

JANUARY 2022
ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 48



STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

SW REGION PAVEMENT MARKING

STN LOCATIONS PER ANNUAL PLAN

VAR HWY SW REGION WIDE

STATE PROJECT NUMBER
1009-12-96

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1009-12-96		

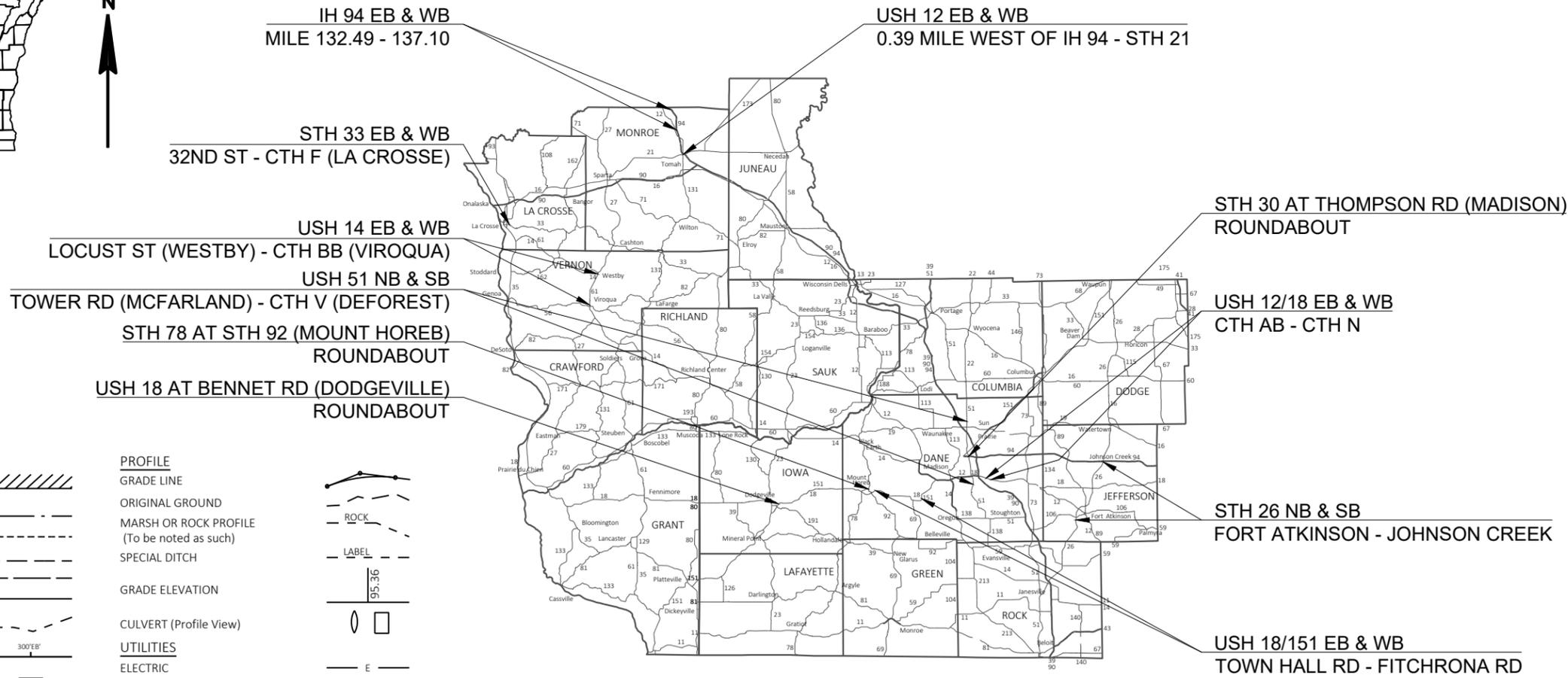
DESIGN DESIGNATION

A.A.D.T.	=
A.A.D.T.	=
D.H.V.	=
D.D.	=
T.	=
DESIGN SPEED	=
ESALS	=

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	



LAYOUT
SCALE 0
TOTAL NET LENGTH OF CENTERLINE =

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
PREPARED BY	
Surveyor	
Designer	IVER PETERSON
Project Manager	
Regional Examiner	
Regional Supervisor	RUCHI DUTTA
APPROVED FOR THE DEPARTMENT	
DATE: 10/21/2021	<i>Ruchi Dutta</i> (Signature)

GENERAL NOTES

ALL EXISTING PAVEMENT MARKINGS SHALL BE CLEAN OF DEBRIS, SALT AND DIRT PRIOR TO APPLICATION OF NEW MARKINGS

THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS. THE CONTRACTOR SHALL COORDINATE HIS CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.



DESIGN CONTACTS

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DNR SPILL HOTLINE (24-hour)
 Wisconsin Department of Natural Resources
 Toll-free: 1-800-943-0003

Estimate Of Quantities

1009-12-96

Line	Item	Item Description	Unit	Total	Qty
0002	619.1000	Mobilization	EACH	1.000	1.000
0004	646.1020	Marking Line Epoxy 4-Inch	LF	1,795,096.000	1,795,096.000
0006	646.3020	Marking Line Epoxy 8-Inch	LF	138,116.000	138,116.000
0008	646.5020	Marking Arrow Epoxy	EACH	309.000	309.000
0010	646.5120	Marking Word Epoxy	EACH	161.000	161.000
0012	646.6120	Marking Stop Line Epoxy 18-Inch	LF	5,159.000	5,159.000
0014	646.6320	Marking Dotted Extension Epoxy 18-Inch	LF	249.000	249.000
0016	646.7020	Marking Diagonal Epoxy 6-Inch	LF	58.000	58.000
0018	646.7120	Marking Diagonal Epoxy 12-Inch	LF	3,851.000	3,851.000
0020	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	5,893.000	5,893.000
0022	646.7520	Marking Crosswalk Epoxy Block Style 24-Inch	LF	372.000	372.000
0024	646.8120	Marking Curb Epoxy	LF	10.000	10.000
0026	646.8220	Marking Island Nose Epoxy	EACH	1.000	1.000
0028	SPV.0060	Special 01. Traffic Control	EACH	1.000	1.000
0030	SPV.0090	Special 01. Marking Crosswalk Block Epoxy 12-Inch	LF	971.000	971.000
0032	SPV.0090	Special 02. Marking Crosswalk Transverse Epoxy 12-Inch	LF	247.000	247.000

PAVEMENT MARKING EPOXY (INTERSTATE LOCATIONS)

MONROE COUNTY (JACKSON COUNTY LINE - WARRENS)

HIGHWAY	LOCATION (From)	LOCATION (To)	646.1020				646.3020			646.6120
			4-INCH EDGELINE WHITE L.F.	4-INCH EDGELINE YELLOW L.F.	4-INCH LANE LINE WHITE L.F.	4-INCH MINI DASHES WHITE L.F.	8-INCH CHANNELIZING (MAINLINE) WHITE L.F.	8-INCH CHANNELIZING (RAMP) WHITE L.F.	8-INCH CHANNELIZING (ISLAND GORE) WHITE L.F.	18-INCH STOP LINE WHITE L.F.
IH 94 EB	Mile 132.49 (Jackson Co - Monroe Co line)	Mile 137.10 (1.26 Miles East of Exit 135, CTH EW) (Warrens)	22720	24340	6085					
	Exit 135, CTH EW (Warrens)	Off Ramp to CTH EW	2265	1426		144	218	225	100	33
	Exit 135, CTH EW (Warrens)	On Ramp from CTH EW	2315	1198			681	676		
	Exit 143, USH 12 (Tomah)	On Ramp from USH 12	1848	728			662	662		
	Exit 143, STH 21 (Tomah)	On Ramp from STH 21	2324	948			636	634		
IH 94 WB	Exit 145, Industrial Avenue (Tomah)	Off Ramp to Industrial Avenue	1778	1255			371	367		42
	Exit 145, Industrial Avenue (Tomah)	On Ramp from Industrial Avenue	2220	721			1013	1021		
	Exit 143, STH 21 (Tomah)	Off Ramp to STH 21	1558	1164	75		290	291	300	51
IH 94 WB	Exit 143, STH 21 (Tomah)	On Ramp from STH 21	2414	1129			767	765		
	Mile 137.10 (1.26 Miles East of Exit 135, CTH EW) (Warrens)	Mile 132.49 (Jackson Co - Monroe Co line)	23305	24340	6085					
	Exit 135, CTH EW (Warrens)	Off Ramp to CTH EW	1970	997		303	242	250	90	38
	Exit 135, CTH EW (Warrens)	ON Ramp from CTH EW	2516	1278			583	585		
Sub Totals			71234	61526	12245	447	6611	6622	490	202

PAVEMENT MARKING EPOXY (DIVIDED HIGHWAYS)

MONROE COUNTY (TOMAH)

HIGHWAY	LOCATION (From)	LOCATION (To)	646.1020				646.3020	646.5020	646.5120	646.7120	646.8120	646.8220	COMMENTS
			4-INCH EDGELINE WHITE L.F.	4-INCH EDGELINE YELLOW L.F.	4-INCH CENTERLINE NO PASSING YELLOW L.F.	4-INCH LANE LINE WHITE L.F.	8-INCH CHANNELIZING WHITE L.F.	ARROW TYPE 2 WHITE EACH	WORD ONLY WHITE EACH	12-INCH DIAGONAL YELLOW L.F.	CURB YELLOW L.F.	ISLAND NOSE YELLOW EACH	
USH 12 EB	0.39 Miles west of IH 94	Andres Street	1248	1068		275							
	Painted Median (Begin Divided)	(Double Yellow)			412				75			Painted Median	
	Andres Street	STH 21 (Includes Mini-Dashes)	1858	954		400							
	IH 94 EB On Ramp	Left Turn Lne		172			110						
	Frontage Road	Left Turn Lne		154			185					Commercial Entrance	
USH 12 EB	USH 12 EB	STH 21 EB (Left Turn Lane) (Median Island)	212	464			144	2	1		10	1	
	USH 12 EB	Ahead (Median Island)	212				144					Not First "ONLY"	
	USH 12 EB	STH 21 WB (Right Turn Lane)					185	2	1				
USH 12 WB	STH 21	Andres Street (Includes Mini-Dashes)	1610	1562		500							
	Frontage Road	Right Turn Lane					110					Commercial Entrance	
	IH 94 EB On Ramp	Right Turn Lane (Includes Mini-Dashes)	140										
USH 12 WB	IH 94 WB On Ramp	Left Turn Lane		230			125						
	Andres Street	0.39 Miles west of IH 94	1258	862		75							
	Painted Median (End Divided)	(Double Yellow)			478							Painted Median	
Sub Totals			6538	5466	890	1250	1003	4	2	75	10	1	

PROJECT NO: 1009-12-96

HWY: IH 94 & USH 12

COUNTY: MONROE

MISCELLANEOUS QUANTITIES

SHEET NO:

E

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PAVEMENT MARKING EPOXY (DIVIDED HIGHWAYS)			646.1020				646.3020			646.5020		646.5120	646.7120	
VERNON COUNTY (WESTBY - VIROQUA)			4-INCH EDGE LINE WHITE L.F.	4-INCH EDGE LINE YELLOW L.F.	4-INCH CENTERLINE YELLOW L.F.	4-INCH LANE LINE WHITE L.F.	8-INCH CHANNELIZING WHITE (MAINLINE) L.F.	8-INCH CHANNELIZING WHITE (ISLAND GORE) L.F.	8-INCH CHANNELIZING WHITE (MINI DASHES) L.F.	ARROWS TYPE 2 WHITE EACH	ARROWS TYPE 5 WHITE EACH	WORDS "ONLY" WHITE EACH	12-INCH DIAGONAL WHITE L.F.	
HIGHWAY	LOCATION (From)	LOCATION (To)												
USH 14 EB	200' West of Locust St (Westby)	0.27 Miles West of CTH BB (Viroqua)	24631	23926	2576	5953								
	Tri State Rd Left Turn Lane						213					2		2
	Tri State Rd Right Turn Lane						201					2		2
	Sherpe Rd Left Turn Lane								776			2		2
	Sherpe Rd Right Turn Lane						210					2		231
	Smith Rd Left Turn Lane								872			2		2
	Smith Rd Right Turn Lane						199					2		241
	Tom Burns Rd Left Turn Lane								921			2		2
	Tom Burns Rd Right Turn Lane						200					2		342
	Driveway Left Turn Lane						147					2		2
	Old Highway 14 Left Turn Lane								848			2		2
	Three Chimney Rd Right Turn Lane						198					2		2
	Oak Knoll Rd Right Turn Lane						212					2		2
	CTH Y Left Turn Lane								837			2		2
	CTH Y Right Turn Lane						196					2		2
	Field Entrance Left Turn Lane						253					2		2
	Driveway Left Turn Lane						247					2		2
	Springville Rd Left Turn Lane						127			32				1
Springville Rd Right Turn Lane					196					2		2		
Two Way Left Turn Lane (Springville Rd to CTH BB)										16				
USH 14 WB	0.27 Miles West of CTH BB (Viroqua)	200' West of Locust St (Westby)	24931	24738	636	5948								
	Springville Rd Left Turn Lane						225					2		2
	Driveway Left Turn Lane						256					2		2
	CTH Y Left Turn Lane								854			2		2
	CTH Y Right Turn Lane						198					2		2
	Oak Knoll Rd Left Turn Lane								822			2		2
	Three Chimney Rd Left Turn Lane								861			2		2
	Old Highway 14 Right Turn Lane						196					2		2
	Driveway Left Turn Lane						255					2		2
	Tom Burns Rd Left Turn Lane								919			2		2
	Tom Burns Rd Right Turn Lane						195					2		2
	Smith Rd Left Turn Lane								859			2		2
	Smith Rd Right Turn Lane						187					2		2
	Sherpe Rd Left Turn Lane								846			2		2
	Tri State Rd Left Turn Lane						317		318			2		2
Tri State Rd Right Turn Lane					170					2		2		
100 Feet West of Tri State Rd												2		
Sub Totals			49562	48664	3212	11901	4598	9733	32	80	2	65	2818	

PROJECT NO: 1009-12-96 HWY: USH 14 EB & WB COUNTY: VERNON MISCELLANEOUS QUANTITIES SHEET NO: E

FILE NAME : _____ PLOT DATE : _____ PLOT BY : _____ PLOT NAME : _____ ORG DATE : _____ ORIGINATOR : DIST _ PLOT SCALE : 1:1

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PAVEMENT MARKING EPOXY (DIVIDED HIGHWAYS)

DANE COUNTY (MOUNT HOREB - VERONA)

HIGHWAY	LOCATION (From)	LOCATION (To)	646.1020			646.3020		
			4-INCH EDGE LINE WHITE L.F.	4-INCH EDGE LINE YELLOW L.F.	4-INCH LANE LINE WHITE L.F.	8-INCH CHANNELIZING (MAINLINE) WHITE L.F.	8-INCH CHANNELIZING (RAMP) WHITE L.F.	8-INCH CHANNELIZING (ISLAND GORE) WHITE L.F.
USH 18/151 EB	Town Hall Rd (0.23 Miles East of Bus 18/151 On Ramp)	Fitchrona Rd (0.95 Miles West of CTH PD (Verona))	65602	67894	17226	5176	628	696
	Exit 70 - CTH PD/P	Off Ramp	1902	961		317	317	
	Exit 70 - CTH PD/P	On Ramp	2374	1043		583	599	
	Exit 75 - Dairy Ridge Rd	Off Ramp	1954	957		362	358	
	Exit 75 - Dairy Ridge Rd	On Ramp	2304	1046		691	691	
	Exit 76 - Epic Ln	Off Ramp	2975	2163	395	231	231	241
	Exit 76 - Epic Ln	On Ramp	3625	2444		407	407	
	Exit 77 - STH 69	Off Ramp	1608	1048		263	263	80
	Exit 77 - STH 69	On Ramp	2511	1060		774	774	
	Exit 79 - CTH PB	Off Ramp	2750	1511		603	603	80
	Exit 79 - CTH PB	On Ramp	2284	1158		527	527	
	Exit 81 - E Verona Ave	On Ramp	3594	2392		666	666	
	USH 18/151 WB	Fitchrona Rd (0.95 Miles West of CTH PD (Verona))	Town Hall Rd (0.23 Miles East of Bus 18/151 On Ramp)	66203	67309	17226	4942	343
Exit 81 - E Verona Ave		Off Ramp	2985	790	576	477	657	
Exit 79 - CTH PB		Off Ramp	2468	761	502	702	812	135
Exit 79 - CTH PB		On Ramp	2860	1359		879	879	
Exit 77 - STH 69		Off Ramp	2026	943		388	388	100
Exit 77 - STH 69		On Ramp	2106	1188		275	275	
Exit 76 - Epic Ln		Off Ramp	2130	1172		272	272	110
Exit 76 - Epic Ln		On Ramp	2813	1331	150	730	730	
Exit 75 - Dairy Ridge Rd		Off Ramp	2317	959		539	524	
Exit 75 - Dairy Ridge Rd		On Ramp	2526	1076		788	788	
Exit 70 - CTH PD/P		Off Ramp	1614	910		325	322	
Exit 70 - CTH PD/P		On Ramp	2360	1046		625	623	
Sub Totals			185891	162521	36075	21542	12677	2342

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PAVEMENT MARKING EPOXY (DIVIDED HIGHWAYS)

DANE COUNTY (MADISON - CTH N)

HIGHWAY	LOCATION (From)	LOCATION (To)	646.1020			646.3020		646.6120
			4-INCH EDGE LINE WHITE L.F.	4-INCH EDGE LINE YELLOW L.F.	4-INCH LANE LINE WHITE L.F.	8-INCH CHANNELIZING (MAINLINE) WHITE L.F.	8-INCH CHANNELIZING (RAMP) WHITE L.F.	18-INCH STOP LINE WHITE L.F.
USH 12/18 EB	(0.69 Miles East of CTH AB) (East of Madison)	1 Mile East of CTH N (Cottage Grove)	22800	22800	5746			
	Sigglekow Rd NB					225		15
	Vilas Rd NB							17
	Exit 272 - CTH N Exit 272 - CTH N N Star Rd NB	Off Ramp On Ramp	1830 2420	1309 1065		345 680	345 680	35 14
USH 12/18 WB	1 Mile East of CTH N (Cottage Grove)	(0.69 Miles East of CTH AB) (East of Madison)	22900	22900	5771			
	N Star Rd SB							18
	Exit 272 - CTH N	Off Ramp	1630	1150		334	334	25
	Exit 272 - CTH N	On Ramp	2430	1195		660	660	
	Vilas Rd SB							23
	Rothamer Rd							17
	Femrite Dr SB					232		27
Sub Totals			54010	50419	11517	2476	2019	191

PROJECT NO: 1009-12-96

HWY: USH 12/18

COUNTY: DANE

MISCELLANEOUS QUANTITIES

SHEET NO:

E

FILE NAME : _____

PLOT DATE : _____

PLOT BY : _____

PLOT NAME : _____

ORG DATE : _____

ORIGINATOR : DIST _

PLOT SCALE : 1:1

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PAVEMENT MARKING EPOXY (DIVIDED HIGHWAYS)			646.1020				646.3020			646.5020			646.5120	646.6120	646.7420	646.7520	
DANE COUNTY (MCFARLAND-DEFOREST)			4-INCH EDGE LINE WHITE L.F.	4-INCH EDGE LINE YELLOW L.F.	4-INCH LANE LINE WHITE L.F.	4-INCH MINI DASHES WHITE L.F.	8-INCH CHANNELIZING (MAINLINE) WHITE L.F.	8-INCH CHANNELIZING (RAMP) WHITE L.F.	8-INCH CHANNELIZING (ISLAND GORE) WHITE L.F.	ARROW TYPE 1 WHITE EACH	ARROW TYPE 2 WHITE EACH	ARROW TYPE 3 WHITE EACH	WORD ONLY WHITE EACH	18-INCH STOP LINE WHITE L.F.	6-INCH CROSSWALK WHITE L.F.	24-INCH CROSSWALK BLOCK WHITE L.F.	
HIGHWAY	LOCATION (From)	LOCATION (To)															
USH 51 NB	800' South of Tower Rd (McFarland)	Milwaukee St (Madison)	57526	56827	13153												
	Exchange St WB at USH 51	Right Turn Lane					205				2		2	22			
	Yahara Dr WB at USH 51													20			
	USH 51NB at Burma Rd(McFarland)													24	80		
	Burma Rd EB at USH 51														117		
	Two Way Left Turn Lane (Center)	(Burma Rd - Farwell St)									3						
	Farwell St at USH 51 NB (McFarland)	Left Turn Lane					70							45	157		
	Farewell St WB at USH 51													22	135		
	Dale-Curtin Dr EB at USH 51 (McFarland)	Left Turn Lane					105				1	1	1	30			
	USH 51NB at Larson Beach Rd (McFarland)	Left Turn Lane					85							47	190		
	Larson Beach Rd WB at USH 51										2	2	1	19	161		
	Exit to Siggelkow Rd	Off Ramp		1414	780		352	352						41	110		
	Exit to Siggelkow Rd	On Ramp		1995	830		663	663									
	USH 51 NB at Voges Rd (Terminal Dr)	Right Turn Lane							264								
	USH 51 NB at Voges Rd (Terminal Dr)	Ahead and Left Turn Lane					423							28			
Voges Rd WB (Terminal Dr) at USH 51													37				
USH 51 NB at CTH BW - Broadway Ave	Right Turn Lane							160					22				
USH 51 NB to CTH BW WB	Dual Left Turn Lane				86	125		242		4		2	62				
CTH BW WB to USH 51 SB	Dual Left Turn				86					2		2	24				
CTH BW WB at USH 51 NB	Ahead and Right Turn Lane												48				
USH 51 NB at Pflaum Rd	Left Turn Lane					970				2		1	15				
USH 51 NB at Pflaum Rd	Ahead and Right Turn Lane					650				2		1	56	252			
Pflaum Rd WB at USH 51 NB													57				
USH 51 NB at CTH AB	Right Turn Lane					524				2		1	22	247			
USH 51 NB at CTH AB	Ahead and Left Turn Lane					873				2		1	40				
CTH AB WB at USH 51 NB													57	49			
Exit at Cottage Grove Rd	Off Ramp		1521	1075		275	275	145					43	208			
Exit at Cottage Grove Rd	On Ramp		2107	1146		693	693										
Exit at Milwaukee St	Off Ramp		930	496	50	245	245	250		6		3	45	127			
Exit at Milwaukee St	On Ramp		2696	1197	250	265	265	250					60	100			
Milwaukee St WB	On Ramp to USH 51 NB & SB					110				1		1		240			
Sub Totals			68189	62351	13453	172	6633	2493	1311	0	29	3	16	918	2173	0	

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PAVEMENT MARKING EPOXY (DIVIDED HIGHWAYS)			646.1020				646.3020			646.5020			646.5120	646.6120	646.7420	646.7520
DANE COUNTY (MCFARLAND-DEFOREST) (Cont.)			4-INCH EDGELINE WHITE L.F.	4-INCH EDGELINE YELLOW L.F.	4-INCH LANELINE WHITE L.F.	4-INCH MINI DASHES WHITE L.F.	8-INCH CHANNELIZING (MAINLINE) WHITE L.F.	8-INCH CHANNELIZING (RAMP) WHITE L.F.	8-INCH CHANNELIZING (ISLAND GORE) WHITE L.F.	ARROW TYPE 1 WHITE EACH	ARROW TYPE 2 WHITE EACH	ARROW TYPE 3 WHITE EACH	WORD ONLY WHITE EACH	18-INCH STOP LINE WHITE L.F.	6-INCH CROSSWALK WHITE L.F.	24-INCH CROSSWALK BLOCK WHITE L.F.
HIGHWAY	LOCATION (From)	LOCATION (To)														
USH 51 NB	Milwaukee St (Madison)	7350' North of CTH V (Deforest)	38350	37884	8769											
	Exit at STH 30	Ahead and Right Turn Lane									2		2	82		
	Exit at STH 30	Left Turn Lane to STH 30 WB				38	766	605	55		4		4	70		
	Exit at STH 30	STH 30 WB Off Ramp				46								52		
	Commercial Ave	Ahead and Left Turn Lane					578							55		
	Commercial Ave	Right Turn Lane					359							26		
	Commercial Ave WB	To USH 51 SB				40								57		
	USH 151 (E Washington Ave)	Right Turn Lane					970				4		4	37	74	
	USH 151 (E Washington Ave)	Ahead and Left Turn Lane				64	177				2		2	36	176	
	USH 151 SB (E Washington St)	Left Turn to USH 51 SB				80								22		
	USH 151 SB (E Washington St)	Ahead and Right Turn Lane												58	140	
	Anderson St	Left Turn Lane					745				4		4	25		
	Anderson St (Service Rd)	Ahead and Right Turn Lane					161				2		1	36		132
	Service Rd (Anderson St) WB										1	1		20	115	
	Kinsman Blvd	Ahead and Turn Lanes					483				2		2	55	120	
	Kinsman Blvd WB						178			1	2		3	40	58	
	Peirstorff St WB													22		
	Exit to Rieder Rd	Off Ramp					393	267						20		
Hanson Rd													22			
Hoepker Rd	Ahead and Right Turn Lane					484				2		2	84			
Hoepker Rd	Left Turn Lane				28	1030							12			
Hoepker Rd WB					28					2		2	48			
Anderson Rd													59			
Anderson Rd WB										1		1	56			
Exit to IH 39/90/94 SB	On Ramp					177	180									
Exit to IH 39/90/94 NB	On Ramp					150	150									
USH 51NB at IH 30/90/94NB OFF Ramp													24			
Exit from IH 39/90/94 NB	Off Ramp					395	395									
Token Creek Ln	Left and Right Turn Lanes					735										
Token Creek Ln WB	At USH 51 NB												40			
Exit to STH 19	Off Ramp		1625	760		258	258	464					38			
Exit to STH 19	On Ramp		2088	900												
Exit to Windsor Rd	Off Ramp		1870	1022												
Exit to Windsor Rd	On Ramp		2612	1307												
Exit to CTH V	Off Ramp		2355	1540												
Exit to CTH V	On Ramp		3420	996												
Sub Totals			52320	44409	8769	324	8039	1855	519	1	28	1	27	1096	683	132

PROJECT NO: 1009-12-96 HWY: USH 51 NB COUNTY: DANE MISCELLANEOUS QUANTITIES SHEET NO: E

FILE NAME : _____ PLOT DATE : _____ PLOT BY : _____ PLOT NAME : _____ ORG DATE : _____ ORIGINATOR : DIST _ PLOT SCALE : 1:1

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3

PAVEMENT MARKING EPOXY (DIVIDED HIGHWAYS)			646.1020				646.3020			646.5020			646.5120	646.6120	646.7420	646.7520
DANE COUNTY (DEFOREST-MCFARLAND)			4-INCH EDGE LINE WHITE L.F.	4-INCH EDGE LINE YELLOW L.F.	4-INCH LANE LINE WHITE L.F.	4-INCH MINI DASHES WHITE L.F.	8-INCH CHANNELIZING (MAINLINE) WHITE L.F.	8-INCH CHANNELIZING (RAMP) WHITE L.F.	8-INCH CHANNELIZING (ISLAND GORE) WHITE L.F.	ARROW TYPE 1 WHITE EACH	ARROW TYPE 2 WHITE EACH	ARROW TYPE 3 WHITE EACH	WORD ONLY WHITE EACH	18-INCH STOP LINE WHITE L.F.	6-INCH CROSSWALK WHITE L.F.	24-INCH CROSSWALK BLOCK WHITE L.F.
HIGHWAY	LOCATION (From)	LOCATION (To)														
USH 51 SB	7350' North of CTH V (Deforest)	Milwaukee St (Madison)	38300	37964	9244											
	Exit to CTH V	Off Ramp	2030	1102												
	Exit to CTH V	On Ramp	2477	1071	77											
	Exit to Windsor Rd	Off Ramp	1947	1321												
	Exit to Windsor Rd	On Ramp	2385	1018												
	Exit to STH 19	Off Ramp	1598	774				296						54		
	Exit to STH 19	On Ramp	1987	758			626	626								
	Token Creek Ln	Left and Right Turn Lanes					611									
	Token Creek Ln EB	At USH 51 NB												48		
	North American Ln EB	At USH 51 SB												20		
	Exit to IH 39/90/94 NB	On Ramp					192	192								
	Exit to IH 39/90/94 SB	On Ramp					145	145								
	USH 51 SB at IH 39/90/94 NB Off Ramp													24		
	Exit from IH 39/90/94 SB	Off Ramp					291	291						15		
	Daentl Rd EB													35		
	CTH CV													53		
	CTH CV EB									4			4	75		
	Hoepker Rd	Ahead and Right Turn Lane					504			2			2	63		
	Hoepker Rd	Left Turn Lane				28	1030							12		
	Hoepker Rd EB					28				2			2	48		
	Amelia Earhart Dr													20		
	Exit to Rieder Rd	Left Turn Lane					439	437						16		
	Peirstorff St EB															
	Kinsman Blvd	Ahead and Turn Lanes					333				4		2	57	80	
	Kinsman Blvd EB						262			2	4		3	35	76	
	Anderson St	Right Turn Lane					298				2		1	19		36
	Anderson St (Service Rd)	Ahead and Left Turn Lane									2		1	35		48
	(Anderson St) EB									2	4		3	28		156
	USH 151 (E Washington Ave)	Right Turn Lane					112				2		1			
	USH 151 (E Washington Ave)	Ahead and Left Turn Lane					381				4		4	89	300	
	USH 151 NB (E Washington St)	Left Turn to USH 51 NB				60								24		
	USH 151 NB (E Washington St)	Ahead and Right Turn Lane				72								60	175	
	Lexington Ave	Ahead and Left Turn Lane					584							54		
	Lexington Ave	Right Turn Lane					362							25		
	Lexington Ave EB	Ahead and Turn Lanes												54		
	Exit at STH 30	Ahead and Right Turn Lane						661			3		3	74		
	Exit at STH 30	Left Turn Lane to STH 30 EB				38		602			4		4	65		
	Exit at STH 30	STH 30 EB Off Ramp												15		
		Sub Totals	50724	44008	9321	226	6170	2954	296	4	37	0	30	1117	631	240

3

3

PAVEMENT MARKING EPOXY (DIVIDED HIGHWAYS)			646.1020				646.3020			646.5020			646.5120	646.6120	646.7420	646.7520
DANE COUNTY (DEFOREST-MCFARLAND) (Cont.)			4-INCH EDGE LINE WHITE L.F.	4-INCH EDGE LINE YELLOW L.F.	4-INCH LANE LINE WHITE L.F.	4-INCH MINI DASHES WHITE L.F.	8-INCH CHANNELIZING (MAINLINE) WHITE L.F.	8-INCH CHANNELIZING (RAMP) WHITE L.F.	8-INCH CHANNELIZING (ISLAND GORE) WHITE L.F.	ARROW TYPE 1 WHITE EACH	ARROW TYPE 2 WHITE EACH	ARROW TYPE 3 WHITE EACH	WORD ONLY WHITE EACH	18-INCH STOP LINE WHITE L.F.	6-INCH CROSSWALK WHITE L.F.	24-INCH CROSSWALK BLOCK WHITE L.F.
HIGHWAY	LOCATION (From)	LOCATION (To)														
USH 51 SB	Milwaukee St (Madison)	800' South of Tower Rd (McFarland)	57450	56946	13866											
	Exit at Milwaukee St	Off Ramp	2635	1782	350		325	325	600	1	6		6	50	144	
	Exit at Milwaukee St	On Ramp	1777	525			659	659							107	
	Milwaukee St EB	On Ramp to USH 51 NB & SB					110				1		1	224	60	
	Exit at Cottage Grove Rd	Off Ramp	1373	1110			115	115	65					59	232	
	Exit at Cottage Grove Rd	On Ramp	1423	1062			331	331								
	USH 51 SB at CTH AB	Right Turn Lane					979							17		
	USH 51 SB at CTH AB	Ahead and Left Turn Lane					1341							36	244	
	CTH AB EB at USH 51 SB													59		
	USH 51 SB at Pflaum Rd	Left Turn Lane					1026							12		
	USH 51 SB at Pflaum Rd	Ahead and Right Turn Lane					682							66	291	
	Pflaum Rd EB at USH 51 SB													56		
	USH 51 SB at CTH BW - Broadway Ave	Right Turn Lane							366					25		
	USH 51 SB to CTH BW EB	Left Turn Lane				86			542		2		1	13		
	USH 51 SB at CTH BW - Broadway Ave	Ahead Lanes												46		
	CTH BW EB to USH 51 NB	Dual Left Turn				86					2		2	25		
	CTH BW WB at USH 51 NB	Ahead and Right Turn Lanes												42		
	USH 51SB at Terminal Dr (Voges Rd)	Right Turn Lane					728							32		
	USH 51SB at Terminal Dr (Voges Rd)	Ahead and Left Turn Lane					516							35		
	Terminal Dr EB (Voges Rd) at USH 51													46		
	Exit to Siggelkow Rd	Off Ramp	1596	851			475	475						39		
	Exit to Siggelkow Rd	On Ramp	1516	766			341	339							135	
	USH 51SB at Larson Beach Rd (McFarland)	Left and Right Turn Lanes					353							53		
	Larson Beach Rd EB at USH 51													26		
	Dale Rd WB at USH 51	Left Turn Lane					105							42		
	USH 51SB at Farwell St	Left Turn Lane					235							37	162	
	Farewell St EB at USH 51													23	114	
	USH 51SB at Burma Rd														89	
	Burma Rd EB at USH 51													18		
	Bible Camp Rd EB at USH 51 (Mcfarland)													39		
		Sub Totals	67770	63042	14216	172	8321	2244	1573	1	11	0	10	1120	1578	0

PROJECT NO: 1009-12-96 HWY: USH 51 SB COUNTY: DANE MISCELLANEOUS QUANTITIES SHEET NO: E

FILE NAME : _____ PLOT DATE : _____ PLOT BY : _____ PLOT NAME : _____ ORG DATE : _____ ORIGINATOR : DIST _ PLOT SCALE : 1:1

PAVEMENT MARKING EPOXY (DIVIDED HIGHWAYS)

JEFFERSON COUNTY (FORT ATKINSON - JOHNSON CREEK)

HIGHWAY	LOCATION (From)	LOCATION (To)	646.1020			646.3020			646.6120
			4-INCH EDGE LINE WHITE L.F.	4-INCH EDGE LINE YELLOW L.F.	4-INCH LANE LINE WHITE L.F.	8-INCH CHANNELIZING (MAINLINE) WHITE L.F.	8-INCH CHANNELIZING (RAMP) WHITE L.F.	8-INCH CHANNELIZING (ISLAND GORE) WHITE L.F.	18-INCH STOP LINE WHITE L.F.
STH 26 NB	1250' South of MM 18 (Fort Atkinson)	River Road (Johnson Creek)	78881	90399	22600				
	Exit 19, STH 106 (Fort Atkinson)	Off RAMP	1513	750		506	313		35
	Exit 19, STH 106 (Fort Atkinson)	On Ramp	2575	1111		887	868		
	Exit 21, USH 12 (Fort Atkinson)	Off RAMP	1670	827		215	215		33
	Exit 21, USH 12 (Fort Atkinson)	On Ramp	2430	1041		749	730		
	Exit 23, Business 26 (Fort Atkinson)	Off RAMP	1607	882		253	253		35
	Exit 23, Business 26 (Fort Atkinson)	On Ramp	2454	1154		620	554		
	Exit 25, Business 26 / To STH 89 (Jeffersrson)	Off RAMP	1851	1107		375	277	80	41
	Exit 25, Business 26 / To STH 89 (Jeffersrson)	On Ramp	1986	836		431	382		
	Exit 27, USH 18 (Jefferson)	Off RAMP	1692	1185		408	373	112	35
	Exit 27, USH 18 (Jefferson)	On Ramp	1990	571		349	198		
	Exit 30, Business 26 (Jefferson)	Off RAMP	1536	836		350	250		24
	Exit 30, Business 26 (Jefferson)	On Ramp	2071	1055		340	289		
	STH 26 SB	River Road (Johnson Creek)	1250' South of MM 18 (Fort Atkinson)	75625	86122	21531			
Exit 30, Business 26 (Jefferson)		Off RAMP	1707	1024		401	311	150	38
Exit 30, Business 26 (Jefferson)		On Ramp	2145	943		531	470		
Exit 27, USH 18 (Jefferson)		Off RAMP	1767	775		426	275	225	35
Exit 27, USH 18 (Jefferson)		On Ramp	2179	975		494	425		
Exit 25, Business 26 / To STH 89 (Jeffersrson)		Off RAMP	1707	910		374	259	91	40
Exit 25, Business 26 / To STH 89 (Jeffersrson)		On Ramp	2105	876		520	454		
Exit 23, Business 26 (Fort Atkinson)		Off RAMP	1711	1193	75	386	386	80	23
Exit 23, Business 26 (Fort Atkinson)		On Ramp	2980	1730		400	341	71	
Exit 21, USH 12 (Fort Atkinson)		Off RAMP	1683	1290		244	244		29
Exit 21, USH 12 (Fort Atkinson)		On Ramp	2077	905		630	610		
Exit 19, STH 106 (Fort Atkinson)		Off RAMP	1356	897		323	323		31
Exit 19, STH 106 (Fort Atkinson)		On Ramp	2019	737		725	707		
Sub Totals			201317	200131	44206	10937	9507	809	399

PROJECT NO: 1009-12-96

HWY: STH 26

COUNTY: JEFFERSON

MISCELLANEOUS QUANTITIES

SHEET NO:

E

FILE NAME :

PLOT DATE :

PLOT BY :

PLOT NAME :

ORG DATE :

ORIGINATOR : DIST _

PLOT SCALE : 1:1

PAVEMENT MARKING EPOXY (DIVIDED HIGHWAYS)

LA CROSSE COUNTY (LA CROSSE)

HIGHWAY	LOCATION (From)	LOCATION (To)	646.1020				646.3020		646.5020	646.5120	646.6120	646.7120	SPV.0090.02
			4-INCH EDGE LINE WHITE L.F.	4-INCH EDGE LINE YELLOW L.F.	4-INCH CENTERLINE YELLOW L.F.	4-INCH LANE LINE WHITE L.F.	8-INCH CHANNELIZING WHITE (MAINLINE) L.F.	8-INCH CHANNELIZING WHITE (ISLAND GORE) L.F.	ARROWS TYPE 2 WHITE EACH	WORDS "ONLY" WHITE EACH	18-INCH STOP LINE WHITE L.F.	12-INCH DIAGONAL YELLOW L.F.	CROSSWALK TRANSVERSE 12-INCH WHITE L.F.
STH 33 EB	150' East of 32nd Street (La Crosse)	0.42 Miles East of CTH F	17726		2308	2237							
	150' East of 32nd Street (La Crosse) (TWLTL)	250' West of Hagen Road (TWLTL)		120	3512				12				
	Hiawatha Avenue	Right Turn Lane					100		1				
	Hagen Road / Wedgewood Drive	Right & Left Turn Lanes					200	321	4	1	37	60	98
	Hagen Road NB	STH 33 EB									30		
	425' East of Hagen Road (TWLTL)	Southdale Drive (TWLTL)			6212				24				
	Southdale Drive	Right Turn Lane					200		2	1			
Boma Road	Left Turn Lane		338			140		2	1				
Boma Road (TWLTL)	Horseshoe Place (TWLTL)			688				4					
Horseshoe Place	Right Turn Lane					100		1					
Irish Court	Left Turn Lane		390			100		1			76		
Irish Court (TWLTL)	250' East of Irish Court (TWLTL)			500							127		
Norseman Road	Right Turn Lane					100		1					
Knobloch Road	Right Turn Lane					100		1			42		
CTH F	Left Turn Lane		737			300		3	2				
Forest Ridge Drive	Right Turn Lane					100		1					
USH 33 WB	0.42 Miles East of CTH F	150' East of 32nd Street (La Crosse)	18300										
	CTH F	Right Turn Lane					200		2	1			
	Knobloch Road	Left Turn Lane		737			295		3	2			
	250' East of Irish Court (TWLTL)	Irish Court (TWLTL)			500								
	Irish Court	Right Turn Lane					100		1				
	Horseshoe Place	Left Turn Lane		390			100		1				
Horseshoe Place (TWLTL)	Boma Road (TWLTL)			688									
Boma Road	Right Turn Lane					100		1					
Southdale Drive	Left Turn Lane		338			140		1	1				
Southdale Drive (TWLTL)	425' East of Hagen Road (TWLTL)			4800									
Hagen Road	Left Turn Lane						604	2	1	27	55	34	
Wedgewood Drive SB	STH 33 WB									22		115	
250' West of Hagen Road (TWLTL)	150' East of 32nd Street (La Crosse) (TWLTL)		150	3512									
Sub Totals			36026	3200	22720	2237	2375	925	68	10	116	360	247

PROJECT NO: 1009-12-96

HWY: STH 33

COUNTY: LA CROSSE

MISCELLANEOUS QUANTITIES

SHEET NO:

E

FILE NAME :

PLOT DATE :

PLOT BY :

PLOT NAME :

ORG DATE :

ORIGINATOR : DIST _

PLOT SCALE : 1:1

3

PAVEMENT MARKING EPOXY (SPECIAL MARKINGS)

DANE COUNTY (ROUNDBABOUTS)

HIGHWAY	LOCATION	646.1020					646.3020		
		4-INCH CENTERLINE YELLOW L.F.	4-INCH CIRCULATORY 6' SEGMENT WHITE L.F.	4-INCH EDGE LINE YELLOW L.F.	4-INCH EDGE LINE WHITE L.F.	4-INCH LANE LINE WHITE L.F.	8-INCH LANE LINE WHITE L.F.	8-INCH CHANNELIZING WHITE L.F.	8-INCH CHANNELIZING YELLOW L.F.
STH 78/92	Dane County Roundabout at CTH ID (Mount Horeb)	1904	90	530		182		217	
STH 30	Thompson Rd (Madison)	2512		600	711	508	40	95	100
SUB TOTALS		4416	90	1130	711	690	40	312	100

3

PAVEMENT MARKING EPOXY (SPECIAL MARKINGS)

DANE COUNTY (ROUNDBABOUTS)

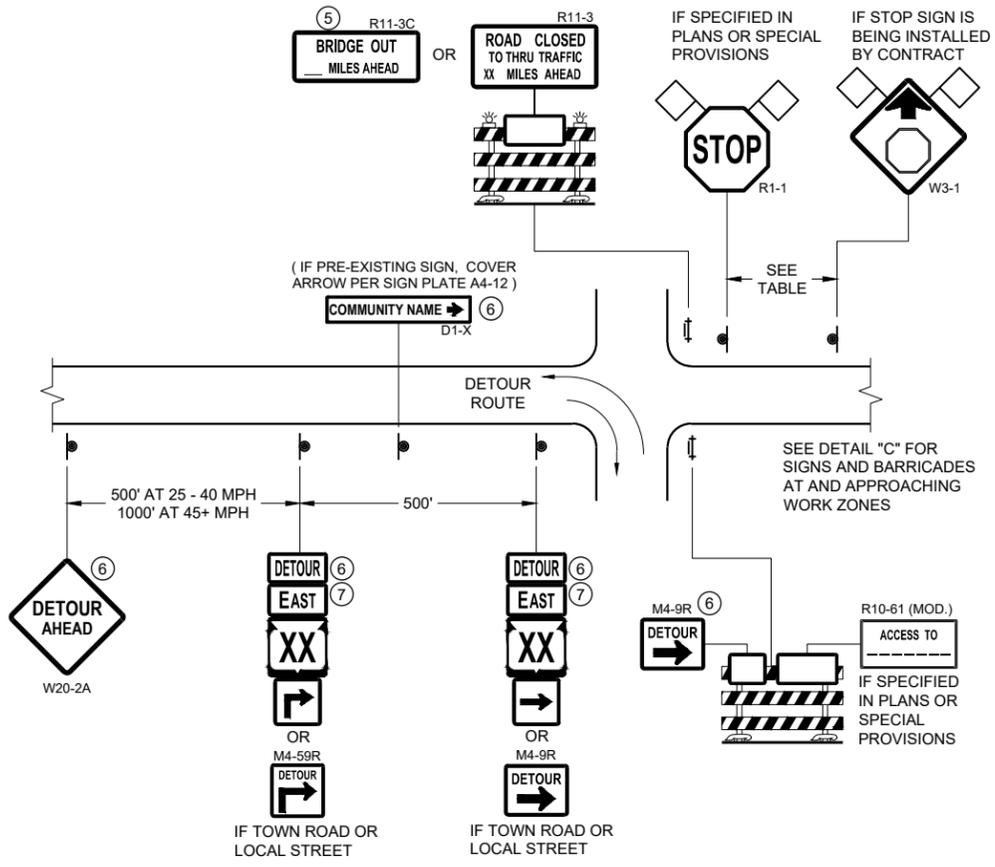
HIGHWAY	LOCATION	646.5020					646.5120	646.6320	646.7120		646.7420	SPV.0090.01
		ARROWS TYPE 1 WHITE EACH	ARROWS TYPE 2R WHITE EACH	ARROWS TYPE 2 WHITE EACH	ARROWS TYPE 3R WHITE EACH	ARROWS TYPE 3 WHITE EACH	WORDS "ONLY" WHITE EACH	18-INCH DOTTED EXTENSION WHITE L.F.	12-INCH DIAGONAL WHITE L.F.	12-INCH DIAGONAL YELLOW L.F.	6-INCH CROSSWALK (BORDER) WHITE L.F.	12-INCH CROSSWALK Block WHITE L.F.
STH 78/92	Dane County Roundabout at CTH ID (Mount Horeb)	1		5	4	5	1	51			243	350
STH 30	Thompson Rd (Madison)	2	2	8	6	7		128	116	234	585	621
SUB TOTALS		3	2	13	10	12	1	179	116	234	828	971

*Do not replace arrows or words after the crosswalks
*Do not replace second set of arrows on STH 30 EB off ramp

HIGHWAY	LOCATION	646.1020				646.3020 8-INCH CHANNELIZING WHITE L.F.	646.6320	646.7020 6-INCH DIAGONAL WHITE L.F.	646.7120		
		4-INCH LANE LINE WHITE L.F.	4-INCH CIRCULATORY 6' SEGMENT WHITE L.F.	4-INCH EDGELINE YELLOW L.F.	4-INCH LANE SEPARATOR WHITE L.F.		18-INCH DOTTED EXTENSION WHITE L.F.		12-INCH DIAGONAL WHITE L.F.	12-INCH DIAGONAL (CHEVRONS) WHITE L.F.	12-INCH DIAGONAL YELLOW L.F.
USH 18	Iowa County Bennett Rd (Dodgeville) (Lane Line 1400' West to 1500' East)	3171	312	1040	865	558	70	58	106	50	92
SUB TOTALS		3171	312	1040	865	558	70	58	106	50	92

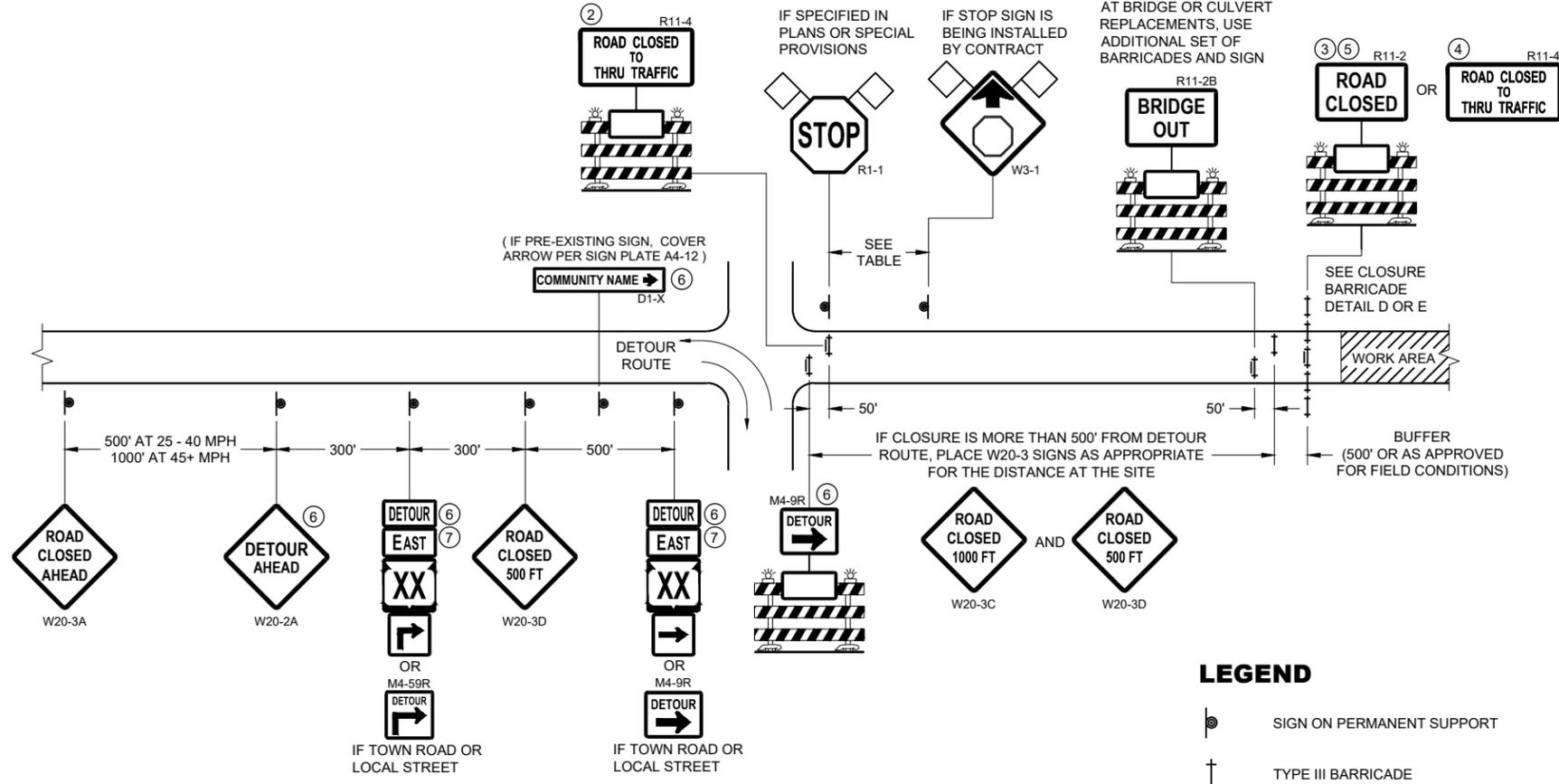
Standard Detail Drawing List

15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C06-09	SIGNING & MARKING FOR TWO LANE BRIDGES
15C07-15B	PAVEMENT MARKING WORDS
15C07-15C	PAVEMENT MARKING ARROWS
15C07-15D	ROUNDBABOUT ARROWS
15C08-20A	LONGITUDINAL MARKING (MAINLINE)
15C08-20B	PAVEMENT MARKING (TURN LANES)
15C08-20C	PAVEMENT MARKING (TURN LANES)
15C11-09B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-07	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C18-05A	MEDIAN ISLAND MARKING PAVEMENT MARKINGS
15C18-05B	MEDIAN ISLAND MARKING MEDIAN ISLAND NOSE
15C19-06A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C19-06B	MOVING PAVEMENT MARKING OPERATION MULTI-LANE UNDIVIDED ROADWAY
15C19-06C	MOVING PAVEMENT MARKING OPERATION MULTI-LANE DIVIDED ROADWAY
15C31-04A	PAVEMENT MARKING EXIT RAMP AND PARALLEL EXIT RAMP
15C31-04B	PAVEMENT MARKING MAJOR SPLIT FREEWAY TO FREEWAY
15C31-04C	PAVEMENT MARKING ENTRANCE RAMP AND PARALLEL ENTRANCE RAMP
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-04A	PAVEMENT MARKING (INTERSECTIONS)
15C35-04B	PAVEMENT MARKING AND SIGNING (CLIMBING LANE & PASSING LANE)
15C35-04C	PAVEMENT MARKING AND SIGNING (CLIMBING LANE & PASSING LANE)
15D12-09A	TRAFFIC CONTROL, LANE CLOSURE
15D20-05A	TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY
15D20-05B	TRAFFIC CONTROL, SINGLE RIGHT LANE CLOSURE, UNDIVIDED NON-FREEWAY/EXPRESSWAY
15D20-05C	TRAFFIC CONTROL, SINGLE LEFT LANE CLOSURE, UNDIVIDED NON-FREEWAY/EXPRESSWAY
15D27-03	TRAFFIC CONTROL, SHOULDER CLOSURE ON DIVIDED ROADWAY, SPEEDS GREATER THAN 40 MPH
15D37-03	TRAFFIC CONTROL, 2-LANE ROUNDBABOUT
15D50-01A	TRAFFIC CONTROL, ADDED LANE CLOSURE WITHOUT LANE SHIFT
15D50-01B	TRAFFIC CONTROL, ADDED LANE CLOSURE WITH LANE SHIFT



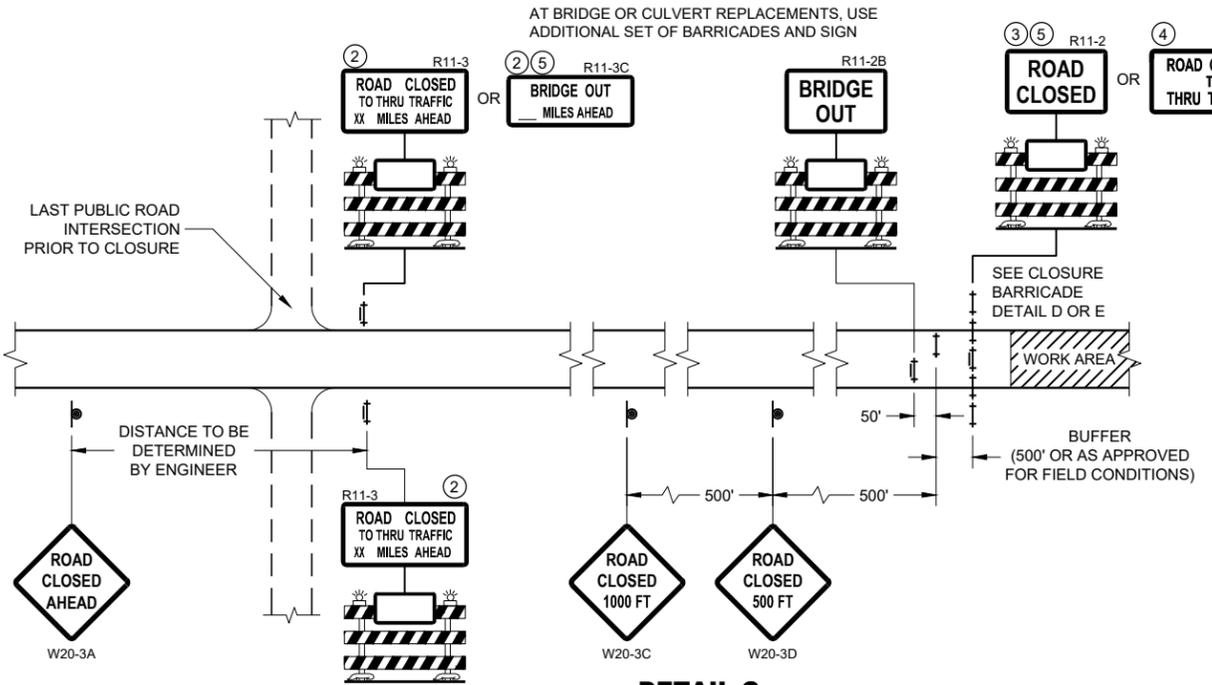
**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)



**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)



**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- M1 - 4 OR M1 - 6 OR M1 - 5A
- M05 - 1 OR M06 - 1

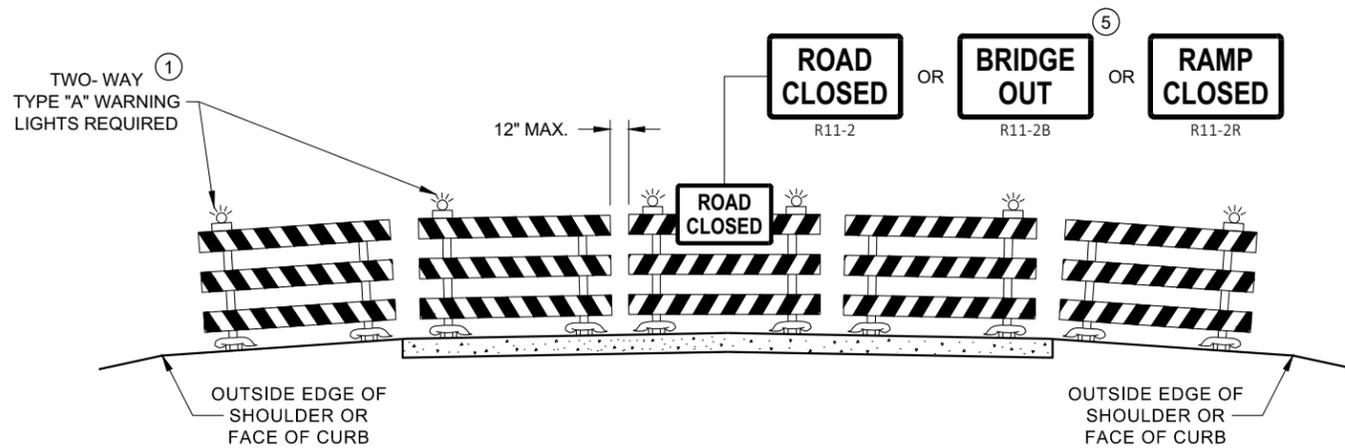
SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

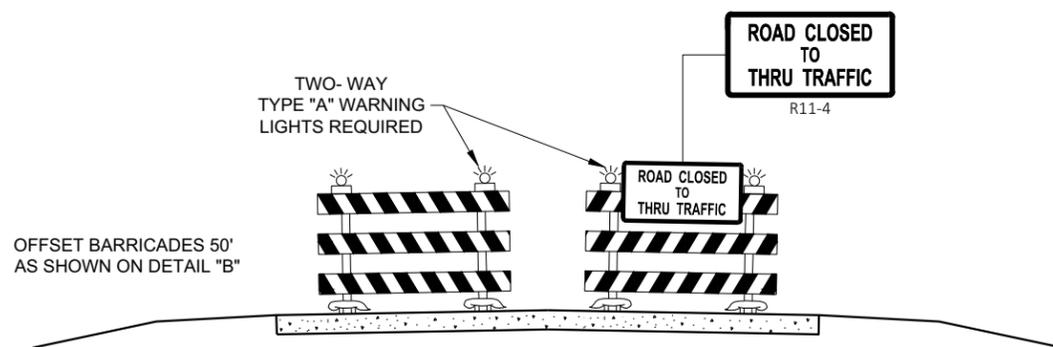
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER

FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

GENERAL NOTES

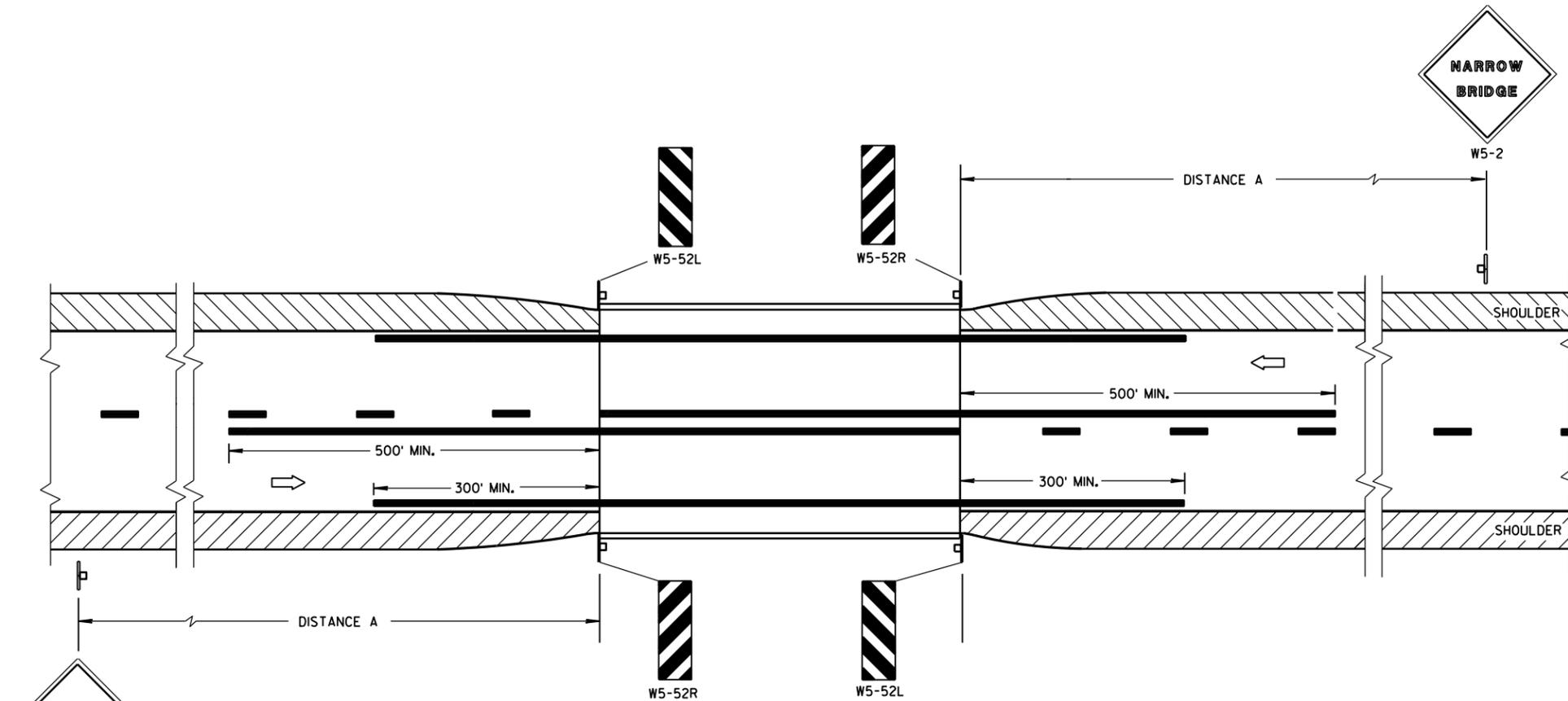
DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

LOCATE W5-52 SIGN POST(S) BEHIND GUARDRAIL WHEN PRESENT.

PLACE THE EDGE OF THE W5-52 SIGN IN LINE WITH FACE OF CURB OR PARAPET.

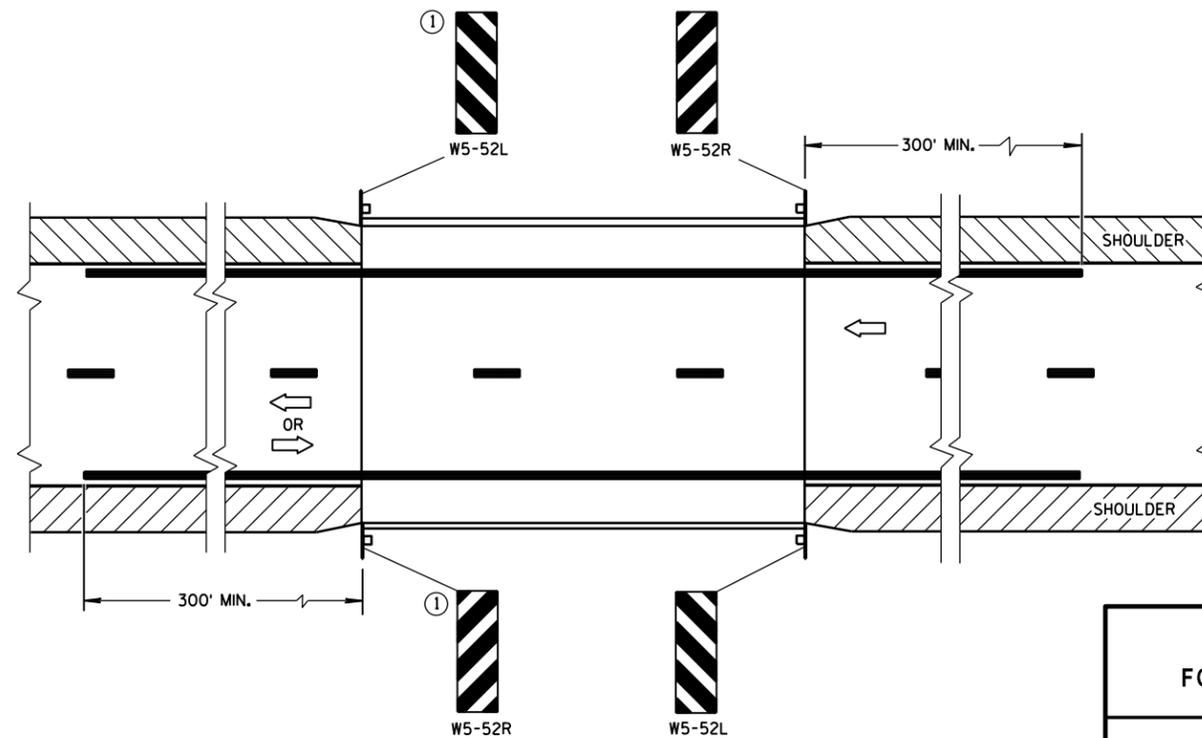
① OMIT ON ONE-WAY TRAVELLED WAYS.

➡ DIRECTION OF TRAFFIC



SITUATION 1

WARRANTING CRITERIA:
BRIDGE WIDTH IS AT LEAST 16 FEET BUT LESS THAN 24 FEET.



SITUATION 2

WARRANTING CRITERIA:
1. BRIDGE WIDTH IS AT LEAST 24 FEET AND
2. BRIDGE SHOULDER WIDTH IS LESS THAN 6 FEET.

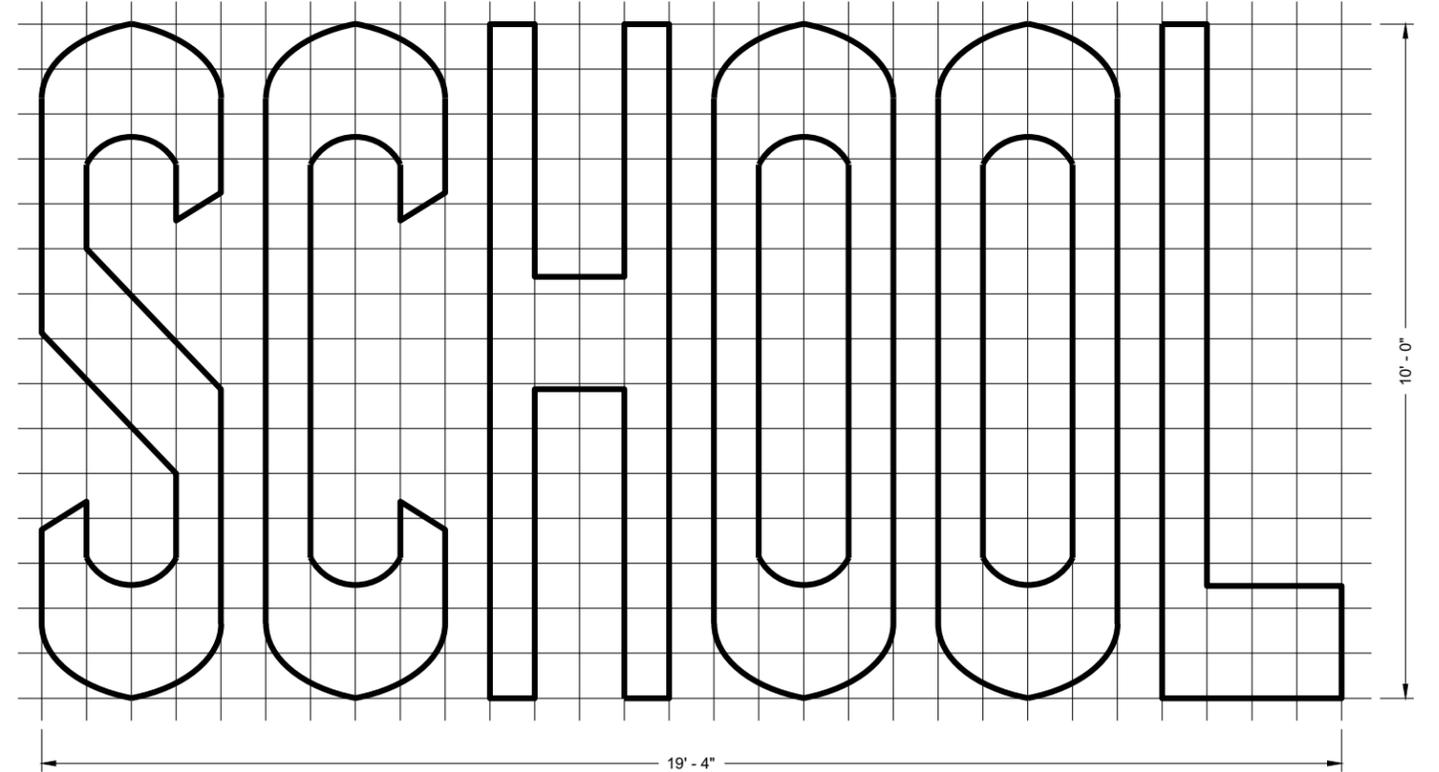
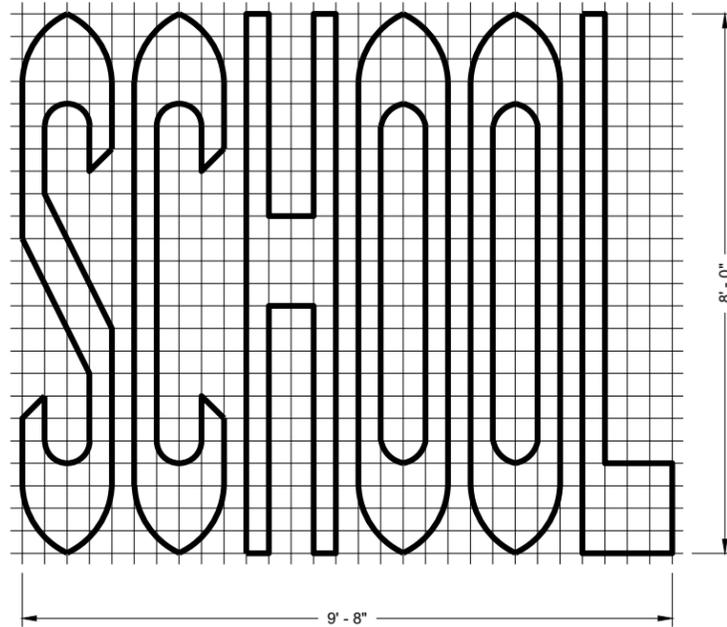
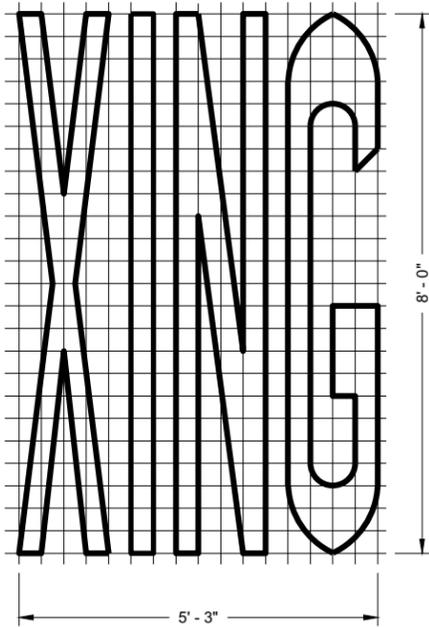
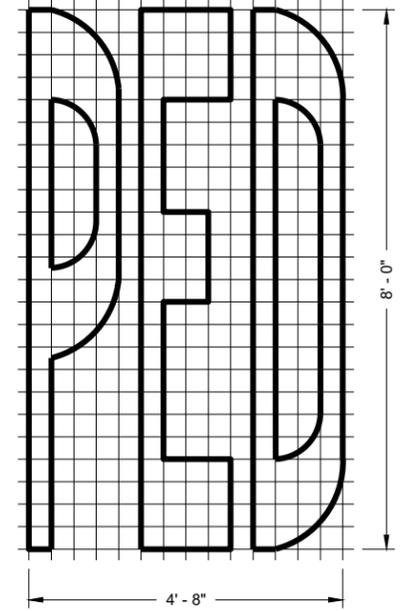
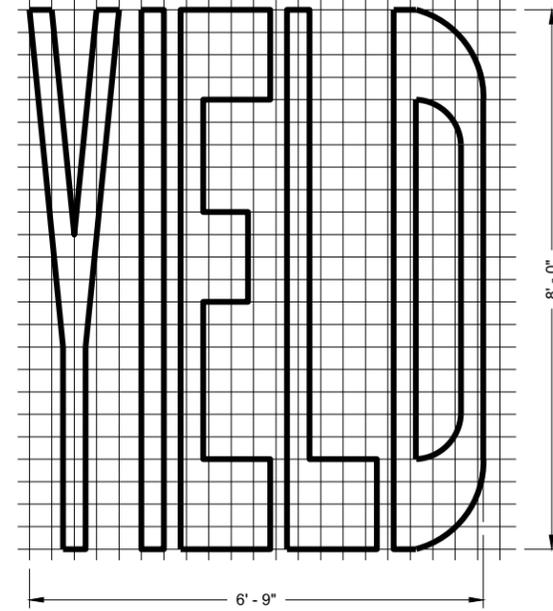
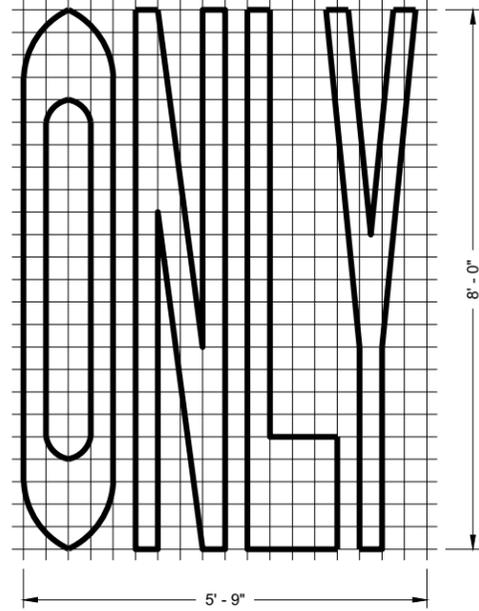
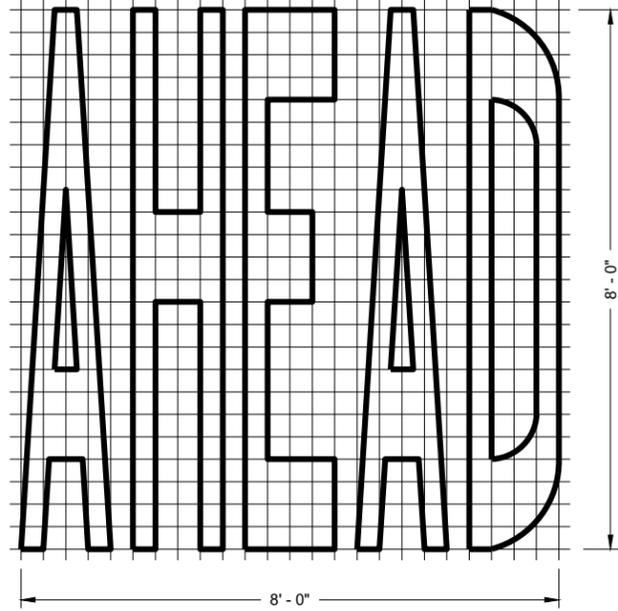
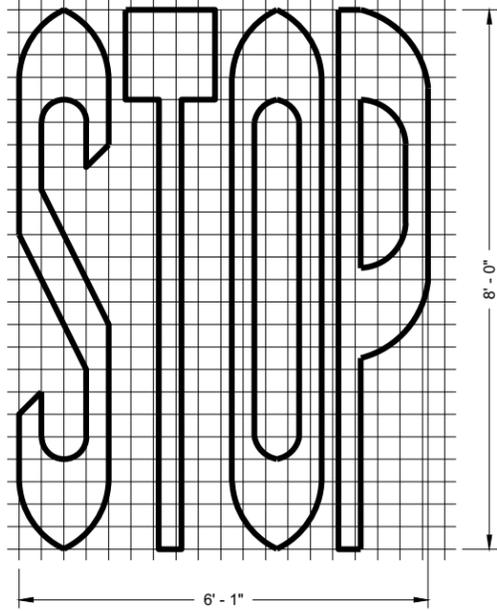
DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
25	150'
30	200'
35	250'
40	300'
45	400'
50	550'
55	750'

SIGNING & MARKING FOR TWO LANE BRIDGES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 /S/ Matthew R. Rauch
DATE STATE SIGNING AND MARKING ENGINEER
FHWA



SINGLE LANE

TWO - LANE

GENERAL NOTES

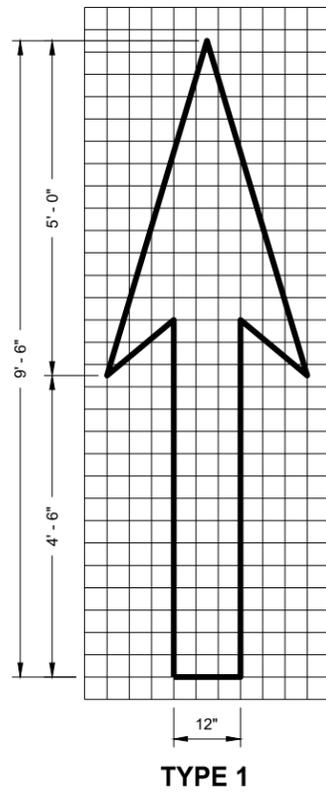
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING WORDS

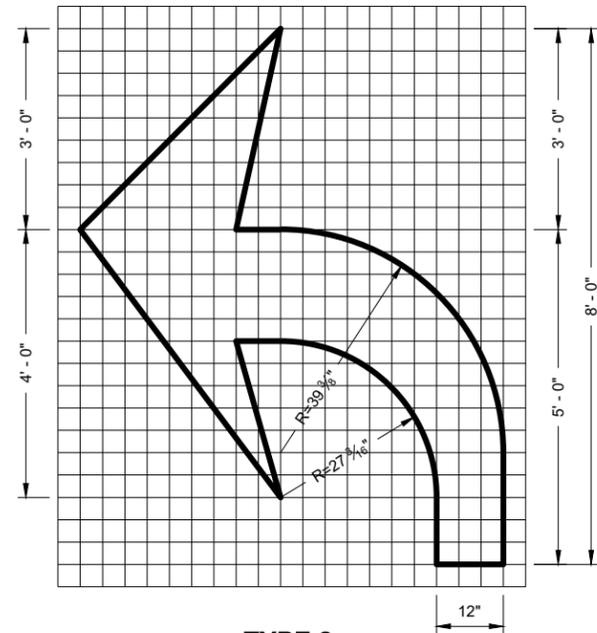
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER

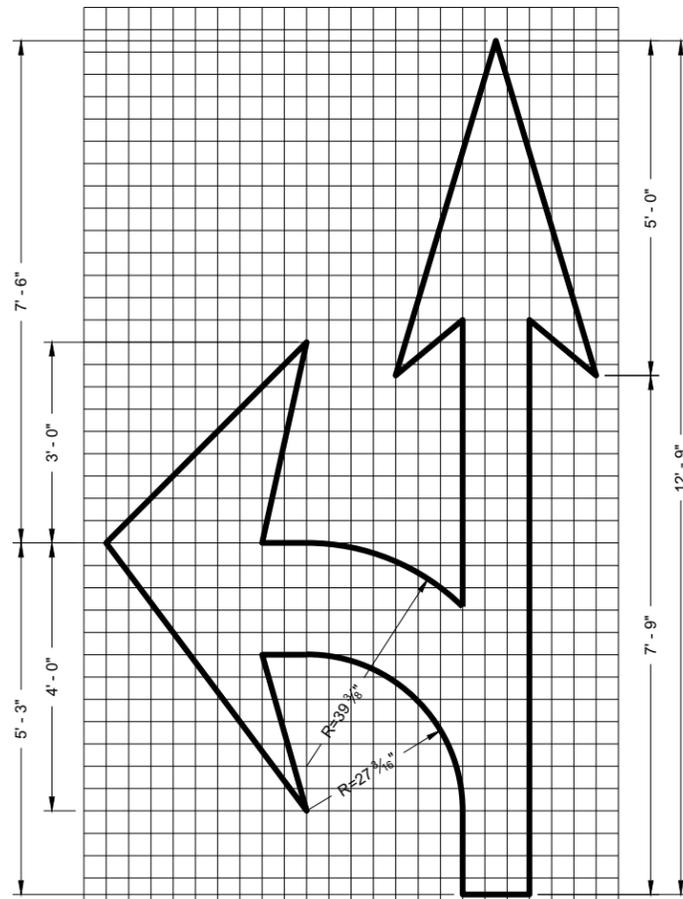
FHWA



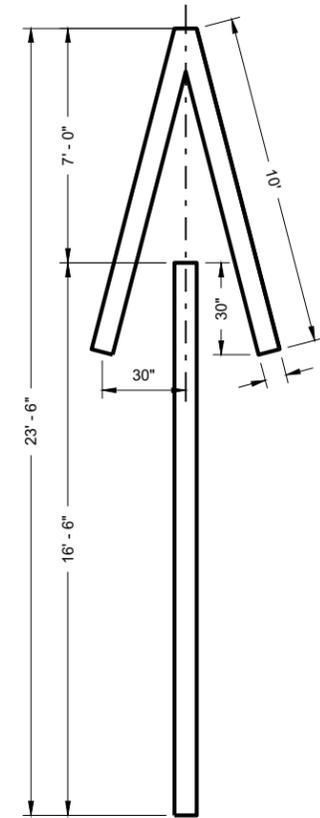
TYPE 1



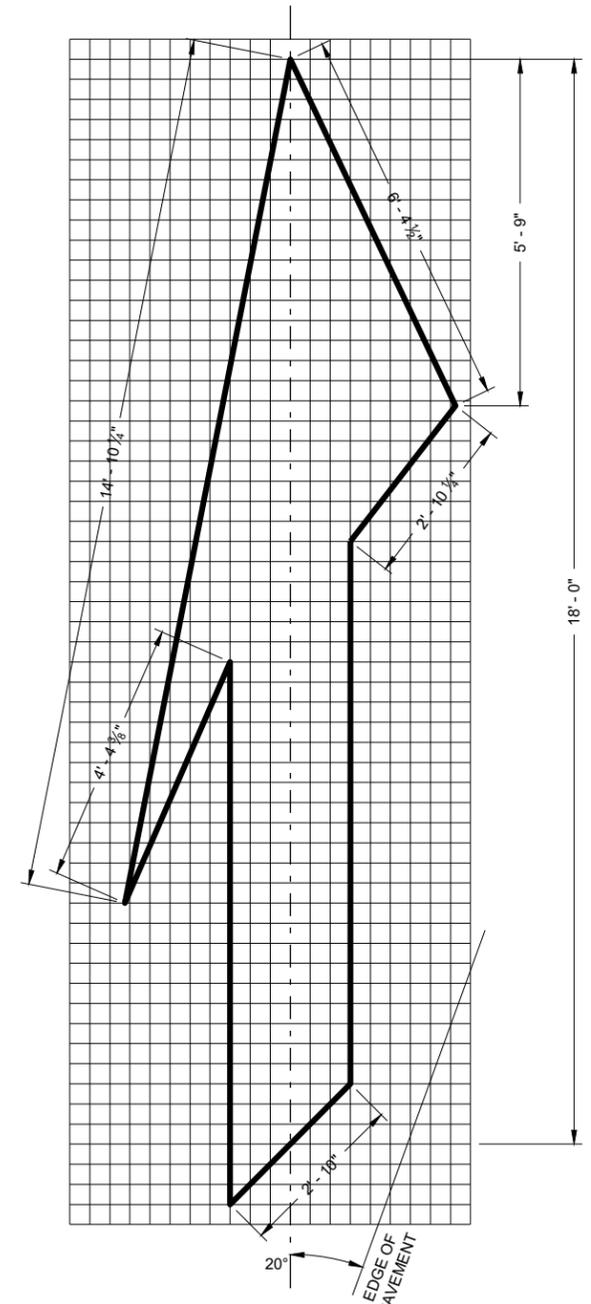
TYPE 2



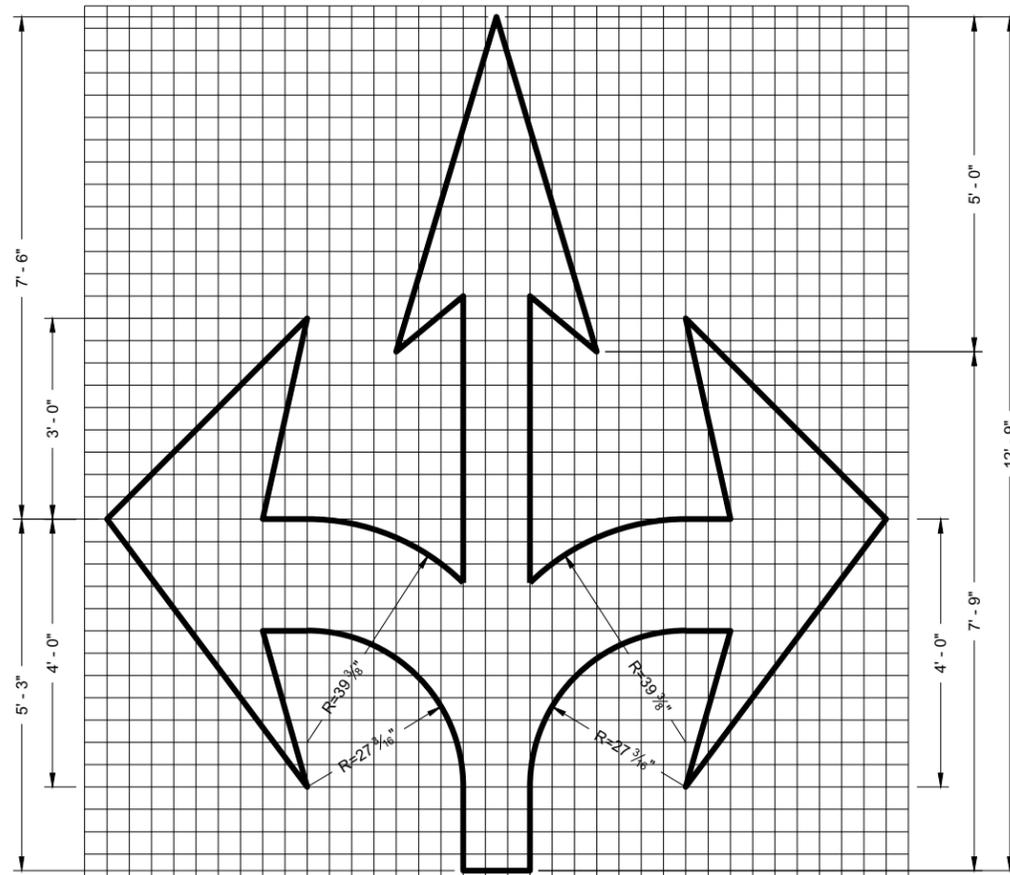
TYPE 3



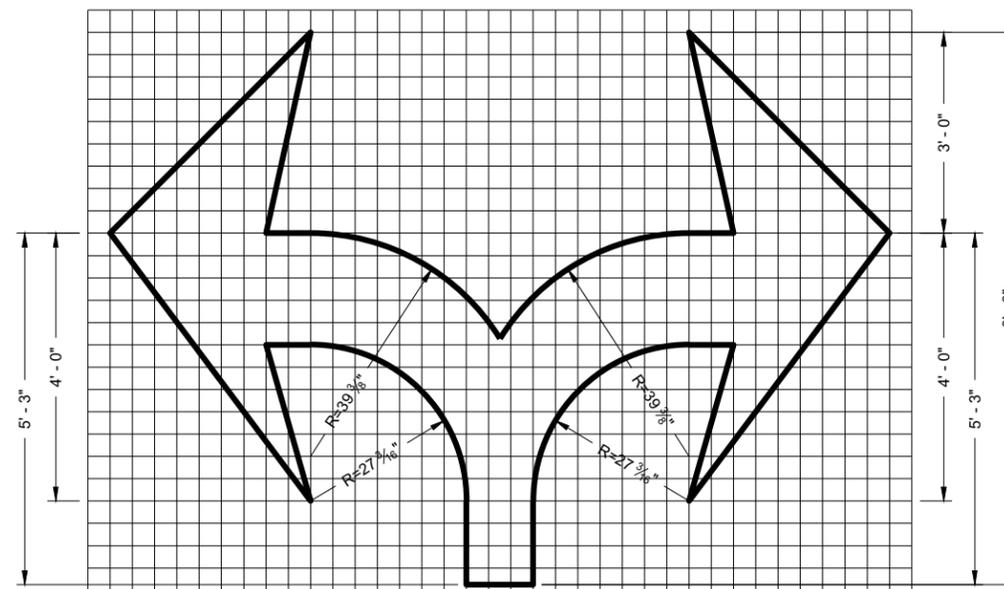
TYPE 4



TYPE 5 LANE DROP ARROW



TYPE 6



TYPE 7

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

November 2019

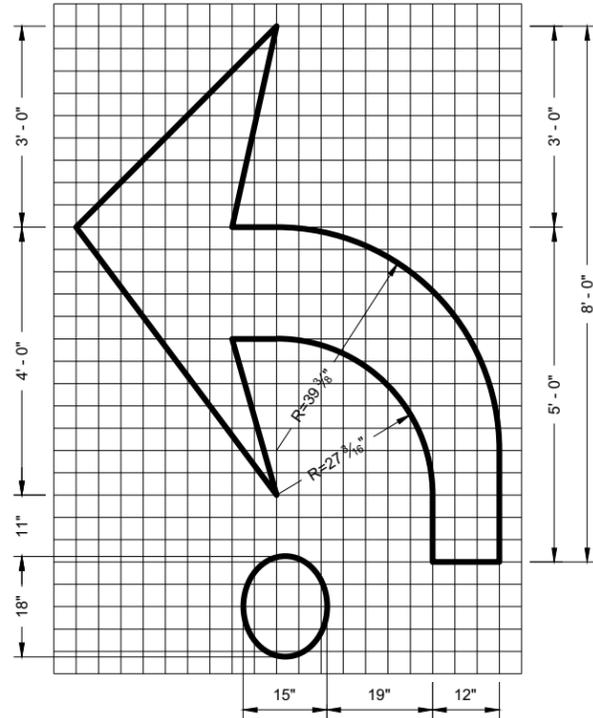
DATE

FHWA

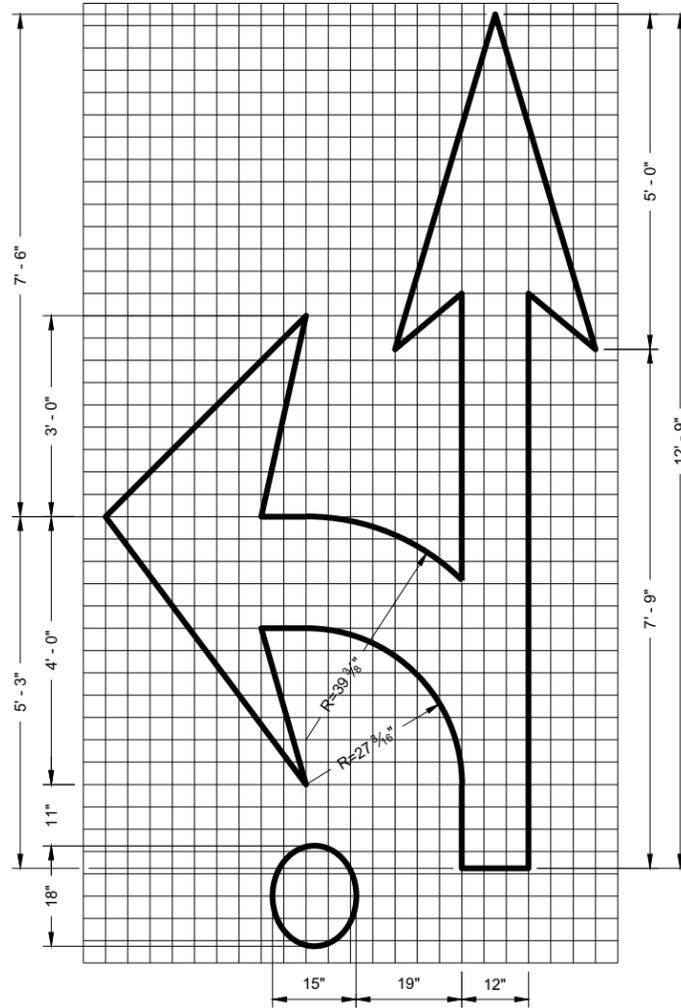
/S/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER

GENERAL NOTES

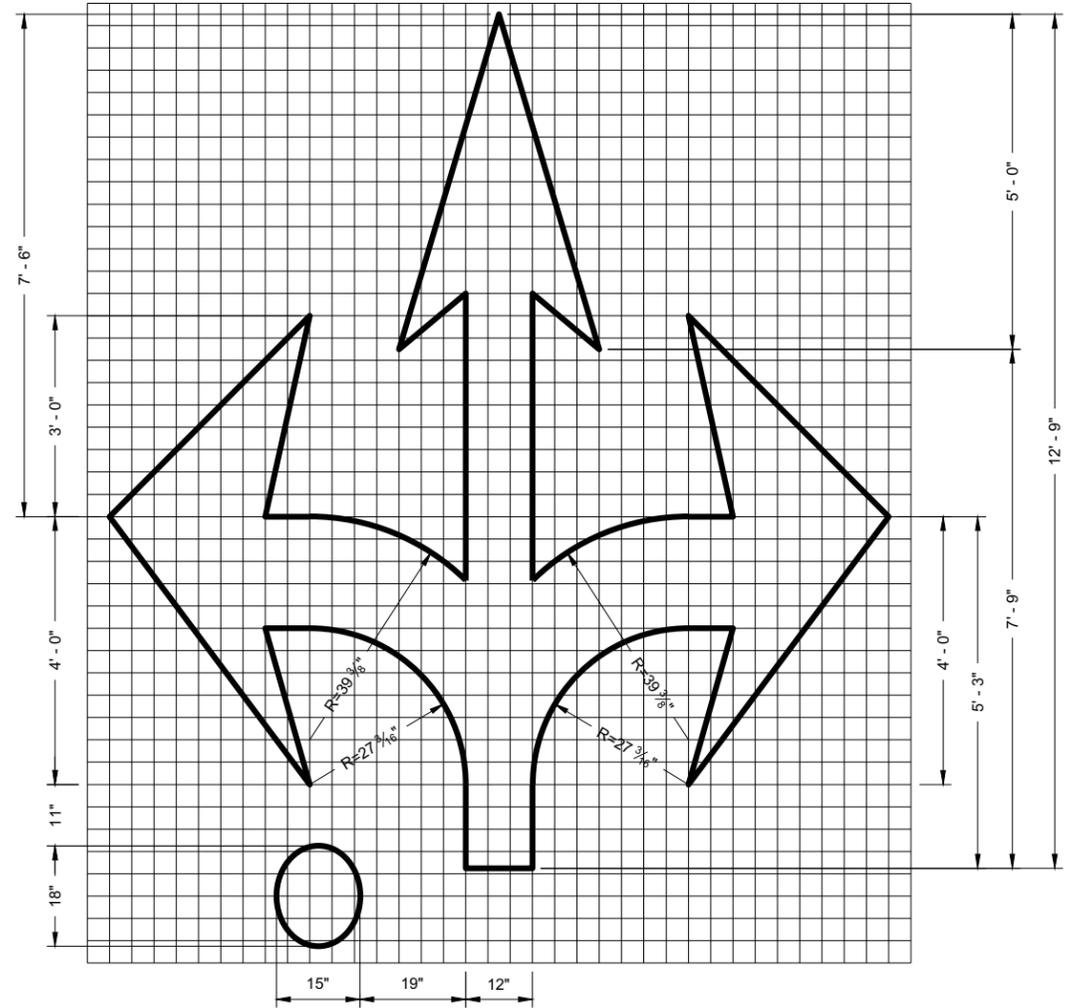
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



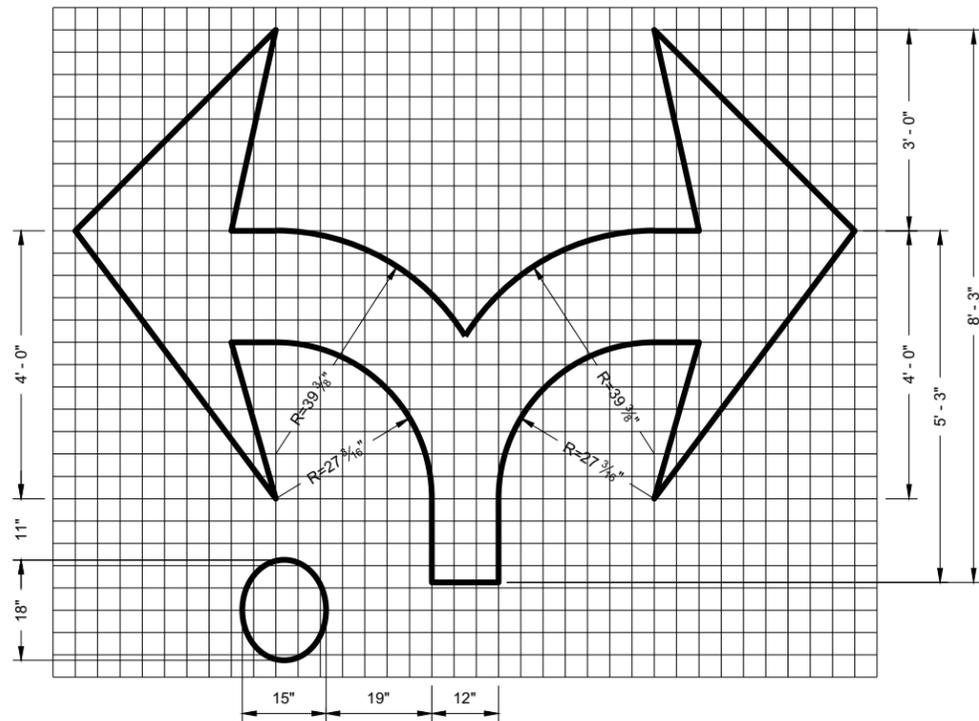
TYPE 2R



TYPE 3R



TYPE 6R



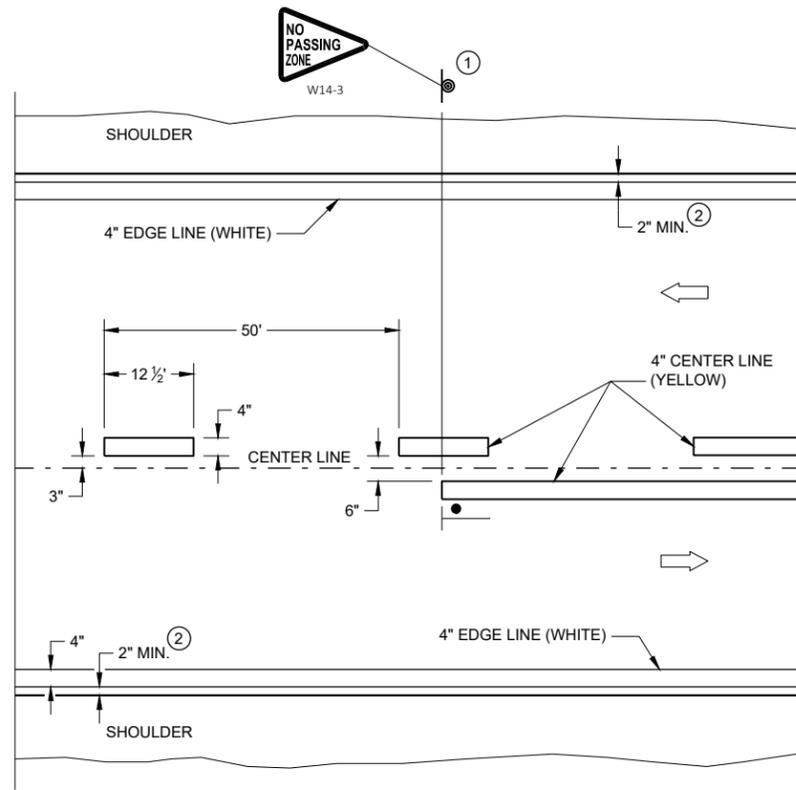
TYPE 7R

**ROUNDBOUT
MARKING ARROWS**

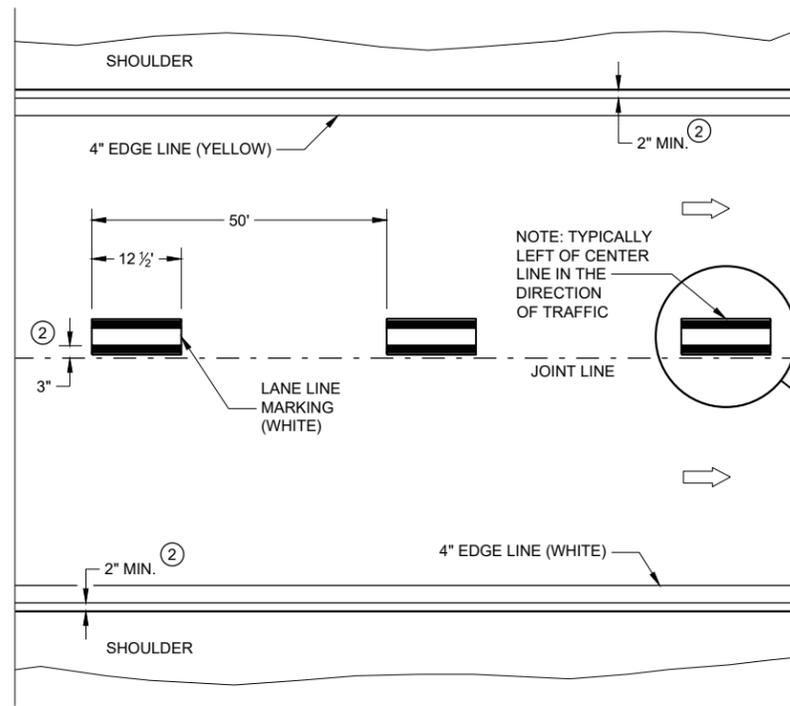
STATE OF WISCONSIN
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APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER

FHWA

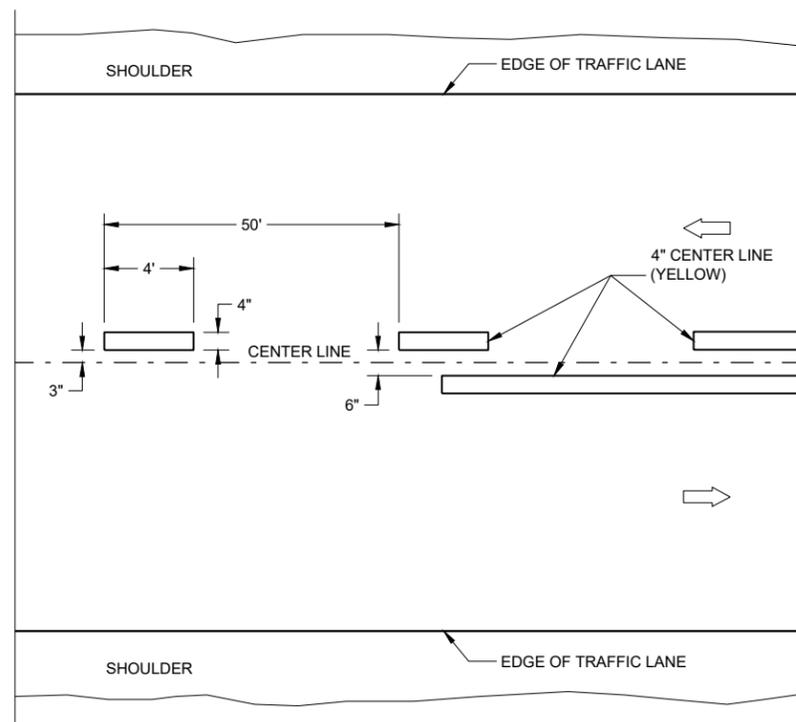


TWO WAY TRAFFIC

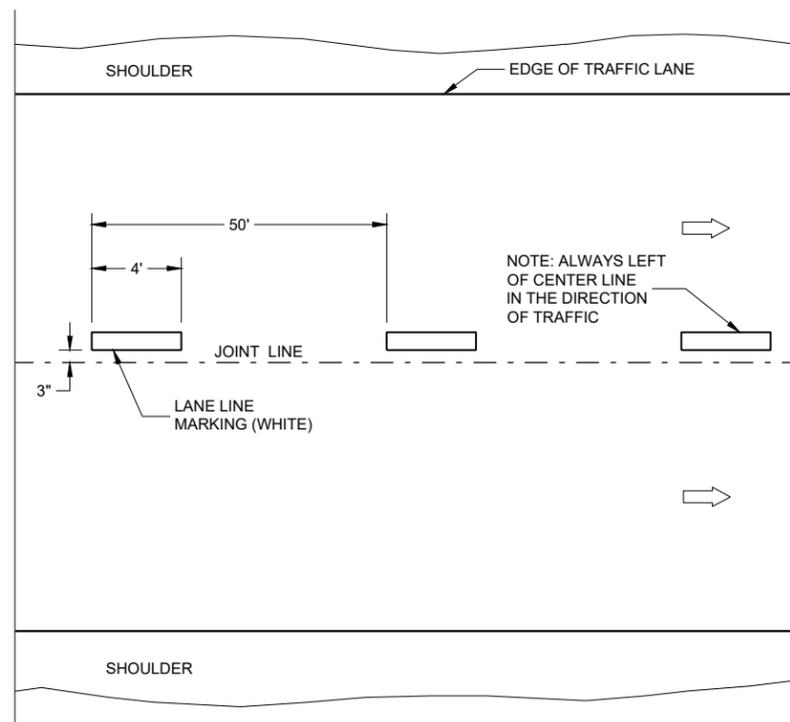


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

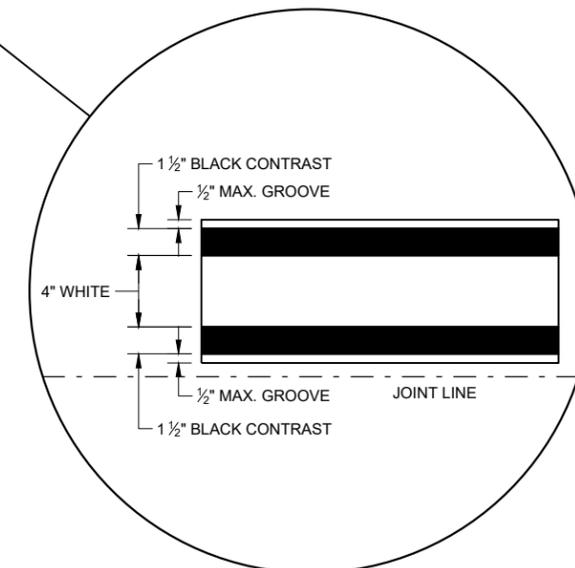
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

- |• "T" MARKING
- ⊙ SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC



LONGITUDINAL MARKING (MAINLINE)

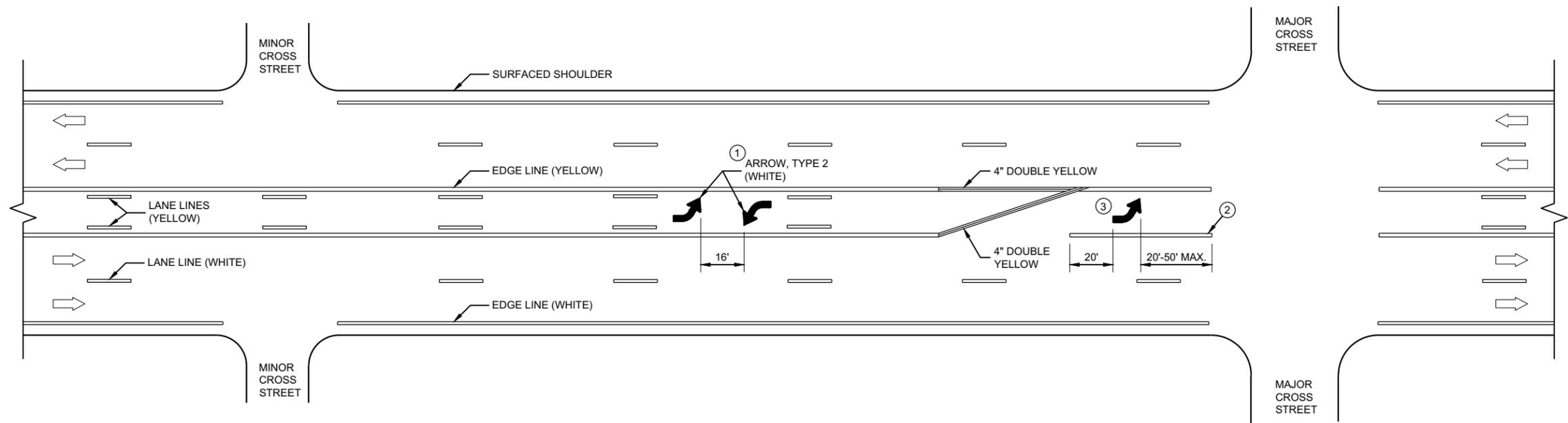
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Matthew Rauch
DATE STATEWIDE SIGNING AND MARKING
ENGINEER

GENERAL NOTES

- ① A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ② 8" WHITE
- ③ TURN BAY LENGTH OF LESS THAN 48' DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT.

➡ DIRECTION OF TRAFFIC



TWO WAY LEFT TURN LANE

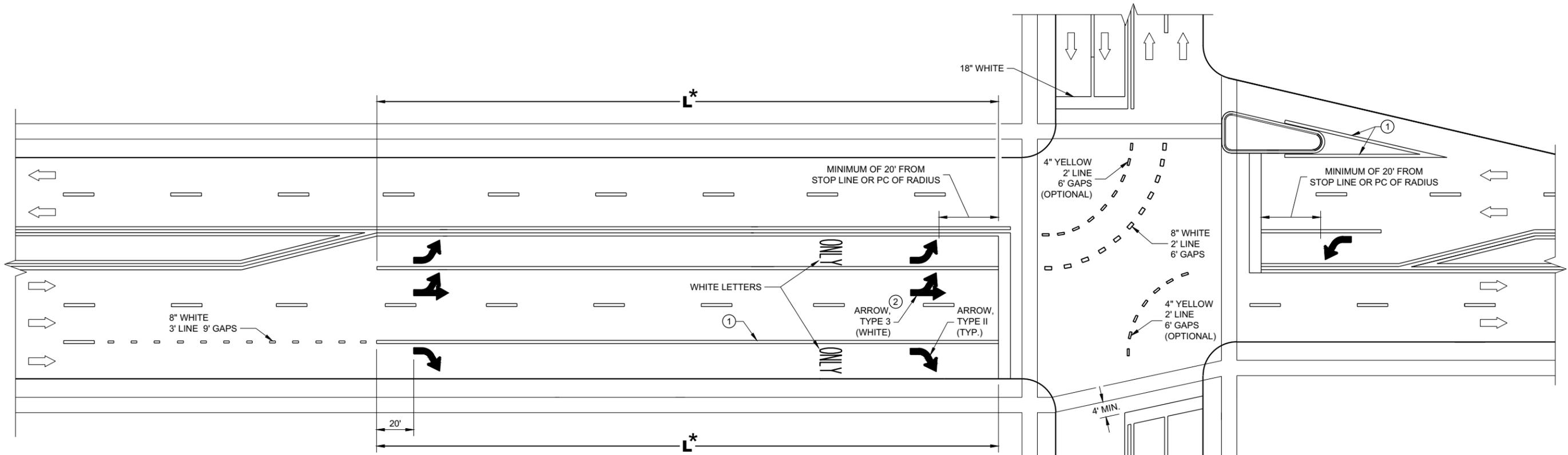
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SDD 15C08 - 20b

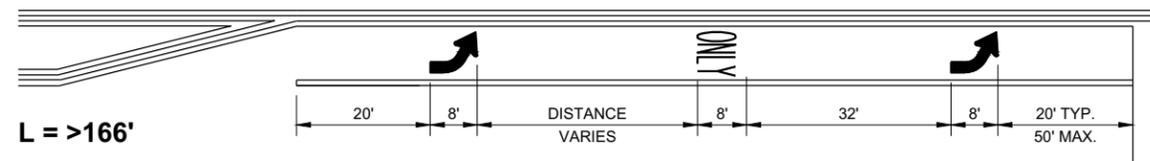
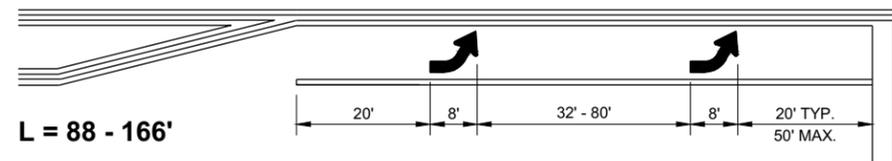
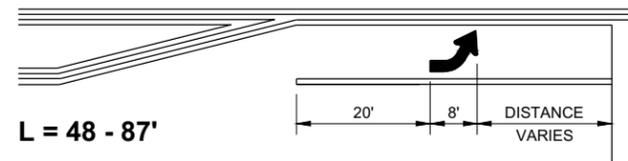
SDD 15C08 - 20b

<p>PAVEMENT MARKING (TURN LANES)</p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>



TURN LANE OPTIONS

LENGTH OF TURN BAY (L) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



*(SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

GENERAL NOTES

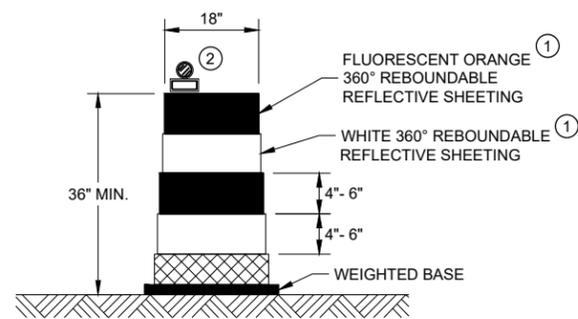
- ① 8" WHITE
- ② QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

➡ DIRECTION OF TRAFFIC

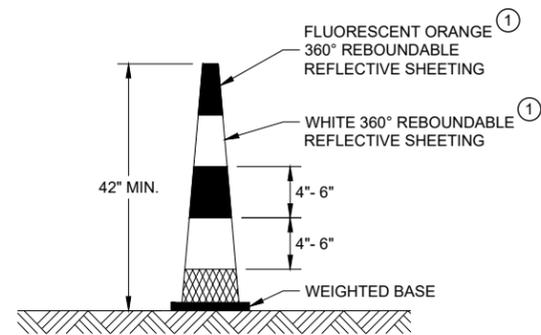
L = LENGTH OF TURN BAY

PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

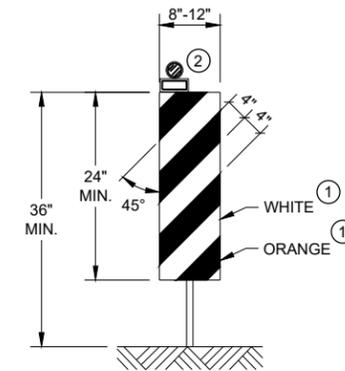


DRUM



42" CONE

DO NOT USE IN TAPERS
 1/2 SPACING OF DRUMS

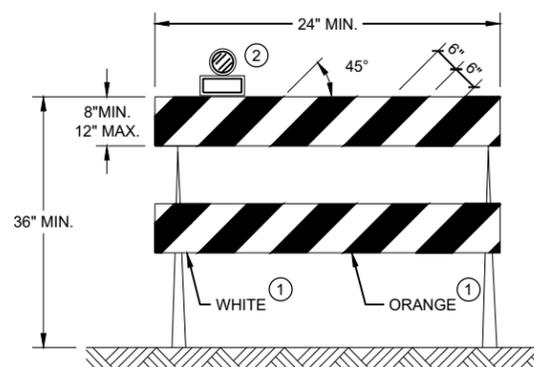


VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.

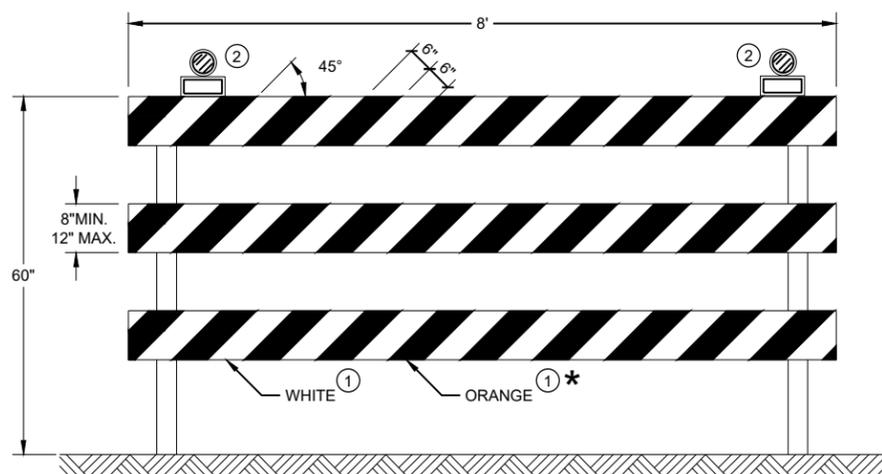
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2021 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
<small>FHWA</small>	

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

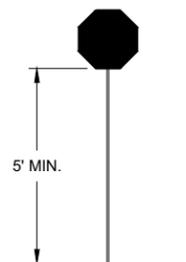
WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

- FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.
- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
 - ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

- UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.
- ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FOR THE APPROVED PRODUCTS LIST.
- INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.
- PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.
- DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



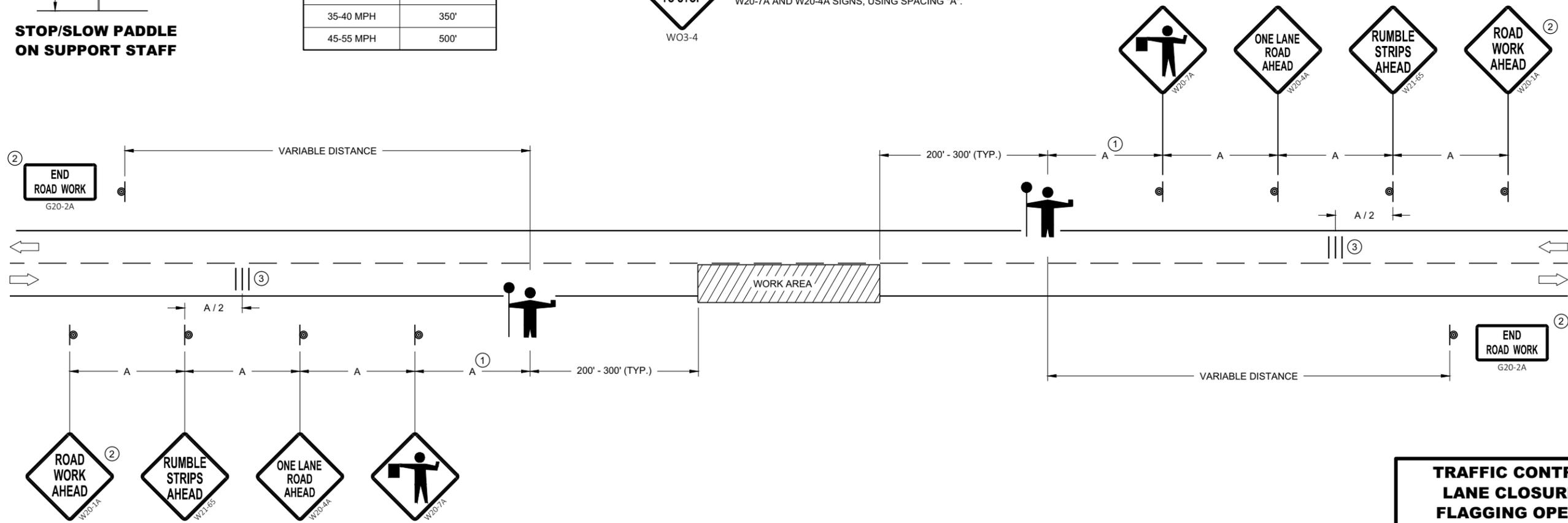
STOP/SLOW PADDLE ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: May 2019 /S/ Andrew Heidtke
WORK ZONE ENGINEER

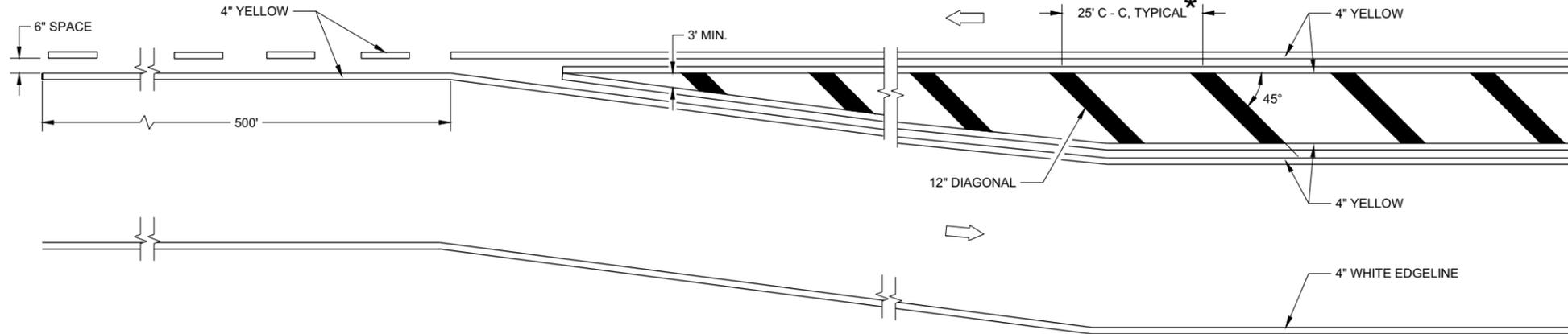
FHWA

GENERAL NOTES

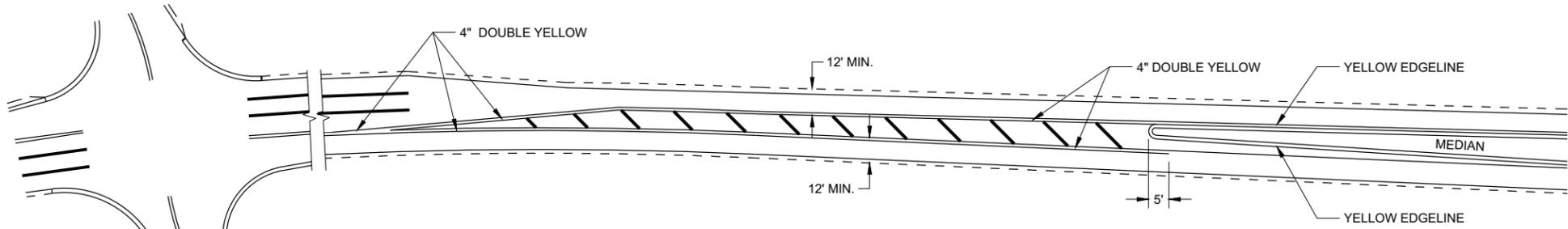
DIAGONALS ARE OPTIONAL WHEN PAINED ISLAND IS LESS THAN 6 FEET AT WIDEST POINT.

➡ DIRECTION OF TRAVEL

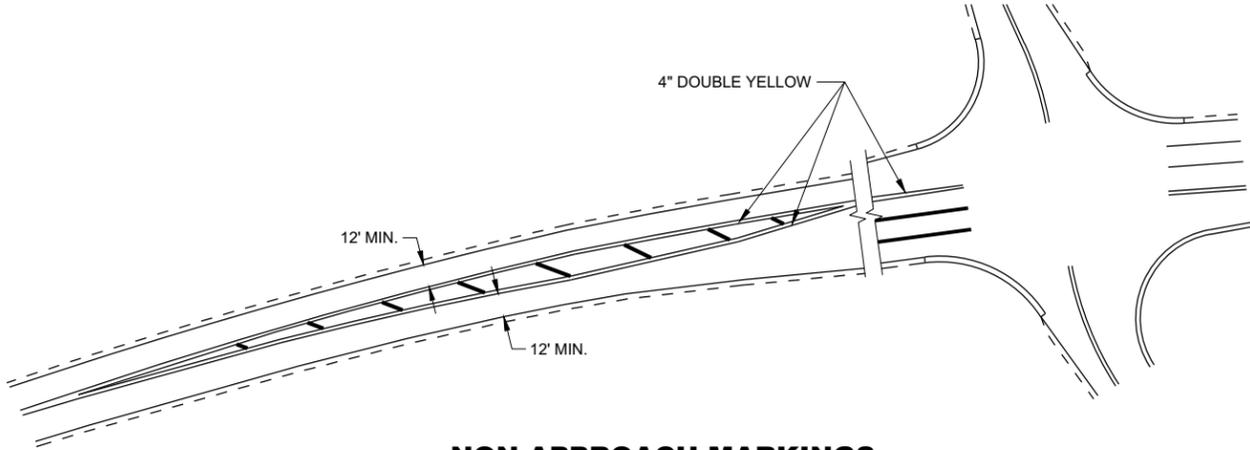
* WHEN THE PAINTED MEDIAN LENGTH IS LESS THAN 50 FEET THE SPACING IS 10'.



MEDIAN ISLAND DETAIL



APPROACH MARKINGS FOR OTHER MEDIAN TYPES



NON-APPROACH MARKINGS

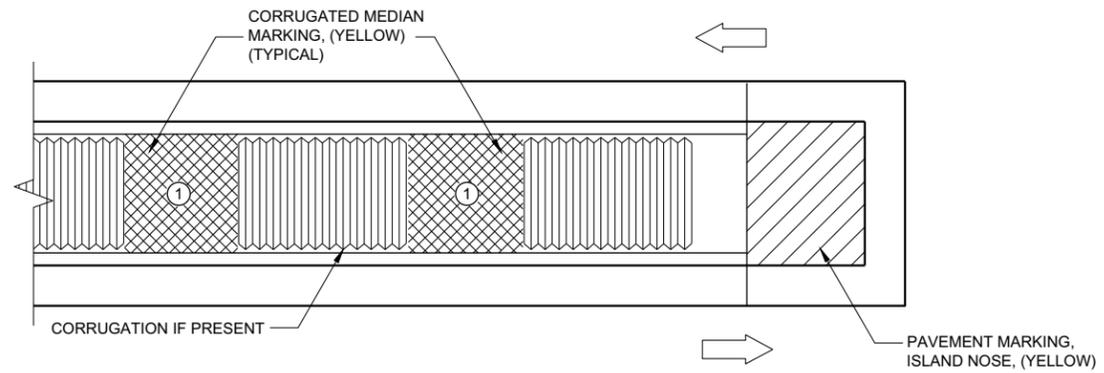
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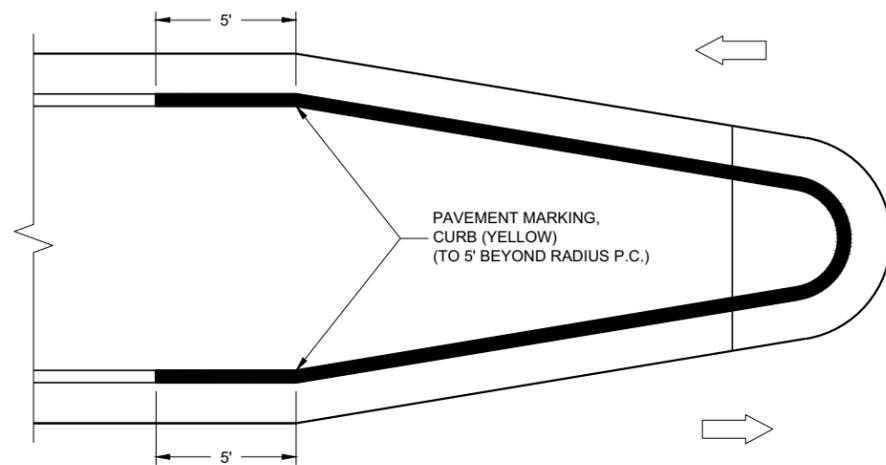
SDD 15C18 - 05a

SDD 15C18 - 05a

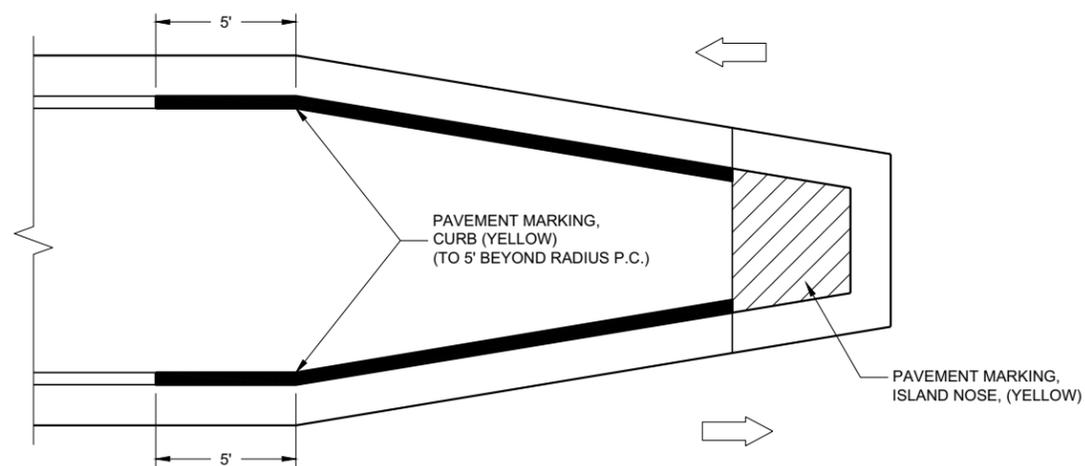
MEDIAN ISLAND PAVEMENT MARKINGS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2021 DATE	/S/ Matthew R. Rauch STATE SIGNING AND MARKING ENGINEER
FHWA	



MEDIAN ISLAND WITH SQUARE BLUNT NOSE



MEDIAN ISLAND WITH ROUND BLUNT NOSE



MEDIAN ISLAND WITH SLOPED NOSE

TYPICAL PLACEMENT OF PAVEMENT MARKING ON MEDIAN ISLANDS

GENERAL NOTES

WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION, YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN. THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.

-  ISLAND NOSE MARKING
-  CURB MARKING
-  CORRUGATED MEDIAN MARKING
-  DIRECTION OF TRAVEL

6

6

SDD 15C18 - 05b

SDD 15C18 - 05b

PAVEMENT MARKINGS, MEDIAN ISLAND NOSE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2021 DATE	/S/ Matthew R. Rauch STATE SIGNING AND MARKING ENGINEER
<small>FHWA</small>	

LEGEND

- V1 LEAD VEHICLE
- V2 MARKING VEHICLE
- V3 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (CAUTION)

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

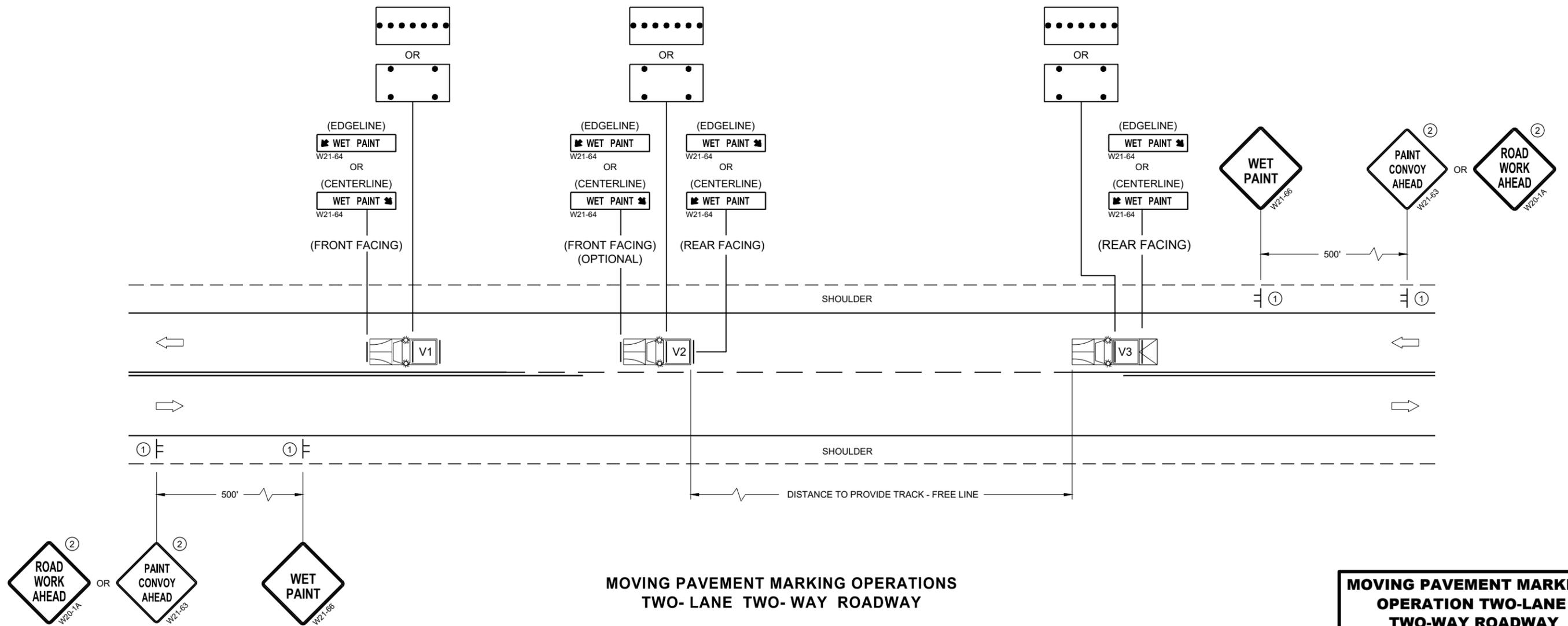
CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING.

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.

6

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**MOVING PAVEMENT MARKING OPERATIONS
TWO-LANE TWO-WAY ROADWAY**

SDD 15C19 - 06a

SDD 15C19 - 06a

MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2019 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

- V1 LEAD VEHICLE
- V2 MARKING VEHICLE
- V3 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (MERGE)
-  FLASHING ARROW PANEL (CAUTION)

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

WHEN WORK ACTIVITY BLOCKS THE LEFT LANE, REVERSE TRAFFIC CONTROL.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

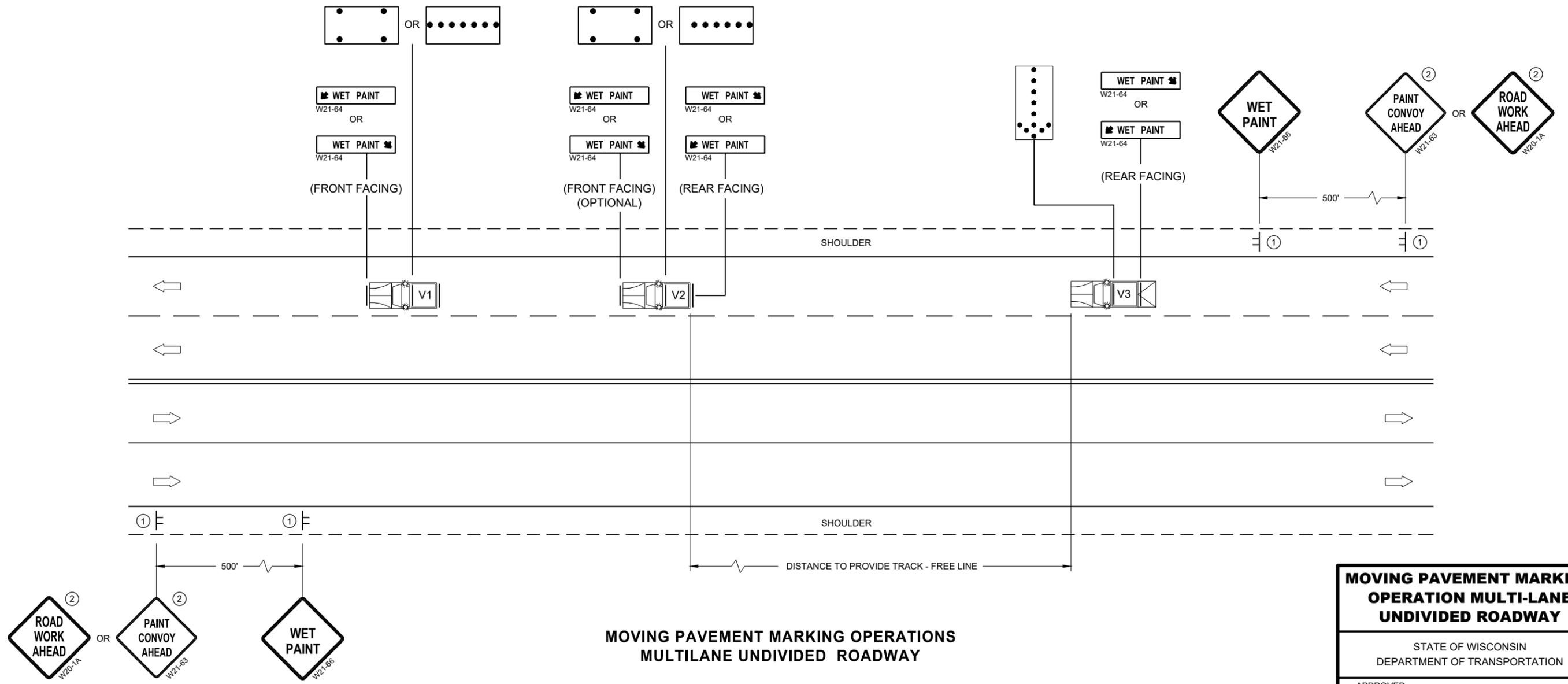
CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLES AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL HAVE A MINIMUM HEIGHT OF 28" FOR WET PAVEMENT MARKINGS.

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.

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SDD 15C19 - 06b

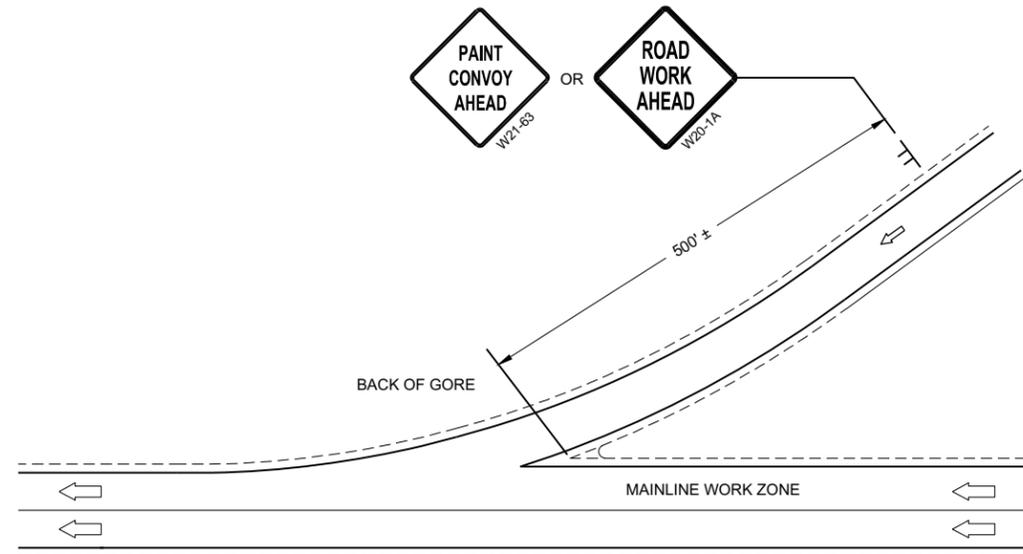
SDD 15C19 - 06b

**MOVING PAVEMENT MARKING OPERATIONS
MULTILANE UNDIVIDED ROADWAY**

MOVING PAVEMENT MARKING OPERATION MULTI-LANE UNDIVIDED ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2019 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

- V1 MARKING VEHICLE
- V2 SHADOW VEHICLE
- V3 TRAIL VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (MERGE)
-  FLASHING ARROW PANEL (CAUTION)



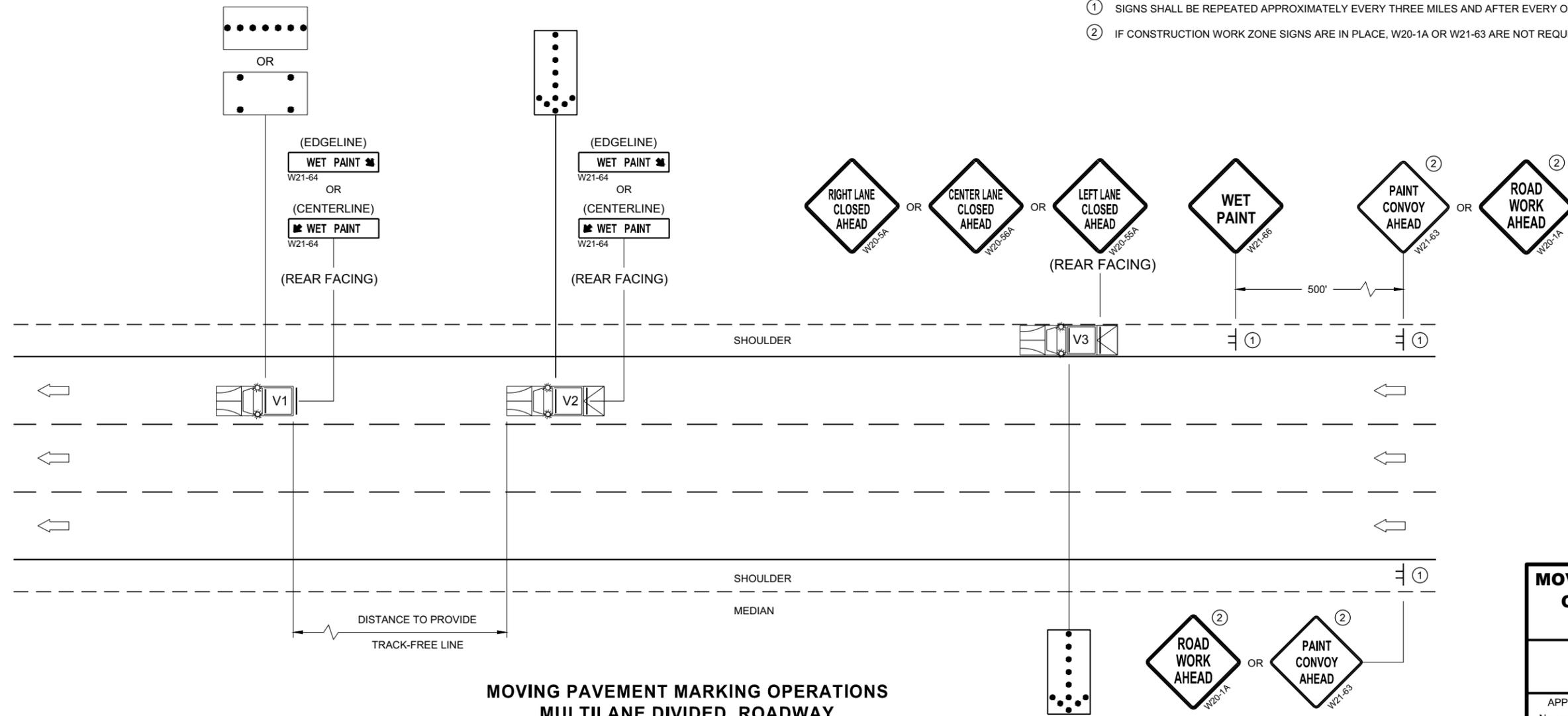
GENERAL NOTES

- ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.
- ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.
- DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- WHEN WORK ACTIVITY BLOCKS THE LEFT LANE, REVERSE TRAFFIC CONTROL.
- WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.
- USE AN ATTENUATOR ON THE REAR MOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.
- IF THE SHOULDER IS TOO NARROW TO ACCOMMODATE THE LAST TRAILING VEHICLE, THE VEHICLE SHOULD STRADDLE THE EDGE LINE.
- WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC
- CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.
- CONES SHALL BE A MINIMUM HEIGHT OF 18" FOR WET PAVEMENT MARKINGS

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES AND AFTER EVERY ON RAMP.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.

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SDD 15C19 - 06C

SDD 15C19 - 06C

**MOVING PAVEMENT MARKING OPERATIONS
MULTILANE DIVIDED ROADWAY**

**MOVING PAVEMENT MARKING
OPERATION MULTI-LANE
DIVIDED ROADWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

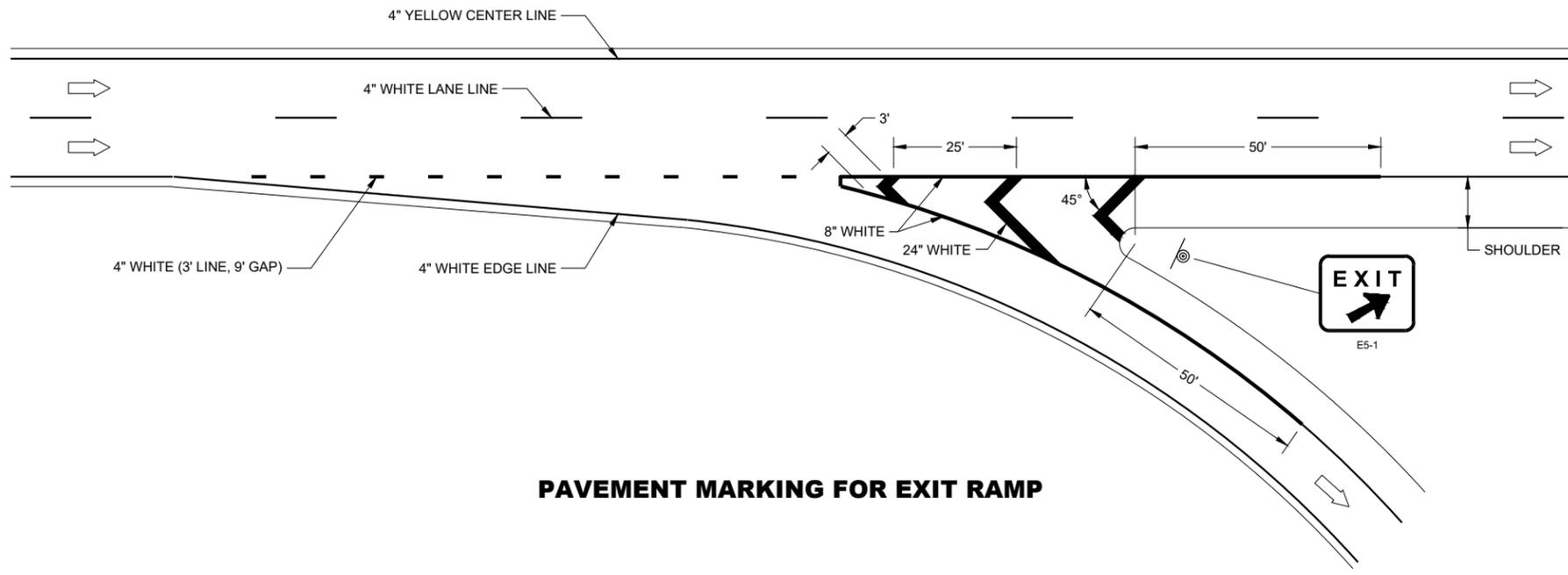
FHWA

GENERAL NOTES

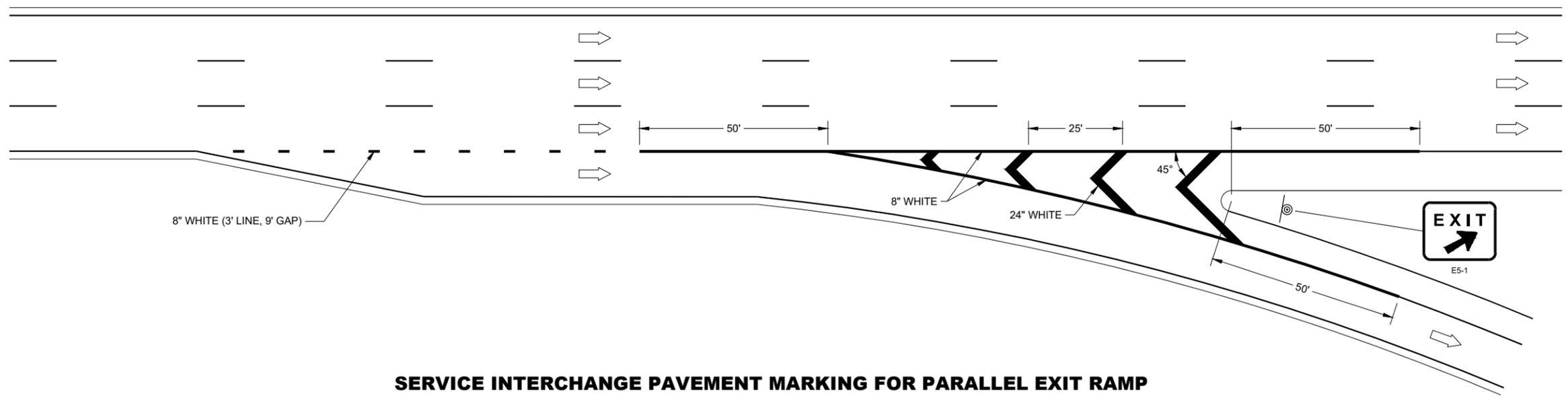
PLACE GROOVE 3 INCHES LEFT OF JOINT.

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAVEL



PAVEMENT MARKING FOR EXIT RAMP



SERVICE INTERCHANGE PAVEMENT MARKING FOR PARALLEL EXIT RAMP

**PAVEMENT MARKING,
EXIT RAMP AND
PARALLEL EXIT RAMP**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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SDD 15C31 - 04a

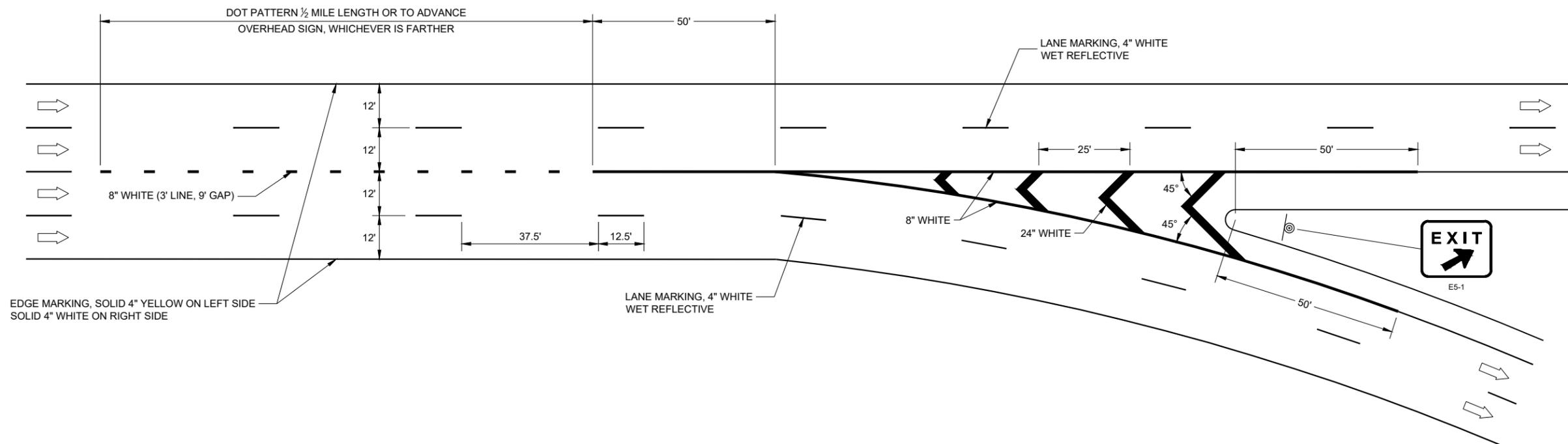
SDD 15C31 - 04a

GENERAL NOTES

PLACE GROOVE 3 INCHES LEFT OF JOINT.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAVEL



6

6

SDD 15C31 - 04b

SDD 15C31 - 04b

**PAVEMENT MARKING,
MAJOR SPLIT
FREEWAY TO FREEWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

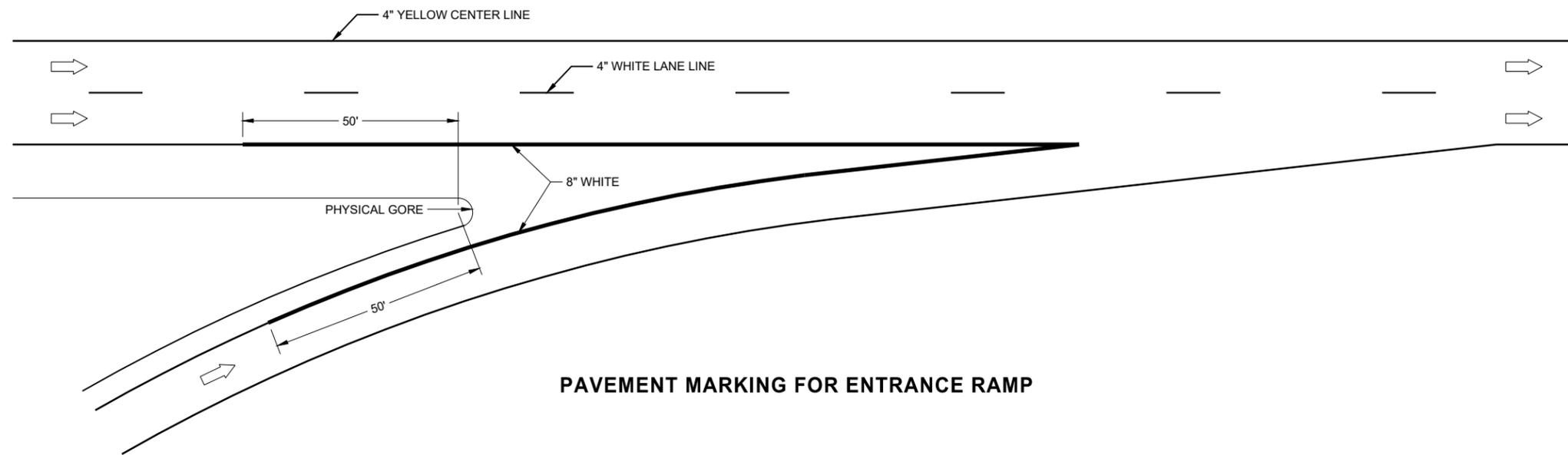
GENERAL NOTES

PLACE GROOVE 3 INCHES LEFT OF JOINT.

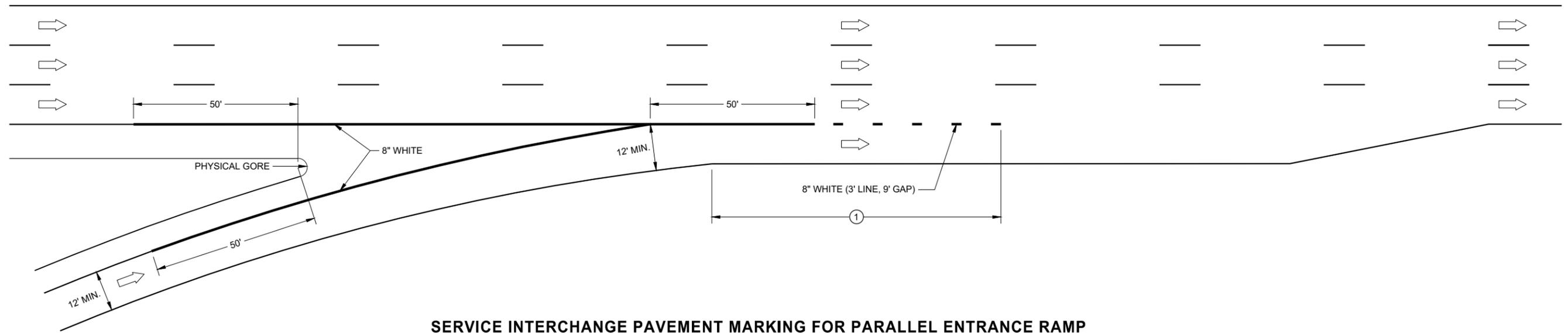
① ½ LENGTH OF FULL WIDTH ACCELERATION LANE.

LEGEND

➡ DIRECTION OF TRAVEL



PAVEMENT MARKING FOR ENTRANCE RAMP



SERVICE INTERCHANGE PAVEMENT MARKING FOR PARALLEL ENTRANCE RAMP

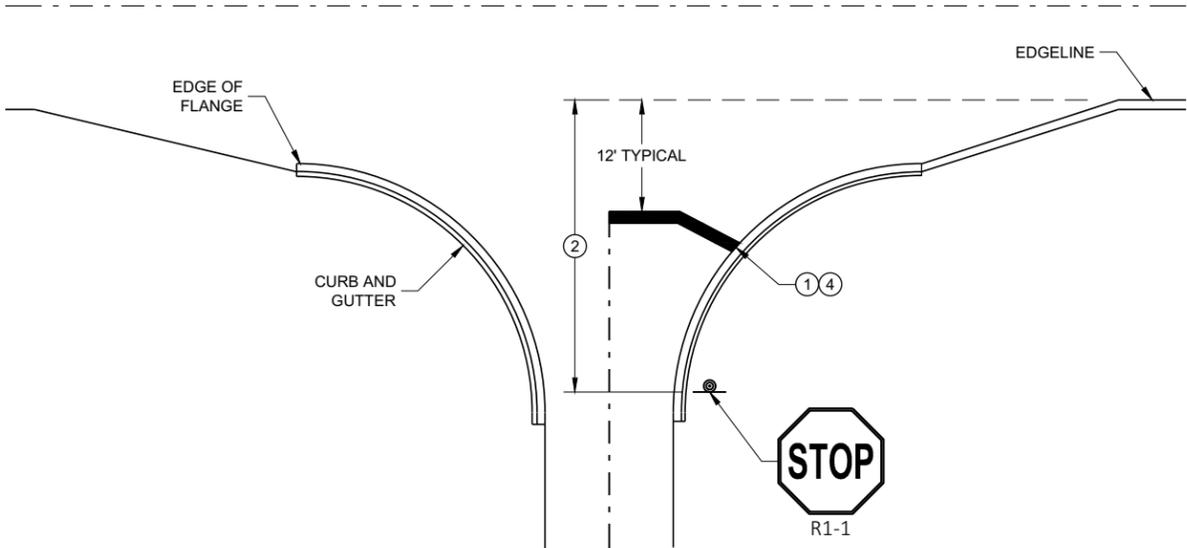
**PAVEMENT MARKING,
ENTRANCE RAMP AND
PARALLEL ENTRANCE RAMP**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

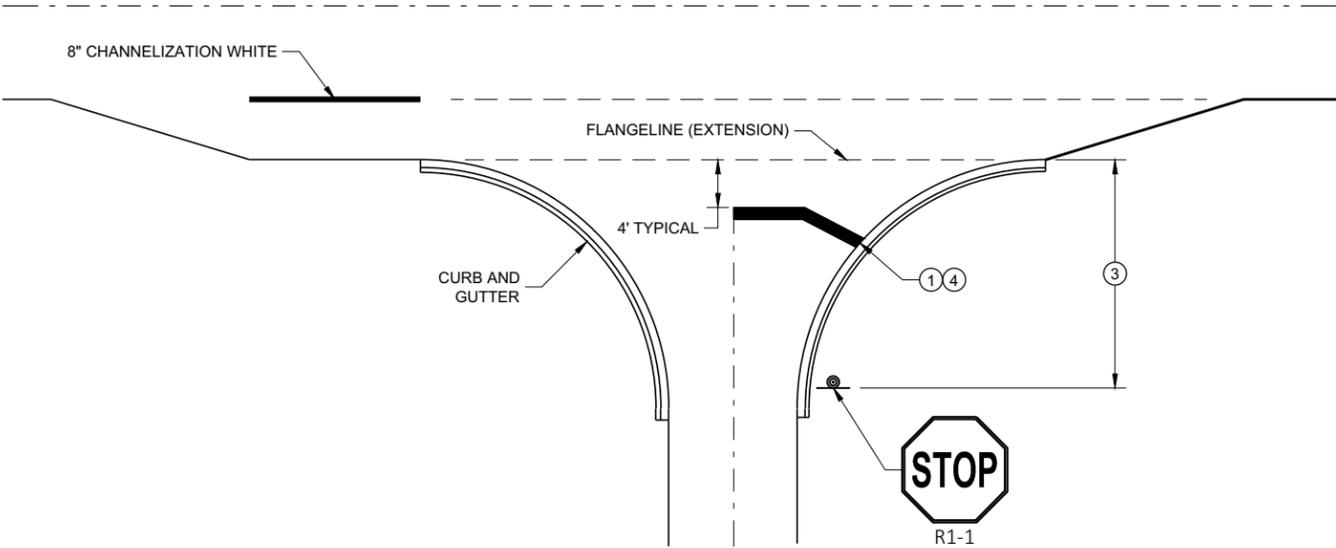
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

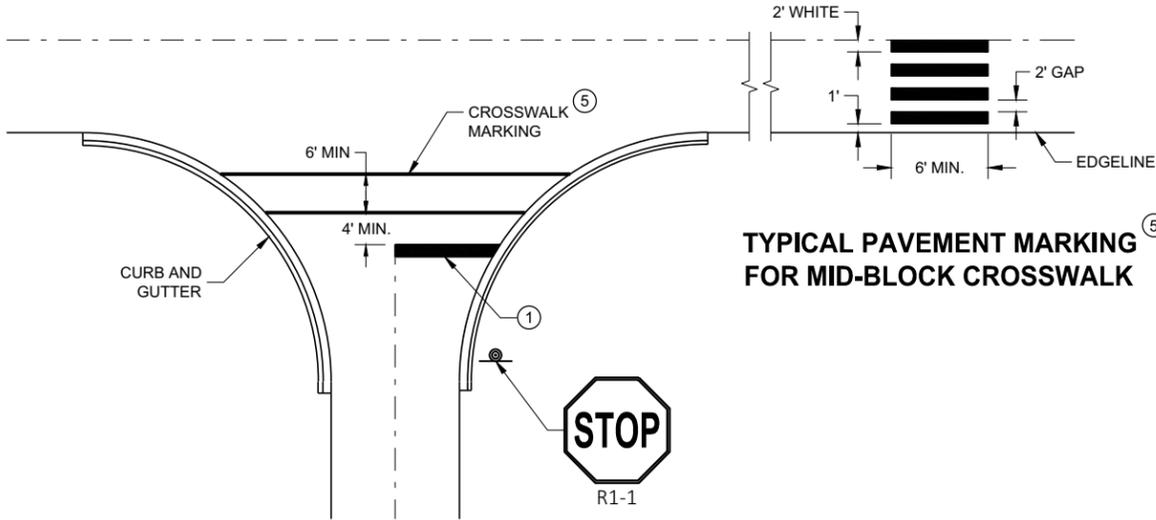
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

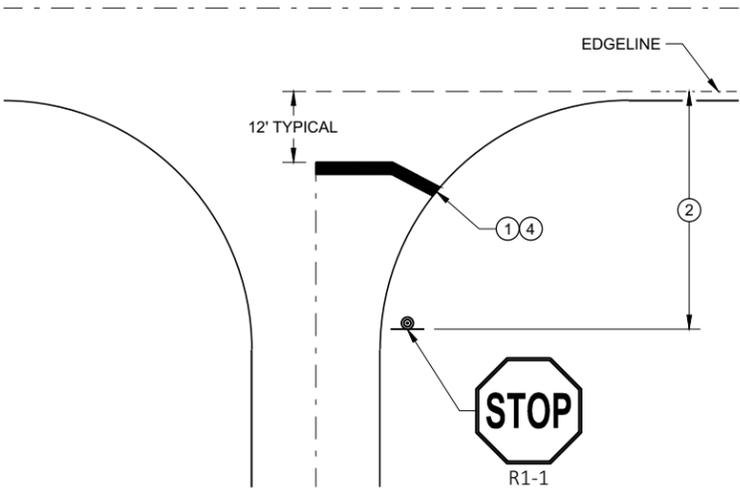


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING

TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER

FHWA

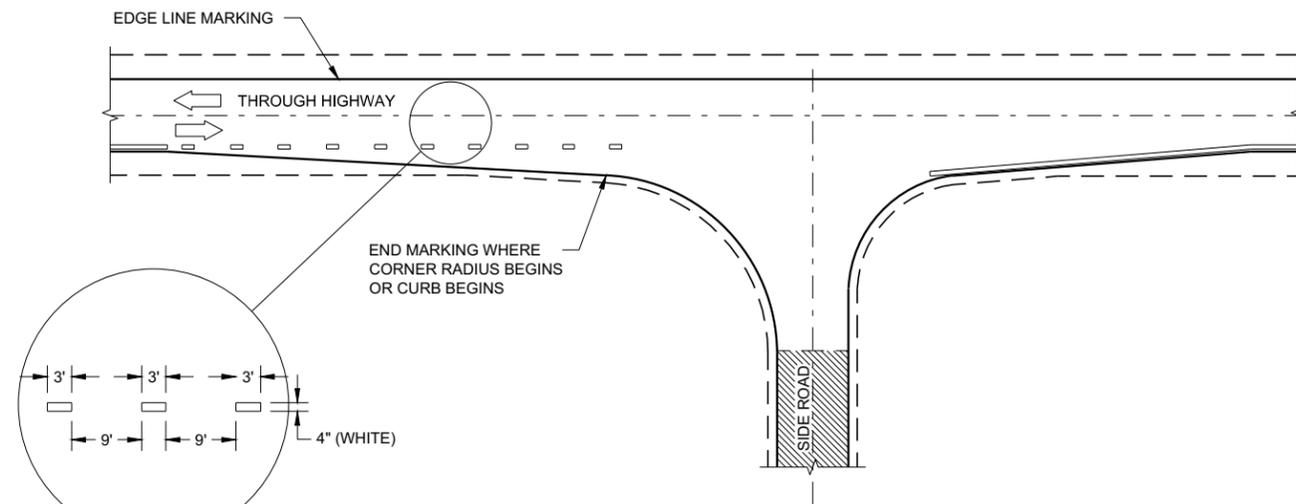
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

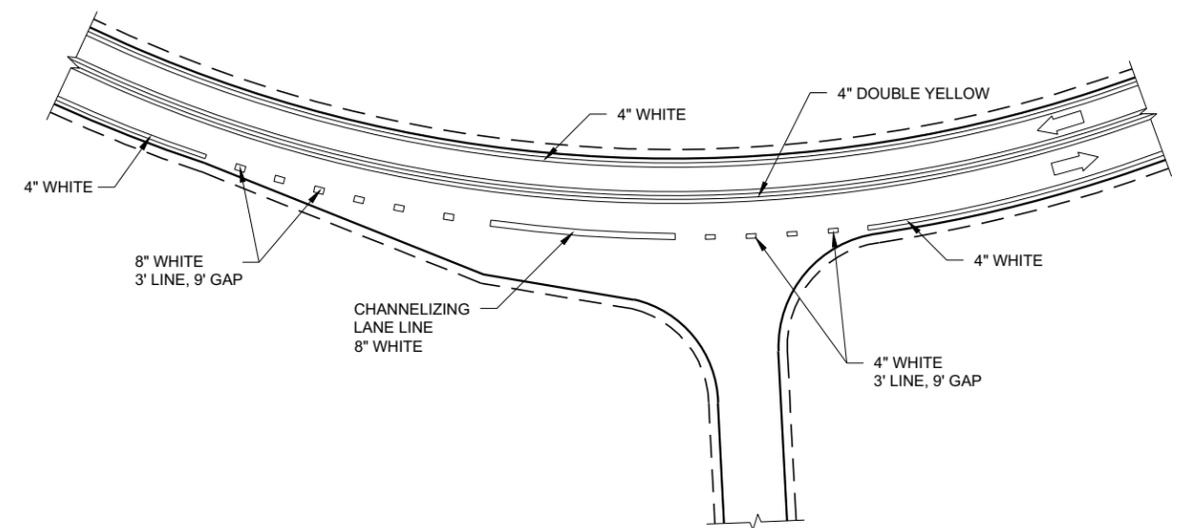
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.

LEGEND

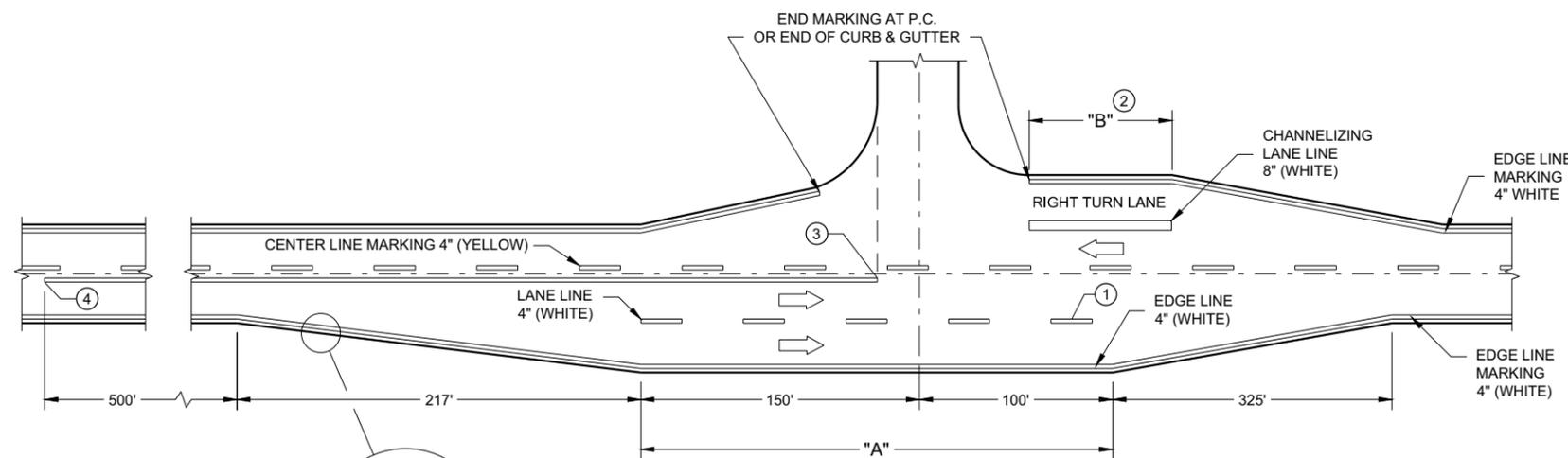
➡ DIRECTION OF TRAVEL



MINOR INTERSECTION



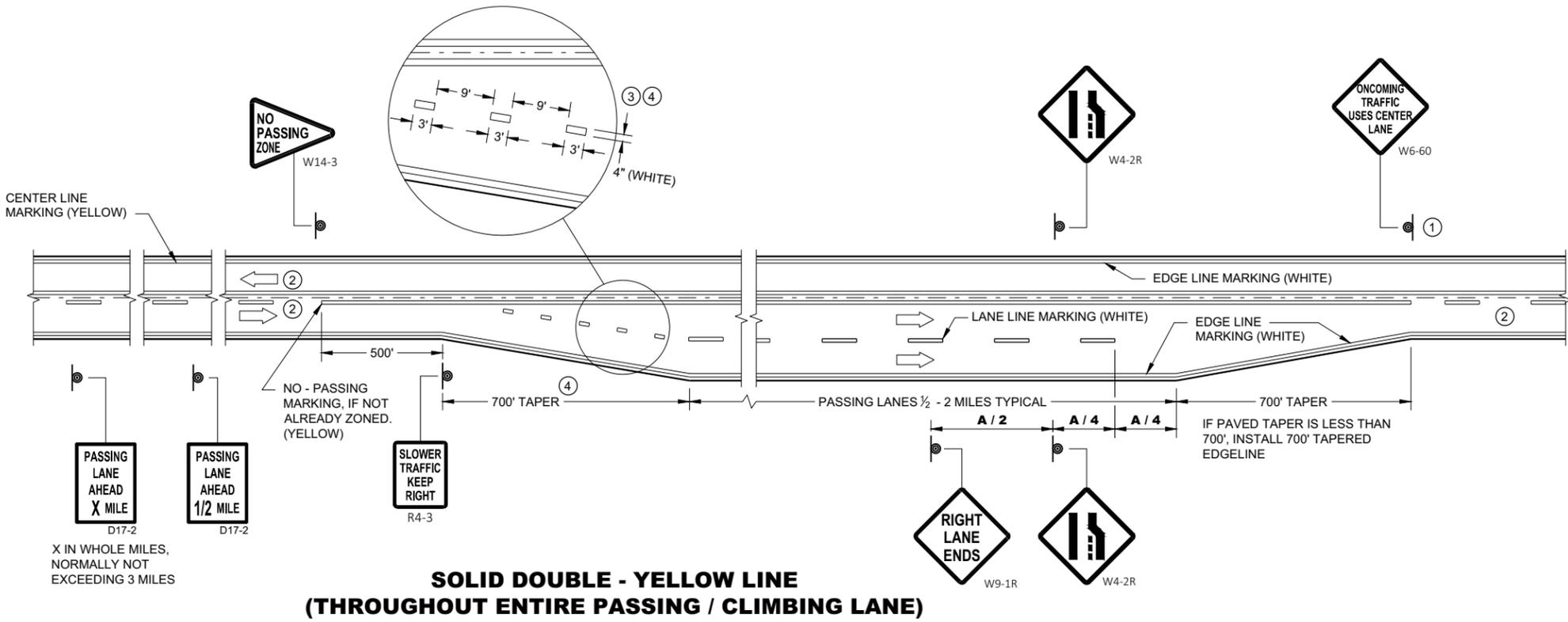
INTERSECTION ON OUTSIDE OF CURVE



**MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**

**PAVEMENT MARKING
(INTERSECTIONS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



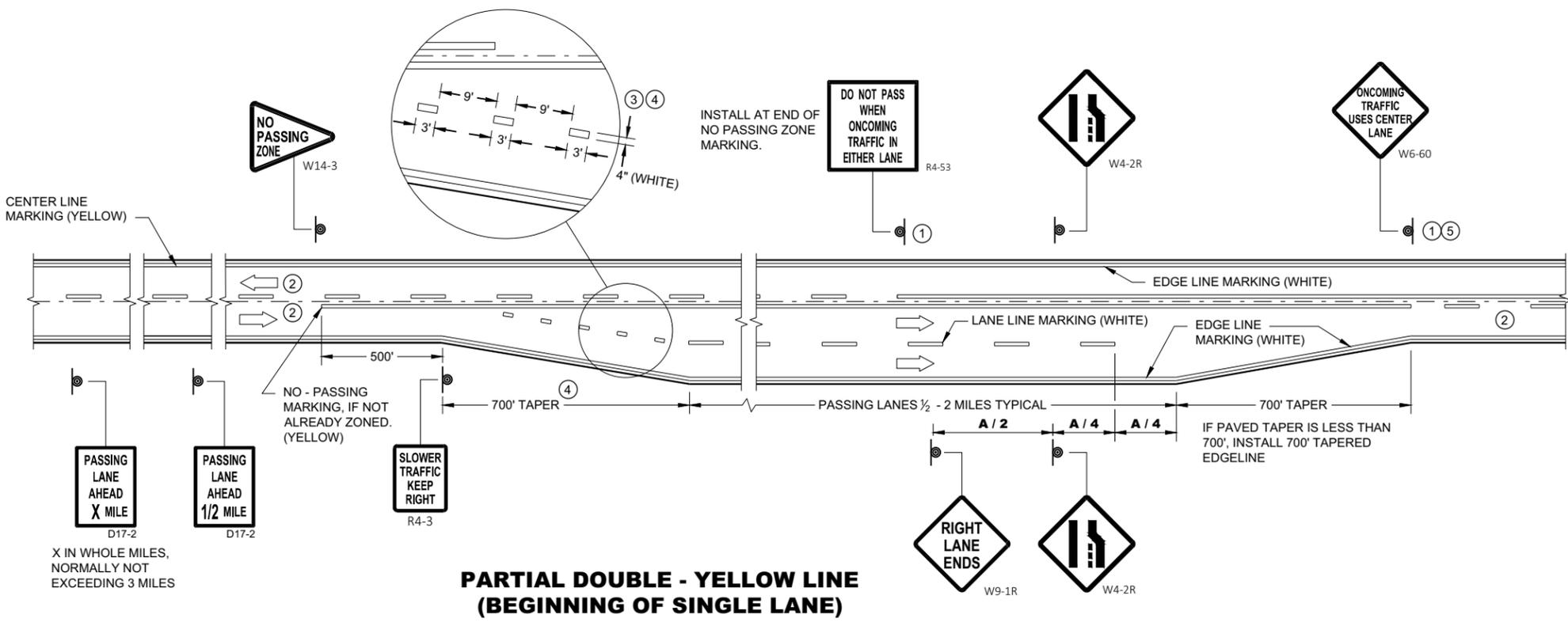
GENERAL NOTES

- ① SIGN SHALL BE REPEATED AT 1 MILE INCREMENTS OR AT THE DISCRETION OF THE REGIONAL TRAFFIC ENGINEER.
- ② THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
- ③ THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3' LINE, 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- ④ WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING / CLIMBING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.
- ⑤ REPEAT EVERY 1 MILE UP UNTIL R4-53.

ARROW SYMBOL () SHOWS DIRECTION OF TRAVEL

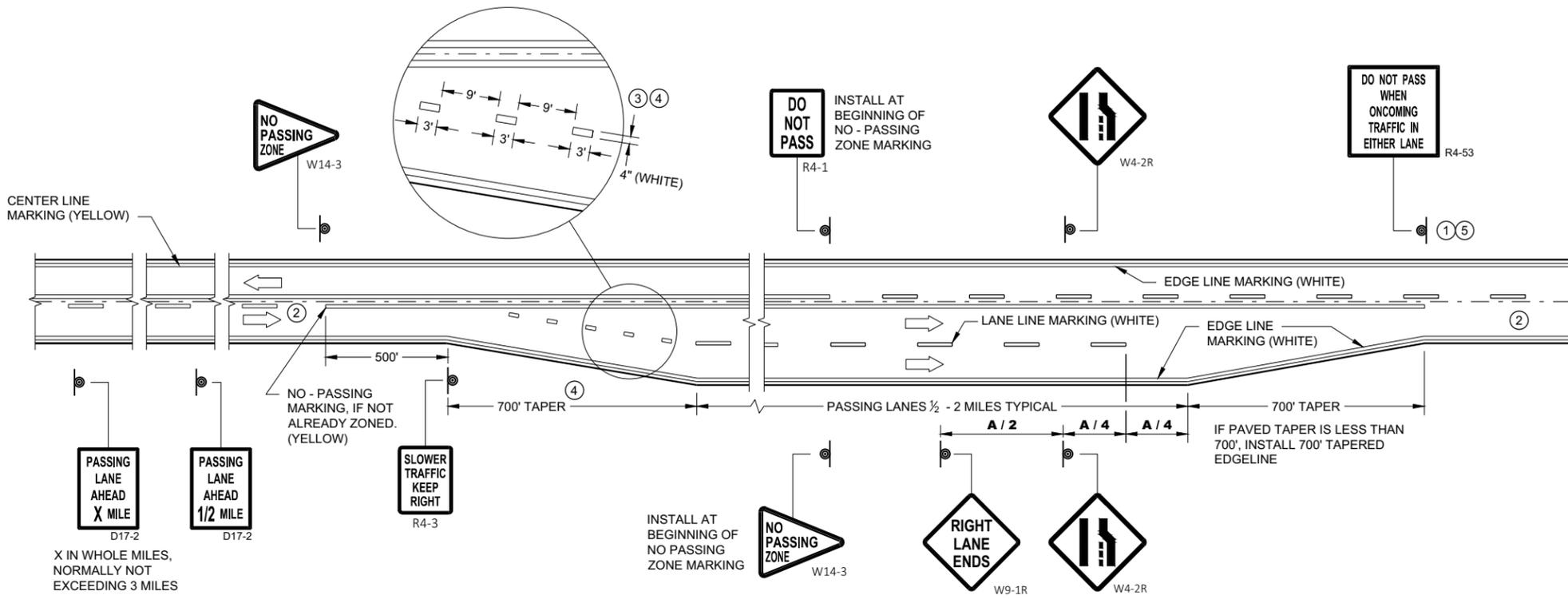
DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
45	775
50	885
55	990

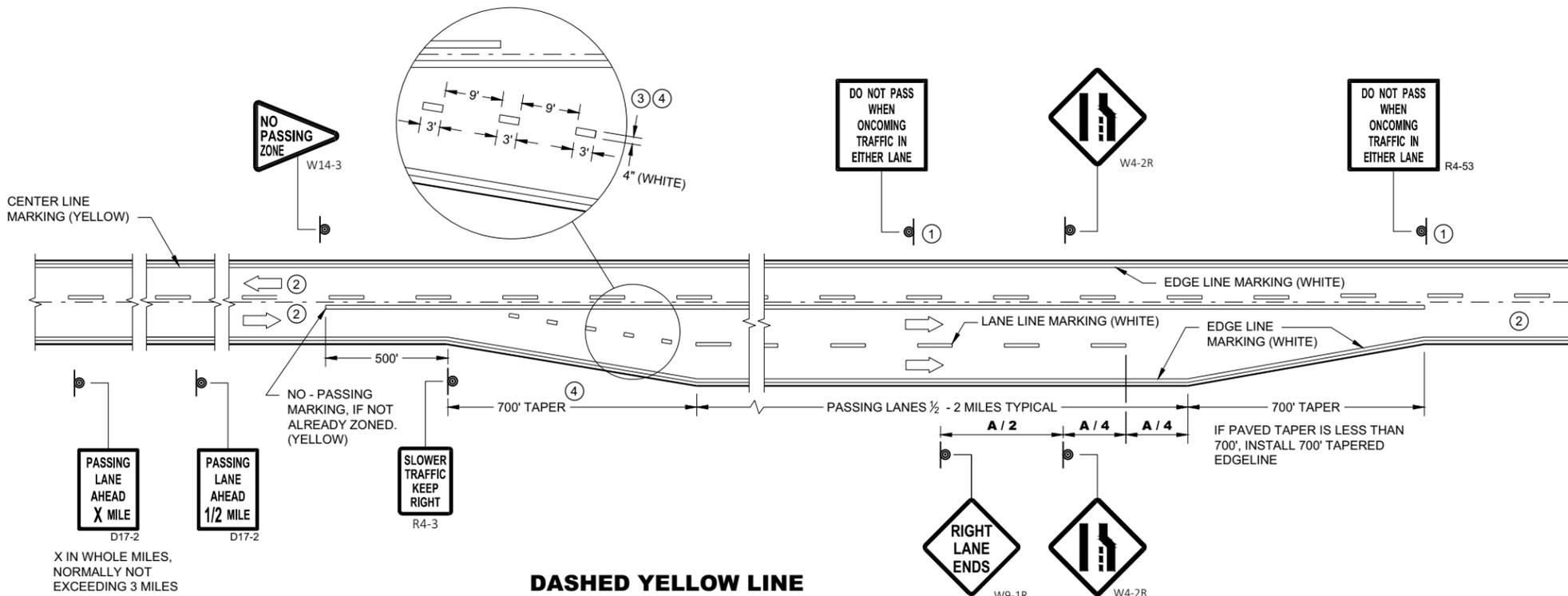


**PAVEMENT MARKING & SIGNING
(CLIMBING LANE & PASSING LANE)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**SOLID DOUBLE - YELLOW LINE
(END OF SINGLE LANE)**



**DASHED YELLOW LINE
(THROUGHOUT SINGLE LANE)**

GENERAL NOTES

- ① SIGN SHALL BE REPEATED AT 1 MILE INCREMENTS OR AT THE DISCRETION OF THE REGIONAL TRAFFIC ENGINEER.
- ② THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
- ③ THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3' LINE, 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- ④ WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING / CLIMBING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.
- ⑤ REPEAT EVERY ONE MILE UP UNTIL NO PASSING ZONE.

ARROW SYMBOL () SHOWS DIRECTION OF TRAVEL

DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
45	775
50	885
55	990

**PAVEMNET MARKING & SIGNING
(CLIMBING LANE & PASSING LANE)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER

FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS

NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

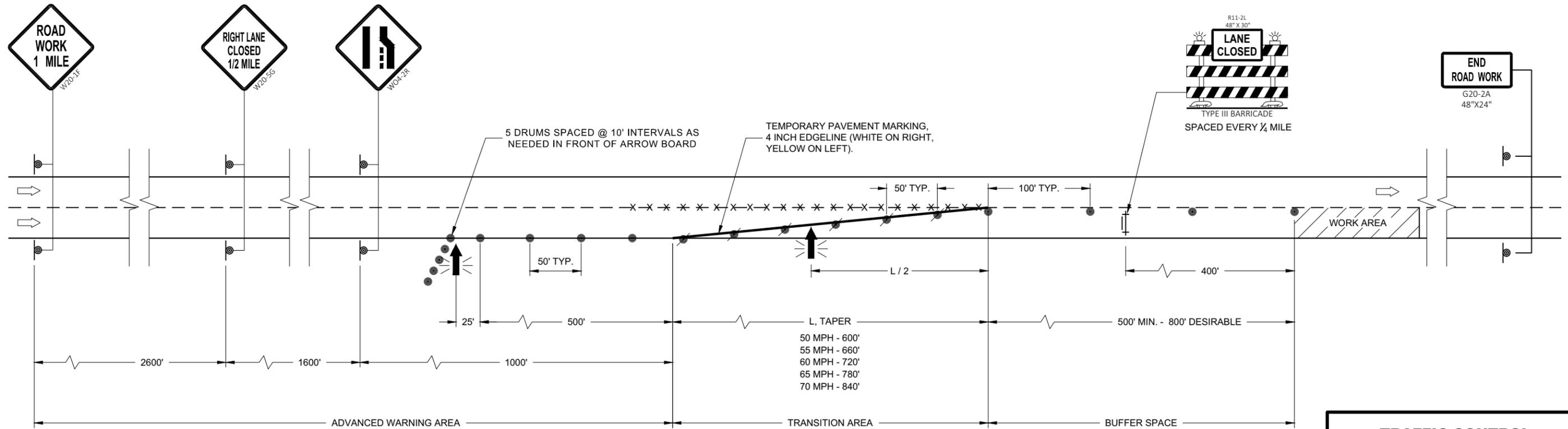
CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  REMOVING PAVEMENT MARKINGS
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLASHING ARROW BOARD

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SDD 15D12 - 09a



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SDD 15D12 - 09a

TRAFFIC CONTROL LANE CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED August 2020 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  FLASHING ARROW BOARD
-  DIRECTION OF TRAFFIC
-  REMOVE PAVEMENT MARKING (SEE GENERAL NOTES)
-  WORK AREA

GENERAL NOTES

FOR WORK ON ROADWAYS WITH SPEEDS GREATER THAN 45MPH, USE SDD 15D12.

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

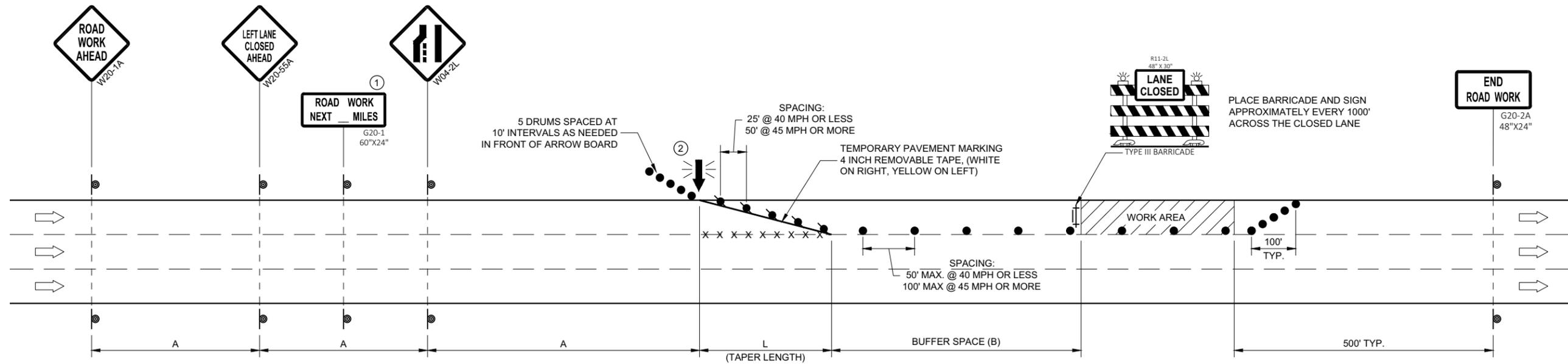
CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

- ① OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.
- ② WHERE THE SHOULDER OR TERRACE HAS INSUFFICIENT SPACE TO PLACE THE ARROW BOARD AS SHOWN, PLACE THE ARROW BOARD AT THE END OF THE TAPER.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'

TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2020 /S/ Andrew Heidtke
DATE STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

FHWA

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  FLASHING ARROW BOARD
-  DIRECTION OF TRAFFIC
-  REMOVE PAVEMENT MARKING (SEE GENERAL NOTES)
-  WORK AREA

GENERAL NOTES

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

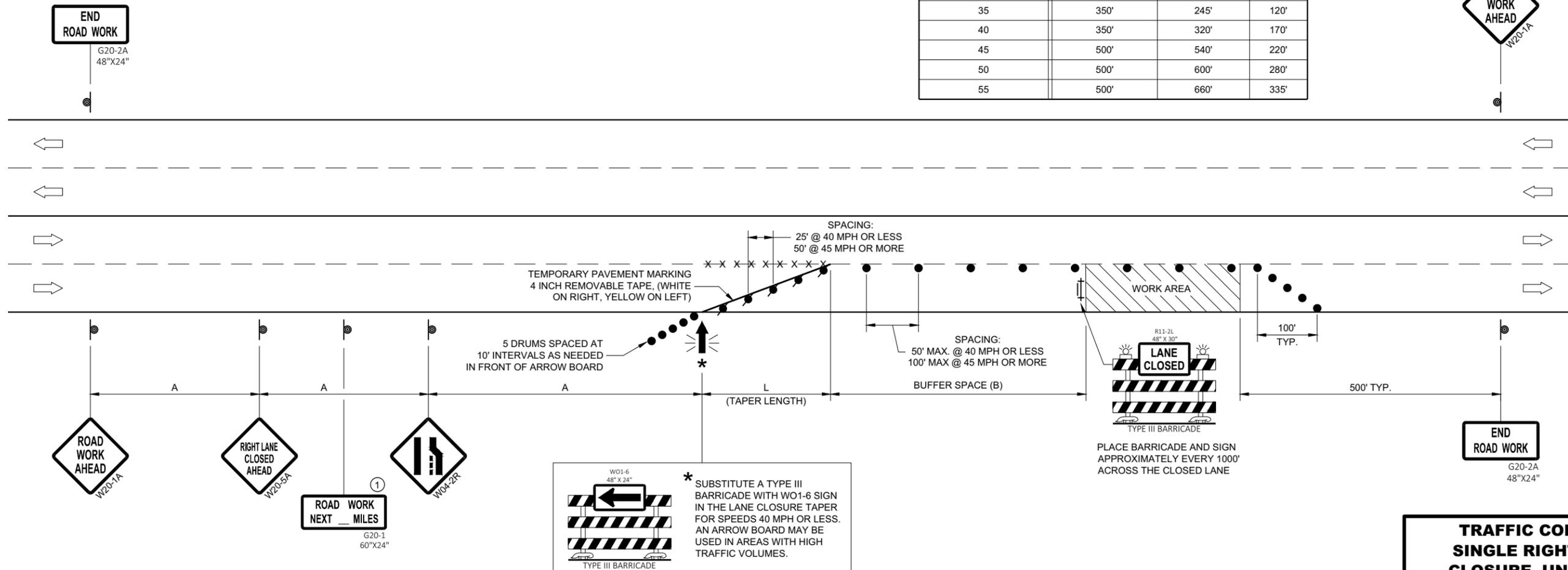
① OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'
50	500'	600'	280'
55	500'	660'	335'



6

6



SDD 15D20 - 05b

SDD 15D20 - 05b

**TRAFFIC CONTROL,
SINGLE RIGHT LANE
CLOSURE, UNDIVIDED
NON-FREEWAY/EXPRESSWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2020 /S/ Andrew Heidtke
DATE STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

FHWA

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  FLASHING ARROW BOARD
-  DIRECTION OF TRAFFIC
-  REMOVE PAVEMENT MARKING (SEE GENERAL NOTES)
-  WORK AREA

GENERAL NOTES

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

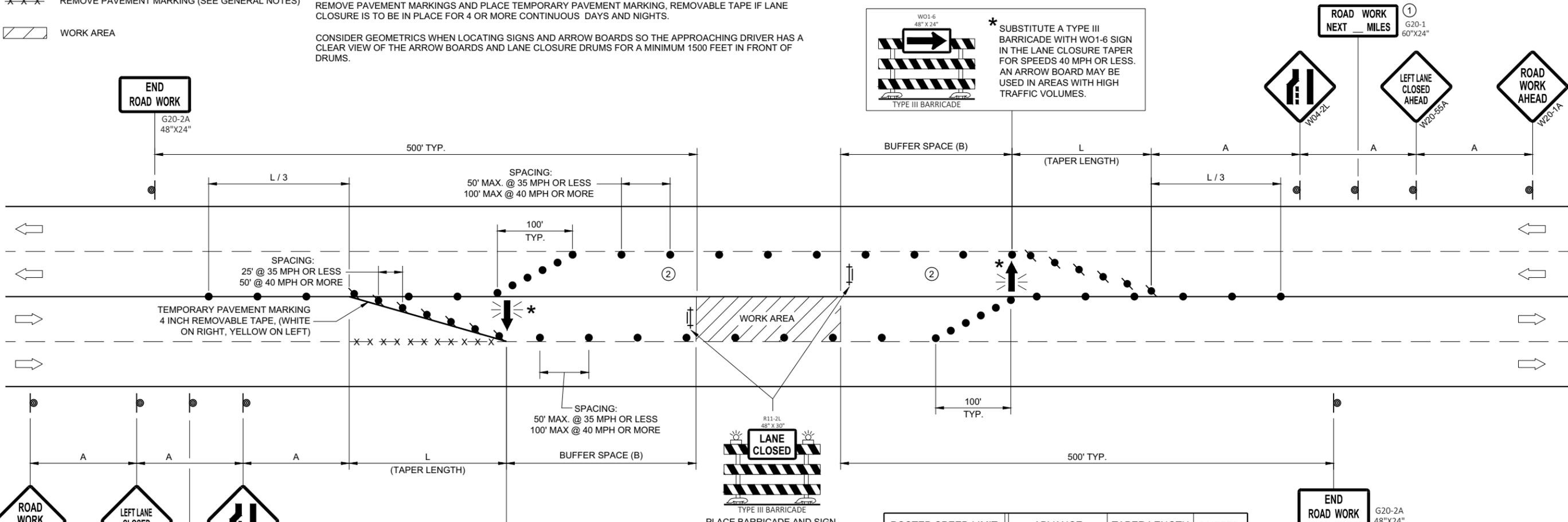
DUE TO LACK OF SHOULDER/MEDIAN, ARROW BOARD IS PLACED AT THE THE END OF THE TAPER.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

- ① OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.
- ② LANE MAY BE OPENED WHEN WORKERS ARE NOT PRESENT IN THE WORK AREA.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'
50	500'	600'	280'
55	500'	660'	335'

**TRAFFIC CONTROL,
SINGLE LEFT LANE
CLOSURE, UNDIVIDED
NON-FREEWAY/EXPRESSWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: May 2020 /S/ Andrew Heidtke
STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

FHWA

SDD 15D20 - 05C

SDD 15D20 - 05C

GENERAL NOTES

THIS DETAIL IS TYPICAL FOR CLOSING THE RIGHT SHOULDER. FOR CLOSING THE LEFT SHOULDER, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR DIVIDED ROADWAYS WITH ANY NUMBER OF TRAVEL LANES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

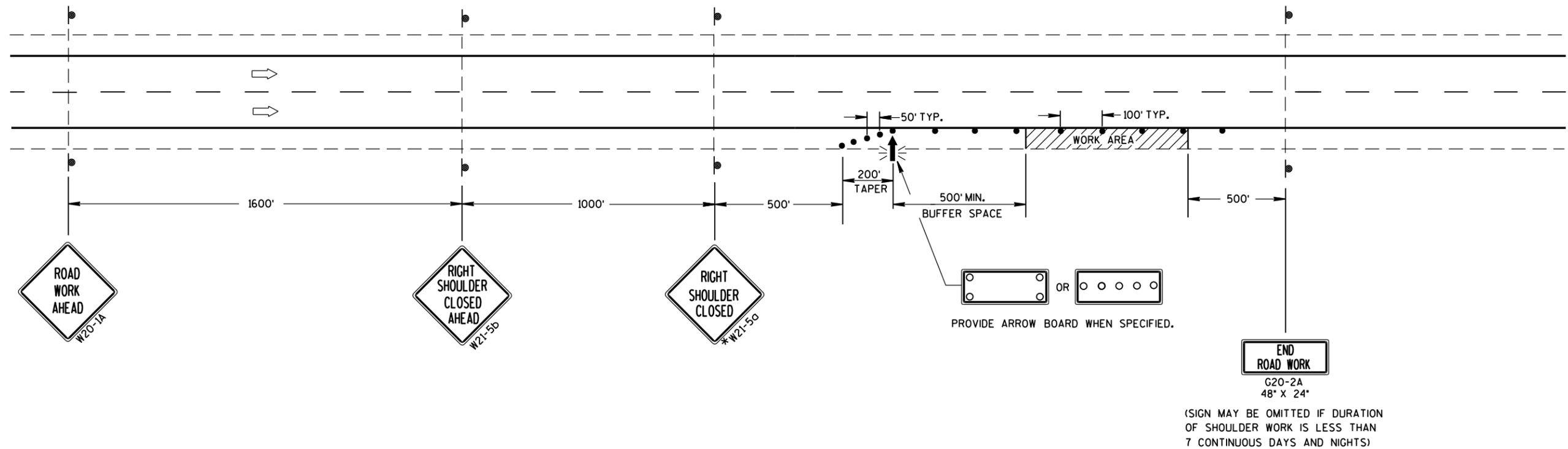
CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

*FOR SHORT DURATION SHOULDER WORK OF LESS THAN ONE HOUR, THE W21-50 SIGN MAY BE OMITTED.

LEGEND

- TRAFFIC CONTROL DRUM
- ⊙ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ⚡ FLASHING ARROW BOARD
- ▨ WORK AREA



TRAFFIC CONTROL SHOULDER CLOSURE ON DIVIDED ROADWAY, SPEEDS GREATER THAN 40 MPH	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2016 DATE	/s/ Peter Amakobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

ALL SIGNS ARE 48"X48" BLACK ON ORANGE UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

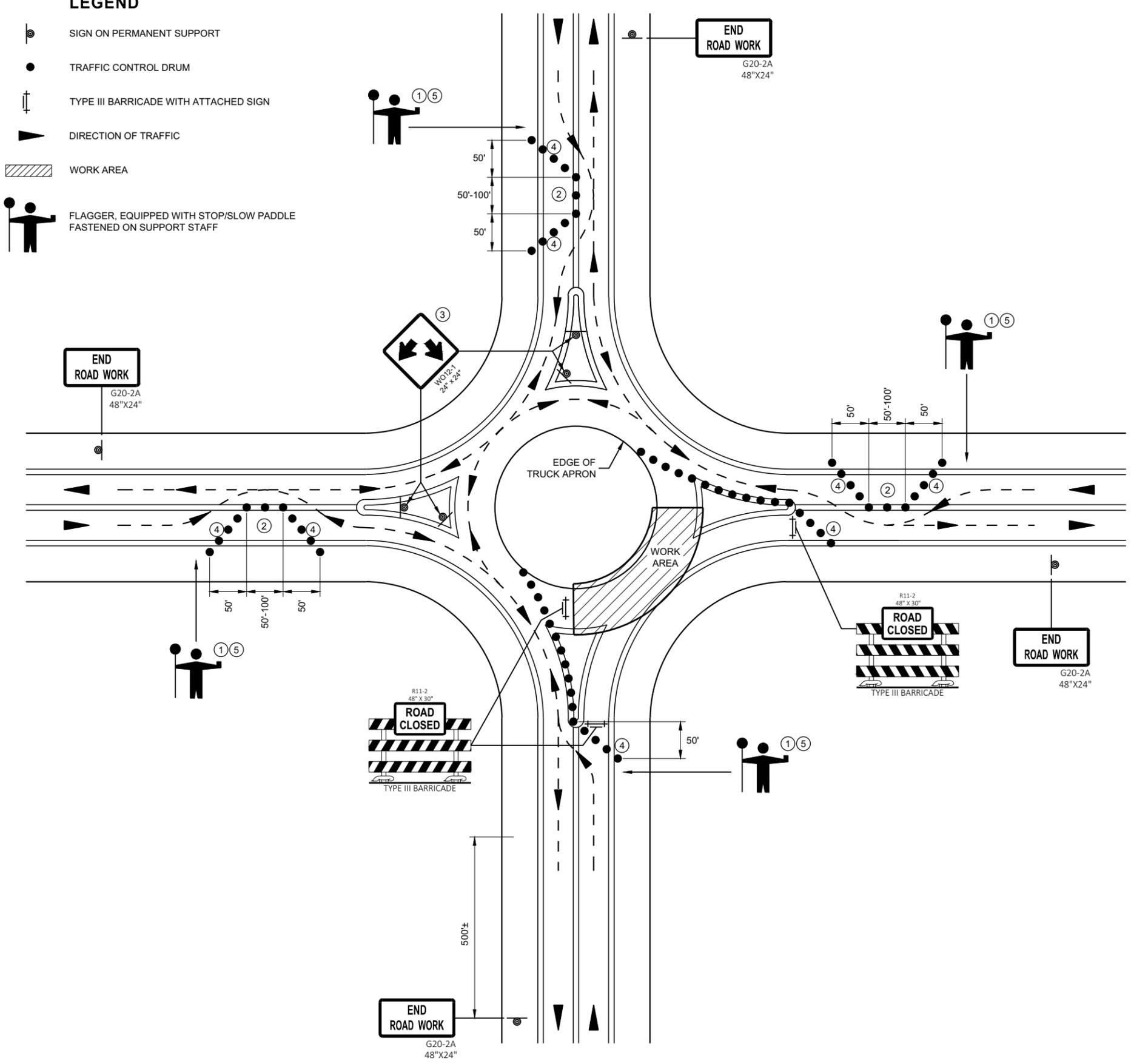
THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ONLY ONE QUADRANT OF TRAFFIC SHALL BE RELEASED AT A TIME.

FLAGGERS SHALL CONTROL TRAFFIC ON ALL APPROACHES OF THE ONE-LANE ROUNDABOUT. ONE FLAGGER SHALL BE DESIGNATED LEAD FLAGGER.

NIGHT TIME WORK REQUIRES ADDITIONAL LIGHTING AT FLAGGER STATION(S).

- ① FLAGGERS SHALL BE IN DIRECT RADIO CONTACT AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING IS NOT IN EFFECT, COVER OR REMOVE ALL TEMPORARY TRAFFIC CONTROL SIGNS.
- ② ADJUST CHANNELIZING DEVICES TO ACCOMMODATE FOR TURNING RADIUS OF LARGE VEHICLES AS DIRECTED BY THE ENGINEER.
- ③ GUIDE SIGN WITH ROAD NAMES MAY BE USED IN LIEU OF THE DOUBLE ARROW (WO12-1) SIGN.
- ④ THE TWO-WAY TAPER SHOULD BE 50 FEET USING 5 EQUALLY SPACED DRUMS.
- ⑤ FOLLOW SDD 15C12 FOR FLAGGING AND ADVANCE WARNING.



RECOMMENDED REDUCED DRUM SPACING WITHIN ROUNDABOUT

MPH	DRUM SPACING (FT)
0-40	25
45-55	50

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SDD 15D37 - 03

SDD 15D37 - 03

TRAFFIC CONTROL, FLAGGING OPERATION, ROUNDABOUT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE May 2020 /S/ Andrew Heidtke
STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

FHWA

LEGEND

-  TYPE III BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE "A" WARNING LIGHT (FLASHING)
-  DIRECTION OF TRAFFIC
-  WORK AREA

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHIFTING TAPER $L/2$					
		W, LATERAL OFFSET (FT)					
		3	4	5	6	7	8
25	200	10	14	17	21	24	28
30	200	15	20	25	30	35	40
35	350	20	27	34	40	47	54
40	350	26	35	44	53	62	70
45	500	45	59	74	89	104	119
50	500	50	66	83	99	116	132
55	500	54	73	91	109	127	145

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"x36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

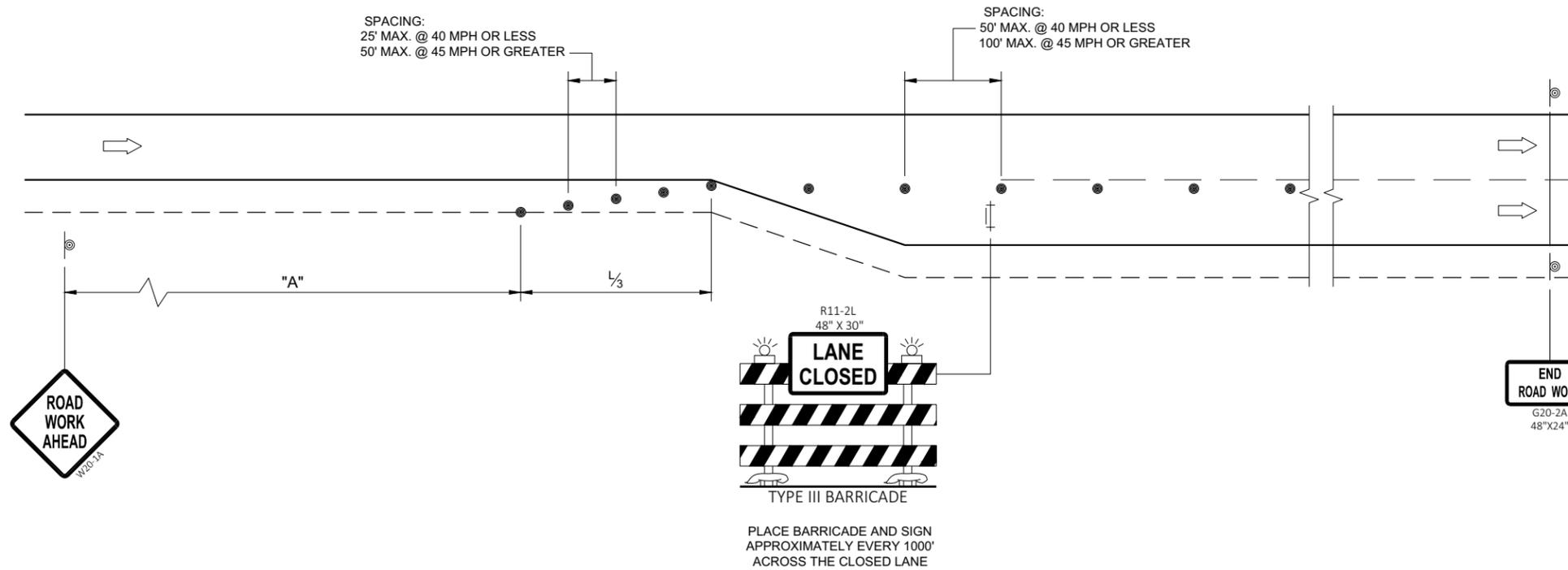
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CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION WORK IS LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS.



ADDED LANE CLOSURE WITHOUT LANE SHIFT

TRAFFIC CONTROL ADDED LANE CLOSURE WITHOUT LANE SHIFT	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2021 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

-  TYPE III BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE "A" WARNING LIGHT (FLASHING)
-  DIRECTION OF TRAFFIC
-  WORK AREA

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHIFTING TAPER (L/2) FEET
25	200	60
30	200	90
35	350	120
40	350	160
45	500	270
50	500	300
55	500	330

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"x36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

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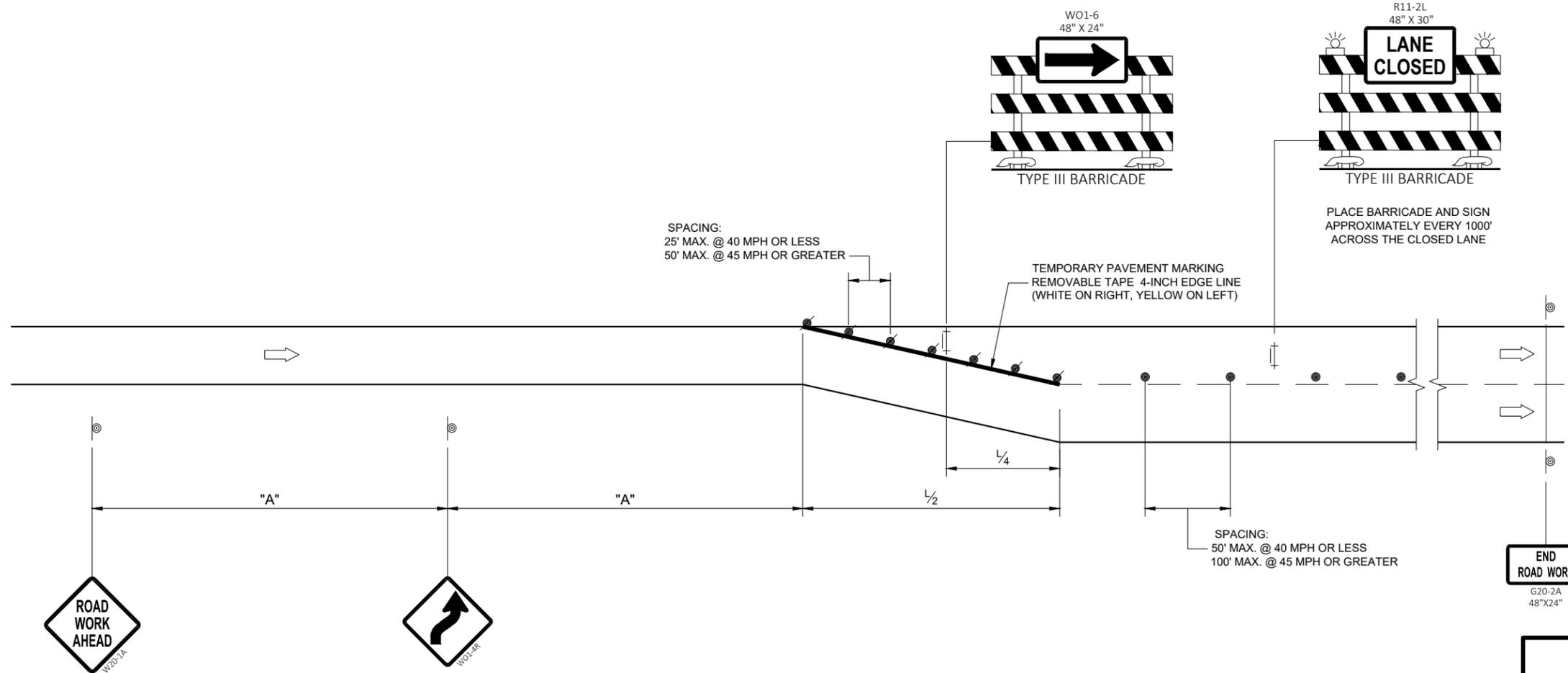
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION WORK IS LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS.

PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR FOUR CONTINUOUS DAYS AND NIGHTS.

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ADDED LANE CLOSURE WITH LANE SHIFT

**TRAFFIC CONTROL,
ADDED LANE CLOSURE
WITH LANE SHIFT**

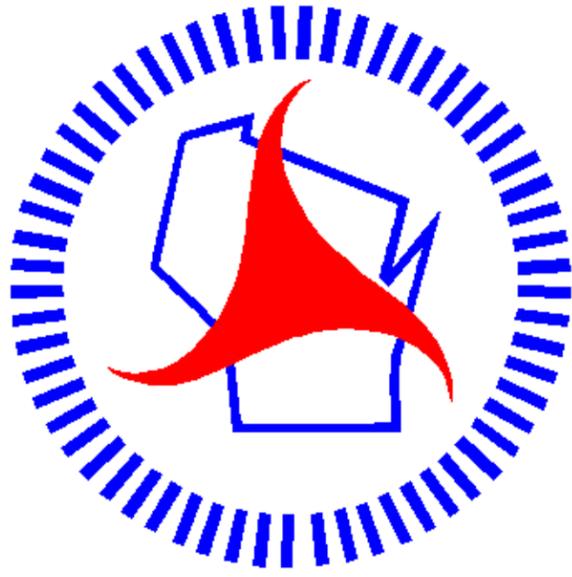
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

SDD 15D50 - 01b

SDD 15D50 - 01b



Wisconsin Department of Transportation

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