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December 29, 2021

On behalf of the Wisconsin Department of Transportation, Bureau of Transportation Safety and Technical Services (BOTS2), I am pleased to present the State of Wisconsin's Annual Evaluation Report of federal fiscal year 2021 highway safety program activities. The report identifies behavioral activities undertaken with both state and federal highway safety funds and some additional activities undertaken by our valued safety partners in support of our transportation safety objectives.

With a strong and active commitment from our safety partners in federal fiscal year 2021, BOTS administered highway safety grants provided by the National Highway Traffic Safety Administration (NHTSA) under 23 U.S.C. Chapter 4. The Wisconsin Department of Transportation continually promotes traffic safety through education, engineering, enforcement, and emergency medical services. The delivery of the multiple programs described in this report collectively work to prevent fatalities and serious injuries on all Wisconsin roads.

Sincerely,

A handwritten signature in blue ink, appearing to read "D. Pabst".

David Pabst, Director
Bureau of Transportation Safety

Foreword

The Wisconsin Highway Safety Program is administered by the Bureau of Transportation Safety (BOTS), which is within the Wisconsin State Patrol. The Wisconsin State Patrol is a division of the Wisconsin Department of Transportation (WisDOT). The Secretary of Transportation, Craig Thompson, is the Governor's Highway Safety Representative, and David Pabst, Director of BOTS, is the State Highway Safety Coordinator.

Mission

The bureau is charged with coordinating statewide behavioral highway safety programs and making effective use of all highway safety funds and other resources. BOTS strives to provide leadership, partnership, innovation, and program support for Wisconsin's traffic safety community, and to decrease crashes, deaths, and injuries on all of Wisconsin's roadways.

Highway safety planning and administration functions are performed by state-funded and federally funded BOTS staff, with the assistance of other safety professionals within WisDOT and throughout the state. Planning and administration functions overlap with WisDOT planning. Jointly, they produce high-quality, data-driven reports, fact sheets, and other publications including the Highway Safety Plan (HSP) and the Strategic Highway Safety Plan (SHSP) as well as this document, which reports on the fiscal performance of the bureau and its adherence to the HSP.

Funds

Wisconsin programmed over \$13.7 million of federal highway safety formula and incentive grants, and state funds during federal fiscal year 2021. This included funds from the Fixing America's Surface Transportation (FAST) Act:

- Section 402 State and Community Highway Safety Grants
- Section 405(b) Occupant Protection Grants
- Section 405(c) State Traffic Safety Information System Grants
- Section 405(d) Impaired Driving Countermeasures Grants
- Section 405(f) Motorcyclist Safety Grants

The Bureau of Transportation Safety also administered over \$1.2 million in State of Wisconsin funds for transportation safety program management and policy analysis, the safe-ride grant program, motorcycle rider education programs, and a media campaign promoting motorist awareness of motorcyclists. Wisconsin achieves its required administrative match for all funds through straight time traffic enforcement performed by the Wisconsin State Patrol.

Goal

Goals for this federal fiscal year 2021 Annual Report were set and committed to in the 2021 Highway Safety Plan (HSP). The results laid out in this document use 2020 calendar year data. The Governors Highway Safety Association (GHSA) and the National Highway Traffic Safety Administration (NHTSA) agreed to a minimum set of performance measures to be used in the development of HSPs. Wisconsin has set a goal of a 5% improvement for most of the "Core Outcome Measures" from the prior five-year rolling average, and a 2% improvement for the number of traffic fatalities and the fatality rate using available state data available at the time. When goals were written in the 2021 Highway Safety Plan (HSP), finalized 2020 FARS crash data were not yet available. All projects in the 2021 HSP and described in this Annual Report were designed to contribute to meeting these targets.

The agreed-upon performance measures and the results are summarized in the matrix on the next page. The results in green indicate measures where the goal was either met or exceeded, and the results in red indicate measures where the goal was not met. This table was requested by NHTSA and includes notes below.

Assessment of Results in Achieving Performance Targets for FY21 and FY20									
Performance Measure	FY 2021					FY 2020			
	Target Period	Target Year(s)	Target Value FY21 HSP	Data Source/ FY 21 Progress Results	Ontrack to Meet FY21 Target Y/N (in-progress)	Target Value FY20 HSP	Target Year(s)	Data Source/ FY20 Final Result	Met FY20 Target Y/N
C-1) Total Traffic Fatalities	5 year	2015-2019	576	2021 FARS	In Progress	564.7	2014-2018	2020 FARS 614	No
C-2) Serious Injuries in Traffic Crashes	5 year	2015-2019	2,897.9	2021 STATE	In Progress	2,907.0	2014-2018	2020 STATE 3,030	No
C-3) Fatalities/VMT	5 year	2015-2019	0.89	2021 FARS	In Progress	0.888	2014-2018	2020 FARS 1.07	No
C-4) Unrestrained Passenger Vehicle Occupant Fatalities, All Seat Positions	5 year	2015-2019	156	2021 FARS	In Progress	177	2014-2018	2020 FARS 179	No
C-5) Alcohol-Impaired Driving Fatalities	5 year	2015-2019	171	2021 FARS	In Progress	119	2014-2018	2020 FARS 210	No
C-6) Speeding-Related Fatalities	5 year	2015-2019	173	2021 FARS	In Progress	173	2014-2018	2020 FARS 216	No
C-7) Motorcyclist Fatalities (FARS)	5 year	2015-2019	78	2021 FARS	In Progress	74	2014-2018	2020 FARS 116	No
C-8) Unhelmeted Motorcyclist Fatalities	5 year	2015-2019	53	2021 FARS	In Progress	56	2014-2018	2020 FARS 83	No
C-9) Drivers Age 20 or Younger Involved in Fatal Crashes	5 year	2015-2019	73	2021 FARS	In Progress	69	2014-2018	2020 FARS 67	No
C-10) Pedestrian Fatalities	5 year	2015-2019	52	2021 FARS	In Progress	51	2014-2018	2020 FARS 50	Yes
C-11) Bicyclist Fatalities	5 year	2015-2019	10	2021 FARS	In Progress	8	2014-2018	2020 FARS 12	No
B-1) Observed Seat Belt Use for Passenger Vehicles, Front Seat Outboard Occupants (State Survey)	5 year	2015-2019	93.05%	2021 STATE 88.1%	No	91.9%	2014-2018	2020 STATE 89.2%	No
B-2) Serious Injuries/VMT	5 year	2015-2019	4.482	2021 STATE	In Progress	4.585	2014-2018	2020 STATE 5.26	No
B-3) Non-Motorized Fatalities and Serious Injuries	5 year	2015-2019	350.2	2021 FARS & STATE	In Progress	344.7	2014-2018	2020 FARS & STATE 342	Yes
		5 yr range for HSP 2020	2021 Targets from the HSP 2021	2021 FARS result (Not Avail)		2020 Targets from the HSP 2020	5 yr range for HSP 2020	2020 FARS result	

Confusing when mentioning Annual Report and Highway Safety Plan. More clearly explained would be better.

Explanation for (F) states FY20 HSP but the chart states FY21? Which one do we use? This in turn leads to confusion of column 'L'. Are we supposed to be using 'G' and for the first '**' under the notes the last sentence is written poorly. Stating that we don't have to use both State and Fed numbers? Why would someone use both and not? What is the reason for listing the year(s) of for 'G' and 'K' when you just want whether it was FARS or State and the Final 2020 number for 'K' and I'm assuming a prelim

PLANNING and ADMINISTRATION

PERFORMANCE GOALS AND MEASURES

The success of any strategic effort requires planning, administration, and commitment from many levels. The analysis, planning, coordination, and direction of the efforts to influence behavioral change on Wisconsin roadways are performed with in-house staff of the Bureau of Transportation Safety. NHTSA rules limit the amount of 402 funds available to support the planning and administration of the program to 15% of total 402 funds expended. States must match federal funds to support the planning and administration program.

Federal highway safety and related state funds were distributed into evidence-based, targeted activities that were most likely to decrease the burden of crashes, deaths, and injuries on Wisconsin roadways.

The following activities were administered following state and federal rules and guidelines:

- The proper application of funds throughout the year.
- The administration of planned activities by the end of federal fiscal year (FFY) 2021.
- Financial management and program management documentation.
- The production of timely, accurate, and complete plans and reports, including this Annual Report and the Highway Safety Plan, both of which were submitted on time during this fiscal year.

Expenses in this section using section 402 funds included wage and fringe benefits for the grants management supervisor, the policy and program supervisor, and three program associates. Data processing, training, travel, printing, and postage costs were also included.

Wage and fringe benefits for the director, the section chief, and two analysts were paid with state funds.

Planning and Administration – Budget Summary				
Fed ID	Fund	Program Activity	Planned	Expended
2021-10-01-PA	402	Planning and Administration	\$320,000.00	\$271,798.43
2021-19-01	State 562	Planning and Administration	\$530,000.00	\$471,762.99
		Program Total	\$850,000.00	\$743,561.42

OCCUPANT PROTECTION PROGRAM

Performance Goals and Measures

In 2020, 44% of Wisconsin's passenger vehicle occupant fatalities were unrestrained occupants. The program missed its safety belt usage goal of 93.1% by only reaching 89.2% average safety belt use. There was an increase in the ejection rate as well as unbelted fatalities and suspected serious 'A' injuries as shown in the tables below.

Average Belt Use

2015-2019 Baseline	88.6%
2020 Goal	93.1%
2020 Actual	89.2%

Unbelted Fatalities and 'A' injuries

2015-2019 Baseline	1.03/100M VMT
2020 Goal	0.97/100M VMT
2020 Actual	1.18/100M VMT

Ejection Rate/Fatal and Injury Crashes of Passenger Vehicles and Utility Trucks

2015-2019 Baseline	0.53%
2020 Goal	0.50%
2020 Actual	0.56%

The occupant protection safety program has four types of programming designed to reduce the number of unrestrained occupant fatalities and injuries:

1. Program Management
2. Evidence-Based Law Enforcement
3. Child Passenger Safety
4. Data and Program Evaluation

Wisconsin's occupant protection program consists of many facets, some of which are summarized elsewhere in this document. These facets include public information and education activities in the media and outreach program section, as well as some law enforcement activities that are summarized in the police traffic section.

Program Management

BOTS's occupant protection specialist oversees all child passenger safety occupant protection grants as well as a portion of the enforcement grants for all major behavioral highway safety areas. This staff person manages grants for education, training, and technical assistance to agencies, organizations, and non-profit programs interested in occupant protection. This person also integrates occupant protection programs into community/corridor traffic safety and other injury prevention programs. Most importantly, the person in this position encourages communities and grantees to view occupant protection as a sustained effort rather than an intermittent enforcement mobilization or campaign.

Evidence-Based Law Enforcement

BOTS planned for statewide participation in evidence-based overtime-funded enforcement using the targeting methodology outlined in the Highway Safety Plan. Counties with a high number of pickup truck registrations were also targeted for an occupant protection grant. A total of 37 grants were given to law enforcement agencies statewide for sustained year-round enforcement. All the grants were high visibility enforcement task forces that included multiple agencies performing deployments together and included media outreach. Fifty percent of this enforcement was conducted during hours designated for nighttime enforcement.

A total of 213 law enforcement agencies from across Wisconsin participated in the 2021 Click It or Ticket national enforcement mobilization.

Child Passenger Safety (CPS)

CPS Program Administration

The State of Wisconsin has continued its contract with the Children’s Hospital and Health System (CHHS) for support and administrative costs for statewide Child Passenger Safety Advisory Committee. The goal of this partnership with CHHS is to support and administer statewide CPS training, ensuring that we recruit, train, recertify, and maintain a sufficient number of child passenger safety technicians. BOTS staff work with CHHS to provide additional CPS training materials to community partners and local events as well as CPS training for law enforcement agencies, judges, and other safety partners with community programs.

CPS Seat Grant Program

The purpose of the program is to make car seats available at low or no cost to low-income families who might otherwise choose to place their children in older, unsafe seats or to transport them unrestrained or improperly restrained, thereby putting them at risk. BOTS staff developed a targeting methodology to determine which counties are selected to receive a CPS grant based on crash data and data provided by partners at the Wisconsin Department of Health Services. Once counties with the most at-risk population were identified, agencies such as county health departments, social workers, the Women, Infants, and Children (WIC) Program, and county injury prevention coalitions partner with BOTS to utilize the grant. The recipient or recipient’s caregiver is given a voucher providing instructions regarding where to call to set up an appointment. At this appointment, recipients receive installation assistance, a new car seat appropriate for the child, and education about transportation safety. The objectives of the program are measured by the number of clients redeeming vouchers. After receiving education and assistance with installation, a survey is completed by participants for each seat distributed.

The national standardized CPS technician certification course is 36 hours long and combines classroom instruction, hands-on work with car seats and vehicles, and a community safety seat checkup event.

From October 1, 2020- September 30, 2021 six Child Passenger Safety Technician (CPST) certification courses were held with 37 new technicians certified. There was also one Child Passenger Safety Technician Renewal Certification Course held for CPSTs who have expired, there were four CPSTs certifications that renewed from this course. Over 65% (27) of course attendees were provided a scholarship to help offset the cost for both the CPST Training Course and CPST Renewal Course. Due to COVID-19 all CPST Training Courses scheduled for the 2021 calendar year had limited course sizes for social distancing, which impacted the number of technicians trained and scholarships awarded for the grant year.

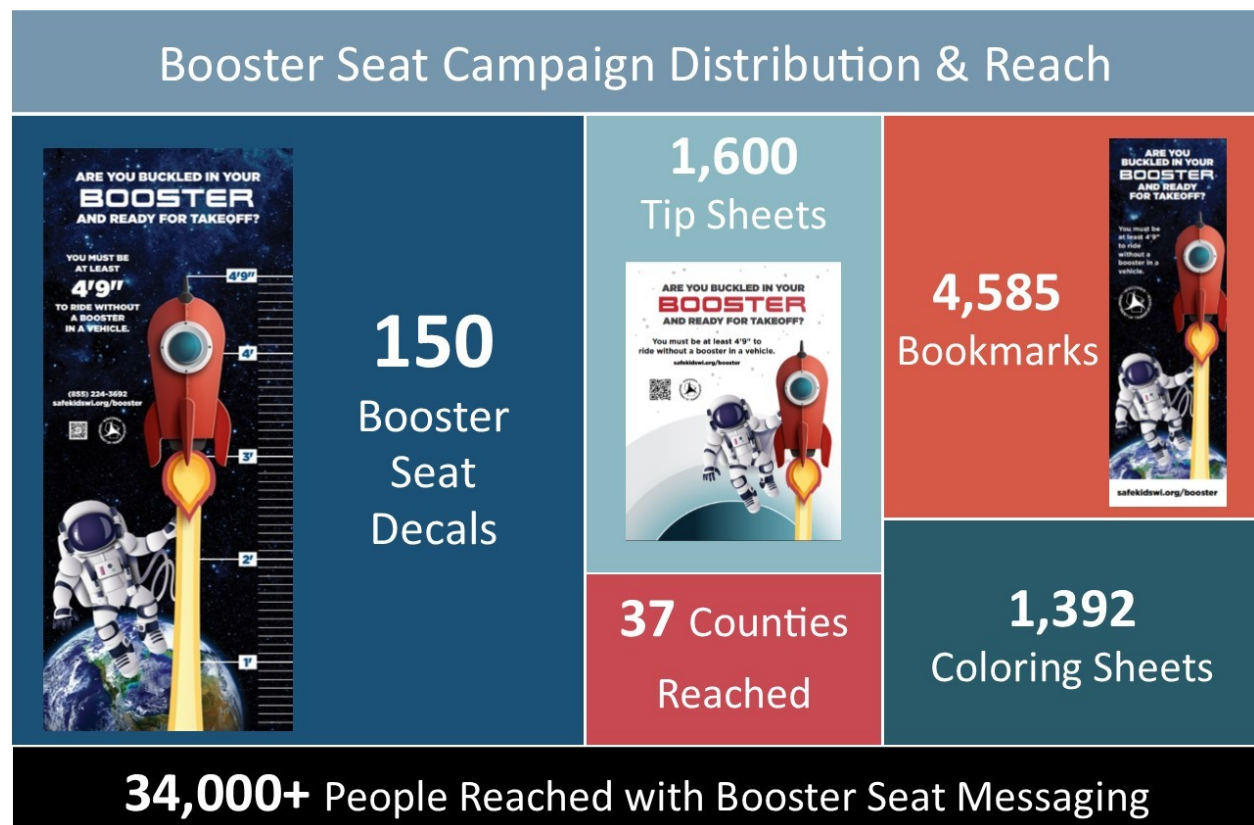
Course Location	Course Date	Number of New Technicians	Scholarships
Fitchburg	11/16/2020-11/18/2020	3	0
Milwaukee	1/18/2020-1/21/2020	5	3
Milwaukee	6/21/2021-6/24/2021	4	3
Janesville	8/3/2021-8/5/2021	10	6
Green Bay	8/16/2021-8/19/2021	8	5
Vernon	9/20/21-9/23/21	7	6
Milwaukee	9/29/2021	4	4

The new technicians represent many organizations and occupations including medical personnel, law enforcement, firefighters, public health staff, car dealership staff, AAA, and Safe Kids coalition representatives.

In 2021, there were 41 newly certified car seat technicians for a total of 502 certified car seat technicians in Wisconsin. Fitting stations are often endorsed through the agencies working with WIC-eligible clients. Fitting stations are also promoted with flyers posted around the community and—when possible—media outlets. Through these grants, there were 1446 car seats checked for proper installation averaging approximately eleven per month per granted agency. This does not include work accomplished with partners who do not receive grant funding.

Programs to Educate the Public

A study by Safe Kids Worldwide revealed that nine out of ten parents moved their child from a booster seat to a seat belt too soon. Transitioning a child from a booster seat to a seat belt too soon can increase the risk of injury or death to a child. Based on these findings the Wisconsin CPS Advisory Board is launched an effort to make parents aware of when it is safest to move a child from a booster seat to a seat belt. A removable wall decal, tip sheet, coloring sheet, LCD screen file and bookmarks have been created and distributed to show that children should be at least 4'9" before making the transition to a seat belt. The WI CPS Advisory Board knows it can be confusing for parents and caregivers when deciding to transition their child from a booster seat to a seat belt. Their goal with this campaign is to help educate families on the importance of booster seats and how long children should remain in them.



Data and Program Evaluation

BOTS contracted with UW-Whitewater to conduct the seat belt observational survey. In 2020, Wisconsin had 89.2% compliance with its mandatory safety belt use laws. See the results of the 2020 survey on the next page.

Occupant Protection Program – Budget Summary

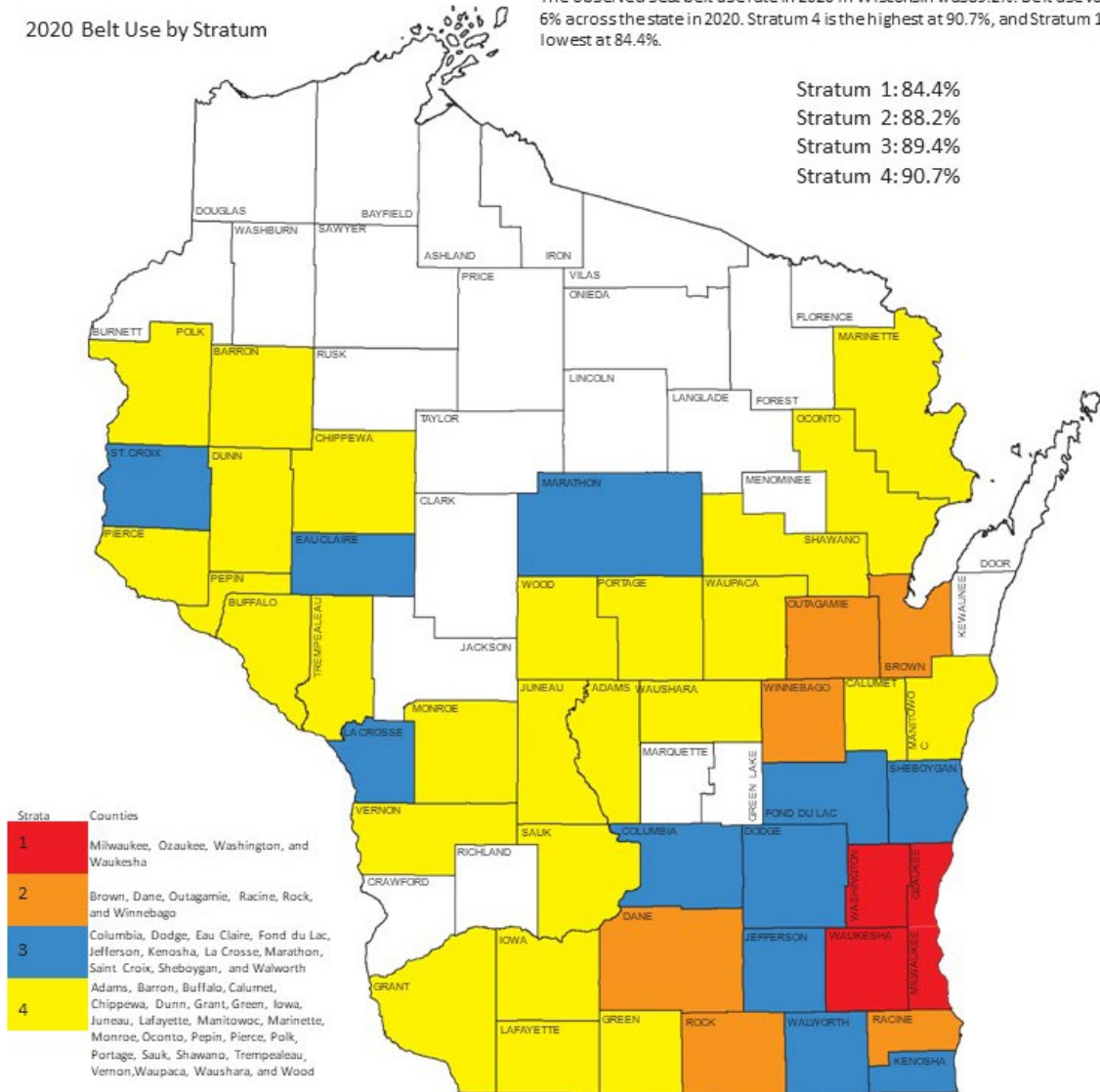
Fed ID	Fund	Program Activity	Planned	Expended
2021-20-01-OP	402	Program Management	\$85,000.00	\$73,272.35
2021-20-02-OP	402	Teen Driving	\$50,000.00	\$36,606.15
2021-20-03-OP	402	Training and Support – Technical*	\$205,000.00	\$151,363.90
2021-20-05-OP	402	Enforcement	\$1,649,394.55	\$1,366,978.07
2021-20-06-OP	402	CPS Equipment	\$117,622.99	\$51,649.21
2021-20-09-OP	402	Evaluation	\$12,600.00	\$12,600.00
2021-25-03-M2	405b	Training and Support - Technical	\$37,000	\$23,585.00
2021-25-05-M2	405b	Enforcement	\$395,376.16	\$356,287.91
2021-25-06-M2	405b	CPS Equipment	\$54,631.47	\$37,342.75
2021-25-09-M2	405b	Evaluation	\$80,995.00	\$55,398.64
		Program Totals	\$2,687,620.17	\$2,165,083.98

WISCONSIN SAFETY BELT SURVEY STRATA

2020 Belt Use by Stratum

The observed seat belt use rate in 2020 in Wisconsin was 89.2%. Belt use varied by 6% across the state in 2020. Stratum 4 is the highest at 90.7%, and Stratum 1 is the lowest at 84.4%.

Stratum 1: 84.4%
 Stratum 2: 88.2%
 Stratum 3: 89.4%
 Stratum 4: 90.7%



IMPAIRED DRIVING PROGRAM

Performance Goals and Measures

Alcohol and Drug Related Motor Vehicle Crashes

2015-2019 Baseline	10.06/100M VMT
2020 Goal	9.55/100M VMT
2020 Actual	12.65/100M VMT

Resulting Deaths and Suspected Serious (A) Injuries

2015-2019 Baseline	1.43/100M VMT
2020 Goal	1.35/100M VMT
2020 Actual	1.95/100M VMT

Proportion of Alcohol and Drug Related Fatalities

2015-2019 Baseline	35.82%
2020 Goal	34.03%
2020 Actual	35.75%

Proportion of Alcohol and Drug Related Fatalities and 'A' Injuries

2015-2019 Baseline	22.60%
2020 Goal	21.47%
2020 Actual	28.41%

Crashes involving alcohol (15-24 years olds)

2015-2019 Baseline	177.02/100K pop.
2020 Goal	168.17/100K pop.
2020 Actual	181.41/100K pop.

Wisconsin did not meet any of its goals above but has instituted some new predictive analytics pilot programs around the state that will involve a systems approach to changing driver behavior going forward into FFY 2022 and forward.

The impaired driving safety program was comprised of five types of programming, all of which were designed to reduce impaired driving fatalities and serious injuries. They are as follows:

1. Program Management and Strategic Planning
2. Prevention through the Promotion of Transportation Alternatives
3. Evidence-Based Enforcement
4. Drug Evaluation and Classification Program
5. Criminal Justice System

Program Management and Strategic Planning

BOTS staff coordinated, planned, and managed the state impaired driving programs. Goals included enhancing volunteer agency participation, increasing community involvement, working with community

organizations and non-profit programs to expand impaired driving activities and efforts, and encouraging state and local input into the Highway Safety Plan development process.

Prevention through the Promotion of Transportation Alternatives

BOTS collaborated with the Tavern League of Wisconsin in administering the state-funded safe-ride grant program.

The Bar Buddies programs in Baraboo, Lodi, Reedsburg, Sauk Prairie, and Spring Green continued to offer alternative transportation to homes as well. Bar Buddies grants are funds to off-set cost for fuel, maintenance, and other costs for agencies providing alternative transportation in their communities. As the programs become self-sufficient, grant amounts decrease. Due to continued COVID restrictions on businesses all Bar Buddies programs had periods of inactivity or low activity.

BOTS offered alternative transportation grants covering community festivals. The grants were offered to the municipalities of Watertown, Arlington, Darlington, and Seymour as well as in Barron and Crawford counties. Agencies provided evidence that poor driver judgment could be expected, and that drinking, and driving had been a problem at the event. The grants would have provided short-term alternative transportation (vans, buses, or vehicles) to transport community members from the local summer community event to their home. Due to continued COVID restrictions only Watertown, Barron County, Darlington, and Seymour accepted the grants for 2021.

BOTS previously developed a Drive Sober mobile application (app) for smart phones, and since inception, use of the app is as follows:

Screen Name	Screen Views
Home	380,578
Blood Alcohol Estimator	176,045
Designated Driver Selector	84,337
Impairment Goggles	77,145
Find a Ride	75,593

This app includes a designated driver selector and a Find a Ride feature that uses a phone's GPS to provide users with a list of taxi services and public transit options to allow users to find a safe ride home.

Evidence-Based Enforcement

BOTS partnered with multiple counties and municipalities and provided overtime funding for evidence based high-visibility enforcement. Law enforcement agencies were selected using the evidence-based targeting methodology discussed in the Highway Safety Plan. We encouraged law enforcement agencies to make OWI a priority by educating the public, having zero tolerance for impaired driving, sponsoring media events, and working overtime in geographic areas where impaired driving is highest. We provided overtime funding for sustained alcohol saturation patrols consisting of at least monthly high-visibility

enforcement (HVE) overtime patrols, including nighttime enforcement accompanied by media in targeted jurisdictions. Activity for enforcement is found in the Police Traffic section.

Wisconsin had 24 multi-jurisdictional impaired driving task forces throughout the state in 2021. Officers from the participating agencies used saturation patrols featuring high-visibility enforcement, which included branding specific to OWI task forces such as vests, squad magnets, and electronic message boards. The various projects received considerable attention from the media and had a strong social media presence, which included Facebook pages dedicated to OWI task forces.

The 2021 Drive Sober or Get Pulled Over-Labor Day national enforcement mobilization had 158 participating law enforcement agencies and Drive Sober or Get Pulled Over – Winter Holidays mobilization had 203 participating agencies. There was a large decrease due in-part to non-funded agencies not participating because the chance for an equipment grant is no longer being provided.

Fiscal year 2021 equipment grants were not provided to agencies due to changes made in the FAST act, BOTS is currently working on a new program to provide agencies with the ability to participate in funded activity that includes appropriate traffic safety equipment purchases.

Drug Evaluation and Classification Program

BOTS funded Drug Recognition Expert (DRE) training including instructor wages, printing, supplies, and lodging for students and instructors. The grant provided funding for the State DRE Coordinator which is a contractor position. BOTS funding covered related programs including Advanced Roadside Impaired Driving Enforcement (ARIDE), Drug Impairment Training for Educational Professionals (DITEP), and Standardized Field Sobriety Testing (SFST). The number of classes are down slightly due to continued COVID restrictions and staffing shortages across the State of Wisconsin. Here are the current results of activity for the year.

Current DREs through September 30, 2021	
Number of certified DREs:	346
Number of certified DRE instructors:	42
Number of State Police/HP DREs:	14
Number of City Police Department DREs:	210
Number of Sheriff’s Department DREs:	118
Number of Other Agency DREs:	4
Number of LE agencies with certified DREs:	41

DRE Evaluations through September 30, 2021	
Number of enforcement evaluations:	848
Number of training evaluations:	399
Other evaluations:	6
Total number of evaluations conducted:	1253
Drug Category (DRE’s Opinion)	
Depressants:	284
Stimulants:	463

Hallucinogens:	14
Dissociative Anesthetics:	26
Narcotic Analgesics:	344
Inhalants:	2
Cannabis:	476
Poly Drug Use	
Total number of cases:	404
Other	
Alcohol Rule Outs:	5
Medical Impairment:	5
No Opinion of Impairment:	18
Tox Results - Pending:	68
Tox Results - No Drugs:	16
Toxicology - Refused:	57
DRE Training	
Number of DRE Schools:	2
Number trained:	45
Number of DRE Instructor courses:	1
Number trained:	5
Number of DRE Instructors certified:	5
Number of DRE Recertification courses:	5
Number of DREs trained:	262
ARIDE Training	
Number of ARIDE classes:	39
Number trained:	536
DITEP Training	
Number of DITEP classes:	2
Total number trained:	11
Number of DITEP Instructor classes:	0
Total number trained:	0
Phlebotomy Training	
Number of classes:	0
Number trained:	0
SFST Training	
Number of SFST courses:	34
Number trained:	612
Number of SFST Instructor courses:	6
Number trained:	50
Number of SFST Master Instructor courses:	3
Number trained:	5

Criminal Justice System

Wisconsin's Traffic Safety Resource Prosecutor (TSRP) grant covered two state experts who specialized in the legal issues of OWI. These TSRPs provided training and technical assistance to hundreds of prosecutors, judges, and law enforcement officers. They also became co-chairs of our Impaired Driving Work Group that meets quarterly.

The Judicial Outreach Liaison position has not yet been decided on and may be pursued in the coming fiscal year, but likely will not use NHTSA funding through our agency.

Adjudication

BOTS provided funds to reimburse travel costs for OWI courts and drug courts to travel to training sponsored by the National Center for DWI Courts (NCDC). No trainings were attended by agencies from Wisconsin, the refresher course that is held in Wisconsin did not occur in 2021 due to COVID travel restrictions.

24-7 Sobriety Programs

24-7 sobriety programs are in the process of being created by the Wisconsin Department of Justice. This project had no expenditures in 2021 since these programs are still being developed.

Data and Program Evaluation

Wisconsin State Lab of Hygiene

Due to COVID -19 pandemic, the planned activity to collect data regarding driving under the influence of drugs in a small metropolitan area was not accomplished. A past project studied this problem in a larger metropolitan area. This project will eventually use roadside oral fluid testing devices in conjunction with forensic toxicology testing of the legal blood draw to examine the validity and efficacy of the roadside devices. Drug Recognition Experts (DRE) will administer the roadside testing and the data from the device and the DRE evaluation will be compared. Wisconsin meets its requirements for match with traffic enforcement conducted on straight time by the Wisconsin State Patrol.

Fed ID	Fund	Program Activity	Planned	Expended
2021-31-01-M5	405d	Program Management	\$85,000.00	\$82,291.42
2021-31-03-M5	405d	Training and Support– Technical	\$462,000.00	\$299,678.21
2021-30-03-AL	402	Training and Support – Technical	\$349,440.00	\$317,281.47
2021-30-04-AL	402	Training and Support – Community	\$44,715.00	\$36,341.90
2021-31-05-M5	405d	Enforcement	\$2,126,699.64	\$1,463,485.90
2021-39-04-WI	State 531	Transportation Alternatives	\$815,000.00	\$638,437.72
		Program Totals	\$3,882,854.64	\$2,837,516.62

Police Traffic Program

Speed-related traffic fatalities have decreased from the prior five-year rolling average (2014-2018) of 182.6 to 163 using state data. Wisconsin failed to reduce both speed related crashes and combined fatalities and suspected serious injuries in speed-related crashes.

Speed Related Crashes	
2015-2019 Baseline	30.41/100M VMT
2020 Goal	28.89/100M VMT
2020 Actual	27.86/100M VMT

Speed-Related Combined Fatalities and Suspected Serious 'A' Injuries	
2015-2019 Baseline	1.67/100M VMT
2020 Goal	1.58/100M VMT
2020 Actual	2.13/100M VMT

The police traffic program was managed collaboratively with multiple staff members. State program managers worked closely with regional program managers, law enforcement liaisons, and law enforcement agencies of all sizes to coordinate enforcement efforts, encourage safe and effective high-visibility enforcement operations, and participate in mobilizations.

The police traffic program employs evidence-based law enforcement aimed at preventing speed and aggressive driving related fatalities and serious injuries.

Evidence-Based Law Enforcement

BOTS planned statewide participation, encouraged voluntary participation, and provided overtime funding for the speed and aggressive driving evidence-based enforcement campaign in areas most at risk for speed-related traffic violations, crashes, and traffic fatalities as determined by the targeting methodology described in detail in the Highway Safety Plan. BOTS encouraged coordination between county and local law enforcement by employing multi-jurisdictional task forces. There were 35 such speed task forces in 2021 with 230 participating law enforcement agencies across 37 counties.

The DSP conducted a total of 59 BOTS funded traffic enforcement flights and 28 total ground details for a grand total of 87 details during the 2020-21 BOTS speed grant. The remaining details the aircraft had to cancel or was not available due to weather or other unforeseen issues, such as the COVID-19 pandemic protocol. It is important to note however that on those details that were scheduled when the aircraft had to cancel the details continued with ground only cars (no air support) conducting speed enforcement with laser or other means.

Summary totals for all enforcement activities for impaired driving, occupant protection, and speed are shown in the table below. For the purposes of continuity in programming, all agencies are required to

report their enforcement numbers, regardless if such numbers were from national enforcement mobilizations or from a targeted grant.

FFY2021 Enforcement Totals	
# Officers Deployed	19,159
Total OT Hours	78,386
Total Straight Time Hours	20,462
Safety Belts OT	12,885
Safety Belts Straight	2,724
Child Restraints OT	284
Child Restraints Straight	114
Speed OT	25,109
Speed Straight	10,979
OWI OT	1,344
OWI Straight	1,644
OAR OT	7,969
OAR Straight	4,710
Open Intox OT	339
Open Intox Straight	279
Vehicle Reg OT	3,472
Vehicle Reg Straight	2,266
Vehicle Equip OT	1,591
Vehicle Equip Straight	1,054
All Other OT	13,510
All Other Straight	8,630
Drug Arrests	1,802
Felony Arrests	1,047
Misdemeanor Arrests	1,736
Warrants Served	1,374

Projects in the 2021 HSP planned with 405(e) or 405(g) funds were not completed since Wisconsin did not qualify for those funds.

Police Traffic Program – Budget Summary				
Fed ID	Fund	Program Activity	Planned	Expended
2020-40-05-PT	402	Enforcement	\$1,668,604.68	\$1,143,587.56
2020-44-05-XX	405e	Enforcement	\$2,400,000	\$0.00
2020-46-05-XX	405g	Enforcement	\$1,000,000	\$0.00
		Program Totals	\$2,671,004.68	\$1,143,587.56

Traffic Records Improvement Program

Document Number	Project ID	Obligated	Approved Reimbursements	Expenditures	Balance	PO
FG-2021-UW-TOPS-05568	2021-58-03-M3	3950981-58-04	\$60,000.00	\$52,468.42	\$7,531.58	0000015898
FG-2021-UW-TOPS-05569	2021-58-03-M3	3950981-58-05	\$50,000.00	\$48,623.92	\$1,376.08	0000015894
FG-2021-UW-TOPS-05570	2021-58-03-M3	3950981-58-06	\$90,000.00	\$88,508.45	\$1,491.55	0000015886
FG-2021-UW-TOPS-05571	2021-58-03-M3	3950981-58-11	\$100,000.00	\$63,259.58	\$36,740.42	0000018037
FG-2021-UW-TOPS-05572	2021-58-03-M3	3950981-58-07	\$65,000.00	\$64,653.75	\$346.25	0000015896
FG-2021-UW-TOPS-05573	2021-58-03-M3	3950981-58-03	\$117,000.00	\$114,922.40	\$2,077.60	0000015872
FG-2021-WI DEPT -05574	2021-58-03-M3	3950981-58-13	\$55,911.00	\$52,585.48	\$3,325.52	0000018291
FG-2021-UW-MILWA-05584	2021-58-03-M3	3950981-58-01	\$24,229.00	\$24,229.00	\$0.00	0000015246
FG-2021-UW-MILWA-05641	2021-58-03-M3	3950981-58-08	\$36,012.00	\$36,004.83	\$7.17	0000016357
FG-2021-UW-MILWA-05642	2021-58-03-M3	3950981-58-09	\$33,102.00	\$33,035.38	\$66.62	0000016358
FG-2021-BOTSCT-05651	2021-58-03-M3	3950981-58-02	\$65,110.00	\$55,216.50	\$65,110.00	
FG-2021-UW-MILWA-05662	2021-58-03-M3	3950981-58-12	\$99,951.00	\$4,752.70	\$95,198.30	0000018032
FG-2021-UW-TOPS-05667	2021-58-03-M3	3950981-58-10	\$55,000.00	\$31,625.80	\$23,374.20	0000016687

In 2019, the TRCC conducted a NHTSA traffic records self-assessment. The intent of this evaluation is to help the committee set a strategic direction moving forward which will be part of the SHSP. The overarching takeaway from this evaluation was that the committee should continue to prioritize projects based on specific goals or objectives (i.e. better integration with roadway data) and that everything needs to be quantifiable. In that light, the committee selected projects for 2020 and 2021 that could quantifiably improve existing, core databases. This core, data network, in turn, supports effective analytical tools which allow BOTS' analysts and others to measure the needs of the state traffic records system.

The following is a summary of the projects funded and completed during the 2020 federal fiscal year.

Traffic Records Program and Policy Analysis

This program is 402 funded support three program evaluation analyst positions. These positions provided support to the Traffic Records Coordinating Committee as well as data requests, and complex statistical analysis. These positions assisted with strategic plan development and implementation. BOTS staff coordinated the traffic records program grant funding and assisted the Traffic Records Coordinating Committee (TRCC), which held meetings quarterly. BOTS continued to increase the number of relationships with partners as well as internal and external stakeholders.

Wisconsin Local Road Data

FG-2021-UW-TOPS-05568

Steven Parker – UW TOPS Lab (\$60,000.00)

Currently, Model Minimum Uniform Crash Criteria (MMUCC) roadway data elements roadway curvature (R2), grade (R3), and annual average daily traffic (AADT, R6) are not available for non-STN roads in Wisconsin. In this project, they proposed to add these three roadway elements for County Trunk Highway (CTH) in Wisconsin. While they would like to do this for the entire CTH system in WI, they may be limited by the cost of the data in being able to do it for all the Counties. There is 0 data in the system for these three elements. In 2019, there was crashes on the 12,588 CTH system.

Wisconsin Ignition Interlock Devices (IID) Data Dictionary

FG-2021-UW-TOPS-05569

Tracy Becker – UW TOPS Lab (\$50,000)

Uniform definitions and standardized IID data dictionary will be created. The research team collaborated with the WisDOT Bureau of Transportation Safety Chemical Test Section, vendors, and other stakeholders to improve data uniformity.

Community Maps

FG-2021-UW-TOPS-05570

Steven Parker – UW TOPS Lab (\$90,000)

A major update to the Community Maps system was rolled out in January 2018 to integrate the WisTransPortal DT4000 crash data as the primary data source in Community Maps. This roll out was coordinated with a significant outreach effort to the County TSCs and Wisconsin State Patrol regional dispatch centers. Community Maps is now updated on a nightly basis and includes crash records for all crash severity levels. The DT4000 data source integration represented a significant update to Community Maps in terms of the quantity of crash data available through the system, as well as the size, frequency, and complexity of queries that were subsequently required by stakeholders. The 2019 and 2020 project years have been primarily focused on improvements to the Basic and Advanced Search interfaces to sustain overall performance and further enhance the analysis capabilities of Community Maps. The 2021 project continued in this line, with additional focus on improvements to the “KMZ Layers” feature to support the ability to incorporate external datasets into the Community Maps visualization and analytics capabilities.

Wisconsin CODES Project - Crash Outcomes and Data Evaluation System

FG-2021-UW-TOPS-05571

Steven Parker – UW TOPS Lab (\$100,000)

Identify projects that received funding from WisDOT through the Safe Routes to School Program. Lists of projects that received funding are available on the WisDOT website. Collect crash and injury data from locations that made improvements to increase the safety of pedestrians and bicyclists. Crash and injury data will be evaluated before and after the Safe Routes to School funded project. Compare crash report injury scores by agency to linked hospital data. Publish reports to the Wisconsin Crash Outcome Data Evaluation System website.

Predictive Analytics

FG-2021-UW-TOPS-05572

Steven Parker – UW TOPS Lab (\$65,000)

This project allowed the UW TOPS Lab to continue researching and developing best practices for predicting where and under what conditions crashes occur. This allows LEAs—and the State Patrol in particular—to expend resources in the most efficient manner possible by being in place where and when crashes are most likely to occur. This visibility will lessen risky driver behaviors and may also allow for better crash outcomes by lessening response times. Specific objectives will include algorithmic improvements based on performance results from the new system and the development of an enhanced reporting feature to download and share the results of an analysis.

Wisconsin Crash Database Timelines Improvements

FG-2021-UW-TOPS-05573

Steven Parker – UW TOPS Lab (\$117,000.00)

The objective of this project was to improve the timeliness of fatal crash records in the crash database by incorporating an automated workflow tool into the Wisconsin Crash Database and Resolve System to monitor, document, and facilitate coordination of fatal crash records in the crash database. This tool will replace a stand-alone, manual process that is currently managed by the Crash Records Unit. Whereas resolution of fatal crash records is ultimately dependent on timely response from the reporting agency, a critical factor in this process is to efficiently identify, track, and report on the status of fatal crash records that are pending action. The quantitative measure is designed to target the overall timeliness for the majority of fatal crashes throughout the year, freeing staff resources to focus on resolving “problem cases” that represent the outliers.

2021 CODES Traffic Crash Record Linkage Project

FG-2021-WI DEPT -05574

Laurie Taylor – Department of Health Services (\$55,911.00)

This project linked several datasets together increasing the number of data points, variables and indicators related to traffic crashes. This project addressed the Data Use and Integration recommendations outlined in the most recent State of Wisconsin Traffic Records Assessment by enhancing the completeness and accuracy of WTC data. For this year, the project linked the 2020 WTC data to hospital inpatient and emergency department (ED) data from Wisconsin, Minnesota, and Iowa as well as to data from the Wisconsin Ambulance Run Data System (WARDS).

Pedestrian Exposure Data for the Wisconsin State Highway System: WisDOT Southeast Region Pilot Study

FG-2021-UW-MILWA-05584

Robert Schneider – UW Milwaukee (\$24,229.00)

By September 30, 2021, the pilot study should provide a pedestrian exposure model that estimates annual pedestrian crossing volume along the State Highway System in the SE Region, a trail roadway crossing crash model that estimates pedestrian and bicyclist crashes in the SE Region, a report that describes how these efforts can be used by WisDOT in practice, and lessons learned about automated video analysis for pedestrian safety.

Using Text Data from the DT4000 to Enhance Crash Analysis

FG-2021-UW-MILWA-05641

Xiao Qin – UW Milwaukee (\$36,012.00)

Develop intelligent text extraction methods for retrieving useful information from digitized crash narratives in the DT4000. Use extracted information to validate crash flags, classify crash types, assess the overall quality of a crash narrative, and enhance crash analysis.

Comprehensive Evaluation of DT4000 Data Quality for Pedestrian and Bicycle Crashes

FG-2021-UW-MILWA-05642

Xiao Qin – UW Milwaukee (\$33,102.00)

The project had two objectives: 1) performing a thorough review and quantitative analysis of all DT4000 pedestrian and bicycle crashes reported since 2017; and 2) providing recommendations for direct improvement on the completeness and quality of pedestrian and bicycle crash data.

Modernize Chem Test Data Storage and Workflow to Improve Arrest System and Driver Record Data Quality

FG-2021-BOTSCT-05651

Heather Barkholtz – Chemical Test Section DSP (\$65,110.00)

Improve the Arrest System and Driver Record data by enhancing the chemical test data workflow, minimizing unresolved URCL Arrest System table entries. This process improvement will ensure breath and blood alcohol concentration data is accurate and included in the Driver Record and Arrest System. The quality factors most improved will be timeliness, accuracy, completeness, integration, and accessibility. Hoping to improve all aspects of chemical test data quality by implementing a near real time system of data quality edit checks with prompt feedback to staff capable of reconciling errors. As chemical test data quality improves in response to this systematic approach, more refined edit checks can be implemented within this framework to facilitate continuous data quality improvements.

Development of the Crash Information Extraction, Analysis and Classification Tool (CIEACT)

FG-2021-UW-MILWA-05662

Xiao Qin – UW Milwaukee (\$99,951.00)

The goal was to develop an online Crash Information Extraction, Analysis and Classification Tool (CIEACT). The engine of the tool is the models developed from NoisyOr classifier and the neural network model GRU. The interface of the tool is an interactive crash map that can display the results and support safety analysis in a spatial context. The functions and analysis offered by CIEACT can provide safety practitioners and professionals with maximum and quick access to information stored in the texts of crash narrative and substantially reduce crash report review time.

WisTransPortal Crash and Citation Traffic Records Data Integration

FG-2021-UW-TOPS-05667

Steven Parker – UW TOPS Lab (\$55,000.00)

This project links crash and citation data, two of the core state safety datasets, within the WisTransPortal system at the University of Wisconsin-Madison. This linkage will support proactive traffic safety planning and research across all levels of government in Wisconsin. The 2021 project will focus on linking three datasets - citations, warnings, and contact summaries from the Wisconsin Badger TraCS system - for several agencies within Dane County. The objective is to start with a focused set of agency partners with overlapping geographic jurisdictions in order to lay the foundation for continued buildout of a complete, statewide linkage of crash and citation data in subsequent years. This project also includes resources to deploy an Oracle database server on the TOPS Lab WisTransPortal system for purposes of setting up the pilot data warehouse environment.

Traffic Records Improvement Program – Budget Summary				
Fed ID	Fund	Program Activity	Planned	Expended
2021-50-01-TR	402	Program Management	\$260,000.00	\$219,499.78
2021-58-03-M3	405c	Training and Support – Community	\$851,315.00	\$668,786.73
		Program Totals	\$1,111,315.00	\$888,286.51

EMS Improvement Program

Injury to Death Ratios

2017-2019 Statewide 3-year Average Baseline		71.5 to 1
2020 Actual		54.5 to 1
2020 Goal		75.1 to 1

Safety Belt Use Rate in personal injury and fatal crashes will increase

2015-2019 Statewide 5-year Average Baseline	88.60%
2020 Actual Usage	89.20%
2020 Goal	93.10%

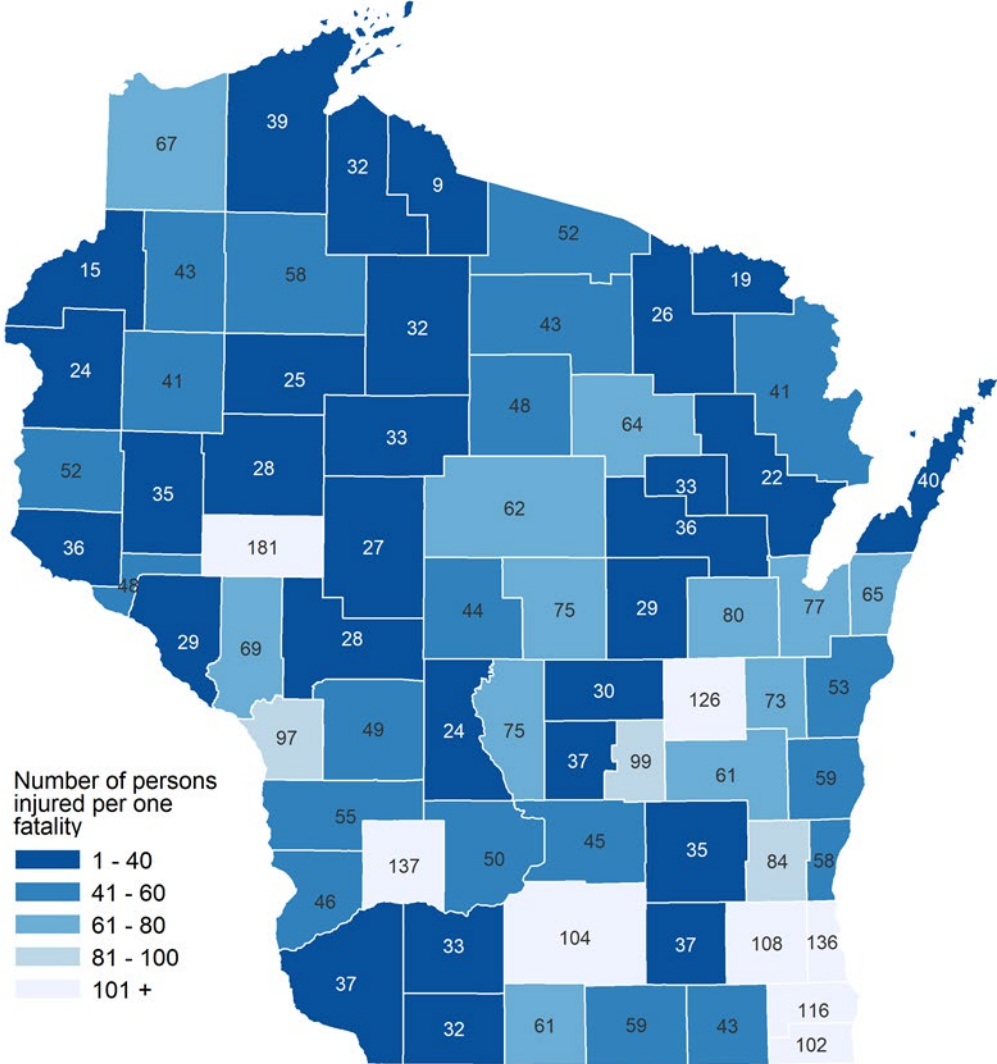
Wisconsin did not meet its goal for safety belt usage this past year. However, crash survivability varies by location in the state, which is a result of many factors including the speed and level of emergency medical response and treatment. Statewide, Wisconsin did meet its goal for the injury-to-death ratio. As has long been the case, recruitment, and retention of first responders is an increasingly significant issue in rural portions of the state. Response times are longer, and outcomes are worse for rural crashes. Three-year average injury-to-death ratios indicate that the areas of highest risk are predominantly rural.

Emergency Response Recruitment and Retention

The Bureau of Transportation Safety (BOTS) offered a grant to The Wisconsin Department of Health Services (DHS) for recruitment and retention of emergency responders for National Association of EMS Educator textbooks. The DHS EMS office did not take the opportunity to apply for the grant again this year. BOTS will be seeking a new partner in order to execute this service.

Emergency Medical Services Improvement – Budget Summary				
Fed ID	Fund	Program Activity	Planned	Expended
2020-60-03-EM	402	Training	\$50,000	\$0.00
		Program Totals	\$50,000	\$0.00

Injury to Death Ratio by County (2017-2019)



Source: WisDOT Crash Database

Motorcyclist Safety Program

Motorcyclist fatalities increased during the Covid 19 pandemic and remain consistently high. The top three contributing circumstances that occurred in these crashes are failure to control, excessive speed, and operating in a careless/erratic manner. The crash data also shows that distraction of the motorcycle operator was not a significant feature of these crashes. Impairment rates for operators were 20% higher in fatal crashes than in injury crashes. Helmet use only occurred in 29% of fatal crashes and 40% of crashes with injuries.

Motorcycle/Moped Crashes	
2015-2019 Baseline	2,271
2020 Goal	2,158
2020 Actual	2,236
Combined Fatalities and Incapacitating Injuries in Motorcycle/Moped Crashes	
2015-2019 Baseline	645
2020 Goal	613
2020 Actual	733
Motorcycle/Moped Killed and Injured	
2015-2019 Baseline	1,893
2020 Goal	1,798
2020 Actual	1,908

Program Management

The program manager coordinated, planned, and managed the WMSP. The program manager also assisted the Wisconsin rider education program through continued clerical support to training sites. This includes wage and fringe, data processing costs, materials and supplies, training and travel, printing and postage, along with providing support to coordinate safety messaging to the general public.

Motorcycle Rider Education and Training

During the 2021 motorcycle training season, the WMSP in partnership with the Motorcycle Safety Foundation (MSF) continued implementation Basic RiderCourse (BRC).

The WMSP Rider Education Program continues to administer classroom and hands-on rider training programs through the training sites funded by the Wisconsin Technical College System as well as private training sites and Harley-Davidson Riding Academy training that meet MSF and WMSP requirements for basic motorcycle/scooter, new, seasoned, and advanced motorcycle riders. The Division of Motor Vehicles reports that 8,300 skills test waivers were entered into the online system and 1,758 Class M licenses were issued via road tests during FFY2021.

Communication and Outreach

Throughout FFY 2021, local, county, and state law enforcement agencies continued to participate in motorcycle-specific activities and events. One of the largest events in the state regularly attracts 30,000-40,000 motorcyclists to northern Wisconsin each fall over a four to five-day period. In partnership with Lincoln County Sheriff's Department that brought increased patrols to the area and media saturation bring awareness to the event, the Tomahawk Ride for the Muscular Dystrophy Association.

The Mobile Outreach team achieved the milestones below:

- 11 Outreach/display event days attended
- 19 Outreach/display days scheduled

Covid-19 continued to limit community gatherings and in turn our ability to staff these events.

Program Evaluation

This project evaluated the effectiveness and consistency of program delivery for the WMSP. The program evaluated training venues to ensure that they followed curriculum provider standards. The evaluation team worked with individual coaches and sites in a mentoring capacity to bring sites and trainers into compliance with curriculum standards. WMSP Quality Assurance Specialists completed visits to 26 rider education sites for a total of 28 evaluations of the 31 training providers.

Motorcycle Awareness and Motorist Education

Using revenue generated from the sale of specialized Harley-Davidson license plates for automobiles and trucks, the WMSP developed a specific media campaign to promote motorist awareness of motorcyclists. This campaign was targeted to coincide with major motorcycling activities taking place during the most active segment of the riding season.

Motorcycle Program – Budget Summary				
Fed ID	Fund	Program Activity	Planned	Expended
2021-79-01-WI	State 562	Program Management	\$85,000.00	\$74,871.98
2021-79-01-WI	State 562	Training and Support —Community	\$463,000.00	\$230,004.51
2021-79-07-WI	State 535	Paid Media	\$180,000.00	\$117,000.00
2021-70-04-MC	402	Training and Support —Community	\$200,000.00	\$22,247.56
2021-72-04-M9	405f	Training and Support – Technical	\$30,000.00	\$4,510.78
2021-70-09-MC	402	Evaluation	\$30,000.00	\$5,550.28
		Program Totals	\$988,000.00	\$454,185.08

Pedestrian and Bicyclist Safety Program

Pedestrian-Motor Vehicle Crashes		Bicyclist-Motor Vehicle Crashes	
2015-19 Baseline	23.98/100K pop.	2015-19 Baseline	15.07/100K pop.
2020 Goal	22.78/100K pop.	2020 Goal	14.31/100K pop.
2020 Actual	18.93/100K pop.	2020 Actual	10.31/100K pop.
Combined Fatalities and Serious 'A' Injuries		Combined Fatalities and Serious 'A' Injuries	
2015-19 Baseline	5.19/100K pop.	2015-19 Baseline	1.58/100K pop.
2020 Goal	4.93/100K pop.	2020 Goal	1.5/100K pop.
2020 Actual	5.06/100K pop.	2020 Actual	1.43/100K pop.
Pedestrian Injuries		Bicyclist Injuries	
2015-19 Baseline	23.5/100K pop.	2015-19 Baseline	13.8/100K pop.
2020 Goal	22.32/100K pop.	2020 Goal	13.11/100K pop.
2020 Actual	18.3/100K pop.	2020 Actual	9.24/100K pop.

The pedestrian safety program met most of its goals for calendar year 2020 for the number of pedestrian- motor vehicle crashes and pedestrian injuries. The number of combined fatalities and suspected serious (A) injuries per 100K of population exceed the goal. There were 50 pedestrian fatalities in 2020, which is lower than the previous year’s total of 56 and equal to the 2019-2020 average.

The bicyclist safety program surpassed some of its goals for calendar year 2020. It met its goals for lowering the number of bicycle-motor vehicle crashes and bicyclist injuries but did not meet the goal for combined fatalities and A-injuries.

The pedestrian and bicyclist safety program utilizes training and outreach as well as law enforcement to reduce these fatalities and serious injuries. Public information and education activities are reported in the media and outreach program section.

Program Management

During the 2021 federal fiscal year (FFY), the incumbent for this position resigned and the duties have been shared with two other positions in the absence of a full-time program manager. The program was planned by the incumbent but managed by others in order to maintain the state pedestrian and bicyclist safety program. The Bureau of Transportation Safety (BOTS) transitioned the Teaching Safe Cycling program and MilWALKe Walks from in-person to digital because of ongoing COVID-19 protocols that continued through 2021. BOTS finalized and posted the updated informational pages on bike safety and crosswalk rules on the Wisconsin Department of Transportation (WisDOT) website which allows others to share the information. This position’s duty will continue to be shared by others until there is a full-time program manager. BOTS will continue to work with several agencies and non-profit programs to educate and improve pedestrian/bicyclist safety. The program manager is a representative on the Governor’s Bicycle Coordinating Council and the Traffic Records Coordinating Committee. We anticipate filling this position in calendar year 2022.

Teaching Safe Cycling

Teaching Safe Cycling (TSB) classes are a train the trainer style program. This program allows the lessons taught in the class to be easily replicated by the trainers who go back into their communities to educate youth on the basics of safely operating a bicycle. The classes were held in person at five locations statewide and QR codes were installed on signage in the city of Menasha to access the

YouTube videos for self-paced learning. Wisconsin Bike Fed will be tracking the performance of the code by video views. These videos are available for viewing by the general public, which has the potential to expand the reach of these trainings.

Bicycle Rodeo Supplies

In 2021 this funding was not utilized due to the COVID-19 pandemic. Targeted sites did not allow in person events.

MilWALKee Walks

BOTS continued its partnership with the Wisconsin Bike Federation on the MilWALKee Walks campaign that promotes walking and pedestrian safety. MilWALKee Walks continued its partnership with Milwaukee DPW and Milwaukee County Parks. Staff engaged with neighbors and community groups, designed and printed promotional materials, and monitored and surveyed locations to ensure a positive implementation of these new Active Streets. The focus of Active Streets was on reckless and distracted driving behaviors that involved pedestrian safety. This year, when many of the large Milwaukee festival events were cancelled, MilWALKee Walks continued to focus on the Pedestrian High Injury Network outlined in the Milwaukee Pedestrian Plan and reckless driving awareness. Volunteers and staff members held signs, helped people cross the street, and handed out law cards and safety information along the high crash corridors. Finally, MilWALKee Walks continued The Pace Car Pledge where drivers take a personal pledge to maintain the lawfully posted speed limit within the city and identify their pledge to others with a sticker (federal funds were not used). This effort was passed on to the Coalition for Safe Driving. The Pace Car Pledge is a nationally recognized initiative that has been adopted by other communities.

Designing for Pedestrian Safety

Due to limitations of agencies to participate through most of the fiscal year. Information was shared upon request.

Pedestrian/Bicycle Law Enforcement Training

In 2021 this funding was not utilized due to the COVID-19 pandemic due to limitations of agencies to meet in person and no virtual curriculum was developed.

Law Enforcement

Bicyclist and Pedestrian Traffic Law Enforcement in FFY 2021 was sustained. There were 11 bicyclist and pedestrian high-visibility enforcement (HVE) projects with the following Police Departments; Appleton, Oshkosh, Fond du Lac, Green Bay, Manitowoc, Sheboygan, West Allis, Madison, Mt. Pleasant, Racine, Beloit, and Janesville. The enforcement efforts focused on behaviors that lead to crashes—failure to yield, red light violations, speeding in advance of marked and unmarked crosswalks that can lead to failure to yield, sudden pedestrian movement, and bicyclist violation of stop signs and stop lights.

Pedestrian and Bicyclist Safety Program – Budget Summary				
Fed ID	Fund	Program Activity	Planned	Expended
2021-89-01-WI	State	Program Management	\$87,000.00	\$45,678.42
2021-80-03-PS	402	Training and Support – Technical	\$6,839.00	\$5,913.15
2021-80-04-PS	402	Training and Support -Community	\$30,000.00	\$21,137.20
2021-80-05-PS	402	Enforcement	\$221,836.16	\$142,169.66
2021-80-06-PS	402	Equipment	\$5,000	\$0
2021-80-09-PS	402	Evaluation	\$20,000	\$0
		Total	\$370,675.16	\$214,898.43

Community Traffic Safety Program

Program Management

In 2021, the Law Enforcement Liaison (LEL) Program started to return to an increased state of normalcy. By the end of the year, most of Wisconsin's County Traffic Safety Commissions (TSC) resumed both their quarterly meetings through a mixture of in-person and hybrid virtual meetings. The LEL program continued to support these traffic safety forums, either in-person or virtually.

The LELs continued to participate in ancillary traffic safety meetings and gatherings wherever possible, whether in actual physical attendance or aided by technology. This included association and committee meetings, conferences, webinars, and other related training events. In addition, the popular *Wisconsin TSC Guidelines*, first developed in 2016, were updated and expanded with additional information included to assist in the conduct of local meetings.

The LELs also spearheaded efforts at the local level to pilot a series of targeted traffic safety efforts based on Predictive Analytics, a new enhancement of Community Maps. In twelve counties chosen for their interest in serious crash reduction, this proof-of-concept pilot project expanded on previous work conducted by the State Patrol which targeted high crash locations through enhanced enforcement. This project is designed to focus not only enforcement, but to engage all stakeholders in a systems approach through the Traffic Safety Commission. The BOTSTS Analyst group provided further data analysis, as well as supporting documentation that could be used at the local level.

With expanded responsibilities covering the entire State of Wisconsin and its 72 counties, our two Regional Program Managers (RPMs) are now down to one. The RPMs have been integral in the development of many of the state's multijurisdictional high-visibility task forces. These task forces focused on various behavioral highway safety issues including OWI, safety belts, speed, and pedestrian safety. There were 95 Task Forces, 25 impaired driving, 35 seat belt, and 35 speed with 230 participating agencies in 37 counties.

Despite the pandemic, RPMs were able to continue providing excellent customer service, increased participating in national mobilizations, and coordinated Task Force activities in the most difficult of times.

Grants Management System Maintenance and Hosting

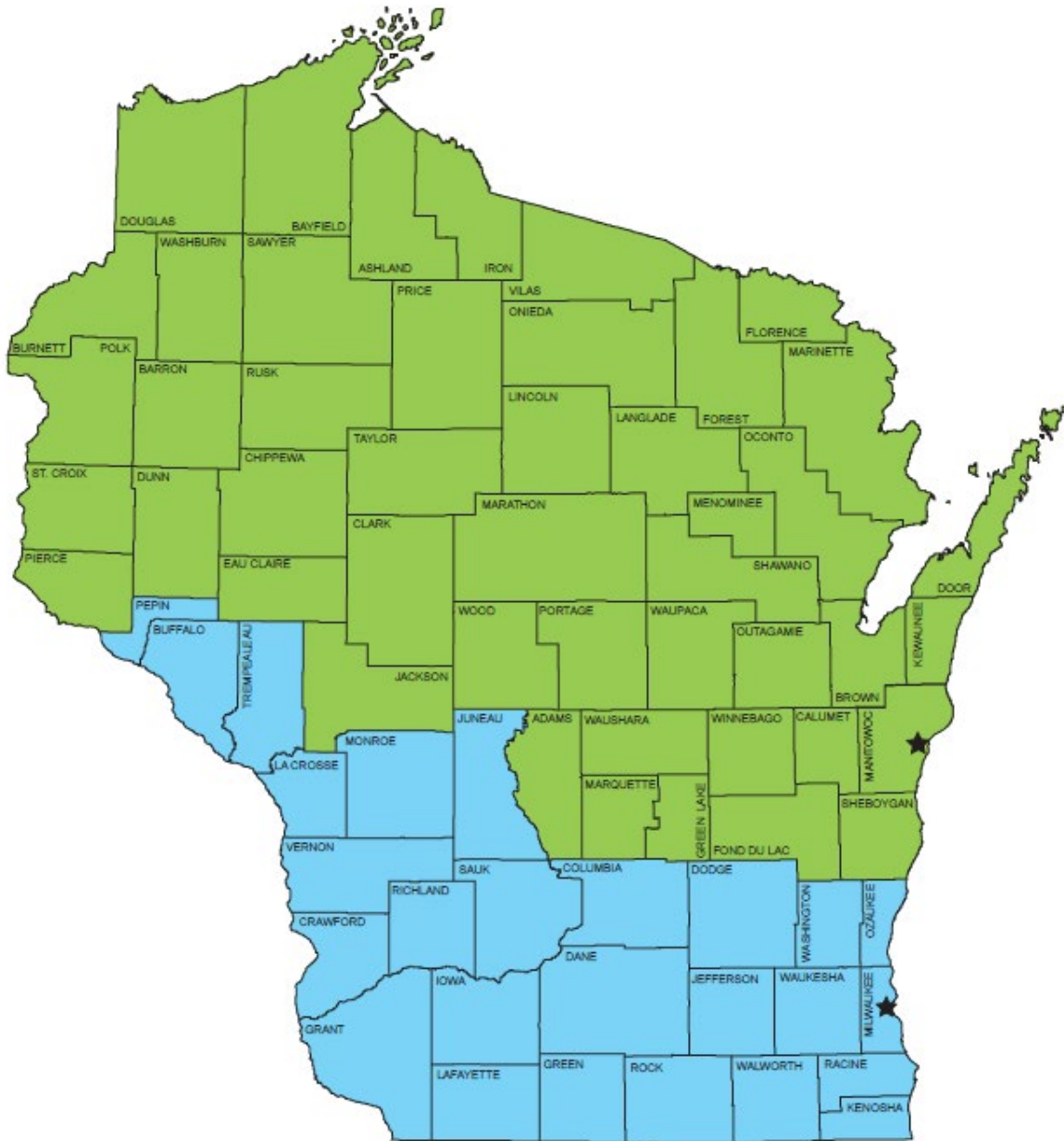
This project funded Wise-Grants, the electronic grants management system used by BOTS and its grantees.

Governor's Conference on Highway Safety and Law Enforcement Recognition Luncheon

BOTS was not able to host its annual Governor's Conference on Highway Safety due to the COVID-19 pandemic, though we did have a successful virtual event.

Community Programs – Budget Summary				
2021-90-01-CP	402	Program Management	\$525,400.00	\$507,343.73
2021-90-04-CP	402	Grants Management System / Community Outreach	\$96,500.00	\$96,100.90
2021-90-06-CP	402	Equipment	\$75,000.00	\$9,080.00
		Program Totals	\$696,900.00	\$612,524.63

Bureau of Transportation Safety (BOTS) Regions



Media and Outreach Program

Public Information and Education -Occupant Protection

For our communication efforts, our staff reviewed and updated information regarding child passenger safety (CPS) as well as safety belt materials which targeted unbuckled occupants. The Bureau of Transportation Safety (BOTS) worked with Children's Wisconsin, which provided English and Spanish CPS materials. The CPS trailer and aids were taken out for eight events. BOTS worked with a public relations agency on a Click It or Ticket paid media campaign.

In partnership with Children's Wisconsin, BOTS supported the Crossroads Teen Driving resource website. The website aims to connect teenagers, schools, and parents with programs and resources to support and cultivate a generation of safe drivers. This activity used 402 funds.

Occupant Protection Outreach Program – Rollover Convincer

The Rollover Convincer had limited deployments due to many of the events being cancelled. In total there were 14 community events equaling 20 days of activity. The Wisconsin State Patrol did use the Rollover Convincer at three Native American POW-WOWs and the Hmong Festival as both an educational outreach and community policing activity.

Communication Program—Impaired Driving

BOTS continued to develop a statewide public information and education campaign to reduce impaired driving injuries and fatalities based on NHTSA's goals and objectives. The campaign utilized various methods such as social media, radio, print, and TV. BOTS worked with a public relations agency to develop a media campaign to coincide with the Drive Sober or Get Pulled Over mobilizations. In 2021 BOTS created new content targeting young male motorcycle riders with the "Twin's Stunt Team". This was rolled out and released in time for the 2021 Drive Sober or Get Pulled Over messaging.

BOTS was awarded \$20 thousand from AAA for six Drug Impaired Education kits. Due to COVID restrictions, training and deployment of these items will not be done until November 2021. The LEL's will target who will receive an education kit among the border counties or communities that are experiencing high rates of marijuana and drug impaired driving.

BOTS was awarded \$30 thousand from Responsibility.org and GHSA to help support the Drug Recognition Expert program. These funds were utilized to schedule the second DRE school that was funded by DHS in the past, but due to budget changes they are no longer able to support. BOTS will take on this expense in order to maintain the necessary education and support of the program in future years.

Motorist Awareness and Motorcyclist Conspicuity

The Wisconsin Department of Transportation (WisDOT) and the Wisconsin Motorcycle Safety Advisory Council (MOSAC) continued their collaborative efforts to encourage active Wisconsin motorcyclists that are riding without a proper license to obtain a license to operate a class M vehicle and complete the

appropriate rider education course. The Wisconsin Motorcycle Safety Program (WMSP) training trailer traveled to 11 events equaling 19 days of activity.

BOTS provided posters to the Motorcycle Safety Advisory Committee, who delivered them to motorcycle dealerships, locations where motorcyclists congregate, driver training and rider education facilities, as well as other prominent venues. BOTS also put out television, radio, and billboard advertisements through several associations. Most of these expenses were covered using state funds.

Additional radio media ran in July surrounding the holiday to extend the awareness message. In October of FY22, the awareness media will run in the second week. This and the July media are targeting high crash weeks in the calendar year and will continue until the data or trendline improves.

BOTS in conjunction with MOSAC created the “Ride your own ride” awareness and training checklist poster. This was a print campaign of 1,000 posters delivered to known gathering places, dealerships, and motorcycle shops across Wisconsin.

Public Information and Education-Pedestrian and Bicyclist

BOTS supported Wisconsin Bicycle Federation print campaign of yard signs for cyclist safety and awareness. There were no expenses that used state or federal funds during this year to develop new PI&E material for pedestrian and bicyclist safety. Wisconsin completed the infographics and FAQs which are hosted on the WisDOT safety webpage. There was limited activity in the high-risk Pedestrian and Bicycle areas due to continuing COVID related restrictions.

Public Information and Education-Community Outreach

During FFY 2021, 215 law enforcement agencies participated in the Drive Sober or Get Pulled Over-Winter Holidays mobilization, 212 agencies participated in the Click It or Ticket mobilization, and 160 agencies participated in the Drive Sober or Get Pulled Over-Labor Day mobilization. Participation required media and community publicity activities by each agency. Results of enforcement activities are summarized in the police traffic section.

Public Information and Education-Campaign Development

BOTS continued to develop statewide public information and education materials on distracted driving, speed, impaired driving, and additional campaigns to reduce injuries and fatalities. BOTS worked with WMSN Fox 47 (MSG2Teens), Wisconsin Inter-Scholastic Athletic Association, Badger Sports, and additional safety partners to educate the public. BOTS has worked in conjunction with the Spanish News Journal, the only Hispanic press in Wisconsin, to deliver Spanish media directly to over 30k subscribers. BOTS is still seeking Hmong media outlets and community support to deliver message to their at-risk populations.

Public Information and Education and Paid Media				
Account	Fund	Program Activity	Planned	Expended
2021-80-02-PS	402	Pedestrian/Bicyclist Safety PI&E	\$20,000.00	\$3,200.00
2021-25-02-M2	402	Occupant Protection PI&E	\$249,944.00	\$205,735.65
2021-31-02-M5	405d	Impaired Driving PI&E	\$105,407.00	\$103,294.62

2021-31-07-M5	405d	Impaired Driving Paid Media	\$475,000.00	\$472,059.19
2021-70-07-MC	405f	Motorcyclist Safety PI&E	\$50,000.00	\$50,000.00
2021-90-02-CP	402	Community Traffic Safety PI&E	\$355,000.00	\$351,570.92
2021-90-07-CP	402	Community Traffic Safety PM	\$905,000.00	\$904,345.46
		Program Totals	\$2,160,351.00	\$2,090,205.84