-6
24"X24"

DETOUR
SOUTH
67

DETOUR
NEXT 4 MILES
G20-51
60"X24"

M4-8
24"X12"
M3-1
24"X12"
M1-6
24"X24"
MO5-1R
21"X21"

DETOUR
NORTH
67

75'

WEST
33
EAST
28

LEGEND

WORK AREA

TRAFFIC CONTROL DRUM WITH/WITHOUT LIGHT TYPE C

NEW TRAFFIC CONTROL SIGN ON PERMANENT SUPPORT

EXISTING TRAFFIC CONTROL SIGN ON PERMANENT SUPPORT

TYPE III BARRICADE WITH ATTACHED TRAFFIC CONTROL SIGN

TRAFFIC FLOW ARROW

HORICON

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	DETOUR - STAGE 1 - DETAIL C	SHEET 41	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0270_DT - DETOUR\0270_DT - DETOUR.DWG
LAYOUT NAME - 027004_dt

PLOT DATE : 6/24/2021 2:26 PM PLOT BY : KEVIN KLOCKZIEM PLOT NAME : PLOT SCALE : 1 IN:100 FT

2

2

N

MAYVILLE

SHEET STAGE 1 - DETAIL E

STH 28

BIRD RD

CTH TW

MOUNTAIN DR

CLARK ST

EWALD ST

HENNINGER ST

COMMERCE ST

MUZZY ST

OAK ST

RUEDEBUSCH AVE

CTH V

APPROXIMATE STAGE 1 WORK ZONE

DETOUR SOUTH 67 — M4-8 24"X12" M3-3 24"X12" M1-6 24"X24"

DETOUR NEXT 8 MILES — G20-51 60"X24"

DETOUR NORTH 67 — M4-8 24"X12" M3-1 24"X12" M1-6 24"X24"

DETOUR SOUTH 67 — M4-8 24"X12" M3-3 24"X12" M1-6 24"X24"

DETOUR NORTH 67 — M4-8 24"X12" M3-1 24"X12" M1-6 24"X24"

LEGEND

- WORK AREA
- TRAFFIC CONTROL DRUM WITH/WITHOUT LIGHT TYPE C
- NEW TRAFFIC CONTROL SIGN ON PERMANENT SUPPORT
- EXISTING TRAFFIC CONTROL SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE WITH ATTACHED TRAFFIC CONTROL SIGN
- TRAFFIC FLOW ARROW

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	DETOUR - STAGE 1 - DETAIL D	SHEET 42	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0270_DT - DETOUR\0270_DT - DETOUR.DWG
LAYOUT NAME - 027005_dt

PLOT DATE : 6/24/2021 2:28 PM PLOT BY : KEVIN KLOCKZIEM PLOT NAME : PLOT SCALE : Custom

2

MAYVILLE

WILLIAMS ST

ROCK RIVER

N

BRIDGE ST

BRIDGE ST

ALLEN ST

ALLEN ST

MUZZY ST

WALNUT ST

JOHN ST / CTH V

SCHOOL ST

STH 28 / N MAIN ST

STH 28

COMMERCE ST

WALNUT ST

JOHN ST / CTH V

SCHOOL ST

STH 67 / S MAIN ST

ROCK RIVER

SOUTH 67 M3-3 24"X12" M1-6 24"X24"

DETOUR AHEAD W20-2A 48"X48"

DETOUR SOUTH 67 M4-8 24"X12" M3-3 24"X12" M1-6 24"X24"

DETOUR SOUTH WEST 67 28 M4-8 24"X12" MO5-1R 21"X21"

DETOUR SOUTH 67 M4-8 24"X12" M3-3 24"X12" M1-6 24"X24" MO6-1 21"X21"

M4-8A 24"X18" END DETOUR NORTH SOUTH 67 67

EAST NORTH 28 67 M3-1 24"X12" MO6-1 21"X21"

R11-3 60" X 30" ROAD CLOSED 1/4 MILES AHEAD LOCAL TRAFFIC ONLY

TYPE III BARRICADE

DETOUR SOUTH 67 M4-8 24"X12" M3-3 24"X12" M1-6 24"X24" MO6-1 21"X21"

LEGEND

- WORK AREA
- TRAFFIC CONTROL DRUM WITH/WITHOUT LIGHT TYPE C
- NEW TRAFFIC CONTROL SIGN ON PERMANENT SUPPORT
- EXISTING TRAFFIC CONTROL SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE WITH ATTACHED TRAFFIC CONTROL SIGN
- TRAFFIC FLOW ARROW

PROJECT NO: 3030-02-72	HWY: STH 67	COUNTY: DODGE	DETOUR - STAGE 1 - DETAIL E	SHEET 43	E

FILE NAME : G:\00\00093\00093580\CADD\DESIGN\DESIGN\C3D\SHEETSPLAN\0270_DT - DETOUR\0270_DT - DETOUR.DWG
LAYOUT NAME - 027006_dt

PLOT DATE : 6/24/2021 2:28 PM PLOT BY : KEVIN KLOCKZIEM PLOT NAME : PLOT SCALE : Custom

Estimate Of Quantities

3030-02-72

Line	Item	Item Description	Unit	Total	Qty
0002	204.0110	Removing Asphaltic Surface	SY	8.000	8.000
0004	204.0115	Removing Asphaltic Surface Butt Joints	SY	456.000	456.000
0006	204.0120	Removing Asphaltic Surface Milling	SY	101,200.000	101,200.000
0008	205.0100	Excavation Common	CY	54.000	54.000
0010	211.0100	Prepare Foundation for Asphaltic Paving (project) 01. 3030-02-72	LS	1.000	1.000
0012	211.0400	Prepare Foundation for Asphaltic Shoulders	STA	12.000	12.000
0014	213.0100	Finishing Roadway (project) 01. 3030-02-72	EACH	1.000	1.000
0016	305.0110	Base Aggregate Dense 3/4-Inch	TON	4,125.000	4,125.000
0018	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	2.000	2.000
0020	305.0504.S	Hauling Excess Shoulder Material	CY	150.000	150.000
0022	455.0605	Tack Coat	GAL	8,949.000	8,949.000
0024	460.0105.S	HMA Percent Within Limits (PWL) Test Strip Volumetrics	EACH	1.000	1.000
0026	460.0110.S	HMA Percent Within Limits (PWL) Test Strip Density	EACH	2.000	2.000
0028	460.2005	Incentive Density PWL HMA Pavement	DOL	8,256.000	8,256.000
0030	460.2007	Incentive Density HMA Pavement Longitudinal Joints	DOL	24,160.000	24,160.000
0032	460.2010	Incentive Air Voids HMA Pavement	DOL	16,944.000	16,944.000
0034	460.6224	HMA Pavement 4 MT 58-28 S	TON	17,032.000	17,032.000
0036	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	126.000	126.000
0038	465.0450	Asphaltic Intersection Rumble Strips	SY	216.000	216.000
0040	465.0475	Asphalt Centerline Rumble Strips 2-Lane Rural	LF	22,108.000	22,108.000
0042	618.0100	Maintenance And Repair of Haul Roads (project) 01. 3030-02-72	EACH	1.000	1.000
0044	619.1000	Mobilization	EACH	1.000	1.000
0046	624.0100	Water	MGAL	70.000	70.000
0048	628.1905	Mobilizations Erosion Control	EACH	6.000	6.000
0050	628.1910	Mobilizations Emergency Erosion Control	EACH	3.000	3.000
0052	628.7020	Inlet Protection Type D	EACH	13.000	13.000
0054	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	4.000	4.000
0056	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	9.000	9.000
0058	634.0808	Posts Tubular Steel 2x2-Inch X 8-FT	EACH	4.000	4.000
0060	637.2210	Signs Type II Reflective H	SF	219.360	219.360
0062	637.2230	Signs Type II Reflective F	SF	16.000	16.000
0064	638.2102	Moving Signs Type II	EACH	1.000	1.000
0066	638.2602	Removing Signs Type II	EACH	16.000	16.000
0068	638.3000	Removing Small Sign Supports	EACH	18.000	18.000
0070	638.4000	Moving Small Sign Supports	EACH	2.000	2.000
0072	642.5401	Field Office Type D	EACH	1.000	1.000
0074	643.0300	Traffic Control Drums	DAY	268.000	268.000
0076	643.0420	Traffic Control Barricades Type III	DAY	852.000	852.000
0078	643.0705	Traffic Control Warning Lights Type A	DAY	1,304.000	1,304.000
0080	643.0715	Traffic Control Warning Lights Type C	DAY	6.000	6.000
0082	643.0900	Traffic Control Signs	DAY	7,568.000	7,568.000
0084	643.0920	Traffic Control Covering Signs Type II	EACH	5.000	5.000
0086	643.1050	Traffic Control Signs PCMS	DAY	28.000	28.000
0088	643.5000	Traffic Control	EACH	1.000	1.000
0090	646.1020	Marking Line Epoxy 4-Inch	LF	37,400.000	37,400.000
0092	646.1040	Marking Line Grooved Wet Ref Epoxy 4-Inch	LF	55,100.000	55,100.000
0094	646.3040	Marking Line Grooved Wet Ref Epoxy 8-Inch	LF	650.000	650.000
0096	646.5320	Marking Railroad Crossings Epoxy	EACH	2.000	2.000
0098	646.6120	Marking Stop Line Epoxy 18-Inch	LF	175.000	175.000

3030-02-72

Line	Item	Item Description	Unit	Total	Qty
0100	646.8120	Marking Curb Epoxy	LF	105.000	105.000
0102	648.0100	Locating No-Passing Zones	MI	5.260	5.260
0104	649.0105	Temporary Marking Line Paint 4-Inch	LF	37,960.000	37,960.000
0106	649.0120	Temporary Marking Line Epoxy 4-Inch	LF	37,400.000	37,400.000
0108	650.8000	Construction Staking Resurfacing Reference	LF	28,004.000	28,004.000
0110	650.9910	Construction Staking Supplemental Control (project) 01. 3030-02-72	LS	1.000	1.000
0112	654.0102	Concrete Bases Type 2	EACH	4.000	4.000
0114	657.0255	Transformer Bases Breakaway 11 1/2-Inch Bolt Circle	EACH	4.000	4.000
0116	657.0305	Poles Type 2	EACH	4.000	4.000
0118	657.0585	Trombone Arms 15-FT	EACH	4.000	4.000
0120	690.0150	Sawing Asphalt	LF	2,032.000	2,032.000
0122	740.0440	Incentive IRI Ride	DOL	21,040.000	21,040.000
0124	SPV.0060	Special 01. Structure Restoration	EACH	1.000	1.000
0126	SPV.0060	Special 02. Landmark Reference Monuments Special	EACH	3.000	3.000
0128	SPV.0060	Special 03. Verify Landmark Reference Monuments	EACH	6.000	6.000
0130	SPV.0180	Special 01. Removing Distressed Pavement Milling	SY	5,000.000	5,000.000

3

REMOVING ASPHALTIC SURFACE

CATEGORY	STATION	LOCATION	(204.0110) REMOVING ASPHALTIC SURFACE SY
0010	202+67	RT	1
	203+00	RT	1
	37+48	RT	1
	37+73	RT	1
	38+31	LT	1
	38+53	LT	1
	203+69	LT	1
	203+98	LT	1
	TOTAL		**8**

SHOULDER WIDENING

CATEGORY	STATION	TO STATION	LOCATION	(205.0100) EXCAVATION COMMON CY	(211.0400) PREPARE FOUNDATION FOR ASPHALTIC SHOULDERS STA	* (460.6224) HMA PAVEMENT 4 MT 58-28 S TON	* (455.0605) TACK COAT GAL	* (690.0150) SAWING ASPHALT LF
0010	202+17	39+30	RT	5	1	10	3	142
	202+28	36+91	LT & RT	6	2	11	2	156
	36+91	204+58	LT	5	1	10	2	133
	39+30	204+52	LT & RT	7	2	14	3	186
	336+81	342+60	LT	21	6	43	10	584
		UNDISTRIBUTED		10	-	-	-	-
		TOTALS		**54**	**12**	**88**	**20**	**1,201**

*ADDITIONAL QUANTITIES FOUND ELSEWHERE

NOTE: HMA PAVEMENT QUANTITY IN THIS TABLE IS JUST FOR THE LOWER LAYER. UPPER LAYER QUANTITY IS INCLUDED IN THE HMA PAVEMENT MISCELLANEOUS QUANTITY TABLE.

REMOVING ASPHALTIC SURFACE BUTT JOINTS

CATEGORY	STATION	TO STATION	LOCATION	* (204.0115) REMOVING ASPHALTIC SURFACE BUTT JOINTS SY	NOTES
0010	99+50	99+52	LT & RT	10	BEGIN PROJECT
	123+54	123+86	RT	7	NEDA ROAD
	123+66	123+95	LT	7	NEDA ROAD
	149+87	150+18	RT	7	GRAYLOG ROAD
	154+50	155+00	LT & RT	167	MILLING TRANSITION
	195+46	195+76	RT	7	MINE ROAD
	36+92	36+94	LT & RT	12	STH 33
	39+28	39+30	LT & RT	12	STH 33
	223+00	223+50	LT & RT	7	RAILROAD
	223+60	224+10	LT & RT	7	RAILROAD
	226+64	227+00	RT	6	LIMESTONE ROAD
	312+87	313+07	RT	4	ZION CHURCH ROAD
	339+15	339+47	RT	7	DUNN ROAD
	377+13	377+15	LT & RT	10	END PROJECT
		TOTAL		**271**	

*ADDITIONAL QUANTITIES FOUND ELSEWHERE

AGGREGATE SHOULDERS

CATEGORY	STATION	TO STATION	LOCATION	* (305.0110) BASE AGGREGATE DENSE 3/4-INCH TON	(305.0504.S) HAULING EXCESS SHOULDER MATERIAL CY	(624.0100) WATER MGAL
0010	99+50	377+15	LT & RT	3,613	-	51
		UNDISTRIBUTED		187	150	19
		TOTALS		**3,800**	**150**	**70**

*ADDITIONAL QUANTITIES FOUND ELSEWHERE

NOTE: BASE AGGREGATE DENSE 3/4-INCH BID ITEM INCLUDES MOVING THE EXISTING AGGREGATE TO ACCOMMODATE PAVING OPERATIONS.

NOTE: WATER IS FOR AGGREGATE SHOULDER DUST CONTROL AND COMPACTION.

REMOVING ASPHALTIC SURFACE MILLING

CATEGORY	STATION	TO STATION	LOCATION	(204.0120) REMOVING ASPHALTIC SURFACE MILLING SY	(SPV.0180.01) REMOVING DISTRESSED PAVEMENT MILLING SY
0010	99+50	155+00	LT & RT	20,680	-
	155+00	357+62	LT & RT	71,690	-
	357+62	377+15	LT & RT	8,830	-
		UNDISTRIBUTED		-	5,000
		TOTALS		**101,200**	**5,000**

3

ASPHALTIC DRIVEWAYS AND ISLANDS

CATEGORY	STATION	TO STATION	LOCATION	* (305.0110) BASE AGGREGATE DENSE 3/4-INCH TON	(305.0120) BASE AGGREGATE DENSE 1 1/4-INCH TON	(465.0120) ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES TON	* (690.0150) SAWING ASPHALT LF	* (204.0115) REMOVING ASPHALTIC SURFACE BUTT JOINTS SY
0010	102+34	102+65	LT	0.7	0	4	23	5
	107+08	107+44	LT	0.8	0	4	27	6
	110+62	110+82	LT	0.4	0	2	14	3
	117+98	118+25	RT	0.6	0	3	22	5
	118+53	118+87	RT	0.7	0	3	19	4
	119+25	119+67	LT	0.7	0	4	30	7
	125+45	125+73	RT	0.8	0	4	28	6
	134+92	135+19	LT	0.6	0	3	20	4
	148+37	148+71	RT	0.5	0	3	18	4
	172+97	173+48	RT	1.3	0	6	51	11
	180+74	181+17	RT	1.1	0	6	43	10
	203+00	-	RT	0.0	0.50	0.50	20	4
	37+73	-	RT	0.0	0.50	0.50	20	4
	38+31	-	LT	0.0	0.50	0.50	20	4
	203+69	-	LT	0.0	0.50	0.50	20	4
	249+27	249+80	LT	1.7	0	8	29	6
	302+90	303+55	LT	1.4	0	7	68	15
	305+67	306+32	RT	1.9	0	9	65	14
	323+88	324+43	LT	1.6	0	8	45	10
	333+43	334+15	RT	1.7	0	8	47	10
	335+38	336+87	LT	2.4	0	12	109	24
	342+60	343+19	LT	3.0	0	16	31	7
	355+90	356+43	LT	1.8	0	9	40	9
	356+45	356+81	RT	1.0	0	5	22	5
	BASE AGGREGATE DRIVEWAYS			300.0	-	-	-	-
	TOTALS			**325**	**2**	**126**	**831**	**185**

*ADDITIONAL QUANTITIES FOUND ELSEWHERE

ASPHALTIC INTERSECTION RUMBLE STRIPS

CATEGORY	STATION	TO STATION	LOCATION	(465.0450) ASPHALTIC INTERSECTION RUMBLE STRIPS SY
0010	193+19	193+44	RT	36
	196+69	196+94	RT	36
	198+94	199+19	RT	36
	207+49	207+74	LT	36
	209+74	209+99	LT	36
	213+24	213+49	LT	36
		TOTAL		**216**

CENTERLINE RUMBLE STRIPS

CATEGORY	STATION	TO STATION	LOCATION	(465.0475) ASPHALTIC CENTERLINE RUMBLE STRIPS 2-LANE RURAL LF
0010	99+58	121+69	LT & RT	2,220
	125+81	148+03	LT & RT	2,222
	152+03	193+87	LT & RT	4,184
	197+89	201+33	LT & RT	344
	205+34	209+17	LT & RT	383
	211+17	222+53	LT & RT	1,136
	228+81	269+93	LT & RT	4,112
	271+93	310+97	LT & RT	3,904
	314+97	337+30	LT & RT	2,233
	343+93	357+62	LT & RT	1,369
		TOTAL		**22,108**

HMA PAVEMENT

CATEGORY	STATION	TO STATION	LOCATION	UPPER LAYER HMA PAVEMENT 4 MT 58-28 S (460.6224) * TON	UPPER LAYER TRAVEL LANE # TON	UPPER LAYER TRAVEL LANE ## TON	UPPER LAYER SHOULDER ## TON	LOWER LAYER HMA PAVEMENT 4 MT 58-28 S (460.6224) * TON	LOWER LAYER TRAVEL LANE # TON	LOWER LAYER TRAVEL LANE ## TON	LOWER LAYER SHOULDER ## TON	* (455.0605) TACK COAT GAL	NOTES
0010	99 + 50	155 + 00	LT & RT	1,330	-	1,330	-	1,709	-	1,709	-	1,628	
	99 + 50	155 + 00	LT & RT	710	-	-	710	913	-	-	913	870	
	155 + 00	357 + 62	LT & RT	6,934	6,934	-	-	-	-	-	-	3,467	
	155 + 00	357 + 62	LT & RT	3,144	-	-	3,144	-	-	-	-	1,572	
	357 + 62	377 + 15	LT & RT	668	668	-	-	-	-	-	-	334	
	357 + 62	377 + 15	LT & RT	568	-	-	568	-	-	-	-	284	
	362 + 95	375 + 85	LT & RT	-	-	-	-	212	212	-	-	221	WEDGE LAYER
	362 + 95	375 + 85	LT & RT	-	-	-	-	196	-	-	196	204	WEDGE LAYER
		**UNDISTRIBUTED		-	-	-	-	560	442	-	118	350	
			TOTALS	**13,354**	**7,602**	**1,330**	**4,422**	**3,590**	**654**	**1,709**	**1,226**	**8,929**	

*ADDITIONAL QUANTITIES FOUND ELSEWHERE
**ASSOCIATED WITH SPV.0180.02 - REMOVING DISTRESSED PAVEMENT MILLING
Tonnage is eligible for Incentive Density PWL HMA Pavement (460.2005) and Incentive Air Voids HMA Pavement (460.2010).
Tonnage is eligible for Incentive Air Voids HMA Pavement (460.2010) and density is tested for acceptance in those areas.

3

MAINTENANCE AND REPAIR OF HAUL ROADS

CATEGORY	DESCRIPTION	(618.0100) EACH
0010	PROJECT 3030-02-72	1
	TOTAL	**1**

MOBILIZATIONS EROSION CONTROL

CATEGORY	PROJECT	(628.1905) MOBILIZATIONS EROSION CONTROL EACH	(628.1910) MOBILIZATIONS EMERGENCEY EROSION CONTROL EACH
0010	3030-02-72	6	3
	TOTALS	**6**	**3**

INLET PROTECTION

CATEGORY	STATION	LOCATION	(628.7020) TYPE D EACH
0010	202+82	RT	1
	203+25	LT	1
	203+36	RT	1
	203+75	LT	1
	360+05	LT	1
	362+85	LT	1
	364+45	LT	1
	367+70	LT	1
	370+95	LT	1
	372+45	LT	1
	373+75	LT	1
	374+60	LT	1
	375+80	LT	1
	TOTAL		**13**

MOVING SIGNS

CATEGORY	STATION	LOCATION	(638.2102) MOVING SIGNS TYPE II EACH	(638.4000) MOVING SMALL SIGN SUPPORTS EACH
0010	202+71	RT	1	2
	TOTALS		**1**	**2**

REMOVING SIGNS

CATEGORY	SIGN NO.	STATION	LOCATION	(638.2602) REMOVING SIGNS TYPE II EACH	(638.3000) REMOVING SMALL SIGN SUPPORTS EACH
0010	1-01	203+87	LT	1	1
	1-02	203+91	LT	1	1
	1-03	205+70	LT	1	2
	1-04	203+67	LT	1	1
	1-05	38+25	LT	1	1
	1-06	38+29	LT	1	1
	1-07	38+22	LT	1	1
	1-08	38+24	LT	1	1
	1-09	37+66	RT	1	1
	1-10	37+68	RT	1	1
	1-11	37+76	RT	1	1
	1-12	37+77	RT	1	1
	1-13	203+00	RT	1	1
	1-14	201+91	RT	1	2
	1-15	202+77	RT	1	1
	1-16	202+78	RT	1	1
		TOTALS		**16**	**18**

NOTE: SIGNS AT THE SAME LOCATION ARE COUNTED AS ONE SIGN REMOVAL

TRAFFIC CONTROL SIGNS PCMS

CATEGORY	LOCATION	DESCRIPTION	(643.1050) TRAFFIC CONTROL SIGNS PCMS DAY
0010	STH 67 SOUTHBOUND, RUEDEBUSCH	ADVANCE PROJECT NOTIFICATION	7
	STH 67 NORTHBOUND, STH 33	ADVANCE PROJECT NOTIFICATION	7
	STH 67 SOUTHBOUND, STH 33	ADVANCE PROJECT NOTIFICATION	7
	STH 67 NORTHBOUND, CTH S	ADVANCE PROJECT NOTIFICATION	7
		TOTAL	**28**

DETOUR

CATEGORY	LOCATION	SHEET SHOWN IN PLAN SET	DETOUR DURATION DAYS	SIGNS NO. DEVICES	INTERIM DURATION DAYS	DRUMS NO. DEVICES	* (643.0300) TRAFFIC CONTROL DRUMS DAYS	BARRICADES TYPE III NO. DEVICES	* (643.0420) TRAFFIC CONTROL BARRICADES TYPE III DAYS	WARNING LIGHTS TYPE A NO. DEVICES	* (643.0705) TRAFFIC CONTROL WARNING LIGHTS TYPE A DAYS	WARNING LIGHTS TYPE C NO. DEVICES	(643.0715) TRAFFIC CONTROL WARNING LIGHTS TYPE C DAYS	* (643.0900) TRAFFIC CONTROL SIGNS DAYS	(643.0920) TRAFFIC CONTROL COVERING SIGNS TYPE II EACH
0010	NORTHBOUND STH 67 AT STH 33	DETOUR - STAGE 1 - DETAIL A	50	10	-	-	-	1	50	2	100	-	-	500	-
	WESTBOUND STH 33 AT STH 67	DETOUR - STAGE 1 - DETAIL A	50	12	1	9	9	1	1	2	2	3	3	600	1
	EASTBOUND STH 33 AT STH 67	DETOUR - STAGE 1 - DETAIL A	50	6	1	9	9	1	1	2	2	3	3	300	3
	WESTBOUND STH 33 AT CTH TW & V	DETOUR - STAGE 1 - DETAIL B	50	6	-	-	-	-	-	-	-	-	-	300	-
	EASTBOUND STH 33 AT CTH TW & V	DETOUR - STAGE 1 - DETAIL B	50	6	-	-	-	-	-	-	-	-	-	300	-
	WESTBOUND STH 33 (E LAKE ST) AT STH 28 (CLASON ST)	DETOUR - STAGE 1 - DETAIL C	50	8	-	-	-	-	-	-	-	-	-	400	-
	EASTBOUND STH 33 (E LAKE ST) AT STH 28 (CLASON ST)	DETOUR - STAGE 1 - DETAIL C	50	4	-	-	-	-	-	-	-	-	-	200	-
	EASTBOUND STH 33 (W LAKE ST) AT STH 28 (CLASON ST)	DETOUR - STAGE 1 - DETAIL C	50	11	-	-	-	-	-	-	-	-	-	550	-
	WESTBOUND STH 28 (CLASON ST) AT STH 33 (LAKE ST)	DETOUR - STAGE 1 - DETAIL C	50	8	-	-	-	-	-	-	-	-	-	400	-
	EASTBOUND STH 28 (CLASON ST) AT STH 33 (LAKE ST)	DETOUR - STAGE 1 - DETAIL C	50	4	-	-	-	-	-	-	-	-	-	200	-
	WESTBOUND STH 28 (HORICON ST)	DETOUR - STAGE 1 - DETAIL D	50	7	-	-	-	-	-	-	-	-	-	350	-
	EASTBOUND STH 28 (HORICON ST)	DETOUR - STAGE 1 - DETAIL D	50	6	-	-	-	-	-	-	-	-	-	300	-
	WESTBOUND STH 28 (HORICON ST)	DETOUR - STAGE 1 - DETAIL E	50	3	-	-	-	-	-	-	-	-	-	150	1
	EASTBOUND STH 28 (HORICON ST)	DETOUR - STAGE 1 - DETAIL E	50	3	-	-	-	-	-	-	-	-	-	150	-
	NORTHBOUND STH 67 (S MAIN ST) AT STH 28 (HORICON ST)	DETOUR - STAGE 1 - DETAIL E	50	4	-	-	-	-	-	-	-	-	-	200	-
	SOUTHBOUND STH 67 (N MAIN ST) AT STH 28 (HORICON ST)	DETOUR - STAGE 1 - DETAIL E	50	9	-	-	-	1	50	2	100	-	-	450	-
	TOTALS						**18**		**102**		**204**		**6**	**5,350**	**5**

*ADDITIONAL QUANTITIES FOUND ELSEWHERE

NOTE: ROAD CLOSURE TRAFFIC CONTROL DEVICES SHOWN ON DETOUR PLAN ARE INCLUDED IN THE TRAFFIC CONTROL MISCELLANEOUS QUANTITY TABLE.
NOTE: THERE IS ONLY ONE CYCLE FOR TRAFFIC CONTROL COVERING SIGNS TYPE II.

TRAFFIC CONTROL

CATEGORY	DESCRIPTION	(643.5000) TRAFFIC CONTROL EACH	DRUMS NO. DEVICES	* (643.0300) TRAFFIC CONTROL DRUMS DAY	BARRICADES TYPE III NO. DEVICES	* (643.0420) TRAFFIC CONTROL BARRICADES TYPE III DAY	WARNING LIGHTS TYPE A NO. DEVICES	* (643.0705) TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	SIGNS NO. DEVICES	* (643.0900) TRAFFIC CONTROL SIGNS DAY	DURATION	
0010	ADVANCE WARNING SIGNS (MAINLINE & SIDEROAD)	-	-	-	-	-	-	-	23	1,150	50	DAYS
	LANE CLOSURE WITH FLAGGING OPERATION	-	-	-	-	-	-	-	8	400	50	DAYS
	MOVING PAVEMENT MARKING OPERATION	-	-	-	-	-	-	-	8	88	11	DAYS
	SHOULDER CLOSURE	-	25	250	-	-	-	-	2	20	10	DAYS
	UNEVEN LANES	-	-	-	-	-	-	-	10	110	11	DAYS
	ROAD CLOSURE	-	-	-	15	750	22	1,100	9	450	50	DAYS
	PROJECT 3030-02-72	1	-	-	-	-	-	-	-	-		
	TOTALS	**1**	**-**	**250**		**750**		**1,100**	**-**	**2,218**		

* ADDITIONAL QUANTITIES FOUND ELSEWHERE

NOTES:
ADVANCE WARNING SIGNS (MAINLINE & SIDEROAD) - SEE S.D.D. "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC"
LANE CLOSURE WITH FLAGGING OPERATION - SEE S.D.D. "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION"
MOVING PAVEMENT MARKING OPERATION - SEE S.D.D. "MOVING PAVEMENT MARKING OPERATION - TWO-LANE TWO-WAY ROADWAY"
SHOULDER CLOSURE - SEE S.D.D. "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY"
ROAD CLOSURES - SEE S.D.D. "BARRICADES AND SIGNS FOR MAINLINE CLOSURES"

PERMANENT SIGNING

CATEGORY	SIGN NO.	STATION	LOCATION	SIGN CODE	SIZE IN X IN	(634.0808) POSTS TUBULAR STEEL 2x2-INCH x 8-FT EACH	(634.0614) POSTS WOOD 4x6-INCH x 14-FT EACH	(634.0616) POSTS WOOD 4x6-INCH x 16-FT EACH	(637.2210) SIGNS TYPE II REFLECTIVE H SF	(637.2230) SIGNS TYPE II REFLECTIVE F SF	(654.0102) CONCRETE BASES TYPE 2 EACH	(657.0255) TRANSFORMER BASES BREAKAWAY 11 1/2-INCH BOLT CIRCLE EACH	(657.0305) POLES TYPE 2 EACH	(657.0585) TROMBONE ARMS 15-FT EACH	NOTES
0010	1-17	203+92	LT	R5-1C	36" X 36"	-	-	-	7.46	-	-	-	-	-	ON POST WITH SIGN R1-1
	1-18	203+95	LT	R1-1	36" X 36"	-	1	-	7.46	-	-	-	-	-	
	1-19	203+95	LT	R1-3P	24" X 9"	-	-	-	1.50	-	-	-	-	-	ON POST WITH SIGN 1-18
	1-20	205+70	LT	J3-2	48" X 57"	-	-	2	19.00	-	-	-	-	-	
	1-21	203+69	LT	R1-1	36" X 36"	-	-	-	7.46	-	1	1	1	1	ON SIGN SUPPORT POLE
	1-22	203+69	LT	R1-3P	24" X 9"	-	-	-	1.50	-	-	-	-	-	ON SIGN SUPPORT POLE
	1-23	203+69	LT	R1-1	36" X 36"	-	-	-	7.46	-	-	-	-	-	OVERHEAD ON TROMBONE ARM
	1-24	203+69	LT	R1-3P	24" X 9"	-	-	-	1.50	-	-	-	-	-	OVERHEAD ON TROMBONE ARM
	1-25	38+31	LT	R5-1C	36" X 36"	-	-	-	7.46	-	-	-	-	-	ON POST WITH SIGN R1-1
	1-26	38+34	LT	R1-1	36" X 36"	-	1	-	7.46	-	-	-	-	-	
	1-27	38+34	LT	R1-3P	24" X 9"	-	-	-	1.50	-	-	-	-	-	ON POST WITH SIGN 1-26
	1-28	40+41	LT	J3-2	48" X 57"	-	-	2	19.00	-	-	-	-	-	
	1-29	38+31	LT	R1-1	36" X 36"	-	-	-	7.46	-	1	1	1	1	ON SIGN SUPPORT POLE
	1-30	38+31	LT	R1-3P	24" X 9"	-	-	-	1.50	-	-	-	-	-	ON SIGN SUPPORT POLE
	1-31	38+31	LT	R1-1	36" X 36"	-	-	-	7.46	-	-	-	-	-	OVERHEAD ON TROMBONE ARM
	1-32	38+31	LT	R1-3P	24" X 9"	-	-	-	1.50	-	-	-	-	-	OVERHEAD ON TROMBONE ARM
	1-33	35+73	RT	J3-2	48" X 57"	-	-	2	19.00	-	-	-	-	-	
	1-34	37+63	RT	R1-1	36" X36"	-	1	-	7.46	-	-	-	-	-	
	1-35	37+63	RT	R1-3P	24" X 9"	-	-	-	1.50	-	-	-	-	-	ON POST WITH SIGN 1-34
	1-36	37+67	RT	R5-1C	36" X 36"	-	-	-	7.46	-	-	-	-	-	ON POST WITH SIGN R1-1
	1-37	37+73	RT	R1-1	36" X 36"	-	-	-	7.46	-	1	1	1	1	ON SIGN SUPPORT POLE
	1-38	37+73	RT	R1-3P	24" X 9"	-	-	-	1.50	-	-	-	-	-	ON SIGN SUPPORT POLE
	1-39	37+73	RT	R1-1	36" X 36"	-	-	-	7.46	-	-	-	-	-	OVERHEAD ON TROMBONE ARM
	1-40	37+73	RT	R1-3P	24" X 9"	-	-	-	1.50	-	-	-	-	-	OVERHEAD ON TROMBONE ARM
	1-41	203+00	RT	R1-1	36" X 36"	-	-	-	7.46	-	1	1	1	1	ON SIGN SUPPORT POLE
	1-42	203+00	RT	R1-3P	24" X 9"	-	-	-	1.50	-	-	-	-	-	ON SIGN SUPPORT POLE
	1-43	203+00	RT	R1-1	36" X 36"	-	-	-	7.46	-	-	-	-	-	OVERHEAD ON TROMBONE ARM
	1-44	203+00	RT	R1-3P	24" X 9"	-	-	-	1.50	-	-	-	-	-	OVERHEAD ON TROMBONE ARM
	1-45	201+91	RT	J3-2	48" X 57"	-	-	2	19.00	-	-	-	-	-	
	1-46	202+73	RT	R1-1	36" X 36"	-	1	-	7.46	-	-	-	-	-	
	1-47	202+73	RT	R1-3P	24" X 9"	-	-	-	1.50	-	-	-	-	-	ON POST WITH SIGN 1-46
	1-48	202+73	RT	R5-1C	36" X 36"	-	-	-	7.46	-	-	-	-	-	ON POST WITH SIGN R1-1
	1-49	39+80	RT	J4-1	24" X 36"	-	-	1	6.00	-	-	-	-	-	
	1-50R	37+48	RT	W12-1D	24" X 24"	1	-	-	-	4.00	-	-	-	-	MEDIAN ISLAND
	1-51R	203+98	LT	W12-1D	24" X 24"	1	-	-	-	4.00	-	-	-	-	MEDIAN ISLAND
	1-52R	38+53	LT	W12-1D	24" X 24"	1	-	-	-	4.00	-	-	-	-	MEDIAN ISLAND
	1-53R	202+67	RT	W12-1D	24" X 24"	1	-	-	-	4.00	-	-	-	-	MEDIAN ISLAND
		TOTALS				**4**	**4**	**9**	**219.36**	**16.00**	**4**	**4**	**4**	**4**	

PAVEMENT MARKINGS

CATEGORY	STATION	TO STATION	(646.1020) MARKING LINE EPOXY 4-INCH (YELLOW) LF	(649.0120) TEMPORARY MARKING LINE EPOXY 4-INCH (YELLOW) LF	(646.1040) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE) LF	(646.3040) MARKING LINE GROOVED WET REF EPOXY 8-INCH (WHITE) LF	(646.5320) MARKING RAILROAD CROSSING EPOXY EACH	(646.6120) MARKING STOP LINE EPOXY 18-INCH LF	(646.8120) MARKING CURB EPOXY LF	(648.0100) LOCATING NO-PASSING ZONES MI	(649.0105) TEMPORARY MARKING LINE PAINT 4-INCH LF
0010	99+50	155+00	4,135	4,135	10,675	-	-	-	-	1.05	6,620
	155+00	377+15	33,265	33,265	44,425	650	2	175	105	4.21	31,340
	TOTALS		**37,400**	**37,400**	**55,100**	**650**	**2**	**175**	**105**	**5.26**	**37,960**

NOTE: TEMPORARY MARKING LINE EPOXY 4-INCH TO BE APPLIED BEFORE CENTERLINE RUMBLE STRIPS ARE INSTALLED

NOTE: MARKING LINE EPOXY 4-INCH TO BE APPLIED AFTER CENTERLINE RUMBLE STRIPS ARE INSTALLED.

PWL MIXTURE USE TABLE

LOCATION	STATION	TO STATION	LOCATION	MIXTURE USE	UNDERLDAYING SURFACE	BID ITEM	TONS	THICKNESS (IN)	QUALITY MANAGEMENT PROGRAM TO BE USED FOR	
									MIXTURE ACCEPTANCES	DENSITY ACCEPTANCES
DRIVING LANES, 2 - 12 FOOT LANES	99+50	155+00	LT & RT	LOWER LAYER	MILLED SURFACE	HMA PAVEMENT 4 LT 58-28 S	1,709	2.25	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
DRIVING LANES, 2 - 12 FOOT LANES	99+50	155+00	LT & RT	UPPER LAYER	HMA PAVEMENT 4 LT 58-28 S	HMA PAVEMENT 4 LT 58-28 S	1,330	1.75	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
MAINLINE 3-FOOT SHOULDER	99+50	155+00	LT & RT	LOWER LAYER	MILLED SURFACE	HMA PAVEMENT 4 LT 58-28 S	913	2.25	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
MAINLINE 3-FOOT SHOULDER	99+50	155+00	LT & RT	UPPER LAYER	HMA PAVEMENT 4 LT 58-28 S	HMA PAVEMENT 4 LT 58-28 S	710	1.75	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
DRIVING LANES, 2 - 12 FOOT LANES	155 + 00	357 + 62	LT & RT	UPPER LAYER	MILLED SURFACE	HMA PAVEMENT 4 LT 58-28 S	6,934	2.50	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
MAINLINE 3-FOOT SHOULDER	155 + 00	357 + 62	LT & RT	UPPER LAYER	MILLED SURFACE	HMA PAVEMENT 4 LT 58-28 S	3,144	2.50	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
'DRIVING LANES, 2 - 12 FOOT LANES	357 + 62	377 + 15	LT & RT	UPPER LAYER	MILLED SURFACE	HMA PAVEMENT 4 LT 58-28 S	668	2.50	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
MAINLINE 3-FOOT SHOULDER	357 + 62	377 + 15	LT & RT	UPPER LAYER	MILLED SURFACE	HMA PAVEMENT 4 LT 58-28 S	568	2.50	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
'DRIVING LANES, 2 - 12 FOOT LANES	362 + 95	375 + 85	LT & RT	WEDGE LAYER	MILLED SURFACE	HMA PAVEMENT 4 LT 58-28 S	212	1.2 (AVG)	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
MAINLINE, 8-FOOT LT SHOULDER, 10-FOOT RT SHOULDER	362 + 95	375 + 85	LT & RT	WEDGE LAYER	MILLED SURFACE	HMA PAVEMENT 4 LT 58-28 S	196	1.2 (AVG)	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
'DRIVING LANES, 2 - 12 FOOT LANES	UNDISTRIBUTED (DISTRESSED PAVEMENT AREAS)		LT & RT	LOWER LAYER	MILLED SURFACE	HMA PAVEMENT 4 LT 58-28 S	442	1.00	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
MAINLINE 3-FOOT SHOULDER	UNDISTRIBUTED (DISTRESSED PAVEMENT AREAS)		LT & RT	LOWER LAYER	MILLED SURFACE	HMA PAVEMENT 4 LT 58-28 S	118	1.00	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE

LANDMARK REFERENCE MONUMENTS

CATEGORY	STATION	LOCATION	NUMBER	(SPV.0060.02) LANDMARK REFERENCE MONUMENTS SPECIAL EACH	(SPV.0060.03) VERIFY LANDMARK REFERENCE MONUMENTS EACH
0010	123+76.9	0.5 LT	36143	1	1
	150+09.3	5.1 LT	36112	1	1
	176+41.4	77.5 LT	36113	-	1
	230+56.3	1.1 RT	36023	1	1
	256+96.6	363.0 RT	46352	-	1
	257+47.6	216.9 RT	36024	-	1
	TOTALS			**3**	**6**

CONSTRUCTION STAKING

CATEGORY	STATION	TO STATION	(650.8000) RESURFACING REFERENCE LF	(650.9910.01) SUPPLEMENTAL CONTROL (PROJECT) LS
0010	99+50	377+15	27,765	-
	36+91	39+30	239	-
	PROJECT 3030-02-72		-	1
	TOTALS		**28,004**	**1**

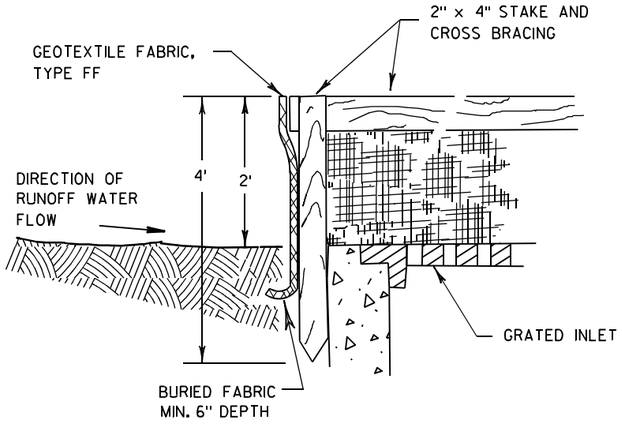
STRUCTURE RESTORATION

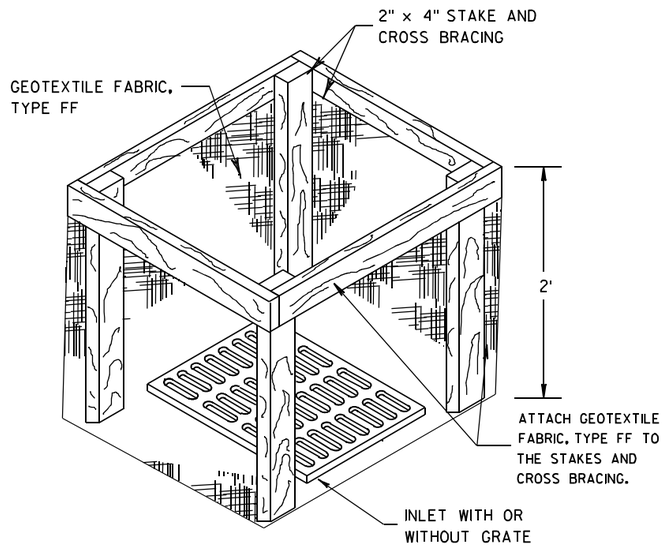
CATEGORY	STATION	LOCATION	(SPV.0060.01) EACH
0010	203+01	24' RT	1
	TOTAL		**1**

Standard Detail Drawing List

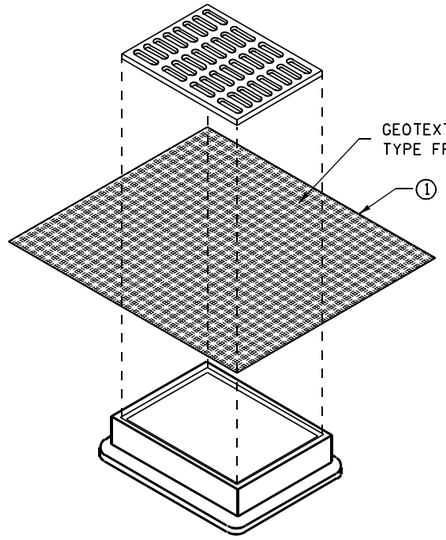
08E10-02	INLET PROTECTION TYPE A, B, C AND D
09C02-09	CONCRETE BASES, TYPES 1, 2, 5, & 6
13A08-01	ASPHALTIC RUMBLE STRIPS AT INTERSECTION
13A11-03A	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13A11-03B	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13C19-03	HMA LONGITUDINAL JOINTS
14B29-01	SAFETY EDGE
15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-08C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C08-20A	LONGITUDINAL MARKING (MAINLINE)
15C09-12A	SIGNING AND PAVEMENT MARKING DETAILS FOR RAILROAD-HIGHWAY GRADE CROSSINGS
15C12-07	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C18-05C	MEDIAN ISLAND MARKING
15C19-06A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-04A	PAVEMENT MARKING (INTERSECTIONS)
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D44-02	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES

6



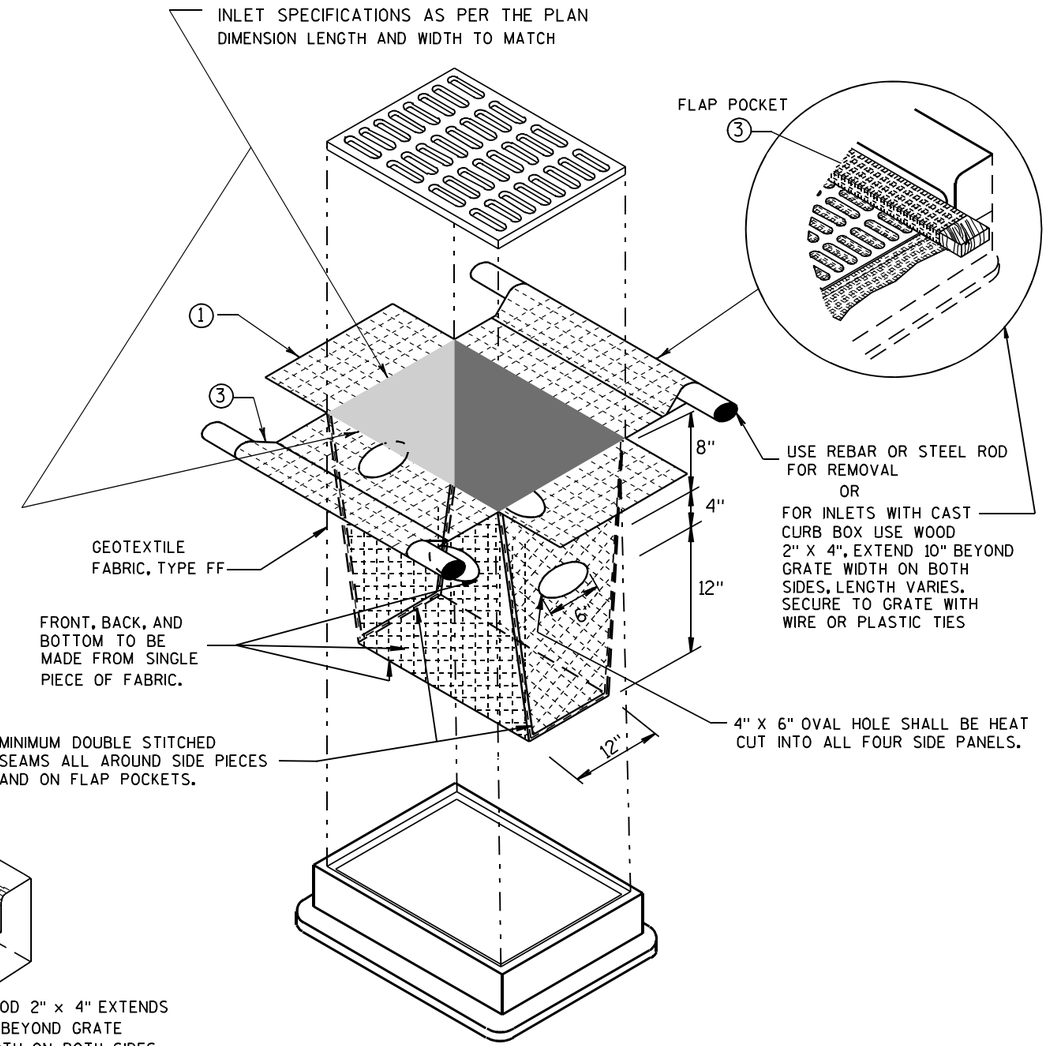


INLET PROTECTION, TYPE A



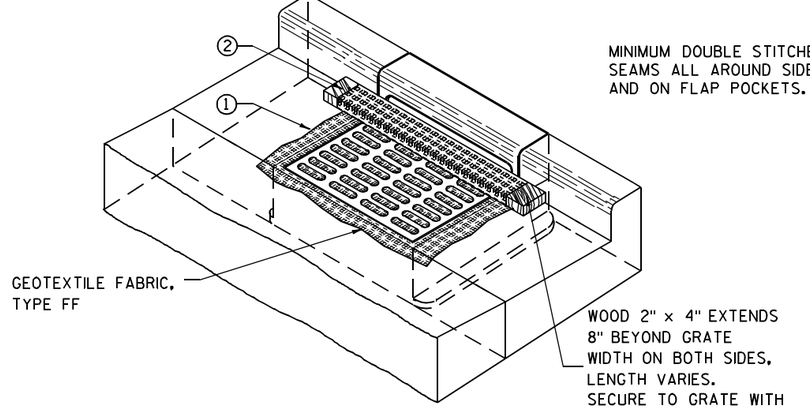
INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH
OR WITHOUT A CURB BOX AS PER NOTE ②)



INLET PROTECTION, TYPE C (WITH CURB BOX)

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.

③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION
TYPE A, B, C, AND D

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

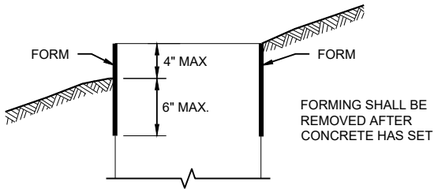
10/16/02
DATE

/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA

6
S.D.D. 8 E 10-2

FORM DEPTH SHALL BE NO MORE THAN 6" BELOW GRADE ON THE LOWER SIDE OF BASE



FORMING DETAIL

QUANTITY REQUIREMENTS	CONCRETE BASE TYPE		
	1	2	5 & 6
APPROX. CUBIC YARDS OF CONCRETE	0.40	0.57	0.40
LBS. OF HOOP BAR STEEL	NONE	23	16
LBS. OF VERTICAL BAR STEEL	NONE	60	18

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWINGSHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

BASES SHALL BE EXCAVATED BY USE OF A CIRCULAR AUGER.

TOP SURFACES OF CONCRETE BASES SHALL BE TROWEL FINISHED SMOOTH AND LEVEL.

CONDUIT SIZES AND LOCATIONS SHALL BE SHOWN ON THE PLANS

THE FINAL OR TERMINATING CONCRETE BASE IN A CONDUIT RUN SHALL HAVE A 6" EXIT STUB INSTALLED FOR FUTURE CABLING USE. THE EXIT STUB SHALL BE SIZED AS USED THROUGHOUT THE CONDUIT RUN AS SHOWN A THE ENTRANCE OF THE BASE.

ENDS OF CONDUIT INSTALLED BELOW GRADE FRO FUTURE USE SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON-METALLIC.

MINIMUM BENDING RADIUS OF CONDUIT IS EQUAL TO 6X THE DIAMETER.

CONDUIT HEIGHT ABOVE CONCRETE BASES SHALL BE 1 INCH. ALL METALLIC CONDUIT ENDS SHALL BE REAMED AND THREADED.

ALL CONDUIT ENDS AT THE TOP OF CONCRETE BASES SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON-METALLIC IMMEDIATELY AFTER PLACEMENT AND BEFORE CONCRETE IS POURED. CONDUITS IN WHICH WIRE OR CABLE IS NOT INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BELL ENDS SHALL BE INSTALLED ON ALL PVC CONDUIT EXPOSED AT THE TOP OF CONCRETE BASES BEFORE INSTALLATION.

WHEN REQUIRED TO CONNECT NON-METALLIC CONDUIT TO METALLIC CONDUIT, ONLY ADAPTER FITTINGS, U.L. LISTED FOR ELECTRICAL USE, SHALL BE USED.

IF A BASE REQUIRES A DEEP FORM BECAUSE OF LOOSE DIRT OR FILL, THE FORM SHALL BE REMOVED BEFORE BACKFILLING AROUND THE BASE. BACKFILL SHALL BE TAMPED TIGHT AGAINST THE BARE CONCRETE BASE IN LAYERS OF 1 FOOT OR LESS.

A NO. 4 AWG STRANDED COPPER EQUIPMENT GROUNDING CONDUCTOR SHALL BE EXOTHERMICALLY WELDED TO THE EQUIPMENT GROUNDING ELECTRODE (GROUND ROD) FOR TYPE 2, TYPE 5 AND TYPE 6 BASES.

THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE FURNISHED AND INSTALLED TO ENTER ALL BASE TYPES THROUGH A 1 INCH CONDUIT INSTALLED FOR GROUNDING PURPOSES, LEAVING A 4 FOOT COIL OF WIRE ABOVE THE CONCRETE BASE. THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE NEATLY COILED AND THE COILS TIED TOGETHER.

ANCHOR RODS SHALL BE THREADED 12" IN LENGTH ON EACH END OF THE ROD. ANCHOR RODS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 654.2.1 OF THE STANDARD SPECIFICATIONS.

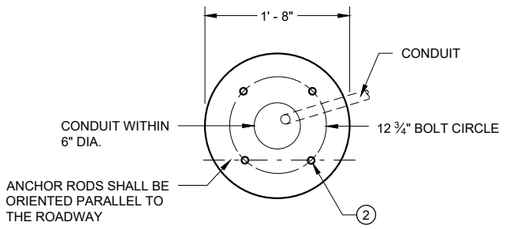
WASHERS AND LOCK WASHERS ARE REQUIRED ON ALL ANCHOR RODS.

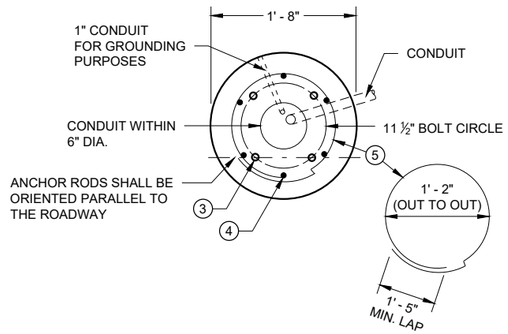
WHEN ANCHOR RODS USING THE ALTERNATE "L" BEND ARE FURNISHED, THE 4 INCH"L" BEND SHALL BE IN ADDITION TO THE SPECIFIED ANCHOR ROD BAR LENGTH. THE "L" BEND SHALL NOT BE THREADED.

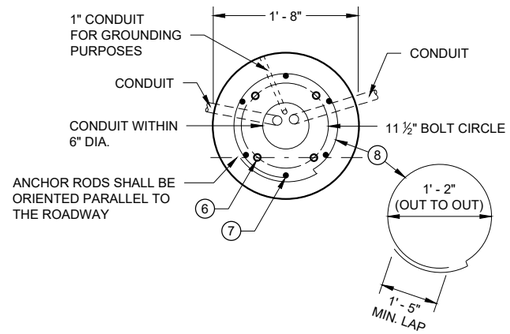
ANCHOR RODS SHALL BE INSTALLED WITH MISALIGNMENTS OF LESS THAN 1:40 FROM VERTICAL.

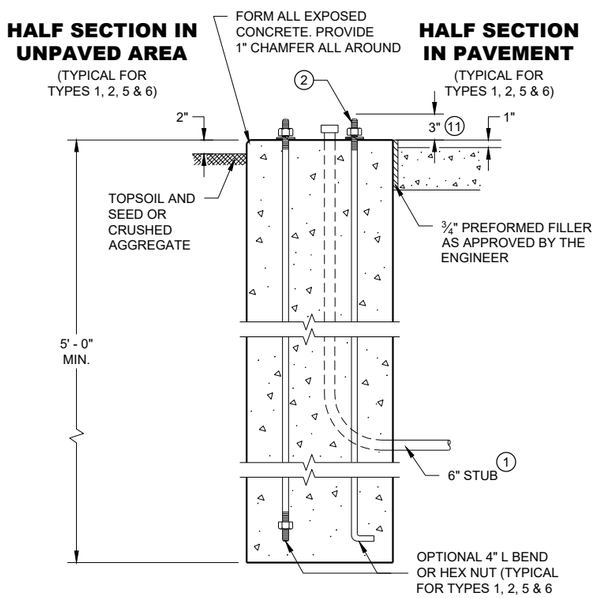
WELDING OF THE ANCHOR RODS TO THE CAGE IS UNACCEPTABLE. TIE WIRES SHALL BE USED.

BAR STEEL REINFORCEMENT SHALL BE COATED WITH POWDERED EPOXY RESIN IN ACCORDANCE WITH SECTION 505 OF THE STANDARD SPECIFICATIONS (LATEST EDITION).

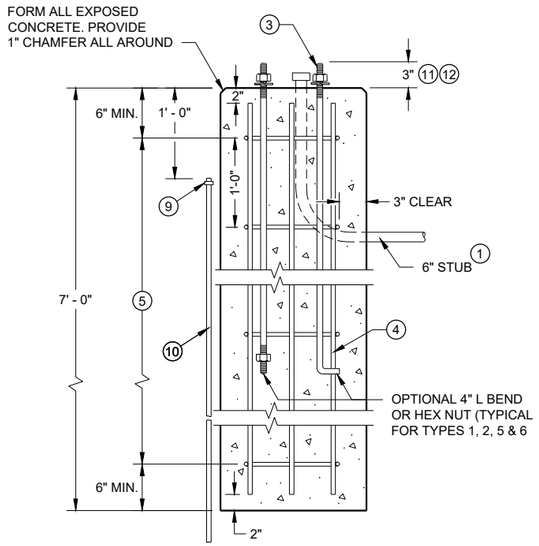




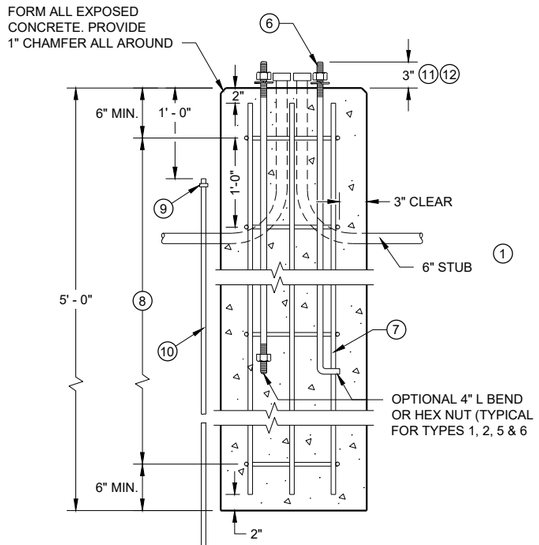




TYPE 1



TYPE 2



TYPE 5 & 6

1 THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE AND INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES. THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE THAT IS NOT INSTALLED BELOW THE TRAVELED WAY SHALL BE 18 INCHES. THE MAXIMUM DEPTH OF ALL CONDUIT SHALL BE 36 INCHES EXCEPT WITH WRITTEN APPROVAL OF THE ENGINEER.

2 (4) 1" DIA. X 3' - 6" ANCHOR RODS.

3 (4) 1" DIA. X 5' - 0" ANCHOR RODS.

4 (6) NO. 6 X 6' - 8" BAR STEEL REINFORCEMENT.

5 (7) NO. 4 X 5' - 1" BAR STEEL REINFORCEMENT @ 1' - 0" C - C.

6 (4) 1" DIA. X 3' - 6" ANCHOR RODS.

7 (6) NO. 4 X 4' - 8" BAR STEEL REINFORCEMENT.

8 (5) NO. 4 X 5' - 1" BAR STELL REINFORCEMENT @ 1' - 0" C -C.

9 EXOTHERMIC CONNECTION TO EUIPMENT GROUNDING CONDUCTOR

10 ⅝" DIA. X 8' -0" COPPERCLAD EQUIPMENT GROUNDING ELECTRODE REQUIRED

11 ANY ANCHOR ROD PROJECTION SHORTER THAN 2 ¾" OR LONGER THAN 3 ¼" SHALL REQUIRE THE BASE TO BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.

12 FOR NON - BREAKAWAY INSTALLATIONS, 4 ½" ± ANCHOR ROD PROJECTION WITH THE USE OF LEVELING NUTS. RODENT SCREEN REQUIRED.

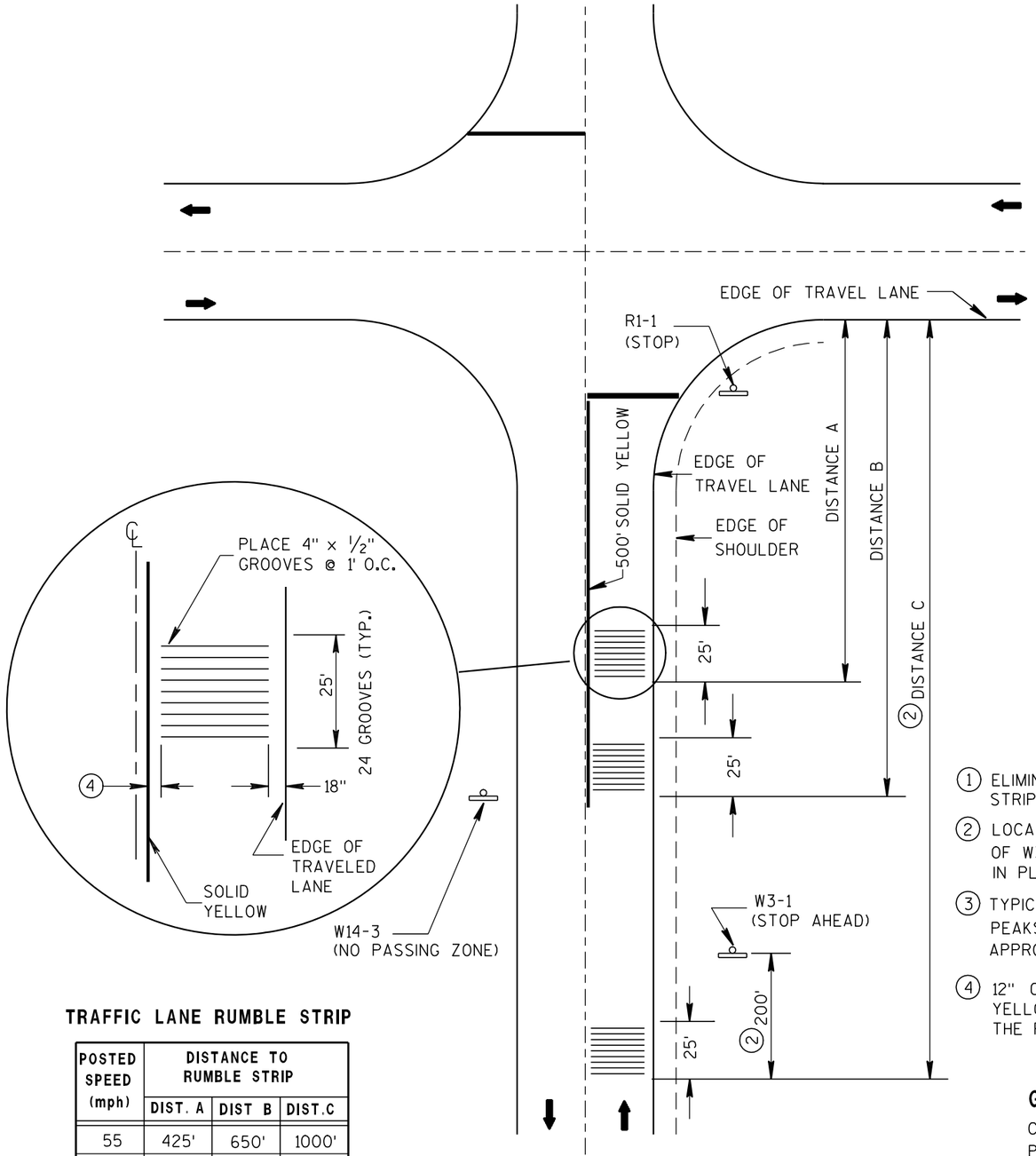
CONCRETE BASES

CONCRETE BASES TYPES 1, 2, 5, & 6

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019
DATE
/S/ Ahmet Demirbilek
STATE ELECTRICAL ENGI

FHWA



ARROW SYMBOL (➡) SHOWS DIRECTION OF TRAVEL

PLAN VIEW

RUMBLE STRIP LOCATION

TRAFFIC LANE RUMBLE STRIP

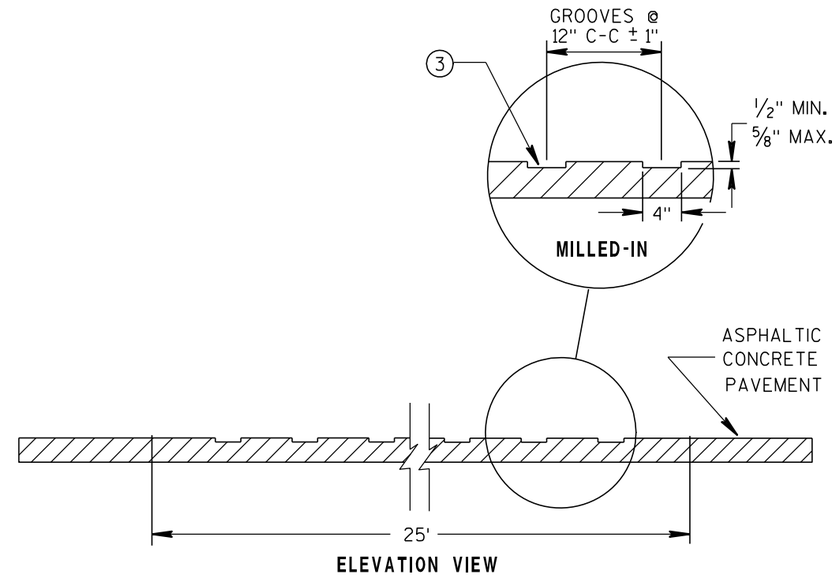
POSTED SPEED (mph)	DISTANCE TO RUMBLE STRIP		
	DIST. A	DIST B	DIST.C
55	425'	650'	1000'
50	325'	450'	800'
45	275'	400'	650'
40	225'	(1)	550'
35	175'	(1)	475'
≤ 30	125'	(1)	425'

(1) ELIMINATE THE MIDDLE SET OF RUMBLE STRIPS.

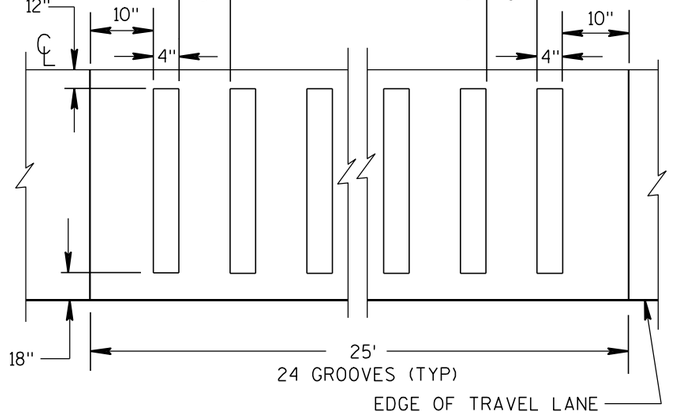
(2) LOCATE RUMBLE STRIP 200' IN ADVANCE OF W3-1 SIGN AS SHOWN. IF W3-1 IS NOT IN PLACE, USE DISTANCE C.

(3) TYPICAL VERTICAL VARIATION BETWEEN PEAKS AND VALLEYS WITHIN THE CUT APPROXIMATELY 1/16"

(4) 12" CLEAR BETWEEN THE SOLID YELLOW LINE AND THE EDGE OF THE RUMBLE.



ELEVATION VIEW



PLAN VIEW

ASPHALTIC PAVEMENT MILLED-IN

GENERAL NOTES

CONTRACTOR SHALL CONFIRM RUMBLE STRIP LOCATION WITH THE ENGINEER PRIOR TO INSTALLATION. THE ENGINEER MAY MODIFY THE RUMBLE STRIP LOCATION AS FIELD CONDITIONS DICTATE.

WHEN ASPHALTIC PAVEMENT IS NEW IN THE RUMBLE AREA THE CONTRACTOR SHALL ALLOW THE PAVEMENT TO CURE A MINIMUM OF 7 DAYS PRIOR TO RUMBLE INSTALLATION.

PAVEMENT MARKING AND SIGNING DETAILS AND SPECIFICATIONS ARE PROVIDED ELSEWHERE IN THE CONTRACT.

ASPHALTIC RUMBLE STRIPS AT INTERSECTION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

8/17/2011
DATE

/S/ Jerry H. Zogg
ROADWAY STANDARDS I 55
ENGINEER

FHWA

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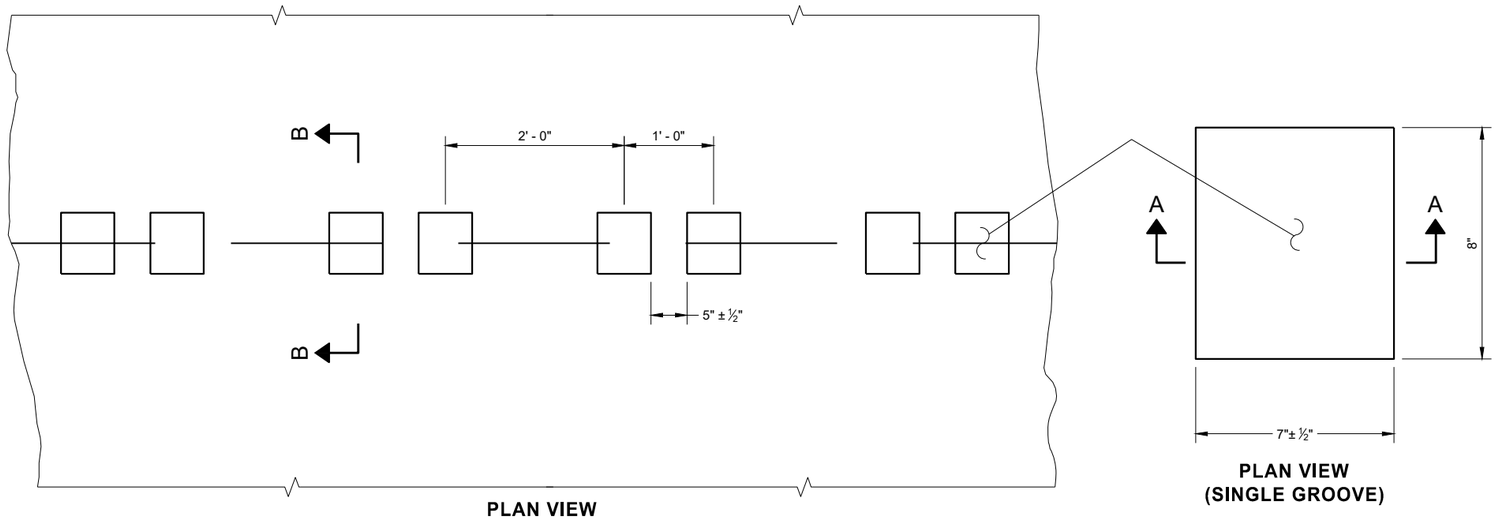
S.D.D. 13 A 8-1

6

S.D.D. 13 A 8-1



PLAN VIEW
SHOULDER WITH GROOVES

PLAN VIEW
(SINGLE GROOVE)

PLACEMENT DETAIL FOR TYPE 1 MILLED RUMBLE STRIP

GENERAL NOTES

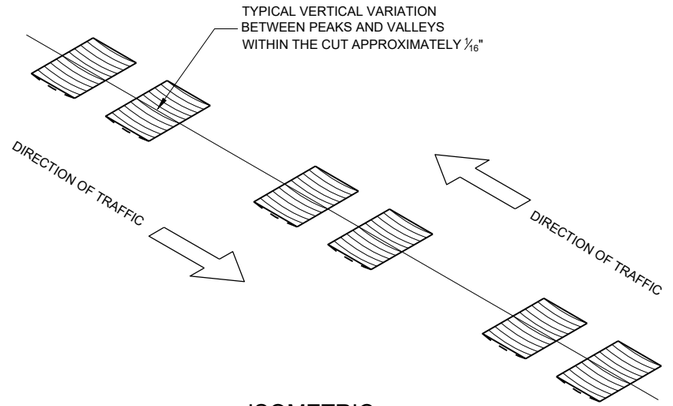
DETAILS OF CONSTRUCTION NO SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL CENTERLINE GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

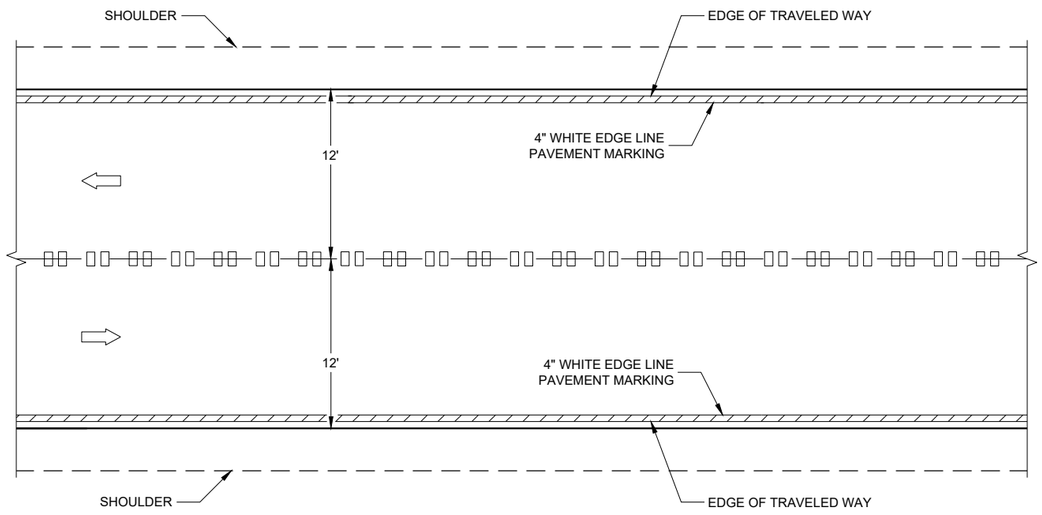
INSTALL PAVEMENT MARKING AFTER THE GROOVES ARE INSTALLED.

SEE SIGNING PLAN FOR SIGN REQUIREMENTS THAT MAY BE NEEDED.

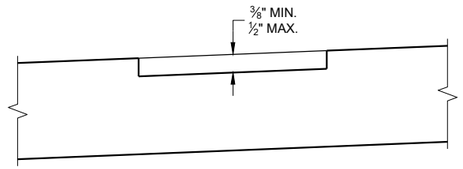
(1) CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



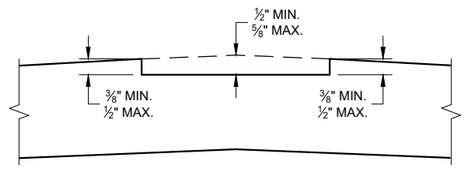
ISOMETRIC



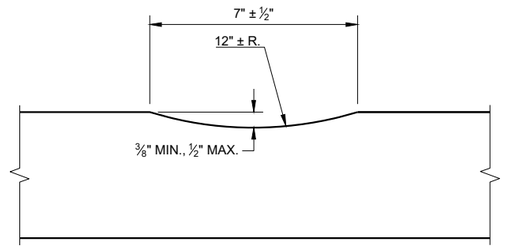
CENTERLINE GROOVES ON TWO-WAY ROADWAYS



SECTION B - B
SUPERELEVATED ROADWAY



SECTION B - B
CROWNED ROADWAY



SECTION A - A

2-LANE RURAL
CENTER LINE RUMBLE STRIP, MILLING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 13A11 - 03a

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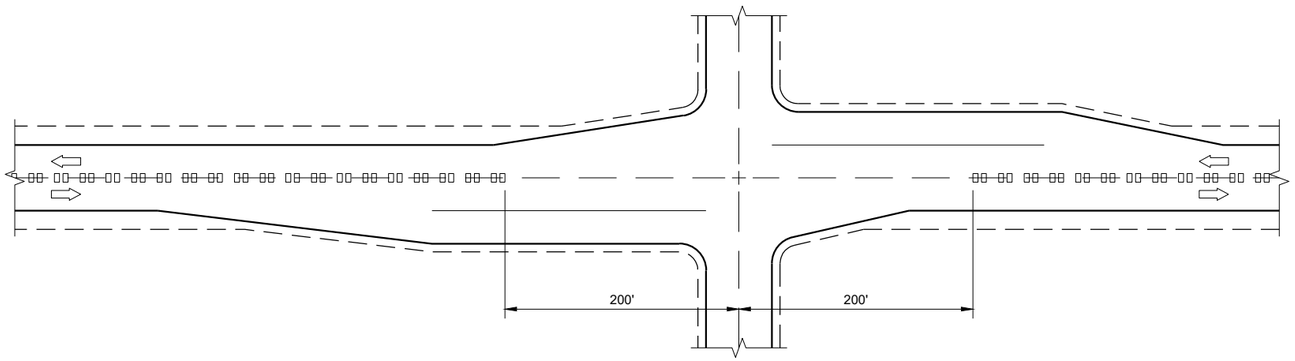
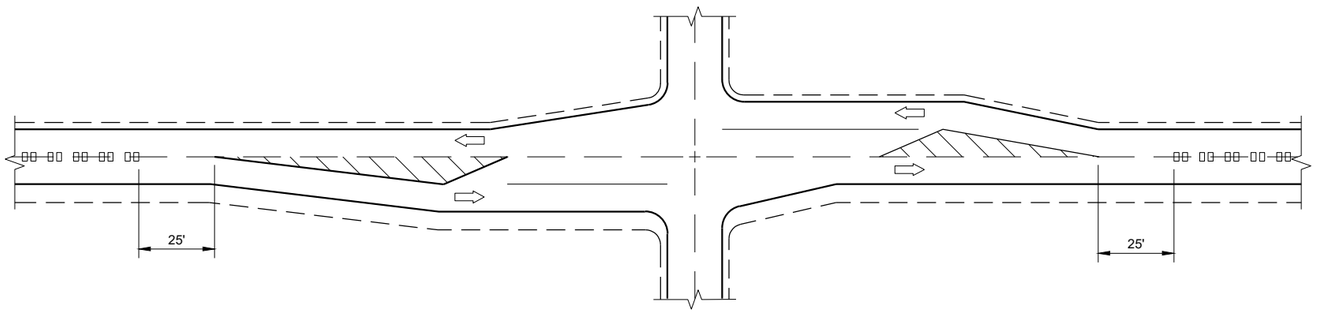
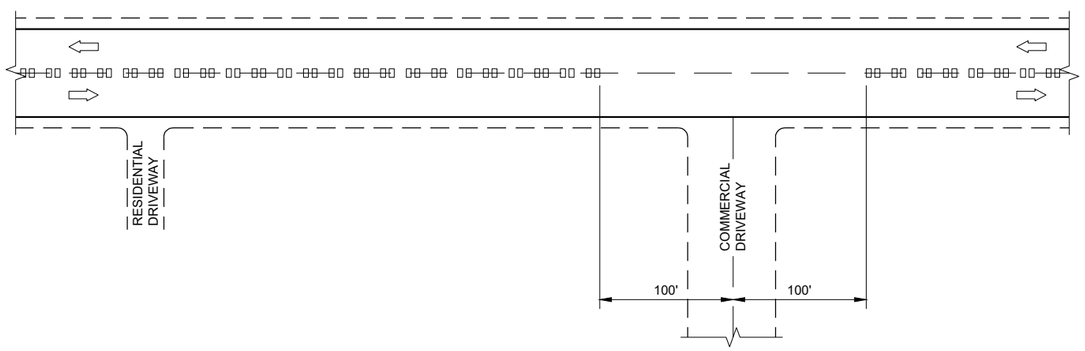
SDD 13A11 - 03a



CENTERLINE GROOVES AT INTERSECTIONS



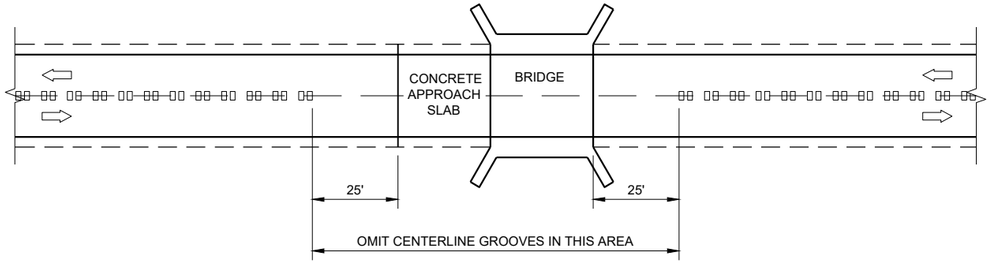
CENTERLINE GROOVES AT INTERSECTIONS (WITH LEFT TURN LANES)



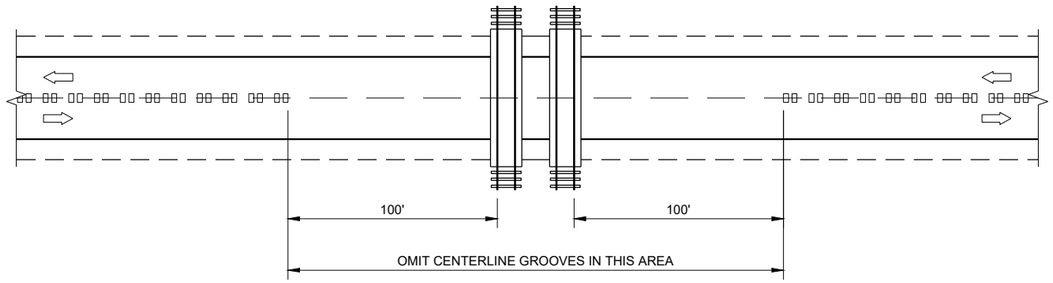
CENTERLINE GROOVES AT DRIVEWAYS (1)

GENERAL NOTES

(1) CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



CENTERLINE GROOVES AT BRIDGES



CENTERLINE GROOVES AT RAILROADS

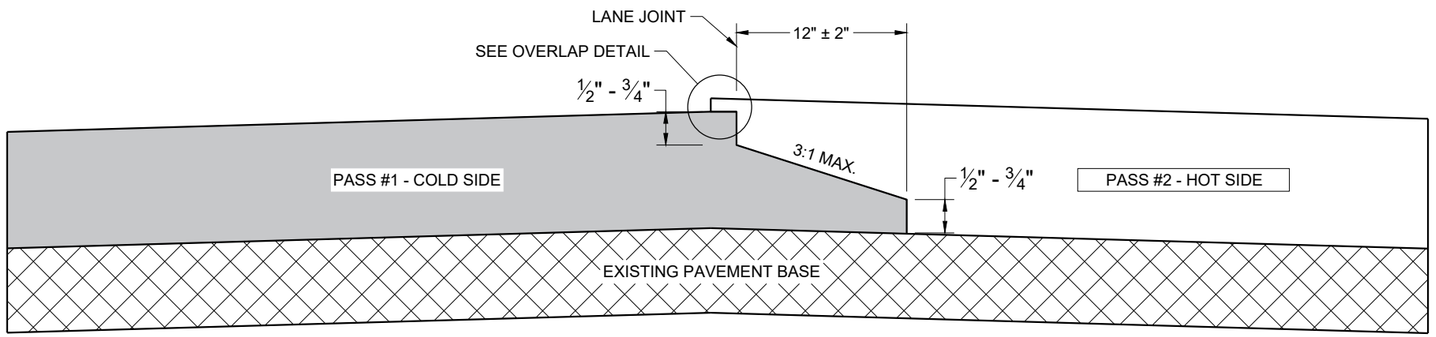
2-LANE RURAL CENTERLINE RUMBLE STRIP, MILLING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

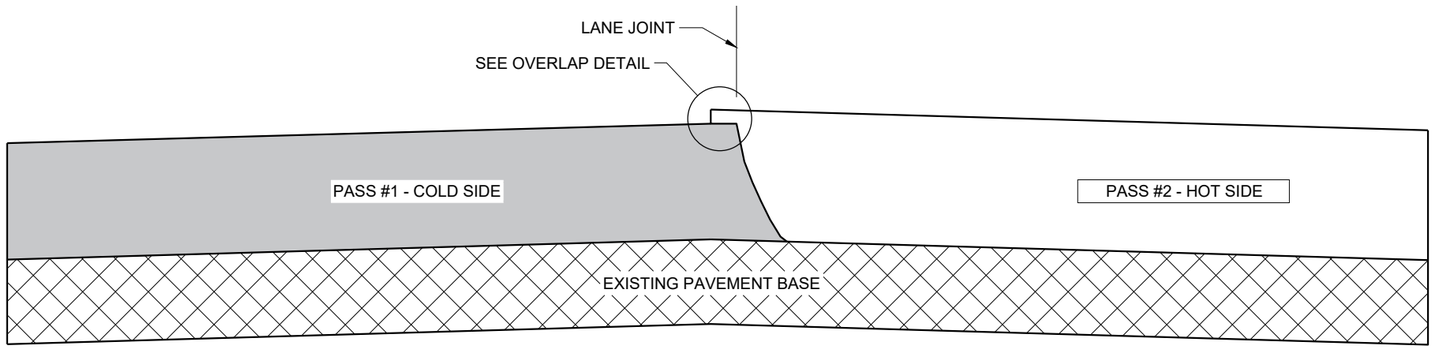
APPROVED
7/2018
DATE
/S/ Rodney Taylor
ROADWAY STANDARDS DEVE ENGINEER

FHWA

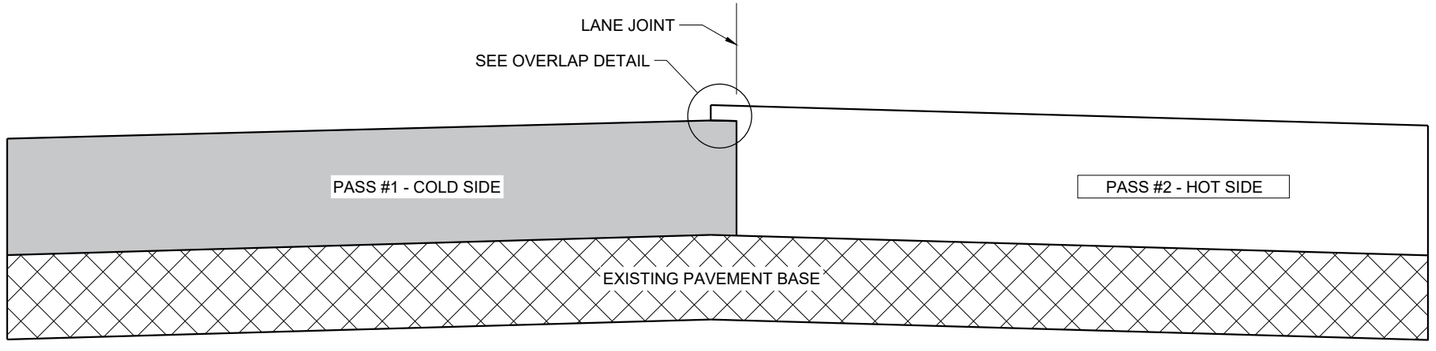
6
SDD 13A11 - 03b



TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)

GENERAL NOTES

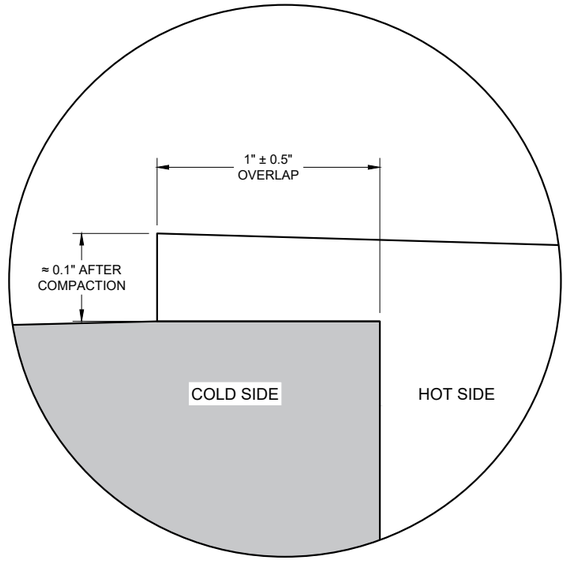
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY 1" ± 0.5" AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

6

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SDD 13C19 - 03

SDD 13C19 - 03

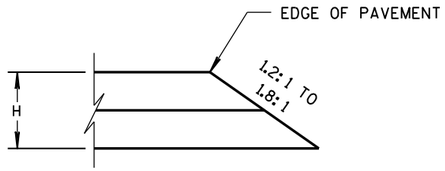
HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2020
DATE

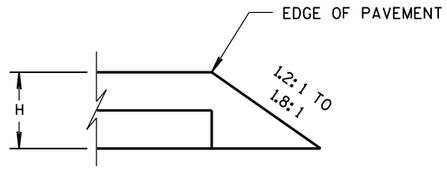
/S/ Steven Hefel
HMA PAVEMENT ENGIN

FHWA



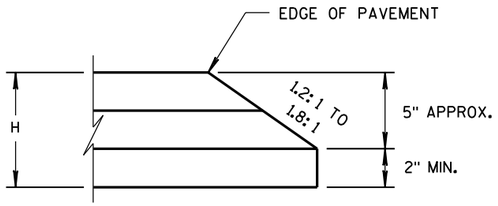
CONSTRUCTED WITH FINAL TWO LAYERS

FOR H 5" OR LESS



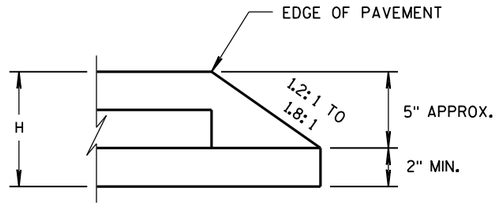
CONSTRUCTED WITH FINAL LAYER

FOR H 5" OR LESS



CONSTRUCTED WITH FINAL TWO LAYERS

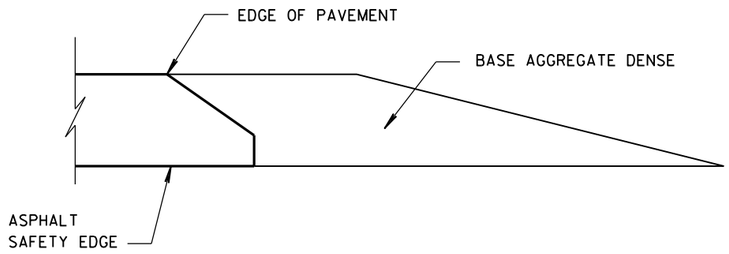
FOR H GREATER THAN 5"



CONSTRUCTED WITH FINAL LAYER

FOR H GREATER THAN 5"

HMA PAVEMENT AND HMA OVERLAYS



FINISHED SHOULDER AGGREGATE PLACEMENT

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S.D.D. 14 B 29-1

S.D.D. 14 B 29-1

SAFETY EDGE SM

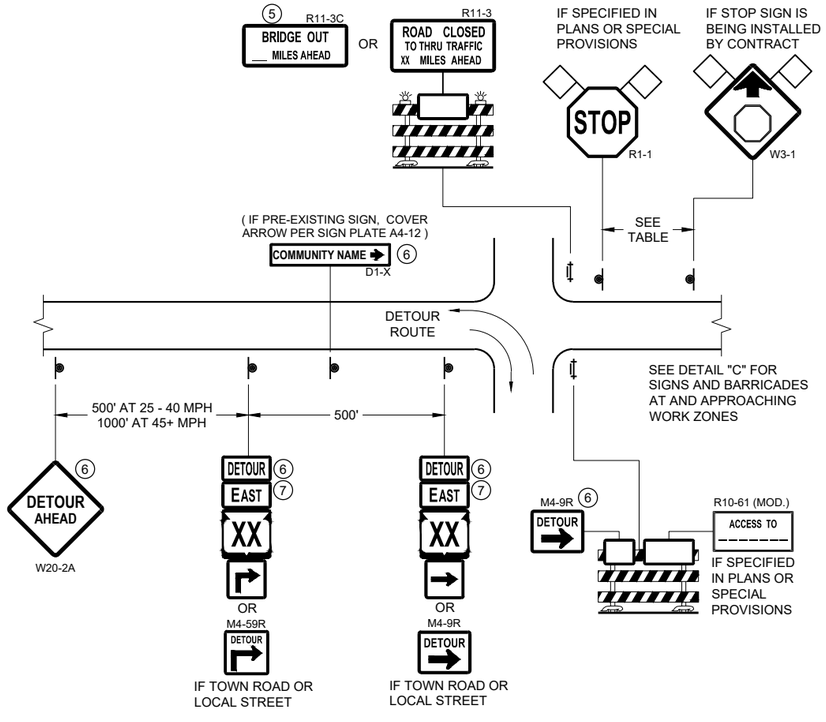
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

11/30/2012
DATE

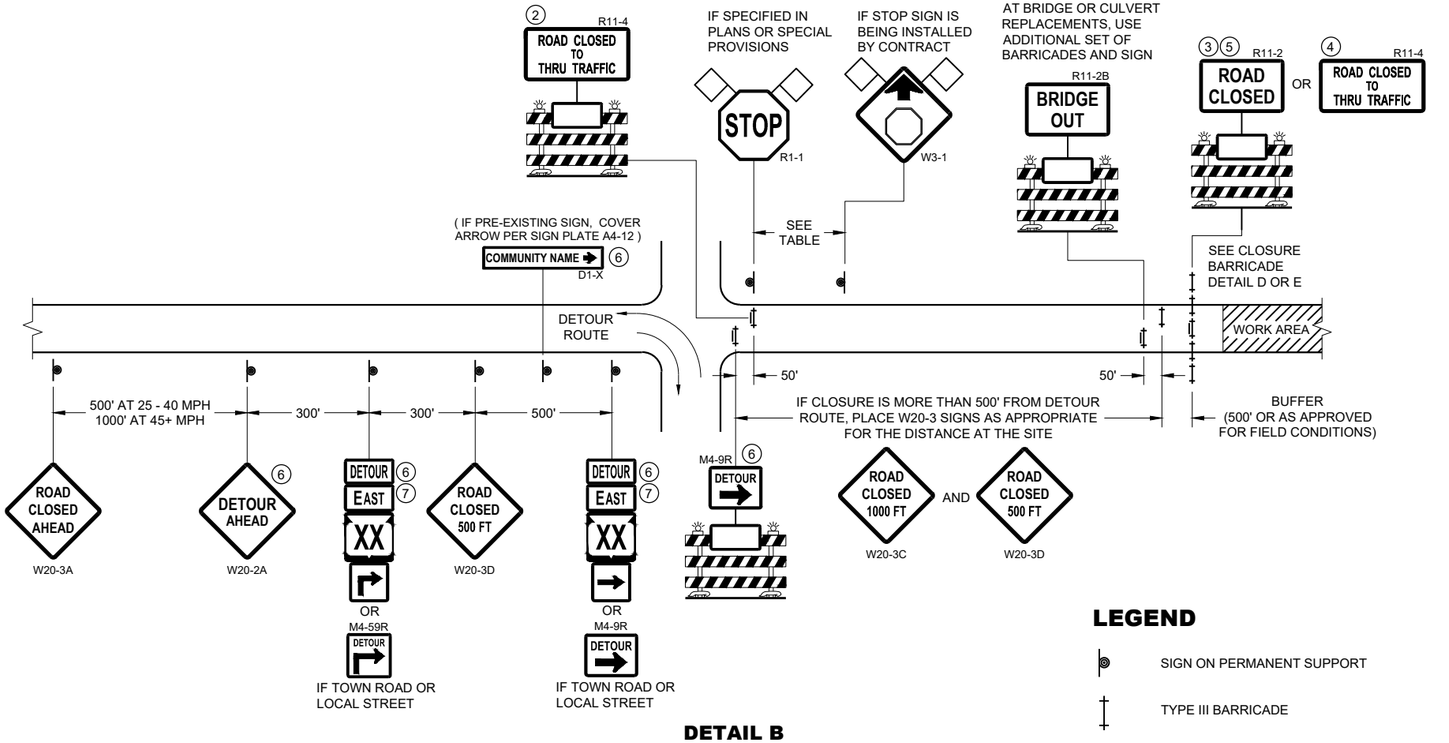
/S/ Jerry H. Zoda
ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA



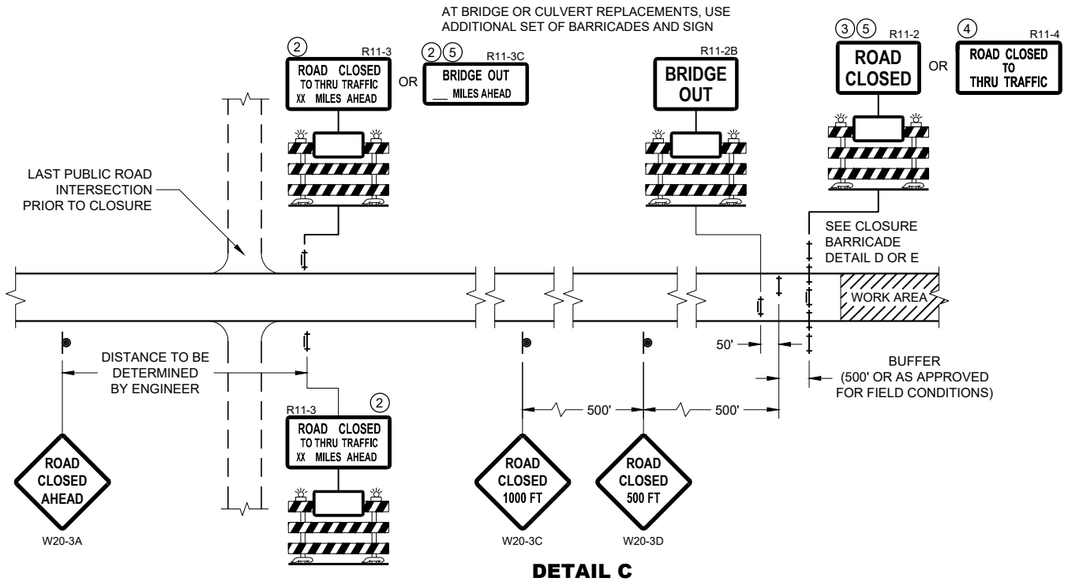
DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN OR EQUAL TO ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)
- DETOUR — M4 - 8
- EAST — M3 - X
- XX — M1 - 4 OR XX — M1 - 6 OR COUNTY X — M1 - 5A
- M05 - 1 OR M06 - 1

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

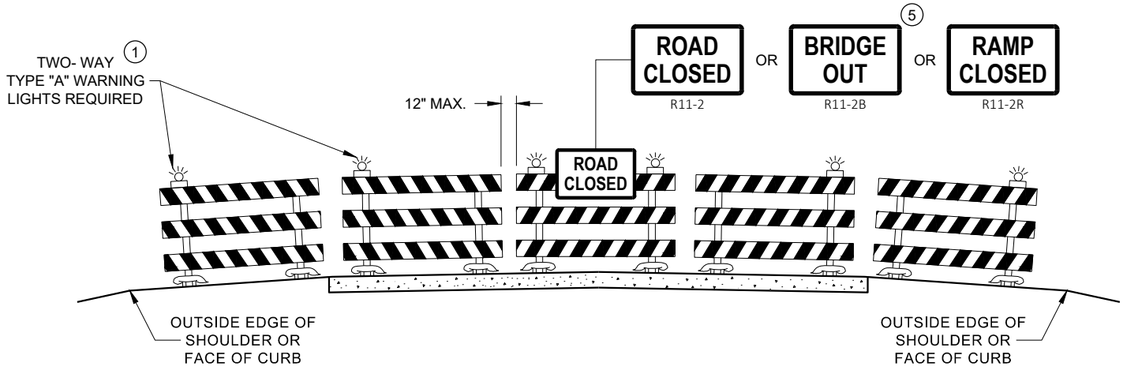
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE

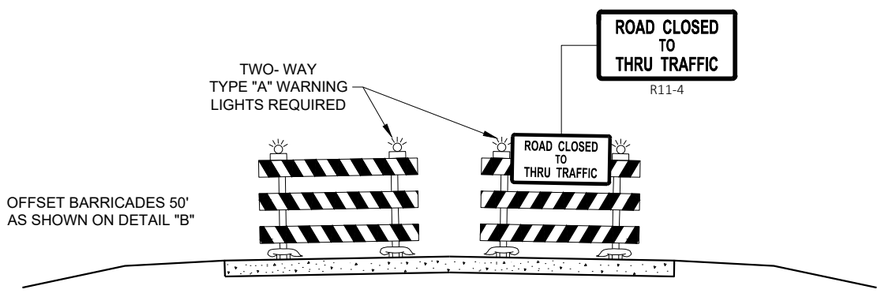
/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

SDD 15C02 - 08a



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60 " X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING.

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR VARIOUS CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEE
FHWA

6

SDD 15C02 - 08b

6

SDD 15C02 - 08b

THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- DETOUR — M4 - 8
- EAST — M3 - X
- XX — M1 - 4 OR XX — M1 - 6 OR COUNTY X — M1 - 5A
- M05 - 1 OR M06 - 1 OR M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

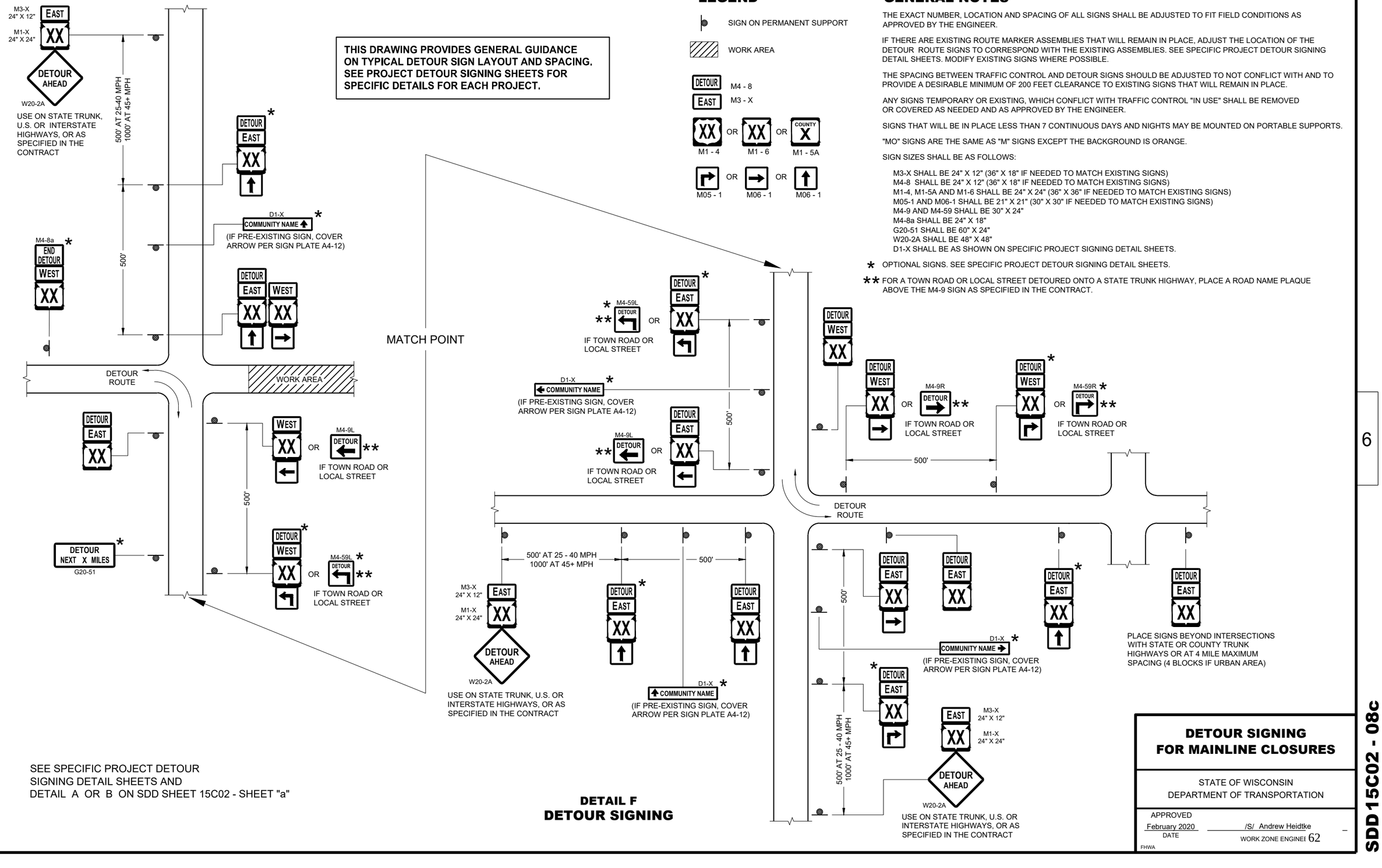
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-59 SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

* OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.

** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.



SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

DETAIL F
DETOUR SIGNING

DETOUR SIGNING FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

SDD 15C02 - 08c

SDD 15C02 - 08c

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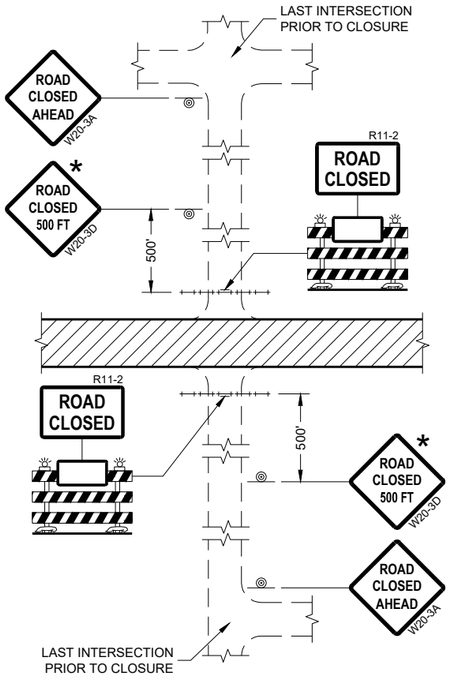
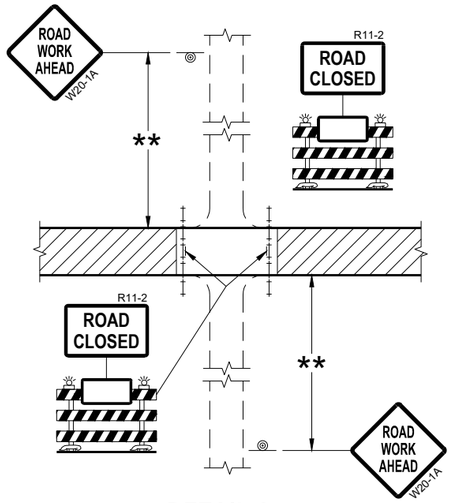


DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

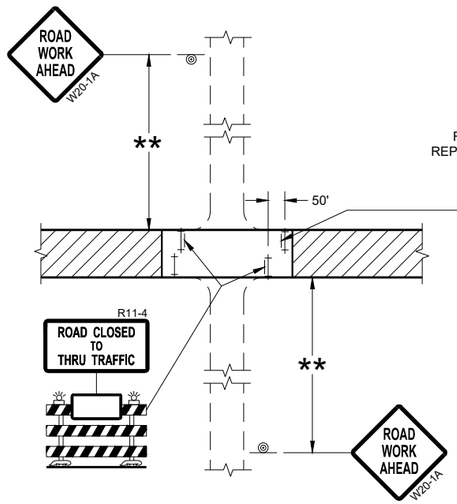
TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

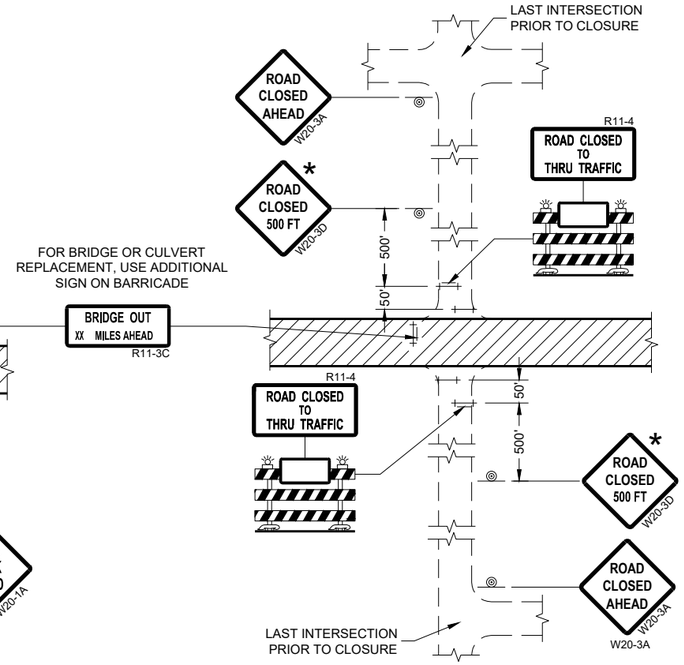
ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

* OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.

** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

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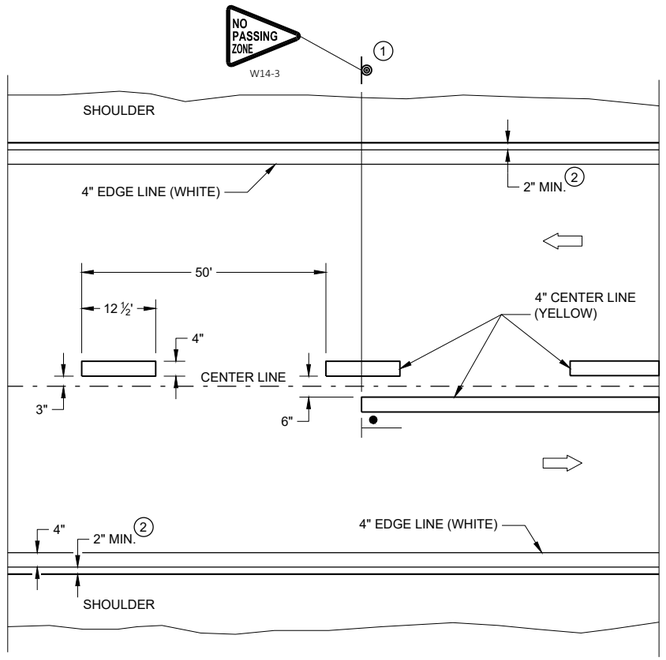
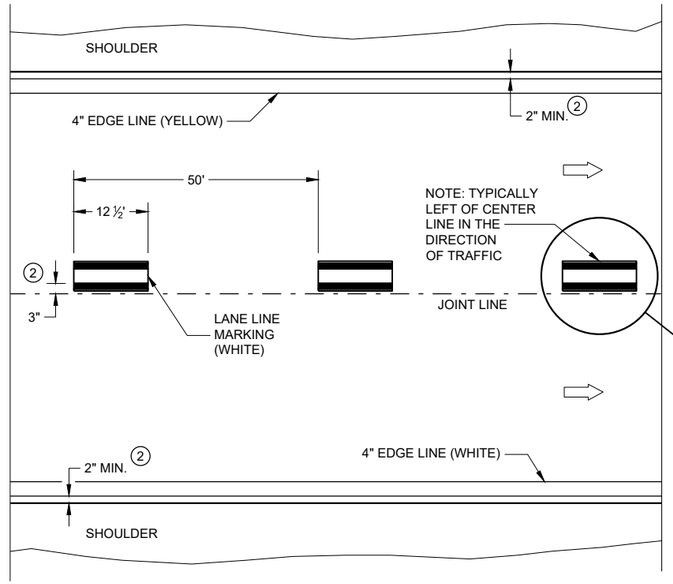
SDD 15C03 - 05

SDD 15C03 - 05

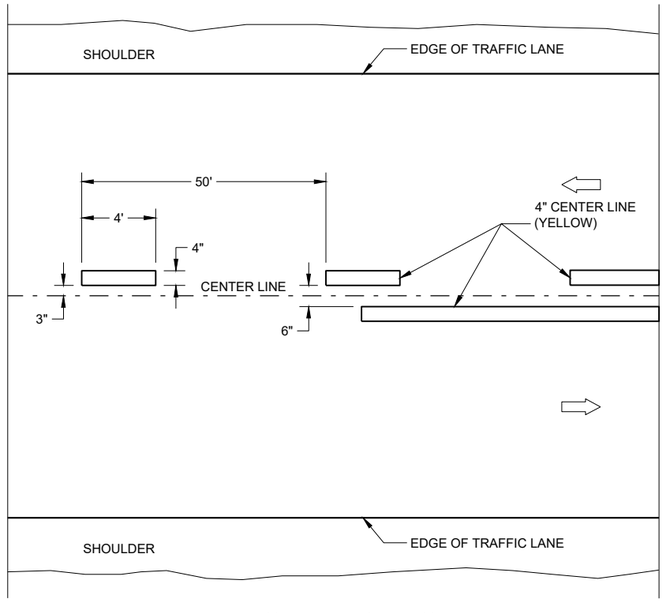


TWO WAY TRAFFIC

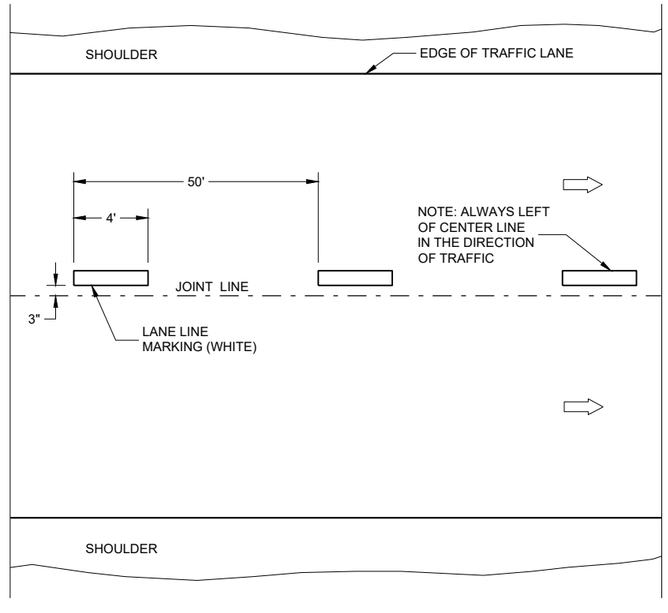


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

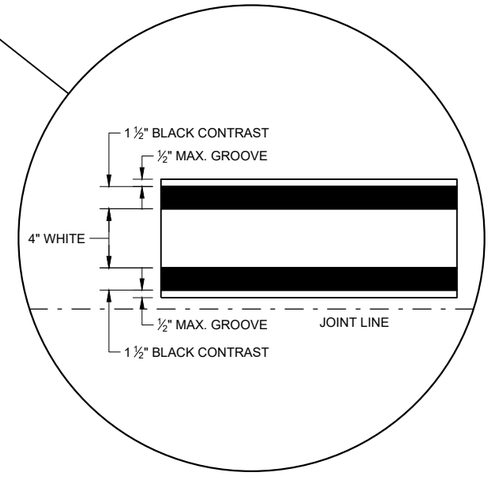
1. LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
2. MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

"T" MARKING

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC



LONGITUDINAL MARKING (MAINLINE)

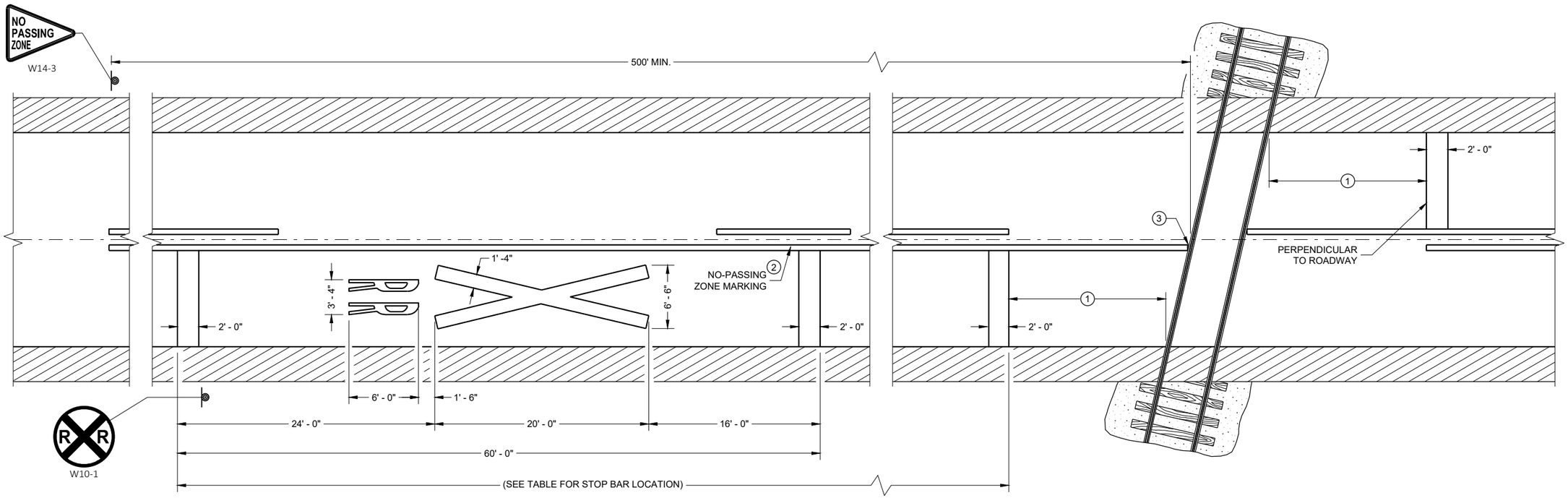
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 — DATE
/S/ Matthew Rauch — STATEWIDE SIGNING AND M ENGINEER

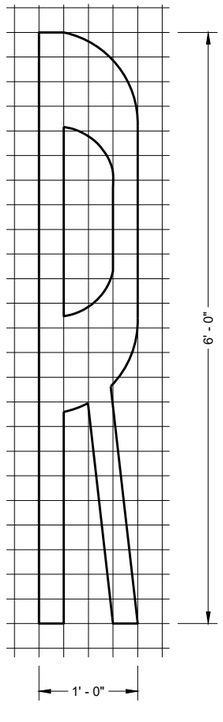
FHWA

SDD 15C08 - 20a

SDD 15C08 - 20a



PAVEMENT MARKING



LEGEND

SIGN ON PERMANENT SUPPORT

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

ON MULTI-LANE ROADS THE TRANSVERSE BANDS SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.

CENTER OR LANE LINES AND NO-PASSING ZONE MARKINGS SHOWN ON THIS DRAWING ARE REQUIRED AND PAID FOR UNDER OTHER ITEMS IN THE CONTRACT.

TRACE EXISTING SYMBOL WHERE EXISTING SYMBOLS ARE PLACED.

(1) MINIMUM 8' FROM ANY RAILROAD WARNING DEVICES (SIGNAL , GATES, ETC.) OR 25' FROM THE NEAREST RAIL, WHICHEVER DISTANCE IS GREATER.

(2) 500' MINIMUM. MARKING LIMITS MAY BE EXTENDED AS DIRECTED BY THE ENGINEER TO MEET ADJACENT NO-PASSING ZONE MARKINGS.

(3) FOR MULTIPLE TRACK CROSSINGS, THE BARRIER LINE SHALL EXTEND TO THE NEAR RAIL OF THE FURTHEST TRACK IN THE DIRECTION OF HIGHWAY TRAVEL.

DISTANCE TABLE

TABLE BASED UPON 2C-4 WISCONSIN SUPPLEMENT OF MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

POSTED SPEED (M.P.H.)	DIMENSION RANGE (FEET)
25	150* - 250'
30	200* - 300'
35	250* - 450'
40	300* - 500'
45	400* - 650'
50	550* - 800'
55	750* - 1000'
60	1000* - 1250'
65	1000* - 1250'

* THE MINIMUM DISTANCES IN THE TABLE ARE DESIRABLE AND SHOULD BE USED. THE DISTANCES MAY BE INCREASED UP TO THE MAXIMUM TO ALLOW FOR FIELD CONDITIONS SUCH AS THE CLOSED PROXIMITY OF DRIVEWAYS, BRIDGES, SIDE ROADS OR OTHER FEATURES THAT WOULD PROHIBIT THE MINIMUM DISTANCES FROM BEING USED.

SIGNING AND PAVEMENT MARKING DETAILS FOR RAILROAD - HIGHWAY GRADE CROSSINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

February 2021
DATE

/S/ Matthew R. Rauch
STATE SIGNING AND MAF ENGINEER

FHWA

6

SDD 15C09 - 12a

SDD 15C09 - 12a

LEGEND

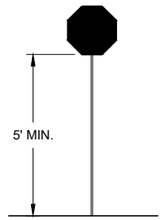
SIGN ON PORTABLE OR PERMANENT SUPPORT

TEMPORARY PORTABLE RUMBLE STRIP ARRAY

DIRECTION OF TRAFFIC

WORK AREA

FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF



STOP/SLOW PADDLE ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

(1) FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.

(2) SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

(3) EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FOR THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

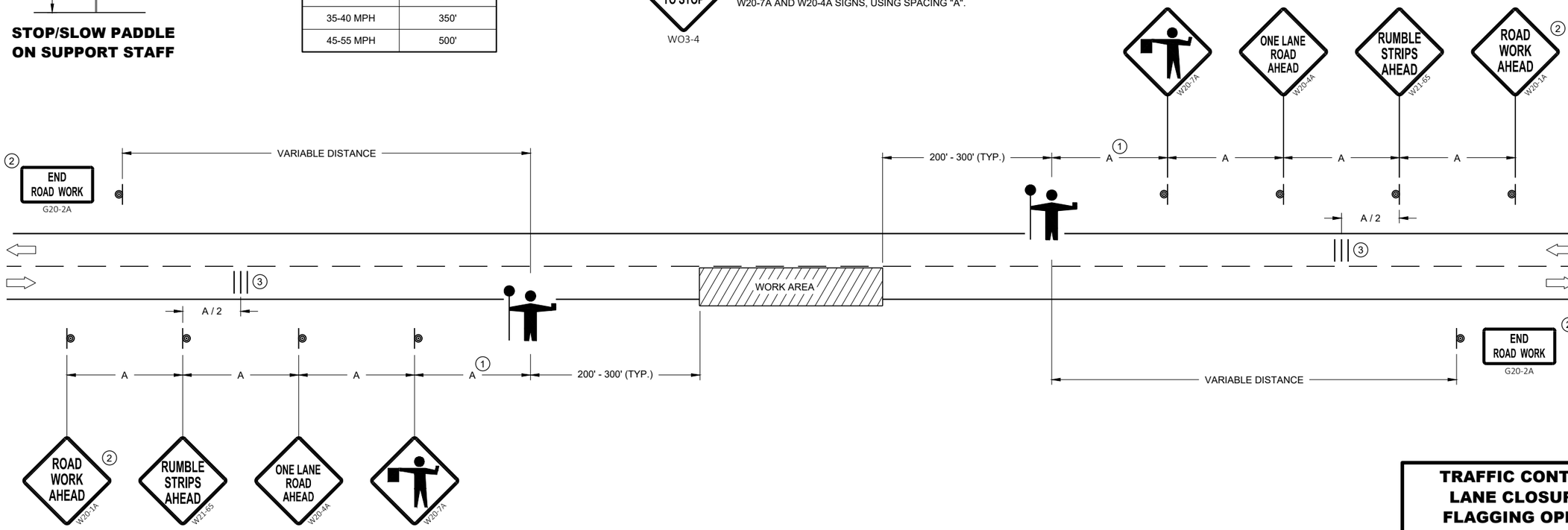
DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'

WO3-4

USE OF WO3-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

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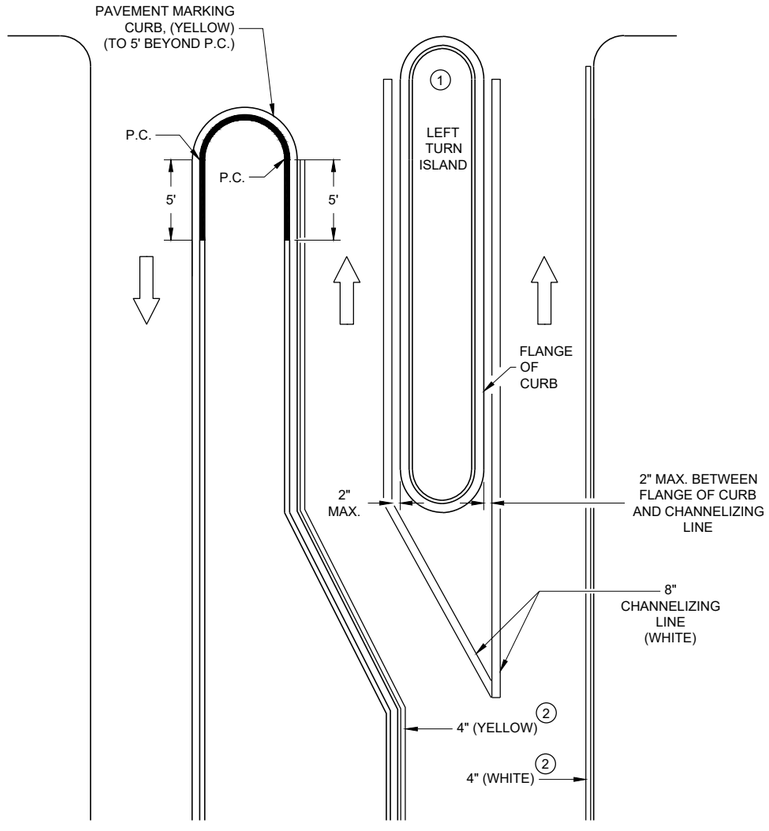
SDD 15C12 - 07

REQUIREMENTS FOR EDGE LINES		
POSTED SPEED	IS THERE CONTINUOUS LIGHTING?	
	YES	NO
≤ 30 MPH	NO	OPTIONAL
35 OR 40 MPH	OPTIONAL	RECOMMENDED
≥ 45 MPH	RECOMMENDED	REQUIRED



LEFT TURN & MEDIAN ISLAND

GENERAL NOTES

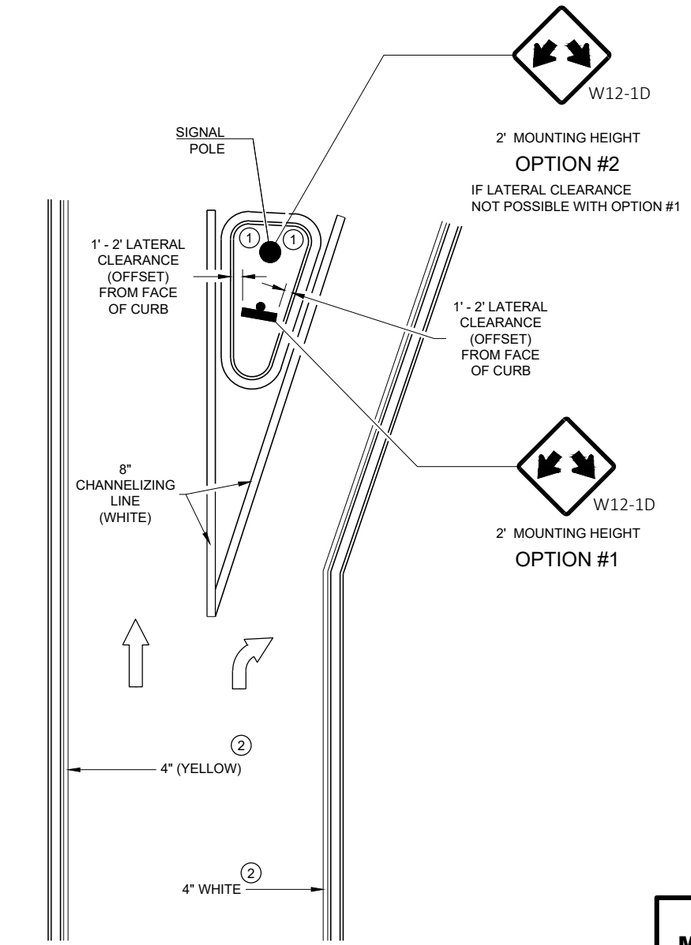
APPLIES TO ISLANDS AT LEFT TURNS AT ONE WAY ROADWAYS AS WELL.

SEE MISCELLANEOUS QUANTITIES FOR SIGN SIZE.

(1) MARK CURB NOSES YELLOW.

(2) MARK ACCORDING TO TABLE.

DIRECTION OF TRAVEL



RIGHT TURN ISLAND

MEDIAN PAVEMENT MARKINGS, DOUBLE ARROW WARNING SIGN PLACEMENT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

February 2021
DATE

/S/ Matthew R. Rauch
STATE SIGNING AND MAF ENGINEER

FHWA

6

SDD 15C18 - 05c

6

SDD 15C18 - 05c

LEGEND

V1 LEAD VEHICLE

V2 MARKING VEHICLE

V3 SHADOW VEHICLE

TRUCK MOUNTED ATTENUATOR (TMA)

SIGN ON TEMPORARY SUPPORT

DIRECTION OF TRAFFIC

FLASHING ARROW PANEL (CAUTION)

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

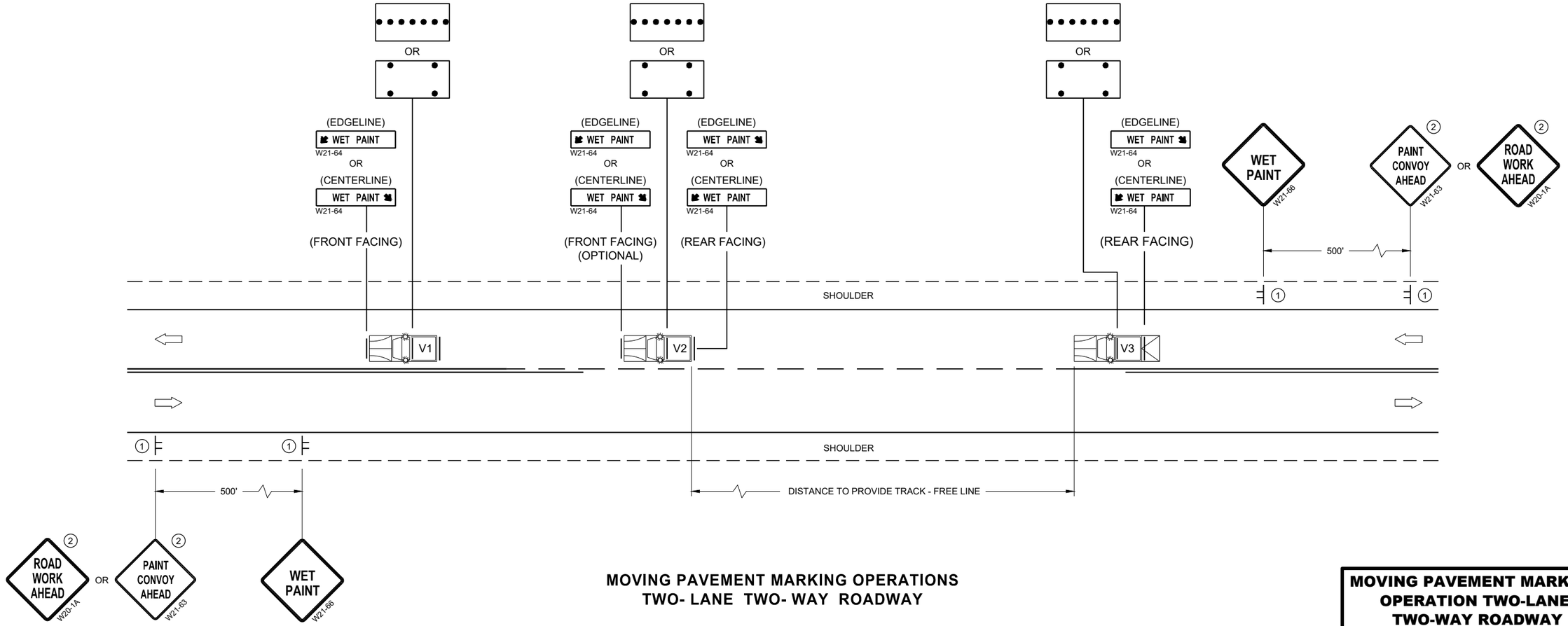
WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING .

(1) SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.

(2) IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.



MOVING PAVEMENT MARKING OPERATIONS TWO- LANE TWO- WAY ROADWAY

MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

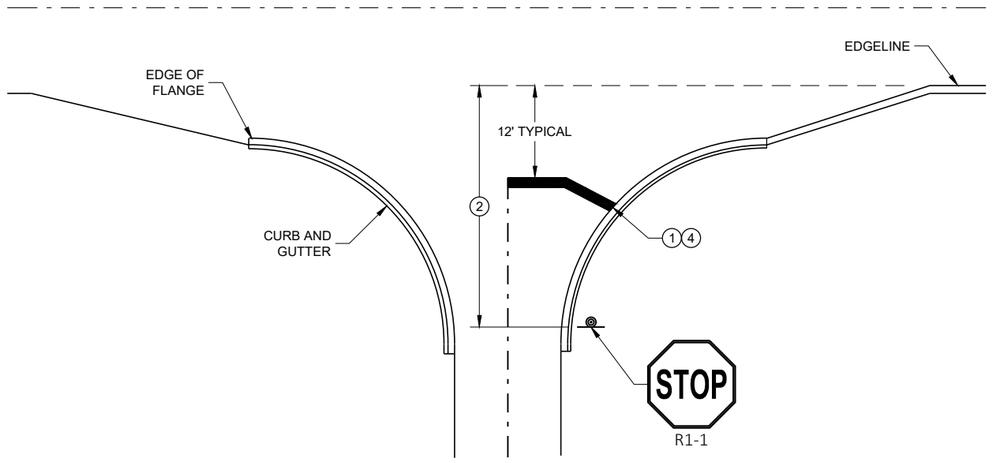
SDD 15C19 - 06a

SDD 15C19 - 06a

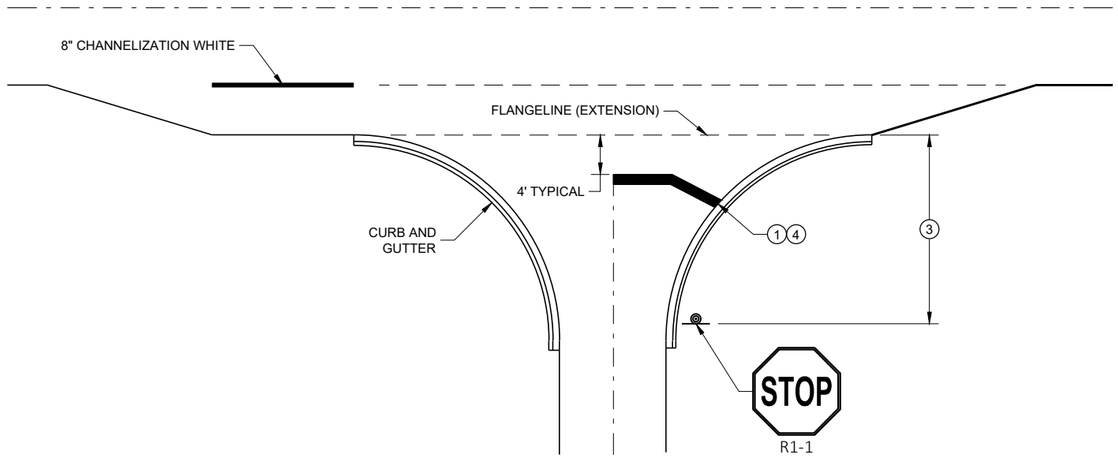
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

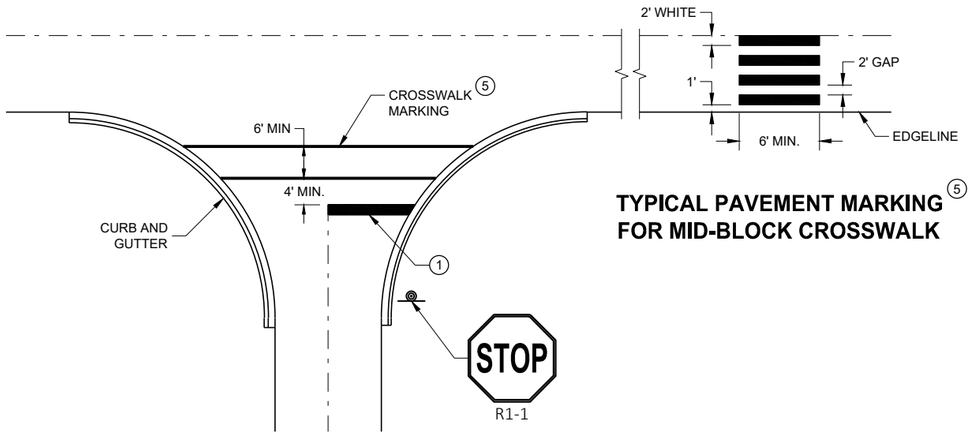
1. 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
2. NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
3. NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
4. MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
5. LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

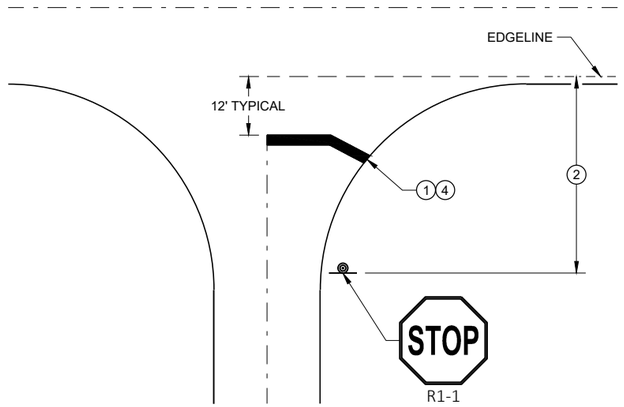


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK 5

TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 — DATE
/S/ Matthew Rauch — STATE SIGNING AND MARKING ENGINEER
FHWA

6

SDD 15C33 - 04

6

SDD 15C33 - 04

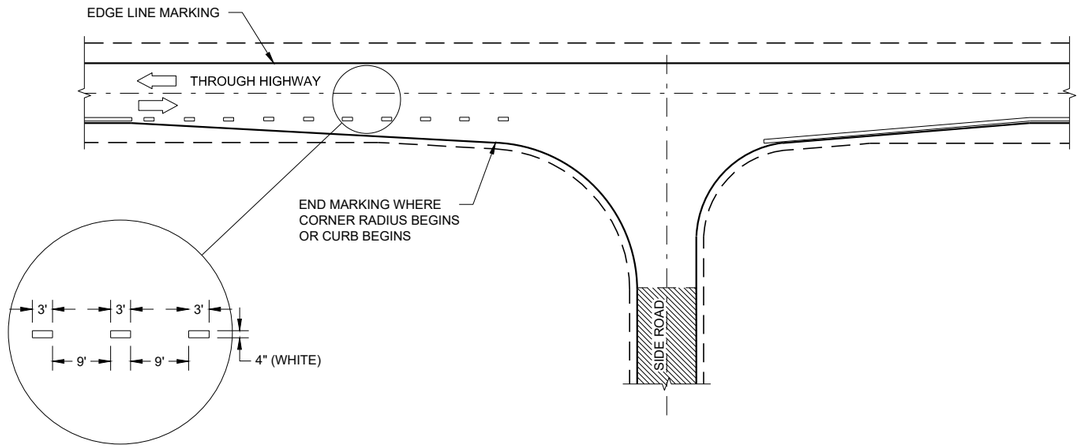
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

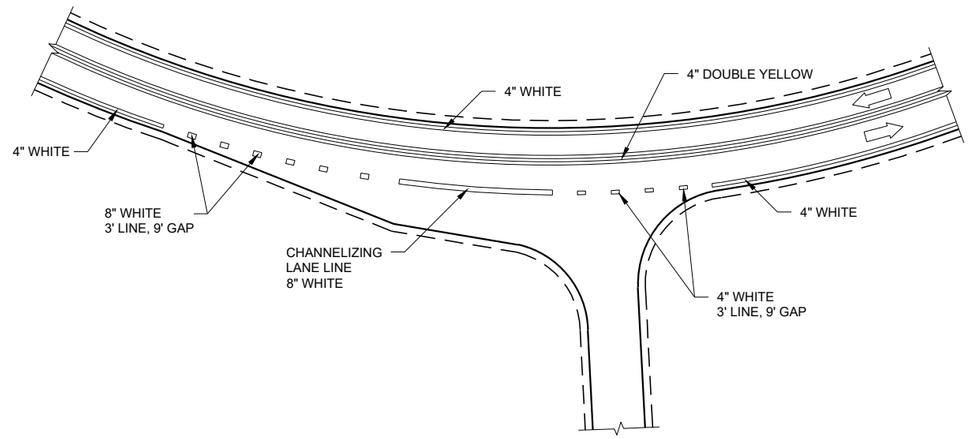
1. WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
2. WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
3. BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
4. BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.

LEGEND

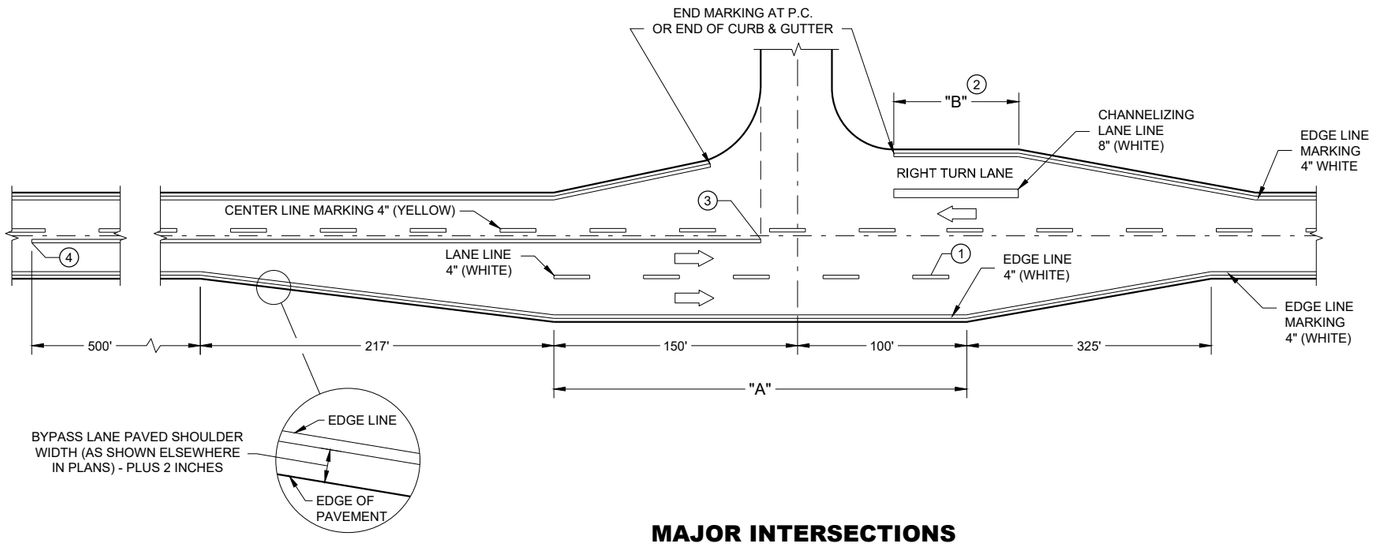
DIRECTION OF TRAVEL



MINOR INTERSECTION



INTERSECTION ON OUTSIDE OF CURVE



MAJOR INTERSECTIONS

(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)

PAVEMENT MARKING (INTERSECTIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 15C35 - 04a

LEGEND

SIGN ON PERMANENT SUPPORT

TRAFFIC CONTROL DRUM

DIRECTION OF TRAFFIC

WORK ZONE

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

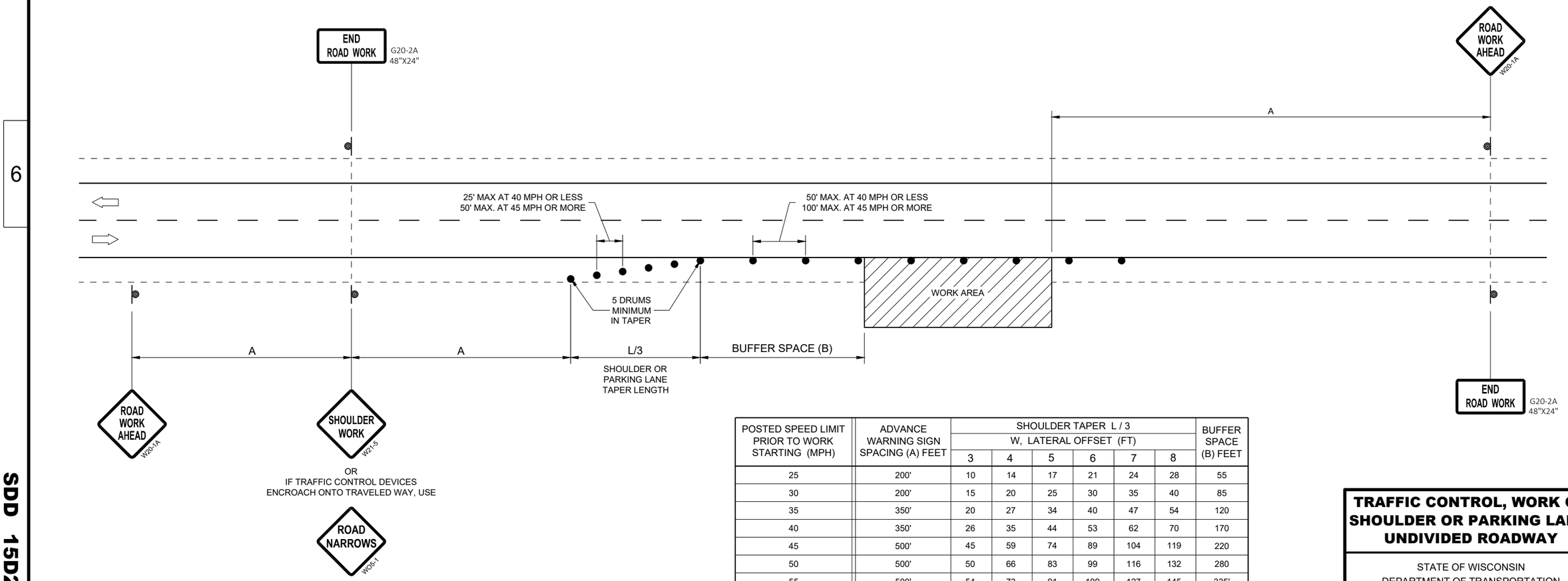
ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHOULDER TAPER L / 3 W, LATERAL OFFSET (FT)						BUFFER SPACE (B) FEET
		3	4	5	6	7	8	
25	200'	10	14	17	21	24	28	55
30	200'	15	20	25	30	35	40	85
35	350'	20	27	34	40	47	54	120
40	350'	26	35	44	53	62	70	170
45	500'	45	59	74	89	104	119	220
50	500'	50	66	83	99	116	132	280
55	500'	54	73	91	109	127	145	335'

TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2020
DATE

/S/ Andrew Heidtke
STATEWIDE WORK ZONE T SAFETY ENGINEER

FHWA

SDD 15D28 - 04

SDD 15D28 - 04

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

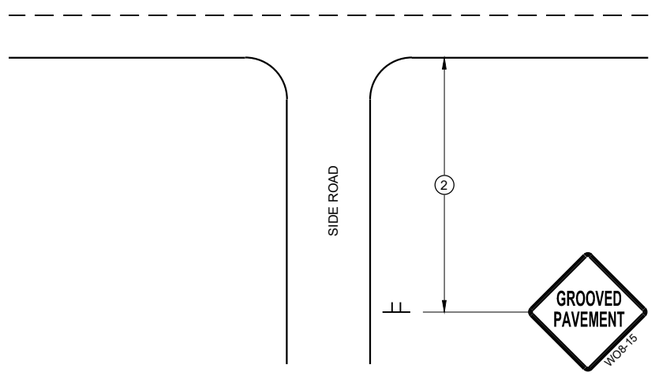
(1) PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.

(2) PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

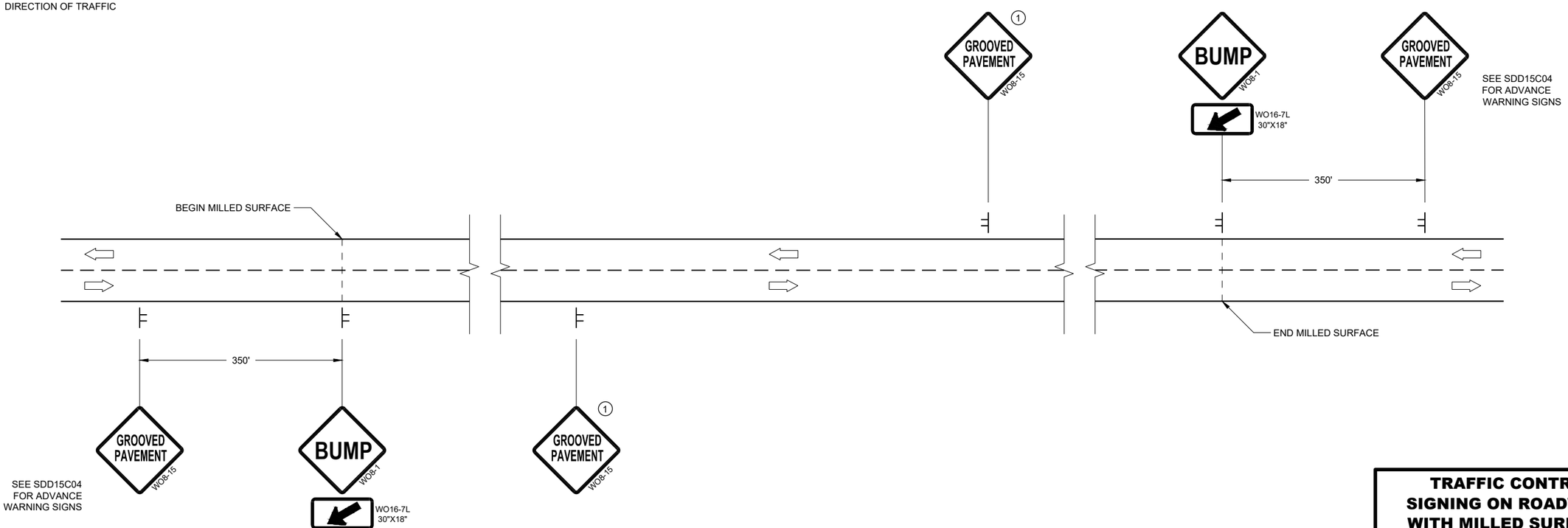
LEGEND

SIGN ON TEMPORARY SUPPORT

DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH SIGN DETAIL



DETAIL FOR SIGNING ON MILLED SURFACES

TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES

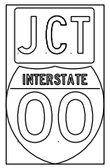
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 — DATE
/S/ Andrew Heidtke — WORK ZONE ENGINEER

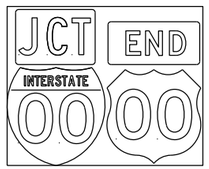
FHWA

6
SDD 15D44 - 02

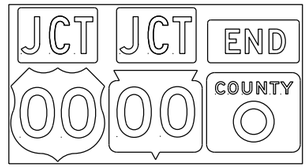
TYPICAL ASSEMBLIES



J1-1



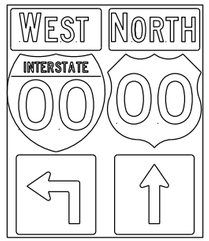
J1-2



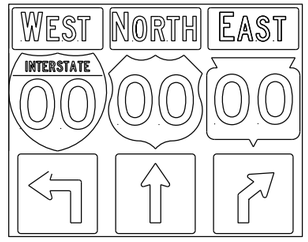
J1-3

JR1-1

J2-1

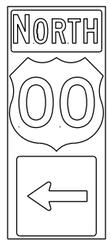


J2-2

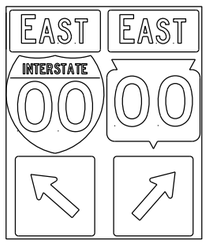


J2-3

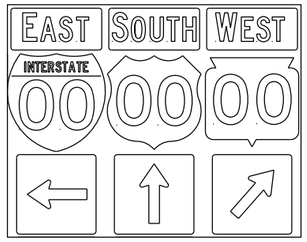
JV
(Typical Vertical J-Assembly
See Note 10 and 11)



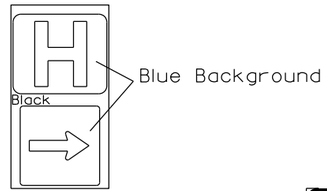
J3-1



J3-2

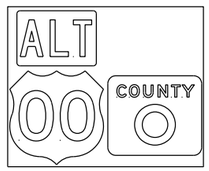


J3-3

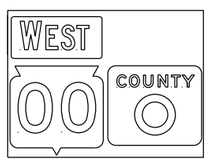


JH-1

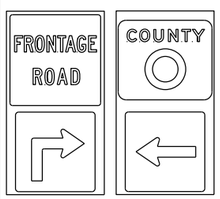
J4-1



J4-2

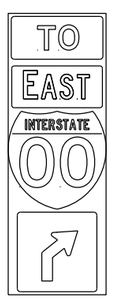


J4-2

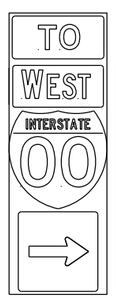


J12-1

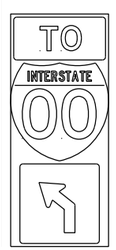
J13-1



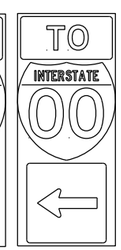
J32-1



J33-1



J22-1

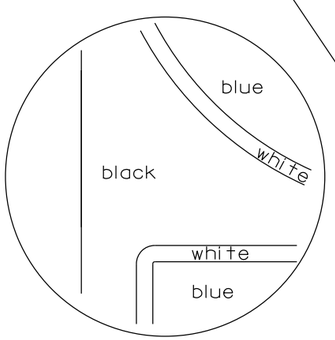


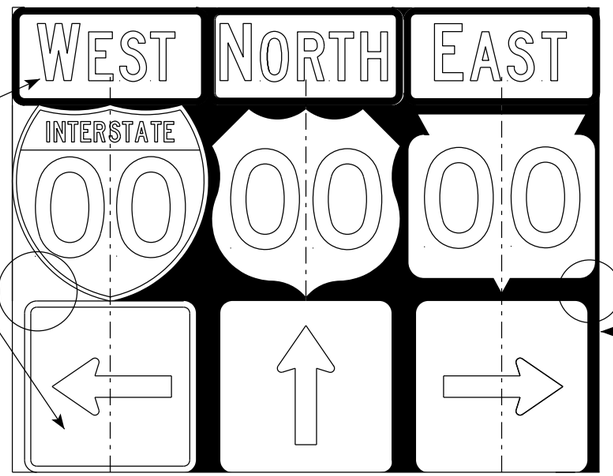
J23-1

JR13-1

JR23-1

JR99-1





NOTES

1. Signs are Type II - Type H Reflective
2. Color:
   Background - Black Non-reflective
   Message - see Note 5
3. Message Series - See Note 5
4. Corners shall be square or rounded if base material is plywood. If base material is metal the corners shall be rounded.
5. The colors and message spacing on each marker shall be according to the applicable route marker panel specifications.
6. Certain marker heads require the component pieces to be the same color. As an example, all the components used with an M1-1 Interstate marker shall be blue.
7. Single panel j-assemblies shall only be used with route marker shields that are same size. If the route marker shields are different size use multiple piece component.
8. Route assemblies that have 24 inch route shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
9. Route assemblies that have 36 inch shields and have dimensions greater than 48 inchs (both vertical and horizontal) shall have two horizontal splices. One horizontal splice shall be between the cardinal direction and route shields and the other horizontal splice shall be between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
10. All Vertical J Assemblies are given a Sign Code of JV
11. For JV Assemblies that have a mixture of Interstate and non Interstate shields, arrows and cardinals shall be white on blue.

ROUTE MARKERS & COMPONENTS
IN TYPICAL ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

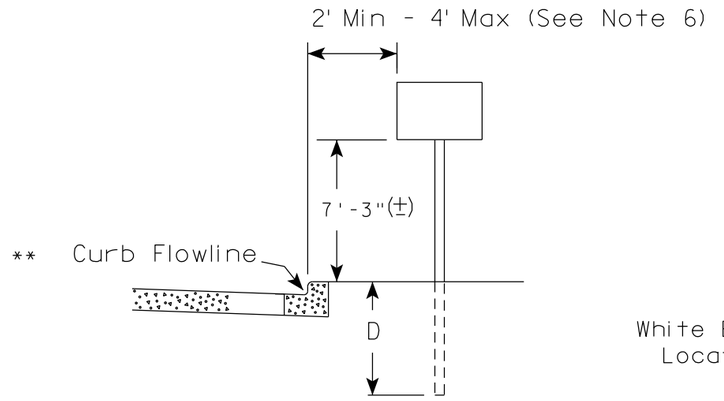
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/18/21 PLATE NO. A2-1S.9

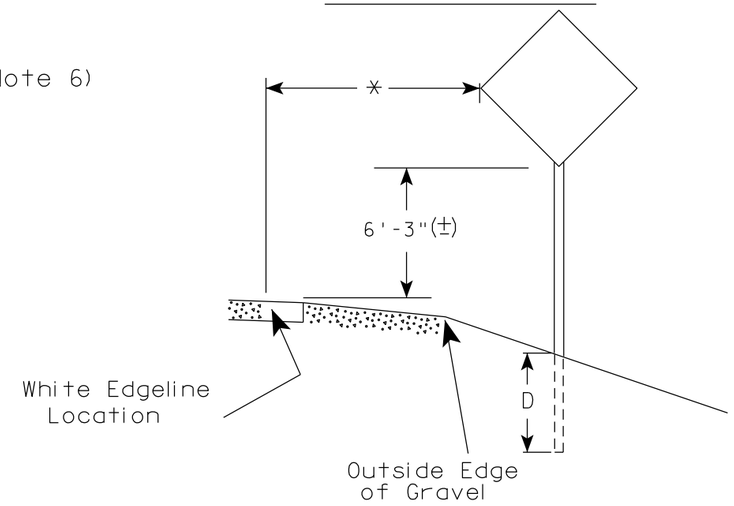
PROJECT NO:

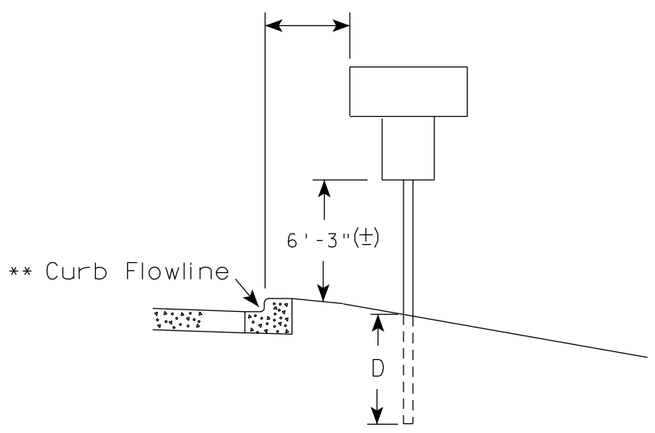
SHEET NO: 73 E

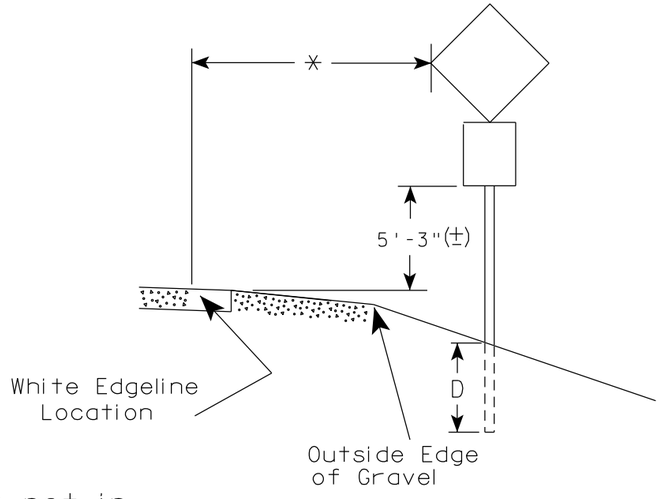
URBAN AREA



RURAL AREA (See Note 2)







** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
   The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directd by the Engineer.

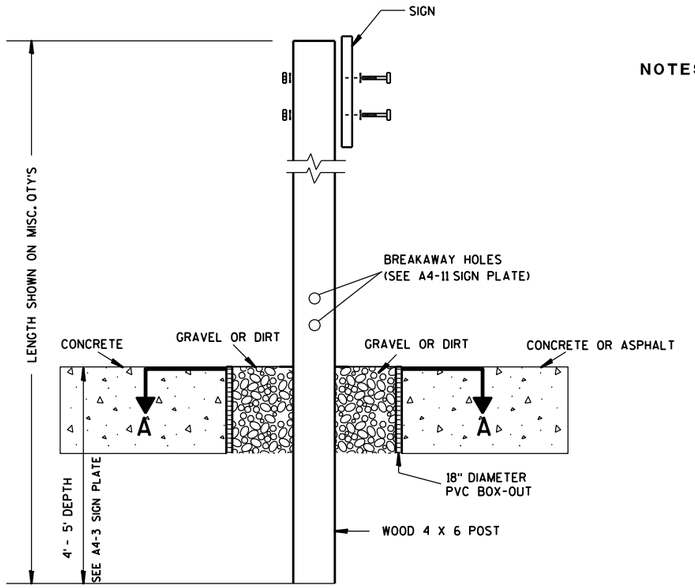
TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/13/2020 PLATE NO. A4-3.22

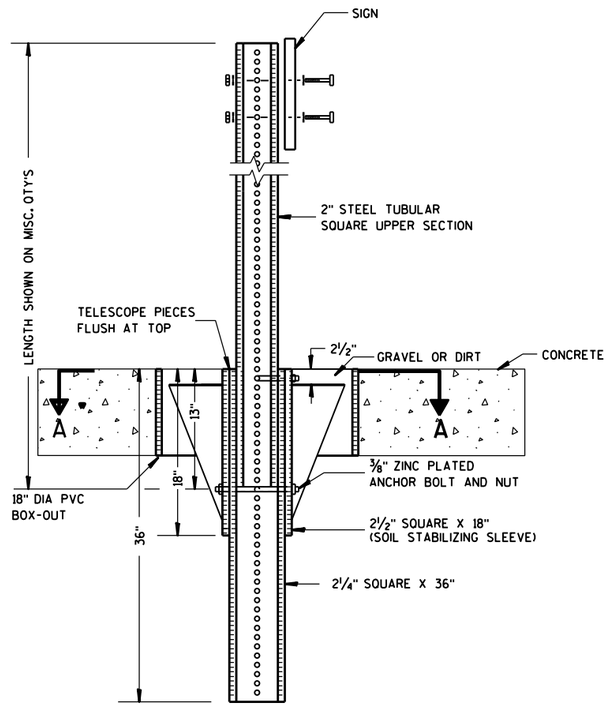
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 74	E



ELEVATION VIEW

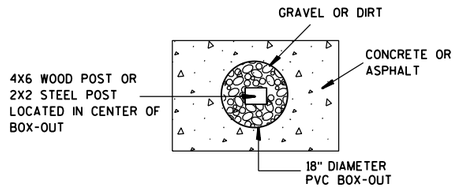
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

NOTES:
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

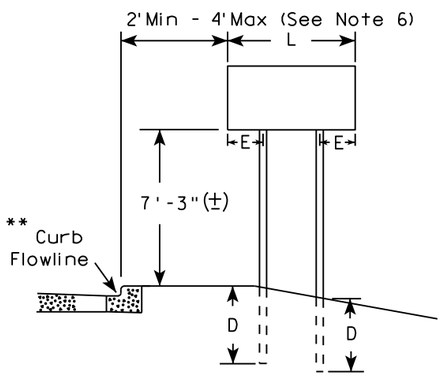
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

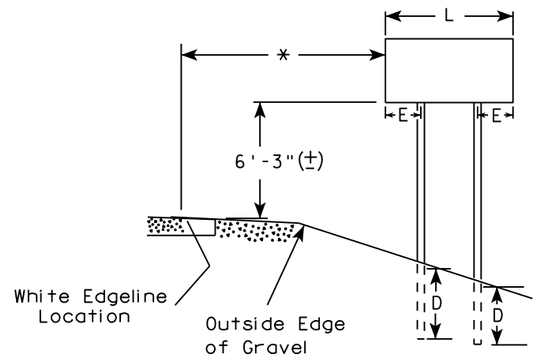
PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

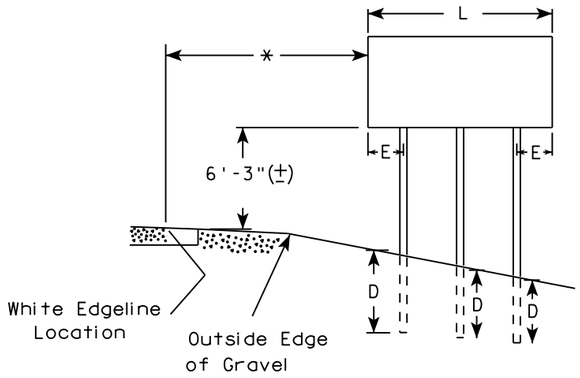
FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN PLOT DATE : 27-JAN-2014 09:48 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42

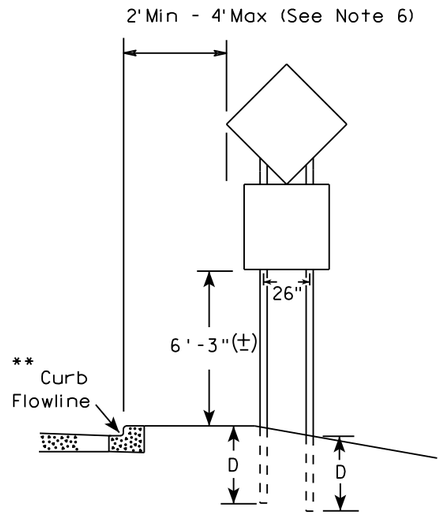
URBAN AREA



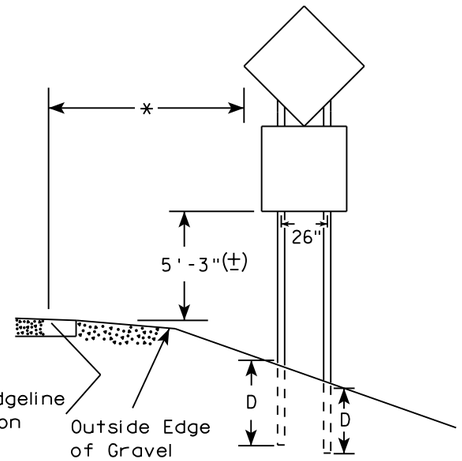
RURAL AREA (See Note 3)







48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)		
	L	E
***	Greater than 48" Less than 60"	12"
	60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

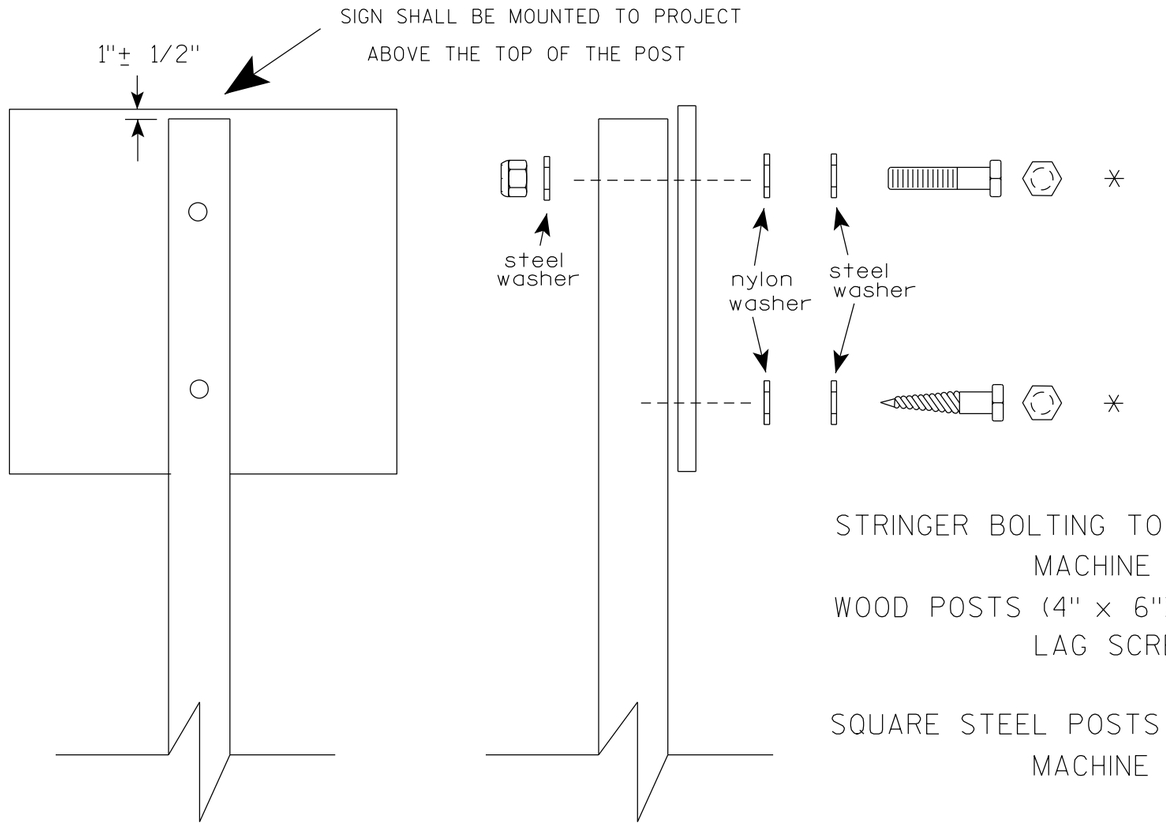
TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 8/21/17 PLATE NO. A4-4.15

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 76	E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3

b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 4/1/2020 PLATE NO. A4-8.9

PROJECT NO:

SHEET NO: 77 E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A48.DGN PLOT DATE : 01-APRIL-2020 PLOT BY : dotc4c

WISDOT/CADDS SHEET 42

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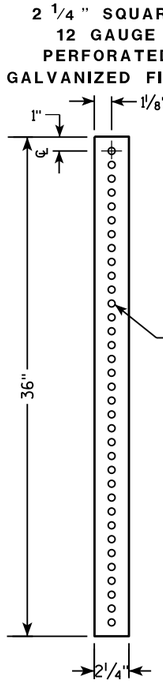
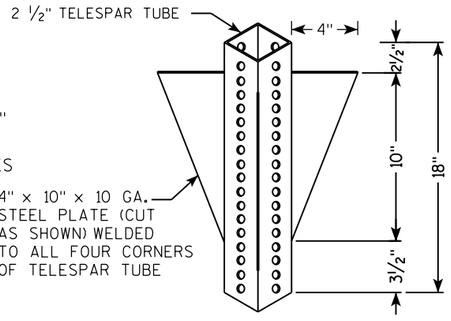
TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM

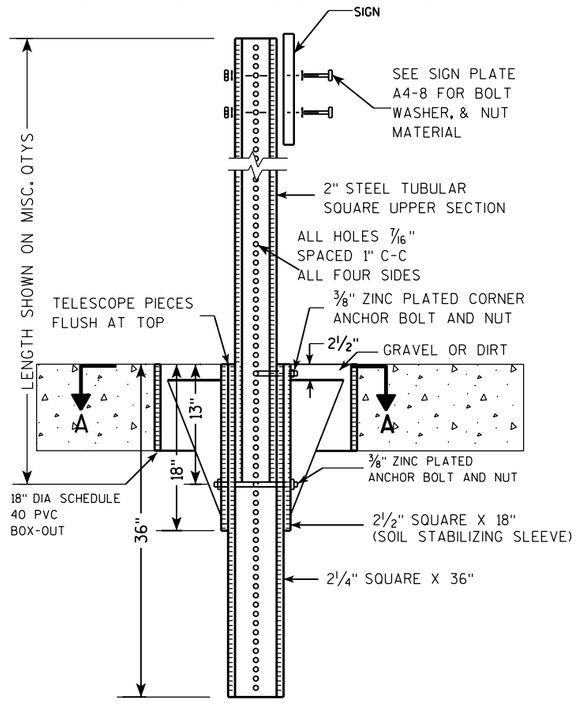
2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



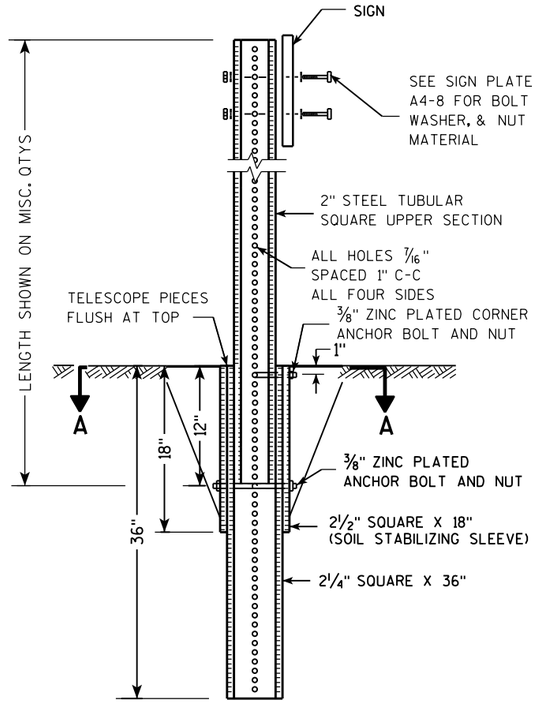
2 1/2 " SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH

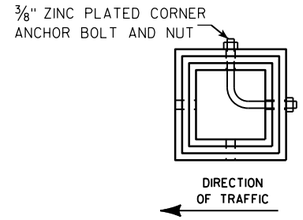


DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)





SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

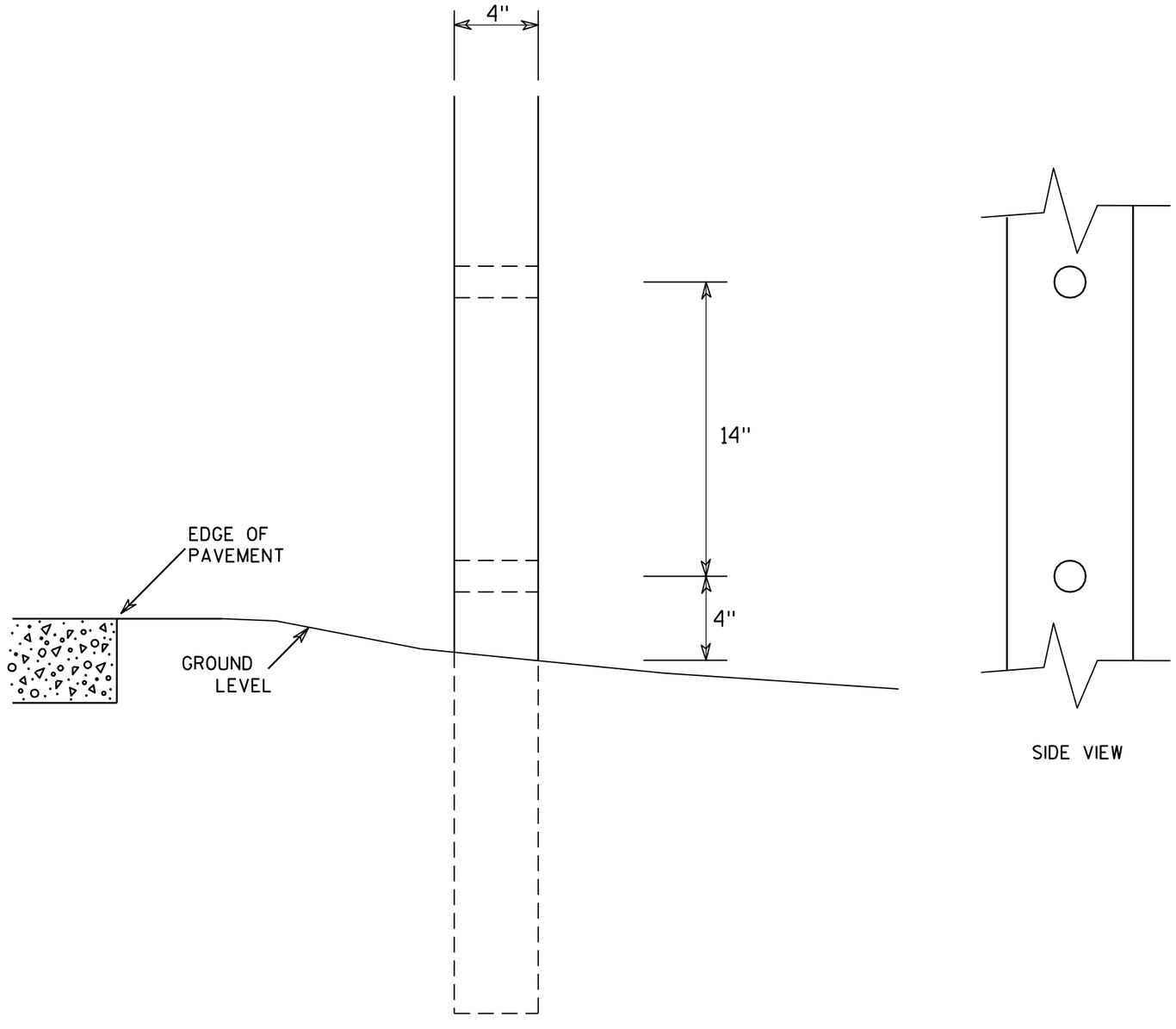
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/05/15 PLATE 78 A4-9.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE : 05-FEB-2015 17:09 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000

WISDOT/CADDS SHEET 42



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

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4 X 6 WOOD POST MODIFICATIONS

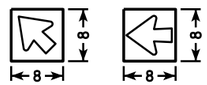
WISCONSIN DEPT OF TRANSPORTATION

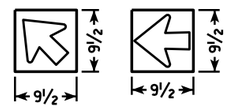
APPROVED Chester J Spang for State Traffic Engineer

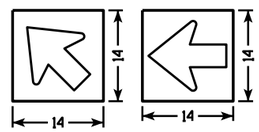
DATE 3/27/97 PLATE NO. A4-11.2

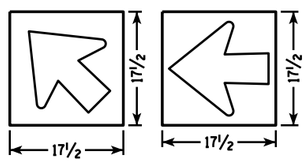
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 79	E

SIGN LAYOUT WITH VARIOUS SIZED MESSAGES







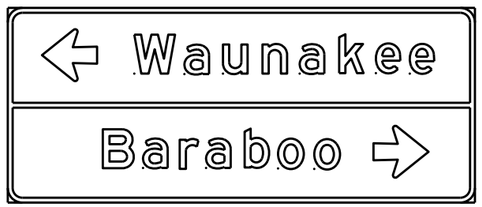


GENERAL NOTES

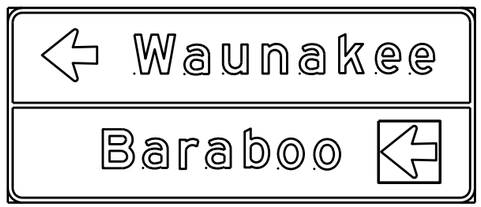
1. Materials shall conform to Standard Specification Section 637.
   Base - Sheet Aluminum 0.040" Thickness
   Sheeting - Orange Type F Reflective
   Arrow - Black Non-Reflective
2. Arrow signs shall be fastened to permanent sign by either aluminum rivets or aluminum self-tapping sheet metal screws. There shall be a minmum of 2 fasteners used per arrow sign.
3. There shall be a spacer consisting of a 0.08" nylon washer between the back of the arrow sign and the face of the permanent sign.
4. Arrows are per standard plate A1-2
5. Use separate arrow sign for each destination
6. Tilt arrow is always at 45 degrees
7. Arrow is centered on arrow sign

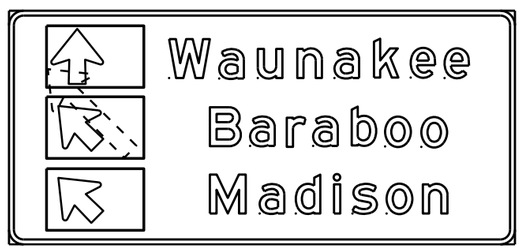
Lower Case Copy Size	Standard Width (Single Arrow)	2 Line Tilt Arrow Cover Width	3 Line Tilt Arrow Cover Width	Height
3¾" Series C	8	9 ½	14 ½	8
4½" Series D & E	9 ½	10	15	9 ½
6" Series D & E	14	16	20 ½	14
8" Series E	17 ½	20 ½	25	17 ½

BEFORE



AFTER





DESTINATION DIRECTIONAL ARROW FOR DETOUR SIGNS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/08/14

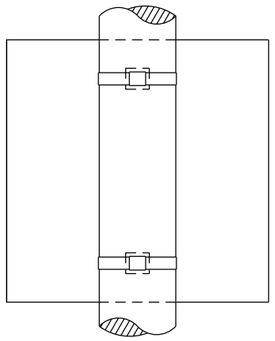
A4-12.2

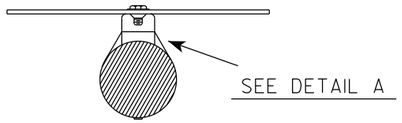
PROJECT NO:

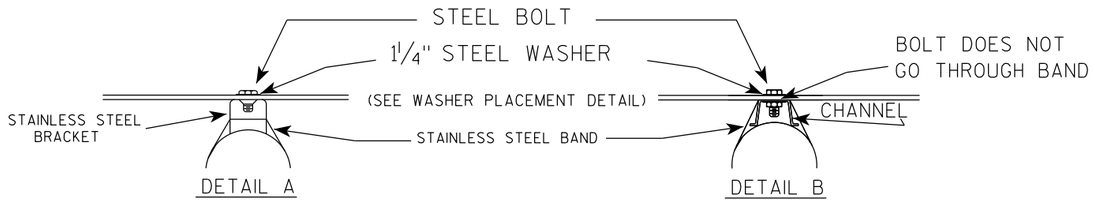
SHEET NO: E

BANDING

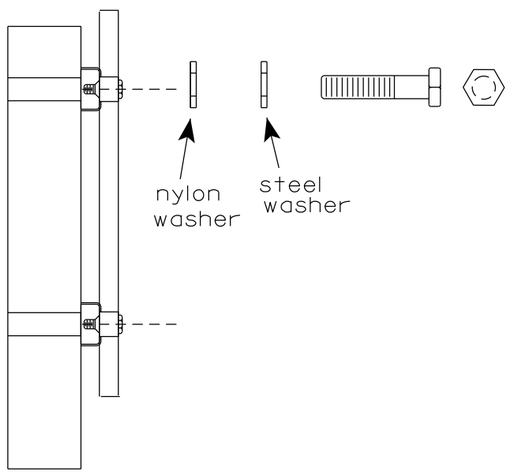
SINGLE SIGN







WASHER PLACEMENT

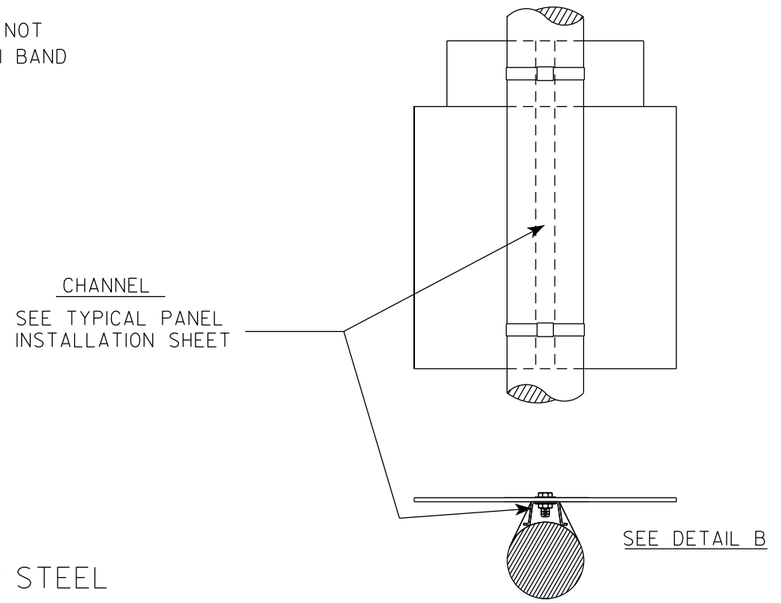


WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be ¾" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 6/10/19 PLATE NO. A5-9.4

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 81	E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A59.dgn PLOT DATE : 10-JUN 2019 4:10 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : $$.....plotscale.....$$ WISDOT/CADDS SHEET 42

7

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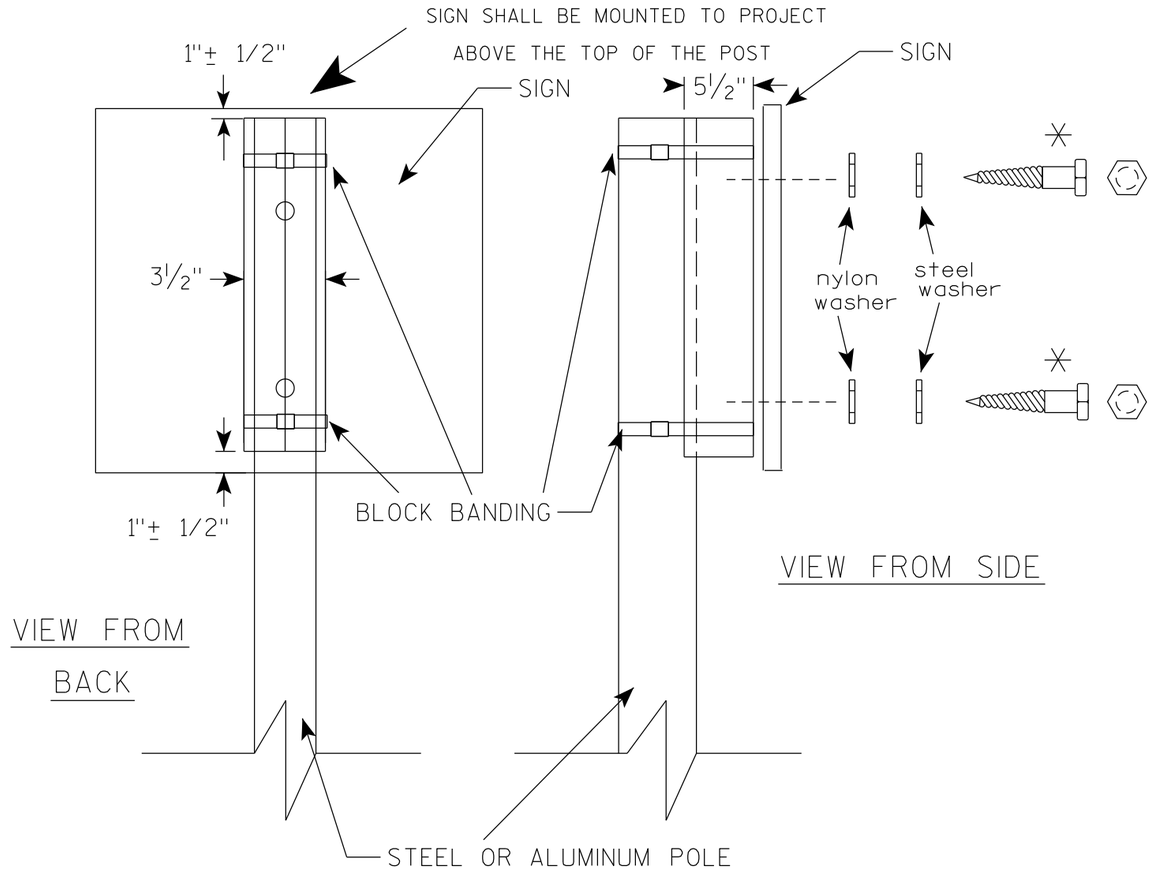
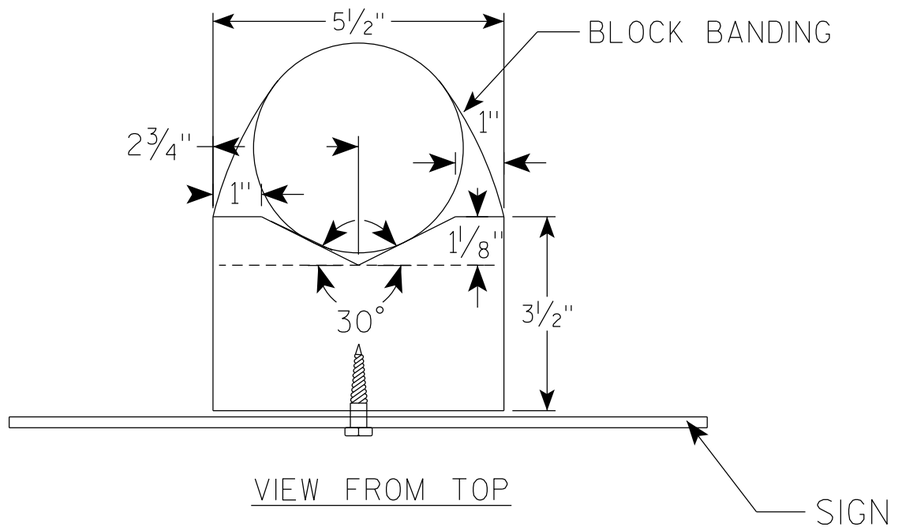




GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WisDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, ¾" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORNALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✳ LAG BOLTS SHALL BE ⅜" X 2½"

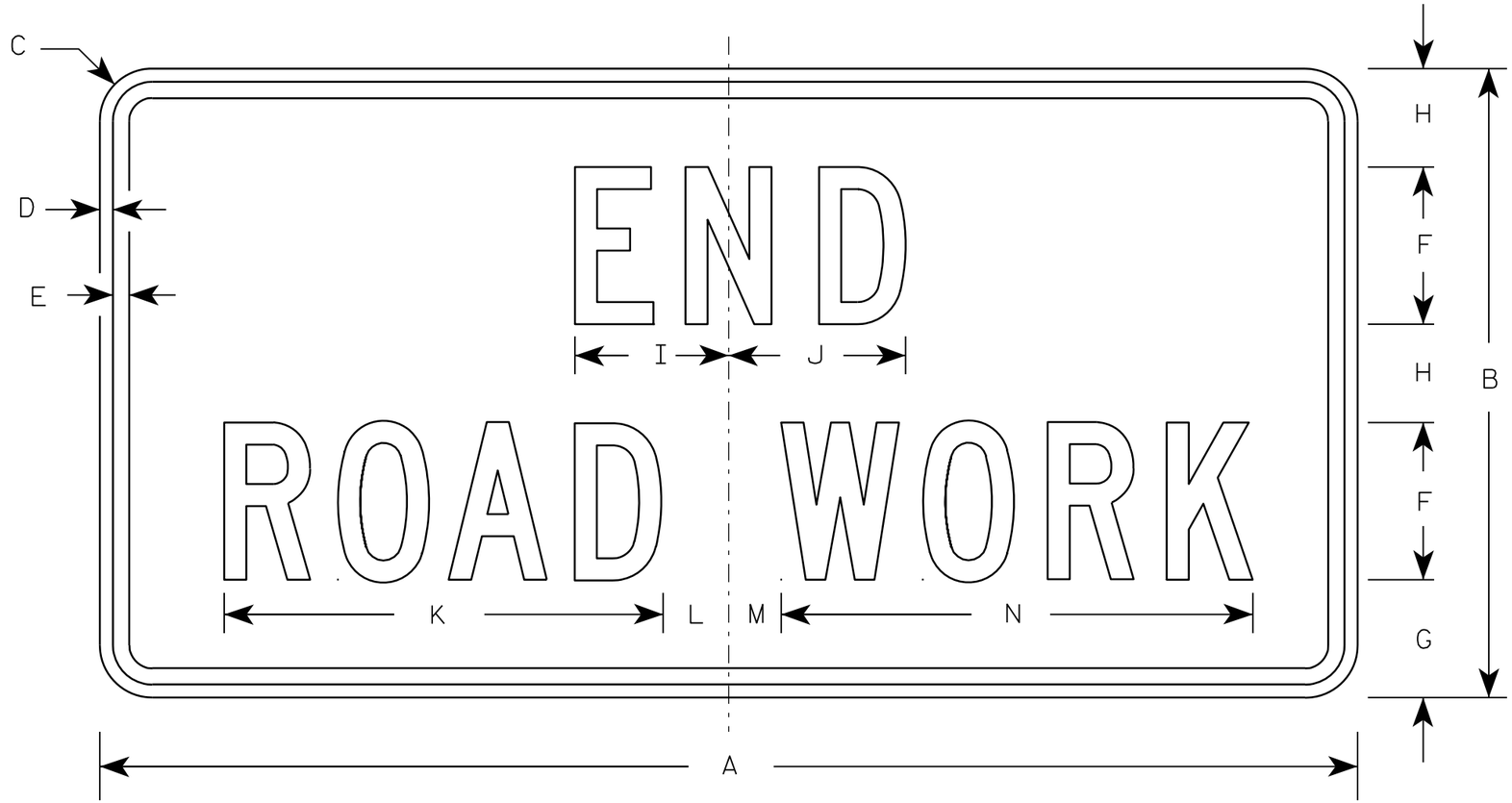
BLOCK BANDING DETAIL (V-BLOCK OPTION)

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 6/10/19 PLATE NO. A5-10.2

PROJECT NO:				SHEET NO: 82	E



G20-2A

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

Metric equivalent for this sign is:

SIZE	
1	900 mm X 450 mm
2	1200 mm X 600 mm
3	1200 mm X 600 mm
4	1200 mm X 600 mm
5	1200 mm X 600 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m2
1	36	18	1 1/8	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5	0.41
2	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
3	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
4	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
5	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72

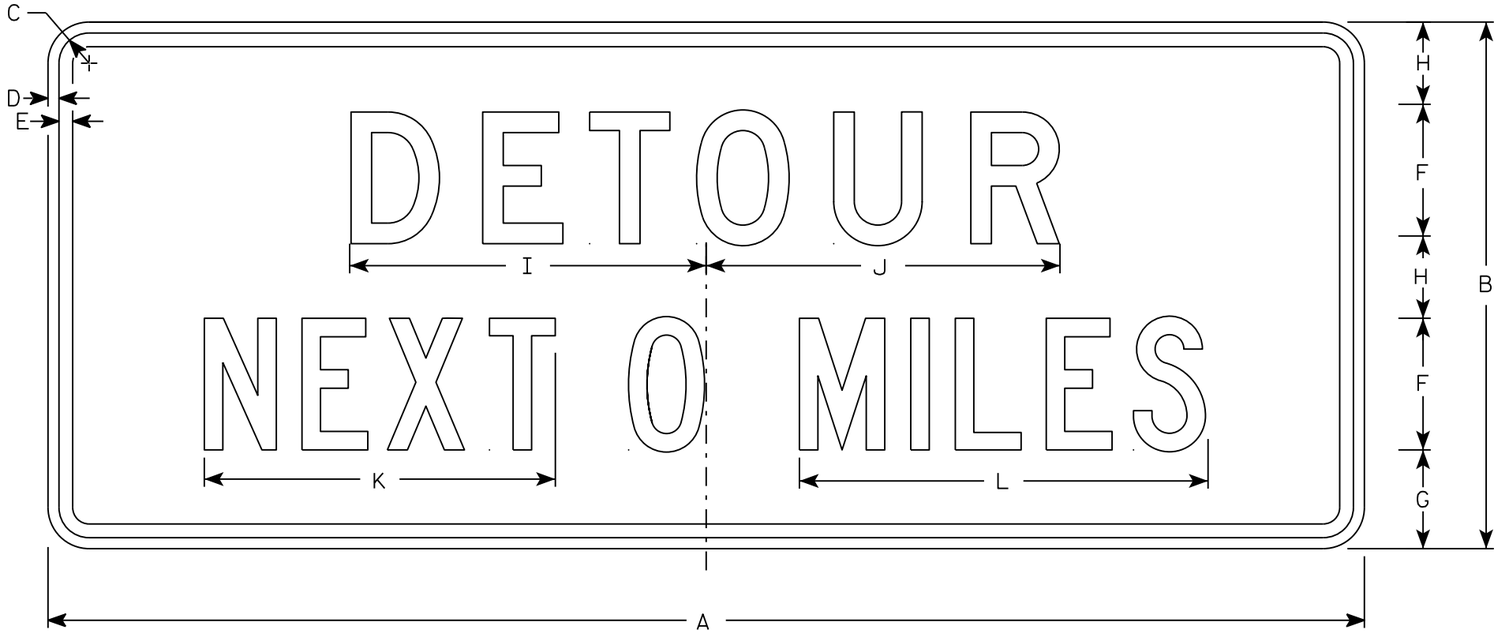
STANDARD SIGN
G20-2A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 9/30/09 PLATE NO. G20-2A.8

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 83	E



G20-51

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - Line 1 is D and Line 2 is C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Round distance to nearest whole Mile and substitute appropriate numerals and optically adjust spacing to achieve proper balance

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	60	24	1 3/8	1/2	5/8	6	4 1/2	3 3/4	16 1/4	16 1/8	16	18 5/8															10
3																											
4	60	24	1 3/8	1/2	5/8	6	4 1/2	3 3/4	16 1/4	16 1/8	16	18 5/8															10
5																											

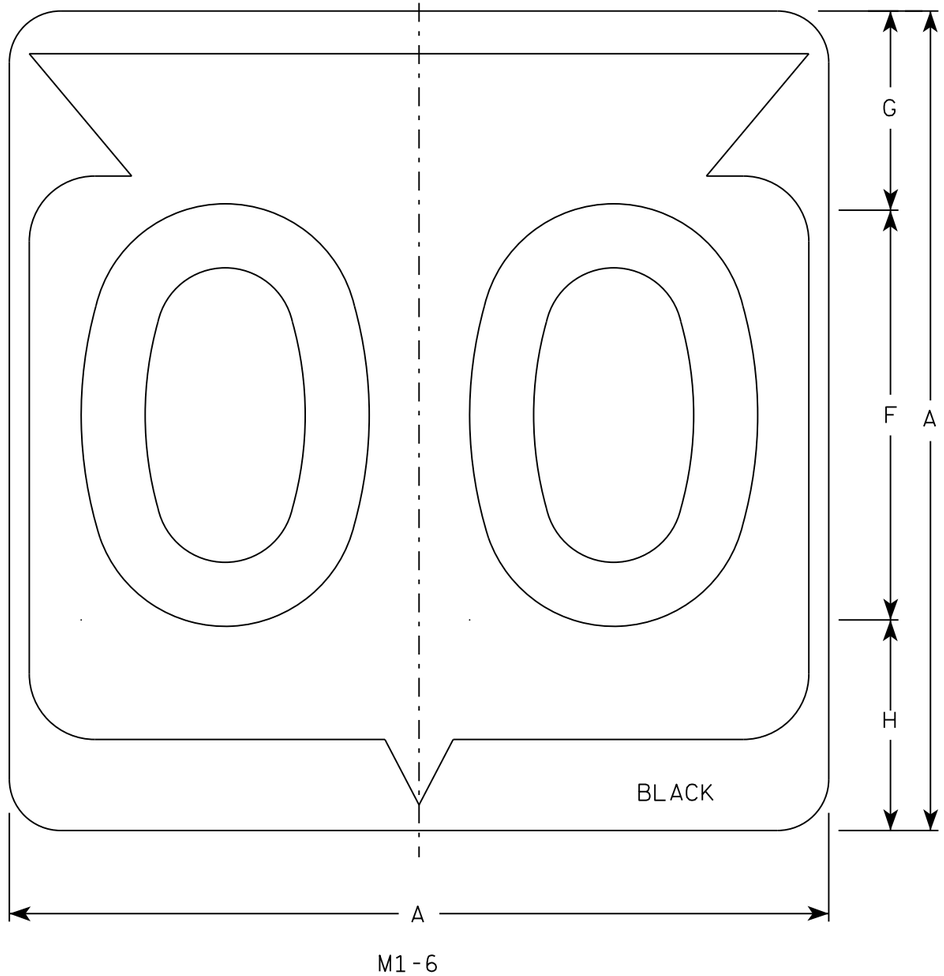
STANDARD SIGN
G20-51

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/14/17 PLATE NO. G20-51.2

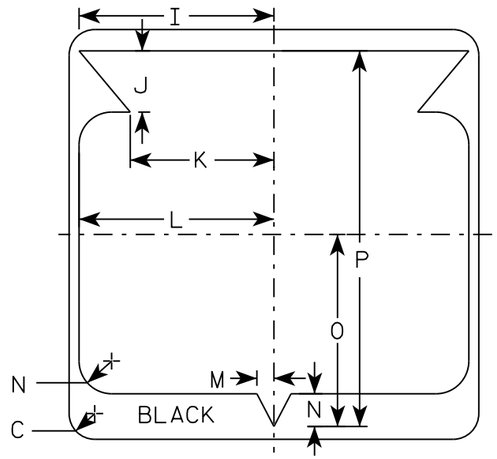
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 84 | E



M1-6

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - D except 3 number signs Series C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0

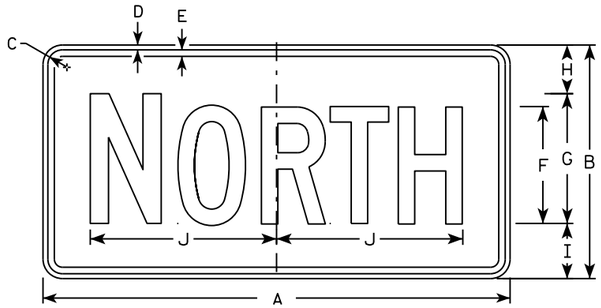
STATE ROUTE MARKER M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

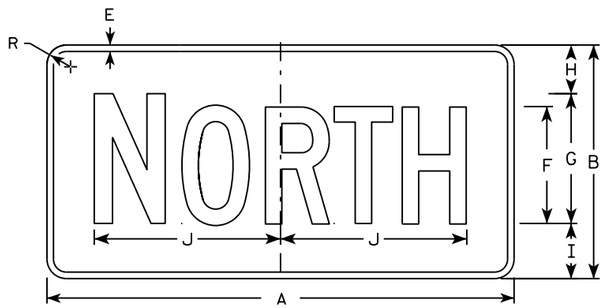
APPROVED Matthew R Rauch for State Traffic Engineer

DATE 3/16/18 PLATE NO. M1-6.10

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 85 | E



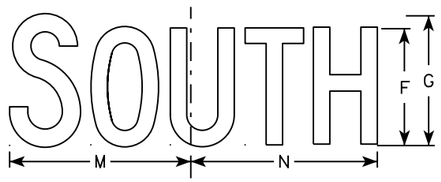
M3-1
MM3-1
MP3-1



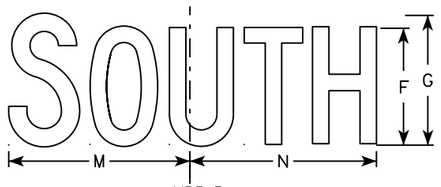
MB3-1
MK3-1
MN3-1

M3-2
MM3-2
MP3-2

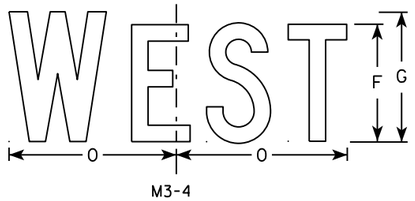
MB3-2
MK3-2
MN3-2



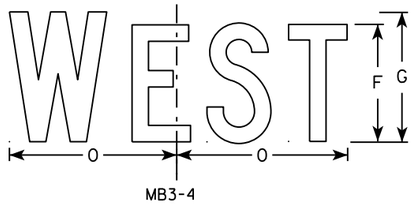
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

NOTES

1. All Signs Type II - Type H
2. Color:
   Background - See note 5
   Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M3-1 thru M3-4 Background - White
   Message - Black
   MB3-1 thru MB3-4 Background - Blue
   Message - White
   MK3-1 thru MK3-4 Background - Green
   Message - White
   MM3-1 thru MM3-4 Background - White
   Message - Green
   MN3-1 thru MN3-4 Background - Brown
   Message - White
   MP3-1 thru MP3-4 Background - White
   Message - Blue
6. Note the first letter of each direction is larger than the remainder of the message.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

STANDARD SIGNS
M3-1 thur M3-4
SERIES

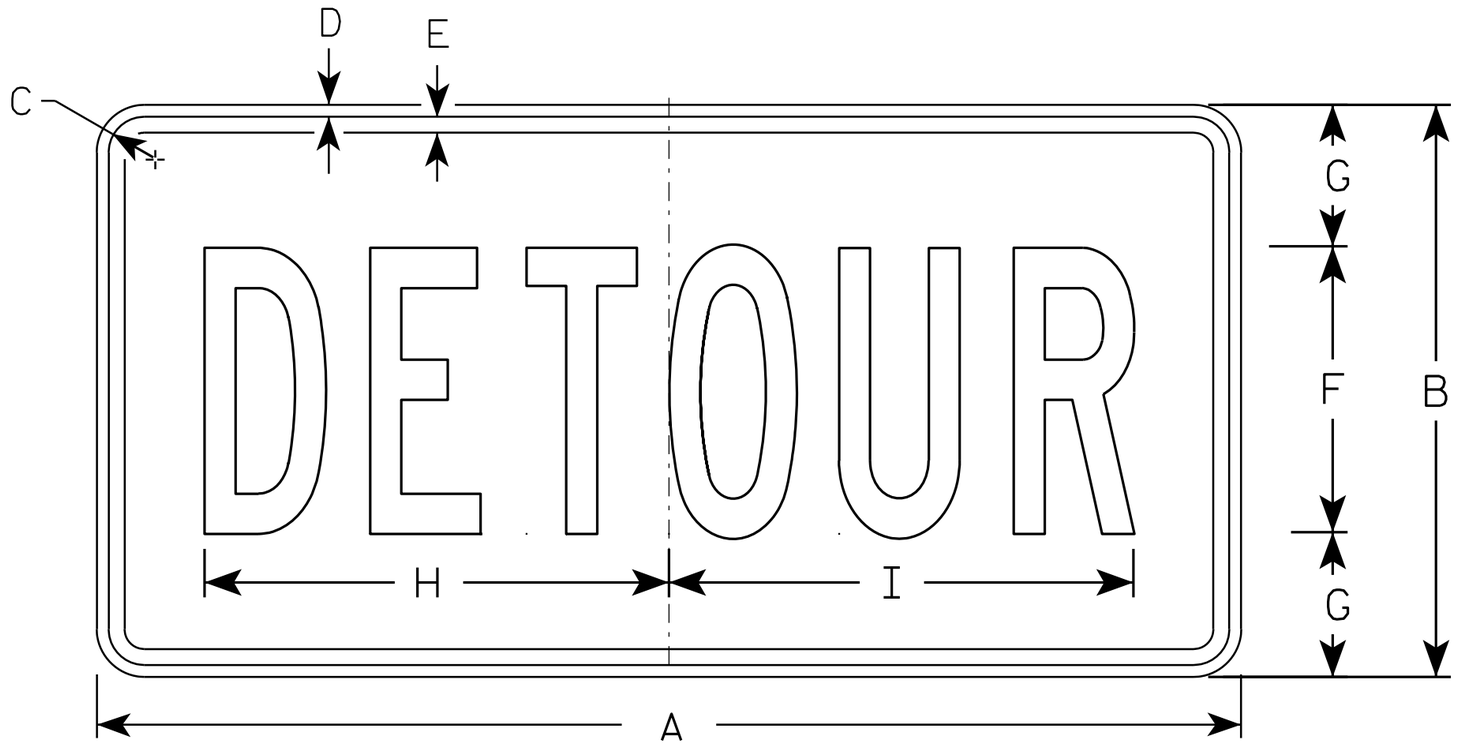
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew P Rauch for State Traffic Engineer

DATE 10/15/15 PLATE NO M3-1.14

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\M31.DGN PLOT DATE : 01-DEC-2015 17:54 PLOT BY : $$ plotuser $$ PLOT NAME : PLOT SCALE : 11.675051:1.000000



M4-8

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	3	10	10 1/4																		2.0
3	36	18	1 1/8	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
4																											
5																											

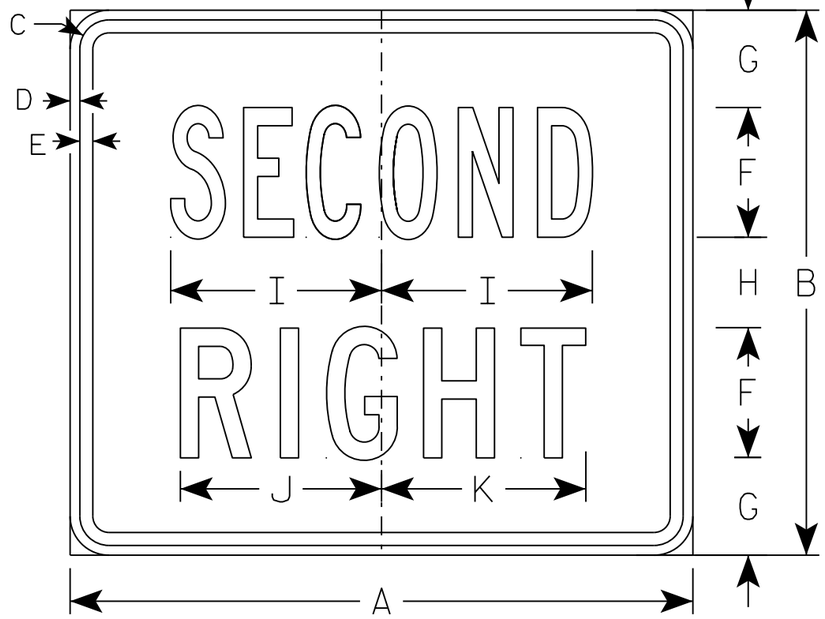
STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

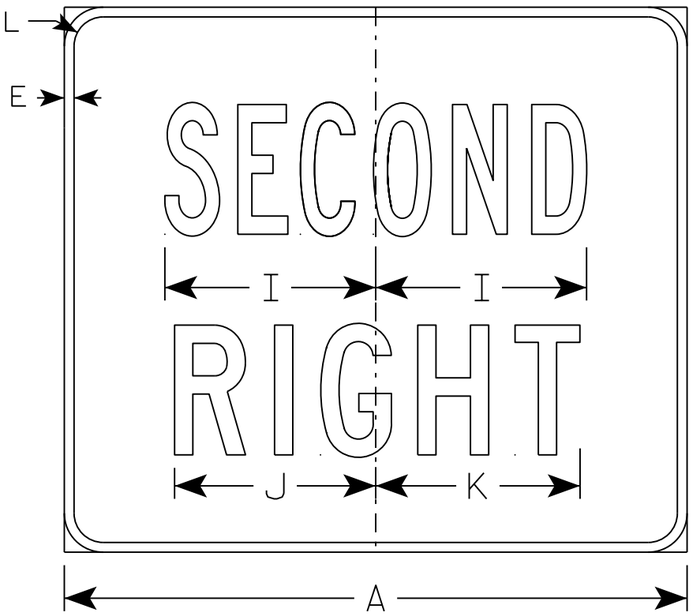
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/10/10 PLATE NO. M4-8.2

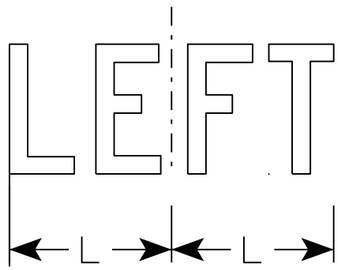
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 87	E



M4-22R
MM4-22R
M04-22R
MP4-22R



MB4-22R
MK4-22R
MN4-22R
MR4-22R



M4-22L
MB4-22L
MK4-22L
MM4-22L
MN4-22L
M04-22L
MP4-22L
MR4-22L

NOTES

1. Sign is Type II - Type H except as Shown
2. Color:
   Background - See note 5
   Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-22 Background - White
   Message - Black
   MB4-22 Background - Blue
   Message - White
   MK4-22 Background - Green
   Message - White
   MM4-22 Background - White
   Message - Green
   MN4-22 Background - Brown
   Message - White
   M04-22 Background - Orange - Type F Reflective
   Message - Black
   MP4-22 Background - White
   Message - Blue
   MR4-22 Background - Brown
   Message - Yellow

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	21	1 1/8	3/8	1/2	5	3 3/4	3 1/2	8 1/8	7 3/4	7 1/8	1 1/2															3.5
3	36	30	1 5/8	5/8	3/4	8	4 3/4	4 1/2	13 1/8	12 5/8	12 1/2	1 7/8															7.5
4																											
5																											

STANDARD SIGN
M4-22

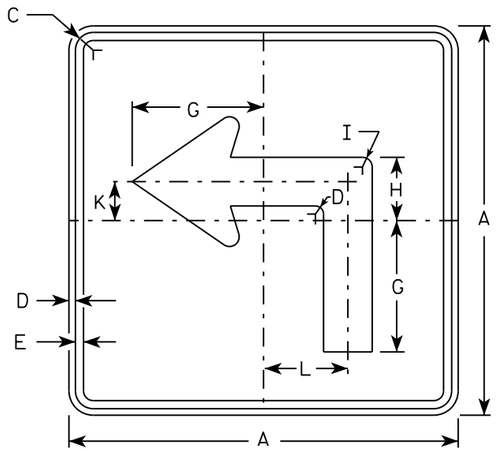
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

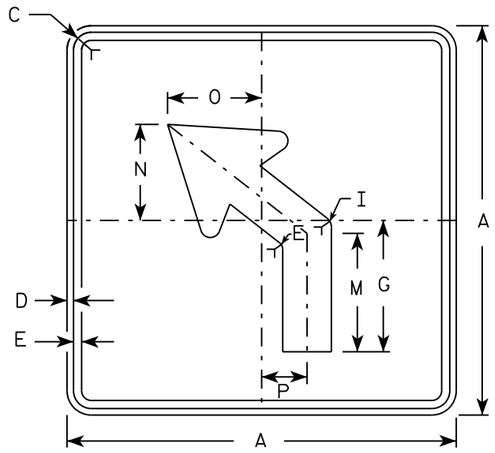
DATE 10/15/15 PLATE NO. M4-22.4

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

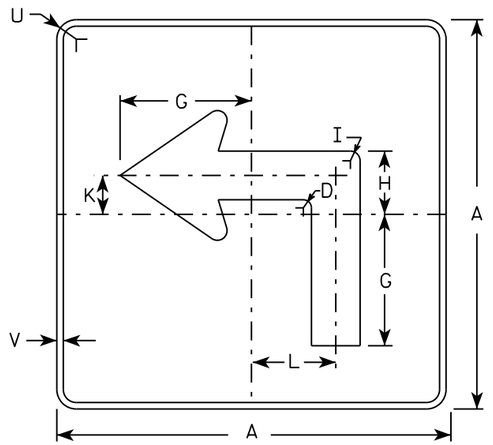
FILE NAME : C:\CAEfiles\Projects\tr_stdplate\M422.DGN PLOT DATE : 01-DEC-2015 18:04 PLOT BY : $$ plotuser $$ PLOT NAME : PLOT SCALE : 5.837526:1.000000



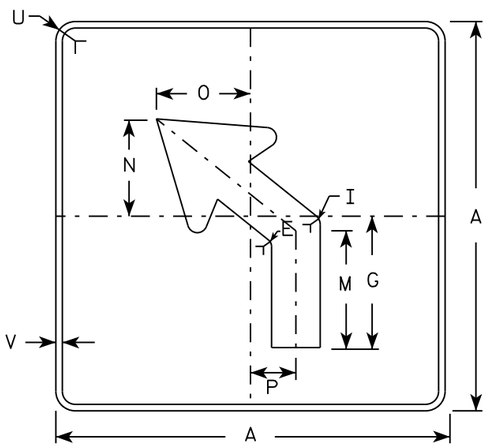
M5-1L
MM5-1L
MO5-1L
MP5-1L



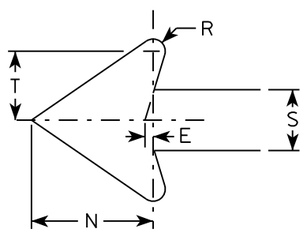
M5-2L
MM5-2L
MO5-2L
MP5-2L



MB5-1L
MK5-1L
MN5-1L
MR5-1L



MB5-2L
MK5-2L
MN5-2L
MR5-2L



NOTES

1. Signs are Type II - Type H reflective except as shown
2. Color:
   Background - See note 4
   Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M5-1 and M5-2 Background - White
   Message - Black
   MB5-1 and MB5-2 Background - Blue
   Message - White
   MK5-1 and MK5-2 Background - Green
   Message - White
   MM5-1 and MM5-2 Background - White
   Message - Green
   MN5-1 and MN5-2 Background - Brown
   Message - White
   MO5-1 and MO5-2 Background - Orange - Type F Reflective
   Message - Black
   MP5-1 and MP5-2 Background - White - Type H Reflective
   Message - Blue
   MR5-1 and MR5-2 Background - Brown
   Message - Yellow
5. M5-1R same as M5-1L except arrow points right.
6. M5-2R same as M5-2L except arrow tilts right.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25

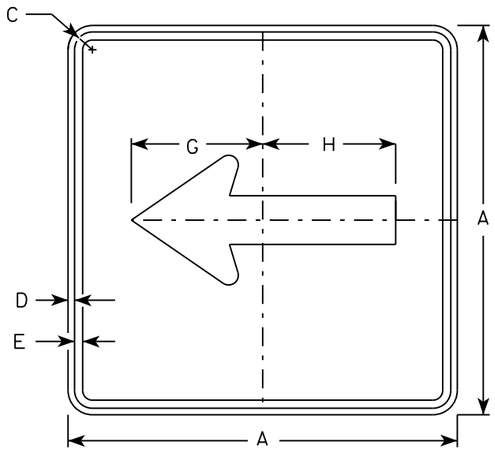
STANDARD SIGN
M5-1 & M5-2

WISCONSIN DEPT OF TRANSPORTATION

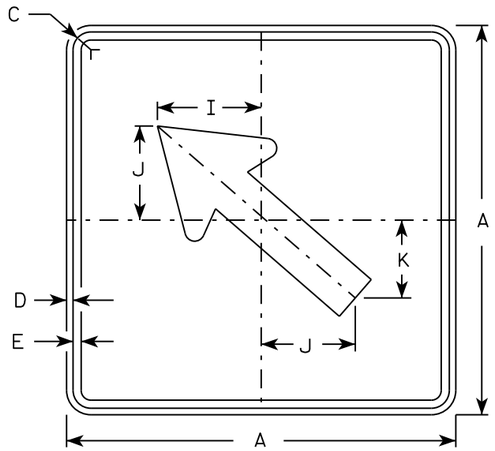
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M5-1.13

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E



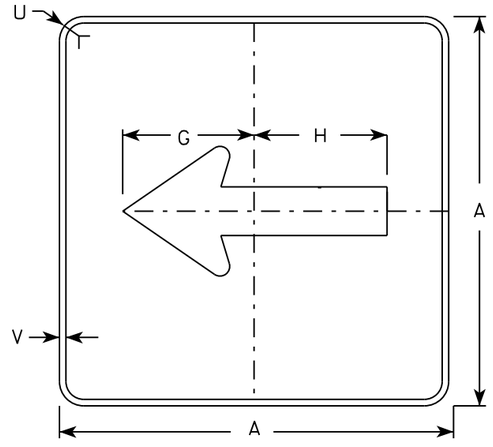
M6-1
MM6-1
MO6-1
MP6-1



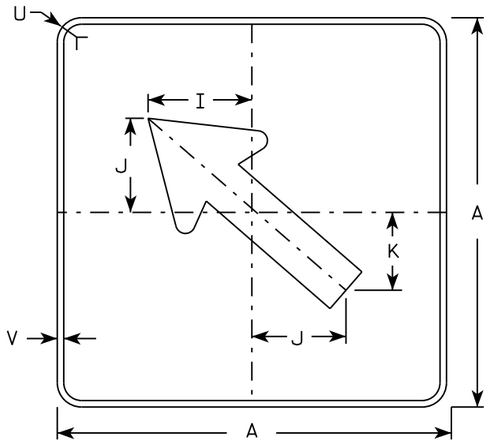
M6-2
MM6-2
MO6-2
MP6-2

NOTES

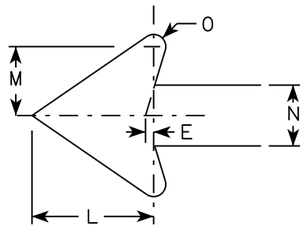
1. Signs are Type II - Type H except as Shown
2. Color:
   Background - See note 4
   Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M6-1 and M6-2 Background - White
   Message - Black
   MB6-1 and MB6-2 Background - Blue
   Message - White
   MK6-1 and MK6-2 Background - Green
   Message - White
   MM6-1 and MM6-2 Background - White
   Message - Green
   MN6-1 and MN6-2 Background - Brown
   Message - White
   MO6-1 and MO6-2 Background - Orange - Type F Reflective
   Message - Black
   MP6-1 and MP6-2 Background - White
   Message - Blue
   MR6-1 and MR6-2 Background - Brown
   Message - Yellow



MB6-1
MK6-1
MN6-1
MR6-1



MB6-2
MK6-2
MN6-2
MR6-2



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2						3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25

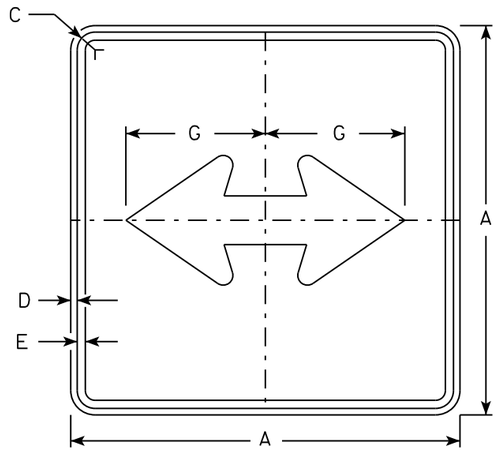
STANDARD SIGN
M6-1 & M6-2
SERIES

WISCONSIN DEPT OF TRANSPORTATION

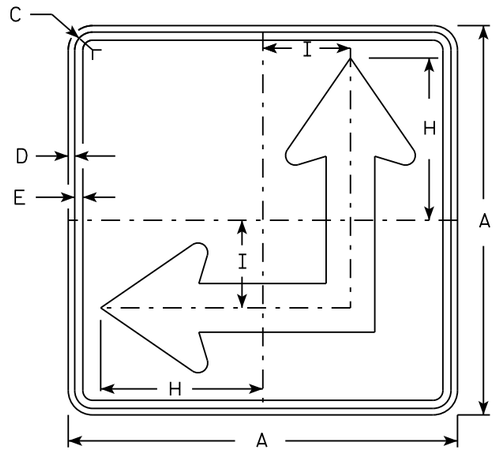
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/15/15 PLATE 90 M6-1.15

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



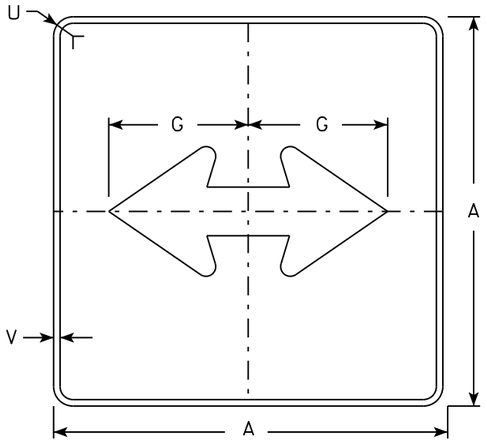
M6-4
MM6-4
MO6-4
MP6-4



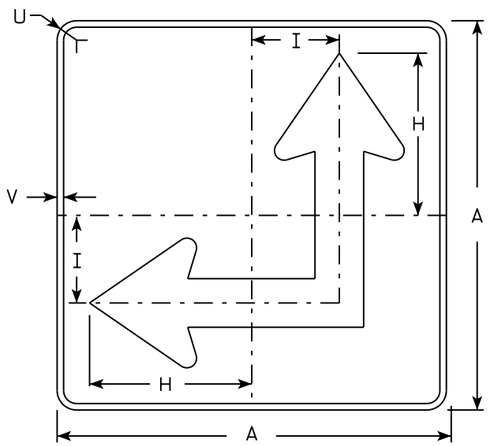
M6-6
MM6-6
MO6-6
MP6-6

NOTES

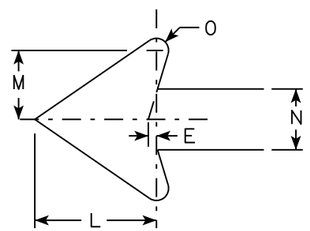
1. Signs are Type II - Type H except as Shown
2. Color:
   Background - See Note 4
   Message - See Note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M6-4 and M6-6 Background - White
   Message - Black
   MB6-4 and MB6-6 Background - Blue
   Message - White
   MK6-4 and MK6-6 Background - Green
   Message - White
   MM6-4 and MM6-6 Background - White
   Message - Green
   MN6-4 and MN6-6 Background - Brown
   Message - White
   MO6-4 and MO6-6 Background - Orange - Type F Reflective
   Message - Black
   MP6-4 and MP6-6 Background - White
   Message - Blue
   MR6-4 and MR6-6 Background - Brown
   Message - Yellow
5. M6-6R same as M6-6L except arrow points ahead and right.



MB6-4
MK6-4
MN6-4
MR6-4



MB6-6
MK6-6
MN6-6
MR6-6



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	8 3/4	4 1/4			5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

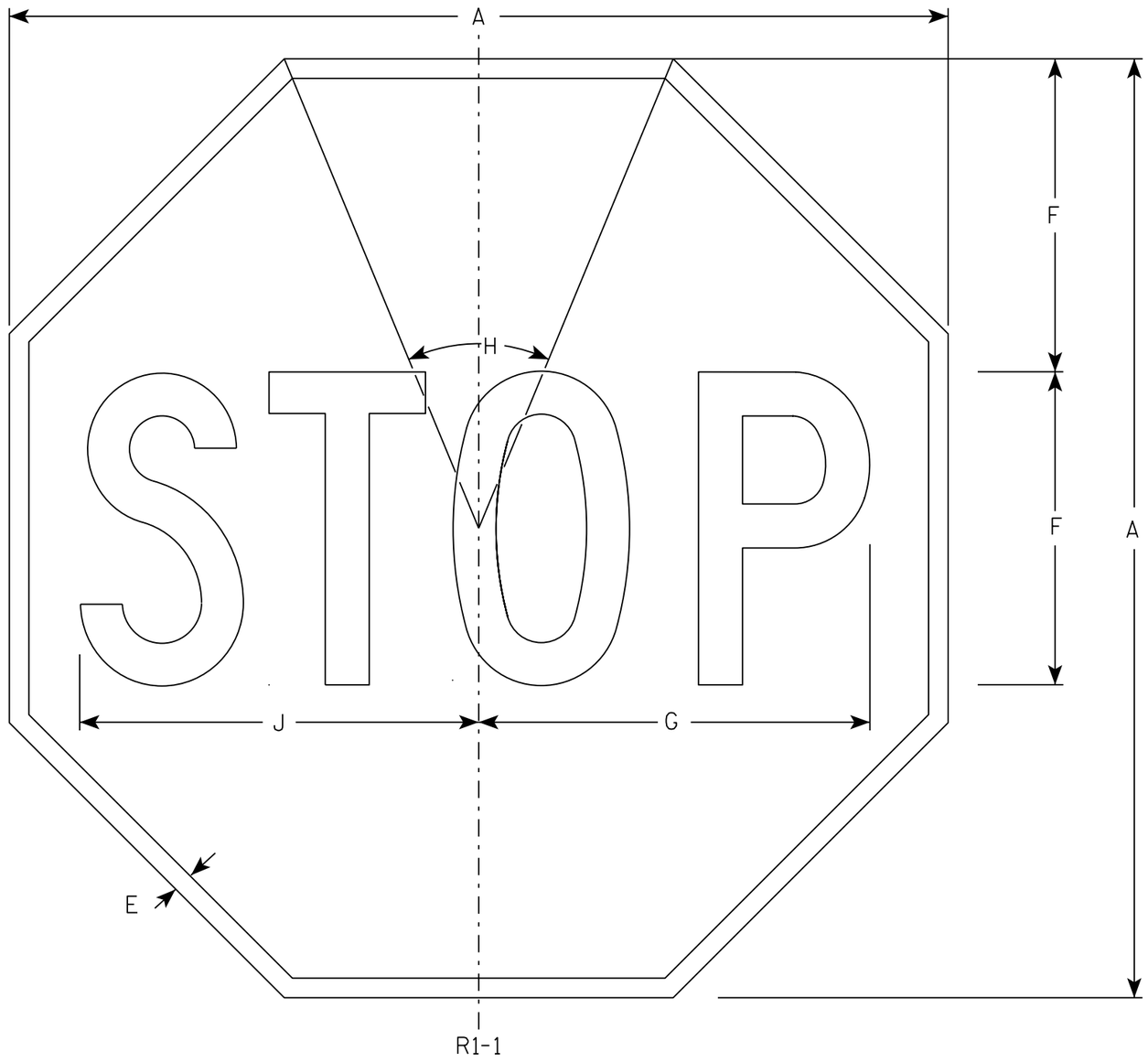
STANDARD SIGN
M6-4 & M6-6
SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/15/15 PLATE 91 M6-4.10

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Red
   Message - White
3. Message Series - C

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

STANDARD SIGN
R1-1

WISCONSIN DEPT OF TRANSPORTATION

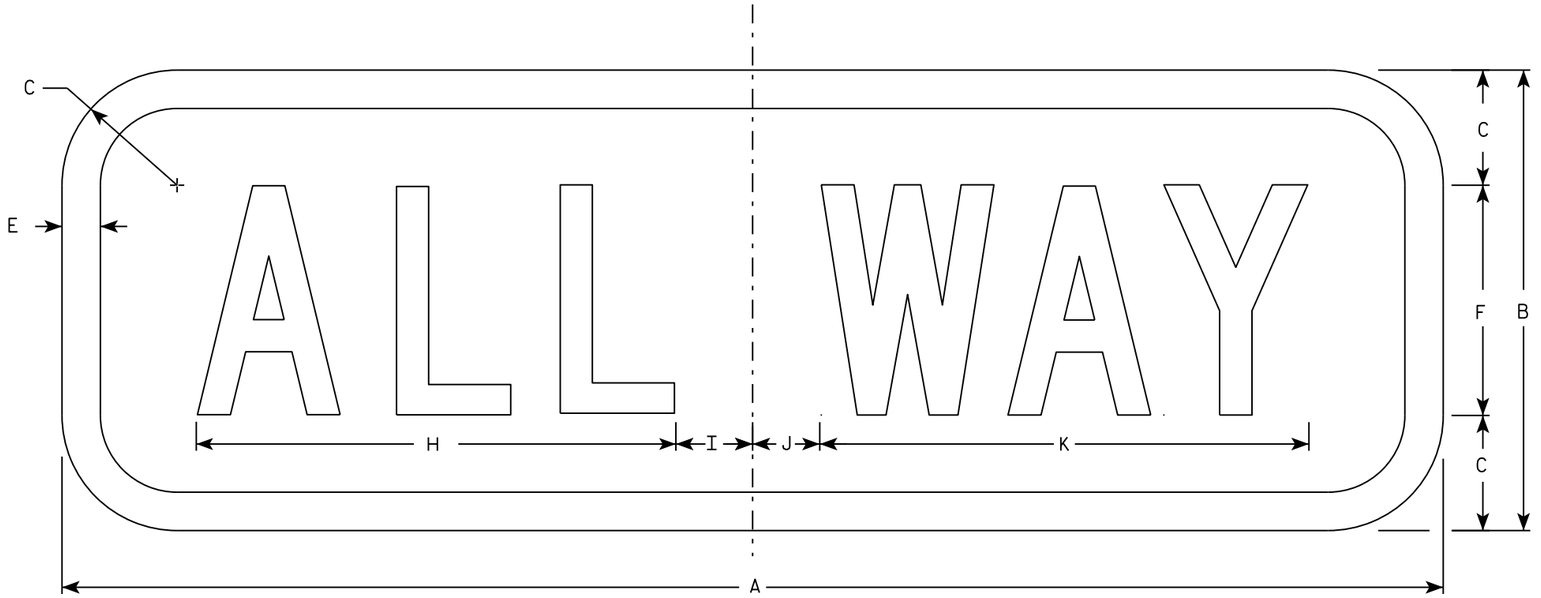
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/12/15 PLATE NO. R1-1.13

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 92	E

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - Red
   Message - White
3. Message Series - C
4. For 30"x30" R1-1 use 18"x6" R1-3P sign
   For 36"x36" R1-1 use 24"x9" R1-3P sign
   For 48"x48" R1-1 use 30"x12" R1-3P sign



R1-3P

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	6	1 1/2		1/2	3		6 1/4	1 1/4	7/8	6 3/8																0.75
2S	18	6	1 1/2		1/2	3		6 1/4	1 1/4	7/8	6 3/8																1.5
2M	24	9	1 1/2		1/2	5		9 1/4	1 1/4	3/4	9 3/4																1.5
3	24	9	1 1/2		1/2	5		9 1/4	1 1/4	3/4	9 3/4																1.5
4	30	12	2 1/4		5/8	6		11	2 1/4	1 1/2	11 3/4																2.5
5	30	12	2 1/4		5/8	6		11	2 1/4	1 1/2	11 3/4																2.5

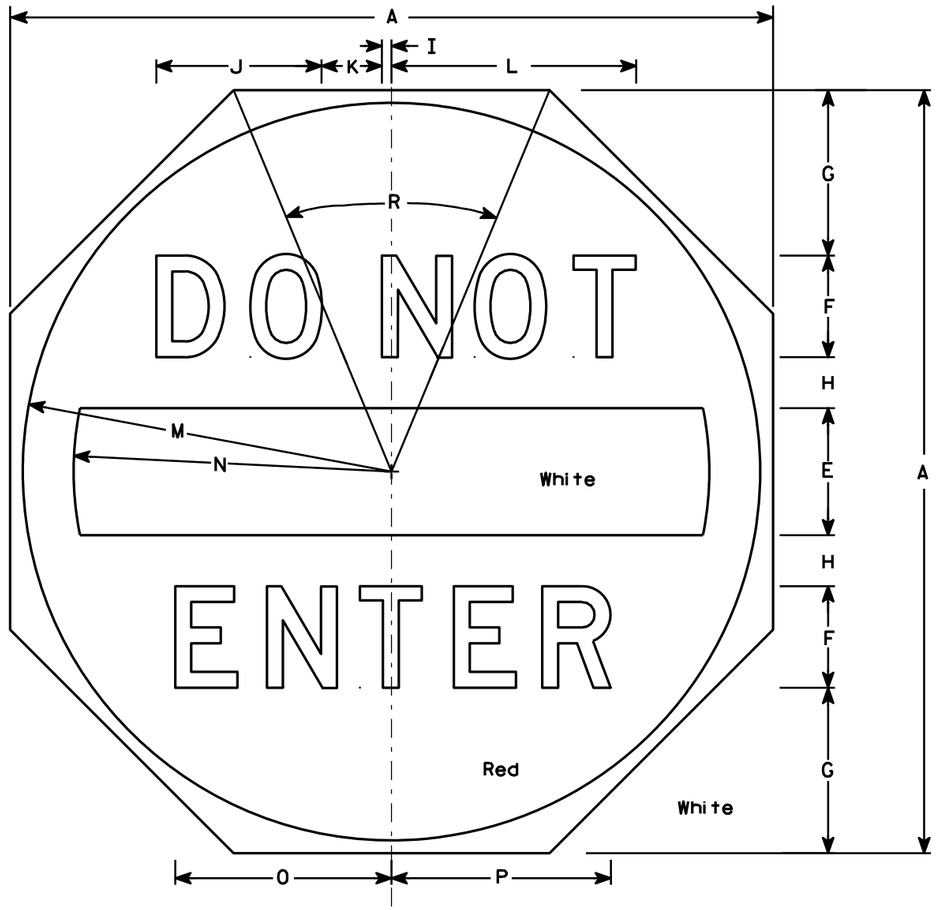
STANDARD SIGN
R1-3P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/29/16 PLATE NO. R1-3P.3

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 93 | E



R5-1C

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - See detail
   Message - White - Type H Reflective
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but when base material is metal, the cornors shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30		1 7/8		5	4	6 1/2	2	3/8	6 1/2	2 3/8	9 5/8	14 1/2	12 1/2	8 1/2	8 5/8		45°									5.18
2M	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4		45°									7.46
3	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4		45°									7.46
4	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4		45°									13.25
5	48		3		8	6	11	3	5/8	9 3/4	3 5/8	14 1/2	23 1/2	20	12 3/4	12 7/8		45°									13.25

STANDARD SIGN
R5-1C

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

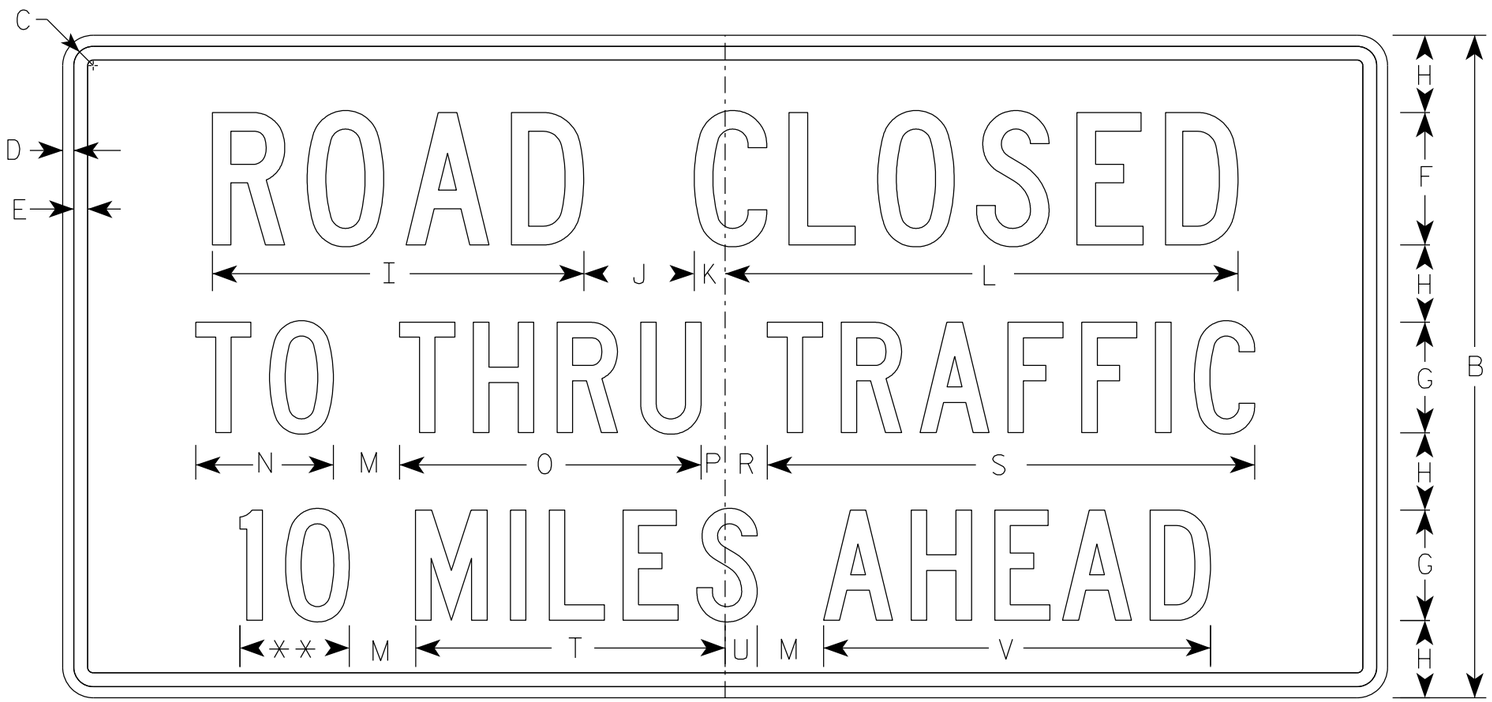
State Traffic Engineer

DATE 3/23/11 PLATE NO. R5-1C.1

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 94 | E

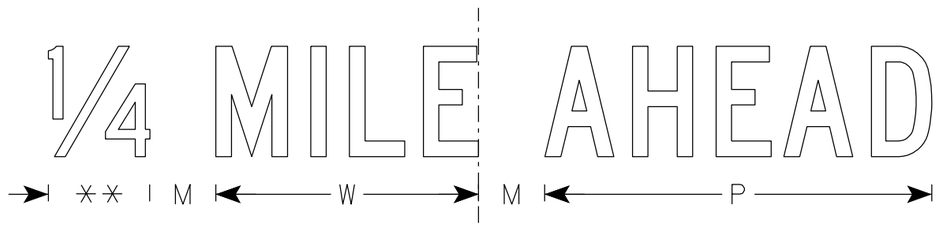
NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals to nearest quarter mile and optically adjust spacing to achieve proper balance.



R11-3

** See Note 5



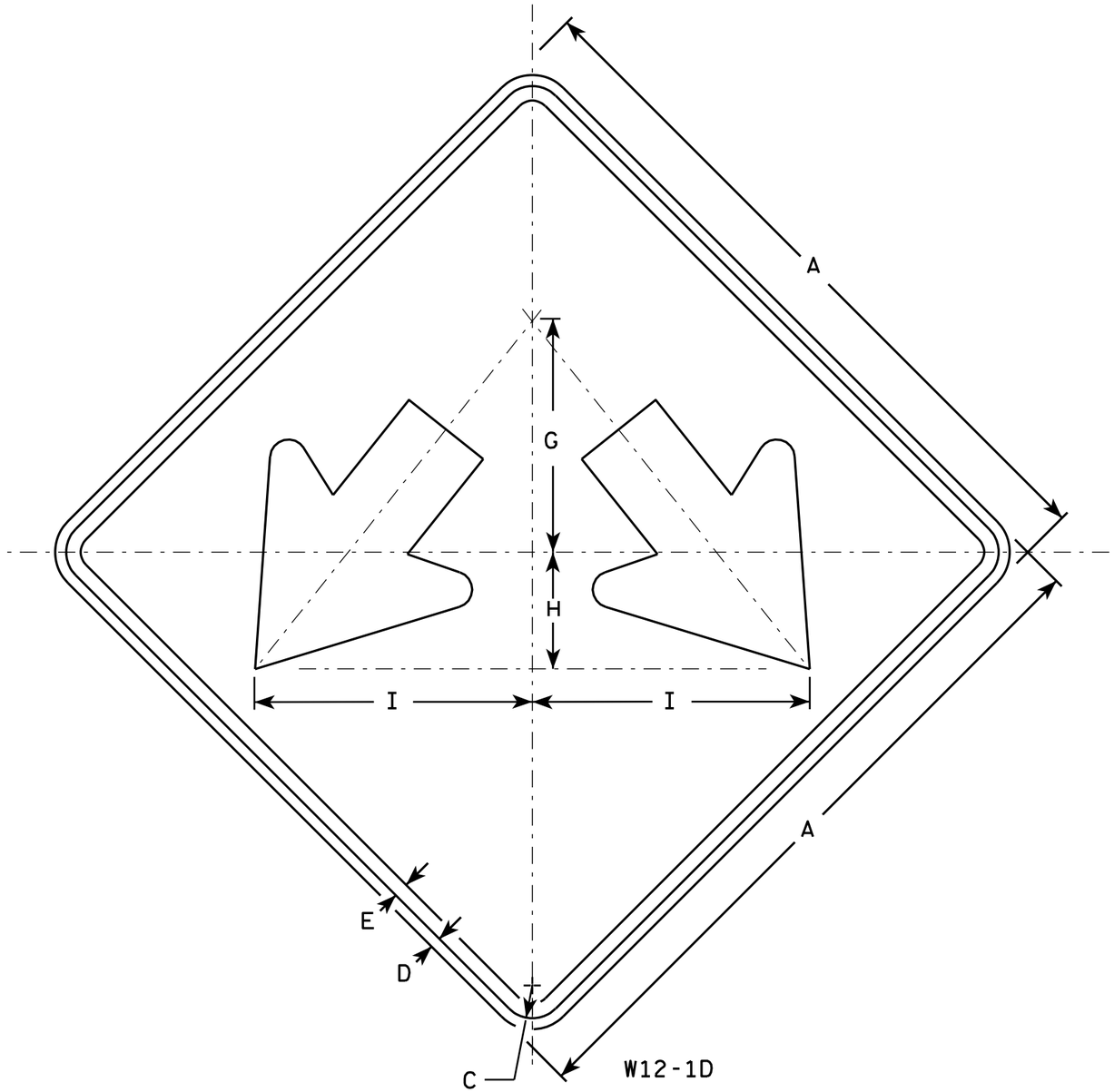
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/4	3/8	3/8	4	3	2	11 1/4	3	1 1/8	15 3/8	2	3 3/4	8 1/4	5/8		1 3/8	13 1/4	8 3/8	7/8	10 1/2	7 1/8				4.5
2S	60	30	1 3/8	1/2	5/8	6	5	3 1/2	16 7/8	5	1 3/8	23 1/4	3	6 1/4	13 5/8	1 1/8		1 7/8	22 1/8	14	1 1/2	17 1/2	11 7/8				12.5
2M	60	30	1 3/8	1/2	5/8	6	5	3 1/2	16 7/8	5	1 3/8	23 1/4	3	6 1/4	13 5/8	1 1/8		1 7/8	22 1/8	14	1 1/2	17 1/2	11 7/8				12.5
3																											
4																											
5																											

STANDARD SIGN
R11-3

WISCONSIN DEPT OF TRANSPORTATION

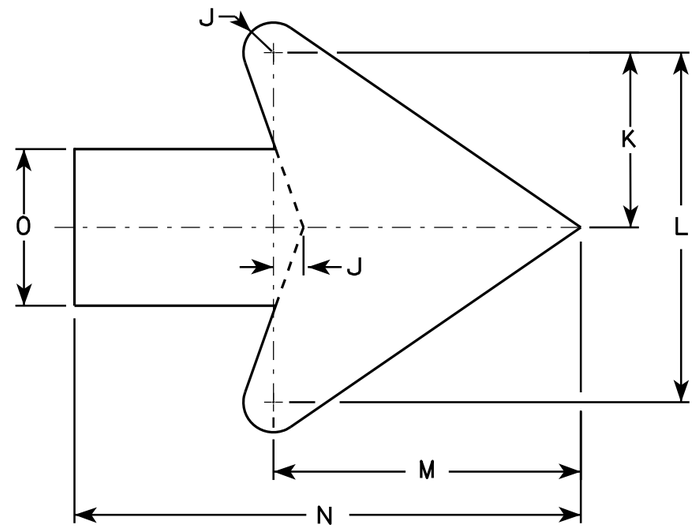
APPROVED Matthew R Rauch
for State Traffic Engineer
DATE 6/14/2021 PLATE NO. R11-3.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 95	E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24		1 1/8	1/2	3/8		8	4	9 1/2	3/8	3 3/8	7 1/4	6 3/8	10 3/8	3 1/4												4.0
2M	24		1 1/8	1/2	3/8		8	4	9 1/2	3/8	3 3/8	7 1/4	6 3/8	10 3/8	3 1/4												4.0
3	30		1 3/8	1/2	5/8		10	5	11 7/8	3/4	4 1/2	9	7 7/8	13	4												6.25
4	36		1 3/8	1/2	5/8		12	6	14 1/4	1	5 1/2	10 7/8	9 5/8	15 3/4	4 3/4												9.0
5	48		2 1/4	3/4	1		16	8	19	1 1/4	7 1/4	14 1/2	12 3/4	21	6 1/4												16.0

STANDARD SIGN
W12-1D

WISCONSIN DEPT OF TRANSPORTATION

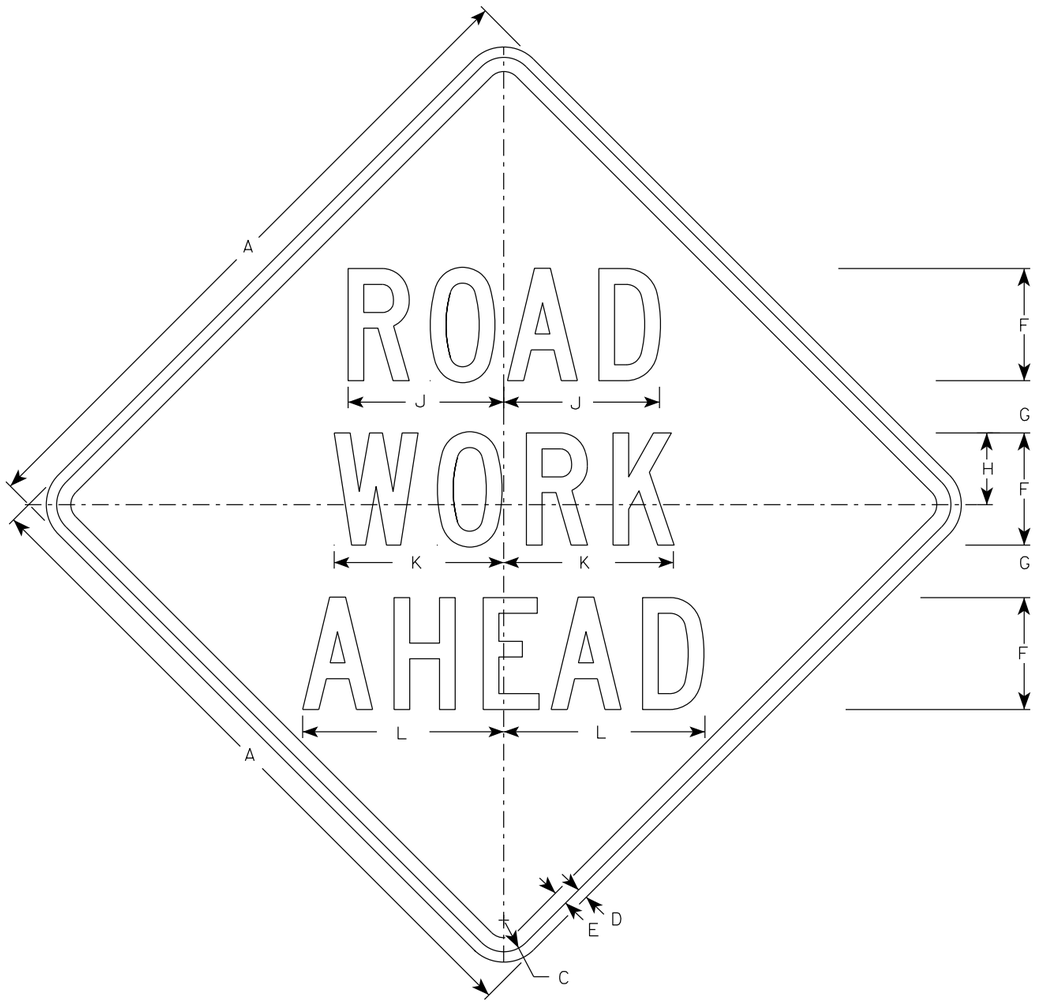
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/13/13 PLATE NO. W12-1D.15

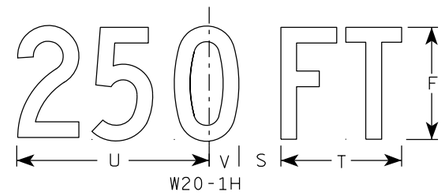
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 96	E

NOTES

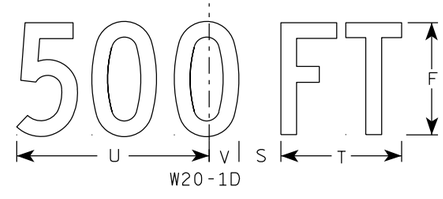
1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



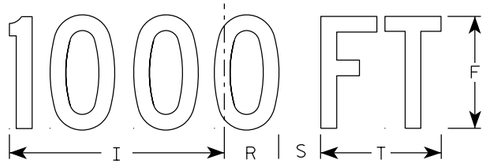
W20-1A



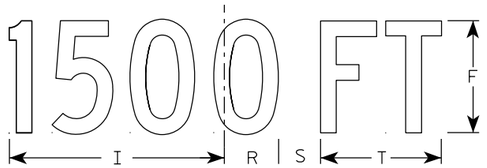
W20-1H



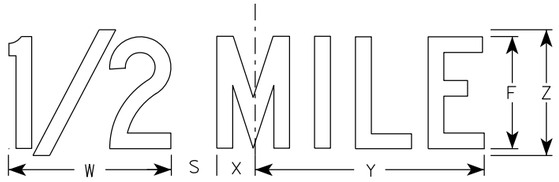
W20-1D



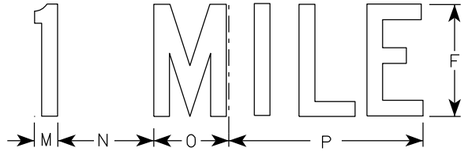
W20-1C



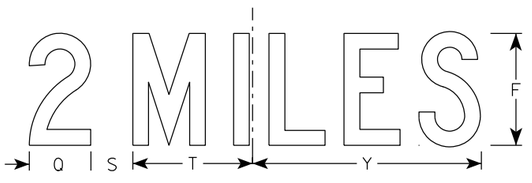
W20-1B



W20-1G



W20-1F



W20-1E

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9	3 1/4	2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN
W20-1A, B, C, D, E, F, G & H

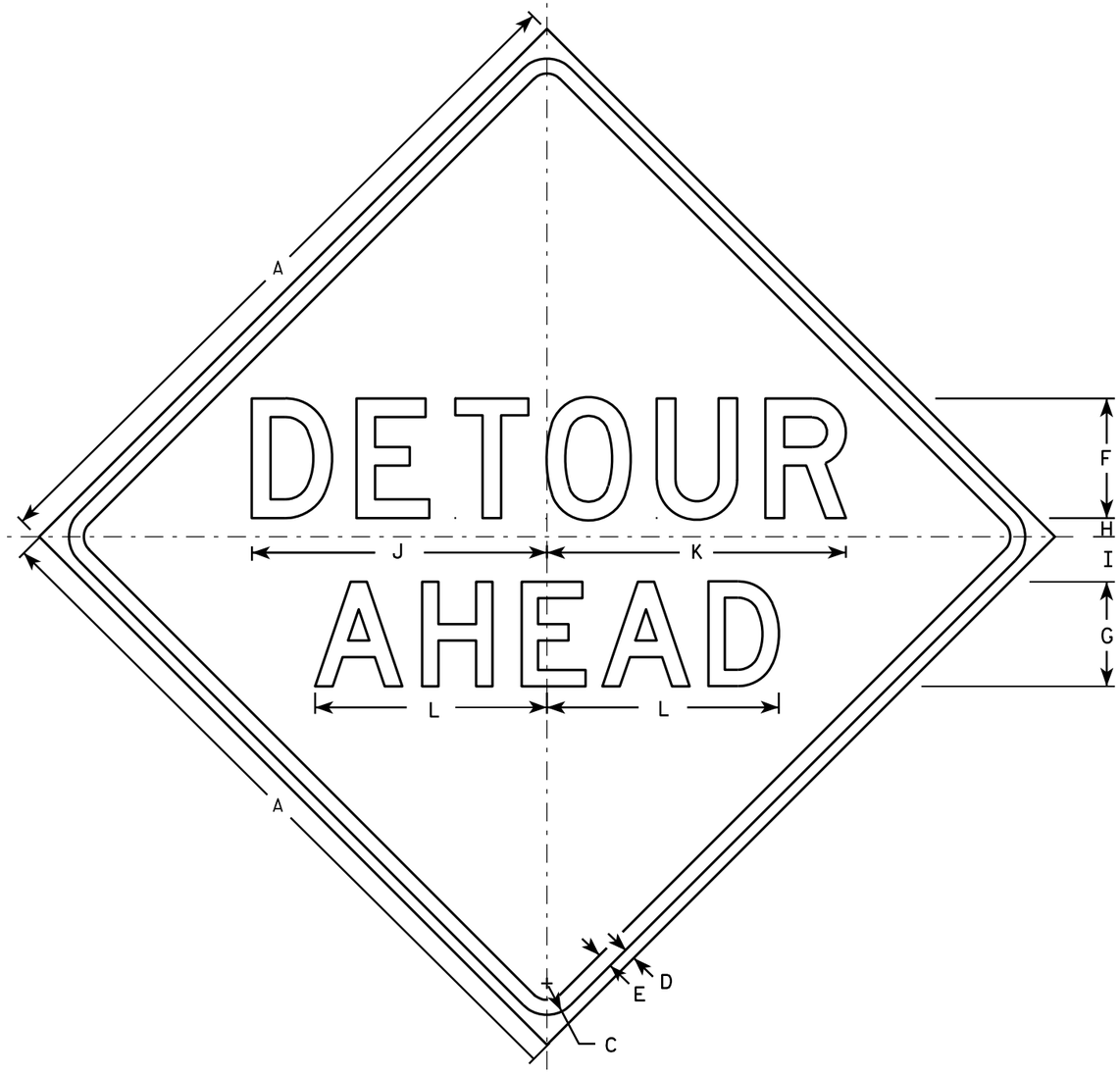
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

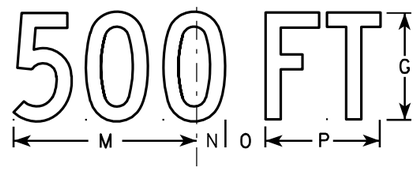
DATE 3/25/2020 PLATE NO. W20-1.11

PROJECT NO:

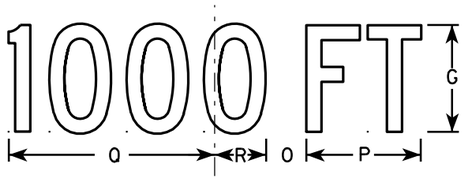
SHEET NO: 97 E



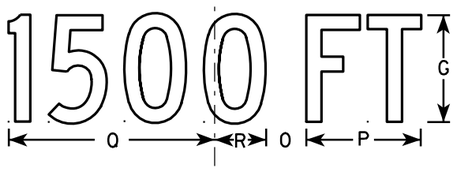
W20-2A



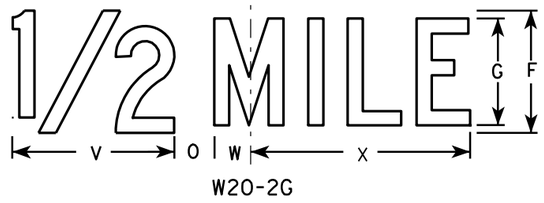
W20-2D



W20-2C



W20-2B



W20-2G

W20-2F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
   Line 2 is Series D for AHEAD and Series C for all other distances.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
2M	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
3	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
4	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
5	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0

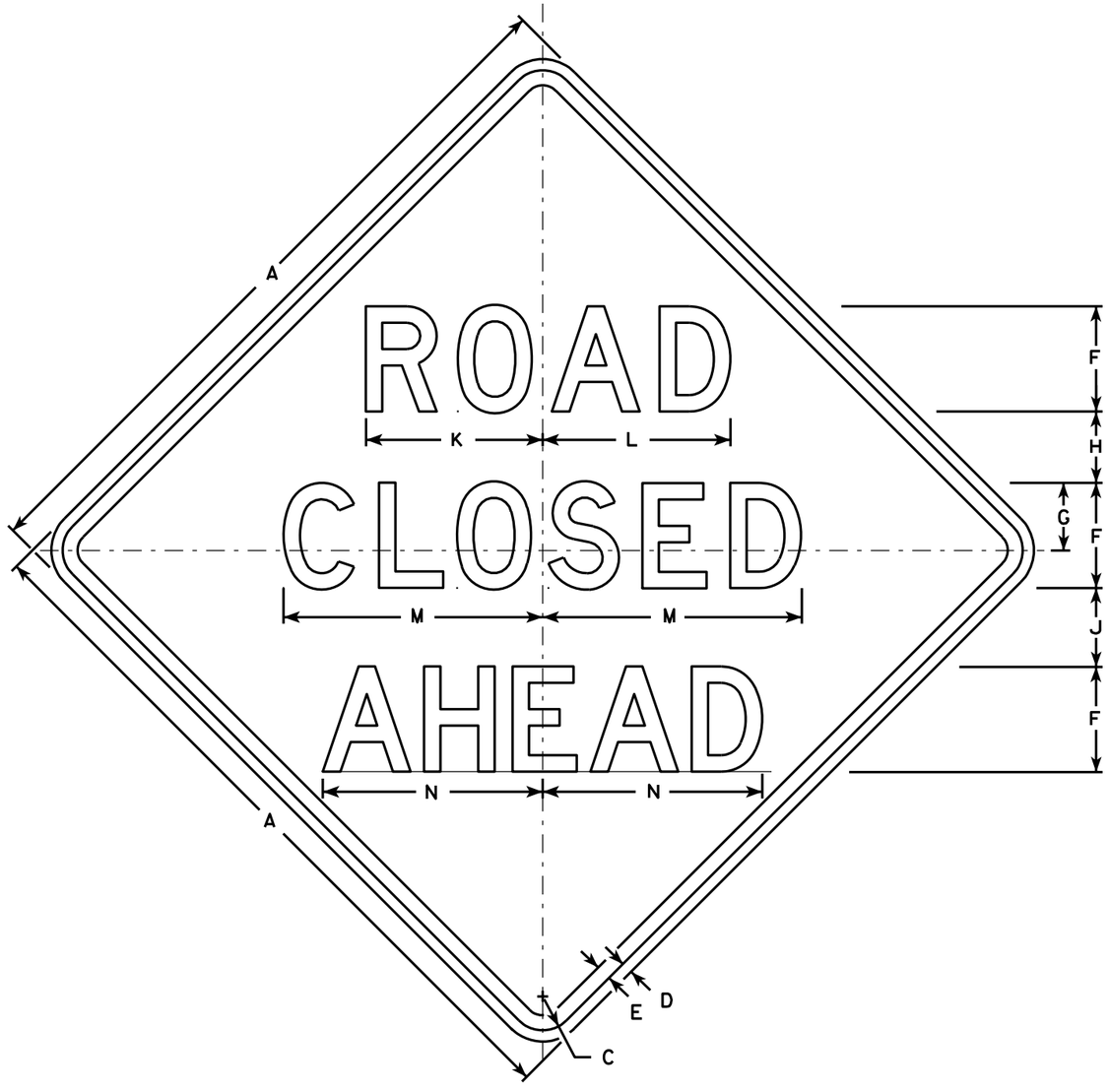
STANDARD SIGN
W20-2A,B,C,D,F & G

WISCONSIN DEPT OF TRANSPORTATION

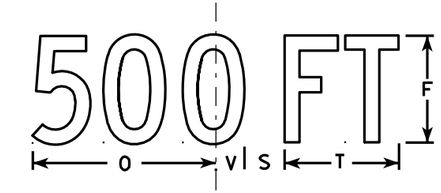
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-2.6

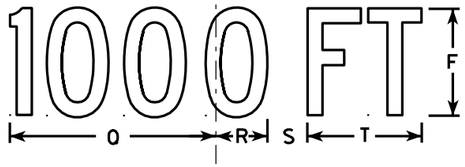
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 98	E



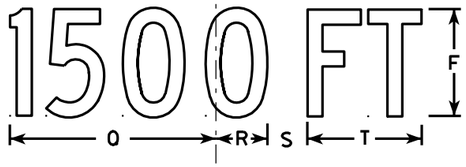
W20-3A



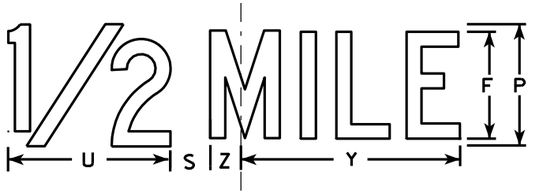
W20-3D



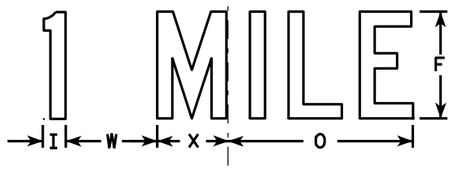
W20-3C



W20-3B



W20-3G



W20-3F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.
   Line 3 is Series D for AHEAD and Series C for all other distances.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	3 3/8	3 1/2	1 1/8	4	8 3/8	8 7/8	12 1/2	11	9	6	10 1/8	2 1/2	1 7/8	5 5/8	8	1 3/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
3	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
4	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
5	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-3A, B, C, D, F & G

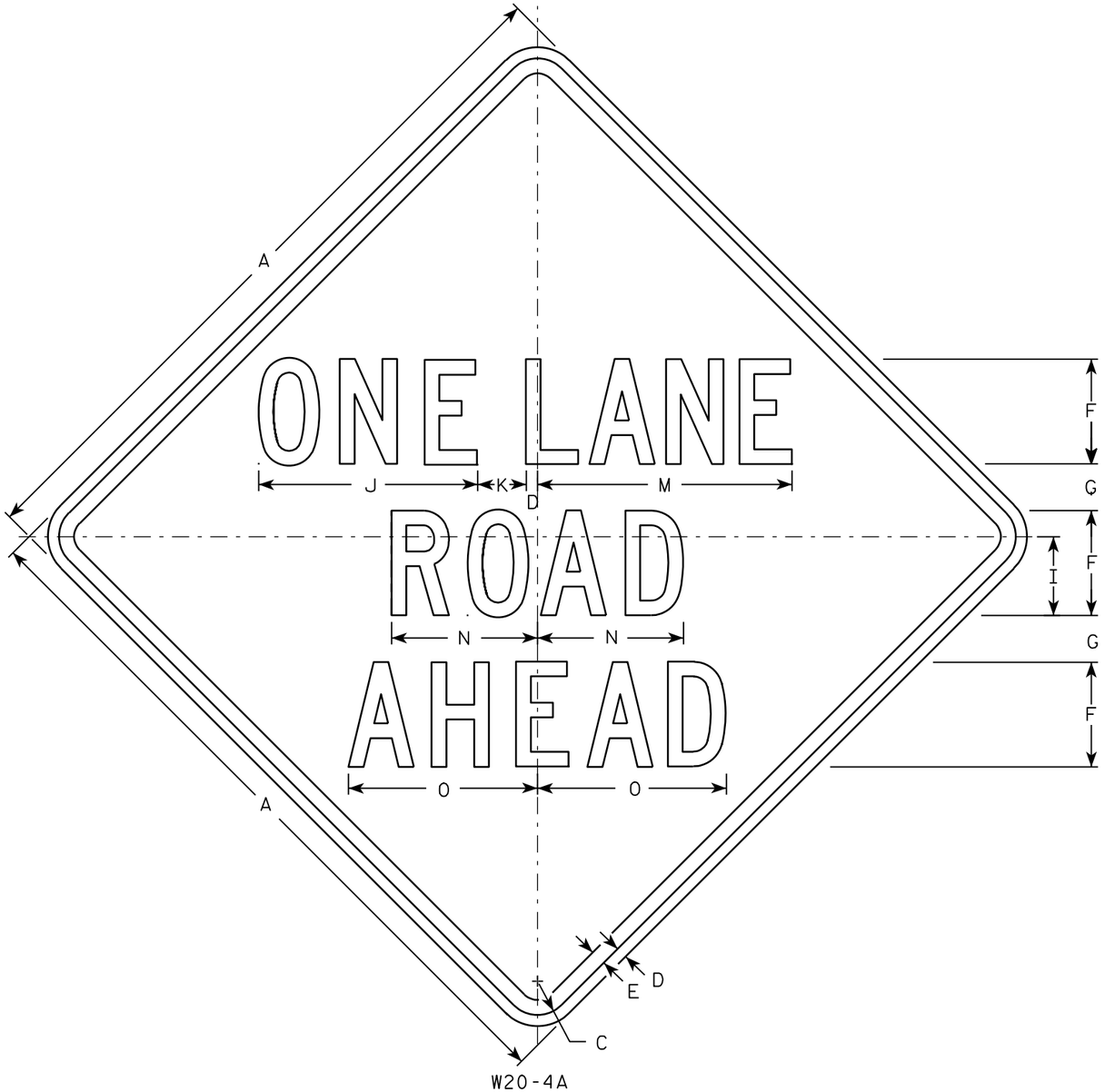
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer
DATE 3/18/11 PLATE NO. W20-3.7

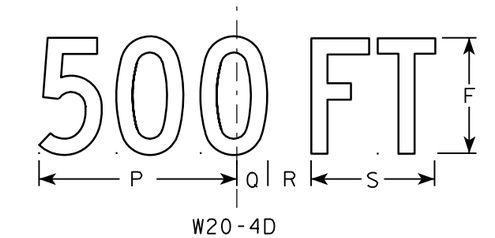
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 99 | E

7

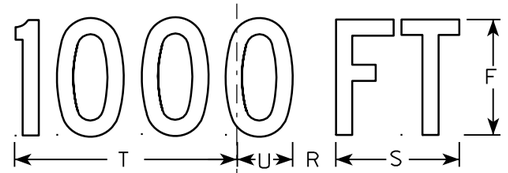
FILE NAME : C:\Users\PROJECTS\tr_stdplate\W203.DGN PLOT DATE : 18-MAR-2011 12:08 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 9.931739:1.000000
WISDOT/CADDS SHEET 42



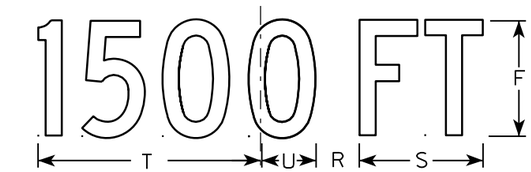
W20-4A



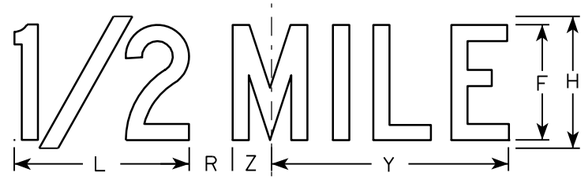
W20-4D



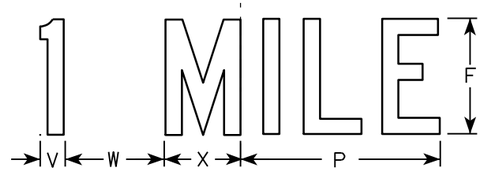
W20-4C



W20-4B



W20-4G



W20-4F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	2 3/8	6	3 3/4	10 3/8	2 3/8	8	13 1/2	7	8 7/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
3	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
4	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
5	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-4A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

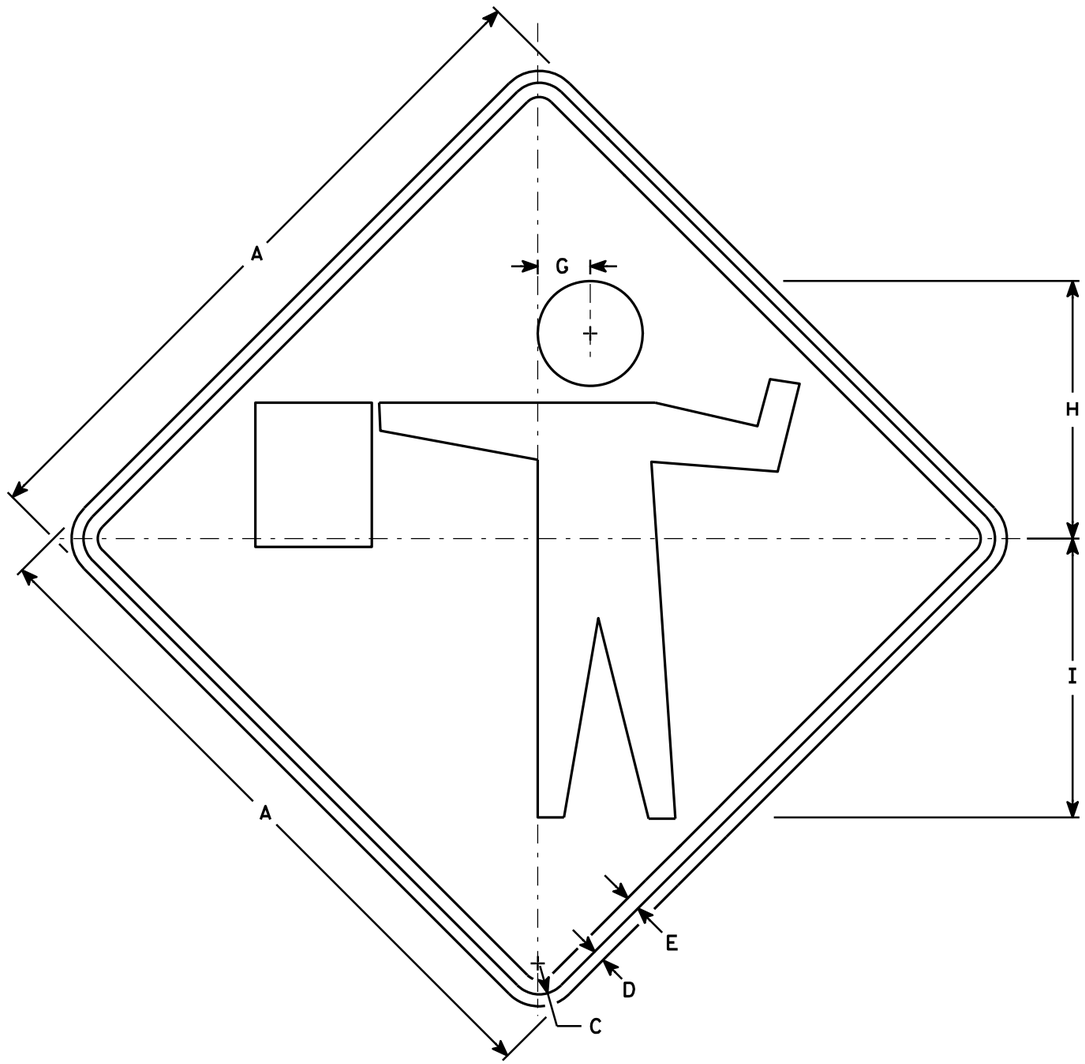
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-4.9

PROJECT NO:

SHEET NO: 100 E

7



W20-7A

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4		2 3/4	13 1/2	14 5/8																		9.00
2S	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00
2M	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00
3	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00
4	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00
5	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00

STANDARD SIGN
W20-7A

WISCONSIN DEPT OF TRANSPORTATION

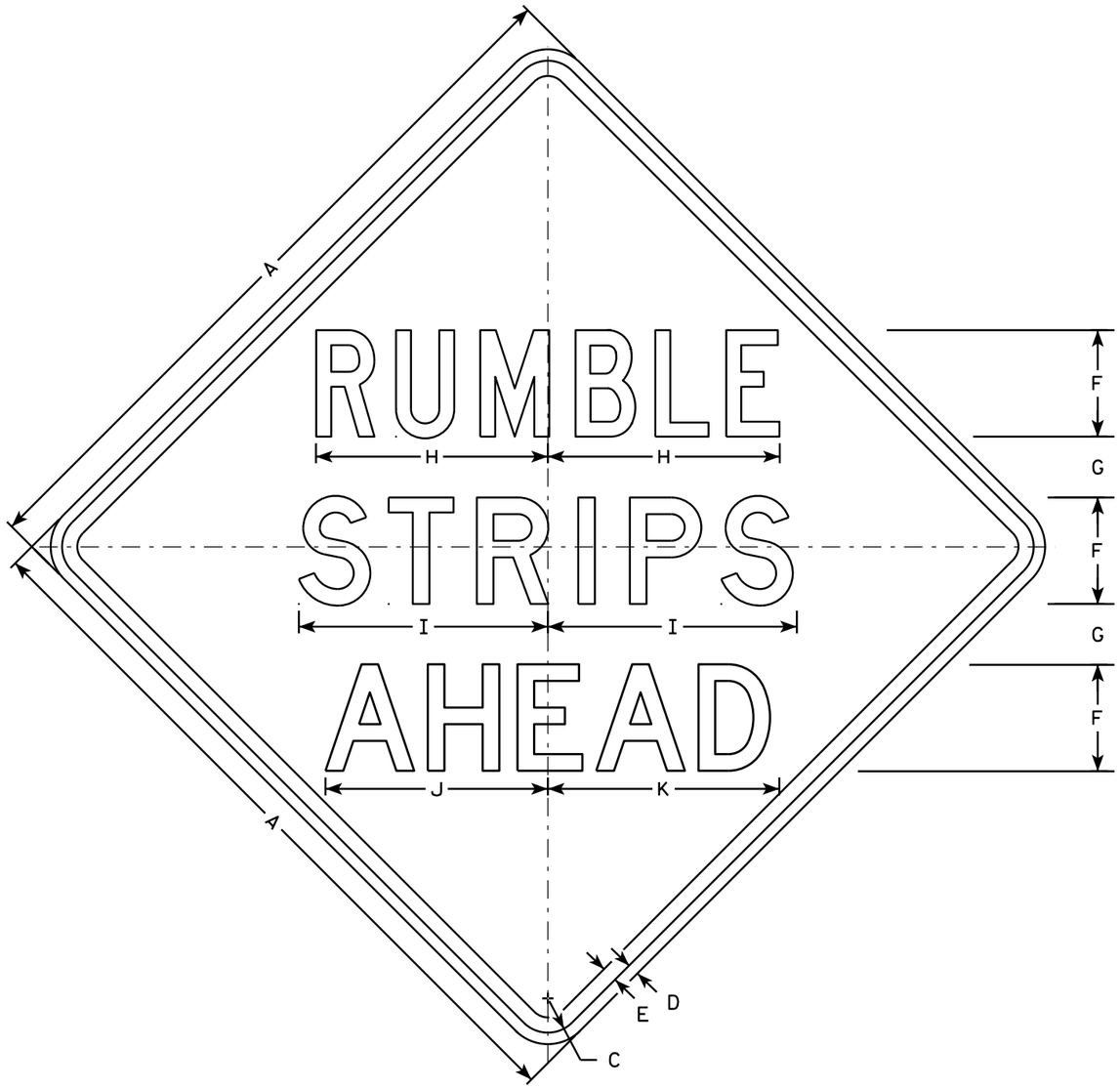
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-7A.5

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 101 | E

7

FILE NAME : C:\Users\PROJECTS\tr_stdplate\W207A.DGN PLOT DATE : 18-MAR-2011 13:14 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 7.945391:1.000000 WISDOT/CADDS SHEET 42



W21-65

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series C
   Lines 2 and 3 are Series D

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	3 1/4	10 7/8	11 5/8	11	11 5/8																9.0
2S	48		2 1/4	3/4	1	7	4	15 1/4	16 3/8	14 5/8	15 1/4																16.0
2M	48		2 1/4	3/4	1	7	4	15 1/4	16 3/8	14 5/8	15 1/4																16.0
3	48		2 1/4	3/4	1	7	4	15 1/4	16 3/8	14 5/8	15 1/4																16.0
4	48		2 1/4	3/4	1	7	4	15 1/4	16 3/8	14 5/8	15 1/4																16.0
5	48		2 1/4	3/4	1	7	4	15 1/4	16 3/8	14 5/8	15 1/4																16.0

STANDARD SIGN

W21-65

WISCONSIN DEPT OF TRANSPORTATION

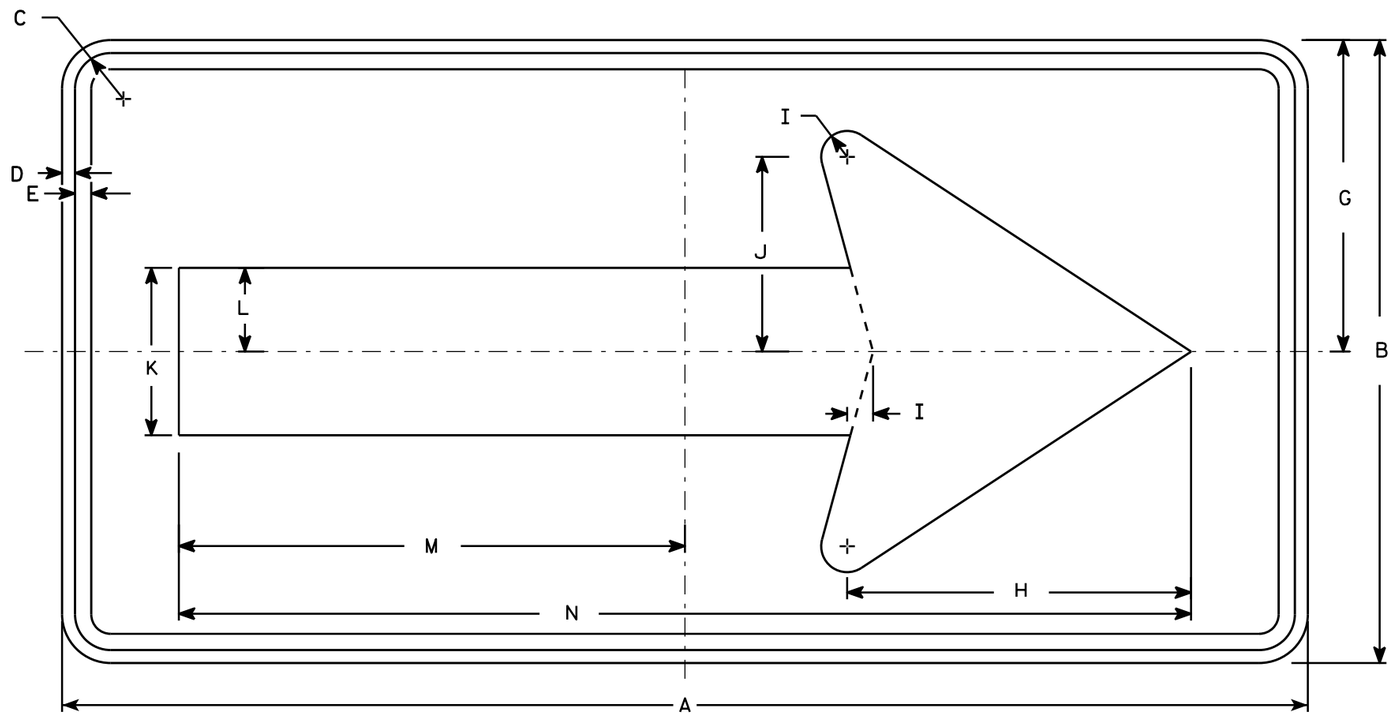
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/28/14 PLATE W21-65.1

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W2165.dgn PLOT DATE : 28-MAY-2014 13:24 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 9.729210:1.000000

WISDOT/CADDS SHEET 42



W01-6

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
5	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5

STANDARD SIGN
W01-6

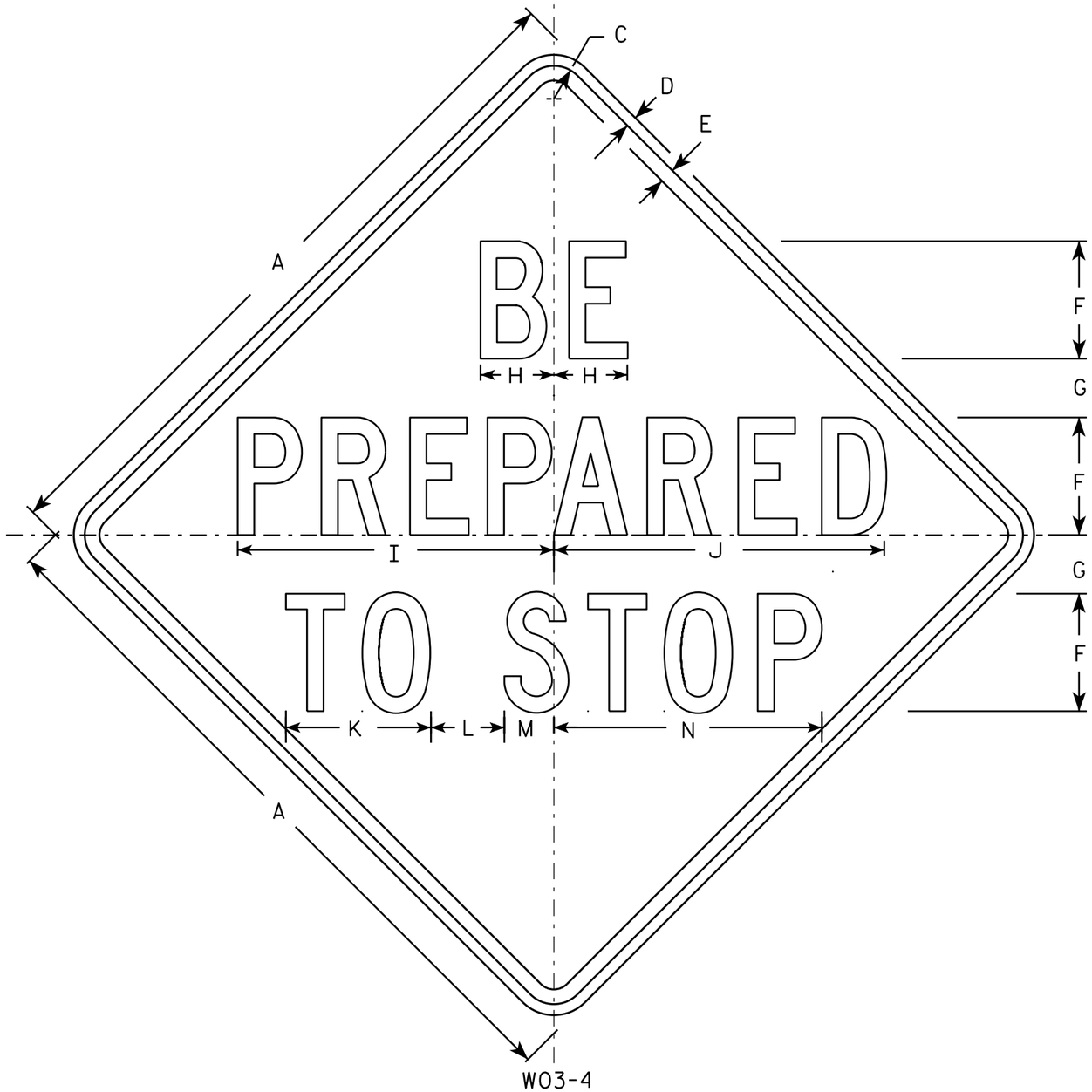
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/18/13 PLATE W01-6.1

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\W016.DGN PLOT DATE : 28-FEB-2014 11:37 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 5.837526:1.000000 WISDOT/CADDS SHEET 42



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 3/8	1/2	5/8	6	2 1/2	3 3/4	15 7/8	16 7/8	7 3/8	4	2 3/8	13 3/4													9.0
2S	48		2 1/4	3/4	1	8	4	5	21 1/2	22 1/2	9 7/8	5	3 3/8	18 1/4													16.0
2M	48		2 1/4	3/4	1	8	4	5	21 1/2	22 1/2	9 7/8	5	3 3/8	18 1/4													16.0
3	48		2 1/4	3/4	1	8	4	5	21 1/2	22 1/2	9 7/8	5	3 3/8	18 1/4													16.0
4	48		2 1/4	3/4	1	8	4	5	21 1/2	22 1/2	9 7/8	5	3 3/8	18 1/4													16.0
5	48		2 1/4	3/4	1	8	4	5	21 1/2	22 1/2	9 7/8	5	3 3/8	18 1/4													16.0

STANDARD SIGN
W03-4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/02/13 PLATE NO. W03-4.1

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W034.DGN PLOT DATE : 02-DEC-2013 14:02 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 8.999518:1.000000
WISDOT/CADDS SHEET 42

Notes

Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

http://www.dot.wisconsin.gov